

STAGE 1

1. PLACE ALL CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS, AND BARRICADES. SHIFT TRAFFIC AS INDICATED AND IMPLEMENT DETOUR ROUTE.
2. CONSTRUCT STORM SEWERS, DRAINAGE STRUCTURES, WATERMAIN, AND SANITARY FORCEMAIN AS SHOWN.
3. CONSTRUCT TEMPORARY PATCHES AS SHOWN ON UTILITY TRENCH PATCHING PLAN.

STAGE 2

1. PLACE ALL CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS, AND BARRICADES. SHIFT TRAFFIC AS INDICATED.
2. REMOVE EXISTING CURB AND GUTTER, PAVEMENT, AND DRIVEWAYS.
3. CONSTRUCT CURB AND GUTTER, H.M.A. BINDER COURSE, SIDEWALK AND ENTRANCES AS SHOWN.

STAGE 3

1. PLACE ALL CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS, AND BARRICADES. ADJUST TEMPORARY TRAFFIC SIGNALS AND SHIFT TRAFFIC AS INDICATED.
2. REMOVE EXISTING CURB AND GUTTER AND DRIVEWAYS AS SHOWN.
3. CONSTRUCT CURB AND GUTTER, H.M.A. BINDER COURSE, SIDEWALK AND ENTRANCES AS SHOWN.
4. INSTALL PROPOSED STREET LIGHTING.
5. PLACE TOPSOIL AND SOD.
6. PLACE SURFACE COURSE TO FINISHED GRADE, MAINTAINING TRAFFIC WITH THE NORTHBOUND ONLY DETOUR.
7. PLACE PERMANENT PAVEMENT MARKINGS AND INSTALL SIGNS.
8. REMOVE ALL CONSTRUCTION SIGNS AND OPEN ALL LANES TO TRAFFIC.

ACCESS MAINTENANCE NOTES

MAINTAINING ACCESS TO DRIVEWAYS IS OF THE UTMOST IMPORTANCE TO THE VILLAGE. ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUB-STAGING THE CONSTRUCTION OF DRIVEWAYS. THE CONTRACTOR SHALL FOLLOW THESE PROCEDURES TO ENSURE PROPER DRIVEWAY ACCESS WHEN DIRECTED BY THE ENGINEER:

1. THE CONTRACTOR SHALL WORK WITH ADJACENT BUSINESS OWNERS AND THE VILLAGE OF SCHAUMBURG TO DETERMINE DRIVEWAY RECONSTRUCTION SCHEDULING. ALL DRIVEWAY CLOSURES SHALL BE APPROVED BY THE ENGINEER.

2. TEMPORARY DRIVES SHALL BE CONSTRUCTED USING 100% RECYCLED ASPHALT PAVEMENT. THE WIDTH OF THE DRIVE SHALL BE DETERMINED BY THE ENGINEER. THE CONTRACTOR SHALL NOTE THAT THE TEMPORARY DRIVEWAY WIDTH MAY EXCEED THE WIDTH OF THE EXISTING DRIVEWAY.

THE COST OF PLACING, MAINTAINING AND REMOVING TEMPORARY DRIVES SHALL BE INCLUDED IN THE COST OF "TEMPORARY ACCESS (COMMERCIAL ENTRANCE)".

3. A 4" PVC DRAIN SHALL BE PLACED UNDER TEMPORARY DRIVES TO PROVIDE POSITIVE DRAINAGE WHEN THE ROADWAY IS EXCAVATED. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF "DRIVEWAY PAVEMENT REMOVAL".

4. QUANTITIES FOR COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL) AND COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (SPECIAL) HAVE BEEN INCLUDED FOR USE IN FRONT OF THE DRIVEWAYS IN ORDER TO EXPEDITE THE COMPLETION OF THIS WORK. THIS WORK SHALL ONLY BE USED WHEN APPROVED BY THE ENGINEER.

5. ALL BARRICADES REQUIRED TO SUB-STAGE DRIVEWAY CONSTRUCTION AND MAINTAIN ACCESS TO DRIVEWAYS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".

SIDEWALK MAINTENANCE NOTE

1. THE SIDEWALK ON ONE SIDE OF THE STREET MUST REMAIN OPEN AND ACCESSIBLE AT ALL TIMES. UTILITY RELOCATIONS SHALL BE COORDINATED WITH THE RE AND CONTRACTOR TO ENSURE ONE SIDEWALK REMAINS OPEN. SIGNING DIRECTING PEDESTRIANS TO THE OPEN SIDEWALK SHALL BE IN ACCORDANCE WITH IDOT HIGHWAY STANDARD 701801-05. THE WORK REQUIRED TO COMPLY WITH THESE REQUIREMENTS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
2. TEMPORARY AGGREGATE REQUIRED TO MAINTAIN PEDESTRIAN ACCESS ACROSS THE WORKZONE SHALL BE INCLUDED IN THE COST OF "TEMPORARY ACCESS (ROAD)".

SEQUENCE OF SIDEROAD AND DRIVEWAY CONSTRUCTION

WALNUT LANE		
STAGE 1	104+41 RT.	FLINTSHIRE DRIVE MAY NOT BE CLOSED AT THE SAME TIME AS 119+45 (DUMONT LANE)
STAGE 2	105+50 LT.	PENNVIEW LANE STAGE CONSTRUCT TO MAINTAIN TWO-WAY TRAFFIC
STAGE 2	109+50 LT.	OXFORD COURT STAGE CONSTRUCT TO MAINTAIN TWO-WAY TRAFFIC
STAGE 2	110+98 LT.	S. CLUBHOUSE ENT. MAY NOT BE CLOSED AT THE SAME TIME AS 115+86 (N. CLUBHOUSE ENT.)
STAGE 1	112+06 RT.	RICHMOND COURT STAGE CONSTRUCT TO MAINTAIN TWO-WAY TRAFFIC
STAGE 1	113+65 RT.	S. FINCHLEY COURT MAY NOT BE CLOSED AT THE SAME TIME AS 114+30 (N. FINCHLEY COURT)
STAGE 1	114+30 RT.	N. FINCHLEY COURT MAY NOT BE CLOSED AT THE SAME TIME AS 113+65 (S. FINCHLEY COURT)
STAGE 2	115+86 LT.	N. CLUBHOUSE ENT. MAY NOT BE CLOSED AT THE SAME TIME AS 110+98 (S. CLUBHOUSE ENT.)
STAGE 1	116+95 RT.	HASTINGS COURT STAGE CONSTRUCT TO MAINTAIN TWO-WAY TRAFFIC
STAGE 2	117+24 LT.	CHELTENHAM PLACE MAY NOT BE CLOSED AT THE SAME TIME AS 121+63 (DENHAM PLACE)
STAGE 1	119+45 RT.	DUMONT LANE MAY NOT BE CLOSED AT THE SAME TIME AS 104+41 (FLINTSHIRE DRIVE)
STAGE 2	121+63 LT.	DENHAM PLACE MAY NOT BE CLOSED AT THE SAME TIME AS 117+24 (CHELTENHAM PLACE)
STAGE 1	120+98 RT.	S. PUMP HOUSE DRIVE MAY NOT BE CLOSED AT THE SAME TIME AS 121+70 (N. PUMP HOUSE DRIVE)
STAGE 1	121+70 RT.	N. PUMP HOUSE DRIVE MAY NOT BE CLOSED AT THE SAME TIME AS 120+98 (S. PUMP HOUSE DRIVE)
STAGE 1	122+95 RT.	S. SHOPPING PLAZA ENT MAY NOT BE CLOSED AT THE SAME TIME AS 124+92 (N. SHOPPING PLAZA ENT.)
STAGE 2	124+73 LT.	WALNUT GREENS ENT. STAGE CONSTRUCT TO MAINTAIN TWO-WAY TRAFFIC
STAGE 1	124+92 RT.	N. SHOPPING PLAZA ENT MAY NOT BE CLOSED AT THE SAME TIME AS 122+95 (S. SHOPPING PLAZA ENT.)

TEMPORARY PAVEMENT FOR ALL STAGES SHALL CONSIST OF THE FOLLOWING:

- 6" TEMP PAVEMENT (HMA BINDER IL-19 mm)
- OR
- 6" PORTLAND CEMENT CONCRETE BASE COURSE

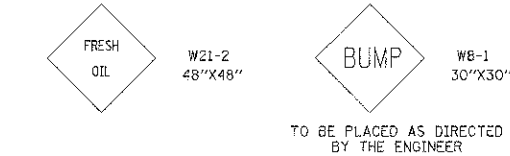
MAINTENANCE OF TRAFFIC GENERAL NOTES

1. TRAFFIC CONTROL DEPICTED IN THESE PLANS AND THE APPLICABLE IDOT DETAILS AND STANDARDS ARE THE MINIMUM REQUIREMENTS. OTHER WORK OR SIGNING MAY BE REQUIRED BY THE ENGINEER. TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, DIVISION 700; APPLICABLE GUIDELINES IN THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS; AND APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL, UNLESS HEREIN REVISED.
2. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND TRAFFIC CONTROL DEVICES SHALL FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
3. ALL CONSTRUCTION SIGNS SHALL HAVE FLUORESCENT ORANGE BACKGROUNDS.
4. ALL SIGNS SHALL BE MOUNTED ON METAL POSTS, 7 FEET ABOVE THE EXISTING GROUND AND DRIVEN A MINIMUM OF 3 FEET INTO THE GROUND. A J.U.L.I.E. LOCATE SHALL BE PERFORMED PRIOR TO THE INSTALLATION OF THE POSTS.
5. BARRICADES WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS WILL BE REQUIRED ADJACENT TO PAVEMENT EDGES WHERE WIDENING, CURB AND GUTTER OR OVERLAYING WORK IS BEING DONE, AS SPECIFIED IN SECTION 701 OF THE STANDARD SPECIFICATIONS. SPACING SHALL BE AS SHOWN ON THE CONSTRUCTION STAGING PLANS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOPS OF THE BARRICADES ARE IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF STANDARD 701901.
6. BARRICADES OR DRUMS EQUIPPED WITH ONE-WAY FLASHING LIGHTS WILL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER STRUCTURES, AND AT ANY OTHER LOCATIONS DESIGNATED BY THE ENGINEER OR LAW ENFORCEMENT AGENCIES. BARRICADES SHALL BE PLACED AT 50' CENTERS ALONG TANGENTS, 25' ALONG TAPERS AND 20' AROUND RADII.
7. DRUMS SHALL HAVE ALTERNATING REFLECTORIZED TYPE AA OR TYPE AP FLUORESCENT ORANGE AND REFLECTORIZED WHITE HORIZONTAL, CIRCUMFERENTIAL STRIPES.
8. DRUMS AND BARRICADES SHALL MEET THE REQUIREMENTS OF THE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) REPORT 350 AND THE SUPPLEMENTAL SPECIAL PROVISION "WORK ZONE TRAFFIC CONTROL DEVICES".
9. TYPE III BARRICADES ARE TO BE PLACED IN ACCORDANCE WITH STANDARD 701901 UNLESS AUTHORIZED BY THE ENGINEER TO USE AN ALTERNATE ARRANGEMENT.
10. THE CONTRACTOR SHALL INFORM THE ENGINEER OF ANY STAGE CHANGE AT LEAST TWO WEEKS IN ADVANCE OF THE CHANGE.
11. EXISTING TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE REMOVED OR RELOCATED BY THE CONTRACTOR AFTER THE TRAFFIC CONTROL REQUIREMENTS ARE MET OR AS AUTHORIZED BY THE ENGINEER. ANY SIGNS OR DEVICES LEFT IN PLACE ARE TO BE PROTECTED FROM DAMAGE AND MAINTAINED. ANY DAMAGE CAUSED BY HIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.
12. THE FIRST TWO WARNING SIGNS IN EACH DIRECTION OF TRAVEL SHALL BE EQUIPPED WITH MONO-DIRECTIONAL AMBER FLASHING LIGHTS DURING HOURS OF DARKNESS. FLAGS ARE OPTIONAL.
13. EXCEPT FOR APPROVED CLOSURES AS DEPICTED ON THE MAINTENANCE OF TRAFFIC PLANS, ALL ROADS SHALL BE KEPT OPEN TO TRAFFIC DURING THE ENTIRE CONSTRUCTION PERIOD. THE CONTRACTOR MAY CLOSE ONE LANE OF TRAFFIC (BECAUSE OF CONSTRUCTION) ONLY BETWEEN THE HOURS OF 9:00 AM AND 3:00 PM.

WHEN NECESSARY TO CLOSE ONE LANE OF THE ROADWAY ON TWO-LANE ROADS, THE CONTRACTOR SHALL MAINTAIN TWO-WAY TRAFFIC DURING THE RESTRICTED HOURS WITH THE USE OF SIGNS AND FLAGGERS AS SHOWN ON THE TRAFFIC CONTROL STANDARDS. WHEN NECESSARY TO CLOSE ONE LANE OF THE ROADWAY ON FOUR-LANE ROADS, THE CONTRACTOR SHALL MAINTAIN TWO-WAY TRAFFIC DURING THE RESTRICTED HOURS WITH THE USE OF SIGNS AND BARRICADES AS SHOWN ON THE TRAFFIC CONTROL STANDARDS. ALL EXISTING LANES OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED BETWEEN 3:00 PM AND 9:00 AM WHEN NO CONSTRUCTION ACTIVITIES ARE BEING CARRIED ON. THE ENGINEER MAY WAIVE THE LANE CLOSURE TIME RESTRICTION AT HIS DISCRETION. THE CONTRACTOR SHALL LIMIT ANY DROP-OFF BETWEEN LANES TO 1 1/2" DURING ANY OVERNIGHT PERIOD.
14. "WORKERS" SIGNS SHALL ONLY BE ERECTED WHEN WORKERS ARE PRESENT. SIGN MUST BE COVERED OR REMOVED WHEN NO WORKERS ARE PRESENT.
15. "FRESH OIL" SIGNS (W21-2-4848) WITH DATE SIGNS SHALL BE ERECTED 48 HOURS PRIOR TO PRIMING. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
16. THE CONTRACTOR SHALL ERECT TEMPORARY STREET NAME SIGNS ON METAL POSTS THROUGHOUT CONSTRUCTION TO THE SATISFACTION OF THE ENGINEER. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
17. TEMPORARY PAVEMENT MARKING TAPE SHALL BE USED ON ALL SURFACES OUTSIDE OF THE PROJECT LIMITS AND ON THE FINAL PAVEMENT SURFACE. THIS WORK SHALL BE PAID FOR AS "WET REFLECTIVE TEMPORARY TAPE TYPE III" OF THE SIZE SPECIFIED.
18. ARROW BOARDS WILL BE REQUIRED WHEN IMPLEMENTING ALL LANE CLOSURES, AND SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
19. THE COST OF SUPPLYING, ERECTING, AND MAINTAINING BARRICADES, DRUMS, WARNING LIGHTS, AND SIGNS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)". QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS, TEMPORARY PAVEMENT MARKINGS, AND WORK ZONE PAVEMENT MARKING REMOVAL ARE NOT INCLUDED IN THE ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)" AND SHALL BE MEASURED SEPARATELY FOR PAYMENT.
20. THE "ROAD CLOSED" (R11-2) SIGNS SHALL BE MOUNTED ABOVE THE TYPE III BARRICADES. ALL TYPE III BARRICADES SHALL HAVE 2 AMBER TYPE A-LOW INTENSITY FLASHING LIGHTS SPACED NEAR THE CENTERLINES OF THE SUPPORTS.
21. THE LENSES OF RAISED REFLECTIVE PAVEMENT MARKERS BEYOND THE PROJECT LIMITS SHALL BE REMOVED WHEN DIRECTED BY THE ENGINEER, WHEN THEY CONFLICT WITH THE TEMPORARY PAVEMENT MARKINGS. UPON COMPLETION OF THE PROJECT, THE LENSES SHALL BE REPLACED. THIS WORK SHALL BE INCLUDED IN THE COST OF "REPLACEMENT REFLECTOR".

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LETTERS SHALL BE 6" HEIGHT. BORDER SHALL BE 4" SPACE. TEXT LINE SPACING SHALL BE 5".