

**IDOT PROJECT LABOR AGREEMENT DETERMINATION**

To: Ann L. Schneider, Secretary

From: William R. Frey, Interim Director

Date: August 23, 2012

Re: FAI, I-70 Bridge Construction, Contract Number 74466, Cumberland County  
{November 9, 2012}

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In accordance with Executive Order 2010-03 (Quinn), it is recommended that a project labor agreement (PLA) be utilized for the above-captioned Project. This recommendation is based on the considerations indicated below.

- 1) The Project is being awarded and administered by IDOT (i.e., not by another governmental agency).
- 2) The Project is being constructed using state or local funds only (i.e., no federal funds).
- 3) The overall size, scope, sequencing, logistics or other aspects of the Project make it particularly challenging to manage, and use of a PLA is expected to help assure that the construction work is performed properly and efficiently under the circumstances.
- 4) The duration of construction activity on the Project is expected to exceed one construction season (i.e., 110 or more working days), or the nature of the Project results in a heightened need for labor force continuity and stability over a substantial period of time.
- 5) There is a firm construction completion date established for the Project thereby increasing the adverse consequences of any work stoppage or other labor disruption.
- 6) The time required to complete the Project is expected to extend beyond the expiration date of one or more existing collective bargaining agreements covering trades likely to be involved in the Project, thereby increasing the likelihood of work stoppage(s) or other labor disruption(s) during construction of the Project.
- 7) In the absence of a PLA, there is an increased likelihood of jurisdictional disputes among unions or of conflict between unionized and non-unionized workers on the Project that could have a potentially material adverse effect on the time, cost, or quality of work performed on the Project.

FAL, I-70 Bridge Construction  
Contract Number 74466  
Cumberland County  
Item

8) This project presents specific safety concerns to the traveling public and a PLA, will ensure labor force continuity and stability, decreasing the length of the safety concern.

9) Use of a PLA is expected to result in improved access to skilled labor, improved efficiency, or improved safety performance on the Project.

10) Use of a PLA on the Project is not expected to have a material adverse effect on the competitive bidding process.

11) Use of a PLA on the Project is not expected to have a material adverse effect on the ability of the Department to achieve other Departmental goals (e.g., utilization of disadvantaged businesses, utilization of Illinois domiciled businesses, development of competitive vendor alternatives over time, etc.).

12) There are other material considerations favoring or disfavoring use of a PLA on this Project as follows:

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\_\_\_\_\_  
\_\_\_\_\_

Based upon the identified considerations, we recommend that you approve use of a PLA on this Project. Upon your approval, the Department shall undertake to negotiate in good faith a PLA with the relevant labor organization(s), and shall include in all necessary bid specifications and other documents information regarding the actual or form of PLA that is to binding upon all contractors and their employees.

Agreed: [Signature] 10/18/12  
{Division Chief} (Date)

Agreed: [Signature] 10/18/12  
{Bureau of Design & Environment} (Date)

Agreed: [Signature] 10-5-12  
{Regional Engineer} (Date)

Approved: [Signature] 10-24-12  
Ann L. Schneider, Secretary (Date)

FHWA concurrence in the PLA for the above mentioned contract

Gregory G. Nadeau	10/01/2012
FHWA Deputy Administrator	(see attached approval page)

**Attachment A:**

Justification for the use of Project Labor Agreement on Contract # 74466, Cumberland County

The use of a Project Labor Agreement on this project is consistent with all State and Local statutory and regulatory requirements.

**Item 2:** This project is federally funded with a state match.

**Item 3:** The Estimated Cost of this Project is \$7,000,000.

The work on this project is located on FAI 70, Marked Route I-70 west of IL 130 interchange north of Greenup. The average daily traffic at this location is 19,000 with 49% (9,375) trucks. In order to complete the I-70 Bridge Construction project without any adverse traffic delays for the traveling public or delays in commerce, IDOT feels a Project Labor Agreement is necessary for this project.

The work on this project consists of superstructure replacement on 4 interstate structures, removing 2 interstate structures and replacing with embankment & hot mix asphalt pavement, milling, guardrail and pavement marking. With the large number of different construction trades that will be involved in the bridge construction on this section of roadway, IDOT feels the use of a Project Labor Agreement is necessary to avoid any labor related delays.

The bridge work involves various construction sequences creating numerous traffic control challenges. The bridge work will be done with crossovers having both directions of traffic on one side of the interstate but separated by barrier wall. The contractor will need a reliable work force to complete the work in a timely manner to maintain safe travel for the traveling public during all stages of traffic control. It is IDOT's findings that the large skilled workforce needed to meet all the challenges with this project can be provided by the union trades involved and is a necessary requirement for a Project Labor Agreement.

The posted speed limit is 65 MPH. Once the projects limits are under construction the speed limit will be 55 MPH while no workers are present. Based on user cost of \$10.00/hour for passenger vehicles and \$20.00/hour for trucks, a user cost of \$555/day will incur due to any work stoppages. Based on the adverse travel due to construction zones, IDOT feels the use of a Project Labor Agreement is necessary.

**Item 4 & 5:** There is an interim completion date of November 15, 2013 for stage 1 and a firm completion date of November 14, 2014 for the project. Stage 1 of the project will need to be completed in one construction season to avoid any lane closures during the winter season. Then stage 2 will need to be completed the following year. To meet this completion date the contractor will need an accelerated work schedule, so avoiding any possible labor issues would be a benefit to the project. A Project Labor Agreement would ensure the stability of the work force allowing the work to be completed in the time allotted.

**Item 6:** This project could extend beyond the expiration date of the collective bargaining agreements with the following locals.

Operators Local 841 contract expires 12-31-2012

Concrete Finishers Local 143 contract expires 3-31-2013

Iron Workers Local 46 contract expires 4-30-2013

Labor Local 159 contract expires 4-30-2013

Teamsters Local 26 contract expires 4-30-2014

<b>Work Zone User cost calculations</b>		
		8/23/2012
<b>Project Number:</b>		
<b>CRS:</b>		
<b>No lane closure cost calculation procedure</b>		
	<b>Passenger Car</b>	<b>B/C Truck</b>
<b>Cost per hour:</b>	\$19.22	\$51.88
<b>Length of Work zone in miles:</b>	4.2	4.2
<b>Free flow speed (normal 85% speed) in mph:</b>	72	72
<b>Work zone speed (85%) in mph:</b>	62	62
<b>Average AADT of full section:</b>	45000	15000
<b>Duration of Closure in days</b>	300	300
<b>Calculated values:</b>		
<b>Travel time in free flow (secs):</b>	210	210
<b>Travel Time in work zone (secs):</b>	244	244
<b>Delay (secs):</b>	34	34
<b>Delay (hours):</b>	0.009408602	0.009408602
<b>Cost per Vehicle:</b>	\$0.181	\$0.488
<b>Cost per day per closure:</b>	\$8,137.50	\$7,321.77
<b>Total Cost for closure duration:</b>	\$2,441,250.00	\$2,196,532.26
<b>Total Cost for all vehicles:</b>	<b>\$4,637,782.26</b>	
<b>Average cost per day</b>	<b>\$15,459.27</b>	

The Average cost per day is the MAXIMUM that may be used as incentive / disincentive

Spreadsheet protection password: CONSTRUCTION

See Contact Reynaldo Stargell in the Office of Traffic Engineering for the latest adjustment factor based on the CPI : <ftp://ftp.bls.gov/pub/special.requests/cpi/cpiat.txt>

### Work Zone User cost calculations

8/23/2012

**Project Number: 74466 Bridge Construction**

**CRS: 4.9**

#### Detour cost calculation procedure

	Passenger Car	B/C Truck
<b>Cost per hour:</b>	<b>\$10.00</b>	<b>\$20.00</b>
<b>Length of work zone in miles:</b>	<b>0.7</b>	<b>0.7</b>
<b>Length of Detour in miles:</b>	<b>0.7</b>	<b>0.7</b>
<b>Free flow speed (normal 85% speed) in mph:</b>	<b>65</b>	<b>65</b>
<b>Detour zone speed (85%) in mph:</b>	<b>55</b>	<b>55</b>
<b>Average AADT of full section:</b>	<b>9625</b>	<b>9375</b>
<b>Duration of Closure in days</b>	<b>730</b>	<b>730</b>
<b>Calculated values:</b>		
Travel time in free flow (secs):	38.76923077	38.76923077
Travel Time in detour (secs):	46	46
Delay (secs):	7	7
Delay (hours):	0.001958042	0.001958042
Cost per Vehicle:	\$0.02	\$0.04
Cost per day per closure:	\$188.46	\$367.13
Total Cost for closure duration:	\$137,576.92	\$268,006.99
<b>Total Cost for all vehicles:</b>	<b>\$405,583.92</b>	
<b>Average cost per day</b>	<b>\$555.59</b>	

The Average cost per day is the MAXIMUM that may be used as incentive / disincentive

Spreadsheet protection password: CONSTRUCTION

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<b>Work Zone User cost calculations</b>		8/23/2012
<b>Project Number:</b>		
<b>CRS:</b>		
<b>Detour cost calculation procedure</b>		
	<b>Passenger Car</b>	<b>B/C Truck</b>
<b>Cost per hour:</b>	\$19.22	\$51.88
<b>Time to drive existing route at free-flow speed in minutes:</b>	4	4
<b>Time to drive detour route in minutes:</b>	15	15
<b>Average AADT of full section:</b>	500	20
<b>Duration of Closure in days</b>	20	20
<b>Calculated values:</b>		
<b>Delay (minutes):</b>	11	11
<b>Delay (hours):</b>	0.183333333	0.183333333
<b>Cost per Vehicle:</b>	\$3.52	\$9.51
<b>Cost per day per closure:</b>	\$1,761.83	\$190.23
<b>Total Cost for closure duration:</b>	\$35,236.67	\$3,804.53
<b>Total Cost for all vehicles:</b>	<b>\$39,041.20</b>	
<b>Average cost per day</b>	<b>\$1,952.06</b>	

The Average cost per day is the MAXIMUM that may be used as incentive / disincentive

Spreadsheet protection password: CONSTRUCTION

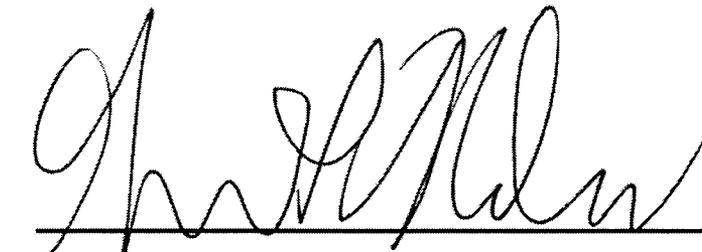
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## PLA Request

Approval of Project Labor Agreement

Disapproval of Project Labor Agreement

**Reason for disapproval:**

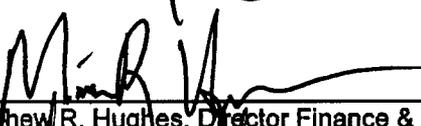
  
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**Signature**

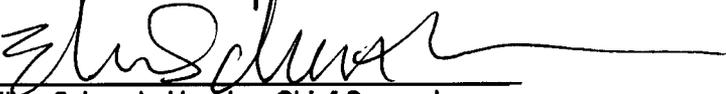
10/1/12  
\_\_\_\_\_  
**Date**

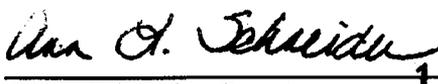
**Execution Page**

**Illinois Department of Transportation**

  
\_\_\_\_\_  
William R. Frey, Interim Director of Highways

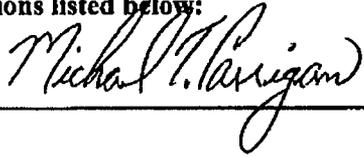
  
\_\_\_\_\_  
Matthew R. Hughes, Director Finance & Administration

  
\_\_\_\_\_  
Ellen Schanzle-Haskins, Chief Counsel

  
\_\_\_\_\_  
Ann L. Schneider, Secretary

10.24.12  
(Date)

**Illinois AFL-CIO Statewide Project Labor Agreement Committee, representing the local unions listed below:**

  
\_\_\_\_\_

October 16, 2012  
(Date)

**List Union Locals:**



Jim Allen  
Bricklayers



Curtis Cade  
United Association

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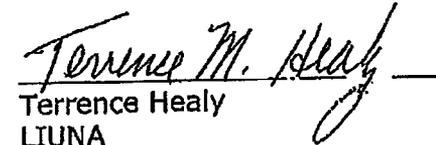
Ed Christensen, Elevator  
Constructors



Terry Fitzmaurice  
Painters



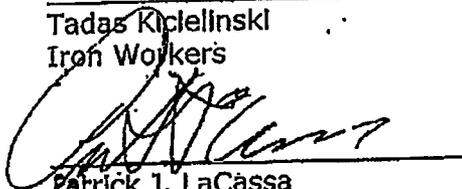
Pat Gleason  
Teamsters



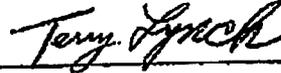
Terrence M. Healy  
LIUNA



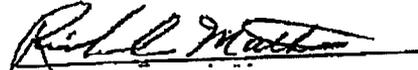
Tadas Kiciejewski  
Iron Workers



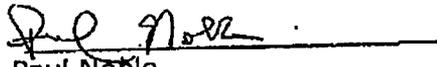
Patrick J. LaCassa  
OPCMIA



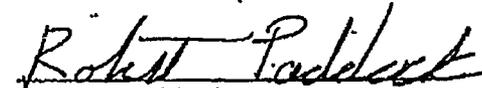
Terry Lynch  
Heat & Frost Insulators & Allied  
Workers



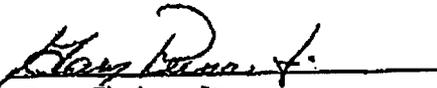
Richard Mathis  
Roofers



Paul Noble  
IBEW



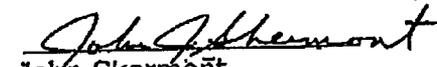
Robert Paddock  
IUOE



Gary Perinar Jr.  
Carpenters



Robert Schneider  
Sheet Metal Workers



John Skermont  
Boilermakers

\*only if Elevator Constructors master agreement  
language is attached to PLA