

041

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	[(8R-HB)BR]BJR	HENDERSON	22	1
BRIDGE REPAIRS		ILLINOIS	CONTRACT NO. 68E25	

**INDEX OF SHEETS:**

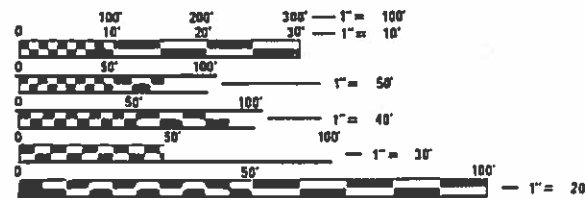
- 1. COVER SHEET
- 2-3. GENERAL NOTES & COMMITMENTS
- 4-5. SUMMARY OF QUANTITIES
- 6-7. TYPICAL SECTIONS
- 8. SCHEDULE OF QUANTITIES
- 9. GENERAL LOCATION OVERVIEW
- 10-13. PROPOSED TRAFFIC CONTROL
- 14. TRAFFIC CONTROL DETAILS
- 15-19. STRUCTURAL DETAILS
- 20. PROTECTIVE COATING DETAIL
- 21-22. D4 STANDARDS

**HIGHWAY STANDARDS:**

- 000001-06
- 001001-02
- 642006            701001-02
- 643001-02        701006-05
- 701400-09        701011-04
- 701401-11        701402-12
- 701411-09
- 701423-10
- 701901-07
- 704001-08
- 780001-05

**OTHER PRINCIPAL ARTERIAL**

2017 AADT = 11900  
 MU = 1400  
 SU = 250



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811

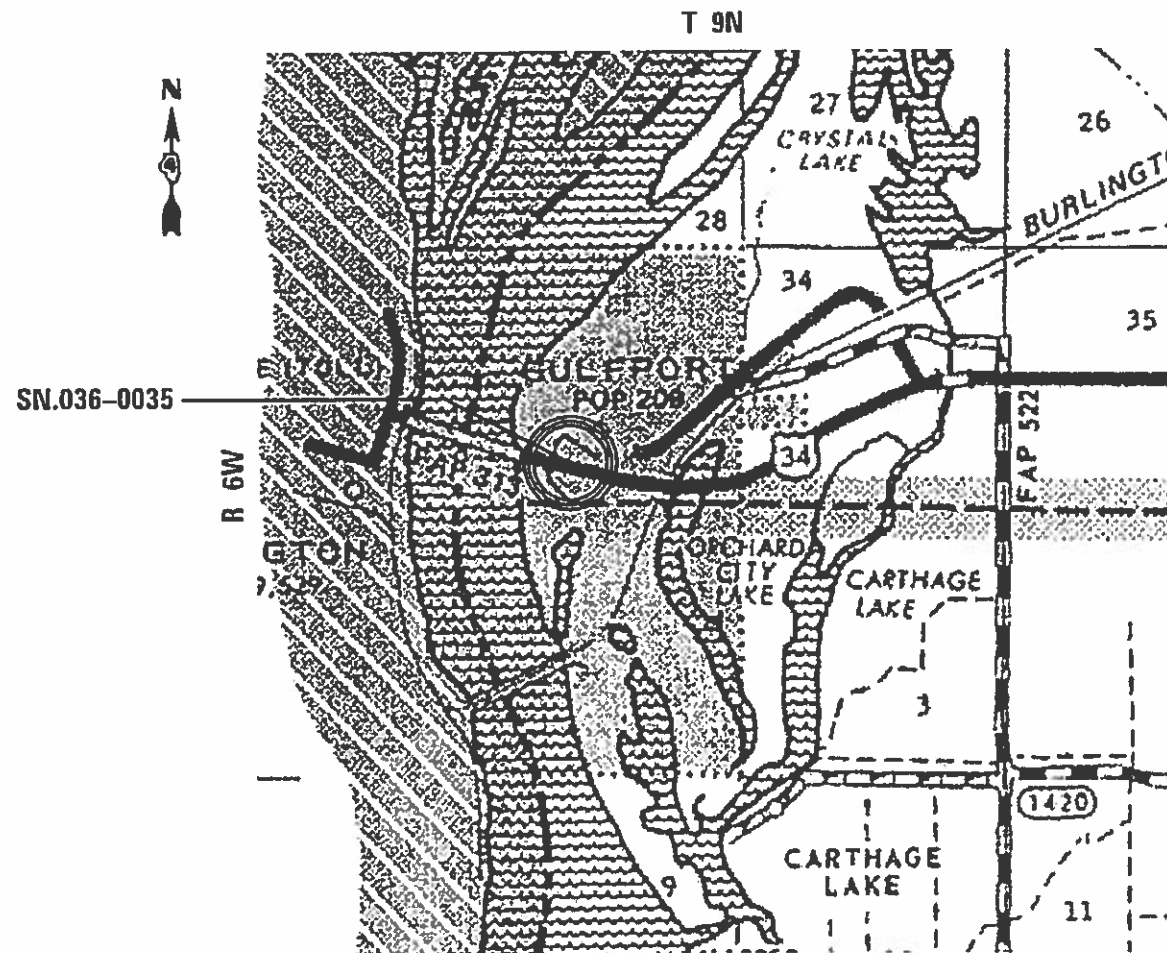
PROJECT DESIGNER: JOSH JOCHUMS (309) 671-3473  
 PROJECT MANAGER: MARK ECKHOFF (309) 671-4463

CONTRACT NO. 68E25 CAT. NO. 035612-00D

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

**PROPOSED  
 HIGHWAY PLANS**

FAP 313 (US34)  
 SECTION [(8R-HB)BR]BJR  
 FEDERAL PROJECT NHPP-D28N(887)  
 BRIDGE REPAIRS  
 HENDERSON COUNTY  
 C-94-060-18



GROSS LENGTH = 148 FT. = 0.028 MILE  
 NET LENGTH = 148 FT. = 0.028 MILE

D-94-038-18



This project consists of bridge joint repairs and parapet wall coating to S.N. 036-0035 carrying US34 over Chinn St. at Gulfport.

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUBMITTED Aug 16 2018  
Kensel A Garna (KSD)  
 REGION THREE ENGINEER

[Signature]  
 ENGINEER OF DESIGN AND ENVIRONMENT

[Signature]  
 DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION 3

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 OF THE STATE OF ILLINOIS

# GENERAL NOTES

## AVAILABILITY OF ELECTRONIC FILES

MicroStation and GEOPAK files of this project will be made available to the Contractor after contract award. If there is a conflict between the electronic files and the printed contract plans and documents, the printed contract plans and documents shall take precedence over the electronic files. The Contractor shall accept all risk associated with using the electronic files and shall hold the Department harmless for any errors or omissions in the electronic files and the data contained therein. Errors or delays resulting from the use of the electronic files by the Contractor shall not result in an extension of time for any interim or final completion date or shall not be considered cause for additional compensation. The Contractor shall not use, share, or distribute these electronic files except for the purpose of constructing this contract. Any claims by third parties due to use or errors shall be the responsibility of the Contractor. The Contractor shall include this disclaimer with the transfer of these electronic files to any other parties and shall include appropriate language binding them to similar responsibilities.

## CRITICAL PATH WORK SCHEDULE REQUIREMENT

The Contractor will submit to the Engineer a satisfactory progress schedule and critical path schedule which shall show the proposed sequence of work at the time of the pre-construction conference.

## ENVIRONMENTAL REVIEWS

Prior to the use of any proposed borrow areas, use areas (temporary access roads, detours, run-arounds, etc.) and/or waste areas, the Contractor shall file the required environmental resource request surveys according to Section 107.22 of the Standard Specifications. These surveys are required in order for the Department to conduct cultural and biological resource surveys for the proposed site.

The required environmental resource documentation shall include the following:

- \* BDE Form 2289 (Cultural and Natural Resources Review of Borrow Areas)
- \* BDE Form 2290 (Waste/Use Area Review)
- \* A location map showing the size limits and location of the use area
- \* Color photographs depicting the use area
- \* Borrow Area Entry Agreement form - D4 PI0101

Prior to any waste materials being removed from the construction site the required environmental resource surveys shall be obtained and filed by the Contractor. Excess waste products removed from the construction site shall be disposed of as required in Section 202.03 of the Standard Specifications.

Any protruding metal bars shall be removed prior to the disposal of broken concrete at approved disposal sites.

Please note that a minimum of four weeks shall be allowed for the District to obtain the required environmental clearances and six weeks for the required borrow site environmental clearances.

## POLYMERIZED BITUMINOUS MATERIALS (TACK COAT) RATES

Surface Type	Residual Rate
Milled (HMA or PCC)	0.08 lb /sq ft
Existing Pavement	0.04 lb /sq ft
Fog Coat (between lifts)	0.04 lb /sq ft

## SECURING DRAINAGE STRUCTURE GRATES

Prior to routing traffic onto the shoulders as shown in the staging plans, the Contractor shall secure gratings on shoulder inlets as directed by the Engineer. This work will not be paid for separately, but shall be included in the cost of TRAFFIC CONTROL AND PROTECTION, STANDARD 701423.

## ADDITIONAL SUPPLEMENTAL TRAFFIC CONTROL

The Department reserves the right at any time to add additional Traffic Control Systems or devices within the active contract limits, by means of an additional contract. All terms of Article 105.08 of the Standard Specifications shall be followed by each Contractor.

## SIGNING

Sign locations may vary from the stations shown on the plans in accordance with directions from the Engineer at the time of construction. Sign locations may be adjusted in the field to avoid any found utilities.

All wood post locations shall be verified with the Bureau of Operations, Traffic Section, before installation.



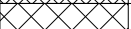
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	PLOT DATE = 8/16/2018	DATE -	REVISED -			ILLINOIS FED. AID PROJECT				
				SCALE:	SHEET OF SHEETS	STA.	TO STA.			

PROJECT SPECIFIC NOTES

1. ALL NEW CONCRETE SUPERSTRUCTURE SHALL BE TREATED WITH PROTECTIVE COAT ONLY. THE REMAINING EXISTING CONCRETE PARAPET AND MEDIAN BARRIER SHALL BE TREATED WITH PROTECTIVE COAT (SPECIAL) AND SURFACE FILLER (SPECIAL).

HMA SHOULDERS, 8"

The following mixture requirements are applicable to this project:

	HMA LIFT	AC/PG:	Design Air Voids:	Mixture Composition: (Gradation Mixture)	Friction Aggregate:	Quality Mgmt. Prog.
1.5 Inch	 Surface Lift	PG 64-22	4% @ N=50	IL 9.5	Mix "D"	QC/QA
2.5 Inch	 2nd Binder Lift	PG 64-22	4% @ N=50	IL 19.0	N/A	QC/QA
4.0 Inch	 1st Binder Lift	PG 64-22	4% @ N=50	IL 19.0	N/A	QC/QA

- Note: 1. Individual lift thickness of each type will not be less than 3 times nominal maximum aggregate size, unless otherwise approved by the Engineer.  
 2. For design purposes, mixture weight for all mixes is determined to be 112.0 lbs./s.y./in. unless other wise noted.  
 3. HMA Shoulders, 8" shall be placed in three seperate lifts.

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				80/20	
				FED/STATE	
				BRIDGE	
				047	
				036-0035	
40600295	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	POUND	478	478	
42001300	PROTECTIVE COAT	SO YD	32.4	32.4	
44004250	PAVED SHOULDER REMOVAL	SO YD	1329	1329	
48203029	HOT - MIX ASPHALT SHOULDERS, 8"	SO YD	1329	1329	
50102400	CONCRETE REMOVAL	CU YD	24.9	24.9	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	24.9	24.9	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2640	2640	
50800515	BAR SPLICERS	EACH	48	48	
52000110	PREFORMED JOINT STRIP SEAL	FOOT	164	164	
64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	1120	1120	
67100100	MOBILIZATION	L SUM	1	1	
70100325	TRAFFIC CONTROL AND PROTECTION, STANDARD 701423	EACH	2	2	
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	1	1	
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1	

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PLOT DATE = 8/16/2018	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	[(8R-HB)BR]BJR	HENDERSON	22	4
			CONTRACT NO. 68E25	
ILLINOIS FED. AID PROJECT				

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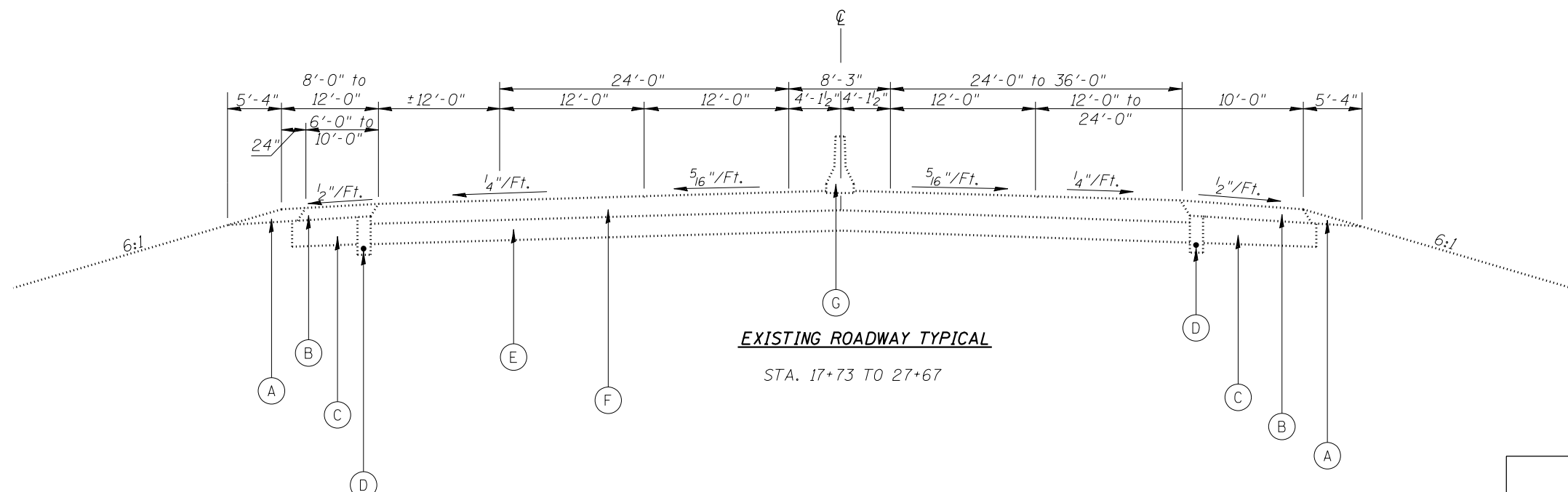
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				80/20	
				FED/STATE	
				BRIDGE	
				047	
				036-0035	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	35	35	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	2414	2414	
70300904	PAVEMENT MARKING TAPE, TYPE IV 4"	FOOT	7314	7314	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1126	1126	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1126	1126	
70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
70600350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
* 78001100	PAINT PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	83	83	
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	1854	1854	
X0326444	SURFACE FILLER (SPECIAL)	GALLON	5	5	
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SO FT	699	699	
X5030550	PROTECTIVE COAT (SPECIAL)	SO YD	394	394	

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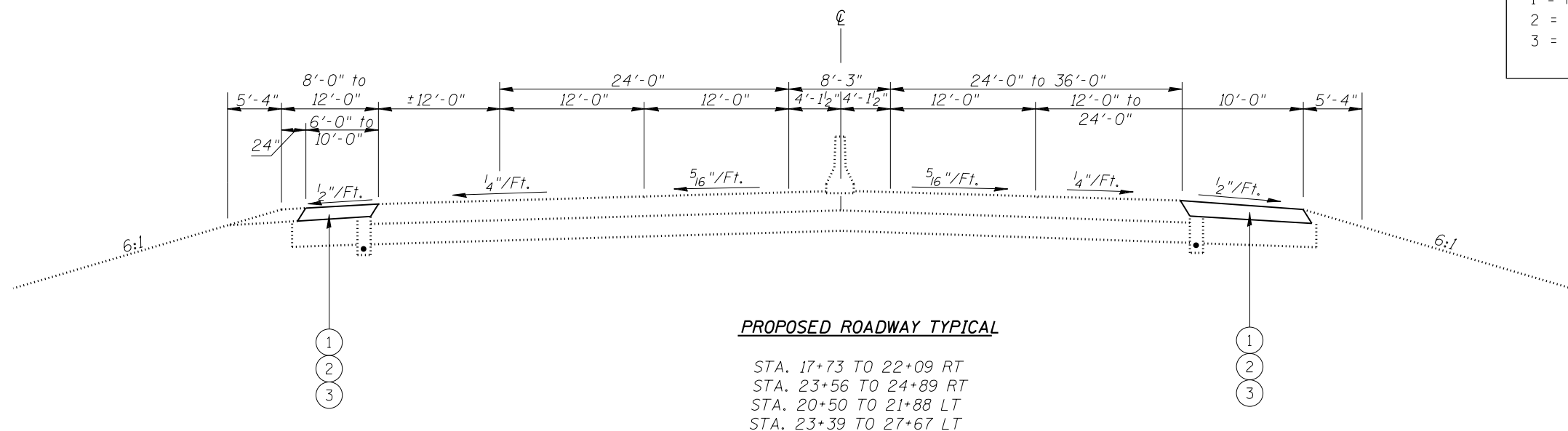
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

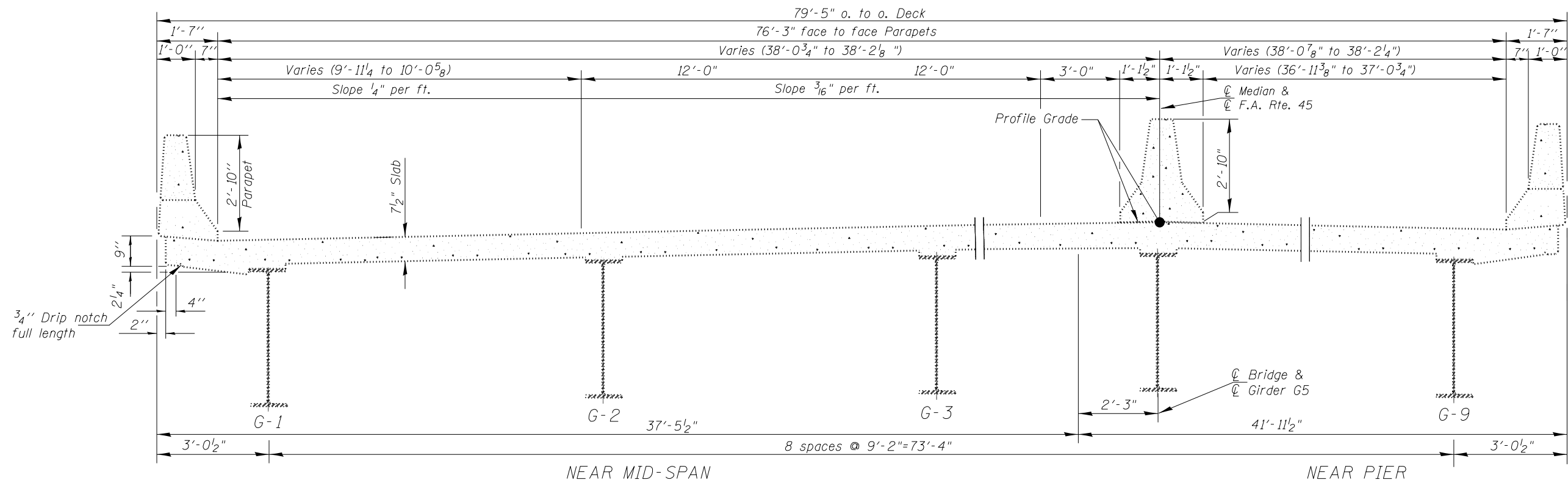
**SUMMARY OF QUANTITIES**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	[(8R-HB)BR]BJR	HENDERSON	22	5
				CONTRACT NO. 68E25
ILLINOIS FED. AID PROJECT				



LEGEND	
A	EXISTING AGGREGATE SHOULDERS
B	EXISTING HMA SHOULDERS, 8"
C	EXISTING SUB-BASE GRANULAR MATERIAL, TYPE C
D	EXISTING PIPE UNDERDRAIN, 4"
E	EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A 12"
F	EXISTING FULL DEPTH HMA PAVEMENT, 11"
G	EXISTING MEDIAN CONCRETE BARRIER
1 = PAVED SHOULDER REMOVAL	
2 = POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	
3 = HMA SHOULDERS, 8"	





**EXISTING BRIDGE TYPICAL**

STA. 22+00 TO 23+48

FILE NAME =	USER NAME = jochumsjg	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL CROSS-SECTIONS STRUCTURE NO. 036-0035</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 8/16/2018	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				
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TABULATION OF RESURFACING QUANTITIES							
STATION	TO	STATION	OFFSET	AREA	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	PAVED SHOULDER REMOVAL	HMA SHOULDER, 8"
				SQ YD	FOG COAT POUND	SQ YD	SY
17+73	TO	22+00	RT	487	175	487	487
23+57	TO	24+89	RT	149	54	149	149
20+50	TO	21+92	LT	192	69	192	192
23+48	TO	27+67	LT	501	180	501	501
<b>GRAND TOTAL</b>					<b>478</b>	<b>1329</b>	<b>1329</b>

TEMPORARY CONCRETE BARRIER							
LOCATION					TEMPORARY CONCRETE BARRIER	TEMPORARY CONCRETE BARRIER	RELOCATE TEMPORARY CONCRETE
STA	TO	STA	OFFSET	NOTE	FOOT		FOOT
18+96	TO	23+70	RT	STAGE 1	474		
21+80	TO	26+67	LT	STAGE 1	487		
				STAGE 2		165	
18+06	TO	23+68	RT	STAGE 2			562
21+81	TO	27+45	LT	STAGE 2			564
SUBTOTAL					961	165	1126
<b>GRANDTOTAL</b>					<b>1126</b>		<b>1126</b>

PAVEMENT MARKING REMOVAL (WATER BLASTING)							
LOCATION					SKIP-DASH WHITE 4"	SOLID YELLOW 4"	LETTERS AND SYMBOLS
STA	TO	STA	OFFSET	NOTES	SF	SF	SF
13+02	TO	24+89	RT	STAGE 1 CENTERLINE	97.9		
21+50	TO	32+47	LT	STAGE 1 CENTERLINE	90.5		
17+73	TO	24+89	RT	STAGE 1 EDGELINE		238.6	
22+00	TO	27+67	LT	STAGE 1 EDGELINE		189.0	
		20+60	LT				41.5
		21+60	LT				41.5
SUBTOTAL					188.4	427.6	83.0
<b>GRAND TOTAL</b>					<b>699</b>		

BRIDGE JOINT REPAIR ITEMS						
LOCATION	CONCRETE REMOVAL	CONCRETE SUPERSTRUCTURE	REINFORCEMENT BARS, EPOXY COATED	BAR SPLICERS	PREFORMED JOINT STRIP SEAL	PROTECTIVE COAT
	CY	CY	POUNDS	EACH	FOOT	SY
EAST JOINT	12.45	12.45	1320	24	82	16.2
WEST JOINT	12.45	12.45	1320	24	82	16.2
<b>TOTAL</b>	<b>24.9</b>	<b>24.9</b>	<b>2640</b>	<b>48</b>	<b>164</b>	<b>32.4</b>

SHOULDER RUMBLE STRIPS, 8 INCH					
LOCATION					LENGTH
STA	TO	STA	OFFSET		FT
17+73	TO	22+00	RT		427
23+57	TO	24+89	RT		132
20+50	TO	21+92	LT		142
23+48	TO	27+67	LT		419
<b>TOTAL</b>					<b>1120</b>

PROTECTIVE COAT (SPECIAL)					
LOCATION				SURFACE FILLER (SPECIAL)	PROTECTIVE COAT (SPECIAL)
				VOLUME	AREA
STA	TO	STA	NOTES	GALLON	SY
21+86	TO	23+61	NORTH PARAPET	1.6	127
21+86	TO	23+61	MEDIAN BARRIER	1.6	141
21+86	TO	23+61	SOUTH PARAPET	1.6	127
<b>TOTAL</b>				<b>5</b>	<b>394</b>

MOBILIZATION	
LOCATION	L SUM
JOBSITE	1.0
<b>TOTAL</b>	<b>1.0</b>

TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	
LOCATION	EACH
WESTBOUND TRAFFIC	1
EASTBOUND TRAFFIC	1
<b>TOTAL</b>	<b>2</b>

TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	
LOCATION	EACH
EASTBOUND TRAFFIC	1
<b>TOTAL</b>	<b>1</b>

TRAFFIC CONTROL AND PROTECTION, STANDARD 701423	
LOCATION	EACH
WESTBOUND TRAFFIC	1
EASTBOUND TRAFFIC	1
<b>TOTAL</b>	<b>2</b>

TRAFFIC CONTROL SURVEILLANCE	
LOCATION	CAL DA
JOBSITE	35
<b>TOTAL</b>	<b>35</b>

PAINT PAVEMENT MARKING						
LOCATION				SKIP-DASH WHITE 4"	SOLID YELLOW 4"	LETTERS AND SYMBOLS
STA	TO	STA	OFFSET	FOOT	FOOT	SF
13+02	TO	24+89	RT	296.8		
21+50	TO	32+47	LT	274.3		
17+73	TO	24+89	RT		716.0	
22+00	TO	27+67	LT		567.0	
		20+60	LT			41.5
		21+60	LT			41.5
SUBTOTAL				571.0	1283.0	83.0
<b>GRAND TOTAL</b>				<b>1854</b>		<b>83</b>

PAVEMENT MARKING TAPE, TYPE IV, 4"					
LOCATION					SOLID WHITE 4"
STA	TO	STA	OFFSET	NOTES	FT
6+42	TO	23+70	RT	STAGE 1	1728
17+73	TO	24+81	RT	STAGE 1	708
21+80	TO	39+07	LT	STAGE 1	1727
20+50	TO	27+67	LT	STAGE 1	717
12+70	TO	16+23	RT	STAGE 2	353
16+84	TO	23+70	RT	STAGE 2	686
21+80	TO	35+75	LT	STAGE 2	1395
<b>TOTAL</b>					<b>7314</b>

SHORT TERM PAVEMENT MARKING REMOVAL					
LOCATION					SOLID WHITE 4"
STA	TO	STA	OFFSET	NOTES	SF
6+42	TO	23+70	RT	STAGE 1	570
17+73	TO	24+81	RT	STAGE 1	234
21+80	TO	39+07	LT	STAGE 1	570
20+50	TO	27+67	LT	STAGE 1	237
12+70	TO	16+23	RT	STAGE 2	116
16+84	TO	23+70	RT	STAGE 2	226
21+80	TO	35+75	LT	STAGE 2	460
<b>TOTAL</b>					<b>2414</b>

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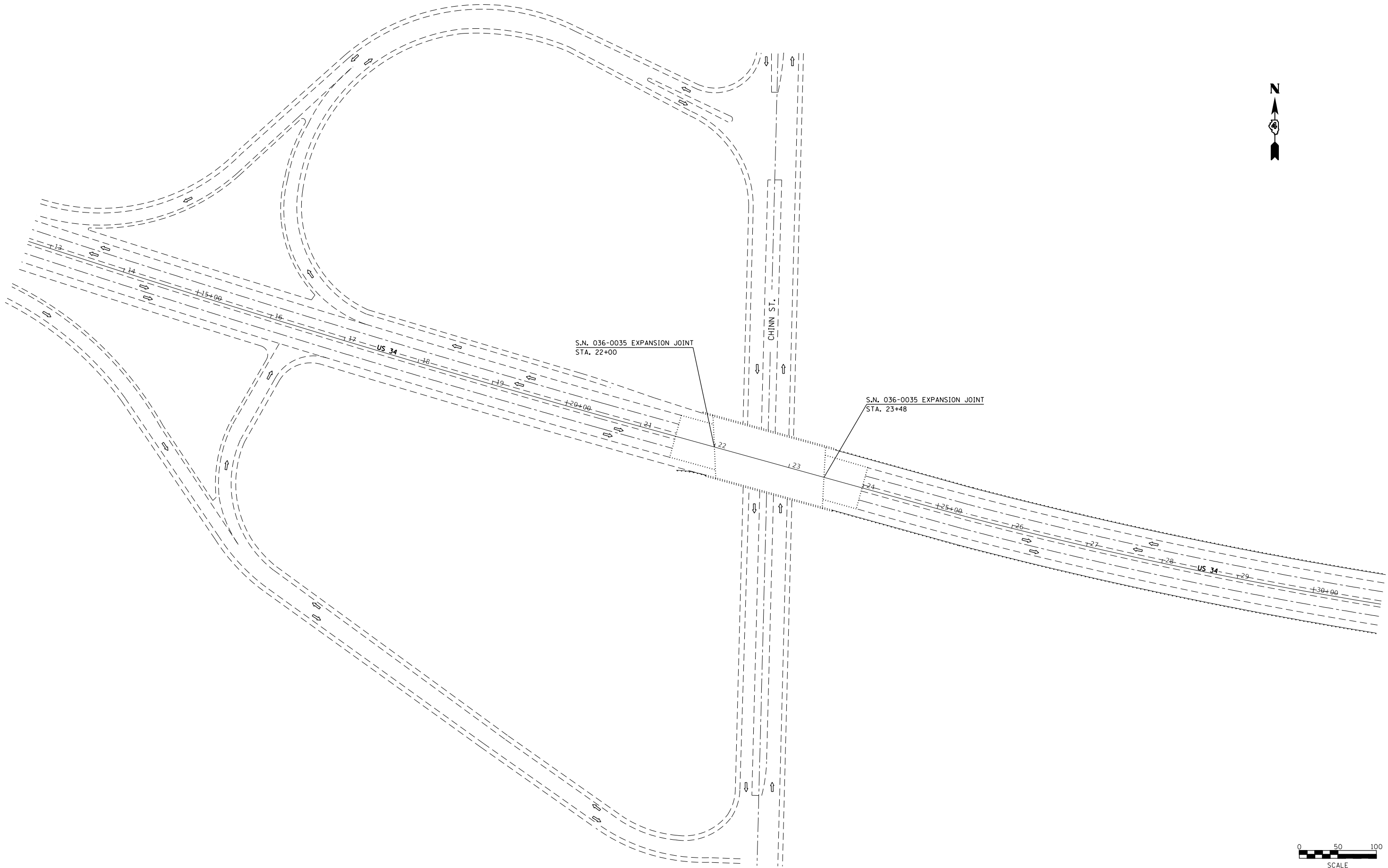
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES  
STRUCTURE NO. 036-0035

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	[(8R-HB)BR]BJR	HENDERSON	22	8
CONTRACT NO. 68E25				
ILLINOIS FED. AID PROJECT				





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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**


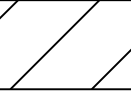



**GENERAL LOCATION OVERVIEW**  
**STRUCTURE NO. 036-0035**

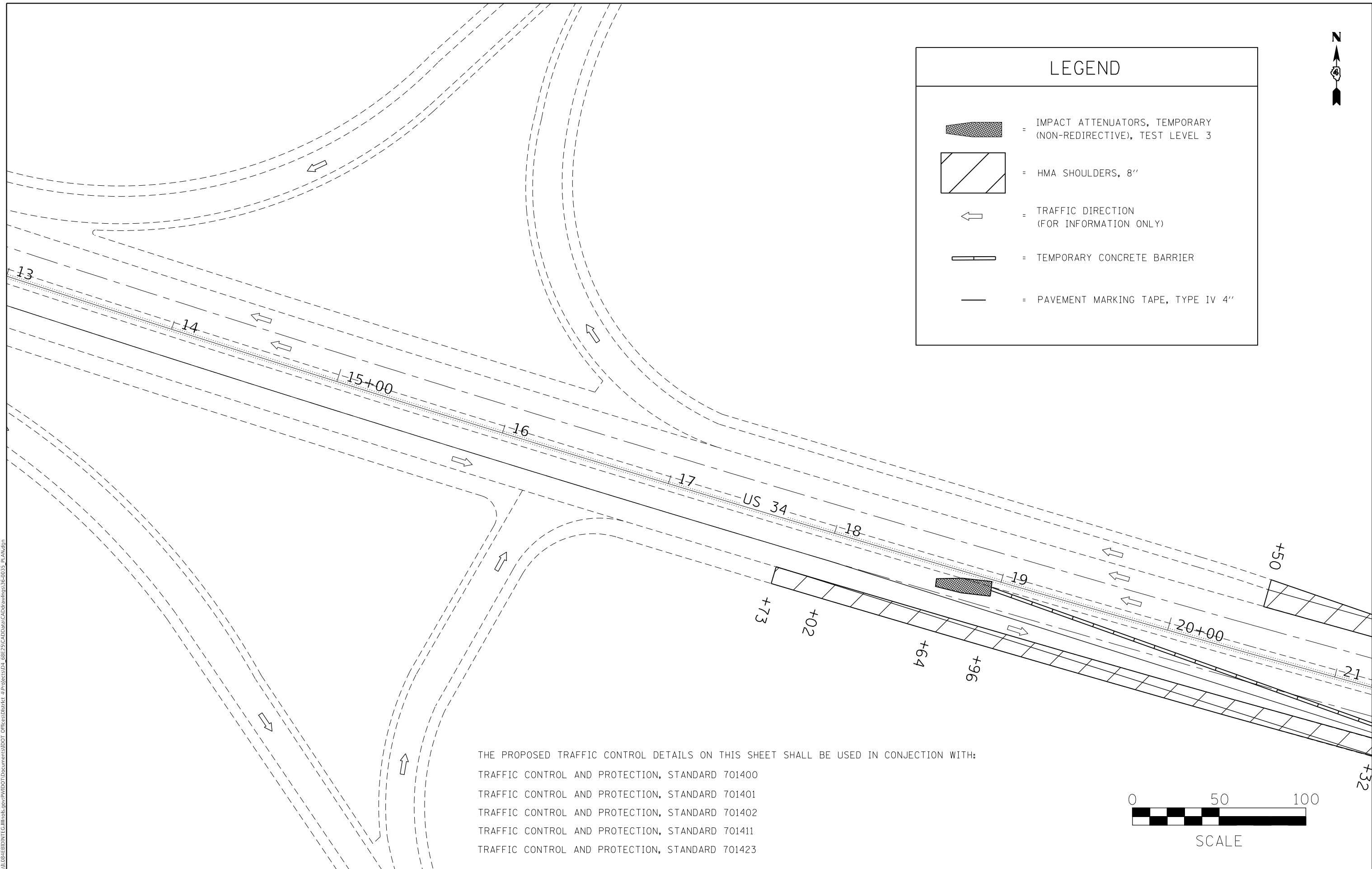
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 68E25				
		ILLINOIS	FED. AID PROJECT	

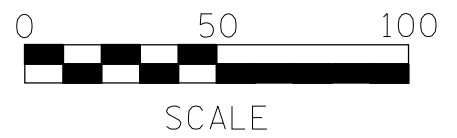


### LEGEND

-  = IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3
-  = HMA SHOULDERS, 8"
-  = TRAFFIC DIRECTION (FOR INFORMATION ONLY)
-  = TEMPORARY CONCRETE BARRIER
-  = PAVEMENT MARKING TAPE, TYPE IV 4"



THE PROPOSED TRAFFIC CONTROL DETAILS ON THIS SHEET SHALL BE USED IN CONJECTION WITH:  
 TRAFFIC CONTROL AND PROTECTION, STANDARD 701400  
 TRAFFIC CONTROL AND PROTECTION, STANDARD 701401  
 TRAFFIC CONTROL AND PROTECTION, STANDARD 701402  
 TRAFFIC CONTROL AND PROTECTION, STANDARD 701411  
 TRAFFIC CONTROL AND PROTECTION, STANDARD 701423



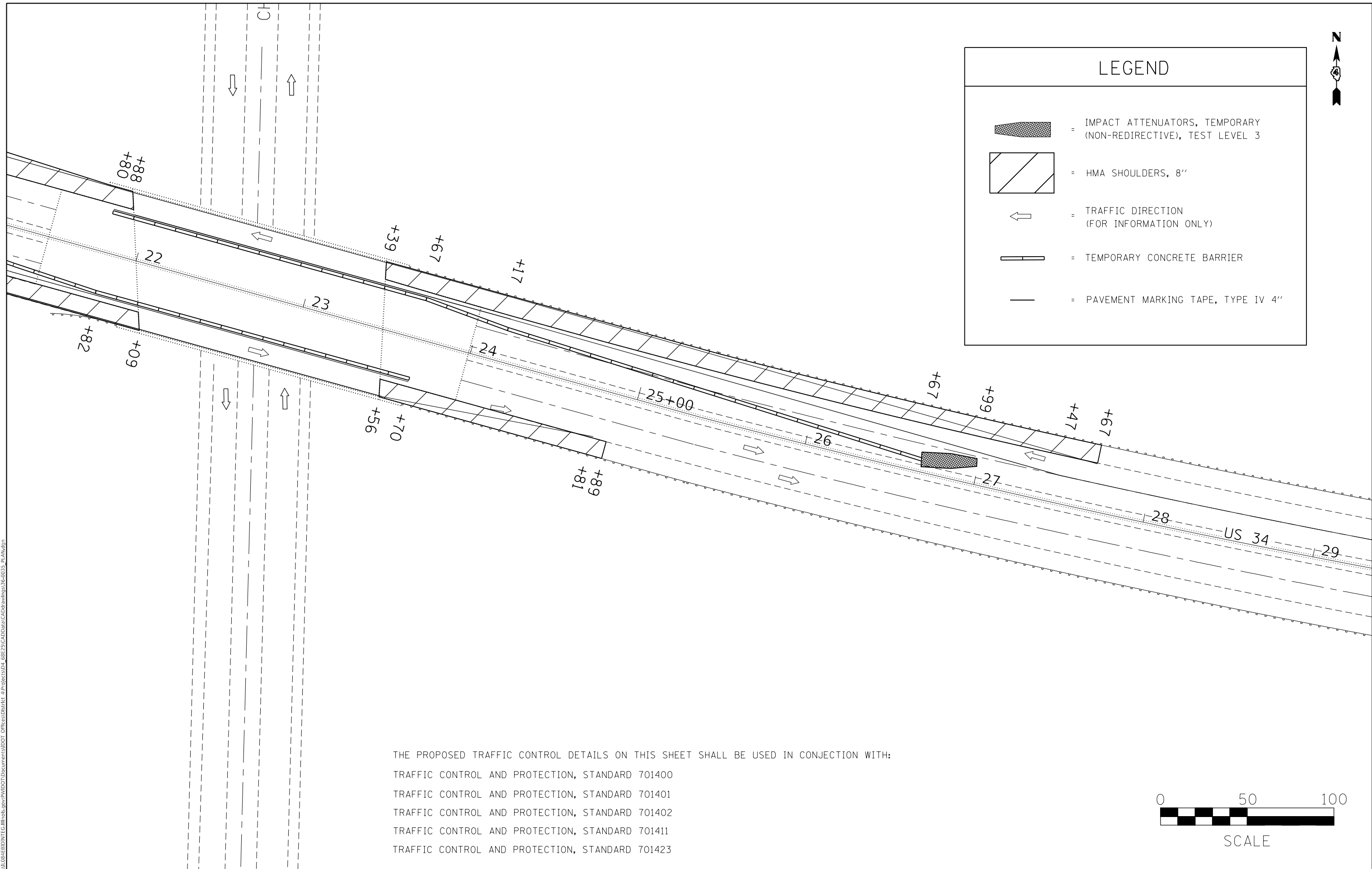
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	USER NAME = jochumsjg	DESIGNED - CEJ	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PROPOSED TRAFFIC CONTROL STAGE 1</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - 05-15-2018	REVISED -					313	[(8R-HB)BR]BJR	HENDERSON	22	10
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -					CONTRACT NO. 68E25				
	PLOT DATE = 8/16/2018	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA. 13+00	TO STA. 21+22.5	ILLINOIS FED. AID PROJECT

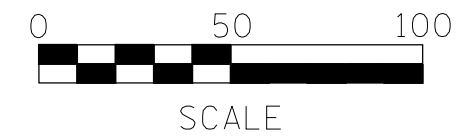


### LEGEND

- = IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3
- = HMA SHOULDERS, 8"
- = TRAFFIC DIRECTION (FOR INFORMATION ONLY)
- = TEMPORARY CONCRETE BARRIER
- = PAVEMENT MARKING TAPE, TYPE IV 4"



THE PROPOSED TRAFFIC CONTROL DETAILS ON THIS SHEET SHALL BE USED IN CONJECTION WITH:  
 TRAFFIC CONTROL AND PROTECTION, STANDARD 701400  
 TRAFFIC CONTROL AND PROTECTION, STANDARD 701401  
 TRAFFIC CONTROL AND PROTECTION, STANDARD 701402  
 TRAFFIC CONTROL AND PROTECTION, STANDARD 701411  
 TRAFFIC CONTROL AND PROTECTION, STANDARD 701423



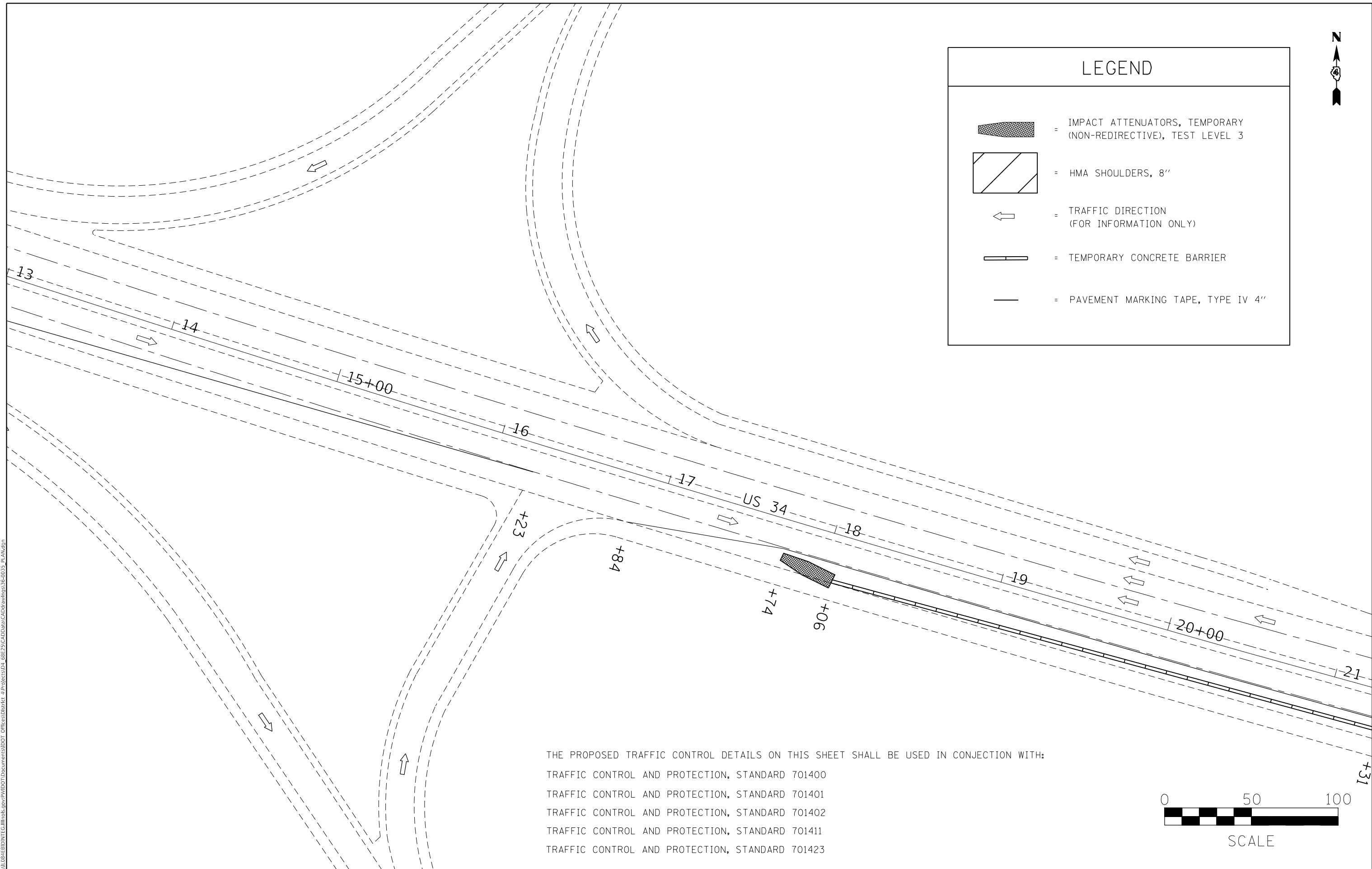
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	USER NAME = jochumsjg	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PROPOSED TRAFFIC CONTROL STAGE I</b>			F.A.P. RTE. 313	SECTION [(8R-HB)BR]BJR	COUNTY HENDERSON	TOTAL SHEETS 22	SHEET NO. 11
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -					CONTRACT NO. 68E25				
	PLOT DATE = 8/16/2018	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				
					SCALE:	SHEET	OF	SHEETS	STA. 21+22.5	TO	STA. 29+34.8	

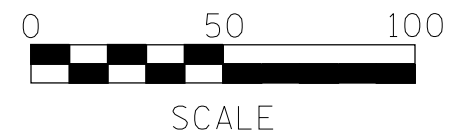


### LEGEND

- = IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3
- = HMA SHOULDERS, 8"
- = TRAFFIC DIRECTION (FOR INFORMATION ONLY)
- = TEMPORARY CONCRETE BARRIER
- = PAVEMENT MARKING TAPE, TYPE IV 4"



THE PROPOSED TRAFFIC CONTROL DETAILS ON THIS SHEET SHALL BE USED IN CONJECTION WITH:  
 TRAFFIC CONTROL AND PROTECTION, STANDARD 701400  
 TRAFFIC CONTROL AND PROTECTION, STANDARD 701401  
 TRAFFIC CONTROL AND PROTECTION, STANDARD 701402  
 TRAFFIC CONTROL AND PROTECTION, STANDARD 701411  
 TRAFFIC CONTROL AND PROTECTION, STANDARD 701423



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USER NAME = jochumsjg	DESIGNED - CEJ	REVISED -
DRAWN - 05-15-2018	REVISIED -	
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 8/16/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

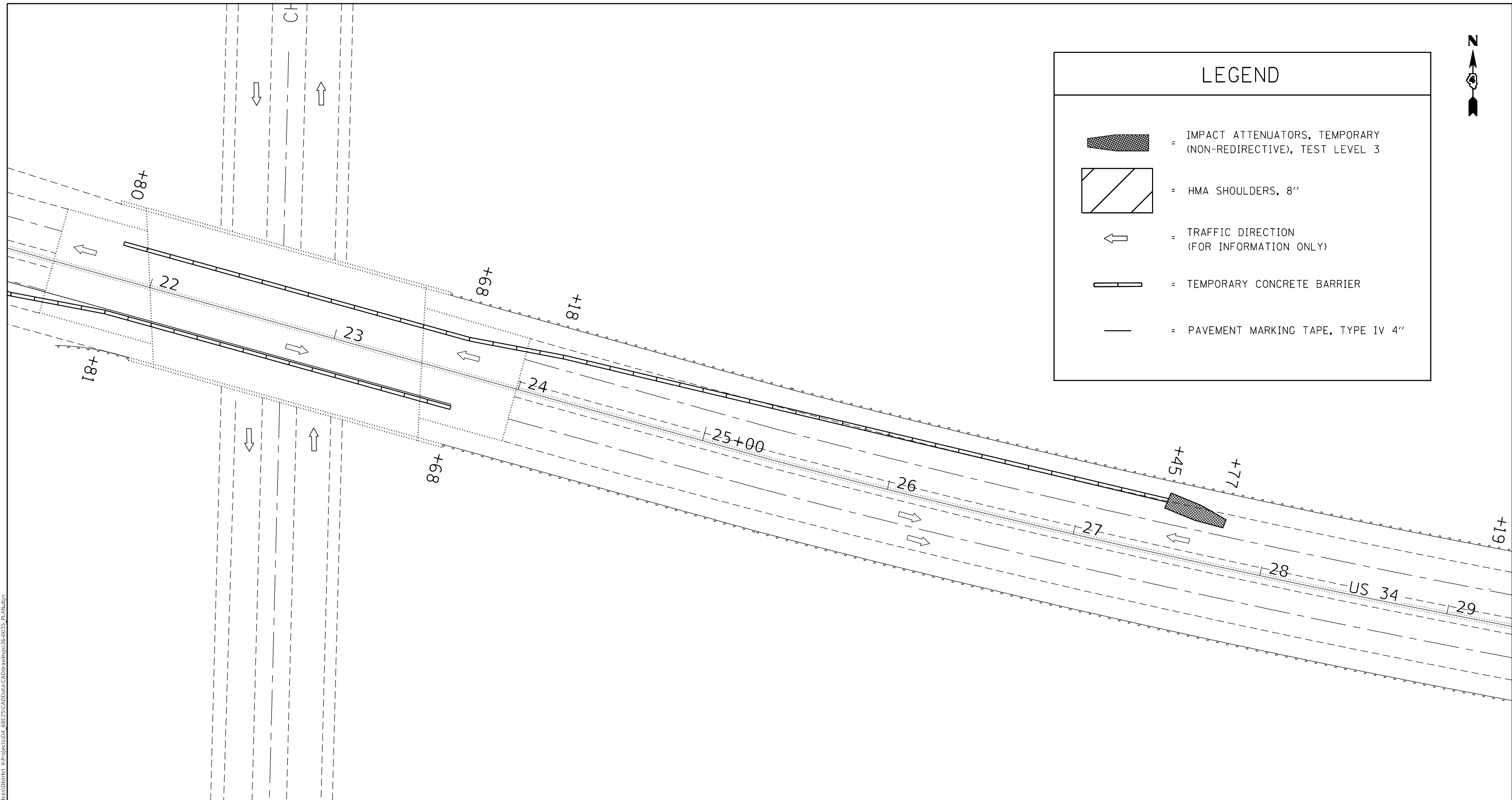
<b>PROPOSED TRAFFIC CONTROL STAGE II</b>			
SCALE:	SHEET	OF	SHEETS
	STA. 13+00		TO STA. 21+22.5

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	[(8R-HB)BR]BJR	HENDERSON	22	12
CONTRACT NO. 68E25				
		ILLINOIS	FED. AID PROJECT	



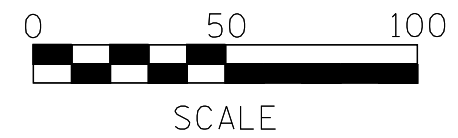
### LEGEND

- = IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3
- = HMA SHOULDERS, 8"
- = TRAFFIC DIRECTION (FOR INFORMATION ONLY)
- = TEMPORARY CONCRETE BARRIER
- = PAVEMENT MARKING TAPE, TYPE IV 4"



THE PROPOSED TRAFFIC CONTROL DETAILS ON THIS SHEET SHALL BE USED IN CONJECTION WITH:

- TRAFFIC CONTROL AND PROTECTION, STANDARD 701400
- TRAFFIC CONTROL AND PROTECTION, STANDARD 701401
- TRAFFIC CONTROL AND PROTECTION, STANDARD 701402
- TRAFFIC CONTROL AND PROTECTION, STANDARD 701411
- TRAFFIC CONTROL AND PROTECTION, STANDARD 701423



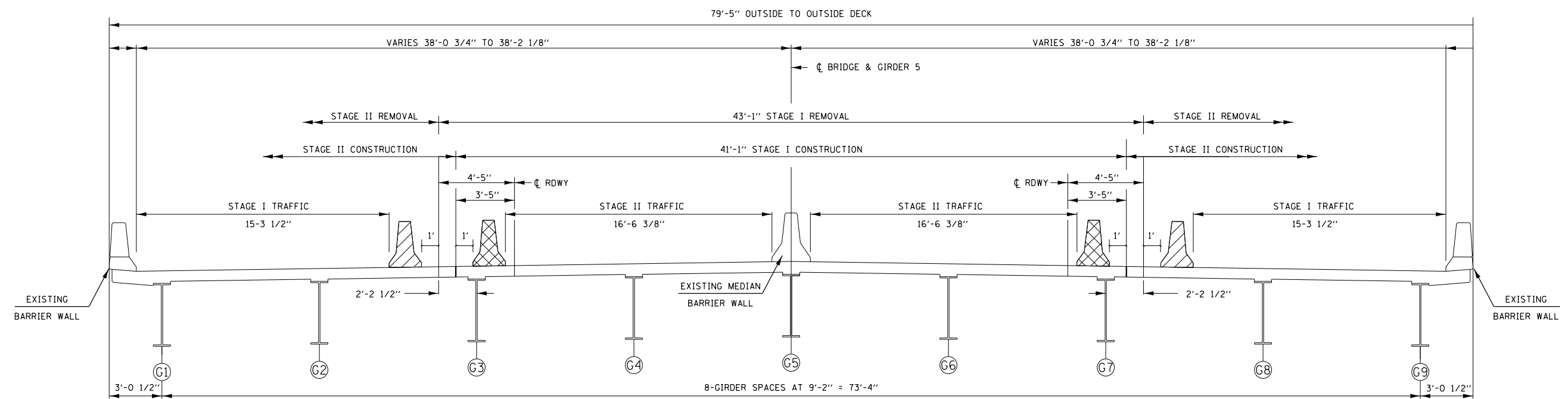
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USER NAME = jochumsjg	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -	
PLOT DATE = 8/16/2018	DATE -	REVISED -	

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

PROPOSED TRAFFIC CONTROL STAGE II			
SCALE:	SHEET	OF	SHEETS
	STA. 21+22.5		TO STA. 29+34.8

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	[(8R-HB)BR]BJR	HENDERSON	22	13
CONTRACT NO. 68E25				
ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL TYPICAL SECTION

LEGEND	
	= STAGE I BARRIER
	= STAGE II BARRIER

THE PROPOSED TRAFFIC CONTROL DETAILS ON THIS SHEET SHALL BE USED IN CONJECTION WITH:

- TRAFFIC CONTROL AND PROTECTION, STANDARD 701400
- TRAFFIC CONTROL AND PROTECTION, STANDARD 701401
- TRAFFIC CONTROL AND PROTECTION, STANDARD 701402
- TRAFFIC CONTROL AND PROTECTION, STANDARD 701411
- TRAFFIC CONTROL AND PROTECTION, STANDARD 701423

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USER NAME = jochumsjg	DESIGNED - CEJ	REVISED -
	DRAWN - 05-15-2018	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 8/16/2018	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PROPOSED TRAFFIC CONTROL  
TYPICAL SECTION

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	[(8R-HB)BR]BJR	HENDERSON	22	14
CONTRACT NO. 68E25				
ILLINOIS FED. AID PROJECT				

**GENERAL NOTES**

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

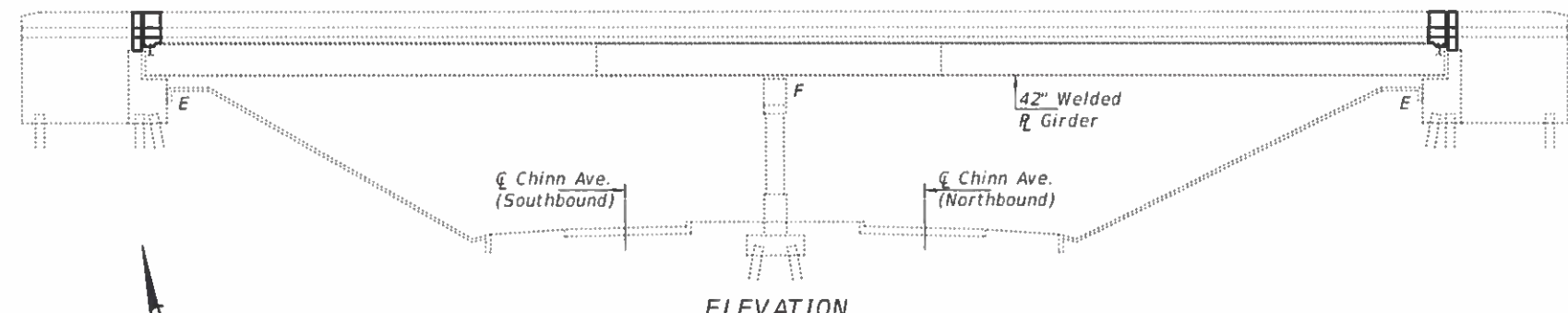
Reinforcement bars designated (E) shall be epoxy coated.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

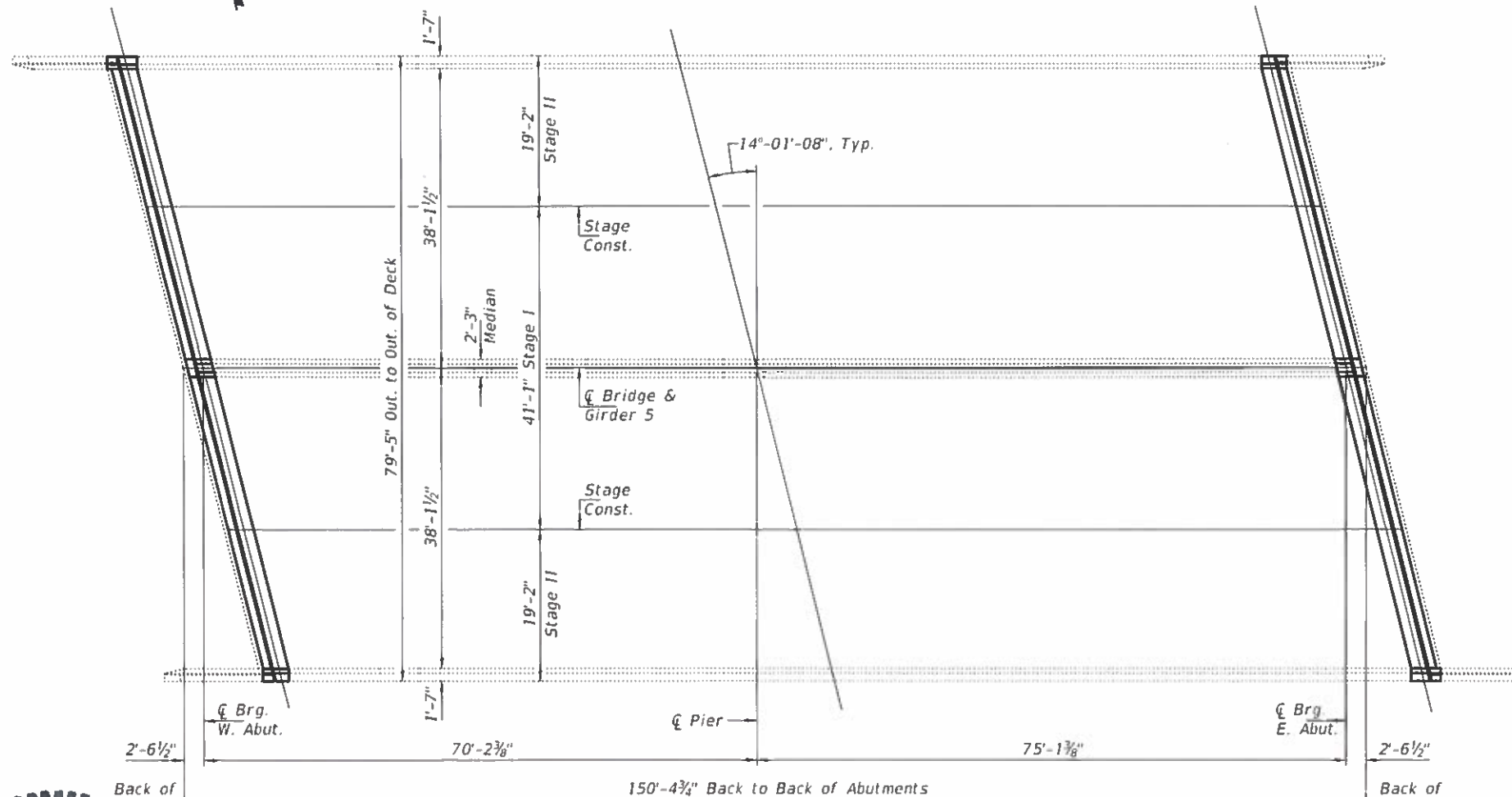
Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.

The deck surface shall have its final finish tined according to Article 420.09(e)(1) of the Standard Specifications. Cost included with Concrete Superstructure.



**ELEVATION**



**PLAN**



**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	24.9
Concrete Superstructure	Cu. Yd.	24.9
Reinforcement Bars, Epoxy Coated	Pound	2640
Bar Splicers	Each	48
Prefomed Joint Strip Seal	Foot	164
Protective Coat	Sq. Yd.	60.1

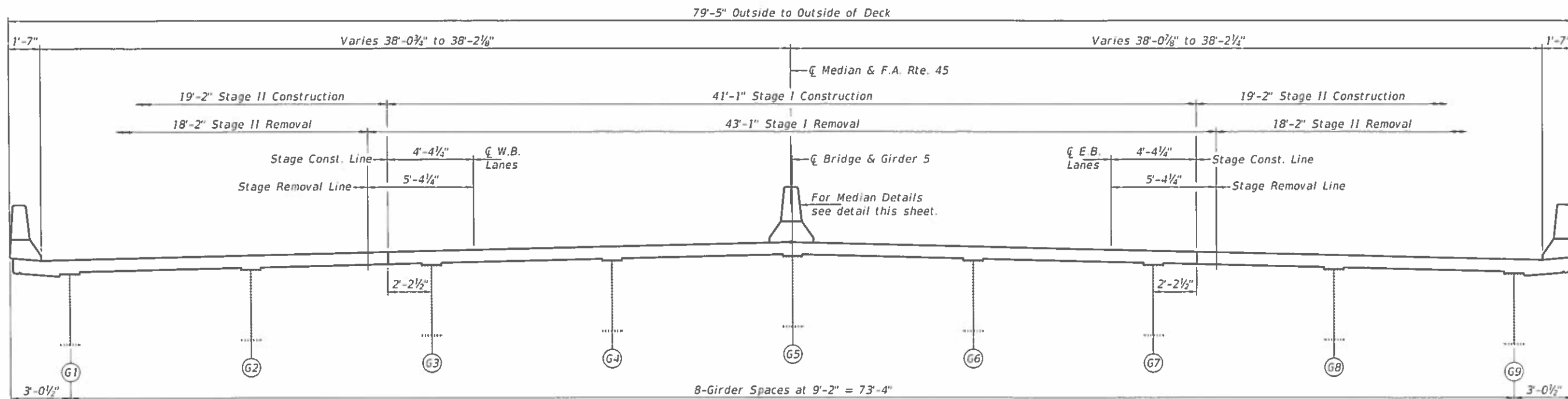
\* On new deck surface and top & inside faces of new parapet & median concrete only.

DESIGNED <i>David D. Koltweit</i>	EXAMINED <i>Timothy A. Dadd</i>	DATE - SEPTEMBER 18, 2018
CHECKED <i>Stephan M. Ryan</i>	PASSED <i>[Signature]</i>	REVISIONS
DRAWN <i>Steffen</i>		REVISIONS
CHECKED <i>CBK</i>		REVISIONS

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

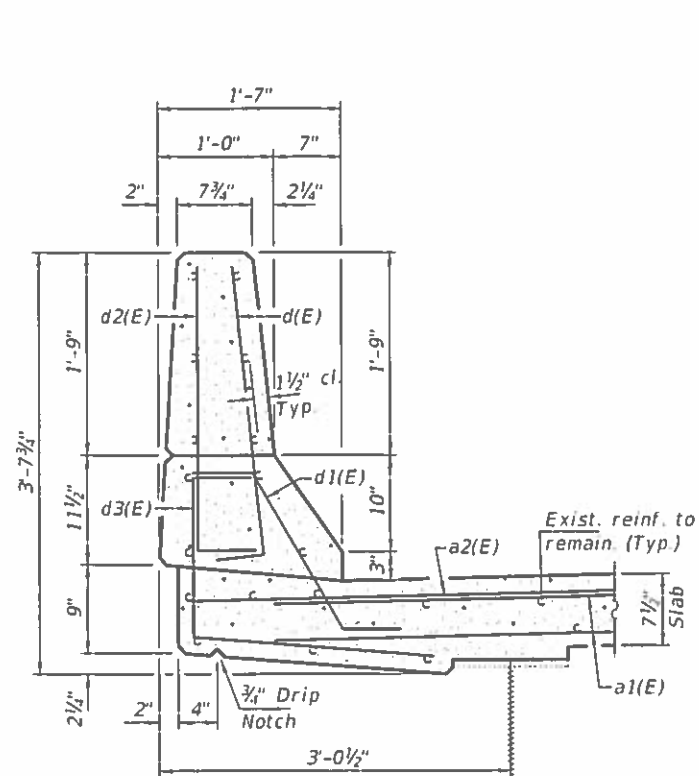
GENERAL PLAN AND ELEVATION  
FAP 313 (US 34) OVER CHINN AVE.  
SN 036-0035

F.A.P. RTE. 313	SECTION [(8R-HB)BR]BJR	COUNTY HENDERSON	TOTAL SHEETS 22	SHEET NO. 15
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68E25	

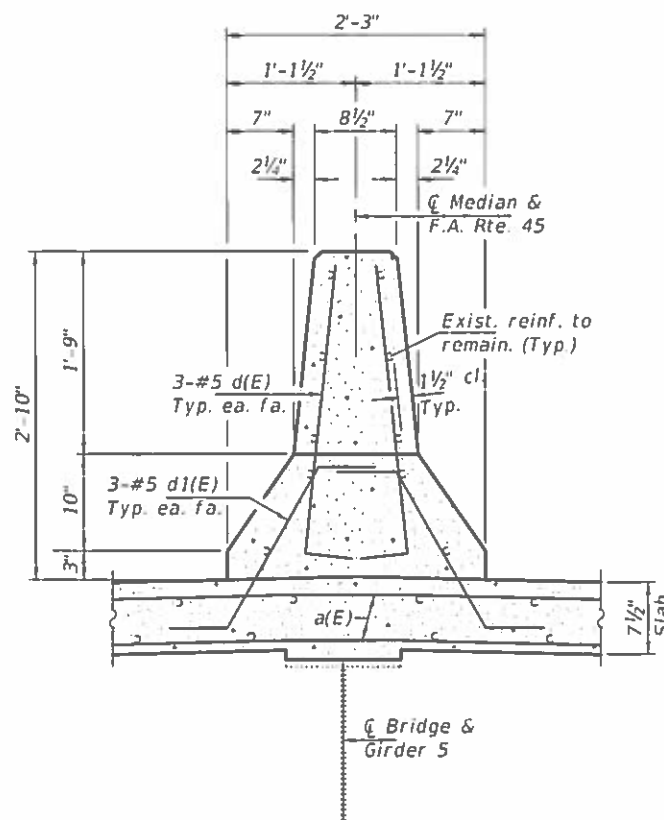


**CROSS-SECTION**

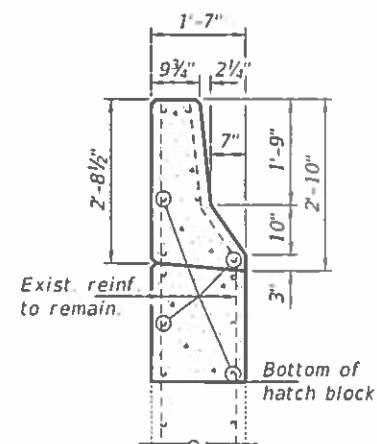
(Looking East)  
 Cross Section is symmetrical about  $\bar{C}$  Bridge  
 except for location of median and profile grade.



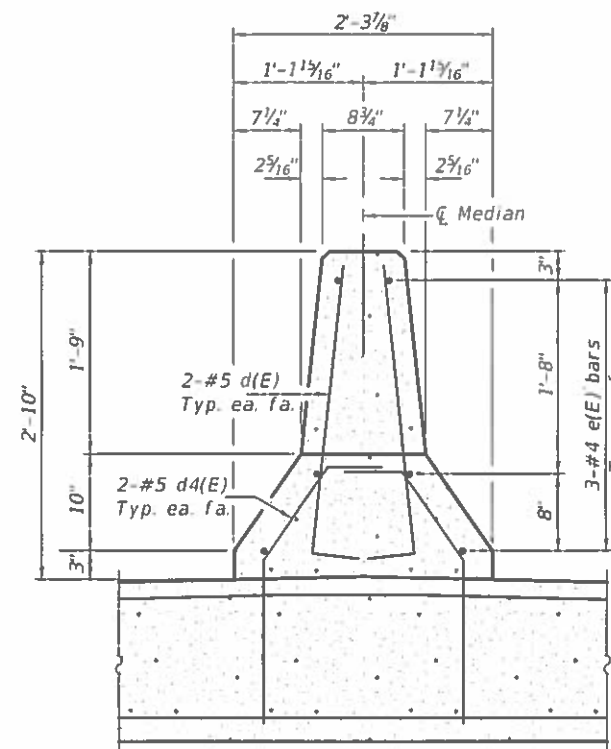
**SECTION THRU BRIDGE PARAPET**



**SECTION THRU BRIDGE MEDIAN**



**SECTION THRU ABUTMENT WINGWALL**



**SECTION THRU ABUTMENT MEDIAN**  
 (Dimensions along skew)

DESIGNED - CDK  
 CHECKED - SMR  
 DRAWN - steffen  
 CHECKED - CDK SMR

EXAMINED  
 PASSED  
 ENGINEER OF STRUCTURAL SERVICES  
 ENGINEER OF BRIDGES AND STRUCTURES

DATE - SEPTEMBER 18, 2018  
 REVISED -  
 REVISED -

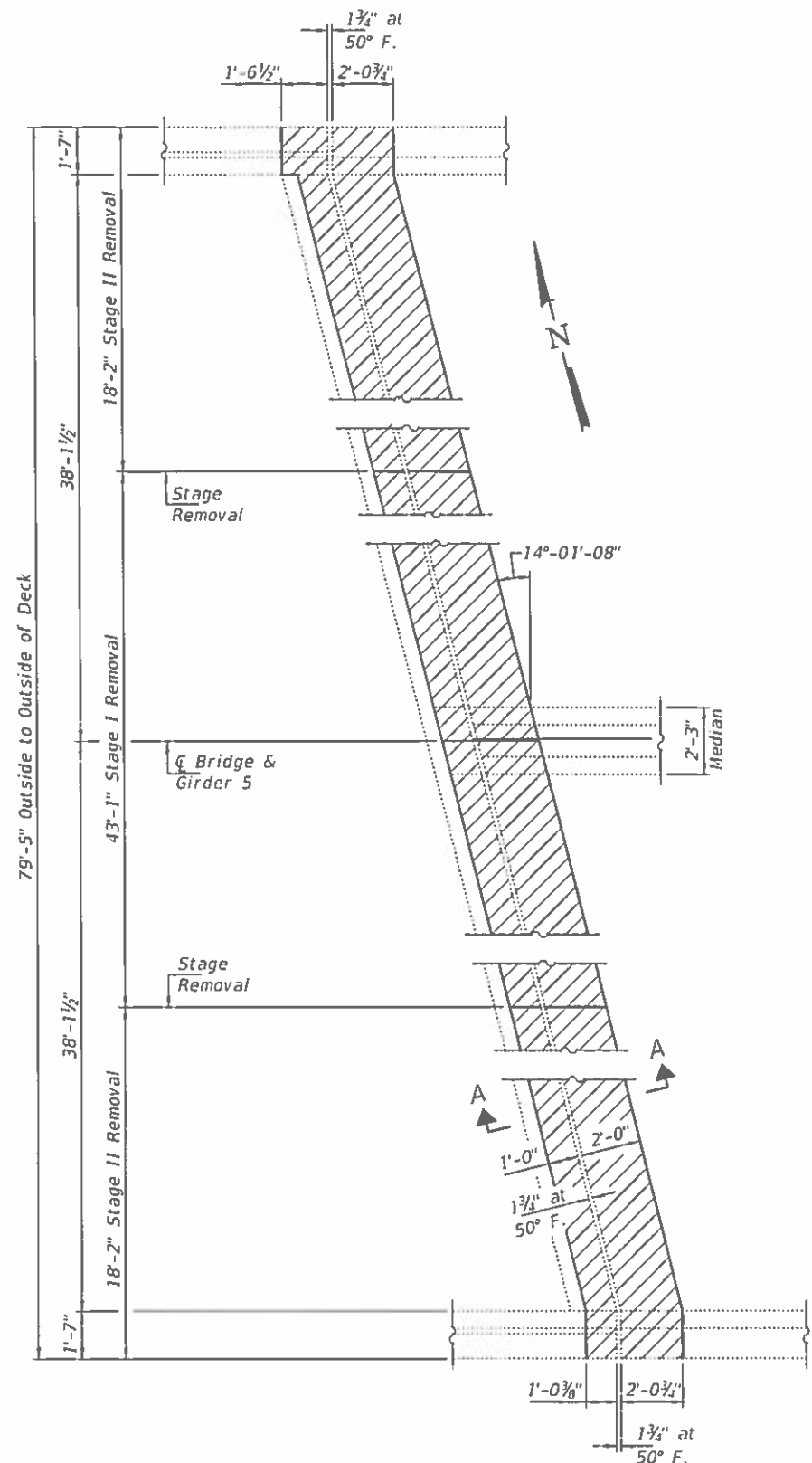
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

STAGING & REPAIR DETAILS  
 SN 036-0035

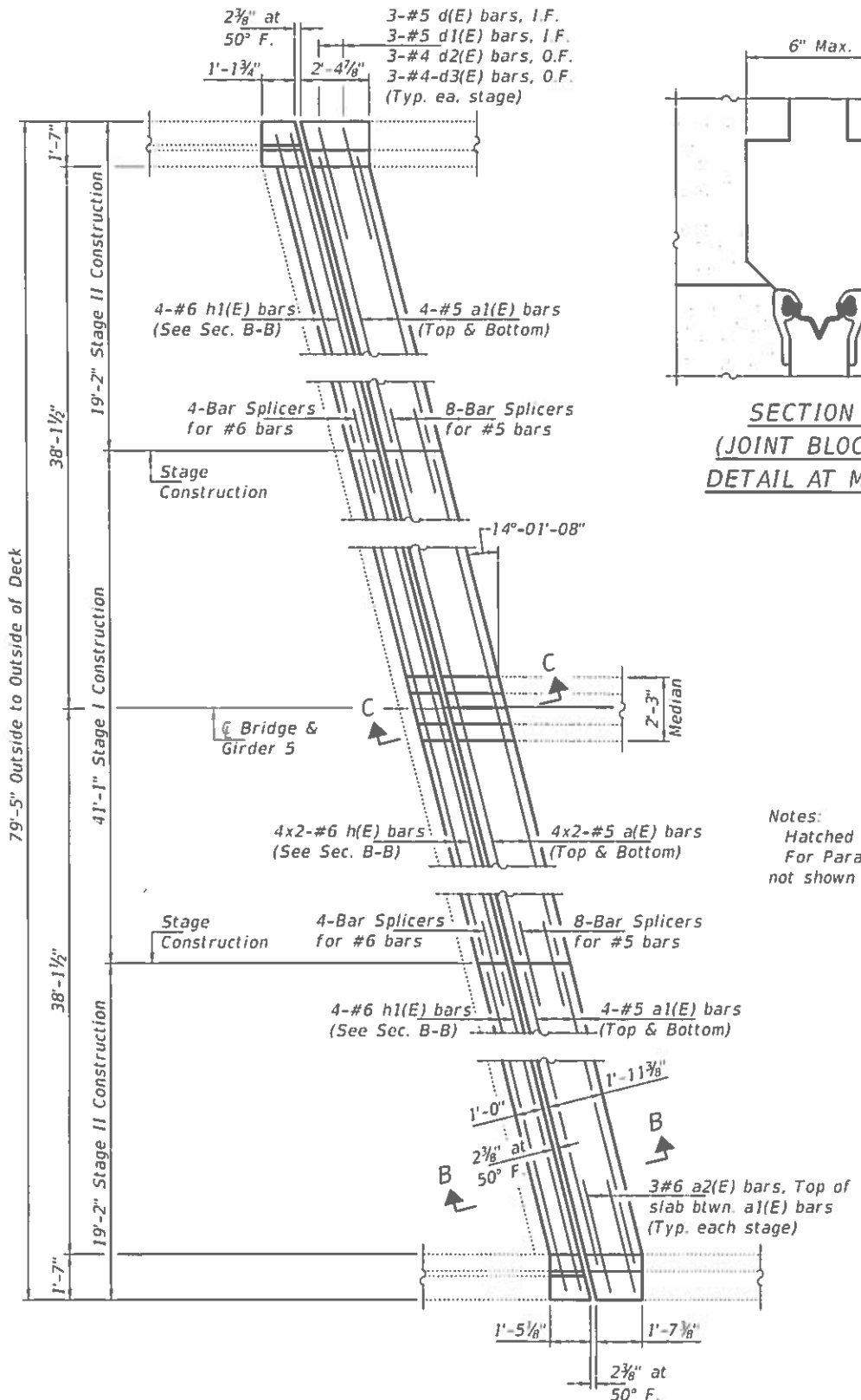
SHEET NO. 2 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	[[BR-HB]BR]BJR	HENDERSON	22	16
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68E25	

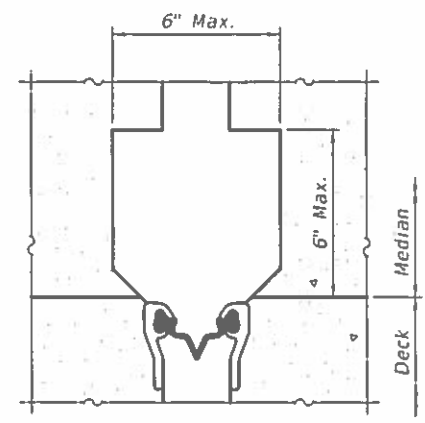




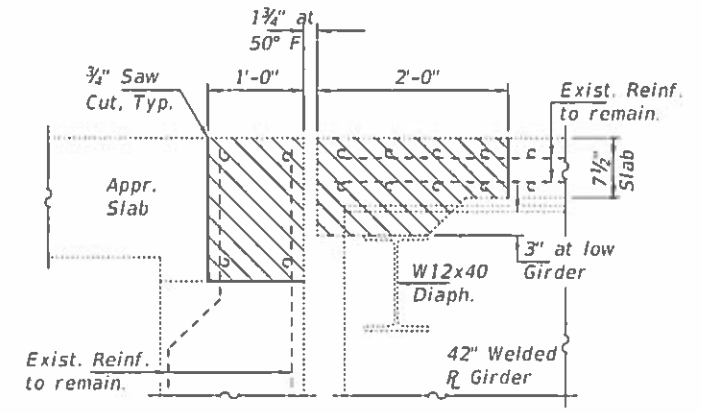
**CONCRETE REMOVAL PLAN**  
W. Abut. shown; E. Abut. similar.



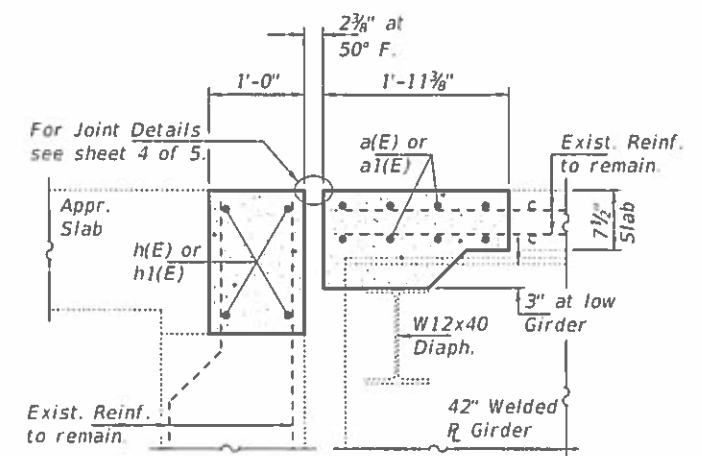
**CONCRETE REPLACEMENT PLAN**  
W. Abut. shown; E. Abut. similar.



**SECTION C-C**  
(JOINT BLOCK OUT  
DETAIL AT MEDIAN)

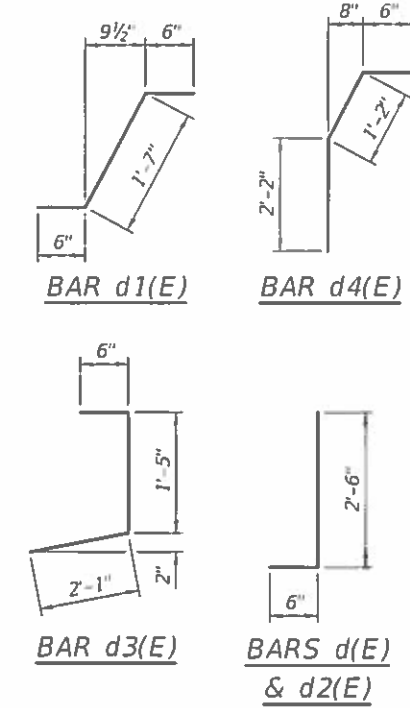


**SECTION A-A**  
(Dims. at Rt. L's)



**SECTION B-B**  
(Dims. at Rt. L's)

Notes:  
Hatched areas indicate Concrete Removal.  
For Parapet & Median Details and reinf.  
not shown see sheet 2 of 5.



**BILL OF MATERIAL**  
(2 ABUTMENTS)

Bar	No	Size	Length	Shape
a(E)	32	#5	22'-7"	—
a1(E)	32	#5	17'-7"	—
a2(E)	12	#6	4'-0"	—
d(E)	32	#5	3'-0"	J
d1(E)	24	#5	2'-7"	J
d2(E)	12	#4	3'-0"	J
d3(E)	12	#4	4'-0"	J
d4(E)	8	#4	3'-10"	J
e(E)	12	#4	10'	—
h(E)	16	#6	23'-4"	—
h1(E)	16	#6	17'-7"	—
Concrete Removal			Cu. Yd.	24.9
Concrete Superstructure			Cu. Yd.	24.9
Bar Splicers			Each	48
Reinforcement Bars, Epoxy Coated			Pound	2640

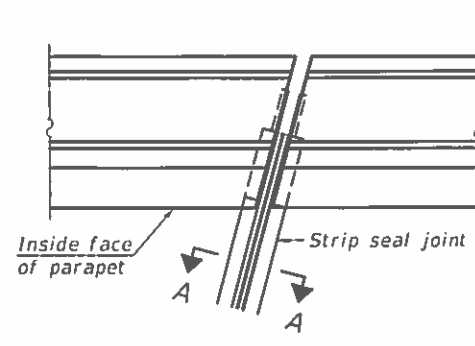
Bars indicated thus 4 x 2-#5 etc. indicates 4 lines of bars with 2 lengths per line.

DESIGNED - CDK	EXAMINED - <i>Imot A. [Signature]</i>	DATE - SEPTEMBER 18, 2018
CHECKED - SMR	PASSED - <i>Carl [Signature]</i>	REVISED -
DRAWN - <i>stieffen</i>	ENGINEER OF BRIDGES AND STRUCTURES	REVISED -
CHECKED - CDK SMR	ENGINEER OF STRUCTURAL SERVICES	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

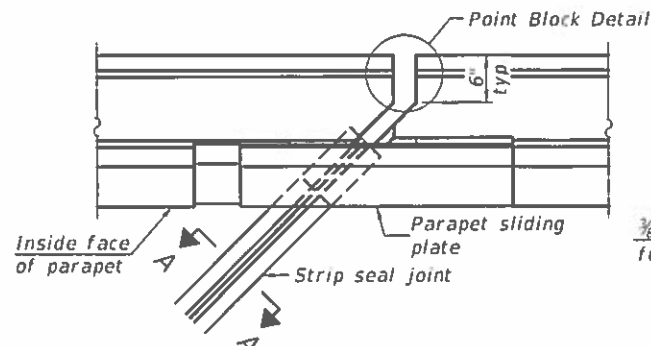
JOINT REMOVAL & REPLACEMENT DETAILS  
SN 036-0035

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	[[8R-HB8R]JR	HENDERSON	22	17
CONTRACT NO. 68E25			ILLINOIS   FED. AID PROJ. CT	

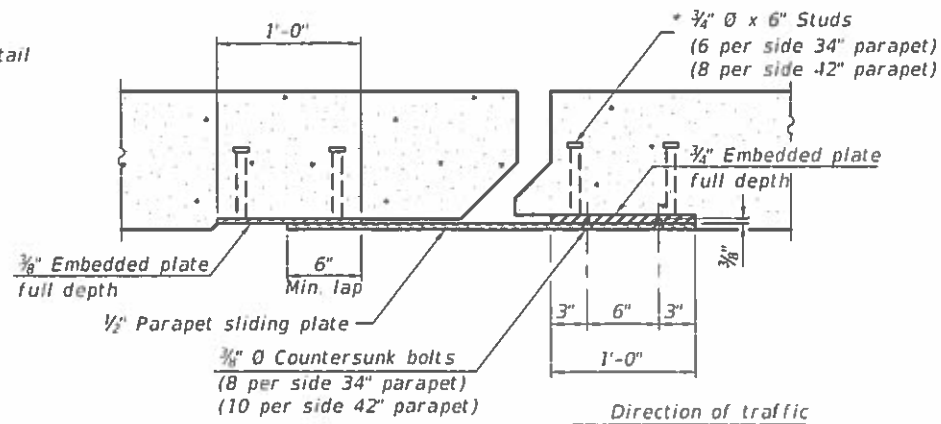


FOR SKEWS  $\leq 30^\circ$

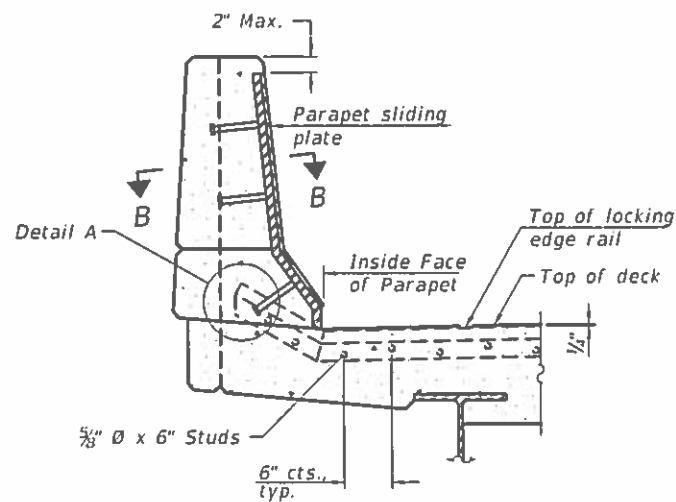
PLAN AT PARAPET



FOR SKEWS  $> 30^\circ$

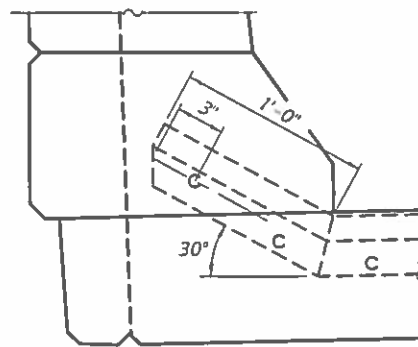


SECTION B-B

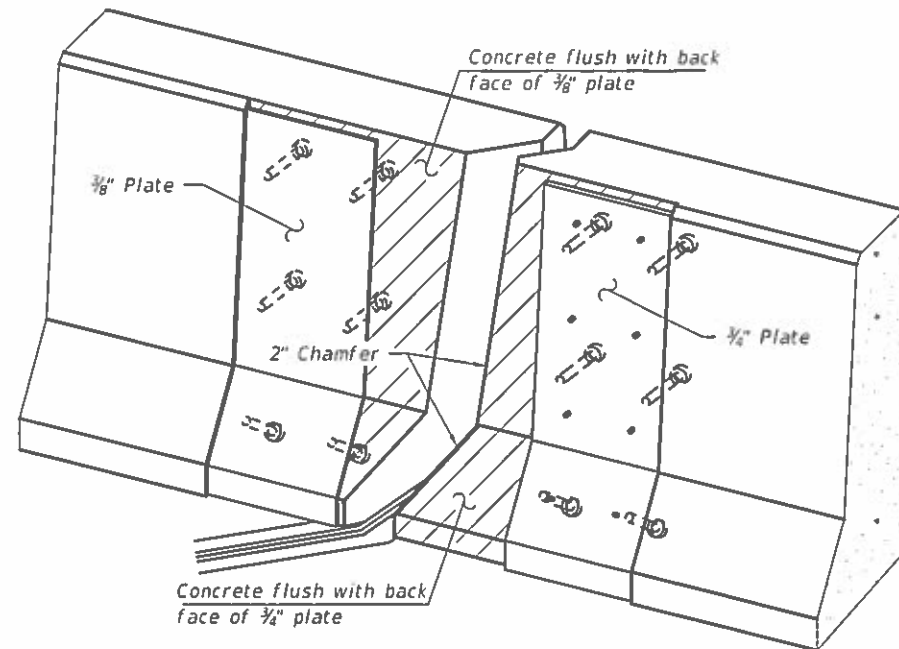


ELEVATION AT PARAPET

(Skews  $> 30^\circ$  shown. Skews  $\leq 30^\circ$  similar except as shown in plan view.)



DETAIL A



TRIMETRIC VIEW  
(Showing embedded plates only)

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of  $1/4$ ". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal will vary based on manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the  $4 1/2$ " maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

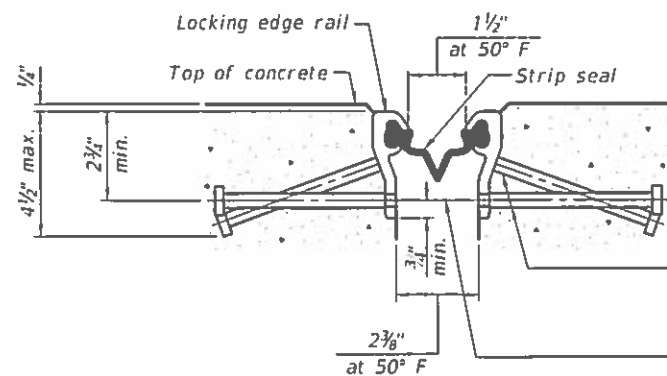
The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be  $3/16$ " and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.

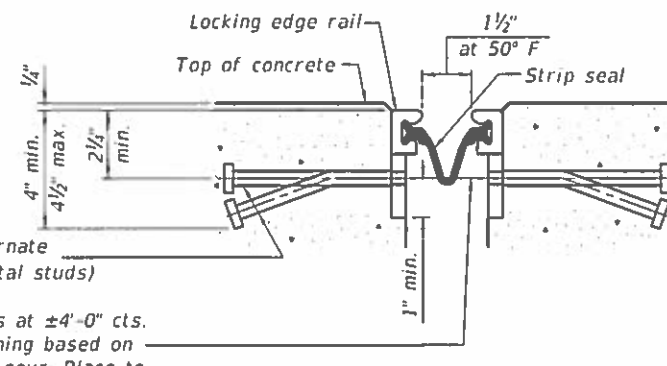
34" F-shape barrier shown, 42" F-shape similar as noted. The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.



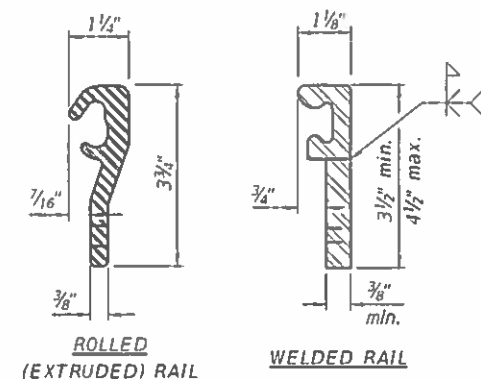
SHOWING ROLLED RAIL JOINT

\*  $5/8$ "  $\phi$  x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

$3/8$ "  $\phi$  threaded rods in  $1/16$ "  $\phi$  holes at  $\pm 4$ "-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

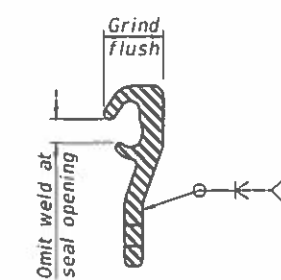


SHOWING WELDED RAIL JOINT



LOCKING EDGE RAILS

\*\* Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	164

EJ-55

8-11-17

DESIGNED - CDK	EXAMINED
CHECKED - SMR	PASSED
DRAWN - steffen	
CHECKED - CDK SMR	

Timothy A. [Signature]  
ENGINEER OF STRUCTURAL SERVICES

Carl [Signature]  
ENGINEER OF BRIDGES AND STRUCTURES

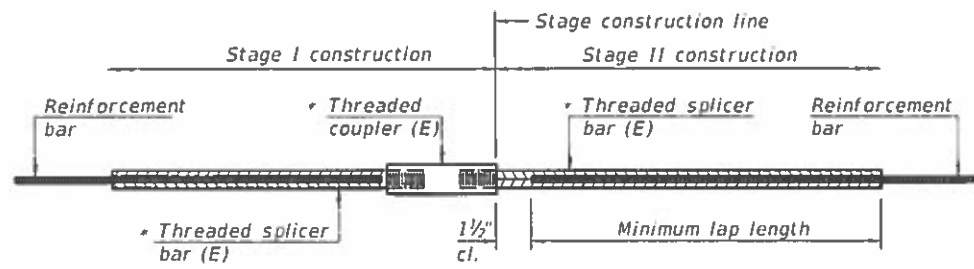
DATE - SEPTEMBER 18, 2018
REVISED -
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL DETAILS  
SN 036-0035

SHEET NO. 4 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	[(8R-HB)BR]BR	HENDERSON	22	18
CONTRACT NO. 6BE25				
ILLINOIS FED. AID PROJECT				

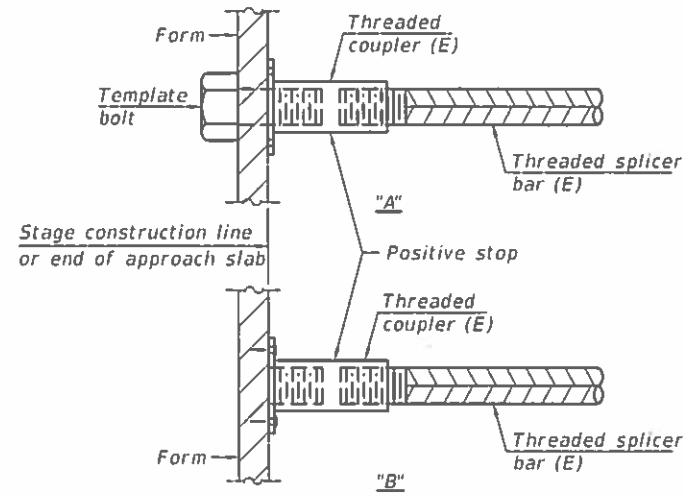


**STANDARD BAR SPLICER ASSEMBLY**

Threaded splicer bar length = min. lap length + 1/2" + thread length

\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Deck	#5	32	3'-6"
Abutment	#6	16	4'-0"

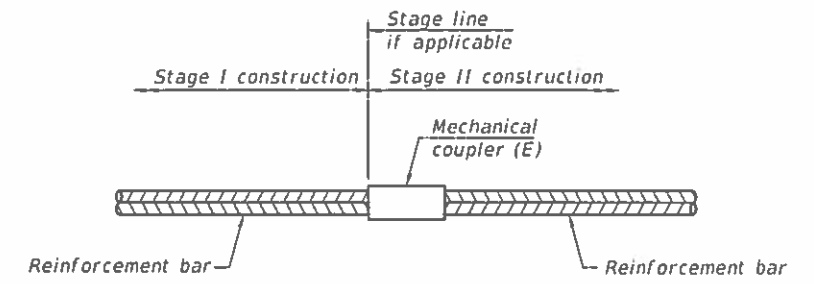


**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.

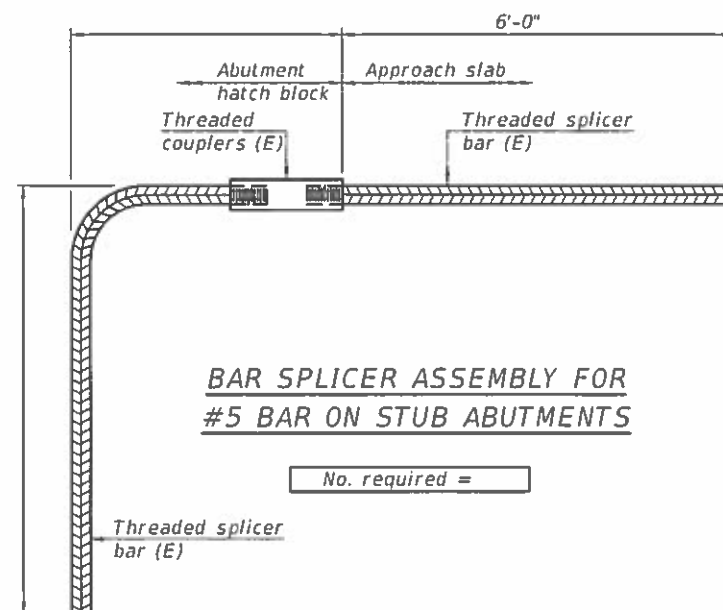
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

No. required =

**NOTES**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

2-17-2017

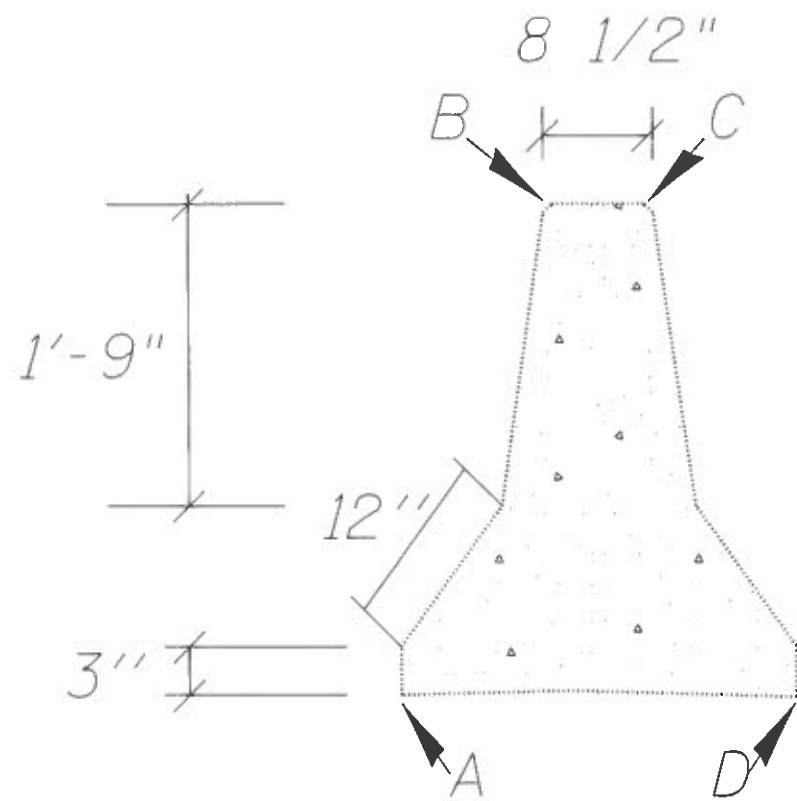
DESIGNED - CDK	EXAMINED	DATE - SEPTEMBER 18, 2018
CHECKED - SMR	<i>Timothy A. Anagnost</i> ENGINEER OF STRUCTURAL SERVICES	REVISED -
DRAWN - steffen	PASSED	REVISED -
CHECKED - CDK SMR	<i>Carl Thompson</i> ENGINEER OF BRIDGES AND STRUCTURES	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

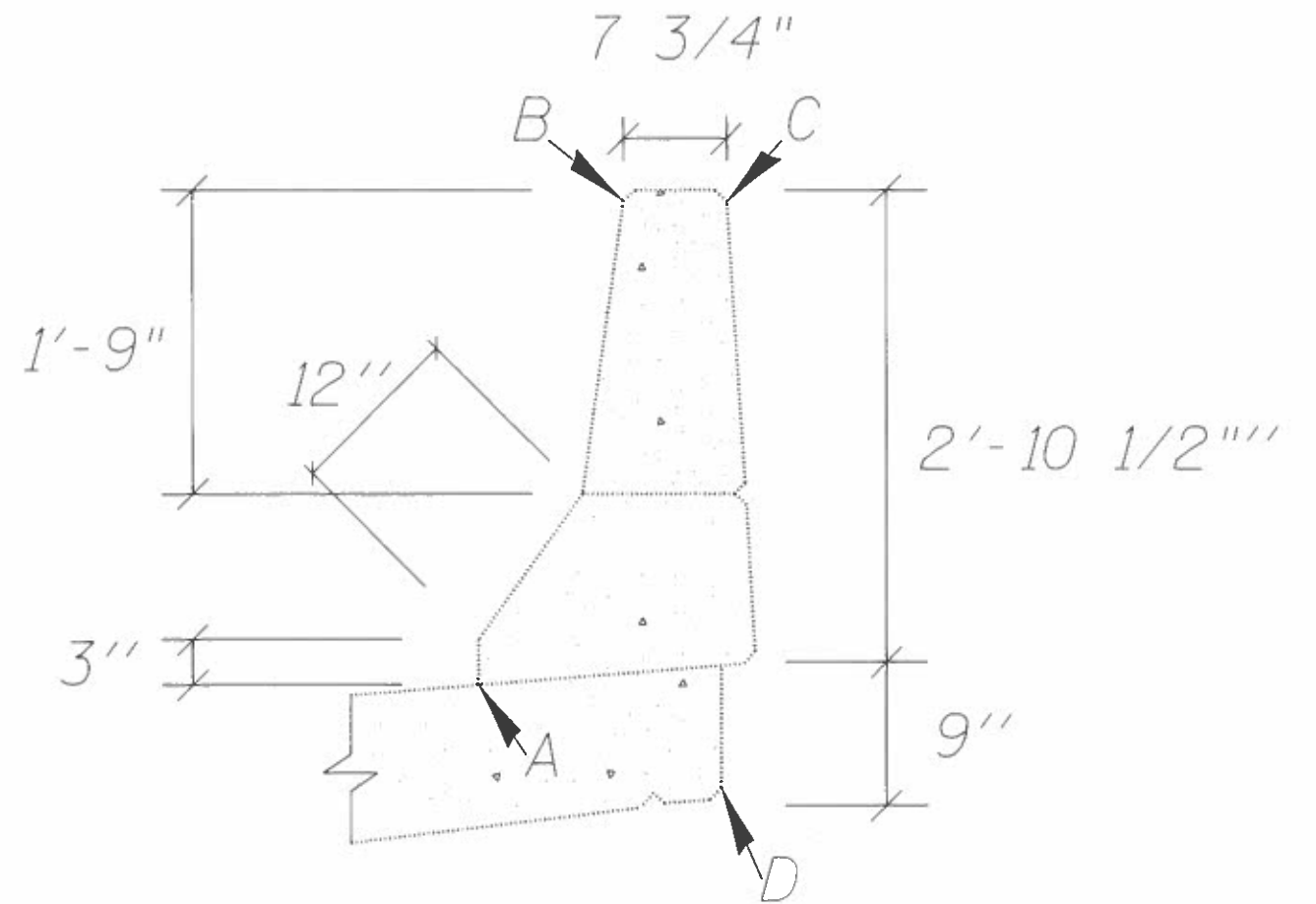
BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
SN 036-0035

SHEET NO. 5 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	[(8R-HB)BR]BJR	HENDERSON	22	19
			CONTRACT NO. 68E25	
ILLINOIS FED. AID PROJECT				



**MEDIAN BARRIER**



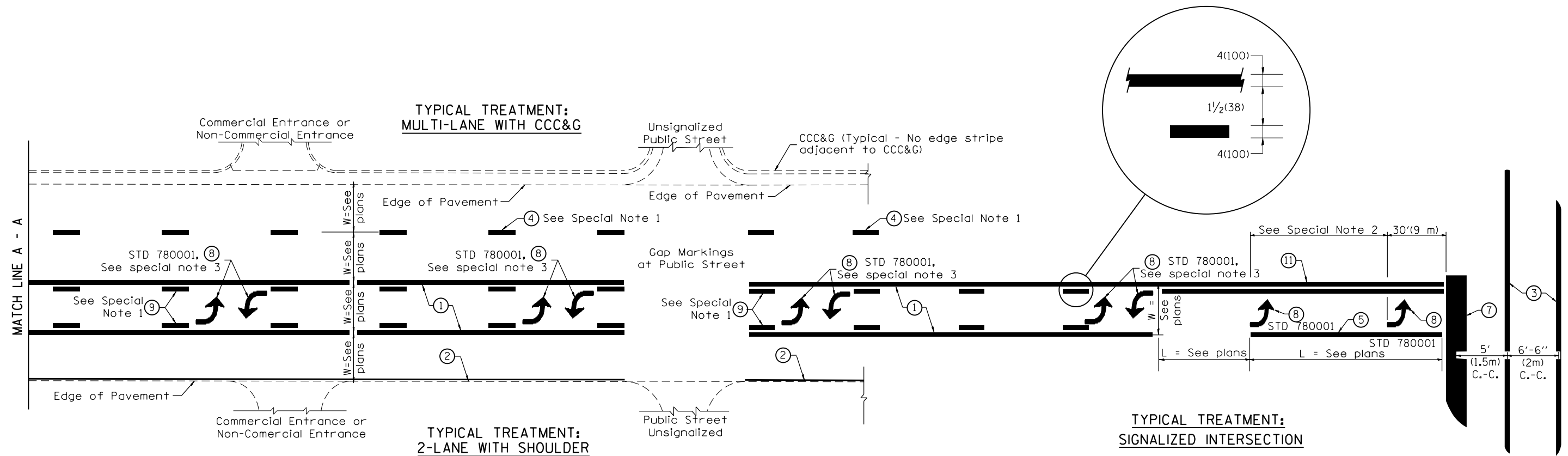
**BRIDGE PARAPET**

**NOTE: PROTECTIVE COAT (SPECIAL) TO COVER FROM POINT A THROUGH POINTS B, C, AND D OF THE EXISTING PARAPET.**

MODEL: D:\MHP...  
 FILE NAME: 68E25\BENTON\BENTON.dwg  
 PLOT DATE: 8/16/2018  
 PLOT SCALE: 100.0000' / in.  
 USER: jochumslg

USER NAME = jochumslg	DESIGNED - CEJ	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PROTECTIVE COATING DETAIL TYPICAL SECTION</b>		F.A.P. RT#	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN - 05-15-2018	REVISOR -	313				[(BR-HB)BJR]	HENDERSON	22	20	
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.		CONTRACT NO. 68E25				
PLOT DATE = 8/16/2018	DATE -	REVISED -		ILLINOIS FED. AID PROJECT						

DESIGNER NOTES:  
1. Include State Standard 780001 (Typical Pavement Markings)



**FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION**

**TYPICAL PAVEMENT MARKING LEGEND**

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)  
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A) ⑩
- ⑪ 4(100) Double Solid (Yellow) ⑪

**SPECIAL NOTES**

1. Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
2. The following shall apply to arrows located in one-way left turn lanes:
  - A. A minimum of two (2) arrows is required.
  - B. The maximum spacing between arrows is 80' (24 m).
  - C. Arrows shall be evenly spaced if three (3) or more are required.
3. The following shall apply to arrow pairs located in two-way left turn lanes:
  - A. A minimum of two (2) arrow pairs is required.
  - B. The maximum spacing between arrow pairs is 200' (61 m).
  - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
  - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

**GENERAL NOTES**

1. Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
2. See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
3. Refer to Article 780.13 for letter, number and symbol areas (sq. ft.)
4. Areas are grooved 1" beyond each edge for the following symbols:  
Through Arrow= 14.8 sq. ft.  
Large Left or Right Arrow= 21.9 sq. ft.  
2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.  
Wrong Way Arrow= 29.5 sq. ft.  
Railroad Crossing Symbol= 69.8 sq. ft.  
(For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

01-01-97	RENUM. F-8.03, NEW REVISION BOX	T.P.	10-16-06	REVISED TO 2007 SPEC.	
02-07-97	ADD BI DIRECTIONAL DIMENSION	J.A.	2/29/16	ADDED GROOVING AREAS	R.D.
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.			
08-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.			

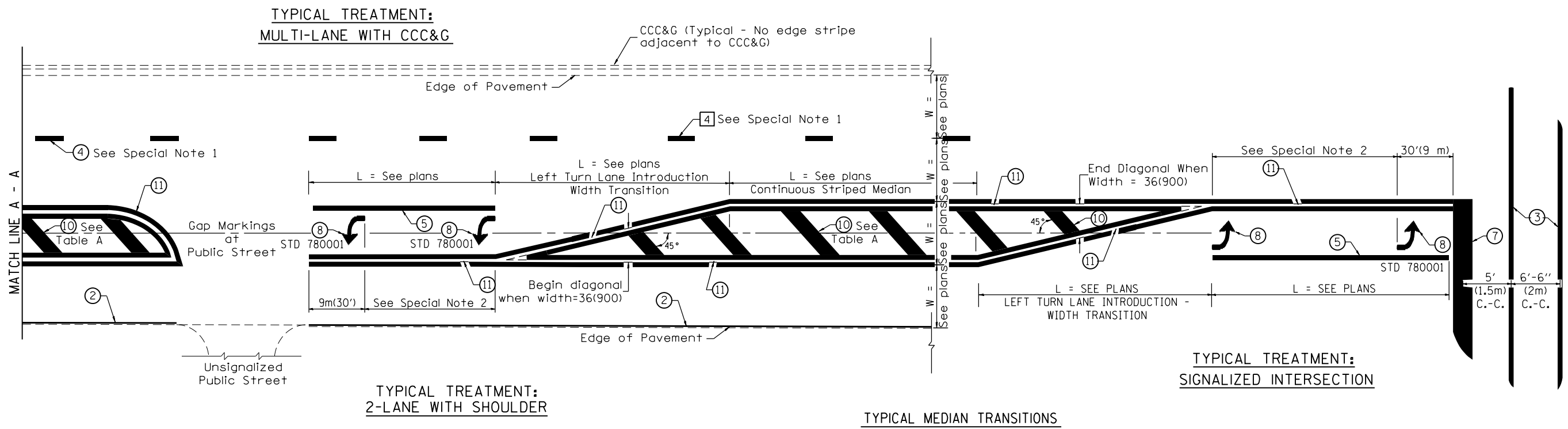
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

NOT TO SCALE

**TYPICAL PAVEMENT MARKINGS**

SHT. 1 OF 2  
CADD STD. 780001-D4

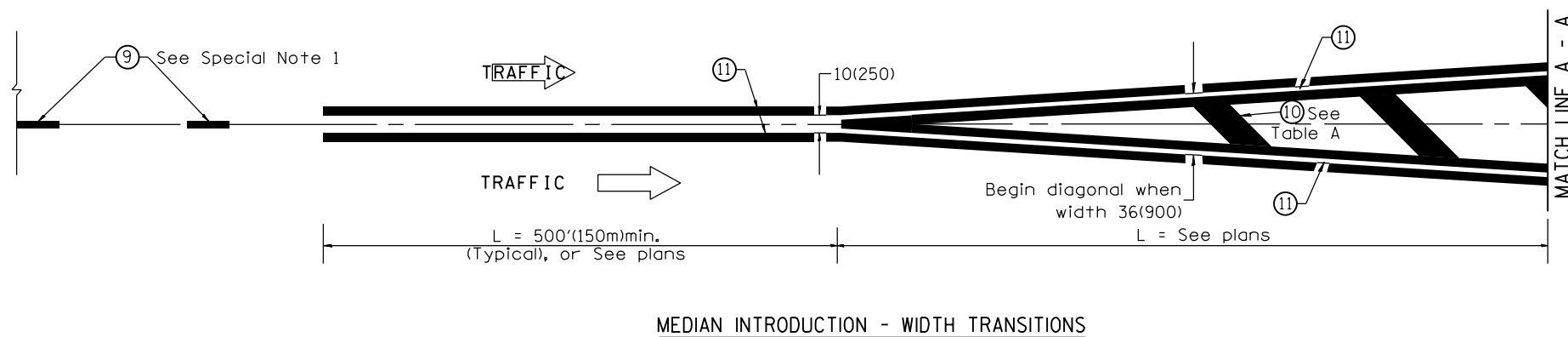
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	[(BR-HB)BR]BJR	HENDERSON	22	21
CONTRACT NO. 68E25				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE

TABLE A  
RECOMMENDED SPACING BETWEEN DIAGONAL LINES

SPEED LIMIT RANGE	INTERSECTION CHANNELIZATION (Includes Width Transitions for Median and Left Turn Lane Introductions)	
	CONTINUOUS	
Less Than 30 mph (50 km/h)	50' (15m)	15' (5m)
30 - 45 mph (50 - 70 km/h)	75' (23m)	20' (6m)
Over 45 mph (70 km/h)	150' (46m)	30' (9m)



All dimensions are in inches (millimeters) unless otherwise noted.