

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 64	95-(5,6)RS-1	WASHINGTON	83	1

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

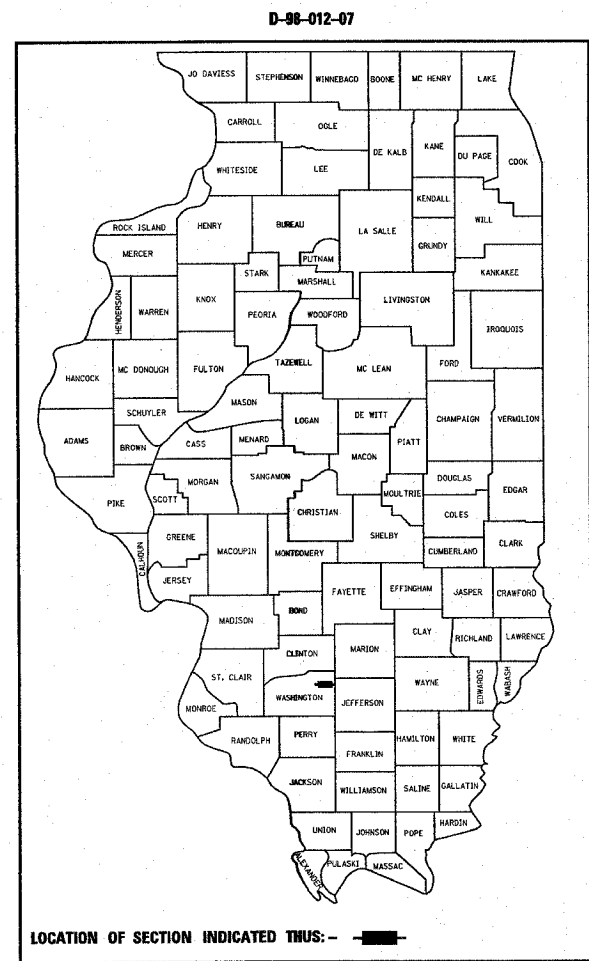
DIVISION OF HIGHWAYS

**PROPOSED  
HIGHWAY PLANS**

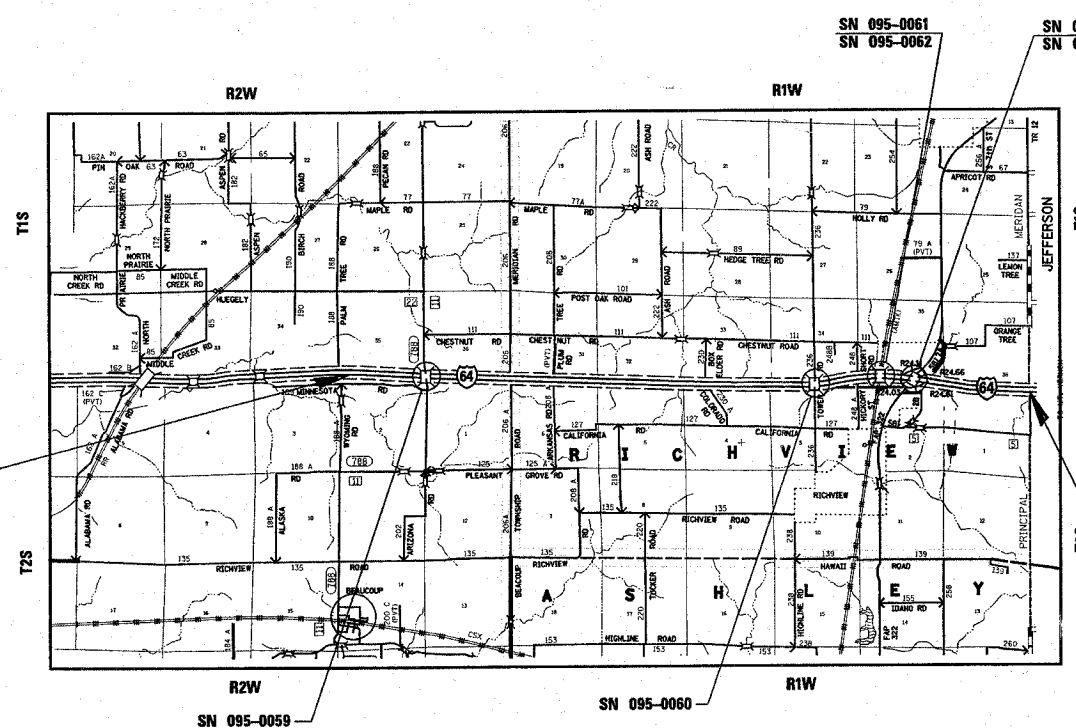
FAI ROUTE 64  
SECTION 95-(5,6)RS-1  
PROJECT: ACIM-064-2(119)057  
WASHINGTON COUNTY

C-98-011-07  
RESURFACING & BRIDGE REPAIRS

FOR INDEX OF SHEETS, SEE SHEET NO. 2



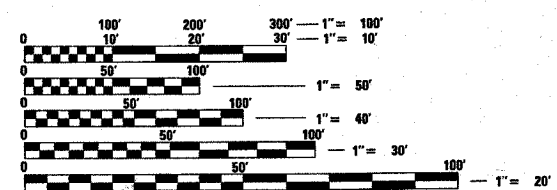
LOCATION OF SECTION INDICATED THUS: —■—



BEGIN PROJECT  
STA 2935+25.00  
LAT 38 23' 29" N  
LONG -89 17' 37" W

END PROJECT  
STA 3355+51.88  
LAT 38 23' 14" N  
LONG -89 08' 44" W

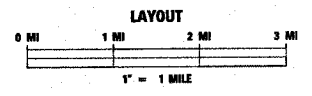
MICROFILMED \_\_\_\_\_  
REEL NUMBER \_\_\_\_\_  
AWARDED \_\_\_\_\_  
RESIDENT ENGINEER \_\_\_\_\_  
AS BUILT CHANGES WERE MADE  
ON THE FOLLOWING SHEETS \_\_\_\_\_



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123

2005 ADT = 19300 (ACTUAL)  
2007 ADT = 19700 (ESTIMATED)  
2027 ADT = 24000 (ESTIMATED)  
S.U. = 5.0% M.U. = 32.6%



PROJECT LENGTH		
GROSS LENGTH	42026.88	7.960 MILES
NET LENGTH	42026.88	7.960 MILES

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED August 22, 2007

*Mary Jamies*  
DEPUTY DIRECTOR OF HIGHWAYS  
REGION FIVE ENGINEER

October 12, 2007  
*Eric E. Horne*  
ENGINEER OF DESIGN AND ENVIRONMENT

October 12, 2007  
*Milton R. Sees*  
DIRECTOR, DIVISION OF HIGHWAYS

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

PROJECT ENGINEER: PATTI LeBEAU (618) 346-3179  
SQUAD LEADER: CHERYL KEPLAR (618) 346-3186

CONTRACT NO. 76A39

GENERAL NOTES

Table with 5 columns: SHEET NO., SECTION, COUNTY, TOTAL SHEETS, SHEET NO. Values: 64, 95-(5,6)RS-1, WASHINGTON, 83, 2.

INDEX OF SHEETS

- 1. COVER SHEET
2. INDEX OF SHEETS, STANDARDS, GENERAL NOTES AND COMMITMENTS
3.-5. SUMMARY OF QUANTITIES
6. LOCATION MAP
7-9. TYPICAL SECTIONS
10.-15. MISCELLANEOUS SCHEDULES
16.-31. PLAN SHEETS
32.-47. PAVEMENT MARKING SHEETS
48.-50. STRUCTURE PLANS 095-0060
51.-57. STRUCTURE PLANS 095-0061, 095-0062
58.-67. STRUCTURE PLANS 095-0063, 095-0064
68.-69. STAGING PLANS FOR MAINLINE STRUCTURES
70.-78. SIGN TRUSS DETAILS
79. SOIL BORING LOGS
80. SIGN PANEL DETAILS
81.-83. MISCELLANEOUS DETAILS
83A. TRAFFIC BARRIER TERMINAL, TYPE 2

- 1. THE STANDARDS AND REVISIONS LISTED APPLY TO THIS PROJECT.
2. ILLINOIS STATE LAW REQUIRES A 48-HOUR NOTICE TO BE GIVEN TO UTILITIES BEFORE DIGGING. FIELD MARKING OF FACILITIES MAYBE OBTAINED BY CONTACTING J.U.L.I.E. OR FOR NON-MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:
- AMEREN IP (ELECTRIC AND GAS)
- CHARTER COMMUNICATIONS, INC.
- AT&T CORPORATION
- COUNTRYMARK COOPERATIVE, LLP
- FRONTIER COMMUNICATIONS COMPANY
- VILLAGE OF HOYLETON
- KINDER MORGAN
- VILLAGE OF RICHVIEW
- AT&T ILLINOIS
- TRI-COUNTY ELECTRIC COOPERATIVE, INC.
- WASHINGTON COUNTY
(MEMBERS OF J.U.L.I.E. (800) 892-0123 ARE INDICATED BY \*. NON J.U.L.I.E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.)
3. THE THICKNESS OF HMA MIXTURES SHOWN IN THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING BASE OR SURFACE ON WHICH THE HMA MIXTURE IS PLACED.

4. THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE TO THIS PROJECT:

Table with 7 columns: MIXTURE USE, SURFACE, BINDER, LEVEL BINDER, PATCHING, I-64 SHOULDERS, TOPLIFT SHOULDERS. Rows include AC/PG, RAP% (MAX), DESIGN AIR VOIDS, MIX COMPOSITION, FRICTION AGG.

\*\*TOPLIFT SHOULDERS - DESIGN THIS MIX AT 2.0% VOID AND ADD ASPHALT TO REDUCE VOIDS TO 1.5%. PLAN QUANTITIES FOR HOT MIX ASPHALT SURFACE COURSE ITEMS ARE CALCULATED USING A UNIT WEIGHT OF 112 LB./SQ YD/IN THICKNESS.

- 5. RAISED REFLECTIVE PAVEMENT MARKERS TO BE REMOVED ARE ESTIMATED ALONG CENTERLINE OF ROADWAY. PRIOR PAVEMENT PATCHING CONTRACT WILL ELIMINATE MANY OF THE RAISED REFLECTIVE PAVEMENT MARKERS. RESIDENT WILL NEED TO CONFIRM QUANTITY IN THE FIELD.
6. THE CONTRACTOR SHALL BE AWARE THAT THERE ARE PCC PATCHES WITHIN THE PROJECT LIMITS. THESE PATCHES SHALL BE MILLED 2" AS PART OF THE 'HMA SURFACE REMOVAL 2"' OPERATION. THE COST FOR MILLING THE PCC PATCHES SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR 'HMA SURFACE REMOVAL 2"' AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
7. THE CONTRACTOR SHALL HAUL 10,000 TONS (89,286 SQ YDS) OF ROTOMILL TO US-51 AND I-64. CONTACT MARK GARD OF THE NASHVILLE YARD AT 618-327-3914 PRIOR TO HAULING. COST SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR 'HOT-MIX ASPHALT SURFACE REMOVAL, 2"' .
8. THE CONTRACTOR AND THE ENGINEER SHALL BE AWARE THAT NO SURVEY WAS PERFORMED FOR THIS PROJECT. THE STATIONING AND TOPO SHOWN IN THE PLANS WAS CREATED USING MICROFILM AND FIELD MEASUREMENTS MADE BY DESIGN PERSONNEL. BOTH SHALL BE ASSUMED TO BE APPROXIMATE.
9. EXISTING INLETS AT THE BRIDGE APPROACHES SHALL BE ADJUSTED TO MATCH THE FINAL SHOULDER SURFACE GRADE, AS DIRECTED BY THE ENGINEER. FRAMES, GRATES & SHOULDER PIPE CULVERTS SHALL BE CLEAN AND FREE OF DEBRIS. THIS WORK WILL BE PAID FOR AS 'INLETS TO BE ADJUSTED'.
10. THE CONTRACTOR SHALL PLACE 'FURNISHED EXCAVATION' AT DRAINAGE STRUCTURES AND LIGHT FOUNDATIONS AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE COST FOR 'AGGREGATE SHOULDER TYPE "B"' .
11. TWO (2) PORTABLE CHANGEABLE MESSAGE SIGNS HAVE BEEN INCLUDED AND WILL BE PLACED AT LOCATIONS DESIGNATED BY THE ENGINEER. NO ADDITIONAL PAYMENT WILL BE MADE FOR ANY RELOCATION OF THE SIGNS.
12. SPEED LIMIT PHOTO ENFORCEMENT SIGNS MAY BE REQUIRED ON THIS PROJECT. IF REQUIRED, THE SIGNS WILL BE PROVIDED BY IDOT OPERATIONS. METHOD OF INSTALLATION TO BE PROVIDED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE TRAFFIC CONTROL PAY ITEMS.

- 13. ONCE THE CONTRACTOR PERFORMS THE HMA SURFACE REMOVAL, HE SHALL HAVE 14 CALENDAR DAYS TO COMPLETE THE PLACEMENT OF UNDERDRAINS AND PLACEMENT OF HMA CONCRETE BINDER COURSE. IF THE CONTRACTOR FAILS TO COMPLY, HE WILL BE SUBJECT TO LIQUIDATED DAMAGES AS SPECIFIED IN ARTICLE 108.09 OF THE STANDARD SPECIFICATIONS.
14. NO OVERNIGHT (DUSK TO DAWN) LANE CLOSURES WILL BE ALLOWED FROM DECEMBER 1 TO APRIL 1 OF ANY YEAR.
15. THE REMOVAL OF ANY ITEM REQUIRED FOR THE EXPANSION JOINT REPLACEMENT WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE JOINT REPLACEMENT PAY ITEM.
16. FLAGMEN SHALL BE PRESENT DURING ALL CLOSURE HOURS WHEN WORKERS ARE PRESENT, INCLUDING LUNCH HOUR AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
17. ALL CONSTRUCTION SIGNS SHALL BE FLUORESCENT ORANGE IN COLOR, NO ADDITIONAL COMPENSATION SHALL BE ALLOWED. ALL SIGNS SHALL BE SKID MOUNTED.
18. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIAL.
19. SHORT-TERM PAVEMENT MARKING SHALL BE APPLIED TO THE MILLED, LEVELING BINDER, AND FINAL BITUMINOUS SURFACE. A QUANTITY FOR TEMPORARY PAVEMENT MARKING EQUAL TO THE AMOUNT OF PERMANENT PAVEMENT MARKING HAS BEEN ADDED TO THE PLANS. ONLY REMOVAL OF SHORT TERM PAVEMENT MARKING REMOVAL FROM FINAL SURFACE TO BE PAID AS 'WORK ZONE PAVEMENT MARKING REMOVAL'.
20. A QUANTITY OF 1361 FT OF 'PAVEMENT MARKING REMOVAL' HAS BEEN INCLUDED IN THE PLANS TO BE USED TO REMOVE THE PAVEMENT MARKING ON THE CONCRETE BRIDGE DECKS AND THE CONCRETE PATCHES WHICH WILL NOT BE RESURFACED.
21. THE QUANTITY FOR THE 'PIPE UNDERDRAIN 4"' IS BASED ON THE APPROXIMATE PROJECT LENGTH MULTIPLIED BY A FACTOR OF FOUR. THE 'PIPE UNDERDRAIN 4"' (SPECIAL) QUANTITY IS COMPUTED PER ARTICLE 601.04 OF THE STANDARD SPECIFICATIONS USING AN ESTIMATED LENGTH OF 20' PER OUTLET.
22. PATCHING QUANTITIES ARE AN ESTIMATE CALCULATED USING A PERCENTAGE OF A PRIOR PATCHING CONTRACT AND ARE INCLUDED IN ANTICIPATION OF FUTURE FAILURES. THE ACTUAL QUANTITIES AND LOCATION OF THE PATCHES SHALL BE DETERMINED BY THE ENGINEER.

HIGHWAY STANDARDS

- 000001-04
280001-03
442001-03
601001-01
601101
630001-07
631011-03
631026-03
631031-06
635001
635006-02
635011-01
642001
701201-02
701400-02
701401-03
701402-05
701406-04
702001-06
704001-03
780001-01
781001-02

Table with 2 columns: REVISIONS, NAME, DATE. Includes a 'Rev.' label.

ILLINOIS DEPARTMENT OF TRANSPORTATION INDEX OF SHEETS, STANDARDS, GENERAL NOTES AND COMMITMENTS. FAI ROUTE 64 SECTION 95-(5,6)RS-1 WASHINGTON COUNTY. DRAWN BY:

8/21/2007

# SUMMARY OF QUANTITIES

SHEET NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-15,6(RS-1)	WASHINGTON	83	3
STA. _____		TO STA. _____		
CONTRACT NO.: 76A39				

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE							
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	I000	X231-2A SN 095-0060	X131-2A SN 095-0061	X131-2A SN 095-0062	X231-2A SN 095-0063	X231-2A SN 095-0064	Y002-1C 100% STATE
28100105	STONE RIPRAP, CLASS A3	SQ YD	12					6	6	
28101200	DUMPED RIPRAP	TON	288	288						
28200200	FILTER FABRIC	SQ YD	24					12	12	
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	197.4	197.4						
40600300	AGGREGATE (PRIME COAT)	TON	718	718						
40600655	LEVELING BINDER (MACHINE METHOD), N105	TON	586	586						
40600895	CONSTRUCTING TEST STRIP	EACH	2	2						
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	2877	2877						
40600990	TEMPORARY RAMP	SQ YD	300	300						
40603095	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N105	TON	30329	30329						
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	135	135						
44000152	HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"	SQ YD	4799	4799						
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	229020	229020						
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	1423	1423						
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SQ YD	834	834						
44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	108	108						
44200525	CLASS A PATCHES, TYPE I, 8 INCH	SQ YD	8	8						
44200529	CLASS A PATCHES, TYPE II, 8 INCH	SQ YD	20	20						
44200533	CLASS A PATCHES, TYPE III, 8 INCH	SQ YD	30	30						
44200573	CLASS A PATCHES, TYPE I, 12 INCH	SQ YD	16	16						
44200577	CLASS A PATCHES, TYPE II, 12 INCH	SQ YD	27	27						
44200581	CLASS A PATCHES, TYPE III, 12 INCH	SQ YD	64	64						
44213000	PATCHING REINFORCEMENT	SQ YD	165	165						
44213200	SAW CUTS	FOOT	825	825						
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	2689	2689						
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	14138	14138						
48203100	HOT-MIX ASPHALT SHOULDERS	TON	30734	30734						
50102400	CONCRETE REMOVAL	CU YD	90.8			11.2	11.2	34.2	34.2	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	90.8			11.2	11.2	34.2	34.2	
50300260	BRIDGE DECK GROOVING	SQ YD	101					50.5	50.5	
50300300	PROTECTIVE COAT	SQ YD	1696			848	848			
50300530	FLOOR DRAIN EXTENSION	EACH	4		4					
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	10190			1860	1860	3235	3235	
50800515	BAR SPLICERS	EACH	128			30	30	34	34	
52000110	PREFORMED JOINT STRIP SEAL	FOOT	383			82	82	109.5	109.5	

PLOT DATE = 9/15/2007  
 PLOT SCALE = 1/8" = 100'-0"  
 PLOT STYLE = \*PREP\*  
 REFERENCE = \*PREP\*

Rev

# SUMMARY OF QUANTITIES

FILE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
64	95-(5,6)RS-1	WASHINGTON	83	4
STA.		TO STA.		
CONTRACT NO.: 76A39				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		I000	X231-2A SN 095-0060	X131-2A SN 095-0061	X131-2A SN 095-0062	X231-2A SN 095-0063	X231-2A SN 095-0064
58700200	BRIDGE SEAT SEALER	SQ FT	486		243	243			
58700300	CONCRETE SEALER	SQ FT	19602				9801	9801	
59000200	EPOXY CRACK INJECTION	FOOT	10				10		
59300100	CONTROLLED LOW-STRENGTH MATERIAL	CU YD	182		104	78			
60100060	CONCRETE HEADWALL FOR PIPE DRAINS	EACH	412	412					
60100072	SHOULDER REMOVAL AND REPLACEMENT 5"	FOOT	171392	171392					
60107600	PIPE UNDERDRAINS 4"	FOOT	164594	164594					
60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	8240	8240					
60260100	INLETS TO BE ADJUSTED	EACH	2	2					
* 63000000	STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	4112.5	4112.5					
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	6	6					
* 63100070	TRAFFIC BARRIER TERMINAL, TYPE 5	EACH	2	2					
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	8	8					
* 63100167	TRAFFIC BARRIER TERMINAL TYPE 1, SPECIAL (TANGENT)	EACH	8	8					
* 63100169	TRAFFIC BARRIER TERMINAL TYPE 1, SPECIAL (FLARED)	EACH	4	4					
63200310	GUARDRAIL REMOVAL	FOOT	3762.5	3762.5					
63500105	DELINEATORS	EACH	333	333					
63500120	DELINEATOR REMOVAL	EACH	270	270					
64200105	SHOULDER RUMBLE STRIP	FOOT	173664	173664					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	18	18					
67100100	MOBILIZATION	L SUM	1	1					
70100207	TRAFFIC CONTROL AND PROTECTION, STANDARD 701402	EACH	4		1	1	1	1	
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	4	4					
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1		1				
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1	1					
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1					
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	16	16					
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	36330	36330					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	172500	172500					
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	2180	2180					
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	12110	12110					
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1475	1475					
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1475	1475					
* 72000300	SIGN PANEL - TYPE 3	SQ FT	184						184

PLOT DATE = 6/23/2007  
 PLOT SCALE = 1/8" = 100'  
 PLOT REFERENCE = #REF\*

\* SPECIALTY ITEMS

# SUMMARY OF QUANTITIES

90% P.E.D. / 10% STATE

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	5
STA. _____		TO STA. _____		
CONTRACT NO.: 76A39				

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE							
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	I000	X231-2A SN 095-0060	X131-2A SN 095-0061	X131-2A SN 095-0062	X231-2A SN 095-0063	X231-2A SN 095-0064	Y002-1C 100% STATE
* 72400330	REMOVE SIGN PANEL - TYPE 3	SQ FT	184	184						
* 73302210	OVERHEAD SIGN STRUCTURE - CANTILEVER, TYPE III-C-A (36" X 7'-0")	FOOT	32	32						
* 73305000	OVERHEAD SIGN STRUCTURE WALKWAY	FOOT	20	20						
* 73400200	DRILLED SHAFT CONCRETE FOUNDATIONS	CU YD	10	10						
* 73600200	REMOVE OVERHEAD SIGN STRUCTURE - CANTILEVER	EACH	1	1						
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	172500	172500						
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	2180	2180						
78004210	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 4"	FOOT	20642	20642						
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	3005	3005						
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	2153	2153						
* 78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	20	20						
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	65	65						
* 78200520	BARRIER WALL MARKERS, TYPE B	EACH	32	32						
* 78200530	BARRIER WALL MARKERS, TYPE C	EACH	32	32						
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	12	12						
78300100	PAVEMENT MARKING REMOVAL	SQ FT	1361	1361						
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	2123	2123						
X0321468	PLUG EXISTING DECK DRAINS	EACH	8		8					
X0322729	MATERIAL TRANSFER DEVICE	TON	58459	58459						
X0322932	SILICONE JOINT SEALER, 1.5"	FOOT	60		60					
X0323491	SLOPE WALL CRACK SEALING	FOOT	434		58	188	188			
X0323586	PIPE DRAIN REMOVAL	FOOT	164594	164594						
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH LESS THAN OR EQUAL TO 5 INCHES)	SQ FT	182			14	14	77	77	
X4066580	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	TON	28130	28130						
Z0010400	CLEANING BRIDGE SEATS	SQ FT	1049			243	243	281.5	281.5	
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	4	4						
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1			0.5	0.5			
Z0076850	UN SOUND CONCRETE REMOVAL	SQ YD	35			16	19			

\*SPECIALTY ITEMS

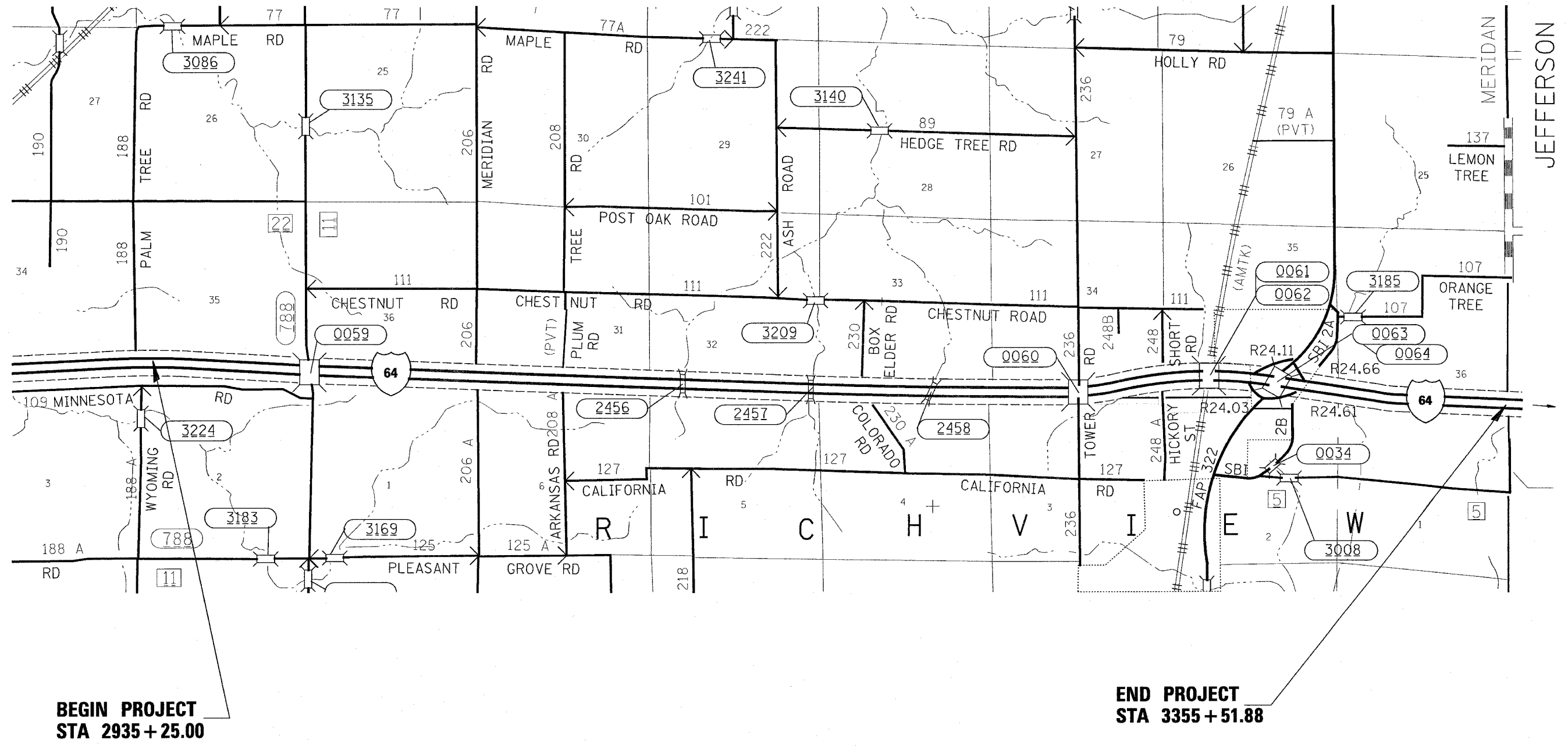
Rev.

FAI ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON		6
STA.	TO STA.			
			CONTRACT NO.: 76A38	

PLAN	DATE
NO.	
BY	
DATE	
REVISIONS	
ALIGNED	
CHECKED	
PLANNED	
FILE NAME	

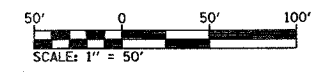
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BY	
DATE	
REVISIONS	
GRADES	
CHECKED	
NOTED	
STRUCTURE	
NOTATION	

PLOT DATE = 8/21/2007  
 PLOT SCALE = 1/8" = 100'  
 REFERENCE = \*REF\*



**BEGIN PROJECT**  
**STA 2935 + 25.00**

**END PROJECT**  
**STA 3355 + 51.88**



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

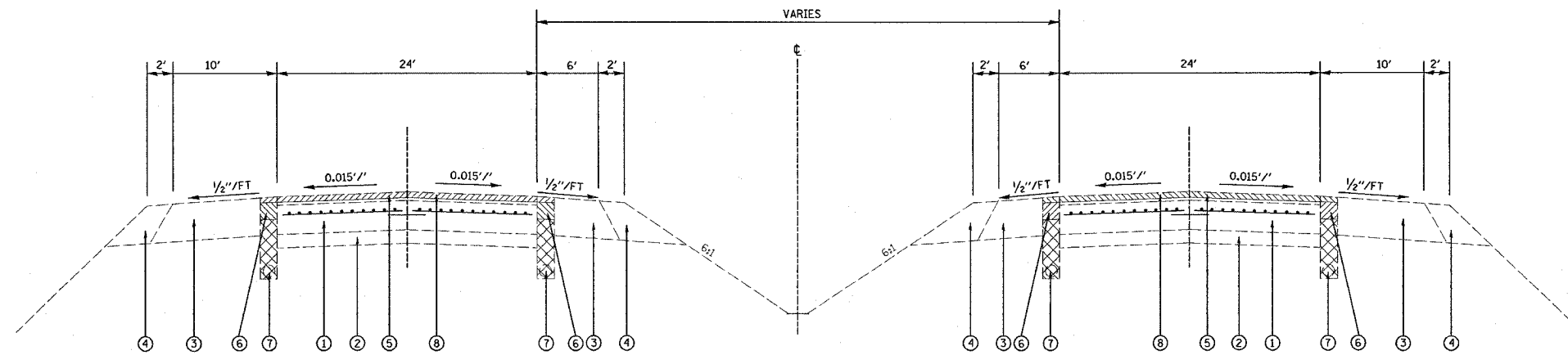
**LOCATION MAP**

FAI ROUTE 64  
SECTION 95-(5,6)RS-1  
WASHINGTON COUNTY

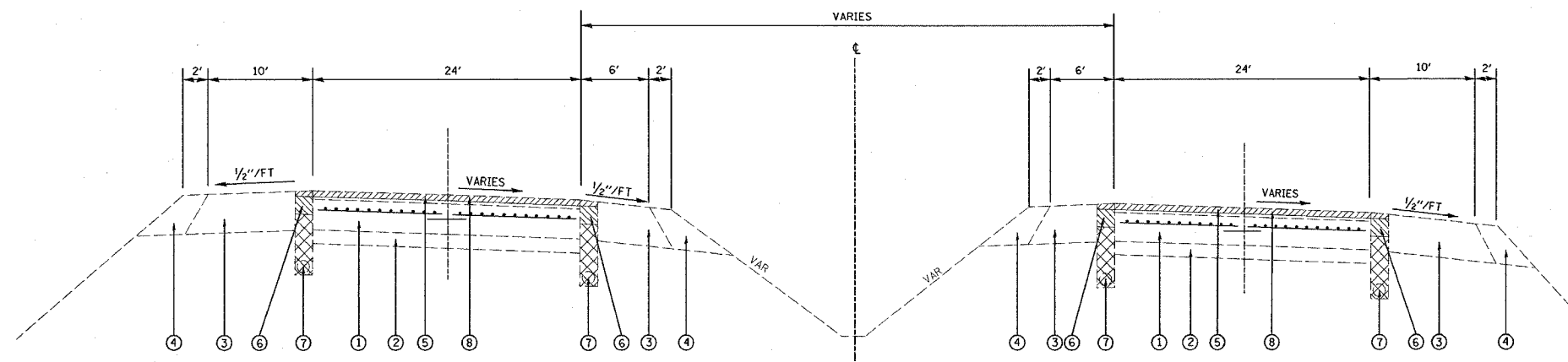
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FAI ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	7
STA.		TO STA.		
CONTRACT NO.: 76A38				

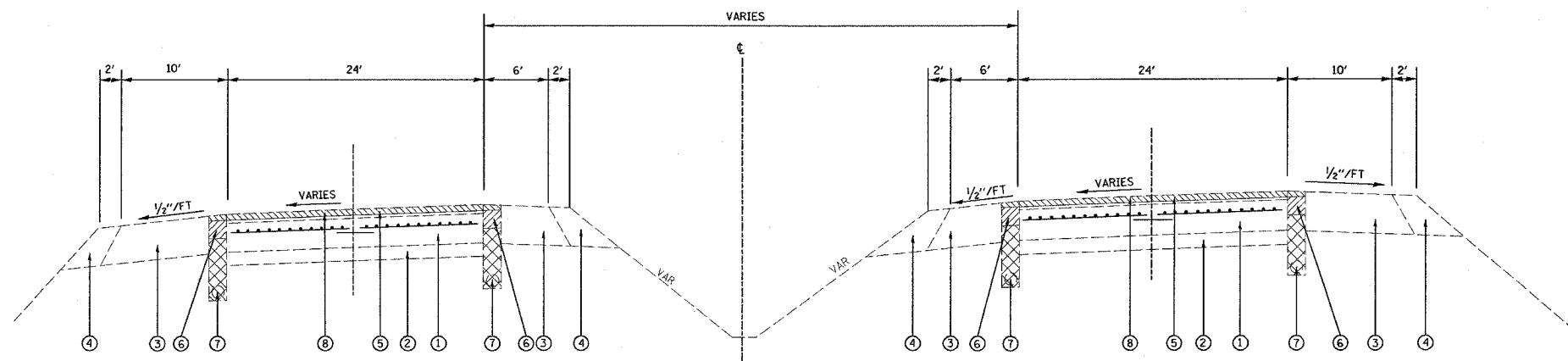
DATE	BY
PLAN	SURVEYED
	ALIGNED
	NOTED
	CHECKED
	NO. FILE NAME



TYPICAL SECTION I-64  
 STA. 2935+25.00 TO STA. 3206+82.50  
 STA. 3233+89.25 TO STA. 3249+91.61  
 STA. 3274+69.87 TO STA. 3310+10.82  
 STA. 3329+80.82 TO STA. 3355+51.88



TYPICAL SECTION I-64  
 STA. 3249+91.61 TO STA. 3274+69.87



TYPICAL SECTION I-64  
 STA. 3206+82.56 TO STA. 3233+89.25  
 STA. 3310+10.82 TO STA. 3329+80.82

LEGEND

- ① EXISTING CONT. REINFORCED P.C.C. PAVEMENT 8"
- ② EXISTING STABILIZED SUB-BASE 4"
- ③ EXISTING STABILIZED SHOULDERS 1 1/4"
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING BITUMINOUS OVERLAY - 3 1/4"
- ⑥ PROPOSED SHOULDER REMOVAL & REPLACEMENT 5"
- ⑦ PROPOSED PIPE UNDERDRAIN REMOVAL
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2"

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**EXISTING**  
**TYPICAL SECTIONS**  
 FAI ROUTE 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

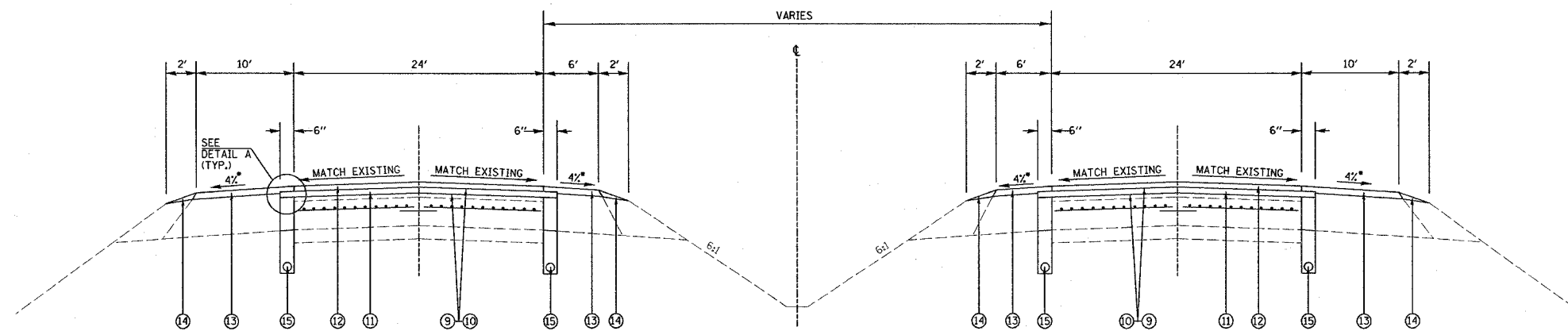
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PLOT DATE: 8/21/2007

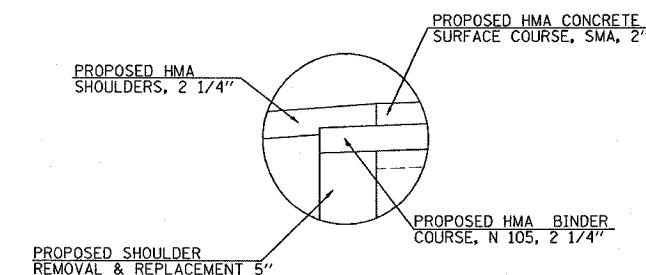
8/21/2007  
 8/21/2007  
 8/21/2007  
 8/21/2007  
 8/21/2007

FAI ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	8
STA.	TO STA.			
CONTRACT NO.: 76A38				

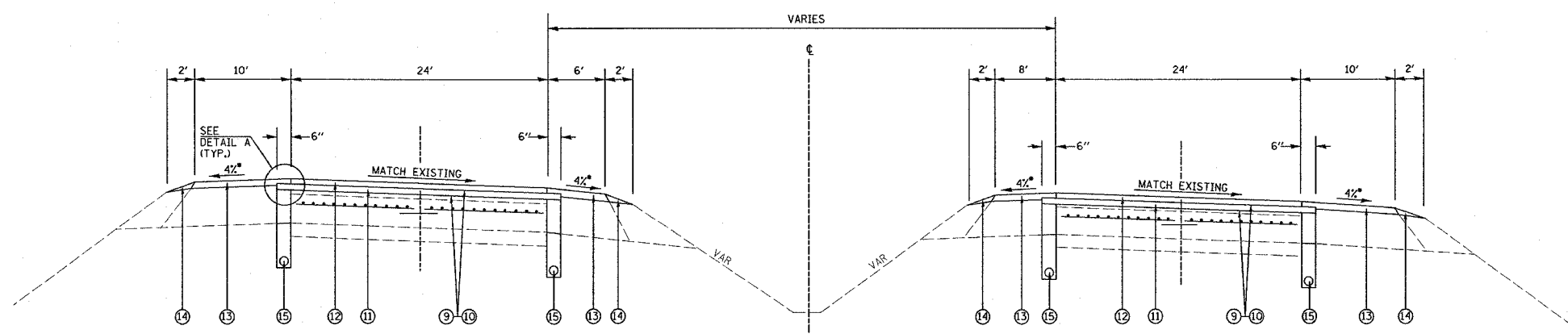
PLAN	DATE	BY
SCREENED		
PLOTTED		
NOTE BOOK		
NO.		



TYPICAL SECTION I-64  
 STA. 2935+25.00 TO STA. 3206+82.50  
 STA. 3233+89.25 TO STA. 3249+91.61  
 STA. 3274+69.87 TO STA. 3310+10.82  
 STA. 3329+80.82 TO STA. 3355+51.88



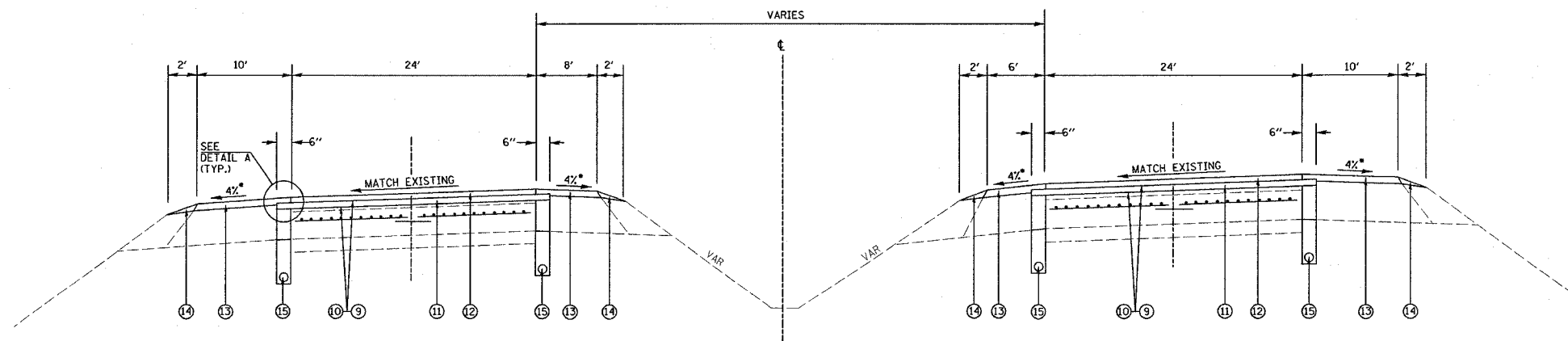
DETAIL A



TYPICAL SECTION I-64  
 STA. 3249+91.61 TO STA. 3274+69.87

LEGEND

- ① EXISTING CONT. REINFORCED P.C.C. PAVEMENT 8"
- ② EXISTING STABILIZED SUB-BASE 4"
- ③ EXISTING STABILIZED SHOULDERS 11 1/4"
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING BITUMINOUS OVERLAY - 3 1/4"
- ⑥ PROPOSED SHOULDER REMOVAL & REPLACEMENT 5"
- ⑦ PROPOSED PIPE UNDERDRAIN REMOVAL
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL
- ⑨ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
- ⑩ PROPOSED AGGREGATE (PRIME COAT)
- ⑪ PROPOSED HOT-MIX ASPHALT BINDER COURSE, N 105, 2 1/4"
- ⑫ PROPOSED HOT-MIX ASPHALT CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, 2"
- ⑬ PROPOSED HOT-MIX ASPHALT SHOULDERS
- ⑭ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑮ PROPOSED PIPE UNDERDRAIN, 4"



TYPICAL SECTION I-64  
 STA. 3206+82.56 TO STA. 3233+89.25  
 STA. 3310+10.82 TO STA. 3329+80.82

\*NOTE: FIELD MEASUREMENTS SHOW THE EXISTING SHOULDER SLOPE VARIES BETWEEN 4% AND 6%. THE HMA SHOULDER PAY ITEM INCLUDES QUANTITY TO BRING THE SHOULDER BACK TO A 4% SLOPE.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PROPOSED  
 TYPICAL SECTIONS**  
 FAI ROUTE 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

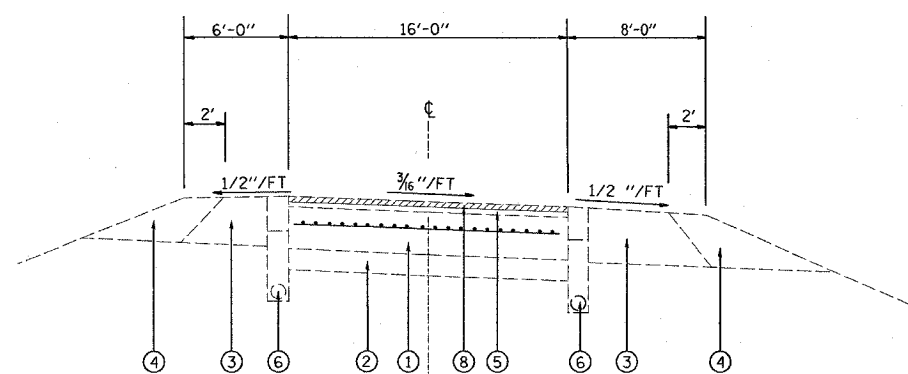
DRAWN BY:

PLOT DATE: 8/21/2007

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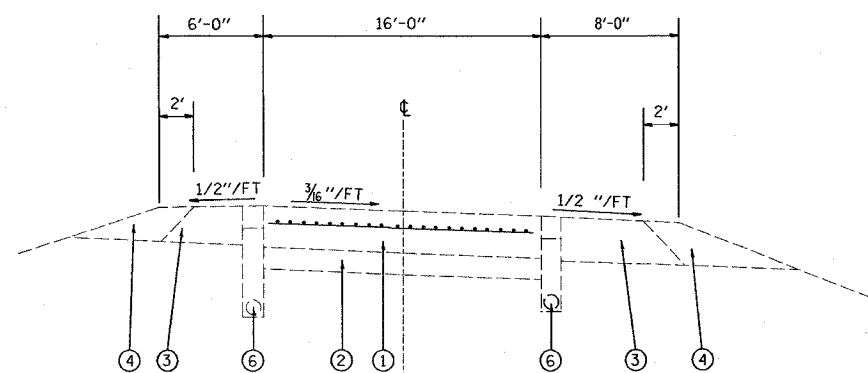


FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	9
SHEET NO.		TO STA.		
CONTRACT NO.: 76A38				



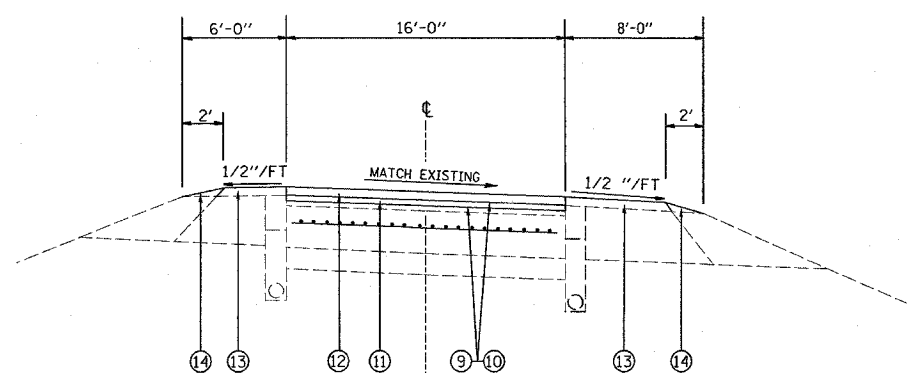
EXISTING TYPICAL SECTION RAMP

- RAMP 1 109+15 - 120+00
- RAMP 2 200+00 - 206+95
- RAMP 3 300+30 - 305+75
- RAMP 4 407+93 - 419+76



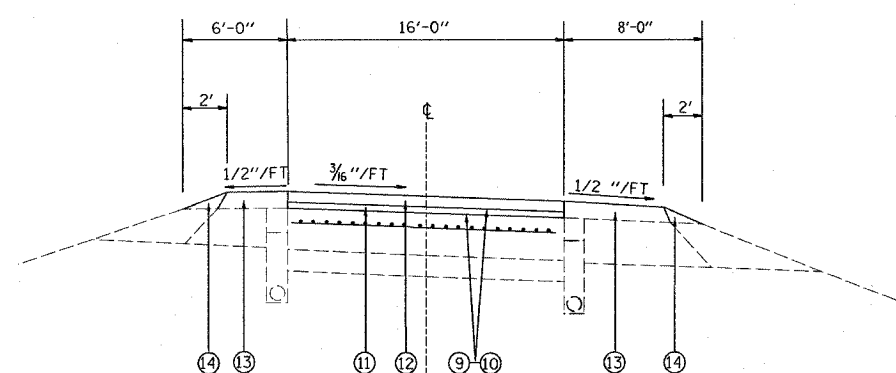
EXISTING TYPICAL RAMP SECTION

- RAMP 1 102+00 - 109+15
- RAMP 2 206+95 - 211+50
- RAMP 3 305+75 - 315+00
- RAMP 4 402+25 - 407+93



PROPOSED TYPICAL SECTION RAMP

- RAMP 1 109+15 - 120+79
- RAMP 2 200+00 - 206+95
- RAMP 3 300+30 - 305+75
- RAMP 4 407+93 - 419+76

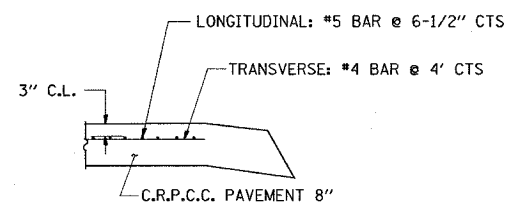


PROPOSED TYPICAL RAMP SECTION

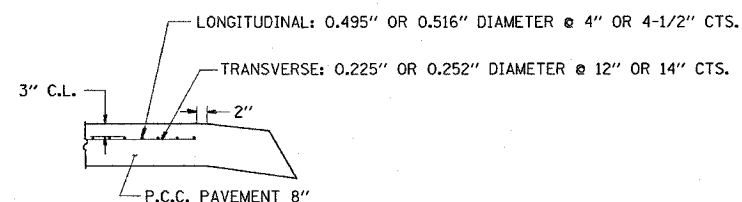
- RAMP 1 102+00 - 109+15
- RAMP 2 206+95 - 211+50
- RAMP 3 305+75 - 315+00
- RAMP 4 402+25 - 407+93

LEGEND

- ① EXISTING CONT. REINFORCED P.C.C. PAVEMENT 8"
- ② EXISTING STABILIZED SUB-BASE 4"
- ③ EXISTING STABILIZED SHOULDERS 11 1/4"
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING BITUMINOUS OVERLAY - 3 1/4"
- ⑥ EXISTING PIPE UNDERDRAIN
- ⑦ PROPOSED SHOULDER REMOVAL & REPLACEMENT 5"
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL 3/4"
- ⑨ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
- ⑩ PROPOSED AGGREGATE (PRIME COAT)
- ⑪ LEVELING BINDER COURSE (MACHINE METHOD) N105, 1"
- ⑫ PROPOSED HOT-MIX ASPHALT CONCRETE SURFACE COURSE, STONE MATRIX ASPHALT, 2"
- ⑬ PROPOSED HOT-MIX ASPHALT SHOULDERS
- ⑭ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑮ PROPOSED PIPE UNDERDRAIN, 4"



DETAIL OF EXISTING PAVEMENT REINFORCEMENT



DETAIL OF EXISTING RAMP REINFORCEMENT

PLOT DATE = 8/24/2007  
 PLOT NAME = 95-5,6RS-1.dwg  
 PLOT SCALE = 1/8" = 1'-0"  
 REFERENCE = #REF#

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION TYPICAL RAMP SECTIONS FAI ROUTE 64 SECTION 95-(5,6)RS-1 WASHINGTON COUNTY
NAME	DATE	
		SCALE: VERT. _____ HORIZ. _____ DATE _____

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1 WASHINGTON	83	10

CONTRACT NO.: 76A38

## PIPE UNDERDRAIN SCHEDULE

LOCATION	PIPE UNDERDRAINS 4"	PIPE UNDERDRAINS 4" (SPECIAL)	CONCRETE HEADWALL FOR PIPE DRAINS	SHOULDER REMOVAL AND REPLACEMENT 5"	PIPE DRAIN REMOVAL
STA TO STA	(FT)	(FT)	(EACH)	(FT)	(FT)
<b>FAI 64 EAST BOUND</b>					
2935+25.00 TO 2943+00.00	1550	78	3	1248	1550
2943+00.00 TO 2973+00.00	6000	300	15	6240	6000
2973+00.00 TO 2981+14.00	1628	81	4	1664	1628
2983+94.00 TO 3002+00.00	3612	181	9	3744	3612
3002+00.00 TO 3033+00.00	6200	310	16	6656	6200
3033+00.00 TO 3063+00.00	6000	300	15	6240	6000
3063+00.00 TO 3093+00.00	6000	300	15	6240	6000
3093+00.00 TO 3123+00.00	6000	300	15	6240	6000
3123+00.00 TO 3153+00.00	6000	300	15	6240	6000
3153+00.00 TO 3183+00.00	6000	300	15	6240	6000
3183+00.00 TO 3213+00.00	6000	300	15	6240	6000
3213+00.00 TO 3221+25.00	1650	87	4	1664	1650
3223+55.00 TO 3243+00.00	3890	195	10	4160	3890
3243+00.00 TO 3262+84.00	3968	198	10	4160	3968
3264+46.00 TO 3273+00.00	1708	85	4	1664	1708
3273+00.00 TO 3282+12.00	1824	91	5	2080	1824
3284+18.00 TO 3288+00.00	764	38	2	832	764
3288+00.00 TO 3303+00.00	3000	150	8	3328	3000
3303+00.00 TO 3333+00.00	6000	300	15	6240	6000
3333+00.00 TO 3355+51.00	4502	225	11	4576	4502
<b>FAI 64 WEST BOUND</b>					
2935+25.00 TO 2943+00.00	1550	78	3	1248	1550
2943+00.00 TO 2973+00.00	6000	300	15	6240	6000
2973+00.00 TO 2981+14.00	1628	81	4	1664	1628
2983+94.00 TO 3002+00.00	3612	181	9	3744	3612
3002+00.00 TO 3033+00.00	6200	310	16	6656	6200
3033+00.00 TO 3063+00.00	6000	300	15	6240	6000
3063+00.00 TO 3093+00.00	6000	300	15	6240	6000
3093+00.00 TO 3123+00.00	6000	300	15	6240	6000
3123+00.00 TO 3153+00.00	6000	300	15	6240	6000
3153+00.00 TO 3183+00.00	6000	300	15	6240	6000
3183+00.00 TO 3213+00.00	6000	300	15	6240	6000
3213+00.00 TO 3221+25.00	1650	88	4	1664	1650
3223+55.00 TO 3243+00.00	3890	195	10	4160	3890
3243+00.00 TO 3262+98.00	3996	200	10	4160	3996
3264+59.00 TO 3273+00.00	1682	84	4	1664	1682
3273+00.00 TO 3282+91.00	1982	99	5	2080	1982
3284+97.00 TO 3288+00.00	606	30	2	832	606
3288+00.00 TO 3303+00.00	3000	150	8	3328	3000
3303+00.00 TO 3333+00.00	6000	300	15	6240	6000
3333+00.00 TO 3355+51.00	4502	225	11	4576	4502
<b>TOTAL=</b>	<b>164594</b>	<b>8240</b>	<b>412</b>	<b>171392</b>	<b>164594</b>

## DELINEATOR/BARRIER WALL MARKER SCHEDULE

WESTBOUND			EASTBOUND			BARRIER WALL MARKERS		
LOCATION	DELINEATOR REMOVAL	DELINEATOR	LOCATION	DELINEATOR REMOVAL	DELINEATOR	TYPE B	TYPE C	
(STATION)	(EACH)	(EACH)	(STATION)	(EACH)	(EACH)	(EACH)	(EACH)	
<b>MAINLINE</b>			<b>MAINLINE</b>					
2900+15			2900+15					
2900+62.3	1	1	2900+62.3	1	1			
2935+25			2935+25					
3355+90	77	105	3355+90	80	105			
<b>U-TURNS</b>			<b>U-TURNS</b>					
3004+50	3	4	3004+50	4	4			
3244+05	5	5	3244+05	4	4			
3350+00	5	5	3350+00	4	4			
<b>U.S. 51 INTERCHANGE</b>			<b>U.S. 51 INTERCHANGE</b>					
RAMP I (N.W.)	26	26	RAMP III (S.W.)	18	23			
RAMP II (N.E.)	16	20	RAMP IV (S.E.)	26	26			
SN 095-0061						8	8	
SN 095-0062						8	8	
SN 095-0063						8	8	
SN 095-0064						8	8	
<b>TOTALS</b>	<b>133</b>	<b>166</b>	<b>TOTALS</b>	<b>137</b>	<b>167</b>			
<b>TOTAL REMOVAL</b>							<b>270</b>	
<b>TOTAL INSTALLATION</b>							<b>333</b>	<b>32</b>

## BUTT-JOINT/TEMP. RAMPS SCHEDULE

LOCATION	HMA SURFACE REMOVAL - BUTT JOINT	TEMPORARY RAMPS
STA. TO STA.	(sq yds)	(sq yds)
2934+95 to 2935+25	EB 133	20
2934+96 to 2935+25	WB 133	20
2981+15 to 2981+45	EB 133	20
2981+15 to 2981+45	WB 133	20
2983+45 to 2983+75	EB 133	20
2983+45 to 2983+75	WB 133	20
3221+15 to 3221+35	EB 133	20
3223+35 to 3223+75	EB 133	20
3281+82 to 3282+12	EB 186	20
3282+61 to 3282+91	WB 186	20
3284+18 to 3284+48	EB 186	20
3284+97 to 3285+27	WB 186	20
3355+52 to 3355+82	EB 133	20
3355+52 to 3355+82	WB 133	20
101+40 to 102+00	R1 200	5
315+00 to 315+60	R2 200	5
211+50 to 212+10	R3 200	5
401+65 to 402+25	R4 200	5
<b>TOTAL</b>	<b>2877</b>	<b>300</b>

## INLET TBA SCHEDULE

LOCATION	INLETS TO BE ADJUSTED
STATION	(EACH)
3284+25 (EB)	1
3284+95 (WB)	1
<b>TOTAL</b>	<b>2</b>

## RIPRAP SCHEDULE

LOCATION	DUMPED RIPRAP
STATION	(TON)
3099+92 (WB)	96
3139+70 (WB)	96
3182+56 (WB)	96
<b>TOTAL</b>	<b>288</b>

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**MISC. SCHEDULES**  
 FAI ROUTE 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

SCALE: VERT.  
 DATE: HORIZ.

DRAWN BY  
 CHECKED BY

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	11
STA. _____ TO STA. _____		CONTRACT NO.: 76A39		

# SURFACE REMOVAL SCHEDULE

LOCATION			HMA SURFACE REMOVAL 2"	HMA SURFACE REMOVAL 2 1/4"	HMA SURFACE REMOVAL 3"	HMA SURFACE REMOVAL 3/4"	HMA SURFACE REMOVAL - VARIABLE DEPTH
STA	TO	STA	(SQ YD)	(SQ YD)	(SQ YD)	(SQ YD)	(SQ YD)
FAI 64 EAST BOUND							
2935+25.00	TO	2943+00.00	2152.78				
2943+00.00	TO	2973+00.00	8333.33				
2973+00.00	TO	2981+45.00	2347.22				
2981+45.00	TO	2983+45.00		355.56			
2983+45.00	TO	3002+00.00	5152.78				
3002+00.00	TO	3033+00.00	8611.11				
3033+00.00	TO	3063+00.00	8333.33				
3063+00.00	TO	3093+00.00	8333.33				
3093+00.00	TO	3123+00.00	8333.33				
3123+00.00	TO	3153+00.00	8333.33				
3153+00.00	TO	3183+00.00	8333.33				
3183+00.00	TO	3213+00.00	8333.33				
3213+00.00	TO	3221+35.00	2319.44				
3221+35.00	TO	3223+35.00		355.56			
3223+55.00	TO	3243+00.00	5402.78				
3243+00.00	TO	3262+84.00	5511.11				
3264+46.00	TO	3273+00.00	2372.22				
3273+00.00	TO	3282+12.00	2533.33				
3284+18.00	TO	3288+00.00	1061.11				
3288+00.00	TO	3303+00.00	4166.67				
3303+00.00	TO	3333+00.00	8333.33				
3333+00.00	TO	3355+51.00	6252.78				
FAI 64 WEST BOUND							
2935+25.00	TO	2943+00.00	2152.78				
2943+00.00	TO	2973+00.00	8333.33				
2973+00.00	TO	2981+45.00	2347.22				
2981+45.00	TO	2983+45.00		355.56			
2983+45.00	TO	3002+00.00	5152.78				
3002+00.00	TO	3033+00.00	8611.11				
3033+00.00	TO	3063+00.00	8333.33				
3063+00.00	TO	3093+00.00	8333.33				
3093+00.00	TO	3123+00.00	8333.33				
3123+00.00	TO	3153+00.00	8333.33				
3153+00.00	TO	3183+00.00	8333.33				
3183+00.00	TO	3213+00.00	8333.33				
3213+00.00	TO	3221+17.00	2269.44				
3221+17.00	TO	3223+67.00		355.56	834.00		
3223+67.00	TO	3243+00.00	5369.44				
3243+00.00	TO	3262+98.00	5550.00				
3264+59.00	TO	3273+00.00	2336.11				
3273+00.00	TO	3282+91.00	2752.78				
3284+97.00	TO	3288+00.00	841.67				
3288+00.00	TO	3303+00.00	4166.67				
3303+00.00	TO	3333+00.00	8333.33				
3333+00.00	TO	3355+51.00	6252.78				
RAMPS							
100+88.00	TO	109+15.00					
109+15.00	TO	120+79.00			1435.00		
200+00.00	TO	206+95.00			1011.00		27
206+95.00	TO	213+02.00					27
300+30.00	TO	305+75.00			800.00		27
305+75.00	TO	316+25.00					
400+60.00	TO	407+93.00					27
407+93.00	TO	419+76.00			1553.00		
TOTAL			229020	1423	834	4799	108

# EASTBOUND RESURFACING SCHEDULE

LOCATION			BIT. MAT'L PRIME COAT	AGGREGATE PRIME COAT	HMA SHOULDER 2 1/4"	SHOULDER RUMBLE STRIP	AGGREGATE WEDGE SHLDR TYPE B	HMA BINDER COURSE 2 1/4"	LEVELING BINDER 1"	HMA SURFACE COURSE 2"	INCIDENTAL HMA ASPHALT SURFACING	STRIP REFLECTIVE CRACK CONTROL
STA	TO	STA	(TON)	(TON)	(TON)	(FOOT)	(TON)	(TON)	(TON)	(TON)	(TON)	(FOOT)
FAI 64 EAST BOUND												
2934+95.00	TO	2943+00.00	1.85	6.84	288.27	1610.00	128.76	295.84		262.96		
2943+00.00	TO	2973+00.00	6.89	25.50	1074.30	6000.00	479.86	1102.50		980.00		
2973+00.00	TO	2981+45.00	1.93	7.19	302.60	1690.00	135.16	310.54		276.03		
2981+45.00	TO	2983+45.00	0.11	0.86	71.62	400.00	31.99					
2983+45.00	TO	3002+00.00	4.28	15.89	679.08	3710.00	296.71	681.71		605.97		
3002+00.00	TO	3033+00.00	7.12	26.44	1119.71	6200.00	495.86	1139.25		1012.66	45	
3033+00.00	TO	3063+00.00	6.89	25.50	1074.30	6000.00	479.86	1102.50		980.00		
3063+00.00	TO	3093+00.00	6.89	25.50	1074.30	6000.00	479.86	1102.50		980.00		
3093+00.00	TO	3123+00.00	6.89	25.50	1074.30	6000.00	479.86	1102.50		980.00		
3123+00.00	TO	3153+00.00	6.89	25.50	1074.30	6000.00	479.86	1102.50		980.00		
3153+00.00	TO	3183+00.00	6.89	25.50	1074.30	6000.00	479.86	1102.50		980.00		
3183+00.00	TO	3213+00.00	6.89	25.50	1074.30	6000.00	479.86	1102.50		980.00		
3213+00.00	TO	3221+35.00	1.92	7.09	299.01	1670.00	133.56	306.86		272.77		
3221+35.00	TO	3223+35.00	0.11	0.86	71.62	400.00	31.99					
3223+55.00	TO	3243+00.00	4.49	16.66	712.77	3890.00	311.11	714.79		635.37		
3243+00.00	TO	3262+84.00	4.56	16.94	718.60	3968.00	317.35	729.12		648.10	45	
3264+46.00	TO	3273+00.00	1.96	7.25	305.82	1708.00	136.60	313.85		278.97		335
3273+00.00	TO	3282+12.00	2.09	7.75	326.59	1824.00	145.88	335.16		297.92		
3284+18.00	TO	3288+00.00	0.87	3.25	136.79	764.00	61.10	140.39		124.78		
3288+00.00	TO	3303+00.00	3.43	12.75	537.15	3000.00	239.93	551.25		490.00		1003
3303+00.00	TO	3333+00.00	6.89	25.50	1074.30	6000.00	479.86	1102.50		980.00		
3333+00.00	TO	3355+51.00	5.17	19.13	806.09	4502.00	360.06	827.24		735.33	45	
RAMPS												
300+30.00	TO	305+75.00	0.53	0.30	21.25	230.00	70.64		47.04	94.08		
305+75.00	TO	315+60.00	1.44	3.28	232.71	1970.00	127.68		103.02	206.03		
401+65.00	TO	407+93.00	0.92	2.10	148.76	1400.00	81.40		65.68	131.36		
407+93.00	TO	419+76.00	1.05	0.64	45.69	220.00	153.34		91.32	182.64		
SUB TOTAL =			98.9	360	15419	87156	7098	15166	307	14095	135	1338

PLOT DATE = 9/5/2007  
FILE NAME = c:\puro\puro\95-05-01\107b.dgn  
REFERENCE = REF 4

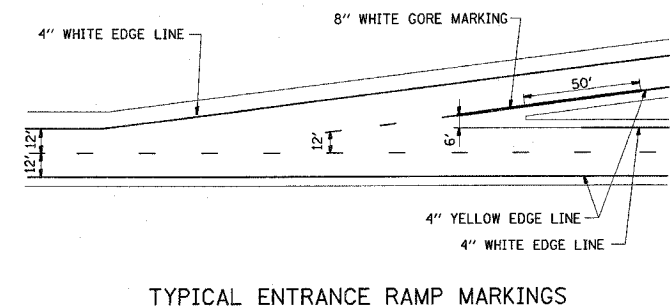
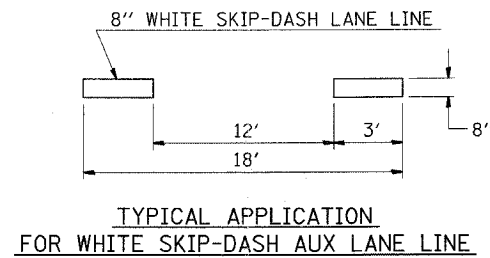
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**RESURFACING SCHEDULES**  
FAI ROUTE 64  
SECTION 95-(5,6)RS-1  
WASHINGTON COUNTY  
SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
CHECKED BY \_\_\_\_\_



# PAVEMENT MARKING SCHEDULE

LOCATION			PREFORMED PLASTIC PAVEMENT MK. TYPE B INLAID	THERMOPLASTIC PAVEMENT MARKING				POLYUREA PAVEMENT MARKING TYPE 1			RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	RAISED REFLECTIVE PAVEMENT MARKER	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	
			4"	4"		8"		4"						
			WHITE SKIP-DASH LANE LINE	SOLID WHITE EDGE LINE	SOLID YELLOW EDGE LINE	WHITE SKIP-DASH	SOLID WHITE	SKIP-DASH	WHITE	YELLOW				
STATION	TO	STATION	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	EACH	EACH
FAI 64 EASTBOUND														
2934+95	TO	2943+00	201	805	805							20	20	
2943+00	TO	2973+00	750	3000	3000							75	75	
2973+00	TO	2981+45	211	845	845							21	21	
2981+45	TO	2983+45						50	200	200		5	5	
2983+45	TO	3002+00	464	1855	1855							46	46	
3002+00	TO	3033+00	775	3100	3100							78	78	
3033+00	TO	3063+00	750	3000	3000							75	75	
3063+00	TO	3093+00	750	3000	3000							75	75	
3093+00	TO	3123+00	750	3000	3000							75	75	
3123+00	TO	3153+00	750	3000	3000							75	75	
3153+00	TO	3183+00	750	3000	3000							75	75	
3183+00	TO	3213+00	750	3000	3000							75	75	
3213+00	TO	3221+35	209	835	835							21	21	
3221+35	TO	3223+35						50	200	200		5	5	
3223+35	TO	3243+00	491	1965	1965							49	49	
3243+00	TO	3262+84	496	1984	1984							50	50	
3262+84	TO	3264+46						41	162	162				4
3264+46	TO	3273+00	214	854	854							21	21	
3273+00	TO	3282+12	228	912	912							23	23	
3282+12	TO	3284+18						52	206	206				6
3284+18	TO	3288+18	100	400	400							10	10	
3288+00	TO	3303+00	375	1500	1500							38	38	
3303+00	TO	3333+00	750	3000	3000							75	75	
3333+00	TO	3355+51	563	2251	2251							56	56	
RAMPS														
RAMP 3				1200	1200			720				22	38	
RAMP 4				743	743	60		300						
SUB-TOTALS			10327	43249	43249	60		1020	193	768	768	1065	1081	10



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

### PAVEMENT MARKING SCHEDULES

FAI ROUTE 64  
SECTION 95-(5,6)RS-1  
WASHINGTON COUNTY

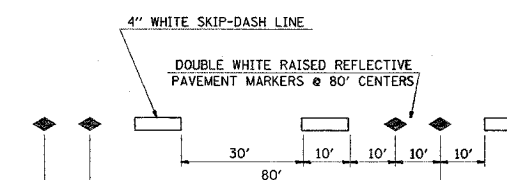
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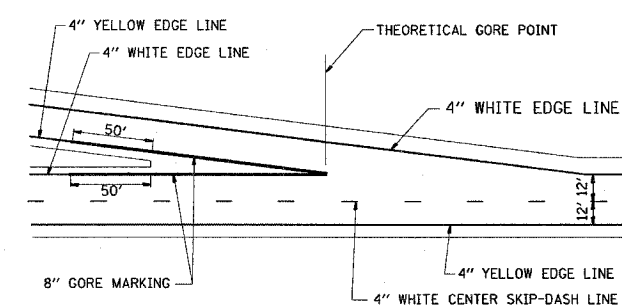
FAI SHEET	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	14
STA. _____		TO STA. _____		
CONTRACT NO.: 76A39				

## PAVEMENT MARKING SCHEDULE

LOCATION			PREFORMED PLASTIC PAVEMENT MK. TYPE B INLAID	THERMOPLASTIC PAVEMENT MARKING				POLYUREA PAVEMENT MARKING TYPE 1			RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	RAISED REFLECTIVE PAVEMENT MARKER	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	
			4"	4"		8"		4"						
			WHITE SKIP-DASH LANE LINE	SOLID WHITE EDGE LINE	SOLID YELLOW EDGE LINE	WHITE SKIP-DASH	SOLID WHITE	SKIP-DASH	WHITE	YELLOW				FOOT
STATION	TO	STATION	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	EACH	EACH	
<b>FAI 64 WESTBOUND</b>														
2934+95	TO	2943+00	201	805	805						20	20		
2943+00	TO	2973+00	750	3000	3000						75	75		
2973+00	TO	2981+45	211	845	845						21	21		
2981+45	TO	2983+45						50	200	200	5	5		
2983+45	TO	3002+00	464	1855	1855						46	46		
3002+00	TO	3033+00	775	3100	3100						78	78		
3033+00	TO	3063+00	750	3000	3000						75	75		
3063+00	TO	3093+00	750	3000	3000						75	75		
3093+00	TO	3123+00	750	3000	3000						75	75		
3123+00	TO	3153+00	750	3000	3000						75	75		
3153+00	TO	3183+00	750	3000	3000						75	75		
3183+00	TO	3213+00	750	3000	3000						75	75		
3213+00	TO	3221+25	206	825	825						21	21		
3223+55	TO	3243+00	486	1945	1945						49	49		
3243+00	TO	3262+98	500	1998	1998						50	50		
3262+98	TO	3264+59						40	161	161			4	
3264+59	TO	3273+00	210	841	841						21	21		
3273+00	TO	3282+91	248	991	991						25	25		
3282+91	TO	3284+97						52	206	206			6	
3284+97	TO	3288+00	76	303	303						8	8		
3288+00	TO	3303+00	375	1500	1500						38	38		
3303+00	TO	3333+00	750	3000	3000						75	75		
3333+00	TO	3355+51	563	2251	2251						56	56		
<b>RAMPS</b>														
RAMP 1				892	892	60	280				22	36		
RAMP 2				850	850		760							
<b>SUB-TOTALS</b>			10315	43001	43001	60	1040	142	567	567	1058	1072	10	
<b>GRAND TOTAL</b>			20642	172500		2180			3005		2123	2153	20	



**TYPICAL APPLICATION FOR WHITE SKIP-DASH LINES WITH RAISED REFLECTIVE PAVEMENT MARKERS**



**TYPICAL EXIT RAMP MARKINGS**

I:\DOT\64\8/21/2007\plan\pml1207b.dgn  
 8/21/2007  
 REF:

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SCHEDULES**  
 FAI ROUTE 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 DRAWN BY:

PLOT DATE: 8/21/2007

FAI ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
64	95-(5,6)RS-1	WASHINGTON	83	15
STA. _____		TO STA. _____		
CONTRACT NO.: 76A39				

# GUARDRAIL SCHEDULE

REMOVAL LOCATION		INSTALLATION LOCATION		SIDE	GUARDRAIL REMOVAL FT	STEEL PLATE BEAM GUARDRAIL FT	TRAFFIC BARRIER TERMINAL TYPE 1 (SPL) TANGENT EACH	TRAFFIC BARRIER TERMINAL TYPE 1 (SPL) FLARED EACH	TRAFFIC BARRIER TERMINAL TYPE 2 EACH	TRAFFIC BARRIER TERMINAL TYPE 5 EACH	TRAFFIC BARRIER TERMINAL TYPE 6 EACH	TERMINAL MARKER DIRECT APPLIED EACH	GUARDRAIL MARKERS EACH
STATION	STATION	STATION	STATION										
I-64 EB MAINLINE													
2982+28	2983+28	2982+03	2983+28	L	100.0	75.0		1	1			1	2
3224+80	3225+80	3224+55	3225+80	L	100.0	75.0		1	1			1	2
3258+72	3262+84	3258+15	3262+84	R	412.5	375.0	1				1	1	6
3261+22	3262+84	3258+53	3262+84	L	162.5	337.5	1				1	1	5
3264+44	3268+19	3264+44	3268+44	R	375.0	375.0			1	1			6
3279+33	3282+20	3278+51	3282+20	R	287.5	275.0	1				1	1	4
3279+95	3282+20	3277+89	3282+20	L	225.0	337.5	1				1	1	5
I-64 WB MAINLINE													
2982+81	2983+81	2982+81	2984+06	L	100.0	75.0		1	1			1	2
3225+33	3226+46	3225+33	3226+58	L	112.5	75.0		1	1			1	2
3256+72	3262+84	3256+47	3262+84	R	612.5	612.5			1	1			9
3264+44	3266+32	3264+44	3268+76	L	187.5	337.5	1				1	1	5
3264+44	3270+32	3264+44	3270+88	R	587.5	550.0	1				1	1	8
3284+30	3286+55	3284+30	3288+62	L	225.0	337.5	1				1	1	5
3284+30	3287+05	3284+30	3287+99	R	275.0	275.0	1				1	1	4
<b>TOTAL</b>					<b>3762.5</b>	<b>4112.5</b>	<b>8</b>	<b>4</b>	<b>6</b>	<b>2</b>	<b>8</b>	<b>12</b>	<b>65</b>

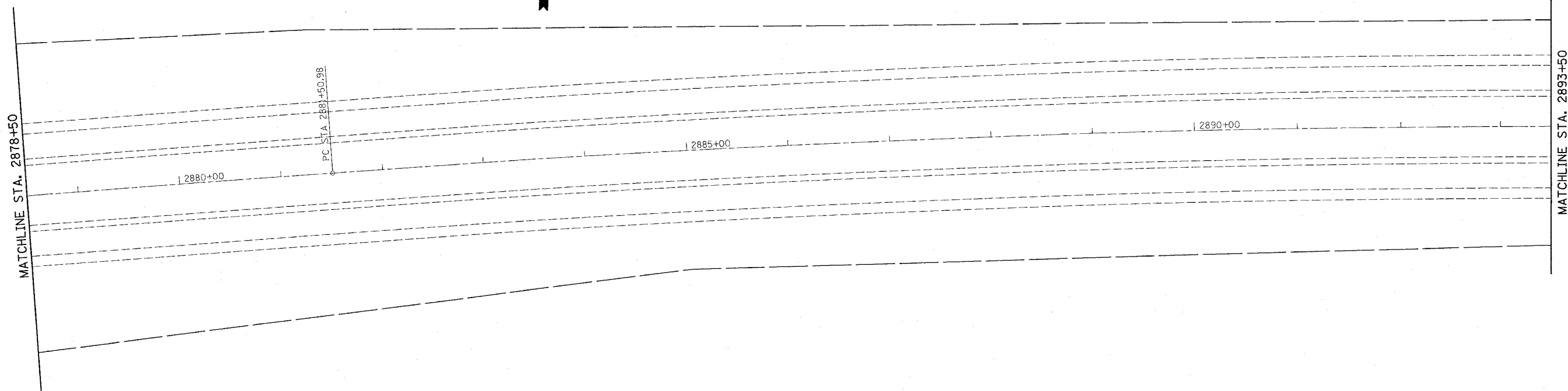
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REVISIONS	
NAME	DATE

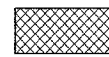
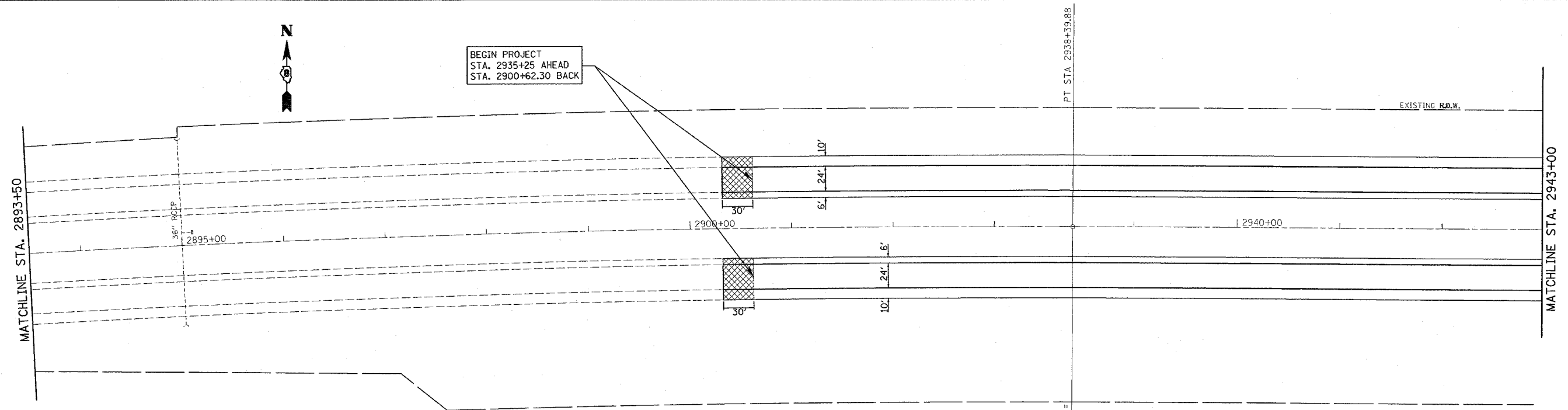
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**GUARDRAIL SCHEDULES**  
 FAI ROUTE 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 DRAWN BY:

PLOT DATE: 8/21/2007

CONTRACT NO. 76A39				
FAI RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	16
STA. 2878+50		TO STA. 2943+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



BEGIN PROJECT  
STA. 2935+25 AHEAD  
STA. 2900+62.30 BACK



- HMA SURFACE REMOVAL-BUTT JOINT

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PLOT SCALE = 90/2000 / 1/4"  
REFERENCE = WHER#

EQUATION:  
STA 2903+77.20 BK =  
STA 2938+39.88 AH

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
FAI RTE. 64  
SECTION 95-(5,6)RS-1  
WASHINGTON COUNTY

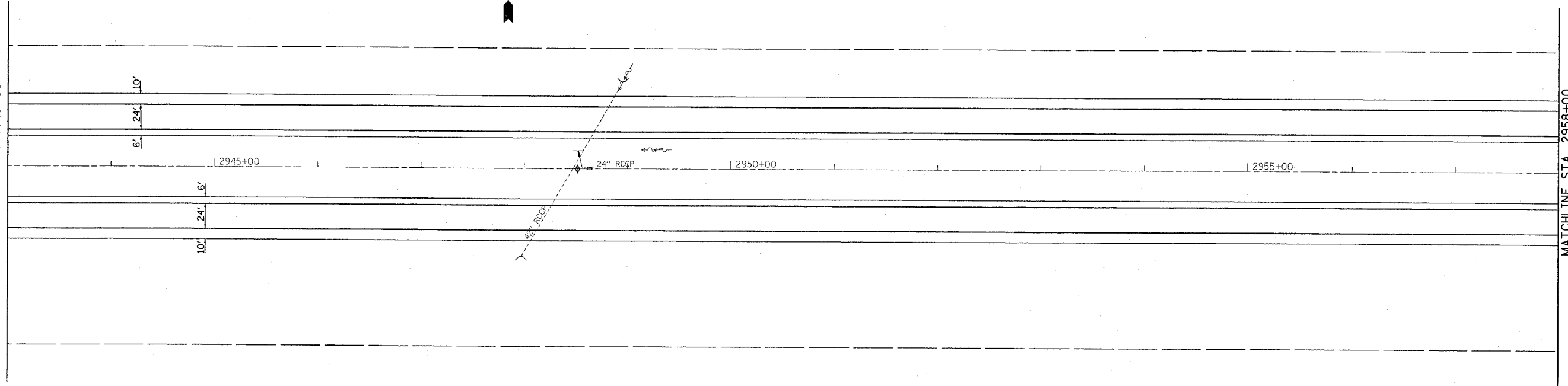
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HORIZ.  
DATE

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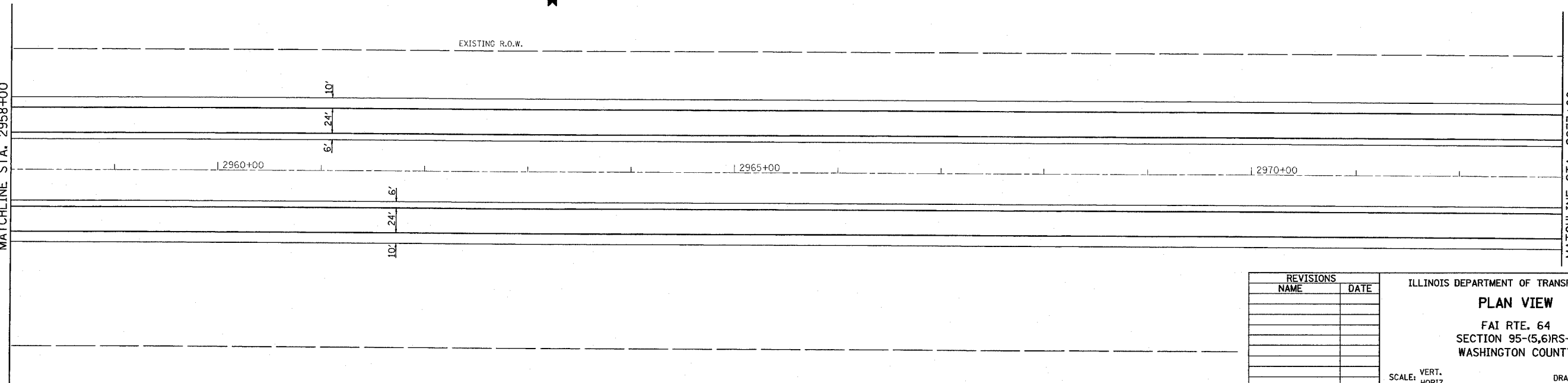
CONTRACT NO. 76A39				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	17
STA. 2943+00		TO STA. 2973+00		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

MATCHLINE STA. 2943+00



MATCHLINE STA. 2958+00

MATCHLINE STA. 2958+00





MATCHLINE STA. 2973+00

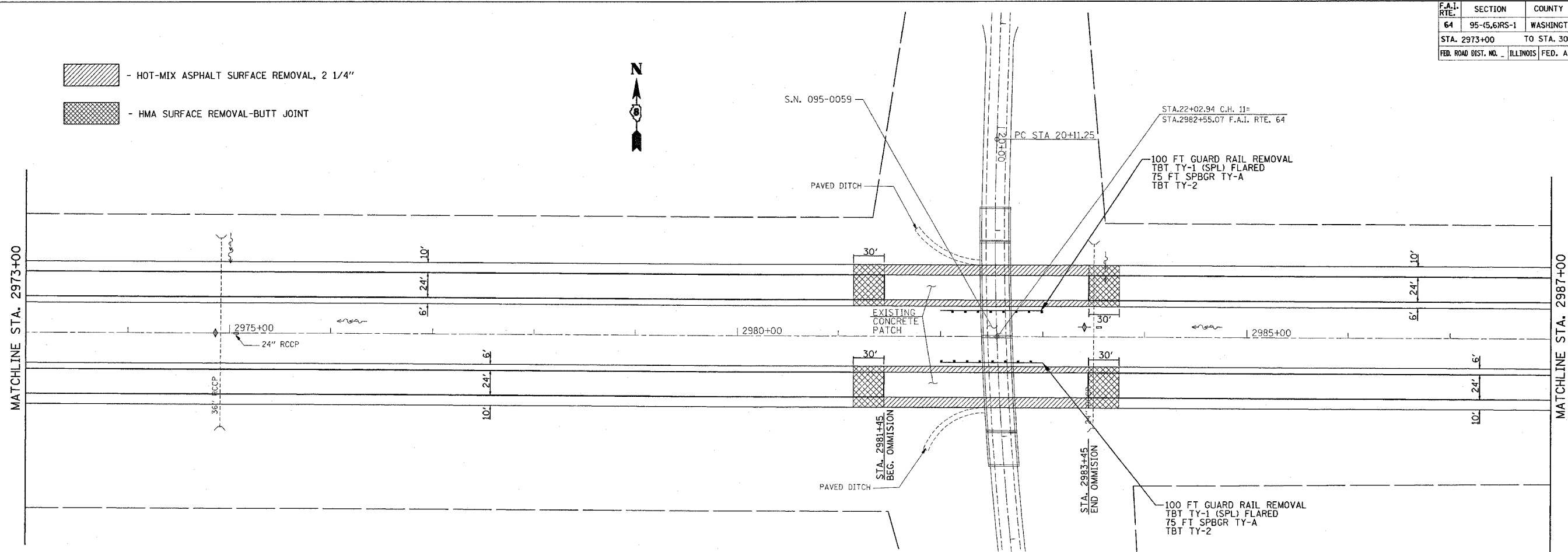
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REVISIONS	
NAME	DATE

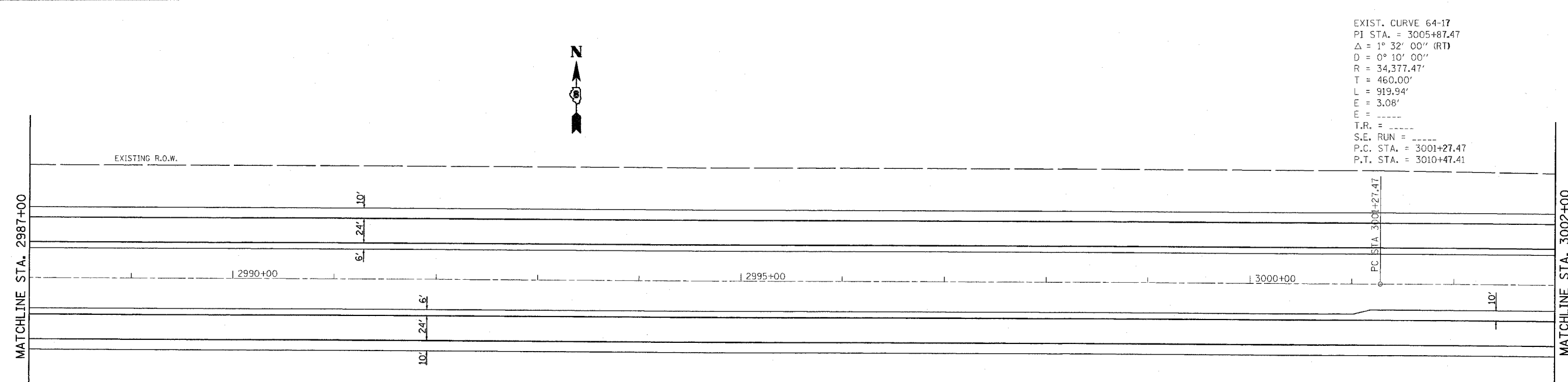
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

CONTRACT NO. 76A39				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	18
STA. 2973+00		TO STA. 3002+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

-  - HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
-  - HMA SURFACE REMOVAL-BUTT JOINT



EXIST. CURVE 64-17  
 PI STA. = 3005+87.47  
 $\Delta = 1^\circ 32' 00''$  (RT)  
 $D = 0^\circ 10' 00''$   
 $R = 34,377.47'$   
 $T = 460.00'$   
 $L = 919.94'$   
 $E = 3.08'$   
 $E = \dots$   
 $T.R. = \dots$   
 $S.E. RUN = \dots$   
 $P.C. STA. = 3001+27.47$   
 $P.T. STA. = 3010+47.41$



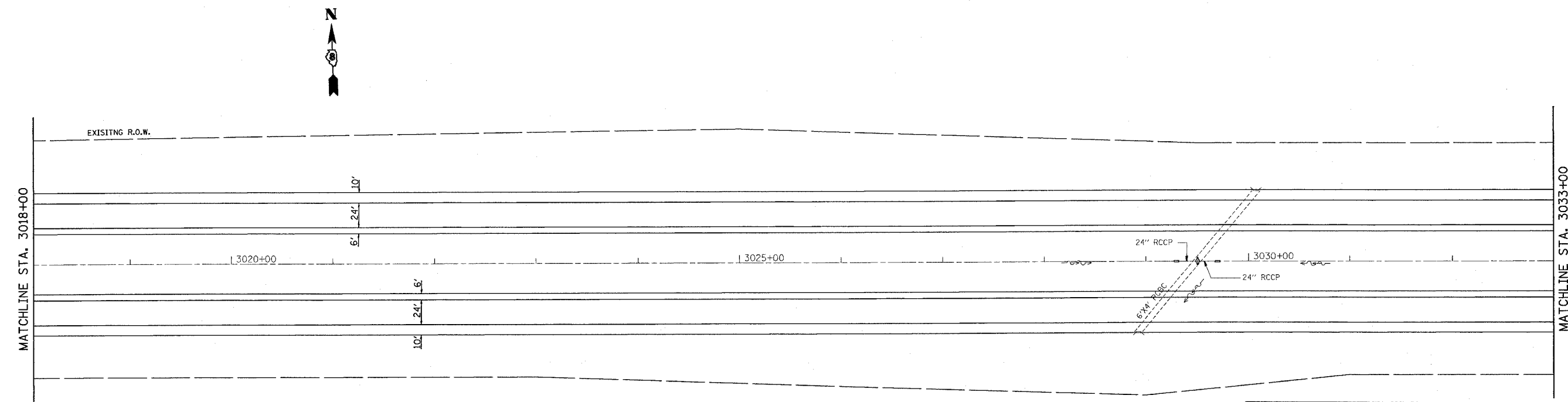
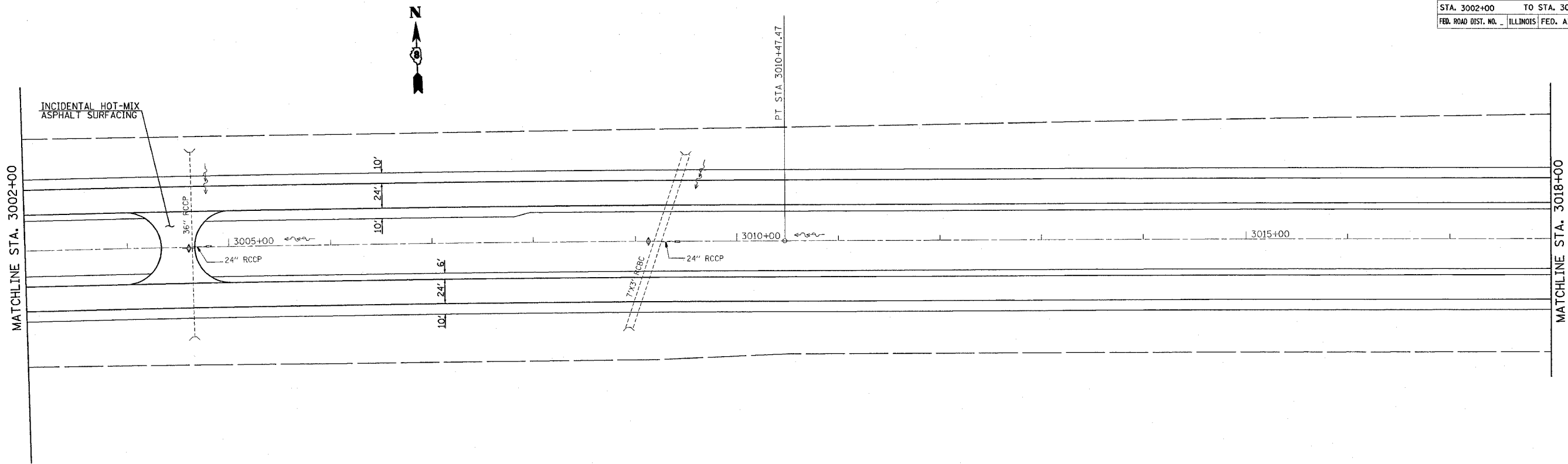
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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

CONTRACT NO. 76A39			
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83 19
STA. 3002+00		TO STA. 3033+00	
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	

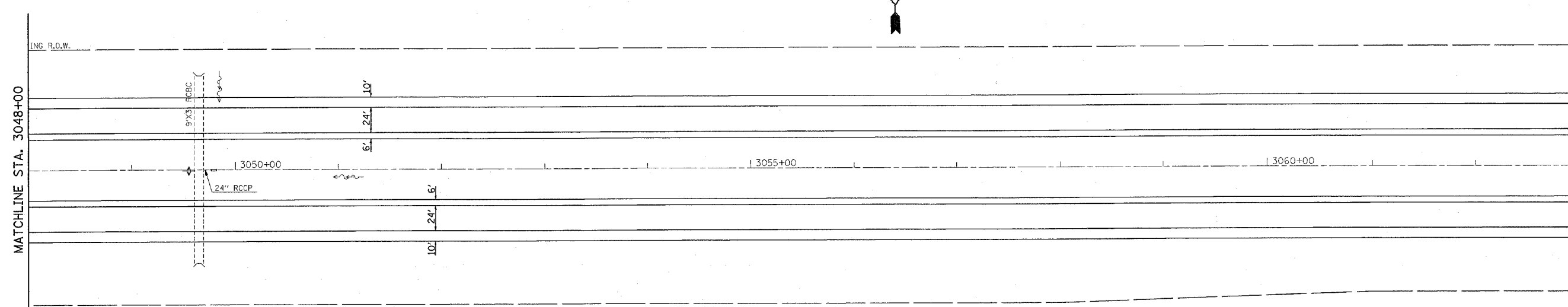
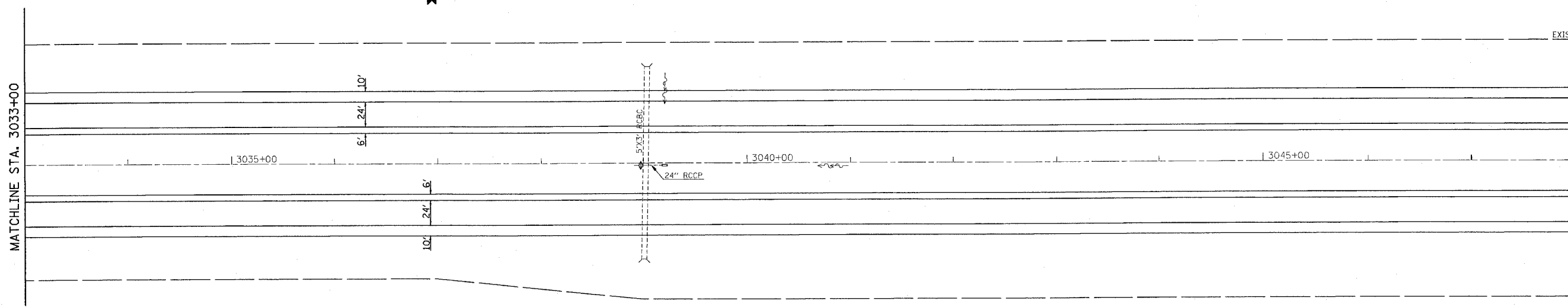


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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT.      DRAWN BY  
           HORIZ.      CHECKED BY  
 DATE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	20
STA. 3033+00		TO STA. 3063+00		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



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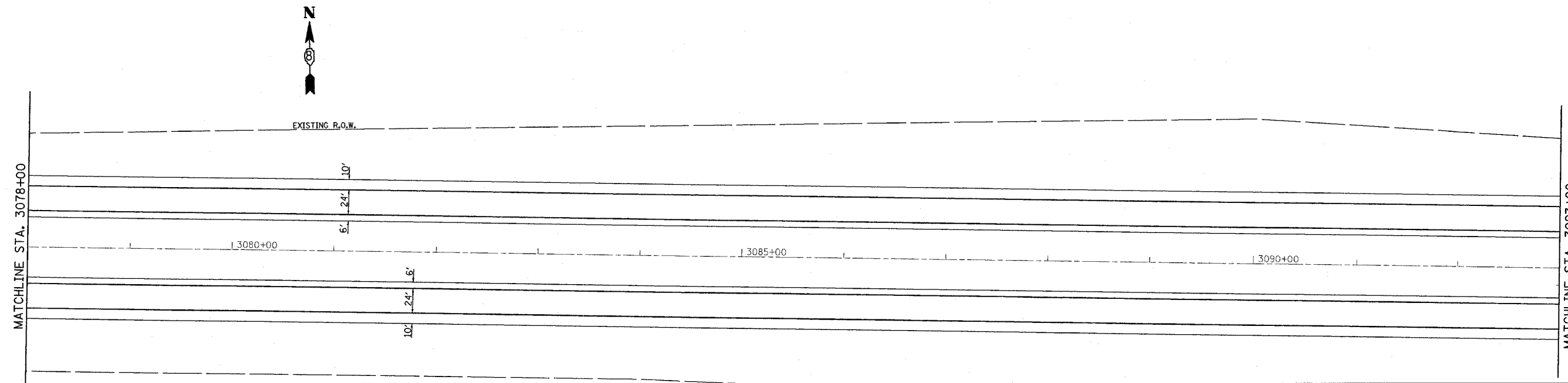
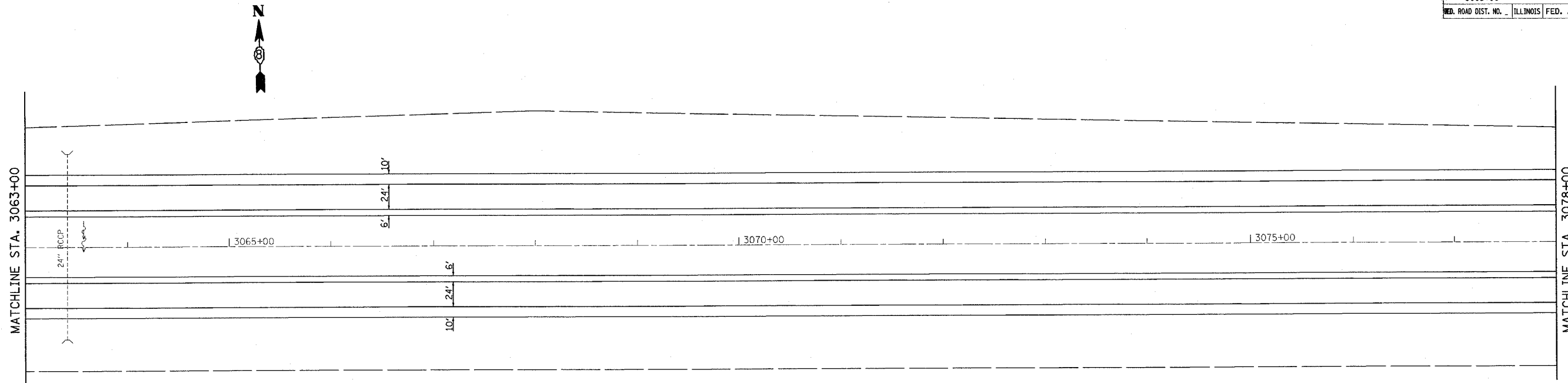
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

FAI-RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	21
STA. 3063+00		TO STA. 3093+00		
RD. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



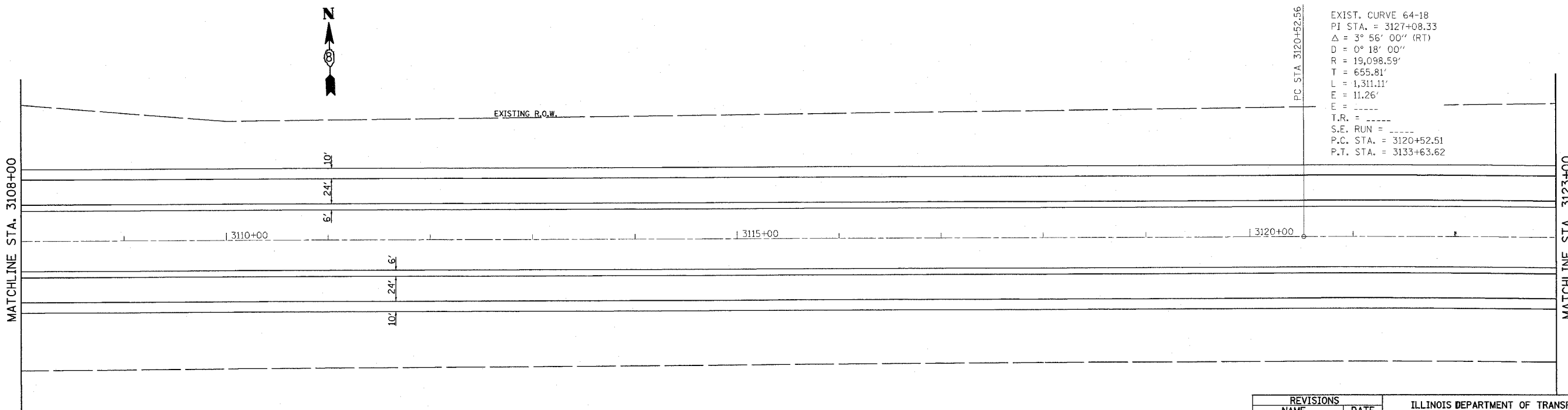
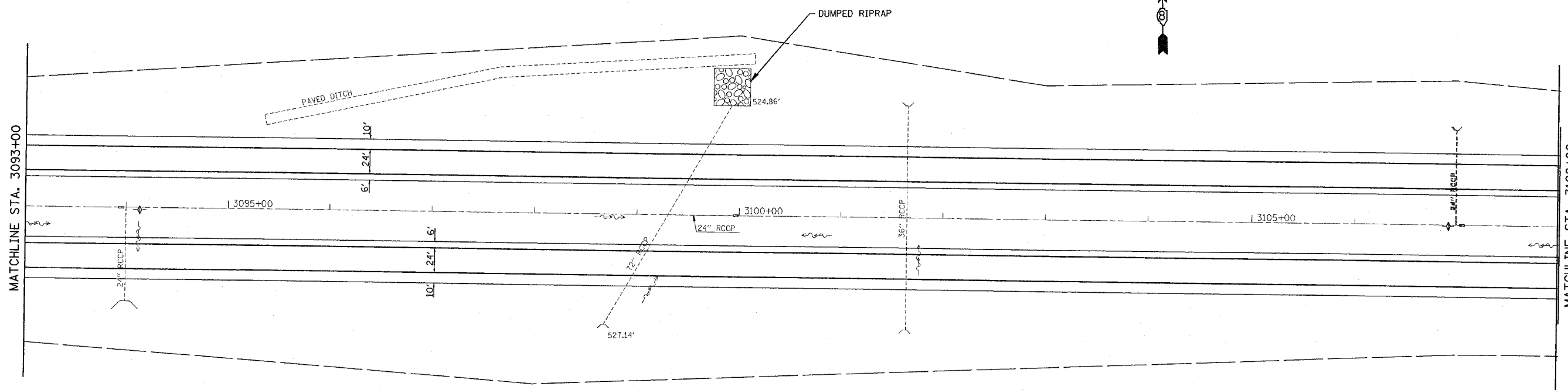
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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

SCALE: VERT.      DRAWN BY  
 HORIZ.              CHECKED BY  
 DATE

FAI RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	22
STA. 3093+00 TO STA. 3123+00				
ILLINOIS FED. AID PROJECT				



PC STA 3120+52.56  
 EXIST. CURVE 64-18  
 PI STA. = 3127+08.33  
 $\Delta = 3^\circ 56' 00''$  (RT)  
 D =  $0^\circ 18' 00''$   
 R = 19,098.59'  
 T = 655.81'  
 L = 1,311.11'  
 E = 11.26'  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 3120+52.51  
 P.T. STA. = 3133+63.62

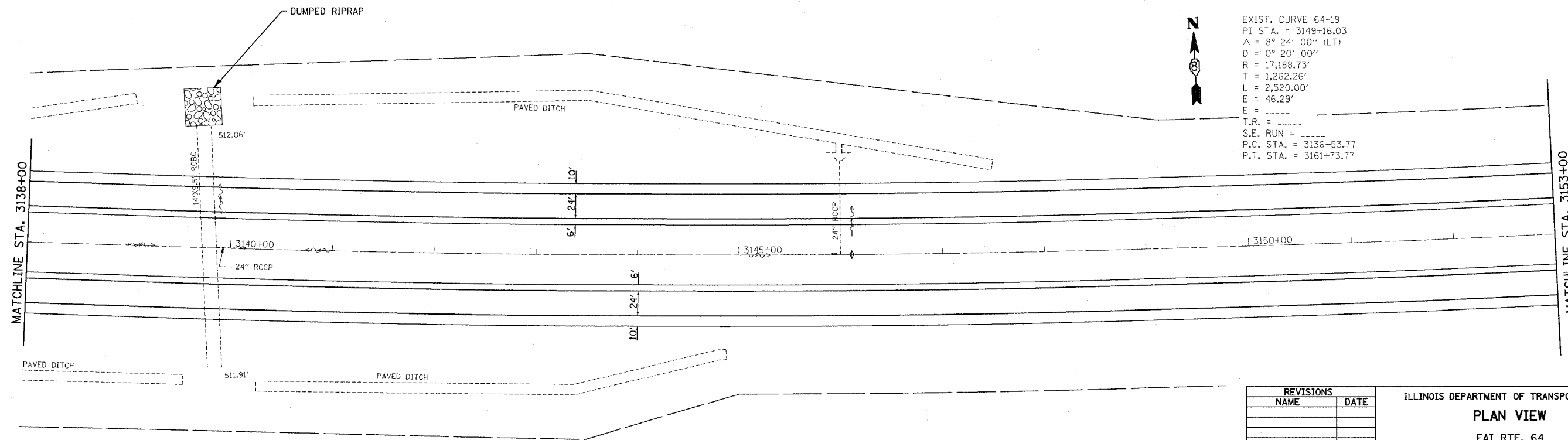
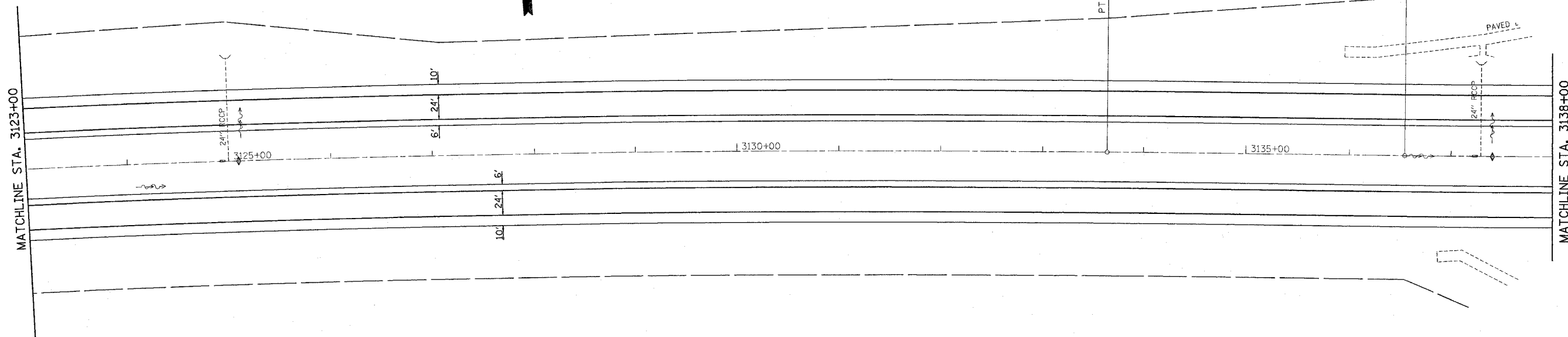
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 8/23/2007  
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 REFERENCE = SHEET#

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	23
STA. 3123+00		TO STA. 3153+00		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT				



EXIST. CURVE 64-19  
 PI STA. = 3149+16.03  
 $\Delta = 8^\circ 24' 00''$  (LT)  
 $D = 0^\circ 20' 00''$   
 $R = 17,188.73'$   
 $T = 1,262.26'$   
 $L = 2,520.00'$   
 $E = 46.29'$   
 $F =$   
 $T.R. =$   
 $S.E. RUN =$   
 $P.C. STA. = 3136+53.77$   
 $P.T. STA. = 3161+73.77$

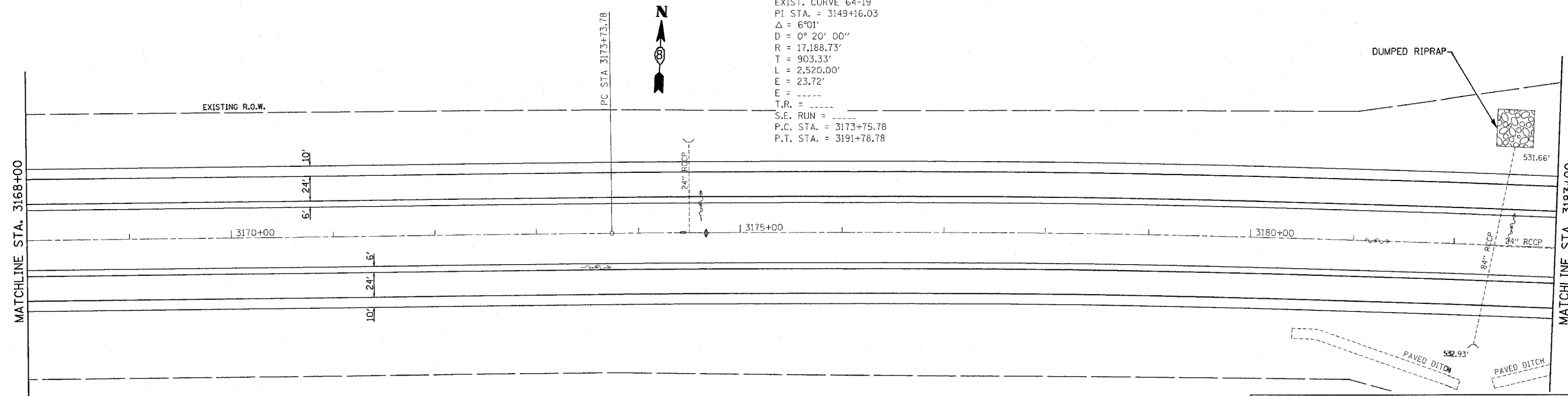
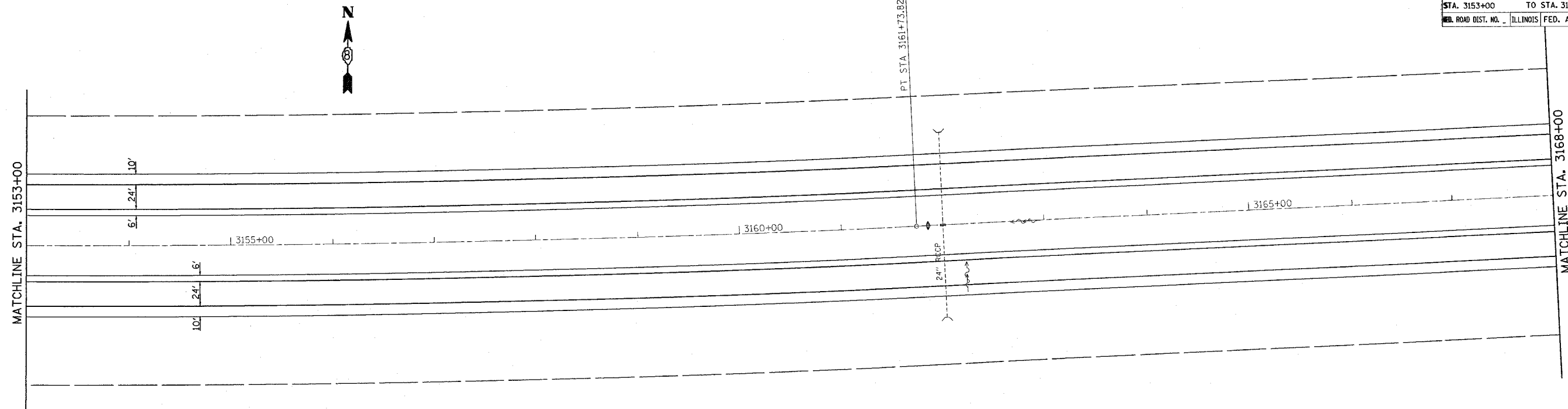
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

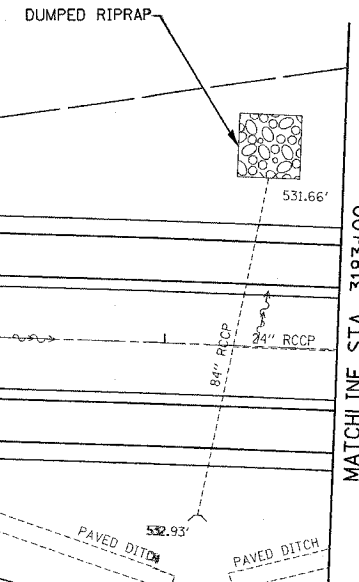
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 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 8/23/2007  
 FILE NAME = c:\projects\95-01287\plan\95-01287b.dgn  
 PLOT SCALE = 1"=40'  
 REFERENCE = #REF#

CONTRACT NO. 76A39				
FAI RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	24
STA. 3153+00		TO STA. 3183+00		
ILL. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



EXIST. CURVE 64-19  
 PI STA. = 3149+16.03  
 $\Delta = 6^{\circ}01'$   
 $D = 0^{\circ}20'00''$   
 $R = 17,188.73'$   
 $T = 903.33'$   
 $L = 2,520.00'$   
 $E = 23.72'$   
 $F = \text{-----}$   
 $T.R. = \text{-----}$   
 $S.E. \text{ RUN} = \text{-----}$   
 $P.C. \text{ STA.} = 3173+75.78$   
 $P.T. \text{ STA.} = 3191+78.78$



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

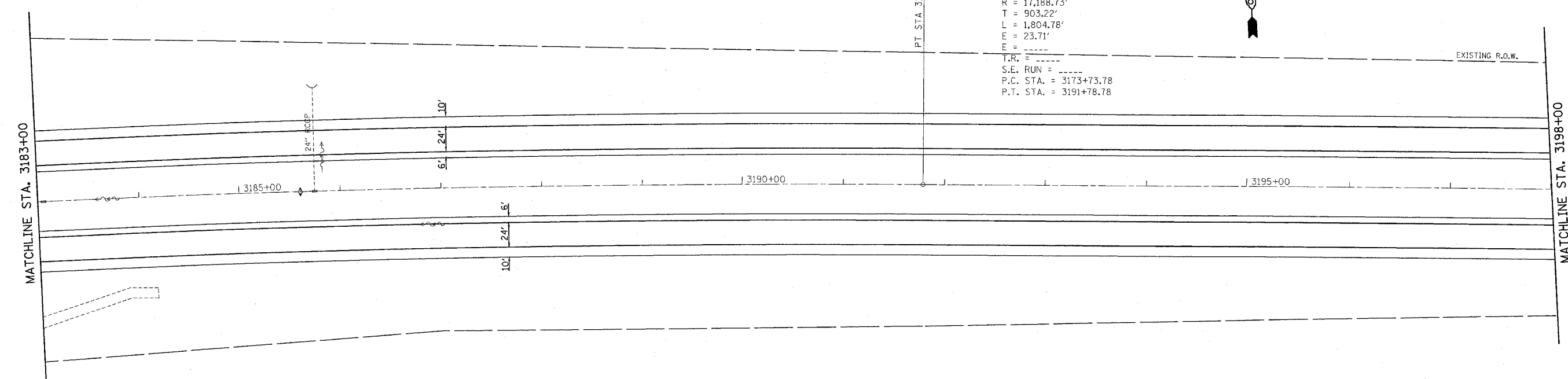
SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 8/23/2007  
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 PREFERENCE = MREF

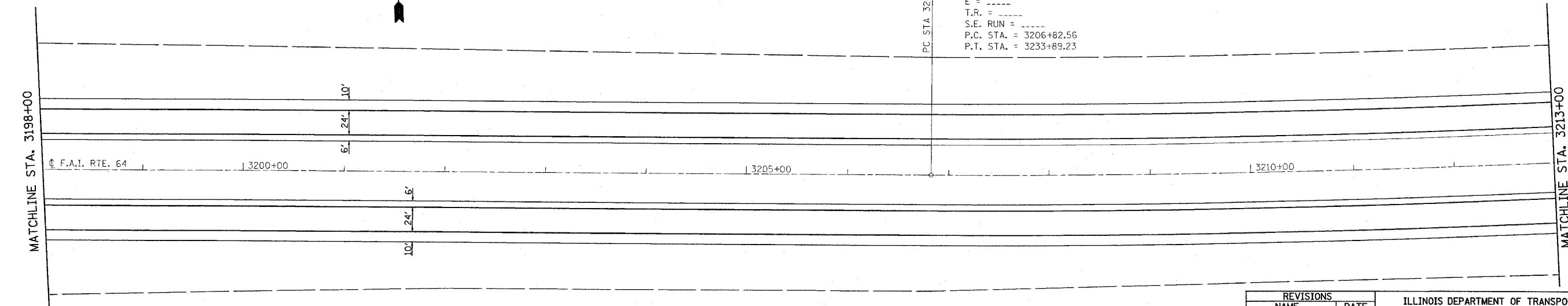


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	25
STA. 3183+00		TO STA. 3213+00		
FED. ROAD DIST. NO. ILLINOIS		FED. AID PROJECT		

EXIST. CURVE 64-20  
 PI STA. = 3182+77.07  
 $\Delta = 6^\circ 00' 57''$  (RT)  
 $D = 0^\circ 20' 00''$   
 $R = 17,188.73'$   
 $T = 903.22'$   
 $L = 1,804.78'$   
 $E = 23.71'$   
 $E = \text{-----}$   
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 3173+73.78  
 P.T. STA. = 3191+78.78



EXIST. CURVE 64-21  
 PI STA. = 3220+43.22  
 $\Delta = 13^\circ 31' 57''$  (LT)  
 $D = 0^\circ 30' 00''$   
 $R = 11,459.15'$   
 $T = 1,359.59'$   
 $L = 2,706.52'$   
 $E = 80.37'$   
 $E = \text{-----}$   
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 3206+82.56  
 P.T. STA. = 3233+89.23



REVISIONS	
NAME	DATE

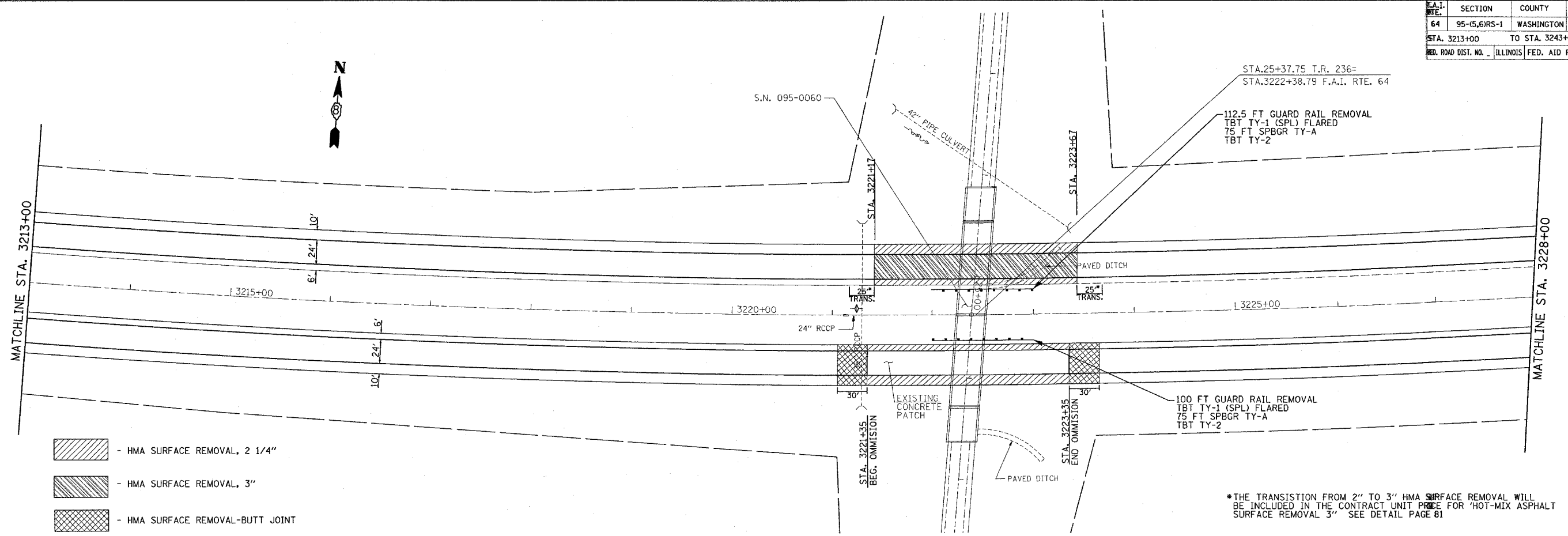
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_

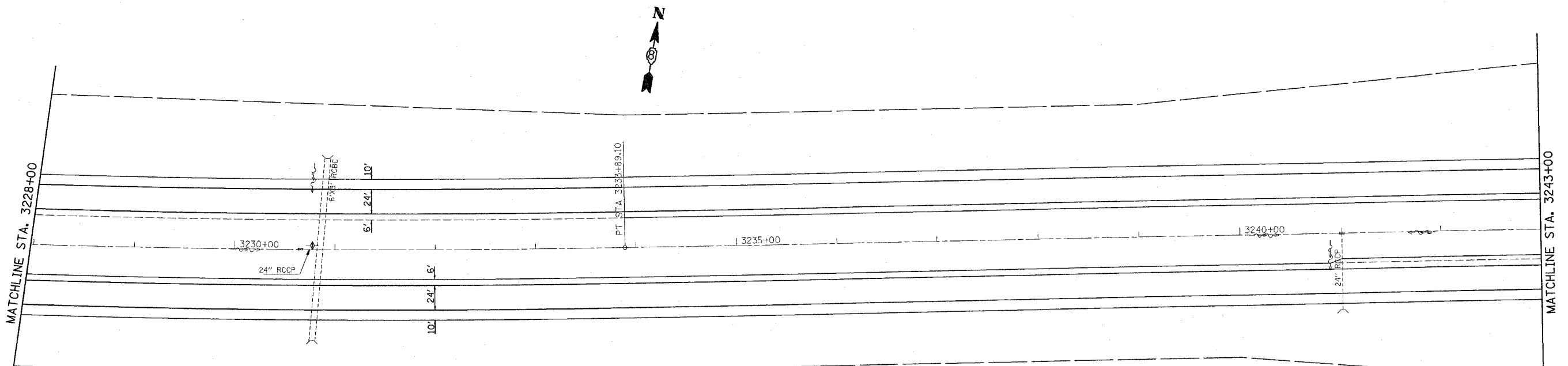
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 CHECKED BY \_\_\_\_\_

PLOT DATE = 8/21/2007  
 FILE NAME = c:\pms\projects\1081207\plan\plan.dwg  
 PLOT SCALE = 1" = 40'  
 REFERENCE = BREF

F.A.I. SHEETS	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	26
STA. 3213+00		TO STA. 3243+00		
ILLINOIS		FED. AID PROJECT		



\*THE TRANSITION FROM 2" TO 3" HMA SURFACE REMOVAL WILL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR HOT-MIX ASPHALT SURFACE REMOVAL 3" SEE DETAIL PAGE 81

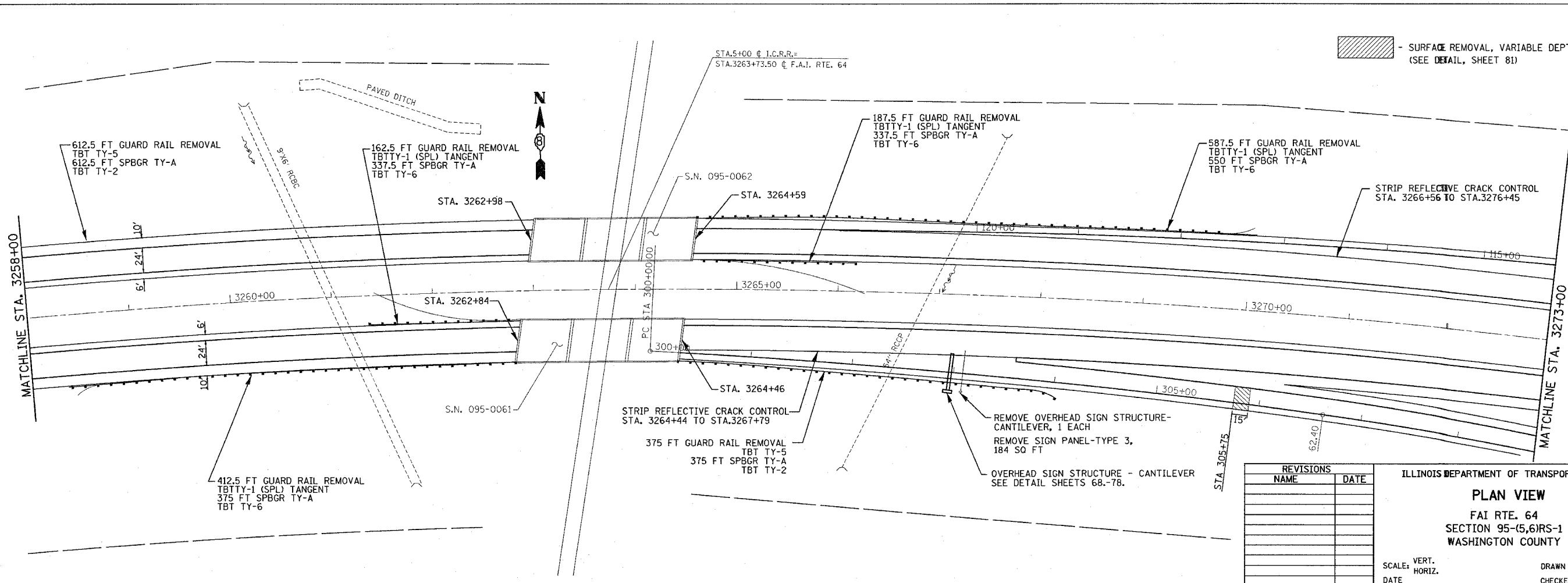
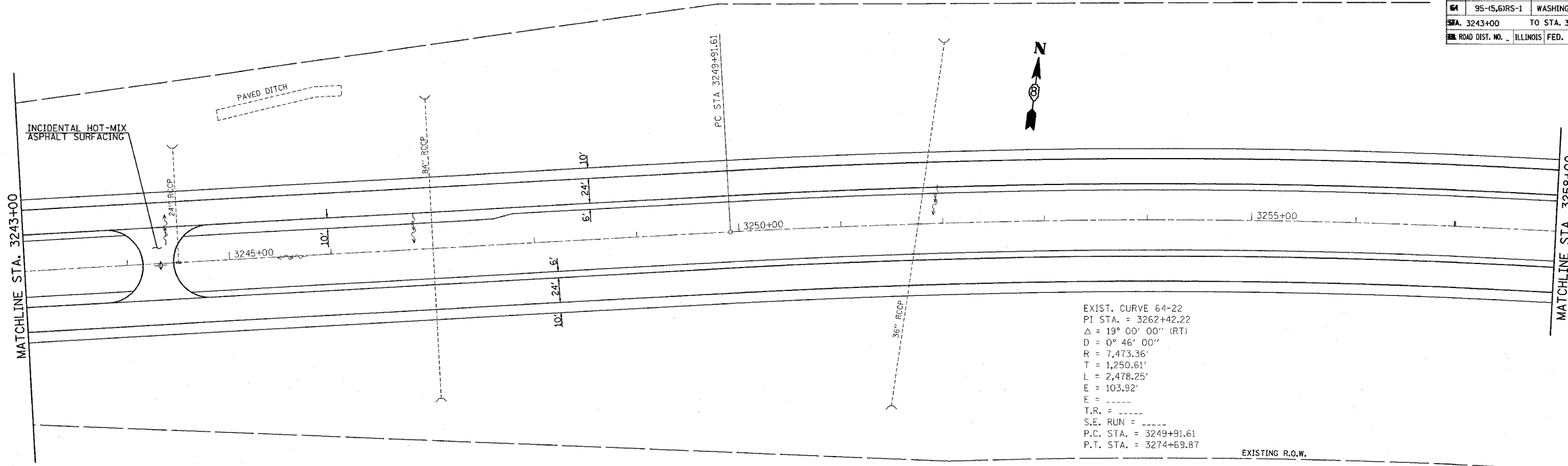


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 8/21/2007  
 FILE NAME = c:\nrc\p\msta\wd1207\plan\p\m01207b.dgn  
 PLOT FILE = m01207b.plt  
 REFERENCE = NONE

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
95-(5,6)RS-1	WASHINGTON	83	27
STA. 3243+00 TO STA. 3273+00			
ROAD DIST. NO. ILLINOIS FED. AID PROJECT			

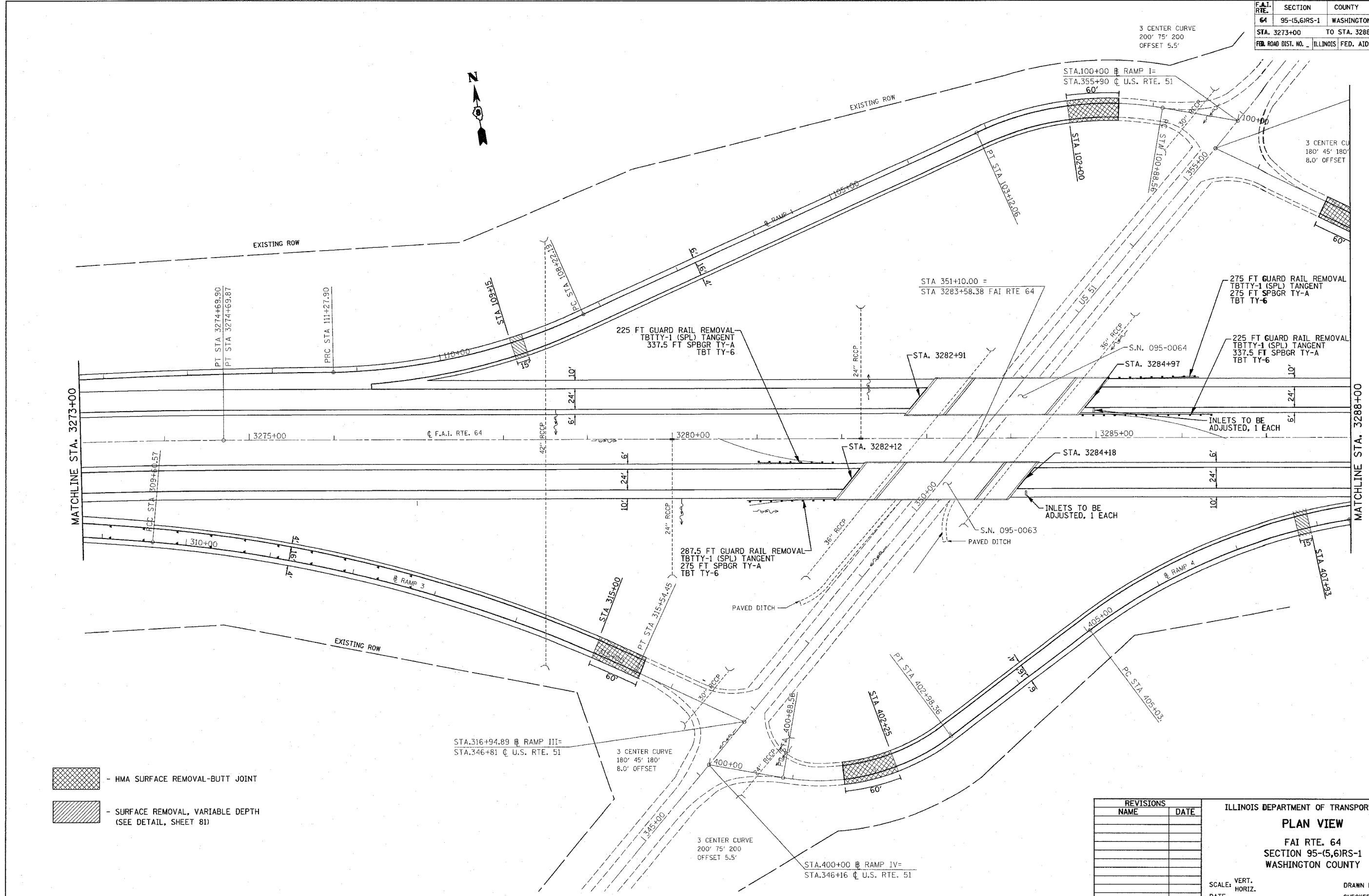



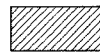
PLOT DATE = 8/21/2007  
 FILE NAME = c:\projects\76a39\plan\plan01207b.dgn  
 PLOT SCALE = 50,000 / IN.  
 REFERENCE = #REF#

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	28
STA. 3273+00		TO STA. 3288+00		
FBI. ROAD DIST. NO. ILLINOIS		FED. AID PROJECT		



-  - HMA SURFACE REMOVAL-BUTT JOINT
-  - SURFACE REMOVAL, VARIABLE DEPTH (SEE DETAIL, SHEET 81)

REVISIONS	
NAME	DATE

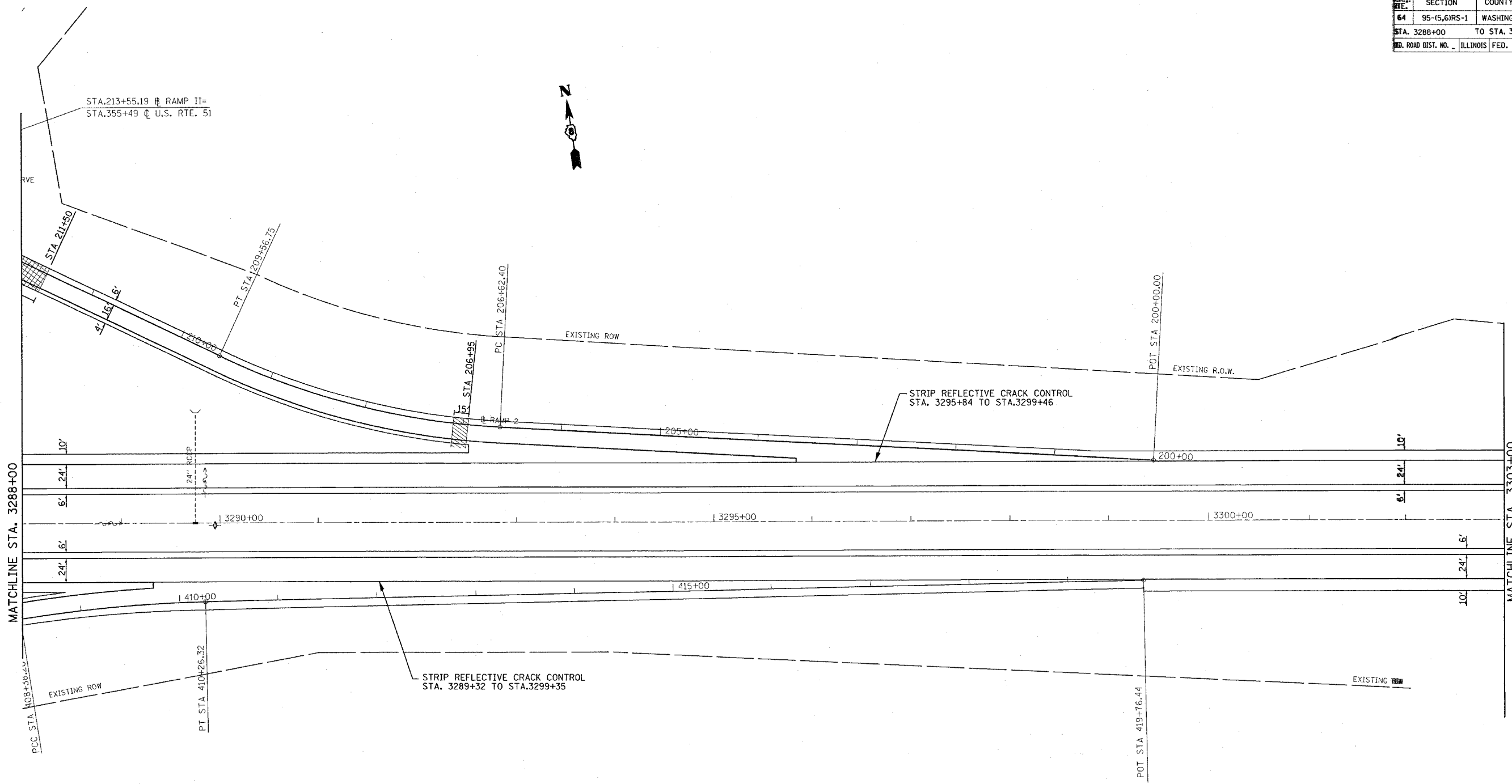
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 8/21/2007  
 PLOT SCALE = 1" = 50'  
 PLOT SOURCE = REFERENCE  
 REFERENCE = #REF#

EA. I. SHE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	29
STA. 3288+00		TO STA. 3303+00		
RD. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



- HMA SURFACE REMOVAL-BUTT JOINT
- SURFACE REMOVAL, VARIABLE DEPTH (SEE DETAIL, SHEET 81)

REVISIONS	
NAME	DATE

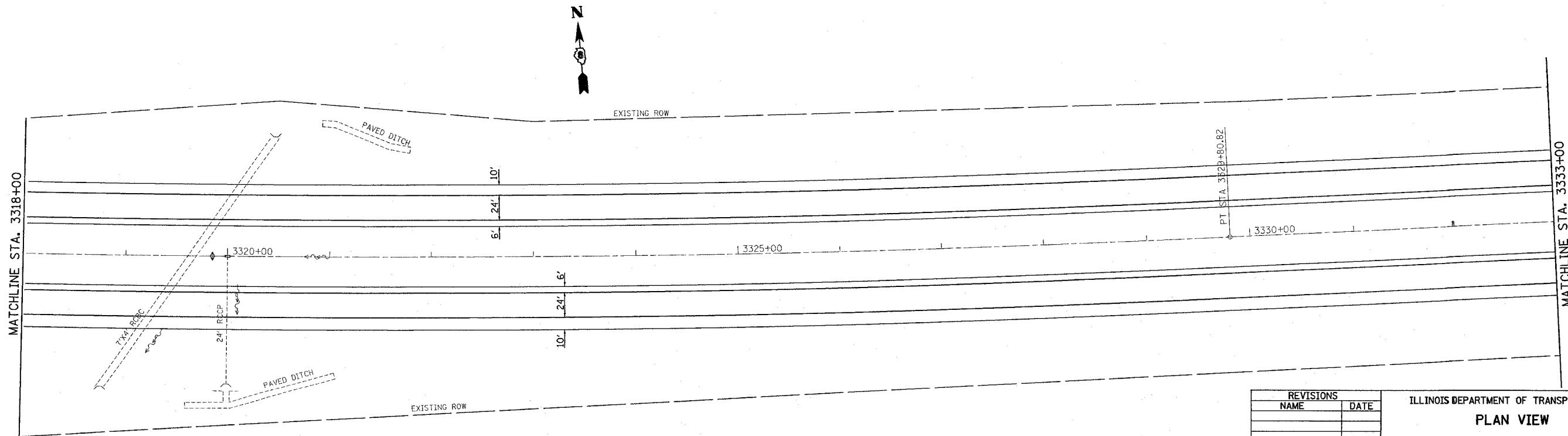
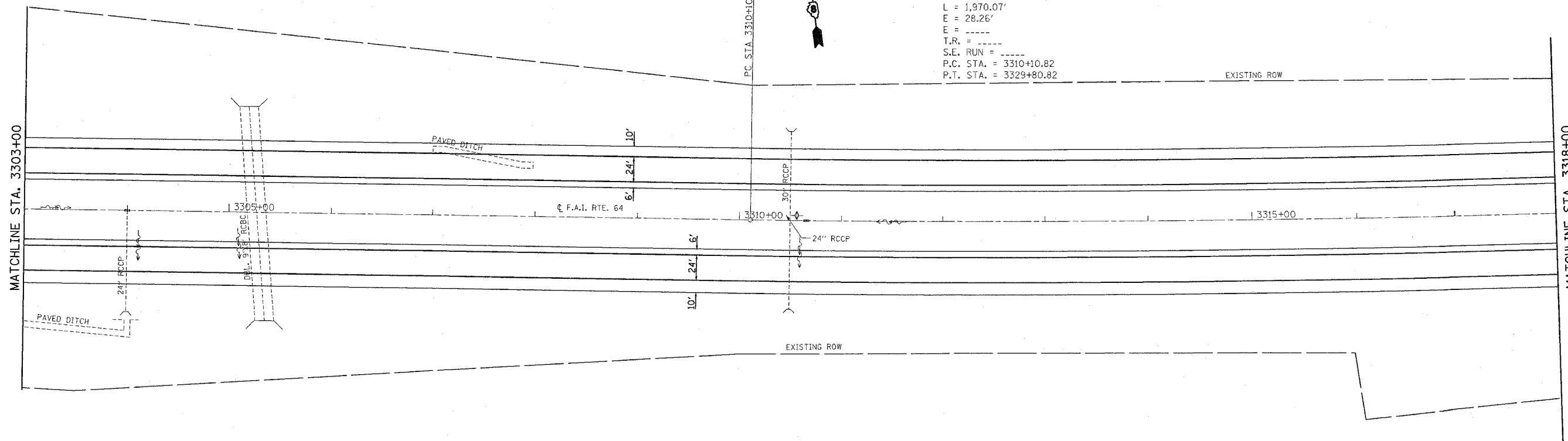
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 8/21/2007  
 PLOT NAME = C:\p1\proj\76a39\plan\p1\p1297b.dgn  
 PLOT SCALE = 80.000 / IN.  
 REFERENCE = #REF#

E.A.I. SHEET NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	30
STA. 3303+00		TO STA. 3333+00		
ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

EXIST. CURVE 64-23  
 PI STA. = 3319+96.90  
 $\Delta = 6^\circ 34' 01''$  (LT)  
 $D = 0^\circ 20' 00''$   
 $R = 17,188.73'$   
 $T = 986.11'$   
 $L = 1,970.07'$   
 $E = 28.26'$   
 $F = \text{-----}$   
 $T.R. = \text{-----}$   
 $S.E. \text{ RUN} = \text{-----}$   
 P.C. STA. = 3310+10.82  
 P.T. STA. = 3329+80.82

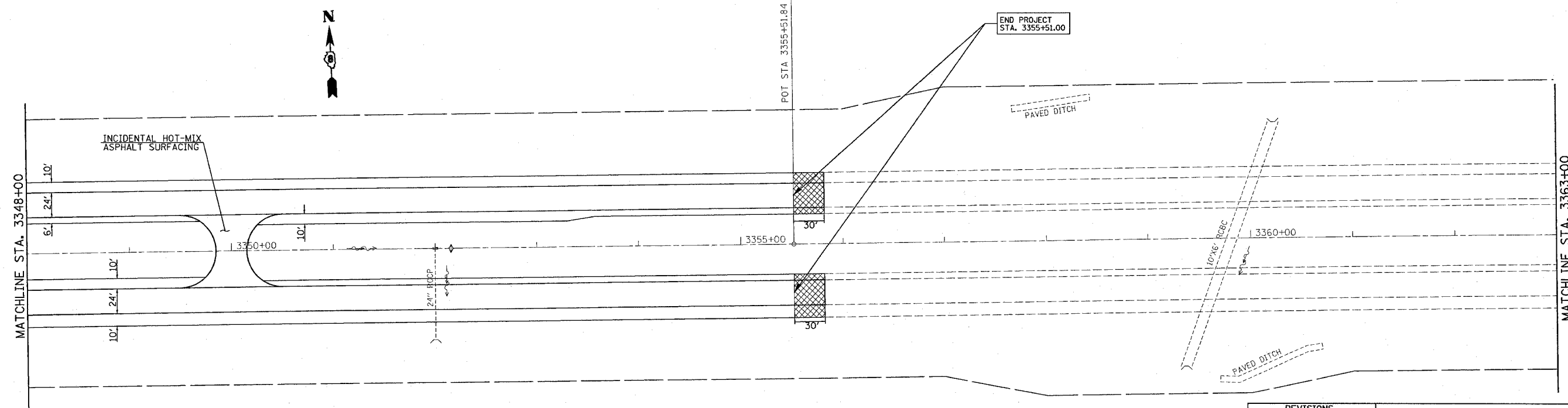
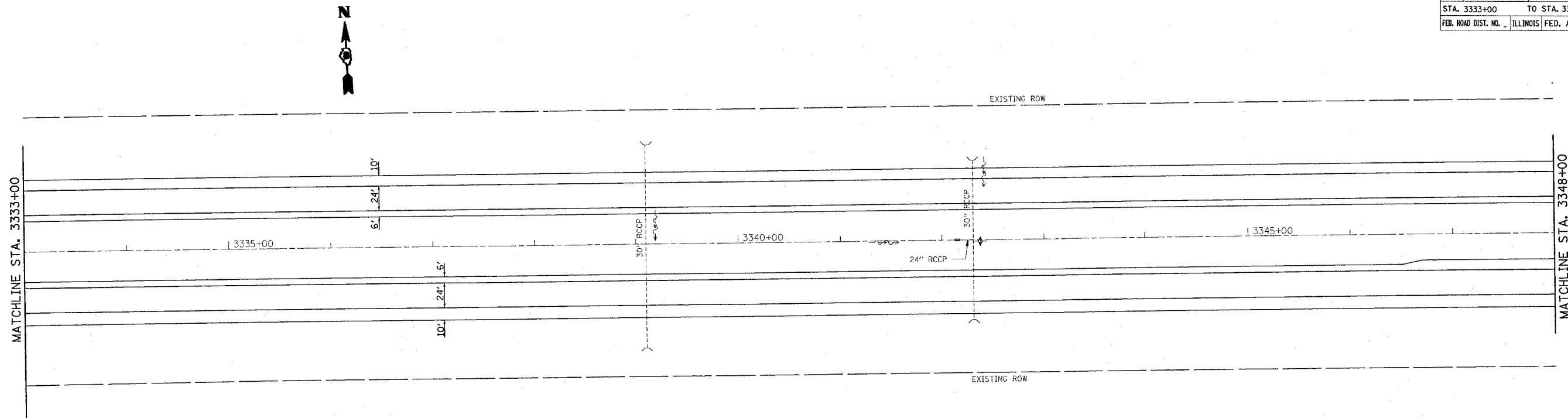


PLOT DATE = 8/21/2007  
 FILE NAME = c:\projects\95-5,6\rs-1\plan\p1.dwg  
 PLOT SCALE = 1/8" = 1' IN.  
 REFERENCE = #REF#

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	31
STA. 3333+00		TO STA. 3363+00		
FED. ROAD DIST. NO. .		ILLINOIS FED. AID PROJECT		



 - HMA SURFACE REMOVAL-BUTT JOINT

REVISIONS	
NAME	DATE

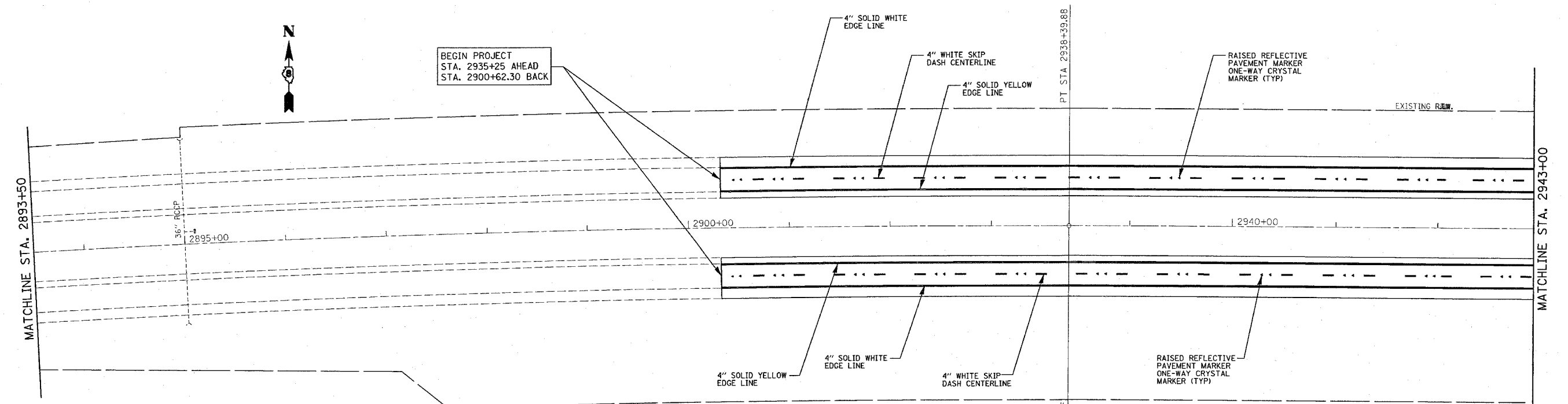
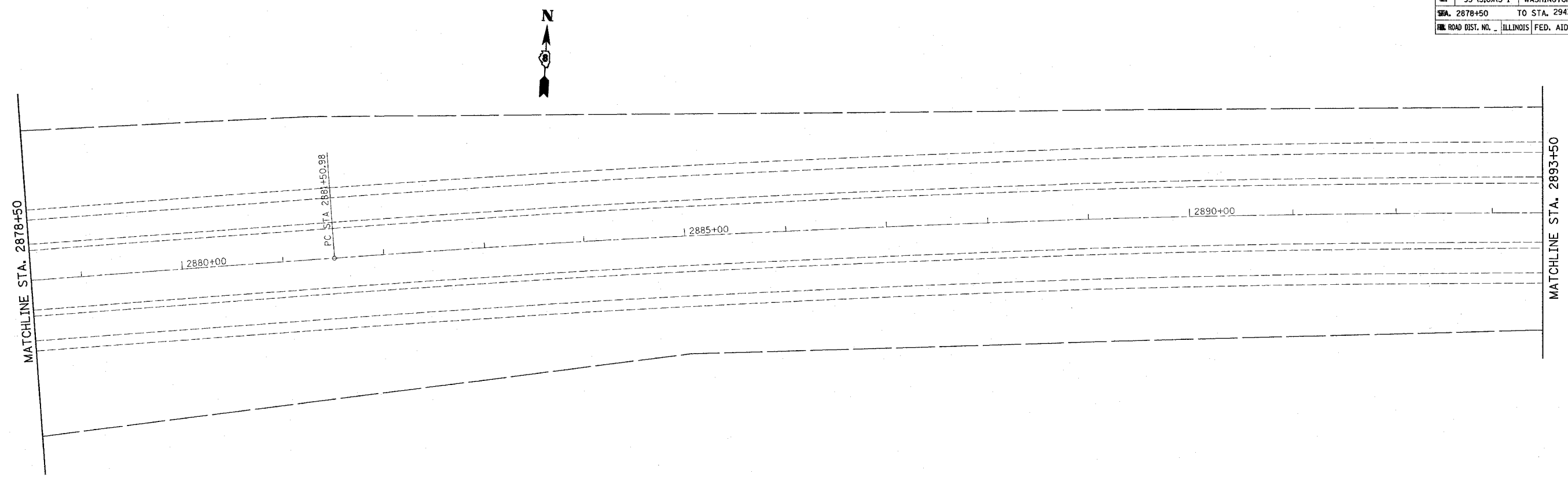
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN VIEW**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 8/21/2007  
 FILE NAME = c:\p\proj\sect\95\1287\plan\plan\01287b.dgn  
 PLOT SCALE = 5/8" = 1' / IN.  
 REFERENCE = #REF#

FAI DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
95	95-(5,6)RS-1	WASHINGTON	83	32
STA. 2878+50		TO STA. 2943+00		
ILLINOIS	FED. AID PROJECT			



PLOT DATE = 8/21/2007  
 FILE NAME = c:\projects\76a39\plan\pvm01207b.dgn  
 PLOT SCALE = 80.0000' / 1" IN.  
 REFERENCE = #REF#

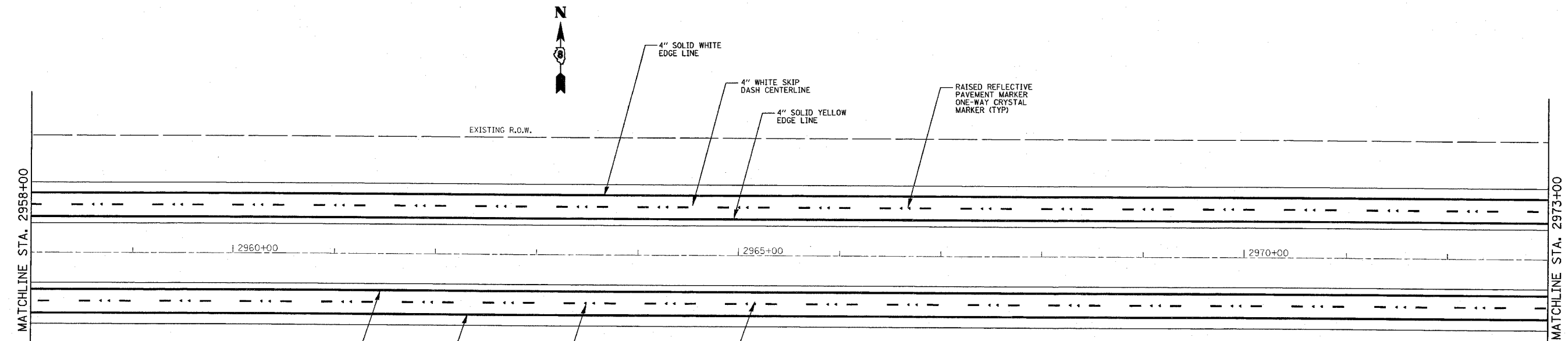
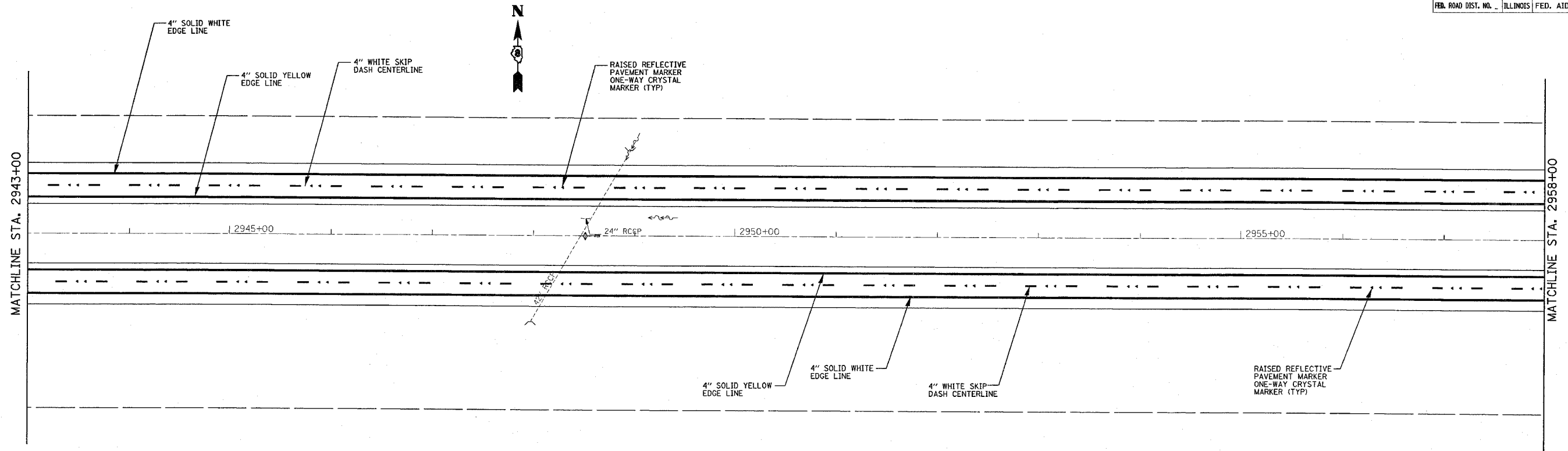
EQUATION:  
 STA 2903+77.20 BK =  
 STA 2938+39.88 AH

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SHEET**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_



FAI- RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	33
STA. 2943+00		TO STA. 2973+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



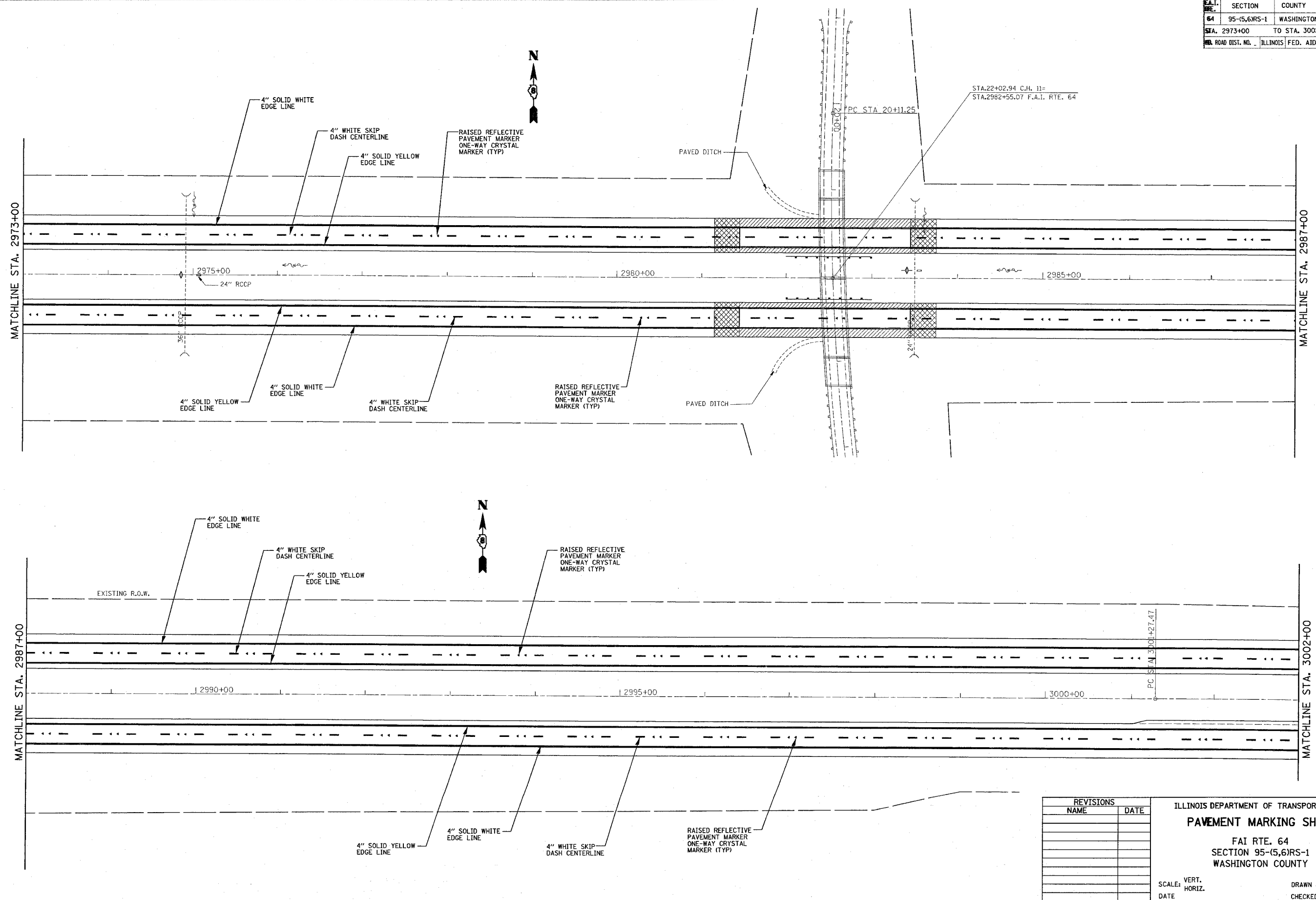
PLOT DATE = 8/21/2007  
 PLOT SCALE = 50' = 1" (VERTICAL)  
 PLOT SCALE = 1" = 100' (HORIZONTAL)  
 REFERENCE = #REF#

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SHEET**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

SCALE: VERT. 50' = 1"  
 HORIZ. 1" = 100'  
 DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

F.A.I. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	34
STA. 2973+00		TO STA. 3002+00		
RD. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

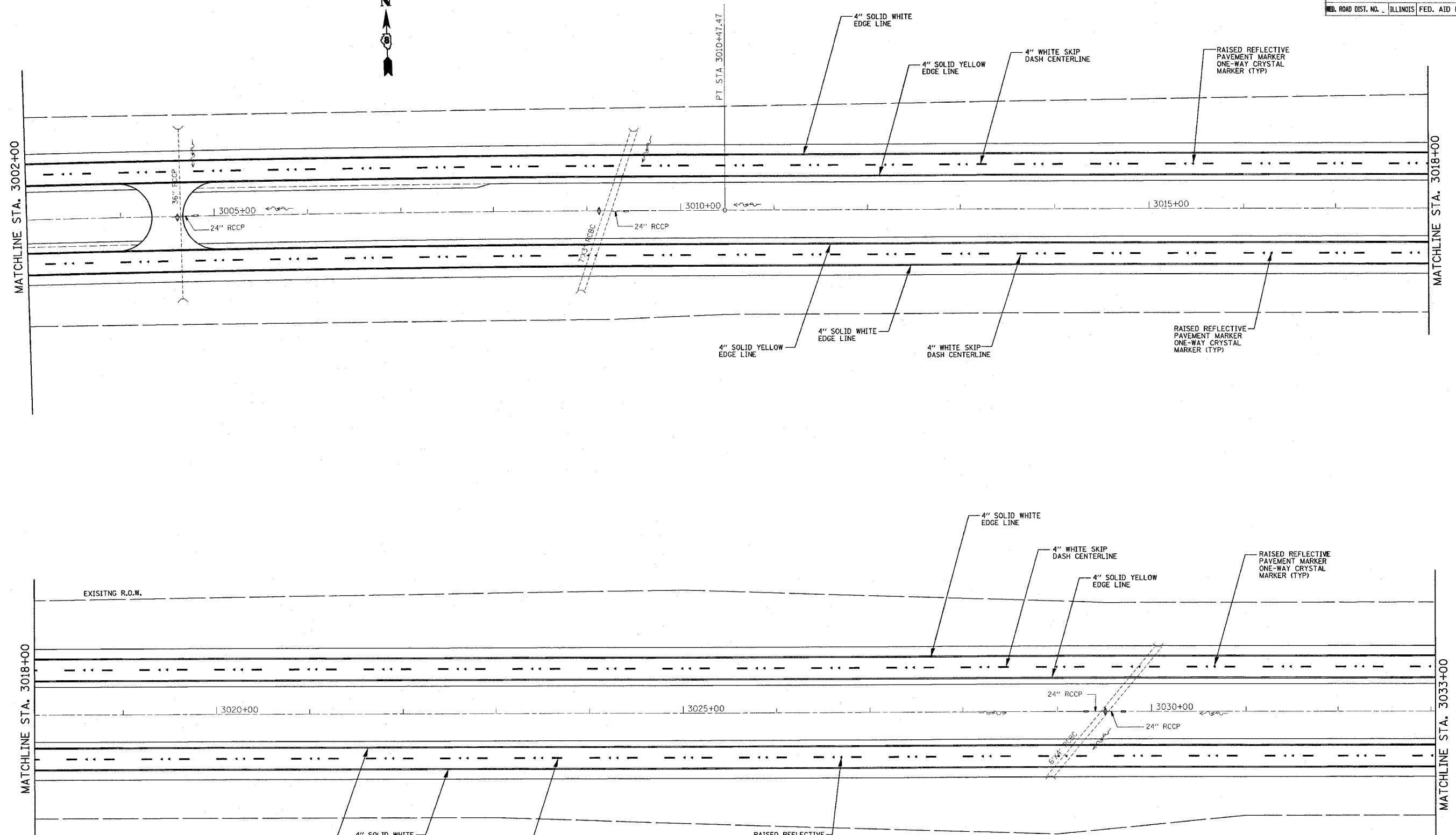


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SHEET**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 8/21/2007  
 PLOT SCALE = 1/8" = 100'  
 REFERENCE = #REF#

FAI RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	35
STA. 3002+00		TO STA. 3033+00		
ILL. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



MATCHLINE STA. 3002+00

MATCHLINE STA. 3018+00

MATCHLINE STA. 3018+00

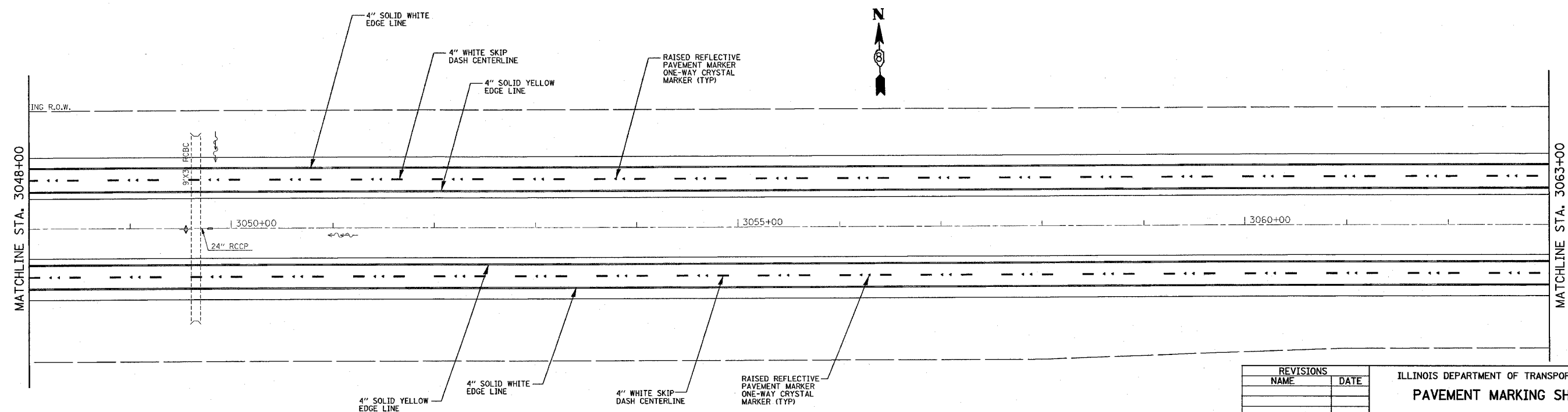
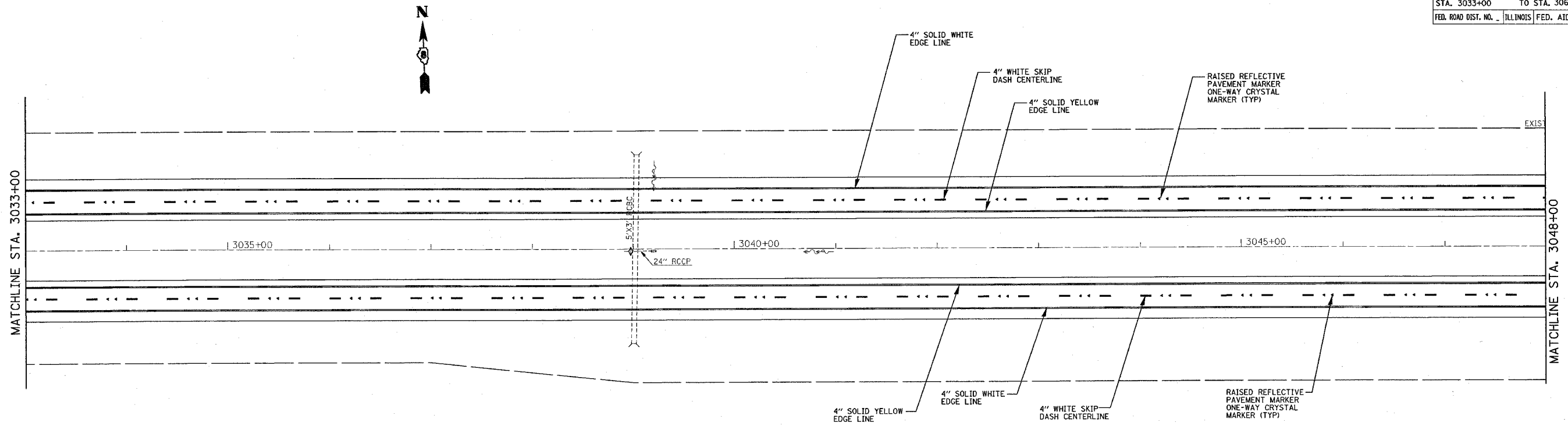
MATCHLINE STA. 3033+00

PLOT DATE = 6/21/2007  
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 PLOT SCALE = 50.0000 / 1" = 50.0000'  
 REFERENCE = #REF#

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SHEET**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT. \_\_\_\_\_ HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_ CHECKED BY \_\_\_\_\_

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	36
STA. 3033+00		TO STA. 3063+00		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

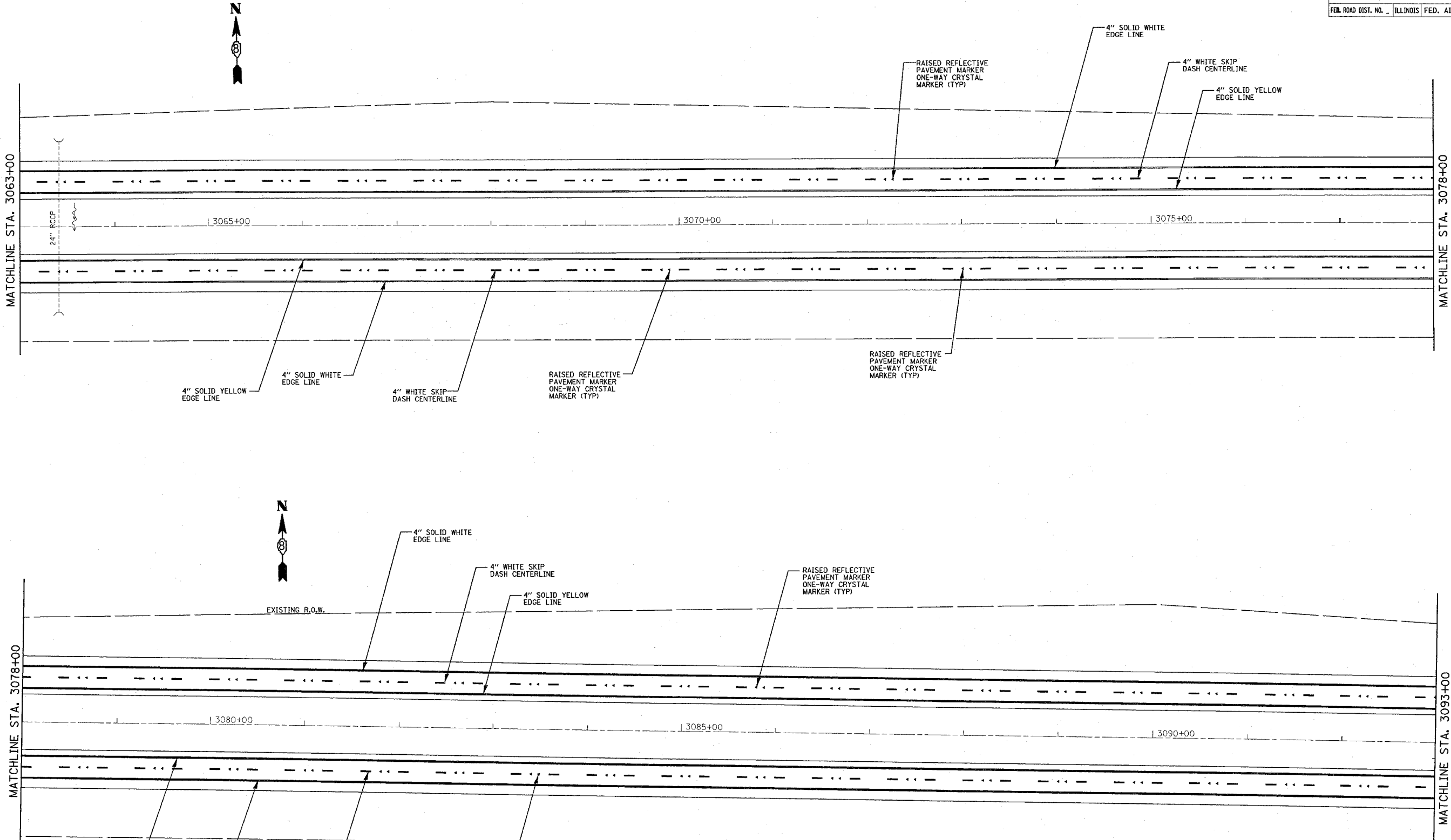


PLOT DATE = 8/21/2007  
 FILE NAME = c:\projects\76a39\plan\pvmrk01207b.dgn  
 PLOT SCALE = 30,000 / 1" IN.  
 REFERENCE = #REF#

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SHEET**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	37
STA. 3063+00 TO STA. 3093+00				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

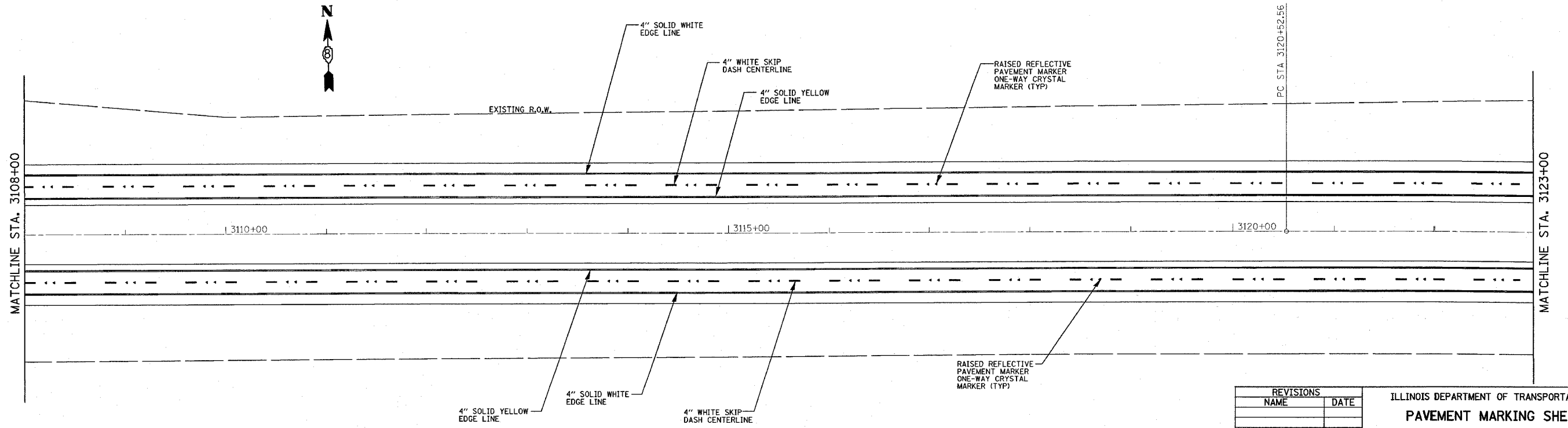
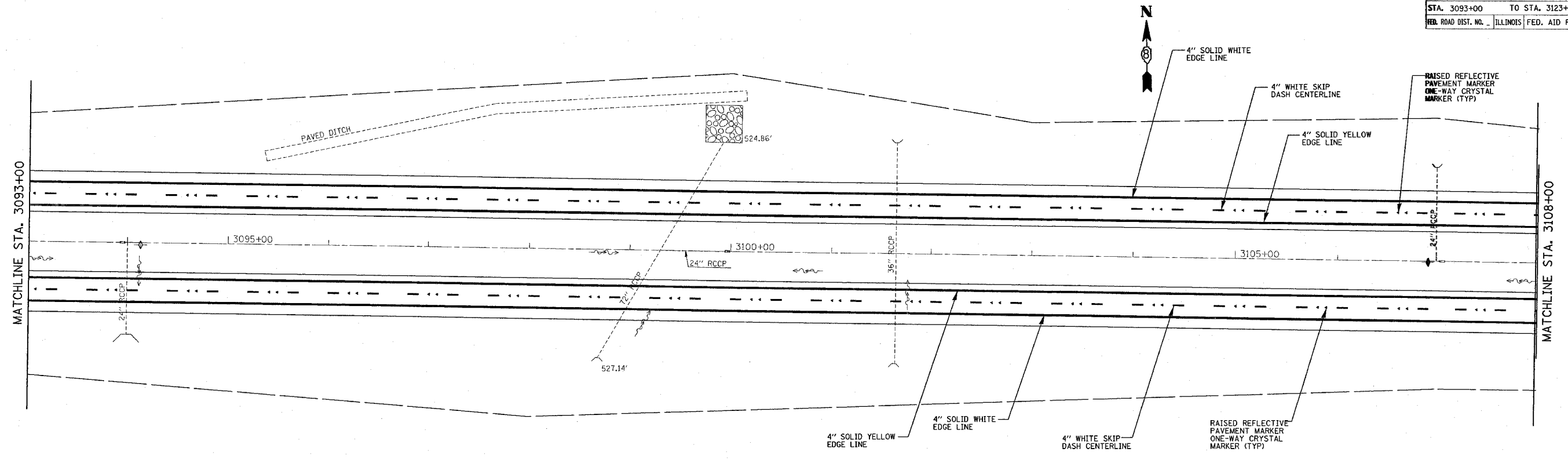


PLOT DATE = 8/21/2007  
 FILE NAME = c:\projects\81207\plan\pvt\mkd1207.dgn  
 PLOT SCALE = 30000 / 1" = 1000'  
 REFERENCE = SHEET 4

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SHEET**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	38
STA. 3093+00		TO STA. 3123+00		
ILL. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



REVISIONS	
NAME	DATE

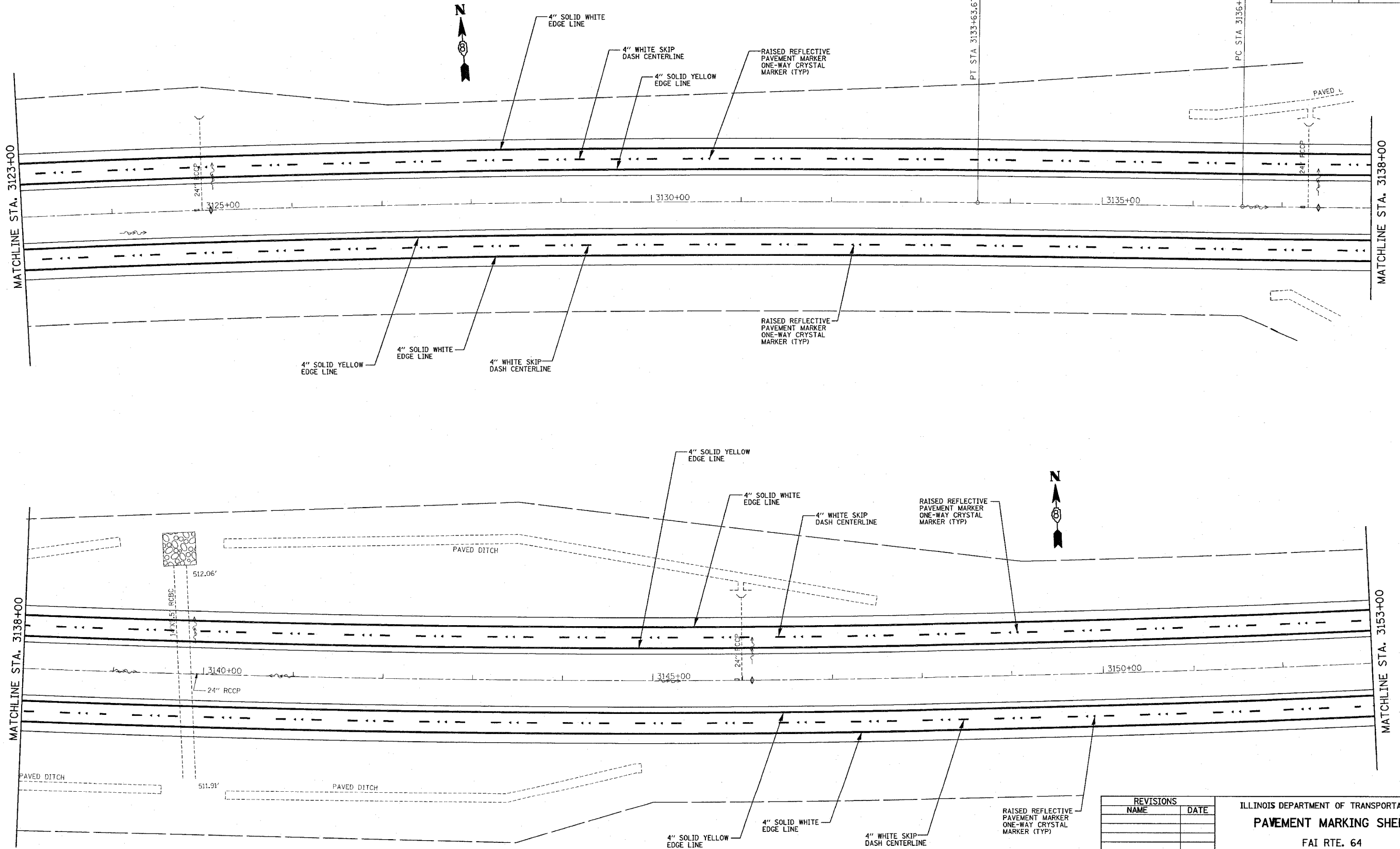
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SHEET**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_

DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 8/21/2007  
 FILE NAME = c:\projects\76a39\plan\pvmk0227b.dgn  
 PLOT SCALE = 1/8" = 1'-0"  
 REFERENCE = #REF#

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	39
STA. 3123+00		TO STA. 3153+00		
ILL. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

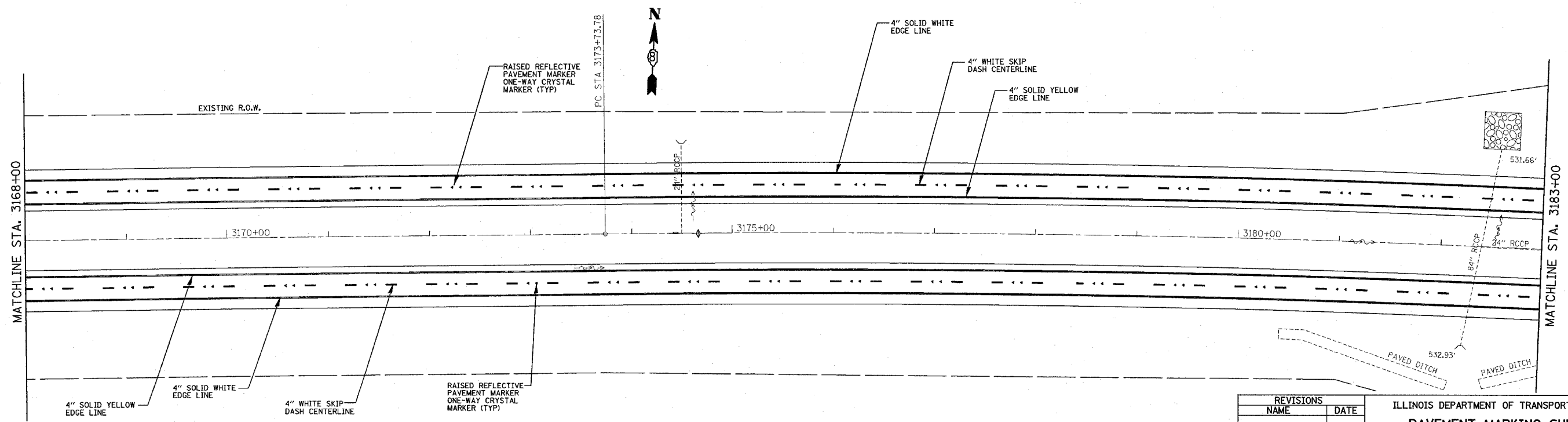
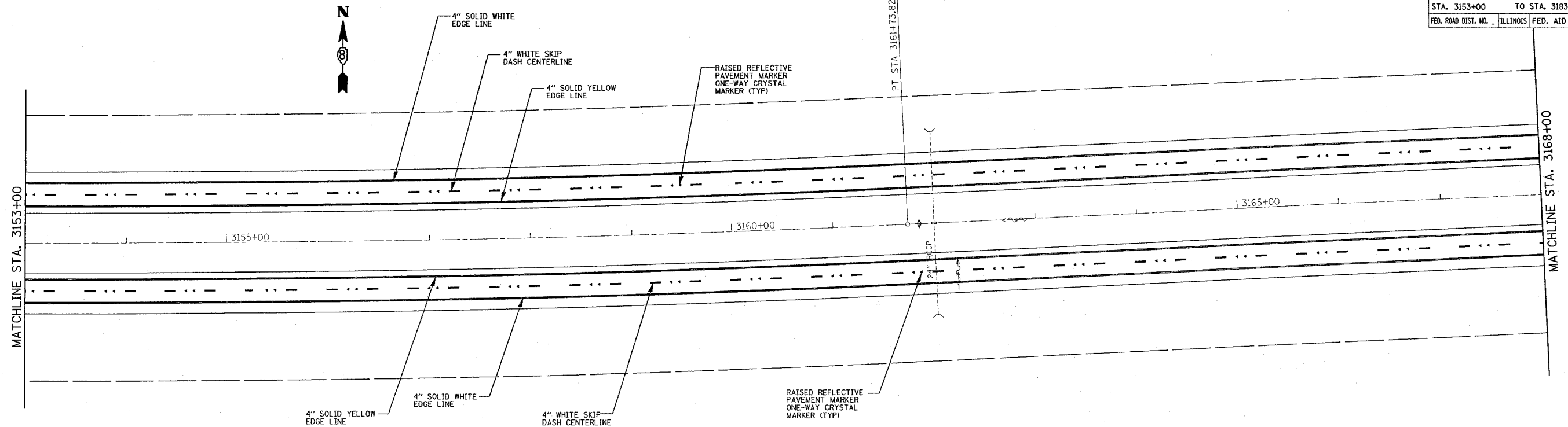


PLOT DATE = 8/21/2007  
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 PLOT SCALE = 50.000 / 1" = 100.000'  
 REFERENCE = SHEET

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SHEET**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	40
STA. 3153+00		TO STA. 3183+00		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



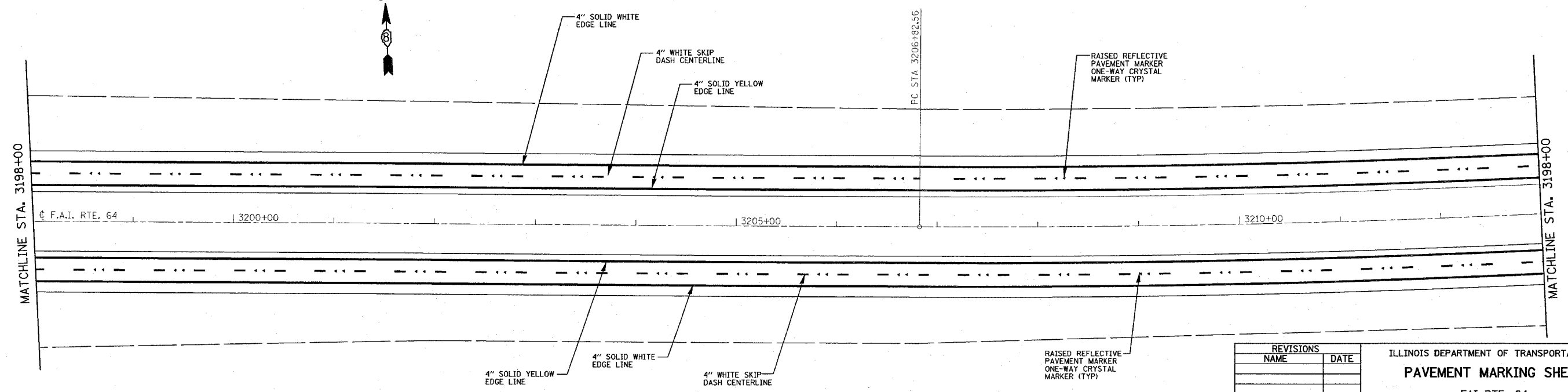
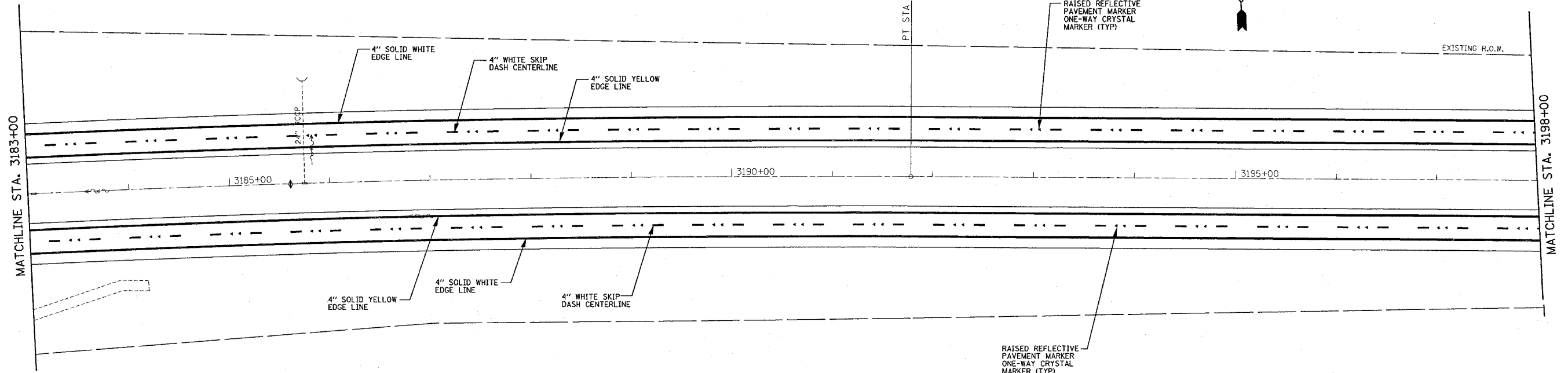
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 REFERENCE = #REF#

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SHEET**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	41
STA. 3183+00		TO STA. 3213+00		
FEB. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SHEET**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

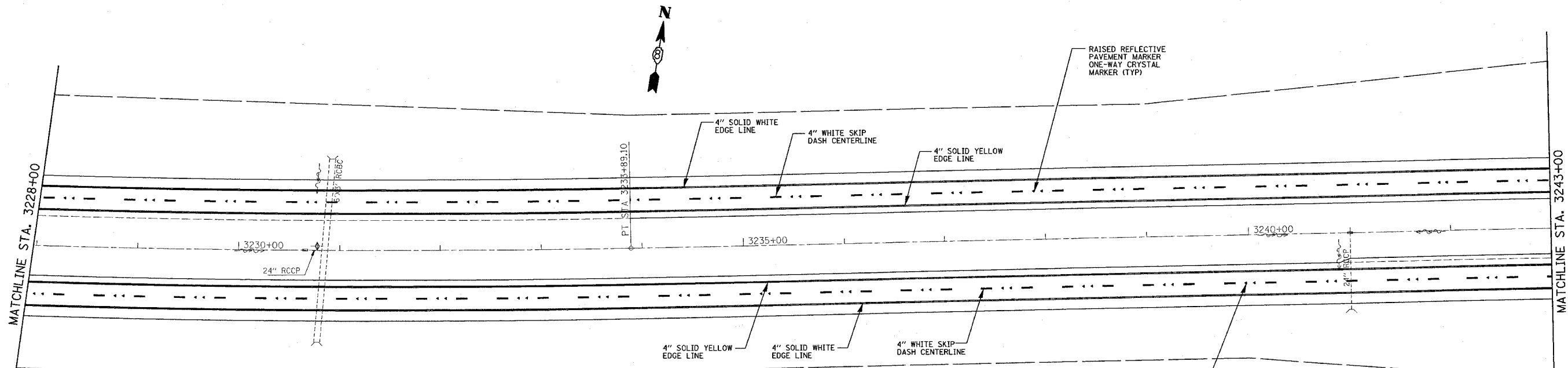
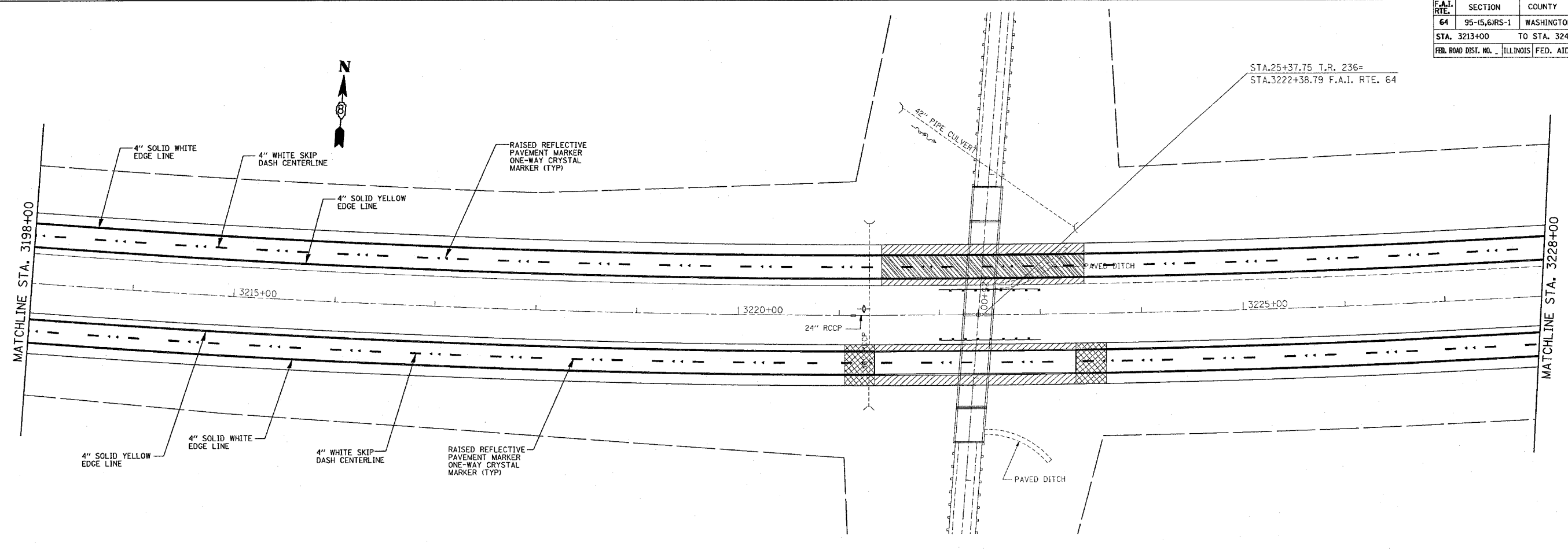
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 HORIZ. \_\_\_\_\_

DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
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 REFERENCE = MREP

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	42
STA. 3213+00		TO STA. 3243+00		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

STA.25+37.75 T.R. 236=  
STA.3222+36.79 F.A.I. RTE. 64

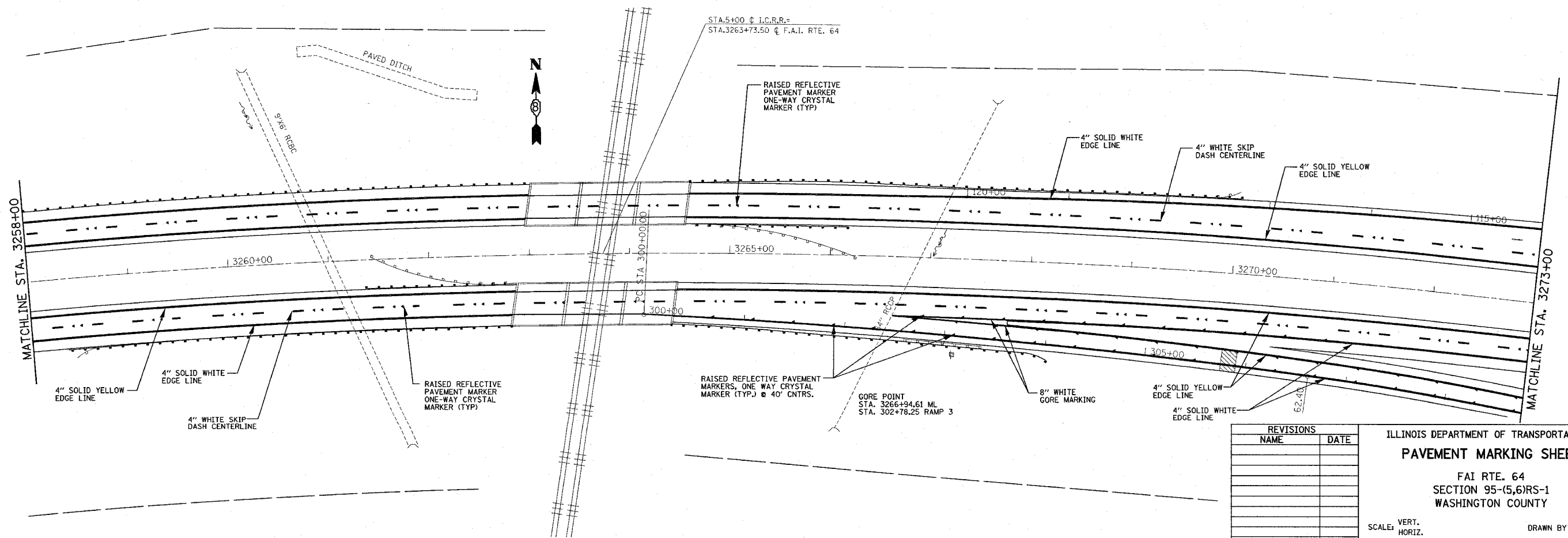
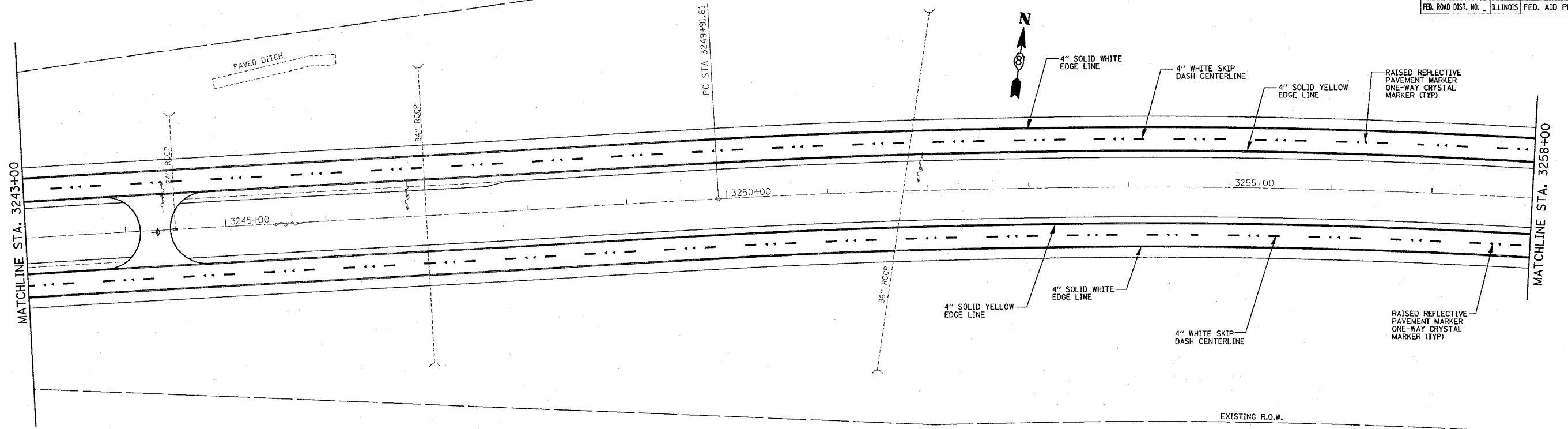


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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SHEET**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	43
STA. 3243+00		TO STA. 3273+00		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



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REVISIONS	
NAME	DATE

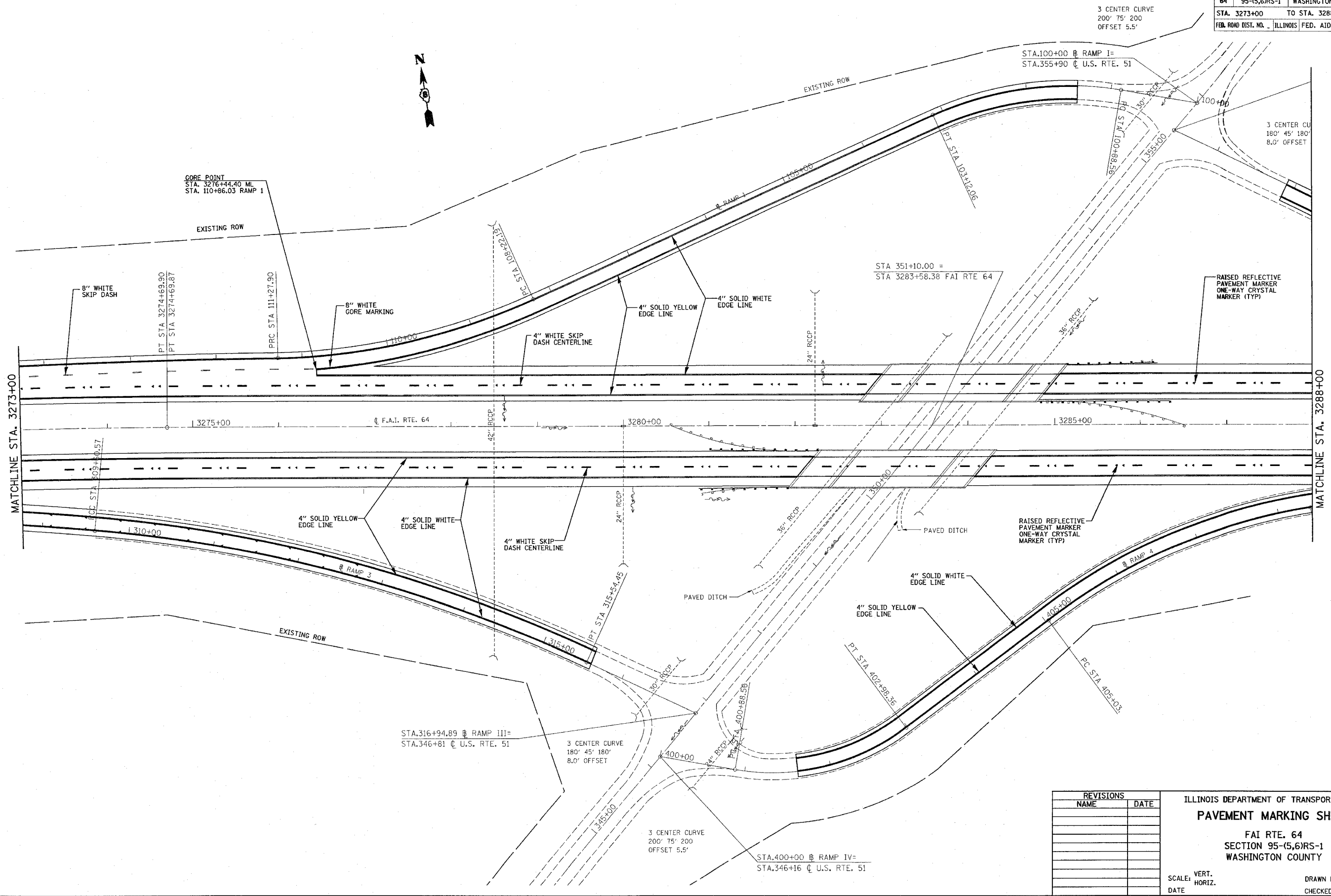
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SHEET**

FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

SCALE: VERT.  
 HORIZ.  
 DATE

DRAWN BY  
 CHECKED BY

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	44
STA. 3273+00		TO STA. 3288+00		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

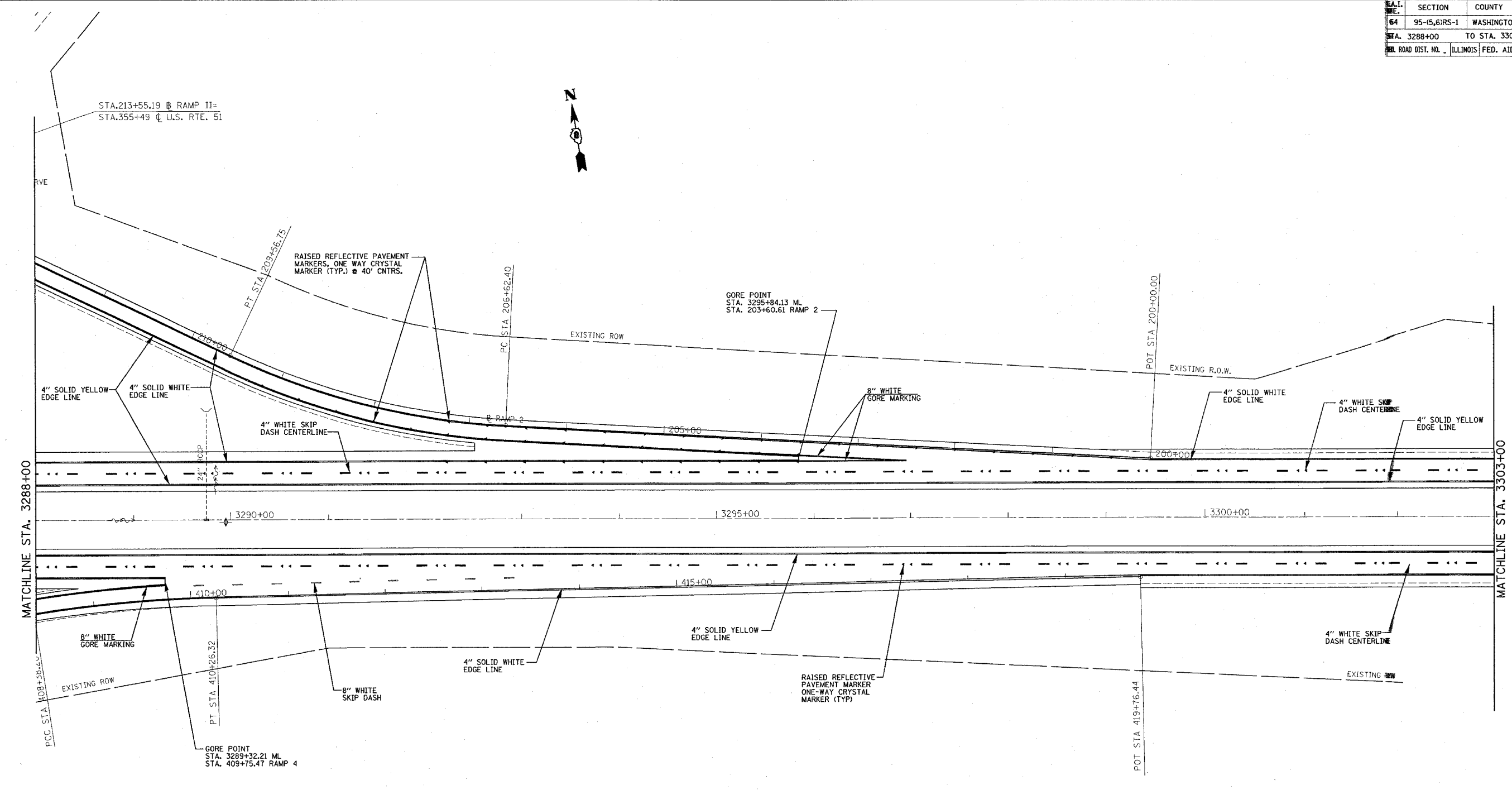


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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SHEET**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

S.A.I. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	45
STA. 3288+00		TO STA. 3303+00		
ILLINOIS FED. AID PROJECT				



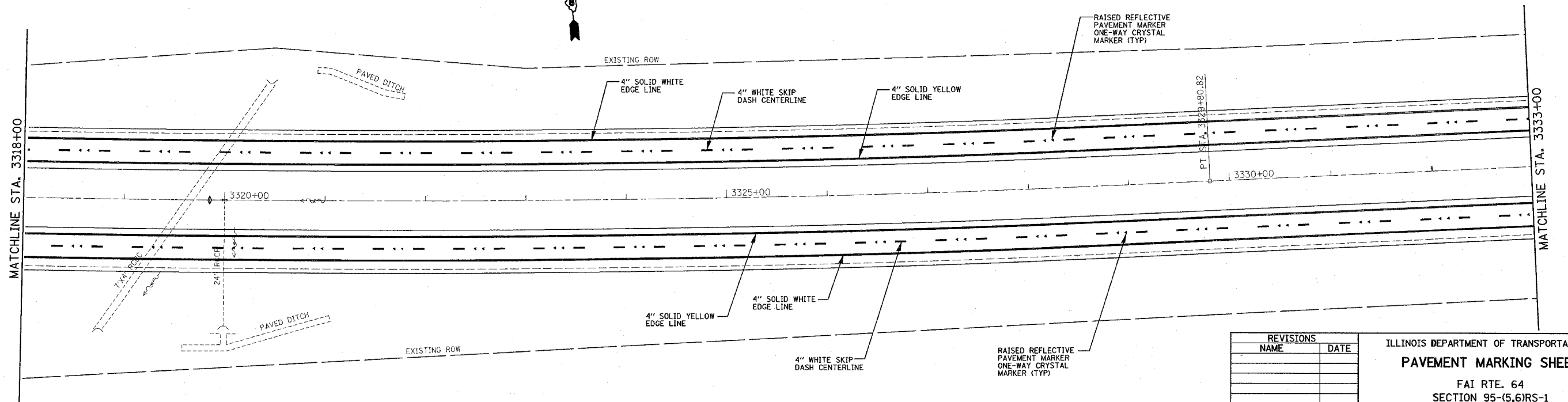
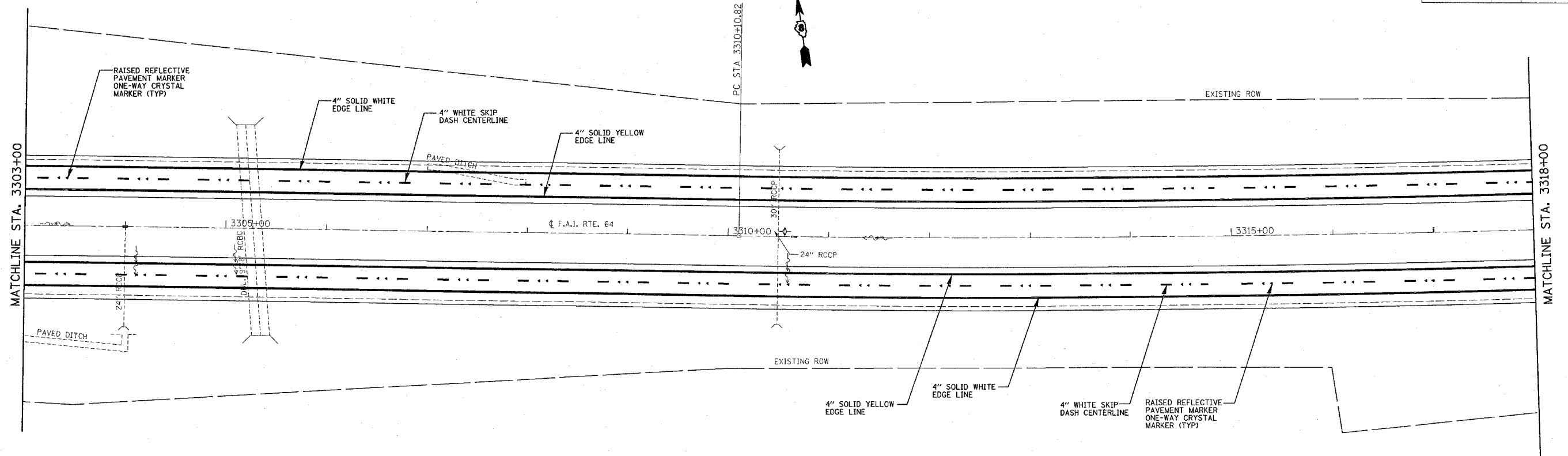
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REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION	
NAME	DATE	<b>PAVEMENT MARKING SHEET</b> FAI RTE. 64 SECTION 95-(5,6)RS-1 WASHINGTON COUNTY	

SCALE: VERT.  
 HORIZ.  
 DATE

DRAWN BY  
 CHECKED BY

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	46
STA. 3303+00		TO STA. 3333+00		
FBI. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

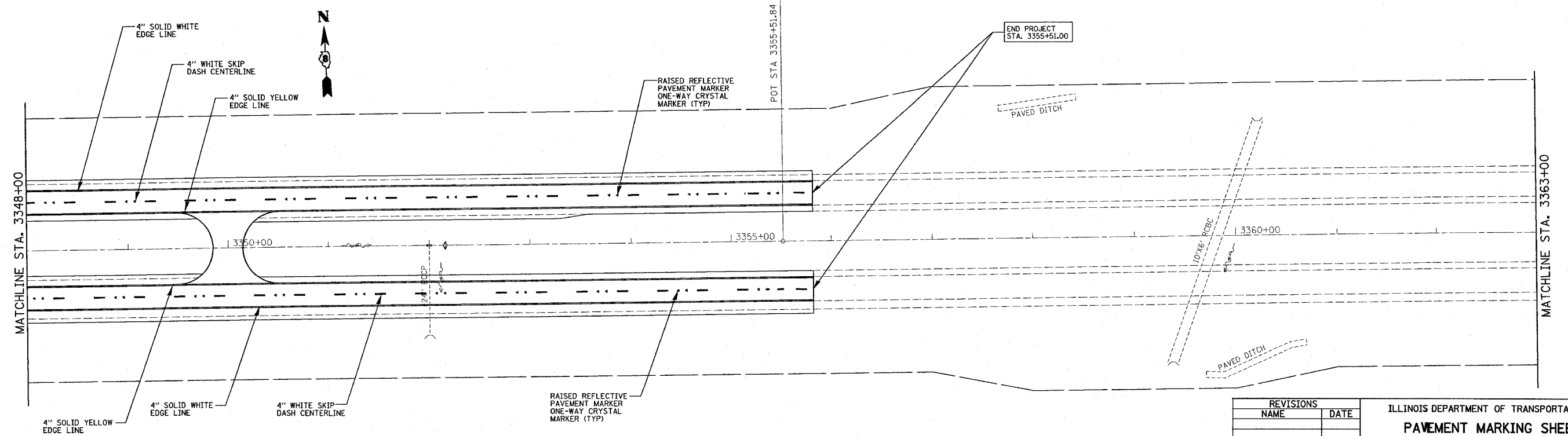
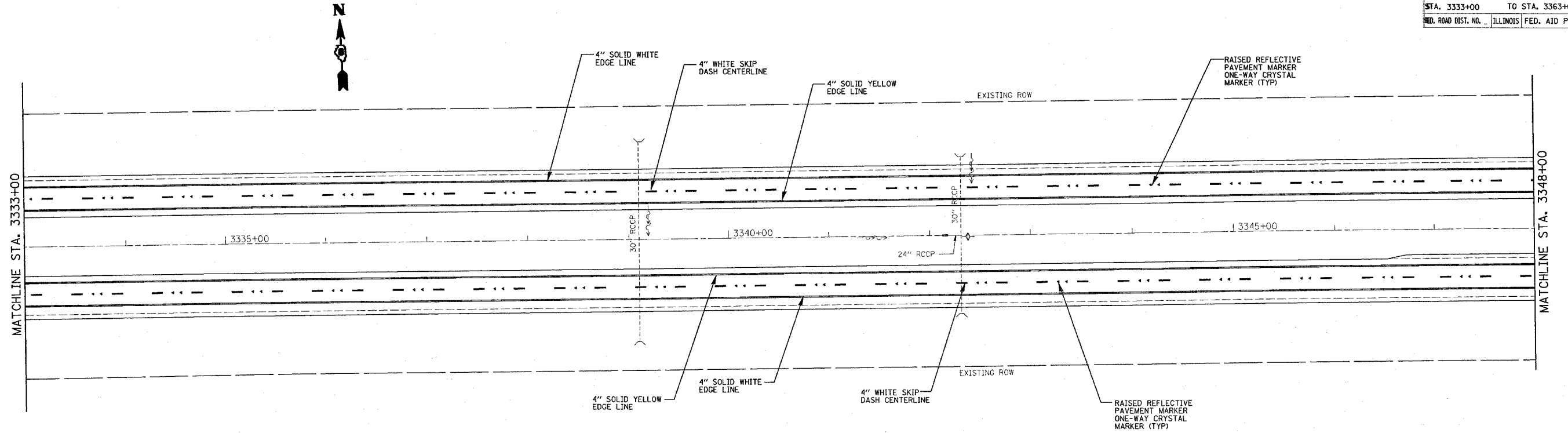


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 REFERENCE = WREF\*

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SHEET**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	47
STA. 3333+00		TO STA. 3363+00		
FED. ROAD DIST. NO. ILLINOIS		FED. AID PROJECT		



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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING SHEET**  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

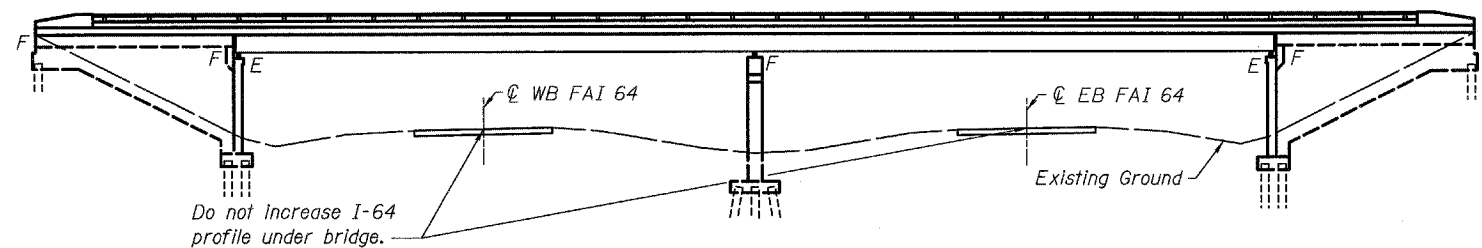
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 1
FAI 64	95-5HB-1	WASHINGTON	83	48	3 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

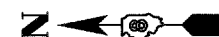
Contract #76A39

**INDEX OF SHEETS**

Sheet No.	Description
1	General Plan
2	Superstructure Details
3	Floor Drain Details



**ELEVATION**



**GENERAL NOTES**

All new fasteners shall be high strength bolts. Holes shall be subpunched or subdrilled  $\frac{1}{16}$ " dia. and reamed in the field to  $\frac{13}{16}$ " dia. for  $\frac{3}{4}$ " dia. bolts, unless otherwise noted. Holes shall be subpunched or subdrilled  $\frac{13}{16}$ " dia. and reamed in the field to  $\frac{15}{16}$ " dia. for  $\frac{7}{8}$ " dia. bolts, unless otherwise noted.

All structural steel shall be AASHTO M 270 Grade 36, unless noted otherwise.

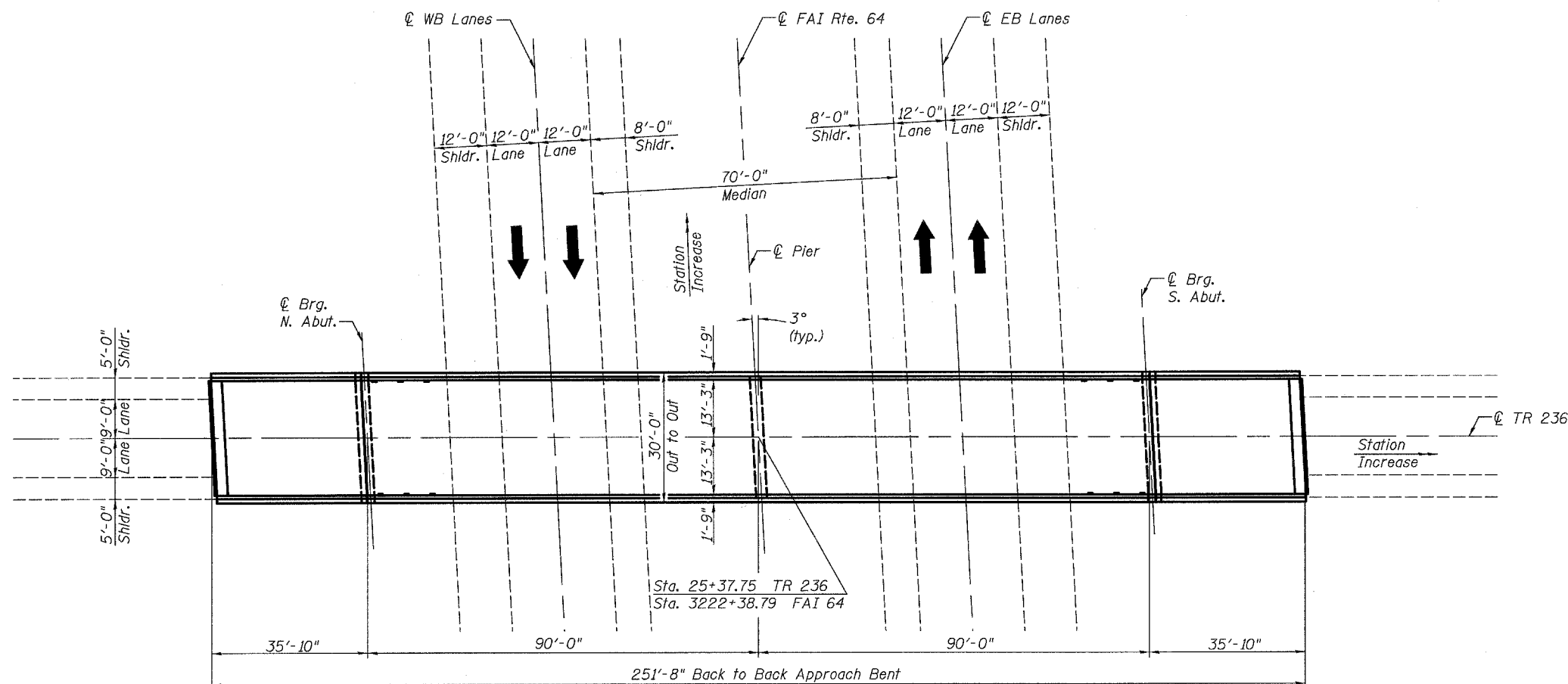
No field welding is permitted except as specified in the contract documents.

Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

Work to be completed for this structure will require traffic control. Highway Standards 701201-02, 701400-02, & 701406-04 shall be used to complete the required construction.

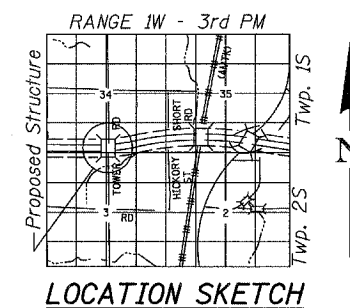
Painting of drains to be completed under a separate paint only contract.



**PLAN**

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	SUPER	SUB	TOTAL
Floor Drain Extension	Each	4	-	4
Plug Existing Deck Drains	Each	8	-	8
Silicone Joint Sealer, 1.5"	Foot	60	-	60
Slope Wall Crack Sealing	Foot	-	58	58
Traffic Control and Protection, Standard 701201	L Sum	1	-	1
Traffic Control and Protection, Standard 701406	L Sum	1	-	1



Plans Prepared By:  
**Oates Associates, Inc.**

Bruce P. Schopp  
8/24/07  
EXP 11/30/08

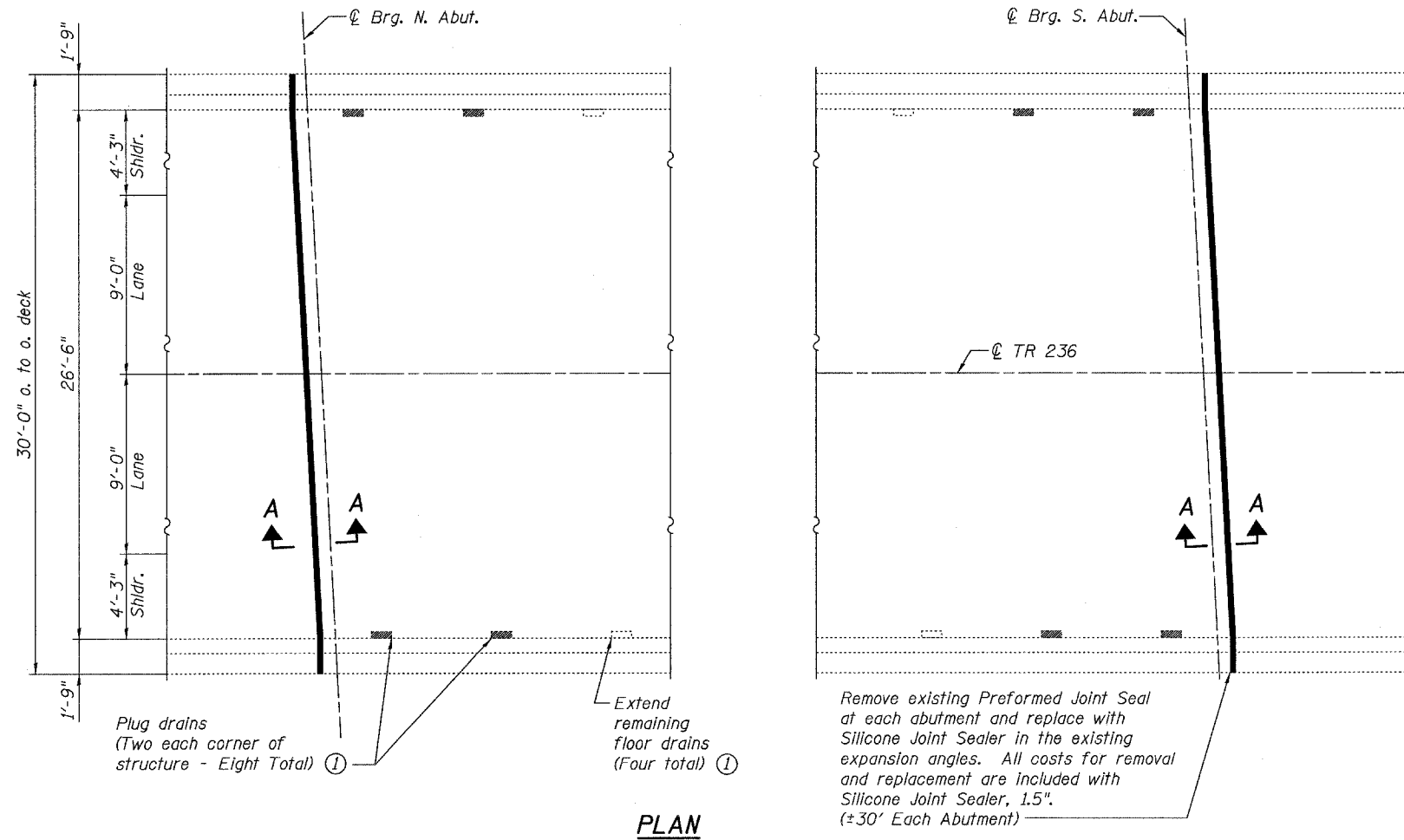
**GENERAL PLAN**  
**TR 236 (TOWER ROAD) OVER**  
**FAI 64**  
**FAI ROUTE 64 SECTION 95-5HB-1**  
**WASHINGTON COUNTY**  
**STATION 3222+38.79**  
**STRUCTURE NO. 095-0060**



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

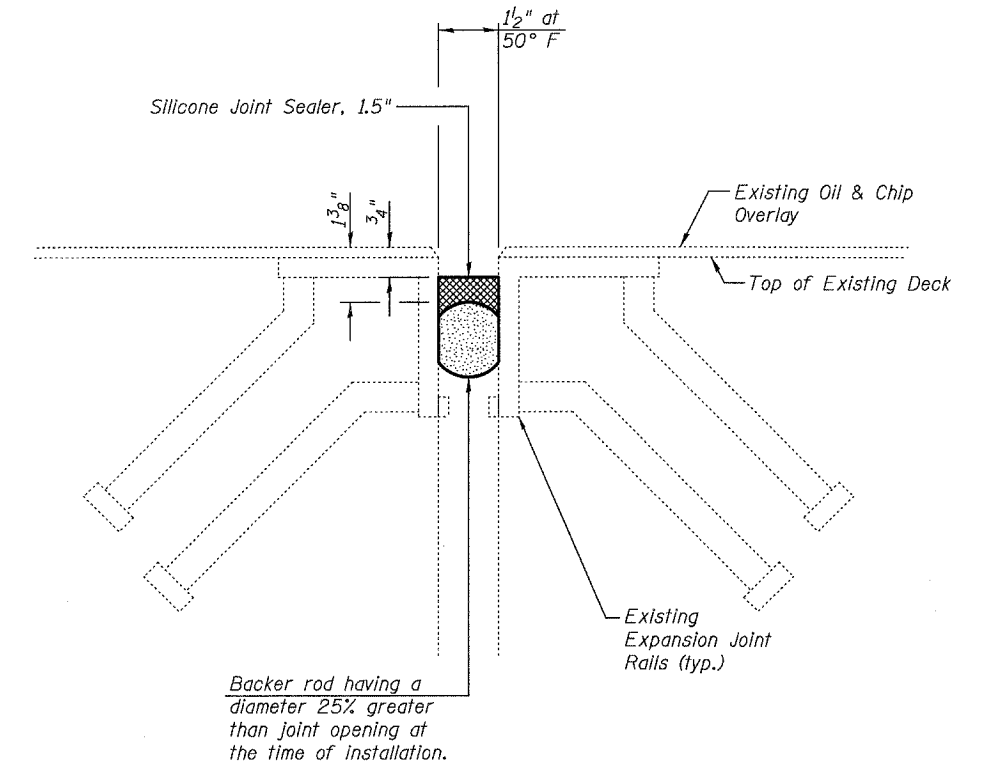
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 2
FAI 64	95-5HB-1	WASHINGTON	83	49	3 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

Contract #76A39



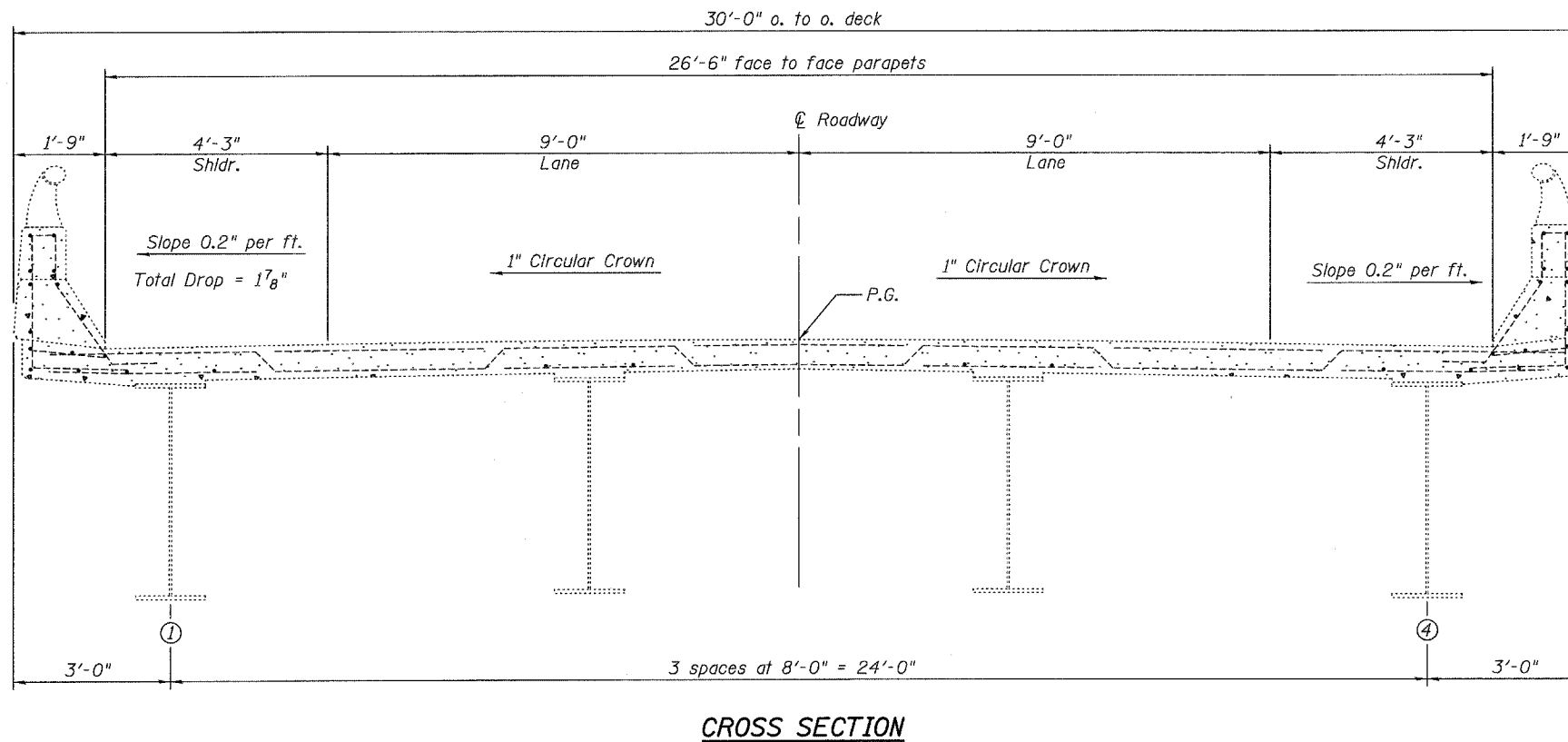
**PLAN**

Remove existing Preformed Joint Seal at each abutment and replace with Silicone Joint Sealer in the existing expansion angles. All costs for removal and replacement are included with Silicone Joint Sealer, 1.5". (±30' Each Abutment)

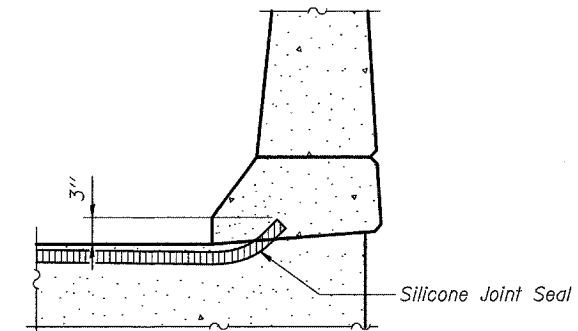


**SECTION A-A**  
(Showing Expansion Joint Repair)

Notes:  
① See sheet 3 of 3 for floor drain details.



**CROSS SECTION**



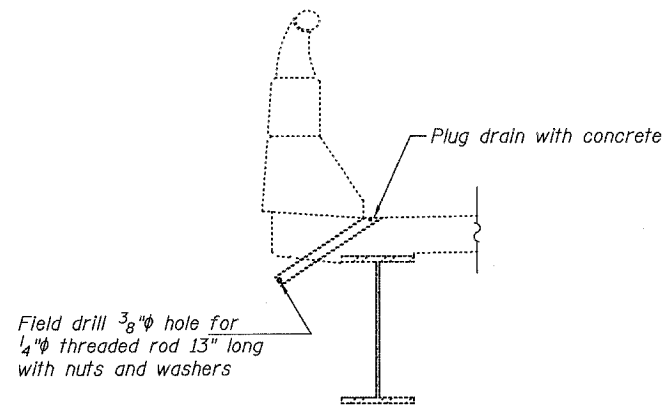
**TYPICAL END OF SEAL TREATMENT AT EXPANSION JOINT**

**SUPERSTRUCTURE DETAILS**  
TR 236 (TOWER ROAD) OVER  
FAI 64  
FAI ROUTE 64 SECTION 95-5HB-1  
WASHINGTON COUNTY  
STATION 3222+38.79  
STRUCTURE NO. 095-0060

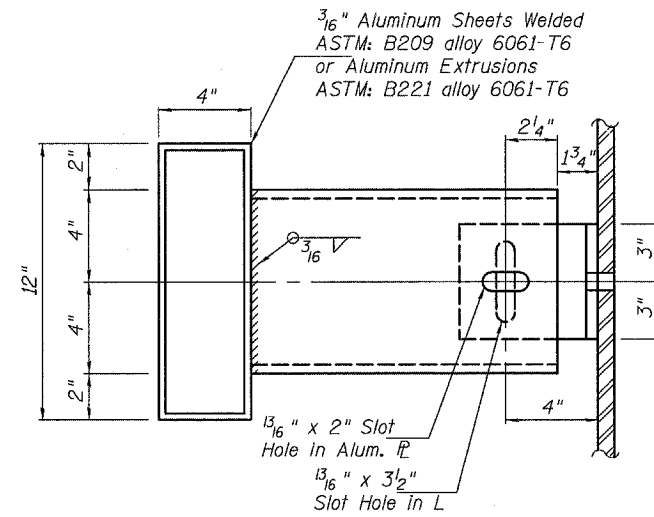
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 3 3 SHEETS
FAI 64	95-5HB-1	WASHINGTON	83	50	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

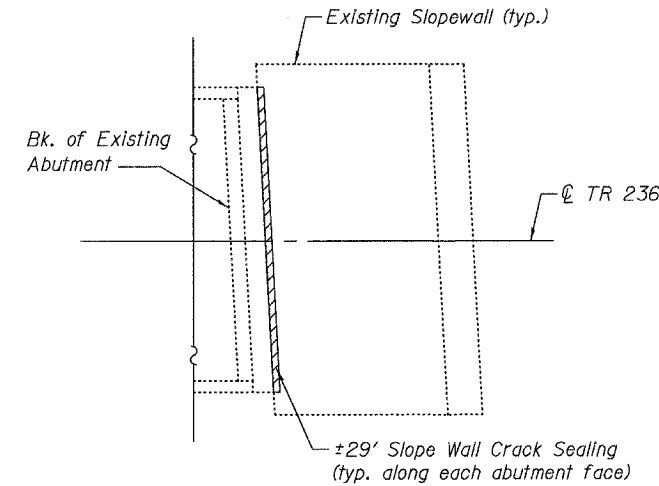
Contract #76A39



**DRAIN ELIMINATION DETAIL**  
(8 required)



**SECTION A-A**



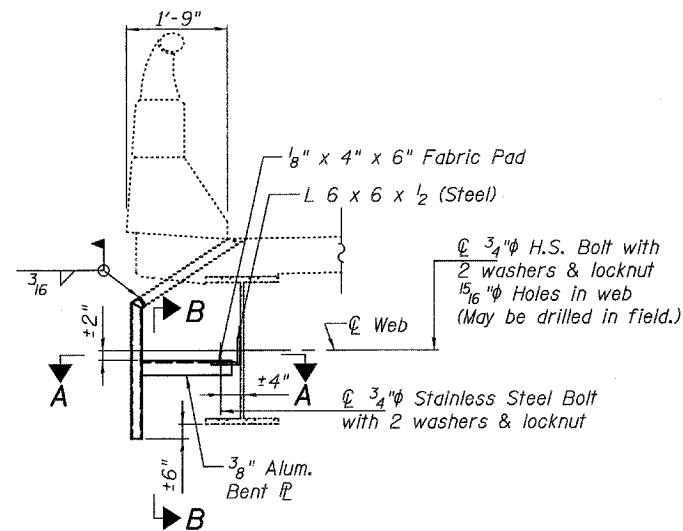
**SLOPE WALL REPAIR PLAN**  
(N. Abut. Shown - S. Abut. Similar)

**LEGEND**

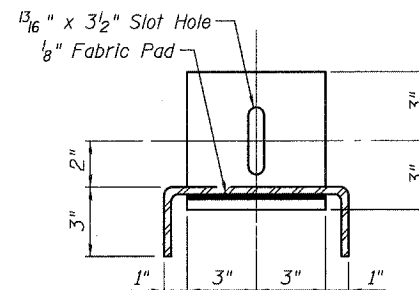
Slope Wall Crack Sealing

**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Floor Drain Extension	Each	4
Slope Wall Crack Sealing	Foot	58
Plug Existing Deck Drains	Each	8



**DRAIN EXTENSION DETAIL**  
(4 required)



**SECTION B-B**

Notes:  
① Estimated quantities are provided for Slope Wall Crack Sealing. Actual quantities will be determined by the Engineer in the field as work progresses.

**FLOOR DRAIN DETAILS**  
TR 236 (TOWER ROAD) OVER  
FAI 64  
FAI ROUTE 64 SECTION 95-5HB-1  
WASHINGTON COUNTY  
STATION 3222+38.79  
STRUCTURE NO. 095-0060

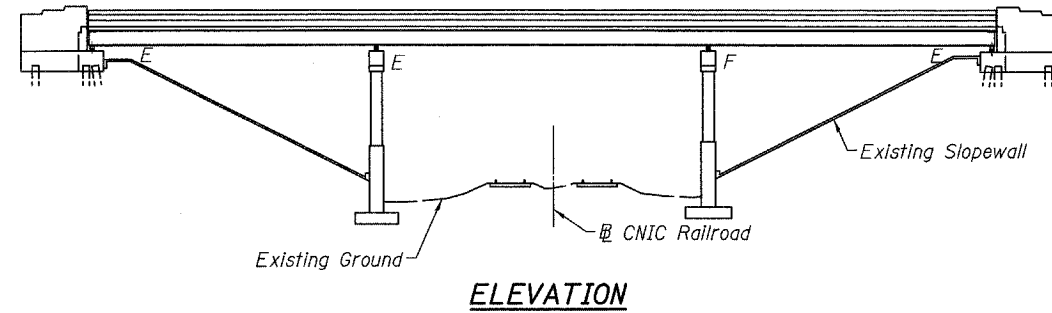
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET	SHEET NO. 1
FAI 64	95-6VB-1	WASHINGTON	83	51	7 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

Contract #76A39

INDEX OF SHEETS

Sheet No.	Description
1	General Plan
2	Deck Plan
3	Expansion Joint Repair
4	Prefomed Joint Strip Seal
5	Abutment Concrete Repairs
6	Sloped Wall Repairs
7	Bar Splicer Assembly Details



GENERAL NOTES

All structural steel shall be AASHTO M 270 Grade 36.  
Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.  
No field welding is permitted except as specified in the contract documents.  
The existing bridge seats at the abutments only (2 each structure) shall be cleaned and dried according to the special provision for Cleaning Bridge Seats.  
Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60 (IL Modified). See Special Provisions.  
Reinforcement bars designated (E) shall be epoxy coated.  
Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contract with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included with Concrete Removal.  
The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.  
Traffic staging required. See roadway plans & sheet 3 of 7.

TOTAL BILL OF MATERIAL

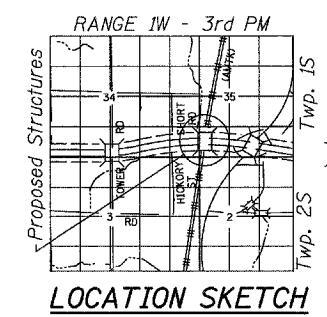
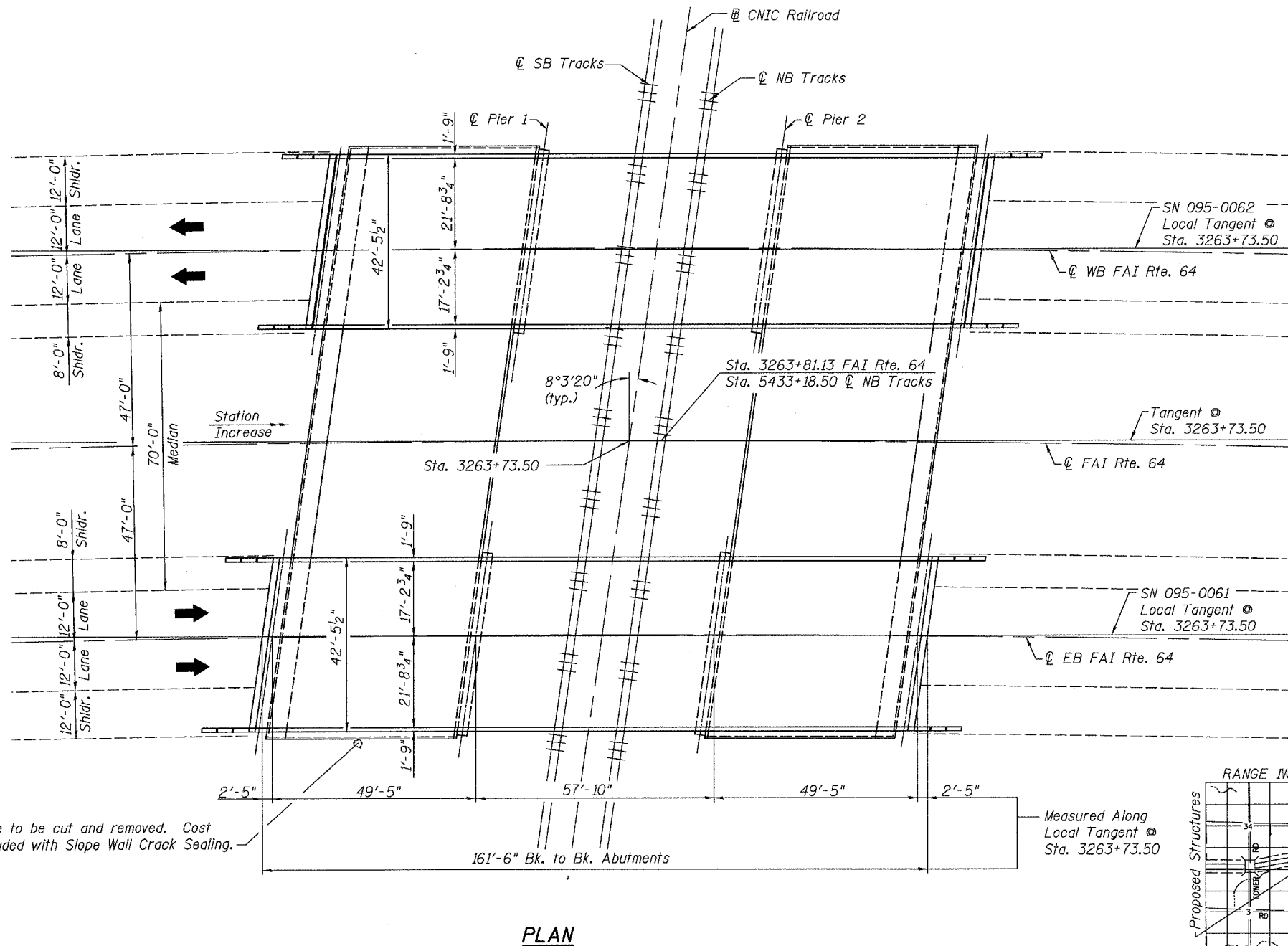
(SN 095-0061)

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	11.2	-	11.2
Concrete Superstructure	Cu. Yd.	11.2	-	11.2
Protective Coat	Sq. Yd.	848	-	848
Reinforcement Bars, Epoxy Coated	Pound	1,860	-	1,860
Bar Splicers	Each	30	-	30
Prefomed Joint Strip Seal	Foot	82	-	82
Bridge Seat Sealer	Sq. Ft.	-	243	243
Controlled Low-Strength Material	Cu. Yd.	-	104	104
Slope Wall Crack Sealing	Foot	-	188	188
Structural Repair of Concrete (Depth Equal To Or Less Than 5")	Sq. Ft.	14	-	14
Cleaning Bridge Seats	Sq. Ft.	-	243	243
Unsound Concrete Removal	Sq. Yd.	16	-	16

TOTAL BILL OF MATERIAL

(SN 095-0062)

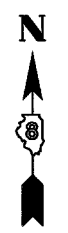
ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	11.2	-	11.2
Concrete Superstructure	Cu. Yd.	11.2	-	11.2
Protective Coat	Sq. Yd.	848	-	848
Reinforcement Bars, Epoxy Coated	Pound	1,860	-	1,860
Bar Splicers	Each	30	-	30
Prefomed Joint Strip Seal	Foot	82	-	82
Bridge Seat Sealer	Sq. Ft.	-	243	243
Controlled Low-Strength Material	Cu. Yd.	-	78	78
Slope Wall Crack Sealing	Foot	-	188	188
Structural Repair of Concrete (Depth Equal To Or Less Than 5")	Sq. Ft.	14	-	14
Cleaning Bridge Seats	Sq. Ft.	-	243	243
Unsound Concrete Removal	Sq. Yd.	19	-	19



Plans Prepared By:  
Oates Associates, Inc.

Bruce P. Schopp  
8/24/07  
EXP 11/30/08

**GENERAL PLAN**  
FAI 64 OVER  
CNIC RAILROAD  
FAI ROUTE 64 SECTION 95-6VB-1  
WASHINGTON COUNTY  
STATION 3263+73.50  
STRUCTURE NO. 095-0061 (EB)  
STRUCTURE NO. 095-0062 (WB)



Tree to be cut and removed. Cost included with Slope Wall Crack Sealing.

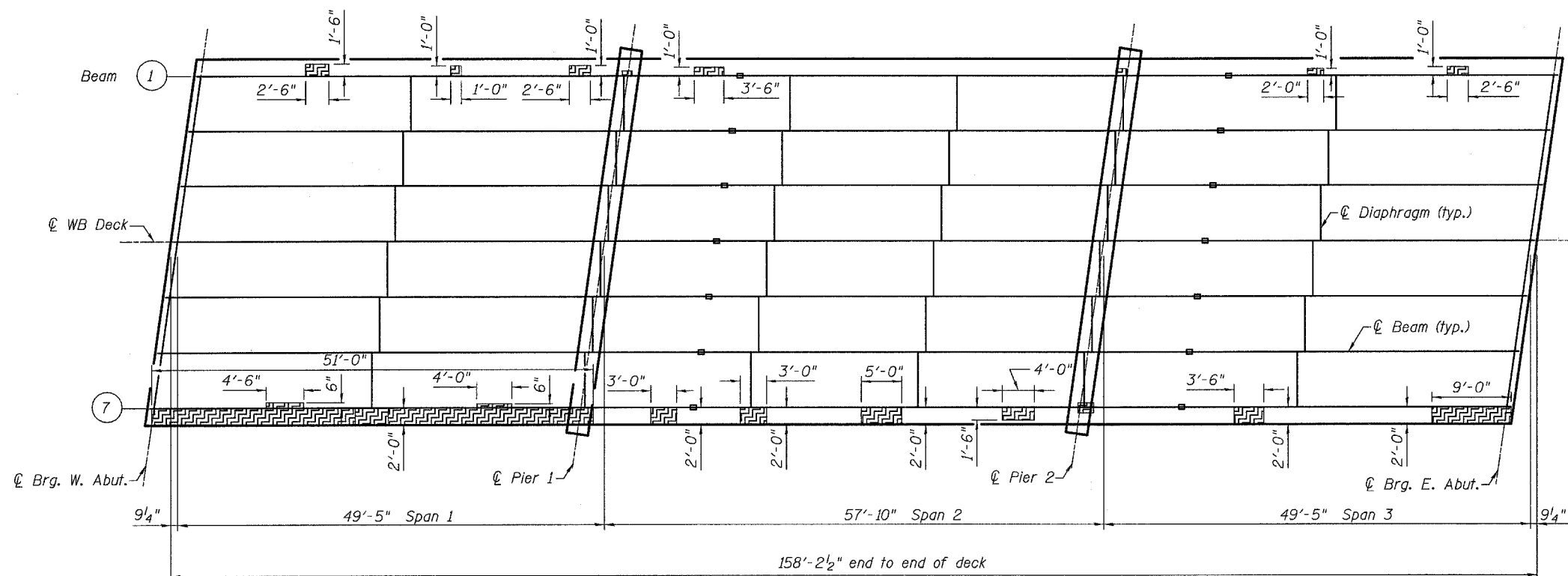
Measured Along Local Tangent @ Sta. 3263+73.50

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

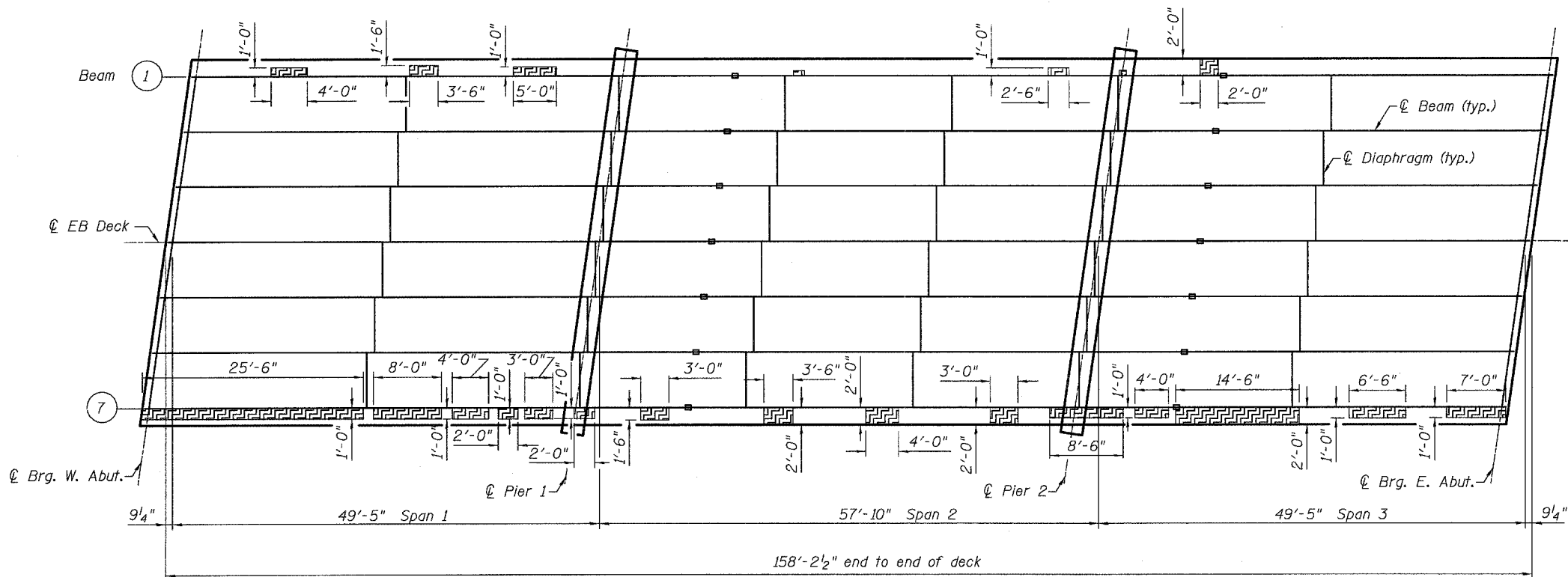
ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET
FAI 64	95-6VB-1	WASHINGTON	83	52
FED. ROAD DIST. NO. 7	ILL. DISTRICT	FED. AID PROJECT-		

SHEET NO. 2  
7 SHEETS

Contract #76A39



**BOTTOM OF DECK - S.N. 095-0062 (WB)**




**BOTTOM OF DECK - S.N. 095-0061 (EB)**



- Notes:
- Deck Condition Survey performed 5/31/2007.
  - Estimated quantities are provided for Unsound Concrete Removal. Actual quantities will be determined by the Engineer in the field as work progresses.
  - Each structure crosses the railroad in Span 2. The Contractor shall take precautions to limit falling debris onto railroad right-of-way. Debris that falls in this area shall be removed immediately. Coordination with the railroad is required prior to beginning construction activities. Cost included with Unsound Concrete Removal. See special provisions.

**LEGEND**

 Unsound Concrete Removal

**BILL OF MATERIAL**  
(SN 095-0061)

ITEM	UNIT	SUPER	SUB	TOTAL
Unsound Concrete Removal	Sq. Yd.	16	-	16

**BILL OF MATERIAL**  
(SN 095-0062)

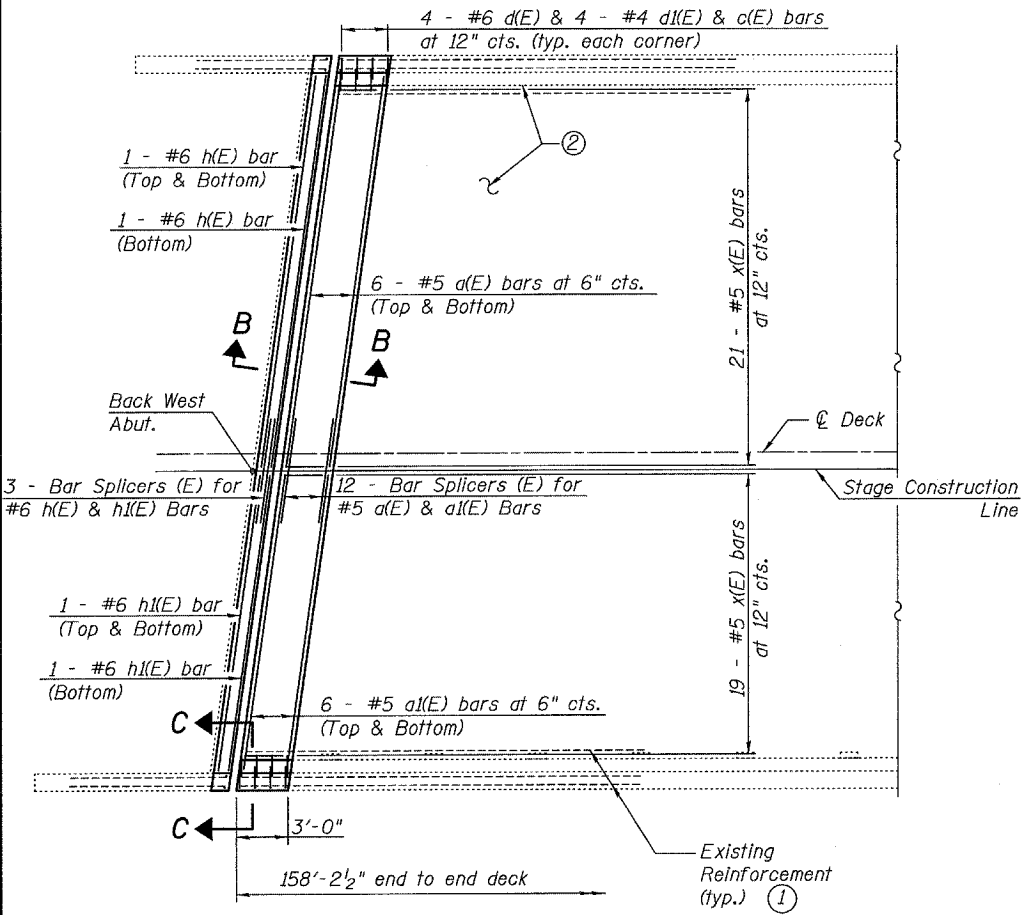
ITEM	UNIT	SUPER	SUB	TOTAL
Unsound Concrete Removal	Sq. Yd.	19	-	19

**DECK PLAN**  
FAI 64 OVER  
CNIC RAILROAD  
FAI ROUTE 64 SECTION 95-6VB-1  
WASHINGTON COUNTY  
STATION 3263+73.50  
STRUCTURE NO. 095-0061 (EB)  
STRUCTURE NO. 095-0062 (WB)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

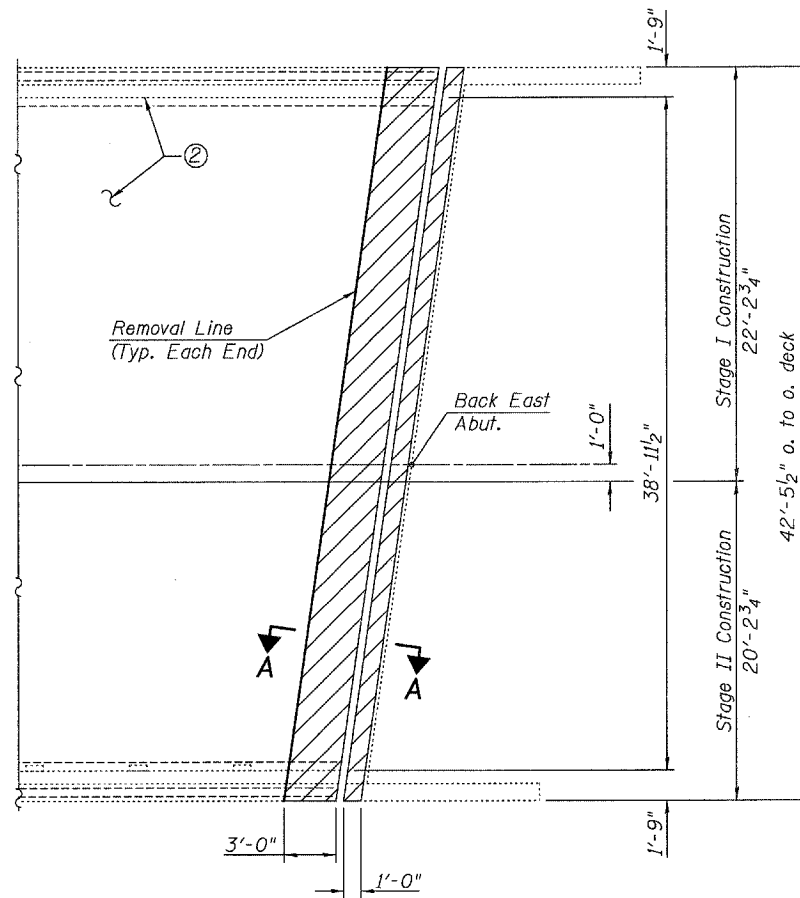
ROUTE NO. FAI 64	SECTION 95- 6VB-1	COUNTY WASHINGTON	TOTAL SHEETS 83	SHEET NO. 53	SHEET NO. 3 7 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT		

Contract #76A39



**NEW CONSTRUCTION**  
(Typical Each End of Deck)

**PLAN**  
(Eastbound shown - Westbound similar)



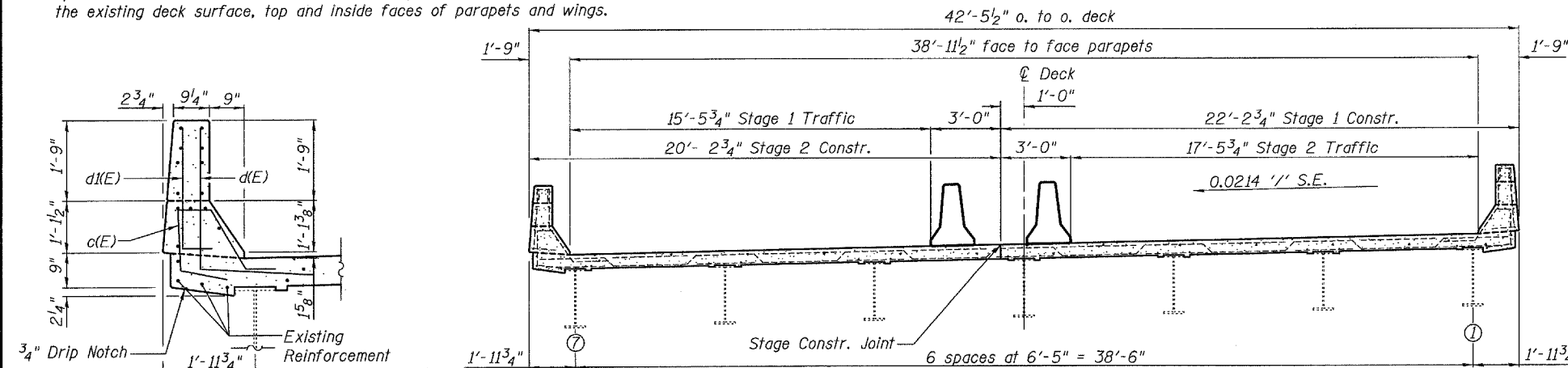
**CONCRETE REMOVAL**  
(Typical Each End of Deck)

Indicates Limits of Concrete Removal

Notes:

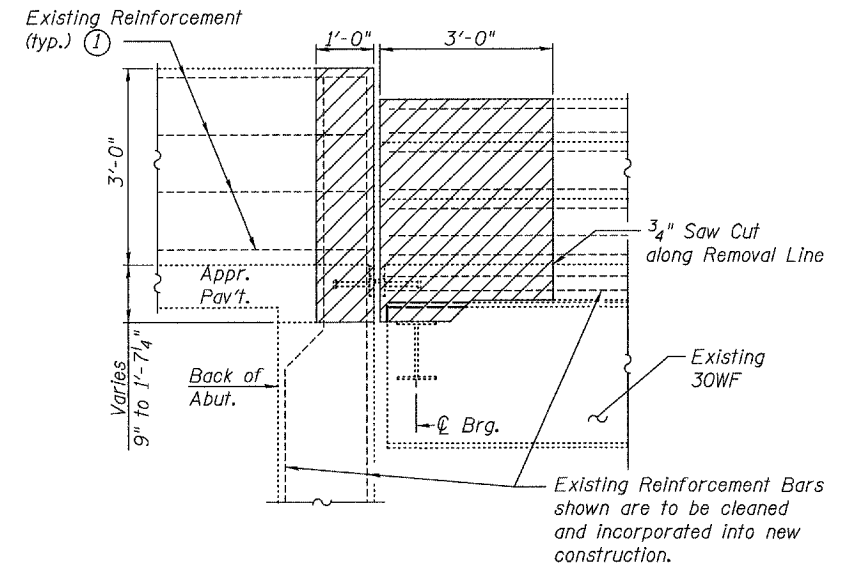
① Existing longitudinal reinforcement bars projecting from the deck and parapets are to remain in place. The existing reinforcement shall be cleaned, straightened and incorporated into the new construction. Cut the existing reinforcement as required so that it will end 2" from the edge of the new deck. Any reinforcement bars damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost Included with Concrete Removal.

② Protective Coat shall be applied to the entire deck surface and as specified in Article 503.19 of the Standard Specifications, which includes the existing deck surface, top and inside faces of parapets and wings.



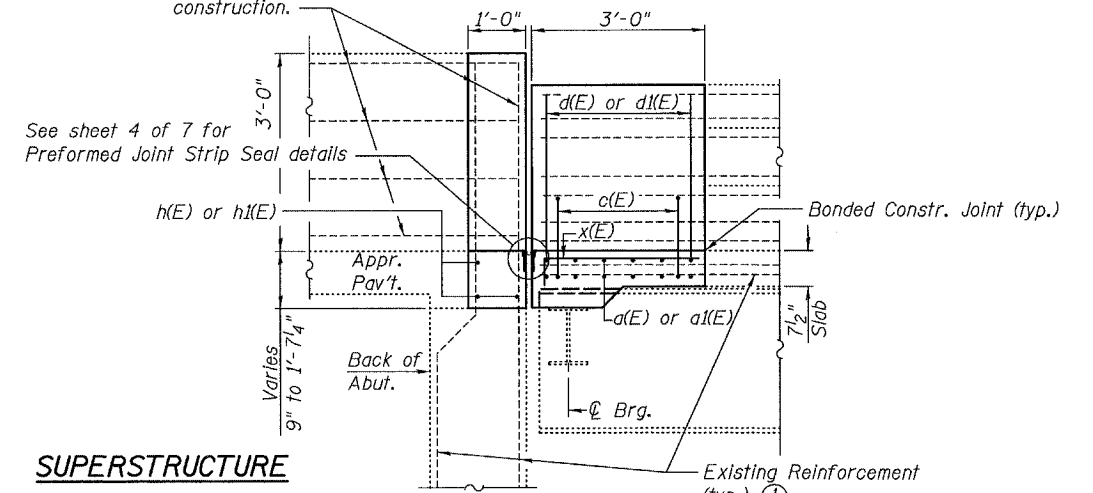
**CROSS SECTION AT END OF DECK**  
(Looking West)

Note:  
Staging is for information only. Coordinate with roadway staging.



**SECTION A-A**

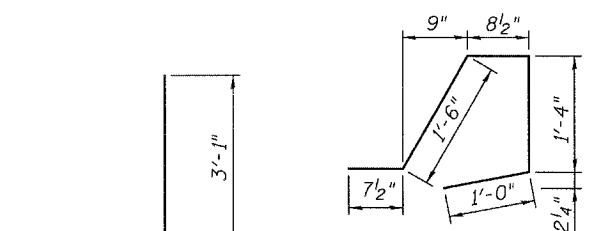
Existing Reinforcement Bars shown are to be cleaned and incorporated into new construction.



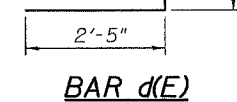
**SECTION B-B**

**SUPERSTRUCTURE  
BILL OF MATERIAL**  
(One Structure)

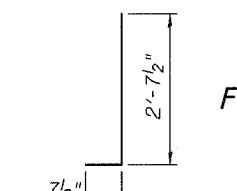
Bar	No.	Size	Length	Shape
a(E)	24	#5	21'-7"	—
a(E)	24	#5	19'-7"	—
c(E)	16	#4	5'-2"	⌋
d(E)	16	#6	5'-6"	⌋
d(E)	16	#4	3'-3"	⌋
h(E)	6	#6	20'-5"	—
h(E)	6	#6	18'-5"	—
x(E)	80	#5	3'-1"	⌋
Reinforcement Bars, Epoxy Coated		Pound	1,860	
Concrete Removal		Cu. Yd.	11.2	
Concrete Superstructure		Cu. Yd.	11.2	
Protective Coat		Sq. Yd.	848	



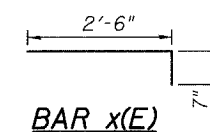
**BAR c(E)**



**BAR d(E)**



**BAR d(E)**



**BAR x(E)**

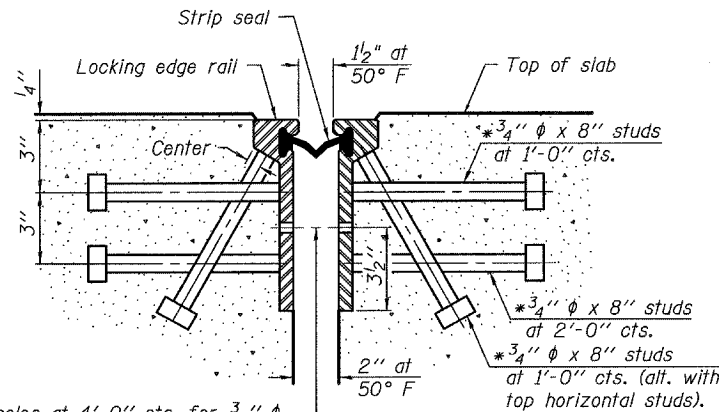
**EXPANSION JOINT REPAIR**  
FAI 64 OVER  
CNIC RAILROAD  
FAI ROUTE 64 SECTION 95-6VB-1  
WASHINGTON COUNTY  
STATION 3263+73.50  
STRUCTURE NO. 095-0061 (EB)  
STRUCTURE NO. 095-0062 (WB)

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

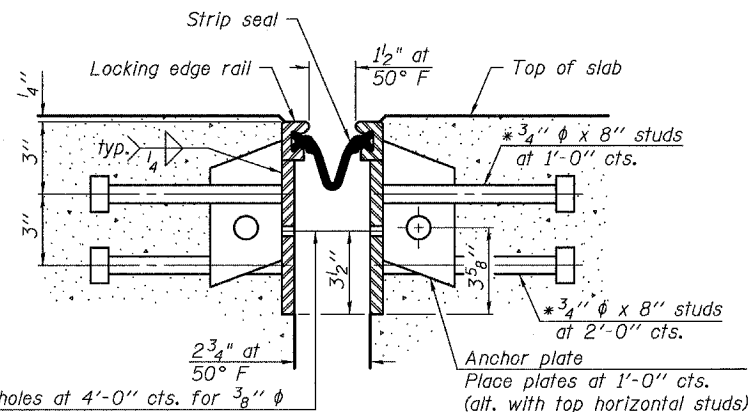
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 4
FAI 64	95-6VB-1	WASHINGTON	83	54	7 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #76A39



7/16 inch phi holes at 4 foot 0 inch cts. for 3/8 inch phi bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU  
ROLLED RAIL JOINT

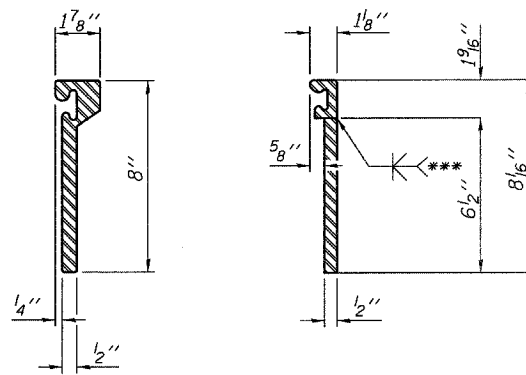


7/16 inch phi holes at 4 foot 0 inch cts. for 3/8 inch phi bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

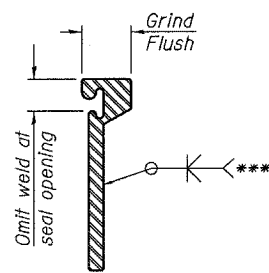
SECTION THRU  
WELDED RAIL JOINT

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4 inch. The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches. The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints. The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State. All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.



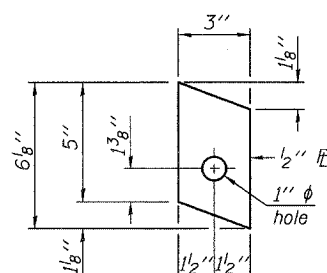
ROLLED  
(EXTRUDED) RAIL      WELDED RAIL



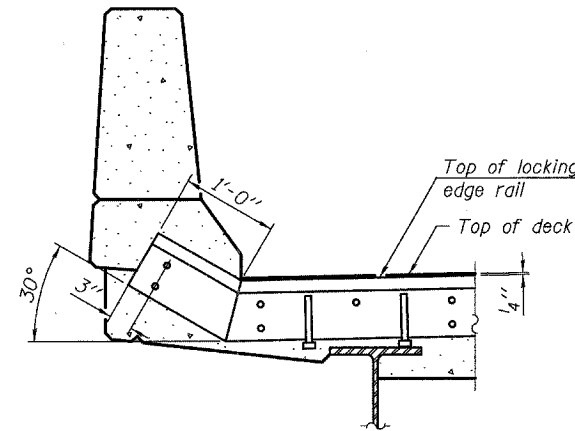
\*\*\* Back gouge not required if complete joint penetration is verified by mock-up.

LOCKING EDGE  
RAIL SPLICE

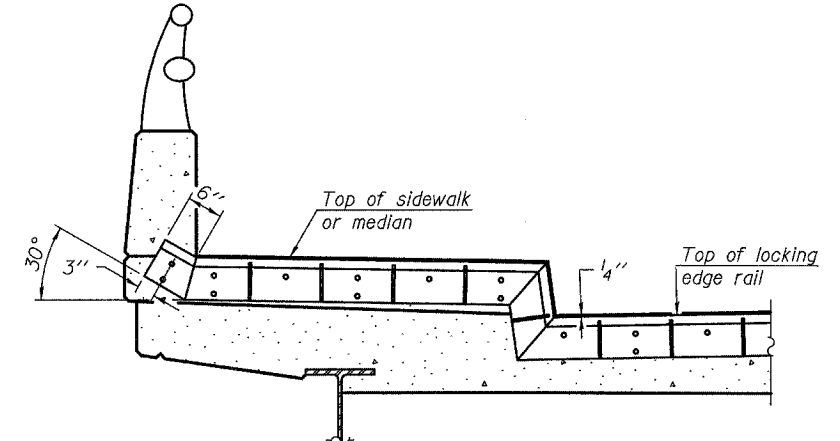
The inside of the locking edge rail groove shall be free of weld residue.



ANCHOR PLATE  
(for welded rail)



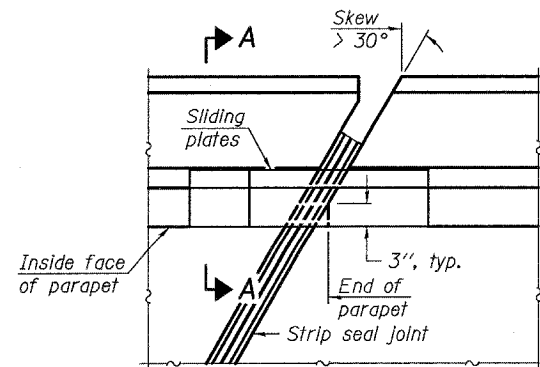
AT PARAPET



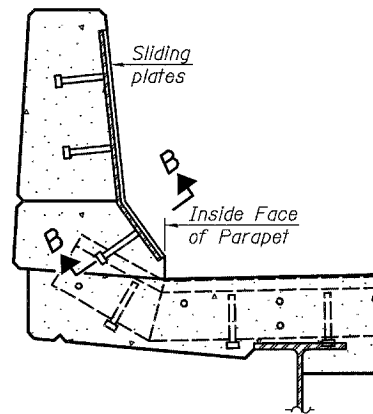
AT SIDEWALK OR MEDIAN

Shorter plates with a single row of studs at 12 inch cts. may be necessary on medians which are shallower than 9 inch. See manufacturer's recommendation.

LOCKING EDGE RAILS



PLAN

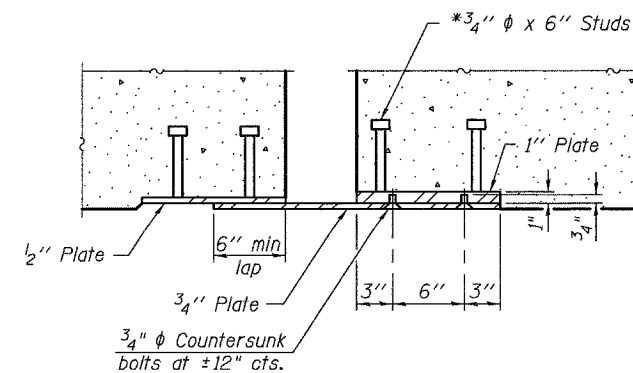


SECTION A-A

POINT BLOCK DETAILS

(for skews > 30 degrees)

TYPICAL END TREATMENTS



SECTION B-B

BILL OF MATERIAL  
(One Structure)

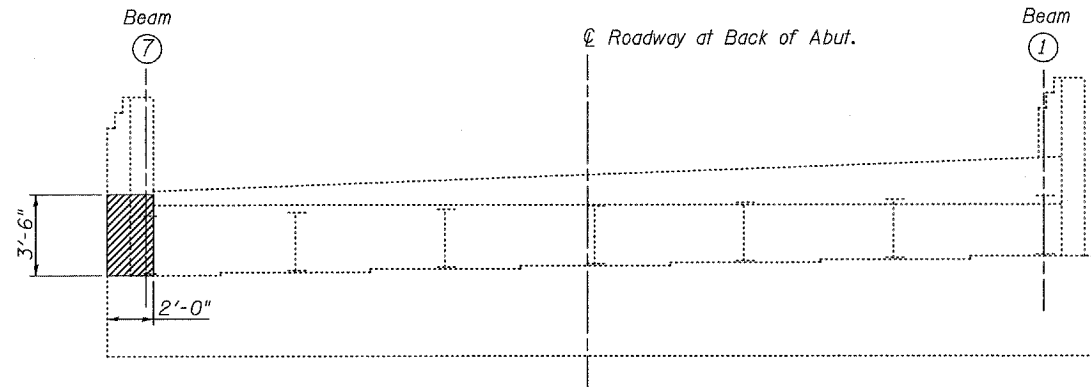
ITEM	UNIT	TOTAL
Preformed Joint Strip Seal	Foot	82

**PREFORMED JOINT STRIP SEAL**  
FAI 64 OVER  
CNIC RAILROAD  
FAI ROUTE 64 SECTION 95-6VB-1  
WASHINGTON COUNTY  
STATION 3263+73.50  
STRUCTURE NO. 095-0061 (EB)  
STRUCTURE NO. 095-0062 (WB)

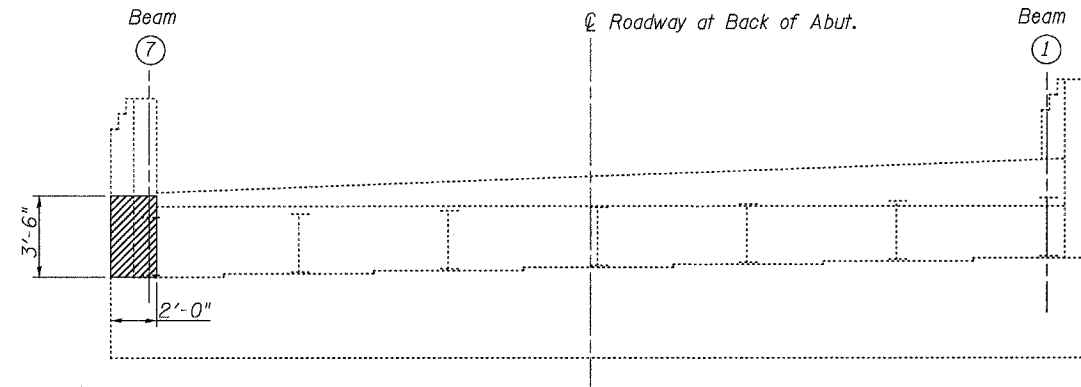
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 5
FAI 64	95-6VB-1	WASHINGTON	23	55	7 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

Contract #76A39



WEST ABUTMENT - SN 095-0061 (EB)



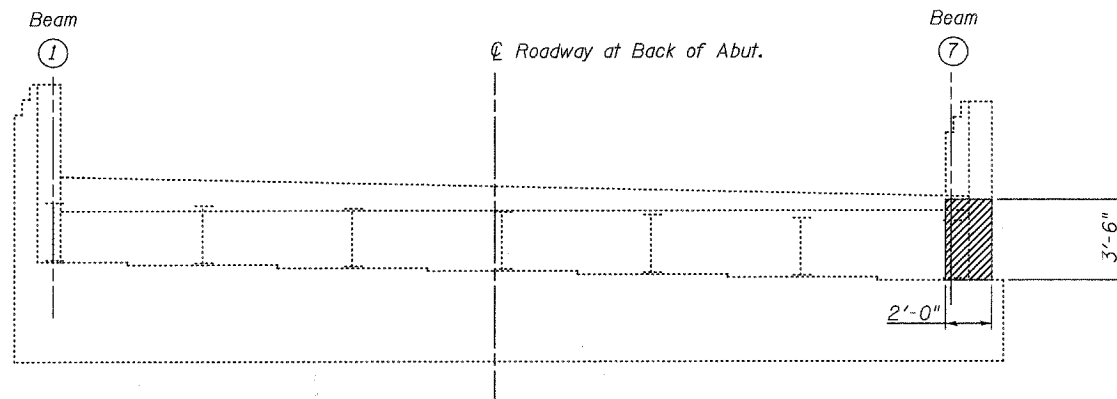
WEST ABUTMENT - SN 095-0062 (WB)

**BILL OF MATERIAL**  
(SN 095-0061)

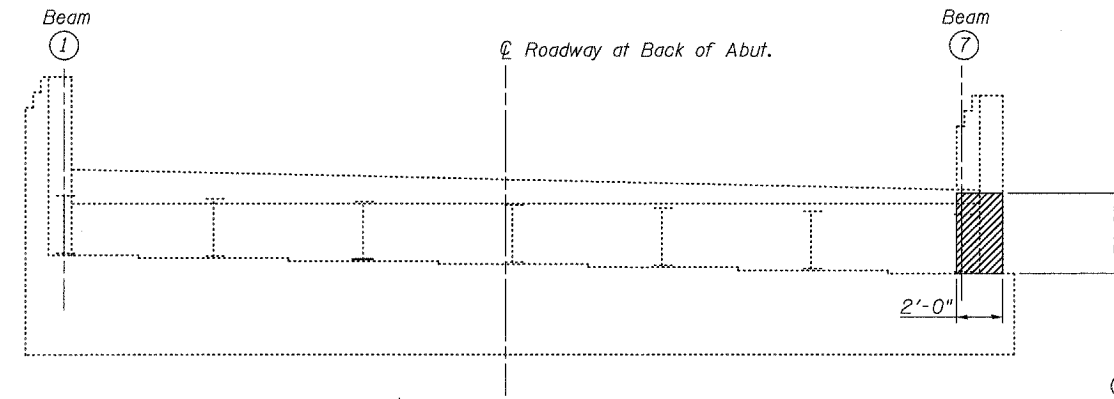
ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth equal to or less than 5")	Sq. Ft.	14
Cleaning Bridge Seats	Sq. Ft.	243
Bridge Seat Sealer	Sq. Ft.	243

**BILL OF MATERIAL**  
(SN 095-0062)

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth equal to or less than 5")	Sq. Ft.	14
Cleaning Bridge Seats	Sq. Ft.	243
Bridge Seat Sealer	Sq. Ft.	243



EAST ABUTMENT - SN 095-0061 (EB)



EAST ABUTMENT - SN 095-0062 (WB)

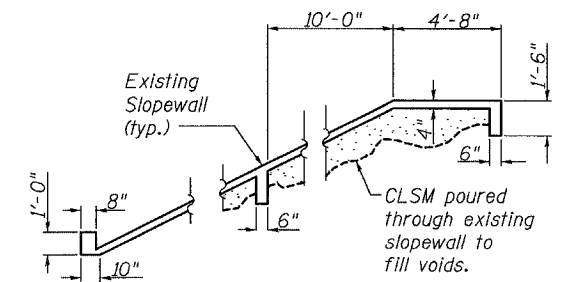
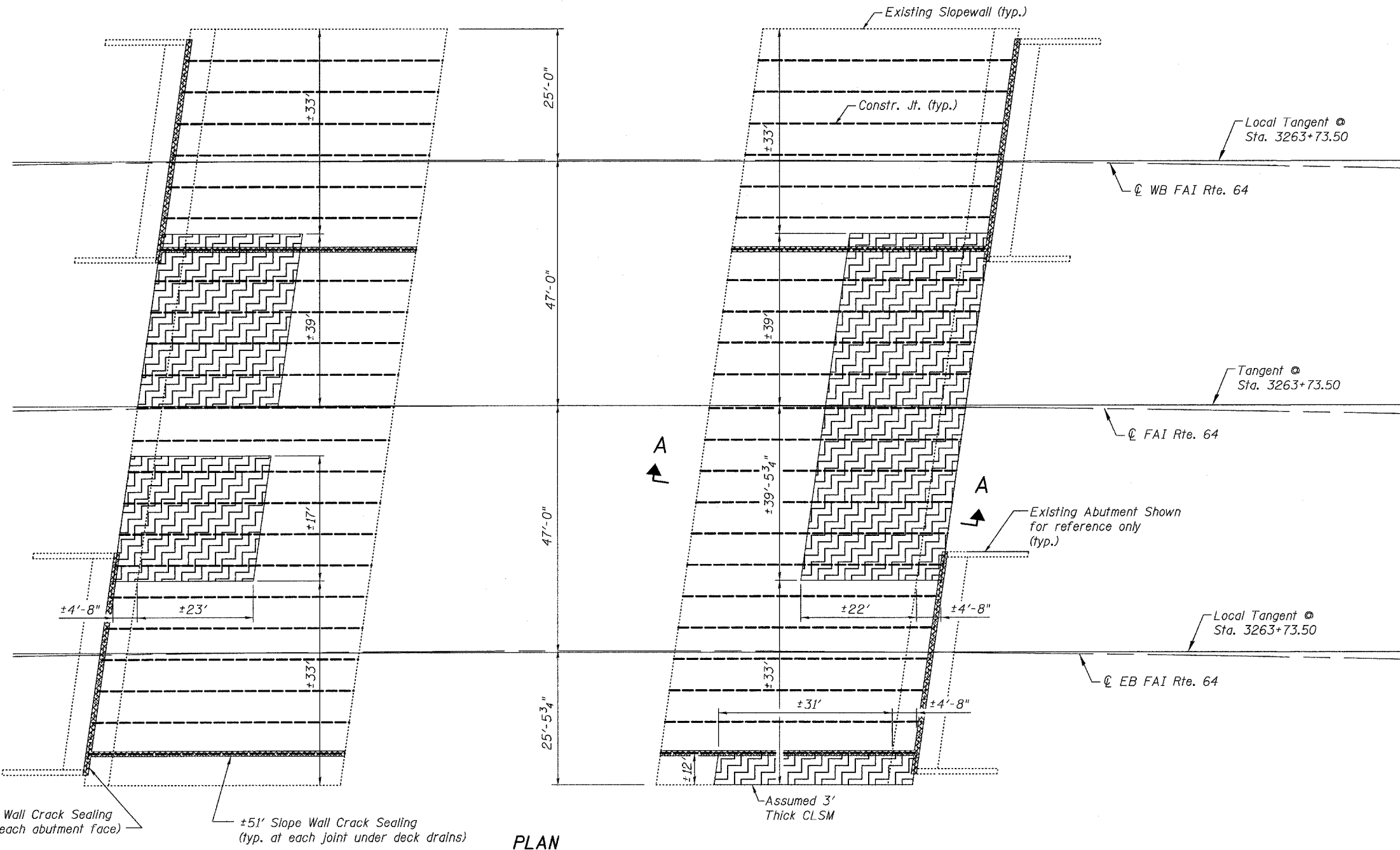
- Notes:
- ① Hatched area indicates approximate area of Structural Repair of Concrete. Exact repairs are to be determined by the Engineer.
  - ② All existing abutment seats shall be cleaned & sealed. Work will be paid for as Cleaning Bridge Seats & Bridge Seat Sealer.

**ABUTMENT CONCRETE REPAIRS**  
FAI 64 OVER  
CNIC RAILROAD  
FAI ROUTE 64 SECTION 95-6VB-1  
WASHINGTON COUNTY  
STATION 3263+73.50  
STRUCTURE NO. 095-0061 (EB)  
STRUCTURE NO. 095-0062 (WB)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 6
FAI 64	95-6VB-1	WASHINGTON	83	56	7 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

Contract #76A39



SECTION A-A

LEGEND

- Controlled Low-Strength Material
- Slope Wall Crack Sealing

- Notes:
- ① Estimated quantities are provided for Controlled Low-Strength Material & Slope Wall Crack Sealing. Actual quantities will be determined by the Engineer in the field as work progresses.
  - ② All Controlled Low-Strength Material (CLSM) areas assume a thickness of 1' unless noted otherwise.
  - ③ CLSM shall be placed so that all voids below the slopewall are filled to the point at which the material protrudes from the joints/cracks. The joints/cracks shall be struck off to prevent the collection of water behind or along the joint/crack to the satisfaction of the Engineer.
  - ④ CLSM shall be poured through a hole in the slopewall at a location determined by the Engineer.
  - ⑤ All dimensions shown in plan are along the length of the slopewall.

BILL OF MATERIAL  
(SN 095-0061)

ITEM	UNIT	TOTAL
Controlled Low-Strength Material	Cu. Yd.	104
Slope Wall Crack Sealing	Foot	188

BILL OF MATERIAL  
(SN 095-0062)

ITEM	UNIT	TOTAL
Controlled Low-Strength Material	Cu. Yd.	78
Slope Wall Crack Sealing	Foot	188

**SLOPEWALL REPAIRS**  
FAI 64 OVER  
CNIC RAILROAD  
FAI ROUTE 64 SECTION 95-6VB-1  
WASHINGTON COUNTY  
STATION 3263+73.50  
STRUCTURE NO. 095-0061 (EB)  
STRUCTURE NO. 095-0062 (WB)



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO. FAI 64	SECTION 95- 6VB-1	COUNTY WASHINGTON	TOTAL SHEETS 83	SHEET NO. 57	SHEET NO. 7 7 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-		

Contract #76A39

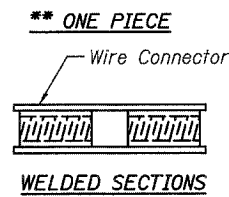
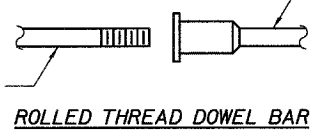
**NOTES**

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.  
Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.  
All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.  
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.  
Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- ① Minimum Capacity (Tension in kips) =  $1.25 \times f_y \times A_t$
  - ② Minimum \*Pull-out Strength (Tension in kips) =  $0.66 \times f_y \times A_t$
- Where  $f_y$  = Yield strength of lapped reinforcement bars in ksi.  
 $A_t$  = Tensile stress area of lapped reinforcement bars.  
\* = 28 day concrete

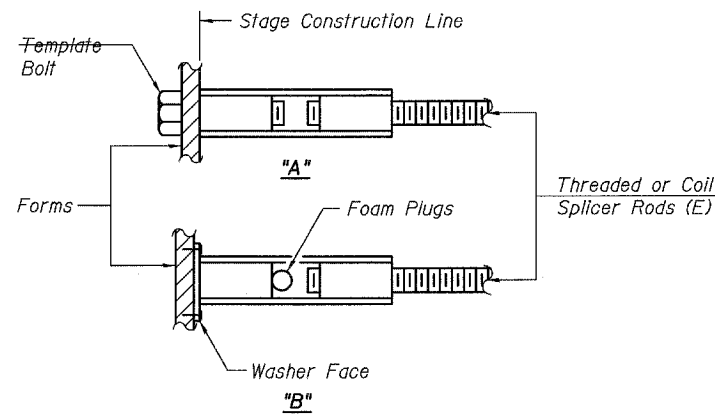
BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-0"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8

The diameter of this part is equal or larger than the diameter of bar spliced.



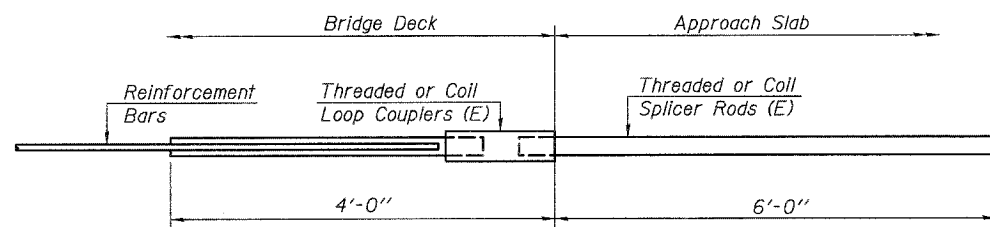
**BAR SPLICER ASSEMBLY ALTERNATIVES**

\*\* Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



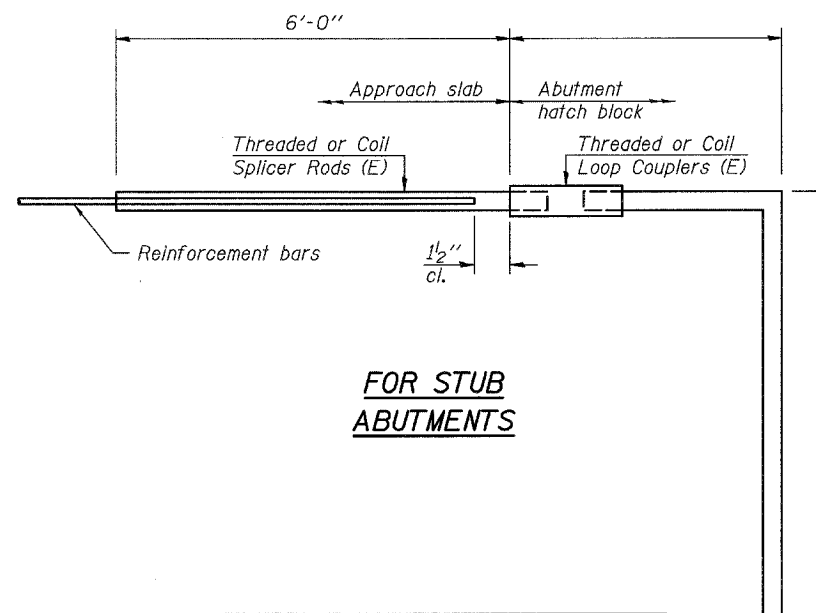
**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
(E) : Indicates epoxy coating.



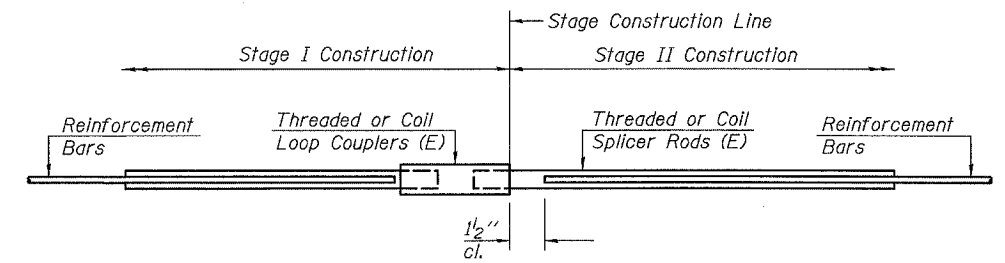
**FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required =



**FOR STUB ABUTMENTS**

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required =



**STANDARD**  
(One Structure)

Bar Size	No. Assemblies Required	Location
#5	24	Slab
#6	6	Backwall

**BAR SPLICER ASSEMBLY DETAILS**

FAI 64 OVER  
CNIC RAILROAD  
FAI ROUTE 64 SECTION 95-6VB-1  
WASHINGTON COUNTY  
STATION 3263+73.50  
STRUCTURE NO. 095-0061 (EB)  
STRUCTURE NO. 095-0062 (WB)

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 1
FAI 64	*	Washington	63	58	10 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT	Contract # *95-(5, 6) RS-1		

**GENERAL NOTES**

No field welding is permitted except as specified in the contract documents.

Reinforcement bars shall conform to the requirements of ASTM A 706 GR 60 (IL Modified). See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by an individual acceptable to the Engineer. Any cracks that can not be removed by grinding 1/4 in. deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.

Concrete Sealer shall be applied to the deck surface, parapet faces and abutment seats.

Cleaning and field painting of structural steel shall be done under a separate painting contract.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

The existing abutment seats shall be cleaned according to the Special Provisions and paid for at the contract unit price for "Cleaning Bridge Seats".

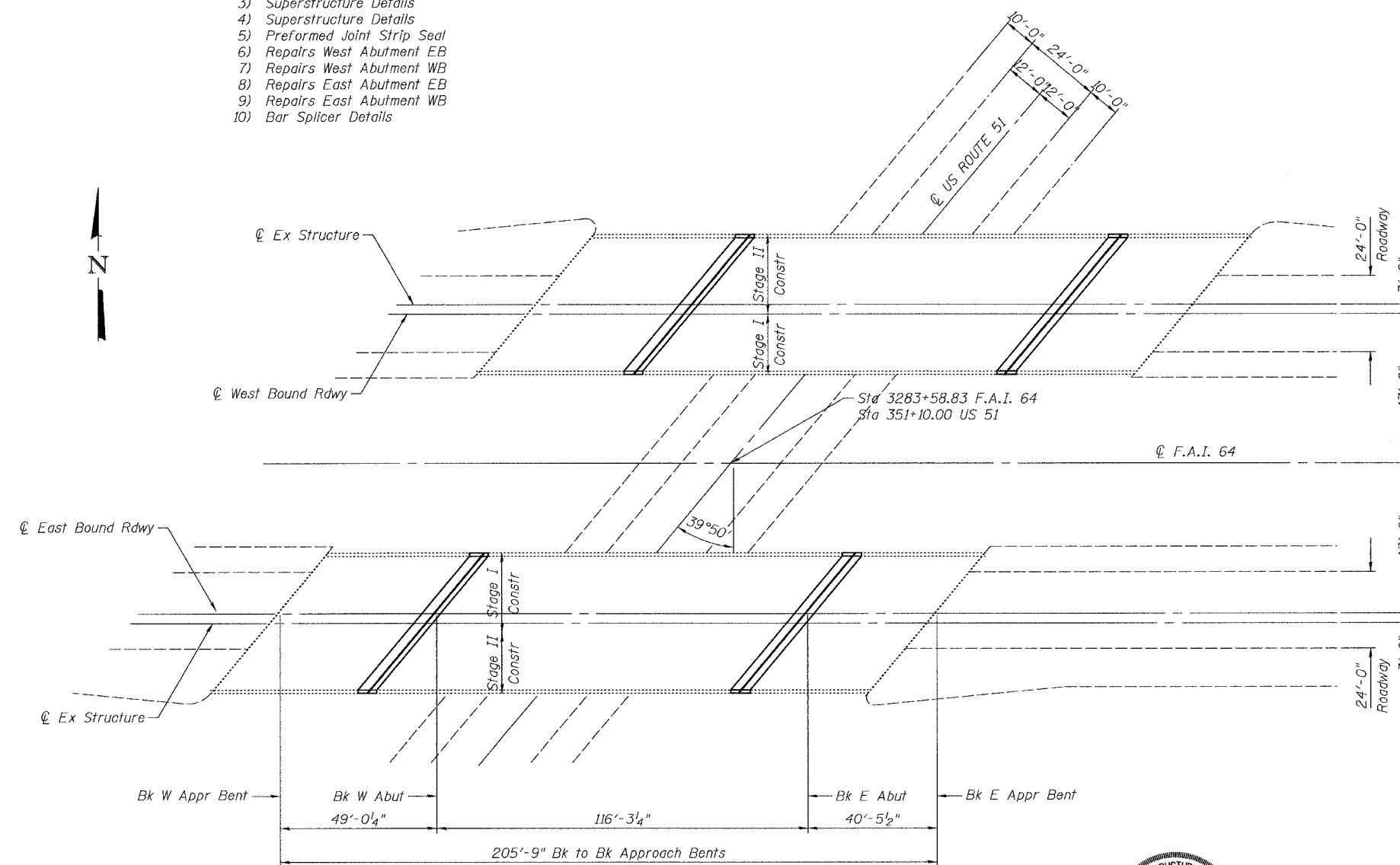
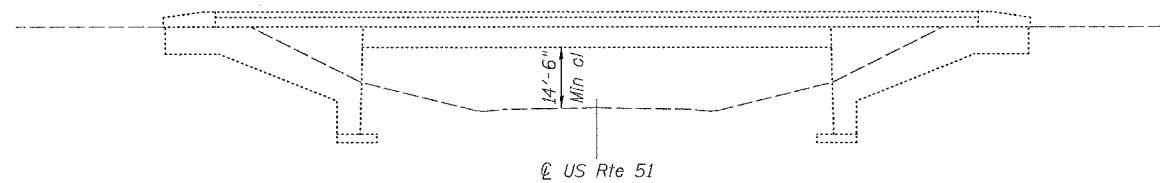
**TOTAL BILL OF MATERIAL**

ITEM	UNIT	SUPER	SUB	TOTAL
Stone Riprap Class A3	Sq Yd		12	12
Filter Fabric	Sq Yd		12	12
Concrete Removal	Cu Yd	68.7		68.7
Concrete Superstructure	Cu Yd	68.7		68.7
Bridge Deck Grooving	Sq Yd	101		101
Reinforcement Bars, Epoxy Coated	Pound	6470		6470
Bar Splicers	Each	68		68
Preformed Joint Strip Seal	Foot	219		219
Concrete Sealer	Sq Ft	19602		19602
Epoxy Crack Injection	Foot		10	10
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft		154	154
Cleaning Bridge Seats	Sq Ft	563		563

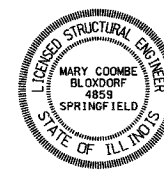
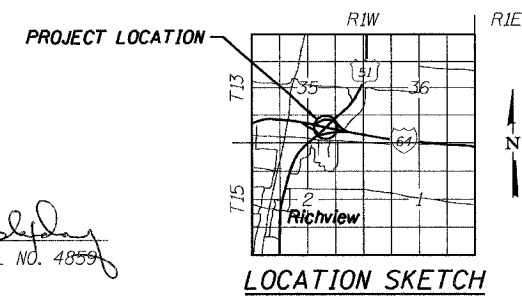
**INDEX OF SHEETS**

- 1) General Plan
- 2) Stage Construction and Concrete Removal
- 3) Superstructure Details
- 4) Superstructure Details
- 5) Preformed Joint Strip Seal
- 6) Repairs West Abutment EB
- 7) Repairs West Abutment WB
- 8) Repairs East Abutment EB
- 9) Repairs East Abutment WB
- 10) Bar Splicer Details

**ELEVATION**



**PLAN**



*Mary Coombe Bloxdorf*  
 ILLINOIS STRUCTURAL NO. 4859  
 EXPIRES 11/30/08  
 DATE: 8/22/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE: GENERAL PLAN AND ELEVATION

PROJECT: F.A.I. 64 OVER US ROUTE 51  
 SECTION 95-(5, 6) RS-1  
 WASHINGTON COUNTY  
 STATION 3283+58.83  
 SN 095-0063 (E.B.) & 095-0064 (W.B.)

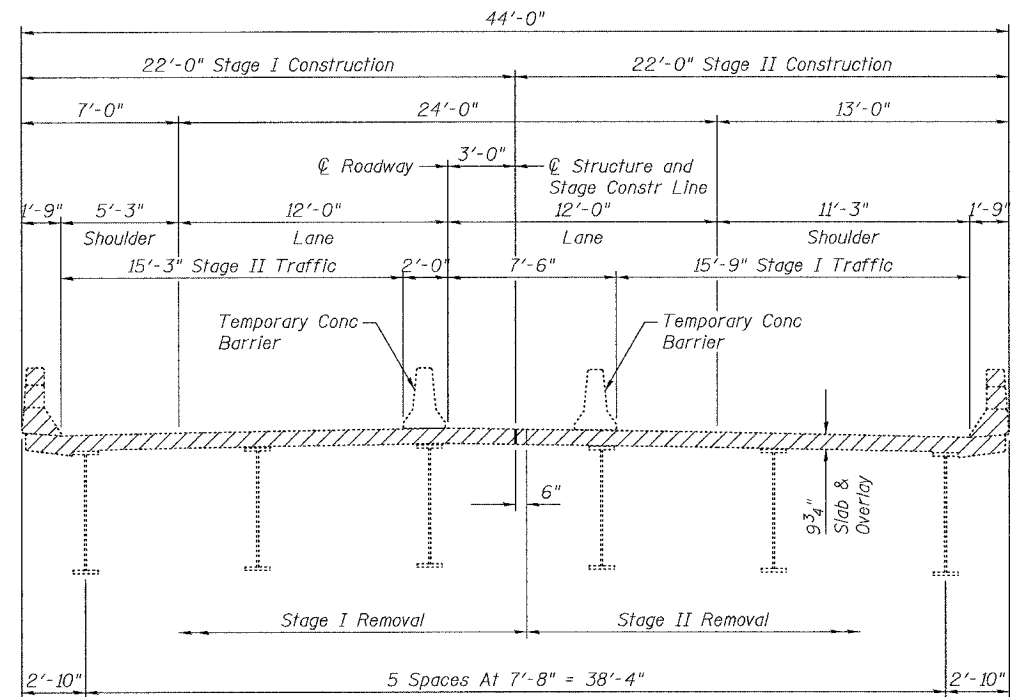
PROJECT NO. 06001-6  
 SCALE: AS SHOWN  
 DATE: 8/21/07  
 DRAWN BY: CFC  
 CHECKED BY: MCB  
 DRAWING NO. 1

**COOMBE-BLOXDORF P.C.**  
 Engineers / Land Surveyors  
 Springfield, Illinois  
 Design Firm License No. 184-002703

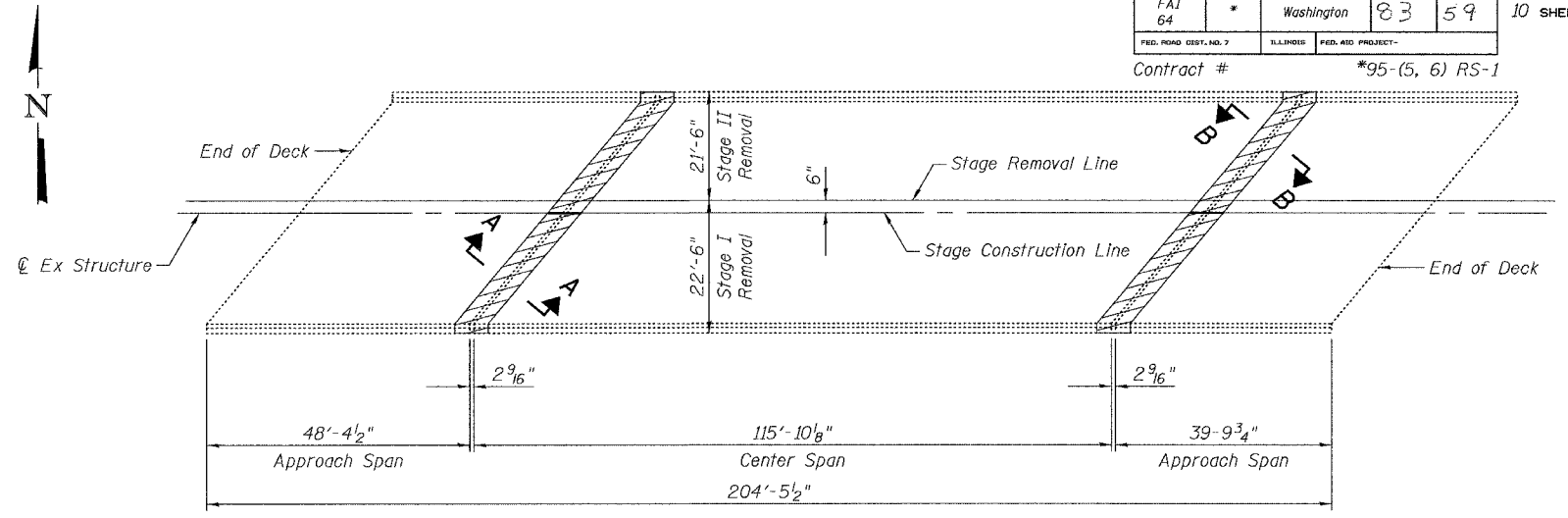
OF 10 SHTS

PLOT DATE = 08/21/2007  
 FILE NAME = \\p1b1-1\cadd\01-general-plan.dgn  
 USER NAME = TFC

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 2 10 SHEETS
FAI 64	#	Washington	83	59	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT	Contract # *95-(5, 6) RS-1		

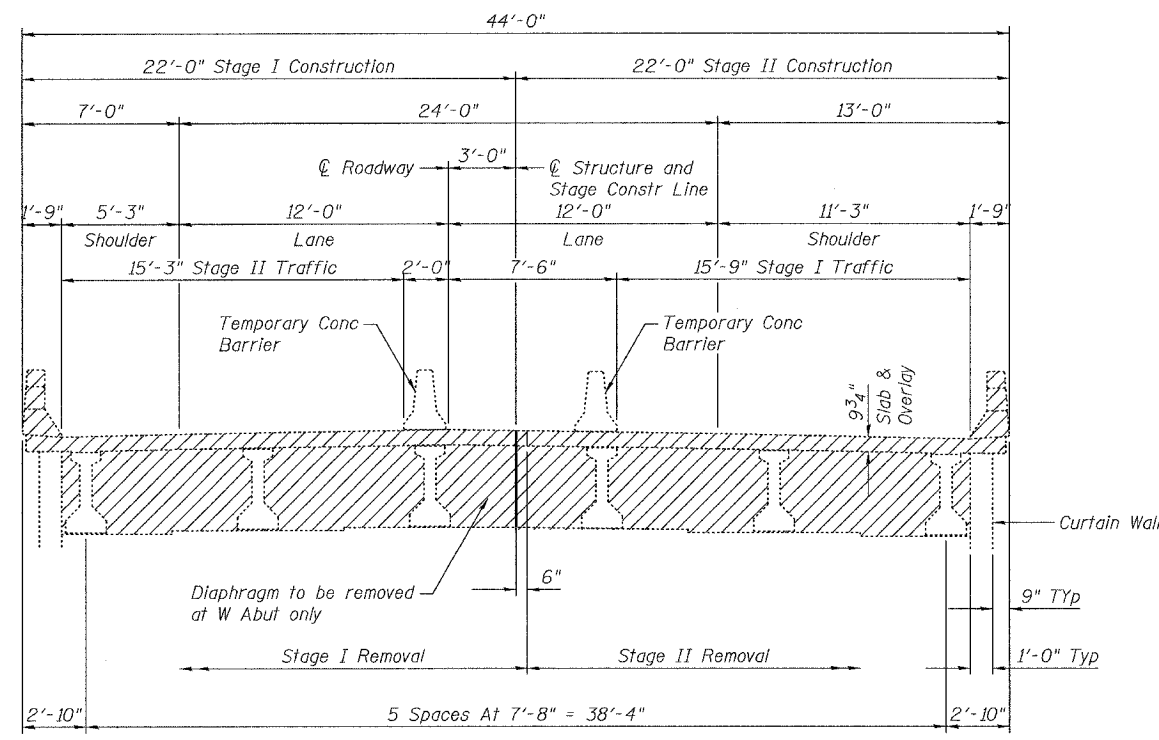


**CROSS SECTION THRU CENTER SPAN AT END OF DECK**  
(Looking West - WB Bridge)  
(Looking East - EB Bridge)

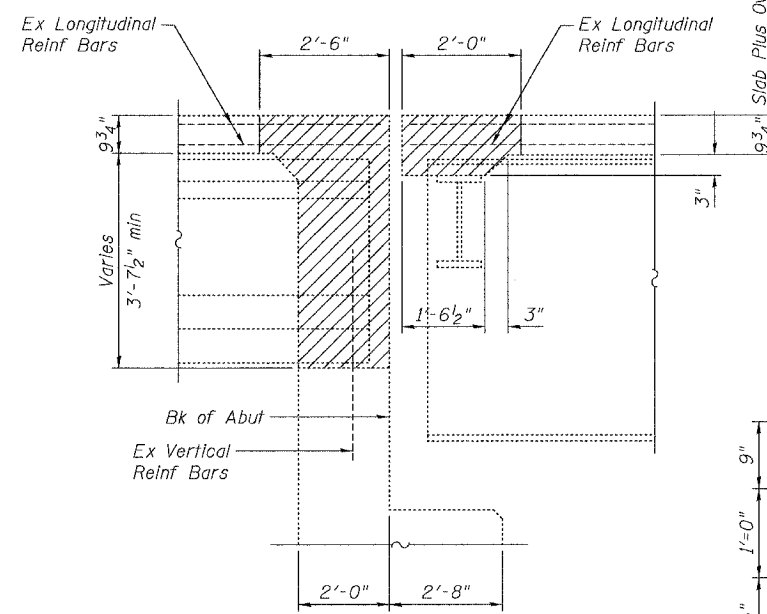


**PLAN**  
(Showing Concrete Removal)  
(Staging for WB Structure, EB is Opposite)

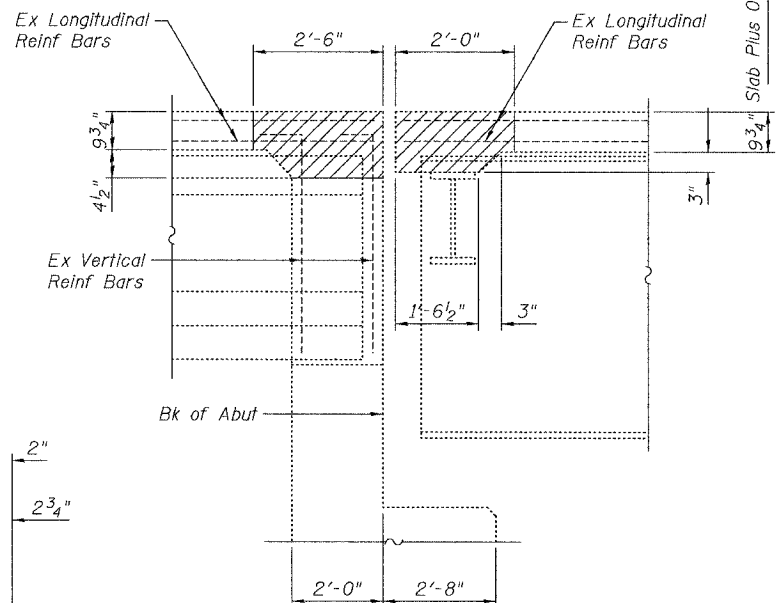
Note:  
Staging is for information only, to be coordinated with roadway staging.



**CROSS SECTION THRU APPROACH SLAB AT DIAPHRAGM**  
(Looking West - WB Bridge)  
(Looking East - EB Bridge)

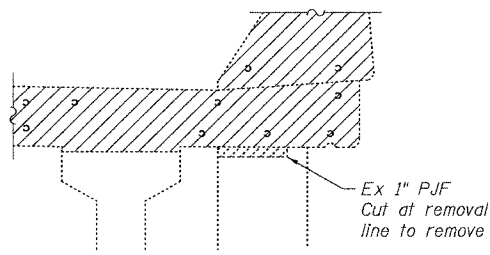


**SECTION A-A**  
(dim at right angles)



**SECTION B-B**  
(dim at right angles)

Notes:  
Extreme care is required for Concrete Removal around the PPC I-Beams so as to avoid damage to the beam.  
Hatched areas indicate "Concrete Removal".  
Existing Longitudinal and vertical reinforcement bars extending into the new construction shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with "Concrete Removal".

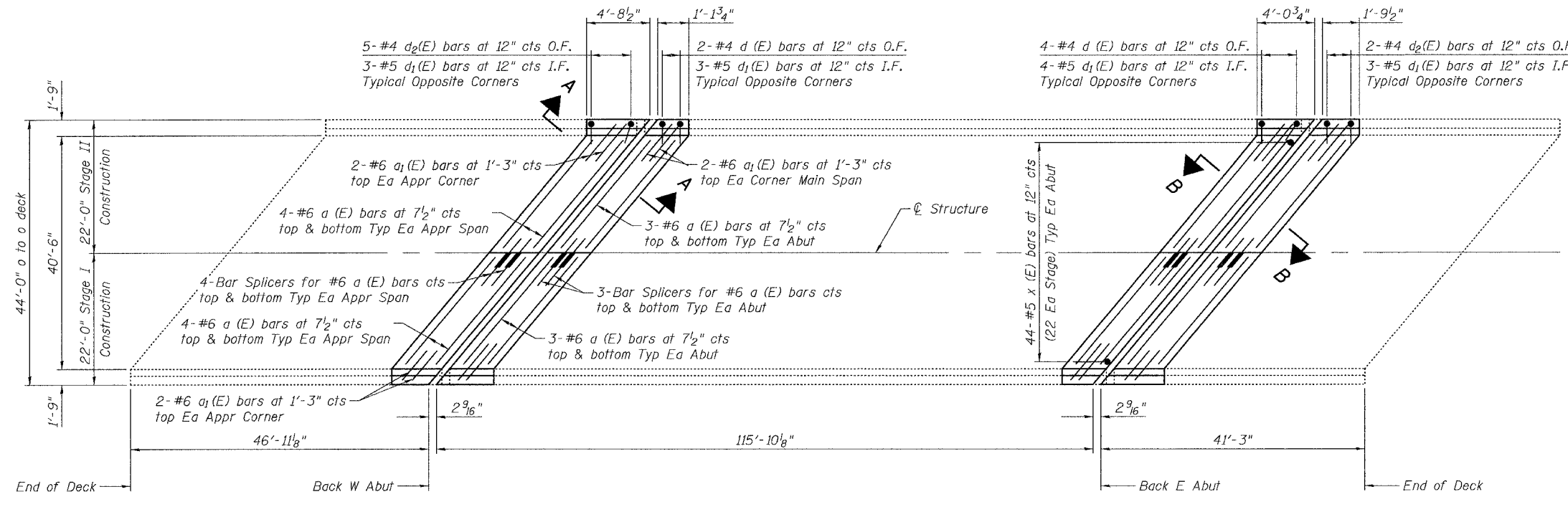


**DETAIL A**  
(Curb on Approach Span)

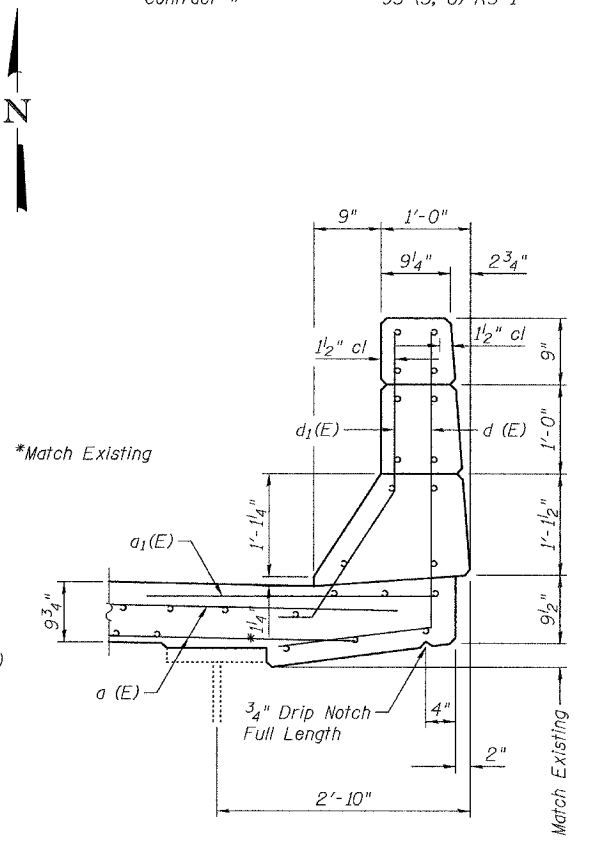
**CURB SECTION MAIN SPAN**  
(Approach similar except watertable See Detail A)

PLOT DATE = 08/21/2007  
 PLOT SCALE = 1/8" = 1'-0"  
 USER NAME = TFC

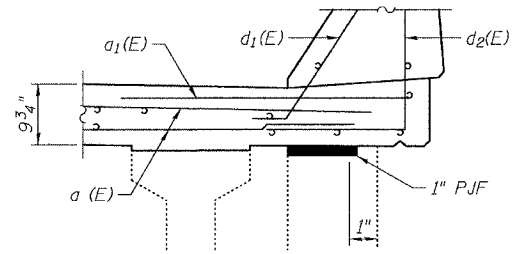
ILLINOIS DEPARTMENT OF TRANSPORTATION	
SHEET TITLE STAGE CONSTRUCTION AND CONCRETE REMOVAL	
PROJECT F.A.I. 64 OVER US ROUTE 51 SECTION 95-(5, 6) RS-1 WASHINGTON COUNTY STATION 3283+58.83 SN 095-0063 (E.B.) & 095-0064 (W.B.)	PROJECT NO. 06001-6 DATE 8/21/07 DRAWN BY CFC CHECKED BY MCB DRAWING NO.
<b>COOMBE-BLOXDORF P.C.</b> Engineers / Land Surveyors Springfield, Illinois Design Firm License No. 184-002703	
2 OF 10 SHTS	



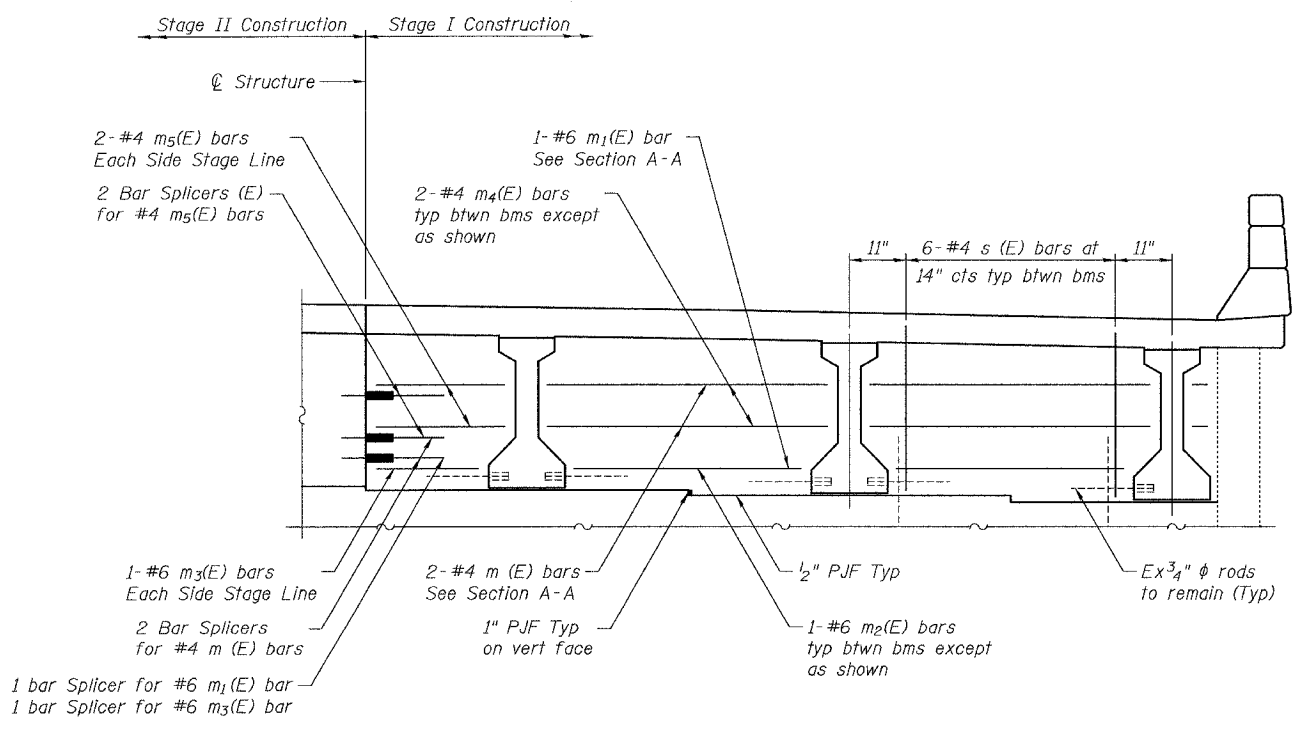
**PLAN**  
(WB Staging Shown, EB Staging Opposite)



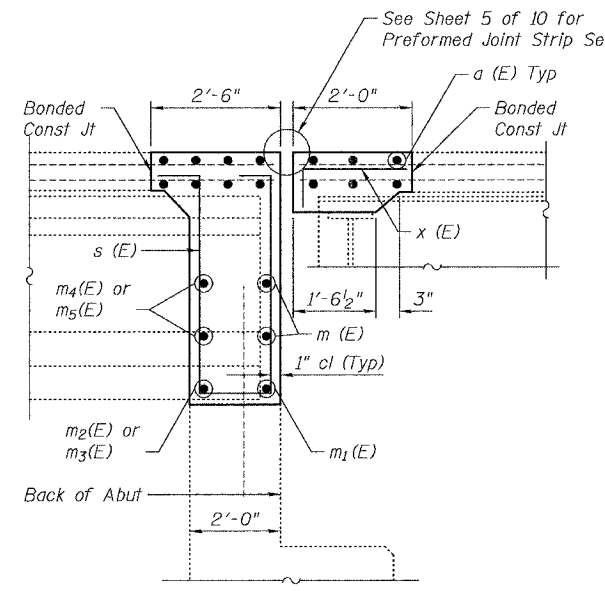
**CURB SECTION MAIN SPAN**  
(Approach Span Similar Except as Shown in Detail A)



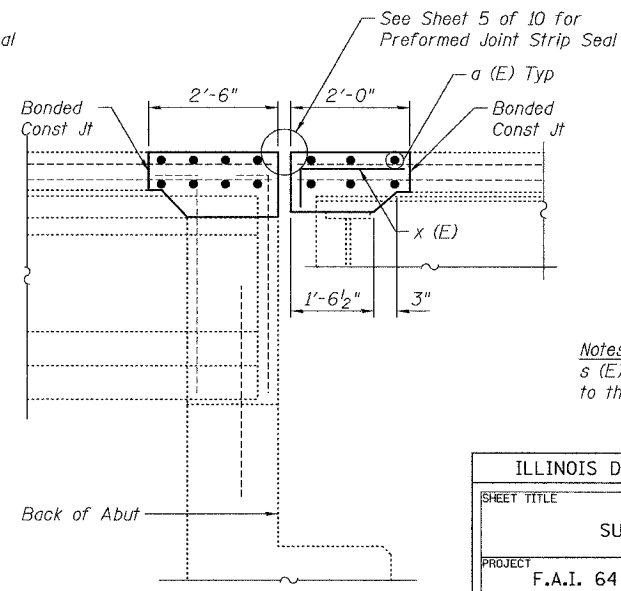
**DETAIL A**  
(Curb on Approach Span)



**DIAPHRAGM AT WEST ABUTMENT**  
(Looking East - Wb Bridge, Looking West - EB Bridge)



**SECTION A-A**  
(Dim at right angles)

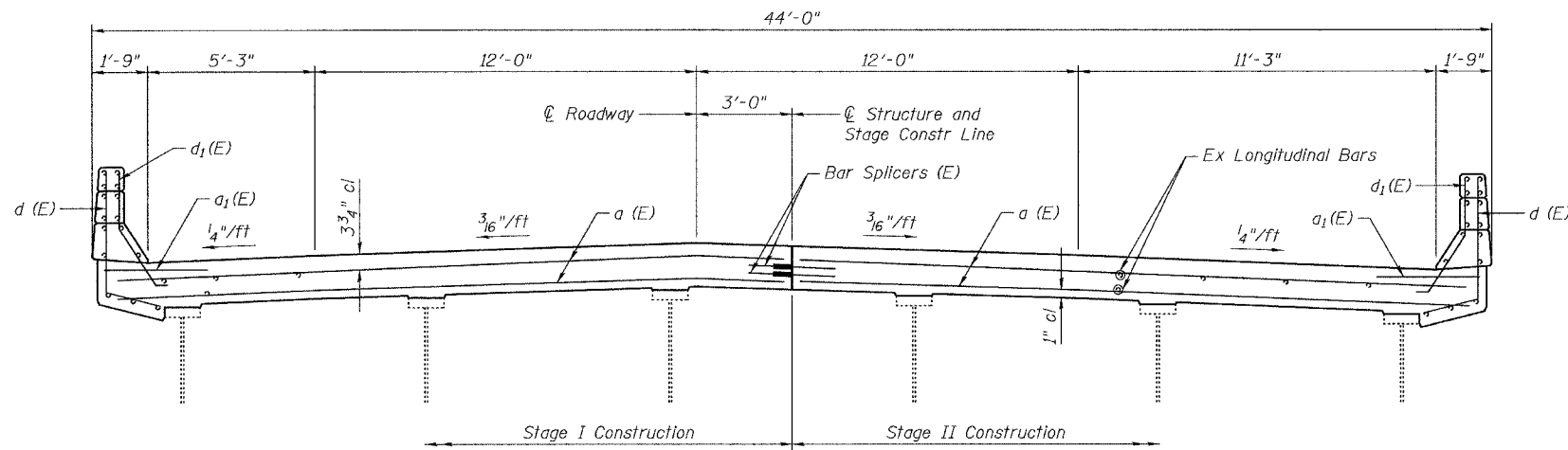


**SECTION B-B**  
(Dim at right angles)

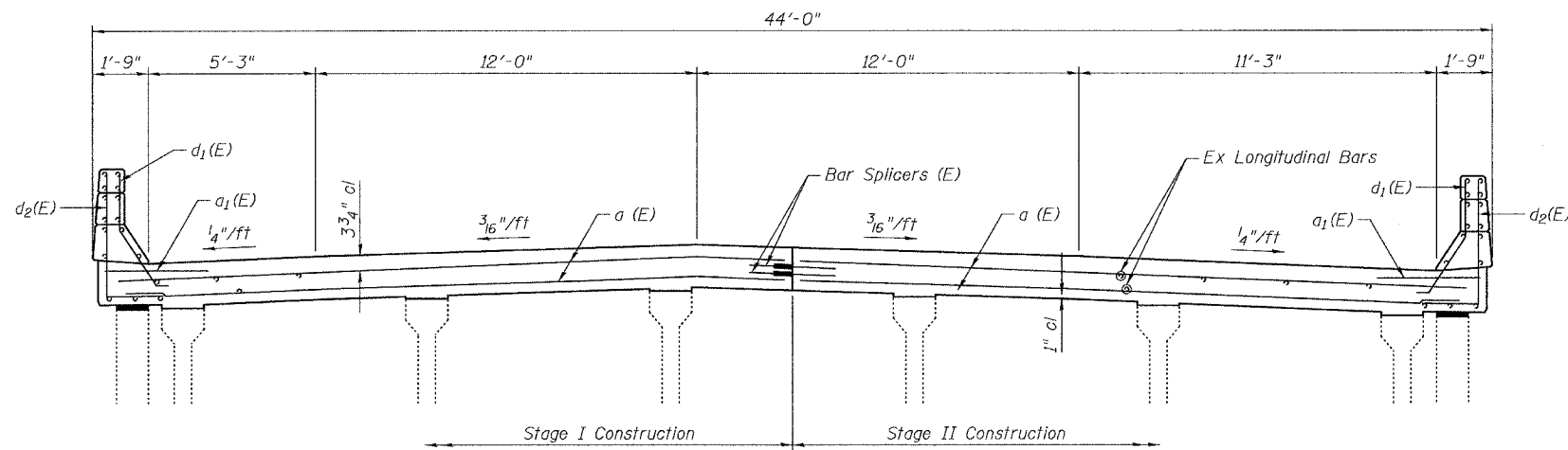
Notes:  
s (E) bars shall be placed parallel to the centerline of the beams

ILLINOIS DEPARTMENT OF TRANSPORTATION		
SHEET TITLE SUPERSTRUCTURE DETAILS		
PROJECT F.A.I. 64 OVER US ROUTE 51 SECTION 95-(5, 6) RS-1 WASHINGTON COUNTY STATION 3283+58.83 SN 095-0063 (E.B.) & 095-0064 (W.B.)	PROJECT NO. 06001-6	DATE 8/21/07
DESIGNED BY CFC	CHECKED BY MCB	DRAWING NO. 3
COOMBE-BLOXDORF P.C. Engineers / Land Surveyors Springfield, Illinois Design Firm License No. 184-002703		OF 10 SHTS

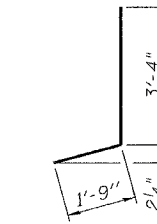
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PLOT SCALE = 1/8" = 1'-0"  
USER NAME = TFC



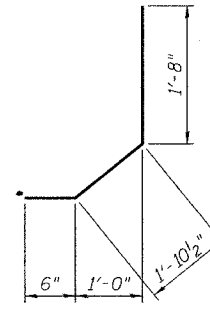
**CROSS SECTION CENTER SPAN**  
(Looking West - WB Bridge, Looking East - EB Bridge)



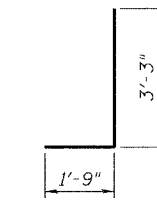
**CROSS SECTION APPROACH SPAN**  
(Looking West - WB Bridge, Looking East - EB Bridge)



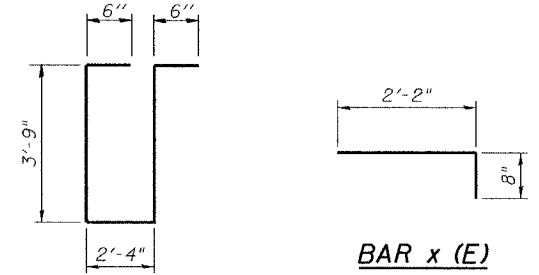
**BAR d (E)**



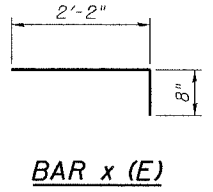
**BAR d1 (E)**



**BAR d2 (E)**



**BAR s (E)**

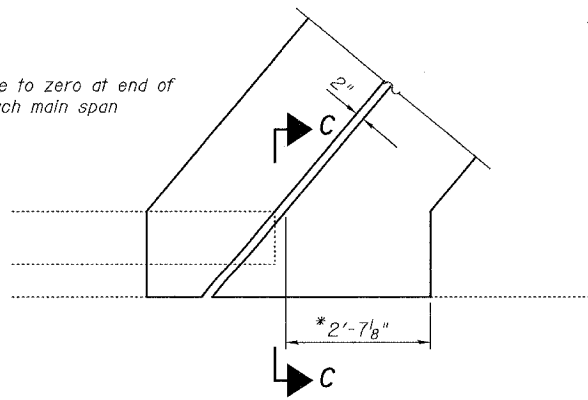


**BAR x (E)**

**BILL OF MATERIAL  
TWO STRUCTURES**

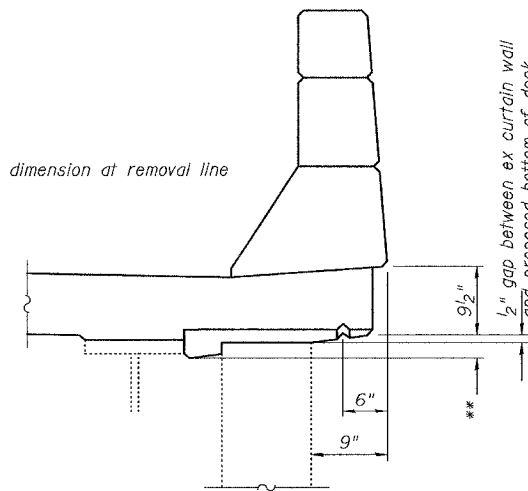
Bar	No.	Size	Length	Shape	
a(E)	112	#6	26'-3"	—	
a1(E)	32	#6	4'-0"	—	
d(E)	24	#4	5'-1"	J	
d1(E)	52	#5	4'-1"	J	
d2(E)	28	#4	5'-0"	J	
m(E)	8	#4	25'-9"	—	
m1(E)	4	#6	25'-9"	—	
m2(E)	8	#6	7'-1"	—	
m3(E)	4	#6	3'-4"	—	
m4(E)	16	#4	8'-10"	—	
m5(E)	8	#4	4'-3"	—	
s(E)	60	#4	10'-10"	L	
x(E)	176	#5	2'-10"	└	
Reinforcement Bars, Epoxy Coated				Pound	6470
Concrete Superstructure				Cu. Yd.	68.7
Bar Splicers				Each	68

\*Taper slope of watertable to zero at end of deck SW & NW Corners each main span  
See Section C-C



**DETAIL A**

\*\*Match existing dimension at removal line



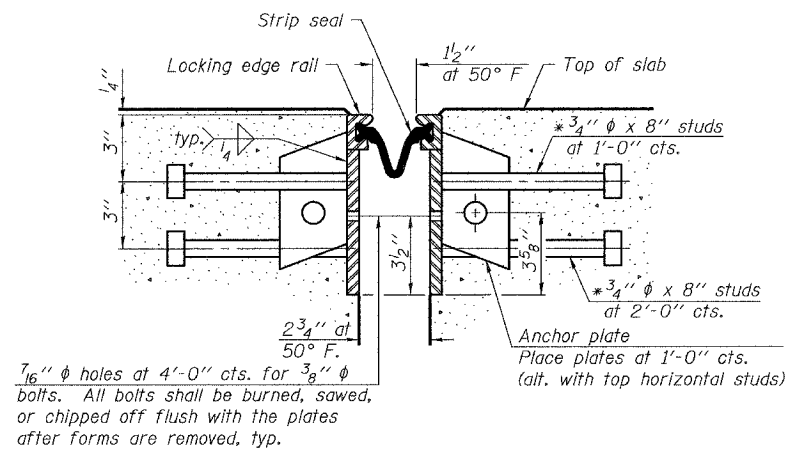
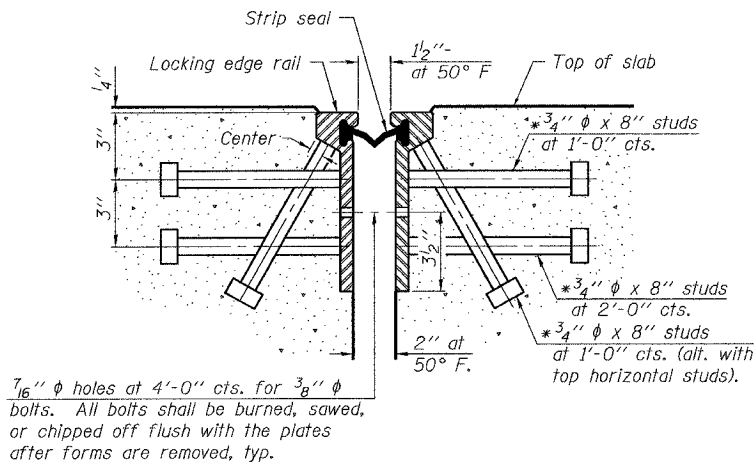
**SECTION C-C**

PLOT DATE = 08/21/2007  
FILE NAME = \\B04-esp\structure-details-2.dgn  
SCALE = 1/8"=1'-0"  
USER NAME = TFC

ILLINOIS DEPARTMENT OF TRANSPORTATION	
SHEET TITLE SUPERSTRUCTURE DETAILS	
PROJECT F.A.I. 64 OVER US ROUTE 51 SECTION 95-(5, 6) RS-1 WASHINGTON COUNTY STATION 3283+58.83 SN 095-0063 (E.B.) & 095-0064 (W.B.)	PROJECT NO. 06001-6 SCALE DATE 8/21/07 DRAWN BY CFC CHECKED BY MCB DRAWING NO.
COOMBE-BLOXDORF P.C. Engineers / Land Surveyors Springfield, Illinois Design Firm License No. 184-002703	
4 OF 10 SHTS	

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

\*\* When joint is fixed, dimension is set at 1 1/2".



Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

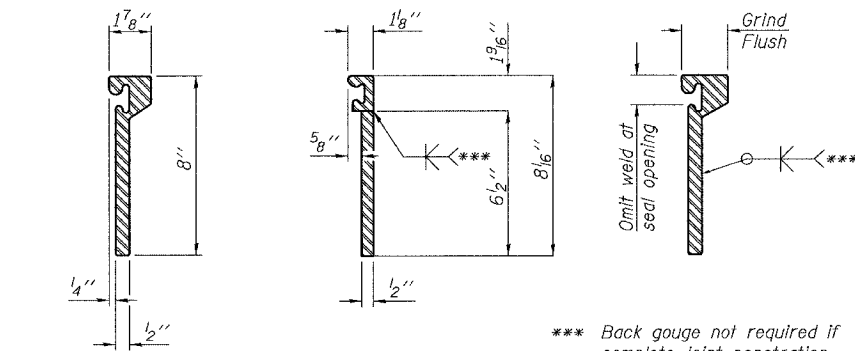
The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 5 10 SHEETS
FAI 64	#	Washington	83	62	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-	Contract # *95-(5, 6) RS-1		

SECTION THRU ROLLED RAIL JOINT

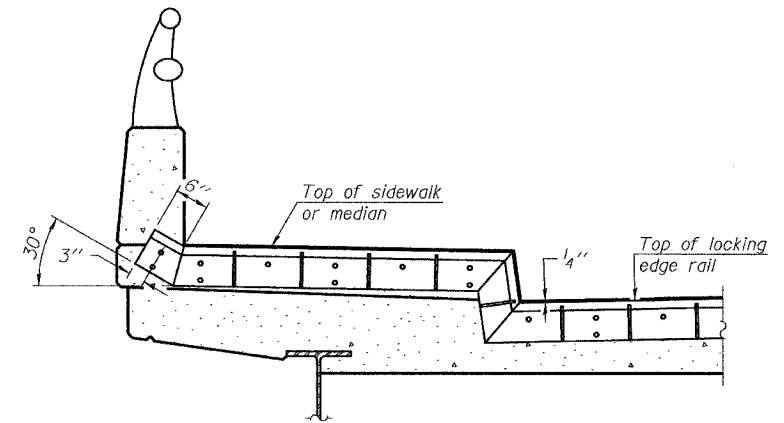
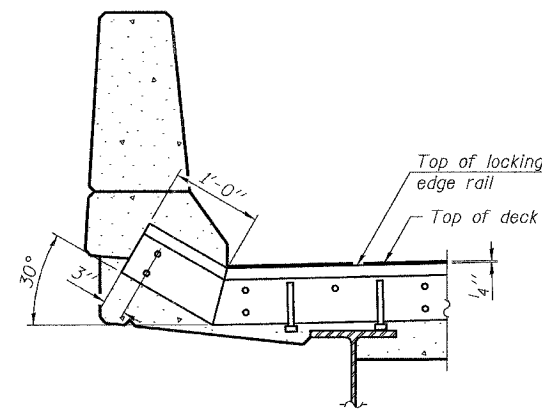
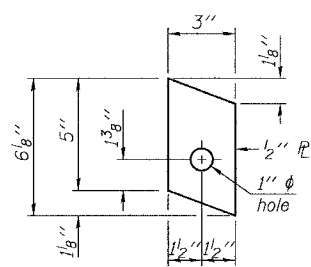
SECTION THRU WELDED RAIL JOINT



\*\*\* Back gouge not required if complete joint penetration is verified by mock-up.

LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.

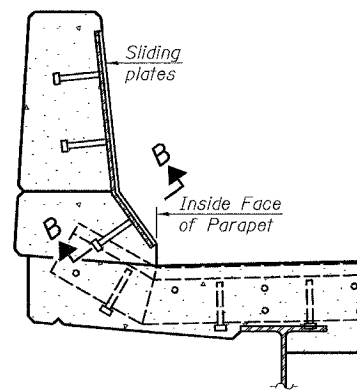
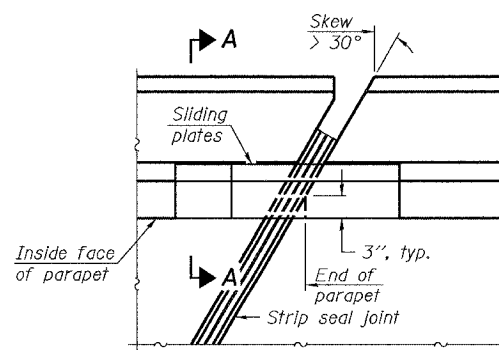


AT SIDEWALK OR MEDIAN

Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.

TYPICAL END TREATMENTS

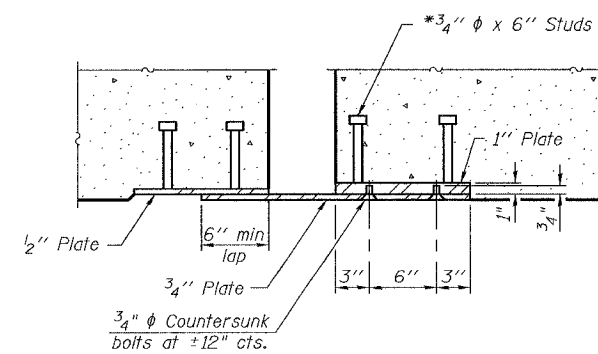
LOCKING EDGE RAILS



SECTION A-A

POINT BLOCK DETAILS

(for skews > 30°)



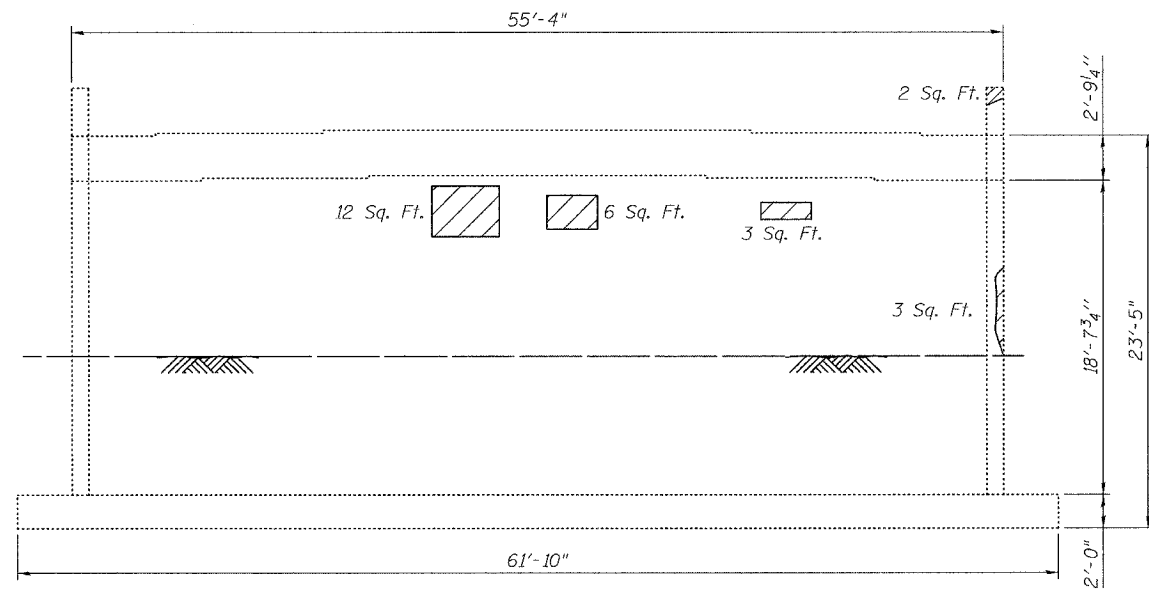
BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	219

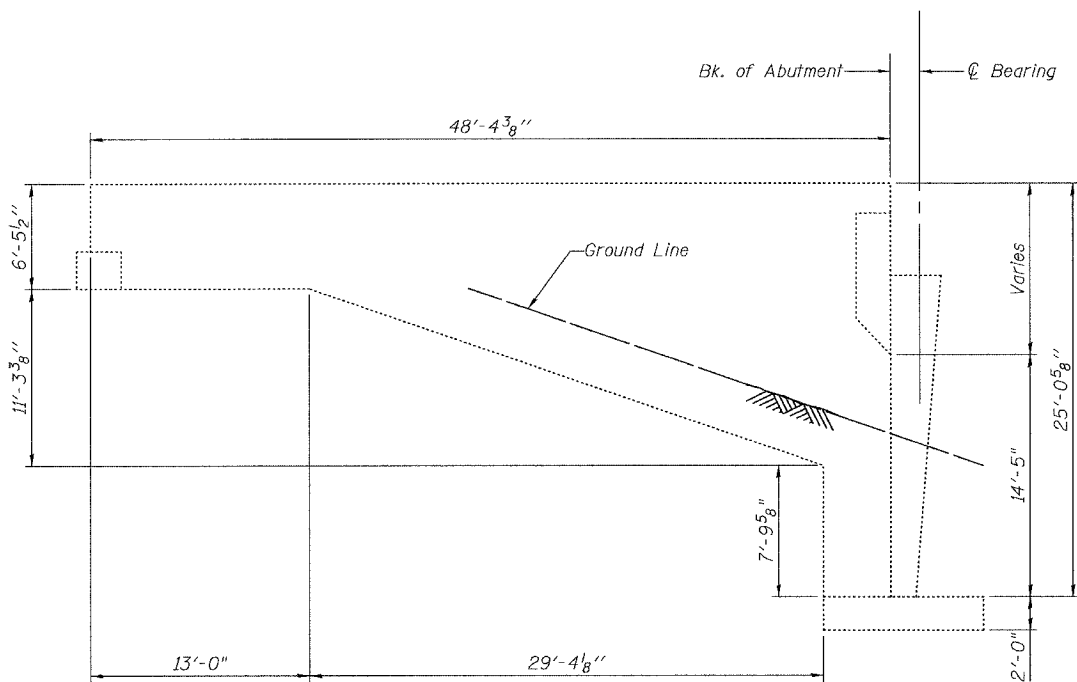
ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE PREFORMED JOINT STRIP SEAL		PROJECT NO. 06001-6
PROJECT FAI RTE 64 OVER US RTE 51 FAI RTE 64 SECTION 95-(5,6)RS-1 WASHINGTON COUNTY STATION 3283+58.83 SN 095-0063 (EB) & 095-0064 (WB)	SCALE DATE 8/21/07 DRAWN BY TFG CHECKED BY KPS/BD/MCB DRAWING NO.	5 OF 10 SHTS
COOMBE-BLOXDORF P.C. Engineers / Land Surveyors Springfield, Illinois Design Firm License No. 184-002703		

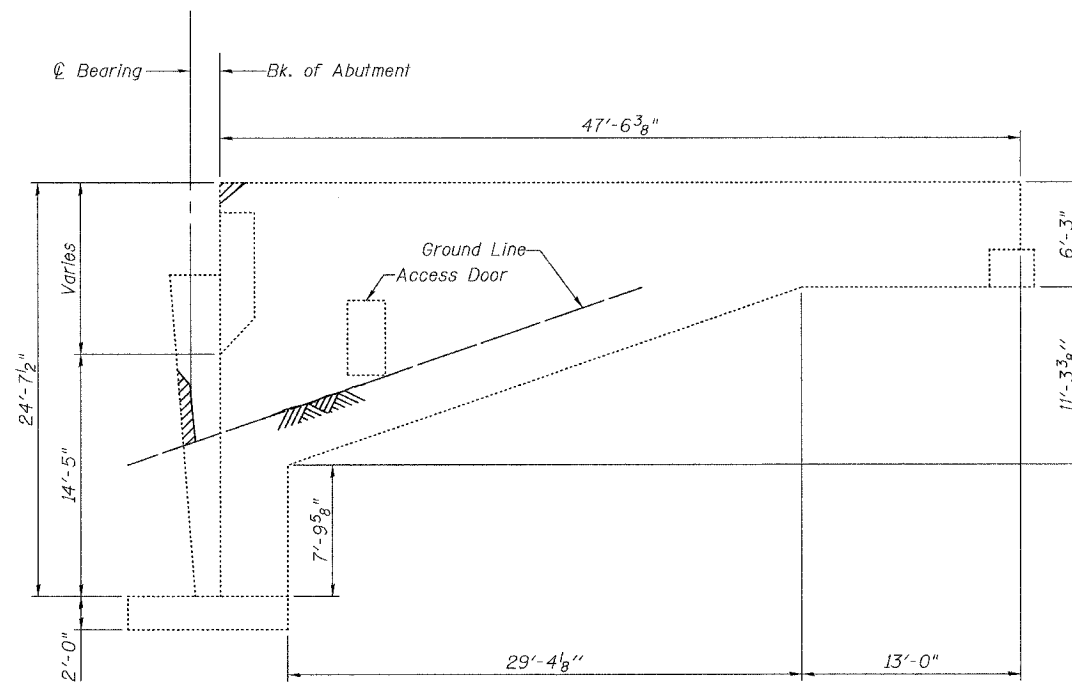
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PLT NAME = kps  
PLOT SCALE = 0.0000 1" = 10'  
USER NAME = TFG



**WEST ABUTMENT**  
(Facing West)



**SOUTH CURTAIN WALL ELEVATION**  
(Facing North)

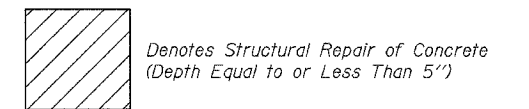


**NORTH CURTAIN WALL ELEVATION**  
(Facing South)

**BILL OF MATERIALS**

Structural Repair of Concrete (Depth Equal to or Less Than 5')	Sq. Ft.	26
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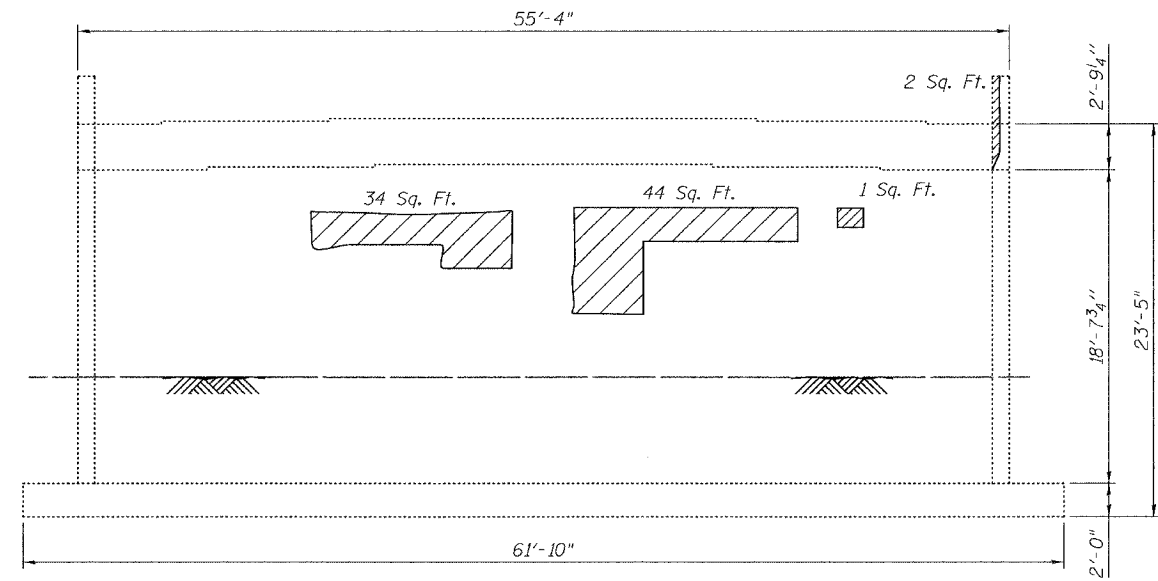
**LEGEND**



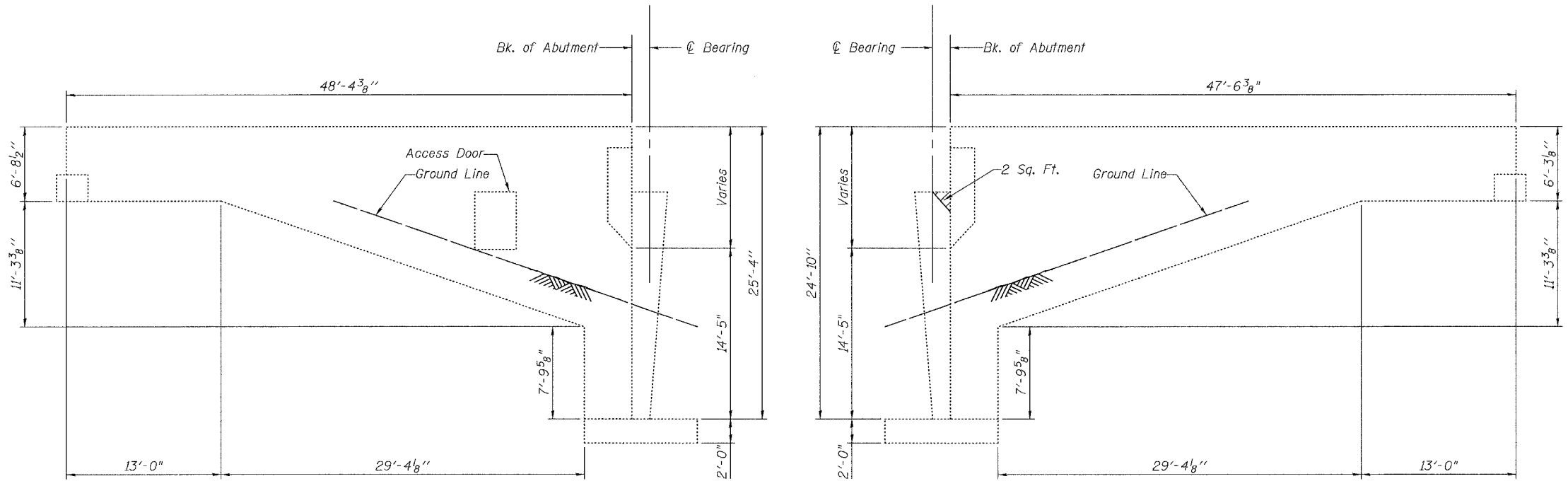
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PLOT SCALE = 5/8" = 1'-0"  
USER NAME = TFC

ILLINOIS DEPARTMENT OF TRANSPORTATION		
SHEET TITLE REPAIRS WEST ABUTMENT EB		
PROJECT FAI RTE 64 OVER US RTE 51 FAI RTE 64 SECTION 95-(5,6)RS-1 WASHINGTON COUNTY STATION 3283+58.83 SN 095-0063 (EB) & 095-0064 (WB)	PROJECT NO. 06001-6	SCALE DATE 8/21/07
DESIGNED BY KPS/BD/MCB	DRAWN BY TFC	CHECKED BY KPS/BD/MCB
COOMBE-BLOXDORF P.C. Engineers / Land Surveyors Springfield, Illinois Design Firm License No. 184-002703		6 OF 10 SHTS

ROUTE NO. FAI 64	SECTION #	COUNTY Washington	SHEET 83	SHEET 64	SHEET NO. 7 10 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT Contract # *95-(5, 6) RS-1		



**WEST ABUTMENT**  
(Facing West)



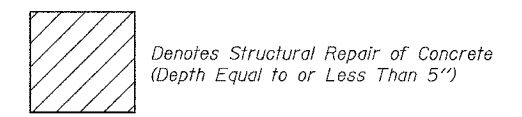
**SOUTH CURTAIN WALL ELEVATION**  
(Facing North)

**NORTH CURTAIN WALL ELEVATION**  
(Facing South)

**BILL OF MATERIAL**

Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq. Ft.	83
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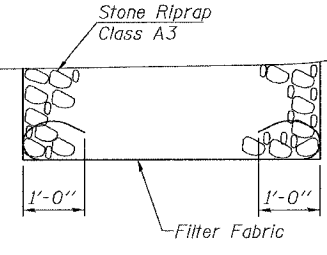
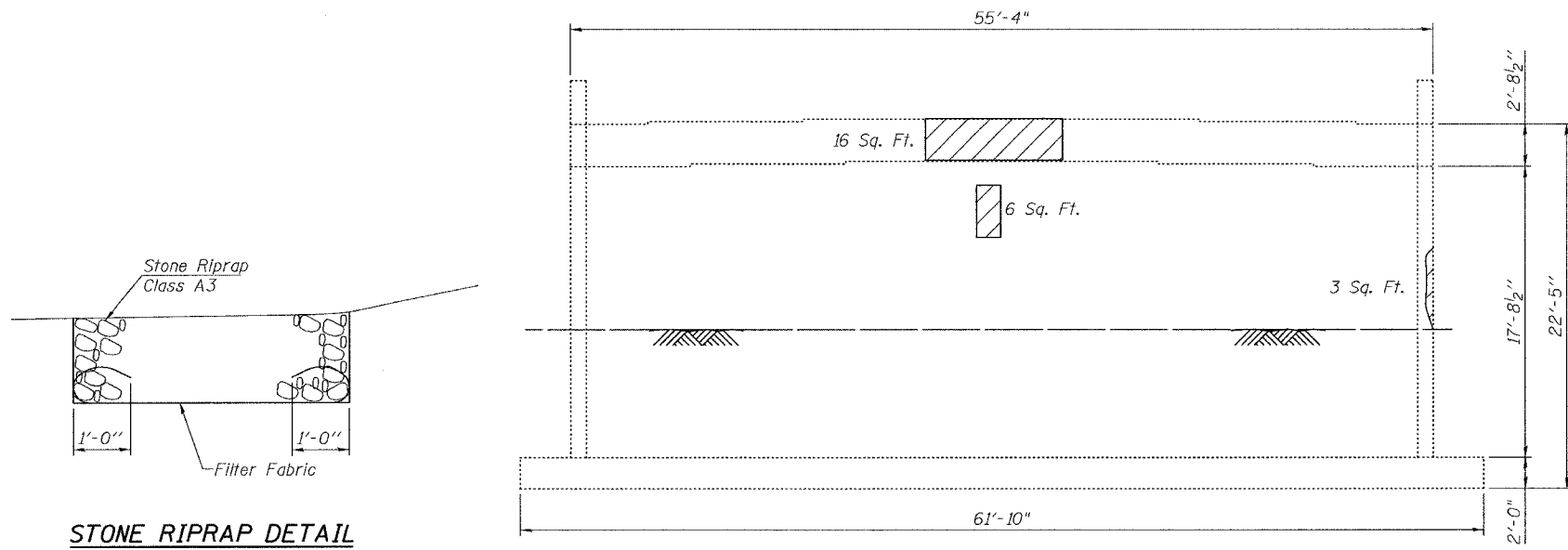
**LEGEND**



ILLINOIS DEPARTMENT OF TRANSPORTATION	
SHEET TITLE REPAIRS WEST ABUTMENT WB	
PROJECT FAI RTE 64 OVER US RTE 51 FAI RTE 64 SECTION 95-(5,6)RS-1 WASHINGTON COUNTY STATION 3283+58.83 SN 095-0063 (EB) & 095-0064 (WB)	PROJECT NO. 06001-6 SCALE DATE 8/21/07 DRAWN BY TFG CHECKED BY KPS/BD/MCB DRAWING NO.
COOMBE-BLOXDORF P.C. Engineers / Land Surveyors Springfield, Illinois Design Firm License No. 184-002703	
7 OF 10 SHTS	

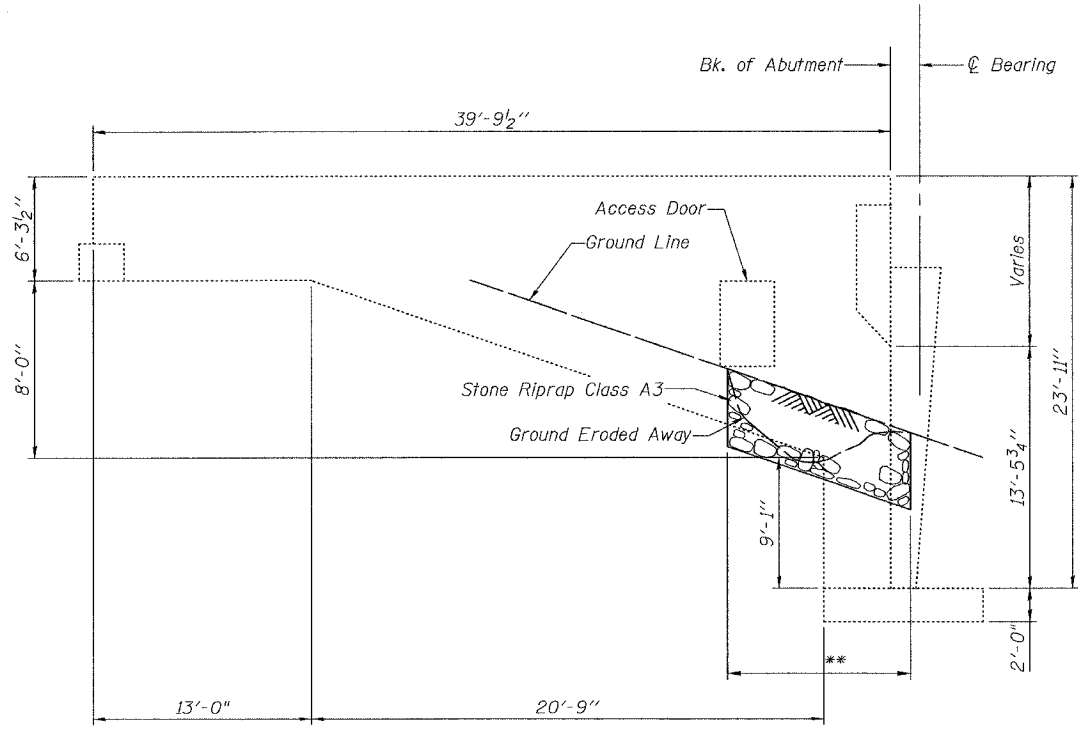
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PLOT SCALE = 1/4" = 1'-0"  
USER NAME = TFG



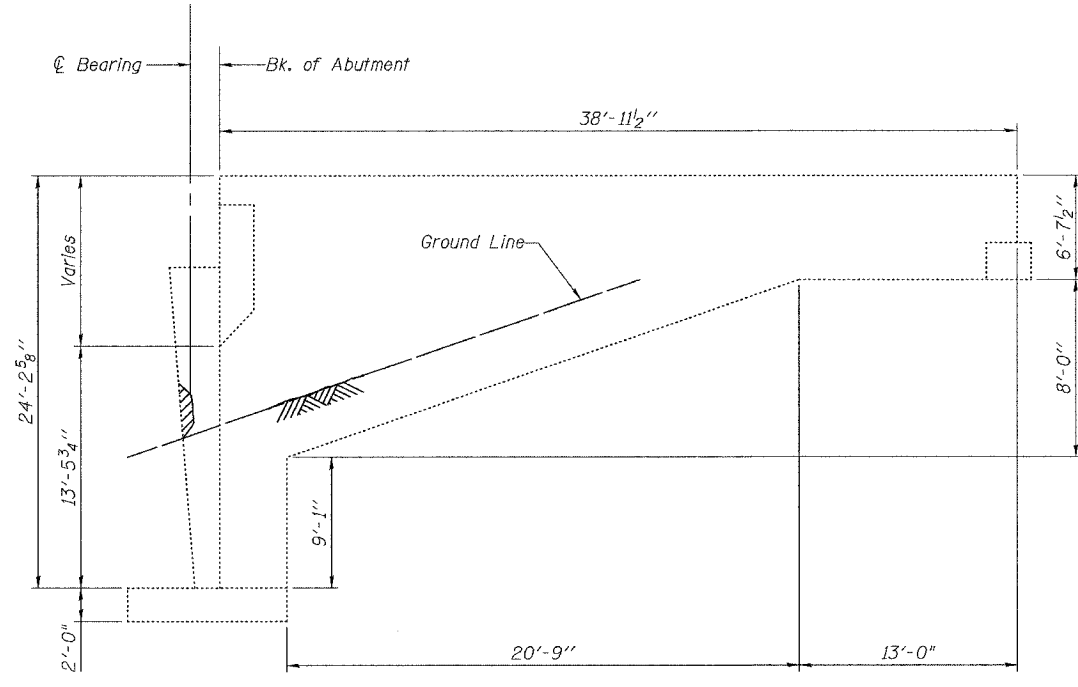


**STONE RIPRAP DETAIL**

**EAST ABUTMENT**  
(Facing East)



**NORTH CURTAIN WALL ELEVATION**  
(Facing South)



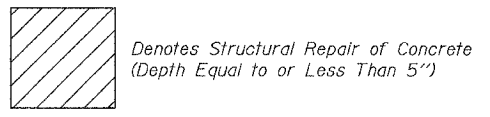
**SOUTH CURTAIN WALL ELEVATION**  
(Facing North)

\*\*Varied in the field as directed by the Engineer. Quantity is estimated.

**BILL OF MATERIAL**

Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq. Ft.	25
Stone Riprap Class A3	Sq. Yd.	12
Filter Fabric	Sq. Yd.	12

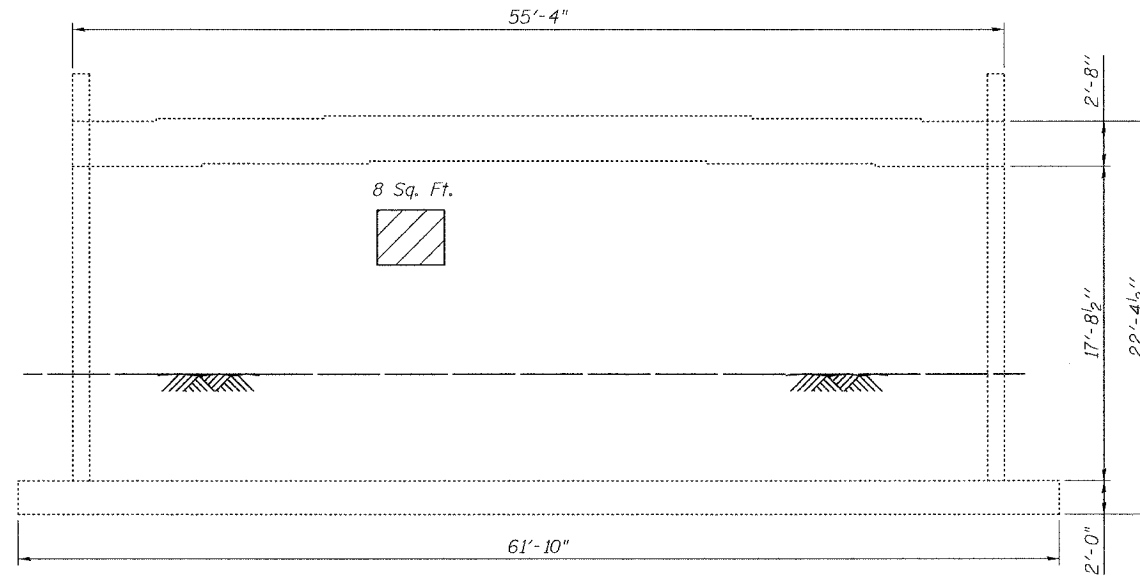
**LEGEND**



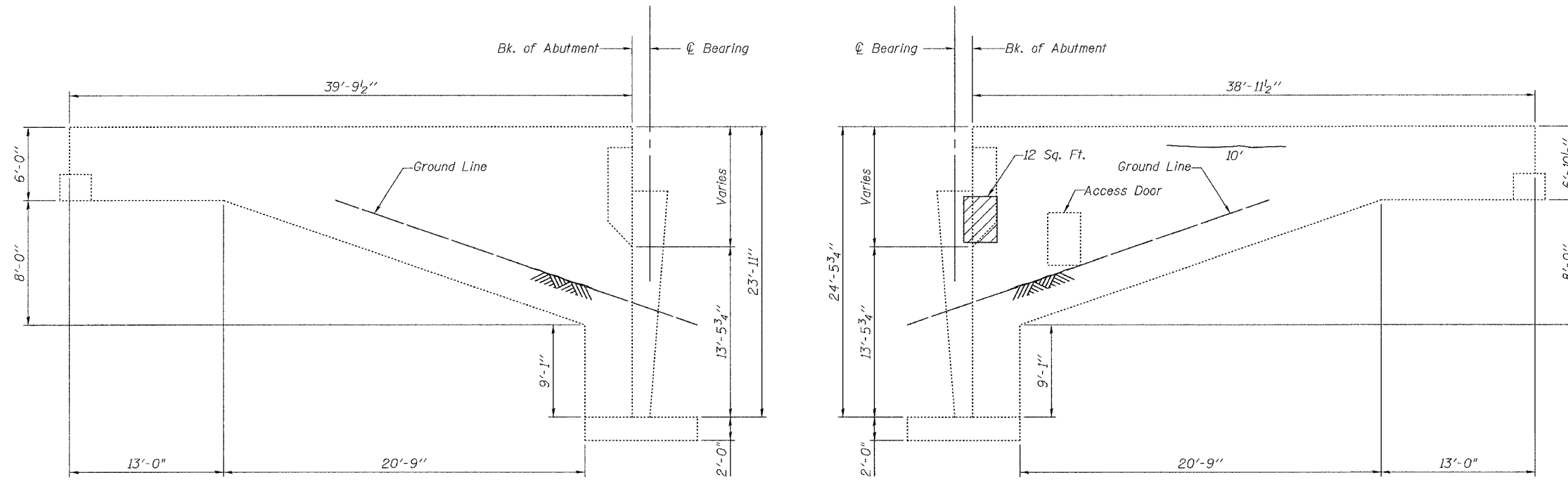
ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE		REPAIRS EAST ABUTMENT EB	
PROJECT	FAI RTE 64 OVER US RTE 51 FAI RTE 64 SECTION 95-(5,6)RS-1 WASHINGTON COUNTY STATION 3283+58.83 SN 095-0063 (EB) & 095-0064 (WB)	PROJECT NO.	06001-6
SCALE	1" = 10'-0"	DATE	8/21/07
DRAWN BY	TFG	CHECKED BY	KPS/BD/MCB
COOMBE-BLOXDORF P.C. Engineers / Land Surveyors Springfield, Illinois Design Firm License No. 184-002703		DRAWING NO.	8
		OF 10 SHTS	

PLOT DATE = 08/21/2007  
FILE NAME = c:\05-repairs-east-abut-eb.dgn  
SCALE = 1" = 10'-0" / 1/4"  
USER NAME = TFG



**EAST ABUTMENT**  
(Facing East)



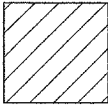

**NORTH CURTAIN WALL ELEVATION**  
(Facing South)

**SOUTH CURTAIN WALL ELEVATION**  
(Facing North)

**BILL OF MATERIALS**

Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq. Ft.	20
Epoxy Crack Injection	Ft.	10

**LEGEND**

-  Denotes Structural Repair of Concrete (Depth Equal to or Less Than 5")
-  Denotes Epoxy Crack Injection

ILLINOIS DEPARTMENT OF TRANSPORTATION	
SHEET TITLE REPAIRS EAST ABUTMENT WB	
PROJECT FAI RTE 64 OVER US RTE 51 FAI RTE 64 SECTION 95-(5,6)RS-1 WASHINGTON COUNTY STATION 3283+58.83 SN 095-0063 (EB) & 095-0064 (WB)	PROJECT NO. 06001-6 SCALE DATE 8/21/07 DRAWN BY TFG CHECKED BY KPS/BD/MCB DRAWING NO.
COOMBE-BLOXDORF P.C. Engineers / Land Surveyors Springfield, Illinois Design Firm License No. 184-002703	
9 OF 10 SHTS	

**NOTES**

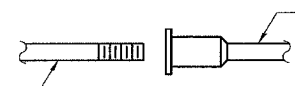
Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.  
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.  
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.  
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- ① Minimum Capacity (Tension in kips) =  $1.25 \times f_y \times A_t$
- ② Minimum \*Pull-Out Strength (Tension in kips) =  $0.66 \times f_y \times A_t$

Where  $f_y$  = Yield strength of lapped reinforcement bars in ksi.  
 $A_t$  = Tensile stress area of lapped reinforcement bars.  
 \* = 28 day concrete

BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-0"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8

The diameter of this part is the same as the diameter of the bar spliced.



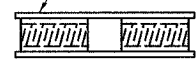
The diameter of this part is equal or larger than the diameter of bar spliced.

**ROLLED THREAD DOWEL BAR**



**\*\* ONE PIECE**

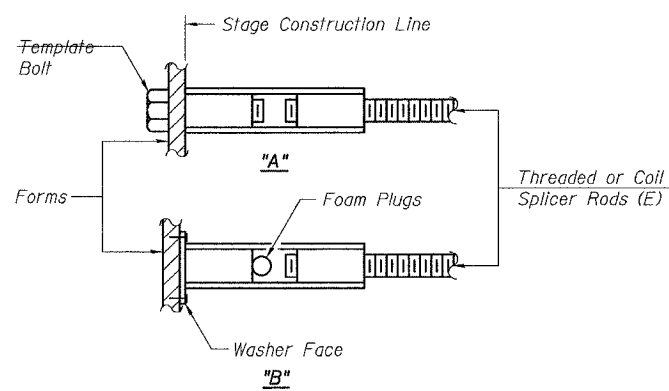
Wire Connector



**WELDED SECTIONS**

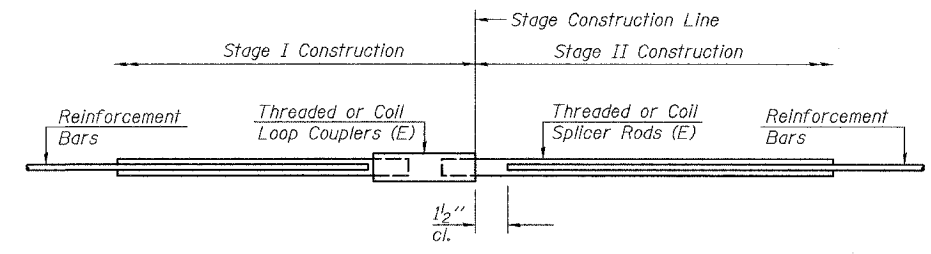
**BAR SPLICER ASSEMBLY ALTERNATIVES**

\*\* Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



**STANDARD**

Bar Size	No. Assemblies Required	Location
#4	8	Diaphragm at W. Abut.
#6	4	Diaphragm at W. Abut.
#6	56	Deck

ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET TITLE  
**BAR SPLICER ASSEMBLY DETAILS**

PROJECT  
 FAI RTE 64 OVER US RTE 51  
 FAI RTE 64 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 STATION 3283+58.83  
 SN 095-0063 (EB) & 095-0064 (WB)

PROJECT NO. 06001-6  
 SCALE  
 DATE 8/21/07  
 DRAWN BY TFG  
 CHECKED BY KPS/BD/MCB  
 DRAWING NO.

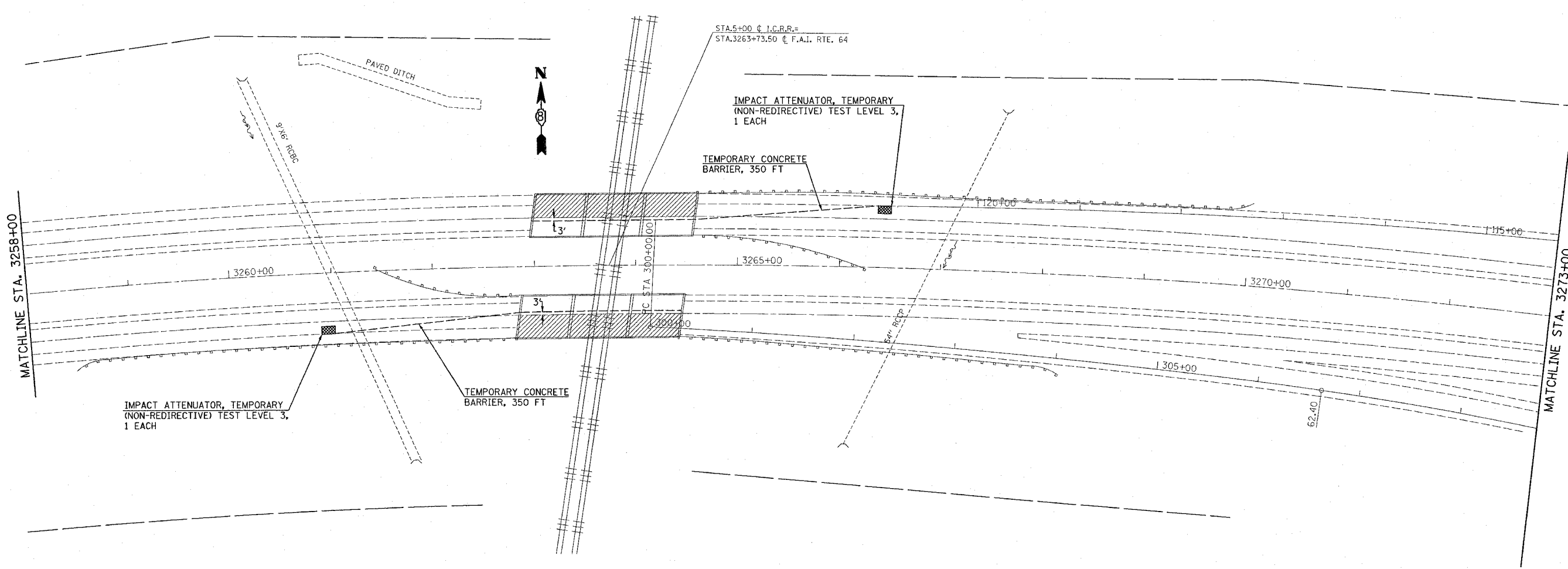
**COOMBE-BLOXDORF P.C.**  
 Engineers / Land Surveyors  
 Springfield, Illinois  
 Design Firm License No. 184-002703

10  
 OF 10 SHTS

PLOT DATE = 08/21/2007  
 FILE NAME = \\s01-bor-splicer-details.dgn  
 USER NAME = TFG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	68

STA.	TO STA.
FED. ROAD DIST. NO. ILLINOIS	FED. AID PROJECT






NOTES:

TRAFFIC CONTROL SHALL CONFORM TO STANDARDS 701402 INCLUDING ALL DEVICES SHOWN ON THE STANDARDS.

USE OF NEW JERSEY CONCRETE BARRIER IN ACCORDANCE WITH SECTION 704 OF THE 2002 EDITION OF THE STANDARD SPECIFICATIONS WILL BE PERMITTED ON THIS PROJECT. ALL OTHER TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE 2007 EDITION OF THE STANDARD SPECIFICATIONS.

TRAFFIC CONTROL FOR STAGE 1 IS SHOWN. TRAFFIC CONTROL FOR STAGE 2 WILL BE A MIRROR IMAGE OF STAGE 1

-  - WORK AREA
-  - TEMPORARY CONCRETE BARRIER
-  - IMPACT ATTENUATOR

REVISIONS	
NAME	DATE

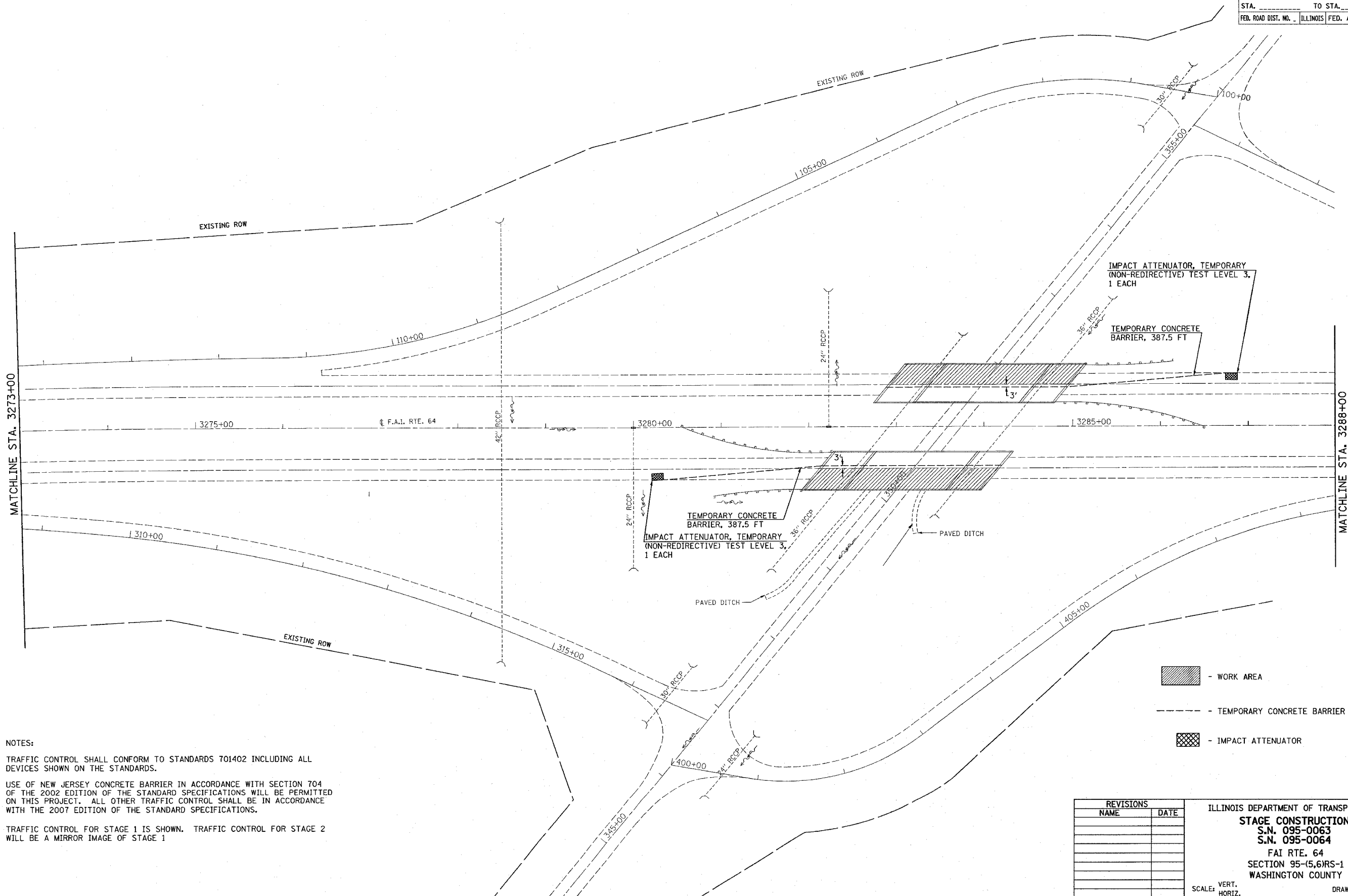
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**STAGE CONSTRUCTION**  
 S.N. 095-0061  
 S.N. 095-0062  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_

DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 8/21/2007  
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	69
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



NOTES:

TRAFFIC CONTROL SHALL CONFORM TO STANDARDS 701402 INCLUDING ALL DEVICES SHOWN ON THE STANDARDS.

USE OF NEW JERSEY CONCRETE BARRIER IN ACCORDANCE WITH SECTION 704 OF THE 2002 EDITION OF THE STANDARD SPECIFICATIONS WILL BE PERMITTED ON THIS PROJECT. ALL OTHER TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE 2007 EDITION OF THE STANDARD SPECIFICATIONS.

TRAFFIC CONTROL FOR STAGE 1 IS SHOWN. TRAFFIC CONTROL FOR STAGE 2 WILL BE A MIRROR IMAGE OF STAGE 1

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**STAGE CONSTRUCTION**  
 S.N. 095-0063  
 S.N. 095-0064  
 FAI RTE. 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
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CONTRACT NO. 76A39				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	70
STA.		TO STA.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

**GENERAL NOTES**

DESIGN: AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals. ("AASHTO Specifications")

CONSTRUCTION: Current (at time of letting) Illinois Department of Transportation Standard Specifications for Road and Bridge Construction, Supplemental Specifications and Special Provisions. ("Standard Specifications")

LOADING: 90 M.P.H. WIND VELOCITY

WALKWAY LOADING: Dead load plus 500 lbs. concentrated live load.

DESIGN STRESSES:  
Field Units  
f'c = 3,500 p.s.i.  
fy = 60,000 p.s.i. (reinforcement)

WELDING: All welds to be continuous unless otherwise shown. All welding to be done in accordance with current AWS D1.1 and D1.2 Structural Welding Codes (Steel and Aluminum) and the Standard Specifications.

MATERIALS: Aluminum Alloys as shown throughout plans. All Structural Steel Pipe shall be ASTM A53 Grade B or A500 Grade B or C. If A500 pipe is substituted for A53, then the outside diameter shall be as detailed and wall thickness greater than or equal to A53.

All Structural Steel Plates and Shapes shall conform to AASHTO M270 Gr. 36, Gr. 50 or Gr. 50W\*. Stainless steel for shims, sleeves and handhole covers shall be ASTM A240, Type 302 or 304, or another alloy suitable for exterior exposure and acceptable to the Engineer.

The steel pipe and stiffening ribs at the base plate for the column shall have a minimum longitudinal Charpy V-Notch (CVN) energy of 15 lb.-ft. at 40° F. (Zone 2) before galvanizing.

FASTENERS FOR ALUMINUM TRUSSES: All bolts noted as "high strength" must satisfy the requirements of AASHTO M164 (ASTM A325), or approved alternate, and must have matching lock nuts. Threaded studs for splices (if Members interfere) must satisfy the requirements of ASTM A449, ASTM A193, Grade B7, or approved alternate, and must have matching lock nuts. Bolts and lock nuts not required to be high strength must satisfy the requirements of ASTM A307. All bolts and lock nuts must be hot dip galvanized per AASHTO M232. The lock nuts must have nylon or steel inserts. A stainless steel flat washer conforming to ASTM A240 Type 302 or 304, is required under both head and nut or under both nuts where threaded studs are used. High strength bolt installation shall conform to Article 505.04 (f) (2)d of the IDOT Standard Specifications for Road and Bridge Construction. Rotational capacity ("ROCAP") testing of bolts will not be required.

U-BOLTS AND EYEBOLTS: U-Bolts and Eyebolts must be produced from ASTM A276 Type 304, 304L, 316 or 316L, Condition A, cold finished stainless steel, or an equivalent material acceptable to the Engineer. All nuts for U-Bolts and Eyebolts must be lock nuts equivalent to ASTM A307 with nylon or steel inserts and hot dip galvanized per AASHTO M232. A stainless steel flat washer conforming to ASTM A240, Type 302 or 304, is required under each U-Bolt and Eyebolt lock nut.

GALVANIZING: All Steel Grating, Plates, Shapes and Pipe shall be Hot Dip Galvanized after fabrication in accordance with AASHTO M111. Painting is not permitted.

ANCHOR RODS: Shall conform to AASHTO M314 Gr. 105 with a minimum Charpy V-Notch (CVN) energy of 15 lb.-ft. at 10° F.

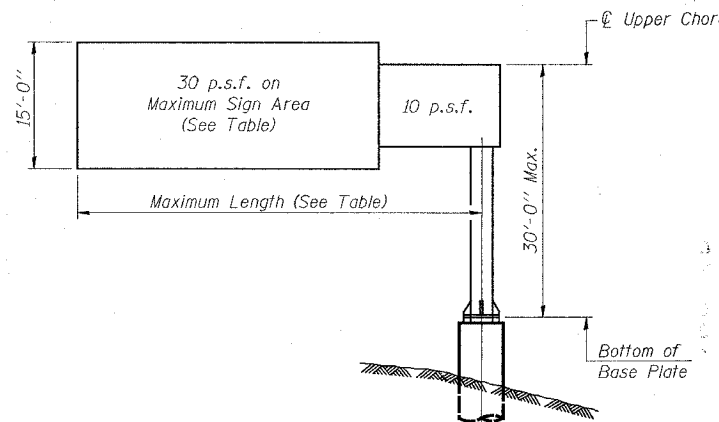
CONCRETE SURFACES: All concrete surfaces above an elevation 6" below the lowest final ground line at each foundation shall be cleaned and coated with Bridge Seat Sealer in accordance with the Standard Specifications.

REINFORCEMENT BARS: Reinforcement Bars designated (E) shall be epoxy coated in accordance with the Standard Specifications.

FOUNDATIONS: The contract unit price for Drilled Shaft Concrete Foundations shall include reinforcement bars complete in place.

Structure Number	Station	Design Truss Type	Cantilever Length (L)	Elev. A	Dim. D	Ds	Total Sign Area
8C0951064R060.6	3267+10	III-C-A	32'-0"	564.43	18'-0"	11'-6"	184 s.f.

Truss Type	Maximum Sign Area	Maximum Length
I-C-A	170 Sq. Ft.	25 Ft.
II-C-A	340 Sq. Ft.	30 Ft.
III-C-A	400 Sq. Ft.	40 Ft.



**DESIGN WIND LOADING DIAGRAM**

Parameters shown are basis for I.D.O.T. Standards. Installations not within dimensional limits shown require special analysis for all components.

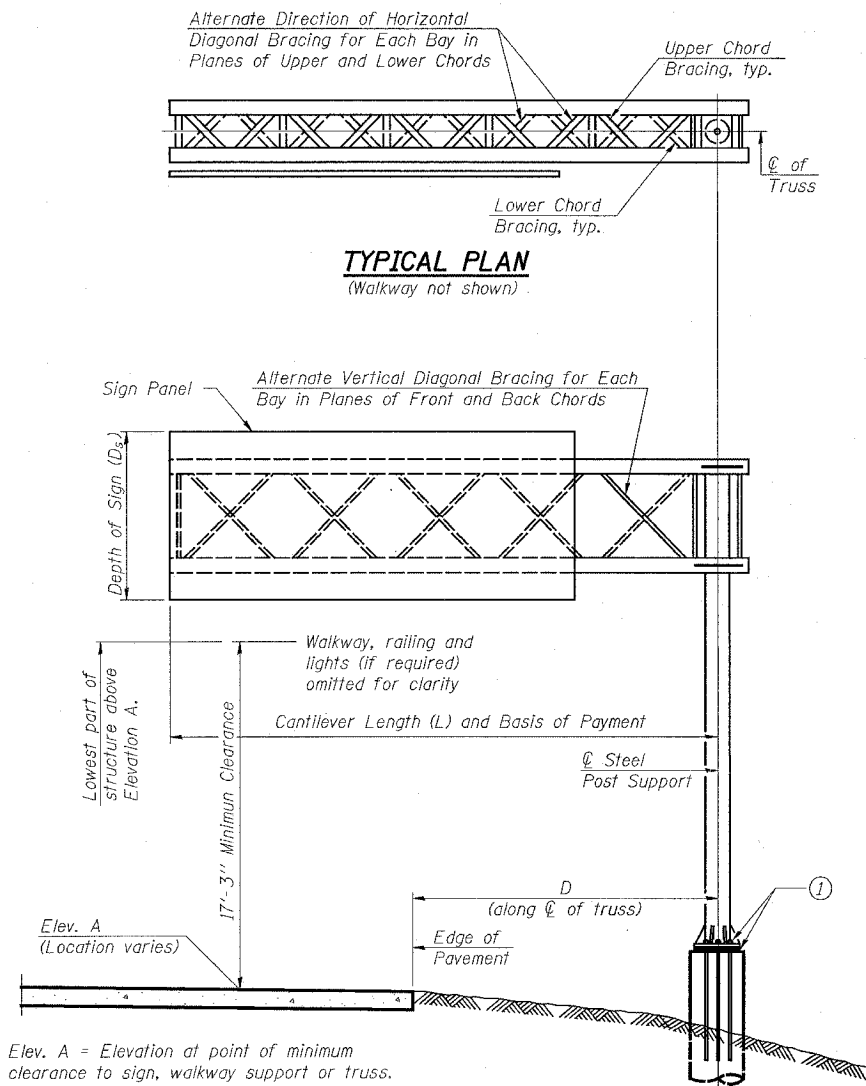
- ① After adjustments to level truss and insure adequate vertical clearance, all top and leveling nuts shall be tightened against the base plate with a minimum torque of 200 lb.-ft. Stainless steel mesh shall then be placed around the perimeter of the base plate. Secure to base plate with stainless steel banding.

Note:  
Trusses shall be shipped individually with adequate provision to prevent detrimental motion during transport. This may require ropes between horizontals and diagonals or energy dissipating (elastic) ties to the vehicle. The contractor is responsible for maintaining the configuration and protection of the trusses.

\* If M270 Gr. 50W (M222) steel is proposed, chemistry for plate to be used shall first be approved by the Engineer as suitable for galvanizing and welding.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	TOTAL
OVERHEAD SIGN STRUCTURE CANTILEVER TYPE I-C-A	Foot	
OVERHEAD SIGN STRUCTURE CANTILEVER TYPE II-C-A	Foot	
OVERHEAD SIGN STRUCTURE CANTILEVER TYPE III-C-A	Foot	32
OVERHEAD SIGN STRUCTURE WALKWAY, TYPE A	Foot	20
DRILLED SHAFT CONCRETE FOUNDATIONS	Cu. Yds.	10



**TYPICAL ELEVATION**

Looking in Direction of Traffic

Sign support structures may be subject to damaging vibrations and oscillations when sign panels are not in place during erection or maintenance of the structure. To avoid these vibrations and oscillations, consideration should be given to attaching temporary blank sign panels to the structure.

NUMBER	REVISION	DATE

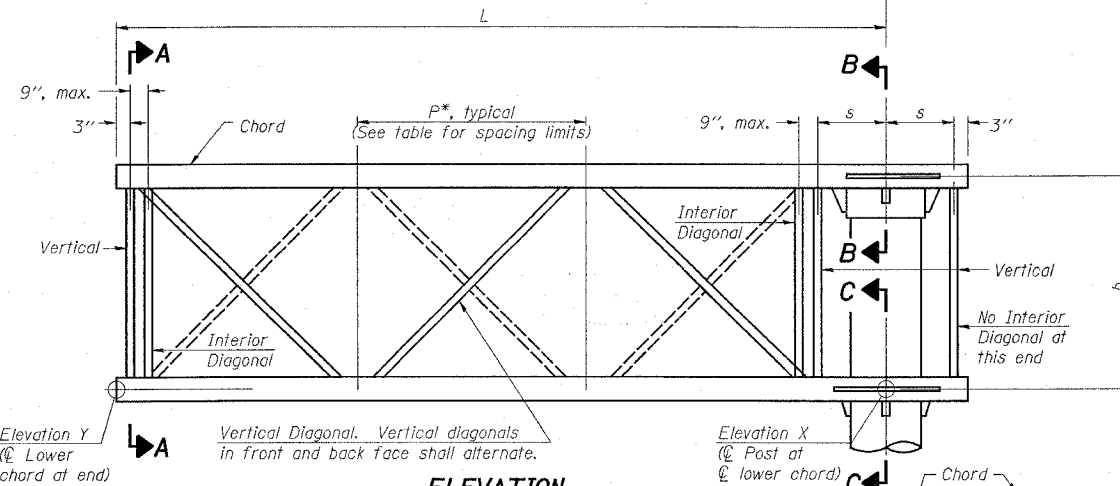
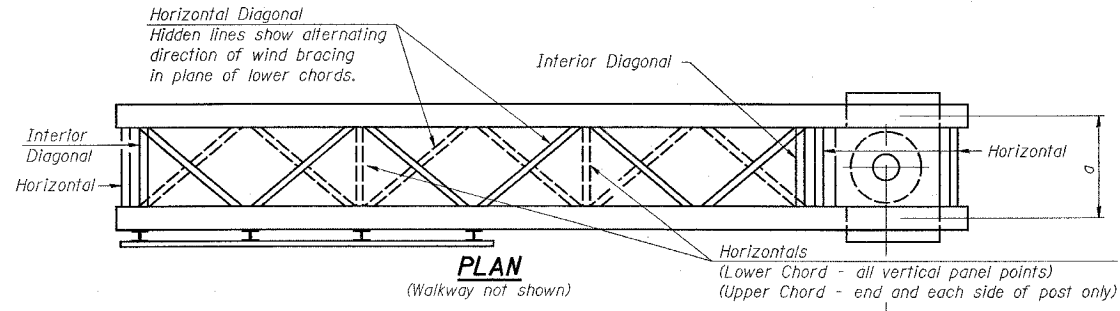
OSC-A-1

6/01/2007

**CANTILEVER SIGN STRUCTURES  
GENERAL PLAN & ELEVATION  
ALUMINUM TRUSS & STEEL POST**

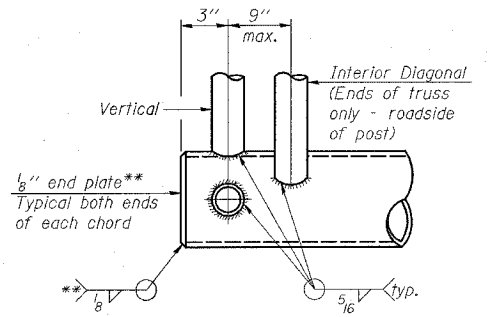
FAI ROUTE 64  
SECTION 95-(5,6)RS-1  
WASHINGTON COUNTY

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA. _____		TO STA. _____		
FED. ROAD DIST. NO. _____		ILLINOIS	FED. AID PROJECT	

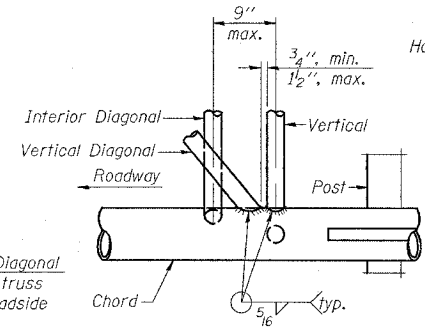


**TYPICAL TRUSS UNIT**  
For Section B-B and Section C-C, see Base Sheet OSC-A-3.

Note: There are twice as many horizontal diagonals as there are vertical diagonals.



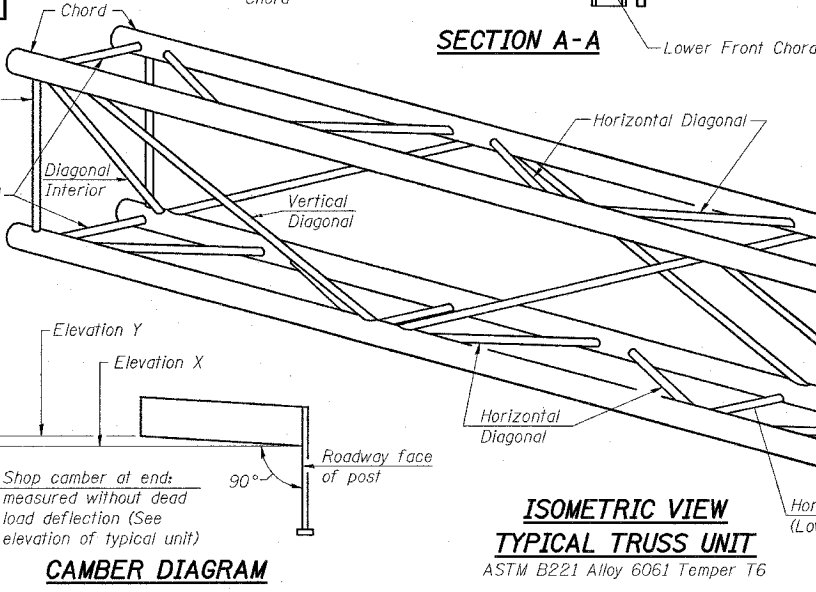
**CANTILEVER END JOINT DETAIL**  
\*\* Contractor may alternatively use standard aluminum drive-fit cap to close ends.



**POST END JOINT DETAIL**

**SHOP CAMBER TABLE**

Unit Length (L)	Shop Camber at End
15'	1 1/2"
16'-17'	1 3/4"
18'-20'	2"
21'-22'	2 1/4"
23'-25'	2 1/2"
26'-27'	2 3/4"
28'-30'	3"
31'-32'	3 1/4"
33'-35'	3 1/2"
36'-37'	4"
38'-40'	4 1/2"

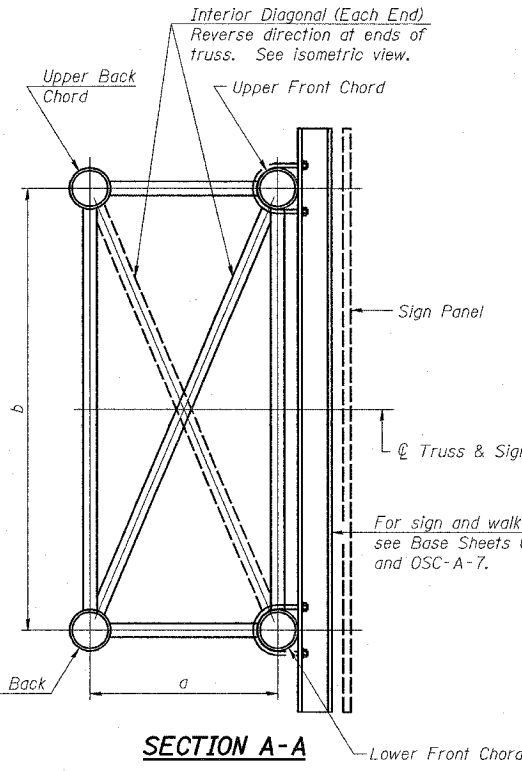


**TYPICAL TRUSS UNIT**  
ASTM B221 Alloy 6061 Temper T6

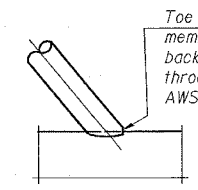
**TRUSS UNIT TABLE**

Truss Type	Dimension "a"	Dimension "b"	Dimension "s"	Limits for Panel Spacing (P)*	Up. & Low. Chord O.D.		Verticals, Horizontals, and Interior Diagonals O.D.	
					Wall	Wall	Wall	Wall
I-C-A	24"	54"	16"	36" min. to 48" max.	5"	5/16"	2 1/2"	5/16"
II-C-A	36"	66"	21"	42" min. to 54" max.	6 1/2"	5/16"	3 1/4"	5/16"
III-C-A (35' Max.)	36"	84"	21"	48" min. to 66" max.	7"	3/8"	3 1/2"	3/8"
III-C-A (>35' to 40')	36"	84"	21"	48" min. to 66" max.	8"	3/8"	3 1/2"	3/8"

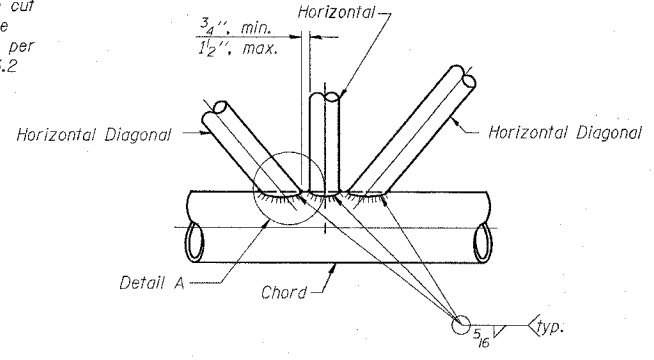
\*P =  $\frac{L-s-3''}{\# \text{ Panels}}$



**SECTION A-A**



**DETAIL A**



**TRUSS INTERIOR JOINT DETAIL**

Structure Number	Station	Truss Type	Design Length (L)	Number of Panels Per Unit	Panel Length (P)*
8C0951064R060.6	3267+10	III-C-A	32'-0"	6	5'-0"

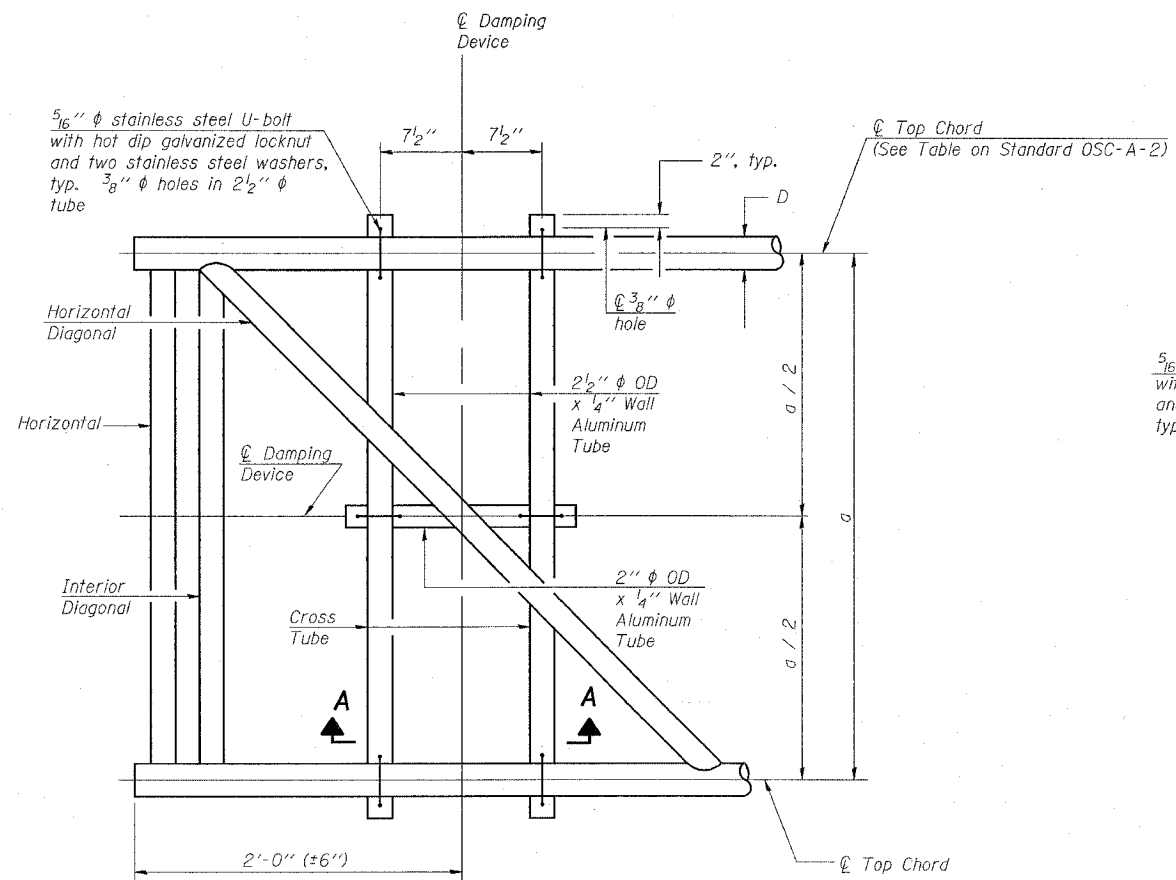
NUMBER	REVISION	DATE

**CANTILEVER SIGN STRUCTURES TRUSS DETAILS ALUMINUM TRUSS & STEEL POST**

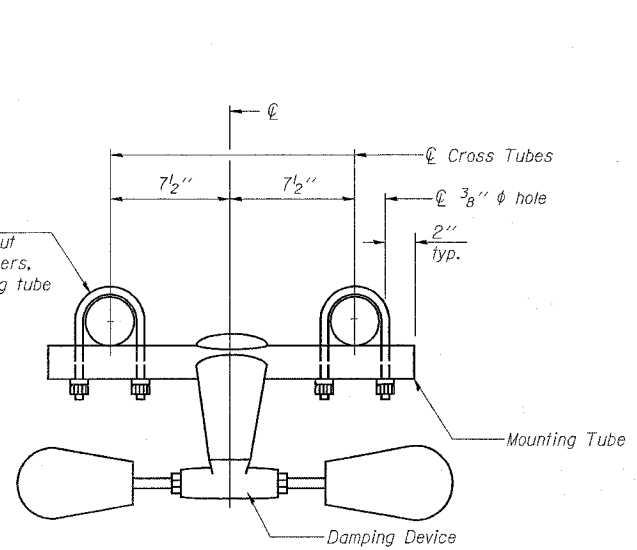
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SECTION 95-(5,6)RS-1  
WASHINGTON COUNTY

PLOT DATE = 8/21/2007  
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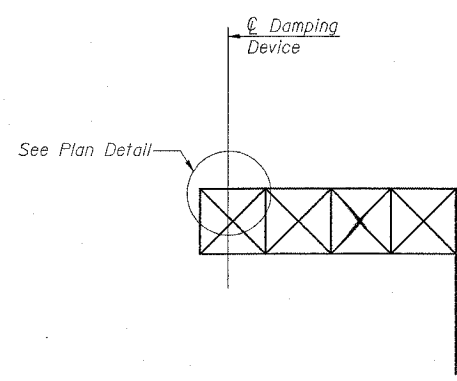
CONTRACT NO. 76A39				
F.A.I. SITE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



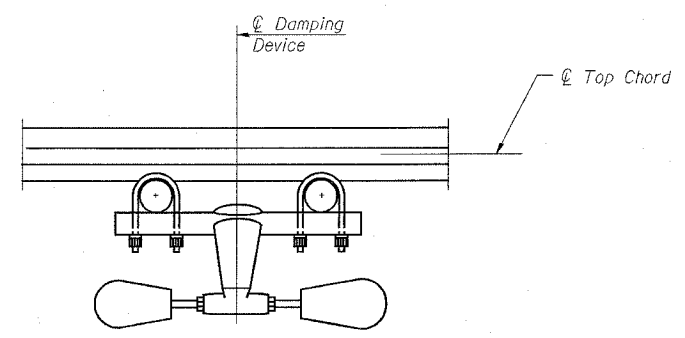
**PLAN DETAIL**



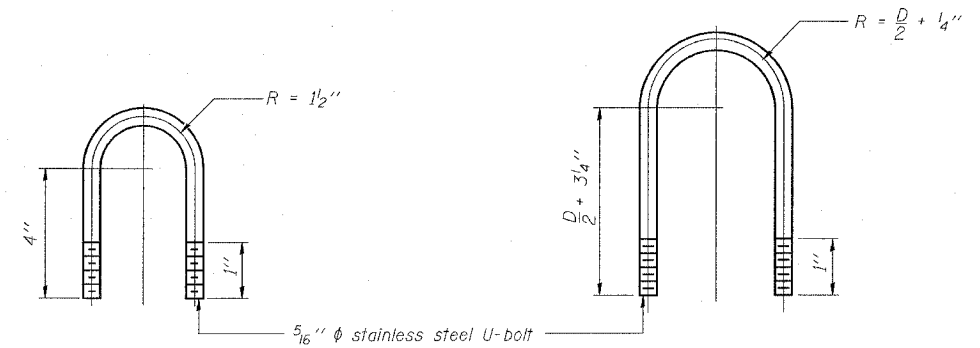
**TRUSS DAMPING DEVICE CONNECTION DETAIL**



**ELEVATION**  
Aluminum Cantilever Sign Structure



**SECTION A-A**



**DAMPING DEVICE MOUNTING TUBE U-BOLT DETAIL**  
(Typical)

**TOP CHORD TO CROSS TUBE U-BOLT DETAIL**  
(Typical)

**GENERAL NOTES**

Damper: One damper per truss. (31 lbs. Stockbridge-Type Aluminum-29" minimum between ends of weights)

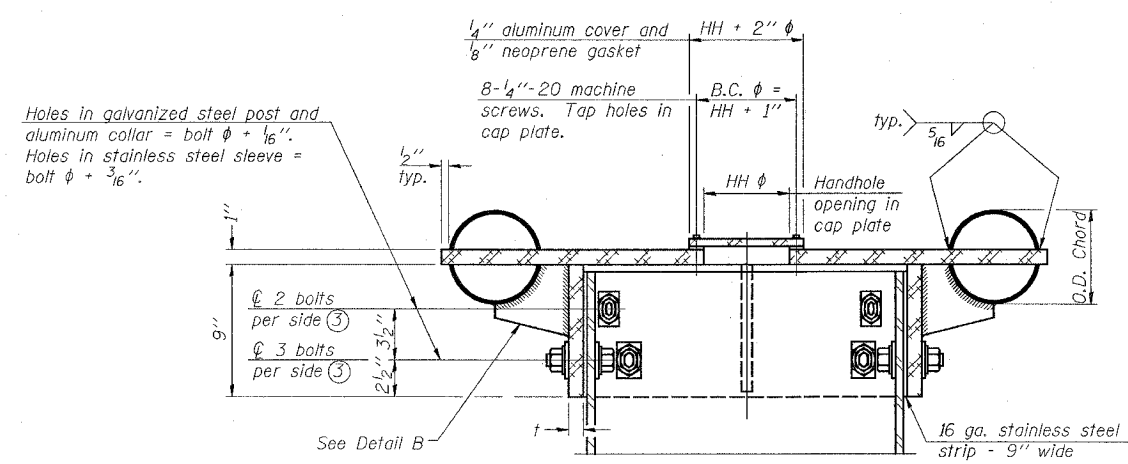
Materials: Aluminum tubes shall be ASTM B221 alloy 6061 temper T6

**CANTILEVER SIGN STRUCTURE DAMPING DEVICE**

FAI ROUTE 64  
SECTION 95-(5,6)RS-1  
WASHINGTON COUNTY

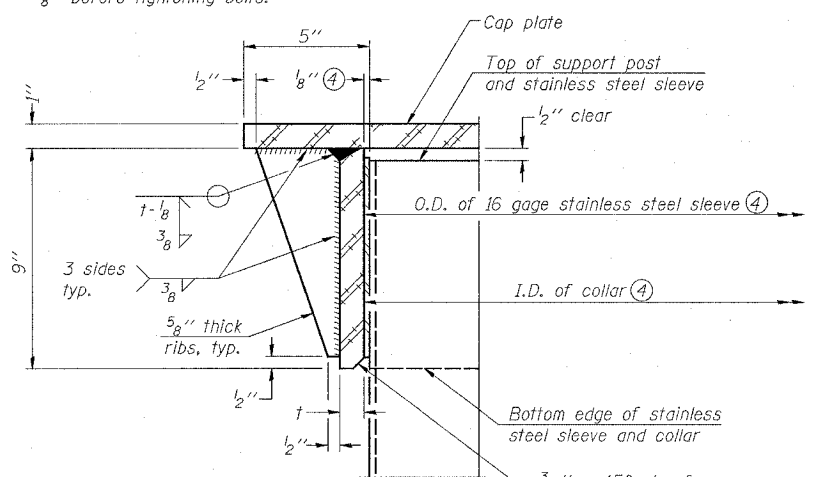
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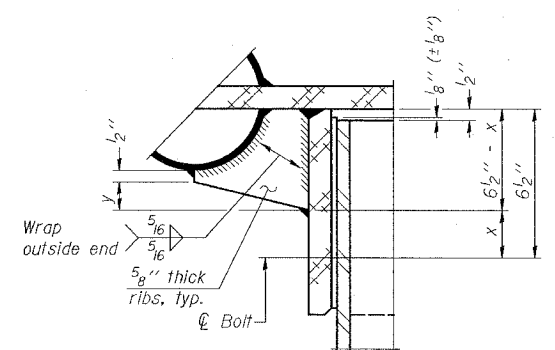


④ Collar I.D. shall be manufactured to correspond to O.D. of actual galvanized post and stainless steel sleeve plus 1/8 inch (± 1/16 inch). Maximum gap between post and collar at any location equals 1/8 inch before tightening bolts.

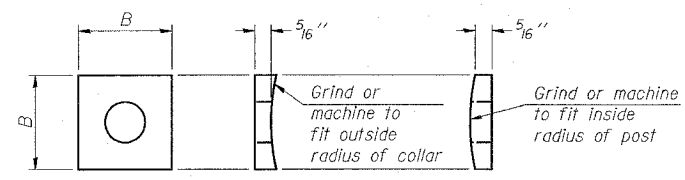
**SECTION B-B**  
 Bolts, washers (including contoured washers), and locknuts shall be stainless steel.



**DETAIL A**  
 (Two locations)  
 3/16 inch - 45° chamfer on inside of collar to facilitate field assembly

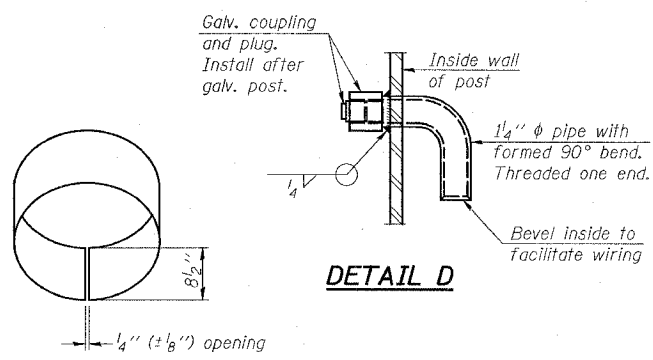


**DETAIL B**  
 Two locations  
 (For details not shown, see Detail C)



**CONTOURED WASHERS**

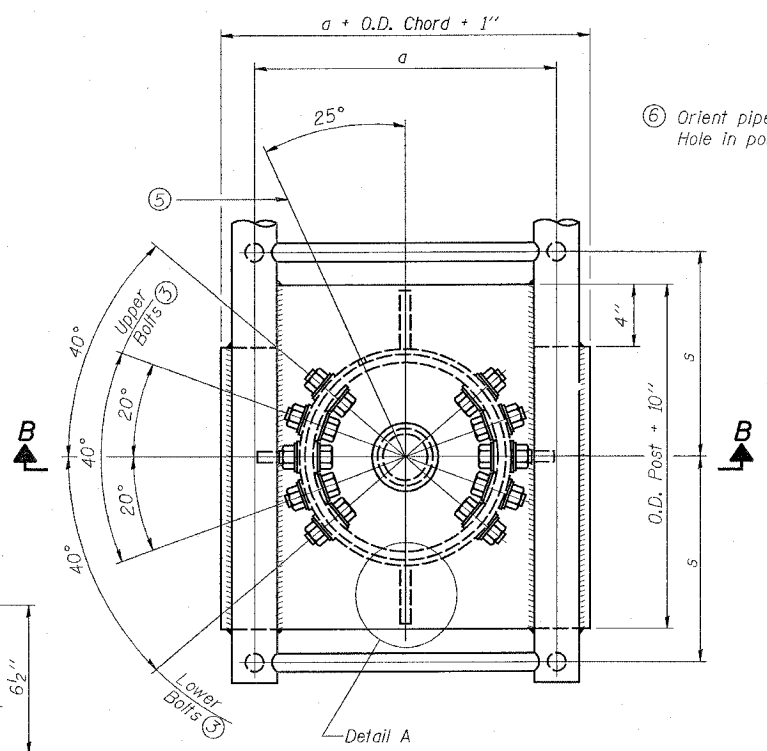
Bolt Size	Contoured Washers	
	Hole Dia.	B
7/8"	1"	2 1/2"
1"	1 1/8"	3"
1 1/4"	1 3/8"	3 1/4"



**DETAIL OF STAINLESS STEEL SLEEVE**

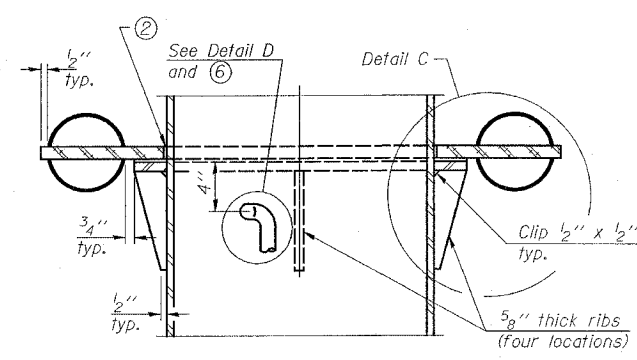
Weld to post after galvanizing. (Prepare post surface to insure tight, uniform fit and allow welding.) Welds to be 1/2 inch long at 6 inch cts. along top edge and at 1/4 inch opening.

NUMBER	REVISION	DATE

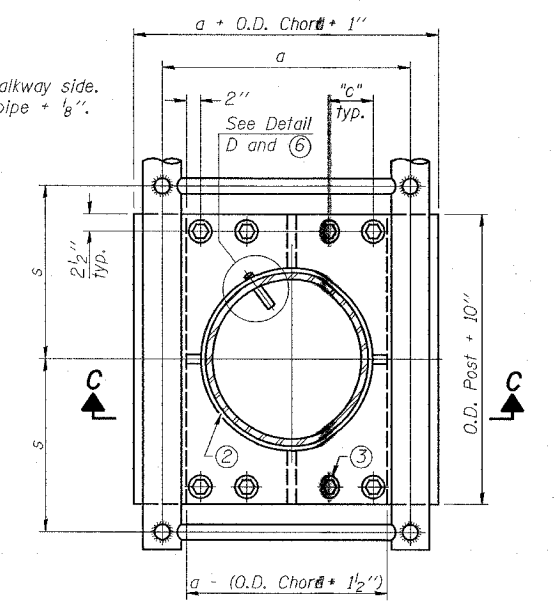


**PLAN VIEW - TOP OF COLUMN**

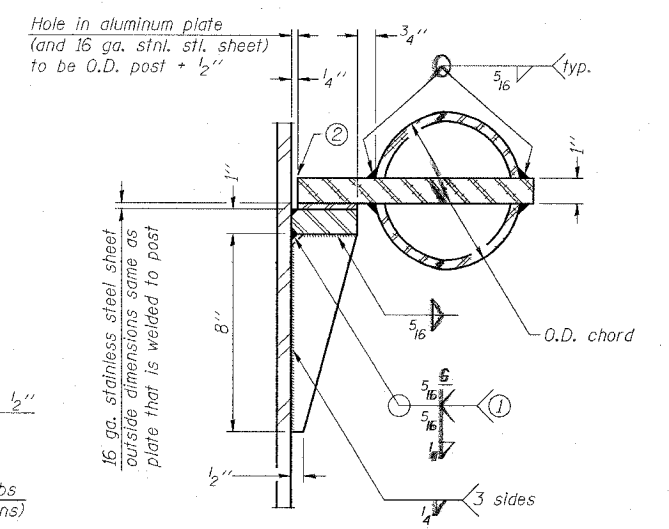
⑤ Optional full penetration weld in collar. (Two locations maximum....(180° apart)....X-ray or UT 100%)



**SECTION C-C**



**SECTION THRU POST ABOVE LOWER CHORDS**



**DETAIL C**

- ① Grind top if required to fully seat aluminum plate and stainless steel sheet.
- ② After tightening lower connection bolts, fill gap with non-hardening, silicone caulk suitable for exterior exposure and acceptable to the Engineer. Cost is included in Overhead Sign Structure Cantilever.

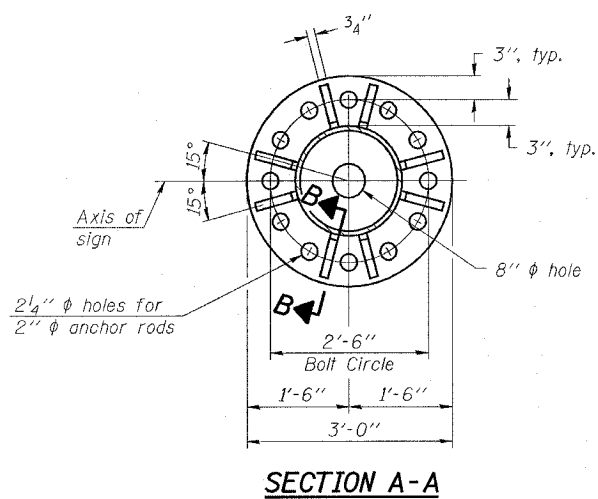
Truss Type	Post Size	Upper & Lower Connection Bolt Diameter ③	Lower Juncture Bolt Spacing Dimension "c" ③	Opening in Cap Plate "HH"	Collar Thickness (t)	Side Ribs	
						x	y
I-C-A	16" φ (83#/#)	7/8"	3 1/4"	8"	5/8"	1 3/4"	2 1/4"
II-C-A	24" φ (125#/#)	1"	3 1/2"	12"	7/8"	2"	1 1/4"
III-C-A (35' max.)	24" φ (125#/#)	1 1/4"	3 1/2"	12"	7/8"	2"	1"
III-C-A (>35' to 40')	24" φ (171#/#)	1 1/4"	3 1/2"	12"	7/8"	2"	1"

③ Upper and lower connection bolts in collar and bolts at lower chord connection shall be high strength with matching locknuts. Connection bolts shall have 2 stainless steel flat washers each.

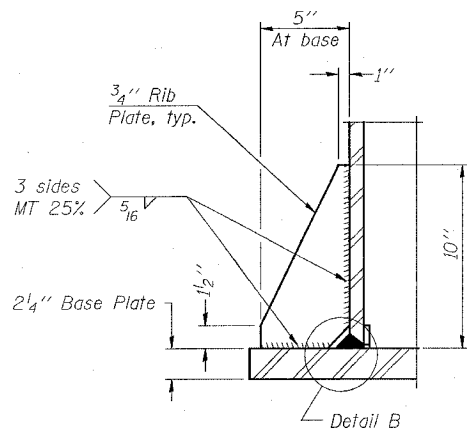
**CANTILEVER SIGN STRUCTURES  
 JUNCTURE DETAILS  
 ALUMINUM TRUSS & STEEL POST**

FAI ROUTE 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

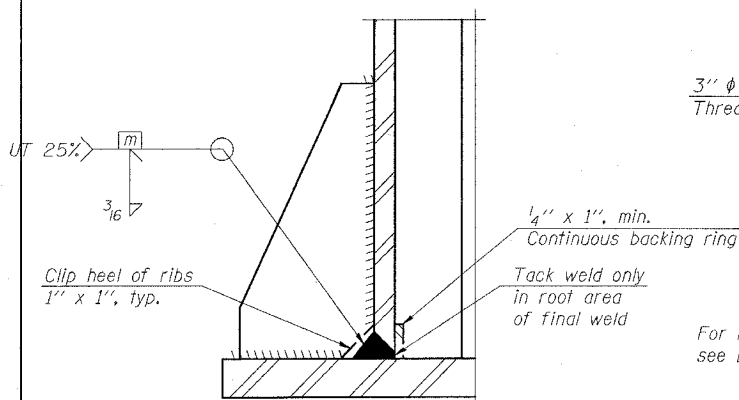
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	74
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



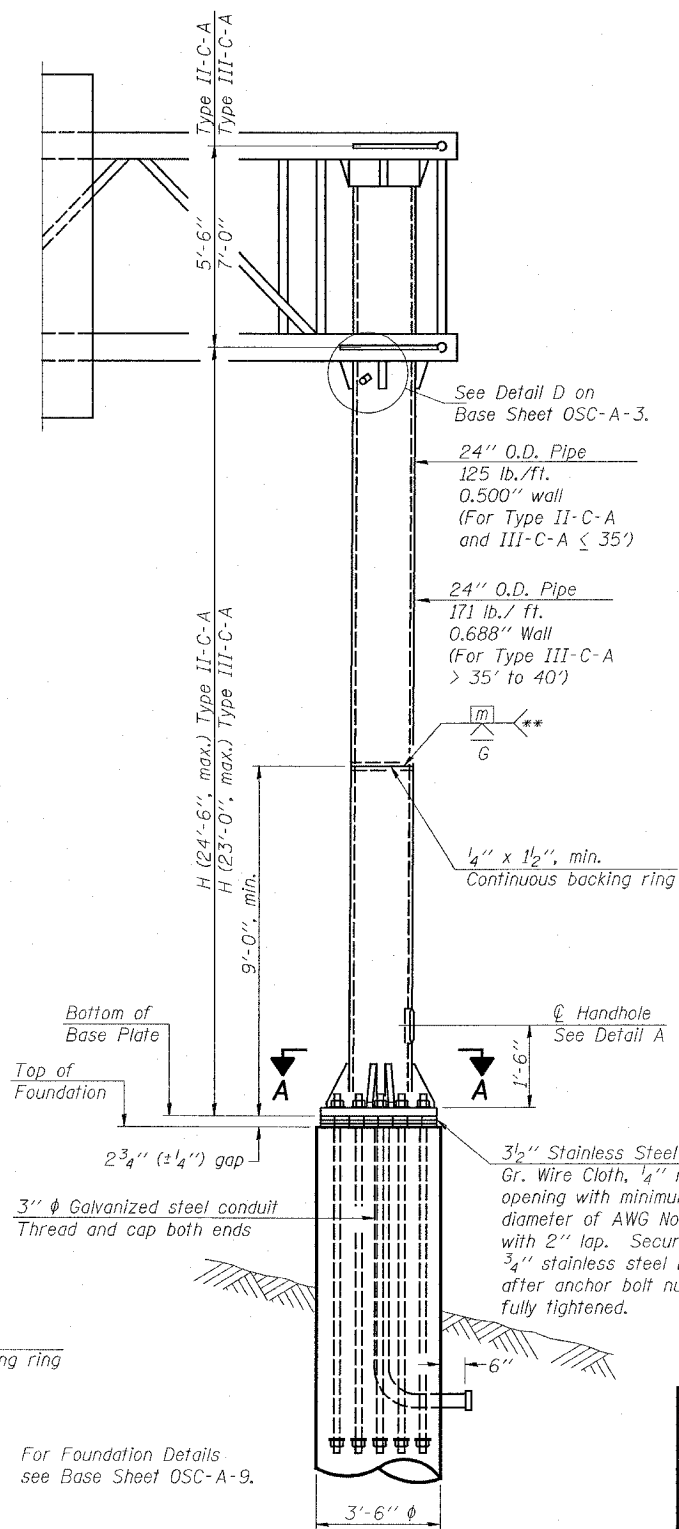
**SECTION A-A**



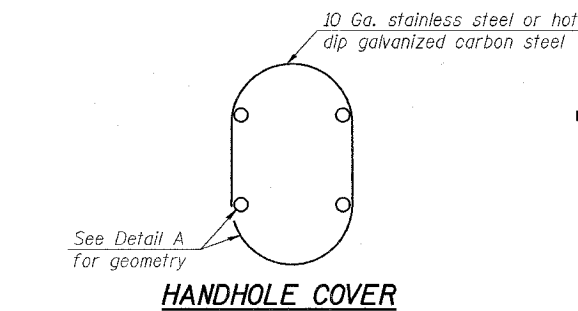
**SECTION B-B**



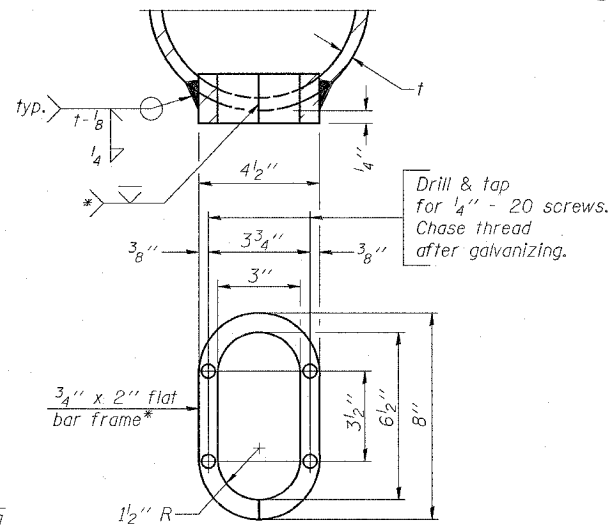
**DETAIL B**  
(Typical rib)



**FRONT ELEVATION**



**HANDHOLE COVER**



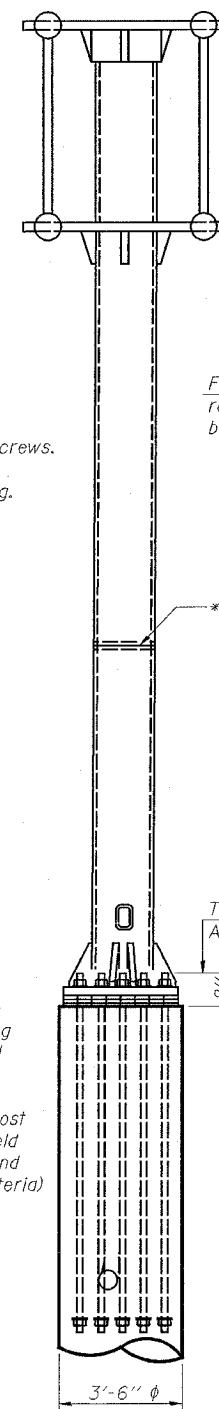
**DETAIL A**

\* Bent bars may be butt welded top and bottom or bottom only. In lieu of fabricated handhole frame as shown, may cut from 2" plate (rolling direction vertical). All cut faces to be ground to ANSI Roughness of 500 min or less.

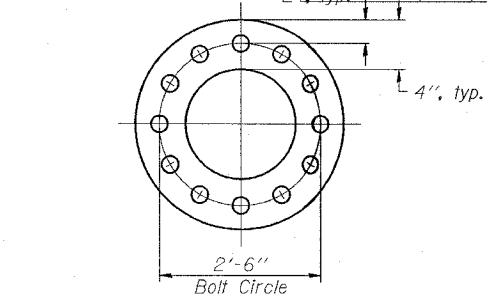
\*\* Butt welded joint in post is only allowed for post heights (H) over 20 ft. in length. If used, weld procedure must be preapproved by Engineer and joint shall receive 100% RT or UT (tension criteria) at Contractor's expense.

Structure Number	Station	H
8C095I064R060.6	3267+10	22'-6"

Note: "H" based on 15'-0" or actual sign height, whichever is greater.



**SIDE ELEVATION**



**SUGGESTED POSITIONING PLATE**

Utilize positioning plate and temporary nuts with leveling nuts or other Engineer approved methods to maintain anchor bolts' alignment during concrete placement. Plate, extra nuts and other positioning aids become Contractor's property. Cost included in Drilled Shaft Concrete Foundations.

Protect threads during concreting with tape, sleeves, or other means.

\*\*\* 18" is minimum to be galvanized. Entire rod may be galvanized at Contractor's option.

All Thread = NC (National Coarse)

Provide 1 uncoated nut per rod. Deform thread or use chemical thread lock to secure.

**ANCHOR ROD DETAIL**

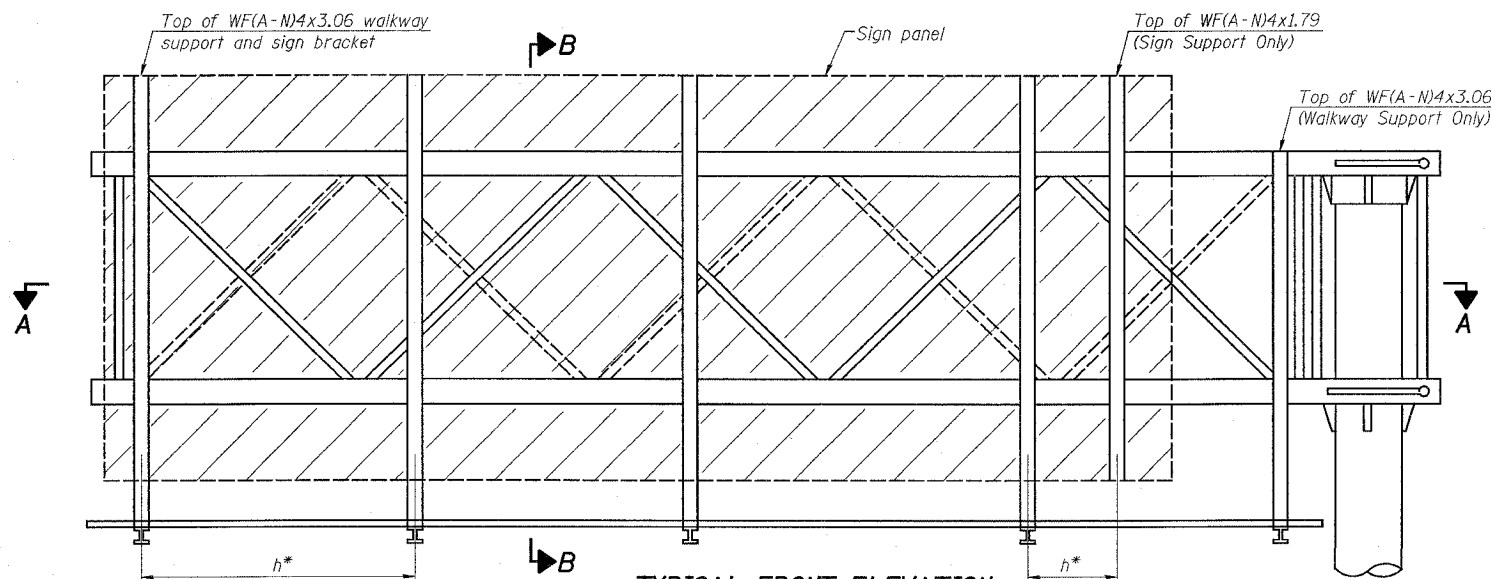
Anchor rods shall conform to AASHTO M314 Grade 105 and meet Charpy V-Notch (CVN) energy of 15 lb.-ft. at 10° F. before galvanizing. Galvanize the upper 18" (minimum\*\*\*), and associated M291, Grade A, C or DH heavy hex nuts and hardened washers per AASHTO M232. No welding shall be permitted on rods. Provide an unfinished nut at bottom, a hexagon locknut and washer above base plate and a leveling nut and washer below base plate. Nuts shall each be tightened with 200 lb.-ft. minimum torque against base plate. Before or after threading, but before galvanizing, each anchor rod shall be ultrasonically tested (UT) by a Level II or III inspector, qualified in accord with ANSI guidelines, using a straight beam, 1/2" diameter 3.5 mhz. transducer, to insure no rejectable flaws exist in the upper 18" (tension criteria). Cost of testing included in Drilled Shaft Concrete Foundations.

**CANTILEVER SIGN STRUCTURES  
TYPE II-C-A & III-C-A TRUSS SUPPORT POST  
ALUMINUM TRUSS & STEEL POST**

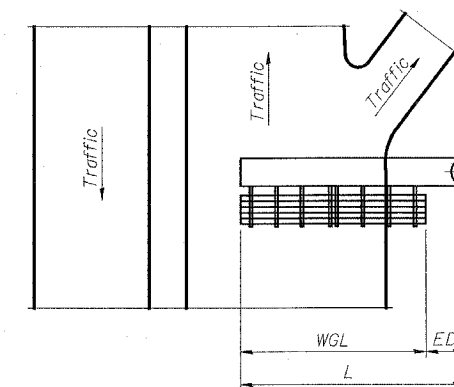
FAI ROUTE 64  
SECTION 95-(5,6)RS-1  
WASHINGTON COUNTY

NUMBER	REVISION	DATE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	75
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

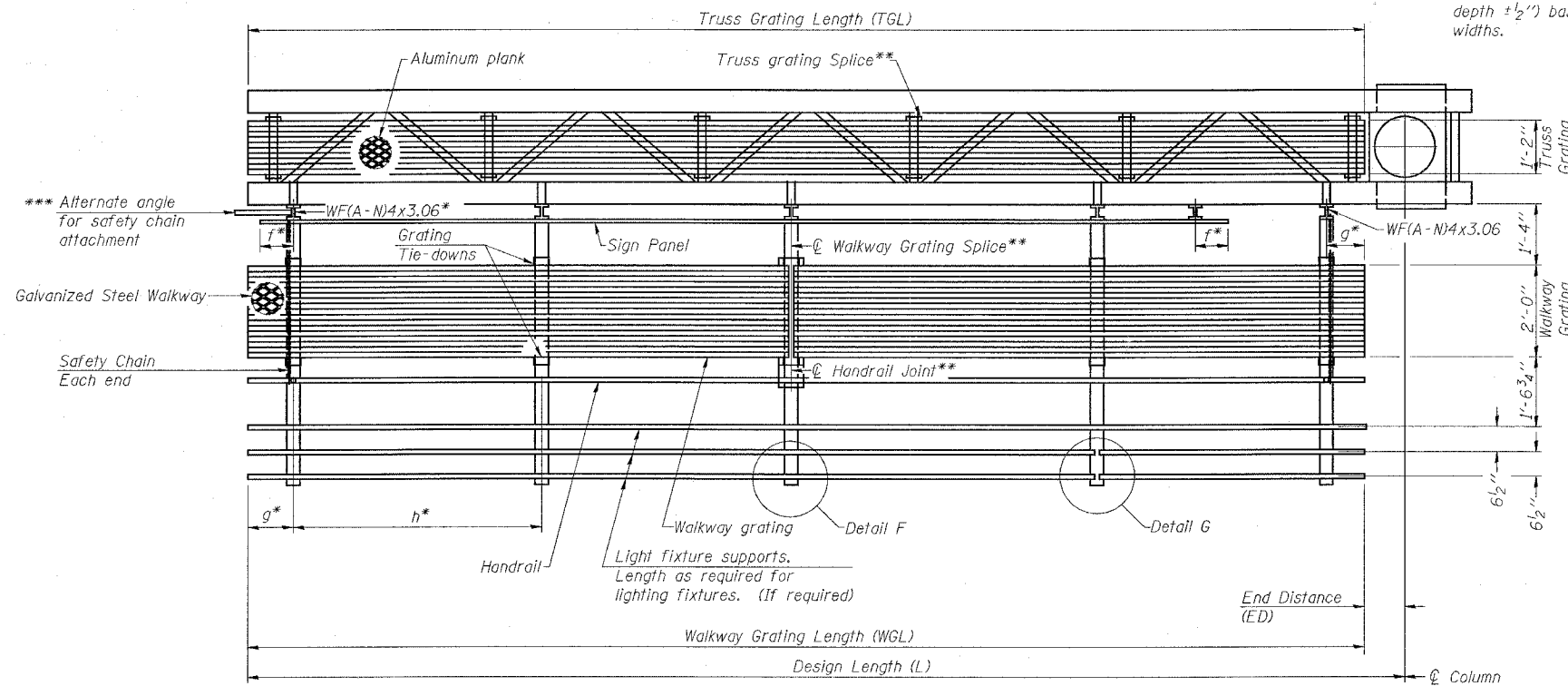


**TYPICAL FRONT ELEVATION**  
With lights and handrail omitted for clarity.



**PLAN WALKWAY AND HANDRAIL SKETCH**  
(Road plan beneath truss varies)

Walkway and truss grating dimensions are nominal and may vary (width ±1/2", depth ±1/2") based on available standard widths.



**SECTION A-A**

Truss grating to facilitate inspection shall run full length of cantilevers. Cost of truss grating is included in Overhead Sign Structure Cantilever.

Handrail and walkway grating shall span a minimum of three brackets between splices. \*\* Use and location of handrail joints or grating splices are optional, based on lengths needed and material availability.

$$TGL = L - \left( \frac{\text{Post O.D.}}{2} + 6'' \right)$$

NUMBER	REVISION	DATE

Structure Number	Station	WGL	ED	TGL
8C0951064R060.6	3267+10	20'-0"	12'-0"	30'-6"

**Notes:**

- Space walkway brackets WF(A-N)4x3.06 and sign brackets WF(A-N)4x1.79 for efficiency and within limits shown:
- f = 12" maximum, 4" minimum (End of sign to center of nearest bracket)
- g = 12" maximum, 4" minimum (End of walkway to center of nearest bracket)
- h = 6'-0" maximum (center to center sign and/or walkway support brackets, WF(A-N)4x1.79 or WF(A-N)4x3.06)
- \*\*\* If walkway bracket at safety chain location is behind sign, add angle to bracket. See alternate safety chain attachment on base sheet OSC-A-B.
- For details of sign placement, sign/walkway brackets, truss and walkway gratings, grating splices and Section B-B, see Base Sheet OSC-A-7S.
- For details of handrail, handrail joint, safety chain and Details F and G, see Base Sheet OSC-A-8.

**BRACKET TABLE**

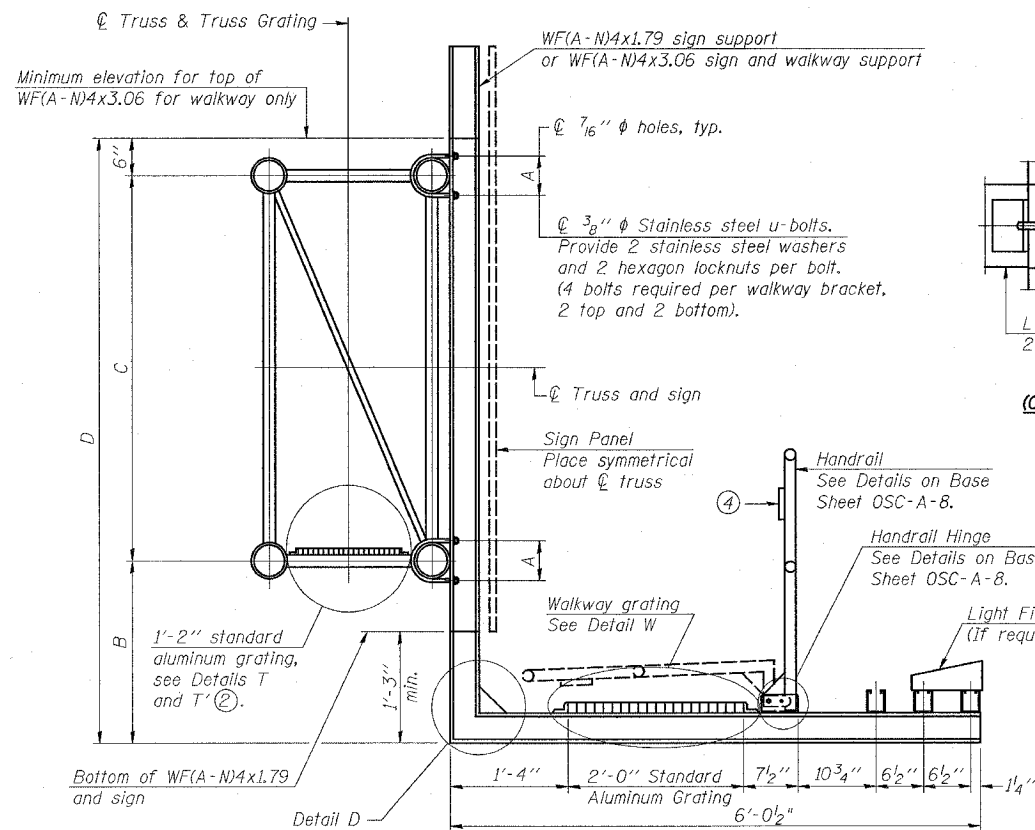
Sign Width		Number Brackets Required
Greater Than	Less Than or Equal To	
8'-0"	8'-0"	2
14'-0"	14'-0"	3
20'-0"	20'-0"	4
26'-0"	26'-0"	5
32'-0"	32'-0"	6

**CANTILEVER SIGN STRUCTURES  
ALTERNATE STEEL WALKWAY DETAILS  
ALUMINUM TRUSS & STEEL POST**

FAI ROUTE 64  
SECTION 95-(5,6)RS-1  
WASHINGTON COUNTY

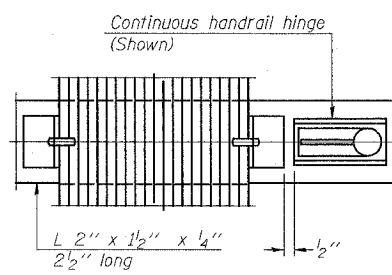
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 USER NAME = danielmyn

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.	TO STA.		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT			

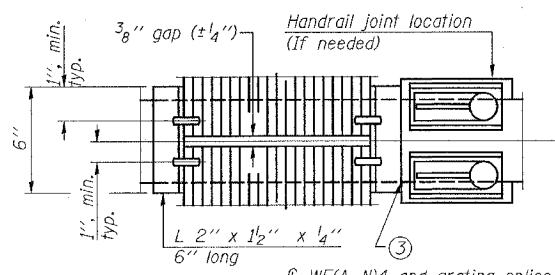


**SECTION B-B**

Sign shall be even with the top of the bracket, but it may extend no more than 6" above the top of the bracket for field adjustments.

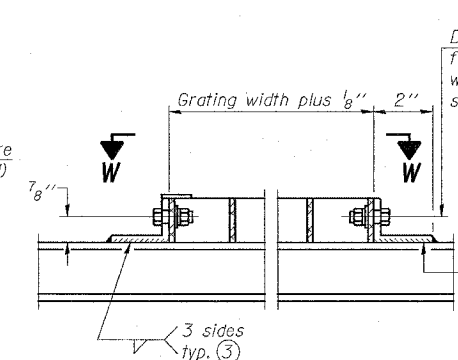


**(CONTINUOUS WALKWAY GRATING)**

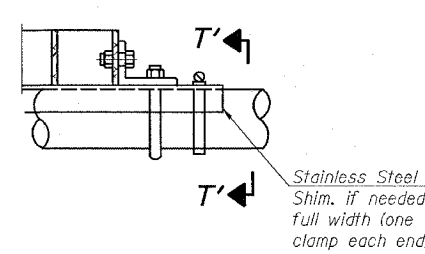


**(AT WALKWAY GRATING SPLICE)**

**SECTION W-W**

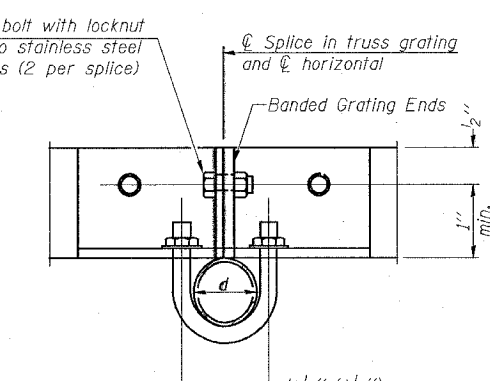


**DETAIL W**  
(Walkway grating)

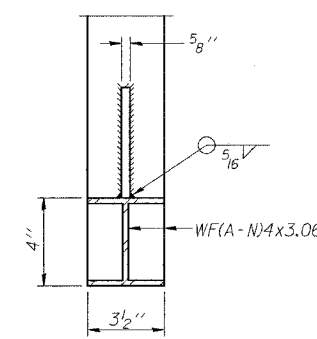


**DETAIL T'**

(Truss grating splice)  
Details not shown same as Detail T.  
Alternate materials may be used subject to the Engineer's review and approval.

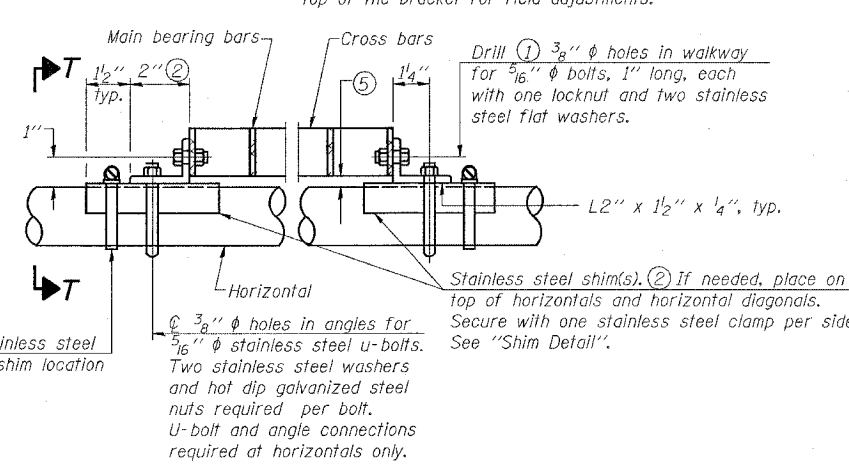


**SECTION T'-T'**



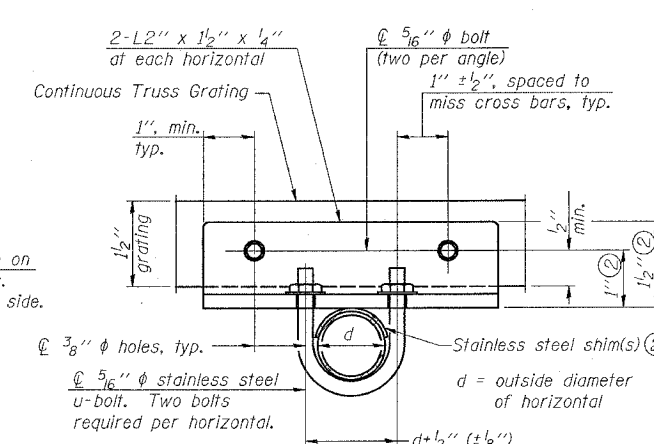
**SECTION D-D**

Screw type stainless steel tube clamp at shim location

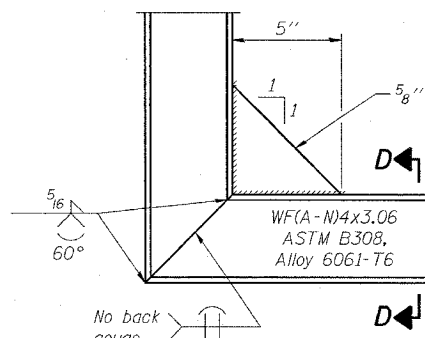


**DETAIL T**

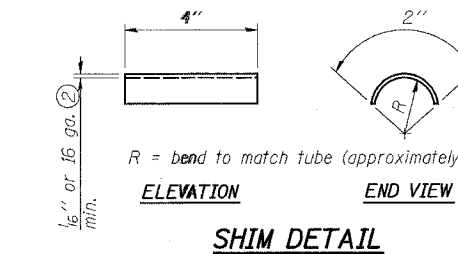
(Continuous Truss grating)



**SECTION T-T'**



**DETAIL D**



**SHIM DETAIL**

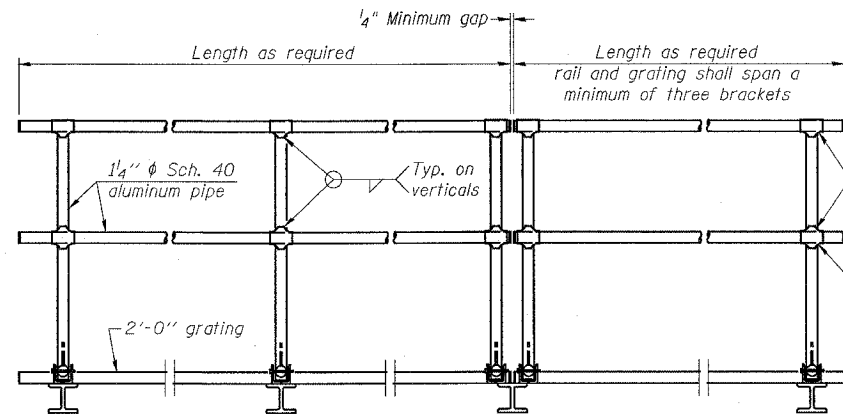
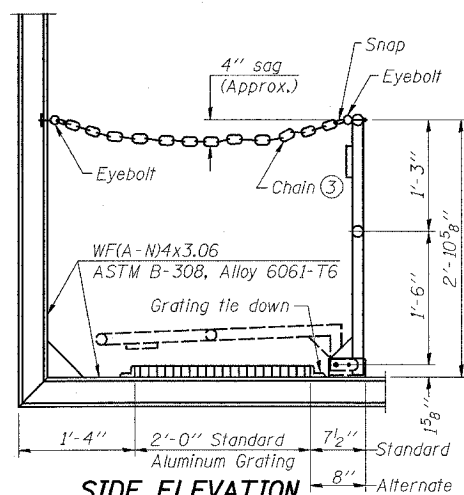
- Drilling holes in grating may be done in shop or field, based on Contractor's preference and subject to accurate alignment.
- Stainless steel shims shall be placed as shown in Detail T if needed to compensate for alignment variations between horizontal and diagonal pipes beyond adjustment provided by angles. Thicker shims may be used subject to shims performing properly.
- If Handrail Joint present, weld angle to WF(A-N)4 and 1/4" extension bars. (See Base Sheet OSC-A-8.)
- 1/8" x 1/2" x 2" welded to handrail posts to protect locations that contact grating.
- Tube to grating gap may vary from 0 to 1/2", max. to align walkway, allow for camber, etc.

Structure Number	Station	A	B	C	D
8C0951064R060.6	3267+10	7 1/2"	3'-6"	7'-0"	11'-0"

**CANTILEVER SIGN STRUCTURES  
WALKWAY DETAILS  
ALUMINUM TRUSS & STEEL POST**

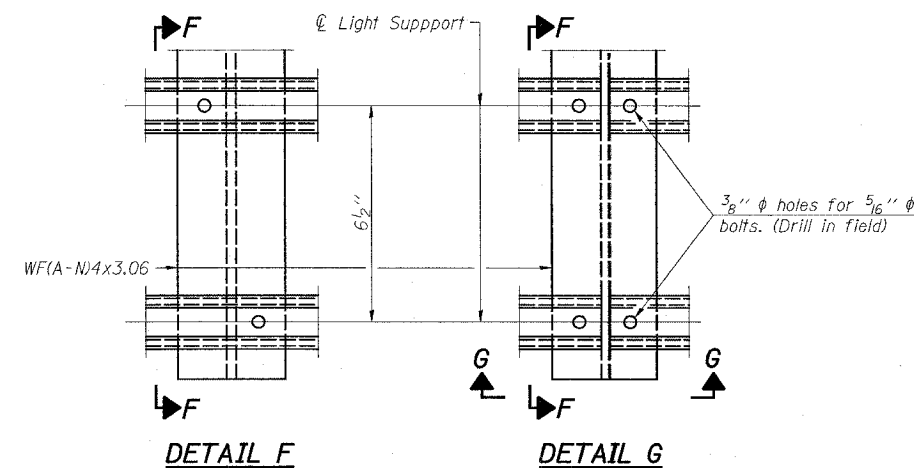
FAI ROUTE 64  
SECTION 95-(5,6)RS-1  
WASHINGTON COUNTY

PLOT DATE = 6/21/2007  
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 USER NAME = dmiller



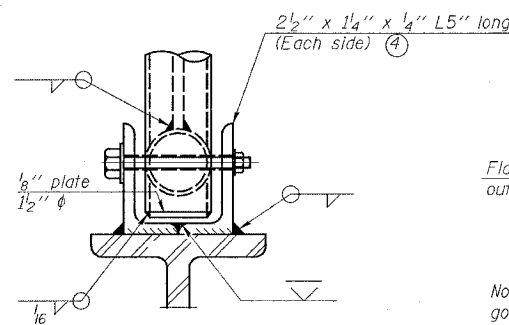
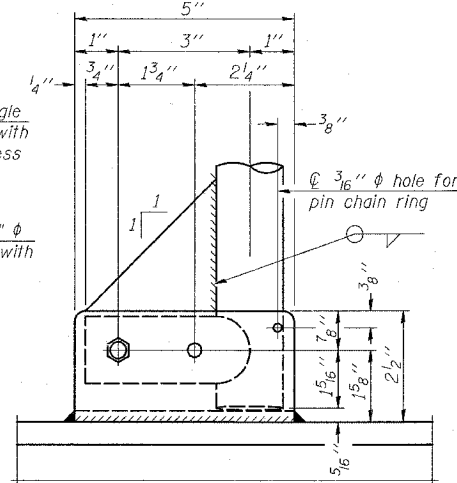
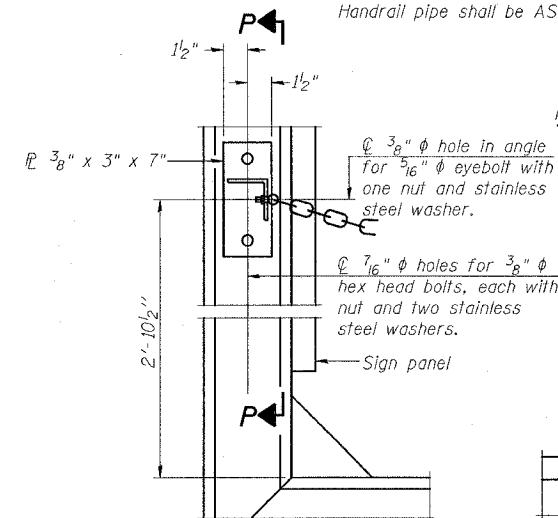
**HANDRAIL DETAILS**

- Install standard force-fit end caps or weld  $\frac{1}{8}$ " end plates with  $\frac{1}{8}$ " c.f.w. and grind smooth. (All rail ends)
- Horizontal handrail member shall be continuous thru fitting. Provide  $\frac{7}{16}$ " hole in fitting for  $\frac{3}{8}$ " bolt. Field drill  $\frac{7}{16}$ " hole in horizontal rail member. Provide locknut and two stainless steel washers for bolt. (Use  $\frac{5}{16}$ " eyebolts in  $\frac{7}{16}$ " holes on top rail at ends only.)



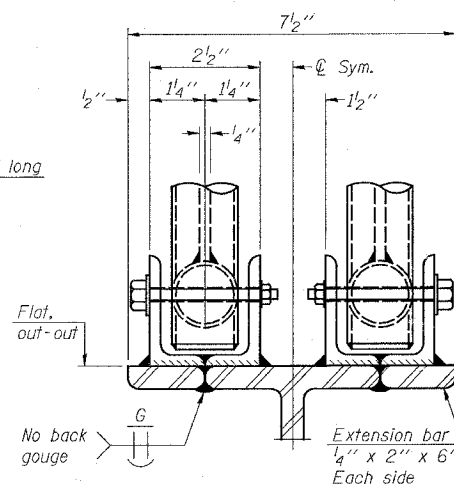
**SECTION F-F**  
**SECTION G-G**  
**LIGHTING FIXTURE MOUNTS (IF REQUIRED)**

- Field cut ends of light support channels shall be free of burrs or hazardous projections and coated with zinc-rich primer or equivalent.



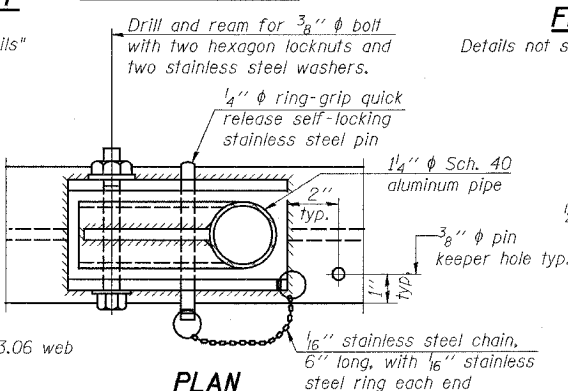
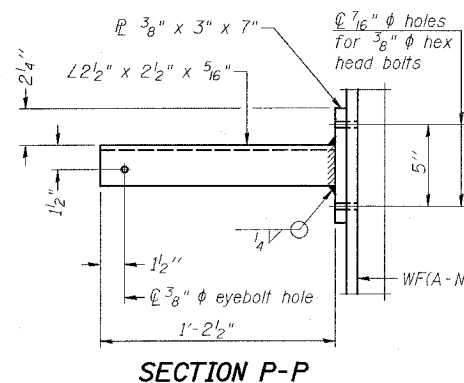
**FRONT ELEVATION**

Details not shown same as "ELEVATION" at right.

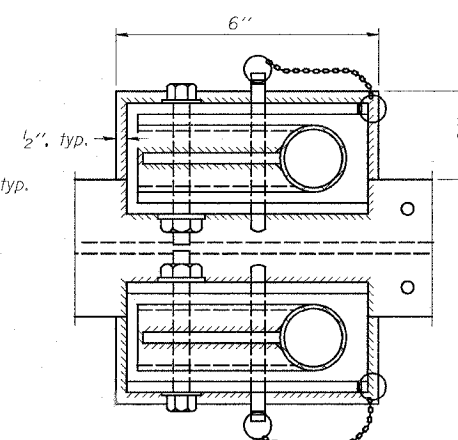


**ELEVATION AT HANDRAIL JOINT**

Details not shown same as "FRONT ELEVATION"

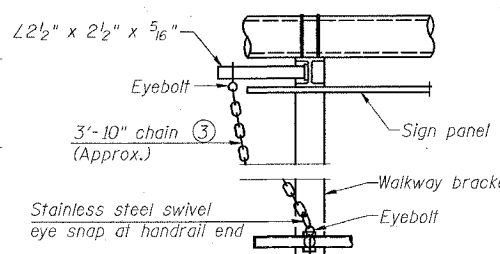


**DETAIL E HANDRAIL HINGE**



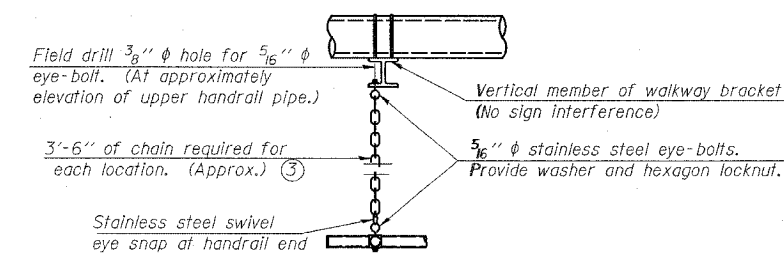
**PLAN AT HANDRAIL JOINT**

Details not shown same as "PLAN"



- $\frac{3}{16}$ " Type 304L stainless steel chain, approximately 12 links per foot.

- Extrusions may be used in lieu of the details shown, with approval of the Engineer.



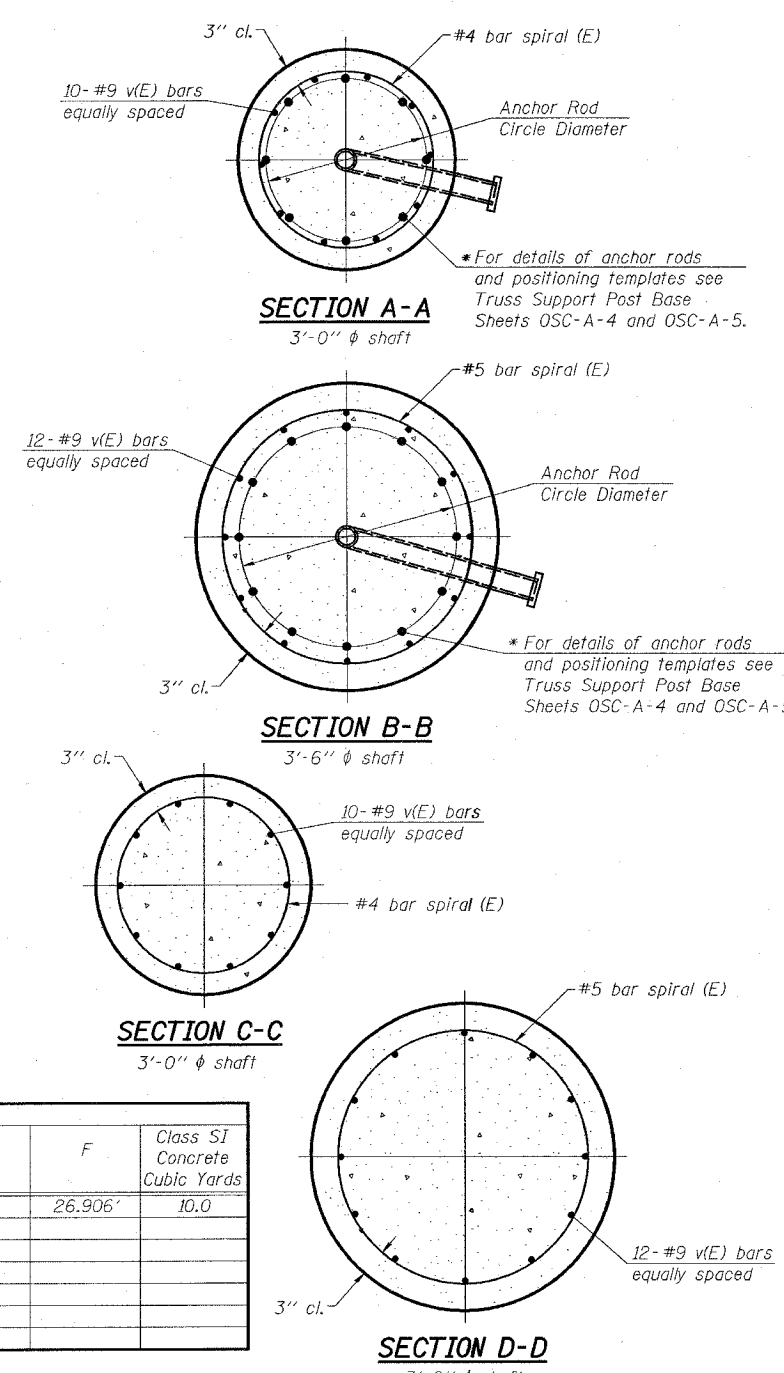
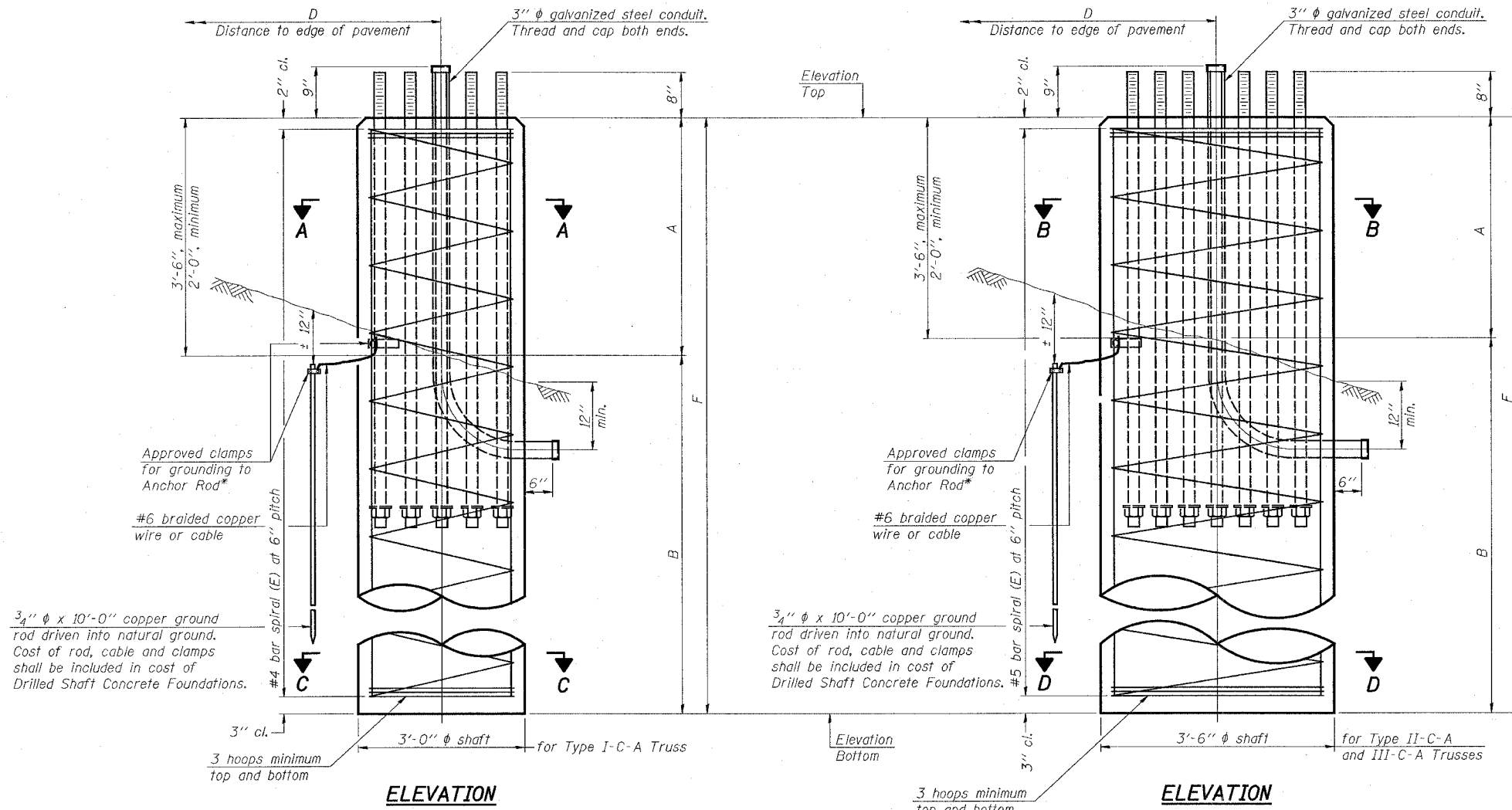
**CANTILEVER SIGN STRUCTURES**  
**HANDRAIL DETAILS**  
**ALUMINUM TRUSS & STEEL POST**

FAI ROUTE 64  
SECTION 95-(5,6)RS-1  
WASHINGTON COUNTY

NUMBER	REVISION	DATE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	78
STA.		TO STA.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

\* Grind anchor rod to bright finish at ground clamp location before installing clamp.



**NOTES:**  
 The foundation dimensions shown in the Foundation Design Table are based on the presence of mostly cohesive soils with an average Unconfined Compressive Strength ( $Q_u$ ) of at least 1.25 tsf, which must be determined by previous soil investigations at the jobsite. When other conditions are indicated, the boring data will be included in the plans and the foundation dimensions shown in the Foundation Data Table will be the result of site specific designs.  
 If the conditions encountered are different than those indicated, the Contractor shall notify the Engineer to determine if the foundation dimensions need to be modified. If dimensions "B" or "F" are revised by more than 12" by the Contractor, "as-built" plans shall be prepared and submitted to the District Bureau of Operations for future reference.  
 No sonotubes or decomposable forms shall be used below the lower conduit entrance. Permanent metal forms or other shielding may not be left in place below that elevation without the Engineer's written permission.  
 Concrete shall be placed monolithically, without construction joints.  
 Backfill shall be placed per Article 502 of Standard Specification and prior to erection of support column.  
 A normal surface finish followed by a Bridge Seat Sealer application will be required on concrete surfaces above the lowest elevation 6" below Finished ground line. Cost included in "Drilled Shaft Concrete Foundation".

Structure Number	Station	Truss Type	Shaft Diameter	Elevation Top	Elevation Bottom	$Q_u$	A	B	F	Class SI Concrete Cubic Yards
8C0951064R060.6	3267+10	III-C-A	3'-6"	564.951'	538.045'	2.45	2.906'	24'	26.906'	10.0

Truss Type	Post Base Sheet	Maximum Cantilever Length (ft)	Maximum Total Sign Area (sq ft)	Shaft Diameter (in)	"B" Depth (ft)	Anchor Rods		Anchor Rod Circle Diameter (in)
						No.	Diameter (in)	
I-C-A	OSC-A-4	25	170	3.0	16.0	8	2	22
II-C-A	OSC-A-5	30	170	3.5	17.0	12	2	30
II-C-A	OSC-A-5	30	340	3.5	21.5	12	2	30
III-C-A	OSC-A-5	35	170	3.5	19.0	12	2	30
III-C-A	OSC-A-5	35	250	3.5	22.5	12	2	30
III-C-A	OSC-A-5	35	400	3.5	26.5	12	2	30
III-C-A	OSC-A-5	40	400	3.5	32.0	12	2	30

NUMBER	REVISION	DATE

**CANTILEVER SIGN STRUCTURES  
 DRILLED SHAFT  
 ALUMINUM TRUSS & STEEL POST**

FAI ROUTE 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

PLOT DATE = 6/21/2007  
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	79
STA. _____		TO STA. _____		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



**Illinois Department of Transportation**  
 Division of Highways  
 Illinois Department of Transportation

**SOIL BORING LOG**

Page 1 of 1

Date 4/98

ROUTE 64 DESCRIPTION SR 14, SR 95, TWP. 4S, R. 10E, S. 27N

LOGGED BY E. Stewart

SECTION SR 14, SR 95, TWP. 4S, R. 10E, S. 27N  
 COUNTY Washington DRILLING METHOD Hammer Hammer TYPE 140P Automatic

DEPTH (ft)	SOIL DESCRIPTION	MOISTURE (%)	WATER CONTENT (%)	UNSATURATED SWELLING (%)	UNSATURATED SHRINKAGE (%)	UNSATURATED PLASTICITY INDEX (%)	UNSATURATED LIQUIDITY INDEX (%)	UNSATURATED PLASTICITY INDEX (%)	UNSATURATED LIQUIDITY INDEX (%)
4	Light Gray Silty LOAM								
7	Light Gray Speckled Red Silty CLAY (medium)								
10	Light Brown Speckled Red Silty LOAM								
13	Light Gray Speckled Red Silty CLAY								
16									
19									
22									
25									
28									
31									
34									
37									
40									
43									
46									
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79									
82									
85									
88									
91									
94									
97									
100									

The Unconfined Compressive Strength (UCS) Values Made in Indicated by (U-Blow, S-Steel, P-Plastrometer)  
 The SPT (N) value is the sum of the last two blow values in each sampling zone (ASTM D 1586)

HSR, Item 127 (Rev. 8-98)

PLOT DATE = 8/21/2007  
 PLOT SCALE = 80,000 / IN.  
 USER NAME = dmdm1man.jn

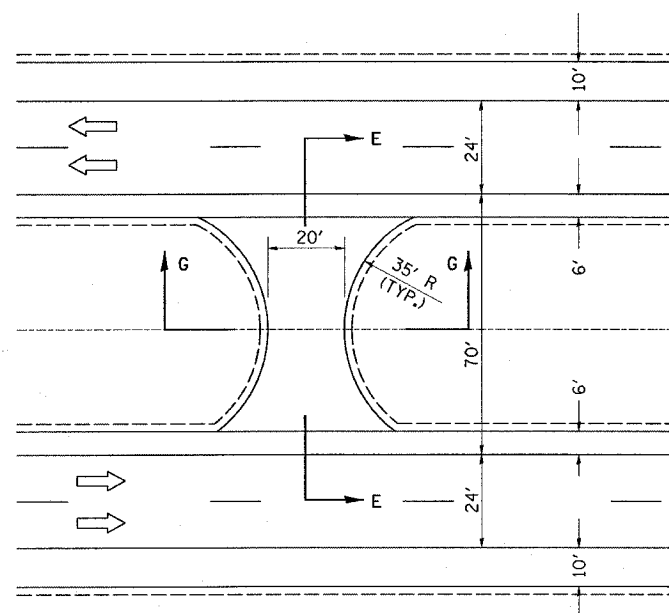
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION	
NAME	DATE	SOIL BORING LOGS	
		FAI ROUTE 64	
		SECTION 95-(5,6)RS-1	
		WASHINGTON COUNTY	
		SCALE: VERT.	DRAWN BY
		HORIZ.	CHECKED BY
		DATE	



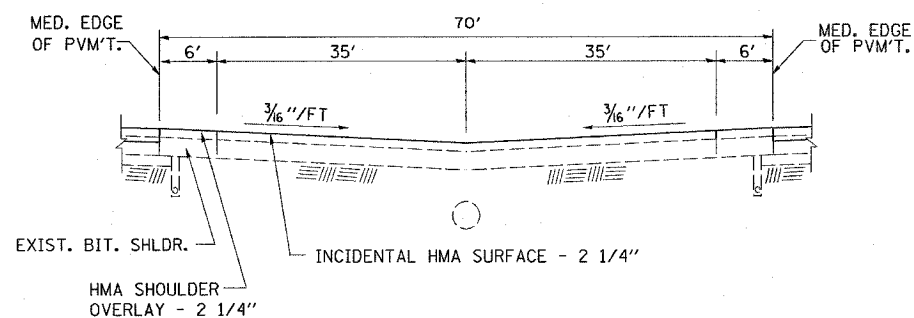


FAI ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA. _____ TO STA. _____		CONTRACT NO.: 76A39		

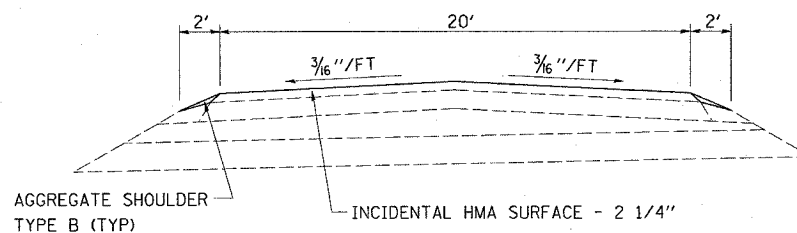
PLAN	DATE
BY	
CHECKED	
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FILE NAME	



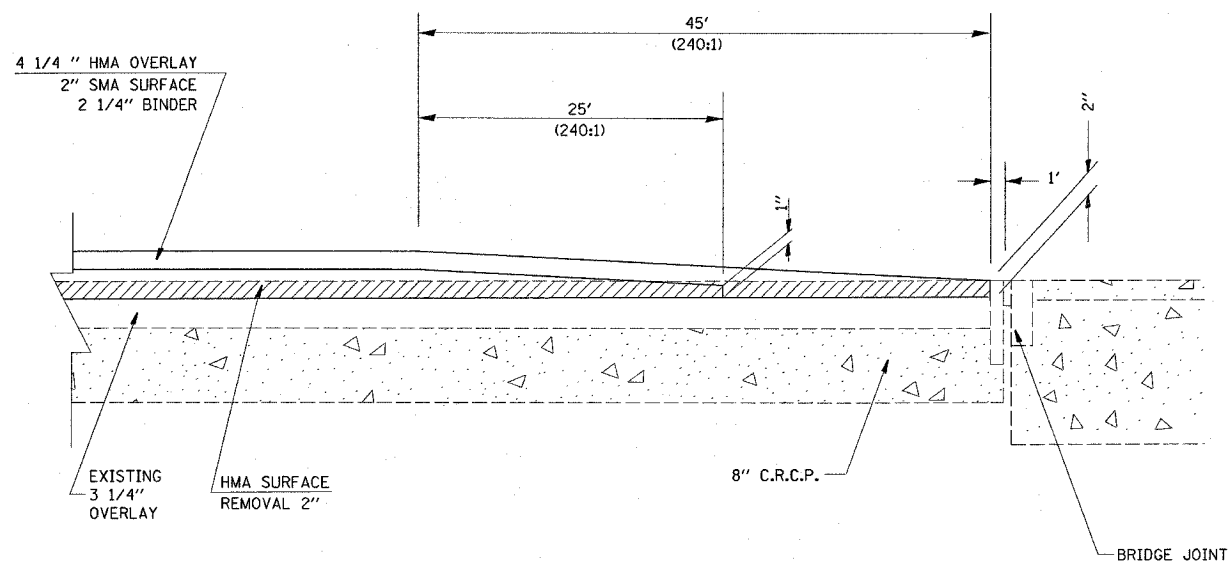
**MEDIAN CROSS-OVER DETAIL**  
 STA. 3244+30  
 STA. 3350+00



**SECTION E-E**



**SECTION G-G**



**BRIDGE JOINT DETAIL**  
 TYPICAL (2) STRUCTURES  
 095-0061 (RAILROAD)  
 095-0063 (U.S. 51)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**MISCELLANEOUS DETAILS**  
 FAI ROUTE 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY

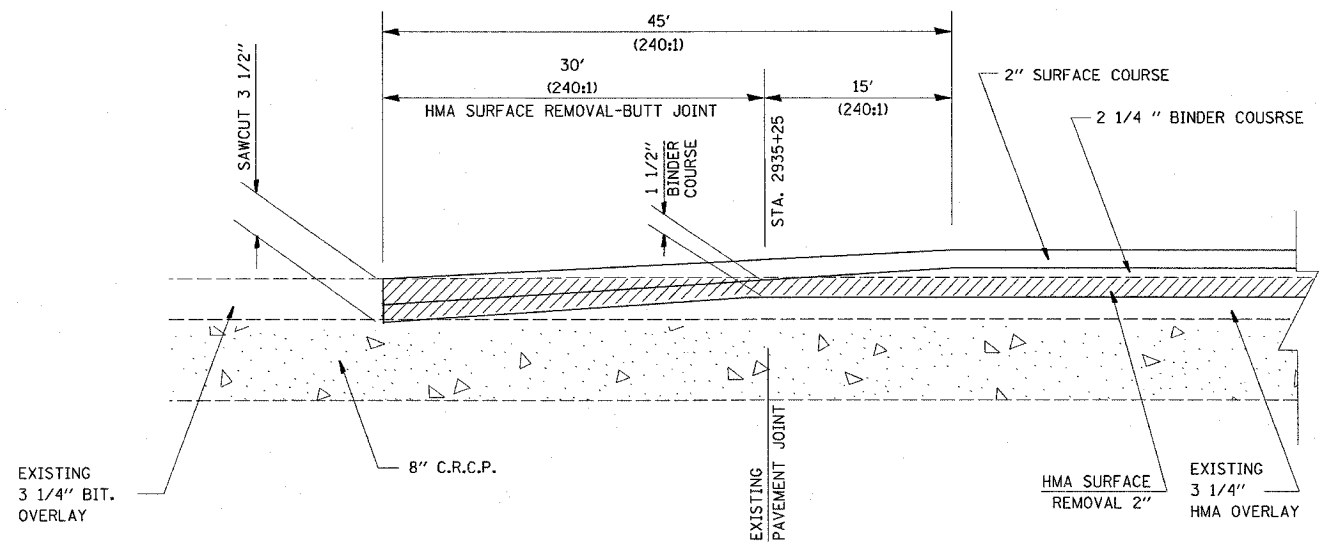
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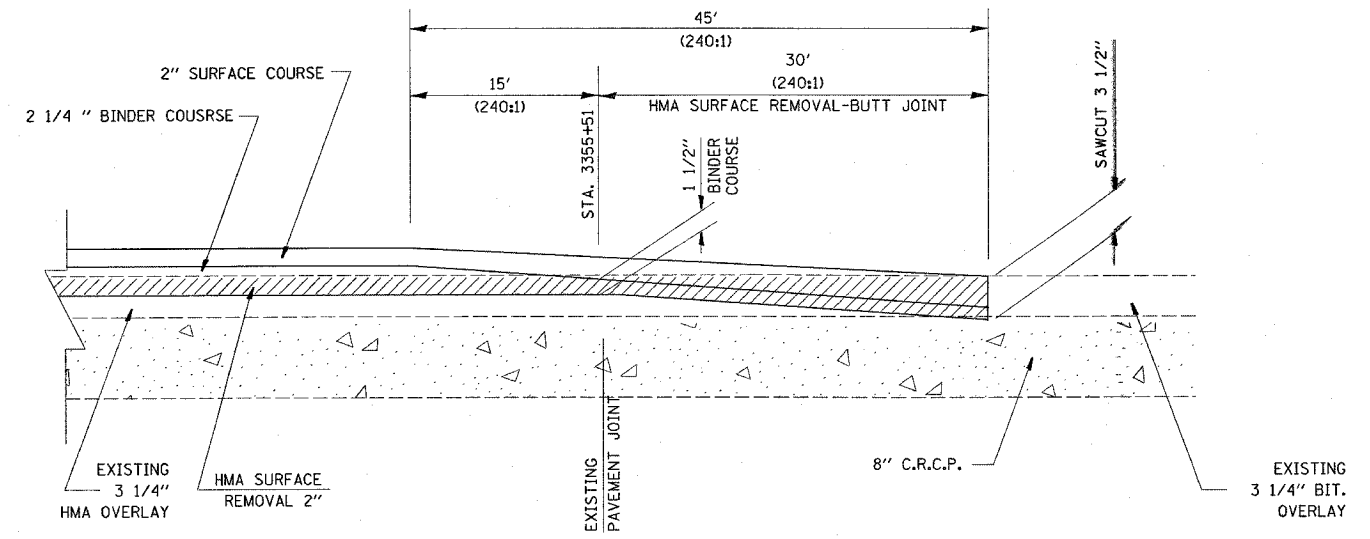
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 8/21/2007

FILE ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-(5,6)RS-1	WASHINGTON	83	82
CONTRACT NO.: T6A38				

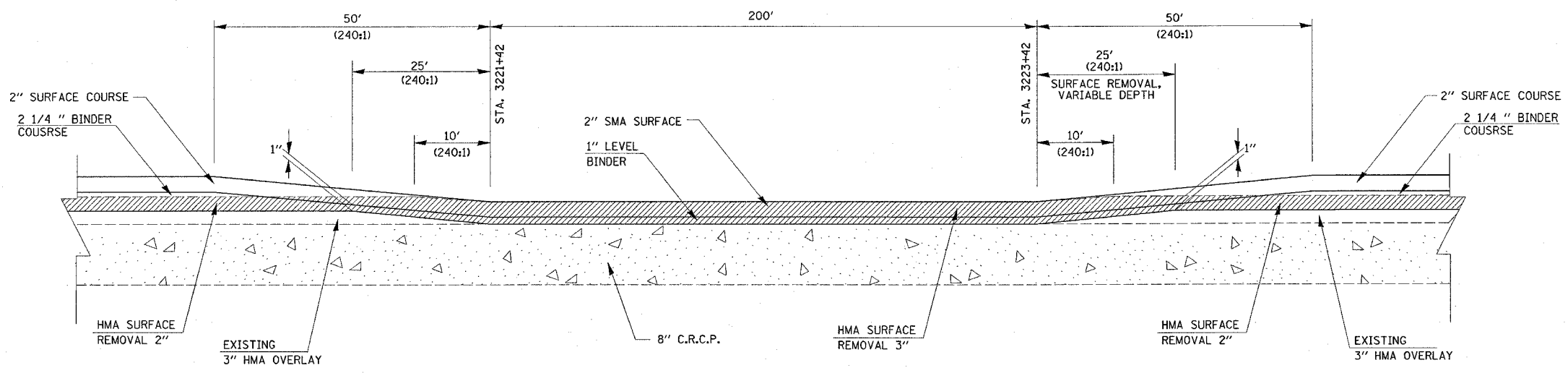
PLAN	DATE
SURVEYED	
NOTED	
CHECKED	
BY	
NO.	
FILE NAME	



WEST END JOINT DETAIL



EAST END JOINT DETAIL



3" MILLING DETAIL  
UNDER STRUCTURE 095-0060  
WESTBOUND ONLY (TOWER RD.)

REVISIONS	
NAME	DATE

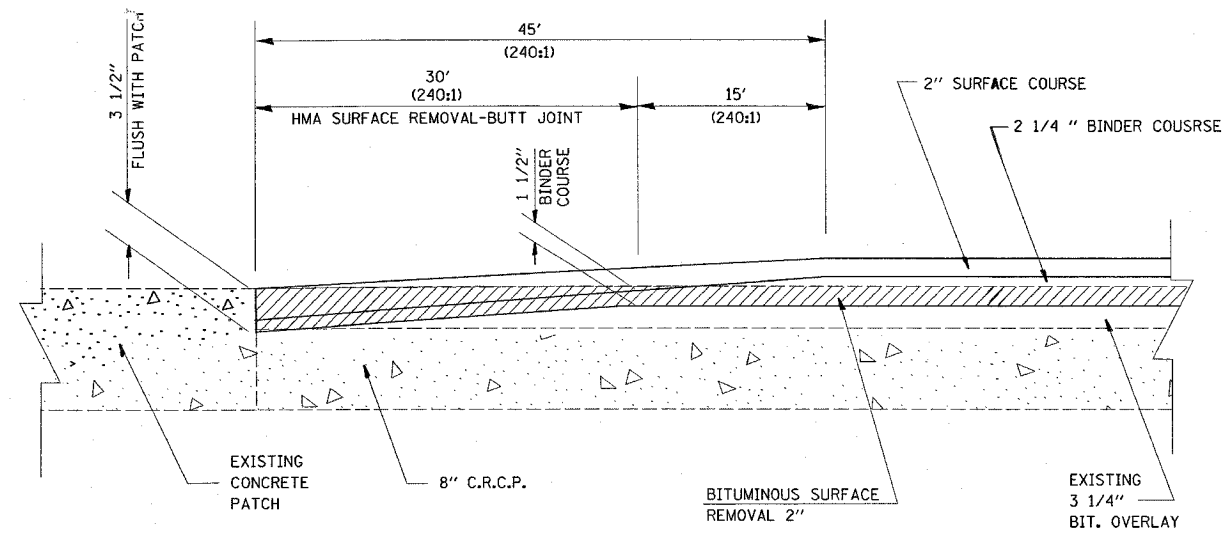
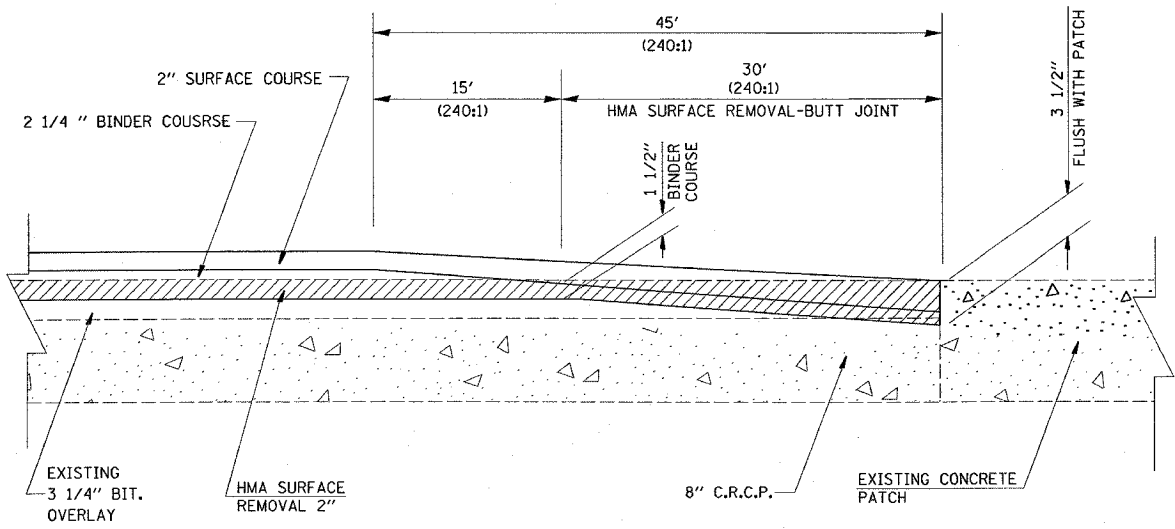
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**JOINT DETAILS**  
FAI ROUTE 64  
SECTION 95-(5,6)RS-1  
WASHINGTON COUNTY  
DRAWN BY:

PLOT DATE: 8/21/2007

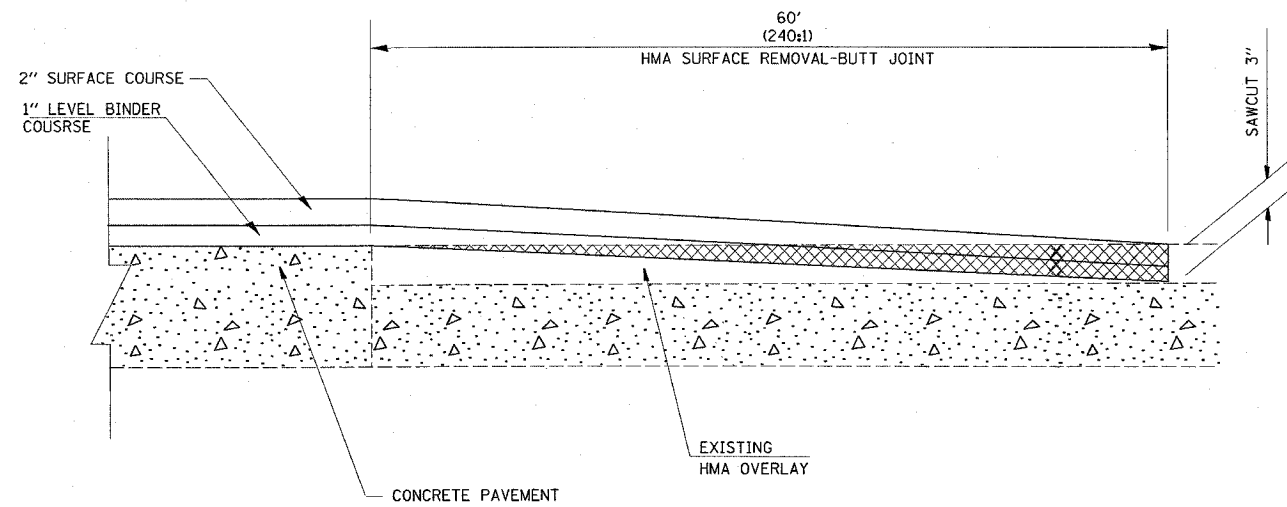
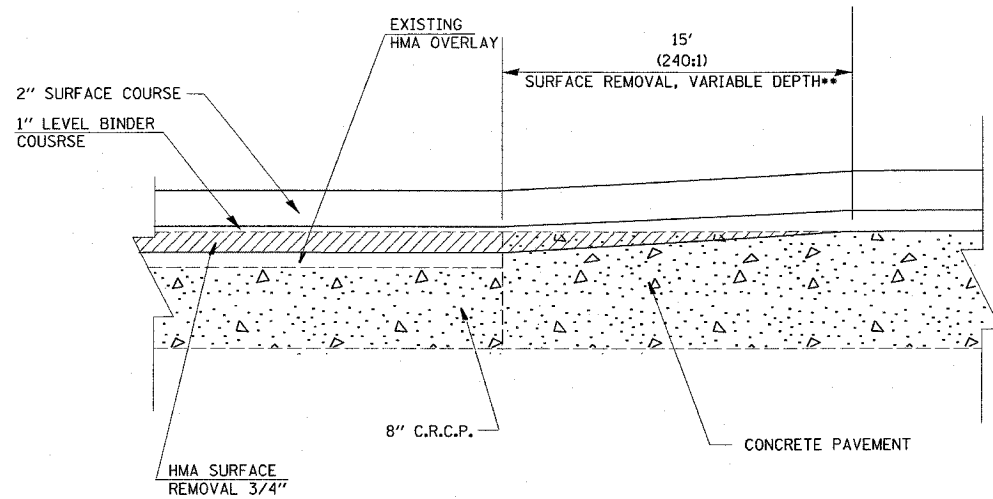
8/21/2007 10:00 AM  
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SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
95-(5,6)RS-1	WASHINGTON	83	83
STA. _____ TO STA. _____		CONTRACT NO.: 76A38	

DATE	BY
8/21/2007	WJF
PROJECT	FAI ROUTE 64
SECTION	SECTION 95-(5,6)RS-1
CADD FILE NAME	
NO. OF PAGES CHECKED	
DATE CHECKED	
BY	
DATE	
PLAN	
NO. OF SHEETS	
DATE	



**OMISSION JOINT DETAIL**  
 TYPICAL (2) PATCHES UNDER STRUCTURE  
 095-0059 (HOYLETON RD.)  
 095-0060 EASTBOUND ONLY (TOWER RD.)



**RAMP JOINT DETAIL**  
 TYPICAL (4) RAMPS  
 U.S. 51 INTERCHANGE

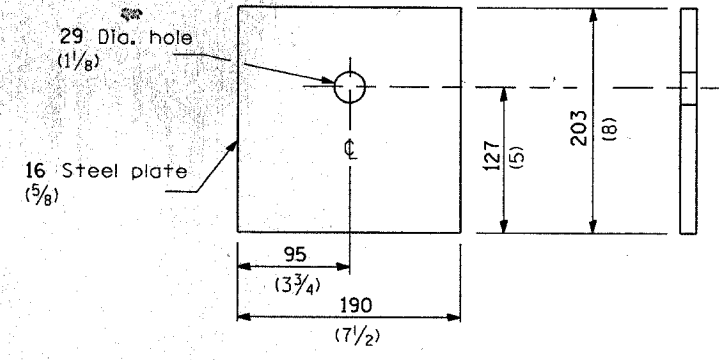
8/21/2007  
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 8/21/2007

REVISIONS	
NAME	DATE

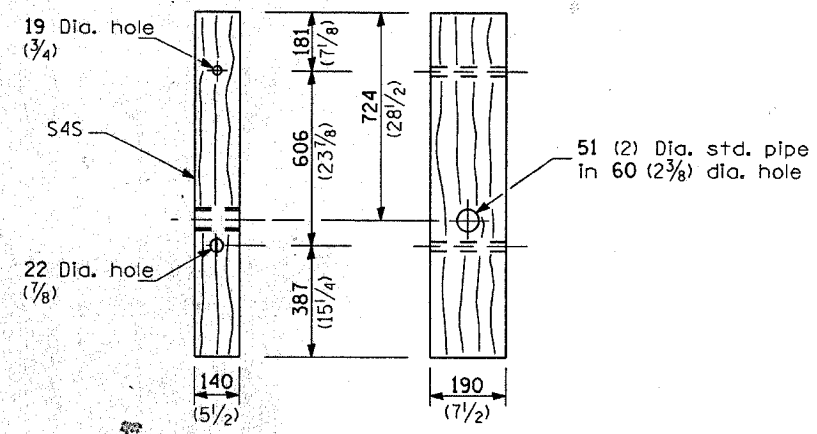
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**JOINT DETAILS**  
 FAI ROUTE 64  
 SECTION 95-(5,6)RS-1  
 WASHINGTON COUNTY  
 DRAWN BY:

PLOT DATE: 8/21/2007

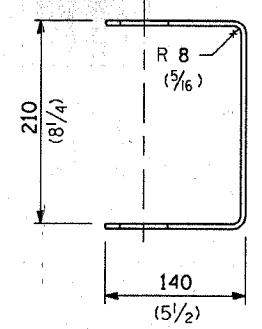
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	95-15.6	RS-1 WASHINGTON	83	83A.
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



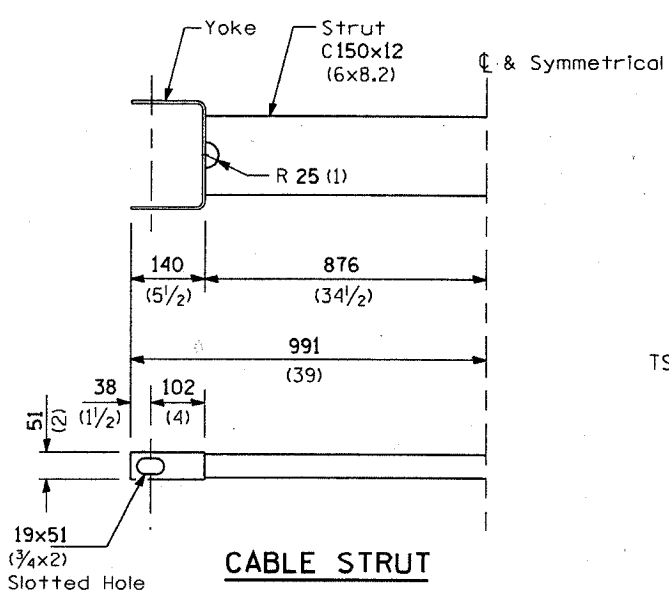
**BEARING PLATE K**



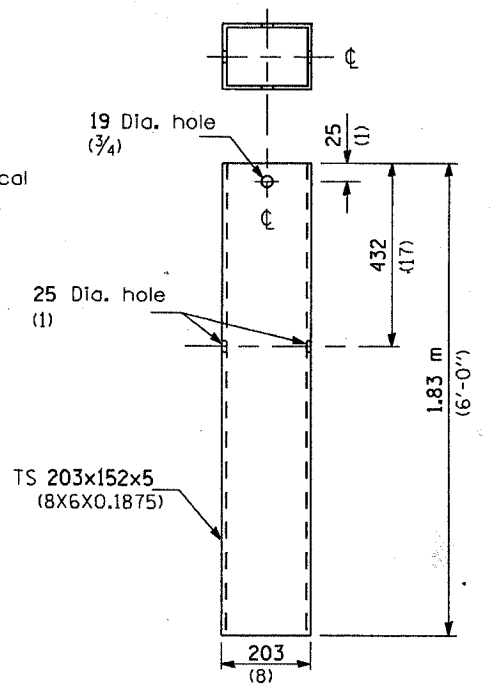
**WOOD POST**



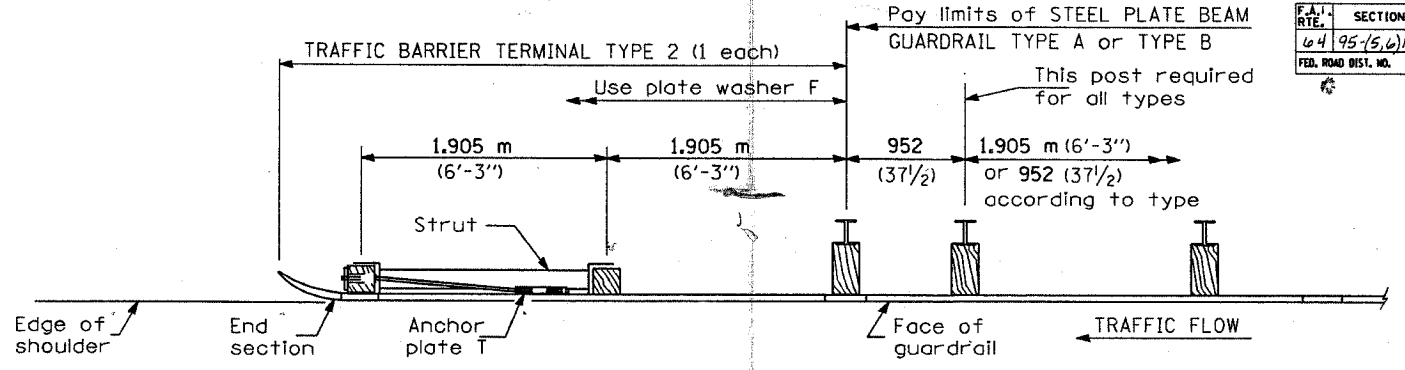
**YOKE**



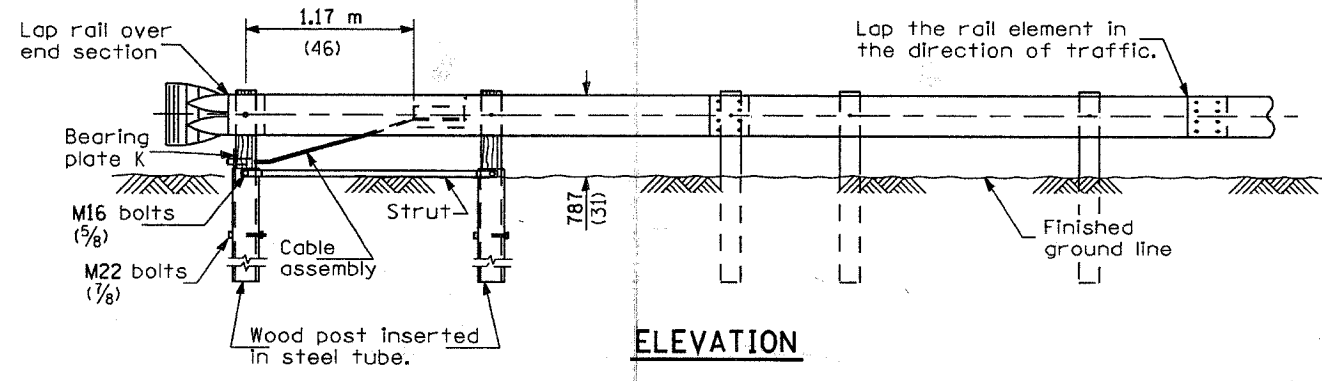
**CABLE STRUT**



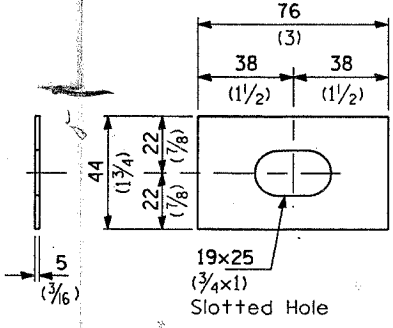
**STEEL TUBE**



**PLAN**



**ELEVATION**



**PLATE WASHER F**

**GENERAL NOTES**

See Standard 630001 for details of guardrail not shown.

The bearing plate K shall be held in position by (2) two eight penny nails driven into the post and bent over the top of the plate.

All dimensions are in millimeters (inches) unless otherwise shown.

**TRAFFIC BARRIER TERMINAL, TYPE 2**

**DETAIL**