

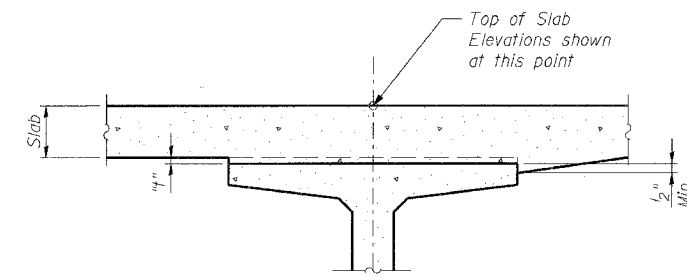
ROUTE No.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 827	12Z-3, 12BR	WABASH, IL GIBSON, IN	158	35
STA.		TO STA.		
F.H.W.A. REGION		ILLINOIS	PROJECT	
BRIDGE SHEET S19 OF S114				CONTRACT NO. 94450

GIRDER 1 AND GIRDER 6, UNIT 4

Location	Station	Offset	Theoretical Grade Elevation	Elevation Adjusted For Dead Load Deflection
⊕ Brg. ⊙ Pier 13 (Span 14)	1042+46.42	-21.458	419.537	419.537
14A	1042+56.42	-21.458	419.487	419.517
14B	1042+66.42	-21.458	419.437	419.497
14C	1042+76.42	-21.458	419.387	419.473
14D	1042+86.42	-21.458	419.337	419.442
14E	1042+96.42	-21.458	419.287	419.403
14F	1043+06.42	-21.458	419.237	419.357
14G	1043+16.42	-21.458	419.187	419.301
14H	1043+26.42	-21.458	419.137	419.237
14J	1043+36.42	-21.458	419.087	419.165
14K	1043+46.42	-21.458	419.037	419.088
14L	1043+56.42	-21.458	418.987	418.987
⊕ Brg. ⊙ Pier 14 (Span 14)	1043+63.75	-21.458	418.950	418.950
⊕ Brg. ⊙ Pier 14 (Span 15)	1043+66.25	-21.458	418.938	418.938
15A	1043+76.25	-21.458	418.888	418.916
15B	1043+86.25	-21.458	418.838	418.894
15C	1043+96.25	-21.458	418.788	418.869
15D	1044+06.25	-21.458	418.738	418.837
15E	1044+16.25	-21.458	418.688	418.799
15F	1044+26.25	-21.458	418.638	418.753
15G	1044+36.25	-21.458	418.588	418.698
15H	1044+46.25	-21.458	418.538	418.634
15J	1044+56.25	-21.458	418.488	418.564
15K	1044+66.25	-21.458	418.438	418.488
15L	1044+76.25	-21.458	418.388	418.409
⊕ Brg. ⊙ Pier 15 (Span 15)	1044+83.75	-21.458	418.351	418.351
⊕ Brg. ⊙ Pier 15 (Span 16)	1044+86.25	-21.458	418.338	418.338
16A	1044+96.25	-21.458	418.288	418.318
16B	1045+06.25	-21.458	418.238	418.297
16C	1045+16.25	-21.458	418.188	418.272
16D	1045+26.25	-21.458	418.138	418.241
16E	1045+36.25	-21.458	418.088	418.203
16F	1045+46.25	-21.458	418.038	418.157
16G	1045+56.25	-21.458	417.988	418.102
16H	1045+66.25	-21.458	417.938	418.038
16J	1045+76.25	-21.458	417.888	417.967
16K	1045+86.25	-21.458	417.838	417.891
16L	1045+96.25	-21.458	417.788	417.812
⊕ Brg. ⊙ Pier 16 (Span 16)	1046+04.25	-21.458	417.748	417.748
⊕ Brg. ⊙ Pier 16 (Span 17)	1046+05.75	-21.458	417.741	417.741
17A	1046+15.75	-21.458	417.691	417.721
17B	1046+25.75	-21.458	417.641	417.700
17C	1046+35.75	-21.458	417.591	417.676
17D	1046+45.75	-21.458	417.541	417.645
17E	1046+55.75	-21.458	417.491	417.607
17F	1046+65.75	-21.458	417.441	417.561
17G	1046+75.75	-21.458	417.391	417.506
17H	1046+85.75	-21.458	417.341	417.443
17J	1046+95.75	-21.458	417.291	417.372
17K	1047+05.75	-21.458	417.241	417.296
17L	1047+15.75	-21.458	417.191	417.217
⊕ Brg. ⊙ Pier 17 (Span 17)	1047+24.25	-21.458	417.149	417.149

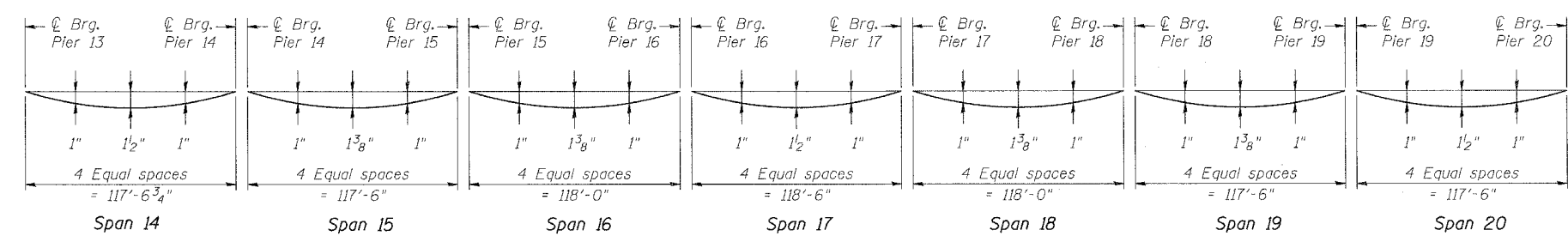
GIRDER 1 AND GIRDER 6, UNIT 4 (Continued)

Location	Station	Offset	Theoretical Grade Elevation	Elevation Adjusted For Dead Load Deflection
⊕ Brg. ⊙ Pier 17 (Span 18)	1047+25.75	-21.458	417.141	417.141
18A	1047+35.75	-21.458	417.091	417.121
18B	1047+45.75	-21.458	417.041	417.100
18C	1047+55.75	-21.458	416.991	417.075
18D	1047+65.75	-21.458	416.941	417.044
18E	1047+75.75	-21.458	416.891	417.006
18F	1047+85.75	-21.458	416.841	416.960
18G	1047+95.75	-21.458	416.791	416.905
18H	1048+05.75	-21.458	416.741	416.841
18J	1048+15.75	-21.458	416.691	416.770
18K	1048+25.75	-21.458	416.641	416.694
18L	1048+35.75	-21.458	416.591	416.615
⊕ Brg. ⊙ Pier 18 (Span 18)	1048+43.75	-21.458	416.551	416.551
⊕ Brg. ⊙ Pier 18 (Span 19)	1048+46.25	-21.458	416.539	416.539
19A	1048+56.25	-21.458	416.489	416.517
19B	1048+66.25	-21.458	416.439	416.496
19C	1048+76.25	-21.458	416.389	416.470
19D	1048+86.25	-21.458	416.339	416.439
19E	1048+96.25	-21.458	416.289	416.400
19F	1049+06.25	-21.458	416.239	416.354
19G	1049+16.25	-21.458	416.189	416.299
19H	1049+26.25	-21.458	416.139	416.235
19J	1049+36.25	-21.458	416.089	416.164
19K	1049+46.25	-21.458	416.039	416.088
19L	1049+56.25	-21.458	415.989	416.010
⊕ Brg. ⊙ Pier 19 (Span 19)	1049+63.75	-21.458	415.952	415.952
⊕ Brg. ⊙ Pier 19 (Span 20)	1049+66.25	-21.458	415.939	415.939
20A	1049+76.25	-21.458	415.889	415.919
20B	1049+86.25	-21.458	415.839	415.898
20C	1049+96.25	-21.458	415.789	415.873
20D	1050+06.25	-21.458	415.739	415.843
20E	1050+16.25	-21.458	415.689	415.805
20F	1050+26.25	-21.458	415.639	415.759
20G	1050+36.25	-21.458	415.589	415.705
20H	1050+46.25	-21.458	415.539	415.641
20J	1050+56.25	-21.458	415.489	415.569
20K	1050+66.25	-21.458	415.439	415.492
20L	1050+76.25	-21.458	415.389	415.412
⊕ Brg. ⊙ East Abutment	1050+83.75	-21.458	415.352	415.352
Bk. of East Abutment	1050+87.33	-21.458	415.334	415.334



To determine "f": After all precast prestressed beams have been erected, elevations of the top flanges of the beams shall be taken at intervals shown in the tables. These elevations subtracted from the "Theoretical Grade Elevations Adjusted For Dead Load Deflections" minus slab thickness, equals the fillet heights "f" above top flanges of beams.

FILLET HEIGHTS - 72" BULB-T GIRDER
(Fascia Girder Shown)



DEAD LOAD DEFLECTION DIAGRAM - UNIT 4
(Includes weight of concrete, excluding beams).

Note:
The above deflections are not for use in the field if the Engineer is working from the Theoretical Grade Elevations Adjusted For Dead Load Deflection.

DESIGNED	ADD
CHECKED	KWS
DRAWN	VH
CHECKED	ADD

benesch
alfred benesch & company
Engineers • Surveyors • Planners
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450
Job No. 3426

ILLINOIS DEPARTMENT OF TRANSPORTATION
IL ROUTE 15/IN ROUTE 64
OVER WABASH RIVER PUBLIC WATERS
FAP 827 SECT 12Z-3, 12BR
**DECK ELEVATIONS
UNIT 4**
SN: 093-0021 (IL)/9502700 (IN) STA. 1036+27
WABASH CO., IL. DATE: JUNE 15, 2007

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