

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|---------|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 1 |

PROJECT ENGINEER: BECKY MARRUFO

SENIOR SQUAD LEADER: MIKE YUSEF (815) 284-5354
SQUAD LEADER: DAN LONG (815) 284-5966

DESIGN ENGINEER: ANTHONY MILLER
HUTCHISON ENGINEERING, INC.: (217) 245-7164

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

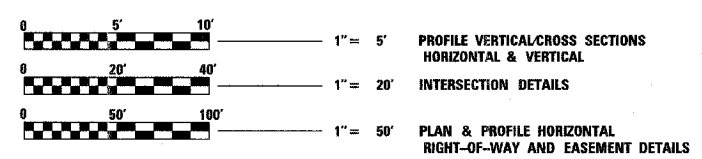
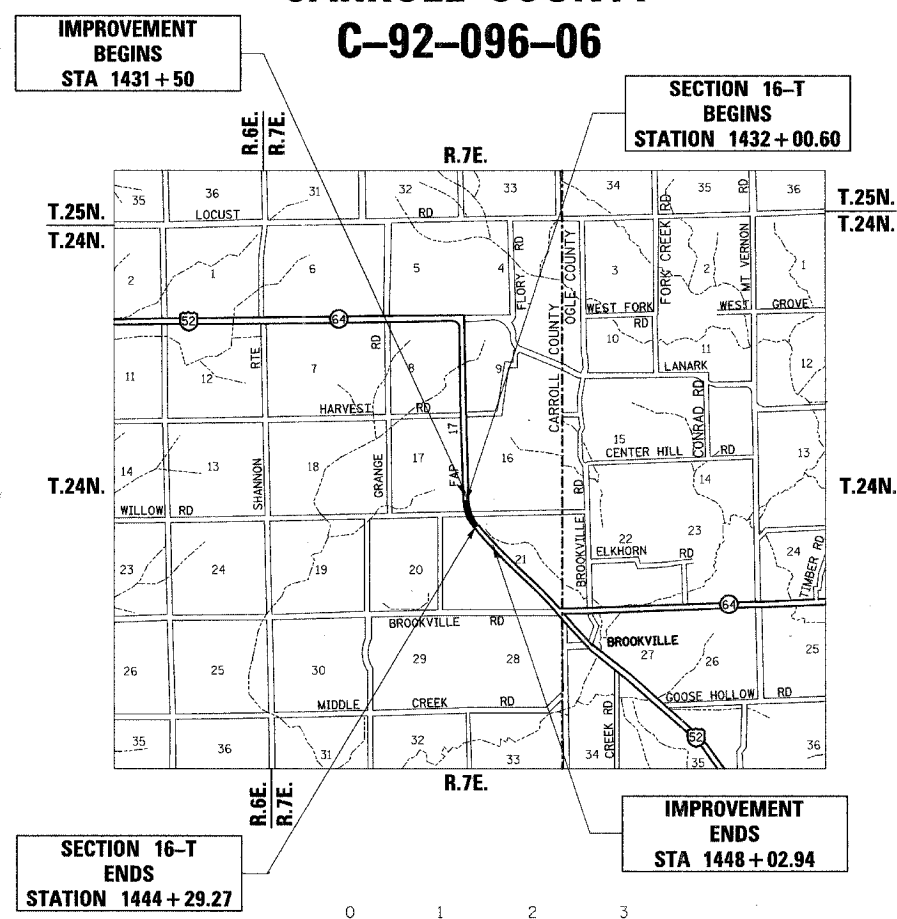
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

FAP ROUTE 17 (US 52/L 64)
SECTION 16-T

CARROLL COUNTY
C-92-096-06

D-92-050-03

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR STATE STANDARDS, SEE SHEET NO. 2



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

LIMA TOWNSHIP (SECTIONS 16, 17 & 21)

CONTRACT NO. 64897

DESIGN DESIGNATION
FAP 17
RURAL PRINCIPAL ARTERIAL
ADT = 1,560 (2006)
% SU = 9%
% MU = 8%

TOTAL LENGTH OF PROJECT = 1228.67 FEET = 0.233 MILES
NET LENGTH OF PROJECT = 1228.67 FEET = 0.233 MILES

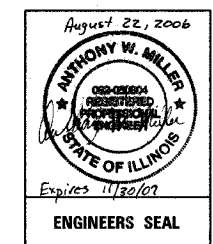
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED 8/28 20 06

Gregory L. Mounts
DEPUTY DIRECTOR OF HIGHWAYS, REGION TWO ENGINEER

October 13, 20 06
Mike Sene
ENGINEER OF DESIGN AND ENVIRONMENT

October 13, 20 06
Milton R. Sene, P.E.
DIRECTOR, DIVISION OF HIGHWAYS



Hutchison Engineering, Inc.
SINCE 1945
1801 West Lafayette
PO Box 820
Jacksonville, Illinois 62651
PHONE : (217)245-7164 FAX (217)243-0468

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|---------------------|----------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 17 | 16-T | CARROLL | 47 | 2 |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

CONTRACT NO. 64897

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- 000001-04 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
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- 280001-02 TEMPORARY EROSION CONTROL SYSTEMS
- 442201-01 CLASS C AND D PATCHES
- 482001 BITUMINOUS SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
- 482006-01 BITUMINOUS SHOULDER ADJACENT TO RIGID PAVEMENT
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- 701006-02 OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
- 701011-01 OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY
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- 701301-02 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701306-01 LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS - DAY ONLY, FOR SPEEDS ≥ 45 MPH
- 701311-02 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
- 701326-02 LANE CLOSURE 2L 2W, PAVEMENT WIDENING, FOR SPEEDS ≥ 45 MPH
- 702001-06 TRAFFIC CONTROL DEVICES
- 720011 METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
- 781001-02 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION

FAP ROUTE 17
INDEX OF SHEETS & STANDARDS

DATE 8/22/2006

DRAWN BY JCW
CHECKED BY AWM

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 17 | 16-T | CARROLL | 47 | 3 |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

CONTRACT NO. 64897

GENERAL NOTES

See cross sections for special ditches and backslopes.

The removal of Bituminous Surfacing not on a rigid type base removed in conjunction with the base shall be removed as EARTH EXCAVATION. The removal of Bituminous Surfacing on a rigid type base removed in conjunction with the base shall be included in the contract unit price for PAVEMENT REMOVAL of the type specified.

The final top four inches of soil in any right-of-way area disturbed by the Contractor must be capable of supporting vegetation. The soil must be from the A horizon (zero to 2' deep) of soil profiles of local soils.

It is estimated that 411 cubic yards of earth will be hauled to the job from outside the project limits. A shrinkage factor of 25% has been used.

The topsoil excavation quantities have been adjusted to allow for shrinkage of topsoil between removal and replacement.

The Contractor shall seed all disturbed areas within the project limits. Class 2A seeding shall be used on front slopes and ditch bottoms. Class 4 seeding shall be used, on all backslopes and areas behind the backslope, and beyond the toe of front slope on fill sections without ditches.

Fertilizer Nutrients shall be applied at the rate specified in Sections 250 and 252 of the Standard Specifications. This shall be included in the cost of the SEEDING.

Previously pugmilled stockpiles of "Type A" older than 1 month will not be approved for use until a moisture check is run to verify moisture content. Material shipped to projects without being tested will not be accepted.

Placement and compaction of the backfill for AR culverts shall conform to Section 502.10 of the Standard Specifications, except that the material shall conform to the Interim Special Provision for COURSE AGGREGATE FOR TRENCH BACKFILL, BACKFILL AND BEDDING, and shall be compacted to a minimum of 95% of the standard laboratory density. The entire excavation, within 2 feet outside of each shoulder, shall be backfilled with trench backfill material to the bottom of the proposed subgrade. This trench backfill material will not be measured for payment, but shall be included in the contract unit price for the class of concrete involved of other unit price item of the work for which it is required.

The minimum patch dimension for full-depth patches will be shown on State Standard 442201.

Place LEVELING BINDER (MACHINE METHOD) on curves to attain additional superelevation as indicated on the typical section. The curves requiring such treatment are included in the schedules. Estimated Total: 621 Tons.

The following Mixture Requirements are applicable for this project:

| Mixture Uses(s): | Bituminous Concrete Base Course, 9" | Bituminous Concrete Surface Course | Machine Method Level Binder | Incidental Bituminous Surfacing | Bituminous Shoulders, 6 1/2" |
|---------------------|-------------------------------------|------------------------------------|-----------------------------|---------------------------------|------------------------------|
| PG: | PG64-22 | PG64-22 | PG64-22 | PG64-22 | PG58-22 |
| RAP% (Max) | 15% | 15% | 25% | 15% | 30% |
| Design Air Voids | 3.0% @ NDesign=50 | 4.2% @ NDesign=50 | 4.2% @ NDesign=50 | 4.2% @ NDesign=50 | 2.0% @ NDesign=50 |
| Mixture Composition | Superpave IL-19.0 | Superpave IL-9.5 or 12.5 | Superpave IL-9.5 | Superpave IL-9.5 or 12.5 | BAM |
| Friction Aggregate | N/A | Mix "C" | N/A | Mix "C" | N/A |
| 20 Year ESAL | 0.64 | 0.64 | 0.64 | 0.64 | 0.64 |

The Contractor will be required to furnish 140 mm (5 1/2") high brass stencils as approved by the Engineer and install stationing at 250' intervals. Stationing shall be placed on both lanes of 2-lane highways and on the outside lanes in both directions on 4-lane highways. The stations shall be placed 150 mm (6") inside the pavement marking edge so they can be read from the shoulder. This work will be included in the cost of the final pavement surface.

Reflective Crack Control shall be placed on the existing surface prior to any resurfacing, unless pavement is milled then it will be placed on the binder course.

Bituminous and Aggregate prime coat shall be placed in accordance with Section 406 of the Standard Specifications. The cost of the prime coats shall be included in the contract unit price per ton for LEVELING BINDER (MACHINE METHOD), SUPERPAVE N50 and BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50.

The Contractor shall clean out all AR culverts and stream flows to the right-of-way lines on the entire section. The cost shall be included in the contract unit price for the pipe culverts of the size and type specified.

The proposed pipes for entrances and side roads shall be placed in line with the existing or proposed ditch line.

Where field tile is encountered, storm sewer protected or pipe drain will be used in accordance with Section 611. The minimum size for replacement will be 6", but the size must be at least 2" larger than the adjoining tile. A Field Tile Junction Vault will be constructed at the right of way line to connect the tile and storm sewer.

Delineators shall be placed at the ends of approach guardrail terminal sections, and at each headwall or end section of AR Culverts. This work will be paid for at the contract unit price each for DELINEATORS.

GENERAL NOTES

Pavement Marking shall be done according to Standard 780001, except as follows:

- All words, such as ONLY, shall be 2.4 m (8 feet) high.
- All non-freeway arrows shall be the large size.
- The distance between yellow no-passing lines shall be 200 mm (8"), not 180 mm (7") as shown in the detail of Typical Lane and Edge Lines.

PERMANENT SURVEY MARKERS, TYPE II, shall be set at intervals of 1.6 Km (1 mile) or as directed by the Engineer. Bridge or culvert projects shall have one survey marker placed near the structure. Estimated: 2 Each.

Permanent Survey Markers, Type II shall be cast-in-place as shown on Highway Standard 667101.

The Contractor shall submit to the Engineer a description of location, elevation, and coordinates for each permanent survey marker. The Engineer shall submit this information to the Survey Crew.

Right-of-way markers will be erected with the back face of the marker on the right-of-way line unless the new right-of-way line has been surveyed and pinned, in which instance the right-of-way markers will be erected 300 mm (12 inches) inside the new right-of-way line.

The Contractor shall be responsible for protecting utility property during construction operations as outlined in Article 107.31 of the Standard Specifications. A minimum of 48 hours advance notice is required for non-emergency work. The JULIE number is 800-892-0123. The following listed utilities located within the project limits or immediately adjacent to the project construction limits are members of JULIE:










Commonwealth Edison Company Citizens

CADD data will be available to Contractors and Consultants working on this project. This information will be provided upon request as MicroStation CADD files and Geopak coordinate geometry files ONLY. If data is required in other formats it will be your responsibility to make these conversions. If any discrepancy or inconsistency arises between the electronic data and the information on the hard copy, the information on the hard copy should be used. Contact the District's Project Engineer to request these files.

POLICY GUIDELINES

All trees removed from the project area (2 trees, max) for construction purposes will be replaced with deciduous tree species which are native to the District 2 area. The location of the replacement trees shall be determined by the District 2 Landscape Architect.

LEGEND

-  PROPOSED RIGHT-OF-WAY MARKER
-  PAVEMENT PATCHING, TYPE, SQ YD
-  ITEM TO BE REMOVED (TREE, ETC.)
-  PROPOSED SPECIAL DITCH
-  PROPOSED STANDARD DITCH
-  SPECIAL DITCH PROFILE LT
-  SPECIAL DITCH PROFILE RT
-  RIPRAP
-  BITUMINOUS SURFACE REMOVAL - BUTT JOINT

COMMITMENTS

none

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|----------------|------|---|
| NAME | DATE | |
| | | <p style="text-align: center;">FAP ROUTE 17 GENERAL NOTES, LEGEND & COMMITMENTS</p> <p style="text-align: right;">DRAWN BY JCW CHECKED BY AWM</p> |
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| DATE 8/22/2006 | | |

SUMMARY OF QUANTITIES

| CODE NO | SUMMARY OF QUANTITIES ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION CODE |
|------------|--|-------|----------------|-------------------|
| | | | 100% STATE | ROADWAY I000-2A |
| 20100110 | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | UNIT | 20 | 20 |
| 20200100 | EARTH EXCAVATION | CU YD | 2,094 | 2,094 |
| 20400800 | FURNISHED EXCAVATION | CU YD | 411 | 411 |
| 21101505 | TOPSOIL EXCAVATION AND PLACEMENT | CU YD | 1,078 | 1,078 |
| * 25000210 | SEEDING, CLASS 2A | ACRE | 1.00 | 1.00 |
| * 25000310 | SEEDING, CLASS 4 | ACRE | 1.25 | 1.25 |
| * 25000750 | MOWING | ACRE | 2.25 | 2.25 |
| * 25100115 | MULCH, METHOD 2 | ACRE | 2.25 | 2.25 |
| 25100630 | EROSION CONTROL BLANKET | SQ YD | 1,255 | 1,255 |
| 28000250 | TEMPORARY EROSION CONTROL SEEDING | POUND | 900 | 900 |
| 28000300 | TEMPORARY DITCH CHECKS | EACH | 59 | 59 |
| 28000400 | PERIMETER EROSION BARRIER | FOOT | 172 | 172 |
| 28000500 | INLET AND PIPE PROTECTION | EACH | 5 | 5 |
| 28100207 | STONE RIPRAP, CLASS A4 | TON | 697 | 697 |
| 28200200 | FILTER FABRIC | SQ YD | 1,047 | 1,047 |
| 35101100 | AGGREGATE BASE COURSE, TYPE A 12" | SQ YD | 1,521 | 1,521 |
| 35102000 | AGGREGATE BASE COURSE, TYPE B 8" | SQ YD | 65 | 65 |
| 40600980 | BITUMINOUS SURFACE REMOVAL - BUTT JOINT | SQ YD | 168 | 168 |
| 40600990 | TEMPORARY RAMP | SQ YD | 28 | 28 |
| 40800040 | INCIDENTAL BITUMINOUS SURFACING | TON | 253 | 253 |
| 44000100 | PAVEMENT REMOVAL | SQ YD | 108 | 108 |
| 44001430 | BITUMINOUS SHOULDER REMOVAL | SQ YD | 395 | 395 |
| 44201789 | CLASS D PATCHES, TYPE II, 12 INCH | SQ YD | 24 | 24 |
| 44201794 | CLASS D PATCHES, TYPE III, 12 INCH | SQ YD | 38 | 38 |
| 44300200 | STRIP REFLECTIVE CRACK CONTROL TREATMENT | FOOT | 2,458 | 2,458 |
| 48101200 | AGGREGATE SHOULDERS, TYPE B | TON | 228 | 228 |
| 48202410 | BITUMINOUS SHOULDERS SUPERPAVE 6 1/2" | SQ YD | 708 | 708 |
| 50100300 | REMOVAL OF EXISTING STRUCTURES NO. 1 | EACH | 1 | 1 |
| 50100400 | REMOVAL OF EXISTING STRUCTURES NO. 2 | EACH | 1 | 1 |
| 542A1093 | PIPE CULVERTS, CLASS A, TYPE 2 48" | FOOT | 96 | 96 |
| 542D0220 | PIPE CULVERTS, CLASS D, TYPE 1 15" | FOOT | 30 | 30 |
| 542D0235 | PIPE CULVERTS, CLASS D, TYPE 1 30" | FOOT | 232 | 232 |
| 54213450 | END SECTIONS 15" | EACH | 2 | 2 |
| 54213465 | END SECTIONS 30" | EACH | 2 | 2 |

| CODE NO | SUMMARY OF QUANTITIES ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION CODE |
|------------|--|--------|----------------|-------------------|
| | | | 100% STATE | ROADWAY I000-2A |
| 5422D042 | PIPE CULVERTS, CLASS D, TYPE 2 42" (TEMPORARY) | FOOT | 16 | 16 |
| 61100500 | EXPLORATION TRENCH 52" DEPTH | FOOT | 200 | 200 |
| 61100605 | MISCELLANEOUS CONCRETE | CU YD | 3 | 3 |
| 61101013 | STORM SEWERS PROTECTED, CLASS A, 12" | FOOT | 220 | 220 |
| 61101017 | STORM SEWERS PROTECTED, CLASS A, 15" | FOOT | 100 | 100 |
| 61133100 | FIELD TILE JUNCTION VAULTS, 2' DIA | EACH | 4 | 4 |
| 61133200 | FIELD TILE JUNCTION VAULTS, 3' DIA | EACH | 1 | 1 |
| 63500105 | DELINEATORS | EACH | 17 | 17 |
| 66600105 | FURNISHING AND ERECTING RIGHT OF WAY MARKERS | EACH | 33 | 33 |
| 66700305 | PERMANENT SURVEY MARKERS, TYPE II | EACH | 2 | 2 |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 |
| 70100450 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701201 | L SUM | 1 | 1 |
| 70100460 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306 | L SUM | 1 | 1 |
| 70100500 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701326 | L SUM | 1 | 1 |
| 70103815 | TRAFFIC CONTROL SURVEILLANCE | CAL DA | 5 | 5 |
| 70300100 | SHORT-TERM PAVEMENT MARKING | FOOT | 246 | 246 |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 41 | 41 |
| * 78001110 | PAINT PAVEMENT MARKING - LINE 4" | FOOT | 7,720 | 7,720 |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 16 | 16 |
| 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 16 | 16 |
| * A2007814 | TREE, TILIA AMERICANA (AMERICAN LINDEN/BASSWOOD), 1-3/4" CALIPER, BALLED AND BURLAPPED | EACH | 2 | 2 |
| X0322263 | CULVERT DROP BOX | EACH | 1 | 1 |
| X0950100 | PIPE CULVERT REMOVAL 15" | FOOT | 10 | 10 |
| X3560130 | BITUMINOUS CONCRETE BASE COURSE WIDENING, SUPERPAVE 9 INCH | SQ YD | 859 | 859 |
| X4066414 | BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 | TON | 362 | 362 |
| X4066765 | LEVELING BINDER (MACHINE METHOD), SUPERPAVE N50 | TON | 715 | 715 |
| X4420500 | TEMPORARY PAVEMENT PATCH | SQ YD | 62 | 62 |
| Z0013798 | CONSTRUCTION LAYOUT | L SUM | 1 | 1 |
| Z0020900 | ESTABLISHING AND REFERENCING LAND SECTION MARKERS | EACH | 1 | 1 |
| Z0023600 | FILLING EXISTING CULVERTS | EACH | 2 | 2 |
| Z0029001 | GRATED CULVERT EXTENSION, NO 1 | EACH | 1 | 1 |
| X0325587 | CORRUGATED STEEL PIPE MULTIPLE END SECTIONS DOUBLE 30" | EACH | 2 | 2 |

* SPECIALTY ITEMS

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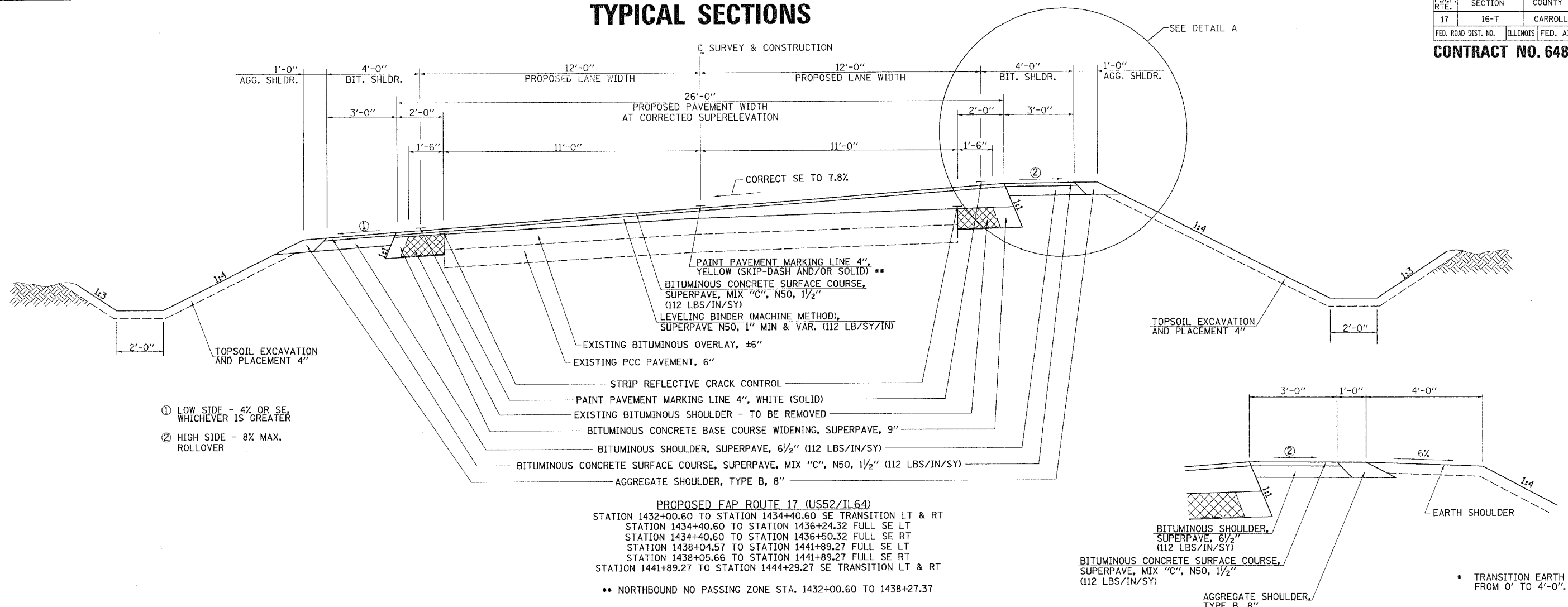
**FAP ROUTE 17
SUMMARY OF QUANTITIES**

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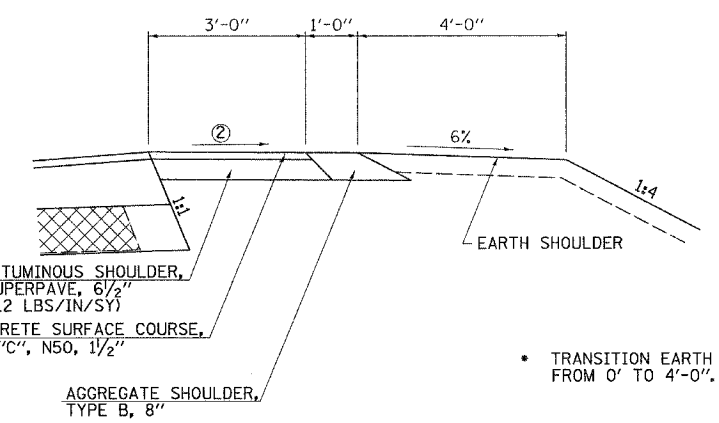
TYPICAL SECTIONS

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 17 | 16-T | CARROLL | 47 | 5 |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

CONTRACT NO. 64897

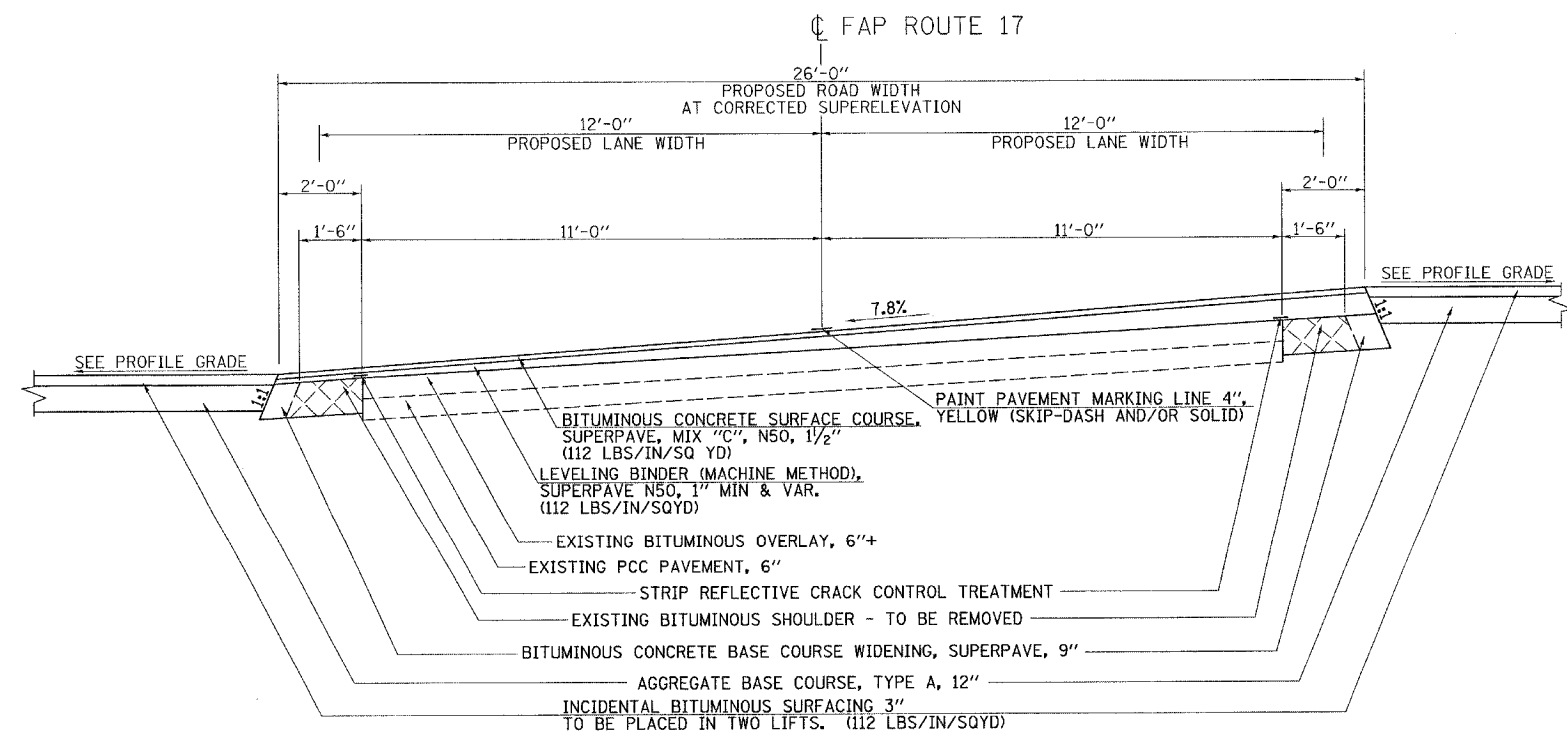


PROPOSED FAP ROUTE 17 (US52/IL64)
 STATION 1432+00.60 TO STATION 1434+40.60 SE TRANSITION LT & RT
 STATION 1434+40.60 TO STATION 1436+24.32 FULL SE LT
 STATION 1434+40.60 TO STATION 1436+50.32 FULL SE RT
 STATION 1438+04.57 TO STATION 1441+89.27 FULL SE LT
 STATION 1438+05.66 TO STATION 1441+89.27 FULL SE RT
 STATION 1441+89.27 TO STATION 1444+29.27 SE TRANSITION LT & RT
 ** NORTHBOUND NO PASSING ZONE STA. 1432+00.60 TO 1438+27.37



DETAIL A
 STATION 1440+65 TO STATION 1441+05 LT *
 STATION 1441+05 TO STATION 1442+05 LT *
 STATION 1442+05 TO STATION 1442+45 LT *
 STATION 1440+35 TO STATION 1440+75 RT *
 STATION 1440+75 TO STATION 1441+75 RT *
 STATION 1441+75 TO STATION 1442+15 RT *

NOTE:
 PAVING SHALL BE COMPLETED USING ALTERNATE LANE CLOSURES SO THAT PAVING OPERATIONS WILL NOT RESULT IN A DROP OFF OF GREATER THAN 3".



PROPOSED FAP ROUTE 17 (US52/IL64)
 STATION 1436+24.32 TO STATION 1438+04.57 FULL SE LT
 STATION 1436+50.32 TO STATION 1438+05.66 FULL SE RT

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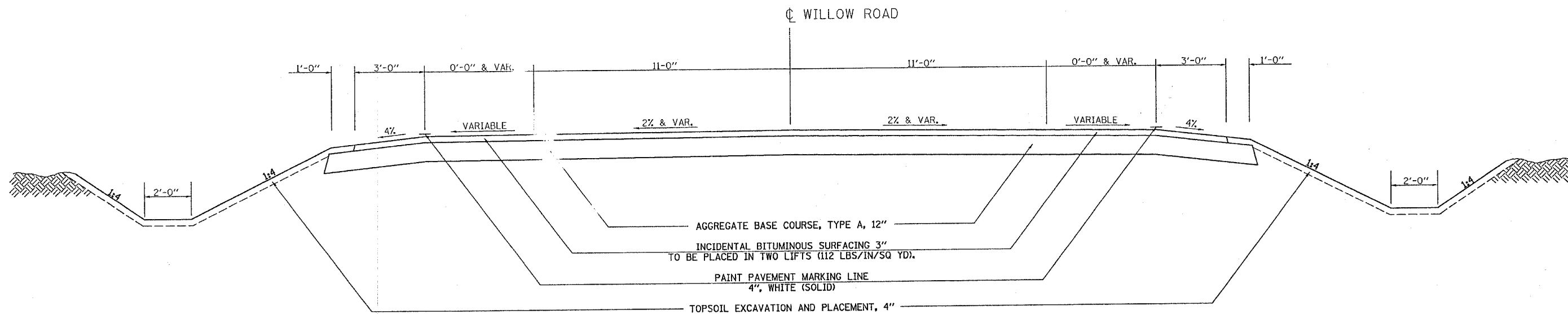
ILLINOIS DEPARTMENT OF TRANSPORTATION
FAP ROUTE 17
TYPICAL SECTIONS

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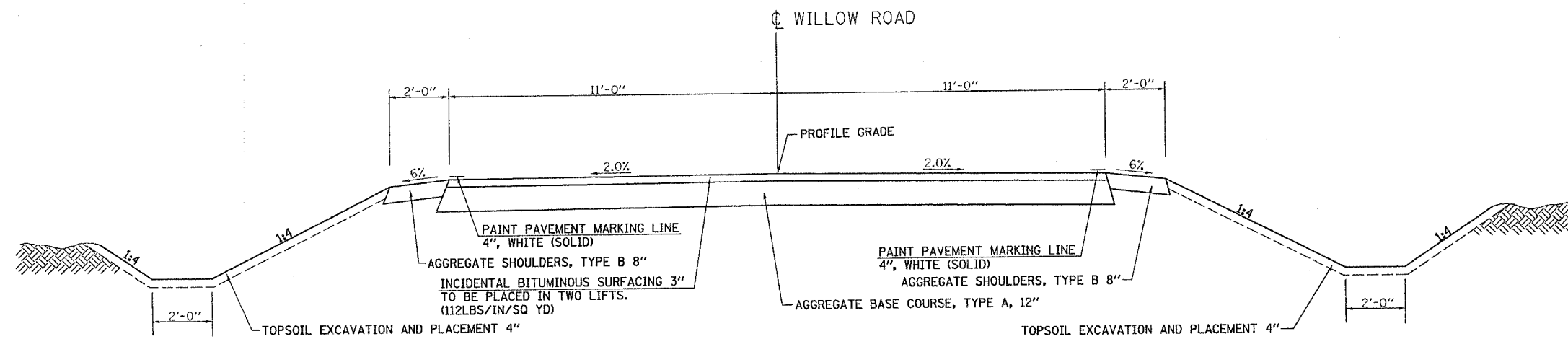
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 17 | 16-T | CARROLL | 47 | 6 |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

CONTRACT NO. 64897

TYPICAL SECTIONS



PROPOSED WILLOW ROAD
STATION 101+75.84 TO STATION 104+58.46



PROPOSED WILLOW ROAD
STATION 104+58.46 TO STATION 105+20.00

| REVISIONS | |
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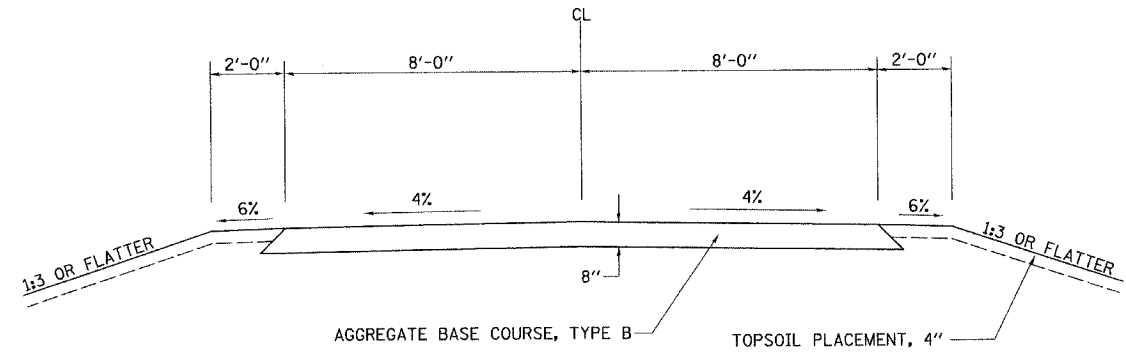
ILLINOIS DEPARTMENT OF TRANSPORTATION

WILLOW ROAD TYPICAL SECTIONS

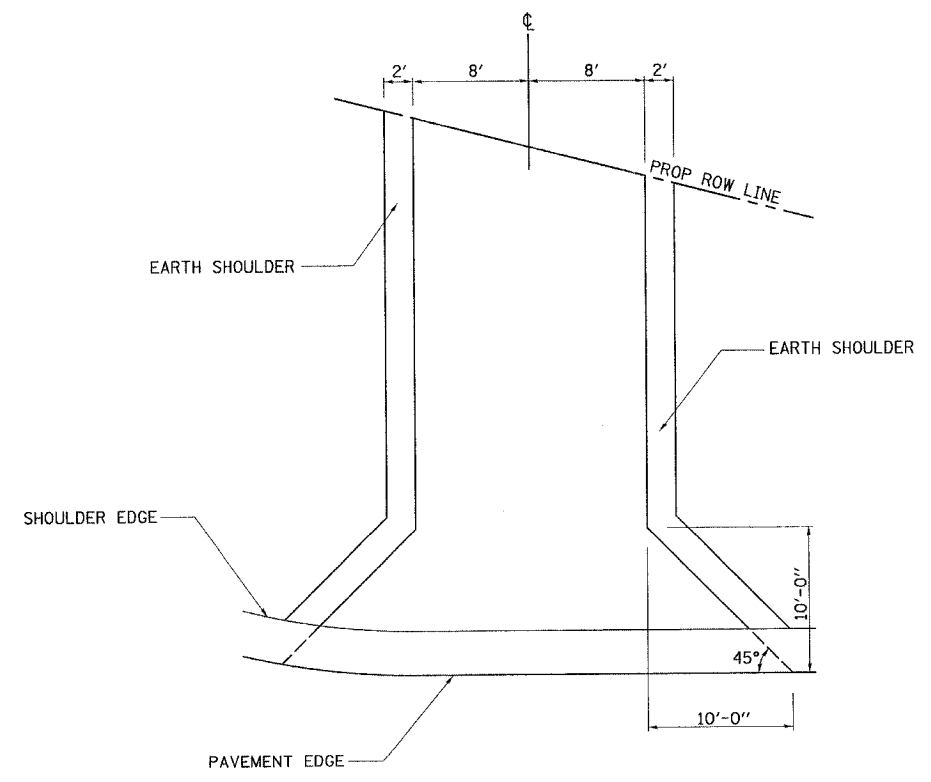
DATE 8/22/2006
DRAWN BY JCW
CHECKED BY AWM

| | | | | |
|---------------------|----------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 17 | 16-T | CARROLL | 47 | 7 |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

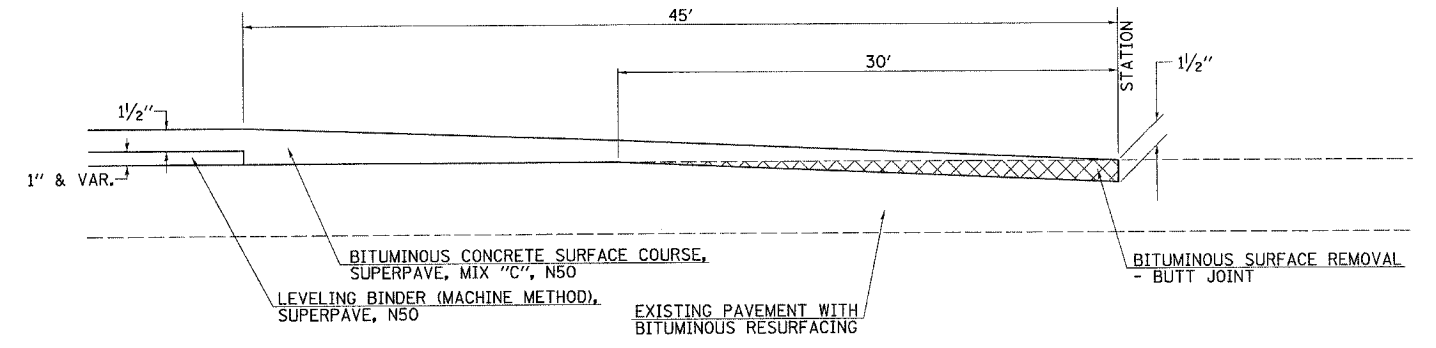
CONTRACT NO. 64897



TYPICAL SECTION
FIELD ENTRANCE
AT STA 102+51.7



PLAN VIEW
FIELD ENTRANCE
AT STA 102+51.7



BITUMINOUS CONCRETE BUTT JOINT

STATION
1432+00.60
1444+29.27

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION

**FIELD ENTRANCE
& BUTT JOINT DETAILS**

DATE 8/10/2006
DRAWN BY TJD
CHECKED BY AWM

CONTRACT NO. 64897

20100110: TREE REMOVAL (6 TO 15 UNITS)

| STATION | SIDE | TREE REMOVAL (6 TO 15 UNITS) | |
|--------------|------|------------------------------|-----------|
| | | OFFSET FOOT | UNIT |
| 1442+09 | LT | 37.8 | 12 |
| 1442+27 | LT | 33.6 | 8 |
| TOTAL | | | 20 |

EARTHWORK SCHEDULE

| STATION TO STATION | 1 | | 2 | | 3 | | 4 | | 5 | | 6 | | 7 | | 8 | | 9 | | 10 | | 11 | | 12 | | 13 | | |
|----------------------|-----------------------|------|--------------------|-------------|-------------------|-------------|--------------------|-------------|---|-------------|-----------------|-------------|----------------------------|------------|---------------------------------|------|-----------------------------|------|------|------|------------|------------|----|------|------|------|------|
| | THEORETICAL EARTHWORK | | TOPSOIL ADJUSTMENT | | TOPSOIL PLACEMENT | | ADJUSTED EARTHWORK | | TOPSOIL EXCAVATION AND PLACEMENT (21101505) | | EXCESS (BORROW) | | CUMULATIVE EXCESS (BORROW) | | FURNISHED EXCAVATION (20400800) | | EARTH EXCAVATION (20200100) | | | | | | | | | | |
| | CUT | FILL | 4" CUT | 4" FILL | 4" | CUT | FILL | FILL x 1.25 | CUT | FILL | CUT | FILL | CUT | FILL | CUT | FILL | CUT | FILL | | | | | | | | | |
| FAP 17 (US 52/IL 64) | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1432+00.60 | 1437+00.00 | LT | 0.24 | 0.18 | 0.42 | 0.42 | 1432+00.60 | 1436+50.00 | RT | 0.23 | 0.26 | 0.50 | 0.50 | 1437+50.00 | 1444+29.27 | LT | 0.25 | 0.24 | 0.49 | 0.49 | 1437+00.00 | 1444+29.27 | RT | 0.22 | 0.24 | 0.46 | 0.46 |
| WILLOW ROAD | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 101+75.84 | 103+00.00 | LT | 0.04 | 0.06 | 0.10 | 0.10 | 101+75.84 | 103+00.00 | RT | 0.01 | 0.01 | 0.02 | 0.02 | 103+50.00 | 105+20.00 | LT | 0.04 | 0.06 | 0.09 | 0.09 | 103+50.00 | 105+20.00 | RT | 0.04 | 0.03 | 0.07 | 0.07 |
| TOTAL | | | 1.07 | 1.08 | 2.15 | 2.15 | USE | | | 1.00 | 1.25 | 2.25 | 2.25 | | | | | | | | | | | | | | |

NOTES:
 A SHRINKAGE FACTOR OF 20% WAS USED TO DETERMINE THE EXCESS AND BORROW QUANTITIES. SHRINKAGE FACTOR = 1.00/0.80 = 1.25.

COLUMN 5 = (COLUMN 3 + COLUMN 4) x 9
 COLUMN 6 = COLUMN 1 + COLUMN 3
 COLUMN 7 = COLUMN 2 - COLUMN 4
 COLUMN 8 = COLUMN 7 x 1.25 (SHRINKAGE FACTOR)
 COLUMN 9 = (COLUMN 3 + COLUMN 4) x 1.25 (SHRINKAGE FACTOR)
 COLUMN 10 = COLUMN 6 - COLUMN 8 - COLUMN 9
 COLUMN 12 = COLUMN 7 - (COLUMN 6 - COLUMN 9) x 0.80
 COLUMN 13 = COLUMN 6 - COLUMN 9

EARTH EXCAVATION = COLUMN 13
 (PAY ITEM QUANTITY) = 2,094 CU YD

FURNISHED EXCAVATION = COLUMN 12
 (PAY ITEM QUANTITY) = 411 CU YD

TOPSOIL EXCAVATION AND PLACEMENT = COLUMN 9
 (PAY ITEM QUANTITY) = 1,078 CU YD

SEEDING

| STATION TO STATION | SIDE | SEEDING | | | | |
|----------------------|------------|------------------------------|-----------------------------|----------------------------|-------------------|-------------|
| | | SEEDING, CLASS 2A (25000210) | SEEDING, CLASS 4 (25000310) | MULCH, METHOD 2 (25100115) | MOWING (25000750) | |
| FAP 17 (US 52/IL 64) | | | | | | |
| 1432+00.60 | 1437+00.00 | LT | 0.24 | 0.18 | 0.42 | 0.42 |
| 1432+00.60 | 1436+50.00 | RT | 0.23 | 0.26 | 0.50 | 0.50 |
| 1437+50.00 | 1444+29.27 | LT | 0.25 | 0.24 | 0.49 | 0.49 |
| 1437+00.00 | 1444+29.27 | RT | 0.22 | 0.24 | 0.46 | 0.46 |
| WILLOW ROAD | | | | | | |
| 101+75.84 | 103+00.00 | LT | 0.04 | 0.06 | 0.10 | 0.10 |
| 101+75.84 | 103+00.00 | RT | 0.01 | 0.01 | 0.02 | 0.02 |
| 103+50.00 | 105+20.00 | LT | 0.04 | 0.06 | 0.09 | 0.09 |
| 103+50.00 | 105+20.00 | RT | 0.04 | 0.03 | 0.07 | 0.07 |
| TOTAL | | | 1.07 | 1.08 | 2.15 | 2.15 |
| USE | | | 1.00 | 1.25 | 2.25 | 2.25 |

25100630: EROSION CONTROL BLANKET

| STATION TO STATION | SIDE | LENGTH | | WIDTH | AREA |
|----------------------|-----------|--------|-------|-------|----------------|
| | | FOOT | FOOT | | |
| FAP 17 (US 52/IL 64) | | | | | |
| 1432+50.0 | 1436+96.6 | LT | 446.6 | 8 | 397.0 |
| 1433+00.0 | 1436+71.7 | RT | 371.7 | 8 | 330.4 |
| 1440+65.0 | 1442+45.0 | LT | 180.0 | 6 | 120.0 |
| WILLOW ROAD | | | | | |
| 101+75.8 | 102+57.1 | LT | 81.3 | 8 | 72.3 |
| 101+75.8 | 102+42.4 | RT | 66.6 | 8 | 59.2 |
| 103+48.7 | 105+20.0 | LT | 171.3 | 8 | 152.3 |
| 103+81.7 | 105+20.0 | RT | 138.3 | 8 | 122.9 |
| TOTAL | | | | | 1,254.1 |
| USE | | | | | 1,255 |

TEMPORARY EROSION CONTROL SEEDING

| STATION TO STATION | SIDE | TEMPORARY EROSION CONTROL SEEDING (28000250) | | |
|-----------------------|------------|--|-------------|------------|
| | | ACRES | POUND | |
| FAP 17 (US 52/IL 64) | | | | |
| 1432+00.60 | 1437+00.00 | LT | 0.42 | 42 |
| 1432+00.60 | 1436+50.00 | RT | 0.50 | 50 |
| 1437+50.00 | 1444+29.27 | LT | 0.49 | 49 |
| 1437+00.00 | 1444+29.27 | RT | 0.46 | 46 |
| WILLOW ROAD | | | | |
| 101+75.84 | 103+00.00 | LT | 0.10 | 10 |
| 101+75.84 | 103+00.00 | RT | 0.02 | 2 |
| 103+50.00 | 105+20.00 | LT | 0.09 | 9 |
| 103+50.00 | 105+20.00 | RT | 0.07 | 7 |
| TOTAL | | | 2.15 | 215 |
| USE | | | 2.25 | 225 |
| 4 APPLICATIONS | | | | 900 |

28000300: TEMPORARY DITCH CHECKS

| STATION TO STATION | SIDE | EACH | |
|----------------------|------------|------|-----------|
| FAP 17 (US 52/IL 64) | | | |
| 1432+00.6 | 1434+00.0 | LT | 7 |
| 1432+00.6 | 1435+25.0 | RT | 2 |
| 1434+00.0 | 1435+25.0 | LT | 3 |
| 1435+25.0 | 1436+50.0 | LT | 3 |
| 1435+25.0 | 1436+69.5 | RT | 4 |
| 1436+50.0 | 1436+90.0 | LT | 1 |
| 1437+47.3 | 1439+50.0 | RT | 6 |
| 1437+87.0 | 1441+00.0 | LT | 7 |
| 1439+50.0 | 1441+00.0 | RT | 2 |
| 1441+00.0 | 1442+25.0 | LT | 3 |
| 1441+10.0 | 1441+50.0 | RT | 1 |
| 1441+50.0 | 1442+00.0 | RT | 2 |
| 1442+00.0 | 1443+50.0 | RT | 2 |
| 1443+50.0 | 1444+29.27 | RT | 1 |
| WILLOW ROAD | | | |
| 101+75.84 | 102+58.0 | LT | 2 |
| 101+75.84 | 102+76.5 | RT | 3 |
| 103+52.7 | 105+20.0 | LT | 5 |
| 103+74.5 | 105+20.0 | RT | 5 |
| TOTAL | | | 59 |

28000400: PERIMETER EROSION BARRIER

| STATION TO STATION | SIDE | LENGTH FOOT | |
|----------------------|-----------|-------------|--------------|
| FAP 17 (US 52/IL 64) | | | |
| 1432+01.1 | 1433+24.8 | RT | 129.5 |
| WILLOW ROAD | | | |
| 103+82.8 | 104+25.0 | RT | 42.2 |
| TOTAL | | | 171.7 |
| USE | | | 172 |

28000500: INLET AND PIPE PROTECTION

| STATION | SIDE | OFFSET FOOT | EACH |
|----------------------|------|-------------|----------|
| FAP 17 (US 52/IL 64) | | | |
| 1441+03.6 | RT | 43.8 | 1 |
| WILLOW ROAD | | | |
| 102+39.1 | RT | 28.4 | 1 |
| 102+54.5 | LT | 38.2 | 2 |
| 103+51.6 | LT | 40.7 | 1 |
| TOTAL | | | 5 |

STONE RIPRAP, CLASS A4 & FILTER FABRIC

| STATION TO STATION | SIDE | STONE RIPRAP CLASS A4 (28100207) | | FILTER FABRIC (28200200) | |
|----------------------|-----------|----------------------------------|--------------|--------------------------|-------|
| | | TON | SQ YD | TON | SQ YD |
| FAP 17 (US 52/IL 64) | | | | | |
| 1437+83.1 | 1438+07.1 | LT | 21.3 | 32 | |
| 1437+42.0 | 1440+00.0 | RT | 228.8 | 344 | |
| 1438+07.1 | 1441+00.0 | LT | 173.1 | 259.7 | |
| 1440+00.0 | 1441+13.3 | RT | 157.2 | 236.4 | |
| 1441+00.0 | 1441+85.7 | LT | 50.7 | 76.2 | |
| 1441+70.6 | 1442+31.9 | LT | 56.8 | 85.3 | |
| WILLOW ROAD | | | | | |
| 102+66.7 | 102+80.5 | RT | 8.4 | 12.5 | |
| TOTAL | | | 696.3 | 1046.1 | |
| USE | | | 697 | 1047 | |

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ILLINOIS DEPARTMENT OF TRANSPORTATION
**FAP ROUTE 17
 SCHEDULES OF QUANTITIES**
 DRAWN BY TJD
 CHECKED BY AWM
 DATE 8/22/2006

CONTRACT NO. 64897

PAVING SCHEDULE

| STATION TO STATION | AGGREGATE BASE COURSE, TYPE A, 12" (35101100) | AGGREGATE BASE COURSE, TYPE B 8" (35102000) | BITUMINOUS CONCRETE BASE COURSE WIDENING, SUPERPAVE 9 INCH (X3560130) | INCIDENTAL BITUMINOUS SURFACING (40800040) | BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50 (X4066414) | LEVELING BINDER (MACHINE METHOD), SUPERPAVE N50 (X4066765) |
|------------------------------------|---|---|---|--|--|--|
| | SQ YD | | | TON | | |
| US 52/IL 64 | | | | | | |
| 1432+00.60 TO 1434+40.60 | | | 146.7 | | 73.2 | 113.7 |
| 1434+40.60 TO 1441+89.27 | | | 457.6 | | 215.0 | 507.7 |
| 1441+89.27 TO 1444+29.27 | | | 146.7 | | 73.2 | 92.8 |
| STAGE CONSTRUCTION | | | | | | |
| 1440+49.00 TO 1441+61.00 | | | 107.4 | | | |
| WILLOW ROAD | | | | | | |
| FIELD ENTRANCE AT 102+51.70 | | | | | | |
| 101+75.26 TO 103+00.49 | 656.7 | | | 105.5 | | |
| 103+27.80 TO 104+58.46 | 713.2 | | | 114.3 | | |
| 104+58.46 TO 105+20.00 | 150.4 | | | 32.5 | | |
| TOTAL | 1520.3 | 64.5 | 858.4 | 252.3 | 361.4 | 714.1 |
| USE | 1,521 | 65 | 859 | 253 | 362 | 715 |

40600980: BITUMINOUS SURFACE REMOVAL - BUTT JOINT

| STATION TO STATION | WIDTH | LENGTH | BITUMINOUS SURFACE REMOVAL - BUTT JOINT | |
|-----------------------------|------------|--------|---|--------------|
| | FOOT | | | SQ YD |
| FAP 17 (US 52/IL 64) | | | | |
| 1432+00.60 | 1432+30.60 | 24.9 | 30.0 | 83.0 |
| 1443+99.27 | 1444+29.27 | 25.3 | 30.0 | 84.3 |
| TOTAL | | | | 167.3 |
| USE | | | | 168 |

40600990: TEMPORARY RAMP

| STATION TO STATION | WIDTH | AREA | |
|-----------------------------|------------|-------|-------------|
| | FOOT | SQ YD | |
| FAP 17 (US 52/IL 64) | | | |
| 1432+00.60 | 1432+05.60 | 24.9 | 13.8 |
| 1444+24.27 | 1444+29.27 | 25.3 | 14.1 |
| TOTAL | | | 27.9 |
| USE | | | 28 |

44000100: PAVEMENT REMOVAL

| STATION TO STATION | SIDE | AREA | |
|--------------------|-----------|------|--------------|
| | | RT | SQ YD |
| 1440+20.0 | 1440+77.9 | RT | 22.5 |
| 1440+77.9 | 1441+58.0 | RT | 62.3 |
| 1441+58.0 | 1442+16.0 | RT | 22.6 |
| TOTAL | | | 107.4 |
| USE | | | 108 |

44001430: BITUMINOUS SHOULDER REMOVAL

| STATION TO STATION | SIDE | SHOULDER REMOVAL | |
|--------------------|------------|------------------|--------------|
| | | LT & RT | SQ YD |
| 1432+00.60 | 1436+92.75 | LT & RT | 164.1 |
| 1436+92.75 | 1437+04.42 | LT | 2.0 |
| 1437+34.65 | 1437+53.24 | RT | 3.1 |
| 1437+53.24 | 1444+29.27 | LT & RT | 225.4 |
| TOTAL | | | 394.6 |
| USE | | | 395 |

CLASS D PATCHES, 12 INCH

| STATION | CLASS D PATCHES | |
|--------------|------------------|-------------------|
| | TY II (44201789) | TY III (44201794) |
| | SQ YD | |
| 1441+42.0 | | 37.4 |
| 1441+73.9 | 23.7 | |
| TOTAL | 23.7 | 37.4 |
| USE | 24 | 38 |

44300200: STRIP REFLECTIVE CRACK CONTROL

| STATION TO STATION | SIDE | LENGTH | |
|-----------------------------|------------|--------|---------------|
| | | FT | TON |
| FAP 17 (US 52/IL 64) | | | |
| 1432+00.60 | 1444+29.27 | LT | 1228.7 |
| 1432+00.60 | 1444+29.27 | RT | 1228.7 |
| TOTAL | | | 2457.4 |
| USE | | | 2458 |

48101200: AGGREGATE SHOULDERS, TYPE B

| STATION TO STATION | SIDE | WIDTH | AGGREGATE SHOULDERS, TYPE B |
|--------------------------|-------|-------|-----------------------------|
| | | FT | TON |
| US 52/IL 64 | | | |
| 1432+00.60 TO 1436+24.32 | LT | 1 | 42.9 |
| 1432+00.60 TO 1436+50.32 | RT | 1 | 45.5 |
| 1438+04.57 TO 1444+29.27 | LT | 1 | 63.2 |
| 1438+05.66 TO 1444+29.27 | RT | 1 | 63.1 |
| WILLOW ROAD | | | |
| 104+58.46 TO 105+20.00 | LT/RT | 2 | 12.5 |
| TOTAL | | | 227.2 |
| USE | | | 228 |

48202410: BITUMINOUS SHOULDERS, SUPERPAVE 6 1/2"

| STATION TO STATION | SIDE | WIDTH | BITUMINOUS SHOULDERS, SUPERPAVE 6 1/2" |
|--------------------------|------|-------|--|
| | | FT | SQ YD |
| US 52/IL 64 | | | |
| 1432+00.60 TO 1436+24.32 | LT | 3 | 141.2 |
| 1432+00.60 TO 1436+50.32 | RT | 3 | 149.9 |
| 1438+04.57 TO 1444+29.27 | LT | 3 | 208.2 |
| 1438+05.66 TO 1444+29.27 | RT | 3 | 207.9 |
| TOTAL | | | 707.3 |
| USE | | | 708 |

REMOVAL OF EXISTING STRUCTURES

| STATION | STRUCTURE NUMBER | STRUCTURE TYPE | REMOVAL OF EXISTING STRUCTURES (50100300) (50100400) |
|--------------------|------------------|----------------|--|
| | | | EACH |
| WILLOW ROAD | | | |
| 103+39.11 | 1 | 2'x2' BOX | 1 |
| 102+90.78 | 2 | 2'x1.5' BOX | 1 |
| TOTAL | | | 2 |

PIPE CULVERT SCHEDULE

| STATION | SIDE | TYPE | STANDARD | CLASS A | CLASS D | | | END SECTIONS | | CORRUGATED STEEL PIPE MULTIPLE END SECTIONS DOUBLE 30" (#2001376) | CULVERT DROP BOX (X0322263) | GRATED CULVERT EXTENSION, NO 1 (Z0029001) |
|-----------------------------------|------|-----------------------------|----------|----------------|----------------|----------------|----------------------------|----------------|----------------|---|-----------------------------|---|
| | | | | TYPE 2 | TYPE 1 | TYPE 2 | 15" | 30" | | | | |
| | | | | 48" (542A1093) | 15" (542D0220) | 30" (542D0235) | 42" (TEMPORARY) (5422D042) | 15" (54213450) | 30" (54213465) | | | |
| | | | | FOOT | | | | | EACH | | | |
| FAP ROUTE 17 (US 52/IL 64) | | | | | | | | | | | | |
| 1441+42.0 | CL | PIPE CULVERT & END SECTION | 542206 | 96 | | | | | | | 1 | 1 |
| 1441+73.9 | RT | PIPE CULVERT (TEMPORARY) | | | | | | 16 | | | | |
| WILLOW ROAD | | | | | | | | | | | | |
| 102+51.7 | RT | PIPE CULVERT & END SECTIONS | 542401 | | 30 | | | | 2 | | | |
| 102+66.0 | CL | PIPE CULVERT & END SECTIONS | 542401 | | | 148 | | | | 2 | | |
| 103+63.5 | CL | PIPE CULVERT & END SECTIONS | 542401 | | | 84 | | | | 2 | | |
| TOTALS | | | | 96 | 30 | 232 | 16 | 2 | 2 | 2 | 1 | 1 |
| USE | | | | 96 | 30 | 232 | 16 | 2 | 2 | 2 | 1 | 1 |

CONTINGENCY ITEMS FOR FIELD TILE REPAIR AND REPLACEMENT

| ITEM | UNIT | QUANTITY |
|---|-------|----------|
| EXPLORATION TRENCH 52" DEPTH (61100500) | FOOT | 200 |
| MISCELLANEOUS CONCRETE (61100605) | CU YD | 3 |
| STORM SEWERS PROTECTED, CLASS A, 12" (61101013) | FOOT | 220 |
| STORM SEWERS PROTECTED, CLASS A, 15" (61101017) | FOOT | 100 |
| FIELD TILE JUNCTION VAULTS, 2' DIA (61133100) | EACH | 4 |
| FIELD TILE JUNCTION VAULTS, 3' DIA (61133200) | EACH | 1 |

63500105: DELINEATORS

| STATION TO STATION | SIDE | DELINEATORS | |
|--------------------|------------|-------------|-----------|
| | | FT | EACH |
| 1433+74.60 | 1442+55.27 | LT | 7 |
| | 1441+59.72 | LT | 1 |
| 1433+74.60 | 1442+55.27 | RT | 8 |
| | 1441+24.75 | RT | 1 |
| TOTAL | | | 17 |

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ILLINOIS DEPARTMENT OF TRANSPORTATION

FAP ROUTE 17
SCHEDULES OF QUANTITIES

DRAWN BY TJD
CHECKED BY AWM
DATE 8/22/2006

66600105: FURNISHING AND ERECTING RIGHT OF WAY MARKERS

| STATION | SIDE | OFFSET | EACH |
|----------------------|------|--------|------|
| | | FEET | |
| FAP 17 (US 52/IL 64) | | | |
| 1431+50.00 | LT | 33.00 | 1 |
| 1431+50.00 | RT | 33.00 | 1 |
| 1432+00.00 | RT | 60.00 | 1 |
| 1433+00.00 | LT | 45.00 | 1 |
| 1433+74.60 | RT | 60.00 | 1 |
| 1434+50.26 | LT | 50.00 | 1 |
| 1434+00.86 | RT | 65.00 | 1 |
| 1436+00.00 | RT | 65.00 | 1 |
| 1436+44.00 | RT | 70.00 | 1 |
| 1436+50.37 | LT | 60.00 | 1 |
| 1436+80.18 | LT | 70.00 | 1 |
| 1437+53.46 | RT | 65.00 | 1 |
| 1437+95.45 | LT | 50.00 | 1 |
| 1438+00.00 | RT | 55.00 | 1 |
| 1439+00.00 | LT | 55.00 | 1 |
| 1439+00.00 | RT | 55.00 | 1 |
| 1440+00.00 | RT | 55.00 | 1 |
| 1440+00.00 | LT | 60.00 | 1 |
| 1441+00.00 | LT | 60.00 | 1 |
| 1441+00.00 | RT | 65.00 | 1 |
| 1442+00.00 | LT | 60.00 | 1 |
| 1442+55.27 | LT | 33.00 | 1 |
| 1444+00.00 | RT | 55.00 | 1 |
| 1445+00.00 | LT | 33.00 | 1 |
| 1446+00.00 | LT | 45.00 | 1 |
| 1446+50.00 | RT | 80.00 | 1 |
| 1447+00.00 | LT | 45.00 | 1 |
| 1448+00.00 | LT | 33.00 | 1 |
| 1448+02.93 | RT | 80.00 | 1 |
| WILLOW ROAD | | | |
| 101+65.00 | LT | 25.00 | 1 |
| 101+65.00 | RT | 25.00 | 1 |
| 105+00.00 | LT | 25.00 | 1 |
| 105+20.00 | RT | 25.00 | 1 |
| TOTAL | | | 33 |

66700305: PERMANENT SURVEY MARKERS, TYPE II

| STATION | DESCRIPTION | PERMANENT SURVEY MARKERS TYPE II |
|----------------------|-------------|----------------------------------|
| | | EACH |
| FAP 17 (US 52/IL 64) | | |
| 1433+74.60 | PC | 1 |
| 1442+55.27 | PT | 1 |
| TOTAL | | 2 |

SHORT-TERM PAVEMENT MARKING AND REMOVAL SCHEDULE

| STATION TO STATION | SIDE | SHORT-TERM PAVEMENT MARKING (70300100) | WORK ZONE PAVEMENT MARKING REMOVAL (70301000) | |
|------------------------|------------|--|---|------|
| | | FOOT | SQ FT | |
| FAP 17 (US 52/IL 64) | | | | |
| LEVELING BINDER COURSE | | | | |
| 1432+00.60 | 1444+29.27 | CL | 122.9 | |
| SURFACE COURSE | | | | |
| 1432+00.60 | 1444+29.27 | CL | 122.9 | 41.0 |
| TOTAL | | | 245.8 | 41.0 |
| USE | | | 246 | 41 |

| | | | | |
|----------------|--------------|----------------|-----------------|--------------|
| F.A.P. RTE. 17 | SECTION 16-T | COUNTY CARROLL | TOTAL SHEETS 47 | SHEET NO. 10 |
|----------------|--------------|----------------|-----------------|--------------|

CONTRACT NO. 64897

PAVEMENT MARKING SCHEDULE

| STATION TO STATION | SIDE | PAINT PAVEMENT MARKING (78001110) | | | RAISED REFLECTIVE PAVEMENT MARKER (78100100) |
|----------------------|------------|-----------------------------------|--------------------------|---------------------------|--|
| | | LINE 4" WHITE EDGE LINE | LINE 4" YELLOW SKIP-DASH | LINE 4" YELLOW SOLID LINE | |
| | | FOOT | | | EACH |
| FAP 17 (US 52/IL 64) | | | | | |
| 1432+00.60 | 1436+24.32 | LT | 423.7 | | |
| 1432+00.60 | 1436+50.32 | RT | 449.7 | | |
| 1432+00.60 | 1438+30.00 | CL | | 157.4 | |
| 1432+00.60 | 1438+30.00 | CL* | | | 8 |
| 1438+04.57 | 1444+29.27 | LT | 624.7 | | |
| 1438+05.66 | 1444+29.27 | RT | 623.6 | | |
| 1438+30.00 | 1444+29.27 | CL | | 149.8 | 8 |
| WILLOW ROAD | | | | | |
| 101+75.84 | 102+81.40 | LT | 137.3 | | |
| 101+75.84 | 103+30.00 | RT | 190.1 | | |
| 103+00.70 | 105+20.0 | LT | 270.3 | | |
| 103+53.70 | 105+20.00 | RT | 203.5 | | |
| TOTAL | | | 2923.0 | 307.2 | 629.4 |
| 1ST APPLICATION | | | | 3860 | |
| 2ND APPLICATION | | | | 3860 | |
| USE | | | | 7720 | 16 |

* NORTHBOUND NO PASSING ZONE

78300200: RAISED REFLECTIVE PAVEMENT MARKER REMOVAL

| STATION TO STATION | SIDE | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | |
|----------------------|------------|---|----|
| | | EACH | |
| FAP 17 (US 52/IL 64) | | | |
| 1432+00.60 | 1444+29.27 | CL | 16 |
| TOTAL | | | 16 |

A2007814: TREE, TILIA AMERICANA (AMERICAN LINDEN/BASSWOOD), 1-3/4" CALIPER, BALLED AND BURLAPPED

| LOCATION | TREE, TILIA AMERICANA (AMERICAN LINDEN/BASSWOOD), 1-3/4" CALIPER, BALLED AND BURLAPPED |
|---------------------------------|--|
| | EACH |
| SEE POLICY GUIDELINES - SHEET 3 | |
| | 2 |
| TOTAL | 2 |

X0950100: PIPE CULVERT REMOVAL, 15"

| STATION | SIDE | PIPE CULVERT REMOVAL, 15" |
|-----------|------|---------------------------|
| | | FOOT |
| 1435+25.0 | LT | 10 |
| TOTAL | | 10 |

X4420500: TEMPORARY PAVEMENT PATCH

| STATION | TEMPORARY PAVEMENT PATCH |
|-----------|--------------------------|
| | SQ YD |
| 1441+42.0 | 38 |
| 1441+73.9 | 23.7 |
| TOTAL | 61.7 |
| USE | 62 |

Z0020900: ESTABLISHING AND REFERENCING LAND SECTION MARKERS

| LOCATION | STATION | SIDE | OFFSET | ESTABLISHING AND REFERENCING LAND SECTION MARKERS |
|-----------------------|------------|------|--------|---|
| | | | | EACH |
| NW CORNER, SECTION 21 | 1437+00.48 | RT | 70.54' | 1 |
| TOTAL | | | | 1 |

Z0023600: FILLING EXISTING CULVERTS

| STATION | FILLING EXISTING CULVERTS |
|------------|---------------------------|
| | EACH |
| 1435+25.00 | 1 |
| 1441+73.93 | 1 |
| TOTAL | 2 |

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION

FAP ROUTE 17 SCHEDULES OF QUANTITIES

DATE 8/22/2006

DRAWN BY TJD
CHECKED BY AWM

| | | | | |
|-------------|---------|---------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 17 | 16-T | CARROLL | 47 | 11 |

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
CONTRACT NO. 64897

| BENCH MARKS | | | | | | | |
|-------------|------------|------------|-----------|--------|------------|-----------|---|
| POINT | NORTH | EAST | ELEVATION | CHAIN | STATION | OFFSET | DESCRIPTION |
| 2408 | 1967666.62 | 2422020.04 | 928.22 | IL64EX | 1437+59.70 | 32.63' LT | RR SPIKE IN PP @ SE QUAD OF WILLOW RD & US 52 |

CHAIN WILLOWRD contains:
30 31

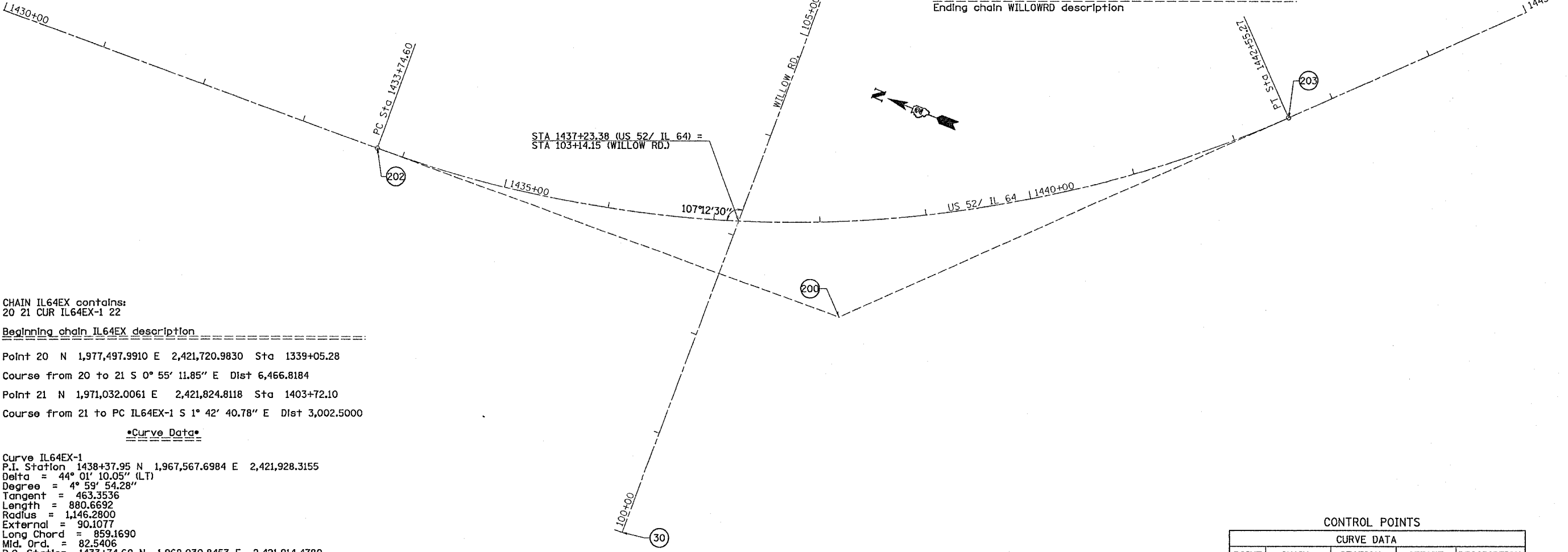
Beginning chain WILLOWRD description

Point 30 N 1,967,681.736 E 2,421,663.317 Sta 100+00.00

Course from 30 to 31 N 88° 38' 52.74" E Dist 633.9121

Point 31 N 1,967,696.6935 E 2,422,297.0472 Sta 106+33.91

Ending chain WILLOWRD description



CHAIN IL64EX contains:
20 21 CUR IL64EX-1 22

Beginning chain IL64EX description

Point 20 N 1,977,497.9910 E 2,421,720.9830 Sta 1339+05.28

Course from 20 to 21 S 0° 55' 11.85" E Dist 6,466.8184

Point 21 N 1,971,032.0061 E 2,421,824.8118 Sta 1403+72.10

Course from 21 to PC IL64EX-1 S 1° 42' 40.78" E Dist 3,002.5000

Curve Data

Curve IL64EX-1

P.I. Station 1438+37.95 N 1,967,567.6984 E 2,421,928.3155

Delta = 44° 01' 10.05" (LT)

Degree = 4° 59' 54.28"

Tangent = 463.3536

Length = 880.6692

Radius = 1,146.2800

External = 90.1077

Long Chord = 859.1690

Mid. Ord. = 82.5406

P.C. Station 1433+74.60 N 1,968,030.8453 E 2,421,914.4780

P.T. Station 1442+55.27 N 1,967,244.2633 E 2,422,260.1082

C.C. N 1,968,065.0777 E 2,423,060.2467

Back = S 1° 42' 40.78" E

Ahead = S 45° 43' 50.83" E

Chord Bear = S 23° 43' 15.81" E

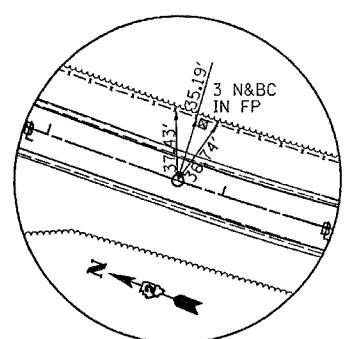
Course from PT IL64EX-1 to 22 S 45° 43' 50.83" E Dist 5,946.3715

Point 22 N 1,963,093.5140 E 2,426,518.1140 Sta 1502+01.64

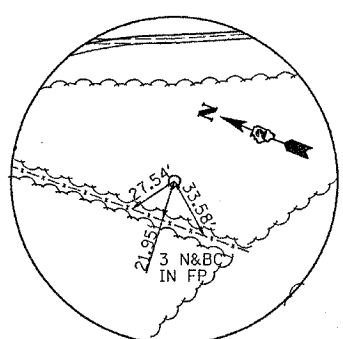
Ending chain IL64EX description

CONTROL POINTS

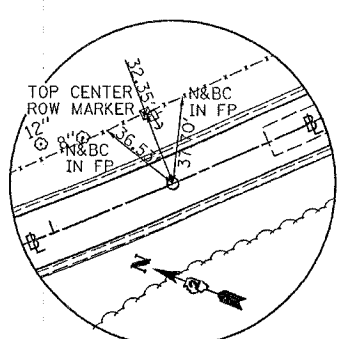
| CURVE DATA | | | | |
|------------|----------|------------|-------------|-------------|
| POINT | CHAIN | STATION | OFFSET | DESCRIPTION |
| 202 | IL64EX | 1433+74.60 | 0' | PC |
| 201 | IL64EX | 1433+74.60 | 1146.28' LT | CC |
| 200 | IL64EX | 1438+37.95 | 0' | PI |
| 203 | IL64EX | 1442+55.27 | 0' | PT |
| 30 | WILLOWRD | 100+00.00 | 0' | POT |
| 31 | WILLOWRD | 106+33.91 | 0' | POT |



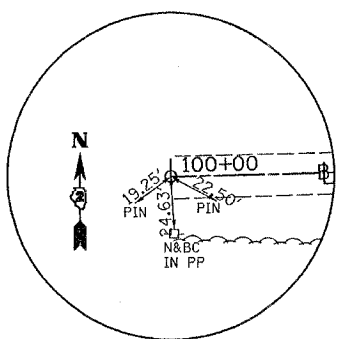
PC STA 1433+74.60
N - 1968030.845 E - 2421914.478
PC #202 - PK NAIL



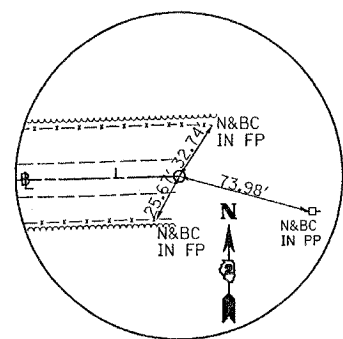
PI STA 1438+37.95
N - 1967567.698 E - 2421928.316
PI #200 - IRON ROD



PT STA 1442+55.27
N - 1967244.263 E - 2422260.108
PT #203 - PK NAIL



STA 100+00.00
N - 1967681.736 E - 2421663.312
POT #30 - IRON ROD



STA 106+33.91
N - 1967696.694 E - 2422297.047
POT #31 - IRON ROD

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION

FAP ROUTE 17
HORIZONTAL & VERTICAL
CONTROL

DATE 8/22/2006

DRAWN BY JCW
CHECKED BY AWM

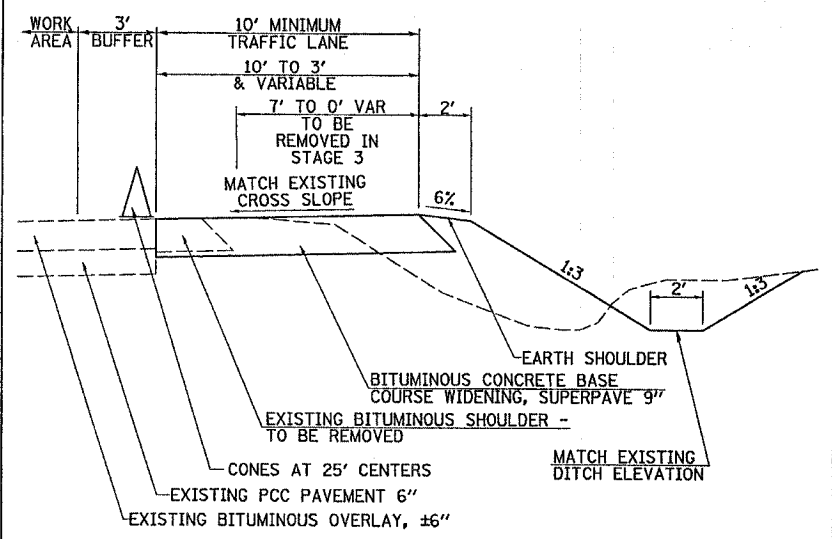
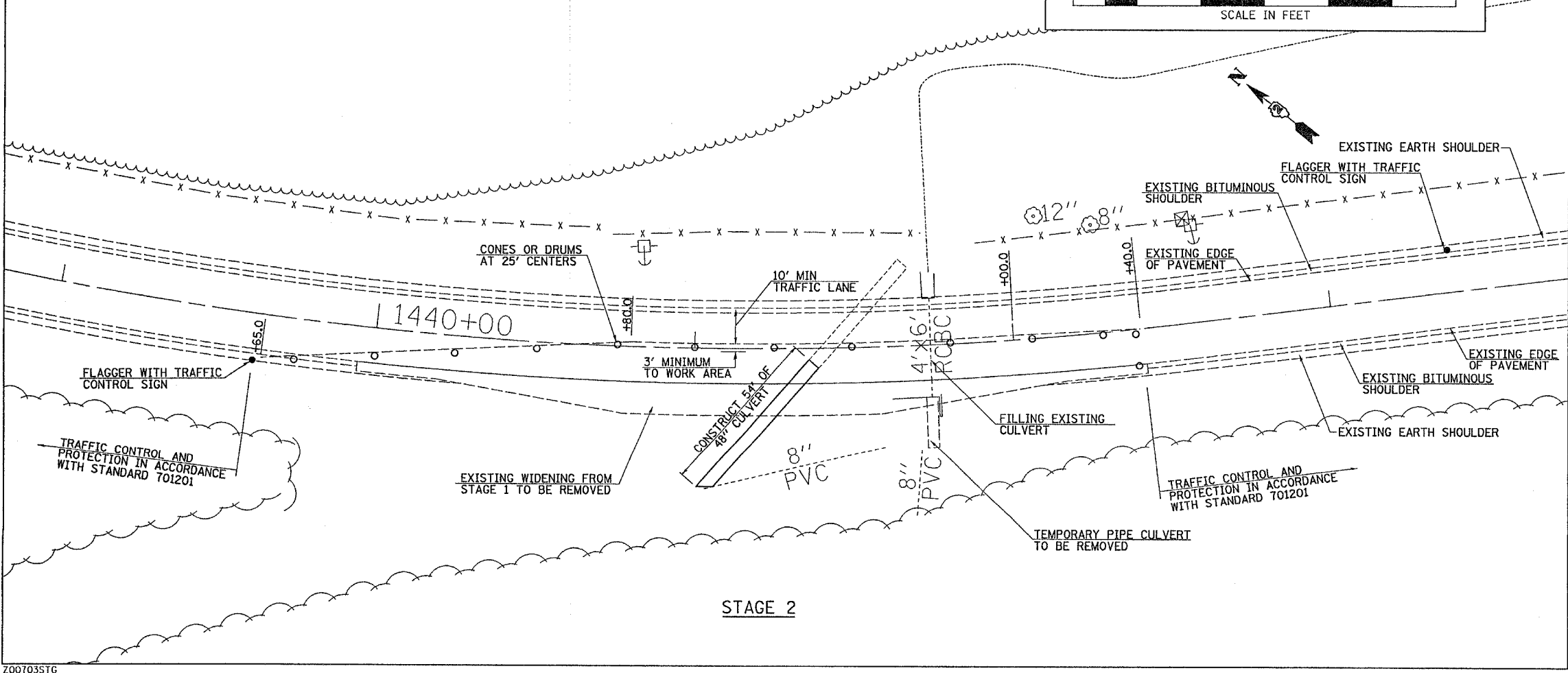
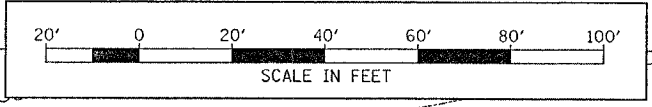
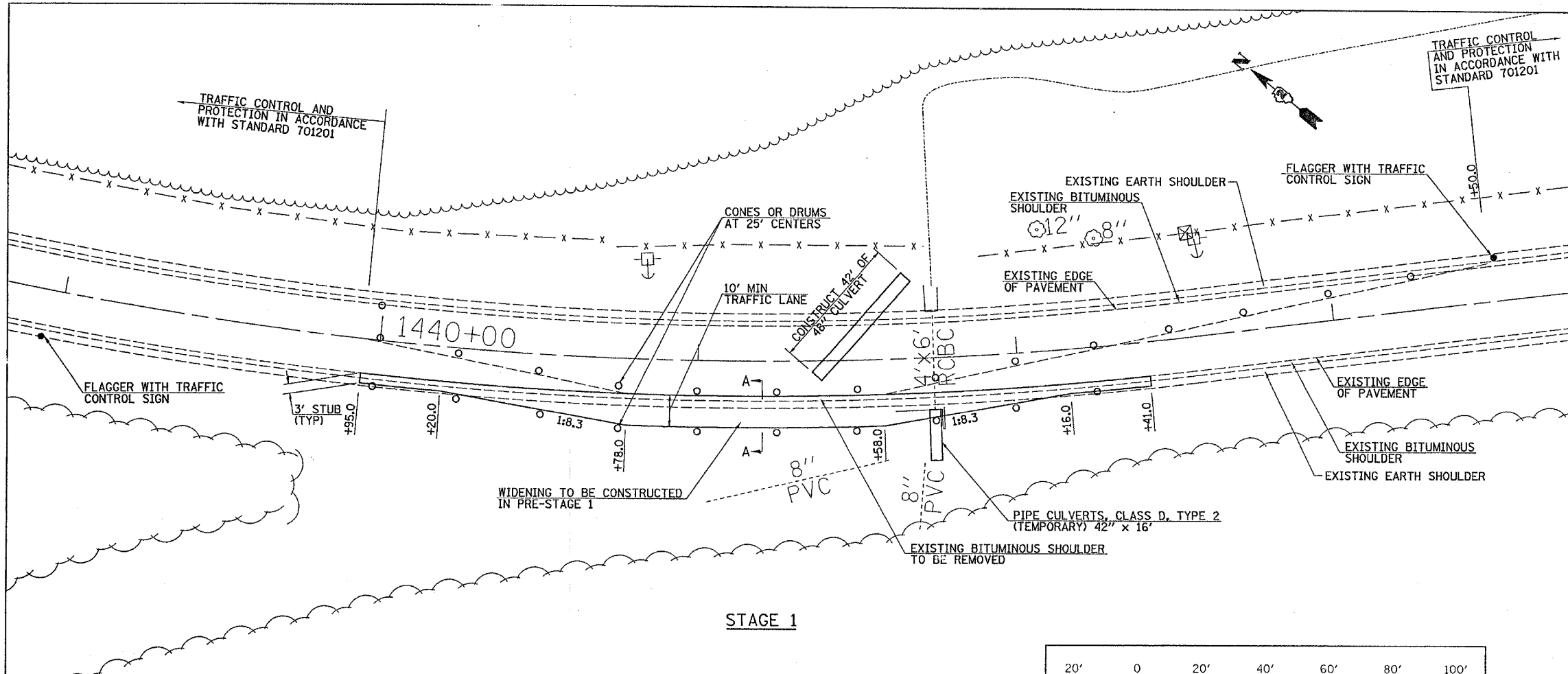
| | | | | |
|---------------------|---------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 17 | 16-T | CARROLL | 47 | 12 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 64897

SEQUENCE OF CONSTRUCTION

- PRE-STAGE 1**
- REMOVE EXISTING BITUMINOUS SHOULDER IN THE AREA OF TEMPORARY WIDENING. CONSTRUCT WIDENING AND TEMPORARY PIPE CULVERT EXTENSION ON THE WEST SIDE OF US 52/IL 64 AS SHOWN. TRAFFIC CONTROL AND PROTECTION SHALL BE IN ACCORDANCE WITH STANDARD 701326.
- STAGE 1**
- PROVIDE ONE-WAY TRAFFIC AROUND WORK AREA ON THE EXISTING SOUTHBOUND LANE AND THE WIDENING CONSTRUCTED IN PRE-STAGE 1. TRAFFIC CONTROL AND PROTECTION SHALL BE IN ACCORDANCE WITH STANDARD 701201.
 - CONSTRUCT THE EASTERN 42' OF THE PROPOSED 48" CULVERT. PATCH THE CULVERT TRENCH. SEE NOTE BELOW.
- STAGE 2**
- PROVIDE ONE-WAY TRAFFIC ON THE EXISTING NORTHBOUND LANE AND BITUMINOUS SHOULDER. TRAFFIC CONTROL AND PROTECTION SHALL BE IN ACCORDANCE WITH STANDARD 701201.
 - CONSTRUCT THE WESTERN 54' OF THE PROPOSED 48" CULVERT. PATCH THE CULVERT TRENCH. SEE NOTE BELOW.
 - REMOVE TEMPORARY PIPE CULVERT EXTENSION AND FILL WESTERN HALF OF EXISTING 4'x6' RCBC AT STA 1441+74. SEE SPECIAL PROVISIONS.
- STAGE 3**
- FILL EASTERN HALF OF EXISTING 4'x6' RCBC AT STA 1441+74. SEE SPECIAL PROVISIONS. TRAFFIC CONTROL AND PROTECTION SHALL BE IN ACCORDANCE WITH STANDARD 701201.
 - PATCH CULVERT TRENCHES RESULTING FROM CULVERT CONSTRUCTION AND CULVERT FILLING INCLUDING PORTION OF BITUMINOUS BASE COURSE WIDENING WHICH WILL BE PART OF ULTIMATE PAVEMENT. TRAFFIC CONTROL AND PROTECTION SHALL BE IN ACCORDANCE WITH STANDARD 701201.
 - REMOVE WIDENING ON WEST SIDE BEYOND ULTIMATE PAVEMENT WIDENING WHICH WAS CONSTRUCTED IN STAGE 1. TRAFFIC CONTROL AND PROTECTION SHALL BE IN ACCORDANCE WITH STANDARD 701326.
 - CONSTRUCT GRATED END SECTION AND DROP BOX AT ENDS OF CULVERT.

NOTE: AT THE END OF EACH WORKDAY, FAP 17 SHALL BE PATCHED AND OPENED TO TWO-WAY TWO-LANE TRAFFIC. IN THE EVENT THE CLASS D PATCHES CAN'T BE PLACED AT THE END OF THE WORK DAY, TEMPORARY PAVEMENT PATCH SHALL BE PLACED UNTIL SUCH TIME THE CLASS D PATCHING CAN BE CONSTRUCTED.



SECTION A-A
NOT TO SCALE

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION

FAP 17
MAINTENANCE OF TRAFFIC
CULVERT CONSTRUCTION
AT STA 1441+42.0

SCALE: VERT. DRAWN BY TJD
 HORIZ. CHECKED BY AWM
DATE 8/22/2006

BM #2 - RR SPIKE IN PP @ SE QUAD OF WILLOW RD & US 52.
32.63' LT, ELEV = 928.22

PI STA = 1438+37.95
 $\Delta = 44^\circ 01' 10''$ (LT)
 $D = 4^\circ 59' 54''$
 $R = 1,146.28'$
 $T = 463.35'$
 $L = 880.67'$
 $E = 90.11'$
 $e = 7.8\%$
 $TR = 40'$
 $SE RUN = 200'$
 $PC STA = 1433+74.60$
 $PT STA = 1442+55.27$
 $SE ATTAINED STA 1432+00.60$ TO STA 1434+40.60
 $SE REMOVED STA 1441+89.27$ TO STA 1444+29.27

SEE SHEET 16 FOR INTERSECTION DETAILS

WATERWAY INFORMATION

| Drainage Area = 35 Acres Pipe Culvert, Class A, 48" | | | |
|--|----------|--------------------------------------|-----------------------------------|
| Freq. Yr. | Q C.F.S. | Headwater Due to Culvert Elev. - ft. | Headwater Due to Weir Elev. - ft. |
| | | Prop. | Prop. |
| 10 | 53 | 913.18 | 916.56 |
| 50 | 74 | 913.92 | 916.82 |
| 100 | 86 | 914.40 | 916.97 |
| 500 | 119 | 916.25 | 917.32 |

SEC 21, T24N, R7E, 4TH PM
DONALD KLUCK

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|-----------------|--------------|------------------|
| 17 | 16-T | CARROLL | 47 | 13 |
| STA. 1430+00 | | TO STA. 1448+00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | | FED. AID PROJECT |

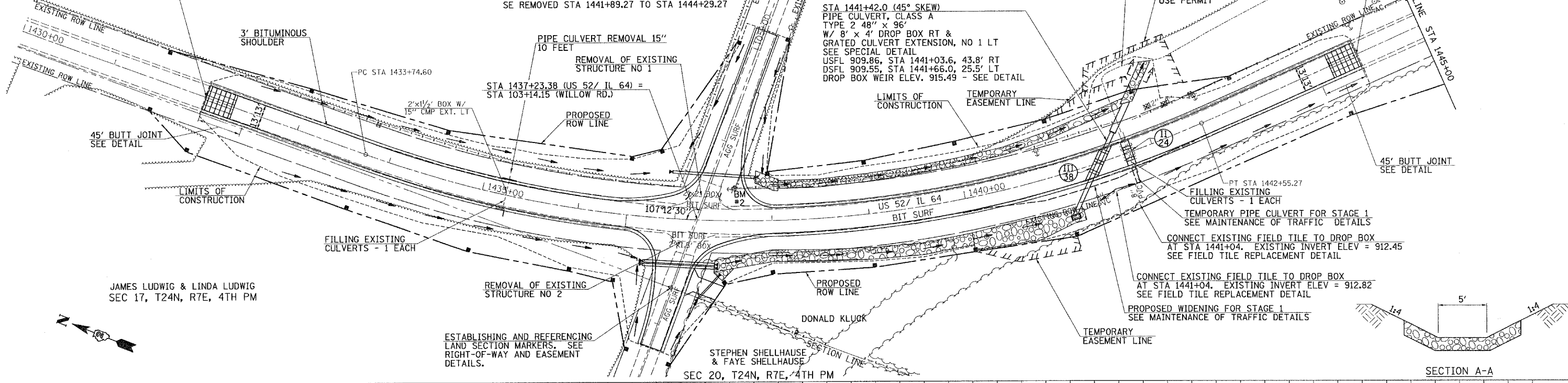
CONTRACT NO. 64897

SECTION 16-T ENDS
STA 1444+29.27

NOTE: ON THE RIGHT SIDE OF US 52 NEAR WILLOW ROAD. TRANSITION FROM 2' WIDE DITCH TO MATCH DOUBLE PIPE CULVERT END SECTIONS OVER A DISTANCE OF 25'.

SEC 16, T24N, R7E, 4TH PM
DONALD KLUCK

SECTION 16-T BEGINS
STA 1432+00.60

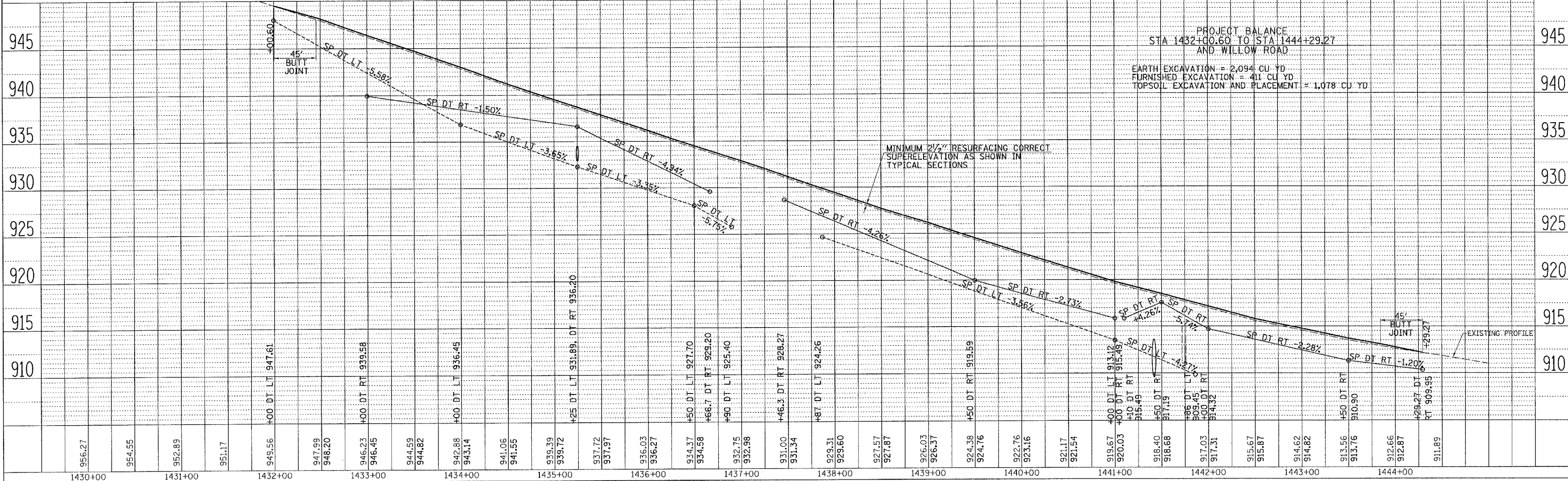


JAMES LUDWIG & LINDA LUDWIG
SEC 17, T24N, R7E, 4TH PM

ESTABLISHING AND REFERENCING LAND SECTION MARKERS. SEE RIGHT-OF-WAY AND EASEMENT DETAILS.

SEC 20, T24N, R7E, 4TH PM

SECTION A-A



ILLINOIS ROUTE 64, STA 1430+00 TO STA 1448+00

BM #2 - RR SPIKE IN PP @ SE QUAD OF WILLOW RD & US 52.
32.63' LT, ELEV = 928.22

SEC 16, T24N, R7E, 4TH PM
DONALD KLUCK

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------------------------|---------|---------|---------------------------|-----------|
| 17 | 16-T | CARROLL | 47 | 14 |
| STA. 101+00 TO STA. 105+39.15 | | | | |
| FED. ROAD DIST. NO. | | | ILLINOIS FED. AID PROJECT | |
| CONTRACT NO. 64897 | | | | |

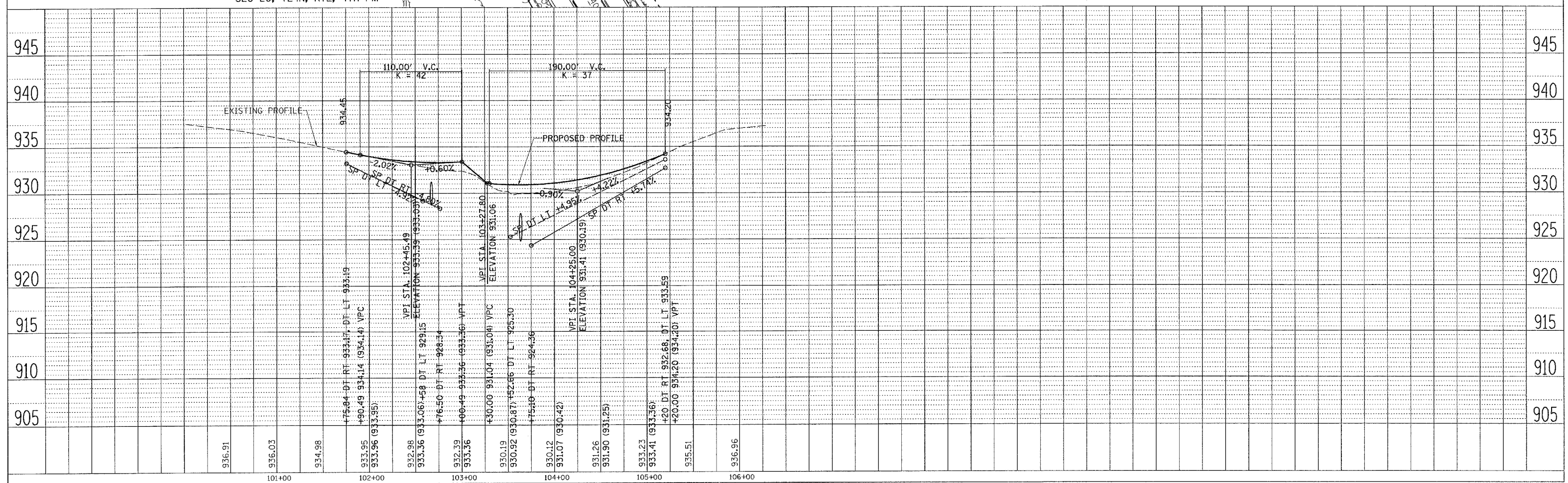
STA 102+66.0 (WILLOW RD.)
PIPE CULVERT, CLASS D
TYPE 1 CULVERT PIPE, 2 - 30"x74'
USFL 929.15, STA 102+54.0, 38.0' LT
DSFL 928.34, STA 102+75.7, 32.7' RT
W/ 2 CORRUGATED STEEL PIPE MULTIPLE
END SECTIONS (SEE SPECIAL DETAIL)
SEE NOTE ON THIS SHEET
Q10 = 39 CFS HW10 = 931.42
Q50 = 55 CFS HW50 = 932.18
Q100 = 63 CFS HW100 = 932.64
Q500 = 88 CFS HW500 = 934.49

PI STA = 1438+37.95
Δ = 44° 01' 10" (LT)
D = 4° 59' 54"
R = 1,146.28'
T = 463.35'
L = 880.67'
E = 90.11'
e = 7.8%
TR = 40'
SE RUN = 200'
PC STA = 1433+74.60
PT STA = 1442+55.27

STA 103+63.5 (WILLOW RD.)
PIPE CULVERT, CLASS D
TYPE 1 CULVERT PIPE 30"x84'
USFL 925.30, STA 103+51.6, 40.7' LT
DSFL 924.36, STA 103+75.1, 39.9' RT
W/ 2 END SECTIONS (STD 542401)

STA 102+51.7 (WILLOW RD)
PIPE CULVERT, CLASS D
TYPE 1 CULVERT PIPE, 15"x30'
USFL 930.14, STA 102+39.1, 28.4' RT
DSFL 928.81, STA 102+66.8, 40.1' RT
W/ 2 END SECTIONS (STD 542401)

STA 1437+23.38 (US 52/ IL 64) =
STA 103+14.15 (WILLOW RD.)



WILLOW RD, STA 101+00 TO STA 105+39.15

| DATE | BY | REVISIONS |
|------|----|-------------|
| | | 1. CHECKED |
| | | 2. PLOTTED |
| | | 3. ALIGNED |
| | | 4. CHECKED |
| | | 5. PLOTTED |
| | | 6. CHECKED |
| | | 7. PLOTTED |
| | | 8. CHECKED |
| | | 9. PLOTTED |
| | | 10. CHECKED |

| DATE | BY | REVISIONS |
|------|----|-------------|
| | | 1. CHECKED |
| | | 2. PLOTTED |
| | | 3. ALIGNED |
| | | 4. CHECKED |
| | | 5. PLOTTED |
| | | 6. CHECKED |
| | | 7. PLOTTED |
| | | 8. CHECKED |
| | | 9. PLOTTED |
| | | 10. CHECKED |



BM #2 - RR SPIKE IN PP SE QUAD OF WILLOW RD & US 52.
32.63' LT, ELEV = 928.22

RIGHT-OF-WAY AND EASEMENT DETAILS

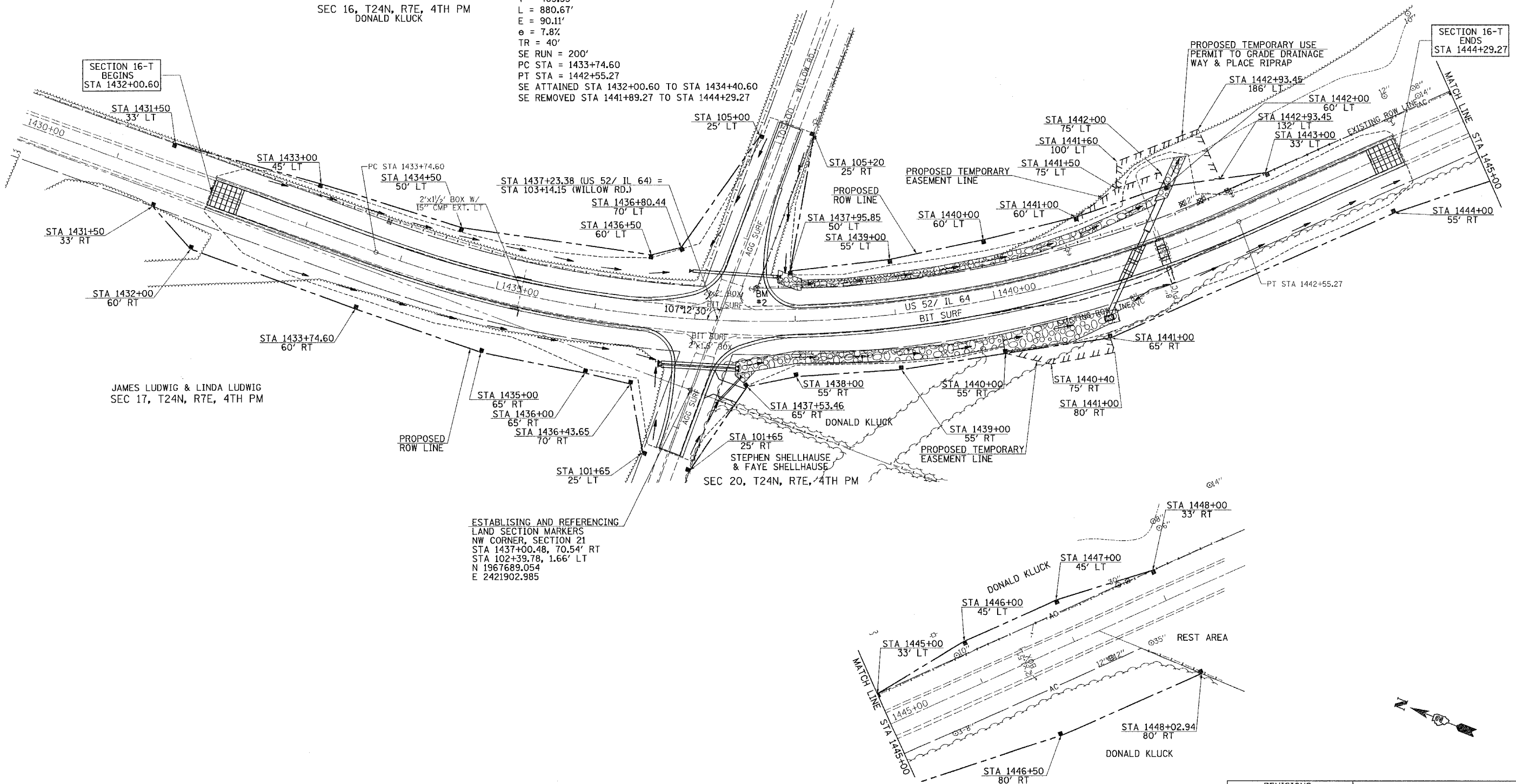
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|---------------------------|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 15 |
| STA. 1430+00 | | TO STA. 1448+00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 64897

SEC 16, T24N, R7E, 4TH PM
DONALD KLUCK

PI STA = 1438+37.95
 $\Delta = 44^\circ 01' 10''$ (LT)
 $D = 4^\circ 59' 54''$
 $R = 1,146.28'$
 $T = 463.35'$
 $L = 880.67'$
 $E = 90.11'$
 $e = 7.8\%$
 $TR = 40'$
 SE RUN = 200'
 PC STA = 1433+74.60
 PT STA = 1442+55.27
 SE ATTAINED STA 1432+00.60 TO STA 1434+40.60
 SE REMOVED STA 1441+89.27 TO STA 1444+29.27

SEC 21, T24N, R7E, 4TH PM
DONALD KLUCK



SECTION 16-T
BEGINS
STA 1432+00.60

SECTION 16-T
ENDS
STA 1444+29.27

JAMES LUDWIG & LINDA LUDWIG
SEC 17, T24N, R7E, 4TH PM

SEC 20, T24N, R7E, 4TH PM

ESTABLISHING AND REFERENCING
LAND SECTION MARKERS
NW CORNER, SECTION 21
STA 1437+00.48, 70.54' RT
STA 102+39.78, 1.66' LT
N 1967689.054
E 2421902.985

| REVISIONS | |
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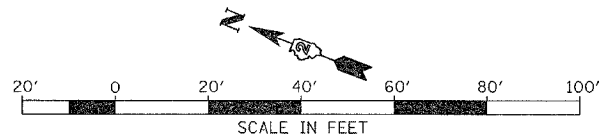
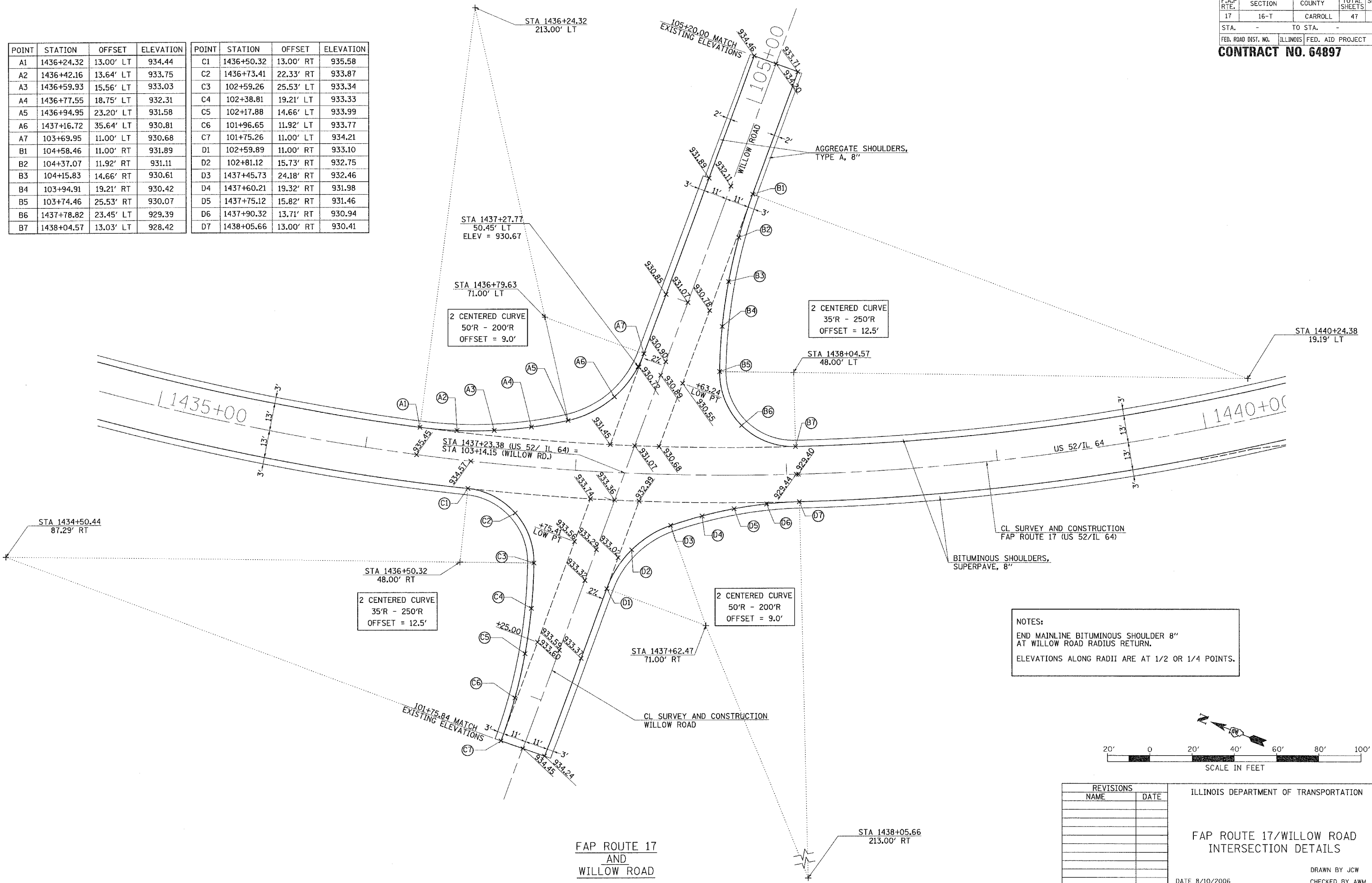
ILLINOIS DEPARTMENT OF TRANSPORTATION

**FAP ROUTE 17
RIGHT-OF-WAY AND
EASEMENT DETAILS**

VERT. SCALE: 8/10/2006
HORIZ. SCALE: 8/10/2006

DRAWN BY TJD
CHECKED BY AWM

| POINT | STATION | OFFSET | ELEVATION | POINT | STATION | OFFSET | ELEVATION |
|-------|------------|-----------|-----------|-------|------------|-----------|-----------|
| A1 | 1436+24.32 | 13.00' LT | 934.44 | C1 | 1436+50.32 | 13.00' RT | 935.58 |
| A2 | 1436+42.16 | 13.64' LT | 933.75 | C2 | 1436+73.41 | 22.33' RT | 933.87 |
| A3 | 1436+59.93 | 15.56' LT | 933.03 | C3 | 102+59.26 | 25.53' LT | 933.34 |
| A4 | 1436+77.55 | 18.75' LT | 932.31 | C4 | 102+38.81 | 19.21' LT | 933.33 |
| A5 | 1436+94.95 | 23.20' LT | 931.58 | C5 | 102+17.88 | 14.66' LT | 933.99 |
| A6 | 1437+16.72 | 35.64' LT | 930.81 | C6 | 101+96.65 | 11.92' LT | 933.77 |
| A7 | 103+69.95 | 11.00' LT | 930.68 | C7 | 101+75.26 | 11.00' LT | 934.21 |
| B1 | 104+58.46 | 11.00' RT | 931.89 | D1 | 102+59.89 | 11.00' RT | 933.10 |
| B2 | 104+37.07 | 11.92' RT | 931.11 | D2 | 102+81.12 | 15.73' RT | 932.75 |
| B3 | 104+15.83 | 14.66' RT | 930.61 | D3 | 1437+45.73 | 24.18' RT | 932.46 |
| B4 | 103+94.91 | 19.21' RT | 930.42 | D4 | 1437+60.21 | 19.32' RT | 931.98 |
| B5 | 103+74.46 | 25.53' RT | 930.07 | D5 | 1437+75.12 | 15.82' RT | 931.46 |
| B6 | 1437+78.82 | 23.45' LT | 929.39 | D6 | 1437+90.32 | 13.71' RT | 930.94 |
| B7 | 1438+04.57 | 13.03' LT | 928.42 | D7 | 1438+05.66 | 13.00' RT | 930.41 |



| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 FAP ROUTE 17/WILLOW ROAD
 INTERSECTION DETAILS
 DATE 8/10/2006
 DRAWN BY JCW
 CHECKED BY AWM

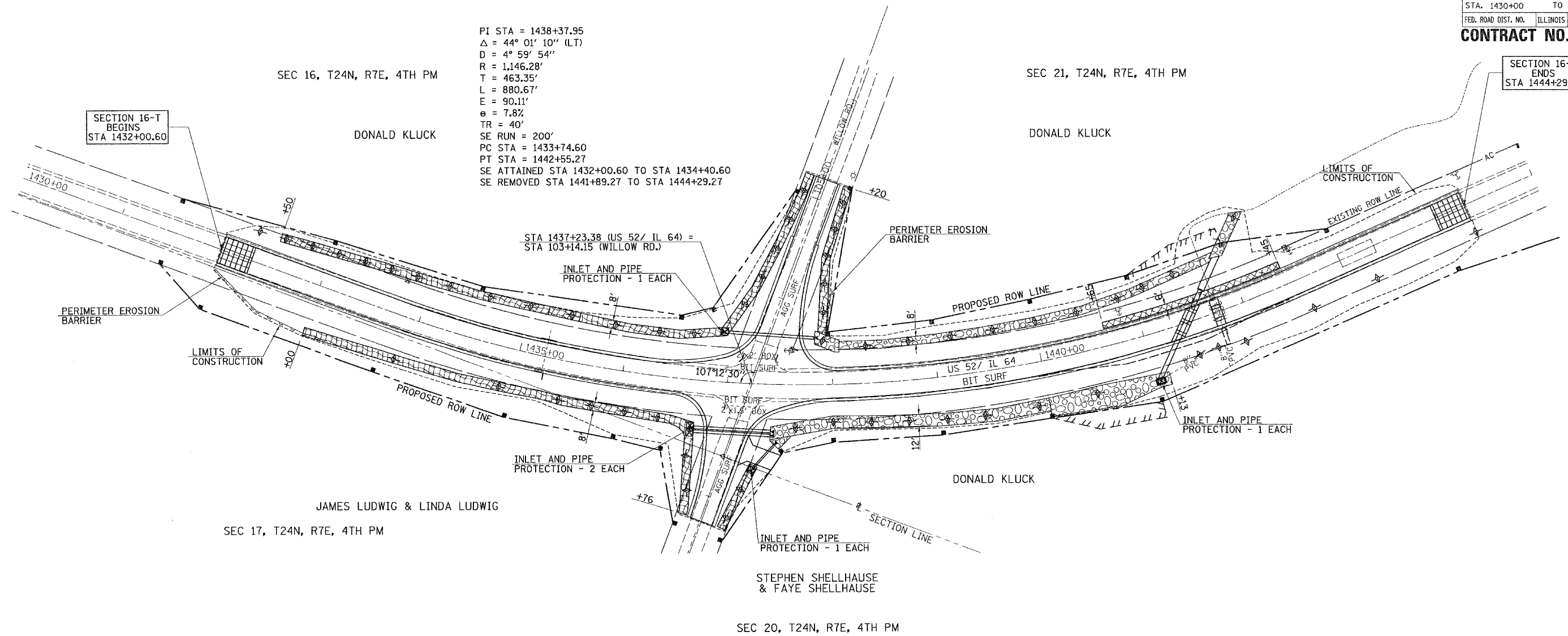
FAP ROUTE 17
 AND
 WILLOW ROAD

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|---------------------------|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 17 |
| STA. 1430+00 | | TO STA. 1445+00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 64897

SECTION 16-T
ENDS
STA 1444+29.27

PI STA = 1438+37.95
 $\Delta = 44^\circ 01' 10''$ (LT)
 $D = 4^\circ 59' 54''$
 $R = 1,146.28'$
 $T = 463.35'$
 $L = 880.67'$
 $E = 90.11'$
 $e = 7.8\%$
 $TR = 40'$
 $SE RUN = 200'$
 $PC STA = 1433+74.60$
 $PT STA = 1442+55.27$
 $SE ATTAINED STA 1432+00.60$ TO $STA 1434+40.60$
 $SE REMOVED STA 1441+89.27$ TO $STA 1444+29.27$



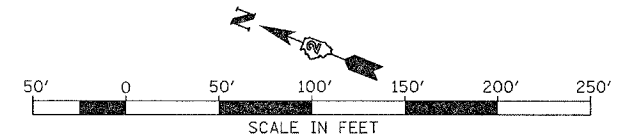
LEGEND FOR STORM WATER POLLUTION PREVENTION PLAN

| ITEM | SYMBOL |
|---|--------|
| TEMPORARY DITCH CHECKS (HAY OR STRAW BALE DITCH CHECKS OR APPROVED SUBSTITUTION) | |
| INLET PIPE PROTECTION (I&PP) (HAY OR STRAW BALE DITCH CHECKS OR APPROVED SUBSTITUTION) | |
| PERIMETER EROSION BARRIER | |
| EROSION CONTROL BLANKET | |
| STONE RIPRAP, CLASS A4 | |

GENERAL NOTES:

ALL ITEMS SHALL BE CONSTRUCTED AS SHOWN ON THIS SHEET, ON STANDARD 280001, AND AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL INSTALL DITCH CHECKS AS DIRECTED BY THE ENGINEER. IN THE ENGINEER ELECTS TO UTILIZE FLUSH RIPRAP DITCH CHECKS IN LIEU OF TEMPORARY DITCH CHECKS AS SHOWN, THE SPACING SHOULD BE DOUBLED.



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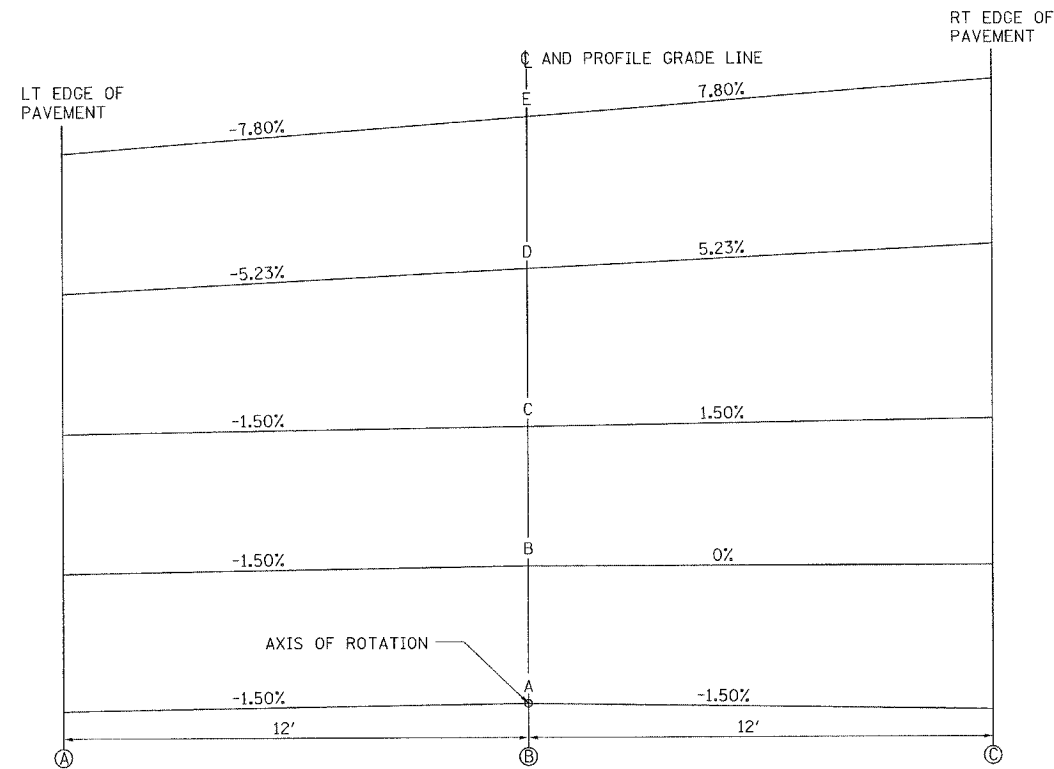
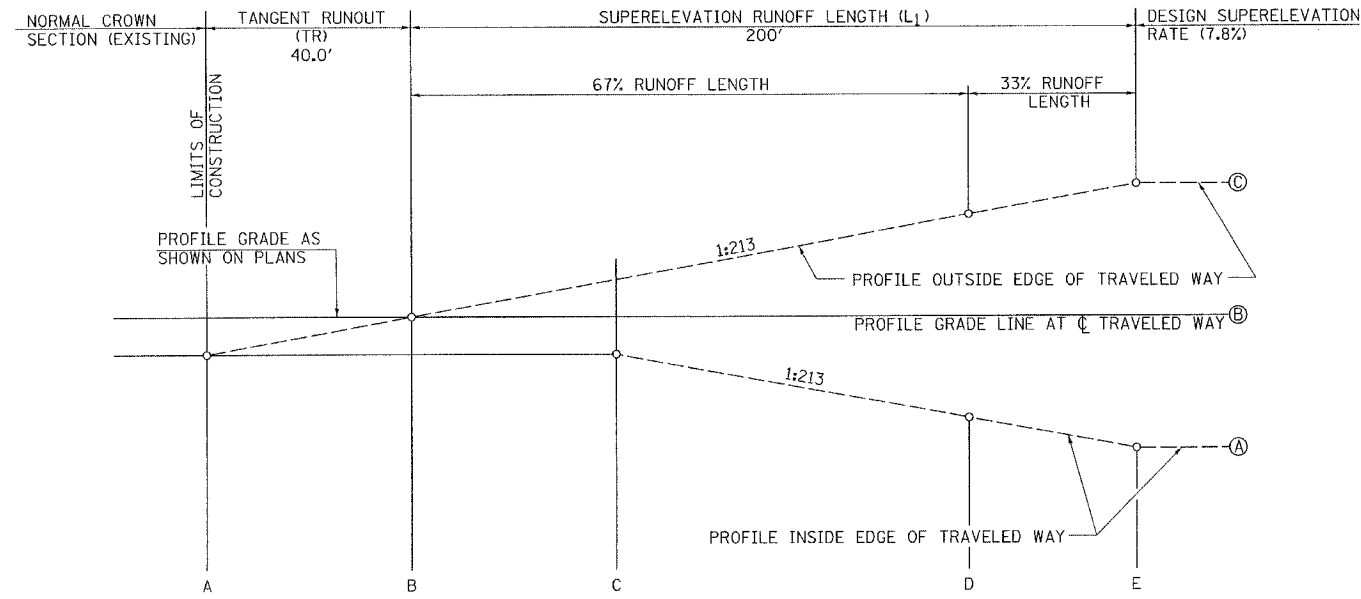
ILLINOIS DEPARTMENT OF TRANSPORTATION

**FAP ROUTE 17
EROSION CONTROL PLAN**

SCALE: VERT.
HORIZ.
DATE 8/10/2006

DRAWN BY TJD
CHECKED BY AWB

CONTRACT NO. 64897



SUPERELEVATION TRANSITION DETAIL

| SECTION | STATION | 13' LT EDGE | SE % | 13' RT EDGE | SE % | |
|----------|------------|-------------|--------|-------------|--------|------|
| A | 1432+00.60 | 949.35 | -1.50 | 949.35 | -1.50 | |
| | 1432+25 | 948.69 | -1.50 | 948.80 | -0.59 | |
| B | 1432+40.60 | 948.26 | -1.50 | 948.45 | 0.00 | |
| | 1432+50 | 948.01 | -1.50 | 948.25 | 0.37 | |
| | 1432+75 | 947.14 | -1.50 | 947.50 | 1.34 | |
| C | 1432+79.06 | 946.99 | -1.50 | 947.38 | 1.50 | |
| | 1433+00 | 946.15 | -2.32 | 946.75 | 2.32 | |
| | 1433+25 | 945.21 | -3.29 | 946.07 | 3.29 | |
| | 1433+50 | 944.26 | -4.27 | 945.38 | 4.27 | |
| D (P.C.) | 1433+74.60 | 943.31 | -5.23 | 944.67 | 5.23 | |
| | 1434+00 | 942.33 | -6.22 | 943.95 | 6.22 | |
| | 1434+25 | 941.42 | -7.19 | 943.24 | 7.19 | |
| | E | 1434+40.60 | 940.80 | -7.80 | 942.83 | 7.80 |
| | | 1434+50 | 940.50 | -7.80 | 942.52 | 7.80 |
| | 1434+75 | 939.63 | -7.80 | 941.65 | 7.80 | |
| | 1435+00 | 938.71 | -7.80 | 940.73 | 7.80 | |
| | 1435+25 | 937.84 | -7.80 | 939.88 | 7.80 | |
| | 1435+50 | 936.96 | -7.80 | 938.98 | 7.80 | |
| | 1435+75 | 936.11 | -7.80 | 938.13 | 7.80 | |
| | 1436+00 | 935.26 | -7.80 | 937.28 | 7.80 | |
| | 1436+25 | 934.42 | -7.80 | 936.44 | 7.80 | |
| | 1436+50 | 933.57 | -7.80 | 935.59 | 7.80 | |
| | 1436+75 | 932.77 | -7.80 | 934.79 | 7.80 | |
| | 1437+00 | 931.97 | -7.80 | 933.99 | 7.80 | |
| | 1437+25 | 931.15 | -7.80 | 933.17 | 7.80 | |
| | 1437+50 | 930.33 | -7.80 | 932.35 | 7.80 | |
| | 1437+75 | 929.46 | -7.80 | 931.48 | 7.80 | |
| | 1438+00 | 928.59 | -7.80 | 930.61 | 7.80 | |
| | 1438+25 | 927.73 | -7.80 | 929.75 | 7.80 | |
| 1438+50 | 926.86 | -7.80 | 928.88 | 7.80 | | |
| 1438+75 | 926.11 | -7.80 | 928.13 | 7.80 | | |
| 1439+00 | 925.36 | -7.80 | 927.38 | 7.80 | | |
| 1439+25 | 924.56 | -7.80 | 926.58 | 7.80 | | |
| 1439+50 | 923.75 | -7.80 | 925.77 | 7.80 | | |
| 1439+75 | 922.95 | -7.80 | 924.97 | 7.80 | | |
| 1440+00 | 922.15 | -7.80 | 924.17 | 7.80 | | |
| 1440+25 | 921.34 | -7.80 | 923.36 | 7.80 | | |
| 1440+50 | 920.53 | -7.80 | 922.55 | 7.80 | | |
| 1440+75 | 919.78 | -7.80 | 921.80 | 7.80 | | |
| 1441+00 | 919.02 | -7.80 | 921.04 | 7.80 | | |
| 1441+25 | 918.35 | -7.80 | 920.37 | 7.80 | | |
| 1441+50 | 917.67 | -7.80 | 919.69 | 7.80 | | |
| 1441+75 | 917.01 | -7.80 | 918.98 | 7.80 | | |
| E | 1441+89.27 | 916.59 | -7.80 | 918.61 | 7.80 | |
| | 1442+00 | 916.35 | -7.38 | 918.27 | 7.38 | |
| | 1442+25 | 915.76 | -6.41 | 917.42 | 6.41 | |
| | 1442+50 | 915.16 | -5.43 | 916.58 | 5.43 | |
| D (P.T.) | 1442+55.27 | 915.08 | -5.23 | 916.44 | 5.23 | |
| | 1442+75 | 914.77 | -4.46 | 915.93 | 4.46 | |
| | 1443+00 | 914.37 | -3.48 | 915.27 | 3.48 | |
| | 1443+25 | 913.96 | -2.51 | 914.62 | 2.51 | |
| 1443+50 | 913.56 | -1.53 | 913.96 | 1.53 | | |
| C | 1443+50.81 | 913.56 | -1.50 | 913.95 | 1.50 | |
| | 1443+75 | 913.13 | -1.50 | 913.39 | 0.56 | |
| B | 1443+89.27 | 912.87 | -1.50 | 913.06 | 0.00 | |
| | 1444+00 | 912.68 | -1.50 | 912.82 | -0.40 | |
| | 1444+25 | 912.12 | -1.50 | 912.14 | -1.34 | |
| A | 1444+29.27 | 912.02 | -1.50 | 912.02 | -1.50 | |

| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION

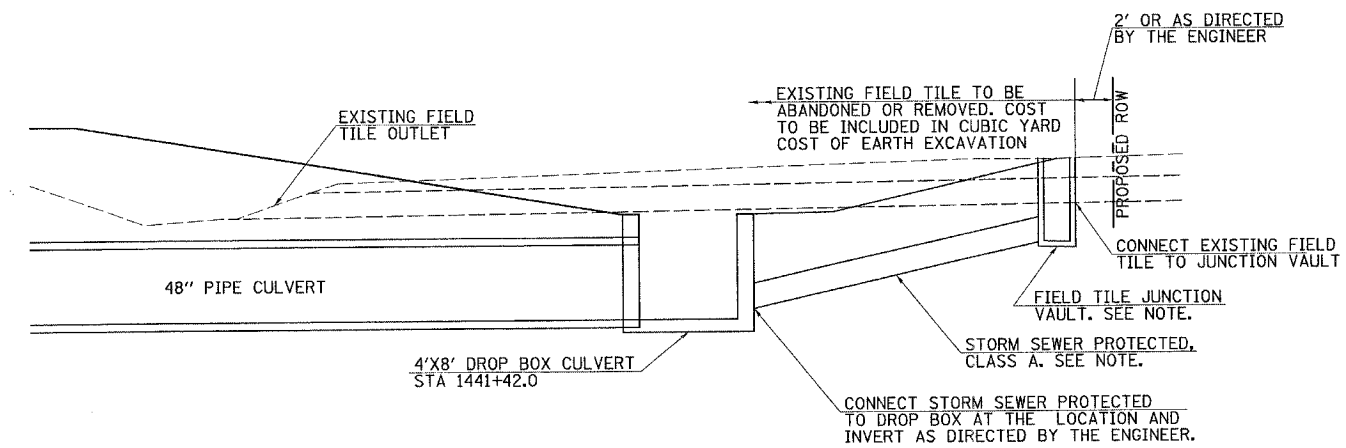
**FAP ROUTE 17
PAVEMENT ELEVATIONS**

DATE 8/10/2006

DRAWN BY JCW
CHECKED BY AWB

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 17 | 16-T | CARROLL | 47 | 19 |
| FED. ROAD DIST. NO. | ILLINOIS | | FED. AID PROJECT | |

CONTRACT NO. 64897



FIELD TILE REPLACEMENT DETAIL

NOTES:
 FIELD TILE SHALL BE REPLACED IN ACCORDANCE WITH SECTION 611 OF THE STANDARD SPECIFICATIONS. THE COST PER CONTRACT UNIT PRICE OF ITEMS INCLUDED IN THIS CONTRACT SHALL BE PAID FOR AS STATED IN SECTION 611 OF THE STANDARD SPECIFICATIONS. IF THE CONTRACT UNIT PRICE IS NOT INCLUDED IN THIS CONTRACT, PAYMENT FOR THIS WORK WILL BE IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

ALL EXISTING FIELD TILE SHALL BE REPLACED WITH STORM SEWER OF THE TYPE REQUIRED FOR BY THE DEPTH OF COVER. THE LINEAL MEASUREMENT WILL BE ALONG THE CENTRODIAL AXIS AND INCLUDE ALL BENDS, ELBOWS, OR PIPE TEE'S WHICH ARE REQUIRED.

EXISTING UNEXPOSED FIELD TILE SHALL BE LOCATED BOTH HORIZONTALLY AND VERTICALLY AT THE LOCATIONS DIRECTED BY THE ENGINEER BY THE USE OF AN EXPLORATION TRENCH. THIS TRENCH SHALL NOT BE LESS THAN 52 INCHES IN DEPTH, MEASURED FROM THE EXISTING GROUND ELEVATION. THE WIDTH OF THE TRENCH SHALL BE SUFFICIENT TO ALLOW PROPER INVESTIGATION OF THE ENTIRE TRENCH.

THE EXISTING TILE DRAINAGE SYSTEM SHALL BE MAINTAINED IN A FUNCTIONAL CONDITION BY IMMEDIATE CONSTRUCTION OF THE NEW STORM SEWER SYSTEM OR BY REPAIRING THE CUT TILE WITH THE SAME DIAMETER UNTIL SUCH TIME AS THE NEW SYSTEM IS COMPLETED.

USE FIELD TILE JUNCTION VAULT. 2' FOR ONE FIELD TILE CONNECTION AND FIELD TILE JUNCTION VAULT, 3' FOR TWO OR MORE FIELD TILE CONNECTIONS IN ACCORDANCE WITH ARTICLE 611.05 OF THE STANDARD SPECIFICATIONS.

STORM SEWER PROTECTED, CLASS A, 12" SHALL BE USED IN CONJUNCTION WITH FIELD TILE JUNCTION VAULT, 2' AND STORM SEWER PROTECTED, CLASS A, 15" SHALL BE USED IN CONJUNCTION WITH FIELD TILE JUNCTION VAULT, 3'.

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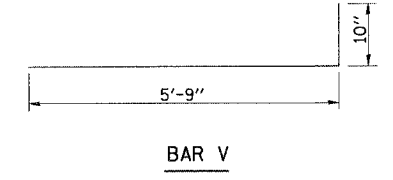
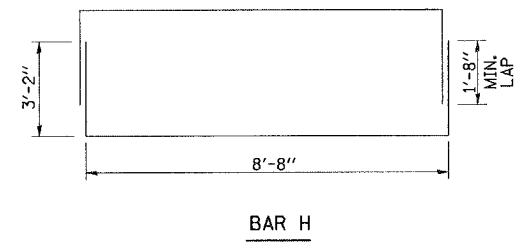
ILLINOIS DEPARTMENT OF TRANSPORTATION

FAP ROUTE 17
 FIELD TILE AND
 VAULT DETAILS

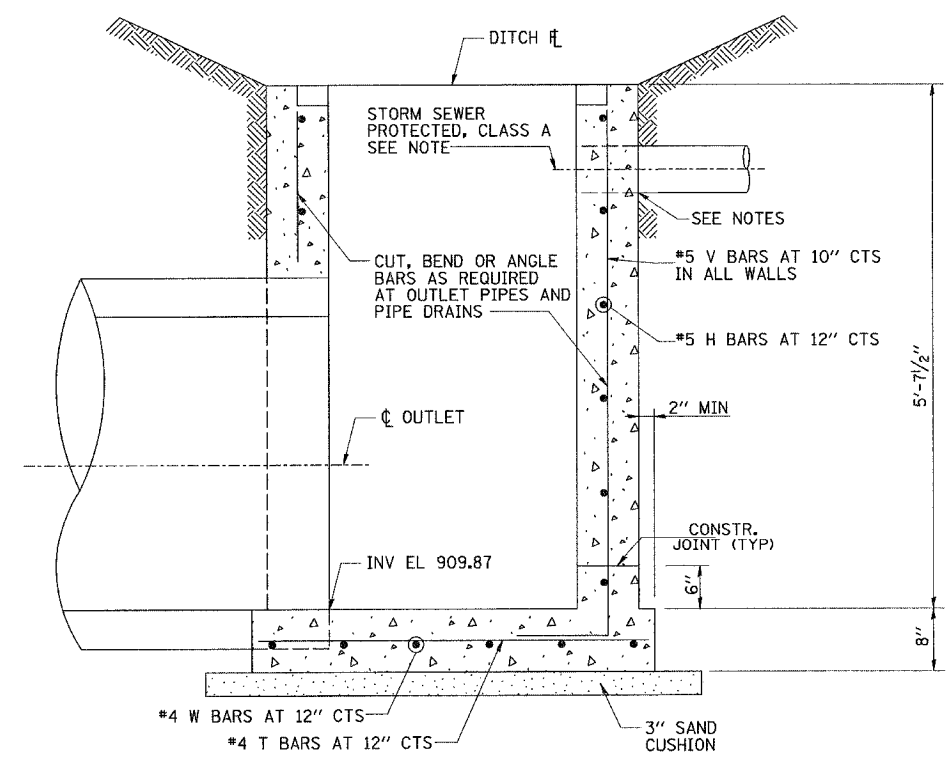
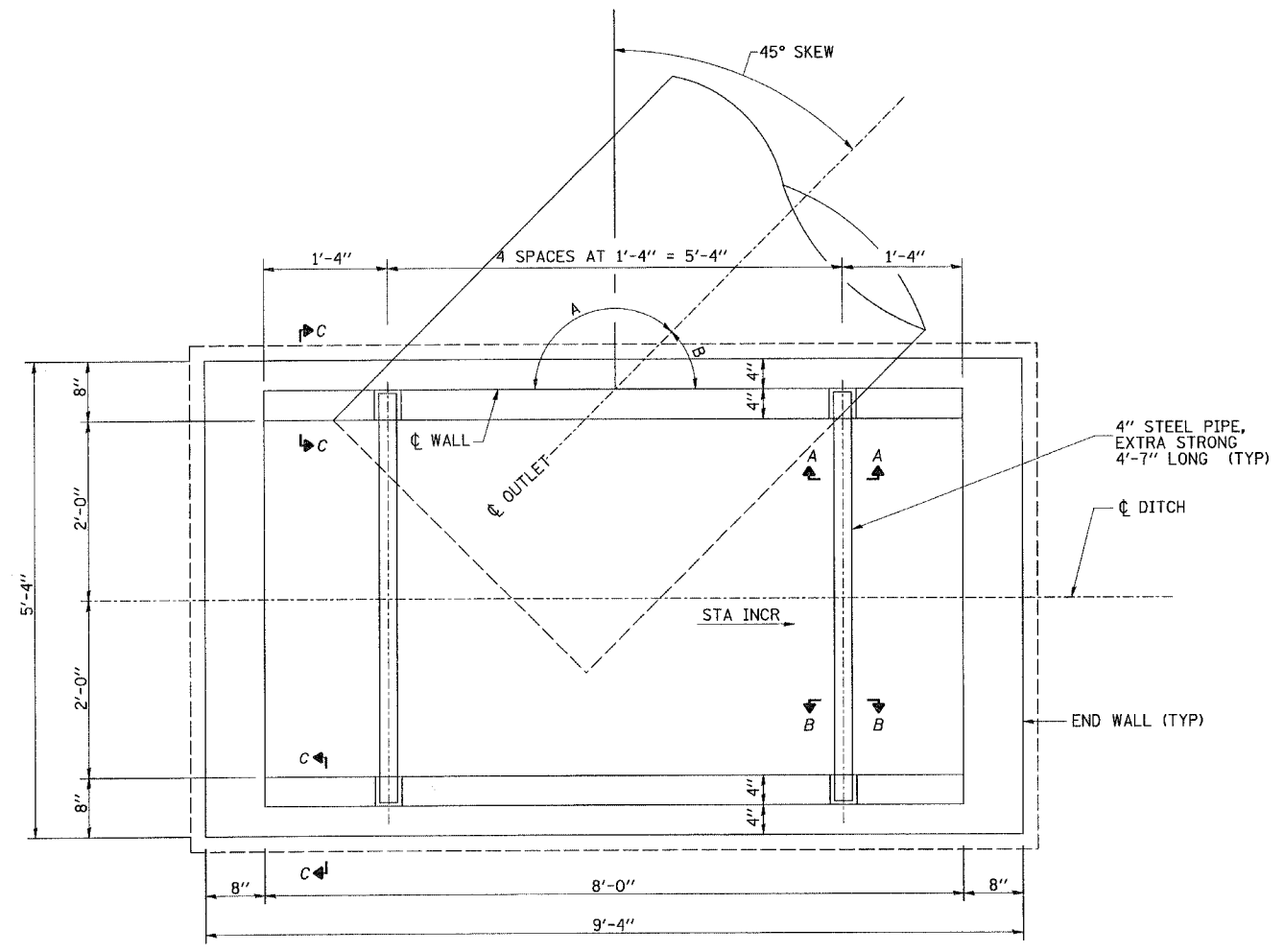
DATE 8/10/2006
 DRAWN BY TJD
 CHECKED BY AWM

CONTRACT NO. 64897

| STATION | OFFSET | OUTLET SIZE | SKEW | A | B | INV ELEV | DITCH # | CONCRETE 1 DROP STR CU YD | REINF BARS - 1 DROP STR | | | | | | | | BARS 1 DROP STR POUND | REMARKS |
|-----------|----------|-------------|--------|------|-----|----------|---------|---------------------------|-------------------------|----|-------------|----|-------------|----|-------------|----|-----------------------|---------|
| | | | | | | | | | #5 H - BARS | | #5 V - BARS | | #4 T - BARS | | #4 W - BARS | | | |
| | | | | | | | | | LGTH | NO | LGTH | NO | LGTH | NO | LGTH | NO | | |
| 1441+04.2 | 45.8' RT | 48" RCCP | 45° LF | 135° | 45° | 909.87 | 915.49 | 4.6 | 15'-0" | 12 | 6'-9" | 32 | 5'-5" | 10 | 9'-5" | 7 | 490 | |



NOTE: STEEL PIPE SHALL CONFORM TO ASTM A53 GRADE B OR ASTM A501.
 ALL STEEL PLATES SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M 270, GRADE 50.
 BOLTS AND NUTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM DESIGNATION A 307.
 ALL BOLTS, NUTS, WASHERS AND LOCK WASHERS SHALL BE GALVANIZED ACCORDING TO AASHTO M 232.
 ALL PLATES AND PIPES SHALL BE GALVANIZED AFTER SHOP FABRICATION ACCORDING TO AASHTO M 111 AND ASTM A 385.
 CALCULATED WEIGHT OF STEEL = 370 LBS.
 STORM SEWER PROTECTED, CLASS A, SHALL BE CONNECTED TO THE PROPOSED DROP BOX AT THE LOCATION AND INVERTS AS DIRECTED BY THE ENGINEER.
 ALL WORK AND MATERIAL INCORPORATED INTO THE CONSTRUCTION OF THE CULVERT DROP BOX SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "CULVERT DROP BOX".



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ILLINOIS DEPARTMENT OF TRANSPORTATION

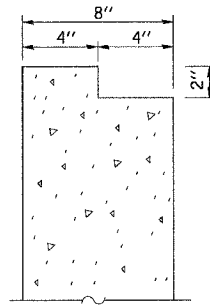
FAP ROUTE 17
 CULVERT DROP BOX DETAILS
 STA 1441+04.2

DRAWN BY TAC
 CHECKED BY BAN

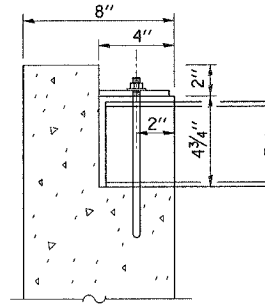
DATE 8/10/2006

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 17 | 16-T | CARROLL | 47 | 21 |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

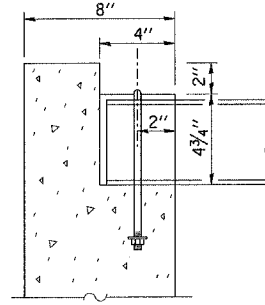
CONTRACT NO. 64897



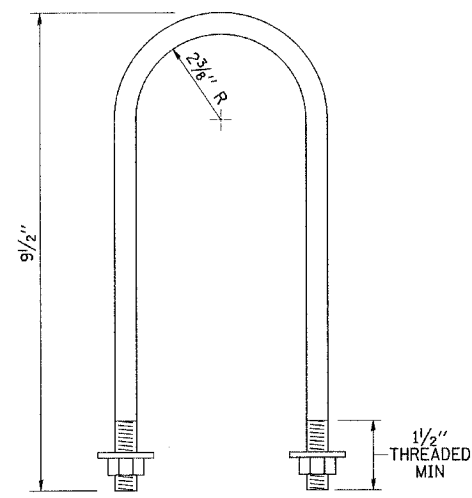
SECTION C-C



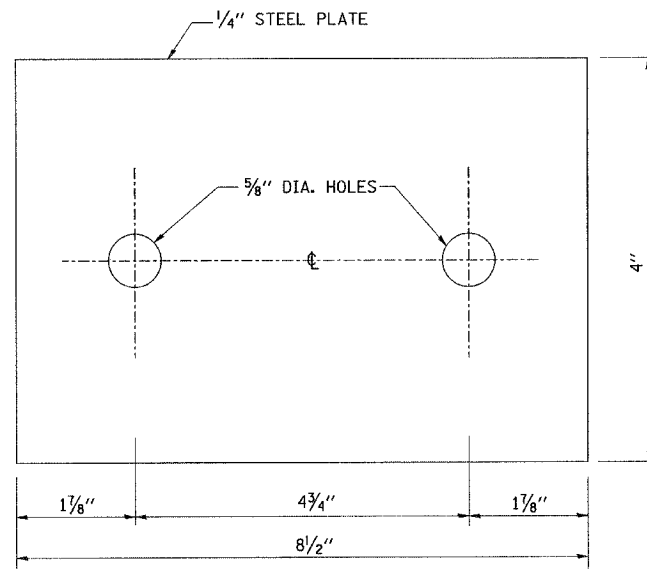
SECTION D-D



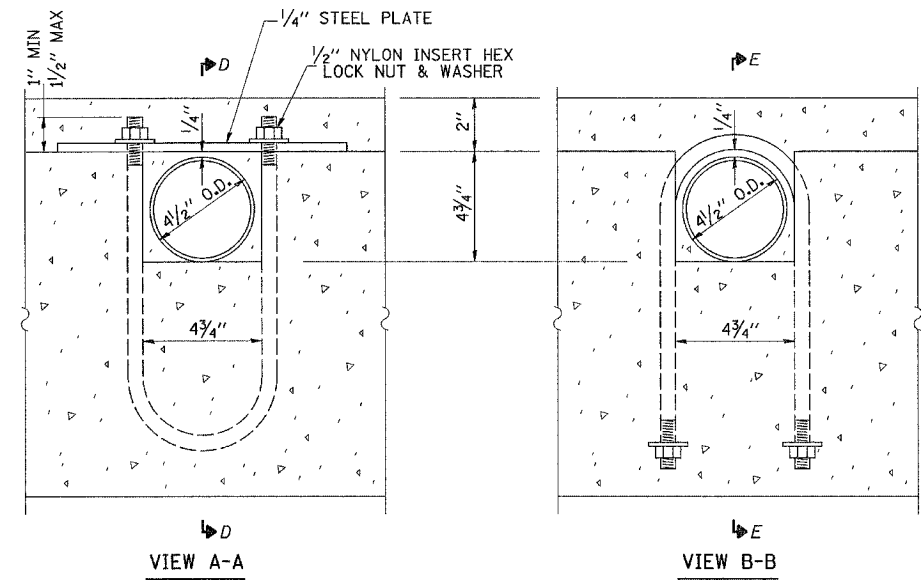
SECTION E-E



1/2" U BOLT
(10 REQ'D)



TOP ANCHOR PLATE
(5 REQ'D)



VIEW A-A

VIEW B-B

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ILLINOIS DEPARTMENT OF TRANSPORTATION

FAP ROUTE 17
CULVERT DROP BOX DETAILS
STA 1441+04.2

DATE 8/10/2006
DRAWN BY TAC
CHECKED BY BAN

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|---------------------|---------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 17 | 16-T | CARROLL | 47 | 22 |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

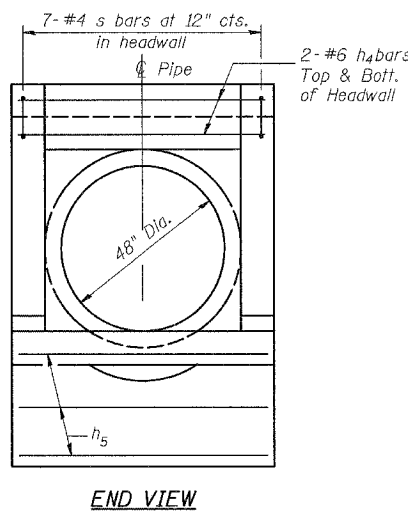
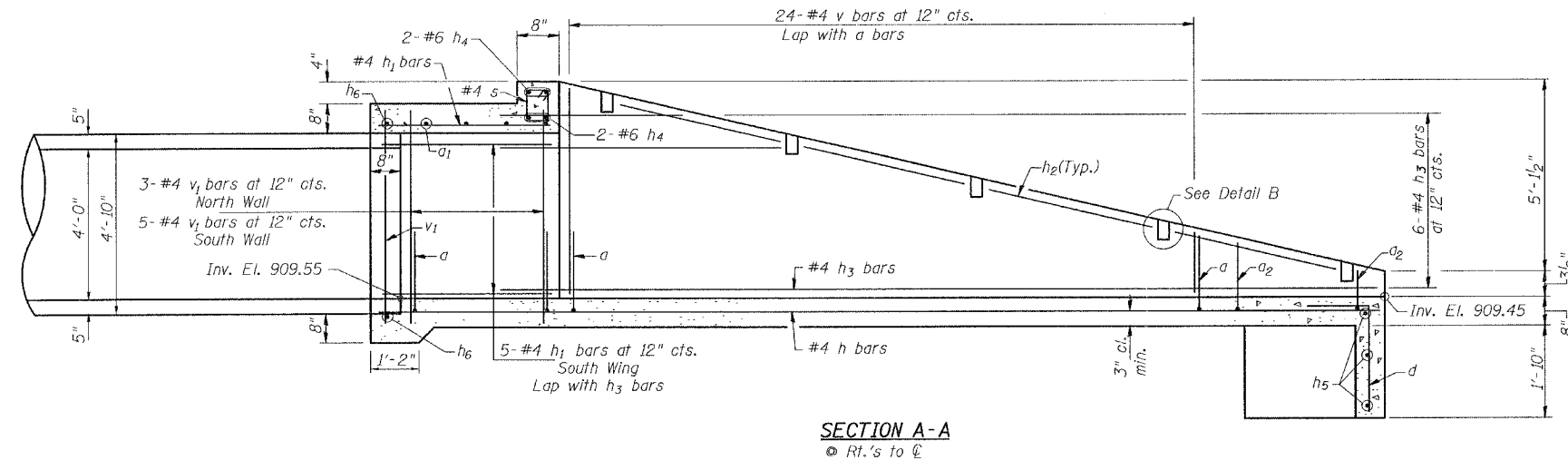
CONTRACT NO. 64897

NOTE: CLASS SI CONCRETE SHALL BE USED THROUGHOUT.
 EXPOSED EDGES SHALL BE BEVELED 3/4"
 REINFORCEMENT SHALL CONFORM TO THE REQUIREMENTS OF AASHTO 31 OR M322, GRADE 60.
 STEEL PIPE SHALL CONFORM TO ASTM A53 GRADE B OR ASTM A501.
 ALL STEEL PLATES SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M 270, GRADE 50.
 BOLTS AND NUTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM DESIGNATION A 307.
 ALL BOLTS, NUTS, WASHERS AND LOCK WASHERS SHALL BE GALVANIZED ACCORDING TO AASHTO M 322.
 ALL PLATES AND PIPES SHALL BE GALVANIZED AFTER SHOP FABRICATION ACCORDING TO AASHTO M 111 AND ASTM A 385.
 CALCULATED WEIGHT OF STEEL = 1,280 LBS.
 ALL WORK AND MATERIAL INCORPORATED INTO THE CONSTRUCTION OF THE GRATED CULVERT EXTENSION, NO 1, SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "GRATED CULVERT EXTENSION, NO 1".

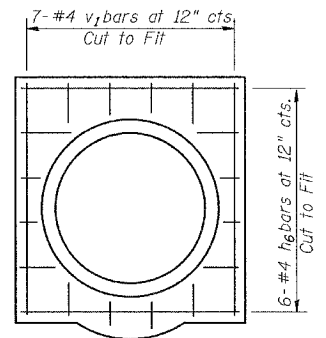
(FOR INFORMATION ONLY)

BILL OF MATERIAL

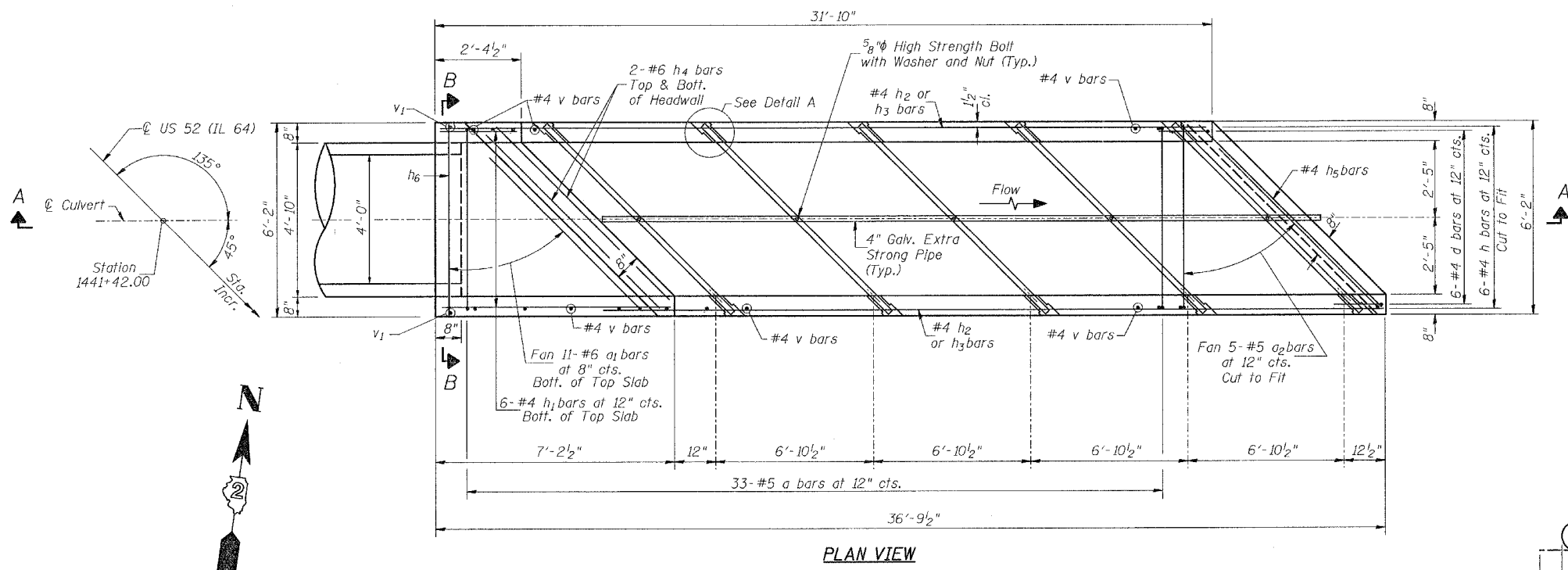
| BAR | NO. | SIZE | LENGTH | SHAPE |
|---------------------------------|-----|-------|--------|-------|
| a | 33 | #5 | 9'-11" | L |
| a ₁ | 11 | #6 | 6'-5" | C |
| a ₂ | 5 | #5 | 7'-7" | J |
| d | 6 | #4 | 3'-9" | T |
| h | 6 | #4 | 36'-6" | — |
| h ₁ | 11 | #4 | 6'-11" | — |
| h ₂ | 8 | #4 | 5'-11" | — |
| h ₃ | 6 | #4 | 31'-7" | — |
| h ₄ | 4 | #6 | 7'-11" | — |
| h ₅ | 3 | #4 | 7'-11" | — |
| h ₆ | 6 | #4 | 5'-11" | — |
| s | 7 | #4 | 3'-1" | □ |
| v | 24 | #4 | 5'-2" | — |
| v ₁ | 15 | #4 | 4'-8" | — |
| Concrete Structures | | CU YD | 11.8 | |
| Reinforcement Bars | | FOUND | 980 | |
| 4" Galvanized Extra Strong Pipe | | FOOT | 75 | |
| 4" Galv. Pipe Caps | | EACH | 10 | |



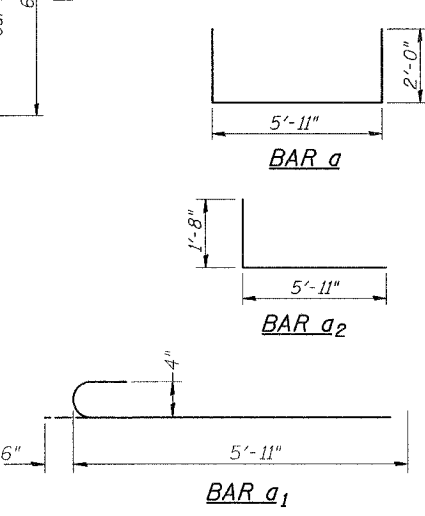
END VIEW



SECTION B-B

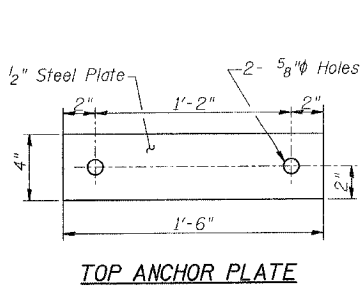


PLAN VIEW

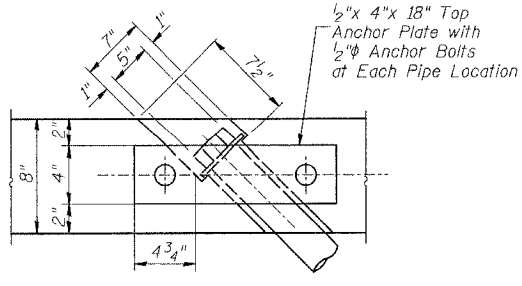


BAR a₁

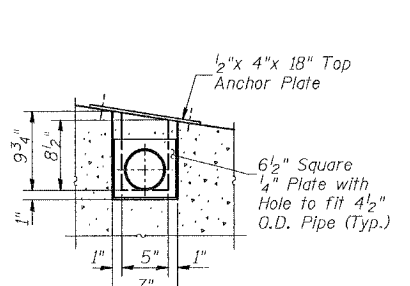
BAR s



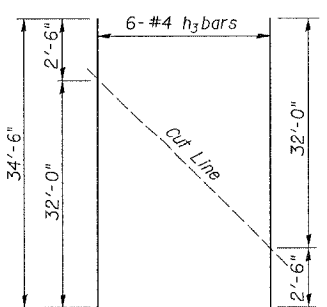
TOP ANCHOR PLATE



DETAIL A

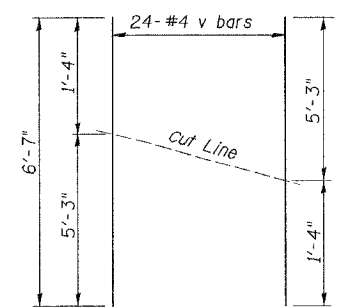


DETAIL B



BAR CUTTING DIAGRAM

Order h₃ bars full length. Cut as shown and use remainder of bars in opposite wingwall.



BAR CUTTING DIAGRAM

Order v bars full length. Cut as shown and use remainder of bars in opposite wingwall.

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ILLINOIS DEPARTMENT OF TRANSPORTATION
 FAP ROUTE 17
 GRATED CULVERT EXTENSION, NO 1
 STA 1441+70.6
 DRAWN BY TAC
 CHECKED BY AN
 DATE 8/10/2006

STORM WATER POLLUTION PREVENTION PLAN

EROSION CONTROL PLAN

| CONTRACT NO. 64897 | | | | |
|---------------------|---------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 17 | 16-T | CARROLL | 47 | 23 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

THE FOLLOWING PLAN WAS ESTABLISHED AND INCLUDED IN THESE PLANS TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE UNDER NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE SILTATION WITHIN THE CONSTRUCTION ZONE AND TO ELIMINATE SEDIMENTS FROM ENTERING AND LEAVING THE CONSTRUCTION ZONE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN ITEMS, AS SHOWN IN THIS PLAN AND REFERENCED BY THE LEGEND, SHALL BE PLACED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE PLACED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION RESULTING FROM THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL PLACE PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A REASONABLE AMOUNT OF TIME; THEREFORE, REDUCING THE AMOUNT OF AREA BEING OPEN TO THE POSSIBILITY OF EROSION AND REDUCING THE AMOUNT OF TEMPORARY EROSION CONTROL SEEDING. THE RESIDENT ENGINEER WILL DETERMINE IF TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED, THE SIZE OF THE PROPOSED DITCH CHECKS, THE PROPER METHOD OF INSTALLATION, AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS SHALL BE ADDED WHICH ARE NOT INCLUDED IN THE PLANS. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN IN STANDARD 280001 OF THE PLANS.

SITE DESCRIPTION

DESCRIPTION OF CONSTRUCTION ACTIVITY:

THIS PROJECT CONSISTS OF WIDENING AND RESURFACING 0.233 MI OF TWO LANE HIGHWAY IN CARROLL COUNTY. CONSTRUCTION CONSIST OF CONSTRUCTING CULVERTS, WIDENING, BITUMINOUS RESURFACING, PLACING BITUMINOUS, AGGREGATE SHOULDERS AND GRADING AND OTHER MISCELLANEOUS WORK.

DESCRIPTION OF INTENDED SEQUENCE OF ACTIVITIES:

THE SEQUENCE OF EVENTS ARE AS FOLLOW: DRAINAGE STRUCTURES, EMBANKMENT, EXCAVATION, GRADING AND PAVING.

THIS PROJECT WILL BE CONSTRUCTED IN SEGMENTS AS SHOWN IN THE MAINTENANCE OF TRAFFIC PLAN AND SPECIAL PROVISIONS.

TOTAL CONSTRUCTION SITE (CONSTRUCTION LIMIT TO CONSTRUCTION LIMIT): 3.1 ACRES.
PROPOSED R.O.W (TOTAL PARCEL AREA): 1.8 ACRES.

DISTURBED BY EXCAVATION (E.O.P TO CONSTRUCTION LIMIT): 2.2 ACRES.

SUPPORTING REPORTS AND PLANS

THE FOLLOWING ASSISTED IN DEVELOPING THE EROSION CONTROL PLAN AS REFERENCED DOCUMENTS:

FIELD REVIEWS, PROJECT DESIGN REPORT, HYDRAULIC REPORT
USGS DRAINAGE MAPS, PROJECT PLAN DOCUMENTS

DRAINAGE TRIBUTARIES RECEIVING WATER FROM CONSTRUCTION SITE:
TRIBUTARY TO ELKHORN CREEK

EROSION CONTROLS AND SEDIMENT CONTROL PROCEDURES

STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:
PERIMETER EROSION CONTROL SHALL BE PLACED PRIOR TO BEGINNING EARTHWORK.

STABILIZATION PRACTICES DURING CONSTRUCTION:

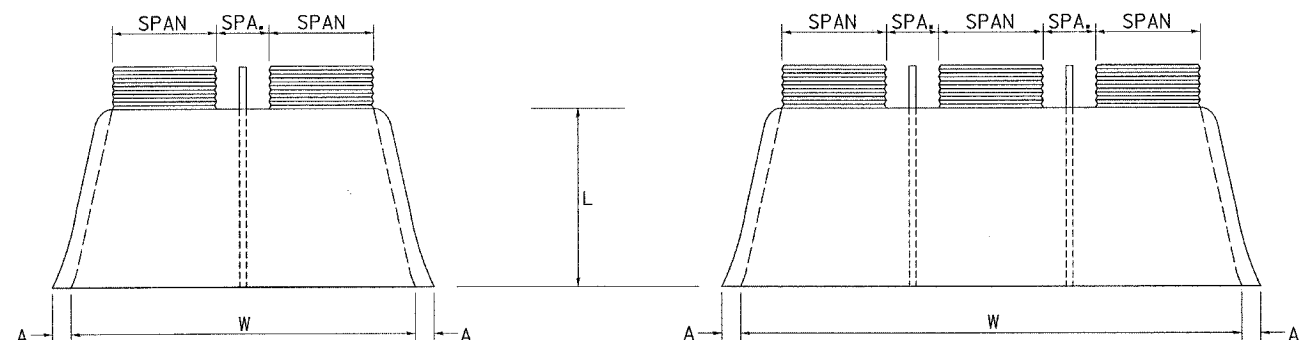
AS EARTH EXCAVATION AND EMBANKMENT ARE BEING COMPLETED THE CONTRACTOR SHALL PLACE DITCH CHECKS, INLET AND PIPE PROTECTION, EROSION CONTROL BLANKET, AND TEMPORARY EROSION CONTROL SEEDING AS STAGES OF THE PROJECT ARE COMPLETED. PERIMETER EROSION BARRIER WILL BE INSTALLED AT ADDITIONAL LOCATIONS AS THE PROJECT PROGRESSES. TEMPORARY EROSION CONTROL SEEDING SHALL BE COMPLETED AS SPECIFIED IN THE STANDARD SPECIFICATIONS.

MAINTENANCE AFTER FINAL GRADING

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS SEEDED AND ESTABLISHED WITH THE PROPER STAND. ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP AND DISTURBED TURF RESEEDDED.

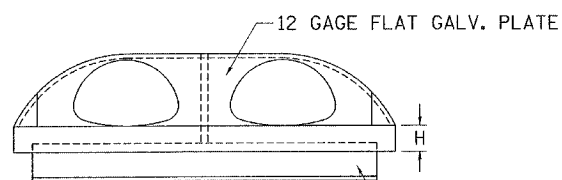
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|---------------------------|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 24 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CORRUGATED STEEL PIPE MULTIPLE END SECTIONS

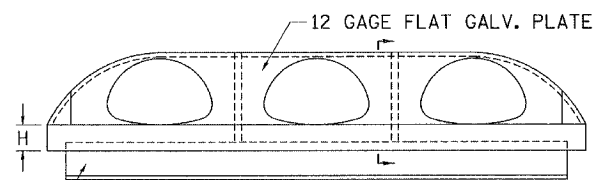


PLAN

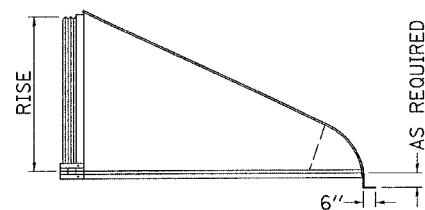
PLAN



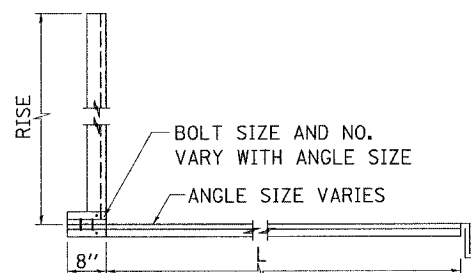
ELEVATION



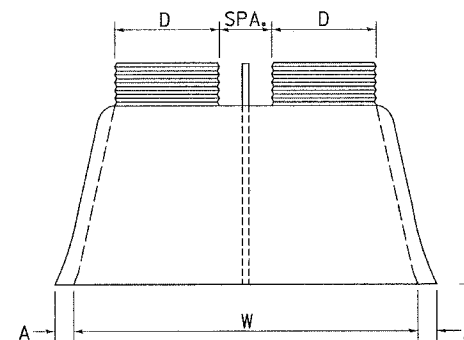
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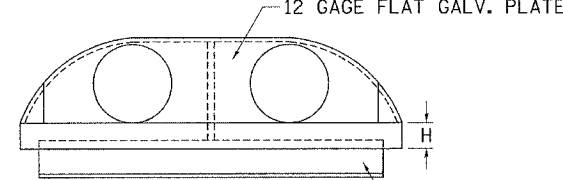
SIDE VIEW



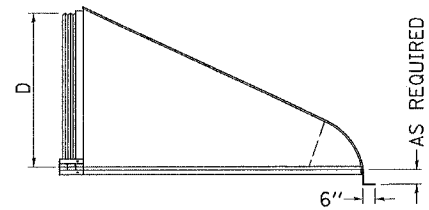
SECTION VIEW



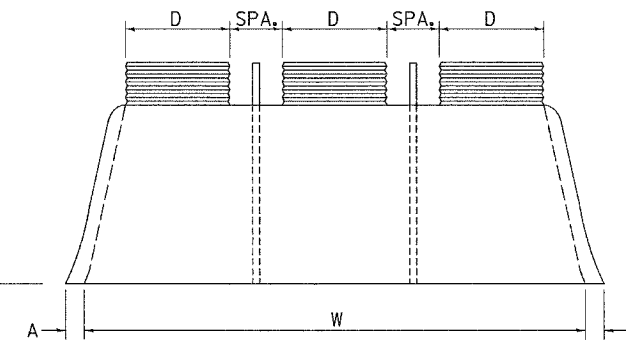
PLAN



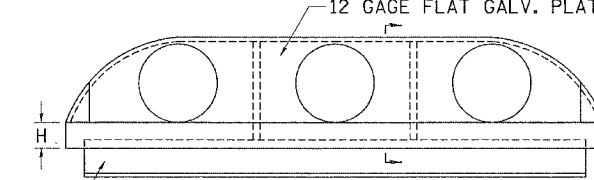
ELEVATION



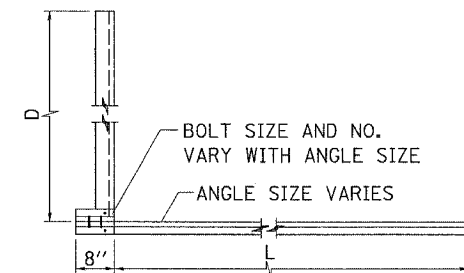
SIDE VIEW



PLAN



ELEVATION



ANGLE SECTION VIEW

| PIPE-ARCH MULTIPLE INLET END SECTIONS | | | | | | | | | | |
|--|-----------------|-------|--------------|-----------|-----------|-----------|-------------|-------------|----------------------|--|
| SPAN x RISE 2-2/3" x 1/2" | EQUIV. ROUND | GAGE | SPA. (In) | A (In) | H (In) | L (In) | DOUBLE W | TRIPLE W | REINFORCING ANGLE | |
| 17 x 13 | 15 | 16 | 12 | 6 1/2 | 6 | 20 | 59 | 88 | 2 x 2 x 1/4 | |
| 21 x 15 | 18 | 16 | 12 | 7 1/2 | 6 | 24 | 69 | 102 | 2 x 2 x 1/4 | |
| 24 x 18 | 21 | 16 | 12 | 8 | 6 | 28 | 78 | 114 | 2 x 2 x 1/4 | |
| 28 x 20 | 24 | 16 | 12 | 8 | 6 | 32 | 88 | 128 | 5 x 3 x 1/4 | |
| 35 x 24 | 30 | 14 | 12 | 10 | 6 | 39 | 107 | 154 | 5 x 3 x 1/4 | |
| 42 x 29 | 36 | 14 | 14 | 12 | 7 1/2 | 46 | 131 | 187 | 5 x 3 x 1/4 | |
| 49 x 33 | 42 | 12 | 17 | 13 1/2 | 9 | 53 | 150 | 216 | 5 x 3 x 1/4 | |
| 57 x 38 | 48 | 12 | 19 | 18 1/2 | 12 | 62 | 166 | 242 | 6 x 4 x 3/8 | |
| 64 x 43 | 54 | 12 | 22 | 18 | 12 | 69 | 188 | 274 | 6 x 4 x 3/8 | |
| 71 x 47 | 60 | 12/10 | 24 | 18 1/2 | 12 | 77 | 209 | 304 | 6 x 4 x 3/8 | |
| 77 x 62 | 66 | 12/10 | 26 | 18 | 12 | 77 | 229 | 332 | 6 x 4 x 3/8 | |
| 83 x 67 | 72 | 12/10 | 28 | 18 | 12 | 77 | 243 | 354 | 6 x 4 x 3/8 | |
| SPAN x RISE 3"x1" & 5"x1" | EQUIV. ROUND | GAGE | SPA. (In) | A (In) | H (In) | L (In) | DOUBLE W | TRIPLE W | REINFORCING ANGLE | |
| 60 x 46 | 54 | 12 | 20 | 18 | 12 | 70 | 182 | 262 | 6 x 4 x 3/8 | |
| 66 x 51 | 60 | 12/10 | 22 | 18 | 12 | 77 | 202 | 290 | 6 x 4 x 3/8 | |
| 73 x 55 | 66 | 12/10 | 25 | 18 | 12 | 77 | 224 | 322 | 6 x 4 x 3/8 | |
| 81 x 69 | 72 | 12/10 | 27 | 18 | 12 | 77 | 246 | 354 | 6 x 4 x 3/8 | |

| ROUND PIPE MULTIPLE INLET END SECTIONS | | | | | | | | | |
|---|-------|--------------|-----------|-----------|-----------|-------------|-------------|----------------------|--|
| PIPE DIA. (D) (In) | GAGE | SPA. (In) | A (In) | H (In) | L (In) | DOUBLE W | TRIPLE W | REINFORCING ANGLE | |
| 12 | 16 | 12 | 6 1/2 | 6 | 21 | 48 | 72 | 2 x 2 x 1/4 | |
| 15 | 16 | 12 | 7 1/2 | 6 | 26 | 57 | 84 | 2 x 2 x 1/4 | |
| 18 | 16 | 12 | 8 | 6 | 31 | 66 | 96 | 2 x 2 x 1/4 | |
| 21 | 16 | 12 | 10 | 6 | 36 | 75 | 108 | 2 x 2 x 1/4 | |
| 24 | 16 | 12 | 10 | 6 | 41 | 84 | 120 | 5 x 3 x 1/4 | |
| 30 | 14 | 15 | 12 1/4 | 8 | 51 | 102 | 147 | 5 x 3 x 1/4 | |
| 36 | 14 | 18 | 14 1/2 | 9 | 60 | 126 | 180 | 5 x 3 x 1/4 | |
| 42 | 12 | 21 | 17 | 10 1/2 | 69 | 147 | 210 | 5 x 3 x 1/4 | |
| 48 | 12 | 24 | 18 1/2 | 12 | 79 | 162 | 234 | 6 x 4 x 1/16 | |
| 54 | 12 | 27 | 18 1/2 | 12 | 84 | 183 | 264 | 6 x 4 x 1/16 | |
| 60 | 12/10 | 30 | 18 | 12 | 88 | 204 | 294 | 6 x 4 x 1/16 | |
| 66 | 12/10 | 33 | 18 | 12 | 87 | 219 | 318 | 6 x 4 x 1/16 | |
| 72 | 12/10 | 36 | 18 | 12 | 88 1/2 | 228 | 336 | 6 x 4 x 1/16 | |
| 78 | 12/10 | 36 | 18 | 12 | 87 1/2 | 252 | 366 | 6 x 4 x 1/16 | |
| 84 | 12/10 | 36 | 18 | 12 | 87 1/2 | 254 | 384 | 6 x 4 x 1/16 | |

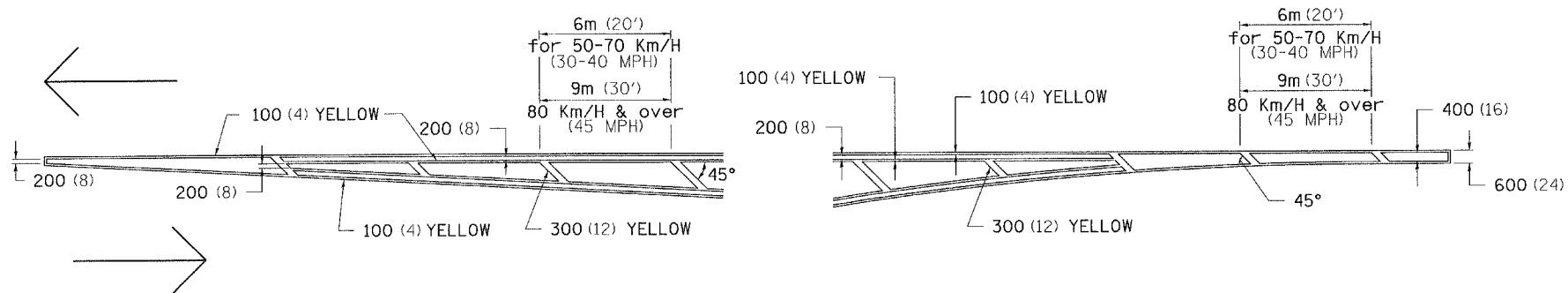
NOTE: ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED

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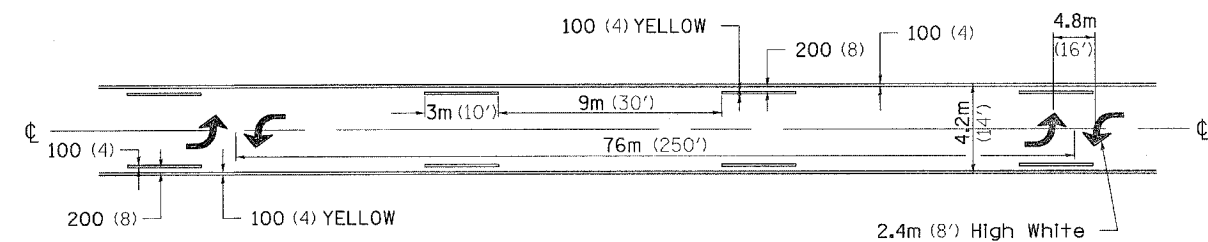
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|---------------------|---------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 17 | 16-T | CARROLL | 47 | 25 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

TYPICAL PAVEMENT MARKINGS

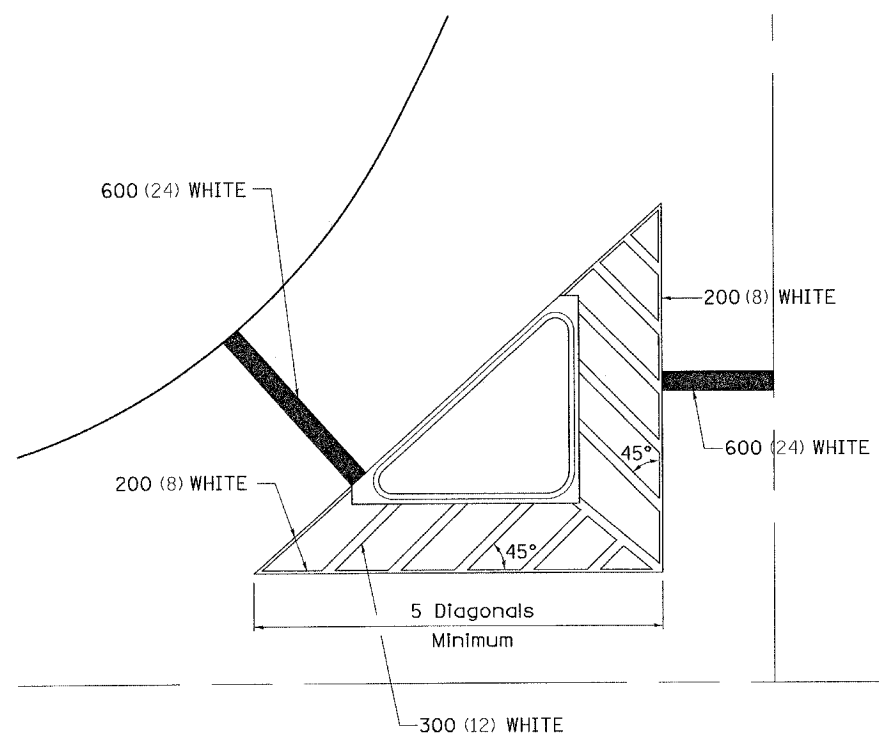
TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN AT LEFT TURN LANE



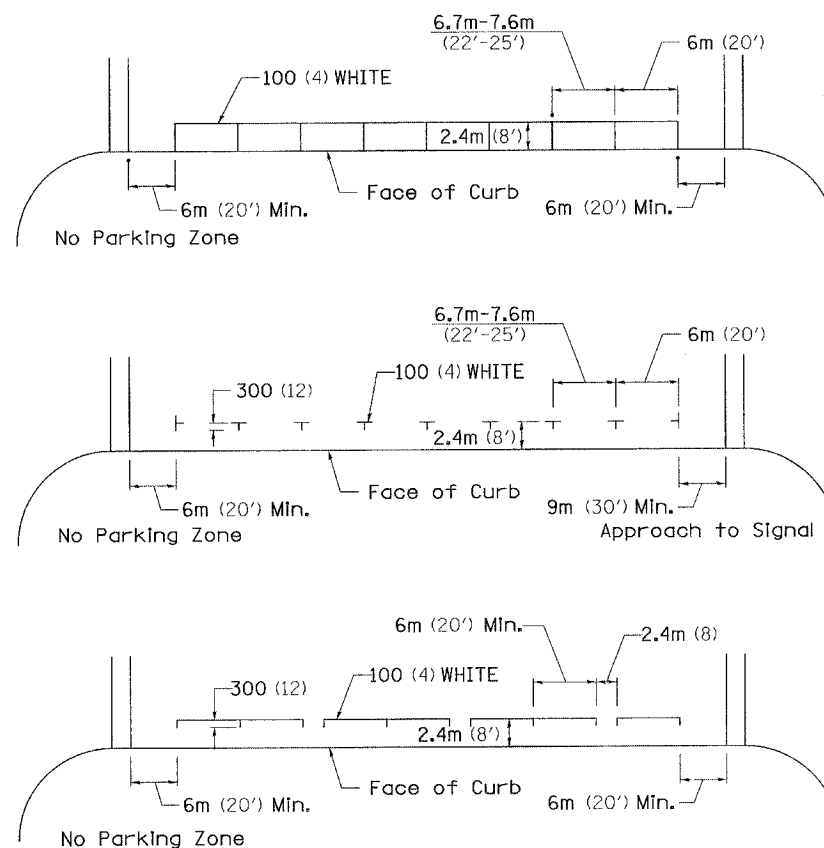
MEDIAN PAVEMENT MARKING



TYPICAL ISLAND OFFSET SHOULDER WIDTH



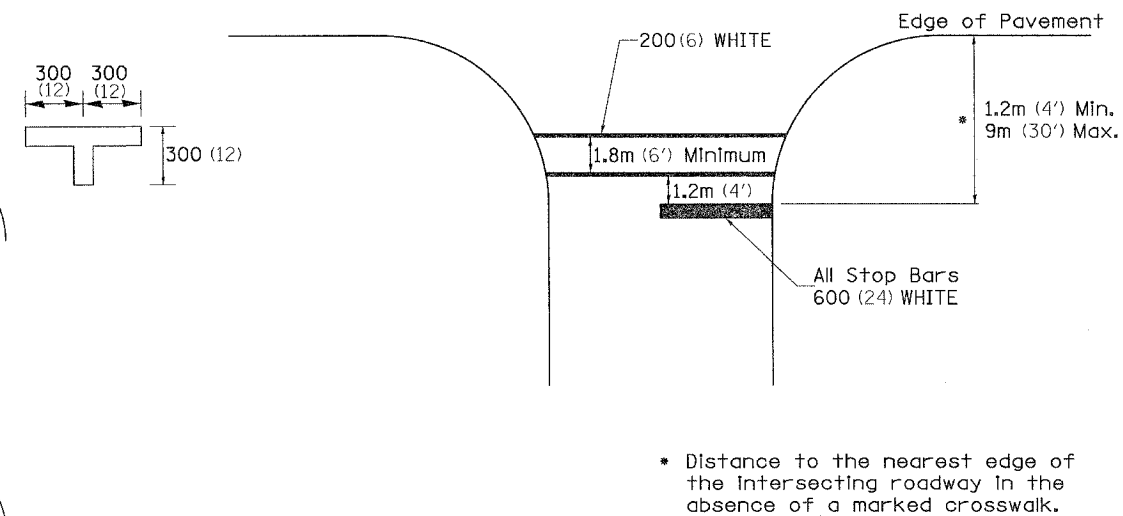
TYPICAL PARKING SPACING



** ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

STANDARD CROSSWALK MARKING

See Schedules for Locations



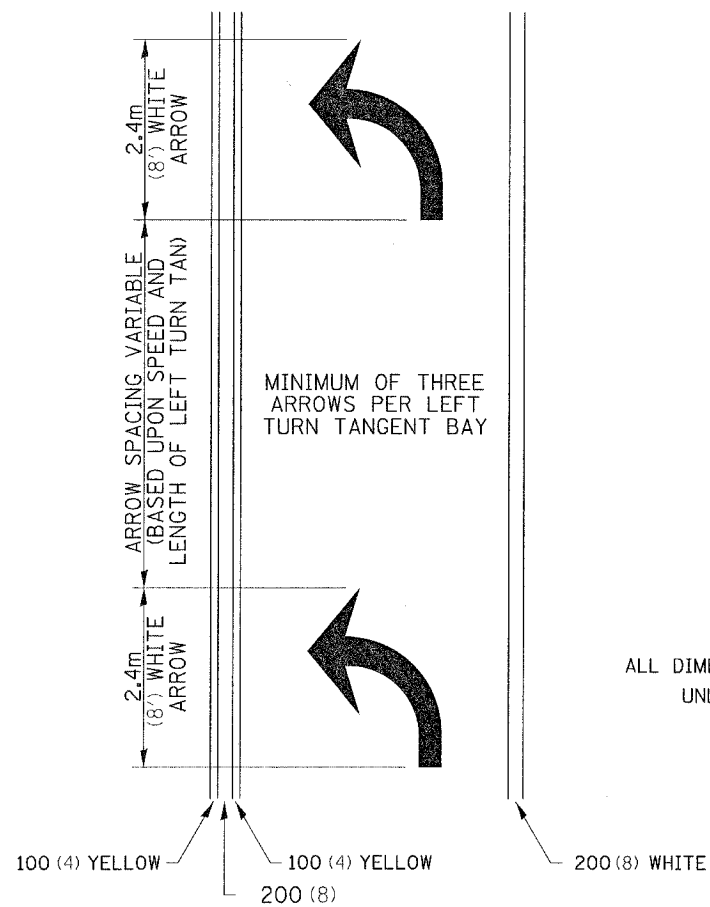
* Distance to the nearest edge of the intersecting roadway in the absence of a marked crosswalk.

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|---------------------------|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 26 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

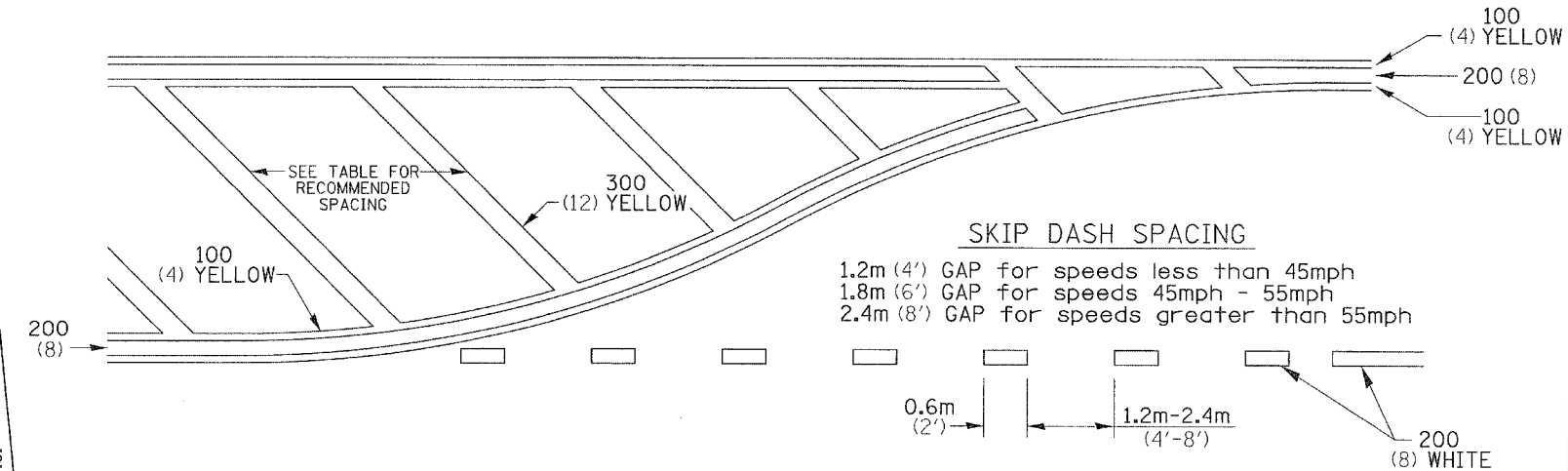
TYPICAL PAVEMENT MARKINGS

ARROW LAYOUT



12.2m
6 at (40') O.C.
APPROACH SIDE ONLY

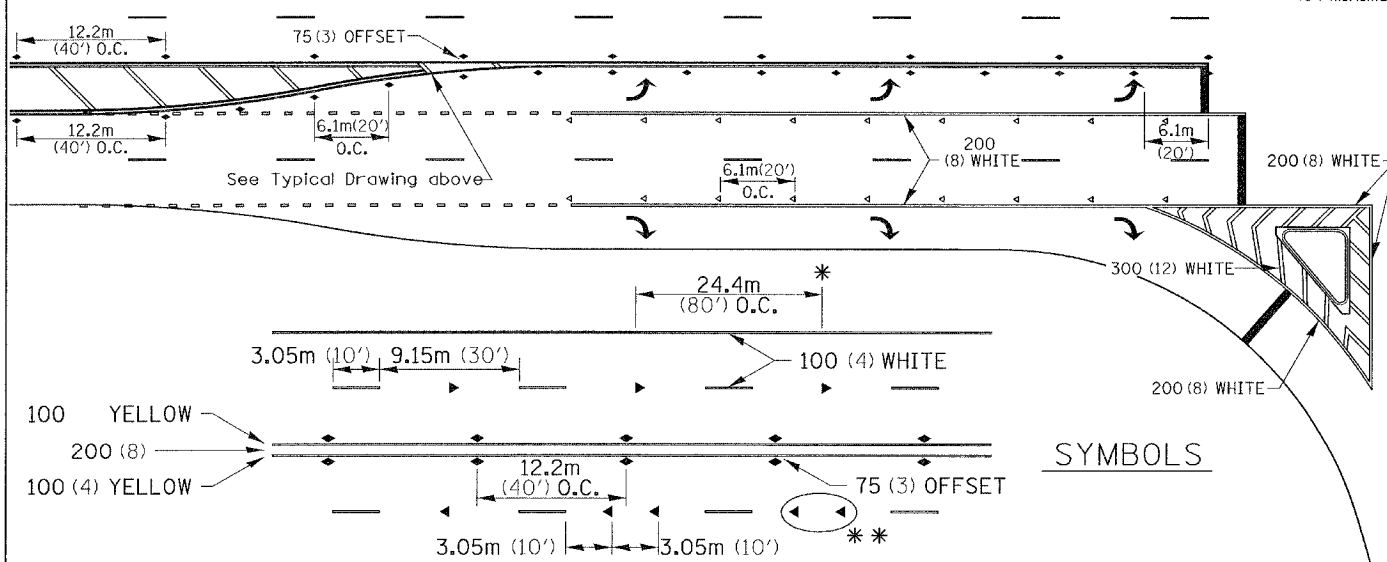
TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN



RECOMMENDED SPACING BETWEEN DIAGONALS (IN FEET)

| Speed Limit Range | Continuous Median Area | Intersection Channelization | Objects (Islands) |
|--------------------------|------------------------|-----------------------------|-------------------|
| less than 50Km/H (30MPH) | 15.3m (50') | 4.53m (15') | 3.05m (10') |
| 50-60Km/H (30-40MPH) | 22.9m (75') | 6.1m (20') | 4.53m (15') |
| 70Km/H (45MPH) & over | 22.9m (75') | 9.05m (30') | 6.1m (20') |

NOTE: if the spacing recommended in the Table does not permit at least five diagonal lines in the area being marked, the spacing from the next lowest speed range should be used. The recommended spacing is measured parallel to the pavement center line.

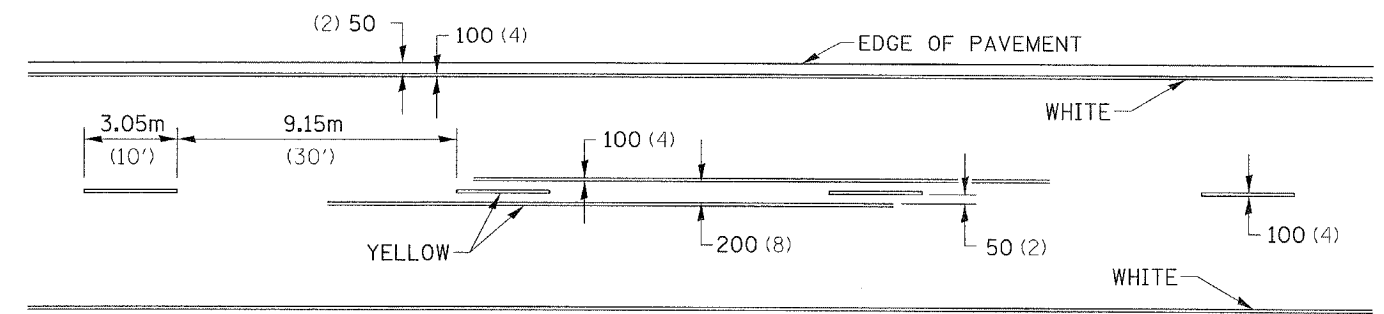


SYMBOLS

See Typical Drawing above

12.2m
6 at (40') O.C.
APPROACH SIDE ONLY

TYPICAL PAVEMENT MARKING FOR TWO LANE SECTION - NO PASSING ZONES



* REDUCE TO 12.2m (40') O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 15Km/H (10MPH) LOWER THAN POSTED SPEEDS.

** USE DOUBLE MARKERS WHEN ADT ≥ 25,000

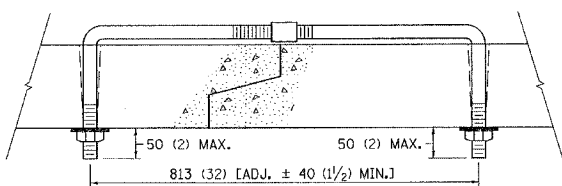
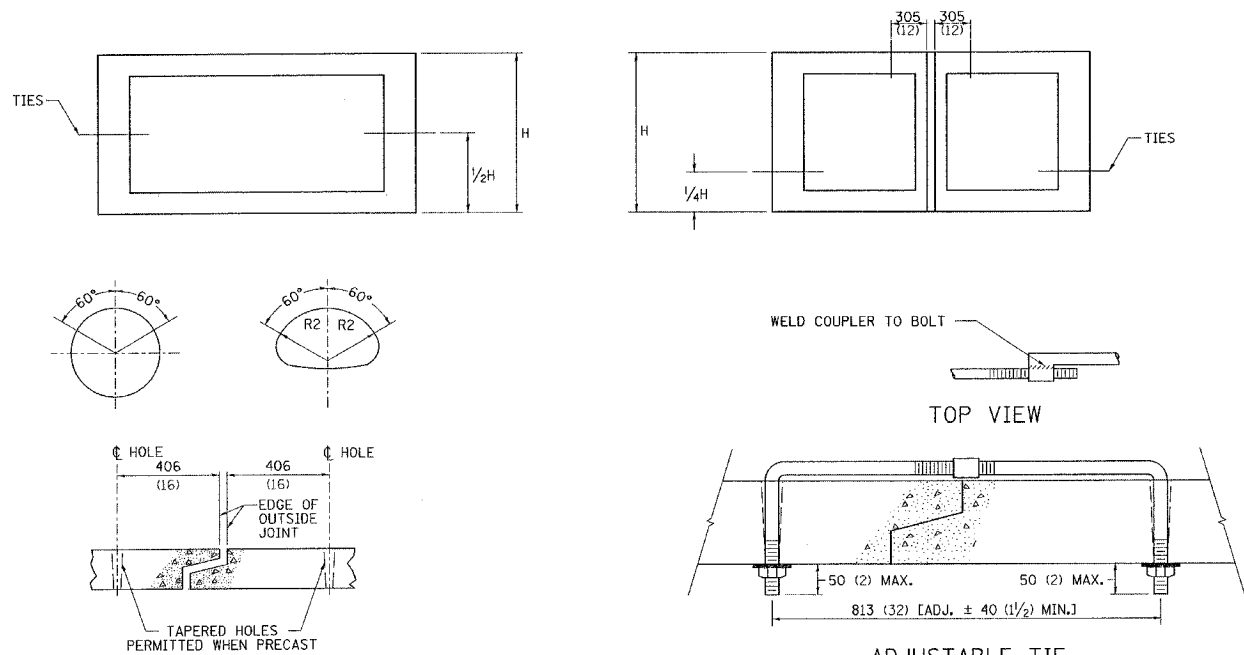
MULTI-LANE / UNDIVIDED

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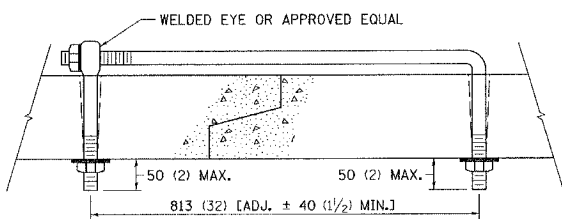
MECHANICAL JOINTS FOR CONCRETE PIPE AND BOX CULVERTS

| | | | | |
|---------------------|---------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 17 | 16-T | CARROLL | 47 | 28 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

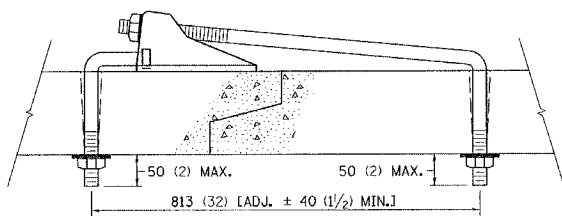
THE CULVERT TIES SHALL BE INCLUDED IN THE COST OF THE CONCRETE PIPE CULVERTS OR THE PRECAST CONCRETE BOX CULVERT. THE MECHANICAL TIES SHALL BE ON THE OUTSIDE OF THE CULVERT. THE NUTS AND WASHERS SHALL BE PLACED ON THE INSIDE OF OF THE CULVERT AND COVERED WITH MASTIC JOINT SEALER CONFORMING TO ARTICLES 1055 OR 1056 IN THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.



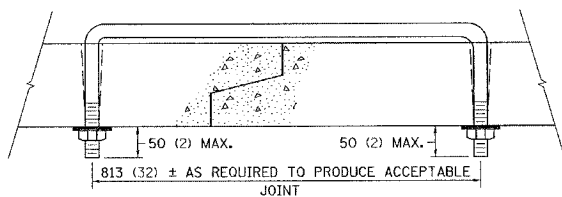
ADJUSTABLE TIE



EYE BOLT TIE



CANOPY TIE



U BOLT TIE

| PLACEMENT OF HOLES | | |
|---------------------------|--------------------------------|-----------------|
| BOX CULVERT METERS (FEET) | PIPE SIZE MILLIMETERS (INCHES) | THREAD DIAMETER |
| | 300 (12) | 16 (5/8) |
| | 380 (15) | ROLLED THREADS |
| | 450 (18) | (SEE NOTE 4) |
| | 530 (21) | |
| | 600 (24) | |
| | 680 (27) | |
| | 760 (30) | |
| 0.9x0.6 (3x2) | 830 (33) | 19 (3/4) |
| 0.9x0.9 (3x3) | 910 (36) | CUT OR ROLLED |
| 1.2x0.6 (4x2) | 1060 (42) | |
| 1.2x0.9 (4x3) | 1210 (48) | |
| 1.2x1.2 (4x4) | 1370 (54) | |
| 1.5x0.9 (5x3) | 1520 (60) | |
| 1.5x1.2 (5x4) | 1670 (66) | |
| 1.5x1.5 (5x5) | 1820 (72) | |
| 1.8x (6x *) | 1980 (78) | |
| 2.1x (7x *) | 2130 (84) | 25 (1) |
| 2.4x (8x *) | 2280 (90) | CUT OR ROLLED |
| 2.7x (9x *) | 2430 (96) | |
| 3.0x (10x *) | 2590 (102) | |
| | 2740 (108) | |
| | 3040 (120) | |
| | 3350 (132) | |
| 3.4x (11x *) | 3500 (138) | 35 (1 1/4) |
| AND GREATER | AND GREATER | |

NOTES:

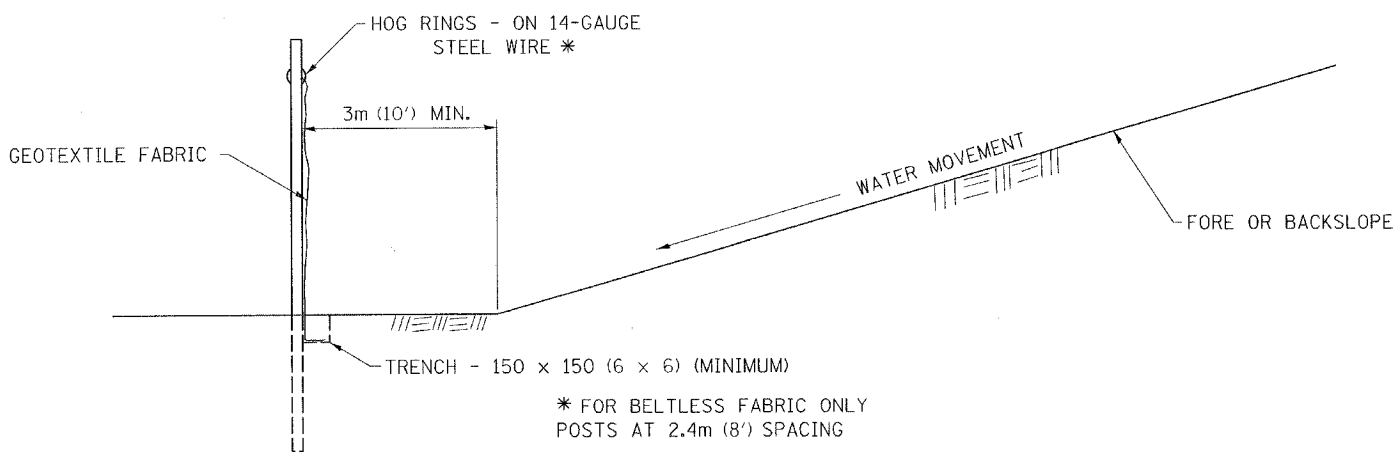
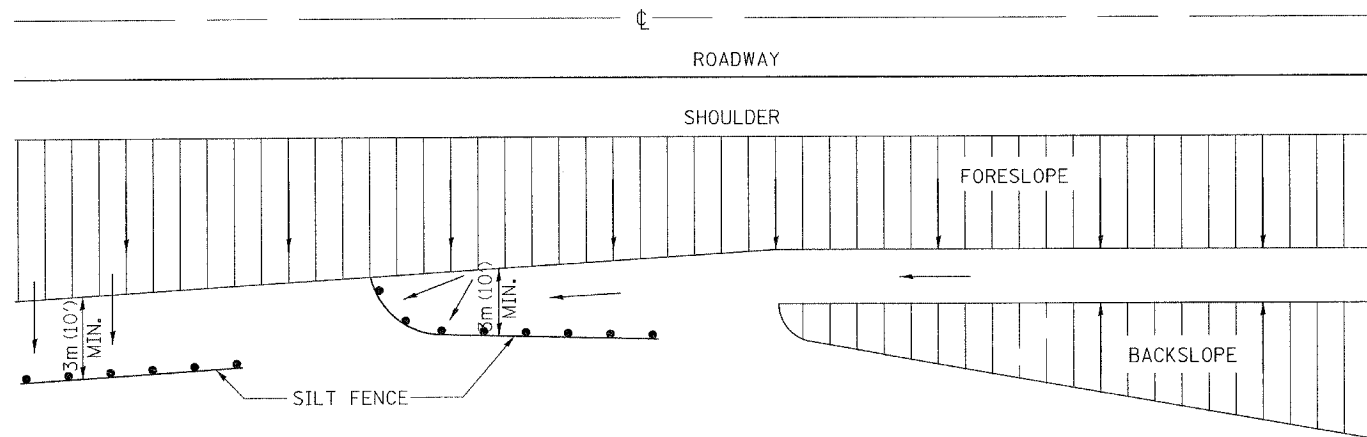
- HOLES SHALL BE CAST-IN OR DRILLED 400 (16) FROM OUTSIDE EDGE OF JOINT.
- NUTS AND WASHERS ARE NOT REQUIRED ON INSIDE OF 675 (27) DIAM. PIPE OR LESS.
- TIES ARE NOT REQUIRED FOR BELL PIPE 600 (24) AND SMALLER. ON OTHER SIZES TIE MAY BE INSERTED FROM INSIDE.
- CUT THREADS MAY BE USED IF WASHER AND NUT ARE USED.
- PIPE SIZE LISTED IS INSIDE DIAM. OF ROUND PIPE OR EQUIVALENT DIAM. OF PIPE ARCH OR ELLIPTICAL.
- GALVANIZING OF TIES IS REQUIRED.
- ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|---------------------------|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 29 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

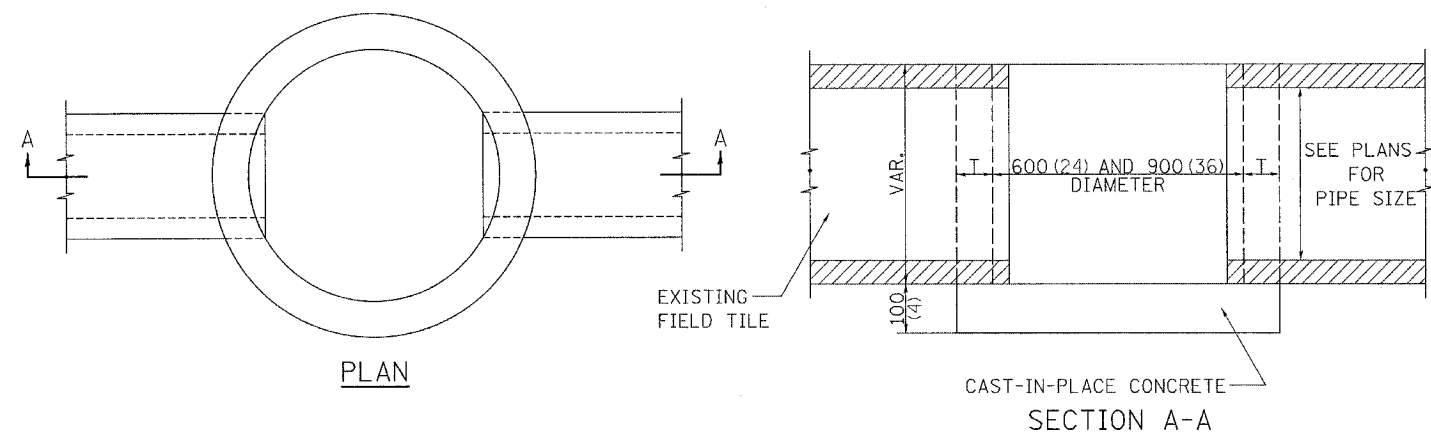
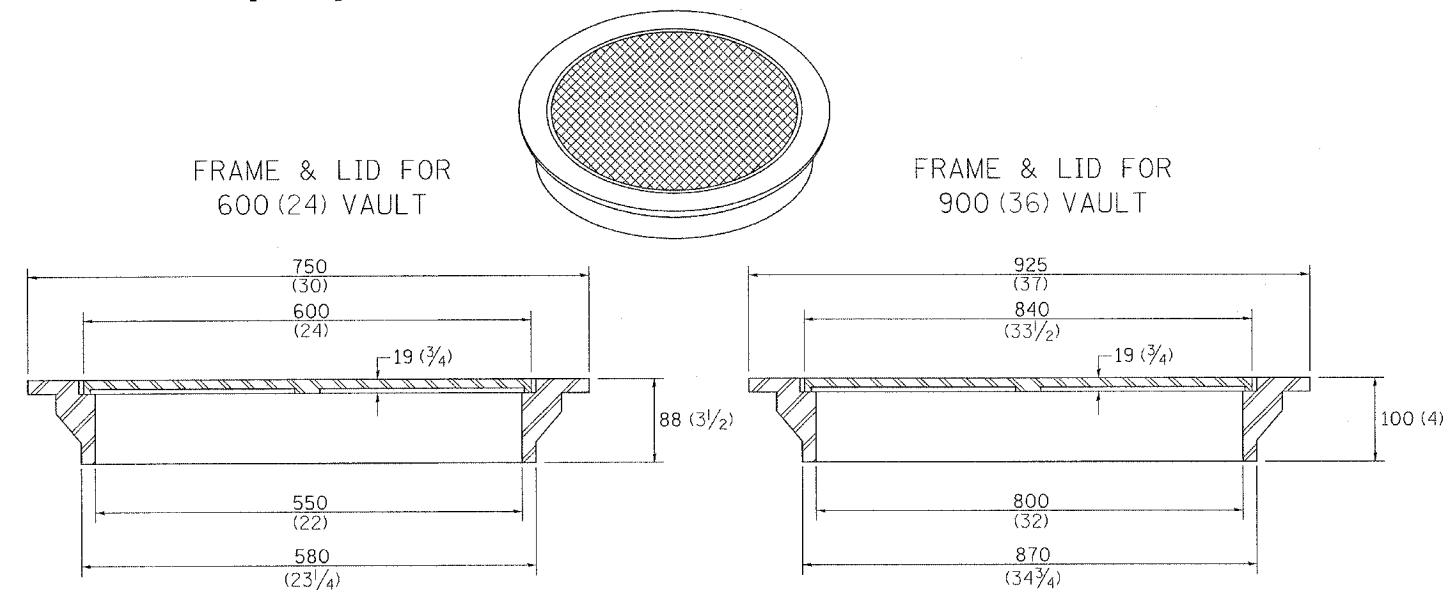
EROSION CONTROL DETAILS FOR SILT FENCE

FIELD TILE JUNCTION VAULTS 600 (24) AND 900 (36) DIA.



DETAILS OF SILT FENCE

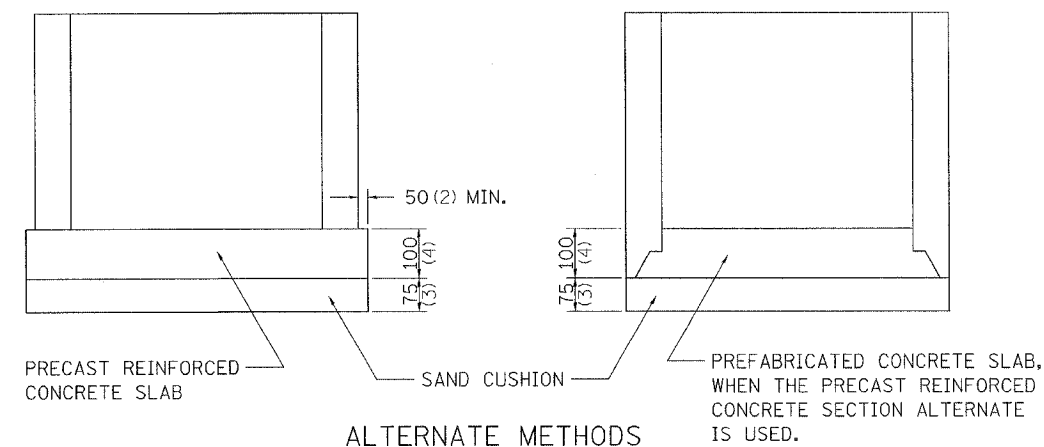
ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.



| ALTERNATE MATERIALS FOR WALLS | T |
|-------------------------------------|---------|
| BRICK MASONRY | 200 (8) |
| CAST-IN-PLACE CONCRETE | 150 (6) |
| CONCRETE MASONRY UNIT | 125 (5) |
| PRECAST REINFORCED CONCRETE SECTION | 75 (3) |

NOTE: THE FRAME AND LID IS REQUIRED ON ALL JUNCTION VAULTS.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.



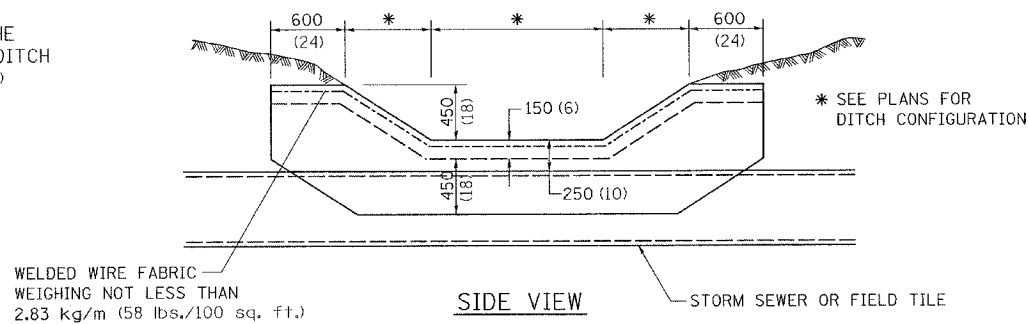
ALTERNATE METHODS

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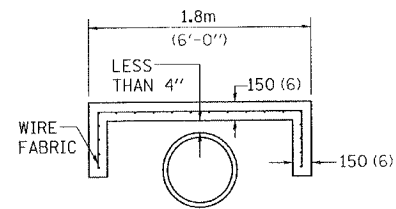
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------|---------|---|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 30 |
| STA. TO STA. | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | |

TREATMENT OF FIELD TILE SYSTEMS UNDER DITCHES

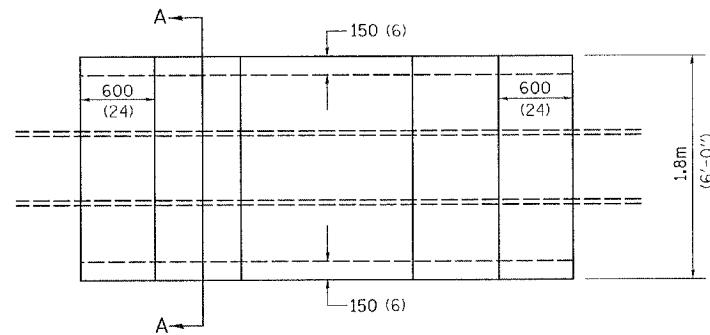
PAVED DITCH
TO BE USED IF COVER OVER THE PIPE AT THE BOTTOM OF THE DITCH IS LESS THAN 250mm (10 Inches)



WELDED WIRE FABRIC WEIGHING NOT LESS THAN 2.83 kg/m (58 lbs./100 sq. ft.)



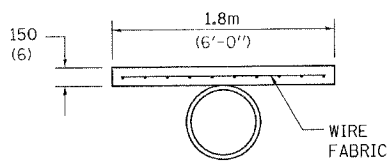
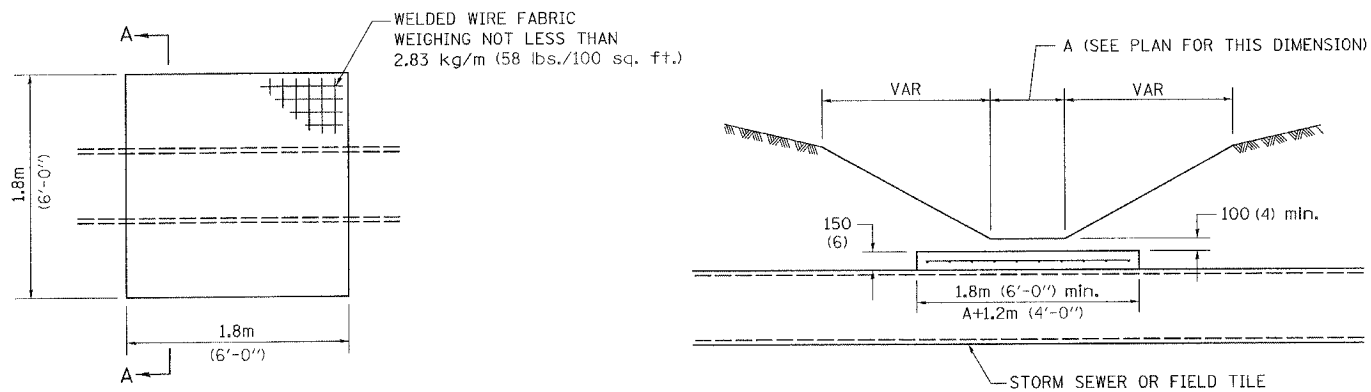
SECTION A-A



PLAN VIEW

CONCRETE SLAB

TO BE USED IF COVER OVER THE PIPE AT THE BOTTOM OF THE DITCH IS 250mm (10 Inches) TO 600mm (24 Inches)



SECTION A-A

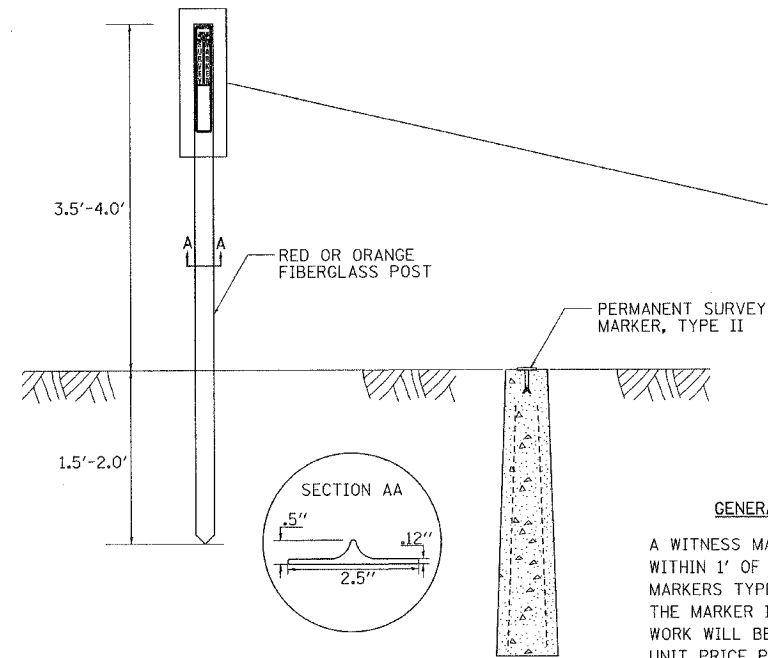
NOTES

THIS WORK SHALL BE DONE IN ACCORDANCE WITH ARTICLE 611.04 OF THE STANDARD SPECIFICATION.

THE CONCRETE SLAB AND PAVED DITCH WILL BE PAID FOR AT THE CONCRETE UNIT PRICE PER CUBIC METER (CUBIC YARD) FOR MISCELLANEOUS CONCRETE.

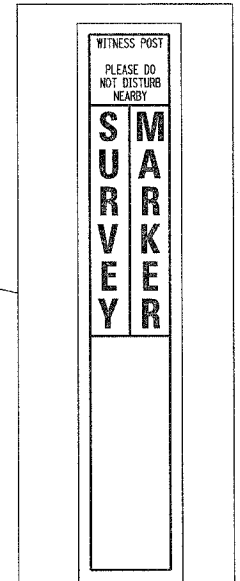
ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

WITNESS MARKER FOR PERMANENT SURVEY MARKERS, TYPE II

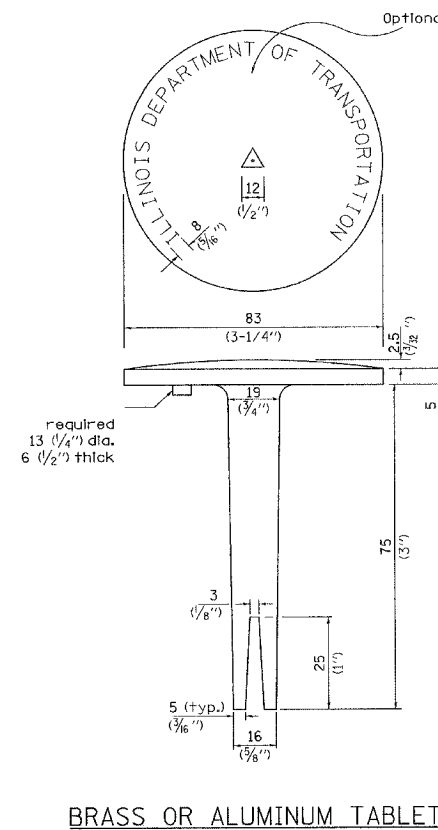


GENERAL NOTES

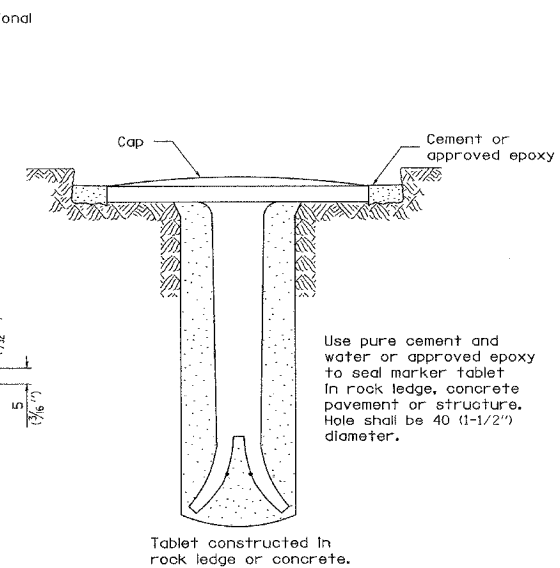
A WITNESS MARKER SHALL BE INSTALLED WITHIN 1' OF ALL PERMANENT SURVEY MARKERS TYPE II EXCEPT IN AREAS WHERE THE MARKER IS IN THE SIDEWALK. THIS WORK WILL BE INCLUDED TO THE CONTRACT UNIT PRICE PER EACH FOR PERMANENT SURVEY MARKERS, TYPE II.



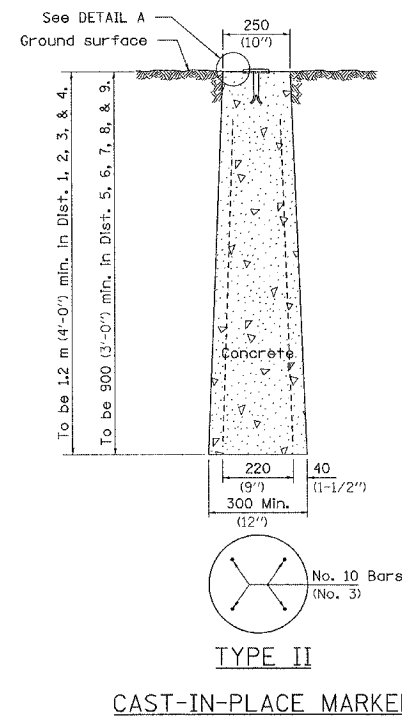
PERMANENT SURVEY MARKERS, TYPE II



BRASS OR ALUMINUM TABLET



TYPE II



TYPE II

CAST-IN-PLACE MARKER

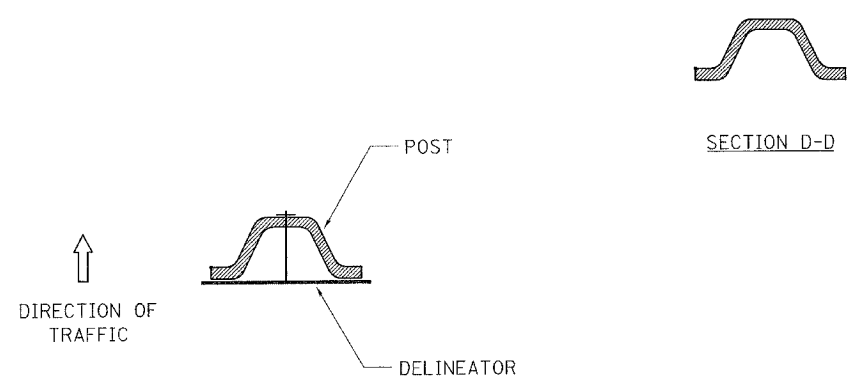
No. 10 Bars (No. 3) No. 10 (No. 3) Bars to be 750 (30") for 900 (36") min. & 1.1 m (3'-6") for 1.2 m (4'-0") min.

DETAIL A

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

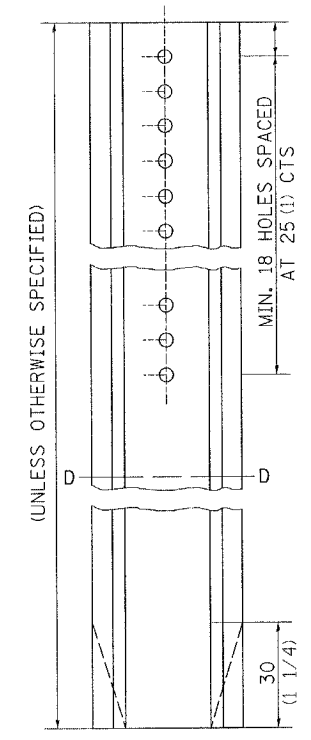
| | | | | |
|---------------------|---------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I7 | 16-T | CARROLL | 47 | 31 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

DELINEATOR AND POST ORIENTATION



DELINEATORS SHALL BE INSTALLED ACCORDING TO STANDARD 635001 EXCEPT THAT THE POST SHALL BE ROTATED 180°. THE POST WILL HAVE THE WIDE SIDE FACING TRAFFIC AND THE DELINEATOR ATTACHED AS SHOWN ABOVE.

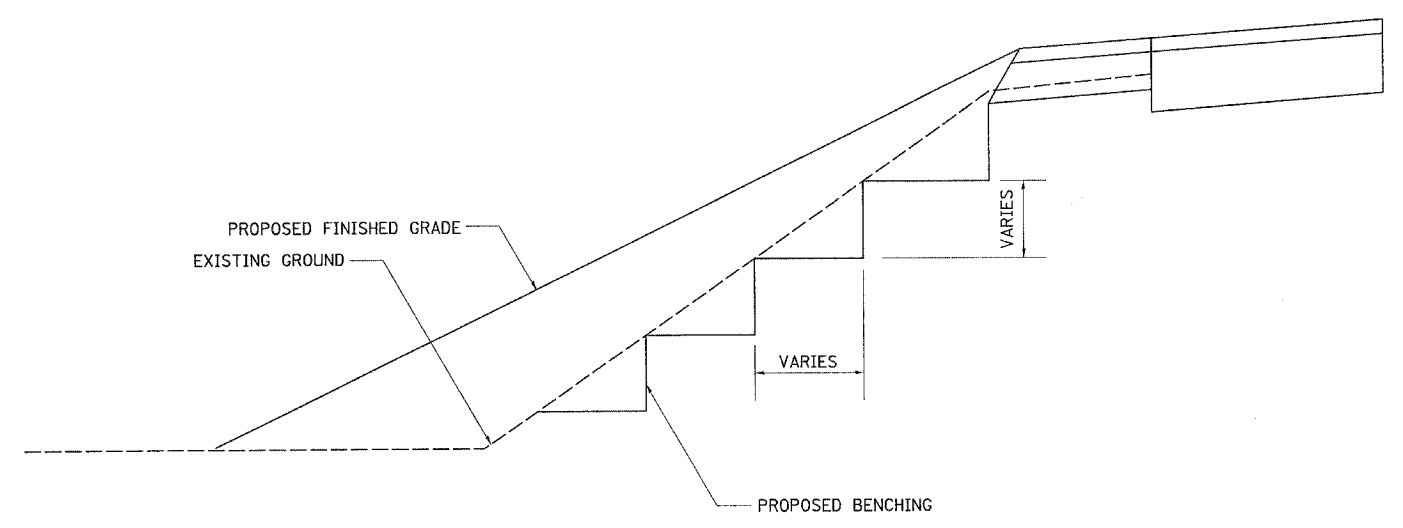
ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.



DELINEATOR AND POST ORIENTATION 37.4

REVISED 1-31-00

TYPICAL BENCHING ON EXISTING EMBANKMENT

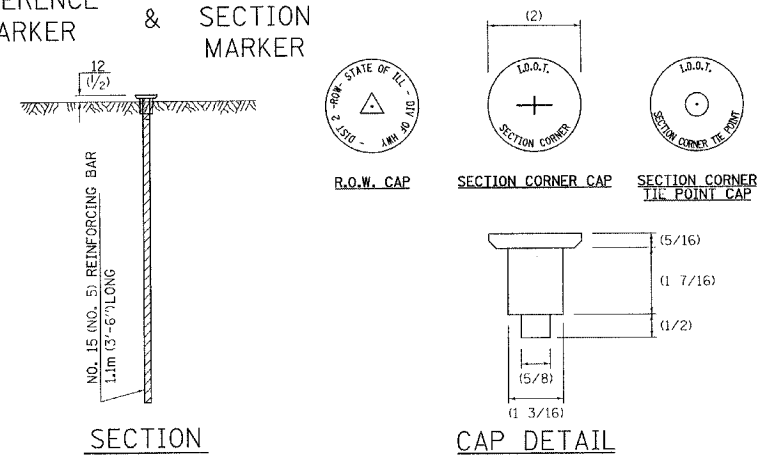


TYPICAL BENCHING ON EXISTING EMBANKMENT 50.4

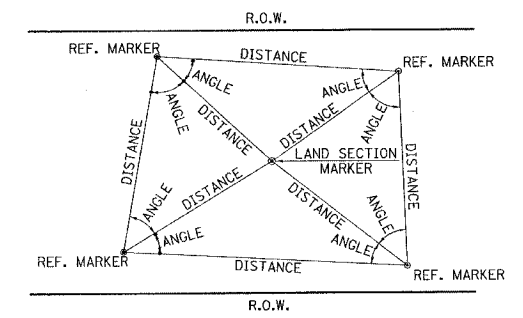
REVISED 2-22-06

LAND SECTION & REFERENCE MARKERS

REFERENCE MARKER & LAND SECTION MARKER



METHOD OF REFERENCING MARKERS



- USE INSTRUMENT TIES TO NEARBY LAND-MARKS (STEEPLES, TOWERS, SILOS, ETC...)
- IN CULTIVATED FIELDS, SET 600(2') OR MORE BELOW GROUND SURFACE.
- IN FENCE LINE OR PROTECTED AREA SET TOP AT GROUND LEVEL.

METHOD OF REFERENCING POINTS
 REFERENCE MARKERS SHALL BE USED TO TIE IN PERMANENT LAND SECTION AND 1/4 SECTION CORNERS. WHERE LAND SECTION MARKERS FALL IN THE SHOULDERS OR GRAVEL SURFACES, THE TOP OF THE BAR SHALL BE KEPT 75(3) BELOW THE SURFACE. LAND SECTION MARKERS LOCATED IN TRAFFIC LANES SHALL NOT BE REPLACED.

METAL CAPS SHALL BE PLACED ON TOP OF THE REINFORCEMENT BAR. THERE ARE 3 TYPES OF CAPS, ONE FOR THE RIGHT-OF-WAY CORNERS, ONE FOR THE SECTION CORNERS AND ONE FOR THE SECTION CORNER TIE POINTS. THE CAPS WILL BE SUPPLIED BY IDOT. CALL CHIP CORDELL (815) 284-5370 A MINIMUM OF ONE WEEK BEFORE THE CAPS ARE NEEDED

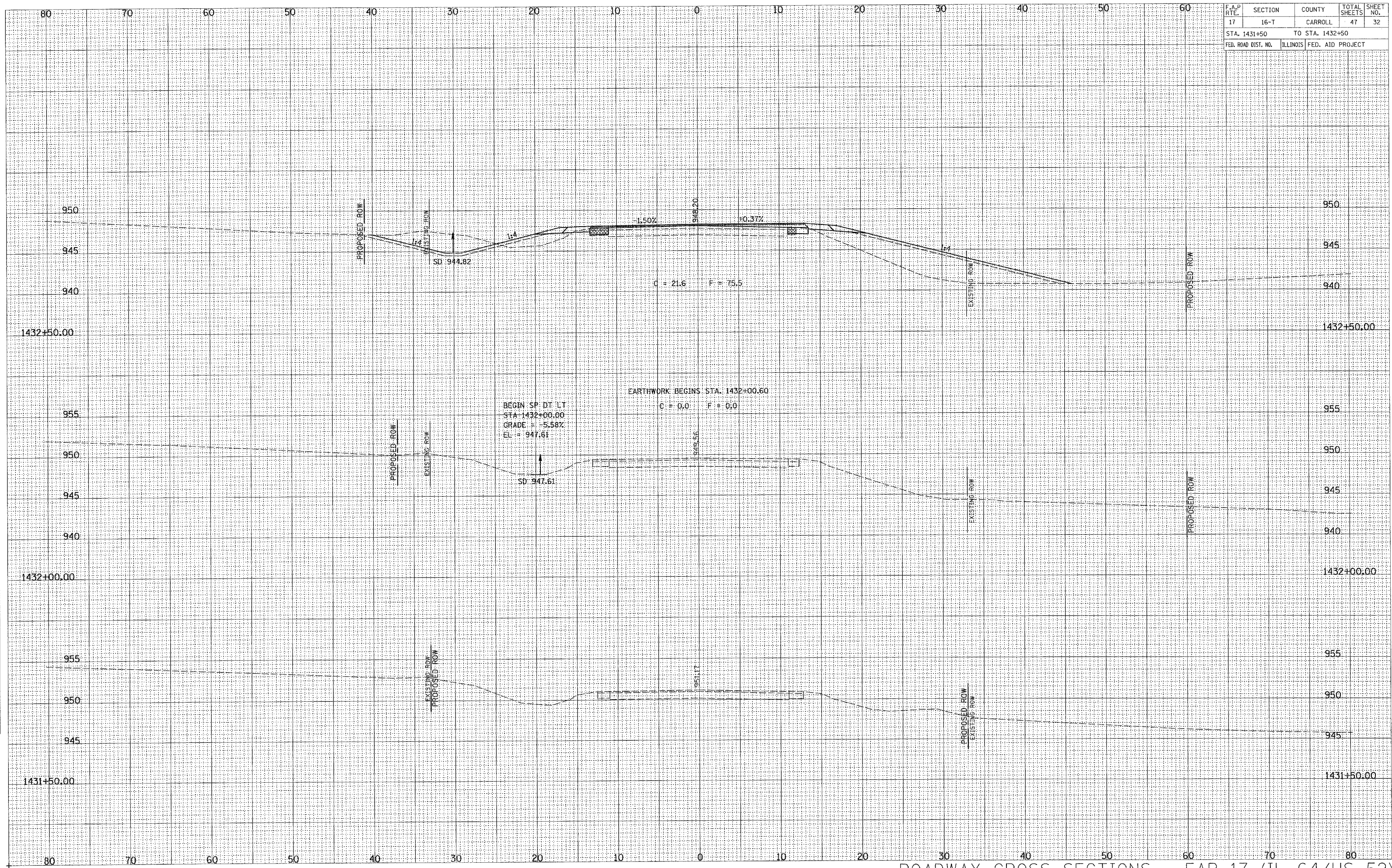
ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

LAND SECTION & REFERENCE MARKERS 63.4

REVISED 4-22-05

PLOT DATE = 08/05/2005
 PLOT BY = JAC
 PLOT SCALE = 1/8" = 1'-0"
 REFERENCE = #REF#

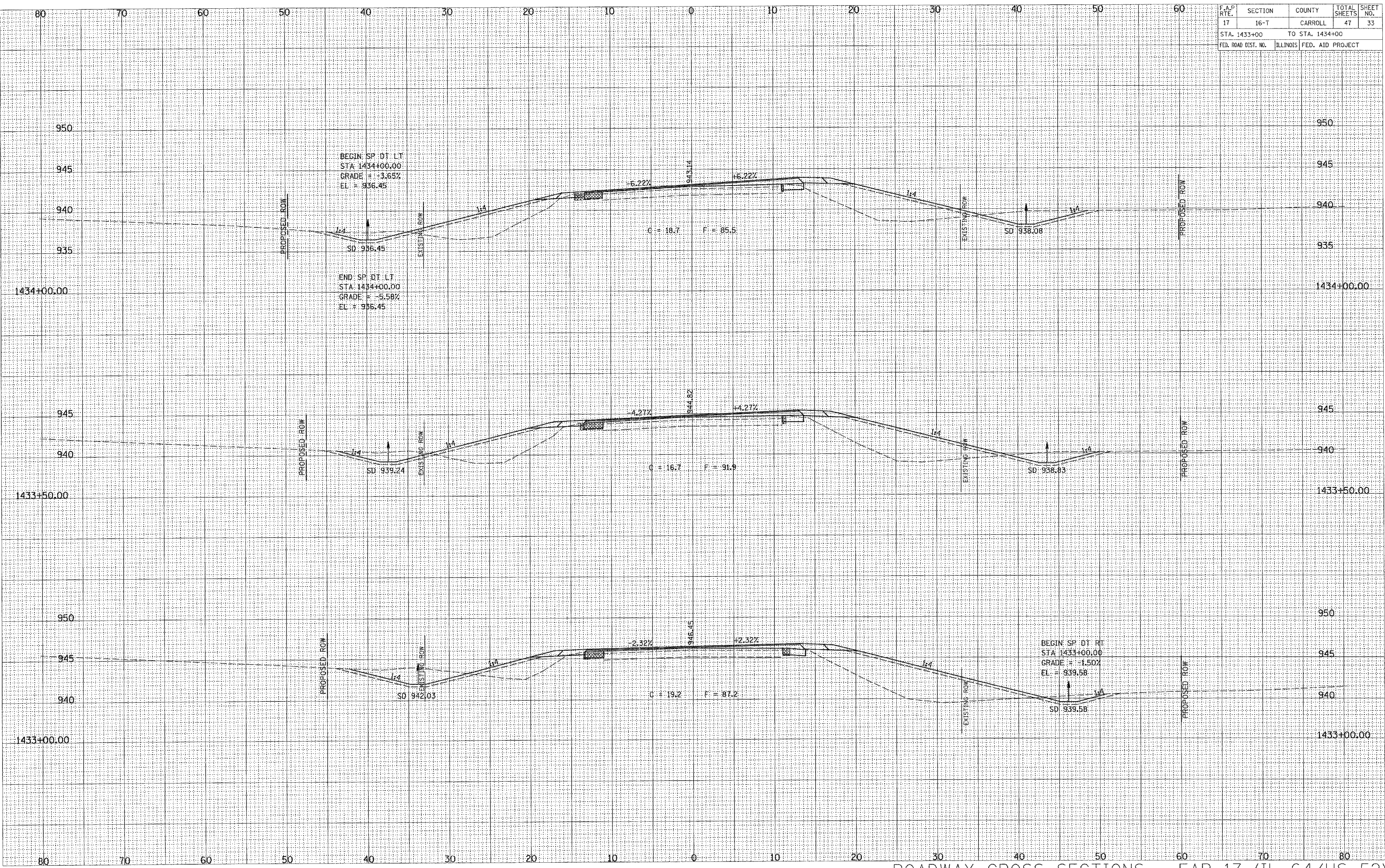
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 32 |
| STA. 1431+50 | | TO STA. 1432+50 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



SURVEYED
 SURVEY
 NOTE BOOK
 TEMPLATE
 AREAS CHECKED
 NO.

SURVEYED
 SURVEY
 NOTE BOOK
 TEMPLATE
 AREAS CHECKED
 NO.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 33 |
| STA. 1433+00 | | TO STA. 1434+00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

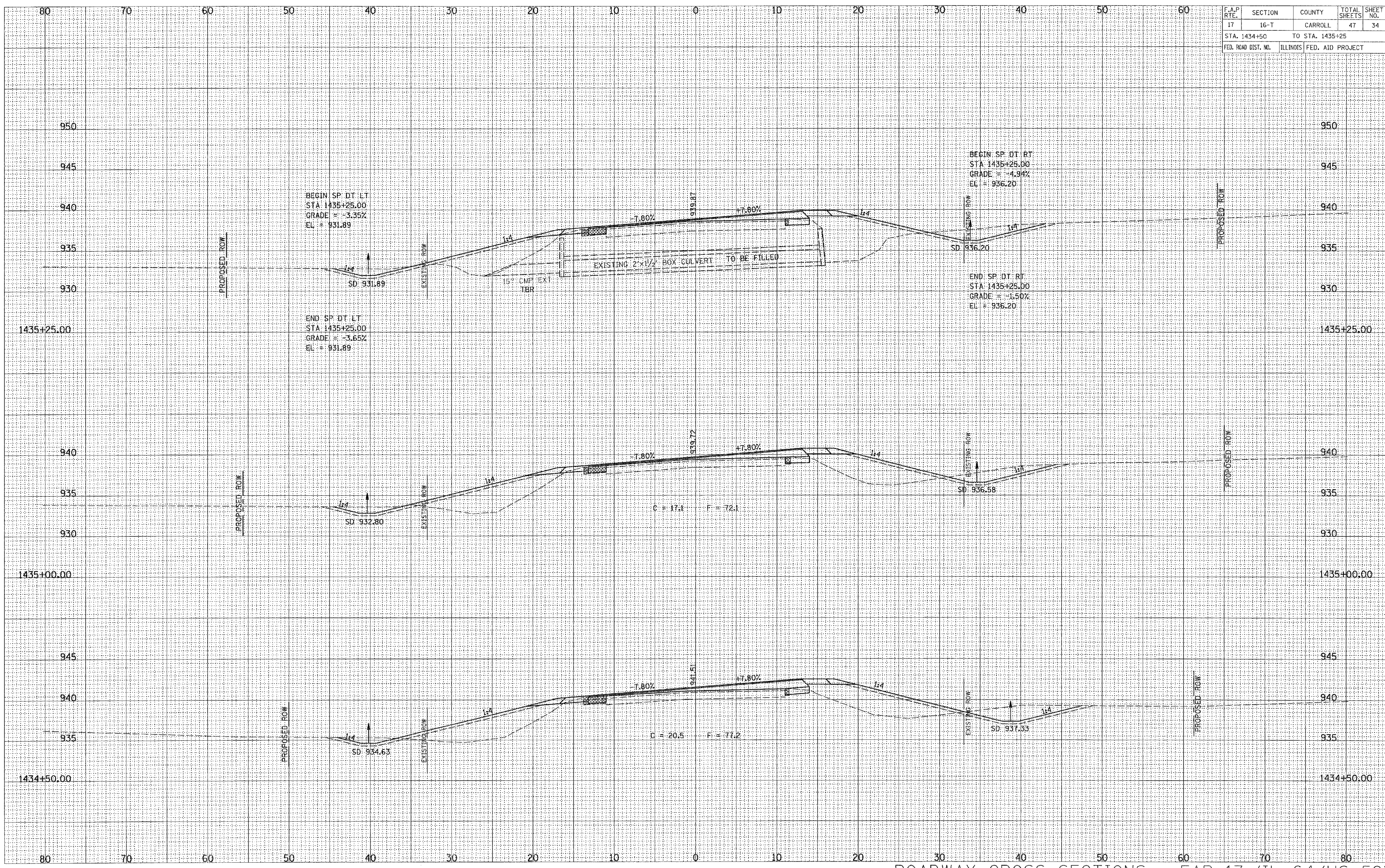


SURVEYED
 PLOTTED
 NOTE BOOK
 NO.

SURVEYED
 PLOTTED
 NOTE BOOK
 NO.

ROADWAY CROSS SECTIONS - FAP 17 (IL 64/US 52)

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|---------------------------|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 34 |
| STA. 1434+50 | | TO STA. 1435+25 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

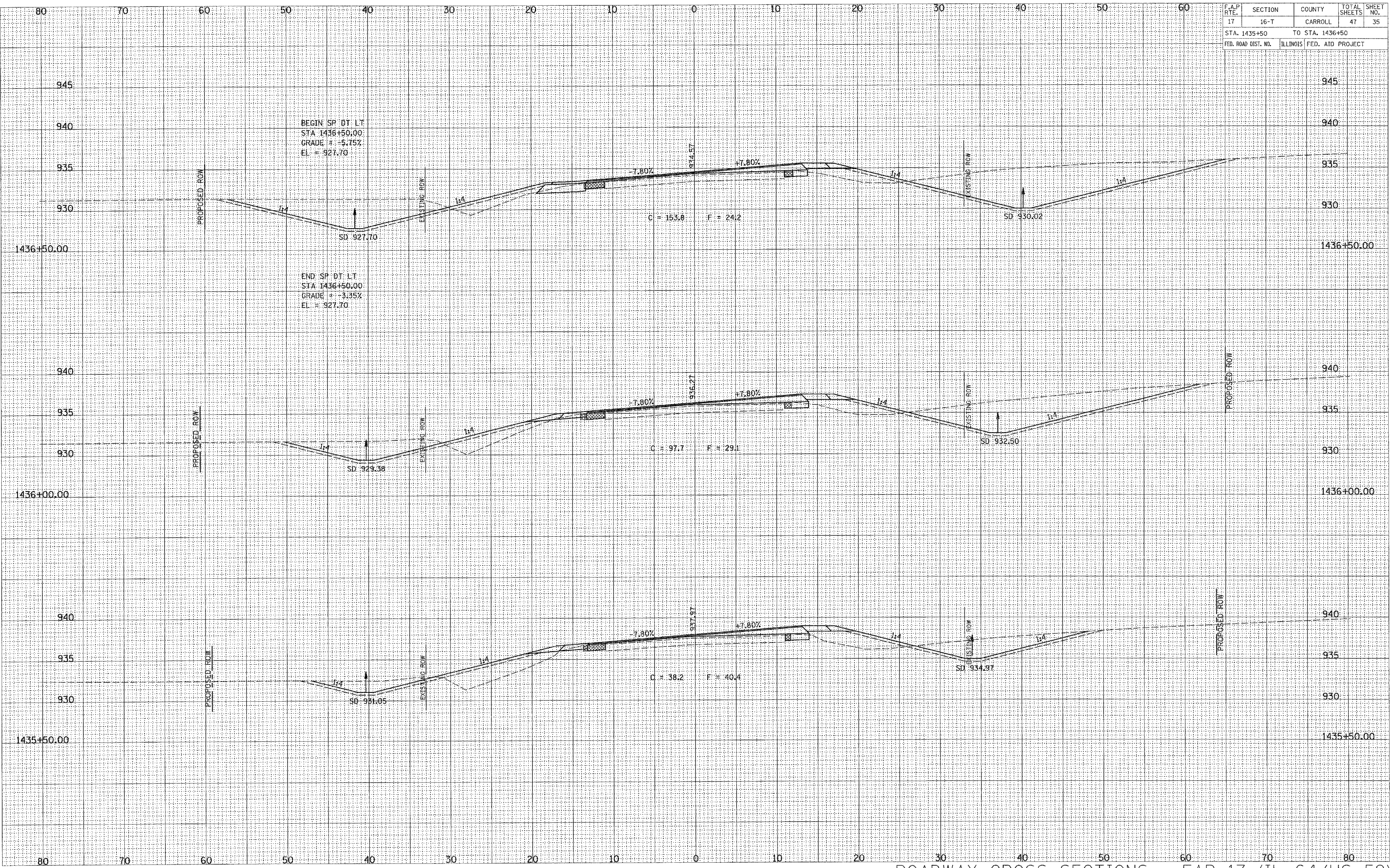


ROADWAY CROSS SECTIONS - FAP 17 (IL 64/US 52)

ORIGINAL SURVEYED PLOTTED TEMPLATE NO. 17-16-T-34

ORIGINAL SURVEYED PLOTTED TEMPLATE NO. 17-16-T-34

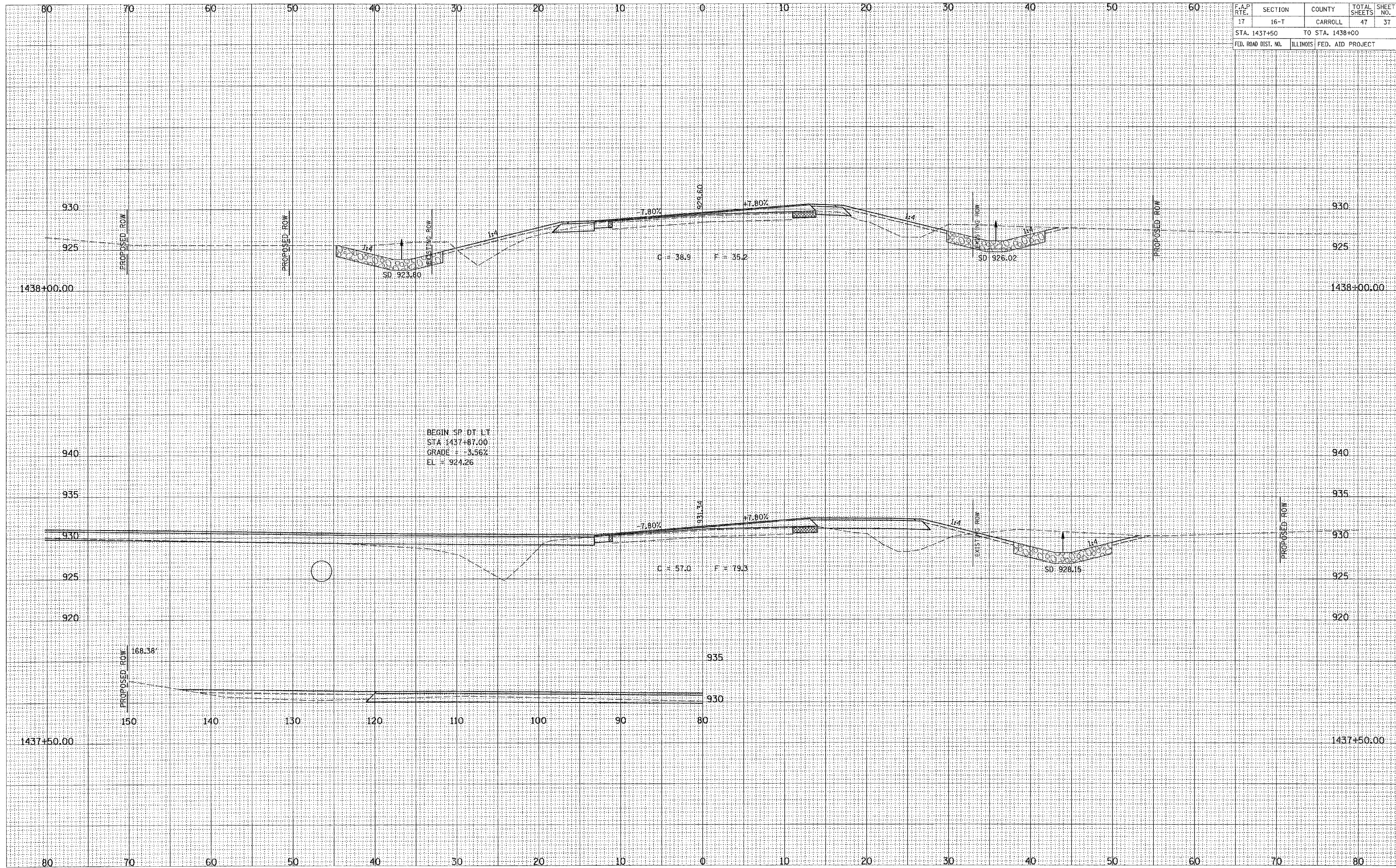
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 35 |
| STA. 1435+50 | | TO STA. 1436+50 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



SURVEYED
 PLOTTED
 NOTE BOOK
 NO.

SURVEYED
 PLOTTED
 NOTE BOOK
 NO.

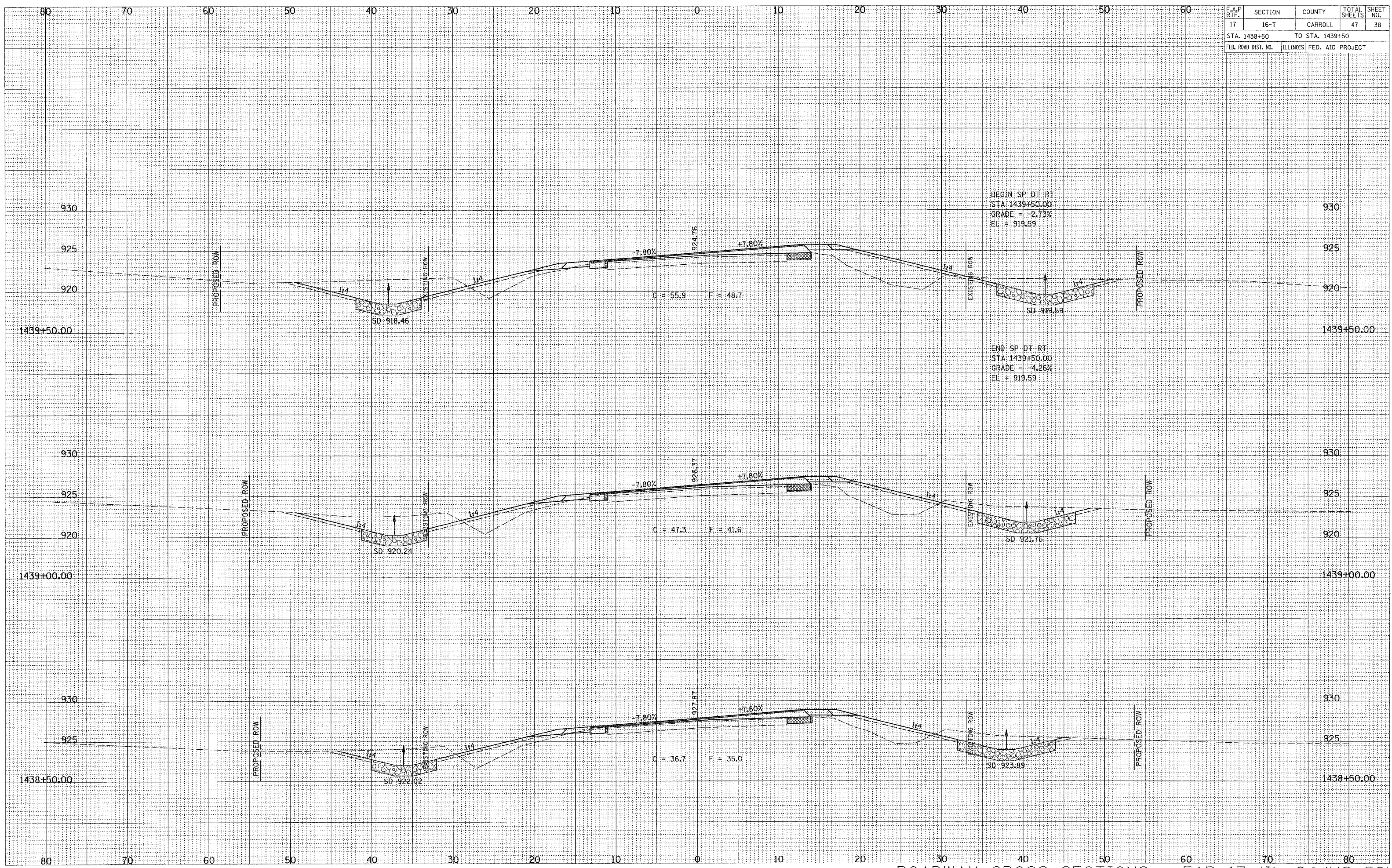
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|---------------------------|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 37 |
| STA. 1437+50 | | TO STA. 1438+00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 38 |
| STA. 1438+50 | | TO STA. 1439+50 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

SURVEYED
 PLOTTED
 TEMPLATE
 AREAS
 CHECKED
 NO.

ORIGINAL SURVEY
 PLOTTED
 TEMPLATE
 AREAS
 CHECKED
 NO.

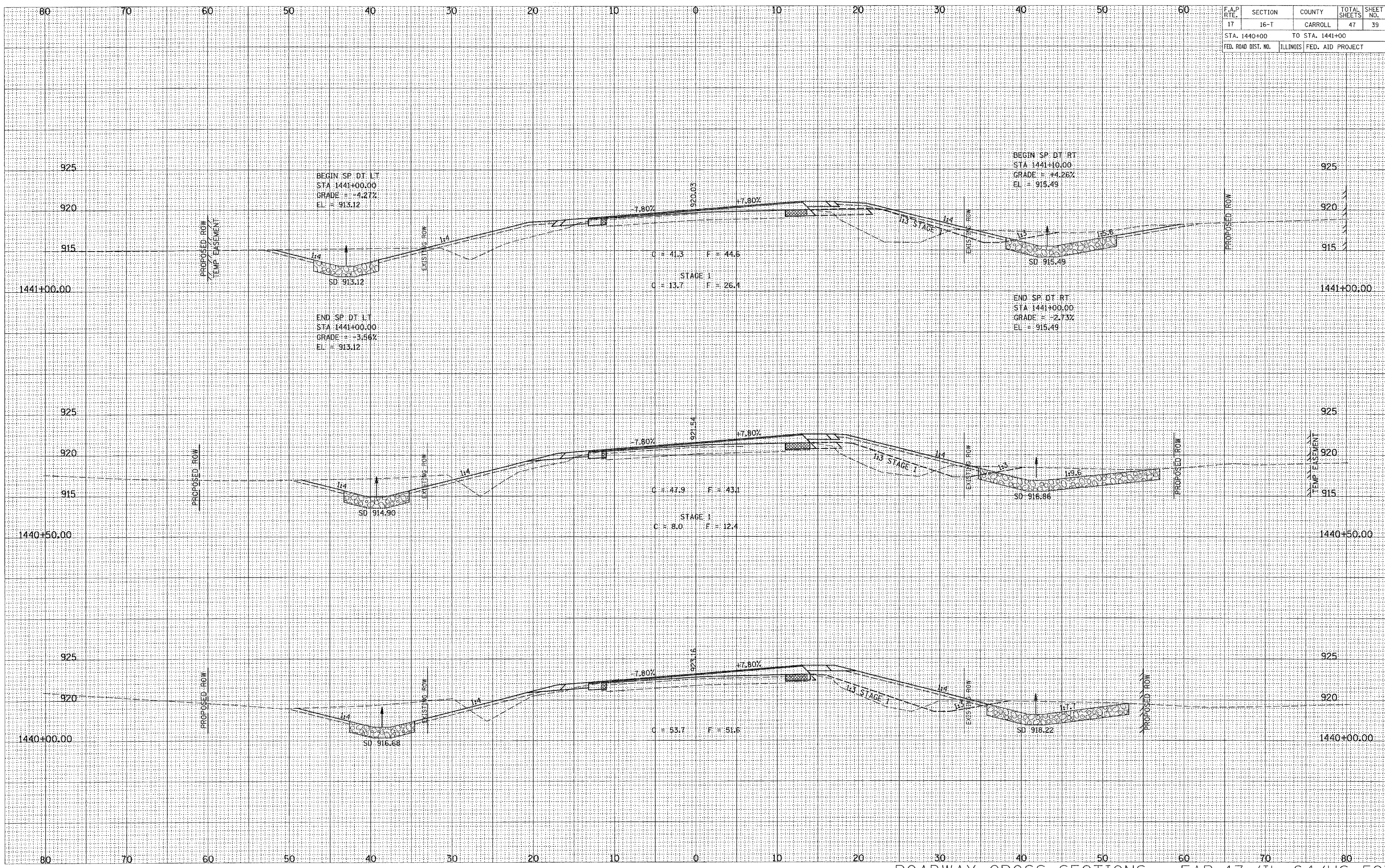


BEGIN SP. DT. RT
 STA. 1439+50.00
 GRADE = -2.73%
 EL. = 919.59

END SP. DT. RT
 STA. 1439+50.00
 GRADE = -4.26%
 EL. = 919.59

ROADWAY CROSS SECTIONS - FAP 17 (IL 64/US 52)

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|-----------------|------------------|-----------|
| 17 | 16-T | CARROLL | 47 | 39 |
| STA. 1440+00 | | TO STA. 1441+00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | | FED. AID PROJECT | |

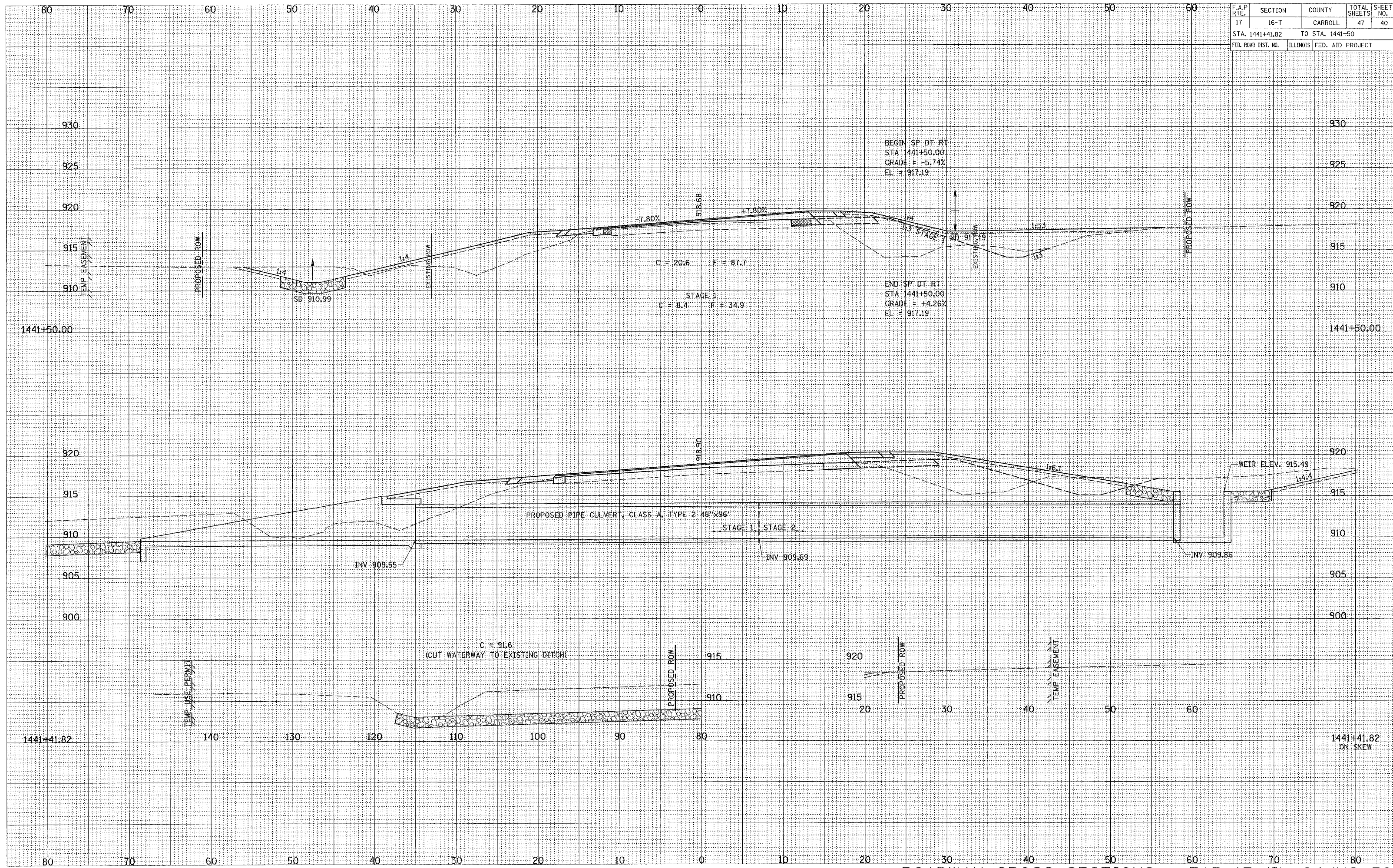


ROADWAY CROSS SECTIONS - FAP 17 (IL 64/US 52)

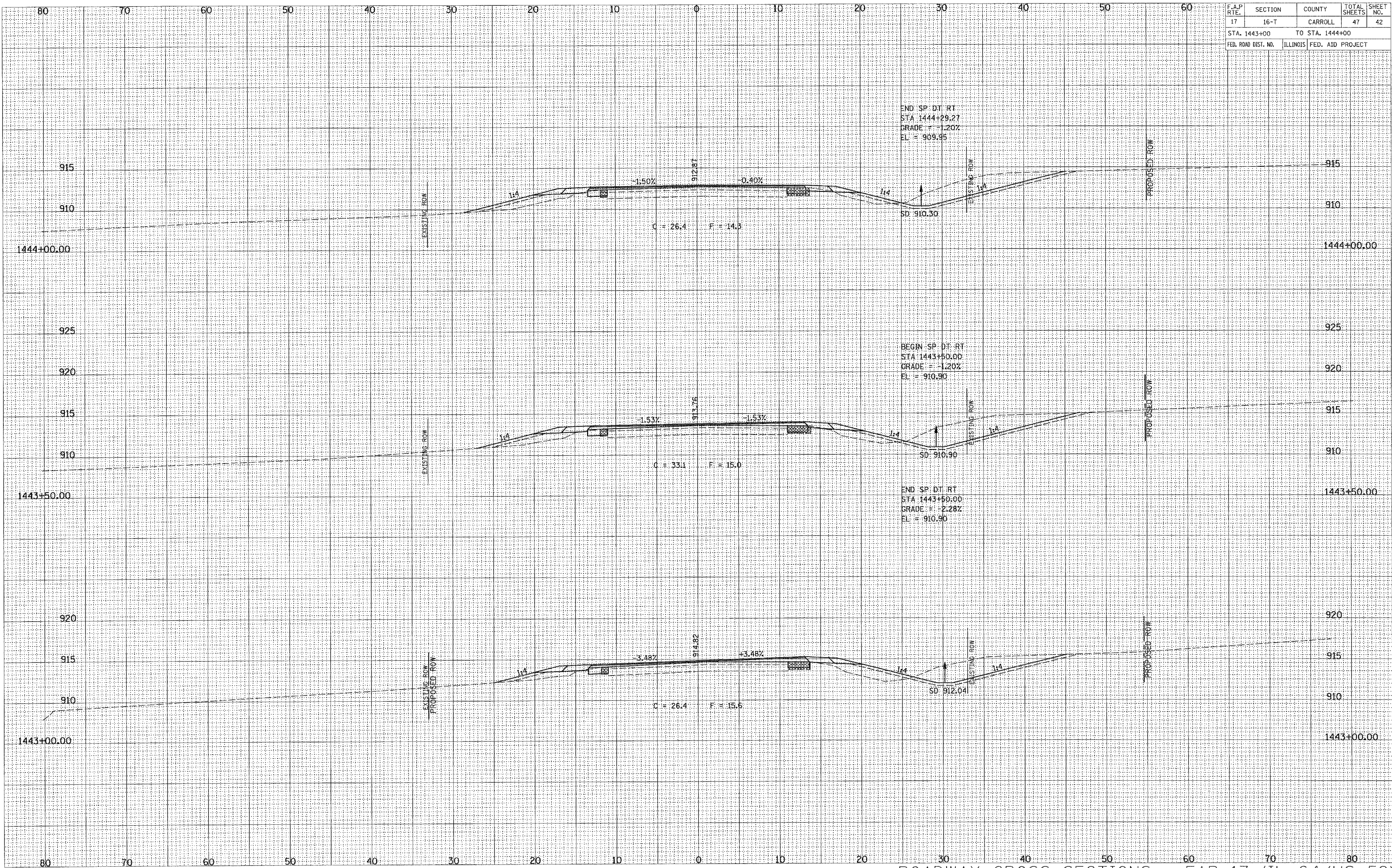
FINAL SURVEY PLOTTED TEMPLATE AREAS CHECKED

ORIGINAL SURVEY PLOTTED TEMPLATE AREAS CHECKED

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------------|---------|---------------------------|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 40 |
| STA. 1441+41.82 TO STA. 1441+50 | | | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



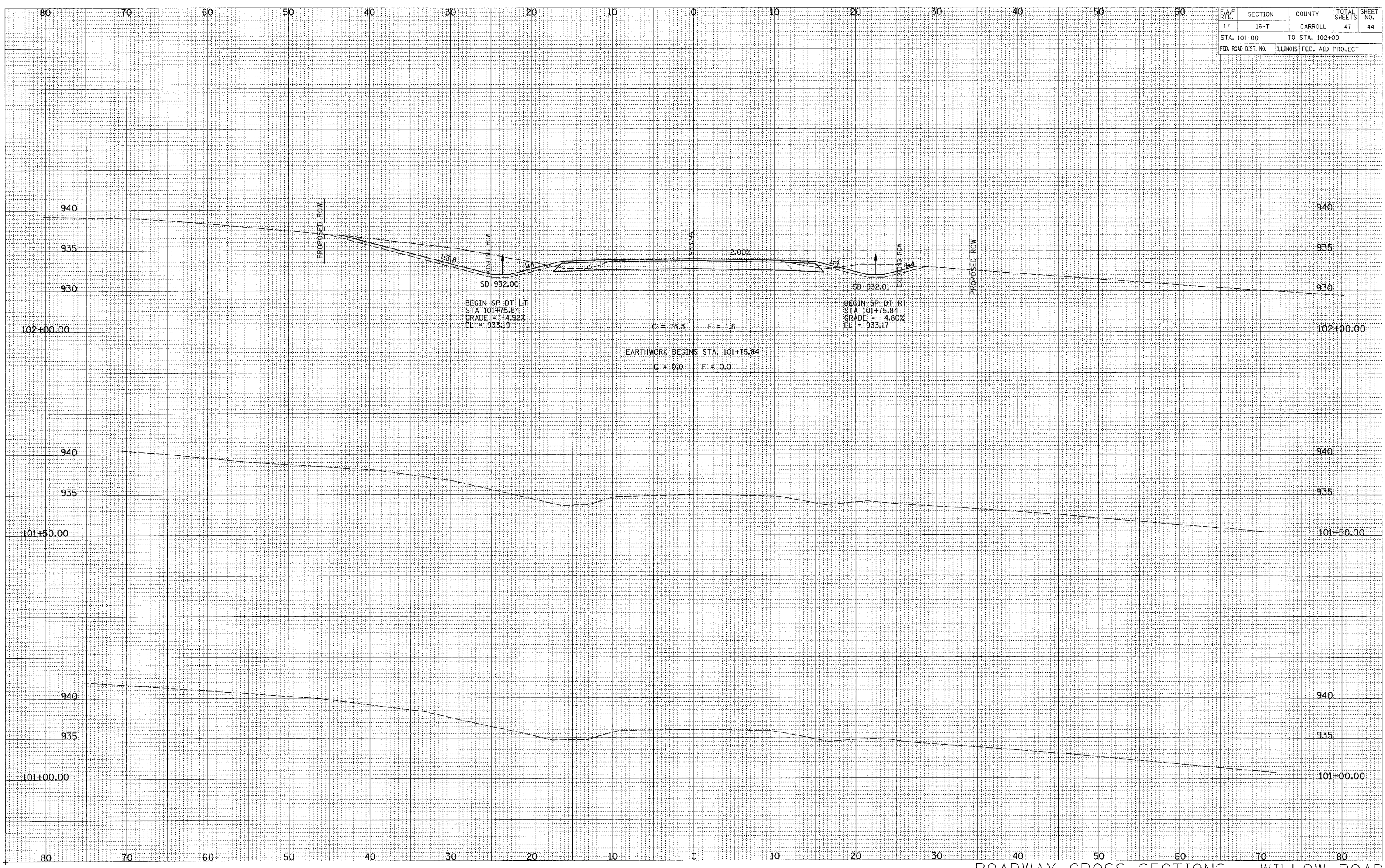
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 42 |
| STA. 1443+00 | | TO STA. 1444+00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



SURVEYED
 PLOTTED
 NOTE BOOK
 TEMPLATE
 AREAS CHECKED
 NO.

SURVEYED
 PLOTTED
 NOTE BOOK
 TEMPLATE
 AREAS CHECKED
 NO.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 17 | 16-T | CARROLL | 47 | 44 |
| STA. 101+00 | | TO STA. 102+00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



SURVEYED
 PLOTTED
 TEMPLATE
 AREAS
 CHECKED
 NO.

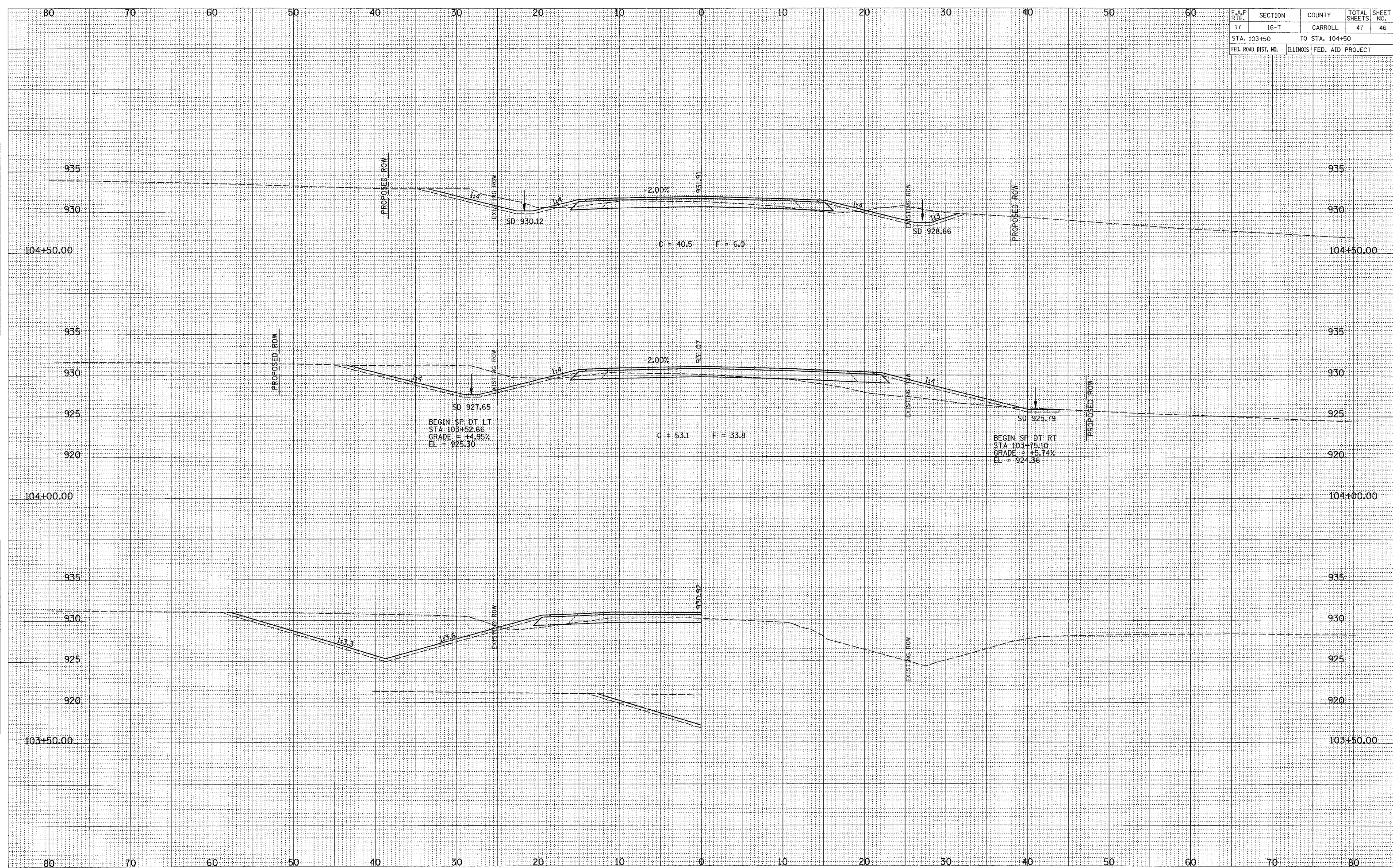
ORIGINAL
 SURVEY
 NO. BOOK
 AREAS
 CHECKED
 NO.

ROADWAY CROSS SECTIONS - WILLOW ROAD

| | | | | |
|---------------------|----------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 17 | 16-T | CARROLL | 47 | 46 |
| STA. 103+50 | | TO STA. 104+50 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

DATE _____
 BY _____
 SURVEYED _____
 ADJUSTED _____
 TEMPLATE _____
 AREAS _____
 AREAS CHECKED _____
 NO. _____

DATE _____
 BY _____
 SURVEYED _____
 ADJUSTED _____
 TEMPLATE _____
 AREAS _____
 AREAS CHECKED _____
 NO. _____

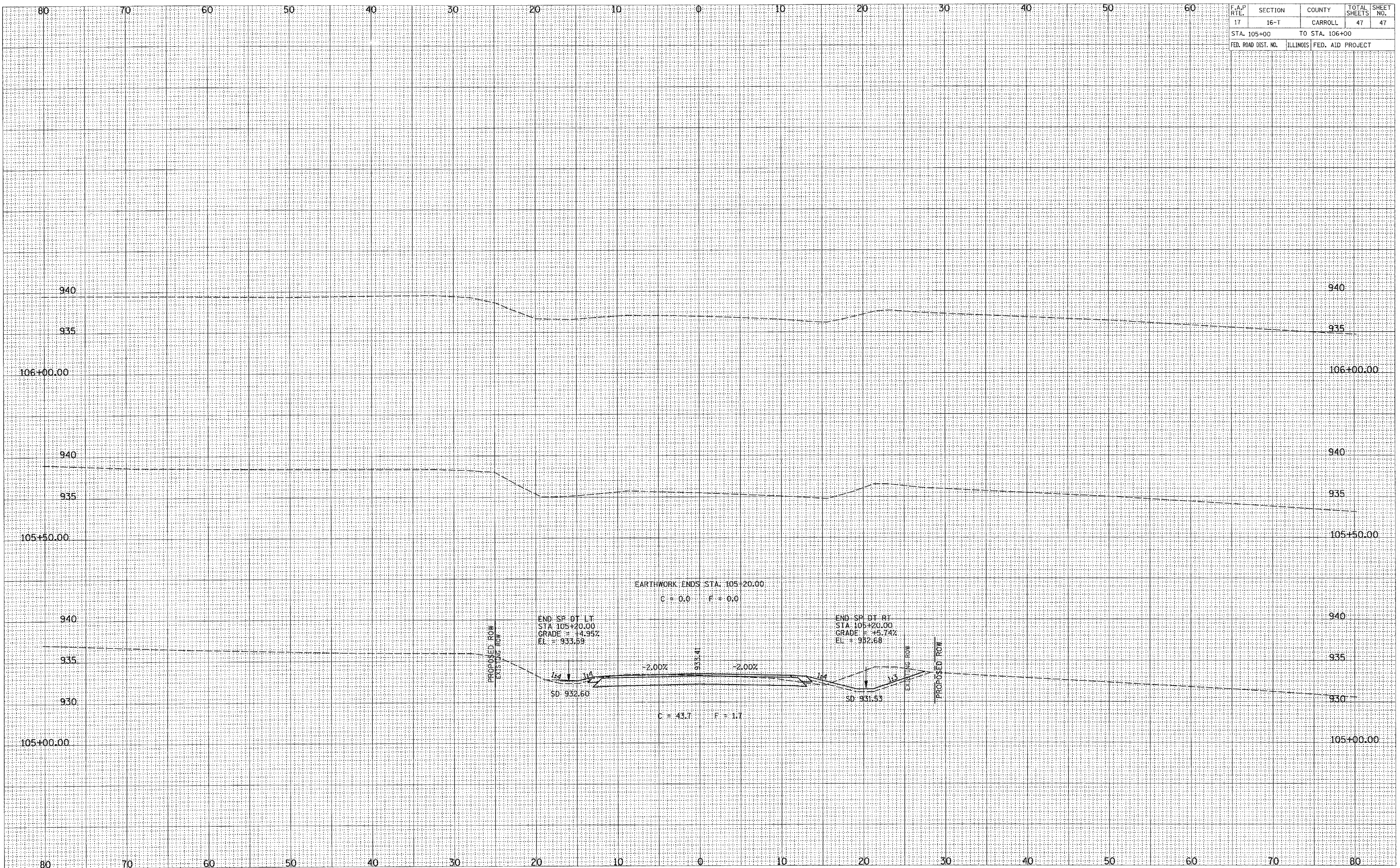


ROADWAY CROSS SECTIONS - WILLOW ROAD

| | | | | |
|---------------------|----------|------------------|--------------|-----------|
| F.A.P. R.T.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 17 | 16-T | CARROLL | 47 | 47 |
| STA. 105+00 | | TO STA. 106+00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

| | | | |
|----------|---------|---------|---------------|
| SURVEYED | PLOTTED | EMPLATE | AREAS CHECKED |
| | | | |
| NO. | | | |

| | | | |
|-----------------|---------|---------|---------------|
| ORIGINAL SURVEY | PLOTTED | EMPLATE | AREAS CHECKED |
| | | | |
| NO. | | | |



ROADWAY CROSS SECTIONS - WILLOW ROAD