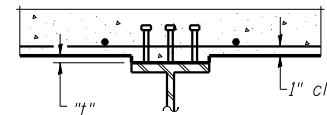
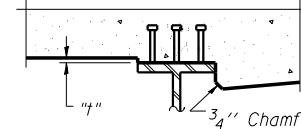


DEAD LOAD DEFLECTION DIAGRAM
(Includes weight of concrete only.)

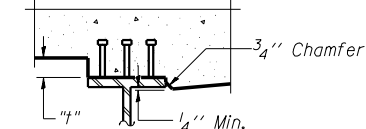
Note:
The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on S-3 and S-4.



At Interior Beam



At Minimum Fillet



At Maximum Fillet

To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection and Grinding" shown on S-3 and S-4, minus the 7/8" deck thickness, equals the fillet heights "t" above top flange of beams. The slab is to be ground after curing to achieve smoothness, but the slab is not to be ground to elevations below the "Theoretical Grade Elevations" shown on S-3 and S-4. For grinding the deck, see Special Provisions

FILLET HEIGHTS

BEAM 3

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
Back of S. Abut.	745+57.500	-3.333	723.056	723.077
☉ Brg. S. Abut	745+58.750	-3.333	723.065	723.086
A	745+68.750	-3.333	723.136	723.175
B	745+78.750	-3.333	723.201	723.248
C	745+88.750	-3.333	723.259	723.304
D	745+98.750	-3.333	723.310	723.343
E	746+08.750	-3.333	723.355	723.374
☉ Brg. Pier 1	746+20.000	-3.333	723.397	723.418
F	746+30.000	-3.333	723.428	723.481
G	746+40.000	-3.333	723.452	723.549
H	746+50.000	-3.333	723.469	723.604
I	746+60.000	-3.333	723.480	723.637
J	746+70.000	-3.333	723.485	723.642
K	746+80.000	-3.333	723.483	723.618
L	746+90.000	-3.333	723.474	723.571
M	747+00.000	-3.333	723.459	723.512
☉ Brg. Pier 2	747+10.000	-3.333	723.437	723.458
N	747+20.000	-3.333	723.408	723.427
O	747+30.000	-3.333	723.373	723.404
P	747+40.000	-3.333	723.332	723.376
Q	747+50.000	-3.333	723.284	723.331
R	747+60.000	-3.333	723.229	723.269
☉ Brg. N. Abut.	747+71.250	-3.333	723.160	723.180
Back of N. Abut.	747+72.500	-3.333	723.151	723.172

☉ ROADWAY & P.G.

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
Back of S. Abut.	745+57.500	0.000	723.108	723.129
☉ Brg. S. Abut	745+58.750	0.000	723.117	723.138
A	745+68.750	0.000	723.188	723.227
B	745+78.750	0.000	723.253	723.300
C	745+88.750	0.000	723.311	723.356
D	745+98.750	0.000	723.362	723.395
E	746+08.750	0.000	723.407	723.426
☉ Brg. Pier 1	746+20.000	0.000	723.449	723.470
F	746+30.000	0.000	723.480	723.533
G	746+40.000	0.000	723.504	723.601
H	746+50.000	0.000	723.521	723.657
I	746+60.000	0.000	723.532	723.689
J	746+70.000	0.000	723.537	723.694
K	746+80.000	0.000	723.535	723.670
L	746+90.000	0.000	723.526	723.623
M	747+00.000	0.000	723.511	723.564
☉ Brg. Pier 2	747+10.000	0.000	723.489	723.510
N	747+20.000	0.000	723.460	723.479
O	747+30.000	0.000	723.425	723.456
P	747+40.000	0.000	723.384	723.428
Q	747+50.000	0.000	723.336	723.384
R	747+60.000	0.000	723.281	723.321
☉ Brg. N. Abut.	747+71.250	0.000	723.212	723.232
Back of N. Abut.	747+72.500	0.000	723.203	723.224

BEAM 4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
Back of S. Abut.	745+57.500	3.333	723.056	723.077
☉ Brg. S. Abut	745+58.750	3.333	723.065	723.086
A	745+68.750	3.333	723.136	723.175
B	745+78.750	3.333	723.201	723.248
C	745+88.750	3.333	723.259	723.304
D	745+98.750	3.333	723.310	723.343
E	746+08.750	3.333	723.355	723.374
☉ Brg. Pier 1	746+20.000	3.333	723.397	723.418
F	746+30.000	3.333	723.428	723.481
G	746+40.000	3.333	723.452	723.549
H	746+50.000	3.333	723.469	723.604
I	746+60.000	3.333	723.480	723.637
J	746+70.000	3.333	723.485	723.642
K	746+80.000	3.333	723.483	723.618
L	746+90.000	3.333	723.474	723.571
M	747+00.000	3.333	723.459	723.512
☉ Brg. Pier 2	747+10.000	3.333	723.437	723.458
N	747+20.000	3.333	723.408	723.427
O	747+30.000	3.333	723.373	723.404
P	747+40.000	3.333	723.332	723.376
Q	747+50.000	3.333	723.284	723.331
R	747+60.000	3.333	723.229	723.269
☉ Brg. N. Abut.	747+71.250	3.333	723.160	723.180
Back of N. Abut.	747+72.500	3.333	723.151	723.172

BEAM 5

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
Back of S. Abut.	745+57.500	10.000	722.952	722.973
☉ Brg. S. Abut	745+58.750	10.000	722.961	722.982
A	745+68.750	10.000	723.032	723.071
B	745+78.750	10.000	723.097	723.144
C	745+88.750	10.000	723.154	723.200
D	745+98.750	10.000	723.206	723.239
E	746+08.750	10.000	723.250	723.270
☉ Brg. Pier 1	746+20.000	10.000	723.293	723.314
F	746+30.000	10.000	723.323	723.376
G	746+40.000	10.000	723.348	723.444
H	746+50.000	10.000	723.365	723.500
I	746+60.000	10.000	723.376	723.533
J	746+70.000	10.000	723.381	723.538
K	746+80.000	10.000	723.378	723.514
L	746+90.000	10.000	723.370	723.467
M	747+00.000	10.000	723.354	723.407
☉ Brg. Pier 2	747+10.000	10.000	723.333	723.353
N	747+20.000	10.000	723.304	723.323
O	747+30.000	10.000	723.269	723.300
P	747+40.000	10.000	723.228	723.272
Q	747+50.000	10.000	723.179	723.227
R	747+60.000	10.000	723.125	723.165
☉ Brg. N. Abut.	747+71.250	10.000	723.055	723.076
Back of N. Abut.	747+72.500	10.000	723.047	723.068

BEAM 6

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection and Grinding
Back of S. Abut.	745+57.500	16.667	722.823	722.844
CL Brg. S. Abut	745+58.750	16.667	722.833	722.854
A	745+68.750	16.667	722.904	722.942
B	745+78.750	16.667	722.968	723.016
C	745+88.750	16.667	723.026	723.071
D	745+98.750	16.667	723.077	723.110
E	746+08.750	16.667	723.122	723.142
CL Brg. Pier 1	746+20.000	16.667	723.164	723.185
F	746+30.000	16.667	723.195	723.248
G	746+40.000	16.667	723.219	723.316
H	746+50.000	16.667	723.237	723.372
I	746+60.000	16.667	723.248	723.405
J	746+70.000	16.667	723.252	723.409
K	746+80.000	16.667	723.250	723.385
L	746+90.000	16.667	723.241	723.338
M	747+00.000	16.667	723.226	723.279
CL Brg. Pier 2	747+10.000	16.667	723.204	723.225
N	747+20.000	16.667	723.176	723.195
O	747+30.000	16.667	723.141	723.172
P	747+40.000	16.667	723.099	723.143
Q	747+50.000	16.667	723.051	723.099
R	747+60.000	16.667	722.996	723.037
CL Brg. N. Abut.	747+71.250	16.667	722.927	722.948
Back of N. Abut.	747+72.500	16.667	722.919	722.939

TOP OF DECK ELEVATIONS II

**IL. ROUTE 47
OVER SANGAMON RIVER
F.A.P. ROUTE 326 SEC. (129BR-3) BR
CHAMPAIGN COUNTY
STATION 746+65.00
STRUCTURE NO. 010-0281**

CHAMPAIGN, ILLINOIS
CHICAGO, ILLINOIS
EVANSVILLE, INDIANA
INDIANAPOLIS, INDIANA
KENOSHA, WISCONSIN
SPRING GREEN, WISCONSIN

REVISIONS	NAME	DATE

DESIGNED BY: SMM PROJECT NO: 102287
DRAWN BY: MEW DATE: 5/05
CHECKED BY: MM
APPROVED BY: SMM
ACTIVITY INITIALS

S-4