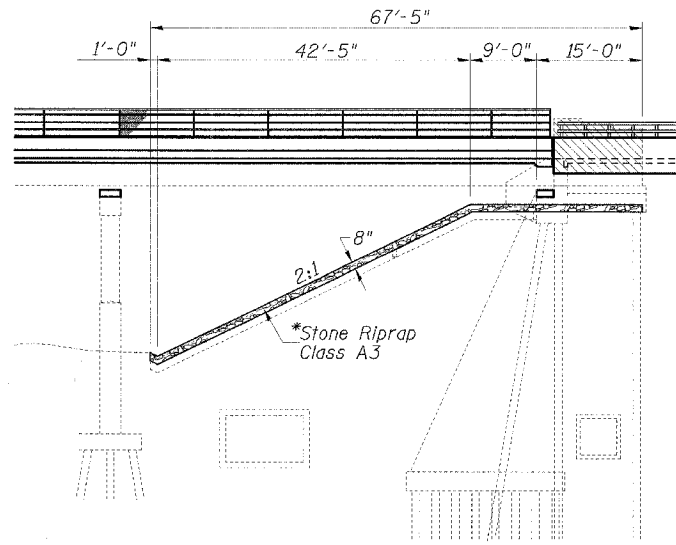


GENERAL NOTES

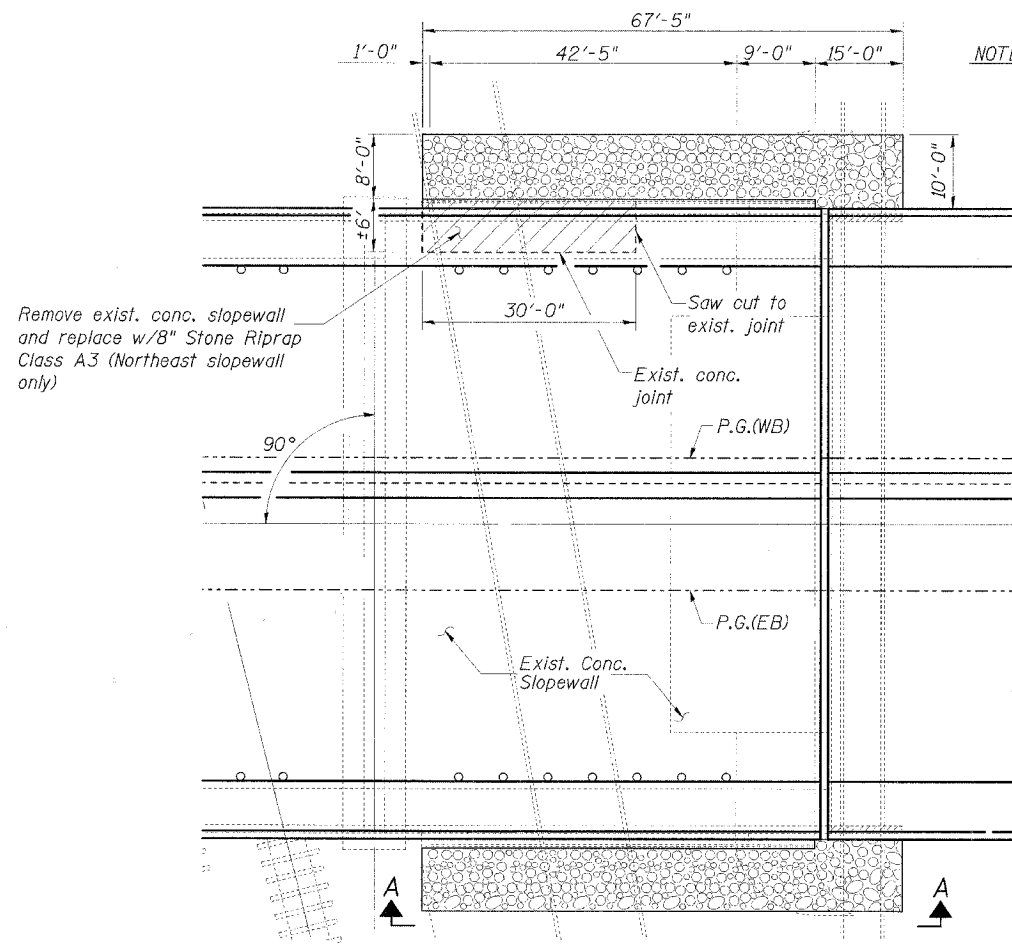
- Prior to beginning any work at the bridge location, it is the responsibility of the Contractor to coordinate with and satisfy all requirements set forth by Metro-Link for work that will be performed on, under, above, across or over the Metro-Link right-of-way. See Special Provisions for Metro-Link's policies and procedures for work performed on their right-of-way.
- Expansion joint plates and attached bars shall be shop painted with the inorganic zinc rich primer.
- Field welding of construction accessories will not be permitted to beams.
- The structural steel bearing plates of the Elastomeric Bearing Assembly shall conform to the requirements of AASHTO M 270 Grade 50.
- Reinforcement bars shall conform to the requirements of ASTM A706 Grade 60.
- Layout of slope protection system may be varied in the field to suit ground conditions as directed by the Engineer.
- Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price for the work.
- Elevations provided on the plans were obtained or calculated from a survey conducted by the Illinois Department of Transportation. The elevations provided were not taken or calculated from the existing bridge plans.
- Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of $\frac{1}{8}$ inch. Adjustment shall be made either by grinding the surface or by shimming the bearing. Two $\frac{1}{8}$ " adjusting shims, of the dimensions of the bottom bearing plate, shall be provided for each bearing in addition to all other plates or shims. For Type I Elastomeric Bearings, two $\frac{1}{8}$ " adjusting shims shall be provided for each bearing and placed as detailed.
- Prior to pouring the new concrete deck, all loose rust, loose mill scale, and all other loose, potentially detrimental foreign material shall be removed from the surfaces of the beams in contact with concrete. The cost of this work will be included in the pay item covering removal of the existing concrete. All heavy rust and other tightly adhered potentially detrimental foreign matter shall also be removed from the surfaces of the beams in contact with concrete. Tightly adhered paint may remain unless otherwise noted. This removal shall be accomplished by methods that will not damage the steel. The cost of this work will be paid for according to Article 109.04.

All existing construction accessories welded to the top flange over the pier between the quarter points of the beams shall be removed. The remaining weld shall be ground smooth and inspected for cracks using magnetic particle testing. Any cracks that can not be removed by grinding approximately $\frac{1}{4}$ inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of this work will be paid for according to Article 109.04.
- Bridge Seat Sealer shall be applied to the seat area of the East and West abutments.
- All construction joints shall be bonded.
- If the Contractor elects to use cantilever forming brackets on the exterior beams or girders, the brackets shall be placed at the same locations as required for the hardwood blocks in Article 503.06 of the Standard Specifications. If additional cantilever forming brackets are required, hardwood blocking shall be wedged between the exterior and first interior beam at each of these additional bracket locations.
- Field painting of structural steel shall be done under a separate painting contract.
- The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
- Remove dirt and debris that is located at the junction of the slopewalls and the piers. Cost included with "Stone Riprap, Class A3".
- See Roadway Plans for embankment widening.



SECTION A-A

*Provide 8" deep Class A3 riprap with 6" Class A1 bedding and filter fabric at all four corners.



PLAN-ABUTMENT SLOPE PROTECTION
(EAST ABUTMENT SHOWN-WEST SIMILAR)

NOTE: All dimensions shown for slope protection are typical for all 4 corners.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
805	122VBR-1	ST. CLAIR	58	27
STA.	N/A	TO STA.	N/A	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

Contract #76558 SHEET 2 OF 26

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Stone Riprap, Class A3	Sq. Yd.		294	294
Filter Fabric	Sq. Yd.		294	294
Concrete Removal	Cu. Yd.		20.3	20.3
Slopewall Removal	Sq. Yd.		22	22
Removal of Existing Concrete Deck	Each	1		1
Floor Drains	Each	42		42
Concrete Structures	Cu. Yd.		12.3	12.3
Concrete Superstructure	Cu. Yd.	528.1		528.1
Bridge Deck Grooving	Sq. Yd.	1248		1248
Protective Coat	Sq. Yd.	1867		1867
Elastomeric Bearing Assembly Type I	Each	24		24
Elastomeric Bearing Assembly Type II	Each	12		12
Structural Repair of Concrete (Depth Equal to or Less Than 5 In.)	Sq. Ft.		57.5	57.5
Furnishing and Erecting Structural Steel	Pound	6730		6730
Stud Shear Connectors	Each	3024		3024
Reinforcement Bars, Epoxy Coated	Pound	120600	4060	124660
Name Plates	Each	1		1
Bridge Seat Sealer	Sq. Ft.		639	639
Epoxy Crack Sealing	Foot		118.5	118.5
Bar Splacers	Each	636	178	814
Drainage System	L. Sum	1		1
Jacking Existing Superstructure	L. Sum	1		1
Protective Shield	Sq. Yd.	1692		1692
Bridge Fence Railing	Foot	360		360
Bridge Joint System (Expansion) 1"	Foot	87		87
Bridge Joint System (Expansion) 1 1/2"	Foot	87		87

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CMT
CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS
SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO
ROCKFORD, IL ■ PEORIA, IL ■ CHICAGO, IL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
NOTES AND BILL OF MATERIAL
F.A.P. ROUTE 805
SECTION 122VBR-1
ST. CLAIR COUNTY
ILLINOIS ROUTE 161 OVER METRO-LINK
STATION 69+96.56 S.N. 082-0091
SCALE: NONE DRAWN BY: GLD
DATE: 7/03/06 CHECKED BY: GBR