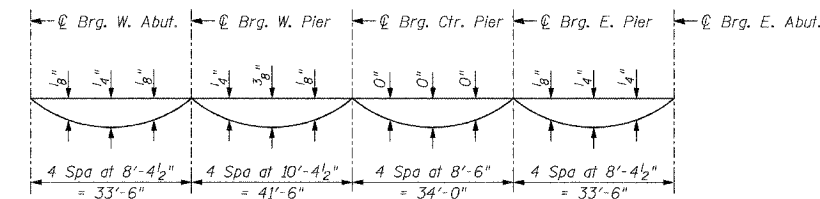


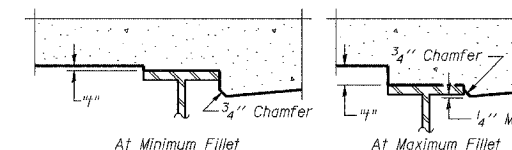
**PLAN**



**DEAD LOAD DEFLECTION DIAGRAM**

(Includes weight of cast-in-place concrete only)

Note: The above deflections are not to be used in the field if the engineers is working from the grade elevations adjusted for dead load deflections as shown.



**FILLET HEIGHTS**

To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted For Dead Load Deflections" shown on sheets SB-4 and SB-5, minus slab thickness, equals the fillet heights "t" above top flange of beams.

**Beam No. 1**

| Location        | Station   | Offset | Theoretical Grade Elevation | Theoretical Grade Elevations Adjusted For Dead Load Deflections |
|-----------------|-----------|--------|-----------------------------|---|
| BK. W. ABUT.    | 200+29.54 | 15.75  | 664.84                      | 664.84  |
| ¢ BRG. W. ABUT. | 200+32.21 | 15.75  | 664.92                      | 664.92  |
| A               | 200+42.21 | 15.75  | 665.22                      | 665.23  |
| B               | 200+52.21 | 15.75  | 665.52                      | 665.53  |
| C               | 200+62.21 | 15.75  | 665.78                      | 665.78  |
| ¢ W. PIER       | 200+65.71 | 15.75  | 665.86                      | 665.86  |
| D               | 200+75.71 | 15.75  | 666.05                      | 666.07  |
| E               | 200+85.71 | 15.75  | 666.19                      | 666.22  |
| F               | 200+95.71 | 15.75  | 666.28                      | 666.30  |
| H               | 201+05.71 | 15.75  | 666.32                      | 666.32  |
| ¢ CTR. PIER     | 201+07.21 | 15.75  | 666.32                      | 666.32  |
| H               | 201+17.21 | 15.75  | 666.29                      | 666.29  |
| I               | 201+27.21 | 15.75  | 666.22                      | 666.22  |
| J               | 201+37.21 | 15.75  | 666.09                      | 666.09  |
| ¢ E. PIER       | 201+41.21 | 15.75  | 666.02                      | 666.02  |
| K               | 201+51.21 | 15.75  | 665.83                      | 665.84  |
| L               | 201+61.21 | 15.75  | 665.61                      | 665.64  |
| M               | 201+71.21 | 15.75  | 665.42                      | 665.43  |
| ¢ BRG. E. ABUT. | 201+74.71 | 15.75  | 665.35                      | 665.35  |
| BK. E. ABUT.    | 201+77.37 | 15.75  | 665.30                      | 665.30  |

**Beam No. 2**

| Location        | Station   | Offset | Theoretical Grade Elevation | Theoretical Grade Elevations Adjusted For Dead Load Deflections |
|-----------------|-----------|--------|-----------------------------|---|
| BK. W. ABUT.    | 200+29.54 | 11.42  | 664.84                      | 664.84  |
| ¢ BRG. W. ABUT. | 200+32.21 | 11.42  | 664.92                      | 664.92  |
| A               | 200+42.21 | 11.42  | 665.22                      | 665.23  |
| B               | 200+52.21 | 11.42  | 665.52                      | 665.53  |
| C               | 200+62.21 | 11.42  | 665.78                      | 665.78  |
| ¢ W. PIER       | 200+65.71 | 11.42  | 665.86                      | 665.86  |
| D               | 200+75.71 | 11.42  | 666.05                      | 666.07  |
| E               | 200+85.71 | 11.42  | 666.19                      | 666.22  |
| F               | 200+95.71 | 11.42  | 666.28                      | 666.30  |
| G               | 201+05.71 | 11.42  | 666.32                      | 666.32  |
| ¢ CTR. PIER     | 201+07.21 | 11.42  | 666.32                      | 666.32  |
| H               | 201+17.21 | 11.42  | 666.29                      | 666.29  |
| I               | 201+27.21 | 11.42  | 666.22                      | 666.22  |
| J               | 201+37.21 | 11.42  | 666.09                      | 666.09  |
| ¢ E. PIER       | 201+41.21 | 11.42  | 666.02                      | 666.02  |
| K               | 201+51.21 | 11.42  | 665.83                      | 665.84  |
| L               | 201+61.21 | 11.42  | 665.61                      | 665.64  |
| M               | 201+71.21 | 11.42  | 665.42                      | 665.43  |
| ¢ BRG. E. ABUT. | 201+74.71 | 11.42  | 665.35                      | 665.35  |
| BK. E. ABUT.    | 201+77.37 | 11.42  | 665.30                      | 665.30  |

**North Gutter Line**

| Location        | Station   | Offset | Theoretical Grade Elevation | Theoretical Grade Elevations Adjusted For Dead Load Deflections |
|-----------------|-----------|--------|-----------------------------|---|
| BK. W. ABUT.    | 200+29.54 | 11.00  | 664.84                      | 664.84  |
| ¢ BRG. W. ABUT. | 200+32.21 | 11.00  | 664.92                      | 664.92  |
| A               | 200+42.21 | 11.00  | 665.22                      | 665.24  |
| B               | 200+52.21 | 11.00  | 665.52                      | 665.54  |
| C               | 200+62.21 | 11.00  | 665.79                      | 665.79  |
| ¢ W. PIER       | 200+65.71 | 11.00  | 665.87                      | 665.87  |
| D               | 200+75.71 | 11.00  | 666.06                      | 666.07  |
| E               | 200+85.71 | 11.00  | 666.20                      | 666.23  |
| F               | 200+95.71 | 11.00  | 666.29                      | 666.31  |
| G               | 201+05.71 | 11.00  | 666.32                      | 666.32  |
| ¢ CTR. PIER     | 201+07.21 | 11.00  | 666.32                      | 666.32  |
| H               | 201+17.21 | 11.00  | 666.30                      | 666.30  |
| I               | 201+27.21 | 11.00  | 666.22                      | 666.23  |
| J               | 201+37.21 | 11.00  | 666.10                      | 666.10  |
| ¢ E. PIER       | 201+41.21 | 11.00  | 666.03                      | 666.03  |
| K               | 201+51.21 | 11.00  | 665.83                      | 665.85  |
| L               | 201+61.21 | 11.00  | 665.62                      | 665.64  |
| M               | 201+71.21 | 11.00  | 665.42                      | 665.43  |
| ¢ BRG. E. ABUT. | 201+74.71 | 11.00  | 665.36                      | 665.36  |
| BK. E. ABUT.    | 201+77.37 | 11.00  | 665.31                      | 665.31  |

**Beam No. 3**

| Location        | Station   | Offset | Theoretical Grade Elevation | Theoretical Grade Elevations Adjusted For Dead Load Deflections |
|-----------------|-----------|--------|-----------------------------|---|
| BK. W. ABUT.    | 200+29.54 | 7.08   | 664.91                      | 664.91  |
| ¢ BRG. W. ABUT. | 200+32.21 | 7.08   | 664.99                      | 664.99  |
| A               | 200+42.21 | 7.08   | 665.29                      | 665.30  |
| B               | 200+52.21 | 7.08   | 665.58                      | 665.60  |
| C               | 200+62.21 | 7.08   | 665.85                      | 665.85  |
| ¢ W. PIER       | 200+65.71 | 7.08   | 665.93                      | 665.93  |
| D               | 200+75.71 | 7.08   | 666.12                      | 666.14  |
| E               | 200+85.71 | 7.08   | 666.26                      | 666.29  |
| F               | 200+95.71 | 7.08   | 666.35                      | 666.37  |
| G               | 201+05.71 | 7.08   | 666.38                      | 666.39  |
| ¢ CTR. PIER     | 201+07.21 | 7.08   | 666.38                      | 666.38  |
| H               | 201+17.21 | 7.08   | 666.36                      | 666.36  |
| I               | 201+27.21 | 7.08   | 666.29                      | 666.29  |
| J               | 201+37.21 | 7.08   | 666.16                      | 666.16  |
| ¢ E. PIER       | 201+41.21 | 7.08   | 666.09                      | 666.09  |
| K               | 201+51.21 | 7.08   | 665.89                      | 665.91  |
| L               | 201+61.21 | 7.08   | 665.68                      | 665.71  |
| M               | 201+71.21 | 7.08   | 665.48                      | 665.49  |
| ¢ BRG. E. ABUT. | 201+74.71 | 7.08   | 665.42                      | 665.42  |
| BK. E. ABUT.    | 201+77.37 | 7.08   | 665.37                      | 665.37  |

DATE: 7/20/2006  
FILENAME: N:\P\30\3078\Structural\_Eldorado\_8278\CAD\Final\_review\8278-uld-ssn01.dgn

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**CC** Ciorba Group, Inc.  
CONSULTING ENGINEERS  
5001 NORTH CLEVELAND AVENUE - CHICAGO, ILLINOIS 60630 - (773) 778-6000

VILLAGE OF WINNETKA, ILLINOIS  
TOP OF SLAB ELEVATIONS I  
ELDORADO STREET OVER THE UNION PACIFIC R.R.  
R.R. MILE POST 17.26 KENOSHA SUBDIVISION  
COOK COUNTY STA. 201+07.21  
STRUCTURE NO. 016-8260

SCALE: NONE  
DATE: JUNE 2006  
FILE: 3278

DRAWN BY: RCD  
DESIGN BY: BWS  
CHECKED BY: SCD