

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED HIGHWAY

FAI 57 (I-57)

SECTION: 99-1-N

**AT MANHATTAN- MONEE ROAD
LEFT TURN LANE CHANNELIZATION**

AND RESURFACING

WILL COUNTY

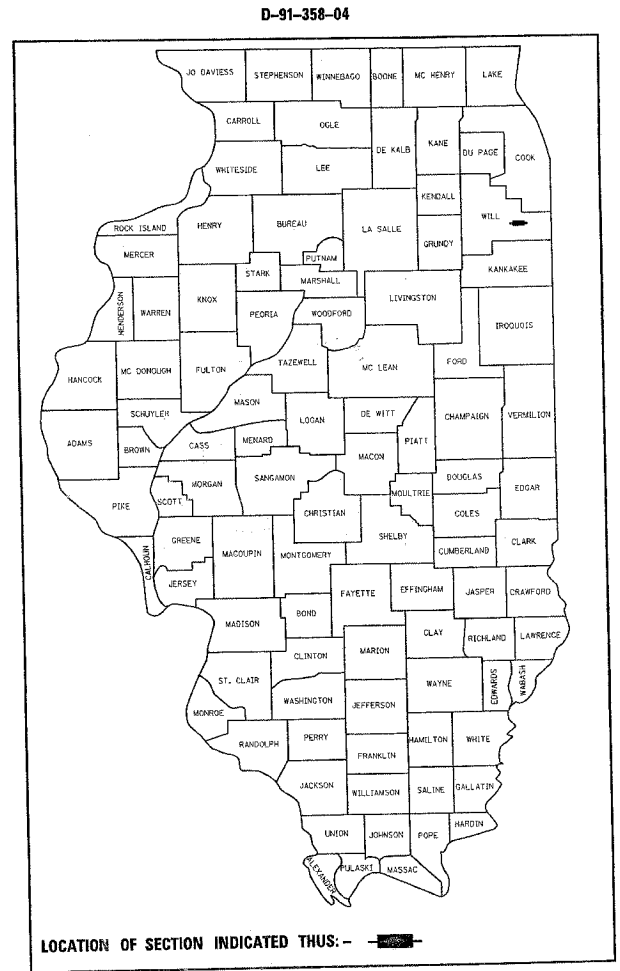
PROJECT:

C-91-358-04

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT LOCATED IN THE
VILLAGE OF MONEE

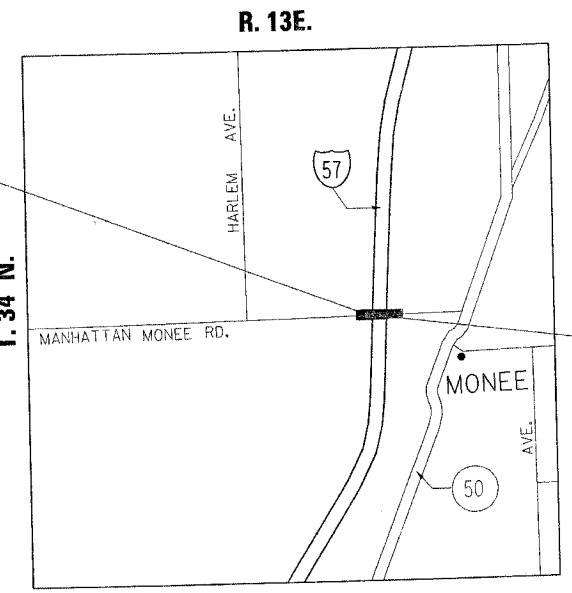
TRAFFIC DATA
POSTED SPEED LIMIT = VARIES 35-45 MPH
1999 ADT = 11,300



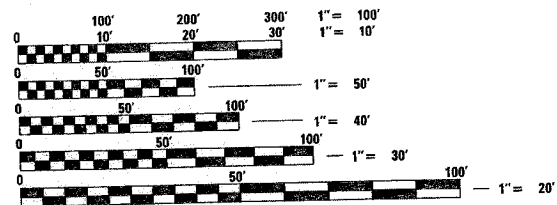
IMPROVEMENT BEGINS
STATION 14030 + 00

OMISSION FROM
STA. 14036 + 37 TO
STA. 14038 + 55

IMPROVEMENT ENDS
STATION 14048 + 03



GROSS LENGTH OF IMPROVEMENT = 1803 LINEAL FEET = 0.34 MILES
NET LENGTH OF IMPROVEMENT = 1585 LINEAL FEET = 0.30 MILES



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

CONTRACT NO. 62834

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED SEPT 19 20 05
Diane Okh
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

October 14, 20 05
Mike Hino
ENGINEER OF DESIGN AND ENVIRONMENT

October 14, 20 05
Eric E. Hino
DEPUTY DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

DISTRICT 1 DESIGN PLAN PREPARATION ENGINEER: KEN ENG /ISSAM RAYYAN (847) 705-4240

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	2
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

CONTRACT NO. 62834

INDEX OF SHEETS:

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STANDARDS, AND GENERAL NOTES
3-4	SUMMARY OR QUANTITIES
5-10	TYPICAL SECTIONS
11-12	EXISTING ROADWAY PLANS
13-14	PROPOSED ROADWAY PLANS
15-18	SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL
19-20	PROPOSED PAVEMENT MARKING/ LANDSCAPING PLANS
21-29	TEMPORARY TRAFFIC SIGNAL PLAN
30-45	DISTRICT DETAILS
46-52	CROSS-SECTIONS

STATE STANDARDS:

000001-04	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-02	TEMPORARY EROSION CONTROL SYSTEMS
482011-01	BIT. SHOULDER STRIPS/SHOULDERS WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
601001	SUB-SURFACE DRAINS
606001-02	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
630001-05	STEEL PLATE BEAM GUARDRAIL
630201-03	PCC/BITUMINOUS STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
631011-02	TRAFFIC BARRIER TERMINAL, TYPE 2
631031-05	TRAFFIC BARRIER TERMINAL, TYPE 6
635006-02	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-01	REFLECTOR MARKER AND MOUNTING DETAILS
701301-02	LANE CLOSURE 2L, 2W SHORT TIME OPERATIONS
701311-02	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS- DAY ONLY
701501-03	URBAN LANE CLOSURE 2L, 2W, UNDIVIDED
701701-04	URBAN LANE CLOSURE MLTILANE INTERSECTION
701801-03	LANE CLOSURE, MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK
702001-05	TRAFFIC CONTROL DEVICES
886001	DETECTOR LOOP INSTALLATIONS

GENERAL NOTES:

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).
- 3 METER (10 FEET) TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB & GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE VILLAGE OF MONEE
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSON FROM THE DEPARTMENT.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 40 MM (1 1/2 INCHES WHERE THE SPEED LIMIT IS 80 KM/H (45 MPH) OF LESS AND 25 MM (1 INCH) WHERE THE SPEED LIMIT IS GREATER THAN 80 KM/H (45 MPH). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 75 MM (3 INCHES) MAY BE ALLOWED IF THE EDGE OF MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND BITUMINOUS TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND PREFORMED PLASTIC AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL. TWO WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS, CONTACT MS CORA MATHIS, AREA TRAFFIC FIELD ENGINEER AT 815-485-6475.
- RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE INSTALLED AS INDICATED ON THE I.D.O.T. TYPICAL APPLICATIONS STANDARD "TYPICAL APPLICATIONS FOR RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)". ALL RAISED REFLECTIVE PAVEMENT MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS, AND OFFSET 4 INCHES FROM THE CENTERLINE OF THE DASH TO MATCH THE EXISTING MARKERS ON THE COUNTY HIGHWAY.

MIXTURE REQUIREMENTS

ITEM	AC TYPE	VOID	RAP %
BITUMINOUS REPLACEMENT OVER PATCHES (BINDER, IL-19MM)	PG 64-22	4% @ 70 GYR	15
ALL CLASS D PATCHES (BINDER, IL-19MM) 9", 14"	PG 64-22	4% @ 70 GYR	15
POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90	SBS/SBR PG 70-22	4% @ 90 GYR	0
POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N90	SBS/SBR PG 70-22	4% @ 90 GYR	0
POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50	SBS/SBR PG 76-28	2.5% @ 50 GYR	0
DRIVEWAYS			
BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50	PG 64-22	4% @ 50 GYR	15
BITUMINOUS BASE COURSE, SUPERPAVE 8"	PG 64-22	2% @ 50 GYR	15
BITUMINOUS SHOULDERS, 6"	PG 64-22	4% @ 70 GYR	15

"THE UNIT WEIGHT USED TO CALCULATE ALL SURFACE MIXTURES IS 112 LBS/SQYD/IN"

MANHATTAN-MONEE ROAD

MANHATTAN-MONEE RD. STATIONS	EARTH EXCAVATION (CU. YD.)	UNSUITABLE MATERIAL (CU.YD.)	EXCAVATION USED AS EMBANKMENT (SHRINKAGE 15%) (CU.YD.)	EMBANKMENT (CU. YD.)	EARTH WORK BALANCE SURPLUS (+) OR SHORTAGE (-) (CU. YD.)
14030+00 TO 14046+42	754	316	641	527	+114
RAMPS	221	60	188	60	+128
TOTAL	975	376	829	587	+242

NOTE:

A THICKNESS OF 6 INCHES OF TOPSOIL STRIPPING SHALL BE USED FOR REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 57/ I-57
 @ MANHATTAN-MONEE ROAD
 INDEX OF SHEETS, LIST OF STATE STANDARDS, SUMMARY OF QUANTITIES AND GENERAL NOTES
 SCALE: VERT. DATE
 HORIZ. DATE
 DRAWN BY
 CHECKED BY

PLOT DATE = 10/05/2005
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 REFERENCE = REF#

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	3
FED. ROAD DIST. NO. 1		ILLINOIS	HIGHWAY PROJECT	

Contract 62834

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	URBAN 100% STATE 1000-2A	TRAFFIC SIGNALS 100% STATE Y031-1F	MONEE F. P. D. 100% Y031-3D	
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	376	376			
20400800	FURNISHED EXCAVATION	CU YD	87	87			
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1359	1359			
25000210	SEEDING, CLASS 2A	ACRE	0.3	0.3			
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	27	27			
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	27	27			
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	27	27			
28000400	PERIMETER EROSION BARRIER	FOOT	1800	1800			
20200100	EARTH EXCAVATION	CU YD	975	975			
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	4	4			
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	1	1			
40600980	BITUMINOUS SURFACE REMOVAL - BUTT JOINT	SQ YD	148	148			
40601000	BITUMINOUS REPLACEMENT OVER PATCHES	TON	52	52			
40600030	AGGREGATE (PRIME COAT)	TON	21	21			
44000008	BITUMINOUS SURFACE REMOVAL 2 1/2"	SQ YD	7069	7069			
44000100	PAVEMENT REMOVAL	SQ YD	433	433			
44000112	BITUMINOUS REMOVAL OVER PATCHES 3"	SQ YD	300	300			
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	291	291			
44001430	BITUMINOUS SHOULDER REMOVAL	SQ YD	1462	1462			
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	52	52			
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	52	52			
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	68	68			
44201815	CLASS D PATCHES, TYPE II, 14 INCH	SQ YD	39	39			
44201819	CLASS D PATCHES, TYPE III, 14 INCH	SQ YD	39	39			
44201821	CLASS D PATCHES, TYPE IV, 14 INCH	SQ YD	50	50			
44213200	SAW CUTS	FOOT	4218	4218			
48101200	AGGREGATE SHOULDERS, TYPE B	TON	9	9			
48200400	BITUMINOUS SHOULDERS 6"	SQ YD	262	262			
60604400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18	FOOT	400	400			

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	URBAN 100% STATE 1000-2A	TRAFFIC SIGNALS 100% STATE Y031-1F	MONEE F. P. D. 100% Y031-3D	
* 63000005	STEEL PLATE BEAM GUARD RAIL, TYPE B	FOOT	950	950			
63200310	GUARDRAIL REMOVAL	FOOT	950	950			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	5	5			
67100100	MOBILIZATION	L SUM	1	1			
70101800	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1	1			
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	20	20			
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	1995	1995			
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	194	194			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	6645	6645			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	428	428			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	53	53			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	202	202			
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	2886	2886			
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	194	194			
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	6645	6645			
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	428	428			
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	53	53			
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	202	202			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	58	58			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	24	24			
* 80802200	WOOD POLE, 40 FT., CLASS 2	EACH	8		8		
* 85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1		1		
* 85700300	FULL-ACTUATED CONTROLLER AND TYPE V CABINET	EACH	1		1		

* SPECIALTY ITEMS

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION SUMMARY OF QUANTITIES
NAME	DATE	

9/20/2005 13:00:00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	4
FED. ROAD DIST. NO. 1		ILLINOIS	HIGHWAY PROJECT	

Contract 62834

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	URBAN 100% STATE 1000-2A	TRAFFIC SIGNALS 100% STATE 10031-1F	MONEE F.P.D. 100% 10031-3D	
* 87200400	SPAN WIRE	FOOT	870		870		
* 87200500	TETHER WIRE	FOOT	870		870		
* 87302225	ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 3C	FOOT	630		630		
* 87302245	ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 5C	FOOT	1765		1765		
* 87302255	ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 7C	FOOT	210		210		
* 87302505	ELECTRIC CABLE AERIAL SUSPENDED, SERVICE, NO. 6 2 C	FOOT	150		150		
* 88700200	LIGHT DETECTOR	EACH	4			4	
* 88700300	LIGHT DETECTOR AMPLIFIER	EACH	2			2	
X0300626	CURB AND GUTTER REMOVAL	FOOT	280	280			
X0329907	PORTLAND CEMENT CONCRETE SURFACE REMOVAL 2 1/2"	SQ YD	213	213			
X3550500	BITUMINOUS BASE COURSE SUPERPAVE 8"	SQ YD	291	291			
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	2	2			
X4066414	BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50	TON	28	28			
X4066548	POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90	TON	1031	1031			
X4066558	POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N90	TON	1139	1139			
X4067100	POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50	TON	440	440			
X7030130	PAVEMENT MARKING TAPE, TYPE III 4", SPECIAL	FOOT	9406	9406			
* X8730350	ELECTRIC CABLE AERIAL SUSPENDED NO. 20 3/C, TWISTED, SHIELDED	FOOT	700			700	
Z0001050	AGGREGATE SUBGRADE 12"	SQ YD	2394	2394			
* XX005723	VIDEO DETECTION SYSTEM (COMPLETE INTERSECTION)	EACH	2		2		
* X8800025	SIGNAL HEAD, L.E.D., 1-FACE, 3 SECTION, SPAN WIRE MOUNTED	EACH	14		14		
* X8800046	SIGNAL HEAD, L.E.D., 1-FACE, 5 SECTION, SPAN WIRE MOUNTED	EACH	4		4		
* X0325134	WIRELESS INTERCONNECT (COMPLETE)	EACH	1		1		

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
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X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	52	52			
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1			
* X8050015	SERVICE INSTALLATION, POLE MOUNT	EACH	1		1		
* 86000100	MASTER CONTROLLER	EACH	1		1		
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	3	3			
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	2	2			
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	5	5			
60107600	PIPE UNDERDRAINS, 4"	FOOT	320	320			
X0325167	POROUS GRANULAR EMBANKMENT, SUBGRADE 24"	SQ YD	50	50			

*SPECIALTY ITEMS.

REVISIONS	
NAME	DATE

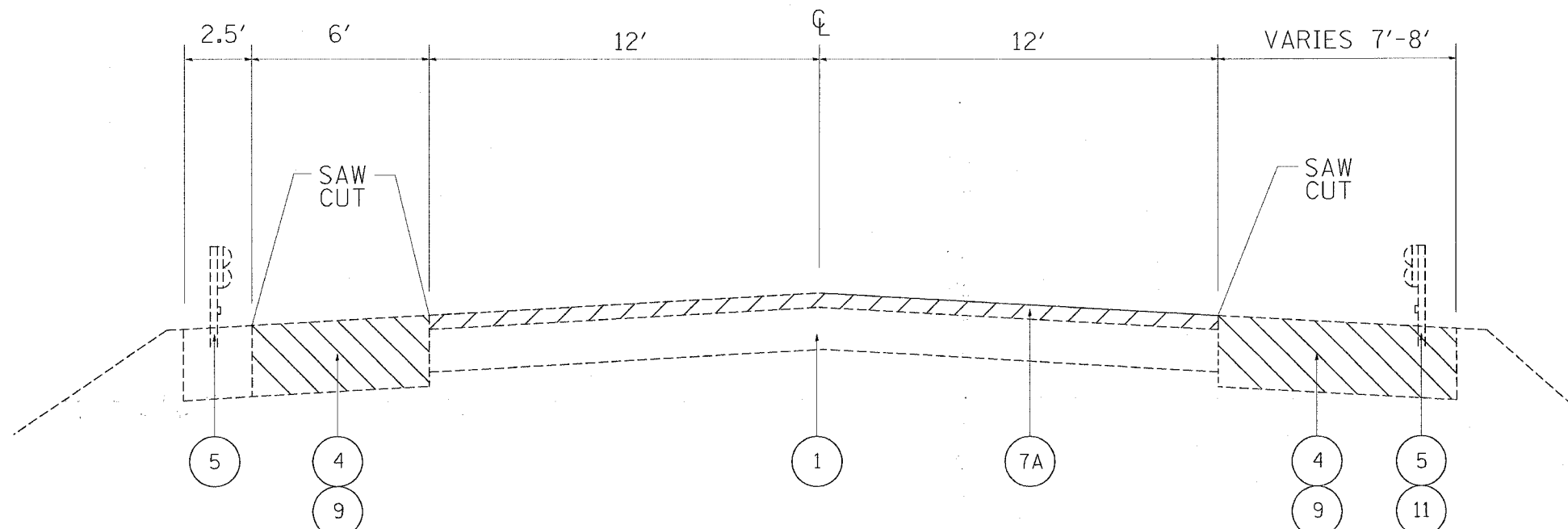
ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES

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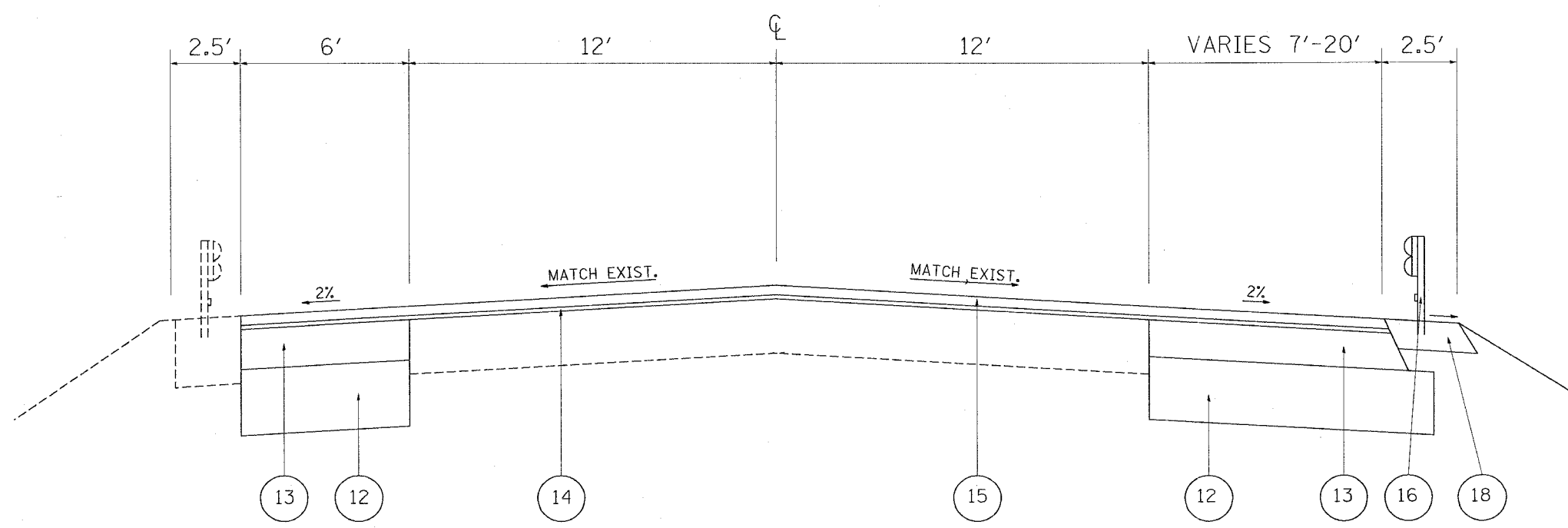
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	5
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 62834



EXISTING TYPICAL SECTION
MANHATTAN-MONEE ROAD
STA. 14030+00 TO STA. 14030+71



PROPOSED TYPICAL SECTION
MANHATTAN-MONEE ROAD
STA. 14030+00 TO STA. 14030+71

LEGEND

- ① EXISTING P.C.C. PAVEMENT, 10.5"
- ② EXISTING BITUMINOUS PAVEMENT, 14"
- ③ EXISTING BITUMINOUS OVERLAY, 4"
- ④ EXISTING BITUMINOUS SHOULDER, 12"
- ⑤ EXISTING GUARDRAIL
- ⑥ EXISTING CURB AND GUTTER
- ⑦ PROPOSED BITUMINOUS SURFACE REMOVAL, 2 1/2 "
- ⑧ PROPOSED PAVEMENT REMOVAL
- ⑨ PROPOSED BITUMINOUS SHOULDER REMOVAL
- ⑩ PROPOSED CURB AND GUTTER REMOVAL
- ⑪ PROPOSED GUARDRAIL REMOVAL
- ⑫ PROPOSED AGGREGATE SUB-GRADE, 12"
- ⑬ PROPOSED BITUMINOUS BINDER COURSE, SUPERPAVE IL-19.0, N90, 8 1/2 "
- ⑭ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50, 3/4 "
- ⑮ PROPOSED POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90, 1 3/4 "
- ⑯ PROPOSED STEEL PLATE BEAM GUARDRAIL, TYPE B
- ⑰ PROPOSED COMBINATION CURB AND GUTTER-TYPE B-6.18
- ⑱ PROPOSED BITUMINOUS SHOULDER, 6"
- ⑲ PROPOSED PGES, 24"
- ⑳ PROPOSED PIPE UNDERDRAINS, 4" (STA 14044+40 TO STA 14046+42)

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REVISIONS	
NAME	DATE

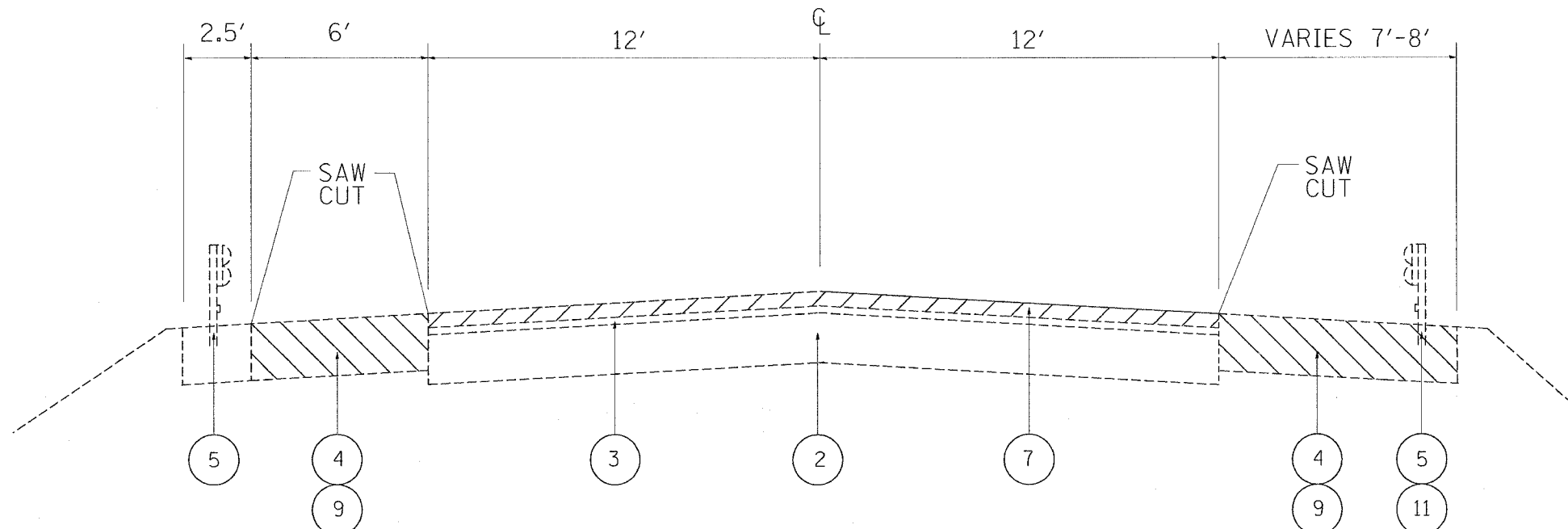
ILLINOIS DEPARTMENT OF TRANSPORTATION
 FAI 57/ I-57
 @ MANHATTAN-MONEE ROAD
 EXISTING/ PROPOSED
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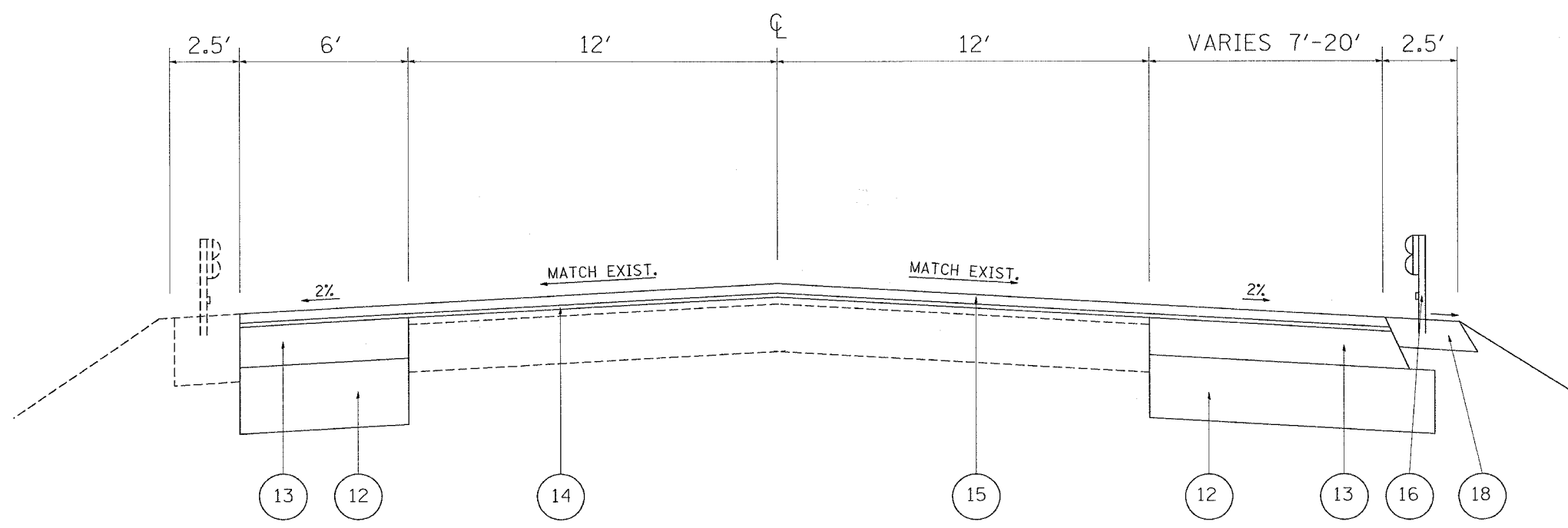
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	6
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 62834



EXISTING TYPICAL SECTION
MANHATTAN-MONEE ROAD
STA. 14030+71 TO STA. 14036+37



PROPOSED TYPICAL SECTION
MANHATTAN-MONEE ROAD
STA. 14030+71 TO STA. 14036+37

LEGEND

- ① EXISTING P.C.C. PAVEMENT, 10.5"
- ② EXISTING BITUMINOUS PAVEMENT, 14"
- ③ EXISTING BITUMINOUS OVERLAY, 4"
- ④ EXISTING BITUMINOUS SHOULDER, 12"
- ⑤ EXISTING GUARDRAIL
- ⑥ EXISTING CURB AND GUTTER
- ⑦ PROPOSED BITUMINOUS SURFACE REMOVAL, 2 1/2 "
- ⑧ PROPOSED PAVEMENT REMOVAL
- ⑨ PROPOSED BITUMINOUS SHOULDER REMOVAL
- ⑩ PROPOSED CURB AND GUTTER REMOVAL
- ⑪ PROPOSED GUARDRAIL REMOVAL
- ⑫ PROPOSED AGGREGATE SUB-GRADE, 12"
- ⑬ PROPOSED BITUMINOUS BINDER COURSE, SUPERPAVE IL-19.0, N90, 8 1/2 "
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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
FAI 57/ I-57
@ MANHATTAN-MONEE ROAD
EXISTING/ PROPOSED
TYPICAL SECTIONS

SCALE: VERT. _____
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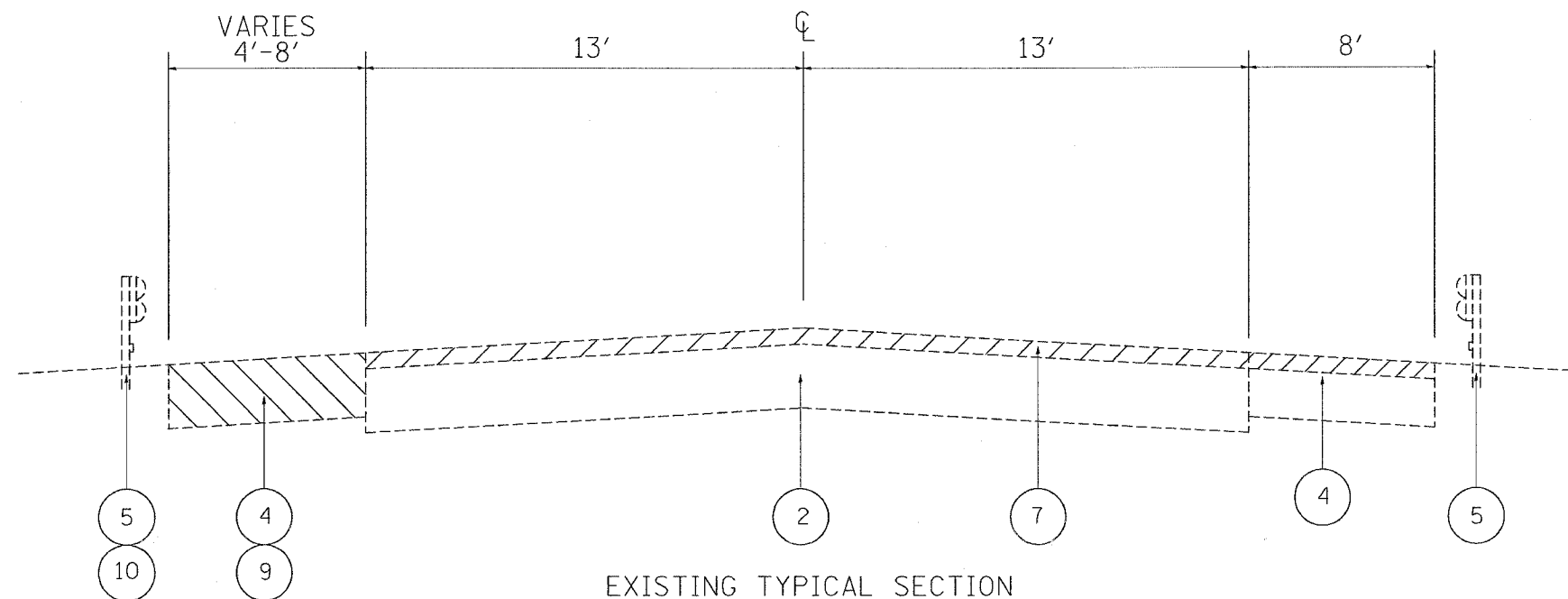
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F.A.I. RTE. 57	SECTION 99-1-N	COUNTY WILL	TOTAL SHEETS 52	SHEET NO. 7
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

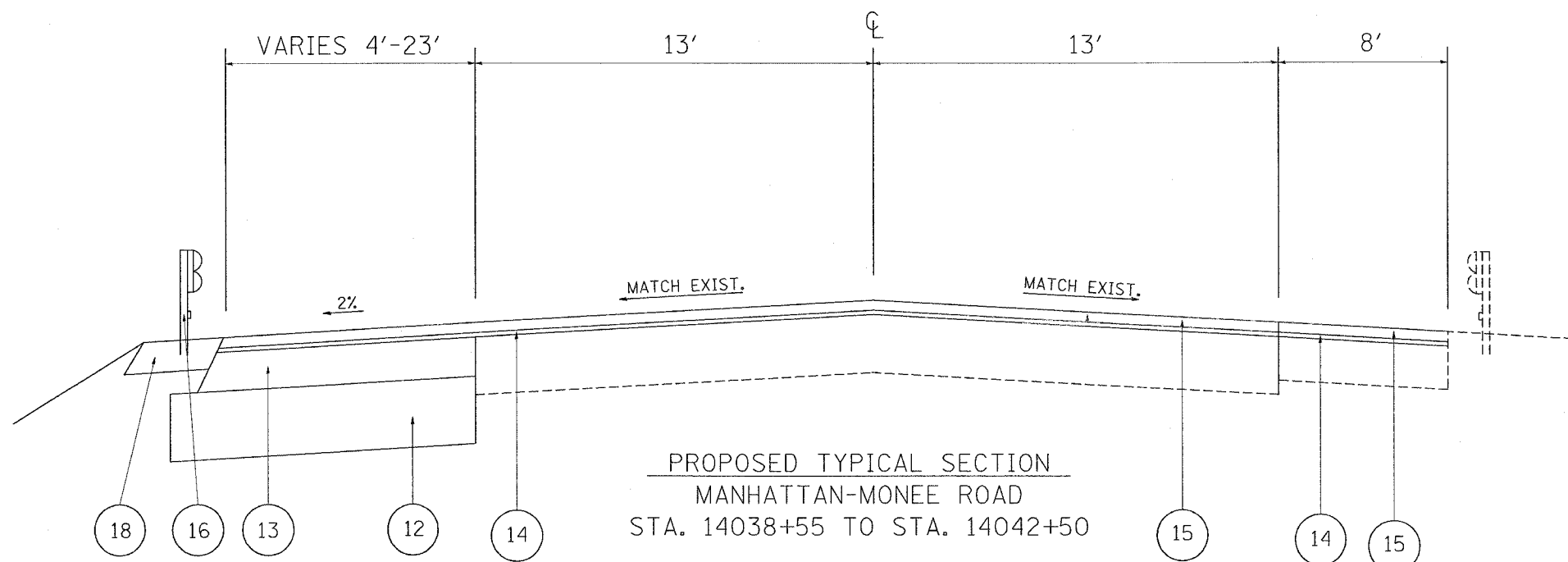
CONTRACT NO. 62834



EXISTING TYPICAL SECTION
MANHATTAN-MONEE ROAD
STA. 14038+55 TO STA. 14042+50

LEGEND

- ① EXISTING P.C.C. PAVEMENT, 10.5"
- ② EXISTING BITUMINOUS PAVEMENT, 14"
- ③ EXISTING BITUMINOUS OVERLAY, 4"
- ④ EXISTING BITUMINOUS SHOULDER, 12"
- ⑤ EXISTING GUARDRAIL
- ⑥ EXISTING CURB AND GUTTER
- ⑦ PROPOSED BITUMINOUS SURFACE REMOVAL, 2 1/2 "
- ⑧ PROPOSED PAVEMENT REMOVAL
- ⑨ PROPOSED BITUMINOUS SHOULDER REMOVAL
- ⑩ PROPOSED CURB AND GUTTER REMOVAL
- ⑪ PROPOSED GUARDRAIL REMOVAL
- ⑫ PROPOSED AGGREGATE SUB-GRADE, 12"
- ⑬ PROPOSED BITUMINOUS BINDER COURSE, SUPERPAVE IL-19.0, N90, 8 1/2 "
- ⑭ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50, 3/4 "
- ⑮ PROPOSED POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90, 1 3/4 "
- ⑯ PROPOSED STEEL PLATE BEAM GUARDRAIL, TYPE B
- ⑰ PROPOSED COMBINATION CURB AND GUTTER-TYPE B-6.18
- ⑱ PROPOSED BITUMINOUS SHOULDER, 6"
- ⑲ PROPOSED PGES, 24"
- ⑳ PROPOSED PIPE UNDERDRAINS, 4" (STA 14044+40 TO STA 14046+42)



PROPOSED TYPICAL SECTION
MANHATTAN-MONEE ROAD
STA. 14038+55 TO STA. 14042+50

SUBGRADE TREATMENT PLAN NOTES

POROUS GRANULAR EMBANKMENT, SUBGRADE (PGES) HAS BEEN RECOMMEND FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS ENCOUNTERED, THE SOIL SHALL BE REMOVED AND REPLACED WITH PGES AS DETERMINED BY THE GEOTECHNICAL ENGINEER. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

STA 14044+40 TO STA 14046+42, 24" DEPTH

EMBANKMENT MATERIAL AND PLACEMENT SHALL BE IN ACCORDANCE WITH SECTIONS 205, 206, 207 AND 210
PGES- POROUS GRANULAR EMBANKMENT, SUBGRADE.

REVISIONS	
NAME	DATE

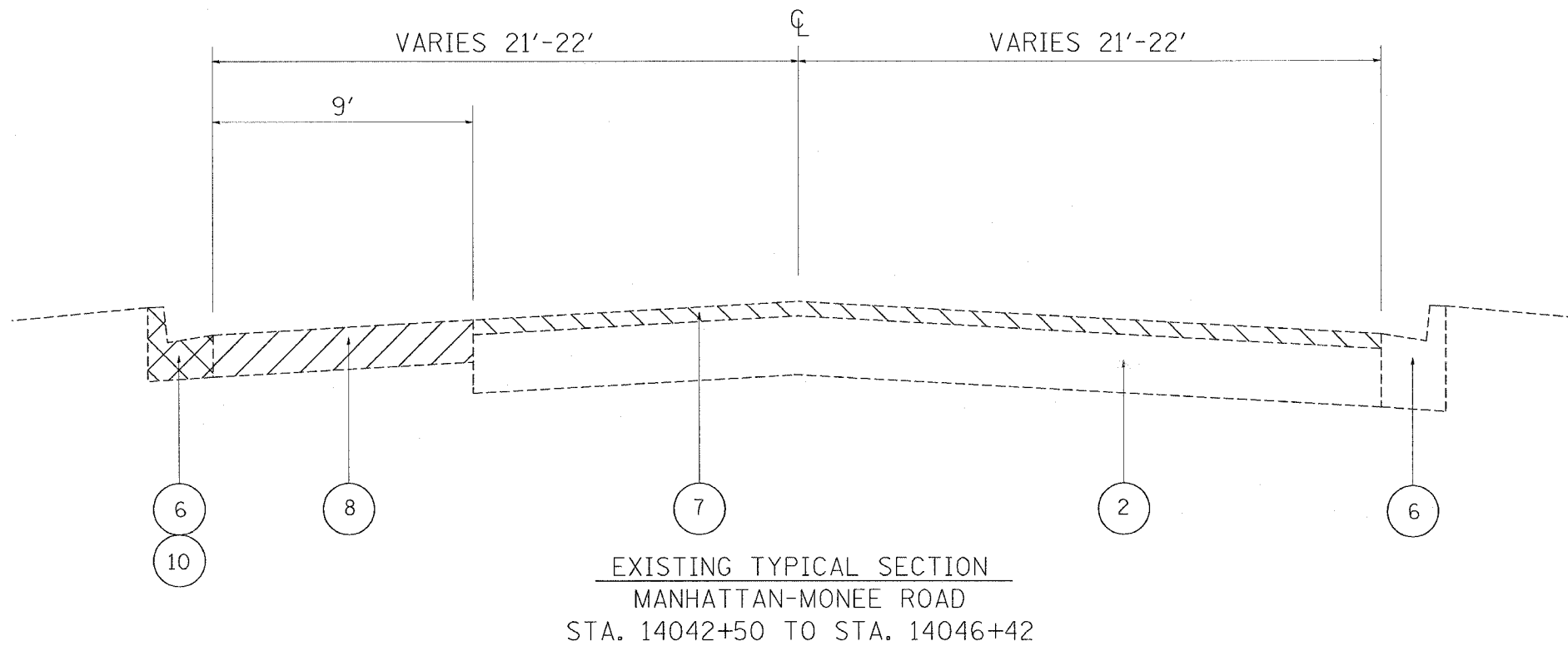
ILLINOIS DEPARTMENT OF TRANSPORTATION
FAI 57/ I-57
@ MANHATTAN-MONEE ROAD
EXISTING/ PROPOSED
TYPICAL SECTIONS

SCALE: VERT.
HORIZ.
DATE

DRAWN BY
CHECKED BY

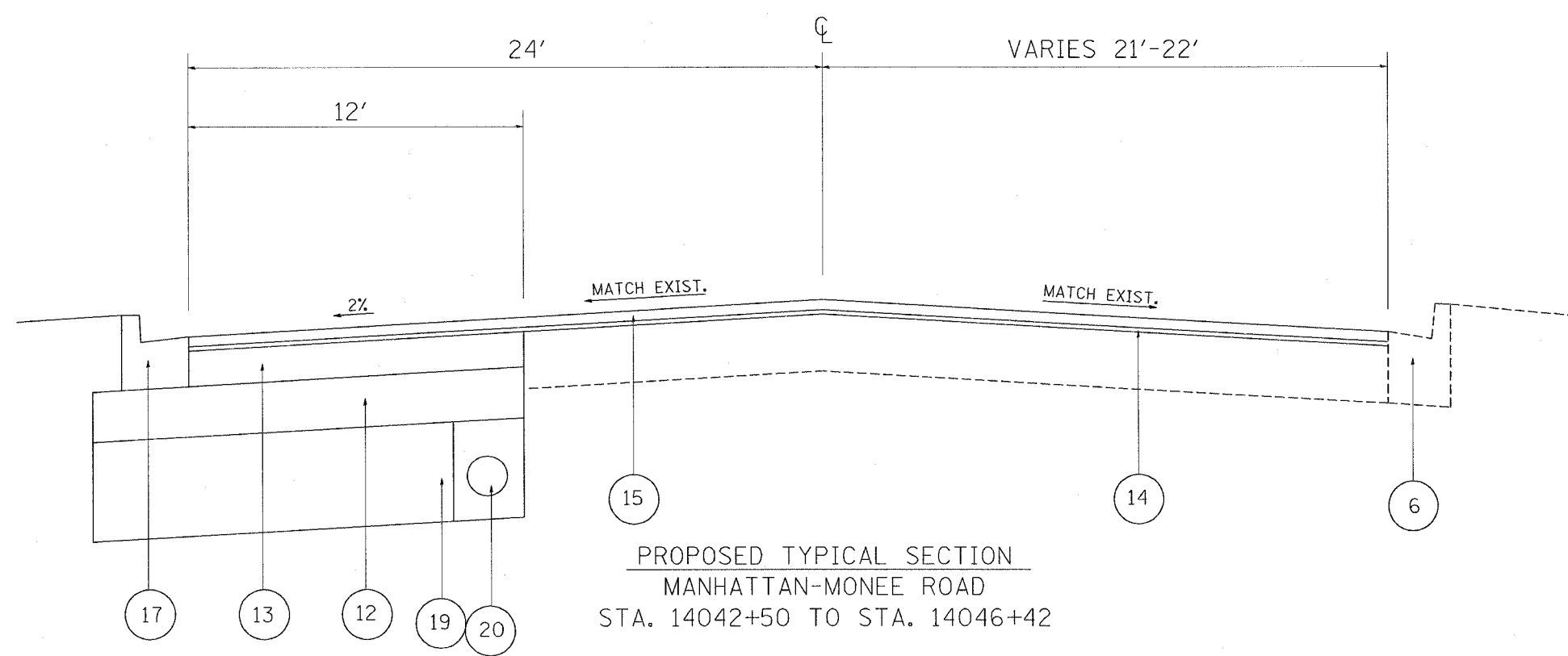
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	8
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
CONTRACT NO. 62834				



LEGEND

- ① EXISTING P.C.C. PAVEMENT, 10.5"
- ② EXISTING BITUMINOUS PAVEMENT, 14"
- ③ EXISTING BITUMINOUS OVERLAY, 4"
- ④ EXISTING BITUMINOUS SHOULDER, 12"
- ⑤ EXISTING GUARDRAIL
- ⑥ EXISTING CURB AND GUTTER
- ⑦ PROPOSED BITUMINOUS SURFACE REMOVAL, 2 1/2 "
- ⑧ PROPOSED PAVEMENT REMOVAL
- ⑨ PROPOSED BITUMINOUS SHOULDER REMOVAL
- ⑩ PROPOSED CURB AND GUTTER REMOVAL
- ⑪ PROPOSED GUARDRAIL REMOVAL
- ⑫ PROPOSED AGGREGATE SUB-GRADE, 12"
- ⑬ PROPOSED BITUMINOUS BINDER COURSE, SUPERPAVE IL-19.0, N90, 8 1/2 "
- ⑭ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50, 3/4 "
- ⑮ PROPOSED POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90, 1 3/4 "
- ⑯ PROPOSED STEEL PLATE BEAM GUARDRAIL, TYPE B
- ⑰ PROPOSED COMBINATION CURB AND GUTTER-TYPE B-6.18
- ⑱ PROPOSED BITUMINOUS SHOULDER, 6"
- ⑲ PROPOSED PGES, 24"
- ⑳ PROPOSED PIPE UNDERDRAINS, 4" (STA 14044+40 TO STA 14046+42)



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 FAI 57/ I-57
 @ MANHATTAN-MONEE ROAD
 EXISTING/ PROPOSED
 TYPICAL SECTIONS

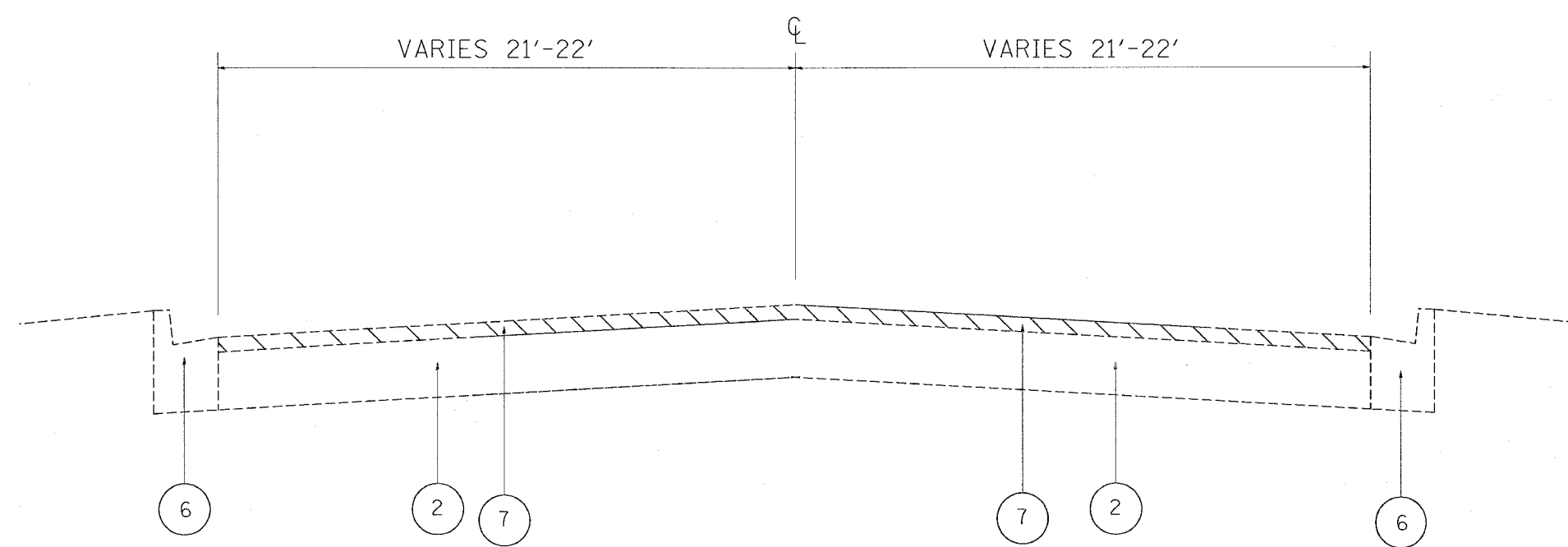
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 HORIZ. _____

DRAWN BY _____
 CHECKED BY _____

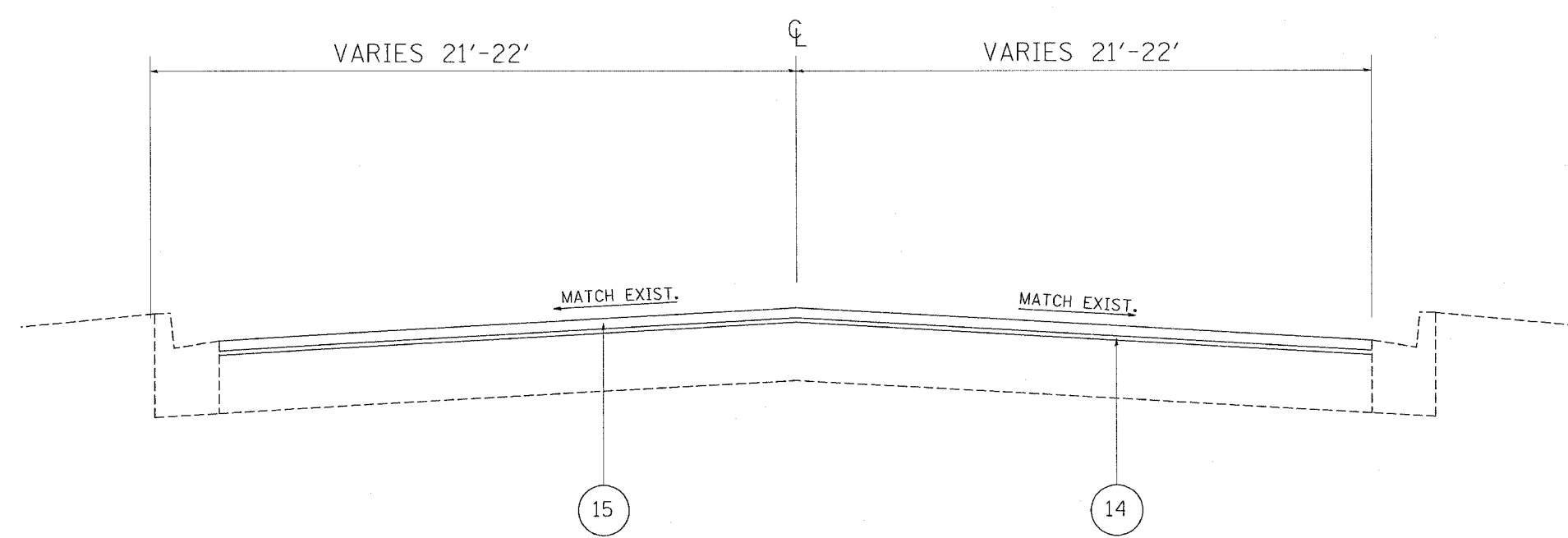
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	9
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 62834



EXISTING TYPICAL SECTION
MANHATTAN-MONEE ROAD
STA. 14046+42 TO STA. 14048+03



PROPOSED TYPICAL SECTION
MANHATTAN-MONEE ROAD
STA. 14046+42 TO STA. 14048+03

LEGEND

- ① EXISTING P.C.C. PAVEMENT, 10.5"
- ② EXISTING BITUMINOUS PAVEMENT, 14"
- ③ EXISTING BITUMINOUS OVERLAY, 4"
- ④ EXISTING BITUMINOUS SHOULDER, 12"
- ⑤ EXISTING GUARDRAIL
- ⑥ EXISTING CURB AND GUTTER
- ⑦ PROPOSED BITUMINOUS SURFACE REMOVAL, 2 1/2 "
- ⑧ PROPOSED PAVEMENT REMOVAL
- ⑨ PROPOSED BITUMINOUS SHOULDER REMOVAL
- ⑩ PROPOSED CURB AND GUTTER REMOVAL
- ⑪ PROPOSED GUARDRAIL REMOVAL
- ⑫ PROPOSED AGGREGATE SUB-GRADE, 12"
- ⑬ PROPOSED BITUMINOUS BINDER COURSE, SUPERPAVE IL-19.0, N90, 8 1/2 "
- ⑭ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50, 3/4 "
- ⑮ PROPOSED POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90, 1 3/4 "
- ⑯ PROPOSED STEEL PLATE BEAM GUARDRAIL, TYPE B
- ⑰ PROPOSED COMBINATION CURB AND GUTTER-TYPE B-6.18
- ⑱ PROPOSED BITUMINOUS SHOULDER, 6"
- ⑲ PROPOSED PGES, 24"
- ⑳ PROPOSED PIPE UNDERDRAINS, 4" (STA 14044+40 TO STA 14046+42)

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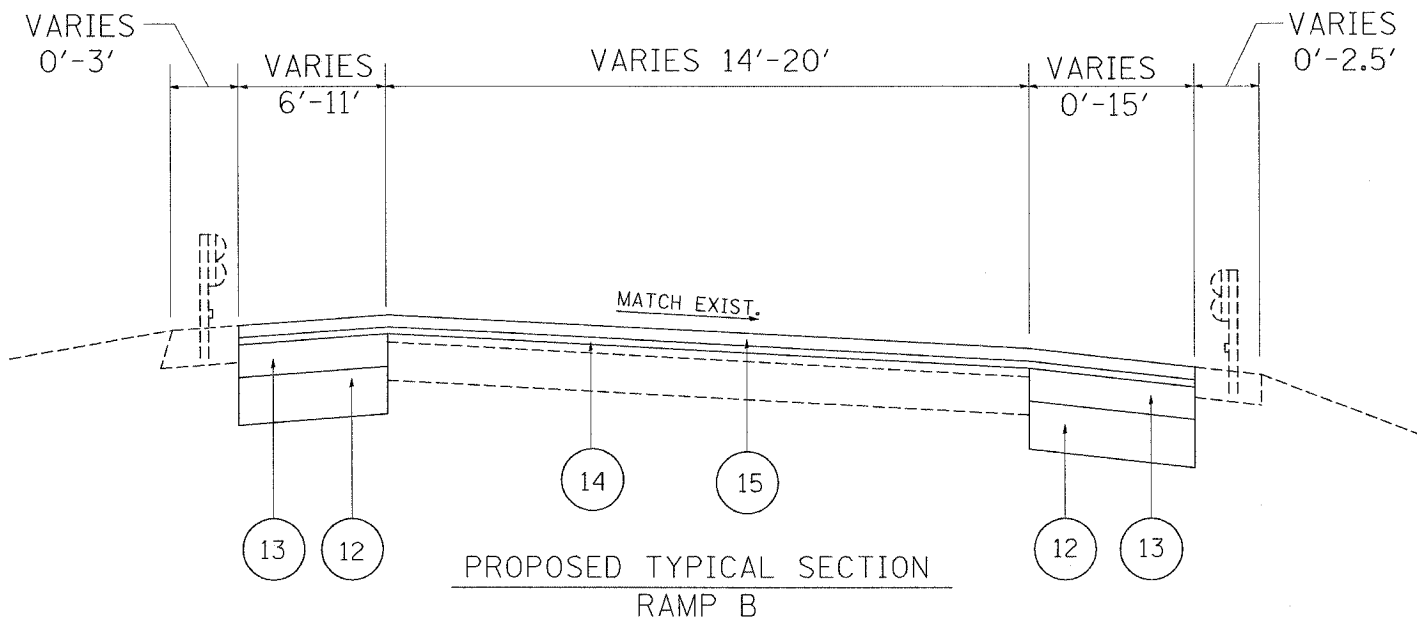
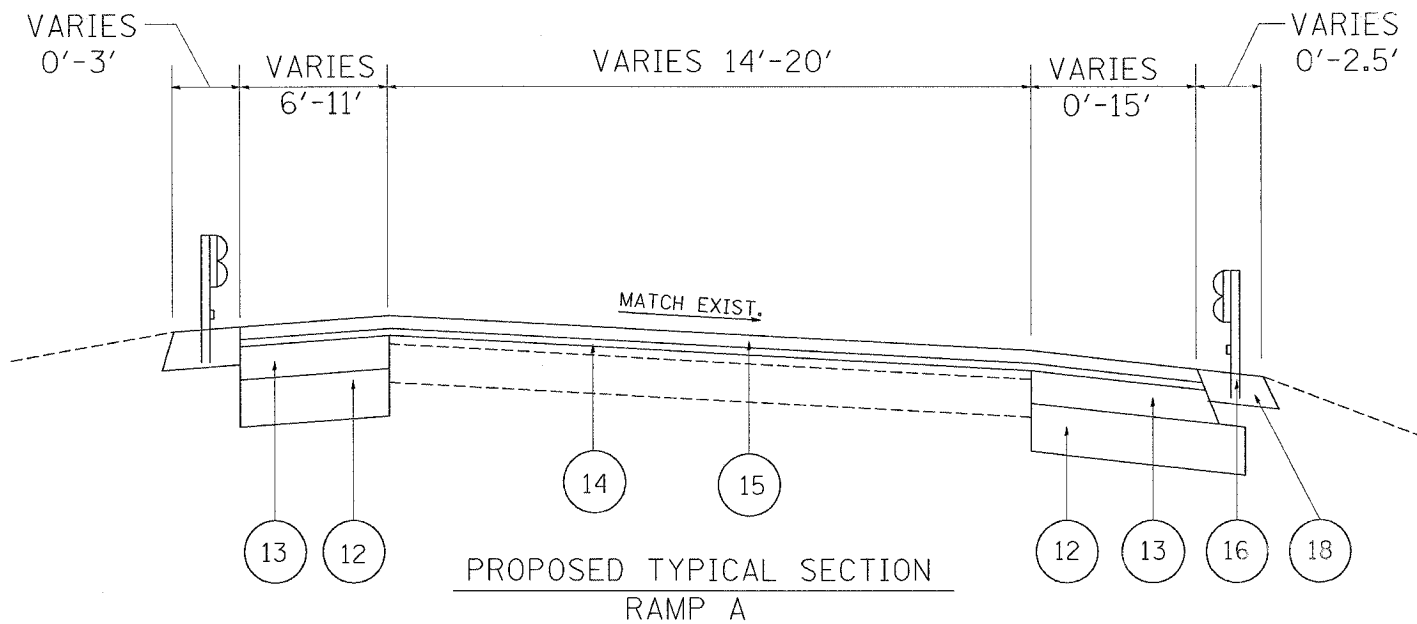
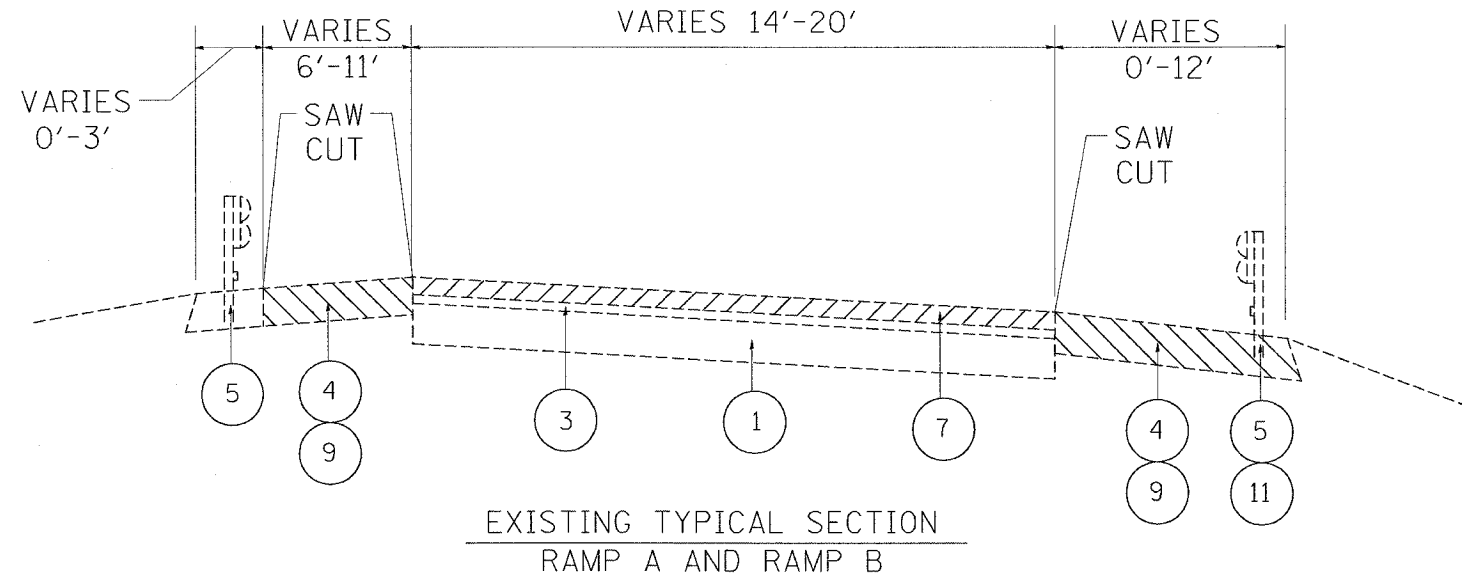
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 FAI 57/ I-57
 @ MANHATTAN-MONEE ROAD
 EXISTING/ PROPOSED
 TYPICAL SECTIONS

SCALE: VERT. _____
 HORIZ. _____
 DATE _____

DRAWN BY _____
 CHECKED BY _____

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	10
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 62834				



LEGEND

- ① EXISTING P.C.C. PAVEMENT, 10.5"
- ② EXISTING BITUMINOUS PAVEMENT, 14"
- ③ EXISTING BITUMINOUS OVERLAY, 4"
- ④ EXISTING BITUMINOUS SHOULDER, 12"
- ⑤ EXISTING GUARDRAIL
- ⑥ EXISTING CURB AND GUTTER
- ⑦ PROPOSED BITUMINOUS SURFACE REMOVAL, 2 1/2 "
- ⑧ PROPOSED PAVEMENT REMOVAL
- ⑨ PROPOSED BITUMINOUS SHOULDER REMOVAL
- ⑩ PROPOSED CURB AND GUTTER REMOVAL
- ⑪ PROPOSED GUARDRAIL REMOVAL
- ⑫ PROPOSED AGGREGATE SUB-GRADE, 12"
- ⑬ PROPOSED BITUMINOUS BINDER COURSE, SUPERPAVE IL-19.0, N90, 8 1/2 "
- ⑭ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50, 3/4 "
- ⑮ PROPOSED POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90, 1 3/4 "
- ⑯ PROPOSED STEEL PLATE BEAM GUARDRAIL, TYPE B
- ⑰ PROPOSED COMBINATION CURB AND GUTTER-TYPE B-6.18
- ⑱ PROPOSED BITUMINOUS SHOULDER, 6"
- ⑲ PROPOSED PGES, 24"
- ⑳ PROPOSED PIPE UNDERDRAINS, 4" (STA 14044+40 TO STA 14046+42)

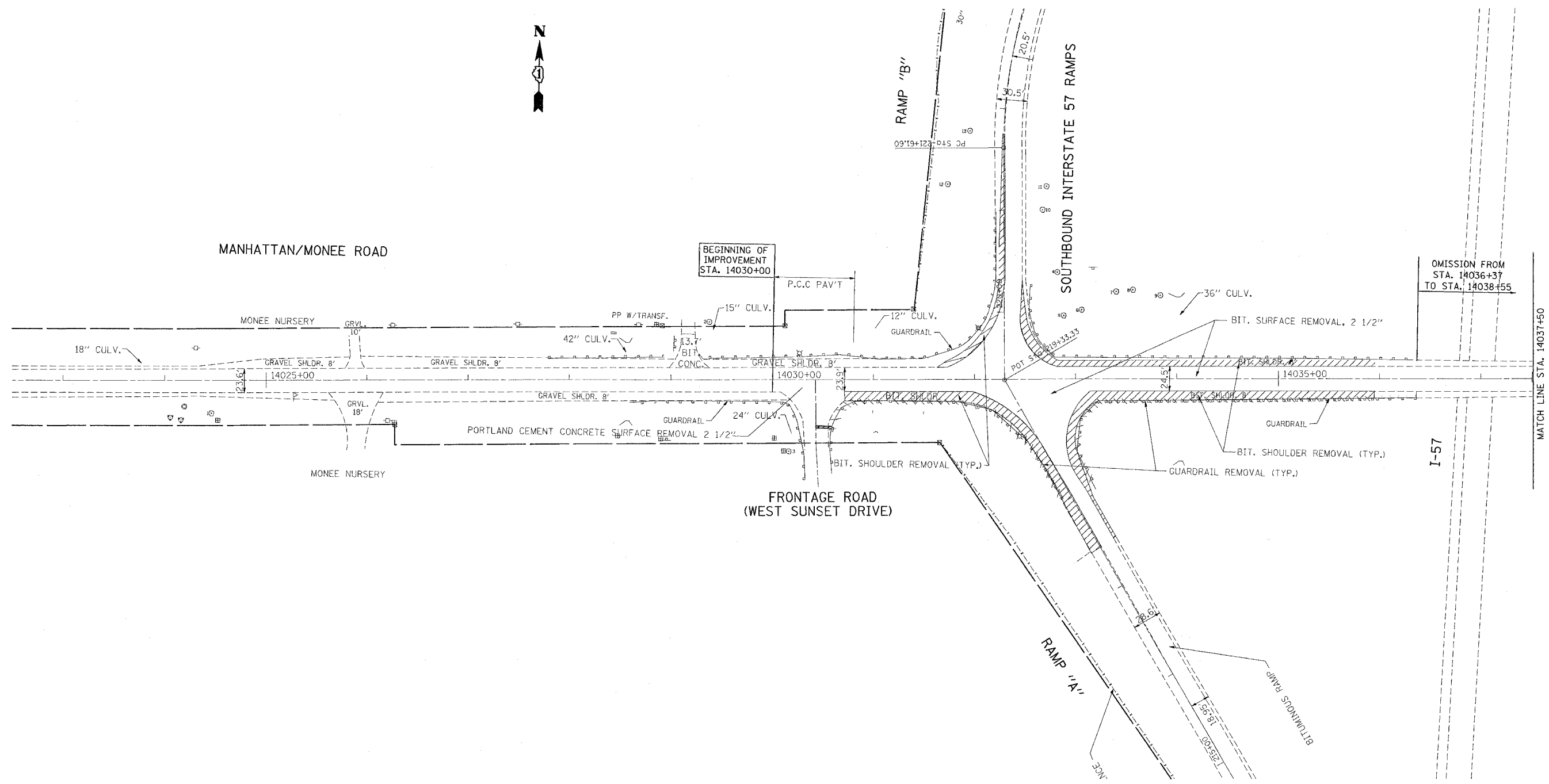
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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 FAI 57/ I-57
 @ MANHATTAN-MONEE ROAD
 EXISTING/ PROPOSED
 TYPICAL SECTIONS
 SCALE: VERT. _____
 HORIZ. _____
 DATE _____ DRAWN BY _____
 CHECKED BY _____

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	11
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

CONTRACT NO. 62834



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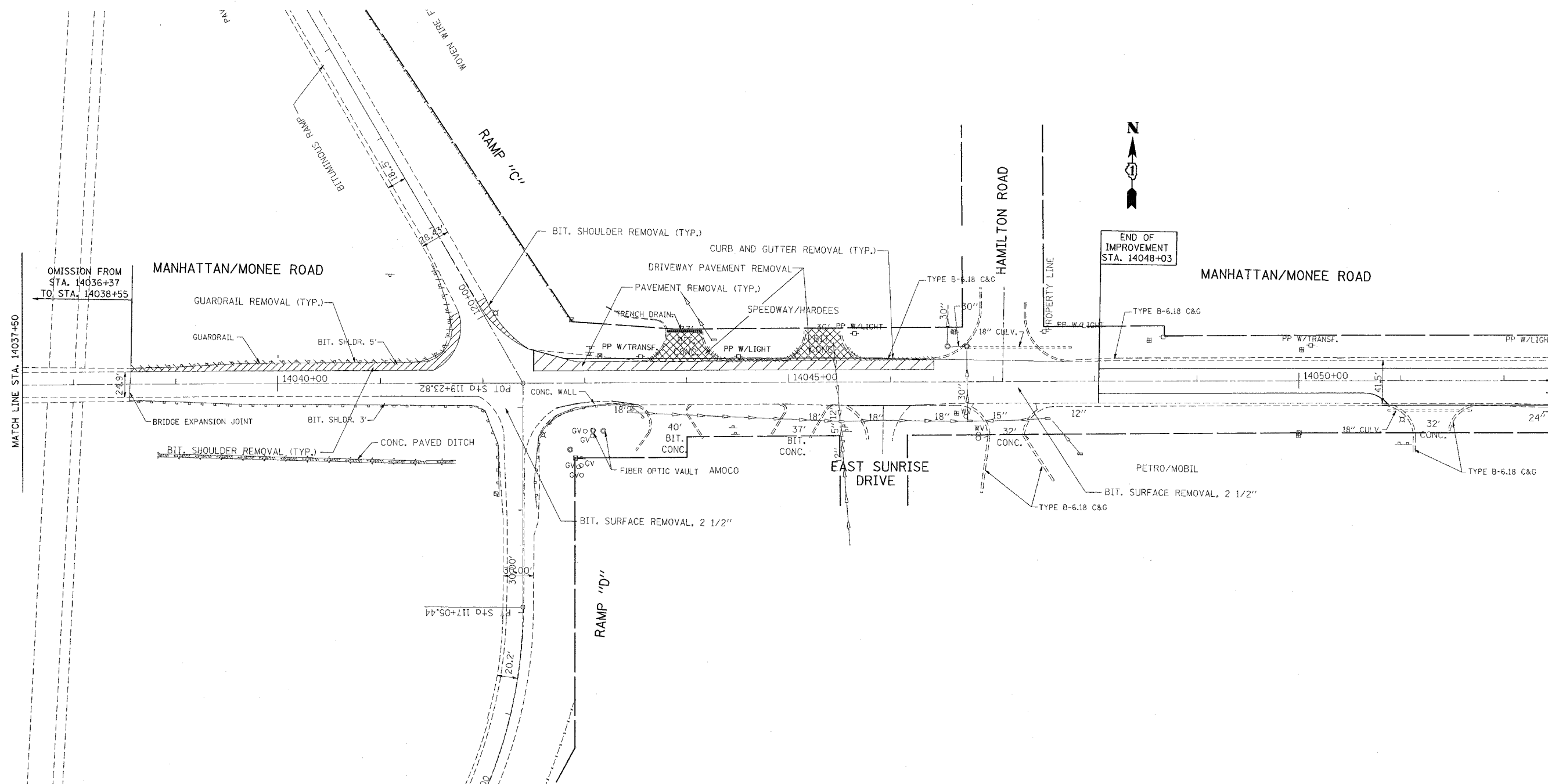
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
FAI 57/ I-57
@ MANHATTAN-MONEE ROAD
EXISTING ROADWAY
SCALE: VERT. 1"=50'
HORIZ. 1"=50'
DATE _____ DRAWN BY _____
CHECKED BY _____

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-I-N	WILL	52	12

STA.	TO STA.
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

CONTRACT NO. 62834



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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

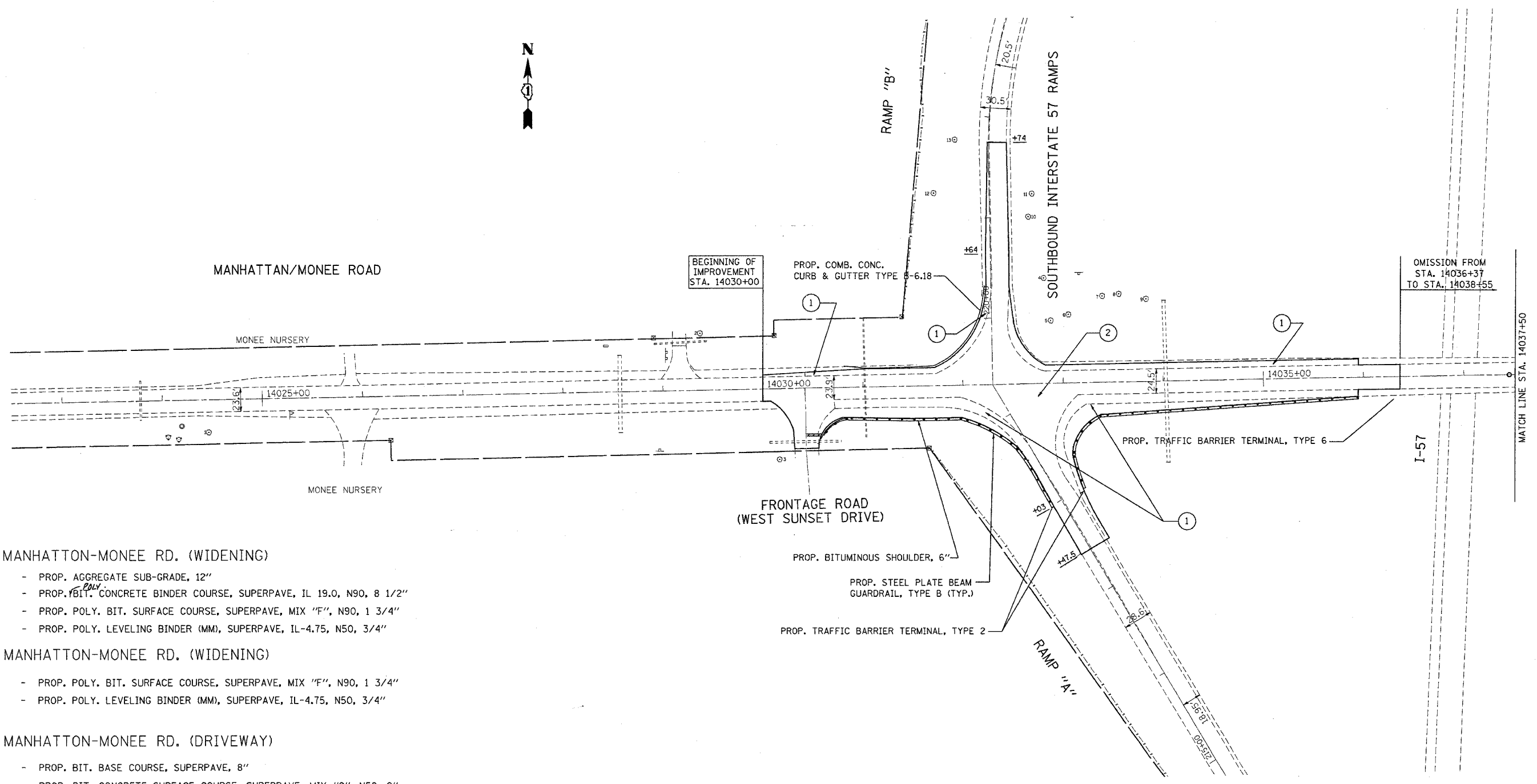
FAI 57/ I-57
 @ MANHATTAN-MONEE ROAD
 EXISTING ROADWAY

SCALE: VERT. 1"=50'
 HORIZ. 1"=50'

DATE _____ DRAWN BY _____
 CHECKED BY _____

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	13
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

CONTRACT NO. 62834



- ① MANHATTAN-MONEE RD. (WIDENING)
 - PROP. AGGREGATE SUB-GRADE, 12"
 - PROP. ²⁰⁴BIT. CONCRETE BINDER COURSE, SUPERPAVE, IL 19.0, N90, 8 1/2"
 - PROP. POLY. BIT. SURFACE COURSE, SUPERPAVE, MIX "F", N90, 1 3/4"
 - PROP. POLY. LEVELING BINDER (MM), SUPERPAVE, IL-4.75, N50, 3/4"
- ② MANHATTAN-MONEE RD. (WIDENING)
 - PROP. POLY. BIT. SURFACE COURSE, SUPERPAVE, MIX "F", N90, 1 3/4"
 - PROP. POLY. LEVELING BINDER (MM), SUPERPAVE, IL-4.75, N50, 3/4"
- ③ MANHATTAN-MONEE RD. (DRIVEWAY)
 - PROP. BIT. BASE COURSE, SUPERPAVE, 8"
 - PROP. BIT. CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50, 2"

PLOT DATE = 9/20/2009
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 REFERENCE = REF#

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAI 57/ I-57
 @ MANHATTAN-MONEE ROAD
 PROPOSED ROADWAY

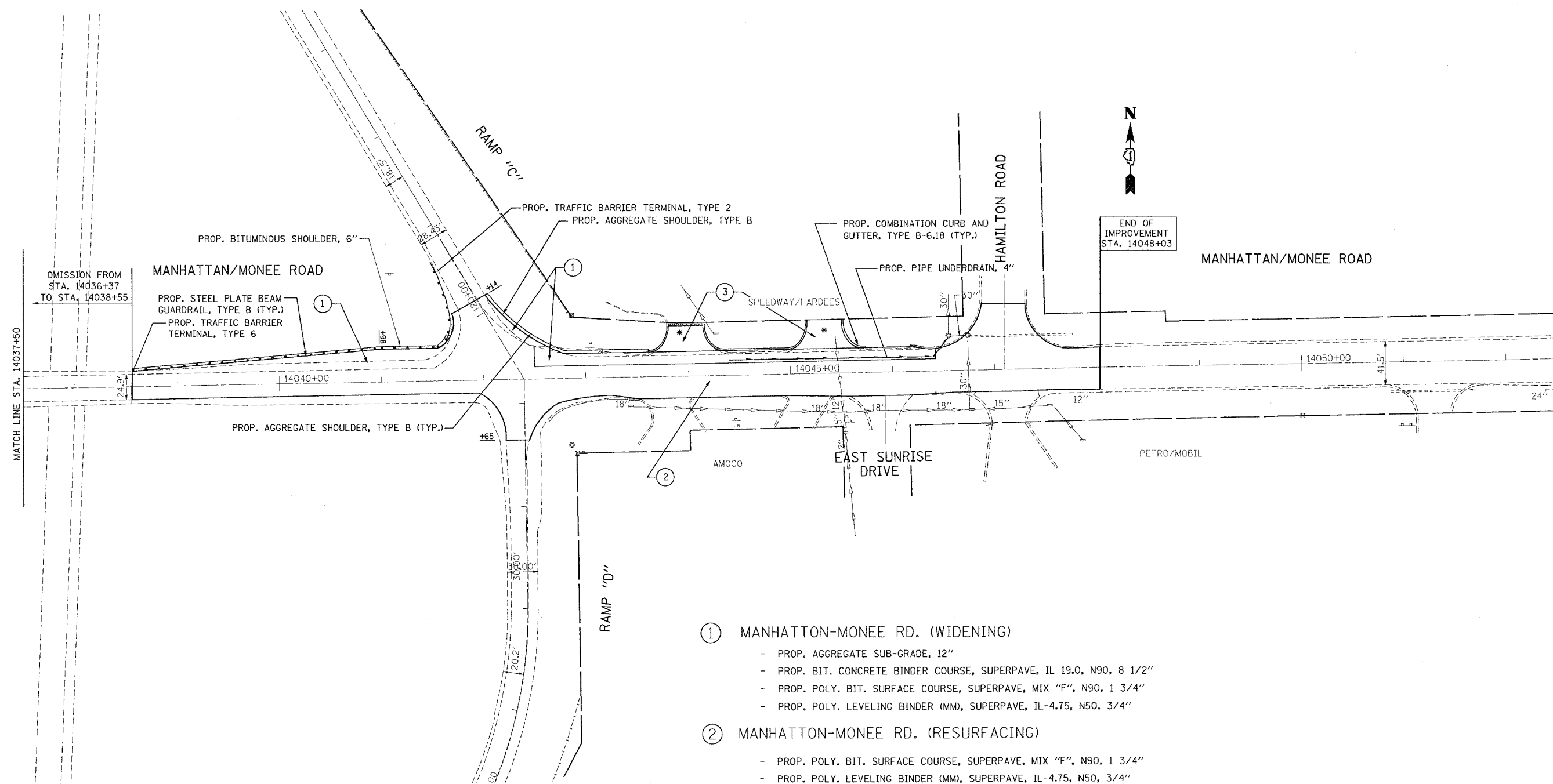
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 HORIZ. DATE

DRAWN BY
 CHECKED BY

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	14

STA.	TO STA.
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

CONTRACT NO. 62834



- ① MANHATTAN-MONEE RD. (WIDENING)
 - PROP. AGGREGATE SUB-GRADE, 12"
 - PROP. BIT. CONCRETE BINDER COURSE, SUPERPAVE, IL 19.0, N90, 8 1/2"
 - PROP. POLY. BIT. SURFACE COURSE, SUPERPAVE, MIX "F", N90, 1 3/4"
 - PROP. POLY. LEVELING BINDER (MM), SUPERPAVE, IL-4.75, N50, 3/4"
 - ② MANHATTAN-MONEE RD. (RESURFACING)
 - PROP. POLY. BIT. SURFACE COURSE, SUPERPAVE, MIX "F", N90, 1 3/4"
 - PROP. POLY. LEVELING BINDER (MM), SUPERPAVE, IL-4.75, N50, 3/4"
 - ③ MANHATTAN-MONEE RD. (DRIVEWAY)
 - PROP. BIT. BASE COURSE, SUPERPAVE, 8"
 - PROP. BIT. CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50, 2"
- * BITUMINOUS DRIVEWAY - COMMERCIAL ENTRANCE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 FAI 57/ I-57
 © MANHATTAN-MONEE ROAD
 PROPOSED ROADWAY

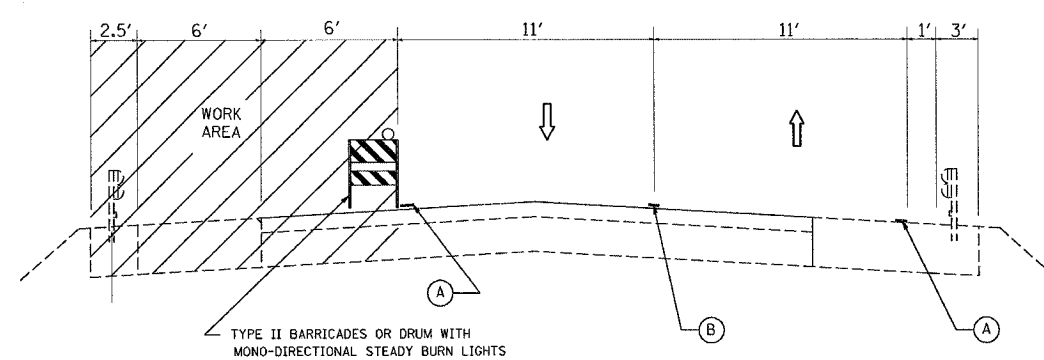
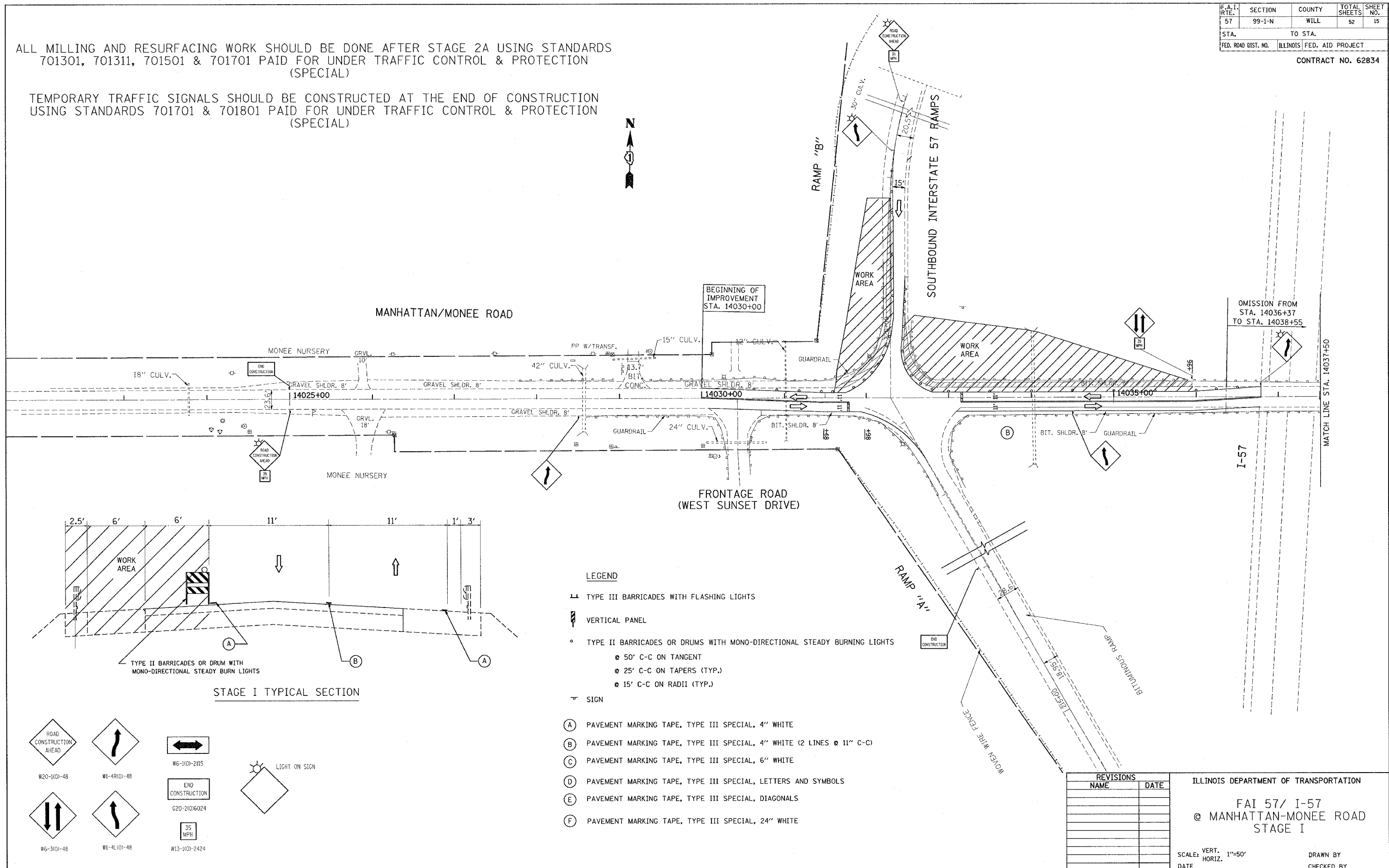
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 CHECKED BY _____

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	15
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62834				

ALL MILLING AND RESURFACING WORK SHOULD BE DONE AFTER STAGE 2A USING STANDARDS 701301, 701311, 701501 & 701701 PAID FOR UNDER TRAFFIC CONTROL & PROTECTION (SPECIAL)

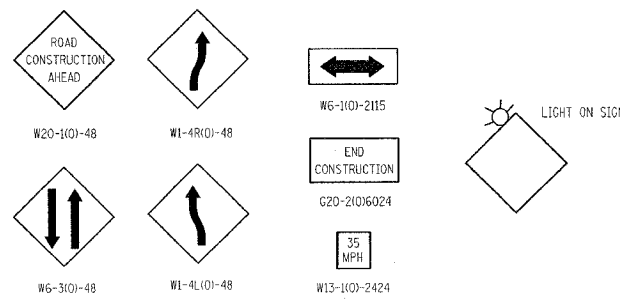
TEMPORARY TRAFFIC SIGNALS SHOULD BE CONSTRUCTED AT THE END OF CONSTRUCTION USING STANDARDS 701701 & 701801 PAID FOR UNDER TRAFFIC CONTROL & PROTECTION (SPECIAL)



STAGE I TYPICAL SECTION

LEGEND

- ⊥ TYPE III BARRICADES WITH FLASHING LIGHTS
- ▨ VERTICAL PANEL
- TYPE II BARRICADES OR DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
 - 50' C-C ON TANGENT
 - 25' C-C ON TAPERS (TYP.)
 - 15' C-C ON RADII (TYP.)
- ⊞ SIGN
- (A) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 4" WHITE
- (B) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 4" WHITE (2 LINES @ 11" C-C)
- (C) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 6" WHITE
- (D) PAVEMENT MARKING TAPE, TYPE III SPECIAL, LETTERS AND SYMBOLS
- (E) PAVEMENT MARKING TAPE, TYPE III SPECIAL, DIAGONALS
- (F) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 24" WHITE



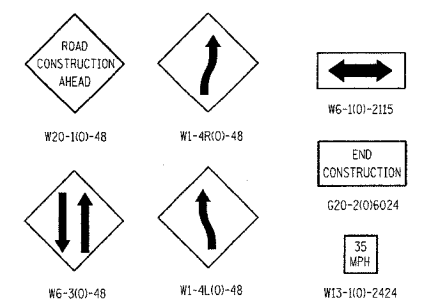
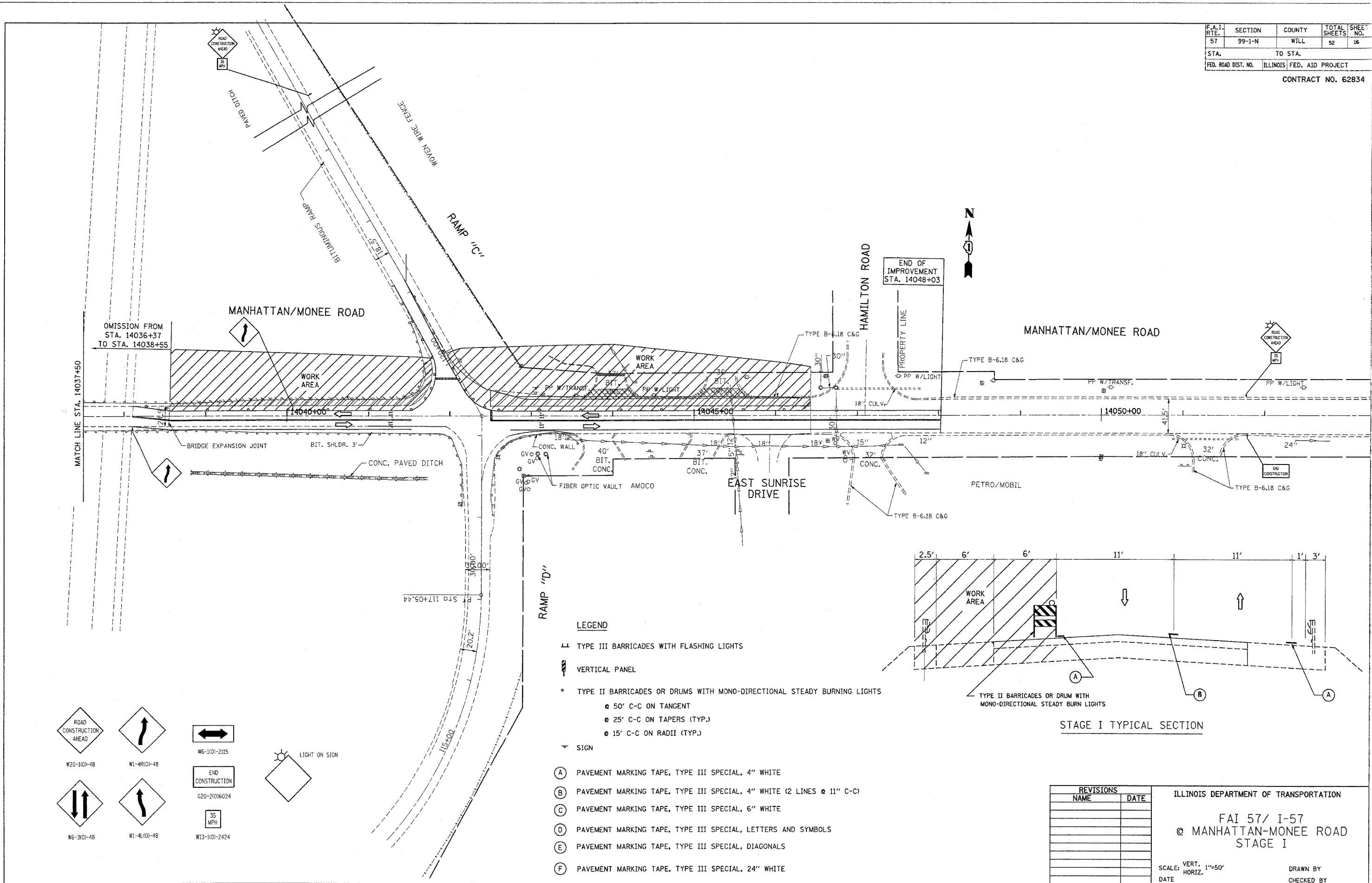
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 FAI 57/ I-57
 @ MANHATTAN-MONEE ROAD
 STAGE I
 SCALE: VERT. 1"=50'
 HORIZ. DATE
 DRAWN BY
 CHECKED BY

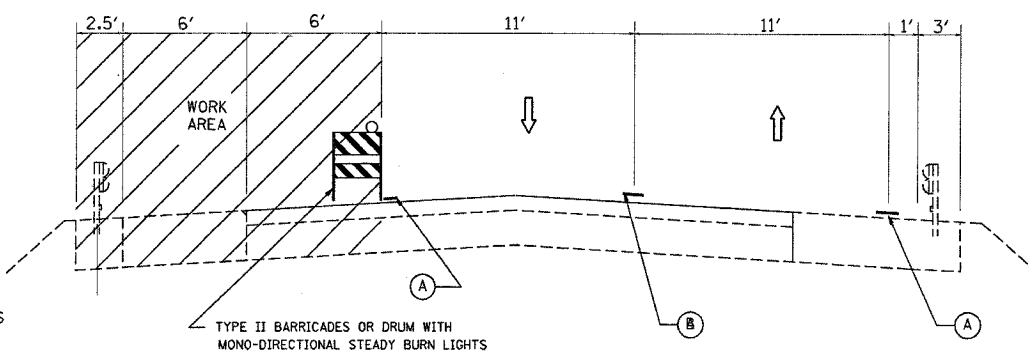
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	16
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

CONTRACT NO. 62834



- LEGEND**
- ▬ TYPE III BARRICADES WITH FLASHING LIGHTS
 - ▬ VERTICAL PANEL
 - TYPE II BARRICADES OR DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
 - 50' C-C ON TANGENT
 - 25' C-C ON TAPERS (TYP.)
 - 15' C-C ON RADII (TYP.)
 - ▬ SIGN
- (A) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 4" WHITE
 - (B) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 4" WHITE (2 LINES @ 11" C-C)
 - (C) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 6" WHITE
 - (D) PAVEMENT MARKING TAPE, TYPE III SPECIAL, LETTERS AND SYMBOLS
 - (E) PAVEMENT MARKING TAPE, TYPE III SPECIAL, DIAGONALS
 - (F) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 24" WHITE



STAGE I TYPICAL SECTION

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 FAI 57/ I-57
 @ MANHATTAN-MONEE ROAD
 STAGE I

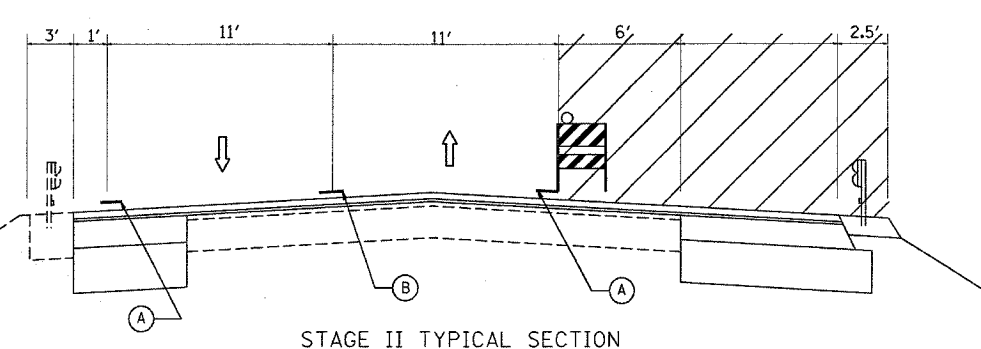
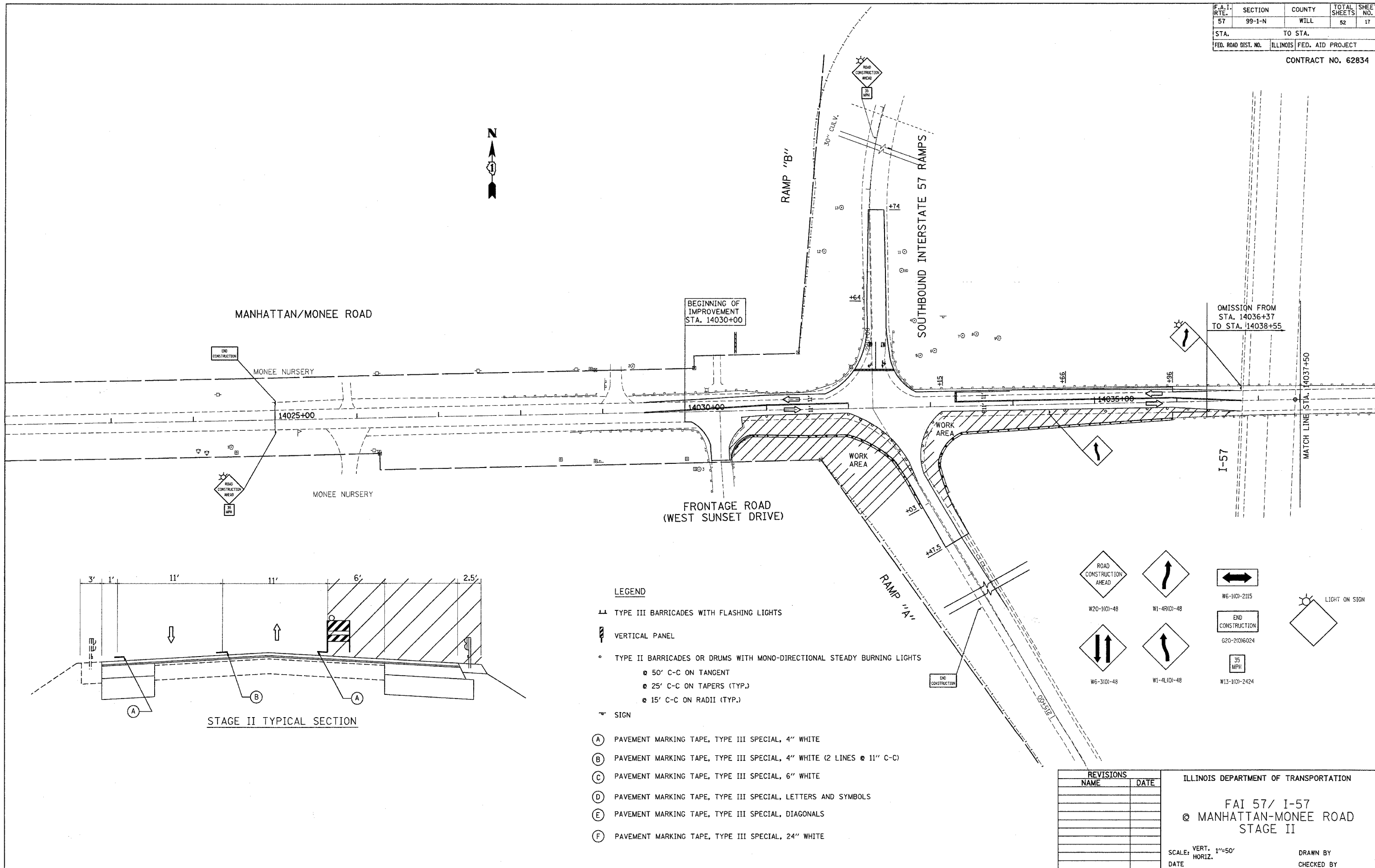
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DATE _____ DRAWN BY _____
 CHECKED BY _____

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	17
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

CONTRACT NO. 62834



- LEGEND**
- ▲ TYPE III BARRICADES WITH FLASHING LIGHTS
 - ▩ VERTICAL PANEL
 - TYPE II BARRICADES OR DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
 - 50' C-C ON TANGENT
 - 25' C-C ON TAPERS (TYP.)
 - 15' C-C ON RADII (TYP.)
 - ▬ SIGN
- (A) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 4" WHITE
 - (B) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 4" WHITE (2 LINES @ 11" C-C)
 - (C) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 6" WHITE
 - (D) PAVEMENT MARKING TAPE, TYPE III SPECIAL, LETTERS AND SYMBOLS
 - (E) PAVEMENT MARKING TAPE, TYPE III SPECIAL, DIAGONALS
 - (F) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 24" WHITE

TRAFFIC SIGN LEGEND:

- ROAD CONSTRUCTION AHEAD (W20-1101-48)
- ROAD CONSTRUCTION AHEAD (W6-1101-2115)
- ONE WAY (W1-4R101-48)
- ONE WAY (W1-4L101-48)
- END OF ROAD AHEAD (G20-21016024)
- END OF ROAD AHEAD (G20-21016024)
- 35 MPH (W13-1101-2424)
- 35 MPH (W6-3101-48)
- ROAD CONSTRUCTION AHEAD (W6-3101-48)
- LIGHT ON SIGN

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAI 57/ I-57
 @ MANHATTAN-MONEE ROAD
 STAGE II

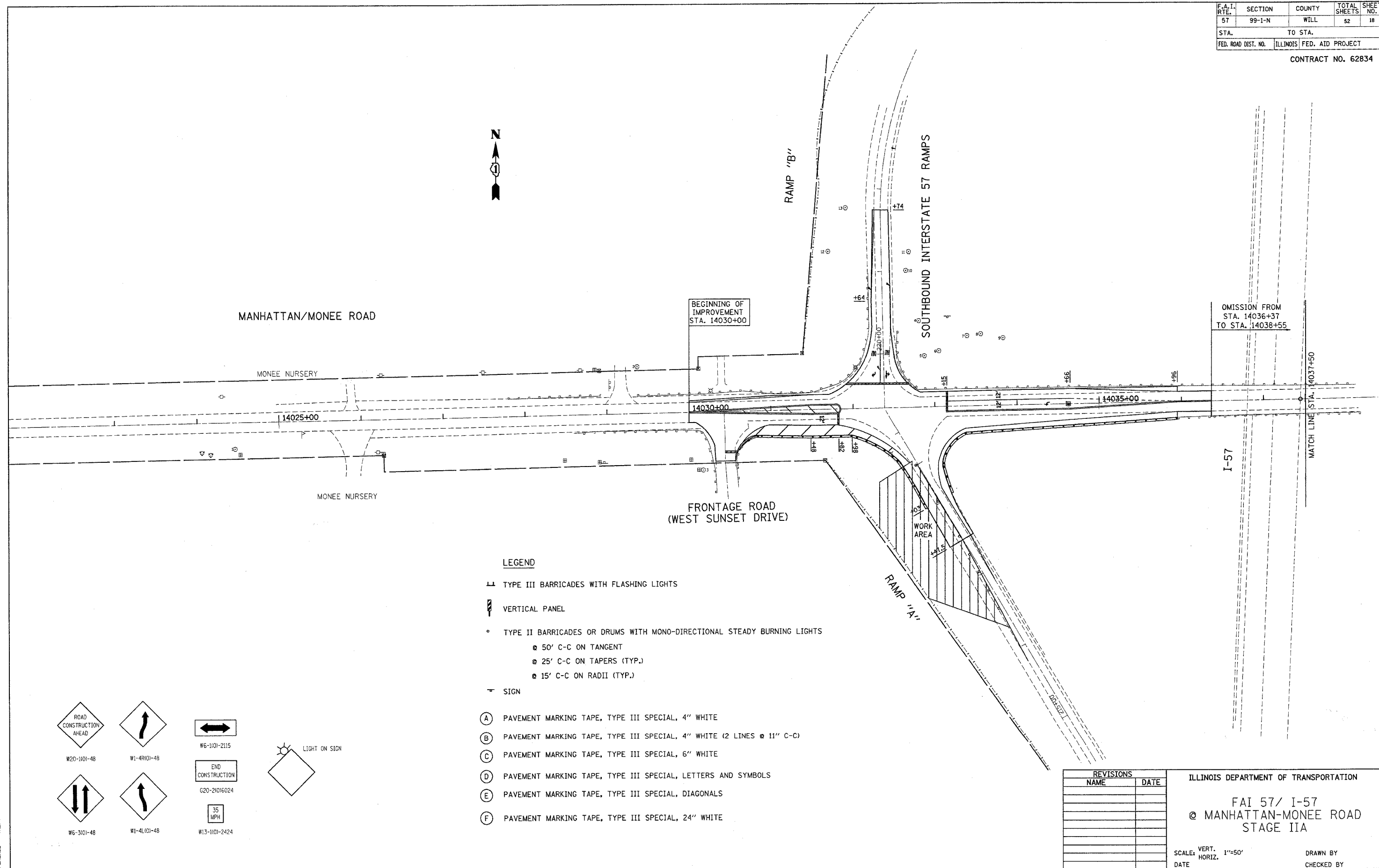
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 HORIZ. 1"=50'

DATE: _____ DRAWN BY: _____
 CHECKED BY: _____

PLOT DATE = 9/28/2005
 PLOT SCALE = 1/8"=1'-0"
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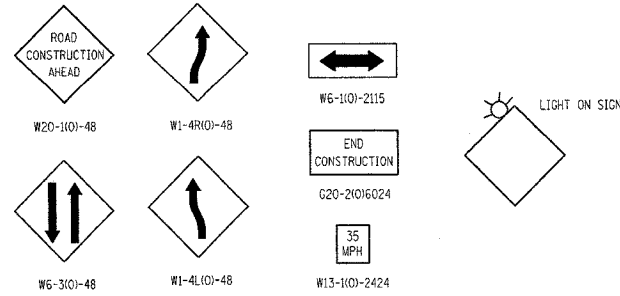
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	18
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

CONTRACT NO. 62834



LEGEND

- ⊥ TYPE III BARRICADES WITH FLASHING LIGHTS
- ▮ VERTICAL PANEL
- TYPE II BARRICADES OR DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
 - 50' C-C ON TANGENT
 - 25' C-C ON TAPERS (TYP.)
 - 15' C-C ON RADII (TYP.)
- ⌄ SIGN
- (A) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 4" WHITE
- (B) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 4" WHITE (2 LINES @ 11" C-C)
- (C) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 6" WHITE
- (D) PAVEMENT MARKING TAPE, TYPE III SPECIAL, LETTERS AND SYMBOLS
- (E) PAVEMENT MARKING TAPE, TYPE III SPECIAL, DIAGONALS
- (F) PAVEMENT MARKING TAPE, TYPE III SPECIAL, 24" WHITE



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAI 57/ I-57
 @ MANHATTAN-MONEE ROAD
 STAGE IIA

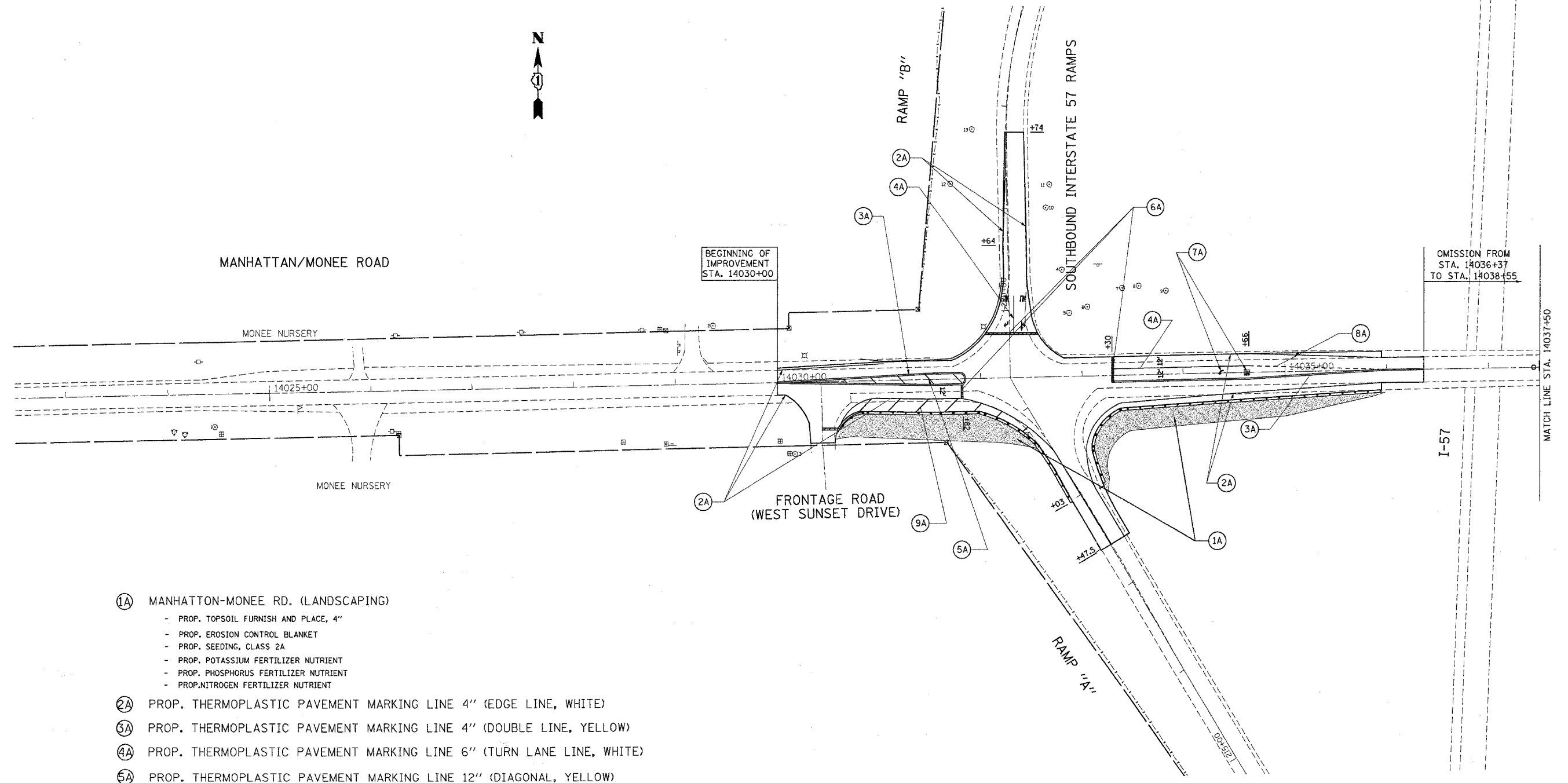
SCALE: VERT. 1"=50'
 HORIZ. DATE

DRAWN BY
 CHECKED BY

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	19
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

CONTRACT NO. 62834



- ①A MANHATTAN-MONEE RD. (LANDSCAPING)
 - PROP. TOPSOIL FURNISH AND PLACE, 4"
 - PROP. EROSION CONTROL BLANKET
 - PROP. SEEDING, CLASS 2A
 - PROP. POTASSIUM FERTILIZER NUTRIENT
 - PROP. PHOSPHORUS FERTILIZER NUTRIENT
 - PROP. NITROGEN FERTILIZER NUTRIENT
- ②A PROP. THERMOPLASTIC PAVEMENT MARKING LINE 4" (EDGE LINE, WHITE)
- ③A PROP. THERMOPLASTIC PAVEMENT MARKING LINE 4" (DOUBLE LINE, YELLOW)
- ④A PROP. THERMOPLASTIC PAVEMENT MARKING LINE 6" (TURN LANE LINE, WHITE)
- ⑤A PROP. THERMOPLASTIC PAVEMENT MARKING LINE 12" (DIAGONAL, YELLOW)
- ⑥A PROP. THERMOPLASTIC PAVEMENT MARKING LINE 24" (STOP BAR, WHITE)
- ⑦A PROP. THERMOPLASTIC PAVEMENT MARKING LETTER AND SYMBOLS (WHITE)
- ⑧A PROP. THERMOPLASTIC PAVEMENT MARKING LINE 6" (DOTTED LINE, WHITE)
- ⑨A PROP. THERMOPLASTIC PAVEMENT MARKING LINE 12" (DIAGONAL, WHITE)

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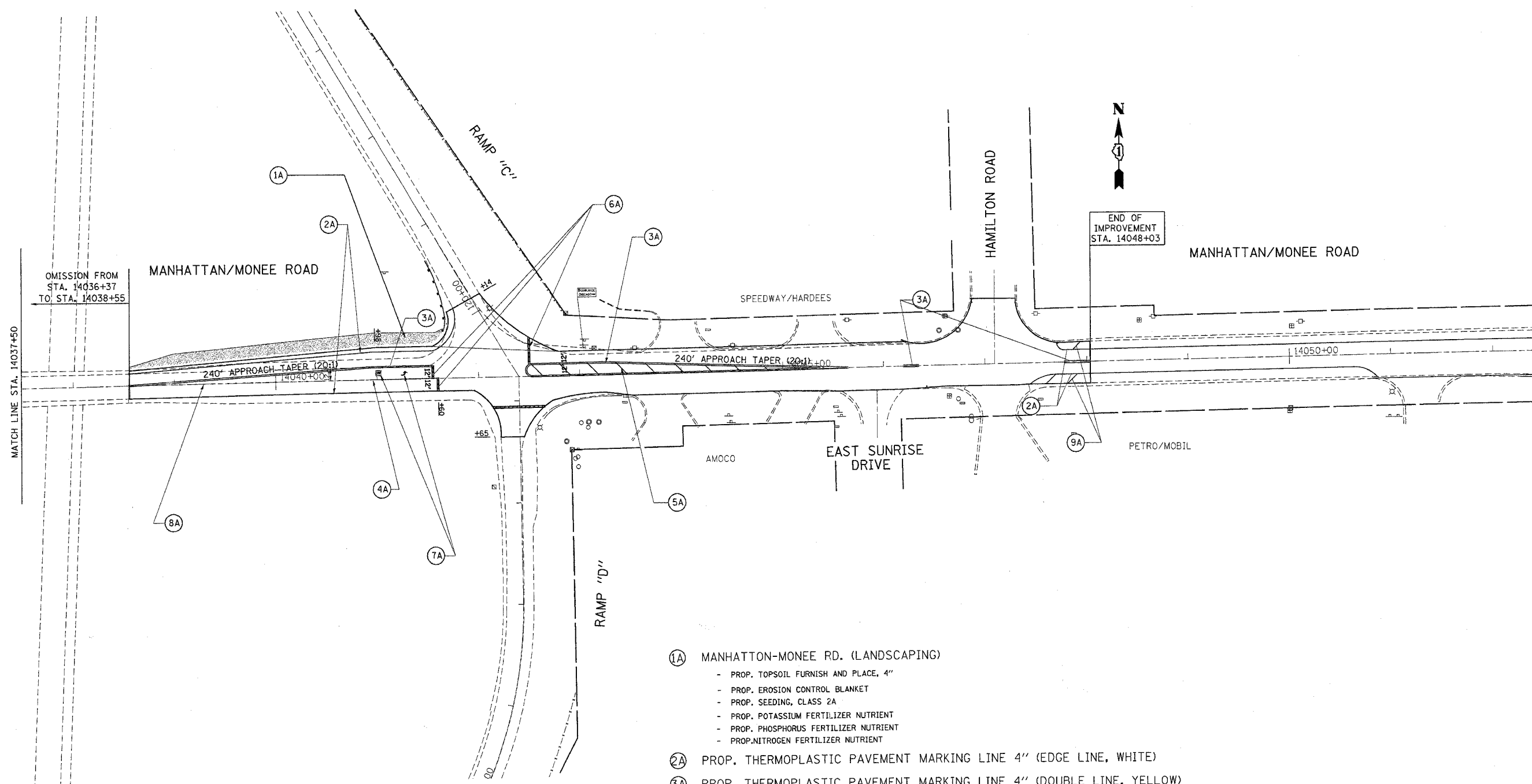
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION	
NAME	DATE		

FAI 57/ I-57
 MANHATTAN-MONEE ROAD
 PROPOSED PAVEMENT MARKING
 AND LANDSCAPING PLAN

SCALE: VERT. 1"=50'
 HORIZ. 1"=50'
 DATE _____ DRAWN BY _____
 CHECKED BY _____

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	20
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

CONTRACT NO. 62834



- ①A MANHATTAN-MONEE RD. (LANDSCAPING)
 - PROP. TOPSOIL FURNISH AND PLACE, 4"
 - PROP. EROSION CONTROL BLANKET
 - PROP. SEEDING, CLASS 2A
 - PROP. POTASSIUM FERTILIZER NUTRIENT
 - PROP. PHOSPHORUS FERTILIZER NUTRIENT
 - PROP. NITROGEN FERTILIZER NUTRIENT
- ②A PROP. THERMOPLASTIC PAVEMENT MARKING LINE 4" (EDGE LINE, WHITE)
- ③A PROP. THERMOPLASTIC PAVEMENT MARKING LINE 4" (DOUBLE LINE, YELLOW)
- ④A PROP. THERMOPLASTIC PAVEMENT MARKING LINE 6" (TURN LANE LINE, WHITE)
- ⑤A PROP. THERMOPLASTIC PAVEMENT MARKING LINE 12" (DIAGONAL, YELLOW)
- ⑥A PROP. THERMOPLASTIC PAVEMENT MARKING LINE 24" (STOP BAR, WHITE)
- ⑦A PROP. THERMOPLASTIC PAVEMENT MARKING LETTER AND SYMBOLS (WHITE)
- ⑧A PROP. THERMOPLASTIC PAVEMENT MARKING LINE 6" (DOTTED LINE, WHITE)
- ⑨A PROP. THERMOPLASTIC PAVEMENT MARKING LINE 12" (DIAGONAL, WHITE)

PLOT DATE = 9/20/2005
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 REFERENCE = #REF#

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 FAI 57/ I-57
 @ MANHATTAN-MONEE ROAD
 PROPOSED PAVEMENT MARKING
 AND LANDSCAPING PLAN

SCALE: VERT. 1"=50'
 HORIZ. 1"=50'

DRAWN BY
 CHECKED BY

CONTRACT NO. 62834				
F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	21
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
D-91-358-04				

Restoration of Work Area. Restoration of the traffic signal work area shall be incidental to the related pay item such as foundation, conduit, handhole, trench and backfill, etc., and no extra compensation shall be allowed. All roadway surfaces such as shoulders, median, sidewalks, pavement, etc. shall be replaced in kind. All damage to mowed lawns shall be replaced with an approved sod, and all damage to unmowed fields shall be seeded in accordance with Standard Specifications 252 and 250 respectively.

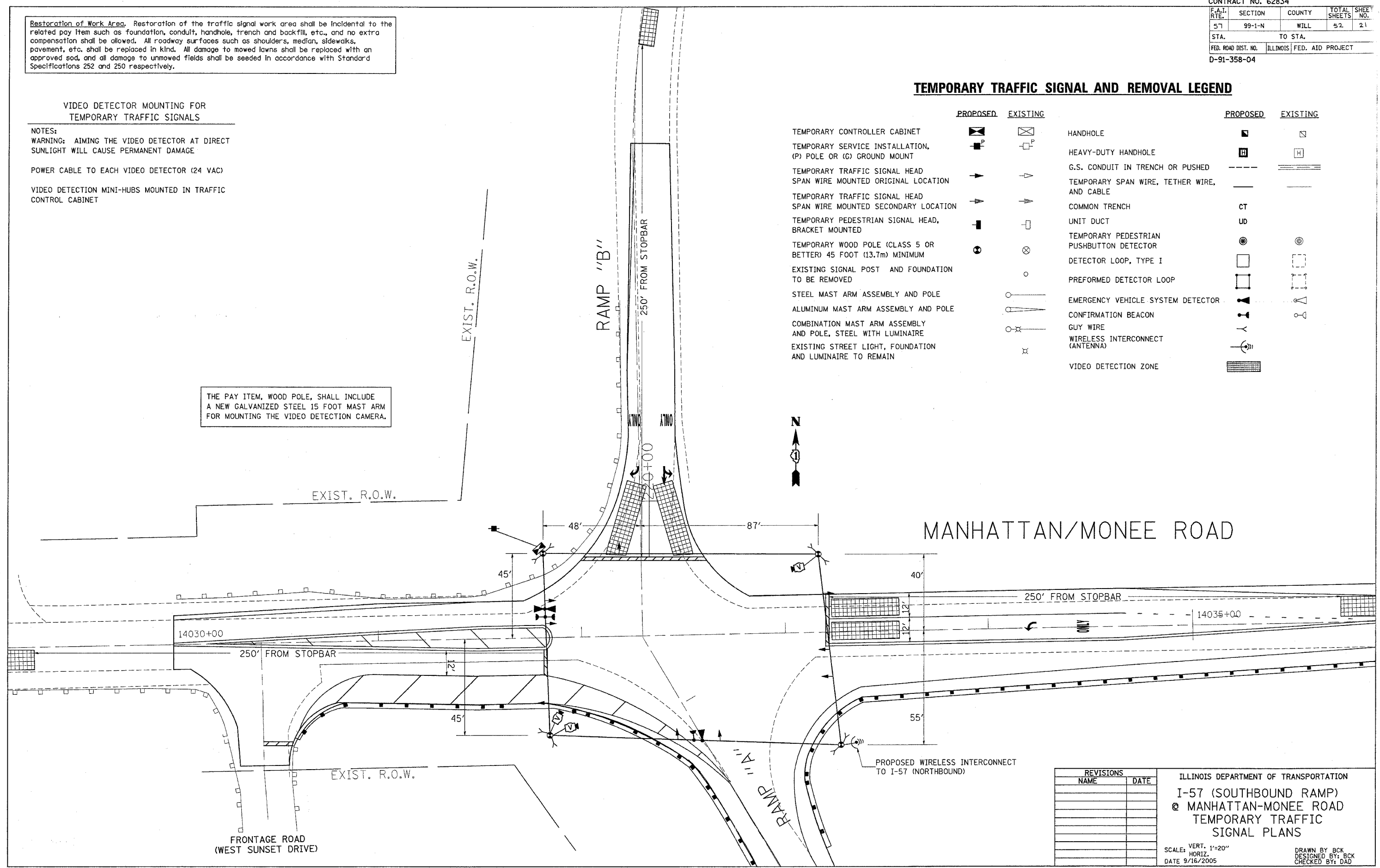
VIDEO DETECTOR MOUNTING FOR TEMPORARY TRAFFIC SIGNALS

- NOTES:
- WARNING: AIMING THE VIDEO DETECTOR AT DIRECT SUNLIGHT WILL CAUSE PERMANENT DAMAGE
 - POWER CABLE TO EACH VIDEO DETECTOR (24 VAC)
 - VIDEO DETECTION MINI-HUBS MOUNTED IN TRAFFIC CONTROL CABINET

THE PAY ITEM, WOOD POLE, SHALL INCLUDE A NEW GALVANIZED STEEL 15 FOOT MAST ARM FOR MOUNTING THE VIDEO DETECTION CAMERA.

TEMPORARY TRAFFIC SIGNAL AND REMOVAL LEGEND

	PROPOSED	EXISTING		PROPOSED	EXISTING
TEMPORARY CONTROLLER CABINET			HANDHOLE		
TEMPORARY SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT			HEAVY-DUTY HANDHOLE		
TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION			G.S. CONDUIT IN TRENCH OR PUSHED		
TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION			TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE		
TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED			COMMON TRENCH	CT	
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM			UNIT DUCT	UD	
EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED			TEMPORARY PEDESTRIAN PUSHBUTTON DETECTOR		
STEEL MAST ARM ASSEMBLY AND POLE			DETECTOR LOOP, TYPE I		
ALUMINUM MAST ARM ASSEMBLY AND POLE			PREFORMED DETECTOR LOOP		
COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE			EMERGENCY VEHICLE SYSTEM DETECTOR		
EXISTING STREET LIGHT, FOUNDATION AND LUMINAIRE TO REMAIN			CONFIRMATION BEACON		
			GUY WIRE		
			WIRELESS INTERCONNECT (ANTENNA)		
			VIDEO DETECTION ZONE		



PLOT DATE = 9/16/2005
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 USER NAME = kent@pco.com

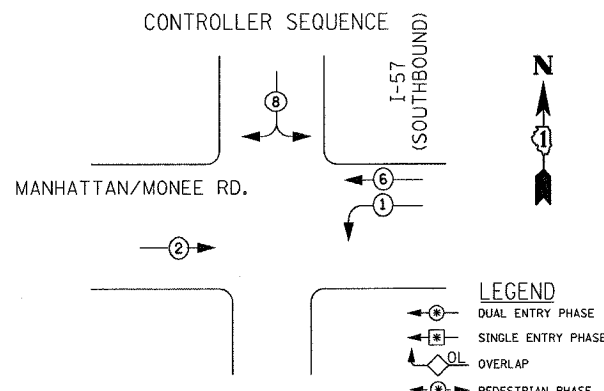
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 I-57 (SOUTHBOUND RAMP)
 @ MANHATTAN-MONEE ROAD
 TEMPORARY TRAFFIC SIGNAL PLANS

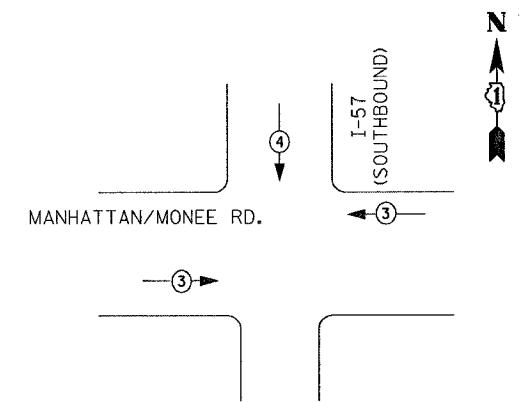
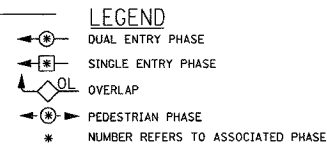
SCALE: VERT. 1"=20"
 HORIZ. DATE 9/16/2005

DRAWN BY: BCK
 DESIGNED BY: BCK
 CHECKED BY: DAD

CONTRACT NO. 62834				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	22
STA.	TO STA.			
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
D-91-358-04				



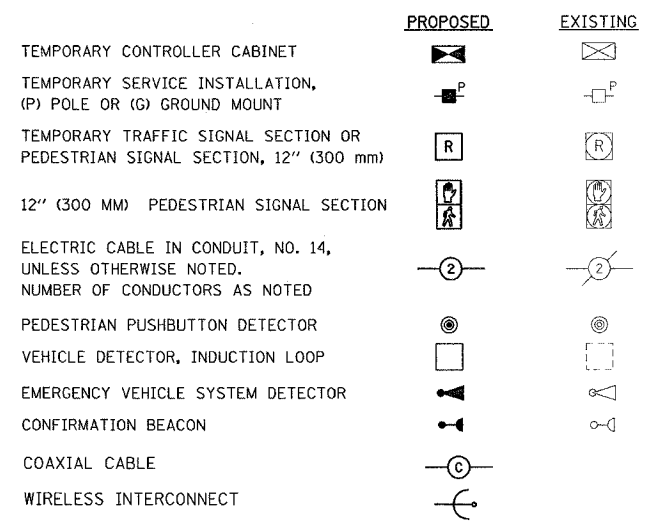
TEMPORARY PHASE DESIGNATION DIAGRAM



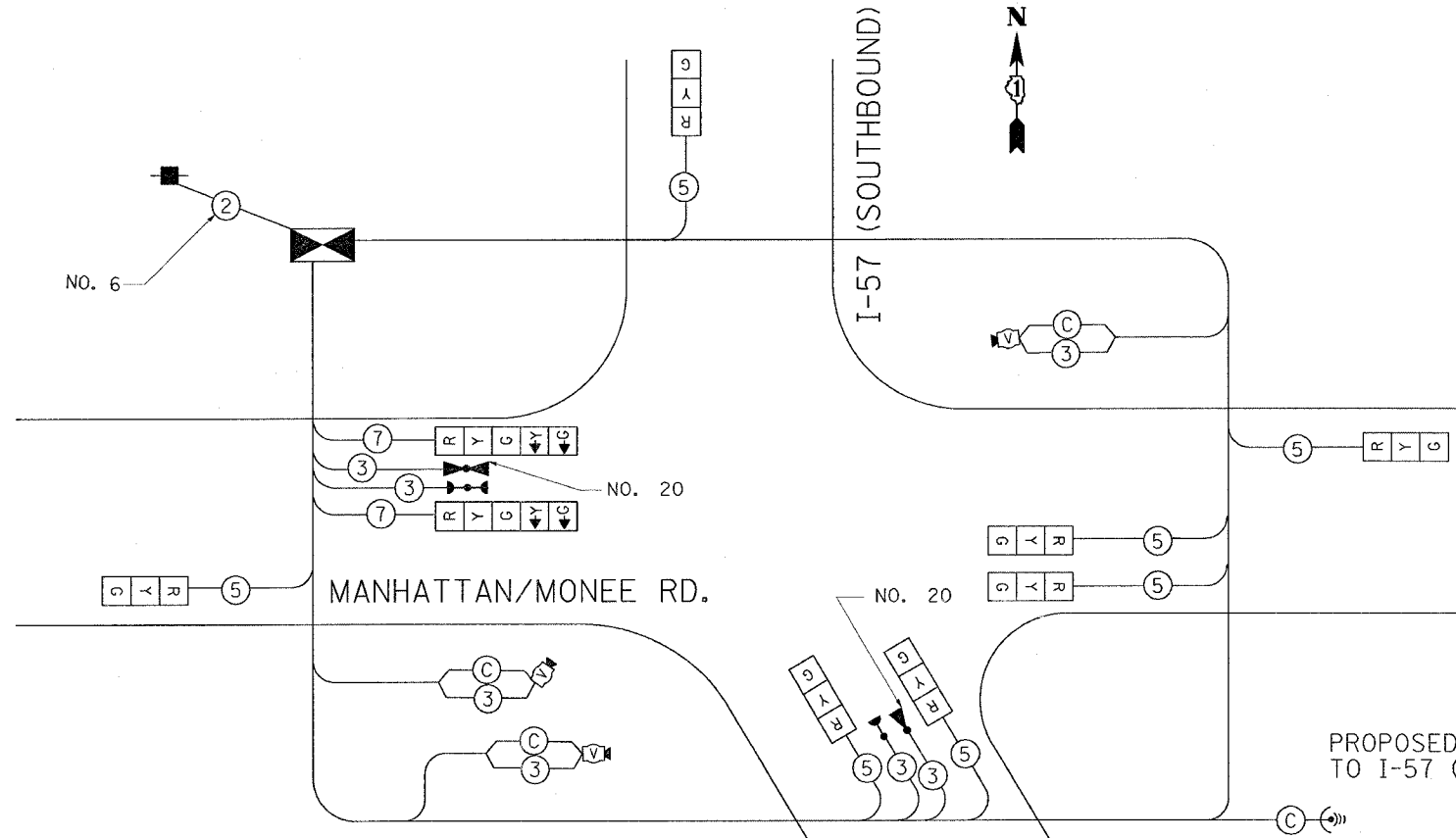
EMERGENCY VEHICLE PREEMPTION SEQUENCE

PROPOSED EMERGENCY VEHICLE PREEMPTION		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	→	↓

TEMPORARY CABLE DIAGRAM LEGEND



* 100% COST TO MONEE FIRE PROTECTION DISTRICT



CABLE PLAN

THE PAY ITEM, WOOD POLE, SHALL INCLUDE A NEW GALVANIZED STEEL 15 FOOT MAST ARM FOR MOUNTING THE VIDEO DETECTION CAMERA.

SCHEDULE OF QUANTITIES

ITEM	UNIT	QUANTITY
VIDEO DETECTION SYSTEM (COMPLETE INTERSECTION)	EACH	1
FULL-ACTUATED CONTROLLER IN TYPE IV CABINET	EACH	1
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 3C	FOOT	320
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 5C	FOOT	950
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 7C	FOOT	75
ELECTRIC CABLE AERIAL SUSPENDED, NO. 20 3/C, TWISTED, SHIELDED	FOOT	390
ELECTRIC CABLE AERIAL SUSPENDED, SERVICE, NO. 6 2C	FOOT	40
WOOD POLE, 40 FT., CLASS 2	EACH	4
LIGHT DETECTOR AMPLIFIER	EACH	1
LIGHT DETECTOR	EACH	2
SIGNAL HEAD, 1-FACE, 3-SECTION	EACH	7
SIGNAL HEAD, 1-FACE, 5-SECTION	EACH	2
SERVICE INSTALLATION, POLE MOUNTED	EACH	1
SPAN WIRE	FOOT	430
TETHER WIRE	FOOT	430

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	INCAND.	LED	OPERATION	
SIGNAL (RED)	9	135	17	0.50	607.50
(YELLOW)	9	135	25	0.25	303.50
(GREEN)	9	135	15	0.25	303.50
ARROW	2	135	12	0.10	27.00
PED. SIGNAL		90	25	1.00	
CONTROLLER	1	100	100	1.00	100.00
ILLUM. SIGN		84		0.05	
FLASHER				0.50	
ENERGY COSTS TO:				TOTAL =	1341.50

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	VERTICAL	FT. (m)
TYPE A - POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS	3.5 (1.0)
D - CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	20'±L-2'
E - M. ARM POLE		SIGNAL POST	2 (1.0)	(6m±L-0.6m)±	
	24" (600mm)	CONTROLLER CAB.	1 (0.5)	BRACKET MOUNTED	13 (4.0)
	30" (750mm)	FIBER OPTIC	13 (4.0)	PED. PUSHBUTTON	4 (1.2)
	36" (900mm)	ELECTRIC SERVICE	1 (0.5)	ELECTRIC SERVICE	13.5 (4.1)
		GROUND CABLE	1 (0.5)	SERVICE TO GROUND	13.5 (4.1)
				POST MOUNTED	6 (1.8)

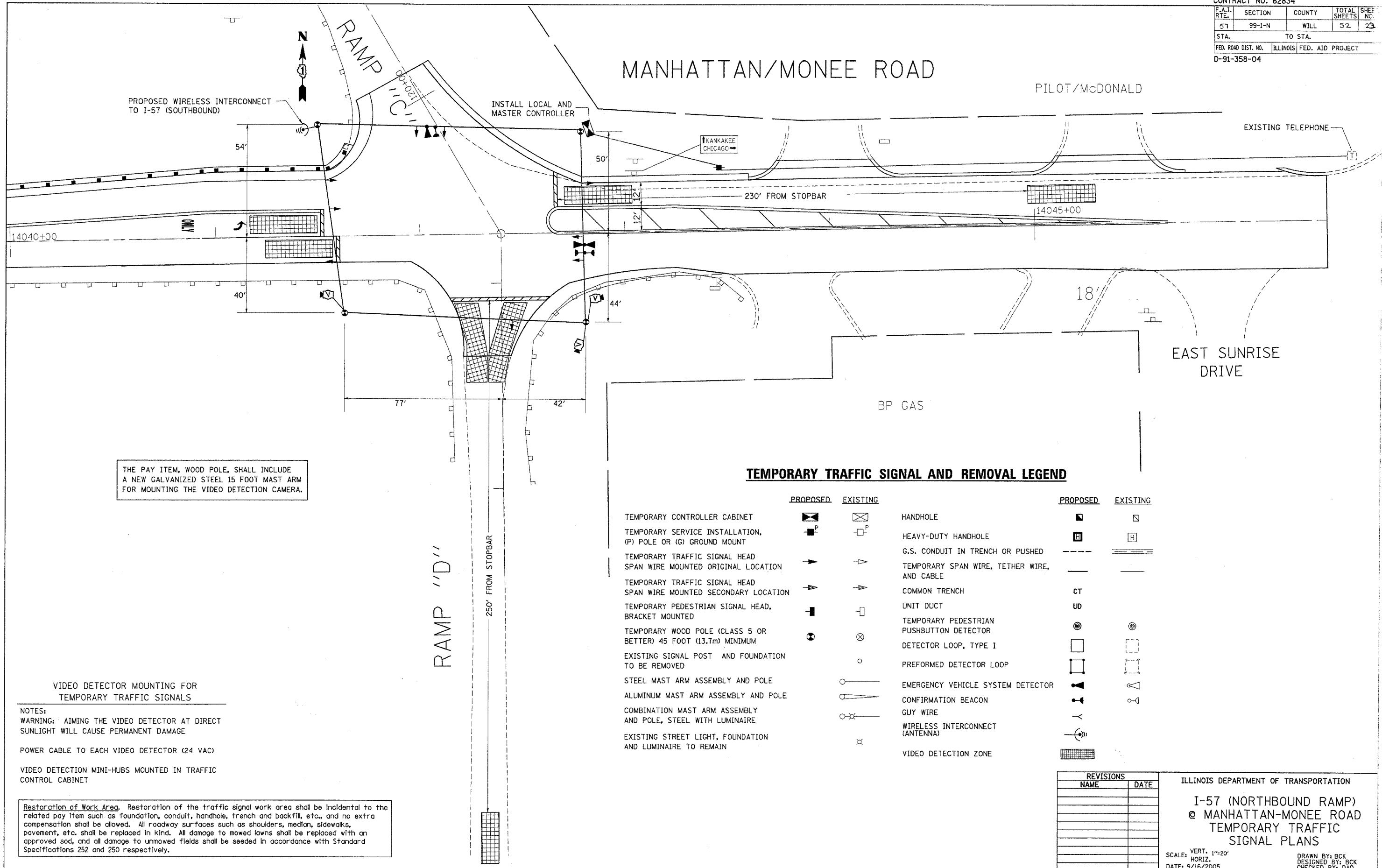


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TEMPORARY CABLE PLAN, PHASE DESIGNATION
 DIAGRAM,
 AND
 SCHEDULE OF QUANTITIES
 (SOUTHBOUND RAMP)
 IL. RTE. 57 @ MANHATTAN/MONEE RD.
 SCALE: 1"=20'
 DATE 10/5/2005
 DRAWN BY BCK
 DESIGN BY BCK
 CHECKED BY DAD

10/5/2005
 c:\projects\td135804\tr\fflcm32
 kanthaphixaybc

CONTRACT NO. 62834				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	23
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
D-91-358-04				



THE PAY ITEM, WOOD POLE, SHALL INCLUDE A NEW GALVANIZED STEEL 15 FOOT MAST ARM FOR MOUNTING THE VIDEO DETECTION CAMERA.

TEMPORARY TRAFFIC SIGNAL AND REMOVAL LEGEND

	PROPOSED	EXISTING		PROPOSED	EXISTING
TEMPORARY CONTROLLER CABINET			HANDHOLE		
TEMPORARY SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT			HEAVY-DUTY HANDHOLE		
TEMPORARY TRAFFIC SIGNAL HEAD			G.S. CONDUIT IN TRENCH OR PUSHED		
SPAN WIRE MOUNTED ORIGINAL LOCATION			TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE		
TEMPORARY TRAFFIC SIGNAL HEAD			COMMON TRENCH	CT	
SPAN WIRE MOUNTED SECONDARY LOCATION			UNIT DUCT	UD	
TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED			TEMPORARY PEDESTRIAN PUSHBUTTON DETECTOR		
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM			DETECTOR LOOP, TYPE I		
EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED			PREFORMED DETECTOR LOOP		
STEEL MAST ARM ASSEMBLY AND POLE			EMERGENCY VEHICLE SYSTEM DETECTOR		
ALUMINUM MAST ARM ASSEMBLY AND POLE			CONFIRMATION BEACON		
COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE			GUY WIRE		
EXISTING STREET LIGHT, FOUNDATION AND LUMINAIRE TO REMAIN			WIRELESS INTERCONNECT (ANTENNA)		
			VIDEO DETECTION ZONE		

VIDEO DETECTOR MOUNTING FOR TEMPORARY TRAFFIC SIGNALS

- NOTES:
- WARNING: AIMING THE VIDEO DETECTOR AT DIRECT SUNLIGHT WILL CAUSE PERMANENT DAMAGE
- POWER CABLE TO EACH VIDEO DETECTOR (24 VAC)
- VIDEO DETECTION MINI-HUBS MOUNTED IN TRAFFIC CONTROL CABINET

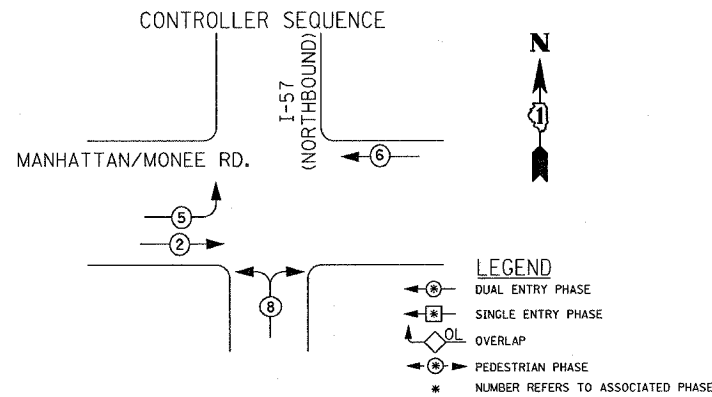
Restoration of Work Area. Restoration of the traffic signal work area shall be incidental to the related pay item such as foundation, conduit, handhole, trench and backfill, etc., and no extra compensation shall be allowed. All roadway surfaces such as shoulders, median, sidewalks, pavement, etc. shall be replaced in kind. All damage to mowed lawns shall be replaced with an approved sod, and all damage to unmowed fields shall be seeded in accordance with Standard Specifications 252 and 250 respectively.

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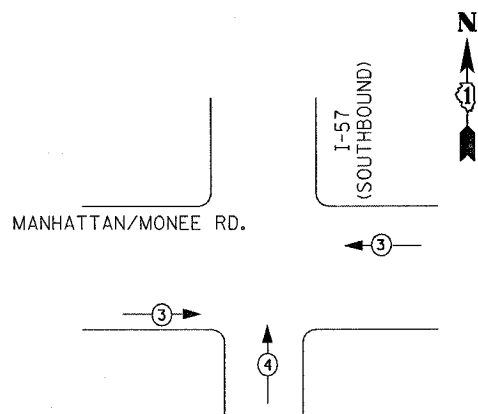
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 I-57 (NORTHBOUND RAMP)
 @ MANHATTAN-MONEE ROAD
 TEMPORARY TRAFFIC SIGNAL PLANS
 SCALE: VERT. 1"=20'
 HORIZ.
 DATE: 9/16/2005
 DRAWN BY: BCK
 DESIGNED BY: BCK
 CHECKED BY: DAD

CONTRACT NO. 62834				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	24
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
D-91-358-04				



TEMPORARY PHASE DESIGNATION DIAGRAM



EMERGENCY VEHICLE PREEMPTION SEQUENCE

PROPOSED EMERGENCY VEHICLE PREEMPTION		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	←	↑

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE (INCAND.)	LED	% OPERATION	
SIGNAL (RED)	17	135	17	0.50	144.50
(YELLOW)	17	135	25	0.25	106.25
(GREEN)	17	135	15	0.25	63.75
ARROW		135	12	0.10	
PED. SIGNAL		90	25	1.00	
CONTROLLER	1	100	100	1.00	100.00
ILLUM. SIGN		84		0.05	
FLASHER				0.50	
ENERGY COSTS TO:				TOTAL =	314.50

ILLINOIS DEPARTMENT OF TRANSPORTATION
 201 WEST CENTER COURT
 SCHALMBURG, ILLINOIS 60196-1096
 CONTACT: JERRY SZEPELAK
 PHONE: (815) 482-9109
 COMPANY: COM. EDISON

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	VERTICAL	FT. (m)
TYPE A - POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS	3.5 (1.0)
D - CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	20'±L-2=
E - M. ARM POLE		SIGNAL POST	2 (1.0)	6m±L-0.6m=	
24" (600mm)	10 (3.0)	CONTROLLER CAB.	1 (0.5)	BRACKET MOUNTED	13 (4.0)
30" (750mm)	15 (4.6)	FIBER OPTIC	13 (4.0)	PED. PUSHBUTTON	4 (1.2)
36" (900mm)	15 (4.6)	ELECTRIC SERVICE	1 (0.5)	ELECTRIC SERVICE	13.5 (4.1)
		GROUND CABLE	1 (0.5)	SERVICE TO GROUND	13.5 (4.1)
				POST MOUNTED	6 (1.8)

THE PAY ITEM, WOOD POLE, SHALL INCLUDE A NEW GALVANIZED STEEL 15 FOOT MAST ARM FOR MOUNTING THE VIDEO DETECTION CAMERA.

PROPOSED WIRELESS INTERCONNECT TO I-57 (SOUTHBOUND)

MANHATTAN/MONEE RD.

CABLE PLAN

TEMPORARY CABLE DIAGRAM LEGEND

	PROPOSED	EXISTING
TEMPORARY CONTROLLER CABINET		
TEMPORARY SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT		
TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION, 12" (300 mm)		
12" (300 MM) PEDESTRIAN SIGNAL SECTION		
ELECTRIC CABLE IN CONDUIT, NO. 14, UNLESS OTHERWISE NOTED. NUMBER OF CONDUCTORS AS NOTED		
PEDESTRIAN PUSHBUTTON DETECTOR		
VEHICLE DETECTOR, INDUCTION LOOP		
EMERGENCY VEHICLE SYSTEM DETECTOR		
CONFIRMATION BEACON		
COAXIAL CABLE		
WIRELESS INTERCONNECT		

SCHEDULE OF QUANTITIES

ITEM	UNIT	QUANTITY
VIDEO DETECTION SYSTEM (COMPLETE INTERSECTION)	EACH	1
FULL-ACTUATED CONTROLLER IN TYPE V CABINET	EACH	1
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 3C	FOOT	310
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 5C	FOOT	815
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 7C	FOOT	135
* ELECTRIC CABLE AERIAL SUSPENDED, NO. 20 3/C, TWISTED, SHIELDED	FOOT	310
ELECTRIC CABLE AERIAL SUSPENDED, SERVICE, NO. 6 2C	FOOT	110
WOOD POLE, 40 FT., CLASS 2	EACH	4
* LIGHT DETECTOR AMPLIFIER	EACH	1
* LIGHT DETECTOR	EACH	2
SIGNAL HEAD, 1-FACE, 3-SECTION	EACH	7
SIGNAL HEAD, 1-FACE, 5-SECTION	EACH	2
SERVICE INSTALLATION, POLE MOUNTED	EACH	1
SPAN WIRE	EACH	440
TETHER	EACH	440

* 100% COST TO MONEE FIRE PROTECTION DISTRICT



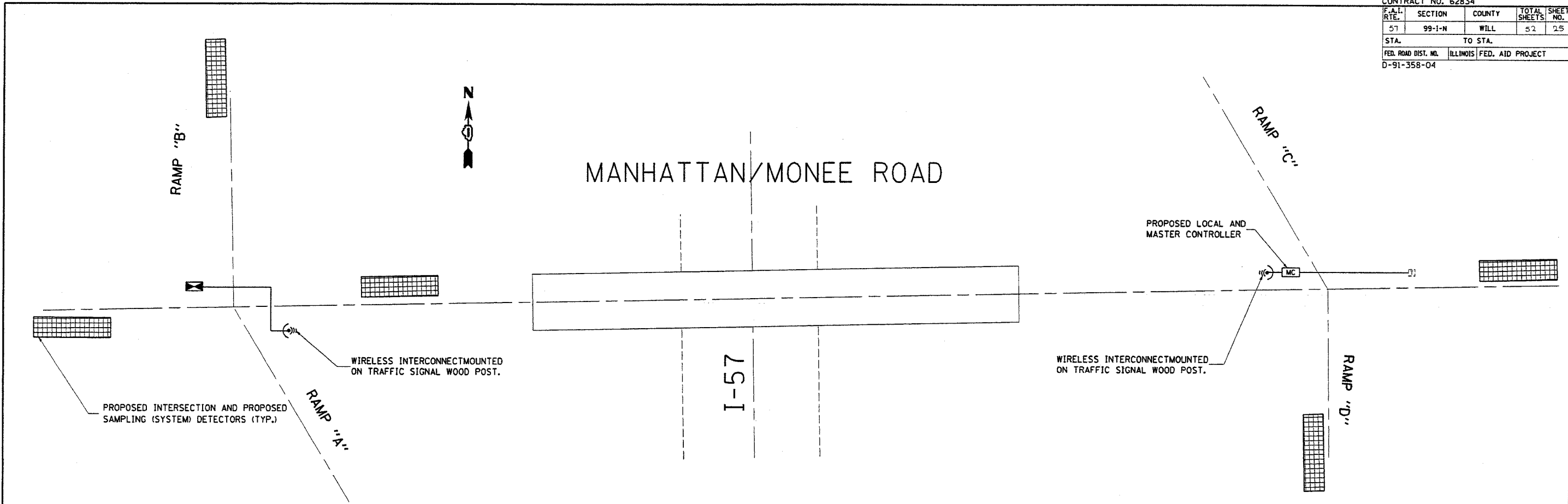
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TEMPORARY CABLE PLAN, PHASE DESIGNATION DIAGRAM,
 AND
 SCHEDULE OF QUANTITIES
 (NORTHBOUND RAMP)
 IL. RTE. 57 @ MANHATTAN/MONEE RD.

SCALE: 1"=20'
 DATE: 10/5/2005

DRAWN BY: BCK
 DESIGN BY: BCK
 CHECKED BY: DAD

CONTRACT NO. 62834				
F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	25
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
D-91-358-04				



INTERCONNECT SCHEMATIC LEGEND

	PROPOSED	EXISTING
INTERSECTION CONTROLLER		
MASTER CONTROLLER		
MASTER MASTER CONTROLLER		
TELEPHONE CONNECTION		
INTERSECTION & SAMPLING (SYSTEM) DETECTORS		
PREFORMED INTERSECTION & SAMPLING (SYSTEM) DETECTORS		
EXISTING INTERSECTION LOOP DETECTORS, PROPOSED SAMPLING (SYSTEM) DETECTORS		
SAMPLING (SYSTEM) DETECTORS		
SAMPLING (SYSTEM) PREFORMED DETECTORS		
EXISTING SAMPLING (SYSTEM) DETECTORS; PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTORS.		
EXISTING SAMPLING (SYSTEM) DETECTORS, PROPOSED SAMPLING (SYSTEM) DETECTORS		
FIBER OPTIC CABLE IN CONDUIT, NUMBER OF FIBERS AS NOTED		
INTERCONNECT CABLE - NO. 18 3 PAIR TWISTED, SHIELDED		
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14, 1 PAIR		
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE		
WIRELESS INTERCONNECT (ANTENNA)		
VIDEO DETECTION ZONE		

DESCRIPTION	QUANTITY	UNIT
MASTER CONTROLLER	1	EACH
WIRELESS INTERCONNECT SYSTEM	1	EACH

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

INTERCONNECT SCHEMATIC AND SCHEDULE OF QUANTITIES I-57 (S.B. RAMP TO N.B. RAMP) AT MONEE/MANHATTAN RD.

SCALE: VERT. 1"=20'
HORIZ. DATE: 8/15/2005

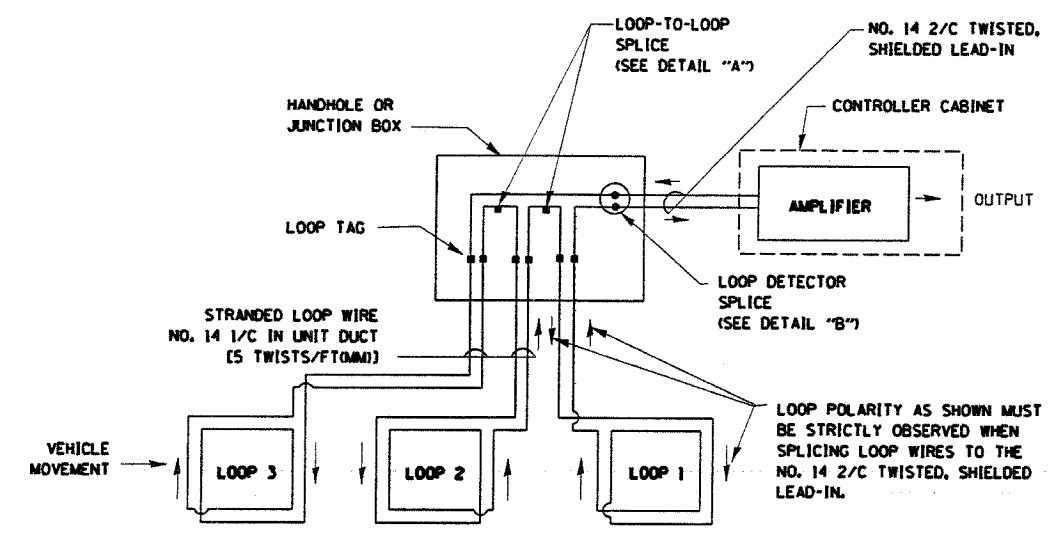
DRAWN BY: BCK
DESIGNED BY: BCK
CHECKED BY: DAD

PLOT DATE: 8/15/2005
FILE NAME: c:\projects\135804\traffic.m32
PLOT SCALE: 42.3029' / INL
USER NAME: karrnphayb

CONTRACT NO. 62834				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	26
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
D-91-358-04				

LOOP DETECTOR NOTES

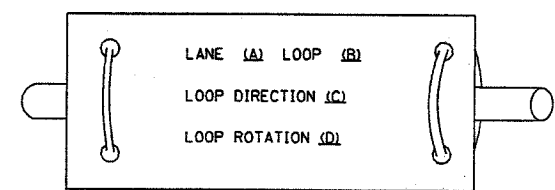
1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.



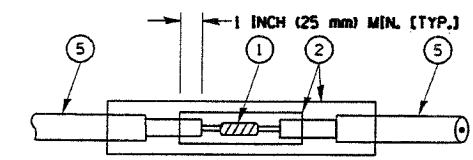
DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

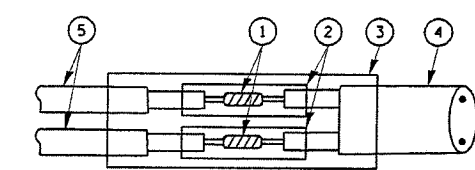
LOOP LEAD-IN CABLE TAG



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**DISTRICT ONE
 STANDARD TRAFFIC SIGNAL
 DESIGN DETAILS**

SCALE: VERT. NONE
 HORIZ. DATE 1-01-02

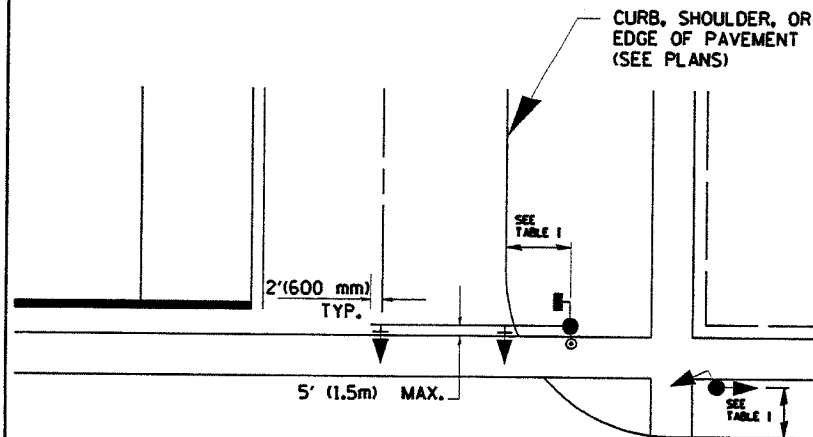
DRAWN BY: RWP
 DESIGNED BY: DAZ
 CHECKED BY: DAZ
 SHEET 1 OF 4

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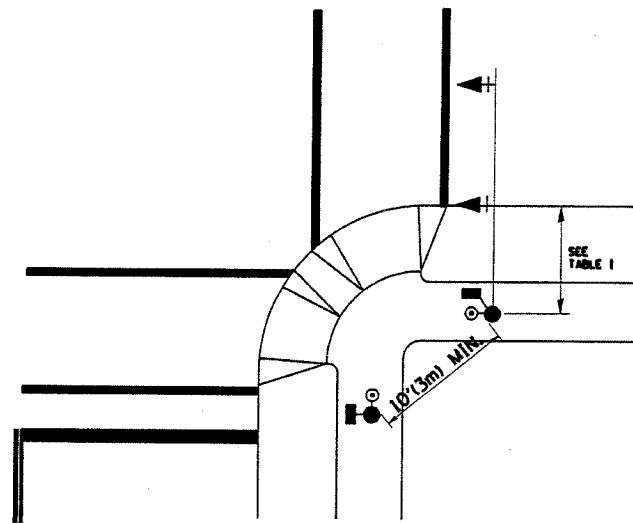
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	27
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
D-91-358-04				

TRAFFIC SIGNAL MAST ARM AND POST

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



PEDESTRIAN SIGNAL PUSHBUTTON



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

NOTES:

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.
 AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.
 PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:
 A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.
 B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.
 C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
 D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).
 E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK.
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

PEDESTRIAN SIGNAL POST

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

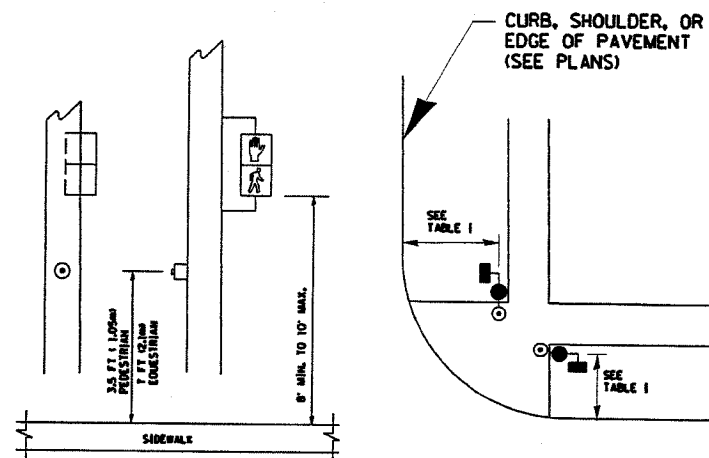


TABLE I

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB)	SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN PUSHBUTTON	SEE NOTE 1	SEE NOTE 1

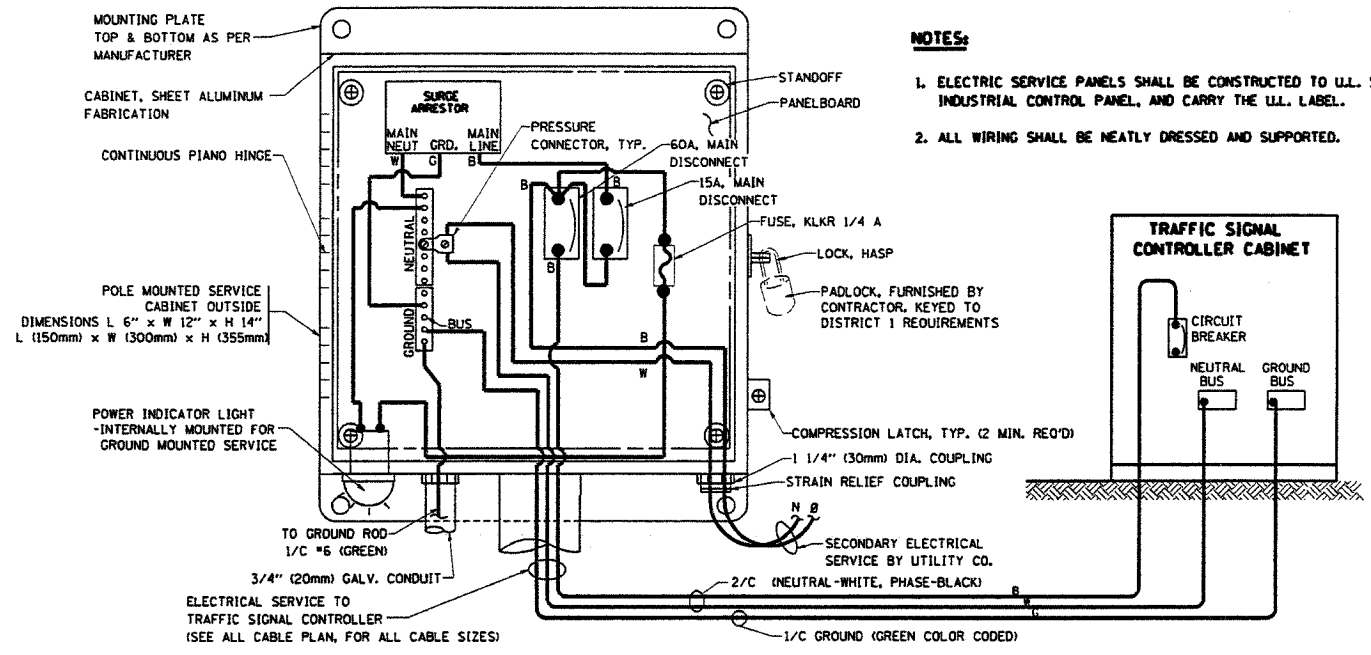
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: VERT. NONE
 HORIZ. DATE 1-01-02

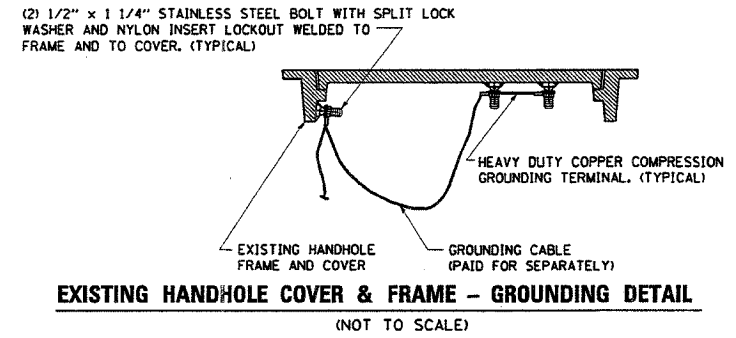
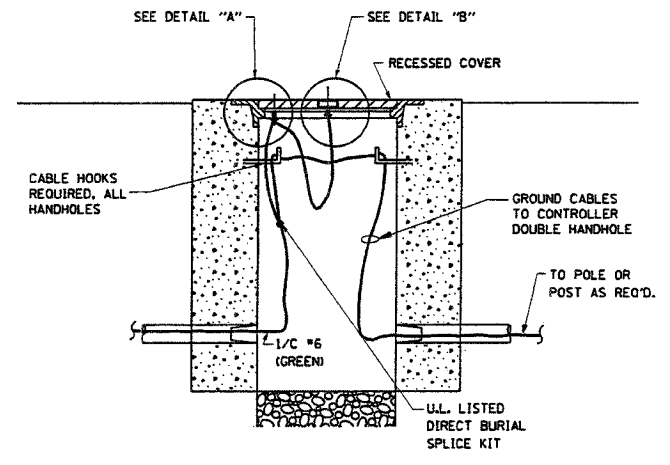
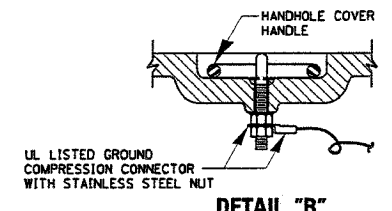
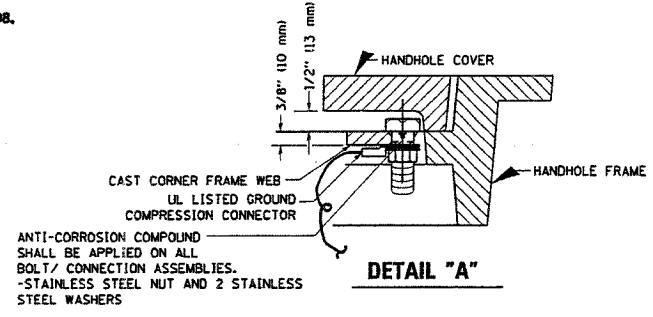
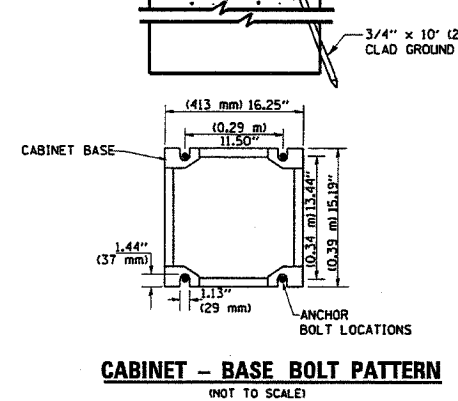
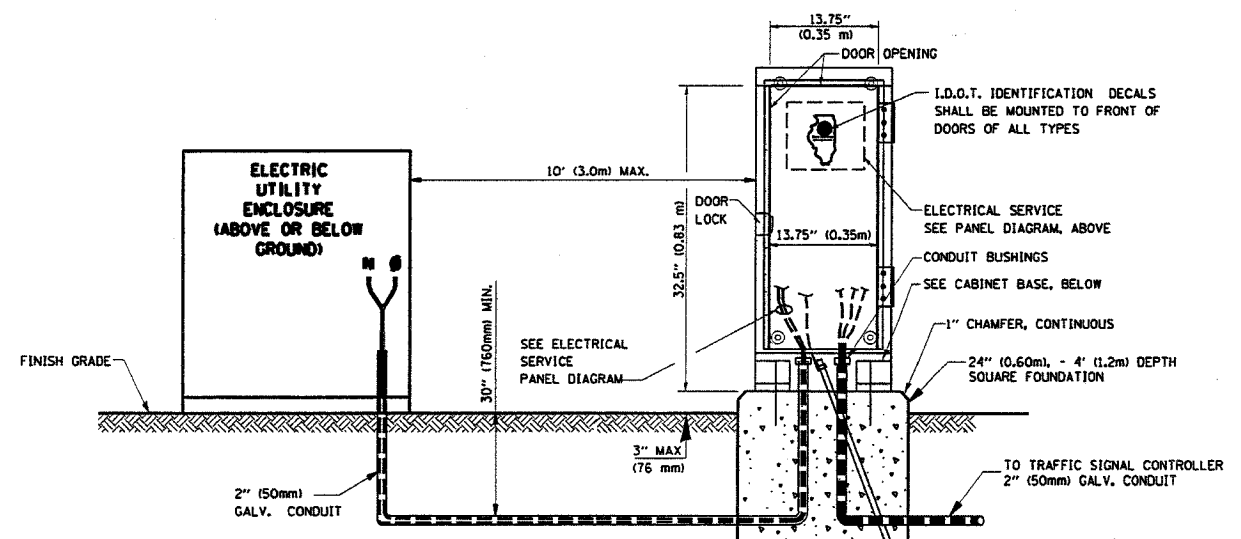
DRAWN BY: RWP
 DESIGNED BY: DAD
 CHECKED BY: DAZ
 SHEET 2 OF 4

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CONTRACT NO. 62834				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	28
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
D-91-358-04				



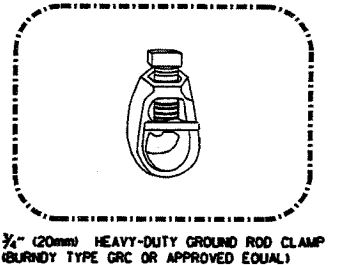
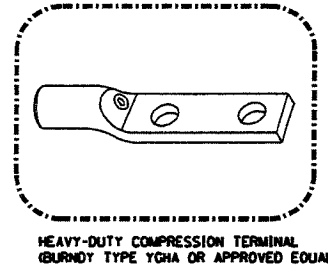
ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



NOTES:

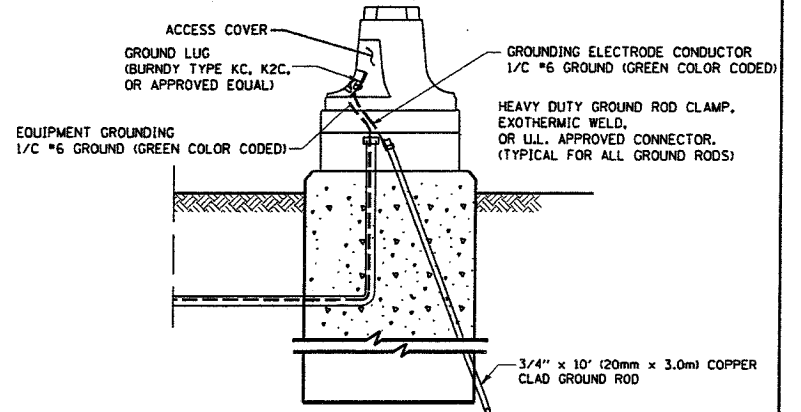
GROUNDING SYSTEM

- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE HANDHOLE, POST, MAST ARM, CONTROLLER, ETC. GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, U.L. APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

SCALE: VERT. NONE
 HORIZ. 1"=10'-0"

DATE: 1-01-02

DRAWN BY: RWP
 DESIGNED BY: DAD
 CHECKED BY: DAZ
 SHEET 3 OF 4

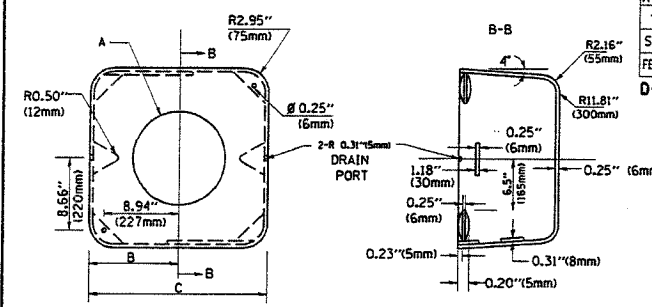
8/16/2005
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CONTRACT NO. 62834

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	NO.
57	99-1-N	WILL	52	29

STA. TO STA.
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
 D-91-358-04

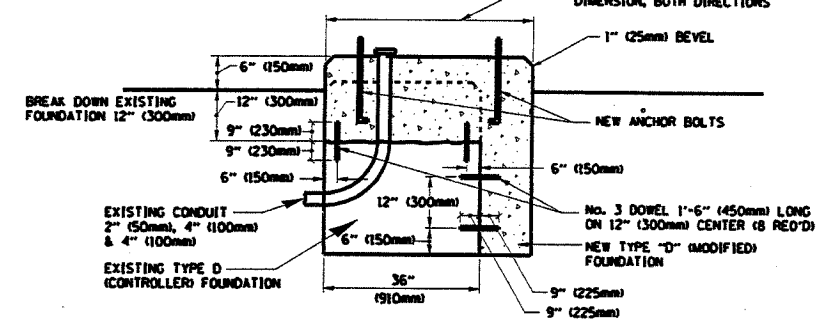
MATERIAL:
 - ASTM A48 CLASS 30 GREY IRON
 - ASTM A123 HOT DIPPED GALVANIZED



TYPE	A	B	C	HEIGHT	WEIGHT
I	Ø 10.125 (257mm)	9.5 (241mm)	19 (483mm)	12 (300mm)	24kg
II	Ø 11.125 (283mm)	10.75 (273mm)	21.5 (546mm)	12 (300mm)	26kg

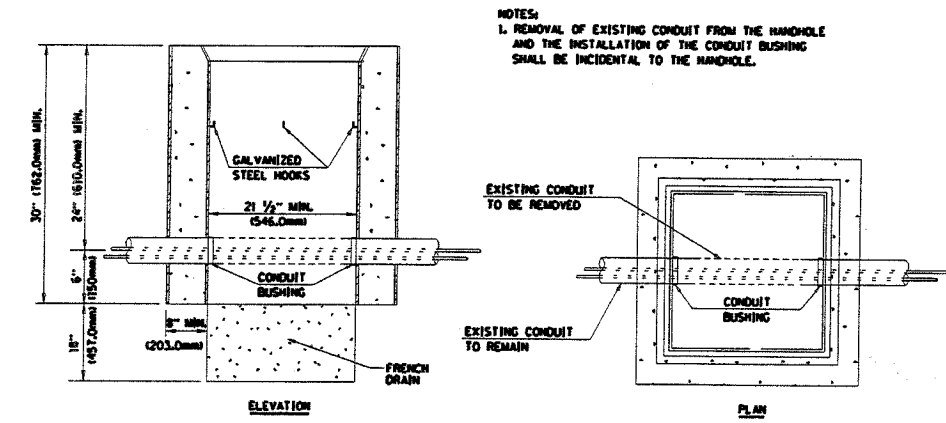
SHROUD DETAIL

NOTE:
 SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION

(NOT TO SCALE)



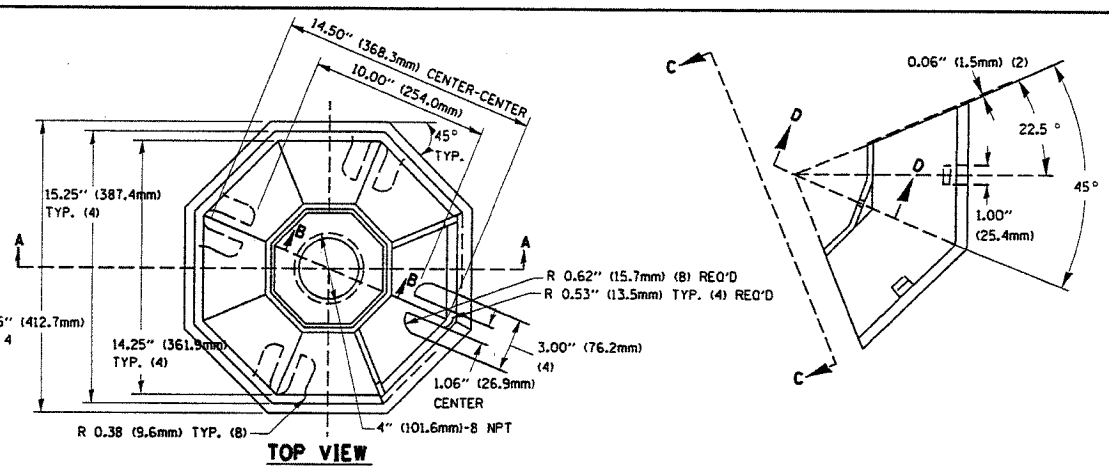
NOTES:
 1. REMOVAL OF EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHING SHALL BE INCIDENTAL TO THE HANDHOLE.

DETAIL HANDHOLE TO INTERCEPT EXISTING CONDUIT N.T.S.

REVISIONS	
NAME	DATE

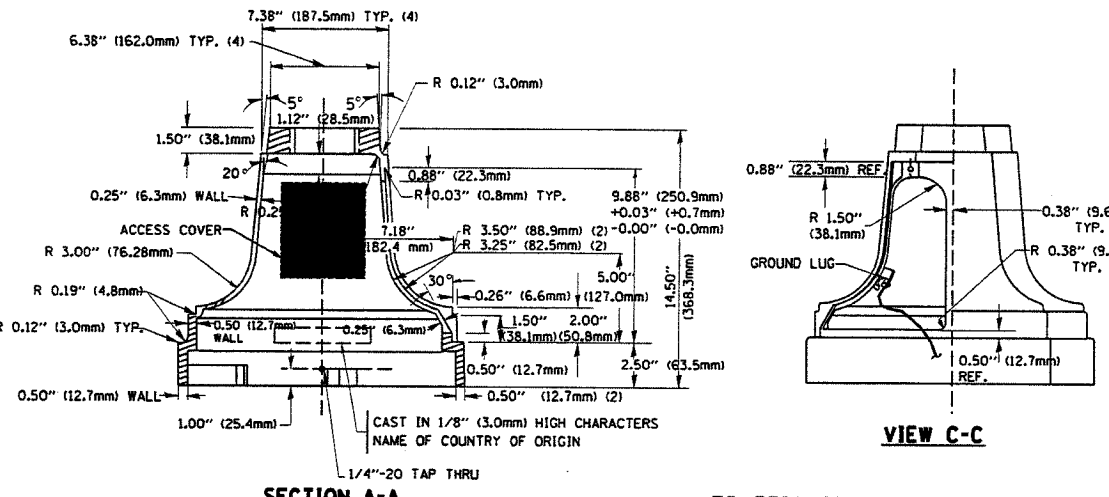
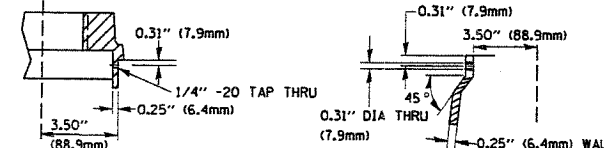
ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT 1
 STANDARD TRAFFIC SIGNAL
 DESIGN DETAILS

SCALE: VERT. NONE
 HORIZ. NONE
 DATE 1-01-02
 DRAWN BY: RWP
 DESIGNED BY: DAD
 CHECKED BY: DAZ
 SHEET 4 OF 4

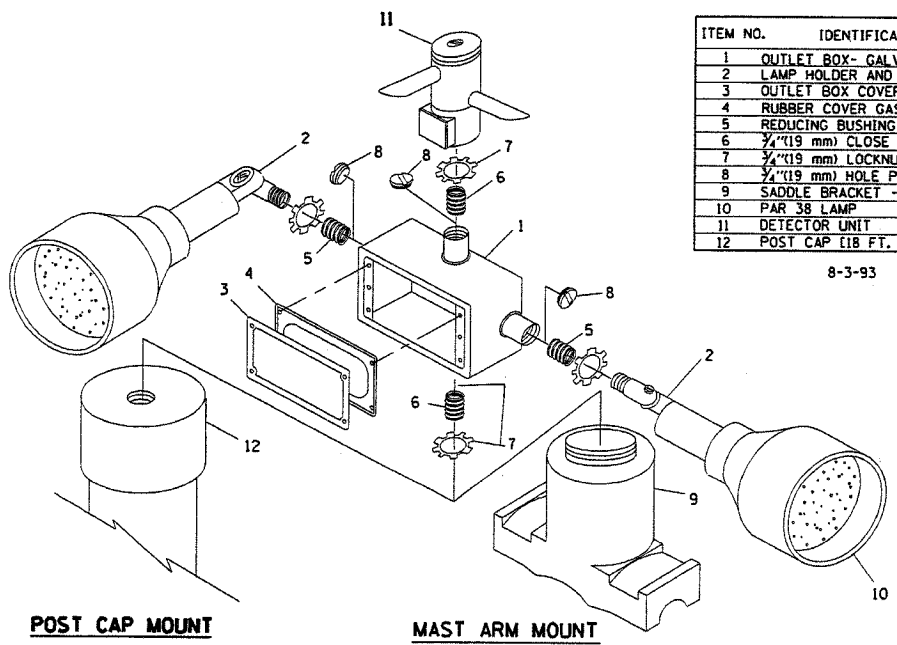


SECTION B-B

SECTION D-D



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A

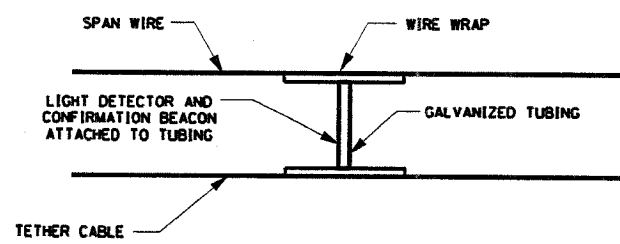


ITEM NO.	IDENTIFICATION
1	OUTLET BOX - GALV. 21 CU. IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4 (19 mm) CLOSE NIPPLE
7	3/4 (19 mm) LOCKNUT
8	3/4 (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	PAR 38 LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

8-3-93

NOTES:

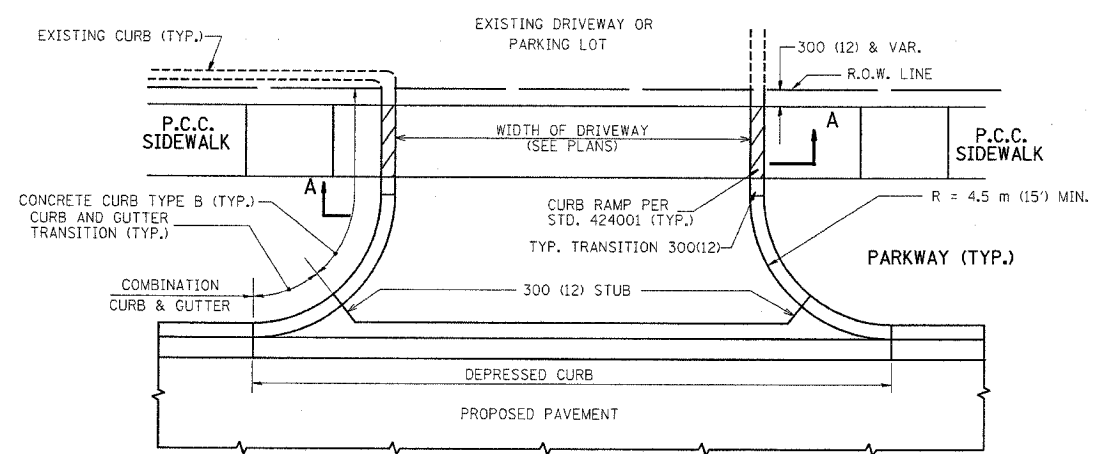
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
 ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
 ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



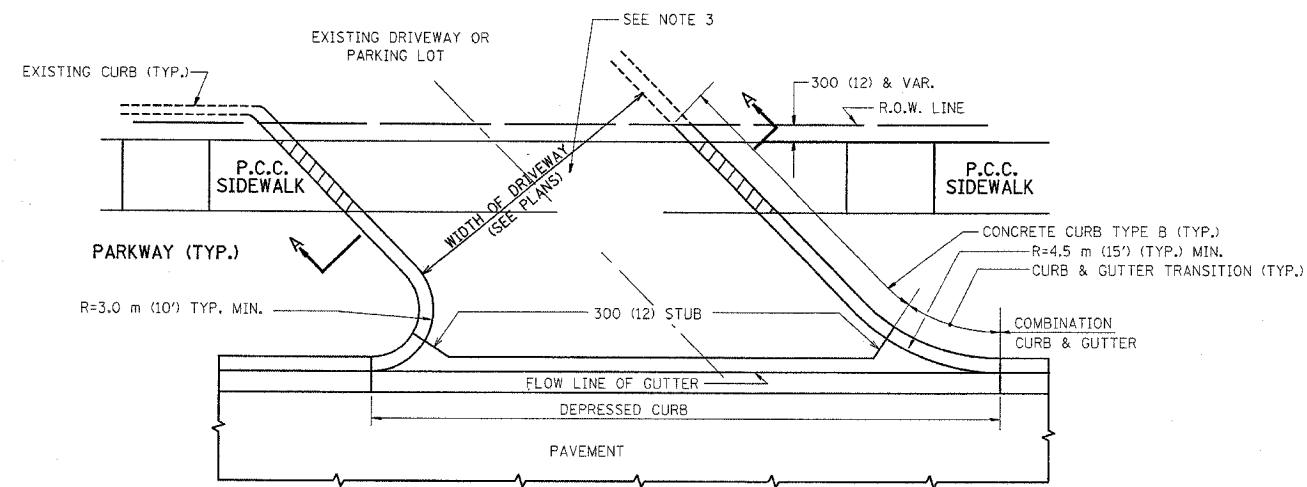
LIGHT DETECTOR AND CONFIRMATION BEACON MOUNTING FOR TEMPORARY TRAFFIC SIGNALS (NOT TO SCALE)

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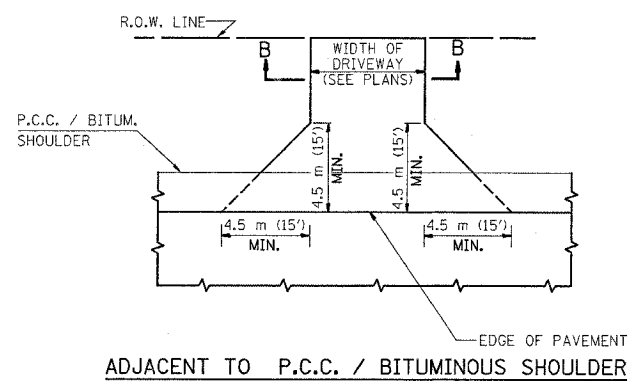
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	30
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



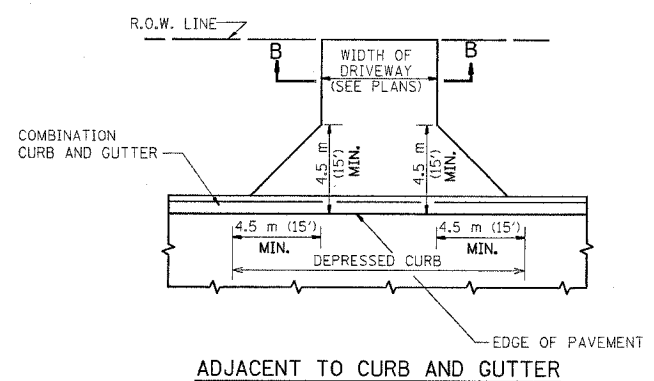
WITH CONCRETE CURB, TYPE B



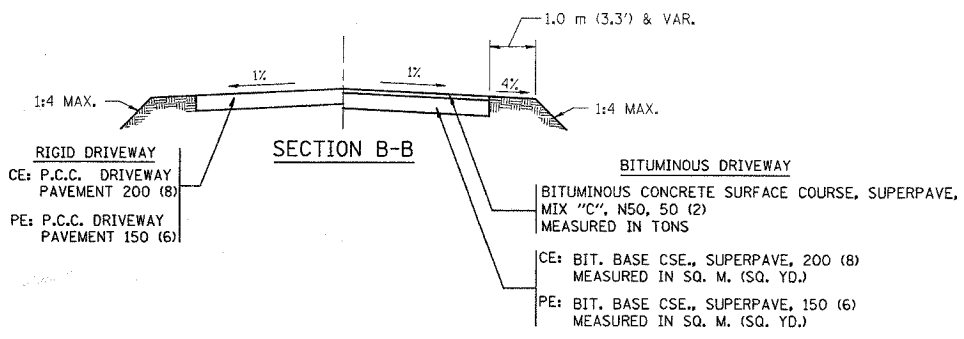
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / BITUMINOUS SHOULDER



ADJACENT TO CURB AND GUTTER



GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

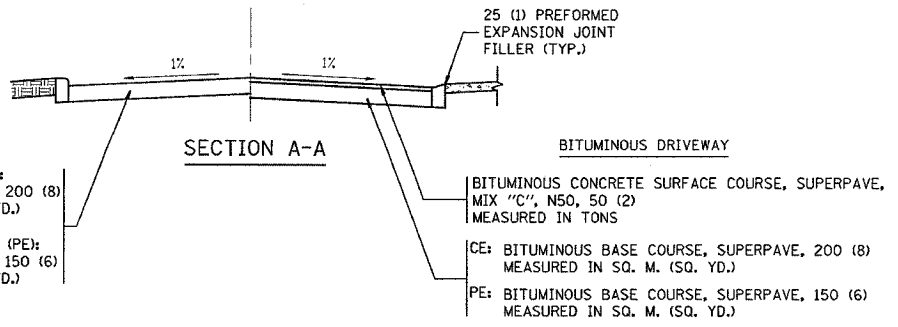
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 1.2 METERS (4 FEET) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

25 (1) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED

REVISIONS	
NAME	DATE
P. LoFLEUR	04-15-03
R. SHAH	11-04-95
J. POLLASTRINI	08-12-96
J. POLLASTRINI	12-14-96
A. ABBAS	03-21-97
T. HOLTZ	04-08-97
M. GOMEZ	04-06-01

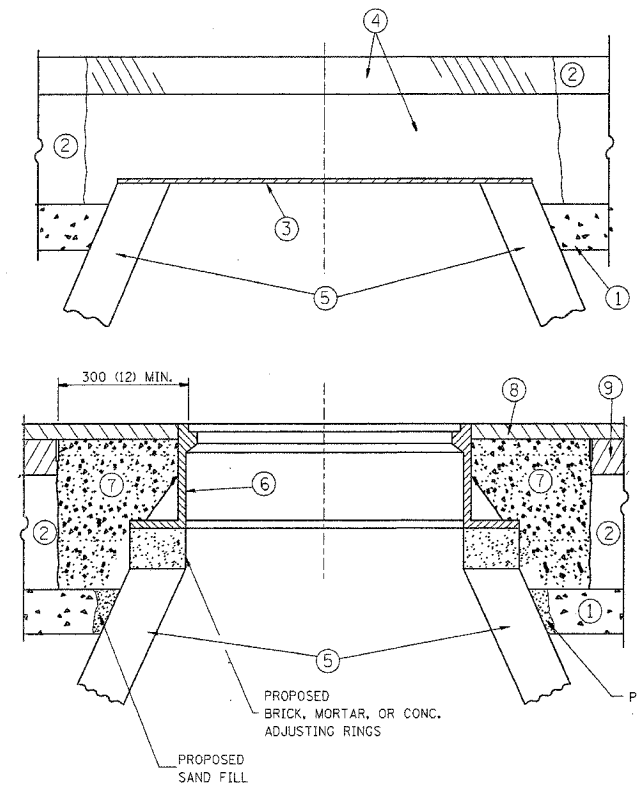
ILLINOIS DEPARTMENT OF TRANSPORTATION
DRIVEWAY DETAILS
 DISTANCE BETWEEN R.O.W. AND FACE OF CURB / EDGE OF SHOULDER >= 4.5 m (15')

SCALE: VERT. HORIZ. DATE: 9/16/2004

DRAWN BY CHECKED BY

BD400-01 (BD-01) REVISION DATE: 04/15/03

F. A. SHEET	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	31
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 300 (12) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 900 (36) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 40 (1 1/2) THICK BITUMINOUS MATERIAL APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE BITUMINOUS MATERIAL AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- | | |
|--|--|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS SI CONCRETE, BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL |
| ③ 900 (36) DIAMETER METAL PLATE | ⑧ PROPOSED BITUMINOUS CONCRETE SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND BITUMINOUS MATERIAL | ⑨ PROPOSED BITUMINOUS CONCRETE BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: FRAMES AND LIDS TO BE ADJUSTED, SPECIAL EACH

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN

ILLINOIS DEPARTMENT OF TRANSPORTATION

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

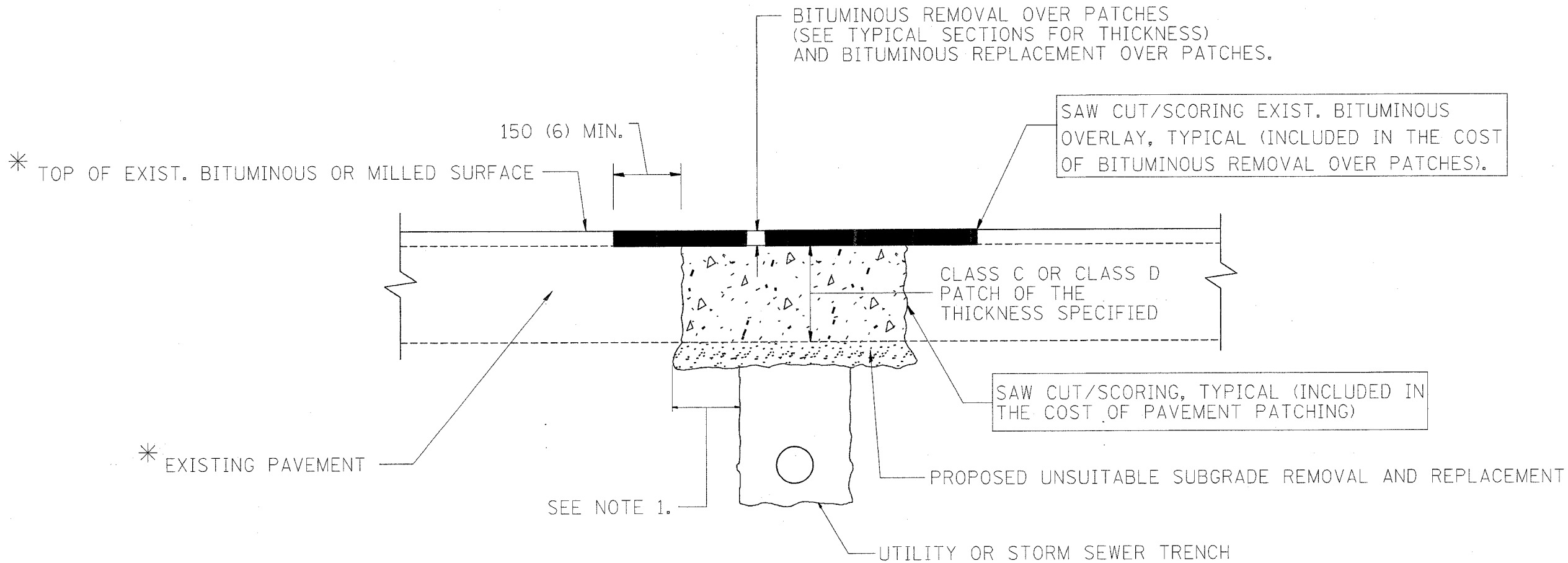
REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04

SCALE: NONE
DATE: 9/16/2004

DRAWN BY
CHECKED BY

BD600-03 (BD-8)
REVISION DATE: 05/17/04

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	32
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 300 (12) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE SPECIAL PROVISION "PATCHING WITH BITUMINOUS OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/14/95
R. SHAH	03/23/95
R. SHAH	04/24/95
A. HOUSEH	03/15/96
A. ABBAS	03/21/97
A. ABBAS	01/20/98
ART ABBAS	04/27/98

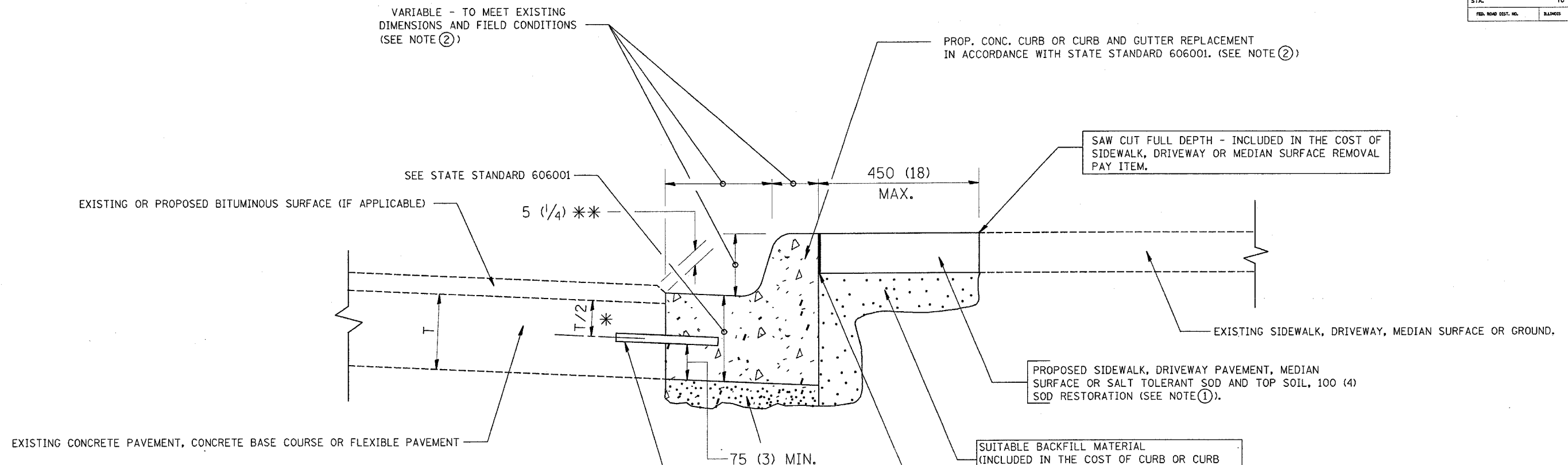
ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR BITUMINOUS SURFACED PAVEMENT

SCALE: VERT. HORIZ. DATE 9/16/2004

DRAWN BY CHECKED BY

F. & A. SITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	33
STA. TO STA.				
FED. ROAD DIST. NO.	BLK/MS	FED. AID PROJECT		



- * 75 (3) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 100 (4) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF BITUMINOUS SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 100 (4) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 100 (4) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED NO. 20 (NO. 6) EPOXY COATED TIE BARS 600 (24) LONG AT 600 (24) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER METER (FOOT) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

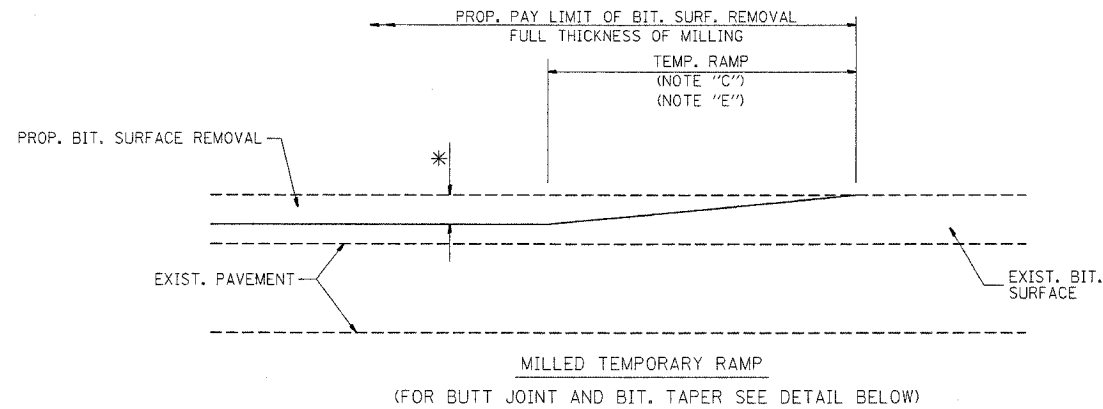
REVISIONS	
NAME	DATE
M. DE YONG	05/28/91
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01

ILLINOIS DEPARTMENT OF TRANSPORTATION
**CURB OR
CURB AND GUTTER
REMOVAL AND REPLACEMENT**

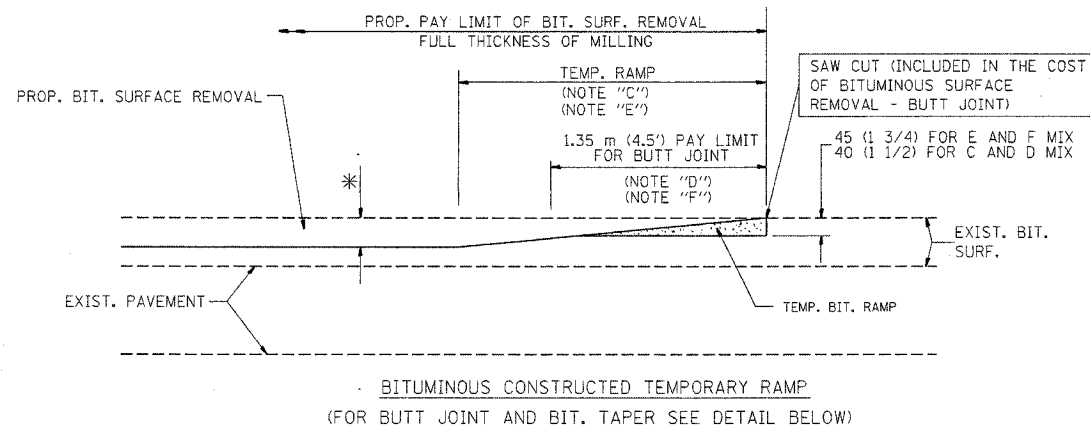
SCALE: NONE
DATE 9/16/2004

DRAWN BY
CHECKED BY
BD600-06 (BD-24)

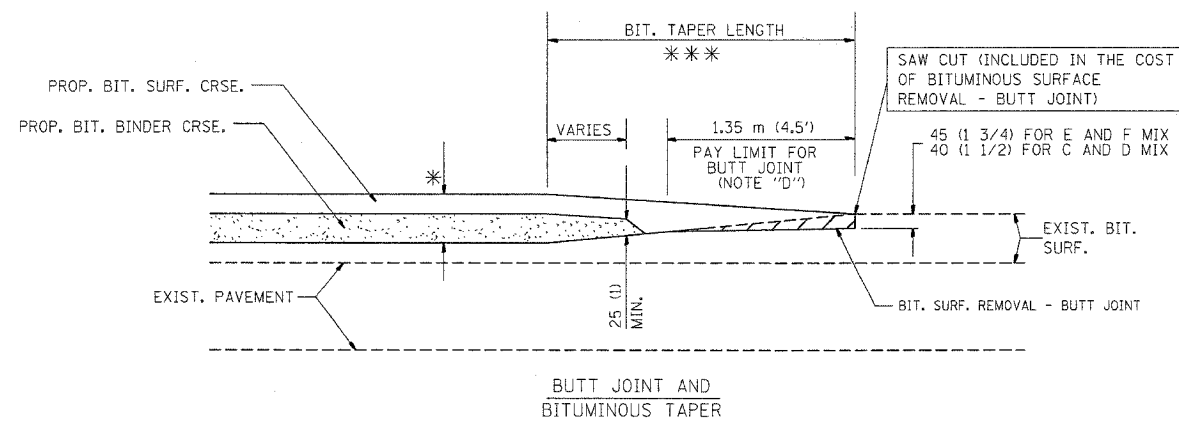
F. A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	34
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



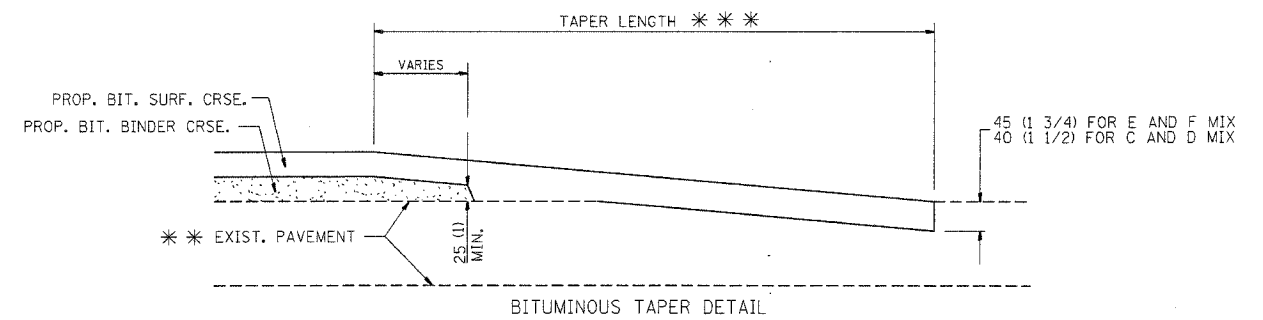
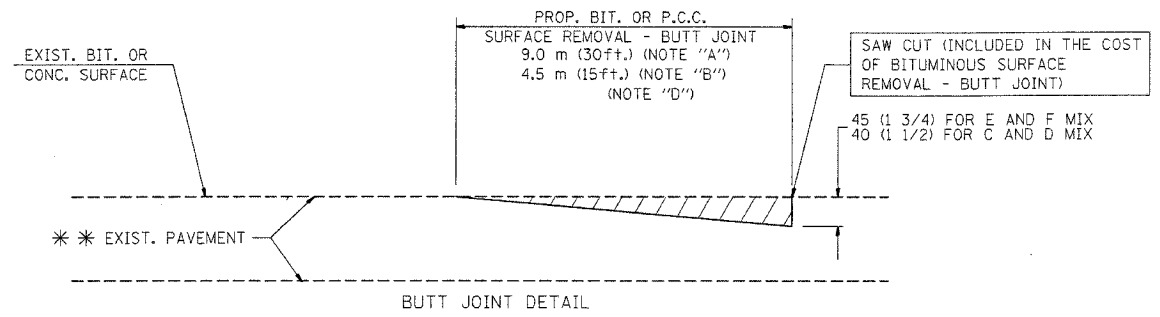
OPTION 1



OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR RESURFACING ONLY

*** PC CONCRETE, BITUMINOUS OR BITUMINOUS RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING BITUMINOUS SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED BITUMINOUS COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 900 (3 FT.) PER INCH OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 1.35 m (4.5') TEMP. BIT. RAMP WILL BE PAID AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT".
 - G: SEE ARTICLE 406.18 AND 406.24 OF THE STANDARD SPECIFICATIONS FOR "BITUMINOUS AND PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 6.1 m (20') PER 25 (1) RESURFACING (NOTE "A")
3.0 m (10') PER 25 (1) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND BITUMINOUS TAPER DETAILS

BASIS OF PAYMENT:

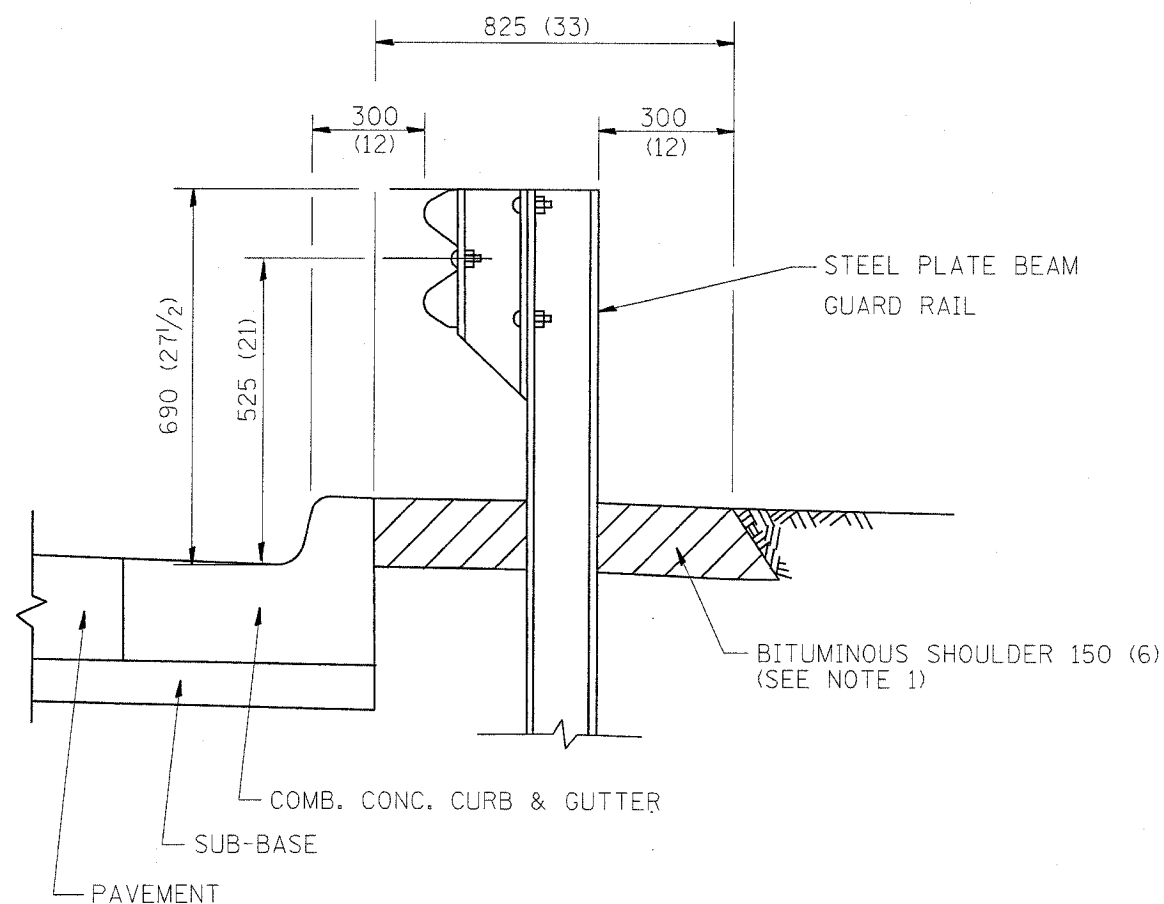
THE BUTT JOINT WILL BE PAID FOR PER SQUARE METER (SQUARE YARD.) AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT" OR AS "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01

SCALE: NONE
DATE PLOTTED: 9/16/2004

DRAWN BY
CHECKED BY
BD400-05 (VI-BD32)

F. & R. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL.	52	35
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

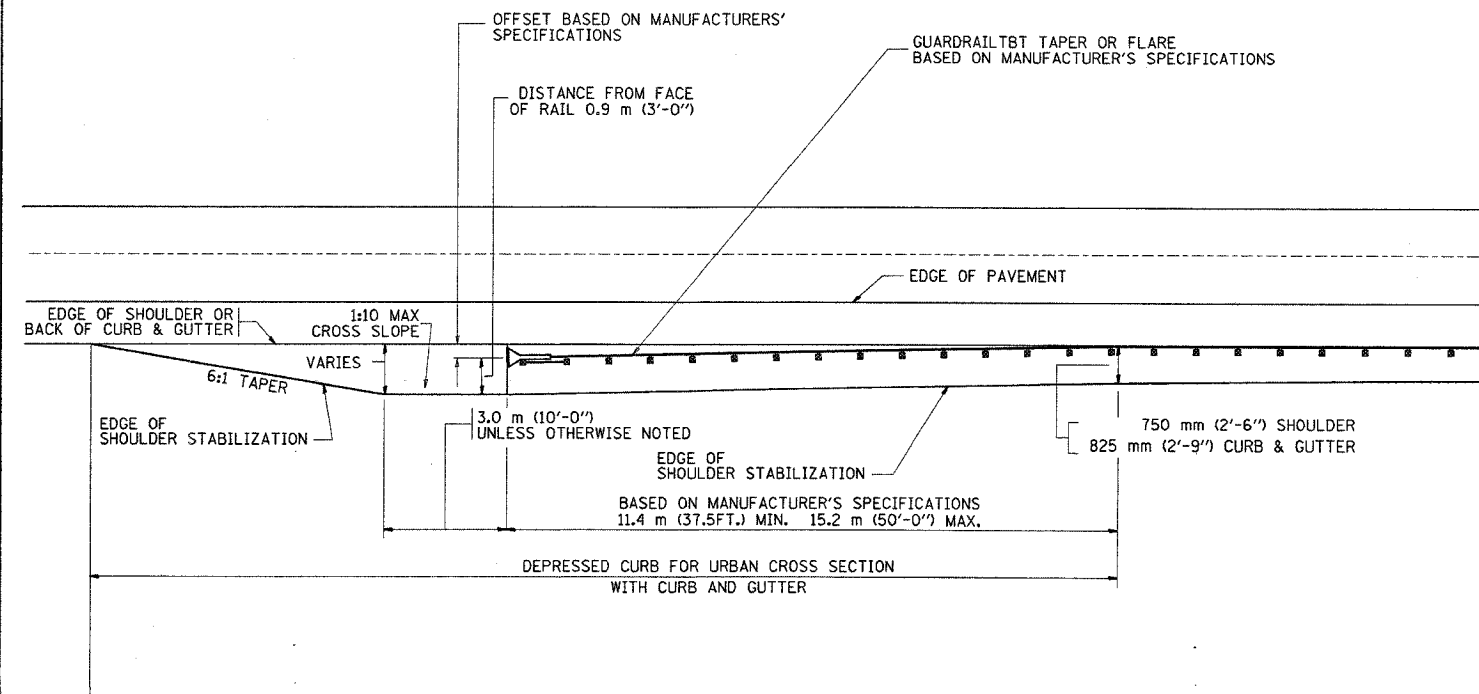


- NOTES: 1. THE BITUMINOUS SHOULDER SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL
2. GUARD RAIL MAY BE PLACED AT THE BACK OF CURB WHEN DIRECTED BY THE ENGINEER.

BASIS OF PAYMENT: BITUMINOUS SHOULDER 150 (6) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER m² (sq. yd.) AS "BITUMINOUS SHOULDER 150 (6)."

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER
 [FOR ROADWAY SPEED 60 kmh (35 MPH) TO 70 kmh (45 MPH)]



STABILIZATION AT TBT TY. 1 SPL.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DETAILS FOR
 STEEL PLATE BEAM GUARD RAIL
 ADJACENT TO CURB AND GUTTER
 STABILIZATION AT TBT TY 1 SPL.

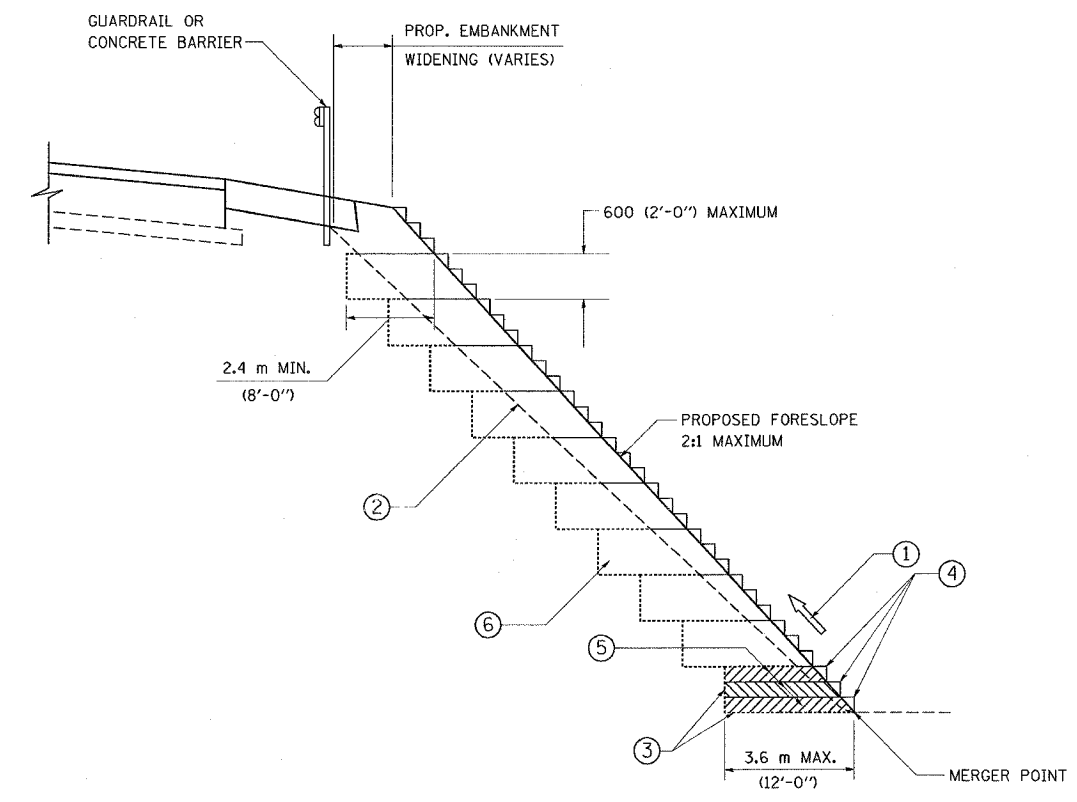
REVISIONS	
NAME	DATE
M. DE YONG	09-22-90
M. DE YONG	07-14-92
R. SHAH	09/09/94
R. SHAH	10/25/94
R. SHAH	02/23/95
A. ABBAS	03/21/97
E. GOMEZ	08/28/00

SCALE: NONE
 DATE 9/16/2004

DRAWN BY Jls
 CHECKED BY

BD600-10 (BD 34)
 REVISION DATE: 08/28/00

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	36
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 200 (8-INCH) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

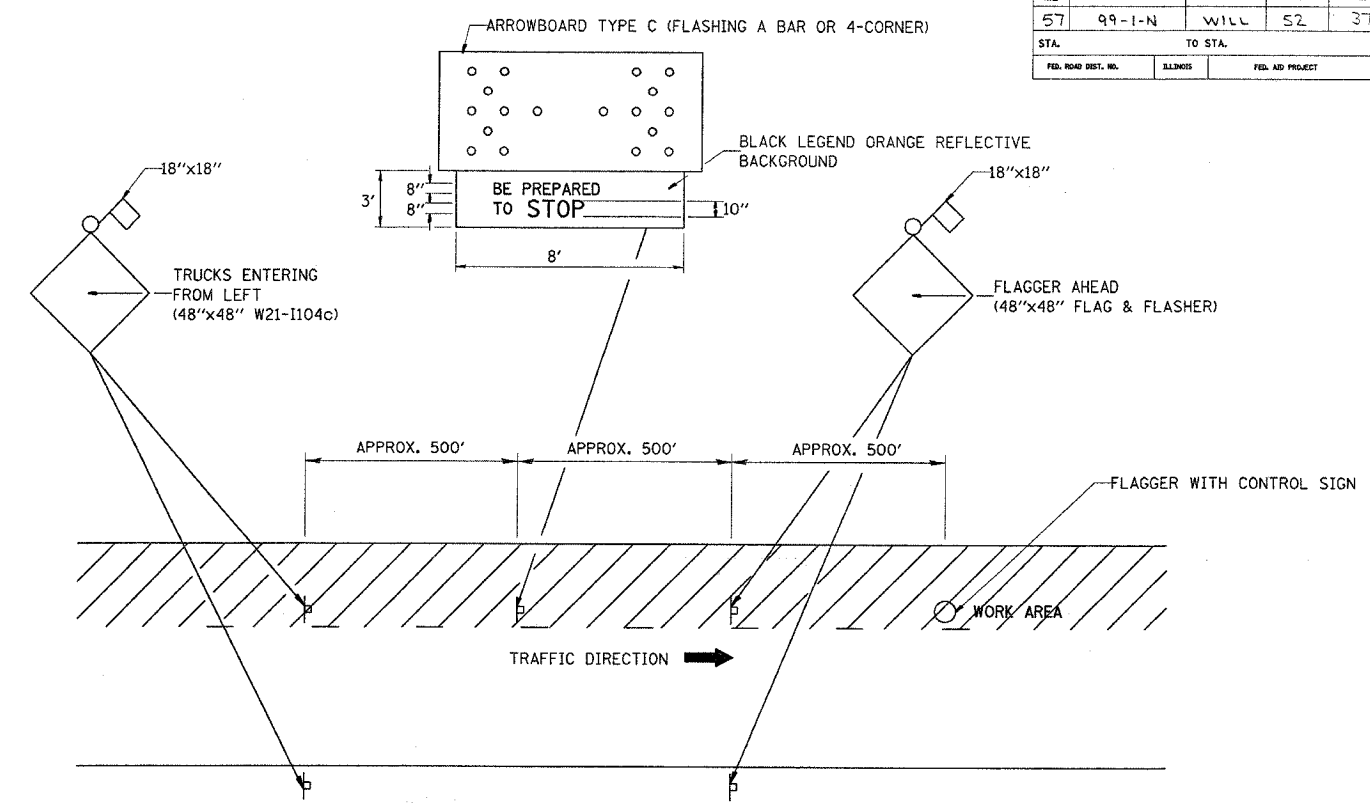
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**BENCHING DETAIL
FOR EMBANKMENT
WIDENING**

SCALE: VERT. DRAWN BY: CADD
 HORIZ. CHECKED BY: S.E.B.
DATE 10/4/2005 BD-51

REVISION DATE: 6-16-2004

P. & A. SHEET NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	37
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



METHOD OF FLAGGING

NOTE:

1. SIGNS SHALL BE MOUNTED AT A MINIMUM CLEARANCE HEIGHT OF 5 FEET
2. ALL SIGNS SHALL BE REMOVED WHEN THE FLAGGING OPERATION CEASES.
3. THIS CASE ALSO APPLIES WHEN THE WORK ZONE IS ON THE RIGHT. UNDER THESE CONDITIONS "TRUCKS ENTERING FROM RIGHT" SIGNS SHALL BE SUBSTITUTED FOR "TRUCKS ENTERING FROM LEFT" SIGNS. ALSO THE ARROWBOARD AND "BE PREPARED TO STOP" SIGNS SHALL BE RELOCATED TO THE RIGHT SIDE OF THE ROAD.
4. WORK ZONE ACCESS POINTS SHOULD BE A MINIMUM OF ONE HALF MILE APART. MEDIAN WORK ZONE ACCESS POINTS SHOULD NOT BE LOCATED OPPOSITE OF EACH OTHER.
5. NIGHTTIME FLAGGING OPERATIONS: THE FLAG STATION SHALL BE LIGHTED WITH ADDITIONAL LIGHTS OTHER THAN STREET LIGHTS. THE FLAGGER CONTROL SIGN AND THE FLAGGER'S VEST SHALL BE REFLECTORIZED. IN ADDITION, THE FLAGGER SHALL HAVE A FLASHLIGHT OR LIGHTED WAND.

REVISIONS	
NAME	DATE
RAY RITCHIE	5/10/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

METHOD OF FLAGGING

SCALE: NOT TO SCALE

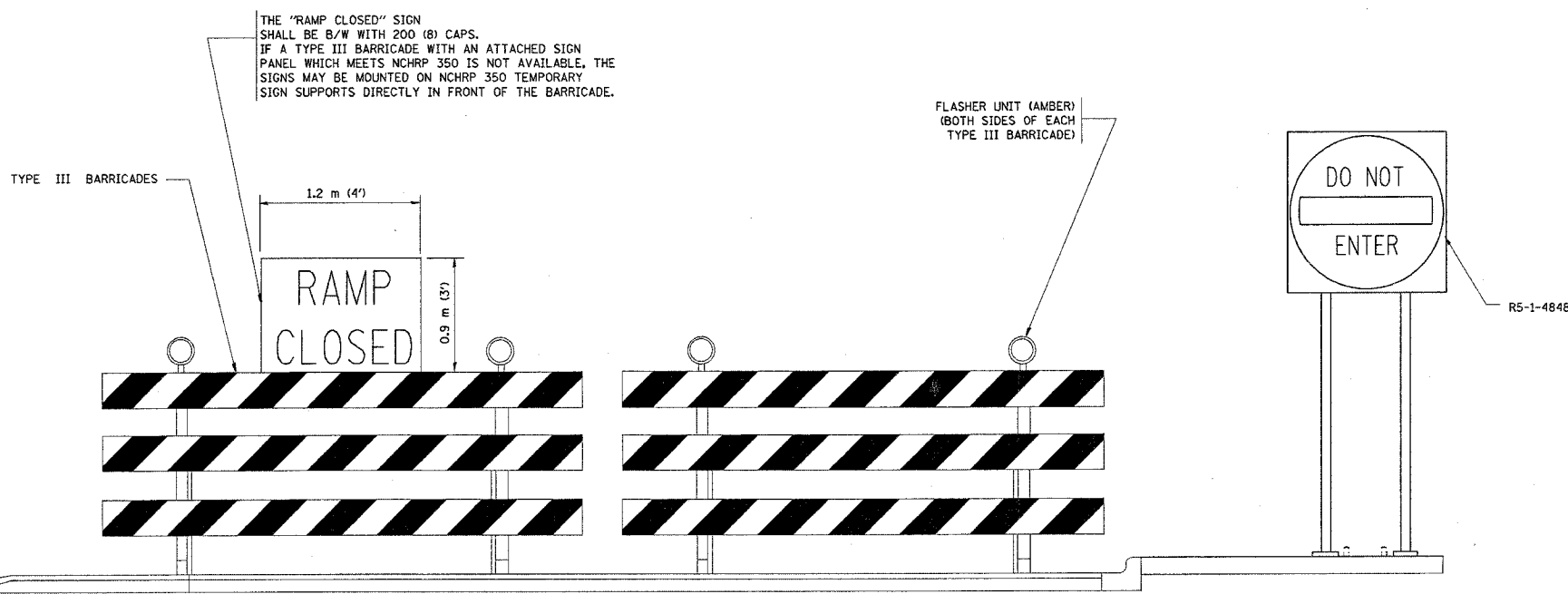
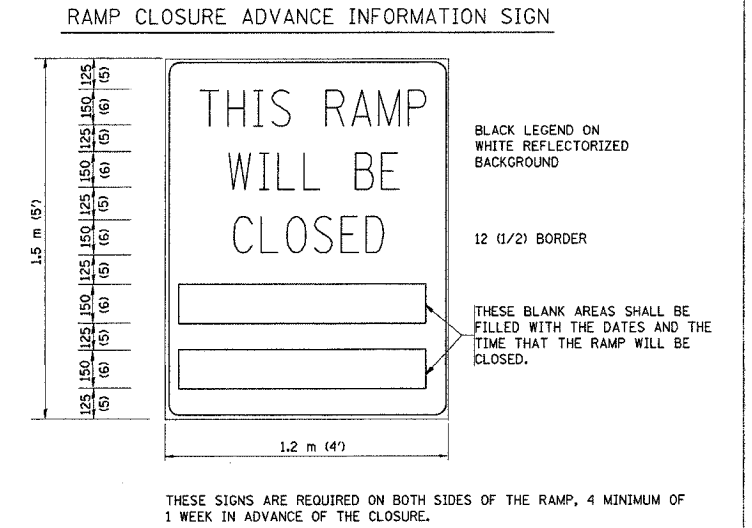
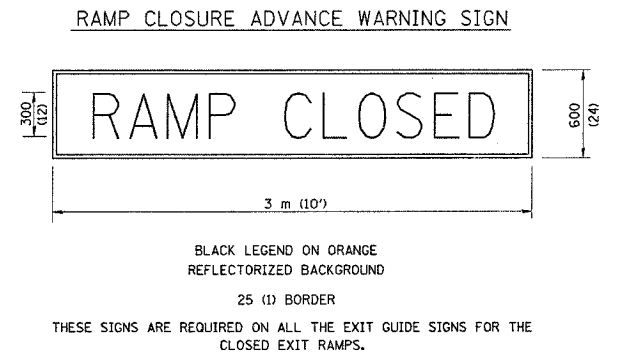
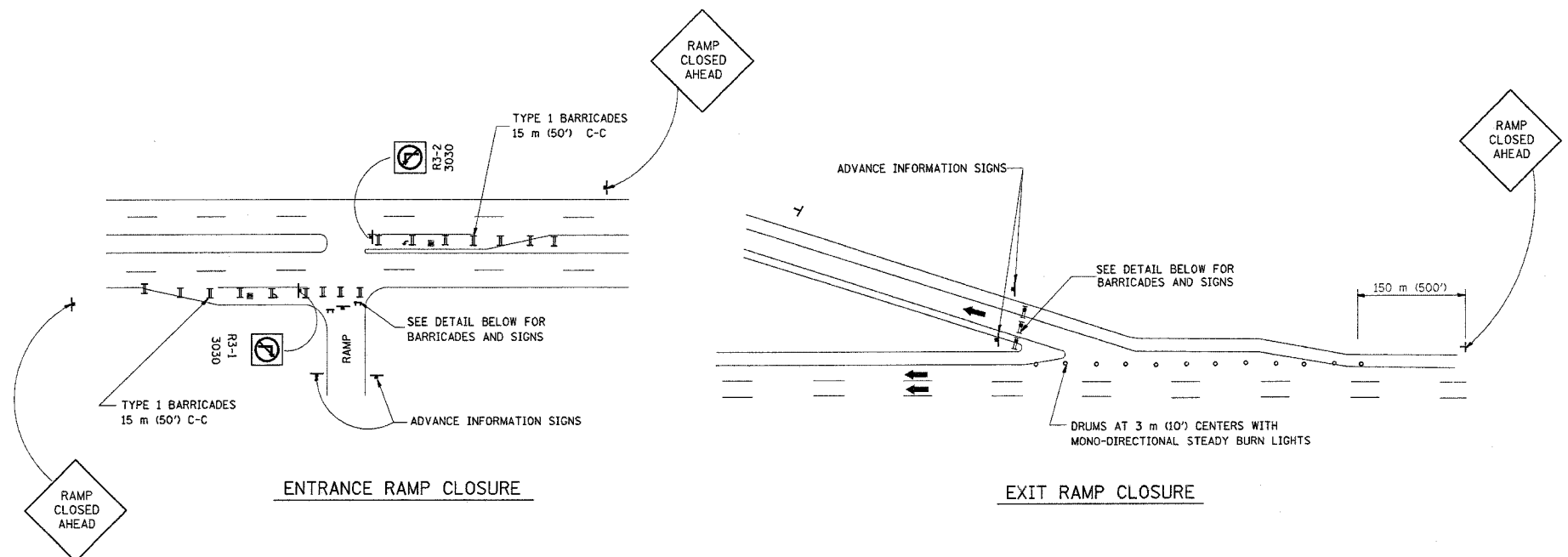
DATE 9/16/2004

DRAWN BY C.A.D.

CHECKED BY

BM-14

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	3B
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



THE "RAMP CLOSED" SIGN SHALL BE B/W WITH 200 (8) CAPS. IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS DIRECTLY IN FRONT OF THE BARRICADE.

DETAIL FOR REQUIRED BARRICADES & SIGNS

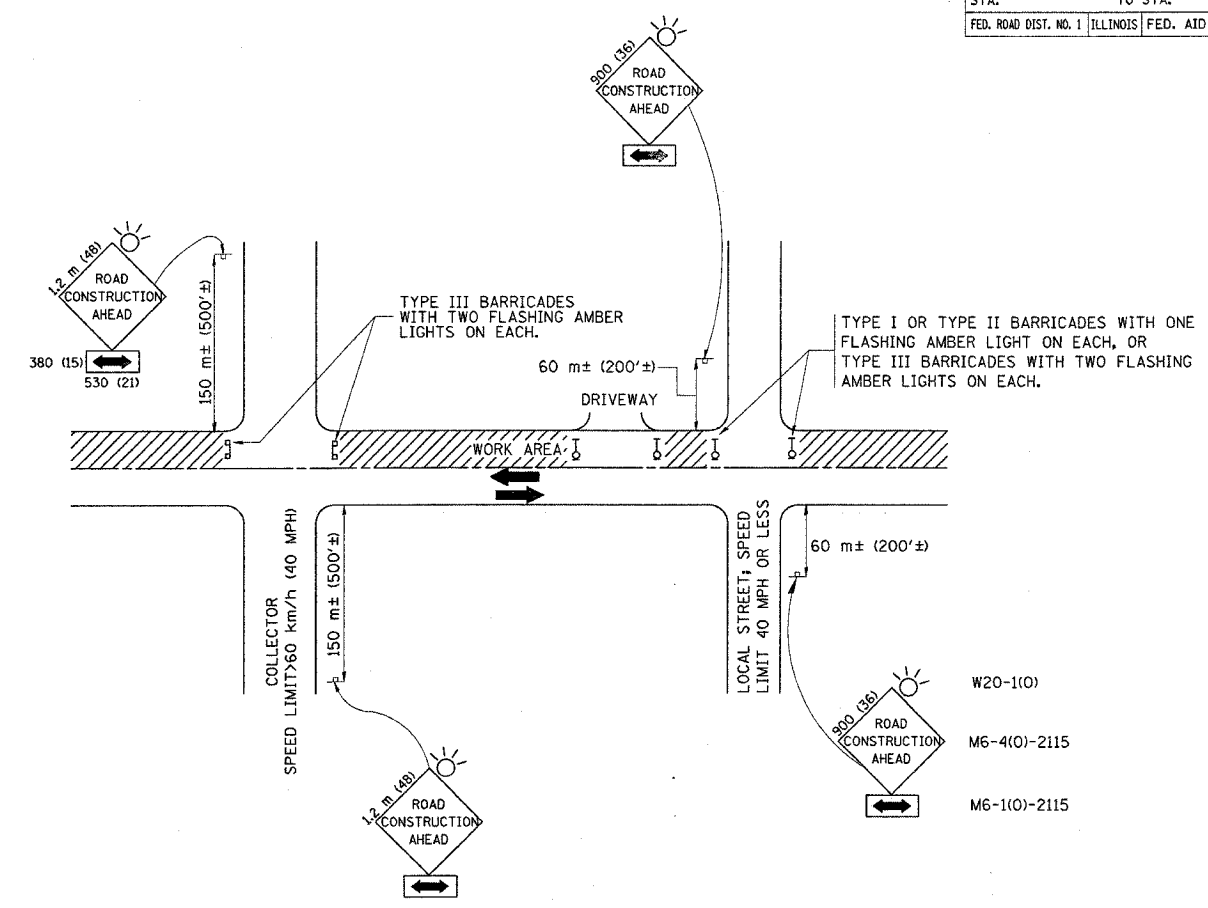
- NOTES:**
- CONES MAY BE SUBSTITUTED FOR TYPE I AND TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28" IN HEIGHT.
 - STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
 - THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS.
 - ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY-FOUR (24) HOURS IN LENGTH.

- GENERAL NOTES:**
- CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 700 (28) HIGH.
 - STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
 - A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
 - FOR DAYTIME RAMP CLOSURES, LASTING 6 HOURS OR LESS, THE CONTRACTOR MAY ELIMINATE THE ADVANCE WARNING SIGNS ON THE EXIT GUIDE SIGNS.
 - ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
 - THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE CONSIDERED INCIDENTAL TO TRAFFIC CONTROL AND PROTECTION.
 - AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

REVISIONS		SCALE: VERT. HORIZ. DATE 9/16/2005	DRAWN BY CHECKED BY
NAME	DATE		
DWS	2-83	ILLINOIS DEPARTMENT OF TRANSPORTATION FREWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS	TC-8 REVISION DATE: 04/03/03
DWS	1/90		
DWS	9/94		
DWS	12/94		
DWS/JAF	12/02		
Revise devices to meet NCHRP 350	4/03		

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	39
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

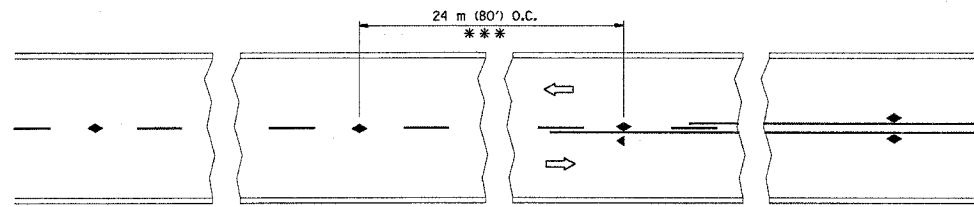
ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: VERT.
 HORIZ.
 DATE 9/16/2004

DRAWN BY
 CHECKED BY
 TC-10

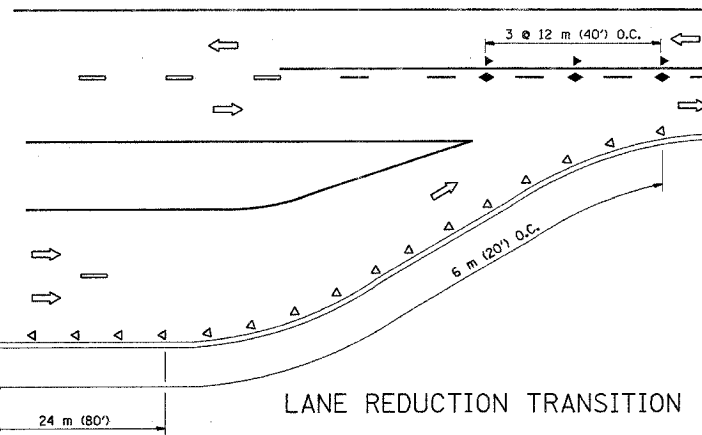
REVISION DATE: 01/06/00

F. A. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	40
STA.		TO STA.		
FED. ROAD DIST. NO.	BILLINGS	FED. AID PROJECT		

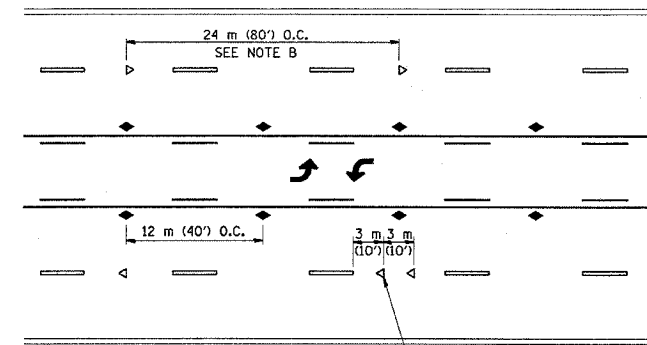


*** REDUCE TO 12 m (40') O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 70 km/h (45 M.P.H.) OR LESS.

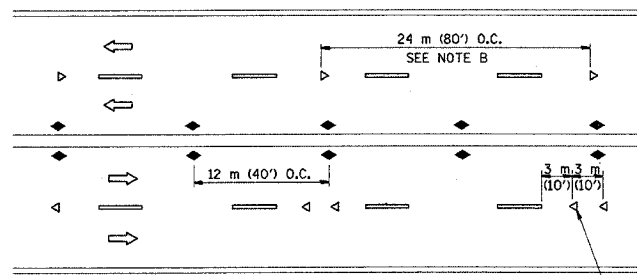
TWO-LANE/TWO-WAY



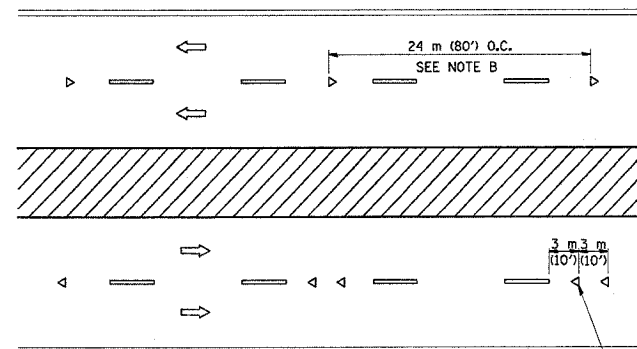
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

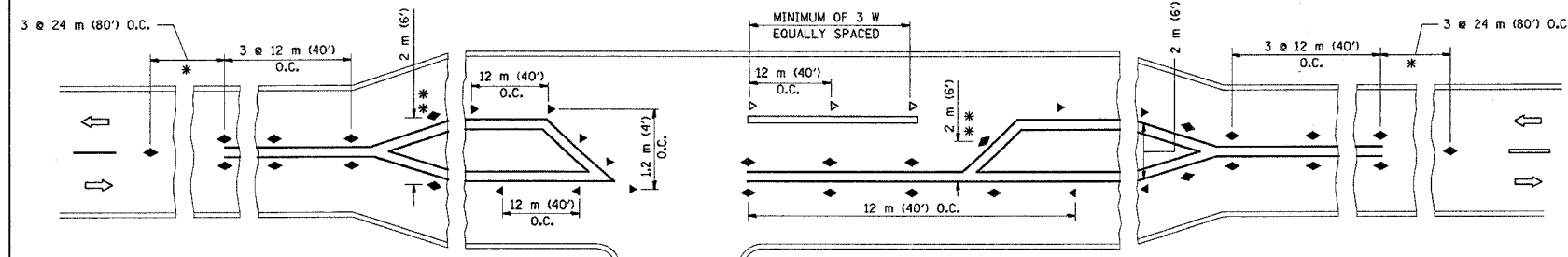
1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 50 TO 75 (2 TO 3') TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 150 m (500') IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 12 m (40') O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 20 km/h (10 M.P.H.) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 2 m (6') OR LESS USE TWO-WAY MARKERS.

RAISED REFLECTIVE PAVEMENT MARKERS: Raised reflective pavement markers shall be installed as indicated on the I.D.O.T. Typical Applications Standard "Typical Applications for Raised Reflective Pavement Markers (Snow-Plow Resistant)". All raised reflective pavement markers used with dashed lines shall be centered in the gap between segments, and offset 4 inches from the centerline of the dash to match the exiting markers on the County Highway.

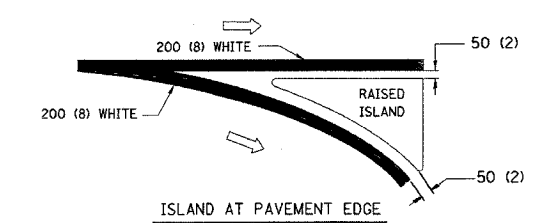
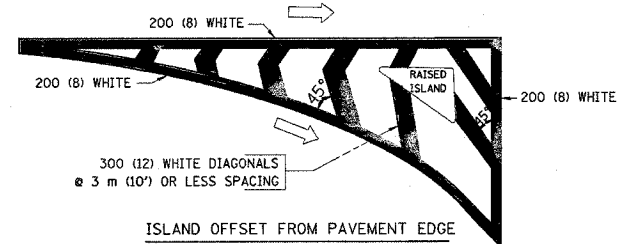
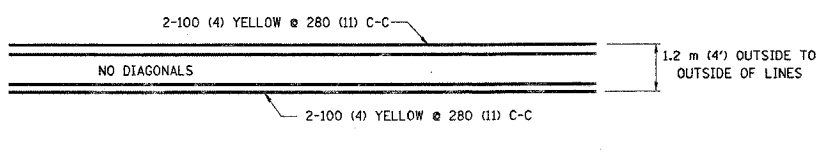
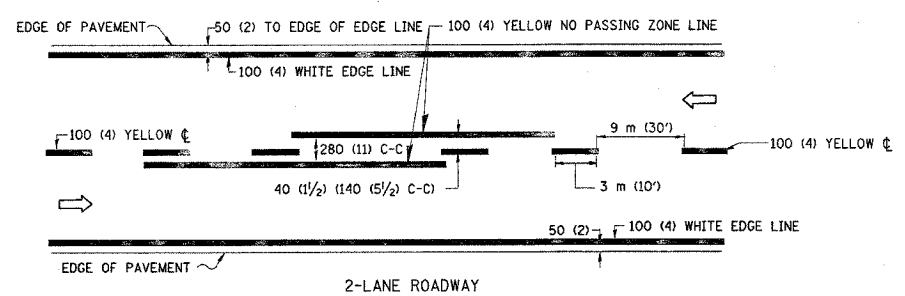
All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS
 (SNOW-PLOW RESISTANT)

SCALE: NONE
 DATE: 9/16/2004
 DRAWN BY CADD
 CHECKED BY

F. & R.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	41
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	3 m (10') LINE WITH 9 m (30') SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	280 (11) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	100 (4) 2 @ 100 (4)	SOLID SOLID	YELLOW YELLOW	140 (5 1/2) C-C FROM SKIP-DASH CENTERLINE OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	3 m (10') LINE WITH 9 m (30') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 m (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5 1/2) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 600 (2') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° NO DIAGONALS USED FOR 1.2 m (4') WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=50.33m ² (3.6 SQ. FT.) EACH "X"=5.0 m ² (54.0 SQ. FT.)
SHOULDER DIAGONALS	300 (12) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

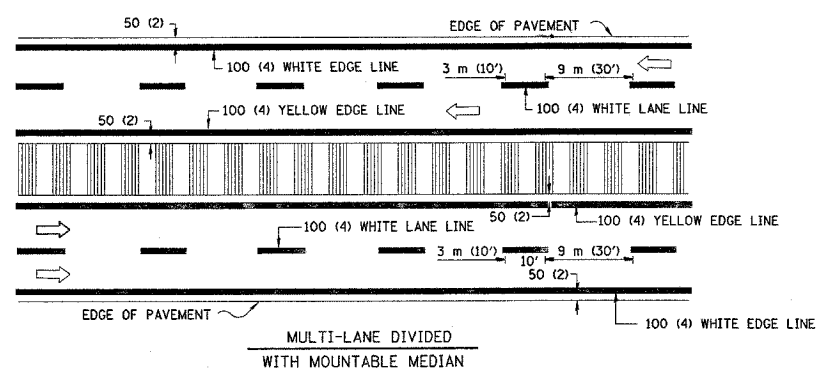
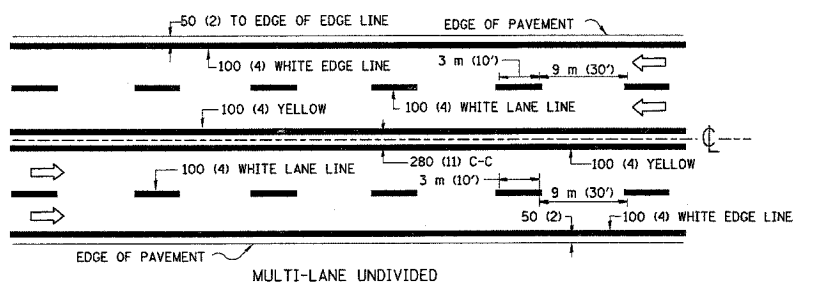
SCALE: NONE
DATE: 9/16/2004

DRAWN BY: CADD
CHECKED BY:
TC-13

REVISIONS

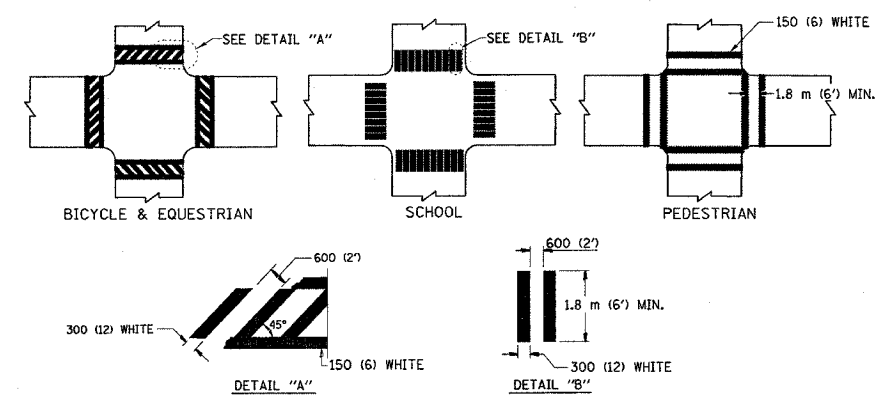
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

REVISION DATE: 01/06/00

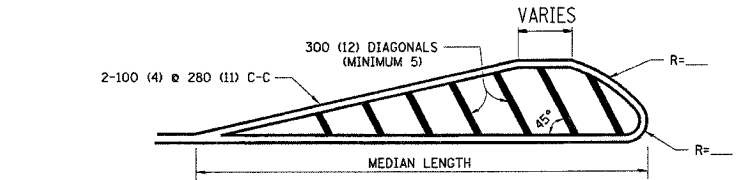


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



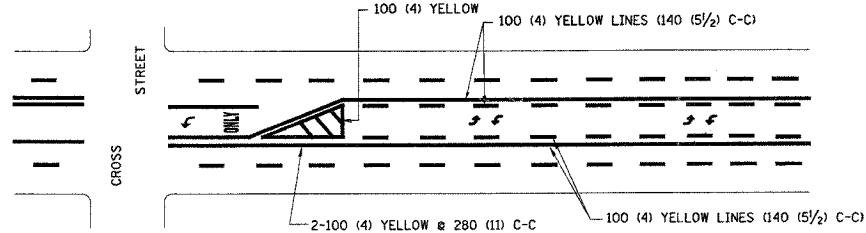
TYPICAL CROSSWALK MARKING



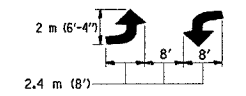
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 15 m (50') C-C (LESS THAN 50 km/h (30 MPH))
25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH))
45 m (150') C-C (MORE THAN 70 km/h (45 MPH))

MEDIANS OVER 1.2 m (4') WIDE

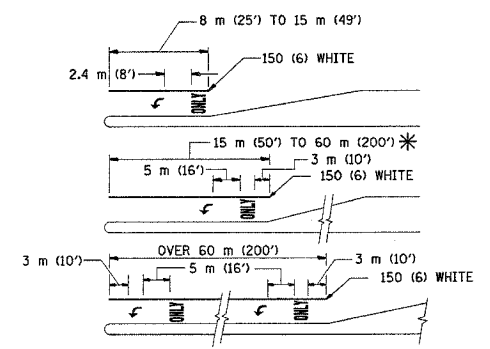


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 60 m (200') TO 90 m (300') INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



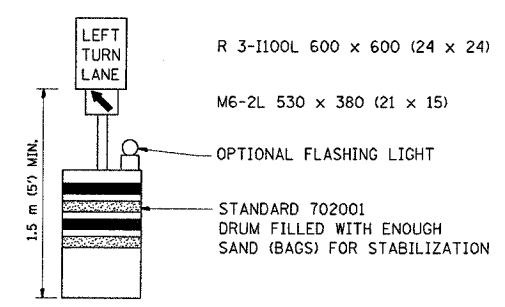
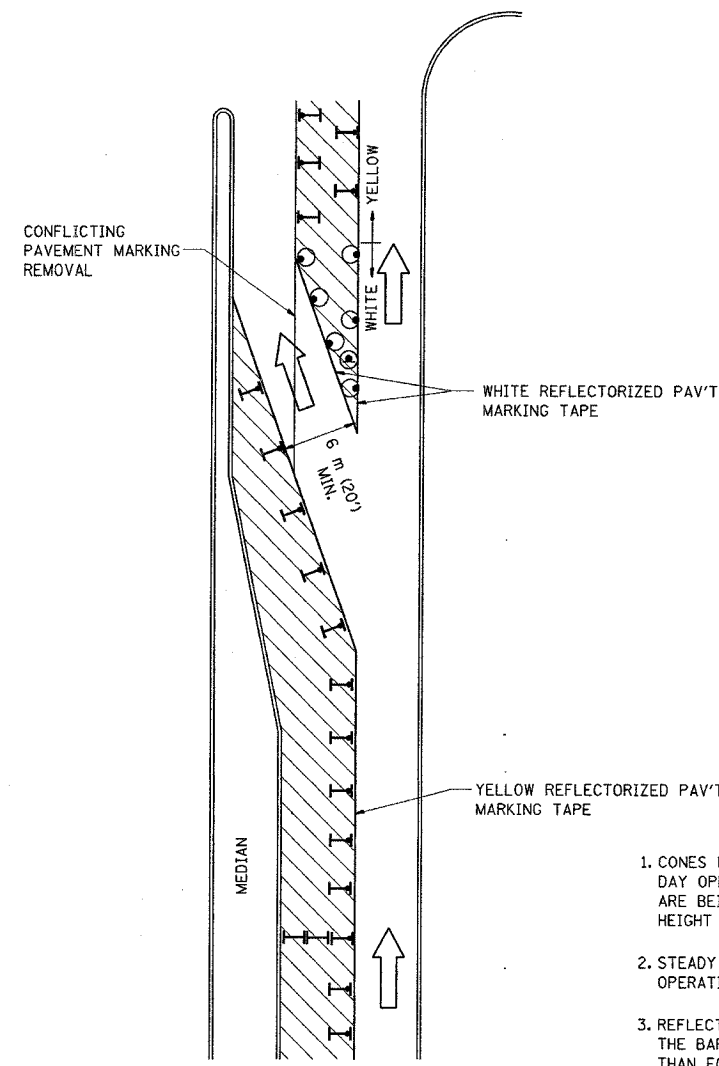
FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED.
AREA = 1.5 m² (15.6 SQ. FT.) AREA = 1.9 m² (20.8 SQ. FT.)

* TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

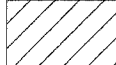
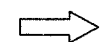




F. A. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	42
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 710 (28) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 1.5 m (5').
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 600 x 600 (24 x 24) AND M6-2R 530 x 380 (21 x 15) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

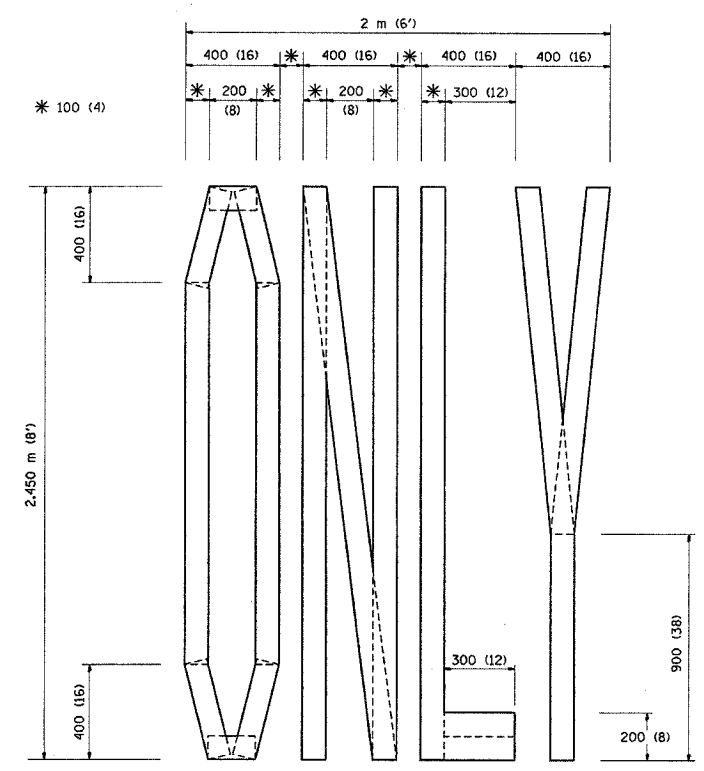
All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09/08/94
A. HOUSEH	11/07/95
A. HOUSEH	10/12/96
T. RAMMACHER	01/06/00

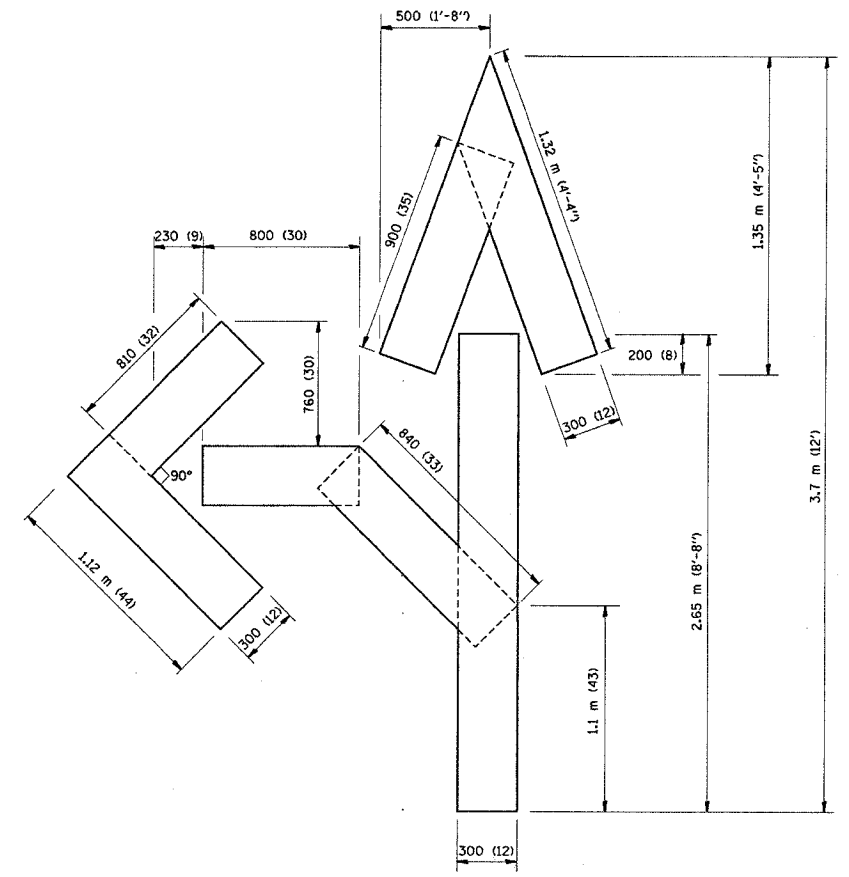
ILLINOIS DEPARTMENT OF TRANSPORTATION
**TRAFFIC CONTROL AND PROTECTION
 AT TURN BAYS
 (TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE
 DATE: 9/16/2004
 DRAWN BY
 CHECKED BY LHA
 TC-14

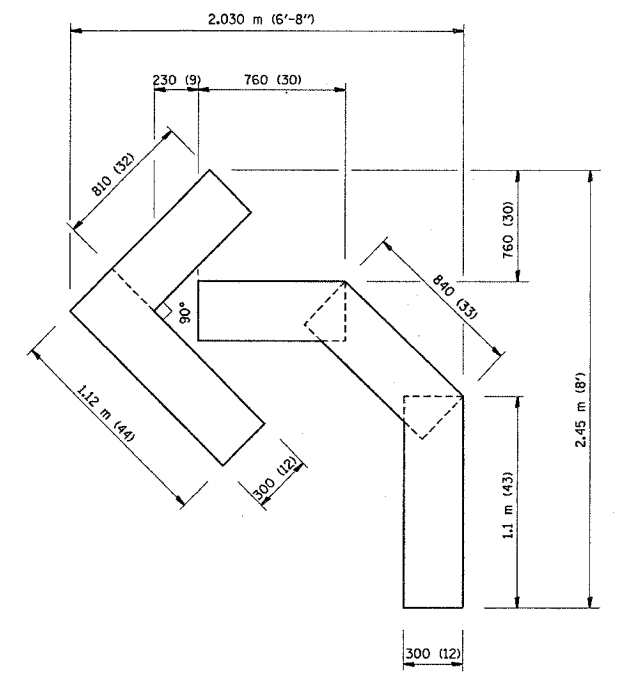
F. & A. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	43
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



QUANTITY
 100 (4) LINE = 19.7 m (64.1 ft.)
 1.97 sq. m (21.1 sq. ft.)



QUANTITY
 100 (4) LINE = 25.3 m (82.5 ft.)
 2.53 sq. m (27.5 sq. ft.)



QUANTITY
 100 (4) LINE = 13.9 m (45.5 ft.)
 1.39 sq. m (15.2 sq. ft.)

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

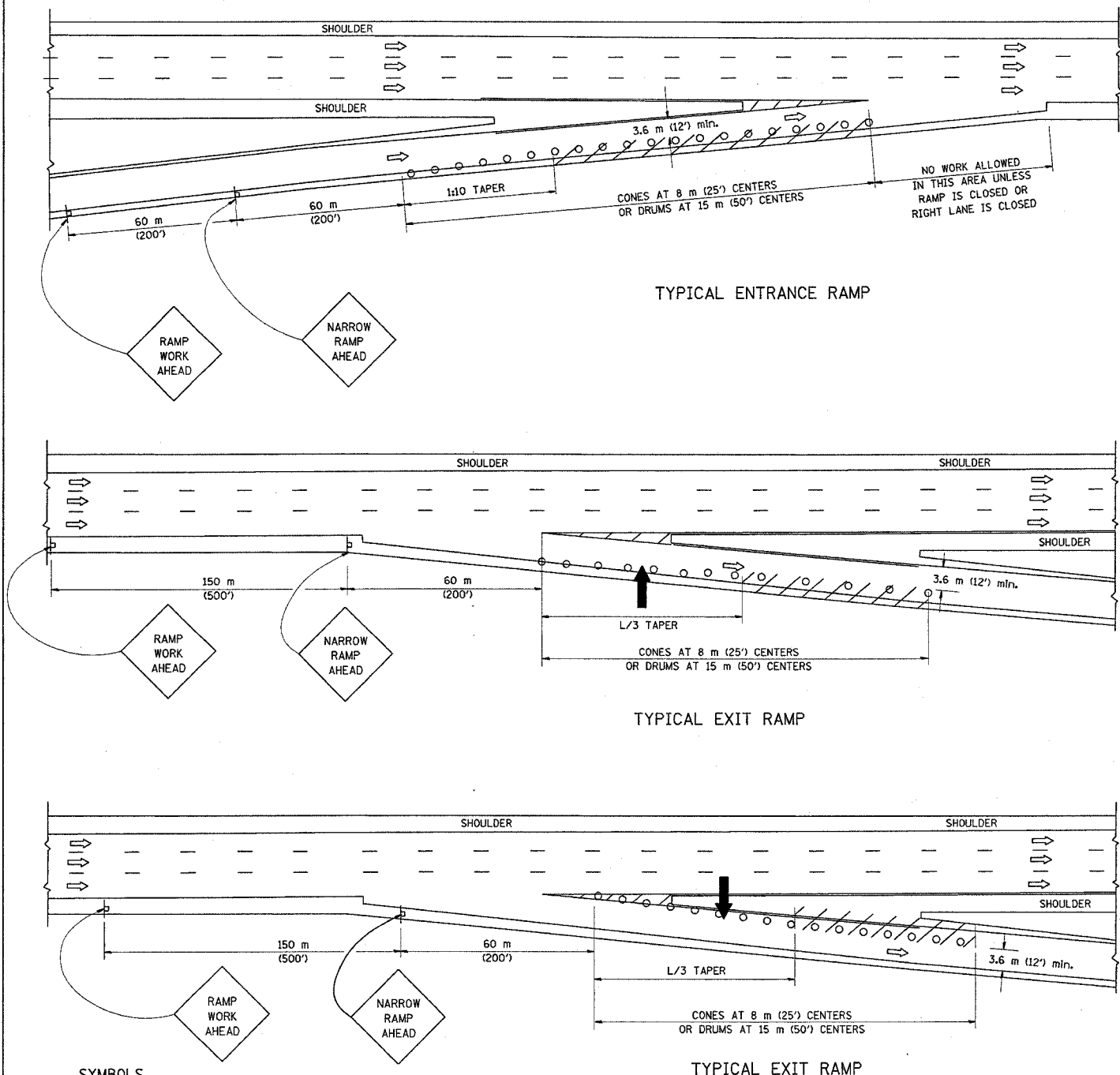
PAVEMENT MARKING
 LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

SCALE: NONE
 DATE 9/16/2004

DRAWN BY CADD
 CHECKED BY TC-16

PARTIAL RAMP CLOSURE DETAILS



SYMBOLS

- ARROWBOARD
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- CONES - 700 (28) IN HEIGHT

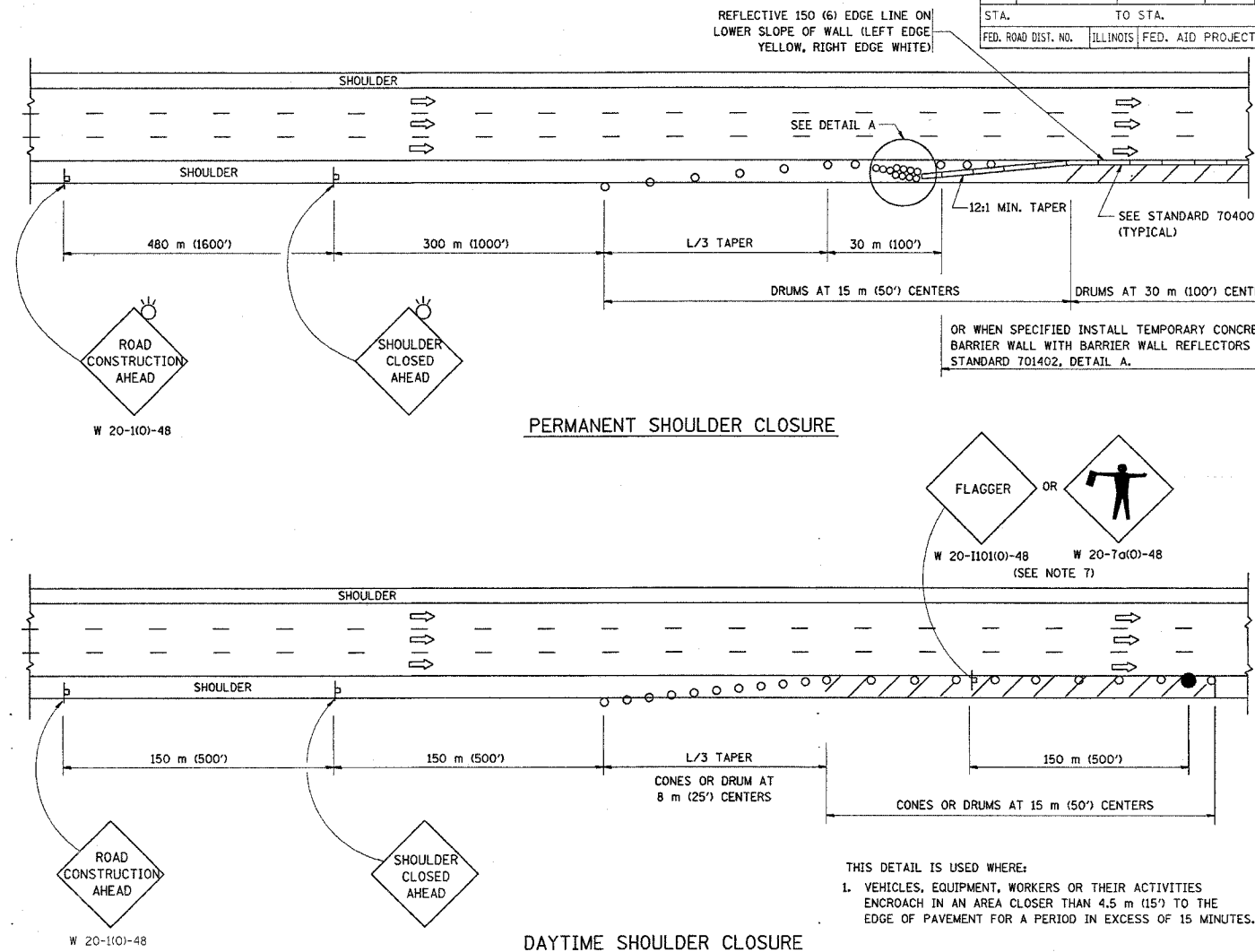
GENERAL NOTES

1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
80 km/h (45 mph) OR GREATER:	METRIC ENGLISH
	$L=0.65(W/S)$ $L=(W/S)$

W = WIDTH OF OFFSET IN METERS (FEET)
 S = NORMAL POSTED SPEED KM/H (MPH)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS



F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	44
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350 COMPLIANT FOR POSTED SPEED.

DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY (SEE NOTE 5)

REVISIONS	
NAME	DATE
DWS	11/96
JAF	12/02
NCHRP 350	04/03

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

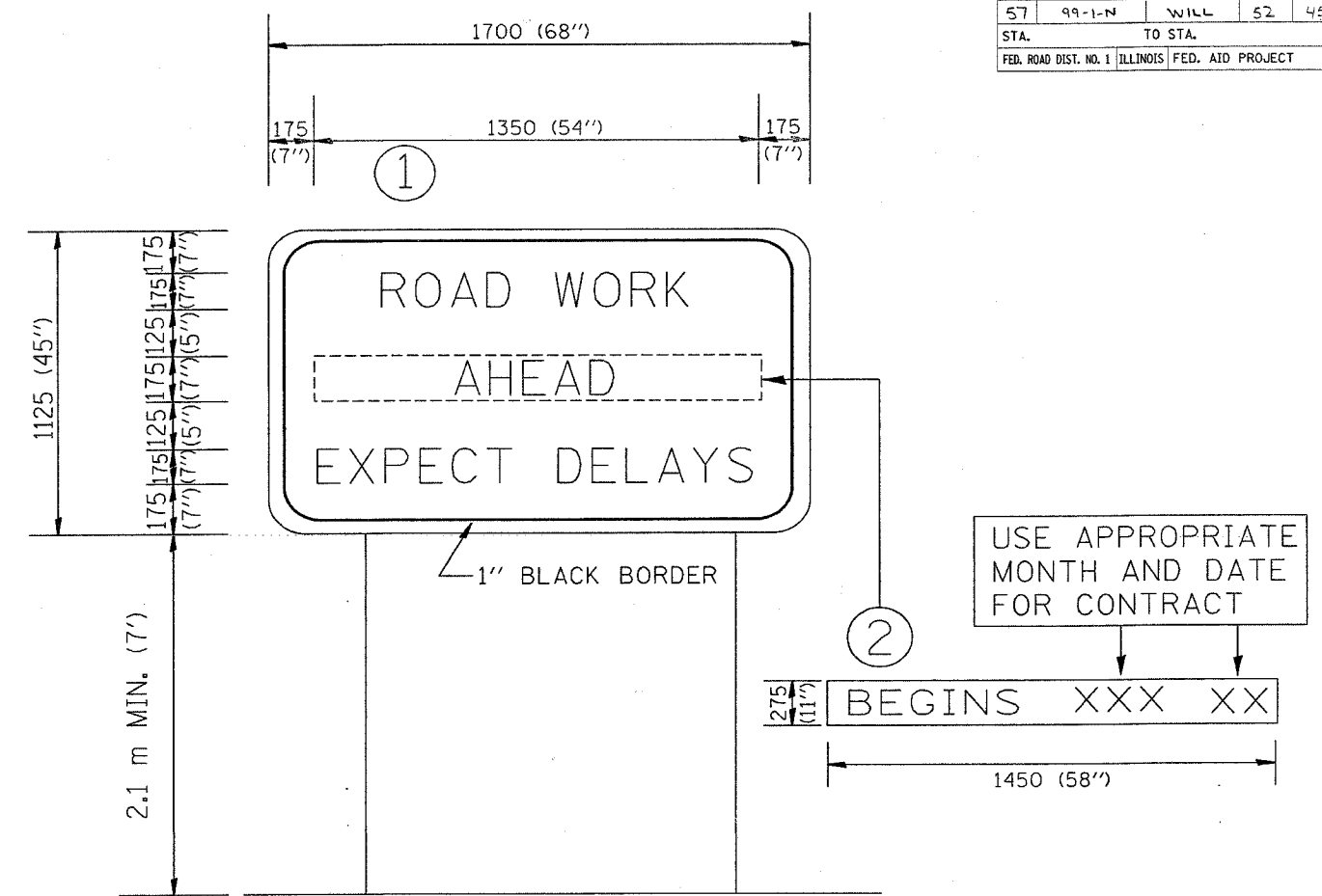
ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES PARTIAL RAMP CLOSURES

SCALE: NONE
DATE: 9/16/2004

DRAWN BY: DWS
DESIGNED BY: DWS
CHECKED BY:
TC-17

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	45
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 2.3 SQ. M. (25.70 SQ. FT.)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES)
UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. MIRS	9-15-97
R. MIRS	2-11-97
T. RAMMACHER	2-2-99

ILLINOIS DEPARTMENT OF TRANSPORTATION
TEMPORARY INFORMATION SIGNING

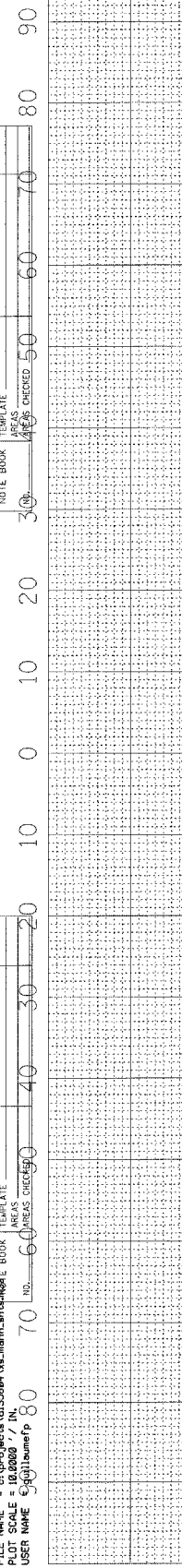
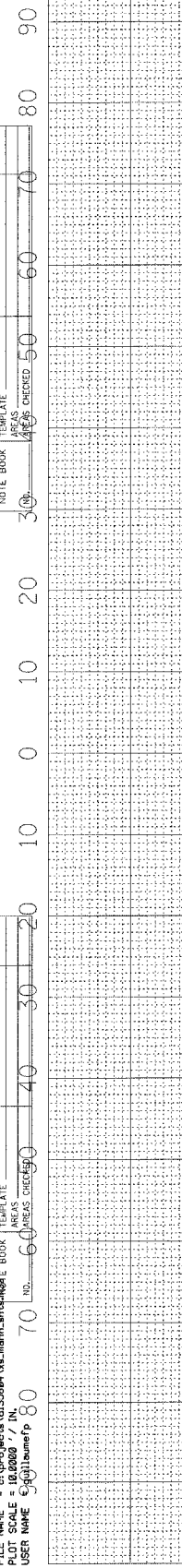
SCALE: DATE 9/16/2004

DRAWN BY: BUR. OF DESIGN
CHECKED BY

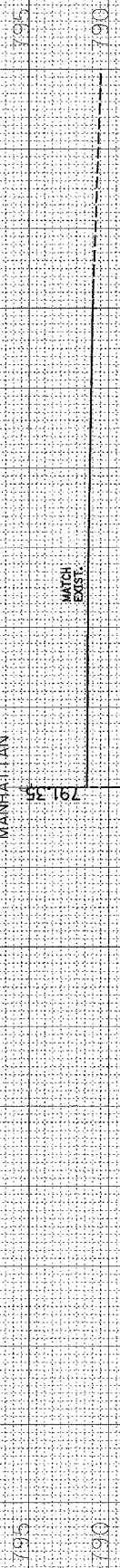
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 FILE NAME = c:\pcc\pccs\ad\355884\m.maha\p140288.dwg
 PLOT SCALE = 10.0000 / IN.
 USER NAME = jgallum\jg

ORIGINAL SURVEY BY DATE
 5" X 7" 1/2" 11/10/04
 SURVEY BOOK TEMPLATE
 29 AREAS
 60 AREAS CHECKED

FINAL SURVEY BY DATE
 SURVEY BOOK TEMPLATE
 29 AREAS
 50 AREAS CHECKED

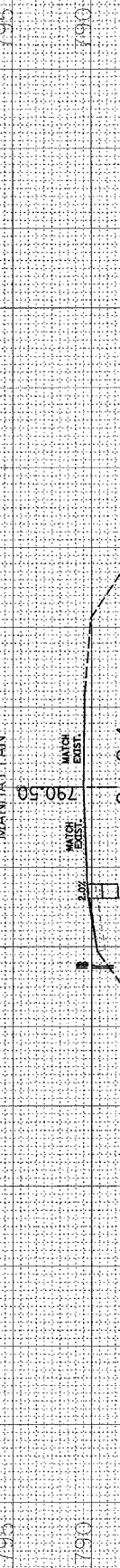


MANHATTAN



14030+43.00

MANHATTAN



14030+00.00

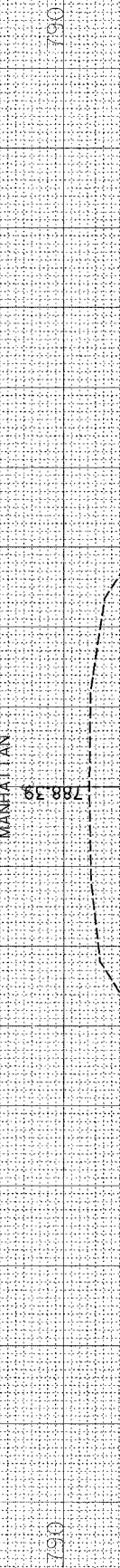
CF = 2.4
 FE = .25
 UNSUITABLE M. = 4.29

MANHATTAN



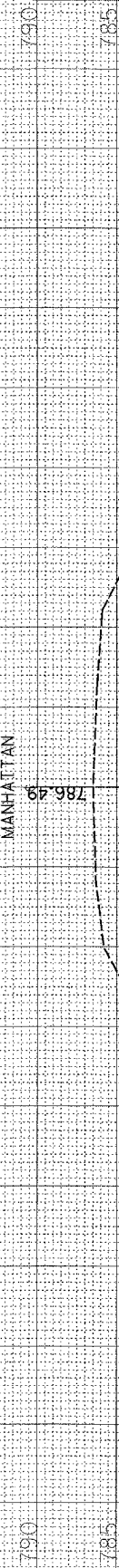
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MANHATTAN



14029+00.00

MANHATTAN



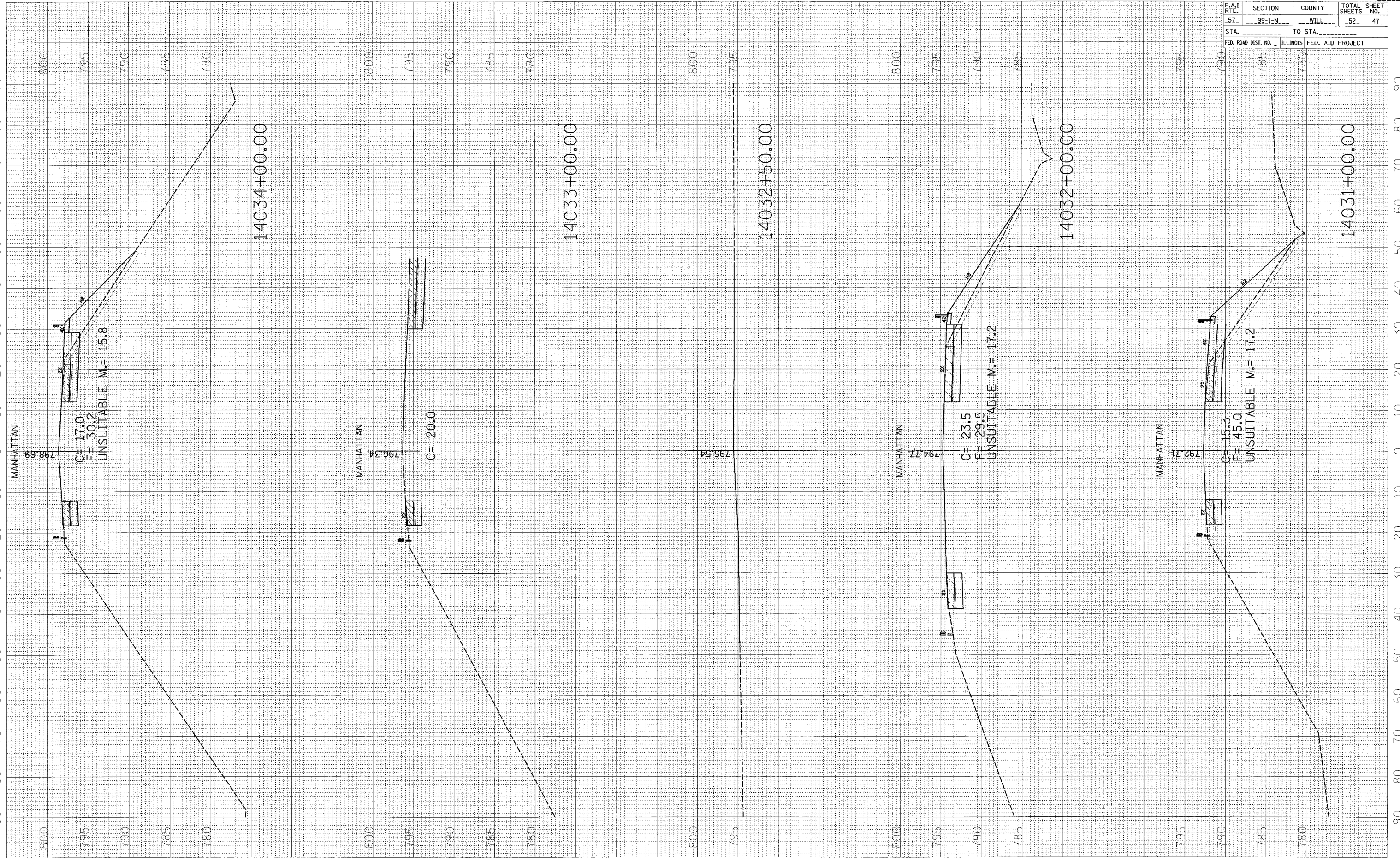
14028+00.00

CONTRACT NO.				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	46
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PLOT DATE = 10/3/2005
 FILE NAME = c:\projects\14032804\14032804.msh
 PLOT SCALE = 10.0000 / in
 USER NAME = g01100000p

ORIGINAL SURVEY
 SURVEYED BY
 P. ID
 DATE
 T.L. DATE
 AREAS CHECKED
 NO. 60 AREAS CHECKED 50

FINAL SURVEY
 SURVEYED BY
 PLOTTED
 DATE
 NOTE BOOK
 AREAS CHECKED
 NO. 60 AREAS CHECKED 50

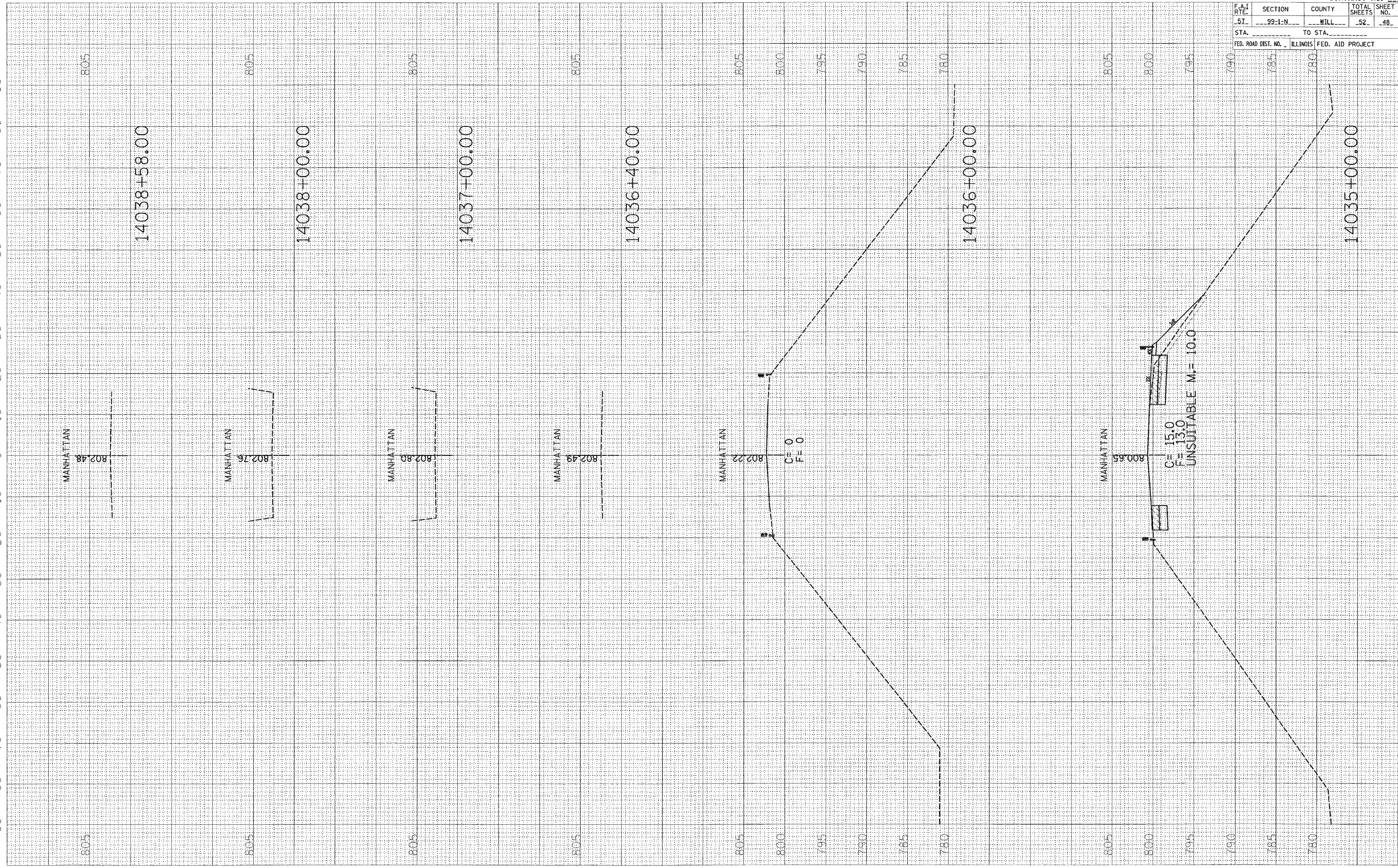


CONTRACT NO.				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	47
STA. _____		TO STA. _____		
FED. ROAD DIST. NO. _____		ILLINOIS FED. AID PROJECT		

PLOT DATE = 10/3/2005
 FILE NAME = c:\projects\140356\140356.dwg
 USER NAME = gcollins

OPTIONAL SURVEY PLOTTED BY DATE
 SURVEY PLOTTED BY DATE
 NOTE BOOK NO. 60 DATED 5/10/05 CREAMED 50

FINAL SURVEY PLOTTED BY DATE
 SURVEY PLOTTED BY DATE
 NOTE BOOK NO. 60 DATED 5/10/05 CREAMED 50



CONTRACT NO.

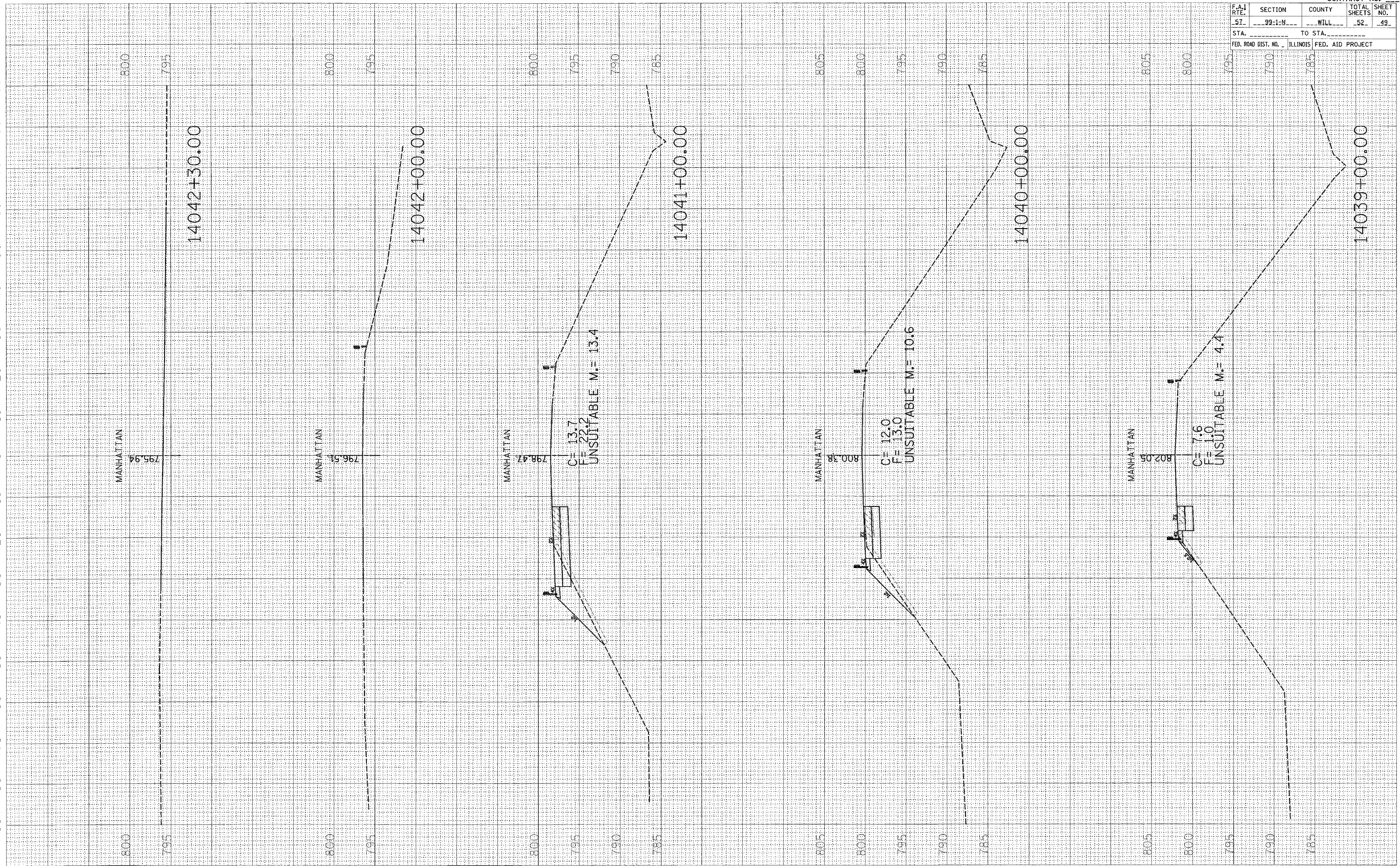
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	48

STA. _____ TO STA. _____
 FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT

PLOT DATE : 10/3/2005
 FILE NAME : c:\projects\14039\14039.dwg
 PLOT SCALE : 1/8"=1'-0"
 USER NAME : jglover

ORIGINAL SURVEY PLOTTED DATE
 SURVEY PLOTTED DATE
 NOTE BOOK NO. 60
 SHEETS CHECKED 50

FINAL SURVEY PLOTTED DATE
 SURVEY PLOTTED DATE
 NOTE BOOK NO. 60
 SHEETS CHECKED 50

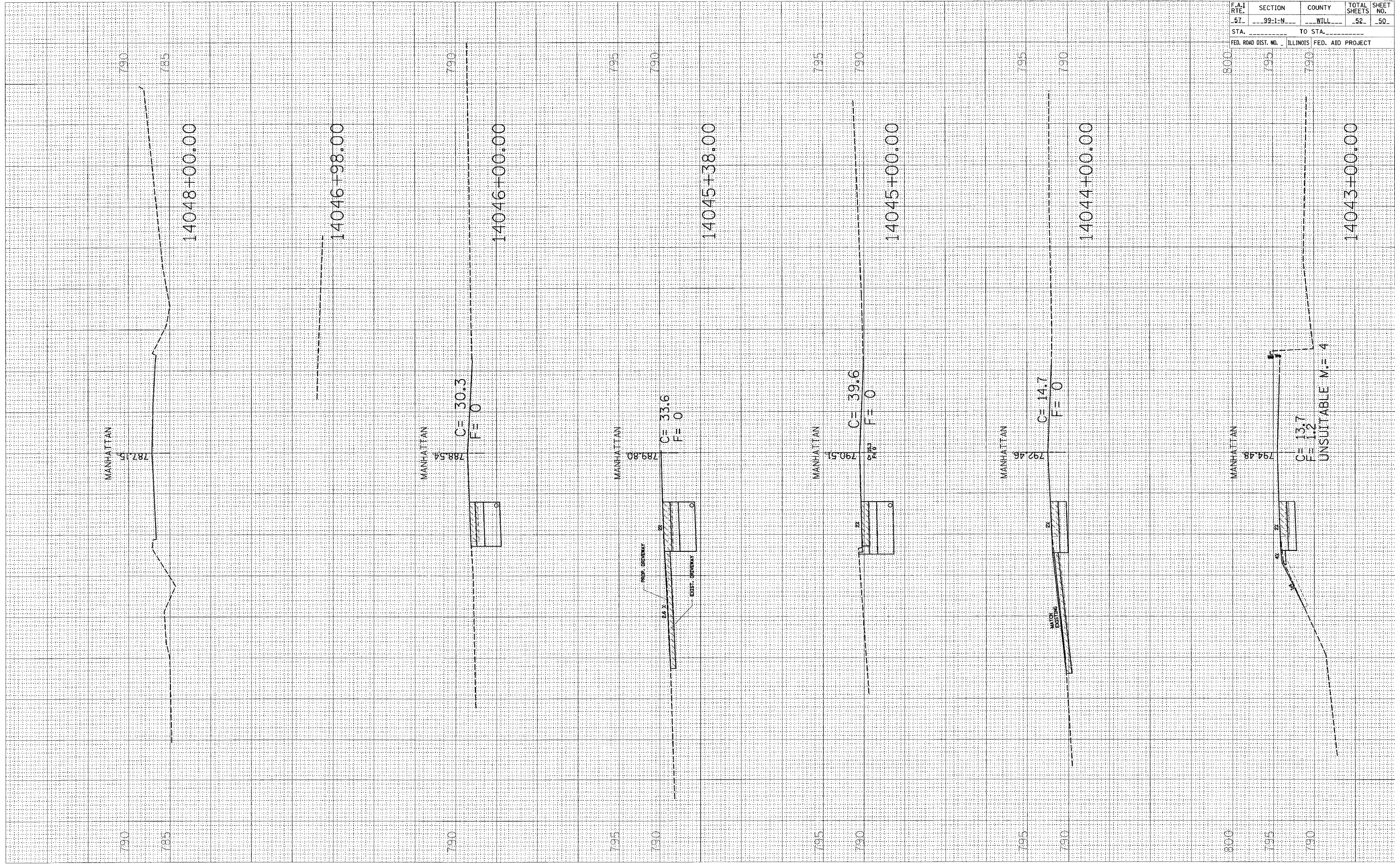


CONTRACT NO.				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-1-N	WILL	52	49
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

PLOT DATE = 10/15/2005
 FILE NAME = c:\p05\p05a\14043881\vs.manh.sh\14043881.p
 PLOT SCALE = 10.00000' / IN.
 USER NAME = jgalliumfp

ORIGINAL SURVEY BY DATE
 SURVEY PLATE
 NO. 60 AREAS CHECKED 40 30 20 10 0 10 20 30 40 50 60 70 80 90

FINAL SURVEY BY DATE
 SURVEY PLATE
 NO. 46 AREAS CHECKED 30 20 10 0 10 20 30 40 50 60 70 80 90

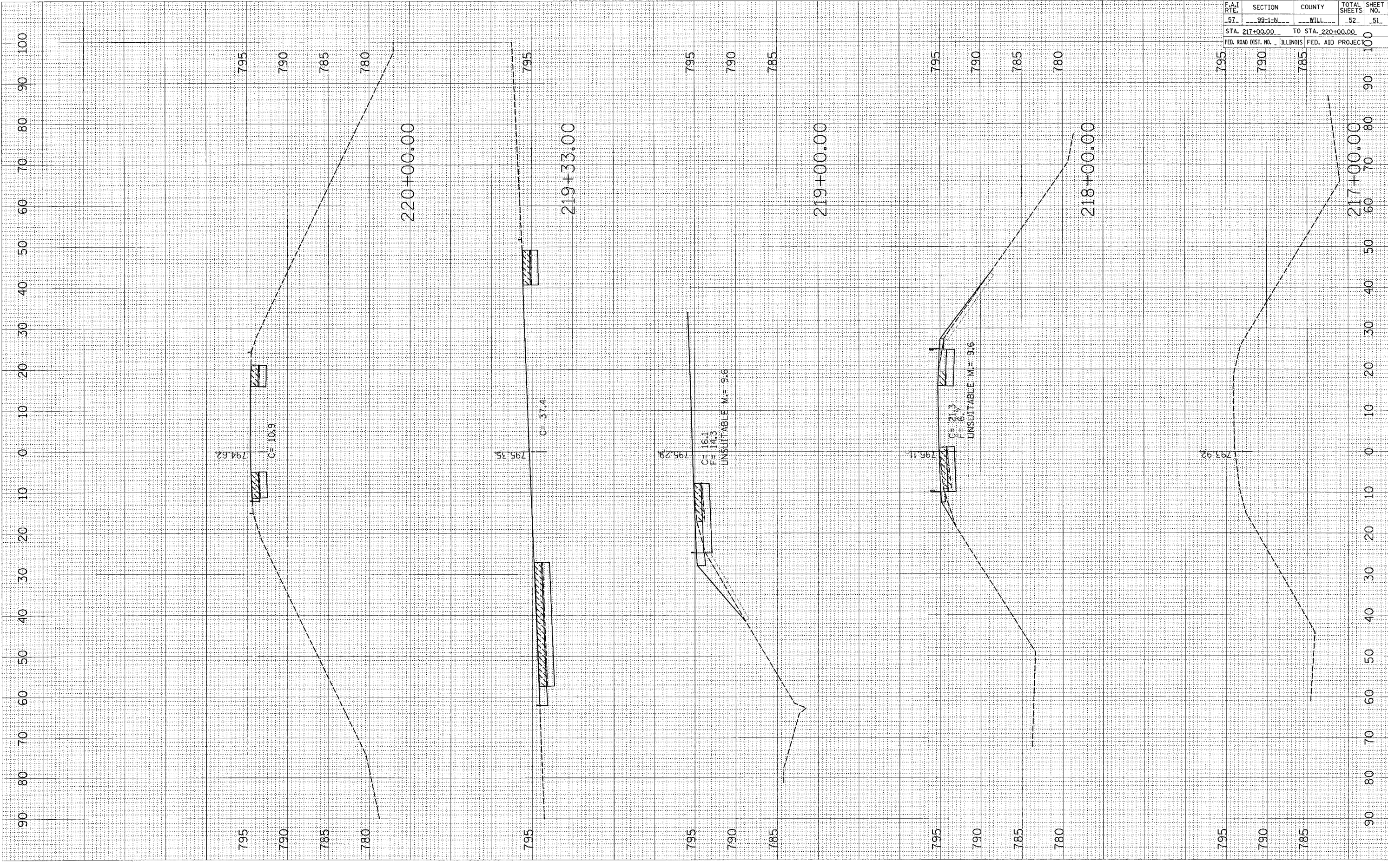


CONTRACT NO.			
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS
57	99-1-N	WILL	52
SHEET NO.		50	
STA.		TO STA.	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			

PLOT DATE = 12/15/2008
 FILE NAME = c:\p00\ecoa\dl3838\va\comp\ah3838\p1.dwg
 PLOT SCALE = 12.00000 / IN.
 USER NAME = guillaumefp

ORIGINAL SURVEY SKETCHED
 SURVEY PLOTTED
 NOTE BOOK DATE
 NO. AREAS CHECKED

FINAL SURVEY SKETCHED
 SURVEY PLOTTED
 NOTE BOOK DATE
 NO. AREAS CHECKED



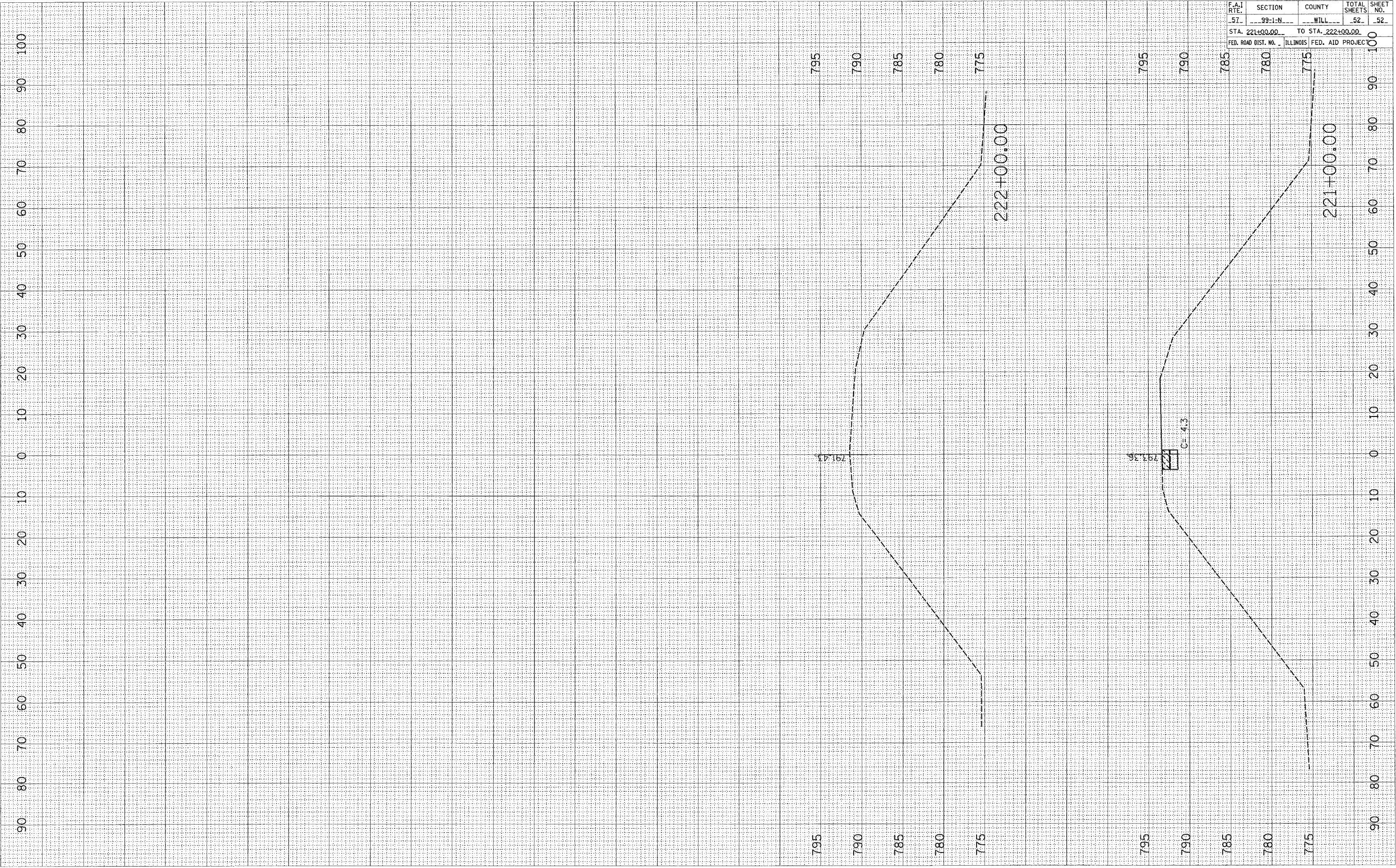
CONTRACT NO.			
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS
57	99-1-N	WILL	52
STA. 217+00.00		TO STA. 220+00.00	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			

SHEET NO.
51

PLOT DATE = 10/21/2005
 FILE NAME = c:\projects\13588A\as-imp-ab\m32\p13588a.dwg
 PLOT SCALE = 18.0000 / IN.
 USER NAME = gullonep

ORIGINAL SURVEY
 SURVEYED BY: _____
 DATE: _____
 CHECKED BY: _____
 NO. _____

FINAL SURVEY
 SURVEYED BY: _____
 DATE: _____
 CHECKED BY: _____
 NO. _____



CONTRACT NO.				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
99-1-N	WILL		52	52
STA. 221+00.00 TO STA. 222+00.00				
FED. ROAD DIST. NO. ILLINOIS			FED. AID PROJECT	