

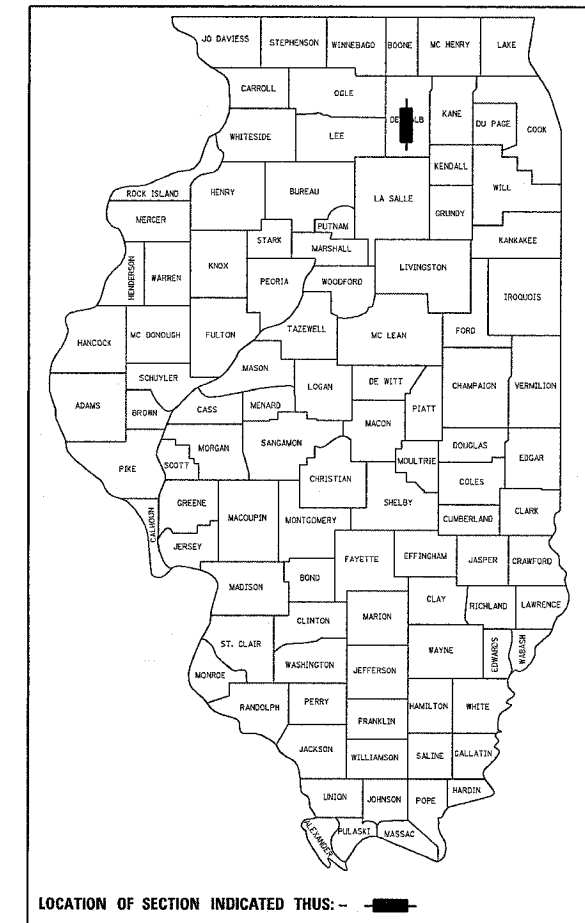
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68	29BR-1	DEKALB	25	1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED HIGHWAY PLANS

F.A.P. ROUTE 68 (IL 23)
SECTION 29BR - 1
PROJECT ACF-0068(096)
DEKALB COUNTY
C-92-152-05

D-92-054-04



PROJECT ENGINEER: BOB WAGNER

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR STATE STANDARDS, SEE SHEET NO. 2

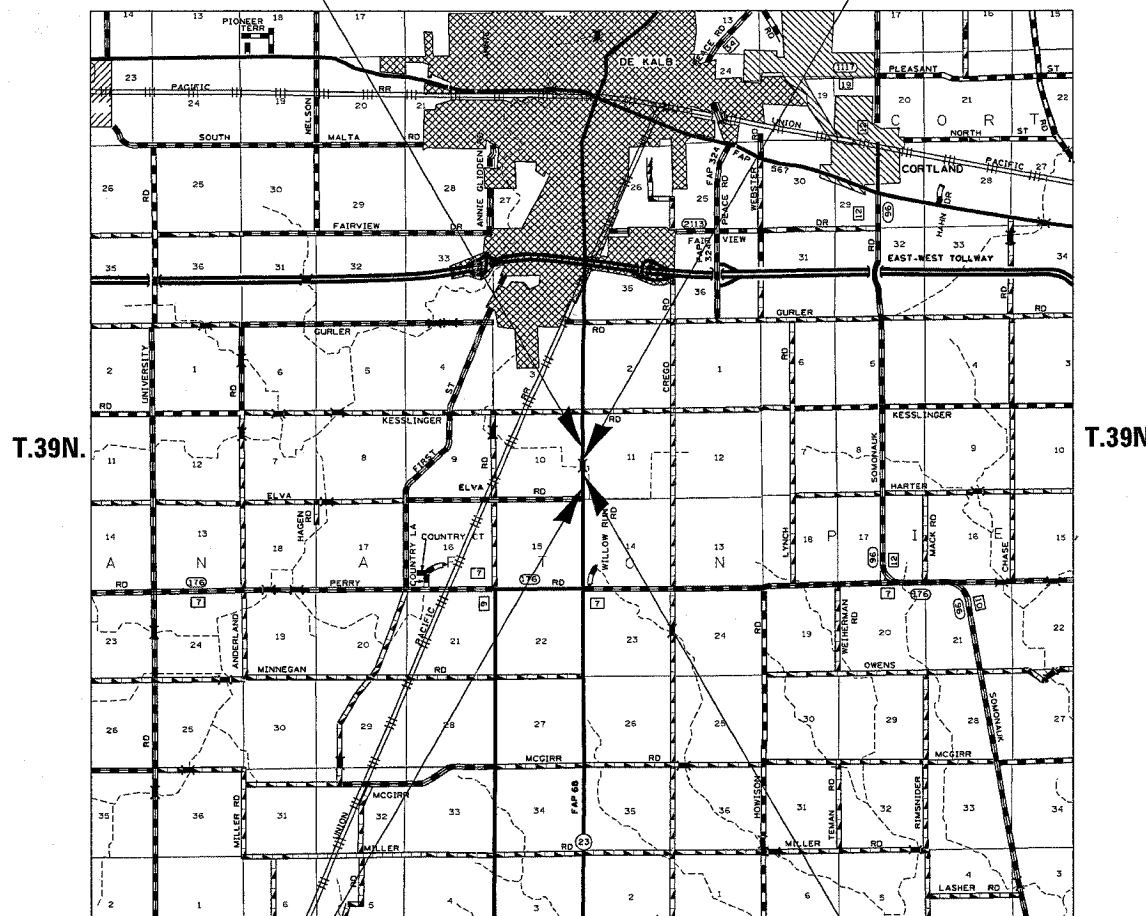
SENIOR SQUAD LEADER: SAMEER ABDULLAH (815) 284 - 5935

AFTON TOWNSHIP SECTION - 10 & 11

IMPROVEMENT ENDS
STA. 878 + 14.21

R.4E.

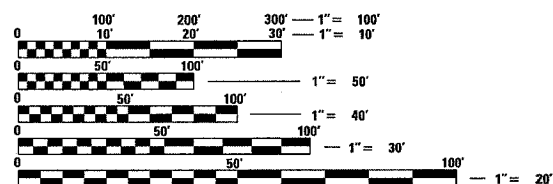
SECTION ENDS
STA. 877 + 27.25



IMPROVEMENT BEGINS
STA. 875 + 14.05

R.4E.

SECTION BEGINS
STA. 875 + 70.25



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

CONTRACT NO. 64A06

NET LENGTH OF PROJECT=157 LIN. FT=0.03 MILES
GROSS LENGTH OF PROJECT=157 LIN. FT=0.03 MILES

FAP ROUTE 68 (IL 23)

SECTION 29BR - 1

DEKALB COUNTY

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED August 24, 2005
Gregory J. Montanari
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

October 14, 2005
Mike Hine
ENGINEER OF DESIGN AND ENVIRONMENT

October 14, 2005
Eric F. Harms
DEPUTY DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

SUMMARY OF QUANTITIES

F.A.P. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68	298R-1	DEKALB	25	3
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

CODE NUMBER	ITEM	UNIT	80% FED 20% STATE		SFTY-3N
			TOTAL QUANTITY	X080-2A 80% FED/ 20% STATE	
20200100	EARTH EXCAVATION	CU YD	20	20	
20400800	FURNISHED EXCAVATION	CU YD	10	10	
40600980	BITUMINOUS SURFACE REMOVAL - BUTT JOINT	SQ YD	241	241	
42001300	PROTECTIVE COAT	SQ YD	266	266	
48101200	AGGREGATE SHOULDERS, TYPE B	TON	72	72	
50101500	REMOVAL OF EXISTING SUPERSTRUCTURES	EACH	1	1	
50300260	BRIDGE DECK GROOVING	SQ YD	249	249	
X5030305	CONCRETE WEARING SURFACE, 5"	SQ YD	254	254	
50301245	FORMED CONCRETE REPAIR (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	187	187	
50400305	PRECAST PRESTRESSED CONCRETE DECK BEAMS (17" DEPTH)	SQ FT	2380	2380	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	3130	3130	
50901005	STEEL BRIDGE RAIL, TYPE SM	FOOT	109	109	
51500100	NAME PLATES	EACH	1	1	
59000100	EPOXY CRACK SEALING	FOOT	117	117	
63100087	TRAFFIC BARRIER TERMINAL, TYPE 6A	EACH	4	4	
63200310	GUARDRAIL REMOVAL	FOOT	174	174	
63301210	REMOVE AND RE - ERECT STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	75	75	
63500105	DELINEATORS	EACH	3	3	
66700305	PERMANENT SURVEY MARKERS, TYPE II	EACH	2	2	
67000400	ENGINEERS FIELD OFFICE, TYPE A	CAL MO	3	3	
67100100	MOBILIZATION	L SUM	1	1	
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	1	1	
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1	
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1	1	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	588	588	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	516	516	
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	2276	2276	
78200410	GUARDRAIL MARKERS, TYPE A	EACH	16	16	
78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	2	2	
78300500	PAINT PAVEMENT MARKING REMOVAL	SQ FT	340	340	
X4066424	BITUMINOUS SURFACE COURSE, SUPERPAVE, MIX "D", N50	TON	63	63	
X6330103	REMOVE AND RE - ERECT TRAFFIC BARRIER TERMINAL, TYPE 1 SPECIAL, TANGENT	EACH	1	1	
X6330104	REMOVE AND RE - ERECT TRAFFIC BARRIER TERMINAL, TYPE 1 SPECIAL, FLARED	EACH	2	2	
Z0002600	BAR SPLICERS	EACH	55	55	
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON - REDIRECTIVE), TEST LEVEL 3	EACH	2		2
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON - REDIRECTIVE), TEST LEVEL 3	EACH	2		2

* SPECIALTY ITEMS

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GENERAL NOTES

ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FAP 68 (IL 23)	29BR-1	DeKalb	25	4
FED ROAD DIST. NO.	ILLINOIS	PROJECT		
Contract #64A06				

The final top 100 mm (four inches) of soil in any right-of-way area disturbed by the Contractor must be capable of supporting vegetation. The soil must be from the A horizon (zero to 2' deep) of soil profiles of local soils.

It is estimated that 7.6 cubic meters (10 cubic yards) of earth will be hauled to the job from outside the project limits. A shrinkage factor of 25% has been used.

The Contractor shall seed all disturbed areas within the project limits. Seeding Class 6 (modified) shall be used on front slopes and ditch bottoms. This work will be included in the contract unit price per Cubic Meter (Cubic Yard) for EARTH EXCAVATION.

Fertilizer shall be applied to all disturbed areas and incorporated into the seedbed prior to seeding or placement of sod at the rate specified in Sections 250 and 252 of the Standard Specifications. This work shall be included in the cost of EARTH EXCAVATION.

Mulch Method II shall be applied over all seeded areas. This shall be included in the cost of the EARTH EXCAVATION.

The following Mixture Requirements are applicable for this project:

Mixture Uses(s):	Mainline Surface Course & Top Shoulder		
PG:	PG 64-22		
RAP%: (Max)	15%		
Design Air Voids	4.2 @ N50		
Mixture Composition (Gradation Mixture)	IL 9.5 or 12.5		
Friction Aggregate	D		
20 Year ESAL	1.6		

Install a "TO ACTUATE SIGNAL" sign for the traffic signal detector loops. The detail of this sign is included in the plans. This work will be included in the cost of TRAFFIC CONTROL AND PROTECTION STANDARD 701321.

Bituminous and Aggregate prime coat shall be placed in accordance with Section 406 of the Standard Specifications. The cost of the prime coats shall be included in the contract unit price per metric ton (ton) for SURFACE COURSE of the type specified.

This structure will retain the same number 019-0007.

The contractor shall submit four copies of the required shop drawings for review and approval to the Bureau of Bridges and Structures, 2300 South Dirksen Parkway, Springfield, IL 62764. After approval of initial submittal, the contractor shall submit one set of shop drawings to Eric Harm, Engineer of Materials, 126 East Ash Street, Springfield, IL 62706, and eight (8) sets of shop drawings to be distributed to:

- District 2 District Engineer (1)
- Fabricator (1)
- Contractor (2)
- Resident Engineer (2)
- District 2 Bureau of Materials (2)

A quantity of 7.6 Cubic Meter (10 Cubic Yards) of Furnish Excavation has been included to further build up the shoulders, if the Engineer determines that the excavated materials from the job are insufficient to bring the shoulders to the proper slope.

One 16d galvanized nail shall be used to toe nail the wood block out to the wood post on all Traffic Barrier Terminal Type I Specials.

Delineators shall be installed as shown in Standard 635001, except that the post shall be rotated 180° and only metal-backed delineators shall be permitted.

Delineators shall be placed at the ends of approach guardrail terminal sections, and at each headwall or end section of AR Culverts. This work will be paid for at the contract unit price each for DELINEATORS.

Pavement Marking shall be done according to Standard 780001.

PERMANENT SURVEY MARKERS, TYPE II, shall be set at intervals of 1.6 Km (1 mile) or as directed by the Engineer. Bridge or culvert projects shall have one survey marker placed near the structure. Estimated: 2 Each.

Permanent Survey Markers, Type II shall be cast-in-place as shown on Highway Standard 667101.

The Contractor shall submit to the Engineer a description of location, elevation, and coordinates for each permanent survey marker. The Engineer shall submit this information to the Survey Crew.

The Contractor shall be responsible for protecting utility property during construction operations as outlined in Article 107.31 of the Standard Specifications. A minimum of 48 hours advance notice is required for non-emergency work. The JULIE number is 800-892-0123. The following listed utilities located within the project limits or immediately adjacent to the project construction limits are members of JULIE:

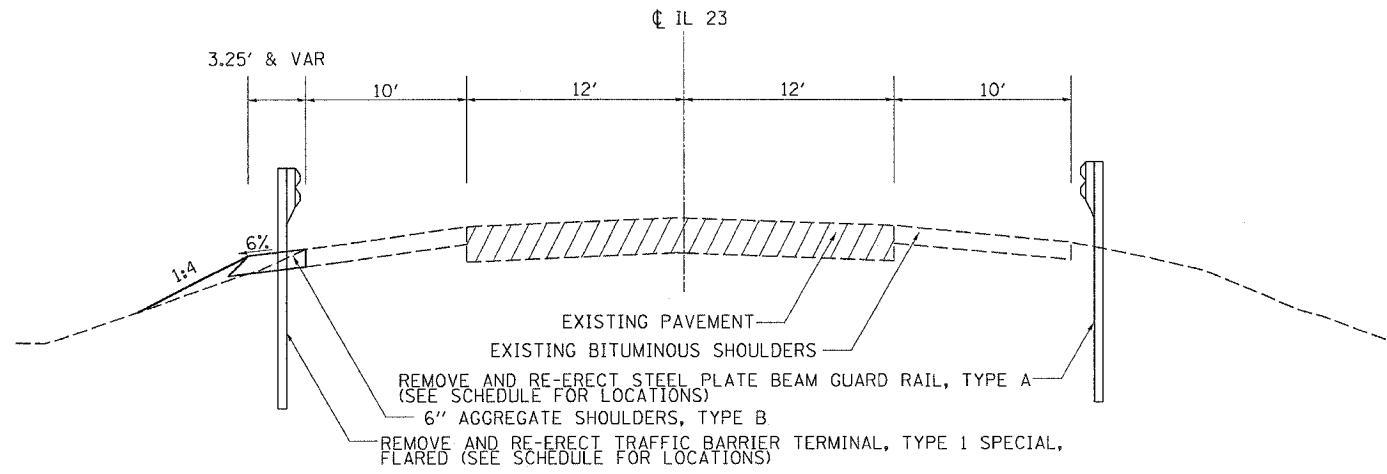
- Commonwealth Edison Co.
- NICOR Gas Co.
- Verizon

CADD data will be available to Contractors and Consultants working on this project. This information will be provided upon request as MicroStation CADD files and Geopak coordinate geometry files ONLY. If data is required in other formats it will be your responsibility to make these conversions. If any discrepancy or inconsistency arises between the electronic data and the information on the hard copy, the information on the hard copy should be used. Contact the District's Project Engineer to request these files.

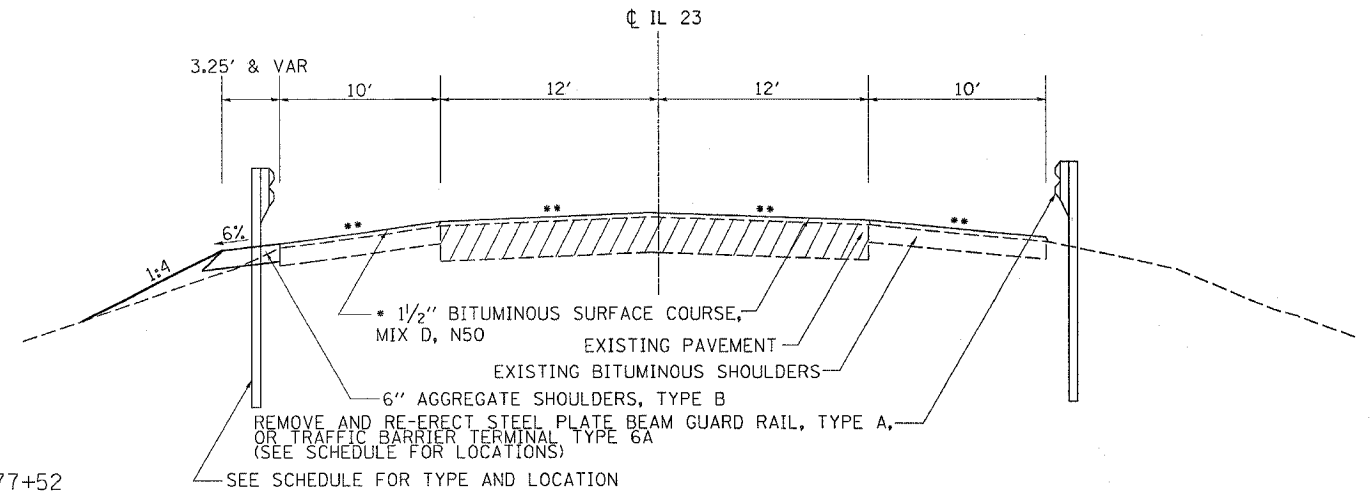
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(Arch. Size)
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200%
Enlarge 107%

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68	29BR-1	DEKALB	25	5
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

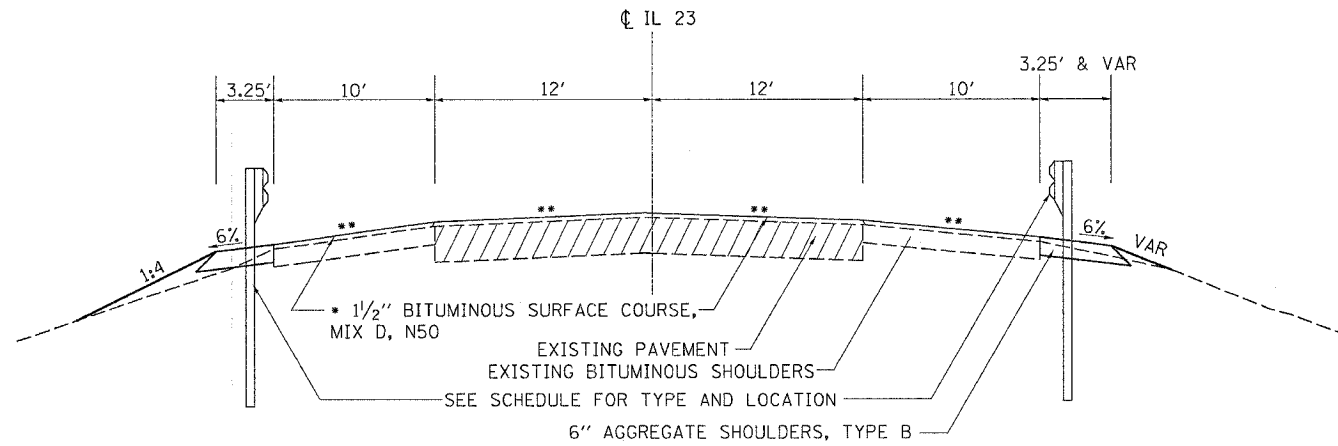
STA. 875+14 - STA. 875+45



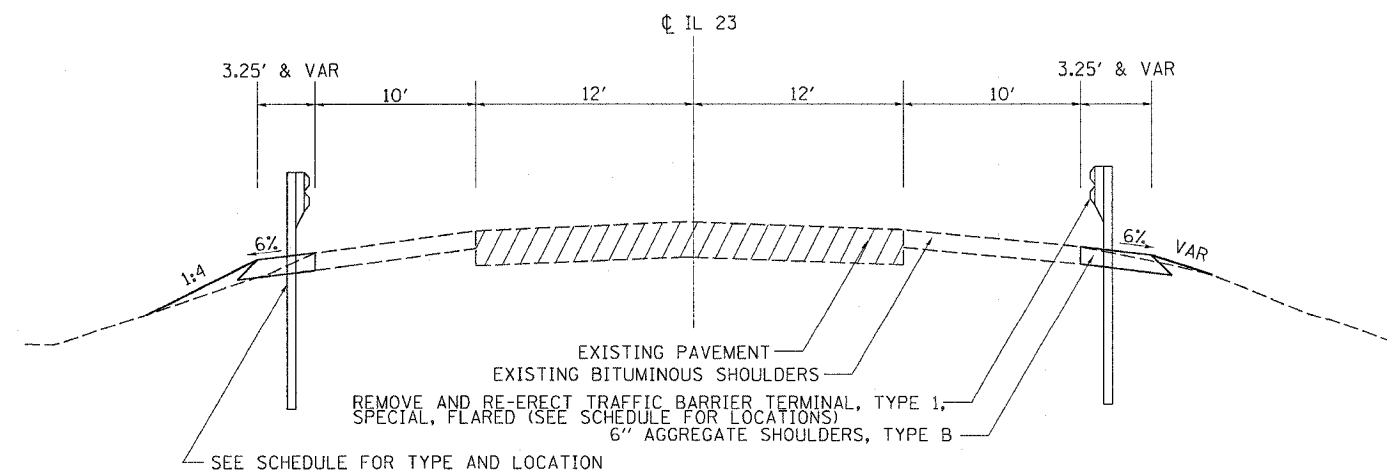
STA. 875+45 - STA. 876+43



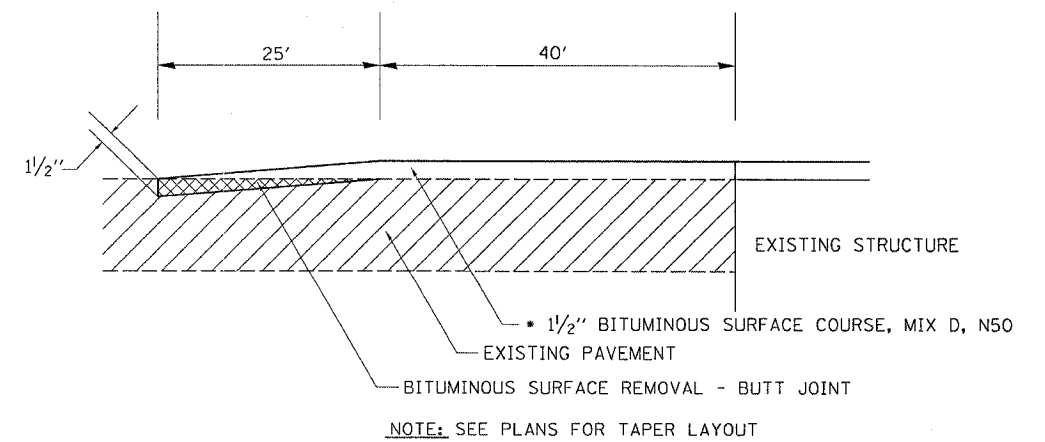
STA. 876+53 - STA. 877+52



STA. 877+52 - STA. 878+14



TYPICAL TAPER
 STA. 875+45 - STA. 876+43
 STA. 876+53 - STA. 877+52



- 112 LBS/ IN / SQ YD
- MATCH EXISTING CROSS SLOPE

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68	29BR-1	DEKALB	25	6
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

SCHEDULE OF QUANTITIES

20200100 EARTH EXCAVATION

CU YD	LOCATION			
3.00	IL 23	875+14	-	876+44
17.00	LT	876+52	-	878+14
20.00	LT&RT			
	TOTAL			

20400800 FURNISHED EXCAVATION

CU YD	LOCATION			
10.00	IL 23	876+52	-	878+14
10.00	LT			
	TOTAL			

40600980 BITUMINOUS SURFACE REMOVAL - BUTT JOINT

SQ YD	LOCATION			
121	IL 23	875+45	-	875+70
120	LT&RT	877+27	-	877+52
241	TOTAL			

48101200 AGGREGATE SHOULDERS, TYPE B

TONS	LOCATION			
30	IL 23	875+14	-	876+44
17	LT	876+52	-	877+82
25	RT	876+97	-	878+14
72	TOTAL			

63100087 TRAFFIC BARRIER TERMINAL, TYPE 6A

EACH	LOCATION			
1	IL 23	876+10	-	876+43
1	LT	875+66	-	875+99
1	RT	876+97	-	877+30
4	TOTAL	876+53	-	876+86

63200310 GUARDRAIL REMOVAL

FOOT	LOCATION			
34	IL 23	875+65	-	875+99
20	RT	875+77	-	875+97
34	LT	876+09	-	876+43
34	RT	876+53	-	876+87
19	LT	876+99	-	877+18
33	RT	876+97	-	877+30
174	TOTAL			

63301210 REMOVE AND RE-ERECT STEEL PLAT BEAM GUARD RAIL, TYPE A

FOOT	LOCATION			
25	IL 23	875+40	-	875+65
12	RT	875+97	-	876+09
12	LT	876+87	-	876+99
25	RT	877+30	-	877+56
75	TOTAL			

63500105 DELINEATORS

EACH	LOCATION			
1	IL 23	875+47		
1	LT	877+49		
1	RT	878+06		
3	TOTAL			

70400100 TEMPORARY CONCRETE BARRIER

FOOT	LOCATION			
588	IL 23	873+54	-	879+41 (STAGE 1)
588	LT			
	TOTAL			

70400200 RELOCATE TEMPORARY CONCRETE BARRIER

FOOT	LOCATION			
516	IL 23	873+90	-	879+05 (STAGE 2)
516	RT			
	TOTAL			

78001110 PAINT PAVEMENT MARKING - LINE 4" (TWO COATS)

FOOT	LOCATION			
476	IL 23	871+72	-	881+22 (YELLOW SKIP DASH)
1000	STA.	874+00	-	879+ (WHITE EDGE LINE)
800	RT	874+50	-	878+50 (WHITE EDGE LINE)
2276	TOTAL			

78200410 GUARDRAIL MARKERS, TYPE A

EACH	LOCATION			
4	IL 23	875+97	-	876+43
4	LT	875+40	-	875+99
4	RT	876+97	-	877+56
4	RT	876+53	-	877+00
16	TOTAL			

78201000 TERMINAL MARKER - DIRECT APPLIED

EACH	LOCATION			
1	IL 23	878+06		
1	LT	877+49		
2	TOTAL			

PAINT PAVEMENT MARKING REMOVAL

FOOT	LOCATION			
60	IL 23	871+72	-	874+00 STAGE 1
60	STA	879+00	-	881+22 STAGE 1
500	RT	874+00	-	879+00 STAGE 1
400	LT	874+50	-	878+50 STAGE 2
1020	TOTAL			

X4066424 BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE MIX D, N50

TON	LOCATION			
32	IL23	875+45	-	876+43
31	LT&RT	876+53	-	877+52
63	TOTAL			

X6330103 REMOVE AND RE-ERECT TRAFFIC BARRIER TERMINAL, TYPE 1 SPECIAL, TANGENT

EACH	LOCATION			
1	IL 23 FROM	877+50	-	878+00
	LT			
	TO			
1	RT	876+99	-	877+49
	TOTAL			

X6330104 REMOVE AND RE-ERECT TRAFFIC BARRIER TERMINAL, TYPE 1 SPECIAL, FLARED

EACH	LOCATION			
1	IL 23 FROM	875+27	-	875+77
	LT			
	TO			
1	RT	875+47	-	875+97
	FROM			
	RT	877+18	-	877+68
	TO			
2	LT	877+56	-	878+06
	TOTAL			

Z0030250 IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3

EACH	LOCATION			
1	IL 23	873+54		STAGE 1
1	LT	879+41		STAGE 1
2	TOTAL			

Z0030350 IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3

EACH	LOCATION			
1	IL 23	873+90		STAGE 2
1	RT	879+05		STAGE 2
2	TOTAL			

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 USER NAME = mgjllj

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	

SCALE: VERT. HORIZ.
DATE

DRAWN BY
CHECKED BY

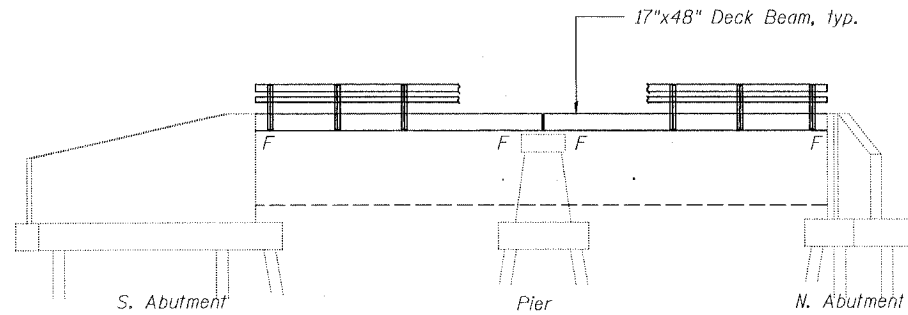
B.M.

Existing Structure:
Built as S.B.I. Route 23 Sec. 29 Sta. 911+60 in 1926, the original reinforced concrete superstructure was replaced with 17" PPC Deck Beams and the closed abutment substructure was widened in 1971. The superstructure is to be removed and replaced utilizing stage construction. No salvage.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. RT. 68 (IL 23)	29 BR-1	DEKALB	25	7
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

SHEET NO. 7
10 SHEETS

Contract #64A06



ELEVATION

INDEX OF SHEETS

1. General Plan
2. Stage Construction
3. Type SM Steel Bridge Rail Side Mounted
4. Concrete Wearing Surface
- 5-7. Superstructure Details
8. Pier and Abutment Repairs
9. Temporary Concrete Barrier For Stage Construction
10. Bar Splicer Assembly Details

GENERAL NOTES

Reinforcement bars shall conform to the requirements of AASHTO M 31 or M 322 Grade 60.

Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price for the work.

All Construction joints shall be bonded.

No in-stream work will be allowed on this project.

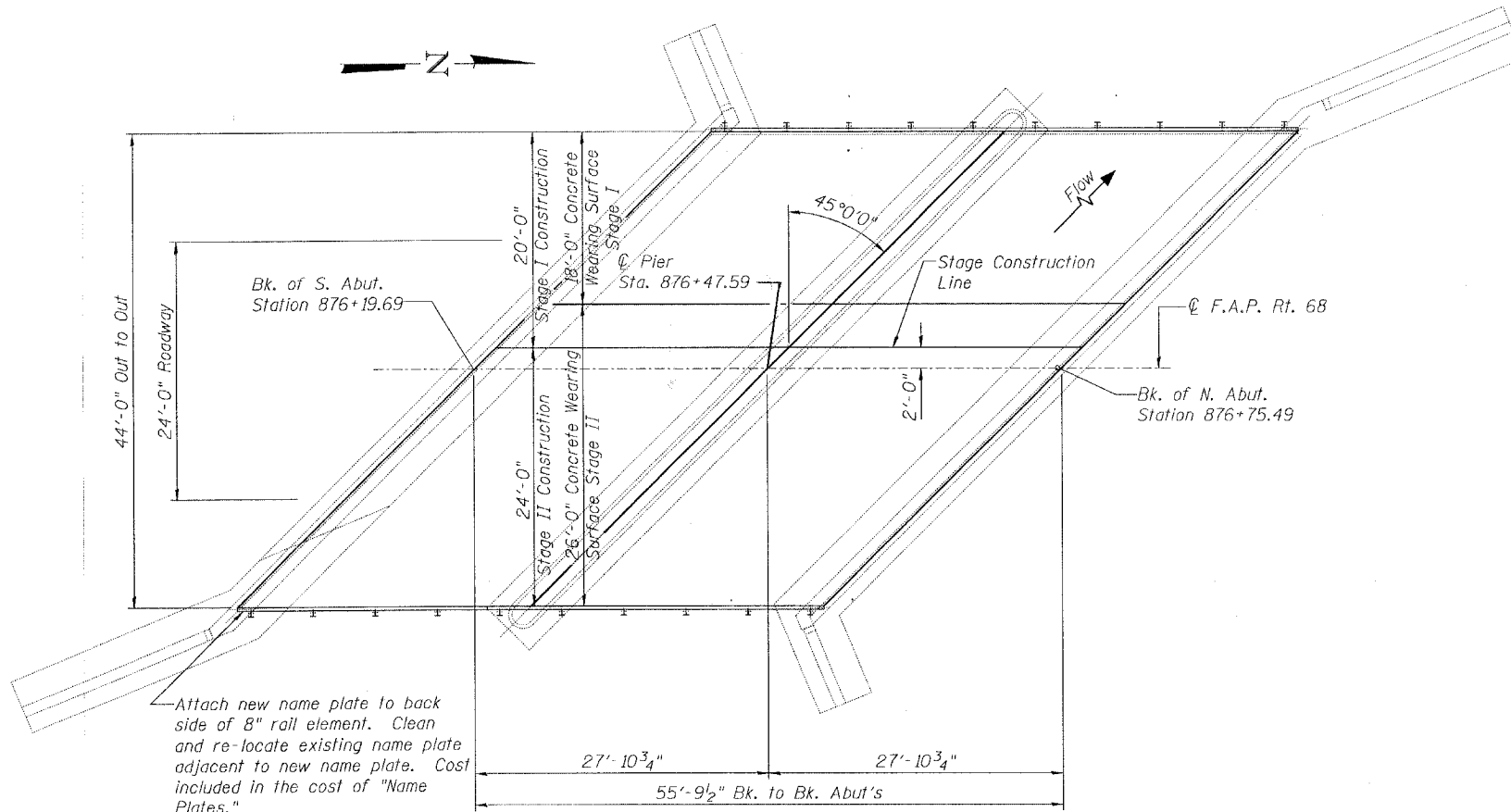
The cut strands at each beam end shall be given two coats of zinc dust spray or paint meeting the requirements of ASTM A780. The zinc dust spray or paint shall be applied before corrosion appears and allowed to dry according to the manufacturer's specifications prior to another coat of zinc. A concrete sealer meeting the requirements of Section 587 of the Standard Specifications shall be applied to the exterior face and 9" in on the underside of the fascia beams. The sealer shall be applied after visible crack growth has subsided. This work shall be performed by the producer and included with the cost of the beam.

The minimum thickness of the Concrete overlay shall be 5" and varies as required to adjust for the new profile grade and beam camber.

The Contractor is advised that the existing PPC Deck Beams are in a deteriorated condition with reduced load carrying capacity. It is the Contractor's responsibility to account for the condition of the beams when developing construction procedures for removal and replacement of the superstructure.

Repair of the abutments and pier caps shall be completed prior to placement of the new deck beams.

If the Contractor's procedure for existing beam removal or replacement of the new beams involves placement of cranes or other heavy equipment on new beams, a detailed procedure shall be submitted to the Engineer for approval. The procedure shall include calculations, prepared and sealed by an Illinois Licensed Structural Engineer, verifying that the equipment and procedure used will not overstress the new beams. To distribute load to multiple beams and protect the concrete, in all cases a double layer mat of heavy timbers shall be used at all times under crane tracks or wheels and any outriggers in the down position. If necessary, shims shall be used under the crane mat to ensure uniform contact with the underlying beams. If cranes or other heavy equipment will be placed on new beams prior to placement of the concrete wearing surface, it shall be done after the dowels rods are grouted and cured for 24 hours minimum and prior to grouting the shear keys. A temporary means of lateral restraint will be required for fascia beams at expansion ends of beams to prevent movement of the beams.



PLAN

STATION 876+47.59
BUILT BY
STATE OF ILLINOIS
F.A.P. RTE. 68
SEC. 29 BR-1
LOADING HS20
STR. NO. 019-0007

NAME PLATE
See Std. 515001

Attach new name plate to back side of 8" rail element. Clean and re-locate existing name plate adjacent to new name plate. Cost included in the cost of "Name Plates."

LOADING HS20-44
Allow 50#/sq. ft. for future wearing surface.
DESIGN SPECIFICATIONS
2002 AASHTO

DESIGN STRESSES

FIELD UNITS
f'c = 5,000 psi
f'ci = 4,000 psi
f's = 270,000 psi (1/2" φ low lax strands)
fsl = 201,960 psi (1/2" φ low lax strands)

PRECAST PRESTRESSED UNITS

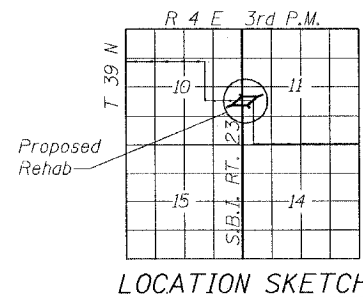
f'c = 5,000 psi
f'ci = 4,000 psi
f's = 270,000 psi (1/2" φ low lax strands)
fsl = 201,960 psi (1/2" φ low lax strands)

NOTE:
See Roadway plans for profile grade information.



Structural Engineer
Clark Dietz, Inc

DATE: 8/18/2005
License Expires 11-30-2006



LOCATION SKETCH

GENERAL PLAN

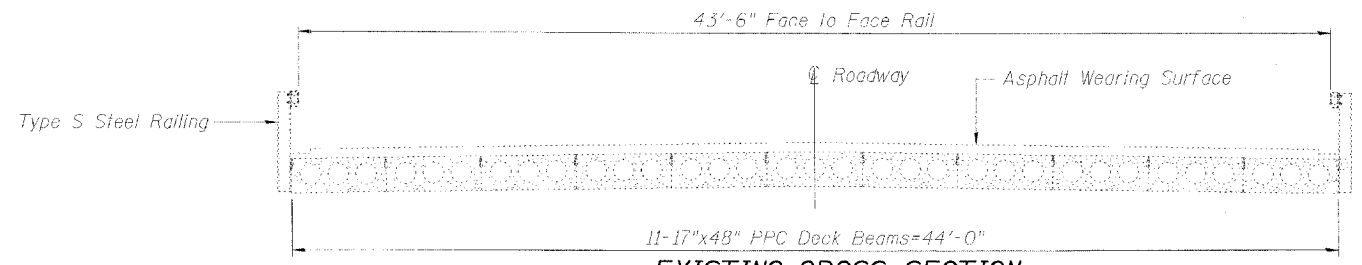
F.A.P. ROUTE 68 (IL 23)
SECTION 29 BR-1
DEKALB COUNTY
STATION 876+47.59
STRUCTURE NO. 019-0007



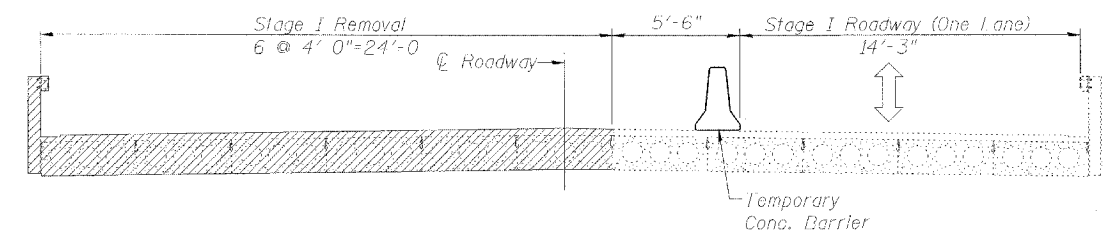
CHAMPAIGN, ILLINOIS
CHICAGO, ILLINOIS
EVANSVILLE, INDIANA
INDIANAPOLIS, INDIANA
KENSHA, WISCONSIN
SPRING GREEN, WISCONSIN

REVISIONS		DRAWING NUMBER
NAME	DATE	
		S-1

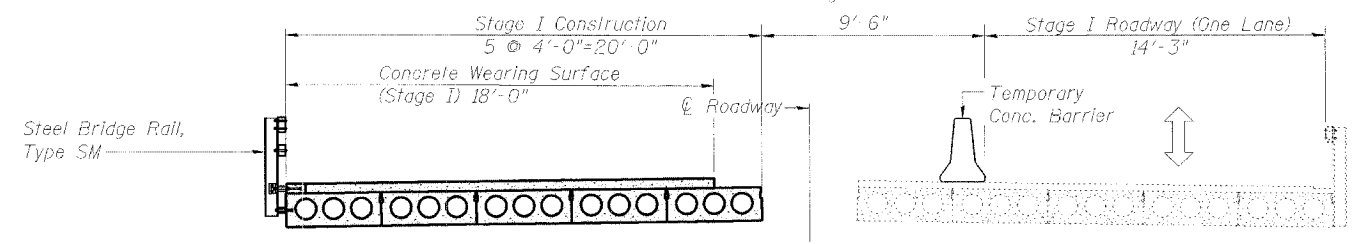
Contract #64A06



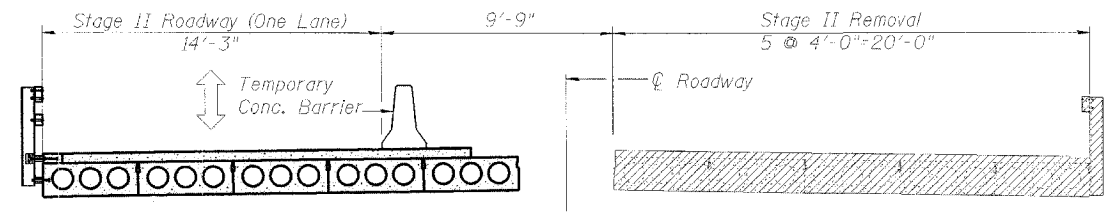
EXISTING CROSS-SECTION
(Looking North)



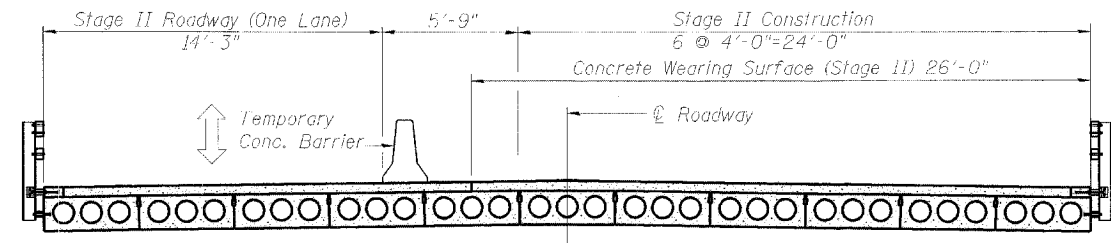
STAGE I REMOVAL
(Looking North)



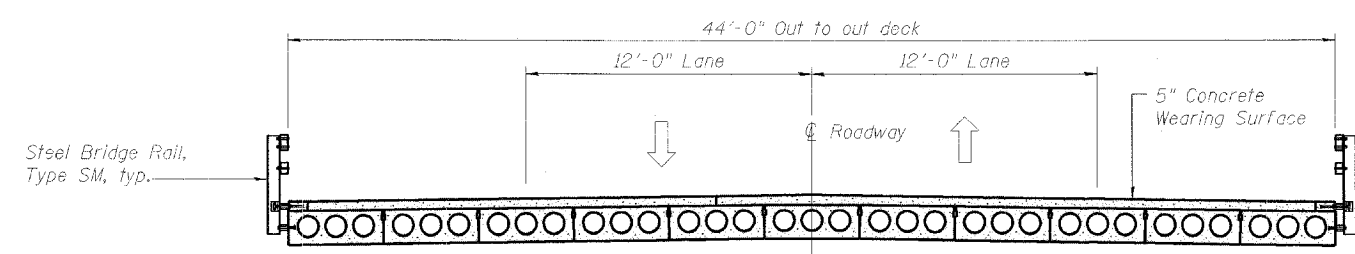
STAGE I CONSTRUCTION
(Looking North)



STAGE II REMOVAL
(Looking North)



STAGE II CONSTRUCTION
(Looking North)



PROPOSED CROSS-SECTION
(Looking North)

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Removal of Existing Superstructures	Each	1		1
Precast Prestressed Concrete Deck Beams (17" Depth)	Sq. Ft.	2,380		2,380
Reinforcement Bars, Epoxy Coated	Pound	3,130		3,130
Concrete Wearing Surface, 5"	Sq. Yd.	254		254
Bridge Deck Grooving	Sq. Yd.	249		249
Steel Bridge Rail, Type SM	Foot	109		109
Name Plates	Each	1		1
Bar Splicers	Each	56		56
Formed Concrete Repair, (Depth equal to or less than 5")	Sq. Ft.		187	187
Epoxy Crack Sealing	Foot		117.2	117.2
Protective Coat	Sq. Yd.	266		266

STAGE CONSTRUCTION

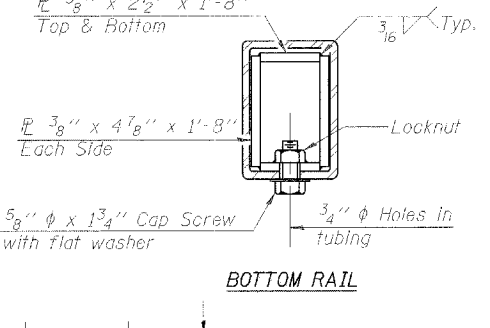
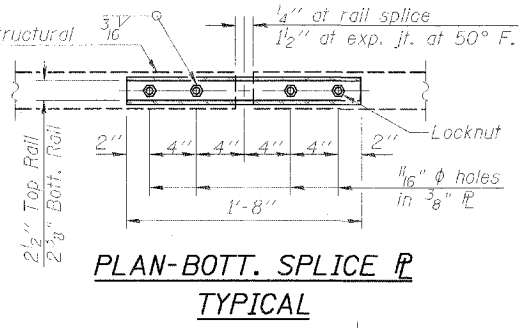
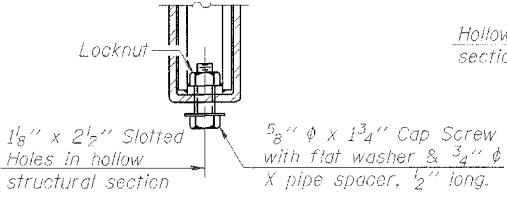
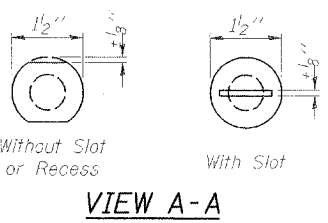
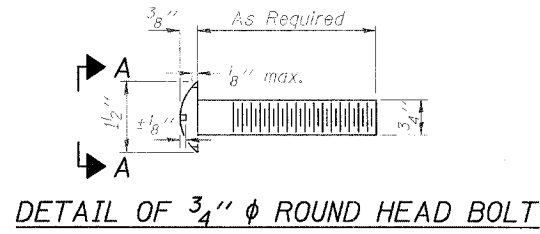
F.A.P. ROUTE 68 (IL 23)
SECTION 29 BR-1
DEKALB COUNTY
STATION 876+47.59
STRUCTURE NO. 019-0007

CHAMPAIGN, ILLINOIS
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EVANSVILLE, INDIANA
INDIANAPOLIS, INDIANA
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SPRING GREEN, WISCONSIN

Clark Dietz

REVISIONS		DATE	DRAWING NUMBER
NAME			
			S-2

2 1/2" SCALE CONSTRUCTION 05/87 07/20/2005 6:34:48 AM



SECTIONS AT RAIL SPLICE

NOTES

Hollow structural sections shall conform to the requirements of ASTM designation A 500 Grade B Structural Steel Tubing and shall meet the longitudinal CVN requirements of 15 ft-lbs at 0° F.

All other steel shapes and plates shall conform to the requirements of AASHTO M 270 Grade 36 except posts and angles shall conform to AASHTO M 270, Grade 50.

Bolts, cap screws, and nuts shall conform to the requirements of ASTM designation A 307 except for high strength bolts, nuts and washers noted which shall conform to AASHTO M 164.

All bolts, nuts, cap screws, washers and lock washers shall be galvanized according to AASHTO M 232.

All posts, railing, rail splices, anchor devices and angles shall be galvanized after shop fabrication according to AASHTO M 111 and ASTM A 385. Galvanized rail shall not be painted.

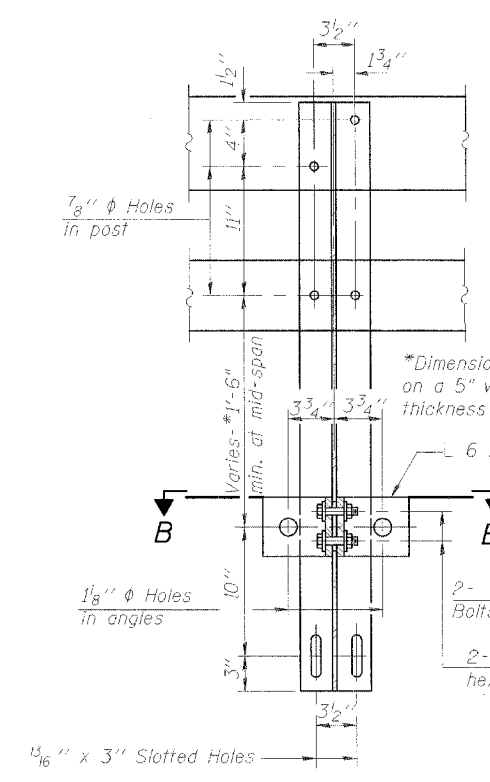
Railing shall be according to Section 509 of the Standard Specifications, except as noted, and will be paid for at the contract unit price per foot for Steel Bridge Rail, Type SM.

All field drilled holes shall be coated with an approved zinc rich paint before erection.

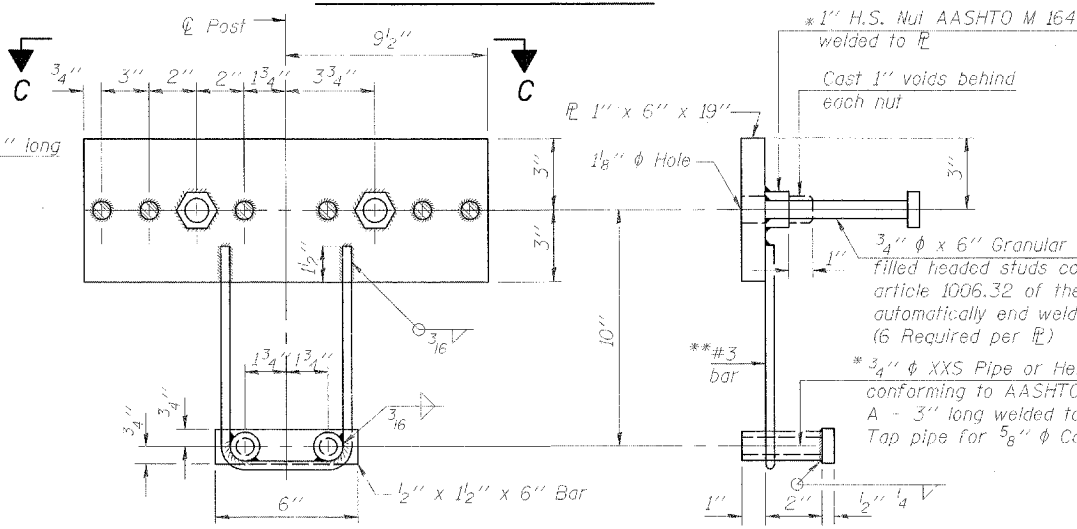
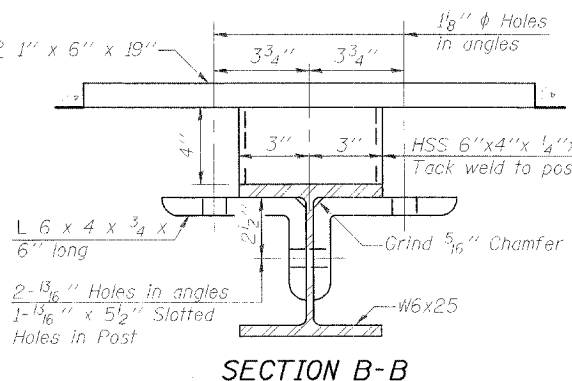
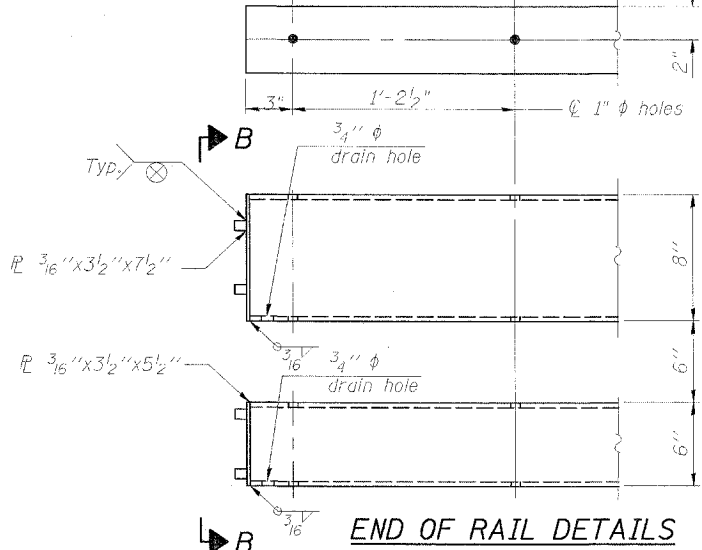
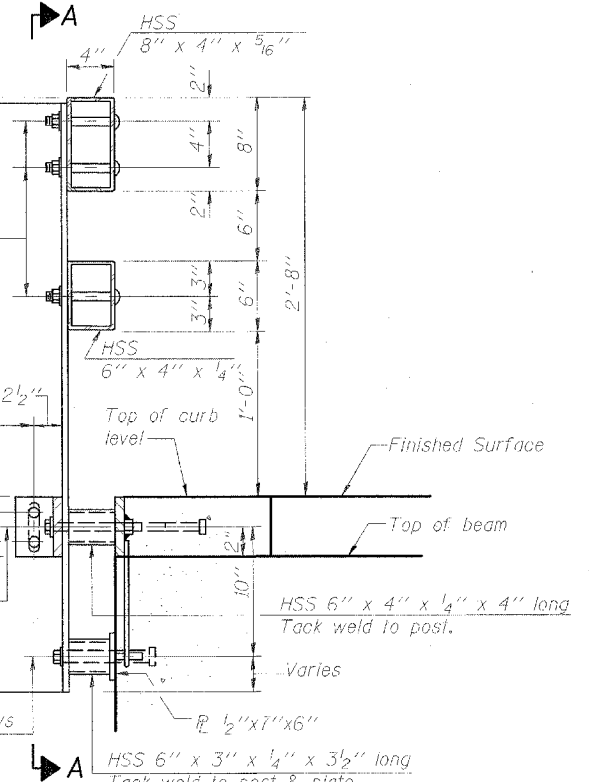
For multi-span bridges, sufficient 1/4" x 6" x 1'-2" galvanized steel shims shall be provided to align rail between adjacent spans. Cost included with Steel Bridge Rail, Type SM.

The 1/2" x 7" x 6" plates that come in contact with concrete shall receive two coats of asphalt paint conforming to Section 1060.07 Type II or place 1/8" fabric bearing pads between the plates and concrete.

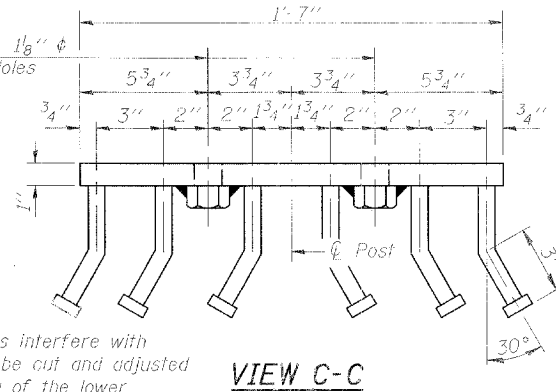
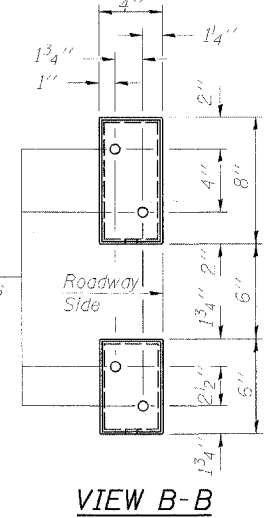
The 3/4" high strength bolts used to connect the 6 x 4 x 3/4 angles to the post shall be tightened according to Article 505.04(F)(2) of the Standard Specifications. The 1" high strength bolts connecting the angles to the concrete shall be tightened to a snug fit and given an additional 1/8 turn. The 5/8" cap screws in bottom of posts shall be tightened to a snug fit only.



4-3/4" φ x 6" Round Head Bolts (With slot or approved recess in head) with locknut & flat washer. 7/8" φ Holes in hollow structural section may be drilled in the field.



1" H.S. Nut AASHTO M 164 welded to R
 Cast 1" voids behind each nut
 3/4" φ x 6" Granular or solid flux filled headed studs conforming to article 1006.32 of the Std. Specs. automatically end welded. (6 Required per R)
 3/4" φ XXS Pipe or Hex Coupler Nuts conforming to AASHTO M291, Grade A - 3" long welded to #3 bar and Tap pipe for 5/8" φ Cap Screw.



TYPE SM
 STEEL BRIDGE RAIL SIDE MOUNTED
 F.A.P. ROUTE 68 (IL 23)
 SECTION 29 BR-1
 DEKALB COUNTY
 STATION 876+47.59
 STRUCTURE NO. 019-0007



REVISIONS		DATE
NAME		

DESIGNED BY: S.L.D. PROJECT NO. 182,901
 DRAWN BY: M.E.W. DATE: 8/2/05
 CHECKED BY: M.M. APPROVED BY: M.M.
 ACTIVITY INITIALS

BILL OF MATERIAL

Item	Unit	Quantity
Steel Bridge Rail, Type SM	Foot	109

* Threaded areas shall be plugged or blocked off during casting of beam. Galvanized after fabrication.

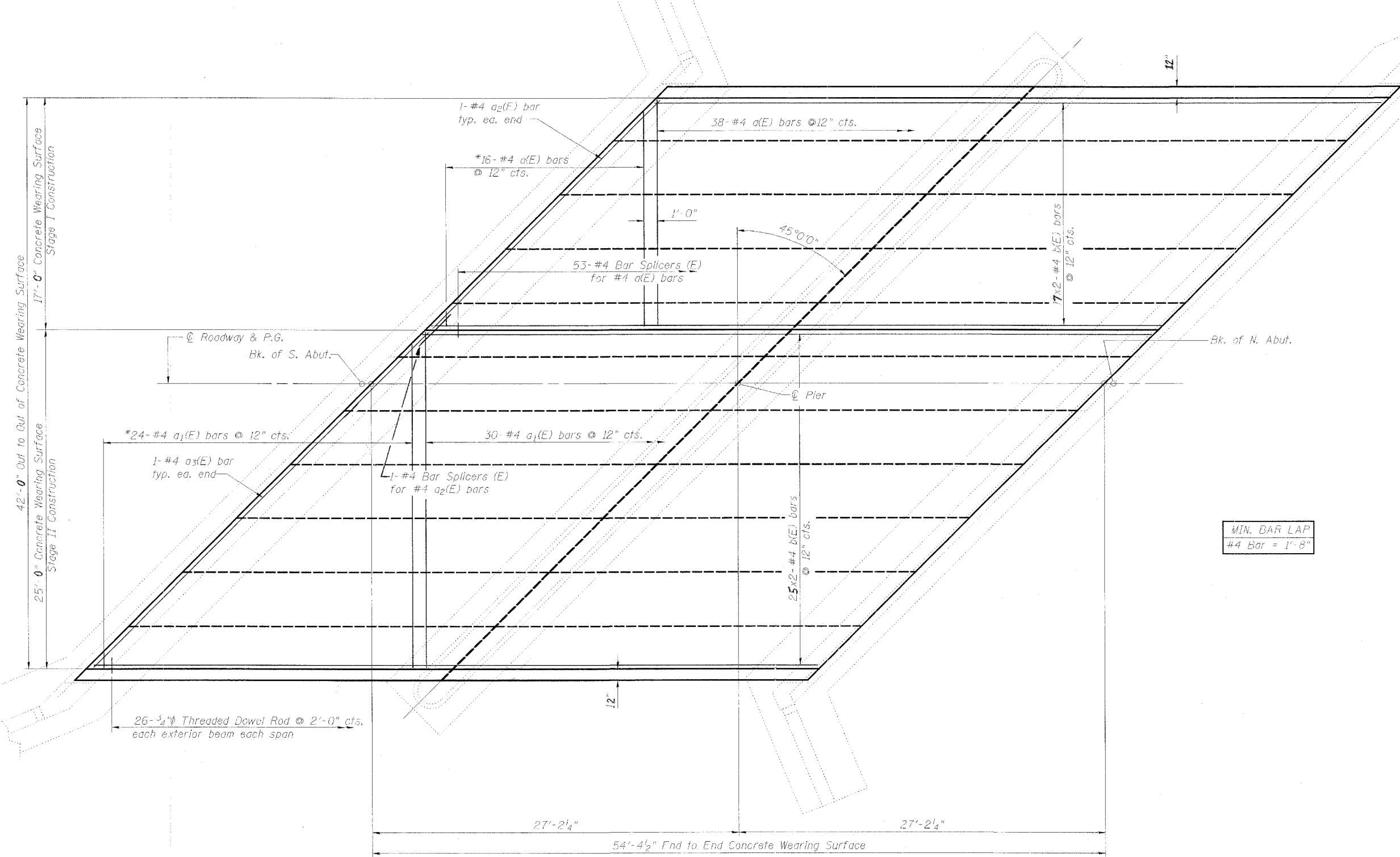
** Whenever the lower insert assemblies interfere with strand locations, the #3 bars shall be cut and adjusted in order to allow raising or lowering of the lower inserts. Maximum adjustment not to exceed 1/2".

(6'-3" Maximum Post Spacing)

* Order a(E) and a₁(E) bars full length.
Cut to fit skew and use remainder of
bars in opposite end.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 4
FAP RT 68 (IL 23)	29 BR-1	DEKALB	25	10	10 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #64A06



**SUPERSTRUCTURE
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	54	#4	16'-8"	—
a ₁ (E)	54	#4	24'-8"	—
a ₂ (E)	2	#4	23'-7"	—
a ₃ (E)	2	#4	34'-10"	—
b(E)	84	#4	27'-10"	—
Reinforcement Bars, Epoxy Coated			Pound	3,130
Concrete Wearing Surface, 5"			Sq. Yd.	254
Bar Splicers			Each	56

Reinforcement bars designated (E) shall be epoxy coated.
For details of Bar Splicers, see sheet 10 of 10.
Bars indicated thus 1 x 2 #5 etc. indicates 1 line of bars with 2 lengths per line.

MIN. BAR LAP
#4 Bar = 1'-8"

PLAN

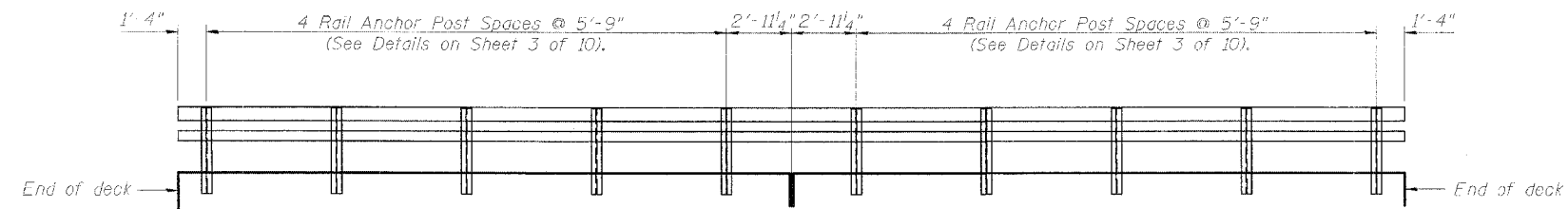
CONCRETE WEARING SURFACE

F.A.P. ROUTE 68 (IL 23)
SECTION 29 BR-1
DEKALB COUNTY
STATION 876+47.59
STRUCTURE NO. 019-0007

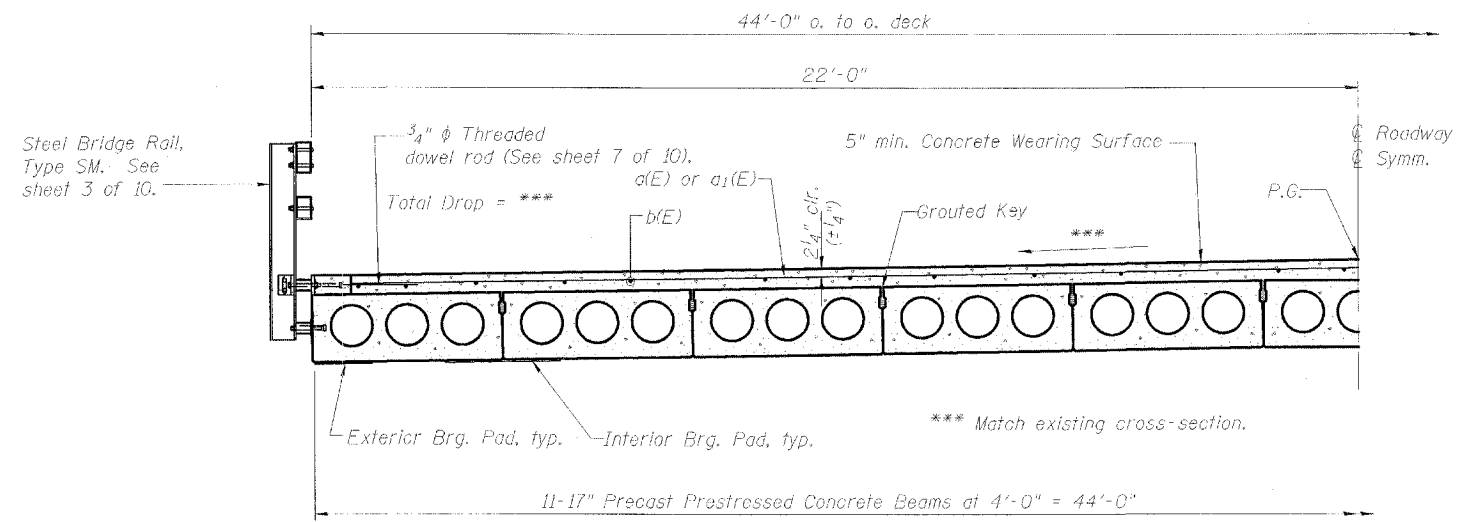
CHAMPAIGN, ILLINOIS
CHICAGO, ILLINOIS
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INDIANAPOLIS, INDIANA
KENOSHA, WISCONSIN
SPRING GREEN, WISCONSIN

REVISIONS		DATE	DRAWING NUMBER
NAME			
			S-4

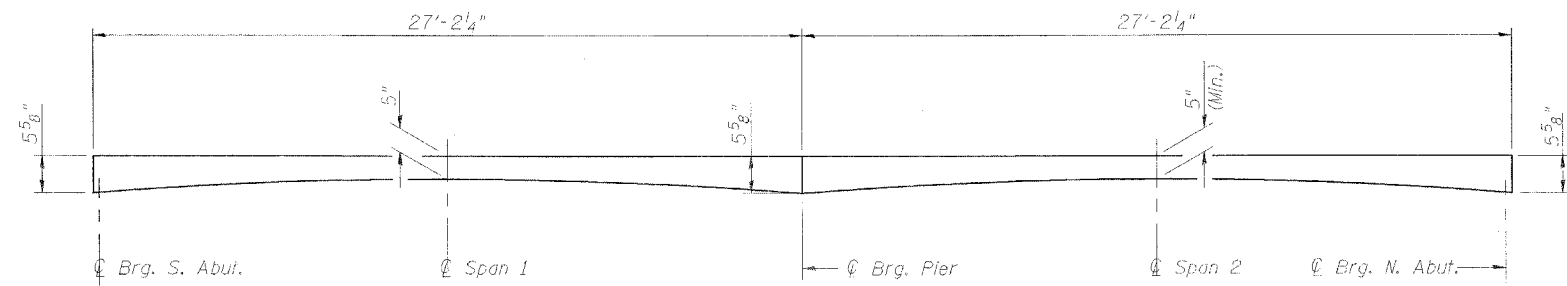
Contract #64A06



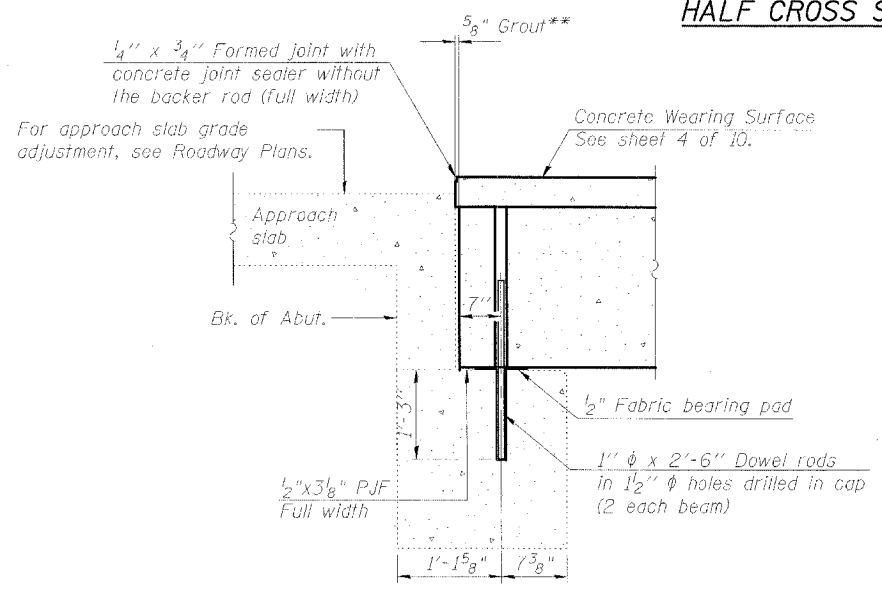
RAIL POST SPACING



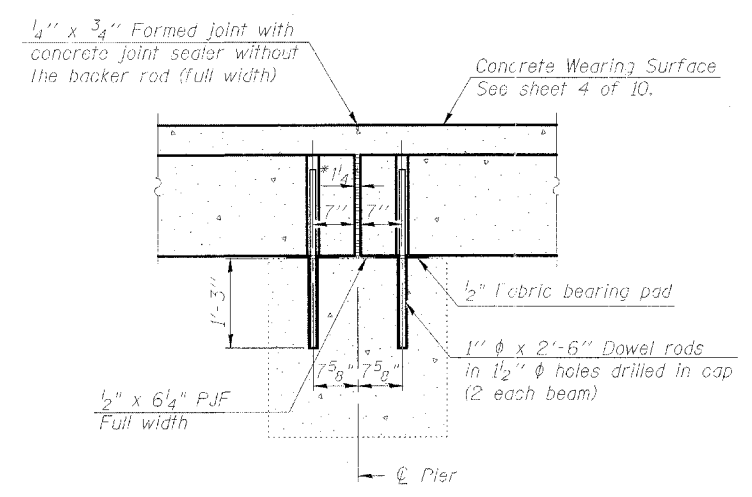
HALF CROSS SECTION



REINFORCED CONCRETE WEARING SURFACE PROFILE



SECTION THRU ABUTMENT



SECTION THRU FIXED PIER

* 1/4" Jt. shall be filled with non-shrink grout. 1/4" dimension may vary to accommodate tolerance in beam lengths.
Dimensions at Rt. L's

**5/8" Joint shall be filled with non-shrink grout. 5/8" Dimension may vary plus or minus to accommodate tolerance in beam lengths.

Notes:
After beams have been erected, holes shall be drilled into substructure and anchor dowels placed. Dowel holes shall be filled with non-shrink grout to top of beam and allowed to cure min. 24 hrs. prior to grouting the shear keys.
All horizontal dimensions are at right angles to beam ends. See sheet 6 of 10 for bearing pad details.
Existing Dowel Rods shall be cut off and ground flush with the top of the existing concrete. Cost to be included in the cost of "Removal of Existing Superstructures".

SUPERSTRUCTURE DETAILS

F.A.P. ROUTE 68 (IL 23)
SECTION 29 BR-1
DEKALB COUNTY
STATION 876+47.59
STRUCTURE NO. 019-0007

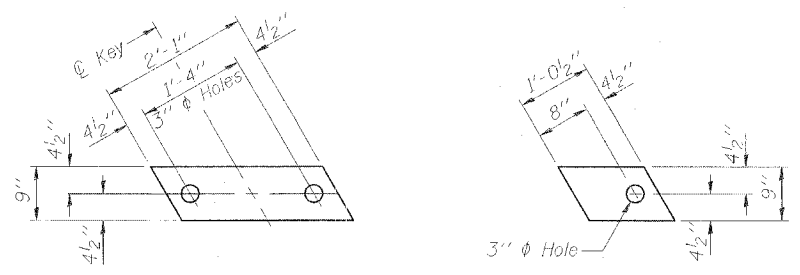
CHAMPAIGN, ILLINOIS
CHICAGO, ILLINOIS
EVANSVILLE, INDIANA
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SPRING GREEN, WISCONSIN

REVISIONS		DATE
NAME		

NOTE: DIMENSIONAL DATA IS NOT TO BE OBTAINED BY SCALING ANY PORTION OF THIS DRAWING.

DESIGNED BY	S.L.C.	DRAWING NO.	162301
QUARTY BY	NEW	DATE	8/6/5
CHECKED BY	M.M.		
APPROVED BY	M.M.		
ACTIVITY	DETAILS		

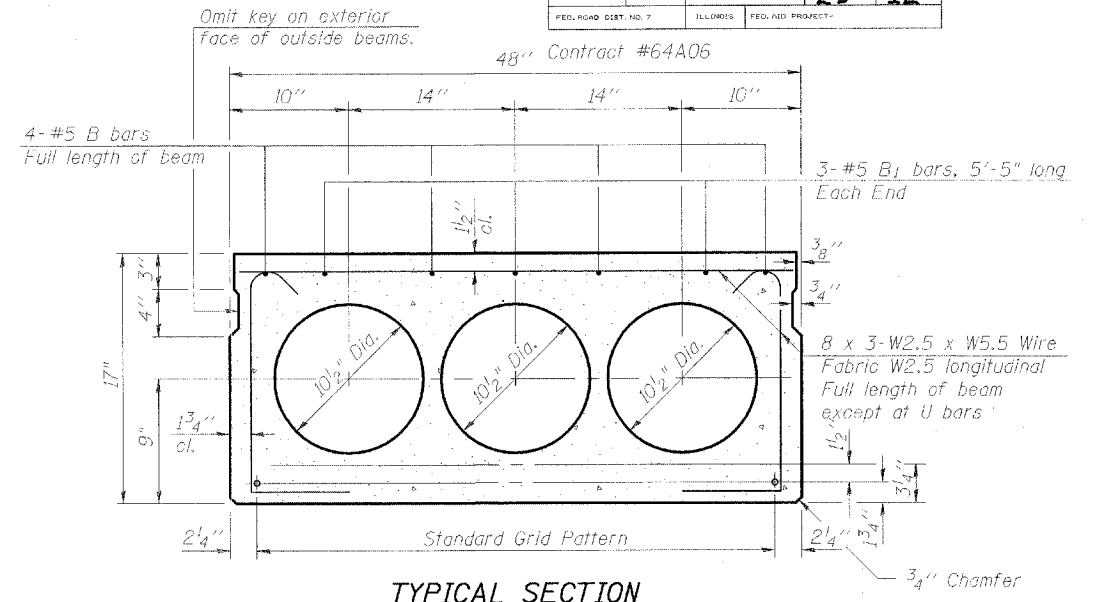
DRAWING NUMBER
S-5



FABRIC BEARING PAD
(Interior)

FABRIC BEARING PAD
(Exterior)

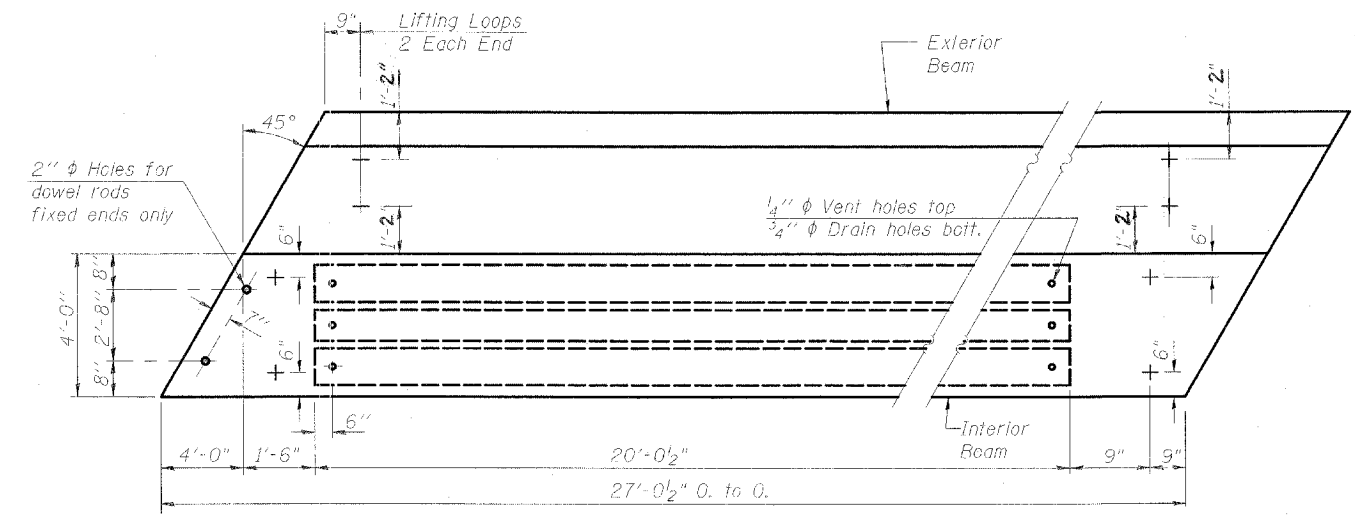
FIXED



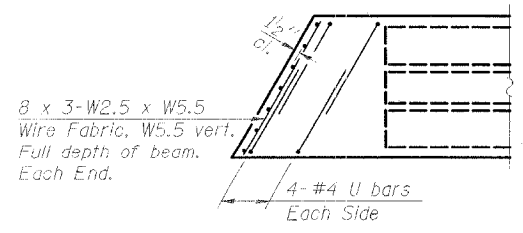
TYPICAL SECTION

1/2" ϕ Strands, Each Strand Stressed to 30,900 Lbs.
8 Strands 1 3/4" up, 4 Strands 3/4" up

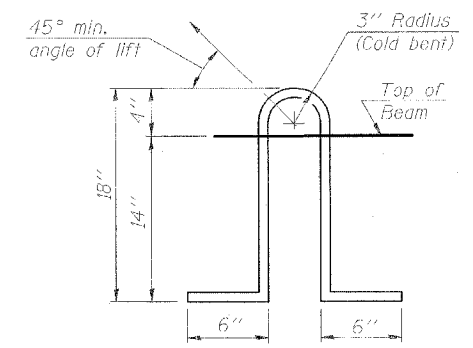
Note:
Place strands symmetrically about ϕ of beam.



PLAN



END PLAN



LIFTING LOOP DETAIL

NOTES

Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.153 sq. in. Lifting loops shall be 2-1/2" ϕ -270 ksi strands, as shown. Non prestressing steel shall conform to AASHTO M-31 or M-322 Grade 60. The bearing seat surfaces shall be adjusted by shimming to assure firm and even bearing. Two 1/8" fabric adjusting shims of the dimensions of the Exterior Bearing Pad shall be provided for each bearing. Keyway surfaces shall be cleaned to remove form oil or other bond breaking material prior to shipment of the beams. Cleaning shall be done by sandblasting the keyway areas between top of the beam and the bottom edge of the key. Corrosion Inhibitor, as covered in the Special Provisions, shall be used in the concrete for precast prestressed concrete deck beams. Required Release Strength, f'ci, shall be 4,000 p.s.i.

SUPERSTRUCTURE DETAILS

F.A.P. ROUTE 68 (IL 23)
SECTION 29 BR-1
DEKALB COUNTY
STATION 876+47.59
STRUCTURE NO. 019-0007



CHAMPAIGN, ILLINOIS
CHICAGO, ILLINOIS
EVANSVILLE, INDIANA
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KENDSHA, WISCONSIN
SPRING GREEN, WISCONSIN

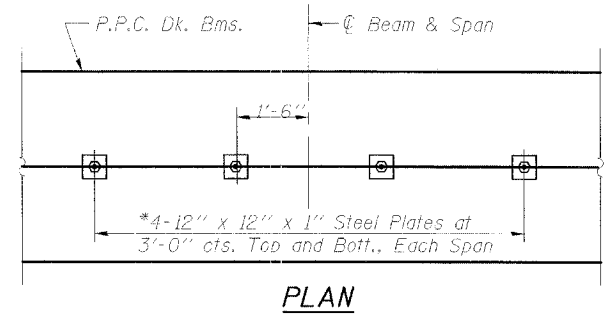
REVISIONS	
NAME	DATE

DRAWING NUMBER
S-6

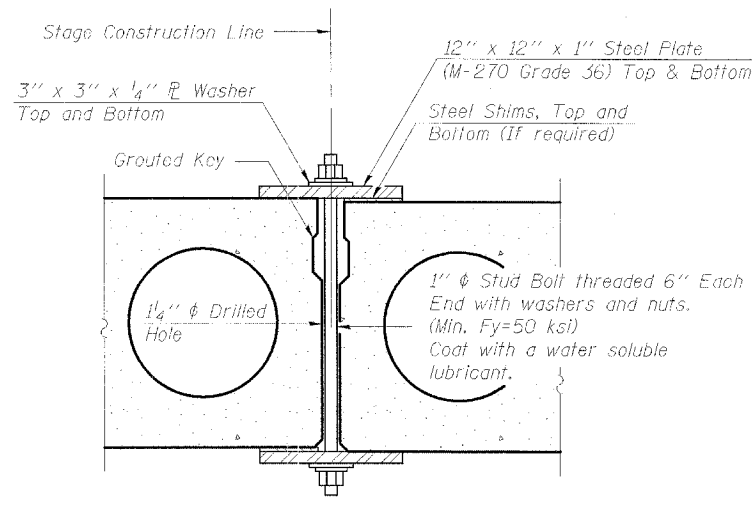
Contract #64A06

BILL OF MATERIAL

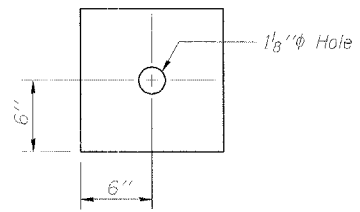
ITEM	Unit	Total
Precast Prestressed Conc. Deck Bms. (17)	Sq. Ft.	2,380



PLAN



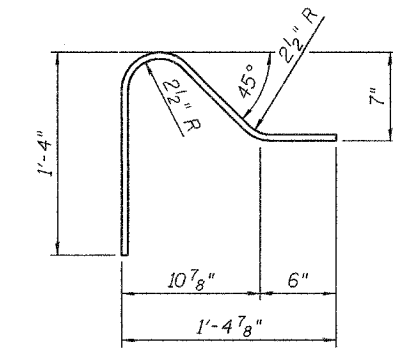
SECTION



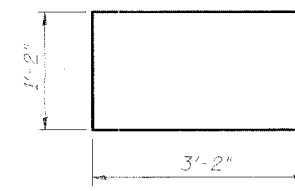
CLAMPING PLATE

SHEAR KEY CLAMPING DETAILS AT STAGE CONST. JT.

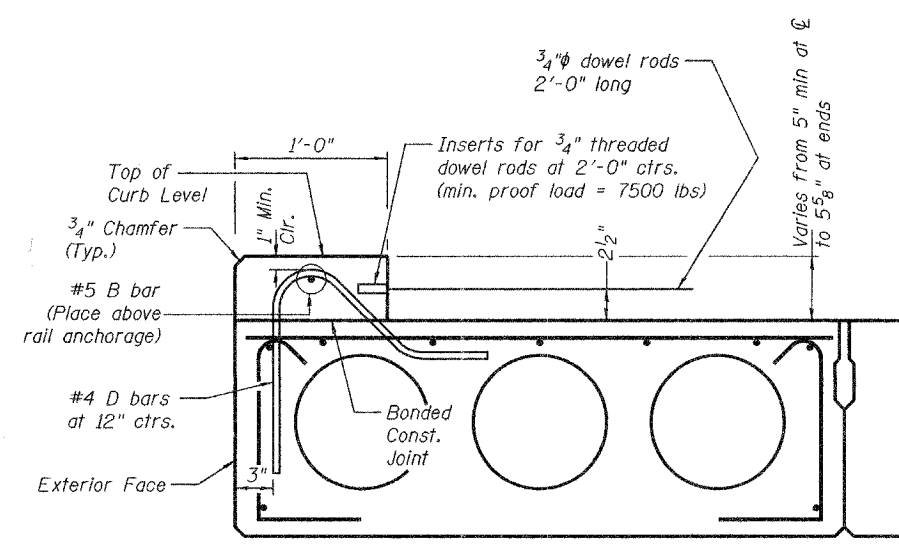
See Special Provisions for Stage Construction of Precast Prestressed Concrete Deck Beams.
See Stage Construction Details for traffic lanes.



#4 D BAR



BAR U



SECTION THRU EXTERIOR BEAMS

See Section Thru Interior Beams for strand pattern, dimensions and bar call outs.

Bridge rail inserts shall be cast in precast beams and curbs. Curbs shall be cast by the precast prestressed concrete supplier after strands have been released and prior to shipping the beam. The concrete in the curb shall be the same as specified for the deck beams.
The curb inserts and threaded dowel rods may be either epoxy coated or galvanized and the cost shall be included with precast prestressed concrete deck beams.

SUPERSTRUCTURE DETAILS

F.A.P. ROUTE 68 (IL 23)
SECTION 29 BR-1
DEKALB COUNTY
STATION 876+47.59
STRUCTURE NO. 019-0007



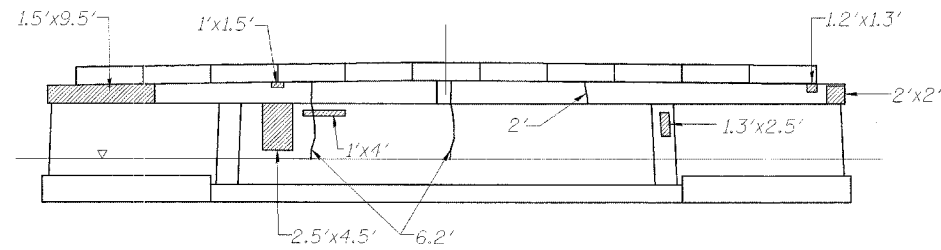
CHAMPAIGN, ILLINOIS
CHICAGO, ILLINOIS
EVANSVILLE, INDIANA
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KENOSHA, WISCONSIN
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REVISIONS		DATE	DRAWING NUMBER
NAME			
			S-7

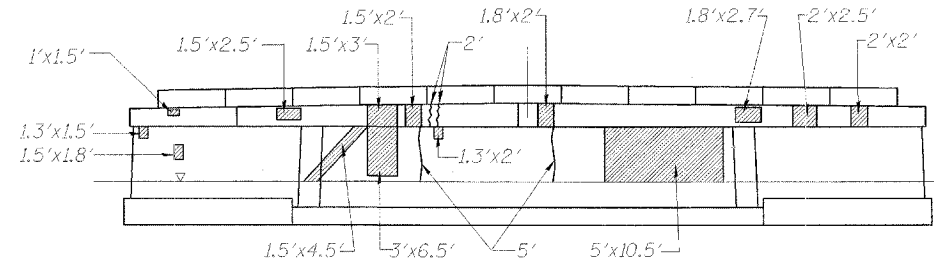
NOTE: DIMENSIONAL DATA IS NOT TO BE OBTAINED BY SCALING ANY PORTION OF THIS DRAWING.

DESIGNED BY: S.L.D. PROJECT NO. 11/2/31
DRAWN BY: M.E. DATE: 8/05
CHECKED BY: M.M.
APPROVED BY: M.M.
ACTIVITY: DETAILS

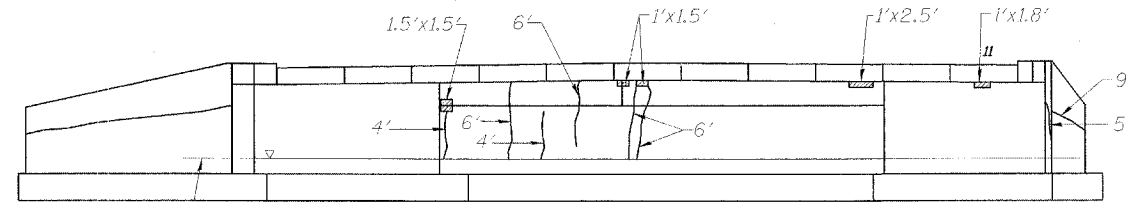
Contract #64A06



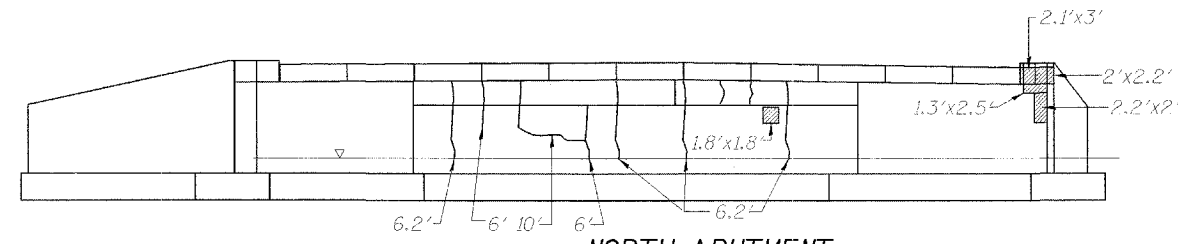
PIER ELEVATION
(South face, looking north)



PIER ELEVATION
(North face, looking south)



SOUTH ABUTMENT
(Looking South)



NORTH ABUTMENT
(Looking North)

LEGEND

Formed Concrete Repair
Depth equal to or less than 5"

3' } Epoxy Crack Sealing

H.L. } Hairline Crack - Not to be sealed

Note: Crack widths are $\frac{1}{8}'' \pm \frac{1}{16}''$
unless otherwise noted.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Epoxy Crack Sealing	Foot	117.2
Formed Concrete Repair (Depth equal to or less than 5")	Sq Ft	187

PIER AND ABUTMENT REPAIRS

F.A.P. ROUTE 68 (IL 23)
SECTION 29 BR-1
DEKALB COUNTY
STATION 876+47.59
STRUCTURE NO. 019-0007



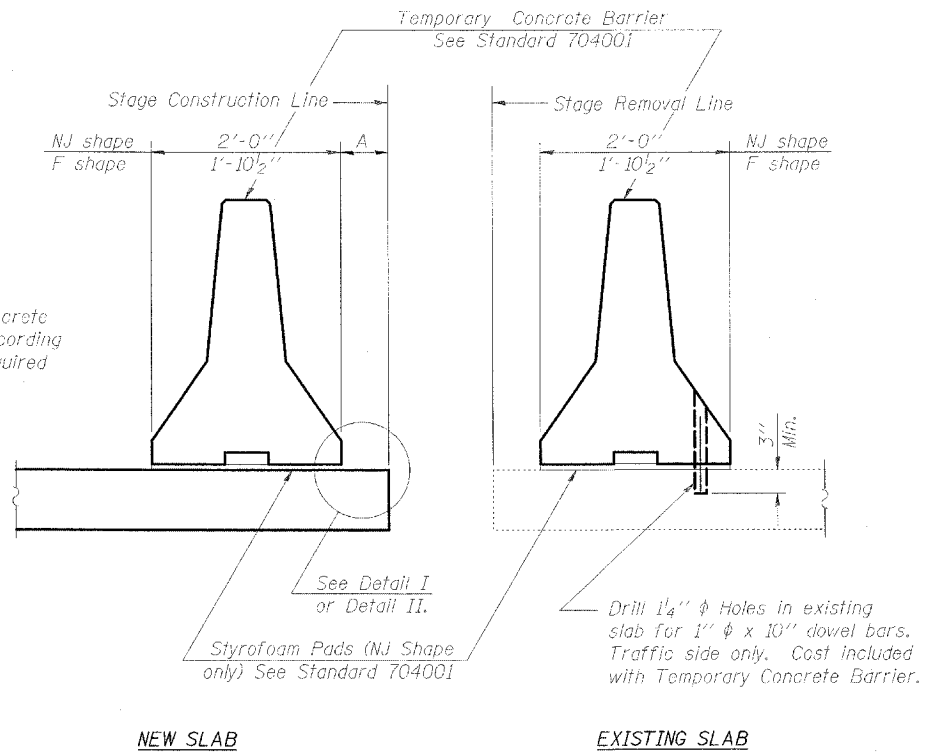
CHAMPAIGN, ILLINOIS
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EVANSVILLE, INDIANA
INDIANAPOLIS, INDIANA
KENOSHA, WISCONSIN
SPRING GREEN, WISCONSIN

REVISIONS		DATE
NAME		

DESIGNED BY: S.L.D.	PROJECT NO.: 102301
DRAWN BY: MEW	DATE: 8/05
CHECKED BY: M.M.	
APPROVED BY: M.M.	
ACTIVITY: INITIALS	

DRAWING NUMBER
S-8

Contract #64A06

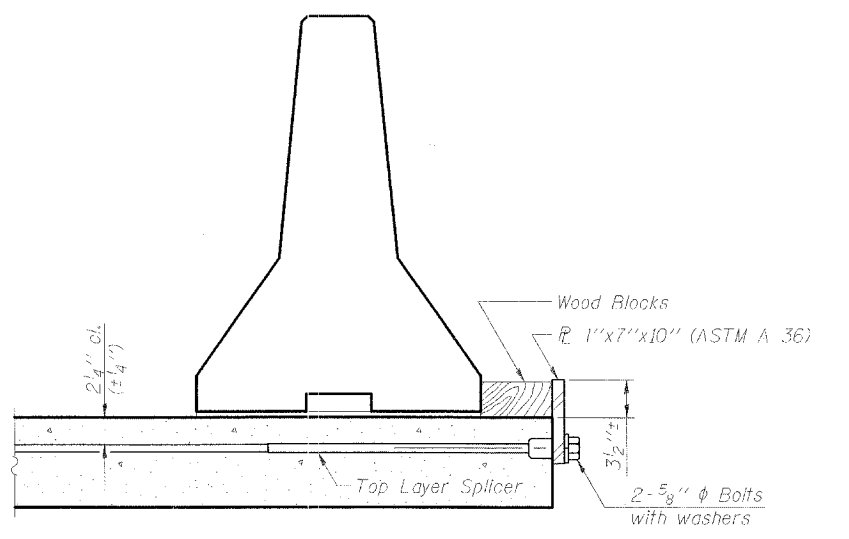


SECTIONS THRU SLAB

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".

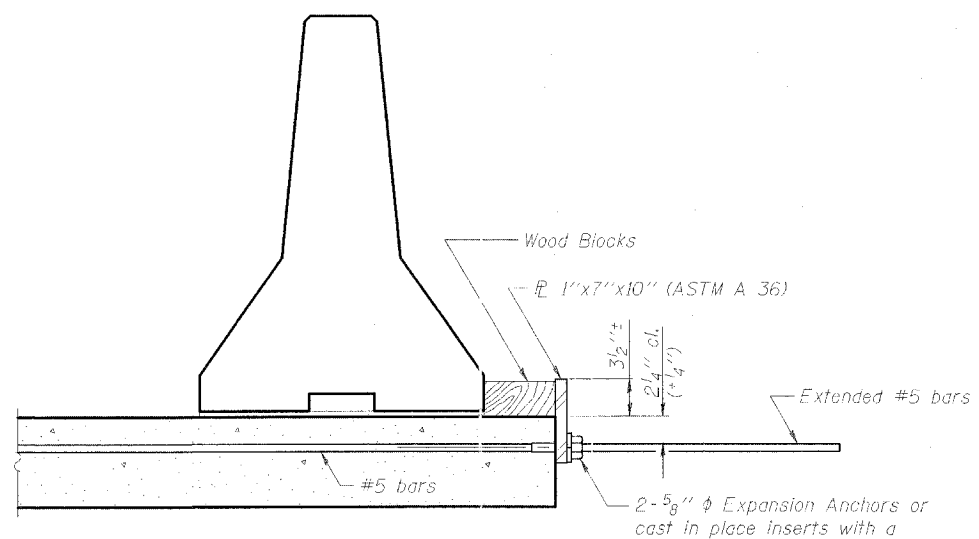
NOTES

- Detail I - With Bar Splicer or Couplers:**
Connect one (1) 1"x7"x10" steel plate to the top layer of couplers with 2-5/8" diameter bolts screwed to coupler at approximate center of each barrier panel.
- Detail II - With Extended Reinforcement Bars:**
Connect one (1) 1"x7"x10" steel plate to the concrete slab with 2-5/8" diameter Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate center of each barrier panel.
- Cost of anchorage is included with Temporary Concrete Barrier.



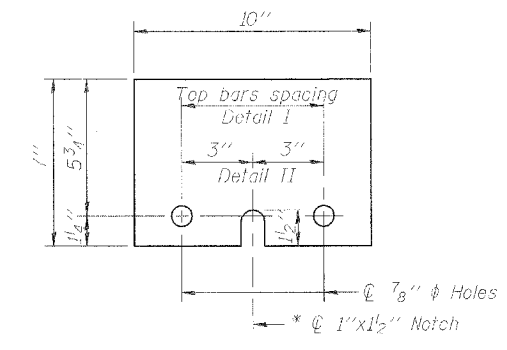
DETAIL I

The 1"x7"x10" Plate shall not be removed until Stage II Construction forms and reinforcement bars are in place.



DETAIL II

The 1"x7"x10" Plate shall not be removed until Stage II Construction forms and all reinforcement bars are in place and the concrete is ready to be placed.



1"x7"x10"

* Required only with Detail II

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION

F.A.P. ROUTE 68 (IL 23)
SECTION 29 BR-1
DEKALB COUNTY
STATION 876+47.59
STRUCTURE NO. 019-0007

Clark Dietz

CHAMPAIGN, ILLINOIS
CHICAGO, ILLINOIS
EVANSVILLE, INDIANA
INDIANAPOLIS, INDIANA
KENDSHA, WISCONSIN
SPRING GREEN, WISCONSIN

REVISIONS	
NAME	DATE

DESIGNED BY: S.H.D.	PROJECT NO. 102301
DRAWN BY: M.E.W.	DATE: 8/05
CHECKED BY: M.M.	
APPROVED BY: H.M.	
ACTIVITY: DETAILS	

DRAWING NUMBER
S-9

Contract #64A06

NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

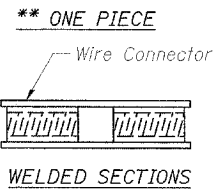
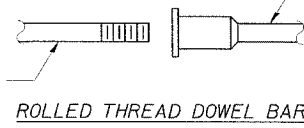
- ① Minimum Capacity (Tension in kips) = $1.25 \times f_y \times A_1$
- ② Minimum *Pull-out Strength (Tension in kips) = $1.25 \times f_{s_{allow}} \times A_1$

Where f_y = Yield strength of lapped reinforcement bars in ksi.
 $f_{s_{allow}}$ = Allowable tensile stress in lapped reinforcement bars in ksi (Service Load)
 A_1 = Tensile stress area of lapped reinforcement bars.
 * = 28 day concrete

BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	5.9
#5	2'-0"	23.0	9.2
#6	2'-7"	33.1	13.3
#7	3'-5"	45.1	18.0
#8	4'-6"	58.9	23.6
#9	5'-9"	75.0	30.0
#10	7'-3"	95.0	38.0
#11	9'-0"	117.4	46.8

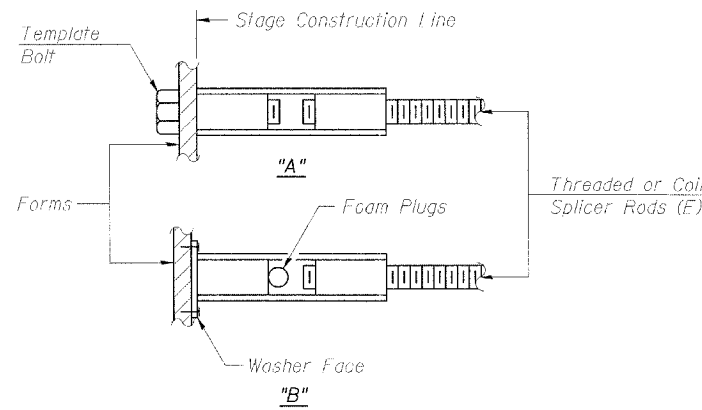
Bar splicer assemblies shall be according to Section 508 of the Standard Specifications, except as noted. The furnishing and installation of bar splicer assemblies will be measured and paid for at the contract unit price each for "BAR SPLICERS."

The diameter of this part is equal or larger than the diameter of bar spliced.



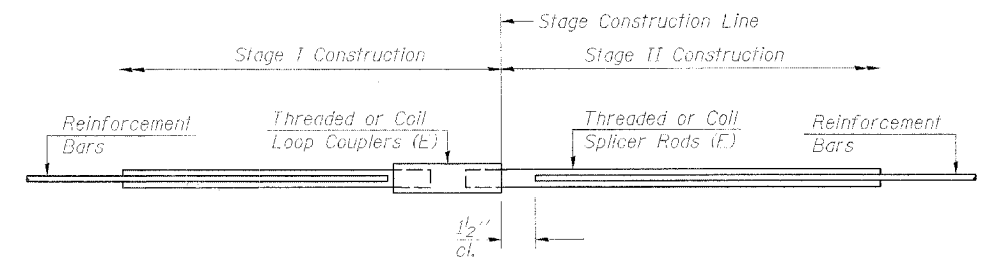
BAR SPLICER ASSEMBLY ALTERNATIVES

** Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



INSTALLATION AND SETTING METHODS

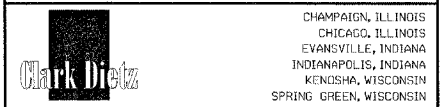
"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD

Bar Size	No. Assemblies Required	Location
4	56	Overlay

BAR SPLICER ASSEMBLY DETAILS
 F.A.P. ROUTE 68 (IL 23)
 SECTION 29 BR-1
 DEKALB COUNTY
 STATION 876+47.59
 STRUCTURE NO. 019-0007

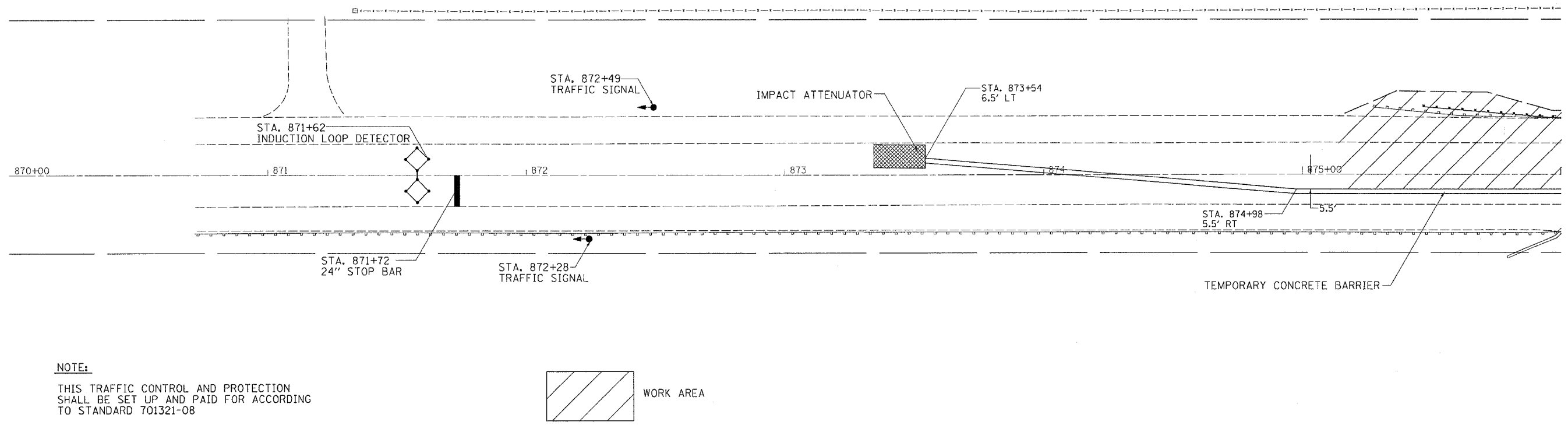


REVISIONS		DATE
NAME		

DESIGNED BY: S.L.D.	PROJECT NO: 182387
DRAWN BY: M.E.W.	DATE: 8/2/95
CHECKED BY: M.M.	
APPROVED BY: M.M.	
ACTIVITY	DETAILS

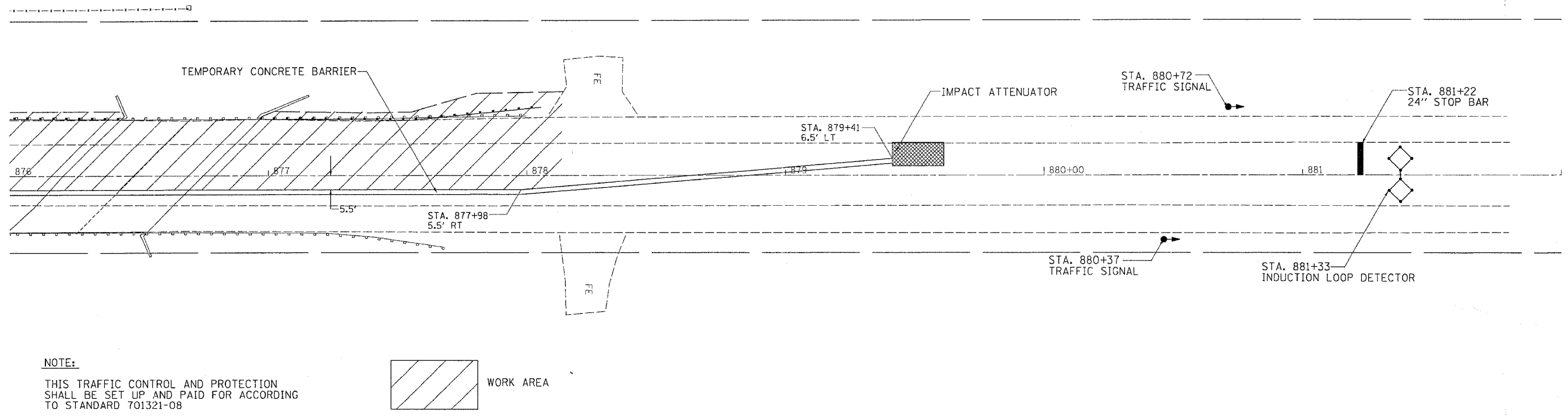
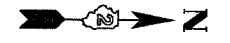
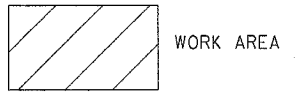
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68	29BR-1	DEKALB	25	18
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

STAGING DETAILS



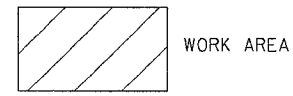
NOTE:

THIS TRAFFIC CONTROL AND PROTECTION SHALL BE SET UP AND PAID FOR ACCORDING TO STANDARD 701321-08



NOTE:

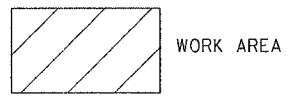
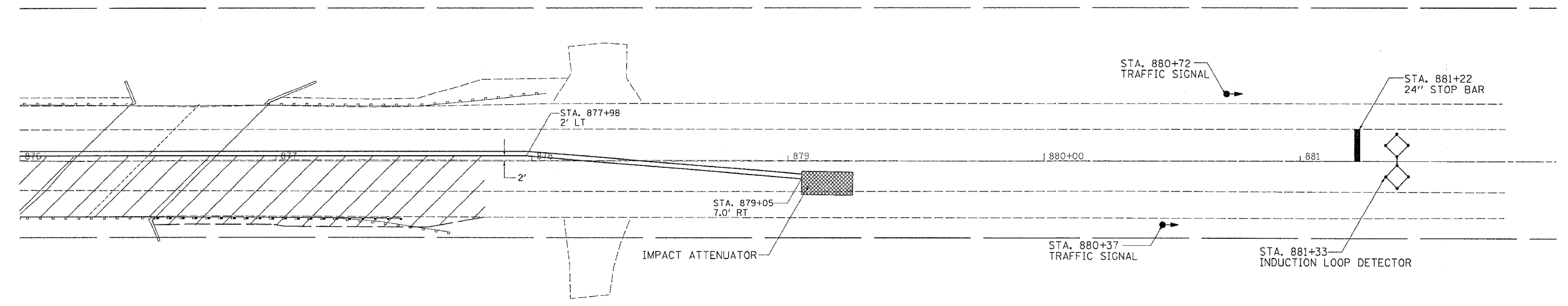
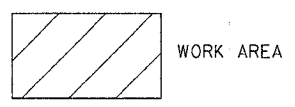
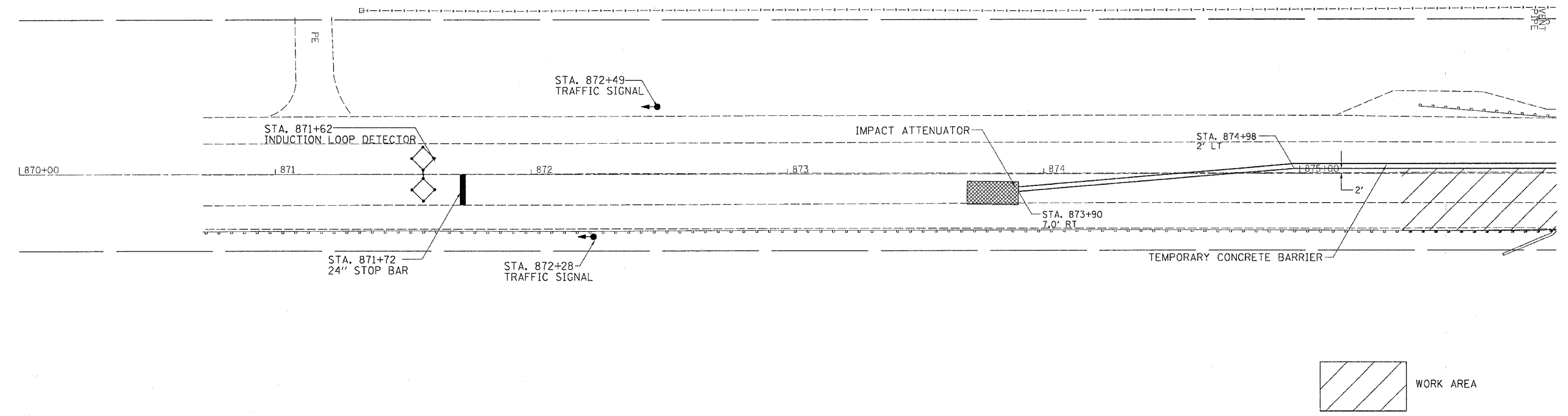
THIS TRAFFIC CONTROL AND PROTECTION SHALL BE SET UP AND PAID FOR ACCORDING TO STANDARD 701321-08



PLOT DATE = Wed Aug 24 09:36:26 2005
FILE NAME = c:\pwork\st1\29BR-1\29BR-1A\29BR-1A.dgn
PLOT SCALE = 28.0000' / IN.
USER NAME = megluj

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68	29BR-1	DEKALB	25	19
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

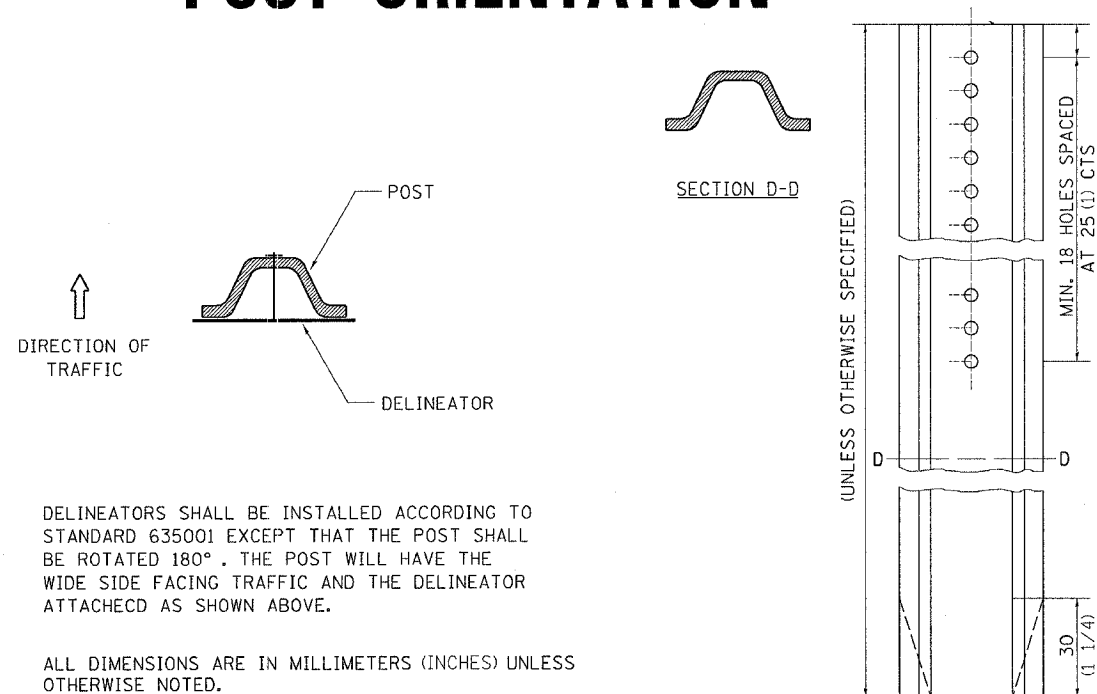
STAGING DETAILS



PLOT DATE = Wed Aug 24 09:35:26 2005
 FILE NAME = S:\projects\64a06\19\staging.dgn
 FILE SCALE = 1/4" = 1' IN.
 USER NAME = mgjllj

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68	29BR-1	DEKALB	25	20
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

DELINEATOR AND POST ORIENTATION



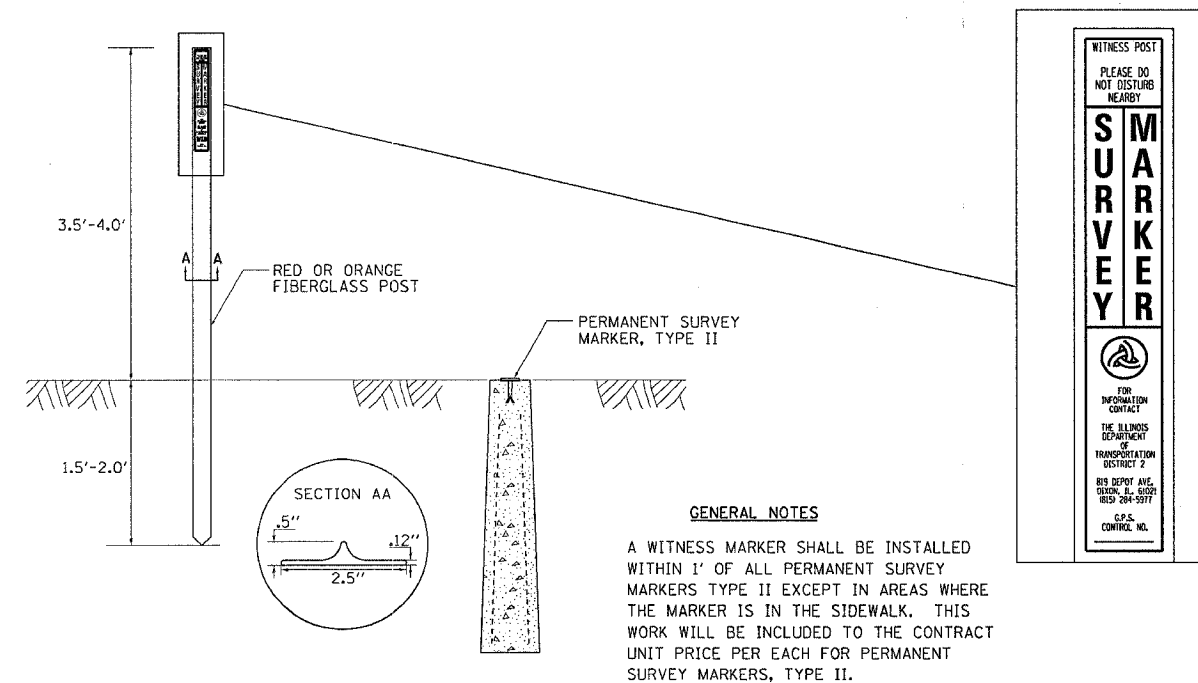
DELINEATORS SHALL BE INSTALLED ACCORDING TO STANDARD 635001 EXCEPT THAT THE POST SHALL BE ROTATED 180°. THE POST WILL HAVE THE WIDE SIDE FACING TRAFFIC AND THE DELINEATOR ATTACHECD AS SHOWN ABOVE.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

DELINEATOR AND POST ORIENTATION 37.4

REVISED 1-31-00

WITNESS MARKER FOR PERMANENT SURVEY MARKERS TYPE II



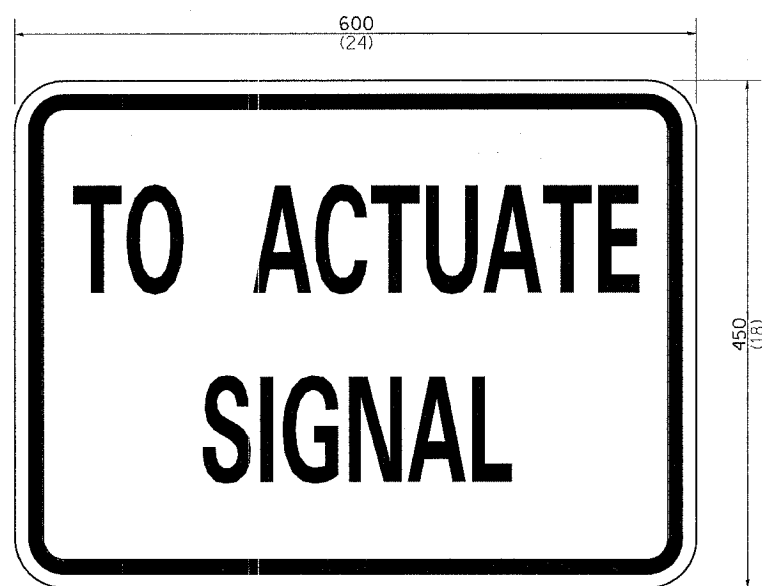
GENERAL NOTES

A WITNESS MARKER SHALL BE INSTALLED WITHIN 1' OF ALL PERMANENT SURVEY MARKERS TYPE II EXCEPT IN AREAS WHERE THE MARKER IS IN THE SIDEWALK. THIS WORK WILL BE INCLUDED TO THE CONTRACT UNIT PRICE PER EACH FOR PERMANENT SURVEY MARKERS, TYPE II.

WITNESS MARKER FOR PERMANENT SURVEY MARKERS TYPE II 38.4

REVISED 1-31-00

STOP LINE SIGN FOR TEMPORARY SIGNALS



SIZE: 600(24) x 450(18)
 100(4) CAPITAL LETTERS - BLACK
 13 (1/2) BORDER - BLACK
 WHITE REFLECTIVE - TYPE B
 ENGINEERING GRADE SHEETING

GENERAL NOTE:

THIS SIGN SHALL BE INSTALLED AT THE STOP LINE AS DIRECTED BY ENGINEER.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

STOP LINE SIGN FOR TEMPORARY SIGNALS 99.4

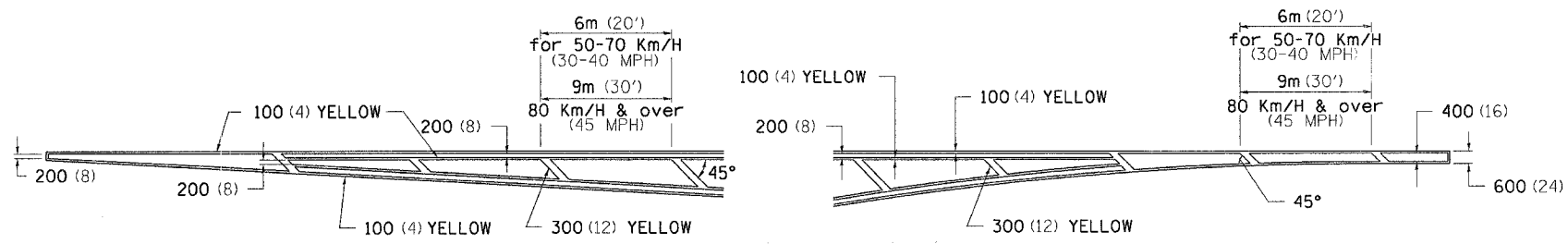
REVISED 8-7-90

PLOT DATE = Wed Aug 24 09:45:34 2005
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 PLOT SCALE = 50.0000 / 1 IN.
 REFERENCE = #REF#

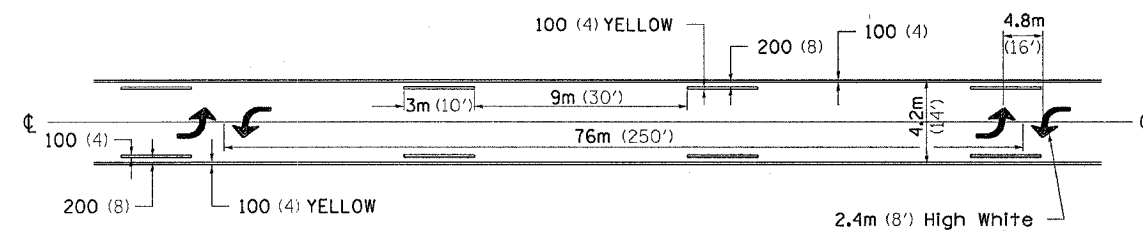
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68	29BR-1	DEKALB	25	21
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

TYPICAL PAVEMENT MARKINGS

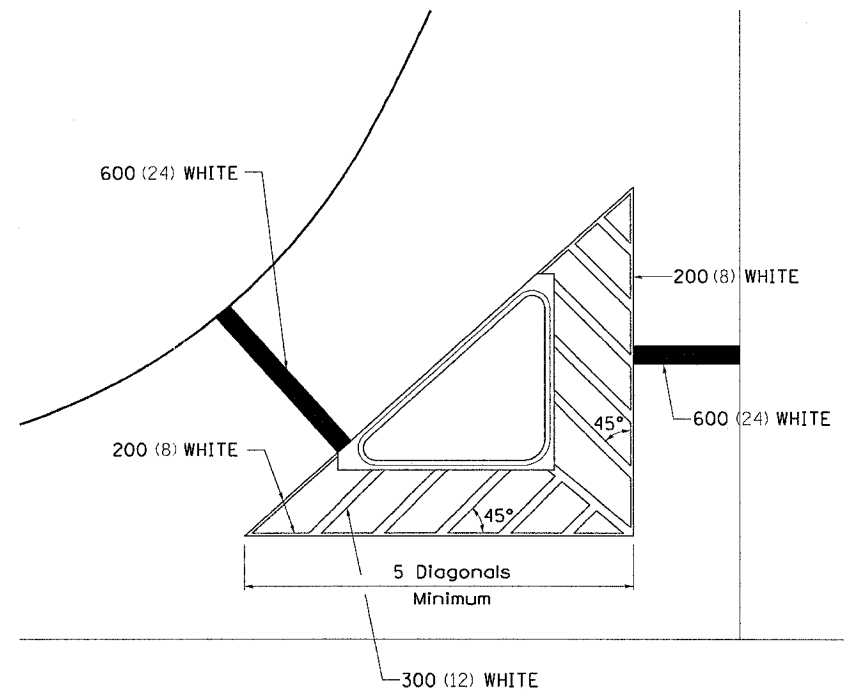
TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN



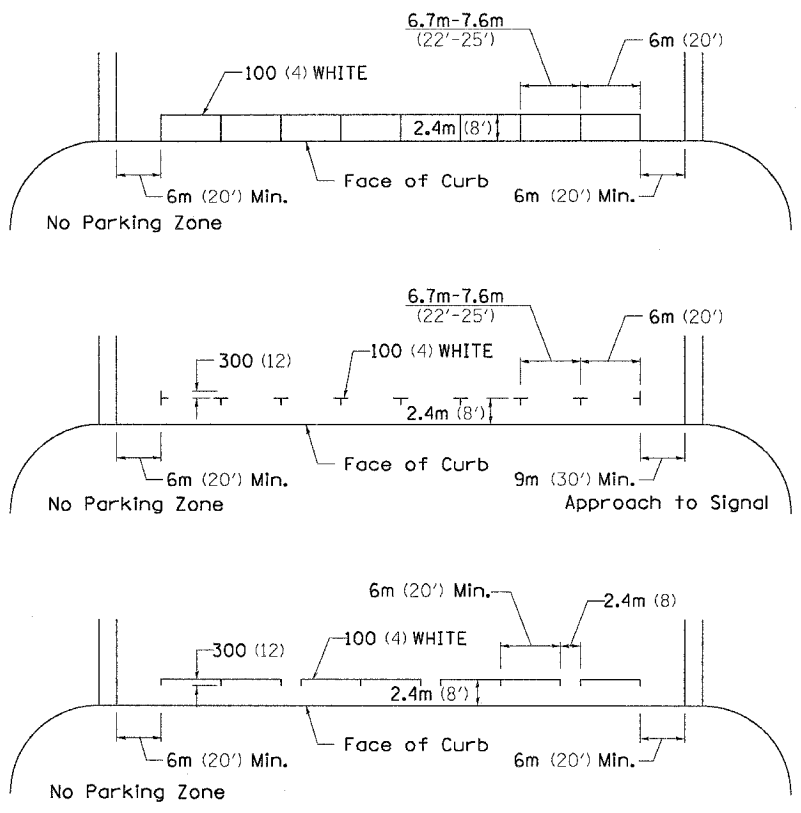
MEDIAN PAVEMENT MARKING



TYPICAL ISLAND OFFSET SHOULDER WIDTH



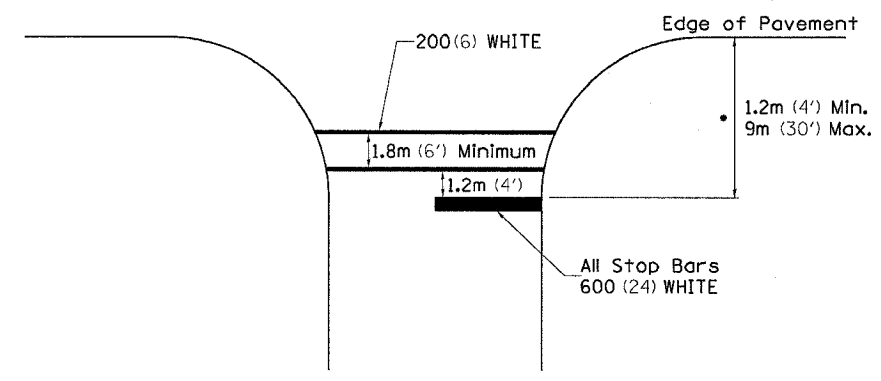
TYPICAL PARKING SPACING



•• ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

STANDARD CROSSWALK MARKING

See Schedules for Locations



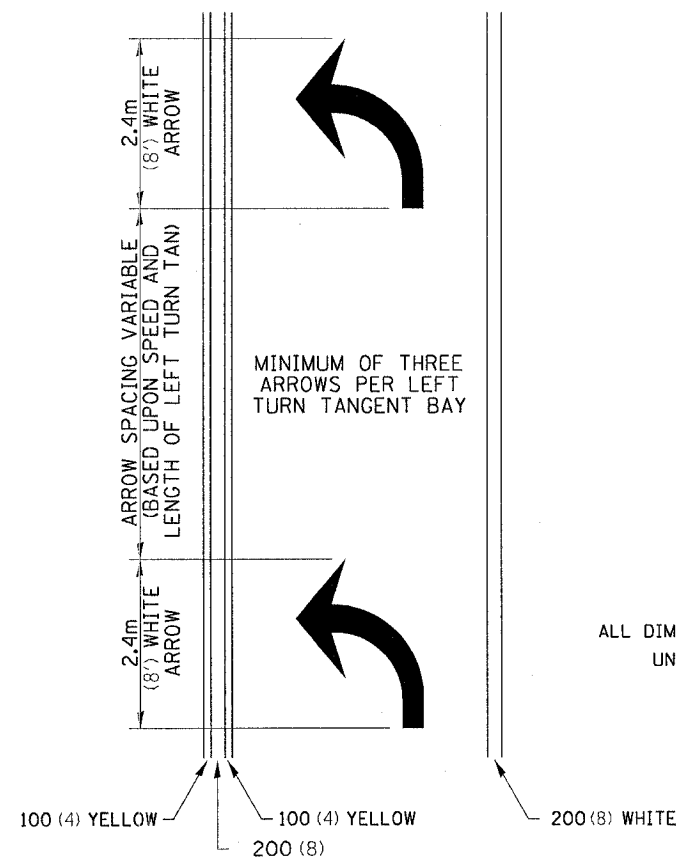
• Distance to the nearest edge of the intersecting roadway in the absence of a marked crosswalk.

Plot Date: Wed Aug 24 09:45:35 2005
 File Name: s:\2005\08\20050824\0824141.dgn
 Plot Scale: 50.00000 / IN.
 Reference: #REF#

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68	29BR-1	DEKALB	25	22
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

TYPICAL PAVEMENT MARKINGS

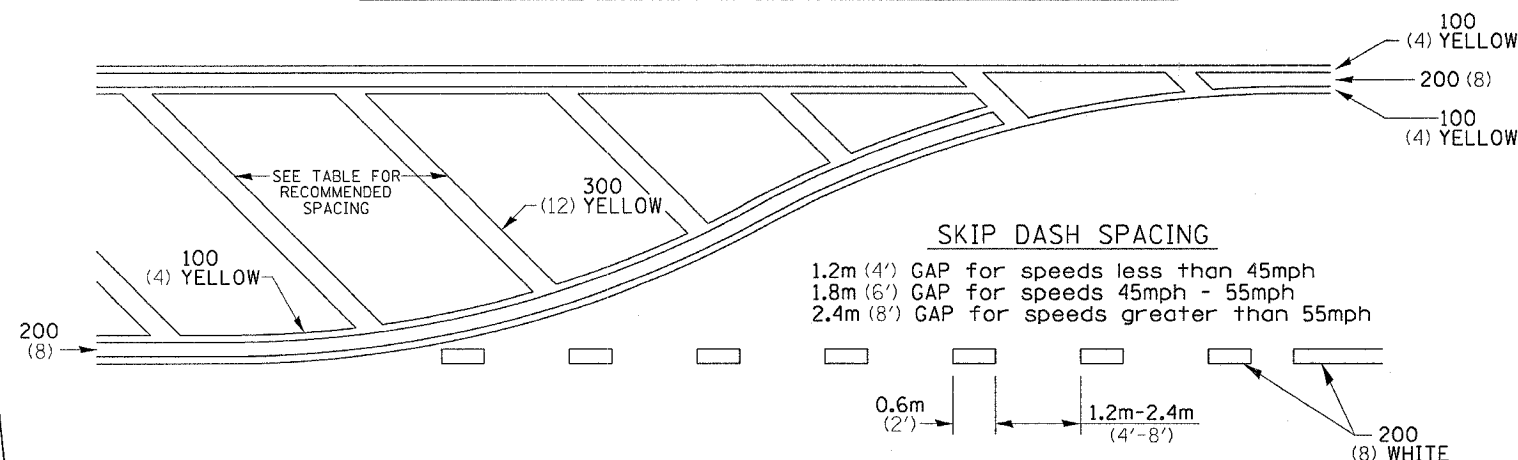
ARROW LAYOUT



- ▲ ONE-WAY AMBER MARKER
- △ ONE-WAY CRYSTAL MARKER
- ◆ TWO-WAY AMBER MARKER

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

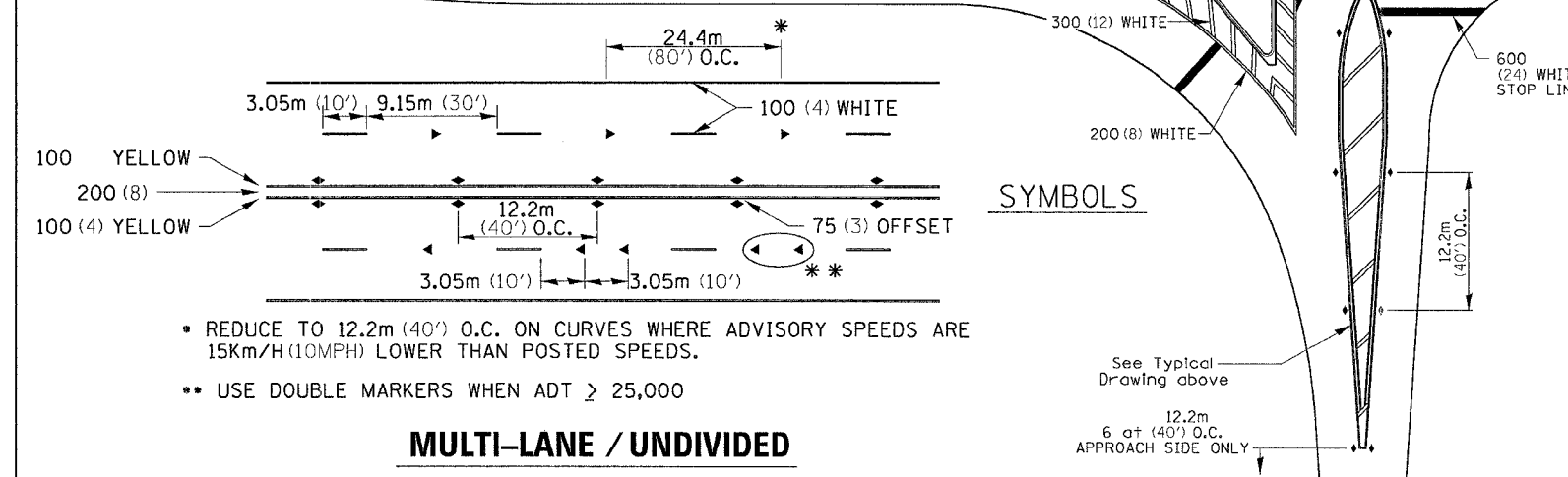
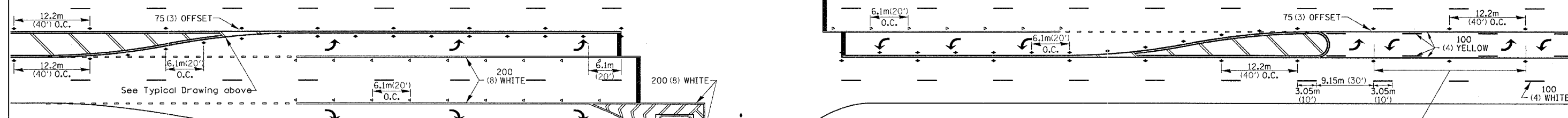
TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN



RECOMMENDED SPACING BETWEEN DIAGONALS (IN FEET)

Speed Limit Range	Continuous Median Area	Intersection Channelization	Objects (Islands)
less than 50Km/H (30MPH)	15.3m (50')	4.53m (15')	3.05m (10')
50-60Km/H (30-40MPH)	22.9m (75')	6.1m (20')	4.53m (15')
70Km/H (45MPH) & over	22.9m (75')	9.05m (30')	6.1m (20')

NOTE: if the spacing recommended in the Table does not permit at least five diagonal lines in the area being marked, the spacing from the next lowest speed range should be used. The recommended spacing is measured parallel to the pavement center line.



- * REDUCE TO 12.2m (40') O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 15Km/H (10MPH) LOWER THAN POSTED SPEEDS.
- ** USE DOUBLE MARKERS WHEN ADT ≥ 25,000

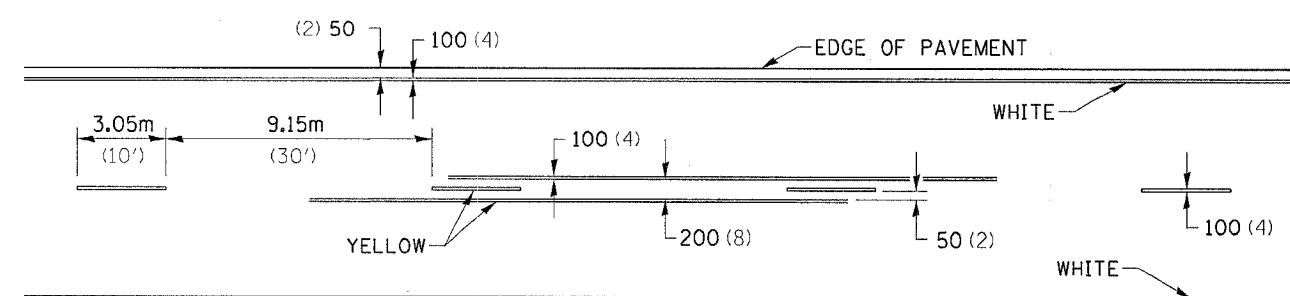
MULTI-LANE / UNDIVIDED

SYMBOLS

See Typical Drawing above

12.2m (40') O.C. APPROACH SIDE ONLY

TYPICAL PAVEMENT MARKING FOR TWO LANE SECTION - NO PASSING ZONES



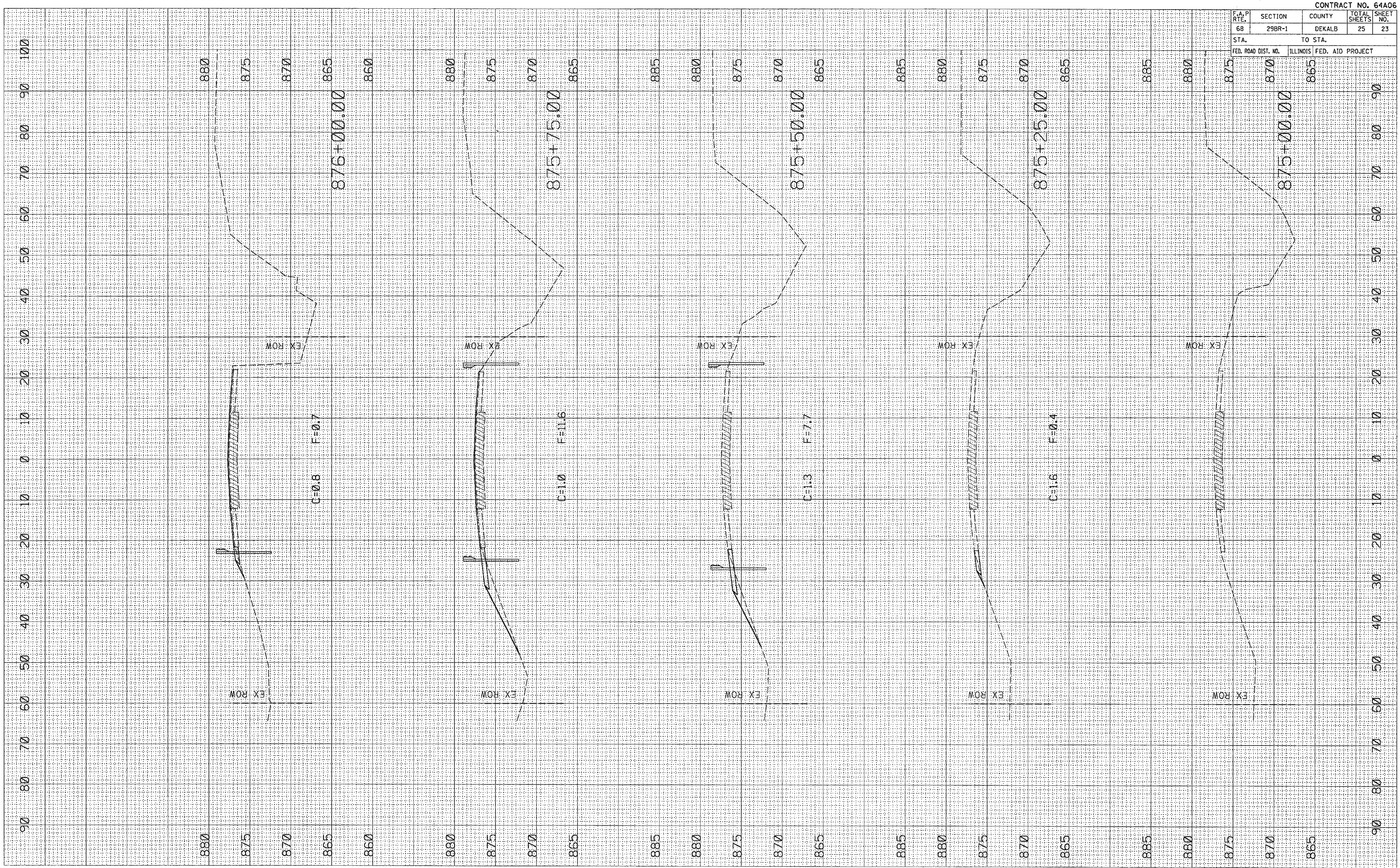
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 PLOT SCALE: 30.0000 / IN.
 REFERENCE: REF#

PLOT DATE = Wed Aug 24 09:38:41 2005
 PLOT SCALE = 10.0000 / IN.
 USER NAME = meglju

ORIGINAL SURVEYED SURVEY PLOTTED
 SURVEY PLOTTED TEMPLATE
 NOTE BOOK NO. AREAS CHECKED

FINAL SURVEY SURVEY PLOTTED
 NOTE BOOK NO. AREAS CHECKED

BY: _____ DATE: _____

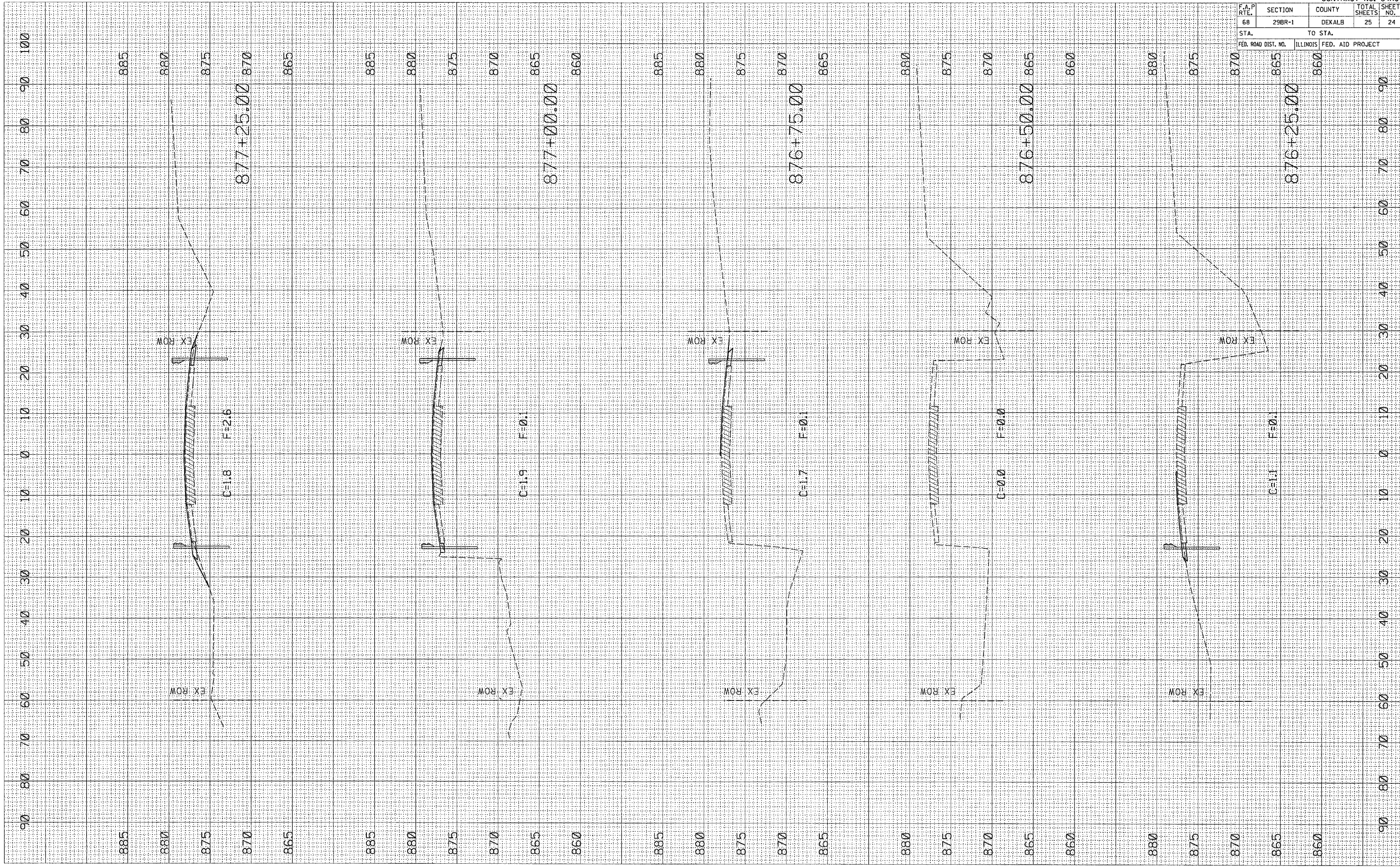


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68	298R-1	DEKALB	25	23
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

PLOT DATE = Wed Aug 24 09:38:42 2005
 PLOT SCALE = 1/4" = 100'
 USER NAME = mgajju

ORIGINAL SURVEYED BY DATE
 SURRET PLOTTED
 NOTE BOOK TEMPLATE
 AREAS CHECKED

FINAL SURVEYED BY DATE
 SURRET PLOTTED
 NOTE BOOK TEMPLATE
 AREAS CHECKED



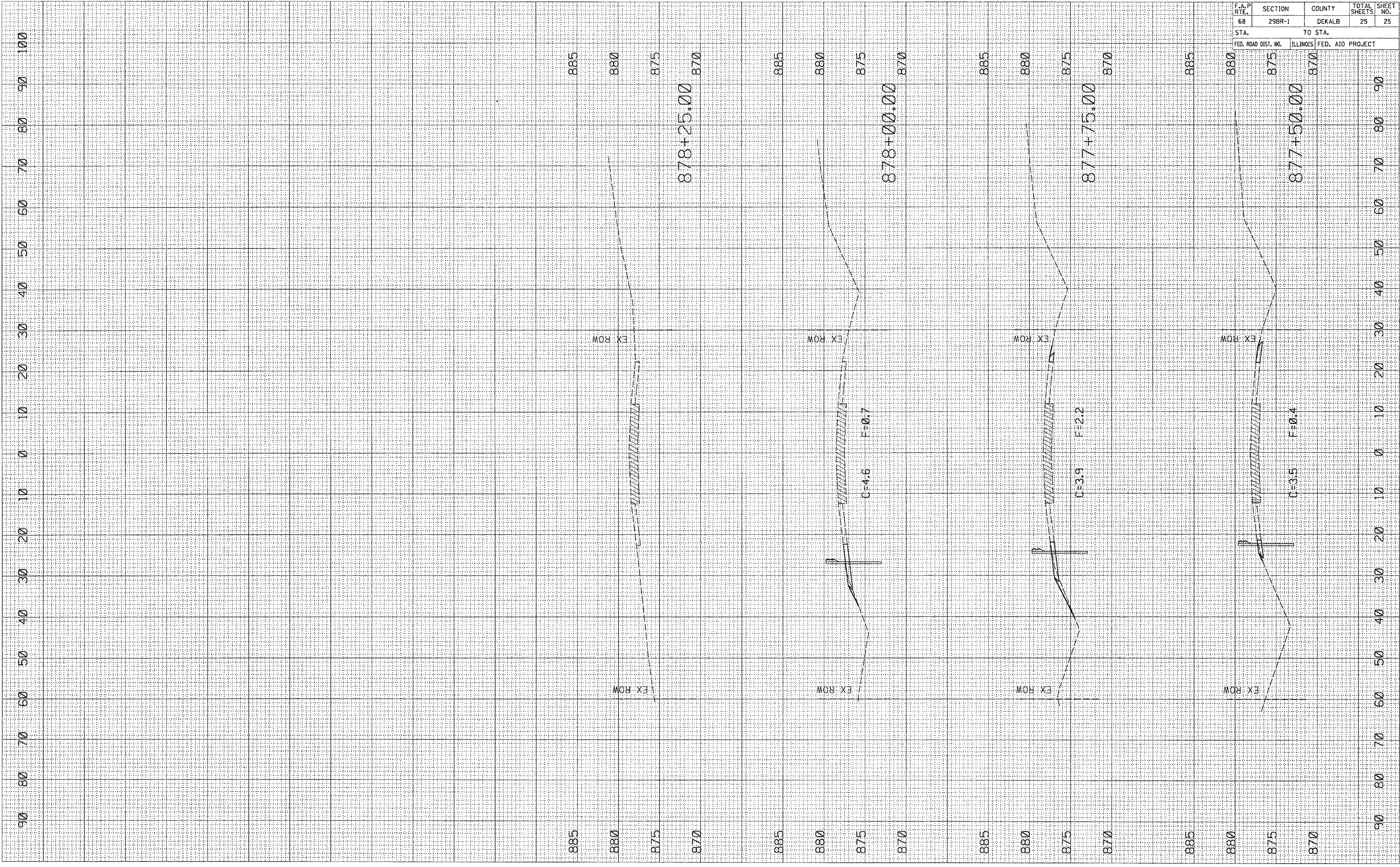
CONTRACT NO. 64A06			
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS
68	29BR-1	DEKALB	25
STA.		TO STA.	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	

PLOT DATE = Wed Aug 24 09:08:43 2005
 FILE NAME = h:\work\251\251A\251A.dwg
 PLOT SCALE = 10.0000 / IN.
 USER NAME = mgj111

ORIGINAL SURVEYED _____
 SURVEY PLOTTED _____
 NOTE BOOK NO. _____
 AREAS CHECKED _____

FIRM SURVEYED _____
 SURVEY PLOTTED _____
 NOTE BOOK NO. _____
 AREAS CHECKED _____

BY _____
 DATE _____



CONTRACT NO. 64A06			
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
68	29BR-1	DEKALB	25
STA.		TO STA.	
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	