

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROPOSED
HIGHWAY PLANS

FAP ROUTE 553 (IL 72)
SECTION 119BR-3
PROJECT ACBHF-0553(137)
OGLE COUNTY
C-92-125-05

INDEX OF SHEETS

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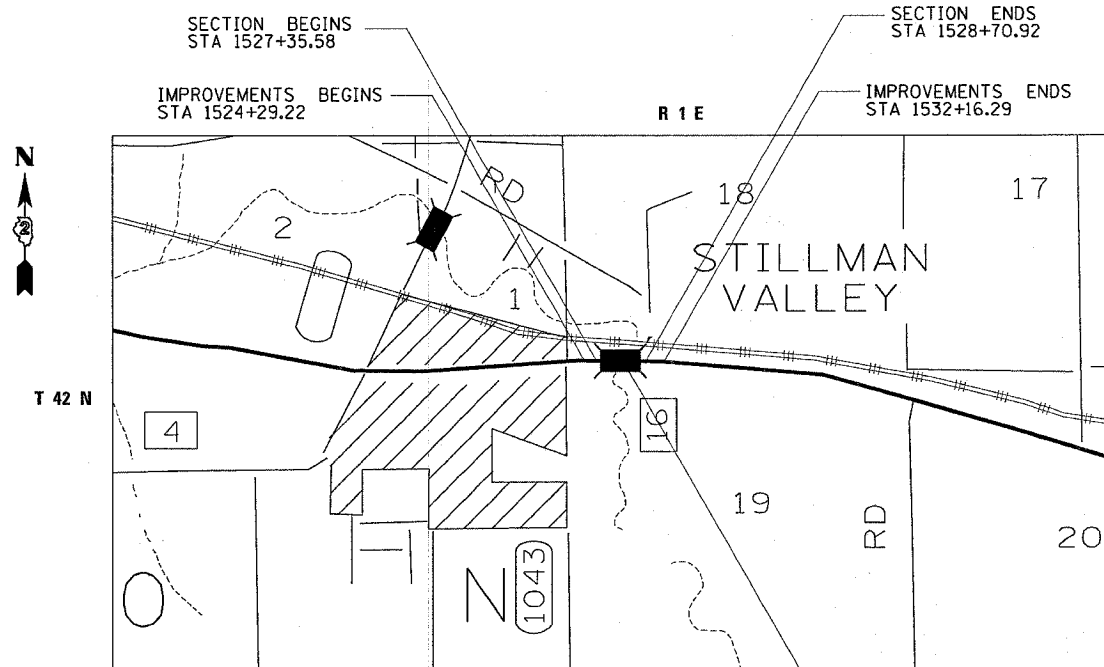
STATE STANDARDS

- 001001 AREAS OF REINFORCEMENT REBARS
- 001006 DECIMAL OF AN INCH AND A FOOT
- 420001-06 PAVEMENT JOINTS BARRIER
- 515001-02 NAME PLATE FOR BRIDGES
- 630001-05 STEEL PLATE BEAM GUARDRAIL
- 630301-03 SHOULDER WIDENING FOR TYPE 1 GUARDRAIL TERMINALS
- 631032-01 TRAFFIC BARRIER TERMINAL, TYPE 6A
- 635001 DELINEATORS
- 635006-02 REFLECTOR AND TERMINAL MARKER REPLACEMENT
- 635011-01 REFLECTOR MARKER AND MOUNTING DETAILS
- 667101 PERMANENT SURVEY MARKERS
- 701006-02 TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES
- 701201-02 TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES
- 701301-02 TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES
- 701311-02 TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES
- 701321-08 TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES
- 702001-05 TRAFFIC CONTROL DEVICES
- 704001-02 TEMPORARY CONCRETE
- 720011 METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
- 780001-01 TYPICAL PAVEMENT MARKINGS
- 886001 DETECTOR LOOP INSTALLATIONS
- 886006 TYPICAL LAYOUT FOR DETECTION LOOPS

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

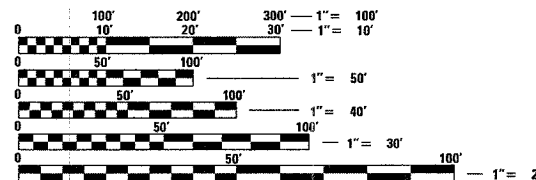
J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123

OGLE COUNTY
T 42 N-R 1 E 3PM TOWNSHIP,
SECTION 18 & 19
CONTRACT NO. 64A47



THE PROJECT IS A BRIDGE DECK BEAM REPLACEMENT PROJECT ON A SPAN STRUCTURE WITH GUARDRAIL UPDATE AND STAGE CONSTRUCTION

GROSS LENGTH OF SECTION = 135.34 FEET = 0.026 MILES
NET LENGTH OF SECTION = 135.34 FEET = 0.026 MILES



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED Sept 2 20 05
[Signature]
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

October 14, 20 05
[Signature]
 ENGINEER OF DESIGN AND ENVIRONMENT

October 14, 20 05
[Signature]
 DEPUTY DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

PLANS ENGINEER: ROBERT WAGNER

SENIOR ENGINEER: THOMAS HALLA
PHONE (815) 284-5993

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	2
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

SUMMARY OF QUANTITIES

CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	X080-2A 80 % FED 20 % STATE	SFTY-3N 80 % FED 20 % STATE
20200100	EARTH EXCAVATION	CU YD	44	44	
20400800	FURNISHED EXCAVATION	CU YD	202	202	
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	738	738	
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	150	150	
25100630	EROSION CONTROL BLANKET	SO YD	855	855	
28000400	PERIMETER EROSION BARRIER	FOOT	906	906	
44000007	BITUMINOUS CONCRETE SURFACE REMOVAL 2"	SO YD	142	142	
48101200	AGGREGATE SHOULDERS, TYPE B	TON	200	200	
48202050	BITUMINOUS SHOULDERS SUPERPAVE 2"	SO YD	24	24	
50101500	REMOVAL OF EXISTING SUPERSTRUCTURES	EACH	1	1	
50300260	BRIDGE DECK GROOVING	SO YD	323	323	
50300300	PROTECTIVE COAT	SO YD	399	399	
50301200	CONCRETE WEARING SURFACE	SO YD	333	333	
50301245	FORMED CONCRETE REPAIR (LESS THAN 5 INCHES)	SO FT	10	10	
50301250	FORMED CONCRETE REPAIR (GREATER THAN 5 INCHES)	SO FT	6	6	
50400405	PRECAST PRESTRESSED CONCRETE DECK BEAMS (21" DEPTH)	SO. FT.	3155	3155	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	4560	4560	
50901005	STEEL BRIDGE RAIL, TYPE SM	FOOT	273	273	
51500100	NAME PLATES	EACH	1	1	
* 63100087	TRAFFIC BARRIER TERMINAL, TYPE 6A	EACH	4	4	
* 63100169	TRAFFIC BARRIER TERMINAL TYPE 1, SPECIAL (FLARED)	EACH	1	1	
* 63100167	TRAFFIC BARRIER TERMINAL TYPE 1, SPECIAL (TANGENT)	EACH	3	3	
63200310	GUARDRAIL REMOVAL	FOOT	325	325	
63301210	REMOVE AND RE-ERECT STEEL PLATE BEAM GUARDRAIL, TYPE A	FOOT	875	875	
63500105	DELINEATORS	EACH	4	4	

* SPECIALTY ITEMS

PLOT DATE = Fri Sep 02 11:41:05 2005
 PLOT SCALE = 5000000 / IN.
 USER NAME = mag111

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		SCALE: VERT. DATE HORIZ. DRAWN BY CHECKED BY

SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	3
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

SUMMARY OF QUANTITIES

CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	X080-2A 80 % FED 20 % STATE	SFTY-3N 80 % FED 20 % STATE
66700305	PERMANENT SURVEY MARKERS, TYPE II	EACH	2	2	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3	
67100100	MOBILIZATION	L SUM	1	1	
70100405	TRAFFIC CONTROL AND PROTECTION STD 701321	EACH	1	1	
70100450	TRAFFIC CONTROL AND PROTECTION STD. 701201	L SUM	1	1	
70106500	TRAFFIC TEMPORARY BRIDGE SIGNALS	EACH	1	1	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	516	516	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	492	492	
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	6296	6296	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	2	2	
78200410	GUARDRAIL MARKERS, TYPE A	EACH	40	40	
78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	2	2	
X0323558	BRIDGE JOINT SYSTEM EXPANSION 1 5/8"	FOOT	36	36	
X0712400	TEMPORARY PAVEMENT	SQ YD	57	57	
X4066414	BITUMINOUS CONCRETE SURFACE COARSE, SUPERPAVE, MIC "C", N50	TON	44	44	
Z0002600	BAR SPLICERS	EACH	104	104	
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2		2
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2		2

* SPECIALTY ITEMS

PLOT DATE = Fri Sep 02 11:42:20 2005
 PLOT SCALE = 5/8" = 1' IN.
 USER NAME = megl11j

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		SCALE: VERT. DATE: HORIZ.

DRAWN BY
CHECKED BY

SUMMARY OF QUANTITIES

GENERAL NOTES

ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FAP 553	119BR-3	Ogle	40	4
FED ROAD DIST. NO.	ILLINOIS	PROJECT		
Contract #64A47				

The final top 100 mm (four inches) of soil in any right-of-way area disturbed by the Contractor must be capable of supporting vegetation. The soil must be from the A horizon (zero to 2' deep) of soil profiles of local soils.

The Contractor shall seed all disturbed areas within the project limits. Seeding Class 4 or 6 (modified) shall be used, except in front of properties where the grass will be mowed, then use Seeding, Class 1 (modified). Class 6 (modified) shall be used on front slopes and ditch bottoms. Class 4 shall be used behind Type A gutter, on all backslopes and areas behind the backslope, and beyond the toe of front slope on fill sections without ditches. This work will be included in the contract unit price per Cubic Meter (Cubic Yard) for FURNISHED EXCAVATION.

Fertilizer shall be applied to all disturbed areas and incorporated into the seedbed prior to seeding or placement of sod at the rate specified in Sections 250 and 252 of the Standard Specifications. This work shall be included in the cost of FURNISHED EXCAVATION.

Mulch Method II shall be applied over all seeded areas. This shall be included in the cost of the FURNISHED EXCAVATION.

The following Mixture Requirements are applicable for this project:

Mixture Uses(s):	Mainline Surface Course
PG:	PG 64-22
RAP%: (Max)	10%
Design Air Voids	4.2 @ N50
Mixture Composition (Gradation Mixture)	IL 9.5 or 12.5
Friction Aggregate	D
20 Year ESAL	4.3

Install a "TO ACTUATE SIGNAL" sign for the traffic signal detector loops. The detail of this sign is included in the plans. This work will be included in the cost of TRAFFIC CONTROL AND PROTECTION STANDARD 701321.

Bituminous and Aggregate prime coat shall be placed in accordance with Section 406 of the Standard Specifications. The cost of the prime coats shall be included in the contract unit price per metric ton (ton) for LEVELING BINDER (MACHINE METHOD) of the type specified.

This structure will retain the same number 071-0042.

Embankment quantities for the construction of the Traffic Barrier Terminals as shown in the plans are included in quantities for Earth Excavation.

The Contractor shall supply the Resident Engineer with the manufacturer's installation requirements for the type of Steel Plate Beam Guardrail Terminal Type 1 Special (Tangent) or Steel Plate Beam Guardrail Terminal Type I Special (Flared).

Bituminous Prime Coat shall be placed in accordance with Section 406 of the Standard Specifications. The cost of the Bituminous Prime Coat shall be included in the contract unit price per TON for SURFACE COURSE, TYPE I, N50.

Existing end sections shall be salvaged and stored in a manner for re-use. When the end sections have been removed, the state maintenance yard shall be contacted for delivery.

One 16d galvanized nail shall be used to toe nail the wood block out to the wood post on all Traffic Barrier Terminal Type I Specials.

Delineators shall be installed as shown in Standard 635001, except that the post shall be rotated 180° and only metal-backed delineators shall be permitted.

Delineators shall be placed at the ends of approach guardrail terminal sections, and at each headwall or end section of AR Culverts. This work will be paid for at the contract unit price each for DELINEATORS.

Pavement marking shall be done according to Standard 780001, except as follows:

1. All words, such as ONLY, shall be 2.4 m (8 feet) high.
2. All non-freeway arrows shall be the large size.
3. The distance between yellow no-passing lines shall be 200 mm (8"), not 180 mm (7") as shown in the detail of Typical Lane and Edge Lines.

Permanent survey markers, Type II shall be cast-in-place as shown on Highway Standard 667101. A marker shall be placed near each end of the structure in such a location that will take into account satellite and future construction. Location shall be determined by the Engineer.

The Contractor shall submit to the Engineer a description of location, elevation, and coordinates for each permanent survey marker. The Engineer shall submit this information to the Survey Crew.

The Contractor shall be responsible for protecting utility property during construction operations as outlined in Article 107.31 of the Standard Specifications. A minimum of 48 hours advance notice is required for non-emergency work. The JULIE number is 800-892-0123. The following listed utilities located within the project limits or immediately adjacent to the project construction limits are members of JULIE:

- | | |
|---------------------|---------------------|
| Mediacom | Commonwealth Edison |
| Alliant Energy | Citizen's Telephone |
| Dome Pipeline Corp. | |

Following are the known utilities located within the project limits or immediately adjacent to the project construction limits which are not members of JULIE and should be notified individually by the contractor:

Mr. Dennis Schultz
IDOT
819 Depot Ave.
Dixon, IL 61021

Due to environmental concerns, the following shall be strictly adhered to:

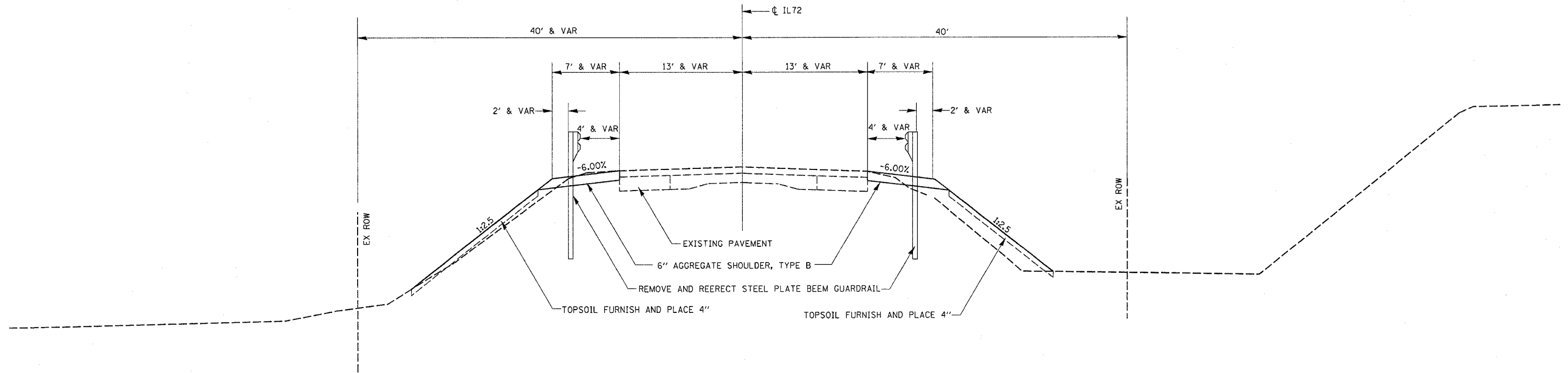
1. All work shall be performed from the existing decks and no work shall take place below the existing structure on the ground.
2. No fill shall be placed in or around Stillman Creek.

Program #5
(Arch. Size)
Enlarge
200%
Enlarge 107%

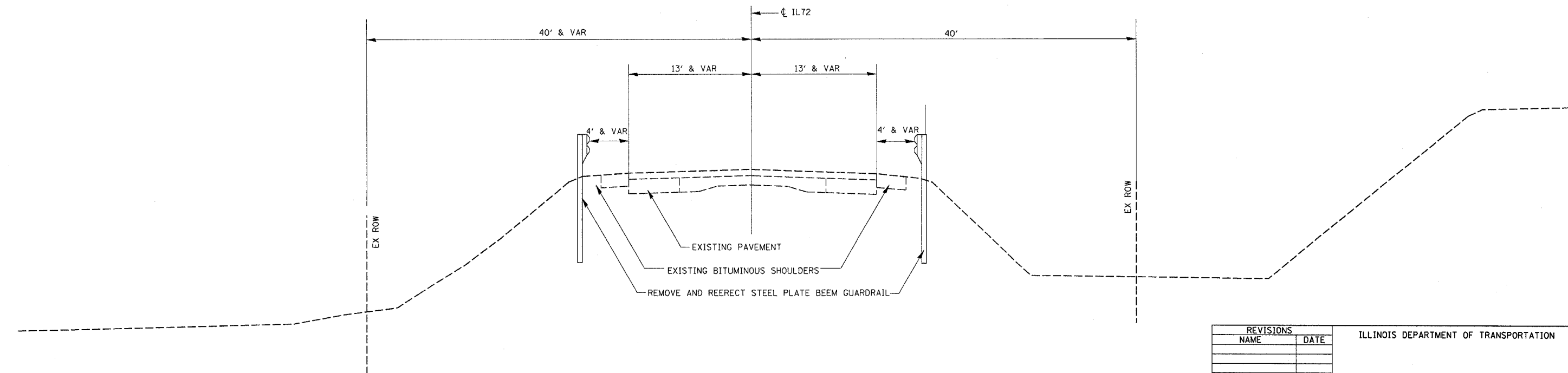
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	5
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

TYPICAL SECTIONS

RT STA 1524+29 TO 1526+19 AND RT STA 1529+53 TO 1532+16
 LT STA 1524+29 TO 1527+02 AND LT STA 1529+67 TO 1532+16



RT STA 1526+19 TO 1527+56 AND RT STA 1528+51 TO 1529+53
 LT STA 1527+02 TO 1527+56 AND LT STA 1528+51 TO 1529+67



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 USER NAME = mgj11j

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCALE: VERT. _____
 HORIZ. _____
 DATE _____

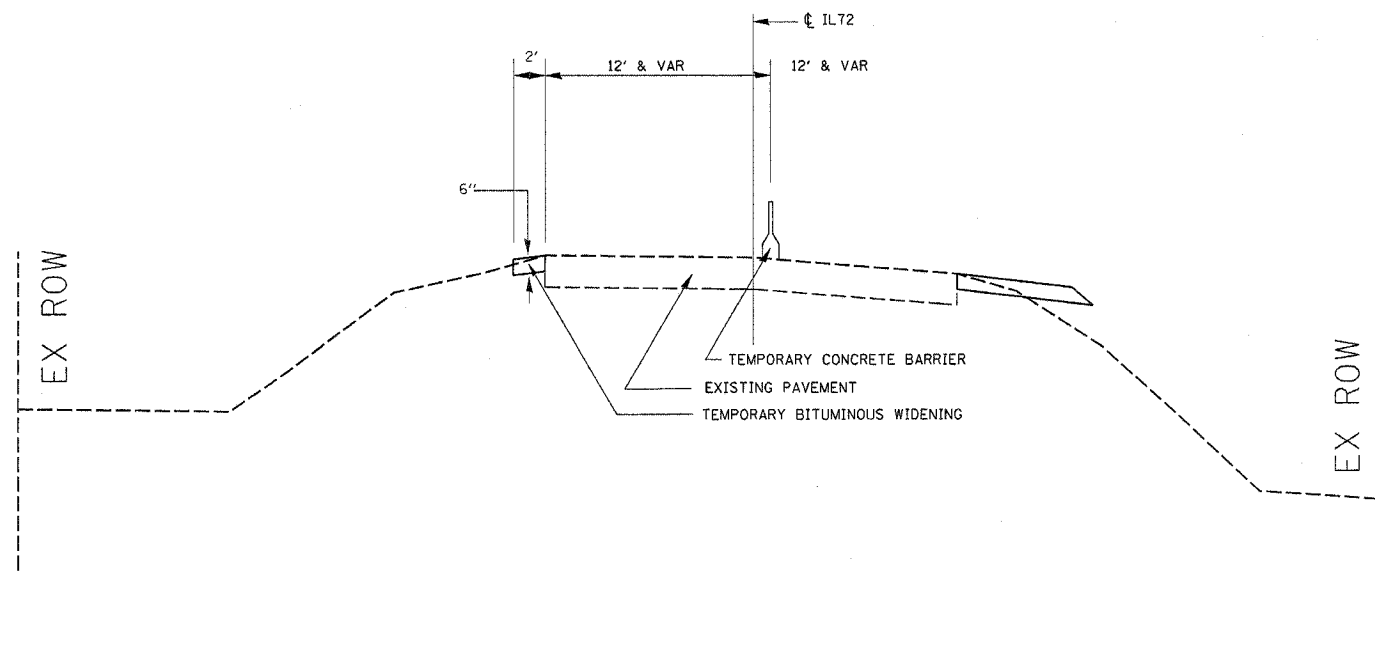
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TYPICAL SECTIONS

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STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

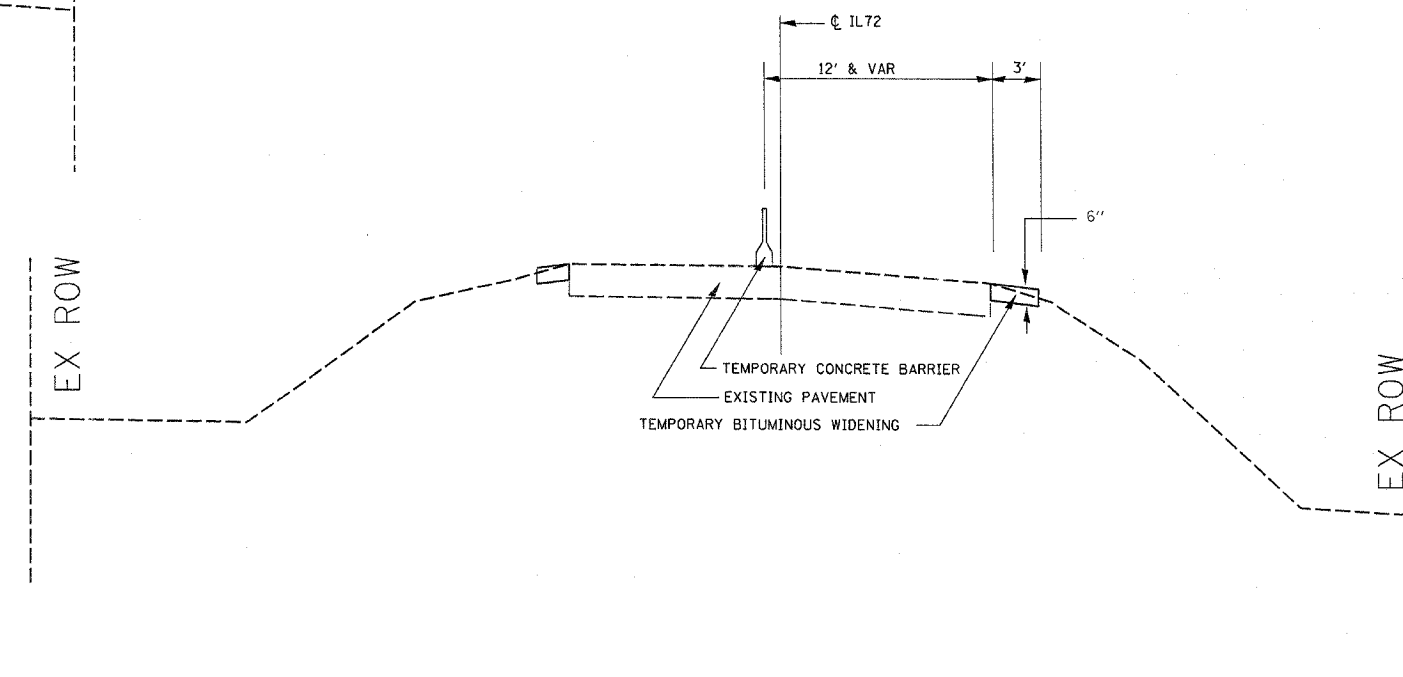
STAGE 1

1529+07 TO 1530+00 RT



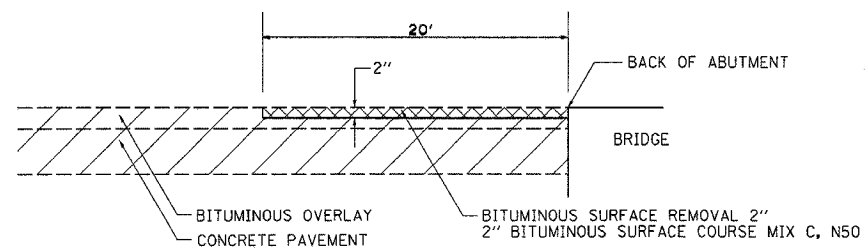
STAGE 2

1529+17 TO 1530+00 RT



BUTT JOINT

BITUMINOUS SURFACE REMOVAL - 2"
 STA 1527+35.58 TO STA 1527+55.58 & STA 1528+50.92 TO STA 1528+70.92



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		SCALE: VERT. HORIZ. DATE
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BUTT JOINT DETAIL

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 USER NAME = mgj11

SCHEDULE OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	7
STA. TO STA.			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	

20200100	EARTH EXCAVATION					
	CU YD	LOCATION				
	22	1524 +	29 -	1531 +	17 RT	
	22	1524 +	89 -	1532 +	16 LT	
	44	TOTAL				
21101615	TOPSOIL FURNISH AND PLACE, 4"					
	SO YD	LOCATION				
	369	1524 +	29 -	1531 +	17 RT	
	369	1524 +	89 -	1532 +	16 LT	
	738	TOTAL				
25100630	EROSION CONTROL BLANKET					
	SO YD	LOCATION				
	444	1524 +	29.22 -	1527 +	55.59 LT & RT	
	411	1529 +	53.31 -	1532 +	16.29 LT & RT	
	832	TOTAL				
28000400	PERIMETER EROSION BARRIER					
	FOOT	LOCATION				
	435	1524 +	29.22 -	1527 +	1.7 LT & RT	
	471	1528 +	50.93 -	1532 +	16.29 LT & RT	
	906	TOTAL				
44000007	BITUMINOUS CONCRETE SURFACE REMOVAL 2"					
	SO YD	LOCATION				
	71	1527 +	35.59 -	1527 +	55.59	
	71	1528 +	50.93 -	1528 +	70.93	
	142	TOTAL				
48101200	AGGREGATE SHOULDERS, TYPE B					
	TON	LOCATION				
	100	1524 +	29.22 -	1527 +	55.59	
	100	1528 +	50.93 -	1532 +	16.29	
	200	TOTAL				
48202600	BITUMINOUS SHOULDERS SUPERPAVE 2"					
	SO YD	LOCATION				
	12.0	1527 +	35.59 -	1527 +	55.59	
	12.0	1528 +	50.93 -	1528 +	70.93	
	24.0	TOTAL				
63100087	TRAFFIC BARRIER TERMINAL, TYPE 6A					
	EACH	LOCATION				
	1	1527 +	14.22 -	1527 +	48.12 RT	
	1	1527 +	28.74 -	1527 +	62.64 LT	
	1	1528 +	43.74 -	1528 +	77.64 RT	
	1	1528 +	58.19 -	1528 +	92.09 LT	
	4	TOTAL				
63100169	TRAFFIC BARRIER TERMINAL, TYPE 1, SPECIAL (FLARED)					
	EACH	LOCATION				
	1	1524 +	64.22 -	1524 +	76.72 RT	
	1	TOTAL				
63100167	TRAFFIC BARRIER TERMINAL, TYPE 1, SPECIAL (TANGENT)					
	EACH	LOCATION				
	1	1525 +	28.74 -	1525 +	41.24 LT	
	1	1530	65.14 -	1530	77.64 RT	
	1	1531 +	54.59 -	1531 +	67.09 LT	
	3	TOTAL				
63200310	GUARDRAIL REMOVAL					
	FOOT	LOCATION				
	39	1524 +	85.71 -	1525 +	24.44 RT	
	50	1525 +	32.86 -	1525 +	84.4 LT	
	34	1527 +	14.37 -	1527 +	48.12 RT	
	34	1527 +	48.12 -	1527 +	62.49 LT	
	34	1528 +	43.8 -	1528 +	77.64 RT	
	34	1528 +	58.19 -	1528 +	92.09 LT	
	50	1530 +	21.74 -	1530 +	74.29 RT	
	50	1531 +	16.19 -	1531 +	69.34 LT	
		325	TOTAL			

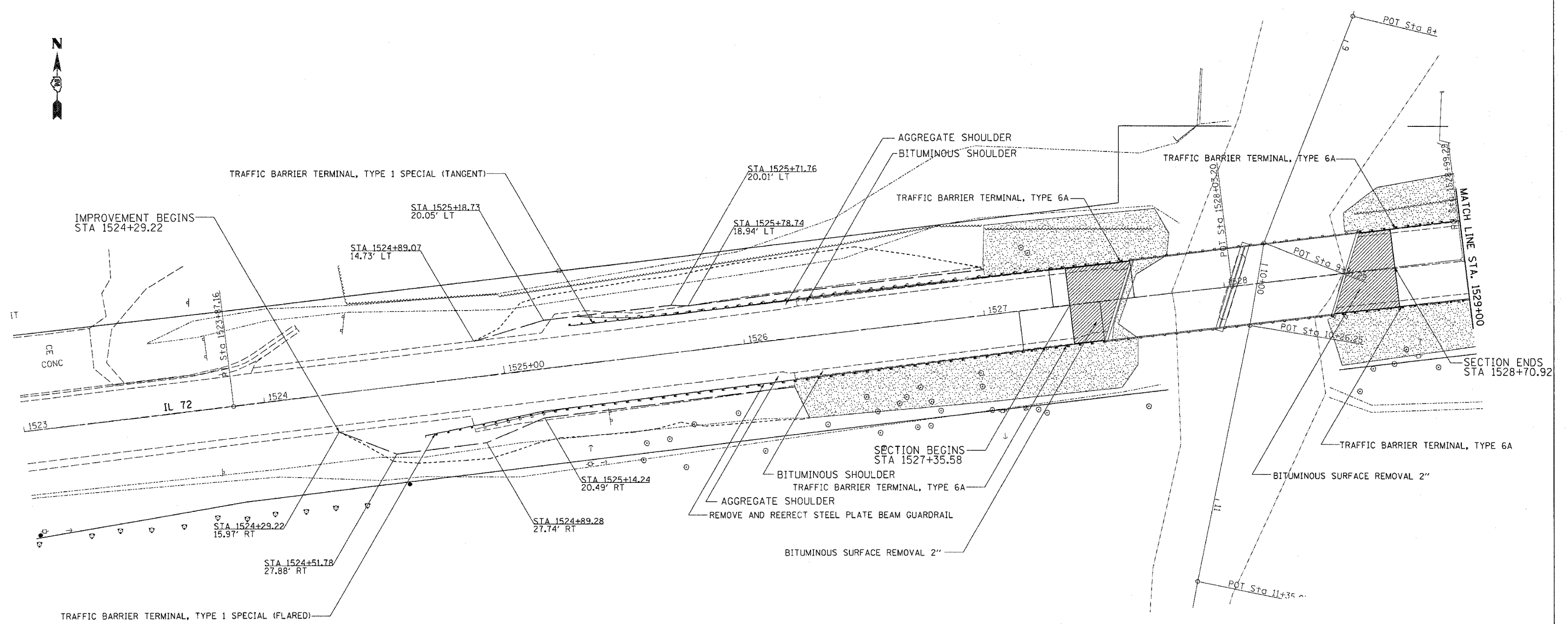
63301210	REMOVE AND RE-ERECT STEEL PLATE BEAM GUARDRAIL, TYPE A				
	FOOT	LOCATION			
	237.5	1524	76.72 -	1527	14.22 RT
	187.5	1525	41.24 -	1527	28.74 LT
	187.5	1528 +	77.64 -	1530 +	65.14 RT
	262.5				
	875	TOTAL			
63500105	DELINEATORS				
	EACH	LOCATION			
	1	1524 +	64.22	RT	
	1	1525 +	28.74	LT	
	1	1530 +	65.14	RT	
	4	TOTAL			
70400100	TEMPORARY CONCRETE BARRIER				
	FOOT	LOCATION			
	516.0	1525 +	20 -	1530 +	34.79
	516	TOTAL			
70400200	RELOCATE TEMPORARY CONCRETE BARRIER				
	FOOT	LOCATION			
	492.0	1525 +	69.56 -	1530 +	61.14
	492	TOTAL			
78001110	PAINT PAVEMENT MARKING LINE 4"				
	FOOT	LOCATION			
	3148	1524 +	29.22 -	1532 +	16.29
	3148	1524 +	29.22 -	1532 +	16.29
	6296	TOTAL			
78100100	RAISED REFLECTIVE PAVEMENT MARKER				
	EACH	LOCATION			
	1	1527 +	45		
	1	1528 +	60		
	2	TOTAL			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL				
	EACH	LOCATION			
	1	1527 +	45		
	1	1528 +	60		
	2	TOTAL			
X2500200	TEMPORARY EROSION CONTROL				
	POUND	LOCATION			
	75	1524 +	29.22 -	1527 +	55.59
	75	1528 +	50.93 -	1532 +	16.29
	150	TOTAL			
X4066414	BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX C, N50				
	TON	LOCATION			
	10	1527 +	35.59 -	1527 +	55.59
	10	1528 +	50.93 -	1528 +	70.93
	20	TOTAL			

PLOT DATE = Fri Sep 02 11:57:51 2005
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 USER NAME = meg111

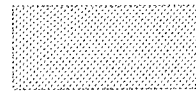
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		SCALE: VERT. DATE: HORIZ. DRAWN BY CHECKED BY

SCHEDULE OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	1198R-3	OGLE	40	8
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



LEGEND

RIP RAP= 

STAGE 1
 STA 1524+29.22 TO STA 1527+55.59
 EARTH EXCAVATION = 13.68
 FURNISHED EXCAVATION = 30.81
 WASTE = 0

STAGE 2
 STA 1524+29.22 TO STA 1527+55.59
 EARTH EXCAVATION = 10.25
 FURNISHED EXCAVATION = 65.43
 WASTE = 0

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCALE: VERT. _____
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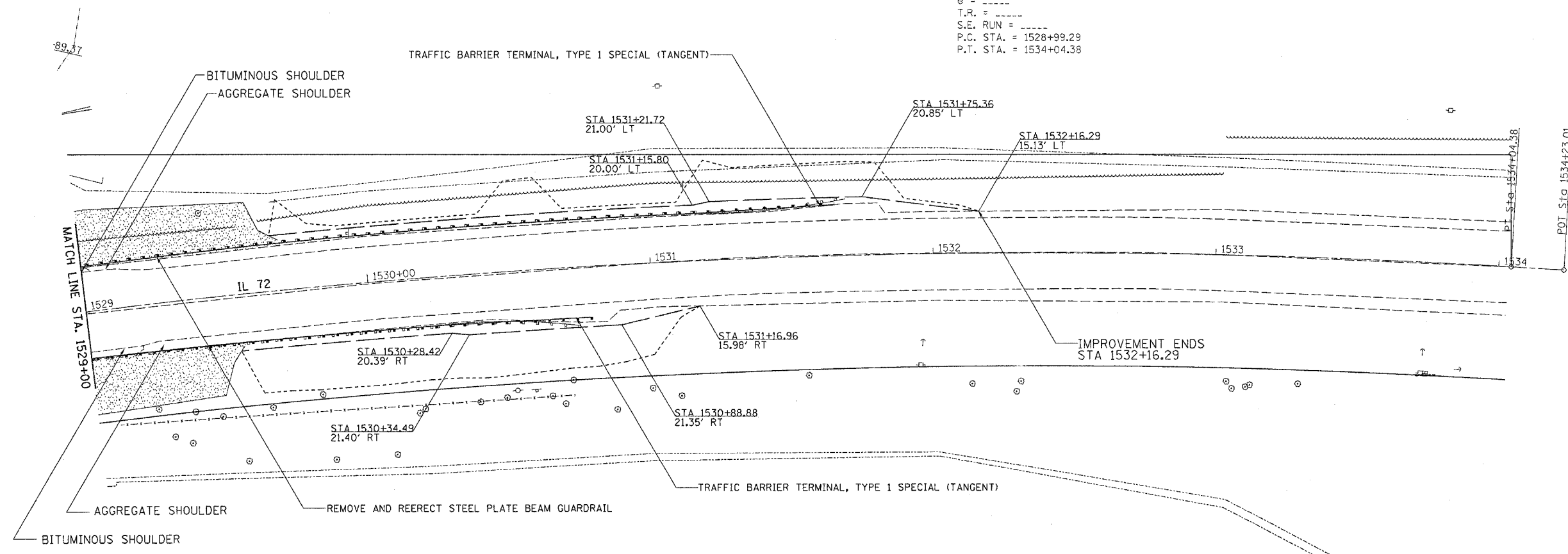
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 USER NAME = megjll

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	9
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



P.C. STA. = 1531+52.51
 $\Delta = 10^\circ 14' 39''$ (RT)
 $D = 2^\circ 01' 41''$
 $R = 2,824.97'$
 $T = 253.22'$
 $L = 505.09'$
 $E = 11.33'$
 $e = \text{---}$
 $T.R. = \text{---}$
 $S.E. RUN = \text{---}$
 $P.C. STA. = 1528+99.29$
 $P.T. STA. = 1534+04.38$



STAGE 1
 STA 1528+50.93 TO STA 1532+16.29
 EARTH EXCAVATION = 7.21
 FURNISHED EXCAVATION = 76.63
 WASTE = 0

STAGE 2
 STA 1528+50.93 TO STA 1532+16.29
 EARTH EXCAVATION = 12.44
 FURNISHED EXCAVATION = 28.73
 WASTE = 0

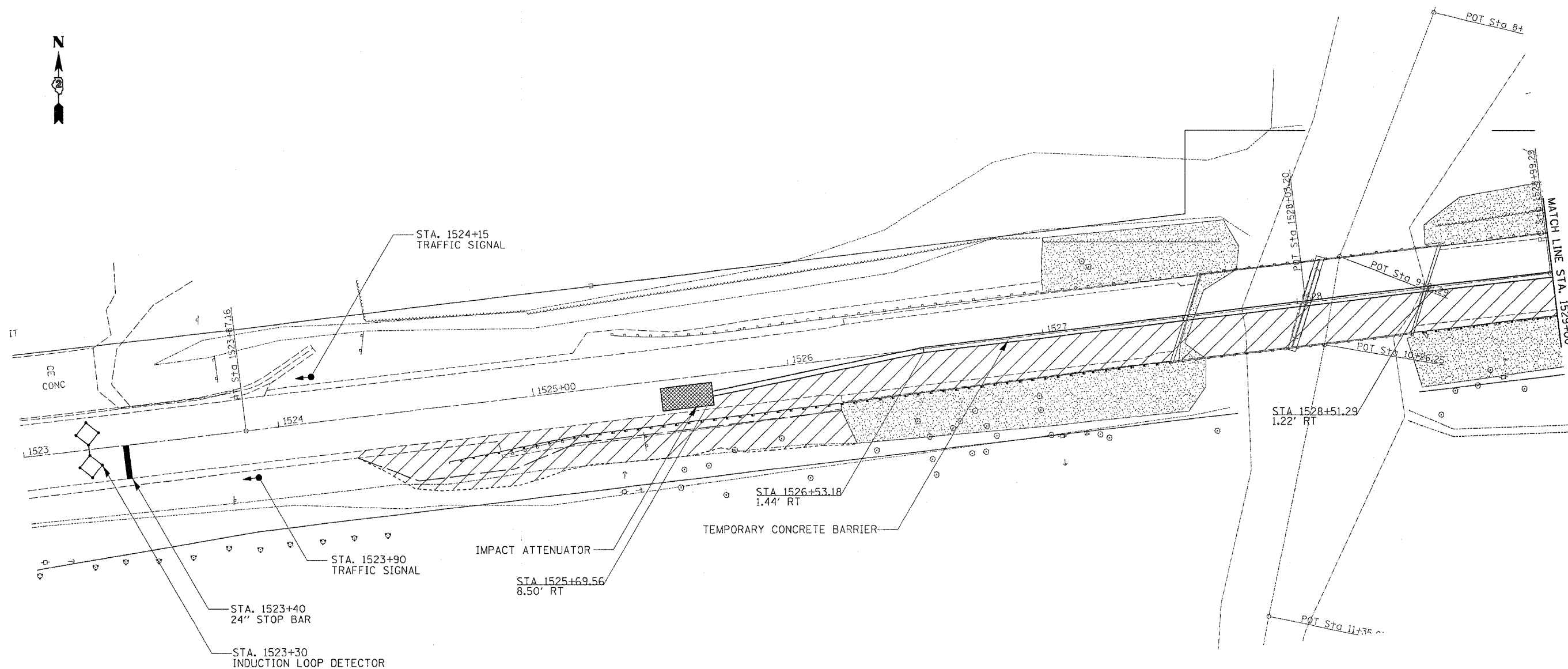
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		SCALE: VERT. / HORIZ. / DATE

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 PLOT SCALE = 20.000000 / IN
 USER NAME = meglju

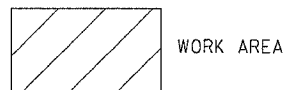
STAGING DETAILS

CONTRACT NO. 64A47

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	10
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



NOTE:
 THIS TRAFFIC CONTROL AND PROTECTION SHALL BE SET UP AND PAID FOR ACCORDING TO STANDARD 701321-08



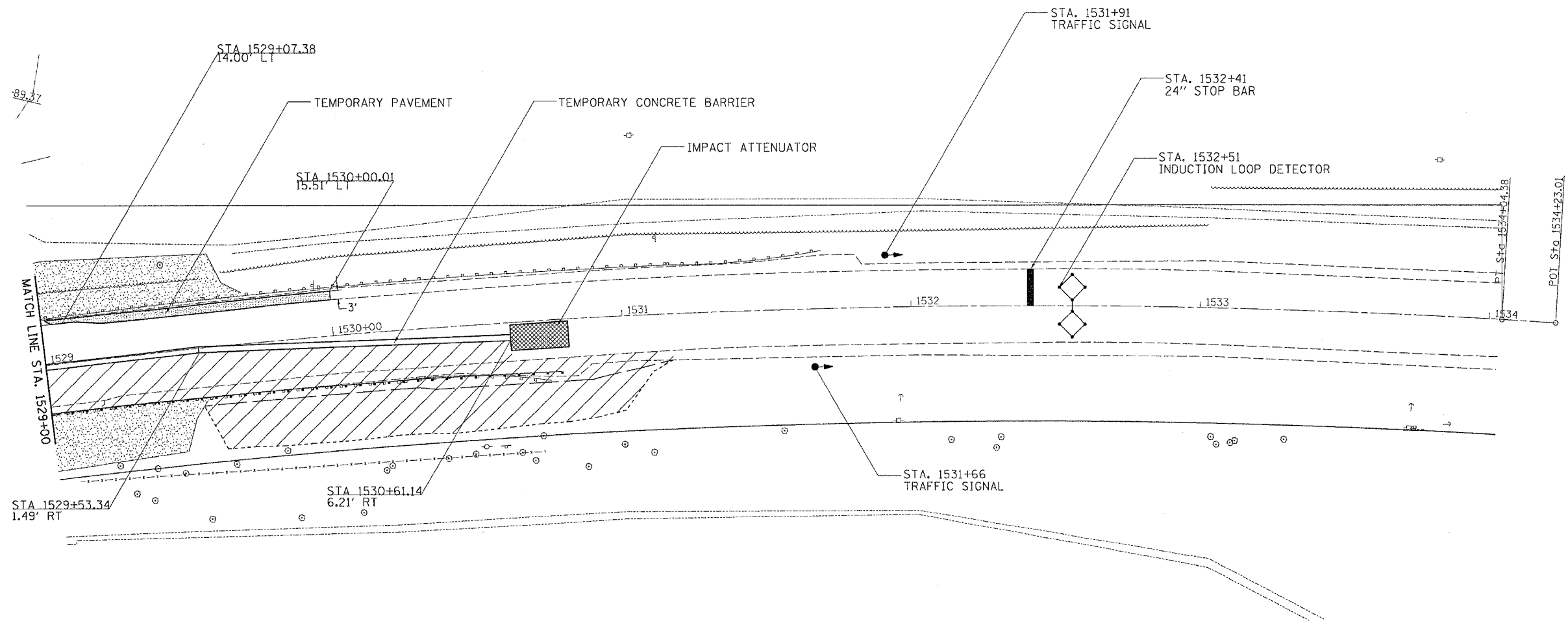
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		SCALE: VERT. HORIZ. DATE
DRAWN BY		CHECKED BY

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 PLOT NAME = I:\2005\119BR-3\119BR-3-10.dgn
 PLOT SCALE = 3/8"=1'-0"
 USER NAME = mg1111

STAGING DETAILS

CONTRACT NO. 64A47

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	11
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



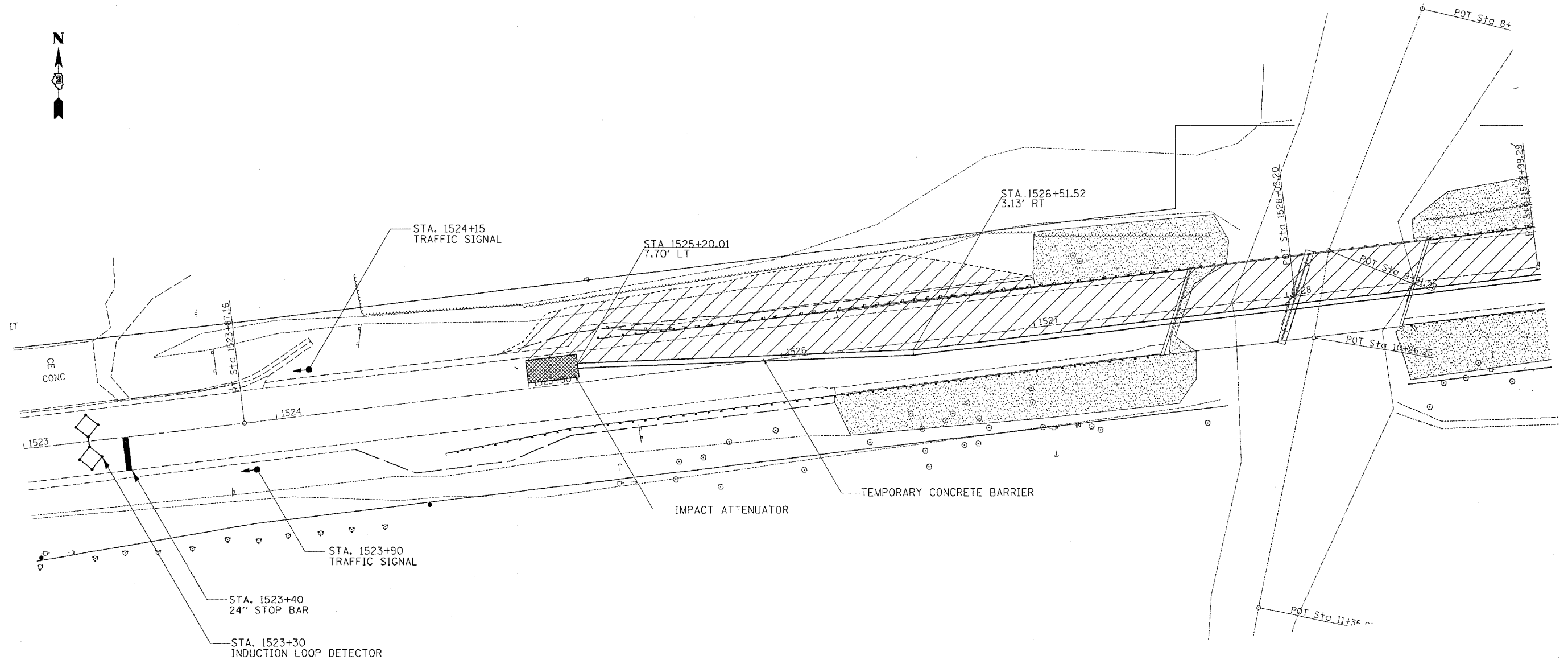
NOTE:
 THIS TRAFFIC CONTROL AND PROTECTION SHALL BE SET UP AND PAID FOR ACCORDING TO STANDARD 701321-08

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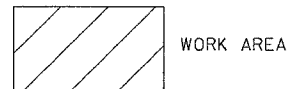
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		SCALE: VERT. HORIZ. DATE DRAWN BY CHECKED BY

STAGING DETAILS

CONTRACT NO. 64A47				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	12
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



NOTE:
THIS TRAFFIC CONTROL AND PROTECTION SHALL BE SET UP AND PAID FOR ACCORDING TO STANDARD 701321-08

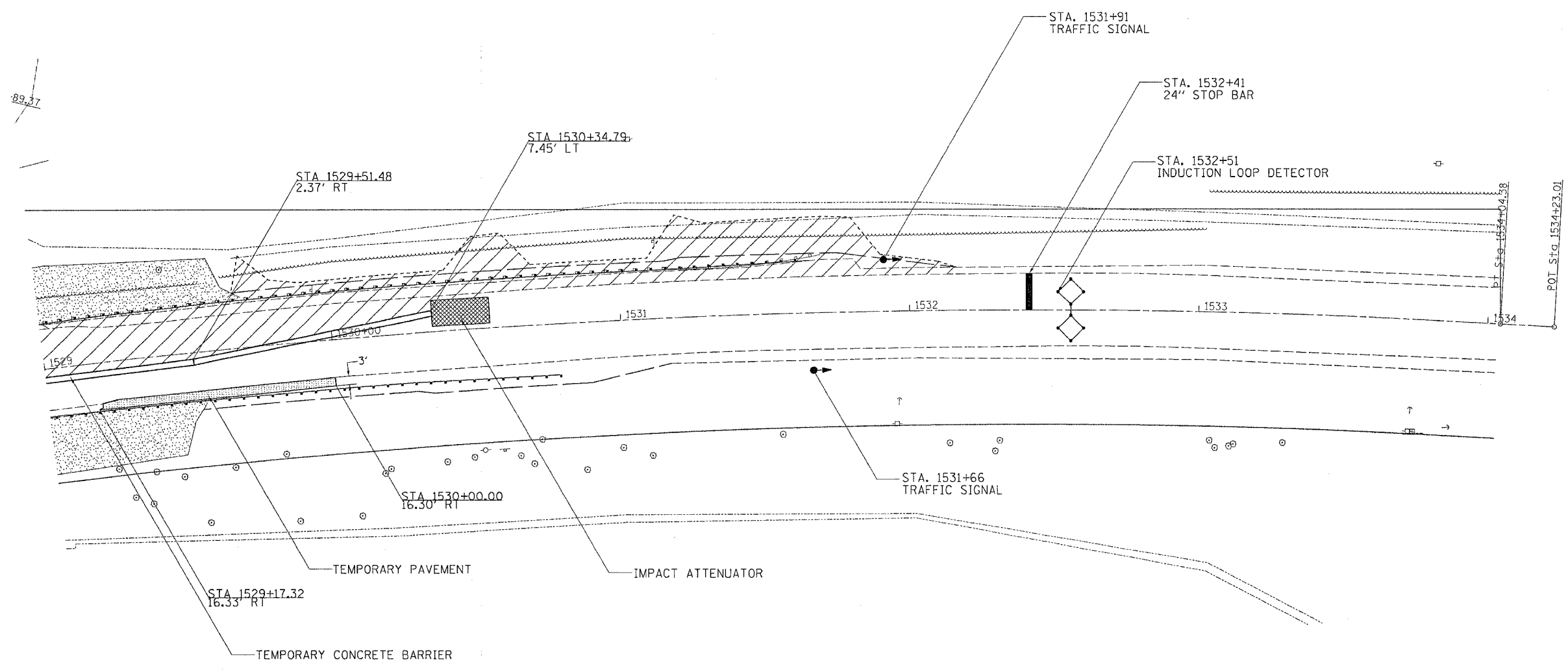


REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		SCALE: VERT. / HORIZ. / DATE
DRAWN BY		CHECKED BY

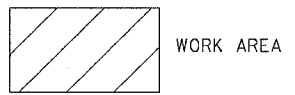
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USER NAME = mgj11j

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	13
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

STAGING DETAILS



NOTE:
 THIS TRAFFIC CONTROL AND PROTECTION SHALL BE SET UP AND PAID FOR ACCORDING TO STANDARD 701321-08



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

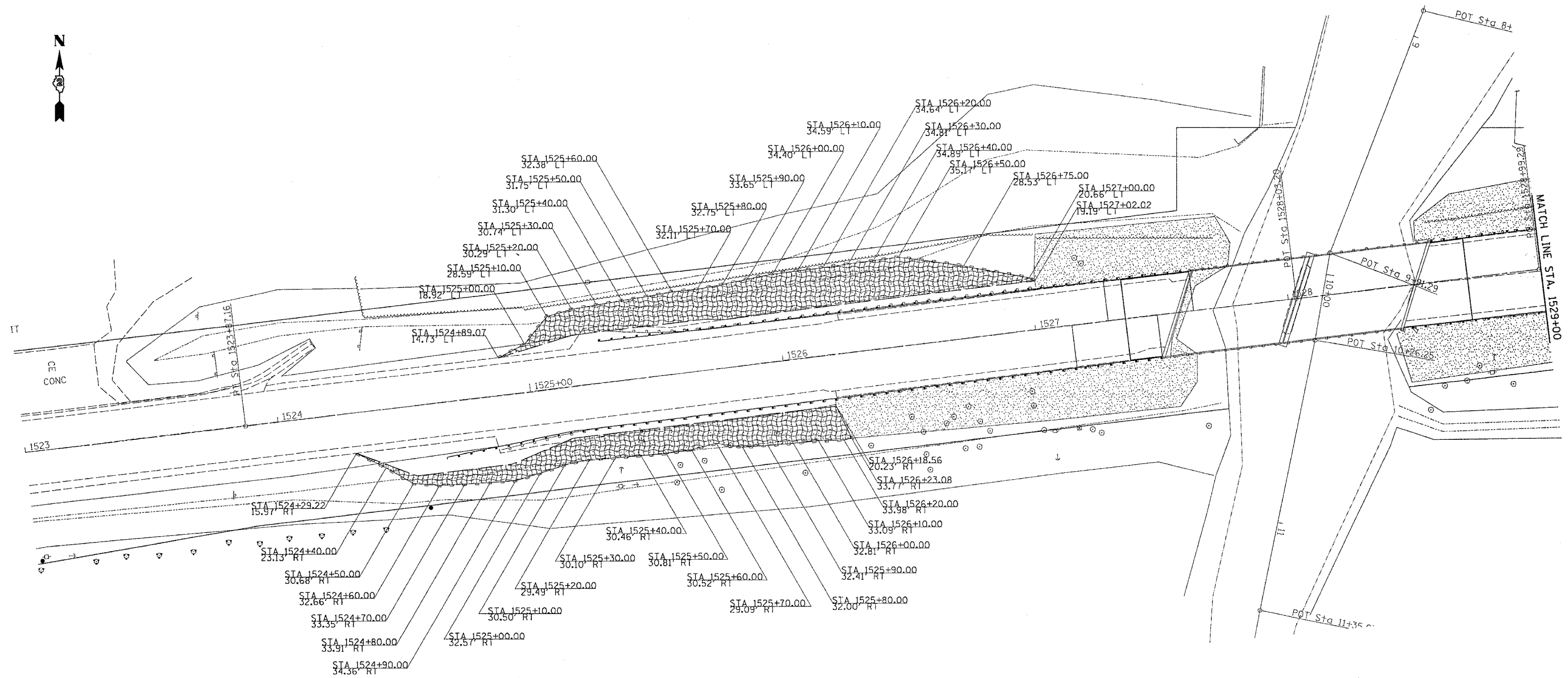
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 HORIZ. _____
 DATE _____

DRAWN BY _____
 CHECKED BY _____

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 PLOT SCALE = 20.0000 1/4" = 1'-0"
 USER NAME = meg111

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	14
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

EROSION CONTROL DETAILS



LEGEND

- = SEEDING
- = EROSION CONTROL BLANKET
- = PERIMETER EROSION BARRIER

PLOT DATE = Fri, Sep 02 11:49:45 2005
 FILE NAME = c:\projects\119BR-3\119BR-3.dgn
 PLOT SCALE = 20.0000' / 1" IN.
 USER NAME = meg111

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCALE: VERT. _____
 HORIZ. _____

DATE _____

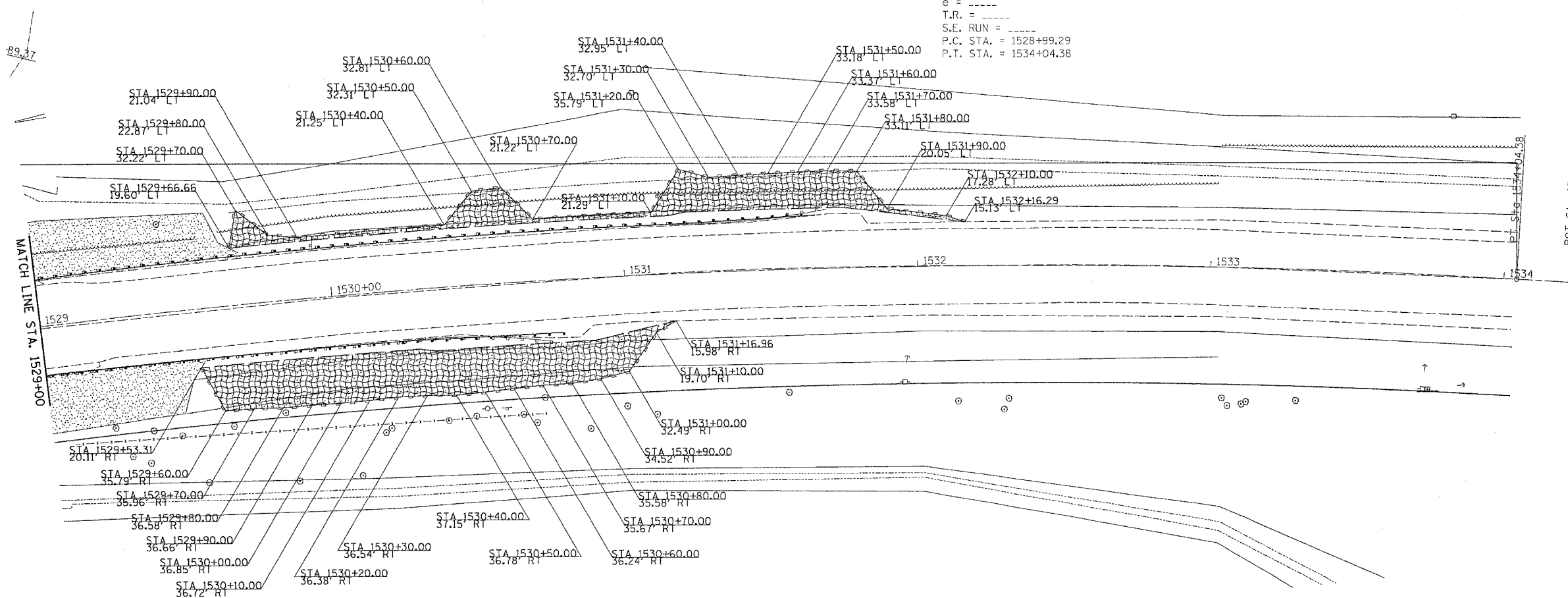
DRAWN BY _____
 CHECKED BY _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGL	40	15
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

EROSION CONTROL DETAILS



P.C.A. = 1531+52.51
 Δ = 10° 14' 39" (RT)
 D = 2° 01' 41"
 R = 2,824.97'
 T = 253.22'
 L = 505.09'
 E = 11.33'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 1528+99.29
 P.T. STA. = 1534+04.38



LEGEND

- = SEEDING
- = EROSION CONTROL BLANKET
- = PERIMETER EROSION BARRIER

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

VERT. SCALE: HORIZ. DATE

DRAWN BY CHECKED BY

EROSION CONTROL DETAILS

PLOT DATE = Fri Sep 02 11:43:45 2005
 FILE NAME = c:\p\c\projects\119BR\1534\1534.dgn
 PLOT SCALE = 20.0000 / IN.
 USER NAME = megillj

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 1
S.B.L.	*	Ogle	40	16	11 SHEETS
F.A.L.					
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

* Section 119BR-3 Contract #64A47

GENERAL NOTES

If the contractor's procedure for existing beam removal or placement of new beams involves placement of cranes or other heavy equipment on new beams, a detailed procedure shall be submitted to the Engineer for approval. The procedure shall include calculations, prepared and sealed by an Illinois Licensed Structural engineer, verifying that the equipment and procedure used will not overstress the new beams. To distribute load to multiple beams and protect the concrete, in all cases a double layer mat of heavy timbers shall be used at all times under crane tracks or wheels and any other outriggers in the down position. If necessary, shims shall be used under the crane mat to ensure uniform contact with the underlying beams. If cranes or other heavy equipment will be placed on new beams prior to placement of the concrete wearing surface, it shall be done after the dowel rods are grouted and cured for 24 hours minimum and prior to grouting the shear keys. A temporary means of lateral restraint will be required for fascia beams at expansion ends of beams to prevent movement of the beams.

Reinforcement bars shall conform to the requirements of AASHTO M-31, or M-322, Grade 60.

Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of work; however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Existing structure construction plans and subsequent repair plans are available from the IDOT District office by written request. The contractor shall be responsible for obtaining and reviewing the plans prior to construction.

The minimum thickness of the Concrete overlay shall be 5" and varies as required to adjust for the new profile grade and beam camber.

All construction joints shall be bonded.

No instream work will be allowed on this project.

The contractor is advised that the existing PPC Deck Beams are in a deteriorated condition with reduced load carrying capacity. It is the contractor's responsibility to account for the condition of the beams when developing construction procedures for removal and replacement of the superstructure.

The contractor is advised that due to the lack of vertical reinforcement in the back of the abutment walls a temporary bracing system must be provided prior to the removal of the existing PPC deck beams in order to ensure stability of the abutments. The details and calculations must be sealed by an Illinois licensed structural engineer and submitted to the Engineer for his approval. Cost included with Removal of Existing Superstructures.

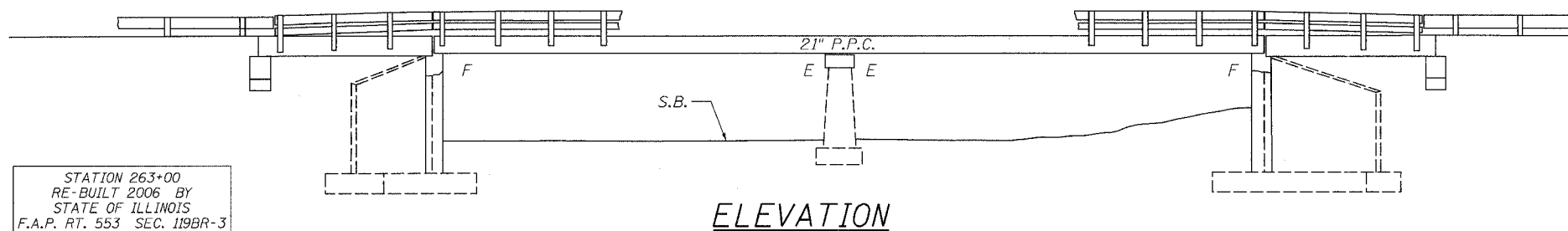
Existing name plate shall be cleaned and relocated adjacent to new name plate. Cost included with name plates.

Repair of pier caps shall be completed prior to placement of the new deck beams.

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Removal of Existing Superstructures	Each	1
Bridge Deck Grooving	Sq Yd	323
Concrete Wearing Surface	Sq Yd	333
Precast Prestressed Concrete Deck Beams (21" Depth)	Sq. Ft.	3155
Reinforcement Bars, Epoxy Coated	Pound	4560
Steel Bridge Rail, Type SM	Foot	273
Formed Concrete Repair (Depth Equal to or Less Than 5 Inches)	Sq. Ft.	10
Formed Concrete Repair (Depth Greater Than 5 Inches)	Sq. Ft.	6
Name Plates	Each	1
Bar Splicers	Each	104
Bridge Joint System Expansion 1 5/8"	Foot	36
Protective coat	Sq. Yd.	399

GENERAL PLAN AND ELEVATION
F.A.P. 553 (IL 72)
OVER STILLMAN CREEK
SECTION 119BR-3
OGLE COUNTY
STR. NO. 071-0042

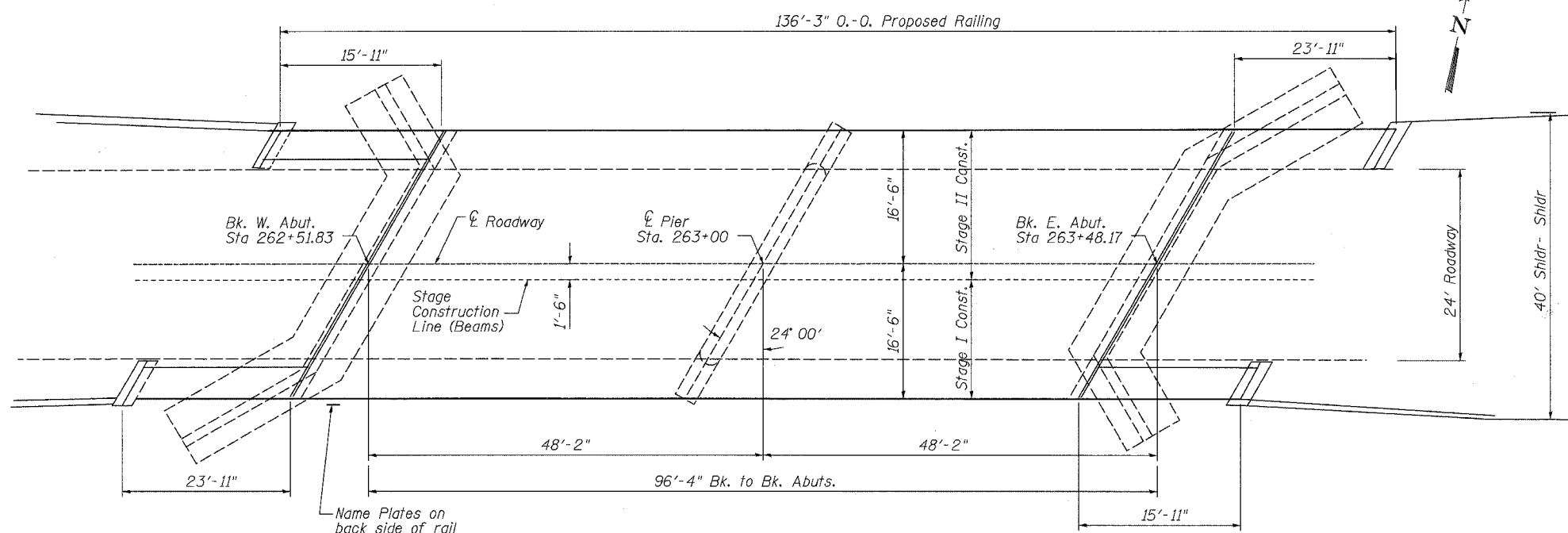


ELEVATION

STATION 263+00
RE-BUILT 2006 BY
STATE OF ILLINOIS
F.A.P. RT. 553 SEC. 119BR-3
LOADING HS20-44
STR. NO. 071-0042

LETTERING FOR NAME PLATE

SEE STD. 515001



PLAN

LOADING HS20-44 (New Construction)

No Allowance for Future Wearing Surface

DESIGN SPECIFICATIONS (New Construction)

2002 AASHTO

DESIGN STRESSES

FIELD UNITS

$f'_c = 3,500$ p.s.i.
 $f_y = 60,000$ p.s.i. (Reinforcement)

PRECAST PRESTRESSED UNITS

$f'_c = 5,000$ p.s.i.
 $f'_{ci} = 4,000$ p.s.i.
 $f'_s = 270,000$ p.s.i. (1/2" Low Lax Strands)
 $f'_{si} = 201,960$ p.s.i. (1/2" Low Lax Strands)

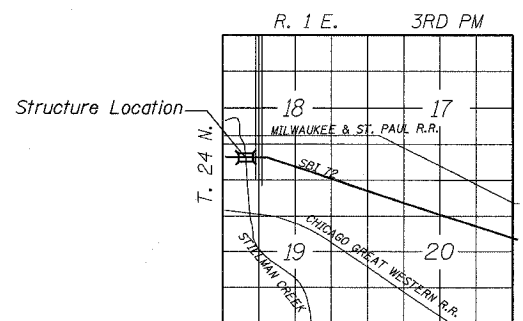
WATERWAY INFORMATION

(For Information Only)

Drainage Area 20,500 Acres

Character Rolling, hilly, sand, clay, wooded & cultivated

Existing Opening 574 Sq. Ft.
 $Q(50) = 2500$ cfs



LOCATION SKETCH

WENDLER ENGINEERING SERVICES, INC.

Illinois Professional Design
Firm No. 184-00848
Bridge plan sheets
1 thru 11 Only



Scott A. Brown 8/20/05
DATE

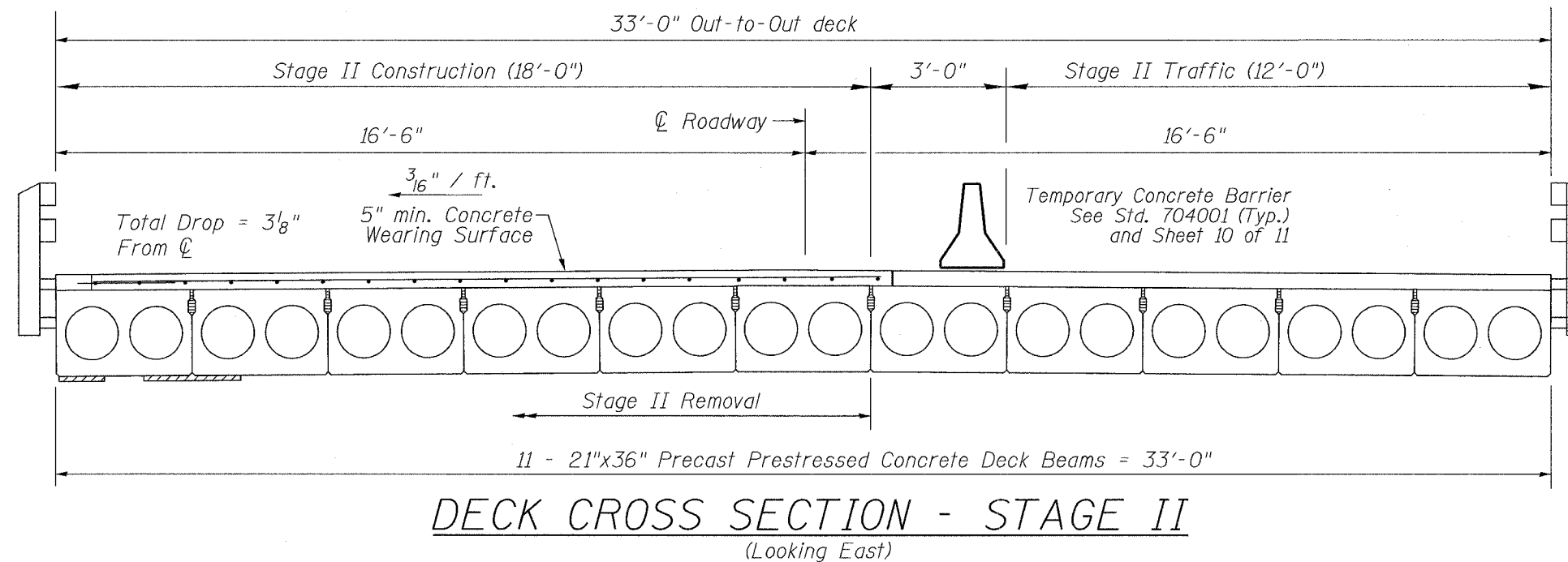
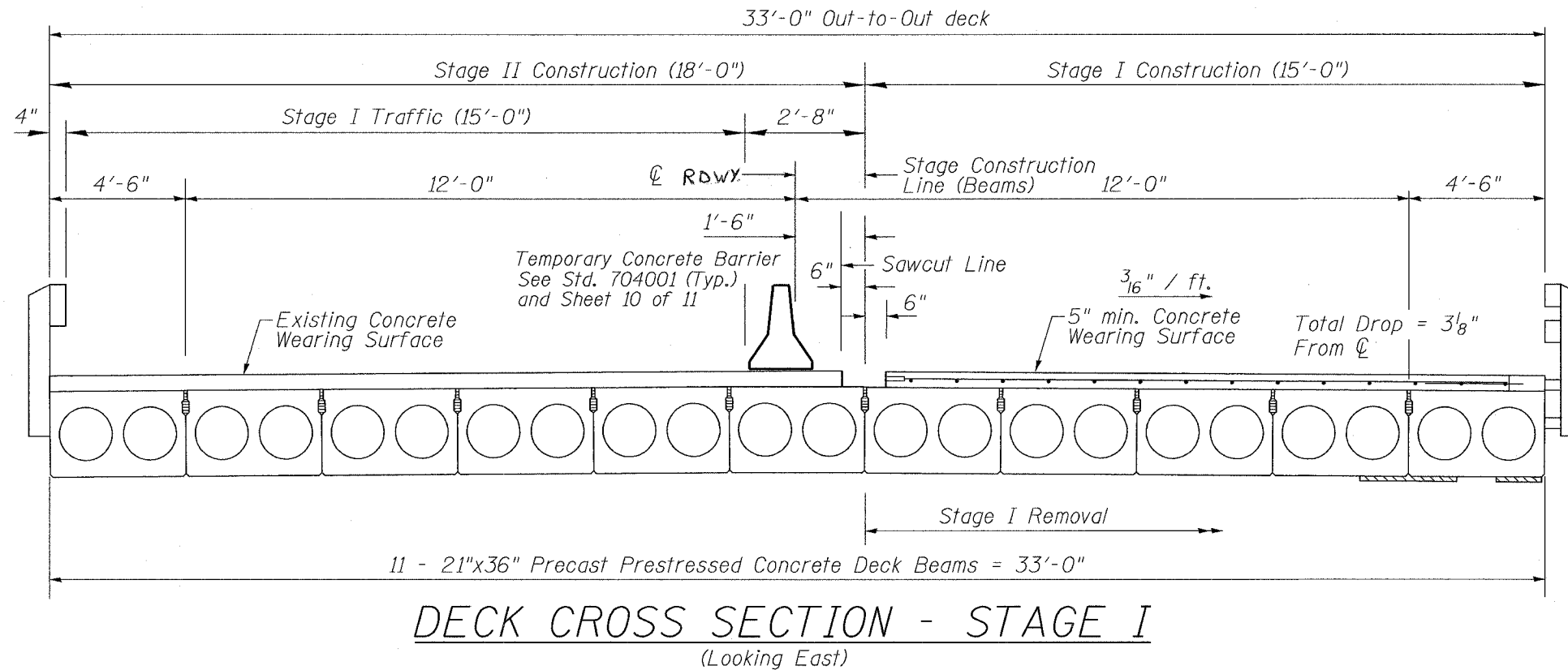
SCOTT A. BROWN
DIXON, ILLINOIS
ILLINOIS LICENSED STRUCTURAL
ENGINEER NO. 081-005981
EXPIRES 11-30-2006

DESIGNED	SB
CHECKED	RB
DRAWN	BH, BS
CHECKED	SB

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. <u>2</u>
F.A.P. 553	*	Ogle	40	17	<u>11</u> SHEETS
FED. ROAD DIST. NO. 7	ALIGNMENT	FED. AID PROJECT			

* Section 119BR-3 Contract #64A47



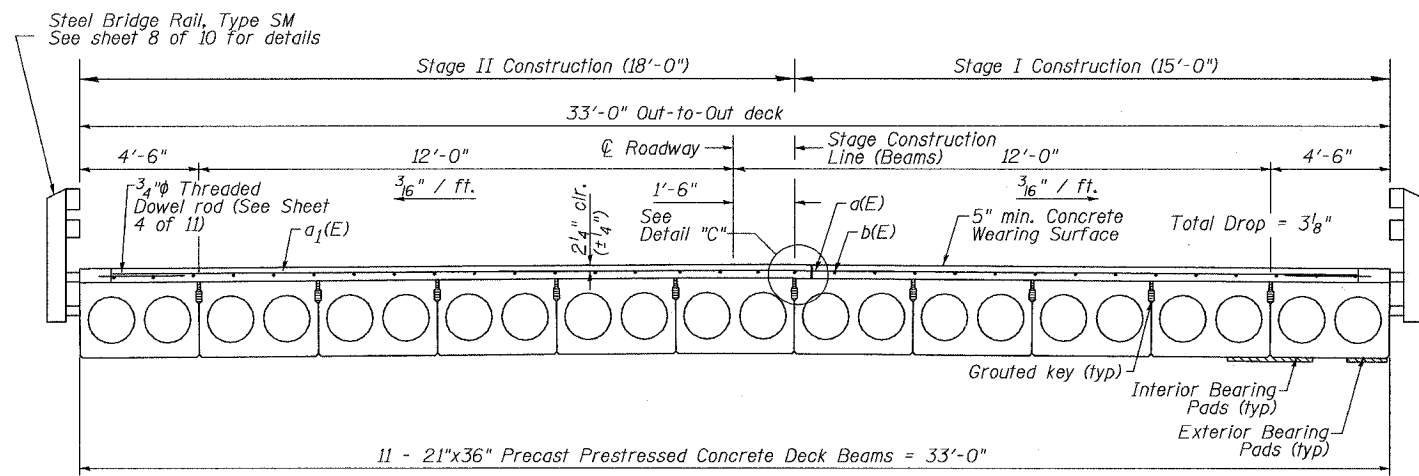
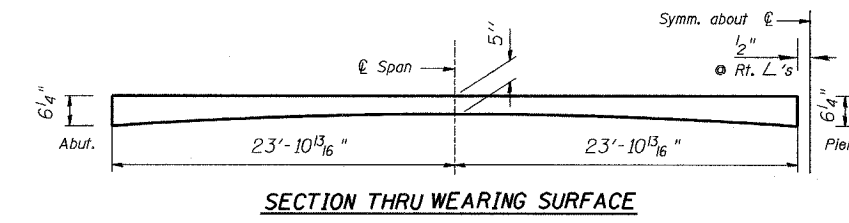
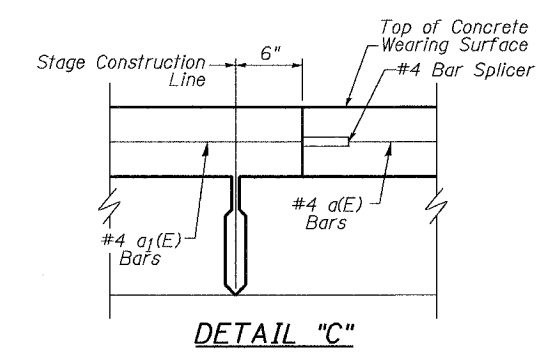
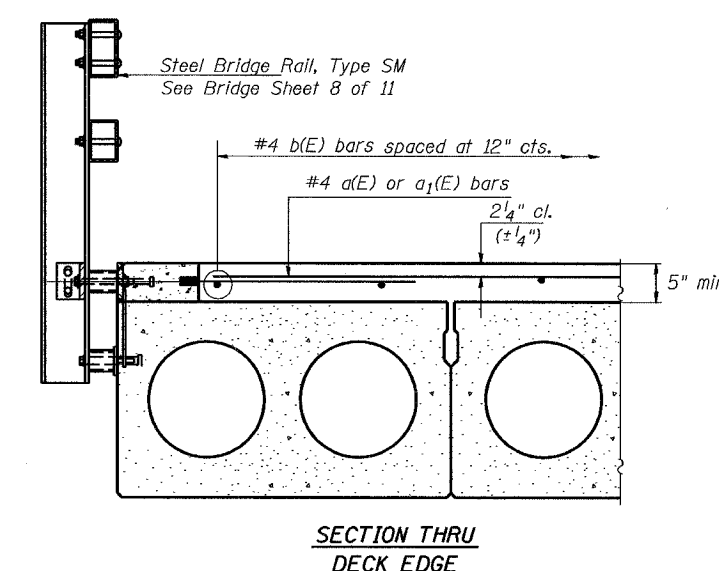
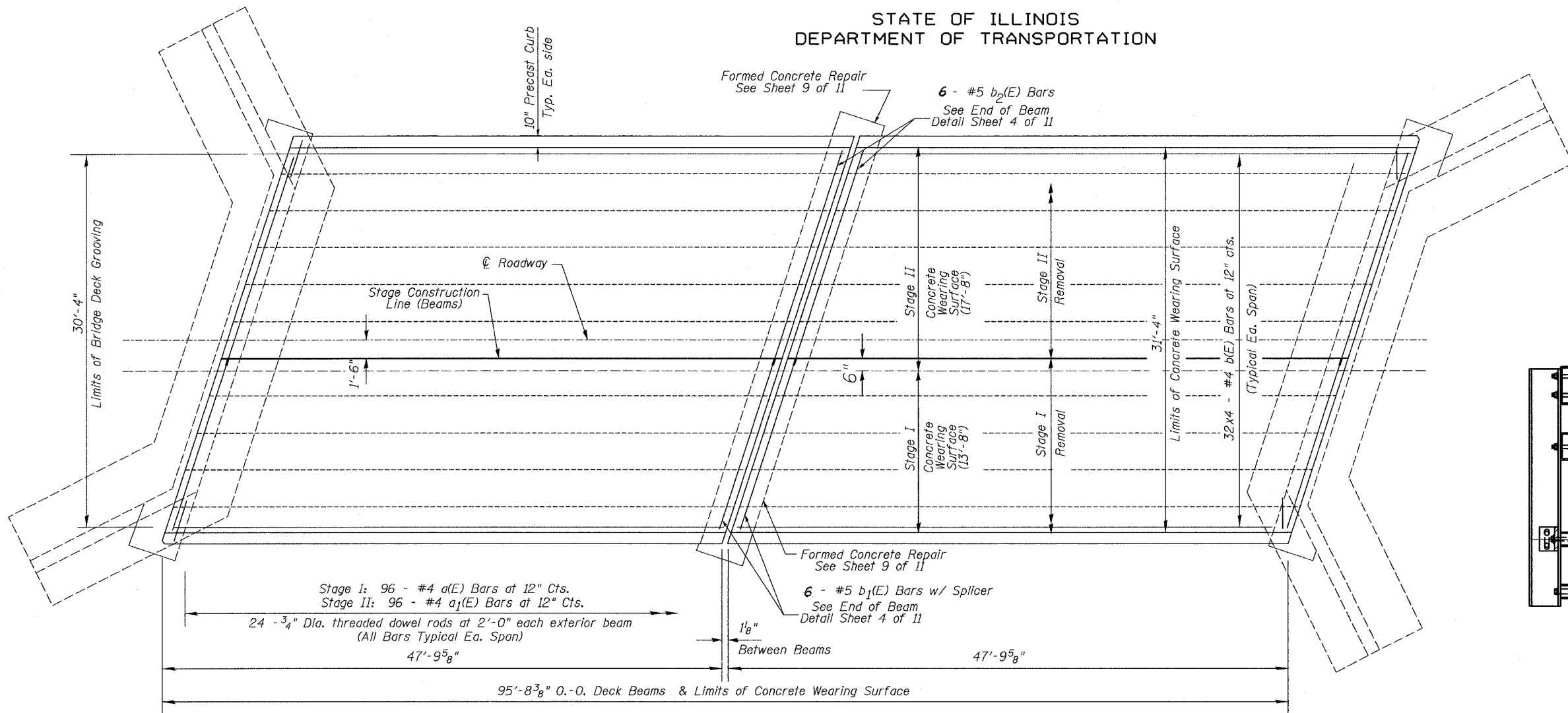
DESIGNED	SB
CHECKED	RB
DRAWN	BH, BS
CHECKED	SB

STAGING PLAN
F.A.P. 553 (IL 72)
OVER STILLMAN CREEK
SECTION 119BR-3
OGLE COUNTY
STR. NO. 071-0042

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 3
S.B.L.	*	Ogle	40	18	11 SHEETS
F.A.P.					
FED. ROAD DIST. NO. 7	ILL. INDS.	FED. AID PROJECT-			

* Section 119BR-3 Contract #64A47



BILL OF MATERIAL - SUPERSTRUCTURE

Bar	Size	Number	Length	Shape
a(E)	#4	96	14'-7"	---
a ₁ (E)	#4	96	19'-8"	---
b(E)	#4	128	25'-0"	---
b ₁ (E)	#5	6	15'-5"	---
b ₂ (E)	#5	6	20'-6"	---
ITEM		UNIT	TOTAL	
Removal of Existing Superstructures		Each	1	
Bridge Deck Grooving		Sq Yd	323	
Concrete Wearing Surface		Sq Yd	333	
* Bar Splacers		Each	102	
Reinforcement Bars (Epoxy Coated)		Pound	4560	

DECK OVERLAY DETAILS
F.A.P. 553 (IL 72)
OVER STILLMAN CREEK
SECTION 119BR-3
OGLE COUNTY
STR. NO. 071-0042

DESIGNED	SB
CHECKED	RB
DRAWN	DB, BS
CHECKED	SB

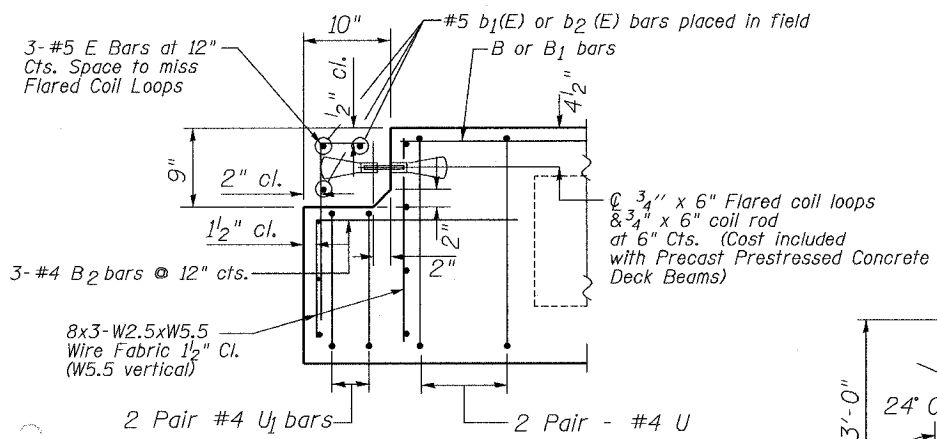
MIN. BAR LAP
#4 Bar = 1'-4"

Reinforcement bars designated (E) shall be epoxy coated.
Bars indicated thus 32x4 - #4 etc. indicates 32 lines of bars with 4 lengths per line.
*See Bar Splicer Assembly Details Br. Sheet 11 of 11.

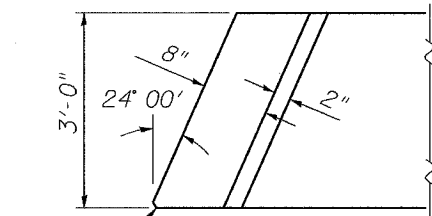
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 4
S.B.I.	*	Ogle	40	19	11 SHEETS
F.A.L.					
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

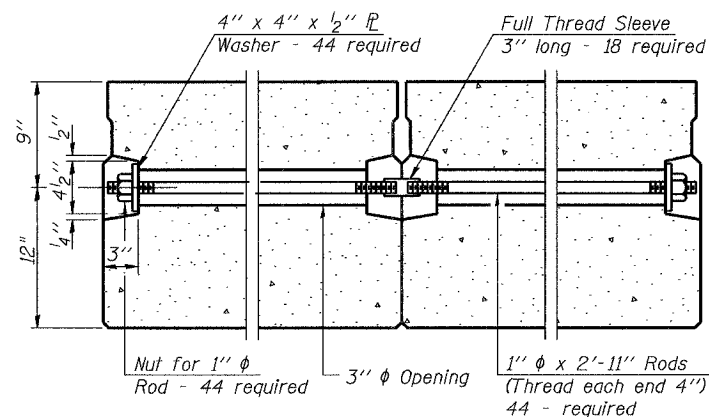
* Section 119BR-3 Contract #64A47



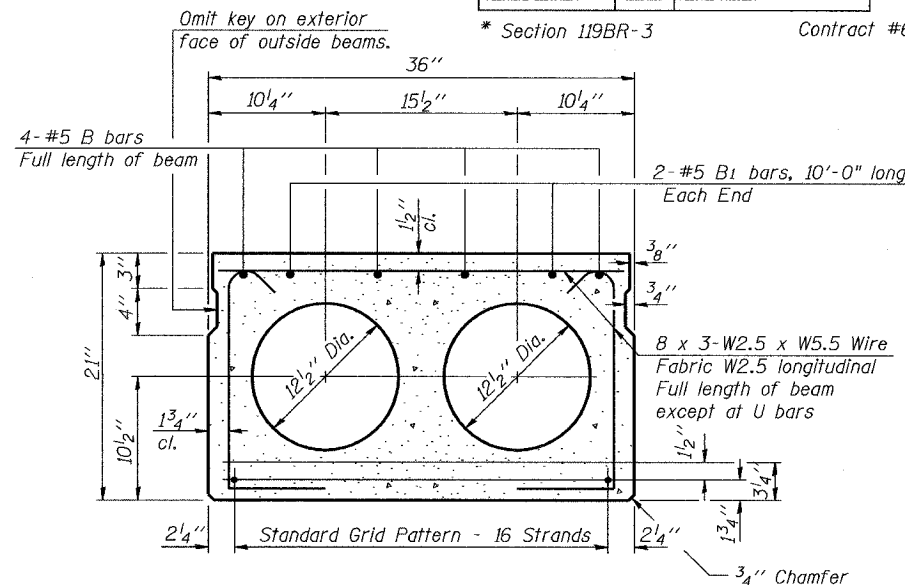
END OF BEAM AT EXPANSION JOINT



END OF BEAM AT EXPANSION JOINT PLAN - PIER ENDS



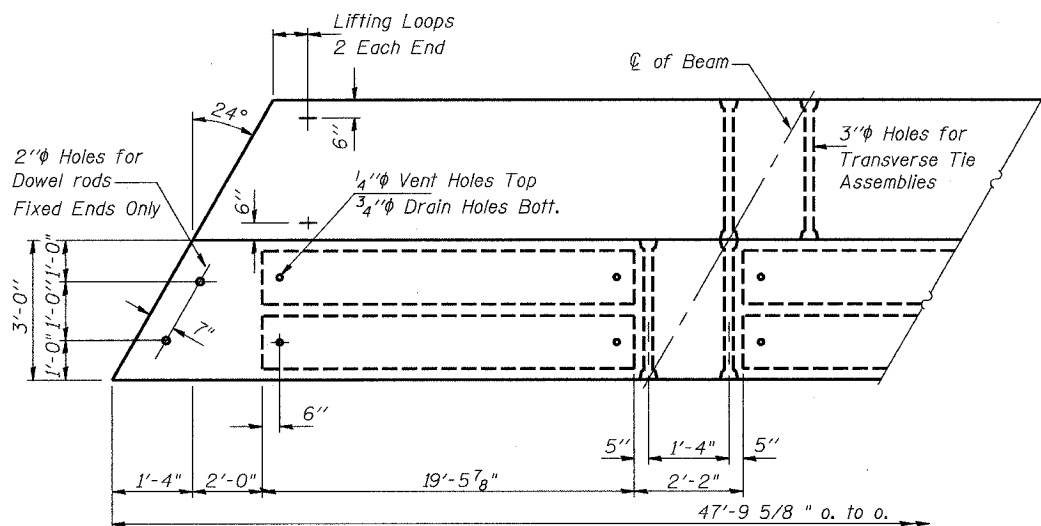
TYPICAL TRANSVERSE TIE ASSEMBLY



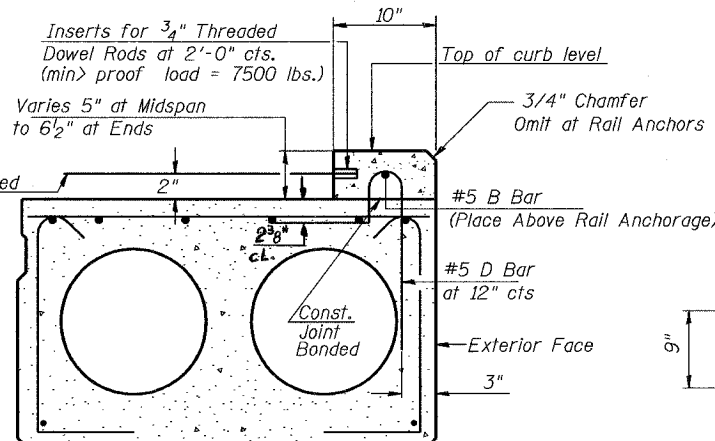
TYPICAL SECTION

16-1/2" ϕ Strands, Each Strand Stressed to 30,900 Lbs.
6-Strands 1 3/4" up, 8-Strands 3/4" up, 2-Strands 9" up

Note:
Place strands symmetrically about ϕ of beam.

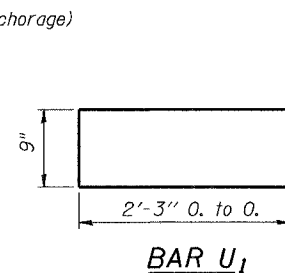


PLAN

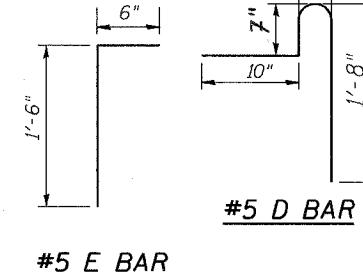


SECTION THRU EXTERIOR BEAMS

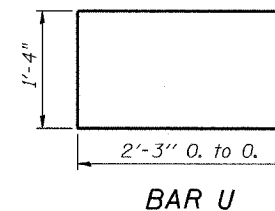
See Section Thru Interior Beams for strand pattern.



BAR U1



#5 E BAR



BAR U

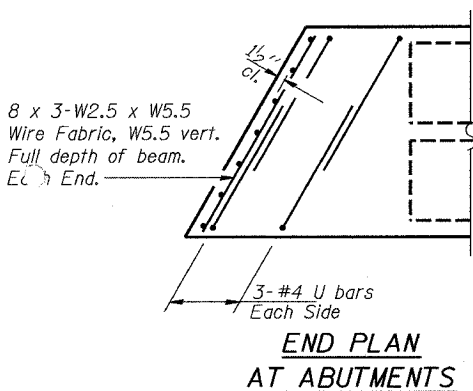
BILL OF MATERIAL

Bar	No./Bm	Size	Length	Shape	
B	4	#5	47'-6"	—	
B1	1	#5	47'-6"	—	
B2	4	#5	10'-0"	—	
D	48	#5	3'-7"	-1	
E	3	#5	2'-0"	-1	
U	10	#4	5'-10"	□	
U1	4	#4	5'-3"	□	
Precast Prestressed Conc. Deck Bms. (21" Depth)				Sq. Ft.	3155

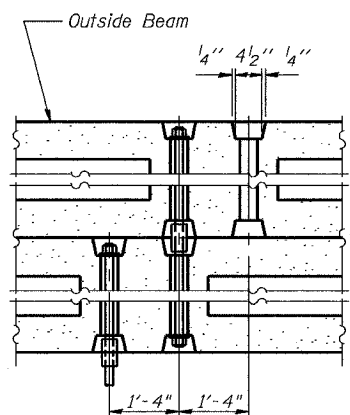
* At exterior beams only

NOTES

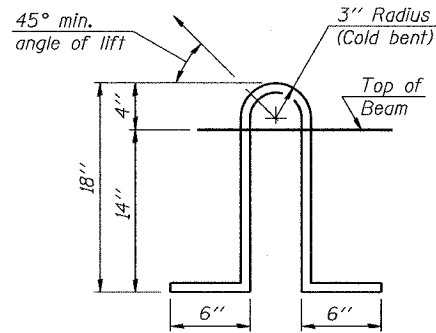
Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.153 sq. in. Lifting loops shall be 2-1/2" Dia. 270 ksi strands, as shown. The 1" Dia. rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets that receive transverse tie bar on outside shall be filled with grout after transverse tie assembly is in place. Non prestressing steel shall conform to AASHTO M-31, or M-322 Grade 60. The bearing seat surfaces shall be adjusted by shimming to assure firm and even bearing. Two 1/8" fabric adjusting shims of the dimensions of the Exterior Bearing Pad shall be provided for each bearing. Keyway surfaces shall be cleaned to remove form oil or other bond breaking material prior to shipment of the beams. Cleaning shall be done by sandblasting the keyway areas between top of the beam and the bottom edge of the key. Corrosion inhibitor, as covered in the Special Provisions, shall be used in the concrete for precast prestressed concrete deck beams. Required Release Strength, f'ci, shall be 4000 p.s.i. Bridge rail inserts shall be cast into the precast beams and curbs. Curbs shall be cast by the precast prestressed concrete fabricator after the strands have been released and prior to shipping the beams. The concrete used in the curb shall meet the requirements specified for the beams. The curbs including all concrete and reinforcement will not be paid for separately but included with Precast Prestressed Concrete Deck Beams. The curb inserts and threaded dowel rods may be either epoxy coated or galvanized and the cost shall be considered included with Precast Prestressed Concrete Deck Beams (21" Depth). The cut strands at each beam end shall be given two coats of zinc dust spray or paint meeting the requirements of ASTM A 780. The zinc dust spray or paint shall be applied before corrosion appears and allowed to dry according to the manufacturer's specifications prior to another coat of zinc. A concrete sealer meeting the requirements of Section 587 of the Standard Specifications shall be applied to the exterior face and 9" in on the underside of the fascia beams. The sealer shall be applied after visible crack growth has subsided. This work shall be performed by the producer and included with the cost of the beam.



END PLAN AT ABUTMENTS



TIE ROD OFFSET DETAIL



LIFTING LOOP DETAIL

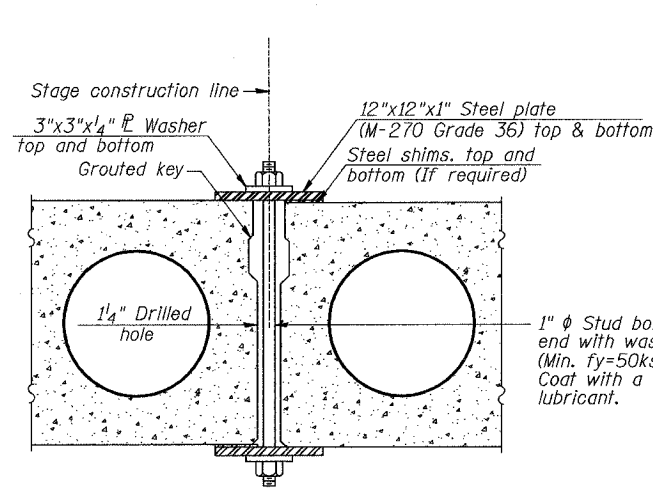
DESIGNED	SB
CHECKED	RB
DRAWN	DB, BS
CHECKED	SB

DECK BEAM DETAILS
F.A.P. 553 (IL 72)
OVER STILLMAN CREEK
SECTION 119BR-3
OGLE COUNTY
STR. NO. 071-0042

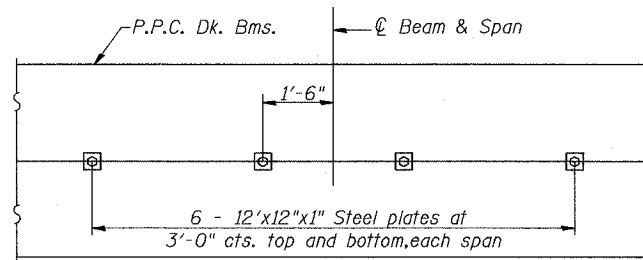
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 5
S.B.I.	*	Ogle	40	20	11 SHEETS
F.A.L.					
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

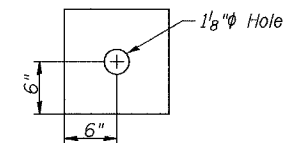
* Section 119BR-3 Contract #64A47



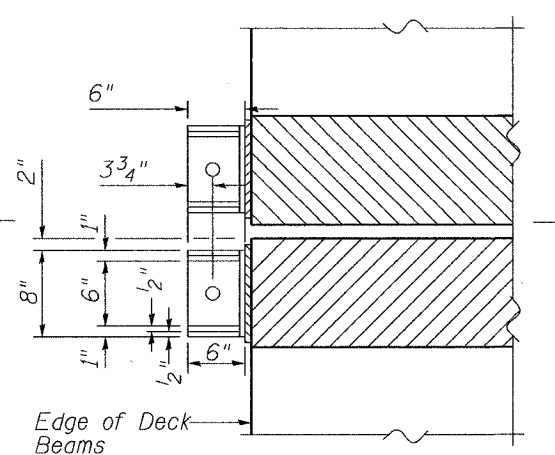
SECTION AT STAGE CONSTRUCTION LINE



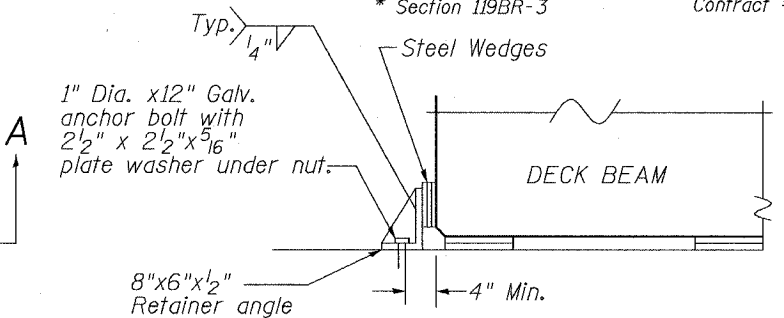
PLAN



CLAMPING PLATE

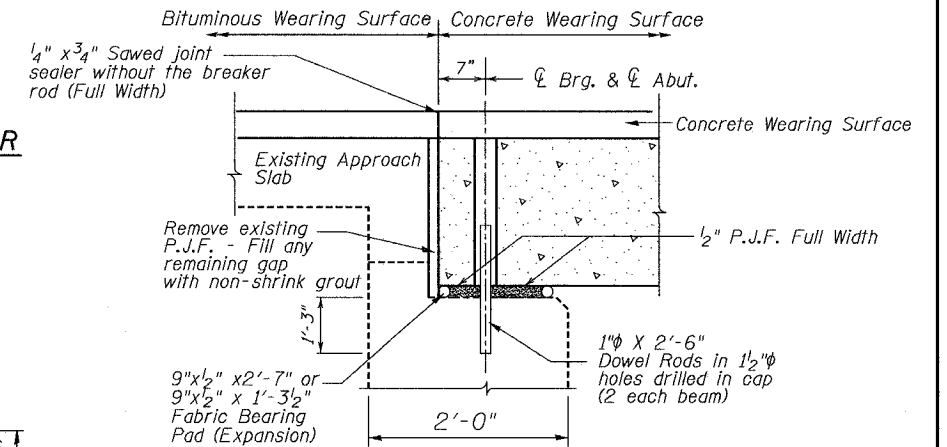


PLAN @ PIER TEMPORARY SIDE RETAINER DETAIL
(4 Required)

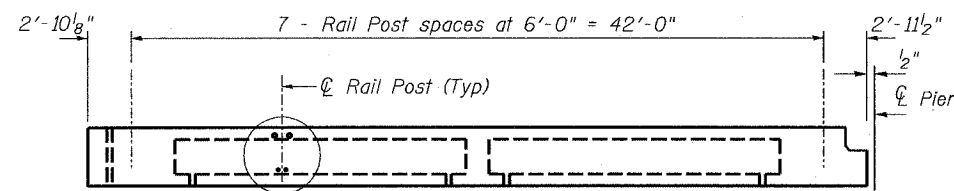


SECTION A-A

Note: After block-outs are poured and cured the retainer angles shall be removed. Anchor bolts may be left in place.

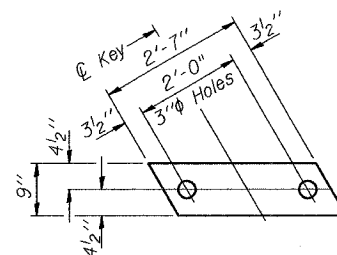


SECTION THRU ABUTMENTS
(Dimension at Rt. L's)

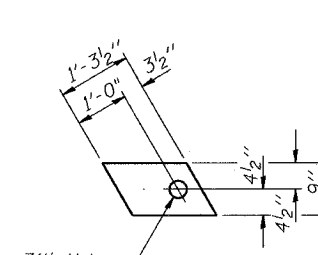


OUTSIDE BEAM ELEVATION

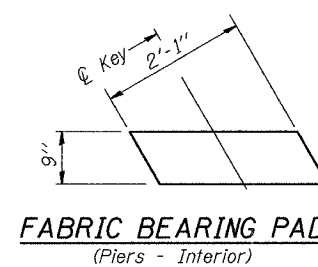
See Rail Post Insert Details on Bridge Sheet 8 of 11



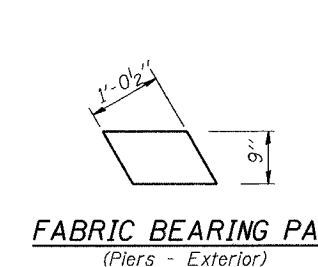
FABRIC BEARING PAD
(Abutments - Interior)



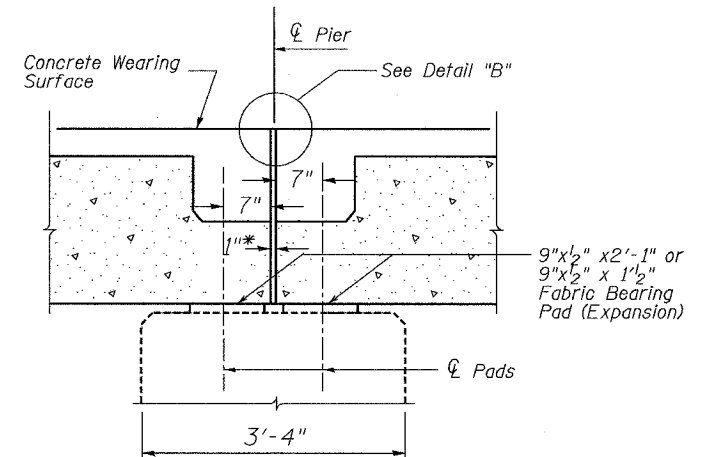
FABRIC BEARING PAD
(Abutments - Exterior)
(Abutments - Stage Line)



FABRIC BEARING PAD
(Piers - Interior)

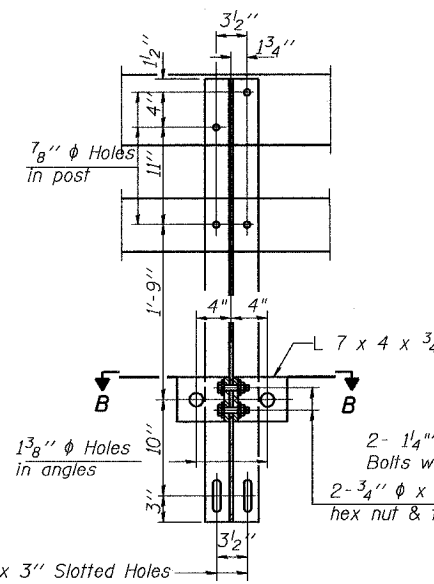


FABRIC BEARING PAD
(Piers - Exterior)
(Piers - Stage Line)



SECTION THRU PIER
(Dimension at Rt. L's)

* 1" Jt. shall be filled with non-shrink grout. 1" dimension may vary to accommodate tolerance in beams lengths.



SECTION A-A

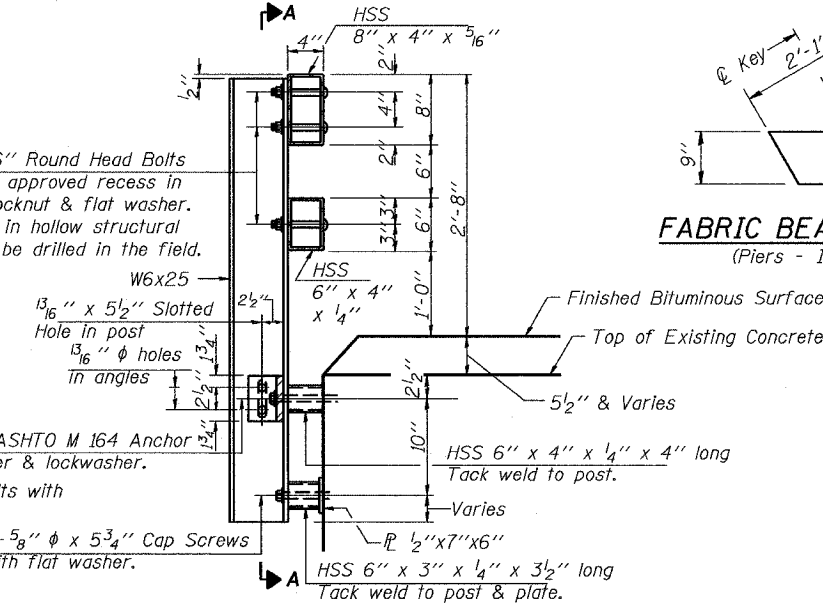
4-3/4" φ x 6" Round Head Bolts (With slot or approved recess in head) with locknut & flat washer. 7/8" φ Holes in hollow structural section may be drilled in the field.

2-1 1/4" φ x 7 3/4" AASHTO M 164 Anchor Bolts with flat washer & lockwasher.
2-3/4" φ x 3 1/4" H.S. Bolts with hex nut & flat washers

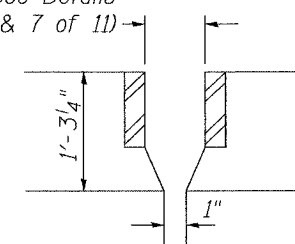
2-5/8" φ x 5 3/4" Cap Screws with flat washer.

SECTION THRU EDGE - APPROACH SPAN

See Bridge Sheet 9 of 11 for Existing Anchor Details



Bridge Joint System Exp. 1 1/8" (See Details Br. Sheet 6 & 7 of 11)



DETAIL "B"
(Dimension at Rt. L's)

DESIGNED	SB
CHECKED	RB
DRAWN	DB, BS
CHECKED	SB

DECK BEAM DETAILS
F.A.P. 553 (IL 72)
OVER STILLMAN CREEK
SECTION 119BR-3
OGLE COUNTY
STR. NO. 071-0042

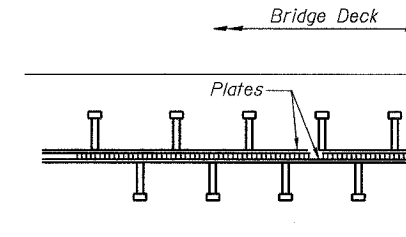
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	STREET	SHEET	SHEET NO.
-	*	Ogle	40	21	6
F.A.P. NO. 553 (IL 72)					II SHEETS
FED. ROAD DIST. NO. 7		ILL. ROAD PROJECT		FED. ROAD PROJECT	

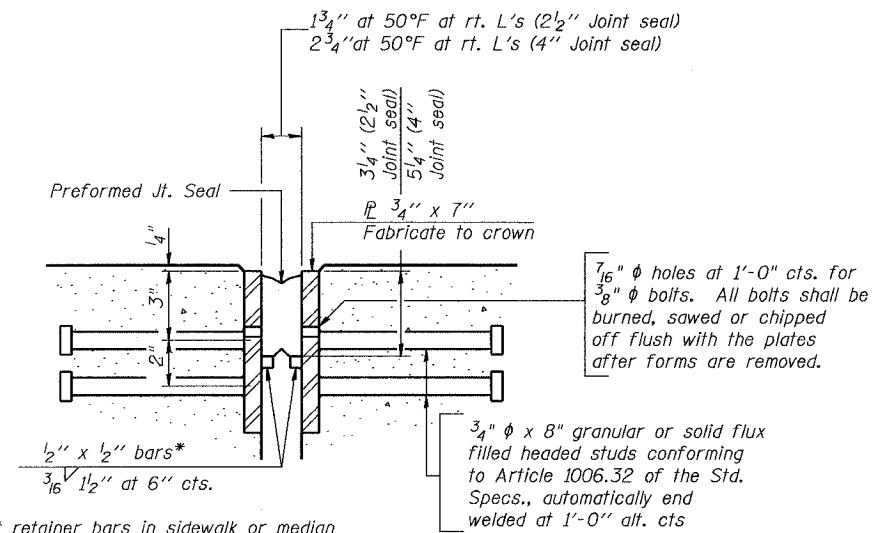
* Section 119BR-3 Contract #64A47

GENERAL NOTES

Furnish steel plates in segments of 20 feet maximum length. Maximum space between installed segments shall be $\frac{3}{16}$ ". Seal space with silicone sealant suitable for structural steel.



PLAN VIEW



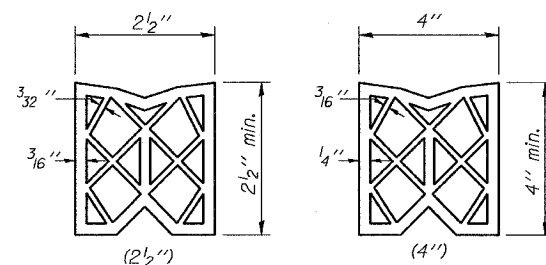
*Cut retainer bars in sidewalk or median 6" short of the sidewalk or median face.

SECTION THRU EXPANSION JOINT
(2 $\frac{1}{2}$ " and 4" joint seals)

Bridge Joint System (Expansion)		
Design Movement	Required Preformed Joint Seal Size	Required Strip Seal Rated movement
1"	2 $\frac{1}{2}$ "	1"
1 $\frac{5}{8}$ "	4"	2"

BILL OF MATERIAL

Item	Unit	Total
Bridge Joint System Expansion 1 $\frac{5}{8}$ "	Foot	36



PREFORMED JOINT SEAL

DESIGNED	SB
CHECKED	RB
DRAWN	BH, BS
CHECKED	SB

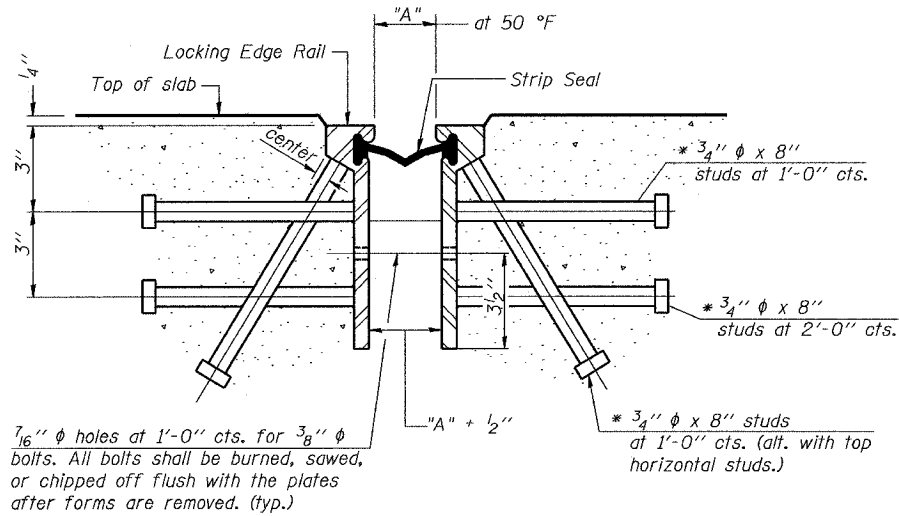
BRIDGE JOINT SYSTEM - EXPANSION
(PREFORMED JOINT SEAL)

F.A.P. 553 (IL 72)
OVER STILLMAN CREEK
SECTION 119BR-3
OGLE COUNTY
STR. NO. 071-0042

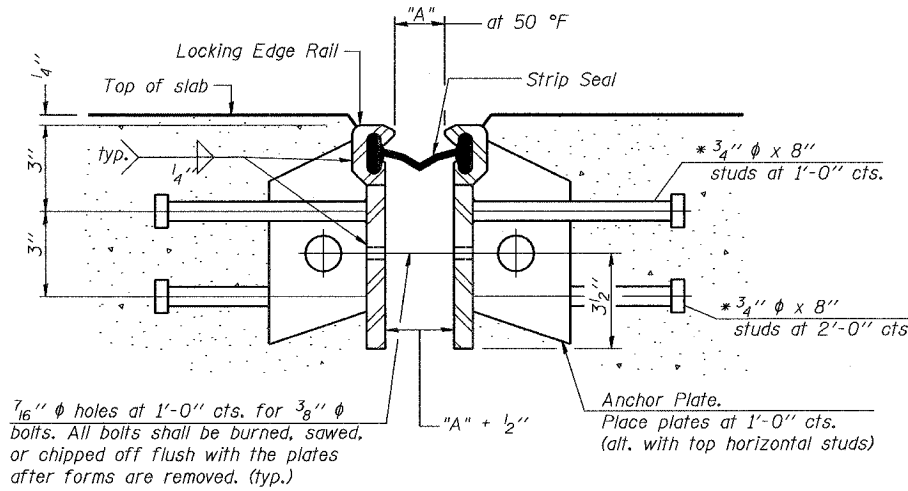
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEETS	SHEET NO.	SHEET NO.
	*	Ogle	40	22	11 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT		

* Section 119BR-3 Contract #64A47



Required Strip Seal rated movement	"A"
1"	1 1/8"
2"	1 3/4"



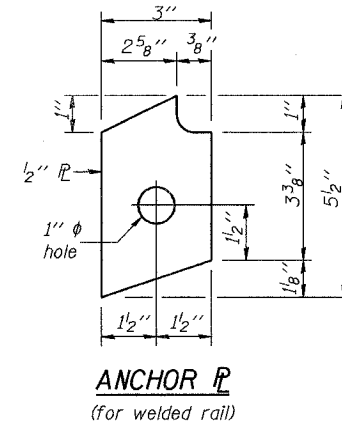
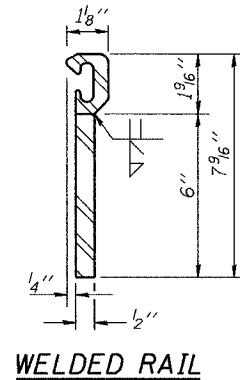
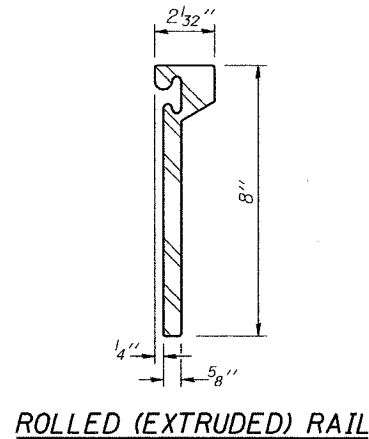
GENERAL NOTES

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails.
The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed.
Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.
The manufacturer's recommended installation methods shall be followed.
The joint opening and deck dimensions detailed on the superstructure are based on a preformed joint seal. If the contractor elects to use the alternate strip seal joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

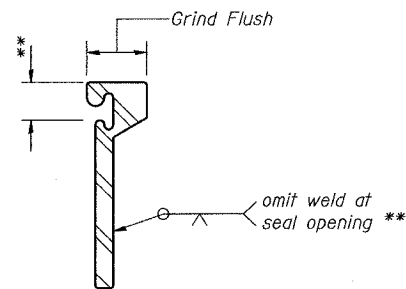
SECTION THRU ROLLED RAIL EXP. JOINT
(180 Studs Required)

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

SECTION THRU WELDED RAIL EXP. JOINT
(110 Studs Required)
(74 Anchor Plates Required)



LOCKING EDGE RAILS



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.

DESIGNED	SB
CHECKED	RB
DRAWN	BH, BS
CHECKED	SB

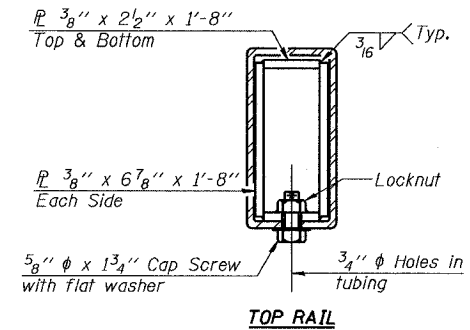
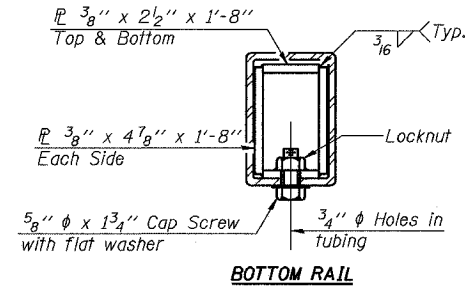
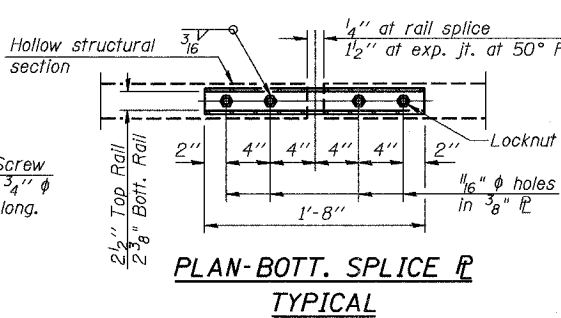
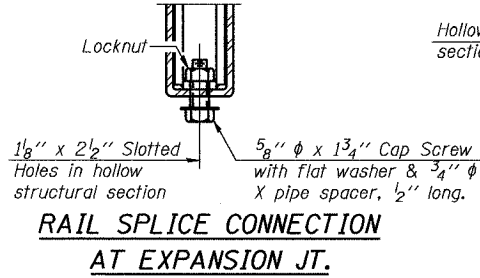
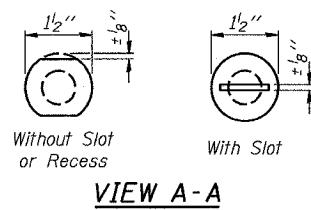
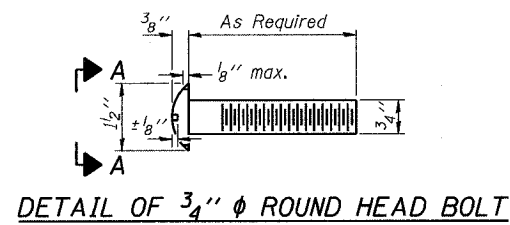
**BRIDGE JOINT SYSTEM - EXPANSION
(ALTERNATE-STRIP SEAL)**

F.A.P. 553 (IL 72)
OVER STILLMAN CREEK
SECTION 119BR-3
OGLE COUNTY
STR. NO. 071-0042

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

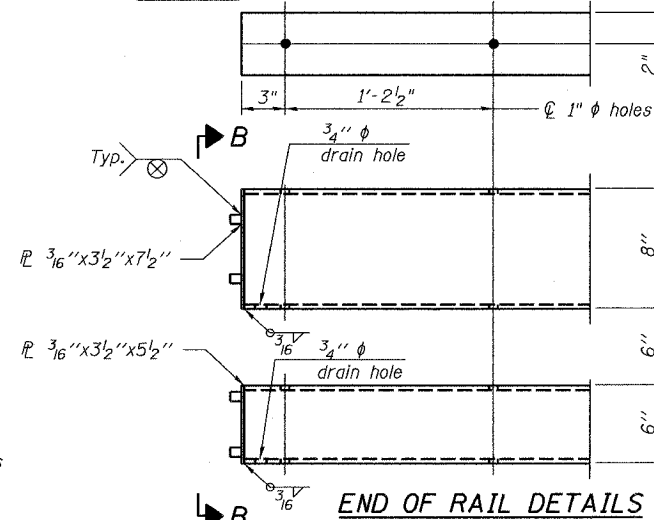
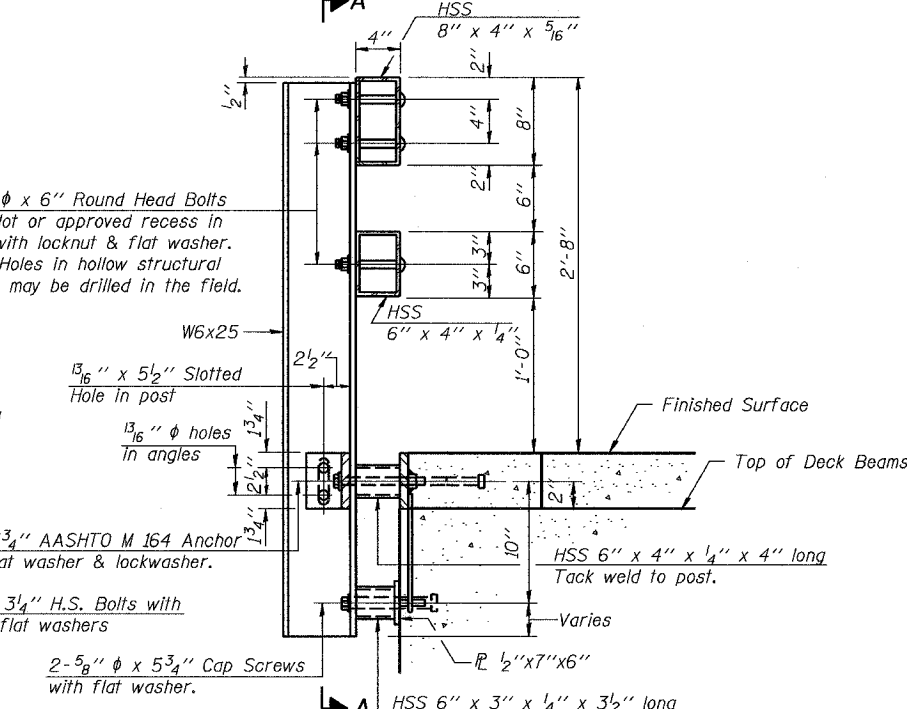
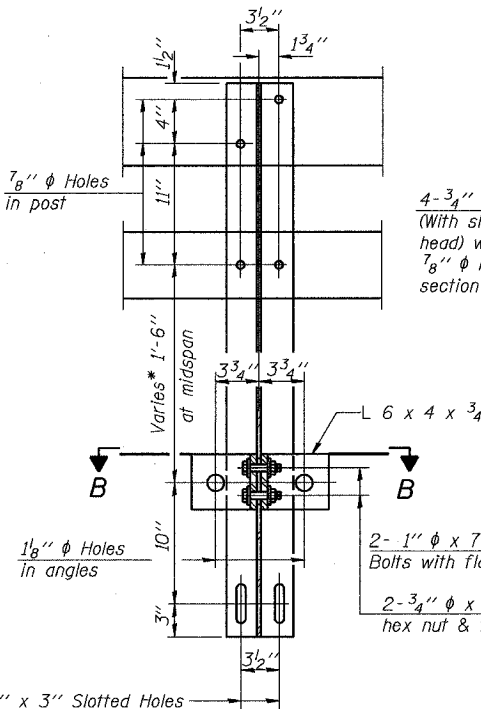
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. <u>8</u>
F.A.P.	*	Ogle	40	23	<u>11</u> SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

* Section 119BR-3 Contract #64A47



SECTIONS AT RAIL SPLICE

* Dimension is based on 5" Wearing Surface thickness at midspan.



NOTES

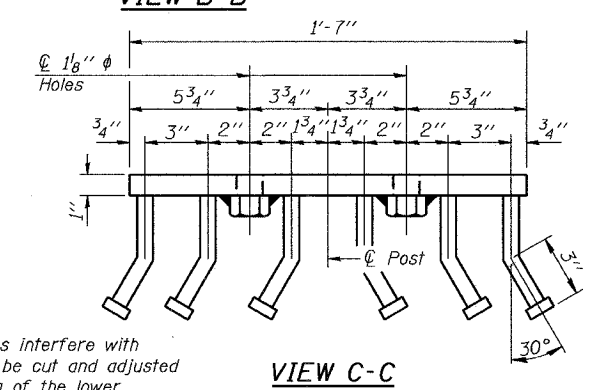
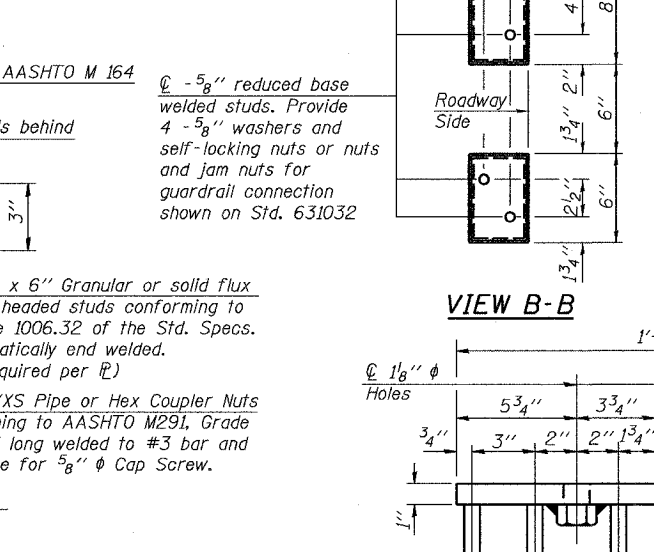
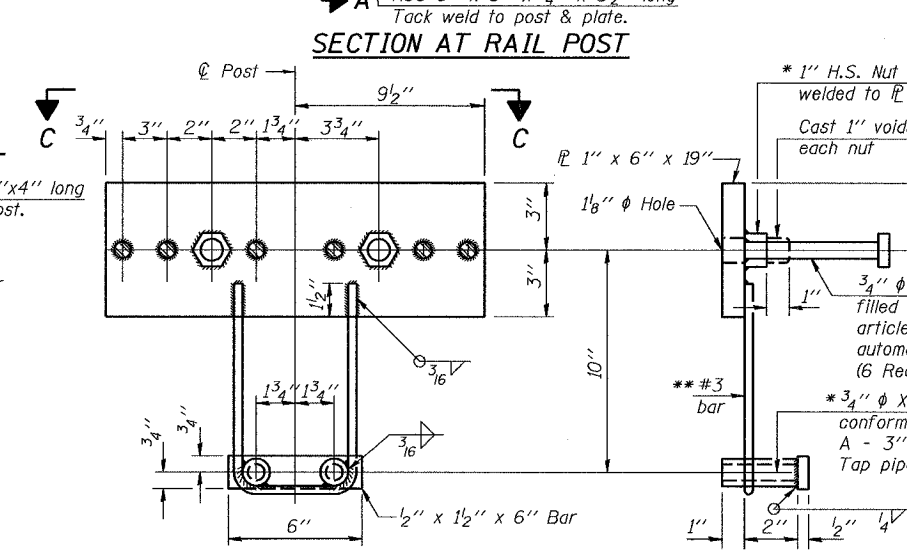
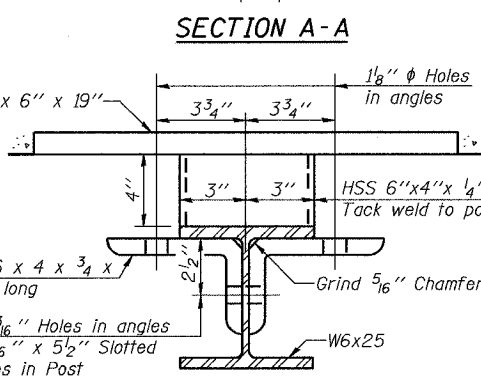
See bridge sheet 9 of 11 for railing connection to the existing approach slabs.
Hollow structural sections shall conform to the requirements of ASTM designation A 500 Grade B Structural Steel Tubing and shall meet the longitudinal CVN requirements of 15 ft-lbs at 0° F.
All other steel shapes and plates shall conform to the requirements of AASHTO M 270 Grade 36 except posts and angles shall conform to AASHTO M 270, Grade 50.
Bolts, cap screws, and nuts shall conform to the requirements of ASTM designation A 307 except for high strength bolts, nuts and washers noted which shall conform to AASHTO M 164.
All bolts, nuts, cap screws, washers and lock washers shall be galvanized according to AASHTO M 232.
All posts, railing, rail splices, anchor devices and angles shall be galvanized after shop fabrication according to AASHTO M 111 and ASTM A 385. Galvanized rail shall not be painted.
Railing shall be according to Section 509 of the Standard Specifications, except as noted, and will be paid for at the contract unit price per foot for Steel Bridge Rail, Type SM.
All field drilled holes shall be coated with an approved zinc rich paint before erection.
For multi-span bridges, sufficient 1/4" x 6" x 1'-2" galvanized steel shims shall be provided to align rail between adjacent spans. Cost included with Steel Bridge Rail, Type SM.
The 1/2" x 7" x 6" plates that come in contact with concrete shall receive two coats of asphalt paint conforming to Section 1060.07 Type II or place 1/8" fabric bearing pads between the plates and concrete.
The 3/4" high strength bolts used to connect the 6 x 4 x 3/4 angles to the post shall be tightened according to Article 505.04(f)(2) of the Standard Specifications. The 1" high strength bolts connecting the angles to the concrete shall be tightened to a snug fit and given an additional 1/8 turn. The 5/8" cap screws in bottom of posts shall be tightened to a snug fit only.
See sheet 5 of 11 for rail post spacing.

BILL OF MATERIAL

Item	Unit	Quantity
Steel Bridge Rail, Type SM	Foot	273

TYPE SM
STEEL BRIDGE RAIL SIDE MOUNTED

F.A.P. 553 (IL 72)
OVER STILLMAN CREEK
SECTION 119BR-3
OGLE COUNTY
STR. NO. 071-0042

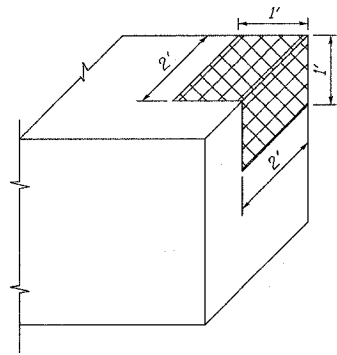


* Threaded areas shall be plugged or blocked off during casting of beam. Galvanized after fabrication.
** Whenever the lower insert assemblies interfere with strand locations, the #3 bars shall be cut and adjusted in order to allow raising or lowering of the lower inserts. Maximum adjustment not to exceed 1/2".

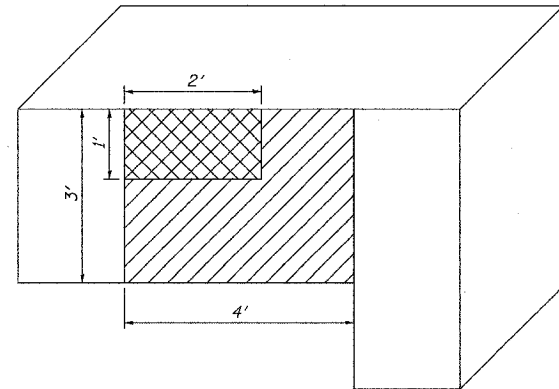
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 9
S.S.L.	*	Ogle	40	24	11 SHEETS
F.A.P.					
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

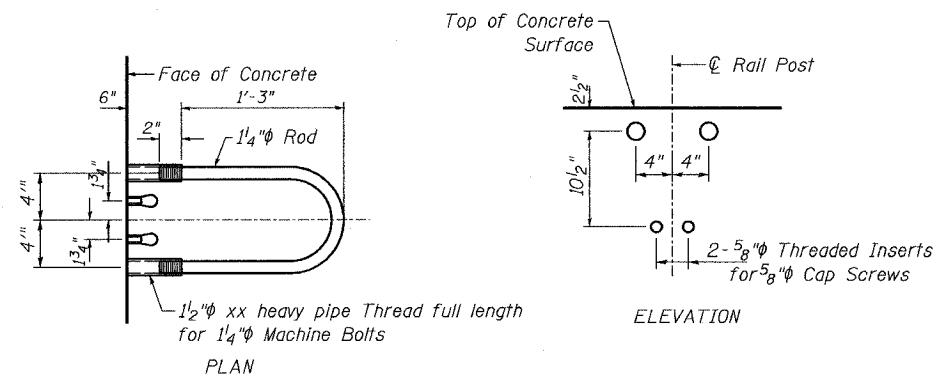
* Section 119BR-3 Contract #64A47



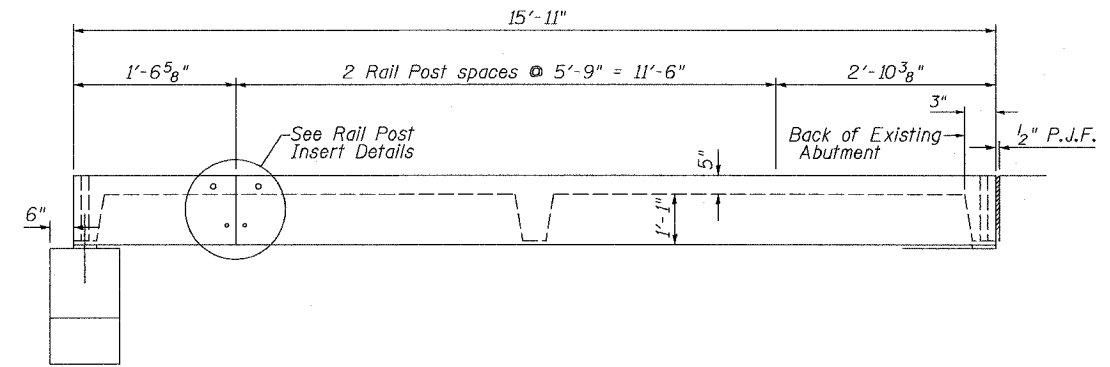
**PIER CAP
NORTH END**
(Looking East)



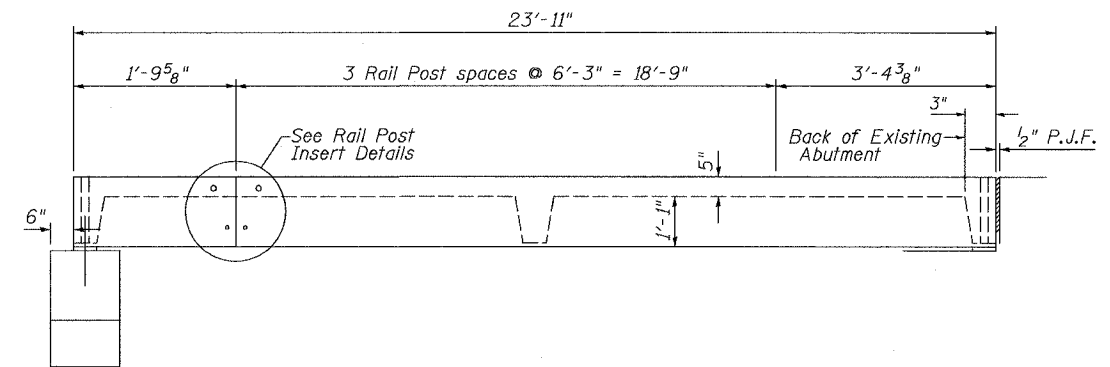
**PIER CAP
SOUTH END**
(Looking East)



**EXISTING APPROACH SLAB
RAIL POST INSERT DETAILS**
(For Information Only)



EXISTING APPROACH SLAB SIDE ELEVATION
(For Information Only)



EXISTING APPROACH SLAB SIDE ELEVATION
(For Information Only)

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Formed Concrete Repair (Depth Equal to or Less Than 5 Inches)	Sq. Ft.	10
Formed Concrete Repair (Depth Greater Than 5 Inches)	Sq. Ft.	6

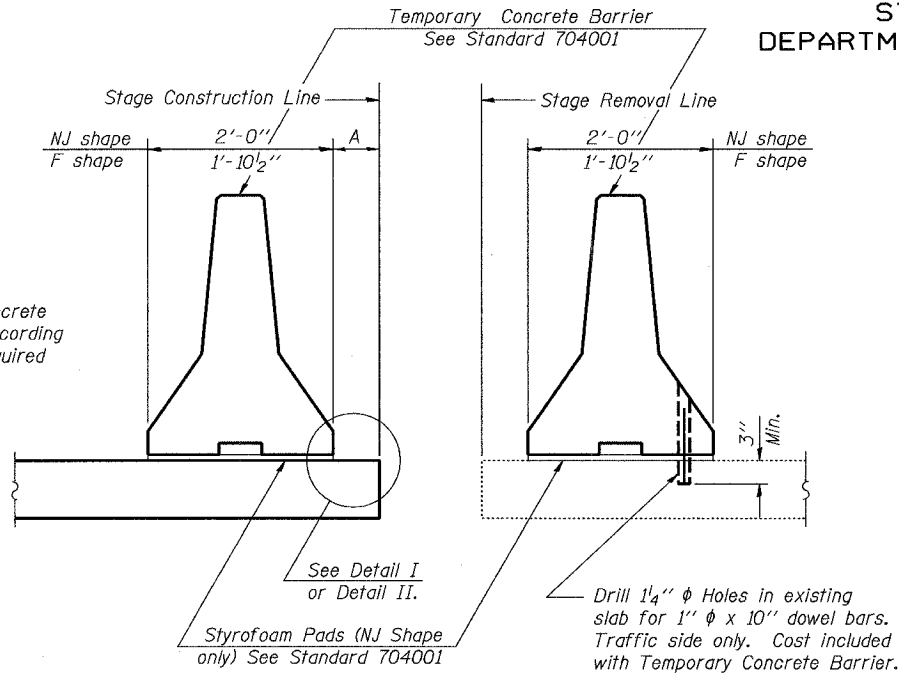
CONCRETE REPAIR DETAILS
F.A.P. 553 (IL 72)
OVER STILLMAN CREEK
SECTION 119BR-3
OGLE COUNTY
STR. NO. 071-0042

DESIGNED	SB
CHECKED	RB
DRAWN	BH, BS
CHECKED	SB

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET	SHEET NO. 10
S.B.S.	*	Ogle	40	25	11 SHEETS
F.A.P.					
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

* Section 119BR-3 Contract #64A47

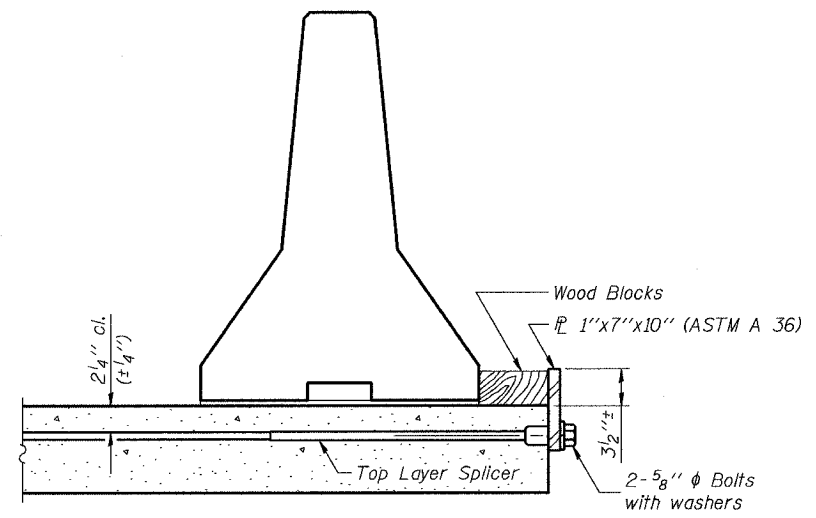


When "A" is 3'-6" or less, the temporary concrete barrier shall be Anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".

NEW SLAB

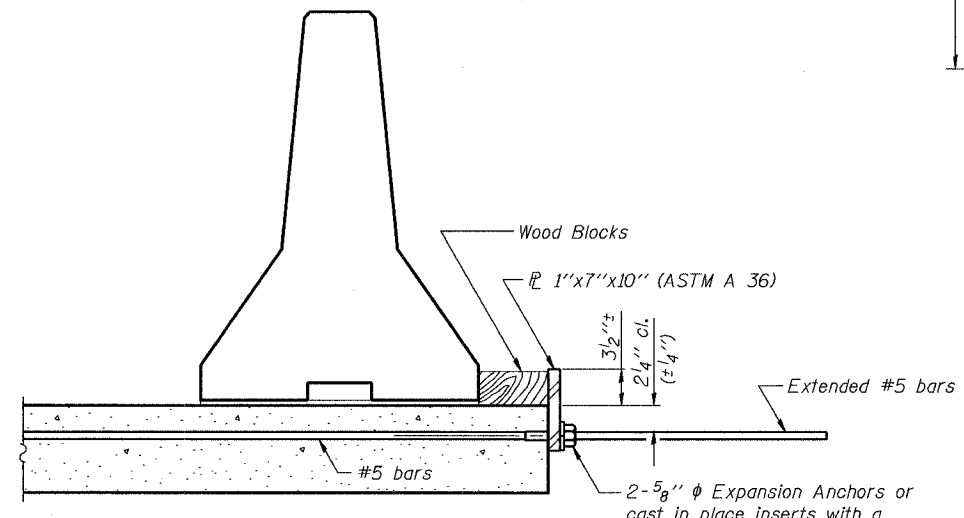
EXISTING SLAB

SECTIONS THRU SLAB



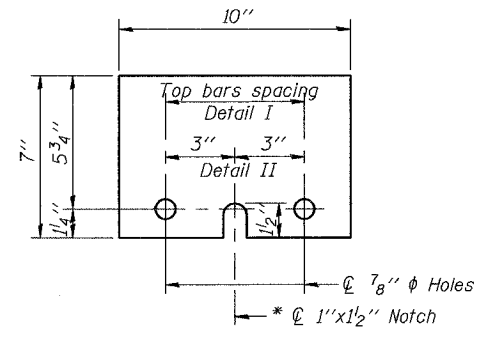
DETAIL I

The 1"x7"x10" Plate shall not be removed until Stage II Construction forms and reinforcement bars are in place.



DETAIL II

The 1"x7"x10" Plate shall not be removed until Stage II Construction forms and all reinforcement bars are in place and the concrete is ready to be placed.



1" x 7" x 10"

* Required only with Detail II

- NOTES**
- Detail I - With Bar Splicer or Couplers:
Connect one (1) 1"x7"x10" steel \bar{L} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.
 - Detail II - With Extended Reinforcement Bars:
Connect one (1) 1"x7"x10" steel \bar{L} to the concrete slab with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.
- Cost of anchorage is included with Temporary Concrete Barrier.

DESIGNED
CHECKED
DRAWN
CHECKED

R-27 10-31-02

TEMPORARY CONCRETE BARRIER
FOR STAGE CONSTRUCTION
F.A.P. 553 (IL 72)
OVER STILLMAN CREEK
SECTION 119BR-3
OGLE COUNTY
STR. NO. 071-0042

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. <u>11</u>
S.B.L.	*	Ogle	40	26	<u>11</u> SHEETS
F.A.P.					
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

* Section 119BR-3 Contract #64A47

NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

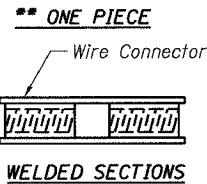
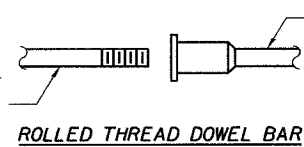
- ① Minimum Capacity = $1.25 \times f_y \times A_t$
(Tension in kips)
- ② Minimum *Pull-out Strength = $1.25 \times f_{s_{allow}} \times A_t$
(Tension in kips)

Where f_y = Yield strength of lapped reinforcement bars in ksi.
 $f_{s_{allow}}$ = Allowable tensile stress in lapped reinforcement bars in ksi (Service Load)
 A_t = Tensile stress area of lapped reinforcement bars.
* = 28 day concrete

Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	5.9
#5	2'-0"	23.0	9.2
#6	2'-7"	33.1	13.3
#7	3'-5"	45.1	18.0
#8	4'-6"	58.9	23.6
#9	5'-9"	75.0	30.0
#10	7'-3"	95.0	38.0
#11	9'-0"	117.4	46.8

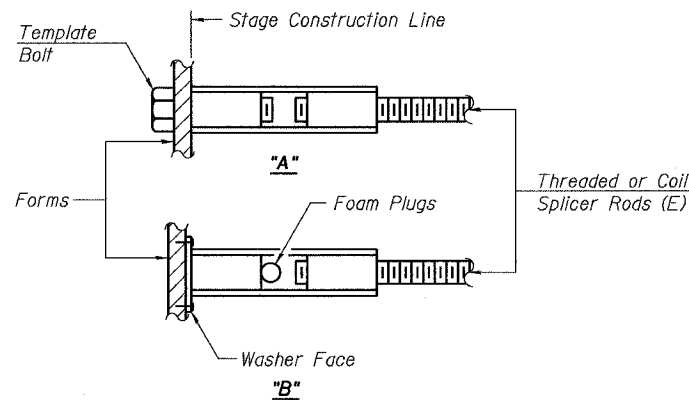
Bar splicer assemblies shall be according to Section 508 of the Standard Specifications, except as noted. The furnishing and installation of bar splicer assemblies will be measured and paid for at the contract unit price each for "BAR SPLICERS."

The diameter of this part is equal or larger than the diameter of bar spliced.
The diameter of this part is the same as the diameter of the bar spliced.



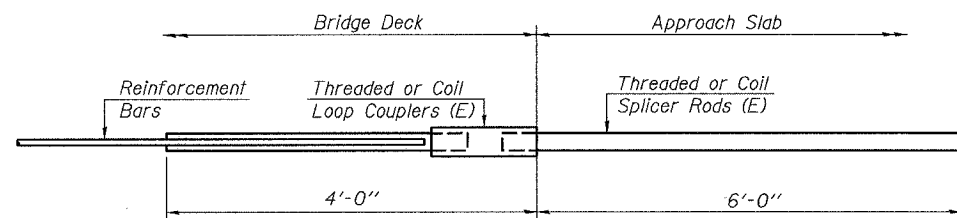
BAR SPLICER ASSEMBLY ALTERNATIVES

** Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



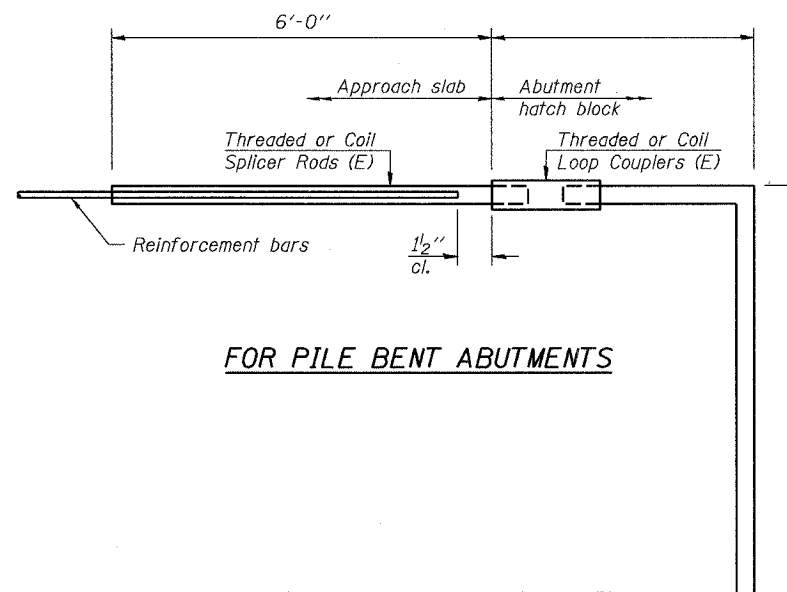
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



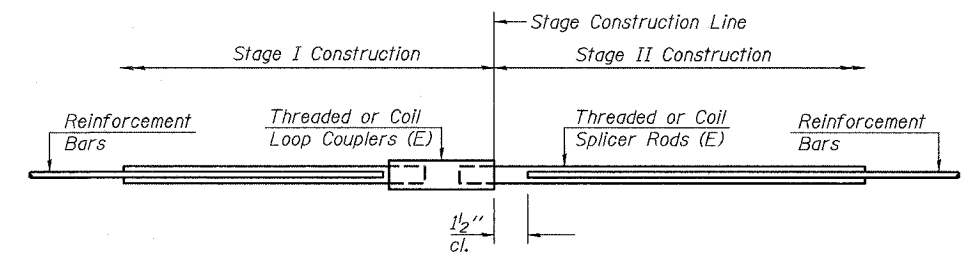
FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 9.2 kips - tension
No. Required =



FOR PILE BENT ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 9.2 kips - tension
No. Required =



STANDARD

Bar Size	No. Assemblies Required	Location
#4	96	Deck
#5	6	Deck Joint

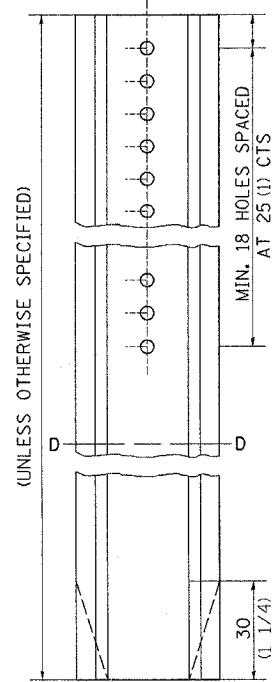
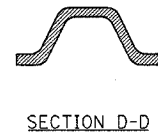
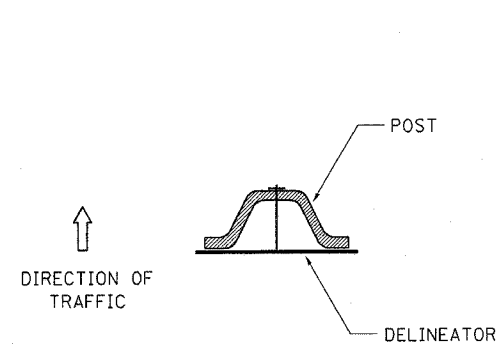
BAR SPLICER ASSEMBLY DETAILS
F.A.P. 553 (IL 72)
OVER STILLMAN CREEK
SECTION 119BR-3
OGLE COUNTY
SN 071-0042

DESIGNED	SB, DP
CHECKED	SB
DRAWN	BH, BS
CHECKED	SB

BSD-1 10-22-04

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	27
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

DELINEATOR AND POST ORIENTATION



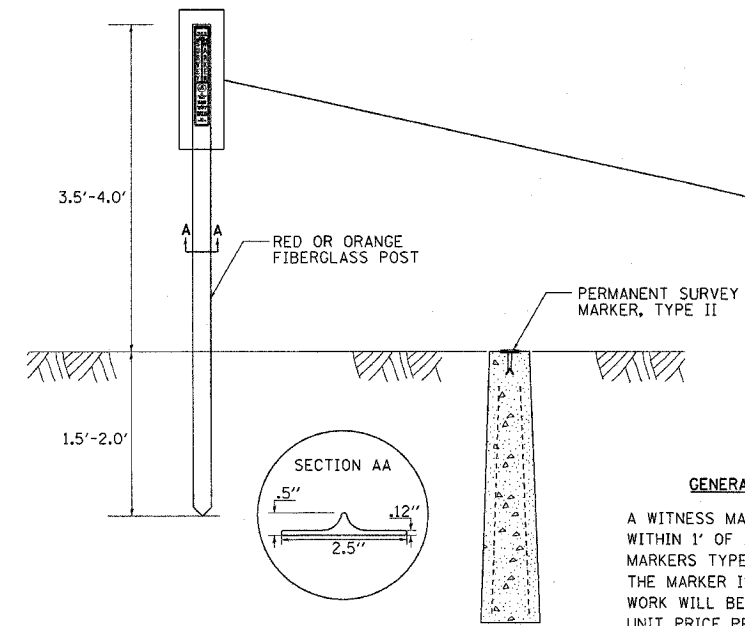
DELINEATORS SHALL BE INSTALLED ACCORDING TO STANDARD 635001 EXCEPT THAT THE POST SHALL BE ROTATED 180°. THE POST WILL HAVE THE WIDE SIDE FACING TRAFFIC AND THE DELINEATOR ATTACHED AS SHOWN ABOVE.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

DELINEATOR AND POST ORIENTATION 37.4

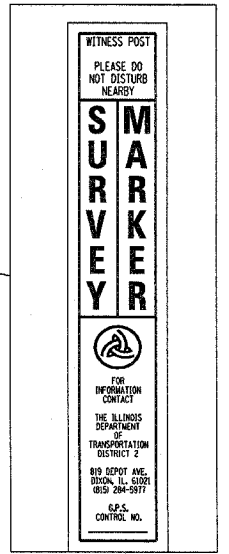
REVISED 1-31-00

WITNESS MARKER FOR PERMANENT SURVEY MARKERS TYPE II



GENERAL NOTES

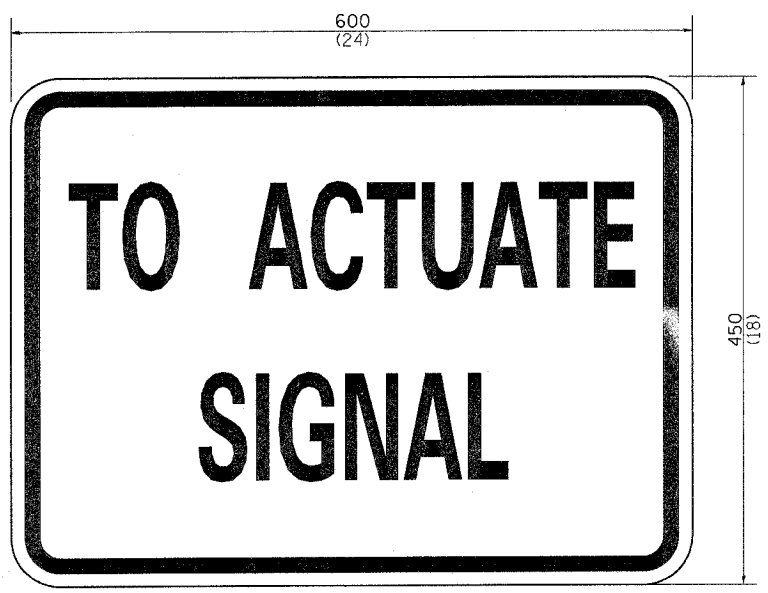
A WITNESS MARKER SHALL BE INSTALLED WITHIN 1' OF ALL PERMANENT SURVEY MARKERS TYPE II EXCEPT IN AREAS WHERE THE MARKER IS IN THE SIDEWALK. THIS WORK WILL BE INCLUDED TO THE CONTRACT UNIT PRICE PER EACH FOR PERMANENT SURVEY MARKERS, TYPE II.



WITNESS MARKER FOR PERMANENT SURVEY MARKERS TYPE II 38.4

REVISED 1-31-00

STOP LINE SIGN FOR TEMPORARY SIGNALS



SIZE: 600(24) x 450(18)
 100(4) CAPITAL LETTERS - BLACK
 13 (1/2) BORDER - BLACK
 WHITE REFLECTIVE - TYPE B
 ENGINEERING GRADE SHEETING

GENERAL NOTE:

THIS SIGN SHALL BE INSTALLED AT THE STOP LINE AS DIRECTED BY ENGINEER.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

STOP LINE SIGN FOR TEMPORARY SIGNALS 99.4

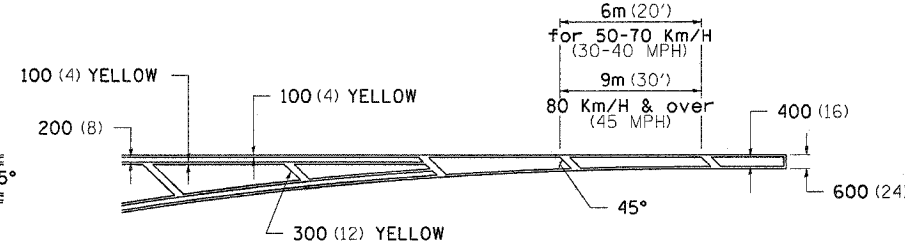
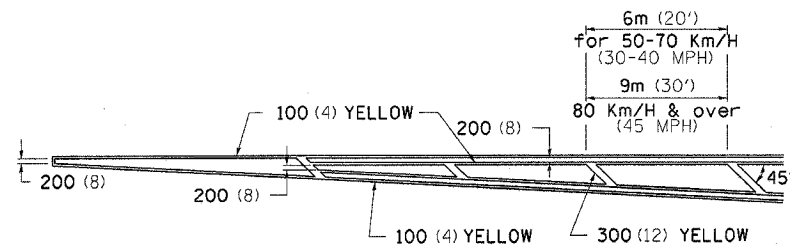
REVISED 8-7-90

PLOT DATE = Fri Sep 02 11:37:56 2005
 PLOT SCALE = 500000
 PLOT SCALE = 500000
 REFERENCE = #REF#

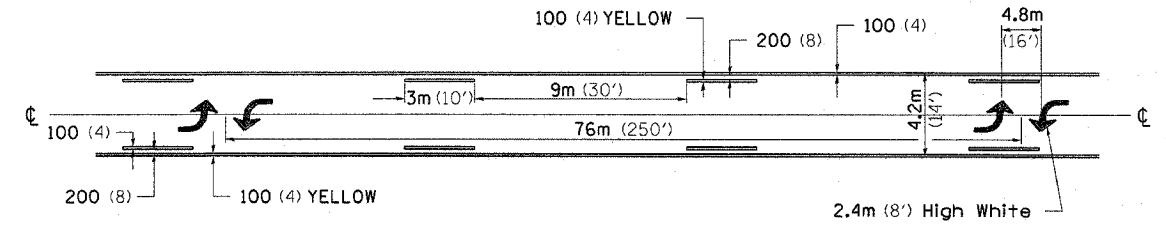
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	28
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

TYPICAL PAVEMENT MARKINGS

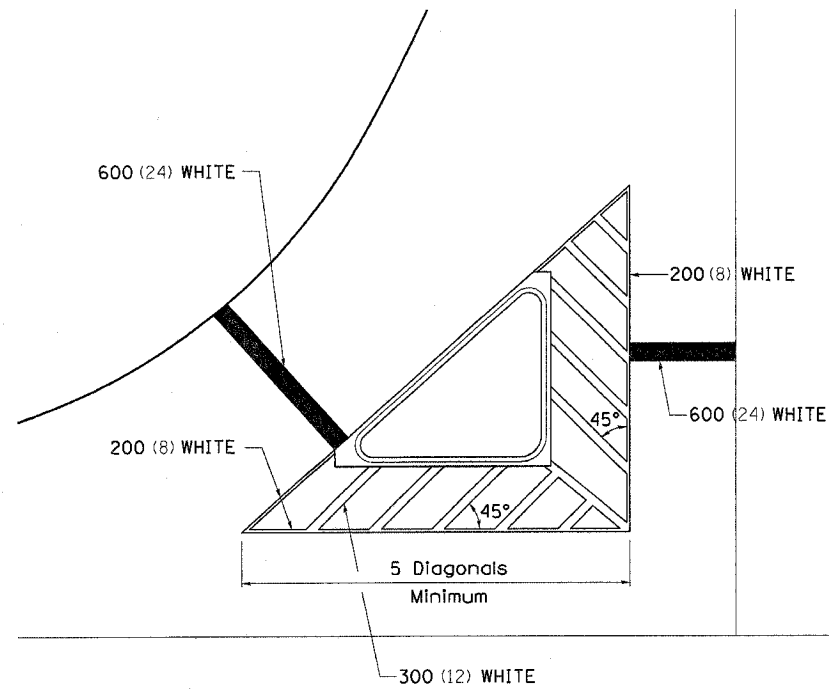
TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN



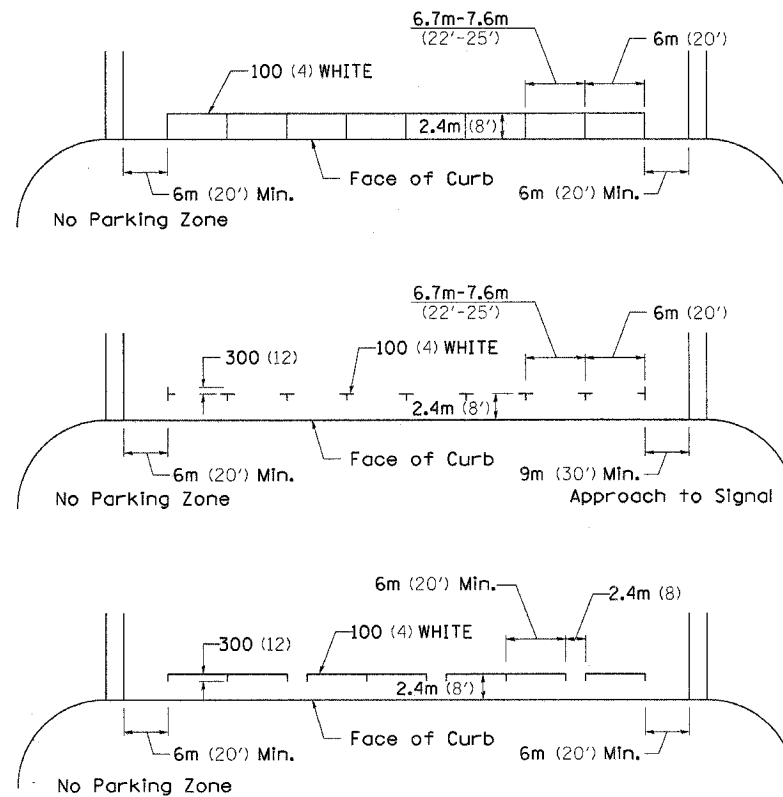
MEDIAN PAVEMENT MARKING



TYPICAL ISLAND OFFSET SHOULDER WIDTH



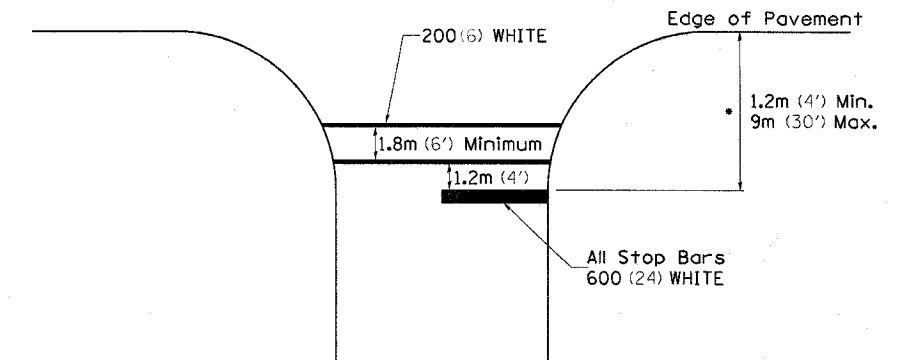
TYPICAL PARKING SPACING



•• ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

STANDARD CROSSWALK MARKING

See Schedules for Locations



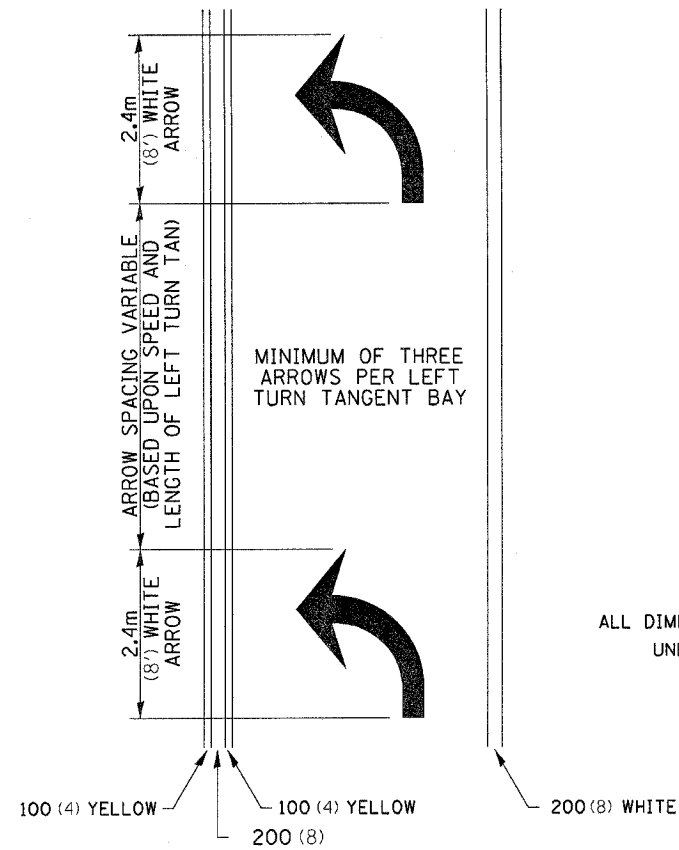
• Distance to the nearest edge of the intersecting roadway in the absence of a marked crosswalk.

PLOT DATE = Fri Sep 02 11:37:51 2005
 FILE NAME = c:\p\projects\119BR\119BR.dgn
 PLOT SCALE = 50:1
 REFERENCE = WHP*

F.A.P. RTE. 553	SECTION 119BR-3	COUNTY OGLE	TOTAL SHEETS 40	SHEET NO. 29
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

TYPICAL PAVEMENT MARKINGS

ARROW LAYOUT



12.2m
6 at (40') O.C.
APPROACH SIDE ONLY

See Typical Drawing at right

12.2m (40') O.C.

1.2m (4')

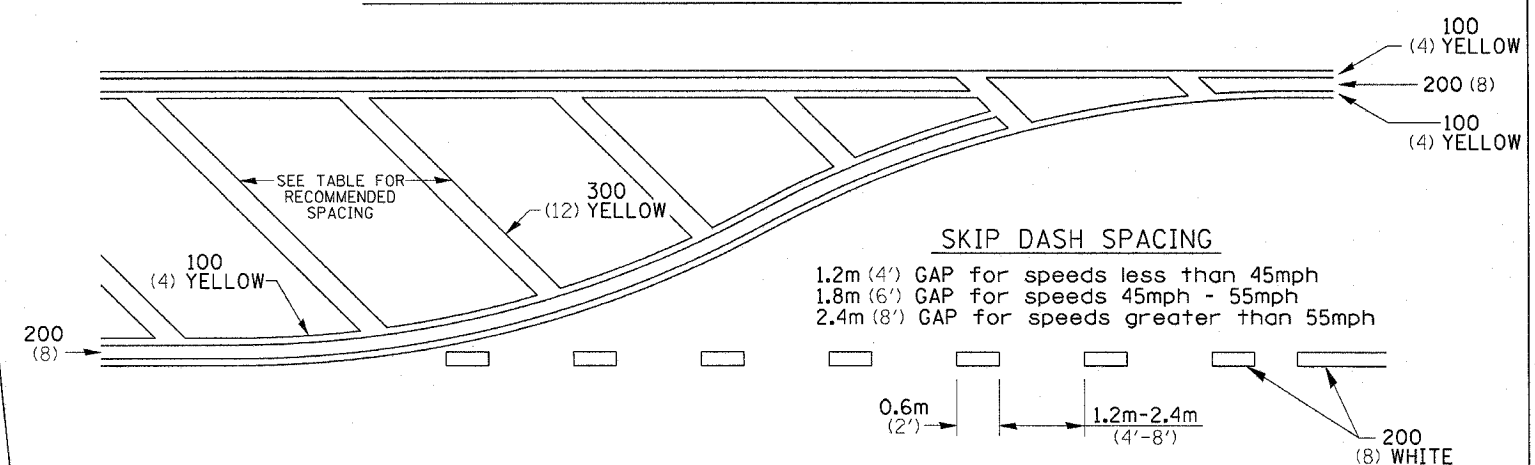
1.8m (6') MINIMUM

6.1m (20') O.C.

12.2m (40') O.C.

12.2m
6 at (40') O.C.
APPROACH SIDE ONLY

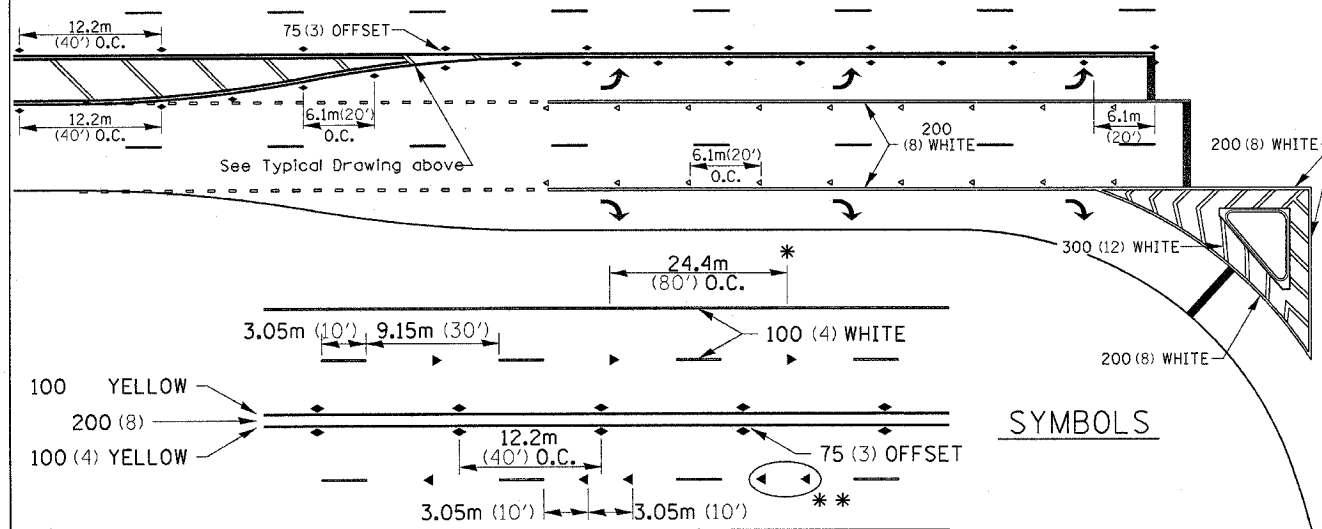
TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN



RECOMMENDED SPACING BETWEEN DIAGONALS (IN FEET)

Speed Limit Range	Continuous Median Area	Intersection Channelization	Objects (Islands)
less than 50Km/H (30MPH)	15.3m (50')	4.53m (15')	3.05m (10')
50-60Km/H (30-40MPH)	22.9m (75')	6.1m (20')	4.53m (15')
70Km/H (45MPH) & over	22.9m (75')	9.05m (30')	6.1m (20')

NOTE: If the spacing recommended in the Table does not permit at least five diagonal lines in the area being marked, the spacing from the next lowest speed range should be used. The recommended spacing is measured parallel to the pavement center line.

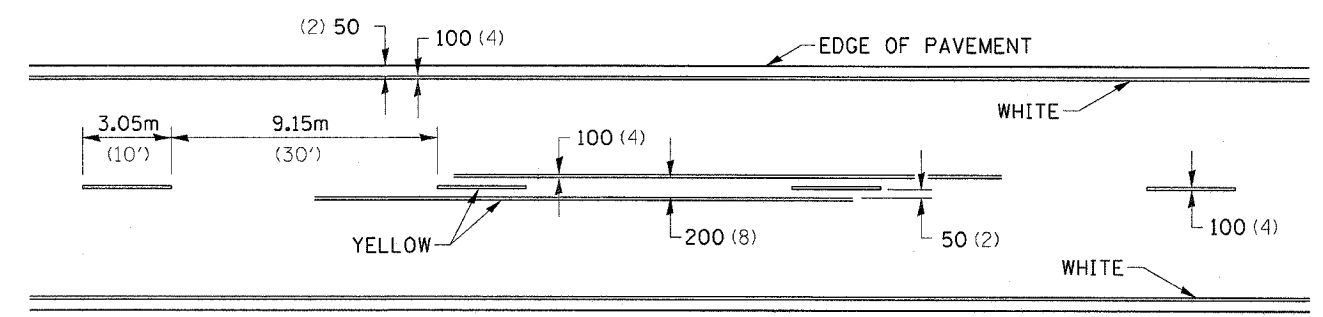


SYMBOLS

See Typical Drawing above

12.2m
6 at (40') O.C.
APPROACH SIDE ONLY

TYPICAL PAVEMENT MARKING FOR TWO LANE SECTION - NO PASSING ZONES



- REDUCE TO 12.2m (40') O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 15Km/H (10MPH) LOWER THAN POSTED SPEEDS.
- USE DOUBLE MARKERS WHEN ADT ≥ 25,000

MULTI-LANE / UNDIVIDED

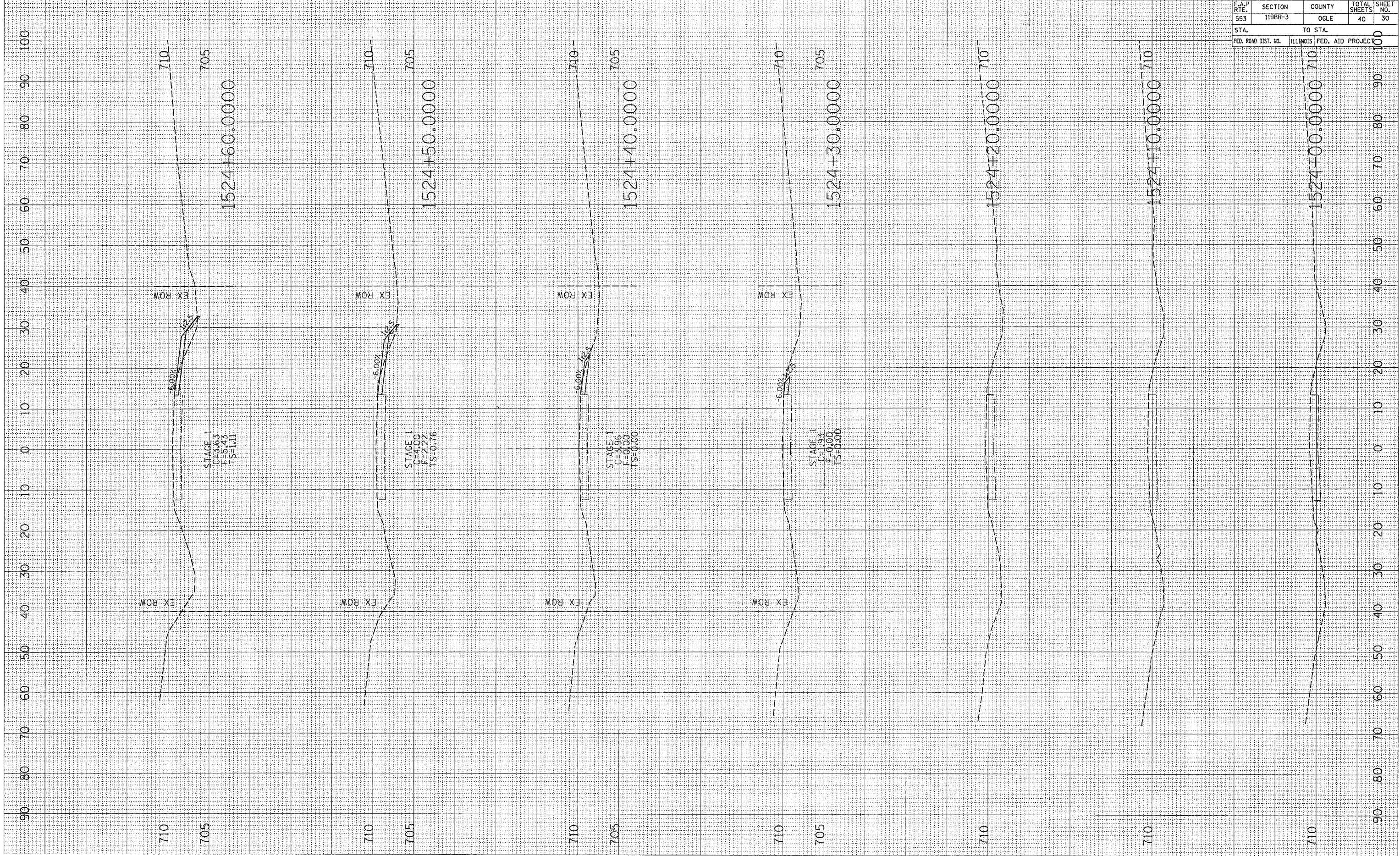
PLOT DATE: Fri Sep 02 11:37:51 2005
 FILE NAME: c:\pavmark\119br3\119br3.dgn
 PLOT SCALE: 1:1
 REFERENCE: #REF#

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	30
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

FINAL SURVEY	DATE
SURVEYED	BY
NOTE BOOK	
AREAS	
CHECKED	
NO.	

ORIGINAL SURVEY	DATE
SURVEYED	BY
NOTE BOOK	
AREAS	
CHECKED	
NO.	

PLOT DATE = Fri Sep 02 11:56:46 2005
 FILE NAME = c:\p\projects\2005\119br-3\119br-3.dwg
 PLOT SCALE = 1/8" = 1' IN
 USER NAME = mgj11j



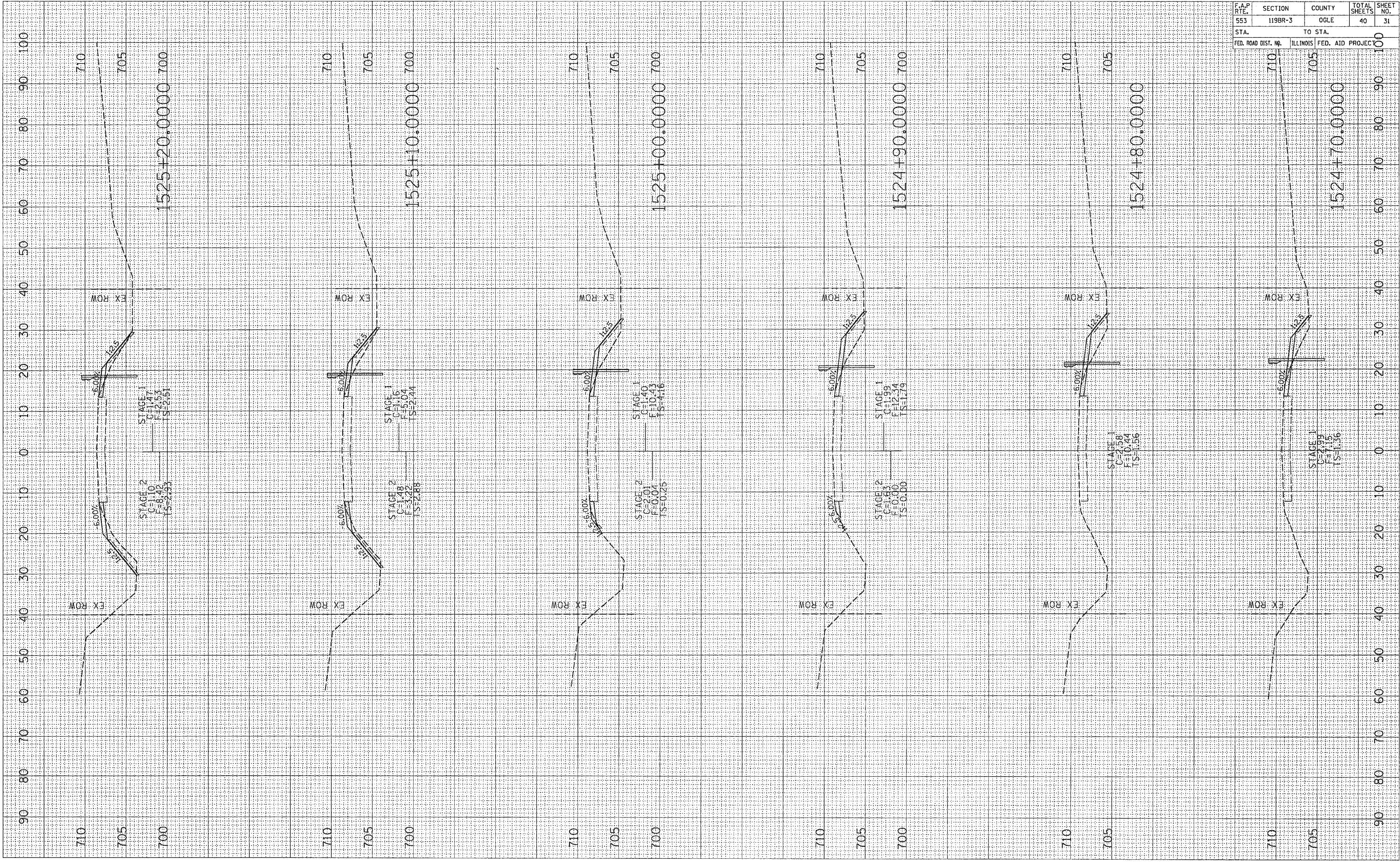
PLOT DATE = Fri, Sep 02 11:55:46 2005
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = mgj11j

ORIGINAL SURVEY
 SURVEY PLOTTED
 NOTE BOOK NO.
 AREAS CHECKED

FINAL SURVEY
 SURVEY PLOTTED
 NOTE BOOK NO.
 AREAS CHECKED

BY _____

DATE _____

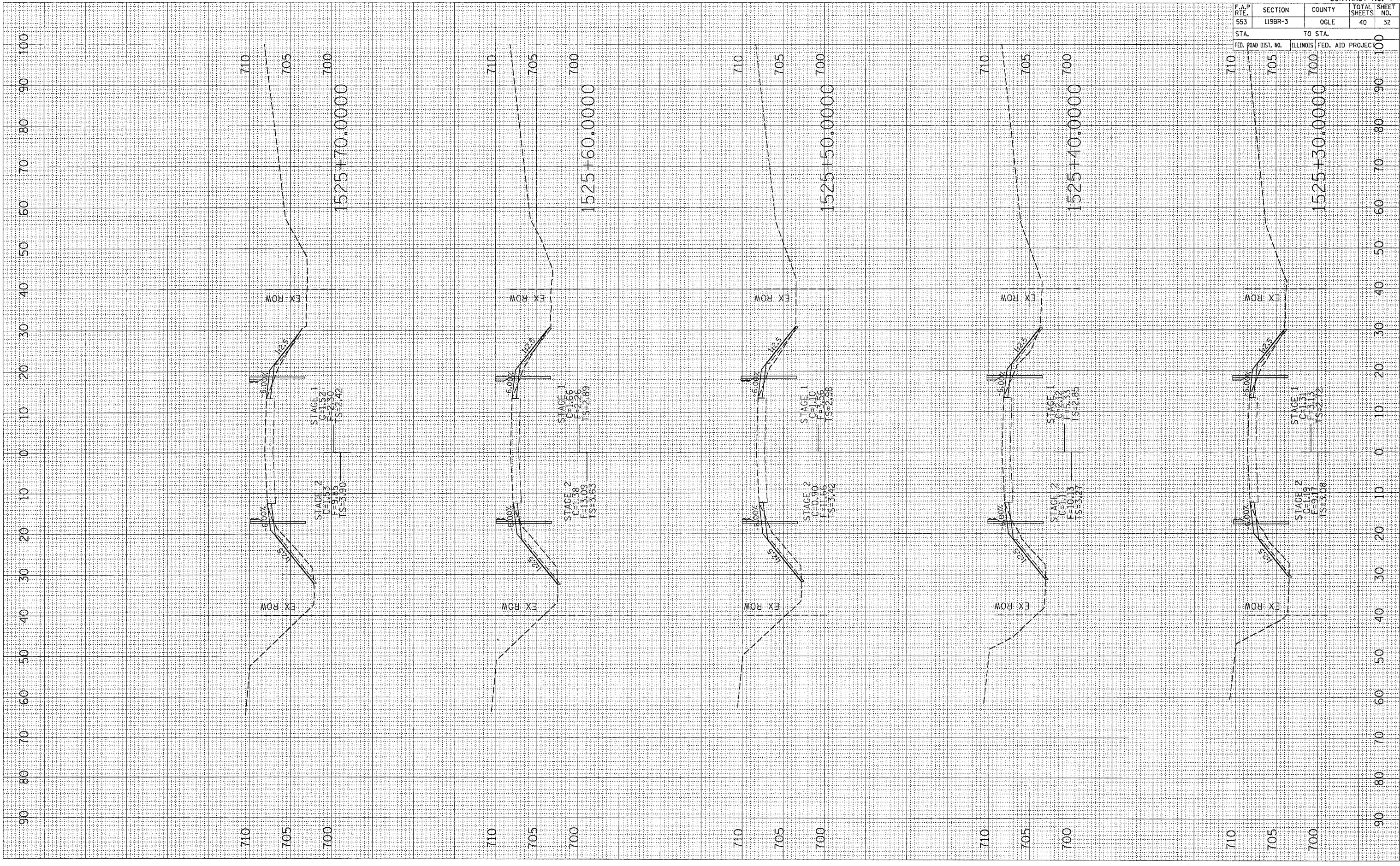


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	31
STA.		TO STA.		100
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		100

PLOT DATE = Fri Sep 02 11:55:47 2005
 PLOT SCALE = 10.0000 / IN.
 USER NAME = mgj11j

ORIGINAL SURVEYED SURVEY PLOTTED AREAS CHECKED
 SURVEYED SURVEY PLOTTED AREAS CHECKED
 NO.

BY DATE
 SURVEYED SURVEY PLOTTED AREAS CHECKED
 NO.



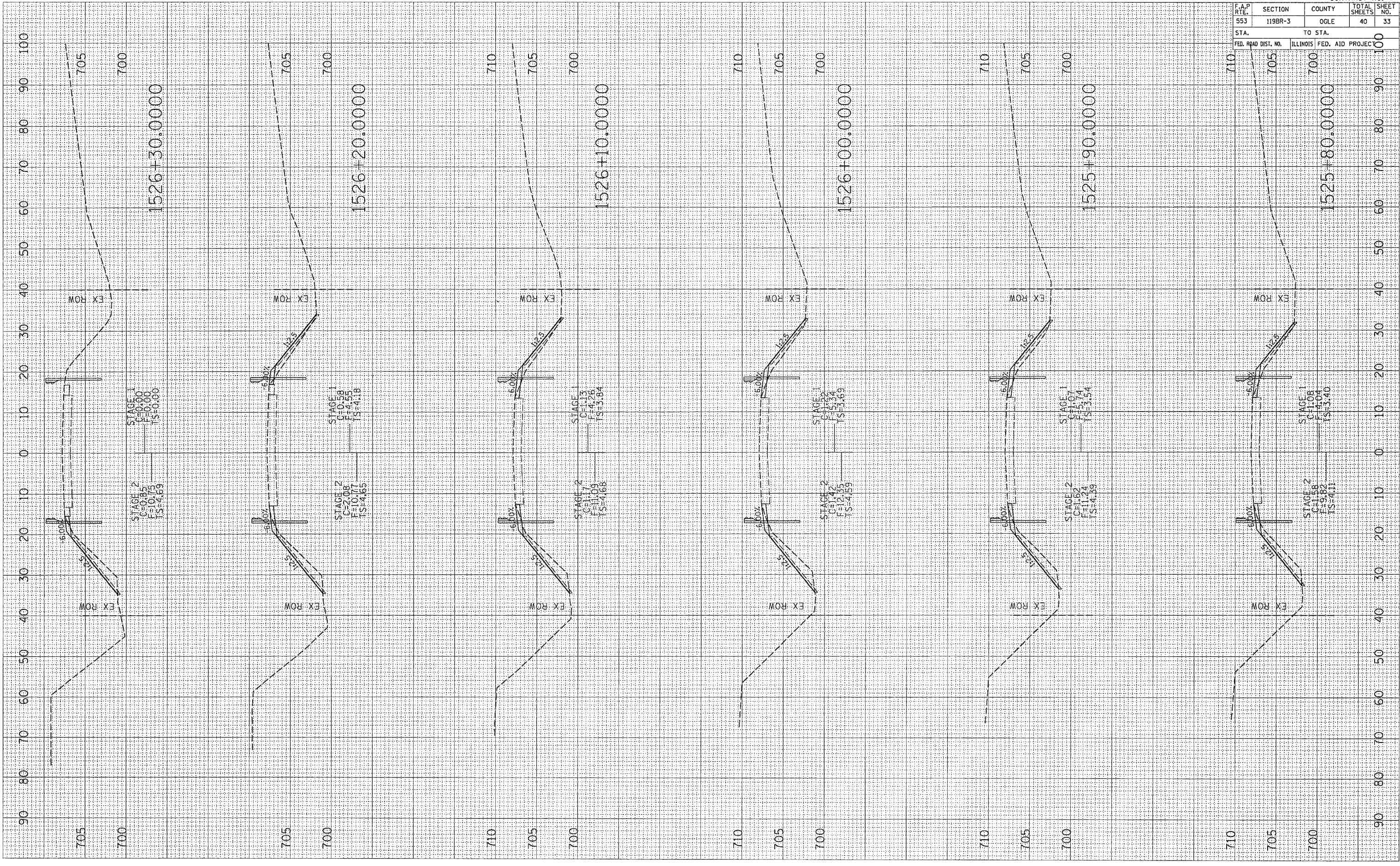
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	1198R-3	OGLE	40	32
STA. 1525+30.0000		TO STA. 1525+70.0000		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		100

CONTRACT NO. 64A47

PLOT DATE = Fri, Sep 02 11:55:47 2005
 FILE NAME = 152526.dwg
 PLOT SCALE = 10.00000 / IN.
 USER NAME = mgj11j

ORIGINAL SURVEYED BY DATE
 SURVEY PLOTTED BY DATE
 NOTE BOOK NO. AREAS CHECKED

FINAL SURVEYED BY DATE
 SURVEY PLOTTED BY DATE
 NOTE BOOK NO. AREAS CHECKED

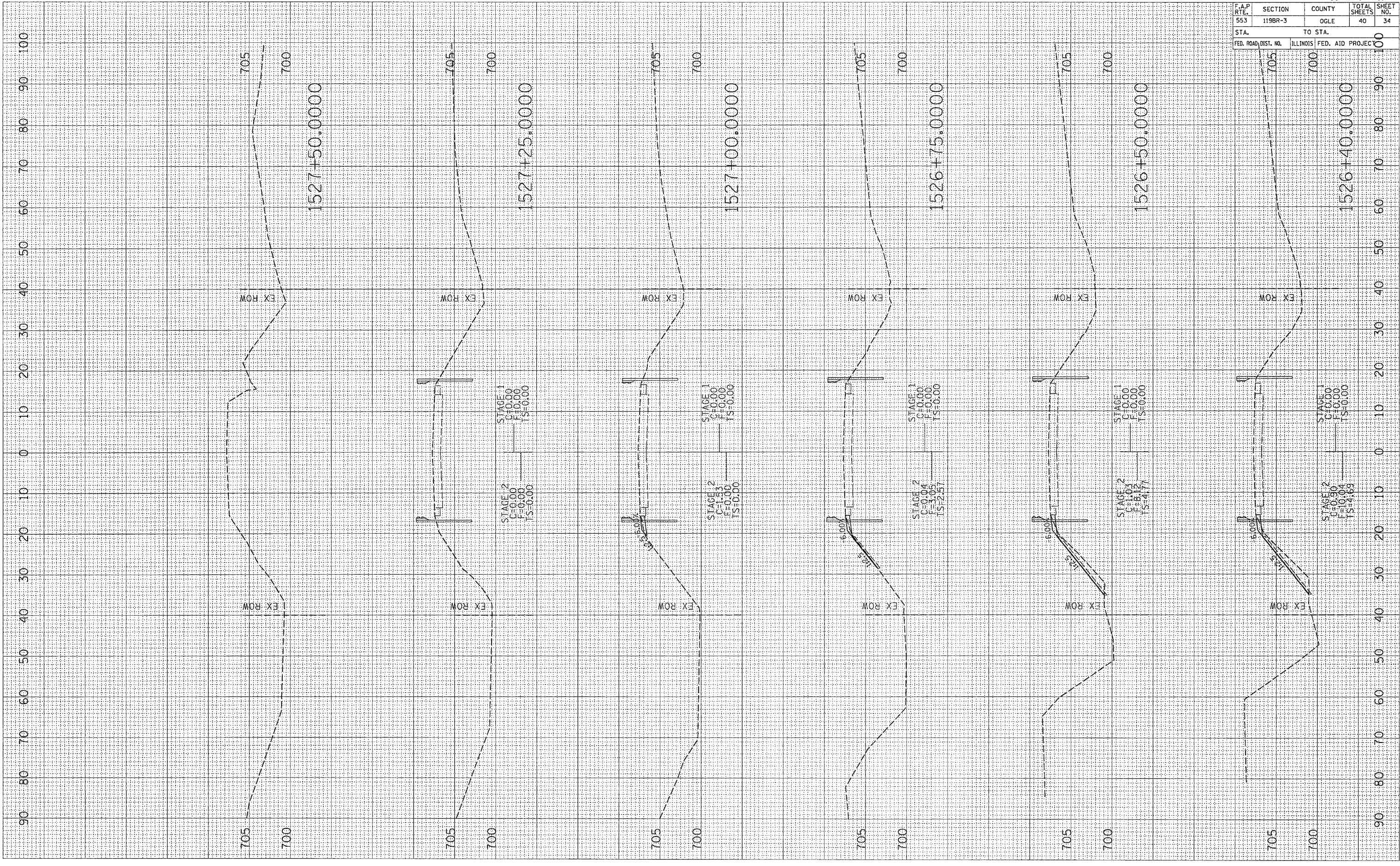


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	33
STA. TO STA.		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		

PLOT DATE = Fri, Sep 02 11:55:47 2005
 PLOT SCALE = 10.0000 / IN.
 USER NAME = meg11j

ORIGINAL SURVEY
 SURVEYED _____
 PLOTTED _____
 NO. _____
 AREAS CHECKED _____

BY _____
 DATE _____
 SURVEYED _____
 PLOTTED _____
 NO. _____
 AREAS CHECKED _____



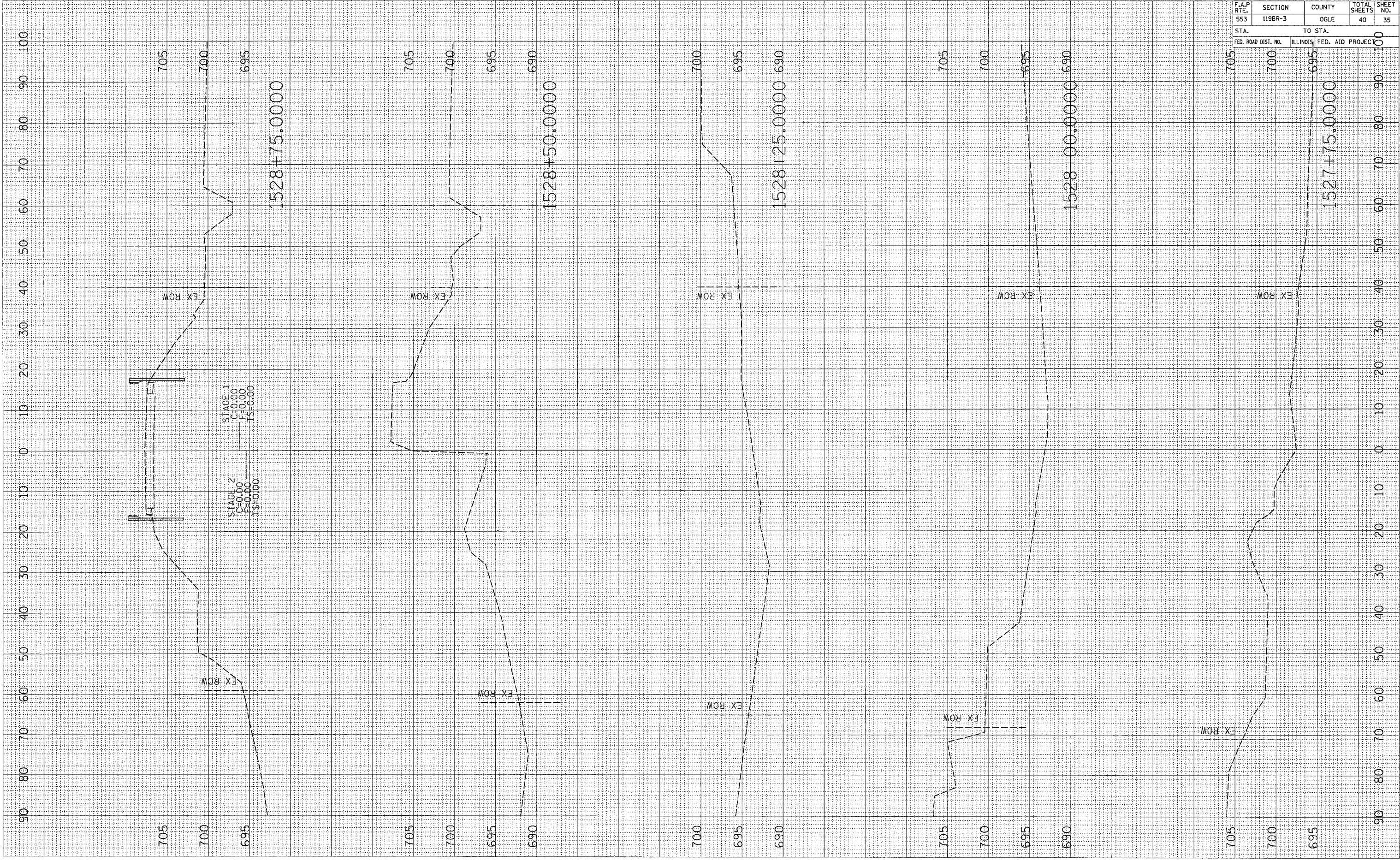
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	34
STA. TO STA.		ILLINOIS FED. AID PROJECT		
FED. ROAD DIST. NO.		100		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	35
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS CHECKED		
NO.		

PLOT DATE = Fri Sep 02 11:55:48 2005
 PLOT SCALE = 10.0000 / IN.
 USER NAME = mg11j

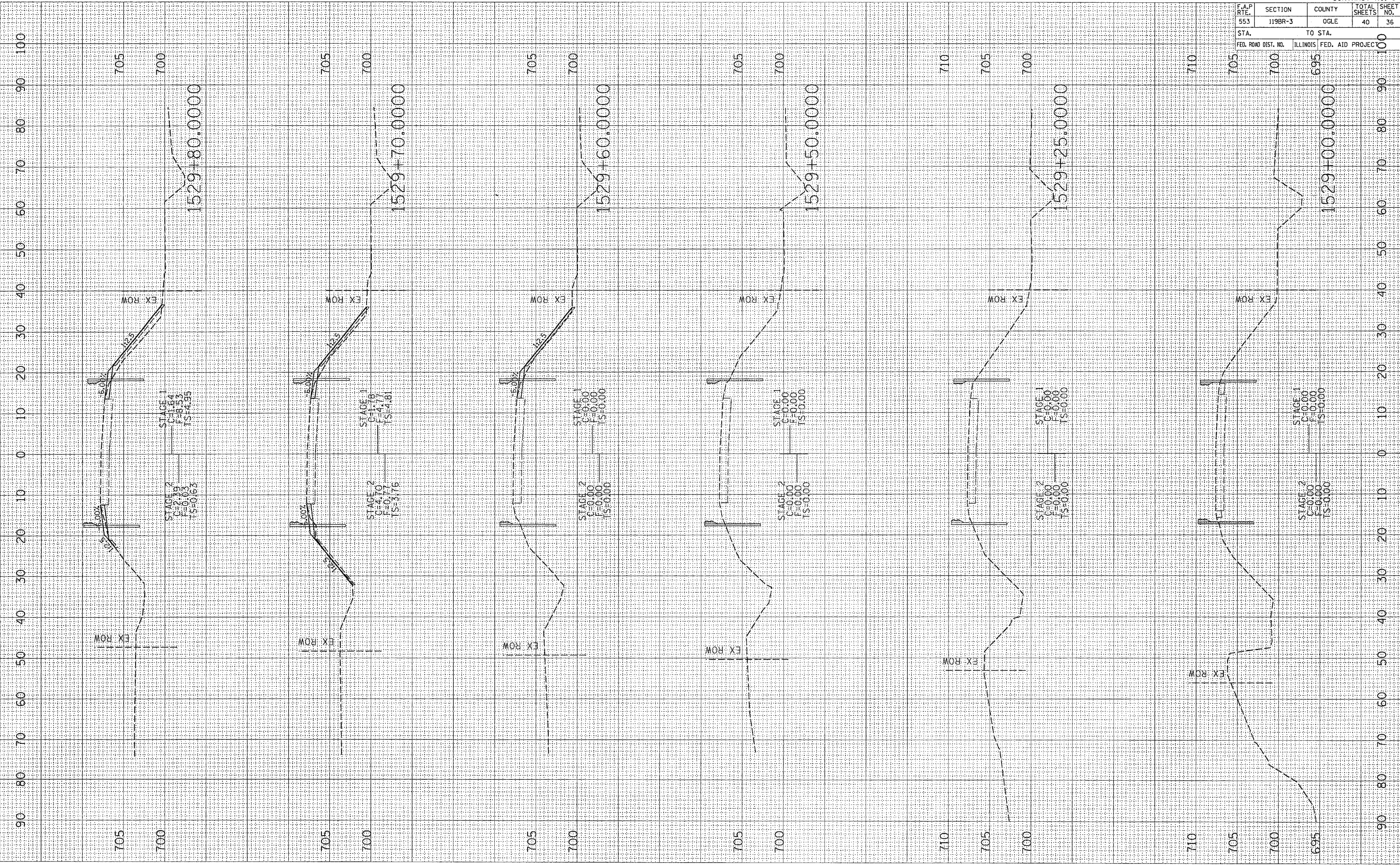


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	1198R-3	OGLE	40	36
STA.		TO STA.		100
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJEC		100

FINAL SURVEY	DATE
NO. _____	_____
BY _____	
NO. _____	
BY _____	
NO. _____	
BY _____	
NO. _____	
BY _____	
NO. _____	

ORIGINAL SURVEY	DATE
NO. _____	_____
BY _____	
NO. _____	
BY _____	
NO. _____	
BY _____	
NO. _____	
BY _____	
NO. _____	

PLOT DATE = Fri Sep 02 11:55:48 2005
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 PLOT SCALE = 1/8" = 1' / IN.
 USER NAME = mgj11j



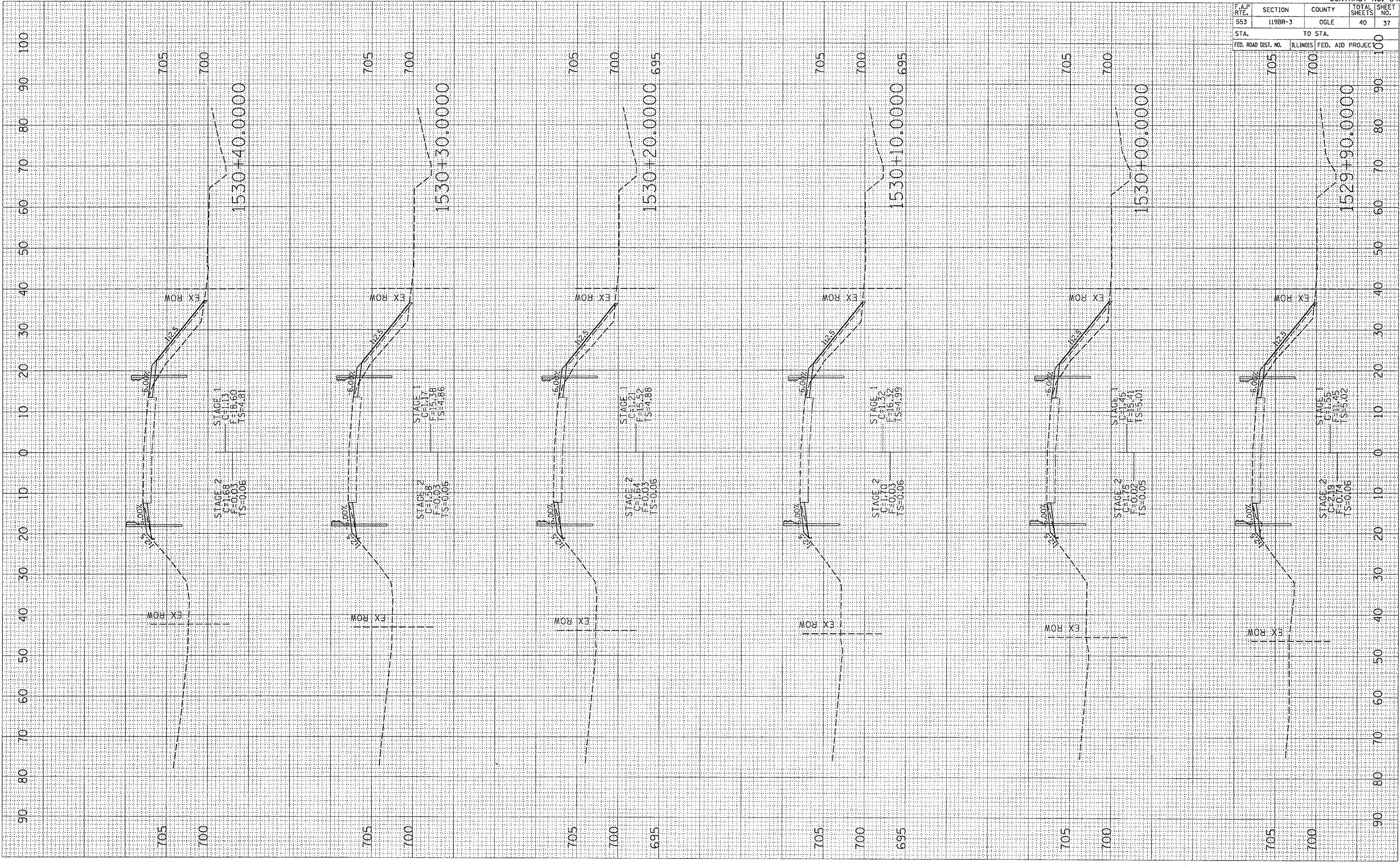
705 700 1529+80.0000
 705 700 1529+70.0000
 705 700 1529+60.0000
 705 700 1529+50.0000
 710 705 700 1529+25.0000
 710 705 700 1529+00.0000

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	37
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJEC	100	

DATE	BY

DATE	BY

PLOT DATE = Fri Sep 02 11:55:49 2005
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 PLOT SCALE = 1/8" = 1' / IN.
 USER NAME = mehjj

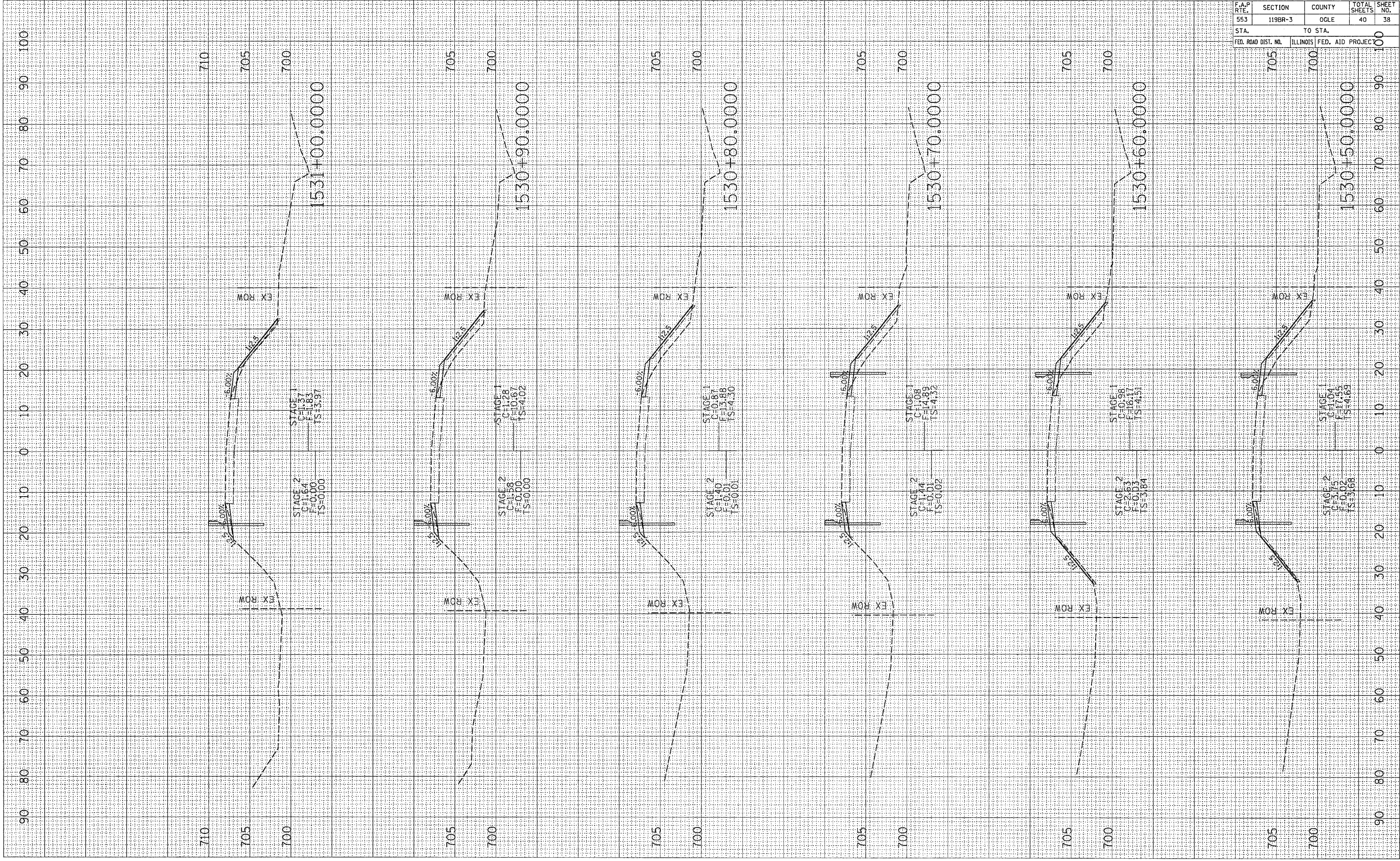


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	38
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT NO.		

FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS CHECKED		
NO.		

PLOT DATE = Fri, Sep 02 11:55:43 2005
 PLOT SCALE = 1/8"=100'
 USER NAME = meghlj

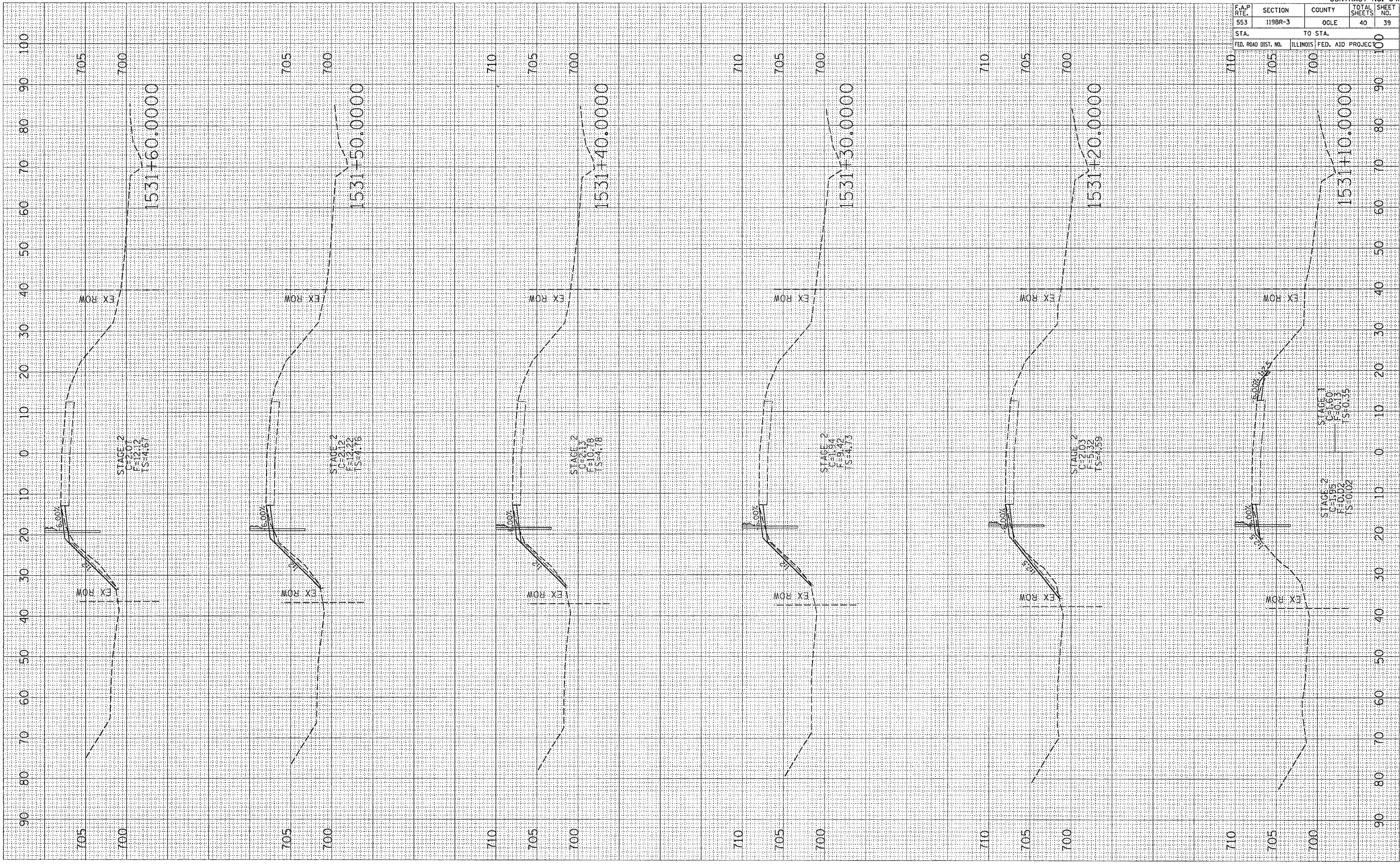


PLOT DATE = Fri Sep 02 11:55:59 2005
 PLOT SCALE = 10.0000 / IN.
 USER NAME = meg111

ORIGINAL SURVEYED
 SURVEY PLOTTED
 SURVEY NOTE BOOK NO. _____
 AREAS CHECKED _____

FINAL SURVEYED
 SURVEY PLOTTED
 SURVEY NOTE BOOK NO. _____
 AREAS CHECKED _____

BY _____ DATE _____



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	39

CONTRACT NO. 64A47

STA.	TO STA.
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
553	119BR-3	OGLE	40	40
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJEC		

FINAL SURVEY	DATE
SURVEYED	
NOTE BOOK	
AREAS CHECKED	
NO.	

ORIGINAL SURVEY	DATE
SURVEYED	
NOTE BOOK	
AREAS CHECKED	
NO.	

PLOT DATE = Fri Sep 02 11:56:58 2005
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 USER NAME = mgj11

