

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	41	1

STATE CONTRACT NO. 72818

TRAFFIC DATA

ADT: IL 100	
2001 = 1650	PV = 94.0%
2006 = 1750	SU = 3.0%
2016 = 1900	MU = 3.0%
2026 = 2100	

INDEX OF SHEETS

- 1 COVER SHEET
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  - 20 DITCH RIPRAP DETAIL
  - 21-26 STRUCTURE PLANS - SN 086-7500
  - 26a-26b SOIL BORINGS
  - 27 CULVERT PROFILE
  - 28-39 CROSS SECTIONS
- HIGHWAY STANDARDS

PROPOSED  
HIGHWAY PLANS

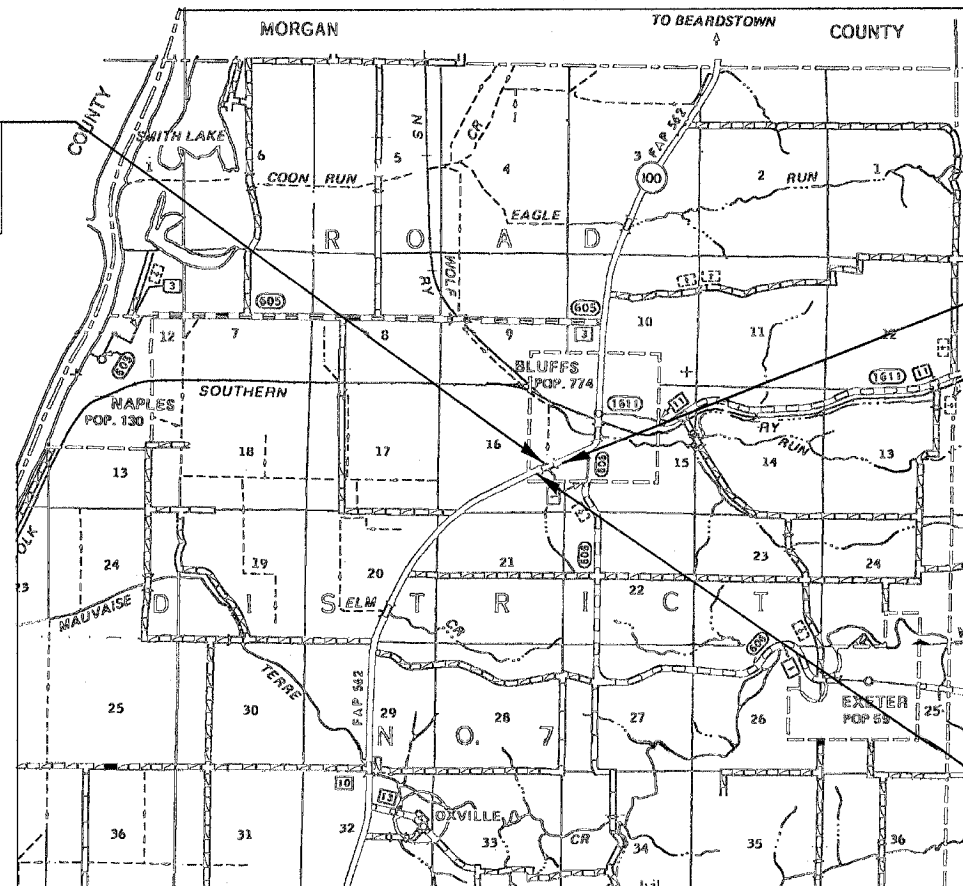
FAP ROUTE 562 (IL 100)  
SECTION 117(B-4)  
PROJECT: ACF-0562(013)  
SCOTT COUNTY  
C-96-501-06

IDOT HIGHWAY STANDARDS

000001-04	701306-01
001001	701311-02
001006	701321-08
280001-02	701326-02
442201-01	702001-05
482011-01	704001-02
666001	780001-01
701006-02	781001-02
701011-01	886001
701201-02	886006

PROPOSED CULVERT

SECTION 117(B-4) INCLUDES:  
BRIDGE REPLACEMENT: REPLACE SN 086-0022 WITH  
10'x6'x90" R.C. BOX CULVERT W/ CAST-IN-PLACE HDWLS  
PROPOSED SN 086-7500  
RESURFACING FROM STA. 296+25 TO STA. 304+00



PROJECT BEGINS  
STA. 296 + 25

PROJECT ENDS  
STA. 304 + 00

NET LENGTH OF PROJECT  
0.1468 MILE = 775.00 FT

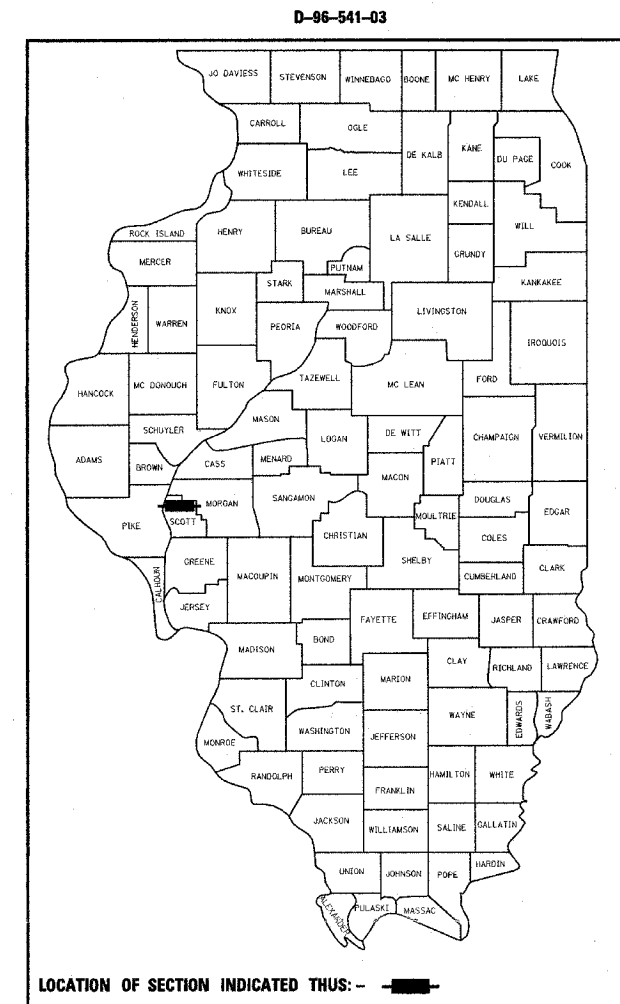
PLANS PREPARED BY

**HORNER & SHIFRIN, INC.**  
ENGINEERS ■ ARCHITECTS ■ PLANNERS

141 MARKET PLACE SUITE 208 ■ FAIRVIEW HEIGHTS, ILLINOIS 62208  
5200 OAKLAND AVENUE ■ ST. LOUIS, MISSOURI 63110  
www.hornershifrin.com



*Steven R. Donahue* 4/27/05  
STEVEN R. DONAHUE, P.E. Date  
License Expires 11/30/2005



LOCATION OF SECTION INDICATED THUS: — ■ —

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED June 7, 2005  
*Chris M. Reed* DISTRICT ENGINEER  
October 14, 2005  
*Mike Hine* ENGINEER OF DESIGN AND ENVIRONMENT  
October 14, 2005  
*Eric E. Howard* DEPUTY DIRECTOR, DIVISION OF HIGHWAYS

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OF THE STATE OF ILLINOIS

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123

CONTRACT NO. 72818

Plot Date: 4/27/2005  
Plot Time: 12:28:43 PM  
Plotted By: ghorntest  
Pen Table: idot.tbl  
Filename: s:\040011\100\Cad\T\_Construction\Plans\01\_Cover\_1100.dgn  
Project Engineer: John Negangard Phone: (217) 782-6390  
Squad Leader: Vince Madonia Phone: (217) 785-9046

**GENERAL NOTES**

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF STANDARD SPECIFICATION, THE ILL. I.E. NUMBER IS 1-800-892-0123.

THE LOCATION OF ALL UTILITIES ARE BASED ON INFORMATION PROVIDED BY OTHERS AND ARE INTENDED TO BE APPROXIMATE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE HIS CONSTRUCTION ACTIVITIES WITH THE VARIOUS UTILITY OWNERS. ALL POTENTIAL CONFLICTS SHALL BE INVESTIGATED AND REMEDIAL ACTION TAKEN PRIOR TO INTERRUPTION OF THE CONTRACTOR'S PROGRESS.

ALL UTILITY FACILITIES THAT REQUIRE RELOCATION WITHIN STATE R.O.W. SHALL BE COMPLETED BY THE UTILITY COMPANY UNLESS OTHERWISE SHOWN ON THE PLANS.

2. IN ADDITION TO FIELD SURVEYS AND AERIAL SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION DUE TO CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

3. ALL STATIONS AND OFFSET REFERENCES ARE TO THE ROADWAY BASELINE (IL 100) OR ROADWAY CENTERLINE (SIDE ROADS) UNLESS OTHERWISE NOTED. THE STATE PLANE COORDINATE SYSTEM HAS BEEN USED FOR THE HORIZONTAL CONTROL.

4. ALL ELEVATIONS SHOWN ON THE PLANS ARE BASED ON U.S.G.S. MEAN SEA LEVEL DATUM.

5. ANY REFERENCE WITHIN THESE PLANS TO A STANDARD SHALL BE INTERPRETED TO MEAN THE EDITION INDICATED BY THE SUB-NUMBER LISTED ON THE PREVIOUS SHEET OR THE COPY INCLUDED IN THESE PLANS.

6. THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS BITUMINOUS LIFTS.

7. IF SO DIRECTED BY THE ENGINEER, DITCHES ADJACENT TO EMBANKMENTS SHALL BE CONSTRUCTED PRIOR TO STARTING THE CONSTRUCTION OF THE EMBANKMENT FILL.

8. GRADING SHALL BE DONE BY HAND AROUND LIGHT POLES, UTILITY POLES, SIGN POSTS, SHRUBS, TREES OR OTHER NATURAL OR MAN-MADE OBJECTS WHERE SHALLOW FILLS OR CUTS ARE ADJACENT TO THE ITEMS. IT IS THE INTENT THAT THE LIMITS OF CONSTRUCTION BE SUCH AS TO PRESERVE IN THE ORIGINAL STATE AS MUCH AREA OF TEMPORARY EASEMENTS AS POSSIBLE. THE DECISION AS TO ITEMS TO REMAIN IN PLACE SHALL BE DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE PER CUBIC YARD FOR EARTH EXCAVATION, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

9. SEEDING SHALL BE DONE ON ALL AREAS THAT ARE DISTURBED BY CONSTRUCTION OPERATIONS AS DIRECTED BY THE ENGINEER. SEEDING SHALL BE PAID FOR ONLY WITHIN THE PROPOSED RIGHT-OF-WAY OR EASEMENT LIMITS. ALL AREAS DISTURBED BY THE CONTRACTOR OUTSIDE THE PROPOSED CONSTRUCTIONS LIMITS SHALL BE SEEDDED AS DIRECTED BY THE ENGINEER, AT THE CONTRACTOR'S EXPENSE.

10. FACTORS USED FOR ESTIMATING PLAN QUANTITIES ARE AS FOLLOWS AND SHALL NOT BE USED FOR THE BASIS OF FINAL QUANTITIES:

BITUMINOUS CONCRETE BASE COURSE	0.056	TON/SQ YD/IN
BITUMINOUS CONCRETE SURFACE COURSE	0.056	TON/SQ YD/IN
AGGREGATE (SURFACE, BASE, & BACKFILL)	2.05	TON/CU YD
BITUMINOUS MATERIALS:		
PRIME COAT FOR BITUMINOUS CONCRETE:		
- ON PAVEMENT	0.00038	TON/SQ YD
- AGGREGATE (PRIME COAT)	0.002	TON/SQ YD
RIP RAP	1.50	TON/CU YD
SEEDING, CLASS 2	200	LB/ACRE
TEMPORARY EROSION CONTROL SEEDING	100	LB/ACRE
NITROGEN FERTILIZER NUTRIENT	90	LB/ACRE
PHOSPHORUS FERTILIZER NUTRIENT	90	LB/ACRE
POTASSIUM FERTILIZER NUTRIENT	90	LB/ACRE
AGRICULTURAL GROUND LIMESTONE	2	TON/ACRE
MULCH	2	TON/ACRE

11. THE QUANTITY OF SHORT TERM PAVEMENT MARKING SHOWN IN THE PLANS IS BASED ON ONE APPLICATION EACH FOR THE BINDER COURSE AND SURFACE COURSE.

12. FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.

13. THE DISTRICT BUREAU OF OPERATIONS SHALL BE NOTIFIED AT LEAST 14 DAYS PRIOR TO PLACEMENT OF FINAL PAVEMENT MARKINGS PH: (217) 782-7314.

14. WHERE PROPOSED CONSTRUCTION ABUTS EXISTING APPURTENANCES, A SAW CUT SHALL BE MADE TO ACHIEVE A NEAT BUTT JOINT. ALL SAWED JOINTS FOR REMOVALS AND BUTT JOINTS SHALL BE CONSIDERED INCLUDED IN ITEM BEING REMOVED OR CONSTRUCTED.

15. THE CONTRACTOR SHALL BE REQUIRED TO COMPLY WITH THE PROVISIONS OF THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) STORM WATER PERMIT AND IMPLEMENT THE EROSION CONTROL PLAN INCLUDED IN THESE PLANS AND SPECIFIED HEREIN. AS SPECIFIED IN ARTICLE 107.23, THE ENGINEER MUST GIVE PRIOR APPROVAL BEFORE DISTURBANCE OF ANY AREA CAN BEGIN.

**COMMITMENTS**

NONE

**MIX DESIGN**

LOCATION(S):	
MIXTURE USE(S):	BITUMINOUS CONCRETE BASE COURSE / WIDENING
AC / PG:	PG 58-22
RAP % (MAX):	15%
DESIGN AIR VOIDS:	2.0% @ N DESIGN = 50
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL 19.0
FRICTION AGGREGATE:	N/A

LOCATION(S):	
MIXTURE USE(S):	BITUMINOUS SHOULDER
AC / PG:	PG 58-22
RAP % (MAX):	15%
DESIGN AIR VOIDS:	2.0% @ N DESIGN = 30
MIXTURE COMPOSITION: (GRADATION MIXTURE)	BAM
FRICTION AGGREGATE:	N/A

LOCATION(S):	
MIXTURE USE(S):	BITUMINOUS BINDER
AC / PG:	PG 64-22
RAP % (MAX):	15%
DESIGN AIR VOIDS:	4.0% @ N DESIGN = 50
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL 19.0
FRICTION AGGREGATE:	N/A

LOCATION(S):	
MIXTURE USE(S):	LEVEL BINDER (MACHINE METHOD)
AC / PG:	PG 64-22
RAP % (MAX):	15%
DESIGN AIR VOIDS:	4.0% @ N DESIGN = 50
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL 9.5
FRICTION AGGREGATE:	N/A

LOCATION(S):	
MIXTURE USE(S):	BITUMINOUS SURFACE
AC / PG:	PG 64-22
RAP % (MAX):	15%
DESIGN AIR VOIDS:	4.0% @ N DESIGN = 50
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL 9.5 OR 12.5
FRICTION AGGREGATE:	MIX C

LOCATION(S):	
MIXTURE USE(S):	INCIDENTAL BITUMINOUS SURFACING
AC / PG:	PG 64-22
RAP % (MAX):	15% RAP
DESIGN AIR VOIDS:	4.0% @ N DESIGN = 50
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL 9.5 OR 12.5
FRICTION AGGREGATE:	MIX C

DISTRICT SIX	
EXAMINED	June 17 2005
OPERATIONS ENGINEER	
EXAMINED	June 20 2005
PROGRAM IMPLEMENTATION ENGINEER	
EXAMINED	June 20 2005
PROGRAM DEVELOPMENT ENGINEER	

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**GENERAL NOTES, COMMITMENTS & MIX DESIGN**  
 F.A.P. 562 (IL 100)  
 SCOTT COUNTY

SCALE: VERT. NONE  
 HORIZ. NONE  
 DATE 04/27/2005

DRAWN BY AJP  
 CHECKED BY JDJ



**ENTRANCE IMPROVEMENT SCHEDULE FOR RURAL /URBAN "3R" PROJECTS**

LOCATION	TYPE OF ENTRANCE	EXISTING MATERIAL TYPE	WIDTH	RT OFFSET	LT OFFSET	LENGTH (FROM EDGE OF PVT/ BIT SHLD TO LIMITS OF IMPROVEMENT)	PROPOSED BITUMINOUS CONCRETE THICKNESS	BITUMINOUS SURFACE REMOVAL - BUTT JOINT	PAVEMENT REMOVAL	PREPARATION OF BASE	AGGREGATE BASE REPAIR	AGGREGATE BASE COURSE TYPE B	AGGREGATE FOR TEMP ACCESS	BITUMINOUS MATERIALS (PRIME COAT)	AGGREGATE (PRIME COAT)	INCIDENTAL BITUMINOUS SURFACING	P. C. C. DRIVEWAY PAVEMENT 6"	P. C. C. DRIVEWAY PAVEMENT 8"
(LT / RT) (STA) ( + )	(FE / PE / CE / MB) (RURAL / URBAN)	(EARTH / AGG. / BIT. / P. C. C.)	FOOT	FOOT	FOOT	FOOT	INCH	SQ. YD.	SQ. YD.	SQ. YD.	TON	TON	TON	TON	TON	TON	SQ. YD.	SQ. YD.
RT STA 297+07.11	PE	BIT	27	NA	NA	15	3.5	70	NA	70	NA	25	16	0.1	1	6	NA	NA
RT STA 299+27.91	PE	AGG	15			9				25		8	6					
LT STA 299+91.04	FE	AGG	17			20				44		14	10					
RT STA 301+32.44	FE	AGG	12			16				28		10	6					
RT STA 301+82.01	FE	AGG	16			10				26		8	6					
RT STA 303+29.24	FE	AGG	16			27				64		23	14					
<b>TOTAL</b>								70	NA	257	NA	88	58*	0.1	1	6	NA	NA

\*ESTIMATED QUANTITY FOR ONE APPLICATION 4" IN DEPTH FOR EACH ENTRANCE

**BITUMINOUS MATERIALS**

STATION TO STATION	SIDE	BIT. CONC. BASE CSE. WIDENING SUPERPAVE 10 IN.	LEVELING BINDER (MACHINE METHOD) SUPERPAVE N50	BIT. CONC. SURF. CSE. SUPERPAVE, MIX D, N50, 1 1/2"	BITUMINOUS SHOULDERS SUPERPAVE	BITUMINOUS MATERIALS (PRIME COAT)	AGGREGATE (PRIME COAT)
		SQ. YD.	TON	TON	TON	TON	TON
296+25.00 TO 304+00.00				188		1.70	9.0
297+75.00 TO 303+50.00	LT	379			53		
296+75.00 TO 303+50.00	RT	392			55		
296+75.00 TO 303+50.00			109				
<b>TOTAL</b>		771	109	188	108	1.7	9.0

**TEMPORARY CONCRETE BARRIER**

STATION	STATION	TEMPORARY CONCRETE BARRIER (FEET)	RELOCATE TEMPORARY CONCRETE BARRIER (FEET)
298+37.00	302+62.00	425	
298+62.50	302+37.50		375
<b>TOTAL</b>		425	375

**EROSION CONTROL**

STATION TO STATION	SIDE	PERIMETER EROSION BARRIER	TEMP. EROSION CONTROL SEEDING	AGGREGATE (EROSION CONTROL)
		FOOT	ACRE	TON
296+25.00 TO 304+00.00	LT		0.11	6
296+25.00 TO 304+00.00	RT		0.27	3
296+63.83 TO 299+82.28	RT	325		
299+98.52 TO 300+39.11	RT	61		
300+60.91 TO 303+08.56	RT	260		
<b>TOTAL</b>		646	200*	9

\*ESTIMATED QUANTITY PROVIDED BY IDOT

**SEEDING, FERTILIZER, & MULCH**

STATION TO STATION	SIDE	SEEDING, CLASS 2	NITROGEN FERTILIZER NUTRIENT	PHOSPHORUS FERTILIZER NUTRIENT	POTASSIUM FERTILIZER NUTRIENT	AGRICULTURAL GROUND LIMESTONE	MULCH METHOD 2
		ACRE	POUND	POUND	POUND	TON	ACRE
296+25.00 TO 304+00.00	LT	0.25	11	11	11		0.25
296+25.00 TO 304+00.00	RT	0.25	12	12	12	0.1	0.25
<b>TOTAL</b>		0.50	23	23	23	0.1	0.50
<b>USE</b>		0.50	35*	35*	35*	0.8*	0.50

\*ESTIMATED QUANTITY PROVIDED BY IDOT

**RIPRAP**

STATION	STATION	SIDE	STONE DUMPED RIPRAP CL. A4	FILTER FABRIC FOR RIPRAP
			TON	SQ YD
299+40.00	300+16.60	LT	43	86
300+16.60	300+35.00	LT	17	34
300+25.00	300+40.00	RT	10	20
300+40.00	300+60.00	RT	23	45
300+60.00	300+75.00	RT	10	20
300+35.00	300+65.00	LT	32	65
300+65.00	300+85.00	LT	11	21
<b>TOTAL</b>			146	291

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

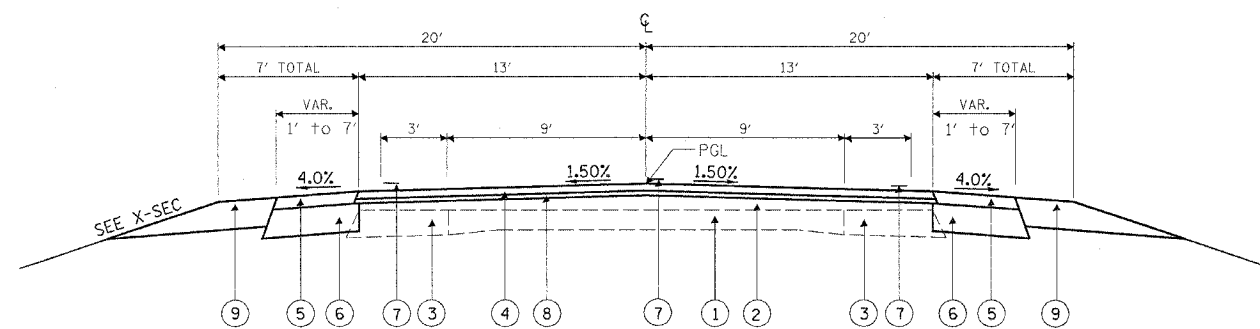
**SCHEDULE OF QUANTITIES**  
F.A.P. ROUTE 562 (IL 100)  
SCOTT COUNTY

SCALE: VERT. NONE  
HORIZ. NONE  
DATE 04/27/2005

DRAWN BY AJP  
CHECKED BY JDJ



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	6
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
STATE CONTRACT NO. 72818				







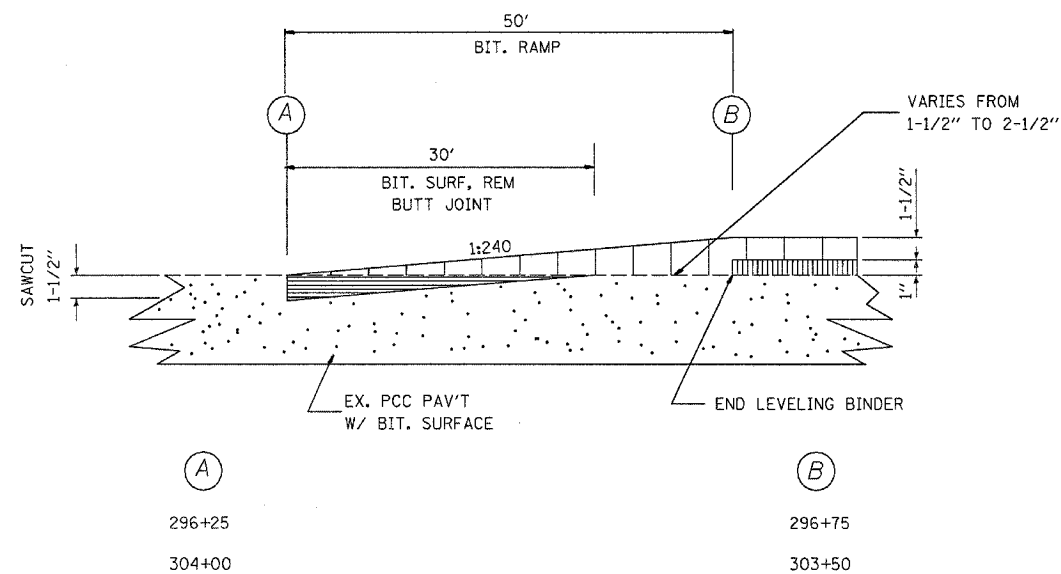
FAP ROUTE 562 (IL 100)  
STA 296+75.00 TO STA 303+50.00

**TYPICAL SECTION LEGEND**

- ① EXISTING 9-6-9 PCC PAVEMENT
- ② EXISTING BITUMINOUS SURFACE, 5" AVG.
- ③ EXISTING BITUMINOUS WIDENING
- ④ PROPOSED BIT. CONC. SURF. CSE, SUPERPAVE, MIX D, N-50, 1-1/2"
- ⑤ PROPOSED BITUMINOUS SHOULDERS SUPERPAVE
- ⑥ PROPOSED BITUMINOUS CONCRETE BASE CSE WIDENING SUPERPAVE, 10"
- ⑦ PROPOSED PAINT PAVEMENT MARKING - LINE 5"
- ⑧ PROPOSED LEVELING BINDER SUPERPAVE, 1"
- ⑨ PROPOSED AGGREGATE SHOULDERS, TYPE B

**BUTT JOINT LEGEND**

-  BITUMINOUS SURFACE REMOVAL - BUTT JOINT
-  PROPOSED BIT. CONC. SURF. CSE, SUPERPAVE, MIX D, N-50, 1-1/2"
-  PROPOSED LEVELING BINDER SUPERPAVE, 1"
-  EXISTING BITUMINOUS CONCRETE PAVEMENT OR OVERLAY



BUTT JOINT DETAIL

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		<b>TYPICAL SECTION &amp; BUTT JOINT DETAIL</b> F.A.P. ROUTE 562 (IL 100) SCOTT COUNTY SCALE: VERT. NONE HORIZ. NONE DATE 04/27/2005 DRAWN BY AJP CHECKED BY JDJ

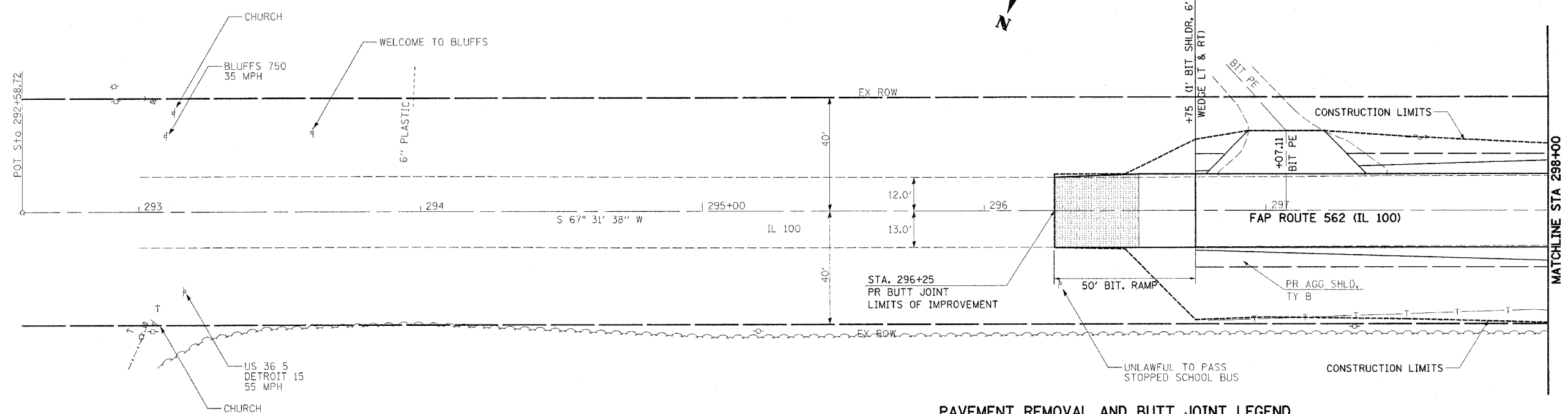
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	7
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FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	STATE CONTRACT NO. 72818	

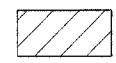

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	PLANNED	
	CHECKED	
	BY	
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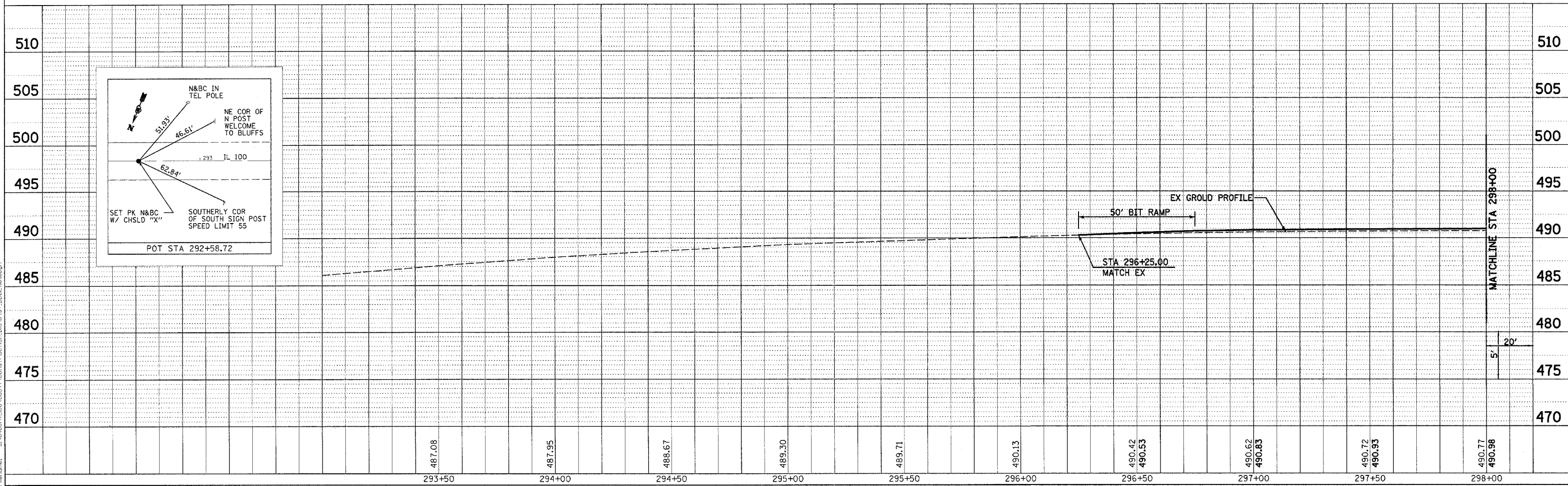
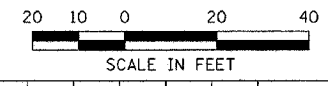
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	PLANNED	
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	BY	
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**PAVEMENT REMOVAL AND BUTT JOINT LEGEND**

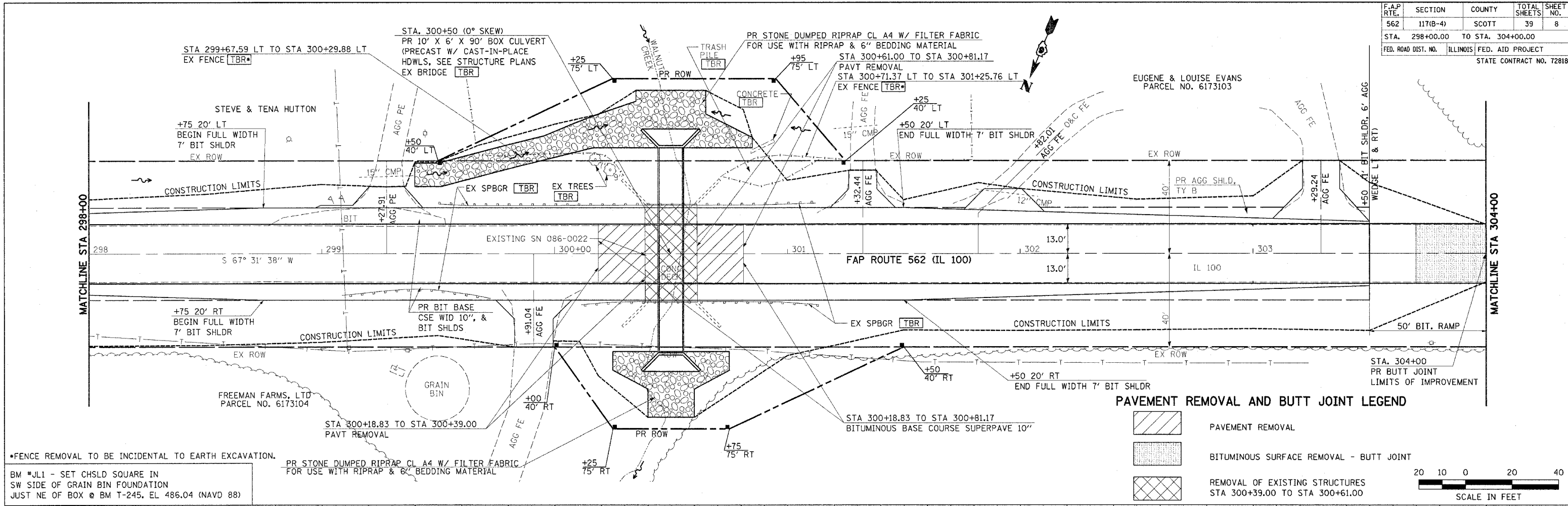
-  PAVEMENT REMOVAL
-  BITUMINOUS SURFACE REMOVAL - BUTT JOINT



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	8
STA. 298+00.00		TO STA. 304+00.00		
FED. ROAD DIST. NO.		ILLINOIS		FED. AID PROJECT
STATE CONTRACT NO. 72818				

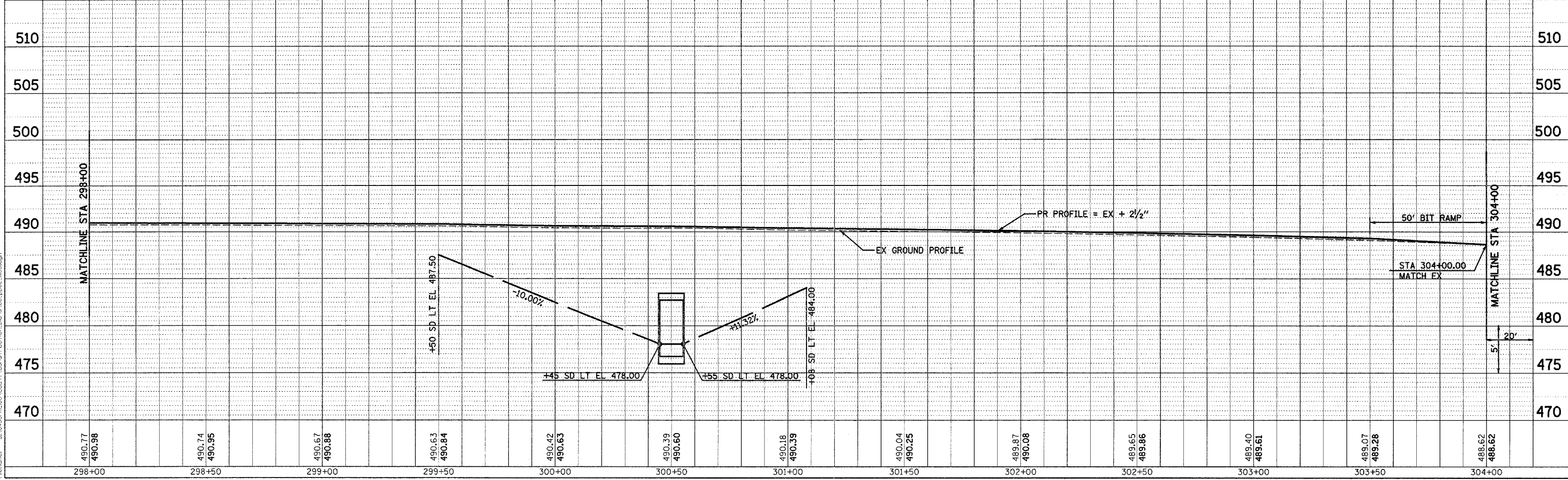
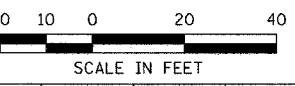
PLAN	DATE	BY

PROFILE	DATE	BY



**PAVEMENT REMOVAL AND BUTT JOINT LEGEND**

- PAVEMENT REMOVAL
- BITUMINOUS SURFACE REMOVAL - BUTT JOINT
- REMOVAL OF EXISTING STRUCTURES STA 300+39.00 TO STA 300+61.00



\*FENCE REMOVAL TO BE INCIDENTAL TO EARTH EXCAVATION.  
 BM \*JL1 - SET CHSLD SQUARE IN SW SIDE OF GRAIN BIN FOUNDATION JUST NE OF BOX @ BM T-245, EL 486.04 (NAVD 88)

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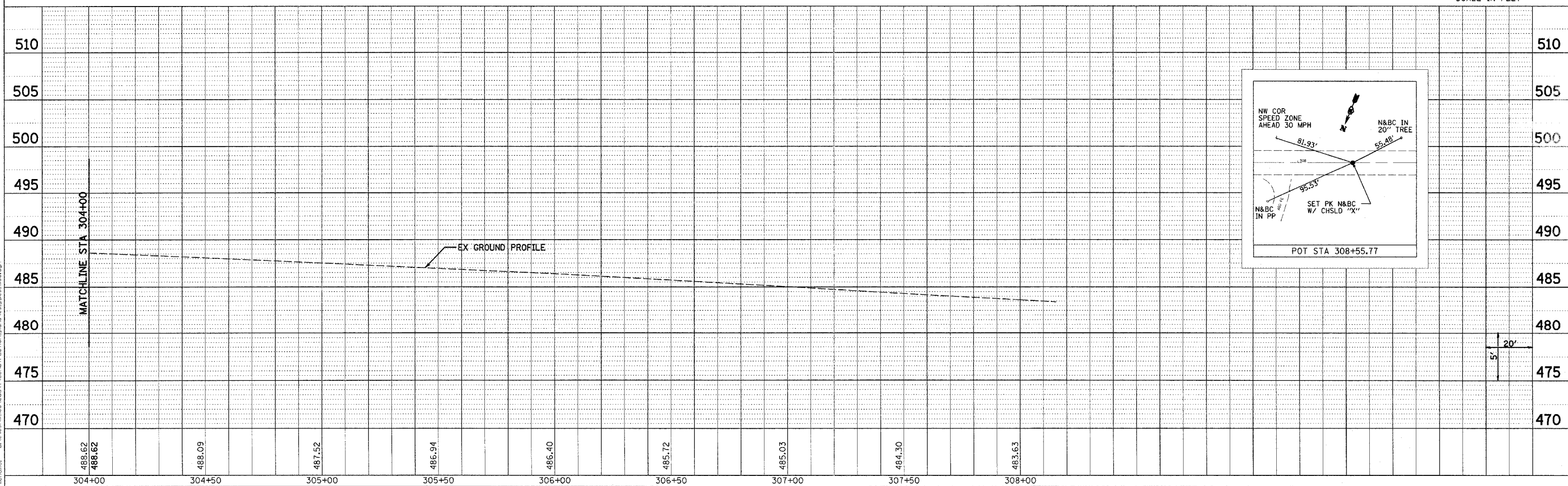
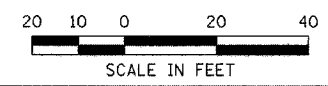
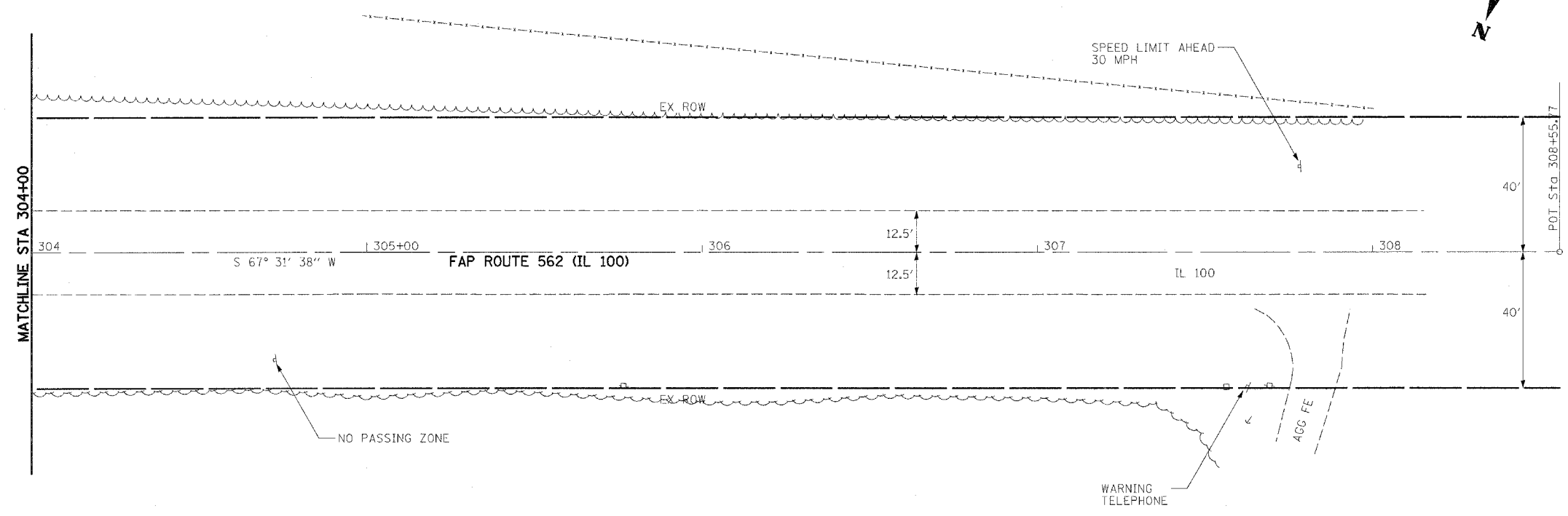


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	9
STA.	304+00.00	TO STA.	308+55.77	
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
STATE CONTRACT NO. 72818				

PLAN	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO. OF WAY CHECKED	
	CADD FILE NAME	

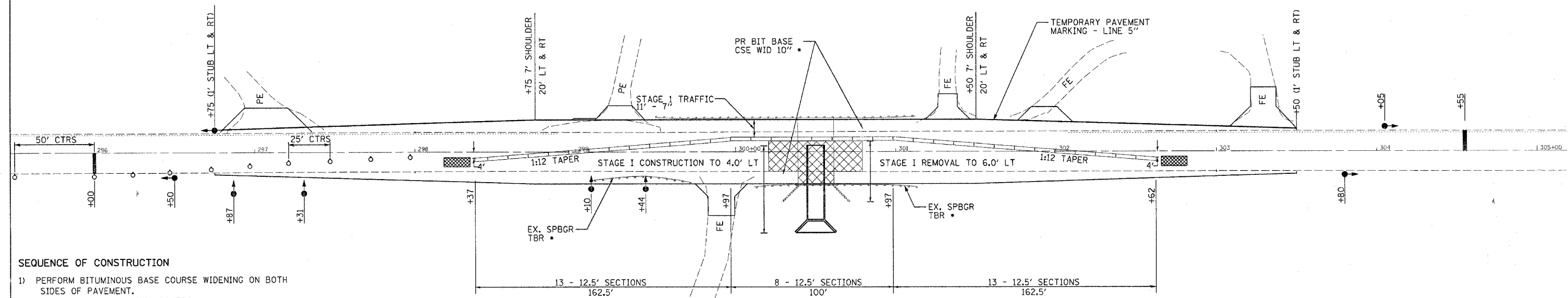
PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	BY	
	NO. OF WAY CHECKED	
	STRUCTURE NOTATIONS CHKD	

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 Pen Table: dcr.tbl  
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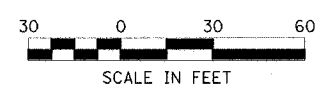
FAP 562 (IL 100) PLAN & PROFILE STA 304+00.00 TO 308+55.77

### STAGE I



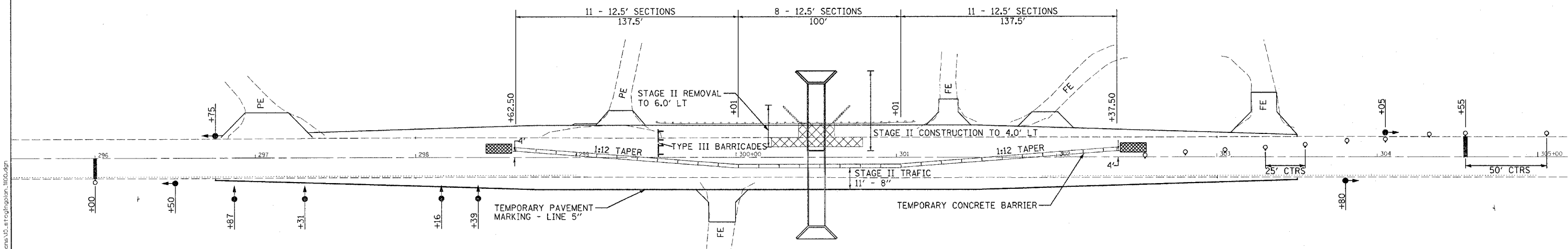
- SEQUENCE OF CONSTRUCTION**
- 1) PERFORM BITUMINOUS BASE COURSE WIDENING ON BOTH SIDES OF PAVEMENT.
  - 2) SET UP STAGE I TRAFFIC CONTROL.
  - 3) REMOVE RIGHT SIDE OF EXISTING STRUCTURE.
  - 4) CONSTRUCT RIGHT SIDE OF NEW BOX CULVERT.
  - 5) PLACE BITUMINOUS BASE COURSE, SUPERPAVE 10" ON RIGHT SIDE.
  - 6) CHANGE TRAFFIC CONTROL TO STAGE II.
  - 7) REMOVE LEFT SIDE OF EXISTING STRUCTURE.
  - 8) CONSTRUCT LEFT SIDE OF NEW BOX CULVERT.
  - 9) PLACE BITUMINOUS BASE COURSE, SUPERPAVE 10" ON LEFT SIDE.
  - 10) REMOVE TRAFFIC CONTROL (BARRIER).
  - 11) COMPLETE RESURFACING.

SEQUENCE OF OPERATIONS												
PHASE	1	2	3	4	5	6	7	8	9	10	11	12
WESTBOUND	G	Y	R	R	R	R	R	R	R	R	R	R
PE 297+07.11	R	R	R	G	Y	R	R	R	R	R	R	R
PE 299+27.91	R	R	R	R	R	R	G	Y	R	R	R	R
EASTBOUND	R	R	R	R	R	R	R	R	R	G	Y	R



• ITEMS TO BE COMPLETED PRIOR TO STAGE I

### STAGE II



ADVANCE WARNING SIGNS, TEMPORARY RUMBLE STRIPS, VERTICAL PANELS, PAVEMENT MARKERS, AND BARRIER WALL REFLECTORS SHALL BE LOCATED IN ACCORDANCE WITH TRAFFIC CONTROL AND PROTECTION STANDARD 701321.

SEE SPECIAL PROVISIONS, STAGING TYPICALS, AND HIGHWAY STANDARD 701321 FOR MORE INFORMATION

IMPACT ATTENUATORS SHALL CONFORM TO:  
BDE MEMO 34-04 "IMPACT ATTENUATORS, (CRASH CUSHIONS)"

TYPE III BARRICADES WILL BE INCIDENTAL TO TRAFFIC CONTROL AND PROTECTION, STANDARD 701321, SPECIAL

REVISIONS	
NAME	DATE

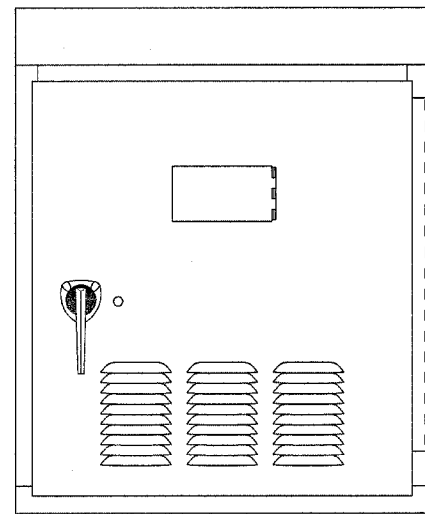
ILLINOIS DEPARTMENT OF TRANSPORTATION

### STAGING DETAILS

F.A.P. ROUTE 562 (IL 100)  
SCOTT COUNTY

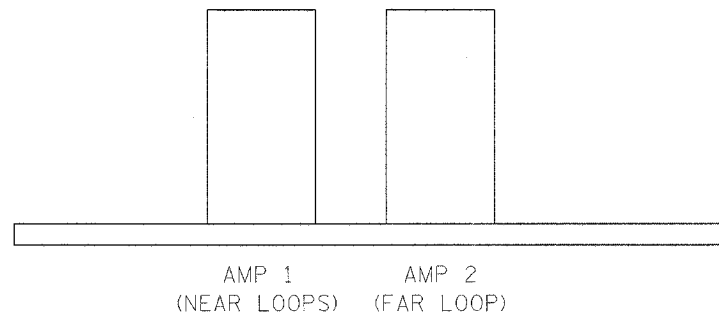
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HORIZ. 1"=30'  
DATE 04/27/2005

DRAWN BY AJP  
CHECKED BY JDJ



Temporary Controller Cabinet

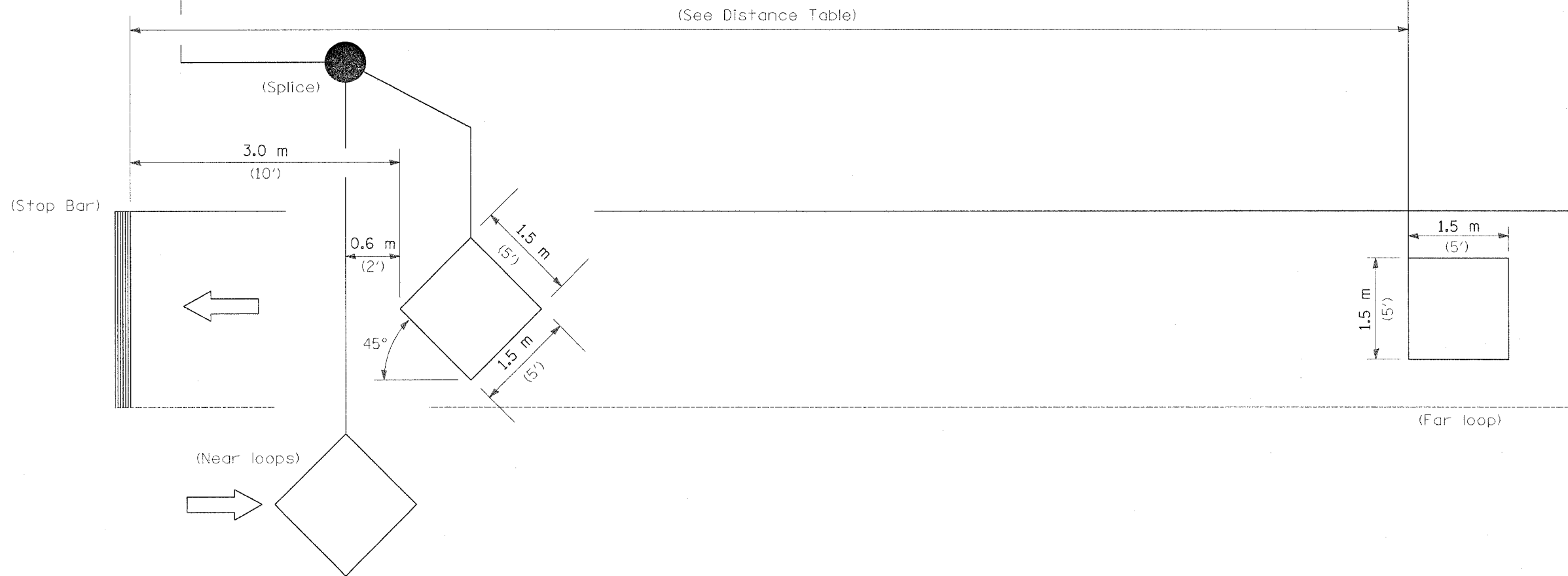
DETECTOR AMPLIFIER NOTES



AMP 1: DELAY = 8 SECONDS  
 DELAY SHALL BE INHIBITED DURING GREEN

AMP 2: NO DELAY

ADVISORY SPEED (MPH)	DISTANCE FROM STOP BAR (FT.)
30 OR LESS	220
35	260
40	300
45	330
50	370
55	400



NOTE: All loops centered in lane.

INDUCTION LOOP DETECTOR

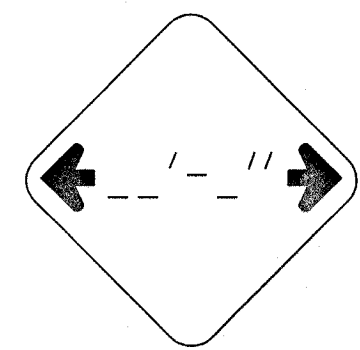
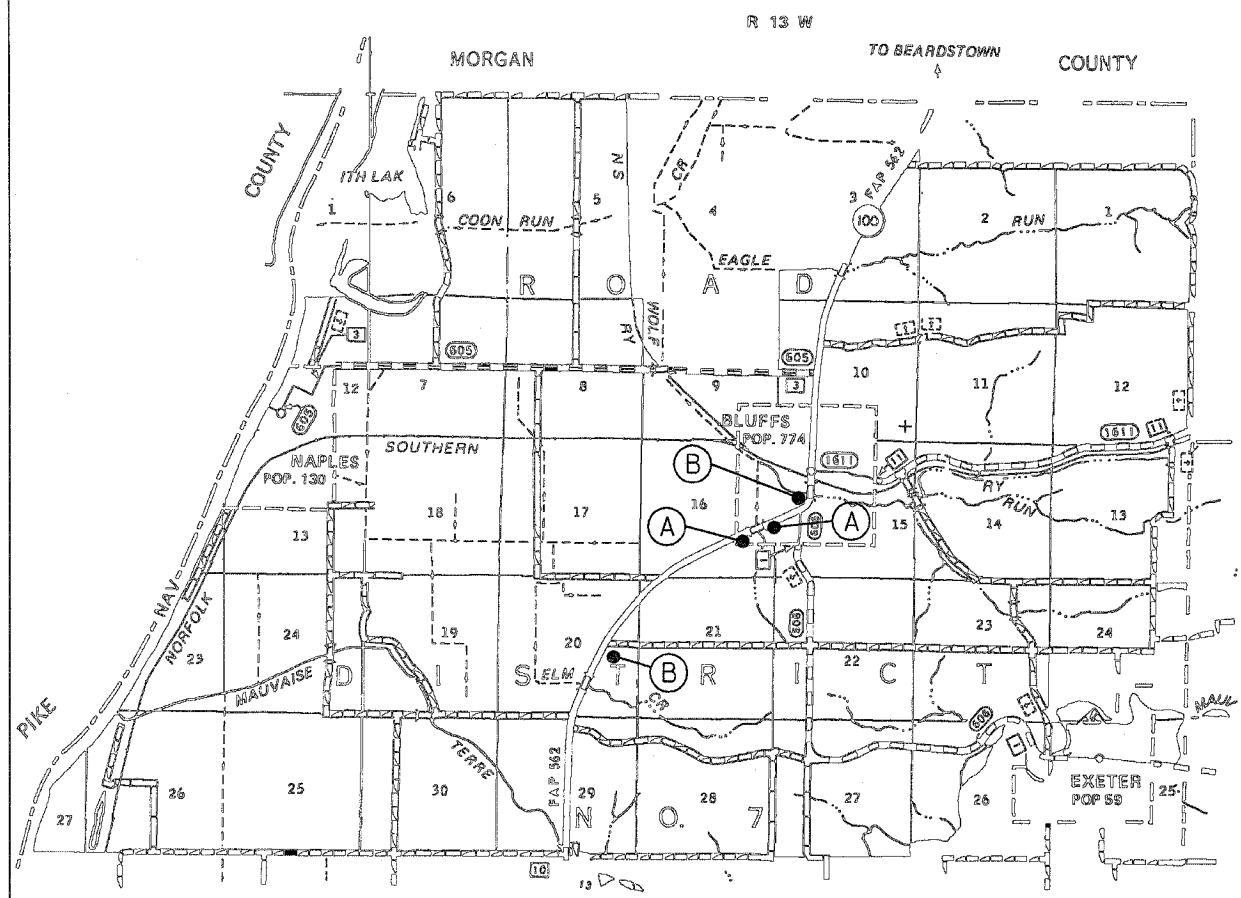
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**TEMPORARY BRIDGE TRAFFIC SIGNAL  
 LOOP PLACEMENT DETAIL SHEET**

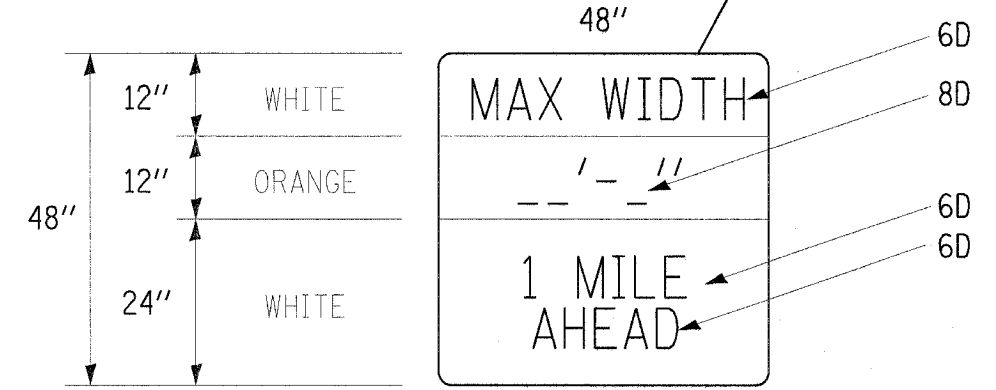
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 HORIZ. NONE  
 DATE 06/16/04

DRAWN BY KDA  
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	12
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
STATE CONTRACT NO. 72818				



W12-2(O)-48  
SIGN A



SIGN PANEL, TYPE II  
SIGN B

SIGN A - 2 SIGNS - (W12-2(O)-48) ARE TO BE PLACED AT EACH END OF THE PROJECT JUST BEFORE SIGNING FOR TC&P STANDARD 702321 SIGNING.

SIGN B - (SIGN PANEL, TYPE II AS SHOWN) ARE TO BE PLACED AT THE JUNCTION OF IL 100 SB AND PEARL ST. AND AT THE JUNCTION OF IL 100 NB AND BANGERT RD. ONE AT EACH LOCATION.

THESE SIGNS SHALL BE INSTALLED WITH FLAGS  
WILL BE PAID FOR AS PER LUMP SUM

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**WIDTH RESTRICTION SIGNAGE**  
F.A.P. 310 (US 67)  
MORGAN COUNTY  
SCALE: VERT. NONE  
HORIZ. NONE  
DATE 04/07/05  
DRAWN BY AJP  
CHECKED BY SRD

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	13
STA. 297+50.00	TO STA. 303+50.00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
STATE CONTRACT NO. 72818				

STORM WATER POLLUTION PREVENTION PLAN

Route: F.A.P. ROUTE 562      Marked: IL 100  
 Section: 117 (B-4)      Project No.:  
 County: Scott      Contract No.: 72818

This plan has been prepared to comply with the provision of the NPDES Permit Number ILR10 \_\_\_\_\_ Issued by the Illinois Environmental Protection Agency for storm water discharges from construction site activities.

I certify under penalty of law that this document and all attachments were prepared under my direction or supervision in accordance with a system designed to assure that qualified personnel gathered and evaluated the information submitted. Based on my inquiry of the person or persons who manage the system, or those persons directly responsible for gathering the information submitted, is, to the best of my knowledge and belief, true, accurate, and complete. I am aware that there are significant penalties for submitting false information, including the possibility of fine and imprisonment for knowing violations.

\_\_\_\_\_  
 (Signature)      \_\_\_\_\_  
 (Date)

\_\_\_\_\_  
 (Title)

Note: The above boxed in area will be filled out by IDOT - Construction after the award of the contract to obtain the required NPDES permit.

The following plan was established and included in these plans to direct the Contractor in the placement of temporary erosion control systems and to provide a storm water pollution prevention plan for compliance under NPDES. The Contractor shall abide to all requirements within this plan as part of the contract.

The purpose of this plan is to prevent / minimize siltation within the construction zone and to eliminate sediments from entering and leaving the construction zone by utilizing proper temporary erosion control systems and providing ground cover within a reasonable time.

Certain items, as shown in this plan and referenced by the legend, shall be placed by the Contractor at the beginning of construction. Other items shall be placed by the Contractor as directed by the Engineer on a case by case situation resulting from the Contractor's sequence of activities, time of the year, and expected weather conditions.

The Contractor shall place permanent erosion control systems and seeding within a reasonable amount of time; therefore, reducing the amount of area being open to the possibility of erosion and reducing the amount of temporary erosion control systems and temporary seeding. The Resident Engineer will determine if temporary erosion control systems shown in the plan can be deleted, the size of the proposed ditch checks, the proper method of installation, and if any additional temporary erosion control systems shall be added which are not included in this plan. The Contractor shall perform all work as directed by the Engineer and as shown in special details and in Standard 280001 of the plans.

The special provisions Temporary Seeding, Temporary Erosion Control Seeding, and Temporary Erosion Control additionally supplement this plan.

All disturbed areas having high potential for erosion, as determined by the Engineer, shall be temporarily seeded or permanently seeded by October 1 of the year of construction and shall not be reopened until after the winter shutdown period.

SITE DESCRIPTION

Description of Construction Activity:

1. The proposed project consists of the replacement of SN 086-0022 on IL 100 in Scott County.
2. Construction consists of structure removal and replacement, ditch grading, channel protection, entrance construction, widening and resurfacing.

Description of Intended Sequence of Major Construction Activities Which Will Disturb Earth and Lead to Possible Erosion for Major Portions of the Construction Site:

1. Tree removal will be completed to clear 2 trees (greater than 6 inches diameter).
2. Excavation will be completed along the entire length to grade out for proposed roadway ditches and waterways.
3. Embankment will be placed in fill areas to raise the existing ground elevation to meet the proposed roadway foreslope and backslope.
4. Drainage structures will be installed before and/or during the construction of the excavation and embankment to allow proper drainage across the proposed two lane facility.
5. Placement, maintenance, removal and proper clean-up of temporary erosion control, such as erosion control fence, hay or straw bale ditch checks, riprap ditch checks, sediment basins, temporary seeding, etc.
6. Placement of permanent erosion control, such as riprap ditch lining, riprap stilling basins, riprap dry dams, excelsior blanket, seeding, etc.
7. Final grading, paving and other miscellaneous items.

Area of Construction Site:

The total drainage area entering and including the construction site is estimated to be 282 acres in which 1.0 acres will be disturbed by excavation, grading or other activities.

Other Reports, Studies and Plans which Aid in the Development of this Storm Water Pollution Prevention Plan as Referenced Documents:

1. Estimated run-off coefficients are contained in the project drainage study which were utilized for proposed placement of the temporary erosion control systems.
2. Information on the soils within the site was obtained from field reviews which were utilized for proposed placement of the temporary erosion control systems.
3. Site maps indicating drainage patterns and approximate slopes were contained in the project design report, USGS drainage maps, project drainage study, and project plan documents were all utilized for proposed placement of the temporary erosion control systems.

Drainage Tributaries Receiving Water from this Construction Site:

1. Walnut Creek

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**STORM WATER POLLUTION PREVENTION PLAN**  
 F.A.P. ROUTE 562 (IL 100)  
 SCOTT COUNTY

SCALE: VERT. NONE  
 HORIZ. NONE  
 DATE 04/27/2005

DRAWN BY AJP  
 CHECKED BY JDJ

Plot Date: 4/27/2005  
 Plot Time: 2:02:47 PM  
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	15
STA. 297+50.00		TO STA. 303+50.00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
STATE CONTRACT NO. 72818				

**CONTRACTOR CERTIFICATION STATEMENT**

This certification statement is part of the Storm Water Pollution Plan for the project described below in accordance with NPDES Permit No. ILR10 \_\_\_\_\_, issued by the Illinois Environmental Protection Agency on \_\_\_\_\_.

Route: FAP ROUTE 562                      Marked: IL 100  
 Section: 117 (B-4)                      Project No.: \_\_\_\_\_  
 County: Scott                              Contract No.: 72818

I certify under penalty of law that I understand the terms of the general National Pollutant Discharge Elimination System (NPDES) permit that authorizes the storm water discharges associated with industrial activity from the construction site identified as part of this certification.

Signature \_\_\_\_\_ Date \_\_\_\_\_  
 Title \_\_\_\_\_  
 Name of Firm \_\_\_\_\_  
 Street Address \_\_\_\_\_  
 City, State, Zip \_\_\_\_\_  
 Phone Number \_\_\_\_\_

Note: The above boxed in area shall be filled out by the Contractor after the award of the contract to obtain the required NPDES Permit from IEPA. This is a requirement for this contract.

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 Plot Time: 10:01:54 AM  
 Plot By: c10111.cst  
 Pen Table: 1001.tbl  
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**SWPPLAN**

REVISIONS	
NAME	DATE

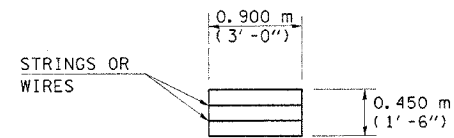
ILLINOIS DEPARTMENT OF TRANSPORTATION

**STORM WATER POLLUTION PREVENTION PLAN**

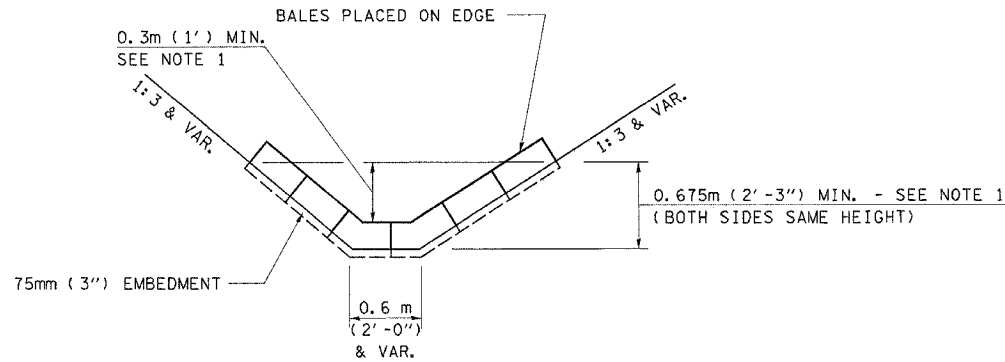
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SCOTT COUNTY

SCALE: VERT. NONE  
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 DATE 04/27/2005

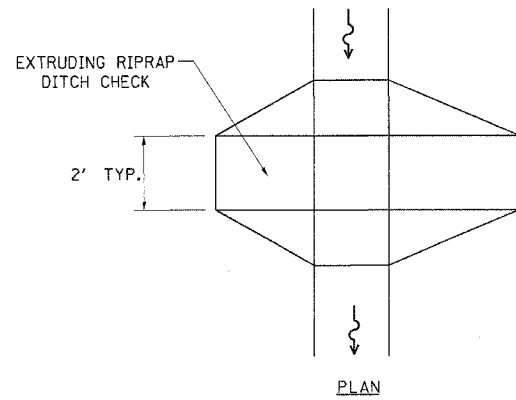
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 CHECKED BY: JDJ



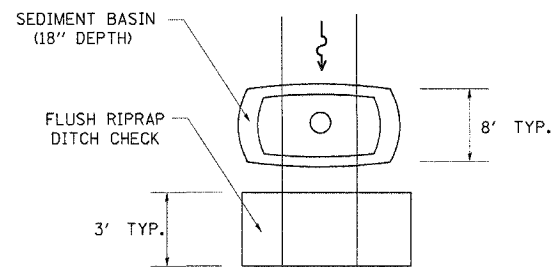
**HAY OR STRAW BALE**  
(TYPICAL ELEVATION)



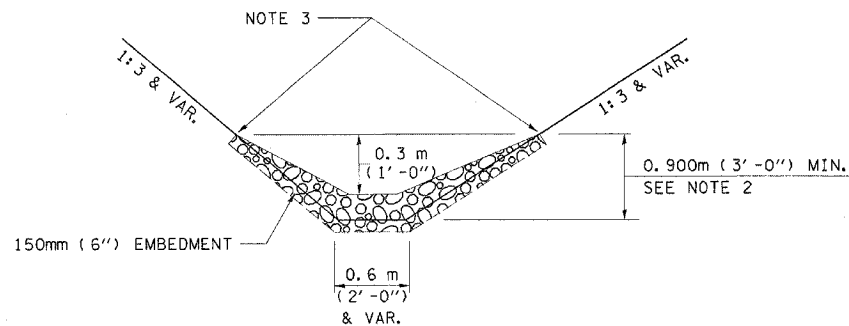
**HAY OR STRAW BALE TEMPORARY DITCH CHECK**  
(TYPICAL & SEE GENERAL NOTES FOR SUBSTITUTION TO FLUSH RIPRAP DITCH CHECK)



PLAN

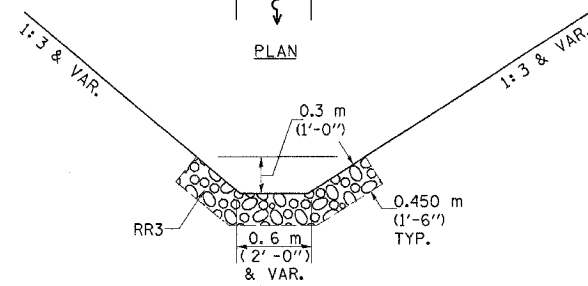


PLAN



ELEVATION

**OPTION 1**  
(EXTRUDING DITCH CHECK)  
RECOMMENDED FOR AREAS W/ RIPRAP DITCH LINING



ELEVATION

**OPTION 2**  
(FLUSH DITCH CHECK)  
RECOMMENDED FOR AREAS W/O RIPRAP DITCH LINING

**STONE DUMPED RIPRAP DITCH CHECK**  
(TYPICAL & OPTIONS 1 & 2 AS DIRECTED BY THE ENGINEER)

NOTE 1: BALES SHALL EXTEND FAR ENOUGH UP THE SLOPES TO ALLOW 0.3m (1') OVERTOPPING TO AVOID ERODING AROUND THE EDGES OF THE BALES.

NOTE 2: RIPRAP SHALL EXTEND FAR ENOUGH UP THE SLOPES TO ALLOW 0.3m (1') OVERTOPPING TO AVOID ERODING AROUND THE EDGES OF THE RIPRAP.

NOTE 3: ENDS SHALL BE TIED INTO SLOPES.

**LEGEND FOR STORM WATER POLLUTION PREVENTION PLAN**

ITEM	SYMBOL
AGGREGATE (EROSION CONTROL) [STONE DUMPED RIPRAP DITCH CHECKS: Height = 0.6m (2')] ]	
TEMPORARY DITCH CHECKS (HAY OR STRAW BALE DITCH CHECKS OR APPROVED SUBSTITUTION)	
INLET PIPE PROTECTION (I&PP) (HAY OR STRAW BALE DITCH CHECKS OR APPROVED SUBSTITUTION)	
EROSION CONTROL BARRIER	
EARTH EXCAVATION FOR EROSION CONTROL (SEDIMENT BASINS)	
PRESERVE EXISTING TREES, WOODLANDS, AND UNDERSTORY (OUTSIDE CONSTRUCTION LIMITS)	
ITEM PLACED AT BEGINNING OF CONSTRUCTION (Requirement)	
ITEM PLACED AS DIRECTED BY ENGINEER (When required by situation)	
DIRECTION OF OVERLAND FLOW	
EROSION CONTROL BLANKET	

**GENERAL NOTES:**

All items shall be constructed as shown on this sheet, on Standard 280001, and as directed by the Engineer.

The symbology on the STORM WATER POLLUTION PREVENTION PLAN sheets does not represent the size or quantity of bales, for number of bales refer to details and notes shown on this sheet and/or as directed by the Engineer.

**THE CONTRACTOR SHALL INSTALL DITCH CHECKS AS DIRECTED BY THE ENGINEER. IF THE ENGINEER ELECTS TO UTILIZE FLUSH RIPRAP DITCH CHECKS IN LIEU OF TEMPORARY DITCH CHECKS AS SHOWN ON THE FOLLOWING PLAN SHEETS, THE SPACING SHOULD BE DOUBLED.**

REVISIONS	
NAME	DATE
CAD Symbol	2AUG99

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**STORM WATER POLLUTION PREVENTION PLAN**  
F.A.P. ROUTE 562 (IL 100)  
SCOTT COUNTY  
SCALE: VERT. NONE  
HORIZ. NONE  
DATE 04/27/2005  
DRAWN BY AJP  
CHECKED BY JDJ



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	17
STA. 297+50.00 TO STA. 303+50.00				
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
STATE CONTRACT NO. 72818				

SEE THE SCHEDULE OF QUANTITIES FOR EROSION CONTROL SCHEDULES

EUGENE & LOUISE EVANS  
PARCEL NO. 6173103

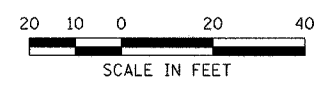
STEVE & TENA HUTTON

FREEMAN FARMS, LTD  
PARCEL NO. 6173104

MATCHLINE STA 298+00.00

MATCHLINE STA 298+00.00

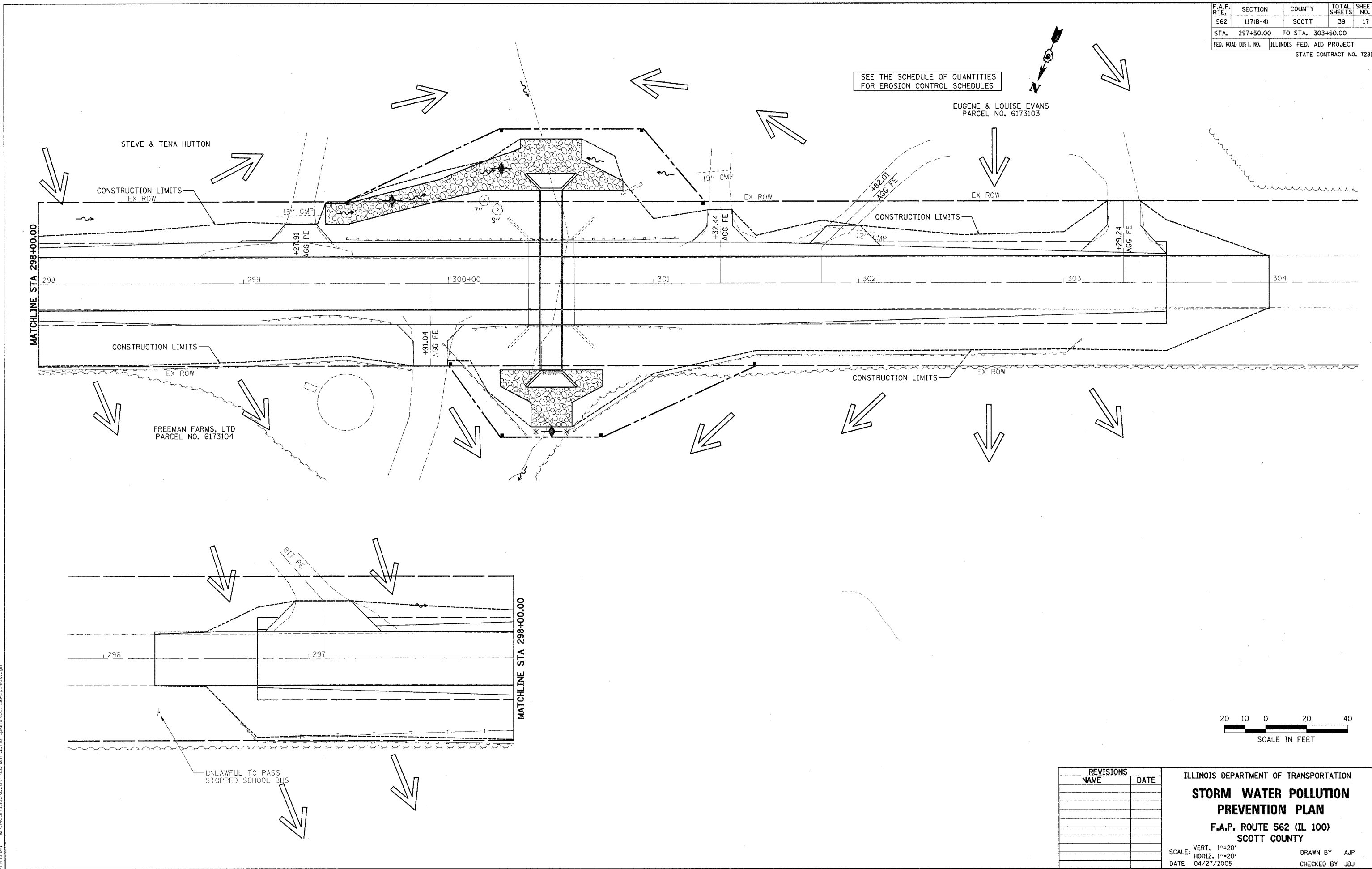
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STOPPED SCHOOL BUS



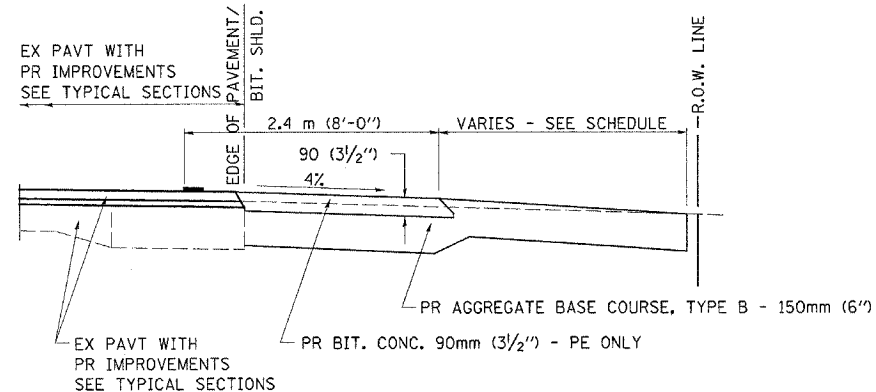
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**STORM WATER POLLUTION PREVENTION PLAN**  
F.A.P. ROUTE 562 (IL 100)  
SCOTT COUNTY  
SCALE: VERT. 1"=20'  
HORIZ. 1"=20'  
DATE 04/27/2005  
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CHECKED BY JDJ

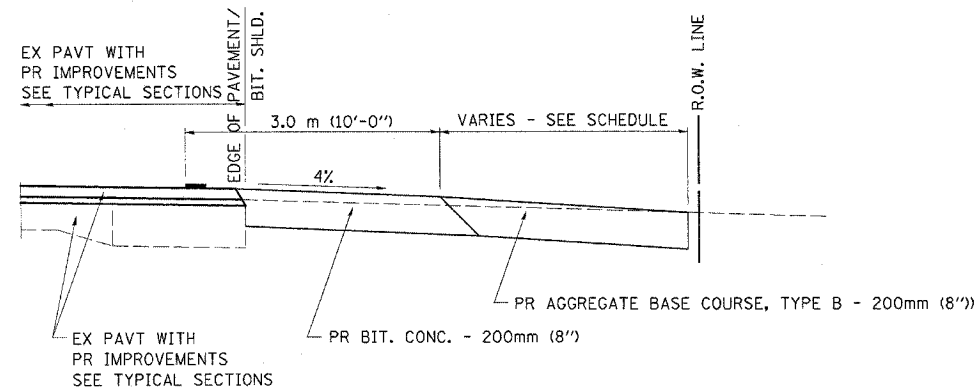
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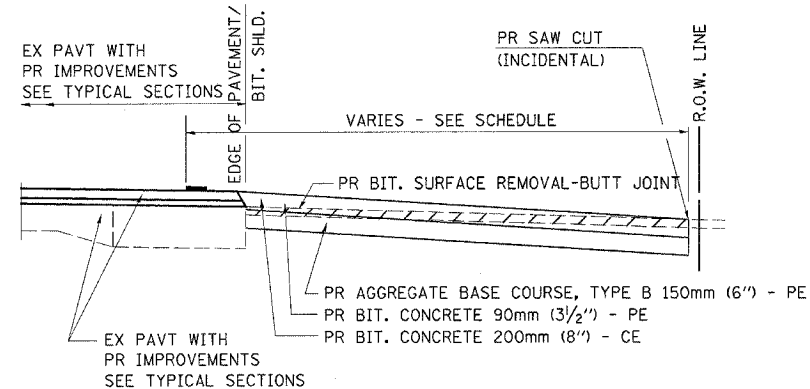
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562	11718-4)	SCOTT	39	18
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
STATE CONTRACT NO. 72818				



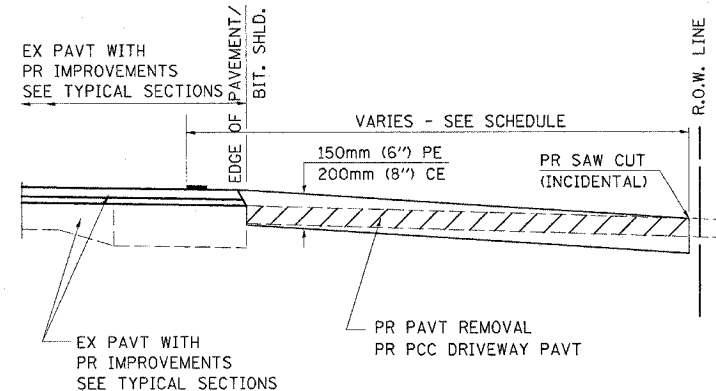
**SECTION A-A FOR EX EARTH/AGGREGATE FE & PE**



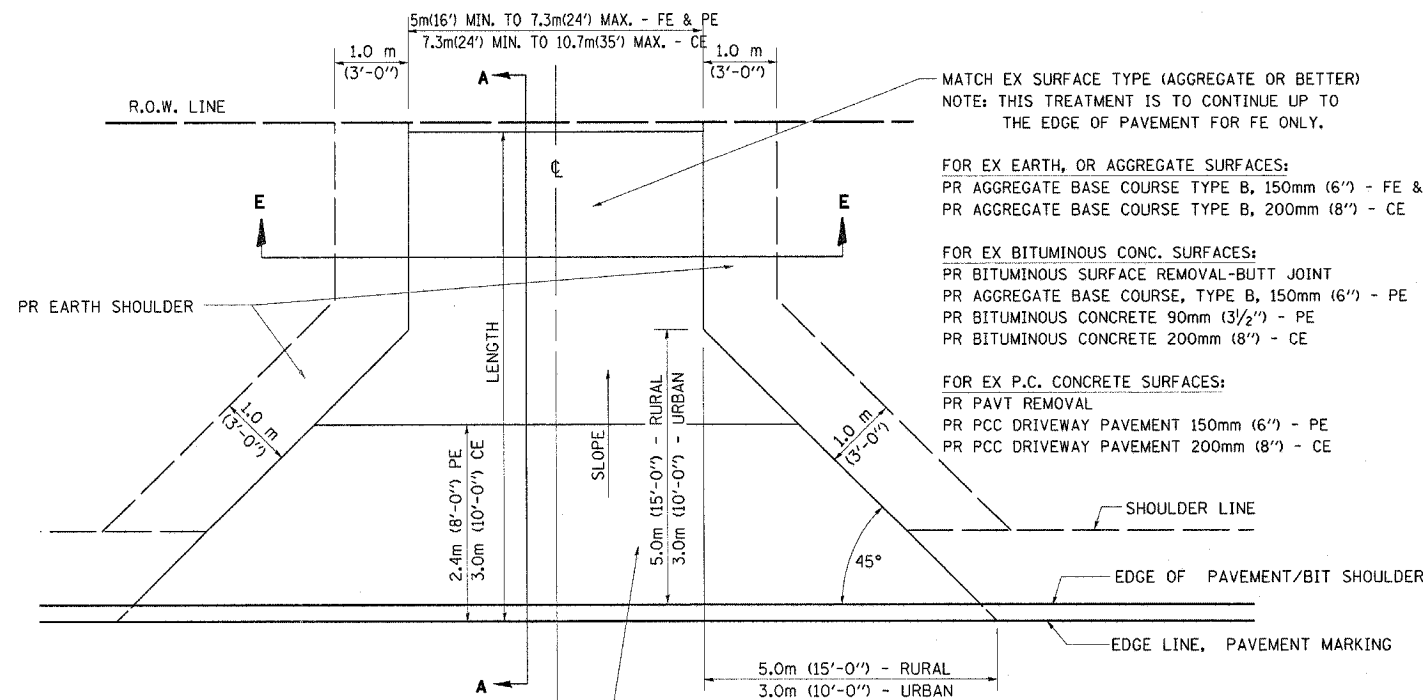
**SECTION A-A FOR EX EARTH/AGGREGATE CE**



**SECTION A-A FOR EX BITUMINOUS PE & CE**



**SECTION A-A FOR EX P.C. CONC. PE & CE**



FOR EX EARTH, AGGREGATE, OR BITUMINOUS CONC SURFACES:  
 PR BIT SURFACE REMOVAL-BUTT JOINT (IF APPLICABLE)  
 PR AGGREGATE BASE COURSE TYPE B 150mm (6") - FE  
 PR AGGREGATE BASE COURSE TYPE B, 150mm (6") &  
 PR BITUMINOUS CONCRETE 90mm (3 1/2") - PE  
 PR BITUMINOUS CONCRETE 200mm (8") - CE

FOR P.C. CONCRETE SURFACES:  
 PR PAVT REMOVAL  
 PR PCC DRIVEWAY PAVT 150mm (6") - PE  
 PR PCC DRIVEWAY PAVT 200mm (8") - CE

**GENERAL NOTES:**

THE RESIDENT ENGINEER WILL DETERMINE THE EXACT TYPE OF IMPROVEMENT TO BE COMPLETED FOR ALL ENTRANCES, SIDEROADS AND MAILBOX TURNOUTS ON THIS PROJECT.

THE PLAN DETAILS AND SCHEDULES SHOULD BE USED AS A GUIDE FOR THE ENGINEER TO IMPLEMENT THE FINAL DESIGN. THE ENGINEER MAY DECIDE TO SALVAGE PORTIONS OF THE EXISTING ENTRANCE PAVEMENT STRUCTURE; THEREFORE, REDUCING PAY ITEM QUANTITIES. NO ADDITIONAL PAYMENT WILL BE ALLOWED FOR THIS REDUCTION IN QUANTITIES.

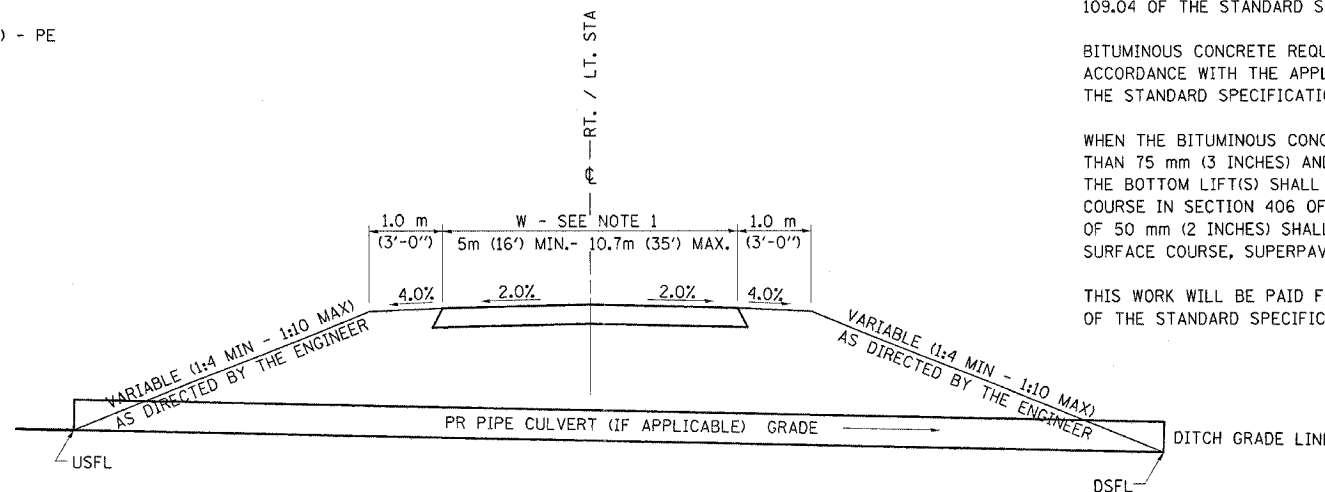
ANY WORK THE ENGINEER REQUIRES WHICH IS NOT COVERED BY A PAY ITEM CONTAINED IN THE PLANS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

BITUMINOUS CONCRETE REQUIRED TO CONSTRUCT THE ENTRANCES SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 406 AND 408 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.

WHEN THE BITUMINOUS CONCRETE PROPOSED FOR THE IMPROVEMENT IS THICKER THAN 75 mm (3 INCHES) AND REQUIRE PLACEMENT IN MORE THAN ONE LIFT. THE BOTTOM LIFT(S) SHALL MEET THE REQUIREMENTS OF BITUMINOUS BASE COURSE IN SECTION 406 OF THE STANDARD SPECIFICATIONS AND THE TOP LIFT OF 50 mm (2 INCHES) SHALL MEET THE REQUIREMENTS OF BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE.

THIS WORK WILL BE PAID FOR IN ACCORDANCE WITH SECTIONS 351, 358, 408, 423 AND 440 OF THE STANDARD SPECIFICATIONS.

ALL DIMENSIONS ARE IN MILLIMETERS ( INCHES ) UNLESS OTHERWISE SHOWN.



**SECTION E - E ENTRANCE TYPICAL SECTION**

NOTE 1: WIDTH OF ENTRANCE MAY BE INCREASED AT THE PIPE CULVERT DUE TO THE DITCHLINE BEING LOCATED IN THE ENTRANCE FLARE AREA.

REVISIONS	
NAME	DATE
JCN	2/19/03

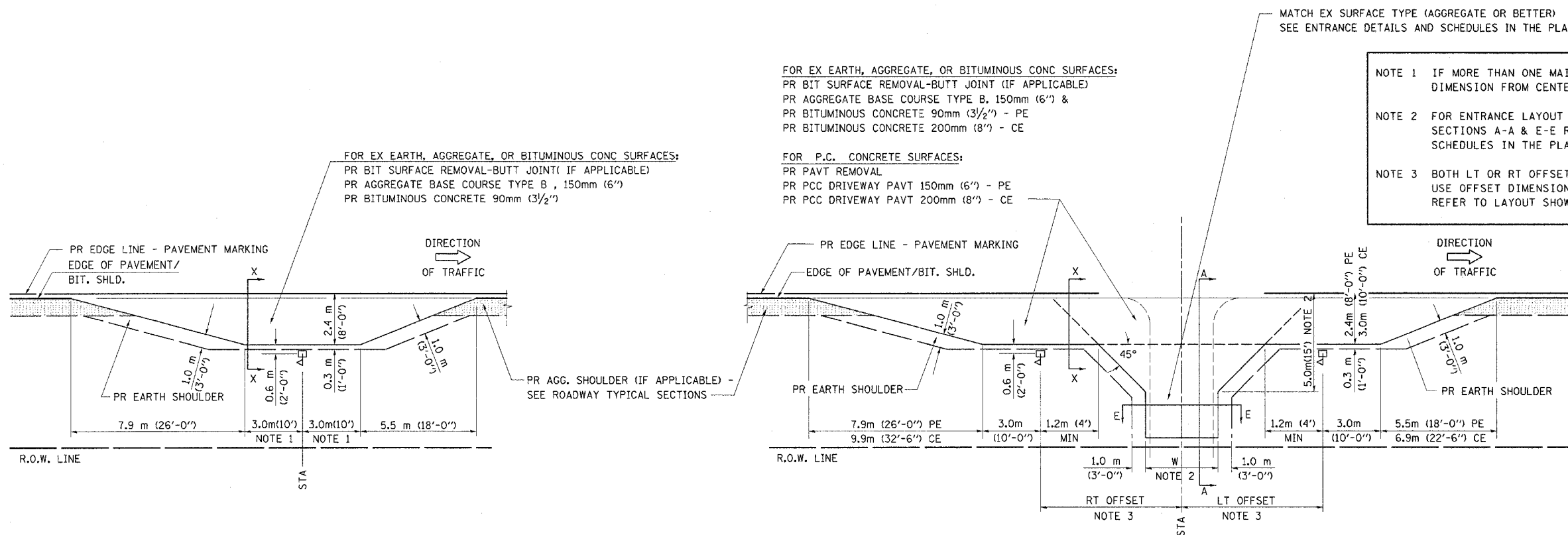
SHEET 1 OF 3  
 ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DISTRICT SIX**  
**DETAILS FOR RURAL / URBAN**  
**ENTRANCE & MAILBOX TURNOUT**  
**W / O CONCRETE GUTTER**  
**(3R - PROJECTS)**

SCALE: VERT. HORIZ.  
 DATE: FEBRUARY 23, 1999  
 DRAWN BY CADD  
 CHECKED BY JCN

Plot Date: 4/27/2005  
 Plot Time: 2:02:49 PM  
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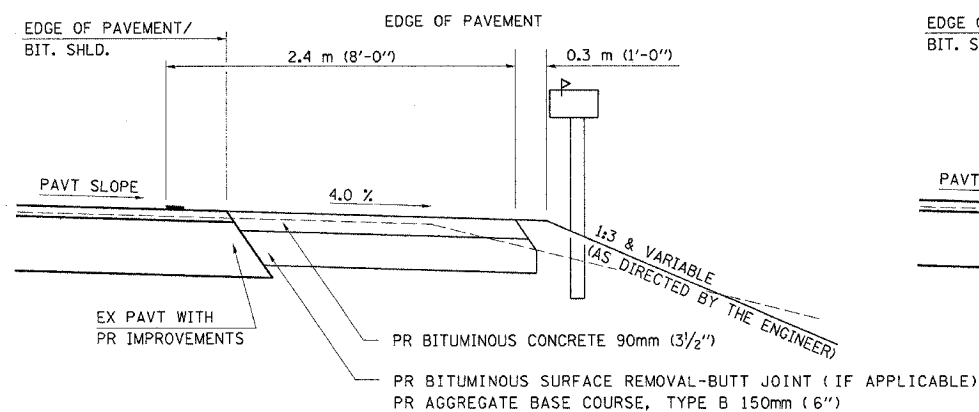
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	19
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
STATE CONTRACT NO. 72818				

**DETAILS OF MAILBOX TURNOUTS**

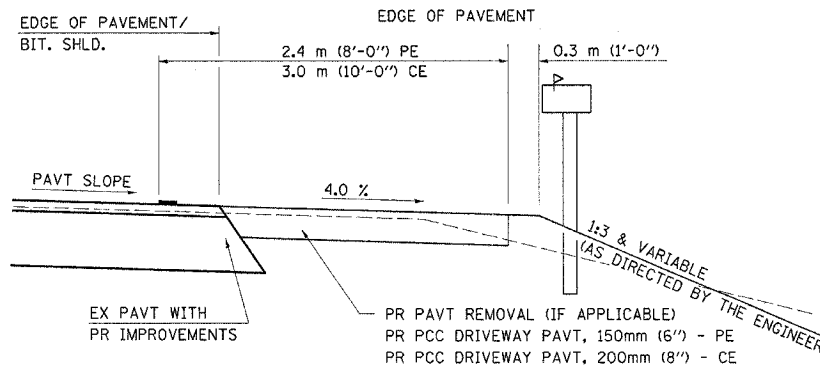


**PLAN - MAILBOX TURNOUTS**

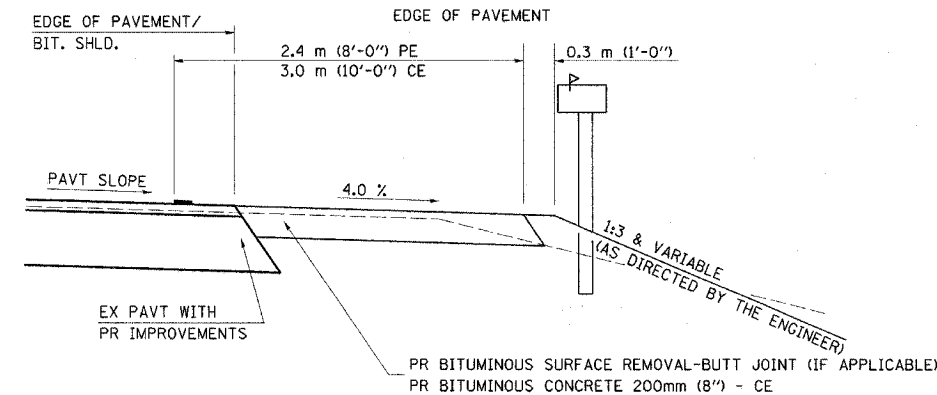
**PLAN - COMBINED MAILBOX TURNOUT WITH TRAILING OR LEADING ENTRANCE**



**SECTION X-X THRU MAILBOX TURNOUT  
 ALSO APPLIES TO MAILBOX TURNOUTS COMBINED WITH  
 EX EARTH, AGGREGATE, OR BITUMINOUS PE & FE**



**SECTION X-X THRU MAILBOX TURNOUT  
 COMBINED WITH EX CONC PE OR CE**



**SECTION X-X THRU MAILBOX TURNOUT  
 COMBINED WITH EX EARTH, AGGREGATE, OR BITUMINOUS CE**

Plot Date: 4/27/2005  
 Plot Time: 2:02:44 PM  
 Plot User: jcn  
 Pen Table: idb.tbl  
 File Name: s:\0402\1100\cadd\constr\function\plans\18\_18\_ent.dwg

ENT 3R

REVISIONS	
NAME	DATE
JCN	2/19/03

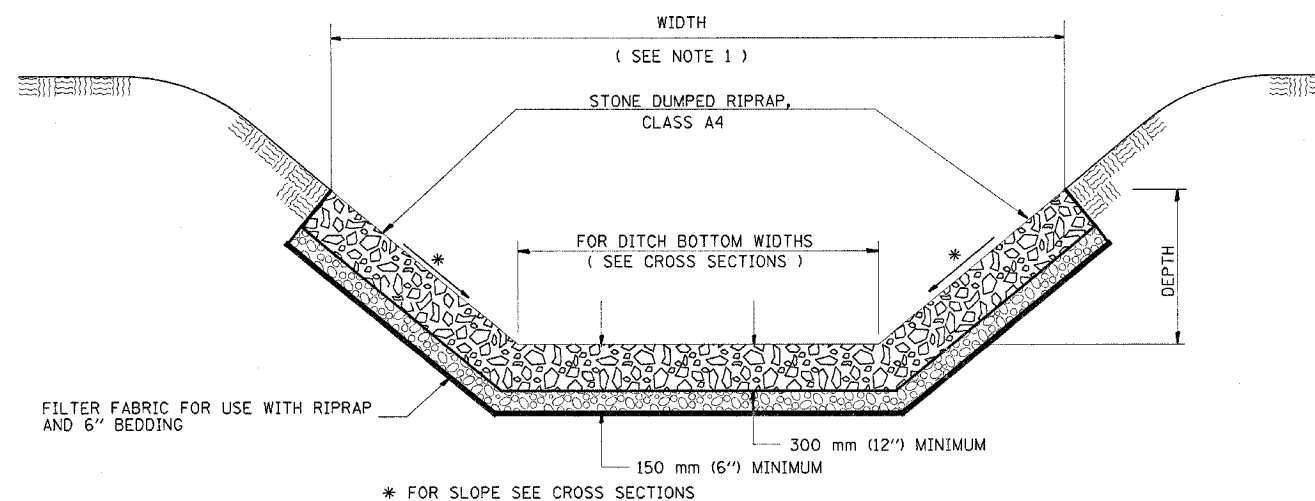
**SHEET 2 OF 3**

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DISTRICT SIX**  
**DETAILS FOR RURAL / URBAN**  
**ENTRANCE & MAILBOX TURNOUT**  
**W / O CONCRETE GUTTER**  
**(3R - PROJECTS)**

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_

DATE: FEBRUARY 23, 1999

DRAWN BY: CADD  
 CHECKED BY: JCN



**STONE DUMPED RIPRAP DITCH DETAIL**

LOCATION STATION TO STATION	LENGTH	WIDTH (SEE NOTE 1)	DEPTH	STONE DUMPED RIPRAP, CLASS A4	FILTER FABRIC FOR USE WITH RIPRAP
	(ft.)	(ft.)	(ft.)	(sq. ft.)	(sq. ft.)
299+40.00 TO 300+16.60 LT	77	10	1	770	770
300+16.60 TO 300+35.00 LT	18	VARIABLE	1	307	307
300+25.00 TO 300+40.00 RT	15	VARIABLE	1	180	180
300+40.00 TO 300+60.00 RT	20	28	1	409	409
300+60.00 TO 300+75.00 RT	15	VARIABLE	1	180	180
300+35.00 TO 300+65.00 LT	30	24.5	1	584	584
300+65.00 TO 300+85.00 LT	20	VARIABLE	1	190	190
<b>TOTALS</b>				<b>2620</b>	<b>2620</b>

NOTE 1: THE WIDTH SHOWN IS THE OUT TO OUT HORIZONTAL DIMENSION OF THE PROPOSED RIPRAP PLACEMENT. THE QUANTITY FOR THE STONE DUMPED RIPRAP AND FILTER FABRIC IS CALCULATED USING THE SUM OF THE DITCH BOTTOM WIDTH AND THE SLOPE DIMENSIONS OF THE FORESLOPE AND THE BACKSLOPE.

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 Plot Time: 2:02:51 PM  
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 Pen Table: ldot.tbl  
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**RIPRAP**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**DITCH  
RIPRAP DETAIL**

F.A.P. ROUTE 562 (IL 100)  
SCOTT COUNTY

SCALE: VERT. NONE  
HORIZ. NONE  
DATE 04/27/2005

DRAWN BY: AJP  
CHECKED BY: JDJ

Bench Mark: #JL1 - Set chiseled square in southwest side of grain bin foundation, just northeast of box at Bench Mark T-245, Elev. 486.04 (NAVD 88).

Existing Structure: S.N. 086-0022 built in 1923 as a single span bridge with a reinforced concrete deck slab supported by closed abutments on wood piles located at Sta. 300+50.00 at 0 degree skew. Back to back of abutments is 22'-0", width is 42'-2". Structure to be removed and new structure to be constructed in stages at Sta. 300+50.00. Traffic to be maintained utilizing stage construction.

The Precast Alternate requires cast-in-place headwalls.

No Salvage.

Note: Backfill to top of culvert with Granular Culvert Backfill. Removal and replacement of weak soils with Rockfill - Foundation may be required beneath the culvert. The Engineer will determine the required depth of improvement following excavation to plan grade.

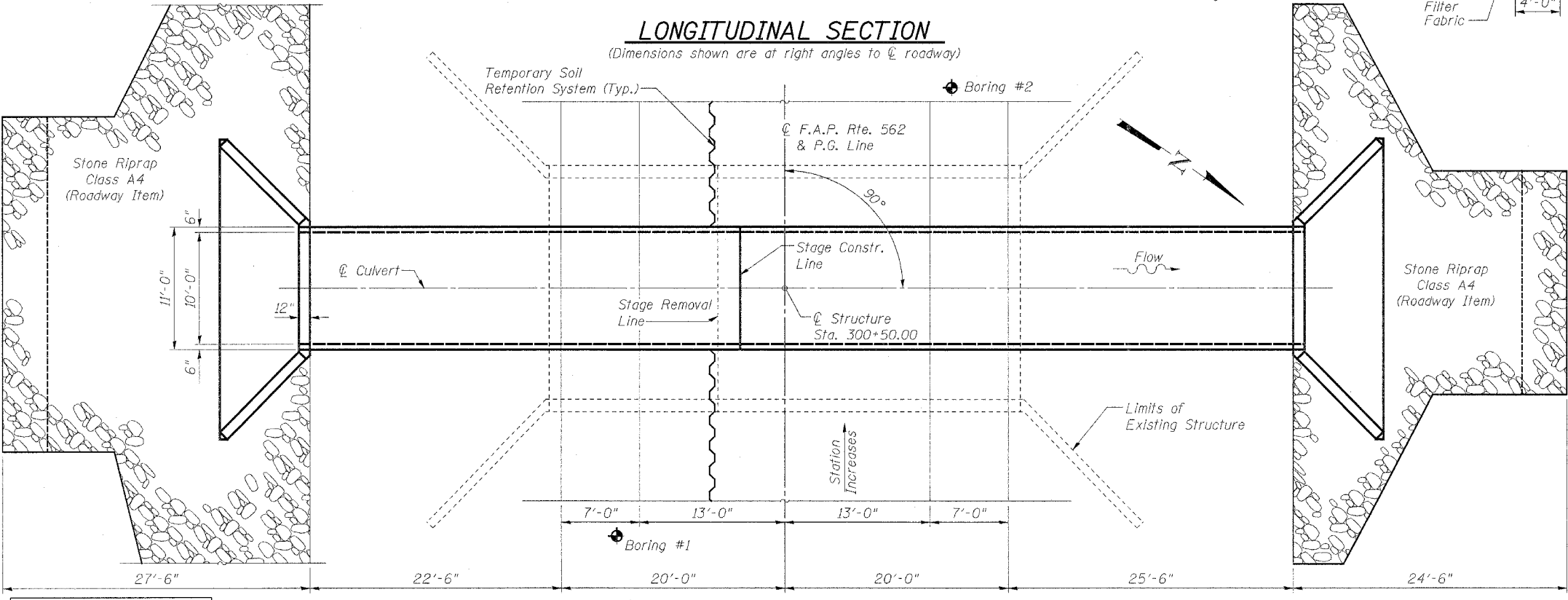
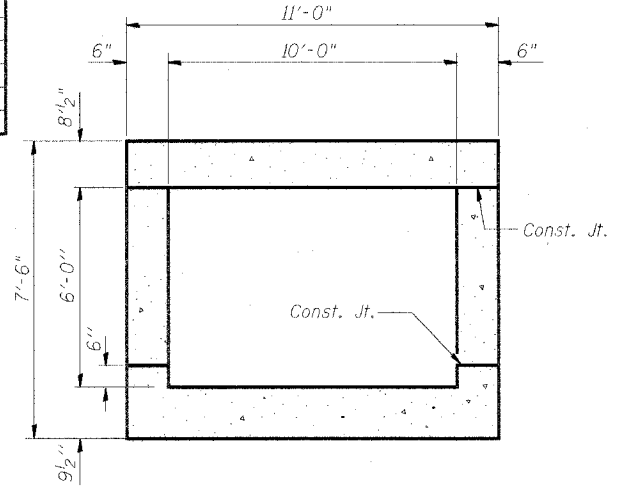
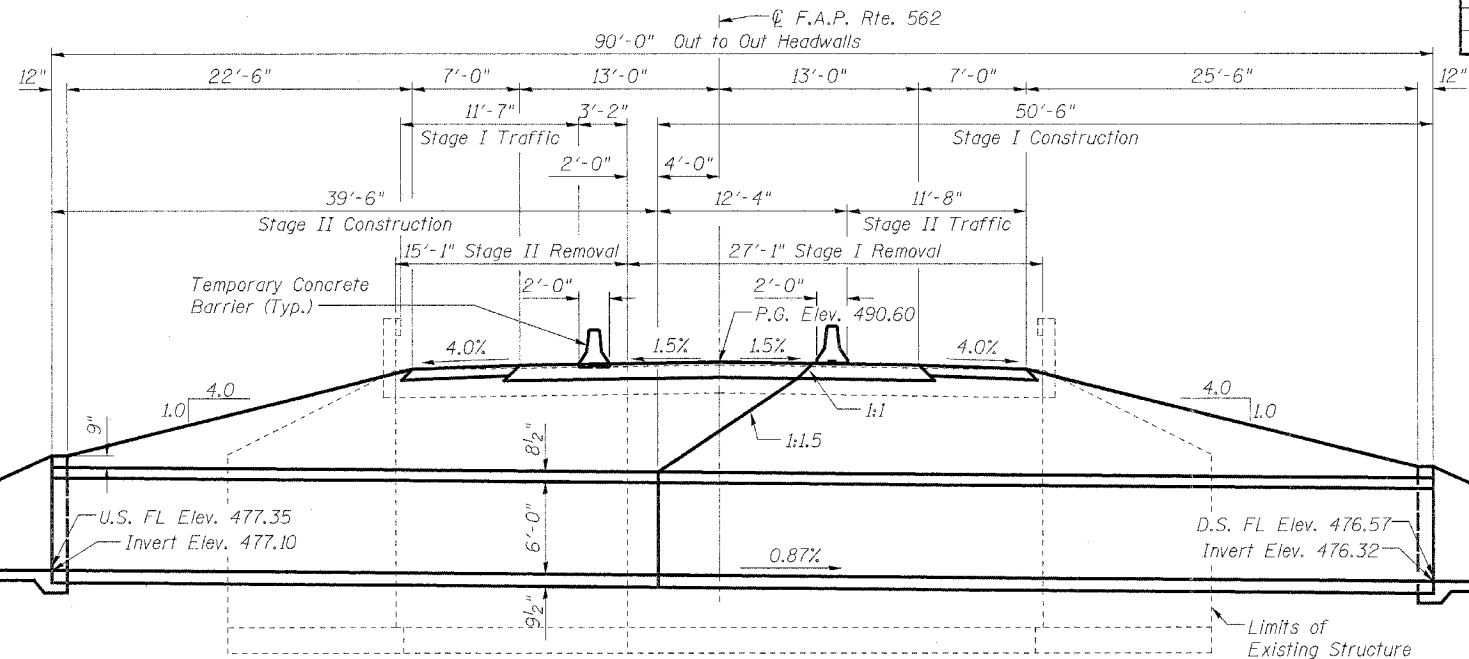
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Furnished Excavation	Cu. Yd.	915
Removal of Existing Structure	Each	1
Structure Excavation	Cu. Yd.	570
Reinforcement Bars, Epoxy Coated	Pound	15,540
Box Culvert End Sections	Each	2
Concrete Box Culverts	Cu. Yd.	75.0
Temporary Soil Retention System	Sq. Ft.	436
Granular Culvert Backfill	Cu. Yd.	350
Bar Splicers	Each	43
Rock Fill - Foundation	Ton	150

ROUTE NO.	SECTION	COUNTY	SHEETS	SHEET NO.
IL 100	117B-4	SCOTT	39	21

6 SHEETS



DESIGNED	S.C. Crowley
CHECKED	K.L. Hayes
DRAWN	S.C. Crowley
CHECKED	K. L. Hayes/ D. Siegfried

Note: Existing abutments are not stable once the existing slab is removed. Excavate behind Stage I abutments prior to Stage I slab removal. Follow same procedure during Stage II removal. Excavation is included in Structure Excavation quantity.

WATERWAY INFORMATION

Drainage Area = 0.44 sq. mi. Low Grade Elev. 489.8 @ Sta. 301+00

Flood	Freq. Yr.	C.F.S.	Opening Sq. Ft. Exist.	Prop.	Nat. H.W.E. Exist.	Prop.	Head - Ft. Exist.	Prop.	Headwater El. Exist.	Prop.
Design	10	115.9	46.4	15.5	480.18	480.56	0.06	1.10	480.24	481.28
Base	50	182.9	54.0	17.8	480.56	480.70	0.22	2.02	480.78	482.58
Overtopping	100	211.6	57.1	18.6	480.70	-	0.29	2.38	480.99	483.08
Max. Calc.	500	280.9	63.8	20.7	481.05	-	0.39	3.17	481.44	484.22

HIGHWAY CLASSIFICATION

F.A.P. 562 - IL Rte. 100

ADT: 1650(2001), 2340(2020)

ADTT: 347(2001), 491(2020)

DHV:

Functional Class: Minor Arterial, (Non-Urban)

Design Speed: 55 mph

DESIGN SPECIFICATIONS

AASHTO Standard Specifications for Highway Bridges Seventeenth Edition, 2002

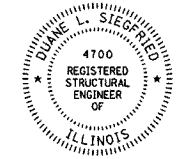
DESIGN STRESSES

$f_y = 60,000$  psi

$f'_c = 3,500$  psi

LOADING HS 20-44

LOCATION SKETCH



Signature: *[Signature]* Date: 4/28/05

License Expires 11/30/2006

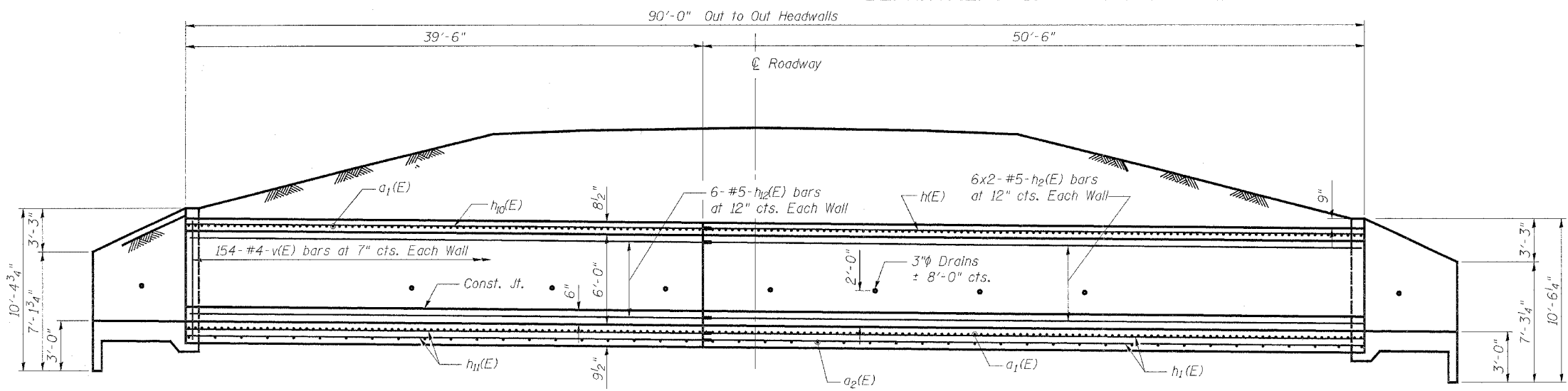
GENERAL PLAN  
ILLINOIS ROUTE 100 OVER  
WALNUT CREEK  
F.A.P. ROUTE 562 - SECTION 117(B-4)  
SCOTT COUNTY  
STATION 300+50.00  
STRUCTURE NO. 086-7500

Plot Date: 4/28/2005  
 Plotted By: csp/cthrst  
 Pen Tables: 100+tbl  
 File Name: s:\0400\11\_100\Cad\S\Wolnut\Wolnut2.Final.dgn

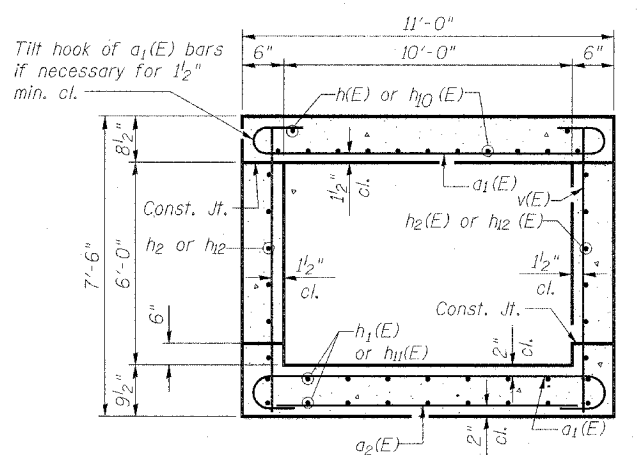
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
IL 100	117B-4	SCOTT	39	22
FED. ROAD DIST. NO. 6		ILLINOIS	FED. AID PROJECT - ROUTE 562	

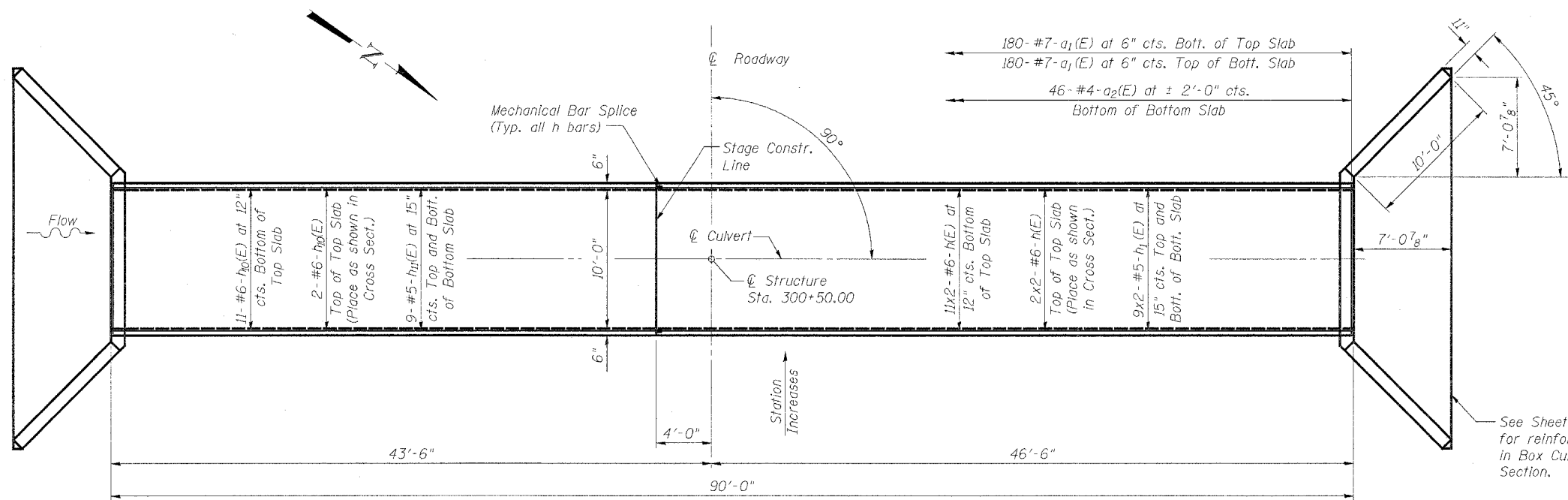
SHEET NO. 2  
6 SHEETS



**LONGITUDINAL SECTION**  
(Dimensions shown are at right angles to  $\text{C.R.}$  roadway)



**SECTION THRU BARREL**



**PLAN**

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a <sub>1</sub> (E)	360	#7	12'-4"	U
a <sub>2</sub> (E)	46	#4	10'-3"	—
h(E)	26	#6	26'-5"	—
h <sub>1</sub> (E)	36	#5	26'-3"	—
h <sub>2</sub> (E)	24	#5	26'-3"	—
h <sub>10</sub> (E)	13	#6	39'-3"	—
h <sub>11</sub> (E)	18	#5	39'-3"	—
h <sub>12</sub> (E)	12	#5	39'-3"	—
v(E)	308	#4	7'-2"	—
Reinforcement Bars Epoxy Coated			Pound	15,540
Concrete Box Culverts			Cu. Yd.	75.0
Bar Splicers			Each	43

**Min. Bar Laps**

#5 Bar = 2'-2"  
#6 Bar = 2'-7"

**DESIGN STRESSES**

f<sub>y</sub> = 60,000 psi  
f'c = 3,500 psi

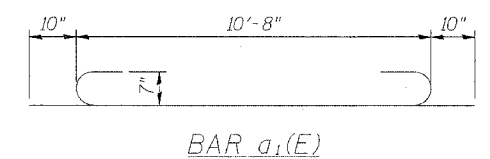
**LOADING HS 20-44**

**BARREL REINFORCING DETAILS**  
**ILLINOIS ROUTE 100 OVER**  
**WALNUT CREEK**  
**F.A.P. ROUTE 562 - SECTION 117(B-4)**  
**SCOTT COUNTY**  
**STATION 300+50.00**  
**STRUCTURE NO. 086-7500**

**NOTES**

Removal and replacement of weak soils with Rockfill - Foundation may be required beneath the culvert. The Engineer will determine the required depth of improvement following excavation to plan grade.  
Reinforcement Bars shall conform to the requirements of AASHTO M-31 M-42 or M-53, Grade 60.  
Bars indicated thus 12x4-#5 etc. indicates 12 lines of bars with 4 lengths per line.  
Reinforcement bars designated (E) shall be epoxy coated and shall conform to the requirements of AASHTO M-284.  
All construction joints shall be bonded.

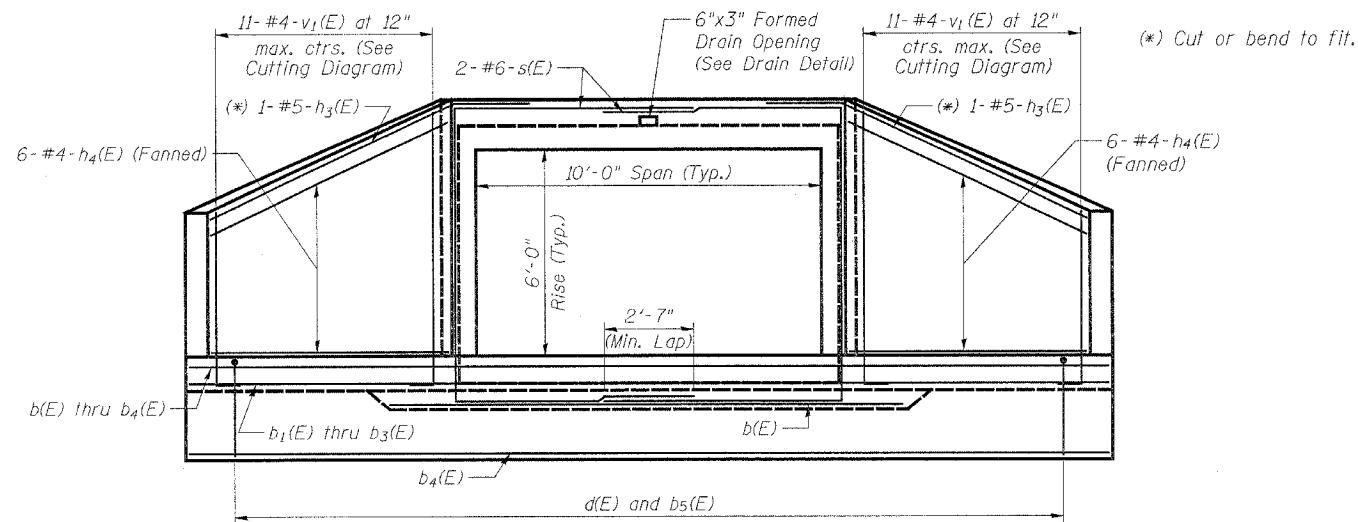
DESIGNED	S.C. Crowley
CHECKED	K.L. Hayes
DRAWN	S.C. Crowley
CHECKED	K.L. Hayes/ K.P. Heffern



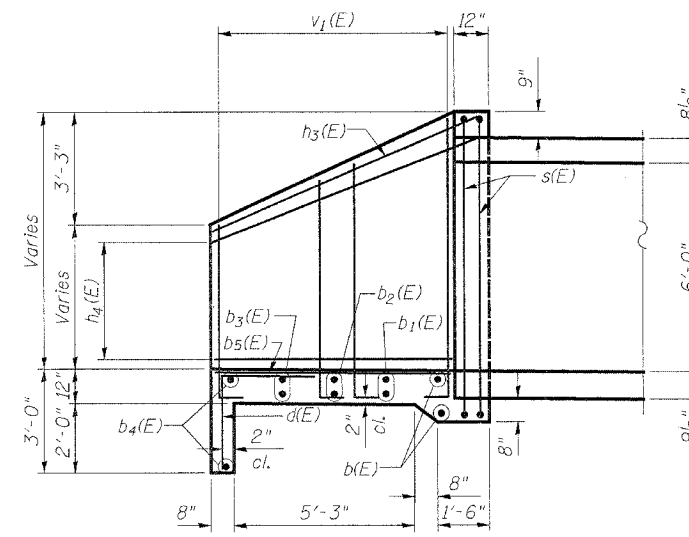
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

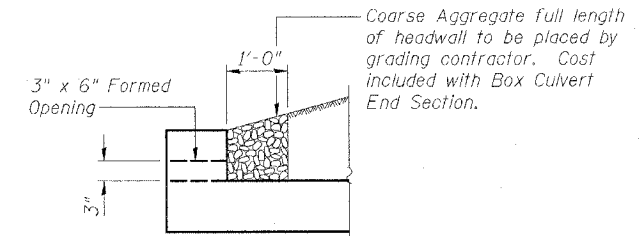
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 3
IL 100	117B-4	SCOTT	39	23	6 SHEETS
FED. ROAD DIST. NO. 6	ILLINOIS	FED. AID PROJECT			



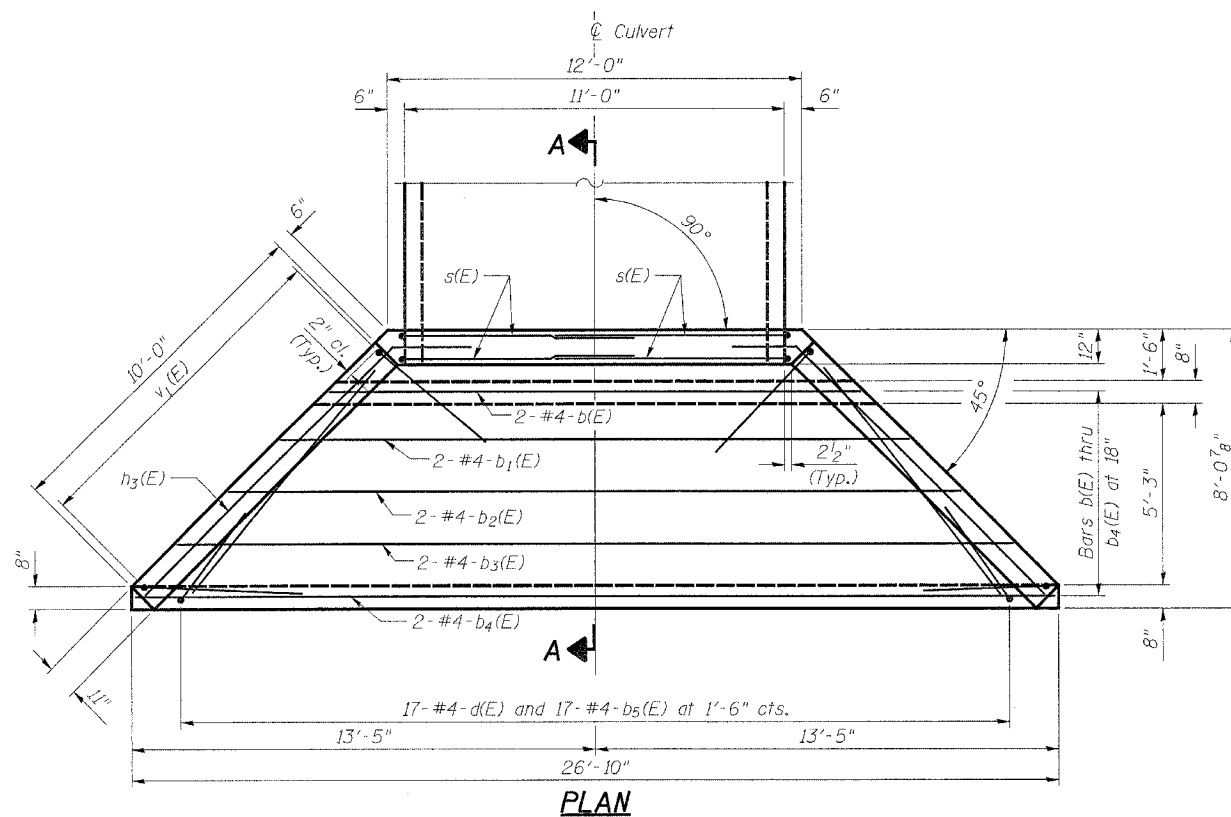
END ELEVATION



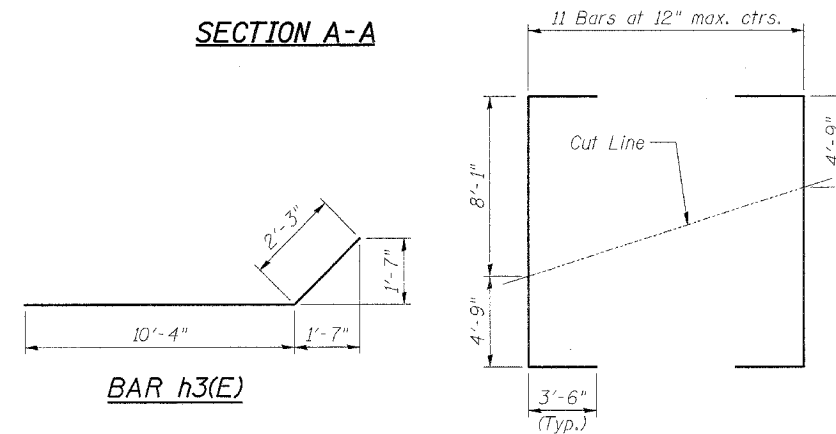
SECTION A-A



DRAIN DETAIL



PLAN

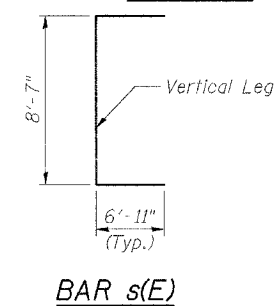


BAR h3(E)

CUTTING DIAGRAM  
BAR v1(E)



BAR d(E)



BAR s(E)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
b1(E)	2	#4	15'-2"	—
b2(E)	2	#4	18'-2"	—
b3(E)	2	#4	21'-2"	—
b4(E)	2	#4	24'-2"	—
b5(E)	17	#4	6'-10"	—
d(E)	17	#4	5'-4"	┌
h3(E)	2	#5	12'-7"	┌
h4(E)	12	#4	10'-3"	—
v1(E)	11	#4	19'-10"	┌
s1(E)	4	#6	22'-5"	┌
CLASS SI CONCRETE			Cu. Yd.	11.8
REINFORCEMENT BARS EPOXY COATED			Pound	670

NOTES:  
Bill of Material for one (1) End Section only.  
Two (2) are required.

Concrete and reinforcement quantities are for information only, pay item is "Box Culvert End Section (Each)".

BOX CULVERT END SECTION  
REINFORCING DETAILS  
ILLINOIS ROUTE 100 OVER  
WALNUT CREEK  
F.A.P. ROUTE 562 - SECTION 117(B-4)  
SCOTT COUNTY  
STATION 300+50.00  
STRUCTURE NO. 086-7500

DESIGNED	S.C. Crowley
CHECKED	K.L. Hayes
DRAWN	S.C. Crowley
CHECKED	K.P. Heffern

Plot Date: 4/28/2005  
Plot Path: \\s0400n11\_000\cadd\117B-4\Walu142\_Final.dgn  
Plot Scale: 1:1  
Pen Table: 1001.tbl  
Filename: s:\0400n11\_000\cadd\117B-4\Walu142\_Final.dgn

NOTES:  
Class SI concrete shall be used throughout.  
Exposed edges shall be beveled 3/4".  
Reinforcement bars shall conform to the requirements of AASHTO M-31, M-42, M-53, Grade 60.  
Reinforcement bars designated (E) shall be epoxy coated and shall conform to the requirements of AASHTO M-284.  
Bar dimensions are out to out.  
Precast substitution for the End Section will not be allowed.

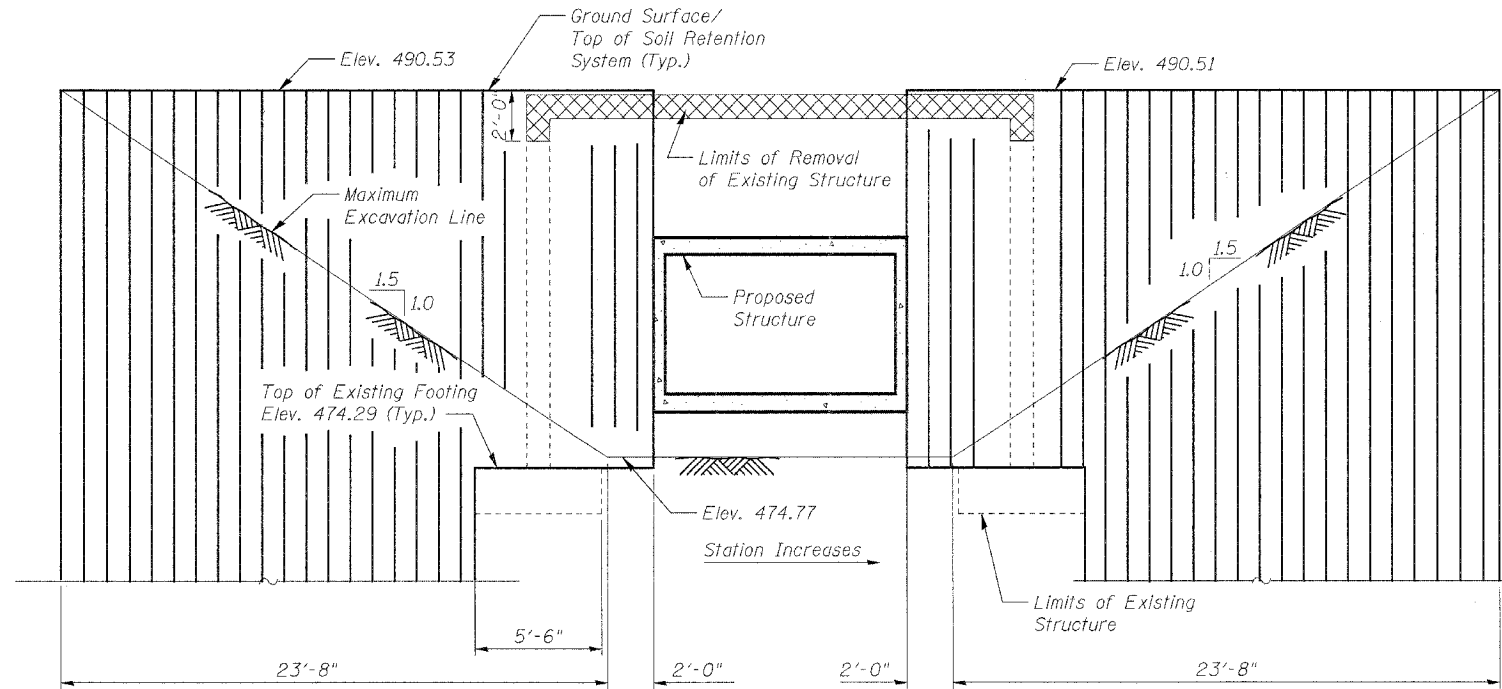
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
IL 100	117B-4	SCOTT	39	24
FED. ROAD DIST. NO. 6		ILLINOIS FED. AID PROJECT - ROUTE 662		

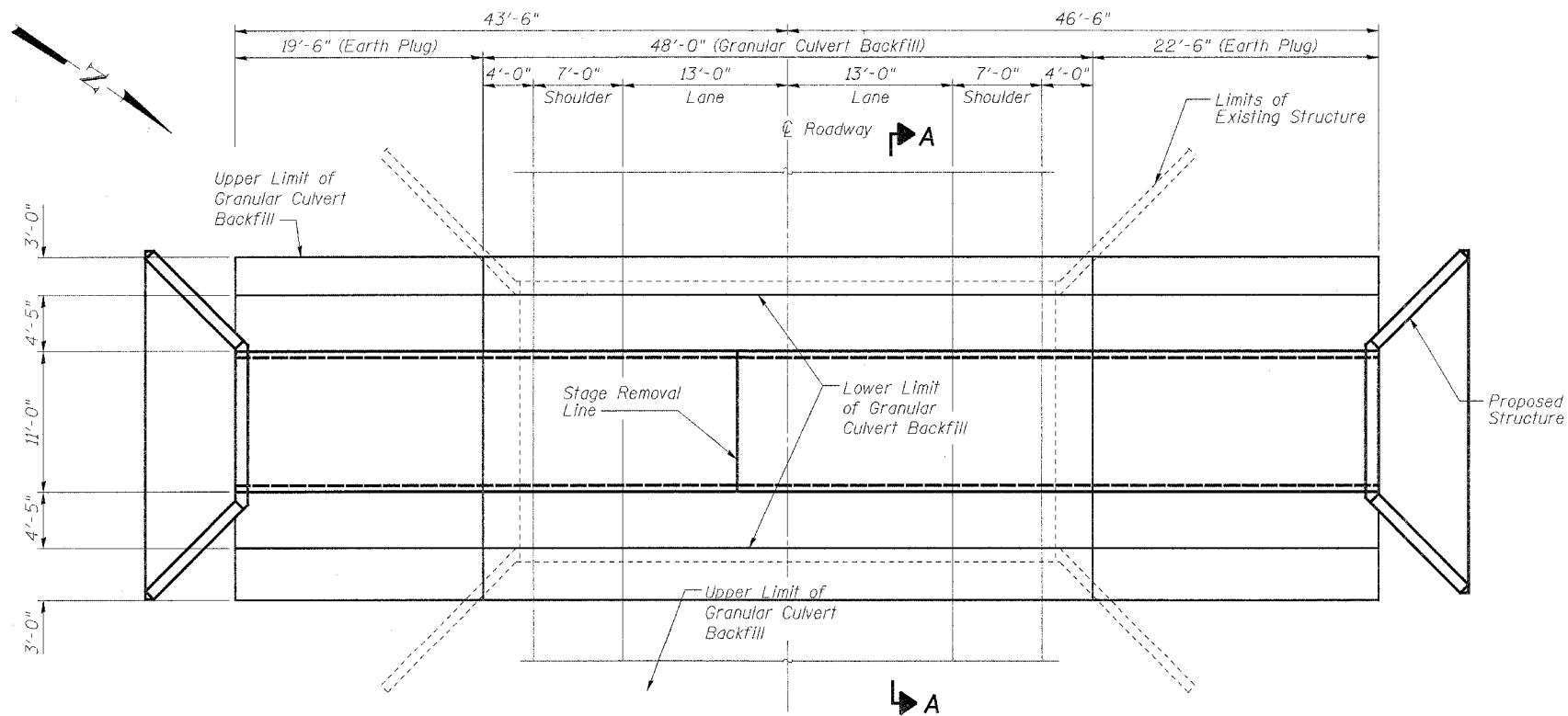
SHEET NO. 4  
6 SHEETS

**NOTES**

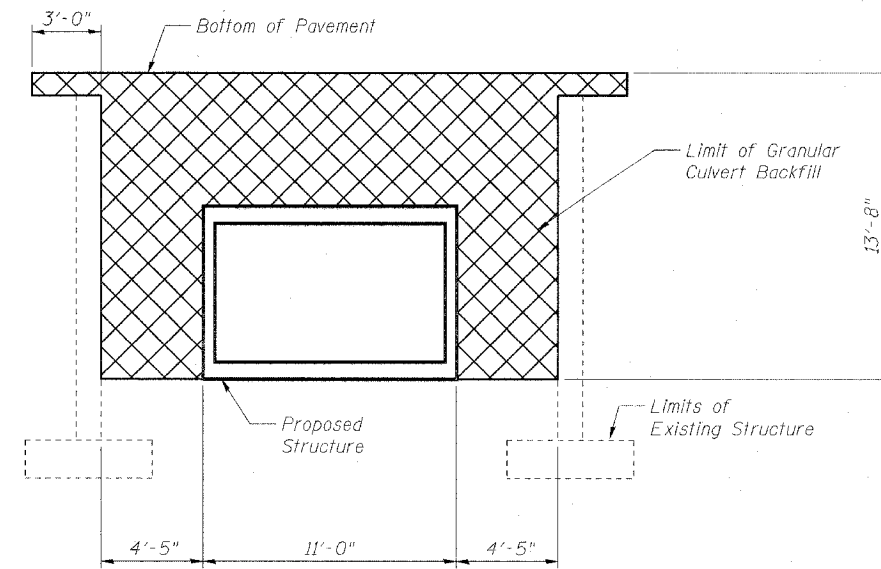
A cantilevered sheet piling design does not appear feasible and additional members or other retention systems may be necessary. The Contractor shall submit a temporary soil retention system design including plan details and calculations for review and acceptance by the Engineer.



**TEMPORARY SOIL RETENTION SYSTEM**



**LIMITS OF GRANULAR CULVERT BACKFILL**



**SECTION A-A**

TEMPORARY SOIL RETENTION SYSTEM  
AND LIMITS OF GRANULAR CULVERT BACKFILL  
ILLINOIS ROUTE 100 OVER  
WALNUT CREEK  
F.A.P. ROUTE 562 - SECTION 117(B-4)  
SCOTT COUNTY  
STATION 300+50.00  
STRUCTURE NO. 086-7500

DESIGNED	I.D.O.T.
CHECKED	S.C. Crawley
DRAWN	I.D.O.T.
CHECKED	S.C. Crawley



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**NOTES**

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.  
Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.  
All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.  
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.  
Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- ① Minimum Capacity =  $1.25 \times f_y \times A_t$   
(Tension in kips)
- ② Minimum \*Pull-out Strength =  $1.25 \times f_{s_{allow}} \times A_t$   
(Tension in kips)

Where  $f_y$  = Yield strength of lapped reinforcement bars in ksi.  
 $f_{s_{allow}}$  = Allowable tensile stress in lapped reinforcement bars in ksi (Service Load)  
 $A_t$  = Tensile stress area of lapped reinforcement bars.  
\* = 28 day concrete

BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	5.9
#5	2'-0"	23.0	9.2
#6	2'-7"	33.1	13.3
#7	3'-5"	45.1	18.0
#8	4'-6"	58.9	23.6
#9	5'-9"	75.0	30.0
#10	7'-3"	95.0	38.0
#11	9'-0"	117.4	46.8

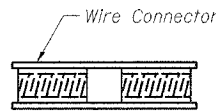
Bar splicer assemblies shall be according to Section 508 of the Standard Specifications, except as noted. The furnishing and installation of bar splicer assemblies will be measured and paid for at the contract unit price each for "BAR SPLICERS."

The diameter of this part is the same as the diameter of the bar spliced.

**ROLLED THREAD DOWEL BAR**



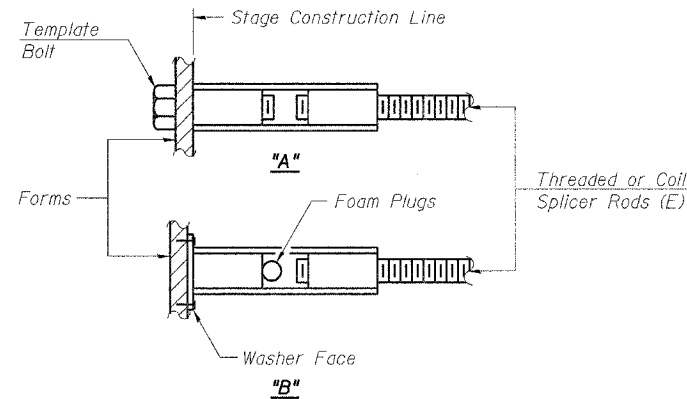
**\*\* ONE PIECE**



**WELDED SECTIONS**

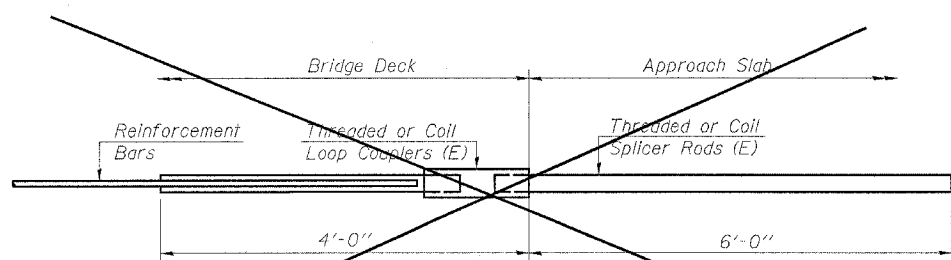
**BAR SPLICER ASSEMBLY ALTERNATIVES**

\*\* Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



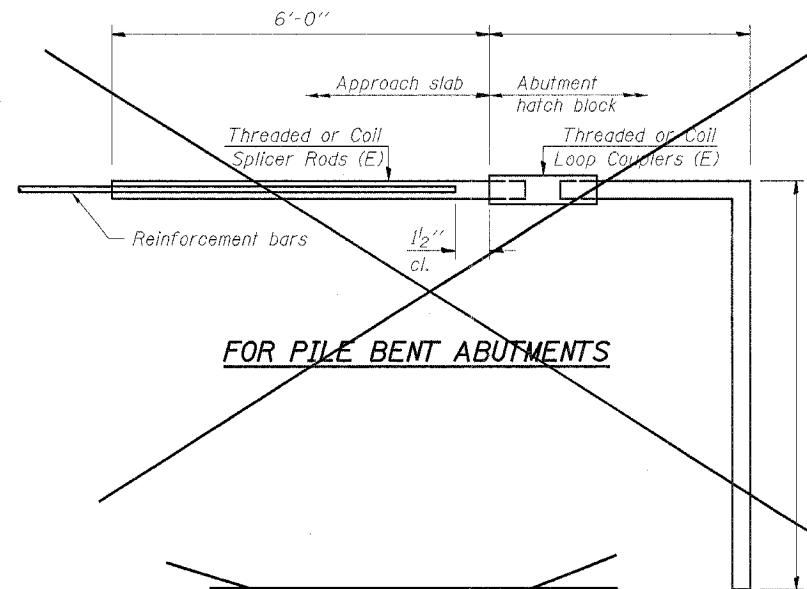
**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
(E) : Indicates epoxy coating.



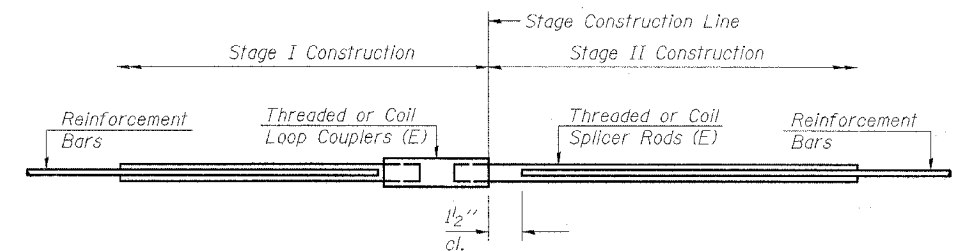
**FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

Bar Splicer for #5 bar		
Min. Capacity =	23.0	kips - tension
Min. Pull-out Strength =	9.2	kips - tension
No. Required =		



**FOR PILE BENT ABUTMENTS**

Bar Splicer for #5 bar		
Min. Capacity =	23.0	kips - tension
Min. Pull-out Strength =	9.2	kips - tension
No. Required =		



**STANDARD**

Bar Size	No. Assemblies Required	Location
#5	30	Bott. Slab & Walls
#6	13	Top Slab

**BAR SPLICER ASSEMBLY DETAILS  
ILLINOIS ROUTE 100 OVER  
WALNUT CREEK  
F.A.P. ROUTE 562 - SECTION 117(B-4)  
SCOTT COUNTY  
STATION 300+50.00  
STRUCTURE NO. 086-7500**

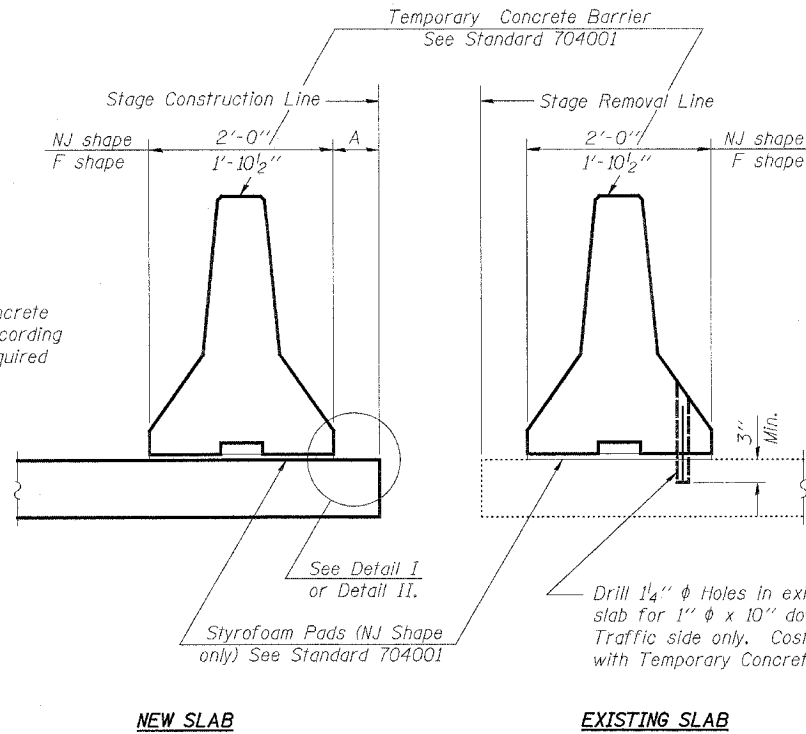
DESIGNED	I.D.O.T.
CHECKED	S.C. Crawley
DRAWN	I.D.O.T.
CHECKED	S.C. Crawley

**BSD-1** 9-01-03

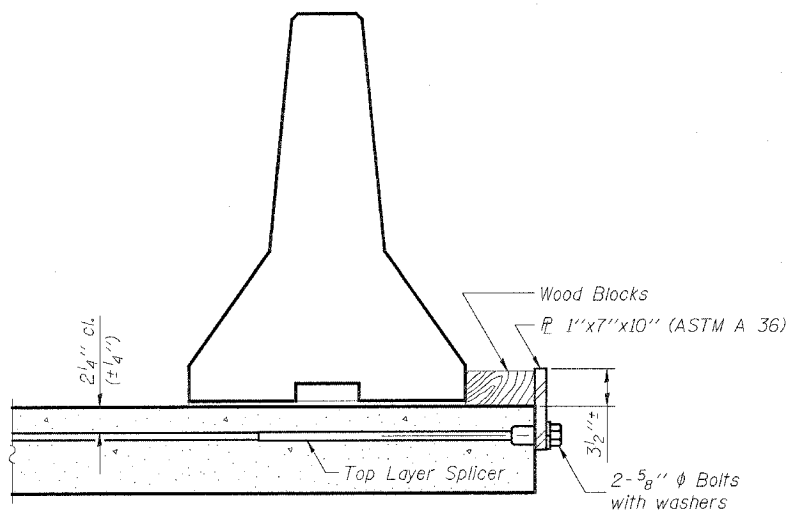
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEETS	SHEET NO.
IL 100	117B-4	SCOTT	39	26
FED. ROAD DIST. NO. 6	ILLINOIS	FED. AID PROJECT - ROUTE 552		

SHEET NO. 6  
6 SHEETS

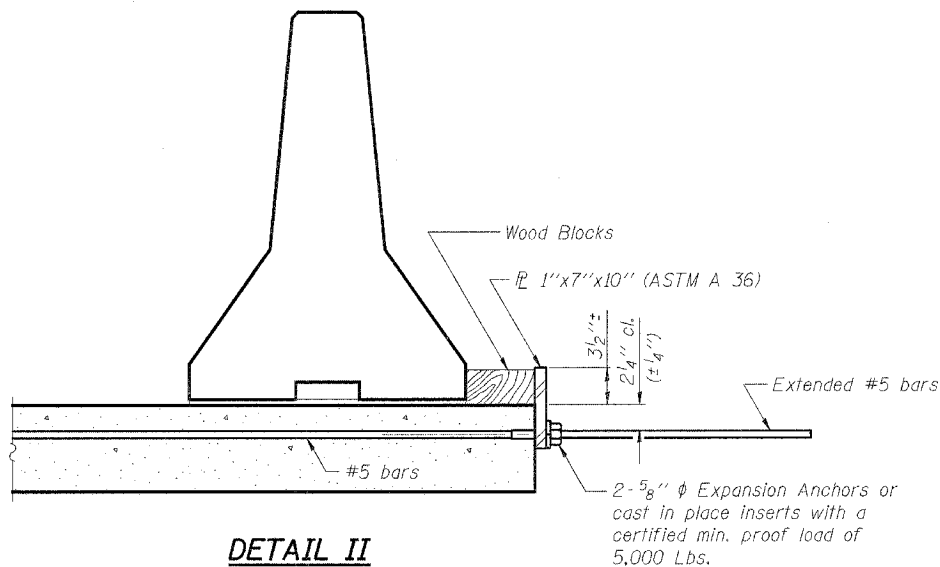


SECTIONS THRU SLAB



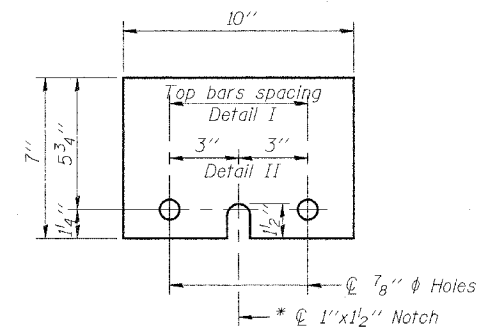
DETAIL I

The 1"x7"x10" Plate shall not be removed until Stage II Construction forms and reinforcement bars are in place.



DETAIL II

The 1"x7"x10" Plate shall not be removed until Stage II Construction forms and all reinforcement bars are in place and the concrete is ready to be placed.



PL 1"x7"x10"

\* Required only with Detail II

NOTES

- Detail I - With Bar Splicer or Couplers:  
Connect one (1) 1"x7"x10" steel PL to the top layer of couplers with 2-5/8"  $\phi$  bolts screwed to coupler at approximate  $\phi$  of each barrier panel.
- Detail II - With Extended Reinforcement Bars:  
Connect one (1) 1"x7"x10" steel PL to the concrete slab with 2-5/8"  $\phi$  Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate  $\phi$  of each barrier panel.
- Cost of anchorage is included with Temporary Concrete Barrier.

DESIGNED	I.D.O.T.
CHECKED	S.C. Crawley
DRAWN	I.D.O.T.
CHECKED	S.C. Crawley

TEMPORARY CONCRETE BARRIER  
FOR STAGE CONSTRUCTION  
ILLINOIS ROUTE 100 OVER  
WALNUT CREEK  
F.A.P. ROUTE 562 - SECTION 117(B-4)  
SCOTT COUNTY  
STATION 300+50.00  
STRUCTURE NO. 086-7500



# SOIL BORING LOG

Page 1 of 2

Date 11/4/03

ROUTE FAP 562 (IL 100) DESCRIPTION Over Walnut Creek LOGGED BY M. Tappan

SECTION 117B-4 LOCATION SE 1/4, SEC. 16, TWP. 16 N, RNG. 13 W, 4 PM

COUNTY Scott DRILLING METHOD HSA HAMMER TYPE 140 # Auto

STRUCT. NO. 086-0022 Ex  
086-C101 Pr  
Station 300+50

BORING NO. 1 NE WW  
Station 300+28  
Offset 15.0ft Lt  
Ground Surface Elev. 490.1 ft

DEPTH	B	U	M	Surface Water Elev.	DEPTH	B	U	M
W	L	C	O		W	L	C	O
H	S	Qu	T		H	S	Qu	T

Stream Bed Elev. 477.9 ft

Groundwater Elev.:  
 First Encounter 435.6 ft  
 Upon Completion 438.1 ft  
 After Hrs. Plugged ft

DEPTH (ft)	B	U	M	Soil Description	DEPTH (ft)	B	U	M
0				Brown and Grey Moist SILT (continued)	0			
1				Wet	1	0.0	29	
2				Slump	2			
466.60				Grey V. Moist SILTY CLAY LOAM w/ Gastropod Shells	466.60			
0					0			
1	0.5	21			1	0.2	64	
2	B				2	B		
481.60				Brown and Grey	481.60			
0				Grey V. Moist SILT LOAM	0			
1	0.6	22			1	0.1	30	
2	B				2	B		
479.10				Grey V. Moist SILT LOAM (Fill)	479.10			
0					0			
1	0.4	22			1	0.7	26	
2	B				2	S-12		
457.60				Brown and Grey Moist SILT	457.60			
0				Grey V. Moist SILT	0			
1	0.7	26			1	0.3	27	
2	S-12				2	B		
437.10				Grey Moist LOAM w/ Interbedded Medium Grained GRAVEL @ 55.0'	437.10			
0				Free Water	0			
1	0.6	25			1	1.0	24	
2	S-10				2	B		
432.60				V. Moist	432.60			
0					0			
1	0.4	25			1	0.4	30	
2	B				2			
424.60				Grey Dry Fissile Clayey SHALE	424.60			
0					0			
1	0.4	27			1	0.4	30	
2	0.4	27		w/ Woody Organics	2	0.4	30	

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer, E-Estimated)  
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) (B.S. - Before Seating)  
 BBS, from 137 (Rev. 8-99)

SOIL BORING 086-0022 IL 100 OVER WALNUT CREEK.GPJ 05TEMP1.GDT 6/17/05  
 Latitude Longitude

PLOT DATE = 6/26/2005  
 FILE NAME = c:\projects\0865403\1100 over walnut creek 086-0022.sxdgn  
 PLOT SCALE = 0.6000 m / IN.  
 USER NAME = bobj



# SOIL BORING LOG

Page 2 of 2

Date 11/4/03

ROUTE FAP 562 (IL 100) DESCRIPTION Over Walnut Creek LOGGED BY M. Tappan

SECTION 117B-4 LOCATION SE 1/4, SEC. 16, TWP. 16 N, RNG. 13 W, 4 PM

COUNTY Scott DRILLING METHOD HSA HAMMER TYPE 140 # Auto

STRUCT. NO. 086-0022 Ex  
086-C101 Pr  
Station 300+50

BORING NO. 1 NE WW  
Station 300+28  
Offset 15.0ft Lt  
Ground Surface Elev. 490.1 ft

DEPTH	B	U	M	Surface Water Elev.	DEPTH	B	U	M
W	L	C	O		W	L	C	O
H	S	Qu	T		H	S	Qu	T

Stream Bed Elev. 477.9 ft

Groundwater Elev.:  
 First Encounter 435.6 ft  
 Upon Completion 438.1 ft  
 After Hrs. Plugged ft

DEPTH (ft)	B	U	M	Soil Description	DEPTH (ft)	B	U	M
0				Grey V. Moist SILT (continued)	0			
1				Grey Dry Fissile Clayey SHALE (continued)	1	0.5	30	
2					2			
424.60				w/ V. Fine Grained SAND Seams	424.60			
0					0			
1	0.5	30			1	0.3	31	
2	B				2	B		
424.60				Boring Completed	424.60			
0				GPS Lat 39 44.653N	0			
1	0.3	31		GPS Long 90 32.537W	1	0.3	31	
2	B			NAD 83	2	B		
437.10				Grey Moist LOAM w/ Interbedded Medium Grained GRAVEL @ 55.0'	437.10			
0				Free Water	0			
1	0.6	25			1	1.0	24	
2	S-10				2	B		
432.60				Grey Dry Fissile Clayey SHALE	432.60			
0					0			
1	0.4	30			1	0.4	30	
2	0.4	27			2			

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer, E-Estimated)  
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) (B.S. - Before Seating)  
 BBS, from 137 (Rev. 8-99)

SOIL BORING 086-0022 IL 100 OVER WALNUT CREEK.GPJ 05TEMP1.GDT 6/17/05  
 Latitude Longitude

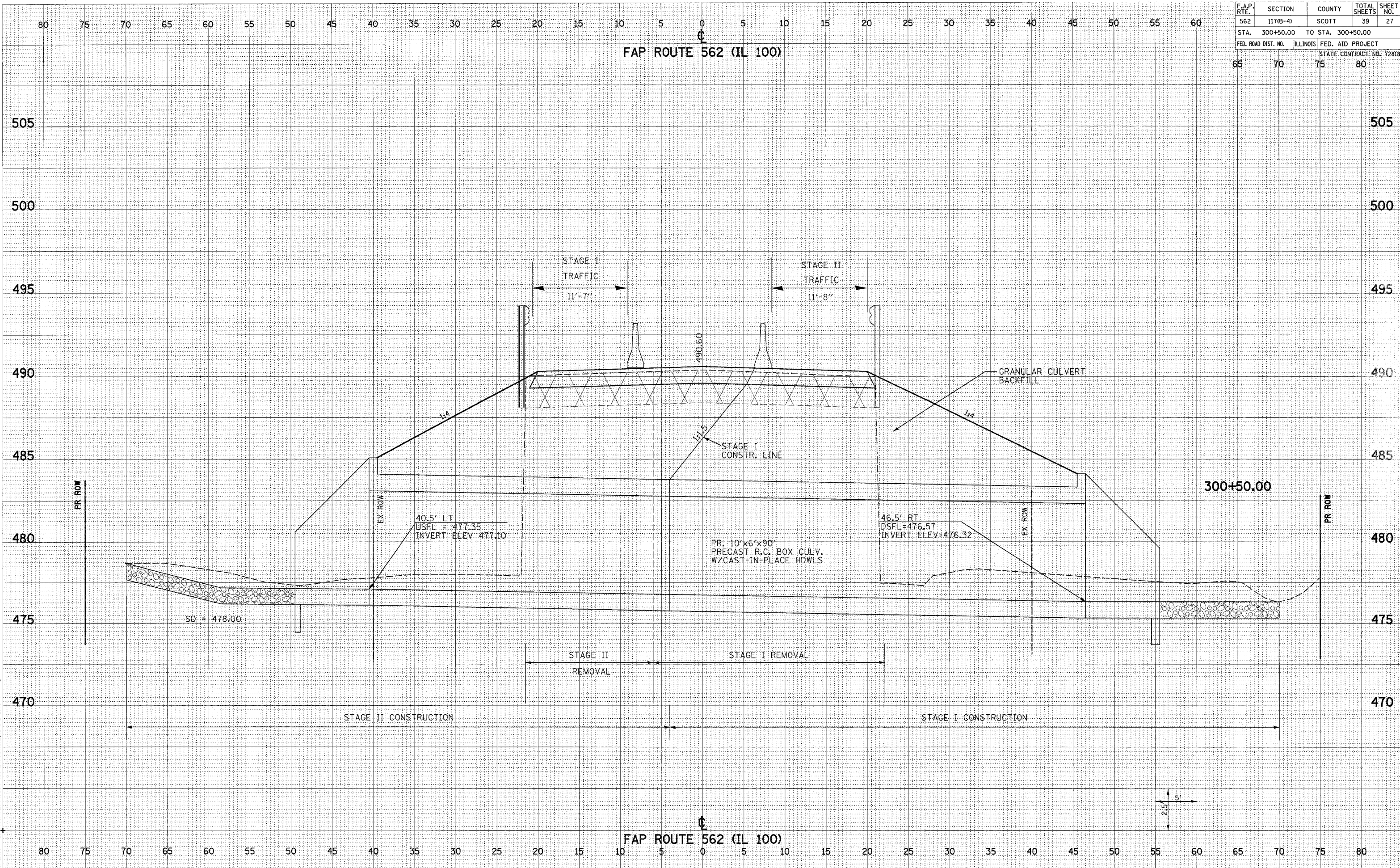
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		SOIL BORINGS FAP ROUTE 562 (IL 100) SCOTT COUNTY

SCALE: VERT. \_\_\_\_\_  
 DATE: \_\_\_\_\_ HORIZ. \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117B-41	SCOTT	39	27
STA. 300+50.00	TO STA. 300+50.00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	STATE CONTRACT NO. 72818	
65	70	75	80	



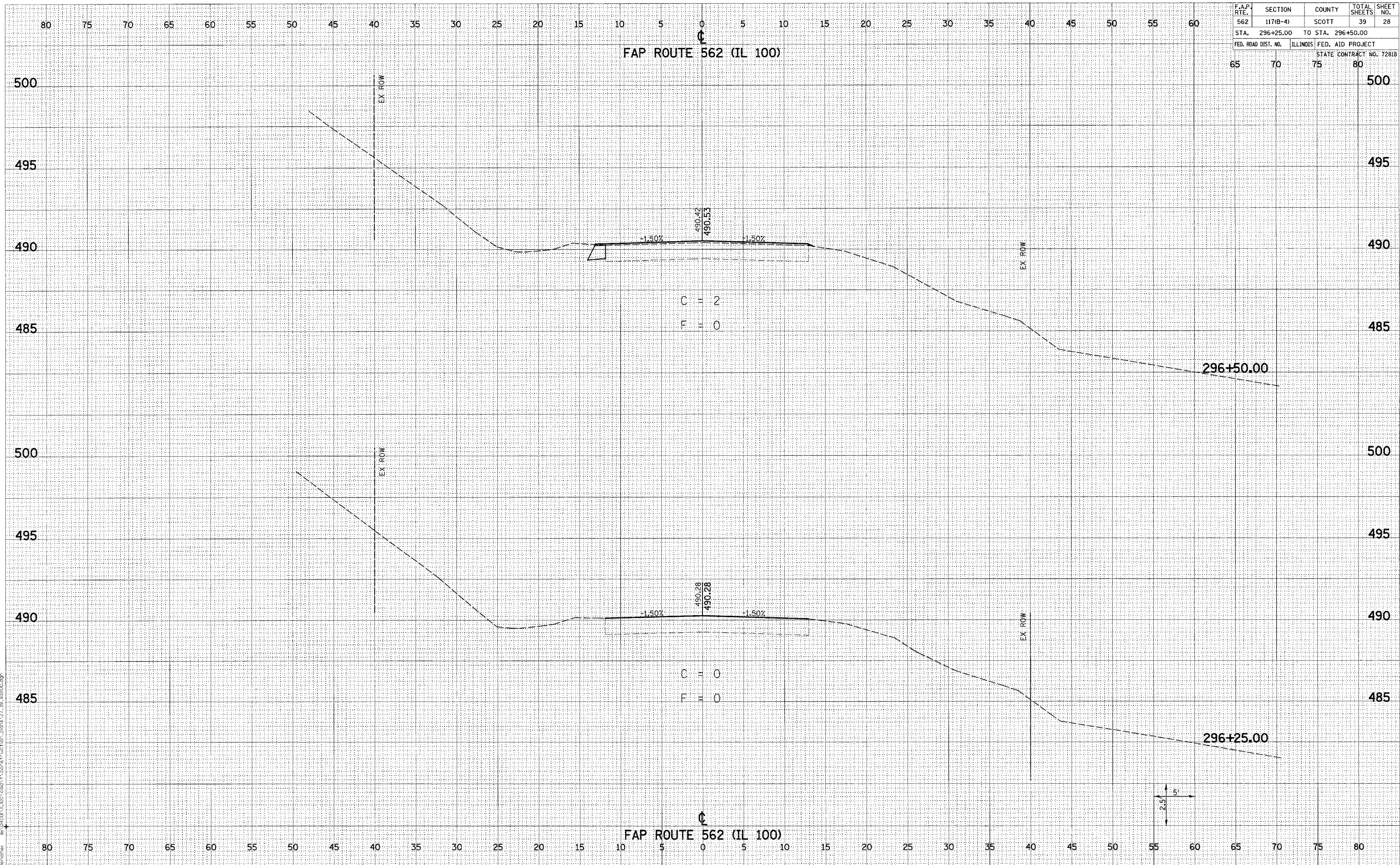
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SURVEYED	
PLOTTED	
AREA	
AREAS CHECKED	
NO.	

DATE	BY
SURVEYED	
PLOTTED	
AREA	
AREAS CHECKED	
NO.	

Plot Date: 4/27/2005  
 Plot By: 2005/1/24  
 Plotted By: abraham  
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	28
STA. 296+25.00 TO STA. 296+50.00		FED. AID PROJECT		
FED. ROAD DIST. NO.	ILLINOIS	STATE CONTRACT NO. 72818		

FAP ROUTE 562 (IL 100)

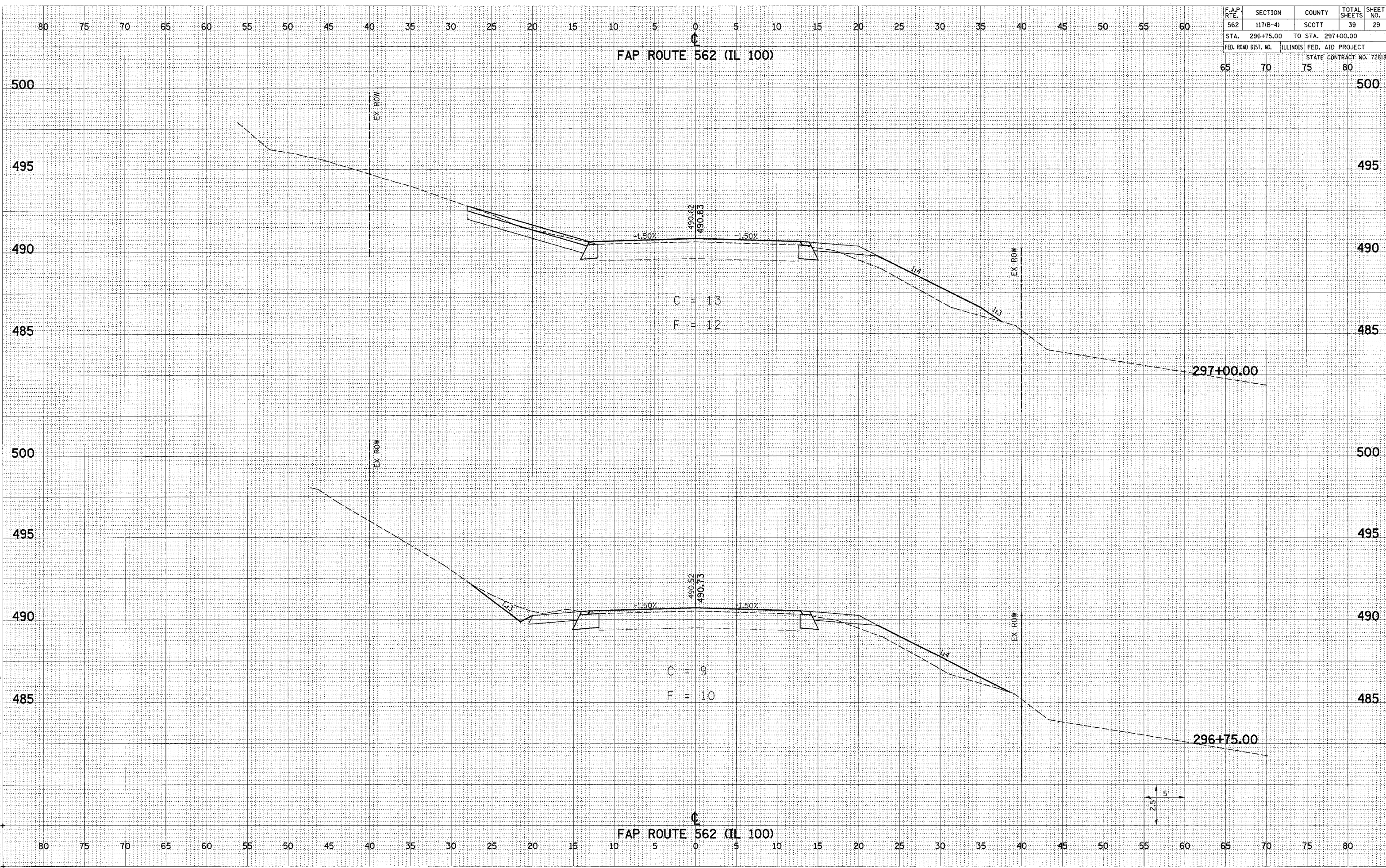


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NO.	TEMP	
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NOTE BOOK	PLOTTED	
NO.	TEMP	
	AREAS	
	CHECKED	

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	29
STA. 296+75.00		TO STA. 297+00.00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	STATE CONTRACT NO. 72818	
65	70	75	80	500



DATE	BY
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED

DATE	BY
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED

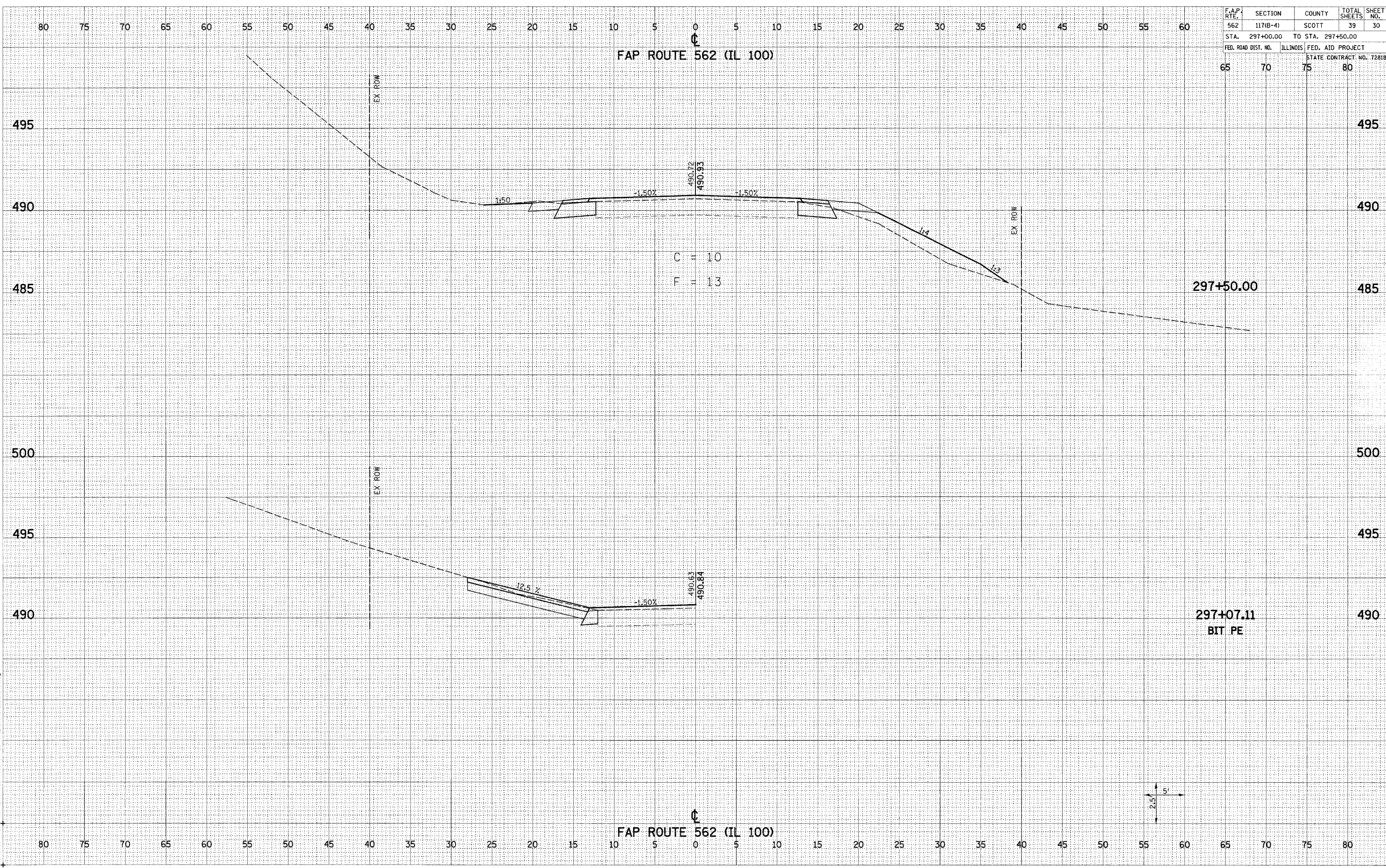
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 Plotted By: apattner  
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	30
STA. 297+00.00		TO STA. 297+50.00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		STATE CONTRACT NO. 72818
65	70	75	80	

DATE	BY

DATE	BY

Plot Date: 4/27/2005  
 Plot Title: 297+07.11  
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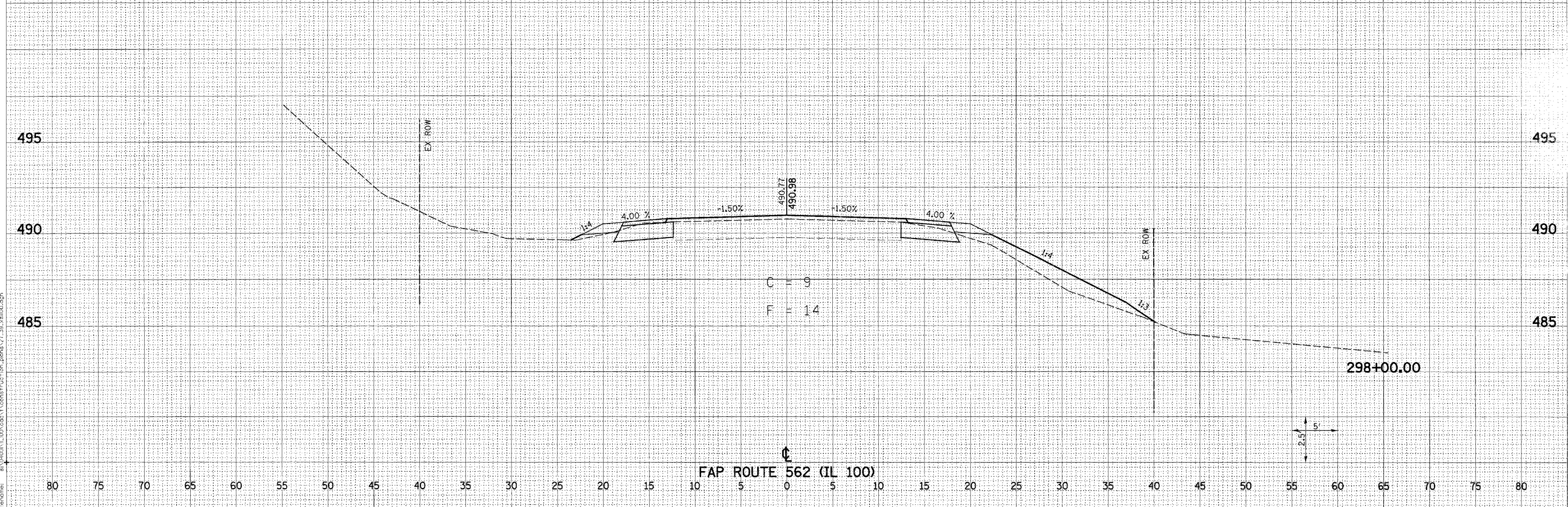
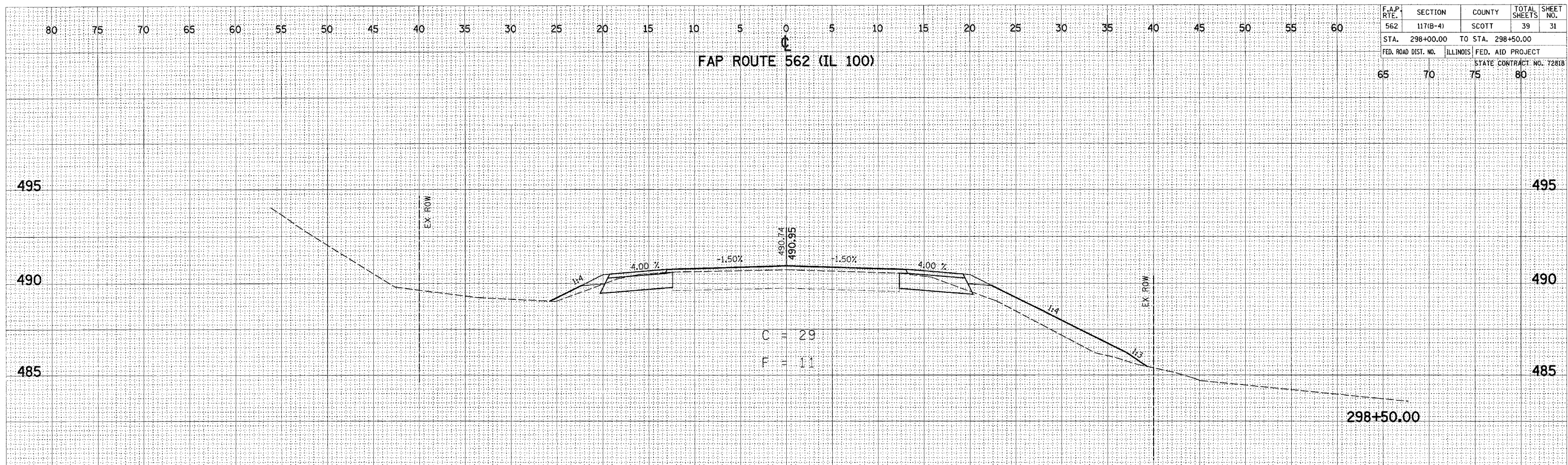


FAP 562 (IL 100) CROSS SECTIONS STA 297+07.11 TO STA 297+50.00



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	31
STA. 298+00.00		TO STA. 298+50.00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
		STATE CONTRACT NO. 72818		
65	70	75	80	

FAP ROUTE 562 (IL 100)



FAP ROUTE 562 (IL 100)

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NOTE BOOK	PLOTTED	
NO.	TEMP.	
	AREAS	
	CHECKED	

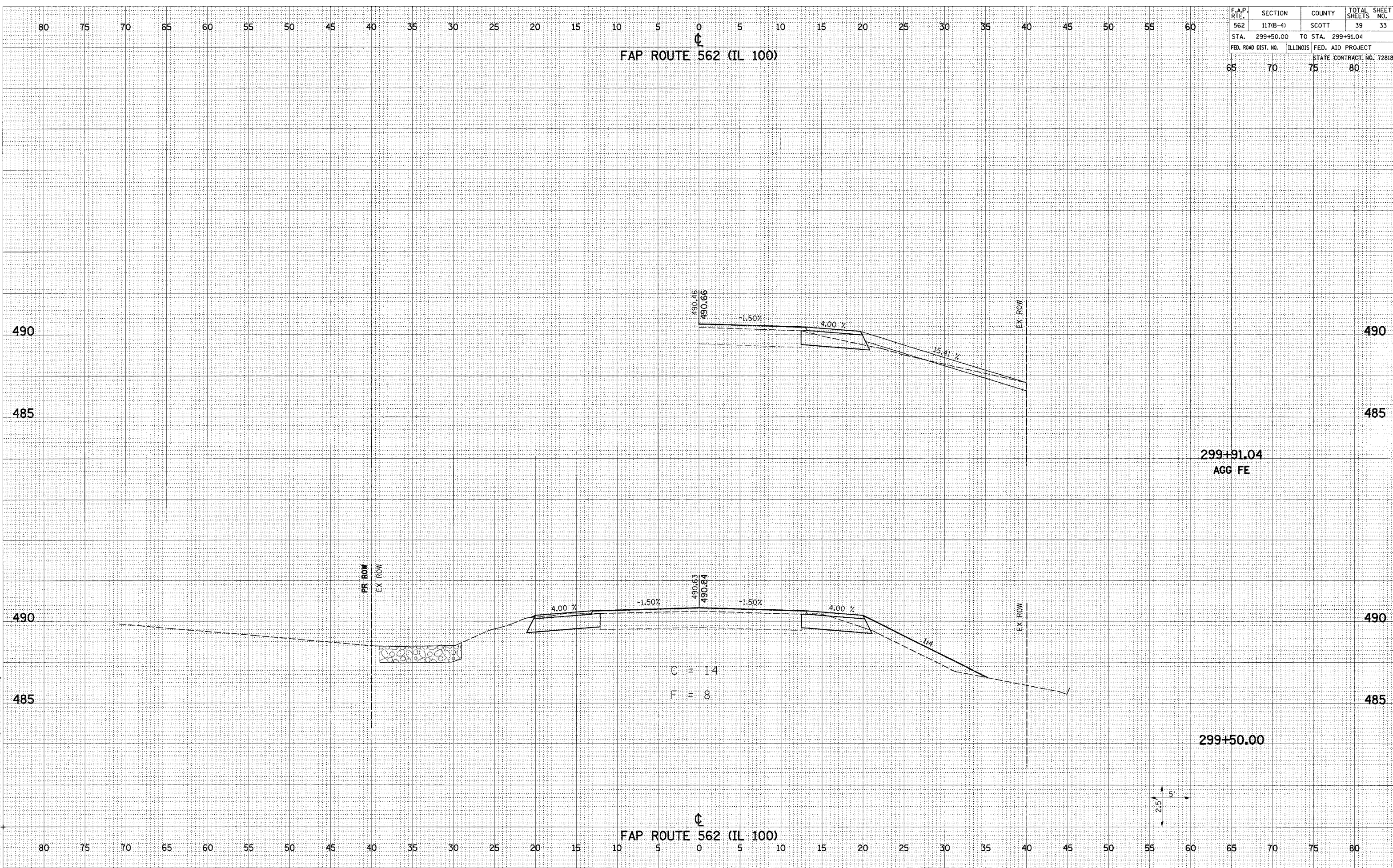
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NO.	TEMP.	
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	CHECKED	

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	33
STA. 299+50.00	TO STA. 299+91.04			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	STATE CONTRACT NO. 72818	
65	70	75	80	

FAP ROUTE 562 (IL 100)



FAP ROUTE 562 (IL 100)

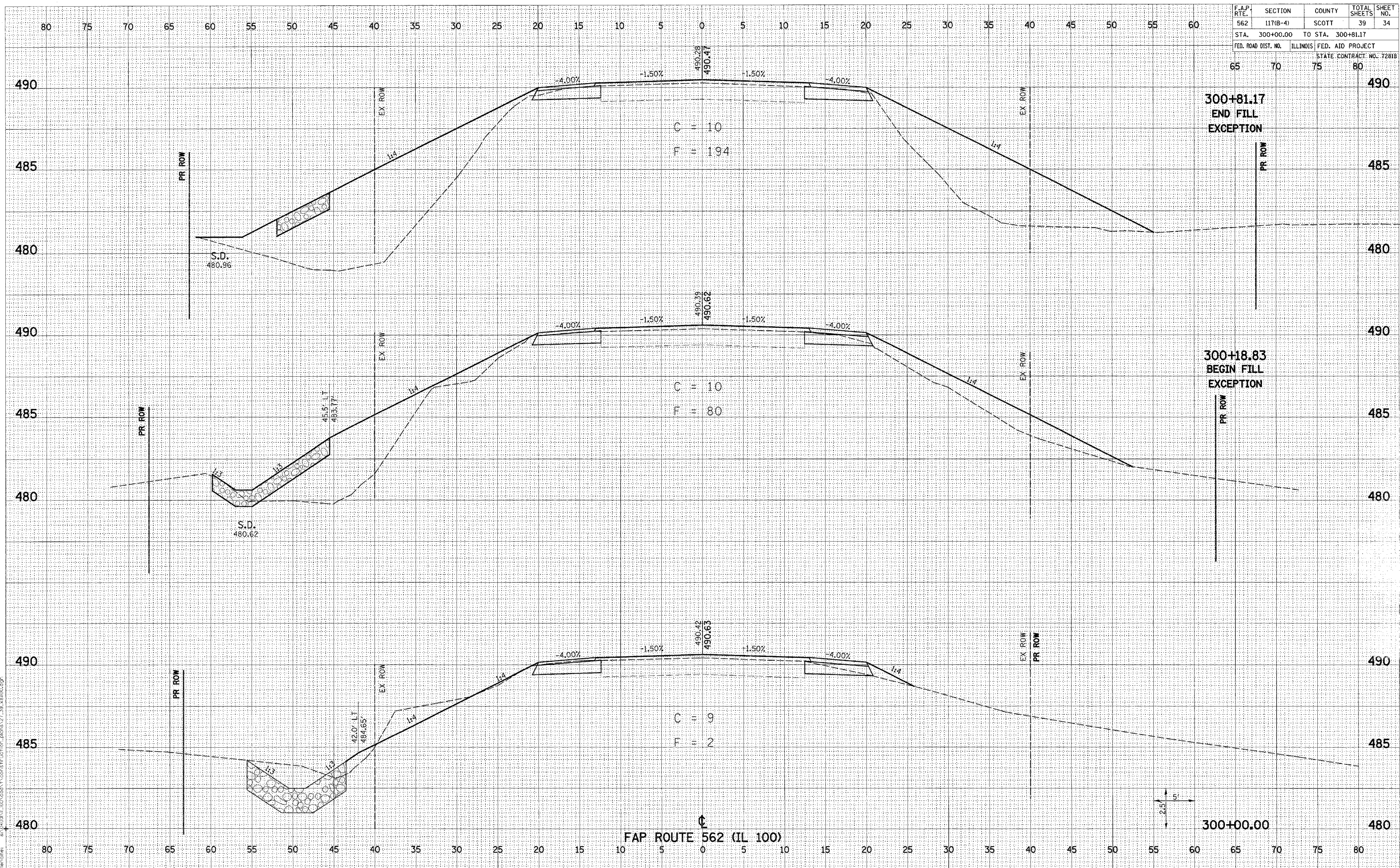
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BY  
SURVEYED  
PLOTTED  
NOTE BOOK  
AREAS CHECKED

DATE  
BY  
SURVEYED  
PLOTTED  
NOTE BOOK  
AREAS CHECKED

Plot Date: 1/27/2005  
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Plotted By: gphost  
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	34
STA. 300+00.00 TO STA. 300+81.17		FED. AID PROJECT		
FED. ROAD DIST. NO. ILLINOIS		STATE CONTRACT NO. 72818		
65	70	75	80	

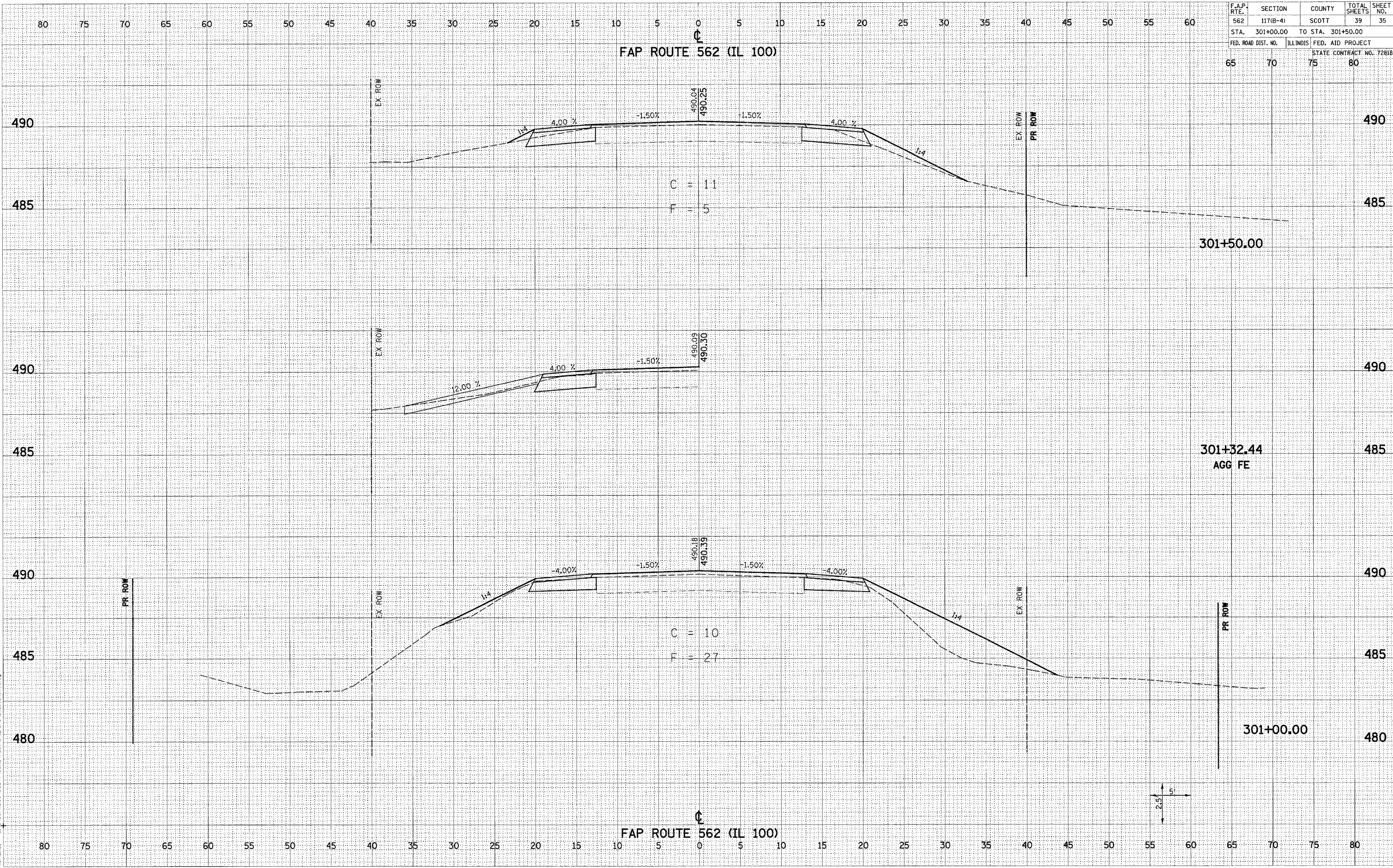


FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS CHECKED		
NO.		

Plot Date: 4/27/2006  
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	35
STA. 301+00.00	TO STA. 301+50.00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	STATE CONTRACT NO. 72818	
65	70	75	80	



DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
 SURVEYED: \_\_\_\_\_  
 PLOTTED: \_\_\_\_\_  
 NOTE BOOK: \_\_\_\_\_  
 AREAS CHECKED: \_\_\_\_\_  
 NO.:

DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
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 PLOTTED: \_\_\_\_\_  
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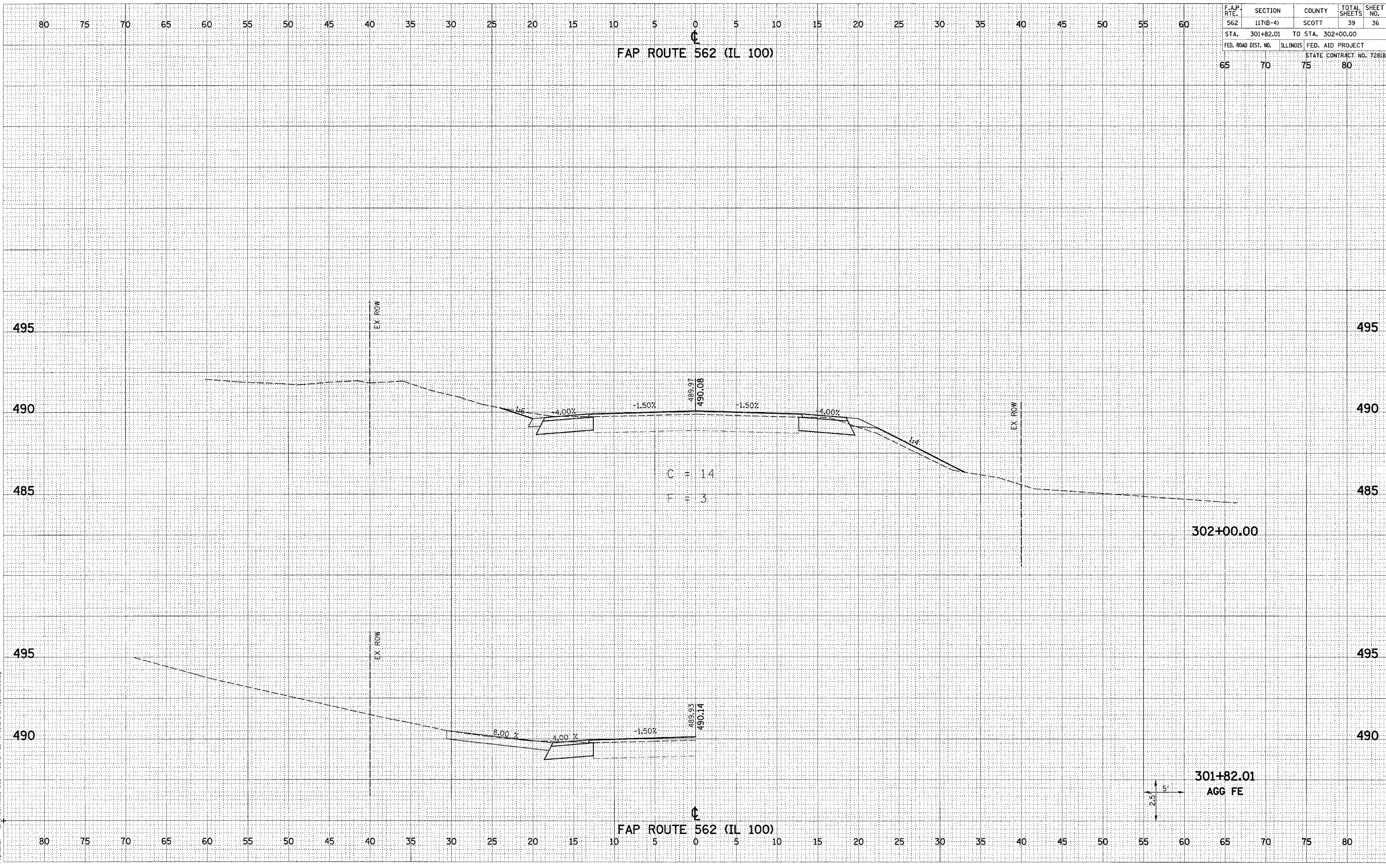
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	36
STA.	301+82.01 TO STA. 302+00.00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	STATE CONTRACT NO. 7281B	
65	70	75	80	

FINAL SURVEY	SURVEYED	DATE
NO. _____	_____	_____
PLOTTED	BY	DATE
_____	_____	_____
NOTE BOOK	TEMP.	_____
NO. _____	_____	_____
AREAS CHECKED	_____	

ORIGINAL SURVEY	SURVEYED	DATE
NO. _____	_____	_____
PLOTTED	BY	DATE
_____	_____	_____
NOTE BOOK	TEMP.	_____
NO. _____	_____	_____
AREAS CHECKED	_____	

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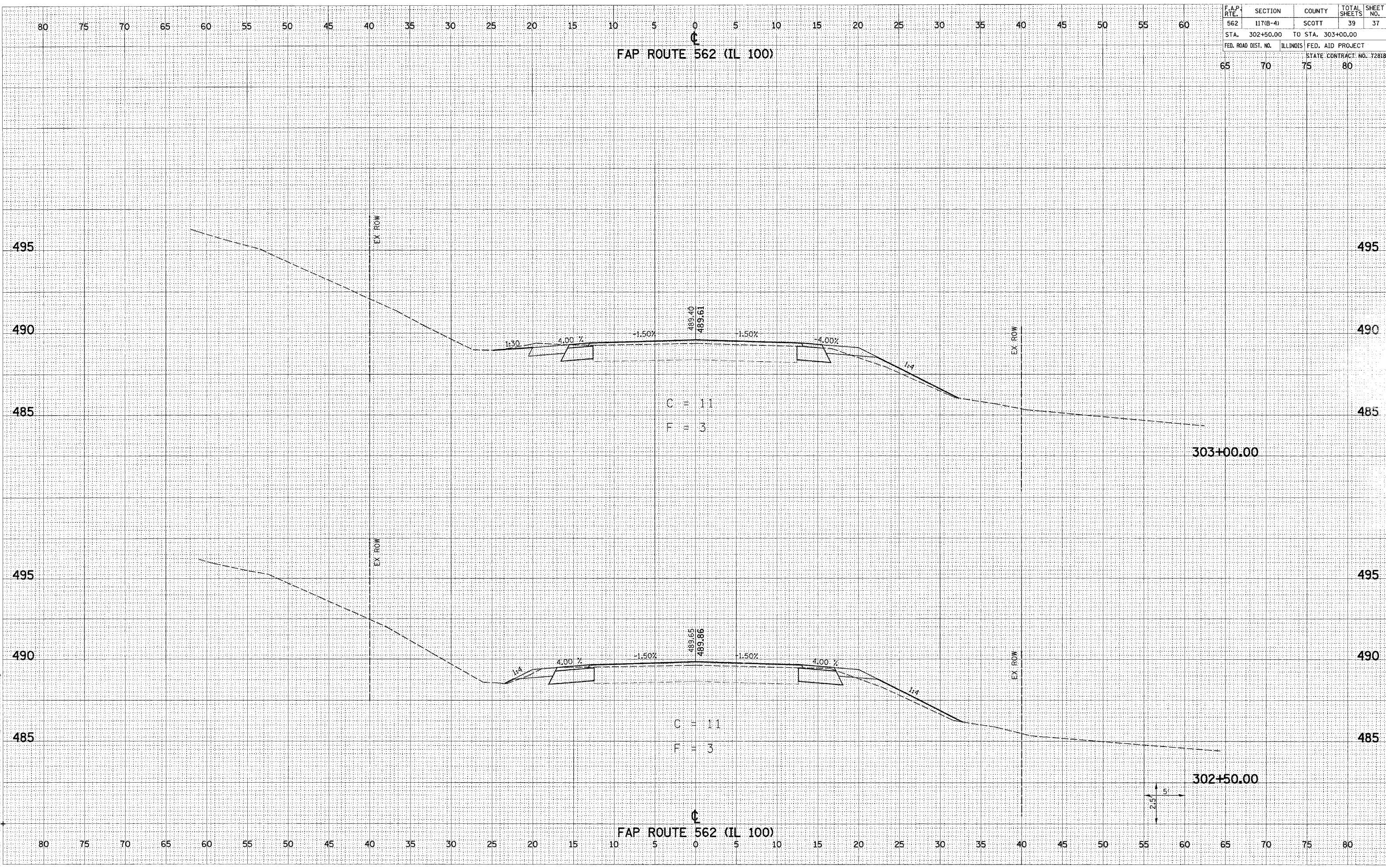


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	37
STA. 302+50.00	TO STA. 303+00.00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	STATE CONTRACT NO. 72818	
65	70	75	80	

DATE	BY
SURVEYED	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	BY
SURVEYED	
PLOTTED	
NOTE BOOK	
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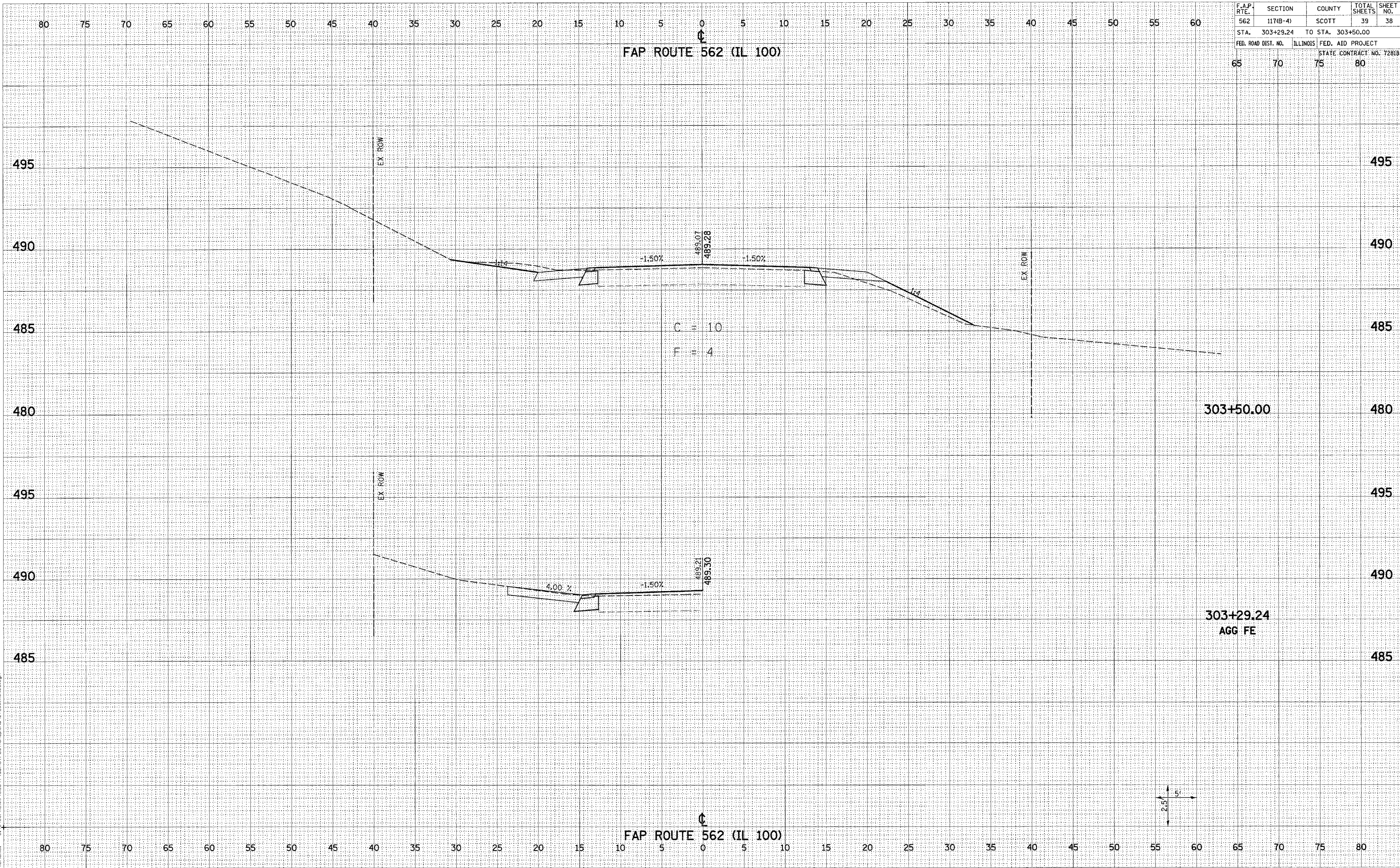
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FAP 562 (IL 100) CROSS SECTIONS STA 302+50.00 TO STA 303+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	38
STA. 303+29.24	TO STA. 303+50.00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	STATE CONTRACT NO. 72818	
65	70	75	80	

FAP ROUTE 562 (IL 100)



FAP ROUTE 562 (IL 100)

DATE	BY
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	TEMP
NO.	AREAS CHECKED

DATE	BY
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	TEMP
NO.	AREAS CHECKED

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
562	117(B-4)	SCOTT	39	39
STA. 304+00.00 TO STA. 304+00.00				
FED. ROAD DIST. NO. ILLINOIS		FED. AID PROJECT		
65 70		STATE CONTRACT NO. 72818		
		75 80		

DATE	
BY	
ORIGINAL SURVEY	
NOTED SURVEY	
NOTE BOOK	
AREAS	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
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NOTE BOOK	
AREAS	
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