

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FUNCTIONAL CLASSIFICATION: INTERSTATE F.A.I.
 ADT: 59,850 (2005), 74,480 (2021)
 DESIGN SPEED LIMIT: 50 MPH
 POSTED SPEED LIMIT: 50 MPH

SCOPE OF WORK

WITHIN THE PROJECT LIMITS SIX (6) BRIDGE STRUCTURES WILL HAVE INTERIM REPAIRS TO THE BRIDGE DECKS. TWO (2) BRIDGES WILL HAVE REPAIRS TO THE PIERS.

IMPROVEMENTS LOCATED IN THE CITY OF CHICAGO,
 COOK COUNTY

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

**PROPOSED
 HIGHWAY PLANS**

F.A.I. RTE. 55 / I-55 (STEVENSON EXPRESSWAY)

SECTION: 2011-031-BR
 PRAIRIE AVE. TO ICG RR

Proj. ACIM-055-7(263)29A

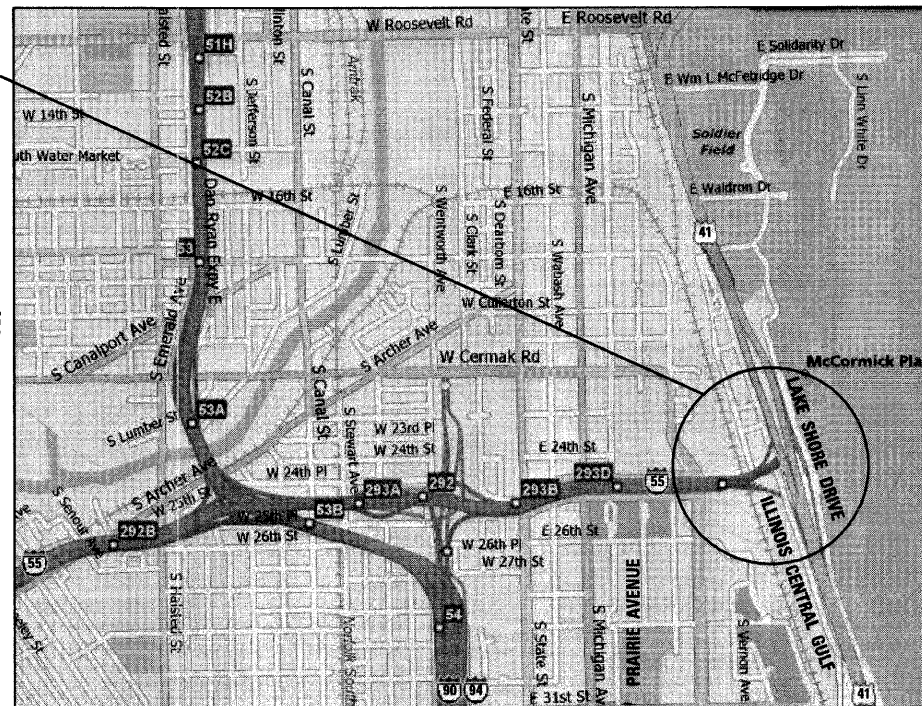
BRIDGE DECK REPAIRS

COOK COUNTY

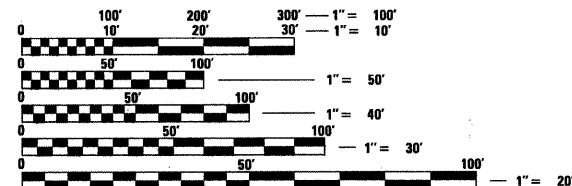
C-91-522-11

SOUTH TOWNSHIP
 R. 14 E.

LOCATION OF WORK:
 F.A.I. 55 AT U.S. 41
 SN 016-0036, SN 016-1045
 SN 016-1048, SN 016-1052
 SN 016-1055, SN 016-1075



GROSS & NET LENGTH = 7,796 FEET = 1.478 MILES



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N.
 CHICAGO UTILITY ALERT NETWORK
 1-312-744-7000

PROJECT MANAGER: ISSAM RAYYAN (842) 705-4178
 PROJECT ENGINEER: ROBERT BORO (847) 705-4237

CONTRACT NO. 60P37

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2011-031-BR	COOK	41*	1
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

D-91-522-11 # 41+2=43



LOCATION OF SECTION INDICATED THIS: -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED September 12, 2011

Diana M. O'Neil
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

October 14, 2011
Scott E. Stett, P.E.
 acting ENGINEER OF DESIGN AND ENVIRONMENT

October 14, 2011
Christine M. Reed
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER



Bhadresh N. Shah
 Bhadresh N. Shah
 Licensed Structural Engineer
 State of Illinois Lic. No. 081-004476
 Expires: 11-30-12



CHRISTIAN-ROGE & ASSOCIATES, INC.
 ENGINEERS / PLANNERS / SURVEYORS
 211 W. WACKER DRIVE CHICAGO, IL 60606
 TELEPHONE: 312-372-2023

PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS

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S.N. 016-1045

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S.N. 016-1048

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S.N. 016-1052

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S.N. 016-1055

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S.N. 016-1075

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40	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS
41	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS


LIST OF STATE STANDARDS

STANDARD NO.	DESCRIPTION
701400-05	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-06	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-07	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS > 45 MPH
701422-03	LANE CLOSURE, MULTILANE, FOR SPEEDS >= 45 MPH TO 55 MPH
701446-02	TWO LANE CLOSURE FREEWAY/EXPRESSWAY
701901-01	TRAFFIC CONTROL DEVICES
704001-06	TEMPORARY CONCRETE BARRIER
780001-02	TYPICAL PAVEMENT MARKINGS

GENERAL NOTES:

- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM I.D.O.T. FIELD MAINTENANCE ENGINEERS.
- THE CONTRACTOR MUST USE EXTREME CAUTION DURING FULL DEPTH DECK SLAB REPAIR AND MUST NOT NICK, CUT OR DAMAGE, IN ANY WAY, ANY OF THE STEEL BEAMS. THE EXISTING REINFORCEMENT BARS SHALL BE CLEANED. IF ANY OF THESE REINFORCEMENT BARS ARE DAMAGED THEY MUST BE REPLACED WITH AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM. THE COST OF THIS WORK IS INCLUDED WITH DECK SLAB REPAIR (FULL DEPTH, TYPE II) .
- THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4151 AT LEAST 72 HOURS PRIOR TO INSTALLATION OF THE TEMPORARY TRAFFIC CONTROL DEVICES.
- THERE ARE ELECTRICAL CONDUITS AND/OR FIBERGLASS DRAIN PIPES ATTACHED TO THE BOTTOM OF THE BRIDGE DECK. THE CONTRACTOR MAY MODIFY THE FORMING AND MUST EXERCISE EXTREME CAUTION DURING FULL DEPTH REPAIR BREAK OUT AND SHALL TEMPORARILY SUPPORT THE CONDUITS AND/OR DRAIN PIPES IF NEED BE, AT NO ADDITIONAL COST TO THE DEPARTMENT.
- WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR MUST EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC.
- PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED UPON THE UNIT PRICE BID FOR THE WORK.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL C.U.A.N. AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- TO COORDINATE ACCESS TO THE McCORMICK PLACE PARKING LOT CALL EUGENE HARTISON AT (312) 791-6081.
- DURING ANY WORK WITHIN THE McCORMICK PLACE PARKING LOT, THE CONTRACTOR MUST COORDINATE WITH THE McCORMICK PLACE PARKING LOT AUTHORITY AS TO AVOID ANY AND ALL CONFLICTS DURING THIS CONTRACT.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- ANY DAMAGES TO THE CONDUITS OR PIPES CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPLACED OR REPAIRED BY THE CONTRACTOR AT HIS/HER EXPENSE.
- ALL PROPOSED RAISED REFLECTIVE PAVEMENT MARKERS (BRIDGE) SHALL BE LOW PROFILE.
- THE CONTRACTOR SHALL COORDINATE AND/OR MODIFY THE WORK SCHEDULE AS DIRECTED BY THE DEPARTMENT TO ACCOMMODATE SPECIAL EVENTS.
- THE CONTRACTOR SHALL PLACE TEMPORARY CONCRETE BARRIER ADJACENT TO TEMPORARY SHORING AND CRIBBING TO PROVIDE A BARRIER FROM PARKING LOT TRAFFIC. AN ESTIMATED THREE (3) SECTIONS OF TEMPORARY CONCRETE BARRIER WILL BE NEEDED AT EACH SHORING AND CRIBBING LOCATIONS.
- THE CONTRACTOR SHALL USE PP3 CONCRETE FOR ALL DECK PATCHES.
- AS PART OF THIS PROJECT, ALL BRIDGE DECKS SHALL BE RE-STRIPED AND RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE REMOVED AND REPLACED.
- THE CONTRACTOR WILL BE ALLOWED ADDITIONAL LANE CLOSURES TO MEASURE AND SAWCUT PATCHING AREAS AND TO LAYOUT WEAVE LOCATIONS.

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 CHRISTIAN-ROGE & ASSOCIATES, INC.	USER NAME = IDOT	DESIGNED - J.C.N./B.N.S.	REVISED -
	PLOT SCALE = 50.0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -
	PLOT DATE = 10/12/2011	DRAWN - F.M.	REVISED -
		DATE - OCTOBER 12, 2011	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, LIST OF STATE STANDARDS AND GENERAL NOTES
F.A.I. RTE. I-55 OVER U.S. 41 / LAKE SHORE DRIVE**

SHEET NO. S2 OF S41 SHEETS

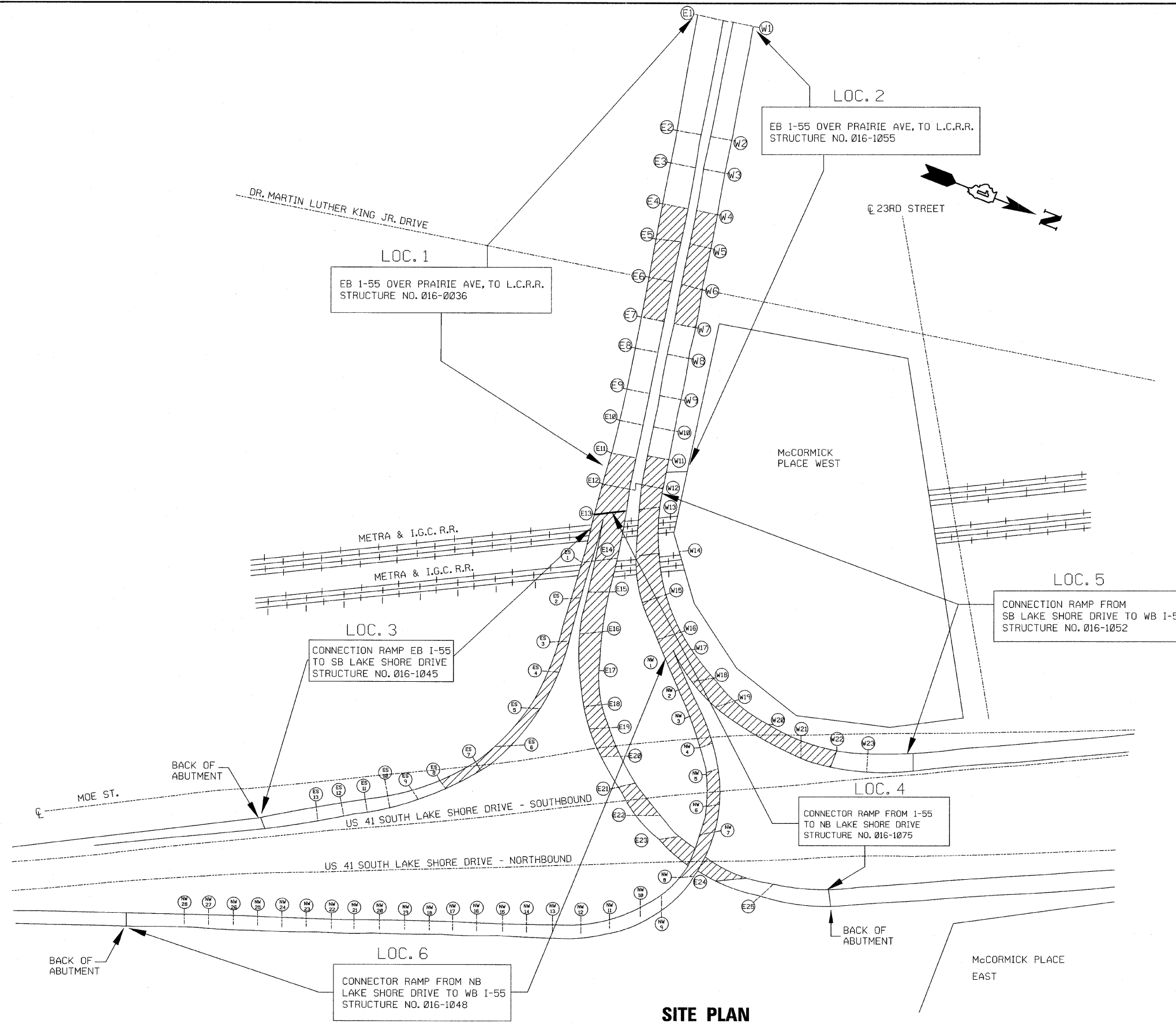
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2011-031-BR	COOK	41	2
				CONTRACT NO. 60P37
ILLINOIS FED. AID PROJECT				

FILE NAME = J:\102908.1-55\STRUCTURAL\sheet\160P37-03-sum of quantities.dgn

SUMMARY OF QUANTITIES				90% FED. / 10% STATE					
				CONSTRUCTION TYPE CODE					
CODE NO.	DESCRIPTION	UNIT	URBAN TOTAL QUANTITIES	S.N. 016-0036 FUNDS	S.N. 016-1045 FUNDS	S.N. 016-1048 FUNDS	S.N. 016-1052 FUNDS	S.N. 016-1055 FUNDS	S.N. 016-1075 FUNDS
				0014	0014	0014	0014	0014	0014
44200581	CLASS A PATCHES, TYPE III, 12 INCH	SQ YD	15	15					
44213000	PATCHING REINFORCEMENT	SQ YD	15	15					
44213200	SAW CUTS	FOOT	68	68					
44213202	TIE BARS, 1"	EACH	10	10					
50102400	CONCRETE REMOVAL	CU YD	24		18				6
50300225	CONCRETE STRUCTURES	CU YD	24		18				6
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	7,230		5,800				1,430
52100520	ANCHOR BOLTS, 1"	EACH	6	2	2				2
52100530	ANCHOR BOLTS, 1 1/4"	EACH	14		8				6
67100100	MOBILIZATION	L SUM	1	0.17	0.17	0.17	0.17	0.16	0.16
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1		1				
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	9	2	2	2	1	1	1
70300520	PAVEMENT MARKING TAPE, TYPE III, 4"	FOOT	5,000	1,800		800	2,400		
70300550	PAVEMENT MARKING TAPE, TYPE III, 8"	FOOT	7,000	2,500		2,100	2,400		
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	7,878	3,828		1,650	2,400		
70400100	TEMPORARY CONCRETE BARRIER	FOOT	475	75	250				150
* 78008210	POLYUREA PAVEMENT MARKING, TYPE I - LINE 4"	FOOT	16,400	2,800	3,000	4,500	1,600	2,100	2,400
* 78008220	POLYUREA PAVEMENT MARKING, TYPE I - LINE 5"	FOOT	1,780	720			210	540	310
* 78008240	POLYUREA PAVEMENT MARKING, TYPE I - LINE 8"	FOOT	1,890	1,590	300				
* 78008250	POLYUREA PAVEMENT MARKING, TYPE I - LINE 12"	FOOT	1,812	900	40	440		192	240
* 78008300	POLYUREA PAVEMENT MARKING, TYPE II - LETTERS AND SYMBOLS	FOOT	36	36					
* 78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	356	144			42	108	62
78300100	PAVEMENT MARKING REMOVAL	SQ FT	200	80		80	40		
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	356	144			42	108	62
X5080600	MECHANICAL SPLICERS	EACH	64		32				32
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	0.17	0.17	0.17	0.17	0.16	0.16
X7013820	TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS	CAL DA	60	10	10	10	10	10	10
Z0001700	APPROACH SLAB REPAIR (FULL DEPTH)	SQ YD	7					7	
Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	7					7	
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	54	54					
Z0012755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	11	11					
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	33	7	2	2	5	4	13
Z0016200	DECK SLAB REPAIR (PARTIAL)	SQ YD	212	50	13	13	35	24	77
Z0030240	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 2	EACH	2	1	1				
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	0.25	0.25		0.25	0.25	
Z0073200	TEMPORARY SHORING AND CRIBBING	EACH	15	2	8				5

* SPECIALTY ITEM

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LEGEND:



EXISTING PROTECTIVE SHIELDING

SITE PLAN

<p>CHRISTIAN-ROGE & ASSOCIATES, INC.</p>	USER NAME = 1001	DESIGNED - J.C.N./B.N.S.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SITE PLAN F.A.I. RTE. I-55 OVER U.S. 41 / LAKE SHORE DRIVE		F.A.I. RTE. 55	SECTION 2011-031-BR	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 4	
	PLOT SCALE = 50.0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -		SHEET NO. 54 OF 541 SHEETS		CONTRACT NO. 60P37		ILLINOIS FED. AID PROJECT			
	PLOT DATE = 10/12/2011	DRAWN - F.M.	REVISED -									
		DATE - OCTOBER 12, 2011	REVISED -									

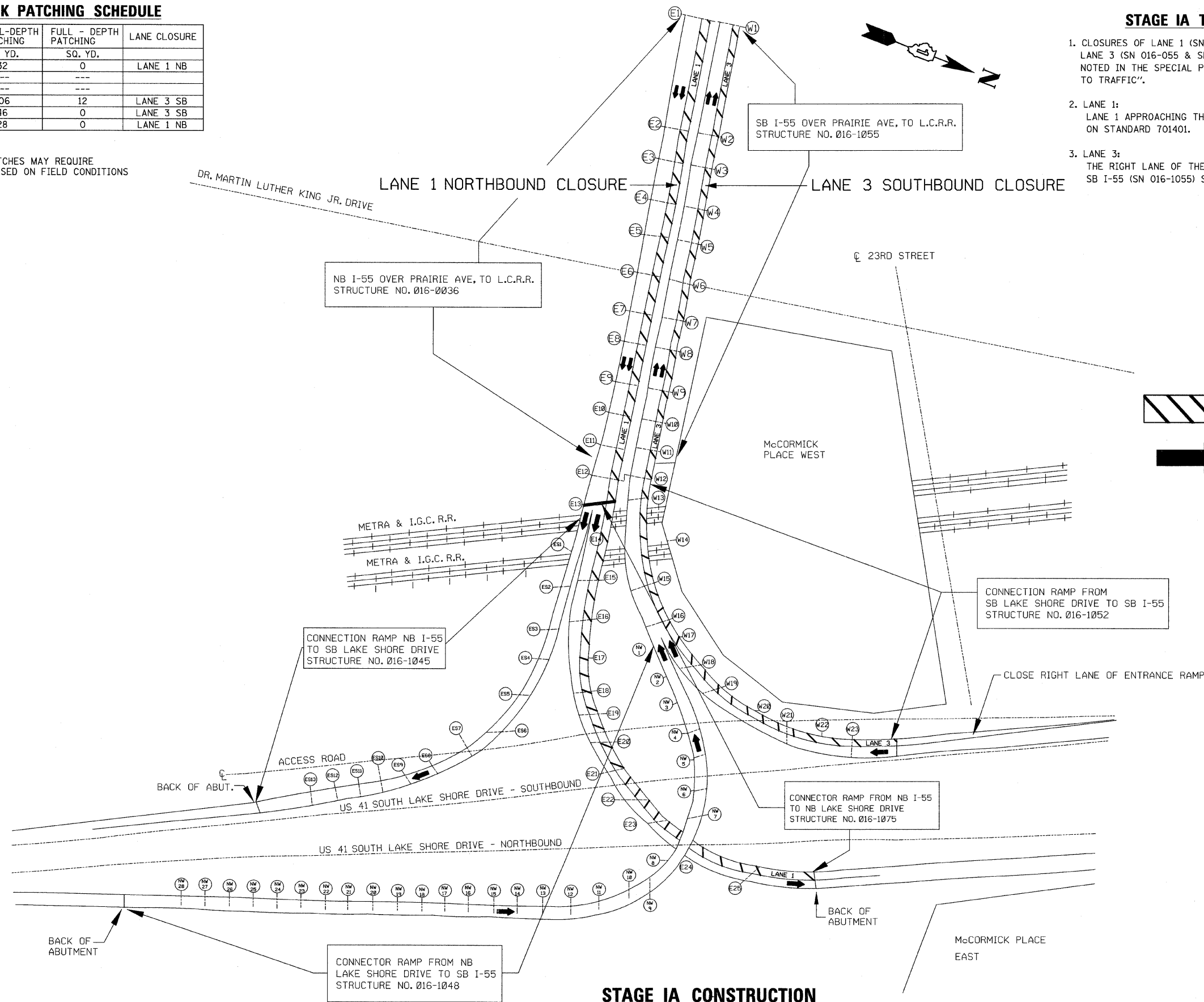
STAGE I DECK PATCHING SCHEDULE

STRUCTURE NO.	LANE NO.	PARTIAL-DEPTH PATCHING SQ. YD.	FULL-DEPTH PATCHING SQ. YD.	LANE CLOSURE
016-0036	1	32	0	LANE 1 NB
016-1045	---	---	---	
016-1048	---	---	---	
016-1052	3	106	12	LANE 3 SB
016-1055	3	46	0	LANE 3 SB
016-1075	1	28	0	LANE 1 NB

NOTE:
SOME PARTIAL-DEPTH PATCHES MAY REQUIRE FULL-DEPTH PATCHING BASED ON FIELD CONDITIONS

STAGE IA TRAFFIC CONTROL NOTES

- CLOSURES OF LANE 1 (SN 016-0036 & SN 016-1075) NB I-55 AND LANE 3 (SN 016-055 & SN 016-0052) SB I-55 WILL BE ALLOWED AS NOTED IN THE SPECIAL PROVISION "KEEPING THE EXPRESSWAY OPEN TO TRAFFIC".
- LANE 1:
LANE 1 APPROACHING THE WORK ZONE SHALL BE CLOSED AS SHOWN ON STANDARD 701401.
- LANE 3:
THE RIGHT LANE OF THE TWO LANE RAMP (SN 016-1052) AND LANE 3 SB I-55 (SN 016-1055) SHALL BE CLOSED AS SHOWN ON STANDARD 701422.



LEGEND:

- WORK ZONE OR LANE CLOSURE
- TRAFFIC DIRECTION

STAGE IA CONSTRUCTION

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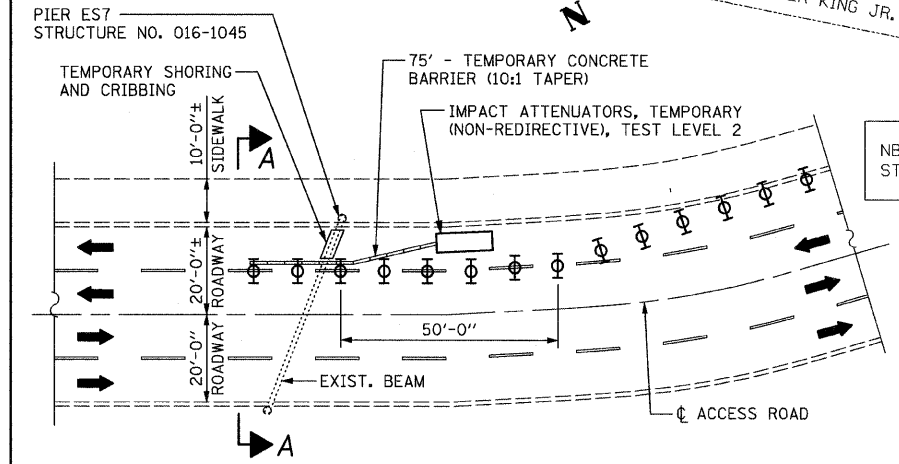
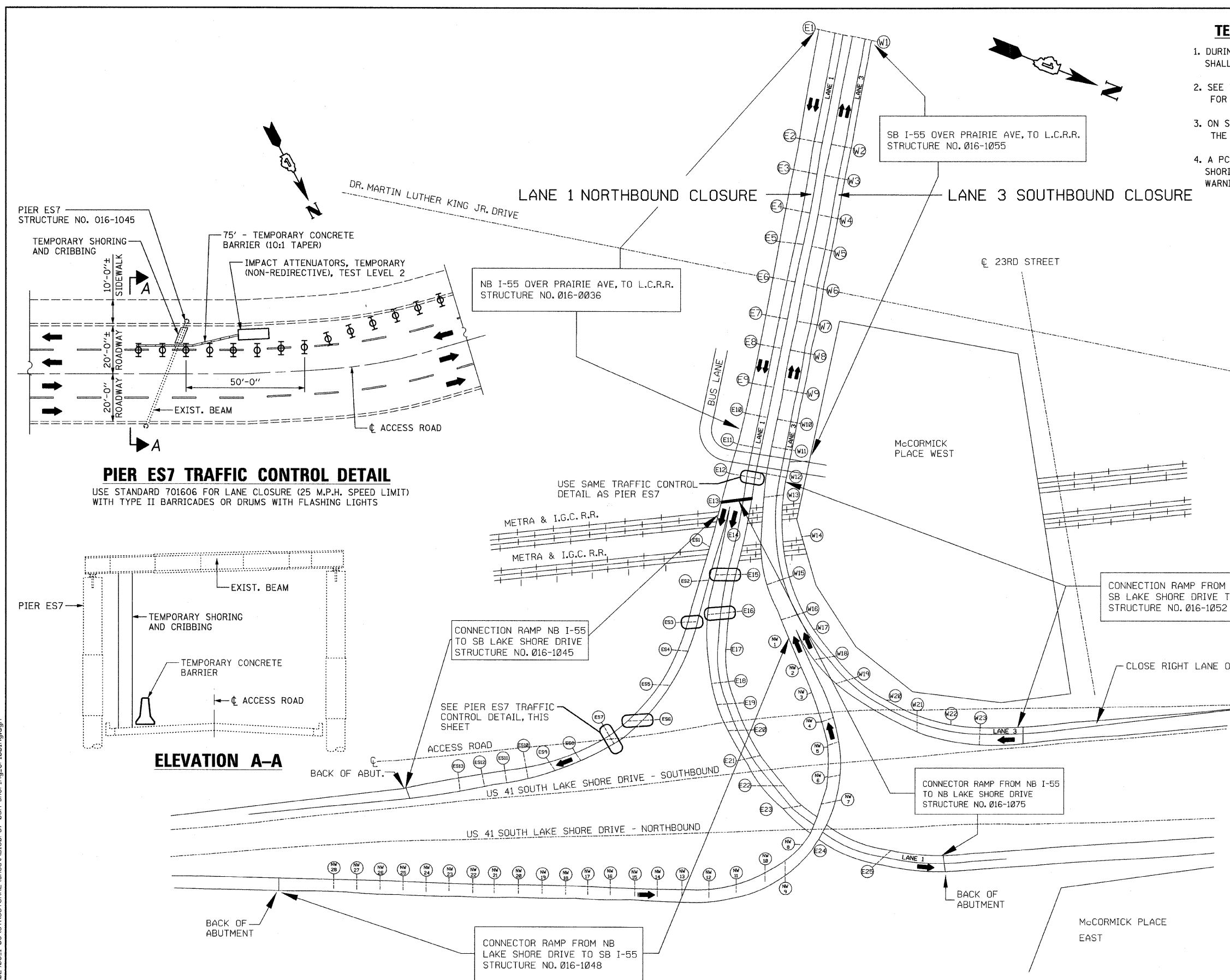
CHRISTIAN-ROGE & ASSOCIATES, INC.	USER NAME = ID01	DESIGNED - J.C.N./B.N.S.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED STAGE IA CONSTRUCTION F.A.I. RTE. I-55 OVER U.S. 41 / LAKE SHORE DRIVE	F.A.I. RTE. 55	SECTION 2011-031-BR	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 5
	PLOT SCALE = 50.0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -				SHEET NO. 55 OF 541 SHEETS		CONTRACT NO. 60P37	
	PLOT DATE = 10/12/2011	DATE - OCTOBER 12, 2011	REVISED -							

TEMPORARY SHORING & CRIBBING NOTES:

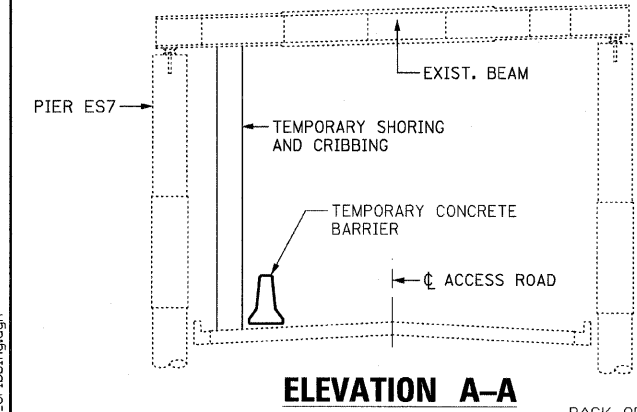
1. DURING THE JACKING AND LOWERING OF THE BEAMS ALL TRAFFIC SHALL BE STOPPED ON THE AFFECTED SPAN.
2. SEE THE "KEEP EXPRESSWAY OPEN TO TRAFFIC" SPECIAL PROVISION FOR ALLOWABLE CLOSURE HOURS AND TIME LIMITS.
3. ON SN 016-1045, THE CONTRACTOR SHALL REMOVE THE JACKS DURING THE DETOUR OF THE RAMP TRAFFIC.
4. A PCMS SHALL BE IN PLACE 48 HOURS PRIOR TO AND DURING TEMPORARY SHORING AND CRIBBING TO PROVIDE ADVANCE CLOSURE AND DELAY WARNINGS.

LEGEND:

 PIER REPAIR WITH TEMPORARY SHORING & CRIBBING



PIER ES7 TRAFFIC CONTROL DETAIL
 USE STANDARD 701606 FOR LANE CLOSURE (25 M.P.H. SPEED LIMIT) WITH TYPE II BARRICADES OR DRUMS WITH FLASHING LIGHTS



ELEVATION A-A

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CHRISTIAN-ROGE & ASSOCIATES, INC.

USER NAME = IDOT	DESIGNED - J.C.N./B.N.S.	REVISED -
PLOT SCALE = 50.0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -
PLOT DATE = 10/12/2011	DRAWN - F.M.	REVISED -
	DATE - OCTOBER 12, 2011	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGING FOR SHORING & CRIBBING
 F.A.I. RTE. I-55 OVER U.S. 41 / LAKE SHORE DRIVE**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2011-031-BR	COOK	41	5A
CONTRACT NO. 60P37				
ILLINOIS FED. AID PROJECT				

SHEET NO. 55A OF 541 SHEETS

STAGE IB DECK PATCHING SCHEDULE

STRUCTURE NO.	LANE NO.	PARTIAL-DEPTH PATCHING SQ. YD.	FULL-DEPTH PATCHING SQ. YD.	LANE CLOSURE
016-0036	---	---	---	
016-1045	---	---	---	
016-1048	---	---	---	
* 016-1052	2	113	0	LANE 2 SB
016-1055	---	---	---	
016-1075	2	117	0	LANE 2 NB

*SPAN SP 1 TO SP 7

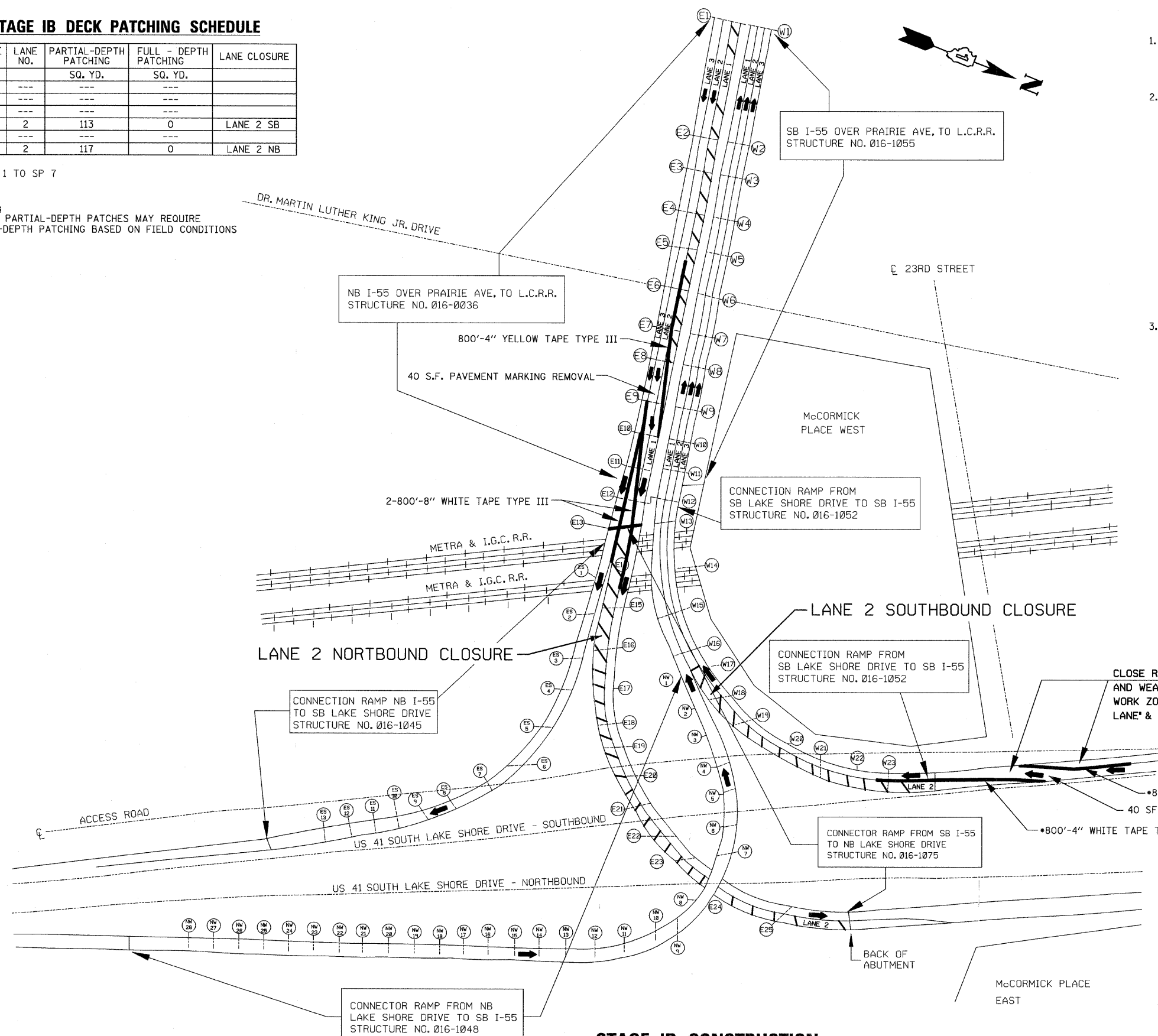
NOTE:
SOME PARTIAL-DEPTH PATCHES MAY REQUIRE FULL-DEPTH PATCHING BASED ON FIELD CONDITIONS

STAGE IB TRAFFIC CONTROL NOTES

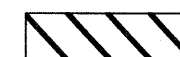

- LANE 2 NB (SN 016-1075) AND LANE 2 SB (SN 016-1052) CLOSURES WILL BE ALLOWED AS NOTED IN THE SPECIAL PROVISION "KEEPING EXPRESSWAY OPEN TO TRAFFIC".
- LANE 2 NB (SN 016-1075)
 - PATCHING DURING STAGE IB WILL ONLY BE ALLOWED IN THE RIGHT LANE OF (SN 016-1075) WHERE THE LANE IS ADJACENT TO THE PARAPET WALL.
 - TRAFFIC CONTROL:
LANE 2 NB (SN 016-1075) SHALL BE CLOSED AS SHOWN. LOCATE WEAVE TO ALLOW PATCHING IN THE GORE AREA & LANE 2 ADJACENT TO THE GORE AREA.

LANE 1 SHALL BE CLOSED AND TRAFFIC APPROACHING THE WORK ZONE SHALL BE REDUCED TO TWO LANES. THE TRAFFIC IN LANE 2 WILL THEN BE WEAVED INTO LANE 1 AS SHOWN ON THE DETAIL FOR "FREEWAY SINGLE LANE" & "MULTI-LANE WEAVE".

WEAVE PLACEMENT - PLACE 4" YELLOW AND 8" WHITE PAVEMENT MARKING TAPE, TYPE III AND REMOVE CONFLICTING PAVEMENT MARKINGS.
- LANE 2 SB (SN 016-1052)
 - PATCHING DURING STAGE IB WILL ONLY BE ALLOWED IN THE LEFT LANE OF (SN 016-1052), WHERE THE LANE IS ADJACENT TO THE PARAPET WALL.
 - TRAFFIC CONTROL:
LANE 2 SB (SN 016-1052) SHALL BE CLOSED AS SHOWN.
THE RIGHT LANE OF THE TWO LANE APPROACH TO RAMP FROM S.B. U.S. 41 TO S.B. I-55 SHALL BE CLOSED.
THE TRAFFIC WILL THEN WEAVE INTO THE RIGHT LANE (LANE 3) OF THE RAMP BEFORE THE WORK ZONE AS SHOWN ON THE "FREEWAY SINGLE LANE WEAVE" DETAIL.



LEGEND:

-  WORK ZONE OR LANE CLOSURE
-  TRAFFIC DIRECTION

*TAPE TYPE III MUST ALSO BE PLACED DURING STAGES II & IIIA

STAGE IB CONSTRUCTION

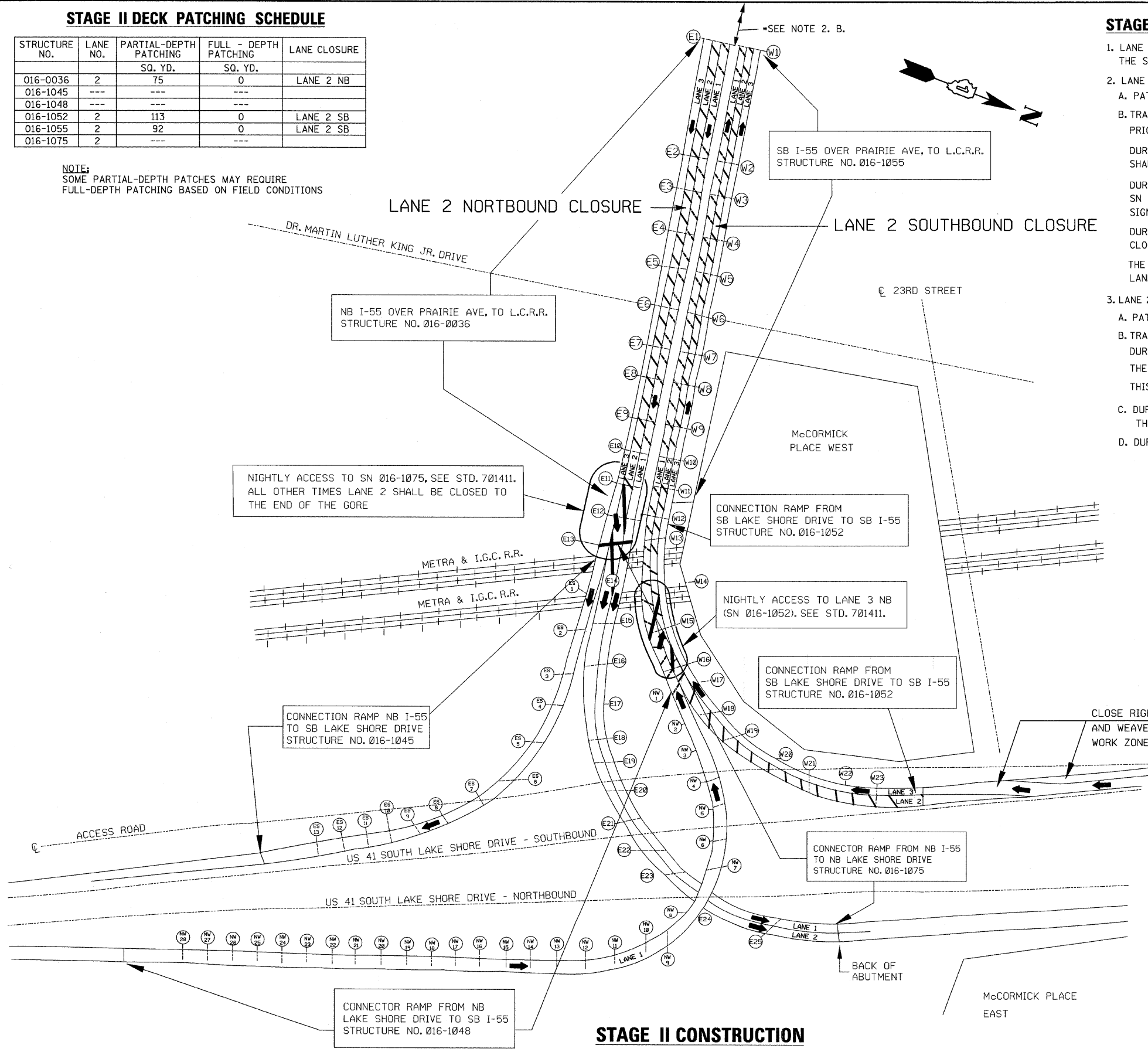
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 CHRISTIAN-ROGE & ASSOCIATES, INC.	USER NAME = IDOT	DESIGNED - J.C.N./B.N.S.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED STAGE IB CONSTRUCTION F.A.I. RTE. I-55 OVER U.S. 41 / LAKE SHORE DRIVE	F.A.I. RTE. 55	SECTION 2011-031-BR	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 6
	PLOT SCALE = 50.0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -			SHEET NO. S6 OF S41 SHEETS			CONTRACT NO. 60P37	
	PLOT DATE = 10/12/2011	DATE - OCTOBER 12, 2011	REVISED -			ILLINOIS FED. AID PROJECT				

STAGE II DECK PATCHING SCHEDULE

STRUCTURE NO.	LANE NO.	PARTIAL-DEPTH PATCHING SQ. YD.	FULL-DEPTH PATCHING SQ. YD.	LANE CLOSURE
016-0036	2	75	0	LANE 2 NB
016-1045	---	---	---	
016-1048	---	---	---	
016-1052	2	113	0	LANE 2 SB
016-1055	2	92	0	LANE 2 SB
016-1075	2	---	---	

NOTE:
SOME PARTIAL-DEPTH PATCHES MAY REQUIRE FULL-DEPTH PATCHING BASED ON FIELD CONDITIONS




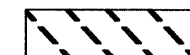

STAGE II TRAFFIC CONTROL NOTES:

- LANE 2 NB (SN 016-0036) AND LANE 2 SB (SN 016-1055) CLOSURES WILL BE ALLOWED AS NOTED IN THE SPECIAL PROVISION "KEEPING EXPRESSWAY OPEN TO TRAFFIC".
- LANE 2 NB (SN 016-0036):
 - PATCHING DURING STAGE II WILL ONLY BE ALLOWED DURING THE NIGHTTIME CLOSURE OF LANE 1 NB.
 - TRAFFIC CONTROL:
 - PRIOR TO WORK ZONE SET UP "CENTER LANE CLOSURE" AS SHOWN ON "CENTER LANE CLOSURE" DETAIL.
 - DURING CONSTRUCTION ACTIVITIES LANE 1 SHALL BE CLOSED NIGHTLY AND ONE LANE OF TRAFFIC SHALL BE MAINTAINED IN LANE 3.
 - DURING THE PATCHING ON LANE 2, THE WEAVE ACCESS TO LANE 1 SHALL BE CLOSED. ACCESS TO SN 016-1075 SHALL BE PROVIDED AS SHOWN ON STANDARD 701411 (APPLICATION 3 OR 4). CONFLICTING SIGNS SHALL BE COVERED AND SIGNING AS SHOWN ON STANDARD 701446 SHALL BE SET UP.
 - DURING PERIODS OF NO CONSTRUCTION ACTIVITIES LANE 1 SHALL BE OPEN AND LANE 2 SHALL BE CLOSED THROUGH THE GORE SEPARATING LANE 2 AND LANE 3.
 - THE TRAFFIC SHALL BE MAINTAINED AS SHOWN ON THE "TRAFFIC CONTROL DETAILS FOR FREEWAY CENTER LANE CLOSURE" DETAIL.
- LANE 2 SB (SN 016-1055):
 - PATCHING DURING STAGE II WILL ONLY BE ALLOWED DURING THE NIGHTTIME CLOSURE OF LANE 1 SB.
 - TRAFFIC CONTROL:
 - DURING CONSTRUCTION ACTIVITIES, ONE LANE OF TRAFFIC SHALL BE MAINTAINED IN LANE 3 SN 016-1055.
 - THE LANE CLOSURES FOR THE APPROACH TO SN 016-1052 SHALL BE THE SAME AS SHOWN IN STAGE 1B. THIS LANE CONFIGURATION SHALL BE MAINTAINED UNTIL LANE 2 IS OPENED TO TRAFFIC.
 - DURING THE CLOSURE OF LANE 1, THE CONTRACTOR SHALL PROVIDE NIGHTLY ACCESS TO LANE 3 FROM THE NB U.S. 41 RAMP (SN 016-1048) AS SHOWN ON THE STANDARD 701411 (APPLICATION 1 OR 2).
 - DURING PERIODS OF NO CONSTRUCTION ACTIVITIES IN LANE 2, LANE 1 SHALL BE OPENED TO TRAFFIC.


***FOR CENTER LANE CLOSURE:**

PLACE: 900' - 8" WHITE TAPE TYPE III
1,000' - 4" YELLOW TAPE TYPE III
40 SF - PAVEMENT MARKING REMOVAL

LEGEND:

-  WORK ZONE OR LANE CLOSURE
-  TEMPORARY NIGHT TIME LANE 1 CLOSURE FOR LANE 2 CONSTRUCTION ACTIVITIES
-  TRAFFIC DIRECTION

FILE NAME = I:\102908_1-55\STRUCTURAL\sheet\0160p37-07-stage-II.dgn

	USER NAME = IDOT	DESIGNED - J.C.N./B.N.S.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED STAGE II CONSTRUCTION F.A.I. RTE. I-55 OVER U.S. 41 / LAKE SHORE DRIVE	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50,0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -			55	2011-031-BR	COOK	41	7
	PLOT DATE = 10/12/2011	DATE - OCTOBER 12, 2011	REVISED -	SHEET NO. 57 OF 541 SHEETS		CONTRACT NO. 60P37				

ILLINOIS FED. AID PROJECT

STAGE IIIB DECK PATCHING SCHEDULE

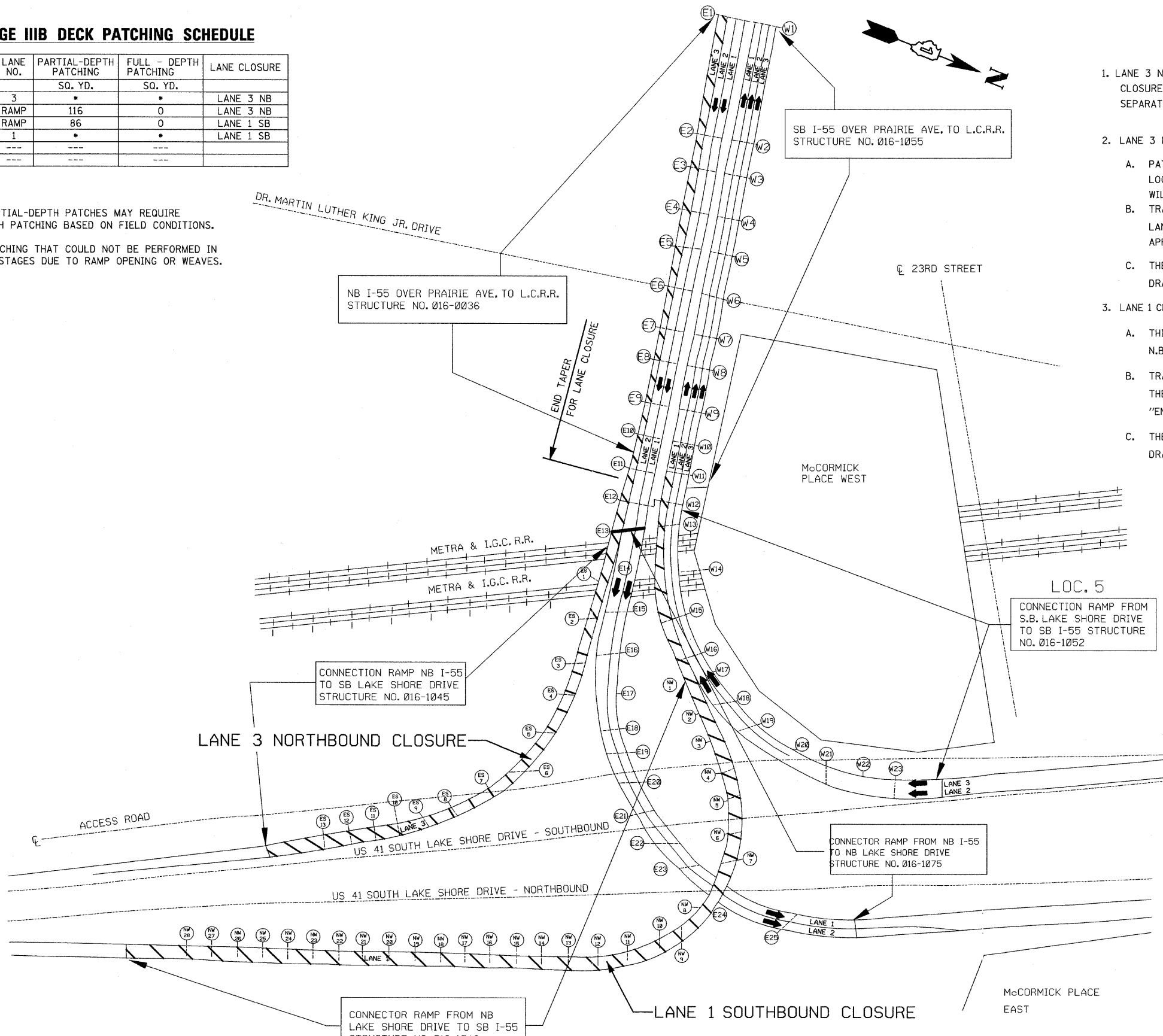
STRUCTURE NO.	LANE NO.	PARTIAL-DEPTH PATCHING SQ. YD.	FULL-DEPTH PATCHING SQ. YD.	LANE CLOSURE
016-0036	3	*	*	LANE 3 NB
016-1045	RAMP	116	0	LANE 3 NB
016-1048	RAMP	86	0	LANE 1 SB
016-1052	1	*	*	LANE 1 SB
016-1055	---	---	---	
016-1075	---	---	---	

NOTES:
SOME PARTIAL-DEPTH PATCHES MAY REQUIRE FULL-DEPTH PATCHING BASED ON FIELD CONDITIONS.

*ANY PATCHING THAT COULD NOT BE PERFORMED IN PREVIOUS STAGES DUE TO RAMP OPENING OR WEAVES.

STAGE IIIB TRAFFIC CONTROL NOTES

- LANE 3 NB (SN 016-0036 & SN 016-1045) AND LANE 1 SB (SN 016-1048 & SN 016-1052) CLOSURES WILL BE ALLOWED, HOWEVER, THESE LANE CLOSURES MUST OCCUR ON SEPARATE WEEKENDS.
- LANE 3 NB (SN 016-0036) & RAMP NB TO SB U.S. 41 (SN 016-1045):
 - PATCHING DURING THIS STAGE WILL BE DONE ON THE RAMP AND ANY REMAINING LOCATIONS IN LANE 3. ALSO DURING THIS STAGE THE JACKS FOR THE PIER REPAIRS WILL BE REMOVED.
 - TRAFFIC CONTROL:
LANE 3 WILL BE CLOSED USING STANDARDS 701400 & 701401. IN ADDITION, THE APPROPRIATE EXIT RAMP CLOSED SIGN SHALL BE INSTALLED.
 - THE RAMP TRAFFIC SHALL BE DETOURED AS SHOWN ON THE "DETOUR PLAN - STAGE IIIB" DRAWING.
- LANE 1 CLOSURE S.B. I-55 & N.B. U.S. 41 RAMP:
 - THIS WORK SHALL CONSIST OF THE CLOSURE OF LANE 1 SB AND THE RAMP FROM N.B. U.S. 41 (SN 016-1048).
 - TRAFFIC CONTROL:
THE ENTRANCE OF THE N.B. U.S. 41 RAMP SHALL BE CLOSED AS SHOWN ON THE "ENTRANCE AND EXIT RAMP CLOSURE DETAIL".
 - THE RAMP TRAFFIC SHALL BE DETOURED AS SHOWN ON THE "DETOUR PLAN - STAGE IIIB" DRAWING.



LEGEND:

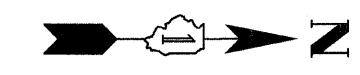
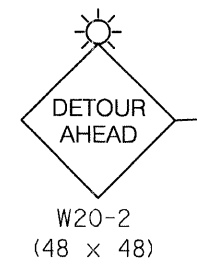
- WORK ZONE OR LANE CLOSURE
- TRAFFIC DIRECTION

STAGE IIIB CONSTRUCTION

FILE NAME = I:\102908.I-55\STRUCTURAL\sheet\0160P37-09-stage-IIIB.dgn

	USER NAME = IDOT	DESIGNED - J.C.N./B.N.S.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED STAGE IIIB CONSTRUCTION F.A.I. RTE. I-55 OVER U.S. 41 / LAKE SHORE DRIVE	F.A.I. RTE. 55	SECTION 2011-031-BR	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 9	
	PLOT SCALE = 50,0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -			CONTRACT NO. 60P37					
	PLOT DATE = 10/12/2011	DATE - OCTOBER 12, 2011	REVISED -			ILLINOIS FED. AID PROJECT					

- (A)**
DETOUR
M4-8
(24 x 12)
- (B)**
END DETOUR
M4-8a
(24 x 18)
- (C)**
SOUTH
M3-3
(21 x 9)
- (D)**
55
M1-1
(24 x 24)
- (E)**
41
M1-4
(24 x 24)
- (F)**
←
M5-1(L)
(21 x 15)
- (G)**
←
M6-1(L)
(21 x 15)
- (H)**
↑
M6-3
(21 x 15)
- (I)**
↗
M6-2(R)
(21 x 15)
- (J)**
↗
M5-2
(21 x 15)



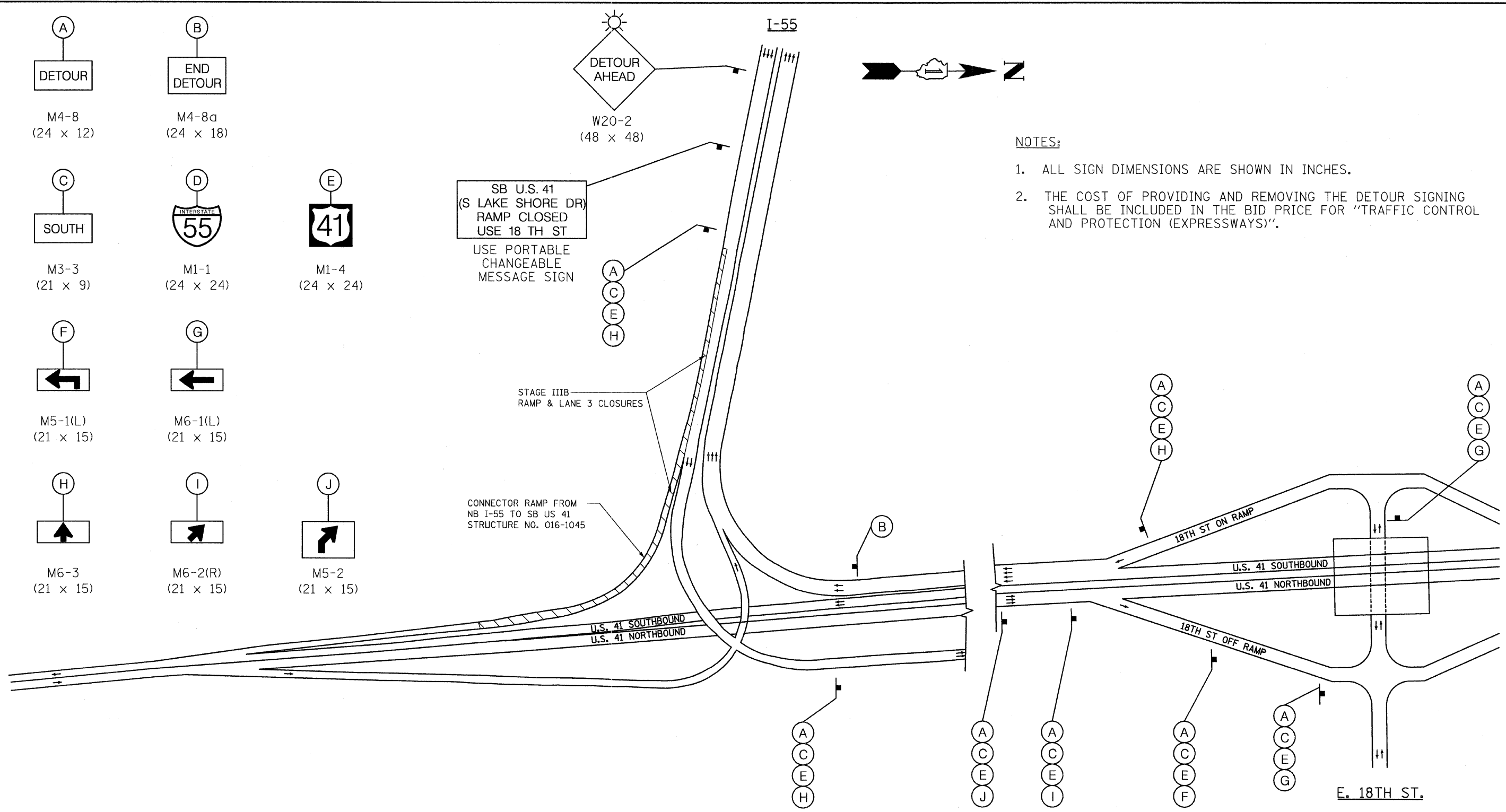
SB U.S. 41
(S LAKE SHORE DR)
RAMP CLOSED
USE 18 TH ST
USE PORTABLE
CHANGEABLE
MESSAGE SIGN

STAGE IIIB
RAMP & LANE 3 CLOSURES

CONNECTOR RAMP FROM
NB I-55 TO SB US 41
STRUCTURE NO. 016-1045

NOTES:

1. ALL SIGN DIMENSIONS ARE SHOWN IN INCHES.
2. THE COST OF PROVIDING AND REMOVING THE DETOUR SIGNING SHALL BE INCLUDED IN THE BID PRICE FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)".

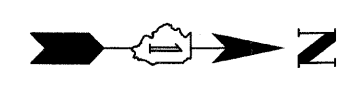


DETOUR STAGE IIIB
N.B. I-55 TO S.B. U.S. 41

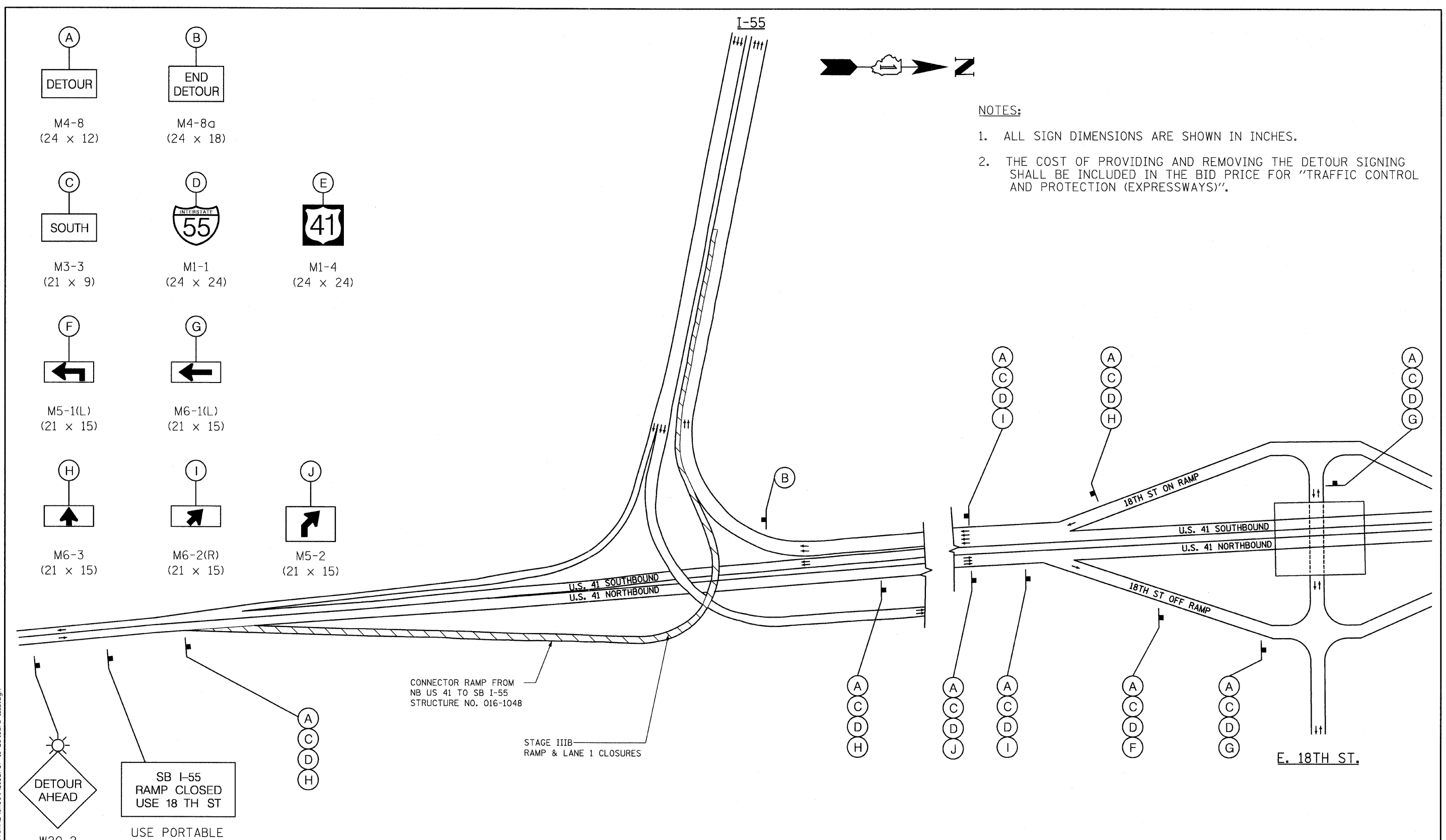
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	PLOT SCALE = 99.9999 1 / IN.	CHECKED - B.N.S./J.C.N.	REVISED -								
	PLOT DATE = 10/12/2011	DRAWN - F.M.	REVISED -								
		DATE - OCTOBER 12, 2011	REVISED -								
SHEET NO. S10 OF S41 SHEETS						CONTRACT NO. 60P37 ILLINOIS FED. AID PROJECT					

- (A) **DETOUR**
M4-8
(24 x 12)
- (B) **END DETOUR**
M4-8a
(24 x 18)
- (C) **SOUTH**
M3-3
(21 x 9)
- (D) **INTERSTATE 55**
M1-1
(24 x 24)
- (E) **41**
M1-4
(24 x 24)
- (F) **←**
M5-1(L)
(21 x 15)
- (G) **←**
M6-1(L)
(21 x 15)
- (H) **↑**
M6-3
(21 x 15)
- (I) **↗**
M6-2(R)
(21 x 15)
- (J) **↗**
M5-2
(21 x 15)



- NOTES:**
- ALL SIGN DIMENSIONS ARE SHOWN IN INCHES.
 - THE COST OF PROVIDING AND REMOVING THE DETOUR SIGNING SHALL BE INCLUDED IN THE BID PRICE FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)".

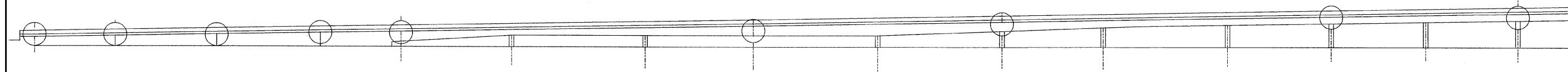


DETOUR STAGE IIIB
N.B. U.S. 41 TO S.B. I-55

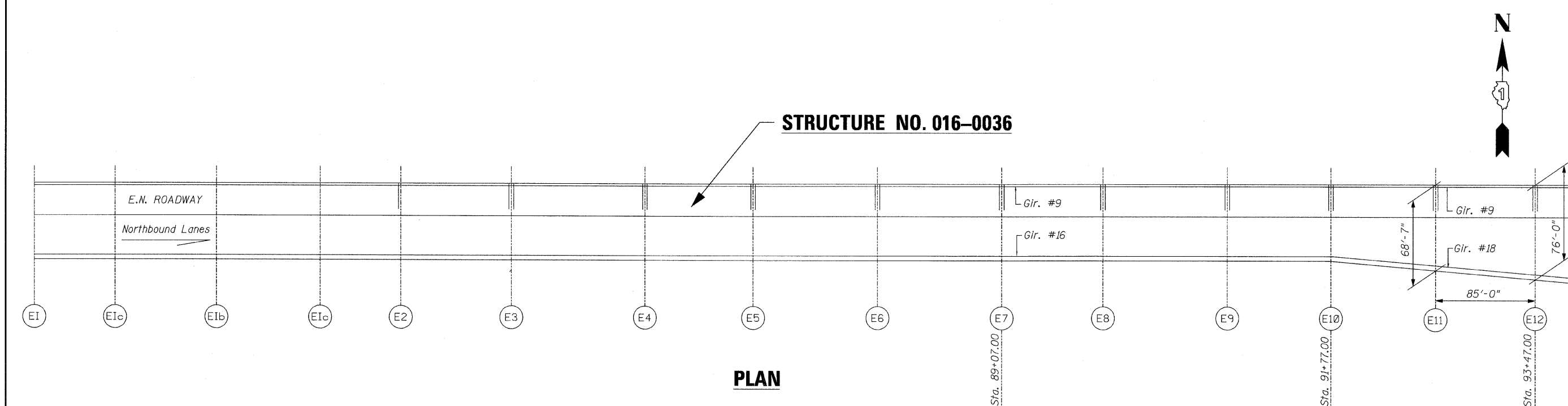
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	PLOT SCALE = 99.9999' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -			CONTRACT NO. 60P37				
	PLOT DATE = 10/12/2011	DRAWN - F.M.	REVISED -			SHEET NO. S11 OF S41 SHEETS				
	DATE - OCTOBER 12, 2011	REVISED -	ILLINOIS FED. AID PROJECT							

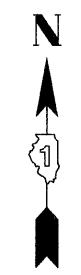
SN 016-1045 (N.B.)
 SN 016-1052 (N.B.)
 SN 016-0036 (N.B.)



ELEVATION - E.N. ROADWAY



PLAN



PAVEMENT PATCHING NOTE:

15 SY(±) PAVEMENT PATCHING IS REQUIRED IN LANE 2 WEST OF THE APPROACH PAVEMENT.

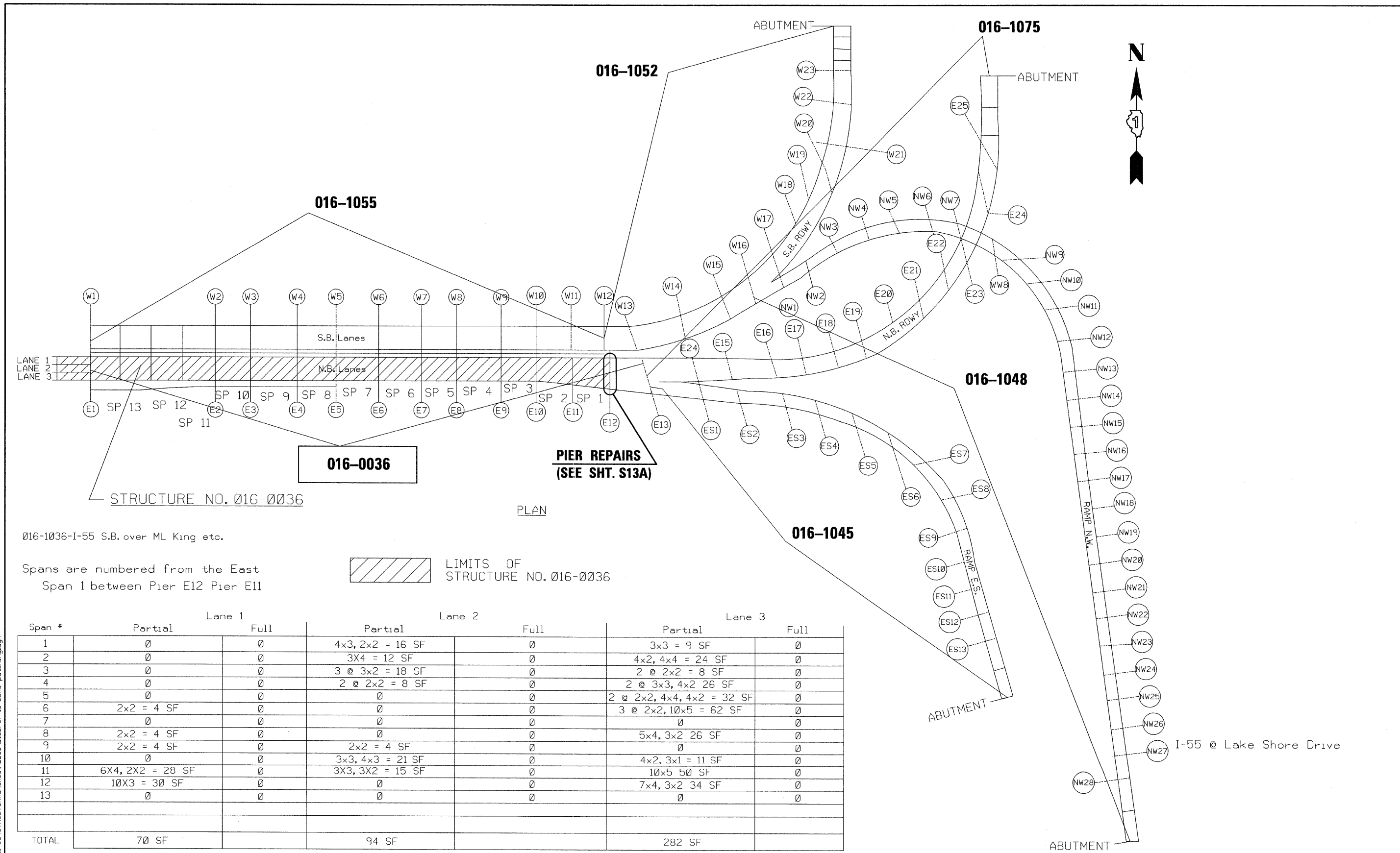
ESTIMATED QUANTITIES ARE AS FOLLOWS:

- 15 SY - CLASS A PATCHES, TYPE III 12 INCH.
- 15 SY - PATCHING REINFORCEMENT
- 68 FT - SAWCUTS
- 10 EACH - TIE BARS, 1"

GENERAL PLAN & ELEVATION
F.A.I. RTE. I-55 OVER
U.S. 41/LAKE SHORE DRIVE
COOK COUNTY
STRUCTURE NO. 016-0036

FILE NAME = I:\102908.I-55\STRUCTURAL\Drawings\016-0036\0160037-12-GPE.dgn

CHRISTIAN-ROGE & ASSOCIATES, INC.	USER NAME == IDOT	DESIGNED - J.C.N./B.N.S.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHEET NO. 512 OF 541 SHEETS	F.A.I. RTE. 55	SECTION 2011-031-BR	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 12
	PLOT SCALE = 50.0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -			CONTRACT NO. 60P37				
	PLOT DATE == 10/12/2011	DRAWN - F.M.	REVISED -			ILLINOIS FED. AID PROJECT				
	DATE - OCTOBER 12, 2011	REVISED -								



016-1036-I-55 S.B. over ML King etc.

Spans are numbered from the East
Span 1 between Pier E12 Pier E11

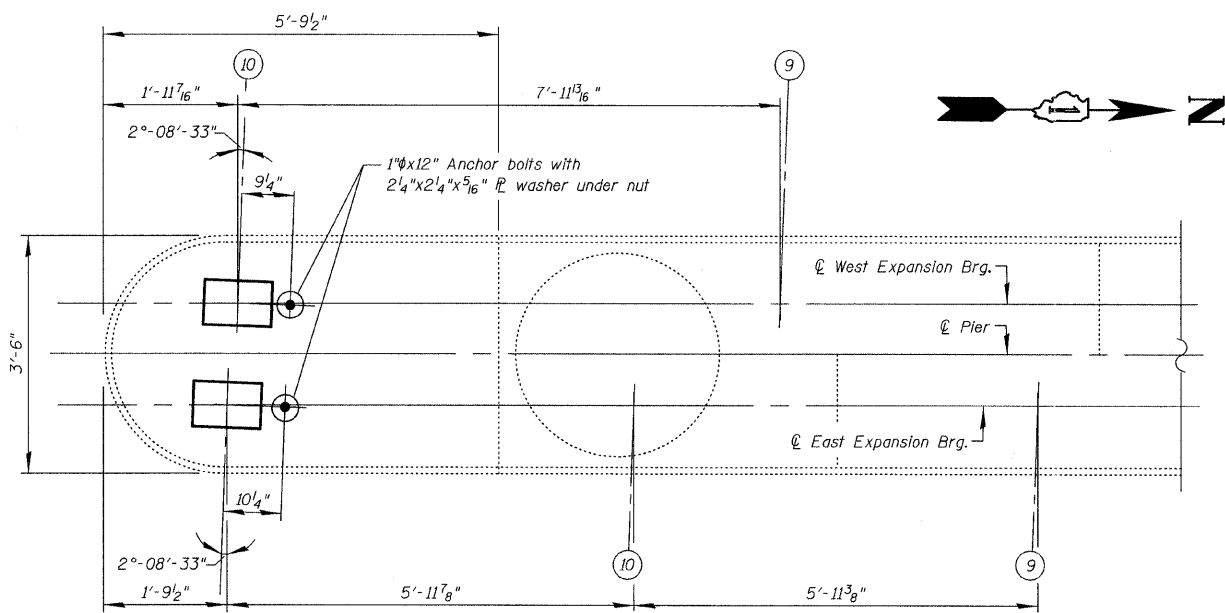
LIMITS OF STRUCTURE NO. 016-0036

Span #	Lane 1		Lane 2		Lane 3	
	Partial	Full	Partial	Full	Partial	Full
1	0	0	4x3, 2x2 = 16 SF	0	3x3 = 9 SF	0
2	0	0	3x4 = 12 SF	0	4x2, 4x4 = 24 SF	0
3	0	0	3 @ 3x2 = 18 SF	0	2 @ 2x2 = 8 SF	0
4	0	0	2 @ 2x2 = 8 SF	0	2 @ 3x3, 4x2 26 SF	0
5	0	0	0	0	2 @ 2x2, 4x4, 4x2 = 32 SF	0
6	2x2 = 4 SF	0	0	0	3 @ 2x2, 10x5 = 62 SF	0
7	0	0	0	0	0	0
8	2x2 = 4 SF	0	0	0	5x4, 3x2 26 SF	0
9	2x2 = 4 SF	0	2x2 = 4 SF	0	0	0
10	0	0	3x3, 4x3 = 21 SF	0	4x2, 3x1 = 11 SF	0
11	6x4, 2x2 = 28 SF	0	3x3, 3x2 = 15 SF	0	10x5 50 SF	0
12	10x3 = 30 SF	0	0	0	7x4, 3x2 34 SF	0
13	0	0	0	0	0	0
TOTAL	70 SF		94 SF		282 SF	

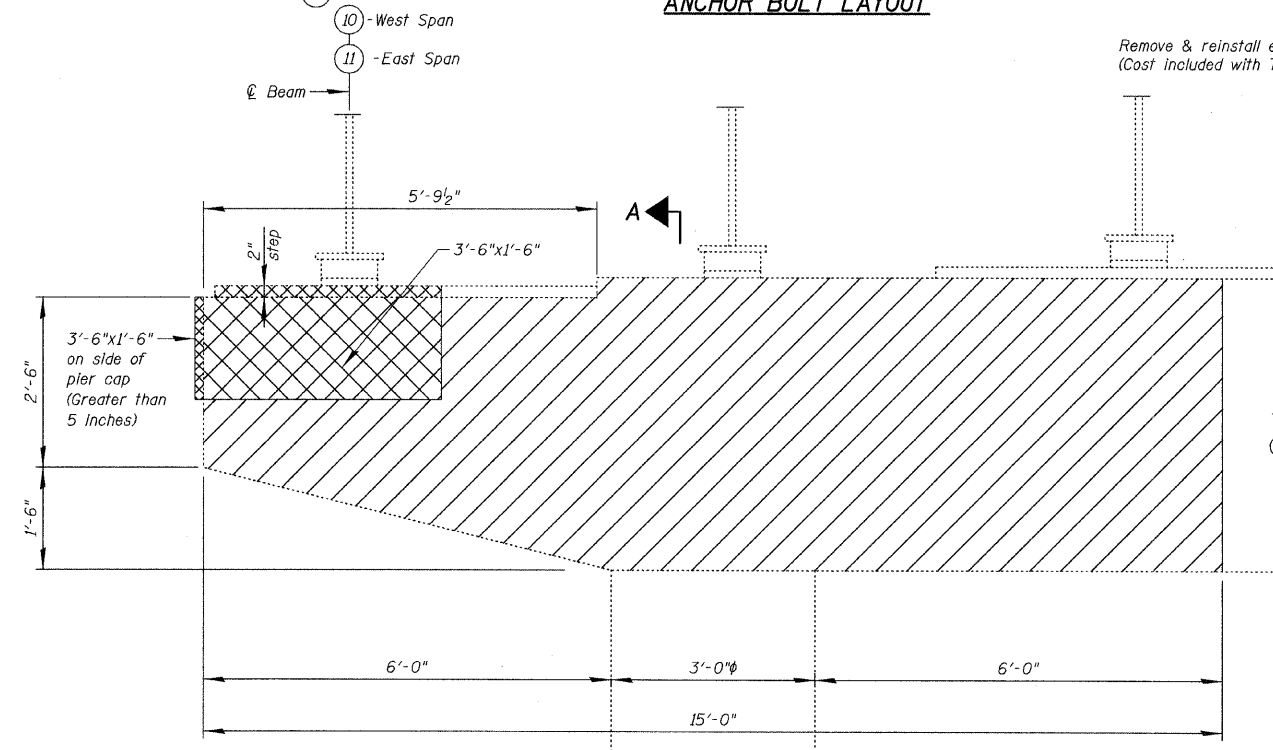
TOTAL : PARTIAL DEPTH GRAND TOTAL = 446 SF
POTENTIAL FULL DEPTH GRAND TOTAL = 67 SF

FILE NAME = I:\025008.I-55\STRUCTURAL\Sheet\0036\0160036-13-conc patching.dgn

	USER NAME == IDOT	DESIGNED - J.C.N./B.N.S.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DECK CONCRETE PATCHING STRUCTURE NO. 016-0036	F.A.I. R.T.E. 55	SECTION 2011-031-BR	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 13		
	PLOT SCALE = 50.0000 "/> <td>DRAWN - F.M.</td> <td>REVISED -</td> <td colspan="5" style="text-align: center;">SHEET NO. S13 OF S41 SHEETS</td> <td colspan="2" style="text-align: center;">CONTRACT NO. 60P37</td>	DRAWN - F.M.	REVISED -			SHEET NO. S13 OF S41 SHEETS					CONTRACT NO. 60P37	
	PLOT DATE == 10/12/2011	DATE - OCTOBER 12, 2011	REVISED -			ILLINOIS FED. AID PROJECT						

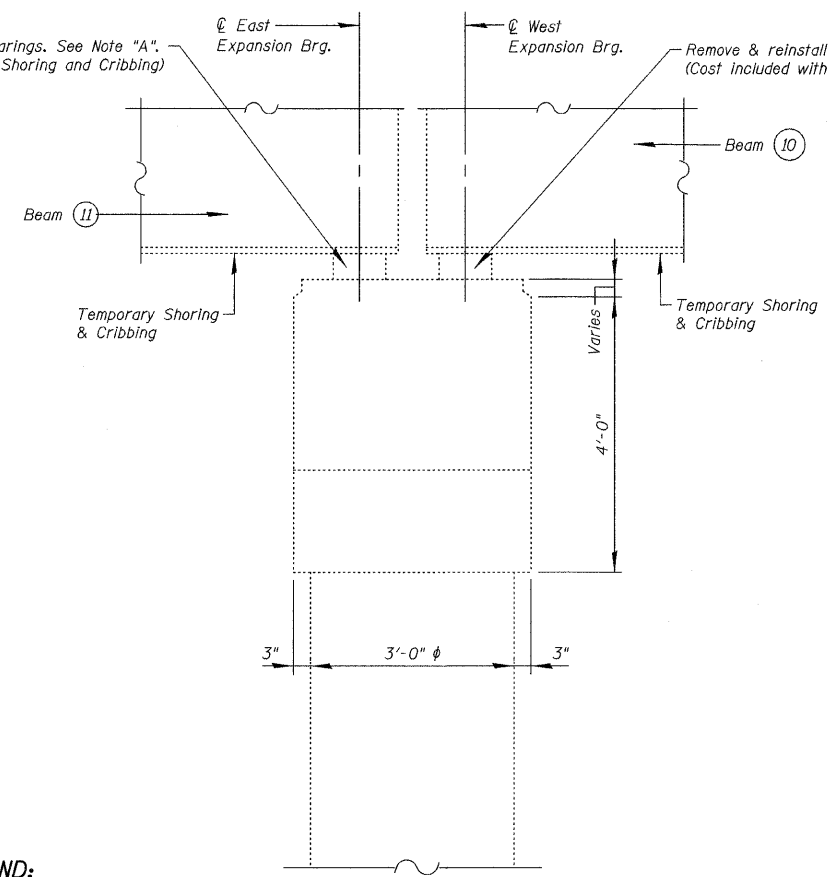


ANCHOR BOLT LAYOUT



ELEVATION - SOUTH END
(Looking West)

Remove & reinstall existing bearings. See Note "A".
(Cost included with Temporary Shoring and Cribbing)



SECTION A-A

LEGEND:

- Structural Repair of Concrete (Depth Equal to or less than 5 Inches)
- Structural Repair of Concrete (Depth Greater than 5 Inches)

Notes:
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
 It shall be the Contractor's responsibility to verify all dimensions between the bottom of the bridge beams and the top of the bearing seats, in the field, prior to construction or ordering of materials. The Contractor shall supply additional shim plates, if required, to bring devices to grade. Cost included with Temporary Shoring and Cribbing.
 Anchor bolts must be installed in holes drilled after supported member is in place. Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications. Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade and diameter specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy-36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
 Temporary shoring and cribbing, as described in the Special Provisions, is to be installed prior to any Structural Repair of Concrete.
 If the analysis submitted to the Contractor for the jacking/temporary support system to be used shows temporary stiffeners are required to prevent web crippling or buckling, the stiffeners shall be steel and bolted to the web. If stiffeners are not required, hardwood timbers shall be installed tightly between the top and bottom flange to prevent flange rotation.

Note "A"
 Bearings are both bolted and welded to bottom flange of beams. However, when reinstalling bearings, the bearings must only be bolted to the beams. No field welding is allowed.

BEAM REACTION TABLE		
	at West Expansion Bearings	at East Expansion Bearings
R ₁ (k)	27.5	38.6
R ₂ (k)	11.6	14.7
R ₃ (k)	45.2	46.6
R ₄ (k)	10.8	11.1
R _{Total} (k)	95.1	111.0
Minimum Jack Capacity	75 tons	85 tons

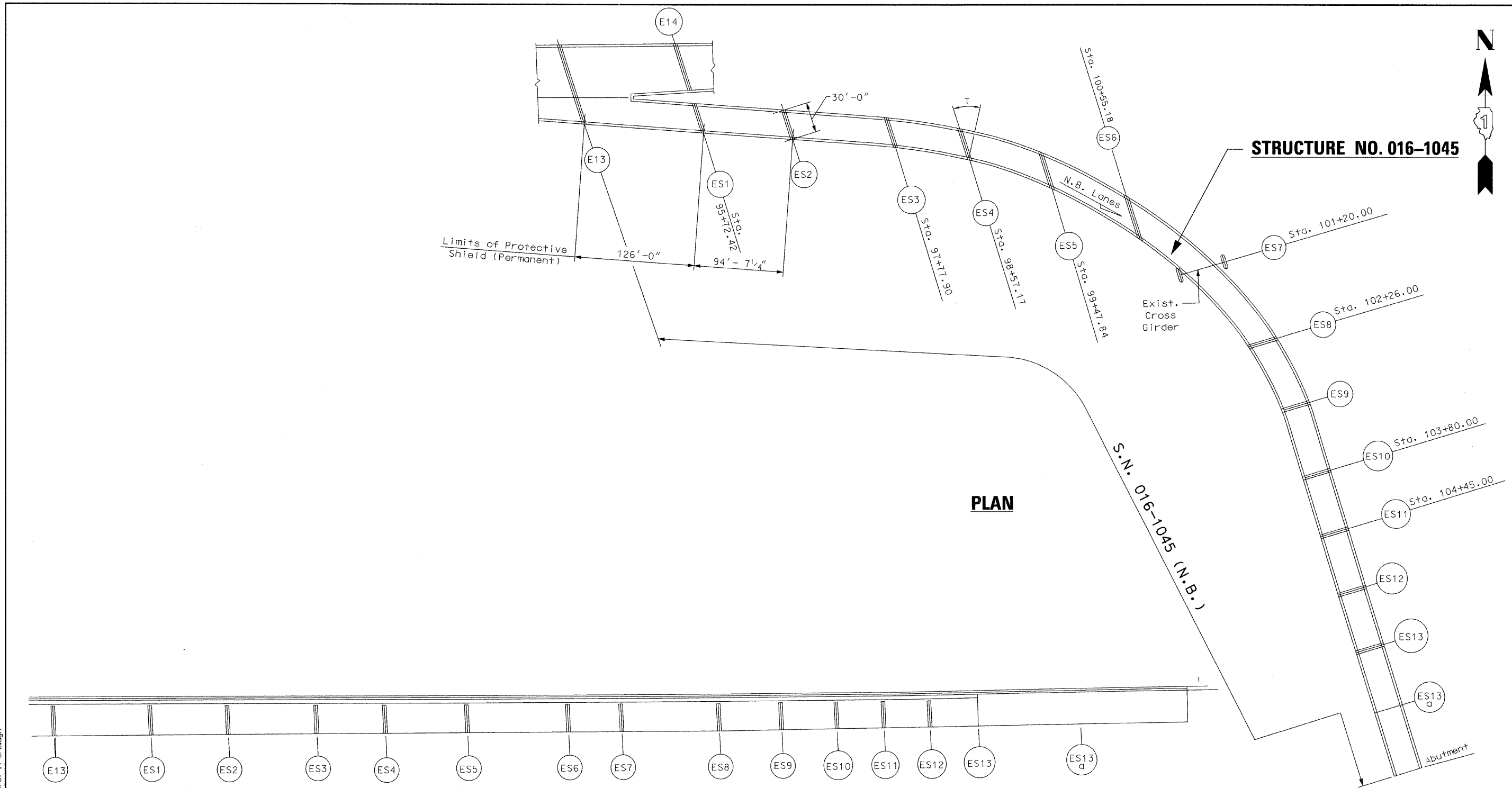
Girder reactions shown are from the existing plan Bearing Data Tables, At Roadway E.N. & Ramp E.S. from S. Prairie Ave. to S. Lake Shore Dr. F.A.I. Rte. 1-55, dated June 18, 1991

BILL OF MATERIAL

Item	Unit	Quantity
Anchor Bolts, 1"	Each	2
Structural Repair of Concrete (Depth Equal to or less than 5 Inches)	Sq. Ft.	54
Structural Repair of Concrete (Depth Greater than 5 Inches)	Sq. Ft.	11
Temporary Shoring and Cribbing	Each	2

FILE NAME = J:\182908.1-55\STRUCTURAL\Drawings\0636\0160P37-13A-pier-E12.dgn

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	PLLOT SCALE = 50.0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -			55	2011-031-BR	COOK	41	13A
	PLLOT DATE == 10/12/2011	DATE - OCTOBER 12, 2011	REVISED -			CONTRACT NO. 60P37				
	SHEET NO. S13A OF S41 SHEETS						ILLINOIS FED. AID PROJECT			



PLAN

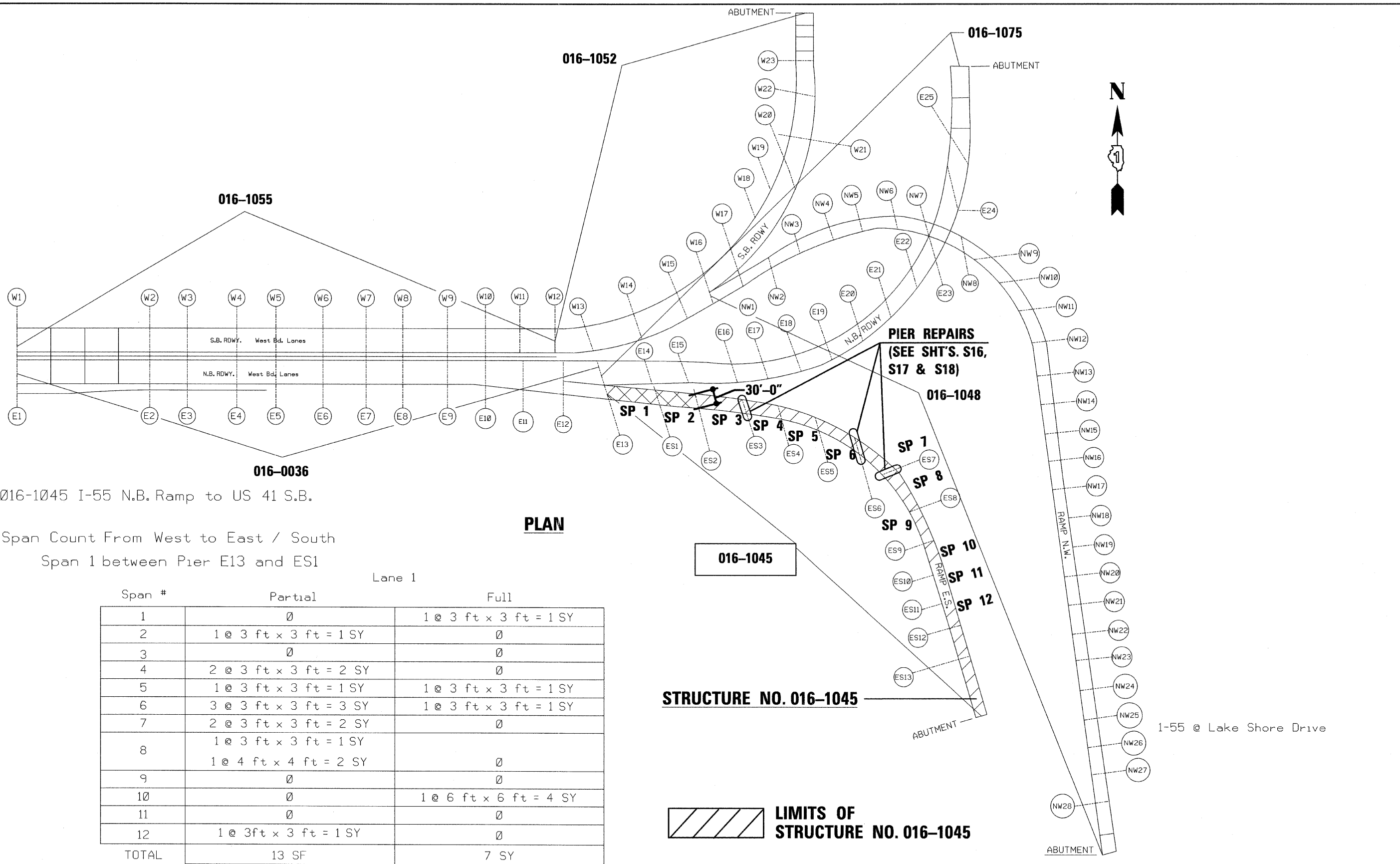
ELEVATION-RAMP E.S.

FOR INFORMATION ONLY

**GENERAL PLAN & ELEVATION
F.A.I. RTE. I-55 OVER
U.S. 41/LAKE SHORE DRIVE
COOK COUNTY
STRUCTURE NO. 016-1045**

FILE NAME = I:\102908_1-55\STRUCTURAL\shoe\1045\016037-14-DPE.dgn

CHRISTIAN-ROGE & ASSOCIATES, INC.	USER NAME == IDOT	DESIGNED - J.C.N./B.N.S.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. RTE. 55	SECTION 2011-031-BR	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 14
	PLOT SCALE = 50.0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -		SHEET NO. S14 OF S41 SHEETS	CONTRACT NO. 60P37		ILLINOIS FED. AID PROJECT	
	PLOT DATE == 10/12/2011	DRAWN - F.M.	REVISED -						
	DATE - OCTOBER 12, 2011	REVISED -							



016-1055

S.B. RDWY. West Bd. Lanes

N.B. RDWY. West Bd. Lanes

016-0036

016-1045 I-55 N.B. Ramp to US 41 S.B.

Span Count From West to East / South

Span 1 between Pier E13 and ES1

PLAN

Span #	Lane 1	
	Partial	Full
1	0	1 @ 3 ft x 3 ft = 1 SY
2	1 @ 3 ft x 3 ft = 1 SY	0
3	0	0
4	2 @ 3 ft x 3 ft = 2 SY	0
5	1 @ 3 ft x 3 ft = 1 SY	1 @ 3 ft x 3 ft = 1 SY
6	3 @ 3 ft x 3 ft = 3 SY	1 @ 3 ft x 3 ft = 1 SY
7	2 @ 3 ft x 3 ft = 2 SY	0
8	1 @ 3 ft x 3 ft = 1 SY	0
	1 @ 4 ft x 4 ft = 2 SY	
9	0	0
10	0	1 @ 6 ft x 6 ft = 4 SY
11	0	0
12	1 @ 3ft x 3 ft = 1 SY	0
TOTAL	13 SF	7 SY

TOTAL : PARTIAL DEPTH GRAND TOTAL = 116 SF
 POTENTIAL FULL DEPTH GRAND TOTAL = 18 SF

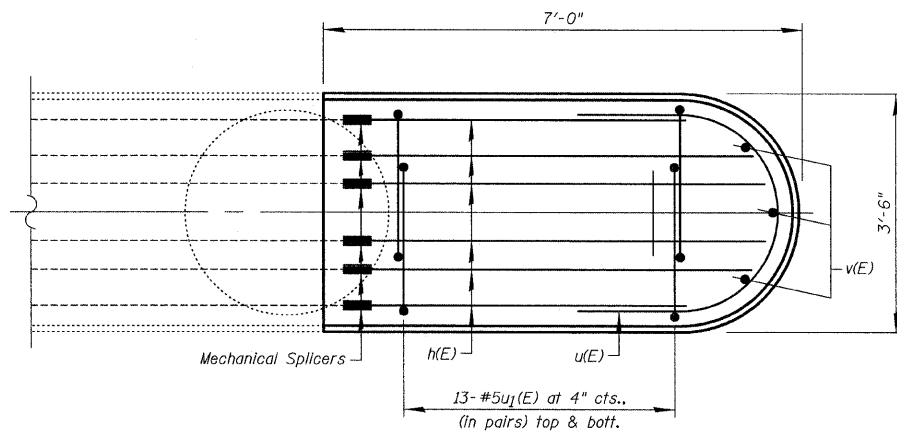
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LIMITS OF STRUCTURE NO. 016-1045

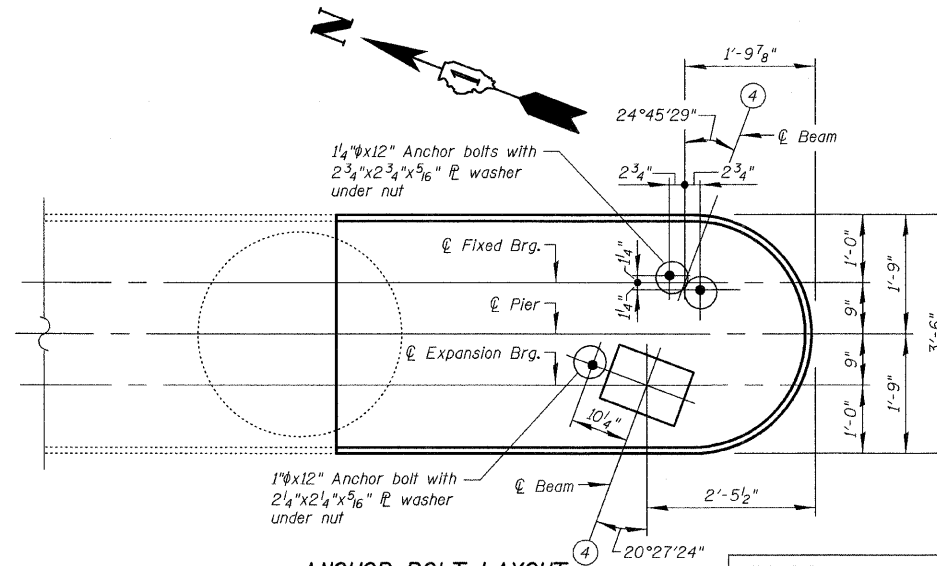
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	PLOT DATE == 10/12/2011	DRAWN - F.M.	REVISED -							
	DATE - OCTOBER 12, 2011	REVISED -				ILLINOIS FED. AID PROJECT				

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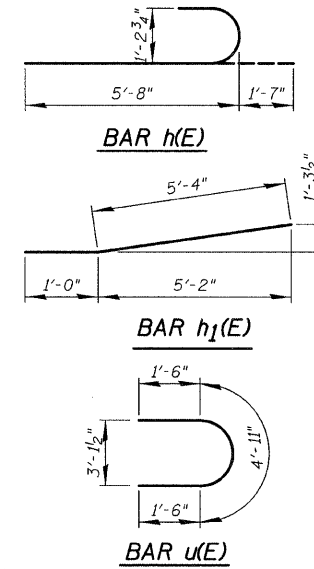


TOP PLAN



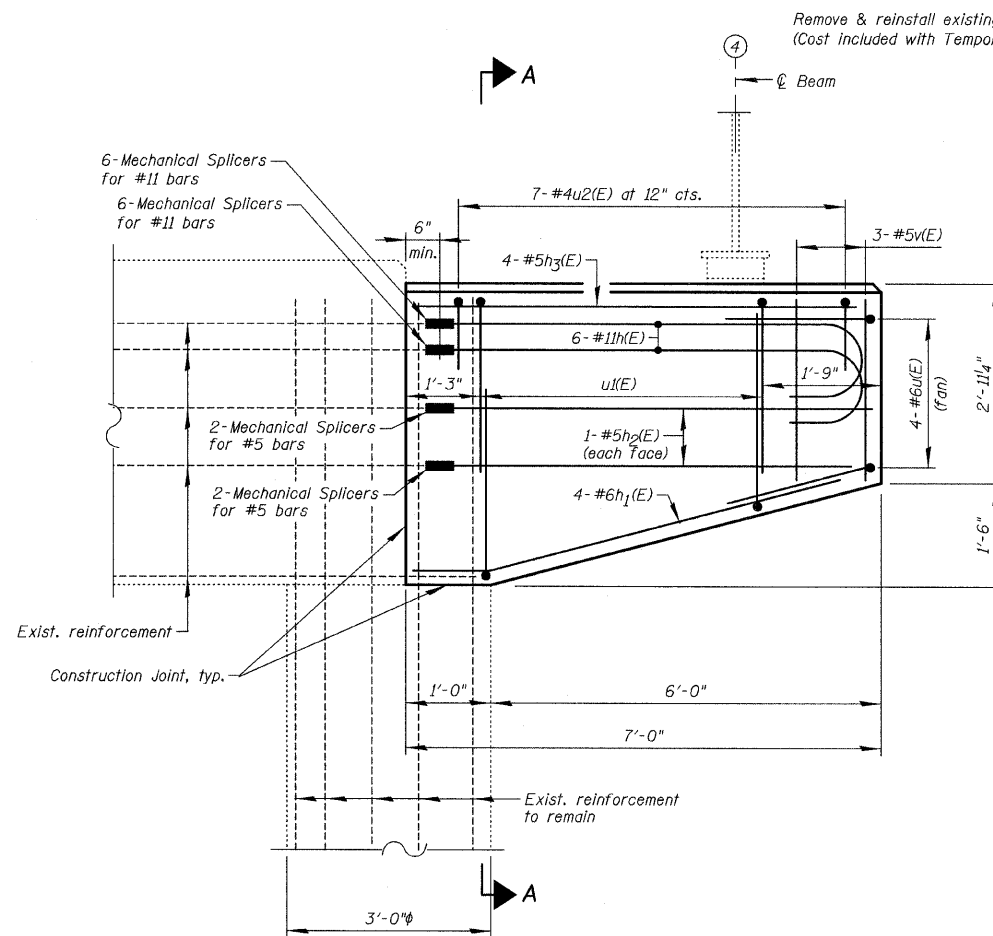
ANCHOR BOLT LAYOUT

Note "A"
Bearings are both bolted and welded to bottom flange of beams. However, when reinstalling bearings, the bearings must only be bolted to the beams. No field welding is allowed.

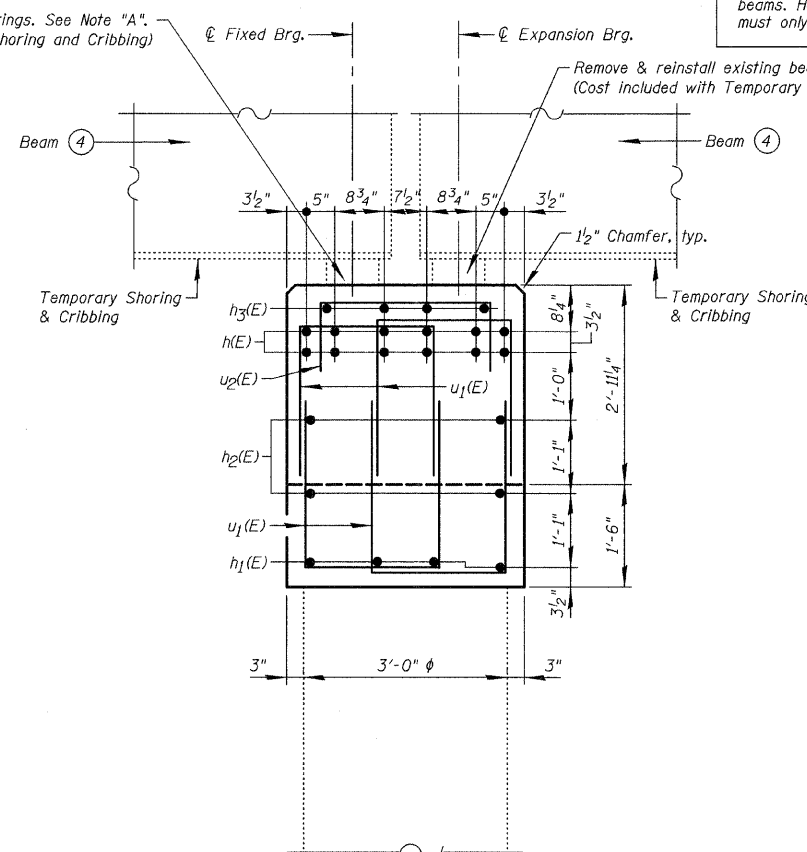


BILL OF MATERIAL

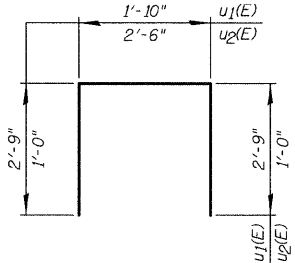
Bar	No.	Size	Length	Shape
h1(E)	12	#11	7'-3"	U
h2(E)	4	#6	6'-4"	—
h3(E)	4	#5	5'-8"	—
u1(E)	4	#6	7'-11"	U
u2(E)	52	#5	7'-4"	□
u3(E)	7	#4	4'-6"	□
v(E)	3	#5	2'-8"	—
Concrete Removal			Cu. Yd.	3.1
Concrete Structures			Cu. Yd.	3.1
Reinforcement Bars, Epoxy Coated			Pound	1,030
Anchor Bolts, 1"			Each	1
Anchor Bolts, 1 1/4"			Each	2
Mechanical Splicers			Each	16
Temporary Shoring and Cribbing			Each	2



ELEVATION - SOUTH END
(Looking East)



SECTION A-A



BARS u1(E) & u2(E)

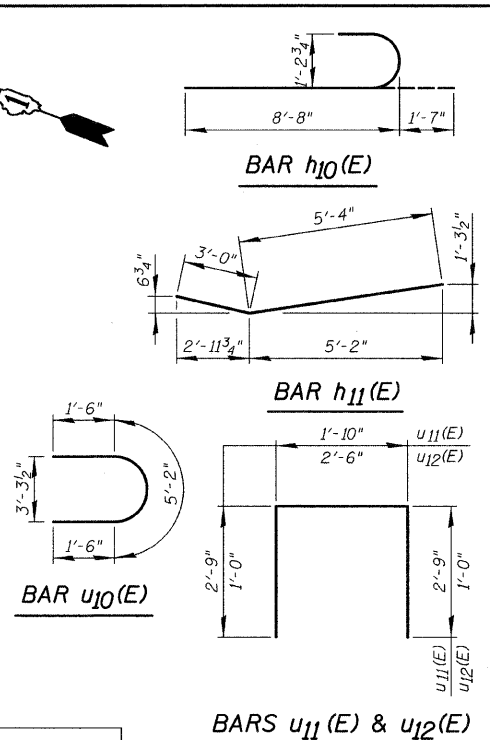
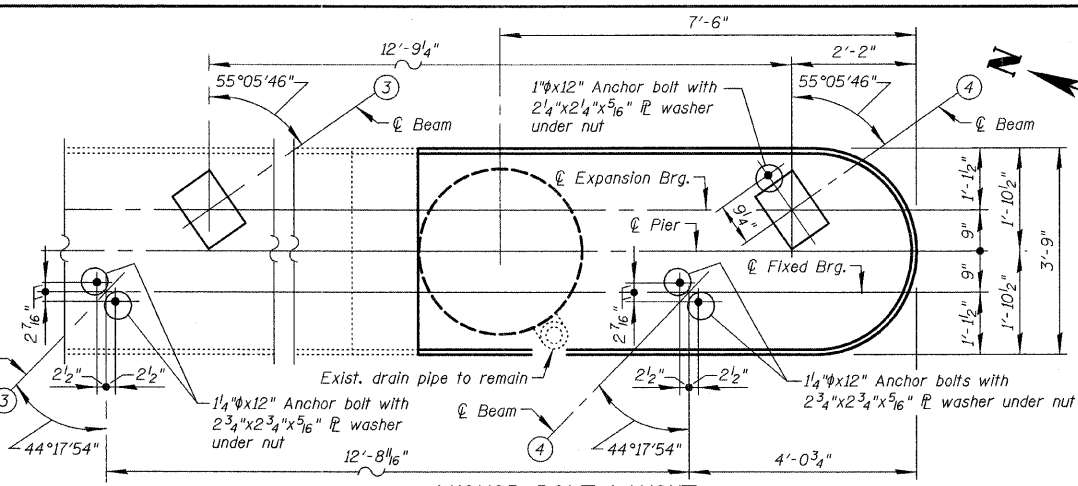
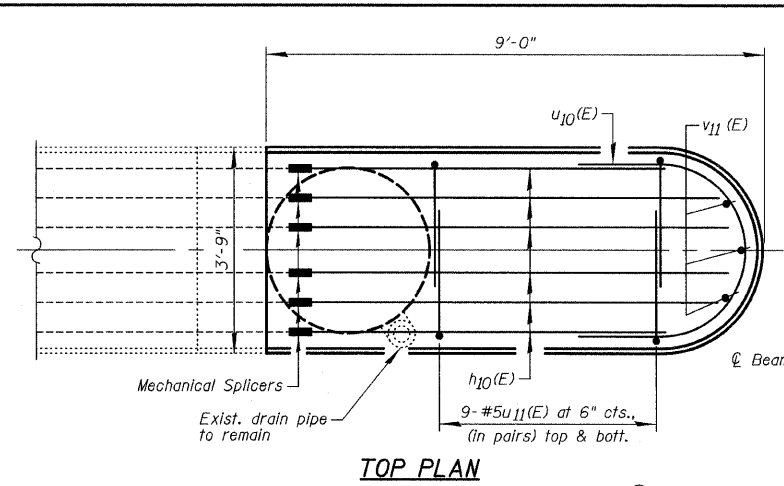
Notes:
Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
It shall be the Contractor's responsibility to verify all dimensions between the bottom of the bridge beams and the top of the bearing seats, in the field, prior to construction or ordering of materials.
The Contractor shall supply additional shim plates, if required, to bring devices to grade. Cost included with Concrete Structures.
Anchor bolts must be installed in holes drilled after supported member is in place. Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications. Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade and diameter specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy-36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
Temporary shoring and cribbing, as described in the Special Provisions, is to be installed prior to any concrete removal.
Existing reinforcement bars projecting from the pier column to remain. The existing reinforcement bars shall be cleaned, straightened and incorporated into the new construction. Cost included with "Concrete Removal".
Existing reinforcement bars projecting from the pier cap shall be cut 6" minimum from the concrete removal line. Cost included with "Concrete Removal".
Any reinforcement bars that are damaged during the concrete removal operation shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".
If the analysis submitted to the Contractor for the jacking/temporary support system to be used shows temporary stiffeners are required to prevent web crippling or buckling, the stiffeners shall be steel and bolted to the web. If stiffeners are not required, hardwood timbers shall be installed tightly between the top and bottom flange to prevent flange rotation.
Reinforcement bars shall conform to the requirements of ASTM A 706 Gr. 60. See Special Provisions. Reinforcement bars designated (E) shall be epoxy coated.

	at Expansion Bearings	at Fixed Bearings
R ₁	(k)	38.7
R ₂	(k)	15.3
R ₃	(k)	47.5
R ₄	(k)	10.2
R _{Total}	(k)	111.7
Minimum Jack Capacity	85 tons	85 tons

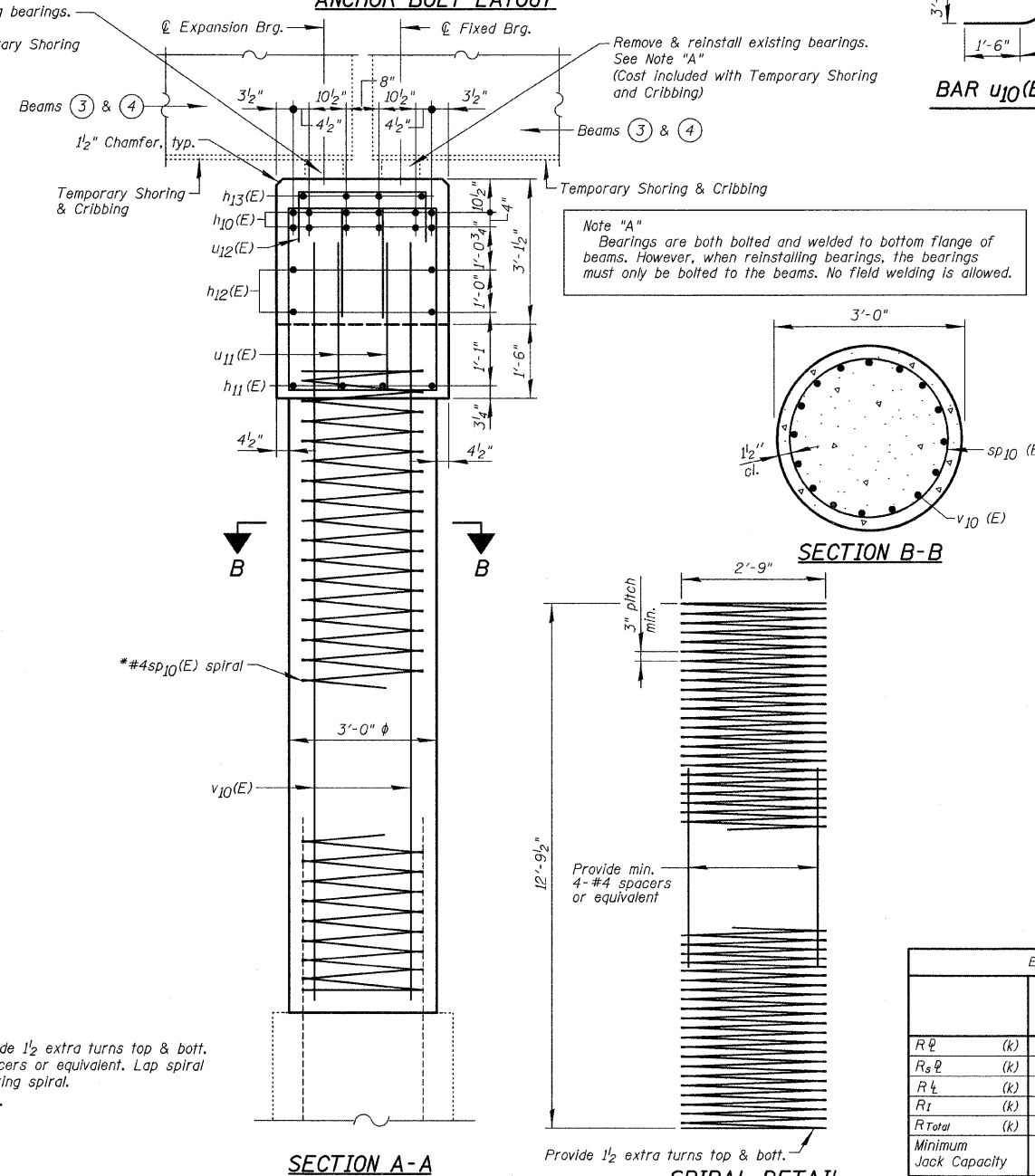
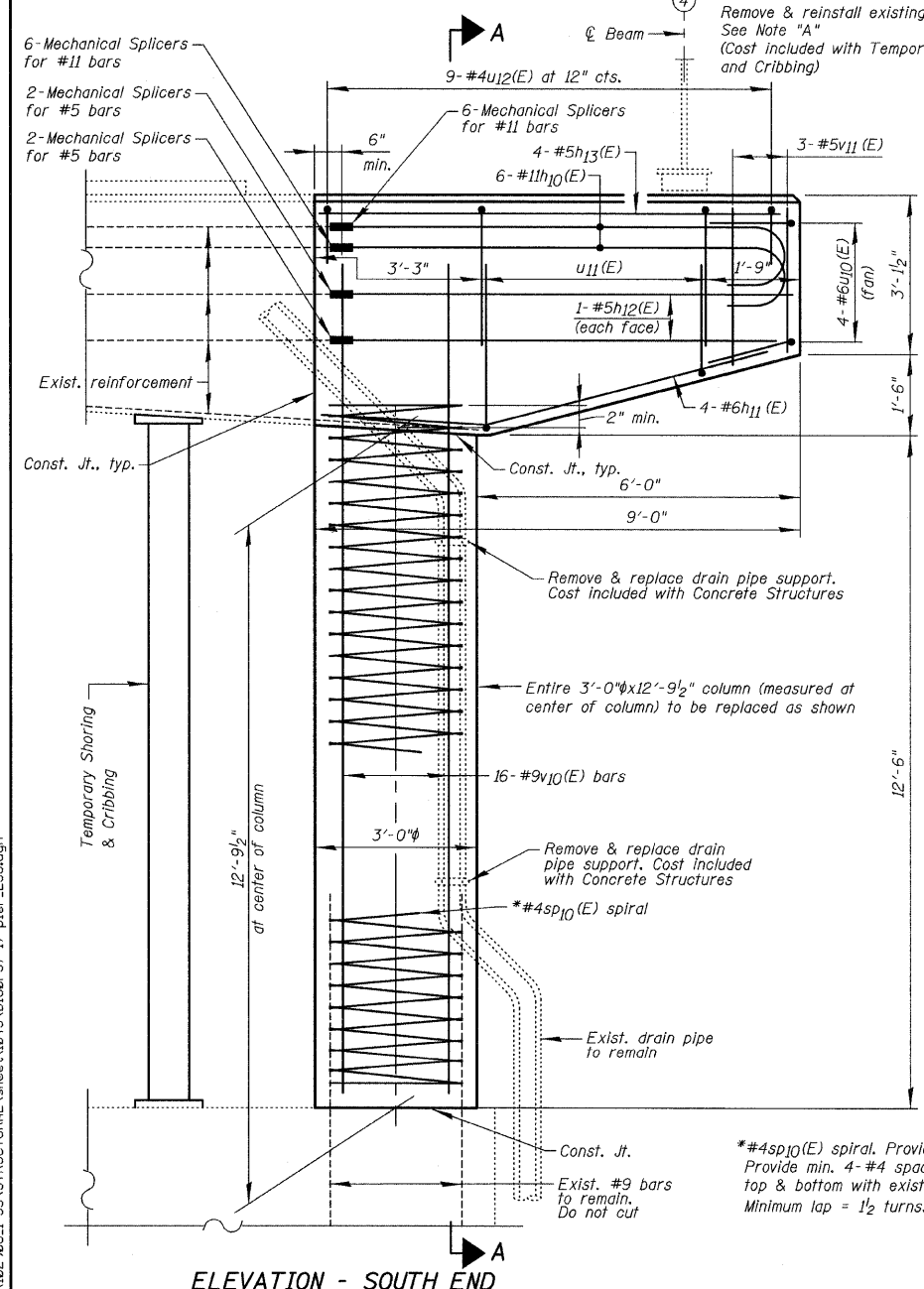
Girder reactions shown are from the existing plan Bearing Data Tables, At Roadway E.N. & Ramp E.S. from S. Prairie Ave. to S. Lake Shore Dr. F.A.I. Rte. 1-55, dated June 18, 1991.

	USER NAME = IDOT	DESIGNED - J.C.N./B.N.S.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PIER E53 REPAIRS STRUCTURE NO. 016-1045	F.A.T. RTE. 55	SECTION 2011-031-BR	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 16
	PLOT SCALE = 50,000 / IN.	CHECKED - B.N.S./J.C.N.	REVISED -			CONTRACT NO. 60P37				
	PLOT DATE = 10/12/2011	DRAWN - F.M.	REVISED -			ILLINOIS FED. AID PROJECT				
		DATE - OCTOBER 12, 2011	REVISED -			SHEET NO. S16 OF S41 SHEETS				

FILE NAME = I:\102906.1-55\STRUCTURAL\sheet\1045\0160P37-17-pier-ES6.dgn



BILL OF MATERIAL				
Bar	No.	Size	Length	Shape
h10(E)	12	#11	10'-3"	U
h11(E)	4	#6	8'-4"	U
h12(E)	4	#5	7'-8"	U
h13(E)	4	#5	8'-8"	U
sp10(E)	1	#4	12'-9 1/2"	W
u10(E)	4	#6	8'-2"	U
u11(E)	36	#5	7'-4"	U
u12(E)	9	#4	4'-6"	U
v10(E)	16	#9	15'-0"	U
v11(E)	3	#5	2'-8"	U
Concrete Removal		Cu. Yd.	8.2	
Concrete Structures		Cu. Yd.	8.2	
Reinforcement Bars, Epoxy Coated		Pound	2,260	
Anchor Bolts, 1"		Each	1	
Anchor Bolts, 1 1/4"		Each	4	
Mechanical Splicers		Each	16	
Temporary Shoring and Cribbing		Each	5	



Note "A"
Bearings are both bolted and welded to bottom flange of beams. However, when reinstalling bearings, the bearings must only be bolted to the beams. No field welding is allowed.

Notes:
Existing reinforcement bars projecting from the pier cap shall be cut 6" minimum from the concrete removal line. Cost included with "Concrete Removal".
Existing reinforcement bars projecting from the pier column to remain.
Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
It shall be the Contractor's responsibility to verify all dimensions between the bottom of the bridge beams and the top of the bearing seats, in the field, prior to construction or ordering of materials.
The Contractor shall supply additional shim plates, if required, to bring devices to grade. Cost included with Concrete Structures.
Anchor bolts must be installed in holes drilled after the supported member is in place. Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications. Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade and diameter specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
Temporary shoring and cribbing, as described in the Special Provisions, is to be installed prior to any concrete removal.
If existing vertical reinforcement bars or spiral extending into removal area to be reused are found to have a cross sectional area lost greater than 10% the Contractor shall notify the Bureau of Bridges and structures for further disposition before installation of the new reinforcement.
If the analysis submitted to the Contractor for the jacking/temporary support system to be used shows temporary stiffeners are required to prevent web crippling or buckling, the stiffeners shall be steel and bolted to the web. If stiffeners are not required, hardwood timbers shall be installed tightly between the top and bottom flange to prevent flange rotation.
Reinforcement bars shall conform to the requirements of AASHTO A 706, Gr. 60, see Special Provisions.
Reinforcement bars designated (E) shall be epoxy coated.

BEAM REACTION TABLE			
	at Expansion Bearings Beam 3	at Expansion Bearings Beam 4	at Fixed Bearings
R _φ	(k) 29.6	23.0	49.9
R _φ l	(k) 12.1	9.7	16.9
R _l	(k) 38.0	36.8	41.8
R _l l	(k) 9.4	9.8	9.1
R _{Total}	(k) 89.1	79.3	117.7
Minimum Jack Capacity	70 tons	60 tons	90 tons

Girder reactions shown are from the existing plan Bearing Data Tables, At Roadway E.N. & Ramp E.S. from S. Prairie Ave. to S. Lake Shore Dr. F.A.I. Rte. I-55, dated June 18, 1991.

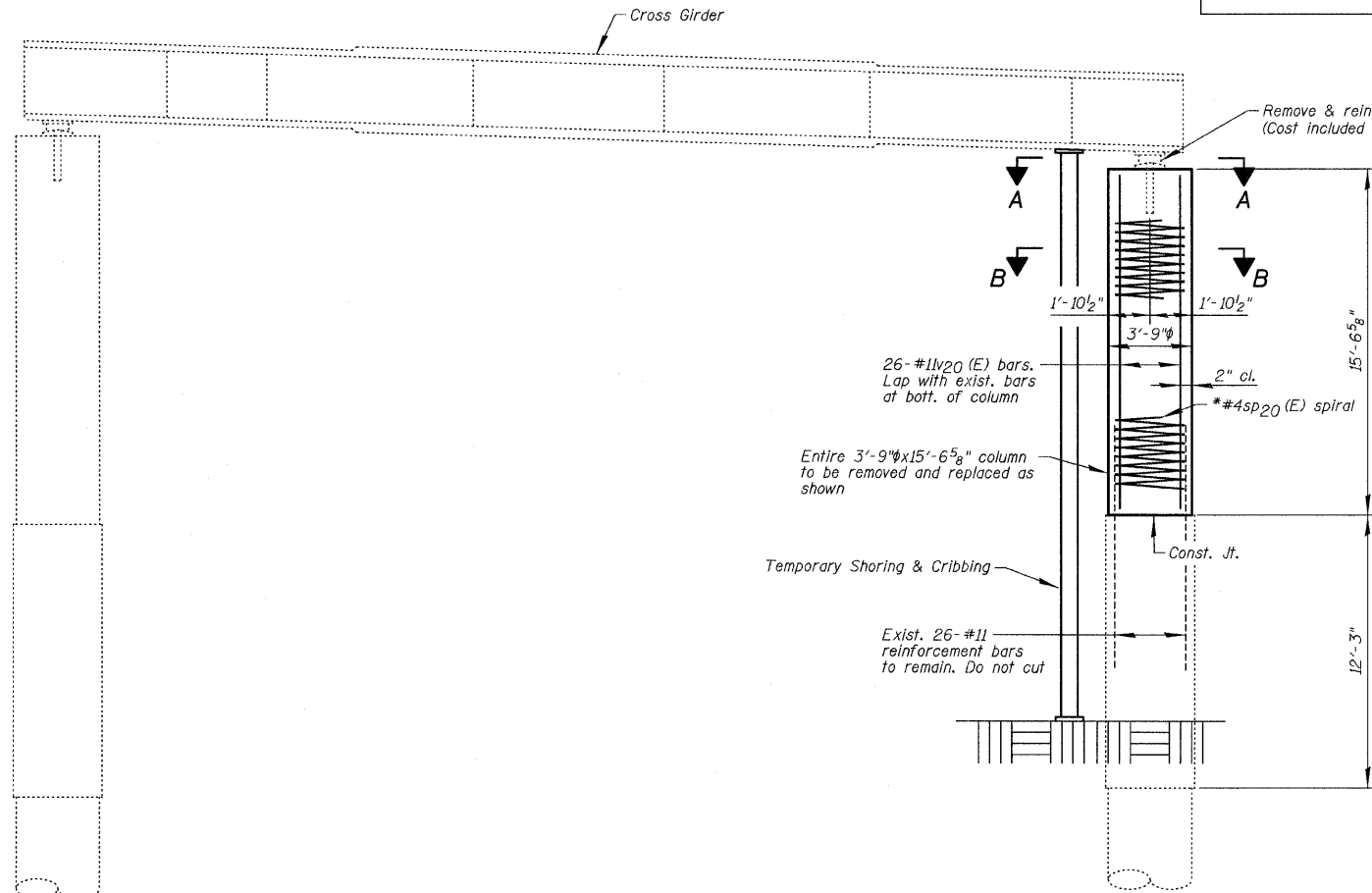
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	PLOT DATE == 10/12/2011	DRAWN - F.M.	REVISED -
		DATE - OCTOBER 12, 2011	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER ES6 REPAIRS
STRUCTURE NO. 016-1045
SHEET NO. S17 OF S41 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2011-031-BR	COOK	41	17
CONTRACT NO. 60P37				
ILLINOIS FED. AID PROJECT				

FILE NAME = I:\102909_1-55\STRUCTURAL\sheet\1045\016p37-18 pr. ES7.dgn



Note "A"
Bearings are both bolted and welded to bottom flange of beams. However, when reinstalling bearings, the bearings must only be bolted to the beams. No field welding is allowed.

Remove & reinstall existing bearings. See Note "A".
(Cost included with Temporary Shoring and Cribbing)

26- #11v20 (E) bars.
Lap with exist. bars
at bott. of column

Entire 3'-9" x 15'-6 5/8" column
to be removed and replaced as
shown

Temporary Shoring & Cribbing

Exist. 26- #11
reinforcement bars
to remain. Do not cut

#4sp20 (E) spiral. Provide 1/2 extra turns top & bott. Provide min. 4- #4 spacers or equivalent. Lap spiral top & bottom with existing spiral. Minimum lap = 1 1/2 turns.

Provide min. 4- #4 spacers
or equivalent

Provide 1/2 extra
turns top & bott.

SPIRAL DETAIL

Note "B"
No spacers in this area to allow for compression of spiral for placement and lapping existing vertical bars as shown. After installation of vertical bars, spiral is to be expanded and lapped with the existing spiral maintaining the proper pitch.

Notes:
Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
It shall be the Contractor's responsibility to verify all dimensions between the bottom of the bridge beams and the top of the bearing seats, in the field, prior to construction or ordering of materials.
The Contractor shall supply additional shim plates, if required, to bring devices to grade. Cost included with Concrete Structures.
Anchor bolts must be installed in holes drilled after the supported member is in place. Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications. Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade and diameter specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy-36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
The Contractor must submit a procedure for installation of the spiral and vertical bars for approval prior to any installation of the reinforcement.
Temporary shoring and cribbing, as described in the Special Provisions, is to be installed prior to any concrete removal.
If existing vertical reinforcement bars or spiral extending into removal area to be reused are found to have a cross sectional area lost greater than 10% the Contractor shall notify the Bureau of Bridges and structures for further disposition before installation of the new reinforcement.
If the analysis submitted to the Contractor for the jacking/temporary support system to be used shows temporary stiffeners are required to prevent web crippling or buckling, the stiffeners shall be steel and bolted to the web. If stiffeners are not required, hardwood timbers shall be installed tightly between the top and bottom flange to prevent flange rotation.
Reinforcement bars shall conform to the requirements of ASTM A706, Gr. 60, see Special Provisions. Reinforcement bars designated (E) shall be epoxy coated.

Note:
Temporary Shoring & Cribbing at this location must be designed to carry all the column loads, which includes portions of reactions from all eight (8) girders supported by the cross girder (shown) plus longitudinal and transverse loads (not shown). The Temporary Shoring & Cribbing will be in place under traffic for an extended period of time.

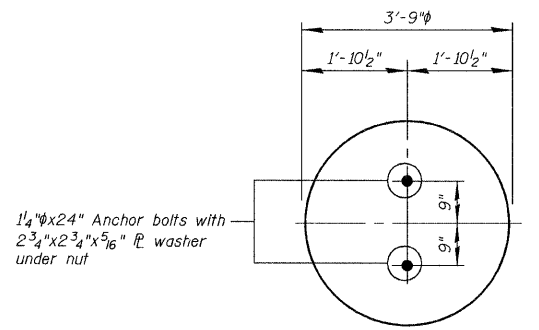
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
** SP20 (E)	1	#4	15'-6"	
v20 (E)	26	#11	15'-2 5/8"	—
Concrete Removal		Cu. Yd.	6.4	
Concrete Structures		Cu. Yd.	6.4	
*** Reinforcement Bars, Epoxy Coated		Pound	2,510	
Anchor Bolts, 1 1/4"		Each	2	
Temporary Shoring and Cribbing		Each	1	

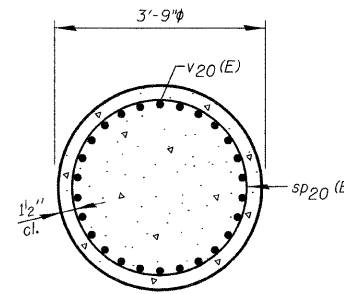
**Length is height of spiral

***Includes weight of spiral & spacers

ELEVATION - PIER E57
(Looking South)



**ANCHOR BOLT LAYOUT
ELEVATION A-A**



SECTION B-B

CROSS GIRDER REACTION TABLE

R _l	(k)	186.6
R _s	(k)	68.1
R _t	(k)	191.3
R _l	(k)	44.3
R _{total}	(k)	490.3
Minimum Jack Capacity		370 tons

CHRISTIAN-ROGE & ASSOCIATES, INC.

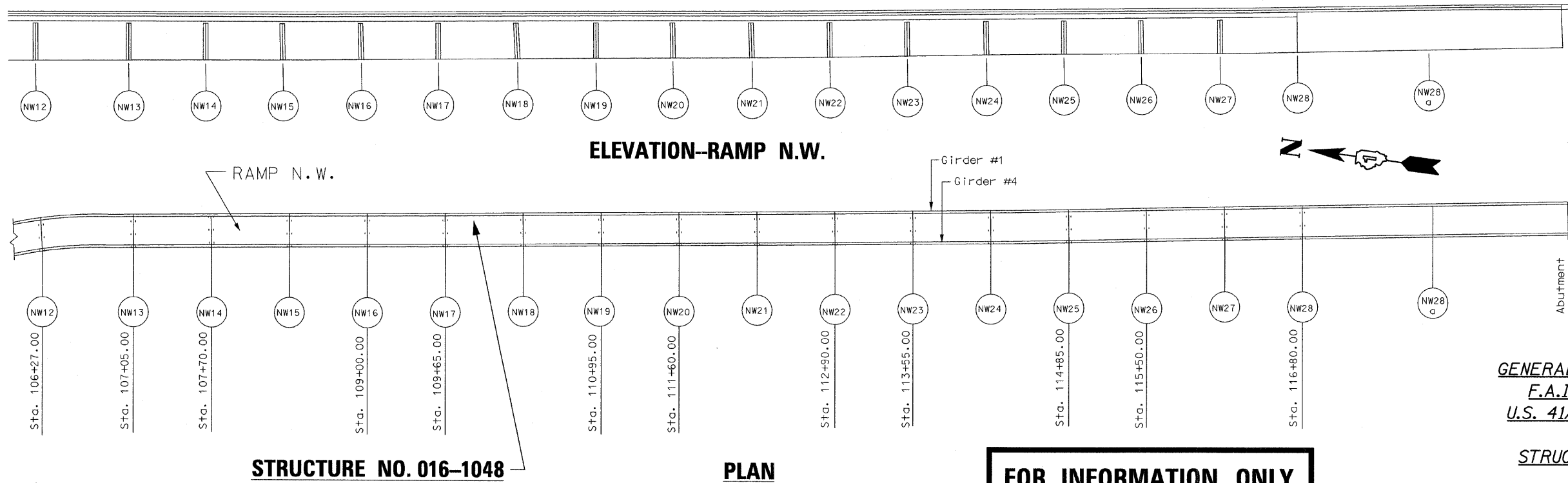
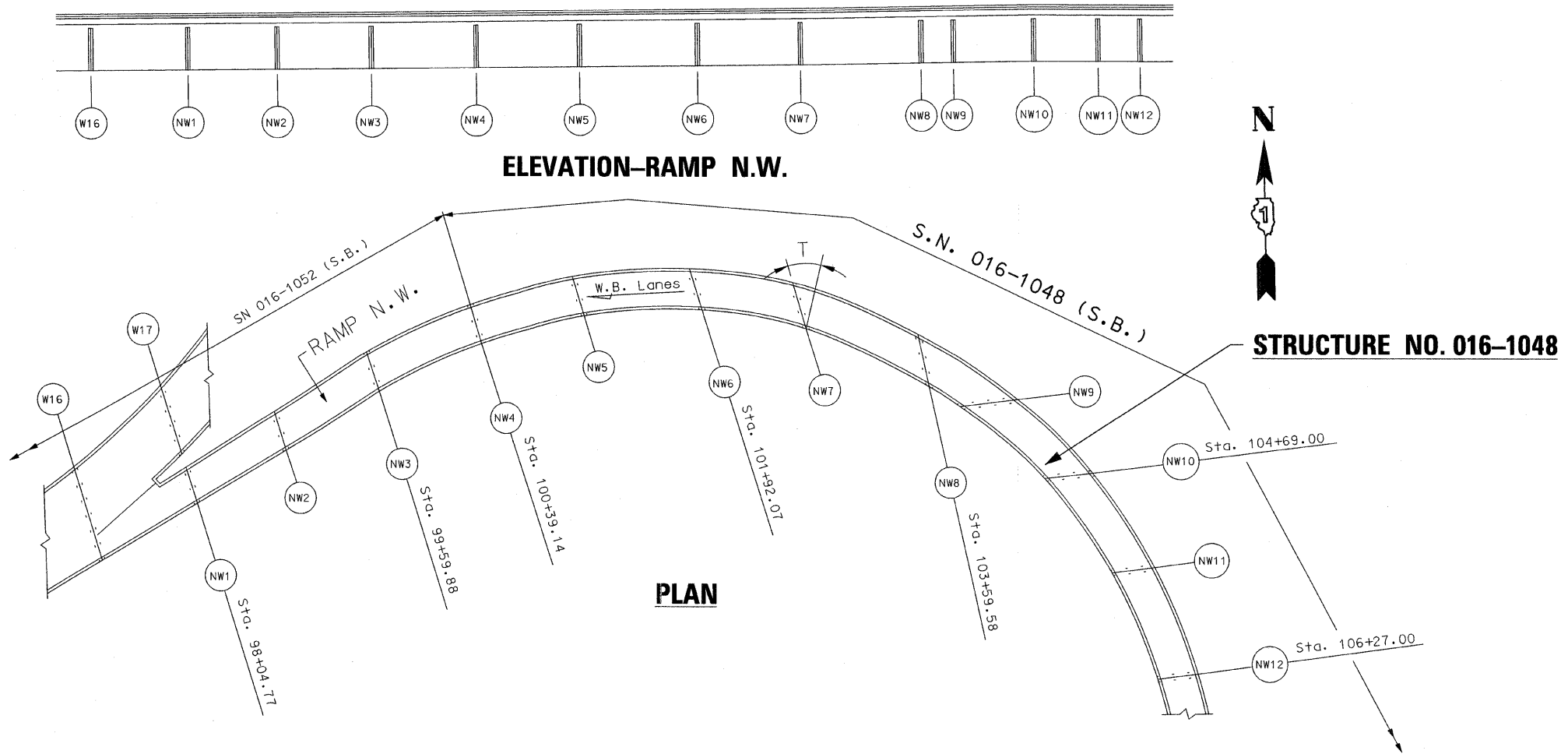
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PLOT DATE == 10/12/2011	DRAWN - F.M.	REVISED -
	DATE - OCTOBER 12, 2011	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER E57 REPAIRS
STRUCTURE NO. 016-1045**
SHEET NO. S18 OF S41 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2011-031-BR	COOK	41	18
CONTRACT NO. 60P37				

ILLINOIS FED. AID PROJECT



FOR INFORMATION ONLY

GENERAL PLAN & ELEVATION
F.A.I. RTE. I-55 OVER
U.S. 41/LAKE SHORE DRIVE
COOK COUNTY
STRUCTURE NO. 016-1048

FILE NAME = I:\102-908-1-55\STRUCTURAL\sheet\1048\0160P37-19-0PE.dgn

CHRISTIAN-ROGE & ASSOCIATES, INC.

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PLOT SCALE = 50.0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -
PLOT DATE == 10/12/2011	DRAWN - F.M.	REVISED -
	DATE - OCTOBER 12, 2011	REVISED -

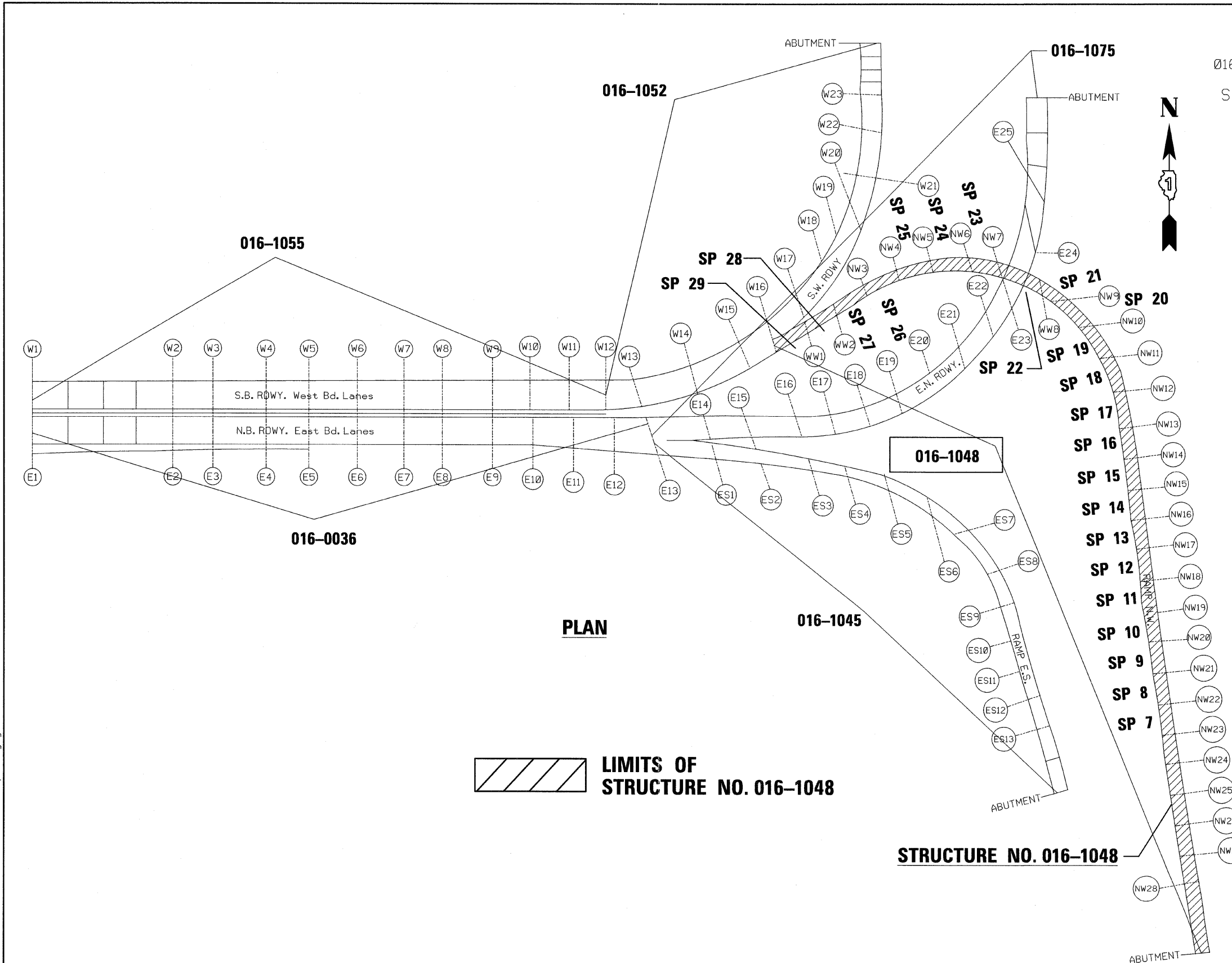
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE. 55	SECTION 2011-031-BR	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 19
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SHEET NO. S19 OF S41 SHEETS

ILLINOIS FED. AID PROJECT

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PLAN

 **LIMITS OF STRUCTURE NO. 016-1048**

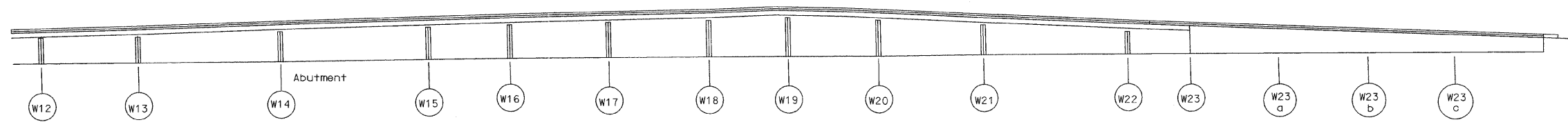
016-1048 US 41 N.B. Ramp to I-55 S.B.
Spans are numbered from the South
Span 7 between Pier NW23 & NW22

Span #	Ramp Lane	
	Partial	Full
7	3 @ 2x2 = 12 SF	0
8	4x2 = 8 SF	0
9	2 @ 6x2 = 24 SF	0
10	2x2 = 4 SF	0
11	2x2 = 4 SF	0
12	4x2 = 8 SF	0
13	1x1 = 1 SF	0
14	1x1 = 1 SF	0
15	2x2 = 4 SF	0
16	1x1 = 1 SF	0
17	0	0
18	0	0
19	0	0
20	2 @ 2x2 = 8 SF	0
21	0	0
22	2 @ 2x2 = 8 SF	0
23	0	0
24	0	0
25	0	0
26	5x4 = 20 SF	0
27	2 @ 2x4 = 16 SF	0
TOTAL	119 SF	

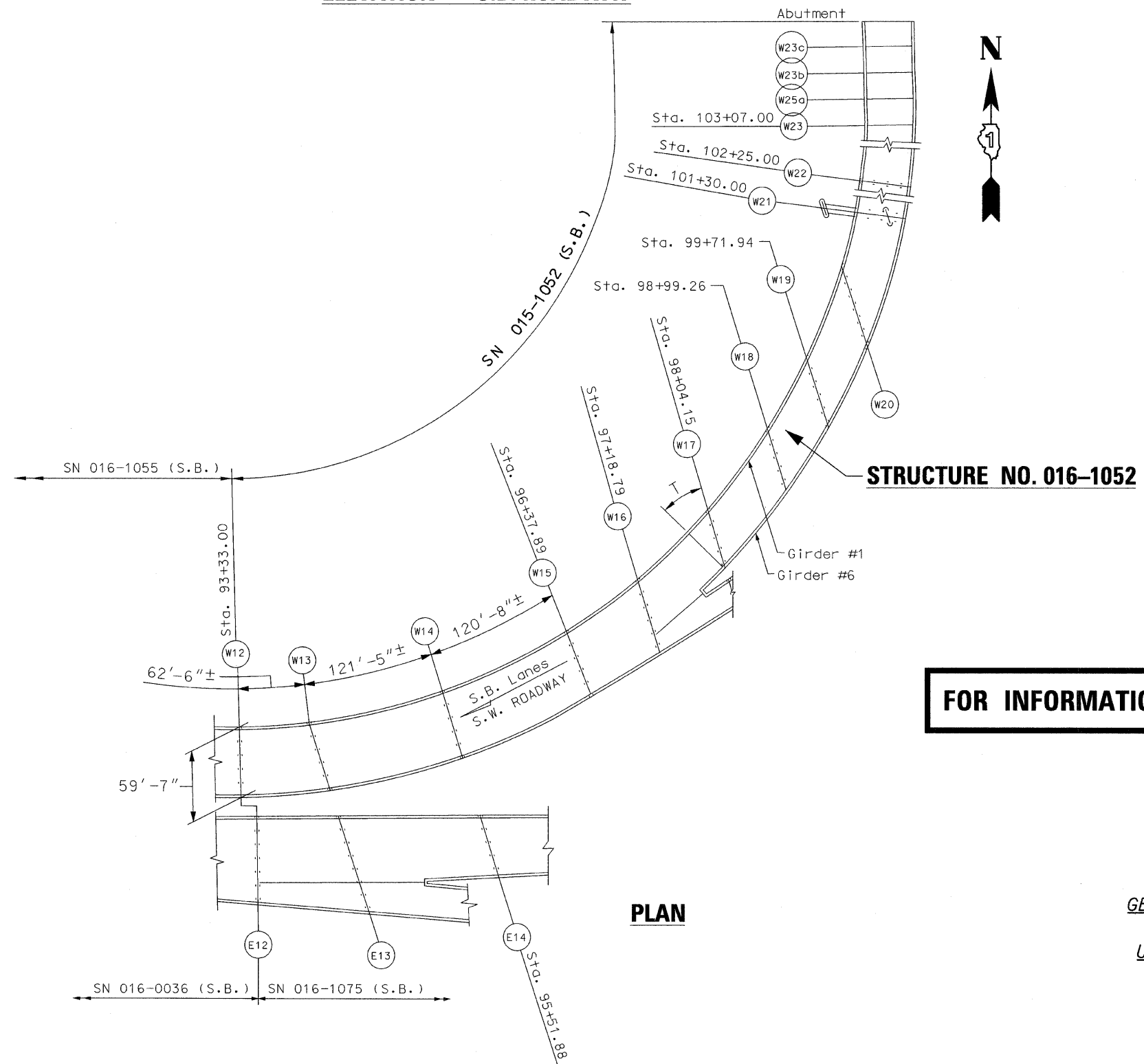
TOTAL : PARTIAL DEPTH
GRAND TOTAL = 119 SF

POTENTIAL FULL DEPTH
GRAND TOTAL = 18 SF

1-55 @ Lake Shore Drive



ELEVATION - S.B. ROADWAY



STRUCTURE NO. 016-1052

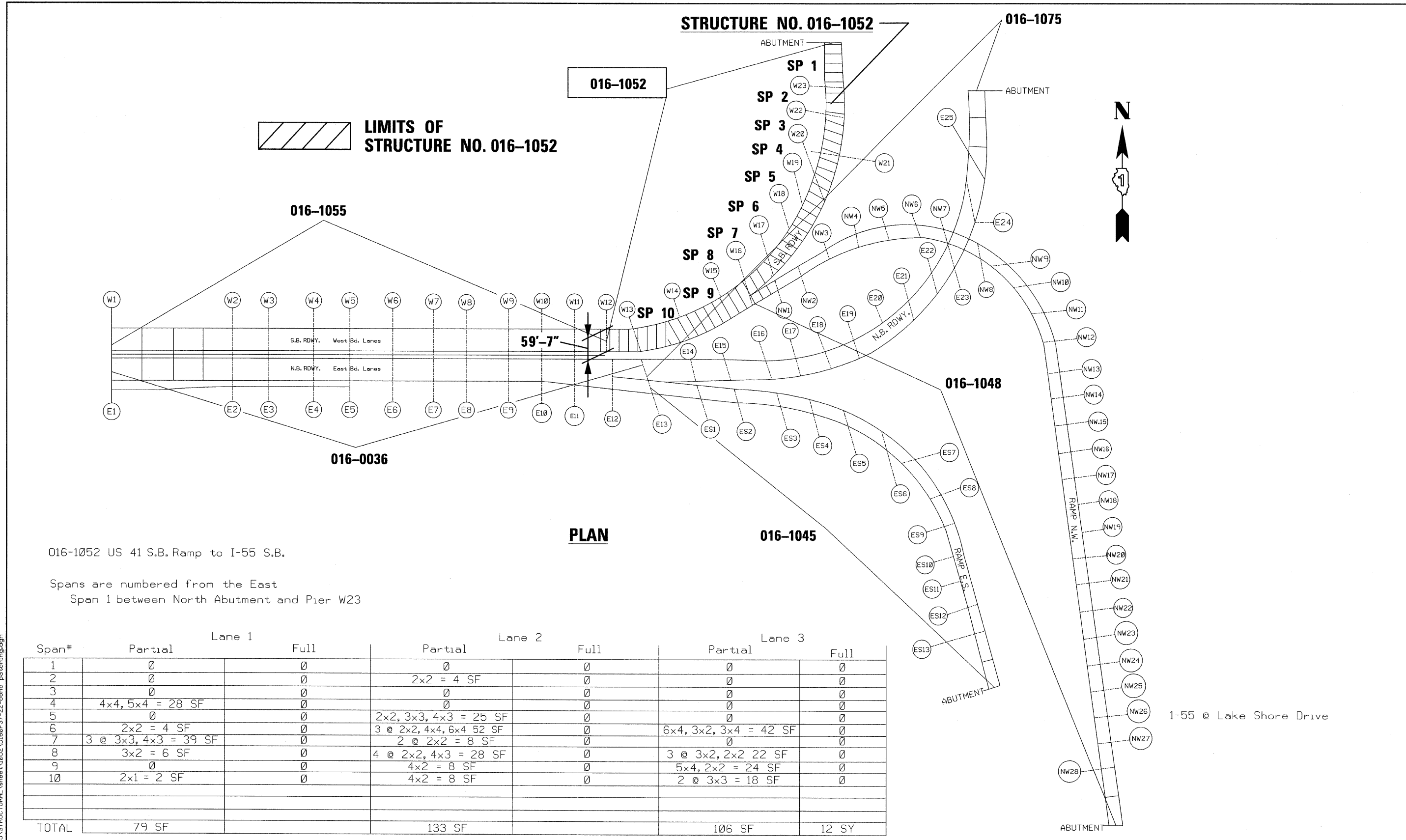
FOR INFORMATION ONLY

PLAN

**GENERAL PLAN & ELEVATION
F.A.I. RTE. I-55 OVER
U.S. 41/LAKE SHORE DRIVE
COOK COUNTY
STRUCTURE NO. 016-1052**

FILE NAME = I:\102908.I-55\STRUCTURAL\sheet\1052.DWG\37-21-CPE.dgn

<p>CHRISTIAN-ROGE & ASSOCIATES, INC.</p>	USER NAME == 1007	DESIGNED - J.C.N./B.N.S.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHEET NO. S21 OF S41 SHEETS	F.A.I. RTE. 55	SECTION 2011-031-BR	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 21	
	PLOT SCALE = 50,0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -			CONTRACT NO. 60P37					
	PLOT DATE == 10/12/2011	DRAWN - F.M.	REVISED -			ILLINOIS FED. AID PROJECT					
	DATE - OCTOBER 12, 2011	REVISED -									



PLAN

016-1052 US 41 S.B. Ramp to I-55 S.B.
 Spans are numbered from the East
 Span 1 between North Abutment and Pier W23

Span#	Lane 1		Lane 2		Lane 3	
	Partial	Full	Partial	Full	Partial	Full
1	0	0	0	0	0	0
2	0	0	2x2 = 4 SF	0	0	0
3	0	0	0	0	0	0
4	4x4, 5x4 = 28 SF	0	0	0	0	0
5	0	0	2x2, 3x3, 4x3 = 25 SF	0	0	0
6	2x2 = 4 SF	0	3 @ 2x2, 4x4, 6x4 52 SF	0	6x4, 3x2, 3x4 = 42 SF	0
7	3 @ 3x3, 4x3 = 39 SF	0	2 @ 2x2 = 8 SF	0	0	0
8	3x2 = 6 SF	0	4 @ 2x2, 4x3 = 28 SF	0	3 @ 3x2, 2x2 22 SF	0
9	0	0	4x2 = 8 SF	0	5x4, 2x2 = 24 SF	0
10	2x1 = 2 SF	0	4x2 = 8 SF	0	2 @ 3x3 = 18 SF	0
TOTAL	79 SF		133 SF		106 SF	12 SY

TOTAL : PARTIAL DEPTH GRAND TOTAL = 318 SF
 POTENTIAL FULL DEPTH GRAND TOTAL = 48 SF

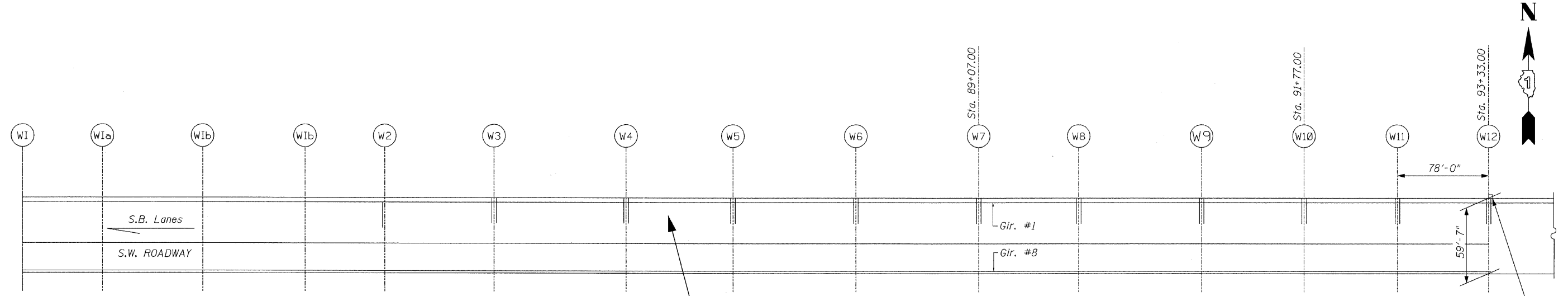
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I-55 @ Lake Shore Drive

SN 016-1045 (N.B.)
SN 016-1052 (S.B.)

SN 016-1055 (S.B.)

ELEVATION - S.B. ROADWAY



STRUCTURE NO. 016-1055

PLAN

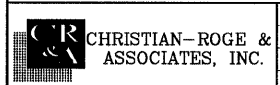
**APPROACH SLAB REPAIR NOTE:
(WEST OF THE BRIDGE)**

- 7 SY(±) - OF APPROACH SLAB REPAIR (FULL DEPTH) &
- 7 SY(±) - OF APPROACH SLAB REPAIR (PARTIAL DEPTH) IS REQUIRED

Sign Truss support
Typical at East Side of
Joints at Piers E10 & E12

**GENERAL PLAN & ELEVATION
F.A.I. RTE. I-55 OVER
U.S. 41/LAKE SHORE DRIVE
COOK COUNTY
STRUCTURE NO. 016-1055**

FILE NAME = JA\102908_1-55\STRUCTURAL\sheet\016-1055\016-1055-23-GPE.dgn

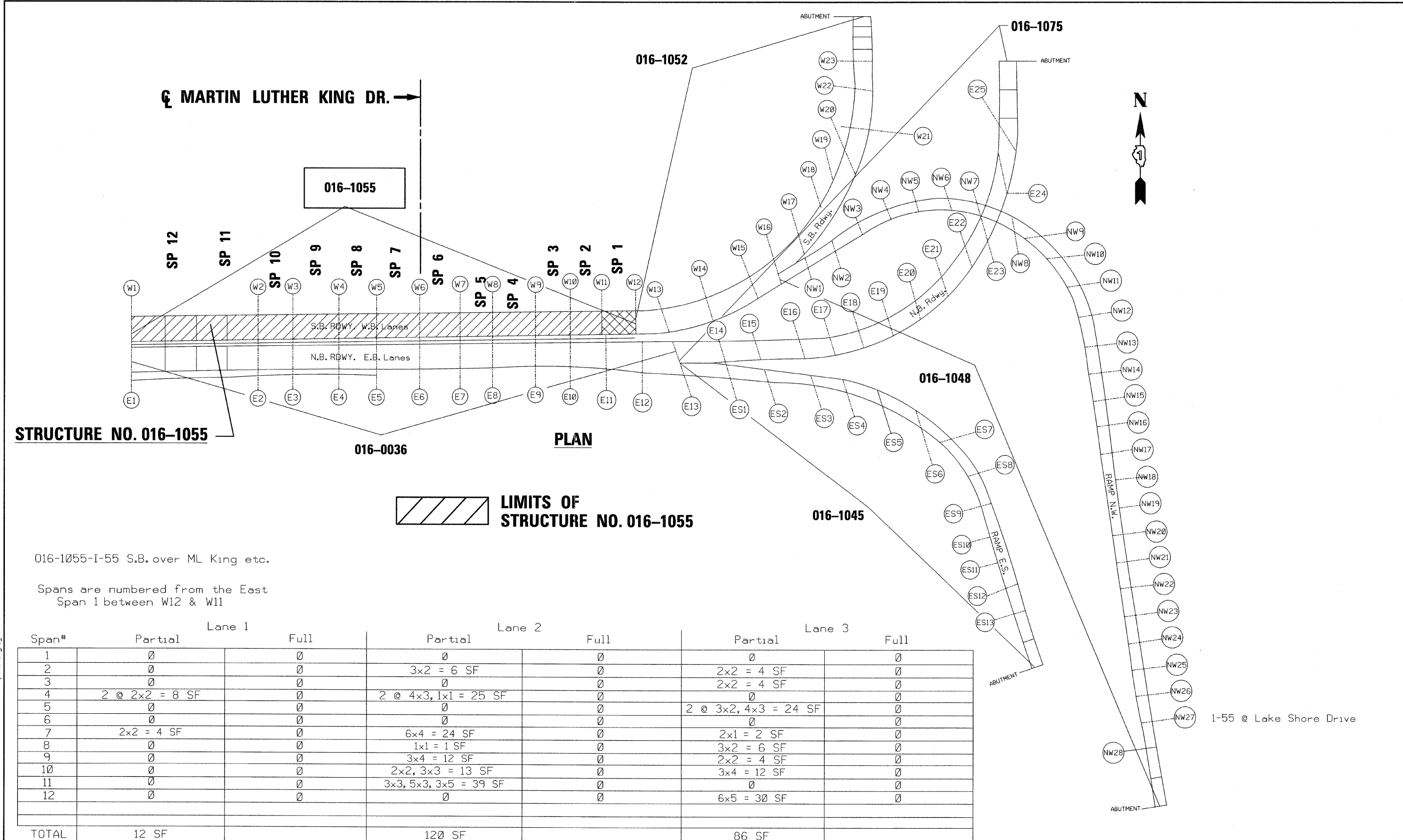


USER NAME = IDOT	DESIGNED - J.C.N./B.N.S.	REVISED -
PLOT SCALE = 50.0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -
PLOT DATE = 10/12/2011	DRAWN - F.M.	REVISED -
	DATE - OCTOBER 12, 2011	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHEET NO. S23 OF S41 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2011-031-BR	COOK	41	23
CONTRACT NO. 60P37				
[ILLINOIS] FED. AID PROJECT				



STRUCTURE NO. 016-1055

PLAN

LIMITS OF STRUCTURE NO. 016-1055

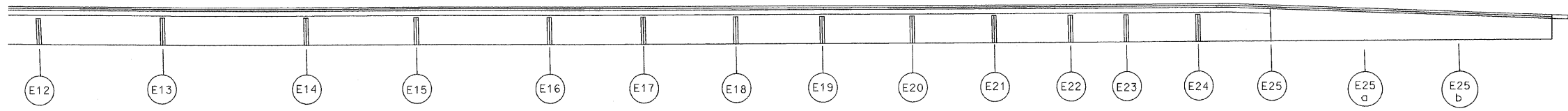
016-1055-I-55 S.B. over ML King etc.
 Spans are numbered from the East
 Span 1 between W12 & W11

Span#	Lane 1		Lane 2		Lane 3	
	Partial	Full	Partial	Full	Partial	Full
1	0	0	0	0	0	0
2	0	0	3x2 = 6 SF	0	2x2 = 4 SF	0
3	0	0	0	0	2x2 = 4 SF	0
4	2 @ 2x2 = 8 SF	0	2 @ 4x3, 1x1 = 25 SF	0	0	0
5	0	0	0	0	2 @ 3x2, 4x3 = 24 SF	0
6	0	0	0	0	0	0
7	2x2 = 4 SF	0	6x4 = 24 SF	0	2x1 = 2 SF	0
8	0	0	1x1 = 1 SF	0	3x2 = 6 SF	0
9	0	0	3x4 = 12 SF	0	2x2 = 4 SF	0
10	0	0	2x2, 3x3 = 13 SF	0	3x4 = 12 SF	0
11	0	0	3x3, 5x3, 3x5 = 39 SF	0	0	0
12	0	0	0	0	6x5 = 30 SF	0
TOTAL	12 SF		120 SF		86 SF	

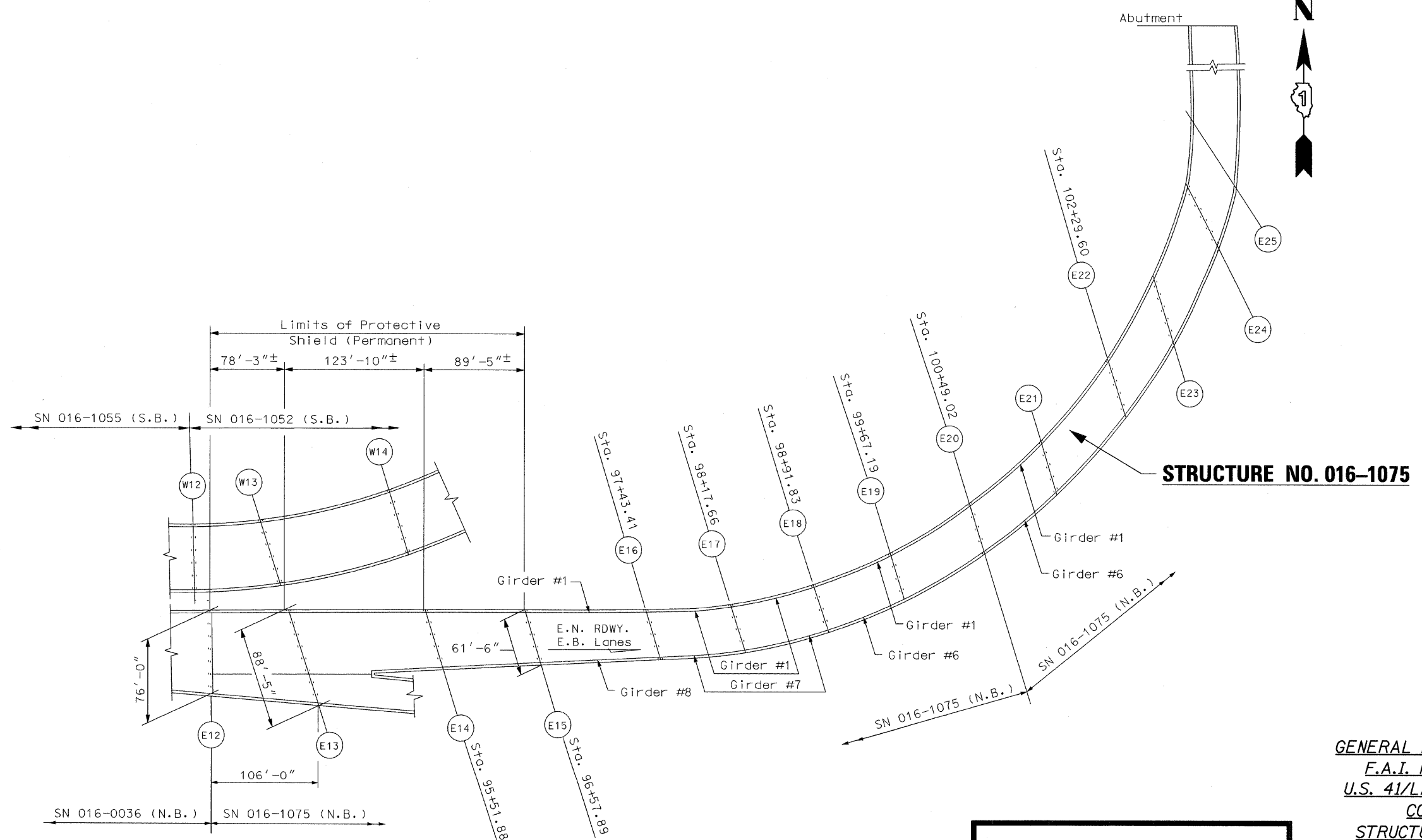
TOTAL : PARTIAL DEPTH GRAND TOTAL = 218 SF
 POTENTIAL FULL DEPTH GRAND TOTAL = 33 SF

FILE NAME = I:\102988_1-55\STRUCTURAL\sheet\016-1055-I-55-001.dwg

CHRISTIAN-ROGE & ASSOCIATES, INC.	USER NAME = IDOT	DESIGNED - J.C.N./B.N.S.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DECK CONCRETE PATCHING STRUCTURE NO. 016-1055	F.A.I. RTE. = 55	SECTION = 2011-031-BR	COUNTY = COOK	TOTAL SHEETS = 41	SHEET NO. = 24
	PLOT SCALE = 50.0000' / IN.	DRAWN - F.M.	REVISOR -			DATE = OCTOBER 12, 2011	SHEET NO. S24 OF S41 SHEETS			CONTRACT NO. 60P37



ELEVATION - N.B. ROADWAY



STRUCTURE NO. 016-1075

PLAN

FOR INFORMATION ONLY

**GENERAL PLAN & ELEVATION
F.A.I. RTE. I-55 OVER
U.S. 41/LAKE SHORE DRIVE
COOK COUNTY
STRUCTURE NO. 016-1075**

FILE NAME = I:\102998-I-55\STRUCTURAL\sheet\1075.D166P37-25-GPE.dgn

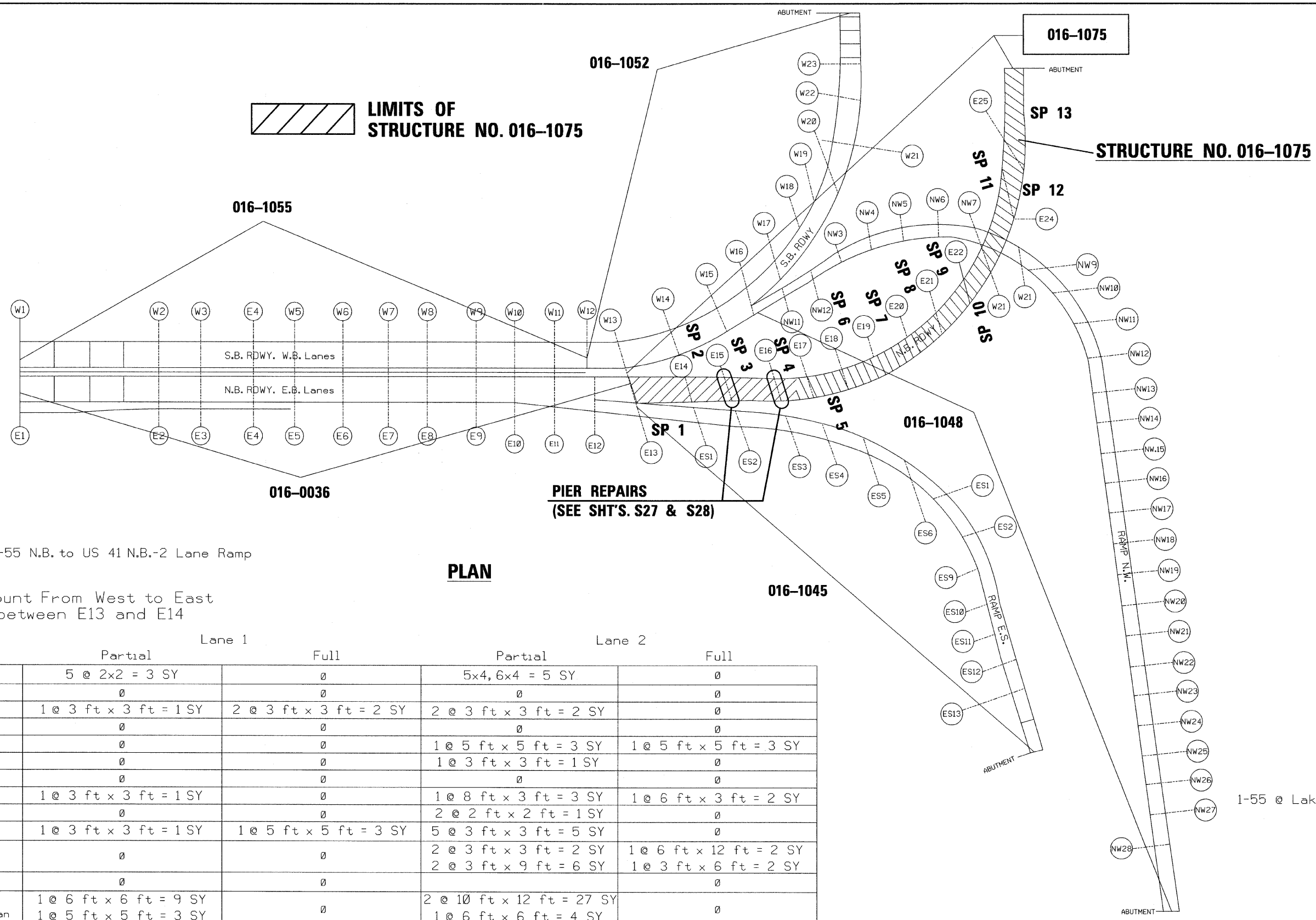
CHRISTIAN-ROGE & ASSOCIATES, INC.

USER NAME == IDOT	DESIGNED - J.C.N./B.N.S.	REVISED -
PLOT SCALE = 50.0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -
PLOT DATE == 10/12/2011	DRAWN - F.M.	REVISED -
	DATE - OCTOBER 12, 2011	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHEET NO. S25 OF S41 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2011-031-BR	COOK	41	25
CONTRACT NO. 60P37				
<small>[ILLINOIS] FED. AID PROJECT</small>				



016-1075-I-55 N.B. to US 41 N.B.-2 Lane Ramp

PLAN

Span Count From West to East
Span 1 between E13 and E14

Span #	Lane 1		Lane 2	
	Partial	Full	Partial	Full
1	5 @ 2x2 = 3 SY	0	5x4, 6x4 = 5 SY	0
2	0	0	0	0
3	1 @ 3 ft x 3 ft = 1 SY	2 @ 3 ft x 3 ft = 2 SY	2 @ 3 ft x 3 ft = 2 SY	0
4	0	0	0	0
5	0	0	1 @ 5 ft x 5 ft = 3 SY	1 @ 5 ft x 5 ft = 3 SY
6	0	0	1 @ 3 ft x 3 ft = 1 SY	0
7	0	0	0	0
8	1 @ 3 ft x 3 ft = 1 SY	0	1 @ 8 ft x 3 ft = 3 SY	1 @ 6 ft x 3 ft = 2 SY
9	0	0	2 @ 2 ft x 2 ft = 1 SY	0
10	1 @ 3 ft x 3 ft = 1 SY	1 @ 5 ft x 5 ft = 3 SY	5 @ 3 ft x 3 ft = 5 SY	0
11	0	0	2 @ 3 ft x 3 ft = 2 SY	1 @ 6 ft x 12 ft = 2 SY
12	0	0	2 @ 3 ft x 9 ft = 6 SY	1 @ 3 ft x 6 ft = 2 SY
13 Vaulted Span	1 @ 6 ft x 6 ft = 9 SY 1 @ 5 ft x 5 ft = 3 SY	0	2 @ 10 ft x 12 ft = 27 SY 1 @ 6 ft x 6 ft = 4 SY	0
TOTAL	18 SY	5 SY	59 SY	9 SY

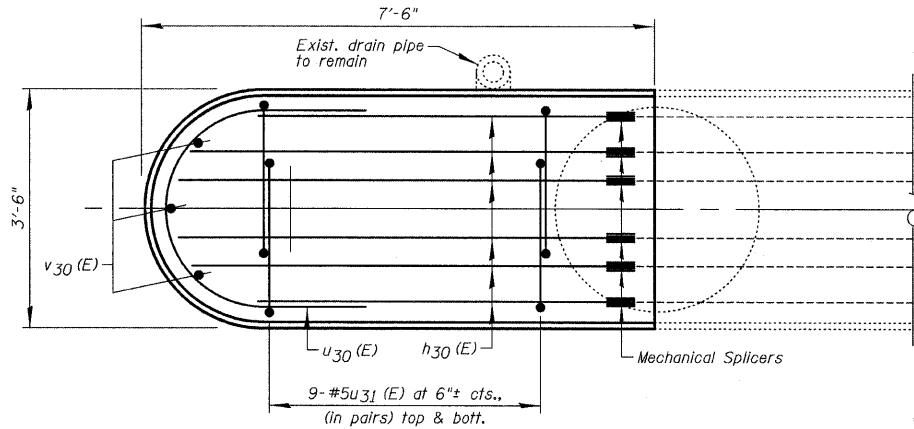
TOTAL : PARTIAL DEPTH = 77 SY = 693 SF
FULL DEPTH = 14 SY = 126 SF

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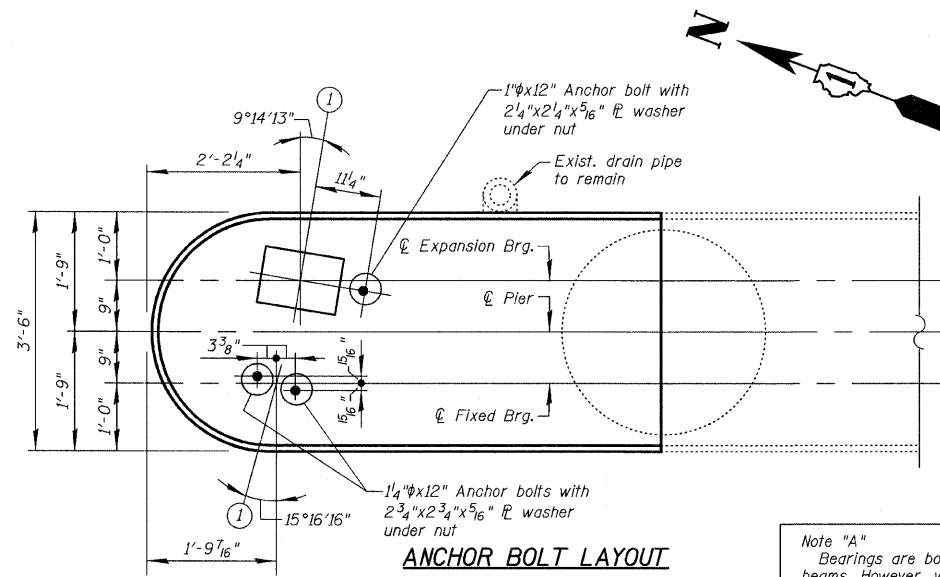
CHRISTIAN-ROGE & ASSOCIATES, INC.	USER NAME == IDOT	DESIGNED - J.C.N./B.N.S.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DECK CONCRETE PATCHING STRUCTURE NO. 016-1075	F.A.I. RTE. 55	SECTION 2011-031-BR	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 26
	PLOT SCALE = 50,0000 "/> IN.	CHECKED - B.N.S./J.C.N.	REVISED -			SHEET NO. S26 OF S41 SHEETS				
	PLOT DATE == 10/12/2011	DRAWN - F.M.	REVISED -			CONTRACT NO. 60P37				
	DATE - OCTOBER 12, 2011	REVISED -		ILLINOIS FED. AID PROJECT						

BILL OF MATERIAL

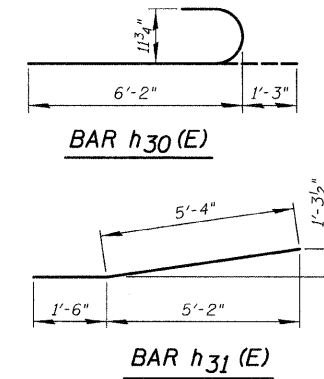
Bar	No.	Size	Length	Shape
h30 (E)	12	#9	7'-5"	U
h31 (E)	4	#6	6'-10"	U
h32 (E)	4	#5	6'-2"	U
h33 (E)	4	#5	7'-2"	U
u30 (E)	4	#6	7'-11"	C
u31 (E)	36	#5	7'-2"	U
u32 (E)	8	#4	4'-6"	U
v30 (E)	3	#5	2'-4"	U
Concrete Removal		Cu. Yd.	3.2	
Concrete Structures		Cu. Yd.	3.2	
Reinforcement Bars, Epoxy Coated		Pound	750	
Anchor Bolts, 1"		Each	1	
Anchor Bolts, 1/4"		Each	2	
Mechanical Splicers		Each	16	
Temporary Shoring and Cribbing		Each	2	



TOP PLAN



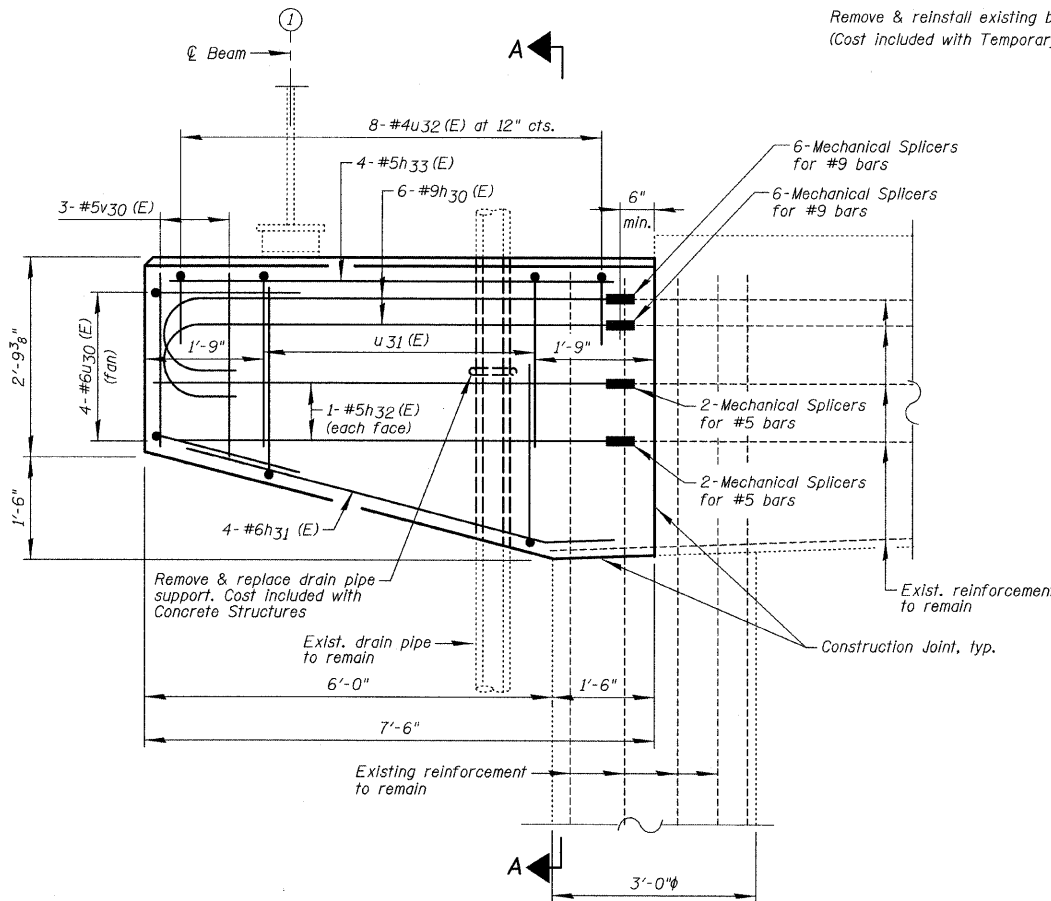
ANCHOR BOLT LAYOUT



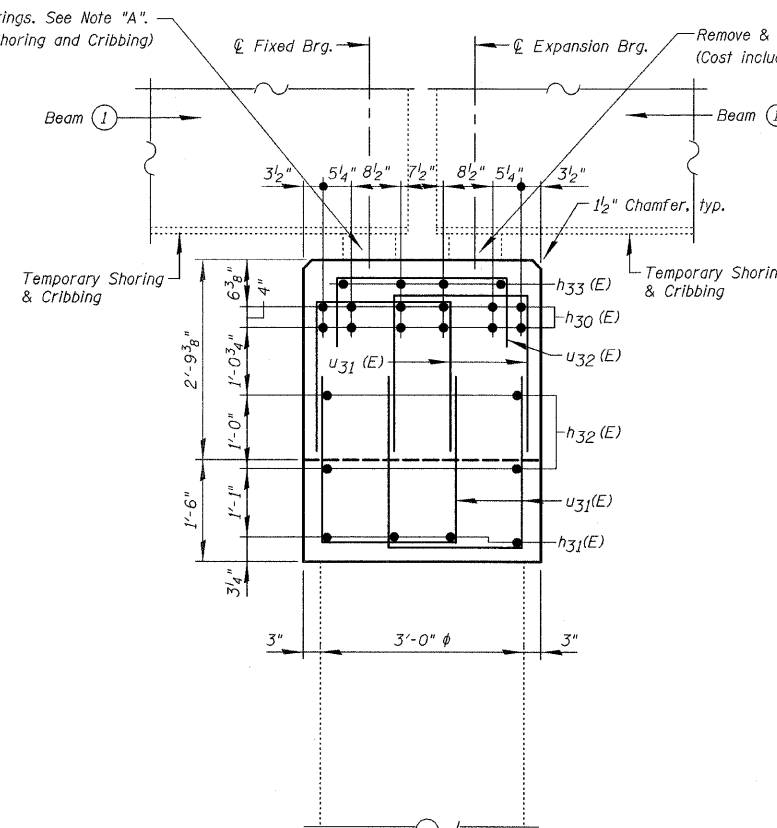
BAR h30 (E)

BAR h31 (E)

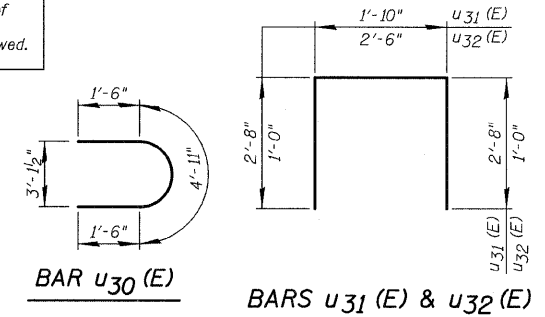
Note "A"
Bearings are both bolted and welded to bottom flange of beams. However, when reinstalling bearings, the bearings must only be bolted to the beams. No field welding is allowed.



ELEVATION - NORTH END
(Looking East)



SECTION A-A



BAR u30 (E)

BARS u31 (E) & u32 (E)

Notes:

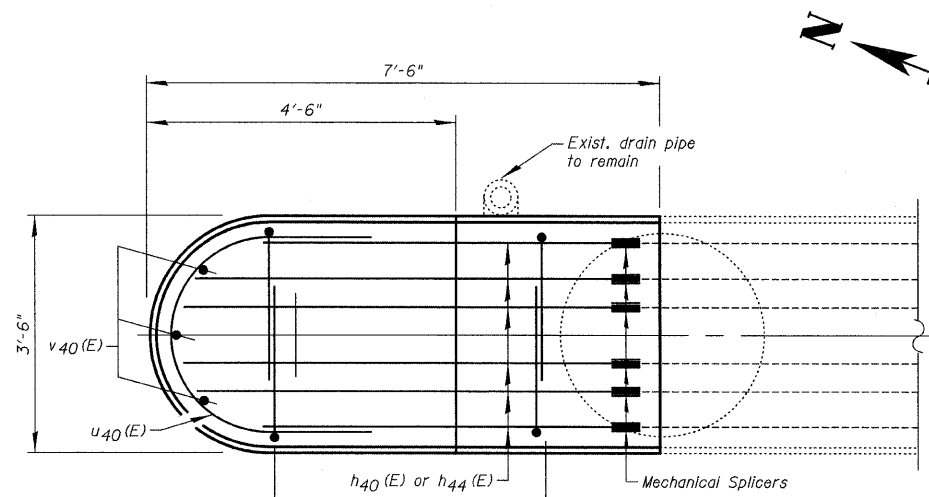
- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- It shall be the Contractor's responsibility to verify all dimensions between the bottom of the bridge beams and the top of the bearing seats, in the field, prior to construction or ordering of materials.
- The Contractor shall supply additional shim plates, if required, to bring devices to grade. Cost included with Concrete Structures.
- Anchor bolts must be installed in holes drilled after supported member is in place. Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications. Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade and diameter specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
- Temporary shoring and cribbing, as described in the Special Provisions, is to be installed prior to any concrete removal.
- Existing reinforcement bars projecting from the pier column to remain. The existing reinforcement bars shall be cleaned, straightened and incorporated into the new construction. Cost included with "Concrete Removal".
- Existing reinforcement bars projecting from the pier cap shall be cut 6" minimum from the concrete removal line. Cost included with "Concrete Removal".
- Any reinforcement bars that are damaged during the concrete removal operation shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".
- If the analysis submitted to the Contractor for the jacking/temporary support system to be used shows temporary stiffeners are required to prevent web crippling or buckling, the stiffeners shall be steel and bolted to the web. If stiffeners are not required, hardwood timbers shall be installed tightly between the top and bottom flange to prevent flange rotation.
- Reinforcement bars shall conform to the requirements of ASTM A 706, Gr. 60. See Special Provisions. Reinforcement bars designated (E) shall be epoxy coated.

	at Expansion Bearings	at Fixed Bearings
R _l	(k)	48.1
R _s	(k)	17.9
R _r	(k)	48.0
R _t	(k)	10.5
R _{total}	(k)	124.5
Minimum Jack Capacity	95 tons	95 tons

Girder reactions shown are from the existing plan Bearing Data Tables, At Roadway E.N. & Ramp E.S. from S. Prairie Ave. to S. Lake Shore Dr. F.A.I. Rte. I-55, dated June 18, 1991.

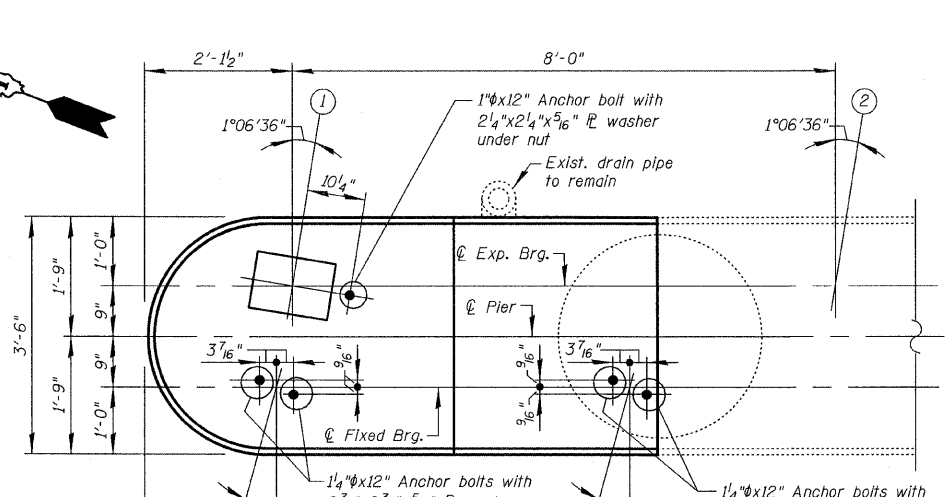
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	PLOT SCALE = 50.0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -			SHEET NO. S27 OF S41 SHEETS				
	PLOT DATE == 10/12/2011	DRAWN - F.M.	REVISED -							
	DATE - OCTOBER 12, 2011	REVISED -	ILLINOIS FED. AID PROJECT							

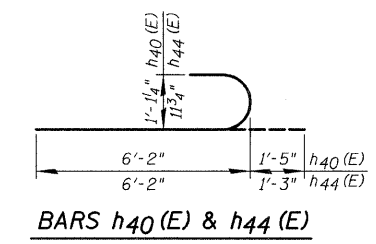


TOP PLAN

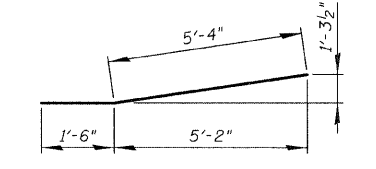
Note "A"
Bearings are both bolted and welded to bottom flange of beams. However, when reinstalling bearings, the bearings must only be bolted to the beams. No field welding is allowed.



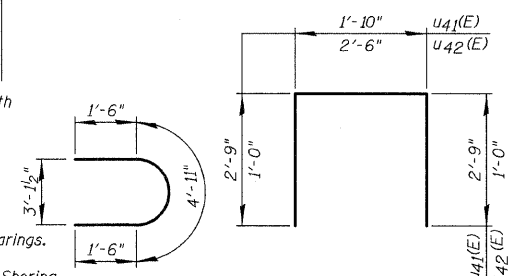
ANCHOR BOLT LAYOUT



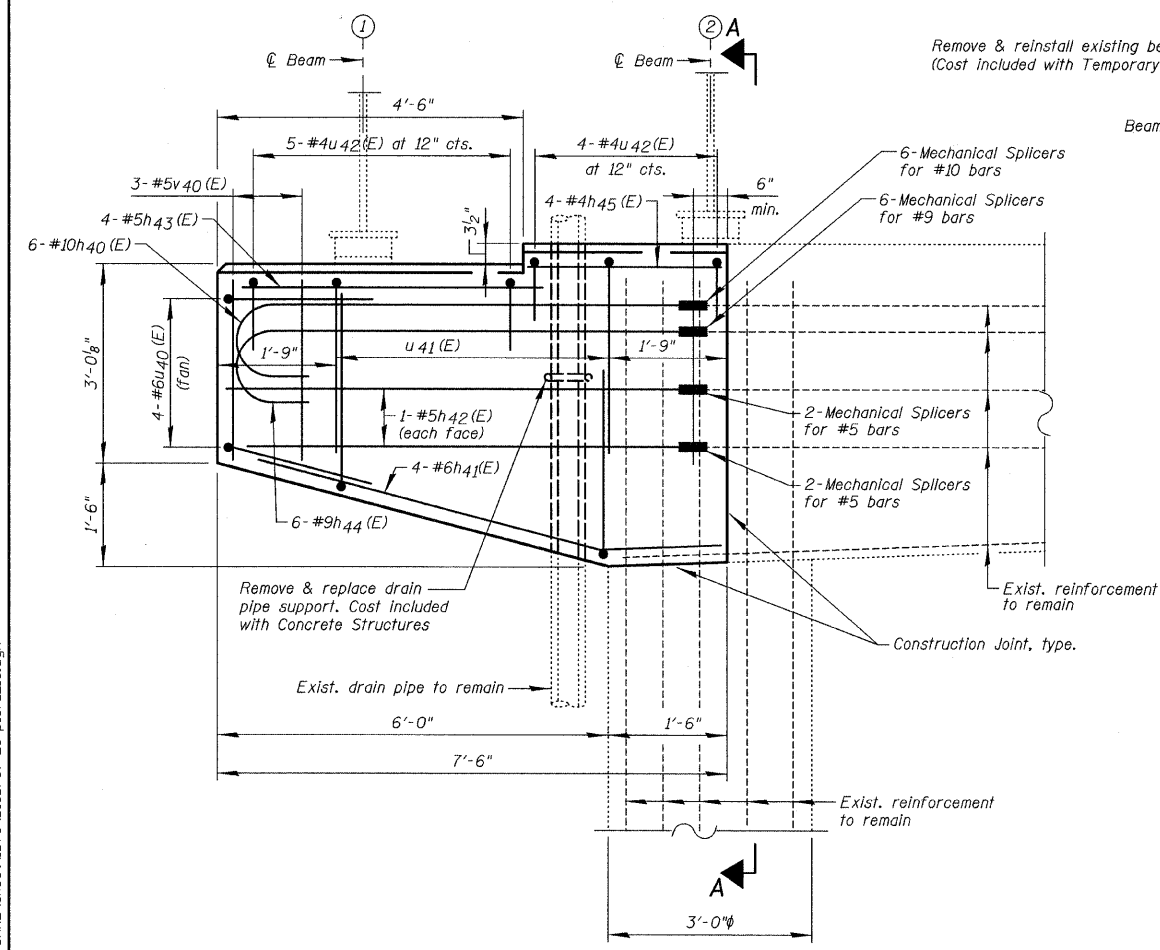
BARS h40(E) & h44(E)



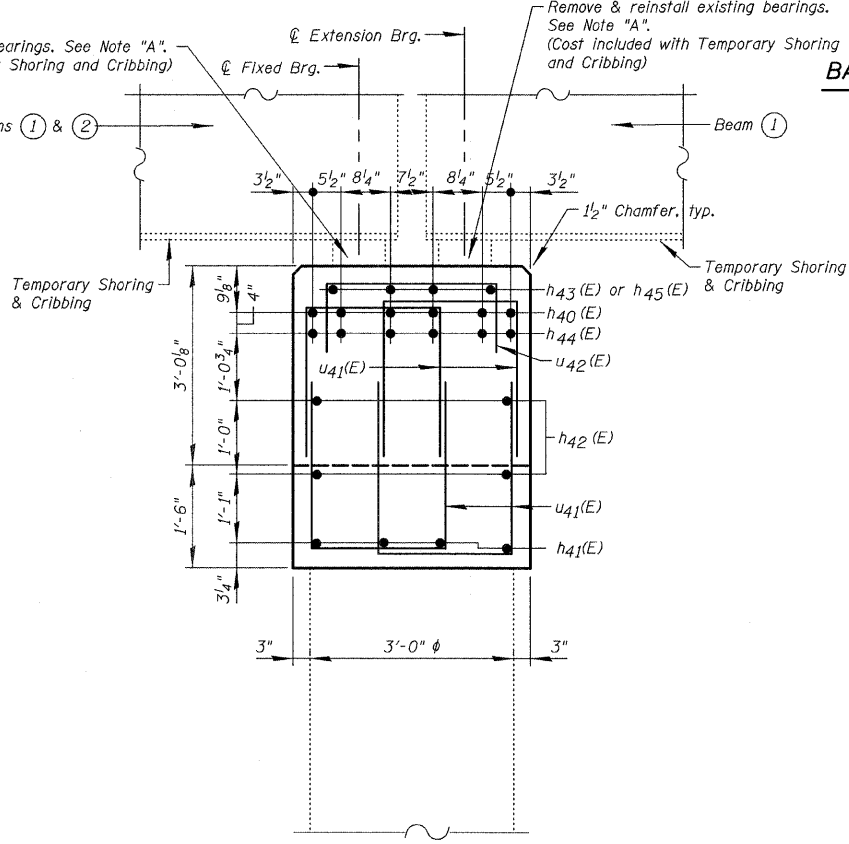
BAR h41(E)



BARS u41(E) & u42(E)



ELEVATION - NORTH END
(Looking East)



SECTION A-A

Notes:
Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
It shall be the Contractor's responsibility to verify all dimensions between the bottom of the bridge beams and the top of the bearing seats, in the field, prior to construction or ordering of materials.
The Contractor shall supply additional shim plates, if required, to bring devices to grade. Cost included with Concrete Structures.
Anchor bolts must be installed in holes drilled after supported member is in place. Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications. Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade and diameter specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
Temporary shoring and cribbing, as described in the Special Provisions, is to be installed prior to any concrete removal.
Existing reinforcement bars projecting from the pier column to remain. The existing reinforcement bars shall be cleaned, straightened and incorporated into the new construction. Cost included with "Concrete Removal".
Existing reinforcement bars projecting from the pier cap shall be cut 6" minimum from the concrete removal line. Cost included with "Concrete Removal".
Any reinforcement bars that are damaged during the concrete removal operation shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".
If the analysis submitted to the Contractor for the jacking/temporary support system to be used shows temporary stiffeners are required to prevent web crippling or buckling, the stiffeners shall be steel and bolted to the web. If stiffeners are not required, hardwood timbers shall be installed tightly between the top and bottom flange to prevent flange rotation.
Reinforcement bars shall conform to the requirements of ASTM A 706, Gr. 60. See Special Provisions. Reinforcement bars designated (E) shall be epoxy coated.

BEAM REACTION TABLE			
	at Expansion Bearings	at Fixed Bearings	
R _l	(k)	31.1	48.1
R _s	(k)	12.5	17.9
R _t	(k)	45.6	48.0
R _r	(k)	11.6	10.5
R _{total}	(k)	100.8	124.5
Minimum Jack Capacity		80 tons	95 tons

Girder reactions shown are from the existing plan Bearing Data Tables, At Roadway E.N. & Ramp E.S. from S. Prairie Ave. to S. Lake Shore Dr. F.A.I. Rte. I-55, dated June 18, 1991.

BILL OF MATERIAL				
Bar	No.	Size	Length	Shape
h40(E)	6	#10	7'-7"	U
h41(E)	4	#6	6'-10"	U
h42(E)	4	#5	6'-2"	U
h43(E)	4	#5	5'-0"	U
h44(E)	6	#9	7'-5"	U
h45(E)	4	#5	2'-9"	U
u40(E)	4	#6	7'-11"	U
u41(E)	20	#5	7'-4"	U
u42(E)	9	#4	4'-6"	U
v40(E)	3	#5	2'-9"	U
Concrete Removal		Cu. Yd.	3.2	
Concrete Structures		Cu. Yd.	3.2	
Reinforcement Bars, Epoxy Coated		Pound	680	
Anchor Bolts, 1"		Each	1	
Anchor Bolts, 1 1/4"		Each	4	
Mechanical Splicers		Each	16	
Temporary Shoring and Cribbing		Each	3	

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CHRISTIAN-ROGE & ASSOCIATES, INC.

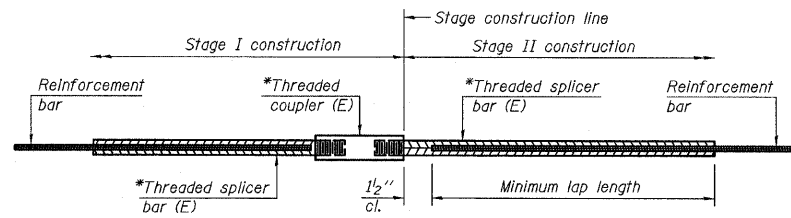
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PLOT SCALE = 50.0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -
PLOT DATE == 10/12/2011	DRAWN - F.M.	REVISED -
	DATE - OCTOBER 12, 2011	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER E16 REPAIRS
STRUCTURE NO. 016-1075

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2011-031-BR	COOK	41	28
CONTRACT NO. 60P37				
ILLINOIS FED. AID PROJECT				

SHEET NO. S28 OF S41 SHEETS



STANDARD BAR SPLICER ASSEMBLY

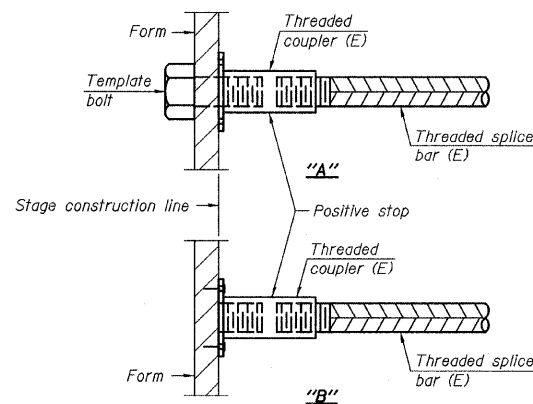
Bar size to be spliced	Minimum Lap Lengths				
	Table 1	Table 2	Table 3	Table 4	Table 5
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-3"
5	1'-9"	2'-5"	2'-7"	2'-11"	2'-10"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-4"
7	2'-9"	3'-10"	4'-2"	4'-8"	4'-6"
8	3'-8"	5'-1"	5'-5"	6'-2"	5'-10"
9	4'-7"	6'-5"	6'-10"	7'-9"	7'-5"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Top bar lap, Class B

Threaded splicer bar length = min. lap length + 1/2" + thread length

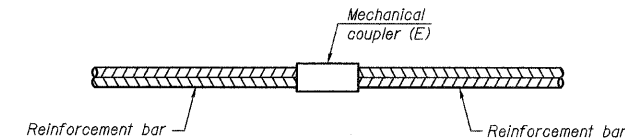
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length



INSTALLATION AND SETTING METHODS

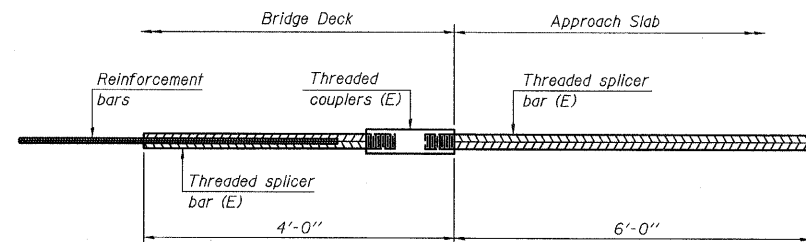
"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

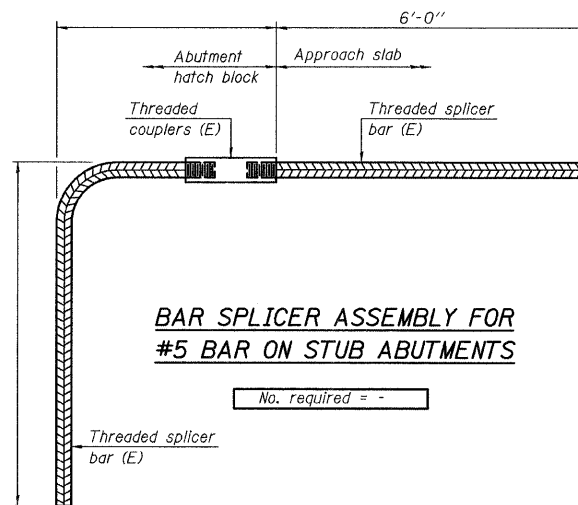
Structure number	Location	Bar size	No. assemblies required
016-1045	Pier ES3 Top of Pier	#11	12
	Pier ES3 East & West Faces of Pier	#5	4
	Pier ES6 Top of Pier	#11	12
	Pier ES6 East & West Faces of Pier	#5	4
	Pier ES6 Column	#9	16

Structure number	Location	Bar size	No. assemblies required
016-1075	Pier E15 Top of Pier	#9	12
	Pier E15 East & West Faces of Pier	#5	4
	Pier E16 Top of Pier	#9	6
	Pier E16 Top of Pier	#10	6
	Pier E16 East & West Faces of Pier	#5	4



BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required = -



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required = -

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See special provision for Mechanical Splicers.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

FILE NAME = I:\102908_1-55\STRUCTURAL\sheet\1075\016037-29-bar_splicer.dgn

BSD-1 7-1-10

CHRISTIAN-ROGE & ASSOCIATES, INC.

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PLOT SCALE = 50.0000' / IN.	CHECKED - B.N.S./J.C.N.	REVISED -
PLOT DATE == 10/12/2011	DRAWN - F.M.	REVISED -
	DATE - OCTOBER 12, 2011	REVISED -

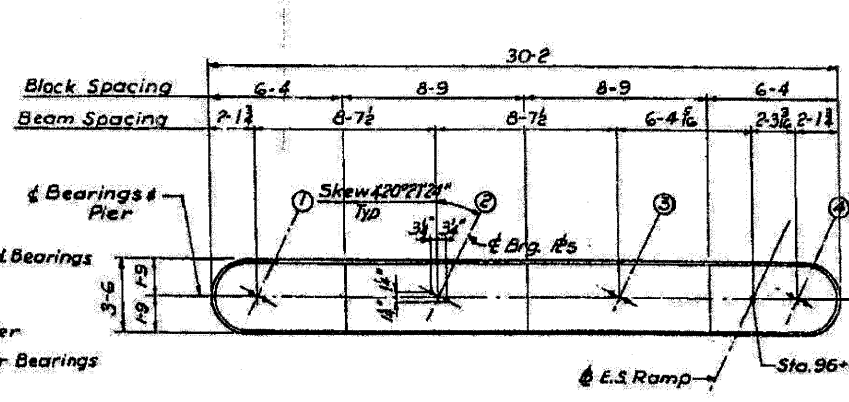
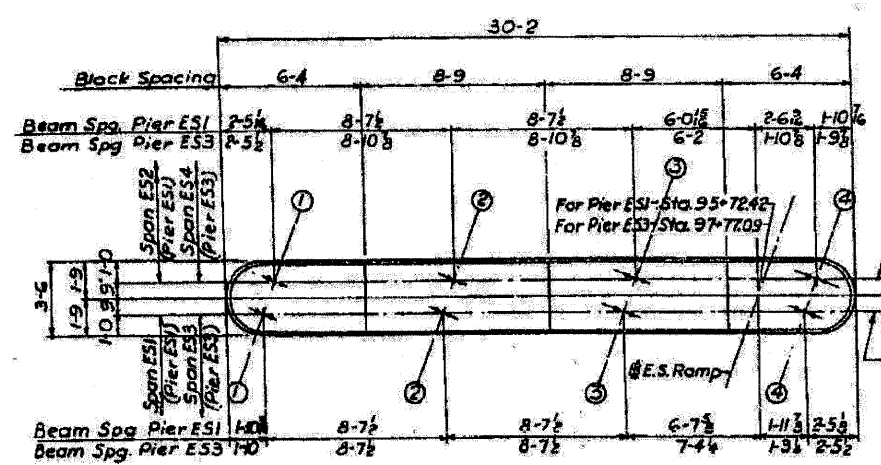
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS STRUCTURE NO'S. 016-1045 & 016-1075

SHEET NO. S29 OF S41 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2011-031-BR	COOK	41	29
CONTRACT NO. 60P37				
ILLINOIS FED. AID PROJECT				

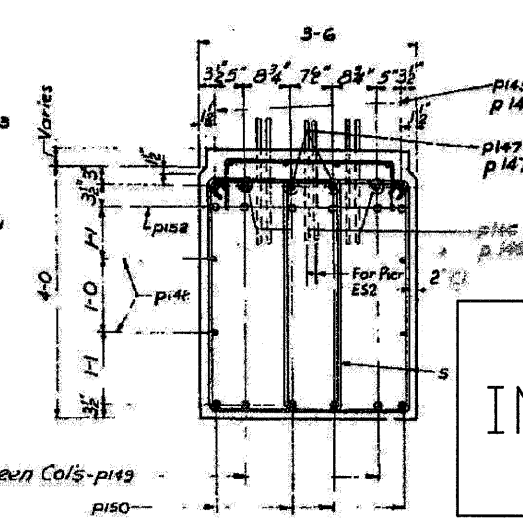
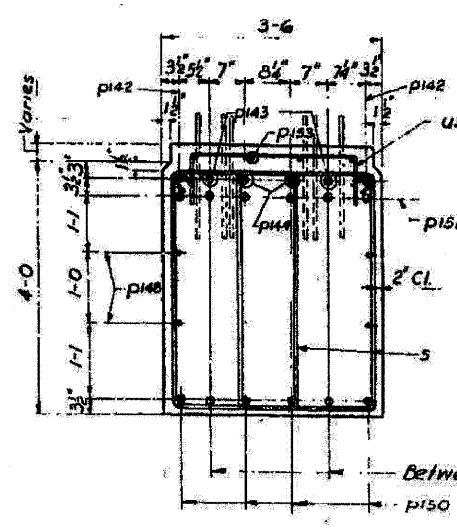
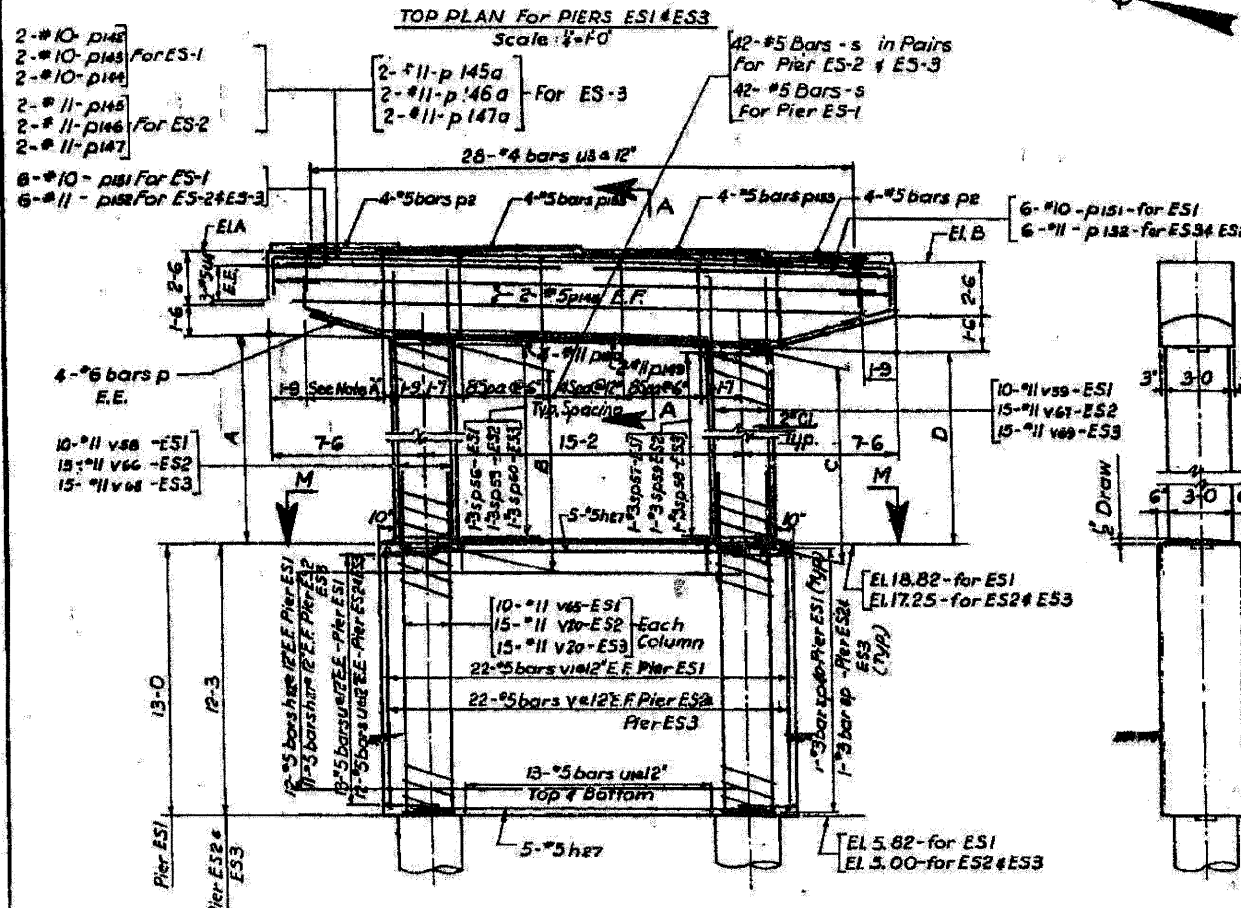
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FED. ROAD DIV. NO. 4	ILLINOIS	PROJECT LG-95-7(181)-200		



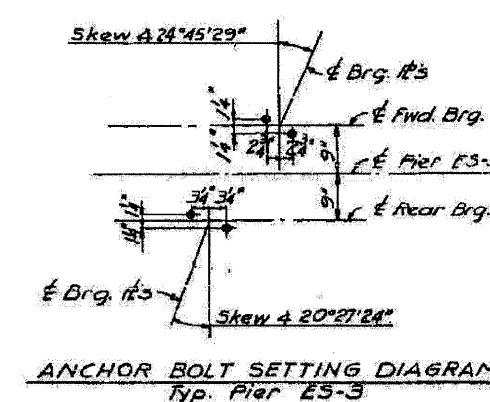
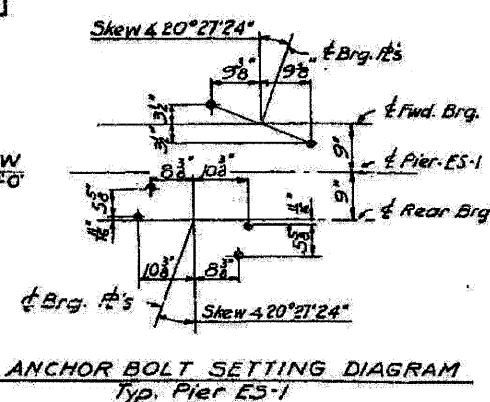
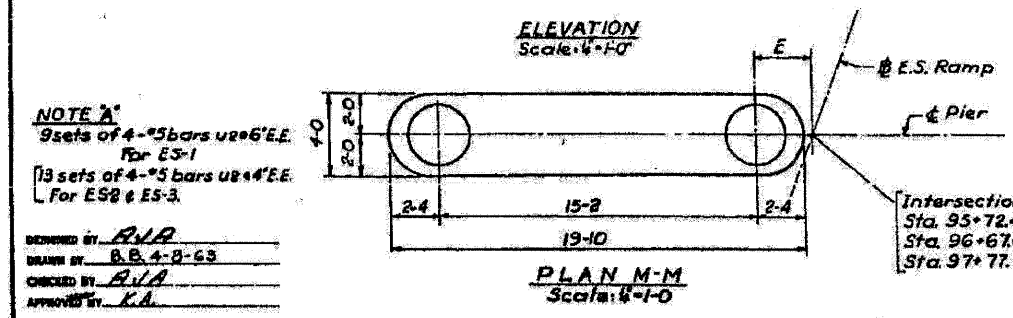
BILL OF MATERIAL

DESCRIPTION	UNIT	QUANTITY		
		Pier ES1	Pier ES2	Pier ES3
Class "X" Concrete	Cu Yds	58.2	57.1	57.2
Reinforcement Bars	Lbs.	8,740	11,080	11,030
Class A Excavation for Structures	Cu Yds	12	14	13

PIER	EL. A	EL. B	Top of Block Elevation				DIMENSIONS				
			Beam No.				A	B	C	D	E
			1	2	3	4					
ES1	34.76	34.17	35.02	34.81	34.64	34.51	11-11 1/2	11-9 1/2	11-6	11-4 1/2	3-1
ES2	35.04	34.47	35.30	35.10	34.93	34.81	13-7 1/2	13-7 1/2	13-4 1/2	13-2 1/2	3-1 1/2
ES3	34.93	33.84	35.27	34.92	34.59	34.28	13-8 1/2	13-5 1/2	13-10 1/2	12-7 1/2	3-9 1/2



FOR INFORMATION ONLY



NOTE: For additional pier sections and notes see sheet No. S30

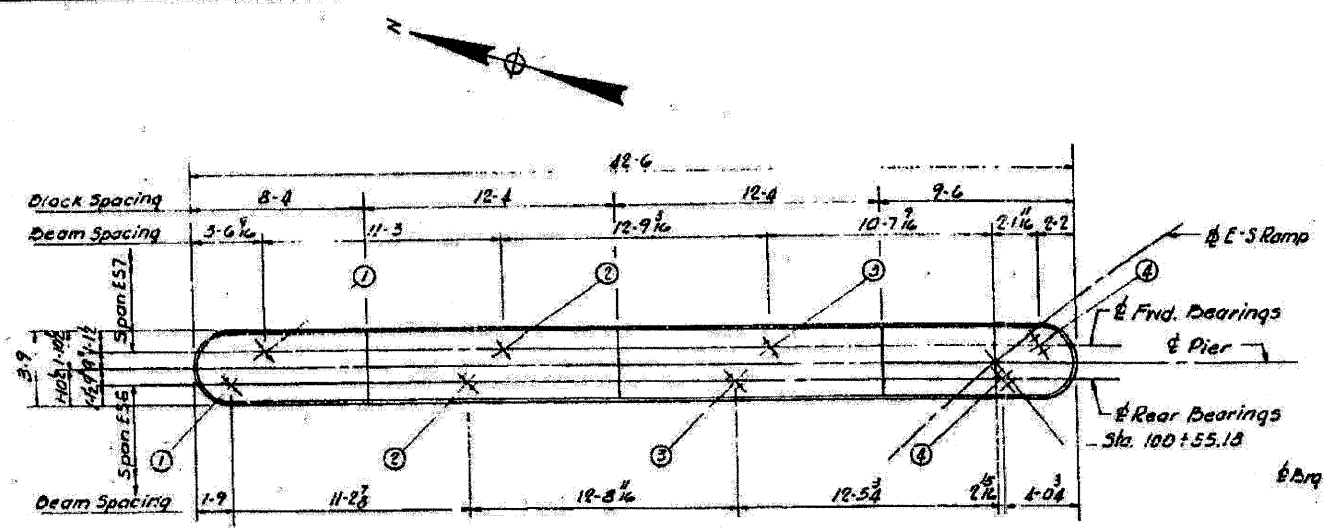
NOTE A
9 sets of 4-#5 bars used for ES-1
13 sets of 4-#5 bars used for ES2 & ES-3.

DESIGNED BY: R.V.R.
DRAWN BY: B.B. 4-2-63
CHECKED BY: R.V.R.
APPROVED BY: K.A.

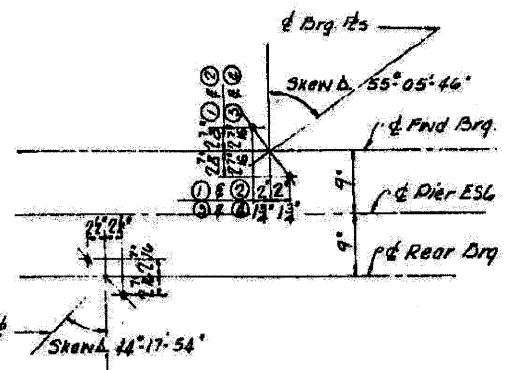
CITY OF CHICAGO
DEPARTMENT OF PUBLIC WORKS
BUREAU OF ENGINEERING
SOUTHWEST ROUTE SUPERHIGHWAY
ELEVATED HIGHWAY
S. LAKE PARK AVE. TO S. LAKE SHORE DRIVE
SECTION SW-1818.3-2H
PIERS ES1, ES2 & ES3
SCALE: AS NOTED DEC. 1984
SHEET NO. S-25 OF 39 SHEETS

FILE NAME = J:\102908_1-55\STRUCTURAL\Sheet\Existing Plans\DISOP37-30-pp-piers ES1-ES2 & ES3.dgn

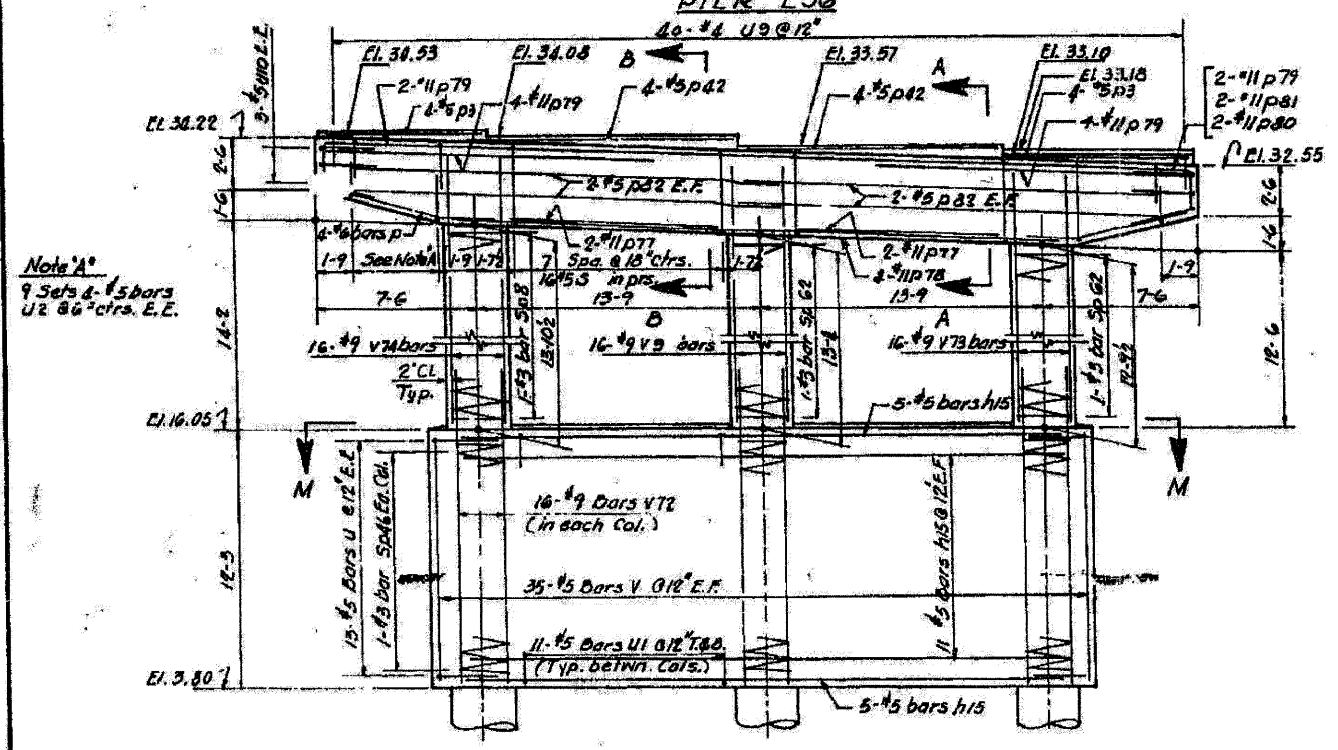
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I.-55	SW-1818.3-2H	COOK	99	48
FED. ROAD DIV. NO. 4		ILLINOIS	PROJECT I.G.-02-7(00)-289	



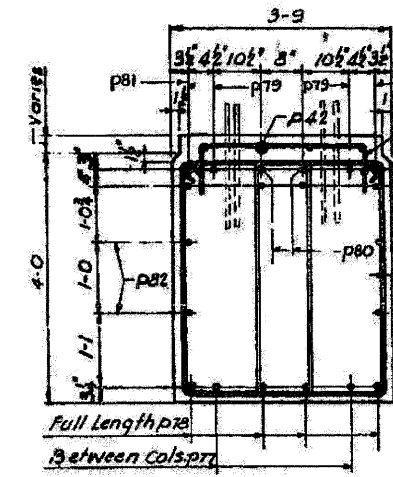
TOP PLAN
Scale 1/4"=1'-0"
PIER ES6
40'-4" U9@12"



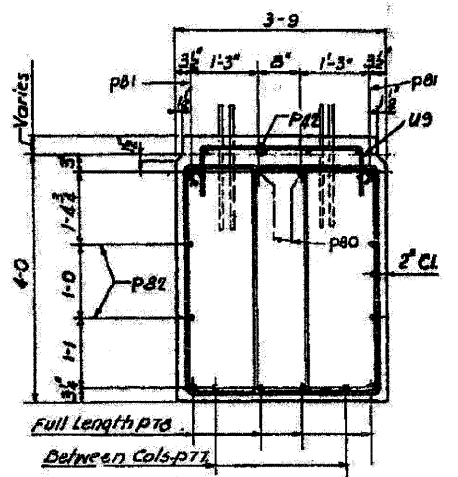
ANCHOR BOLT SETTING DIAGRAM
(Typ. Pier ES6 except as noted)



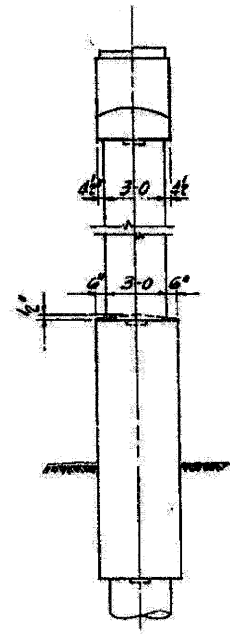
ELEVATION
Scale 1/4"=1'-0"



SECTION A-A
Scale 3/4"=1'-0"



SECTION B-B
Scale 1/4"=1'-0"



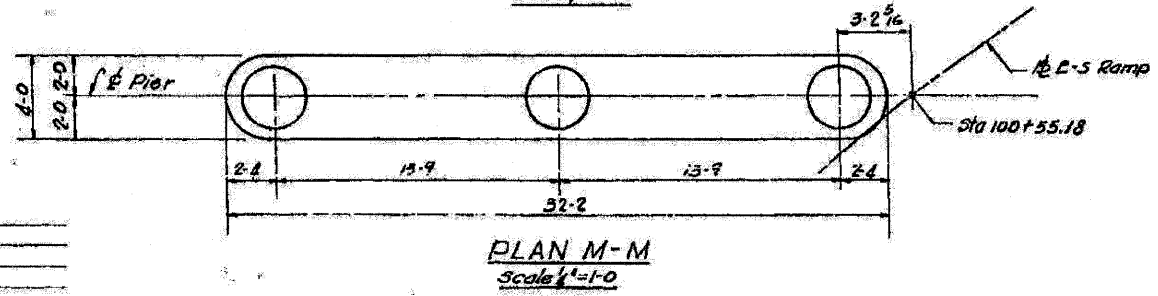
END VIEW
Scale 1/4"=1'-0"

FOR INFORMATION ONLY

Note 'A'
9 Sets of #5 bars
U2 @ 6" c/crs. E.E.

Note
For additional Pier Sections & notes see
Sheet 530

DESIGNED BY: A.I.A.
DRAWN BY: A.S.
CHECKED BY: A.I.A.
APPROVED BY: K.A.



PLAN M-M
Scale 1/4"=1'-0"

BILL OF MATERIAL

DESCRIPTION	UNIT	QUANTITY
Class X Concrete	Cu.Yds.	921
Reinforcement Bars	Lbs.	18,250
Class A Excavation for Structures	Cu.Yds.	24

CITY OF CHICAGO
DEPARTMENT OF PUBLIC WORKS
BUREAU OF ENGINEERING
SOUTHWEST ROUTE SUPERHIGHWAY
ELEVATED HIGHWAY
S. LAKE PARK AVE TO S. LAKE SHORE DRIVE
SECTION SW-1818.3-2H
PIER ES6
SCALE: AS NOTED
DEC. 1984
SHEET NO. 5-27 OF 99 SHEETS

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CHRISTIAN-ROGE & ASSOCIATES, INC.

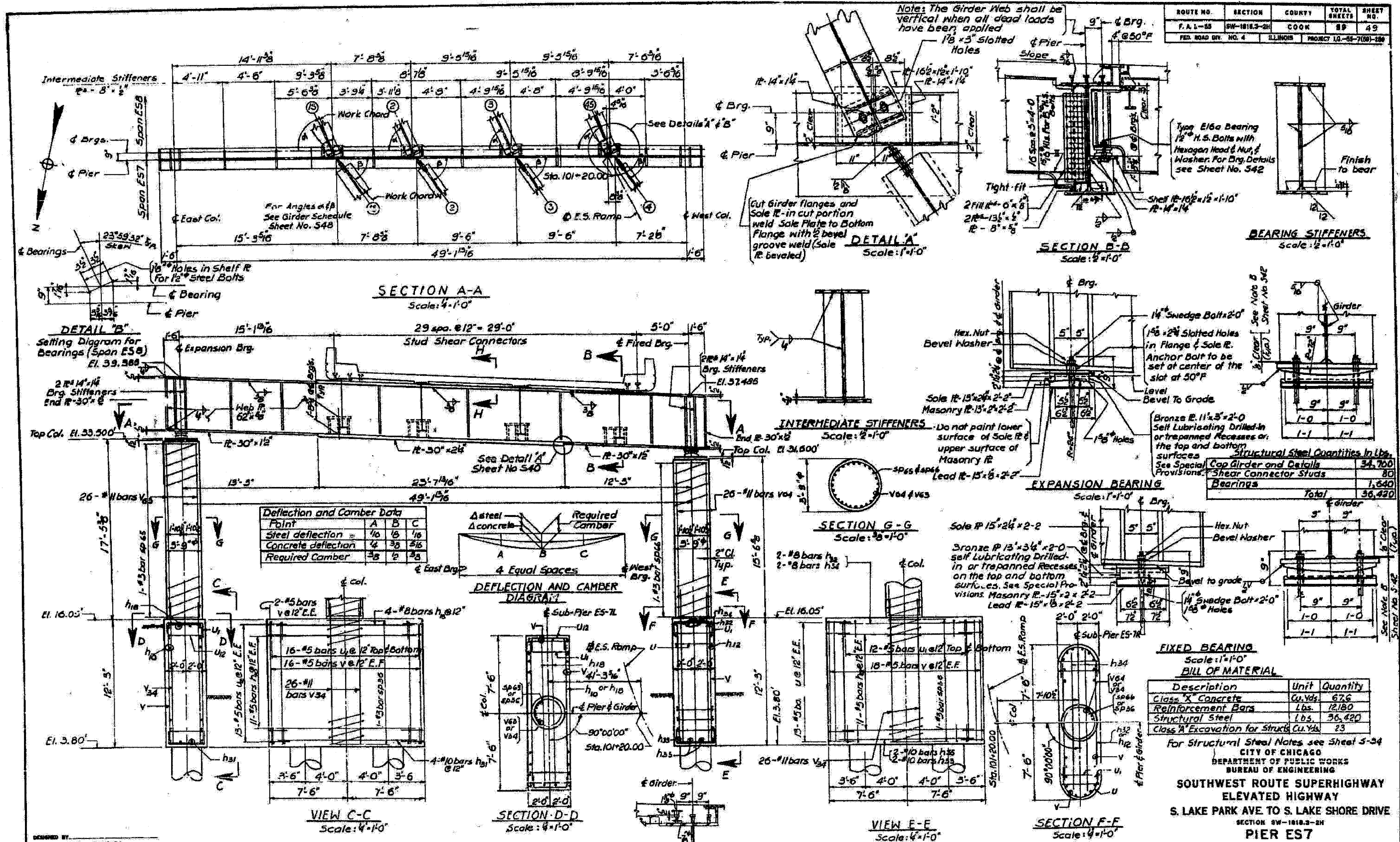
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PLOT DATE = 10/12/2011	DRAWN - F.M.	REVISED -
	DATE - OCTOBER 12, 2011	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER ES6
EXISTING PLANS
SHEET NO. S31 OF S41 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2011-031-BR	COOK	41	31
ILLINOIS FED. AID PROJECT				CONTRACT NO. 60P37

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.L-55	SW-1813-2H	COOK	89	49
FED. ROAD DIV. NO. 4		ILLINOIS	PROJECT 10-65(708)-128	



Point	A	B	C
Steel deflection	1/16	1/8	1/16
Concrete deflection	4	38	5/16
Required Camber	3/8	2	3/8

Item	Quantity
Cap Girder and Details	34,700
Shear Connector Studs	80
Bearings	1,640
Total	36,420

Description	Unit	Quantity
Class "X" Concrete	Cu. Yds.	67.6
Reinforcement Bars	Lbs.	12,180
Structural Steel	Lbs.	36,420
Class A Excavation for Structure	Cu. Yds.	23

FOR INFORMATION ONLY

FILE NAME = I:\102908-1-55\STRUCTURAL\Sheet\Existing Plans\0160P37-32-per-ES7.dgn

DESIGNED BY: J.C.N./B.N.S.
 DRAWN BY: F.M.
 CHECKED BY: A.M. 4/2/63

USER NAME = IDOT	DESIGNED - J.C.N./B.N.S.	REVISED -
CHRISTIAN-ROGE & ASSOCIATES, INC.	CHECKED - B.N.S./J.C.N.	REVISED -
PLOT SCALE = 50.0000' / IN.	DRAWN - F.M.	REVISED -
PLOT DATE = 10/12/2011	DATE - OCTOBER 12, 2011	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PIER ES7
 EXISTING PLANS
 SHEET NO. S32 OF S41 SHEETS

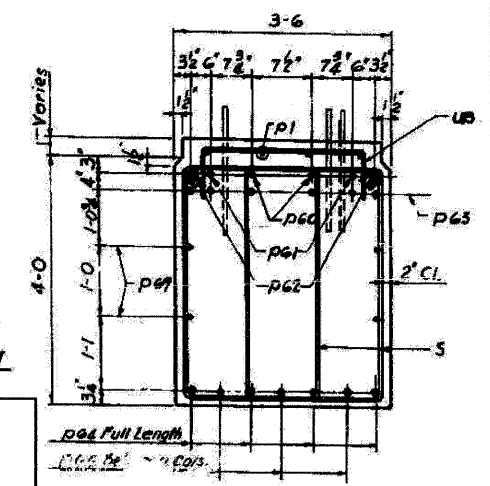
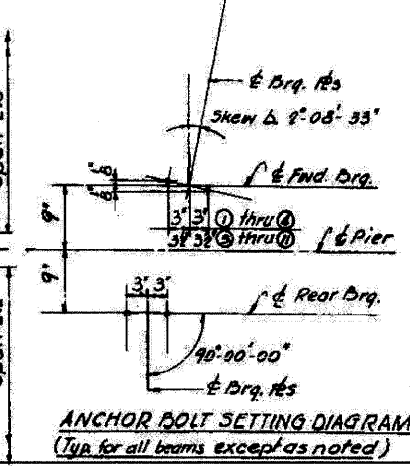
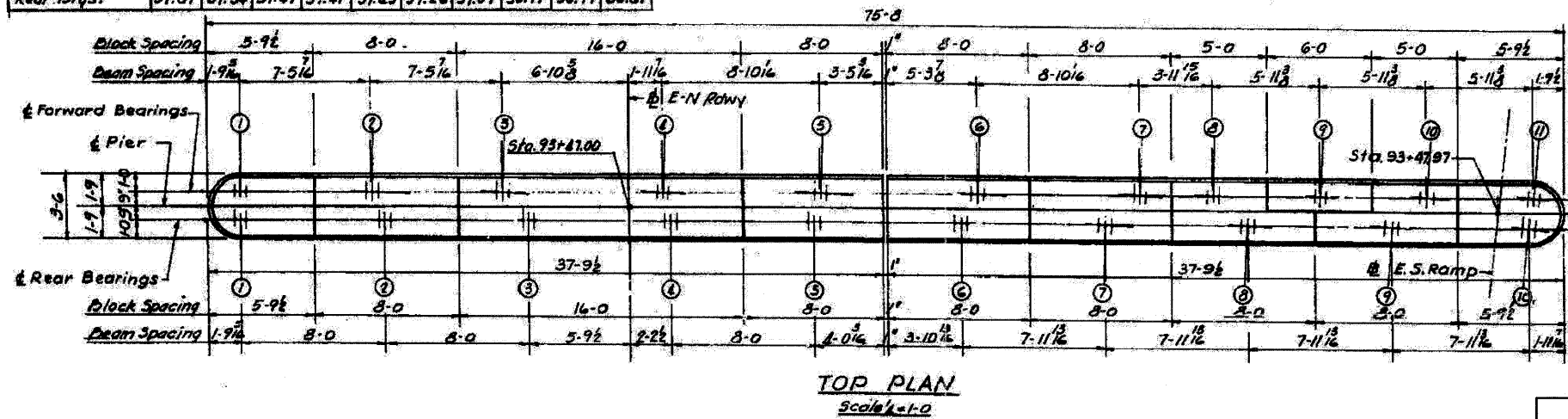
F.A.I. R.T.E. 55	SECTION 2011-031-BR	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 32
CONTRACT NO. 60P37				ILLINOIS FED. AID PROJECT

SCALE: AS NOTED
 SHEET NO. S-28 OF 89 SHEETS
 DEC. 1964

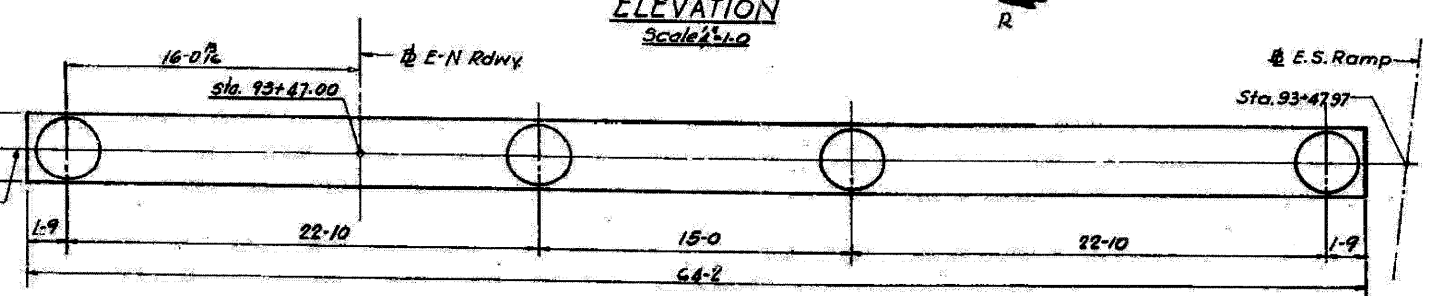
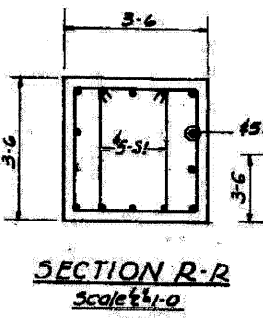
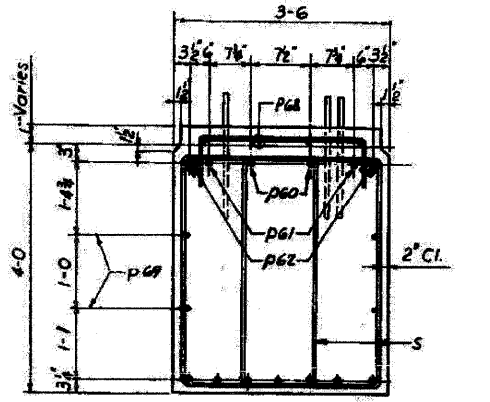
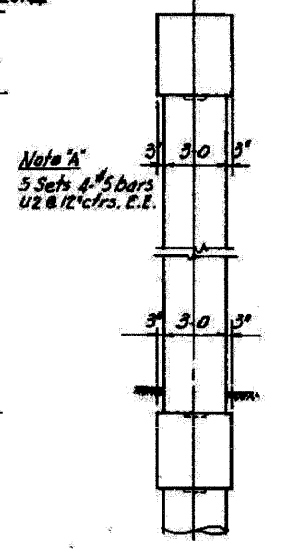
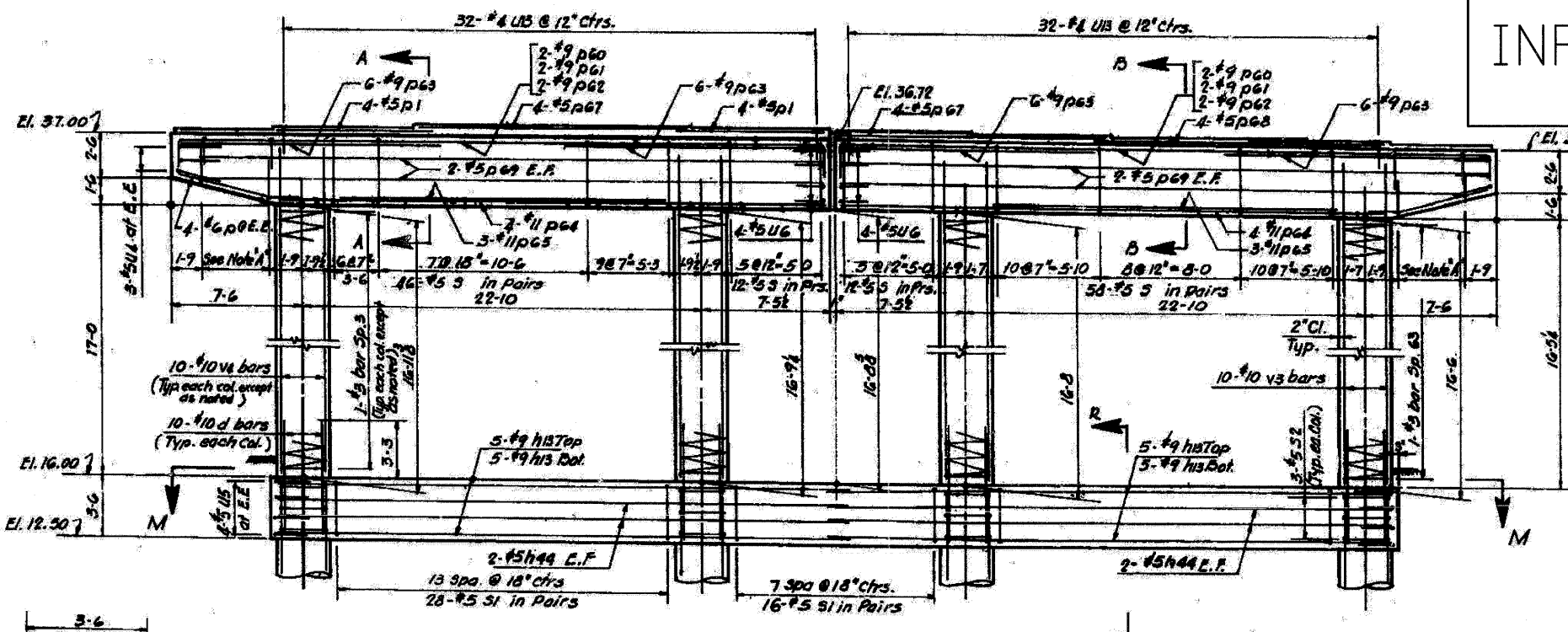
For Structural Steel Notes see Sheet S-34
 CITY OF CHICAGO
 DEPARTMENT OF PUBLIC WORKS
 BUREAU OF ENGINEERING
 SOUTHWEST ROUTE SUPERHIGHWAY
 ELEVATED HIGHWAY
 S. LAKE PARK AVE TO S. LAKE SHORE DRIVE
 SECTION SW-1813-2H
 PIER ES7

ELEVATIONS TOP OF BLOCK											
Beam No.	1	2	3	4	5	6	7	8	9	10	11
Forward Brgs.	37.07	37.34	37.47	37.47	37.43	37.26	37.09	36.97	36.89	36.77	36.61
Rear Brgs.	37.07	37.34	37.47	37.47	37.43	37.26	37.09	36.97	36.77	36.61	

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I.-55	SW-1818.3-2H	COOK	99	39
FED. ROAD DIV. NO. 4		ILLINOIS	PROJECT LG-55-7(50)-200	



FOR INFORMATION ONLY



BILL OF MATERIAL

DESCRIPTION	UNIT	QUANTITY
Class X Concrete	Cu.Yds.	89.0
Reinforcement Bars	Lbs.	16,290
Class A Excavation for Structure	Cu.Yds.	37

NOTE:
For additional pier sections and notes see sheet No. 330.

CITY OF CHICAGO
DEPARTMENT OF PUBLIC WORKS
BUREAU OF ENGINEERING
**SOUTHWEST ROUTE SUPERHIGHWAY
ELEVATED HIGHWAY**
S. LAKE PARK AVE TO S. LAKE SHORE DRIVE
SECTION SW-1818.3-2H
PIER E12
SCALE: AS NOTED DEC. 1964
SHEET NO. 5-18 OF 99 SHEETS

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DESIGNED BY
DRAWN BY A.S.
CHECKED BY A.M. 4.22.65
APPROVED BY K.A.

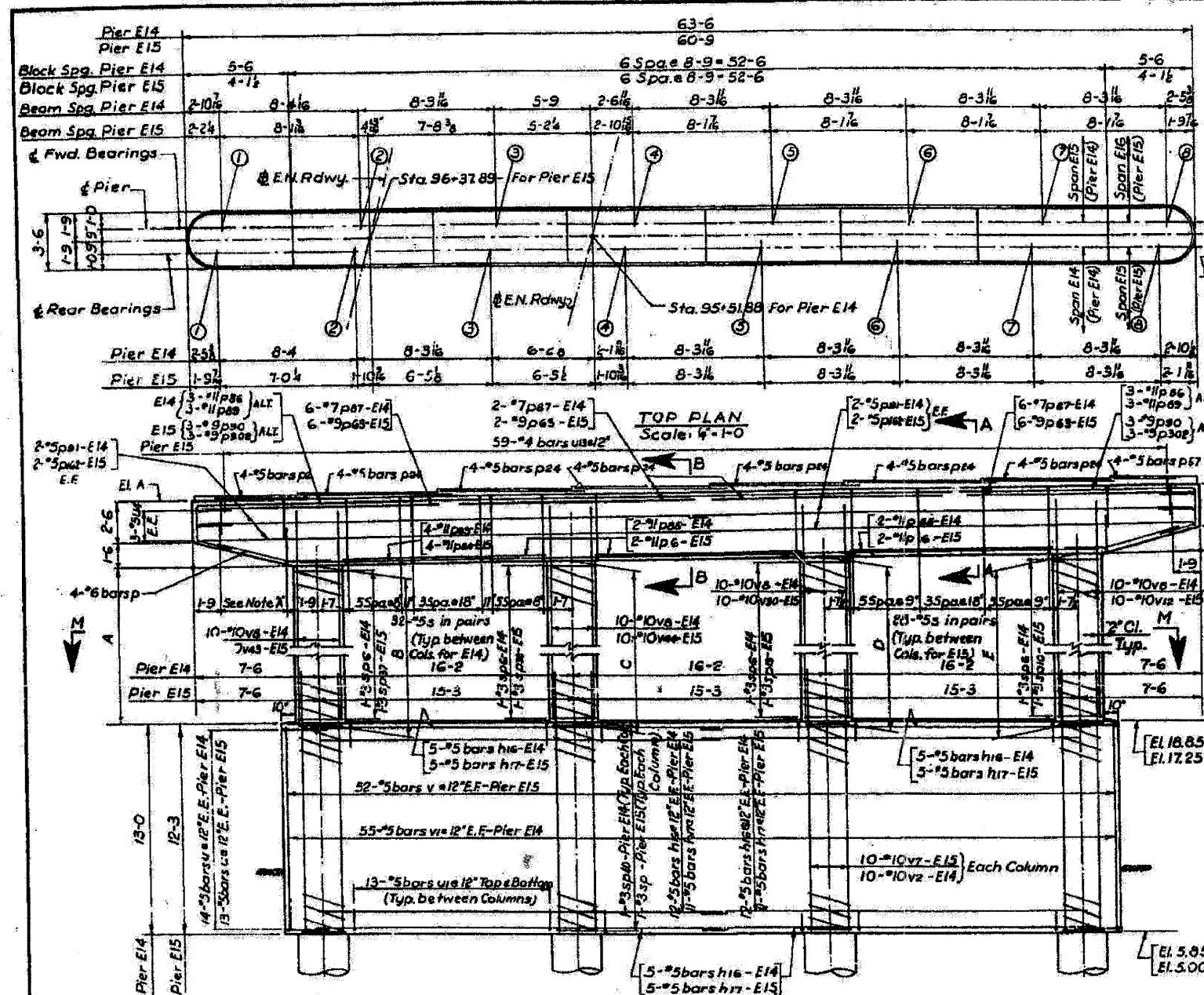
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PLOT DATE = 10/12/2011	DRAWN - F.M.	REVISED -
	DATE - OCTOBER 12, 2011	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

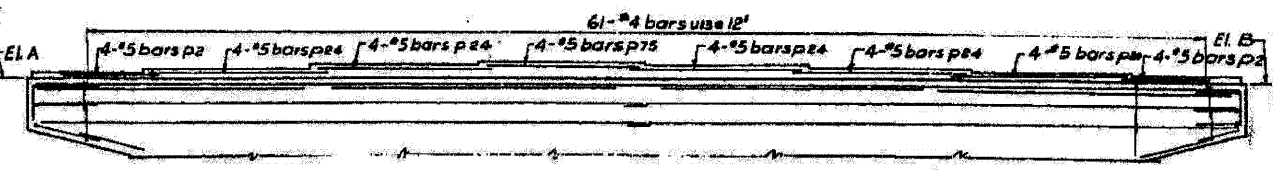
PIER E12
EXISTING PLANS
SHEET NO. S33 OF S41 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2011-031-BR	COOK	41	33
				CONTRACT NO. 60P37
ILLINOIS FED. AID PROJECT				

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.L.-85	SW-18183-2H	COOK	99	41
FED. ROAD DIV. NO. 4 ILLINOIS PROJECT LG-50-7(20)-229				



PIER	ELEVATIONS																DIMENSIONS															
	A	B	Top of Block Rear Bearings								Top of Block Forward Bearings								A	B	C	D	E	F								
			1	2	3	4	5	6	7	8	1	2	3	4	5	6	7	8														
E14	35.33	35.33	35.49	35.71	35.79	35.79	35.70	35.61	35.61	35.49	35.71	35.79	35.79	35.70	35.61	35.61	35.49	35.71	35.79	35.79	35.70	35.61	35.61	12-5 1/2	12-5 1/2	12-5 1/2	12-5 1/2	12-5 1/2	12-5 1/2			
E15	35.42	36.70	35.70	35.95	36.10	36.26	36.42	36.60	36.75	36.98	35.70	35.95	36.10	36.26	36.42	36.60	36.75	36.98	35.70	35.95	36.10	36.26	36.42	36.60	36.75	36.98	14-2	14-3 1/2	14-7 1/2	14-11 1/2	15-3 1/2	15-5 1/2

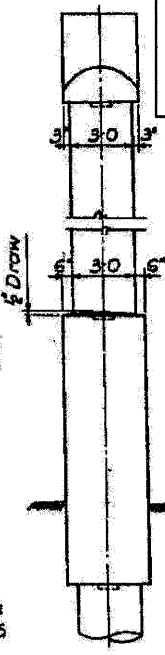


PART ELEVATION - For Pier E14
For Remainder of Elevation and Reinforcement see Pier Elevation

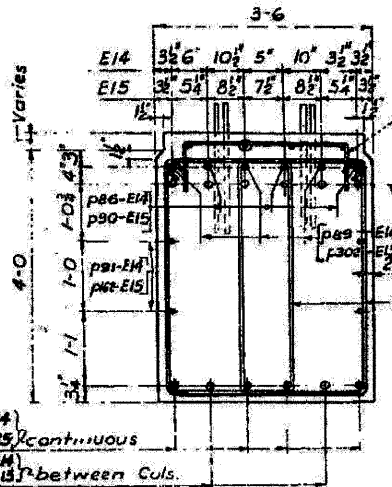
BILL OF MATERIAL

DESCRIPTION	UNIT	QUANTITY	
		Pier E14	Pier E15
Class X Concrete	Cu.Yds.	148.8	138.2
Reinforcement Bars	Lbs.	16,790	16,330
Class A Excavation for Structures	Cu.Yds.	29	39

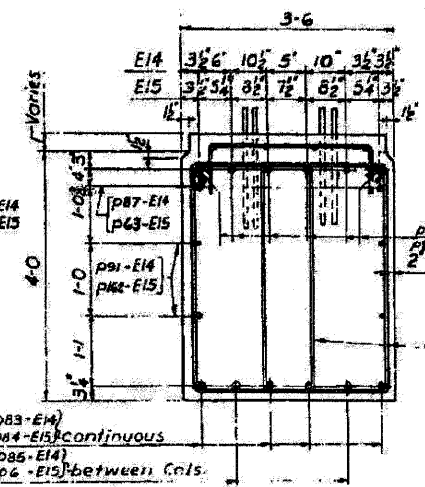
FOR INFORMATION ONLY



END VIEW Scale: 1/4"=1'-0"

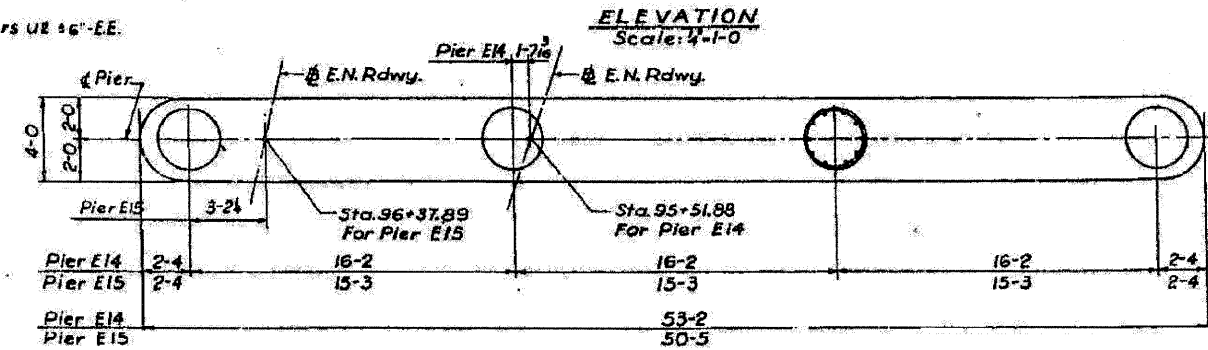


SECTION A-A Scale: 1/4"=1'-0"

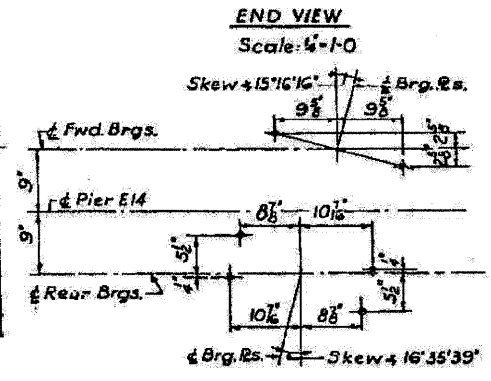


SECTION B-B Scale: 1/4"=1'-0"

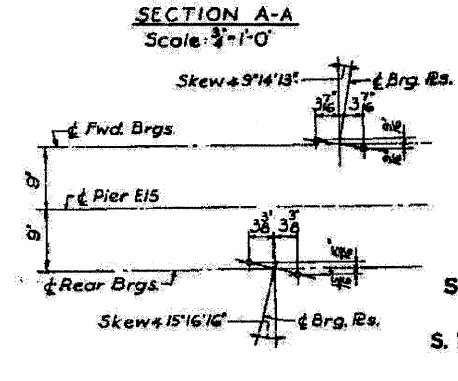
NOTE X
9 Sets of 4-#5 bars use 16"-E.E.



ELEVATION Scale: 1/4"=1'-0"



ANCHOR BOLT SETTING DIAGRAM (Typ. for all Beams at Pier E14)



ANCHOR BOLT SETTING DIAGRAM (Typ. for all Beams at Pier E15)

NOTE:
For additional pier sections & notes see sheet No. 530

CITY OF CHICAGO
DEPARTMENT OF PUBLIC WORKS
BUREAU OF ENGINEERING
**SOUTHWEST ROUTE SUPERHIGHWAY
ELEVATED HIGHWAY**
S. LAKE PARK AVE TO S. LAKE SHORE DRIVE
SECTION SW-18183-2H
PIERS E14 & E15
SCALE: AS NOTED
SHEET NO. 5-20 OF 99 SHEETS
DEC. 1944

FILE NAME = I:\1025986-1-55\STRUCTURAL\Sheet\Existing Plans\10168937-34-piers E14 & E15.dgn

DESIGNED BY RLR
DRAWN BY B.D. 3-17-63
CHECKED BY B.M. 4-23-63
APPROVED BY K.A.

USER NAME = IDOT
PLOT SCALE = 50,0000' / IN.
PLOT DATE = 10/12/2011

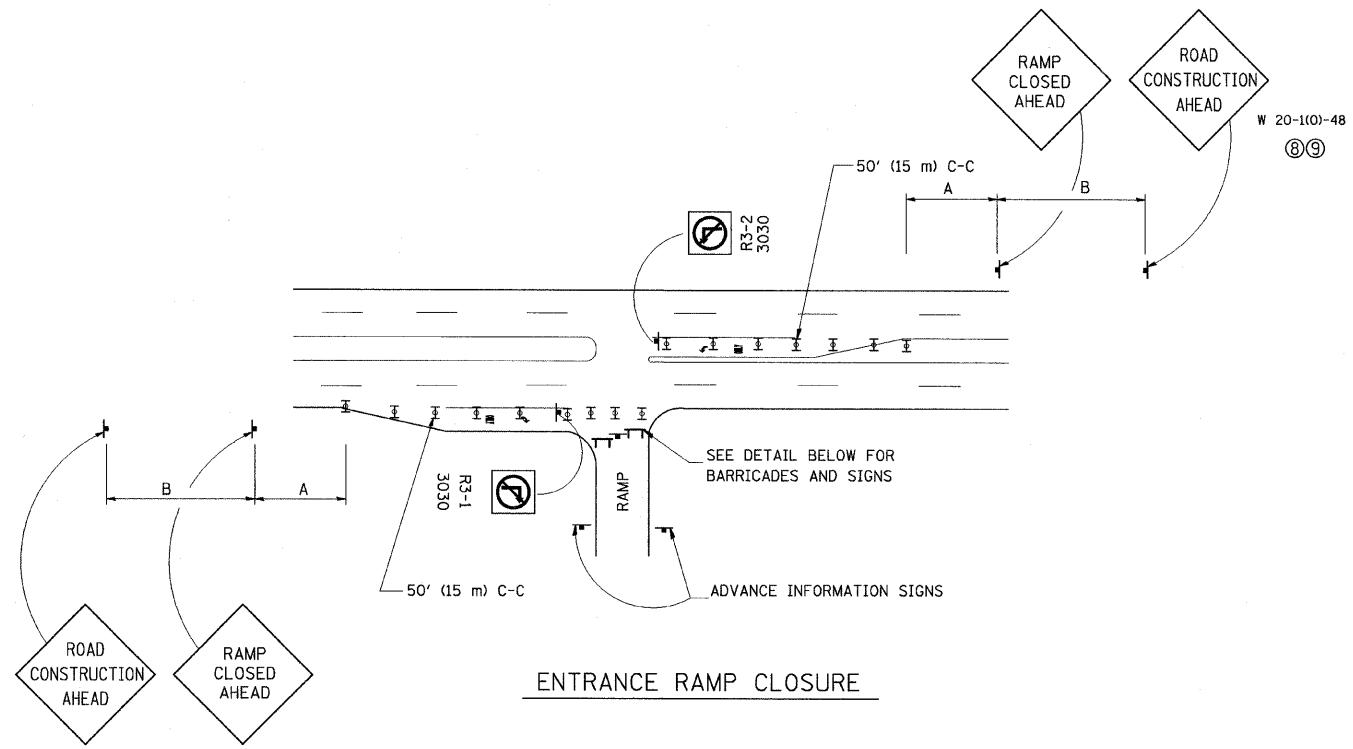
DESIGNED - J.C.N./B.N.S.
CHECKED - B.N.S./J.C.N.
DRAWN - F.M.
DATE - OCTOBER 12, 2011

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIERS E14 & E15
EXISTING PLANS

SHEET NO. 534 OF 541 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2011-031-BR	COOK	41	34
CONTRACT NO. 60P37				
ILLINOIS FED. AID PROJECT				



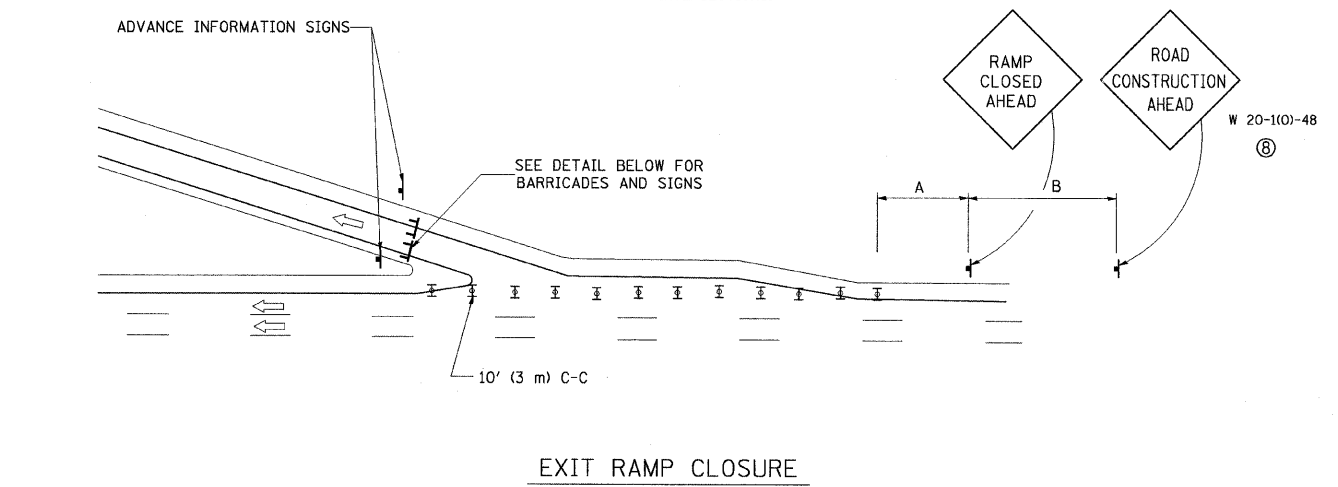
ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL ≥45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	150' (45 m)	150' (45 m)

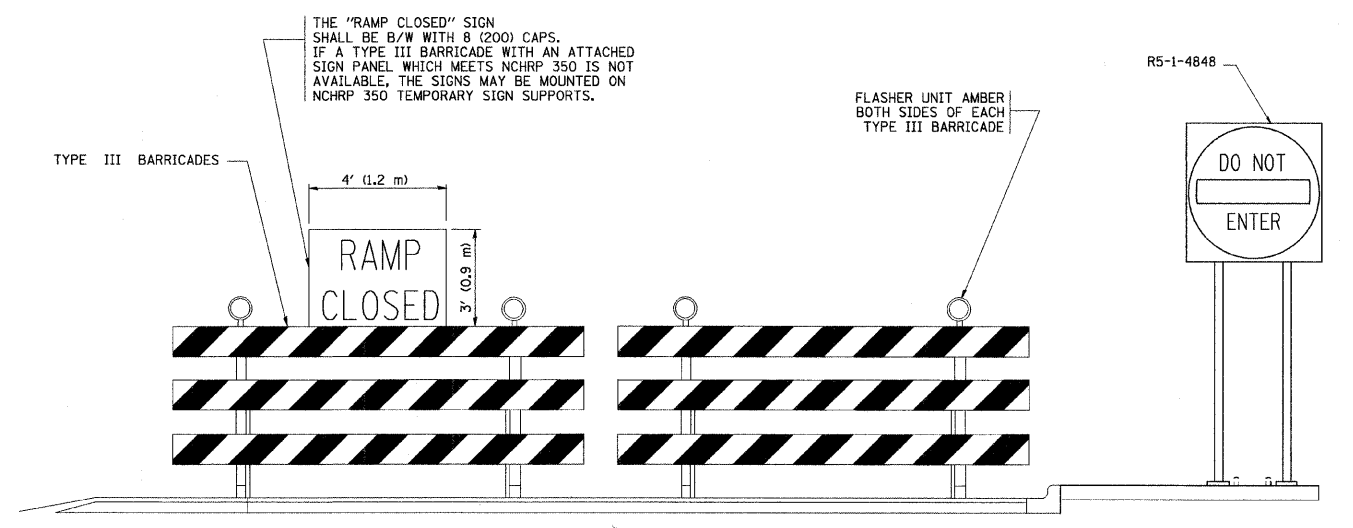
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.

W 20-100-48 (8)(9)

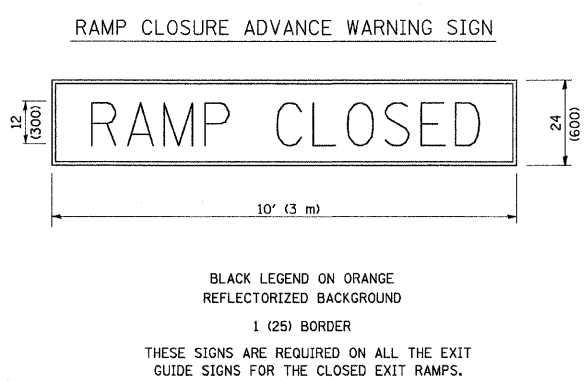


EXIT RAMP CLOSURE

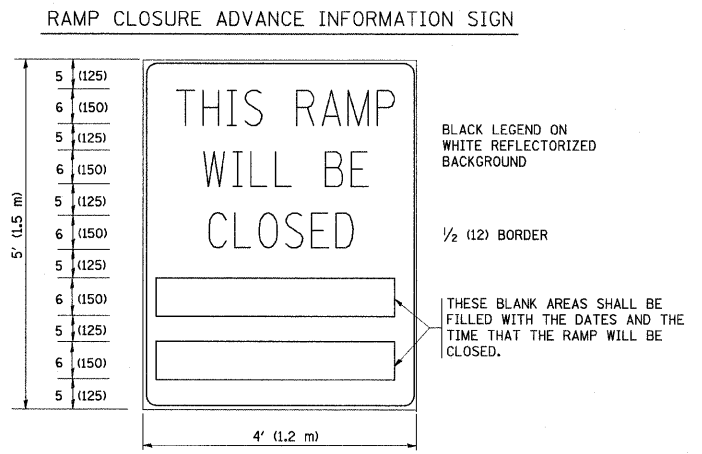
- SYMBOLS**
- ▬ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
 - ▬ TYPE III BARRICADE WITH FLASHING LIGHT



DETAIL FOR REQUIRED BARRICADES & SIGNS



BLACK LEGEND ON ORANGE REFLECTORIZED BACKGROUND
1 (25) BORDER
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR THE CLOSED EXIT RAMPS.



THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

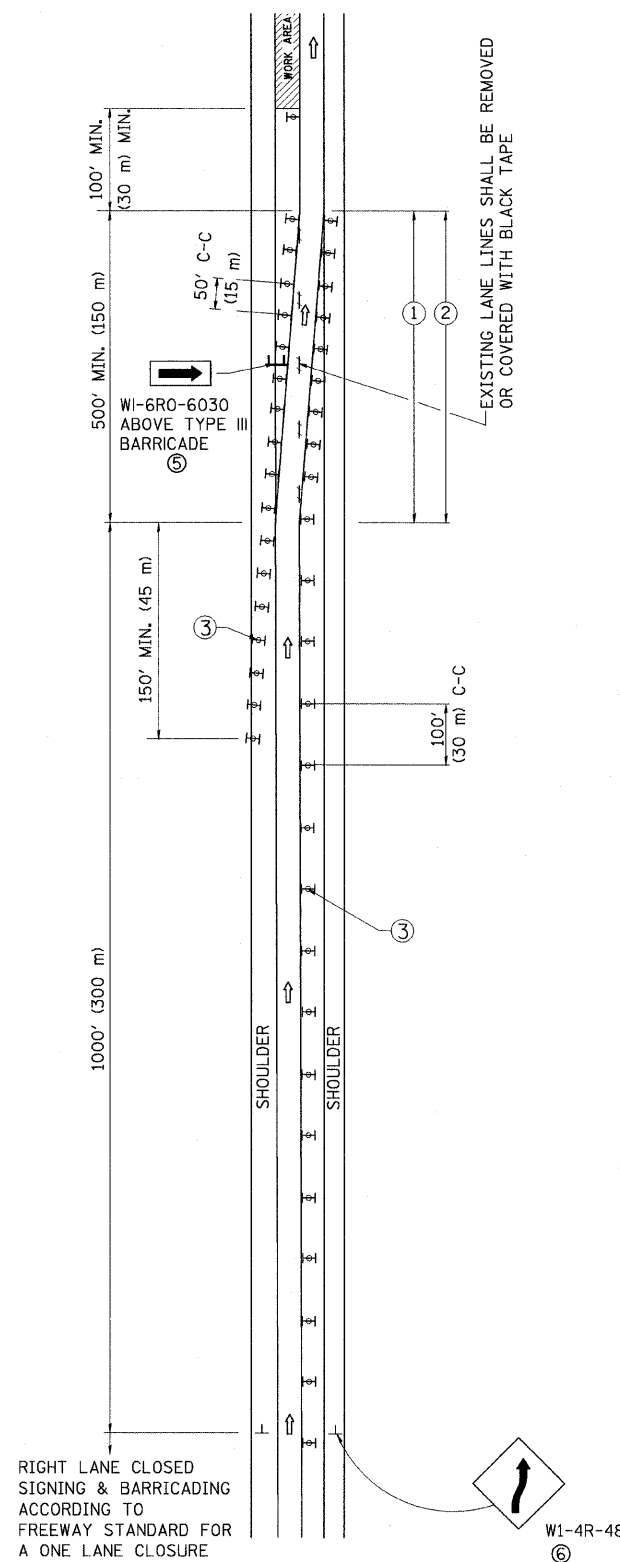
GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY FOUR (24) HOURS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED ON CLOSURES LESS THAN 24 HOURS IN DURATION.

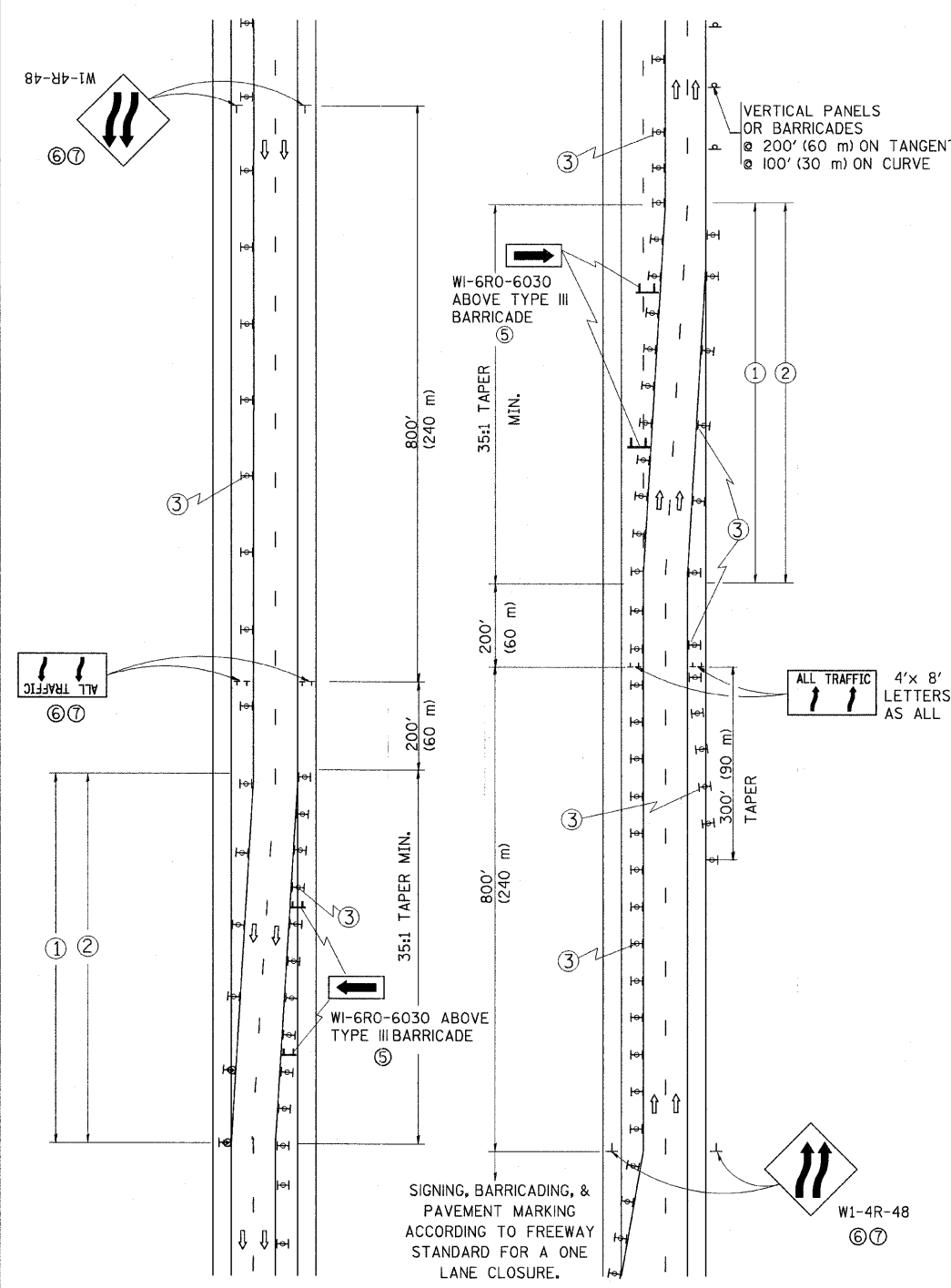
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22\34\to08.dgn	USER NAME = leusa	DESIGNED - DWS	REVISED - DWS/JAF 12-02	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FREEWAY ENTRANCE AND EXIST RAMP CLOSURE DETAILS		F.A.I. RTE. 55	SECTION 2011-031-BR	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 36	
	PLOT SCALE = 50.000 "/ IN.	DRAWN -	REVISED - JAF 02-06		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-08		CONTRACT NO. 60P37	
	PLOT DATE = 1/26/2018	CHECKED -	REVISED - SPB 01-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE - 02-83	REVISED - SPB 12-09									

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 24 HOURS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS. TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

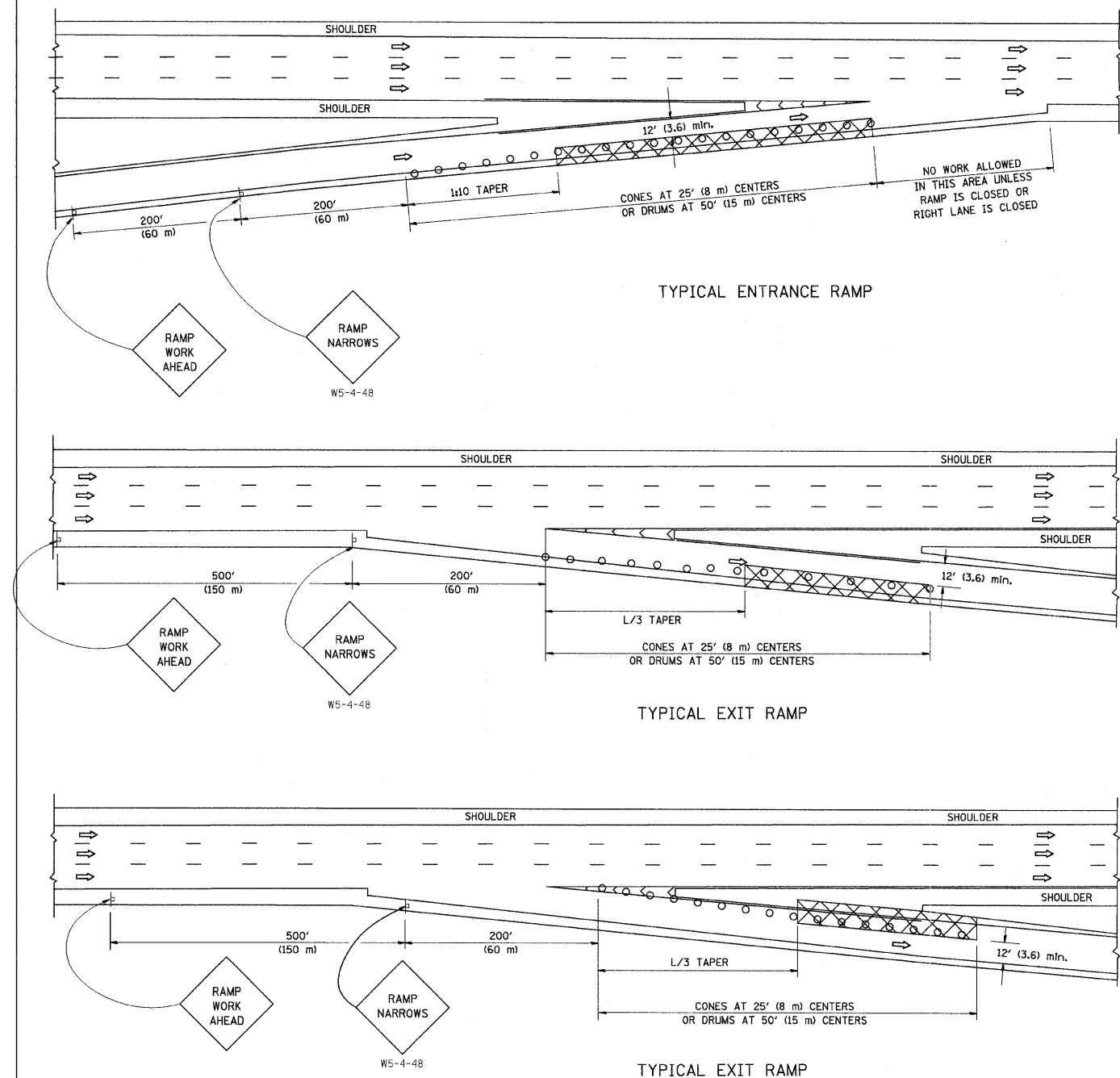
SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\diststd\22x34\to09.dgn	USER NAME = loyca	DESIGNED - DWS	REVISED - JAF 01-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE			F.A.I. RTE. 55	SECTION 2011-031-BR	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 37
PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - JAF 02-06	REVISED - SPB 01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-09 CONTRACT NO. 60P37				
PLOT DATE = 1/26/2010	DATE - 02-87	REVISED - SPB 12-09										
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT												

PARTIAL RAMP CLOSURE DETAILS



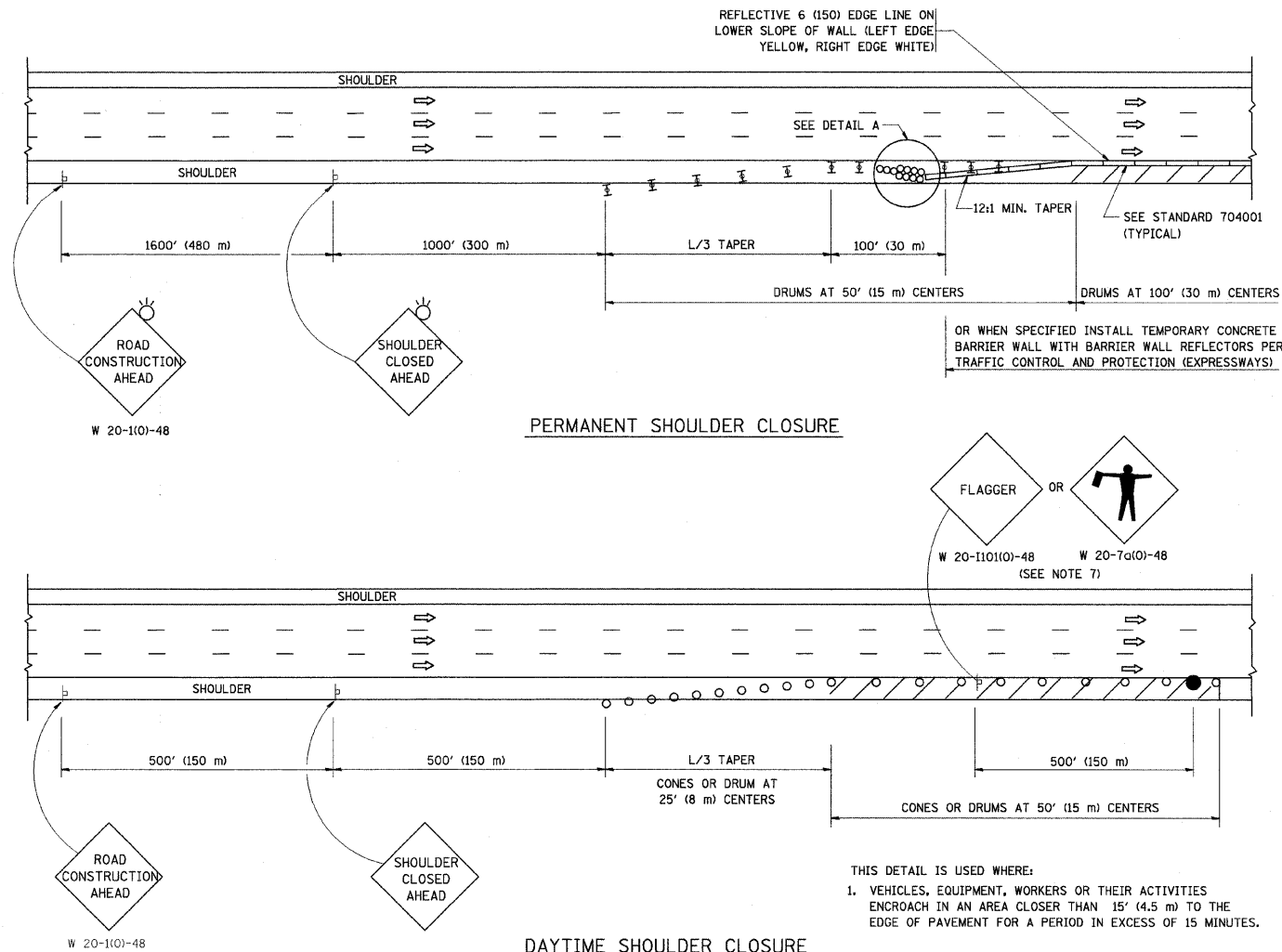
SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

GENERAL NOTES

1. THE "L" DISTANCE EQUALS:
SPEED LIMIT FORMULAS
45 mph (80 km/h) METRIC ENGLISH
OR GREATER: L=0.65(W/S) L=(W/S)
W = WIDTH OF OFFSET IN FEET (METERS)
S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS



PERMANENT SHOULDER CLOSURE

DAYTIME SHOULDER CLOSURE

- THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.

ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350 COMPLIANT.

DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\17.dgn

USER NAME = loysoo
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 1/26/2012

DESIGNED -
DRAWN - D.W.S.
CHECKED -
DATE - 11-96

REVISED - 04-03
REVISED - J.A.F. 12-06
REVISED - S.P.B. 01-07
REVISED - S.P.B. 12-09

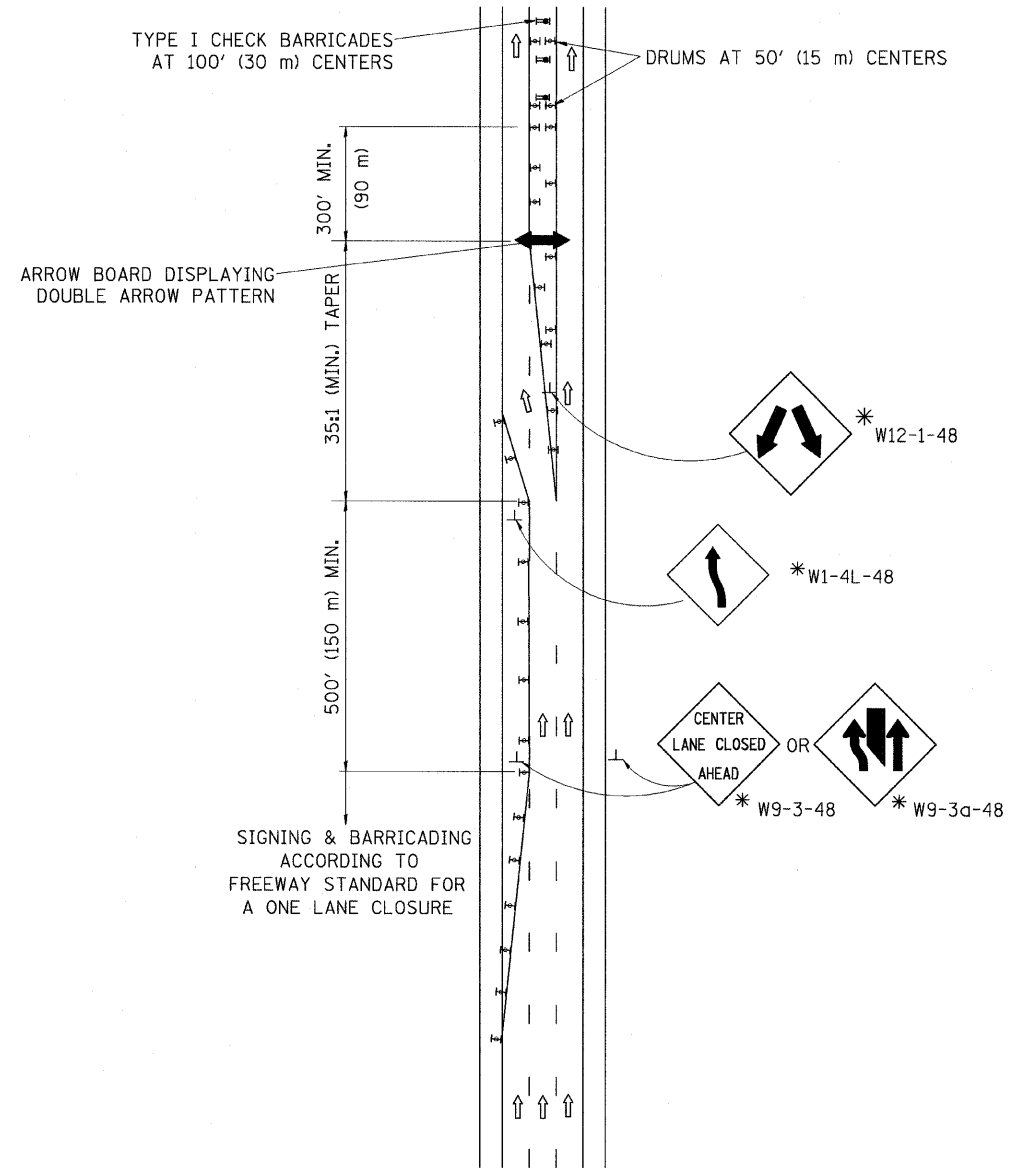
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES

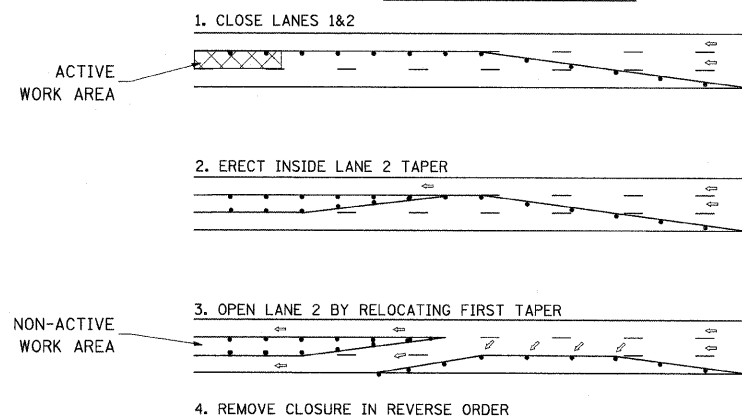
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2011-031-BR	COOK	41	38
TC-17			CONTRACT NO. 60P37	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

CENTER LANE CLOSURE



INSTALLATION SEQUENCE

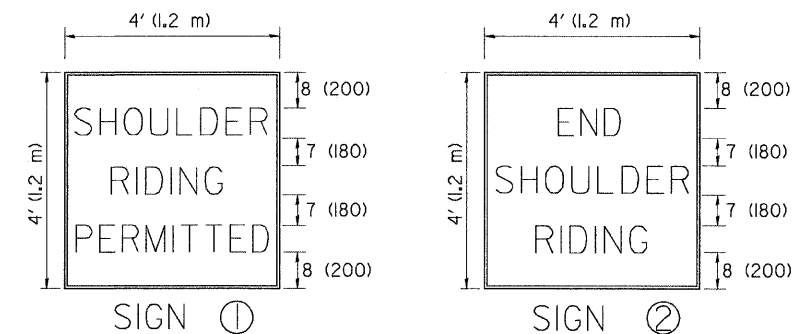
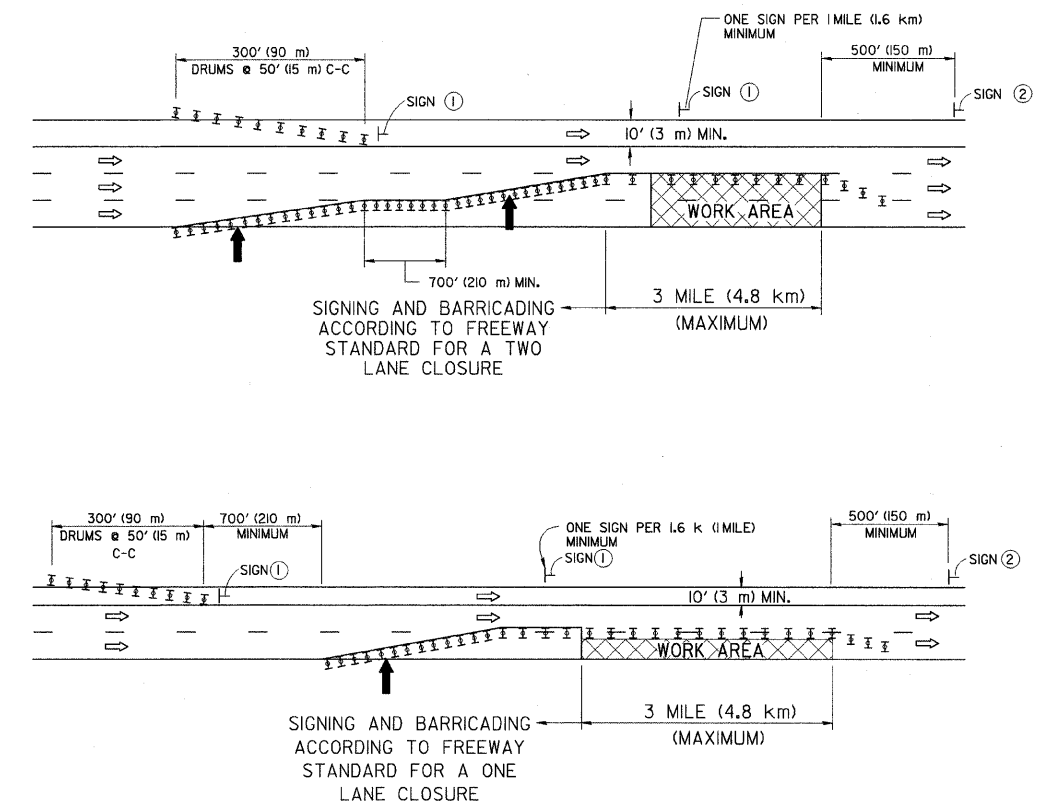


NOTES

1. DRUMS WITH STEADY BURN LIGHTS SHALL BE USED AT 50' (15 m) CENTERS ON ALL TAPERS AND TANGENTS IN ADVANCE OF WORK AREA.
2. CLOSURE SHALL BE USED ONLY FOR OPERATIONS LASTING 72 HOURS OR LESS.
3. CENTER LANE CLOSURE CONFIGURATION IS NOT TO BE USED WITH WORKERS PRESENT.

SHOULDER LANE

NOTE: CLOSURE SHALL BE USED ONLY FOR OPERATIONS LASTING 72 HOURS OR LESS.



6 (150) SERIES "C" LEGEND
BLACK LEGEND
WHITE REFLECT. BACKGROUND
(125) BORDER

SYMBOLS

- ↑ DIRECTION OF TRAFFIC
- ➔ ARROWBOARD
- ▣ ACTIVE WORK AREA
- ⊥ SIGN ON PORTABLE OR PERMANENT SUPPORT *
- ⊥ TYPE II BARRICADE, OR DRUM WITH MONO-DIRECTIONAL STEADY BURN LIGHT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

* ALL SIGNS SHALL BE MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).

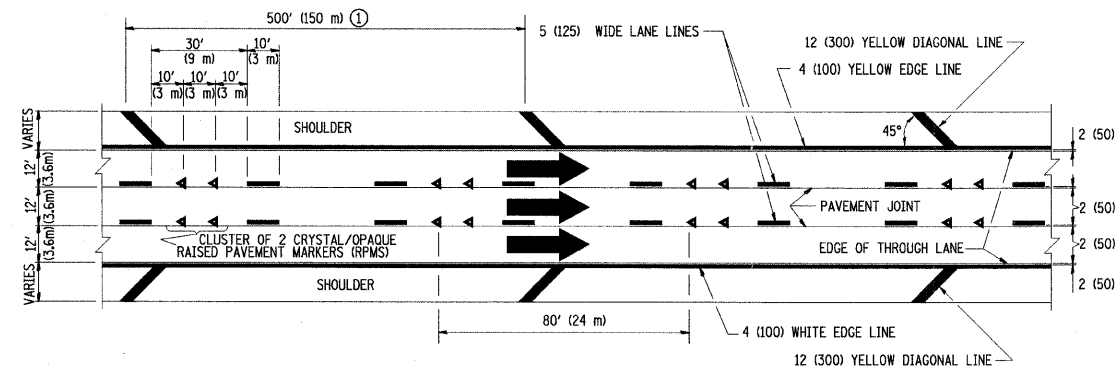
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		DRAWN -	REVISED - S.P.B. 01-07
	PLOT SCALE = 50,000 / IN.	CHECKED -	REVISED - S.P.B. 12-09
	PLOT DATE = 1/26/2010	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY
CENTER LANE CLOSURE SHOULDER LANE

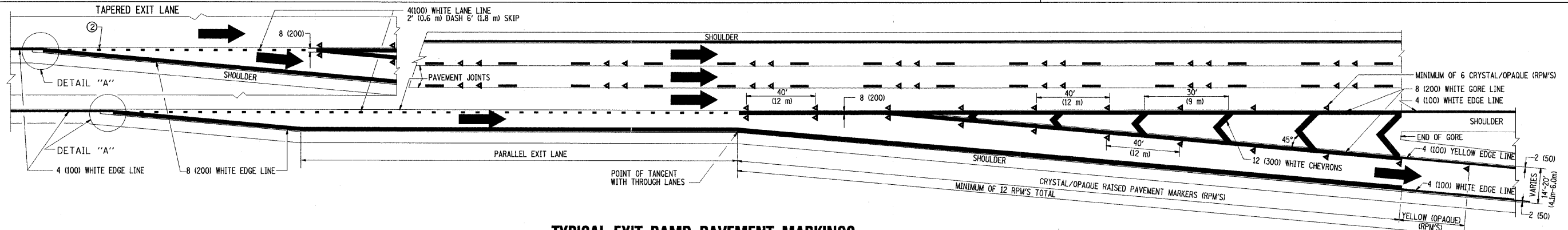
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-25			CONTRACT NO. 60P37	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

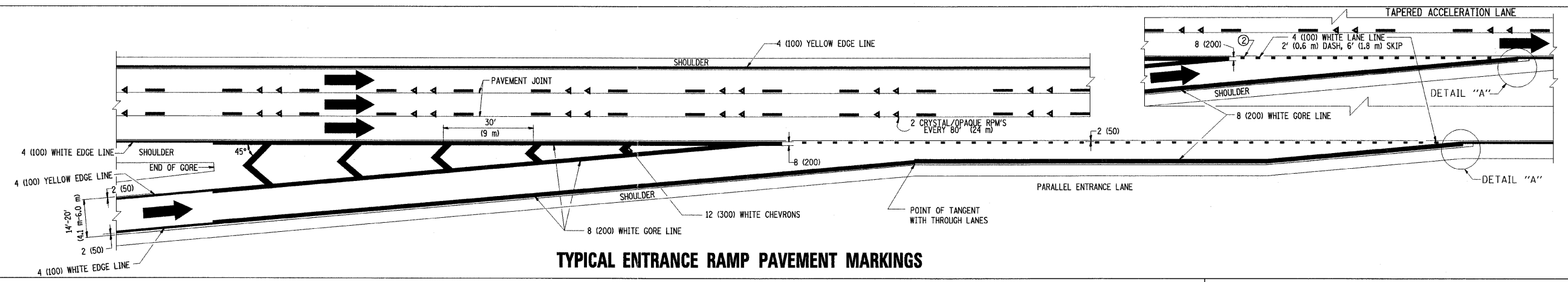


TYPICAL EDGE LINES & LANE LINES

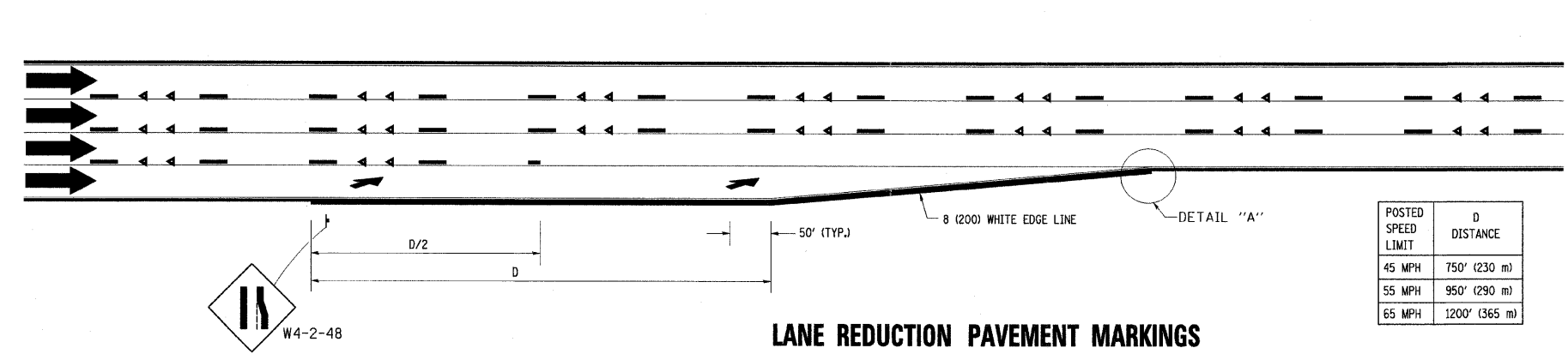
- PAVEMENT MARKING MATERIALS**
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
 2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE SHALL BE USED FOR ALL LANE LINES ON BITUMINOUS PAVEMENT.
 3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC.



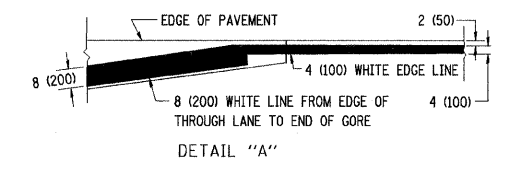
TYPICAL EXIT RAMP PAVEMENT MARKINGS



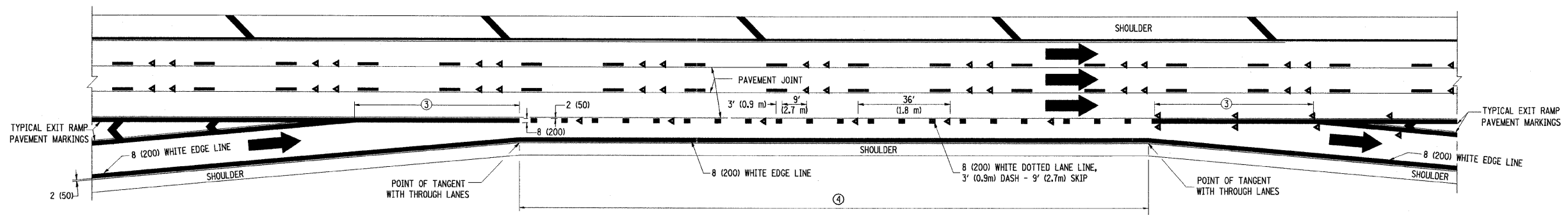
TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



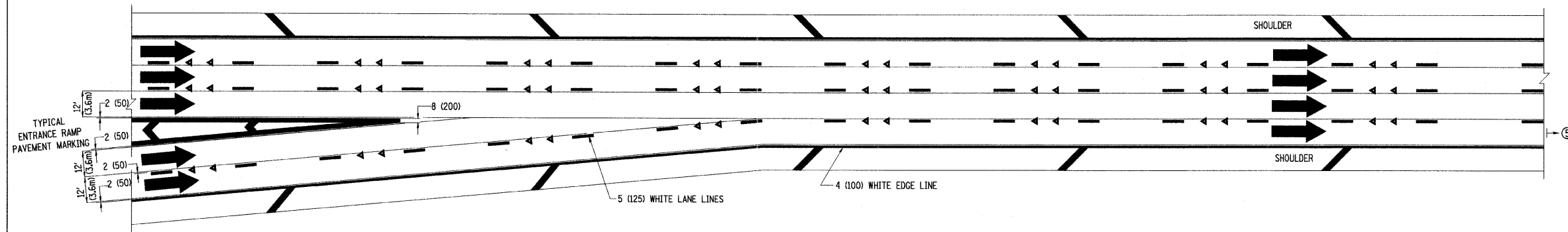
LANE REDUCTION PAVEMENT MARKINGS



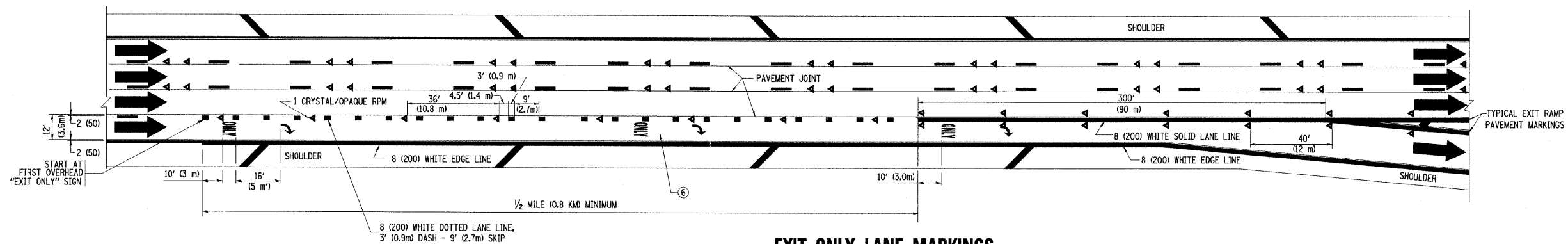
- NOTES:**
- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
 - ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.



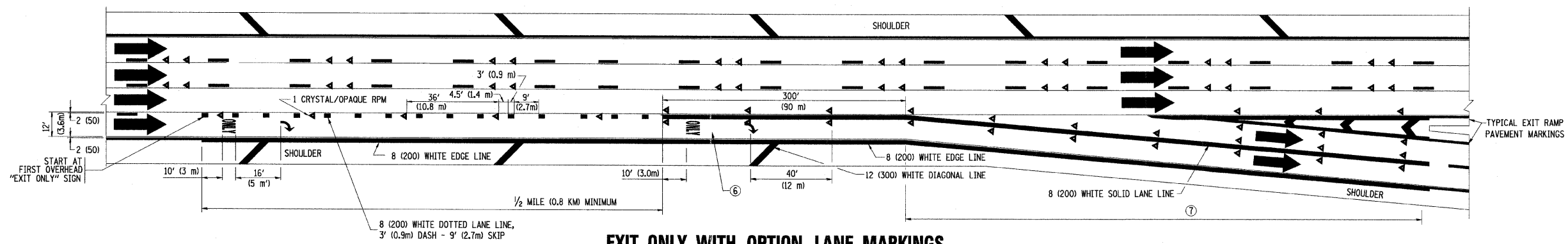
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

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USER NAME = loyso
 PLOT SCALE = 50,000 "/>

DESIGNED - D.W.S.
 DRAWN -
 CHECKED -
 DATE - 01-90

REVISED - D.W.S. 07-96
 REVISED - J.A.F. 02-06
 REVISED - S.P.B. 01-07
 REVISED - S.P.B. 01-10

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**MULTI-LANE FREEWAY
 PAVEMENT MARKING DETAILS**

SCALE: NONE SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2011-031-BR	COOK	41	41
TC-12		CONTRACT NO. 60P37		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				