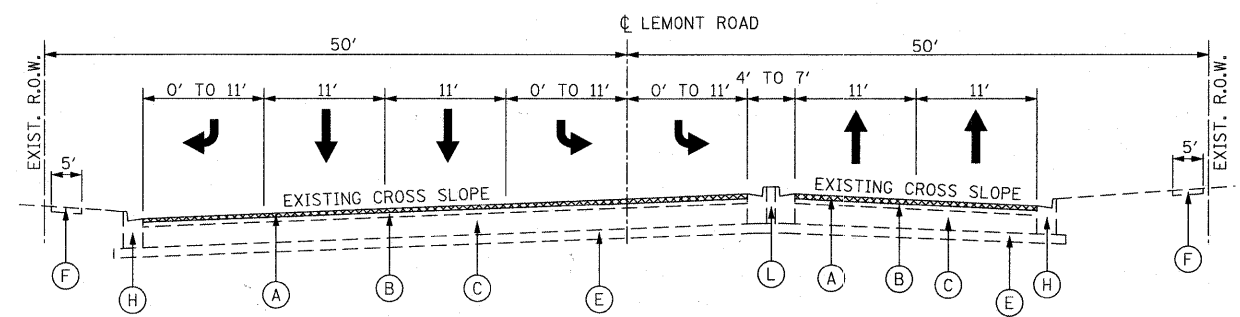
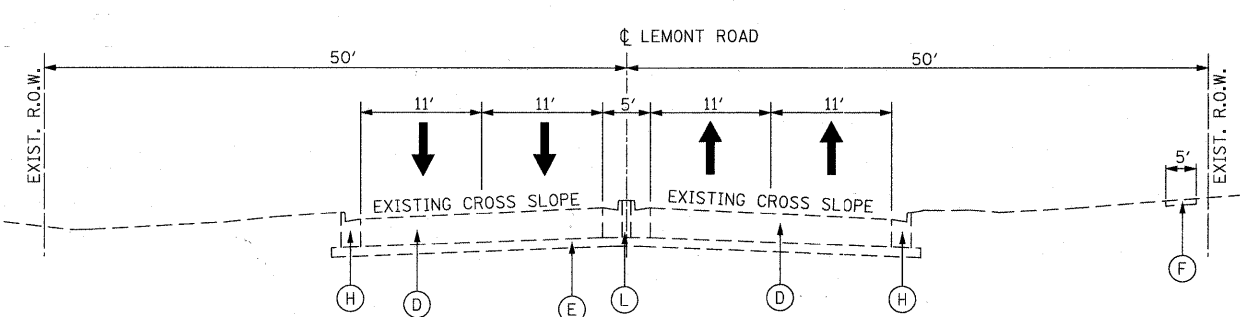


EXISTING TYPICAL SECTION  
LEMONT ROAD  
STA. 14+08 TO STA. 19+31



EXISTING TYPICAL SECTION  
LEMONT ROAD  
STA. 20+59 TO STA. 24+51.30



EXISTING TYPICAL SECTION  
LEMONT ROAD  
STA. 24+51.30 TO STA. 25+00

PAVEMENT DESIGN

ITEM	LEMONT ROAD	
	FLEXIBLE	COMPOSITE
DESIGN TYPE	FLEXIBLE	COMPOSITE
STRUCTURAL DESIGN TRAFFIC (2020)	35,199	35,199
ROAD CLASSIFICATION	I	I
PASSENGER CARS	32,735	32,735
SINGLE UNITS	2,112	2,112
MULTIPLE UNITS	352	352
TRAFFIC FACTOR	4.08	4.97
DESIGN THICKNESS	1.75+2.25+9.0	1.75+2.25+9.0

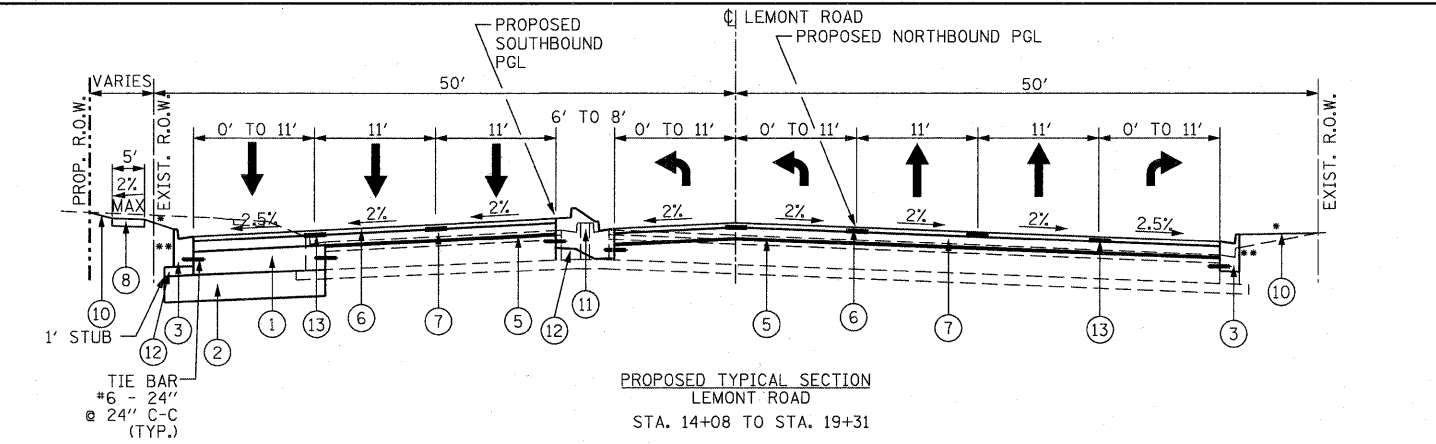
THICKNESS = (HMA SURF + HMA BINDER + PCC BASE COURSE)

EXISTING

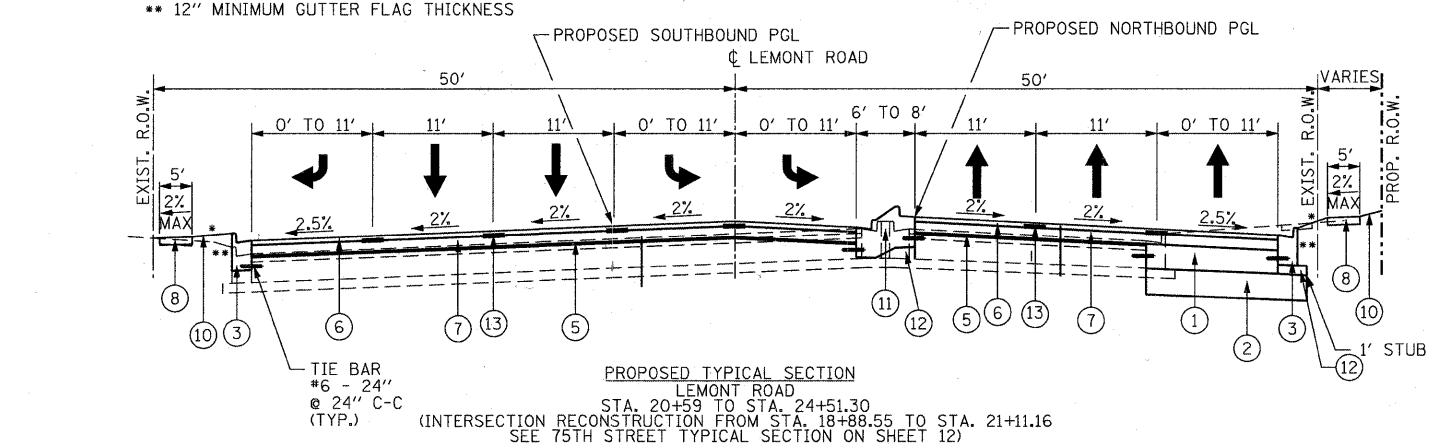
- (A) 1 1/2" BITUMINOUS CONCRETE SURFACE, CLASS I
  - (B) 1 1/2" BITUMINOUS CONCRETE BINDER
  - (C) 8" P.C.C. BASE COURSE/PAVEMENT
  - (D) 10" P.C.C. BASE COURSE/PAVEMENT
  - (E) 4" AGGREGATE SUB-BASE
  - (F) P.C.C. SIDEWALK 5"
  - (G) SWALE/DITCH
  - (H) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.18
  - (I) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
  - (J) BITUMINOUS SHOULDER
  - (K) LANDSCAPED MEDIAN
  - (L) SB-9.12 CONCRETE CURB & GUTTER BARRIER MEDIAN
- HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"

PROPOSED

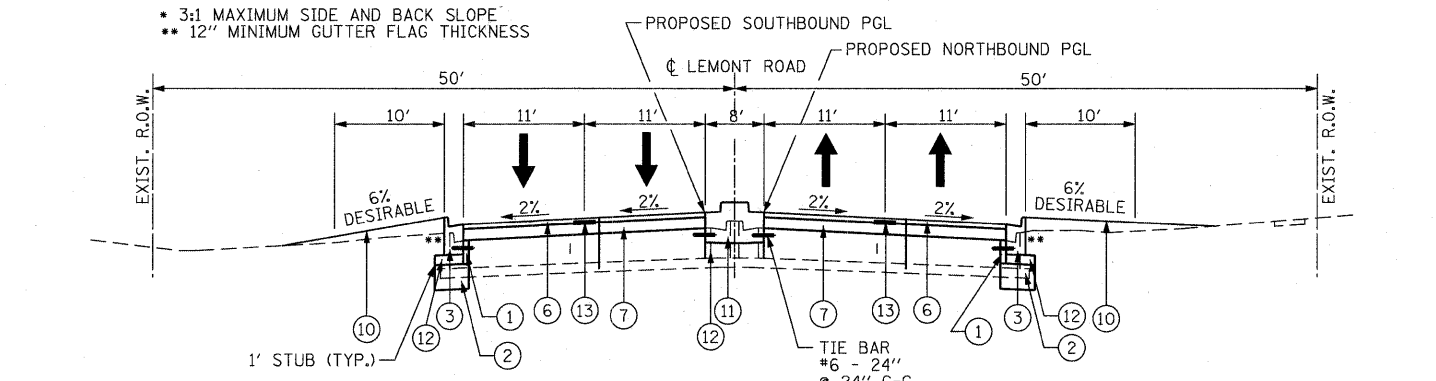
- (1) P.C.C. BASE COURSE, 9"
- (1A) P.C.C. BASE COURSE WIDENING, 9" \*\*\*\*
- (2) AGGREGATE SUBGRADE, 16"
- (3) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 \*\*
- (4) COMBINATION CONCRETE CURB & GUTTER, TYPE B-9.12 \*\*
- (5) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" AND VARIES
- (6) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"
- (7) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 2 1/4"
- (8) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (9) BITUMINOUS BIKE PATH (2" HMA SURFACE COURSE + 4" AGGREGATE BASE COURSE, TYPE B)
- (10) TOPSOIL PLACEMENT 6" AND SODDING (SALT TOLERANT) OR SODDING
- (11) SB-9.12 CONCRETE CURB & GUTTER BARRIER MEDIAN
- (12) SUBBASE GRANULAR MATERIAL, TYPE B, 2"



PROPOSED TYPICAL SECTION  
LEMONT ROAD  
STA. 14+08 TO STA. 19+31



PROPOSED TYPICAL SECTION  
LEMONT ROAD  
STA. 20+59 TO STA. 24+51.30  
(INTERSECTION RECONSTRUCTION FROM STA. 18+88.55 TO STA. 21+11.16  
SEE 75TH STREET TYPICAL SECTION ON SHEET 12)



PROPOSED TYPICAL SECTION  
LEMONT ROAD  
STA. 24+51.30 TO STA. 25+00

• 3:1 MAXIMUM SIDE AND BACK SLOPE  
• 12" MINIMUM GUTTER FLAG THICKNESS  
• \*\*\*\* WHEN WIDENING WIDTH IS < 6'  
NOTE: ALL CURB AND GUTTER ADJACENT TO PAVEMENT SLOPING AWAY FROM THE CURB AND GUTTER SHALL MATCH THE PAVEMENT SLOPE AND FLOW DIRECTION.

NOTE: DOWEL BARS ARE TO BE DRILLED AND GROUTED. THIS IS TO BE INCLUDED IN THE CONTRACT UNIT PRICE FOR PCC BASE COURSE 9", PCC BASE COURSE WIDENING 9", CURB TYPE B, OR COMBINATION GURB AND GUTTER.

FILE NAME = F:\451-030 75th st duppage det\engr\drawing\451-030-ah1-typical.dgn



USER NAME = gellwanger	DESIGNED - GJE	REVISED -
PLOT SCALE = 50.0000' / IN.	DRAWN - DC	REVISED -
PLOT DATE = 9/7/2011	CHECKED - JP	REVISED -
	DATE - 9/08/11	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXISTING & PROPOSED TYPICAL SECTIONS  
LEMONT ROAD

SCALE: NONE SHEET NO. 13 OF 198 SHEETS STA. TO STA.

F.A.P. RTE. 0369	SECTION 07-00232-04-WR	COUNTY DuPAGE	TOTAL SHEETS 198	SHEET NO. 13
CONTRACT NO. 63632				ILLINOIS FED. AID PROJECT