

CITY OF AURORA AURORA, ILLINOIS

CONSTRUCTION PLANS FOR AURORA MUNICIPAL AIRPORT

AIRFIELD PAVEMENT REHABILITATION - PHASE 4

ILLINOIS PROJECT: ARR-4785
 S.B.G. PROJECT: 3-17-SBGP-162/171

NOVEMBER 19, 2021
 REVISED: SEPTEMBER 23, 2022

811 Know what's below.
 Call before you dig.

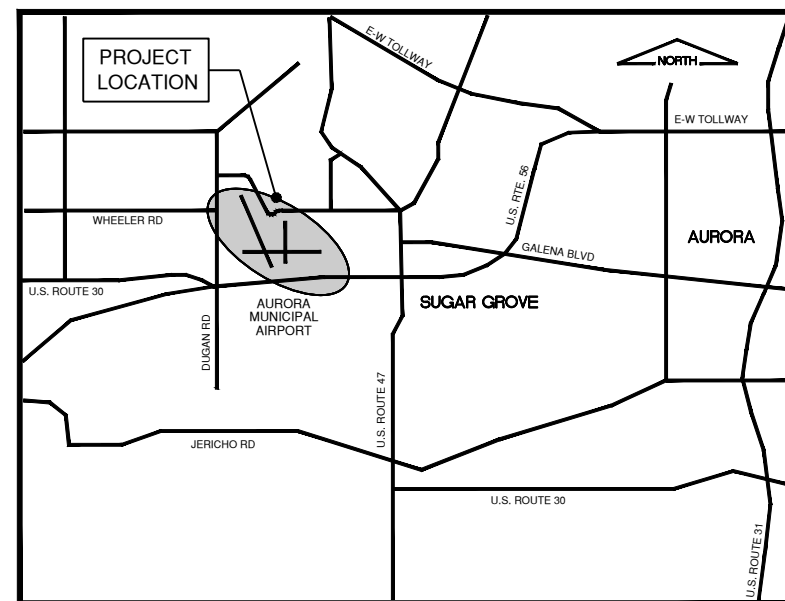
J.U.L.I.E.
 JOINT UTILITY LOCATING
 INFORMATION FOR EXCAVATORS
 www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE. SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS. OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

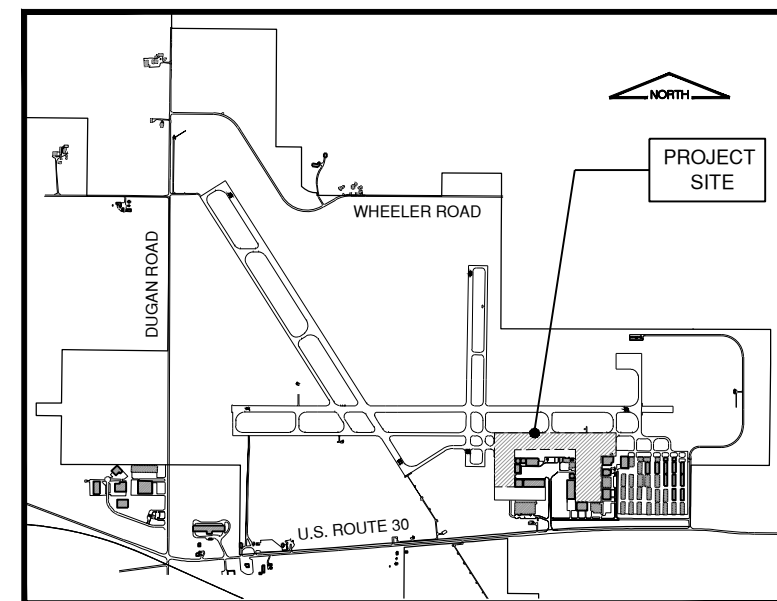
CALL J.U.L.I.E BEFORE EXCAVATING
 AT 811

AURORA MUNICIPAL AIRPORT

SECTION: 17 AND 18 COUNTY: KANE
 RANGE: 7 EAST U.S. ROUTE 30
 TOWNSHIP: 38 NORTH SUGAR GROVE TOWNSHIP



LOCATION MAP



SITE PLAN

AURORA ILLINOIS CITY OF LIGHTS
 AURORA MUNICIPAL AIRPORT

CITY OF AURORA, ILLINOIS

APPROVED BY *[Signature]* MAYOR
 RICHARD C. IRVIN

DATE 2021

APPROVED BY *[Signature]* AIRPORT MANAGER
 STEPHEN K. ANDRAS, P.E.

DATE 9-8-21 2021

CMT 200285-02
 CRAWFORD MURPHY & TILLY, INC.
 CONSULTING ENGINEERS

LICENSED PROFESSIONAL ENGINEER
 ANTONIO R. MARIN
 062-057994
 STATE OF ILLINOIS
 EXPIRES 11/30/2021

SUBMITTED BY *[Signature]*
 ANTONIO R. MARIN, P.E.

DATE 11/19/21



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NOVEMBER 19, 2021
REVISED: SEPTEMBER 23, 2022
AIRFIELD PAVEMENT
REHABILITATION - PHASE 4

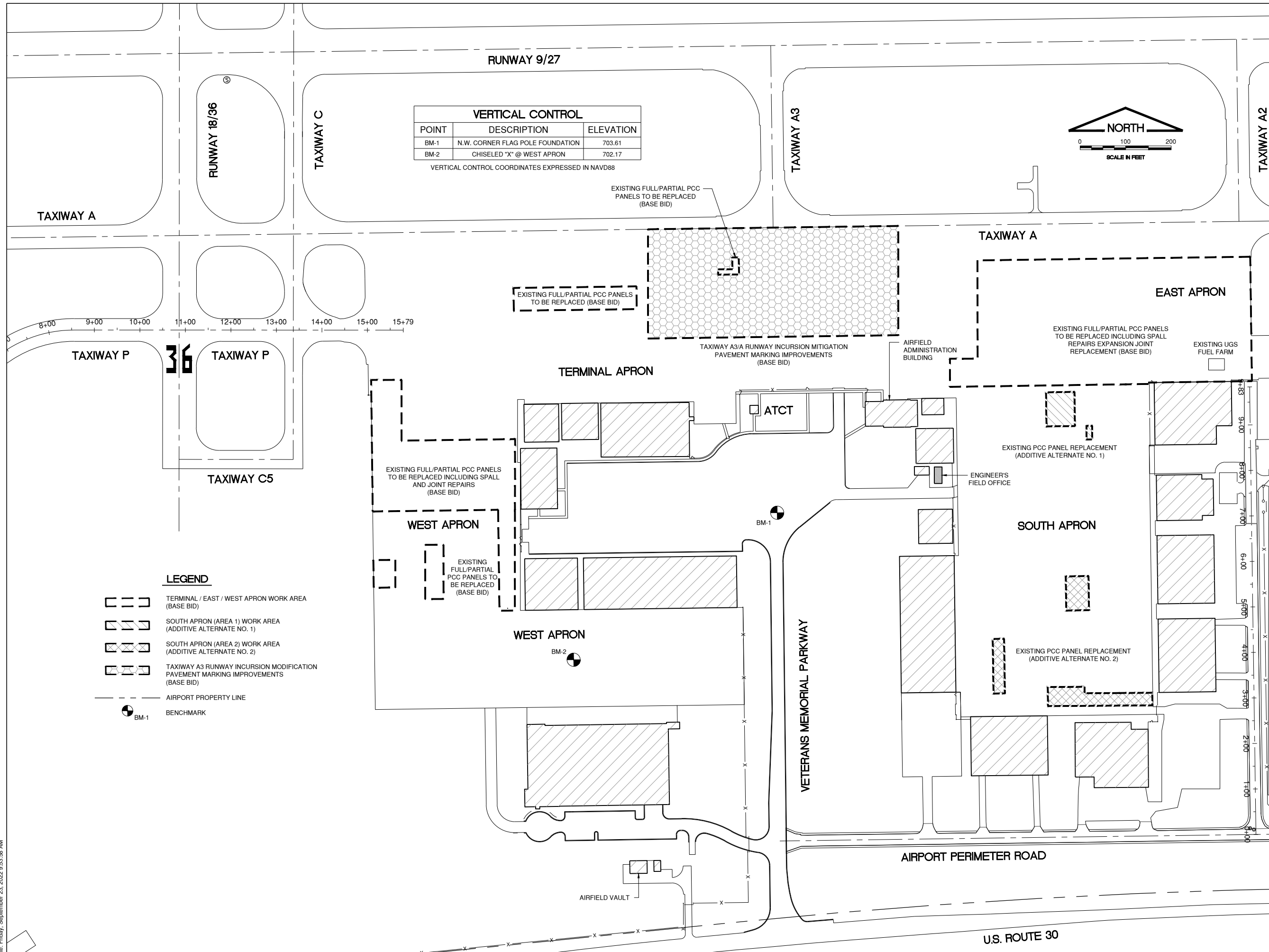


AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS

MARK | DATE | DESCRIPTION

AIP PROJ. NO. 3-17-SBGP-162/TBD
IL PROJ. NO. ARR-4785
CMT PROJECT NO: 200285-02
CAD DWG FILE: 20028502 - SITE PLAN.DWG
DESIGNED BY: ARM
DRAWN BY: JRO
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SHEET TITLE
**SITE PLAN AND
PROJECT CONTROL
PLAN**



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AIRFIELD PAVEMENT
REHABILITATION - PHASE 4



AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS

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SHEET TITLE
**SEQUENCE OF
CONSTRUCTION -
PHASE 1**

**PHASE 1
SUGGESTED SEQUENCE OF
CONSTRUCTION**

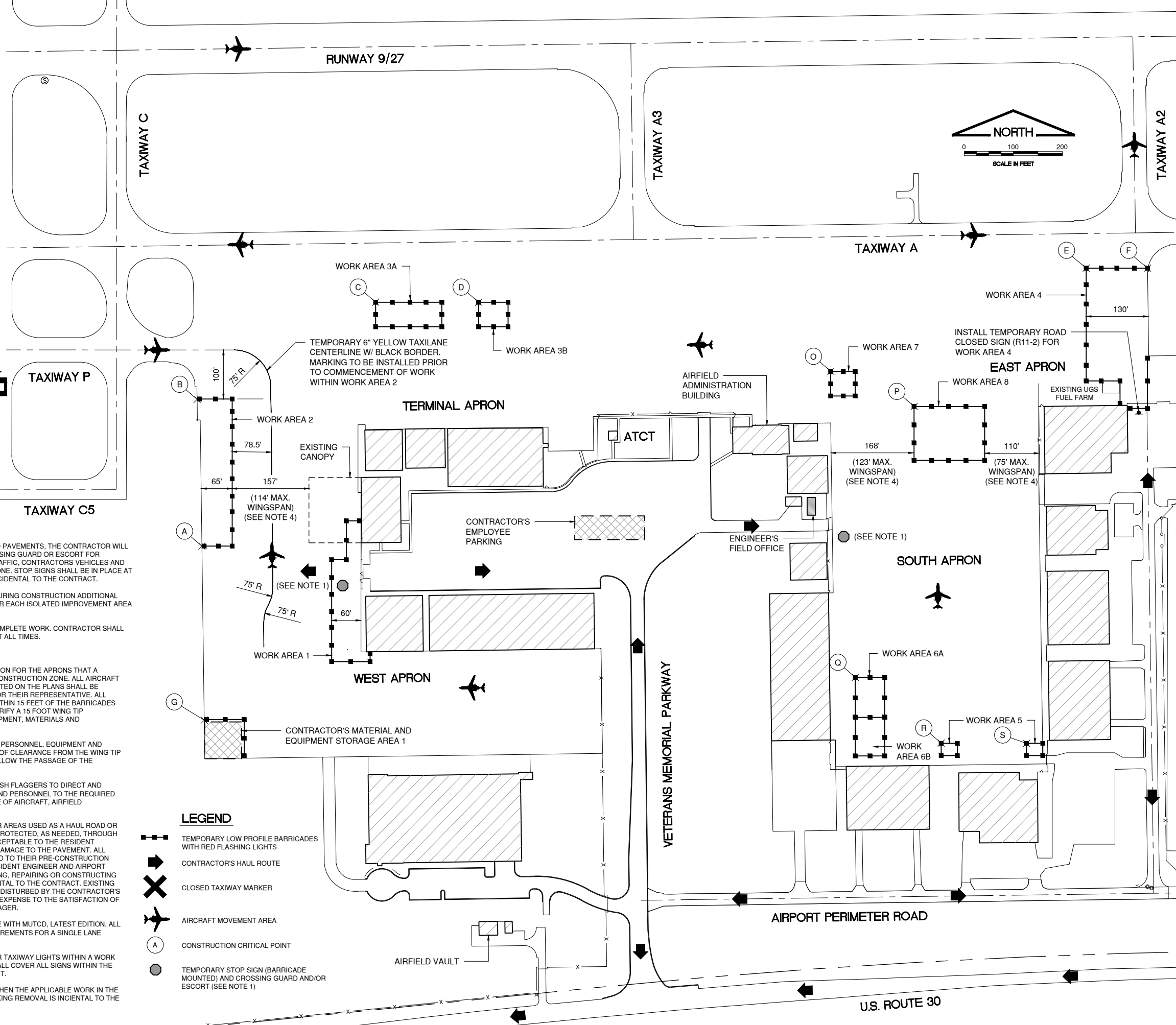
- NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER 10 DAYS PRIOR TO THE START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
- WORK AREA
 - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.
 - PLACE REQUIRED BARRICADES AND PAVEMENT CLOSURE MARKERS (SEE NOTE 4).
 - COMPLETE PAVEMENT REMOVALS.
 - COMPLETE UTILITY ADJUSTMENTS.
 - COMPLETE PAVEMENT IMPROVEMENTS.
 - CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
 - ONCE PCC PAVEMENTS OBTAIN THE REQUIRED CURE TIME/STRENGTH THEY MAY BE OPENED TO TRAFFIC.
 - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER PAVEMENT CLOSURES FOR NEXT WORK AREA.
 - RELOCATE BARRICADES FOR NEXT WORK AREA.

NOTES

1. WHEN HAUL ROUTE IS IN USE ON ACTIVE AIRFIELD PAVEMENTS, THE CONTRACTOR WILL BE REQUIRED TO BE UNDER CONTROL BY A CROSSING GUARD OR ESCORT FOR CONSTRUCTION PERSONNEL, CONSTRUCTION TRAFFIC, CONTRACTORS VEHICLES AND EQUIPMENT CROSSING BY, TO OR FROM WORK ZONE. STOP SIGNS SHALL BE IN PLACE AT ALL TIMES IN THIS AREA. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.
2. WORK AREAS WILL REMAIN OPEN TO AIRCRAFT DURING CONSTRUCTION ADDITIONAL BARRICADES MAY BE REQUIRED THAN SHOWN FOR EACH ISOLATED IMPROVEMENT AREA WITHIN THE WORK AREA.
3. CONTRACTOR MAY COMBINE WORK AREAS TO COMPLETE WORK. CONTRACTOR SHALL MAINTAIN ACCESS TO HALF THE HANGAR DOOR AT ALL TIMES.
4. APRON: AIRCRAFT RESTRICTIONS
 - a. AIRPORT WILL ISSUE NOTAM OF CONSTRUCTION FOR THE APRONS THAT A REDUCED WING TIP CLEARANCE NEAR THE CONSTRUCTION ZONE. ALL AIRCRAFT WHOSE WING SPANS ARE GREATER THAN NOTED ON THE PLANS SHALL BE TOWED THROUGH THE SITE BY THE OWNER OR THEIR REPRESENTATIVE. ALL OVERSIZED AIRCRAFT WHOSE WINGS ARE WITHIN 15 FEET OF THE BARRICADES SHALL ALSO PROVIDE A WING WALKER TO VERIFY A 15 FOOT WING TIP CLEARANCE FROM ALL CONSTRUCTION EQUIPMENT, MATERIALS AND PERSONNEL.
 - b. CONTRACTOR SHALL BE REQUIRED TO MOVE PERSONNEL, EQUIPMENT AND MATERIALS TO PROVIDE A MINIMUM 15-FOOT OF CLEARANCE FROM THE WING TIP OR ANY OTHER PART OF THE AIRCRAFT TO ALLOW THE PASSAGE OF THE AIRCRAFT.
 - c. CONTRACTOR SHALL BE REQUIRED TO FURNISH FLAGGERS TO DIRECT AND CONTROL ALL CONSTRUCTION EQUIPMENT AND PERSONNEL TO THE REQUIRED SET BACK DISTANCE FOR THE SAFE PASSAGE OF AIRCRAFT, AIRFIELD PERSONNEL AND/OR AIRPORT PERSONNEL.
5. ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE PROTECTED, AS NEEDED, THROUGH THE USE OF STEEL PLATES OR OTHER MEANS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT IN ORDER TO PREVENT DAMAGE TO THE PAVEMENT. ALL DAMAGED PAVEMENT SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF PROTECTING, MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
6. ALL ROADWAY SIGNAGE SHALL BE IN COMPLIANCE WITH MUTCD, LATEST EDITION. ALL SIGNS SHALL AT A MINIMUM MEET THE SIZE REQUIREMENTS FOR A SINGLE LANE CONVENTIONAL ROAD.
7. CONTRACTOR SHALL INSTALL A PVC SLEEVE OVER TAXIWAY LIGHTS WITHIN A WORK AREA OR CLOSED PAVEMENTS. CONTRACTOR SHALL COVER ALL SIGNS WITHIN THE WORK AREA LIMITS. COST INCIDENTAL TO PROJECT.
8. ALL TEMPORARY MARKING SHALL BE REMOVED WHEN THE APPLICABLE WORK IN THE AREA IS COMPLETED. COST OF TEMPORARY MARKING REMOVAL IS INCIDENTAL TO THE TEMPORARY MARKING PAY ITEM.

LEGEND

- TEMPORARY LOW PROFILE BARRICADES WITH RED FLASHING LIGHTS
- CONTRACTOR'S HAUL ROUTE
- CLOSED TAXIWAY MARKER
- AIRCRAFT MOVEMENT AREA
- CONSTRUCTION CRITICAL POINT
- TEMPORARY STOP SIGN (BARRICADE MOUNTED) AND CROSSING GUARD AND/OR ESCORT (SEE NOTE 1)



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NOVEMBER 19, 2021
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AIRFIELD PAVEMENT
REHABILITATION - PHASE 4



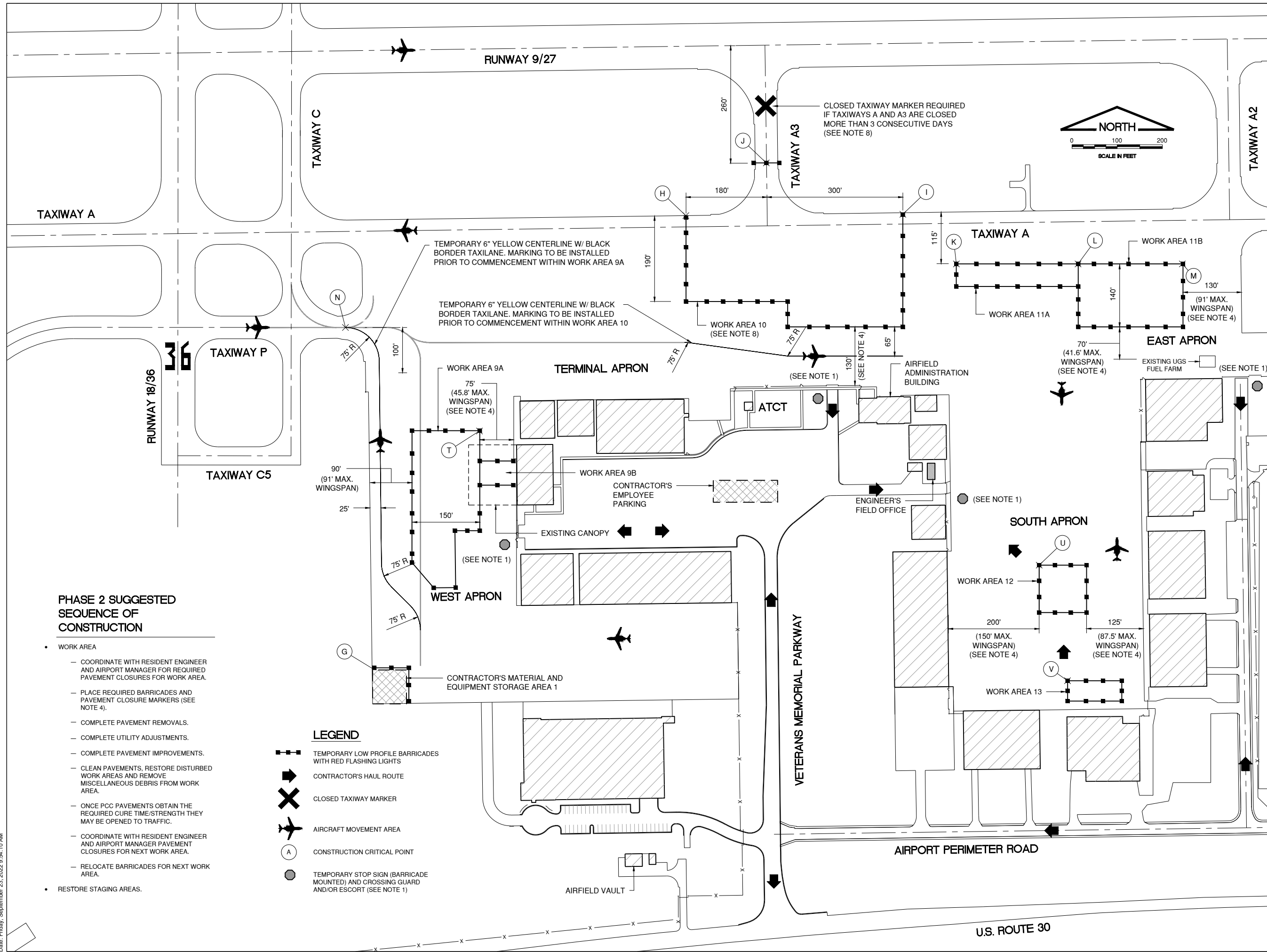
AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS

MARK | DATE | DESCRIPTION

AIP PROJ. NO. 3-17-SBGP-162/TBD
IL PROJ. NO. ARR-4785
CMT PROJECT NO: 200285-02
CAD DWG FILE: 20028502 - SEQUENCE - 2.DWG
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APPROVED BY: ARM

SHEET TITLE

SEQUENCE OF
CONSTRUCTION -
PHASE 2



**PHASE 2 SUGGESTED
SEQUENCE OF
CONSTRUCTION**

WORK AREA

- COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.
- PLACE REQUIRED BARRICADES AND PAVEMENT CLOSURE MARKERS (SEE NOTE 4).
- COMPLETE PAVEMENT REMOVALS.
- COMPLETE UTILITY ADJUSTMENTS.
- COMPLETE PAVEMENT IMPROVEMENTS.
- CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
- ONCE PCC PAVEMENTS OBTAIN THE REQUIRED CURE TIME/STRENGTH THEY MAY BE OPENED TO TRAFFIC.
- COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER PAVEMENT CLOSURES FOR NEXT WORK AREA.
- RELOCATE BARRICADES FOR NEXT WORK AREA.

RESTORE STAGING AREAS.

LEGEND

- TEMPORARY LOW PROFILE BARRICADES WITH RED FLASHING LIGHTS
- CONTRACTOR'S HAUL ROUTE
- CLOSED TAXIWAY MARKER
- AIRCRAFT MOVEMENT AREA
- CONSTRUCTION CRITICAL POINT
- TEMPORARY STOP SIGN (BARRICADE MOUNTED) AND CROSSING GUARD AND/OR ESCORT (SEE NOTE 1)

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AIRFIELD PAVEMENT
REHABILITATION - PHASE 4



AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS

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		IL PROJ. NO. ARR-4785
		CMT PROJECT NO: 200285-02
		CAD DWG FILE: 20028502 - PROPOSED IMPROVEMENTS -
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SHEET TITLE
**PROPOSED
IMPROVEMENTS - 1**

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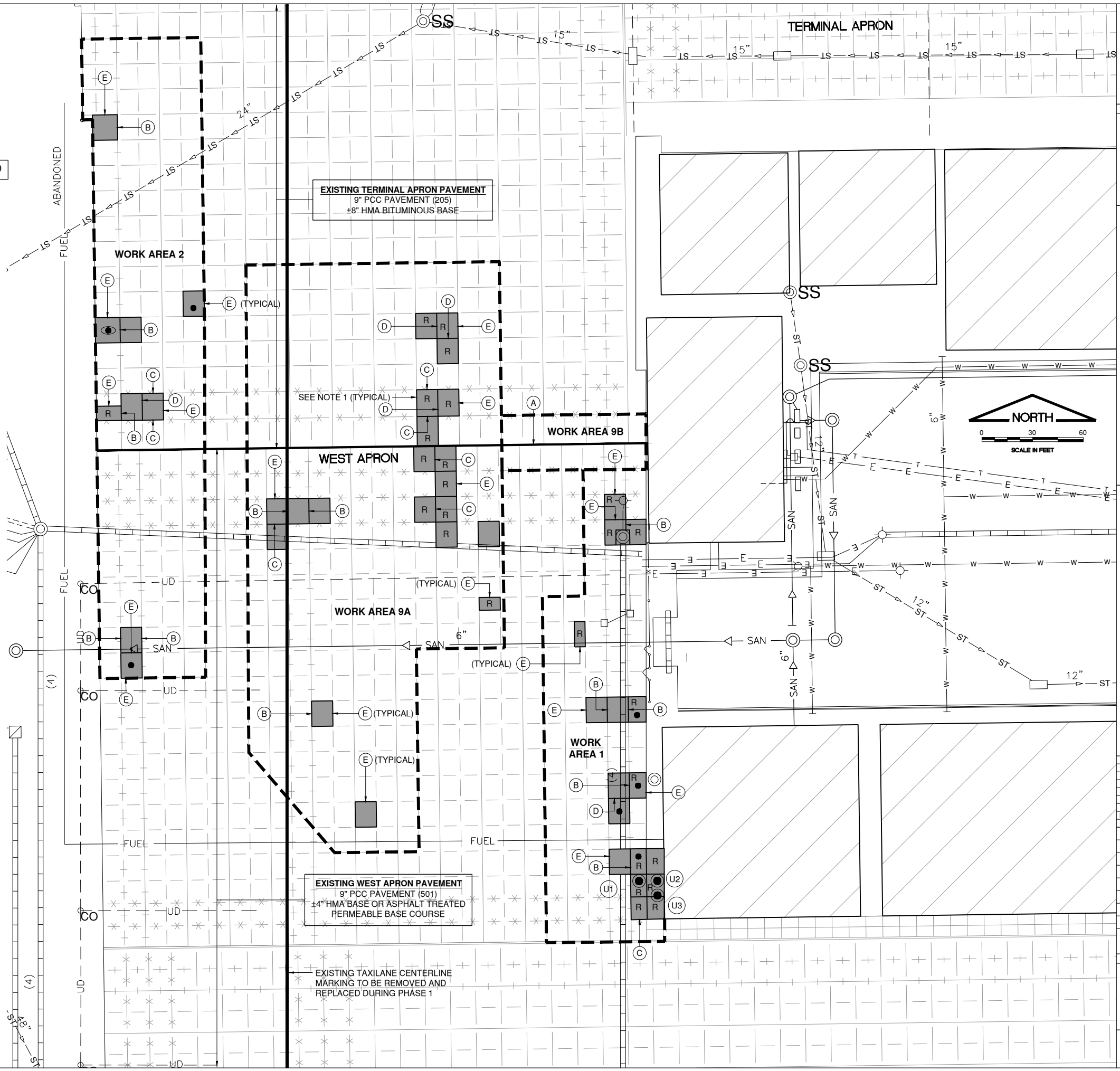
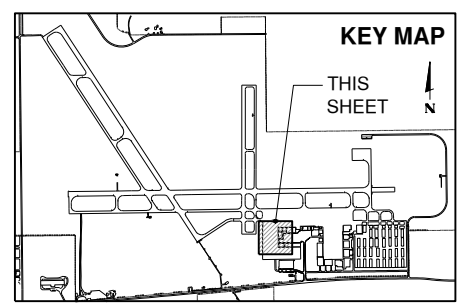
SEE SHEET 2 FOR EXISTING CONDITIONS LEGEND

LEGEND

- UTILITY TO BE RECONSTRUCTED
- NEW TIE DOWN (AR/AS/ATS10510)
- NEW TYPE B - HINGED CONTRACTION JOINT
- NEW TYPE C - DOWELED CONTRACTION JOINT
- NEW TYPE D DUMMY CONTRACTION JOINT
- NEW TYPE E - DOWELED CONTRACTION JOINT
- R REINFORCED PANEL
- (A) WIDEN, CLEAN AND SEAL ISOLATION JOINT
- EXISTING PANEL TO BE REMOVED AND REPLACED (BASE BID)
- ▨ EXISTING PANEL TO BE REMOVED AND REPLACED (ADDITIVE ALTERNATE NO. 1)
- ▩ EXISTING PANEL TO BE REMOVED AND REPLACED (ADDITIVE ALTERNATE NO. 2)
- ▧ EXISTING PANEL TO BE REMOVED AND REPLACED HIGH EARLY MIX (ADDITIVE ALTERNATE NO. 2)
- - - - - WORK AREA LIMITS
- GROUND ROD

NOTES

1. INSTALL ISOLATION JOINT AT CANOPY COLUMNS.
2. PRIOR TO REMOVAL OF PCC PANELS, CONTRACTOR SHALL RECON THE LOCATIONS AND NUMBER OF TIE DOWNS AND GROUND RODS TO BE REPLACED.
3. TYPICAL AVERAGE JOINT SPACING FOR PCC PANELS IS 12.5' X 15'.



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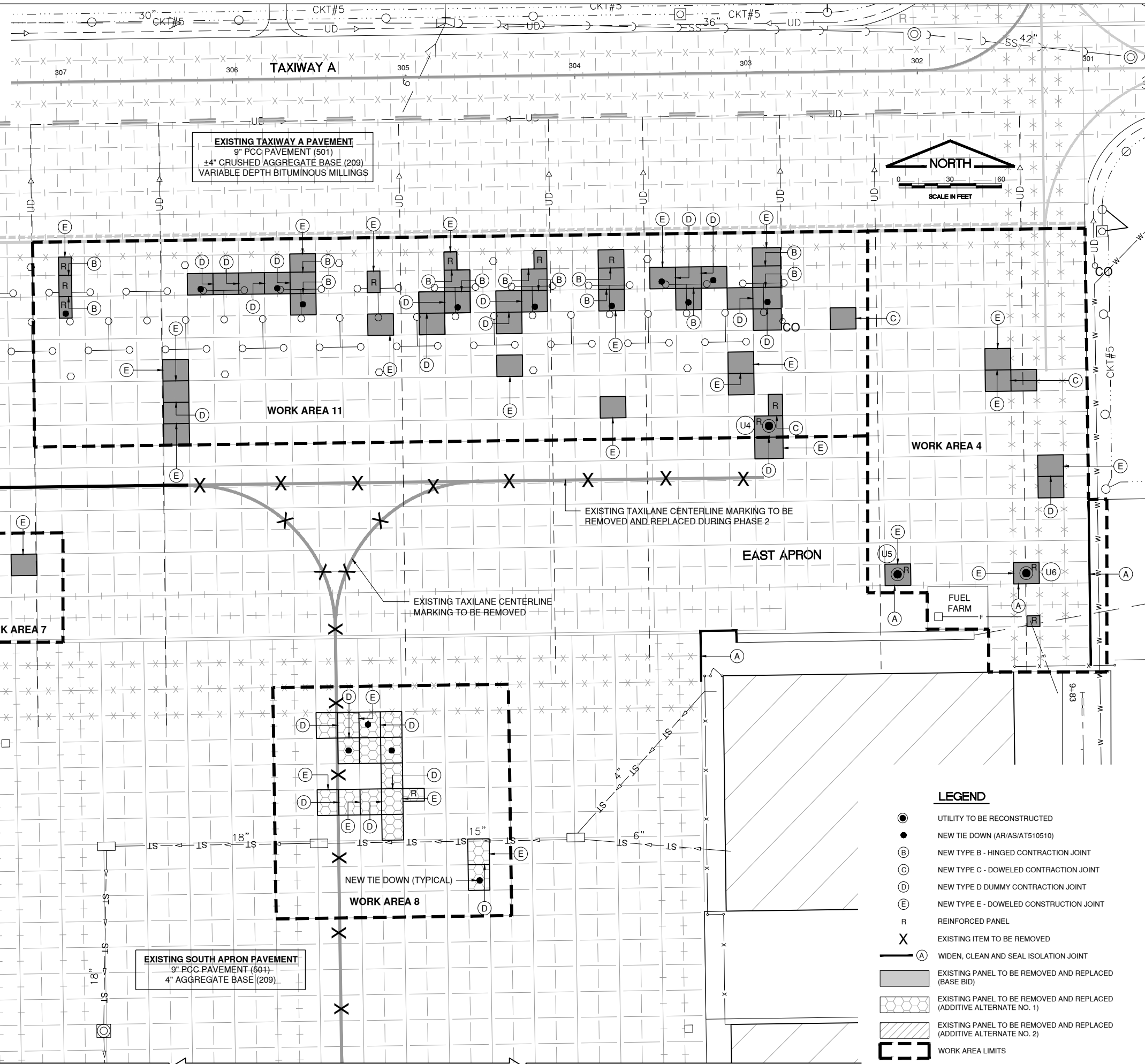
NOVEMBER 19, 2021
REVISED: SEPTEMBER 23, 2022
AIRFIELD PAVEMENT
REHABILITATION - PHASE 4

OWNER
AURORA ILLINOIS
CITY OF LIGHTS



AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS

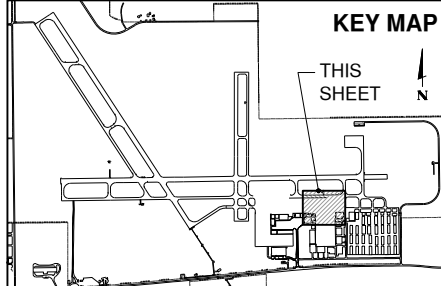
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MATCHLINE - SEE SHEET 9 FOR CONTINUATION

MATCHLINE - SEE SHEET 11 FOR CONTINUATION

- LEGEND**
- UTILITY TO BE RECONSTRUCTED
 - NEW TIE DOWN (AR/AS/AT510510)
 - (B) NEW TYPE B - HINGED CONTRACTION JOINT
 - (C) NEW TYPE C - DOWELED CONTRACTION JOINT
 - (D) NEW TYPE D DUMMY CONTRACTION JOINT
 - (E) NEW TYPE E - DOWELED CONTRACTION JOINT
 - R REINFORCED PANEL
 - X EXISTING ITEM TO BE REMOVED
 - (A) WIDEN, CLEAN AND SEAL ISOLATION JOINT
 - EXISTING PANEL TO BE REMOVED AND REPLACED (BASE BID)
 - EXISTING PANEL TO BE REMOVED AND REPLACED (ADDITIVE ALTERNATE NO. 1)
 - EXISTING PANEL TO BE REMOVED AND REPLACED (ADDITIVE ALTERNATE NO. 2)
 - WORK AREA LIMITS



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MARK	DATE	DESCRIPTION
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IL PROJ. NO. ARR-4785		
CMT PROJECT NO: 200285-02		
CAD DWG FILE: 20028502 - PROPOSED IMPROVEMENTS - 3.DWG		
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SHEET TITLE
PROPOSED IMPROVEMENTS - 3
SHEET 10 OF 15



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AIRFIELD PAVEMENT
REHABILITATION - PHASE 4

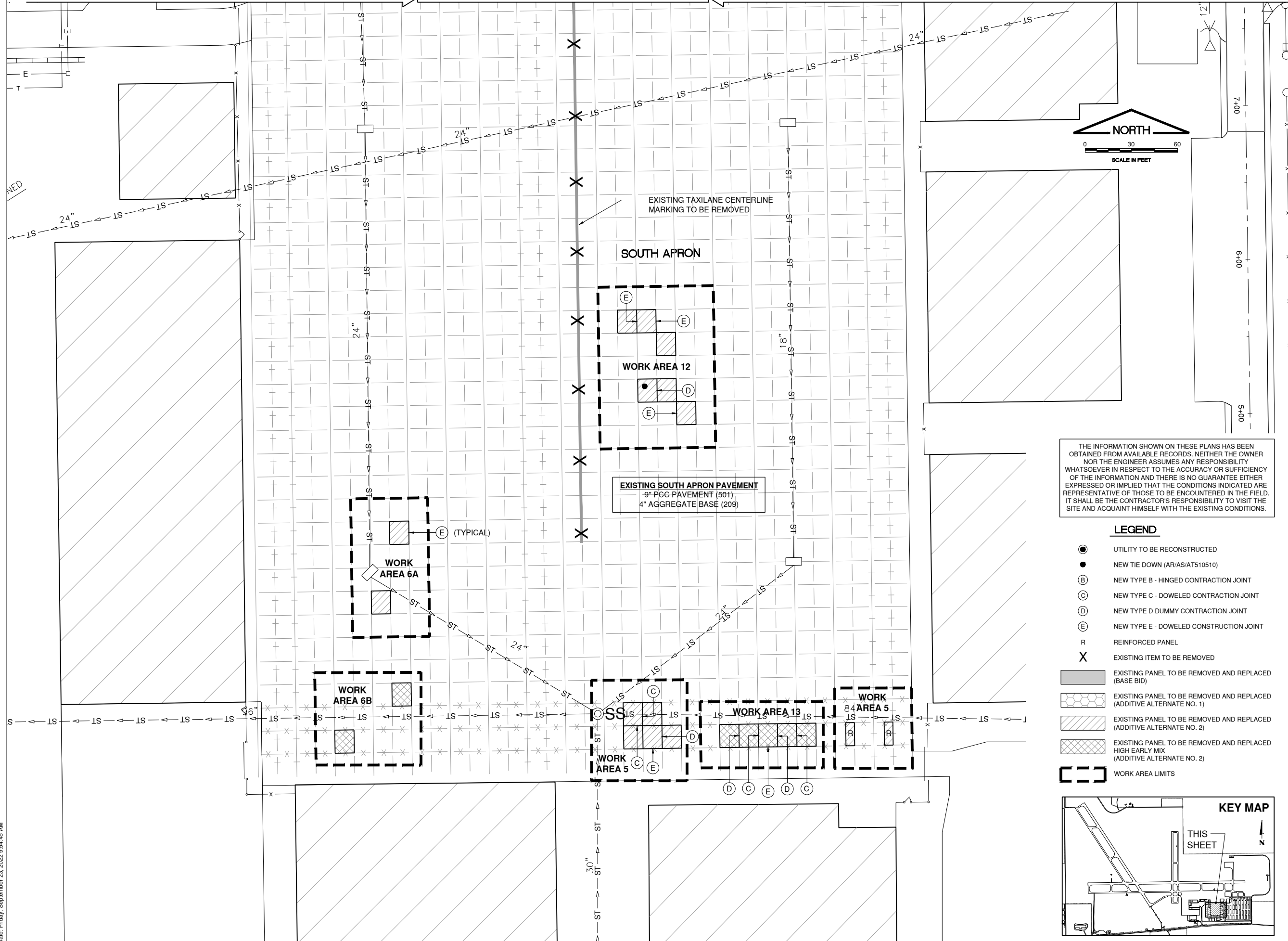


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IL PROJ. NO.	ARR-4785	
CMT PROJECT NO.	200285-02	
CAD DWG FILE:	20028502 - PROPOSED IMPROVEMENTS - 4.DWG	
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PROPOSED IMPROVEMENTS - 4

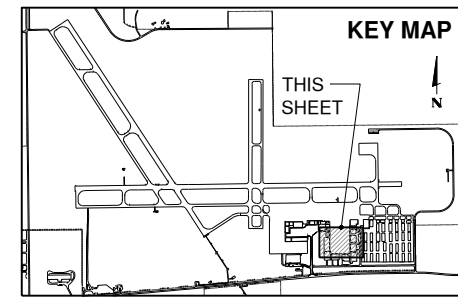
MATCHLINE - SEE SHEET 11 FOR CONTINUATION



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LEGEND

- UTILITY TO BE RECONSTRUCTED
- NEW TIE DOWN (AR/AS/AT510510)
- (B) NEW TYPE B - HINGED CONTRACTION JOINT
- (C) NEW TYPE C - DOWELED CONTRACTION JOINT
- (D) NEW TYPE D DUMMY CONTRACTION JOINT
- (E) NEW TYPE E - DOWELED CONTRACTION JOINT
- R REINFORCED PANEL
- X EXISTING ITEM TO BE REMOVED
- EXISTING PANEL TO BE REMOVED AND REPLACED (BASE BID)
- EXISTING PANEL TO BE REMOVED AND REPLACED (ADDITIVE ALTERNATE NO. 1)
- EXISTING PANEL TO BE REMOVED AND REPLACED (ADDITIVE ALTERNATE NO. 2)
- EXISTING PANEL TO BE REMOVED AND REPLACED HIGH EARLY MIX (ADDITIVE ALTERNATE NO. 2)
- WORK AREA LIMITS



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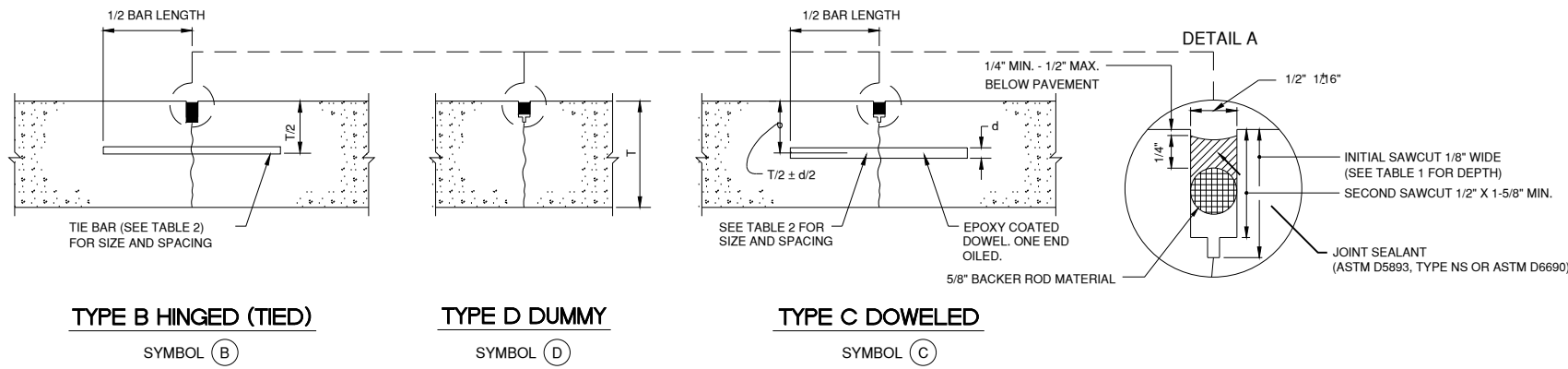
AIRFIELD PAVEMENT
REHABILITATION - PHASE 4



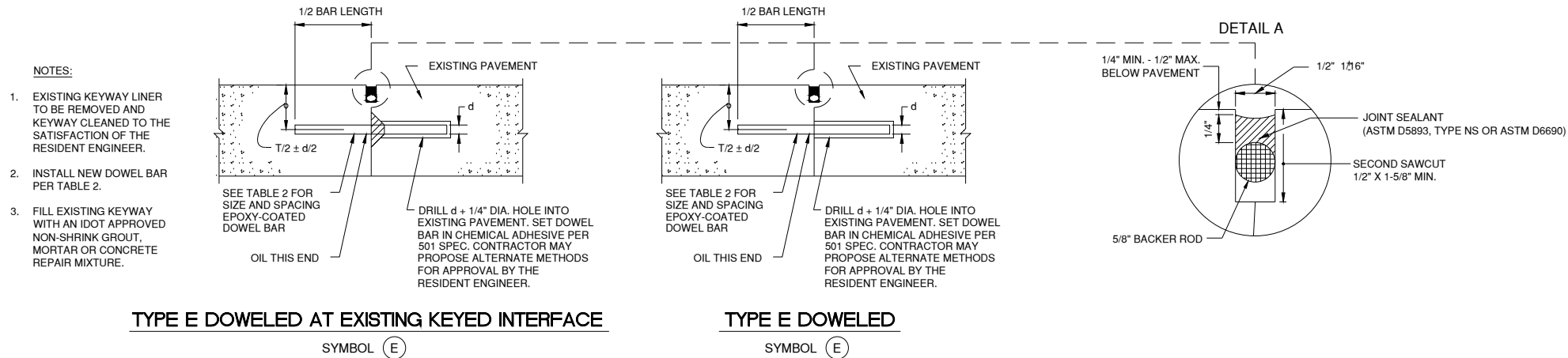
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AURORA, ILLINOIS

JOINT NOTES

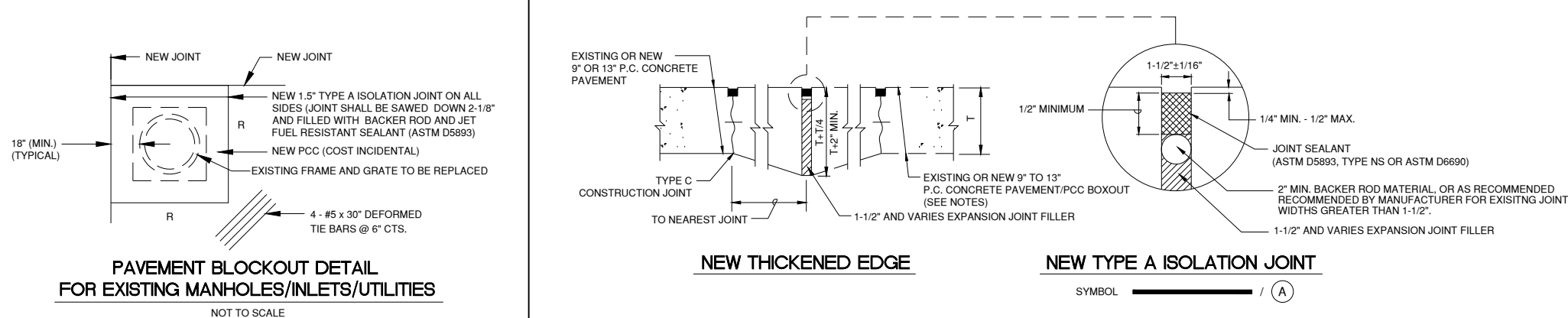
- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THE THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY, WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
- ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR PLACED BY OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH ASTM A615 OR ASTM A616, EXCEPT THAT RAIL STEEL BARS, GRADE 50 OR 60 SHALL NOT BE USED FOR THE BARS THAT ARE TO BE BENT OR RESTRAIGHTEND DURING CONSTRUCTION. TIE BARS DESIGNATED AS GRADE 40 IN ASTM A615 CAN BE USED FOR CONSTRUCTION REGARDING BENT BARS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSION OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.
- COST OF ALL JOINT SAWING, CLEANING AND SEALING OF NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
- SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
- DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.



CONTRACTION JOINTS



CONSTRUCTION JOINTS



- ABOVE BLOCKOUT SHALL BE CONSTRUCTED FOR EXISTING MANHOLE/ INLET/UTILITIES CONSTRUCTION. DIMENSION OF BOXOUT MAY VARY TO FIT FIELD CONDITIONS.
- R - DENOTES ODD SHAPED REINFORCED PANELS TO BE REINFORCED WITH WIRE FABRIC AS SHOWN ON THIS SHEET. ALL NON RECTANGULAR SHAPED PANELS SHALL BE REINFORCED. (REINFORCEMENT NOT SHOWN).
- ALL CONCRETE AND OTHER MATERIALS SHALL BE IN CONFORMANCE WITH THE SPECIFICATION FOR ITEMS 501 AND 605.

NOTES

- THE CONTRACTOR SHALL VISIT THE PROJECT SITE AND ACQUAINT HIMSELF WITH THE REQUIRED WORK. CONTRACTOR SHALL BE FAMILIAR TO THE SITE AND REQUIRED WORK PRIOR TO BID. SITE VISITS SHALL BE COORDINATED WITH THE AIRPORT MANAGER AT (630) 256-3120.
- PRIOR TO ORDERING NEW MATERIAL, CONTRACTOR SHALL FIELD MEASURE FOR JOINTING MATERIAL SIZES AND AMOUNT. MEASUREMENTS SHALL ALSO DETERMINE BACKER ROD SIZE AND QUANTITIES. ALSO PER NEW JOINTS DETAILS, DEPTH OF NEW JOINT SEALANT SHALL BE AS RECOMMENDED BY MANUFACTURER. NO EXTRA COMPENSATION WILL BE MADE FOR EXCESS MATERIALS NOT USED.
- FOR VARYING JOINT WIDTHS AND DEPTHS, THERE SHALL BE NO ADDITIONAL COST FOR DEEPER SAWCUTS, LARGER BACKER RODS AND SEALANT MATERIAL USED.

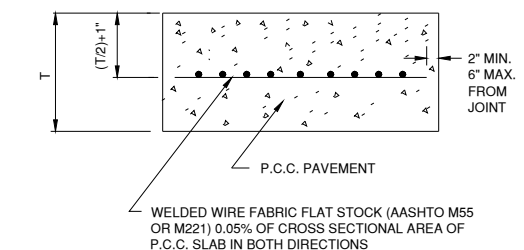
TABLE 1

PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT T, INCHES T=(T/4) ±1/4"
9	2.25"
10	2.50"
11	2.75"
12	3"
13	3.25"

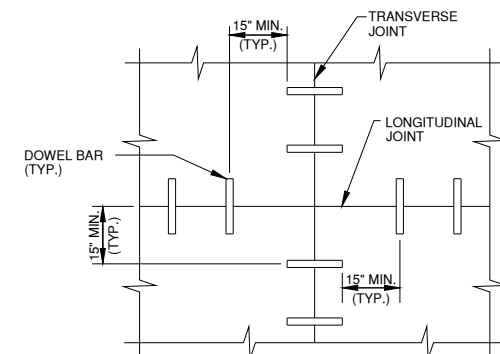
TABLE 2

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA. (d)	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
8' - 12"	1"	19"	12"	#5	30"	30"

DIMENSION TABLES



NOTE: REINFORCEMENT SHALL NOT CROSS ANY JOINT



MARK | DATE | DESCRIPTION

AIP PROJ. NO. 3-17-SBGP-162/TBD
IL PROJ. NO. ARR-4785
CMT PROJECT NO: 200285-02
CAD DWG FILE: 20028502 - JOINTING DETAILS.DWG
DESIGNED BY: ARM
DRAWN BY: JRO
CHECKED BY: DKP
APPROVED BY: ARM
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SHEET TITLE

PAVEMENT JOINTING DETAILS

