

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR HIGHWAY STANDARDS, SEE SHEET NO. 2



TRAFFIC DATA

ADT = 3,300 (2014)
SPEED LIMIT = 25 MPH

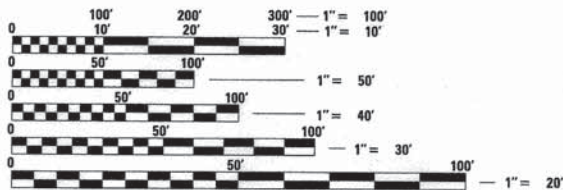
DESIGN DESIGNATION
MAJOR COLLECTOR

PROJECT NUMBER:
F.A.U. ROUTE 210 (LONG LAKE DRIVE)
BEGIN RESURFACING
STA 21+00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PLANS FOR PROPOSED
FEDERAL AID PROJECT**
F.A.U. ROUTE 210 (LONG LAKE DRIVE)
LOTUS DRIVE TO SUNSET DRIVE
RESURFACING
SECTION No: 12-00035-00-RS
PROJECT No: M-9003(940)
VILLAGE OF ROUND LAKE
LAKE COUNTY
JOB No: C-91-216-12

LOCATION MAP
(NOT TO SCALE)
R10E

PROJECT NUMBER:
F.A.U. ROUTE 210 (LONG LAKE DRIVE)
END RESURFACING
STA 37+17



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. DESIGN STAGE REQUEST
DIG. No. X3040314



CONTACT JULIE AT 811 OR 800-892-0123 WITH THE FOLLOWING:

COUNTY = LAKE
CITY-TWNSHP. = ROUND LAKE - AVON
SEC. & 1/4 SEC. NO. = 19NE, 20NW, T45N, R10E
48 HOURS (2 working days) BEFORE YOU DIG

R10E
SECTIONS 19 AND 20, T45N, R10E, OF THE THIRD PRINCIPAL MERIDIAN
AVON TOWNSHIP
GROSS LENGTH OF IMPROVEMENT = 1,617 LF OR 0.306 MILES
NET LENGTH OF IMPROVEMENT = 1,617 LF OR 0.306 MILES



CONTRACT NO. 61A71

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
210	12-00035-00-RS	LAKE	17	1
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED AID PROJECT		
CONTRACT NO. 61A71				



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 4 AUGUST 2014
Ron Brown
VILLAGE OF ROUND LAKE, DIRECTOR OF PUBLIC WORKS

PASSED SEPTEMBER 3, 2014
Christopher Holt
DISTRICT ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW September 3, 2014
John Foraneman Jr.
DEPUTY DIRECTOR OF
HIGHWAYS, REGION 1 ENGINEER

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OF THE STATE OF ILLINOIS



"LICENSE EXPIRES 11-30-2015"

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 8/7/2004 9:52:21 AM
 FEDERAL AID PROGRAM ENGINEER FAWAD AQUEEL, PE PTOE (847-705-4021), SCHAMBOURG, IL

GENERAL NOTES

- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" AND THE "MANUAL OF TEST PROCEDURES FOR MATERIALS".
- THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES, INCLUDING SPRINKLER SYSTEMS, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. PRIOR TO CONSTRUCTION OF ANY PROPOSED UTILITIES, THE CONTRACTOR SHALL EXCAVATE AND LOCATE THE EXISTING UTILITIES TO VERIFY THEIR LOCATION, SIZE, AND DEPTH TO INSURE THAT GRADE CONFLICTS WILL NOT OCCUR. THE COST OF THIS EXPLORATION SHALL BE PAID FOR AS EXPLORATION TRENCH, SPECIAL.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER.
- THE CONTRACTOR SHALL NOTIFY THE VILLAGE DIRECTOR OF PUBLIC WORKS AND THE ENGINEER AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE VILLAGE SUPERINTENDENT OF PUBLIC WORKS AND THE ENGINEER.
- MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, DRIVEWAYS, CURB AND GUTTER, HOT-MIX ASPHALT SURFACES, SIDEWALKS AND EXCAVATION FOR NEW SIDEWALKS ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGMENT OF THE ENGINEER, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE ENGINEER WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR WILL BE BILLED (CHARGED) ACCORDINGLY.
- THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING BAN" IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER IF DEEMED NECESSARY.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE ENGINEER WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE VILLAGE TO RESIDENTS. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED. THE COST OF THIS WORK IS INCLUDED IN COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNERS, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- ALL SIGNS AND MAILBOXES THAT ARE IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED AND REPLACED IN ACCORDANCE WITH VILLAGE STANDARDS AND IN ACCORDANCE WITH ARTICLE 107.20. MAIL SERVICE SHALL BE MAINTAINED AT ALL TIMES.
- EXISTING PAVEMENT, DRIVEWAY PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE COST OF THE APPROPRIATE REMOVAL PAY ITEM.
- IN AREAS WHERE THE EXISTING DRIVEWAY (HOT-MIX ASPHALT OR GRAVEL), SIDEWALK, OR CURB AND GUTTER IS TO BE REMOVED AND REPLACED, THE REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL REQUIRED TO ESTABLISH THE PROPOSED DRIVEWAY, SIDEWALK, OR CURB AND GUTTER SUBGRADE ELEVATION SHALL BE INCLUDED IN THE PAY ITEMS, HMA SURFACE REMOVAL OF THE TYPE SPECIFIED, SIDEWALK REMOVAL OR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- THE PRIME COAT APPLICATION RATE SHALL BE 0.1 GAL/ SY FOR HOT-MIX ASPHALT SURFACES AND 0.3 GAL/ SY FOR AGGREGATE SURFACES.
- ALL AGGREGATE USED ON THIS PROJECT SHALL BE CRUSHED MATERIAL.
- THE DAYS PAVING OPERATION SHALL RESULT IN A SINGLE TRANSVERSE JOINT. COLD LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY PAVING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE PAVING OF THE ADJACENT LANE IN THE SAME DAY.
- THE CONTRACTOR SHALL UTILIZE A MECHANICAL SWEEPER TO CLEAN STREETS AFFECTED BY CONTRACTORS OPERATIONS, INCLUDING HAUL ROUTES, AT LEAST TWICE PER WEEK AND ADDITIONALLY AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF MOBILIZATION.
- CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
- PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 8-INCHES WHERE THE SIDEWALK CROSSES COMMERCIAL DRIVEWAYS. 6" x 6" WELDED WIRE FABRIC SHALL BE INSTALLED IN THE SIDEWALK WHERE THE SIDEWALK CROSSES EITHER PRIVATE OR COMMERCIAL DRIVEWAYS. TRANSVERSE EXPANSION JOINTS 3/4-INCH SHALL BE PLACED EVERY 50 FEET OR AS DETERMINED BY THE ENGINEER. TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED EVERY 5-FEET. THIS WORK IS INCLUDED IN THE COST OF THE APPLICABLE PCC SIDEWALK PAY ITEM.
- THE CONTRACTOR WILL ONLY BE ALLOWED TO REMOVE AND REPLACE CURB AND GUTTER ON ONE SIDE OF THE ROAD AT A TIME TO MINIMIZE CONGESTION. REPLACEMENT MUST BE COMPLETE ON ONE SIDE OF THE ROAD BEFORE THE CONTRACTOR IS ALLOWED TO BEGIN REMOVING CURB AND GUTTER ON THE OTHER SIDE OF THE ROAD.
- THE FINISHED HOT-MIX ASPHALT SURFACE SHALL BE CONSTRUCTED 0.25 INCHES ABOVE THE GUTTER FLAG.
- A 1/2-INCH THICK EXPANSION JOINT SHALL BE PROVIDED AT THE JUNCTION OF THE DRIVEWAY APRON AND CURB, AND AT THE JUNCTION OF THE DRIVEWAY APRON AND THE SIDEWALK. THIS WORK WILL BE INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT.
- THE CONTRACTOR SHALL CONTACT THE LOCAL AGENCY MATERIAL INSPECTOR (SOIL AND MATERIAL CONSULTANTS - THOMAS JOHNSON - 847-870-0544) AT LEAST 48 HOURS PRIOR TO ANY CONCRETE OR HOT-MIX ASPHALT MATERIAL DELIVERIES.
- ALL FRAME AND LID CASTINGS LOCATED WITHIN THE PAVEMENT WHICH REQUIRE RESETTING TO FINISH GRADE SHALL BE BACKFILLED WITH CLASS S1 CONCRETE AND ALLOWED TO CURE FOR 72 HOURS. HMA MATERIALS WILL NOT BE ALLOWED AS BACKFILL AROUND AN ADJUSTED CASTING. THIS WORK SHALL APPLY AT ALL CASTINGS ADJUSTED OR RECONSTRUCTED AS PART OF THIS CONTRACT, WHETHER PAID FOR SEPARATELY OR INCLUDED IN OTHER CONTRACT WORK. SEE SPECIAL PROVISION.
- THE COST OF MAKING ANY SEWER CONNECTIONS TO EXISTING DRAINAGE STRUCTURES OR PIPE SHALL BE INCLUDED IN THE COST OF THE NEW SEWER OR STRUCTURE. ANY ADDITIONAL STORM SEWER PIPE REQUIRED TO MAKE THE CONNECTION SHALL BE OF THE SAME SIZE AND MATERIAL TYPE AS THE EXISTING STORM SEWER AND SHALL BE INCLUDED IN THE COST OF THE SEWER OR STRUCTURE.
- IF ANY STORM SEWER LATERALS ARE FOUND DURING CONSTRUCTION AND ARE NOT IDENTIFIED ON THE PLANS, THEY SHALL BE CONNECTED TO THE PROPOSED STORM SEWER SYSTEM AND INCLUDED IN THE COST OF THE STORM SEWER CONSTRUCTION.
- STORM STRUCTURE OFFSET LOCATIONS ARE TO THE EDGE OF PAVEMENT IF THE STRUCTURE IS IN THE CURB LINE OR TO THE CENTER OF STRUCTURE IF THE STRUCTURE IS NOT IN THE CURBLINE.
- IN ALL TRENCHES CROSSING DRIVEWAYS, SIDEWALKS, AND ALL PROPOSED AND EXISTING ROADWAYS, THE MATERIAL FOR THE TOP 12 INCHES SHALL BE CA-6 CRUSHED GRAVEL OR CRUSHED STONE AND BE INCLUDED IN THE PAY ITEM FOR TRENCH BACKFILL.
- FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF COST OF THE STRUCTURE.
- A PORTABLE BATHROOM(S) SHALL BE PLACED ON THE JOB SITE(S) AND RELOCATED WHEN NECESSARY SO IT IS ACCESSIBLE TO WORKERS. IF WORK IS OCCURRING AT SEVERAL LOCATIONS, ONE PORTABLE BATHROOM SHALL BE PLACED AT EACH LOCATION WITHIN A REASONABLE DISTANCE FROM THE WORK AS DETERMINED BY THE ENGINEER. THIS SHALL BE INCLUDED IN THE PAY ITEM FOR MOBILIZATION IN ACCORDANCE WITH ARTICLE 107.08.
- THE DETAIL FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT SHOWN ON THE PLANS SHALL BE MODIFIED TO INCLUDE THE FOLLOWING. THE WORK SHALL INCLUDE SAW-CUTTING AND REMOVING THE EXISTING PAVEMENT A MINIMUM OF 6-INCHES MEASURED FROM THE EXISTING EDGE OF PAVEMENT, AND FILLING THE 6" GAP WITH CLASS S1 CONCRETE TO AN ELEVATION 5-1/4" BELOW THE PROPOSED CURB AND GUTTER FLAG. IF THE CONCRETE IS PLACED HIGHER THAN 5-1/4" FROM THE GUTTER FLAG FOR STREETS TO BE RESURFACED, THE CONTRACTOR WILL BE REQUIRED TO GRIND ADDITIONAL CONCRETE TO THE REQUIRED 5-1/4" DEPTH. THE AREA BEHIND THE PROPOSED CURB AND GUTTER SHALL BE RESTORED WITH NATIVE SOD IN ACCORDANCE WITH SECTION 1081.03 RATHER THAN SALT TOLERANT SOD. THIS WORK IS INCLUDED IN THE COST OF THE APPLICABLE CURB AND GUTTER PAY ITEMS.
- ON STREETS TO BE FULL WIDTH MILLED (2" OR MORE), THE EXISTING STRUCTURES IN THE PAVEMENT SHALL BE ADJUSTED IN ACCORDANCE WITH THE IDOT DETAIL "DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING". THIS WORK SHALL BE IN ADDITION TO THE REQUIREMENTS FOR ANY STRUCTURE TO BE ADJUSTED AND SHALL BE PAID FOR ONCE AT THE CONTRACT UNIT PRICE FOR THE APPLICABLE PAY ITEM.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND SHALL NOT EXCEED 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. A MAXIMUM GRADE DIFFERENCE OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H), AS DETERMINED BY THE ENGINEER.

INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS, LEGEND, AND GENERAL NOTES
3 - 4	SUMMARY OF QUANTITIES
5	TYPICAL SECTIONS AND HOT-MIX ASPHALT MIXTURE REQUIREMENTS
6 - 9	PLAN AND PROFILE - LONG LAKE DRIVE
10	DISTRICT 1 DETAIL - BD-01 DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5m)
11	DISTRICT 1 DETAIL - BD-08 FRAMES AND LIDS ADJUSTMENT WITH MILLING AND FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING
12	DISTRICT 1 DETAIL - BD-24 CURB AND CURB AND GUTTER REMOVAL AND REPLACEMENT
13	DISTRICT 1 DETAIL - TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
14	DISTRICT 1 DETAIL - TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
15	DISTRICT 1 DETAIL - TC-22 ARTERIAL ROAD INFORMATION SIGN
16 - 17	CROSS SECTIONS - LONG LAKE DRIVE

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-07	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-01	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-01	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-01	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424026-01	ENTRANCE/ALLEY PEDESTRIAN CROSSINGS
602001-02	CATCH BASIN TYPE A
602011-02	CATCH BASIN TYPE C
602401-03	MANHOLE TYPE A
602406-06	MANHOLE TYPE A 6' DIAMETER
602601-03	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-03	FRAME AND LIDS TYPE 1
604051-03	FRAME AND GRATE TYPE 11
606001-05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORE TIME OPERATIONS
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-03	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
728001-01	TELESCOPING STEEL SIGN SUPPORT
780001-04	TYPICAL PAVEMENT MARKINGS

LEGEND

EXISTING ELEVATION + STATION
PROPOSED ELEVATION

BENCHMARKS

BM*1	RRS IN POWER POLE AT NORTHWEST CORNER OF LONG LAKE DRIVE AND LOTUS DRIVE ELEV = 751.68
BM*2	RRS IN POWER POLE WITH LIGHT AT ROUND LAKE BEACH PUBLIC WORKS DRIVE ELEV = 757.15
BM*3	RRS IN POWER POLE ON EAST SIDE OF SUNSET DRIVE AT LONG LAKE DRIVE ELEV = 759.19
DATUM IS NAVD 88	

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BAXTER & WOODMAN
Consulting Engineers

DESIGNED - MWP	REVISED - IDOT REVIEW 8-18-14
DRAWN - KAR	REVISED - IDOT REVIEW 8-28-14
CHECKED - TMS	REVISED -
DATE - 06-16-14	FILE - 100345SHT-GenNotes.dgn

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**INDEX OF SHEETS, HIGHWAY STANDARDS,
LEGEND, AND GENERAL NOTES**

SCALE: NONE

STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
210	12-00035-00-RS	LAKE	17	2
CONTRACT NO. 61A71				
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003 (940)				

SUMMARY OF QUANTITIES

CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE	
				RESURFACING	NON PARTICIPATING
				0005 STU	(100% VILLAGE) 0005
20200100	EARTH EXCAVATION	CU YD	134	134	0
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	193	187	6
20800150	TRENCH BACKFILL	CU YD	75	75	0
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1,350	1,350	0
21400100	GRADING AND SHAPING DITCHES	FOOT	130	130	0
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	17	17	0
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	17	17	0
25200110	SODDING, SALT TOLERANT	SQ YD	1,350	1,350	0
25200200	SUPPLEMENTAL WATERING	UNIT	21	21	0
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	28	28	0
28000305	TEMPORARY DITCH CHECKS	FOOT	40	40	0
28000400	PERIMETER EROSION BARRIER	FOOT	200	200	0
28000510	INLET FILTERS	EACH	15	14	1
# 30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	39	33	6
35101400	AGGREGATE BASE COURSE, TYPE B	TON	341	315	26
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	971	971	0
35102400	AGGREGATE BASE COURSE, TYPE B 12"	SQ YD	632	607	25

* SPECIALTY ITEM
INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE AND/OR DETAIL
\$ INDICATES CONSTRUCTION CODE 0042 TRAINEES

CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE	
				RESURFACING	NON PARTICIPATING
				0005 STU	(100% VILLAGE) 0005
# 35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	14	14	0
# 35800100	PREPARATION OF BASE	SQ YD	6,315	5,825	49
# 40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	1,090	1,005	85
# 40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	727	670	57
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	1	1	0
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	162	162	0
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	6,300	6,300	0
42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SQ FT	855	855	0
42400800	DETECTABLE WARNINGS	SQ FT	40	40	0
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	277	277	0
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	582	582	0
44000600	SIDEWALK REMOVAL	SQ FT	6,838	6,838	0
550A2320	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 12"	FOOT	196	196	0
60201105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 11 FRAME AND GRATE	EACH	2	2	0
60207905	CATCH BASINS, TYPE C, TYPE 11 FRAME AND GRATE	EACH	2	2	0
60219300	MANHOLES, TYPE A, 4'-DIAMETER TYPE 11 FRAME AND GRATE	EACH	1	1	0
60224020	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 11 FRAME AND GRATE	EACH	1	1	0

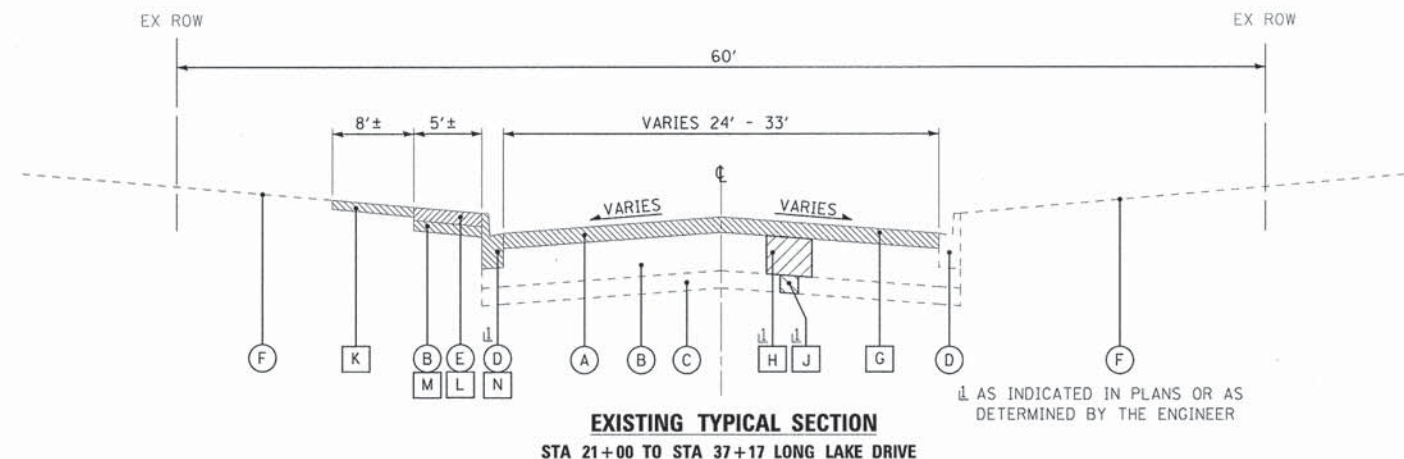
* SPECIALTY ITEM
INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE AND/OR DETAIL
\$ INDICATES CONSTRUCTION CODE 0042 TRAINEES

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BAXTER & WOODMAN Consulting Engineers	DESIGNED - MWP	REVISED - IDOT REVIEW 8-18-14
	DRAWN - KAR	REVISED - IDOT REVIEW 8-28-14
	CHECKED - TMS	REVISED
	DATE - 06-16-14	FILE - 100345SHT-S00.dgn

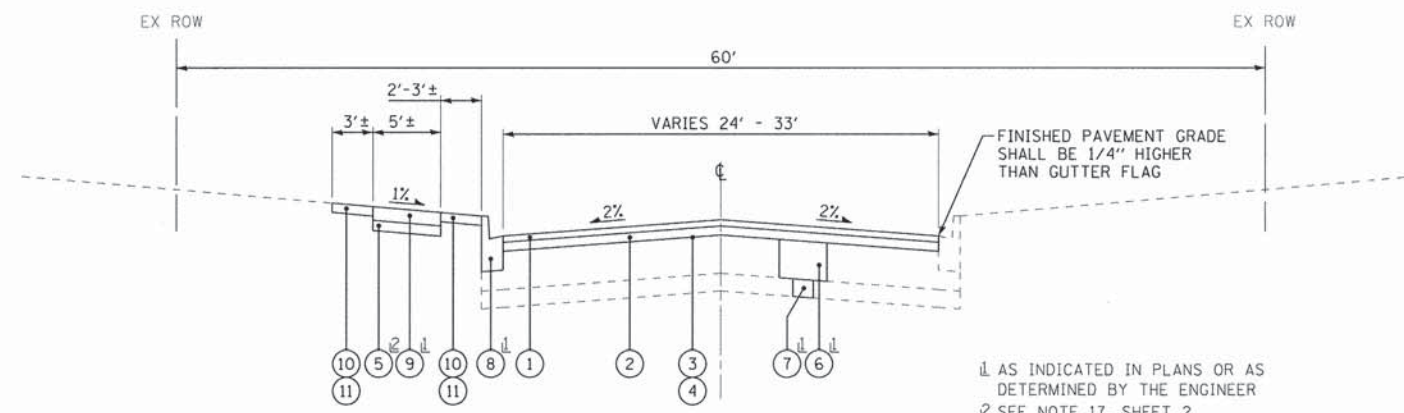
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES		F.A.U. RTE. 210	SECTION 12-00035 00-RS	COUNTY LAKE	TOTAL SHEETS 17	SHEET NO. 3
SCALE: NONE	STA.	STA.	CONTRACT NO. 61A7I			
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-9003 (940)						



EXISTING TYPICAL SECTION
STA 21+00 TO STA 37+17 LONG LAKE DRIVE

↓ AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER



PROPOSED TYPICAL SECTION
STA 21+00 TO STA 37+17 LONG LAKE DRIVE

↓ AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER
⊗ SEE NOTE 17, SHEET 2

•• AGGREGATE SUBGRADE IMPROVEMENT (ASI) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ASI WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50; (IL 9.5mm); 2"	4% @ 50 Gyr.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50; 3"	4% @ 50 Gyr.
DRIVEWAYS (COMMERCIAL)	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50; (IL 9.5mm); 2"	4% @ 50 Gyr.
HMA BASE COURSE (HMA BINDER IL-19.0) N50; 8" (3 LIFTS)	4% @ 50 Gyr.
DRIVEWAYS (INCIDENTAL HOT-MIX ASPHALT SURFACING)	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50; (IL 9.5mm); 3"	4% @ 50 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS

EXISTING LEGEND

- (A) EXISTING HOT-MIX ASPHALT PAVEMENT
- (B) EXISTING AGGREGATE BASE COURSE
- (C) EXISTING SUB-GRADE
- (D) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (E) EXISTING SIDEWALK
- (F) EXISTING GROUND SURFACE
- (G) HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL
- (H) BASE COURSE REMOVAL (SPECIAL)
- (J) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (UNDERCUT)••
- (K) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (TOPSOIL)
- (L) SIDEWALK REMOVAL
- (M) AGGREGATE BASE COURSE REMOVAL (INCLUDED IN EARTH EXCAVATION PAY ITEM)
- (N) COMBINATION CURB AND GUTTER REMOVAL
- [Hatched Box] ITEM TO BE REMOVED

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2"
- (2) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 - 3"
- (3) PREPARATION OF BASE
- (4) AGGREGATE BASE COURSE, TYPE B
- (5) AGGREGATE BASE COURSE, TYPE B 4"
- (6) AGGREGATE BASE COURSE, TYPE B 12"
- (7) AGGREGATE SUB-GRADE IMPROVEMENT••
- (8) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (9) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (10) TOPSOIL FURNISH AND PLACE, 4"
- (11) SODDING

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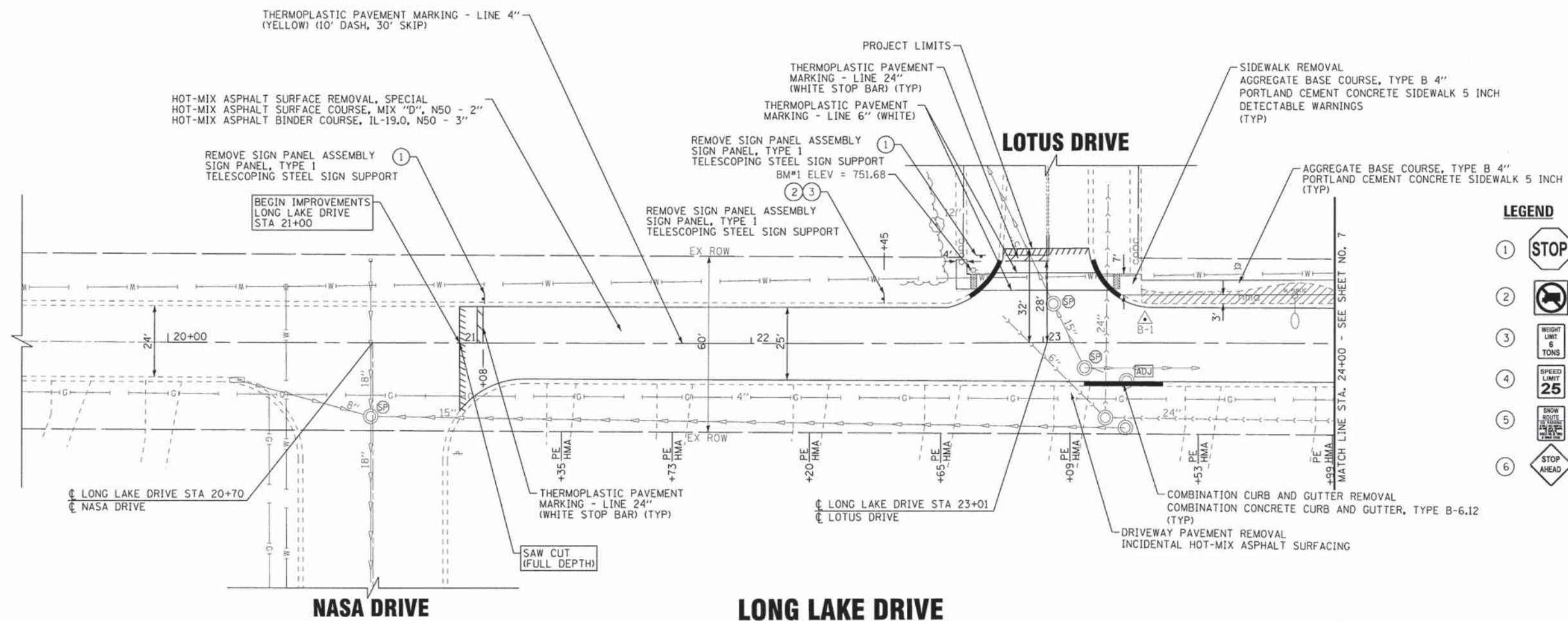
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DATE - 06-16-14	FILE - 100345SHT-TypSec.dgn

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS AND HMA MIXTURE REQUIREMENTS

SCALE: NONE STA. TO STA.

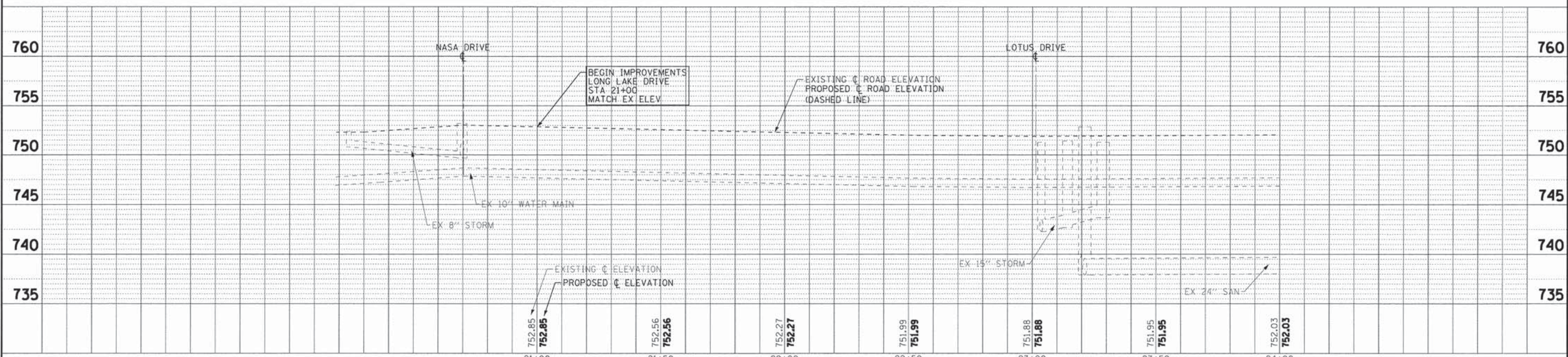
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
210	12-00035-00-RS	LAKE	17	5
CONTRACT NO. 61A71				
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-9003 (940)				



LEGEND

1		R1-1 36" X 36"
2		R5-2 30" X 30"
3		R12-1 24" X 30"
4		R2-1 24" X 30"
5		R7-203 24" X 30"
6		W3-1a 30" X 30"

NOTE:
PCC SIDEWALK IS 5 FEET WIDE
UNLESS OTHERWISE NOTED.

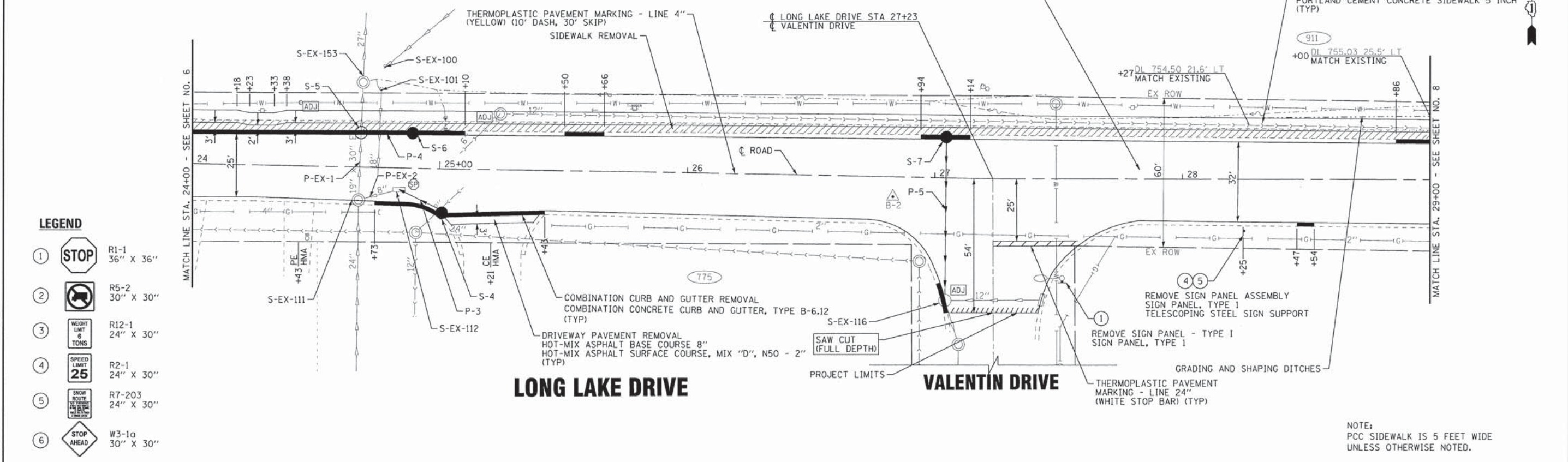


BAXTER & WOODMAN Consulting Engineers	DESIGNED - MWP	REVISED - IDOT REVIEW 8-18-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN AND PROFILE LONG LAKE DRIVE	F.A.U. RTE. 210	SECTION 12-00035-00-RS	COUNTY LAKE	TOTAL SHEETS 17	SHEET NO. 6
	DRAWN - KAR	REVISED - IDOT REVIEW 8-28-14			SCALE: H: 1"=20' V: 1"=5'	STA. 20+00 TO STA. 24+00	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT M-9003 (940)	CONTRACT NO. 61A71
	CHECKED - TMS	REVISED -							
	DATE - 06-16-14	FILE - 100345SHT-PP1.dgn							

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STORM STRUCTURES							
STRUCTURE NUMBER	STATION	OFFSET	STRUCTURE	FRAME AND LID	RIM	INVERTS	
S-EX-100	24+78	40.5' LT	STORM SEWER	-	-	EX 751.12 8" (NE)	
S-EX-101	24+76	31' LT	STORM SEWER	-	-	EX 748.90 18" (S)	
S-EX-111	24+67	13' RT	MANHOLE 5' DIA	TYPE 11	EX 751.74	EX 742.16 19" x 30" (N)	EX 742.16 19" x 30" (S) EX 746.74 8" (NE)
S-EX-112	24+84	9' RT	INLET 2' DIA	TYPE 1 OPEN LID	EX 751.58	EX 748.78 8" (SW)	748.78 12" (SE)
S-EX-153	24+69	34' LT	MANHOLE 5' DIA	TYPE 1 OPEN LID	EX 750.28	EX 741.69 19" x 30" (N)	EX 741.69 19" x 30" (S)
S-4	25+01	18' RT	CATCH BASIN, TYPE C 2' DIA	TYPE 11	751.50±	749.18 12" (NW)	
S-5	24+68	13' LT	MANHOLE TYPE 6' DIA	TYPE 11	751.50±	EX 741.89± 19" x 30" (N)	EX 741.89± 19" x 30" (S) 746.50 12" (E)
S-6	24+89	13' LT	CATCH BASIN, TYPE A 4' DIA	TYPE 11	751.58±	746.92 12" (W)	
S-7	27+05	15.5' LT	CATCH BASIN, TYPE A 4' DIA	TYPE 11	753.12±	749.62 12" (S)	
S-EX-116	27+05	50' RT	CATCH BASIN, TYPE A 4' DIA	TYPE 1 OPEN LID	752.74±	EX 749.29 12" (S)	EX 749.29 12" (E) 749.29 12" (N)

STORM PIPES					
PIPE NO.	CLASS/TYP	DIAMETER	LENGTH (FT)	SLOPE (%)	TBF CU YD
P-EX-1	EXISTING REINFORCED CONCRETE STORM SEWER ELLIPTICAL	19" x 30"	47	1.00%±	
P-EX-2	EXISTING REINFORCED CONCRETE STORM SEWER	8"	17	12.00%±	
P-3	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1	12"	20	2.00%	8
P-4	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1	12"	21	2.00%	9
P-5	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 2	12"	65	0.50%	26



- LEGEND**
- 1 STOP 36" X 36"
 - 2 R5-2 30" X 30"
 - 3 R12-1 24" X 30"
 - 4 R2-1 24" X 30"
 - 5 R7-203 24" X 30"
 - 6 W3-1a 30" X 30"

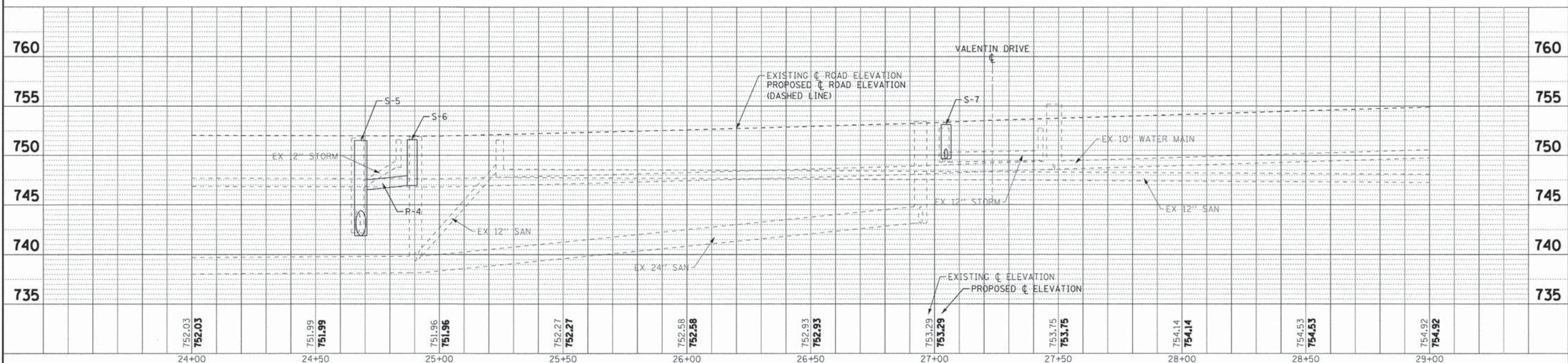
HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2"
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 - 3"

AGGREGATE BASE COURSE, TYPE B 4"
PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (TYP)

LONG LAKE DRIVE

VALENTIN DRIVE

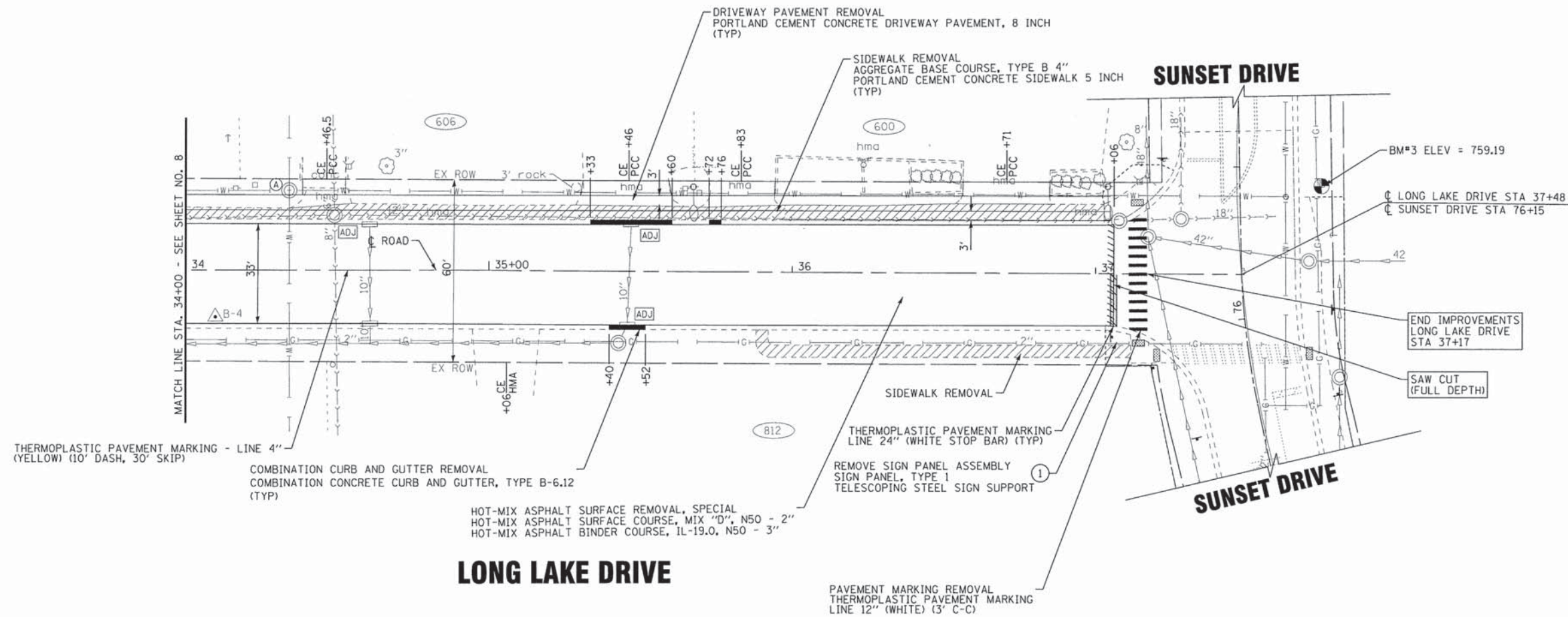
NOTE:
PCC SIDEWALK IS 5 FEET WIDE UNLESS OTHERWISE NOTED.



BAXTER & WOODMAN Consulting Engineers	DESIGNED - MWP	REVISED - IDOT REVIEW 8-18-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN AND PROFILE LONG LAKE DRIVE	F.A.U. RTE. 210	SECTION 12-00035-00-RS	COUNTY LAKE	TOTAL SHEETS 17	SHEET NO. 7	
	DRAWN - KAR	REVISED - IDOT REVIEW 8-28-14			CONTRACT NO. 61A71					
	CHECKED - TMS	REVISED -			SCALE: H: 1"=20' V: 1"=5'					
	DATE - 06-16-14	FILE - 100345SHT-PP2.dgn			STA. 24+00 TO STA. 29+00					

FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003 (940)

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 PLOT: 120035.dwg
 PLOT DATE: 8/28/2014 3:44:57 PM



THERMOPLASTIC PAVEMENT MARKING - LINE 4\"/>

COMBINATION CURB AND GUTTER REMOVAL
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
(TYP)

LONG LAKE DRIVE

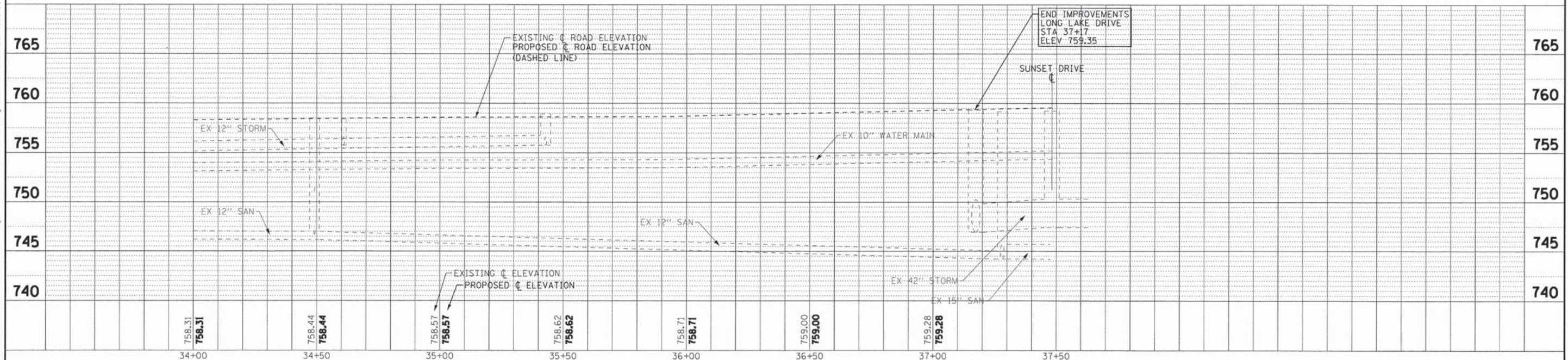
HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2\"/>

PAVEMENT MARKING REMOVAL
THERMOPLASTIC PAVEMENT MARKING
LINE 12\"/>

LEGEND

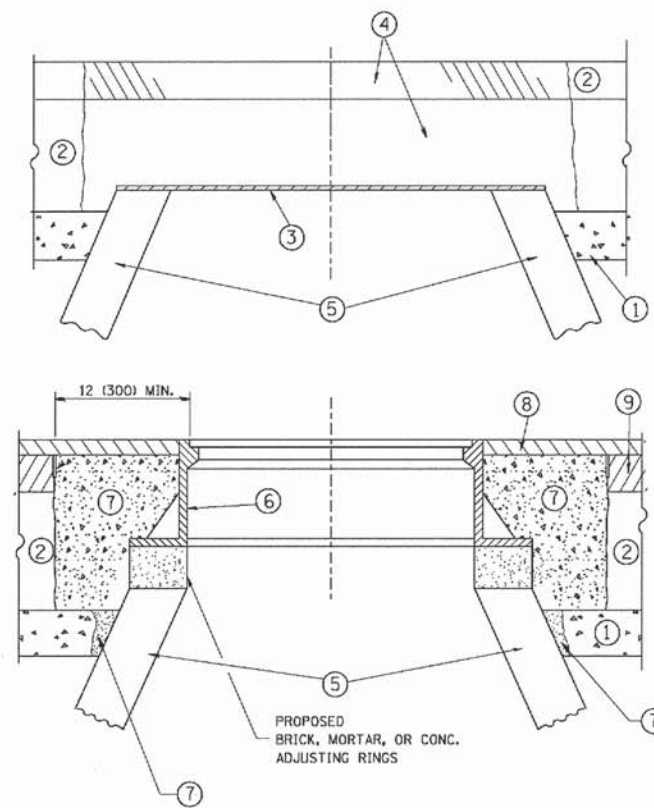
- 1 STOP R1-1 36" X 36"
- 2 R5-2 30" X 30"
- 3 WEIGHT LIMIT 5 TONS R12-1 24" X 30"
- 4 SPEED LIMIT 25 R2-1 24" X 30"
- 5 SHOW ROUTE TO BE CLOSED TO TRAFFIC R7-203 24" X 30"
- 6 STOP AHEAD W3-1a 30" X 30"

NOTE:
PCC SIDEWALK IS 5 FEET WIDE
UNLESS OTHERWISE NOTED.



BAXTER & WOODMAN Consulting Engineers	DESIGNED - MWP	REVISED - IDOT REVIEW 8-18-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN AND PROFILE LONG LAKE DRIVE	F.A.U. RTE. 210	SECTION 12-00035-00-RS	COUNTY LAKE	TOTAL SHEETS 17	SHEET NO. 9	
	DRAWN - KAR	REVISED - IDOT REVIEW 8-28-14			CONTRACT NO. 61A71					
	CHECKED - TMS	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003 (940)					
	DATE - 06-16-14	FILE - 100345SHT-PP4.dgn			SCALE: H: 1"=20' V: 1"=5'					

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NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

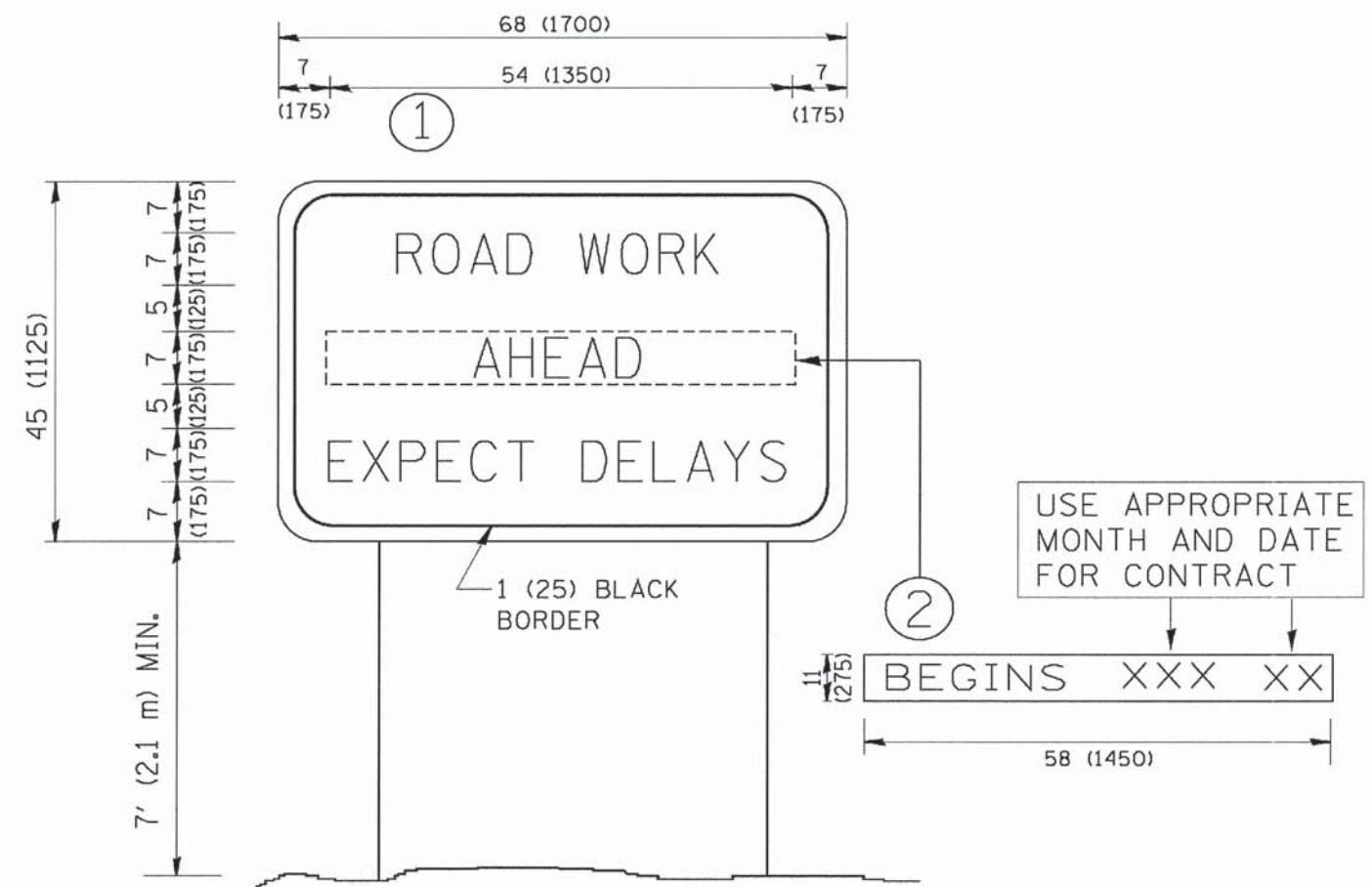
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...	...	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE. 210	SECTION 12-00035-00-RS	COUNTY LAKE	TOTAL SHEETS 17	SHEET NO. 11
BD600-03 (BD-8)			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

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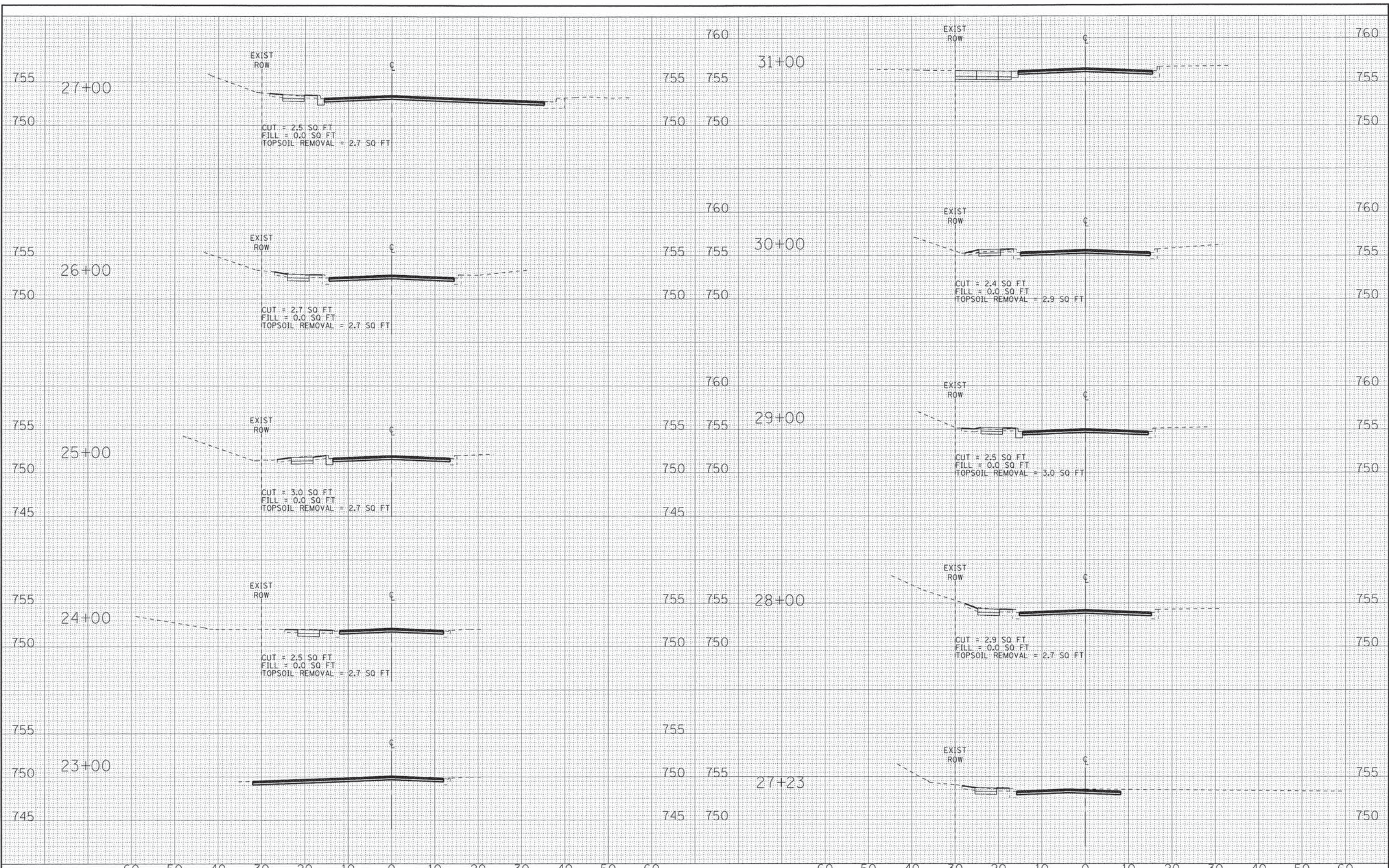
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		CHECKED -	REVISED - T. RAMMACHER 02-02-99
		DATE -	REVISED - C. JUCLUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ARTERIAL ROAD INFORMATION SIGN	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
210	12-00035-00-RS	LAKE	17	15
TC-22		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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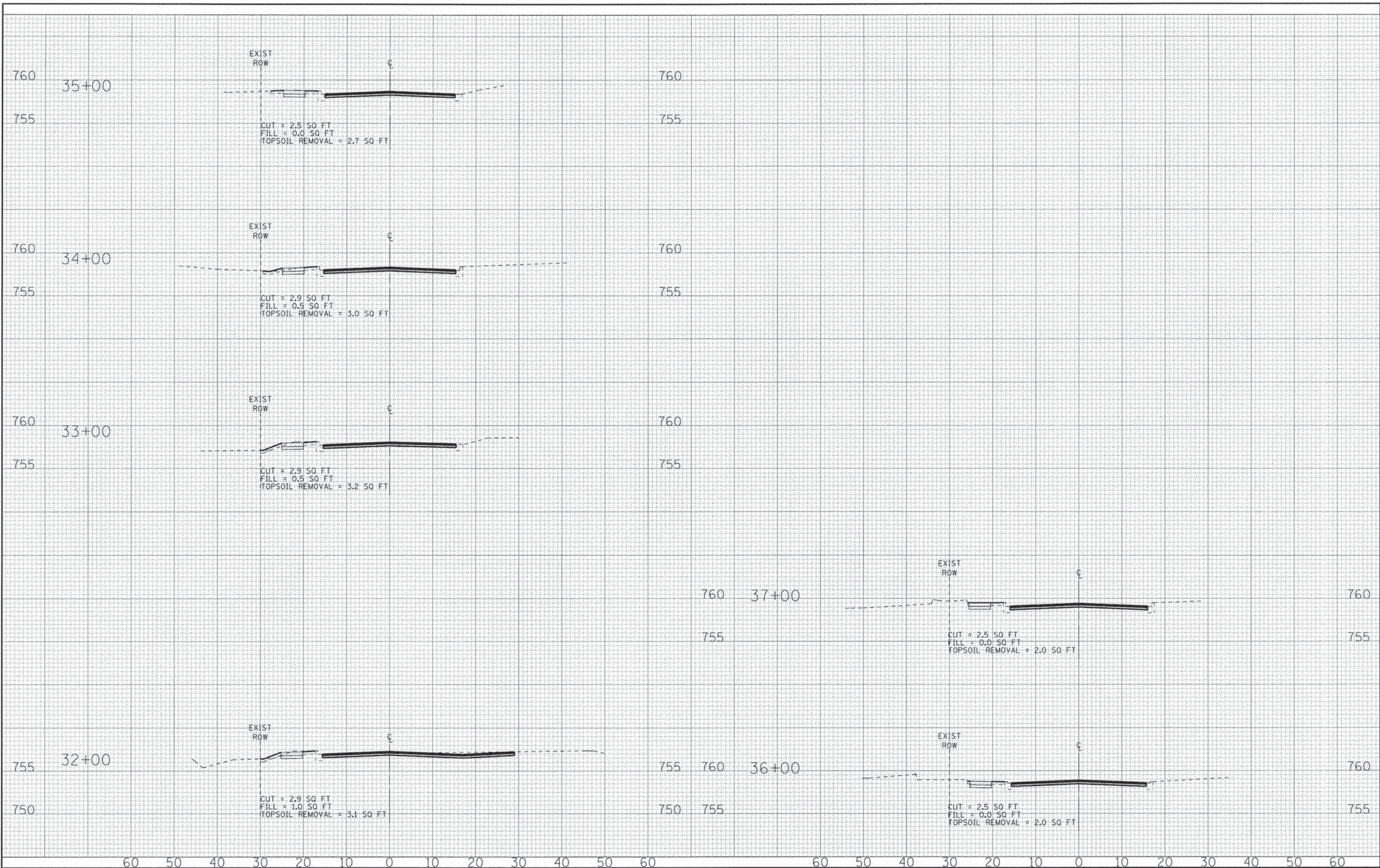
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DRAWN - KAR	REVISED -
CHECKED - TMS	REVISED -
DATE - 06-16-14	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
LONG LAKE DRIVE
 SCALE: H: 1"=10' V: 1"=5'
 STA. 23+00 TO STA. 31+00

F.A.U. RTE. 210	SECTION 12-00035-00-RS	COUNTY LAKE	TOTAL SHEETS 17	SHEET NO. 16
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

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DESIGNED - MWP	REVISED - IDOT REVIEW 8-28-14
DRAWN - KAR	REVISED -
CHECKED - TMS	REVISED -
DATE - 06-16-14	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
LONG LAKE DRIVE**

SCALE: H: 1"=10' V: 1"=5'

STA. 32+00 TO STA. 37+00

F.A.J. RTE. 210	SECTION 12-00035-00-RS	COUNTY LAKE	TOTAL SHEETS 17	SHEET NO. 17
CONTRACT NO.			ILLINOIS FED. AID PROJECT	