

01-15-2021 LETTING ITEM 160

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU ROUTE 2539 (ROY AVENUE)
OVER ADDISON CREEK
BRIDGE SUPERSTRUCTURE REHABILITATION
SECTION NO.: 14-00087-00-BR
PROJECT NO.: YSD1(569)
CITY OF NORTHLAKE
COOK COUNTY
C-91-072-18

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	1
ILLINOIS CONTRACT NO.			61G88	

FOR INDEX OF SHEETS, SEE SHEET NO. 2



DESIGN DESIGNATION: MINOR COLLECTOR

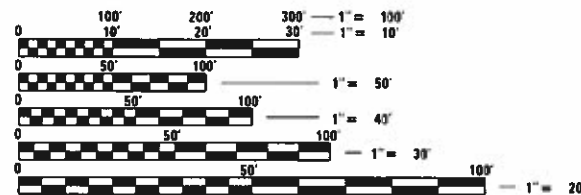
ROY AVENUE

2018 AADT = 1750

2032 AADT = 1700

DESIGN SPEED = 30 MPH

POSTED SPEED = 25 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

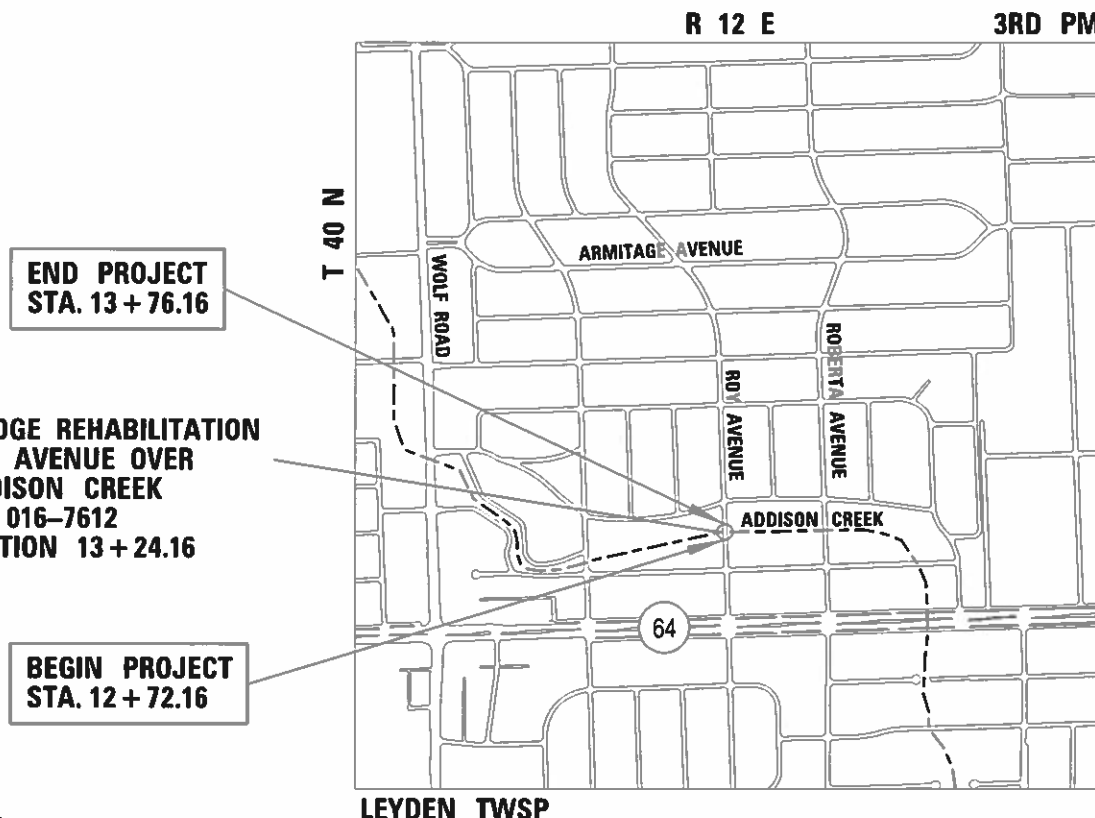
J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT MANAGER: MARK WRZESZCZ (847) 823-0500

CB CHRISTOPHER B. BURKE ENGINEERING, LTD.
9575 W. Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

CONTRACT NO. 61G88

FEDERAL AID ENGINEER: CARMEN E. RAMOS, P.E., SCHAUMBURG, IL



BRIDGE REHABILITATION
ROY AVENUE OVER
ADDISON CREEK
S.N. 016-7612
STATION 13+24.16

LOCATION MAP
NOT TO SCALE

GROSS LENGTH = 84.00 FT. = 0.016 MILE
NET LENGTH = 84.00 FT. = 0.016 MILE

11 / 06 / 2020

JASON G. SOUDEN
REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS
ILLINOIS REGISTRATION No. 062-050850
EXPIRATION DATE: 11/30/2020

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED: [Signature] 11 / 06 / 2020
CITY OF NORTHLAKE MAYOR

PASSED: Nov 12 20 20
[Signature]
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW: NOVEMBER 12 20 20
[Signature]
REGIONAL ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, LIST OF HIGHWAY STANDARDS, GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	TYPICAL SECTIONS
5	ALIGNMENT, TIES, AND BENCHMARKS
6	REMOVAL PLANS
7	ROADWAY PLAN AND PROFILE
8	MAINTENANCE OF TRAFFIC / DETOUR
9 - 10	EROSION AND SEDIMENT CONTROL
11 - 15	ELECTRICAL PLANS - PROPOSED & EXISTING
16 - 28	STRUCTURAL SHEETS
29 - 32	DISTRICT DETAILS / STANDARDS
33	CROSS SECTIONS

INDEX OF HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
420406	PAVEMENT CONNECTOR (HMA) FOR BRIDGE APPROACH SLAB
515001-04	NAME PLATE FOR BRIDGES
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701001-02	OFF-ROAD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5 M) AWAY
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
701011-04	OFF-ROAD OPERATIONS, 2L, 2W, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS

DISTRICT 1 DETAILS

DETAIL NO.	DESCRIPTION
BD-24	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
BD-32	BUTT JOINT AND HMA TAPERS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-21	DETOUR SIGNING FOR CLOSING STATE HIGHWAYS

GENERAL NOTES

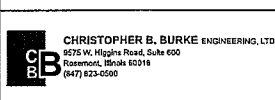
- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "DETAILS" IN THE PLANS, THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS, THE APRIL 1, 2016 EDITION OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", THE JANUARY 1, 2021 EDITION OF "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE 2009 EDITION AND JUNE 2014 REVISION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (IMUTCD), THE AMERICANS WITH DISABILITIES ACT OF 1990 ACCESSIBILITY GUIDELINES, THE "DRAFT" REHABILITATION ACT OF 1973 (SECTION 504), AND THE LATEST PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES.
- THE CONTRACTOR SHALL COMPLY WITH ALL RULES AND REGULATIONS OF OSHA DURING CONSTRUCTION OF IMPROVEMENTS AND RESTORATION. NEITHER THE CITY, DEPARTMENT, NOR THE APPOINTED ENGINEER SHALL BE RESPONSIBLE FOR THE CONTRACTOR'S COMPLIANCE WITH OSHA.
- THE CONTRACTOR SHALL GIVE NOTICES AND COMPLY WITH APPLICABLE LAWS, ORDINANCES, RULES, REGULATIONS AND LAWFUL ORDERS OF ALL PUBLIC AUTHORITIES BEARING ON SAFETY OF PERSONS OR PROPERTY OR THEIR PROTECTION FROM DAMAGE, INJURY OR LOSS.
- IT IS THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR FAILURE TO VERIFY EXISTING DIMENSIONS OR CONDITIONS.
- THE CONTRACTOR SHALL LIMIT CONSTRUCTION ACTIVITIES TO THE WORK AREAS DESIGNATED ON THE PLANS. ANY DAMAGE TO AREAS OUTSIDE OF THESE LIMITS SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER.
- THE CONTRACTOR SHALL RELOCATE OR REMOVE AND REPLACE SIGNS THAT INTERFERE WITH CONSTRUCTION OPERATIONS, AND TO TEMPORARILY RESET ALL SUCH SIGNS DURING CONSTRUCTION OPERATIONS. IF EXISTING SIGNS ARE DAMAGED DURING THE REMOVAL AND REPLACEMENT PROCESS, THE SIGN SHALL BE REPLACED.
- AT THE END OF EACH DAY, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ASSURE THAT ALL STREETS ADJACENT TO THE PROJECT ARE FREE OF ALL CONSTRUCTION RELATED DEBRIS INCLUDING DIRT, STONE, NAILS, ETC. THE WORK SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER AS COORDINATED WITH THE CITY OF NORTHLAKE.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

COMMITMENTS

NONE.

- THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES, SUCH AS: WATER MAIN, SEWERS, GAS LINES, ETC. AS SHOWN ON THE PLANS, HAVE BEEN DETERMINED FROM THE BEST AVAILABLE INFORMATION AND ONLY REPRESENT THE OPINION OF THE CITY OF NORTHLAKE AS TO THEIR LOCATIONS. THE PROVIDED LOCATIONS OF EXISTING UNDERGROUND UTILITIES IS GIVEN FOR THE CONVENIENCE OF THE BIDDER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING THE UTILITY COMPANIES LOCATE THEIR FACILITIES IN THE FIELD PRIOR TO CONSTRUCTION. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL JULIE AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED UTILITIES (48-HOUR NOTIFICATION IS REQUIRED).
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER.
- DRAINAGE: DURING THE CONSTRUCTION OPERATIONS WHEN ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR DRAINAGE STRUCTURES SO THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS ALL DRAINAGE STRUCTURES SHALL BE FREE FROM ALL DIRT AND DEBRIS CAUSED BY THE CONSTRUCTION.
- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR JOB SITE SAFETY AS WELL AS SUPERVISION/ DIRECTION AND MEANS/METHODS OF CONSTRUCTION.
- THE CONTRACTOR SHALL INSTALL/MAINTAIN/REMOVE INLET FILTERS IN ALL OPEN LID DRAINAGE STRUCTURES IN THE PAVEMENT THAT ARE WITHIN THE WORK ZONE OR ACCEPT STORMWATER THAT FLOWS OUT OF THE WORK ZONE, AND AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- AREAS OF PARKWAY RESTORATION ARE SHOWN ON THE PLANS. AREAS DISTURBED BY THE CONTRACTOR BEYOND THOSE SHOWN IN THE PLANS SHALL BE REPAIRED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.
- SOIL STOCKPILES OR OTHER CONSTRUCTION MATERIALS SHALL NOT BE LOCATED WITHIN THE FLOOD PROTECTION AREA.
- NO SPOILS RESULTING FROM CONSTRUCTION ACTIVITIES SHALL LEAVE THE SITE. ANY SPOILS RESULTING FROM CONSTRUCTION ACTIVITIES SHALL BE PLACED AT THE LOCATION DESIGNATED ON THE PROPOSED ROADWAY PLAN OR AS DIRECTED BY THE ENGINEER AS COORDINATED WITH THE CITY. NO SPOILS SHALL BE PLACED IN THE FLOOD PLAIN.
- VERTICAL DATUM: NAVD 1988

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 <p>CHRISTOPHER B. BURKE ENGINEERING, LTD. 8275 W. Higgins Road, Suite 600 Rosemont, Illinois 60018 (847) 823-0500</p>	USER NAME = dconnell	DESIGNED - DOC	REVISED -	<p>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</p>	<p>GENERAL NOTES AND INDEX OF SHEETS, STANDARDS, AND DETAILS</p>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 1"	DRAWN - DOC	REVISED -					2539	14-00087-00-BR	COOK	33	2	
	PLOT DATE = 11/17/2020	CHECKED - JMB	REVISED -					CONTRACT NO. 61G88					
					SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS	FED. AID PROJECT

0013

CODE NO.	ITEM	UNIT	TOTAL QUANTITY (80% Federal 20% Local)
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD.	10
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	167
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	150
25200110	SODDING, SALT TOLERANT	SQ YD	150
25200200	SUPPLEMENTAL WATERING	UNIT	1
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	2
28000400	PERIMETER EROSION BARRIER	FOOT	110
28000510	INLET FILTERS	EACH	4
28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	45
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	5
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	167
35800200	AGGREGATE BASE REPAIR	TON	5
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	150
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	30
42600070	PAVEMENT CONNECTOR (HMA) FOR BRIDGE APPROACH SLAB	SQ YD	67
44000100	PAVEMENT REMOVAL	SQ YD	167
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	100
44000600	SIDEWALK REMOVAL	SQ FT	459
50101500	REMOVAL OF EXISTING SUPERSTRUCTURES	EACH	1

0013

CODE NO.	ITEM	UNIT	TOTAL QUANTITY (80% Federal 20% Local)
50102400	CONCRETE REMOVAL	CU YD	16.7
50200100	STRUCTURE EXCAVATION	CU YD	45
50300225	CONCRETE STRUCTURES	CU YD	27.2
50300255	CONCRETE SUPERSTRUCTURE	CU YD	38.3
50300260	BRIDGE DECK GROOVING	SQ YD	185
50300300	PROTECTIVE COAT	SQ YD	345
50301350	CONCRETE SUPERSTRUCTURE (APPROACH SLAB)	CU YD	43.5
50400505	PRECAST PRESTRESSED CONCRETE DECK BEAMS (27" DEPTH)	SQ FT	2,322
50800205	REINFORCEMENT BARS, EPOXY-COATED	POUND	29,410
* 50900105	ALUMINUM RAILING, TYPE L	FOOT	100
51500100	NAME PLATES	EACH	2
58600101	GRANULAR BACKFILL FOR STRUCTURES	CU YD	145
67100100	MOBILIZATION	L SUM	1
Δ * 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	5
Δ * 66900530	SOIL DISPOSAL ANALYSIS	EACH	1
Δ * 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1
Δ * 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1
Δ * 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	3
* 81100400	CONDUIT, ATTACHED TO STRUCTURE GALVANIZED STEEL, 1 1/4" DIA.	FOOT	70

0013

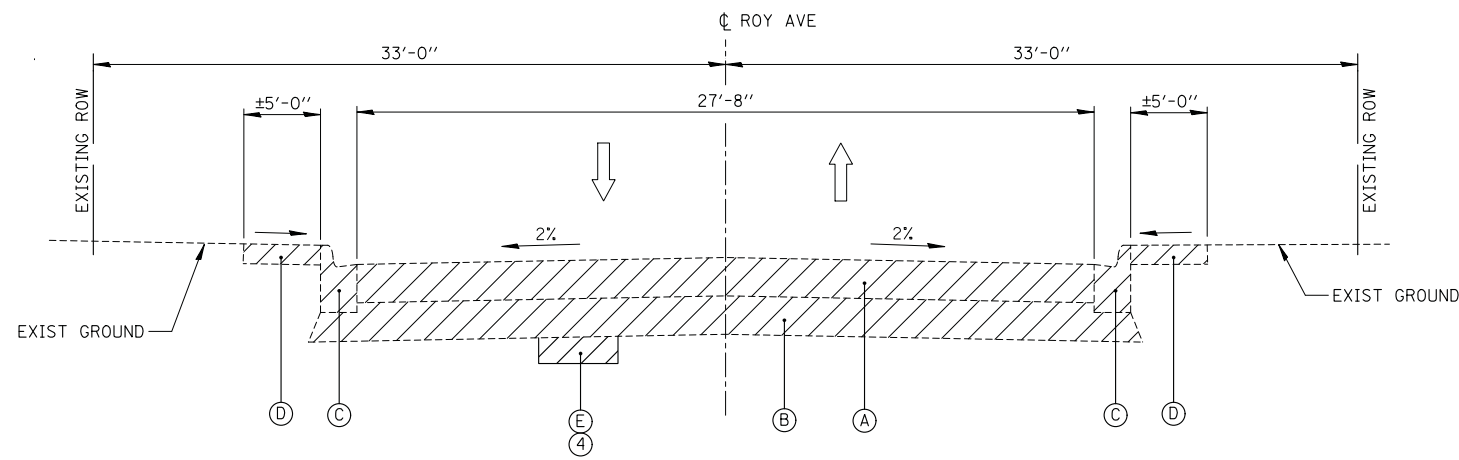
CODE NO.	ITEM	UNIT	TOTAL QUANTITY (80% Federal 20% Local)
* 81702120	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 8	FOOT	200
* 81702130	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6	FOOT	800
* 81800415	AERIAL CABLE, 4-1/C #6 WITH MESSENGER WIRE	FOOT	200
* X8130100	REMOVE EXISTING CONDUIT, ATTACHED TO STRUCTURE	FOOT	70
* 89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	350
Δ * X8130120	RELOCATE EXISTING JUNCTION BOX	EACH	2
Δ * X8951011	REMOVE AERIAL CABLE	FOOT	200
Δ X1700034	FORM LINER TEXTURED SURFACE, SPECIAL	SQ FT	525
Δ X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5-INCH, SPECIAL	SQ FT	497
Δ X5030305	CONCRETE WEARING SURFACE, 5"	SQ YD	258
Δ X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1
Δ * XX006834	ELECTRICAL CONNECTION TO EXISTING LIGHTING SYSTEM	EACH	2
Δ XX009048	CURB AND GUTTER (SPECIAL)	FOOT	40
Δ Z0013798	CONSTRUCTION LAYOUT	L SUM	1
Δ Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	45
Δ * Z0033024	MAINTENANCE OF EXISTING LIGHTING SYSTEM	L SUM	1
Δ # Z0076500	TRAINEES	HOURL	500
Δ # Z0076504	TRAINEES TRAINING PROGRAM GRADUATE	HOURL	500

#0042

* SPECIALTY ITEMS
 Δ SPECIAL PROVISION

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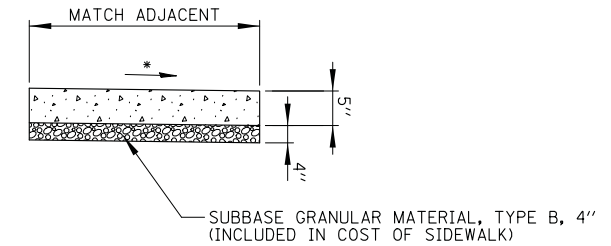
<p>CHRISTOPHER E. BURKE ENGINEERING, LTD. 2075 W. Highway 146, Suite 400 Glenview, Illinois 60016 (847) 423-2500</p>	USER NAME = doconnell	DESIGNED - DOC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.U. RTE. 2539	SECTION 14-00087-00-BR	COUNTY COOK	TOTAL SHEETS 33	SHEET NO. 3	
	PLOT SCALE = 1"	CHECKED - JMB	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 61G88			
	PLOT DATE = 11/17/2020	DATE = 11/17/2020	REVISED -		ILLINOIS FED. AID PROJECT									



EXISTING TYPICAL SECTION

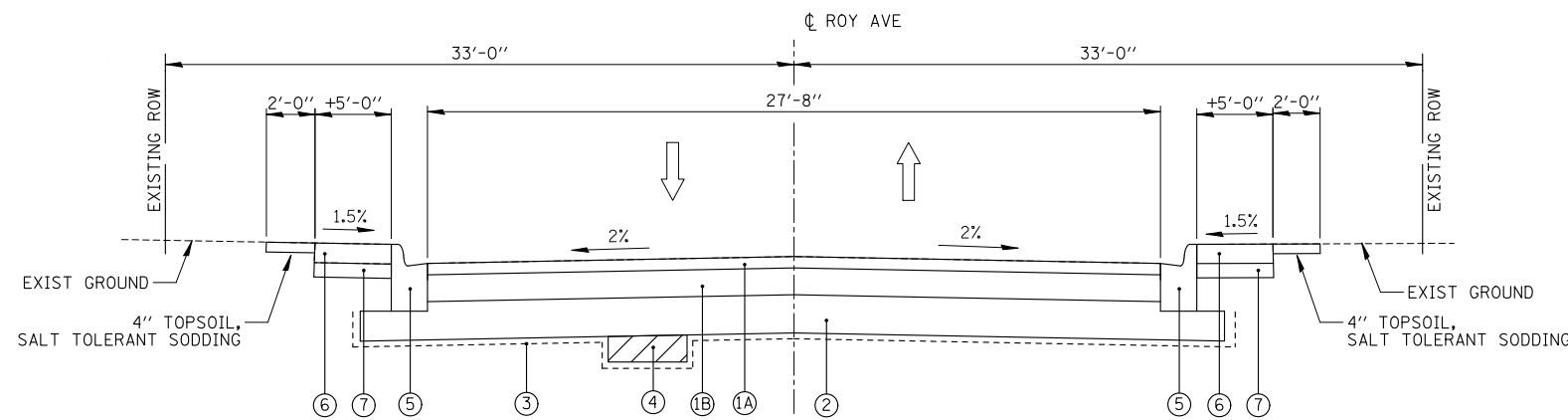
ROY AVENUE
 STA. 12+72.16 TO 13+76.16
 BRIDGE OMISSION: 12+96.16 TO 13+52.16
 SCALE: N.T.S.

NOTE:
 SEE STRUCTURAL PLAN SHEETS FOR TYPICAL
 APPROACH SLAB AND BRIDGE SECTIONS



P.C.C. SIDEWALK 5 INCH, SPECIAL
 NOT TO SCALE

NOTE:
 ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK
 SHALL BE INCLUDED IN THE COST OF P.C.C. SIDEWALK 5 INCH, SPECIAL.



PROPOSED TYPICAL SECTION

ROY AVENUE
 STA. 12+72.16 TO 13+76.16
 BRIDGE & APPROACH OMISSION: 12+82.16 TO 13+66.16
 SCALE: N.T.S.

NOTE:
 SEE STRUCTURAL PLAN SHEETS FOR TYPICAL
 APPROACH SLAB AND BRIDGE SECTIONS

NOTE:

1. THE EXISTING PAVEMENT SECTION CONSISTS OF VARIABLE DEPTH HMA.
2. CONTRACTOR SHALL REMOVE AND REPLACE ANY UNSUITABLE MATERIAL UNDER SIDEWALK, CURB AND GUTTER REPLACEMENT AND PATCHING LOCATIONS AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SIDEWALK 5", SPECIAL OR CURB AND GUTTER (SPECIAL)
3. AGGREGATE BASE REPAIR (SUBBASE GRANULAR MATERIAL, TYPE B) UNDER SIDEWALKS, CURB AND GUTTER, AND PAVEMENT PATCHES WILL BE DETERMINED BY THE ENGINEER IN THE FIELD. COST SHALL BE INCLUDED IN AGGREGATE BASE REPAIR
4. ADDITIONAL STONE BACK FILLING SHALL BE INCLUDED IN THE COST FOR CURB AND GUTTER (SPECIAL).

HOT - MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VIOLS (%) @ Ndes
PAVEMENT CONNECTOR:	
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	4% @ 70 GYR
HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	4% @ 70 GYR

NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE QUANTITIES IS 112 LB/SQ TD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY SPECIAL PROVISIONS.
3. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

EXISTING LEGEND

- (A) PAVEMENT REMOVAL
- (B) EARTH EXCAVATION
- (C) COMBINATION CURB AND GUTTER REMOVAL
- (D) SIDEWALK REMOVAL
- (E) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (SHALL BE DETERMINED BY ENGINEER IN THE FIELD)

PROPOSED LEGEND

- (1) PAVEMENT CONNECTOR:
 - (1A) 2" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
 - (1B) VARIABLE DEPTH HMA BINDER COURSE, IL-19.0, N70
- (2) AGGREGATE SUBGRADE IMPROVEMENT 12"
- (3) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- (4) AGGREGATE SUBGRADE IMPROVEMENT (SHALL BE DETERMINED BY ENGINEER IN THE FIELD)
- (5) CONCRETE CURB AND GUTTER (SPECIAL)
- (6) PCC SIDEWALK 5", SPECIAL
- (7) AGGREGATE BASE COURSE, TYPE B 4" (INCLUDED IN THE COST OF PCC SIDEWALK 5", SPECIAL)

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

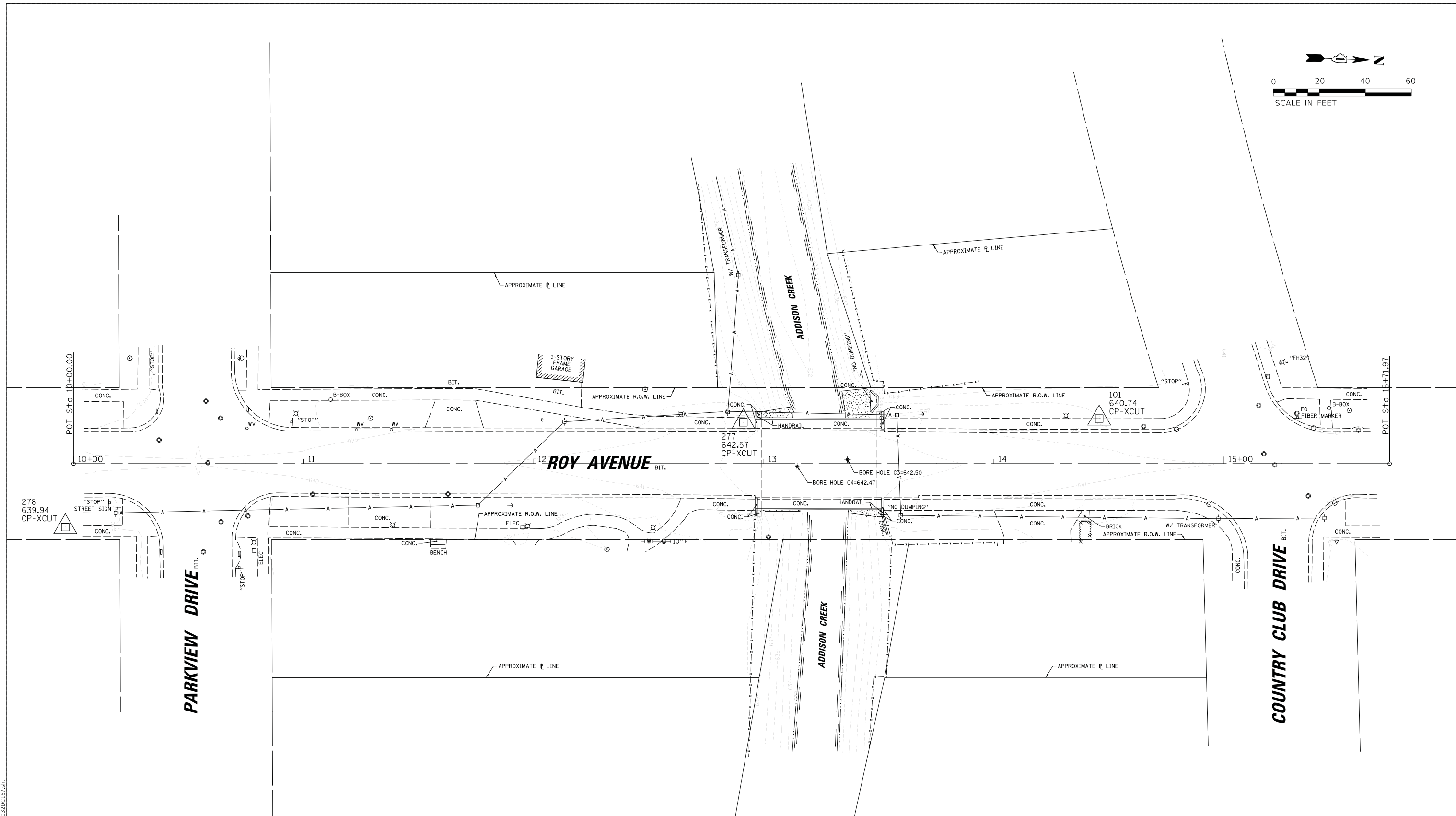
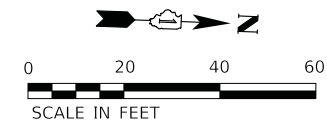
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F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	4
CONTRACT NO. 61G88				
ILLINOIS FED. AID PROJECT				

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CB CHRISTOPHER B. BURKE ENGINEERING, LTD.
 3075 W. Higgins Road, Suite 400
 Rosemont, IL 60018
 (847) 823-0500

USER NAME = dcoconnell	DESIGNED - DOC	REVISED -
	DRAWN - DOC	REVISED -
PLOT SCALE = 5'	CHECKED - JMB	REVISED -
PLOT DATE = 11/17/2020	DATE - 11/17/2020	REVISED -



ELEVATION BENCHMARKS DATUM: NAVD 1988		
NO.	DESCRIPTION	ELEV.
OSBM 1	MINI RAILROAD SPIKE (RECOVERED) IN SOUTH FACE OF POWER POLE WITH LIGHT AT THE NORTHEAST CORNER OF NORTH AVENUE & PRATER AVENUE (REF JOB: 940032DC141)	645.77

HORIZONTAL CONTROL POINTS				
CP NO.	NORTHING (Y)	EASTING (X)	DESCRIPTION	ELEV.
101	N 1 909 897.97	E 1 103 265.52	CP-XCUT	640.74
277	N 1 909 743.52	E 1 103 272.76	CP-XCUT	642.57
278	N 1 909 450.65	E 1 103 329.06	CP-XCUT	639.94
4950	N 1 909 799.64	E 1 102 648.06	CP-XCUT	642.04
6544	N 1 909 878.26	E 1 102 994.38	CP-XCUT	641.39

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CB CHRISTOPHER B. BURKE ENGINEERING, LTD.
 3075 W. Higgins Road, Suite 400
 Rosemont, Illinois 60018
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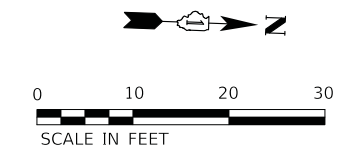
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	DRAWN - DOC	REVISED -
PLOT SCALE = 20'	CHECKED - JMB	REVISED -
PLOT DATE = 11/17/2020	DATE - 11/17/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, TIES, AND BENCHMARKS				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	5
ILLINOIS FED. AID PROJECT				

CONTRACT NO. 61G88



APPROXIMATE ϕ LINE



INL 362
RIM=640.20
INV NE 10" DIP=636.50
INV NW 10" RCP=636.88
BLIND CONNECT

VV 361
RIM=640.82
TOP N/S 6" DIP=633.78

APPROXIMATE ϕ LINE

ADDISON CREEK

27" RCP CULVERT
INV=634.23
W/ CONC. HDW

APPROXIMATE R.O.W. LINE

APPROXIMATE R.O.W. LINE

ROY AVENUE

PROJECT BEGINS
STA 12+72.16

PROJECT ENDS
STA 13+76.16

SAN 422
RIM=638.75
INV SE 6" VCP=634.21
INV SW 24" VCP=628.95
INV NW 24" VCP=628.95

VV 798
RIM=640.09
TOP N/S 6" DIP=634.49

VV 317
RIM=640.82
TOP N/S 10" DIP=634.32

NOTE:
SEE STRUCTURAL SHEETS FOR BRIDGE REMOVAL DETAILS

CB 199
RIM=639.97
INV SW 10" RCP=637.76

VV 160
RIM=640.93
TOP W/E 8" DIP=634.73

- LEGEND:**
- PAVEMENT REMOVAL
 - SIDEWALK REMOVAL
 - CURB AND GUTTER REMOVAL

APPROXIMATE ϕ LINE

APPROXIMATE ϕ LINE

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CB CHRISTOPHER B. BURKE ENGINEERING, LTD.
3075 W. Higgins Road, Suite 400
Rosemont, Illinois 60018
(847) 823-0500

USER NAME = docconnell	DESIGNED - DOC	REVISED -
	DRAWN - DOC	REVISED -
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PLOT DATE = 11/17/2020	DATE - 11/17/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

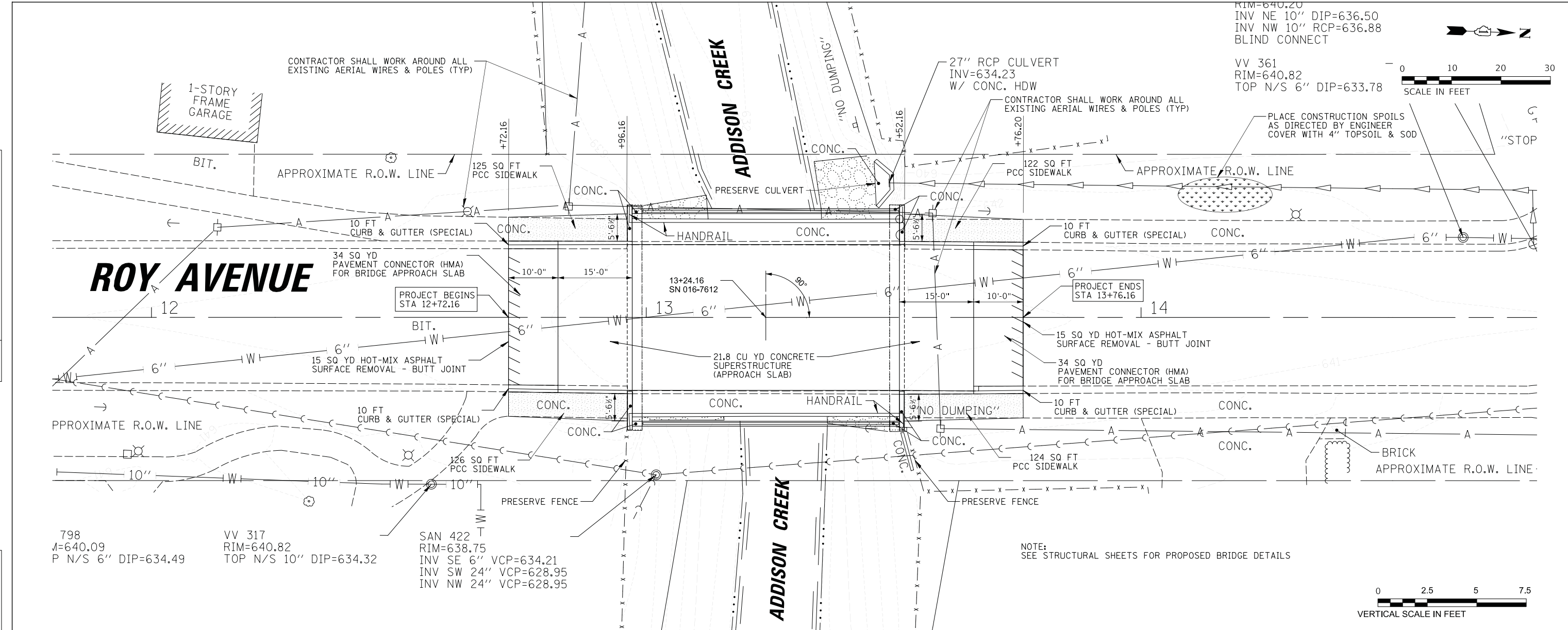
EXISTING CONDITIONS AND REMOVAL PLAN

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	6
CONTRACT NO. 61G88				
		ILLINOIS	FED. AID PROJECT	

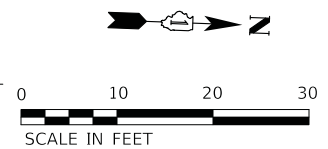
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	PLOTTED
	GRADES CHECKED
	STRUCTURE NOTATIONS CHECKED
NOTE BOOK NO.	
STRUCTURE NOTATIONS CHECKED	



RIM=640.20
 INV NE 10" DIP=636.50
 INV NW 10" RCP=636.88
 BLIND CONNECT

VV 361
 RIM=640.82
 TOP N/S 6" DIP=633.78

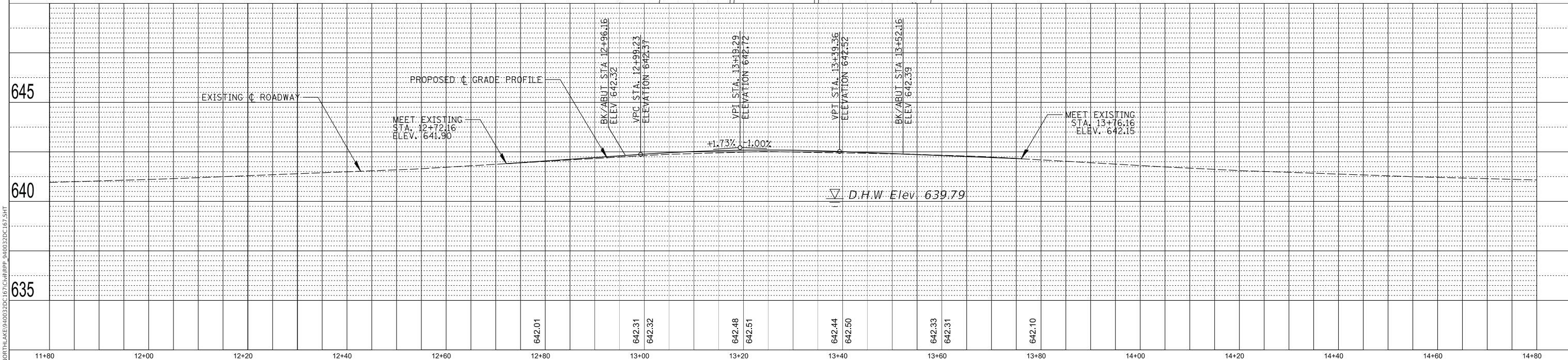


798
 A=640.09
 P N/S 6" DIP=634.49

VV 317
 RIM=640.82
 TOP N/S 10" DIP=634.32

SAN 422
 RIM=638.75
 INV SE 6" VCP=634.21
 INV SW 24" VCP=628.95
 INV NW 24" VCP=628.95

NOTE:
 SEE STRUCTURAL SHEETS FOR PROPOSED BRIDGE DETAILS



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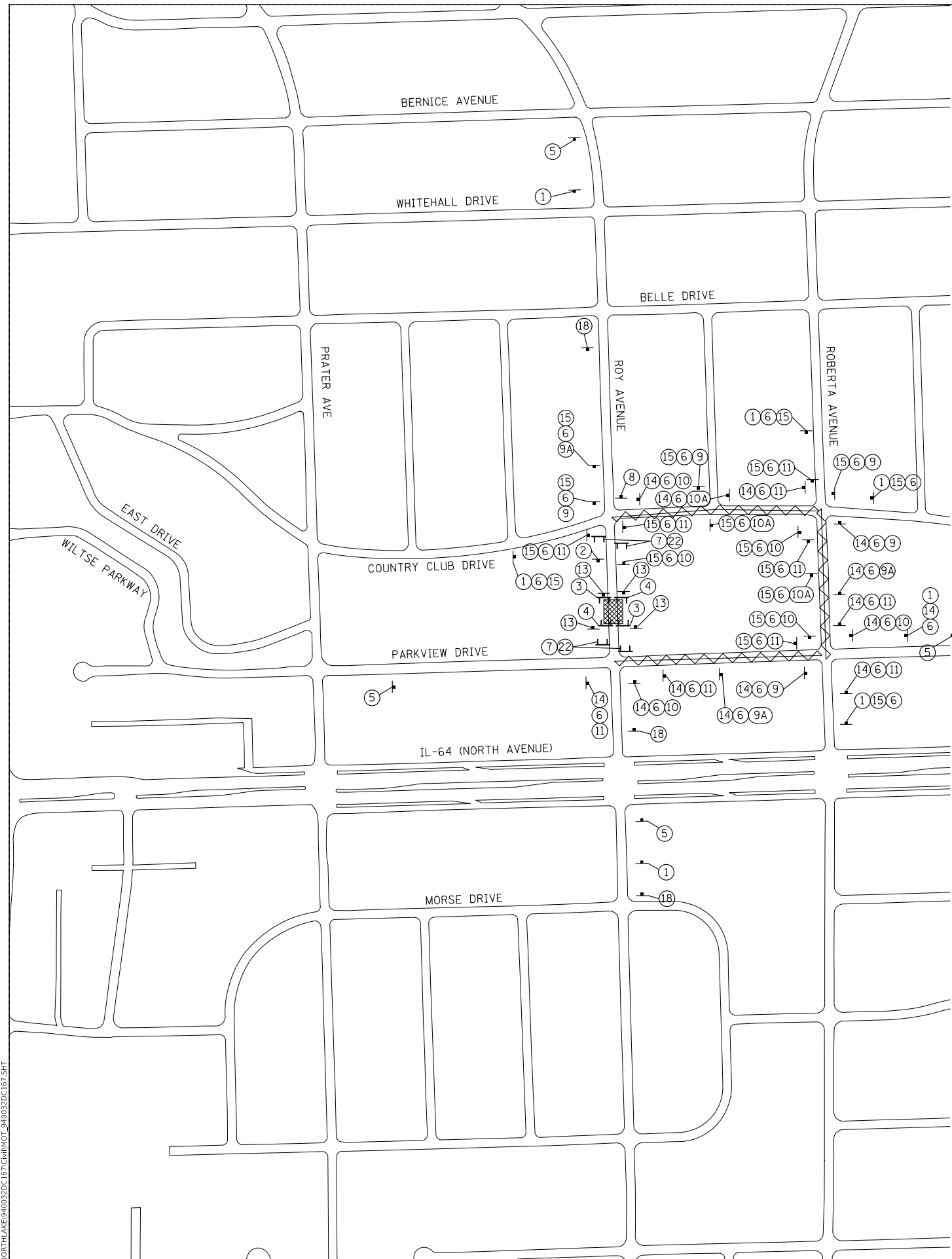
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PLOT DATE =	11/17/2020	DATE -	11/17/2020	REVISED -	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN AND PROFILE

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	7
				CONTRACT NO. 61G88
				ILLINOIS FED. AID PROJECT



①		⑥	Roy Avenue	⑭	NORTH
	W20-2(O) 4848		6" BLACK LETTERS, ORANGE REFLECTIVE BACKGROUND		M3-1(O) 2412
②		⑦	ROAD CLOSED TO THRU TRAFFIC	⑮	SOUTH
	W20-3(O) 4848		R11-4 6030		M3-3(O) 2412
③	ROAD CLOSED	⑧	END DETOUR	⑯	
	R11-2 4830		M4-8A 2418		R3-1 2424
④	BRIDGE OUT	⑨	DETOUR	⑰	
	R11-2 4830		M4-9L 3021		R3-2 2424
⑤	Roy Ave CLOSED AT Addison Creek	⑩	DETOUR		
	6" BLACK LETTERS, ORANGE REFLECTIVE BACKGROUND		M4-9R 3021		W20-3 4848
		⑪	DETOUR	⑱	DETOUR
			M4-9 3021		M4-10L 4818
		⑬	SIDEWALK CLOSED	⑳	DETOUR
			R11-110I 2430		M4-10R 4818

NOTE: REFER TO DISTRICT 1 DETAIL TC-21 FOR GUIDANCE ON TYPICAL SIGN LAYOUT AND SPACING

- LEGEND:**
- SIGN
 - TYPE III BARRICADE
 - PROJECT AREA (COMPLETE CLOSURE)
 - DETOUR ROUTE

1. ALL SIGNING SHALL BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE LATEST EDITION OF THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", "THE QUALITY STANDARD FOR WORK ZONE TRAFFIC CONTROL DEVICES ADOPTED 2010", THE DETAILS IN THESE PLANS, THE LATEST EDITION OF THE STATE OF ILLINOIS "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", THE SPECIAL PROVISIONS FOR TRAFFIC CONTROL AND PROTECTION.
2. THE ENGINEER SHALL BE NOTIFIED IN WRITING AT LEAST THREE WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT. THE ENGINEER SHALL DETERMINE THE HOUR OF CLOSURE. THE ENGINEER WILL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES. CITY WILL PROVIDE CONTRACTOR WITH AGENCY NAMES AND CONTACT INFORMATION ONCE THE NOTICE IS SUBMITTED TO THE CITY FOR REVIEW.
3. THE CONTRACTOR SHALL SUPPLY TO THE ENGINEER THE NAMES AND TELEPHONE NUMBERS OF HIS REPRESENTATIVES ON THE CONSTRUCTION SITE AND HIS REPRESENTATIVE RESPONSIBLE FOR THE DETOUR SIGNING PRIOR TO THE START OF THE WORK.
4. IF REQUESTED BY THE CONTRACTOR IN WRITING AT LEAST THREE WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT, THE ENGINEER WILL FIELD LOCATE THE POSITIONS OF ANY SIGNS.
6. THE ROAD SHALL NOT BE CLOSED UNTIL ALL SIGNING IS ERECTED IN ACCORDANCE WITH THE DETOUR PLAN AND INSPECTED AND APPROVED BY THE ENGINEER.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THAT ALL BARRICADES, SIGNS, LIGHTS, AND OTHER DEVICES INSTALLED BY HIM/HER ARE IN PLACE AND OPERATING 24 HOURS EACH DAY INCLUDING SUNDAYS AND HOLIDAYS DURING THE TIME THE DETOUR IS IN EFFECT.
8. THE TRAFFIC CONTROL SHOWN ON THE DETOUR PLAN IS THE MINIMUM NECESSARY FOR THIS ROAD CLOSURE. THE CONTRACTOR SHALL MAKE ALL CHANGES IN TRAFFIC CONTROL THAT IS DEEMED NECESSARY BY THE ENGINEER.
9. ALL EXISTING SIGNING THAT IS NOT APPLICABLE WHILE THE DETOUR IS IN EFFECT SHALL BE COMPLETELY COVERED BY THE CONTRACTOR, IN A MANNER APPROVED BY THE ENGINEER.
10. ALL DETOUR SIGNING SHALL BE POST MOUNTED IF THE ROAD CLOSURE IS TO EXCEED FOUR (4) CALENDAR DAYS.
11. ALL DETOUR SIGNING EXCEPT REGULATORY SIGNS SHALL HAVE BLACK LEGENDS ON FLUORESCENT ORANGE SHEETING AND STANDARD BLACK BORDERS. THE FLUORESCENT ORANGE REFLECTIVE SHEETING SHALL MEET THE REQUIREMENTS OF ARTICLE 1084.02 OF THE STANDARD SPECIFICATIONS. ALL DETOUR SIGNING SHALL BE NEW OR LIKE NEW CONDITION OF THE SIGNS. THE ENGINEER SHALL BE THE SOLE JUDGE OF THE CONDITION OF THE SIGNS.
12. THE SIZES OF ALL SIGNS NOT SPECIFIED IN THESE PLANS SHALL BE AS REQUIRED BY THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
13. AS A MINIMUM, ALL AMBER FLASHING LIGHTS THAT ARE REQUIRED FOR THIS DETOUR SHALL MEET THE REQUIREMENTS FOR TYPE A-LOW INTENSITY FLASHING LIGHTS IN ARTICLE 1084.01 OF THE STANDARD SPECIFICATIONS. ALL LIGHTS SHALL OPERATE DURING THE HOURS OF DARKNESS. ONLY LIGHTS THAT HAVE BEEN APPROVED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION SHALL BE USED.
14. ALL BARRICADES SHALL HAVE REFLECTORIZED STRIPING ON BOTH SIDES OF THE BARRICADES. THE TYPE III BARRICADES USED AT THE POINT OF CLOSURE TO THRU TRAFFIC SHALL NOT EXCEED 8 FEET IN WIDTH EACH, FOR A SINGLE APPROACH LANE.
15. THE "ROAD CLOSED" (R11-2), THE "ROAD CLOSED TO THRU TRAFFIC" (R11-4) SIGNS SHALL BE MOUNTED ABOVE THE TOP OF THE BARRICADE. ALL TYPE III BARRICADES SHALL HAVE TWO (2) AMBER TYPE A-LOW INTENSITY FLASHING LIGHTS SPACED NEAR THE CENTERLINE OF THE SUPPORTS.
16. THE ROAD NAME SIGN SHALL HAVE A BLACK LEGEND ON FLUORESCENT ORANGE REFLECTIVE SHEETING. THE SIGN BLANK SHALL BE A 9" X VARIABLE OR A 12" X VARIABLE WITH DESIGN SERIES C LETTERS. THE CAPITAL LETTERS SHALL BE 6" WITH 4.5" LOWER CASE.
17. DURING NON-WORKING HOURS AT THE POINT OF ROAD CLOSURE TO ALL TRAFFIC THE CONTRACTOR SHALL PROVIDE A MEANS TO RESTRAIN THE BARRICADES FROM EASY MOVEMENT BY VANDALS. THE CHOSEN METHOD SHALL BE APPROVED BY THE ENGINEER.
18. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE VISIBILITY OF ALL DETOUR AND CONSTRUCTION SIGNING, INCLUDING BRUSHING BACK VEGETATION IF DEEMED NECESSARY BY THE ENGINEER.
19. THE ENGINEER SHALL BE NOTIFIED AT LEAST TWO (2) HOURS BEFORE THE ROAD IS TO BE OPENED TO TRAFFIC. THE ENGINEER WILL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES. CITY WILL PROVIDE CONTRACTOR WITH AGENCY NAMES AND CONTACT INFORMATION ONCE THE NOTICE IS SUBMITTED TO THE CITY FOR REVIEW.
20. BRIDGE MAY BE CLOSED FOR NO LONGER THAN 60 DAYS.
21. NO SIGNS SHALL BE PLACED ON IDOT ROW

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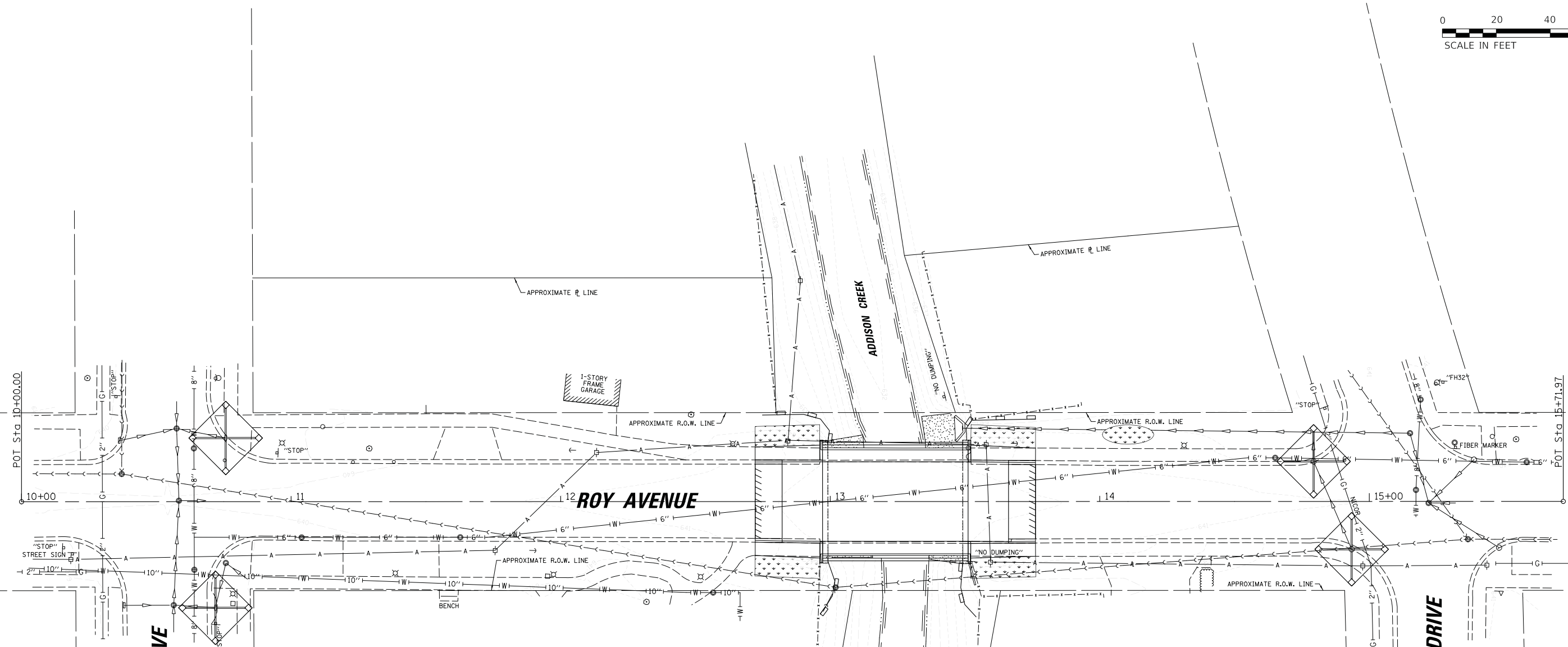
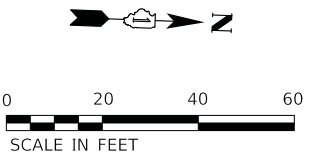
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETOUR PLAN AND
MAINTENANCE OF TRAFFIC PLAN**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	8
CONTRACT NO. 61G88				
		ILLINOIS	FED. AID PROJECT	



EROSION CONTROL NOTES:

1. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE AND REPAIR.
2. ANY SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
3. SOIL STOCKPILES SHALL NOT BE LOCATED IN A FLOOD PRONE AREA OR A DESIGNATED BUFFER PROTECTING WATERS OF THE UNITED STATES.
4. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G. SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURE).
5. THE CONTRACTOR SHALL INSTALL/MAINTAIN/ REMOVE PERIMETER EROSION BARRIER ALONG THE CREEK, AND AT ALL OTHER LOCATIONS WHERE STORM WATER SHEET FLOWS OUT OF THE WORK ZONE, AND AT LOCATIONS AS DIRECTED BY THE ENGINEER.
6. THE CONTRACTOR SHALL INSTALL/MAINTAIN/REMOVE INLET FILTERS IN ALL OPEN LID DRAINAGE STRUCTURES IN THE PAVEMENT THAT ARE WITHIN THE WORK ZONE OR ACCEPT STORMWATER THAT FLOWS OUT OF THE WORK ZONE, AND AT LOCATIONS AS DIRECTED BY THE ENGINEER.
7. THE CONTRACTOR SHALL INSTALL/MAINTAIN/REMOVE INLET AND PIPE PROTECTION IN ALL OPEN LID DRAINAGE STRUCTURES IN THE PARKWAYS THAT ARE WITHIN THE WORK ZONE OR ACCEPT STORMWATER THAT FLOWS OUT OF THE WORK ZONE, AND AT LOCATIONS AS DIRECTED BY THE ENGINEER.
8. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.

LEGEND

- PERIMETER EROSION BARRIER
- INLET FILTER
- TEMPORARY EROSION CONTROL SEEDING
TEMPORARY EROSION CONTROL BLANKET
TOPSOIL & SODDING

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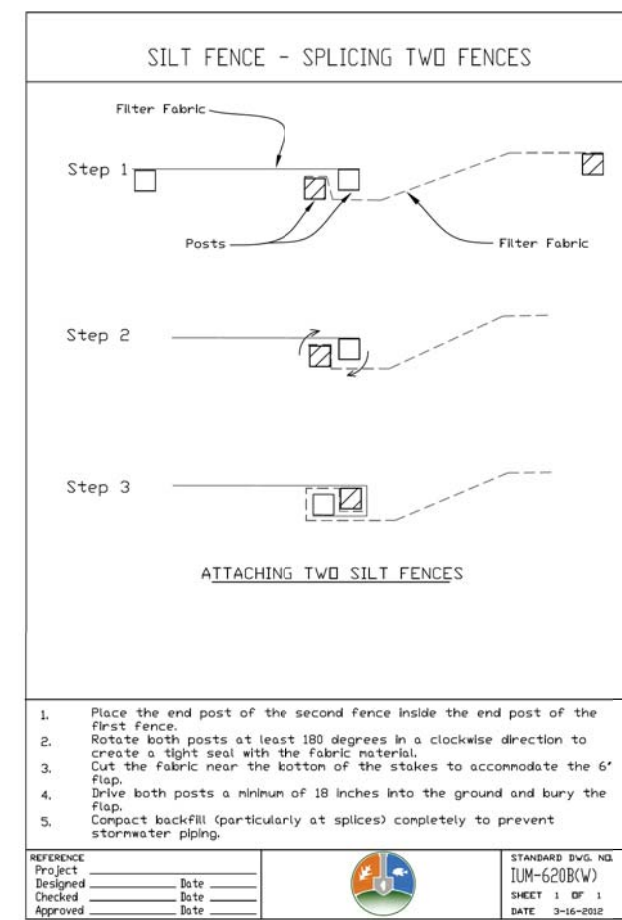
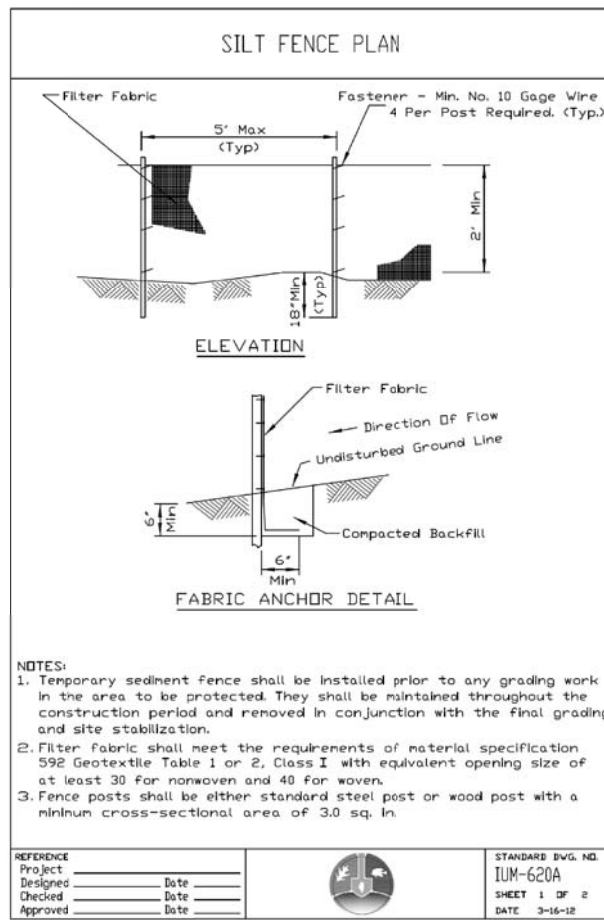
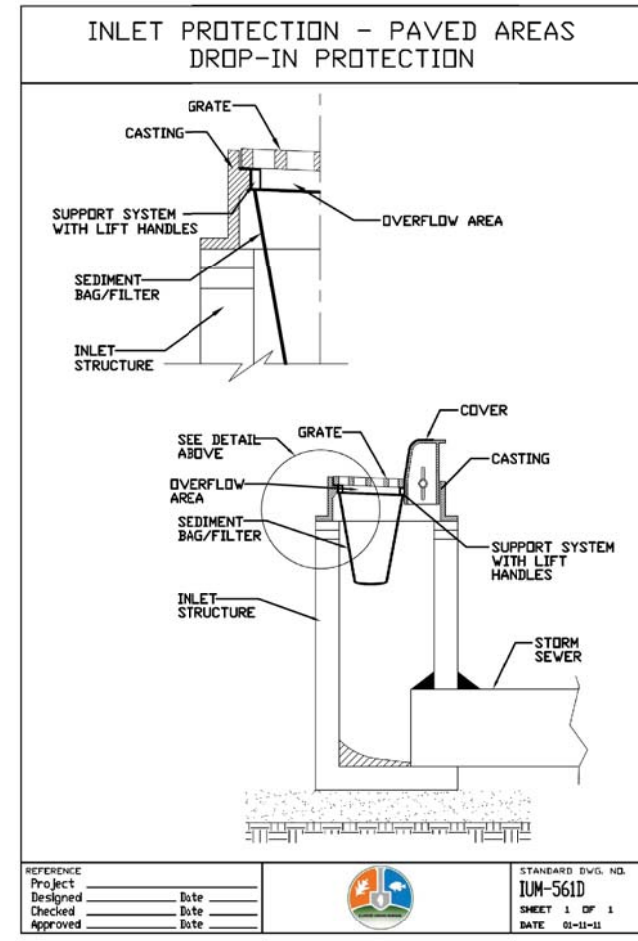
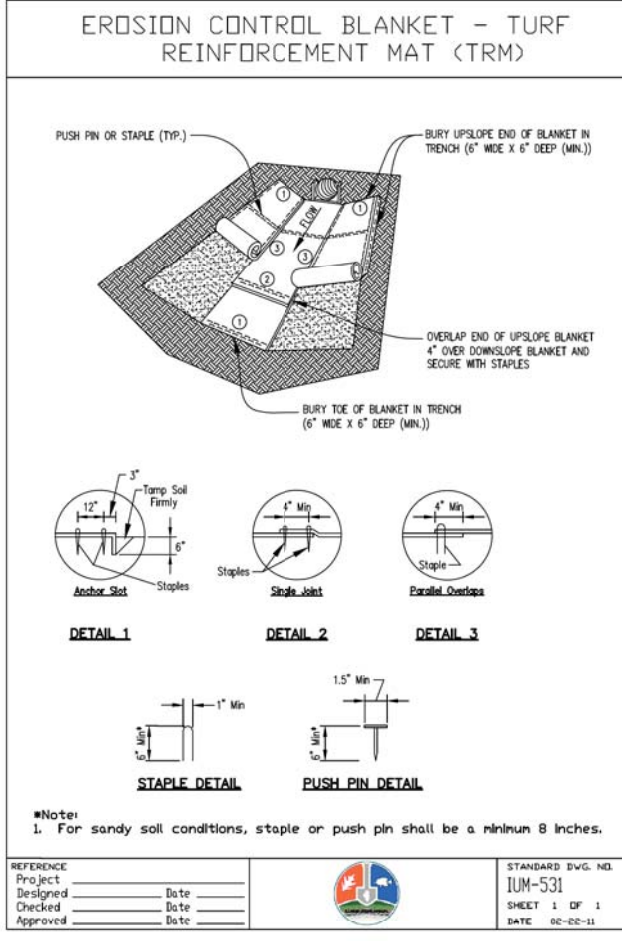
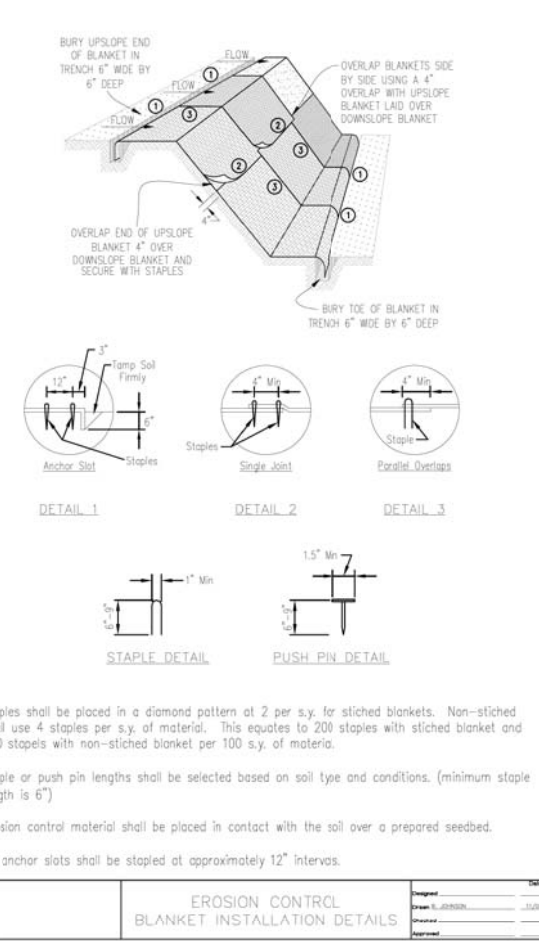
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION CONTROL PLAN

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	9
CONTRACT NO. 61G88				
ILLINOIS FED. AID PROJECT				



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 (847) 823-0500

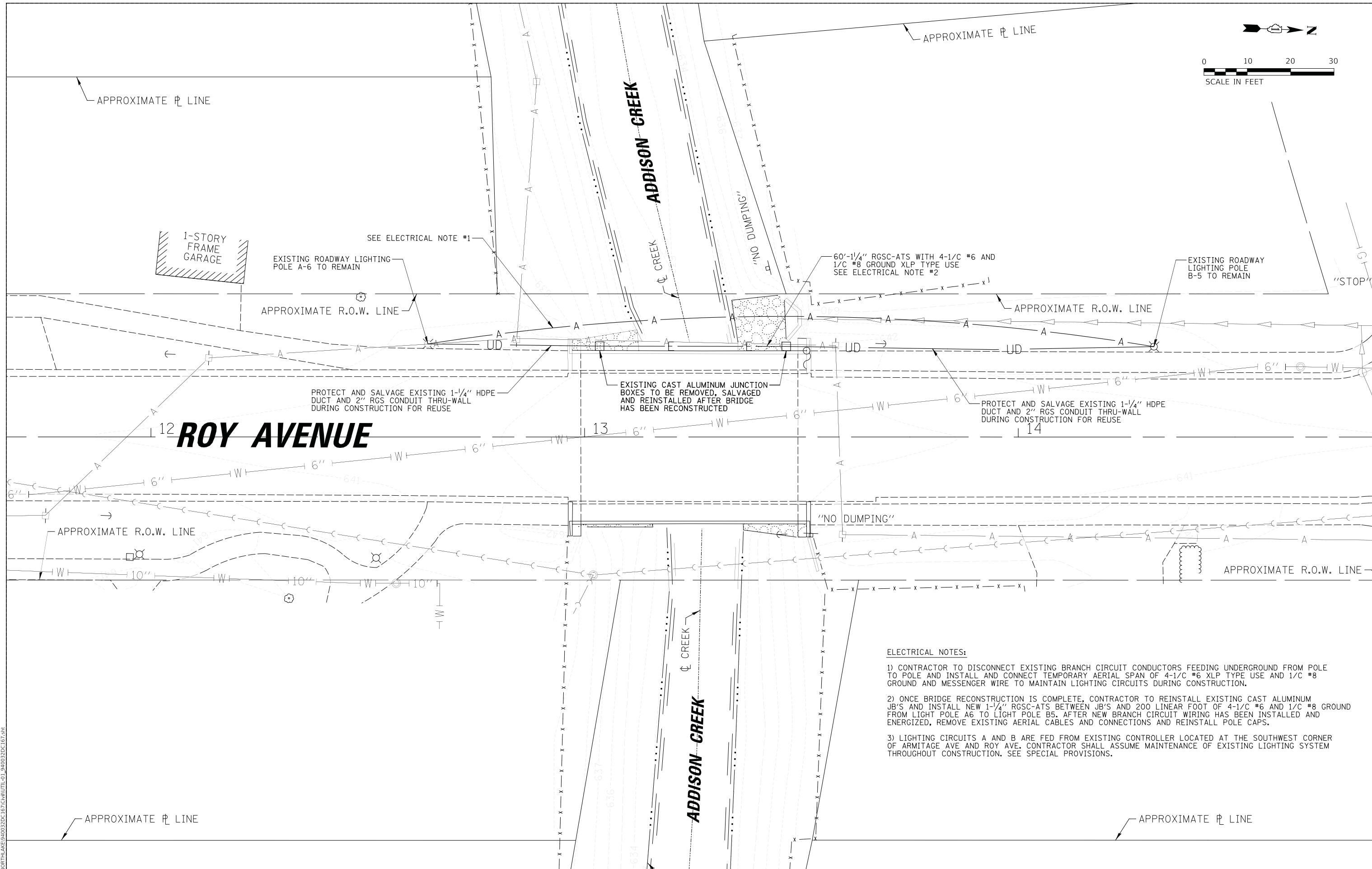
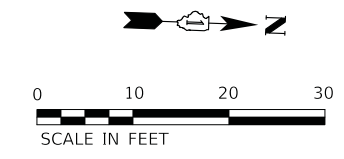
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REFERENCE Project _____ Date _____	DESIGNED _____ Date _____	CHECKED _____ Date _____	APPROVED _____ Date _____
STANDARD DWG. NO. IUM-620A		SHEET 1 OF 2	
DATE 3-16-12			

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EROSION CONTROL DETAIL			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	10
CONTRACT NO. 61G88				
ILLINOIS FED. AID PROJECT				



ELECTRICAL NOTES:

- 1) CONTRACTOR TO DISCONNECT EXISTING BRANCH CIRCUIT CONDUCTORS FEEDING UNDERGROUND FROM POLE TO POLE AND INSTALL AND CONNECT TEMPORARY AERIAL SPAN OF 4-1/2 #6 XLP TYPE USE AND 1/2 #8 GROUND AND MESSENGER WIRE TO MAINTAIN LIGHTING CIRCUITS DURING CONSTRUCTION.
- 2) ONCE BRIDGE RECONSTRUCTION IS COMPLETE, CONTRACTOR TO REINSTALL EXISTING CAST ALUMINUM JB'S AND INSTALL NEW 1-1/4" RGSC-ATS BETWEEN JB'S AND 200 LINEAR FOOT OF 4-1/2 #6 AND 1/2 #8 GROUND FROM LIGHT POLE A6 TO LIGHT POLE B5. AFTER NEW BRANCH CIRCUIT WIRING HAS BEEN INSTALLED AND ENERGIZED, REMOVE EXISTING AERIAL CABLES AND CONNECTIONS AND REINSTALL POLE CAPS.
- 3) LIGHTING CIRCUITS A AND B ARE FED FROM EXISTING CONTROLLER LOCATED AT THE SOUTHWEST CORNER OF ARMITAGE AVE AND ROY AVE. CONTRACTOR SHALL ASSUME MAINTENANCE OF EXISTING LIGHTING SYSTEM THROUGHOUT CONSTRUCTION. SEE SPECIAL PROVISIONS.

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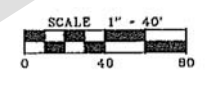
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

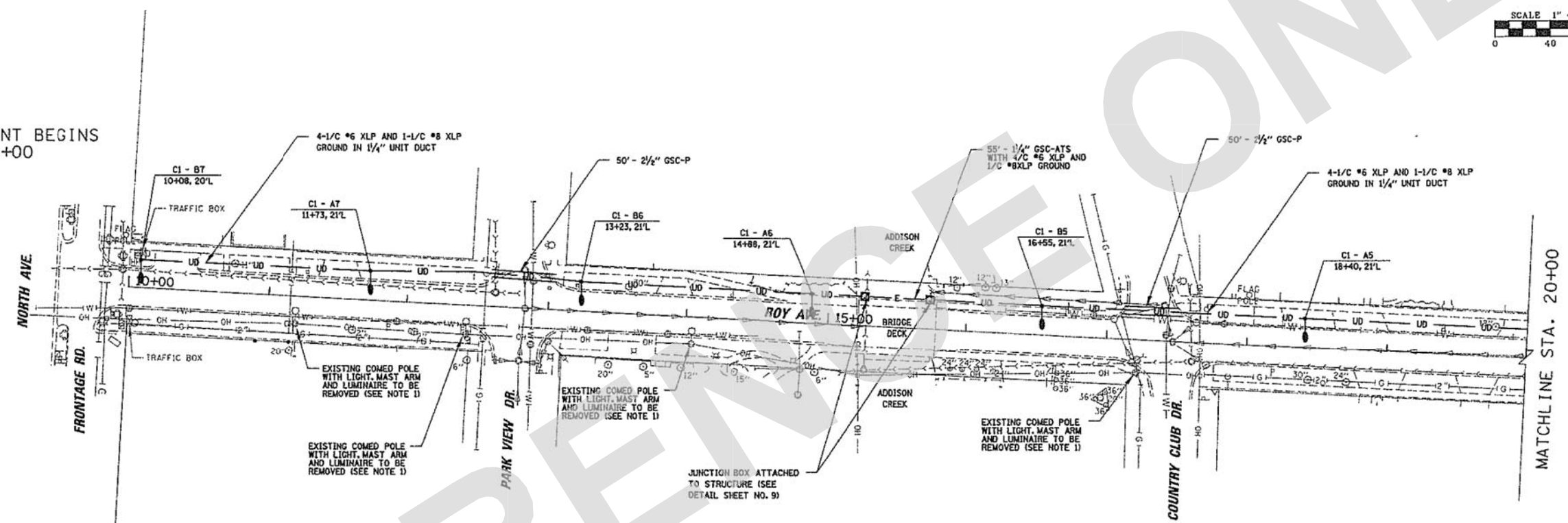
STREET LIGHTING CONDUIT AND WIRING REMOVAL AND REPLACEMENT
 SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	11
CONTRACT NO. 61G88				
		ILLINOIS	FED. AID PROJECT	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539 00-00052-00-17	14-00087-00-BR	COOK	33	12
STA. 10+00	TO STA. 20+00			
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



IMPROVEMENT BEGINS STA. 10+00



LEGEND

- UD PROPOSED 1/4" UNIT DUCT WITH 4-1/2" NO. 6 AWG AND 1/2" NO. 8 COPPER GROUND WIRE OR 1" UNIT DUCT WITH 2-1/2" NO. 6 AWG AND 1/2" NO. 8 COPPER GROUND WIRE
- UD PROPOSED UNIT DUCT IN GALVANIZED STEEL CONDUIT 1 1/2" UNLESS OTHERWISE SPECIFIED
- E PROPOSED 3/4" NO. 2 XLP TYPE ELECTRIC CABLES IN 1 1/2" GALVANIZED STEEL CONDUIT FOR SERVICE TO CONTROLLER C-1 AND C-2 OR 1/2" NO. 6 R/P AND 1/2" NO. 8 XLP GROUND IN 1 1/4" GALVANIZED STEEL CONDUIT ATTACHED TO CONCRETE BRIDGE
- PROPOSED LIGHT STANDARD AND 240V/175W METAL HALIDE LUMINAIRE, 30' S.H. AND 10' MAST ARM
- PROPOSED LIGHT STANDARD AND LUMINAIRE WITH PHOTOCELL
- PROPOSED CONCRETE HANDHOLE
- PROPOSED JUNCTION BOX
- PROPOSED LIGHTING CONTROLLER
- CONTROLLER DESIGNATION
- CI - BT CIRCUIT AND POLE IDENTIFIER
- 10+08, 20' L LOCATION AND OFFSET
- T TRENCHED CONDUIT
- P PUSHED CONDUIT
- A. T. S CONDUIT ATTACHED TO STRUCTURE
- PROPOSED COM ED SERVICE LOCATION
- EXISTING COM ED POLE WITH LIGHT
- EXISTING SANITARY SEWER
- EXISTING STORM SEWER
- EXISTING WATERMAIN
- EXISTING OVERHEAD WIRES
- EXISTING GAS LINE

NOTES

- ALL EXISTING COM ED LIGHTING EQUIPMENT CALLED OUT TO BE REMOVED SHALL BE COORDINATED BY THE CITY OF NORTHLAKE PRIOR TO FINAL ACCEPTANCE OF THE LIGHTING SYSTEM.



REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION ROY AVENUE STREET LIGHTING PLAN (1 OF 6)
NAME	DATE	

01/03/01

DRAWN BY AJO
CHECKED BY

CHRISTOPHER B. BURKE ENGINEERING LTD. PLAN
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Rosemont, Illinois 60018
(847) 823-0500

DATE	BY	REVISION

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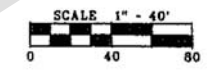
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING STREET LIGHTING PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	12
CONTRACT NO. 61G88				
ILLINOIS FED. AID PROJECT				

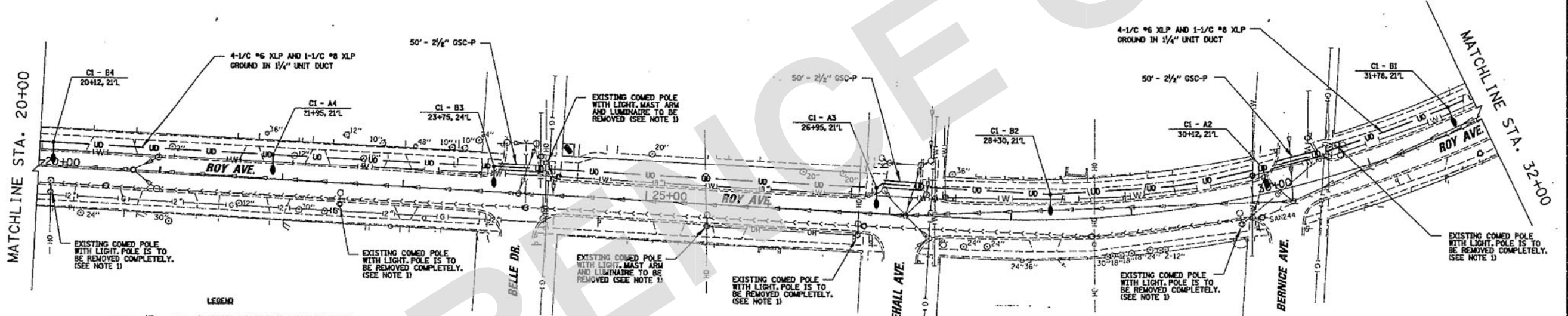
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2539	00-00052-00-L	COOK	10	4
STA. 20+00		TO STA. 32+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



DATE	BY

CHRISTOPHER B. BURKE ENGINEERING LTD.
 9575 West Higgins Road, Suite 600
 Rosemont, Illinois 60018
 (847) 823-0500

DATE	BY



- LEGEND**
- UD PROPOSED 1 1/4" UNIT DUCT WITH 4-1/2" NO. 6 AWG AND 1-1/2" NO. 8 COPPER GROUND WIRE OR 1 1/4" UNIT DUCT WITH 4-1/2" NO. 6 AWG AND 1-1/2" NO. 8 COPPER GROUND WIRE
 - UD PROPOSED UNIT DUCT IN GALVANIZED STEEL CONDUIT, 2 1/2" UNLESS OTHERWISE SPECIFIED
 - E PROPOSED 3/4" NO. 2 XLP TYPE ELECTRIC CABLES IN 2 1/2" GALVANIZED STEEL CONDUIT FOR SERVICE TO CONTROLLER C-1 AND C-2 IN 1 1/4" GALVANIZED STEEL CONDUIT ATTACHED TO CONCRETE BRIDGE
 - PROPOSED LIGHT STANDARD AND 240W/175W METAL HALIDE LUMINAIRE, 35' H.P. AND 10' MAST ARM
 - PROPOSED LIGHT STANDARD AND LUMINAIRE WITH PHOTOCELL
 - PROPOSED CONCRETE HANDHOLE
 - PROPOSED JUNCTION BOX
 - PROPOSED LIGHTING CONTROLLER
 - CONTROLLER DESIGNATION
 - CIRCUIT AND POLE IDENTIFIER
 - LOCATION AND OFFSET
 - T TRUNCHED CONDUIT
 - P PUSHED CONDUIT
 - A.T.S CONDUIT ATTACHED TO STRUCTURE
 - PROPOSED COM ED SERVICE LOCATION
 - O-O EXISTING COM ED POLE WITH LIGHT
 - SANITARY
 - EXISTING SANITARY
 - EXISTING STORM SEWER
 - EXISTING WATERMAIN
 - OH EXISTING OVERHEAD WIRES
 - GI EXISTING GAS LINE

NOTES :

1. ALL EXISTING COM ED LIGHTING EQUIPMENT CALLED OUT TO BE REMOVED SHALL BE COORDINATED BY THE CITY OF NORTHLAKE PRIOR TO FINAL ACCEPTANCE OF THE LIGHTING SYSTEM.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
ROY AVENUE STREET LIGHTING PLAN (2 OF 6)
 DRAWN BY AJO
 CHECKED BY
 01/03/01

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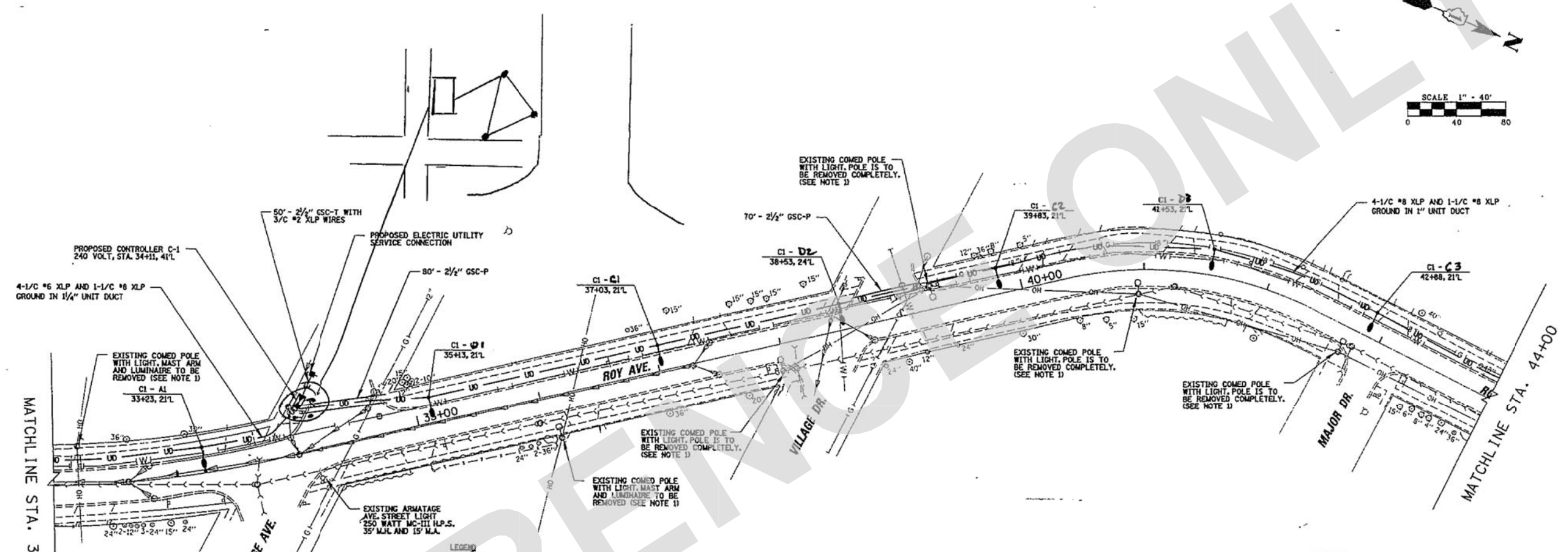
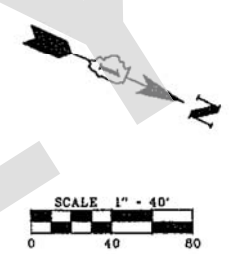
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PLOT SCALE = 1"	DRAWN - DOC	REVISED -
PLOT DATE = 11/17/2020	CHECKED - JMB	REVISED -
	DATE - 11/17/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:		SHEET	OF	SHEETS	STA.	TO	STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	13
CONTRACT NO. 61G88				ILLINOIS FED. AID PROJECT

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	14
STA.	TO STA.			
32+00	44+00			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



- LEGEND**
- U — PROPOSED 1 1/4" UNIT DUCT WITH 4-1/2" NO. 8 AND 1/2" NO. 8 COPPER GROUND WIRE OR 1 1/4" UNIT DUCT WITH 4-1/2" NO. 8 AND 1/2" NO. 8 COPPER GROUND WIRE
 - U — PROPOSED UNIT DUCT IN GALVANIZED STEEL CONDUIT 2 1/2" UNLESS OTHERWISE SPECIFIED
 - E — PROPOSED 3/4" NO. 2 XLP TYPE ELECTRIC CABLES IN 2 1/2" GALVANIZED STEEL CONDUIT FOR SERVICE TO CONTROLLER C-1 AND C-2 OR 1/2" NO. 8 XLP AND 1/2" NO. 8 XLP GROUND IN 1 1/4" GALVANIZED STEEL CONDUIT ATTACHED TO CONCRETE BRIDGE
 - L — PROPOSED LIGHT STANDARD AND 240W/175W METAL HALIDE LUMINAIRE, 30' M.H. AND 10' MAST ARM
 - L — PROPOSED LIGHT STANDARD AND LUMINAIRE WITH PARACELL
 - C — PROPOSED CONCRETE HANDHOLE
 - J — PROPOSED JUNCTION BOX
 - C — PROPOSED LIGHTING CONTROLLER
 - C — CONTROLLER DESIGNATION
 - CI - B1 — CIRCUIT AND POLE IDENTIFIER
 - CI - B1 — LOCATION AND OFFSET
 - T — TRENCHED CONDUIT
 - P — PUSHED CONDUIT
 - A.T.S. — CONDUIT ATTACHED TO STRUCTURE
 - ■ — PROPOSED COM ED SERVICE LOCATION
 - ○ — EXISTING COM ED POLE WITH LIGHT
 - — — — — EXISTING SANITARY SEWER
 - — — — — EXISTING STORM SEWER
 - — — — — EXISTING WATERMAIN
 - — — — — EXISTING OVERHEAD WIRES
 - — — — — EXISTING GAS LINE

NOTES:

1. ALL EXISTING COM ED LIGHTING EQUIPMENT CALLED OUT TO BE REMOVED SHALL BE COORDINATED BY THE CITY OF NORTHLAKE PRIOR TO FINAL ACCEPTANCE OF THE LIGHTING SYSTEM.



REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION ROY AVENUE STREET LIGHTING PLAN (3 OF 6)
NAME	DATE	
		DRAWN BY AJD CHECKED BY

01/03/01

DATE	BY
DATE	BY

CHRISTOPHER B. BURKE ENGINEERING LTD.
375 West Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0800

USER NAME = doconnell	DESIGNED - DOC	REVISED -
PLOT SCALE = 1"	CHECKED - JMB	REVISED -
PLOT DATE = 11/17/2020	DATE - 11/17/2020	REVISED -

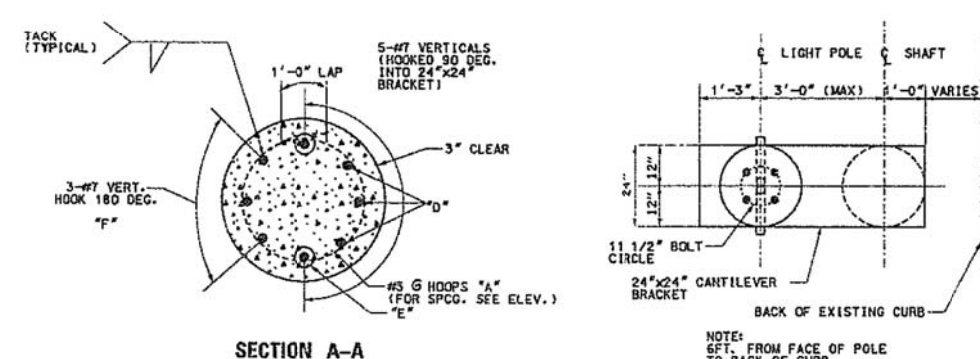
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.

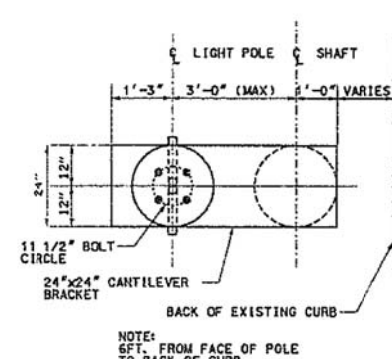
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CONTRACT NO. 61G88				
ILLINOIS FED. AID PROJECT				

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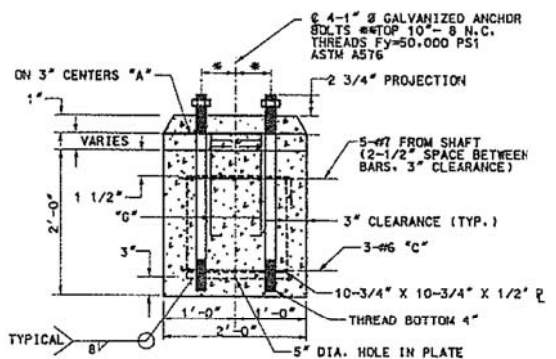
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STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



SECTION A-A
N.T.S.

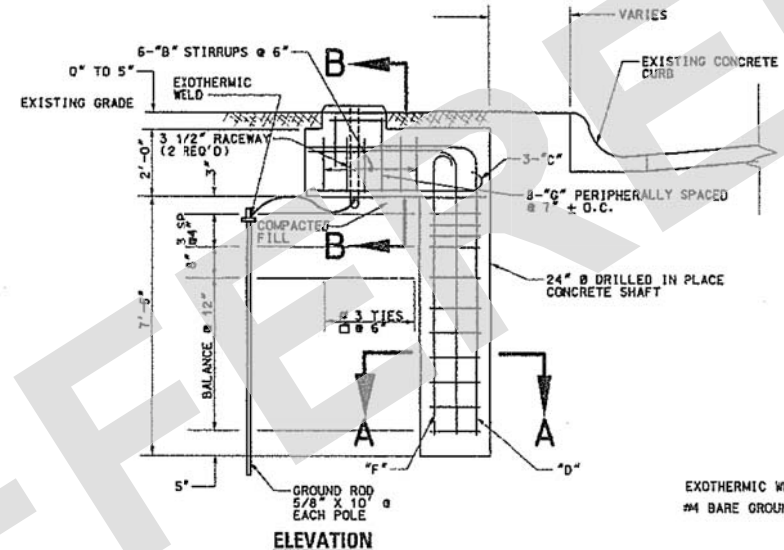


PLAN-CANTILEVERED BRACKET
N.T.S.

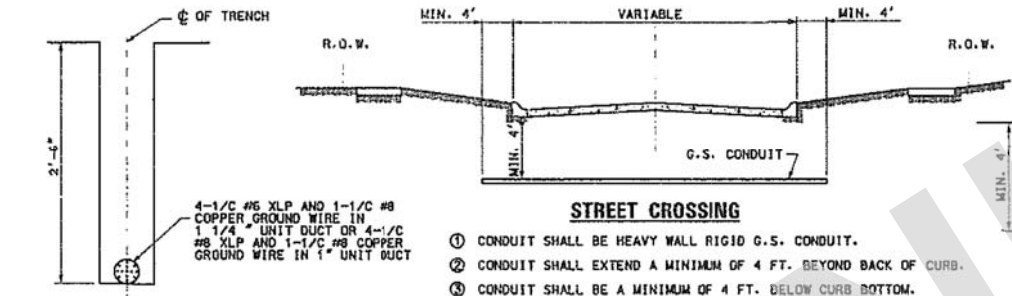


SECTION B-B
N.T.S.

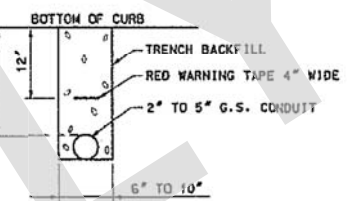
BILL OF MATERIALS				
QUAN.	MARK	SIZE	LENGTH	SHAPE
11	A	NO. 3	5'-9"	○
6	B	NO. 3	6'-8"	□
3	C	NO. 6	5'-3 1/2"	U
8	G	NO. 6	2'-1"	U
3	D	NO. 7	13'-6"	U
2	E	NO. 7	12'-0"	U
3	F	NO. 7	9'-11"	U
REINFORCING BARS LBS.			285	
CLASS X CONCRETE CU. YDS.			1.7	
ANCHOR BOLTS NO.			4	
ANCHOR BOLT PLATE NO.			1	



POLE FOUNDATION, SPECIAL DETAILS
N.T.S.



TRENCH CROSS SECTION
N.T.S.

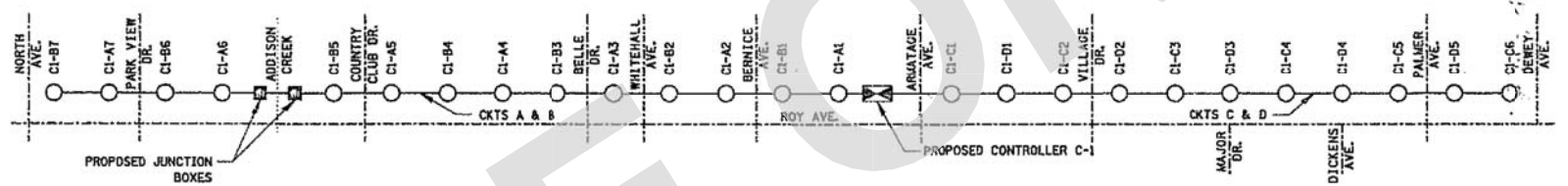


TRENCH SECTION
N.T.S.

STREET CROSSING

- ① CONDUIT SHALL BE HEAVY WALL RIGID G.S. CONDUIT.
- ② CONDUIT SHALL EXTEND A MINIMUM OF 4 FT. BEYOND BACK OF CURB.
- ③ CONDUIT SHALL BE A MINIMUM OF 4 FT. BELOW CURB BOTTOM.

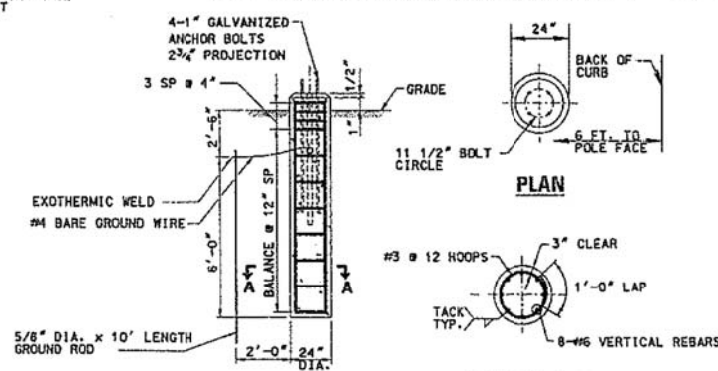
ELECTRIC CONDUIT UNDER PAVEMENT
N.T.S.



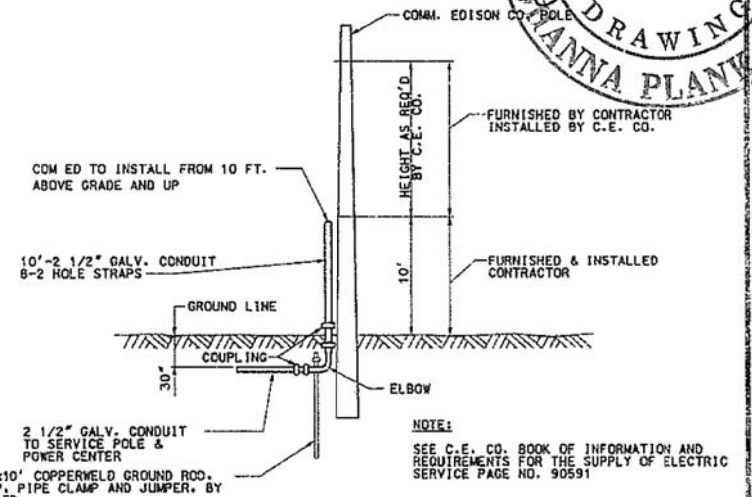
ONE LINE CIRCUIT DIAGRAM
N.T.S.

24 IN. DIA. FOUNDATION DEPTH TABLE
DESIGN BASE ON 80 M.P.H. AASHTO WIND LOADING

TYPE OF SOIL	FOUNDATION DEPTH D	REINFORCEMENT IN FOUNDATION	
		VERTICAL BARS	HORIZONTAL BARS
ROCK OR SOLIDIFIED SLAG	5'-0"	NONE	NONE
DENSE SAND	7'-9"	8-#6 x 7'-4"	#3 x 73'
MEDIUM SAND	8'-3"	8-#6 x 7'-10"	#3 x 78'
LOOSE SAND	9'-0"	8-#6 x 8'-7"	#3 x 85'
STIFF CLAY	7'-0"	8-#6 x 6'-7"	#3 x 66'
MEDIUM CLAY	9'-6"	8-#6 x 9'-1"	#3 x 90'
SOFT CLAY	13'-0"	8-#6 x 12'-7"	#3 x 122'



CONCRETE FOUNDATION DETAIL
N.T.S.



C.E. CO. OVERHEAD CONNECTION POLE
N.T.S.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
ROY AVENUE STREET LIGHTING
SUMMARY OF QUANTITIES
DETAILS - 2

01/03/01
DRAWN BY AJD
CHECKED BY

MODEL: D:\64bit...
 FILE NAME: N:\NORTHLAKE\940023DC167\CI\UTIL\05_940023DC167_51T
 CHRISTOPHER B. BURKE ENGINEERING LTD. PLAN
 9575 West Higgins Road, Suite 600
 Rosemont, Illinois 60018
 (847) 823-0500

Benchmark: OSBM 1 Railroad spike (recovered) in south face of power pole with light at the northeast corner of North Avenue and Prater Avenue. Elevation 645.77

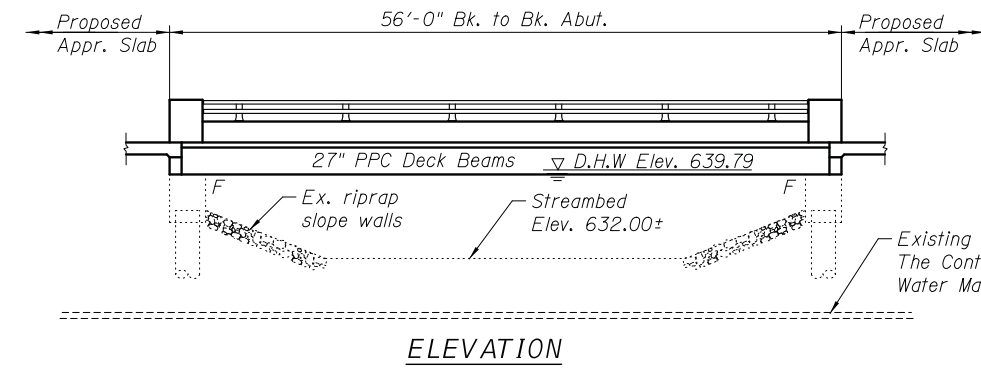
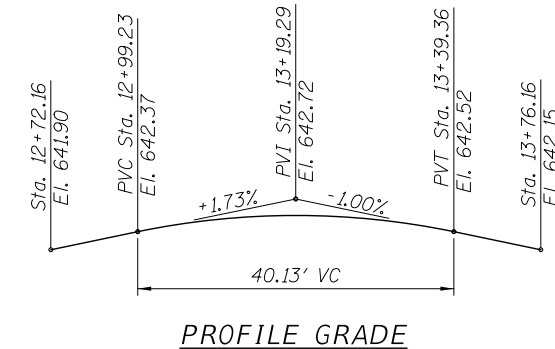
Existing Structure:
SN. 016-7612 was constructed in 1966. The bridge is a single span structure with a length 56'-0" back to back of abutments and has no skew. The superstructure consists of 13-27"x36" PPC deck beams with bituminous overlay and has a total deck width of 40'-0". The deck provides two lanes of traffic with a 4'-6" wide sidewalk with a pipe handrail type railing mounted to the top of the sidewalk. The bridge will be closed during construction and traffic detoured on local routes. No stage construction. The substructure will be reused.

Salvage: None

Traffic: Traffic will be detoured during construction.

WATERWAY INFORMATION

Flood		Q	Opening	Sq. Ft.	Nat.	Head - Ft.	Headwater El.
Yr.	Freq.	C.F.S.	Exist.	Prop.	H.W.E.	Exist.	Prop.
10	236	216.5	216.5	638.89	0.00	0.00	638.89
30	337	236.3	236.3	639.79	0.02	0.02	639.81
100	505	236.3	236.3	640.46	0.06	0.06	640.52
>500							
Max. Calc.	500	860	236.3	236.3	641.20	0.05	0.05



DESIGN SCOUR ELEVATION TABLE

Design	Design Scour Elevations (ft.)		Item
	N. Abut.	S. Abut.	
Q100	638.3	638.4	113
Q200	638.3	638.4	5
Design	638.3	638.4	

INDEX OF SHEETS

- S-1 General Plan and Elevation
- S-2 General Notes and Total Bill of Materials
- S-3 Deck Plan and Cross Section
- S-4 Superstructure Details
- S-5 Aluminum Railing, Type L
- S-6 27"x36" PPC Deck Beam Details (1 of 2)
- S-7 27"x36" PPC Deck Beam Details (2 of 2)
- S-8 27"x48" PPC Deck Beam Details (1 of 2)
- S-9 27"x48" PPC Deck Beam Details (2 of 2)
- S-10 Abutment Plan, Sections and Details (1 of 2)
- S-11 Abutment Plan, Sections and Details (2 of 2)
- S-12 Approach Slab Details (1 of 2)
- S-13 Approach Slab Details (2 of 2)

DESIGN SPECIFICATIONS

2017 AASHTO LRFD Bridge Design Specifications, 8th Edition with Interims

DESIGN STRESSES

FIELD UNITS (New Construction)

$f'_c = 3,500$ psi
 $f_y = 60,000$ psi (Reinforcement)

PRECAST PRESTRESSED UNITS

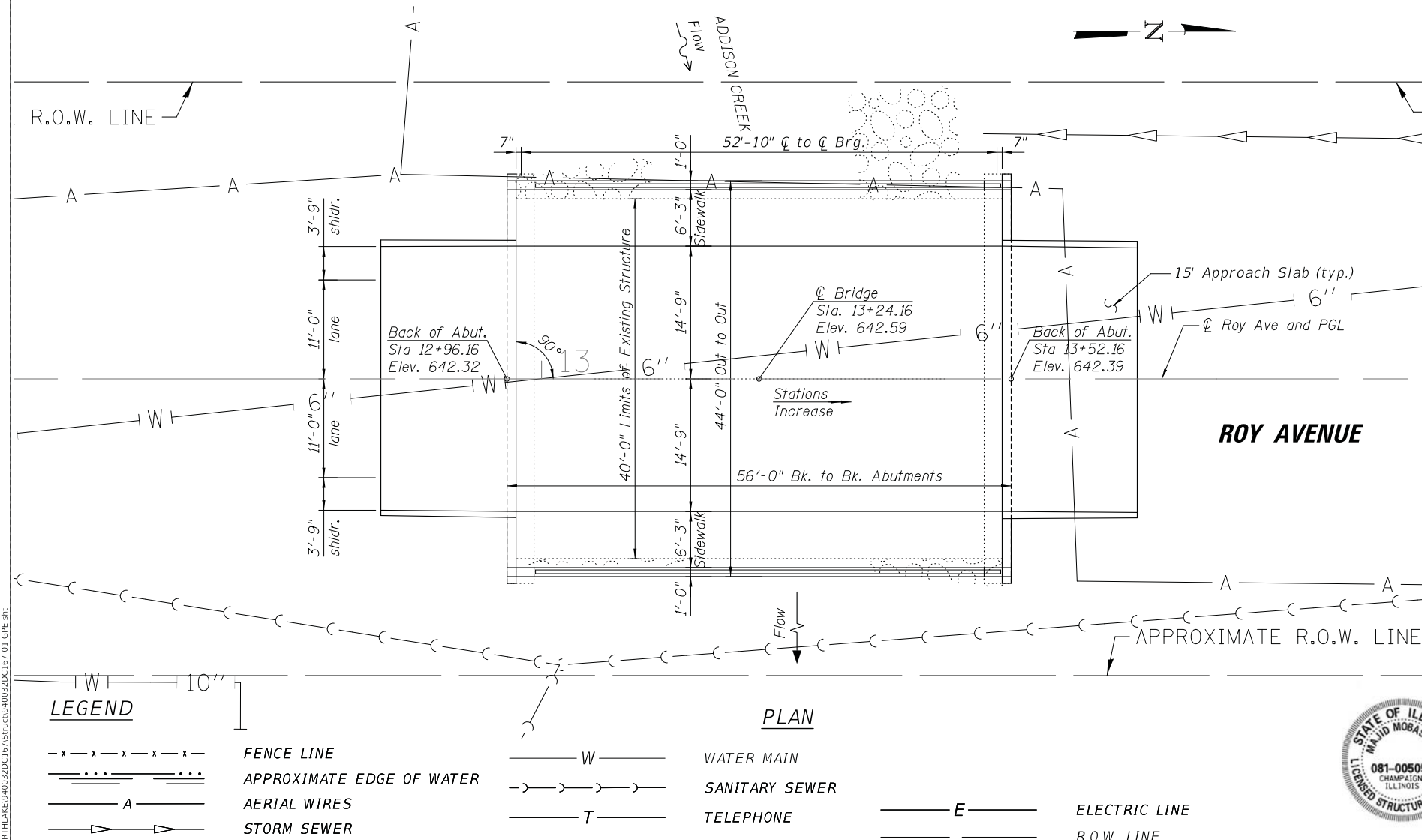
$f'_c = 6,000$ psi
 $f'_{ci} = 5,000$ psi
 $f_{pu} = 270,000$ psi (1/2" low relax. strands)
 $f_{pb} = 201,960$ psi (1/2" low relax. strands)

SEISMIC DATA

Seismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec. (SD1) = 0.101g
Design Spectral Acceleration at 0.2 sec. (SDS) = 0.131g

LOADING HL-93

Allow 50#/sq. ft. for future wearing surface.



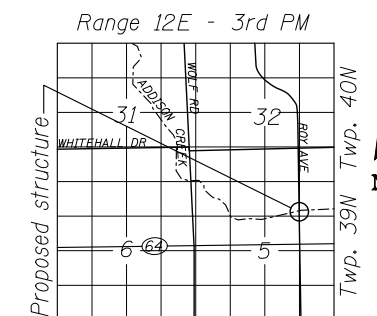
LEGEND

- x-x-x-x-x- FENCE LINE
- APPROXIMATE EDGE OF WATER
- A-A-A- AERIAL WIRES
- >->->- STORM SEWER
- W- WATER MAIN
- S-S-S- SANITARY SEWER
- T- TELEPHONE
- E- ELECTRIC LINE
- - - - - R.O.W. LINE

ADDISON CREEK
RE-BUILT 2021 BY
CITY OF NORTHLAKE
SECTION 14-00087-00-BR
STATION 13+24.16
STR. NO. 016-7612 LOADING HL-93

NAME PLATE 1 DETAIL

Note:
Existing Name Plate shall be cleaned and relocated next to new Name Plates.
Cost included with Name Plates.
See Sheet S-2 for Name Plate 2 Detail



GENERAL PLAN

ROY AVENUE OVER
ADDISON CREEK
SECTION 14-00087-00-BR
COOK COUNTY
STA. 13+24.16
STRUCTURE No. 016-7612

I Certify That To The Best Of My Knowledge, Information And Belief, This Bridge Design Is Structurally Adequate For The Design Loading Shown On The Plans. The Design Is An Economical One For The Style Of Structure And Complies With Requirements Of The Current "AASHTO Standard Specification For Highway And Bridges".

Majid Mobasseri 11/17/20
MAJID MOBASSERI
ILLINOIS REGISTRATION No. 081-005058 STRUCTURAL ENGINEER
EXPIRATION DATE: 11/30/22



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION
ROY AVENUE OVER ADDISON CREEK
STRUCTURE NO. 016-7612

SCALE: N.T.S. SHEET S-1 OF 5-13 SHEETS STA. 13+24.16 TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	16
CONTRACT NO. 61G88			ILLINOIS FED. AID PROJECT	

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CHRISTOPHER B. BURKE ENGINEERING, LTD.
3075 W. Higgins Road, Suite 400
Rosemont, Illinois 60018
(847) 823-0500

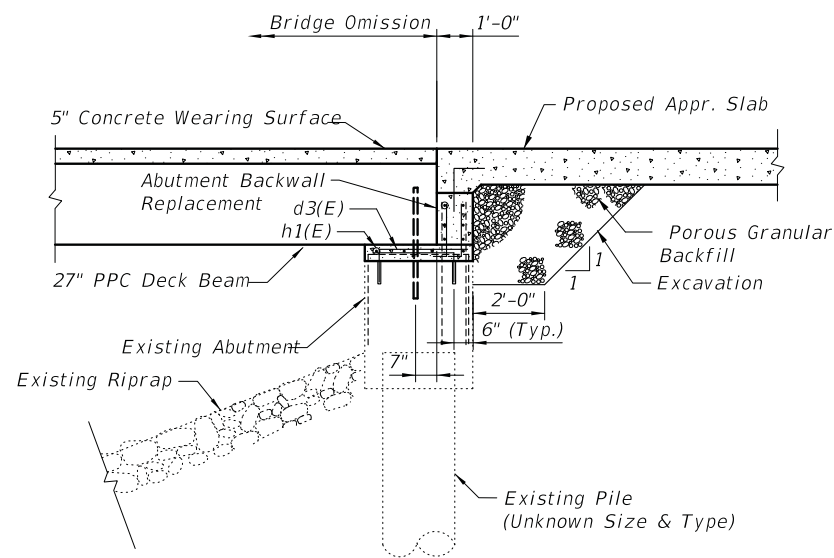
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= doconnell	- JMB	-
	DRAWN - PR	REVISIONS -
PLOT SCALE = 8"	CHECKED - MM	REVISIONS -
PLOT DATE = 11/17/2020	DATE - 11/17/2020	REVISIONS -

GENERAL NOTES

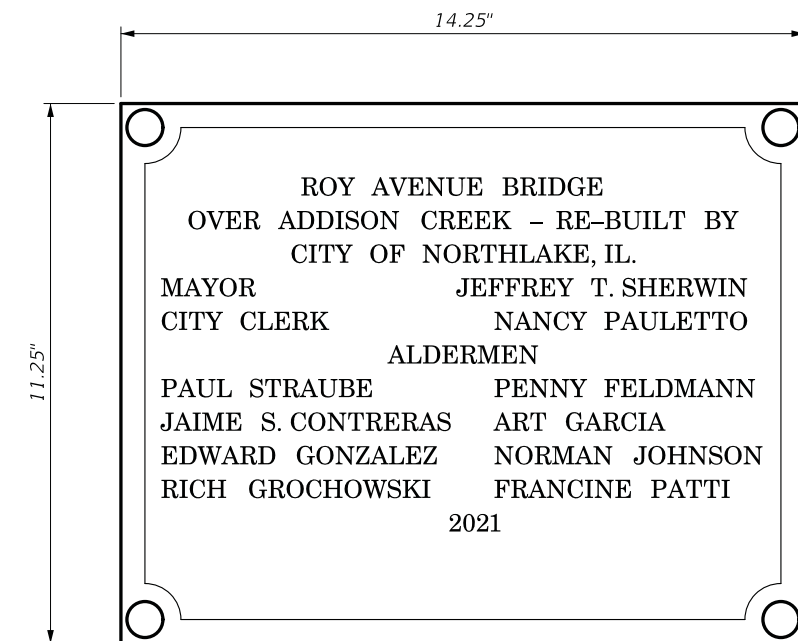
1. Reinforcement bars designated (E) shall be epoxy coated.
2. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contactor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work; however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
3. Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.
4. Existing Name Plate shall be cleaned and relocated next to new Name Plate. Cost included with Name Plates.
5. Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.
6. Installation of concrete cap dowels to be included in "Concrete Structures".
7. See Special Provisions for form liner textured surface pattern and staining information. The pattern and stain shall be submitted to the City for review and approval.

TOTAL BILL OF MATERIAL

PAY ITEM	ITEM	UNIT	QUANTITY
50101500	Removal of Existing Superstructure	Each	1
50102400	Concrete Removal	Cu. Yd.	16.7
50200100	Structure Excavation	Cu. Yd.	45
50300225	Concrete Structures	Cu. Yd.	27.2
50300255	Concrete Superstructure	Cu. Yd.	38.3
50300260	Bridge Deck Grooving	Sq. Yd.	185
50300300	Protective Coat	Sq. Yd.	345
50301350	Concrete Superstructure (Approach Slab)	Cu. Yd.	43.5
50400505	Precast Prestressed Concrete Deck Beams (27" Depth)	Sq. Ft.	2,322
50800205	Reinforcement Bars, Epoxy Coated	Pound	29,410
50900105	Aluminum Railing, Type L	Foot	100
51500100	Name Plate	Each	2
58600101	Granular Backfill for Structures	Cu. Yd.	145
X1700034	Form Liner Textured Surface, Special	Sq. Ft.	525
X5030305	Concrete Wearing Surface, 5"	Sq. Yd.	258



ABUTMENT SECTION - PROPOSED



NAME PLATE 2 DETAIL

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CB CHRISTOPHER B. BURKE ENGINEERING, LTD.
 9075 W. Higgins Road, Suite 400
 Rosemont, Illinois 60018
 (847) 823-0500

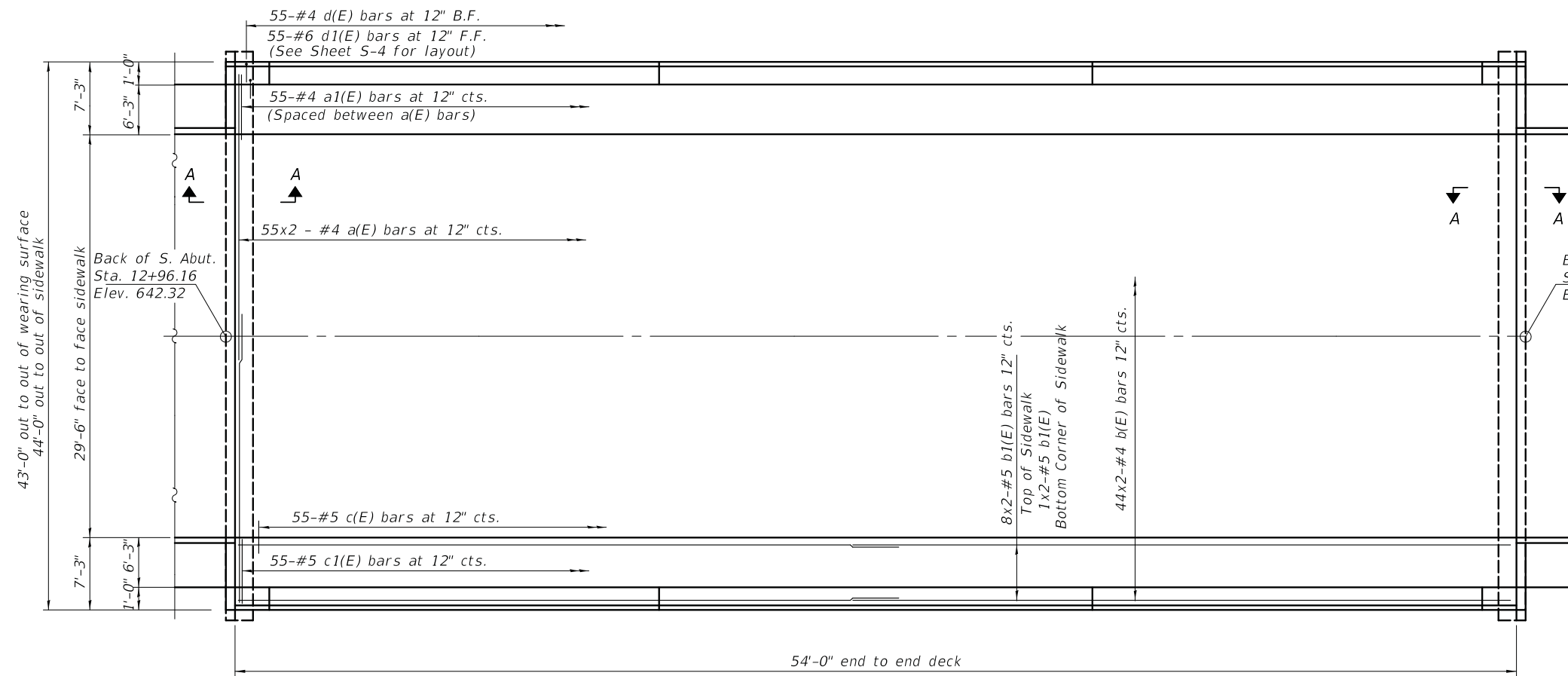
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PLOT DATE = 11/17/2020	DATE - 11/17/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

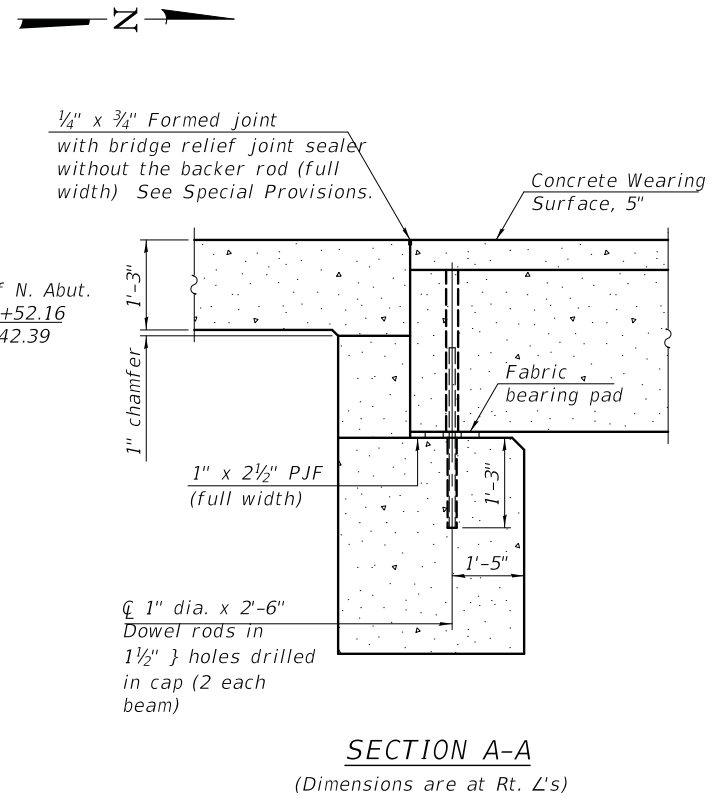
**GENERAL NOTES AND TOTAL BILL OF MATERIALS
 ROY AVENUE OVER ADDISON CREEK
 STRUCTURE NO. 016-7612**

SCALE: N.T.S. SHEET S-2 OF 5-13 SHEETS STA. 13+24.16 TO STA.

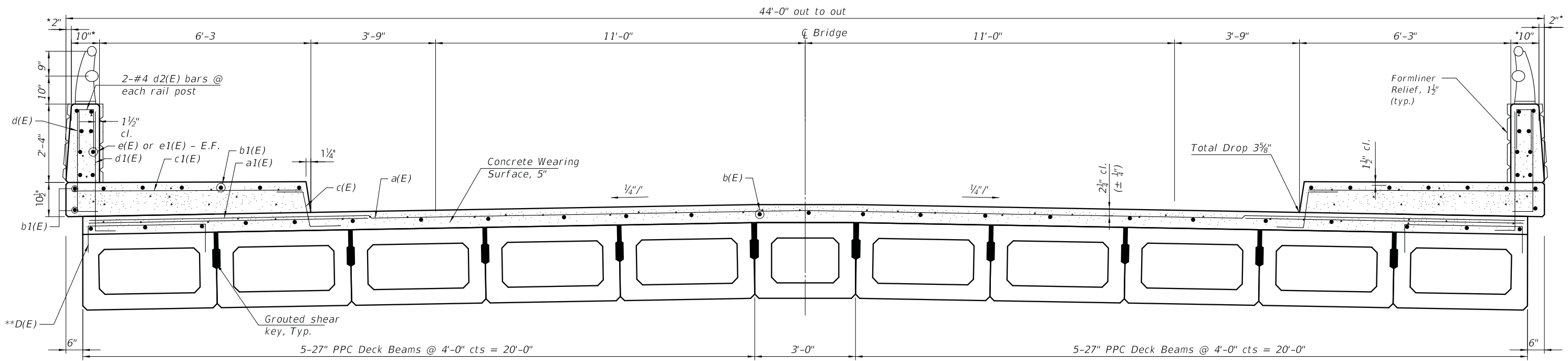
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	17
				CONTRACT NO. 61G88
		ILLINOIS	FED. AID PROJECT	



PLAN



SECTION A-A
(Dimensions are at Rt. L's)



CROSS SECTION
(Looking North)

** Place #4 D(E) bars at 9" cts. in fascia beam.
D(E) bar included in cost of beam.

Notes:
See sheet S7 and S9 for Bearing Pad Details.
See sheet S4 of S13 for Superstructure Details
and Bill of Material.
Bars indicated thus 20 x 2-#4 etc. indicates
20 lines of bars with 2 lengths per line.

MINIMUM BAR LAP
#4 bar = 2'-7"
#5 bar = 3'-2"

* Dimensions are for Barrier Only. Add 1 1/2" for
Formliner Relief to Each Side of Barrier.

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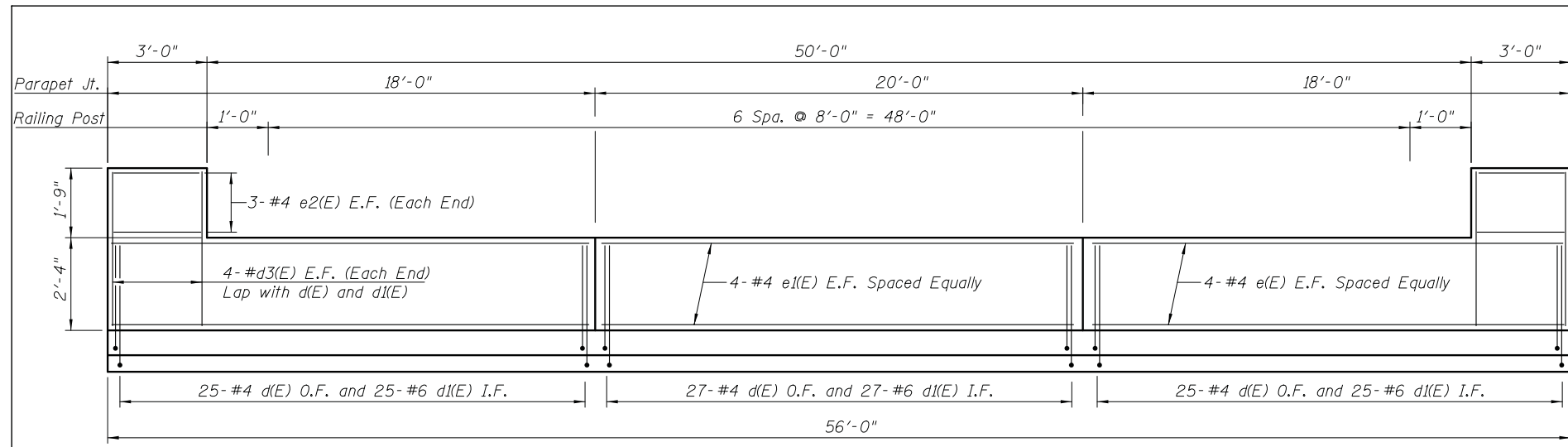
CB CHRISTOPHER B. BURKE ENGINEERING, LTD.
3075 W. Higgins Road, Suite 400
Rosemont, IL 60018
(847) 823-0500

USER NAME = doconnell	DESIGNED - JMB	REVISED -
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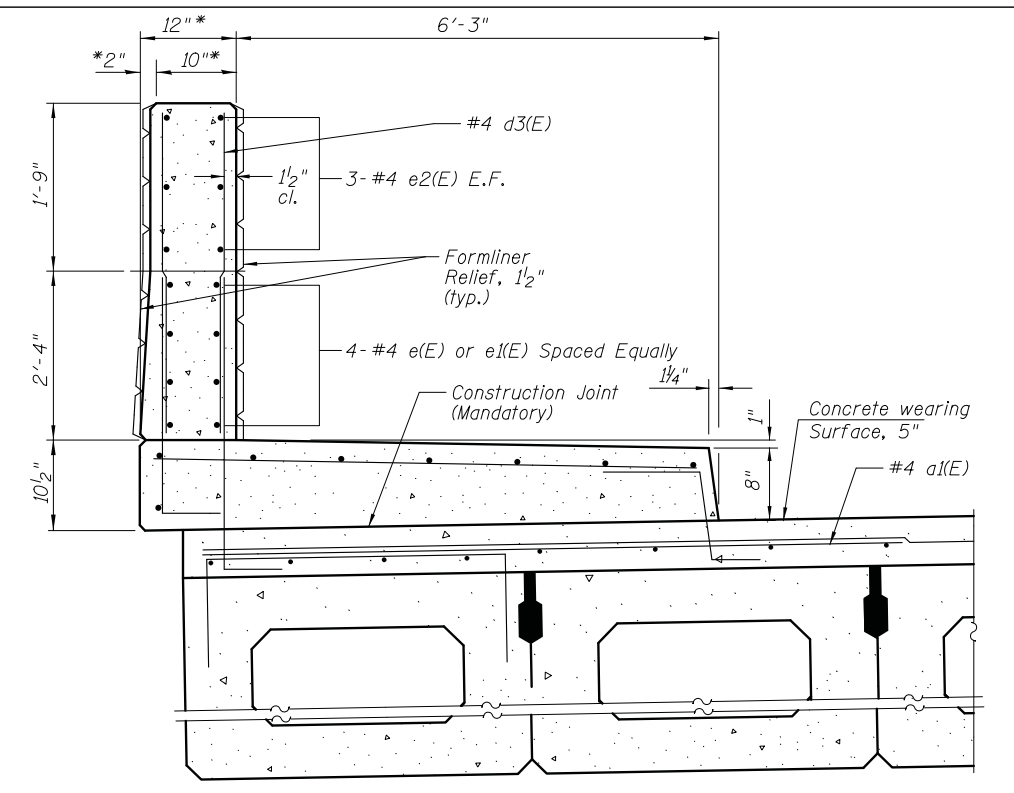
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK PLAN AND CROSS SECTION
ROY AVENUE OVER ADDISON CREEK
STRUCTURE NO. 016-7612
SCALE: N.T.S. SHEET S-3 OF 5-13 SHEETS STA. 13+24.16 TO STA.

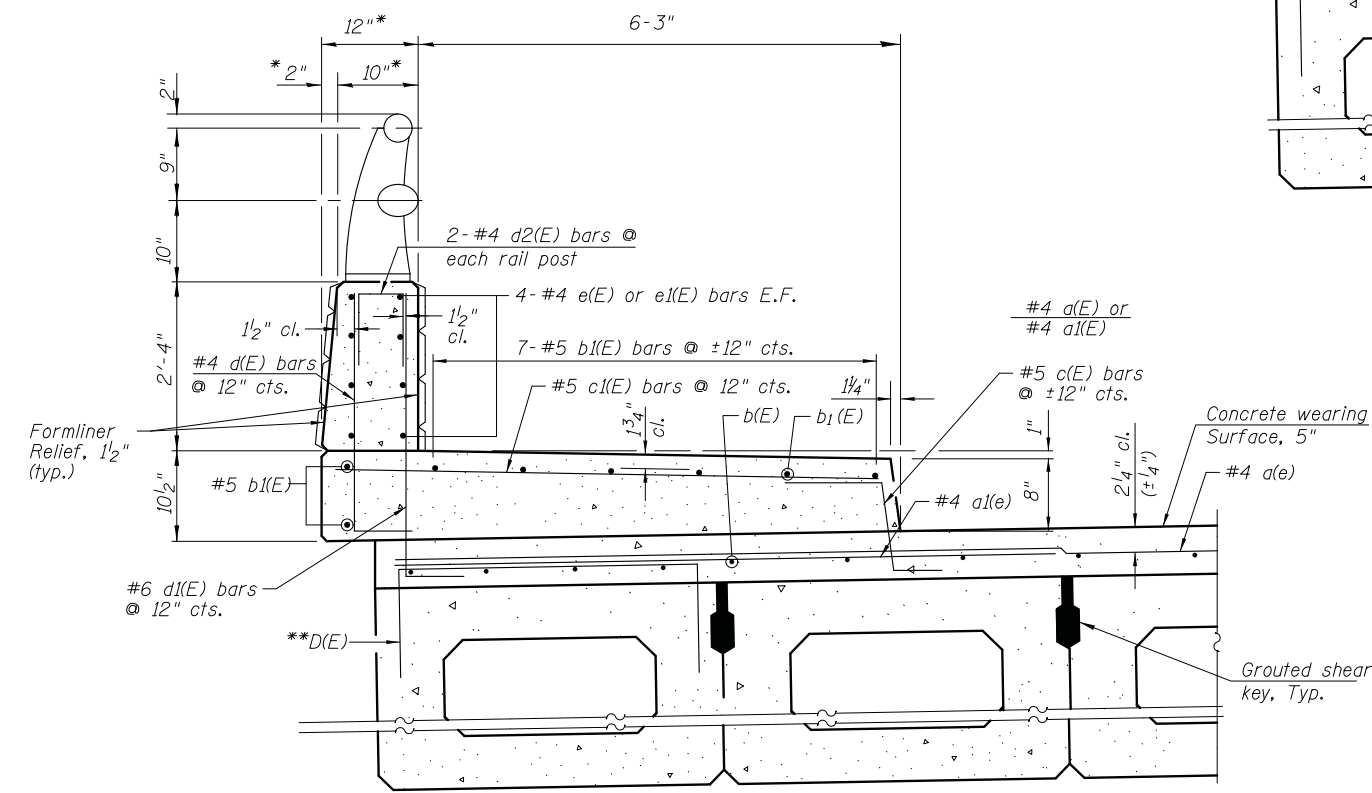
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2539	14-00087-00-BR	COOK	33	18
CONTRACT NO. 61G88				
ILLINOIS FED. AID PROJECT				



BRIDGE SIDEWALK PARAPET ELEVATION

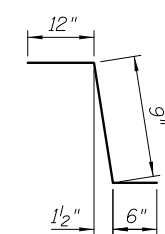


SECTION THRU PARAPET AT END POST

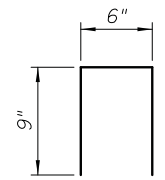


SECTION THRU PARAPET

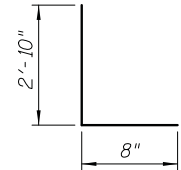
- * Dimensions are for Barrier Only. Add 1 1/2" for Formliner Relief to Each Side of Barrier.
- ** Place #4 D(E) bars at 9" cts. in fascia beam. D(E) bar included in cost of beam.



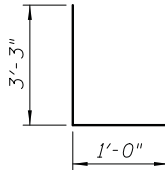
BAR C(E)



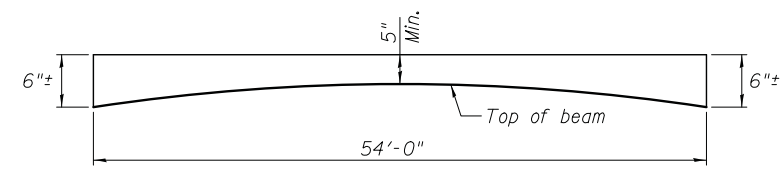
BAR d2(E)



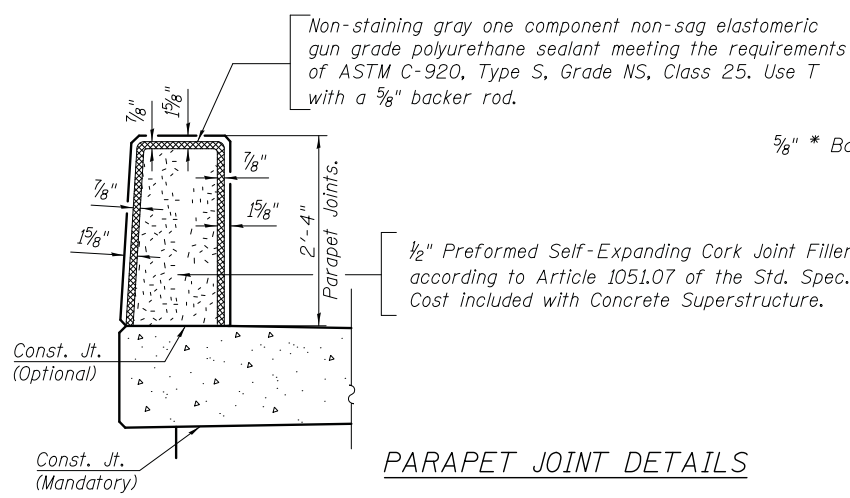
BAR d(E)



BAR d1(E)



ANTICIPATED CONCRETE WEARING SURFACE PROFILE (For information only)



PARAPET JOINT DETAILS

SUPERSTRUCTURE BILL OF MATERIAL

Bar	No.	Size	Length	Shape	
a(E)	110	#4	23'-4"	—	
a1(E)	110	#4	7'-4"	—	
b(E)	88	#4	28'-4"	—	
b1(E)	36	#5	28'-7"	—	
c(E)	110	#5	2'-3"	┘	
c1(E)	110	#5	6'-9"	—	
d(E)	110	#4	3'-6"	L	
d1(E)	110	#6	4'-3"	L	
d2(E)	28	#4	2'-0"	□	
d3(E)	32	#4	3'-10"	—	
e(E)	32	#4	17'-8"	—	
e1(E)	16	#4	19'-8"	—	
e2(E)	24	#4	2'-8"	—	
Reinforcement Bars, Epoxy Coated				Pound	7,730
Concrete Superstructure				Cu. Yd.	34.5
Concrete Wearing Surface, 5"				Sq. Yd.	268

Bars indicated thus 1 x 2 - #4 etc. indicates 1 line of bars with 2 lengths per line.

MODEL: D:\c\p\... FILE NAME: N:\NORTH LAKE\940032DC167151\167151\940032DC167151-04-Suprc_02.rvt

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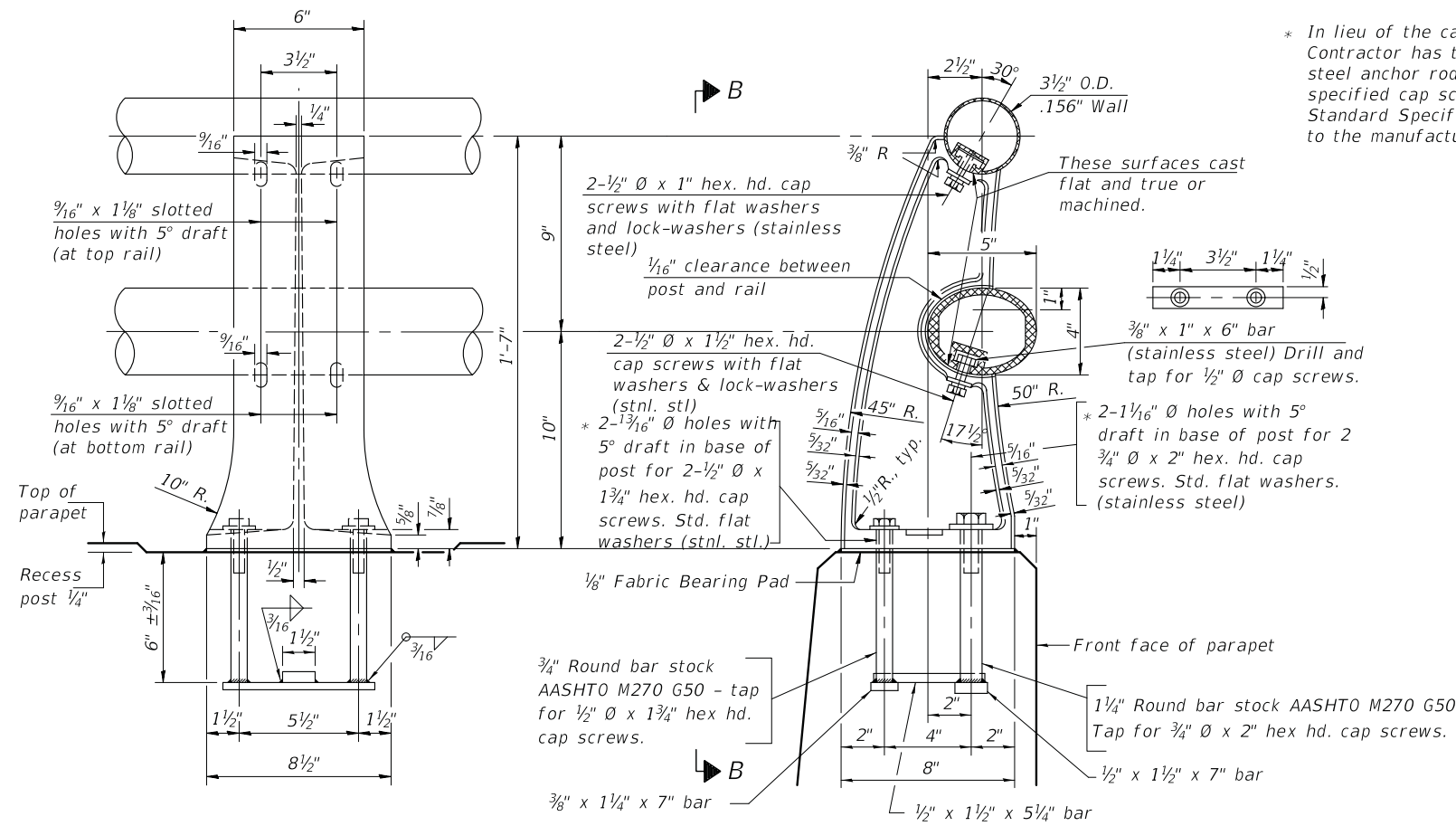
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PLOT SCALE = 8"	DRAWN - PR	REVISED -
PLOT DATE = 11/17/2020	CHECKED - MM	REVISED -
	DATE - 11/17/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

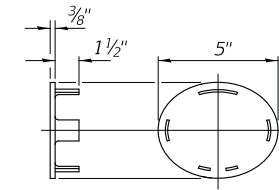
SUPERSTRUCTURE DETAILS
ROY AVENUE OVER ADDISON CREEK
STRUCTURE NO. 016-7612

SCALE: N.T.S. SHEET 5-4 OF 5-13 SHEETS STA. 13+24.16 TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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				CONTRACT NO. 61G88
				ILLINOIS FED. AID PROJECT

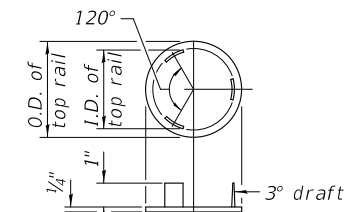


* In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting stainless steel anchor rods of the same diameter and grade as the specified cap screws according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.



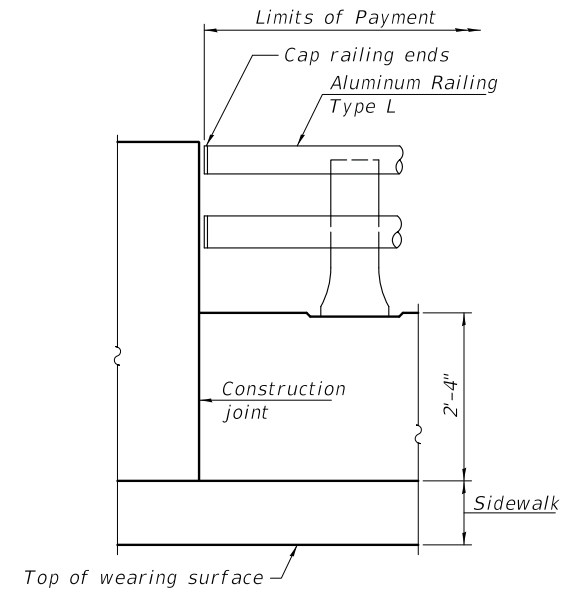
CAST END CAP

For bottom rail
DRIVE FIT TYPE



CAST END CAP

For top rail

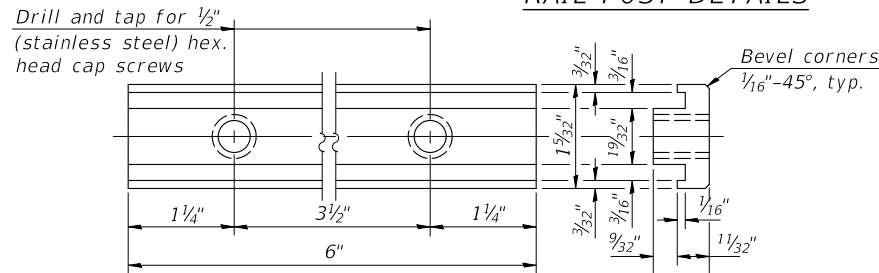


RAIL END TREATMENT

VIEW B-B

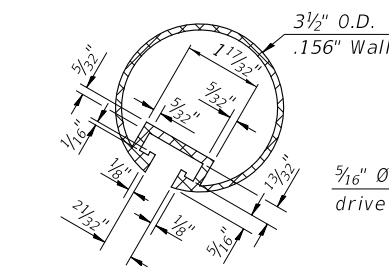
SECTION A-A

RAIL POST DETAILS

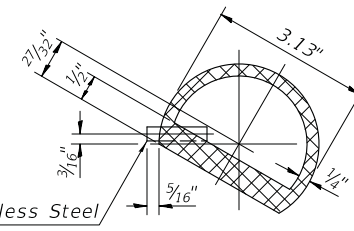


RAIL POST CLAMP BAR

For Top Rail

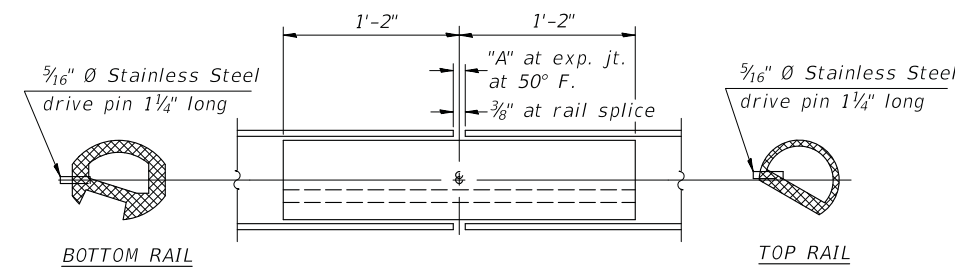


SECTION THRU TOP RAIL



SECTION THRU SPLICE

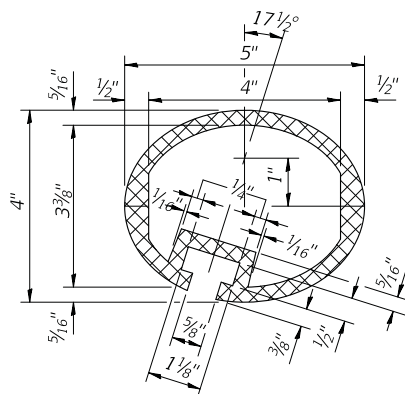
For Top Rail



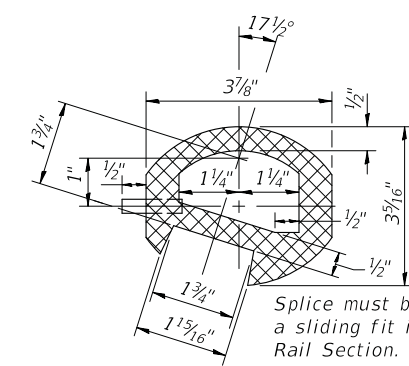
RAIL SPLICE

T	"A"
≤ 4"	2 1/2"
> 4" ≤ 6 1/2"	3 3/4"
> 6 1/2" ≤ 9"	5"
> 9" ≤ 13"	7"

T = Total movement at expansion joint as shown on the design plans.



SEC. THRU ELLIPTICAL RAIL SECTION



SEC. THRU SPLICE

Notes:
All Posts shall be normal to parapet.
All joints in rail shall be spliced per detail.
All exposed rail ends shall be capped per detail.
Provide 1-1/8" and 2-1/16" Aluminum Shims for 25% of the Posts. Rail elements shall be parallel to Grade-high spots will be ground and low spots shimmed.
See sheet S-4 for rail post spacing.

BILL OF MATERIAL

Item	Unit	Quantity
Aluminum Railing, Type L	Foot	100

MODEL: D:\c\p\11... FILE NAME: N:\NORTHLAKE\940032DC167\STRUCT\940032DC167-05-Railing.dwg

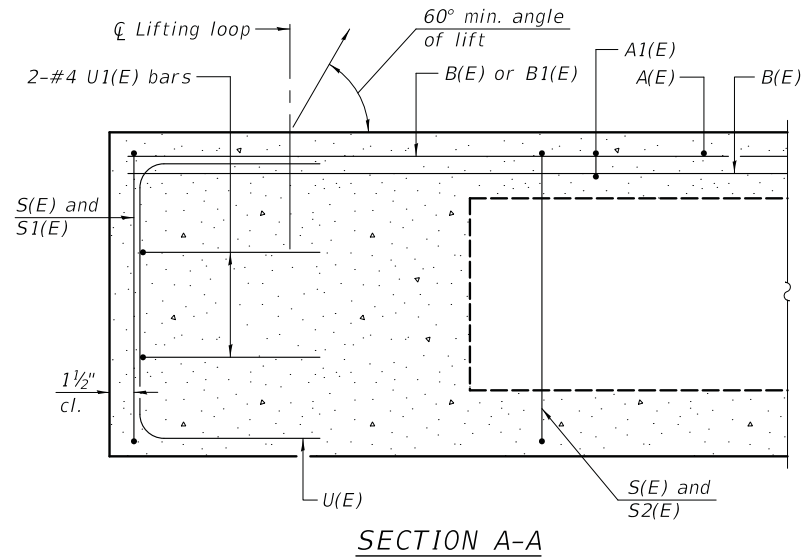
CB CHRISTOPHER B. BURKE ENGINEERING, LTD.
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Rosemont, IL 60018
(847) 823-0500

USER NAME = doconnell	DESIGNED - JMB	REVISED -
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PLOT DATE = 11/17/2020	CHECKED - MM	REVISED -
	DATE - 11/17/2020	REVISED -

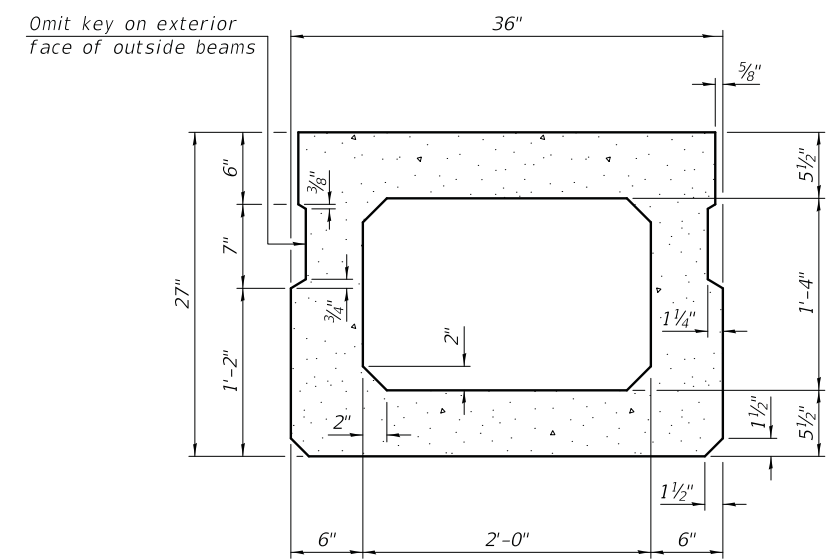
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ALUMINUM RAILING, TYPE L ROY AVENUE OVER ADDISON CREEK STRUCTURE NO. 016-7612			
SCALE: N.T.S.	SHEET S-5	OF 5-13 SHEETS	STA. 13+24.16 TO STA.

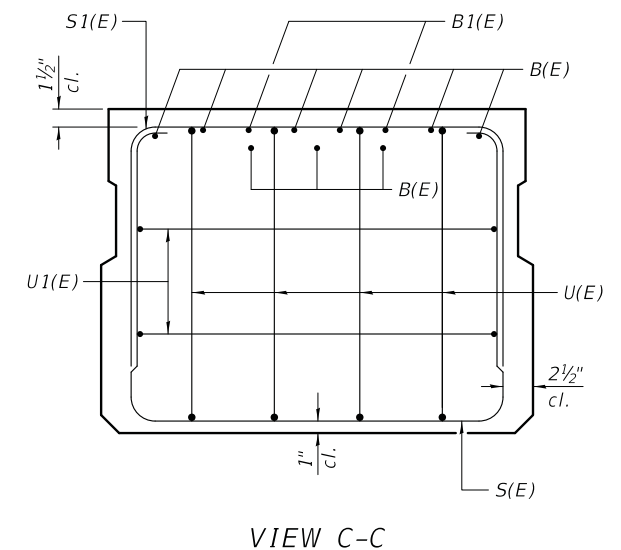
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	20
				CONTRACT NO. 61G88
				ILLINOIS FED. AID PROJECT



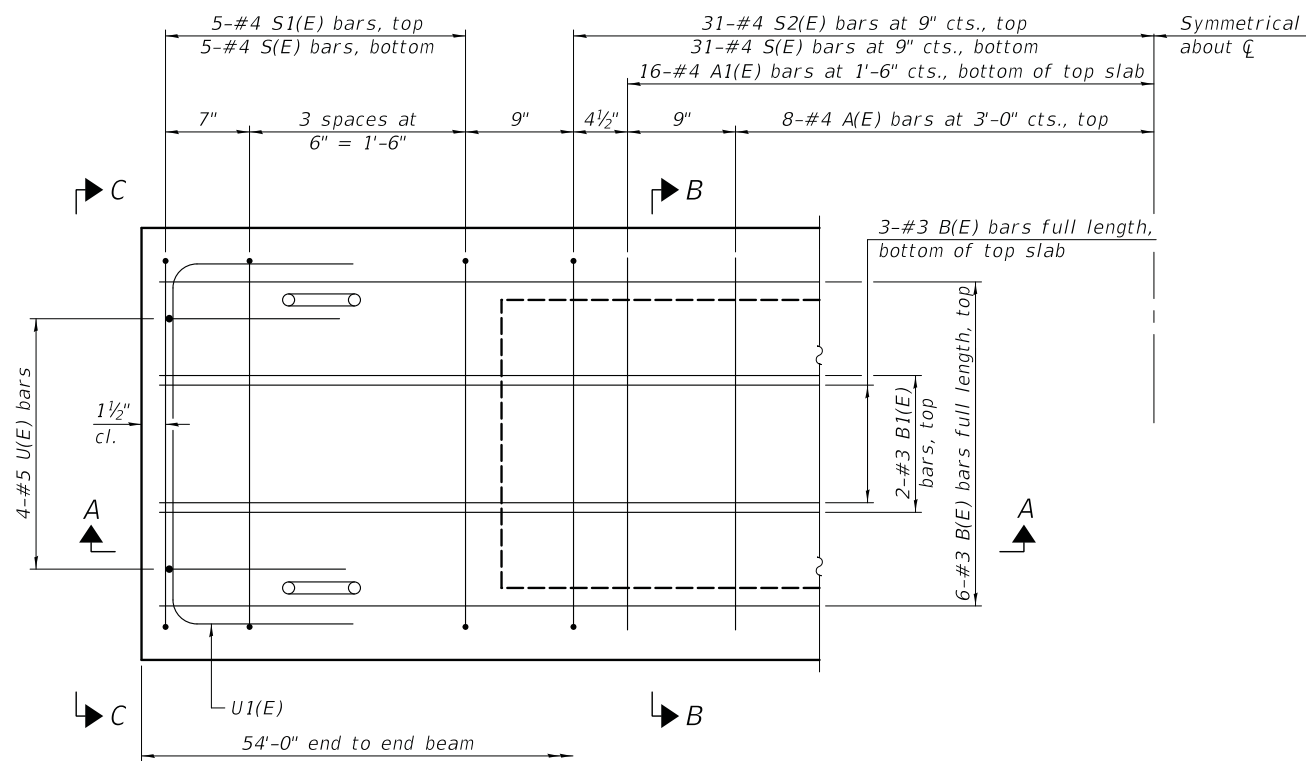
SECTION A-A



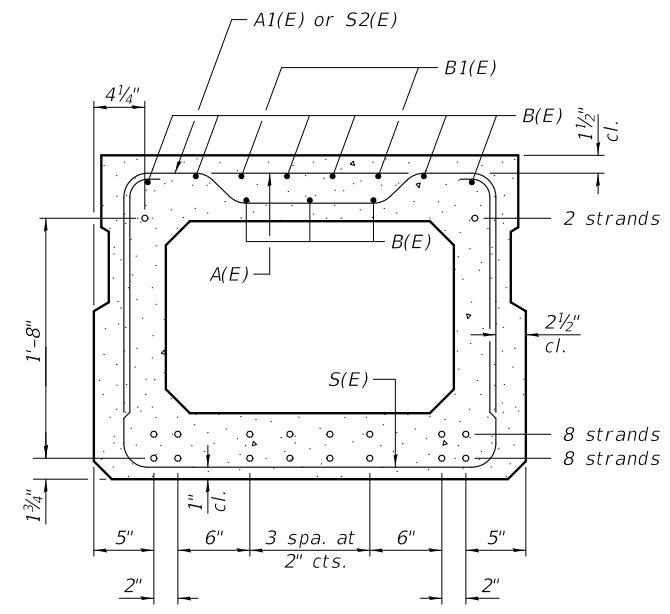
SECTION B-B
(Showing dimensions)



VIEW C-C



PLAN VIEW



SECTION B-B
(Showing reinforcement and permissible strand locations)

Note: Place the number of strands specified in each row symmetrically about the centerline of beam in the permissible strand locations shown.

BAR LIST
ONE BEAM ONLY
(For information only)

Bar	No.	Size	Length	Shape
A(E)	16	#4	2'-7"	—
A1(E)	32	#4	2'-10"	~
B(E)	18	#3	27'-8"	—
B1(E)	4	#3	10'-0"	—
S(E)	72	#4	7'-5"	⌊
S1(E)	10	#4	5'-11"	⌊
S2(E)	62	#4	6'-2"	⌊
U(E)	8	#5	4'-6"	⌊
U1(E)	4	#4	5'-0"	⌊

MINIMUM BAR LAP
#3 bar = 1'-6"

Note:
Spacing of S(E) and S2(E) bars may be adjusted up to 4" in the immediate area of the transverse tie diaphragms to miss the block outs for the transverse ties.

MODEL: D:\c\p1\... FILE NAME: N:\NORTH LAKE\940032DC167\STRUCT\940032DC167-06-36-PPC Deck Beam 01.rvt

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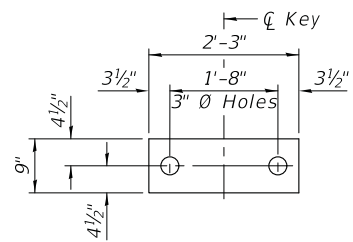
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

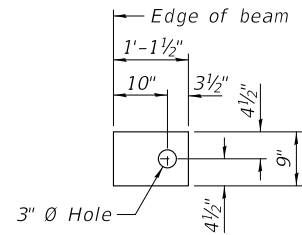
27"x36" PPC DECK BEAM DETAILS (1 OF 2)
ROY AVENUE OVER ADDISON CREEK
STRUCTURE NO. 016-7612

SCALE: N.T.S. SHEET S-6 OF 5-13 SHEETS STA. 13+24.16 TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	21
CONTRACT NO. 61G88				
ILLINOIS FED. AID PROJECT				



FABRIC BEARING PAD
(Interior)

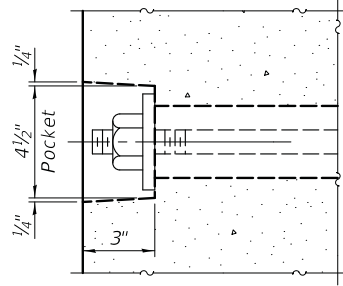


FABRIC BEARING PAD
(Exterior)

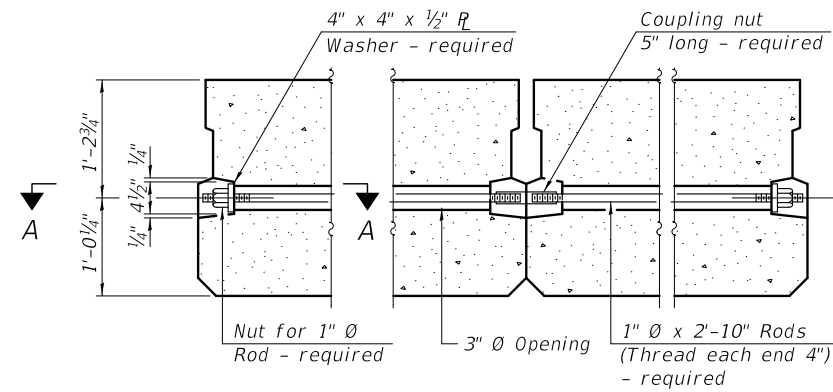
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Notes:

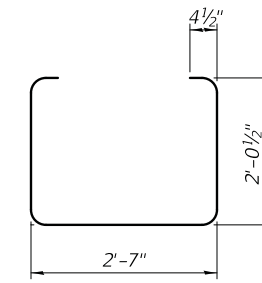
- All bearing pads shall be 1" thick.
- Omit holes when using expansion bearings.
- Expansion bearing pads shall be bonded to the substructure.



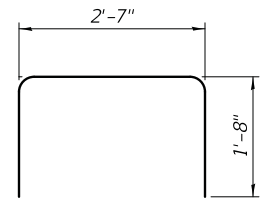
SECTION A-A



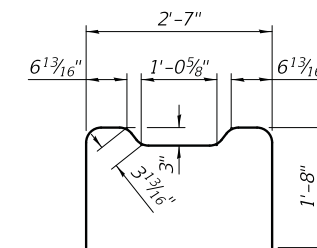
TYPICAL TRANSVERSE TIE ASSEMBLY



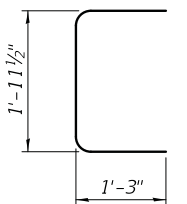
BAR S(E)



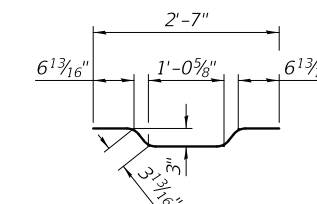
BAR S1(E)



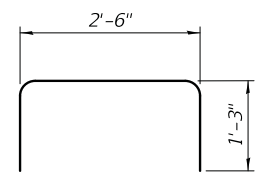
BAR S2(E)



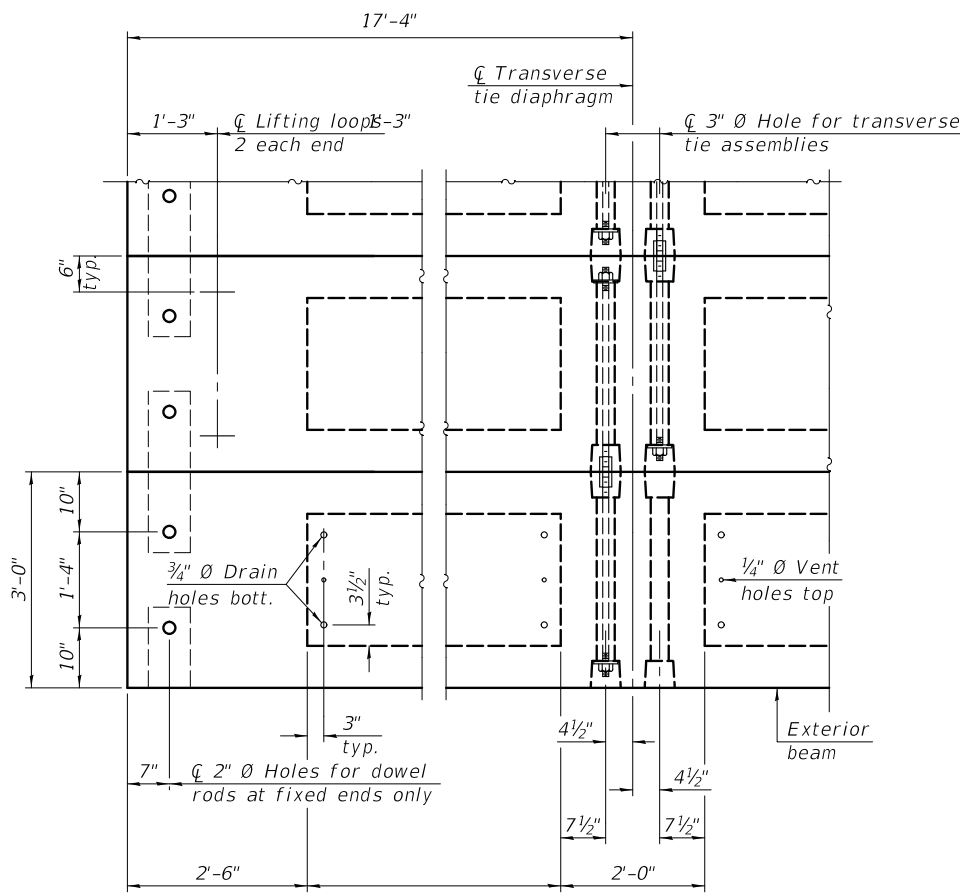
BAR U(E)



BAR A1(E)



BAR U1(E)

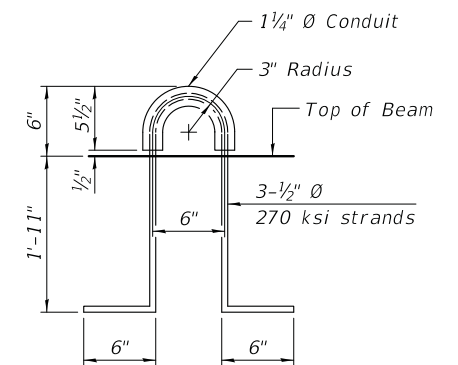


PLAN VIEW

Note:
Connect beams in pairs with the transverse tie configuration shown.

NOTES

- Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.153 sq. in.
- The 1" diameter rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets on exterior faces of bridge shall be filled with grout after transverse tie assembly is in place.
- Two 1/8" fabric adjusting shims of the dimensions of the exterior bearing pad shall be provided for each bearing pad location.
- A minimum 2 1/2" diameter lifting pin shall be used to engage the lifting loops during handling.
- Corrosion Inhibitor, per Article 1020.05(b)(10) and 1021.07 of the Standard Specifications, shall be used in the concrete for precast prestressed concrete deck beams.
- Compressive strength of prestressed concrete, f'c, shall be 6000 psi.
- Compressive strength of prestressed concrete at release, f'ci, shall be 5000 psi.



LIFTING LOOP DETAIL

BILL OF MATERIAL

Precast Prestressed Conc. Deck Bms. (27" depth)	162	Sq. Ft.
---	-----	---------

Note: Quantity is for 36" wide beam. See sheet S-9 for quantity of 48" wide beams. Total quantity for pay item is sum of 36" and 48" wide beam areas (2,322 Sq. Ft.)

MODEL: D:\c\h\... FILE NAME: N:\NORTH LAKE\940032DC167151\c1940032DC1671-01-36-PPC Deck Beam-02.dwg

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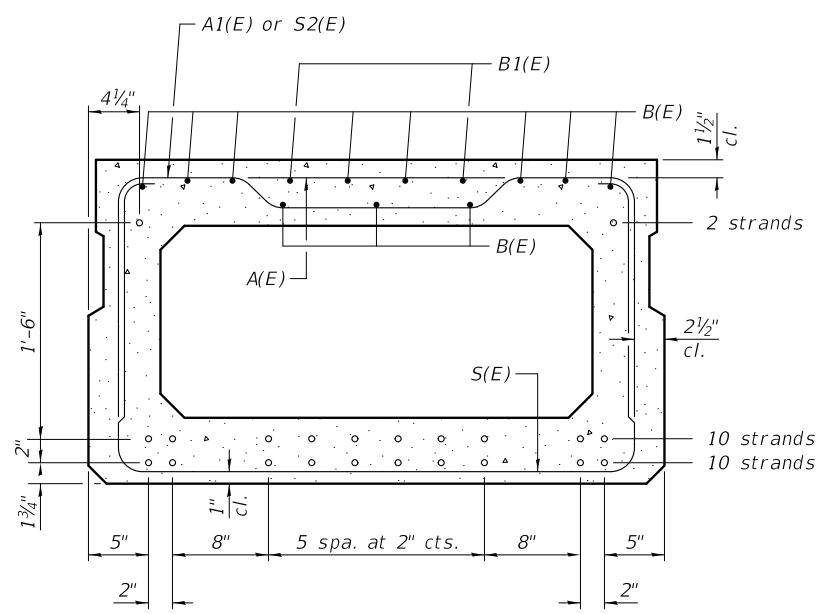
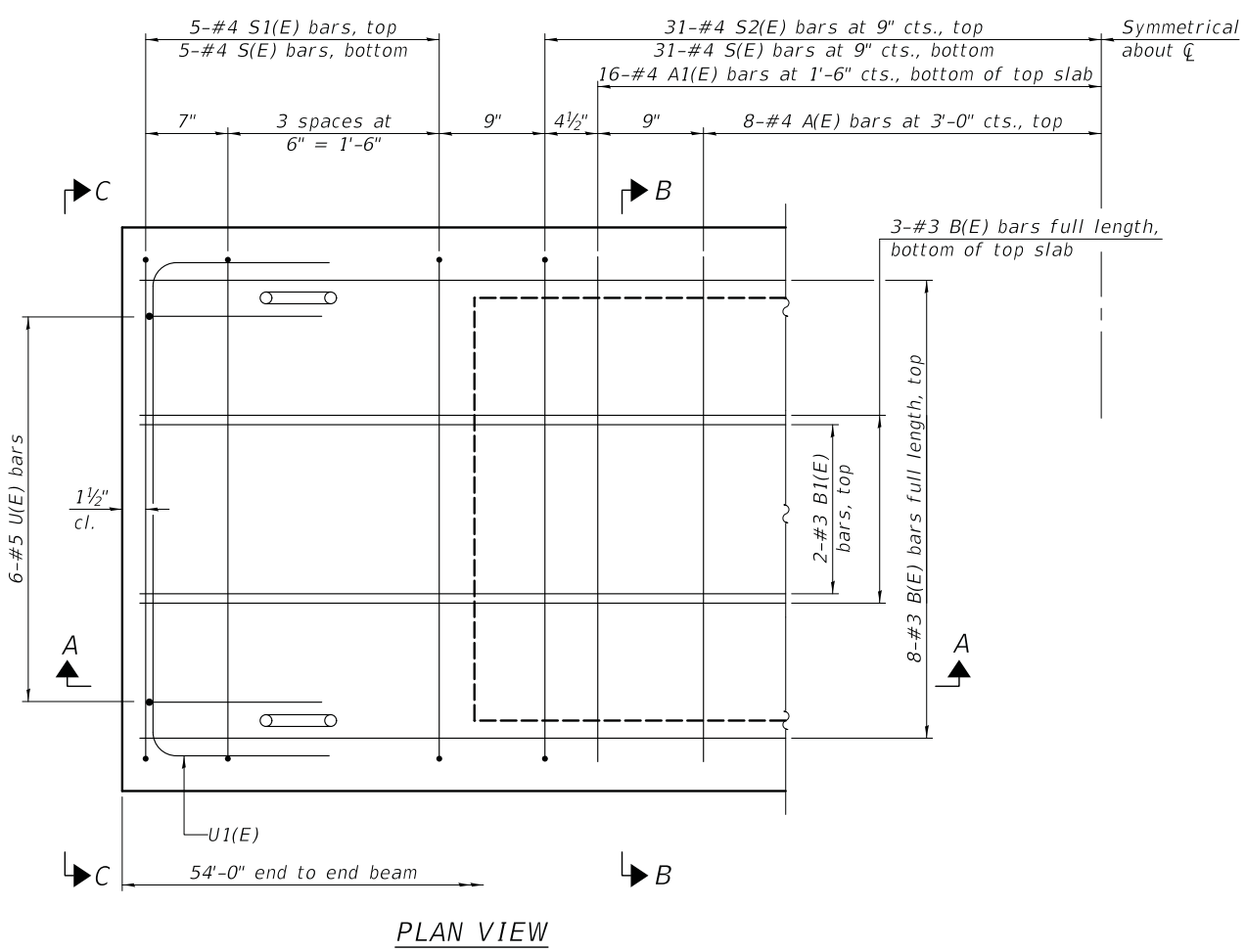
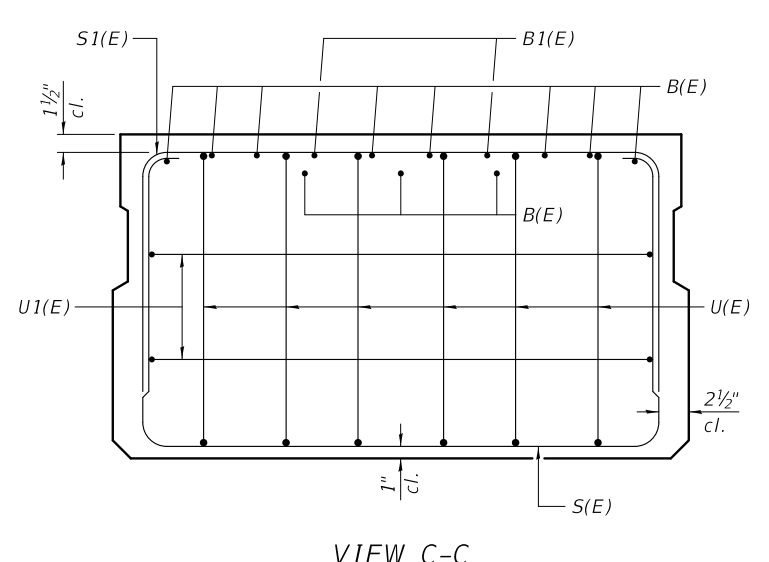
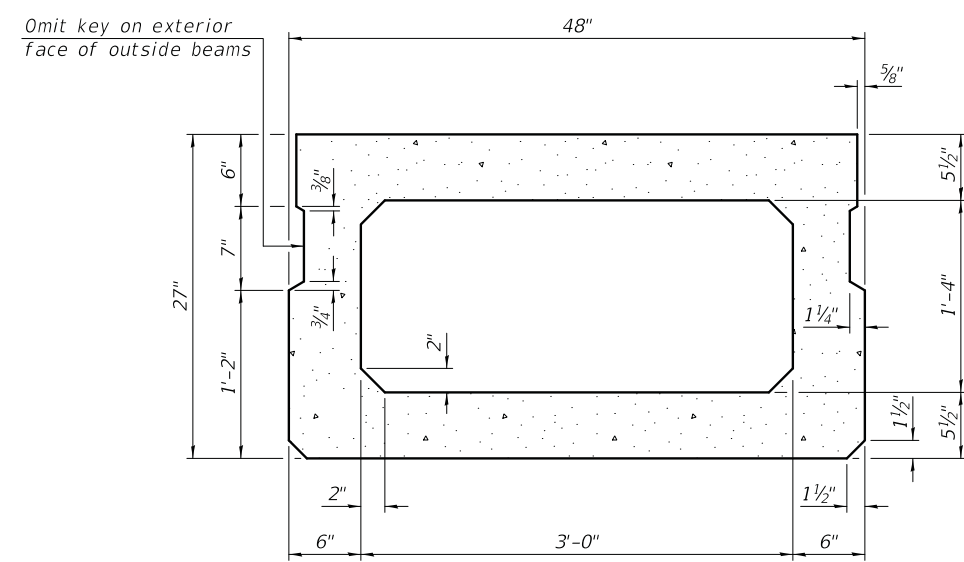
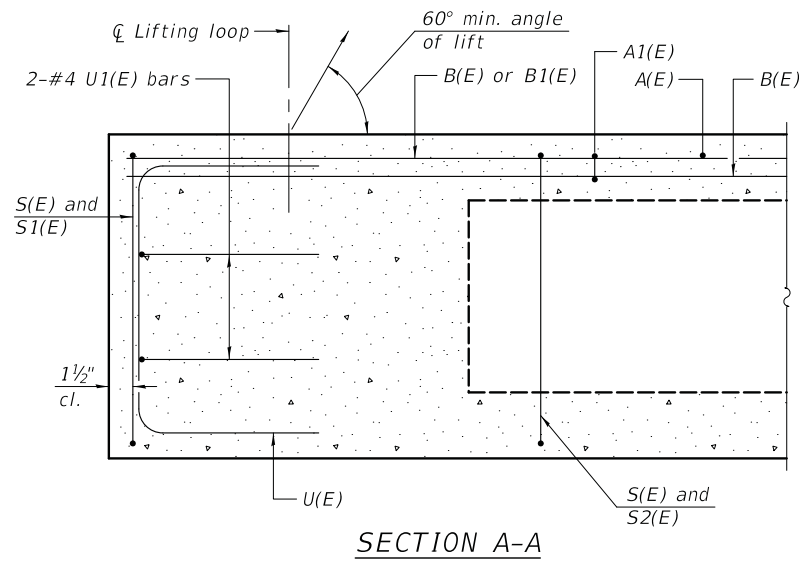
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PLOT SCALE = 1"	CHECKED - MM	REVISED -
PLOT DATE = 11/17/2020	DATE - 11/17/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

27"x36" PPC DECK BEAM DETAILS (2 OF 2)
ROY AVENUE OVER ADDISON CREEK
STRUCTURE NO. 016-7612

SCALE: N.T.S. SHEET S-7 OF 5-13 SHEETS STA. 13+24.16 TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	22
CONTRACT NO. 61G88				
ILLINOIS FED. AID PROJECT				



BAR LIST
ONE BEAM ONLY
(For information only)

Bar	No.	Size	Length	Shape
A(E)	16	#4	3'-7"	—
A1(E)	32	#4	3'-10"	~
B(E)	22	#3	27'-8"	—
B1(E)	4	#3	10'-0"	—
S(E)	72	#4	8'-5"	□
S1(E)	10	#4	6'-11"	□
S2(E)	62	#4	7'-2"	□
U(E)	12	#5	4'-6"	□
U1(E)	4	#4	6'-0"	□

Note: See sheet S-4 for additional details for D(E) bars in exterior beams.

Note:
Spacing of S(E) and S2(E) bars may be adjusted up to 4" in the immediate area of the transverse tie diaphragms to miss the block outs for the transverse ties.

SECTION B-B
(Showing reinforcement and permissible strand locations)

Note: Place the number of strands specified in each row symmetrically about the centerline of beam in the permissible strand locations shown.

MINIMUM BAR LAP
#3 bar = 1'-6"

MODEL: D:\c\p\... FILE NAME: N:\NORTH LAKE\940032DC1675\STRUCT\940032DC1675-08-4B-PPC Deck Beams\01.rvt

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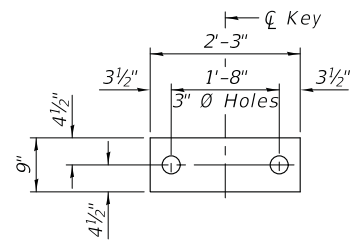
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

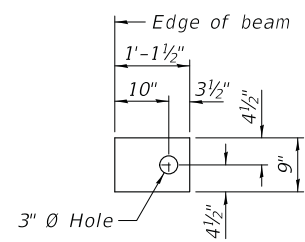
27"x48" PPC DECK BEAM DETAILS (1 OF 2)
ROY AVENUE OVER ADDISON CREEK
STRUCTURE NO. 016-7612

SCALE: N.T.S. SHEET S-8 OF 5-13 SHEETS STA. 13+24.16 TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	23
CONTRACT NO. 61G88			ILLINOIS FED. AID PROJECT	



FABRIC BEARING PAD
(Interior)

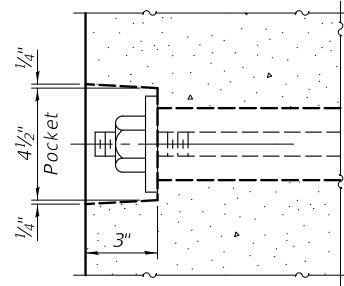


FABRIC BEARING PAD
(Exterior)

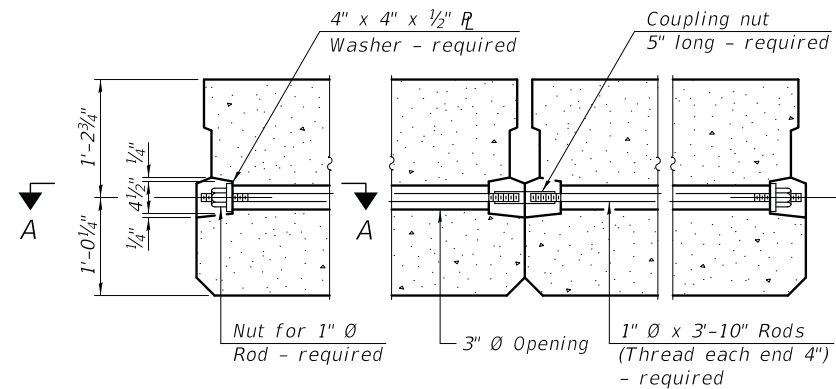
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Notes:

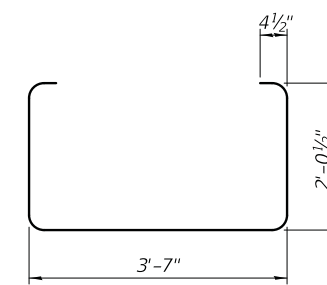
- All bearing pads shall be 1" thick.
- Omit holes when using expansion bearings.
- Expansion bearing pads shall be bonded to the substructure.



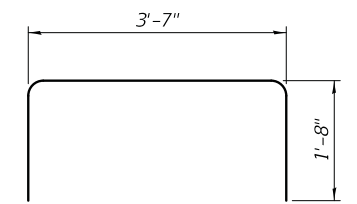
SECTION A-A



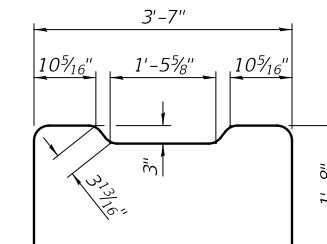
TYPICAL TRANSVERSE TIE ASSEMBLY



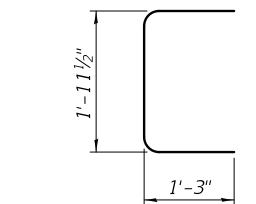
BAR S(E)



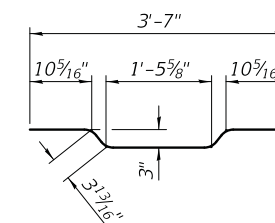
BAR S1(E)



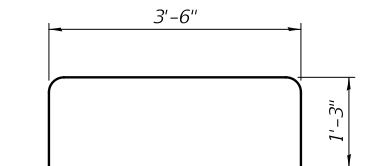
BAR S2(E)



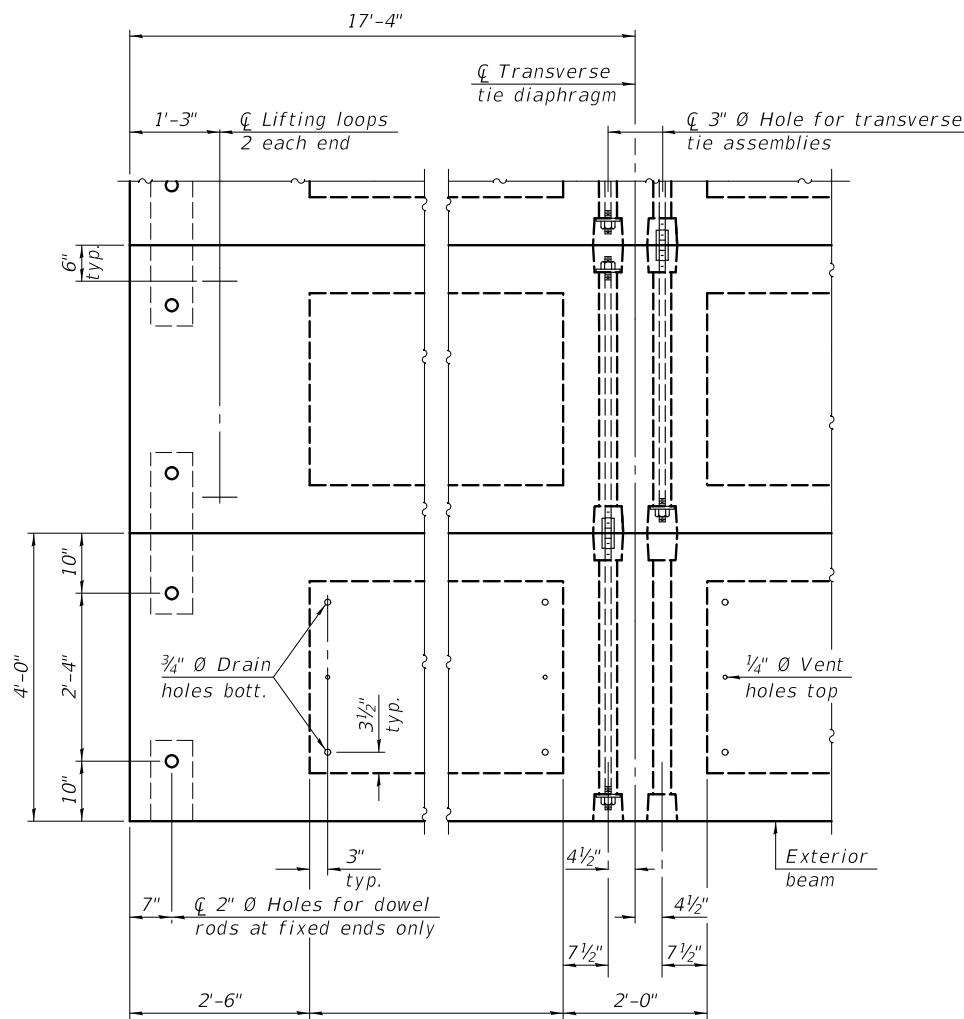
BAR U(E)



BAR A1(E)



BAR U1(E)

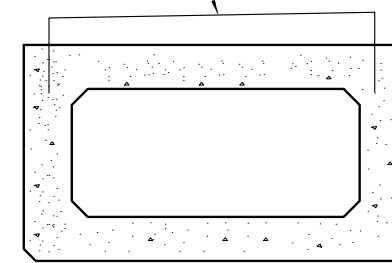


PLAN VIEW

Note:

- Connect beams in pairs with the transverse tie configuration shown.

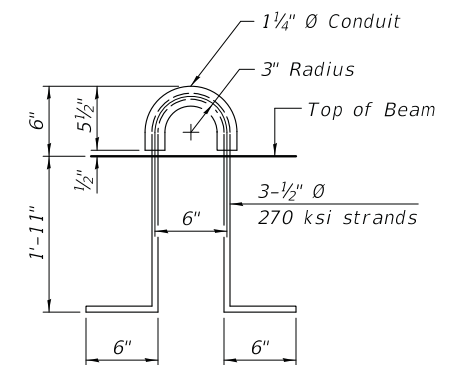
#4 D(E) Bars @ 9" cts.



SECTION THRU FASCIA BEAMS WITH SIDEWALK REINFORCEMENT
(Opposite Fascia Beam Will Be Mirror Image.)
See Sheet S-4 for reinforcement details

NOTES

- Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.153 sq. in.
- The 1" Ø rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets on exterior faces of bridge shall be filled with grout after transverse tie assembly is in place.
- Two 1/8" fabric adjusting shims of the dimensions of the exterior bearing pad shall be provided for each bearing pad location.
- A minimum 2 1/2" Ø lifting pin shall be used to engage the lifting loops during handling.
- Corrosion Inhibitor, per Article 1020.05(b)(10) and 1021.07 of the Standard Specifications, shall be used in the concrete for precast prestressed concrete deck beams.
- Compressive strength of prestressed concrete, f'c, shall be 6000 psi.
- Compressive strength of prestressed concrete at release, f'ci, shall be 5000 psi.



LIFTING LOOP DETAIL

BILL OF MATERIAL

Precast Prestressed Conc. Deck Bms. (27" depth)	Sq. Ft.	2,160
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Note: Quantity is for 48" wide beam. See sheet S-7 for quantity of 36" wide beams. Total quantity for pay item is sum of 36" and 48" wide beam areas (2,322 Sq. Ft.)

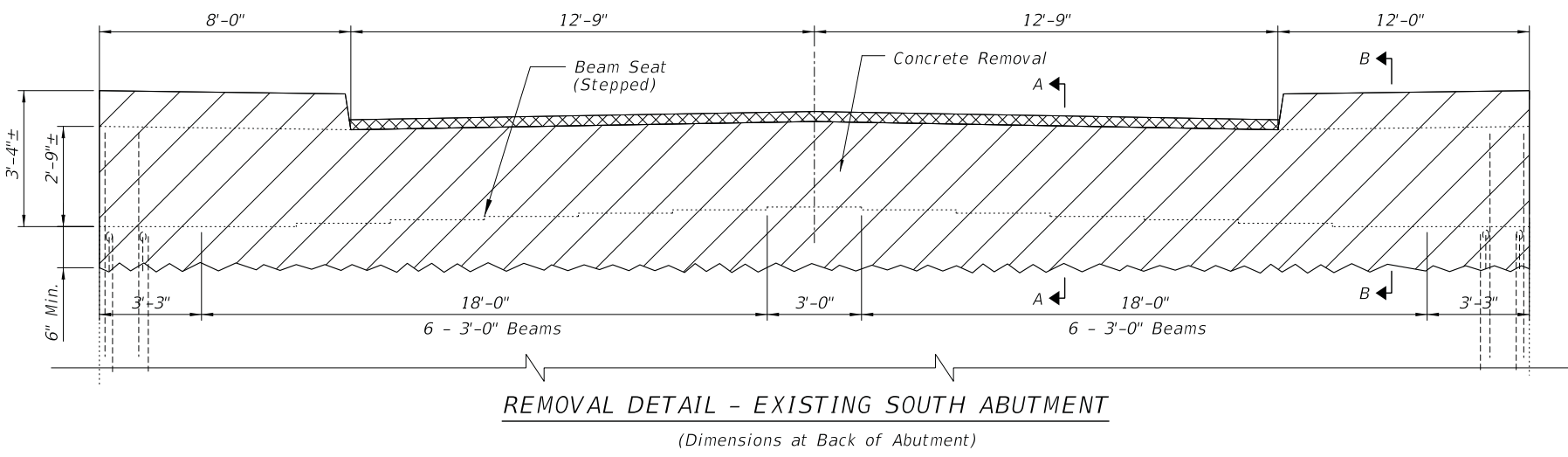
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(847) 823-0500

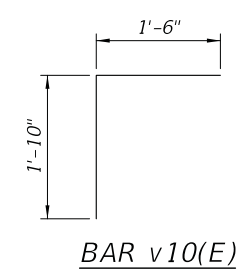
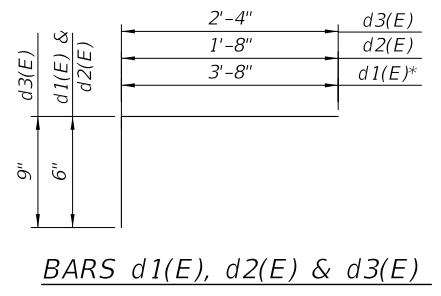
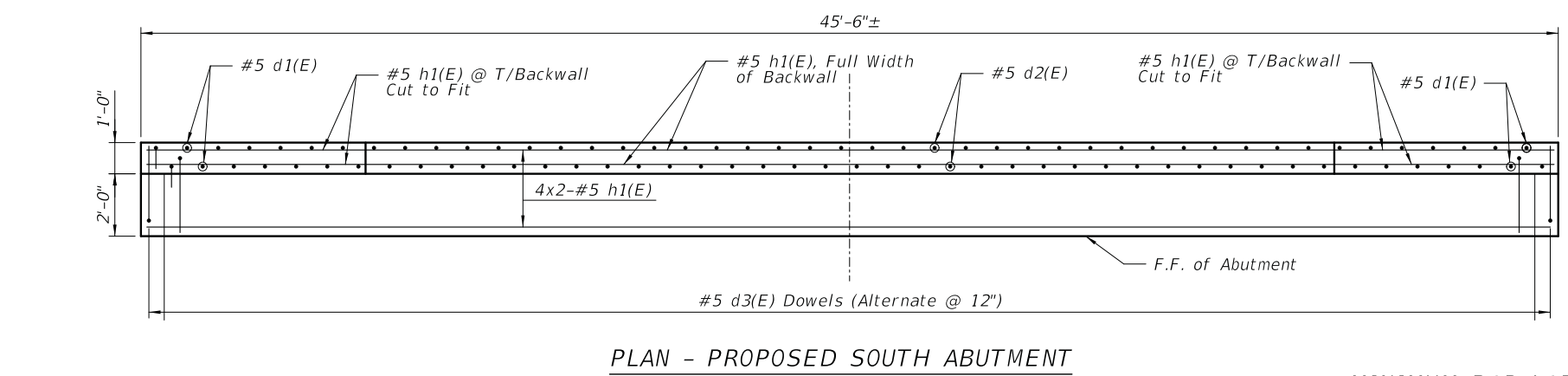
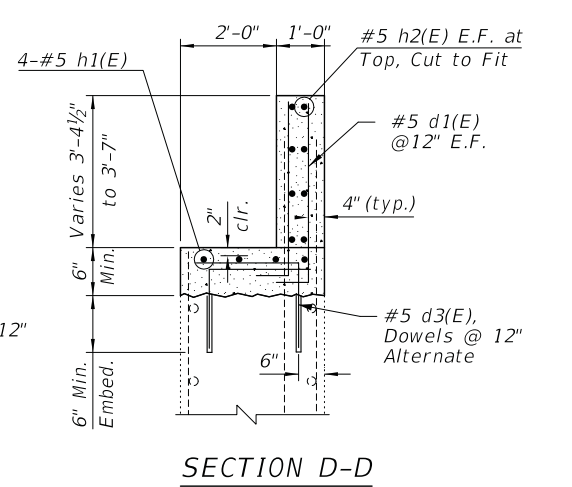
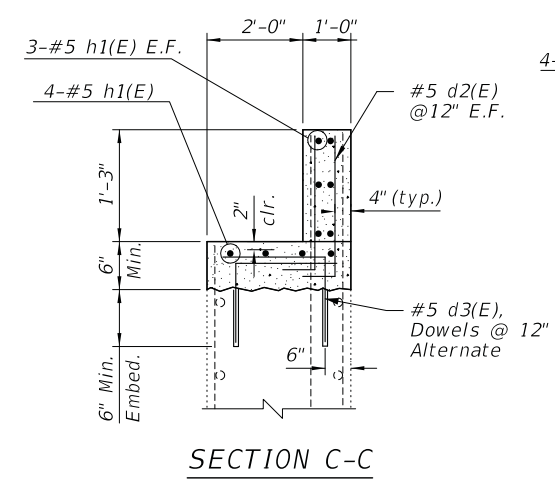
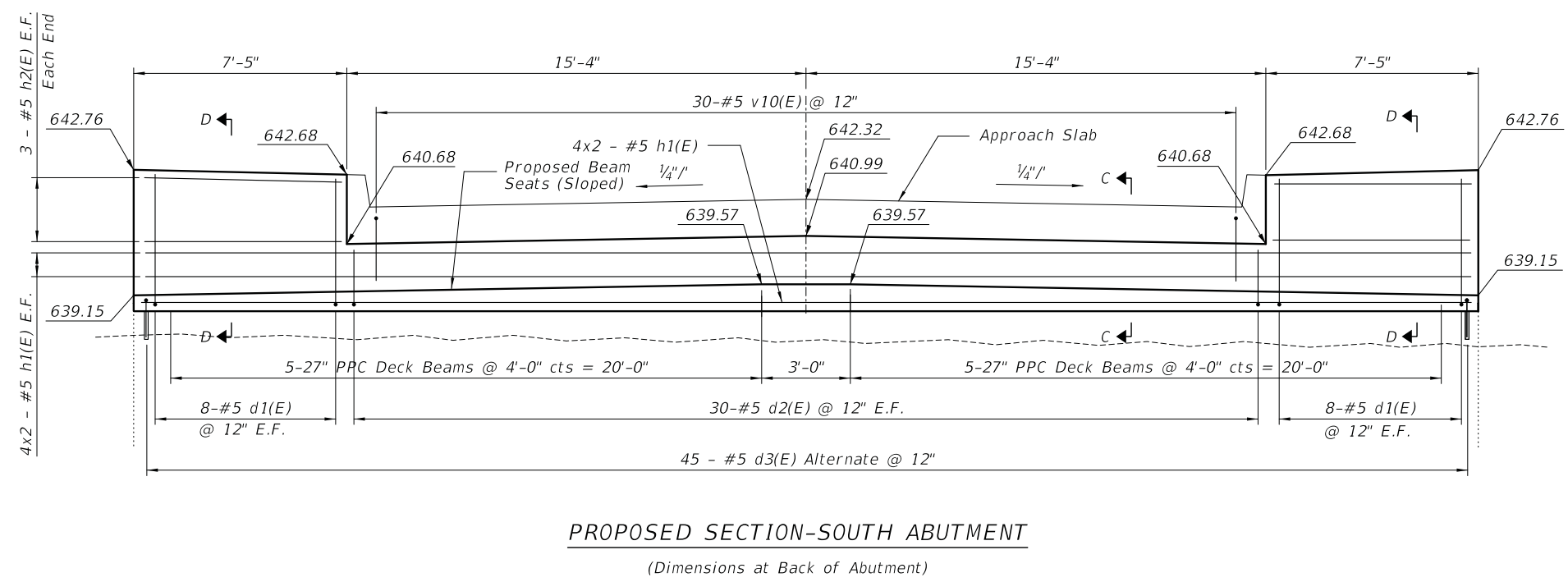
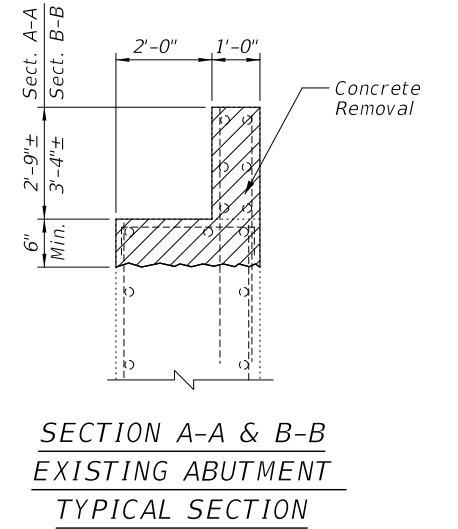
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	DATE - 11/17/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

27"x48" PPC DECK BEAM DETAILS (2 OF 2)		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ROY AVENUE OVER ADDISON CREEK		2539	14-00087-00-BR	COOK	33	24
STRUCTURE NO. 016-7612		CONTRACT NO. 61G88				
SCALE: N.T.S.	SHEET S-9	OF 5-13 SHEETS	STA. 13+24.16	TO STA.	ILLINOIS FED. AID PROJECT	



- NOTES:
- Existing vertical reinforcement shall be cleaned and incorporated into the new construction. Cost included with "Concrete Removal".
 - Any reinforcement bars that are damaged during concrete removal operation shall be repaired using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".
 - Drilling and grouting dowel bars into the existing abutment shall be included in the cost of "Reinforcement Bars, Epoxy Coated". Minimum embedment is 6". Dowel bars shall have 4" cover from edges.
 - Stagger d1(E) and h2(E) bars.
 - Cut d1(E) and d2(E) to fit and paint cut end with epoxy paint, or increase embedment of bars to fit.



ABUTMENT - BILL OF MATERIAL (SOUTH ABUTMENT)

Bar	No.	Size	Length	Shape
d1(E)	32	#5	4'-3"	┌
d2(E)	60	#5	1'-11"	┌
d3(E)	90	#5	3'-1"	┌
h1(E)	24	#5	24'-5"	—
h2(E)	110	#5	7'-1"	—
v10(E)	30	#5	3'-4"	┌
Concrete Removal			Cu. Yd.	12.3
Structure Excavation			Cu. Yd.	40
Concrete Structures			Cu. Yd.	14.0
Reinforcement Bars, Epoxy Coated			Pound	2,080

Bars indicated thus 1 x 2 -#4 etc. indicates 1 line of bars with 2 lengths per line.

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CHRISTOPHER B. BURKE ENGINEERING, LTD.
3075 W. Higgins Road, Suite 400
Rosemont, Illinois 60018
(847) 823-0500

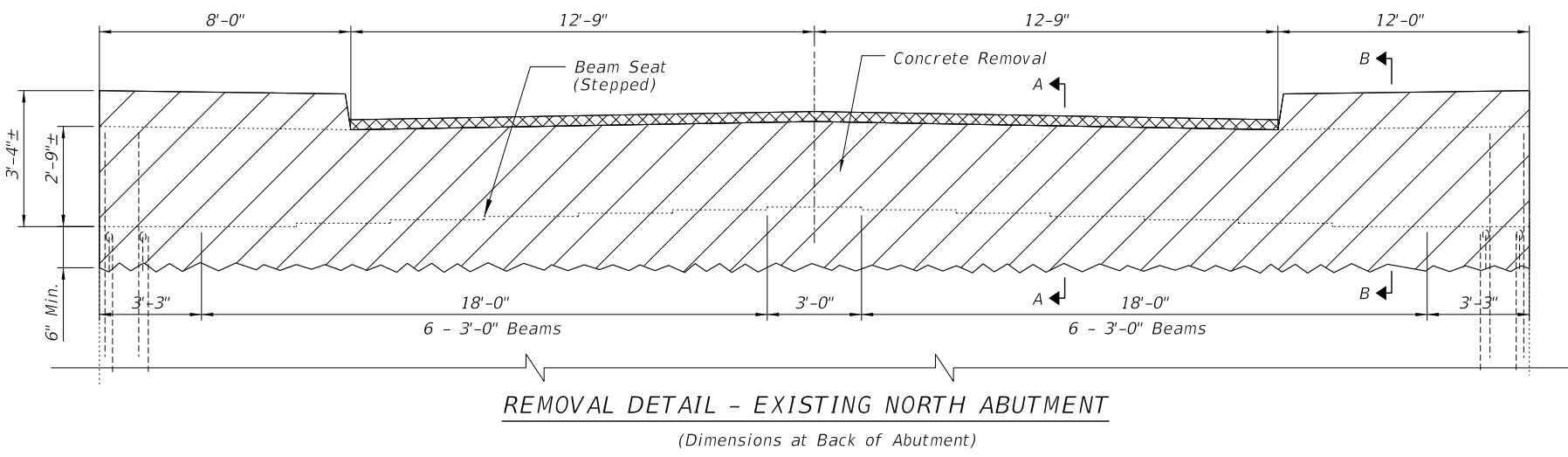
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

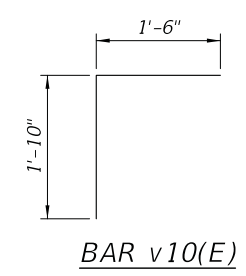
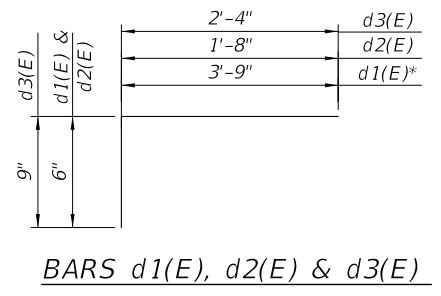
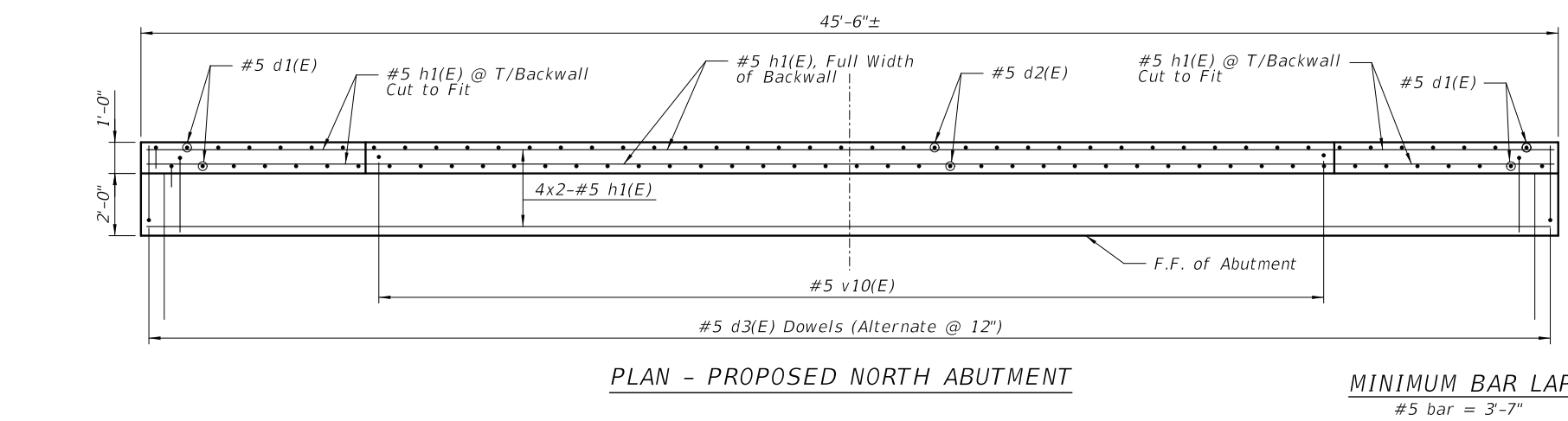
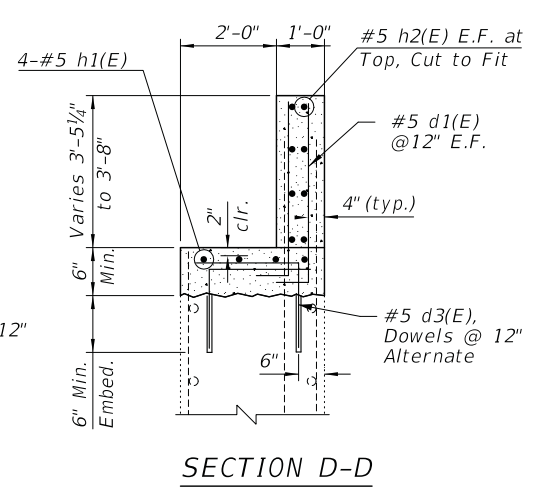
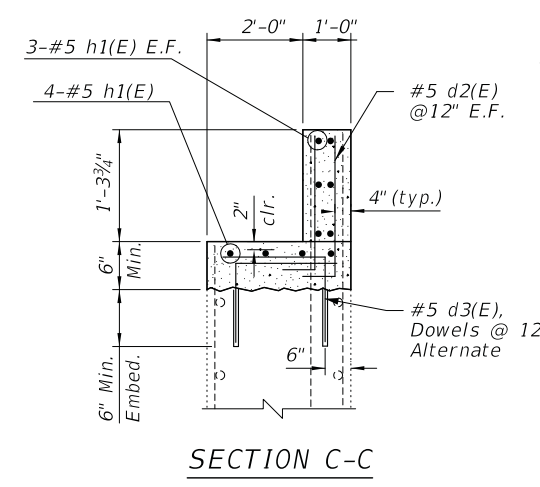
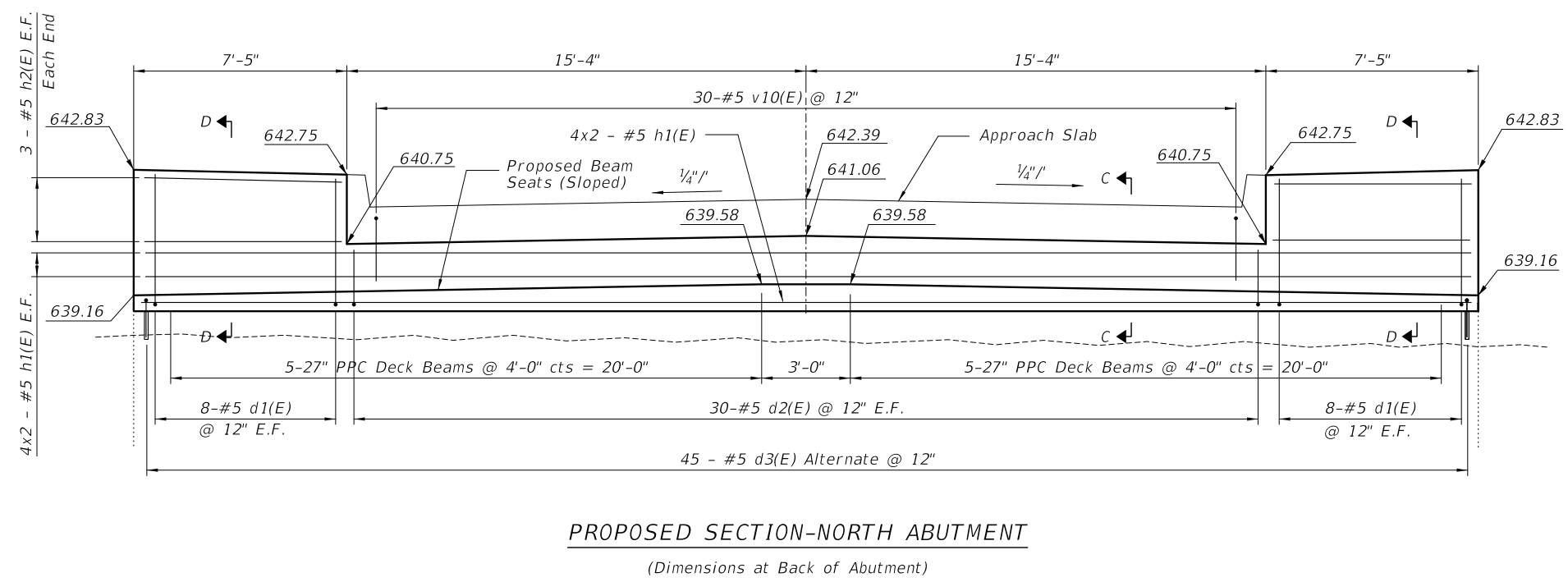
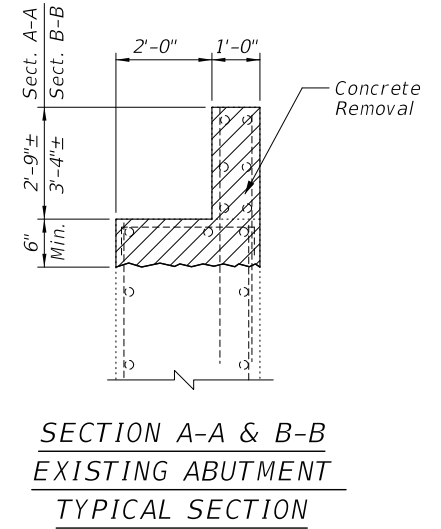
SOUTH ABUTMENT PLAN, SECTIONS AND DETAILS
ROY AVENUE OVER ADDISON CREEK
STRUCTURE NO. 016-7612

SCALE: N.T.S. SHEET 5-10 OF 5-13 SHEETS STA. 13+24.16 TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	25
CONTRACT NO. 61G88				
ILLINOIS FED. AID PROJECT				



- NOTES:
- Existing vertical reinforcement shall be cleaned and incorporated into the new construction. Cost included with "Concrete Removal".
 - Any reinforcement bars that are damaged during concrete removal operation shall be repaired using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".
 - Drilling and grouting dowel bars into the existing abutment shall be included in the cost of "Reinforcement Bars, Epoxy Coated". Minimum embedment is 6". Dowel bars shall have 4" cover from edges.
 - Stagger d1(E) and h2(E) bars.
 - Cut d1(E) and d2(E) to fit and paint cut end with epoxy paint, or increase embedment of bars to fit.



ABUTMENT - BILL OF MATERIAL (NORTH ABUTMENT)

Bar	No.	Size	Length	Shape
d1(E)	32	#5	4'-3"	┌
d2(E)	60	#5	1'-11"	┌
d3(E)	90	#5	3'-1"	┌
h1(E)	24	#5	24'-5"	—
h2(E)	110	#5	7'-1"	—
v10(E)	30	#5	3'-4"	┌
Concrete Removal			Cu. Yd.	12.3
Structure Excavation			Cu. Yd.	40
Concrete Structures			Cu. Yd.	14.0
Reinforcement Bars, Epoxy Coated			Pound	2,080

Bars indicated thus 1 x 2 -#4 etc. indicates 1 line of bars with 2 lengths per line.

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3075 W. Higgins Road, Suite 400
Rosemont, IL 60018
(847) 823-0500

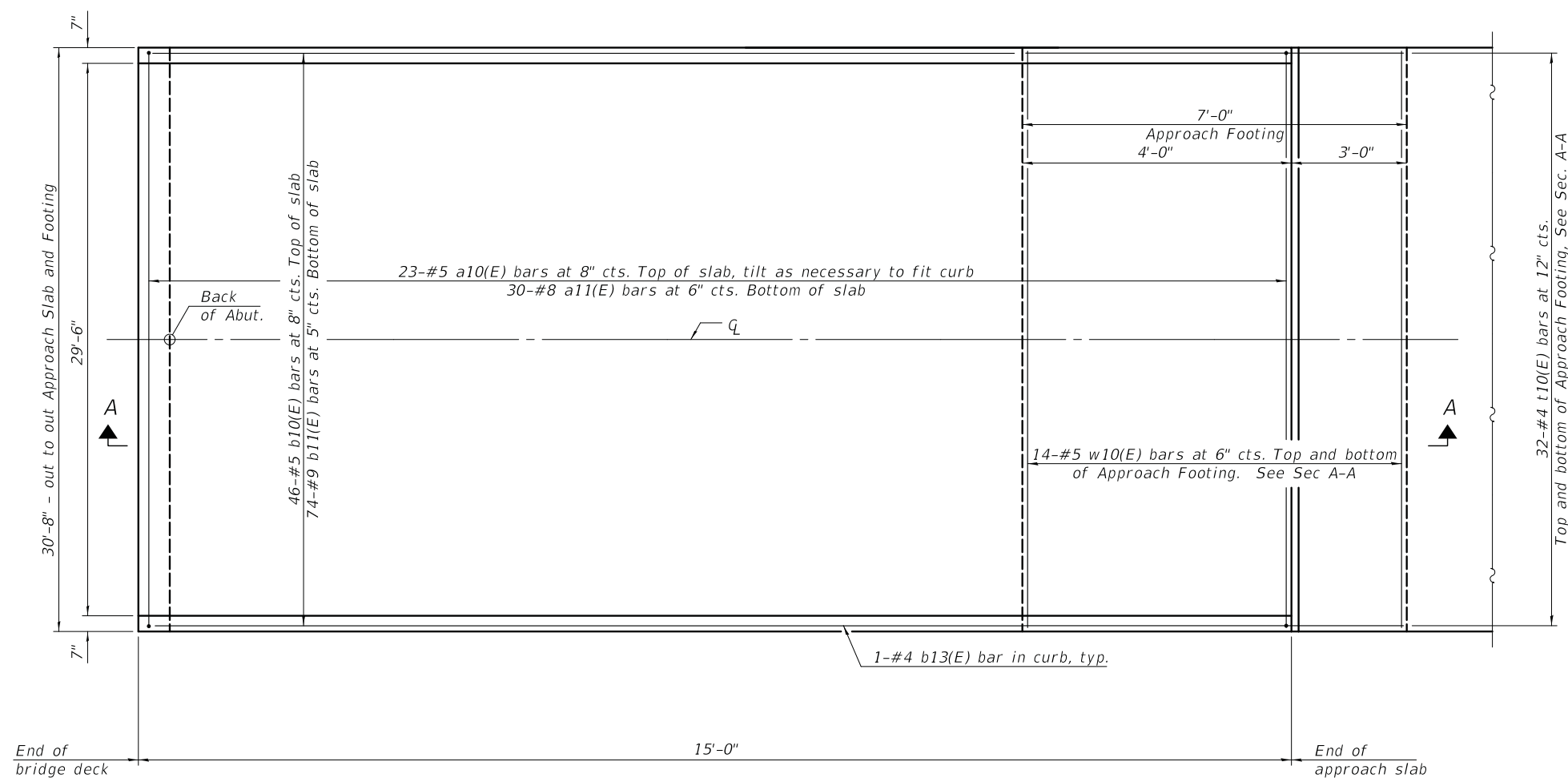
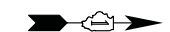
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

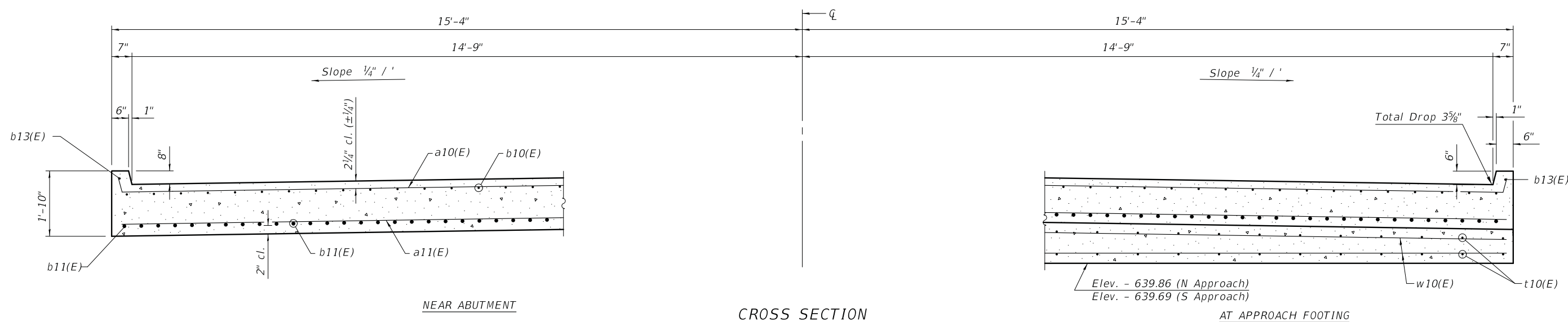
NORTH ABUTMENT PLAN, SECTIONS AND DETAILS
ROY AVENUE OVER ADDISON CREEK
STRUCTURE NO. 016-7612

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	26
CONTRACT NO. 61G88				
ILLINOIS FED. AID PROJECT				

SCALE: N.T.S. SHEET S-11 OF 5-13 SHEETS STA. 13+24.16 TO STA.



NORTH APPROACH SLAB PLAN
(MIRROR FOR SOUTH APPROACH)



CROSS SECTION
(Looking North)

MODEL: D:\p\h\...
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CB CHRISTOPHER B. BURKE ENGINEERING, LTD.
 9775 W. Higgins Road, Suite 400
 Rosemont, Illinois 60018
 (847) 823-0500

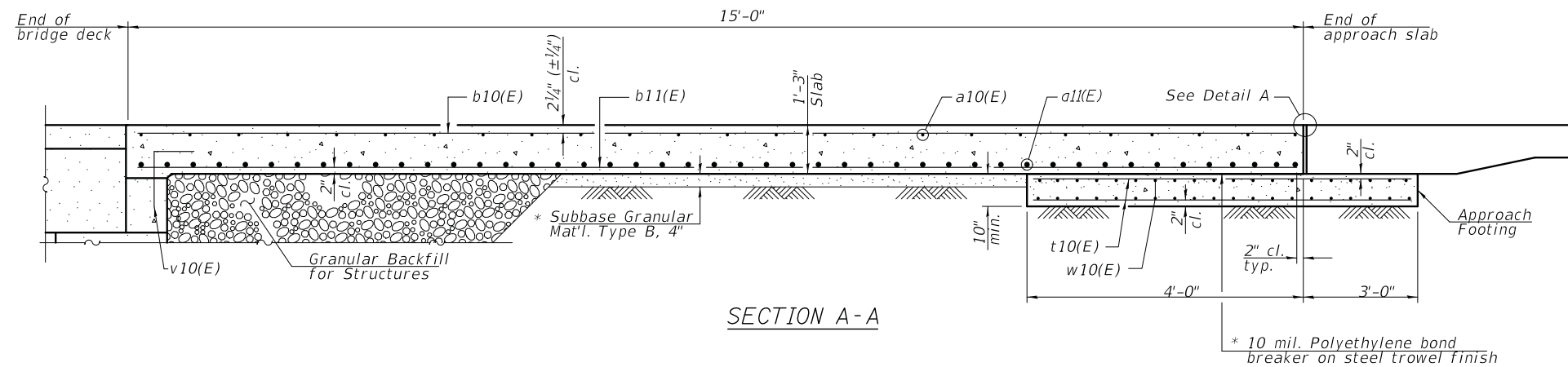
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PLOT DATE = 11/17/2020	DATE - 11/17/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROACH SLAB DETAILS (1 OF 2)
ROY AVENUE OVER ADDISON CREEK
STRUCTURE NO. 016-7612

SCALE: N.T.S. SHEET 5-12 OF 5-13 SHEETS STA. 13+24.16 TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 61G88				
ILLINOIS FED. AID PROJECT				

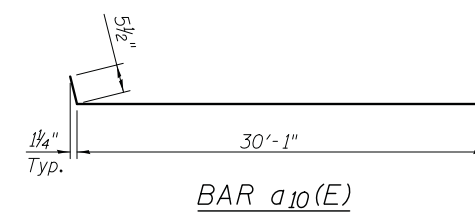


Notes:

Approach slab shall be paid for as Concrete Superstructure (Approach Slab).
 Approach footing concrete shall be paid for as Concrete Structures.
 The approach footing maximum applied service bearing pressure (Q_{max}) = 2.0 ksf.
 Cost of excavation for approach footing included with Concrete Structures.

TWO APPROACHES
 BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a10(E)	46	#5	31'-0"	┌
a11(E)	60	#8	30'-4"	—
b10(E)	92	#5	14'-8"	—
b11(E)	148	#9	14'-8"	—
b13(E)	4	#4	14'-8"	—
t10(E)	128	#4	6'-8"	—
v10(E)	62	#5	3'-4"	└
w10(E)	56	#5	30'-4"	—
Concrete Superstructure (Approach Slab)			Cu. Yd.	43.5
Concrete Structures			Cu. Yd.	13.2
Reinforcement Bars, Epoxy Coated			Pound	17,520



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CB CHRISTOPHER B. BURKE ENGINEERING, LTD.
 9775 W. Higgins Road, Suite 400
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 (847) 823-0500

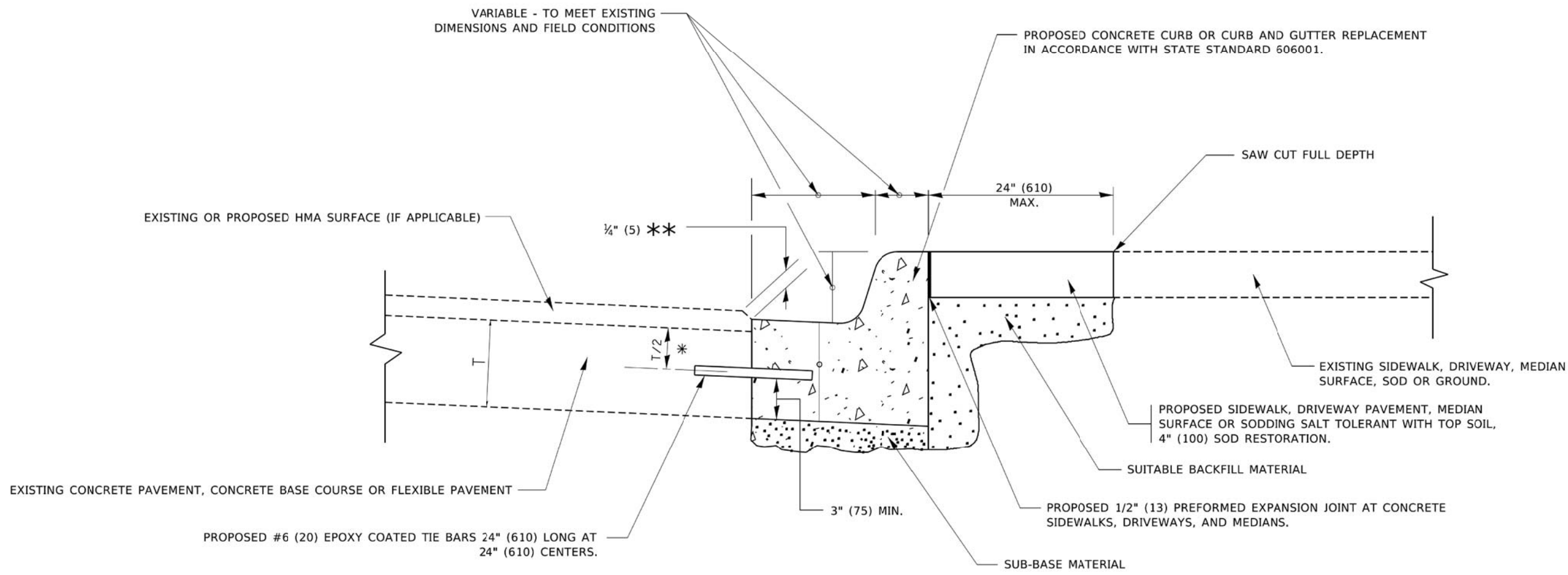
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	DATE - 11/17/2020	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

APPROACH SLAB DETAILS (2 OF 2)
 ROY AVENUE OVER ADDISON CREEK
 STRUCTURE NO. 016-7612

SCALE: N.T.S. SHEET 5-13 OF 5-13 SHEETS STA. 13+24.16 TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	28
				CONTRACT NO. 61G88
ILLINOIS FED. AID PROJECT				



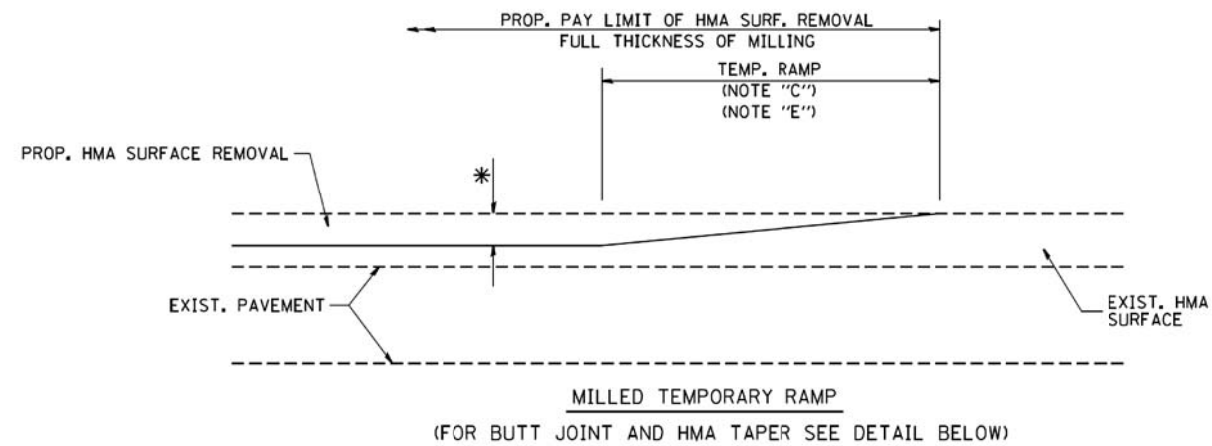
- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

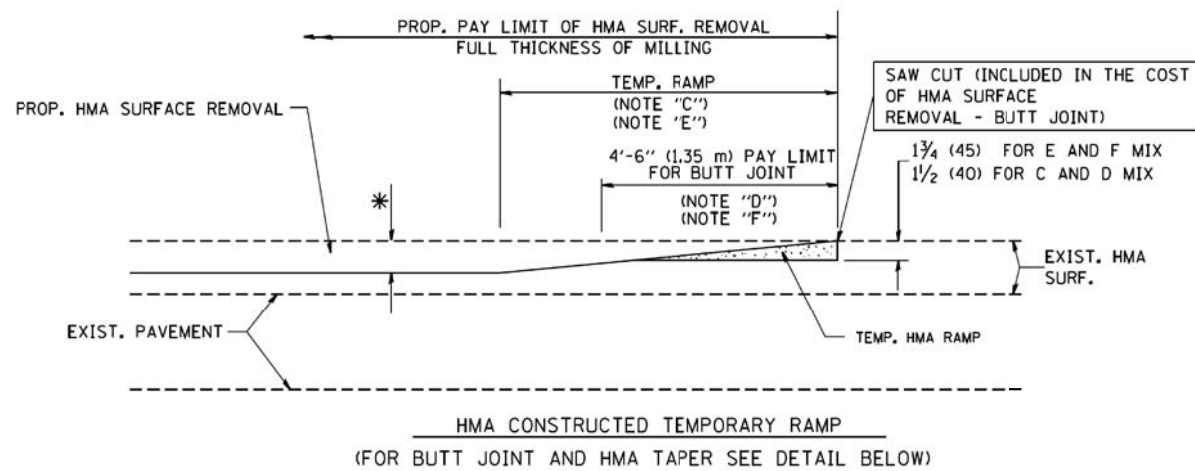
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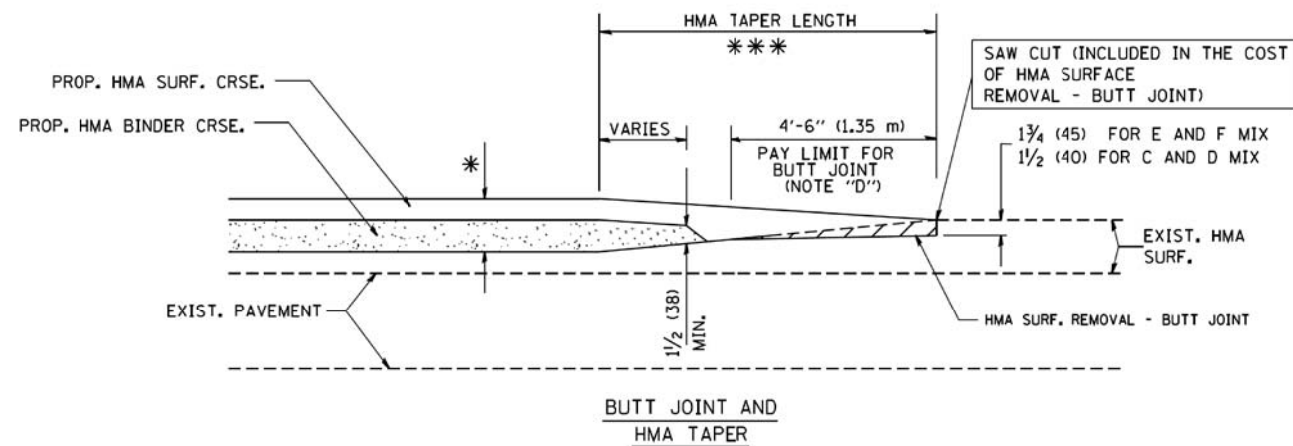


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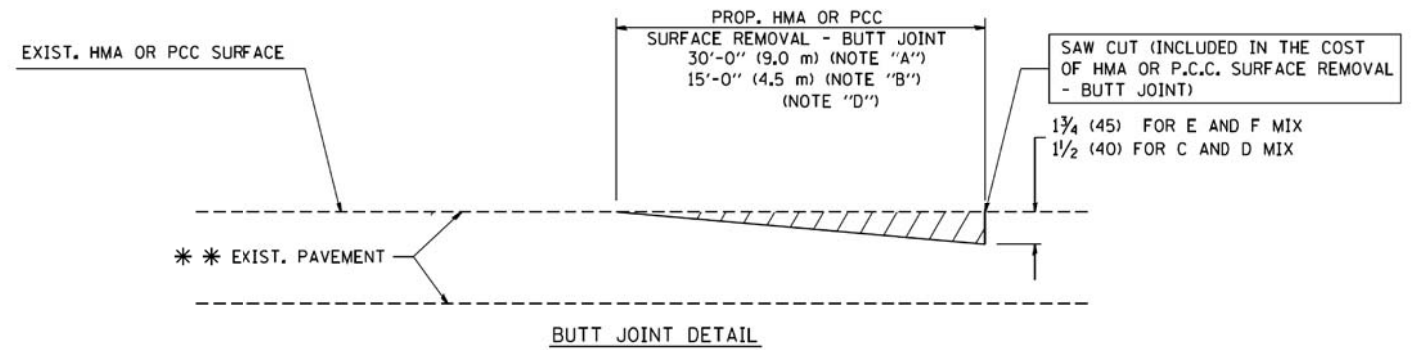


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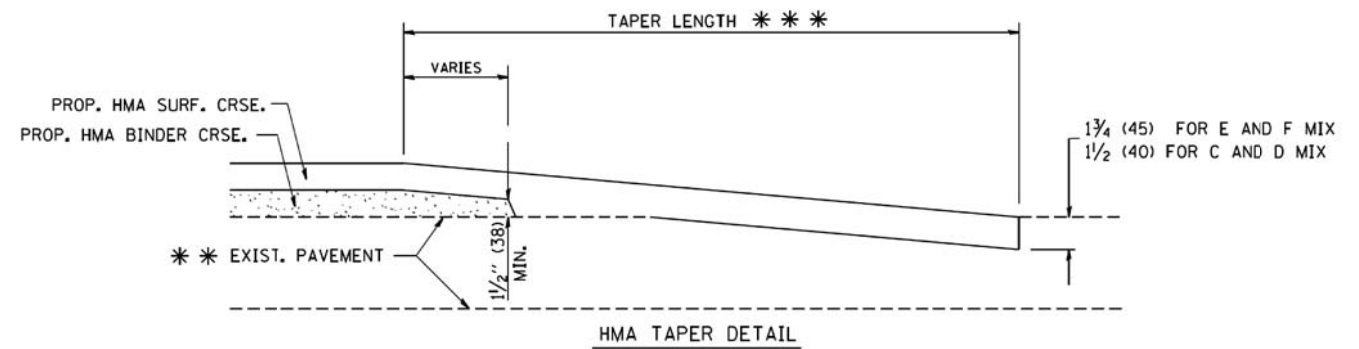
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

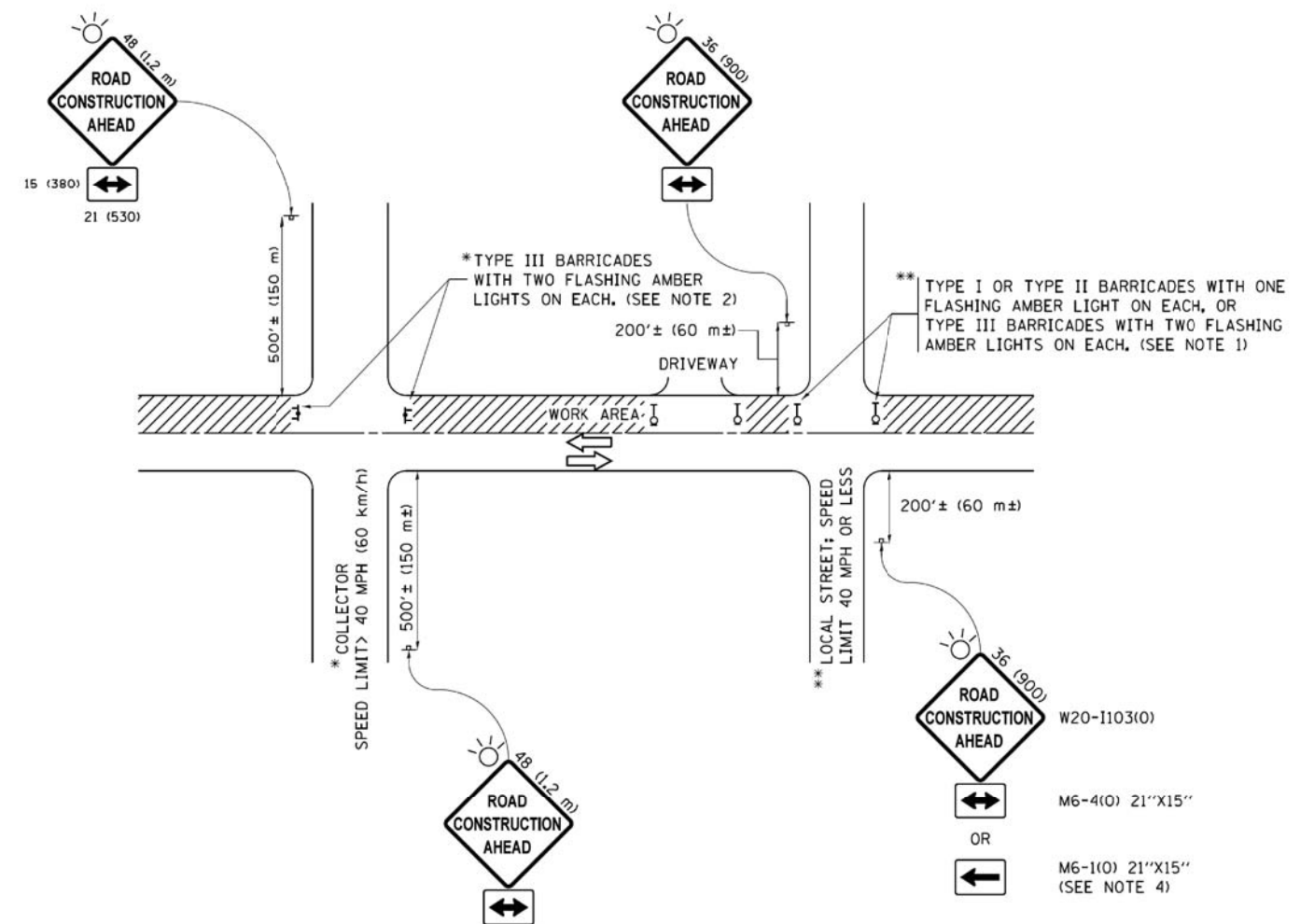
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	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	30
BD400-05 BD32		CONTRACT NO. 61G88		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
per\IL084EBIDINTEG\Illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\Dist	Documents\IDOT Offices\District 1\Projects\Dist	DRAWN - CADData\CADsheets\colB.dgn	REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 50,000' / 1"	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	31
TC-10			CONTRACT NO. 61G88	
ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

ROUTE MARKERS

FOR U.S. ROUTES
MI-40-2424

FOR ILLINOIS ROUTES
MI-50-2424

R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

ARROWS SIGNS

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-1-2115

M6-3-2115

CARDINAL DIRECTION & DETOUR SIGNS

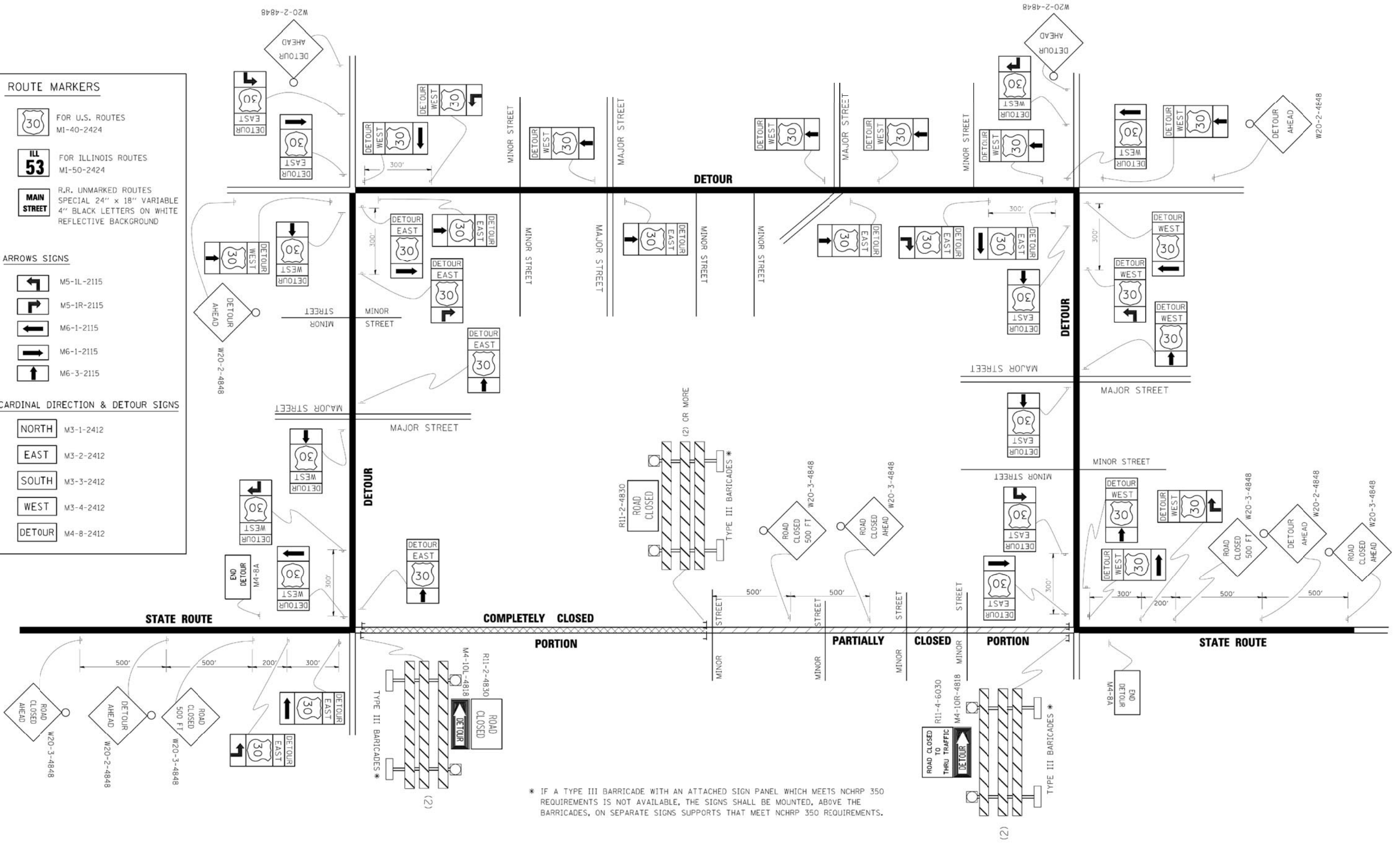
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EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

DETOUR M4-8-2412



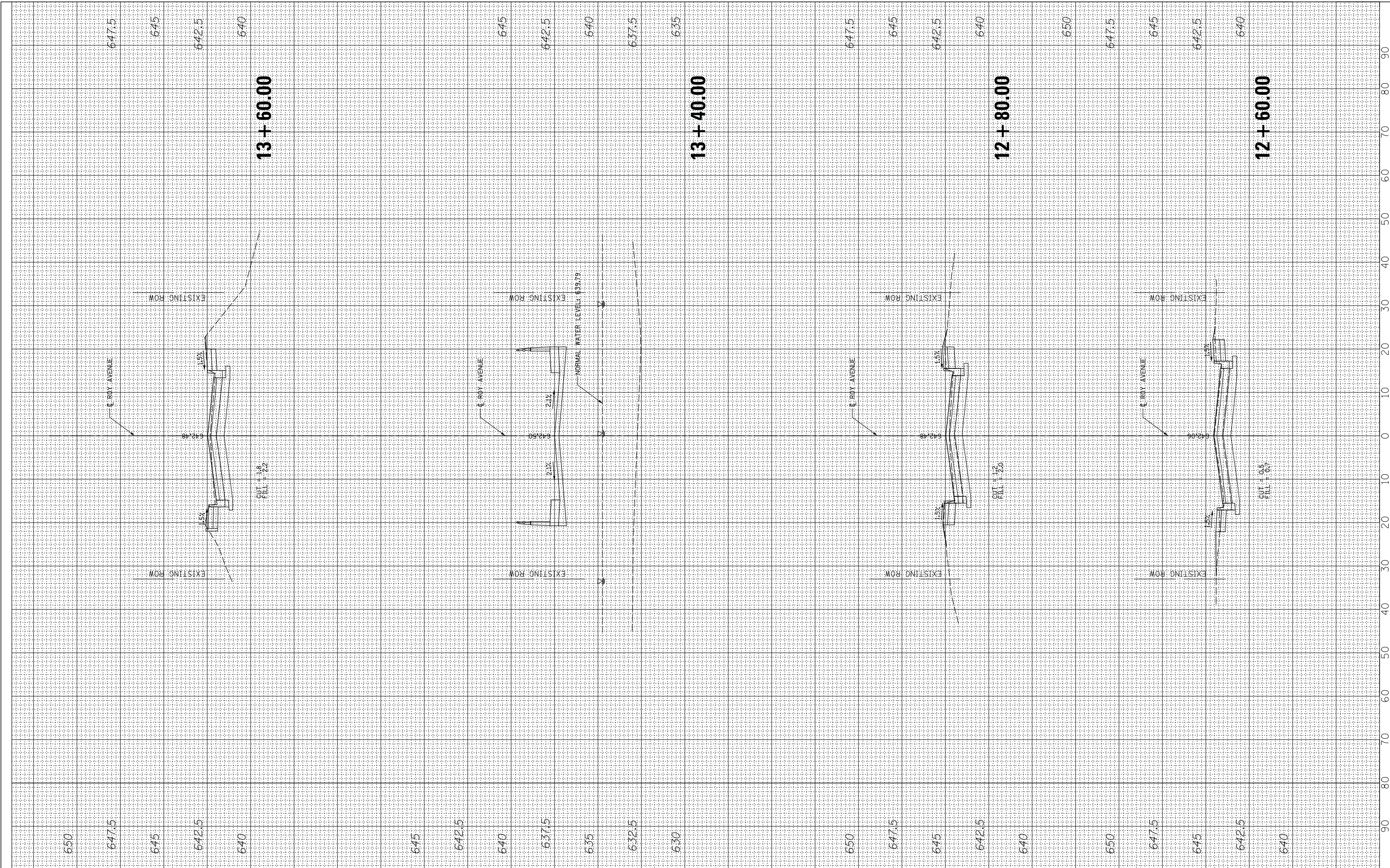
* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

FILE NAME -	USER NAME - driveadgn	DESIGNED -	REVISED - 10-18-02	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETOUR SIGNING FOR CLOSING STATE HIGHWAYS			FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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ORIGINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE

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DRAWN -	DOC
CHECKED -	JMB
DATE -	11/17/2020

REVISED -	
REVISED -	
REVISED -	
REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS

SCALE: 20H 5V SHEET 1 OF 1 SHEETS STA. 12+60.00 TO STA. 13+60.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	14-00087-00-BR	COOK	33	33
CONTRACT NO. 61G88				
ILLINOIS		FED. AID PROJECT		