F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 61E13		

FOR INDEX OF SHEETS, SEE SHEET NO. 2

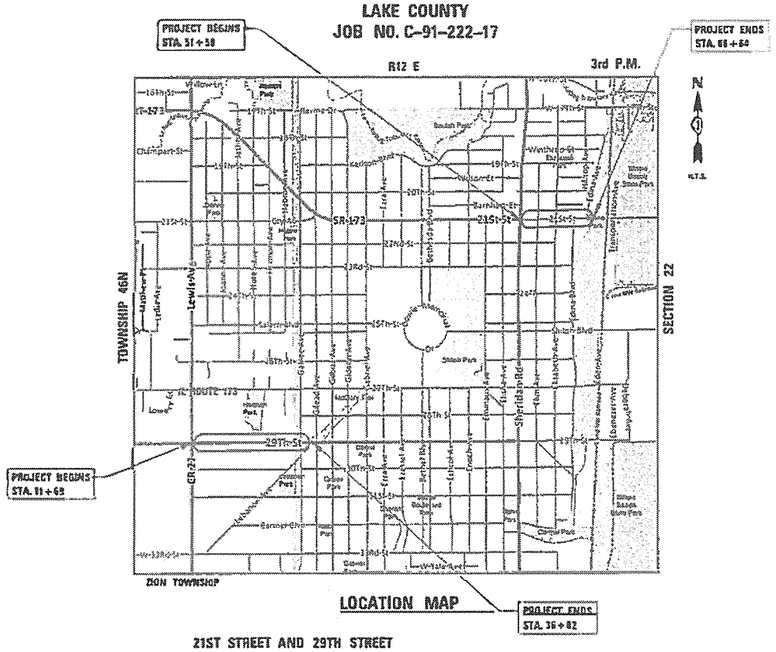
FOR LIST OF APPLICABLE HIGHWAY STANDARDS
SEE SHEET 2

01-19-2018 LETTING ITEM 089

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

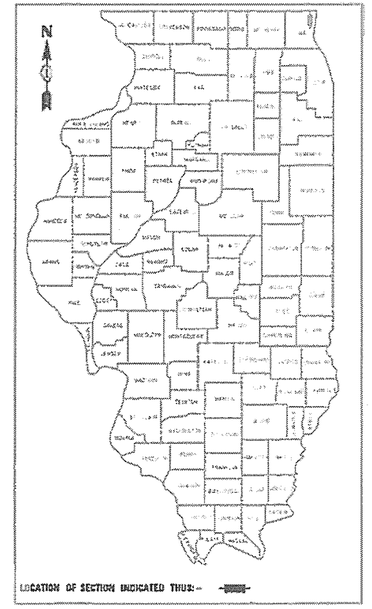
PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU 1202 (21ST STREET) AND FAU 1207 (29TH STREET)
21ST STREET: FAU 352 (SHERIDAN ROAD) TO DEAD END
29TH STREET: FAU 2730 (LEWIS AVENUE) TO FAU 2740 (GALILEE AVENUE)
RESURFACING
SECTION NO. 17-00088-00-RS
PROJECT NO. VGQR (277)
CITY OF ZION
LAKE COUNTY
JOB NO. C-91-222-17



LOCATION MAP

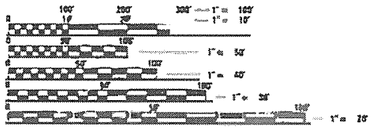
21ST STREET AND 29TH STREET
GROSS LENGTH OF PROJECT = 4,019 FEET (0.76 MILES)
NET LENGTH OF PROJECT = 4,019 FEET (0.76 MILES)



LOCATION OF SECTION INDICATED THUS:-

TRAFFIC DATA
21ST STREET
ADT (YEAR) = 2200 (2015)
POSTED SPEED LIMIT = 30 MPH
29TH STREET
ADT (YEAR) = 1200 (2015)
POSTED SPEED LIMIT = 30 MPH

21ST STREET DESIGN DESIGNATION: MINOR COLLECTOR
29TH STREET DESIGN DESIGNATION: MINOR COLLECTOR



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
8-1-1 OR 1-800-892-0123

CHRISTOPHER B. BURKE ENGINEERING LTD.
9575 West Higgins Road, Suite 600
Rosemont, Illinois 60018 (847) 823-0500

PROFESSIONAL DESIGN FIRM NO. 184-001175
EXPIRATION DATE: 04/30/19

CONTRACT NO. 61E13

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED _____ 2017
PUBLIC WORKS DIRECTOR, CITY OF ZION

PASSED November 9 2017
DISTRICT ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW Nov. 9 2017
REGIONAL ENGINEER

LEE M. FELL — DULY REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS

_____ 9/14/17
ENGINEER DATE

LEE M. FELL
ILLINOIS REGISTRATION No. 062-053708
EXPIRATION DATE: 11/30/2017

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. (847) 705-4406 SCHAUMBURG, ILLINOIS

GENERAL NOTES

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE LATEST REVISION; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JUNE 2014 SEVENTH EDITION, THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT HIGHWAY STANDARD.

CODES OF THE IEPA TITLE 35, AND O.S.H.A. SHALL BE ADHERED TO FOR THE CONSTRUCTION OF THIS PROJECT.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

ALL REQUIRED PERMITS FROM THE PROPER GOVERNING AGENCY SHALL BE OBTAINED FOR CONSTRUCTION ALONG OR ACROSS EXISTING STREETS OR HIGHWAYS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THE PROPER BRACING, SHEETING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE AGENCY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ADEQUATE SIGNS AND WARNING DEVICES TO INFORM AND PROTECT THE PUBLIC.

UTILITIES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITY FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE CITY DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 8-1-1 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE.

THE CONTRACTOR SHALL CONTACT IDOT'S BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS BEFORE PLACING HOT-MIX ASPHALT OR PORTLAND CEMENT CONCRETE.

WATER, STORM SEWER AND SANITARY SEWER

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. CONTACT THE CITY OF ZION WATER DEPARTMENT (TEL. 847-746-4060) FOR THEM TO TURN VALVES OR OPERATE HYDRANTS. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

MISCELLANEOUS

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL AND REPLACEMENT, DRIVEWAY REMOVAL AND REPLACEMENT, AND STRUCTURES TO BE ADJUSTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD AND WILL NOT EXCEED THE PLANNED QUANTITY.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

DETECTABLE WARNINGS FOR THE HANDICAPPED SHALL BE INSTALLED AT INTERSECTING STREETS, DRIVEWAYS, AND ALLEYS AT THE LOCATIONS SHOWN ON THE PLANS, SEE DETAIL ON SHEET 16 FOR ADA RAMPS AT PCC SIDEWALK LIMITS.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH ARTICLE 107.25 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES.

FRESH OIL SIGNS SHALL BE POSTED AT BOTH ENDS OF THE ROADWAY AND ALL SIDE STREETS AS DIRECTED BY THE ENGINEER. CONSTRUCTION AHEAD SIGNS SHALL BE PLACED AT ALL SIDE STREETS AND BOTH ENDS OF THE ROADWAY WHILE CONSTRUCTION IS IN PROGRESS.

CONTRACTOR SHALL NOT PLACE SOD UNTIL THE TEMPERATURE IS 80° OR LESS AND THE FORECAST FOR THE NEXT 7 DAYS SHOWS TEMPERATURES OF 80° OR LESS. IF ALL OTHER PAY ITEMS ARE COMPLETED, THE CONTRACTOR WILL NOT BE CHARGED WORKING DAYS FOR DELAYS IN PARKWAY RESTORATION DUE TO TEMPERATURE.

NO CONSTRUCTION SHALL BEGIN UNTIL ALL PROPER TEMPORARY SIGNS AND BARRICADES HAVE BEEN INSTALLED.

AT NO TIME SHALL LESS THAN HALF OF THE STREET BE AVAILABLE FOR PARKING.

ALL ROADS MUST HAVE ONLY ONE LONGITUDINAL JOINT WHILE PAVING.

VANDALISM - SPECIAL ATTENTION IS CALLED TO THE SPECIAL PROVISION FOR "INSPECTION" AS WELL AS ARTICLE 107.30 OF THE "STANDARD SPECIFICATIONS." ANY DEFACED WORK AS DETERMINED AND DIRECTED BY THE CITY SHALL BE CORRECTED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THE CITY OF ZION WILL COOPERATE WITH THE CONTRACTOR TO MINIMIZE VANDALISM, BUT THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE TO CORRECT ANY DAMAGE. THE CITY WILL NOT BE RESPONSIBLE FOR THE SECURITY OF THE WORK SITE IN THIS REGARD, OTHER THAN NORMAL PATROLLING AND REPONSE TO EMERGENCIES.

ALL DETECTABLE WARNING TACTILES SHALL BE FIELD VERIFIED AND THE SIZE TO BE DETERMINED BY THE CONTRACTOR PRIOR TO ORDERING.

INDEX OF SHEETS

HIGHWAY STANDARDS AND DISTRICT ONE DETAILS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-10	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
604001-04	FRAME AND LIDS TYPE 1
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER, OR CROSSWALK CLOSURE
701901-07	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS, & DELINEATORS
729001-01	APPLICATIONS OF TYPE A&B METAL POSTS (FOR SIGNS & MARKERS)
780001-05	TYPICAL PAVEMENT MARKINGS
BD-08	FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
BD-24	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TS-07	DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

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		DRAWN - JEH	REVISED -
	PLOT SCALE = NOT TO SCALE	CHECKED - LMF	REVISED -
	PLOT DATE = 11/8/2017	DATE - 7/28/17	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

21st ST. AND 29th ST.
GENERAL NOTES AND HIGHWAY STANDARDS

SCALE: N.T.S. | SHEET NO. 2 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	2
		CONTRACT NO. 61E13		
		ILLINOIS FED. AID PROJECT		

SUMMARY OF QUANTITIES

				CONSTRUCTION CODE 0005
	CODE NO.	ITEM	UNIT	TOTAL QUANTITY
	20200100	EARTH EXCAVATION	CU YD	5
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	300
	21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	1,350
	25200100	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	400
	25200110	SODDING, SALT TOLERANT	SQ YD	400
	28000500	INLET AND PIPE PROTECTION	EACH	20
~	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	300
	35800100	PREPARATION OF BASE	SQ YD	8,539
	35800200	AGGREGATE BASE REPAIR	TON	230
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	14,000
	40600400	MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS	TON	25
	40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	252
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	673
	40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N-50	TON	1,184
	40603335	HOT MIX ASPHALT SURFACE COURSE, "MIX D", N50	TON	1,479
~	42400800	DETECTABLE WARNING	SQ FT	180
	44000600	SIDEWALK REMOVAL	SQ FT	1,905
~	44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	350
~	44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	350
~	44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	350
~	44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	350
	60404300	FRAMES AND GRATES, TYPE 3	EACH	15
~	60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	5
	60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	3
	67100100	MOBILIZATION	LSUM	1
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LSUM	1
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	3,020
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1,800
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	50
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	850
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,600
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	300
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	250
*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
~*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	125
~	X0326862	STRUCTURES TO BE ADJUSTED	EACH	18
~	X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	1,905
~	X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	13,162
~	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	24
~	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	582
~	Z0013798	CONSTRUCTION LAYOUT	LSUM	1
~	X0003435	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT	SQ YD	7
~	X0006947	HOT-MIX ASPHALT DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT	SQ YD	62

*INDICATES SPECIALTY ITEM
~ INDICATES SPECIAL PROVISION

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		DRAWN - JEH	REVISED -
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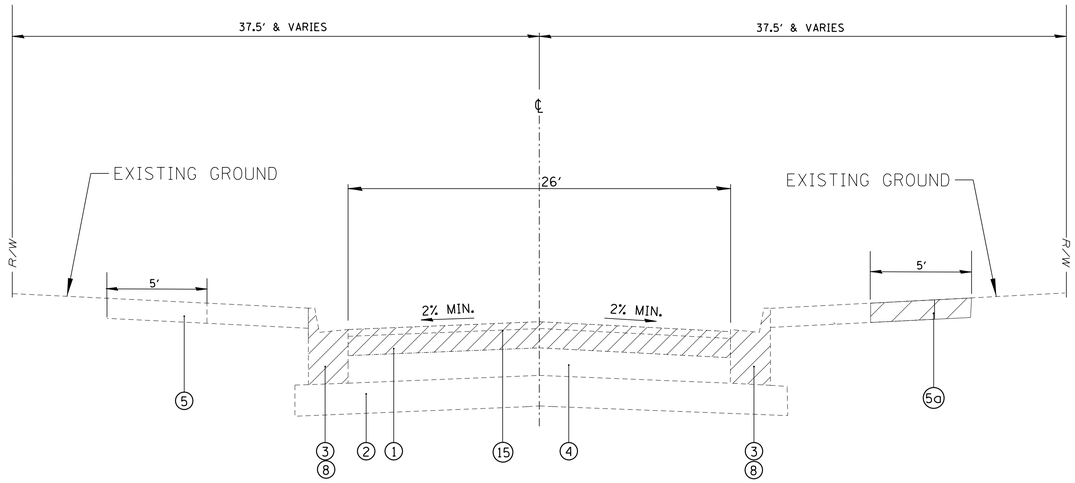
	PLOT DATE = 11/8/2017	DATE - 7/28/17	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

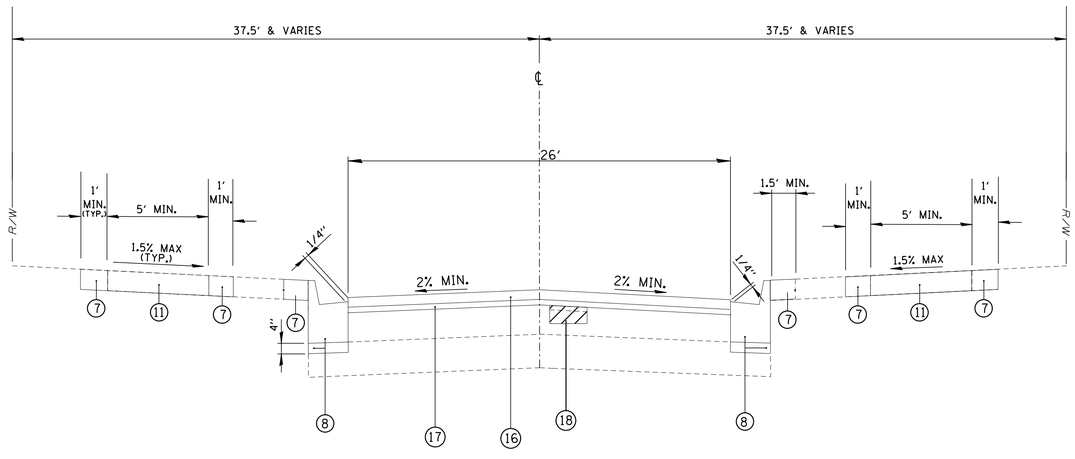
21st STREET AND 29th STREET
SUMMARY OF QUANTITIES

SCALE: N.T.S. | SHEET NO. 3 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	3
		CONTRACT NO. 61E13		
	ILLINOIS FED. AID PROJECT			



EXISTING TYPICAL SECTION
STA. 51+58 TO STA. 52+94 - 21ST STREET



PROPOSED TYPICAL SECTION
STA. 51+58 TO STA. 52+94 - 21ST STREET

NOTES:

1. PAVING OF THE FULL ROADWAY WIDTH SHALL BE COMPLETED AT THE END OF EACH DAY OF PAVING TO PREVENT A LONGITUDINAL COLD JOINT FROM APPEARING WHEN OPPOSITE SIDES OF THE ROAD ARE PAVED ON DIFFERENT DAYS. THE CONTRACTOR SHALL ALSO ENSURE THAT AT THE END OF EACH DAY EACH PASS ENDS AT APPROXIMATELY THE SAME STATION TO PREVENT A COLD JOINT.
2. ANY AGGREGATE BASE REMOVAL DUE TO PROPOSED ASPHALT SHALL BE CONSIDERED INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH.
3. ANY EXCAVATION OF DIRT/CLAY NECESSARY TO OBTAIN THE NECESSARY DEPTH FOR THE PROPOSED PAVEMENT SHALL BE PAY FOR AS REMOVAL AND AND DISPOSAL OF UNSUITABLE MATERIAL.

USES	HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
	ITEM	VOIDS
ROADWAY RESURF. AND DRIVEWAYS	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 2"	4% @ 50 GYR.
ROADWAY RESURF.	HMA BINDER COURSE, IL-19.0, N50, 2.25"	4% @ 50 GYR.
ROADWAY RESURF. AND DRIVEWAYS	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 1.5"	4% @ 50 GYR.
ROADWAY RESURF.	LEVELING BINDER (MACHINE METHOD), N50 - 0.75"	4% @ 50 GYR.
ROADWAY PATCHING	CLASS D PATCHES, 8 INCH, SPECIAL	4% @ 70 GYR.

MIXTURE REQUIREMENT NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE IS 112 LBS/SY/IN. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
2. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

LEGEND

1. EXISTING HOT-MIX ASPHALT PAVEMENT
2. EXISTING AGGREGATE SUBBASE
3. EXISTING CURB AND GUTTER
4. EXISTING AGGREGATE BASE
5. EXISTING PCC SIDEWALK
5a. SIDEWALK REMOVAL
6. HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (4.25"-5.25")
7. SODDING, SALT TOLERANT & TOPSOIL FUNISH AND PLACE, 6"
8. COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER), INCLUDES 4" SUBBASE GRANULAR MATERIAL TYPE B.
9. HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2"
10. HMA BINDER COURSE, IL - 19.0, N50 - 2.25"
11. PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL AS DIRECTED BY THE ENGINEER) (SIDEWALKS THROUGH DRIVEWAYS SHALL BE 6 INCHES THICK - THIS WORK WILL BE INCLUDED IN THE PAY ITEM FOR PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH, SPECIAL)
12. •REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL •GEOTECHNICAL FABRIC FOR GROUND STABILIZATION •AGGREGATE SUBGRADE IMPROVEMENT
13. PREPARATION OF BASE
14. AGGREGATE BASE REPAIR
15. HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (2"-3.5")
16. HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 1.5"
17. LEVELING BINDER (MACHINE METHOD), N50 - 0.75"
18. CLASS D PATCHES, 8 INCH, TYPE I-IV

CORE DETAILS

CORE NUMBER (AS SHOWN IN REPORT)	ASPHALT TOTAL THICKNESS	SUBBASE THICKNESS	CORE STATION
1A	5.5"	36"	STA. 51+62
1	8.25"	30"	STA. 52+35
2	9.5"	18"	STA. 59+61
3	5.5"	24"	STA. 65+35
AVG.	7.2"	27"	

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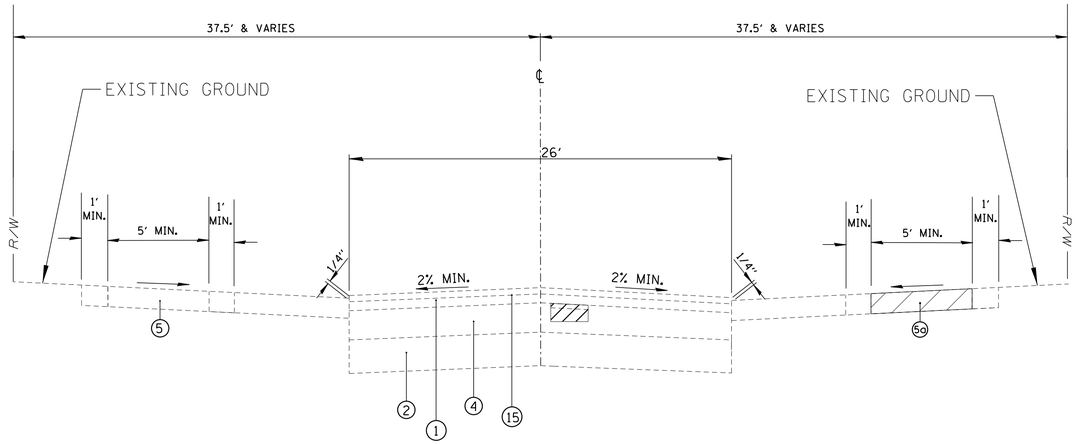
		DRAWN - JEH	REVISED -
	PLOT SCALE = NOT TO SCALE	CHECKED - LMF	REVISED -
	PLOT DATE = 11/8/2017	DATE - 7/28/17	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

21st STREET
TYPICAL SECTIONS

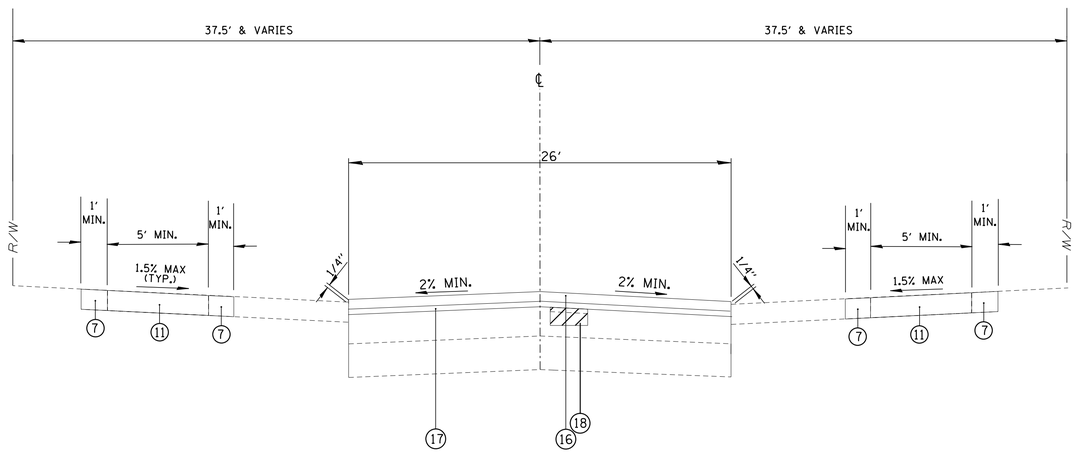
SCALE: N.T.S. | SHEET NO. 4 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	4
		CONTRACT NO. 61E13		
		ILLINOIS FED. AID PROJECT		



EXISTING TYPICAL SECTION

STA. 52+94 TO STA. 66+64 - 21ST STREET



PROPOSED TYPICAL SECTION

STA. 52+94 TO STA. 66+64 - 21ST STREET

NOTES:

1. PAVING OF THE FULL ROADWAY WIDTH SHALL BE COMPLETED AT THE END OF EACH DAY OF PAVING TO PREVENT A LONGITUDINAL COLD JOINT FROM APPEARING WHEN OPPOSITE SIDES OF THE ROAD ARE PAVED ON DIFFERENT DAYS. THE CONTRACTOR SHALL ALSO ENSURE THAT AT THE END OF EACH DAY EACH PASS ENDS AT APPROXIMATELY THE SAME STATION TO PREVENT A COLD JOINT.
2. ANY AGGREGATE BASE REMOVAL DUE TO PROPOSED ASPHALT SHALL BE CONSIDERED INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH.
3. ANY EXCAVATION OF DIRT/CLAY NECESSARY TO OBTAIN THE NECESSARY DEPTH FOR THE PROPOSED PAVEMENT SHALL BE PAY FOR AS REMOVAL AND AND DISPOSAL OF UNSUITABLE MATERIAL.

USES	HOT-MIX ASPHALT MIXTURE REQUIREMENTS ITEM	VOIDS
ROADWAY RESURF. AND DRIVEWAYS	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 2"	4% @ 50 GYR.
ROADWAY RESURF.	HMA BINDER COURSE, IL-19.0, N50, 2.25"	4% @ 50 GYR.
ROADWAY RESURF. AND DRIVEWAYS	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 1.5"	4% @ 50 GYR.
ROADWAY RESURF.	LEVELING BINDER (MACHINE METHOD), N50 - 0.75"	4% @ 50 GYR.
ROADWAY PATCHING	CLASS D PATCHES, 8 INCH, SPECIAL	4% @ 70 GYR.

MIXTURE REQUIREMENT NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE IS 112 LBS/SY/IN. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
2. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

LEGEND

1. EXISTING HOT-MIX ASPHALT PAVEMENT
2. EXISTING AGGREGATE SUBBASE
3. EXISTING CURB AND GUTTER
4. EXISTING AGGREGATE BASE
5. EXISTING PCC SIDEWALK
5a. SIDEWALK REMOVAL
6. HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (4.25"-5.25")
7. SODDING, SALT TOLERANT & TOPSOIL FUNISH AND PLACE, 6"
8. COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER). INCLUDES 4" SUBBASE GRANULAR MATERIAL TYPE B.
9. HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2"
10. HMA BINDER COURSE, IL - 19.0, N50 - 2.25"
11. PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL AS DIRECTED BY THE ENGINEER) (SIDEWALKS THROUGH DRIVEWAYS SHALL BE 6 INCHES THICK - THIS WORK WILL BE INCLUDED IN THE PAY ITEM FOR PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH, SPECIAL)
12. •REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL •GEOTECHNICAL FABRIC FOR GROUND STABILIZATION •AGGREGATE SUBGRADE IMPROVEMENT
13. PREPARATION OF BASE
14. AGGREGATE BASE REPAIR
15. HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (2"-3.5")
16. HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 1.5"
17. LEVELING BINDER (MACHINE METHOD), N50 - 0.75"
18. CLASS D PATCHES, 8 INCH, TYPE I-IV

CORE DETAILS

CORE NUMBER (AS SHOWN IN REPORT)	ASPHALT TOTAL THICKNESS	SUBBASE THICKNESS	CORE STATION
1A	5.5"	36"	STA. 51+62
1	8.25"	30"	STA. 52+35
2	9.5"	18"	STA. 59+61
3	5.5"	24"	STA. 65+35
AVG.	7.2"	27"	

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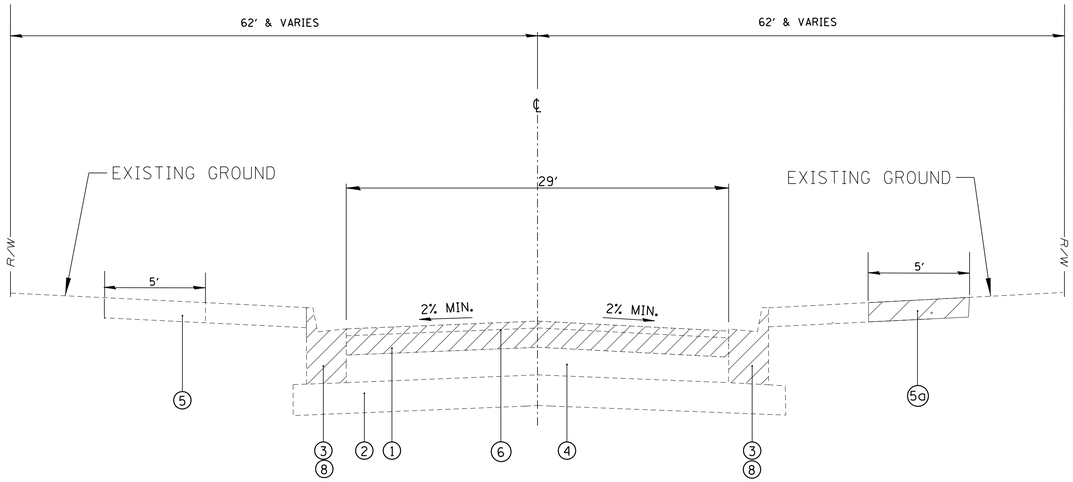
	PLOT DATE = 11/8/2017	DATE - 7/28/17	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

21st STREET
TYPICAL SECTIONS
(FOR PORTIONS WITHOUT CURB AND GUTTER)

SCALE: N.T.S. | SHEET NO. 5 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	5
		CONTRACT NO. 61E13		
		ILLINOIS FED. AID PROJECT		



EXISTING TYPICAL SECTION
STA. 11+69 TO STA. 36+82 - 29TH STREET

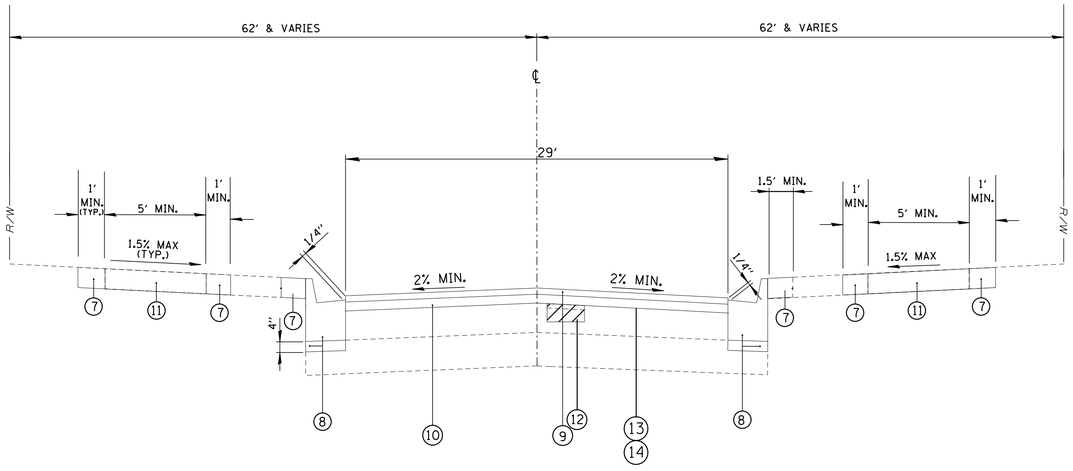
NOTES:

1. PAVING OF THE FULL ROADWAY WIDTH SHALL BE COMPLETED AT THE END OF EACH DAY OF PAVING TO PREVENT A LONGITUDINAL COLD JOINT FROM APPEARING WHEN OPPOSITE SIDES OF THE ROAD ARE PAVED ON DIFFERENT DAYS. THE CONTRACTOR SHALL ALSO ENSURE THAT AT THE END OF EACH DAY EACH PASS ENDS AT APPROXIMATELY THE SAME STATION TO PREVENT A COLD JOINT.
2. ANY AGGREGATE BASE REMOVAL DUE TO PROPOSED ASPHALT SHALL BE CONSIDERED INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH.
3. ANY EXCAVATION OF DIRT/CLAY NECESSARY TO OBTAIN THE NECESSARY DEPTH FOR THE PROPOSED PAVEMENT SHALL BE PAY FOR AS REMOVAL AND AND DISPOSAL OF UNSUITABLE MATERIAL.

USES	HOT-MIX ASPHALT MIXTURE REQUIREMENTS ITEM	VOIDS
ROADWAY RESURF.	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 2"	4% @ 50 GYR.
ROADWAY RESURF.	HMA BINDER COURSE, IL-19.0, N50, 2.25"	4% @ 50 GYR.

MIXTURE REQUIREMENT NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE IS 112 LBS/SY/IN. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
2. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.



PROPOSED TYPICAL SECTION
STA. 11+69 TO STA. 36+82 - 29TH STREET

LEGEND

1. EXISTING HOT-MIX ASPHALT PAVEMENT
2. EXISTING AGGREGATE SUBBASE
3. EXISTING CURB AND GUTTER
4. EXISTING AGGREGATE BASE
5. EXISTING PCC SIDEWALK
5a. SIDEWALK REMOVAL
6. HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (4.25"-5.25")
7. SODDING, SALT TOLERANT & TOPSOIL FUNISH AND PLACE, 6"
8. COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER). INCLUDES 4" SUBBASE GRANULAR MATERIAL TYPE B.
9. HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2"
10. HMA BINDER COURSE, IL - 19.0, N50 - 2.25"
11. PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL AS DIRECTED BY THE ENGINEER) (SIDEWALKS THROUGH DRIVEWAYS SHALL BE 6 INCHES THICK - THIS WORK WILL BE INCLUDED IN THE PAY ITEM FOR PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH, SPECIAL)
12. •REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL •GEOTECHNICAL FABRIC FOR GROUND STABILIZATION •AGGREGATE SUBGRADE IMPROVEMENT
13. PREPARATION OF BASE
14. AGGREGATE BASE REPAIR
15. HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (2"-3.5")
16. HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 1.5"
17. LEVELING BINDER (MACHINE METHOD), N50 - 0.75"
18. CLASS D PATCHES, 8 INCH, TYPE I-IV

CORE DETAILS

CORE NUMBER (AS SHOWN IN REPORT)	ASPHALT TOTAL THICKNESS	SUBBASE THICKNESS	CORE STATION
4	5.25"	21"	STA. 12+21
5	4.25"	15"	STA. 17+40
6	4.25"	16"	STA. 22+43
7	5.25"	17"	STA. 27+41
8	4.25"	19"	STA. 32+61
AVG.	4.65"	17.6"	

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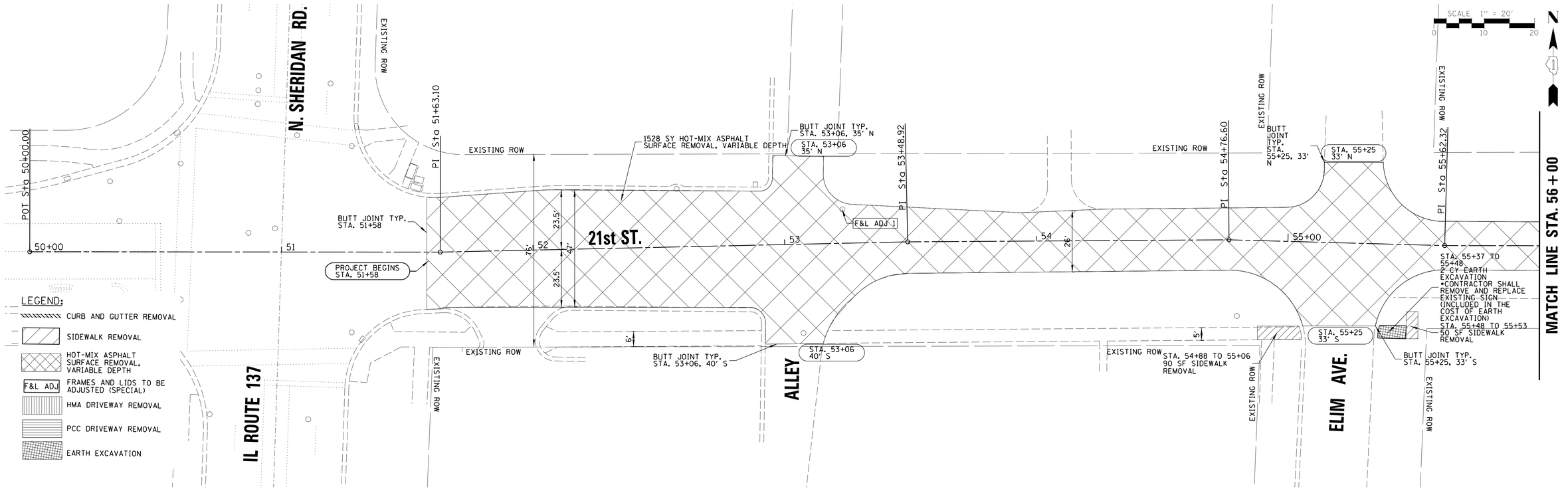
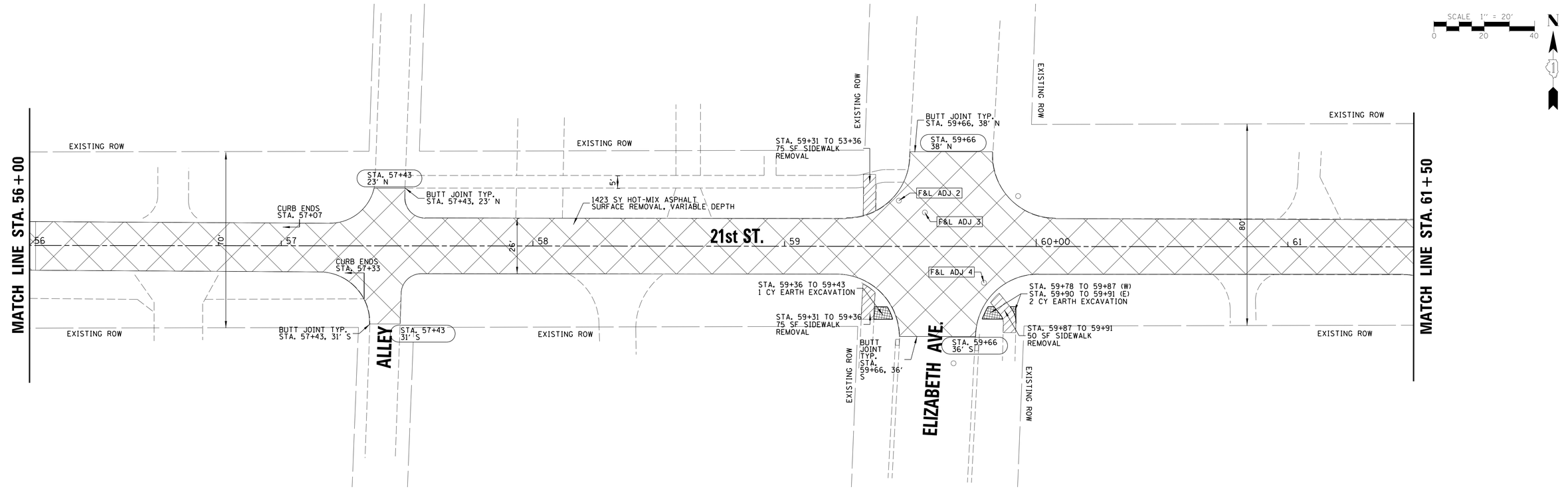
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

29th STREET
TYPICAL SECTIONS

SCALE: N.T.S. | SHEET NO. 6 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	6
		CONTRACT NO. 61E13		
		ILLINOIS FED. AID PROJECT		





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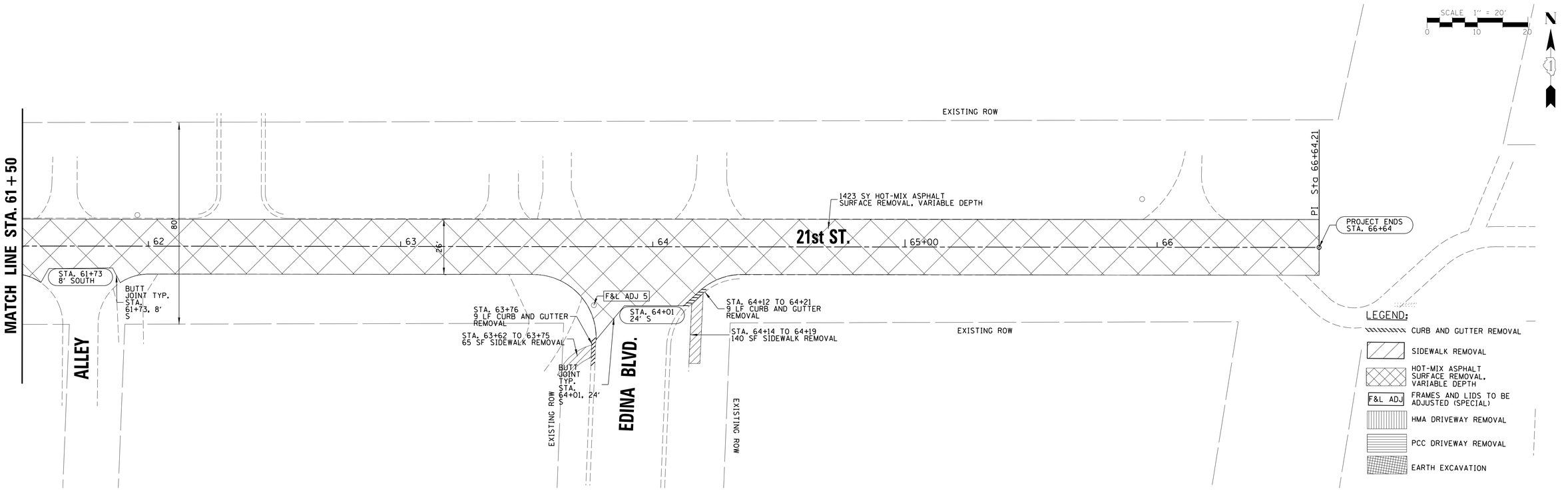
	PLOT DATE = 11/8/2017	DATE - 7/31/17	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

21st STREET
EXISTING CONDITIONS AND REMOVAL PLAN

SCALE: 20' | SHEET NO. 7 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	7
		CONTRACT NO. 61E13		
		ILLINOIS FED. AID PROJECT		



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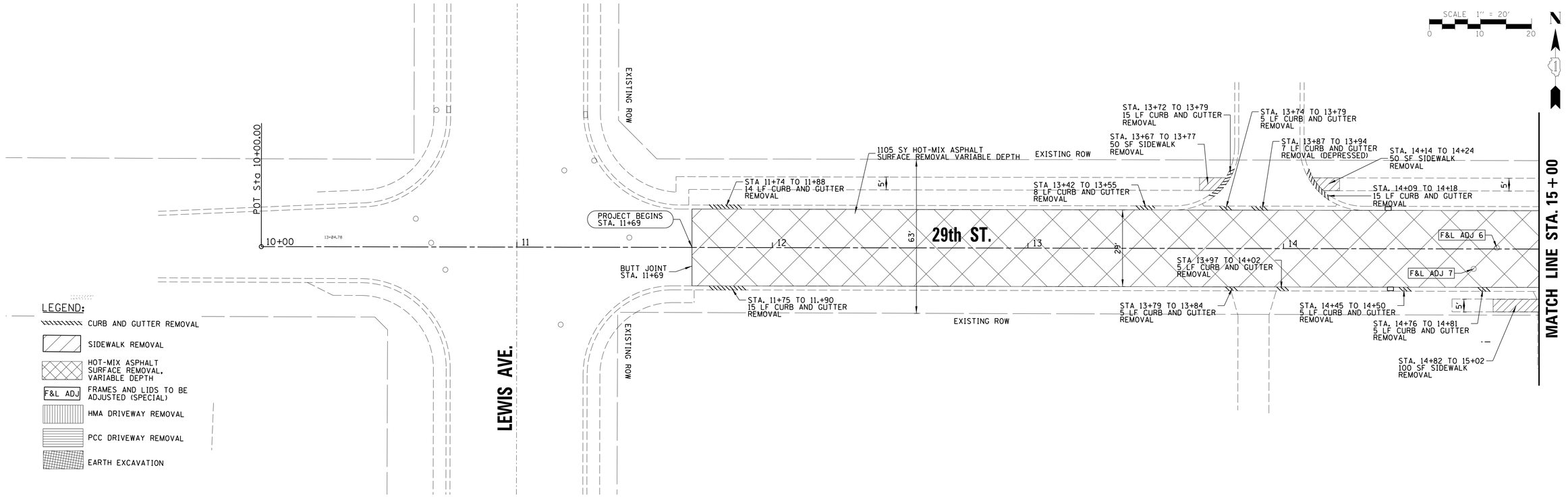
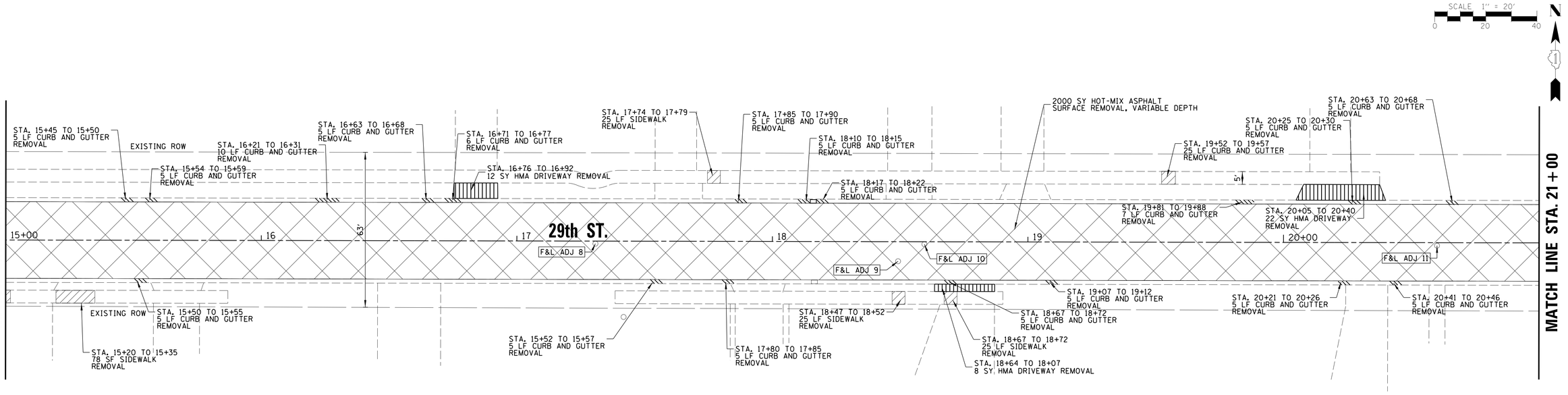
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

21st STREET
EXISTING CONDITIONS AND REMOVAL PLAN

SCALE: 20' | SHEET NO. 8 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	8
		CONTRACT NO. 61E13		
	ILLINOIS FED. AID PROJECT			





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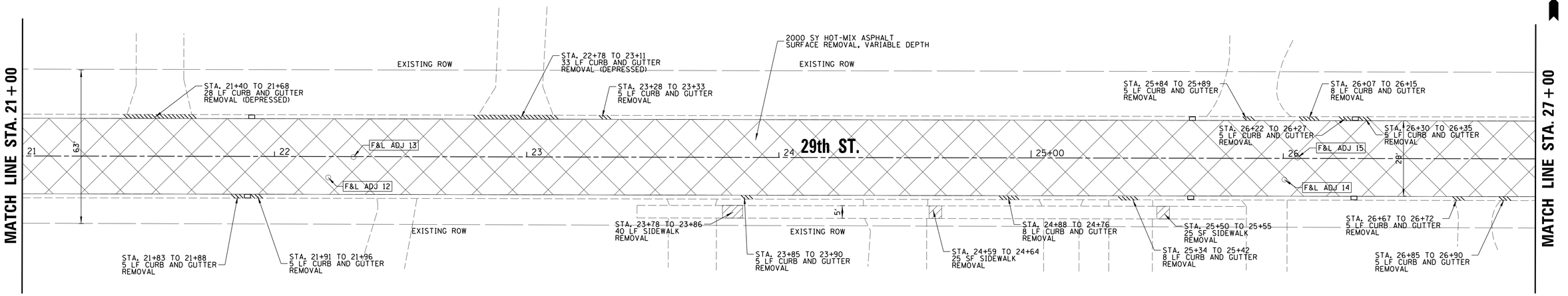
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

29th STREET
EXISTING CONDITIONS AND REMOVAL PLAN

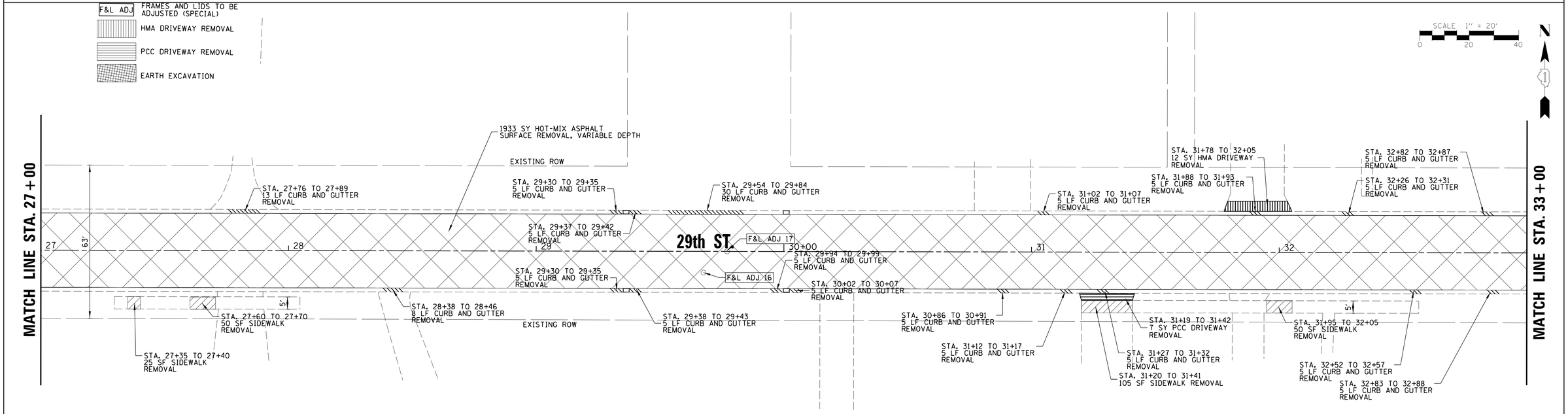
SCALE: 20' | SHEET NO. 9 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	9
		CONTRACT NO. 61E13		
		ILLINOIS FED. AID PROJECT		



LEGEND:

- CURB AND GUTTER REMOVAL
- SIDEWALK REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- F&L ADJ — FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
- HMA DRIVEWAY REMOVAL
- PCC DRIVEWAY REMOVAL
- EARTH EXCAVATION



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

29th STREET
EXISTING CONDITIONS AND REMOVAL PLAN

SCALE: 20' | SHEET NO. 10 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	10
		CONTRACT NO. 61E13		
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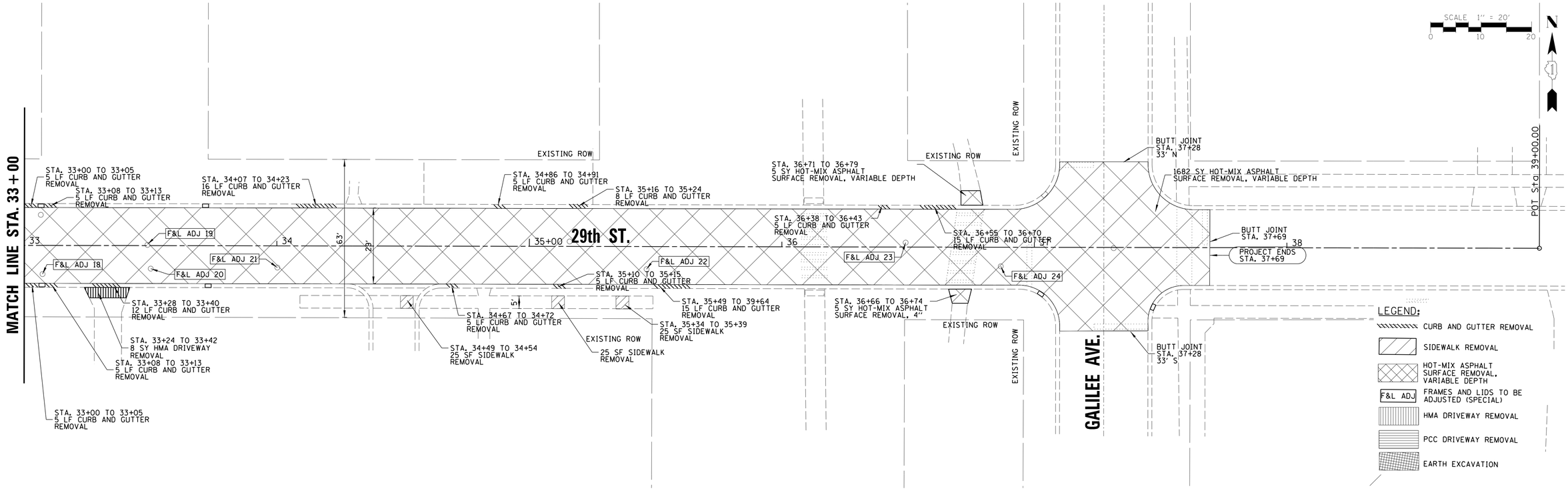

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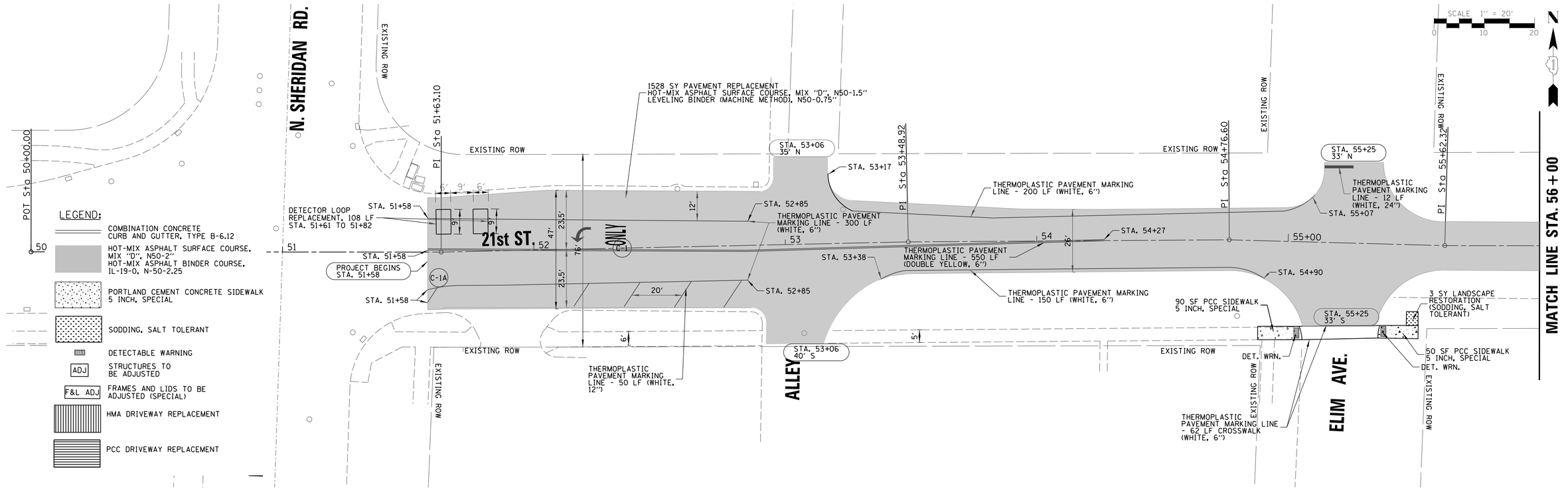
	PLOT DATE = 11/8/2017	DATE - 7/31/17	REVISED -

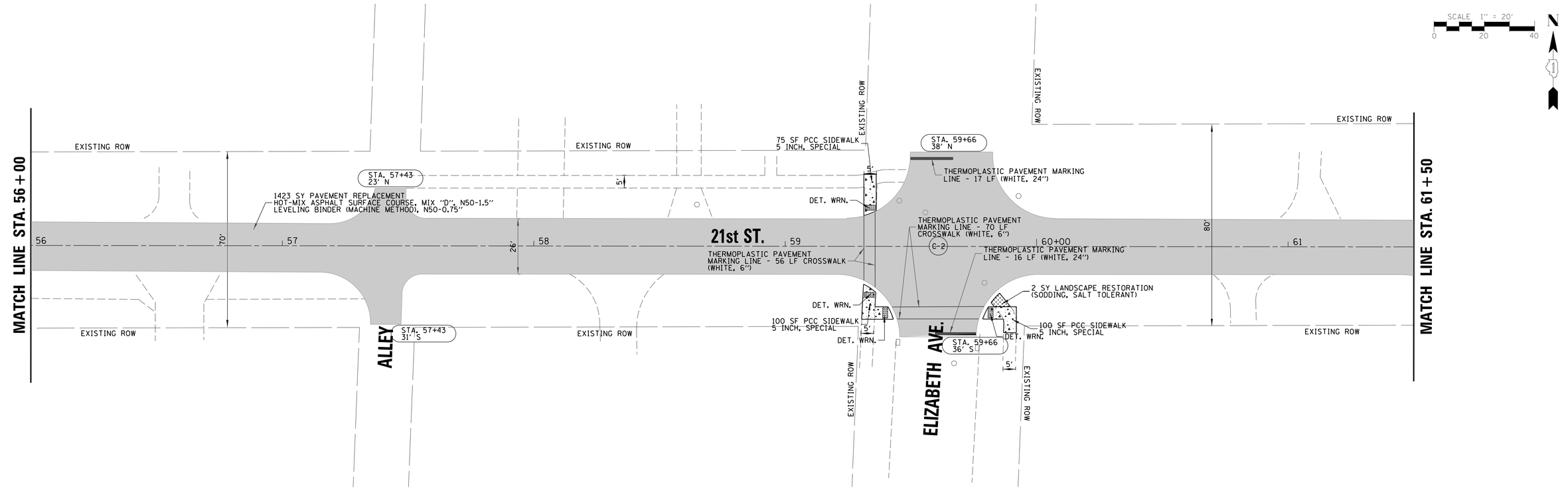
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

29th STREET
EXISTING CONDITIONS AND REMOVAL PLAN

SCALE: 20' | SHEET NO. 11 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	11
		CONTRACT NO. 61E13		
	ILLINOIS FED. AID PROJECT			





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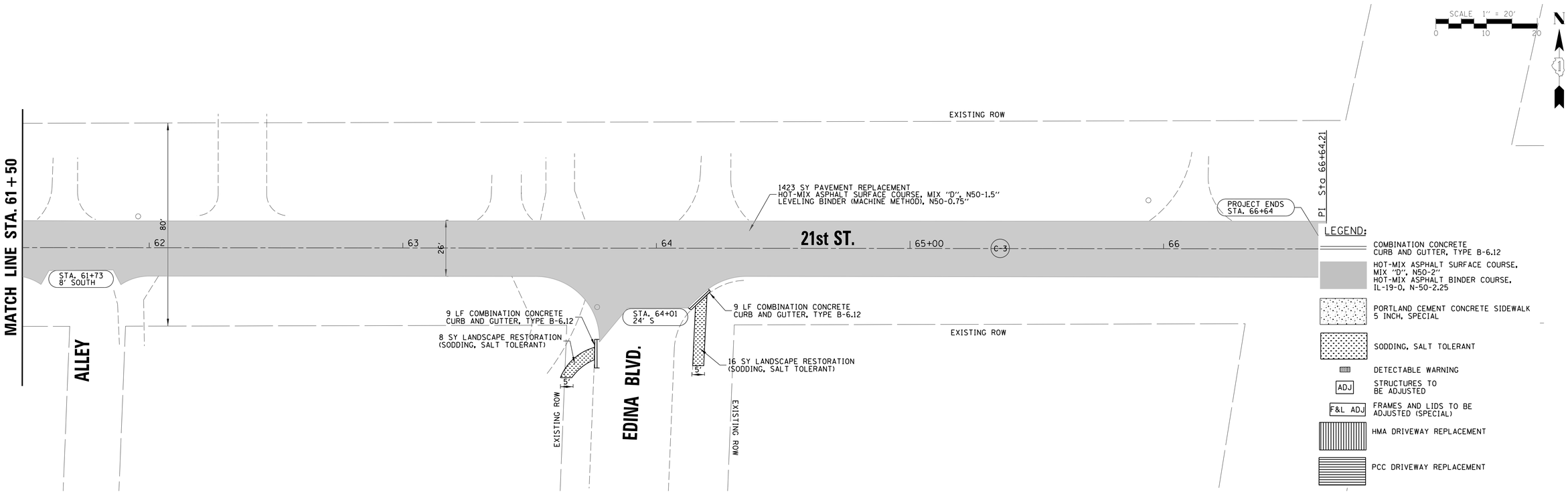
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

21st STREET
PROPOSED PLAN

SCALE: 20' | SHEET NO. 12 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	12
		CONTRACT NO. 61E13		
		ILLINOIS FED. AID PROJECT		



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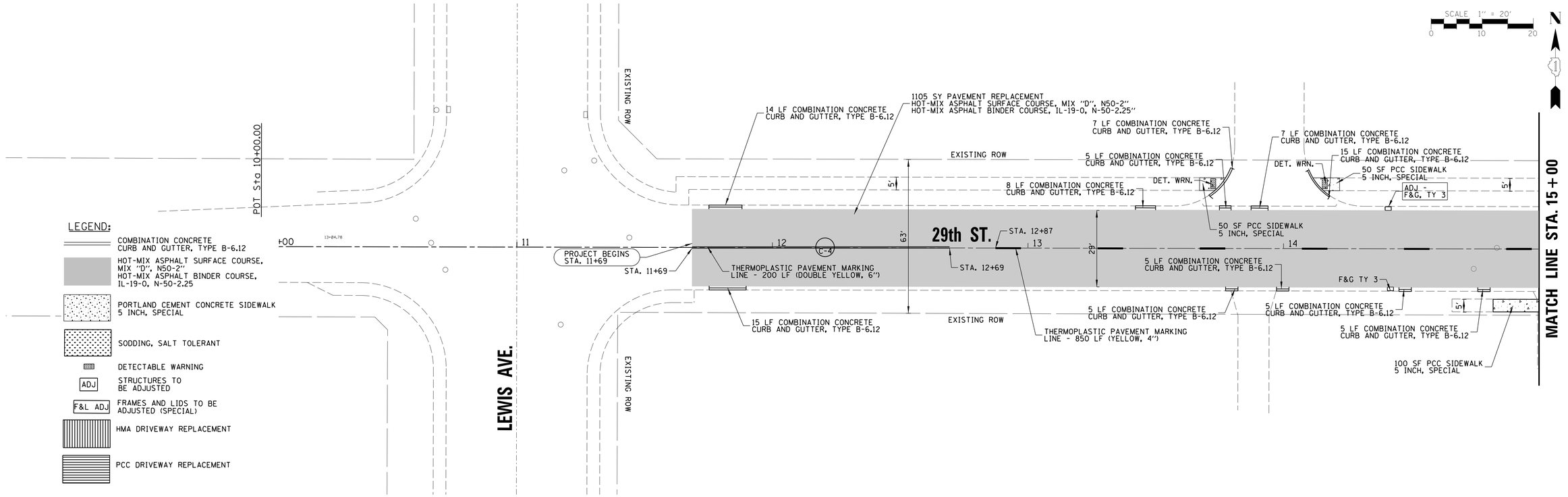
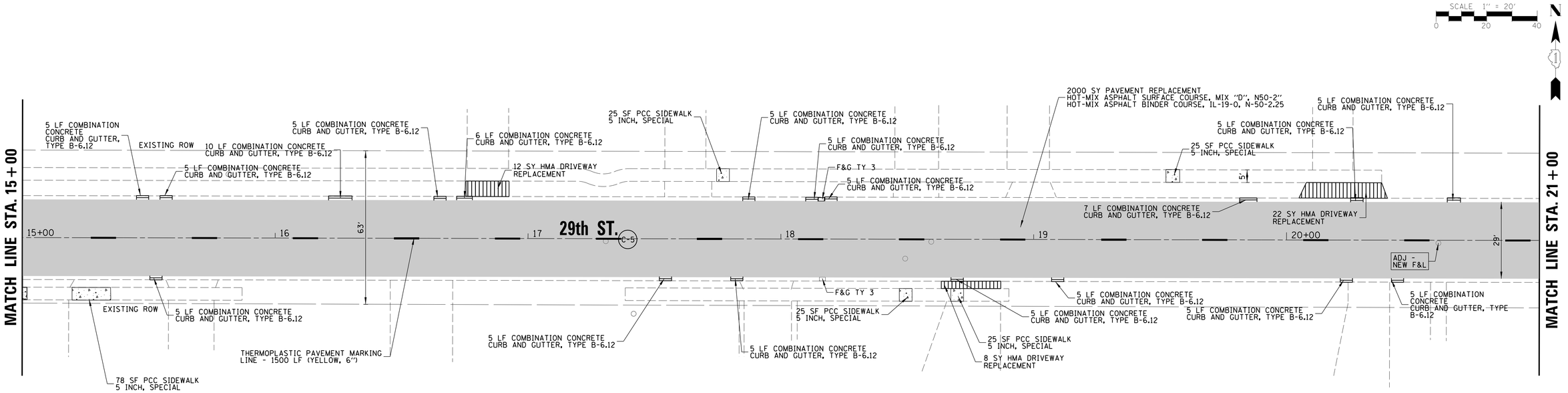
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

21st STREET
PROPOSED PLAN

SCALE: 20' | SHEET NO. 13 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	13
		CONTRACT NO. 61E13		
	ILLINOIS FED. AID PROJECT			





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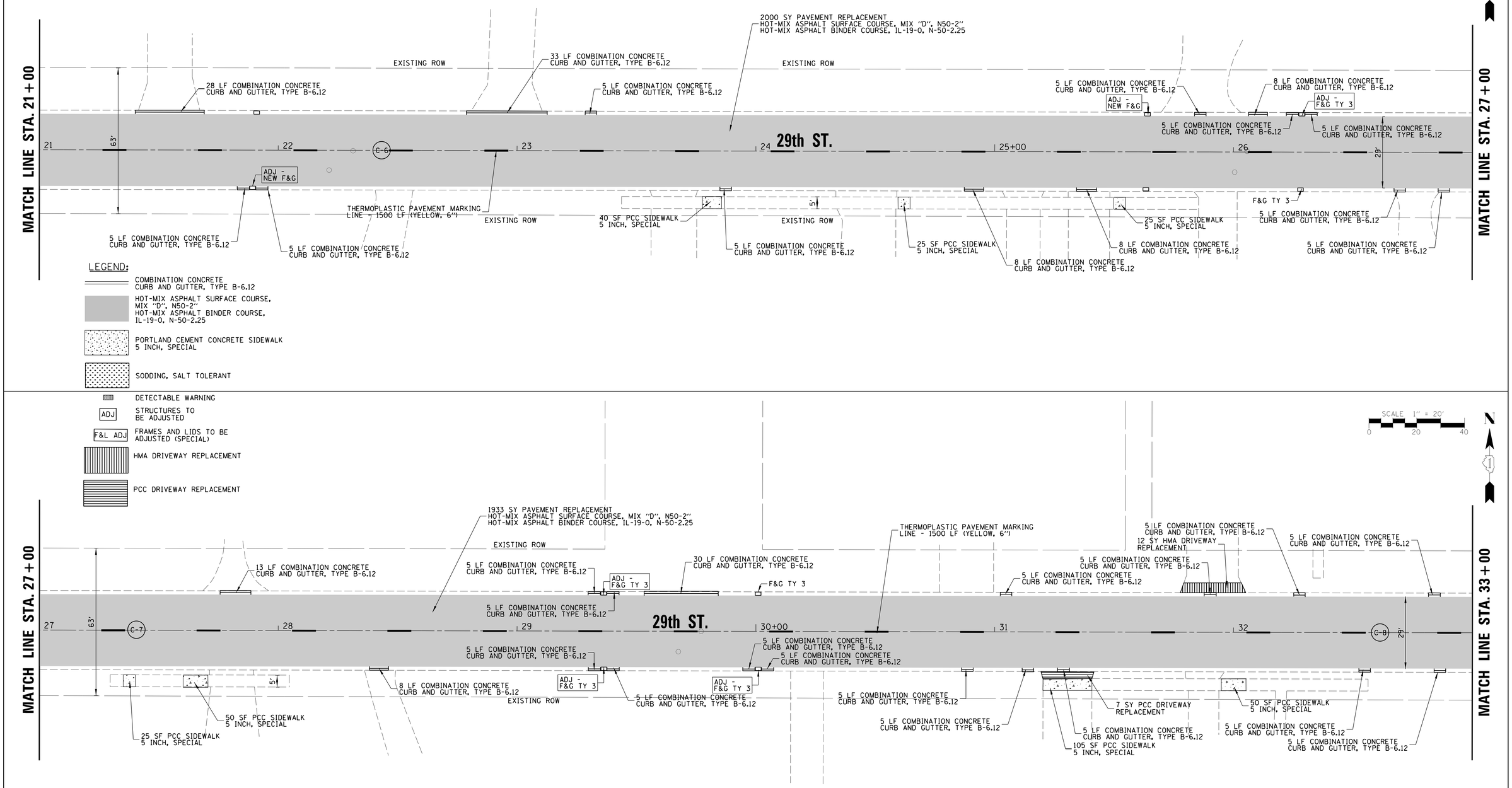
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

29th STREET
PROPOSED PLAN

SCALE: 20' | SHEET NO. 14 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	14
		CONTRACT NO. 61E13		
		ILLINOIS FED. AID PROJECT		



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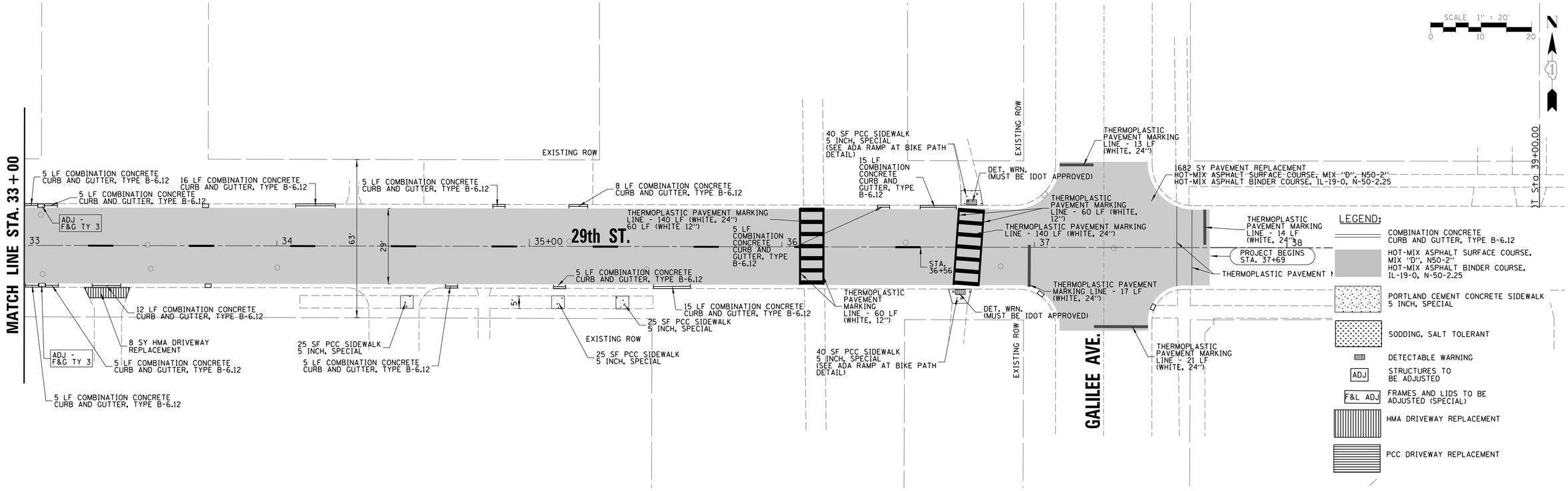
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

29th STREET
PROPOSED PLAN

SCALE: 20' | SHEET NO. 15 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	15
		CONTRACT NO. 61E13		
	ILLINOIS FED. AID PROJECT			



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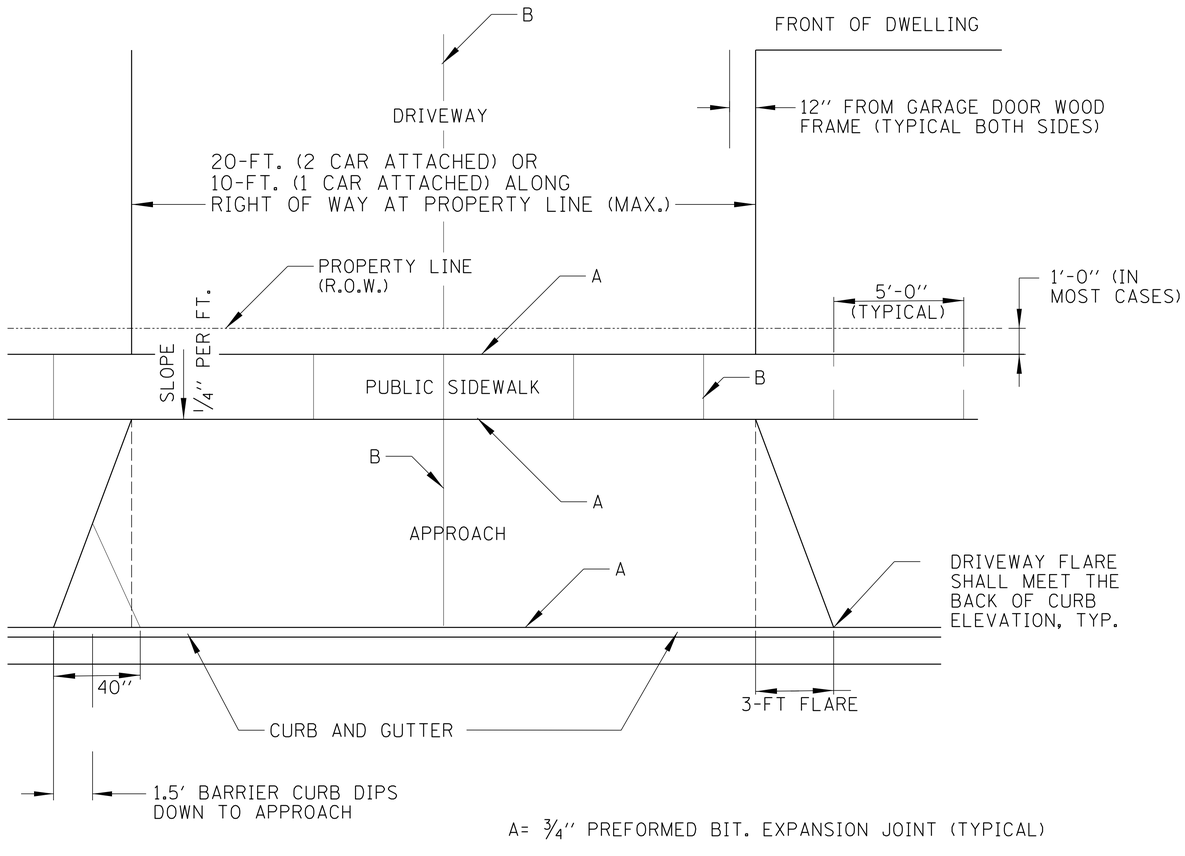
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

29th STREET
PROPOSED PLAN

SCALE: 20' | SHEET NO. 16 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	16
		CONTRACT NO. 61E13		
	ILLINOIS FED. AID PROJECT			

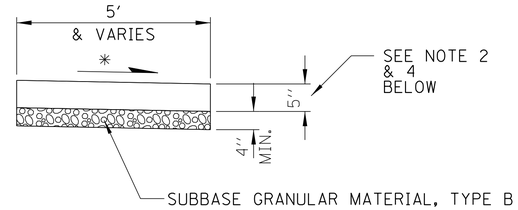


A= ¾'' PREFORMED BIT. EXPANSION JOINT (TYPICAL)

B= TOLLED OR SAWED CONTRACTION JOINTS

DRIVEWAY WITH A CURB AND GUTTER

GENERAL NOTES:
1. DRIVEWAY SHALL HAVE A MIN. SLOPE OF 2% AND MAX. SLOPE OF 6%.
2. APPROACH SHALL HAVE A MIN. SLOPE OF 2% AND MAX. OF 6%.
3. ALL AGGREGATE SUB-BASE SHALL BE MECHANICALLY COMPACTED. (95% PROCTOR)
4. PUBLIC SIDEWALK SHALL BE 6'' AT RESIDENTIAL DRIVEWAYS AND 8'' AT COMMERCIAL/INDUSTRIAL DRIVEWAYS. (NO WIRE MESH)
5. MINIMUM THICKNESS FOR APPROACH. (NO WIRE MESH). THIS WILL BE PAID FOR BY THE FOLLOWING ITEMS:
   A. 7'' THK. P.C. CONCRETE ON 2'' AGGREGATE BASE COURSE TYPE B OR
   B. 3'' THK. HOT-MIX ASPHALT SURFACE, MIX ''D'' N50 ON 6'' AGGREGATE BASE COURSE TYPE B
6. SODDING, SALT TOLERANT & TOPSOIL FURNISH AND PLACE, 6'' WILL BE PAID FOR SEPARATELY.



* CROSS SLOPE 2% OR AS SHOWN ON CROSS SECTIONS

1. ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK SHALL BE INCLUDED IN THE COST OF P.C.C. SIDEWALK 5 INCH, REMOVE AND REPLACE
2. WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFIELD WITHIN 24 HOURS.
3. SODDING AND TOPSOIL, 6'' (100) RESTORATION WILL BE PAID FOR SEPARATELY
4. PUBLIC SIDEWALK SHALL BE 6'' AT RESIDENTIAL DRIVEWAYS AND 8'' AT COMMERCIAL/INDUSTRIAL DRIVEWAYS.

P.C.C. SIDEWALK DETAIL

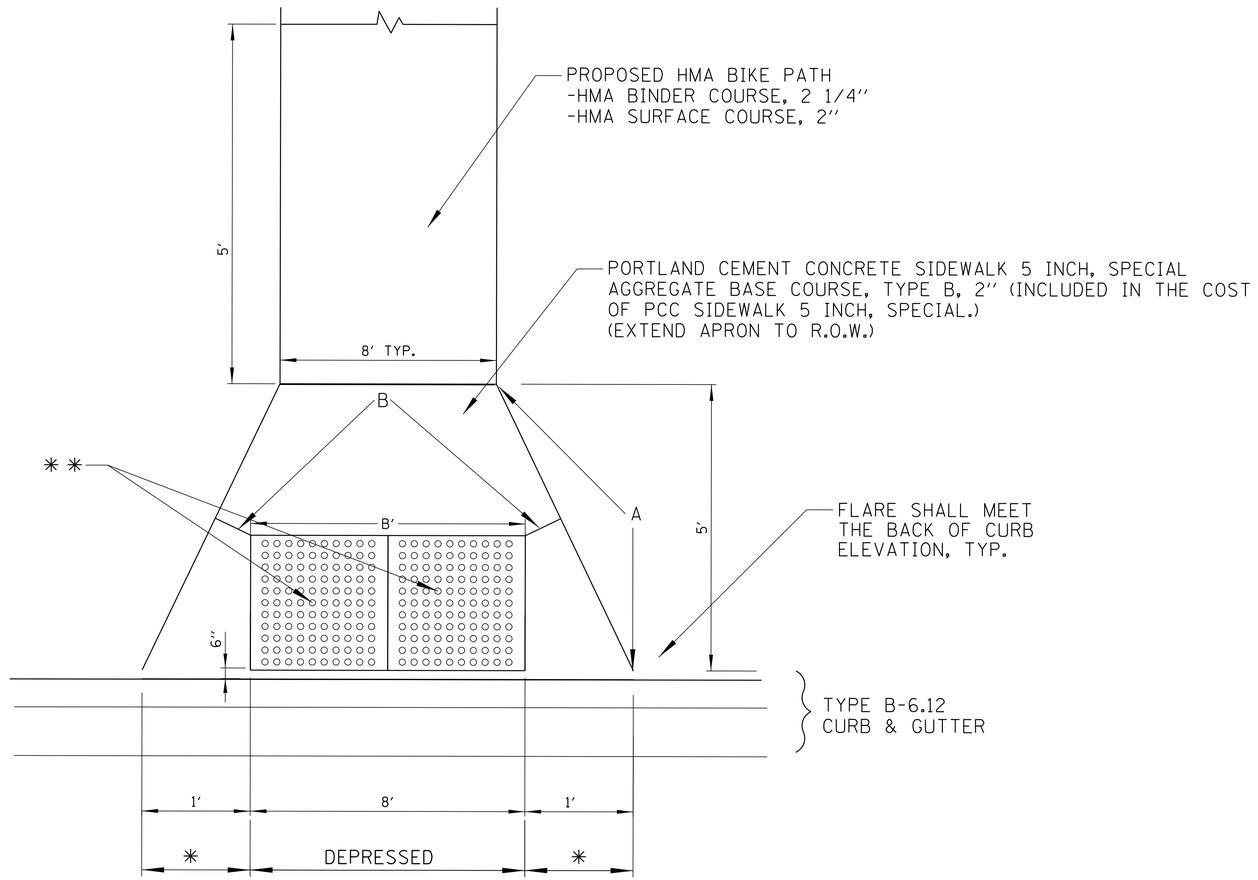
FILE NAME = N:\ZION\170143\Civil\det_170143-01.sht	USER NAME = jhouseh	DESIGNED - JEH	REVISED -
		DRAWN - JEH	REVISED -
	PLOT SCALE = NOT TO SCALE	CHECKED - LMF	REVISED -
	PLOT DATE = 8/4/2017	DATE - 7/28/17	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

SCALE: N.T.S. | SHEET NO. 17 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	17
		CONTRACT NO. 61E13		
		ILLINOIS FED. AID PROJECT		



* 1' TRANSITION FROM DEPRESSED CURB AND GUTTER TO FULL HEIGHT CURB AND GUTTER

** THE BIKE PATH ADA RAMP DETECTABLE WARNINGS MUST BE IDOT APPROVED.

A = ½'' PREFORMED EXPANSION JOINT (TYP.)

B = TOOLED OR SAWED CONTRACTION JOINT

ADA RAMP AT BIKE PATH

NOT TO SCALE

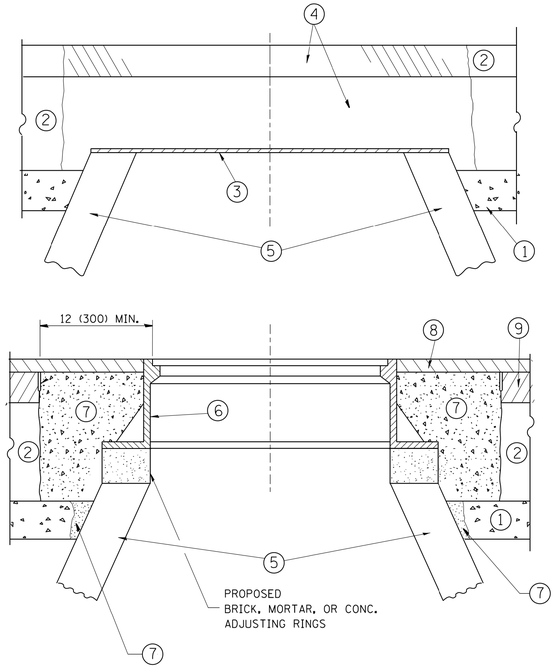
FILE NAME = N:\ZION\170143\Civil\detl_170143.sht	USER NAME = jhouseh	DESIGNED - JEH	REVISED -
		DRAWN - JEH	REVISED -
	PLOT SCALE = NOT TO SCALE	CHECKED - LMF	REVISED -
	PLOT DATE = 8/4/2017	DATE - 7/28/17	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIKE PATH CROSSING DETAIL

SCALE: N.T.S. | SHEET NO. 18 OF 25 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	18
		CONTRACT NO. 61E13		
		ILLINOIS FED. AID PROJECT		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS PP-1* CONCRETE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF ZION CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = c:\pw_work\pwidot\bauerdl\d0108315\bd08.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 1968.5000 ' / m	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

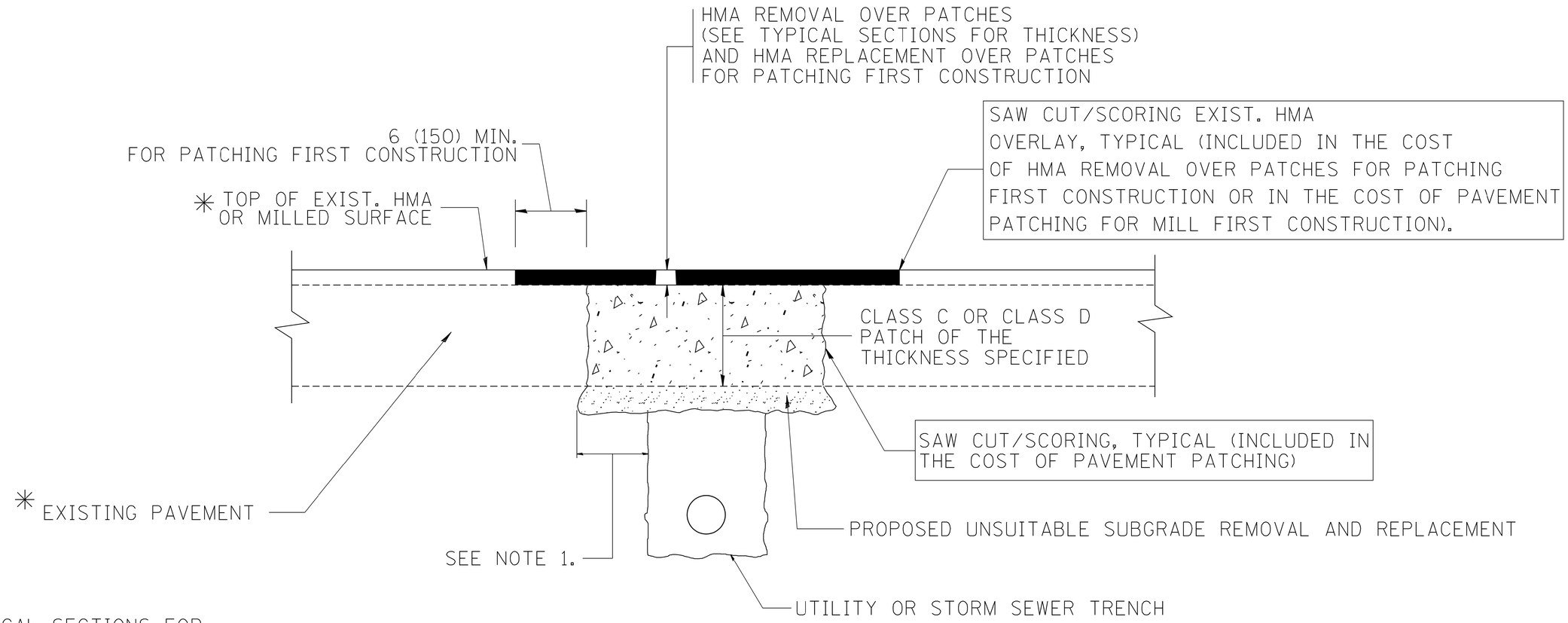
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	19
	BD600-03 (BD-8)	CONTRACT NO. 61E13		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

bd08.dgn 12/6/2011 10:53:13 AM User=bauerdl



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST $4\frac{1}{2}$ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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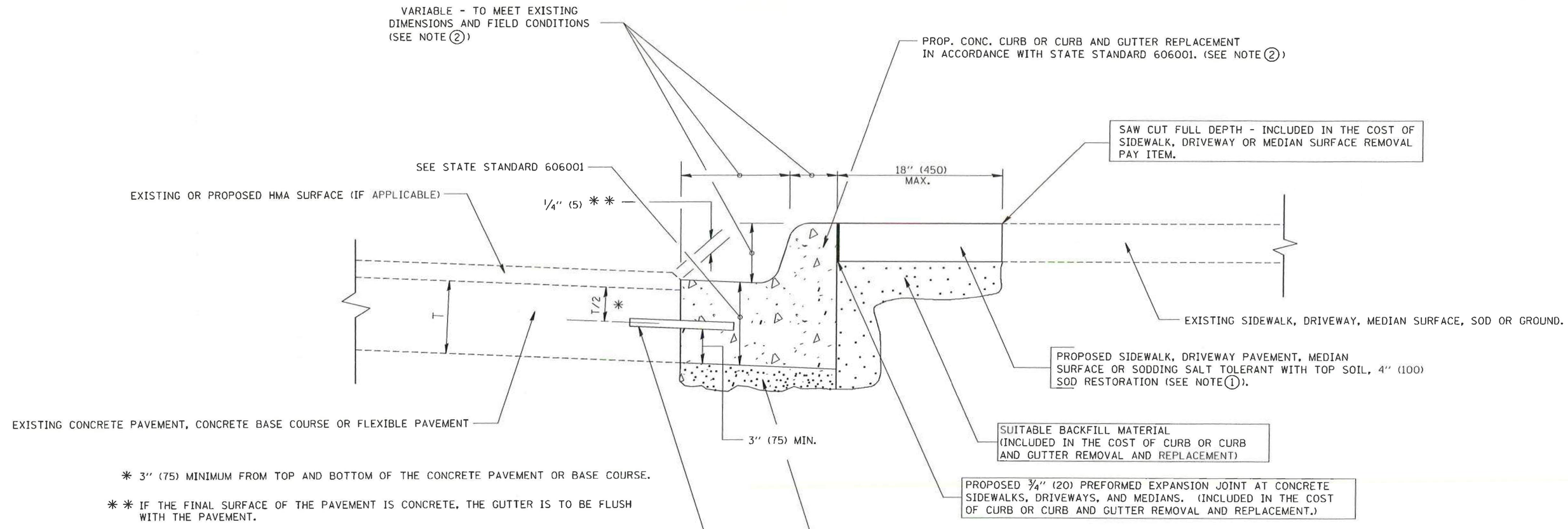
		DRAWN -	REVISED - R. BORO 01-01-07			1207 AND 1209	17-00088-00-RS	LAKE	25	20
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - R. BORO 09-04-07				BD400-04 (BD-22)		CONTRACT NO. 61E13	
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

(2) FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

(3) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(4) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(5) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(7) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(8) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

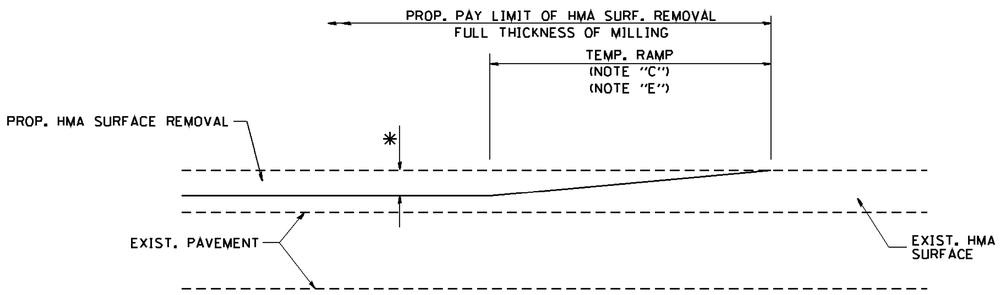
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\pw_work\pwidot\drivakosgn\d0108315\bd24.dgn	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
		DRAWN -	REVISED - A. ABBAS 03-21-97		
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01		SCALE: NONE / SHEET NO. 1 OF 1 SHEETS / STA. / TO STA.
	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09		

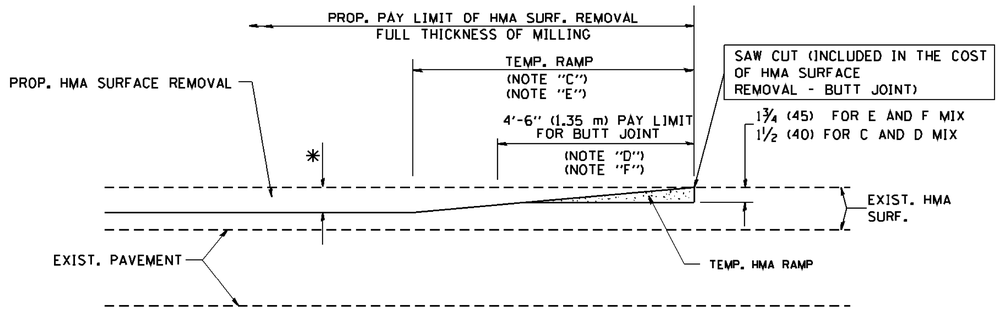
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	21
	BD600-06 (BD-24)	CONTRACT NO. 61E13		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

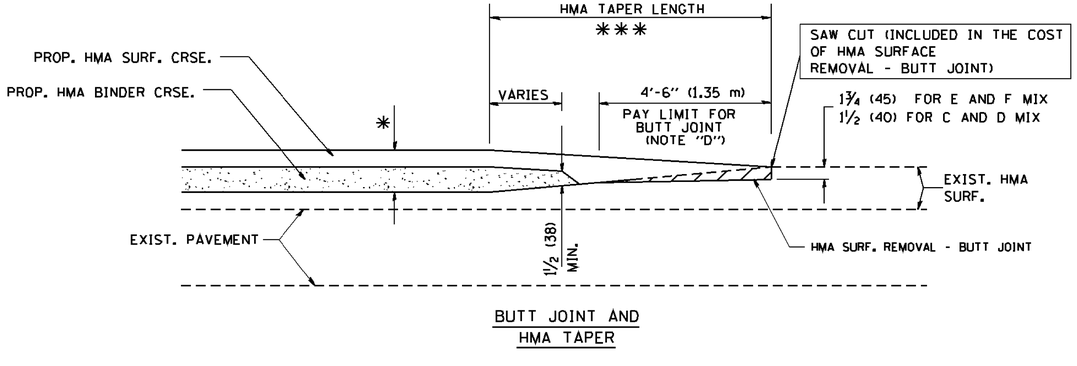


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

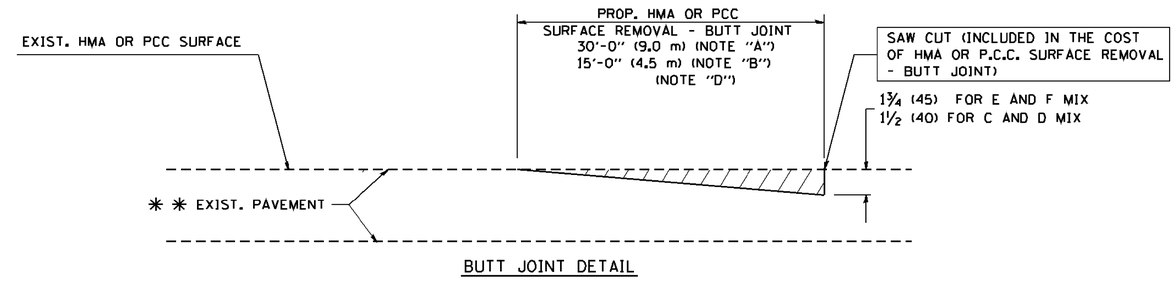
OPTION 2

TYPICAL TEMPORARY RAMP

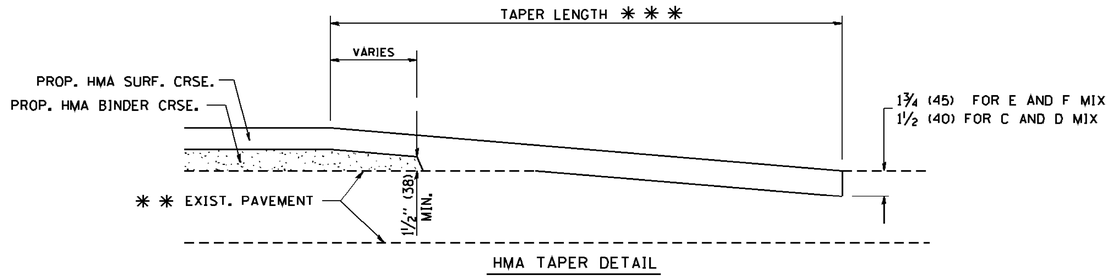


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

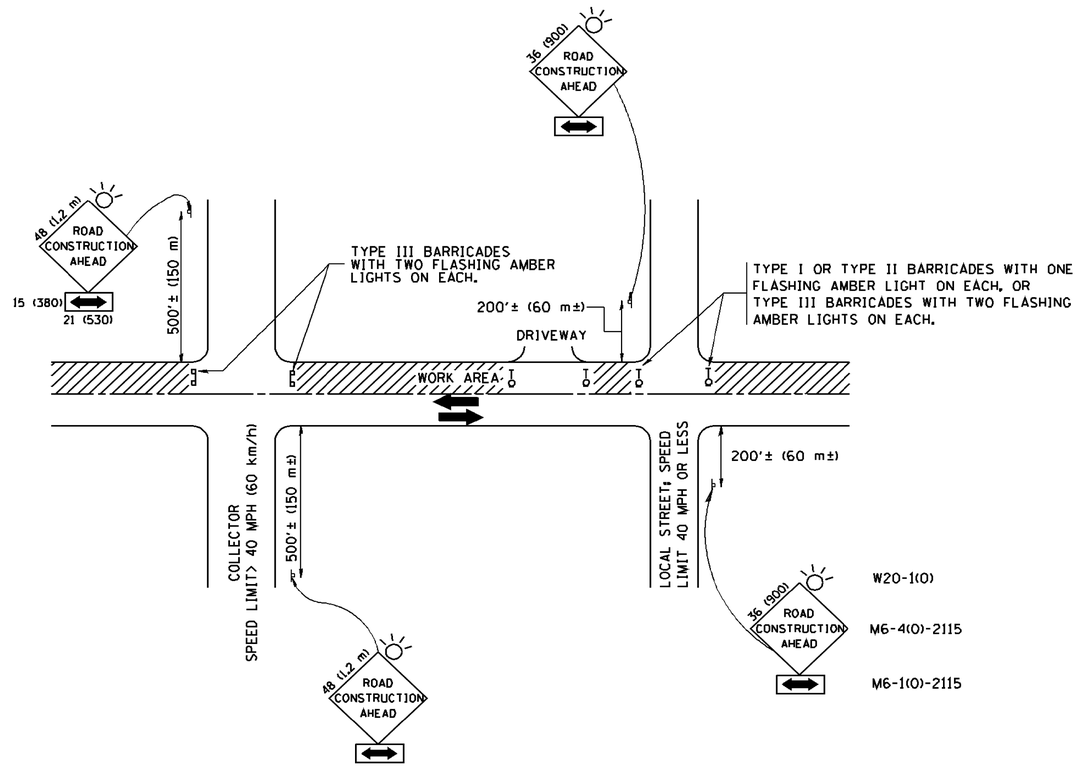
FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	22
	BD400-05 BD32	CONTRACT NO. 61E13		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

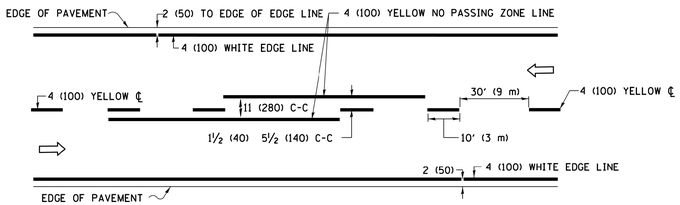
FILE NAME = W:\diststd\22x34\tc10.dgn	USER NAME = gaglianobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

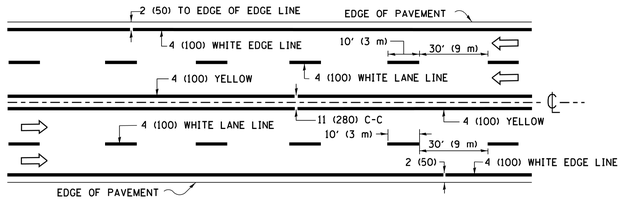
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

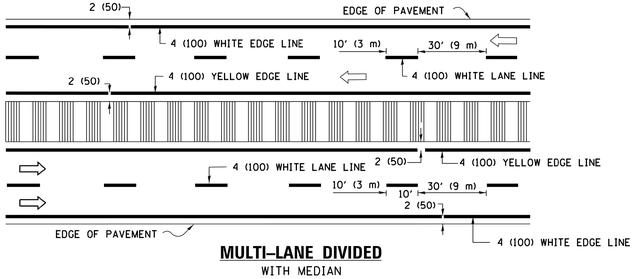
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	23
	TC-10	CONTRACT NO. 61E13		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



2–LANE ROADWAY

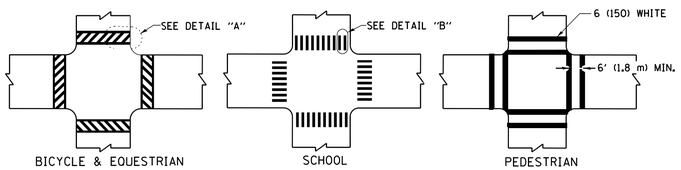


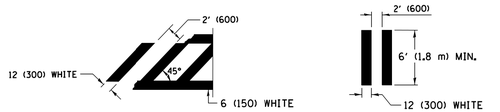
MULTI–LANE UNDIVIDED



MULTI–LANE DIVIDED
WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

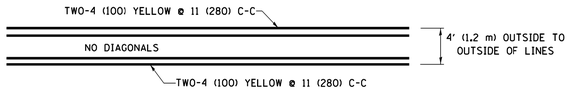




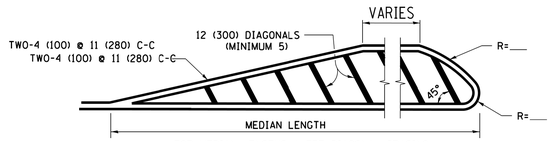
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

✳ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



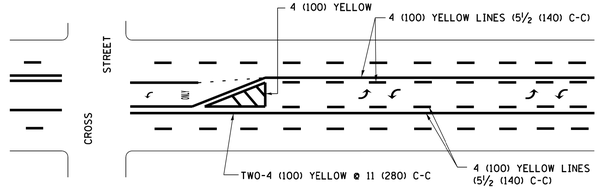
4' (1.2 m) WIDE MEDIANS ONLY



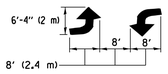
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

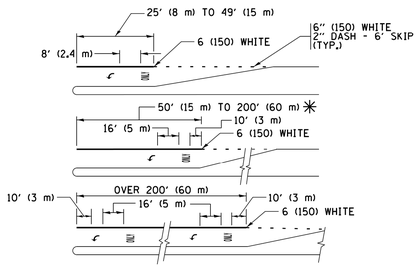


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

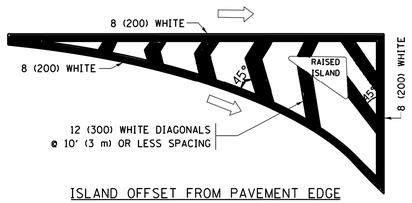


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1.9 m^2)

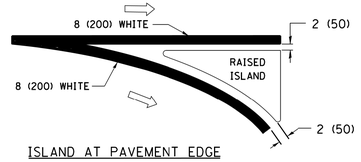
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

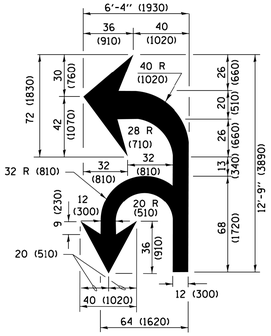


ISLAND OFFSET FROM PAVEMENT EDGE

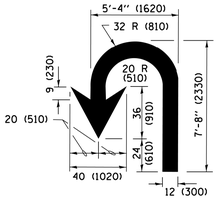


ISLAND AT PAVEMENT EDGE

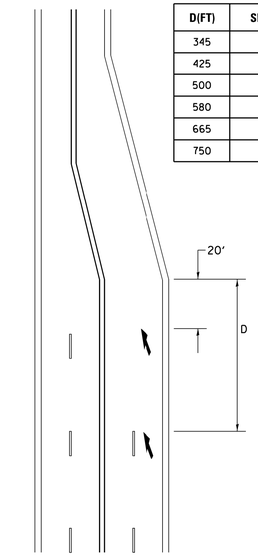
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U–TURN



U–TURN



LANE REDUCTION TRANSITION

✳ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 ⊠ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 ⊠ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 ⊠ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 ⊠ 6 (150) 12 (300) ⊠ 45° 12 (300) ⊠ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 ⊠ 4 (100) WITH 12 (300) DIAGONALS ⊠ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS ⊠ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) ⊠ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT DATE = 4/13/2016	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
			REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	24
	TC-13	CONTRACT NO. 61E13		
		ILLINOIS FED. AID PROJECT		

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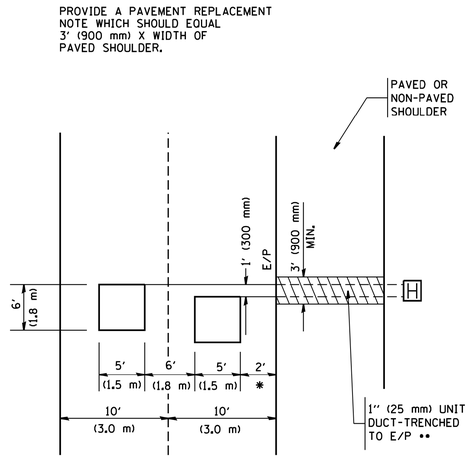
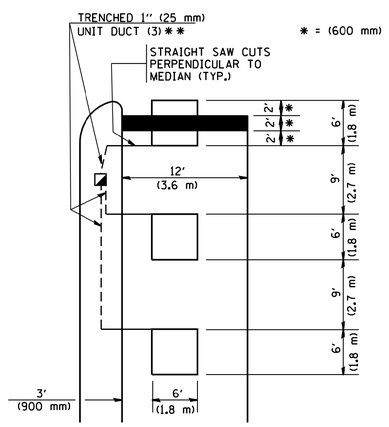
LOOPS NEXT TO SHOULDERS

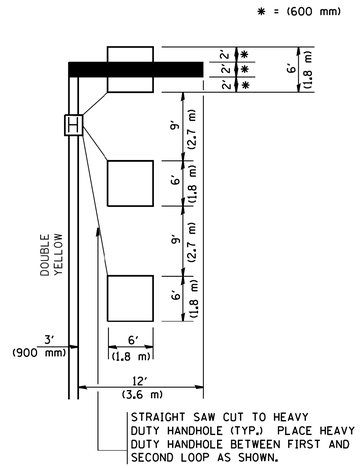


LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



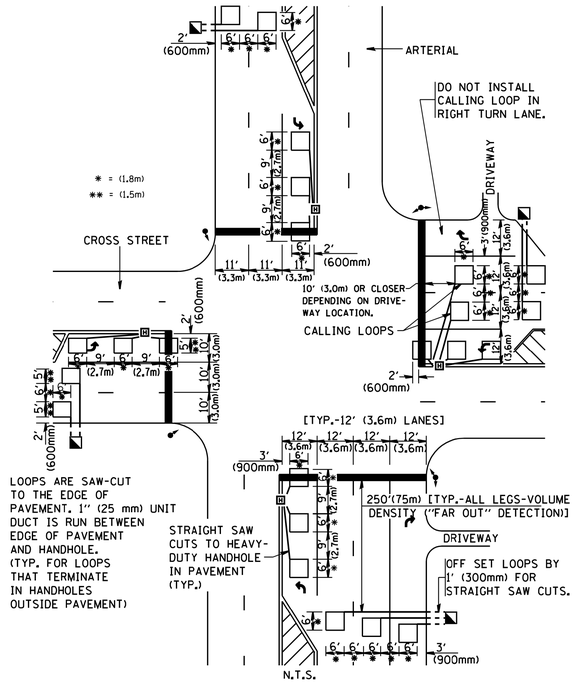
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



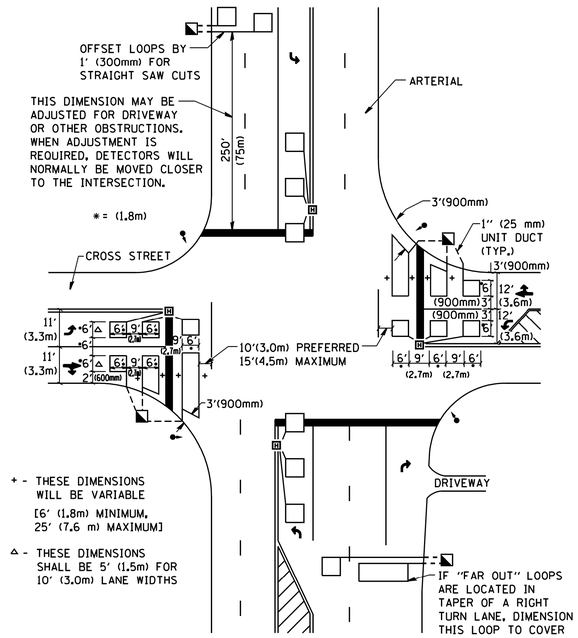
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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	PLOT DATE = 1/4/2008	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1202 AND 1207	17-00088-00-RS	LAKE	25	25
	TS-07	CONTRACT NO. 61E13		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		