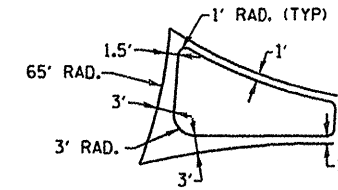


N/F HOWARD BEHRENS
04-29-200-17

PROP. CURVE PN7H2
PI STA. = 50+71.36
 $\Delta = 14^\circ 13' 17''$ (LT)
D = 14° 19' 26"
R = 400.00'
T = 49.90'
L = 99.28'
E = 3.10'
e = N/C
P.C. STA. = 50+21.46
P.T. STA. = 51+20.74

PROP. CURVE PVIN2
PI STA. = 117+54.59
 $\Delta = 32^\circ 25' 32''$ (LT)
D = 19° 05' 55"
R = 300.00'
T = 87.23'
L = 169.78'
E = 12.42'
e = N/C
P.C. STA. = 116+67.36
P.T. STA. = 118+37.14



SPLITTER ISLAND DETAIL
NOTE: SPLITTER ISLANDS SHALL BE SOLID MEDIAN SURFACE.

N/F HOWARD BEHRENS
04-28-100-003

2 CENTERED CURVE
175'-60'
12.5' OFFSET

2 CENTERED CURVE
175'-52'
17.4' OFFSET

EXIST. CURVE VENI
PI STA. = 114+25.72
 $\Delta = 31^\circ 28' 25''$ (RT)
D = 8° 30' 00"
R = 674.07'
T = 189.94'
L = 370.28'
E = 26.25'
e = 47'
P.C. STA. = 112+35.78
P.T. STA. = 116+06.06

PROP. CURVE PVIN3
PI STA. = 118+89.21
 $\Delta = 16^\circ 55' 29''$ (RT)
D = 16° 22' 13"
R = 350.00'
T = 52.07'
L = 103.39'
E = 3.85'
e = N/C
P.C. STA. = 118+37.14
P.T. STA. = 119+40.53

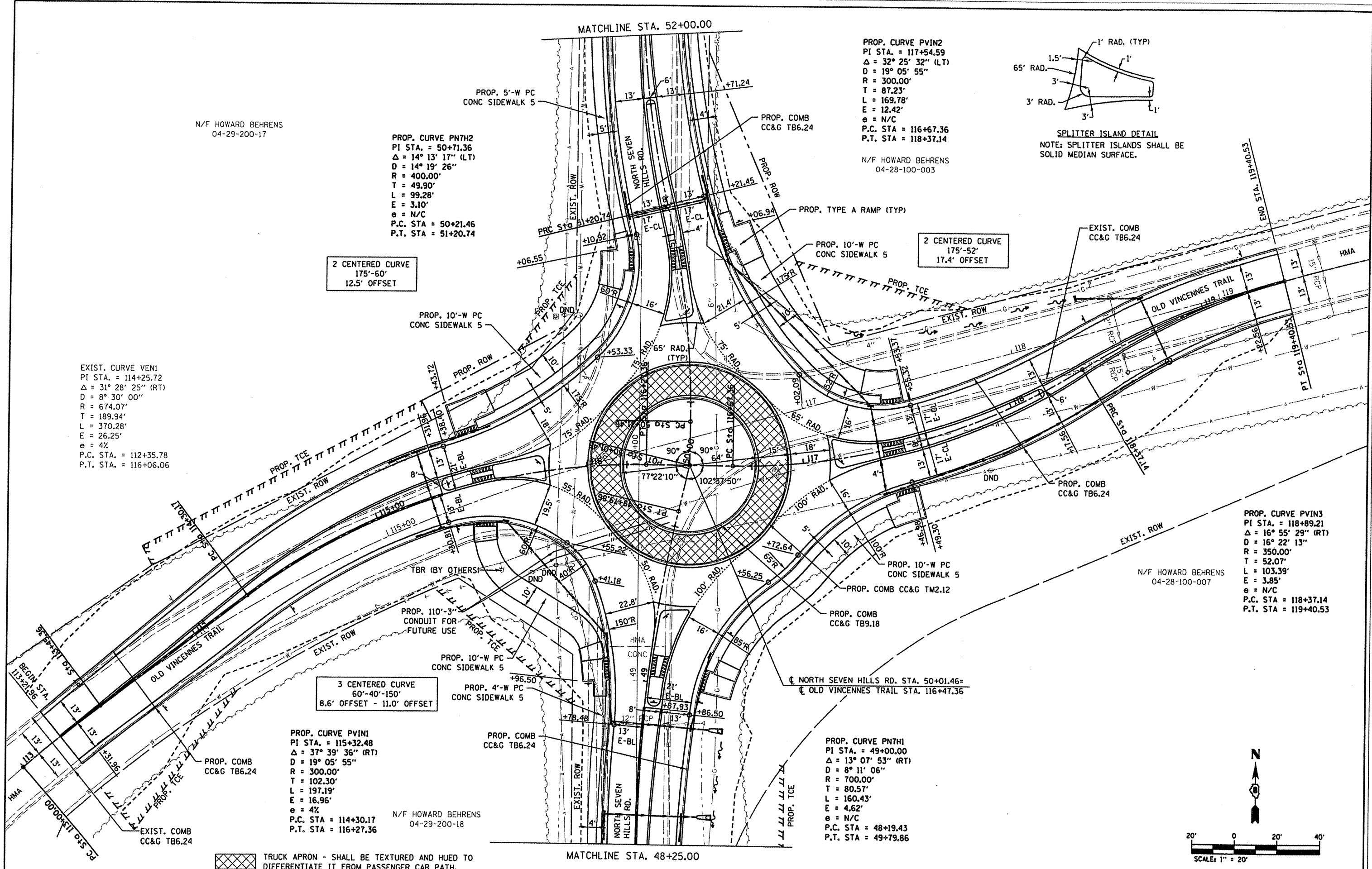
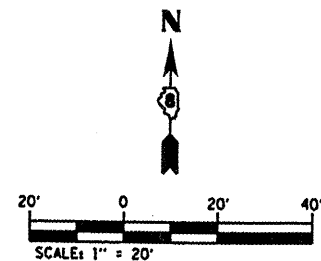
N/F HOWARD BEHRENS
04-28-100-007

3 CENTERED CURVE
60'-40'-150'
8.6' OFFSET - 11.0' OFFSET

PROP. CURVE PVINI
PI STA. = 115+32.48
 $\Delta = 37^\circ 39' 36''$ (RT)
D = 19° 05' 55"
R = 300.00'
T = 102.30'
L = 197.19'
E = 16.96'
e = 47'
P.C. STA. = 114+30.17
P.T. STA. = 116+27.36

N/F HOWARD BEHRENS
04-29-200-18

PROP. CURVE PN7H1
PI STA. = 49+00.00
 $\Delta = 13^\circ 07' 53''$ (RT)
D = 8° 11' 06"
R = 700.00'
T = 80.57'
L = 160.43'
E = 4.62'
e = N/C
P.C. STA. = 48+19.43
P.T. STA. = 49+79.86



TRUCK APRON - SHALL BE TEXTURED AND HUED TO DIFFERENTIATE IT FROM PASSENGER CAR PATH.

FILE NAME = 0829051-shr-Plan.dgn

USER NAME = joelitaliano
PLOT TIME = 9:17:04 AM
PLOT SCALE = 40.0000 / IN.
PLOT DATE = 10/5/2011

DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

CITY OF O'FALLON

ROADWAY PLAN SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9172	10-00036-03-PV	ST CLAIR	51	15

CONTRACT NO. 97478

SCALE: 1"=20' SHEET NO. 01 OF 02 SHEETS