

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

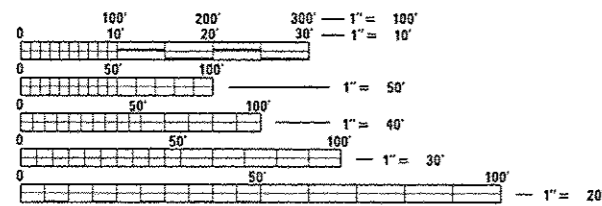
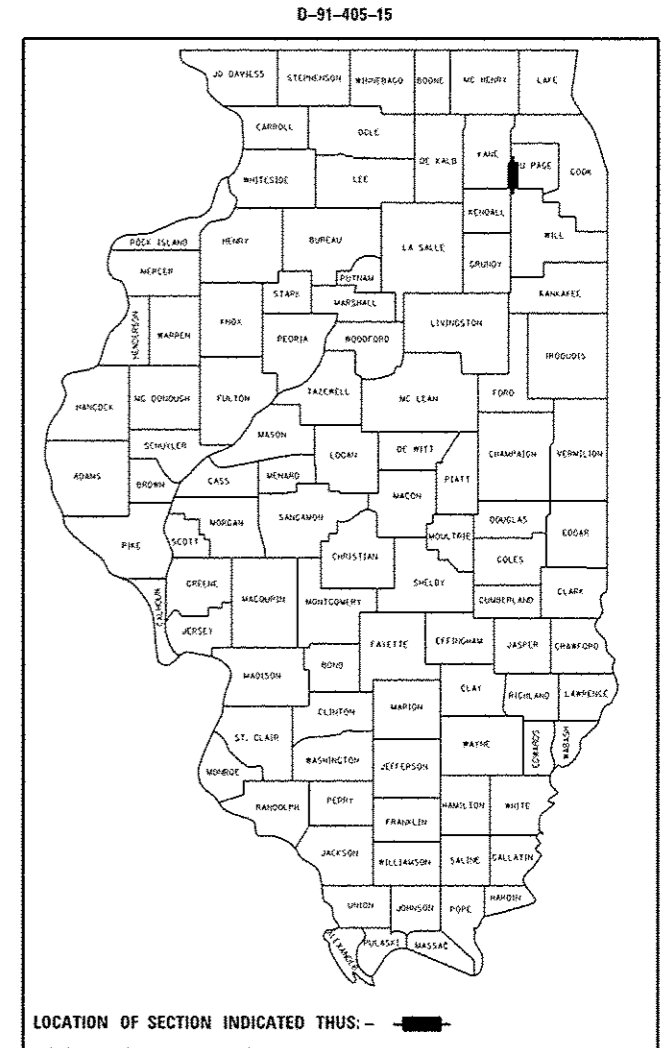
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	112 & 113 RS-10	DUPAGE	24	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 62B25		

FOR INDEX OF SHEETS, SEE SHEET NO. 2

# PROPOSED HIGHWAY PLANS

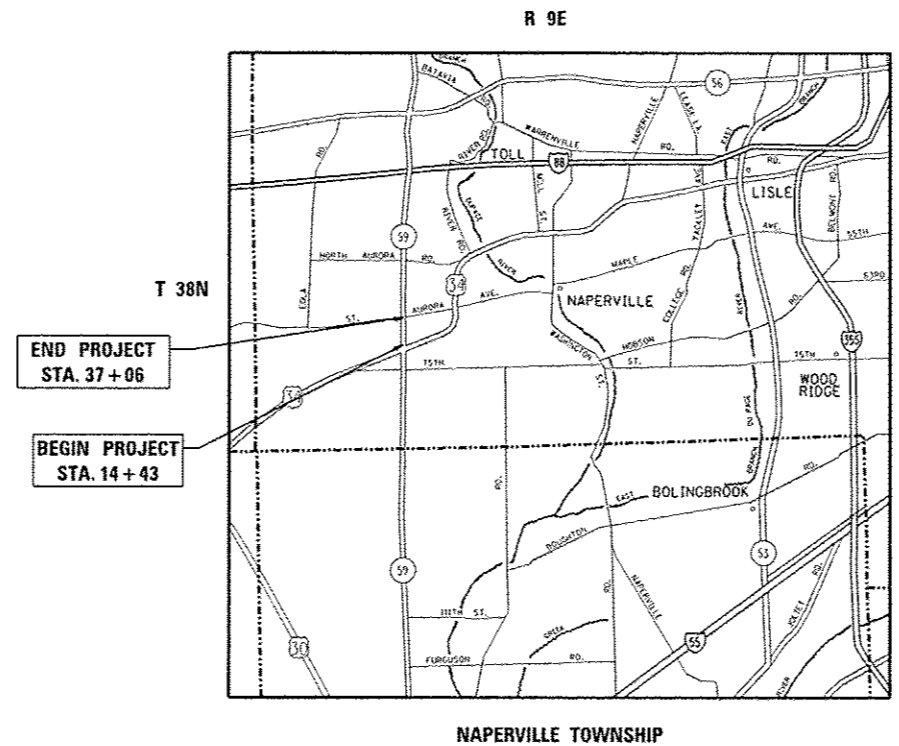
F.A.P. 338 (ILL 59)  
SECTION (112&113)RS-10  
FROM AURORA AVE. TO MCCOY DR.  
RESURFACING  
PROJECT: ACNHPP-0338(057)  
DUPAGE COUNTY  
C-91-405-15

PROJECT IS LOCATED  
WITHIN THE CITIES OF AURORA  
AND NAPERVILLE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811



TRAFFIC DATA  
2013 ADT - 48,200  
SPEED LIMIT - 40 MPH

PROJECT ENGINEER: JENPAI CHANG (847) 705-4432  
PROJECT MANAGER: FAWAD AQUEEL (847) 702-4247

CONTRACT NO. 62B25

GROSS AND NET LENGTH OF PROJECT = 2,263 FT = 0.43 MI.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED *October 25, 2016*

*John Fordman*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

*Maureen M. Addis*  
ENGINEER OF DESIGN AND ENVIRONMENT

*Chris Duncan*  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

LIST OF STATE STANDARDS ✓

GENERAL NOTES

<u>SHEET NO.</u>	<u>DESCRIPTION</u>	<u>STANDARD NO.</u>	<u>DESCRIPTION</u>
1	COVER SHEET	000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
2	INDEX OF SHEETS, STANDARDS, AND GENERAL NOTES	420001-08	PAVEMENT JOINTS
3-4	SUMMARY OF QUANTITIES	420701-03	PAVEMENT FABRIC
5-6	EXISTING & PROPOSED TYPICAL SECTIONS	442201-03	CLASS C AND D PATCHES
7-8	ROADWAY & PAVEMENT MARKINGS PLANS	604001-04	FRAME AND LIDS, TYPE 1
9-11	DETECTOR LOOP PLANS	606301-04	COMBINATION CONCRETE AND GUTTER
12	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8)	606001-06	PCC ISLANDS AND MEDIANS
13	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)	701201-04	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS > 45 MPH
14	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)	701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS < 45 MPH
15	BUTT JOINT AND HMA TAPER DETAILS (BD-32)	701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
16	HMA TAPER AT EDGE OF P.C.C. PAVEMENT (BD-33)	701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
17	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)	701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
18	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)	701801-06	LANE CLOSURE, MULTILANE 1W OR 2W CROSSWALK TO SIDEWALK CLOSURE
19	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)	701901-06	TRAFFIC CONTROL DEVICES
20	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)		
21	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)		
22	ARTERIAL ROAD INFORMATION SIGN (TC-22)		
23	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS (TS-05)		
24	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)		

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITIES OF AURORA AND NAPERVILLE.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (45 KM/H) OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (45 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE RESIDENT ENGINEER SHALL CONTACT MR. DON CHIARUGI, AREA TRAFFIC FIELD ENGINEERS, AT DON.CHIARUGI@ILLINOIS.GOV A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKING.

THE RESIDENT ENGINEER SHALL VERIFY ALL EXISTING PAVEMENT MARKINGS BEFORE MILLING.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		ROADWAY 0005					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	6418	6418					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	8	8					
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	654	654					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	50 YD	150	150					
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	50 YD	3100	3100					
44201765	CLASS D PATCHES, TYPE II, 10 INCH	50 YD	406	406					
44201769	CLASS D PATCHES, TYPE III, 10 INCH	50 YD	42	42					
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	50 YD	107	107					
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	7	7					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3					
67100100	MOBILIZATION	LSUM	1	1					
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1					
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	LSUM	1	1					
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		ROADWAY 0005					
70200100	NIGHTTIME WORK ZONE LIGHTING	LSUM	1	1					
70300100	SHORT TERM PAVEMENT MARKING	FOOT	370	370					
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	50 FT	50	50					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	7935	7935					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	380	380					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	125	125					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	135	135					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	50 FT	50	50					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	7935	7935					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	380	380					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	125	125					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	135	135					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	224	224					

FILE NAME: USER NAME: QUANTITIES  
 DESIGNER: [ ] REVISOR: [ ]  
 DRAWN: [ ] REVISION: [ ]  
 CHECKED: [ ] REVISION: [ ]  
 DATE: [ ] REVISION: [ ]

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

IL 59  
 SUMMARY OF QUANTITIES  
 SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE. 338	SECTION (112&113)RS-10	COUNTY COOK	TOTAL SHEETS 24	SHEET NO. 3
CONTRACT NO. 62825				

13

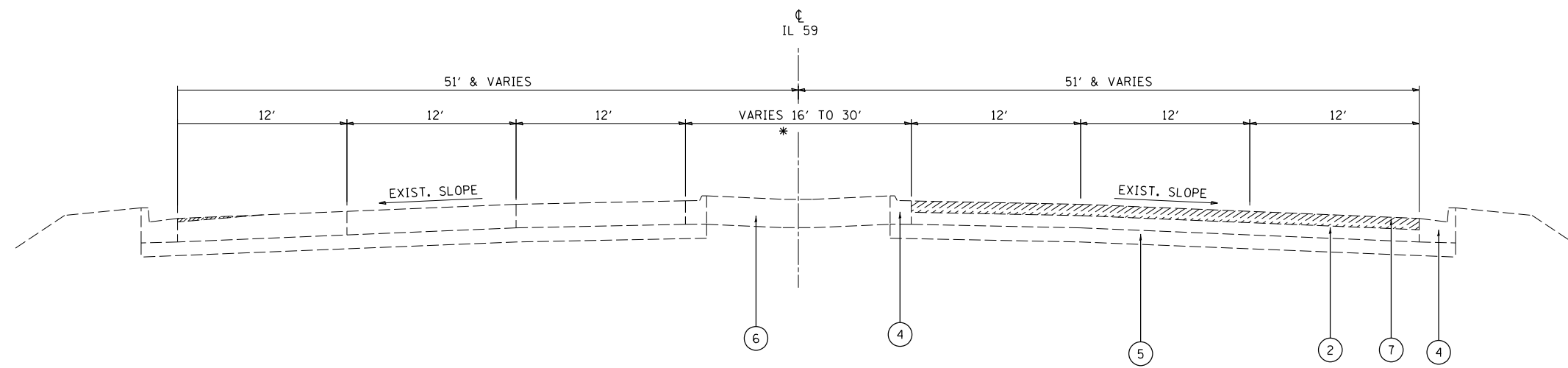
\* SPECIALTY ITEM

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY 0005					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY 0005				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	224	224													
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	2131	2131													
X4060004	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80	TON	2500	2500													
X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SQ YD	6100	6100													
X4405030	LONGITUDINAL PARTIAL DEPTH REMOVAL 3"	FOOT	4273	4273													
X4420900	LONGITUDINAL PARTIAL DEPTH PATCHING	TON	240	240													
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	4	4													
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	450	450													
Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	5	5													
Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	13578	13578													

\* SPECIALTY ITEMS  
NP - NON-PARTICIPATING ITEMS

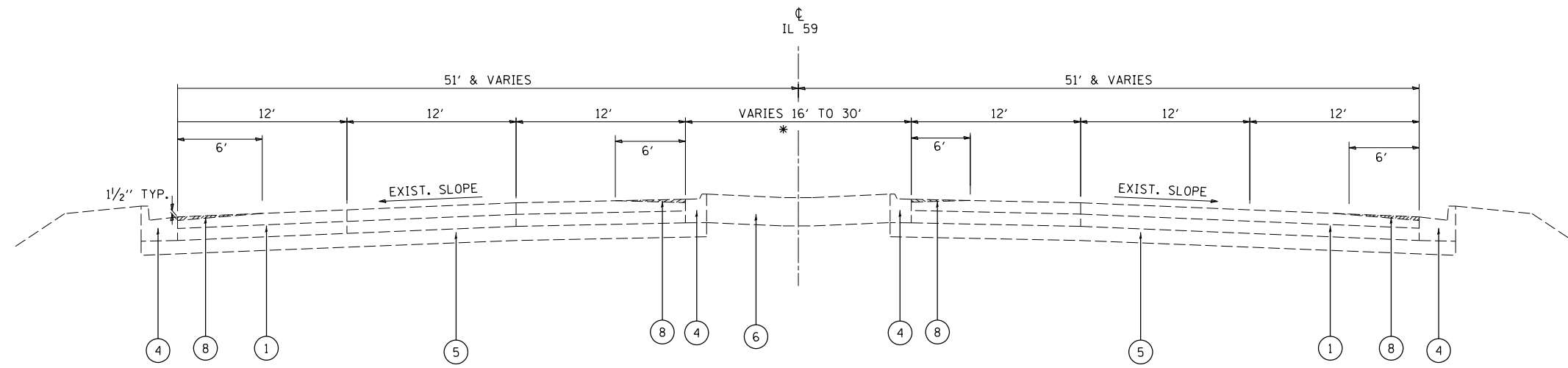
FILE NAME	USER NAME	DESIGNED	REVISED	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				IL 59 SUMMARY OF QUANTITIES				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN	REVISED									338	(112611)RS-10	COOK	24	4
		CHECKED	REVISED									CONTRACT NO. 62825				
		DATE	REVISED									FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

\* SPECIALTY ITEM



**EXISTING TYPICAL CROSS SECTION  
ILL 59**

STA. 14+43 TO STA. 21+98 (NORTHBOUND)



**EXISTING TYPICAL CROSS SECTION  
ILL 59**

STA. 14+43 TO STA. 37+06

\* RIGHT TURN LANE FROM APPROX. STA. 24+20 TO STA. 26+20 & STA. 26+00 TO STA. 29+50  
LEFT TURN LANE FROM APPROX. STA. 15+36 TO STA. 18+75, STA. 24+20 TO STA. 29+45 & STA. 36+00 TO STA. 37+06

**NOTE**  
THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING

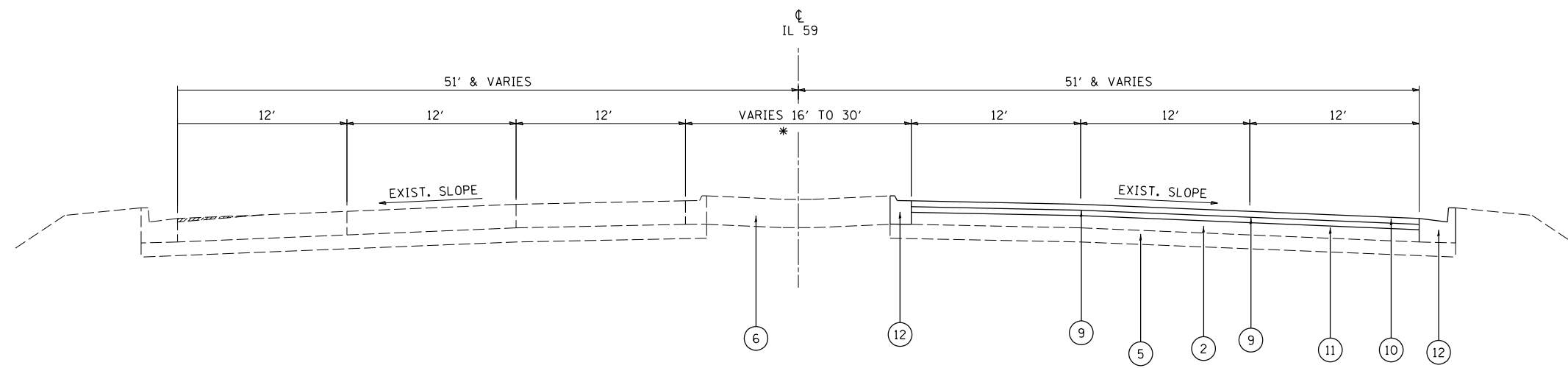
**LEGEND:**

- ① EXISTING PCC PAVEMENT, ±10"
- ② EXISTING HMA PAVEMENT, ±6"
- ③ EXISTING P.C.C. BASE COURSE, 9"
- ④ EXISTING COMB. CONC. CURB AND GUTTER
- ⑤ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B, 4"
- ⑥ EXISTING P.C.C. MEDIAN
- ⑦ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ⑧ PROPOSED PCC REMOVAL (VAR. DEPTH)
- ⑨ PROPOSED LONGITUDINAL JOINT SEALANT (SHALL BE APPLIED ON THE ROADWAY AS DESCRIBED IN THE SPECIAL PROVISION)
- ⑩ PROPOSED POLY. HMA SURFACE COURSE, SMA, N80, 1 3/4"
- ⑪ PROPOSED POLY. HMA BINDER (MM), IL-4.75, N50, 3/4"
- ⑫ PROPOSED COMB. CONC. CURB AND GUTTER (WHERE NECESSARY)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS(%) @ Ndes	
<b>PAVEMENT RESURFACING</b>		
POLY. HMA SURFACE COURSE, SMA, N80 (IL 9.5 mm)	3.5% @ 80 GYR.	QCP
POLY. HMA BINDER COURSE (MM), IL-4.75, N50	3.5% @ 50 GYR.	QCP
<b>PATCHING</b>		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR.	QC/QA
LONG. PARTIAL DEPTH PATCHING (HMA BINDER IL-19.0)	4% @ 70 GYR.	QC/QA
<b>QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)</b>		

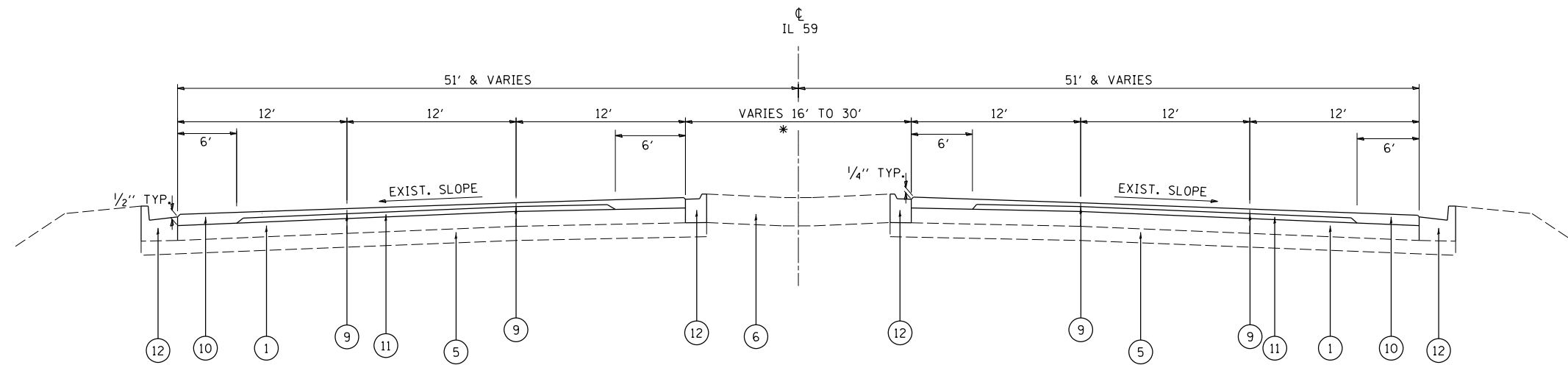
NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SO YD/IN.  
NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.  
FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.  
QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

FILE NAME =	USER NAME = qureshiya	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING TYPICAL SECTIONS</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwwork\pwwork\qureshiya\d0458038\140515-sht-plan.dgn	140515-sht-plan.dgn	DRAWN -	REVISED -					338	(112&113)RS-10	COOK	24	5
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 62B25				
PLOT DATE = 10/25/2016		DATE -	REVISED -					FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



**PROPOSED TYPICAL CROSS SECTION  
ILL 59**

STA. 14+43 TO STA. 21+98 (NORTHBOUND)



**PROPOSED TYPICAL CROSS SECTION  
ILL 59**

STA. 14+43 TO STA. 37+06

\* RIGHT TURN LANE FROM APPROX. STA. 24+20 TO STA. 26+20 &  
STA. 26+00 TO STA. 29+50  
LEFT TURN LANE FROM APPROX. STA. 15+36 TO STA. 18+75, STA.  
24+20 TO STA. 29+45 &  
STA. 36+00 TO STA. 37+06

**NOTE**

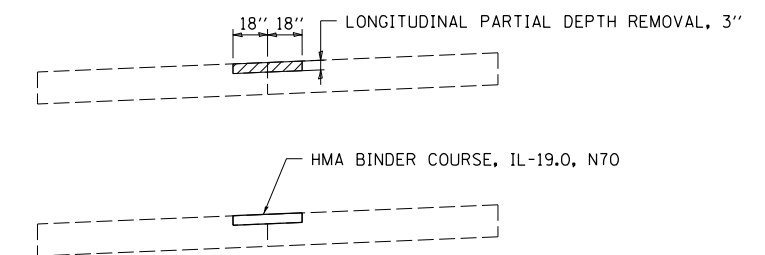
THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING

**LEGEND:**

- ① EXISTING PCC PAVEMENT, ±10"
- ② EXISTING HMA PAVEMENT, ±6"
- ③ EXISTING P.C.C. BASE COURSE, 9"
- ④ EXISTING COMB. CONC. CURB AND GUTTER
- ⑤ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE B, 4"
- ⑥ EXISTING P.C.C. MEDIAN
- ⑦ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ⑧ PROPOSED PCC REMOVAL (VAR. DEPTH)
- ⑨ PROPOSED LONGITUDINAL JOINT SEALANT (SHALL BE APPLIED ON THE ROADWAY AS DESCRIBED IN THE SPECIAL PROVISION)
- ⑩ PROPOSED POLY. HMA SURFACE COURSE, SMA, N80, 1 3/4"
- ⑪ PROPOSED POLY. HMA BINDER (MM), IL-4.75, N50, 3/4"
- ⑫ PROPOSED COMB. CONC. CURB AND GUTTER (WHERE NECESSARY)

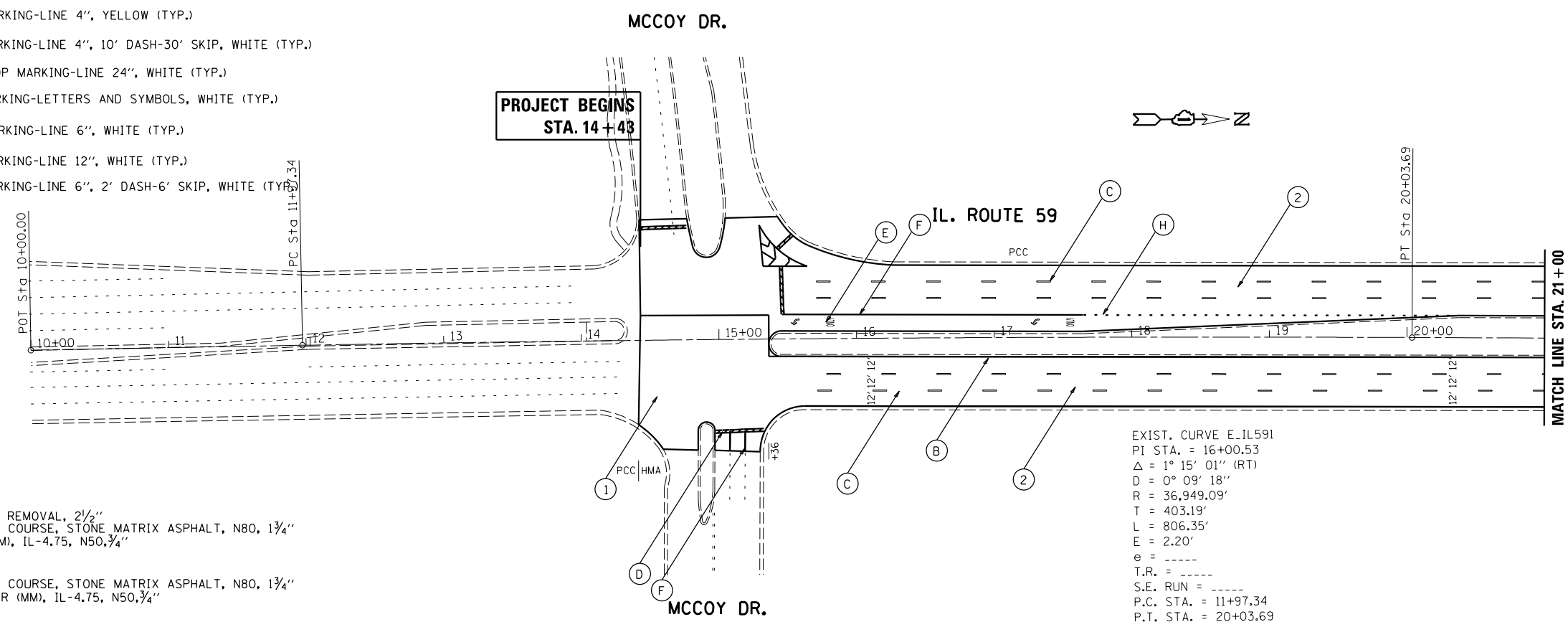
**DETAIL A  
LONGITUDINAL PARTIAL DEPTH PATCH (TYP.)**

(LOCATIONS TO BE DETERMINED BY ENGINEER)



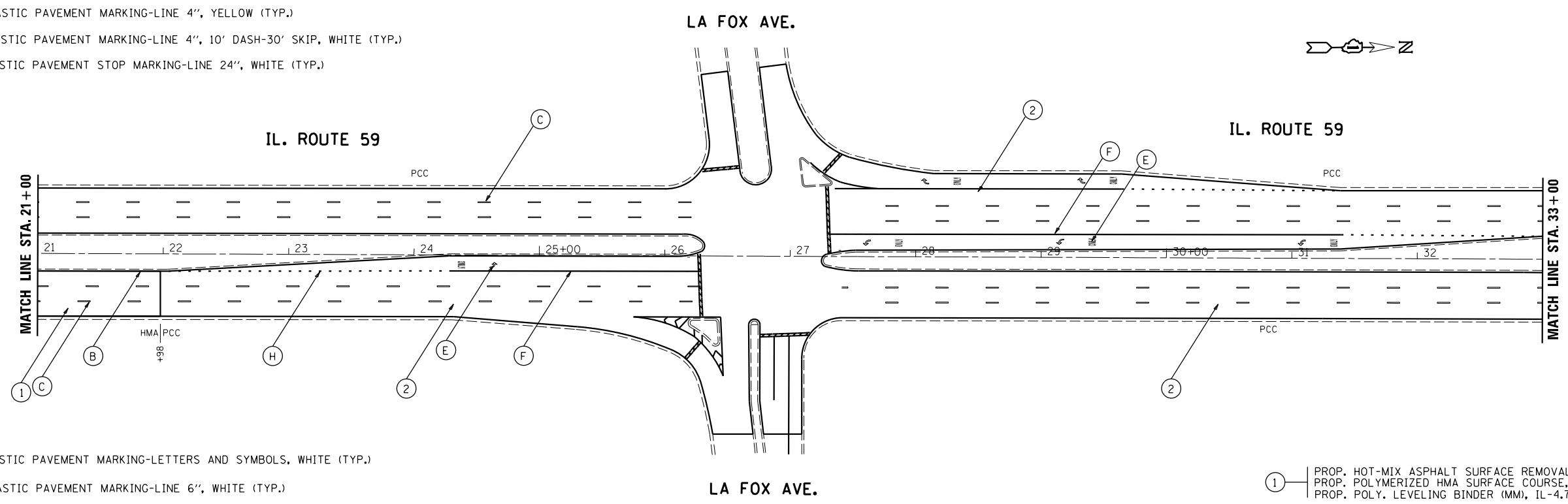
FILE NAME =	USER NAME = qureshiya	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING TYPICAL SECTIONS</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\p\idot\qureshiya\d0458038\0140515-sht-plan.dgn	10/25/2016	DRAWN -	REVISED -					338	(112&113)RS-10	COOK	24	6
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED -		SCALE:      SHEET NO.    OF    SHEETS    STA.                    TO STA.			CONTRACT NO. 62B25				
PLOT DATE = 10/25/2016		DATE -	REVISED -		FED. ROAD DIST. NO.    ILLINOIS FED. AID PROJECT							

- (A) PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 4", WHITE (TYP.)
- (B) PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 4", YELLOW (TYP.)
- (C) PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 4", 10' DASH-30' SKIP, WHITE (TYP.)
- (D) PROP. THERMOPLASTIC PAVEMENT STOP MARKING-LINE 24", WHITE (TYP.)
- (E) PROP. THERMOPLASTIC PAVEMENT MARKING-LETTERS AND SYMBOLS, WHITE (TYP.)
- (F) PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 6", WHITE (TYP.)
- (G) PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 12", WHITE (TYP.)
- (H) PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 6", 2' DASH-6' SKIP, WHITE (TYP.)



- (1) PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"  
PROP. POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"  
PROP. POLY. LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- (2) PROP. POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"  
PROP. POLY. HMA LEVELING BINDER (MM), IL-4.75, N50, 3/4"

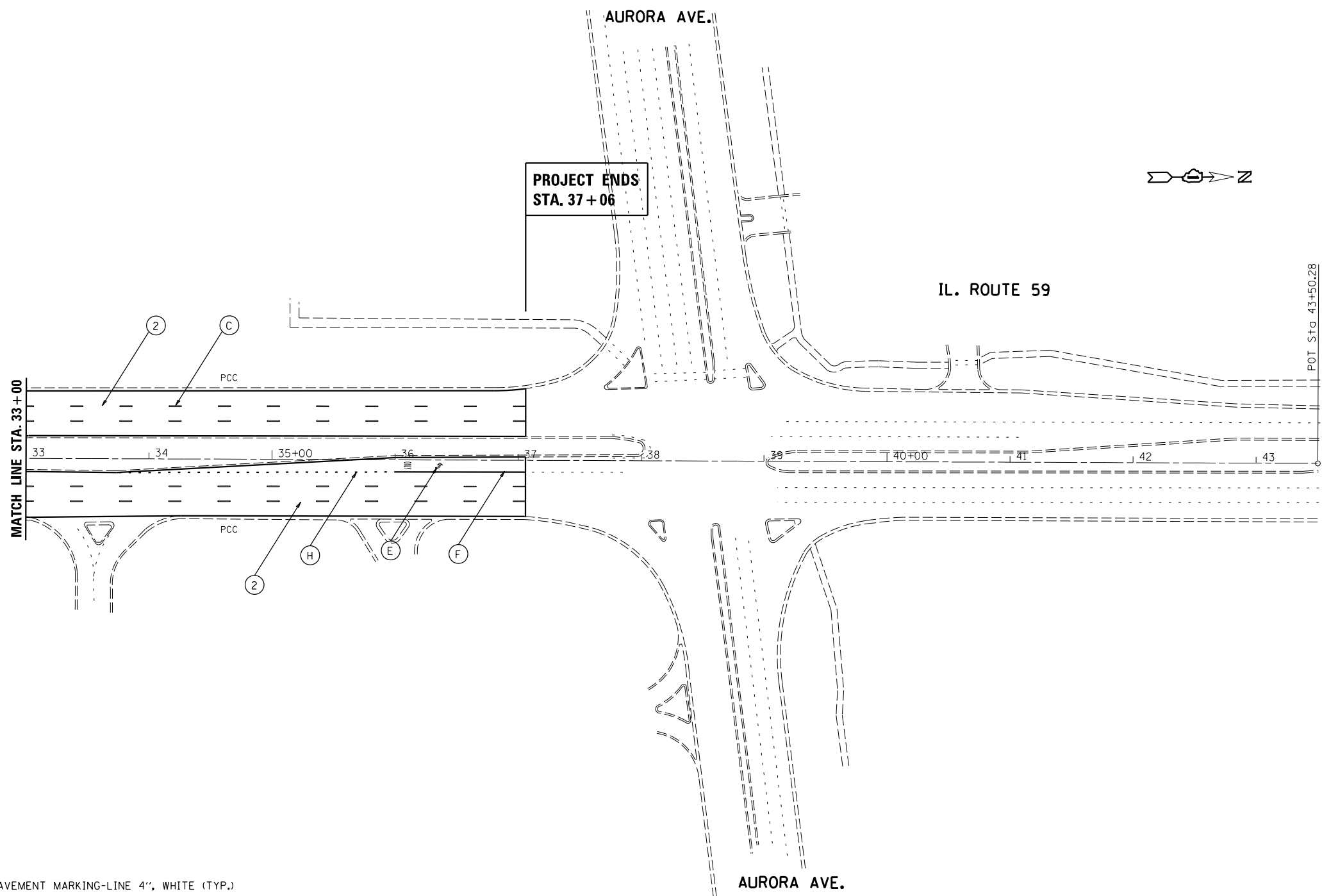
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- (E) PROP. THERMOPLASTIC PAVEMENT MARKING-LETTERS AND SYMBOLS, WHITE (TYP.)
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- (1) PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"  
PROP. POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"  
PROP. POLY. LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- (2) PROP. POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"  
PROP. POLY. HMA LEVELING BINDER (MM), IL-4.75, N50, 3/4"

FILE NAME =	USER NAME = qureshiya	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pw_work\p\id\dot\qureshiya\140515-sht-plan.dgn	140515-sht-plan.dgn	DRAWN -	REVISED -		<b>IL. ROUTE 59 (AURORA AVE. TO MCCOY RD.)</b>			338	(112 & 113) RS-10	DUPAGE	24	7	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE: 1" = 50'	SHEET	OF	SHEETS	STA.	TO	STA.	<b>CONTRACT NO. 62B25</b>	
	PLOT DATE = 10/27/2016	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								



- (A) PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 4", WHITE (TYP.)
- (B) PROP. THERMOPLASTIC PAVEMENT MARKING-LINE 4", YELLOW (TYP.)
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- (1) PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"  
PROP. POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"  
PROP. POLY. LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- (2) PROP. POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"  
PROP. POLY. HMA LEVELING BINDER (MM), IL-4.75, N50, 3/4"

FILE NAME =	USER NAME = qureshiya	DESIGNED -	REVISED -
Default	140515-sht-plan.dgn	DRAWN -	REVISED -
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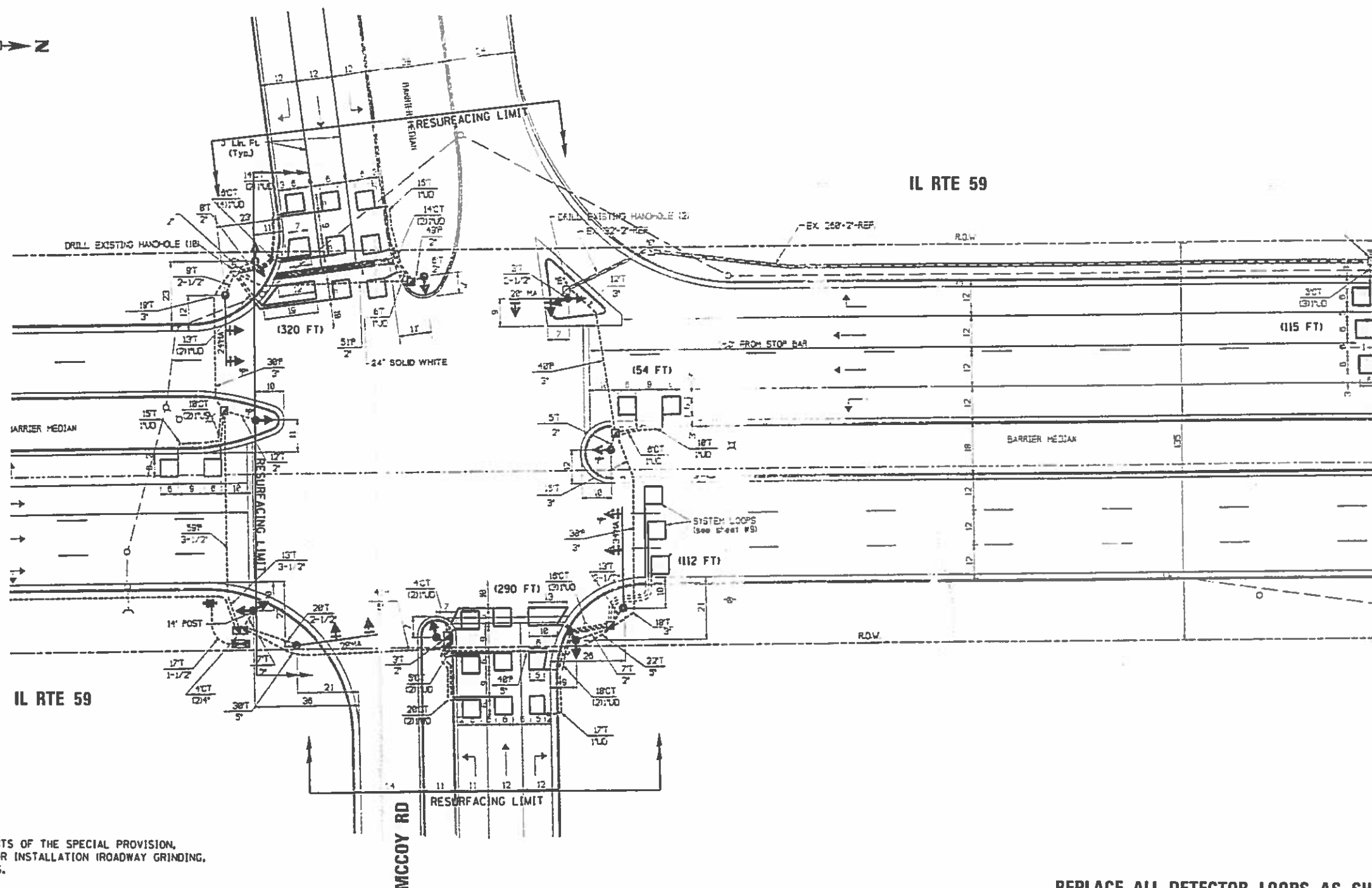
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
IL. ROUTE 59 (AURORA AVE. TO MCCOY RD.)**

SCALE: 1" = 50'      SHEET      OF      SHEETS      STA.      TO      STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(112 & 113) RS-10	DUPAGE	24	8
CONTRACT NO. 62B25			ILLINOIS FED. AID PROJECT	





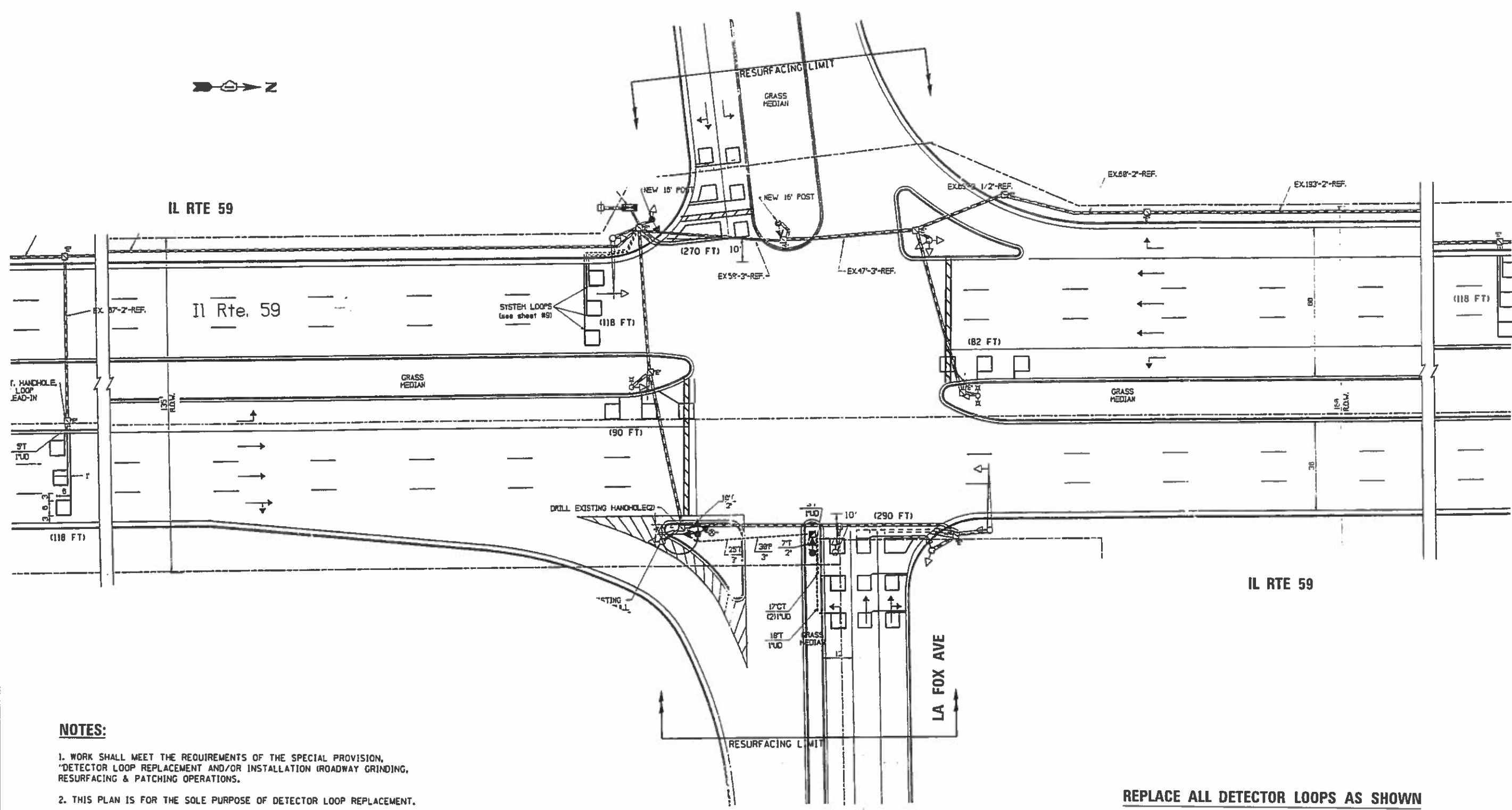
**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	891	FOOT

FILE NAME : S:\WP\Design\toen\Resurfacing\62825.dwg	USER NAME : plascencia	DESIGNED - IP	REVISED - IP 9/30/2016	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETECTOR LOOP REPLACEMENT PLAN IL RTE 59 AT MCCOY RD</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	PLT SCALE : 48.0000' / 1" =	CHECKED - LP	REVISED -		338	1112 & 1131 R5-10	DUPAGE	24	9				
	PLT DATE : 10/15/2016	DATE - 8/15/2016	REVISED -		CONTRACT NO. 62825				ILLINOIS FED. AID PROJECT				
					SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.		



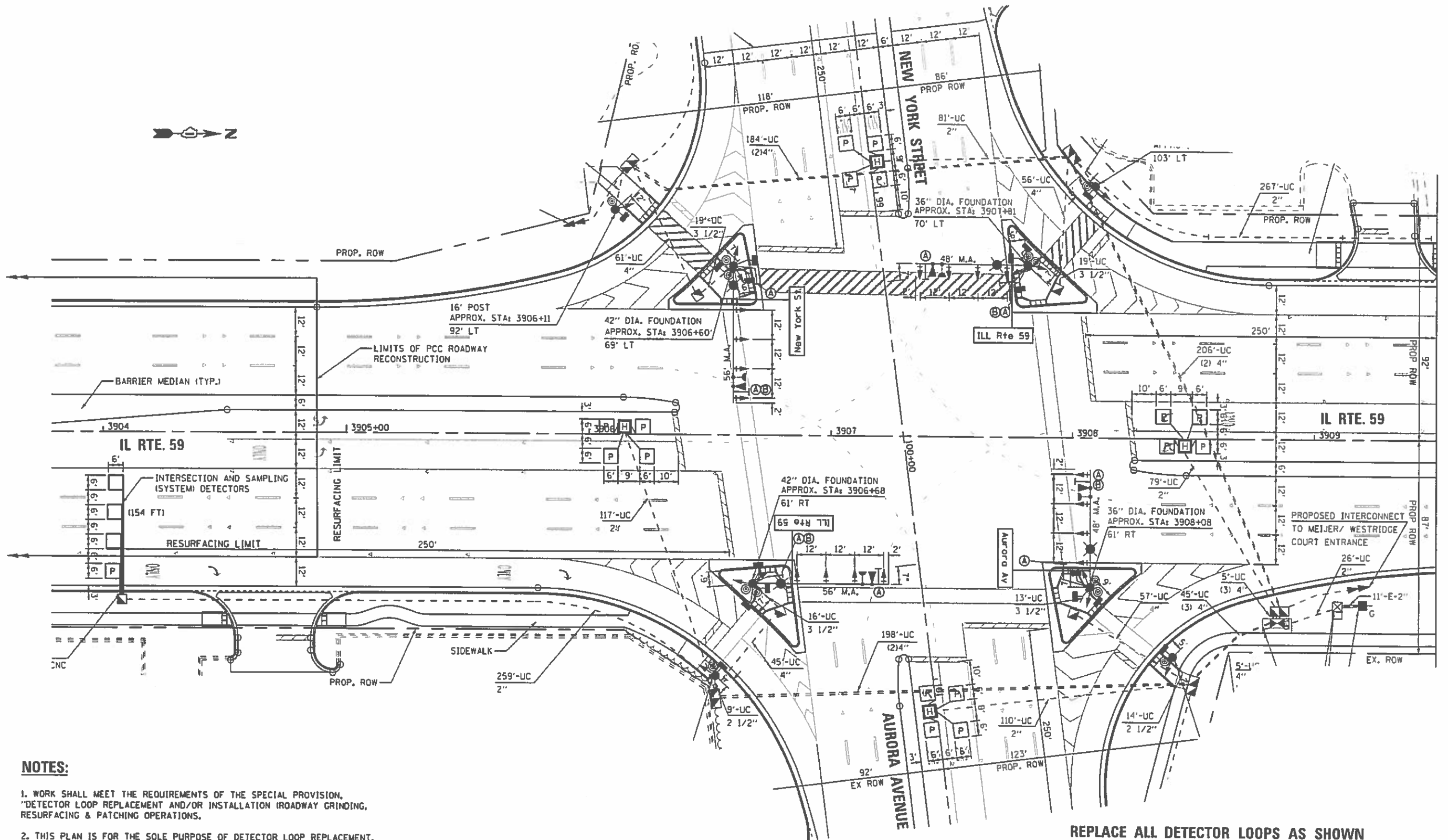
**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	1086	FOOT

FILE NAME *	USER NAME * plascencia	DESIGNED - IP	REVISED - IP 9/30/2016	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETECTOR LOOP REPLACEMENT PLAN IL RTE 59 AT LA FOX AVE</b>		F.A.P. RTE. 338	SECTION (112 & 113) RS-10	COUNTY OUPAGE	TOTAL SHEETS 24	SHEET NO. 10	
S:\WP\Design\Ivan\Resurfacing\62825_IL	659@Aurora-McCoy\CountyLineResurf	DR/AN/IN - IP	REVISOR -		SCALE:	SHEET OF SHEETS	STA. TO STA.	<b>CONTRACT NO. 62825</b>		ILLINOIS FED. AID PROJECT		
Default	PLOT SCALE = 40.0000' / in.	CHECKED - LP	REVISOR -									
	PLOT DATE = 10/15/2016	DATE - 8/15/2016	REVISOR -									



**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	154	FOOT

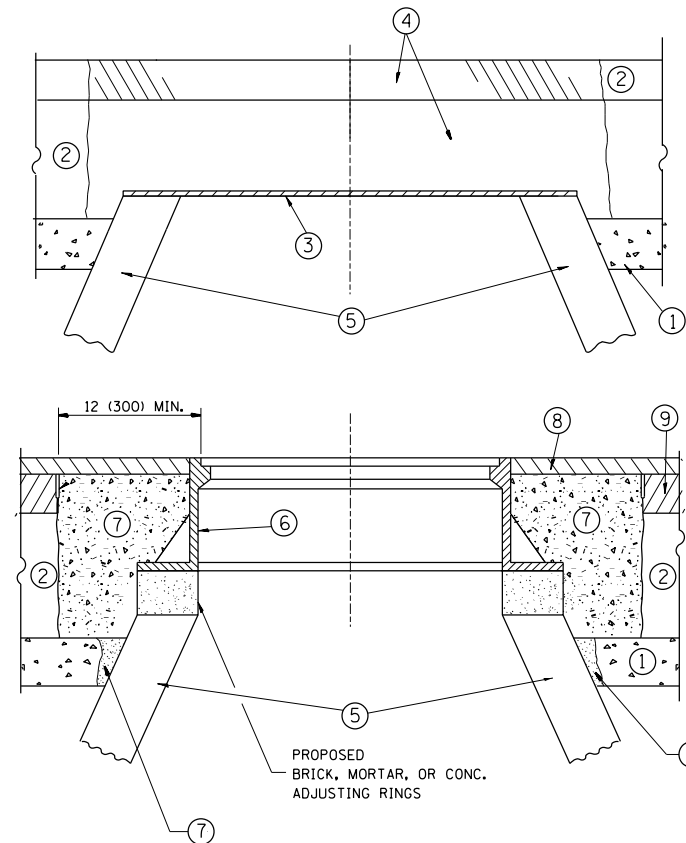
FILE NAME *	USER NAME * pascenciac	DESIGNED - IP	REVISED - IP 9/30/2016
S:\WP\Design\New\Resurfacing\62B25.LU	6398AuroraAve+McCoyDr+CountyLineRdCarried	DR/PLN/ENR	REVISED -
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	PLOT DATE = 8/15/2016	DATE - 8/15/2016	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN  
IL RTE 59 AT NEW YORK ST /AURORA AVE**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	1112 & 1131 RS-10	DUPAGE	24	11
CONTRACT NO. 62B25			ILLINOIS FED. AID PROJECT	

SCALE: SHEET OF SHEETS STA. TO STA.



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

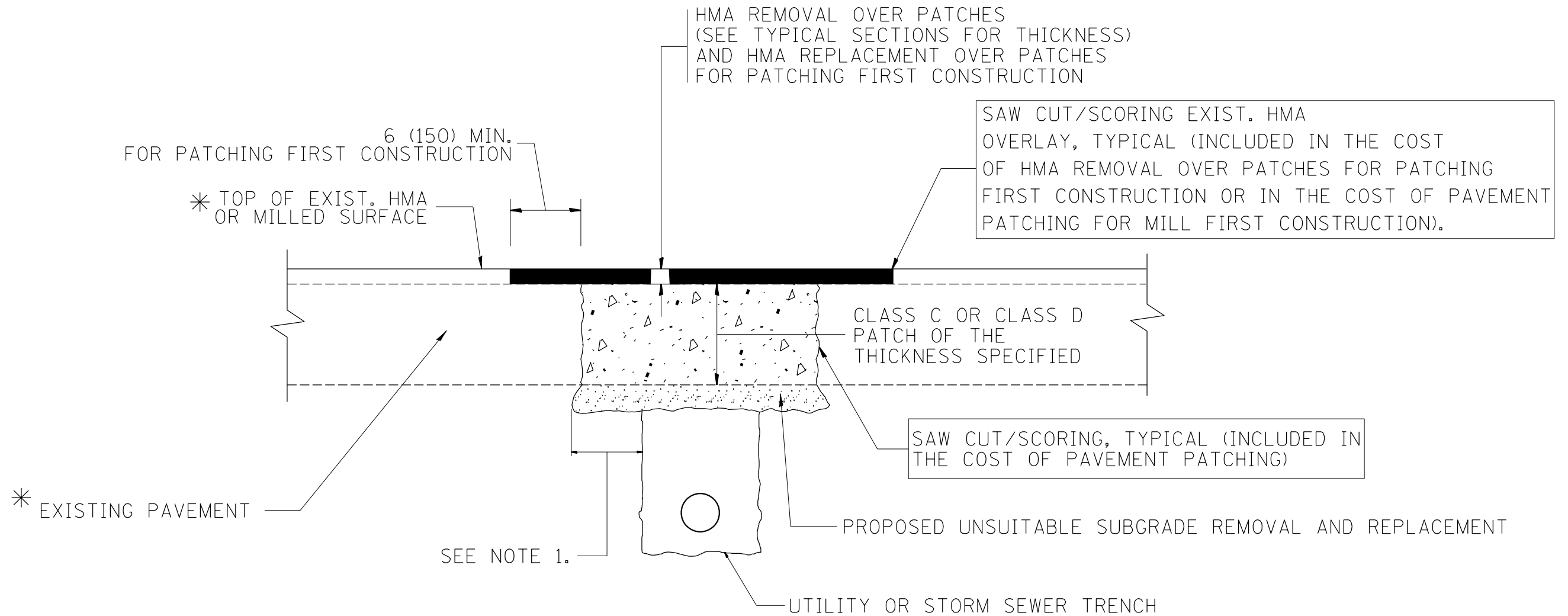
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = qureshiya	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
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	PLOT DATE = 10/25/2016	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(112&113)RS-10	COOK	24	12
<b>BD600-03 (BD-8)</b>		<b>CONTRACT NO. 62B25</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = qureshiya	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwork\pwork\qureshiya\0458038\Diststd.dgn		DRAWN -	REVISED - R. BORO 01-01-07					338	(112&113)R5-10	COOK	24	13
	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED - R. BORO 09-04-07		<b>BD400-04 (BD-22)</b>			<b>CONTRACT NO. 62B25</b>				
	PLOT DATE = 10/25/2016	DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001  
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

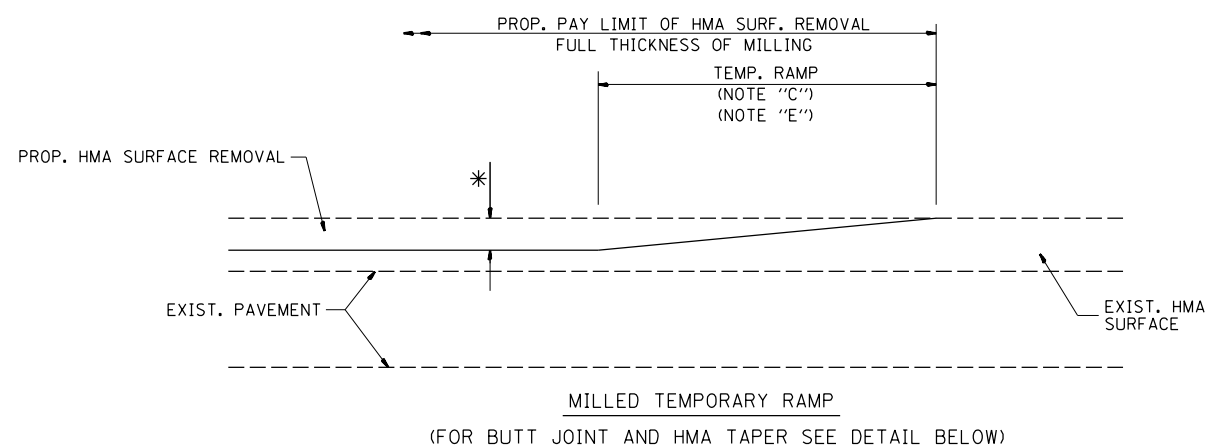
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

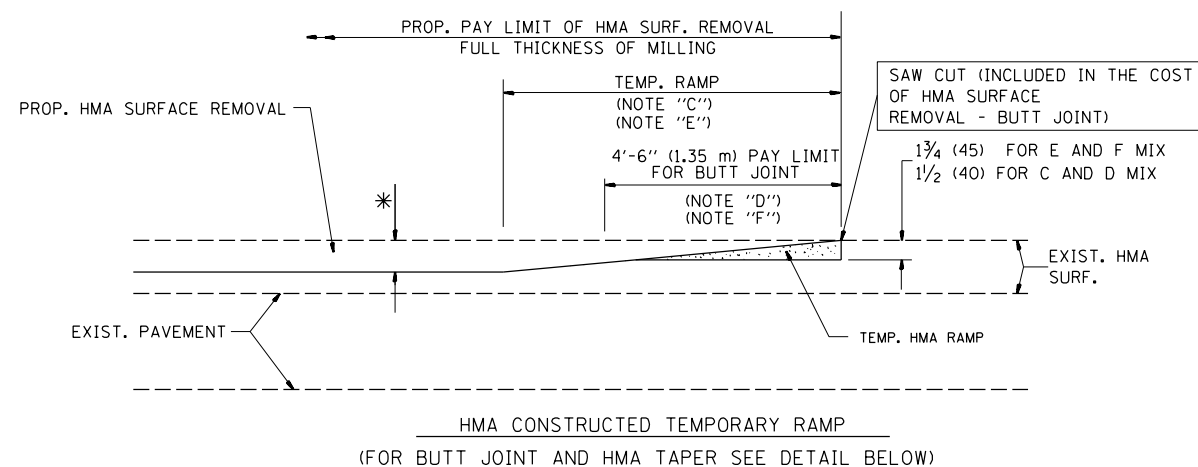
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = qureshiya	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\p\id\dot\qureshiya\d0458038\Diststd.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97			338	(112&113)RS-10	COOK	24	14
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED - M. GOMEZ 01-22-01			<b>BD600-06 (BD-24)</b>		<b>CONTRACT NO. 62B25</b>		
PLOT DATE = 10/25/2016		DATE - 03-11-94	REVISED - R. BORO 12-15-09			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

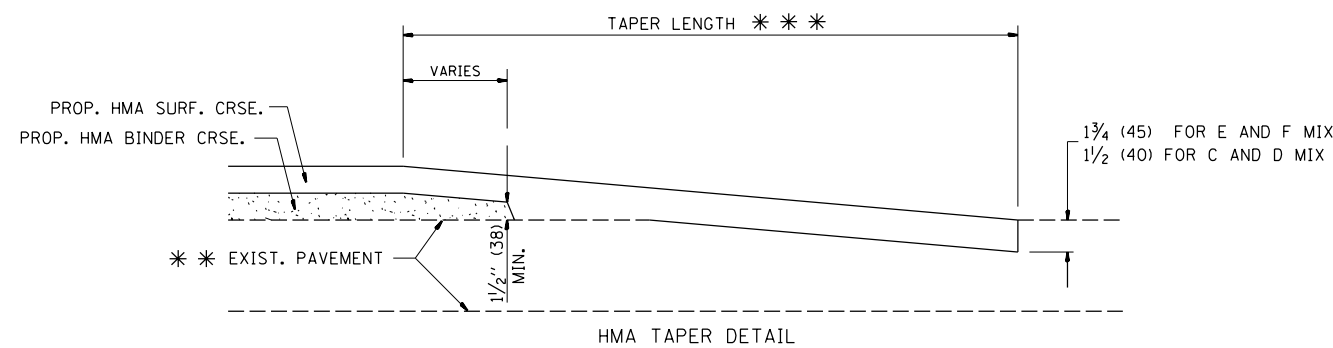
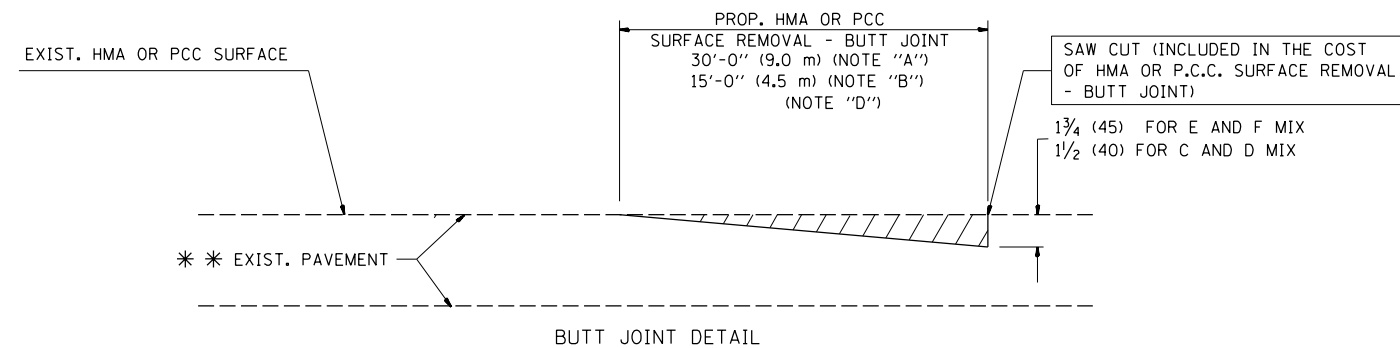


**OPTION 1**



**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

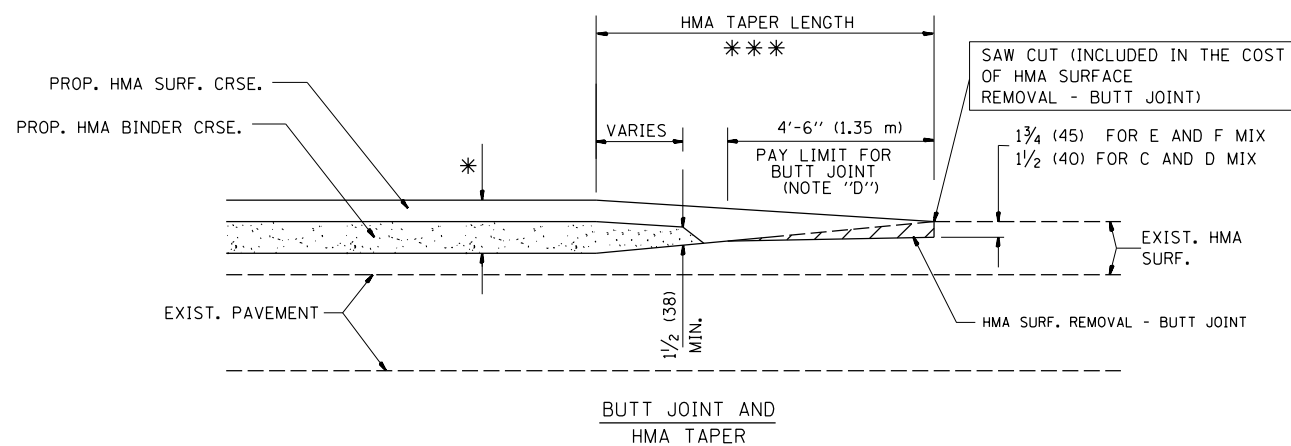
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

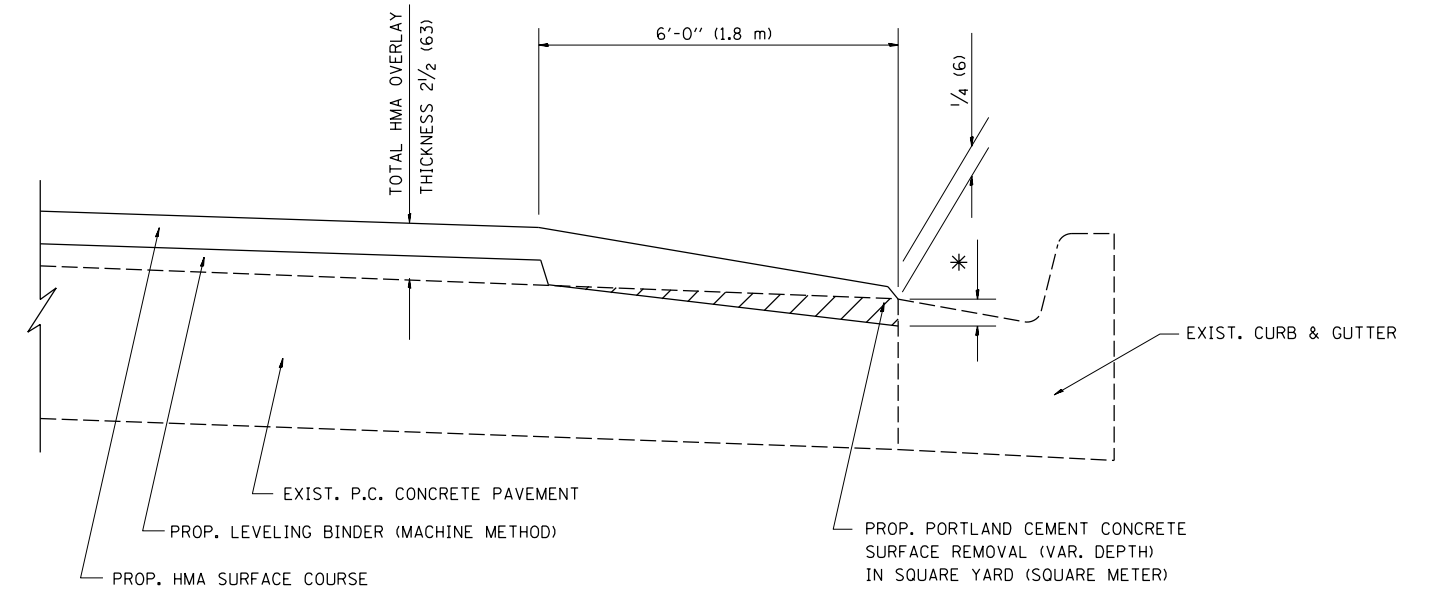
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	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 10/25/2016	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(112&113)RS-10	COOK	24	15
BD400-05 BD32		CONTRACT NO. 62B25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



HMA TAPER AT  
EDGE OF P.C.C. PAVEMENT

HMA SURFACE	THICKNESS	LEVELING BINDER THICKNESS	* MILLING AT GUTTER FLAG
C OR D	1 1/2 (38)	1 (25)	1/4 (33)
E	1 3/4 (44)	3/4 (19)	1/2 (38)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

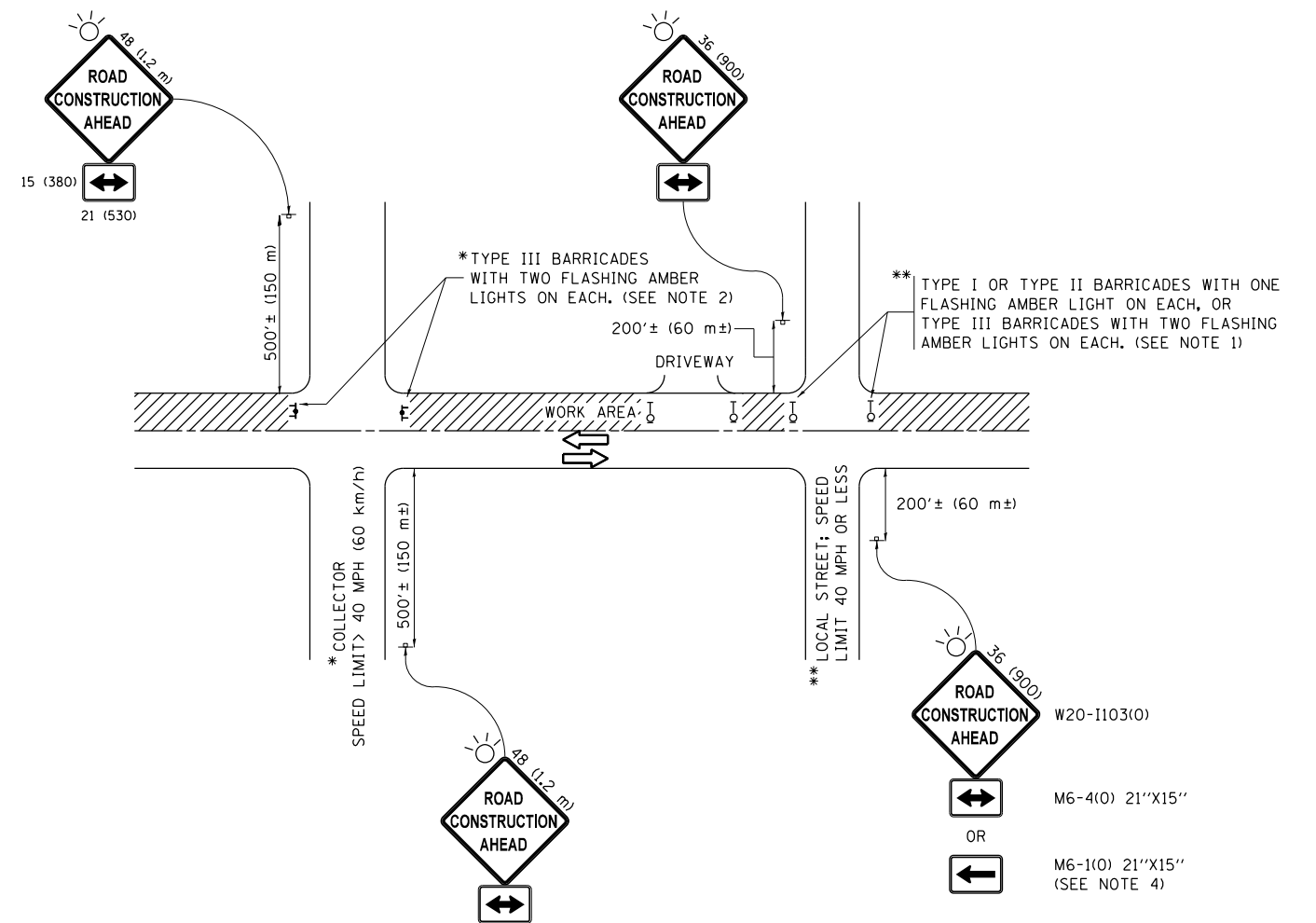
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Default	PLOT SCALE = 100.0000' / 1in.	CHECKED - A. ABBAS	REVISED - R. BORO 01-01-07
	PLOT DATE = 10/25/2016	DATE - 09-10-94	REVISED - JP CHANG 07-08-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>HMA TAPER AT EDGE OF P.C.C. PAVEMENT</b>			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(112&113)RS-10	COOK	24	16
BD400-06 (BD33)		CONTRACT NO. 62B25		
ILLINOIS FED. AID PROJECT				





**NOTES:**

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

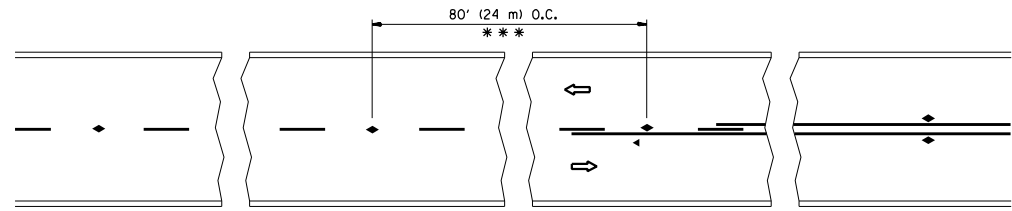
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Default	Default	DRAWN -	REVISED - T. RAMMACHER 01-06-00
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 10/25/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

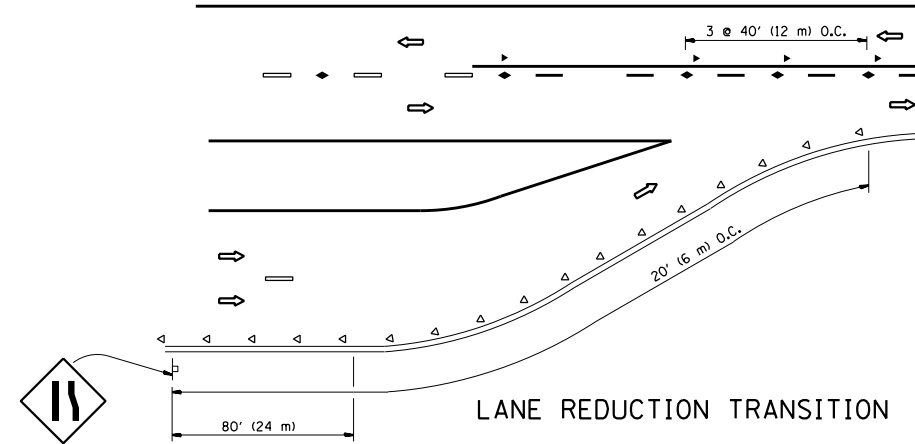
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(112&113)RS-10	COOK	24	17
<b>TC-10</b>			<b>CONTRACT NO. 62B25</b>	
ILLINOIS FED. AID PROJECT				

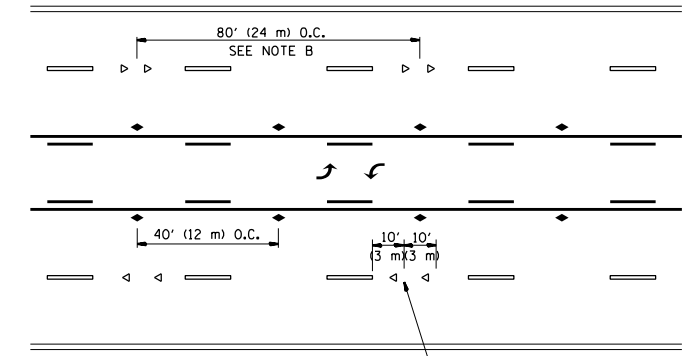


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

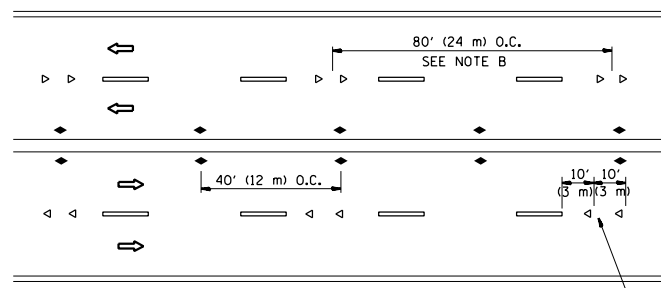
TWO-LANE/TWO-WAY



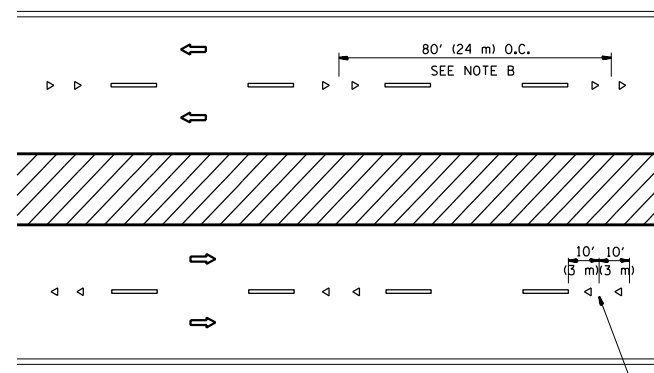
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

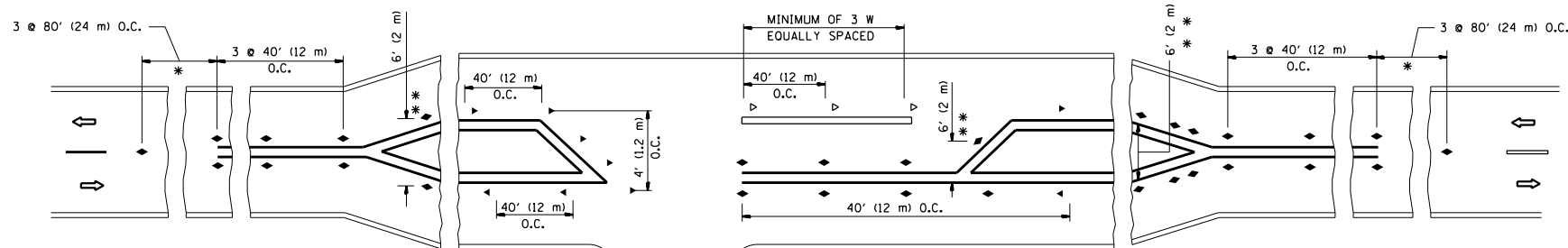
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

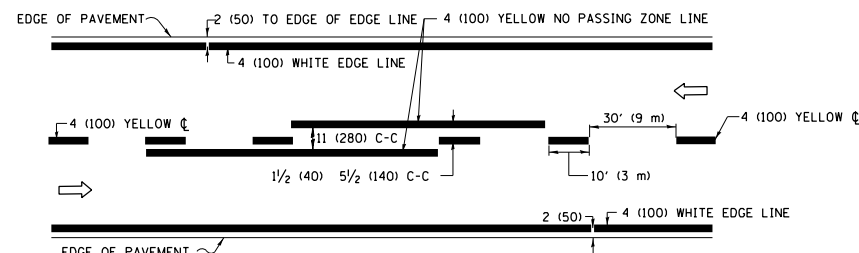


LEFT TURN

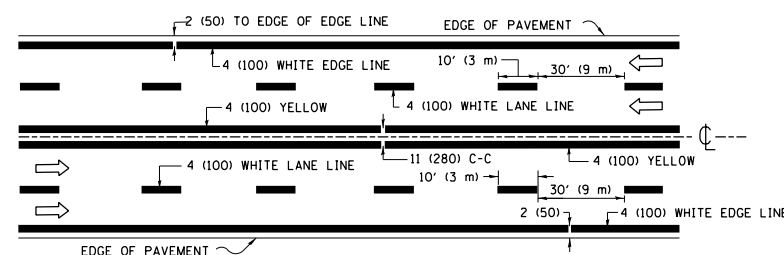
\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (1.2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

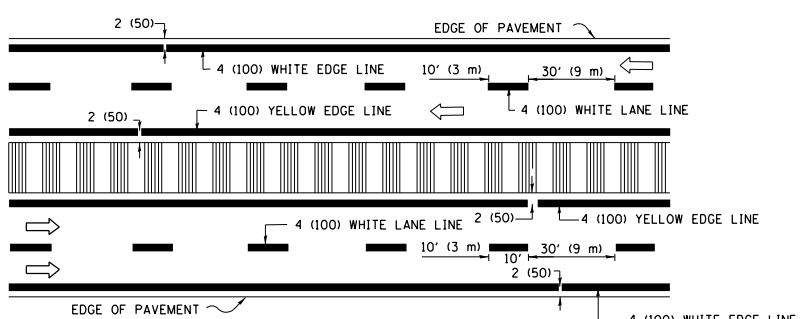
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PLOT SCALE = 100.4537' / in.	CHECKED -	REVISED - T. RAMMACHER 03-12-99	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	<b>TC-11</b>		<b>CONTRACT NO. 62B25</b>	
PLOT DATE = 10/25/2016	DATE -	REVISED - C. JUCIUS 09-09-09			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



**2-LANE ROADWAY**

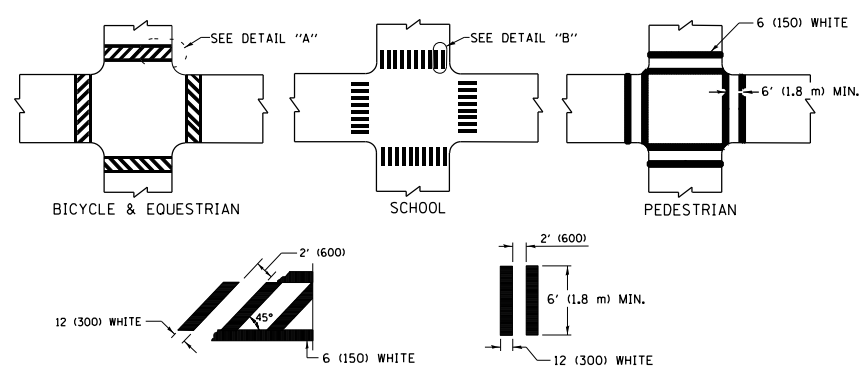


**MULTI-LANE UNDIVIDED**



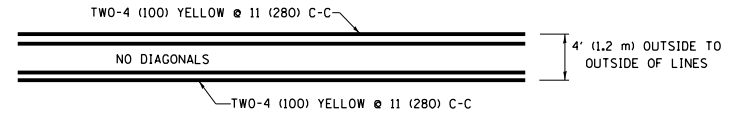
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

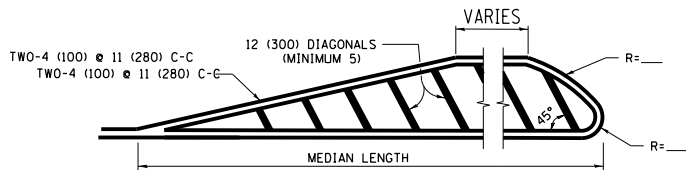


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

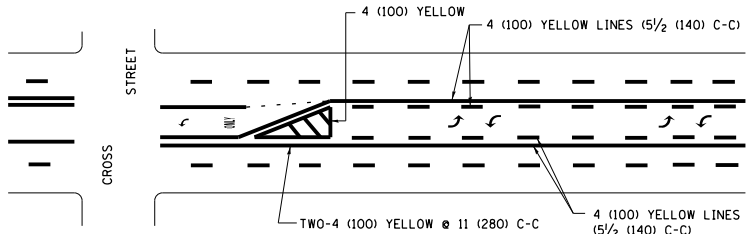


**4' (1.2 m) WIDE MEDIANS ONLY**



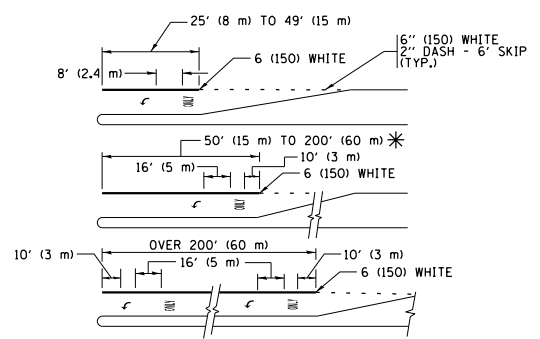
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

**MEDIANS OVER 4' (1.2 m) WIDE**



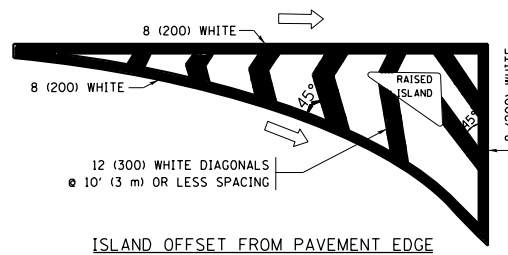
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

**MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING**

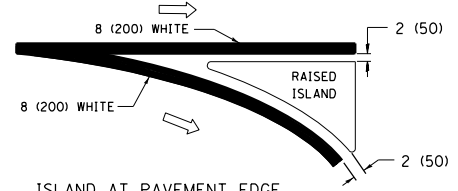


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

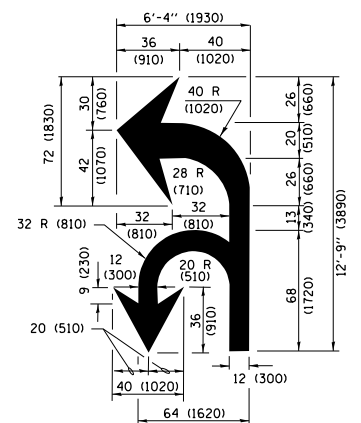
**TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING**



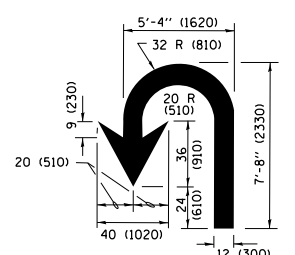
**ISLAND OFFSET FROM PAVEMENT EDGE**



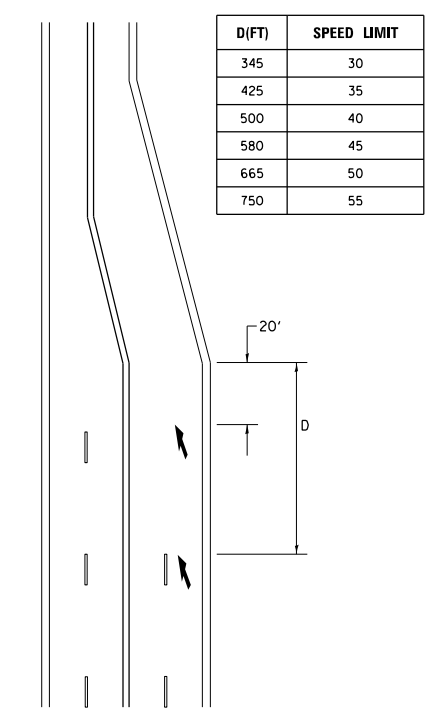
**ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**



**LANE REDUCTION TRANSITION**  
 \* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINE; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

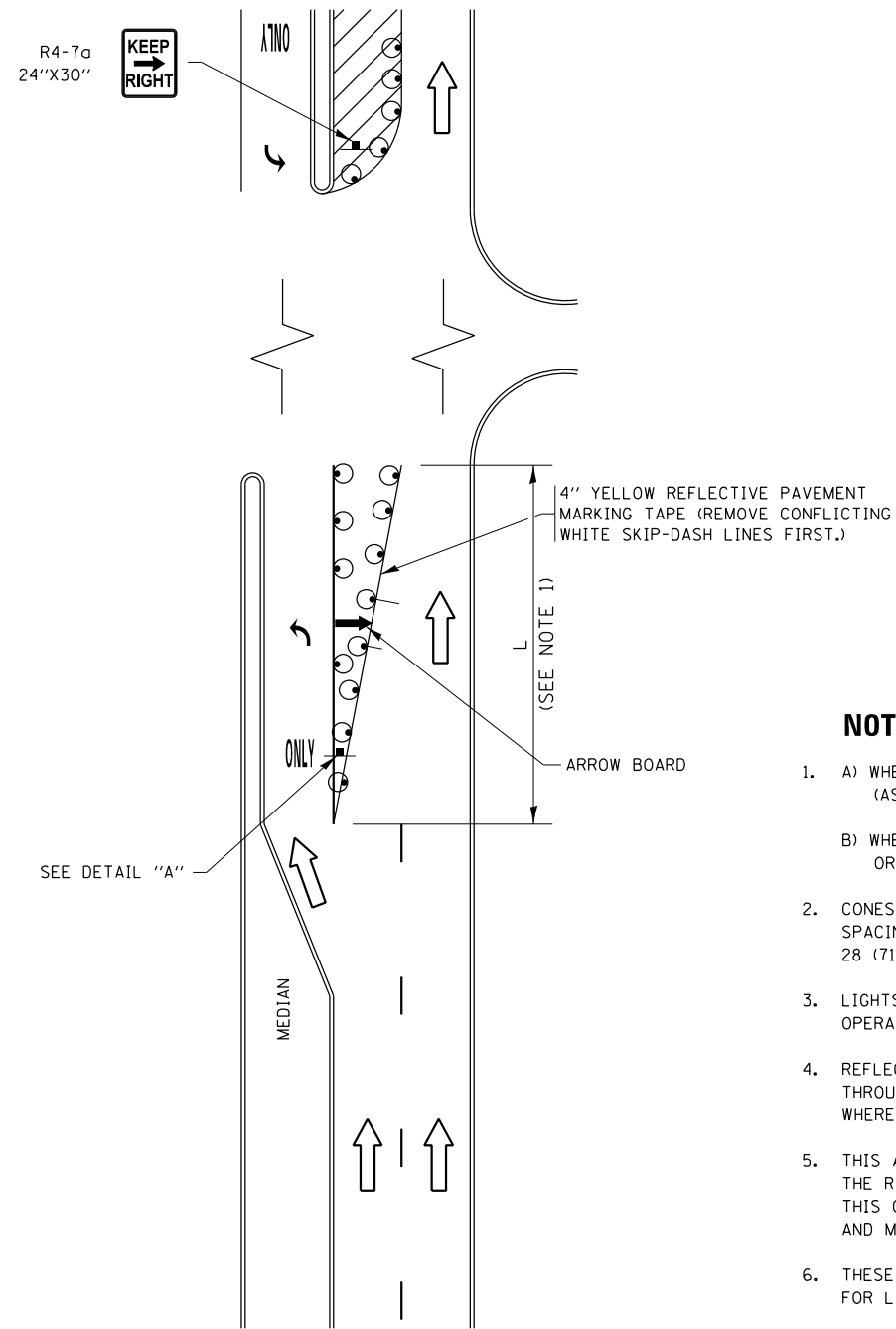
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	PLOT SCALE = 100.4537' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 10/25/2016		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT ONE TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	

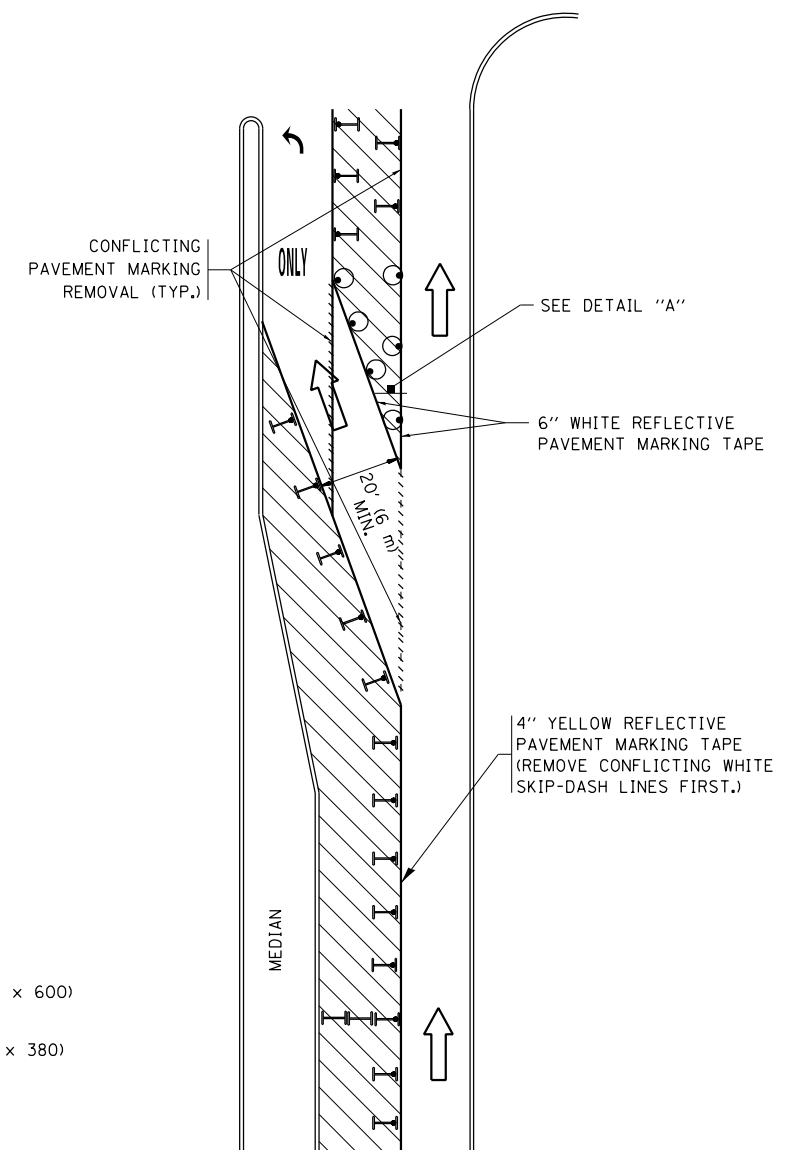
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(112&113)RS-10	COOK	24	19
<b>TC-13</b>		<b>CONTRACT NO. 62B25</b>		
ILLINOIS FED. AID PROJECT				

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



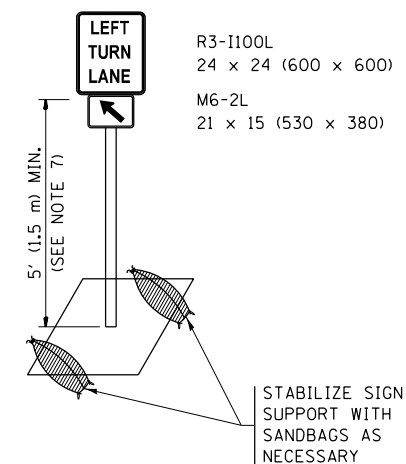
**FIGURE 2**

### LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

### NOTES:

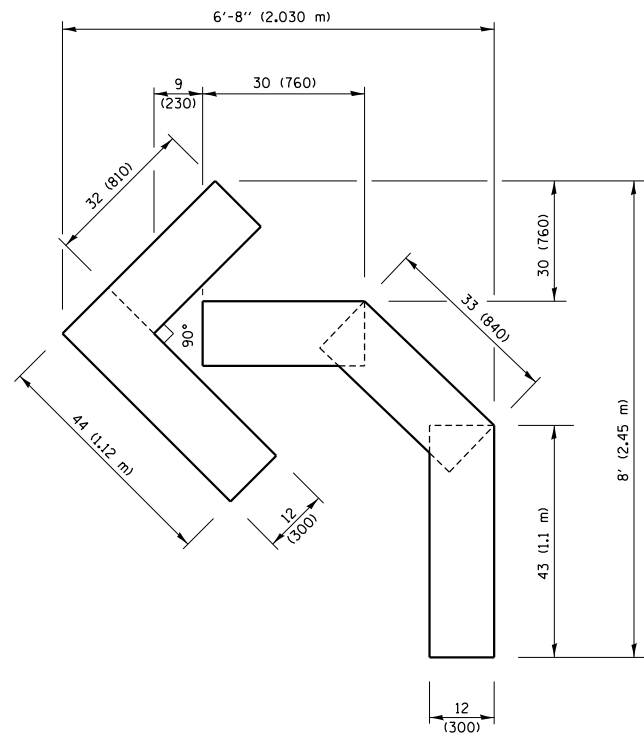
1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



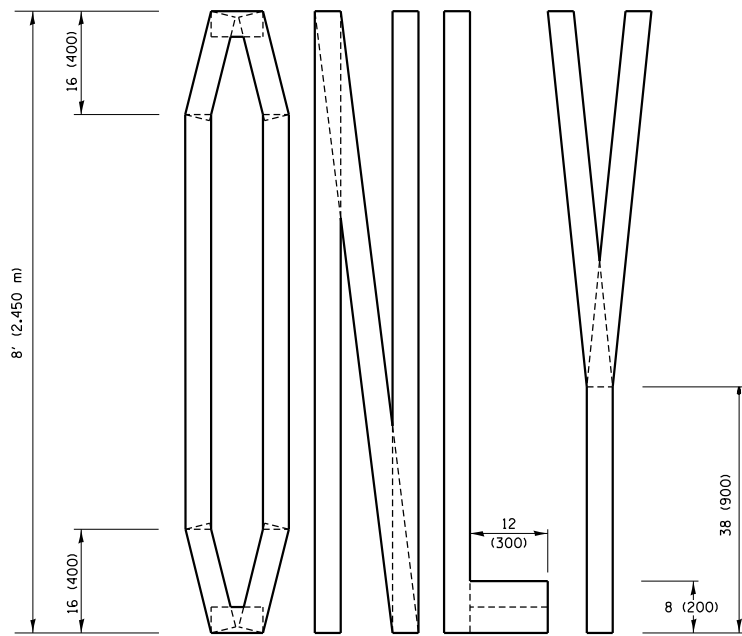
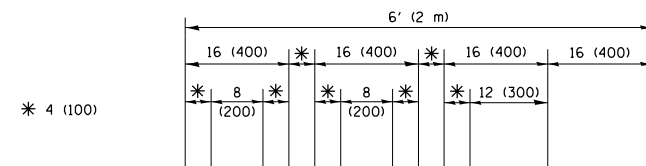
**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

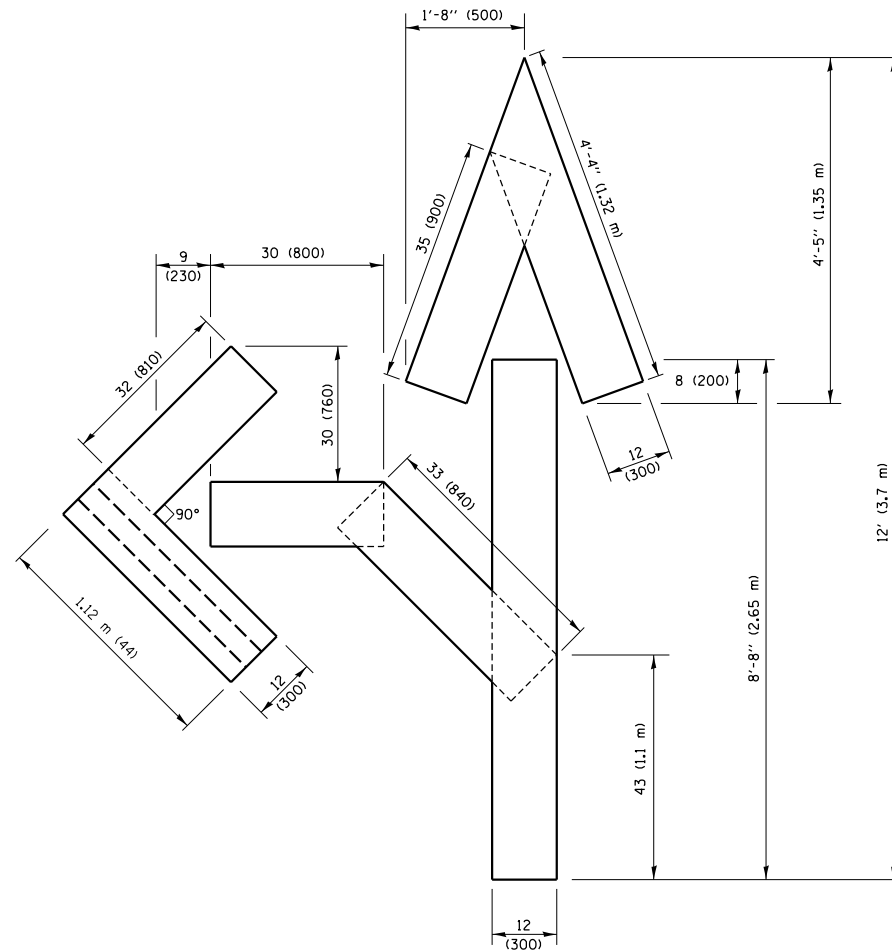
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Default	Default	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13			338	(112&113)RS-10	COOK	24	20	
		REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			<b>TC-14</b>		<b>CONTRACT NO. 62B25</b>		ILLINOIS FED. AID PROJECT	
		REVISED - T. RAMMACHER 01-06-00	REVISED -			SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.			



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)



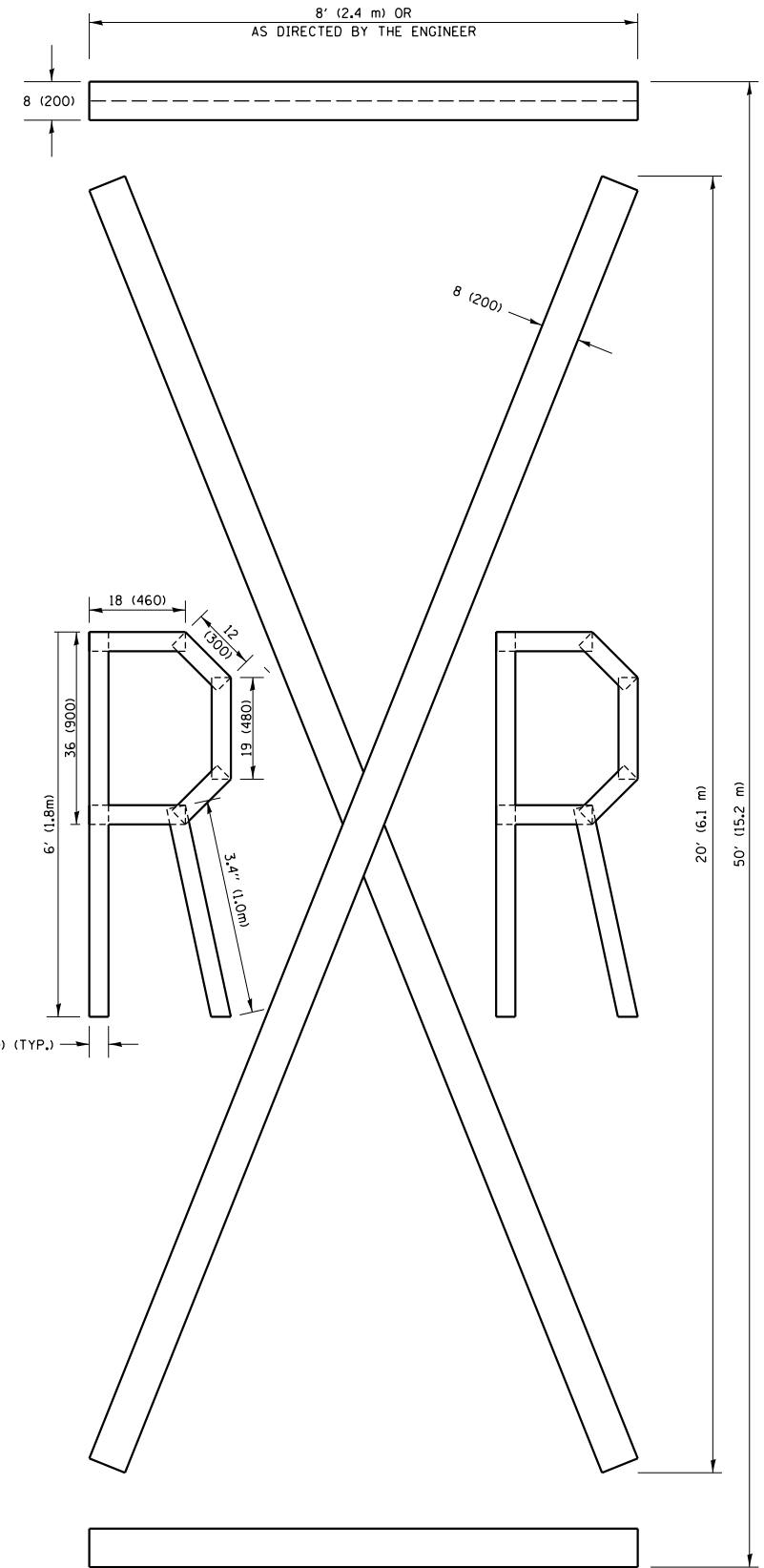
**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

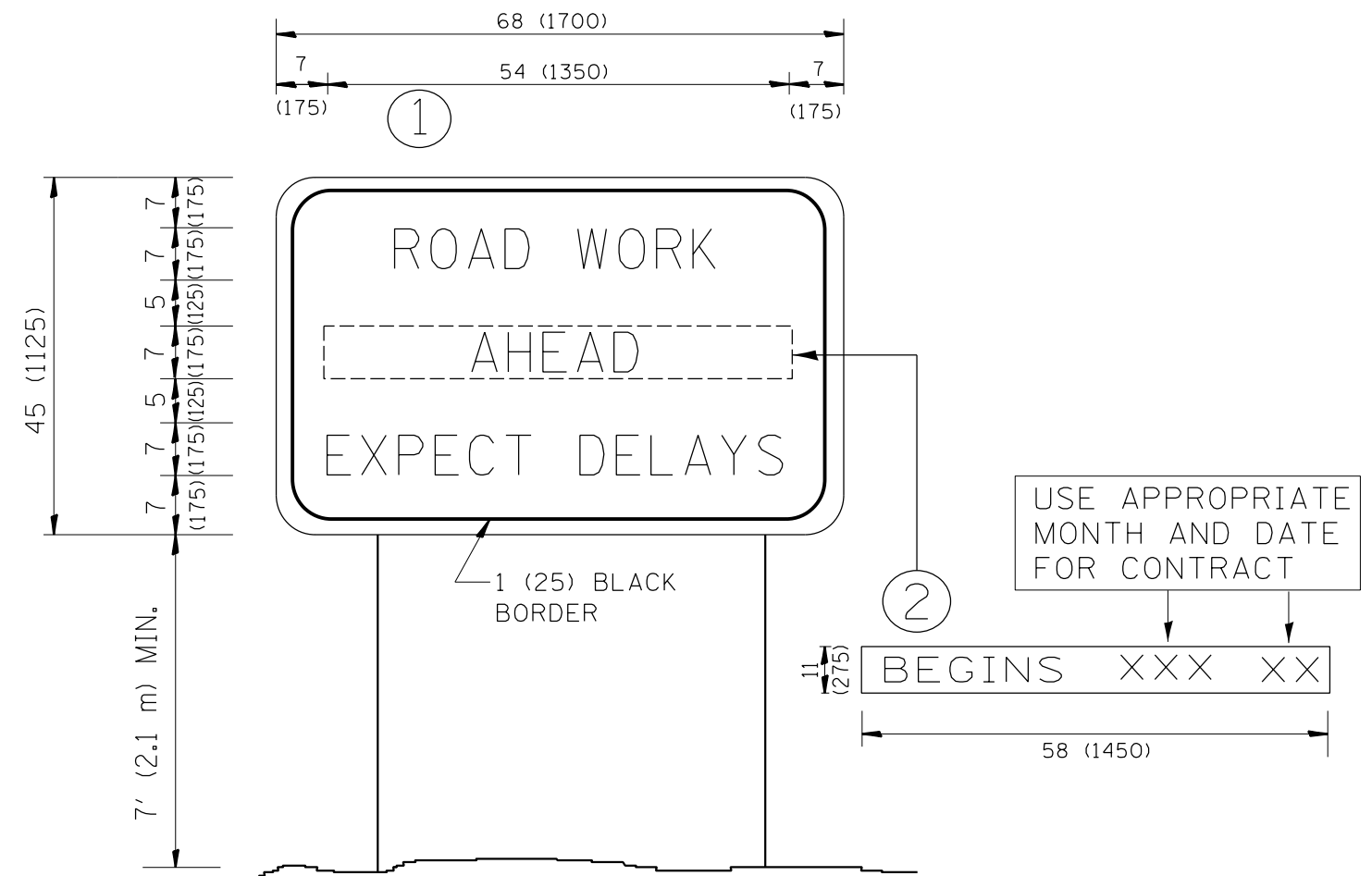
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	PLOT SCALE = 100.0010' / 1"	CHECKED -	REVISED - E. GOMEZ 08-28-00
	PLOT DATE = 10/25/2016	DATE -	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(112&113)RS-10	COOK	24	21
TC-16		CONTRACT NO. 62B25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = qureshiya	DESIGNED -	REVISED - R. MIRS 09-15-97
c:\pwork\pwork\qureshiya\d0458038\Disstd.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
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	PLOT DATE = 10/25/2016	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

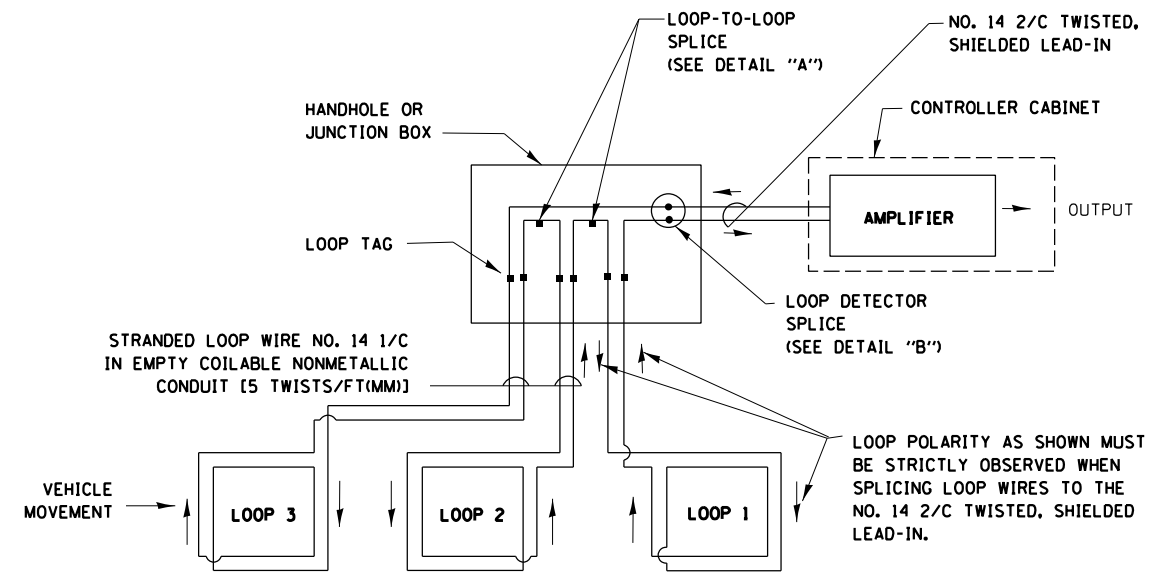
**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-22			CONTRACT NO. 62B25	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

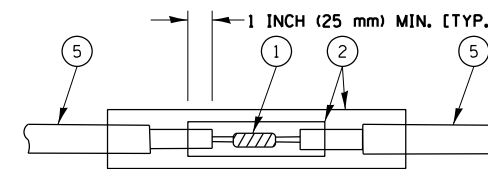
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

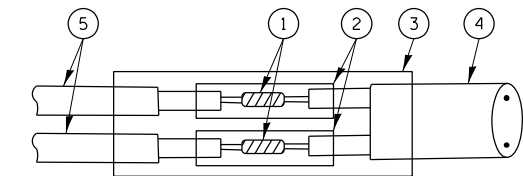


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

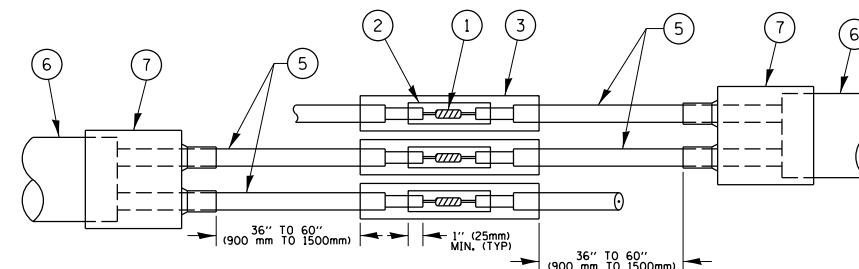


**DETAIL "A"  
LOOP-TO-LOOP SPLICE**



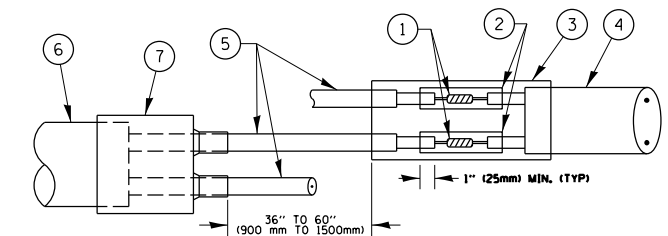
**DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE**

**TYPE I LOOP**



**DETAIL "A"  
LOOP-TO-LOOP SPLICE**

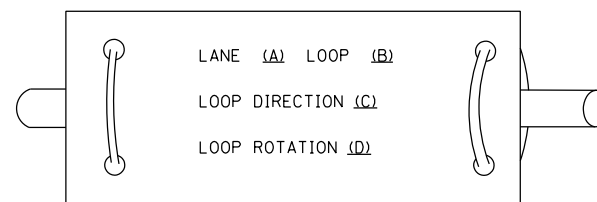
**PREFORMED LOOP**



**DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE**

**LOOP DETECTOR SPLICE**

**LOOP LEAD-IN CABLE TAG**



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

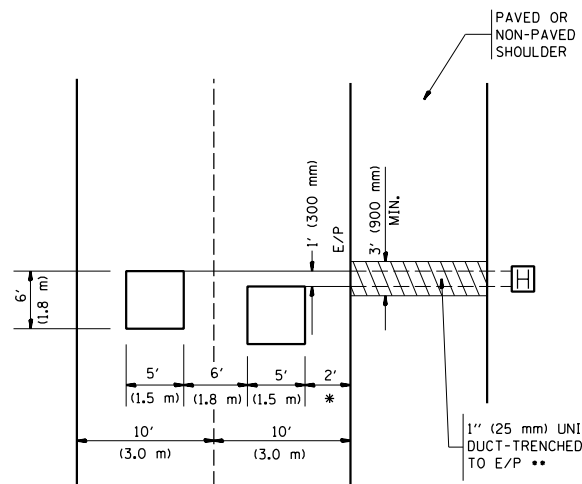
- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.

- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PREFORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = qureshiya	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
ci:\pw\work\pidot\qureshiya\0458038\Diststd.dgn		DRAWN -	REVISED -		SCALE: NONE	SHEET 2	OF 7	SHEETS	STA.	TO STA.	338	(112&113)RS-10	COOK	24	23
Default		PLOT SCALE = 100.3215 ' / in.	REVISED -						<b>TS-05</b>		<b>CONTRACT NO. 62B25</b>				
		PLOT DATE = 10/25/2016	REVISED -						ILLINOIS FED. AID PROJECT						

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



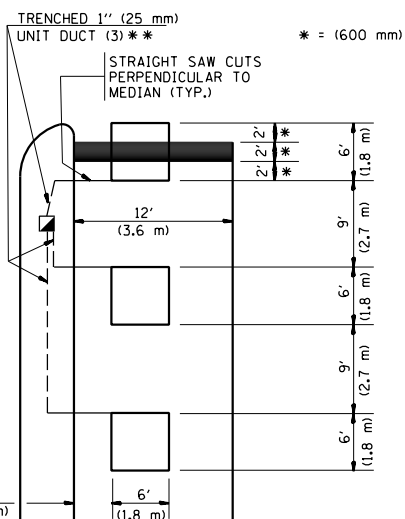
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



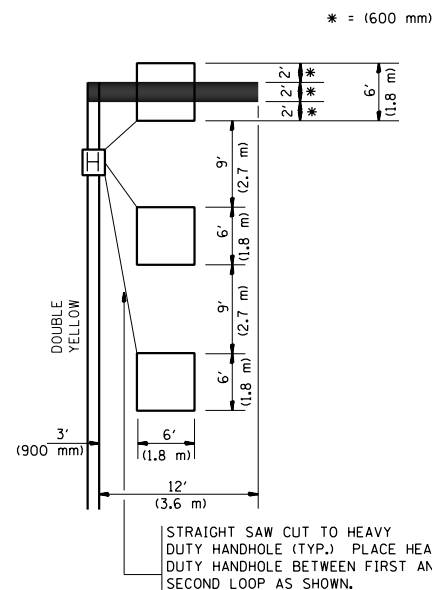
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



\* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

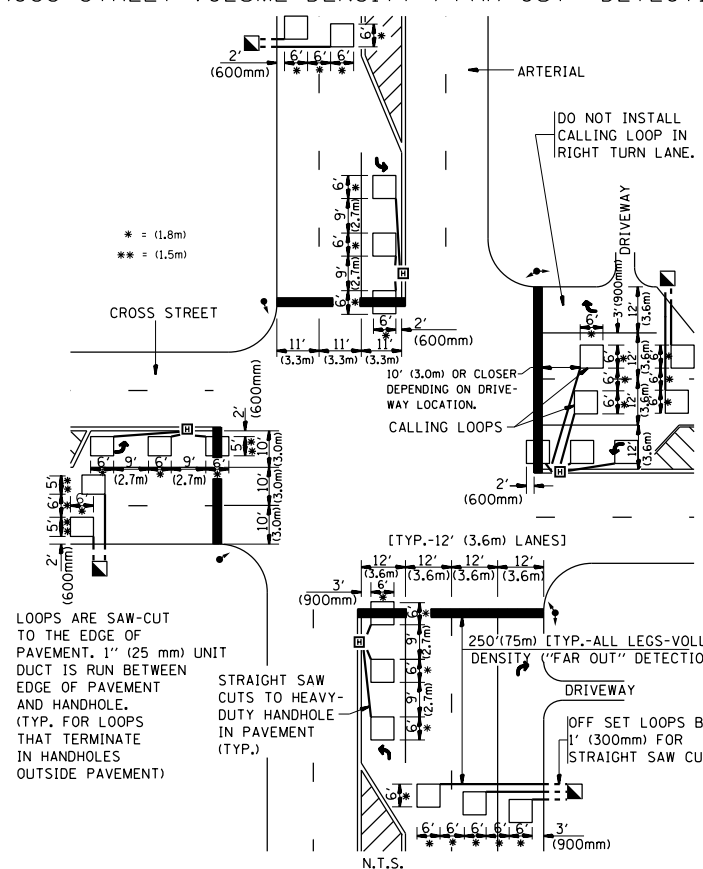
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



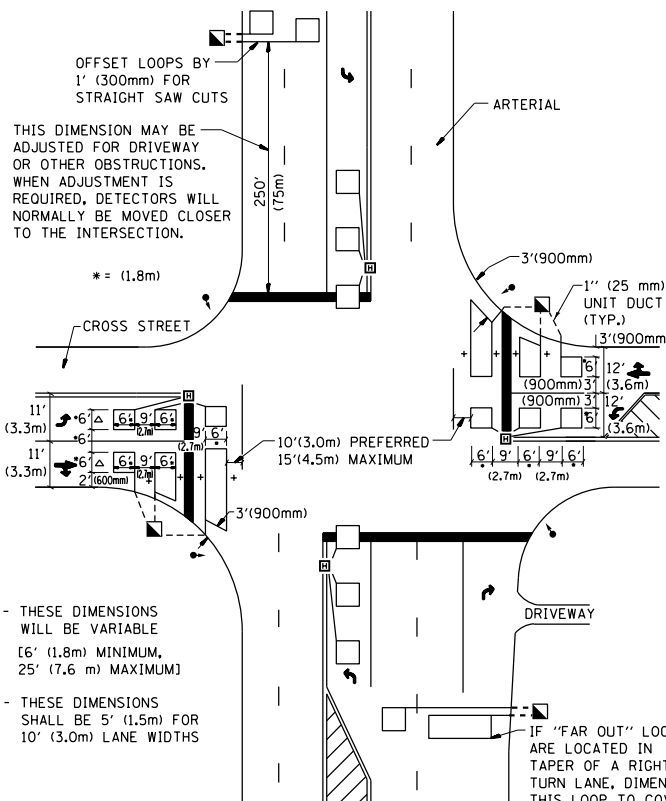
\* = (1.8m)  
\*\* = (1.5m)

LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

**DETAIL 1  
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

Δ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

**DETAIL 2  
N.T.S.**

FILE NAME =	USER NAME = qureshiya	DESIGNED -	REVISED -
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		CHECKED - R.K.F.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION  
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	(112&113)RS-10	COOK	24	24
<b>TS-07</b>		<b>CONTRACT NO. 62B25</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				