

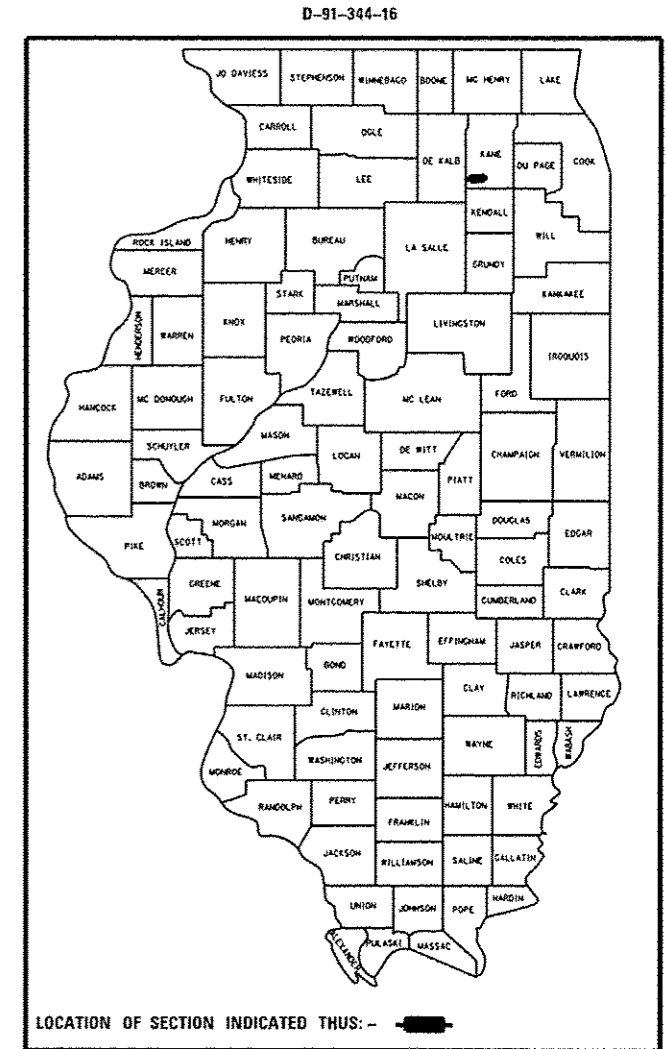
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117R)RS-7	KANE	47	1
		ILLINOIS	CONTRACT NO.: 62C37	

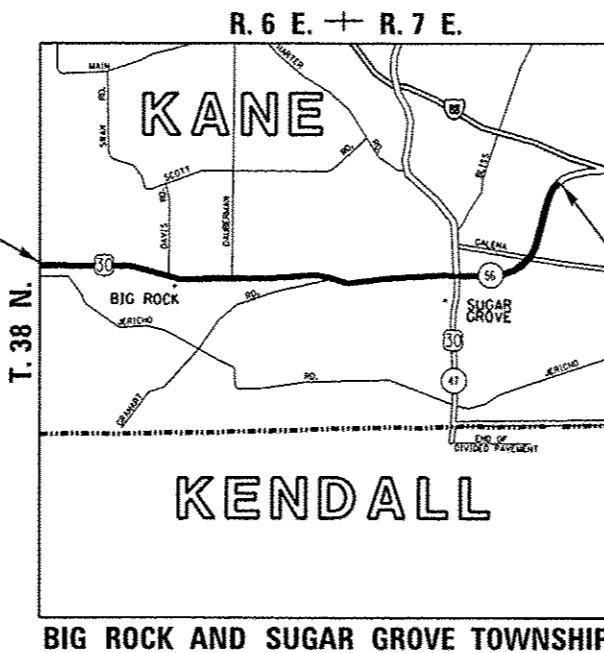
FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGES OF BIG ROCK AND SUGAR GROVE

**PROPOSED HIGHWAY PLANS**  
**F.A.P. 573 (US 30 /IL 56)**  
**DE KALB COUNTY LINE TO I-88**  
**RESURFACING (3P)**  
**SECTION NO.: (27, 30, 61 & 117R)RS-7**  
**PROJECT: ACNHPP-0573(500)**  
**KANE COUNTY**  
**C-91-344-16**



PROJECT BEGINS  
STA. 18 + 47



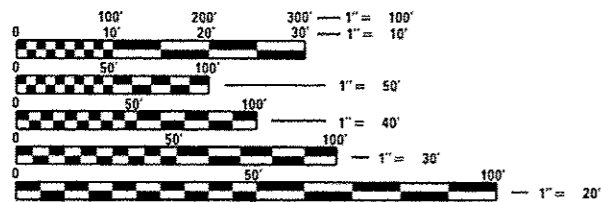
**TRAFFIC DATA:**

2015 ADT - 6450 TO 16700  
SPEED LIMIT - 35 TO 55 MPH

**OMISSIONS:**

- WEST BRANCH BIG ROCK CREEK  
STA. 49 + 98 TO STA. 50 + 57
- EAST BRANCH BIG ROCK CREEK  
STA. 106 + 91 TO STA. 108 + 14
- WELCH CREEK  
STA. 250 + 24 TO STA. 250 + 74
- DUGAN RD. TO MUNICIPAL DR.  
STA. 321 + 62 TO STA. 419 + 19
- US 30 /IL 47  
STA. 456 + 54 TO STA. 459 + 42
- BLACKBERRY CREEK  
STA. 555 + 29 TO STA. 556 + 81

PROJECT ENDS  
STA. 626 + 77



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: JENPAI CHANG (847) 705-4432  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62C37

GROSS LENGTH OF IMPROVEMENT - 60830 FEET - 11.52 MILES  
NET LENGTH OF IMPROVEMENT - 50401 FEET - 9.55 MILES

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED October 21, 2016  
*John Fortman*  
REGION ENGINEER

Dec 9, 2016  
*Muhammad M. Addis*  
ENGINEER OF DESIGN AND ENVIRONMENT

Dec 9, 2016  
*Kareem*  
DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

SHEET NO.	DESCRIPTION:	STATE STANDARDS	GENERAL NOTES:
1	COVER SHEET	000001-06 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS	BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR "CUAN" (CHICAGO UTILITY ALERT NETWORK) AT 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOURS NOTIFICATION IS REQUIRED).
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	442201-03 CLASS C AND D PATCHES	
3-4	SUMMARY OF QUANTITIES	604001-04 FRAMES AND LIDS, TYPE 1	10 FEET (3 METERS) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
5-9	TYPICAL SECTIONS	604006-05 FRAMES AND GRATES, TYPE 23	
10-30	ROADWAY AND PAVEMENT MARKING PLANS	606001-06 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER	THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES AND THE VILLAGES OF BIG ROCK AND SUGAR GROVE.
31-33	SIDEWALK RAMP DETAILS	701001-02 OFF ROAD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5m) AWAY	
34	DETAILS FOR FRAMES AND LIDS ADJUSTMENTS WITH MILLING (BD-8)	701006-05 OFF ROAD OPERATIONS, 2L, 2W, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE	THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
35	DETAILS FOR PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)	701101-05 OFF ROAD OPERATIONS, MULTILANE, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE	
36	DETAILS FOR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)	701106-02 OFF ROAD OPERATIONS, MULTILANE, MORE THAN 15' (4.5m) AWAY	ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
37	BUTT JOINT AND HMA TAPER DETAILS (BD-32)	701201-04 LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS GREATER THAN OR EQUAL TO 45 MPH	
38	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)	701301-04 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS	ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
39	TYPICAL APPLICATION FOR RAISED REFLECTIVE PAVEMENT MARKERS (TC-11)	701306-06 LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY GREATER THAN OR EQUAL TO 45 MPH	
40-41	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS (TC-12)	701311-03 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS DAY ONLY	ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
42	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)	701606-10 URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN	
43	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)	701701-10 URBAN LANE CLOSURE, MULTILANE INTERSECTION	LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
44	PAVEMENT MARKING--LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)	701901-06 TRAFFIC CONTROL DEVICES	
45	DISTRICT ONE - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)	780001-05 TYPICAL PAVEMENT MARKING	DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
46	DETECTOR LOOP LOCATION DETAILS	781001-04 TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS	
47	ARTERIAL ROAD INFORMATION SIGNING (TC-22)		IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENT SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT IN PART OF THIS CONTRACT.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING MACHINE IS SLOPED A MINIMUM (1:3).

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL CENTER AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL. (TC-13)

THE RESIDENT ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC ENGINEER, AT DON.CHIARUGI@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	90% FED 10% STATE			
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1			
20200100	EARTH EXCAVATION	CU YD	6	6			
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	105	105			
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	1	1			
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	1	1			
25200110	SODDING, SALT TOLERANT	SO YD	105	105			
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	154074	154074			
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	343	343			
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	9416	9416			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	690	690			
40603565	<sup>POLYMERIZED</sup> HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70	TON	22370	22370			
42001300	PROTECTIVE COAT	SO YD	147	147			
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	338	338			

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	90% FED 10% STATE			
42400800	DETECTABLE WARNINGS	SO FT	42	42			
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	228257	228257			
44000600	SIDEWALK REMOVAL	SO FT	357	357			
44201839	CLASS D PATCHES, TYPE II, 16 INCH	SO YD	1493	1493			
44201843	CLASS D PATCHES, TYPE III, 16 INCH	SO YD	448	448			
44201845	CLASS D PATCHES, TYPE IV, 16 INCH	SO YD	530	530			
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	5005	5005			
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	6	6			
* 66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1			
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6			
67100100	MOBILIZATION	LSUM	1	1			
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	LSUM	1	1			
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	LSUM	1	1			

\* SPECIALTY ITEMS  
 NON-PARTICIPATING ITEMS

FILE NAME :	USER NAME = STEEOPA	DESIGNED -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES  
FAP 573 (US 30 /IL 56) — DE KALB COUNTY LINE TO I-88**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)R5-7	KANE	47	3
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	90% FED 10% STATE				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	14512	14512				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	2419	2419				
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	329	329				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	159155	159155				
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	973	973				
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	5726	5726				
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	2727	2727				
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	138	138				
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	7256	7256				
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	329	329				
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	159155	159155				
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	973	973				
*78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	5726	5726				

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	90% FED 10% STATE				
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	2727	2727				
*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	138	138				
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1832	1832				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1558	1558				
*85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1	1				
*88600600	DETECTOR LOOP REPLACEMENT	FOOT	1597	1597				
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SO FT	56542	56542				
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	1252	1252				
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	7	7				
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	660	660				
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4				

\* SPECIALTY ITEMS  
 NON-PARTICIPATING ITEMS

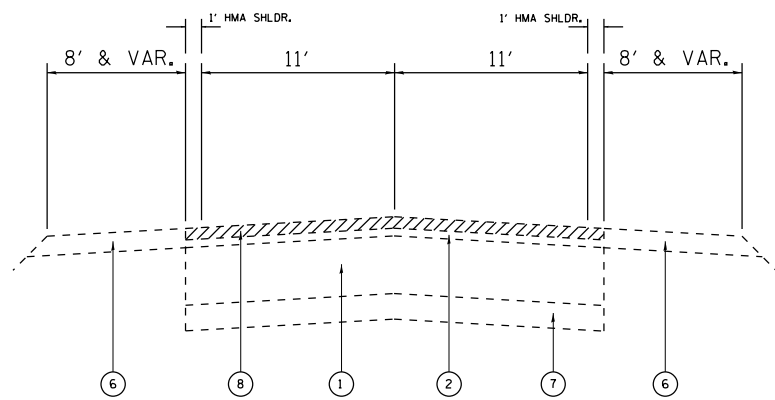
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	PLOT DATE : 10/26/2016		

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

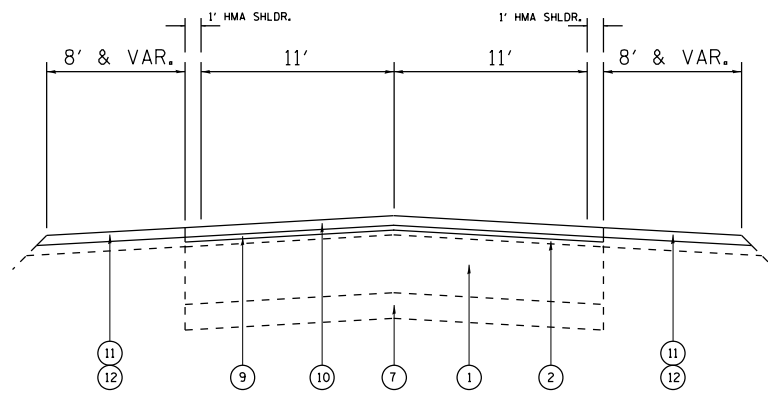
SUMMARY OF QUANTITIES  
FAP 573 (US 30 /IL 56) - DE KALB COUNTY LINE TO I-88

SCALE: SHEET NO. OF SHEETS STA. TO STA.

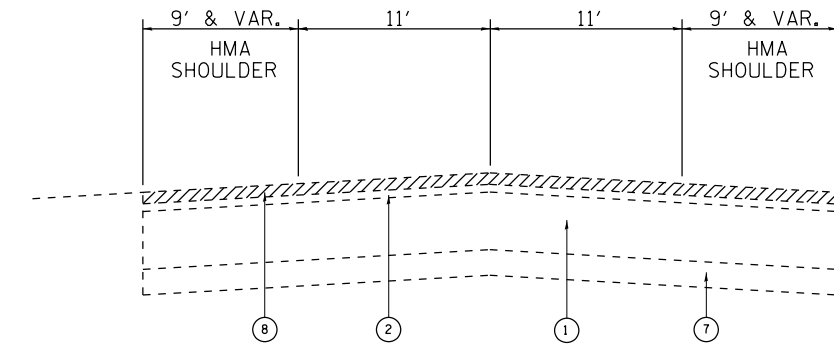
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	127, 30, 61 & 117RHS-7	KANE	47	4
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62C37	



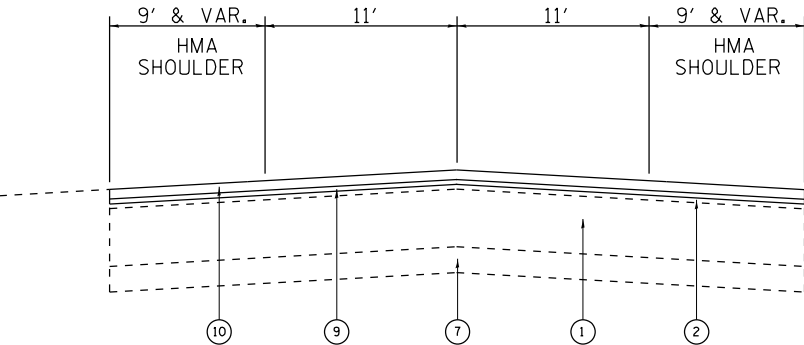
EXISTING TYPICAL CROSS SECTION  
 STA. 18+47 TO 34+90  
 STA. 40+87 TO 44+80  
 STA. 53+66 TO 103+39  
 STA. 109+83 TO 156+81  
 STA. 188+81 TO 247+77  
 STA. 253+00 TO 321+62



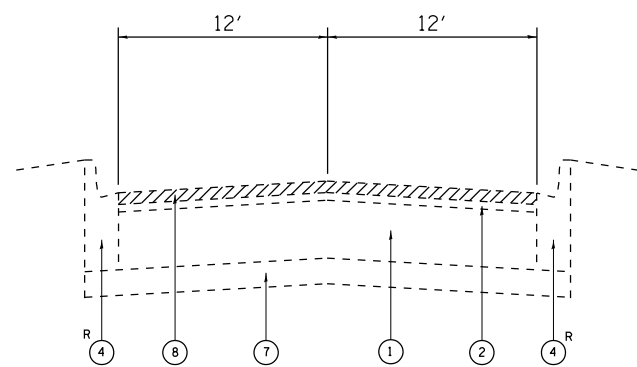
PROPOSED TYPICAL CROSS SECTION  
 STA. 18+47 TO 34+90  
 STA. 40+87 TO 44+80  
 STA. 53+66 TO 103+39  
 STA. 109+83 TO 156+81  
 STA. 188+81 TO 247+77  
 STA. 253+00 TO 321+62



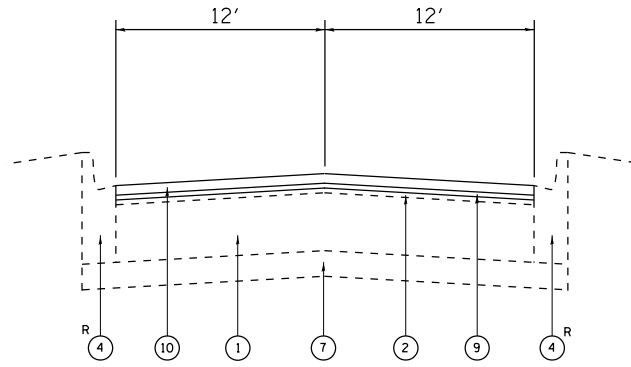
EXISTING TYPICAL CROSS SECTION  
 STA. 44+80 TO 53+66  
 STA. 103+39 TO 109+65  
 STA. 247+77 TO 253+00



PROPOSED TYPICAL CROSS SECTION  
 STA. 44+80 TO 53+66  
 STA. 103+39 TO 109+65  
 STA. 247+77 TO 253+00



EXISTING TYPICAL CROSS SECTION  
 STA. 156+81 TO 188+81



PROPOSED TYPICAL CROSS SECTION  
 STA. 156+81 TO 188+81

**LEGEND:**

- ① EXISTING P.C.C. PAVEMENT ±10"
  - ② EXISTING HOT-MIX ASPHALT SURFACE, ±6" (AFTER MILLING)
  - ③ EXISTING LANDSCAPE MEDIAN
  - ④ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
  - ⑤ EXISTING P.C.C. SIDEWALK
  - ⑥ EXISTING AGGREGATE SHOULDER
  - ⑦ EXISTING STABILIZED SUB-BASE
  - ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
  - ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
  - ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1¾"
  - ⑪ PROPOSED GRADING AND SHAPING SHOULDERS
  - ⑫ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

**MIXTURE REQUIREMENTS**

MIXTURE TYPE	AIR VOIDS @ Ndes	OMP
<b>PATCHING</b>		
CLASS "D" PATCHES (HMA BINDER COURSE, IL-19MM)	4% @ 70 GYR.	QC / OA
<b>PAVEMENT RESURFACING</b>		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70 (IL 9.5MM)	4% @ 70 GYR.	QCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% @ 50 GYR.	QC / OA
OMP DESIGNATION: QUALITY CONTROL FOR PERFORMANCE (QCP); QUALITY CONTROL / QUALITY ASSURANCE (QC / OA); PAY FOR PERFORMANCE (PFP)		

**NOTES:**

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT QUANTITIES IS 112 LBS./SQ. YD./ IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

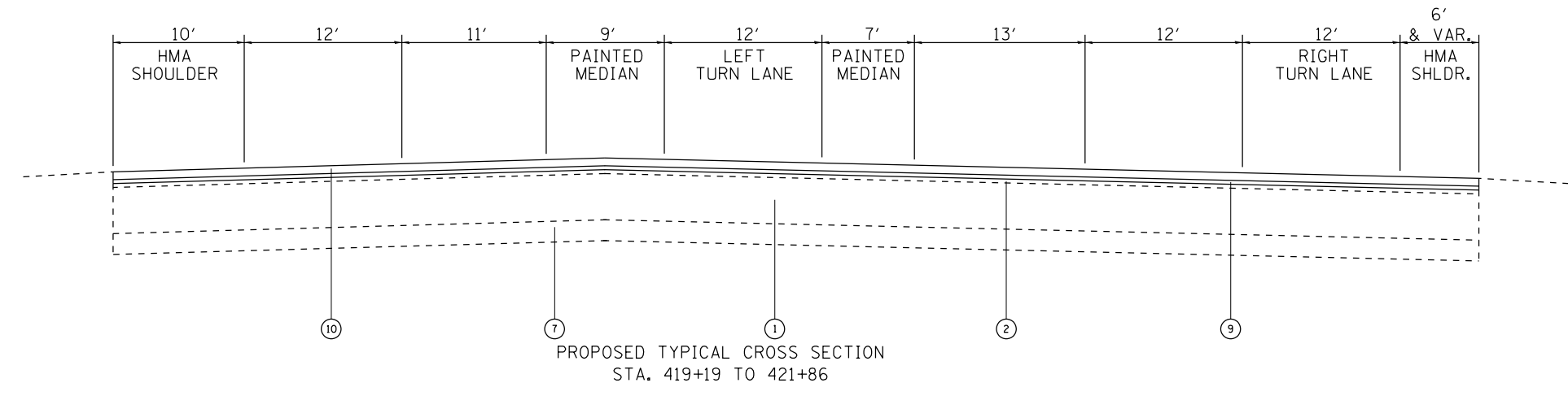
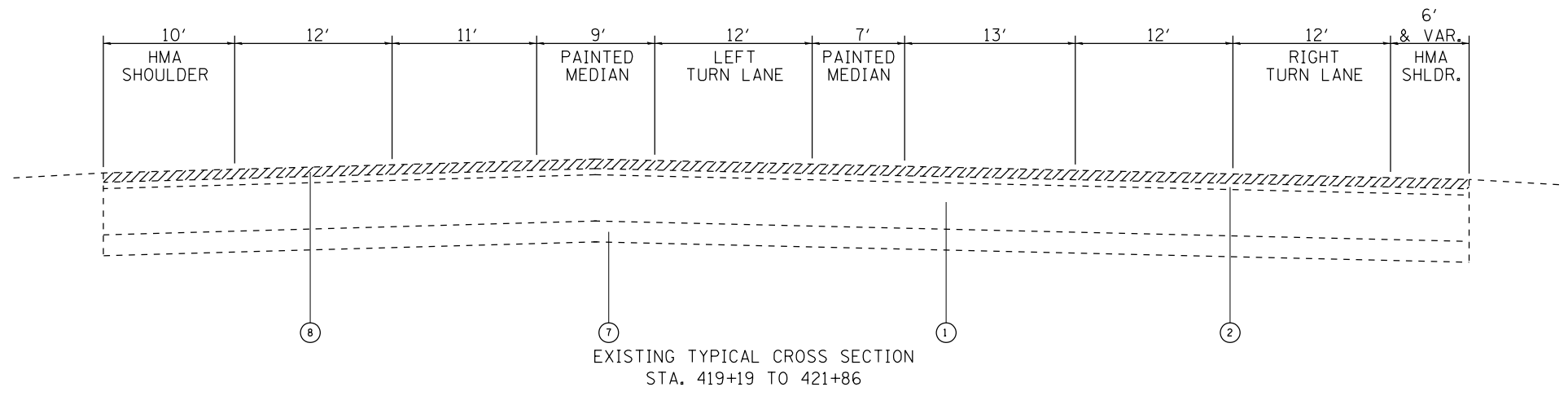
THE MILLING SHALL BE DONE PRIOR TO PATCHING

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

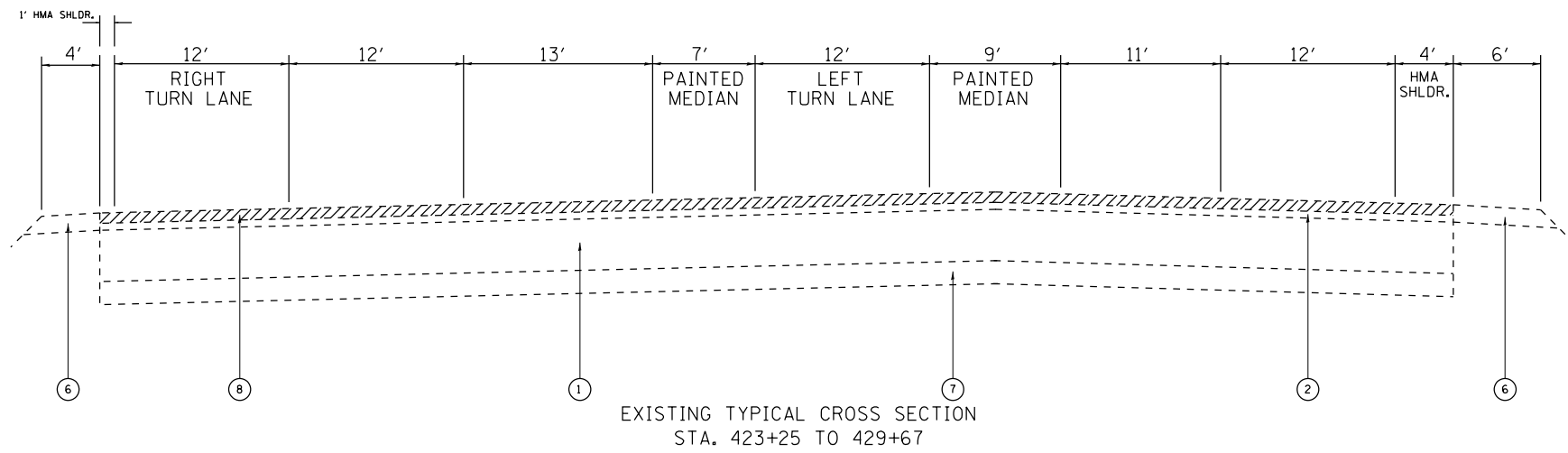
FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS

**LEGEND:**

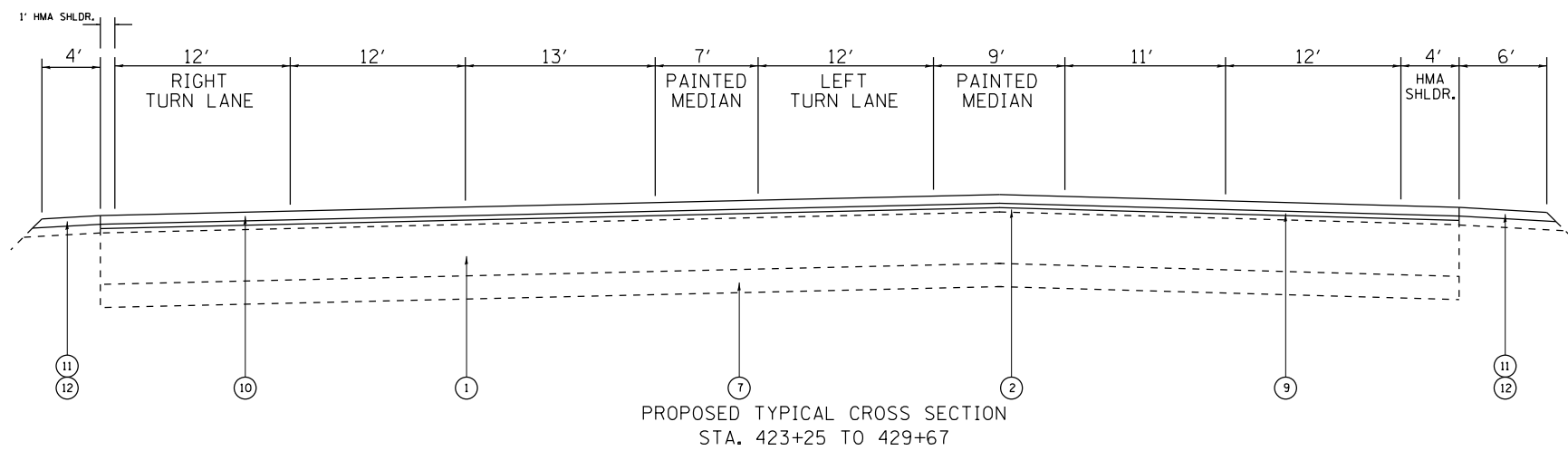
- ① EXISTING P. C. C. PAVEMENT ±10"
  - ② EXISTING HOT-MIX ASPHALT SURFACE, ±6" (AFTER MILLING)
  - ③ EXISTING LANDSCAPE MEDIAN
  - ④ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
  - ⑤ EXISTING P. C. C. SIDEWALK
  - ⑥ EXISTING AGGREGATE SHOULDER
  - ⑦ EXISTING STABILIZED SUB-BASE
  - ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
  - ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
  - ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1¾"
  - ⑪ PROPOSED GRADING AND SHAPING SHOULDERS
  - ⑫ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
- R** CURB AND GUTTER REMOVAL AND REPLACEMENT  
(AS DIRECTED BY THE ENGINEER)



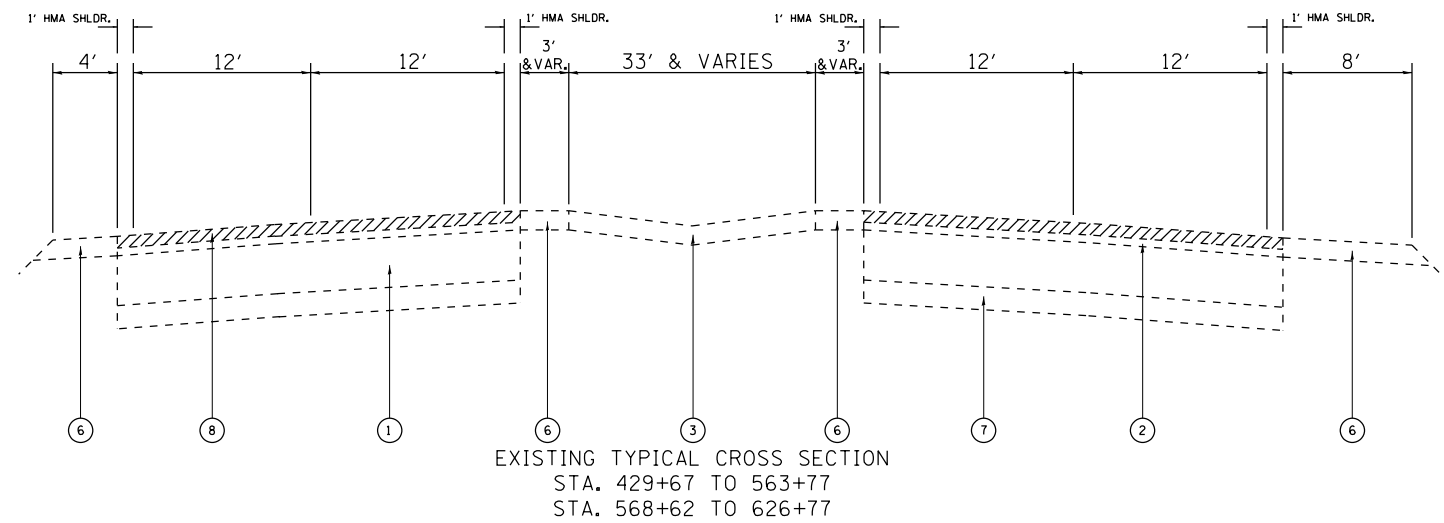
FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING AND PROPOSED TYPICAL SECTIONS FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI344\BROWns\Design\DI34416-sh-t-plen.dgn						573	(27, 30, 61 & 117)RS-7	KANE	47	6	
Default						CONTRACT NO. 62C37					
PLOT SCALE = 100.0000' / in.						ILLINOIS FED. AID PROJECT					
PLOT DATE = 11/3/2016				CHECKED -		REVISED -		REVISED -		REVISED -	
DATE -				DATE -		DATE -		DATE -		DATE -	
						SCALE:		SHEET OF SHEETS		STA. TO STA.	



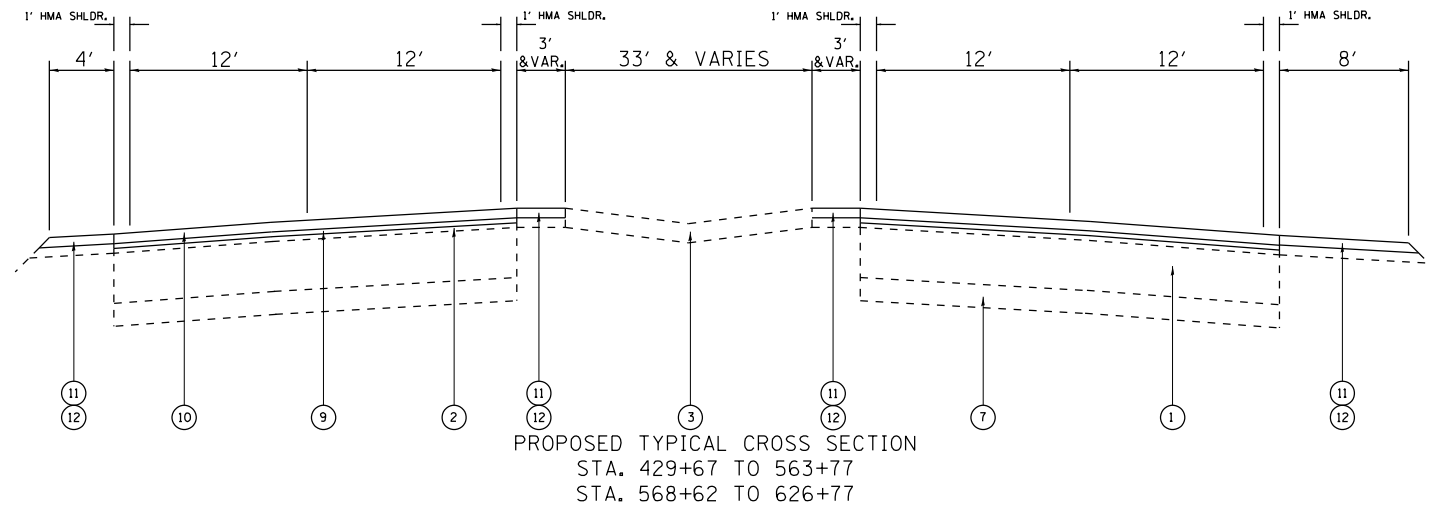
- LEGEND:**
- ① EXISTING P.C.C. PAVEMENT ±10"
  - ② EXISTING HOT-MIX ASPHALT SURFACE, ±6" (AFTER MILLING)
  - ③ EXISTING LANDSCAPE MEDIAN
  - ④ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
  - ⑤ EXISTING P.C.C. SIDEWALK
  - ⑥ EXISTING AGGREGATE SHOULDER
  - ⑦ EXISTING STABILIZED SUB-BASE
  - ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
  - ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
  - ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1¾"
  - ⑪ PROPOSED GRADING AND SHAPING SHOULDERS
  - ⑫ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING AND PROPOSED TYPICAL SECTIONS FAU 2692 (WOLF RD.)—US 12 (RAND RD.) TO IL 58 (GOLF RD.)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI3446\Drawings\Design\DI34416-sh-t-pln.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			573	(27, 30, 61 & 117)RS-7	KANE	47	7
Default	PLOT DATE = 11/3/2016	DATE -	REVISED -			CONTRACT NO. 62C37			ILLINOIS FED. AID PROJECT	
						SCALE:	SHEET	OF	SHEETS	STA.

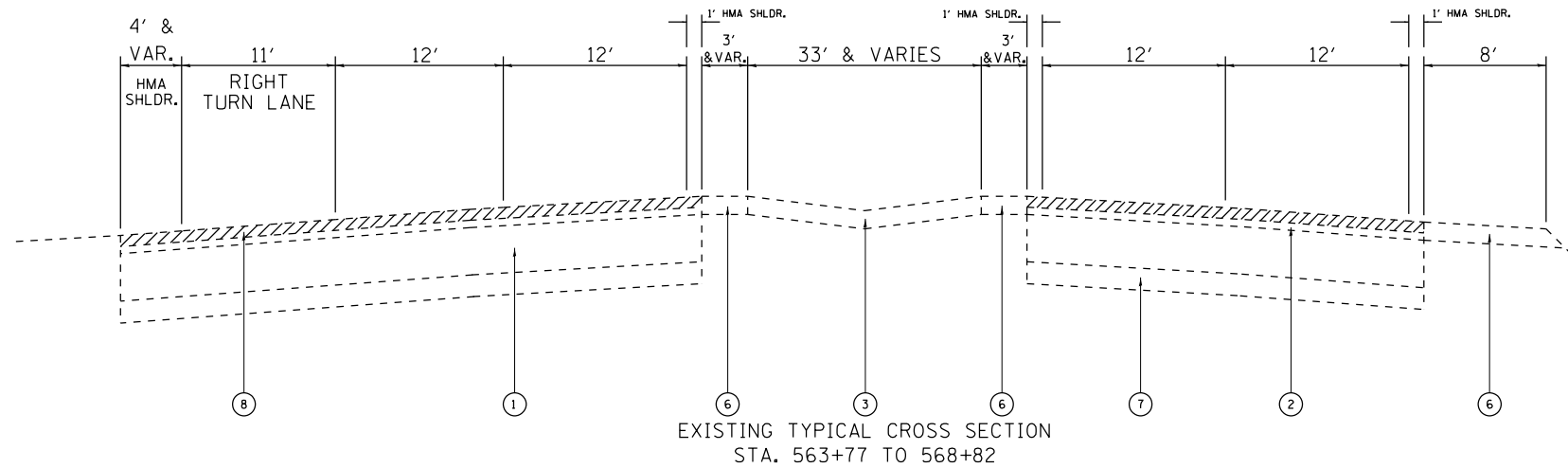


- LEGEND:**
- ① EXISTING P.C.C. PAVEMENT ±10"
  - ② EXISTING HOT-MIX ASPHALT SURFACE, ±6" (AFTER MILLING)
  - ③ EXISTING LANDSCAPE MEDIAN
  - ④ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
  - ⑤ EXISTING P.C.C. SIDEWALK
  - ⑥ EXISTING AGGREGATE SHOULDER
  - ⑦ EXISTING STABILIZED SUB-BASE
  - ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
  - ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
  - ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1¾"
  - ⑪ PROPOSED GRADING AND SHAPING SHOULDERS
  - ⑫ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
- R CURB AND GUTTER REMOVAL AND REPLACEMENT  
 (AS DIRECTED BY THE ENGINEER)

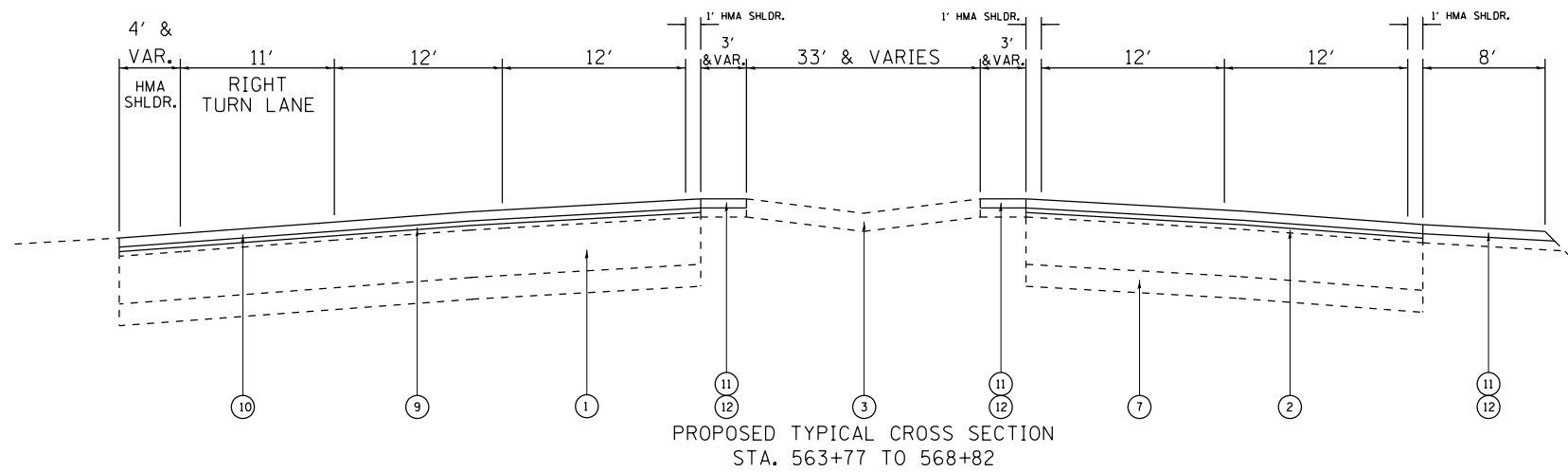


FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING AND PROPOSED TYPICAL SECTIONS FAP 573 (US 30 / IL 56)—DE KALB COUNTY LINE TO I-88</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI344\BROW\Meta\Design\DI34416-sh-t-plen.dgn		CHECKED -	REVISED -			573	(27, 30, 61 & 117)RS-7	KANE	47	8
Default	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -			CONTRACT NO. 62C37			ILLINOIS FED. AID PROJECT	
	PLOT DATE = 11/3/2016					SCALE:	SHEET OF SHEETS	STA. TO STA.		





- LEGEND:**
- ① EXISTING P.C.C. PAVEMENT ±10"
  - ② EXISTING HOT-MIX ASPHALT SURFACE, ±6" (AFTER MILLING)
  - ③ EXISTING LANDSCAPE MEDIAN
  - ④ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
  - ⑤ EXISTING P.C.C. SIDEWALK
  - ⑥ EXISTING AGGREGATE SHOULDER
  - ⑦ EXISTING STABILIZED SUB-BASE
  - ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
  - ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
  - ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1¾"
  - ⑪ PROPOSED GRADING AND SHAPING SHOULDERS
  - ⑫ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

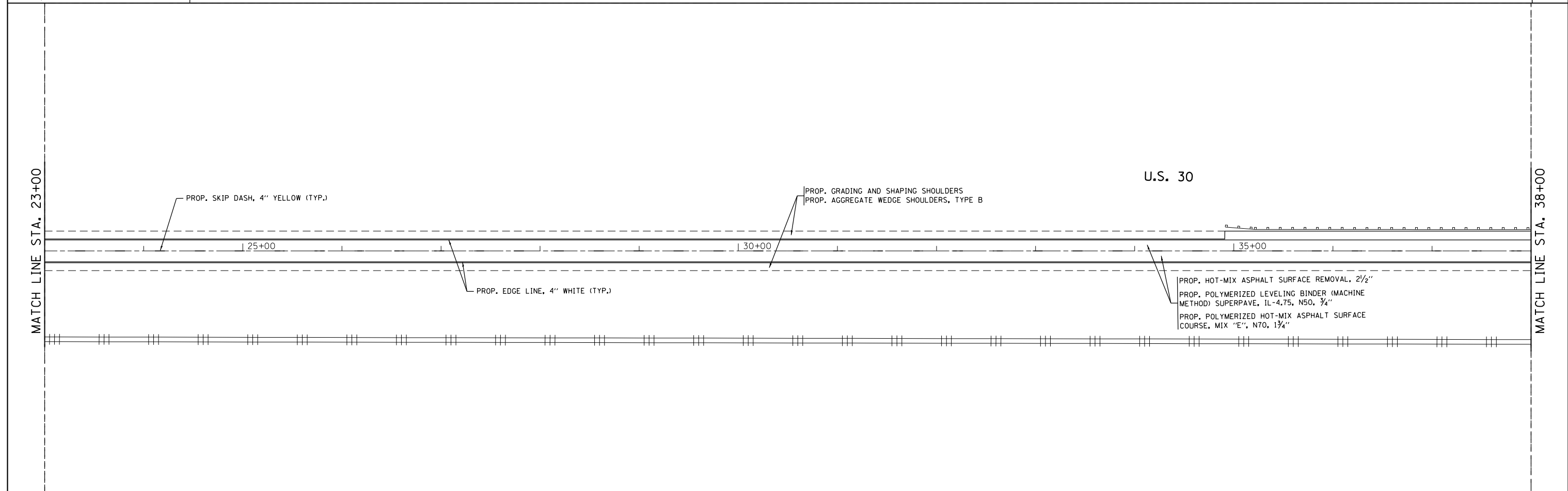
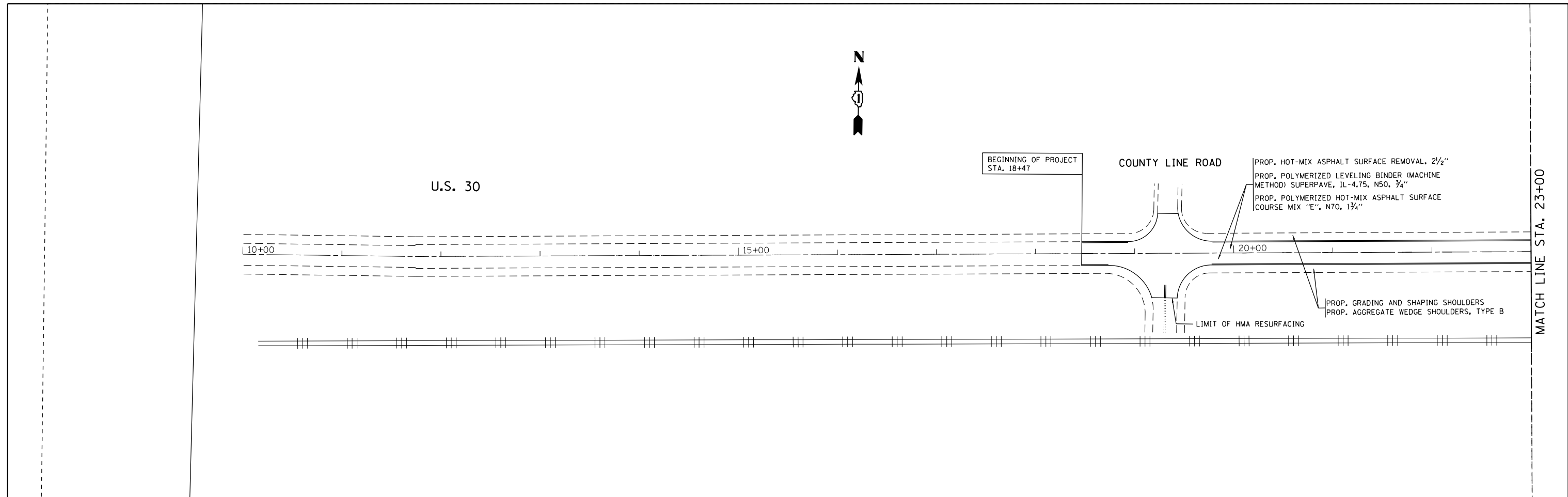


FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI3446\Drawings\Design\DI34416-sh-t-plen.dgn		CHECKED -	REVISED -
Default	PLOT DATE = 11/3/2016	DATE -	REVISED -

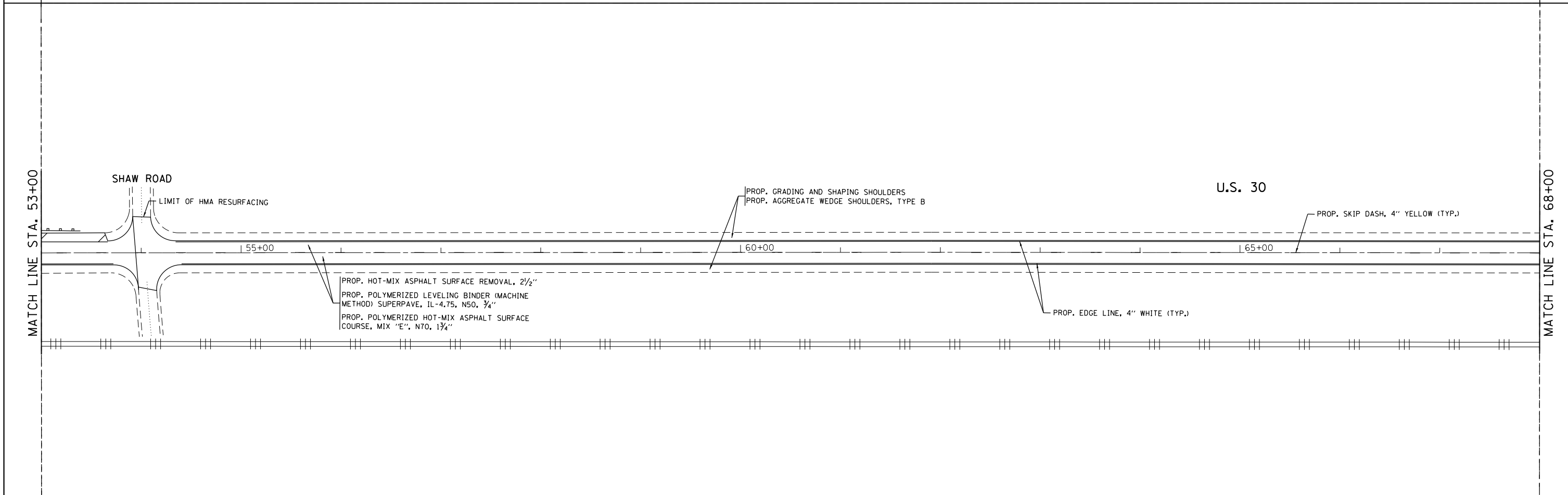
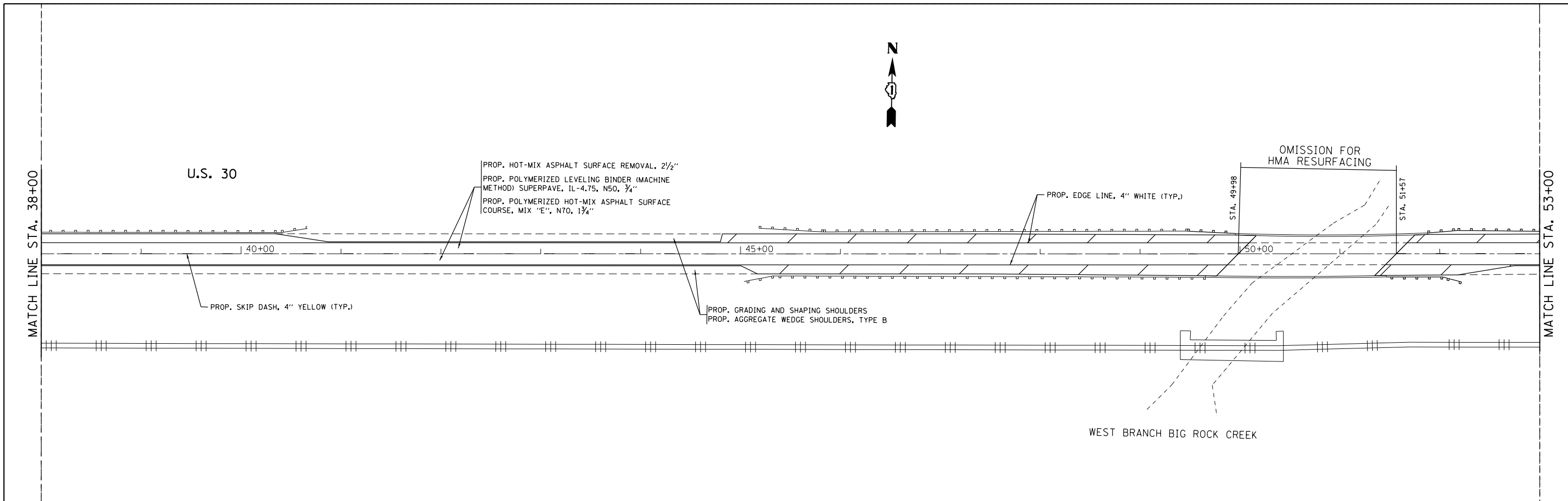
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>EXISTING AND PROPOSED TYPICAL SECTIONS</b>			
<b>FAP 573 (US 30 / IL 56)—DE KALB COUNTY LINE TO I-88</b>			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	9
<b>CONTRACT NO. 62C37</b>				
ILLINOIS FED. AID PROJECT				



FILE NAME = p:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI34416\Browns\Design\DI34416-shr-plan.dgn	USER NAME = STEEDPA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		<b>US ROUTE 30 / IL ROUTE 56 (DEKALB COUNTY LINE TO I-88)</b>		573	(27, 30, 61 & 117)RS-7	KANE	47	10
Default	PLOT DATE = 11/3/2016	DATE -	REVISED -	SCALE: 1" = 50'	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT CONTRACT NO. 62C37				



FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI3446\Drawings\Design\DI34416-shr-plan.dgn		CHECKED -	REVISED -			573	(27, 30, 61 & 117)RS-7	KANE	47	11
Default		DATE -	REVISED -			CONTRACT NO. 62C37				
	PLOT DATE = 11/3/2016					ILLINOIS FED. AID PROJECT				

SCALE: 1" = 50'    SHEET OF SHEETS    STA. TO STA.



MATCH LINE STA. 68+00

U.S. 30

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50, 3/4"  
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX 'E', N70, 1 1/4"

PROP. EDGE LINE, 4" WHITE (TYP.)

PROP. SKIP DASH, 4" YELLOW (TYP.)

PROP. GRADING AND SHAPING SHOULDERS  
 PROP. AGGREGATE WEDGE SHOULDERS, TYPE B

MATCH LINE STA. 83+00

MATCH LINE STA. 83+00

U.S. 30

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50, 3/4"  
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX 'E', N70, 1 1/4"

PROP. SKIP DASH, 4" YELLOW (TYP.)

PROP. EDGE LINE, 4" WHITE (TYP.)

PROP. GRADING AND SHAPING SHOULDERS  
 PROP. AGGREGATE WEDGE SHOULDERS, TYPE B

MATCH LINE STA. 98+00

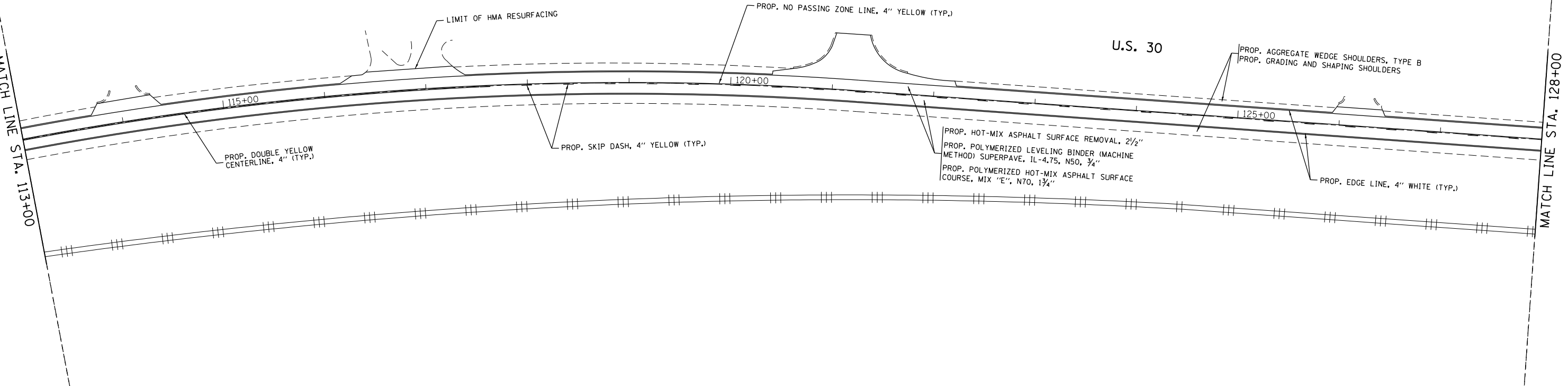
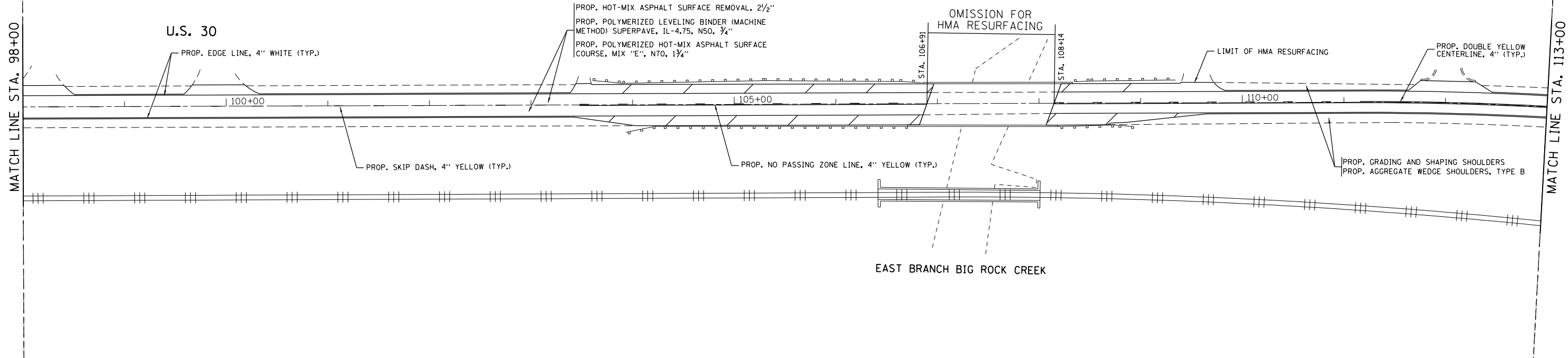
FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -
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Default	PLOT DATE = 11/3/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
 FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88**

SCALE: 1" = 50'      SHEET      OF      SHEETS      STA.      TO      STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	12
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62C37	



FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -
p:\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI34416\Meta\Design\DI34416-sh-t-plan.dgn		REVISED -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 11/3/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**  
**FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88**

SCALE: 1" = 50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	13
CONTRACT NO. 62C37				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 128+00

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50, 3/4"  
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"

U.S. 30

PROP. SKIP DASH, 4" YELLOW (TYP.)

LIMIT OF HMA RESURFACING

130+00

135+00

140+00

PROP. GRADING AND SHAPING SHOULDERS  
 PROP. AGGREGATE WEDGE SHOULDERS, TYPE B

PROP. EDGE LINE, 4" WHITE (TYP.)

MATCH LINE STA. 143+00

MATCH LINE STA. 143+00

U.S. 30 (GALENA ST.)

DAVIS ROAD

PROP. EDGE LINE, 4" WHITE (TYP.)

PROP. AGGREGATE WEDGE SHOULDERS, TYPE B  
 PROP. GRADING AND SHAPING SHOULDERS

145+00

150+00

155+00

PROP. SKIP DASH, 4" YELLOW (TYP.)

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50, 3/4"  
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"

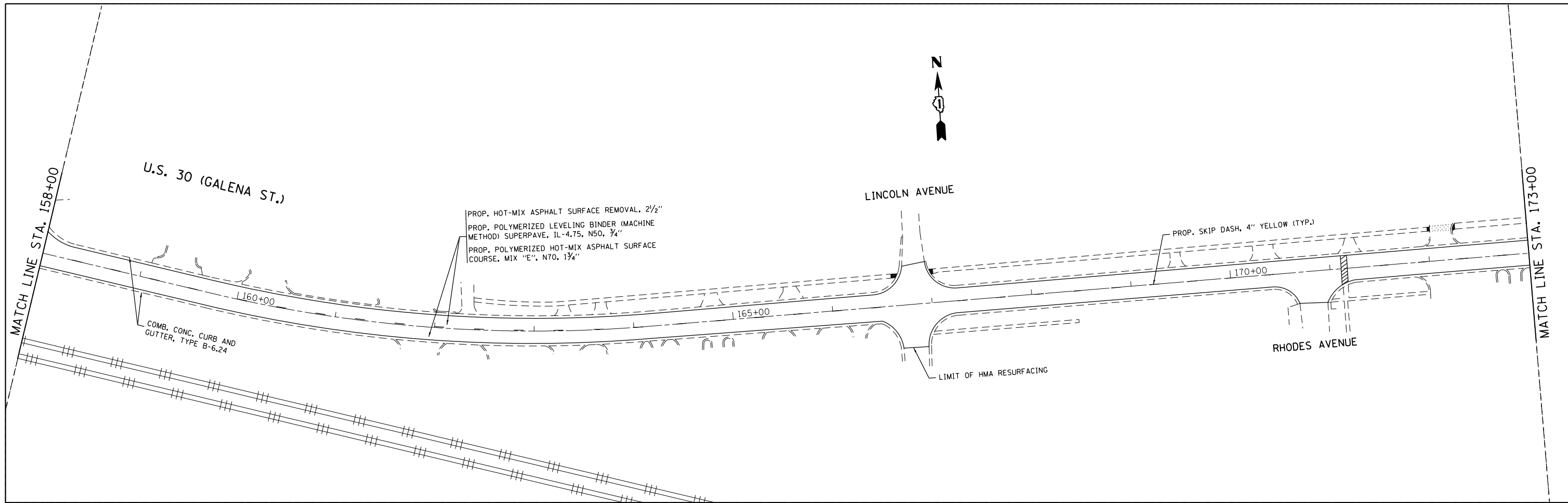
MATCH LINE STA. 158+00

FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -
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Default	PLOT DATE = 11/3/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**  
**FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88**  
 SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

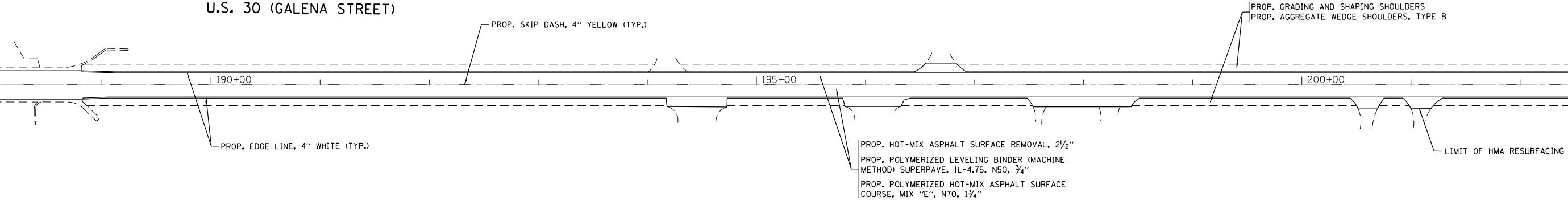
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	14
CONTRACT NO. 62C37				
ILLINOIS FED. AID PROJECT				





MATCH LINE STA. 188+00

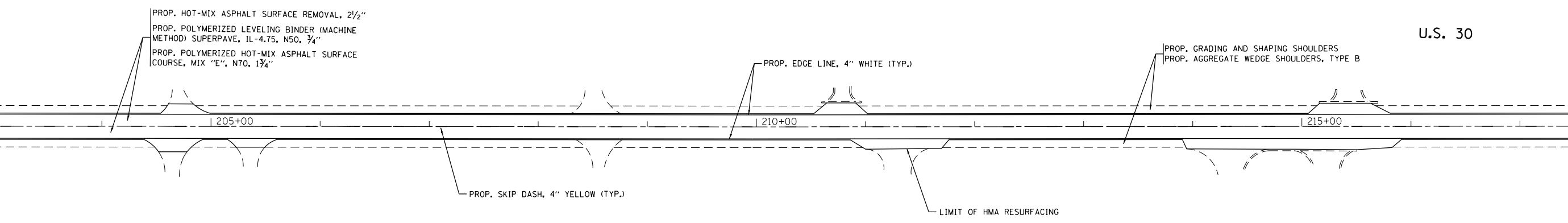
U.S. 30 (GALENA STREET)



MATCH LINE STA. 203+00

MATCH LINE STA. 203+00

U.S. 30



MATCH LINE STA. 218+00

FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI34416\Drawings\Design\DI34416-sh-t-plan.dgn		DRWN -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 11/3/2016	DATE -	REVISED -

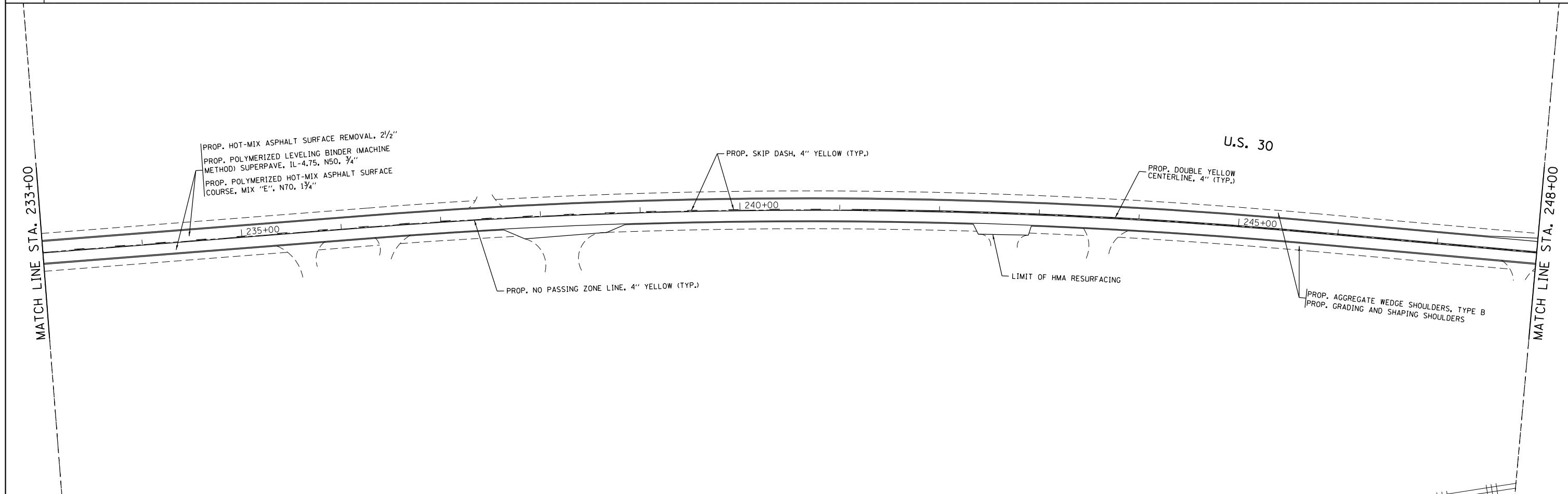
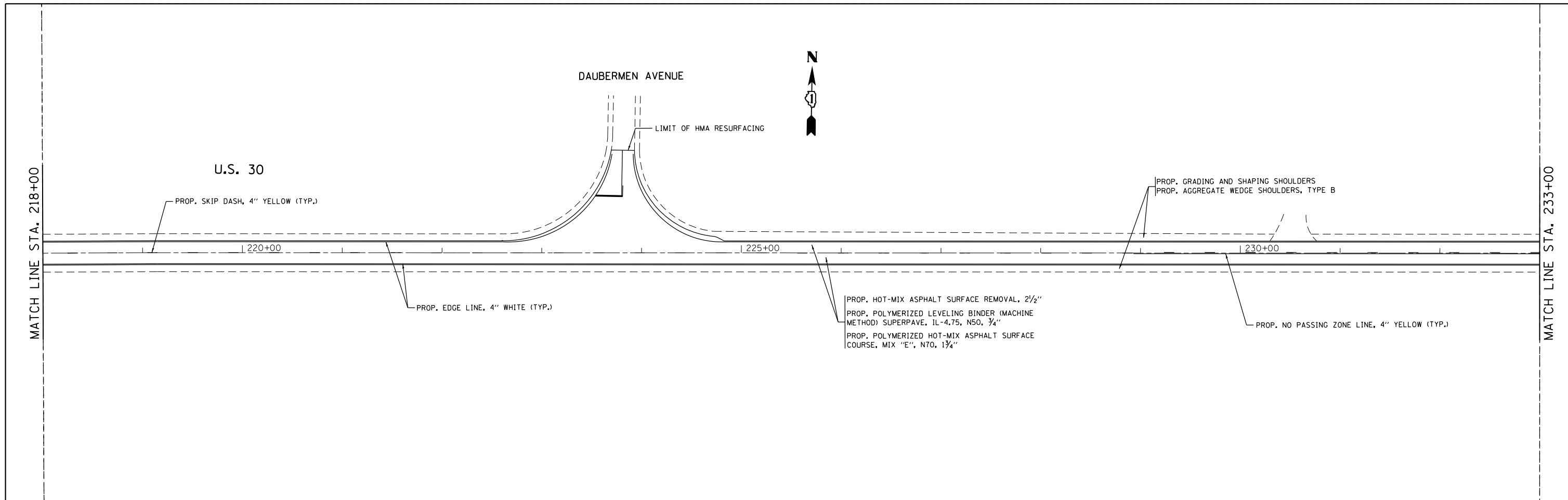
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**  
**FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88**

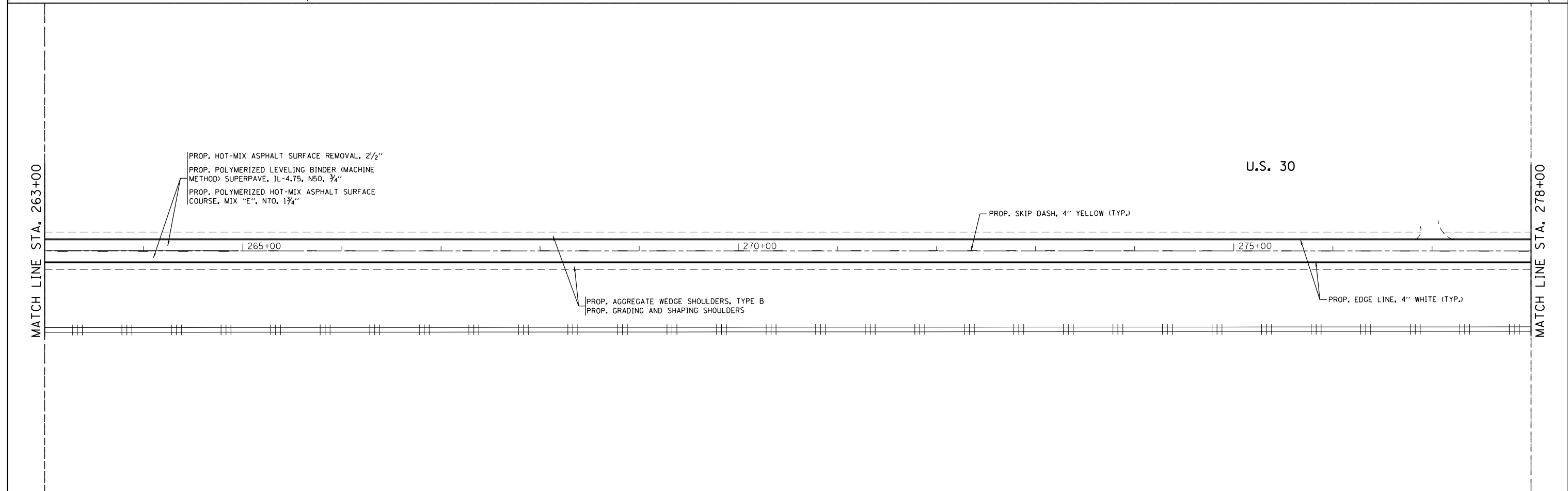
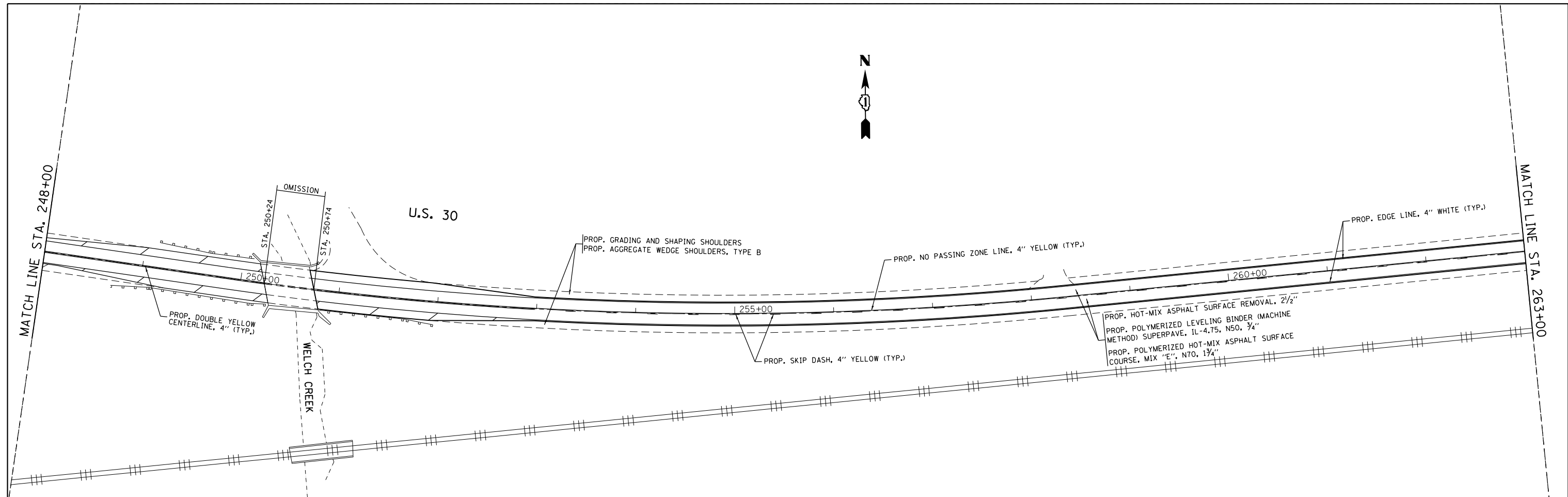
SCALE: 1" = 50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	16
CONTRACT NO. 62C37				
ILLINOIS FED. AID PROJECT				





FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI34416\Drawings\Design\DI34416-shr-plan.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			573	(27, 30, 61 & 117)RS-7	KANE	47	17
Default	PLOT DATE = 11/3/2016	DATE -	REVISED -			CONTRACT NO. 62C37				
						SCALE: 1" = 50'	SHEET OF SHEETS STA. TO STA.		ILLINOIS FED. AID PROJECT	



FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI34416\BROWNS\Meta\Design\DI34416-sh-t-plan.dgn		REVISION	REVISED -			573	(27, 30, 61 & 117)RS-7	KANE	47	18	
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 62C37								
Default	DATE -	REVISED -	ILLINOIS FED. AID PROJECT								



MATCH LINE STA. 278+00

MATCH LINE STA. 293+00

U.S. 30

PROP. GRADING AND SHAPING SHOULDERS  
PROP. AGGREGATE WEDGE SHOULDERS, TYPE B

PROP. EDGE LINE, 4" WHITE (TYP.)

280+00

285+00

290+00

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"  
PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50, 3/4"  
PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"

PROP. SKIP DASH, 4" YELLOW (TYP.)

MATCH LINE STA. 293+00

MATCH LINE STA. 308+00

U.S. 30

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"  
PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50, 3/4"  
PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"

PROP. SKIP DASH, 4" YELLOW (TYP.)

295+00

300+00

305+00

PROP. GRADING AND SHAPING SHOULDERS  
PROP. AGGREGATE WEDGE SHOULDERS, TYPE B

PROP. EDGE LINE, 4" WHITE (TYP.)

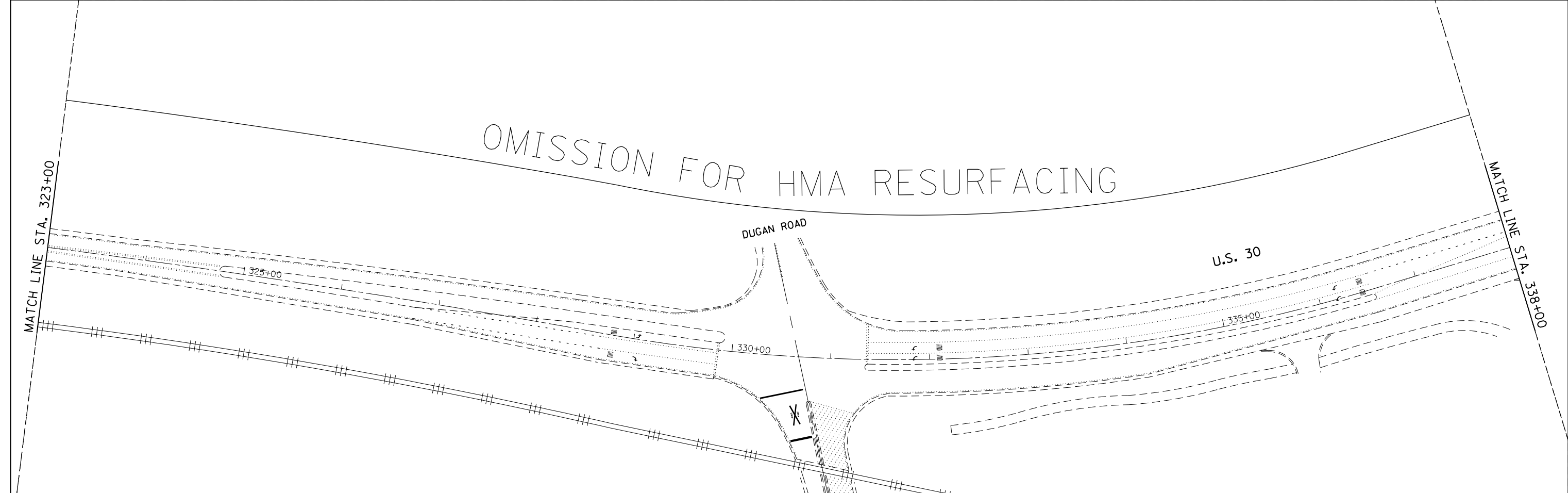
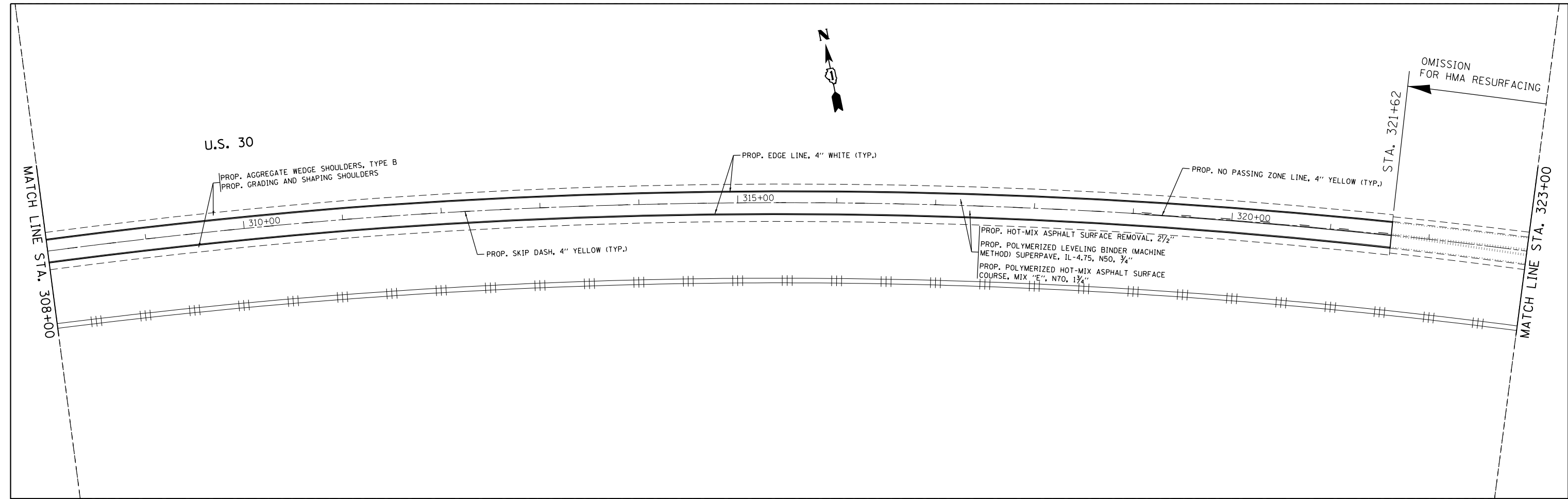
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 11/3/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88**

SCALE: 1" = 50'      SHEET      OF      SHEETS      STA.      TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	19
CONTRACT NO. 62C37				
ILLINOIS FED. AID PROJECT				

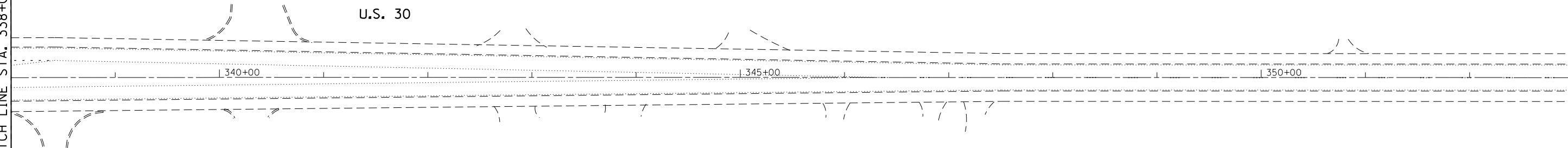


FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI3446\Drawings\Design\DI34416-shr-pln.dgn		CHECKED -	REVISED -		<b>FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88</b>			573	(27, 30, 61 & 117)RS-7	KANE	47	20	
Default		DATE -	REVISED -		SCALE: 1" = 50'	SHEET	OF	SHEETS	STA.	TO	STA.	<b>CONTRACT NO. 62C37</b>	
												ILLINOIS FED. AID PROJECT	

# OMISSION FOR HMA RESURFACING



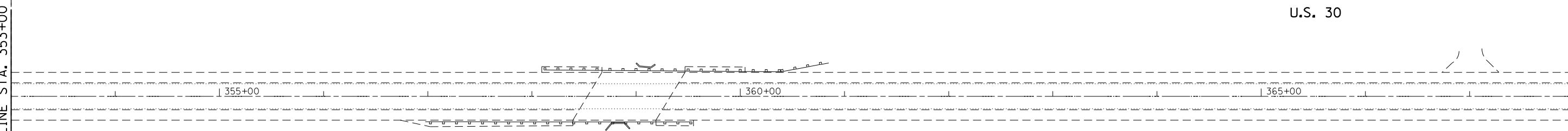
MATCH LINE STA. 338+00



MATCH LINE STA. 353+00

# OMISSION FOR HMA RESURFACING

MATCH LINE STA. 353+00



MATCH LINE STA. 368+00

FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI3446\Drawings\Design\DI34416-shr-plan.dgn		CHECKED -	REVISED -
Default	PLOT DATE = 11/3/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**  
**FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88**

SCALE: 1"= 50'    SHEET    OF    SHEETS    STA.    TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	21
CONTRACT NO. 62C37				
ILLINOIS FED. AID PROJECT				

# OMISSION FOR HMA RESURFACING



MATCH LINE STA. 368+00

U.S. 30

| 370+00 | 375+00 | 380+00

MATCH LINE STA. 383+00

# OMISSION FOR HMA RESURFACING

MATCH LINE STA. 383+00

VETERAN'S MEMORIAL PKWY.

U.S. 30

| 385+00 | 390+00 | 395+00

INDIGO DRIVE

MATCH LINE STA. 398+00

FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI3446\Drawings\Design\DI34416-sht-pln.dgn		REVISED -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 11/3/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88**

SCALE: 1" = 50'      SHEET      OF      SHEETS      STA.      TO      STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	22
CONTRACT NO. 62C37				
ILLINOIS FED. AID PROJECT				

# OMISSION FOR HMA RESURFACING



U.S. 30

MATCH LINE STA. 398+00

MATCH LINE STA. 413+00

1400+00

1405+00

1410+00

# OMISSION FOR HMA RESURFACING

MUNICIPAL DRIVE

STA. 419+19

LIMIT OF HMA RESURFACING

U.S. 30

MATCH LINE STA. 413+00

MATCH LINE STA. 428+00

1415+00

1420+00

1425+00

PROP. SKIP DASH, 4" WHITE (TYP.)

PROP. STOP BAR, 24" WHITE (TYP.)

PROP. MEDIAN EDGE LINE, 4" YELLOW (TYP.)

PROP. TURN LANE LINE, 6" WHITE (TYP.)

PROP. DOTTED DASHED LINE, 6" WHITE (TYP.)

FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -
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Default	PLOT DATE = 11/3/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**  
**FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88**

SCALE: 1" = 50'      SHEET      OF      SHEETS      STA.      TO      STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	23
CONTRACT NO. 62C37				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 428+00

U.S. 30

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50, 3/4"  
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"

PROP. SKIP DASH, 4" WHITE (TYP.)

PROP. EDGE LINE, 4" WHITE (TYP.)

PROP. DOTTED DASHED LINE, 6" WHITE (TYP.)

PROP. MEDIAN EDGE LINE, 4" YELLOW (TYP.)

PROP. GRADING AND SHAPING SHOULDERS  
 PROP. AGGREGATE WEDGE SHOULDERS, TYPE B

MATCH LINE STA. 443+00

MATCH LINE STA. 443+00

U.S. 30

OMISSION FOR HMA RESURFACING

PROP. MEDIAN EDGE LINE, 4" YELLOW (TYP.)

PROP. CHEVRONS, 12" WHITE (TYP.)

PROP. EDGE LINE, 4" WHITE (TYP.)

STA. 456+54

445+00

450+00

455+00

PROP. SKIP DASH, 4" WHITE (TYP.)

PROP. GRADING AND SHAPING SHOULDERS  
 PROP. AGGREGATE WEDGE SHOULDERS, TYPE B

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50, 3/4"  
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"

US ROUTE 30/ IL ROUTE 47

MATCH LINE STA. 458+00

FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -
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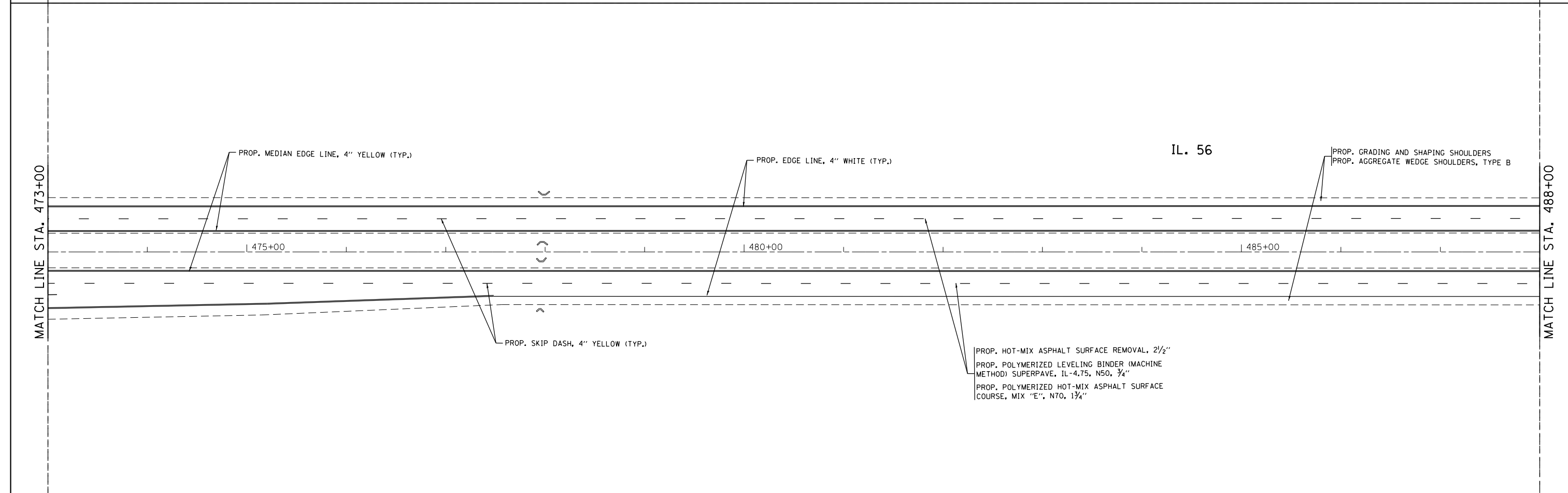
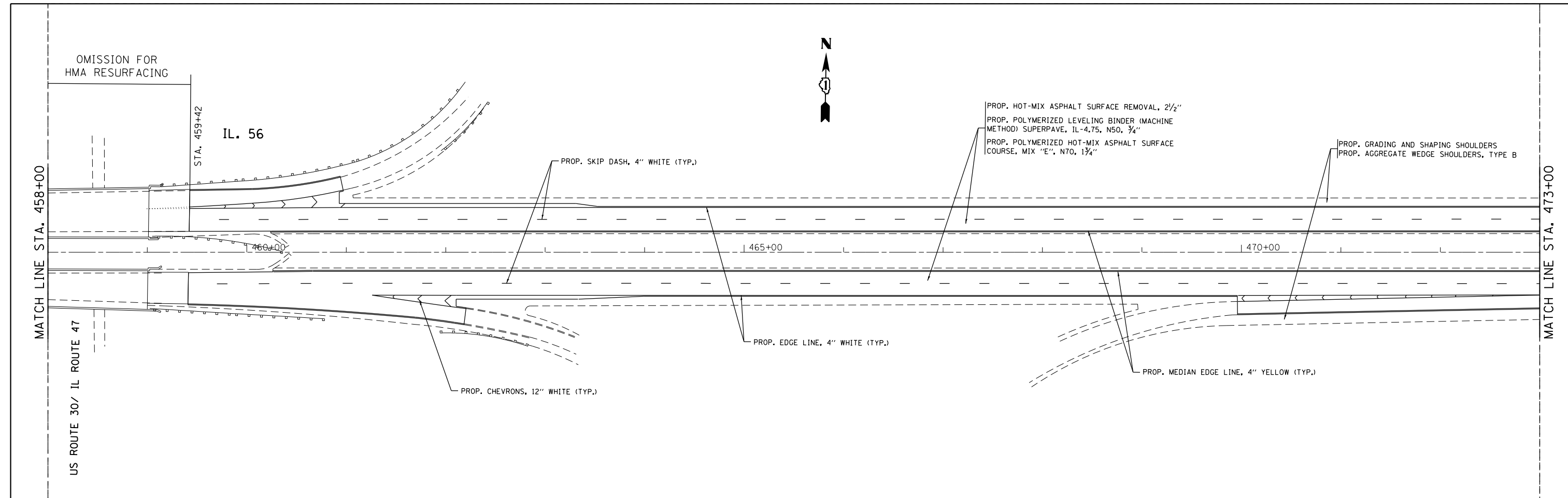
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**  
**FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88**

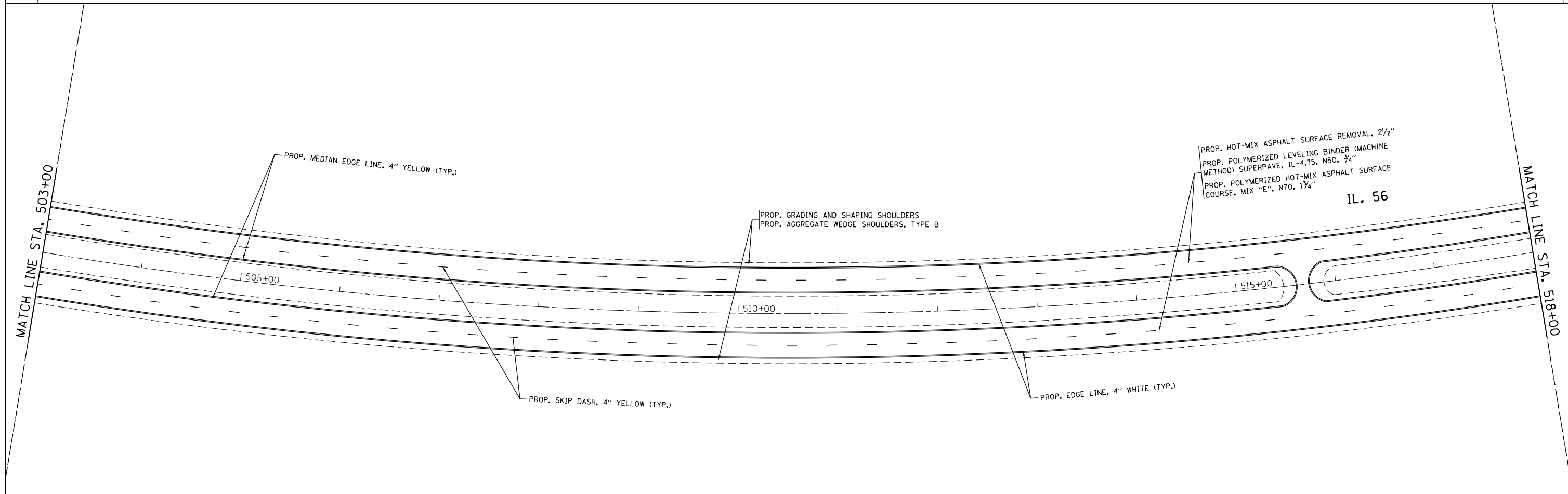
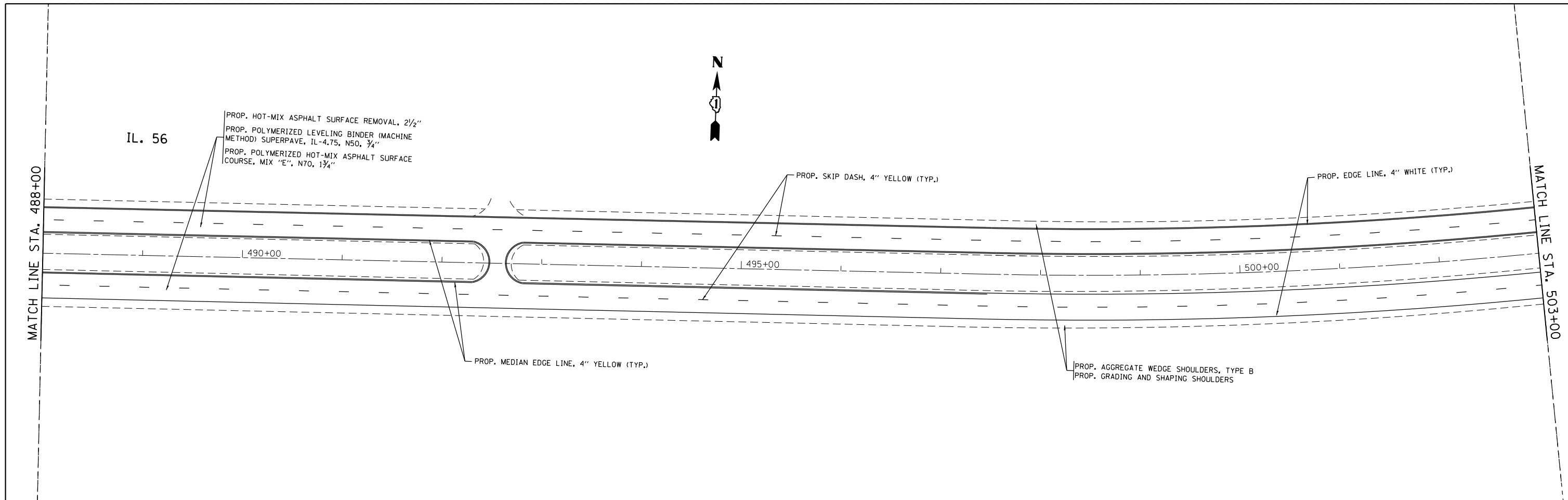
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	24
CONTRACT NO. 62C37				
ILLINOIS FED. AID PROJECT				

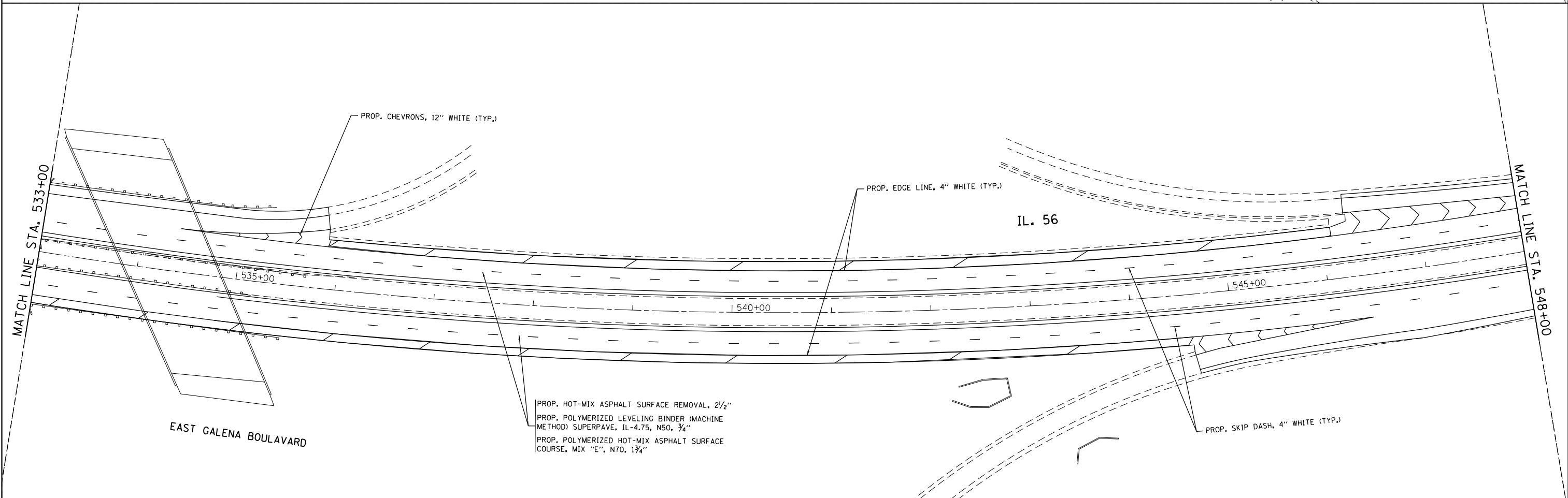
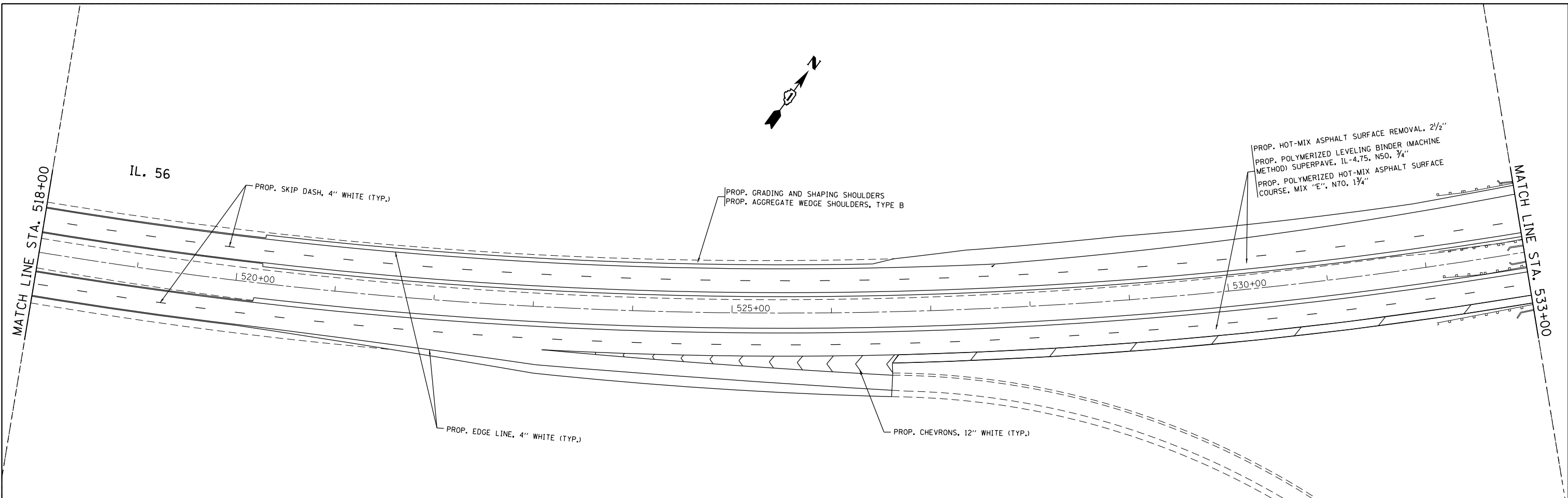
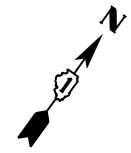




FILE NAME = p:\IL084EBIDINTEG\Illinois.gov\PI\DOT\Documents\IDOT Offices\District 1\Projects\DI34416\Drawings\Design\DI34416-shr-plan.dgn	USER NAME = STEEDPA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN FAP 573 (US 30 /IL 56) — DE KALB COUNTY LINE TO I-88</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -					573	(27, 30, 61 & 117)RS-7	KANE	47	25
Default	PLOT DATE = 11/3/2016	DATE -	REVISED -	SCALE: 1" = 50'	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT CONTRACT NO. 62C37	



FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI34416\Drawings\Design\DI34416-shr-pln.dgn		CHECKED -	REVISED -			573	(27, 30, 61 & 117)RS-7	KANE	47	26
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -			CONTRACT NO. 62C37				
Default						ILLINOIS FED. AID PROJECT				



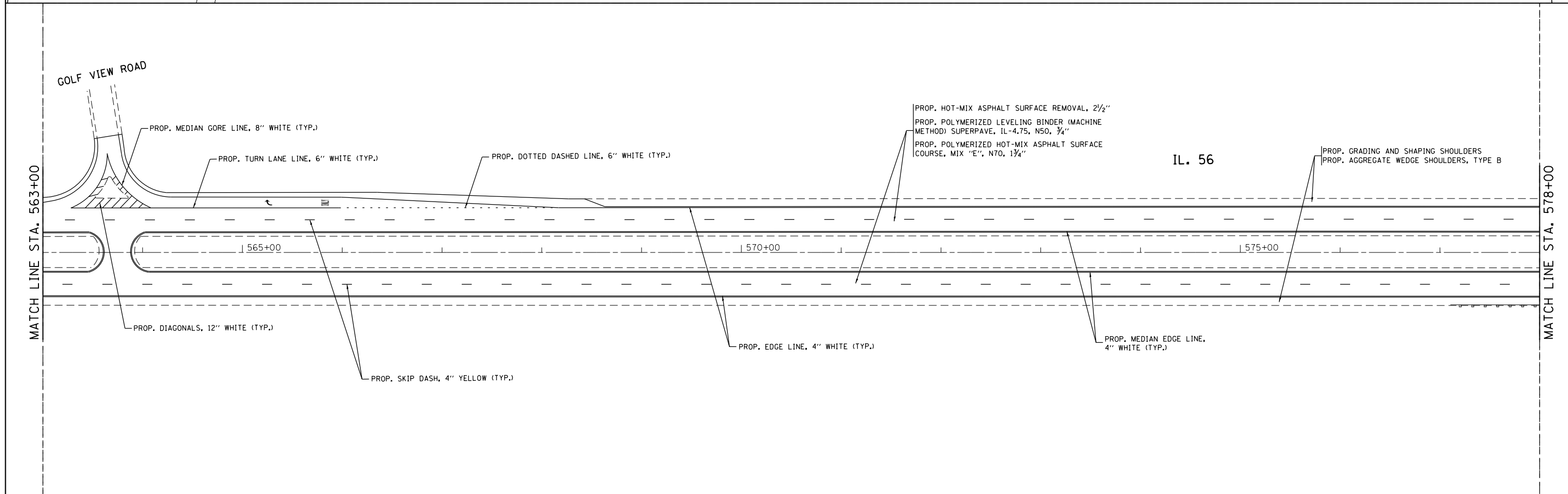
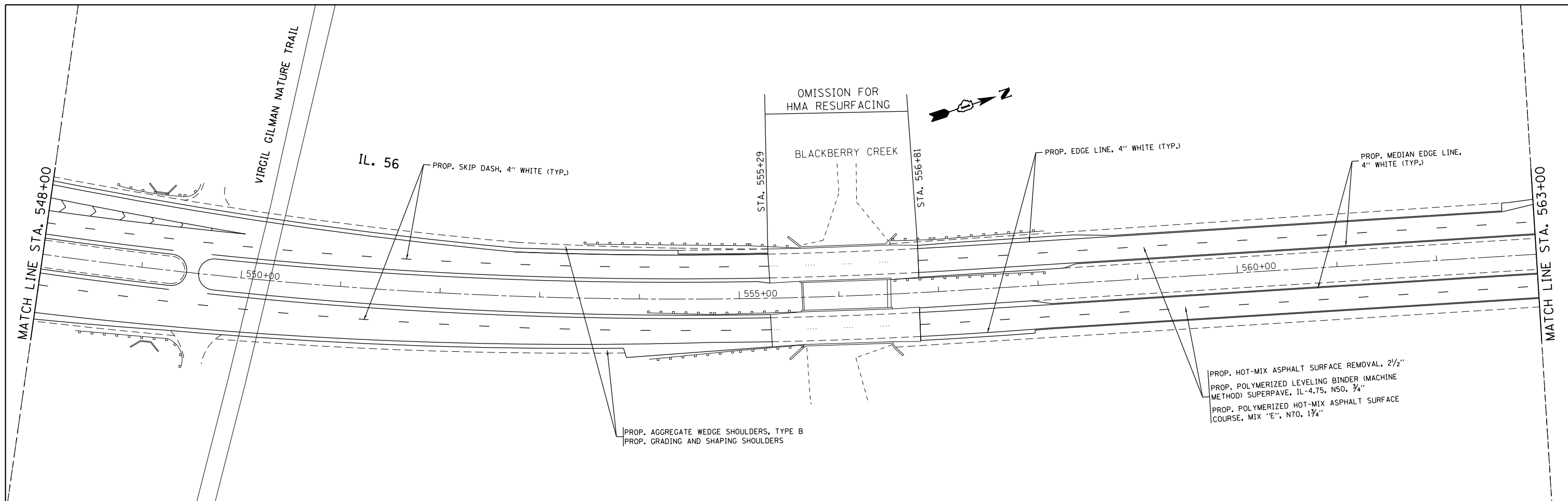
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 11/3/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**  
**FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88**

SCALE: 1" = 50'      SHEET      OF      SHEETS      STA.      TO      STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	27
CONTRACT NO. 62C37				
ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 11/3/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**  
**FAP 573 (US 30 /IL 56—DE KALB COUNTY LINE TO I-88)**

SCALE: 1" = 50'      SHEET      OF      SHEETS      STA.      TO      STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	28
CONTRACT NO. 62C37				
ILLINOIS FED. AID PROJECT				



HANKES ROAD

IL. 56

MATCH LINE STA. 578+00

MATCH LINE STA. 593+00

PROP. EDGE LINE, 4" WHITE (TYP.)

PROP. MEDIAN EDGE LINE, 4" WHITE (TYP.)

PROP. SKIP DASH, 4" WHITE (TYP.)

580+00

585+00

590+00

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50, 3/4"  
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"

PROP. AGGREGATE WEDGE SHOULDERS, TYPE B  
 PROP. GRADING AND SHAPING SHOULDERS

MATCH LINE STA. 593+00

MATCH LINE STA. 608+00

IL. 56

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-4.75, N50, 3/4"  
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"

PROP. GRADING AND SHAPING SHOULDERS  
 PROP. AGGREGATE WEDGE SHOULDERS, TYPE B

PROP. SKIP DASH, 4" WHITE (TYP.)

595+00

600+00

605+00

PROP. EDGE LINE, 4" WHITE (TYP.)

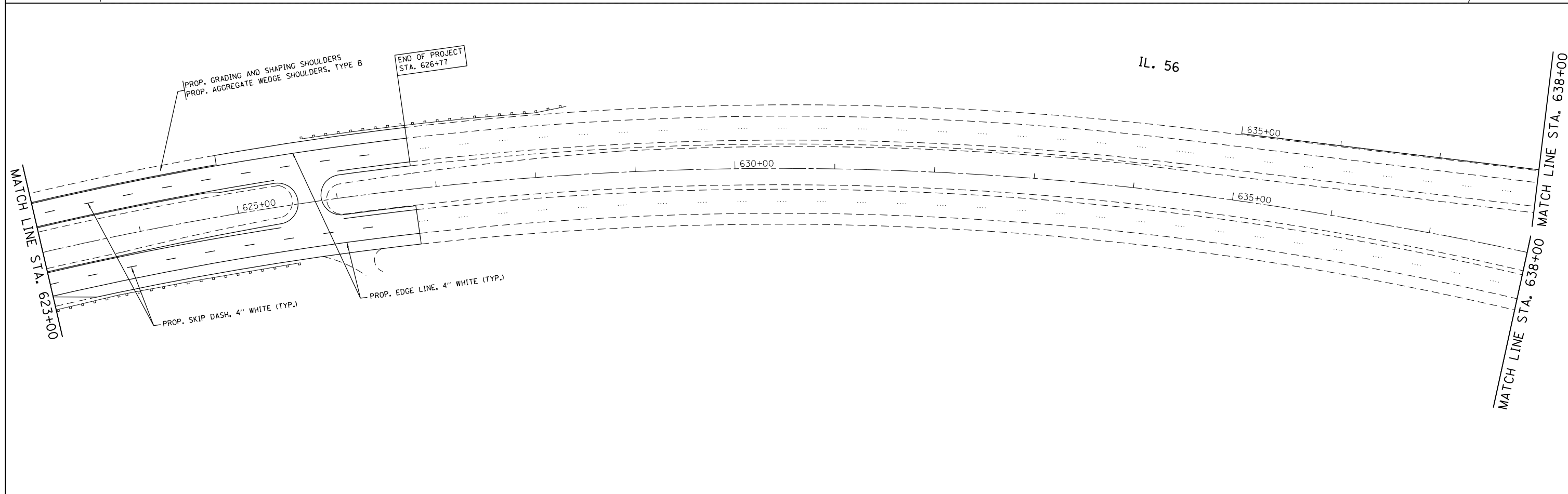
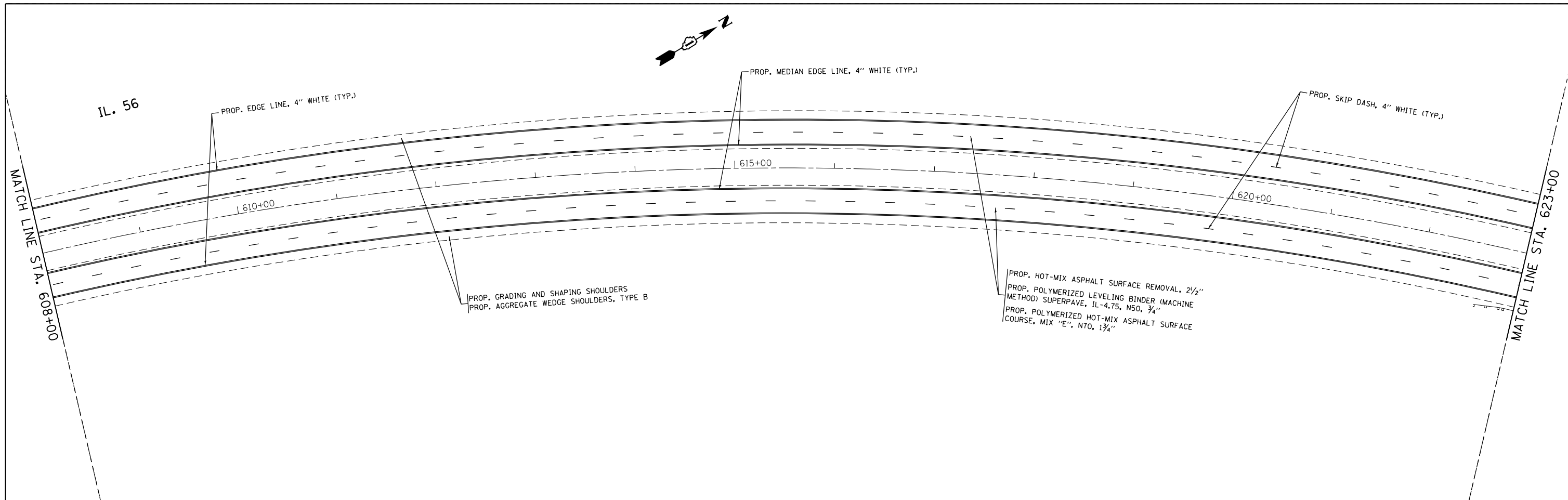
PROP. MEDIAN EDGE LINE, 4" WHITE (TYP.)

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Default	PLOT DATE = 11/3/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**  
**FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88**  
 SCALE: 1"= 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	29
CONTRACT NO. 62C37				
ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -
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Default	PLOT DATE = 11/3/2016	CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**  
**FAP 573 (US 30 /IL 56)—DE KALB COUNTY LINE TO I-88**

SCALE: 1" = 50'      SHEET      OF      SHEETS      STA.      TO      STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	30
CONTRACT NO. 62C37				
ILLINOIS FED. AID PROJECT				

**BENCHMARK:**

RAILROAD SPIKE IN THE SOUTH SIDE OF THE SECOND POWER POLE, WEST OF RHODES STREET AND SOUTH OF US ROUTE 30  
ELEV=709.90 NAVD 88



POINT	ELEV	NOTE
A	709.19	MATCH
B	709.25	MATCH
C	709.25	TW
D	709.30	TW
E	709.36	TW=TC
F	709.44	TW=TC
G	709.38	MATCH
H	709.49	MATCH
I	709.23	MATCH
J	709.32	MATCH
K	709.27	TW=TC
L	709.32	TW=TC
M	708.95	TW
N	709.03	TW
O	708.91	MATCH
P	708.99	MATCH

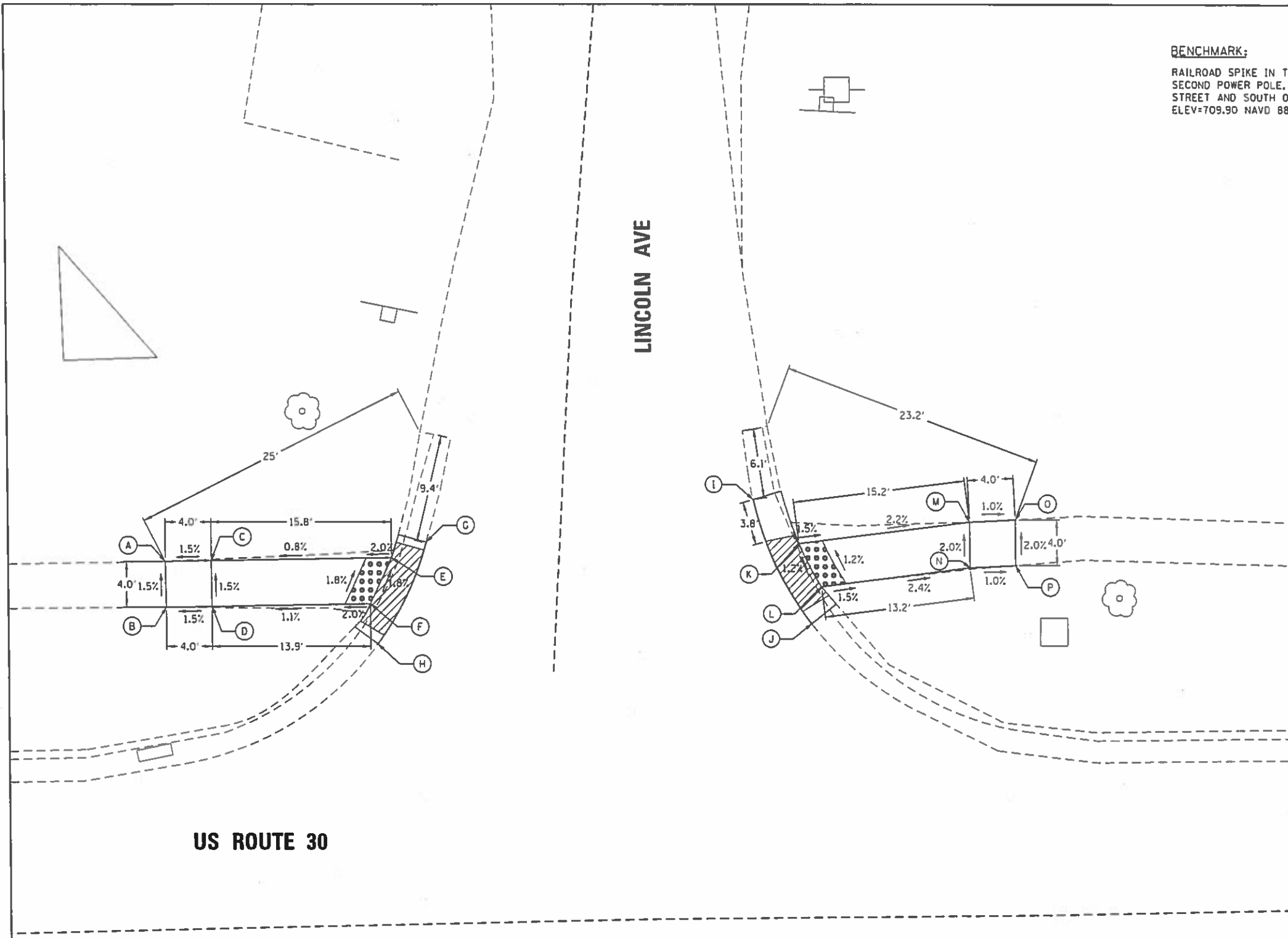
**LEGEND**

- (A) POINT
- DETECTABLE WARNINGS
- CURB DEPRESSION

- TW PROPOSED TOP-OF-WALK ELEVATION
- TC PROPOSED TOP-OF-CURB ELEVATION
- EOP PROPOSED EDGE-OF-PAVEMENT ELEVATION
- MATCH MATCH EXISTING ELEVATION

LINCOLN AVE

US ROUTE 30



COMPANY NAME: HRGreen  
PROJECT CONTACT: [unreadable]  
DATE PLOTTED: 10/25/2016 10:24 AM  
FILE NAME: 62C37-011-01-01.dwg  
PLOT DRIVER: [unreadable]

**HRGreen**  
HRGreen.com  
Illinois Professional Design Firm  
# 104-001322

USER NAME	ibhar tme	DESIGNED	BH	REVISED	-
FILE NAME	0162C37-011-01-01.dwg	CHECKED	DMS	REVISED	-
PLOT SCALE	1" = 10'	CHECKED	-	REVISED	-
PLOT DATE	10/25/2016	DATE	09/09/16	REVISED	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SIDEWALK RAMP DETAILS  
US ROUTE 30  
SCALE: 1" = 10' SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	31
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 62C37	

**BENCHMARK:**

RAILROAD SPIKE IN THE SOUTH SIDE OF THE SECOND POWER POLE, WEST OF RHODES STREET AND SOUTH OF US ROUTE 30  
ELEV=709.90 NAVD 88

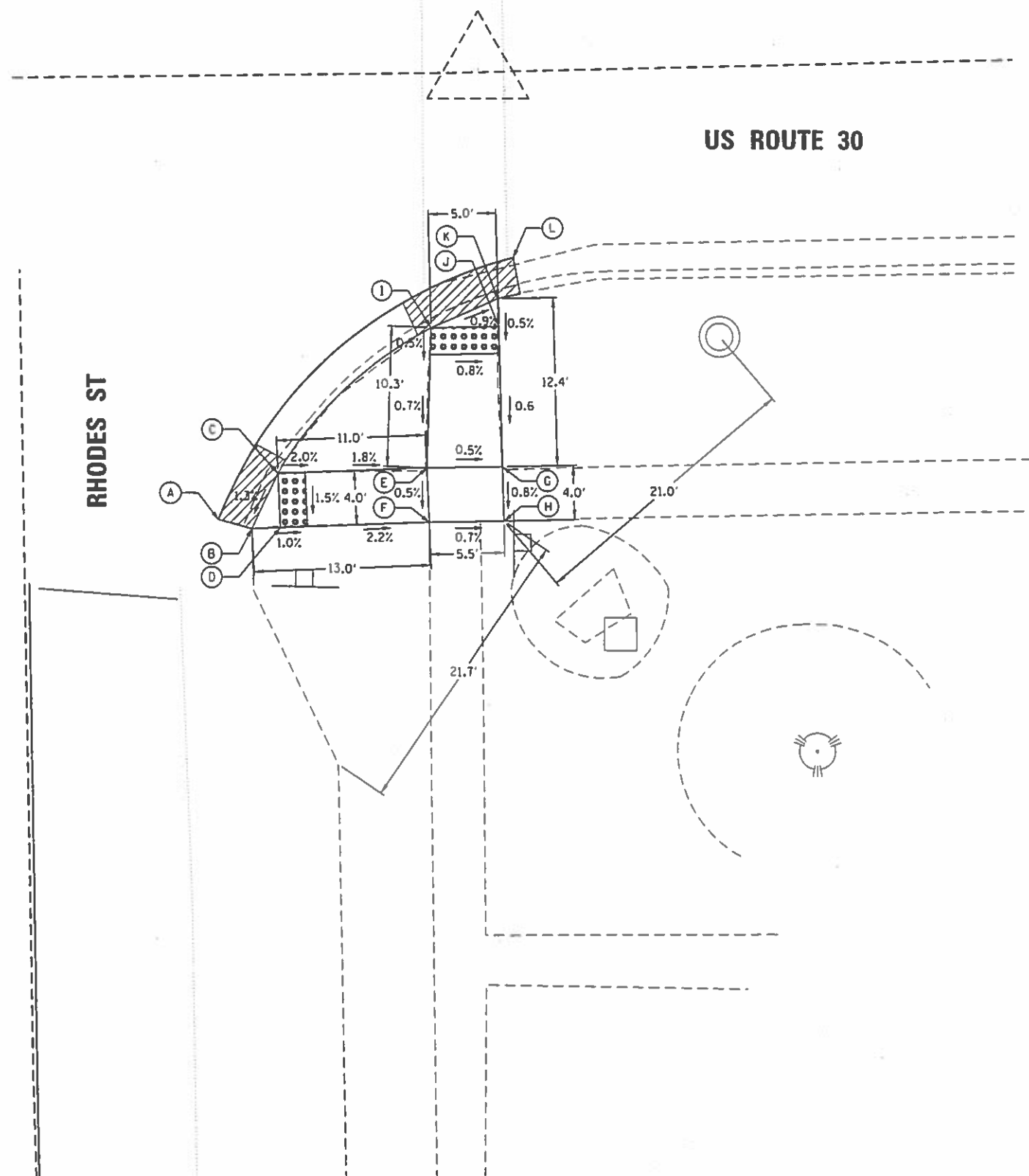


POINT	ELEV	NOTE
A	708.00	MATCH
B	708.00	TW=TC
C	708.06	TW=TC
D	707.98	TW
E	707.86	TW
F	707.84	MATCH
G	707.83	MATCH
H	707.80	MATCH
I	707.95	TW=TC
J	707.89	TW
K	707.90	TW=TC
L	707.88	MATCH

**LEGEND**

- (A) POINT
- [Grid of dots] DETECTABLE WARNINGS
- [Hatched area] CURB DEPRESSION

- TW PROPOSED TOP-OF-WALK ELEVATION
- TC PROPOSED TOP-OF-CURB ELEVATION
- EOP PROPOSED EDGE-OF-PAVEMENT ELEVATION
- MATCH MATCH EXISTING ELEVATION



COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: [illegible]  
 DATE PLOTTED: 10/25/2016 7:38:29 AM  
 PLOT DRIVER: [illegible]

**HRGreen.com**  
 Illinois Professional Design Firm  
 # 194-001322

USER NAME	lhbar One	DESIGNED	BH	REVISED	-
FILE NAME	D162C37-sh1-ramp.s_B162C37	DRAWN	DMS	REVISED	-
PLOT SCALE	1" = 10'	CHECKED	-	REVISED	-
PLOT DATE	10/25/2016	DATE	09/09/16	REVISED	-

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>SIDEWALK RAMP DETAILS US ROUTE 30</b>			
SCALE: 1" = 10'	SHEET NO. OF SHEETS	STA. TO STA.	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	127, 30, 61 & 117RHS-7	KANE	47	32
CONTRACT NO. 62C37				
ILLINOIS FED. AID PROJECT				



**BENCHMARK:**

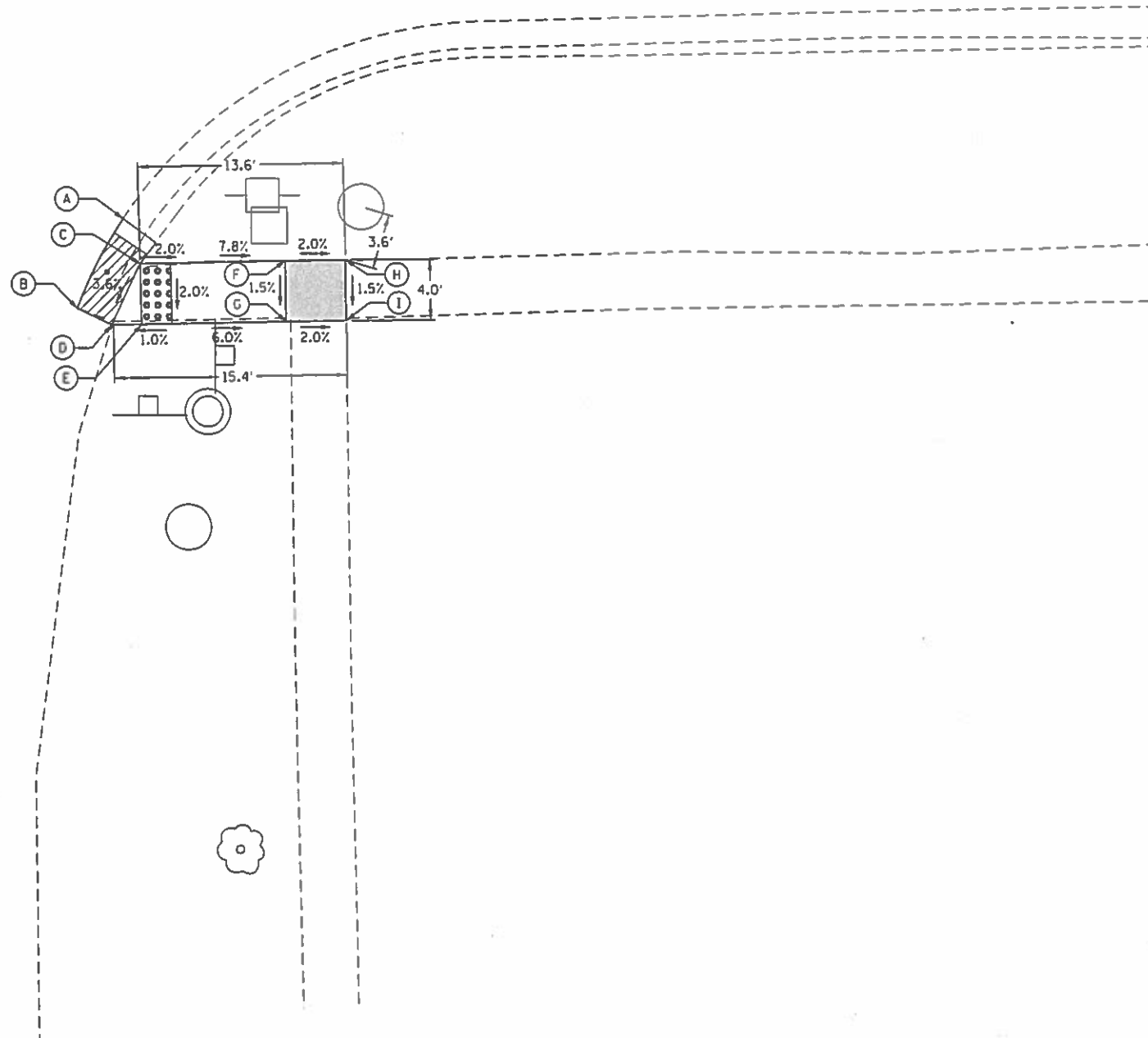
RAILROAD SPIKE IN THE SOUTH SIDE OF THE SECOND POWER POLE, WEST OF RHODES STREET AND SOUTH OF US ROUTE 30  
ELEV=709.90 NAVD 88

POINT	ELEV	NOTE
A	705.40	MATCH
B	705.25	MATCH
C	705.32	TW
D	705.16	MATCH
E	705.18	TW
F	704.69	TW
G	704.75	MATCH
H	704.61	MATCH
I	704.67	MATCH



**US ROUTE 30**

**JEFFERSON AVE**



**LEGEND**

- (A) POINT
- [Dotted pattern] DETECTABLE WARNINGS
- [Hatched pattern] CURB DEPRESSION
- TW PROPOSED TOP-OF-WALK ELEVATION
- TC PROPOSED TOP-OF-CURB ELEVATION
- EOP PROPOSED EDGE-OF-PAVEMENT ELEVATION
- MATCH MATCH EXISTING ELEVATION

COMPANY NAME: HRGreen.com  
PROJECT CONTACT: bher@hrgreen.com  
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PEN TABLE: plottbl.tbl

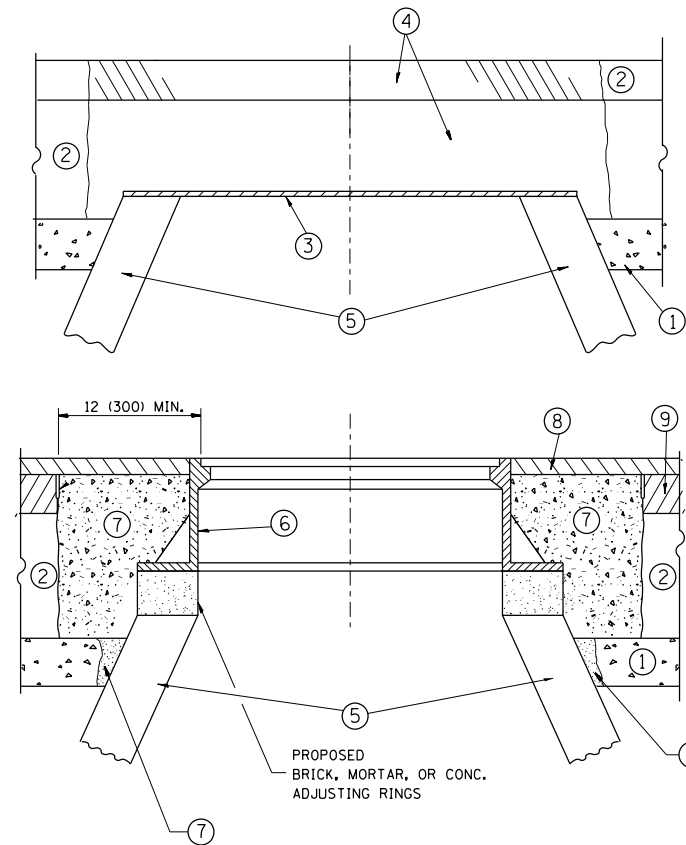


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PLOT DATE	10/25/2016	DATE	09/09/16	REVISED	

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SIDEWALK RAMP DETAILS US ROUTE 30			
SCALE: 1" = 10'	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	33
CONTRACT NO. 62C37				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ⑥ FRAME AND LID (SEE NOTES)
- ② EXISTING PAVEMENT
- ⑦ CLASS PP-1\* CONCRETE
- ③ 36 (900) DIAMETER METAL PLATE
- ⑧ PROPOSED HMA SURFACE COURSE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑨ PROPOSED HMA BINDER COURSE
- ⑤ EXISTING STRUCTURE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

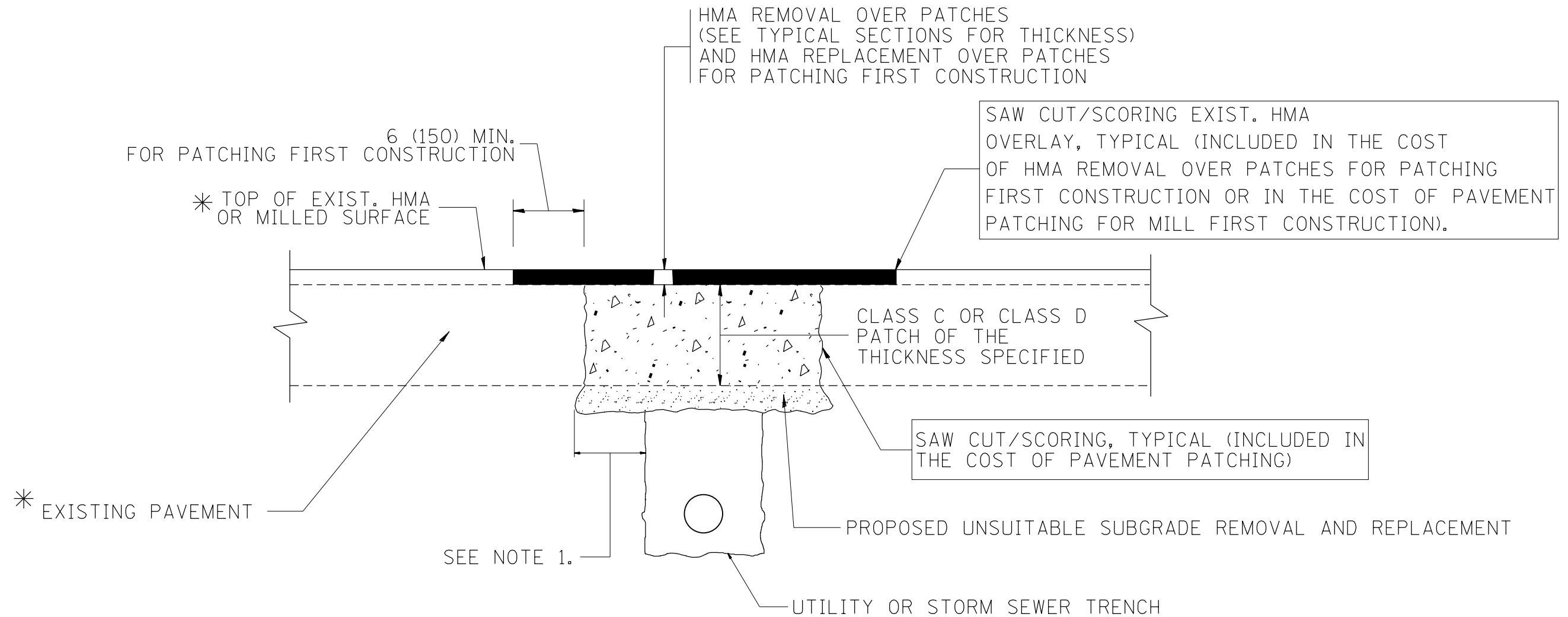
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = STEEDPA	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI344\BROWNS\Meta\Design\Diststd.dgn			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 10/26/2016	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	34
<b>BD600-03 (BD-8)</b>		<b>CONTRACT NO. 62C37</b>		
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = STEEDPA	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\1344\BROWNS\Design\Diststd.dgn		CHECKED -	REVISED - R. BORO 01-01-07		573	(27, 30, 61 & 117)RS-7	KANE	47	35			
PLOT SCALE = 100.0000' / 1in.		DATE - 10-25-94	REVISED - R. BORO 09-04-07		<b>BD400-04 (BD-22)</b>		<b>CONTRACT NO. 62C37</b>					
PLOT DATE = 10/26/2016			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT			

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001  
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

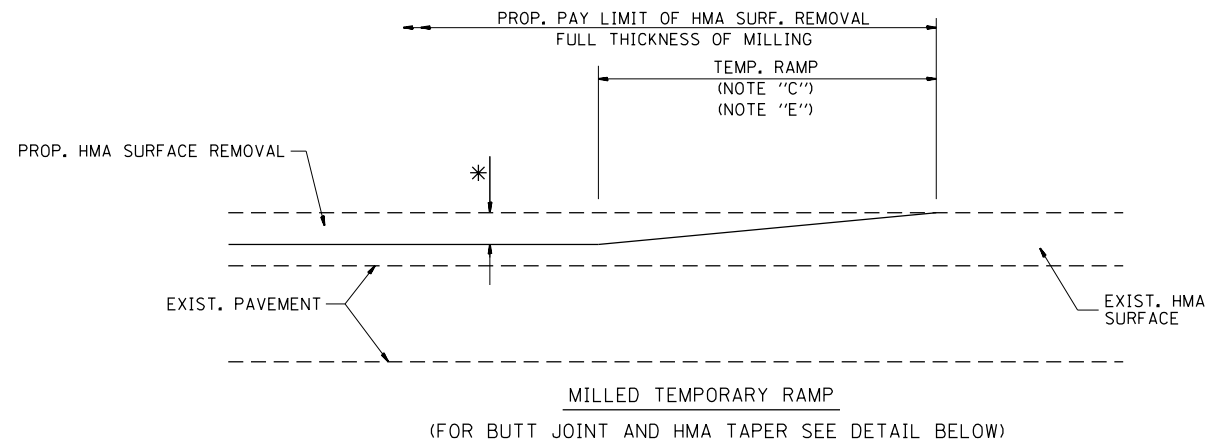
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

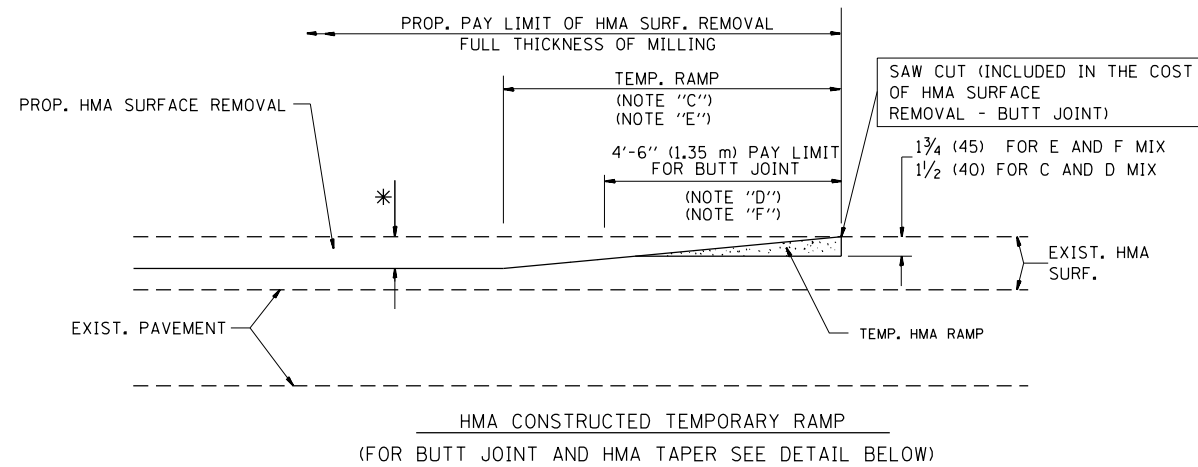
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = STEEDPA	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI344\BROWNS\Design\Diststd.dgn		REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01			573	(27, 30, 61 & 117)RS-7	KANE	47	36
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 01-22-01	REVISED - R. BORO 12-15-09			<b>BD600-06 (BD-24)</b>		<b>CONTRACT NO. 62C37</b>		
PLOT DATE = 10/26/2016	DATE - 03-11-94					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT

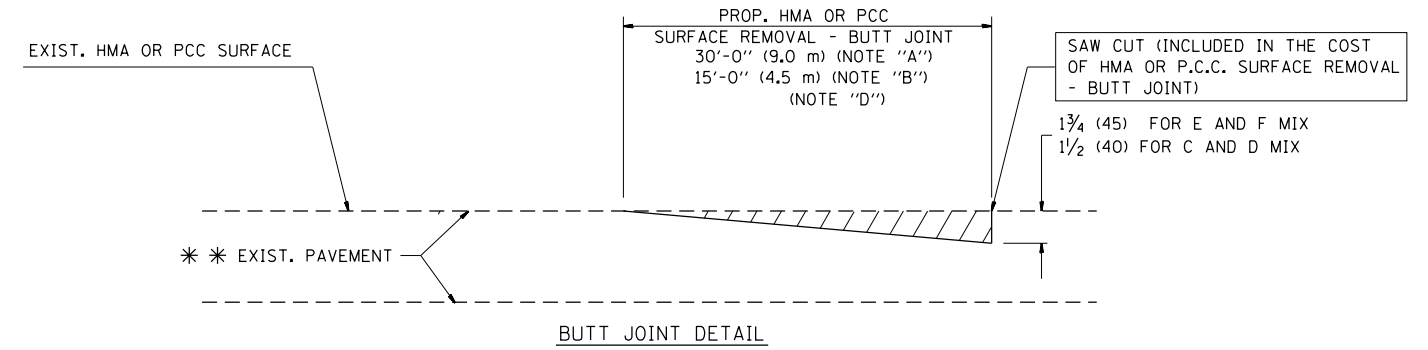


**OPTION 1**

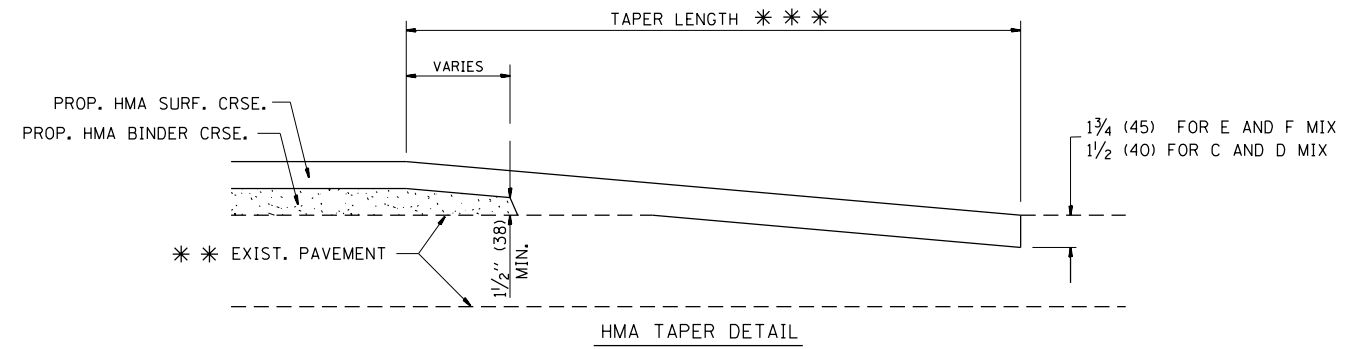


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\* \* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

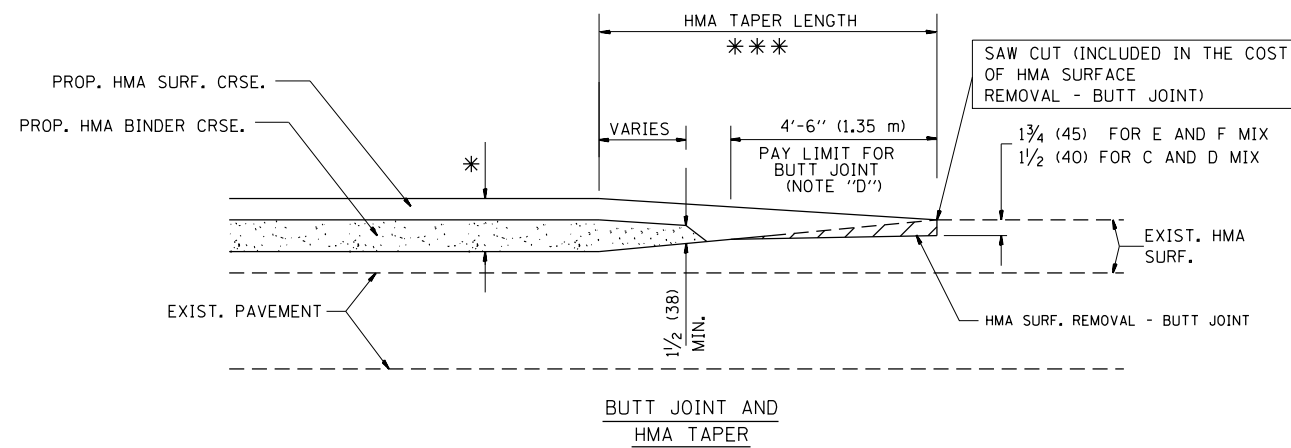
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

\* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

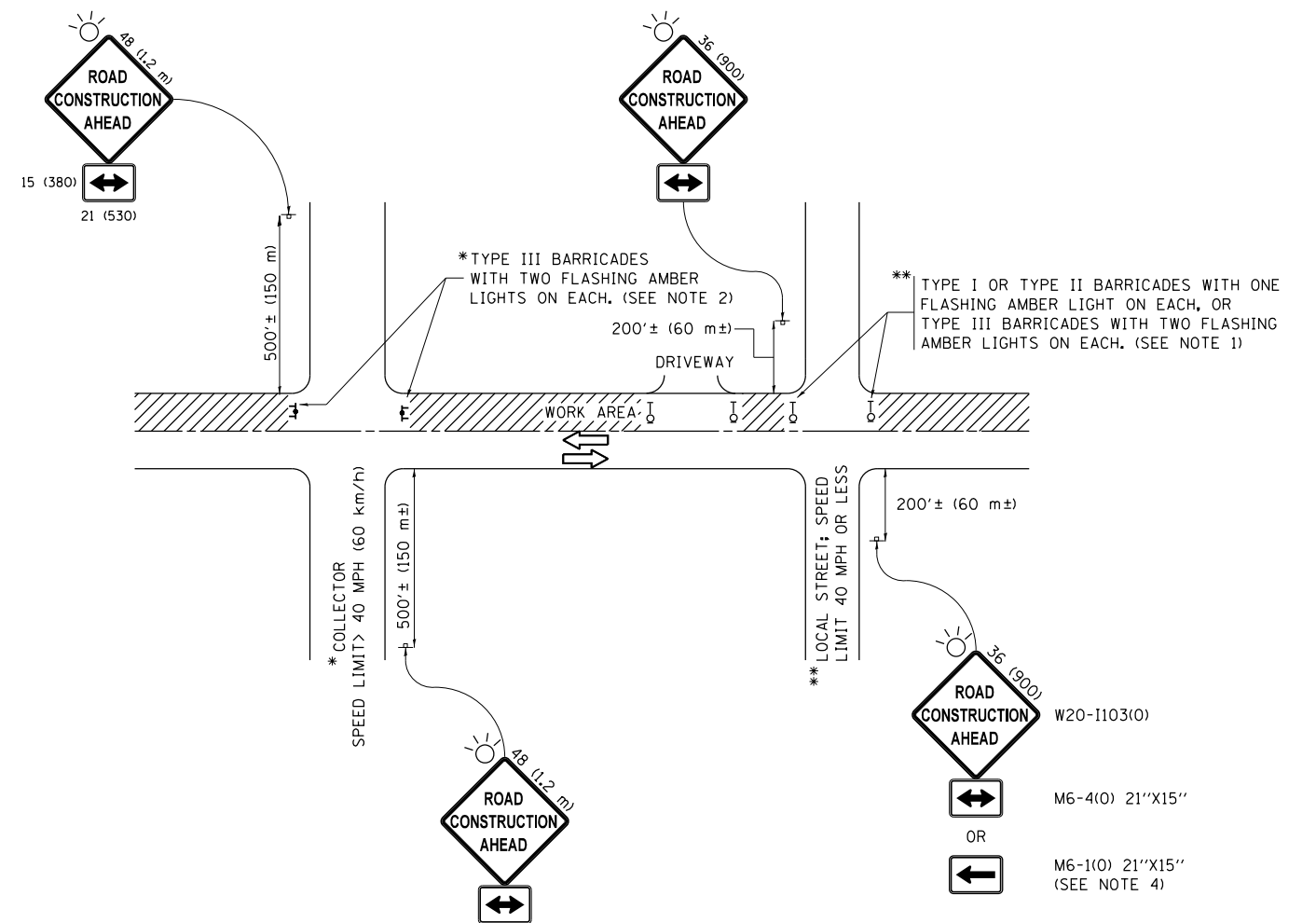
FILE NAME =	USER NAME = STEEDPA	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI344\Drawings\Design\Diststd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 10/26/2016	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	37
<b>BD400-05 BD32</b>		<b>CONTRACT NO. 62C37</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

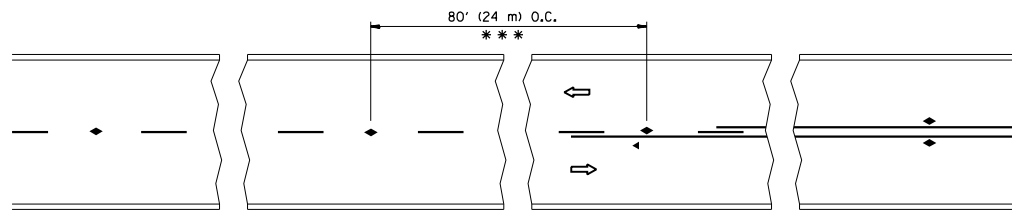
FILE NAME =	USER NAME = STEEDPA	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
pw:\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\1344\Drawings\Design\Diststd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 10/26/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

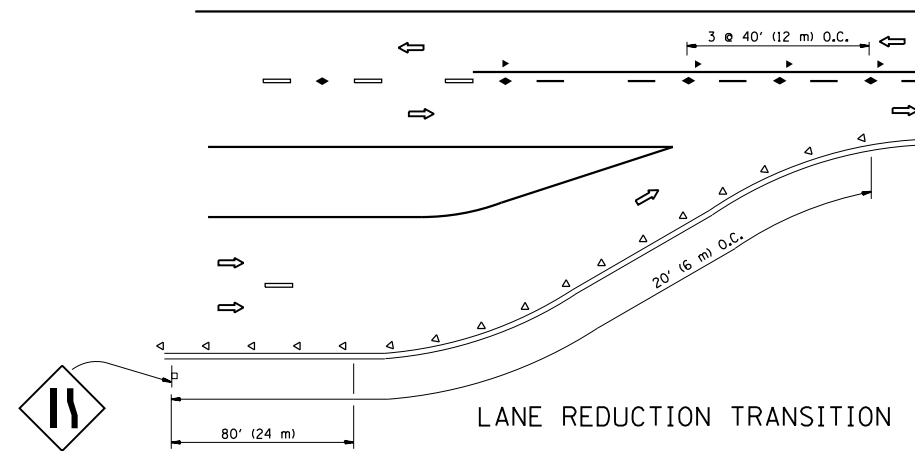
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	38
<b>TC-10</b>			<b>CONTRACT NO. 62C37</b>	
ILLINOIS FED. AID PROJECT				

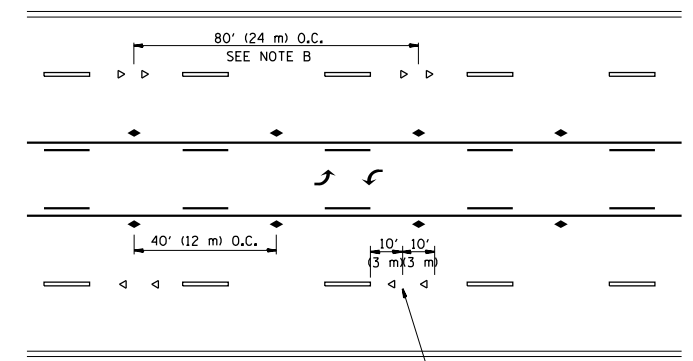


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

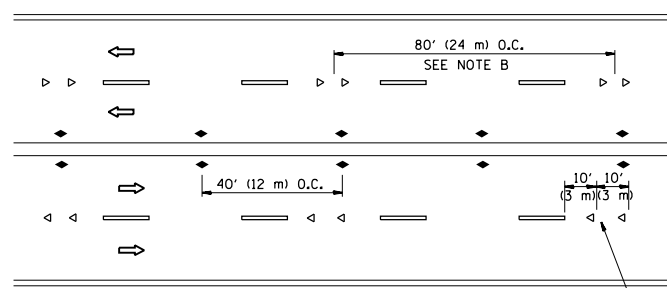
TWO-LANE/TWO-WAY



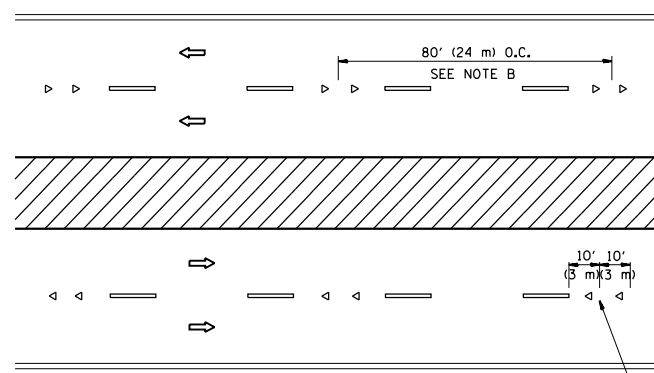
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

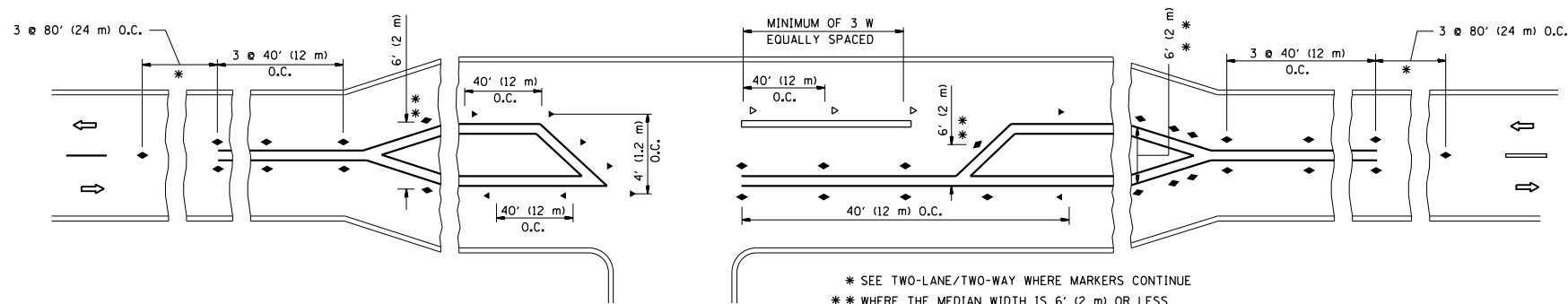
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

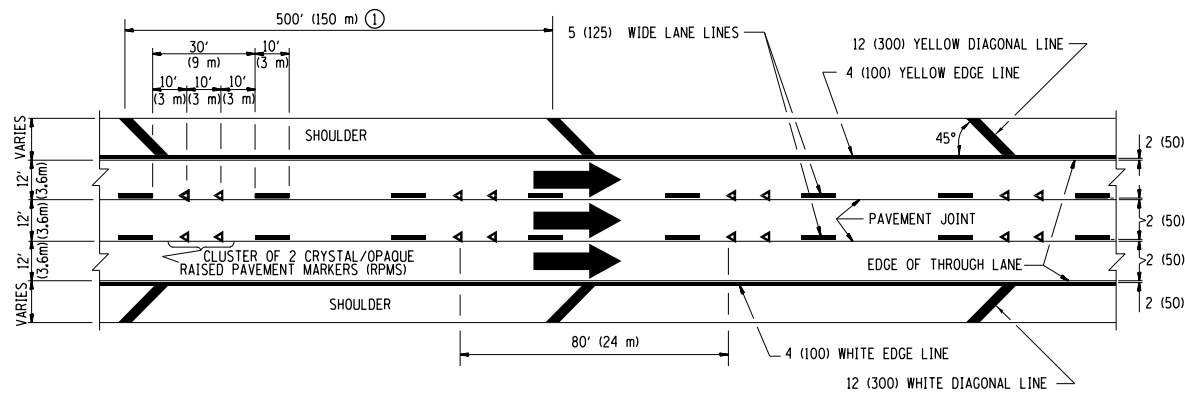
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
pw:\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI344\BROWN\Meta\Design\Diststd.dgn		REVISED - T. RAMMACHER 03-12-99	REVISED - T. RAMMACHER 01-06-00
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - C. JUCIUS 09-09-09
	PLOT DATE = 10/26/2016	DATE -	

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

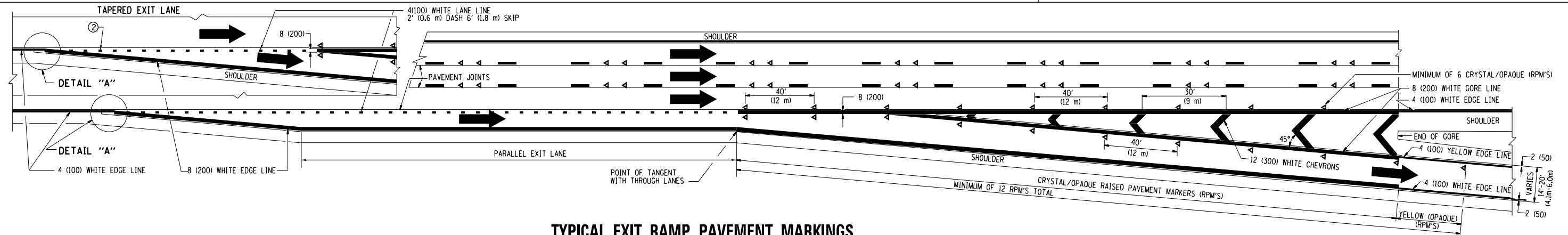
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	39
TC-11			CONTRACT NO. 62C37	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



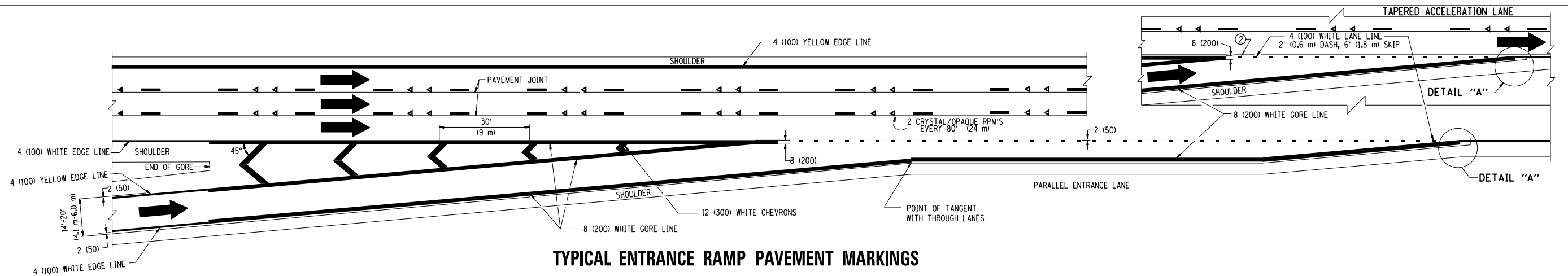
**TYPICAL EDGE LINES & LANE LINES**

**PAVEMENT MARKING MATERIALS**

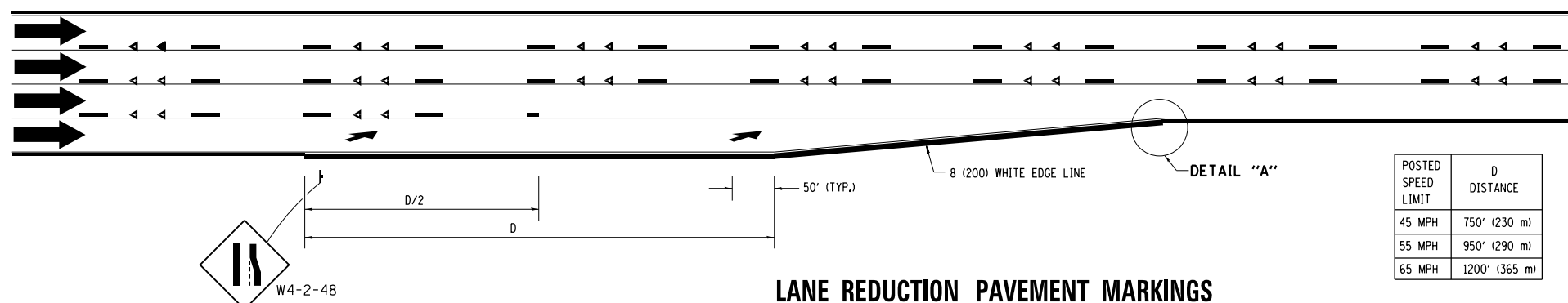
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE; INLAID OR GROOVED IN SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENT PROJECTS.
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC PROJECTS.



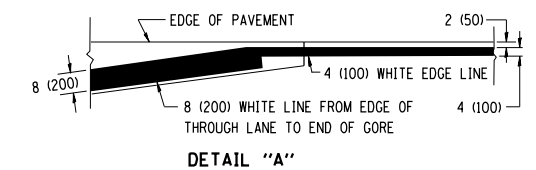
**TYPICAL EXIT RAMP PAVEMENT MARKINGS**



**TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS**



**LANE REDUCTION PAVEMENT MARKINGS**



**NOTES:**

- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
- ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)

FILE NAME =  
 USER NAME = STEEDPA  
 DESIGNED - D.W.S.  
 REVISED - J.A.F. 02-06  
 REVISED - S.P.B. 01-07  
 PLOT SCALE = 100.0000' / 1in.  
 CHECKED -  
 REVISED - S.P.B. 01-10  
 PLOT DATE = 10/26/2016  
 DATE - 01-90  
 REVISED - M.D. 05-13

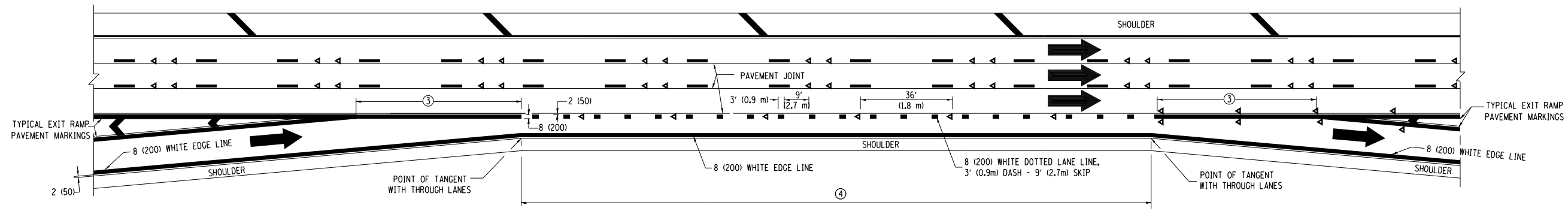
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**MULTI-LANE FREEWAY  
 PAVEMENT MARKING DETAILS**

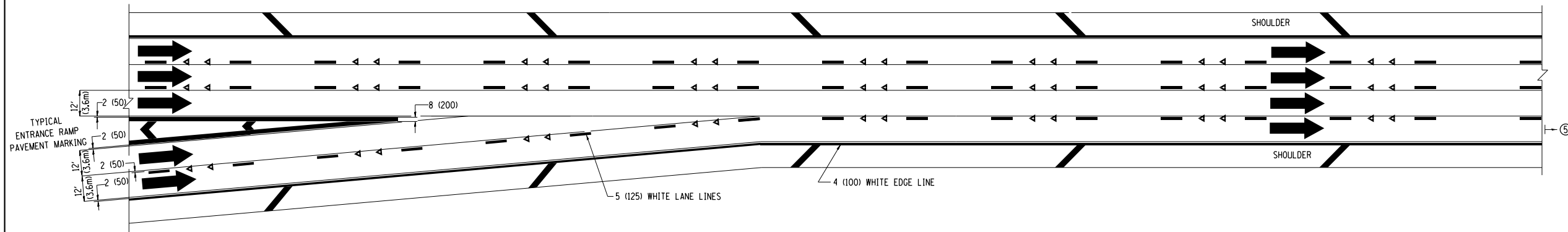
SCALE: NONE SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	40
<b>TC-12</b>			<b>CONTRACT NO. 62C37</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

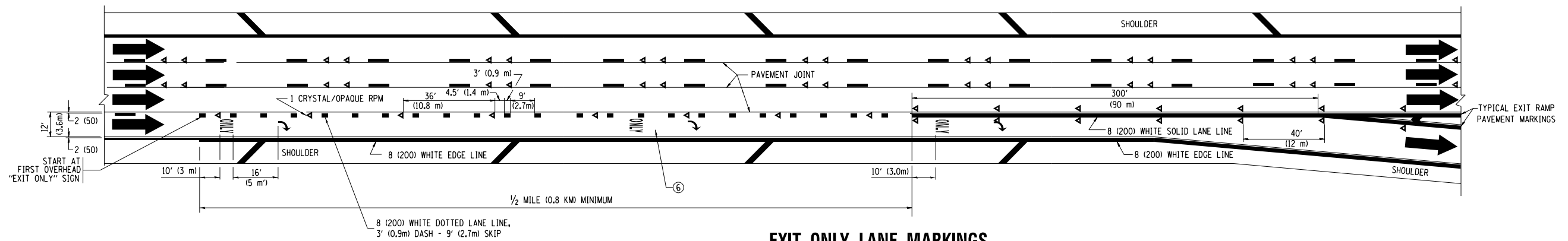




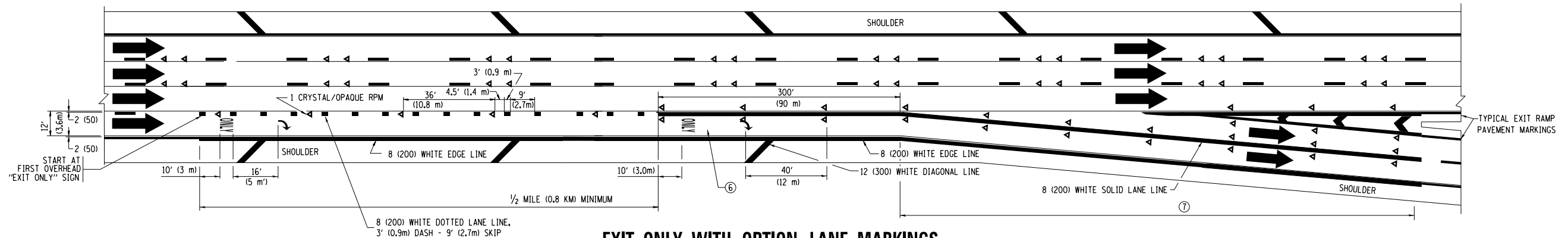
**AUXILIARY LANE MARKINGS**



**TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS**



**EXIT ONLY LANE MARKINGS**



**EXIT ONLY WITH OPTION LANE MARKINGS**

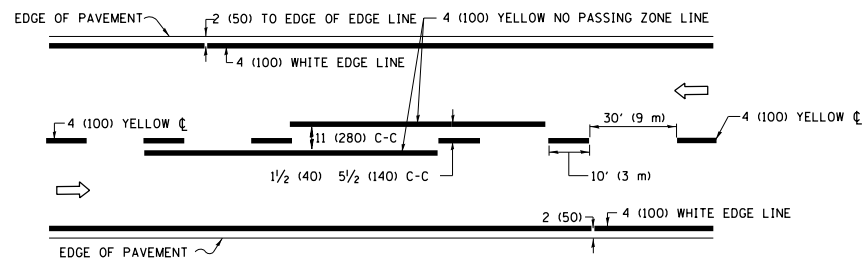
- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
  - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
  - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
  - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
  - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

FILE NAME =	USER NAME = STEEDPA	DESIGNED - D.W.S.	REVISED - D.W.S. 07-96
p:\11\084EBIDINTEG\11\inois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI344\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - J.A.F. 02-06
		DATE - 01-90	REVISED - S.P.B. 01-07
			REVISED - S.P.B. 01-10

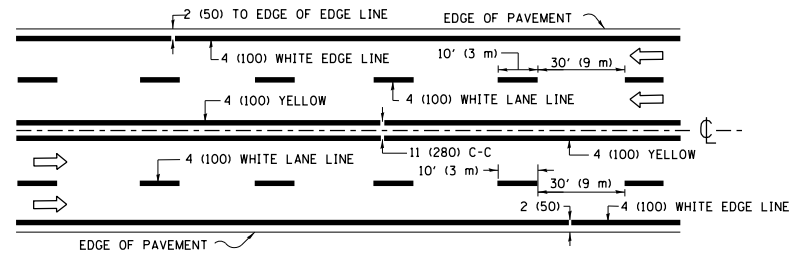
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS</b>			
SCALE: NONE	SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.

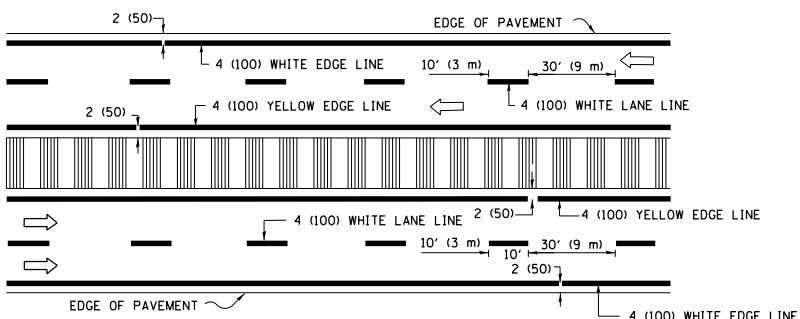
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	41
<b>TC-12</b>		<b>CONTRACT NO. 62C37</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**2-LANE ROADWAY**

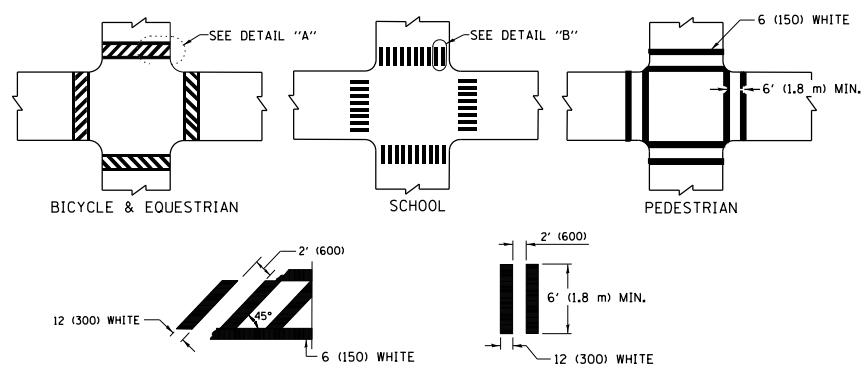


**MULTI-LANE UNDIVIDED**



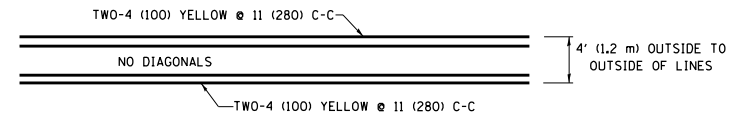
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

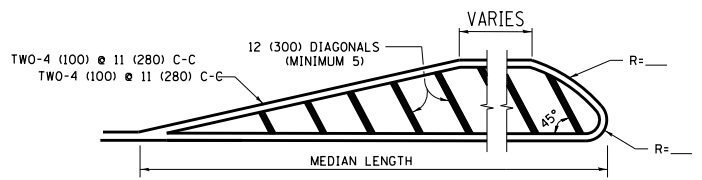


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

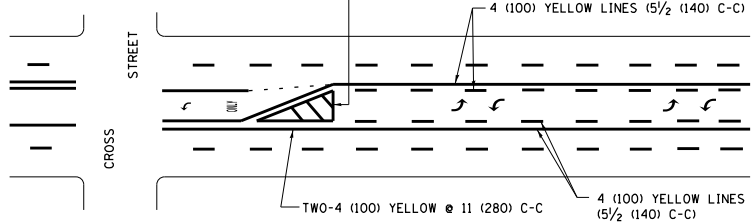


**4' (1.2 m) WIDE MEDIANS ONLY**

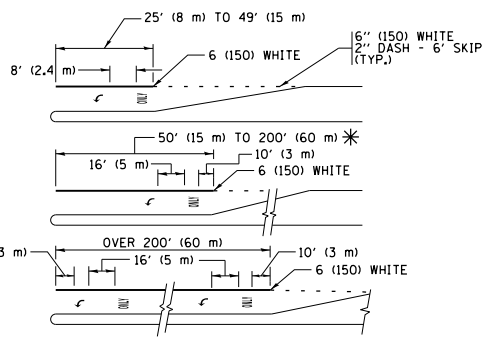


**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

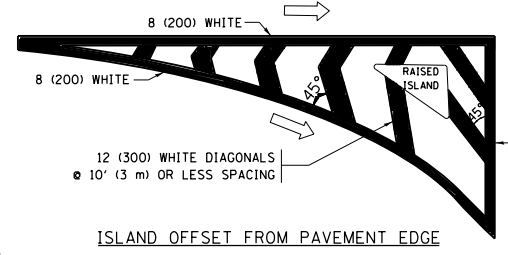


**MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING**

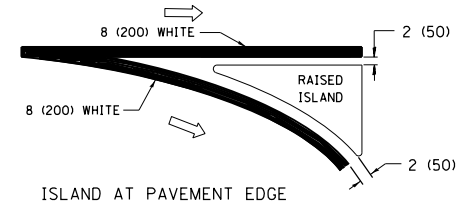


**TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING**

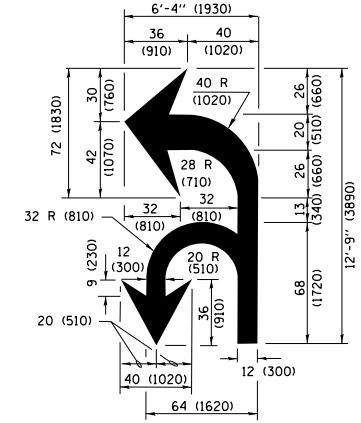
FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



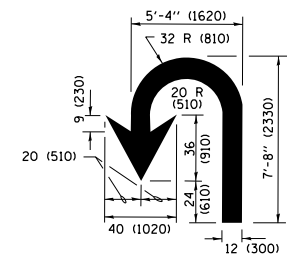
**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = STEEDPA	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01344\BROWN\Meta\Design\Diststd.dgn			REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 10/26/2016	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

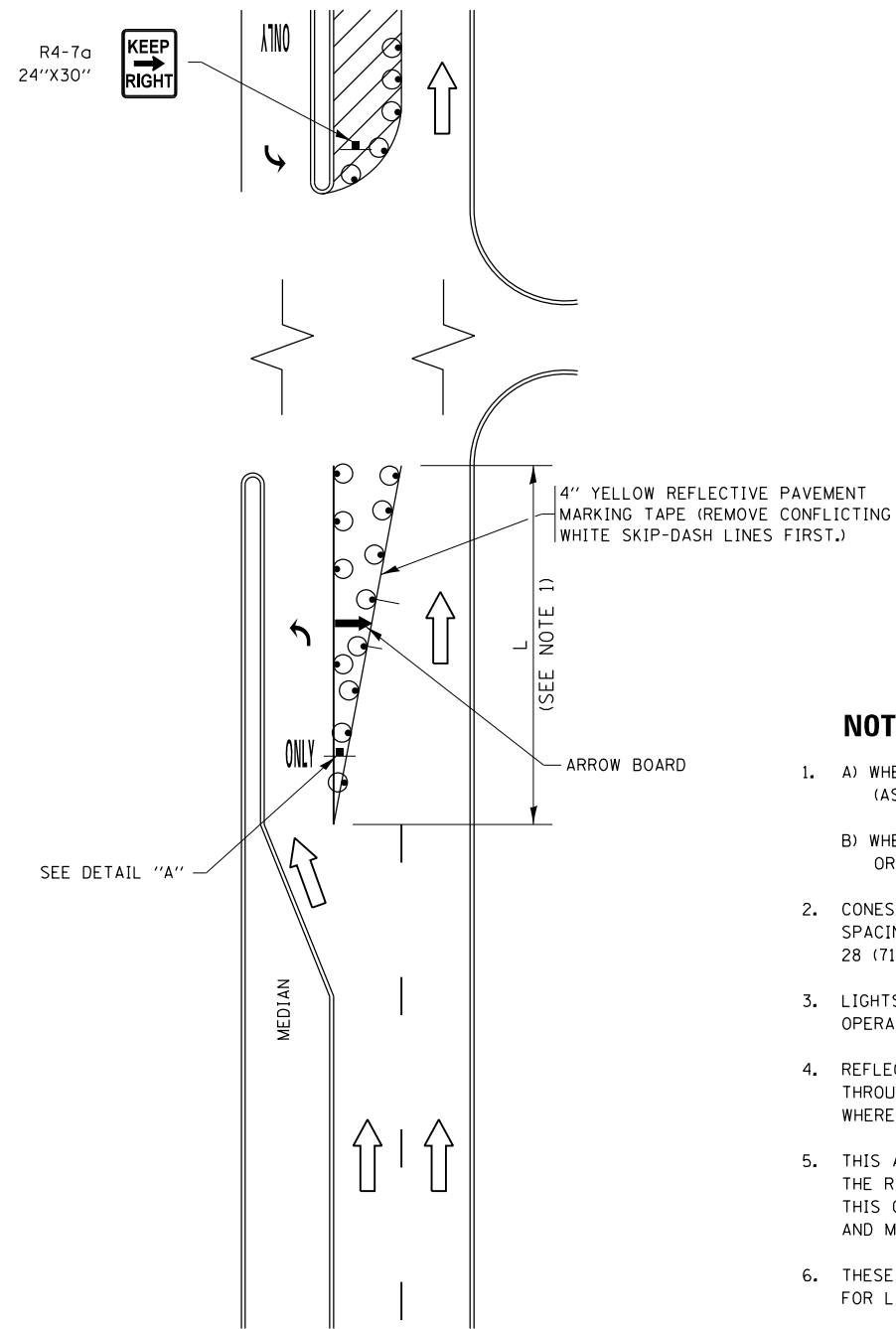
**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

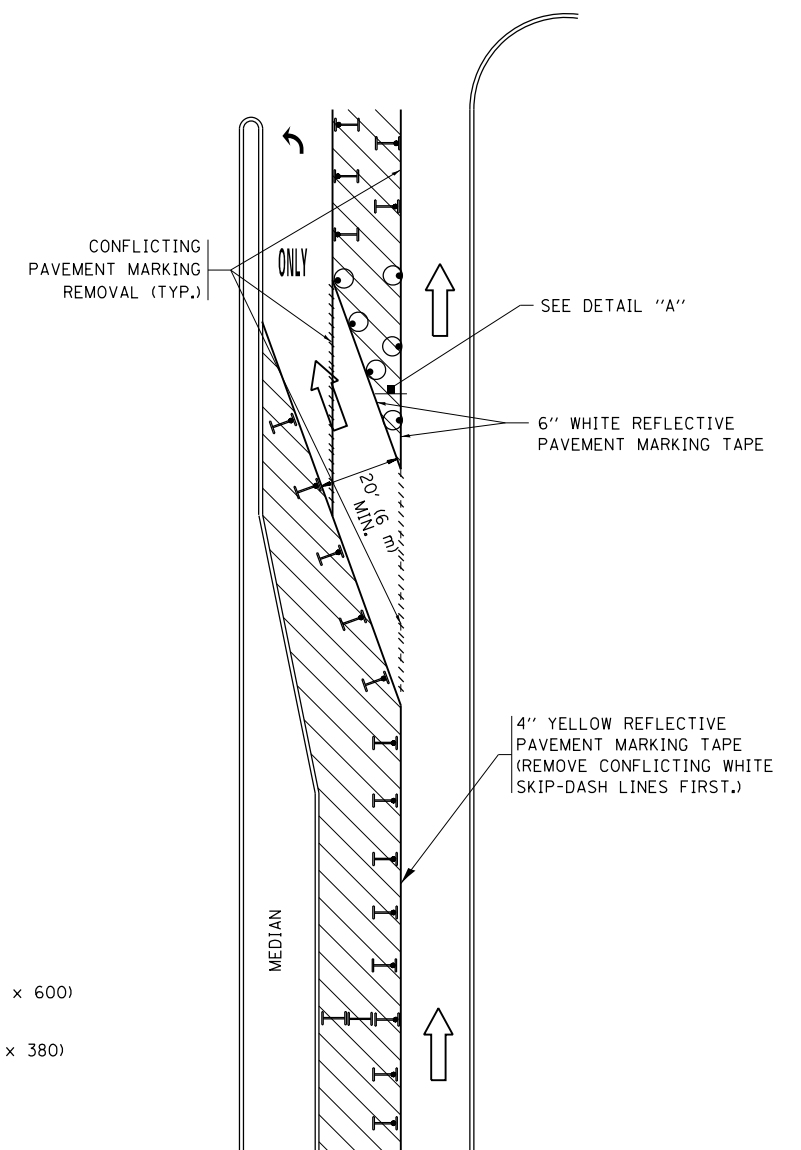
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	42
<b>TC-13</b>		<b>CONTRACT NO. 62C37</b>		
ILLINOIS FED. AID PROJECT				

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



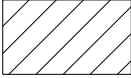






**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



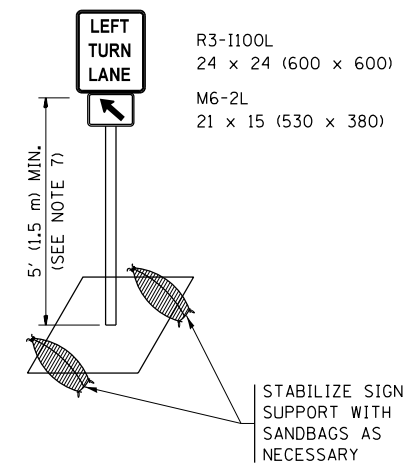
**FIGURE 2**

## LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  ARROW BOARD
-  TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  SIGN ASSEMBLY
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

## NOTES:

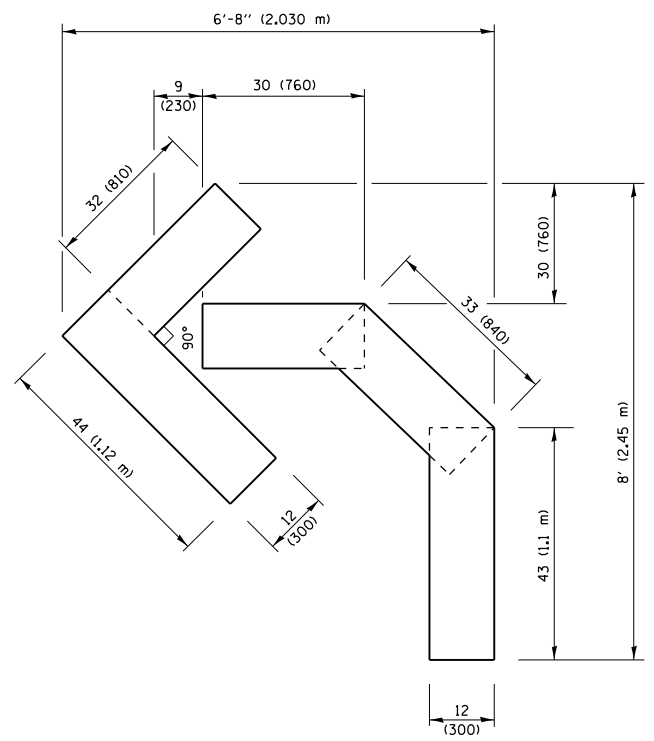
1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



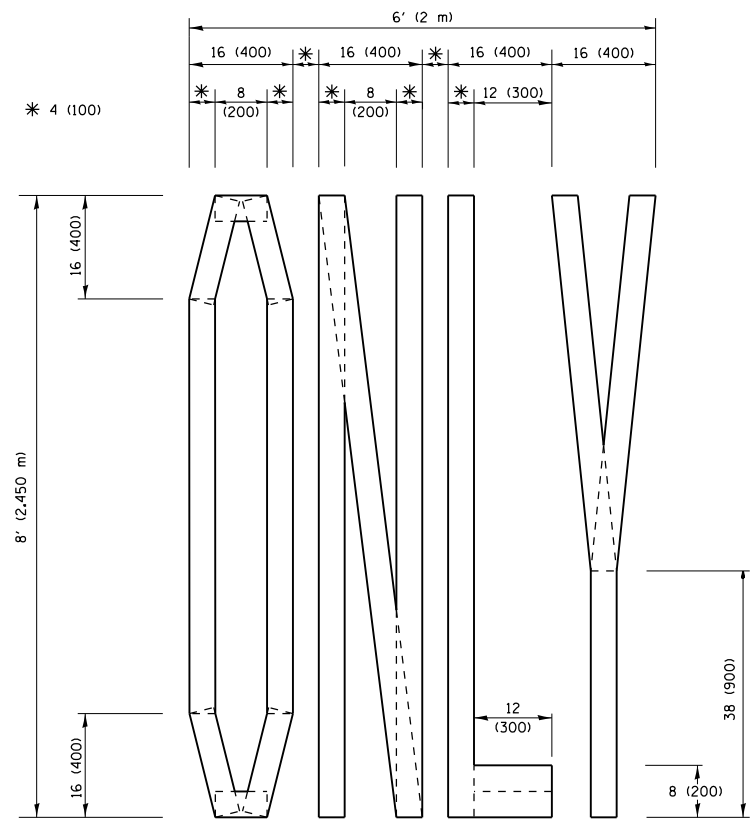
**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

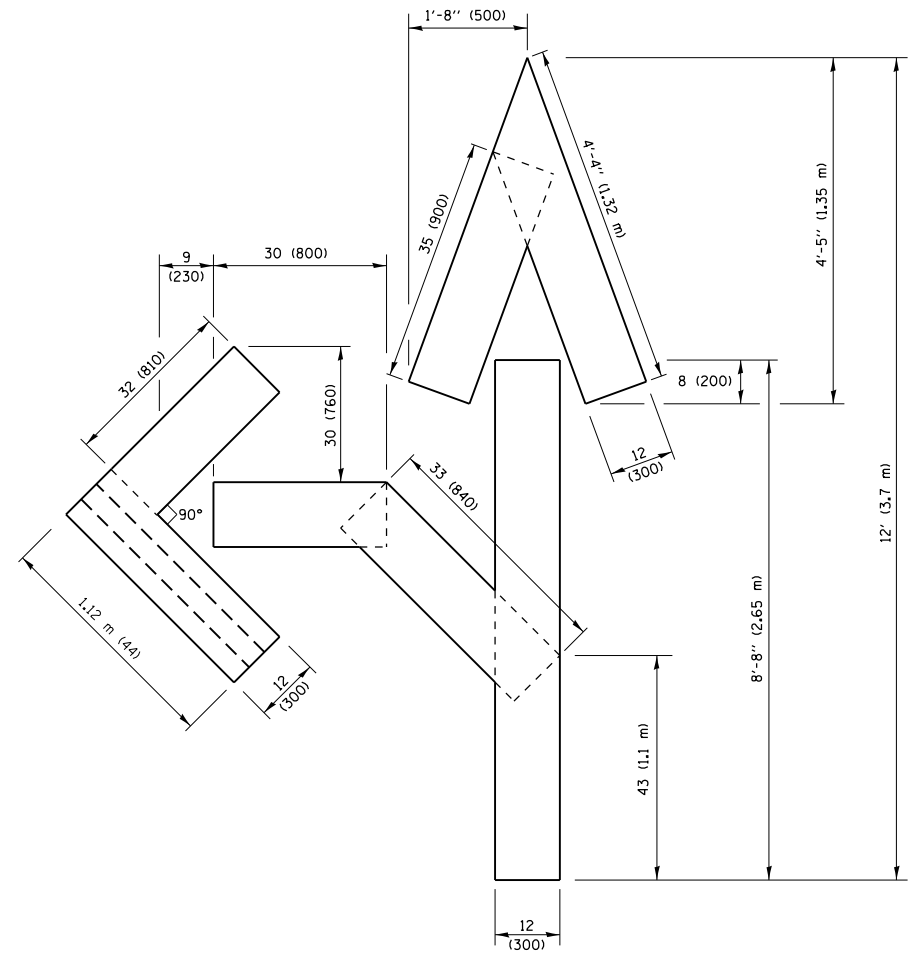
FILE NAME =	USER NAME = STEEDPA	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>	F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\01344\REVISED Design\HOUSEH 11-07-95	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	REVISED - A. SCHUETZE 07-01-13			573	(27, 30, 61 & 117)RS-7	KANE	47	43
Default	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			<b>TC-14</b>		<b>CONTRACT NO. 62C37</b>		ILLINOIS FED. AID PROJECT
	PLOT DATE = 10/26/2016	REVISED - T. RAMMACHER 01-06-00	REVISED -			SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.		



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

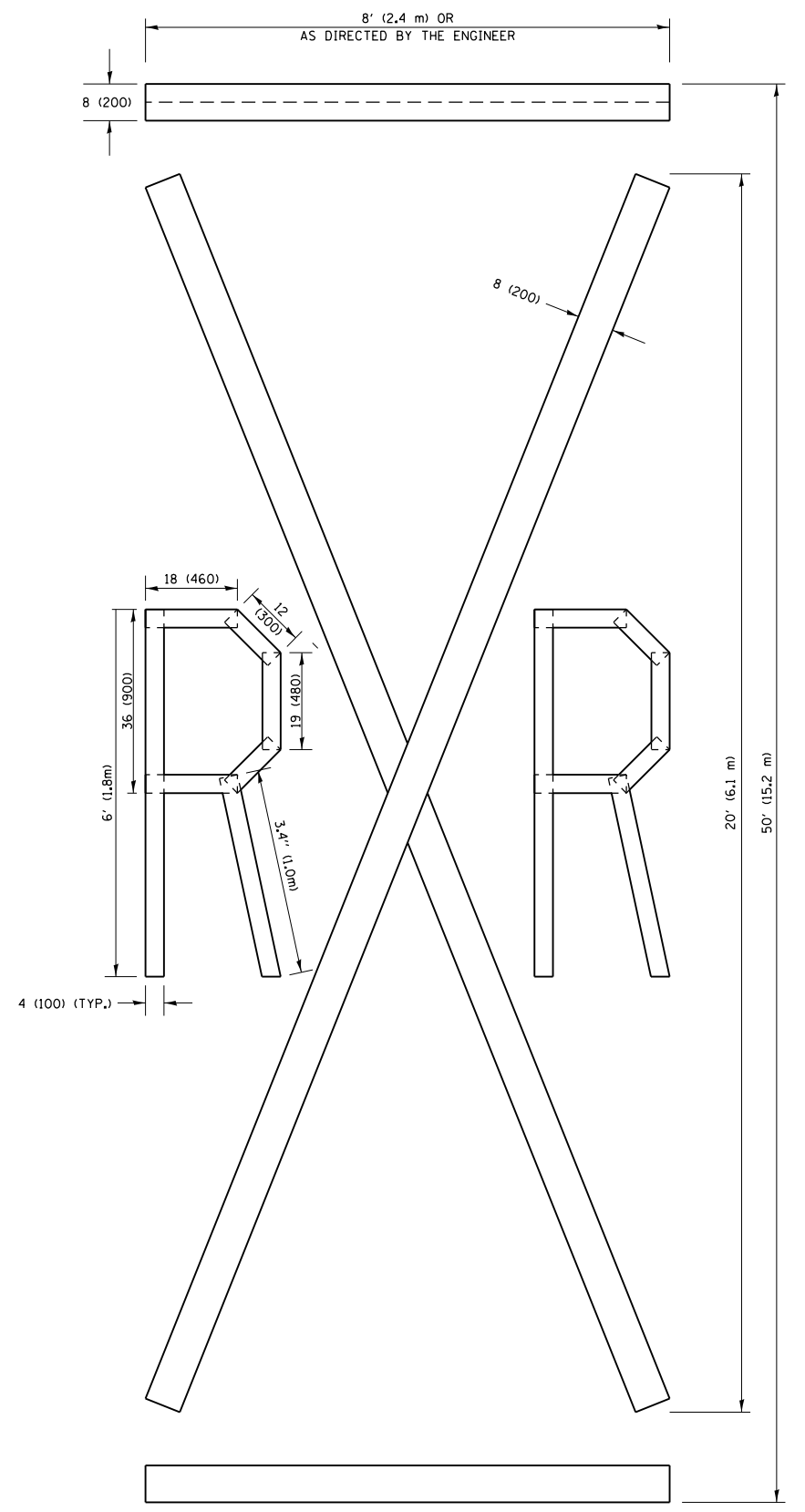


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
pw:\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\01344\BROWNS\Notes\Design\Diststd.dgn		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

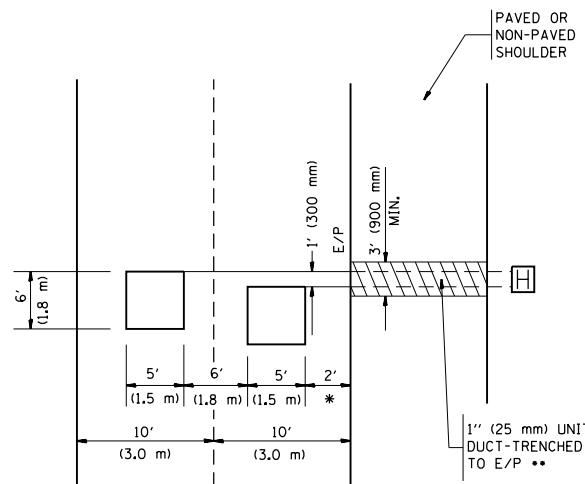
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(27, 30, 61 & 117)RS-7	KANE	47	44
<b>TC-16</b>		<b>CONTRACT NO. 62C37</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



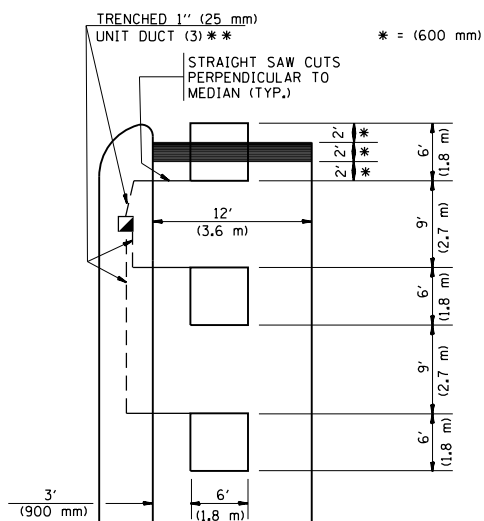
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

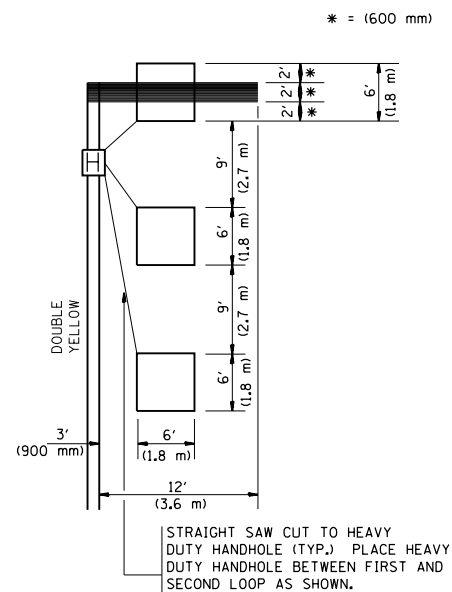


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

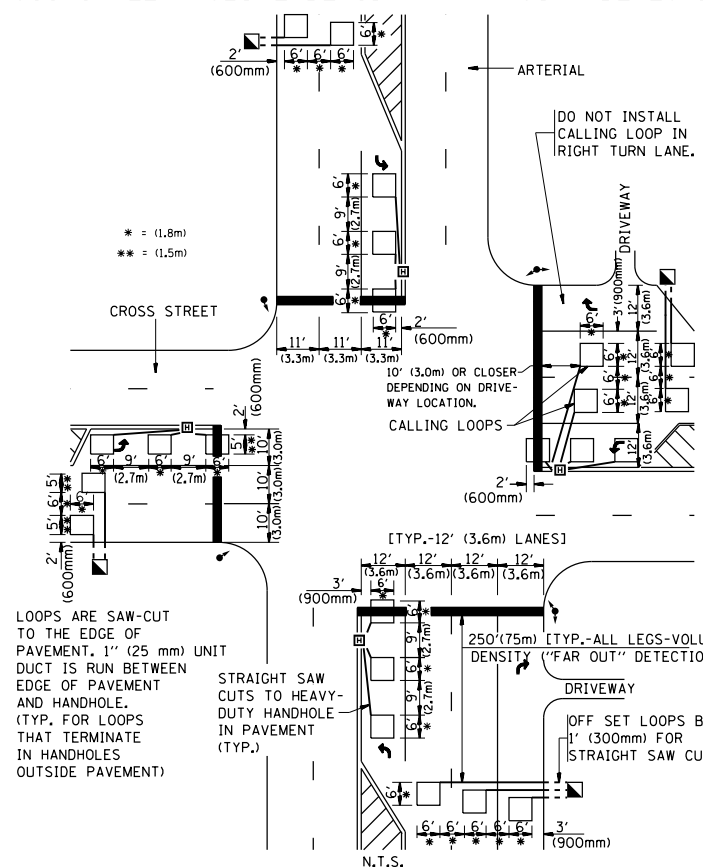
**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



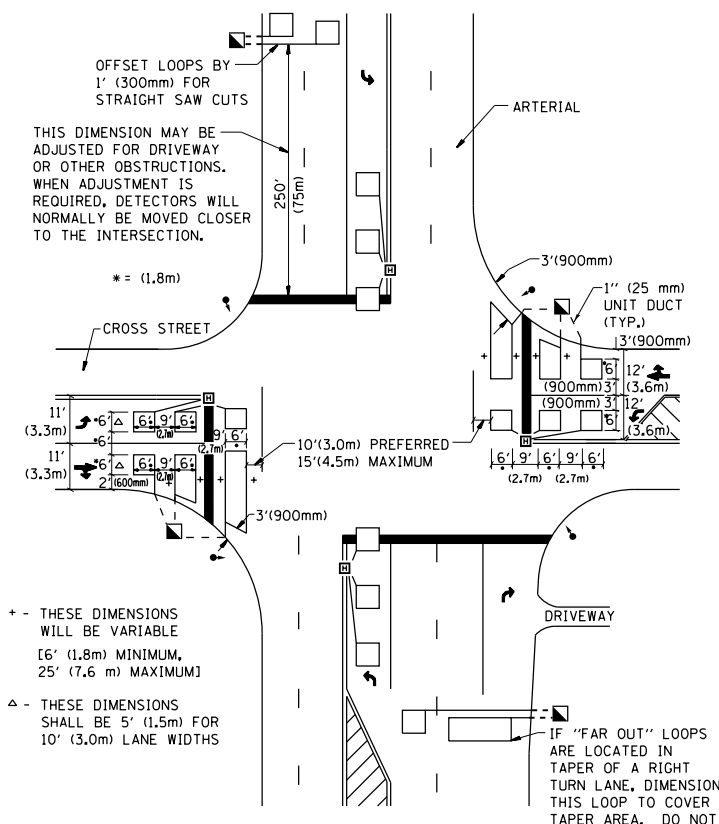
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1  
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2  
N.T.S.**

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

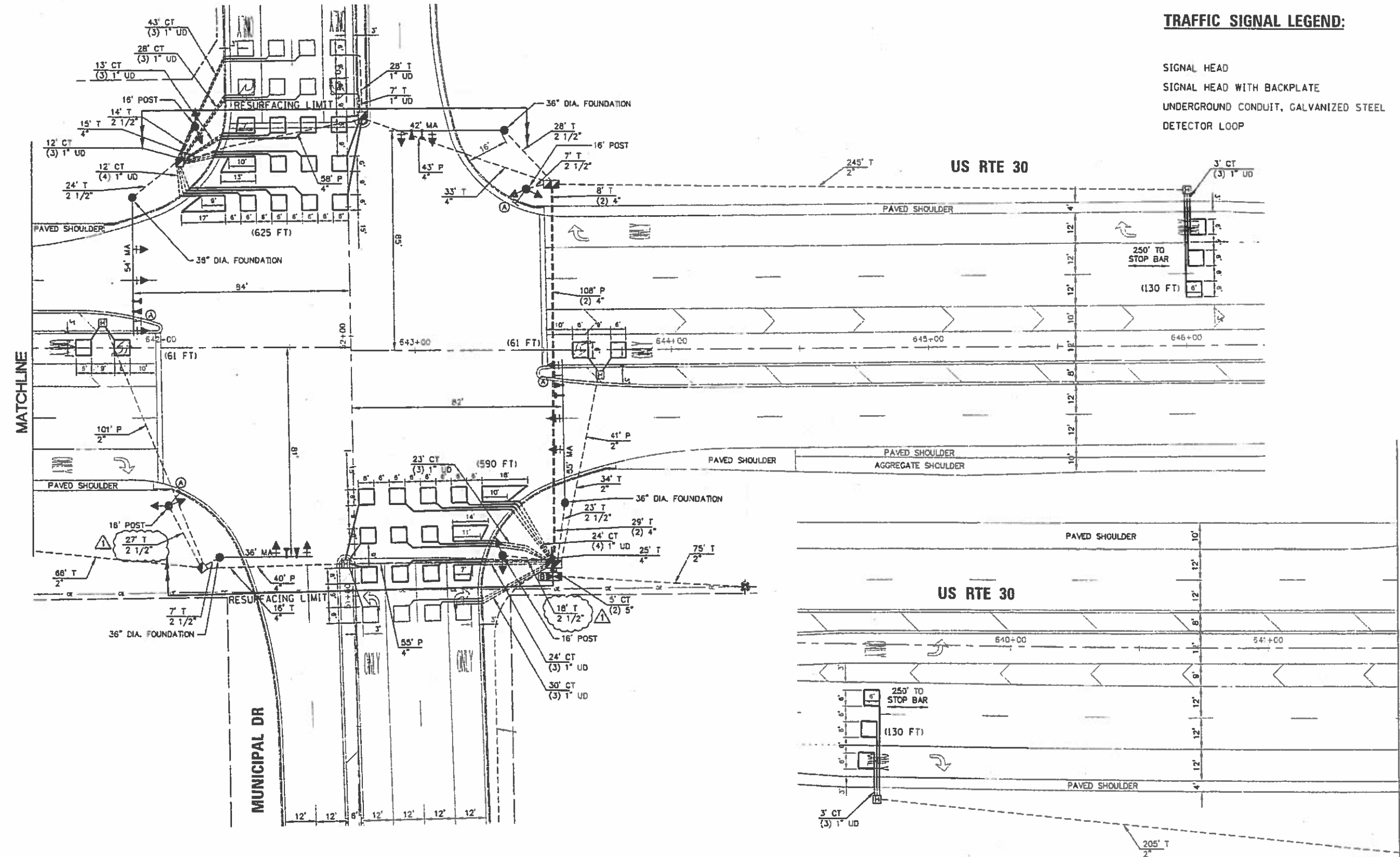
LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:  
ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

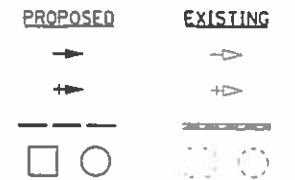
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>	F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBID\INTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI344\Design\Diststd.dgn		CHECKED - R.K.F.	REVISED -			573	(27, 30, 61 & 117)RS-7	KANE	47	45
PLOT SCALE = 100.0010 / 1 in.		DATE -	REVISED -			<b>TS-07</b>		<b>CONTRACT NO. 62C37</b>		
PLOT DATE = 10/26/2016						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			



**TRAFFIC SIGNAL LEGEND:**

SIGNAL HEAD  
 SIGNAL HEAD WITH BACKPLATE  
 UNDERGROUND CONDUIT, GALVANIZED STEEL  
 DETECTOR LOOP



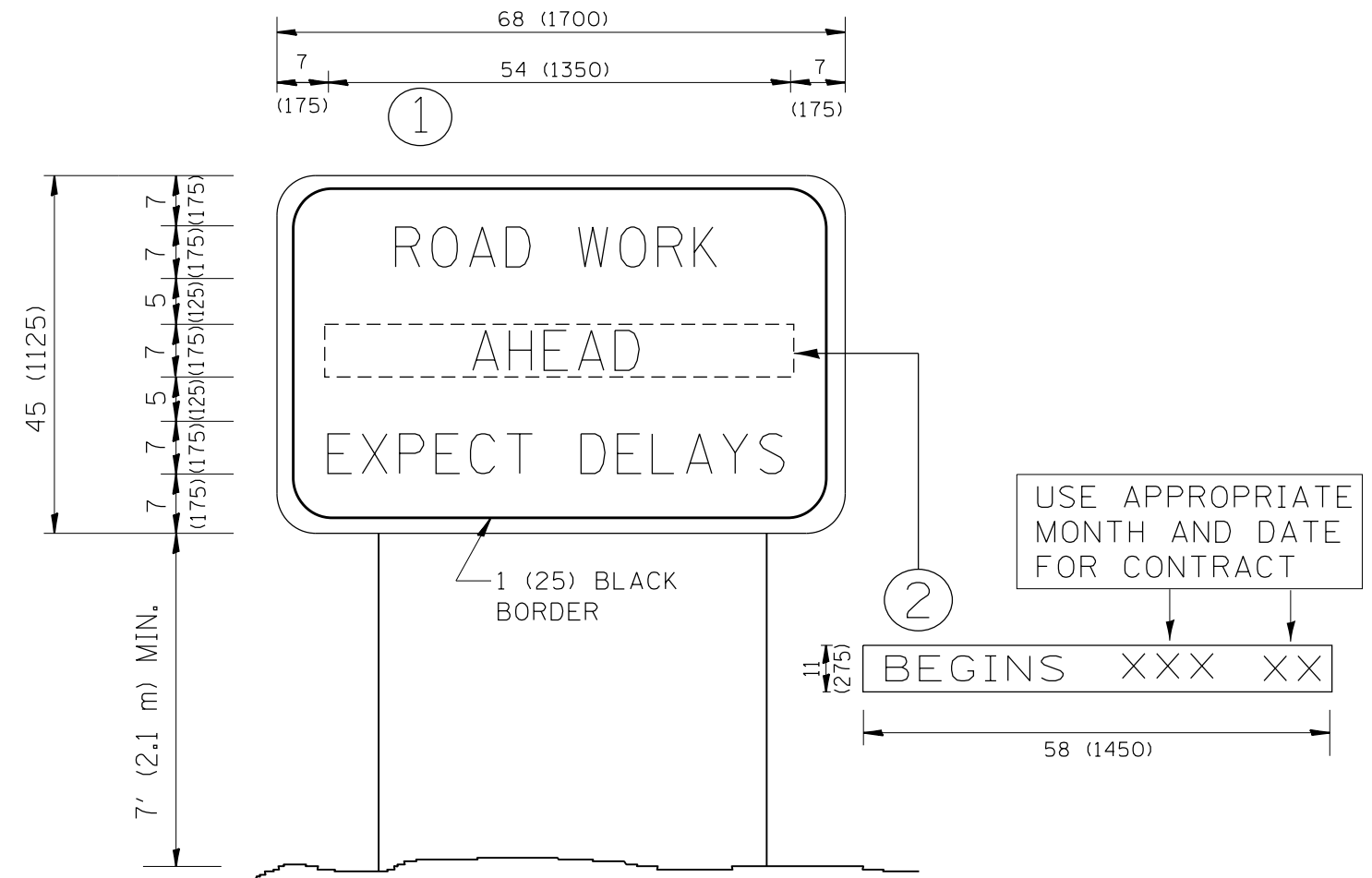
**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN  
 (WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	1597	FOOT

FILE NAME *	USER NAME * plascencia	DESIGNED - IP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETECTOR LOOP REPLACEMENT PLAN US RTE 30 AT MUNICIPAL DR</b>	F.A.P. RTE. 573	SECTION (27, 30, 61 & 117RIS-7)	COUNTY KANE	TOTAL SHEETS 47	SHEET NO. 46		
St: WP Design to an Resurfacing 62C37.Dwg	ectorLoopReplacementSheet.Example.01.22.16	DRAWN - IP	REVISED -			SCALE:	SHEET OF SHEETS	STA. TO STA.	CONTRACT NO. 62C37			
Default	PLOT SCALE * 40.0000 in.	CHECKED - LP	REVISED -			ILLINOIS FED. AID PROJECT						
	PLOT DATE * 8/16/2016	DATE - 8/15/2016	REVISED -									



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED - R. MIRS 09-15-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI344\BROWNS\Design\Diststd.dgn			REVISED - R. MIRS 12-11-97			573	(27, 30, 61 & 117)RS-7	KANE	47	47
PLOT SCALE = 100.0010' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99				<b>TC-22</b>		<b>CONTRACT NO. 62C37</b>		
PLOT DATE = 10/26/2016	DATE -	REVISED - C. JUCIUS 01-31-07	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT