

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------|------------|--------------------|-----------|
| 42 | 102-RS-3 | WASHINGTON | 26 | 1 |
| | | ILLINOIS | CONTRACT NO. 76J10 | |

FOR INDEX OF SHEETS, SEE SHEET NO. 2

MOCKINGBIRD ROAD TO I-64
2015 ADT = 9000
SU = 8.9% MU = 9.7%

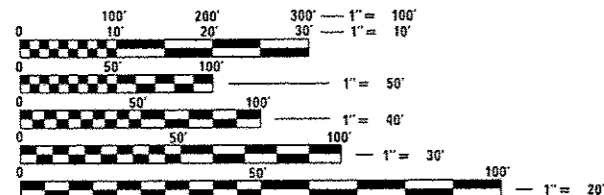
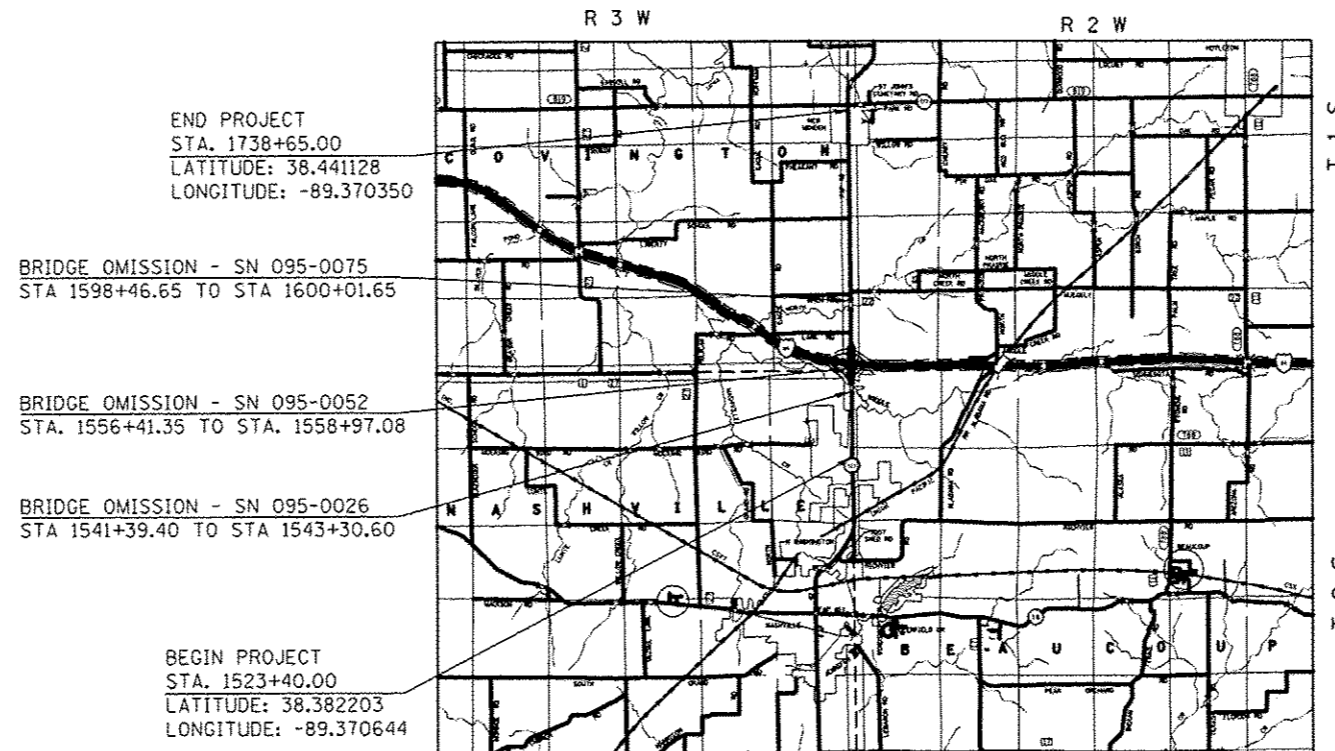
I-64 TO WREN ROAD
2015 ADT = 4250
SU = 9.4% MU = 9.4%

WREN ROAD TO IL 177
2015 ADT = 3800
SU = 5.7% MU = 7.9%

PROPOSED HIGHWAY PLANS

FAP ROUTE 42 (IL 127)
SECTION 102-RS-3
PROJECT ACF-0042(108)
3P RESURFACING
WASHINGTON COUNTY

C-98-032-16

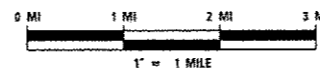


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: TIM PADGETT (618) 346-3325
PROJECT MANAGER: JOHNSON DUROSIER (618) 346-3454

CONTRACT NO. 76J10



GROSS LENGTH = 21,525.00 FT. = 4.077 MILE
NET LENGTH = 20,923.07 FT. = 3.963 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED Oct. 7 2016
Jeffrey Z. Kan... REGION ENGINEER
Dec 9 2016
Maureen M. Addis, P.E. ENGINEER OF DESIGN AND ENVIRONMENT
Dec 9 2016
Maureen M. Addis DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

| | |
|-------|--|
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LIST OF STANDARDS

| | |
|-----------|---|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 001001-02 | AREAS OF REINFORCEMENT BARS |
| 001006 | DECIMAL OF AN INCH AND OF A FOOT |
| 420001-08 | PAVEMENT JOINTS |
| 420701-03 | PAVEMENT WELDED WIRE REINFORCEMENT |
| 442101-07 | CLASS B PATCHES |
| 442201-03 | CLASS C AND D PATCHES |
| 482011-03 | HMA SHOULDER STRIPS/SHOULDERS WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS |
| 606001-06 | CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER |
| 642006 | SHOULDER RUMBLE STRIPS, 8 INCH |
| 701201-04 | LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS ≥ 45 MPH |
| 701306-03 | LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS ≥ 45 MPH |
| 701311-03 | LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY |
| 701336-06 | LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS ≥ 45 MPH |
| 701451-04 | RAMP CLOSURE FREEWAY/EXPRESSWAY |
| 701502-07 | URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE |
| 701901-06 | TRAFFIC CONTROL DEVICES |
| 728001-01 | TELESCOPING STEEL SIGN SUPPORT |
| 780001-05 | TYPICAL PAVEMENT MARKINGS |
| 781001-04 | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS |

GENERAL NOTES

① ILLINOIS STATE LAW REQUIRES A 48-HOUR NOTICE BE GIVEN TO UTILITIES BEFORE DIGGING. FIELD MARKING OF FACILITIES MAY BE OBTAINED BY CONTACTING J.U.L.I.E. (1-800-892-0123 OR 811) OR FOR NON-MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:

| UTILITY | ABOVE GROUND | BELOW GROUND |
|--|--------------|--------------|
| AMEREN ILLINOIS | X | X |
| EXXONMOBIL PIPELINE COMPANY | | X |
| FRONTIER COMMUNICATIONS | X | X |
| VILLAGE OF HOYLETON | | X |
| HOYLETON RURAL WATER COMPANY | | X |
| CITY OF NASHVILLE - WATER & SANITARY SEWER | | X |
| CITY OF NASHVILLE - GAS | | X |
| VILLAGE OF NEW MINDEN | | X |

MEMBERS OF J.U.L.I.E. CALL TOLL FREE (800) 892-0123 OR 811 AND ARE INDICATED BY *. NON-J.U.L.I.E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.

- ② "ROAD CONSTRUCTION AHEAD" SIGNS SHALL BE PLACED AT EACH END OF THE PROJECT PLUS THE INTERSECTING SIDE ROADS AND RAMP, AND WILL BE CONSIDERED INCLUDED IN THE TRAFFIC CONTROL PAY ITEMS. ALL CONSTRUCTION SIGNS SHALL BE 48" X 48" FLUORESCENT ORANGE.
- ③ ALL AREAS DISTURBED FOR ANY REASON SHALL BE PERMANENTLY SEEDED AND MULCHED AS DIRECTED BY THE ENGINEER. ALL AREAS DISTURBED BY THE CONTRACTOR OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE SEEDED AT THE CONTRACTOR'S EXPENSE.
- ④ THE CONTRACTOR AND THE ENGINEER SHALL BE AWARE THAT NO SURVEY WAS PERFORMED FOR THIS PROJECT. THE STATIONING, TOPOGRAPHY, AND QUANTITIES SHOWN IN THE PLANS WERE CREATED USING MICROFILM AND FIELD MEASUREMENTS. ALL SHALL BE ASSUMED TO BE APPROXIMATE. THE CONTRACTOR SHALL VERIFY DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- ⑤ SIX CHANGEABLE MESSAGE BOARDS SHALL BE REQUIRED TO BE PLACED TWO WEEKS PRIOR TO THE RAMP CLOSURES, AS WELL AS DURING THE DURATION OF CLOSURE TO ADVISE MOTORISTS OF THE RAMP CLOSURE DATES.

- ⑥ THE VARIOUS THICKNESS OF BITUMINOUS SURFACE REMOVAL SHOWN ON THE PLANS ARE THE AVERAGE THICKNESSES BASED UPON CONTROLLING THICKNESS AS INDICATED. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE. BITUMINOUS SURFACE REMOVAL HAS BEEN INCLUDED IN THE PLANS FOR THE PURPOSE OF REMOVING HIGH IRREGULARITIES AND TO ESTABLISH CROSS SLOPE.
- ⑦ THE REMOVAL OF THE EXISTING AGGREGATE SHOULDER WILL BE INCLUDED IN THE EARTH EXCAVATION QUANTITY.
- ⑧ THE THICKNESS OF HOT-MIX ASPHALT SURFACE MIXTURES SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
- ⑨ AN ESTIMATED QUANTITY OF 7,536 TONS OF CUTTINGS FROM THE HMA SURFACE REMOVAL OPERATION IS ANTICIPATED.
- ⑩ NO OVERNIGHT LANE CLOSURES WILL BE PERMITTED.
- ⑪ THE PROPOSED PAVEMENT MARKING SHALL MATCH THE LOCATIONS OF THE EXISTING PAVEMENT MARKING, AS DIRECTED BY THE ENGINEER.
- ⑫ MEDIAN REMOVAL PARTIAL DEPTH SHALL INCLUDE THE REMOVAL OF THE CORRUGATED PCC MEDIAN AT THE SOUTHERN END OF THE I-64 RAMP TERMINAL AT A DEPTH TO MATCH THE MAINLINE MILLING DEPTH. THIS AREA SHALL BE RESURFACED WITH HMA BINDER AND SURFACE COURSE TO MATCH THE MAINLINE RESURFACING DEPTH.
- ⑬ THE CONTRACTOR SHALL INSTALL THE PROPOSED ARCH CORRUGATED METAL PIPE CULVERT AT THE LOCATION SHOWN ON THE PLANS, WHICH SHALL BE PAID FOR AS PIPE CULVERTS, SPECIAL. THE PROPOSED CULVERT IS STORED BEHIND THE TRUCK REPAIR BUILDING NEXT TO SHELL GAS STATION. CONTACT KEITH BOYCE (NASHVILLE YARD) AT 618-327-3914.
- ⑭ THE FULL DEPTH PAVEMENT REMOVAL AND REPLACEMENT AT THE I-64 RAMP TERMINALS SHOWN ON THE PLANS SHALL BE PAID FOR AS CLASS B PATCHING.
- ⑮ THE PAVEMENT REPLACEMENT AT THE GAS STATION ENTRANCE SHALL BE PAID FOR AS HIGH-EARLY-STRENGTH PORTLAND CEMENT CONCRETE PAVEMENT, 10 INCH.

COMMITMENTS

CLOSE NORTHERN SHELL GAS STATION ENTRANCE AT STA. 1537+67 FOR A MAXIMUM DURATION OF TWO DAYS TO ALLOW THE REMOVAL AND REPLACEMENT OF EXISTING ENTRANCE CULVERT. FOLLOWING THE CLOSURE, THE ENTRANCE WILL BE OPEN TO TRAFFIC THROUGH THE USE OF TEMPORARY AGGREGATE. THE REINFORCED CONCRETE ENTRANCE WILL BE REPLACED BY STAGE CONSTRUCTION WITHIN 30 DAYS AFTER THE TEMPORARY CLOSURE AND NO OVERNIGHT CLOSURES WILL BE PERMITTED DURING THIS WORK. ALL ENTRANCE WORK WILL BE COMPLETED BY MAY 31ST, 2017.

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

| | |
|----------|--------------|
| ROUTE | FAP ROUTE 42 |
| SECTION | 102-RS-3 |
| COUNTY | WASHINGTON |
| CONTRACT | 76J10 |

| | |
|--------------|---|
| DESCRIPTION: | 3P RESURFACING ON IL 127 FROM IL 177 TO 0.4 MI N OF MOCKINGBIRD LANE IN NASHVILLE |
|--------------|---|

| MIXTURE USE | SURFACE-2 1/4" | LEVEL BINDER | POLY SURF-(1 1/2") | SHOULDERS ≥ 2.25" | SHOULDERS ≤ 2.25" |
|-----------------------------|-----------------------|-----------------------|-----------------------|-----------------------|-----------------------|
| AC/PG | PG 64-22 | PG 64-22 | SBS PG 76-22 | PG 64-22 | PG 64-22 |
| RAP % (MAX) | SEE SPECIAL PROVISION | SEE SPECIAL PROVISION | SEE SPECIAL PROVISION | SEE SPECIAL PROVISION | SEE SPECIAL PROVISION |
| DESIGN AIR VOIDS | 4.0% @ Ndes=70 | 4.0% @ Ndes=70 | 4.0% @ Ndes=90 | 4.0% @ Ndes=30 | 4.0% @ Ndes=30 |
| MIX COMPOSITION (Gradation) | IL 9.5 | IL 9.5FG | IL 9.5 | IL 19.0L | IL 9.5L |
| FRICTION AGG | MIXTURE "D" | MIXTURE "C" | MIXTURE "D" | | |
| QUALITY MGMT PROGRAM | OCP | OC/OA | OC/OA | OC/OA | OC/OA |

Plan quantities for Bituminous Surface Course items are calculated using a unit weight of 112 lb/sq yd/in (59.8 kg/sq m/25 mm thickness)

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTR. CODE |
|----------|--|-------|----------------|----------------------|
| | | | | 80% FED 20% STATE |
| | | | | ROADWAY |
| | | | | 0005 |
| | | | | RURAL |
| 44000156 | HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4" | SQ YD | 53723 | 53723 |
| 44000159 | HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2" | SQ YD | 16221 | 16221 |
| 44000300 | CURB REMOVAL | FOOT | 4 | 4 |
| 44003510 | MEDIAN REMOVAL PARTIAL DEPTH | SQ FT | 1808 | 1808 |
| 44200050 | WELDED WIRE REINFORCEMENT | SQ YD | 2081 | 2081 |
| 44200144 | PAVEMENT PATCHING, TYPE II, 12 INCH | SQ YD | 246 | 246 |
| 44200148 | PAVEMENT PATCHING, TYPE III, 12 INCH | SQ YD | 214 | 214 |
| 44200150 | PAVEMENT PATCHING, TYPE IV, 12 INCH | SQ YD | 1224 | 1224 |
| 44201000 | CLASS B PATCHES, TYPE IV, 12 INCH | SQ YD | 2081 | 2081 |
| 44201299 | DOWEL BARS 1 1/2" | EACH | 84 | 84 |
| 44213200 | SAW CUTS | FOOT | 1344 | 1344 |
| 44213204 | TIE BARS 3/4" | EACH | 297 | 297 |
| 44300200 | STRIP REFLECTIVE CRACK CONTROL TREATMENT | FOOT | 35358 | 35358 |
| 48102100 | AGGREGATE WEDGE SHOULDER, TYPE B | TON | 2113 | 2113 |
| 48203021 | HOT-MIX ASPHALT SHOULDERS, 6" | SQ YD | 8550 | 8550 |

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| | | | |
|--|------------------------|------------|-----------|
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| Default | PLOT DATE = 12/28/2016 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

| | | | | |
|---------------------------|----------|------------|--------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 42 | 102-RS-3 | WASHINGTON | 26 | 4 |
| SCALE: | | | CONTRACT NO. 76J10 | |
| SHEET 2 OF 5 SHEETS | | STA. | TO STA. | |
| ILLINOIS FED. AID PROJECT | | | | |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTR. CODE | |
|----------|---|--------|----------------|----------------------|--|
| | | | | 80% FED 20% STATE | |
| | | | | ROADWAY | |
| | | | | 0005 | |
| | | | | RURAL | |
| 48203100 | HOT-MIX ASPHALT SHOULDERS | TON | 1118 | 1118 | |
| 50105220 | PIPE CULVERT REMOVAL | FOOT | 105 | 105 | |
| 60100080 | FRENCH DRAIN | CU YD | 5 | 5 | |
| 64200108 | SHOULDER RUMBLE STRIPS, 8 INCH | FOOT | 26509 | 26509 | |
| 60100085 | GEOTECHNICAL FABRIC FOR FRENCH DRAINS | SQ YD | 16 | 16 | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 6 | 6 | |
| 60600605 | CONCRETE CURB, TYPE B | FOOT | 4 | 4 | |
| 67100100 | MOBILIZATION | LSUM | 1 | 1 | |
| 70100450 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701201 | LSUM | 1 | 1 | |
| 70100460 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306 | LSUM | 1 | 1 | |
| 70100600 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701336 | LSUM | 1 | 1 | |
| 70100820 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701451 | LSUM | 1 | 1 | |
| 70102622 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701502 | LSUM | 1 | 1 | |
| 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 5580 | 5580 | |
| 70300150 | SHORT TERM PAVEMENT MARKING REMOVAL | SQ FT | 1860 | 1860 | |
| 70300210 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS | SQ FT | 597 | 597 | |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 120609 | 120609 | |

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|-------------|-----------------------|------------|-----------|---|------------------------------|---------|-------------|------|---------------------------|---------|----------|--------------|-----------|---|
| FILE NAME = | USER NAME = durozierj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUMMARY OF QUANTITIES | | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| Default | | CHECKED - | REVISED - | | SCALE: | SHEET 3 | OF 5 SHEETS | STA. | TO STA. | 42 | 102-R5-3 | WASHINGTON | 26 | 5 |
| | | DATE - | REVISED - | | | | | | CONTRACT NO. 76J10 | | | | | |
| | | | | | | | | | ILLINOIS FED. AID PROJECT | | | | | |

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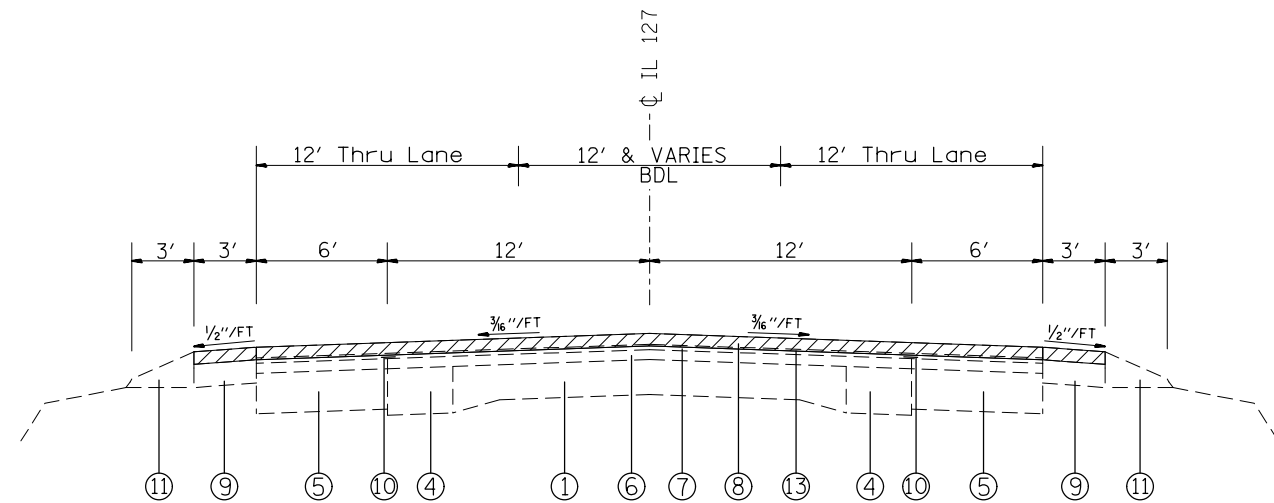
| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTR. CODE | |
|------------|------------------------------------|------|----------------|--------------|------------|
| | | | | 80 % FED | 20 % STATE |
| | | | | ROADWAY | |
| | | | | 0005 | |
| | | | | RURAL | |
| Z0016702 | DETOUR SIGNING | LSUM | 1 | 1 | |
| Z0033700 | LONGITUDINAL JOINT SEALANT | FOOT | 22143 | 22143 | |
| Z0034105 | MATERIAL TRANSFER DEVICE | TON | 6473 | 6473 | |
| Z0036200 | PAINT CURB | FOOT | 348 | 348 | |
| φ Z0076600 | TRAINEES | Hour | 1500 | 1500 | |
| Z0070100 | SURVEY MONUMENT COVER ASSEMBLY | EACH | 6 | 6 | |
| φ Z0076604 | TRAINEES TRAINING PROGRAM GRADUATE | Hour | 1500 | 1500 | |
| XZ00094 | PIPE CULVERTS, SPECIAL | FOOT | 108 | 108 | |

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φ 0042

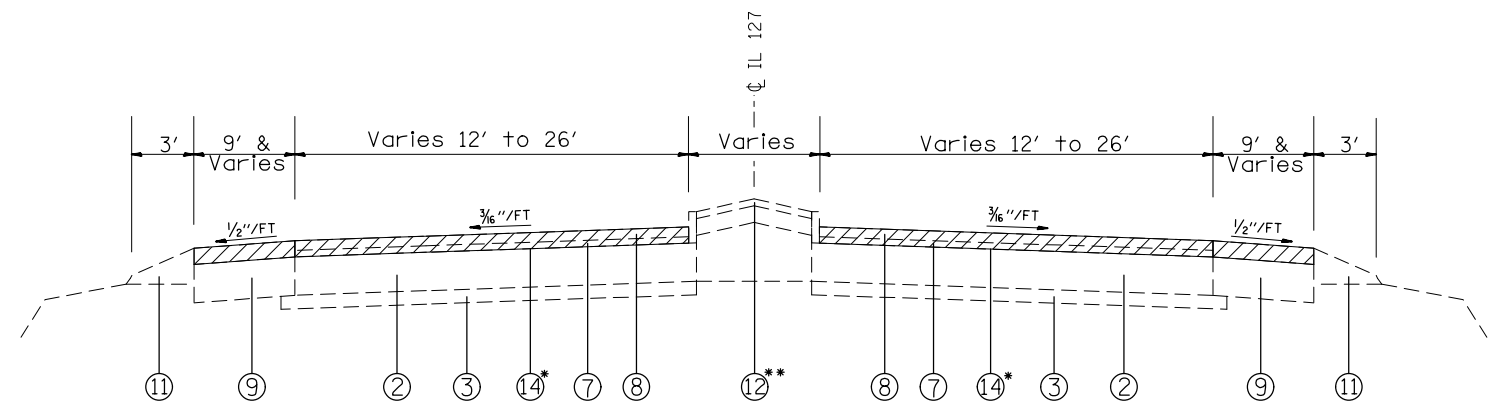
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| Default | | CHECKED - | REVISED - | | SCALE: | SHEET 5 | OF 5 SHEETS | STA. | TO STA. | 42 | 102-RS-3 | WASHINGTON | 26 | 7 |
| | | DATE - | REVISED - | | | | | | | | | | | |
| | | | | | | | | | | | | | | |

ILLINOIS FED. AID PROJECT



EXISTING TYPICAL SECTION

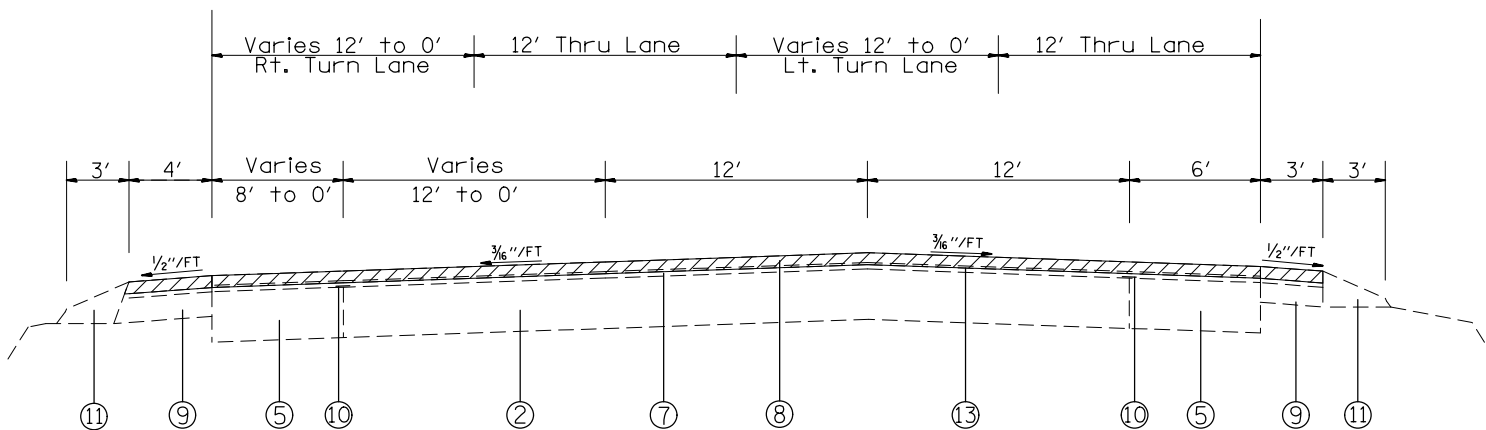
STA. 1523+40.00 TO STA. 1537+10.00



EXISTING TYPICAL SECTION

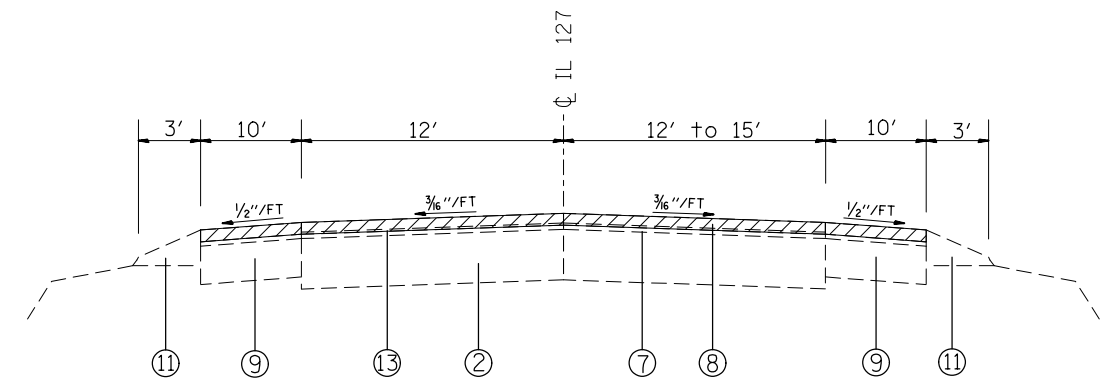
STA. 1543+30.60 TO STA. 1572+17.00
BRIDGE OMISSION: STA. 1556+41.35 TO STA. 1558+97.08

- THE INTENT IS TO MILL DOWN TO BARE CONCRETE.
- REMOVE CORRUGATED PCC MEDIAN FROM STA. 1543+37 TO STA. 1545+01



EXISTING TYPICAL SECTION

STA. 1537+10.00 TO STA. 1543+30.60
BRIDGE OMISSION: STA. 1541+39.40 TO STA. 1543+30.60



EXISTING TYPICAL SECTION

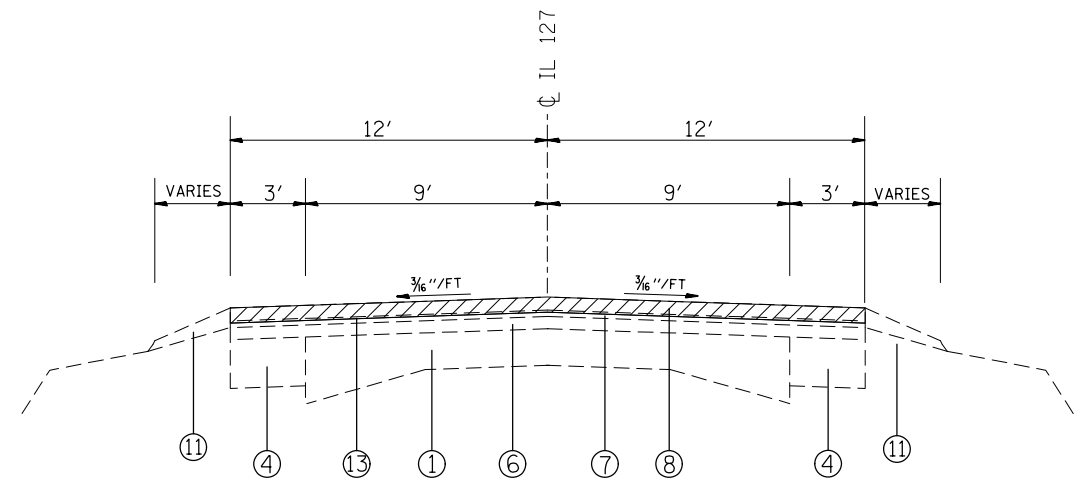
STA. 1572+17.00 TO STA. 1576+76.00

LEGEND

- | | |
|---|--|
| ① EXISTING PCC PAVEMENT 9-6-9 | ⑬ PROPOSED HMA SURFACE REMOVAL, 1 3/4" |
| ② EXISTING PCC BASE COURSE 9" | ⑭ PROPOSED HMA SURFACE REMOVAL, 2 1/2"* |
| ③ EXISTING STABILIZED SUB-BASE, 4" | ⑮ PROPOSED BITUMINOUS MATERIALS (TACK COAT) |
| ④ EXISTING WIDENING 8" PCC BASE COURSE | ⑯ PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT |
| ⑤ EXISTING BASE COURSE WIDENING 9" | ⑰ PROPOSED LONGITUDINAL JOINT SEALANT |
| ⑥ EXISTING HMA RESURFACING, VARIES | ⑱ PROPOSED LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N70, 1" |
| ⑦ EXISTING HMA LEVELING BINDER, 3/4" TO 1" | ⑲ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX D, N90, 1 1/2" |
| ⑧ EXISTING HMA SURFACE COURSE, 1 1/2" | ⑳ PROPOSED HMA SURFACE COURSE, MIX D, N70, 2 1/4" |
| ⑨ EXISTING HMA SHOULDER, VARIES | ㉑ PROPOSED HMA SHOULDERS, 2 1/4" |
| ⑩ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT | ㉒ PROPOSED HMA SHOULDERS, 2 1/2" |
| ⑪ EXISTING AGGREGATE WEDGE SHOULDER, TYPE B | ㉓ PROPOSED HMA SHOULDERS, 6" |
| ⑫ EXISTING PCC MEDIAN SURFACE, 4" | ㉔ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B |

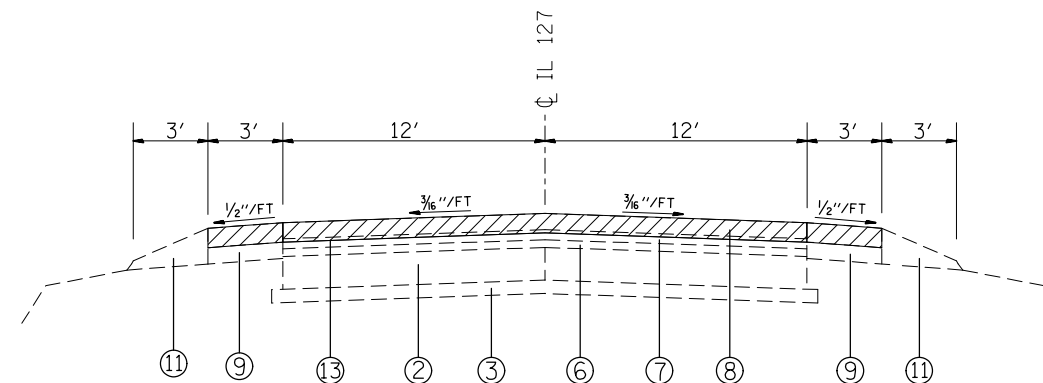


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|--|-------------------------------|------------|-----------|---|----------------------------------|---------------------|------------|-------------|---------------------------|--------|--------------|-----------|
| FILE NAME = | USER NAME = durosierj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | EXISTING TYPICAL SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| pw:\IL\084EBIDINTEG\Illinois.gov\PWIDOT\Documents\IDOT Offices\District 8\Projects\0875\DRAWING\LEADSheets\0876J10-sht-pland | | DRAWN | REVISED | | 42 | 102-RS-3 | WASHINGTON | 26 | 8 | | | |
| Default | PLOT SCALE = 100.0000' / 1in. | CHECKED - | REVISED - | | CONTRACT NO. 76J10 | | | | | | | |
| | PLOT DATE = 10/20/2016 | DATE - | REVISED - | | SCALE: | SHEET 1 OF 4 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT | | | |



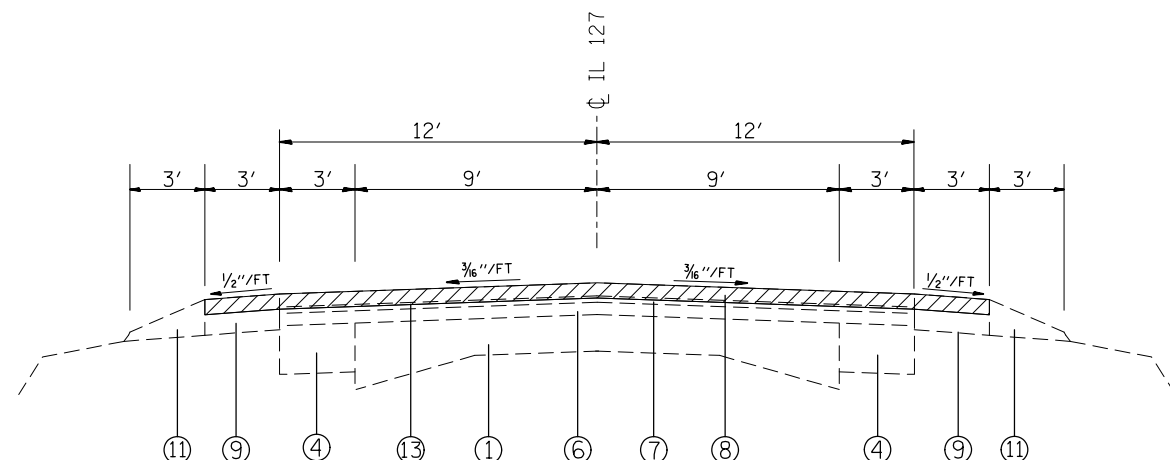
EXISTING TYPICAL SECTION

STA. 1576+76.00 TO STA. 1596+50.00
 STA. 1602+00.00 TO STA. 1738+65.00



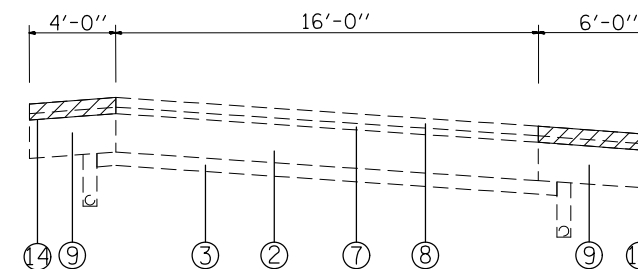
EXISTING TYPICAL SECTION

STA. 1597+70.00 TO STA. 1600+79.50
 BRIDGE OMISSION: STA. 1598+46.65 TO STA. 1600+01.65



EXISTING TYPICAL SECTION

STA. 1596+50.00 TO STA. 1597+70.00
 STA. 1600+79.50 TO STA. 1602+00.00



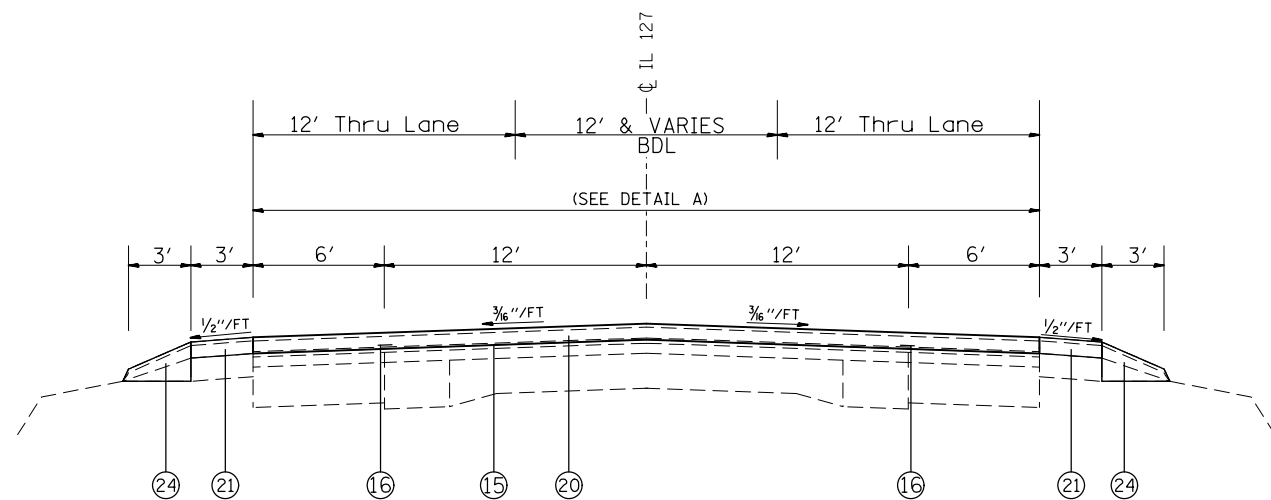
RAMP TYPICAL SECTION

LEGEND

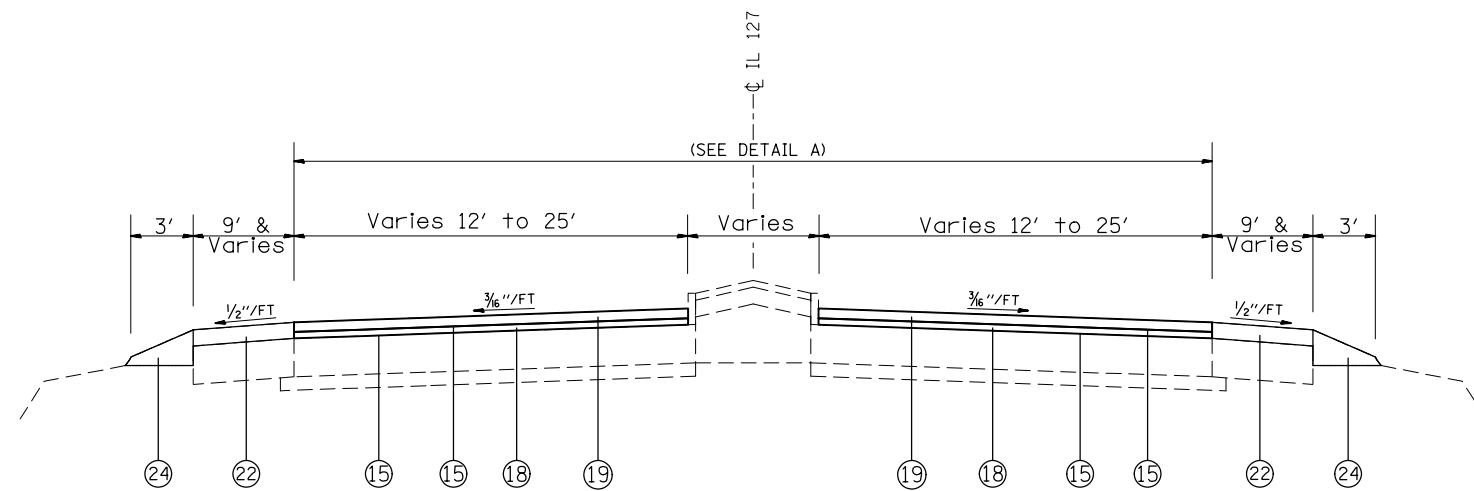
- | | |
|---|--|
| ① EXISTING PCC PAVEMENT 9-6-9 | ⑬ PROPOSED HMA SURFACE REMOVAL, 1 3/4" |
| ② EXISTING PCC BASE COURSE 9" | ⑭ PROPOSED HMA SURFACE REMOVAL, 2 1/2" |
| ③ EXISTING STABILIZED SUB-BASE, 4" | ⑮ PROPOSED BITUMINOUS MATERIALS (TACK COAT) |
| ④ EXISTING WIDENING 8" PCC BASE COURSE | ⑯ PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT |
| ⑤ EXISTING BASE COURSE WIDENING 9" | ⑰ PROPOSED LONGITUDINAL JOINT SEALANT |
| ⑥ EXISTING HMA RESURFACING, VARIES | ⑱ PROPOSED LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N70, 1" |
| ⑦ EXISTING HMA LEVELING BINDER, 3/4" TO 1" | ⑲ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX D, N90, 1 1/2" |
| ⑧ EXISTING HMA SURFACE COURSE, 1 1/2" | ⑳ PROPOSED HMA SURFACE COURSE, MIX D, N70, 2 1/4" |
| ⑨ EXISTING HMA SHOULDER, VARIES | ㉑ PROPOSED HMA SHOULDERS, 2 1/4" |
| ⑩ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT | ㉒ PROPOSED HMA SHOULDERS, 2 1/2" |
| ⑪ EXISTING AGGREGATE WEDGE SHOULDER, TYPE B | ㉓ PROPOSED HMA SHOULDERS, 6" |
| ⑫ EXISTING PCC MEDIAN SURFACE, 4" | ㉔ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B |



| | | | | | | | | | | | | |
|--|-------------------------------|------------|-----------|---|---|----------|------------|--------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = durosierj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | EXISTING TYPICAL SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| pw:\IL\084EBIDINTEG\illinois.gov\PIDOT\Documents\IDOT Offices\District 8\Projects\0876\DRAWING\LEADSheets\0876J10-sht-plan.dwg | | DRAWN - | REVISED - | | 42 | 102-RS-3 | WASHINGTON | 26 | 9 | | | |
| Default | PLOT SCALE = 100.0000' / 1in. | CHECKED - | REVISED - | | SCALE: SHEET 2 OF 4 SHEETS STA. TO STA. | | | CONTRACT NO. 76J10 | | | | |
| | PLOT DATE = 10/20/2016 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |

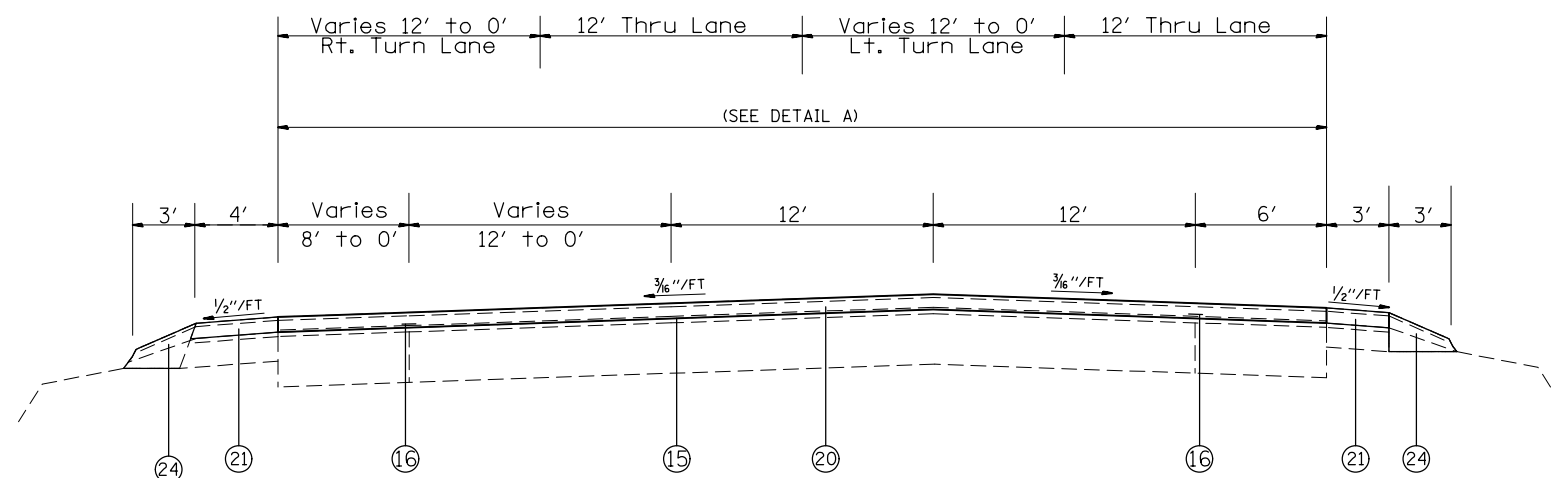


PROPOSED TYPICAL SECTION
STA. 1523+40.00 TO STA. 1537+10.00

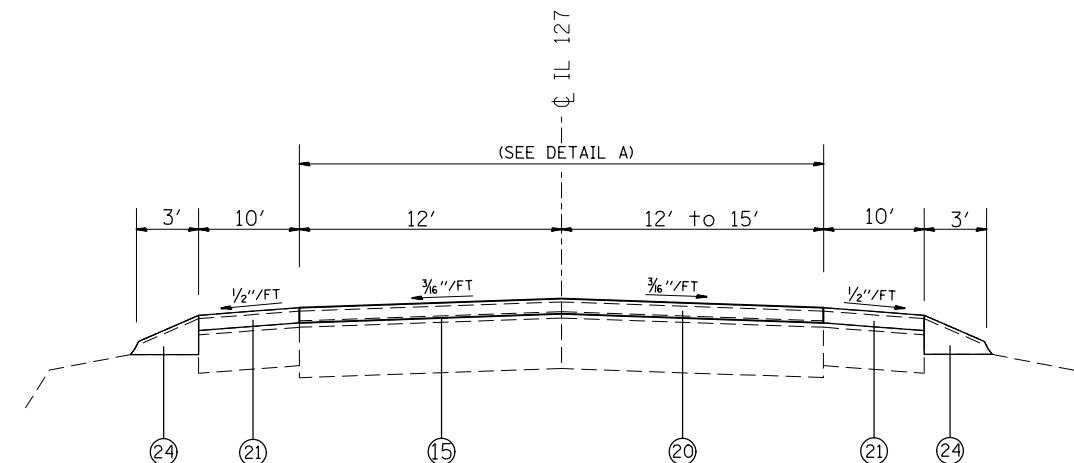


PROPOSED TYPICAL SECTION
STA. 1543+30.60 TO STA. 1572+17.00*
BRIDGE OMISSION: STA. 1541+39.40 TO STA. 1543+30.60

*RESURFACE MILLED CORRUGATED PCC MEDIAN AREA FROM STA. 1543+37 TO STA. 1545+01 WITH HMA BINDER AND SURFACE COURSE TO MATCH MAINLINE RESURFACING



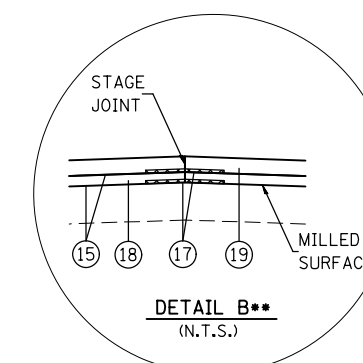
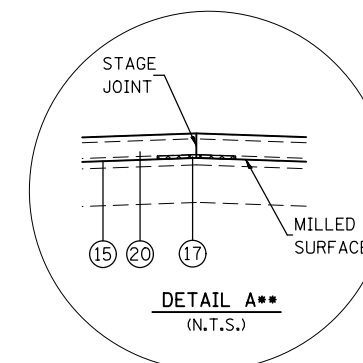
PROPOSED TYPICAL SECTION
STA. 1537+10.00 TO STA. 1543+30.60



PROPOSED TYPICAL SECTION
STA. 1572+17.00 TO STA. 1576+76.00

LEGEND

- ① EXISTING PCC PAVEMENT 9-6-9
- ② EXISTING PCC BASE COURSE 9"
- ③ EXISTING STABILIZED SUB-BASE, 4"
- ④ EXISTING WIDENING 8" PCC BASE COURSE
- ⑤ EXISTING BASE COURSE WIDENING 9"
- ⑥ EXISTING HMA RESURFACING, VARIES
- ⑦ EXISTING HMA LEVELING BINDER, 3/4" TO 1"
- ⑧ EXISTING HMA SURFACE COURSE, 1 1/2"
- ⑨ EXISTING HMA SHOULDER, VARIES
- ⑩ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ⑪ EXISTING AGGREGATE WEDGE SHOULDER, TYPE B
- ⑫ EXISTING PCC MEDIAN SURFACE, 4"
- ⑬ PROPOSED HMA SURFACE REMOVAL, 1 3/4"
- ⑭ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑮ PROPOSED BITUMINOUS MATERIALS (TACK COAT)
- ⑯ PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ⑰ PROPOSED LONGITUDINAL JOINT SEALANT
- ⑱ PROPOSED LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N70, 1"
- ⑲ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX D, N90, 1 1/2"
- ⑳ PROPOSED HMA SURFACE COURSE, MIX D, N70, 2 1/4"
- ㉑ PROPOSED HMA SHOULDERS, 2 1/4"
- ㉒ PROPOSED HMA SHOULDERS, 2 1/2"
- ㉓ PROPOSED HMA SHOULDERS, 6"
- ㉔ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B



**DETAIL "A" AND "B" SHALL NOT BE APPLIED ALONG THE EDGE OF PAVEMENT

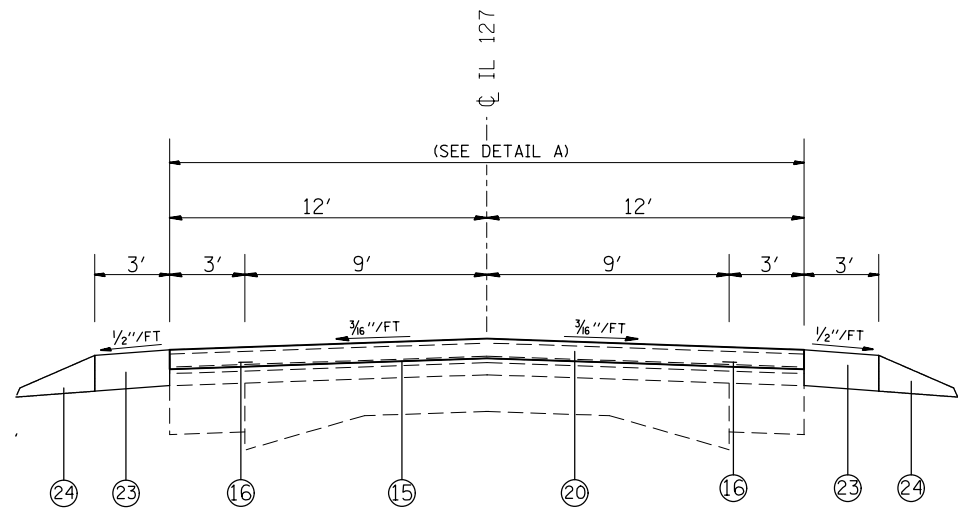
| | | | |
|---|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = durosier.j | DESIGNED - | REVISED - |
| pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 8\Projects\0876\DRAWING\LEADSheets\0876J10-sht-pland | | DRAWN - | REVISED - |
| Default | PLOT SCALE = 100.0000' / 1" | CHECKED - | REVISED - |
| | PLOT DATE = 10/20/2016 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED TYPICAL SECTIONS

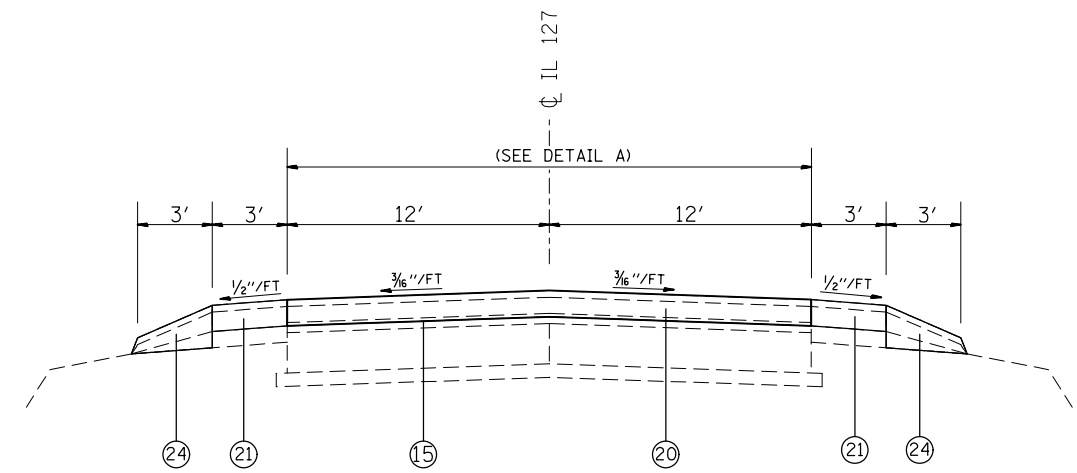
SCALE: SHEET 3 OF 4 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|------------|--------------------|-----------|
| 42 | 102-RS-3 | WASHINGTON | 26 | 10 |
| | | | CONTRACT NO. 76J10 | |
| ILLINOIS FED. AID PROJECT | | | | |



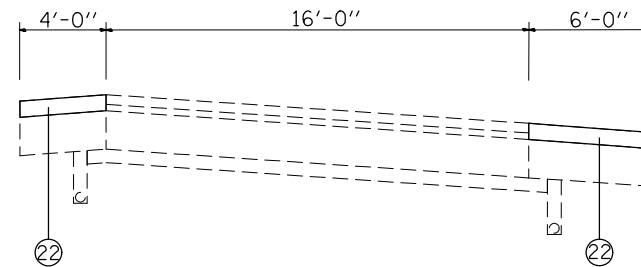
PROPOSED TYPICAL SECTION

STA. 1576+76.00 TO STA. 1596+50.00
 STA. 1602+00.00 TO STA. 1713+22.00

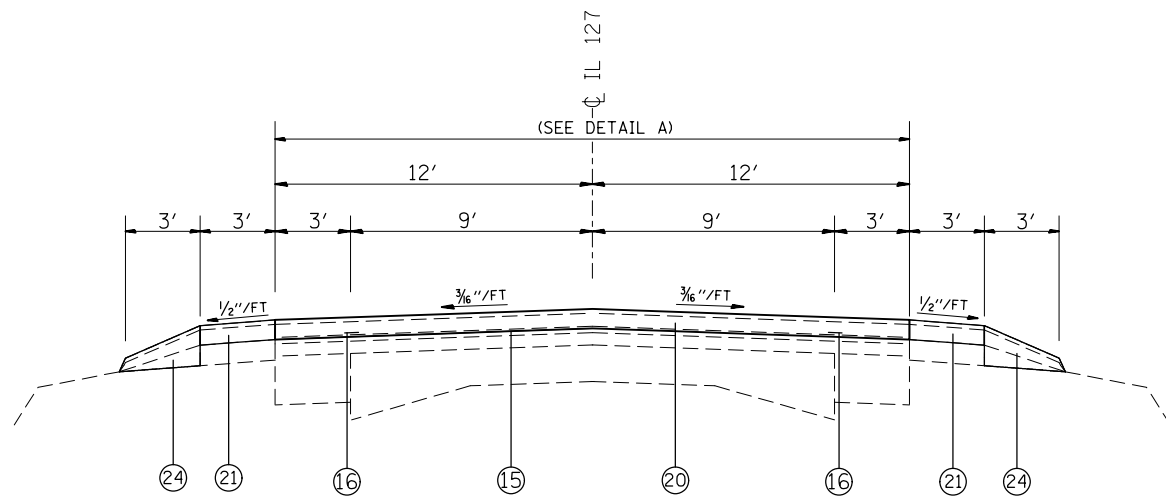


PROPOSED TYPICAL SECTION

STA. 1597+70.00 TO STA. 1600+79.50
 BRIDGE OMISSION: STA. 1598+46.65 TO STA. 1600+01.65

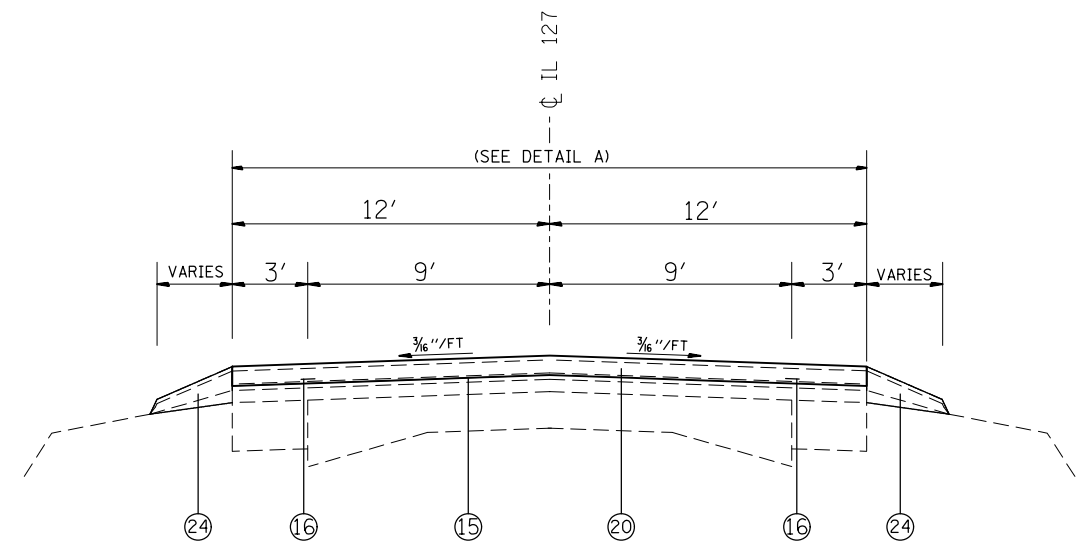


RAMP TYPICAL SECTION



PROPOSED TYPICAL SECTION

STA. 1596+50.00 TO STA. 1597+70.00
 STA. 1600+79.50 TO STA. 1602+00.00

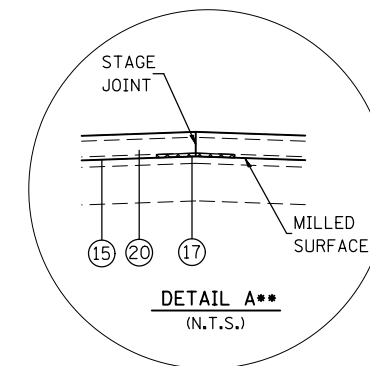


PROPOSED TYPICAL SECTION

STA. 1714+30.00 TO STA. 1738+65.00

LEGEND

- | | |
|---|--|
| ① EXISTING PCC PAVEMENT 9-6-9 | ⑬ PROPOSED HMA SURFACE REMOVAL, 1 3/4" |
| ② EXISTING PCC BASE COURSE 9" | ⑭ PROPOSED HMA SURFACE REMOVAL, 2 1/2" |
| ③ EXISTING STABILIZED SUB-BASE, 4" | ⑮ PROPOSED BITUMINOUS MATERIALS (TACK COAT) |
| ④ EXISTING WIDENING 8" PCC BASE COURSE | ⑯ PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT |
| ⑤ EXISTING BASE COURSE WIDENING 9" | ⑰ PROPOSED LONGITUDINAL JOINT SEALANT |
| ⑥ EXISTING HMA RESURFACING, VARIES | ⑱ PROPOSED LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N70, 1" |
| ⑦ EXISTING HMA LEVELING BINDER, 3/4" TO 1" | ⑲ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX D, N90, 1 1/2" |
| ⑧ EXISTING HMA SURFACE COURSE, 1 1/2" | ⑳ PROPOSED HMA SURFACE COURSE, MIX D, N70, 2 1/4" |
| ⑨ EXISTING HMA SHOULDER, VARIES | ㉑ PROPOSED HMA SHOULDERS, 2 1/4" |
| ⑩ EXISTING STRIP REFLECTIVE CRACK CONTROL TREATMENT | ㉒ PROPOSED HMA SHOULDERS, 2 1/2" |
| ⑪ EXISTING AGGREGATE WEDGE SHOULDER, TYPE B | ㉓ PROPOSED HMA SHOULDERS, 6" |
| ⑫ EXISTING PCC MEDIAN SURFACE, 4" | ㉔ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B |



••DETAIL "A" SHALL NOT BE APPLIED ALONG THE EDGE OF PAVEMENT

| | | | | | | | | | | | | | |
|--|----------------------|------------|-----------|---|----------------------------------|---------------------|------|--------------------|---------|----------|--------------|-----------|----|
| FILE NAME = | USER NAME = duosierj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PROPOSED TYPICAL SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| pw:\IL\084EBIDINTEG.illinois.gov\PIDOT\Documents\IDOT Offices\District 8\Projects\0875\DRAWING\DATA\LEADSheets\0876J10-sht-pland | | DRAWN - | REVISED - | | SCALE: | SHEET 4 OF 4 SHEETS | STA. | TO STA. | 42 | 102-RS-3 | WASHINGTON | 26 | 11 |
| Default | | CHECKED - | REVISED - | | | | | CONTRACT NO. 76J10 | | | | | |
| | | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | | |

| MAINLINE RESURFACING | | | | | | | | | | | |
|----------------------|----|---------|----------------------------|----------------------------|------------------------------|----------------------------------|---|------------------------------------|-------------------------------|--|----------------------------|
| LOCATION | | | HMA SURFACE REMOVAL 1 3/4" | HMA SURFACE REMOVAL 2 1/2" | MEDIAN REMOVAL PARTIAL DEPTH | BITUMINOUS MATERIALS (TACK COAT) | LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N70 | POLY HMA SURFACE COURSE, MIX D N90 | HMA SURFACE COURSE, MIX D N70 | STRIP REFLECTIVE CRACK CONTROL TREATMENT | LONGITUDINAL JOINT SEALANT |
| | | | SQ YD | SQ YD | SQ FT | POUND | TON | TON | TON | FOOT | FOOT |
| STA | TO | STA | | | | | | | | | |
| 1523+40 | TO | 1526+21 | 936.7 | | | 421.5 | | | 118.0 | 562 | 281 |
| 1526+21 | TO | 1539+26 | 5317.3 | | | 2392.8 | | | 670.0 | 2610 | 2878 |
| 1539+26 | TO | 1540+70 | 672.0 | | | 302.4 | | | 84.7 | 288 | 432 |
| 1540+70 | TO | 1541+39 | 262.2 | | | 118.0 | | | 33.0 | 139 | 139 |
| 1543+31 | TO | 1545+87 | | 997.6 | 1807.5 | 673.4 | 55.9 | 83.8 | | | 485 |
| 1545+87 | TO | 1549+77 | | 1286.9 | | 868.7 | 72.1 | 108.1 | | | |
| 1549+77 | TO | 1553+23 | | 1589.4 | | 1072.9 | 89.0 | 133.5 | | | 439 |
| 1553+23 | TO | 1556+41 | | 1193.4 | | 805.6 | 66.8 | 100.2 | | | 278 |
| 1558+97 | TO | 1562+09 | | 1169.1 | | 789.1 | 65.5 | 98.2 | | | 272 |
| 1562+09 | TO | 1565+61 | | 1588.0 | | 1071.9 | 88.9 | 133.4 | | | 446 |
| 1565+61 | TO | 1572+17 | | 2052.0 | | 1385.1 | 114.9 | 172.4 | | | |
| 1572+17 | TO | 1576+76 | 1377.0 | | | 619.7 | | | 173.5 | | 459 |
| 1576+76 | TO | 1598+47 | 5788.4 | | | 2604.8 | | | 729.3 | 4188 | 2171 |
| 1600+02 | TO | 1737+47 | 36654.3 | | | 16494.4 | | | 4618.4 | 27335 | 13745 |
| 1737+47 | TO | 1738+65 | 367.1 | | | 165.2 | | | 46.3 | 27571 | 118 |
| TOTAL: | | | 51375 | 9876 | 1807 | 29785 | 553 | 830 | 6473 | 62693 | 22143 |

| SHOULDER RESURFACING | | | | | | | | | |
|----------------------|----|---------|-----------------------------|-----------------------------|----------------------------------|---------------------------|--------------------------------|-----------------------------------|---------------------------------|
| LOCATION | | | HMA SURFACE REMOVAL, 1 3/4" | HMA SURFACE REMOVAL, 2 1/2" | BITUMINOUS MATERIALS (TACK COAT) | HOT MIX ASPHALT SHOULDERS | HOT MIX ASPHALT SHOULDERS, 6"* | AGGREGATE WEDGE SHOULDER, TYPE B* | SHOULDER RUMBLE STRIPS, 8 INCH* |
| | | | SQ YD | SQ YD | POUND | TON | SQ YD | TON | FOOT |
| STA | TO | STA | | | | | | | |
| 1523+40 | TO | 1536+59 | 835.3 | | 375.9 | 81.9 | | 133.1 | |
| 1536+59 | TO | 1537+14 | 18.3 | | 8.3 | 1.8 | | 3.1 | |
| 1538+53 | TO | 1541+39 | 211.1 | | 95.0 | 20.7 | | 31.2 | |
| 1543+31 | TO | 1549+11 | | 1230.9 | 553.9 | 172.3 | | 61.5 | |
| 1550+18 | TO | 1556+41 | | 1385.2 | 623.4 | 193.9 | | 69.3 | |
| 1558+97 | TO | 1565+27 | | 1399.8 | 629.9 | 196.0 | | 68.9 | |
| 1566+66 | TO | 1572+17 | | 1224.4 | 551.0 | 171.4 | | 60.1 | |
| 1572+17 | TO | 1576+76 | 1020.0 | | 459.0 | 100.0 | | 43.2 | |
| 1576+76 | TO | 1596+50 | | | | | 1247.7 | 196.6 | 3743.0 |
| 1596+50 | TO | 1598+47 | 131.1 | | 59.0 | 12.8 | | 21.8 | 393.3 |
| 1600+02 | TO | 1602+00 | 132.2 | | 59.5 | 13.0 | | 22.0 | 396.7 |
| 1602+00 | TO | 1713+22 | | | | | 7325.3 | 1194.2 | 21976.0 |
| 1712+87 | TO | 1738+65 | | | | | | 211.8 | |
| RAMP A | | | | 296.7 | 133.5 | 41.5 | | | |
| RAMP B | | | | 256.7 | 115.5 | 35.9 | | | |
| RAMP C | | | | 272.2 | 122.5 | 38.1 | | | |
| RAMP D | | | | 278.9 | 125.5 | 39.0 | | | |
| TOTAL: | | | 2348 | 6345 | 3912 | 1118 | 8573.0 | 2117 | 26509 |

*INCLUDES SIDEROAD OMISSIONS

| EARTH EXCAVATION | | | | |
|------------------|----|---------|-------|------------------|
| LOCATION | | | NB/SB | EARTH EXCAVATION |
| STA | TO | STA | | CU YD |
| 1576+76 | TO | 1607+95 | NB | 96.0 |
| 1608+78 | TO | 1660+23 | NB | 308.7 |
| 1660+23 | TO | 1713+90 | NB | 284.3 |
| 1713+90 | TO | 1714+30 | NB | 2.4 |
| TOTAL: | | | | |
| 1576+76 | TO | 1579+57 | SB | 15.1 |
| 1580+38 | TO | 1606+58 | SB | 71.6 |
| 1607+23 | TO | 1659+79 | SB | 245.3 |
| 1660+59 | TO | 1713+90 | SB | 228.9 |
| 1713+90 | TO | 1714+30 | SB | 2.6 |
| TOTAL: | | | | |
| | | | | 1254.9 |

| ENTRANCE SCHEDULE | | | | | | | | | |
|-------------------|-------|--------|-------|----------------------------------|--------------------------|--|----------------------|---------------------------|--------------------------------|
| LOCATION | | LENGTH | WIDTH | BITUMINOUS MATERIALS (TACK COAT) | INCIDENTAL HMA SURFACING | HIGH-EARLY-STRENGTH PORTLAND CEMENT CONCRETE PAVEMENT, 10" | PIPE CULVERT REMOVAL | WELDED WIRE REINFORCEMENT | AGGREGATE FOR TEMPORARY ACCESS |
| STATION | RT/LT | FOOT | FOOT | POUND | TON | SQ YD | FOOT | SQ YD | TON |
| 1524+11 | RT | 57 | 3 | 8.6 | 0.53 | | | | |
| 1527+06 | LT | 53 | 3 | 8.0 | 0.49 | | | | |
| 1531+60 | RT | 132 | 3 | 19.8 | 1.23 | | | | |
| 1537+67 | RT | 91 | 3 | 13.7 | 0.85 | 426.7 | 105.0 | 426.7 | 237.0 |
| 1537+67 | LT | 172 | 3 | 25.8 | 1.61 | | | | |
| 1543+96 | RT | 44 | 3 | 6.6 | 0.41 | | | | |
| 1572+55 | RT | 141 | 3 | 21.2 | 1.32 | | | | |
| 1580+25 | RT | 48 | 3 | 7.2 | 0.45 | | | | |
| 1588+62 | RT | 120 | 3 | 18.0 | 1.12 | | | | |
| 1588+62 | LT | 20 | 3 | 3.0 | 0.19 | | | | |
| 1591+40 | RT | 45 | 3 | 6.8 | 0.42 | | | | |
| 1610+60 | LT | 40 | 3 | 6.0 | 0.37 | | | | |
| 1613+23 | RT | 35 | 3 | 5.3 | 0.33 | | | | |
| 1626+62 | RT | 33 | 3 | 5.0 | 0.31 | | | | |
| 1638+69 | LT | 25 | 3 | 3.8 | 0.23 | | | | |
| 1646+70 | RT | 26 | 3 | 3.9 | 0.24 | | | | |
| 1662+74 | LT | 170 | 3 | 25.5 | 1.59 | | | | |
| 1663+76 | RT | 20 | 3 | 3.0 | 0.19 | | | | |
| 1672+94 | RT | 30 | 3 | 4.5 | 0.28 | | | | |
| 1673+70 | LT | 25 | 3 | 3.8 | 0.23 | | | | |
| 1686+45 | RT | 22 | 3 | 3.3 | 0.21 | | | | |
| 1687+04 | RT | 30 | 3 | 4.5 | 0.28 | | | | |
| 1705+57 | RT | 25 | 3 | 3.8 | 0.23 | | | | |
| 1714+04 | LT | 40 | 3 | 6.0 | 0.37 | | | | |
| 1715+68 | LT | 33 | 3 | 5.0 | 0.31 | | | | |
| 1716+09 | LT | 20 | 3 | 3.0 | 0.19 | | | | |
| 1716+09 | RT | 27 | 3 | 4.1 | 0.25 | | | | |
| 1717+01 | RT | 27 | 3 | 4.1 | 0.25 | | | | |
| 1717+25 | LT | 40 | 3 | 6.0 | 0.37 | | | | |
| 1718+03 | RT | 51 | 3 | 7.7 | 0.48 | | | | |
| 1719+67 | LT | 16 | 3 | 2.4 | 0.15 | | | | |
| 1723+13 | RT | 30 | 3 | 4.5 | 0.28 | | | | |
| 1723+68 | LT | 15 | 3 | 2.3 | 0.14 | | | | |
| 1723+90 | RT | 28 | 3 | 4.2 | 0.26 | | | | |
| 1724+79 | RT | 48 | 3 | 7.2 | 0.45 | | | | |
| 1725+37 | LT | 25 | 3 | 3.8 | 0.23 | | | | |
| 1728+05 | LT | 92 | 3 | 13.8 | 0.86 | | | | |
| 1728+05 | RT | 100 | 3 | 15.0 | 0.93 | | | | |
| 1730+31 | RT | 45 | 3 | 6.8 | 0.42 | | | | |
| 1733+11 | LT | 23 | 3 | 3.5 | 0.21 | | | | |
| 1734+02 | RT | 51 | 3 | 7.7 | 0.48 | | | | |
| 1734+63 | LT | 32 | 3 | 4.8 | 0.30 | | | | |
| 1735+01 | RT | 40 | 3 | 6.0 | 0.37 | | | | |
| 1735+31 | LT | 34 | 3 | 5.1 | 0.32 | | | | |
| 1736+27 | RT | 40 | 3 | 6.0 | 0.37 | | | | |
| 1736+27 | LT | 17 | 3 | 2.6 | 0.16 | | | | |
| 1736+88 | RT | 27 | 3 | 4.1 | 0.25 | | | | |
| 1737+45 | LT | 50 | 3 | 7.5 | 0.47 | | | | |
| 1738+16 | LT | 15 | 3 | 2.3 | 0.14 | | | | |
| 1738+30 | RT | 25 | 3 | 3.8 | 0.23 | | | | |
| TOTAL: | | | | 359.3 | 22.4 | 426.7 | 105 | 426.7 | 237 |

| SIDEROAD SCHEDULE | | | | | | |
|-------------------|---------|-------|--------|-------|----------------------------------|--------------------------|
| LOCATION | | | LENGTH | WIDTH | BITUMINOUS MATERIALS (TACK COAT) | INCIDENTAL HMA SURFACING |
| NAME | STATION | RT/LT | FOOT | FOOT | POUND | TON |
| LARK RD | 1580+25 | LT | 75 | 3 | 11.3 | 0.70 |
| WREN RD | 1606+92 | LT | 63 | 3 | 9.5 | 0.59 |
| CH 22 | 1608+48 | RT | 80 | 3 | 12.0 | 0.75 |
| LIBERTY SCHOOL RD | 1660+23 | LT | 77 | 3 | 11.6 | 0.72 |
| PHEASANT RD | 1700+60 | LT | 48 | 3 | 7.2 | 0.45 |
| EAST SOUTH ST | 1714+70 | RT | 72 | 3 | 10.8 | 0.67 |
| EAST FRONT ST | 1719+15 | RT | 47 | 3 | 7.1 | 0.44 |
| SECOND ST | 1721+90 | RT/LT | 122 | 3 | 18.3 | 1.14 |
| WEST NORTH ST | 1726+93 | LT | 50 | 3 | 7.5 | 0.47 |
| EAST NORTH ST | 1727+36 | RT | 32 | 3 | 4.8 | 0.30 |
| TOTAL: | | | | | 99.9 | 6.2 |

| BUTT JOINT SCHEDULE | | | | | |
|---------------------|-------------------|--------|--------|----------------------------------|----------------|
| STATION | LOCATION | LENGTH | WIDTH | HMA SURFACE REMOVAL - BUTT JOINT | TEMPORARY RAMP |
| | | (FEET) | (FEET) | SQ YD | SQ YD |
| 1523+40 | IL 127 | 10 | 30 | 33.3 | 19.4 |
| 1541+39 | IL 127 | 10 | 40 | 44.4 | 25.9 |
| 1543+31 | IL 127 | 12 | 49 | 65.3 | 45.4 |
| 1556+41 | IL 127 | 12 | 23 | 30.7 | 21.3 |
| 1556+41 | IL 127 | 12 | 23 | 30.7 | 21.3 |
| 1558+97 | IL 127 | 12 | 23 | 30.7 | 21.3 |
| 1558+97 | IL 127 | 12 | 23 | 30.7 | 21.3 |
| 1572+12 | IL 127 | 10 | 51 | 56.7 | 14.2 |
| 1580+25 | LARK RD | 75 | 3 | 25.0 | 48.6 |
| 1598+47 | IL 127 | 10 | 30 | 33.3 | 19.4 |
| 1600+02 | IL 127 | 10 | 30 | 33.3 | 19.4 |
| 1606+92 | WREN RD | 63 | 3 | 21.0 | 40.8 |
| 1608+48 | CH 22 | 80 | 3 | 26.7 | 51.9 |
| 1660+23 | LIBERTY SCHOOL RD | 77 | 3 | 25.7 | 49.9 |
| 1700+60 | PHEASANT RD | 48 | 3 | 16.0 | 31.1 |
| 1714+70 | EAST SOUTH ST | 72 | 3 | 24.0 | 46.7 |
| 1719+15 | EAST FRONT ST | 47 | 3 | 15.7 | 30.5 |
| 1721+90 | SECOND ST | 62 | 3 | 20.7 | 40.2 |
| 1721+90 | SECOND ST | 60 | 3 | 20.0 | 38.9 |
| 1726+93 | WEST NORTH ST | 50 | 3 | 16.7 | 32.4 |
| 1727+36 | EAST NORTH ST | 32 | 3 | 10.7 | 20.7 |
| 1738+65 | IL 127 | 10 | 32 | 35.6 | 20.7 |
| TOTAL: | | | | 646.7 | 681.4 |

| PAVEMENT PATCHING | | | | | | | |
|----------------------------|----------|--------|-------|--------|---------|----------|---------|
| STATION | LOCATION | LENGTH | WIDTH | 12" | | | |
| | | | | TYPE I | TYPE II | TYPE III | TYPE IV |
| | | | | FEET | FEET | SQ YD | SQ YD |
| 1563+22 | NB | 10 | 12 | | 13.3 | | |
| 1576+70 | NB | 30 | 12 | | | | 40.0 |
| 1581+60 | NB | 6 | 12 | | 8.0 | | |
| 1595+02 | NB | 10 | 12 | | 13.3 | | |
| 1613+03 | NB | 30 | 12 | | | | 40.0 |
| 1624+67 | NB | 45 | 12 | | | | 60.0 |
| 1625+25 | NB | 30 | 12 | | | | 40.0 |
| 1628+15 | NB | 10 | 12 | | 13.3 | | |
| 1628+30 | NB | 20 | 12 | | | | 26.7 |
| 1629+30 | NB | 30 | 12 | | | | 40.0 |
| 1644+80 | NB | 6 | 12 | | 8.0 | | |
| 1648+01 | NB | 40 | 12 | | | | 53.3 |
| 1648+42 | NB | 40 | 6 | | | | 26.7 |
| 1664+00 | NB | 30 | 12 | | | | 40.0 |
| 1664+40 | NB | 25 | 6 | | | 16.7 | |
| 1664+65 | NB | 12 | 12 | | | 16.0 | |
| 1664+77 | NB | 25 | 6 | | | 16.7 | |
| 1671+00 | NB | 10 | 12 | | 13.3 | | |
| 1673+84 | NB | 20 | 12 | | | | 26.7 |
| 1575+00 | NB | 15 | 12 | | | 20.0 | |
| 1685+60 | NB | 60 | 6 | | | | 40.0 |
| 1686+20 | NB | 25 | 12 | | | | 33.3 |
| 1686+45 | NB | 40 | 6 | | | | 26.7 |
| 1686+90 | NB | 15 | 12 | | | 20.0 | |
| 1691+02 | NB | 60 | 6 | | | | 40.0 |
| 1697+50 | NB | 50 | 6 | | | | 33.3 |
| 1698+00 | NB | 6 | 12 | | 8.0 | | |
| 1701+00 | NB | 80 | 6 | | | | 53.3 |
| 1715+00 | NB | 80 | 6 | | | | 53.3 |
| SUB-TOTAL | | | | | 77.3 | 89.3 | 673.3 |
| ANTICIPATED FAILURES (15%) | | | | | 11.6 | 13.4 | 101 |
| TOTAL: | | | | | 88.9 | 102.7 | 774.3 |

| PAVEMENT PATCHING | | | | | | | |
|----------------------------|----------|--------|-------|--------|---------|----------|---------|
| STATION | LOCATION | LENGTH | WIDTH | 12" | | | |
| | | | | TYPE I | TYPE II | TYPE III | TYPE IV |
| | | | | FEET | FEET | SQ YD | SQ YD |
| 1539+25 | SB | 6 | 12 | | 8.0 | | |
| 1544+00 | SB | 30 | 6 | | | 20.0 | |
| 1546+11 | SB | 6 | 12 | | 8.0 | | |
| 1601+50 | SB | 6 | 12 | | 8.0 | | |
| 1612+50 | SB | 6 | 12 | | 8.0 | | |
| 1613+23 | SB | 60 | 6 | | | | 40.0 |
| 1617+75 | SB | 30 | 12 | | | | 40.0 |
| 1618+00 | SB | 60 | 6 | | | | 40.0 |
| 1626+62 | SB | 100 | 6 | | | | 66.7 |
| 1639+00 | SB | 60 | 6 | | | | 40.0 |
| 1646+00 | SB | 20 | 6 | | 13.3 | | |
| 1649+50 | SB | 25 | 6 | | | 16.7 | |
| 1650+00 | SB | 15 | 6 | | 10.0 | | |
| 1651+90 | SB | 10 | 12 | | 13.3 | | |
| 1652+00 | SB | 6 | 12 | | 8.0 | | |
| 1652+62 | SB | 40 | 6 | | | | 26.7 |
| 1659+00 | SB | 15 | 12 | | | 20.0 | |
| 1663+40 | SB | 30 | 6 | | | 20.0 | |
| 1664+00 | SB | 30 | 6 | | | 20.0 | |
| 1665+90 | SB | 6 | 12 | | 8.0 | | |
| 1666+50 | SB | 10 | 12 | | 13.3 | | |
| 1686+00 | SB | 10 | 12 | | 13.3 | | |
| 1687+20 | SB | 30 | 12 | | | | 40.0 |
| 1699+10 | SB | 20 | 6 | | 13.3 | | |
| 1701+50 | SB | 20 | 14 | | | | 31.1 |
| 1702+00 | SB | 20 | 12 | | | | 26.7 |
| 1714+50 | SB | 10 | 36 | | | | 40.0 |
| SUB-TOTAL | | | | | 124.7 | 96.7 | 391.1 |
| ANTICIPATED FAILURES (15%) | | | | | 18.7 | 14.5 | 58.7 |
| TOTAL: | | | | | 143.4 | 111.2 | 449.8 |
| GRAND TOTAL: | | | | | 232.3 | 213.9 | 1224.1 |

| FRENCH DRAIN SCHEDULE | | | | | | | | | |
|-----------------------|-------|--------|-------|-------|---|---------------|---------------------------------------|--------------|-----------------------|
| STATION | NB/SB | LENGTH | WIDTH | DEPTH | REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL | FRENCH DRAINS | GEOTECHNICAL FABRIC FOR FRENCH DRAINS | CURB REMOVAL | CONCRETE CURB, TYPE B |
| | | FEET | FEET | FEET | CU YD | CU YD | SQ YD | FOOT | FOOT |
| 1545+76 | SB | 4 | 35 | 1.0 | 5.2 | 5.2 | 15.6 | 4 | 4 |
| TOTAL: | | | | | 5.2 | 5.2 | 15.6 | 4 | 4 |

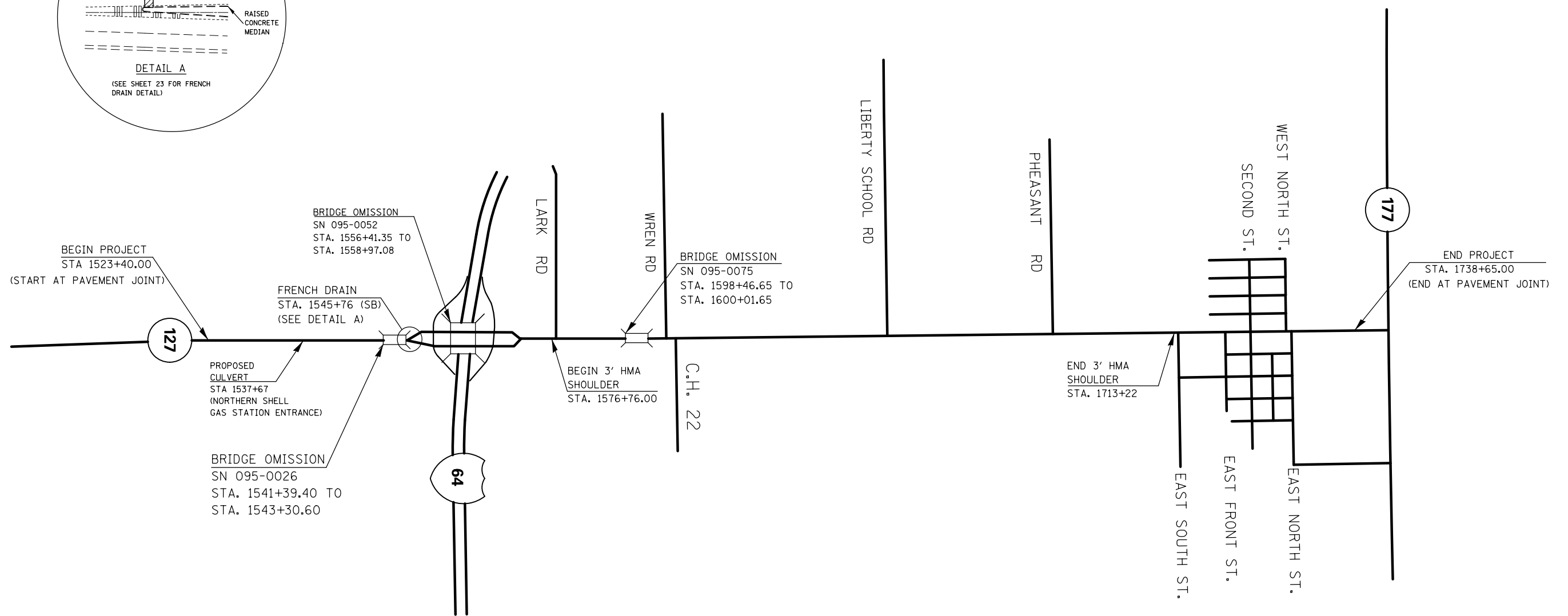
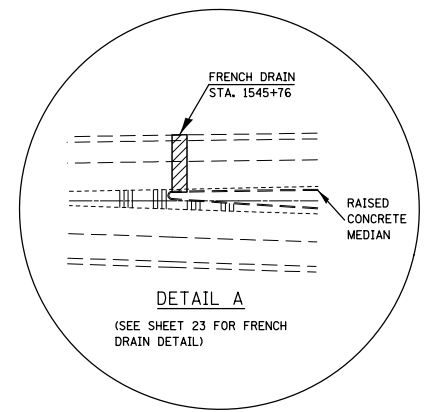
NOTE: SEE SHEET 23 FOR FRENCH DRAIN DETAIL

| CLASS B PAVEMENT PATCHING 12 INCH | | | | | | | | | | | |
|-----------------------------------|--------|------------|----------------------|-----------------------|------------------------|------------------------|-----------------------|----------|------------------------|---------------------------|--------|
| LOCATION | LENGTH | LANE WIDTH | CLASS B PATCH TYPE I | CLASS B PATCH TYPE II | CLASS B PATCH TYPE III | CLASS B PATCH TYPE IV* | DOWEL BARS 1 1/2 inch | TIE BARS | PATCHING REINFORCEMENT | WELDED WIRE REINFORCEMENT | SAWCUT |
| | (FT) | (FT) | SQ YD | SQ YD | SQ YD | SQ YD | EACH | EACH | SQ YD | SQ YD | FOOT |
| RAMP A | 200.0 | 16.0 | | | | 795.8 | 28 | 99 | 795.8 | 795.8 | 448.0 |
| RAMP B | 200.0 | 16.0 | | | | 651.1 | 28 | 99 | 651.1 | 651.1 | 448.0 |
| RAMP C | 200.0 | 16.0 | | | | 674.4 | 28 | 99 | 674.4 | 674.4 | 448.0 |
| RAMP D | 200.0 | 16.0 | | | | 755.8 | 28 | 99 | 755.8 | 755.8 | 448.0 |
| | | | | | | | | | | | |
| | | | | | | | | | | | |
| SUBTOTAL: | | | | | | 2081.3 | 84 | 297 | 2081.3 | 2081.3 | 1344.0 |
| TOTAL: | | | | | | 2081 | 84 | 297 | 2081 | 2081 | 1344 |

*AREA INCLUDES ENTIRE PAVEMENT TO BE REPLACED WITHIN CONCRETE APRON

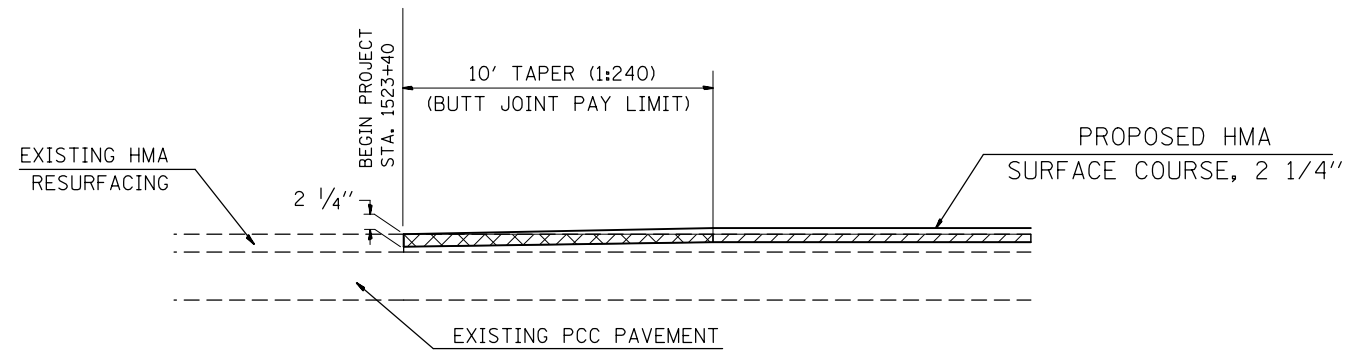
| LOCATION | THERMOPLASTIC PAVEMENT MARKING | | | | | | | | PAINT PAVEMENT MARKING | | | | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | RAISED REFLECTIVE PAVEMENT MARKER | SHORT TERM PAVEMENT MARKING | SHORT TERM PAVEMENT MARKING REMOVAL | PAINT CURB | | |
|--------------------|--------------------------------|------------------------|-----------------------|---------------------|-----------------------------|--------------|---------------------|-------------|------------------------|-----------------------|---------------------|-------------|---|-----------------------------------|-----------------------------|-------------------------------------|------------|------|-----|
| | LINE - 4" | | | | LINE - 12" | LINE - 24" | LETTERS AND SYMBOLS | LINE - 4" | | LINE - 24" | LETTERS AND SYMBOLS | | | | | | | | |
| | SOLID WHITE EDGELINE* | MEDIAN/CL SOLID YELLOW | TURN LANE SOLID WHITE | SOLID DOUBLE-YELLOW | SKIP-DASH YELLOW CENTERLINE | SOLID YELLOW | | SOLID WHITE | SOLID WHITE EDGELINE | SOLID YELLOW EDGELINE | | SOLID WHITE | | | | | | | |
| FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | SQ FT | FOOT | SQ FT | FOOT | SQ FT | EACH | EACH | FOOT | SQ FT | FOOT | | | |
| 1523+40 TO 1537+10 | 2573 | 1914 | 92 | 1388.0 | | 98 | 164.9 | | | | | | 68 | 68 | 498.2 | 166.1 | | | |
| 1537+10 TO 1541+39 | 581 | | 332 | 1179.6 | | 72.3 | 78.0 | | 25.0 | | | | 20 | 20 | 156.1 | 52.0 | | | |
| 1543+31 TO 1556+41 | 2383 | 2346 | 253 | 173.6 | | 102.2 | | | | | | | 6 | 6 | 924.7 | 308.2 | 174 | | |
| 1558+97 TO 1572+17 | 2401 | 2454 | 258 | | | | | | | | | | 6 | 6 | 931.1 | 310.4 | 174 | | |
| 1572+17 TO 1576+76 | 918 | 459 | | | 114.8 | | | | | | | | | | 156.9 | 52.3 | | | |
| 1576+76 TO 1598+47 | 4242 | | | | 542.7 | | | | | | | | 27 | 27 | 394.7 | 131.6 | | | |
| 1600+02 TO 1738+65 | 27099 | | | | 3465.8 | | | | | | | | 173 | 173 | 2520.6 | 840.2 | | | |
| RAMP A | | | | | | | | | 267 | 203 | 59 | 55.5 | | | | | | | |
| RAMP B | | | | | | | | | 231 | 211 | | | | | | | | | |
| RAMP C | | | | | | | | | 245 | 207 | | | | | | | | | |
| RAMP D | | | | | | | | | 251 | 206 | 50 | 55.5 | | | | | | | |
| SUB-TOTAL | | | 40197 | 7173 | 935 | 2741 | 4123 | 272 | 35 | 243 | 994 | 827 | 134 | 111 | 301 | 301 | 5582 | 1861 | 348 |
| TOTAL | | | 55169 | | | | 272 | 35 | 243 | 1821 | | 134 | 111 | 301 | 301 | 5582 | 1861 | 348 | |

*FINAL QUANTITY INCLUDES SIDEROAD OMISSIONS

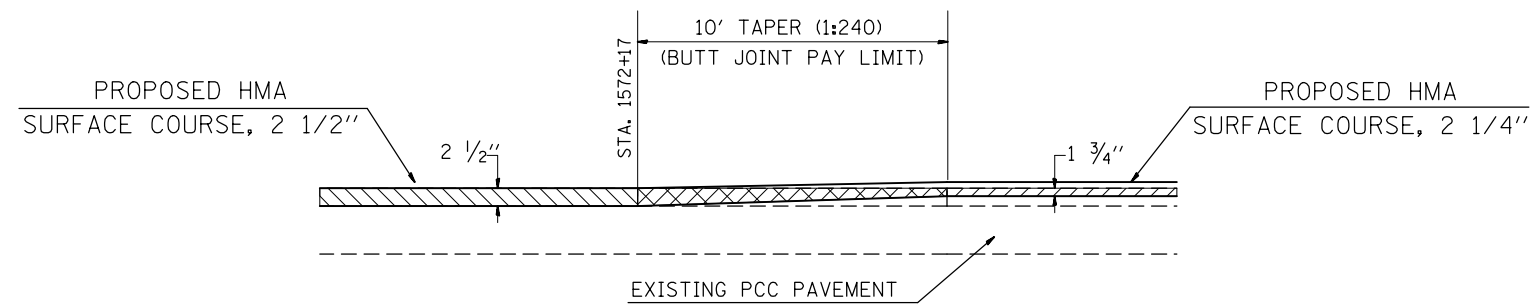


| SIDE ROAD | STATION |
|--------------------|-----------------|
| LARK RD. | 1580+24.64 (LT) |
| WREN RD. | 1606+91.86 (LT) |
| C.H. 22 | 1608+47.86 (RT) |
| LIBERTY SCHOOL RD. | 1660+23.16 (LT) |
| PHEASANT RD. | 1700+59.66 (LT) |
| EAST SOUTH ST. | 1714+70.20 (RT) |
| EAST FRONT ST. | 1719+14.52 (RT) |
| SECOND ST. | 1721+90.11 |
| WEST NORTH ST. | 1726+92.80 (LT) |
| EAST NORTH ST. | 1727+36.23 (RT) |

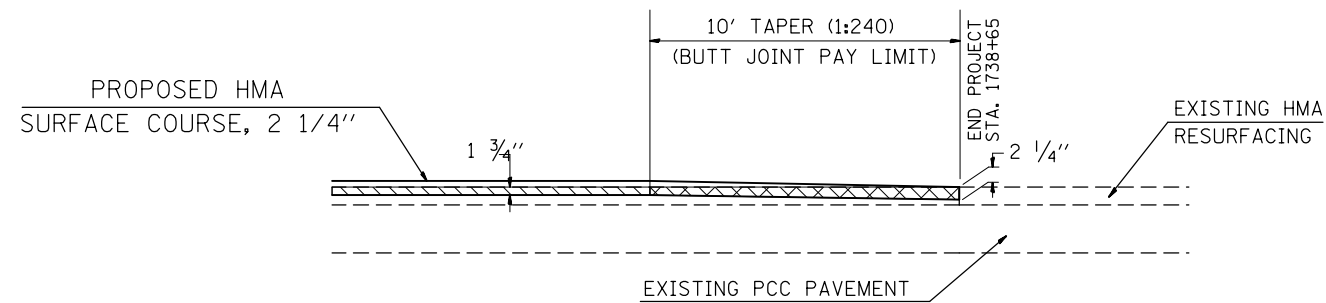
(NOT TO SCALE)



BEGIN BUTT JOINT DETAIL
ELEVATION VIEW

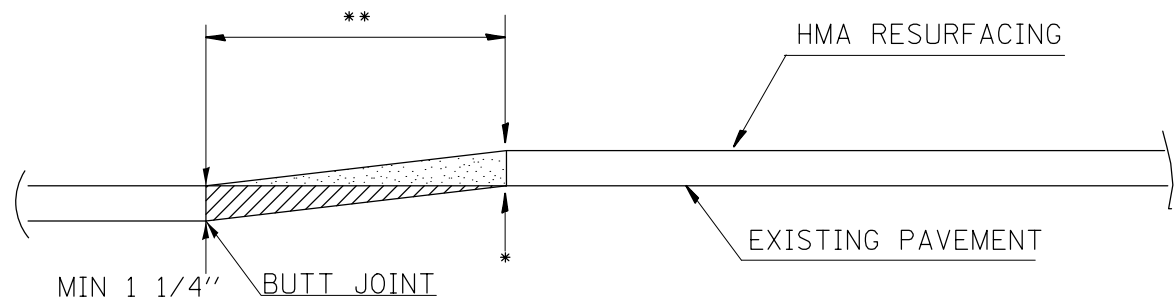


TRANSITION BUTT JOINT DETAIL
ELEVATION VIEW

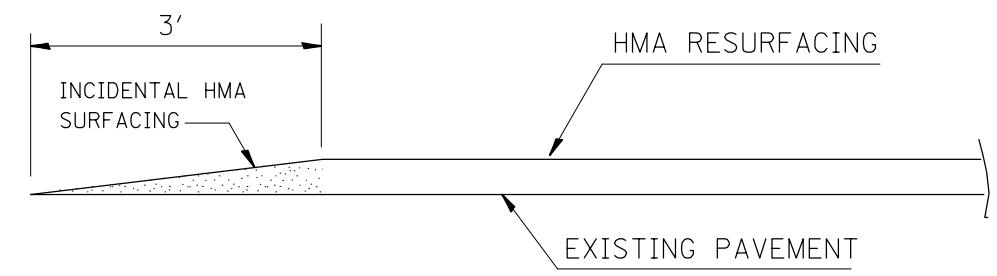


END BUTT JOINT DETAIL
ELEVATION VIEW

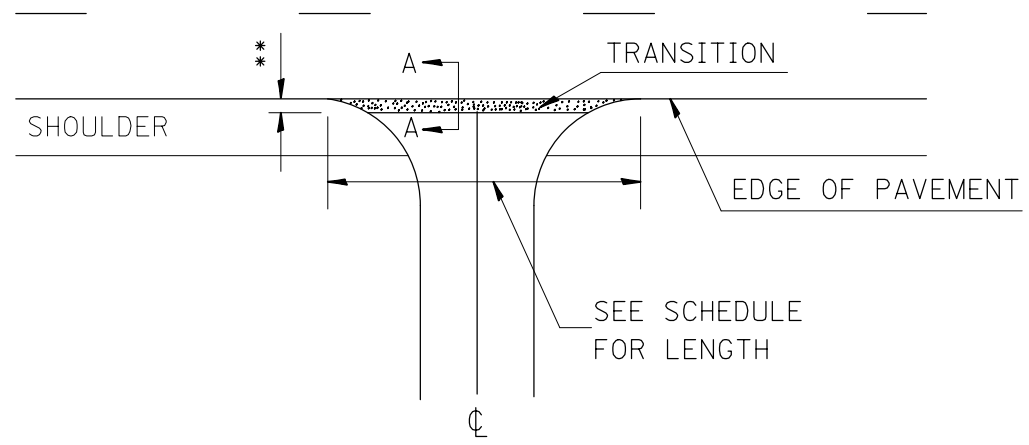
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|--|------------------------------|------------|-----------|---|---|--|--|--------------------|----------|------------|--------------|-----------|
| FILE NAME = | USER NAME = durosierj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | RESURFACING DETAILS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| pw:\11\084EBIDINTEG.illinois.gov\PIDOT\Documents\DOT Offices\District 8\Projects\0875\Drawings\LEADsheets\0875J10-sht-plan.dwg | PLotted | CHECKED - | REVISED - | | | | | 42 | 102-RS-3 | WASHINGTON | 26 | 17 |
| Default | PLOT SCALE = 100.0000' / in. | DATE - | REVISED - | | SCALE: SHEET 1 OF 1 SHEETS STA. TO STA. | | | CONTRACT NO. 76J10 | | | | |
| | PLOT DATE = 10/20/2016 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |



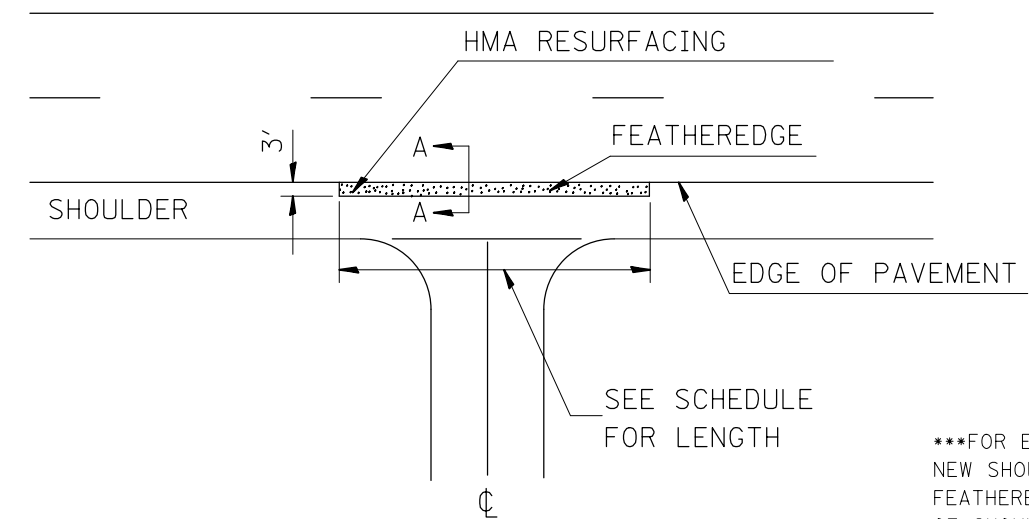
TRANSITION DETAIL
SECTION A-A
BUTT-JOINT ADDITION



FEATHEREDGE DETAIL
SECTION A-A



SIDEROAD DETAIL

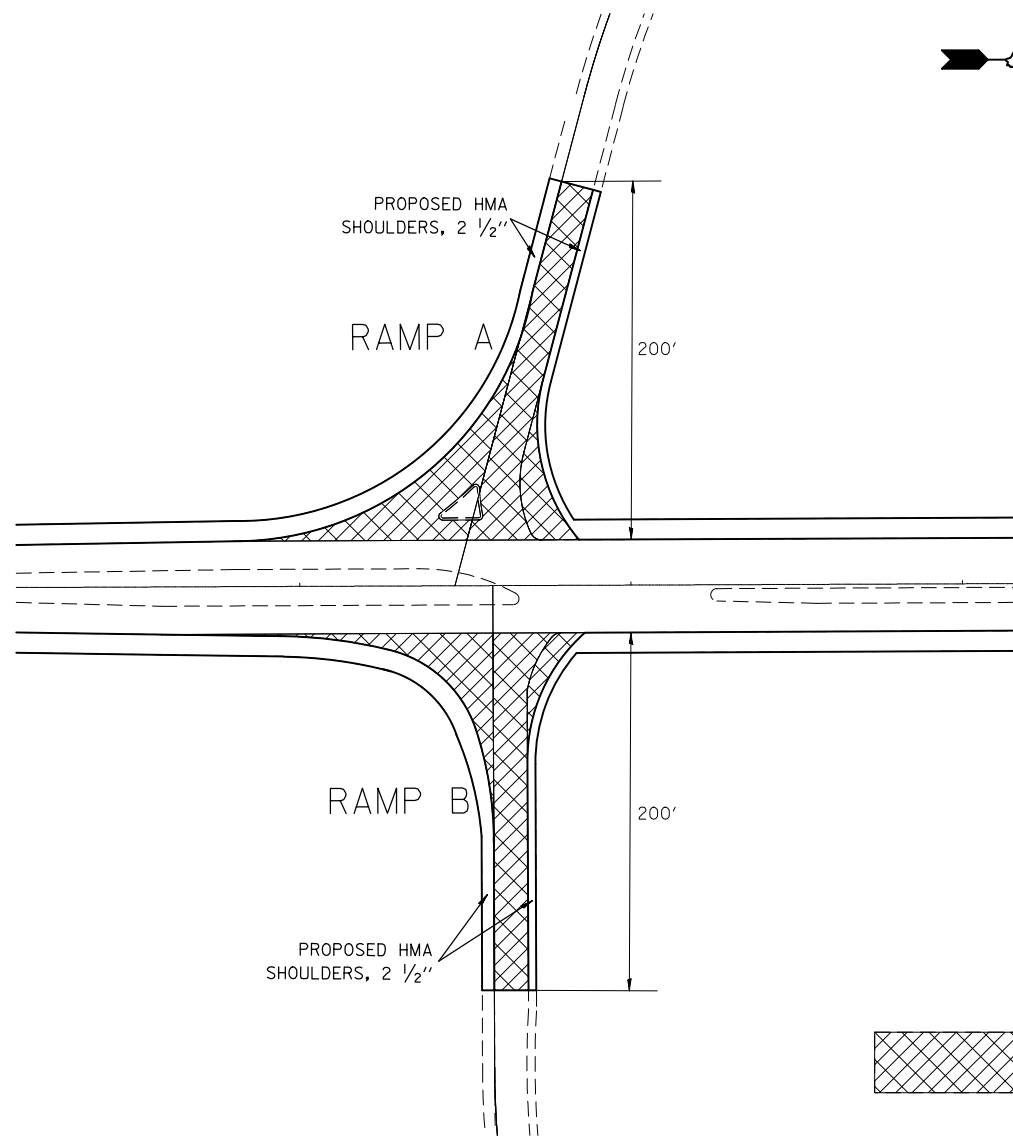


ENTRANCE DETAIL***

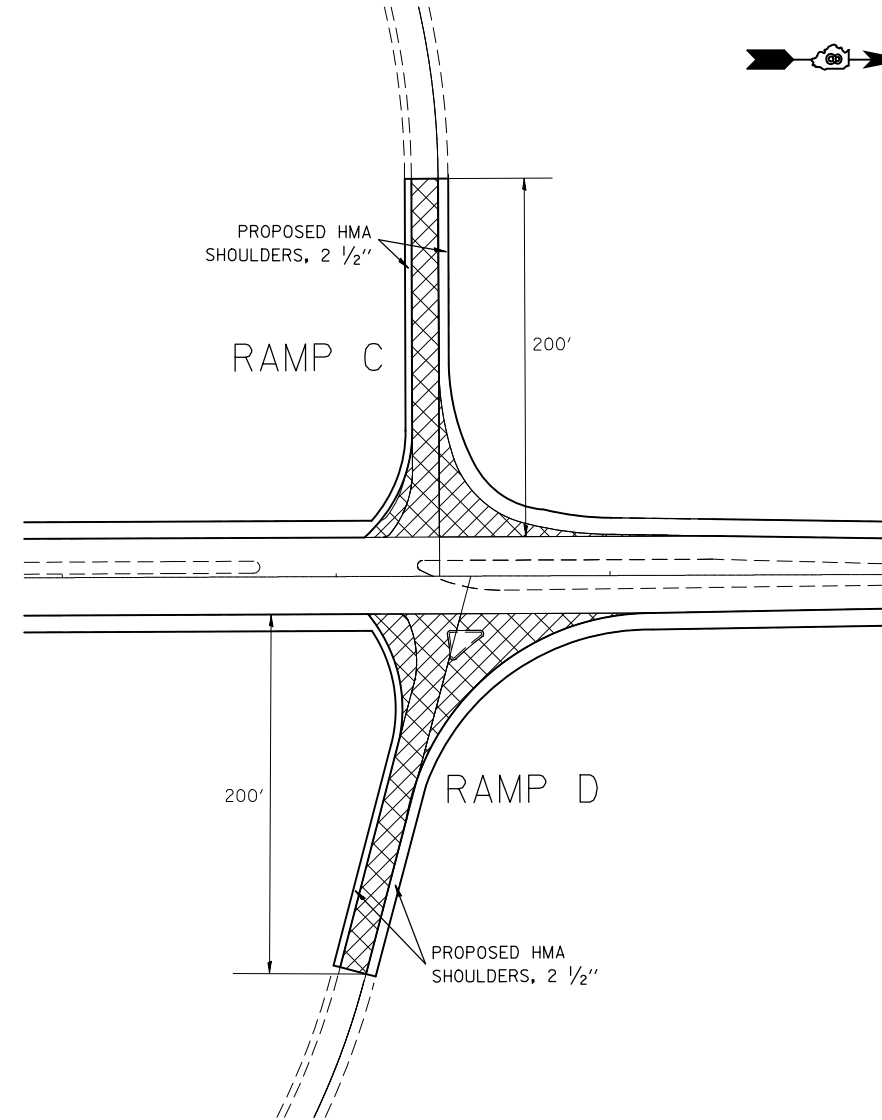
***FOR ENTRANCES WITHIN THE LIMITS OF NEW SHOULDER CONSTRUCTION, THE FEATHEREDGE SHALL BEGIN AT THE EDGE OF SHOULDER.

| *EOP ELEVATION CHANGE | **TOTAL TRANSITION LENGTH |
|-----------------------|---------------------------|
| 1/2" | 3.0' |
| 3/4" | 3.5' |
| 1" | 5' |
| 1-1/2" | 7.5' |
| 2" | 10' |
| 2-1/2" | 12.5' |
| 3" | 15' |
| 3-1/2" | 17.5' |
| 4" | 20' |
| 4-1/2" | 22.5' |
| 5" | 25' |
| 5-1/2" | 27.5' |
| 6" | 30' |

NOTE:
WHERE THE HMA TRANSITION IS MATCHING INTO AN EXISTING HMA SIDE ROAD SURFACE, A MILLED BUTT JOINT SHALL BE CONSTRUCTED WITHIN THE LIMITS OF THE TOTAL TRANSITION LENGTH ON THE LOCAL ROUTE.



SOUTH I-64 RAMP TERMINAL



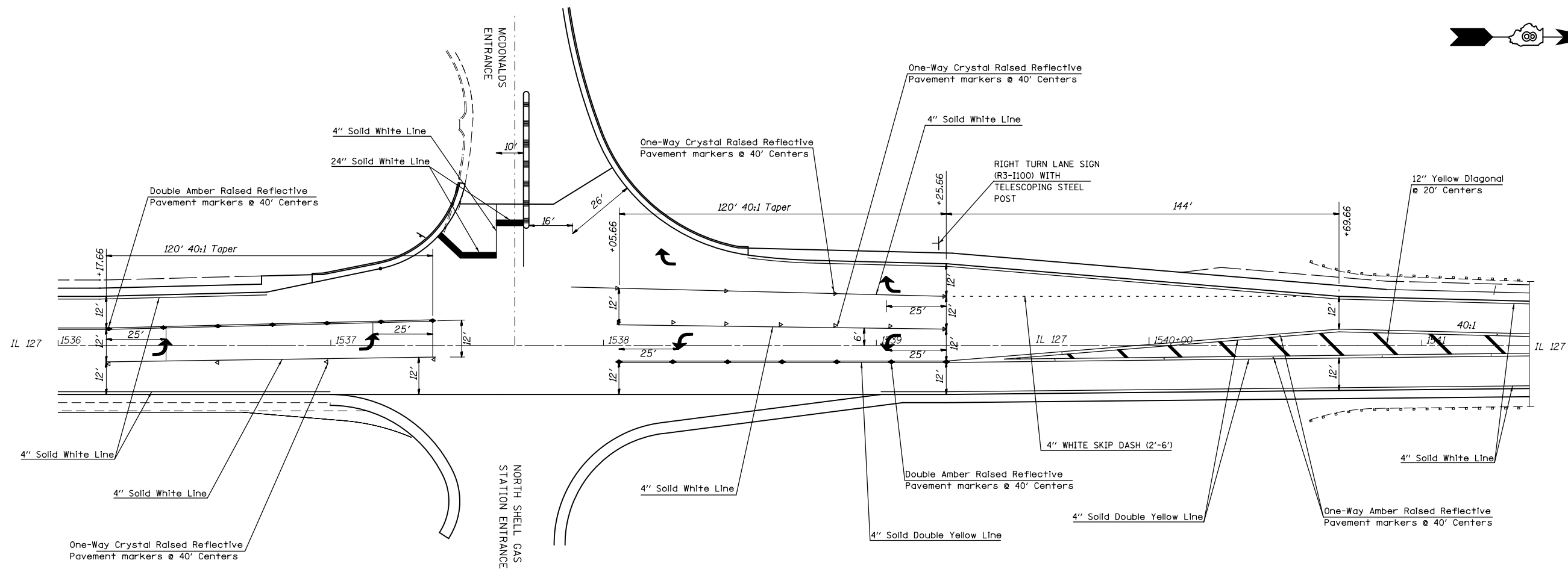
NORTH I-64 RAMP TERMINAL

 CLASS B PATCHING, 12''*

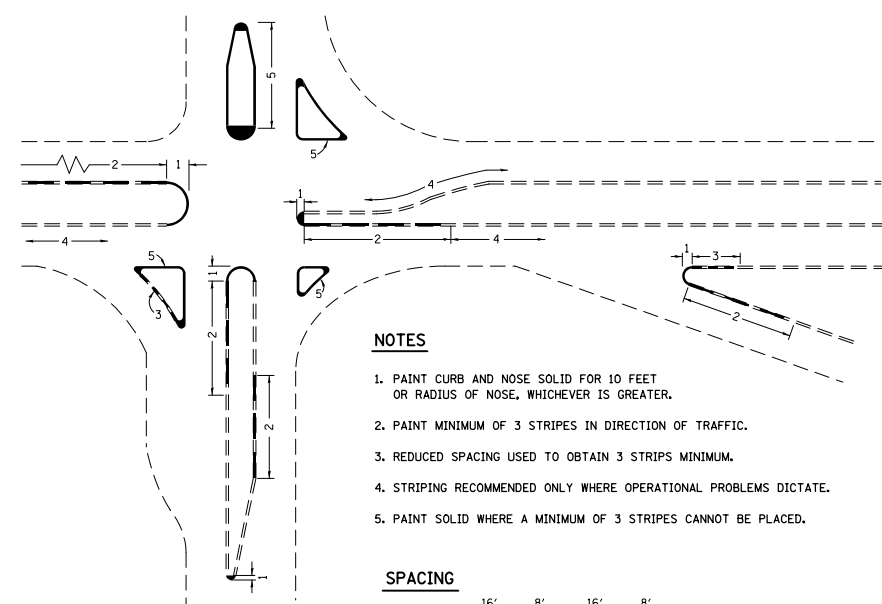
*MATCH EXISTING SLOPE

(NOT TO SCALE)

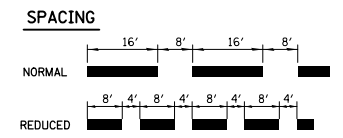
| RAMP | DESCRIPTION |
|--------|----------------------------|
| RAMP A | I-64 EB OFF-RAMP TO IL 127 |
| RAMP B | IL 127 TO I-64 EB ON-RAMP |
| RAMP C | IL 127 TO I-64 WB ON-RAMP |
| RAMP D | I-64 WB OFF-RAMP TO IL 127 |



INTERSECTION PAVEMENT MARKING DETAIL



- NOTES**
1. PAINT CURB AND NOSE SOLID FOR 10 FEET OR RADIUS OF NOSE, WHICHEVER IS GREATER.
 2. PAINT MINIMUM OF 3 STRIPES IN DIRECTION OF TRAFFIC.
 3. REDUCED SPACING USED TO OBTAIN 3 STRIPS MINIMUM.
 4. STRIPING RECOMMENDED ONLY WHERE OPERATIONAL PROBLEMS DICTATE.
 5. PAINT SOLID WHERE A MINIMUM OF 3 STRIPES CANNOT BE PLACED.



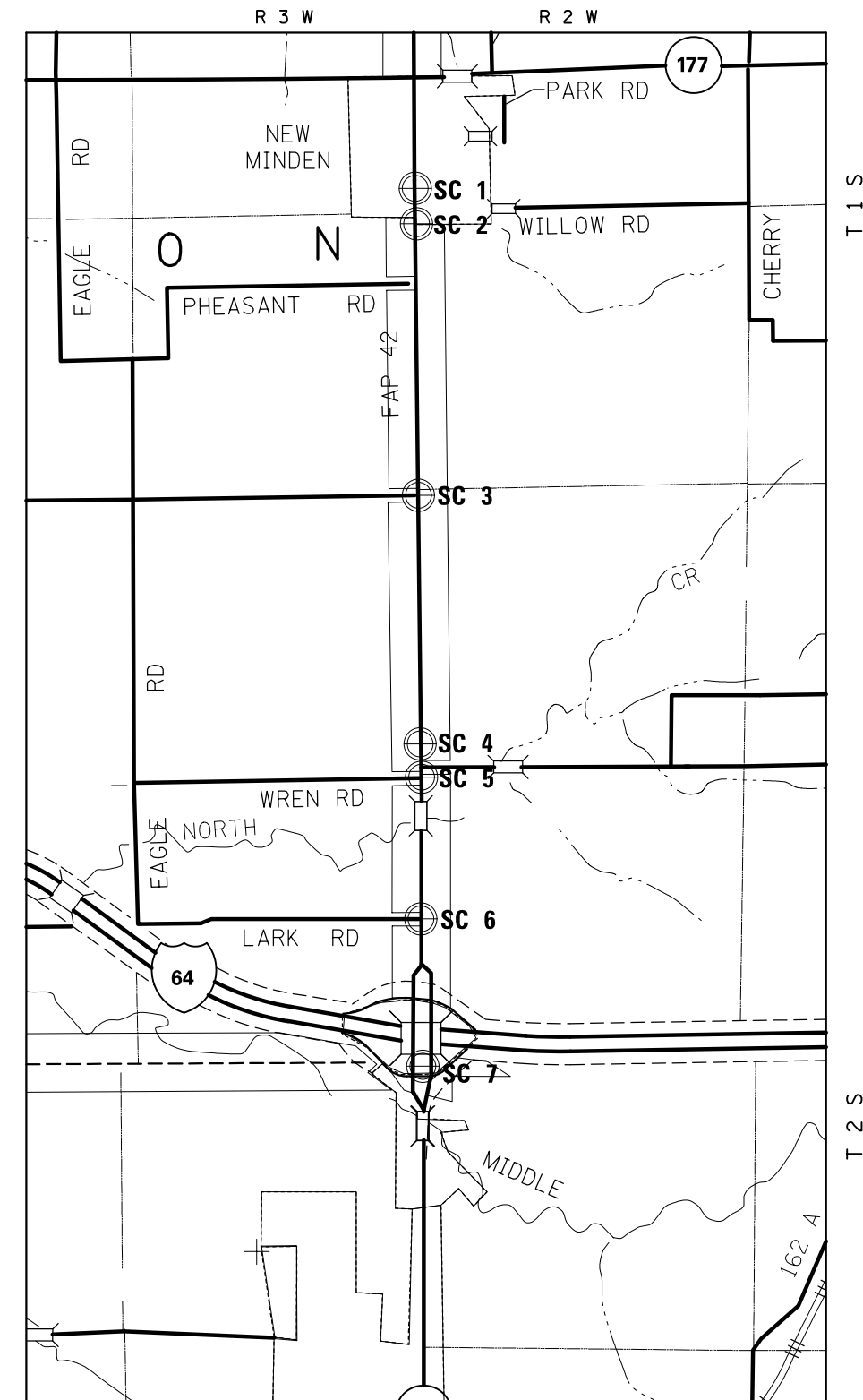
CURB MARKING

| | | | | | | | | | | | |
|---|-------------------------------|------------|-----------|---|--|--|--------------------|----------|------------|--------------|-----------|
| FILE NAME = | USER NAME = durosierj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | INTERSECTION PAVEMENT MARKING DETAILS | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| pw:\IL\084EBIDINTEG\illinois.gov\PIDOT\Documents\IDOT Offices\District 8\Projects\0875\Drawings\LEADsheets\0876J10-shr-plan.dwg | PLotted | CHECKED - | REVISED - | | | | 42 | 102-RS-3 | WASHINGTON | 26 | 20 |
| Default | PLOT SCALE = 100.0000' / 1in. | DATE - | REVISED - | | SCALE: SHEET 1 OF 1 SHEETS STA. TO STA. | | CONTRACT NO. 76J10 | | | | |
| | PLOT DATE = 10/20/2016 | | | | ILLINOIS FED. AID PROJECT | | | | | | |

NOTES:

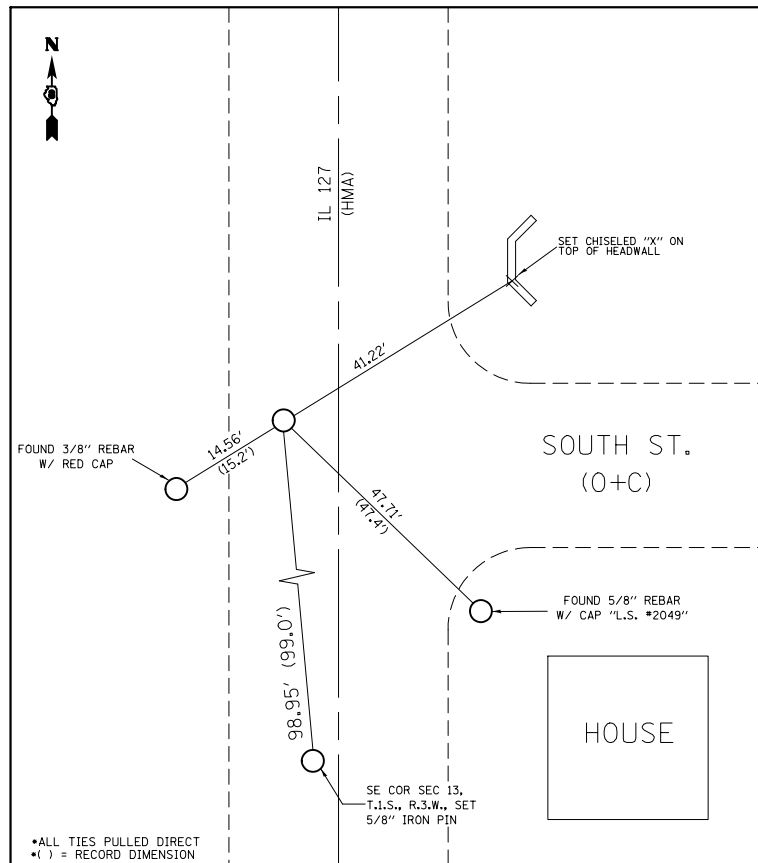
SECTION CORNERS SHALL BE SET IN ACCORDANCE SPECIAL PROVISION.

| SECTION CORNER COORDINATE TABLE | | |
|---------------------------------|---|--|
| POINT NO. | DESCRIPTION | CORNER |
| SC 1 (5) | FOUND 1/2" IRON PIN | SW COR SEC 18, T.1.S., R.2.W. |
| SC 2 (8) | SET 5/8" IRON PIN | SE COR SEC 13, T.1.S., R.3.W. |
| SC 3 (6) | FOUND PK NAIL LYING SIDWAYS UNDER MONUMENT COVER ASSEMBLY (SHOT CENTER OF PLUG) | NE COR SEC 25, T.1.S., R.3.W. |
| SC 4 (9) | FOUND IRON PIN W/ BRASS CAP STAMP #2065 | NW COR SEC 31, T.1.S., R.2.W. |
| SC 5 (4) | FOUND NAIL W/ CHISELED "X" ON IT | SE COR SEC 25, T.1.S., R.3.W. |
| SC 6 (3) | FOUND BRASS PLUG INSIDE VAULT | W 1/4 COR SEC 31, T.1.S., R.2.W. |
| SC 7 (10) | FOUND IRON PIN, POSSIBLY W/ ALUMINUM CAP, UNDER MONUMENT COVER ASSEMBLY | SW COR SEC 31, T.1.S., R.2.W. NW COR SEC 6, T.2.S., R.2.W. NE COR SEC 1, T.2.S., R.3.W. SE COR SEC 36, T.1.S., R.3.W. |

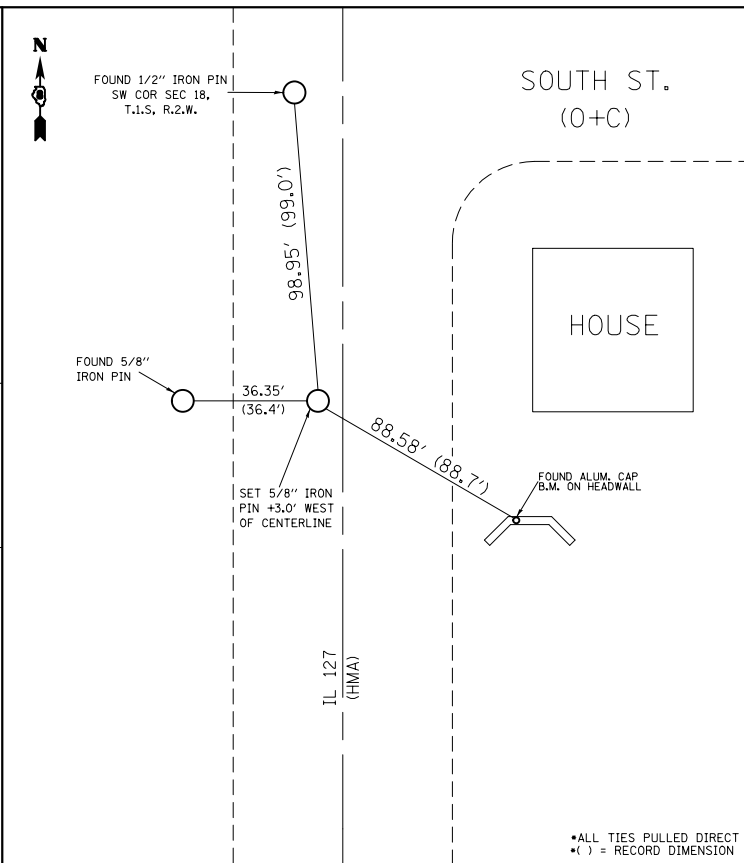


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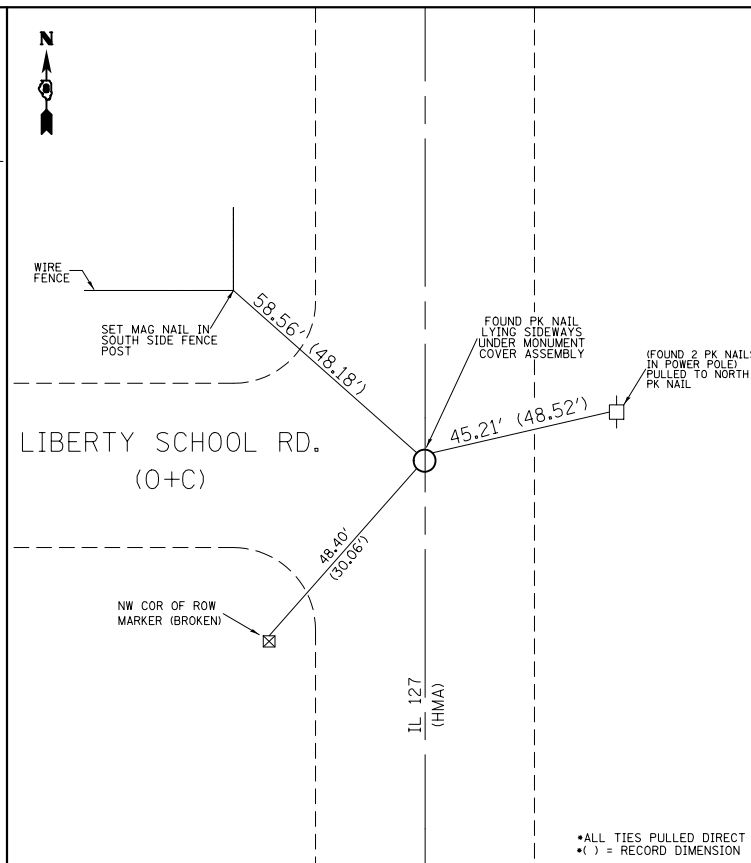
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|--|-------------------------------|------------|-----------|---|---------------------------|----------|-------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = durosierj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SURVEY MONUMENTS | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| pw:\11084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 8\Projects\0876\Drawings\AD\Drawings\087610-sht-plan.dwg | | DRAWN | REVISED | | 42 | 102-RS-3 | WASHINGTON | 26 | 21 | | |
| Default | PLOT SCALE = 100.0000' / 1in. | CHECKED - | REVISED - | | CONTRACT NO. 76J10 | | | | | | |
| | PLOT DATE = 10/20/2016 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | |



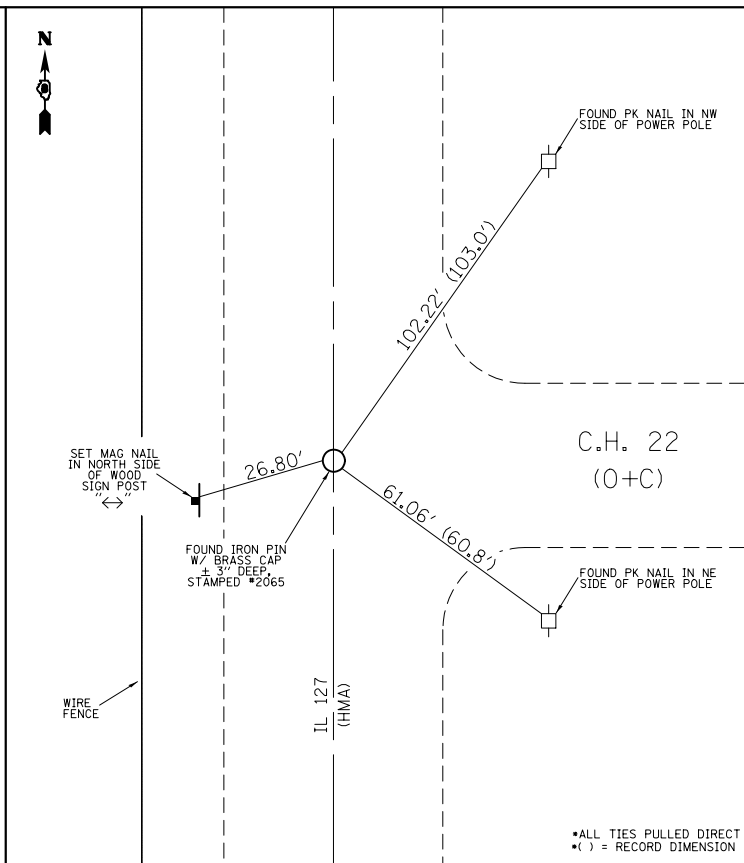
SC 1 SW COR SC 18, T.1.S., R.2.W.



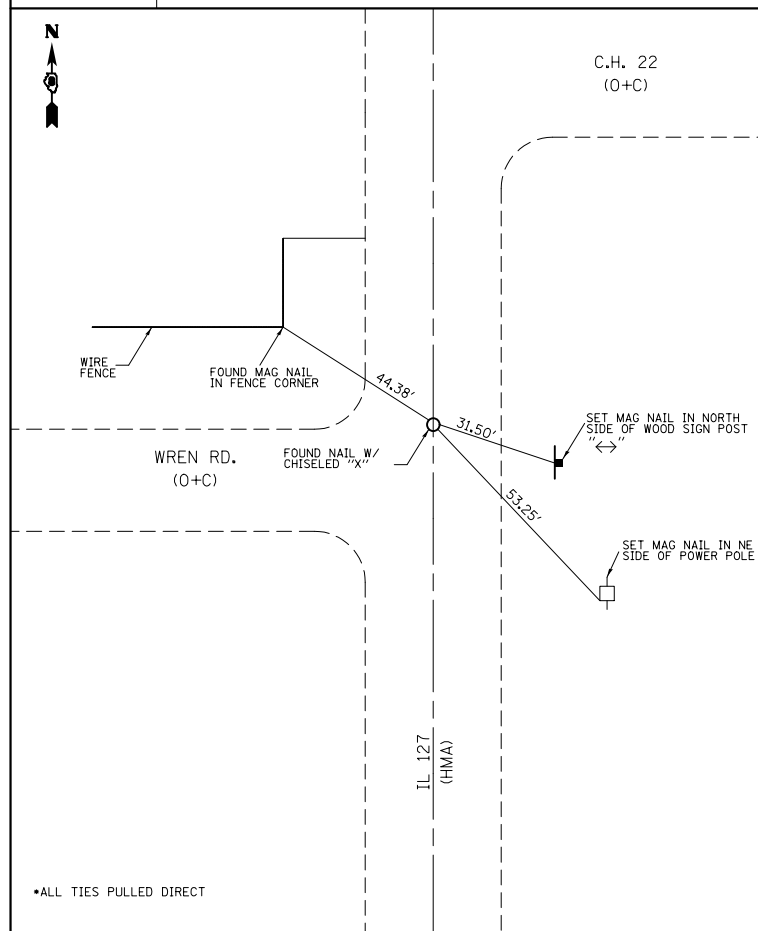
SC 2 SE COR SC 13, T.1.S., R.3.W.



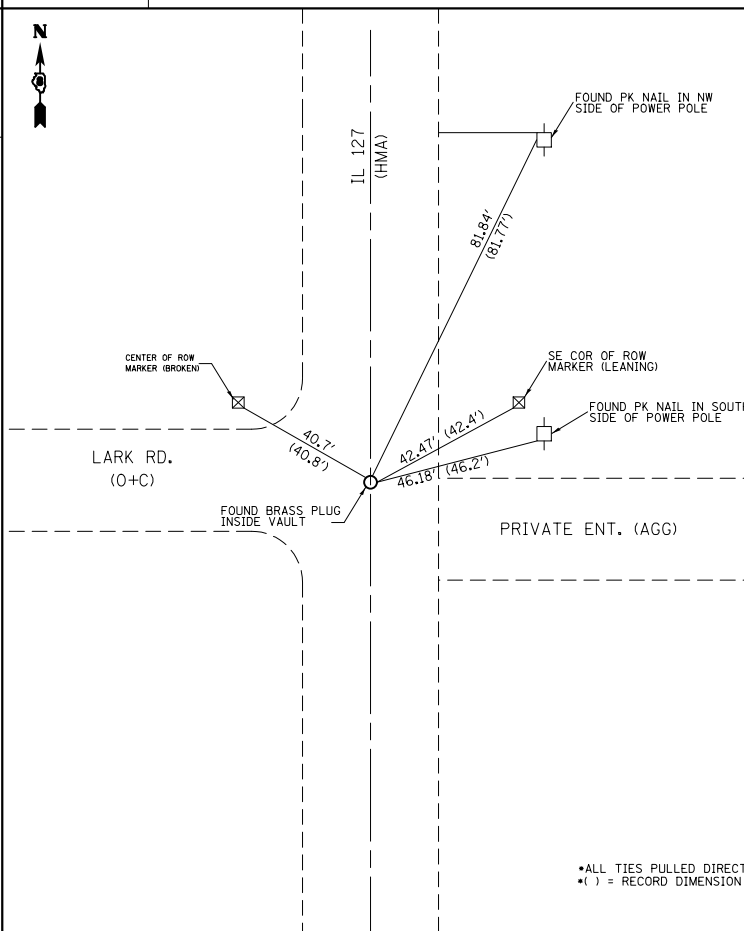
SC 3 NE COR SC 25, T.1.S., R.3.W.



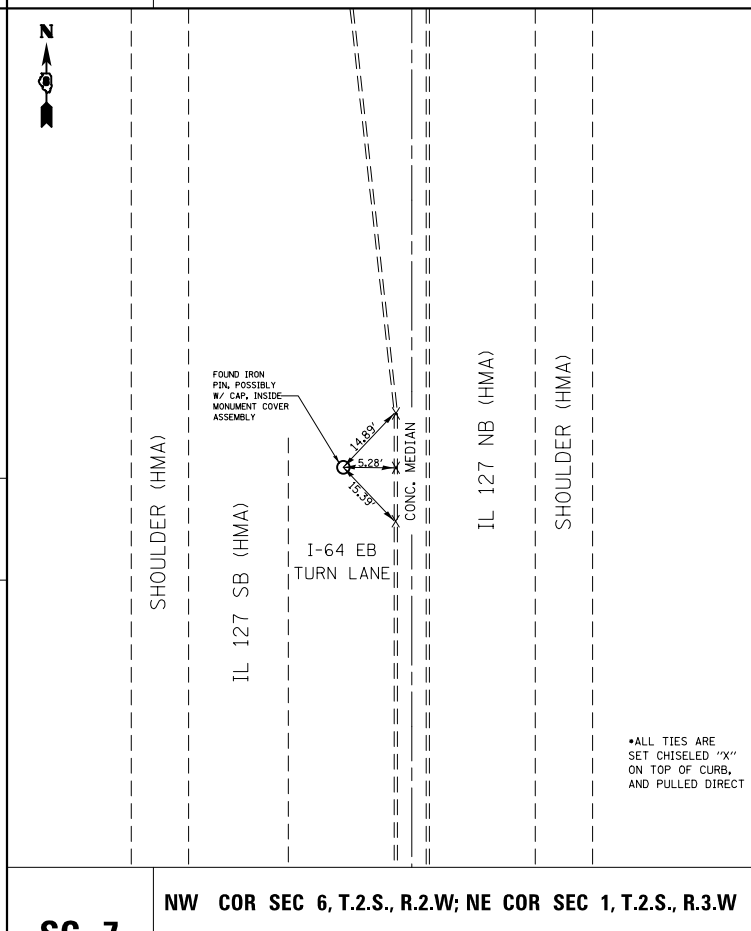
SC 4 NW COR SC 31, T.1.S., R.2.W.



SC 5 SE COR SEC 25, T.1.S., R.3.W., 3RD PM



SC 6 W 1/4 COR SEC 31, T.1.S., R.2.W.



**SC 7 NW COR SEC 6, T.2.S., R.2.W; NE COR SEC 1, T.2.S., R.3.W
SW COR SEC 31, T.1.S., R.2.W; SE COR SEC 36, T.1.S., R.3.W**

| | | | |
|---|------------------------|------------|-----------|
| FILE NAME = | USER NAME = durosier.j | DESIGNED - | REVISED - |
| pw:\IL084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 8\Projects\0875\DRAWING\DATA\LEADSHEETS\0876J10-sht-plan.dwg | | CHECKED - | REVISED - |
| Default | PLOT DATE = 10/20/2016 | DATE - | REVISED - |

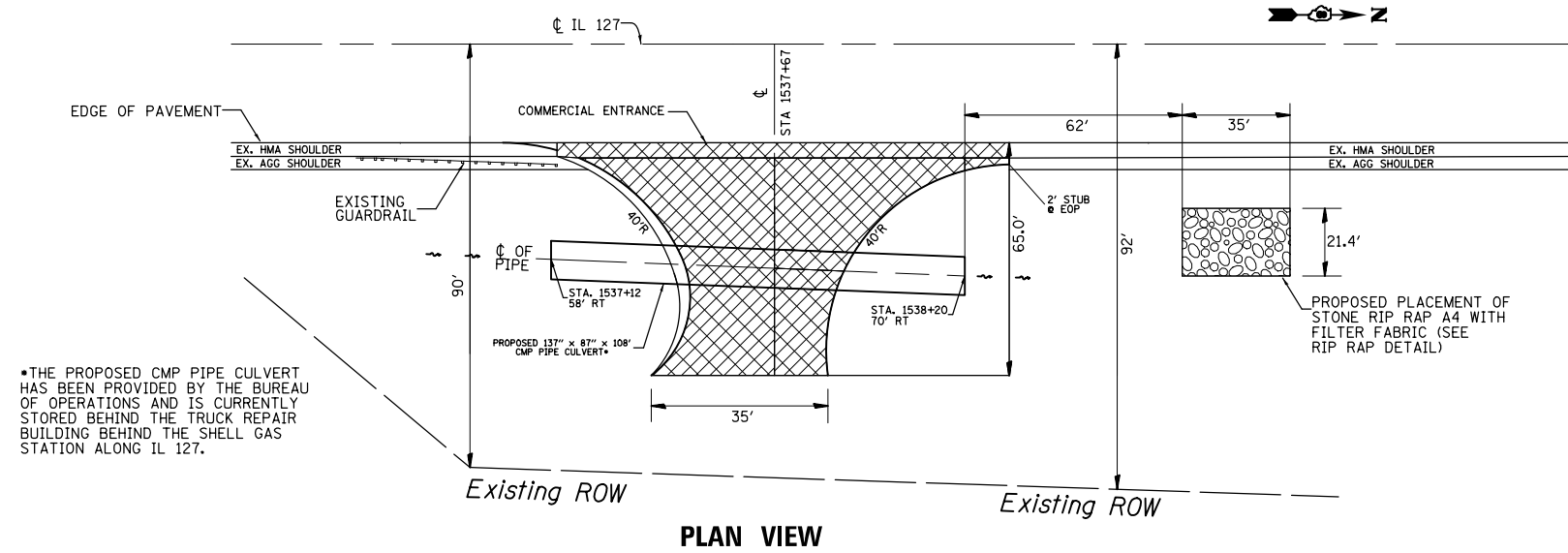
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SURVEY MONUMENTS

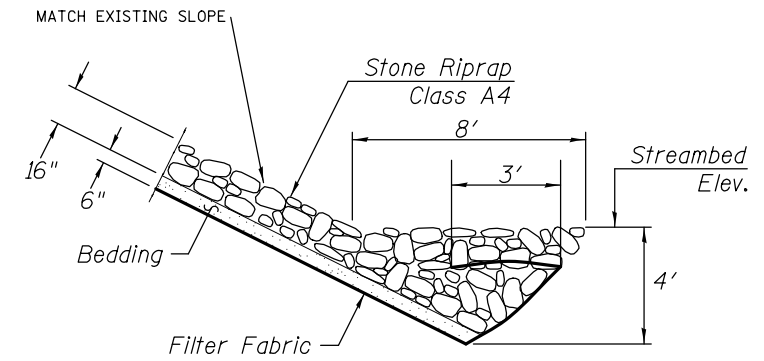
SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|------------|--------------|-----------|
| 42 | 102-RS-3 | WASHINGTON | 26 | 22 |
| CONTRACT NO. 76J10 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

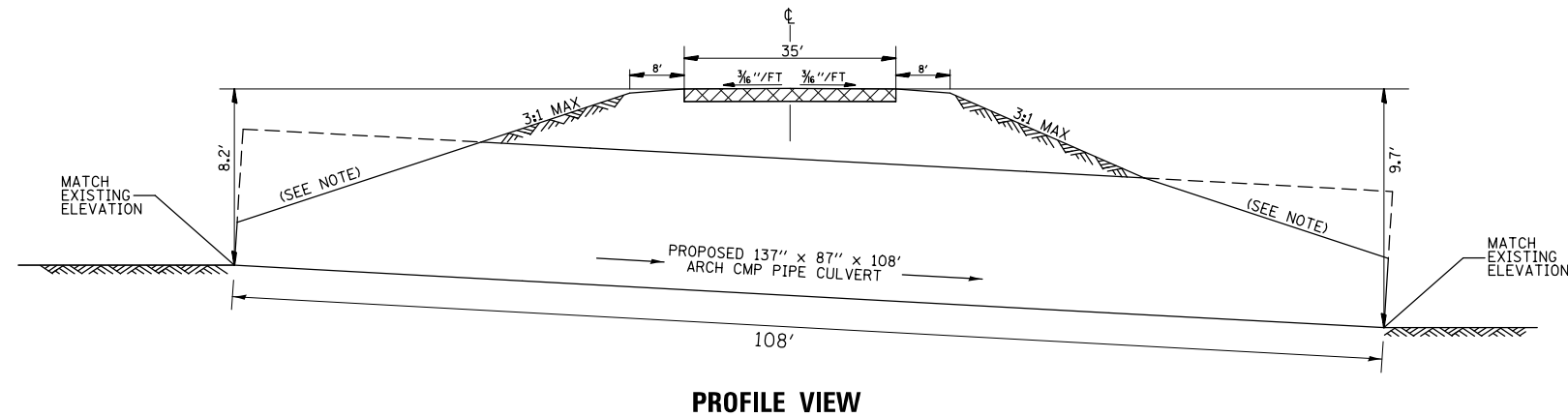
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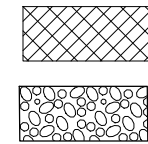
•THE PROPOSED CMP PIPE CULVERT HAS BEEN PROVIDED BY THE BUREAU OF OPERATIONS AND IS CURRENTLY STORED BEHIND THE TRUCK REPAIR BUILDING BEHIND THE SHELL GAS STATION ALONG IL 127.



RIP RAP DETAIL



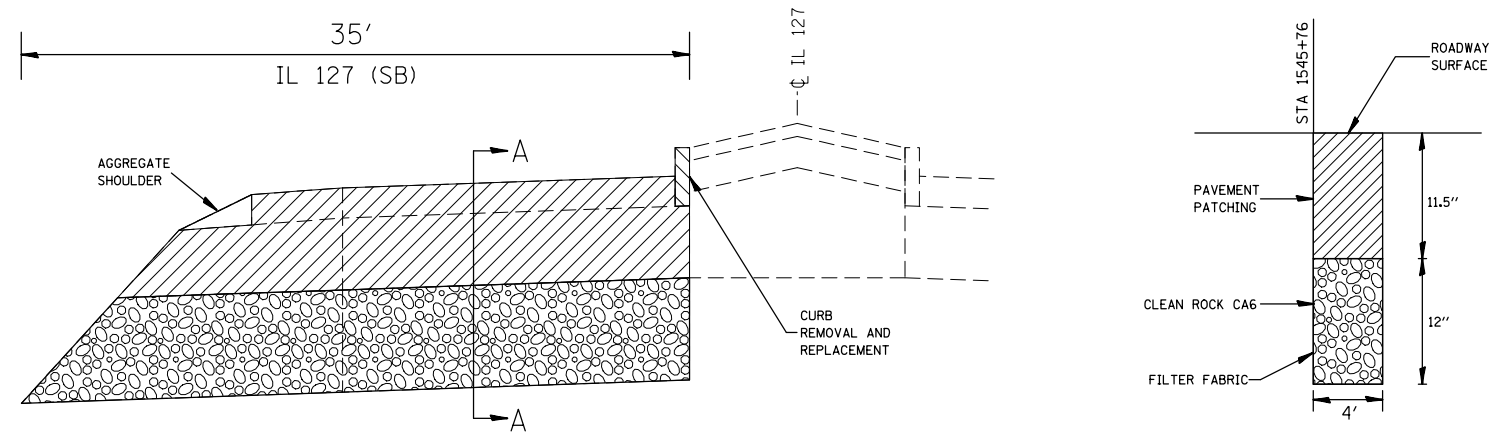
LEGEND



HIGH-EARLY STRENGTH RPCC PAVEMENT, 10" (WITH WELDED WIRE REINFORCEMENT)
 STONE RIP RAP A4 WITH FILTER FABRIC

NOTE: CUT TOP OF PIPE AT 3:1 BEVEL (ANGLE) TO MATCH EXISTING GROUND SLOPE. THE CUT EDGE OF THE INSTALLED CULVERT SHALL BE PROTECTED WITH GALVANIZED PAINT AS APPROVED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE PIPE CULVERTS, SPECIAL PAY ITEM.

PIPE CULVERT REMOVAL AND REPLACEMENT DETAIL



FRENCH DRAIN DETAIL

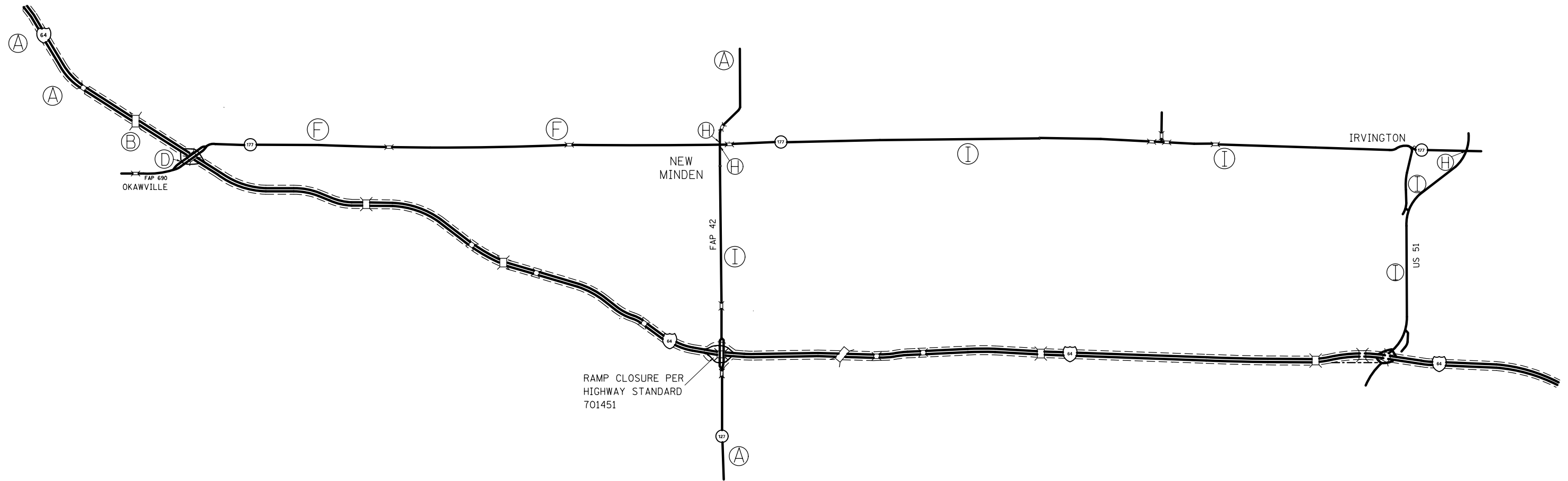
NOTES
 1. EARTH AND ROCK EXCAVATION WILL BE INCLUDED IN THE COST OF FRENCH DRAINS.
 2. THE FRENCH DRAIN SHALL EXTEND FROM THE CONCRETE CURB TO THE BRIDGE EMBANKMENT

| | | | | | | | | | | | | | |
|---|------------------------|------------|-----------|---|---------------------------------|---------------------|------|--------------------|---------|----------|---------------------------|-----------|----|
| FILE NAME = | USER NAME = durosierj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | CULVERT/DRAINAGE DETAILS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| pw:\IL\084EBIDINTEG\illinois.gov\PIDOT\Documents\IDOT Offices\District 8\Projects\0876\DRAWING\DATA\LEADSheets\0876J10-sht-plan.dwg | | CHECKED - | REVISED - | | SCALE: | SHEET 1 OF 1 SHEETS | STA. | TO STA. | 42 | 102-RS-3 | WASHINGTON | 26 | 23 |
| Default | PLOT DATE = 11/10/2016 | DATE - | REVISED - | | | | | CONTRACT NO. 76J10 | | | ILLINOIS FED. AID PROJECT | | |
| | | | | | | | | | | | | | |

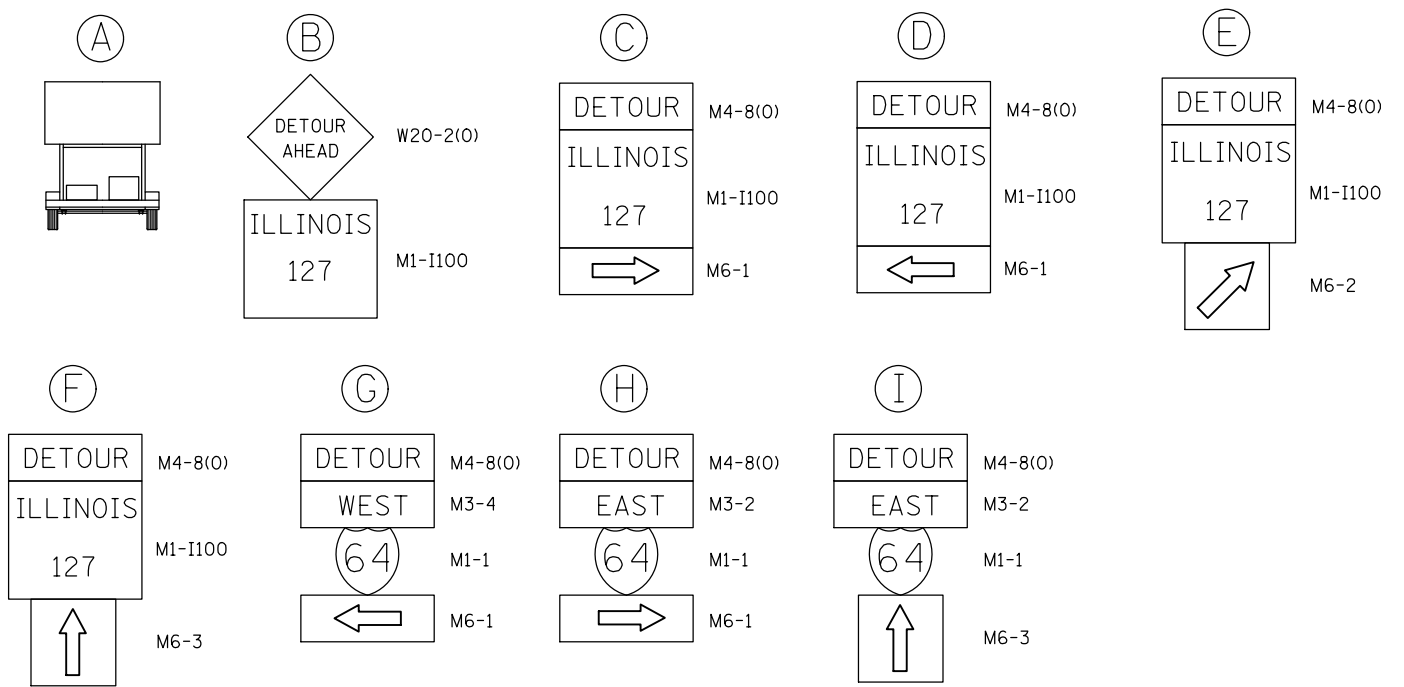
NOTES

1. THIS DETOUR SHALL ONLY BE APPLIED FOR RAMP PAVEMENT RECONSTRUCTION. THIS STAGES OF RAMP CLOSURE ACCOUNTED FOR IN THIS DETOUR PLAN WILL OCCUR IN TWO STAGES: (1) CLOSE I-64 EB ON-RAMP, AND I-64 EB OFF-RAMP, AND (2) CLOSE I-64 WB ON-RAMP, AND I-64 WB OFF-RAMP.
2. THE DETOUR SIGNS REQUIRED SHALL BE SUPPLIED BY THE CONTRACTOR.
3. THE CONTRACTOR SHALL FURNISH ALL SIGNS, POSTS, FLASHING LIGHTS, AND ERECT SIGNS AT THE LOCATIONS SHOWN ON THE PLANS, AS DIRECTED BY THE RESIDENT ENGINEER. THE SIGNS SHOWN ON THE PLAN SHALL CORRESPOND WITH THE PHASE OF RAMP CLOSURES.
4. CONTACT JEAN SLAPE AT 618-394-2189 FOR "CLOSED" PANELS TO PLACE ON EXIT SIGNS PRIOR TO THE CLOSED RAMPS.
5. SIX CHANGEABLE MESSAGE SIGNS SHALL BE REQUIRED AND PLACED AT THE LOCATIONS SHOWN ON THE PLANS, AS DIRECTED BY THE RESIDENT ENGINEER, TWO WEEKS PRIOR TO RAMP CLOSURES.
6. THIS WORK SHALL BE INCLUDED IN THE COST FOR DETOUR SIGNING

| | | | | | | | | | | | | |
|--|-------------------------------|------------|-----------|---|----------------------|---------------------|--------------|---------------------------|----------|------------|--------------|-----------|
| FILE NAME = | USER NAME = durosierj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | DETOUR ROUTES | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| pw:\IL\084EBIDINTEG.illinois.gov\PIDOT\Documents\DOT Offices\District 8\Projects\0876-DRAWING\DATA\ADSheets\0876J10-shr-plan.dwg | | CHECKED - | REVISED - | | | | | 42 | 102-RS-3 | WASHINGTON | 26 | 24 |
| Default | PLOT SCALE = 100.0000' / 1in. | DATE - | REVISED - | | | | | CONTRACT NO. 76J10 | | | | |
| | PLOT DATE = 10/20/2016 | DATE - | REVISED - | | SCALE: | SHEET 1 OF 3 SHEETS | STA. TO STA. | ILLINOIS FED. AID PROJECT | | | | |



STAGE 1



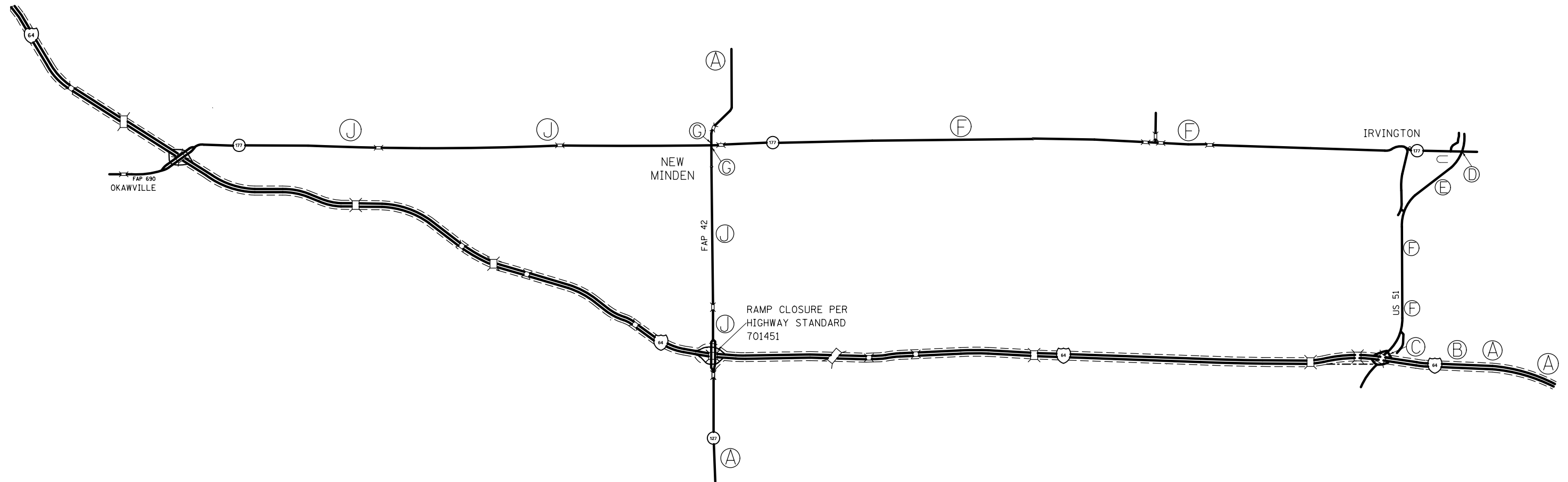
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|--|------------------------------|------------|-----------|
| FILE NAME = | USER NAME = durosierj | DESIGNED - | REVISED - |
| pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 8\Projects\0875\DRAWING\LEADSheets\D876J10-sht-pland | | REVISIONS | REVISED - |
| Default | PLOT SCALE = 100.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 10/20/2016 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

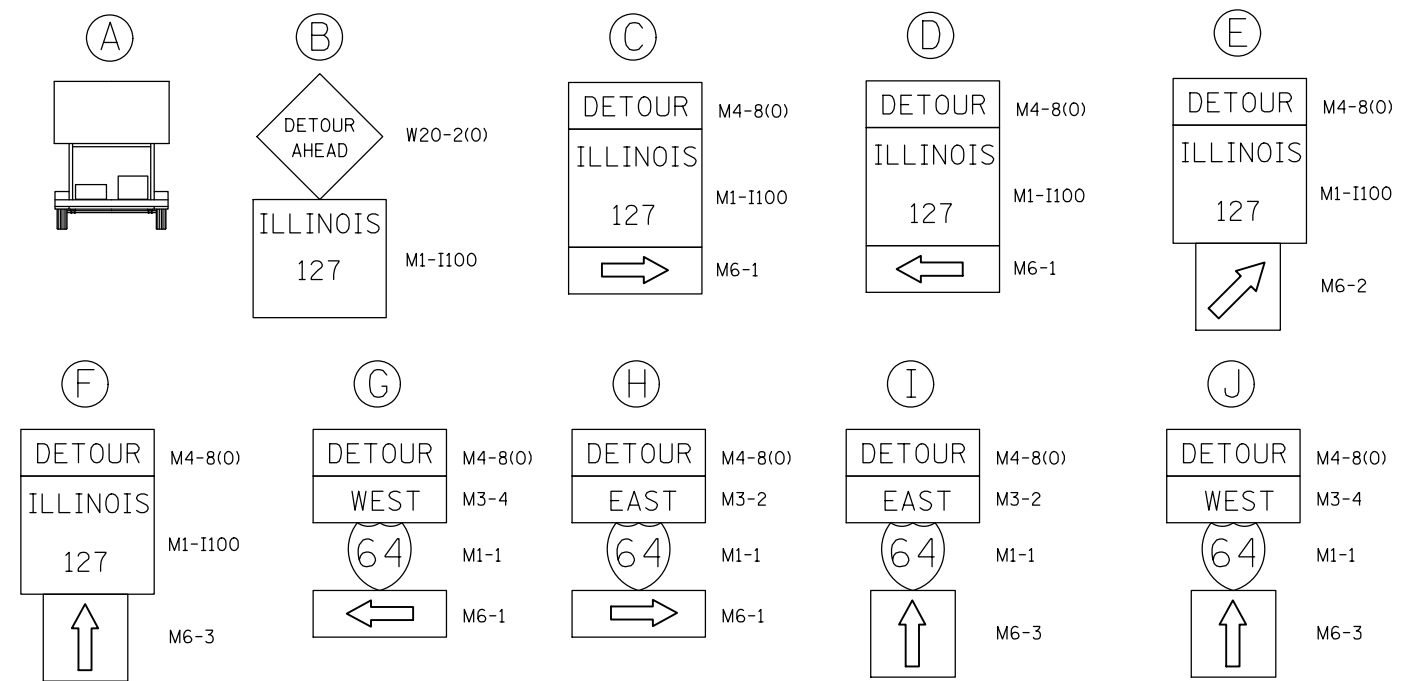
DETOUR ROUTES

SCALE: SHEET 2 OF 3 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|------------|--------------|-----------|
| 42 | 102-RS-3 | WASHINGTON | 26 | 25 |
| CONTRACT NO. 76J10 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



STAGE 2



| | | | |
|--|------------------------|------------|-----------|
| FILE NAME = | USER NAME = durosierj | DESIGNED - | REVISED - |
| pw:\IL\084EBIDINTEG.illinois.gov\PWIDOT\Documents\IDOT Offices\District 8\Projects\0875-DRAWING\DATA\LEADSheets\0876J10-sht-plan.dwg | | CHECKED - | REVISED - |
| Default | PLOT DATE = 10/20/2016 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETOUR ROUTES

SCALE: SHEET 3 OF 3 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------|------------|--------------|--------------------|
| 42 | 102-RS-3 | WASHINGTON | 26 | 26 |
| | | | | CONTRACT NO. 76J10 |
| ILLINOIS FED. AID PROJECT | | | | |