01-20-2017 LETTING ITEM 128

FOR INDEX OF SHEETS, SEE SHEET NO. 2

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STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

PROPOSED HIGHWAY PLANS

F.A.U. ROUTE 7706
(I55 BUS, OLD US 66, LINCOLN PARKWAY)
SECTION (22) RS-6, MED & (23) RS-5
PROJECT ACHSIP-ACM-7706(005)
TYPE of IMPROVEMENT 3R & SAFETY
LOGAN COUNTY

C-96-022-11

ROADWAY CLASSIFICATION STATION 137+86.63 IL 10 = STATION 0+00.35 I-55 BUSINESS LOOP/LINCOLN PARKWAY BEGIN PROJECT I-55 BUSINESS/LINCOLN PARKWAY MINOR ARTERIAL STATION 1758+05.91 STATION 137+65.65 IL 10 = STATION 1762+29.37 I-55 BUSINESS LOOP/LINCOLN PARKWAY **AVERAGE DAILY TRAFFIC (2013)** IL 10WOODLAWN ROAD TO 5TH STREET END PROJECT ILLINOIS ROUTE 10 STATION 143+90.83 BEGIN PROJECT ILLINOIS ROUTE 10 STATION 133+69.81 7,500 (2013) PV = 94.90%SU = 3.77%MU = 1.33%5TH STREET TO STRINGER AVENUE STATION 13+64.18 5TH STREET = STATION 39+64.22 I-55 BUSINESS LOOP/LINCOLN PARKWAY 4,000 (2013) PV = 90.25%STATION 40+00.00 STALHUT DR = STATION 19+70.44 I-55 BUSINESS LOOP/LINCOLN PARKWAY SU = 6.25%MU = 3.50%PROJECT LIMITS 5TH STREET STATION 12+41.03 END PROJECT I-55 BUSINESS LOOPLINCOLN PARKWA' STATION 76+90.11 PROJECT LIMITS 5TH STREET STATION 16+15.08 FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD STATION 23+22.40 STRINGER AVENUE = STATION 69+99.08 I-55 BUSINESS ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT LOOP/LINCOLN PARKWAY CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

F.A. SECTION COUNTY TOTAL SHEET NO. 343 1

JILLINOIS CONTRACT NO. 72E02

D-96-022-11



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED OF Solve 20 20 16

REGIONAL ENGINEER

20 10

ENGINEER OF DESIGN AND ENVIRONMENT

DELLA 20

DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PROJECT ENGINEER JAY WAVERING 217-785-9046
PROJECT MANAGER MICHAEL WHITLING 217-524-0239

JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION

GROSS LENGTH = 8114.35 FT. = 1.536 MILE NET LENGTH = 8114.35 FT. = 1.536 MILE

CONTRACT NO. 72E02

1-800-892-0123

INDEX OF SHEETS

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18-19	TYPICAL SECTIONS LOCATION MAP
20-44	TYPICAL SECTIONS LINCOLN PARKWAY
45-48	TYPICAL SECTIONS IL 10/121/WOODLAWN ROAD
49	TYPICAL SECTIONS 5TH STREET
50	TYPICAL SECTIONS STRINGER AVENUE
51	TYPICAL SECTIONS FRONTAGE ROAD
52	TYPICAL SECTIONS CULVERT REMOVAL
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337	CROSS SECTION STRINGER AVENUE
338-343	DISTRICT DETAILS

APPLICATION RATES

THE FOLLWOING RATES OF APPLICATION WERE UTILIZED TO CALCULATE QUANTITIES

CRANULAR MATERIALS

2.05 TONS/CU YD

BITUMINOUS MATERIALS - TACK COAT

EXISTING HMA, MILLED SURFACE,

0.05 LB/SQ FT

CONCRETE, MILLED/TINED CONCRETE)

0.025 LB/SQ FT

BITUMINOUS MATERIALS - TACK COAT (BETWEEN HMA LIFTS)

HMA SURFACE/BINDER

0.056 TONS/SO YO+IN

NITROGEN FERTILIZER NUTRIENT 90 LBS/ACRE
PHOSPHOROUS FERTILIZER NUTRIENT 90 LBS/ACRE
POTASSIUM FERTILIZER NUTRIENT 90 LBS/ACRE
AGGRICULTURE GROUND LIMESTONE 2 TONS/ACRE
MULCH 2 TONS/ACRE

TEMPORARY EROSION CONTROL SEEDING 100 LBS/ACRE

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-07	TEMPORARY EROSION CONTROL SYSTEM
424006-02	DIAGONAL CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHLD. STRIPS/SHLDS. WITH RESURFACING OR
	WIDENING AND RESURFACING PROJECTS
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
602301-04	INLET-TYPE A
602306-03	INLET-TYPE B
602401-03	MANHOLE TYPE A
602601-04	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
60400605	FRAME AND GRATE TYPE 3
60403603	GRATE TYPE 8
604061-03	FRAME AND GRATE TYPE 12
606001-06	CONCRETE CURB TYPE B AND COMBINATION
	CONCRETE CURB AND GUTTER
60630104	PC CONCRETE ISLANDS AND MEDIANS
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5 m) AWAY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24"
	(600 mm) FROM PAVEMENT EDGE
701306-03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS
	DAY ONLY FOR SPEEDS > OR = 45 MPH
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT
	RAMP, FOR SPEEDS > OR = 45 MPH
701422-09	LANE CLOSURE, MULTILANE, FOR SPEEDS > OR =
	45 MPH TO 55 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR
	MOVING OPER, FOR SPEEDS > OR = 45 MPH
701701–10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-06	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720016-03	MAST ARM MOUNTED STREET NAME SIGNS
728001-01	TELESCOPING STEEL SIGN SUPPORT BASE FOR TELESCOPING STEEL SIGN SUPPORT
731001–01 780001–05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED RELFLECTIVE PAVEMENT
781001-04	MARKERS
782001-01	CURB REFLECTORS
	HANDHOLES
	LUMINAIRE WIRING IN POLE
	LIGHT POLE FOUNDATION
	TRAFFIC SIGNAL GROUNDING & BONDING
87600104	PEDESTRIAN PUSH BUTTON POST
	CONCRETE FOUNDATION DETAILS
	TRAFFIC SIGNAL MOUNTING DETAILS
	COMMITTATION
	COMMITMENTS

 NGS CONTROL POINTS AWE AND AWE RM2 SHALL BE PRESERVED. IN ORDER TO PRESERVE THESE CONTROL POINTS THE CONTRACTOR SHALL SAW CUT 24-30 INCHES AROUND THE OUTSIDE EDGE OF THE VAULT BEFORE ANY REMOVAL OF ITEMS TAKES PLACE.

NGS CONTROL POINT	STATION	OFFSET	ELEVATION
AWE	140+49.659	57.472 LT	582.036
AWE RM2	142+14.535	41.308 LT	583.230

MIXTURE REQUIREMENTS				
LOCATION	IL 10 MAINLINE & TURN LANES	LINCOLN PARKWAY & 5TH ST. MA(NL(NE & TURN LANES & 5TH STREET		
MIXTURE USE(S)	POLY HMA SURFACE	HMA SURFACE		
PC	SBS PG 70-28	PC 64-22		
DESIGN AIR VOID	4.0% o N50	4. 0% c NSO		
MIXTURE COMP	(L 9.5	IL 9.5		
FRICTION AGG	MIX "D"	MIX "D"		
QUALITY MANAGEMENT	QC/QA	QCP		
SUBLOT SIZE	N/A	1000 TONS		

	MIXTURE REQUIREMEN	ITS
LOCATION	ALL	ALL
MIXTURE USE(S)	LEVELING BINDER.	BASE COURSE/HMA SHOULDERS 6.5+
PG	PG 64-22	PG 64-22
DESIGN AIR VOID	4.0% e N50	4.0% a N50
MIXTURE COMP	IL 9.5•	IL 19.0
FRICTION AGG	N/A	N/A
QUALITY MANAGEMENT	OCP+	OC/QA
SUBLOT SIZE	1000 TONS	N/A

*SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION

MIXTURE REQUIREMENTS					
LOCATION	ALL/SIDE ROADS STRINGER AVENUE				
MIXTURE USE(S)	INCIDENTAL HMA SURFACE/ HMA SURFACE/HMA SHOULDERS				
PG	PG 64-22				
DESIGN AIR VOID	4. 0% to N50				
MIXTURE COMP	11 9.5				
FRICTION AGG	MIX "C"				
QUALITY MANAGEMENT	OC/QA				
SUBLOT SIZE	N/A				

EXAMINED _	October	6	20	16
Au	Mrs P. Myan			
PROGRAM/	DEVELOPMENT	ENGIN	EER	

	DISTRICT SIX	
EXAMINED	October 12th	20 16
OPERATION	S ENGINEER(\	

EXAMINED _	October Ron lucha	II mbeo	20	16
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NDEX 0	F SHEET	S, HIGHWAY STAN	DARDS	F.A.U. RTE.	SECTION	COUNTY	TOT
	MIYT	URE DESIGNS		7706	•	LOGAN	34
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. (22)RS-6. MED & (23) RS-5

GENERAL NOTES

EXCAVATION IN OLD GRAVEL OR EXISTING OIL AND CHIP ROADWAYS IS TO BE INCLUDED IN THE COST OF EARTH EXCAVATION.

CRADING SHALL BE DONE BY HAND AROUND LIGHT POLES, UTILITY POLES, SIGN POSTS, SHRUBS, TREES OR OTHER NATURAL OR MAN-MADE OBJECTS WHERE SHALLOW FILLS OR CUTS ARE ADJACENT TO THE ITEMS.

THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR EXISTING ITEMS WHICH WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT THE CONTRACTOR'S EXPENSE.

AT ANY AREA WHERE GRADING IS NOT PROPOSED. THE EXISTING GROUND COVER SHALL NOT BE DISTURBED.

ONLY THOSE TREES DESIGNATED BY THE ENGINEER OR LISTED IN THE TREE REMOVAL SCHEDULE SHALL BE REMOVED. THE CONTRACTOR SHALL PROTECT ALL REMAINING TREES FROM DAMAGE DUE TO HIS OPERATIONS.

EXISTING PAVEMENT DAMAGED DUE TO THE CONTRACTOR'S OPERATIONS, AND NOT OTHER WISE NECESSARY TO REPLACE, SHALL BE REPLACED AT THE EXPENSE OF THE CONTRACTOR.

SAW CUTS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

ALL DRAINAGE STRUCTURES SHALL BE FREE OF SILT, DEBRIS, OR OTHER SUCH OBSTRUCTIONS AT THE TIME OF FINAL INSPECTION, THE CLEANING OF THESE DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE VARIOUS PAY ITEMS INVOLVED. THE WORK AREA SHALL BE POSITIVELY DRAINED DURING CONSTRUCTION, FINAL GRADES SHALL BE PROTECTED AGAINST DAMAGE FROM EROSION, SEDIMENTATION, AND TRAFFIC.

LENGTHS AND SIZES OF STORM SEWERS AS SHOWN ON THE PLANS AND DRAINAGE STRUCTURE ELEVATIONS SHALL BE VERIFIED BY THE CONTRACTOR IN THE FIELD PRIOR TO ORDERING AND INSTALLATION OF DRAINAGE ITEMS. THE INVERTS OF THE PROPOSED DRAINAGE STRUCTURES MAY REQUIRE REVISIONS TO MEET EXISTING FIELD CONDITIONS. ANY ADJUSTMENTS SHALL BE AS DIRECTED BY THE ENGINEER.

FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.

THE CONTRACTOR WILL BE RESPONSIBLE FOR THE TRAFFIC CONTROL DEVICES AT ALL TIMES DURING CONSTRUCTION ACTIVITIES AND SHALL COORDINATE THE ITEMS OF WORK IN ORDER TO KEEP HAZARD TRAFFIC INCONVIENCES TO A MINIMUM.

SEEDING SHALL BE DONE ON ALL AREAS THAT ARE DISTURBED BY CONSTRUCTION OPERATIONS AS DIRECTED BY THE ENGINEER. ALL AREAS DISTURBED BY THE CONTRACTOR OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE SEEDED. AS DIRECTED BY THE ENGINEER, AT THE CONTRACTOR'S EXPENSE, SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION.

ALL PRECAST CONCRETE BOX CULVERT SEGMENTS WITH PIPE CULVERTS INTERSECTING THE SIDWALLS SHALL BE DETAILED AS PER THE REQUIREMENTS OF ASTM C1577. EXCEPT THAT THE AS2 AND AS5 REINFORCEMENT AREAS SHALL BE INCREASED TO 0.40 SO, IN. PER FOOT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ADEQUATE SIGNS, TRAFFIC CONTROL DEVICES, AND WARNING DEVICES TO INFORM AND PROTECT THE PUBLIC DURING ALL PHASES OF CONSTRUCTION.

ALL PAVEMENT MARKINGS SHALL BE VERIFIED BY RESIDENT/FIELD ENGINEER PRIOR TO THEIR PLACEMENT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.

THE CONTRACTOR SHALL CONTACT JULLILE. PRIOR TO ANY EXCAVATION TO HAVE ALL PUBLIC AND PRIVATE UTILITIES LOCATED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.26 OF THE STANDARD SPECIFICATIONS. THE JULLILE. NUMBER IS (800) 892-0123. A MINIMUM OF 48 HOURS ADVANCE NOTICE IS REQUIRED.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUB-NUMBER LISTED ON THE INDEX OF SHEETS OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.

ALL ELEVATIONS REFER TO USGS MEAN SEA LEVEL DATUM.

ALL PAVEMENT MARKING LINES THAT CONFLICT WITH STAGE CONSTRUCTION THAT IS LOCATED OFF THE PROJECT SHALL BE REMOVED BY WATER BLASTING AND SHALL BE PAID FOR AS PAVEMENT MARKING REMOVAL - WATER BLASTING.

UTILITES ARE SHOWN IN ACCORDANCE WITH THE BEST AVAILABLE INFORMATION, AND THEIR TRUE LOCATION IS NOT GUARANTEED TO BE AS SHOWN ON THE PLANS. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE EXACT LOCATION OF ALL UTILITIES AND CARRY OUT HIS/HER OPERATIONS ACCORDINGLY.
MEMBERS OF JULILIE. KNOWN TO BE WITHIN THE LIMITS OF THE IMPROVEMENT ARE:

AMEREN CILCO - GAS

ILLINOIS AMERICAN WATER - WATER

FRONTIER COMMUNICATIONS - FIBER OPTICS

METRO COMMUNICATIONS COMPANY INC. - COMMUNICATIONS

COMCAST COMMUNICATIONS - CABLE

AMEREN/CENTRAL ILLINOIS - ELECTRIC

PANHANDLE EASTERN PIPELINE CO. - PIPELINE

FIBER UTILITY GROUP - FIBER OPTIC

CITY OF LINCOLN - SEWER

CITY OF LINCOLN (STREET DEPT.) - WATER

CMS FIBER - FIBER OPTIC

SCALE:

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GEN	IERAL NOTES		7706	•	LOGAN	343	3
					CONTRACT	NO. 7	2E02
OF	SHEETS STA.	TO STA.		ILLINOIS FED. AT	D PROJECT		

CONSTRUCTION CODE											
				PPS# 6-00625-0000 80%FED/20%STATE STP ROADWAY	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE HSIP ROADWAY	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE STP TRAFFIC SIGNALS	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL STP TRAFFIC SIGNALS	PPS# 6-00625-0000 5TH STREET INTERSECTION 80 %FED/IS %STATE/ 5%LOCAL STP TRAFFIC SIGNALS	PPS# 6-00477-0000 5TH STREET INTERSECTION 90 %FED/10%STATE HS1P TRAFFIC SIGNALS	PPS# 6-00625-0000 80%FED/20%LOCAL STP SIDEWALK	PPS# 6-00625-000 100%LOCAL STP LIGHTING
CODE			TOTAL	0004	0004	0021	0021	0021	0021	0028	0021
NO	ITEM	UNIT	QUANTITY	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
		-									
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	203	203							
20200100	EARTH EXCAVATION	CU YD	5468	4820	604					44	
20800150	TRENCH BACKFILL	CU YD	202. 9	102.7	95. 4					4.8	
25000200	SEEDING, CLASS 2	ACRE	7. 4	7. 4							
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	669	669							
denders med der sem eine der der der der der der der der der de											
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	669	669							
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	669	669							
25000700	AGRICULTURAL GROUND LIMESTONE	TON	15	15	And the state of t				***************************************		
					north and the same of the same					<u> </u>	
25100115	MULCH, METHOD 2	ACRE	7.4	7.4							
					university of the second secon						
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	744	744	TO A						
28000305	TEMPORARY DITCH CHECKS	FOOT	560	560	-						
					ACA CONTRACTOR AND						
28000400	PERIMETER EROSION BARRIER	FOOT	581	581	- Control of the Cont						
28000500	INLET AND PIPE PROTECTION	EACH	34	34	V-444						
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SO YO	13445	10675	2770						
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							CONSTRUCTION	ON CODE			
				PPS# 6-00625-0000 80%FED/20%STATE STP	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE HSIP	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13,33%STATE/ 6.67%LOCAL STP	PPS# 6-00625-0000 5TH STREET INTERSECTION 80%FED/6 %STATE/ 5%LOCAL STP	PPS# 6-00477-0000 5TH STREET INTERSECTION qo%FED/ I V%STATE H\$1P	PPS# 6-00625-0000 80%FED/20%LOCAL STP	PPS# 6-00625-000 100%LOCAL STP
		-		ROADWAY	ROADWAY	TRAFFIC SIGNALS	TRAFFIC SIGNALS	TRAFFIC SIGNALS	TRAFFIC SIGNALS	SIDEWALK	LIGHTING
CODE			TOTAL		****				4-4-4-10-10-10-10-10-10-10-10-10-10-10-10-10-		
NO	ITEM	UNIT	QUANTITY	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
35101800	AGGREGATE BASE COURSE, TYPE B 6"	SO YD	95	95							
35300310	PORTLAND CEMENT CONCRETE BASE COURSE 8 1/2"	SQ YD	2757	2257	500						
35300500	PORTLAND CEMENT CONCRETE BASE COURSE 10"	SO YD	2991	786	2205						
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	9780	9780							
35501318	HOT-MIX ASPHALT BASE COURSE, 8 1/2"	SQ YD	99	99							
· · · · · · · · · · · · · · · · · · ·											
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	62684	38086	24598						
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	5332	3275	2057						
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	1573	784	789						
40600990	TEMPORARY RAMP	SO YD	384	188	196						
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	4873	3017	1856						
40603535	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D". N50	TON	709		709						
4000000											
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	239	138	101						
42400100	PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH	SO FT	2358							2358	
42400800	DETECTABLE WARNINGS	SO FT	10		TOTAL PARTY AND THE PARTY AND					10	
		 		 	 						

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1	Summery of Weentities	PLOT DATE . 10/21/2016	DATE ~	REVISED -		SCALE:	SHEET	OF	SHEETS STA.	TO STA.		ILLINOIS FED.	AID PROJECT		

				***************************************			CONSTRUCTE	ON CODE			
				PPS# 6-00625-0000 80%FED/20%STATE STP	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE HSIP	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL STP	PPS# 6-00625-0000 5TH STREET INTERSECTION 90%FED/15%STATE/ 5%LOCAL	PPS# 6-00 477-0000 5TH STREET INTERSECTION 90%FED/ 10%STATE	PPS# 6-00625-0000 80%FED/20%LOCAL STP	PPS# 6-00625-000 100%LOCAL STP
				ROADWAY	ROADWAY	TRAFFIC SIGNALS	TRAFFIC SIGNALS	TRAFFIC SIGNALS	TRAFFIC SIGNALS	SIDEWALK	LIGHTING
CODE		223475	TOTAL QUANTITY	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
NO	ITEM	UNIT	QUANTITY	URBAN	URDAN	URDAN	UNDAN	UNDAN	ONDAIN	ONDAG	ONDAN
44000100	PAVEMENT REMOVAL	SO YD	311	311							
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	141	141							
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	4182	446	3736						
		And the state of t			man proposition of the control of th			XXX			
44000600	SIDEWALK REMOVAL	SQ FT	71						, , ,	71	
44003100	MEDIAN REMOVAL	SO FT	3472	-	3472						
				The state of the s			The state of the s				
44004250	PAVED SHOULDER REMOVAL	SO YD	4087	4003	84						
44200094	PAVEMENT PATCHING, TYPE II. 8 INCH	SO YD	13.4	13.4							
44200099	PAVEMENT PATCHING, TYPE III, 8 INCH	SO YD	67.8	67.8							
44200101	PAVEMENT PATCHING, TYPE IV, 8 INCH	SO YD	102	102			The state of the s		TANAN		
77200101		The state of the s		- Company		Territoria de la constanta de	42-24-24-24-24-24-24-24-24-24-24-24-24-2		The second secon		
44200120	PAVEMENT PATCHING, TYPE II, 10 INCH	SO YD	15	15							
44200144	PAVEMENT PATCHING, TYPE II. 12 INCH	SQ YD	710	710	THE PROPERTY OF THE PROPERTY O						,
44200148	PAVEMENT PATCHING, TYPE III, 12 INCH	SO YD	94	94			Transfer and the second		1.00 mm m m m m m m m m m m m m m m m m m		
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	1308	1119	189		1				
48203023	HOT-MIX ASPHALT SHOULDERS, 6 1/2"	so yo	2954	2954		**************************************			-		
				 	 		1	<u> </u>		<u> </u>	

FILE NAME :	USER NAME = whitlingme	DESIGNED -	REVISED -								F.A.U. RTE.	SECTION	COUNTY	TOTAL SH SHEETS	IEET VO.
pwt\\IL884EBIDINTEG.111zmoze.gov/PWI00T\Do	uments\1001 Offices\District 6\Projects\Q67		L	STATE OF ILLINOIS		S	UMMARY	OF QUA	INTITIES		7706		LOGAN	343	6
	PLOT SCALE * 188.9888 1/ 10.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION				· · · · · · · · · · · · · · · · · · ·					CONTRAC	CT NO. 72E	02
Summary of Quantities	PLOT DATE > 18/21/2016	DATE -	REVISED -		SCALE	SHEET	OF .	SHEETS	STA.	TO STA.		ILLINOIS FEO. A	ID PROJECT		

							CONSTRUCTI	ON CODE			
				PPS# 6-00625-0000 80%FED/20%STATE STP ROADWAY	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE HSIP ROADWAY	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE STP TRAFFIC SIGNALS	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL STP TRAFFIC SIGNALS	PPS# 6-00/25-0000 5TH STREET INTERSECTION 90%FED/15%STATE/ 5%LOCAL 51P TRAFFIC SIGNALS	PPS# 6-00477-0000 5TH STREET INTERSECTION 90%FED/10%STATE HS1P TRAFFIC SIGNALS	PPS# 6-00625-0000 80%FED/20%LOCAL STP SIDEWALK	PPS# 6-00625-0000 100%LOCAL STP LIGHTING
CODE			TOTAL	NOADIA							
NO	ITEM	UNIT	QUANTITY	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
48203100	HOT-MIX ASPHALT SHOULDERS	TON	1624	1398	226						
50102400	CONCRETE REMOVAL	CU YD	16.2	16. 2							
50104400	CONCRETE HEADWALL REMOVAL	EACH	6	6	4 - 4 - 4 - 4 - 4 - 4 - 4 - 4 - 4 - 4 -						
50105220	PIPE CULVERT REMOVAL	FOOT	760	753						7	
50200100	STRUCTURE EXCAVATION	CU YD	161.8	161.8							
50800105	REINFORCEMENT BARS	POUND	709	709							
54001001	BOX CULVERT END SECTIONS, CULVERT NO. 1	EACH	2	2							
54001002	BOX CULVERT END SECTIONS, CULVERT NO. 2	EACH	2	2							
54001003	BOX CULVERT END SECTIONS, CULVERT NO. 3	EACH	2	2							
		- 100									
54001004	BOX CULVERT END SECTIONS, CULVERT NO. 4	EACH	1	1				of coloring of converse		PARATA INCOME DE LA CANADA DEL CANADA DE LA CANADA DEL CANADA DE LA CANADA DEL CANADA DE LA CANADA DEL CANADA DE LA CANADA DEL CANADA DE LA CANADA D	
54001005	BOX CULVERT END SECTIONS, CULVERT NO. 5	EACH	2	2				PATE AND ADDRESS A			
54001006	BOX CULVERT END SECTIONS, CULVERT NO. 6	EACH	2	2							
54002020	EXPANSION BOLTS 3/4 INCH	EACH	48	48							
54010302	PRECAST CONCRETE BOX CULVERTS 3' X 2'	FOOT	66	66							
		Topic Control of Contr								**************************************	

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FILE NAME =	USER NAME = whitlingma	DESIGNED -	REVISED -			··					F.A.U. RYE.	SECTION	COUNTY	TOTAL SHEET SHEETS NO.
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	PLOT SCALE = 188.8888 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION									CONTRAC	T NO. 72E02
Summary of Quantities	PLOT DATE = 18/21/2016	DATE -	REVISED -		SCALE:	SHEET	QF.	SHEETS	STA.	TO STA.		[GERMAIN], ED	. AID PROJECT	
										• (22)RS	-6. MED &	(23) RS-5		

							CONSTRUCTI	ON CODE			
			-	PPS# 6-00625-0000 80%FED/20%STATE STP ROADWAY	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE HSIP ROADWAY	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE STP TRAFFIC SIGNALS	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL STP TRAFFIC SIGNALS	PPS# 6-00625-0000 5TH STREET INTERSECTION 80%FED/15%STATE/ 5%LOCAL STP TRAFFIC SIGNALS	PPS# 6-00477-0000 5TH STREET INTERSECTION 90%FED/10%STATE H51P TRAFFIC SIGNALS	PPS# 6-00625-0000 80%FED/20%LOCAL STP SIDEWALK	PPS# 6-00625-0000 100%LOCAL STP LIGHTING
CODE	the state of the s		TOTAL								
NO	ITEM	UNIT	QUANTITY	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
			·								
542A0220	PIPE CULVERTS, CLASS A, TYPE 1 15"	FOOT	85	60						25	
54200220	PIPE CULVERTS, CLASS D. TYPE 1 15"	FOOT	4							4	
54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"	EACH	10	10							
		Article Control of the Control of th							**************************************		
54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	2	2							
54248510	CONCRETE COLLAR	CU YD	6. 5	6. 5							
				A SAME AND							
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	64	64							
550A0070	STORM SEWERS, CLASS A. TYPE 1 15"	FOOT	94	94							
550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	928	928							
550A0120	STORM SEWERS, CLASS A. TYPE 1 24"	FOOT	88	88							
			***************************************						,		
55100500	STORM SEWER REMOVAL 12"	FOOT	505		505						
60218500	MANHOLES, TYPE A. 4'-DIAMETER, TYPE 3 FRAME AND GRATE	EACH	2	2							
60219000	MANHOLES, TYPE A, 4' -DIAMETER, TYPE 8 GRATE	EACH	1							1	
60219400	MANHOLES, TYPE A, 4' -DIAMETER, TYPE 12 FRAME AND GRATE	EACH	1	1							
60235700	INLETS, TYPE A, TYPE 3 FRAME AND GRATE	EACH	2	2							
				 	<u> </u>	······································	 		 		

FILE NAME X	USER NAME = whitlingme	DESIGNED -	REVISED -							F.A.U. RTF.	SECTION	COUNTY	TOTAL S	HEET
,	PLOT SCALE = 188,9888 1/ in.	CHECKED -	PREVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		S	UMMARY O	F QUANTITIES		7706	•	LOGAN	343	8
Summery of Quentities	PLOT DATE = 19/21/2816	DATE -	REVISED -	DEPARTMENT OF TRANSPORTATION	SCALE:	SHEET	OF S	CHEETS STA.	TO STA.	 	ILLINOIS FED.	CONTRACT	T NO. 728	.02

							CONSTRUCTI	ON CODE			
in the second se	-			PPS# 6-00625-0000 80%FED/20%STATE STP ROADWAY	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE HSIP ROADWAY	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE STP TRAFFIC SIGNALS	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL STP TRAFFIC SIGNALS	PPS# 6-00/25/-0000 5TH STREET INTERSECTION 80%FED/\15%STATE/ 5%LOCAL 6TP TRAFFIC SIGNALS	PPS# 6-00477-0000 5TH STREET INTERSECTION 90%FED/10%STATE HSIP TRAFFIC SIGNALS	PPS# 6-00625-0000 80%FED/20%LOCAL STP SIDEWALK	PPS# 6-00625-0000 100%LOCAL STP LIGHTING
CODE			TOTAL		and the second s	**************************************			URBAN	URBAN	URBAN
NO	ITEM	UNIT	QUANTITY	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	UNDAN	ONDAIL
											<u> </u>
60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	1	1	And the second s						
									mary and at a fact of the fact	Transfer de transfer de la constanta de la con	
	THE THE LANGE TO SPANS AND SPANS	EACH	1	1							
60236900	INLETS, TYPE A, TYPE 12 FRAME AND GRATE	EACH	1	 							
				<u> </u>							
60240301	INLETS, TYPE B, TYPE 8 GRATE	EACH	1	1				<u> </u>			
									Arrange in the second s		
C0340715	INLETS, TYPE B. TYPE 12 FRAME AND GRATE	EACH	1	1						-	
60240313	INCETS, TIFE B, TIFE IZ TOOME ONE CONT.				 	 					
						<u> </u>	<u></u>			<u> </u>	
60255500	MANHOLES TO BE ADJUSTED	EACH	5	2	3	<u> </u>		-			
				Birthannin							
60262700	INLETS TO BE RECONSTRUCTED	EACH	1							1	
90202100	THE TO TO BE RESOURCE					<u> </u>					
				_	_						
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	7	2	5						
		er Avenue de la Contraction de	La de la constanta de la const								
60402210	GRATES, TYPE 8	EACH	5	5	eave transport.	***************************************		-			

		5.60	7		7						
60500060	REMOVING INLETS	EACH			-	e de la companya de l					
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6. 12	FOOT	532	532	***************************************	***					
									and the state of t	elised arrived to the state of	
	COMBINATION CONCRETE CURB AND GUTTER, TYPE 8-6.18	FOOT	114	114							
60604400	COMBINATION CONCRETE CURB AND GOTTER, TIPE D-0. TO							<u> </u>			
	·				<u> </u>						
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6. 24	FOOT	265	265						<u> </u>	

60609563	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12	FOOT	5951	5951					-	-	
2000000	COMPTANTION CONTINUES COMP AND COLICENT THE PARTY				+	 					
				***************************************	 		<u> </u>				
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SO FT	30026	30026							
		1		1	-	1			•	1	1

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FILE NA	ME =	USER NAME = whitlingme	DESIGNED -	REVISED -			RIE, SACCIO 380.
		umenta\IDST Offices\District 6\Projects\D67	EEEEEEEE000000000000000000000000000000	D.BEVISED -	STATE OF ILLINOIS	SUMMARY OF QUANTITIES	7706 • LOGAN 343 9
	-	PLOT SCALE * 188,0000 1/ 10.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	CALLE CUESTS OF CUESTS STA TO STA	CONTRACT NO. 72E02
Summer	y of Quantities	PLOT DATE * 19/21/2816	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA. • (22)RS-6	6, MED & (23) RS-5

							CONSTRUCTION	ON CODE			
				PPS# 6-00625-0000 80%FED/20%STATE STP ROADWAY	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE HSIP ROADWAY	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE STP TRAFFIC SIGNALS	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL STP TRAFFIC SIGNALS	PPS# 6-00625-0000 5TH STREET INTERSECTION 80%FED/15%STATE/ 5%LOCAL 5TP TRAFFIC SIGNALS	PPS# 6-00477-0000 5TH STREET INTERSECTION 90%FED/10%STATE HSIP TRAFFIC SIGNALS	PPS# 6-00625-0000 80%FED/20%LOCAL STP SIDEWALK	PPS# 6-00625-000 100%LOCAL STP LIGHTING
CODE			TOTAL	I I I I I I I I I I I I I I I I I I I	******	LIDDAN	URBAN	URBAN	URBAN	URBAN	URBAN
NO	ITEM	UNIT	QUANTITY	URBAN	URBAN	URBAN	URDAN	UNDAIN	UNDAN	URBAN	ORBAN
60622305	CONCRETE MEDIAN, TYPE SM - 4.12	SO FT	882	882					A and the same of		
			······································								
60622400	CONCRETE MEDIAN, TYPE SM-6.06	SO FT	603		603	,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,					
									4 manual		
60622800	CONCRETE MEDIAN. TYPE SM-6.12	SO FT	2561		2561				The state of the s		
66900200	NON-SPECIAL WASTE DISPOSAL	Cu YD	2000	2000				-	de alla de la constanta de la		
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	15	15					15 mm		
66900480	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	and the state of t							
67100100	MOBILIZATION	LSUM	1	1	na veri			Account the same of the same o			
66900530	SOIL DISPOSAL ANALYSIS	EACH	l l l l l	I(o						The second secon	
70100320	TRAFFIC CONTROL AND PROTECTION, STANDARD 701422	LSUM	ı	1	***************************************					and the second s	
67201100	SEALING ABANDON MONITORING WELLS	EACH	1	1	the production of the state of					anger and a second a second and	
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	1	1							
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	LSUM	I	1						· · · · · · · · · · · · · · · · · · ·	
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1							
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	60	60							
				***************************************						-	
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	48	48							
70300100	SHORT TERM PAVEMENT MARKING	FOOT	4233	2587	1646					4	
			······································	 	the Control of the Co			The state of the s	4	**************************************	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1397	854	543						
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	592. 8	93. 6	499. 2						
	The second secon							I	L	<u> </u>	

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p+s\\ILB84EBIDINTEG:slisnoss.goviPWIDDT\E	comments\iDDT_Offices\District_6\Projects\D6	7 (SR XXXIII) 0 to \6A0ahoo to \0672682-aht-83-Si	D.BEVISED -	STATE OF ILLINOIS		S	UMMARY	OF QUA	NTITIES		7706	•	LOGAN	343 1	5
	PLGT SCALE = 188,8888 1/ In.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION									CONTRAC	T NO. 72E	j 2
Summory of Quantities	PLGT DATE + 18/21/2016	DATE -	REVISEO -		SCALE:	SHEET	OF	SHEETS	STA. TO	STA.		ILLINOIS FEO. A	ID PROJECT		二

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		many grand open management of the control of the co		PPS# 6-00625-0000 80%FED/20%STATE	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE HSIP	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL STP	PPS# 6-00(25-0000 5TH STREET INTERSECTION 80%FED/15%STATE/ 5%LOCAL STP	PPS# 6-00477-0000 5TH STREET INTERSECTION 90%FED/10%STATE	PPS# 6-00625-0000 80%FED/20%LOCAL STP	PPS# 6-00625-0000 100%LOCAL STP
angeneraphy company		P. C.		ROADWAY	ROADWAY	TRAFFIC SIGNALS	TRAFFIC SIGNALS	TRAFFIC SIGNALS	TRAFFIC SIGNALS	SIDEWALK	LIGHTING
CODE	The state of the s		TOTAL	***************************************		Avada					
NO	ITEM	UNIT	QUANTITY	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	37250	23143	14107						
72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	11	2	9						
						Andrews and the second					
72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	19	18						1	
72400310	REMOVE SIGN PANEL - TYPE I	SO FT	146	128	18						
72400320	REMOVE SIGN PANEL - TYPE 2	SO FT	14	14							
72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	9	1	8						
					Table 1 Annual Park 1 Annual P					*	
72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	EACH	19	18							
72400710	RELOCATE SIGN PANEL - TYPE 1	SQ FT	139	121	18						
72400720	RELOCATE SIGN PANEL - TYPE 2	SO FT	14	14	The state of the s			Control of the Contro			
72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	812	630	154					28	
774-001-00	BASE FOR TELESCOPING STEEL SIGN SUPPORT	EACH	10	1	9		No. 41.				
* /3100100	BASE FOR TELESCUPING STEEL STON SUFFORT	EAGO	10	-	-						
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	738	272	466						
* 78003100	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LETTERS AND SYMBOLS	SQ FT	1361	536	825						
	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 5"	FOOT	4741	3128	1613		Victoria de la constanta de la				

* SPECIALTY ITEM

FILE NAME :	USER NAME = whitlingme	DESIGNED -	REVISED -		<u> </u>			w.,		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	HEET NO.
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	PLOT SCALE * 1880.88880 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION								CONTRAC	T NO. 72	2E02
Summary of Quantities	PLOT DATE = 18/21/2016	DATE -	REVISED -		SCALE;	SHEET	OF	SHEETS STA.	TO STA,		INCECTOR 1	D. AID PROJECT		
									* (22)RS	5-6, MED 8	& (23) RS-5			

				and the same of th			CONSTRUCTI	ON CODE			
				PPS# 6-00625-0000 80%FED/20%STATE STP	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE HSIP	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL STP	PPS# 6-00625-0000 5TH STREET INTERSECTION 80%FED/15%STATE/ 5%LOCAL STP	PPS# 6-00477-0000 5TH STREET INTERSECTION 90%FED/10%STATE HS1P	PPS# 6-00625-0000 80%FED/20%LOCAL STP	PPS# 6-00625-0000 100%LOCAL STP
1				ROADWAY	ROADWAY	TRAFFIC SIGNALS	TRAFFIC SIGNALS	TRAFFIC SIGNALS	TRAFFIC SIGNALS	SIDEWALK	LIGHTING
CODE		The state of the s	TOTAL	*********	verteinminus verte						
NO	ITEM	UNIT	QUANTITY	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
78003130	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 6"	FOOT	1034	230	804						
78009005	MODIFIED URETHANE PAVEMENT MARKING ~ LINE 5"	FOOT	43995	28248	15747						
78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	6208	1111	5097						
78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	1854	583	1271						
78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	3098	1541	1557				The state of the s		
18003012	MODIFICE OIL HARL PATEMENT MANAGENO - LISE 12		3020	***							
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	676	303	373						
78200020	CURB REFLECTORS	EACH	403	242	161						
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	676	303	373						
81028350	UNDERGROUND CONDUIT, PVC, 2" DIA.	FOOT	1748			1748					
							-		A Copyright of the Copy		
81028370	UNDERGROUND CONDUIT, PVC. 3" DIA.	FOOT	71	P		Anna tamana da	71	1			
81028390	UNDERGROUND CONDUIT, PVC, 4" DIA.	FOOT	272				131		141		
81400100	HANDHOLE	EACH	2				2				
81500100	GULFBOX JUNCTION	EACH	8			8					
7.20700			-								
81500130	GULFBOX JUNCTION REMOVAL	EACH	9			9					
1			A-1-1-1-1-1-1-1-1-1-1-1-1-1-1-1-1-1-1-1			1			4	1	

SPECIALTY ITEM

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1	pwi\\iL884E8IDINTEG.illinois-goviPWIOOT\Do	umenta\IDOT Offices\District 6\Projects\D67	EBRXXXXXXXxxxxxXXXXXXXXXXXXXXXXXXXXXXXXX	D.RIGVISED -	STATE OF ILLINOIS		SUMM	IARY OF QUANTITIES		7706	•	LOGAN	343	12
- 1		PLOT SCALE . 188.8888 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION							CONTRACT	T NO. 72E	.02
1	Summery of Quantities	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET OF	SHEETS STA.	TO STA.		ILLINOIS FED, A	NO PROJECT		

							CONSTRUCTION	ON CODE,			
				PPS# 6-00625-0000 80%FED/20%STATE	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL	PPS# 6-00/25-0000 5TH STREET INTERSECTION 80%FED/ 15%STATE/ 5%LOCAL		PPS# 6-00625-0000 80%FED/20%LOCAL	PPS# 6-00625-0000
				STP	HSIP	STP	STP TRAFFIC SIGNALS	STP TRAFFIC SIGNALS	HS19 TRAFFIC SIGNALS	STP SIDEWALK	STP LIGHTING
CODE		***************************************	TOTAL	ROADWAY	ROADWAY	TRAFFIC SIGNALS	I KAFFIC SIGNALS	IRAFFIC SIGNALS	TRAFFIC SIGNALS	SIDEVAMEN	LIGHTING
NO	ITEM	UNIT	QUANTITY	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
<u></u>											
81702130	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6	FOOT	1935							1935	

83600300	LIGHT POLE FOUNDATION, 30" DIAMETER	FOOT	6, 5					-		6. 5	
The state of the s				The state of the s							
83800650	BREAKAWAY DEVICE, COUPLING WITH STAINLESS STEEL SCREEN	EACH	ı							1	
		3-A									
84200804	REMOVAL OF POLE FOUNDATION	EACH	1		The state of the s					1	
84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	1							ı.	
						,					
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	3			1		eran eran eran eran eran eran eran eran			
					The state of the s						
87300215	ELECTRIC CABLE IN TRENCH, SIGNAL, NO. 14 2C	FOOT	654			654					
			And the second s		***************************************						
87300225	ELECTRIC CABLE IN TRENCH, SIGNAL, NO. 14 3C	FOOT	654		***************************************	654		The second secon			
		district the second sec									
87301705	ELECTRIC CABLE IN CONDUIT, COMMUNICATION NO. 18 3 PAIR	FOOT	2973			1052	691	1230			
				***************************************		V					
87500600	TRAFFIC SIGNAL POST, 10 FT.	EACH	2			2		·			

87800100	CONCRETE FOUNDATION, TYPE A	FOOT	6	-		6			Arando de la companio del companio de la companio del la companio del companio de la companio de la companio de la companio del companio de la companio del compa		
					THE STATE OF THE S				***	-	
87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	23. 5	 			23. 5		-		
V.000-00	The state of the s	. * * '				a de la constanta de la consta		And the state of t	**************************************	***************************************	
87900200	DRILL EXISTING HANDHOLE	EACH	20		-	16	2	1	2	***************************************	
					Table and the same	1	-	**************************************		A STATE OF THE STA	
88102717	PEDESTRIAN SIGNAL HEAD, LED. 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	FACH	2	7		2	***	Acceptance of the control of the con			
JUIVE ! I !	COUNTDOWN TIMER	2,,0,,,		1	1	•	1	1	1		

* SPECIALTY ITEM

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		DEFABILITY OF INAMOPORTATION									CONTRACT	T NO. 72E0
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				***			CONSTRUCTION	ON CODE			
				PPS# 6-00625-0000 80%FED/20%STATE STP ROADWAY	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE HSIP ROADWAY	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE STP TRAFFIC SIGNALS	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL STP TRAFFIC SIGNALS	PPS# 6-00625-0000 5TH STREET INTERSECTION 80%FED/15%STATE/ 5%LOCAL 6TP TRAFFIC SIGNALS	PPS# 6-00477-0000 5TH STREET INTERSECTION 90%FED/ 10%STATE H51P TRAFFIC SIGNALS	PPS# 6-00625-0000 80%FED/20%LOCAL STP SIDEWALK	PPS# 6-00625-0000 100%LOCAL STP LIGHTING
CODE		distribution of the state of th	TOTAL					Acres de la constanta de la co			
NO	ITEM	UNIT	QUANTITY	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
88102747	PEDESTRIAN SIGNAL HEAD. LED. 2-FACE. BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2			2			ANALY STATE OF THE		
88200510	TRAFFIC SIGNAL BACKPLATE, RETROREFLECTIVE	EACH	34			11	10	13			
88800100	PEDESTRIAN PUSH-BUTTON	EACH	6			6					
89500100	RELOCATE EXISTING SIGNAL HEAD	EACH	4				3		1		
89501250	RELOCATE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	2				2				
89501300	RELOCATE EXISTING MAST ARM ASSEMBLY AND POLE	EACH	2	-			2				
89502210	MODIFY EXISTING CONTROLLER CABINET	EACH	3			1	1	T			
89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	2758		111111111111111111111111111111111111111	1052		1126		580	
89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	4726			2207	1790		729		
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	13			8		5			
89502380	REMOVE EXISTING HANDHOLE	EACH	6			3	2		1	The state of the s	
89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	3				2		1		
x0325279	CLASS SI CONCRETE (MISCELLANEOUS)	CU YD	0.5	0. 5							
x0326694	PLUG EXISTING STORM SEWERS	CU YD	2, 2		2. 2			The state of the s	THE PROPERTY AND ADDRESS OF THE PROPERTY ADDRESS OF THE PROPERTY AND ADDRESS OF THE PROPERTY ADDRESS OF THE PROPERTY AND ADDRESS OF THE PROPERTY ADDRE	Available to the second	

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										• (22)RS	5-6, MED & (23) RS-5			

						CONSTRUCTI	ON CODE			
			PPS# 6-00625-0000 80%FED/20%STATE	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL	PPS# 6-00625-0000 5TH STREET INTERSECTION 8 0%FED/ \5%STATE/ 5%LOCAL	PPS# 6-00477-0000 5TH STREET INTERSECTION 90%FED/10%STATE	PPS# 6-00625-0000 80%FED/20%LOCAL	PPS# 6-00625-0000
		TOTAL QUANTITY	STP ROADWAY	HSIP ROADWAY URBAN	STP TRAFFIC SIGNALS URBAN	STP TRAFFIC SIGNALS URBAN	STP TRAFFIC SIGNALS URBAN	HSIP TRAFFIC SIGNALS URBAN	STP SIDEWALK URBAN	STP LIGHTING URBAN
ITEM	UNIT	QUANTIT	URBAN	URBAN	URDAN	URDAN	URDAIN	ORDAN	ORDAN	ORBAN
PLUG EXISTING PIPE	CU YD	3. 7	3. 7							
PAVEMENT MARKING REMOVAL - GRINDING	SO FT	9377	6457	2920						
PAVEMENT MARKING REMOVAL - WATER BLASTING	SO FT	2758	1829	929						
5 LEVELING BINDER (MACHINE METHOD), SPECIAL	TON	3690	2651	1039						
B HOT-MIX ASPHALT SURFACE REMOVAL. VARIABLE DEPTH	SO YD	73240	43300	29940						
CONCRETE MEDIAN SURFACE REMOVAL	SQ FT	15268		15268						
PRECAST CONCRETE BOX CULVERTS 2' X 2' (SPECIAL)	FOOT	261	261	4411						
D REMOVE AND REPLACE CONCRETE HEADWALLS FOR PIPE DRAINS	EACH	2	2							
5 TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	LSUM	1	TA T		THE STATE OF THE S					
2 TRAFFIC CONTROL AND PROTECTION. STANDARD BLR 21 (SPECIAL	LSUM	1	1					WANTED THE PROPERTY OF THE PRO		
4 TRAFFIC CONTROL AND PROTECTION, STANDARD 701701, SPECIAL	LSUM	1		1						
8 CAMERA MOUNTING ASSEMBLY	EACH	11		Age of the control of	4	3	4			
8 CONSTRUCTION LAYOUT	LSUM	1	11		A COLUMN TO THE STATE OF THE ST					
O FILLING EXISTING CULVERTS	CU YD	39. 4	39. 4		The state of the s					

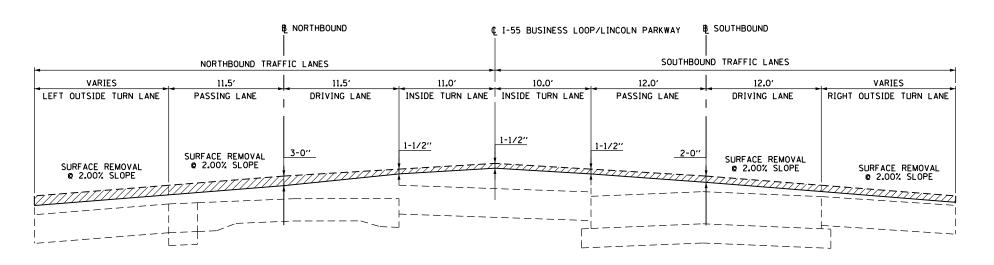
* SPECIALTY ITEM

| RTÉ. | SECTION | COUNTY | TOTAL | SHEET | SHEETS | NO. | TOTAL | S USER NAME * whitingme DESIGNED -REVISED -STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION SUMMARY OF QUANTITIES umants\1007 Offices\0;striot 6\Projects\067**:588%M9**Doto\6A0shoets\0672682-sht-83-S02.86V1SED pwr\\iL884E8IDINTEG.;}Isnos*.gov;PWIDDT PLOT SCALE * 182.8888 1/ In.
PLOT DATE * 18/21/2816 CHECKED -REVISED -SCALE: SHEET OF SHEETS STA. Summery of Quantities DATE REVISED -TO STA.

				***************************************			CONSTRUCTION	ON CODE			
7.00				PPS# 6-00625-0000 80%FED/20%STATE STP ROADWAY	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE HSIP ROADWAY	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE STP TRAFFIC SIGNALS	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL STP TRAFFIC SIGNALS	PPS# 6-00625-0000 5TH STREET INTERSECTION 80%FED/ 15%STATE/ 5%LOCAL 5TP TRAFFIC SIGNALS	PPS# 6-00477-0000 5TH STREET INTERSECTION 90%FED/10%STATE HS 1P TRAFFIC SIGNALS	PPS# 6-00625-0000 80%FED/20%LOCAL STP SIDEWALK	PPS# 6-00625-00 100%LOCAL STP LIGHTING
CODE	ITEM	UNIT	TOTAL QUANTITY	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
	1 Call		42311111								
70023602	GRANULAR CULVERT BACKFILL	CU YD	138	138							
		A CONTRACTOR OF THE CONTRACTOR									
70033072	VIDEO VEHICLE DETECTION SYSTEM	EACH	3			1	1	1	,		
20033012	TIPLO TENTOLE BETTER TOWN STOLEN	LAVIC				•	•	*			
70077700	ANGLITUDIANA JOANIT SEALANIT		70100	17040	16240						***************************************
	LONGITUDINAL JOINT SEALANT	FOOT	30189	13940	16249					! 	
10048665	DATI BOAD SPOTECTIVE LIBITATE TO VICENIES	LSUM	•								
20040505	RAILROAD PROTECTIVE LIABILITY INSURANCE	LSUM	1	1							
		***************************************		4000							
(0076600	TRAINEES	-HOUR	4000	-4000							,
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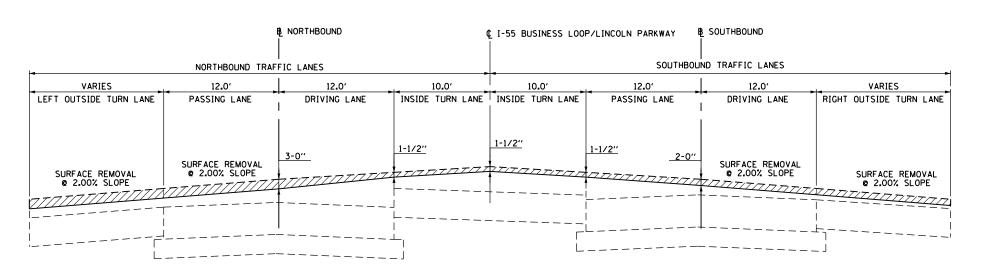
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SURFACE REMOVAL, VARIABLE DEPTH DETAIL 1-55 BUSINESS LOOP/LINCOLN PARKWAY

STATION 1+32.17 TO STATION 17+60.00 STATION 21+75.00 TO RIGHT STATION 37+00.00



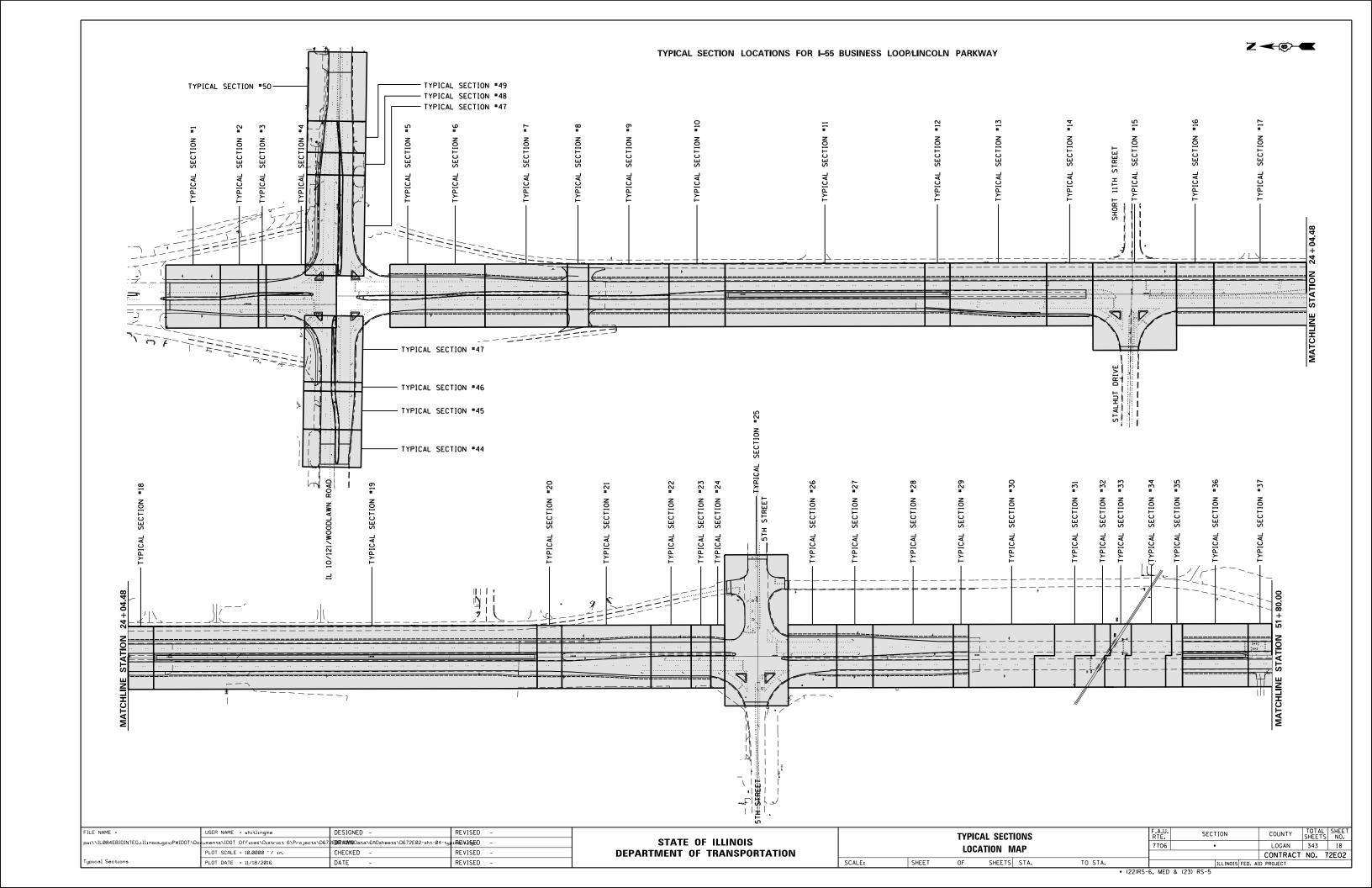
SURFACE REMOVAL, VARIABLE DEPTH DETAIL I-55 BUSINESS LOOP/LINCOLN PARKWAY

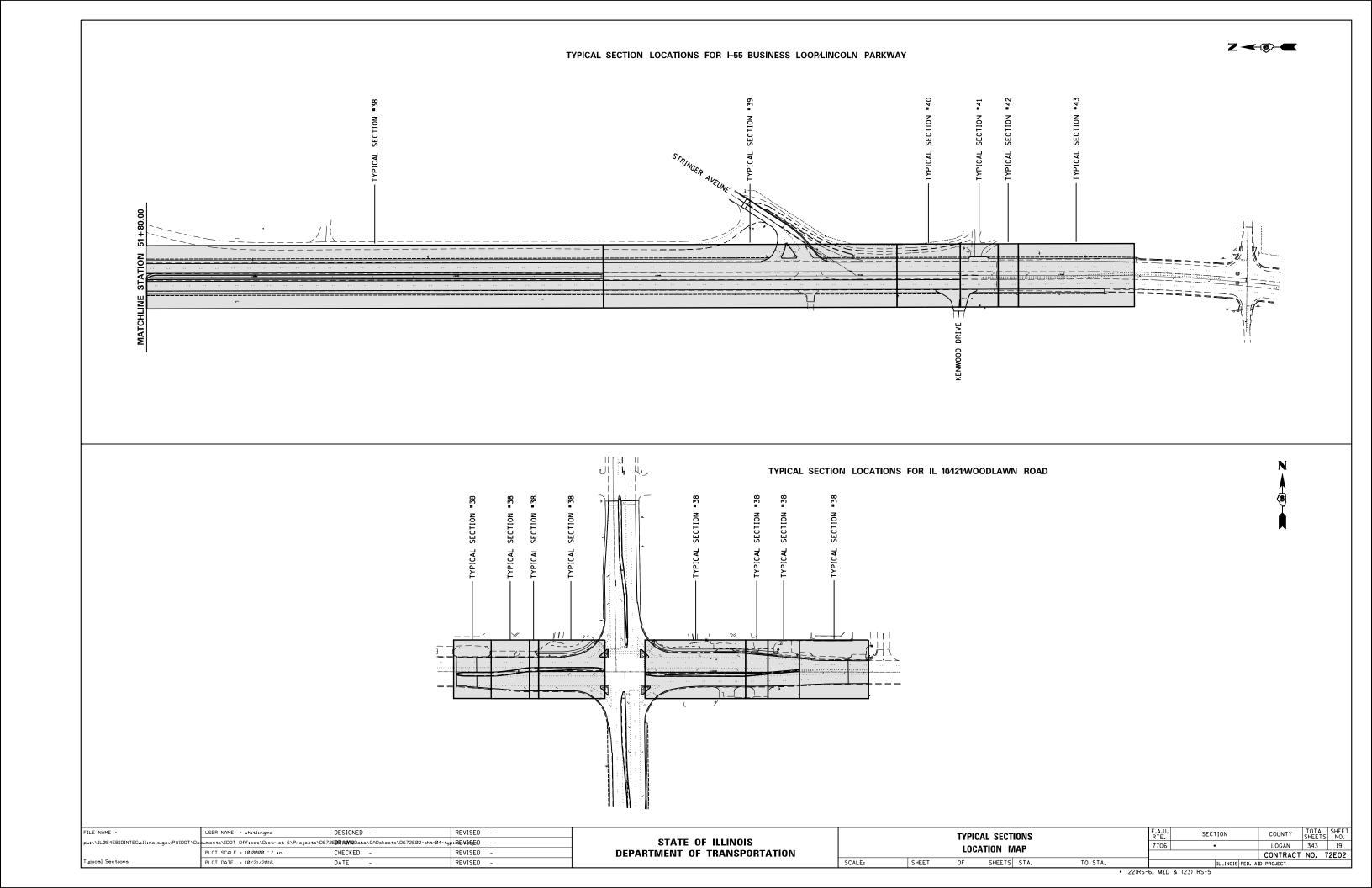
RIGHT STATION 42+50.00 TO RIGHT STATION 46+50.00 RIGHT STATION 49+75.00 TO RIGHT STATION 73+50.00 LEFT STATION 42+50.00 TO LEFT STATION 47+00.00 LEFT STATION 49+00.00 TO LEFT STATION 73+50.00

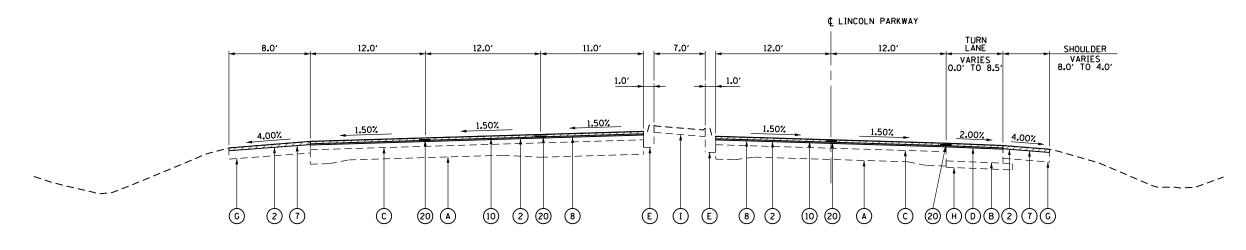
INTERSECTIONS IL 10121WOODLAWN ROAD STALHUT DRIVE 5TH STREET

ALL LANES SHALL BE 2" SURFACE REMOVAL

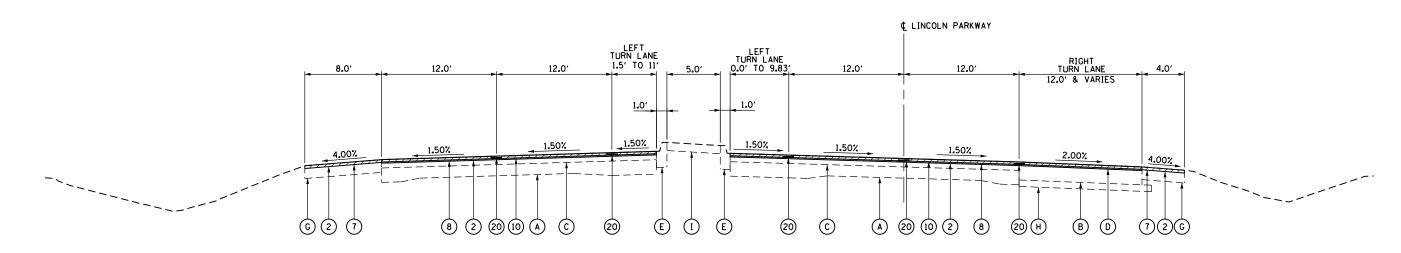
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				·				• (22)RS-6, MED & (23) RS-5							







TYPICAL SECTION #1 I-55 BUSINESS LOOP/LINCOLN PARKWAY NORTH OF IL 10 INTERSECTION STATION 1758+05.85 TO STATION 1759+41.28



TYPICAL SECTION #2 I-55 BUSINESS LOOP/LINCOLN PARKWAY NORTH OF IL 10 INTERSECTION STATION 1759+41.28 TO STATION 1760+36.63

TYPICAL SECTION LEGEND

- (A) EXISTING PCC PAVEMENT 9"-7"-9"
- (B) EXISTING PCC BASE COURSE WIDENING 9"
- C EXISTING PCC PAVEMENT 10"
- D EXISTING HMA OVERLAY
- E EXISTING CC&G TYPE M-6.06
- F EXISTING CC&G TYPE B-6.24
- (G) EXISTING HMA SHOULDER 8"
- (H) EXISTING SUB-BASE GRANULAR MATERIAL 4"

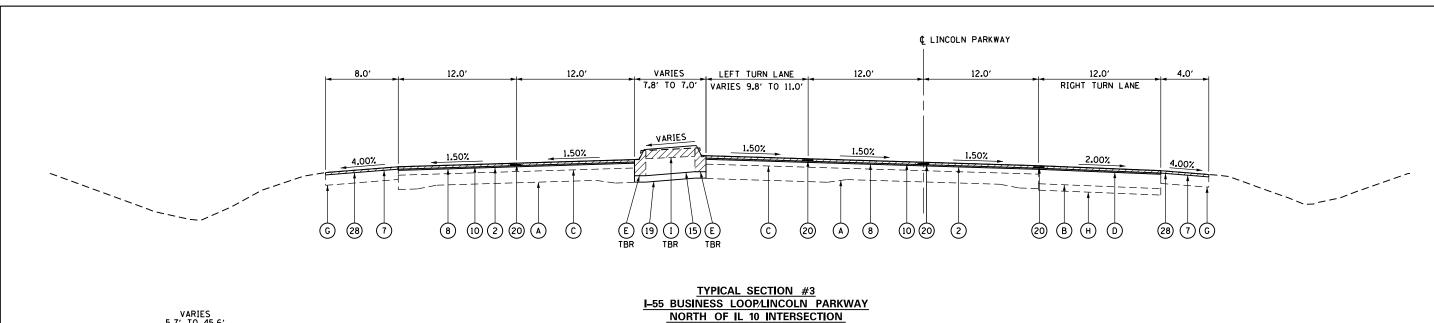
- I EXISTING MEDIAN SURFACE 4"
- J EXISTING HMA BASE COURSE 9"
- K EXISTING GRANULAR BORROW 6"
- L) EXISTING HMA PAVEMENT 12-1/2"
- (M) EXISTING HMA PAVEMENT 8"
- (N) EXISTING STABILIZED SOIL 12"
- TBR TO BE REMOVED

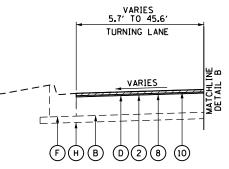
- 1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")
- 2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")
- 3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")
- (4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)
- (5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")
- (6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")
- 7) PROPOSED HMA SHOULDERS
- (8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")
- (9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)
- (9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

- (10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")
- (11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")
- (12) PROPOSED HMA BASE COURSE, 8"
- (13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"
- (14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"
- (15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06
- (16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12
- (17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12
- (18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

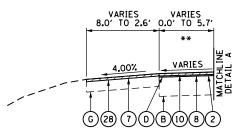
- (19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
- (20) LONGITUDINAL JOINT SEAL
- (21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- 22) PROPOSED SEEDING, CLASS 2
- (23) PROPOSED INCIDENTAL HMA SURFACING
- (24) PROPOSED HMA SHOULDER, 6-1/2"
- 25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- (26) PROPOSED HMA BASE COURSE, 8 1/2"
- (27) PROPOSED PAVEMENT REMOVAL
- (28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

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Typical Sections	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO				TO STA.		ILLINOIS FED. A	AID PROJECT		
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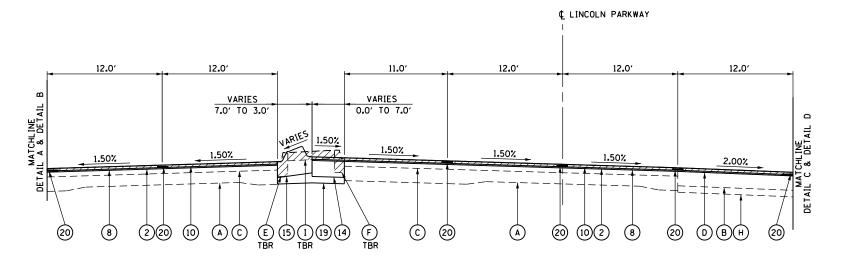


DETAIL B STATION 1760+96.87 TO STATION 1761+70.82



** TURN LANE STATION 1760+58.46 TO STATION 1760+96.87

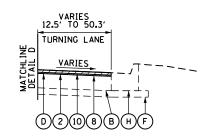
STATION 1760+54.93 TO STATION 1760+96.87



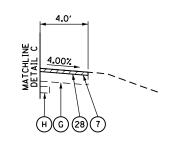
STATION 1760+36.63 TO STATION 1760+54.93

TYPICAL SECTION #4

I-55 BUSINESS LOOP/LINCOLN PARKWAY NORTH OF IL 10 INTERSECTION STATION 1760+54.93 TO STATION 1762+29.37



DETAIL D STATION 1760+97.52 TO STATION 1761+70.82



DETAIL D STATION 1760+54.93 TO STATION 1760+97.52

TYPICAL SECTION LEGEND

- A EXISTING PCC PAVEMENT 9"-7"-9"
- (B) EXISTING PCC BASE COURSE WIDENING 9"
- C EXISTING PCC PAVEMENT 10"
- D EXISTING HMA OVERLAY
- E EXISTING CC&G TYPE M-6.06
- F EXISTING CC&G TYPE B-6.24
- (G) EXISTING HMA SHOULDER 8"
- H EXISTING SUB-BASE GRANULAR MATERIAL 4"

- I EXISTING MEDIAN SURFACE 4"
- J EXISTING HMA BASE COURSE 9"
- (K) EXISTING GRANULAR BORROW 6"
- L) EXISTING HMA PAVEMENT 12-1/2"
- M EXISTING HMA PAVEMENT 8"
- (N) EXISTING STABILIZED SOIL 12"
- TBR TO BE REMOVED

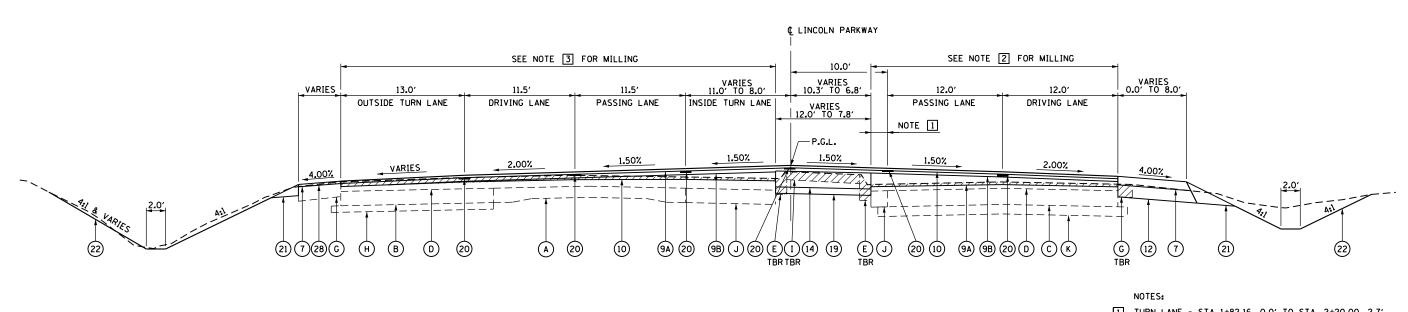
- 1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")
 - (2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")
 - (3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")
 - (4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)
 - (5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")
 - (6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")
 - (7) PROPOSED HMA SHOULDERS
 - (8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")
 - (9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)
 - (9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

- (10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")
- (11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")
- (12) PROPOSED HMA BASE COURSE, 8"
- (13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"
- (14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"
- (15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06
- (16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12
- (17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12
- (18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

- (19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
- 20 LONGITUDINAL JOINT SEAL
- (21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- 22) PROPOSED SEEDING, CLASS 2
- (23) PROPOSED INCIDENTAL HMA SURFACING
- PROPOSED HMA SHOULDER, 6-1/2"
- (25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- (26) PROPOSED HMA BASE COURSE, 8 1/2"
- (27) PROPOSED PAVEMENT REMOVAL
- (28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

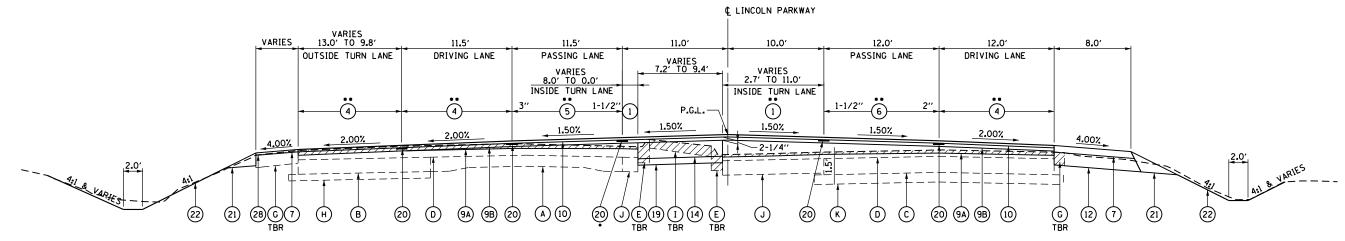
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• (22)RS-6, MED & (23) RS-5



TYPICAL SECTION #5 I-55 BUSINESS LOOP/LINCOLN PARKWAY TRANSITION FROM IL 10/121/WOODLAWN ROAD INTERSECTION STATION 1+32.17 TO STATION 2+20.00

- 1 TURN LANE STA 1+82.16, 0.0' TO STA. 2+20.00, 2.7'
- SB SURFACE REMOVAL MAINTAIN 2" STATION 2+20.00 TO STATION 1+32.17
- 3 NB SURFACE REMOVAL TRANSITION FROM 3" STATION 2+20.00 TO 2" STATION 1+32.17



• ENDS AT STATION 2+32.49

TYPICAL SECTION #6 I-55 BUSINESS LOOP/LINCOLN PARKWAY

STATION 2+20.00 TO STATION 3+68.00

FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL

		TYPICAL SECTION	LEGEND

- A EXISTING PCC PAVEMENT 9"-7"-9"
- (B) EXISTING PCC BASE COURSE WIDENING 9"
- C EXISTING PCC PAVEMENT 10"
- D EXISTING HMA OVERLAY
- E EXISTING CC&G TYPE M-6.06
- F EXISTING CC&G TYPE B-6.24
- (G) EXISTING HMA SHOULDER 8"
- (H) EXISTING SUB-BASE GRANULAR MATERIAL 4"

- (I) EXISTING MEDIAN SURFACE 4"
- J EXISTING HMA BASE COURSE 9"
- (K) EXISTING GRANULAR BORROW 6"
- L EXISTING HMA PAVEMENT 12-1/2"
- (M) EXISTING HMA PAVEMENT 8"
- (N) EXISTING STABILIZED SOIL 12"
- TBR TO BE REMOVED
- (7) PROPOSED HMA SHOULDERS (8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")
- (9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)

1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")

3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")

(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")

(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)

PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")

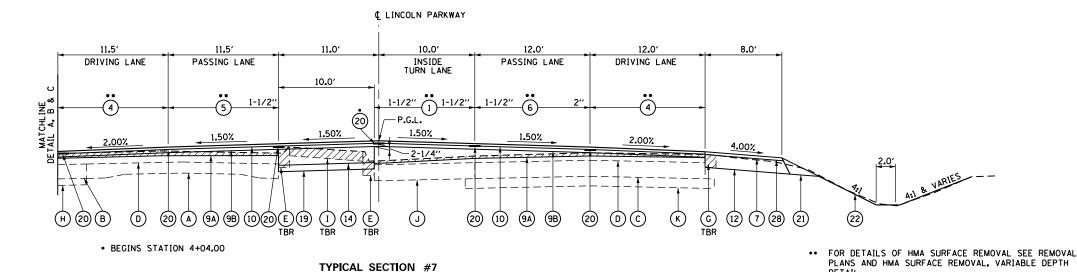
(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")

- (10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")
- (11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")
- (12) PROPOSED HMA BASE COURSE, 8"
- (13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"
- (14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"
- (15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06
- (16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12
- (17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12
- 18 PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

- (19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
- 20 LONGITUDINAL JOINT SEAL
- (21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- 22) PROPOSED SEEDING, CLASS 2
- PROPOSED INCIDENTAL HMA SURFACING
- (24) PROPOSED HMA SHOULDER, 6-1/2"
- PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- (26) PROPOSED HMA BASE COURSE, 8 1/2"
- (27) PROPOSED PAVEMENT REMOVAL
- (28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

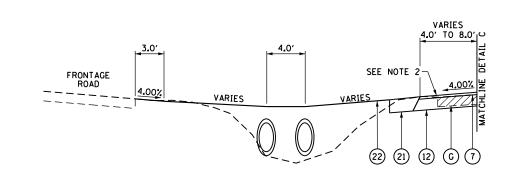
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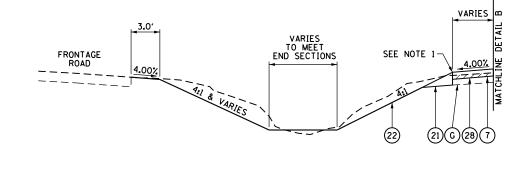
• (22)RS-6, MED & (23) RS-5



TYPICAL SECTION #7 I-55 BUSINESS LOOP/LINCOLN PARKWAY

STATION 3+68.00 TO STATION 5+72.62





DETAIL B STATION 5+13.30 TO STATION 5+25.00

DETAIL C STATION 5+25.00 TO STATION 5+72.62

- 1. EXISTING SHOULDER TO REMAIN STATION 3+68.00 TO STATION 4+77.42.
- 2. PROPOSED SHOULDER STATION 4+77.42 TO STATION 5+57.68

TYPICAL SECTION LEGEND

- A EXISTING PCC PAVEMENT 9"-7"-9"
- (B) EXISTING PCC BASE COURSE WIDENING 9"
- C EXISTING PCC PAVEMENT 10"
- D EXISTING HMA OVERLAY
- E EXISTING CC&G TYPE M-6.06
- F EXISTING CC&G TYPE B-6.24
- (G) EXISTING HMA SHOULDER 8"
- (H) EXISTING SUB-BASE GRANULAR MATERIAL 4"

I EXISTING MEDIAN SURFACE 4"

VARIES 9.8' TO 0.0'

OUTSIDE TURN LANE

21 G 28 7 H B D 4 9A 9B 10

DETAIL A

STATION 3+68.00 TO STATION 5+13.30

- J EXISTING HMA BASE COURSE 9"
- K EXISTING GRANULAR BORROW 6"
- L EXISTING HMA PAVEMENT 12-1/2"
- (M) EXISTING HMA PAVEMENT 8"
- (N) EXISTING STABILIZED SOIL 12"
- TBR TO BE REMOVED
- 2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")
- - 3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4") 4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)

1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")

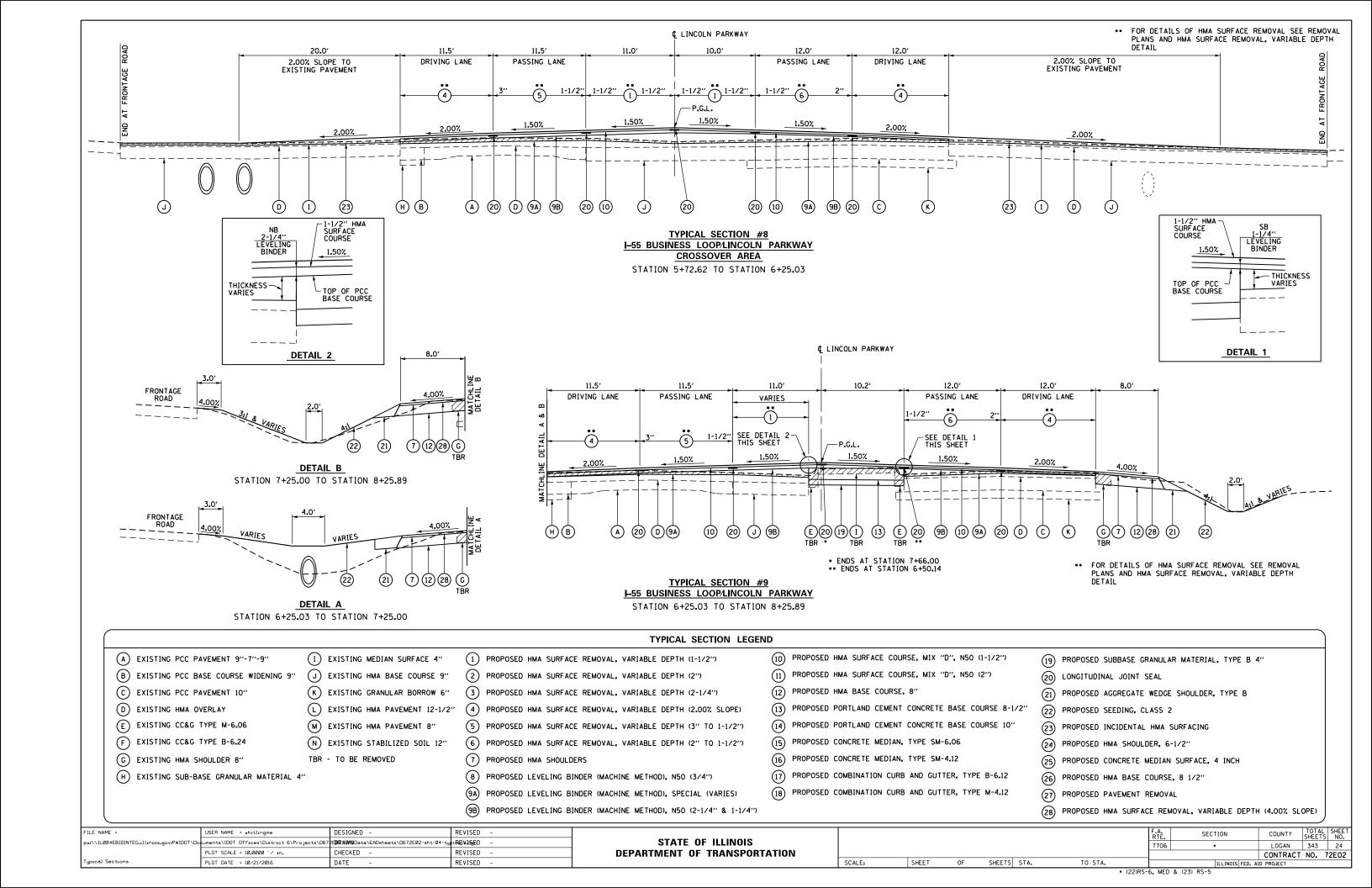
- (5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")
- (6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")
- (7) PROPOSED HMA SHOULDERS
- 8 PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")
- (9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)
- (9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

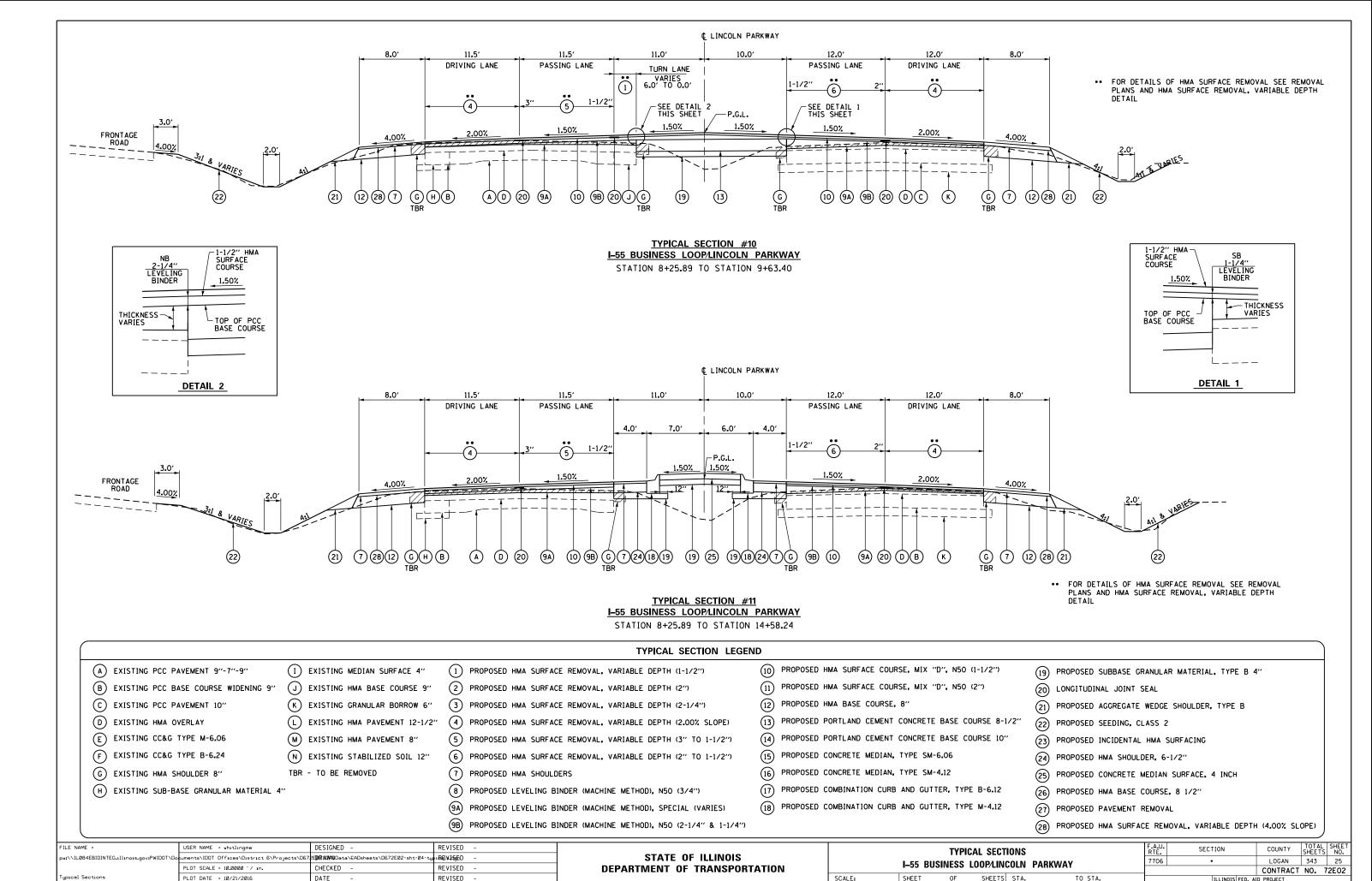
- (10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")
- (11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")
- (12) PROPOSED HMA BASE COURSE, 8"
- (13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"
- (14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"
- (15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06
- (16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12
- (17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12
- 18 PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

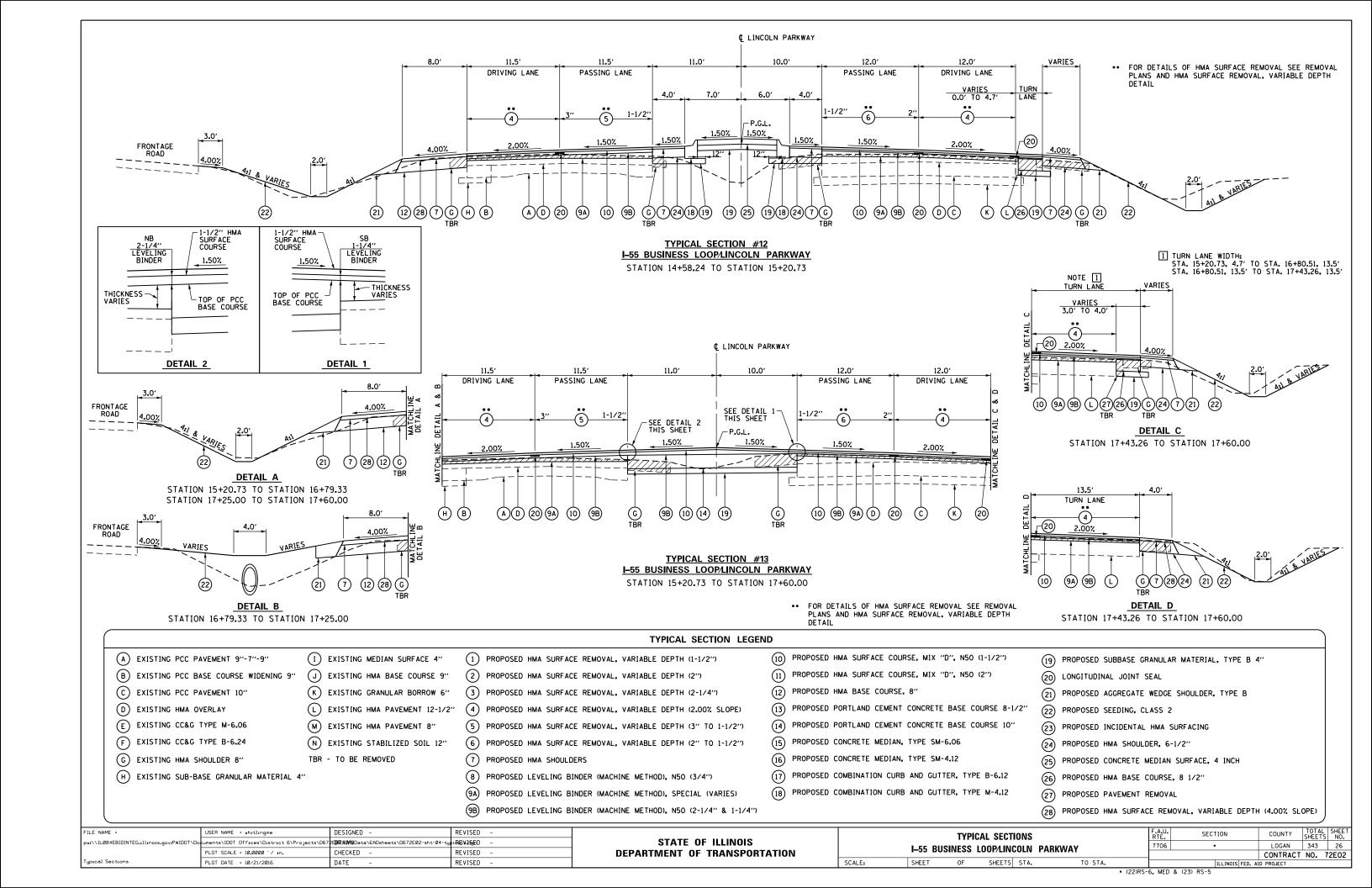
- (19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
- 20 LONGITUDINAL JOINT SEAL
- (21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- (22) PROPOSED SEEDING, CLASS 2
- (23) PROPOSED INCIDENTAL HMA SURFACING
- PROPOSED HMA SHOULDER, 6-1/2"
- PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- (26) PROPOSED HMA BASE COURSE, 8 1/2"
- (27) PROPOSED PAVEMENT REMOVAL
- (28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

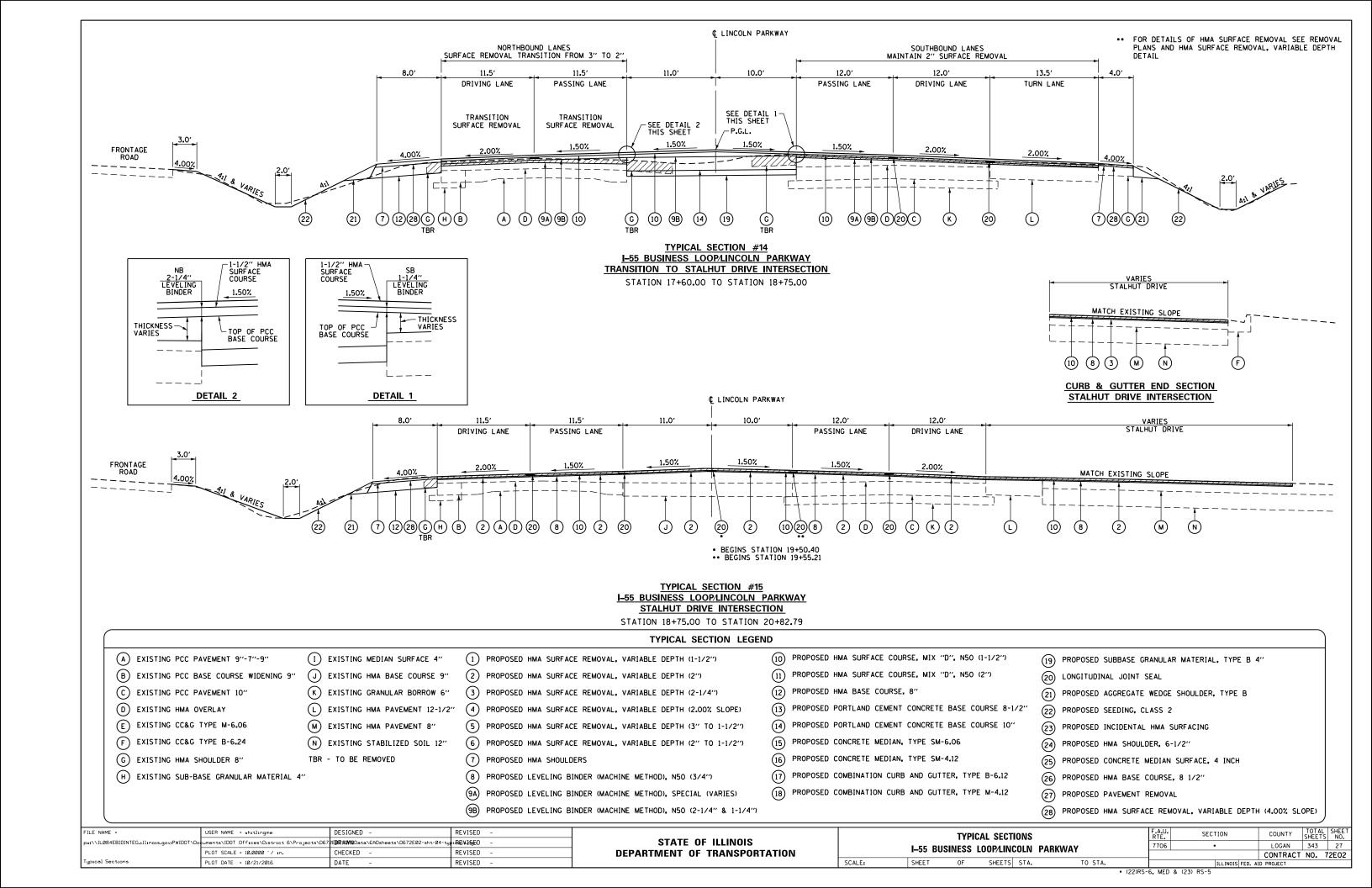
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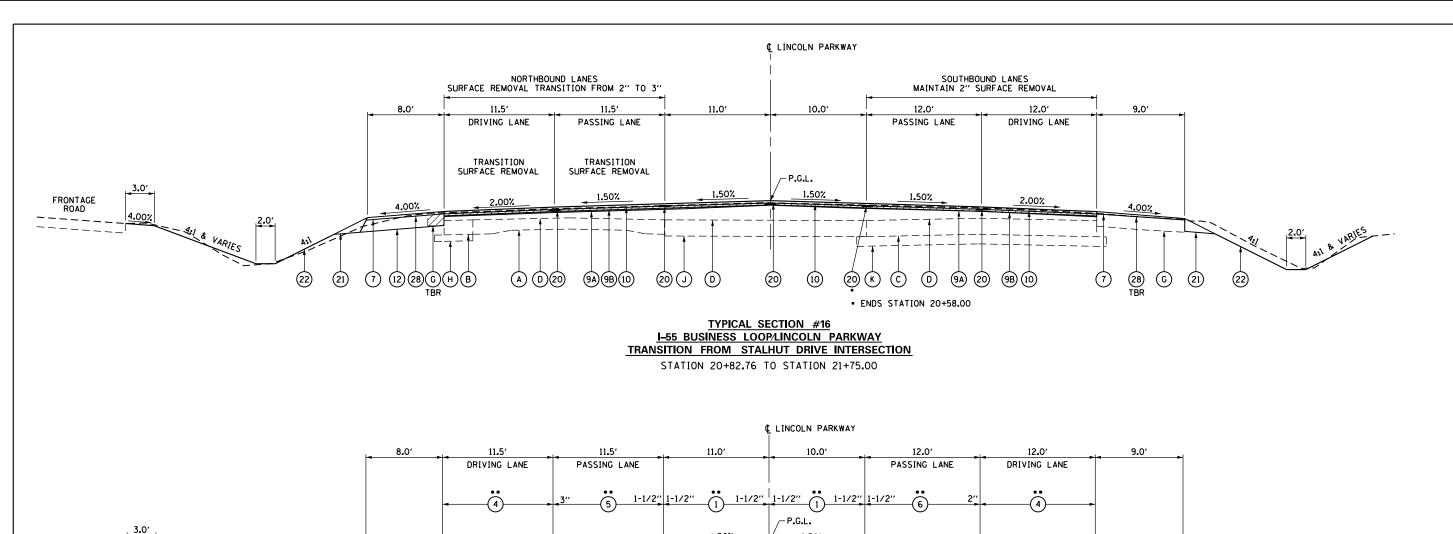
• (22)RS-6, MED & (23) RS-5

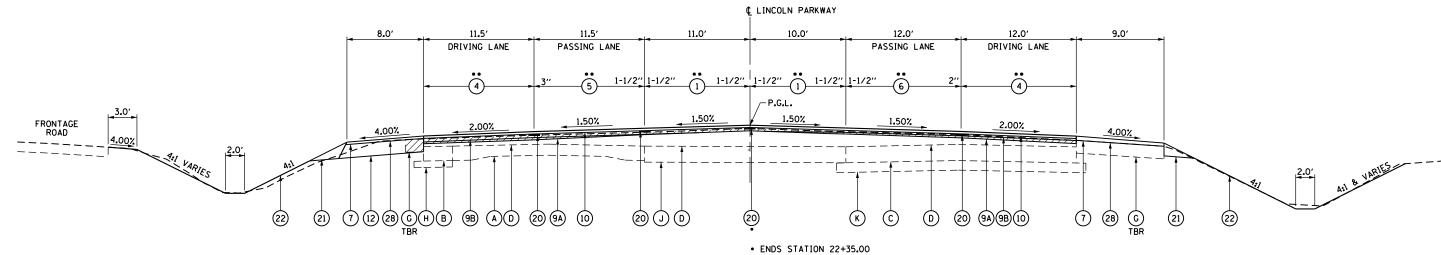












TYPICAL SECTION #17 I-55 BUSINESS LOOP/LINCOLN PARKWAY

STATION 21+75.00 TO STATION 24+04.48

		TYPICAL SECTION	LEGEND
ISTING PCC PAVEMENT 9"-7"-9"	() EXISTING MEDIAN SURFACE 4"	1) PROPOSED HMA SURFACE REMOVAL. VARIABLE DEPTH (1-1/2")	(10

- (A) EXIS
- (B) EXISTING PCC BASE COURSE WIDENING 9"
- C EXISTING PCC PAVEMENT 10"
- D EXISTING HMA OVERLAY
- E EXISTING CC&G TYPE M-6.06
- F EXISTING CC&G TYPE B-6.24
- (G) EXISTING HMA SHOULDER 8"

- (H) EXISTING SUB-BASE GRANULAR MATERIAL 4"

- (J) EXISTING HMA BASE COURSE 9"
- K EXISTING GRANULAR BORROW 6"
- L EXISTING HMA PAVEMENT 12-1/2"
- (M) EXISTING HMA PAVEMENT 8"
- (N) EXISTING STABILIZED SOIL 12"
- TBR TO BE REMOVED

- 4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)

2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")

3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")

- (5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")
- (6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")
- (7) PROPOSED HMA SHOULDERS
- 8 PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")
- (9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)
- (9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

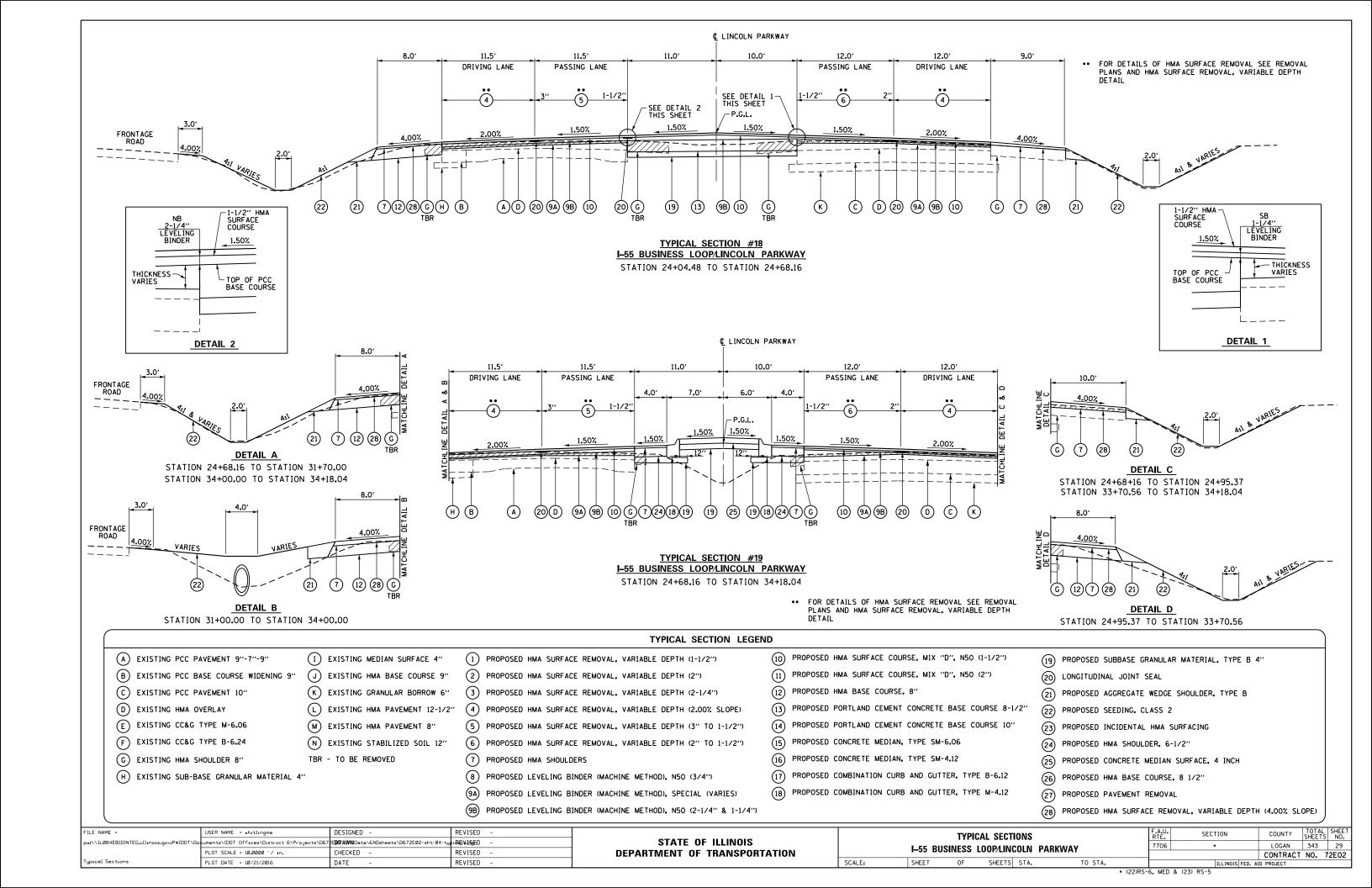
- (10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")
- (11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")
- (12) PROPOSED HMA BASE COURSE, 8"
- (13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"
- (14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"
- (15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06
- (16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12
- (17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12
- (18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

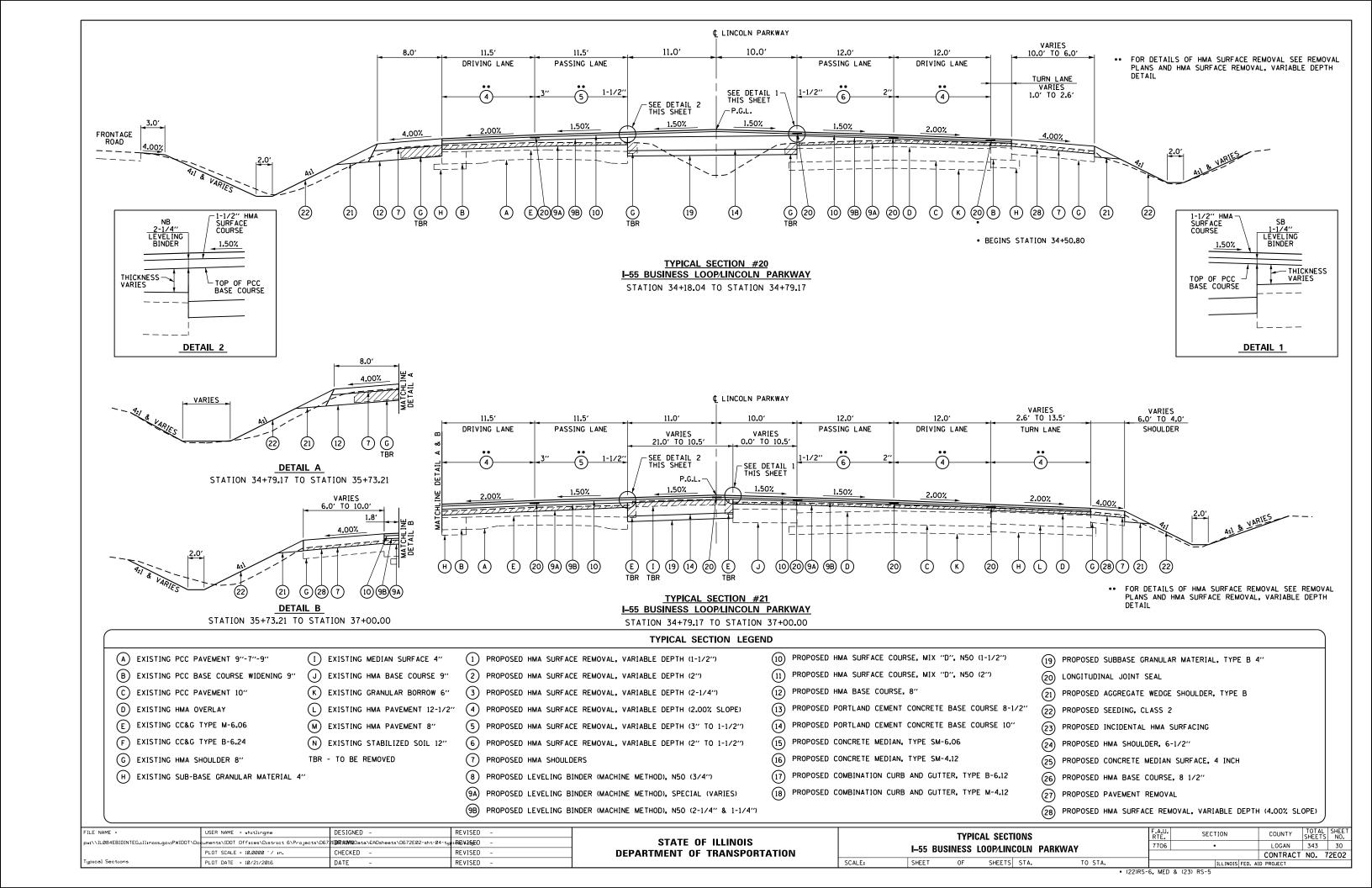
(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"

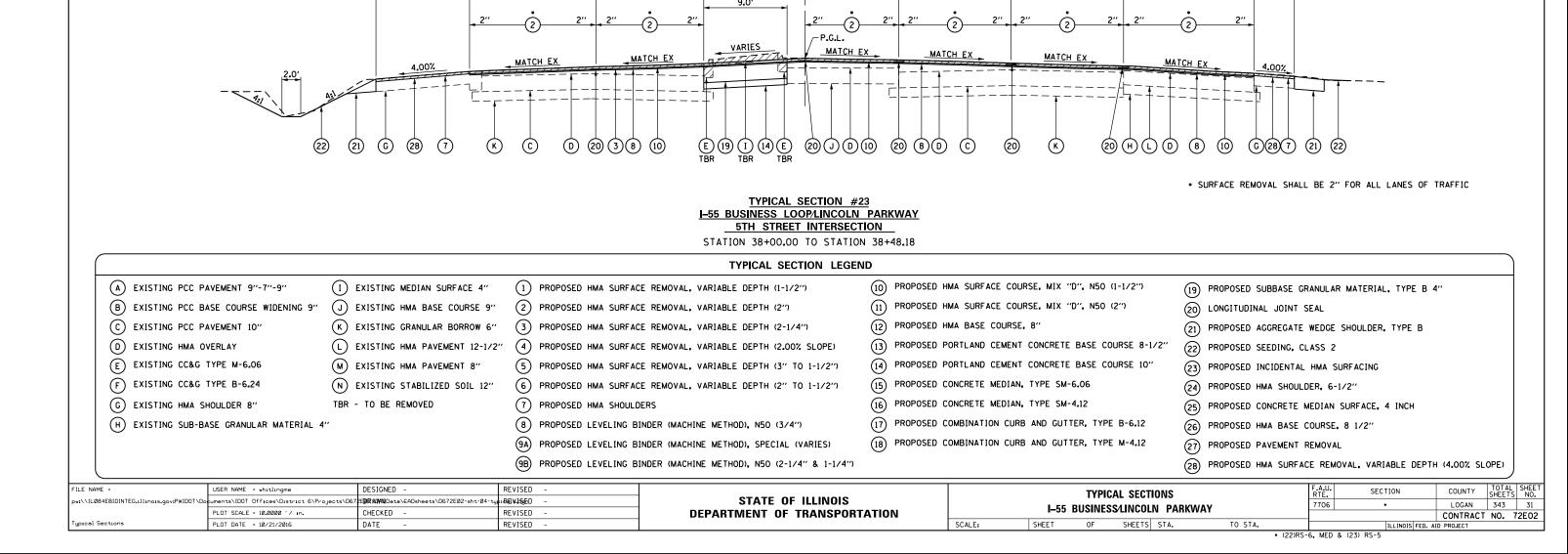
FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH

- 20 LONGITUDINAL JOINT SEAL
- (21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- (22) PROPOSED SEEDING, CLASS 2 (23) PROPOSED INCIDENTAL HMA SURFACING
- PROPOSED HMA SHOULDER, 6-1/2"
- PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- (26) PROPOSED HMA BASE COURSE, 8 1/2"
- (27) PROPOSED PAVEMENT REMOVAL
- (28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

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STATION 37+00.00 TO STATION 38+00.00 LINCOLN PARKWAY 11.0 PASSING LANE PASSING LANE TURN LANE DRIVING LANE DRIVING LANE 9.0'

TYPICAL SECTION #22 I-55 BUSINESS LOOP/LINCOLN PARKWAY TRANSITION TO 5TH STREET INTERSECTION

LINCOLN PARKWAY

10.0'

1.50%

11.0'

P.G.L.-1.50%

NORTHBOUND LANES
SURFACE REMOVAL TRANSITION FROM 3" TO 2"

D 20

11.5

PASSING LANE

TRANSITION

SURFACE REMOVAL

1.50%

9A 9B 10

13.51

DRIVING LANE

TRANSITION

SURFACE REMOVAL

10.0'

4.00%

SOUTHBOUND LANES MAINTAIN 2" SURFACE REMOVAL

12.0'

DRIVING LANE

2.00%

13.5

TURN LANE

2.00%

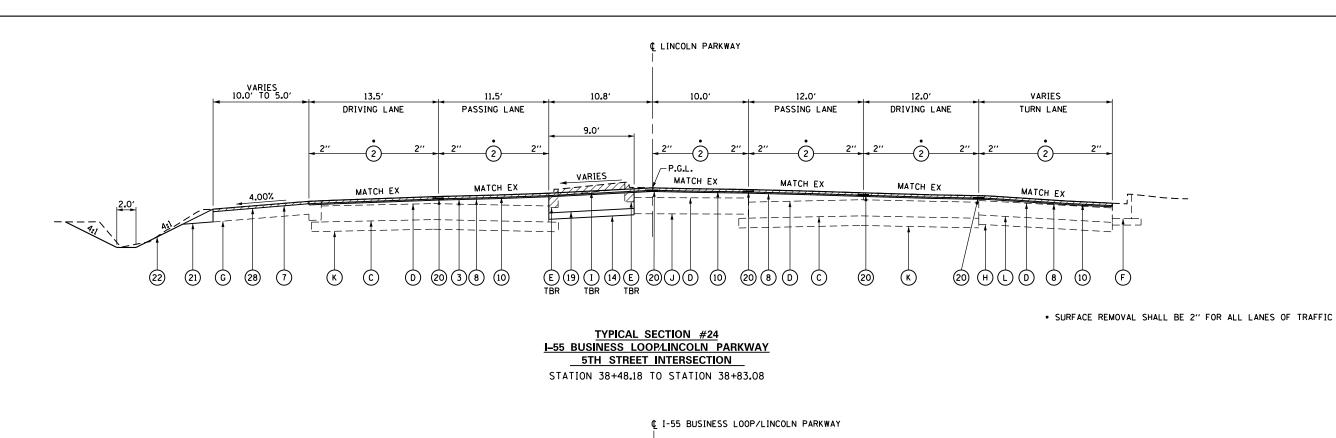
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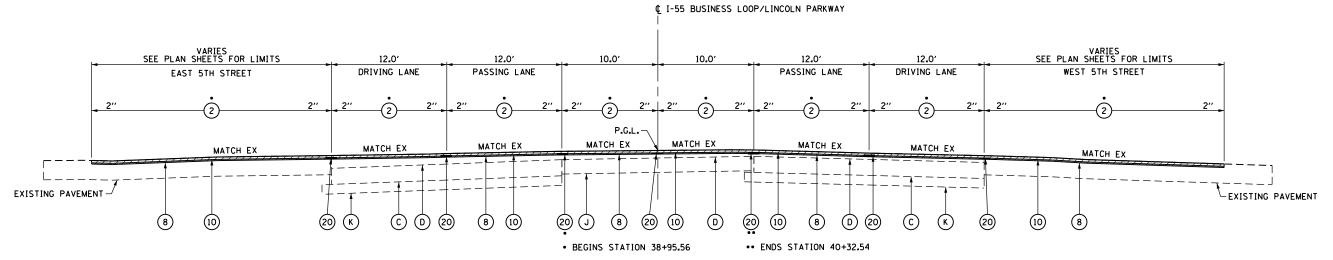
4.00%

12.0'

PASSING LANE

1.50%





TYPICAL SECTION #25 I-55 BUSINESS LOOP/LINCOLN PARKWAY 5TH STREET INTERSECTION STATION 38+83.08 TO STATION 40+39.89

• SURFACE REMOVAL SHALL BE 2" FOR ALL LANES OF TRAFFIC 5TH STREET INCLUDED

TYPICAL SECTION LEGEND (A) EXISTING PCC PAVEMENT 9"-7"-9" I EXISTING MEDIAN SURFACE 4" 1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")

- (B) EXISTING PCC BASE COURSE WIDENING 9"
- C EXISTING PCC PAVEMENT 10"
- D EXISTING HMA OVERLAY
- E EXISTING CC&G TYPE M-6.06
- F EXISTING CC&G TYPE B-6.24
- (G) EXISTING HMA SHOULDER 8"
- (H) EXISTING SUB-BASE GRANULAR MATERIAL 4"

- J EXISTING HMA BASE COURSE 9"
- K EXISTING GRANULAR BORROW 6"
- L EXISTING HMA PAVEMENT 12-1/2"
- (M) EXISTING HMA PAVEMENT 8"
- (N) EXISTING STABILIZED SOIL 12"
- TBR TO BE REMOVED

- 7) PROPOSED HMA SHOULDERS (8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")
- (9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)

2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")

3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")

4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)

(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")

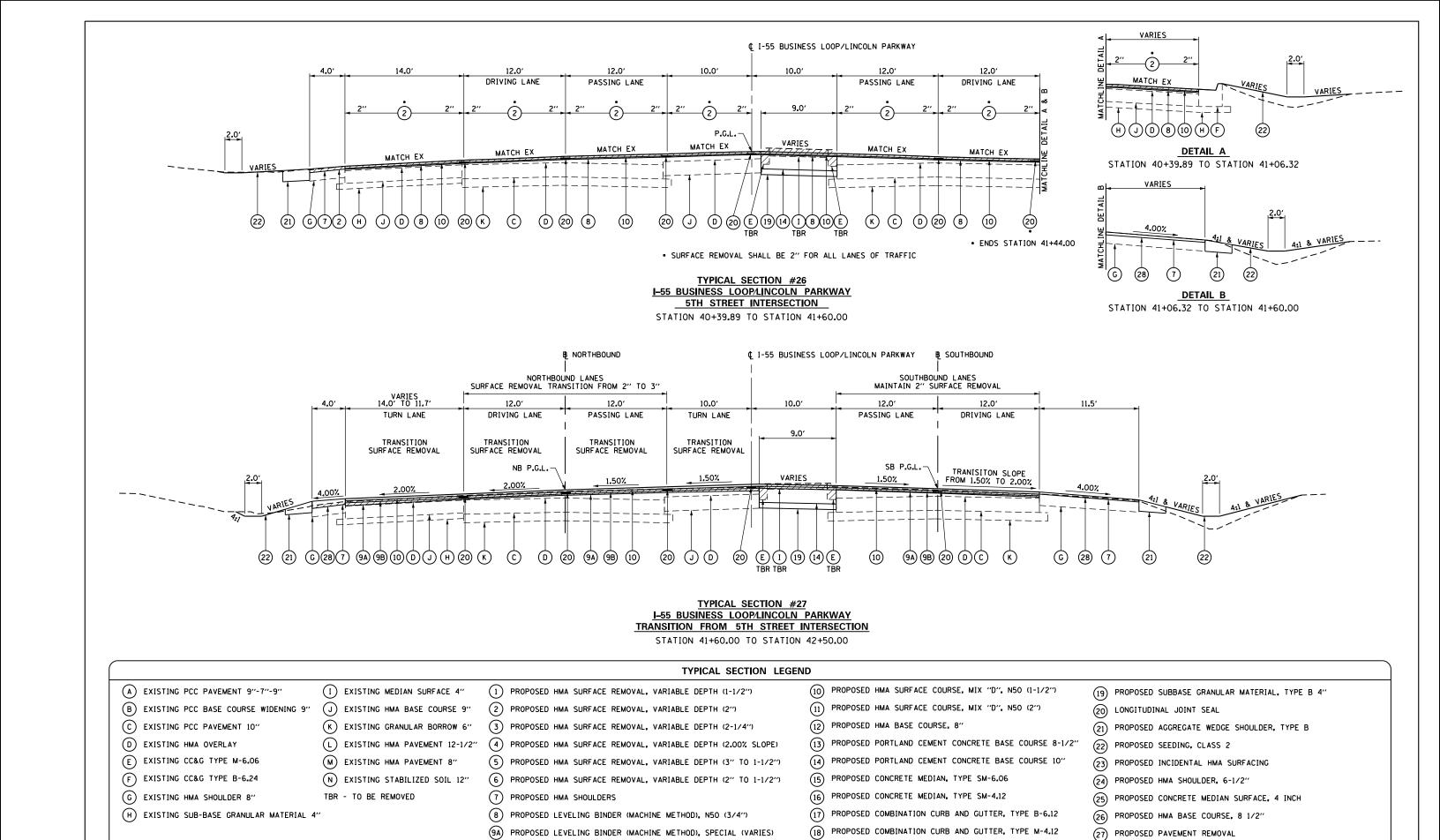
PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")

(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

- (10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")
- (11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")
- (12) PROPOSED HMA BASE COURSE, 8"
- (13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"
- (14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"
- (15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06
- (16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12
- (17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12
- (18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

- (19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
- 20 LONGITUDINAL JOINT SEAL
- (21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- (22) PROPOSED SEEDING, CLASS 2
- PROPOSED INCIDENTAL HMA SURFACING
- (24) PROPOSED HMA SHOULDER, 6-1/2"
- PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- (26) PROPOSED HMA BASE COURSE, 8 1/2"
- (27) PROPOSED PAVEMENT REMOVAL
- (28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

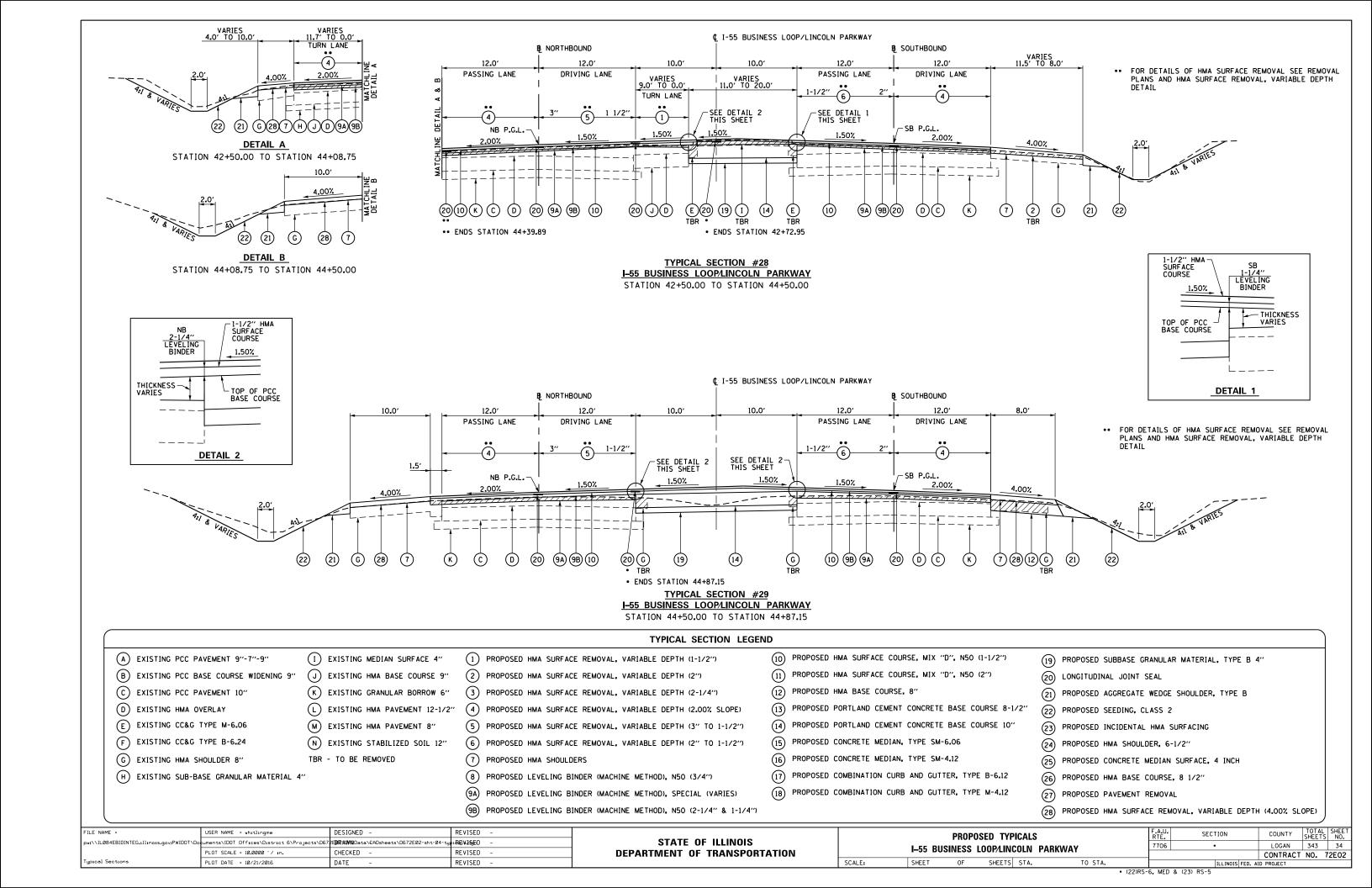
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							• (22)RS-6, MED & (23) RS-5									

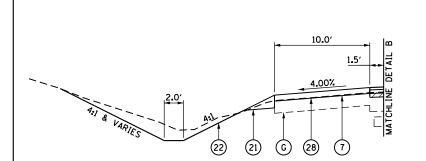


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										• (22)	RS-6. MED 8	k (23) RS-5				•

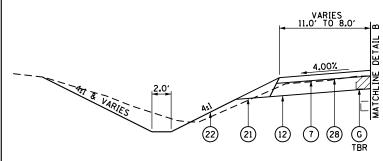
(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

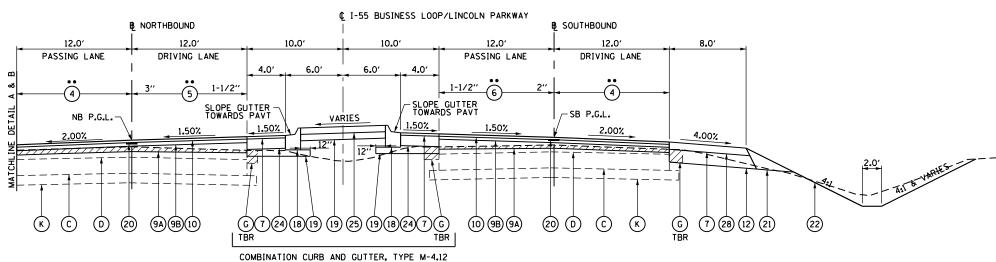




DETAIL A STATION 44+87.15 TO STATION 45+15.17



DETAIL B STATION 45+15.17 TO STATION 47+00.00

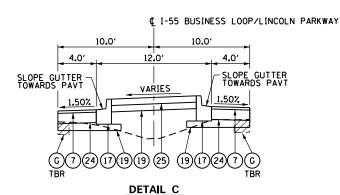


LEFT AND RIGHT STA. 44+87.15 TO STA. 46+42.00 SEE DETAIL C

COMBINATION CURB AND GUTTER, TYPE B-6.12 RIGHT STA. 46+42.00 TO RIGHT STA. 46+50.00 LEFT STA. 46+42.00 TO LEFT STA. 47+00.00

TYPICAL SECTION #30 I-55 BUSINESS LOOP/LINCOLN PARKWAY

RIGHT STATION 44+87.15 TO RIGHT STATION 46+50.00 LEFT STATION 44+87.15 TO LEFT STATION 47+00.00



RIGHT STATION 46+42.00 TO RIGHT STATION 46+50.00 LEFT STATION 46+42.00 TO LEFT STATION 47+00.00

TYPICAL SECTION LEGEND

- A EXISTING PCC PAVEMENT 9"-7"-9"
- (B) EXISTING PCC BASE COURSE WIDENING 9"
- C EXISTING PCC PAVEMENT 10"
- D EXISTING HMA OVERLAY
- E EXISTING CC&G TYPE M-6.06
- F EXISTING CC&G TYPE B-6.24
- (G) EXISTING HMA SHOULDER 8"
- H EXISTING SUB-BASE GRANULAR MATERIAL 4"

- I EXISTING MEDIAN SURFACE 4"
- J EXISTING HMA BASE COURSE 9"
- K EXISTING GRANULAR BORROW 6"
- L) EXISTING HMA PAVEMENT 12-1/2"
- M EXISTING HMA PAVEMENT 8"
- (N) EXISTING STABILIZED SOIL 12"

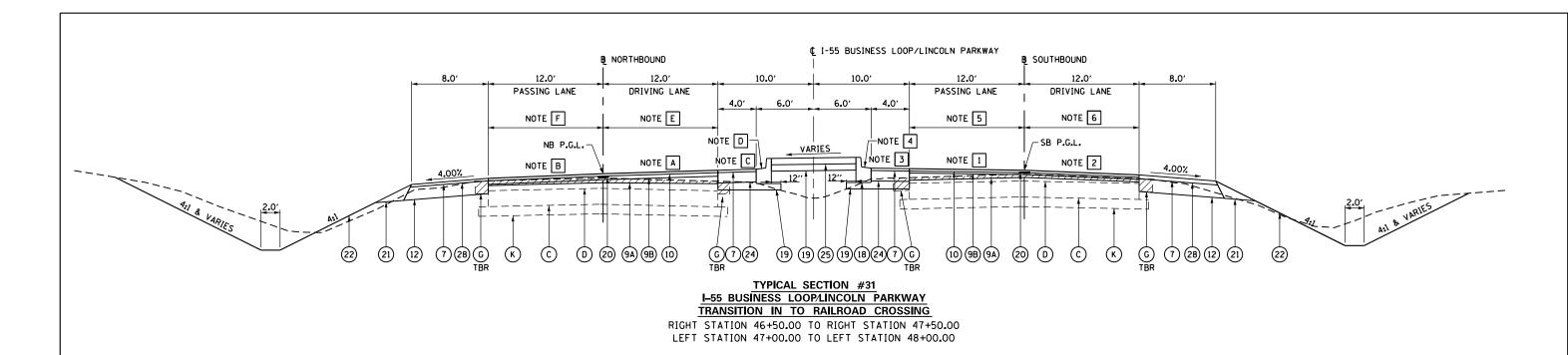
- TBR TO BE REMOVED

- 1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")
- 2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")
- 3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")
- 4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)
- (5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")
- (6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")
- (7) PROPOSED HMA SHOULDERS
- (8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")
- (9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)
- (9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

- (10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")
- (11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")
- (12) PROPOSED HMA BASE COURSE, 8"
- (13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"
- (14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"
- (15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06
- (16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12
- (17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12
- (18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

- (19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
- 20 LONGITUDINAL JOINT SEAL
- (21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- 22) PROPOSED SEEDING, CLASS 2
- (23) PROPOSED INCIDENTAL HMA SURFACING
- PROPOSED HMA SHOULDER, 6-1/2"
- (25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- (26) PROPOSED HMA BASE COURSE, 8 1/2"
- (27) PROPOSED PAVEMENT REMOVAL
- (28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

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LEFT SIDE NOTES:

NOTE A LEFT PASSING LANE SLOPE - TRANSITION SLOPE STATION 47+00.00 (-1.50%) TO STATION 48+00.00 (MATCH EXISTING)

NOTE B LEFT DRIVING LANE SLOPE - TRANSITION SLOPE STATION 47+00.00 (-2.00%) TO STATION 48+00.00 (MATCH EXISTING)

NOTE C LEFT INSIDE SHOULDER SLOPE - SHALL BE THE SAME AS THE PASSING LANE SLOPE

NOTE D LEFT CC&C TYPE - LEFT STATION 47+00.00 TO LEFT STATION 47+42.00 - TYPE B-6.12

LEFT STATION 47+42.00 TO LEFT STATION 48+00.00 - TYPE M-4.12 LEFT GUTTER SLOPE - LEFT STATION 47+00.00 TO LEFT STATION 47+65.00 GUTTER SHALL

SLOPE TOWARDS PAVEMENT.

LEFT STATION 47+65.00 TO LEFT STATION 47+75.00 GUTTER SHALL TRANSITION TOWARDS PAVEMENT TO AWAY FROM PAVEMENT. LEFT STATION 47+75.00 TO LEFT STATION 48+00.00 GUTTER SHALL SLOPE AWAY FROM PAVEMENT.

NOTE E LEFT PASSING LANE MILLING - TRANSSITION FROM VARIABLE DEPTH SURFACE REMOVAL LEFT STATION 47+00.00 TO 2" SURFACE REMOVAL LEFT STATION 48+00.00

NOTE F LEFT DRIVING LANE MILLING - TRANSITION FROM 2.00% SLOPED SURFACE REMOVAL LEFT STATION 47+00.00 TO 2" SURFACE REMOVAL LEFT STATION 48+00.00

RIGHT SIDE NOTES:

NOTE 1 RIGHT PASSING LANE SLOPE - TRANSITION SLOPE STATION 46+50.00 (-1.50%) TO STATION 47+50.00 (MATCH EXISTING)

NOTE 2 RIGHT DRIVING LANE SLOPE - TRANSITION SLOPE STATION 46+50.00 (-2.00%) TO STATION 47+50.00 (MATCH EXISTING)

NOTE 3 RIGHT INSIDE SHOULDER SLOPE - SHALL BE THE SAME AS THE PASSING LANE SLOPE

NOTE 4 RIGHT CC&C TYPE - RIGHT STATION 46+50.00 TO RIGHT STATION 47+50.00 - TYPE B-6.12

RIGHT GUTTER SLOPE - RIGHT STATION 46+50.00 TO RIGHT STATION 46+90.00 GUTTER SHALL

SLOPE TOWARDS PAVEMENT. RIGHT STATION 46+90.00 TO RIGHT STATION 47+00.00 GUTTER SHALL TRANSITION TOWARDS PAVEMENT TO AWAY FROM PAVEMENT.

RIGHT STATION 47+00.00 TO RIGHT STATION 47+50.00 GUTTER SHALL SLOPE AWAY FROM PAVEMENT.

NOTE 5 RIGHT PASSING LANE MILLING - TRANSITION FROM VARIABLE DEPTH SURFACE REMOVAL RIGHT STATION 46+50.00 TO 2" SURFACE REMOVAL RIGHT STATION 47+50.00

NOTE 6 RIGHT DRIVING LANE MILLING - TRANSITION FROM 2.00% SLOPED SURFACE REMOVAL TO 2" SURFACE REMOVAL FROM RIGHT STATION 46+50.00 TO RIGHT STATION 47+50.00

TYPICAL SECTION LEGEND

A EXISTING PCC PAVEMENT 9"-7"-9"

(B) EXISTING PCC BASE COURSE WIDENING 9"

(C) EXISTING PCC PAVEMENT 10"

(D) EXISTING HMA OVERLAY

E EXISTING CC&G TYPE M-6.06

F EXISTING CC&G TYPE B-6.24

(G) EXISTING HMA SHOULDER 8"

(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"

I EXISTING MEDIAN SURFACE 4"

J EXISTING HMA BASE COURSE 9"

(K) EXISTING GRANULAR BORROW 6"

L EXISTING HMA PAVEMENT 12-1/2"

(M) EXISTING HMA PAVEMENT 8"

(N) EXISTING STABILIZED SOIL 12"

TBR - TO BE REMOVED

1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")

2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")

(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")

4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE) (5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")

(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")

7) PROPOSED HMA SHOULDERS

(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")

(9A) PROPOSED LEVELING BINDER (MACHINE METHOD). SPECIAL (VARIES)

(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")

(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")

(12) PROPOSED HMA BASE COURSE, 8"

(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"

(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"

(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06 (16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12

(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12

(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"

(20) LONGITUDINAL JOINT SEAL

(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B

(22) PROPOSED SEEDING, CLASS 2

(23) PROPOSED INCIDENTAL HMA SURFACING

(24) PROPOSED HMA SHOULDER, 6-1/2"

PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH

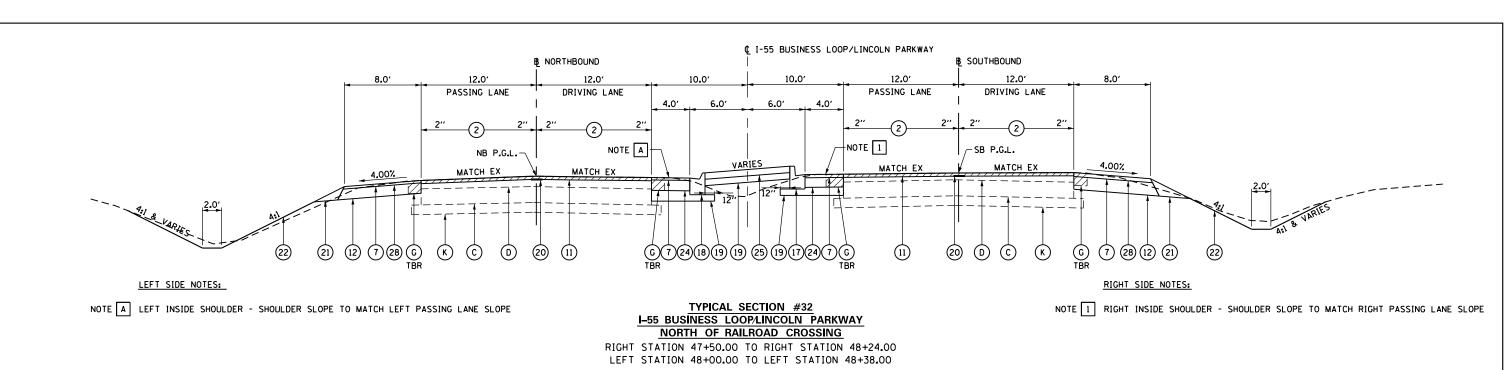
(26) PROPOSED HMA BASE COURSE, 8 1/2"

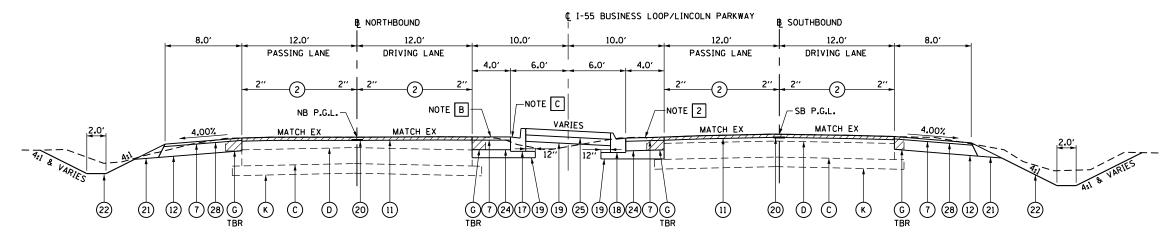
(27) PROPOSED PAVEMENT REMOVAL

(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

(22)RS-6, MED & (23) RS-5

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LEFT SIDE NOTES:

NOTE B LEFT INSIDE SHOULDER - SHOULDER SLOPE TO MATCH LEFT PASSING LANE SLOPE

NOTE C LEFT GUTTER SLOPE - LEFT STATION 48+49.00 TO LEFT STATION 48+80.00 GUTTER SHALL SLOPE AWAY FROM PAVEMENT.

LEFT STATION 48+80.00 TO LEFT STATION 48+90.00 GUTTER SHALL TRANSITION AWAY FROM PAVEMENT TOWARDS PAVEMENT.

TYPICAL SECTION #33 I-55 BUSINESS LOOP/LINCOLN PARKWAY SOUTH OF RAILROAD CROSSING

RIGHT STATION 48+21.00 TO RIGHT STATION 48+75.00 LEFT STATION 48+49.00 TO LEFT STATION 48+90.00

NOTE 2 RIGHT INSIDE SHOULDER - SHOULDER SLOPE TO MATCH RIGHT PASSING LANE SLOPE

TYPICAL SECTION LEGEND

- A EXISTING PCC PAVEMENT 9"-7"-9" I EXISTING MEDIAN SURFACE 4"
- (B) EXISTING PCC BASE COURSE WIDENING 9"
- C EXISTING PCC PAVEMENT 10"
- D EXISTING HMA OVERLAY
- E EXISTING CC&G TYPE M-6.06
- F EXISTING CC&G TYPE B-6.24
- (G) EXISTING HMA SHOULDER 8"
- (H) EXISTING SUB-BASE GRANULAR MATERIAL 4"

- J EXISTING HMA BASE COURSE 9"
- (K) EXISTING GRANULAR BORROW 6"
- L EXISTING HMA PAVEMENT 12-1/2"
- (M) EXISTING HMA PAVEMENT 8"
- (N) EXISTING STABILIZED SOIL 12"

- TBR TO BE REMOVED

- 1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")
- (2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")
- PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")
- PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)
- PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")
- (6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")

(7) PROPOSED HMA SHOULDERS

- (8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")
- (9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)
- (9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

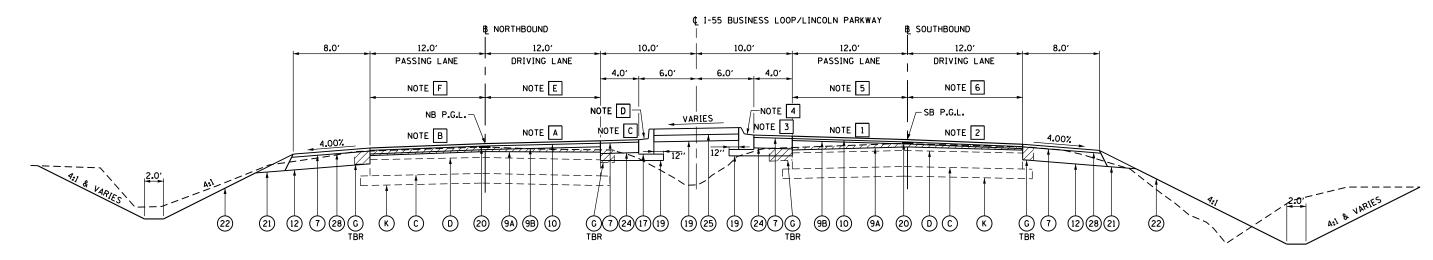
- (10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")
- (11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")
- (12) PROPOSED HMA BASE COURSE, 8"
- (13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"
- (14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"
- (15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06
- (16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12
- (17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12
- (18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

- (19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
- (20) LONGITUDINAL JOINT SEAL
- (21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- (22) PROPOSED SEEDING, CLASS 2

RIGHT SIDE NOTES:

- PROPOSED INCIDENTAL HMA SURFACING
- (24) PROPOSED HMA SHOULDER, 6-1/2"
- PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- (26) PROPOSED HMA BASE COURSE, 8 1/2"
- 27) PROPOSED PAVEMENT REMOVAL
- (28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

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TYPICAL SECTION #34 I-55 BUSINESS LOOP/LINCOLN PARKWAY TRANSITION OUT FROM RAILROAD CROSSING

RIGHT STATION 48+75.00 TO RIGHT STATION 49+75.00 LEFT STATION 48+90.00 TO LEFT STATION 49+90.00

LEFT SIDE NOTES:

NOTE A LEFT PASSING LANE SLOPE - TRANSITION SLOPE STATION 48+90.00 (MATCH EXISTING) TO STATION 49+90.00 (-1.50%)

NOTE B LEFT DRIVING LANE SLOPE - TRANSITION SLOPE STATION 48+90.00 (MATCH EXISTING%) TO STATION 49+90.00 (-2.00%)

NOTE C LEFT INSIDE SHOULDER SLOPE - SHALL BE THE SAME AS THE PASSING LANE SLOPE

NOTE D LEFT CC&C TYPE - LEFT STATION 48+90.00 TO LEFT STATION 49+90.00 - TYPE B-6.12

LEFT GUTTER SLOPE - LEFT STATION 48+90.00 TO LEFT STATION 49+90.00 GUTTER SHALL SLOPE TOWARDS PAVEMENT.

NOTE E LEFT PASSING LANE MILLING - TRANISITION FROM 2" SURFACE REMOVAL LEFT STATION 48+90.00 TO VARIABLE DEPTH SURFACE REMOVAL LEFT STATION 49+90.00

NOTE F LEFT DRIVING LANE MILLING - TRANSITION FROM 2" SURFACE REMOVAL LEFT STATION 48+90.00 TO 2.00% SURFACE REMOVAL LEFT STATION 49+90.00

RIGHT SIDE NOTES:

NOTE 1 RIGHT PASSING LANE SLOPE - TRANSITION SLOPE STATION 48+75.00 (MATCH EXISTING) TO STATION 49+75.00 (-1.50%)

NOTE 2 RIGHT DRIVING LANE SLOPE - TRANSITION SLOPE STATION 48+75.00 (MATCH EXISTING) TO STATION 49+75.00 (-2.00%)

NOTE 3 RIGHT INSIDE SHOULDER SLOPE - SHALL BE THE SAME AS THE PASSING LANE SLOPE

NOTE 4 RIGHT CC&G TYPE - RIGHT STATION 48+75.00 TO RIGHT STATION 49+17.00 - TYPE M-4.12 RIGHT STATION 49+17.00 TO RIGHT STATION 49+75.00 - TYPE B-6.12

RIGHT GUTTER SLOPE - RIGHT STATION 48+75.00 TO RIGHT STATION 46+15.00 GUTTER SHALL

SLOPE AWAY FROM PAVEMENT. RIGHT STATION 49+15.00 TO RIGHT STATION 49+25.00 GUTTER SHALL TRANSITION AWAY FROM PAVEMENT TOWARDS PAVEMENT.

RIGHT STATION 49+25.00 TO RIGHT STATION 49+75.00 GUTTER SHALL SLOPE TOWARDS PAVEMENT.

NOTE 5 RIGHT PASSING LANE MILLING - TRANSITION FROM 2" SURFACE REMOVAL RIGHT STATION 48+75.00 TO VARIABLE DEPTH SURFACE REMOVAL RIGHT STATION 49+75.00

NOTE 6 RIGHT DRIVING LANE MILLING - TRANSITION FROM 2" SURFACE REMOVAL TO 2.00% VARIABLE DEPTH FROM RIGHT STATION 48+75.00 TO RIGHT STATION 49+75.00

TYPICAL SECTION LEGEND

- A EXISTING PCC PAVEMENT 9"-7"-9"
- (B) EXISTING PCC BASE COURSE WIDENING 9"
- C EXISTING PCC PAVEMENT 10"
- (D) EXISTING HMA OVERLAY
- E EXISTING CC&G TYPE M-6.06
- F EXISTING CC&G TYPE B-6.24
- (G) EXISTING HMA SHOULDER 8"
- (H) EXISTING SUB-BASE GRANULAR MATERIAL 4"

- I EXISTING MEDIAN SURFACE 4"
- J EXISTING HMA BASE COURSE 9"
- (K) EXISTING GRANULAR BORROW 6"
- L EXISTING HMA PAVEMENT 12-1/2"
- (M) EXISTING HMA PAVEMENT 8"
- (N) EXISTING STABILIZED SOIL 12"
- TBR TO BE REMOVED

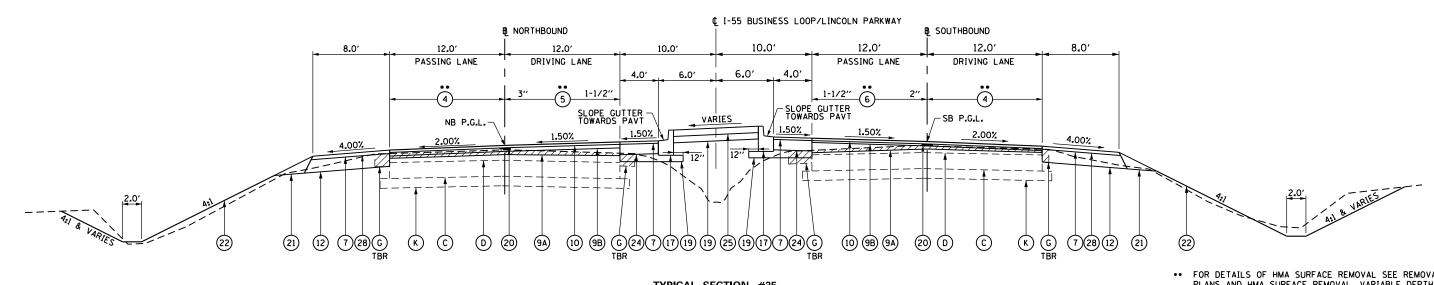
- 1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")
- (2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")
- (3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")
- 4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)
- (5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")
- (6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")
- (7) PROPOSED HMA SHOULDERS
- (8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")
- (9A) PROPOSED LEVELING BINDER (MACHINE METHOD). SPECIAL (VARIES)
- (9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

- (10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")
- (11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")
- (12) PROPOSED HMA BASE COURSE, 8"
- (13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"
- (14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"
- (15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06
- (16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12
- (17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12
- (18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

- (19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
- (20) LONGITUDINAL JOINT SEAL
- (21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- (22) PROPOSED SEEDING, CLASS 2
- (23) PROPOSED INCIDENTAL HMA SURFACING
- (24) PROPOSED HMA SHOULDER, 6-1/2"
- (25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- (26) PROPOSED HMA BASE COURSE, 8 1/2"
- (27) PROPOSED PAVEMENT REMOVAL
- (28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

• (22)RS-6, MED & (23) RS-5

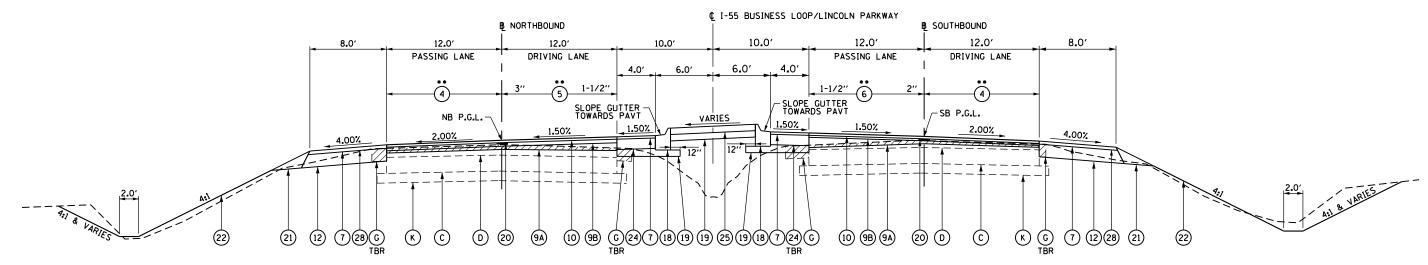
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TYPICAL SECTION #35 I-55 BUSINESS LOOP/LINCOLN PARKWAY

RIGHT STATION 49+75.00 TO RIGHT STATION 50+17.00 LEFT STATION 49+90.00 TO LEFT STATION 50+17.00

•• FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL



•• FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL

TYPICAL SECTION #36 I-55 BUSINESS LOOP/LINCOLN PARKWAY STATION 51+17.00 TO STATION 51+80.00

TYPICAL SECTION LEGEND

- A EXISTING PCC PAVEMENT 9"-7"-9"
- (B) EXISTING PCC BASE COURSE WIDENING 9"
- C EXISTING PCC PAVEMENT 10"
- D EXISTING HMA OVERLAY
- E EXISTING CC&G TYPE M-6.06
- F EXISTING CC&G TYPE B-6.24
- (G) EXISTING HMA SHOULDER 8"
- (H) EXISTING SUB-BASE GRANULAR MATERIAL 4"

- I EXISTING MEDIAN SURFACE 4"
- J EXISTING HMA BASE COURSE 9"
- (K) EXISTING GRANULAR BORROW 6"
- L EXISTING HMA PAVEMENT 12-1/2"
- (M) EXISTING HMA PAVEMENT 8"
- (N) EXISTING STABILIZED SOIL 12"

- TBR TO BE REMOVED

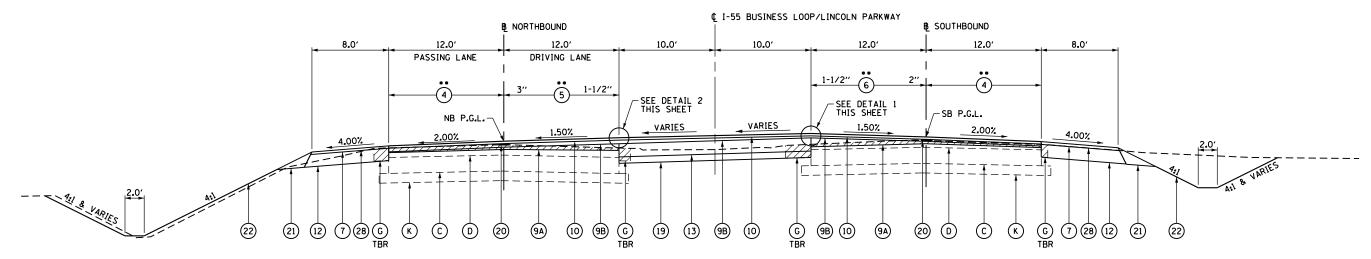
- 1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")
- 2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")
- 3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")
- 4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)
- PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")
- (6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")
- 7) PROPOSED HMA SHOULDERS
- 8 PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")
- (9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)
- (9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

- (10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")
- (11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")
- (12) PROPOSED HMA BASE COURSE, 8"
- (13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"
- (14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"
- (15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06
- (16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12
- (17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12
- 18 PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

- (19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
- (20) LONGITUDINAL JOINT SEAL
- (21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- (22) PROPOSED SEEDING, CLASS 2
- PROPOSED INCIDENTAL HMA SURFACING
- (24) PROPOSED HMA SHOULDER, 6-1/2"
- PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- PROPOSED HMA BASE COURSE, 8 1/2"
- 27) PROPOSED PAVEMENT REMOVAL
- (28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -				TVPIC	AL SECT	TIONS		F.A.U.	SECTION	COUNTY	TOTAL SHEETS	SHEET
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	PLOT SCALE = 10.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	SCALE: SHEET OF SHEETS STA. TO STA.				AKKWAY			CONTRACT	T NO.	72E02	
Typical Sections	PLOT DATE = 10/21/2016	DATE -	REVISED -						TO STA.		ILLINOIS FE	D. AID PROJECT			

• (22)RS-6, MED & (23) RS-5

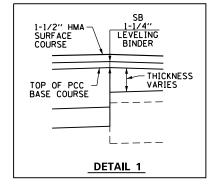


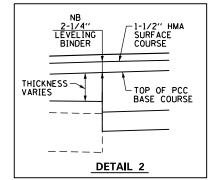
TYPICAL SECTION #37

I-55 BUSINESS LOOP/LINCOLN PARKWAY

STATION 51+80.00 TO STATION 52+40.00

•• FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL





TYPICAL SECTION LEGEND

- (A) EXISTING PCC PAVEMENT 9"-7"-9"
- (B) EXISTING PCC BASE COURSE WIDENING 9"
- C EXISTING PCC PAVEMENT 10"
- D EXISTING HMA OVERLAY
- (E) EXISTING CC&G TYPE M-6.06
- F EXISTING CC&G TYPE B-6.24
- (G) EXISTING HMA SHOULDER 8"
- (H) EXISTING SUB-BASE GRANULAR MATERIAL 4"

- I EXISTING MEDIAN SURFACE 4"
- J EXISTING HMA BASE COURSE 9"
- K EXISTING GRANULAR BORROW 6"
- L EXISTING HMA PAVEMENT 12-1/2"
- M EXISTING HMA PAVEMENT 8"
- \sim
- (N) EXISTING STABILIZED SOIL 12"
- TBR TO BE REMOVED
- " (2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")
 - 3 PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")

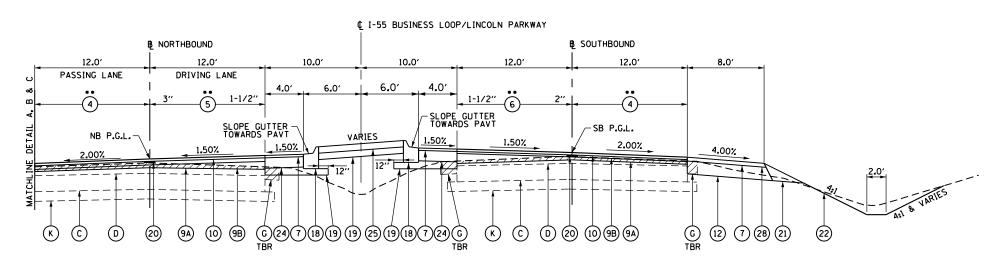
1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")

- 4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)
- 5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")
- (6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")
- (7) PROPOSED HMA SHOULDERS
 - 8 PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")
 - (9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)
 - 9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

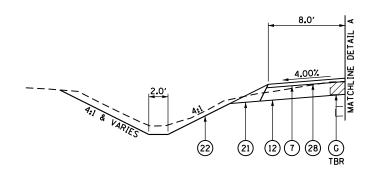
- (10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")
- PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")
- (12) PROPOSED HMA BASE COURSE, 8"
- (13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"
- (14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"
- 15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06
- (16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12
- (17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12
- 18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

- 19 PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
- (20) LONGITUDINAL JOINT SEAL
- (21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- (22) PROPOSED SEEDING, CLASS 2
- (23) PROPOSED INCIDENTAL HMA SURFACING
- (24) PROPOSED HMA SHOULDER, 6-1/2"
- (25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- (26) PROPOSED HMA BASE COURSE, 8 1/2"
- (27) PROPOSED PAVEMENT REMOVAL
- (28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

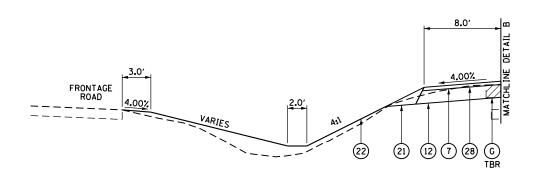
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Typical Sections	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FED.	AID PROJECT		
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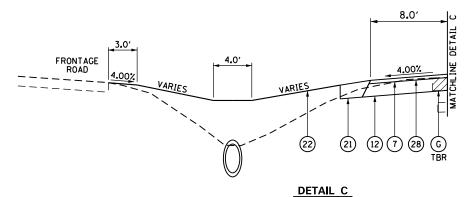
TYPICAL SECTION #38 I-55 BUSINESS LOOP/LINCOLN PARKWAY STATION 52+40.00 TO STATION 63+72.32



DETAIL A STATION 52+40.00 TO STATION 54+25.00 STATION 55+75.00 TO STATION 61+50.00



DETAIL B STATION 54+25.00 TO STATION 54+61.28 STATION 55+00.00 TO STATION 55+75.00 STATION 63+00.00 TO STATION 63+72.32



STATION 54+61.28 TO STATION 55+00.00 STATION 61+50.00 TO STATION 63+00.00

TYPICAL SECTION LEGEND

- (A) EXISTING PCC PAVEMENT 9"-7"-9"
- (B) EXISTING PCC BASE COURSE WIDENING 9"
- C EXISTING PCC PAVEMENT 10"
- D EXISTING HMA OVERLAY
- E EXISTING CC&G TYPE M-6.06
- F EXISTING CC&G TYPE B-6.24
- (G) EXISTING HMA SHOULDER 8"
- (H) EXISTING SUB-BASE GRANULAR MATERIAL 4"

- I EXISTING MEDIAN SURFACE 4"
- J EXISTING HMA BASE COURSE 9"
- K EXISTING GRANULAR BORROW 6"

- (N) EXISTING STABILIZED SOIL 12"

- L) EXISTING HMA PAVEMENT 12-1/2"
- (M) EXISTING HMA PAVEMENT 8"

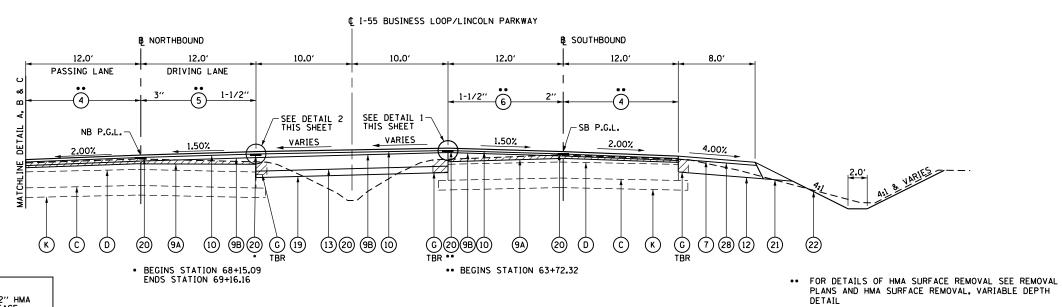
- TBR TO BE REMOVED

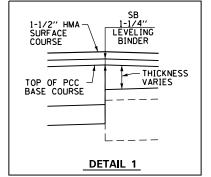
- 1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")
- 2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")
- 3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")
- (4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)
- (5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")
- (6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")
- 7) PROPOSED HMA SHOULDERS
- 8 PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")
- (9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)
- (9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

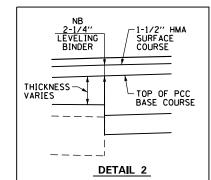
- (10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")
- (11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")
- (12) PROPOSED HMA BASE COURSE, 8"
- (13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"
- (14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"
- (15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06
- (16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12
- (17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12
- 18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

- (19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
- 20 LONGITUDINAL JOINT SEAL
- (21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- (22) PROPOSED SEEDING, CLASS 2
- PROPOSED INCIDENTAL HMA SURFACING
- PROPOSED HMA SHOULDER, 6-1/2"
- PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- (26) PROPOSED HMA BASE COURSE, 8 1/2"
- 27) PROPOSED PAVEMENT REMOVAL
- (28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

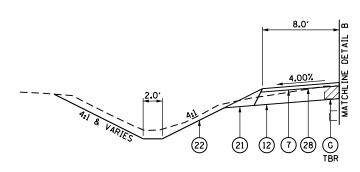
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										• (22)RS	-6, MED &	(23) RS-5		

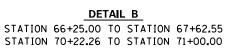


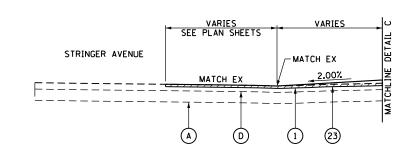




TYPICAL SECTION #39 I-55 BUSINESS LOOP/LINCOLN PARKWAY STATION 63+72.32 TO STATION 71+00.00







DETAIL C STATION 67+62.55 TO STATION 70+22.26 STRINGER AVENUE

FRONTAGE 4.00% 7 28 G TBR (12)

DETAIL A STATION 63+72.32 TO STATION 66+25.00

TYPICAL SECTION LEGEND

- A EXISTING PCC PAVEMENT 9"-7"-9"
- (B) EXISTING PCC BASE COURSE WIDENING 9"
- C EXISTING PCC PAVEMENT 10"
- D EXISTING HMA OVERLAY
- E EXISTING CC&G TYPE M-6.06
- F EXISTING CC&G TYPE B-6.24
- (G) EXISTING HMA SHOULDER 8"
- (H) EXISTING SUB-BASE GRANULAR MATERIAL 4"

- I EXISTING MEDIAN SURFACE 4"
- J EXISTING HMA BASE COURSE 9"
- (K) EXISTING GRANULAR BORROW 6"
- L EXISTING HMA PAVEMENT 12-1/2"
- (M) EXISTING HMA PAVEMENT 8"
- (N) EXISTING STABILIZED SOIL 12"

- TBR TO BE REMOVED

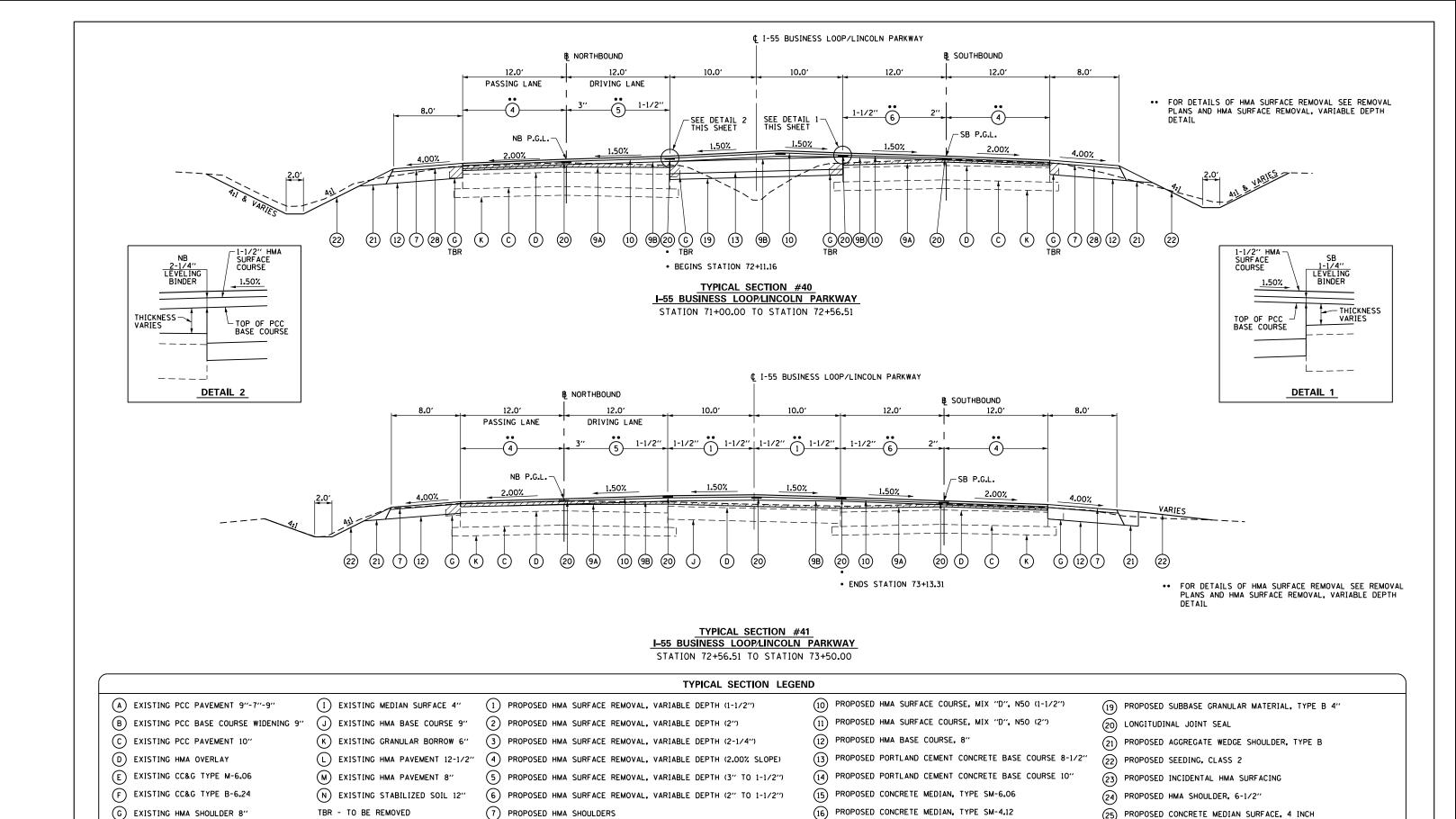
- 1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")
- 2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")
- 3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")
- 4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)
- (5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")
- (6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")
- 7) PROPOSED HMA SHOULDERS
- 8 PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")
- (9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)
- (9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

- (10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")
- (11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")
- (12) PROPOSED HMA BASE COURSE, 8"
- (13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"
- (14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"
- (15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06
- (16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12
- (17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12
- 18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

- (19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
- 20 LONGITUDINAL JOINT SEAL
- (21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- (22) PROPOSED SEEDING, CLASS 2
- PROPOSED INCIDENTAL HMA SURFACING
- (24) PROPOSED HMA SHOULDER, 6-1/2"
- PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- (26) PROPOSED HMA BASE COURSE, 8 1/2"
- 27) PROPOSED PAVEMENT REMOVAL
- (28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -				TVPIC	AL SECT	TIONS		F.A.U.	SECTION	COUNTY	TOTAL SHEETS	SHEET
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	PLOT SCALE = 10.00000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	I-55 BUSINESS LOUP/LINCULN PARKWAY				AKKVVAY			CONTRACT	T NO. 7	72E02	
Typical Sections	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FE	D. AID PROJECT		

• (22)RS-6, MED & (23) RS-5



DESIGNED REVISED USER NAME = whitlingme COUNTY TYPICAL SECTION STATE OF ILLINOIS w:\\IL084EBIDINTEG.illin ments\IDOT Offices\District 6\Projects\D672E@RANMNData\&ADsheets\D672E02-sht-04-AREA V.LEGED 7706 LOGAN 343 43 I-55 BUSINESS LOOP/LINCOLN PARKWAY PLOT SCALE = 10.0000 '/ in. CHECKED REVISED **DEPARTMENT OF TRANSPORTATION** CONTRACT NO. 72E02 SCALE: REVISED PLOT DATE = 10/21/2016

(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12

(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

8 PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")

(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)

PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"

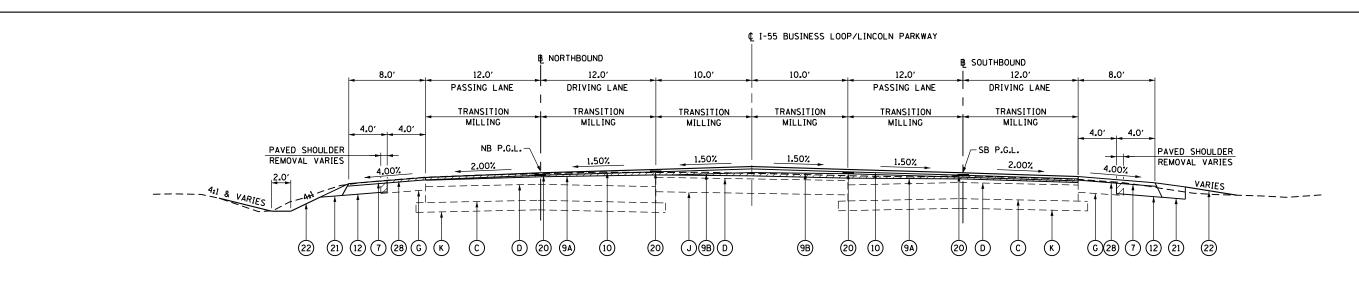
• (22)RS-6, MED & (23) RS-5

(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH

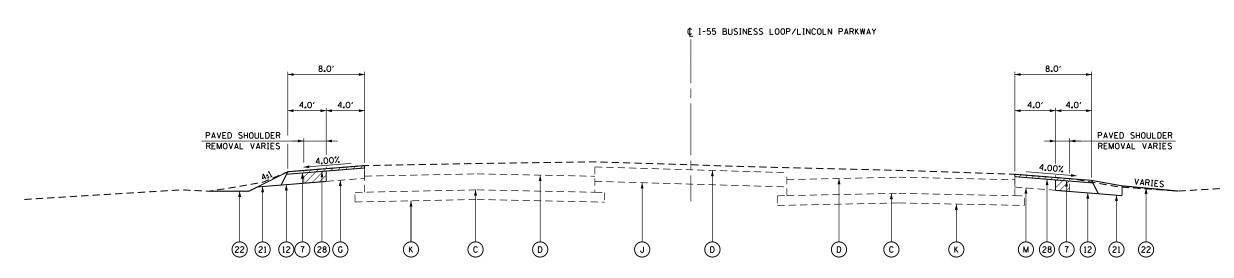
PROPOSED HMA BASE COURSE, 8 1/2"

PROPOSED PAVEMENT REMOVAL



TYPICAL SECTION #42 I-55 BUSINESS LOOP/LINCOLN PARKWAY **BUTT JOINT**

STATION 73+50.00 TO STATION 74+00.00



TYPICAL SECTION #43 I-55 BUSINESS LOOP/LINCOLN PARKWAY

RIGHT STATION 74+00.00 TO RIGHT STATION 76+12.80 LEFT STATION 74+00.00 TO LEFT STATION 76+90.00

TYPICAL SECTION LEGEND (A) EXISTING PCC PAVEMENT 9"-7"-9" I EXISTING MEDIAN SURFACE 4" 1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2") (B) EXISTING PCC BASE COURSE WIDENING 9" J EXISTING HMA BASE COURSE 9" 2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")

C EXISTING PCC PAVEMENT 10"

E EXISTING CC&G TYPE M-6.06

F EXISTING CC&G TYPE B-6.24

(G) EXISTING HMA SHOULDER 8"

(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"

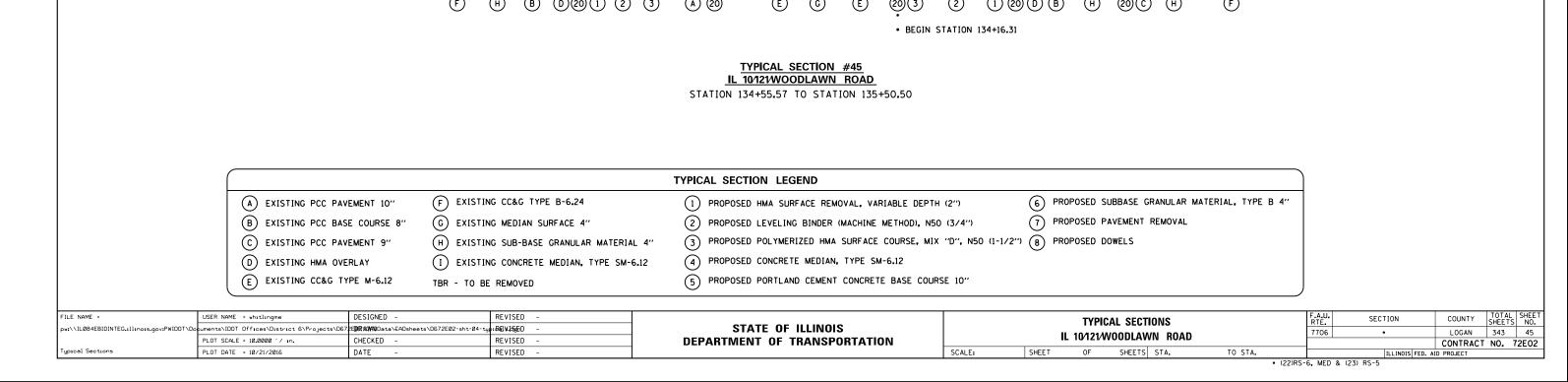
D EXISTING HMA OVERLAY

- K EXISTING GRANULAR BORROW 6" 3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4") L EXISTING HMA PAVEMENT 12-1/2" 4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)
- (M) EXISTING HMA PAVEMENT 8" PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")
- (6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2") (N) EXISTING STABILIZED SOIL 12"
- TBR TO BE REMOVED 7) PROPOSED HMA SHOULDERS
 - (8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")
 - (9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)
 - (9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")

- (10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")
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- (17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12
- (18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12

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- 20 LONGITUDINAL JOINT SEAL
- (21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- (22) PROPOSED SEEDING, CLASS 2
- PROPOSED INCIDENTAL HMA SURFACING
- (24) PROPOSED HMA SHOULDER, 6-1/2"
- PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
- (26) PROPOSED HMA BASE COURSE, 8 1/2"
- 27) PROPOSED PAVEMENT REMOVAL
- (28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -		TYPICAL SECTIONS I-55 BUSINESS LOOP/LINCOLN PARKWAY					F.A.U.	SECTION	COUNTY	TOTAL SHEET	
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	PLOT SCALE = 10.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION							CONTRACT	NO. 72E02		
Typical Sections	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FED. A	AID PROJECT	
										• (22)RS	6-6. MED &	(23) RS-5		



L IL 10/121/WOODLAWN ROAD 12.0' 12.0' 11.0' 12.0' 12.0' TURN LANE VARIES PASSING LANE DRIVING LANE PASSING LANE DRIVING LANE 7.0' TO 12.0' TURN LANE TURN LANE VARIES 12.0' TO 0.0' VARIES 7.0' TO 12.0' MATCH EX MATCH EX MATCH EX MATCH EX 00000 (3) E **©** 1 20 D B 2

L IL 10/121/WOODLAWN ROAD

(

TYPICAL SECTION #44
IL 10/121/WOODLAWN ROAD
STATION 133+62.35 TO STATION 134+55.57

12.0'

PASSING LANE

MATCH EX

3 2 1200

12.0'

DRIVING LANE

MATCH EX

12.0'

PASSING LANE

MATCH EX

12.0'

DRIVING LANE

MATCH EX

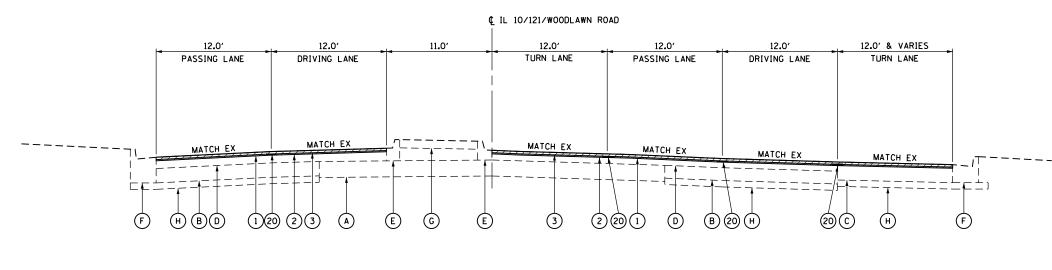
11.0'

TURN LANE

MATCH EX

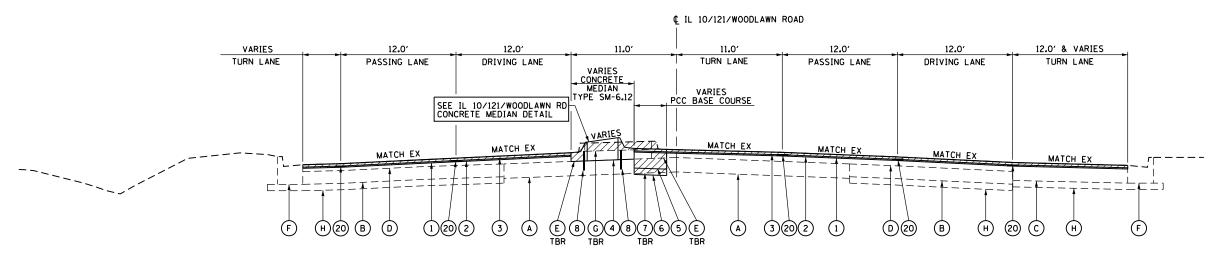
TURN LANE VARIES

0.0' TO 7.0'



TYPICAL SECTION #46 IL 10/121/WOODLAWN ROAD

STATION 135+50.50 TO STATION 135+71.11



TYPICAL SECTION #47 IL 10/121/WOODLAWN ROAD STATION 135+71.11 TO STATION 137+37.49

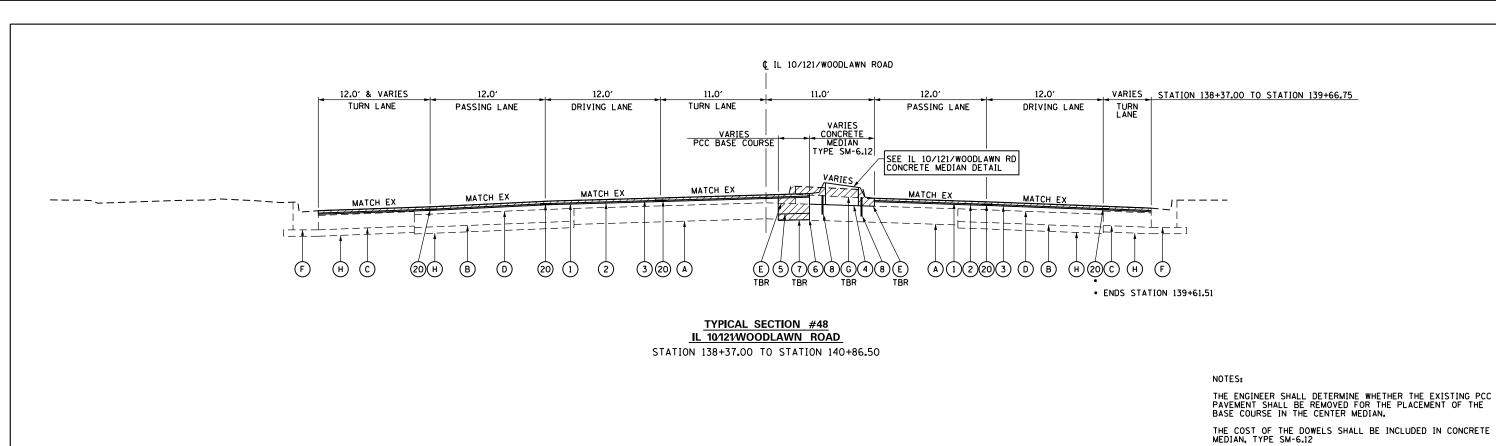
NOTES:

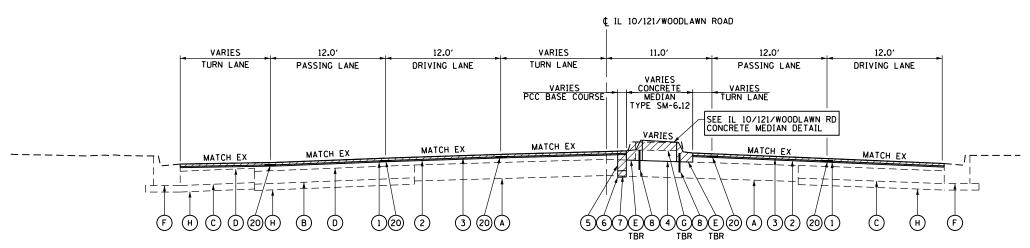
THE ENGINEER SHALL DETERMINE WHETHER THE EXISTING PCC PAVEMENT SHALL BE REMOVED FOR THE PLACEMENT OF THE BASE COURSE IN THE CENTER MEDIAN.

THE COST OF THE DOWELS SHALL BE INCLUDED IN CONCRETE MEDIAN, TYPE ${\rm SM-6.12}$

TYPICAL SECTION LEGEND (6) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4" F EXISTING CC&G TYPE B-6.24 A EXISTING PCC PAVEMENT 10" 1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2") B EXISTING PCC BASE COURSE 8" G EXISTING MEDIAN SURFACE 4" 2 PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4") 7 PROPOSED PAVEMENT REMOVAL C EXISTING PCC PAVEMENT 9" 3 PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "D", N50 (1-1/2") 8 PROPOSED DOWELS H EXISTING SUB-BASE GRANULAR MATERIAL 4" 4 PROPOSED CONCRETE MEDIAN, TYPE SM-6.12 D EXISTING HMA OVERLAY I EXISTING CONCRETE MEDIAN, TYPE SM-6.12 E EXISTING CC&G TYPE M-6.12 (5) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10" TBR - TO BE REMOVED

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -		TYPICAL SECTIONS					F.A.U. RTF.	SECTION	COUNTY	TOTAL	. JHEE!	
pw:\\ILØ84EBIDINTEG.:111:no:s.gov:PV	WIDOT\Documents\IDOT Offices\District 6\Proje		2E02-sht-04-typ1R4DV159ED -	STATE OF ILLINOIS							7706	•	LOGAN	343	46
1	PLOT SCALE = 10.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	IL 10/121/WOODLAWN ROAD							CONTRAC	T NO.	72E02	
Typical Sections	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS ST	Α.	TO STA.		ILLINOIS FED	AID PROJECT		



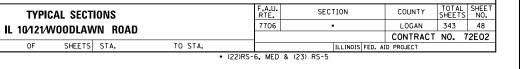


TYPICAL SECTION #49 IL 10/12/WOODLAWN ROAD STATION 140+86.50 TO STATION 141+41.71

		TYPICAL SECTION LEGEND
A EXISTING PCC PAVEMENT 10"	F EXISTING CC&G TYPE B-6.24	1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2") 6 PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
B EXISTING PCC BASE COURSE 8"	G EXISTING MEDIAN SURFACE 4"	2) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4") 7 PROPOSED PAVEMENT REMOVAL
C EXISTING PCC PAVEMENT 9"	H EXISTING SUB-BASE GRANULAR MATERIAL 4"	3 PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "D", N50 (1-1/2") 8 PROPOSED DOWELS
D EXISTING HMA OVERLAY	I EXISTING CONCRETE MEDIAN, TYPE SM-6.12	4) PROPOSED CONCRETE MEDIAN, TYPE SM-6.12
E EXISTING CC&G TYPE M-6.12	TBR - TO BE REMOVED	5) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"

COUNTY TOTAL SHEETS NO.

LOGAN 343 47 FILE NAME = DESIGNED -REVISED USER NAME = whitlingme SECTION TYPICAL SECTIONS STATE OF ILLINOIS ow:\\ILØ84EBIDINTEG.:111:no: uments\IDOT Offices\District 6\Projects\D67**2EBRANMN**Data\&ADsheets\D672E02-sht-04-t_i PERMINATED 7706 IL 10/121/W00DLAWN ROAD PLOT SCALE = 10.0000 '/ in. CHECKED REVISED **DEPARTMENT OF TRANSPORTATION** CONTRACT NO. 72E02 SCALE: SHEET SHEETS STA. REVISED PLOT DATE = 10/21/2016 DATE



SECTION

TYPICAL SECTION LEGEND (6) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4" A EXISTING PCC PAVEMENT 10" F EXISTING CC&G TYPE B-6.24 1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2") 7 PROPOSED PAVEMENT REMOVAL B EXISTING PCC BASE COURSE 8" G EXISTING MEDIAN SURFACE 4" 2 PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4") C EXISTING PCC PAVEMENT 9" (H) EXISTING SUB-BASE GRANULAR MATERIAL 4" 3 PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "D", N50 (1-1/2") 8 PROPOSED DOWELS D EXISTING HMA OVERLAY I EXISTING CONCRETE MEDIAN, TYPE SM-6.12 4 PROPOSED CONCRETE MEDIAN, TYPE SM-6.12 E EXISTING CC&G TYPE M-6.12 (5) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10" TBR - TO BE REMOVED

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

TYPICAL SECTION #51 IL 10/121/WOODLAWN ROAD STATION 142+19.73 TO STATION 143+91.32

¢ IL 10/121/WOODLAWN ROAD

12.0'

MATCH EX

SCALE:

SHEET

(3) (20) (D)

20 (1) (2)

PASSING LANE

12.0'

MATCH EX

TYPICAL SECTIONS

DRIVING LANE

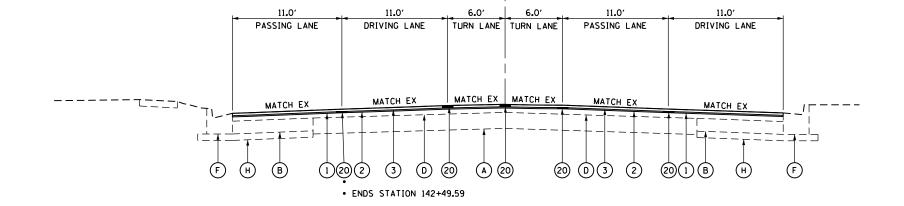
VARIES 9.5' TO 7.3'

TURN LANE

MATCH EX

L IL 10/121/WOODLAWN ROAD

TYPICAL SECTION #50 IL 10/121/WOODLAWN ROAD STATION 141+41.71 TO STATION 142+19.73



VARIES 8.0' TO 6.0'

TURN LANE

VARIES 8.0' TO 0.0'

MATCH EX

12.0'

DRIVING LANE

MATCH EX

VARIES

TURN LANE

DESIGNED -

CHECKED -

DATE

uments\IDOT Offices\District 6\Projects\D67**2E@RXWW**Data\&ADsheets\D672E02-sht-04-tį

PLOT SCALE = 10.0000 '/ in.

PLOT DATE = 10/21/2016

REVISED

REVISED

REVISED

- REDVIESED

FILE NAME =

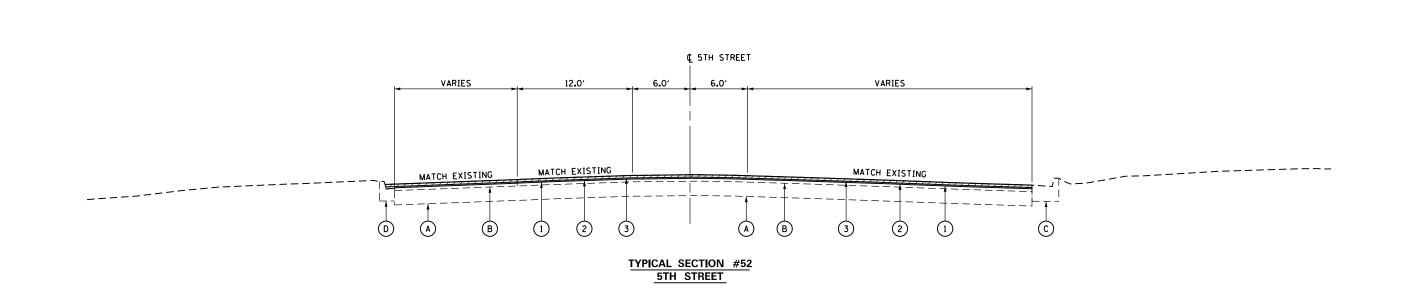
ow:\\ILØ84EBIDINTEG.:111:no

12.0'

PASSING LANE

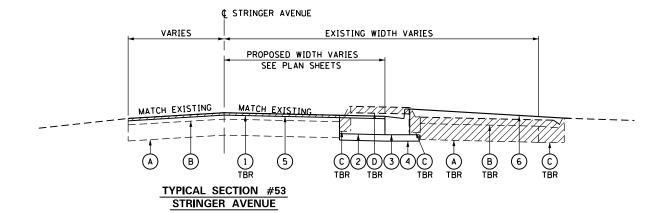
MATCH EX

(3)(20)



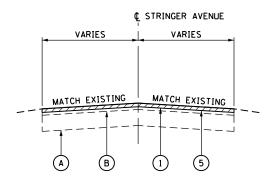
	TYPICAL SECTION LEGEND	
A EXISTING PCC PAVEMENT 10"	1 PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	
B EXISTING HMA OVERLAY	2 PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	
C EXISTING CC&G TYPE B-6.24	3 PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	
D EXISTING CC&G TYPE B-6.12	4 PROPOSED SEEDING, CLASS 2	
TBR - TO BE REMOVED		

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -				TVPIC	CAL SECTIONS		F.A.U.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\\IL084EBIDINTEG.:ll:nois.gov:PWIDOT\	Ocuments\IDOT Offices\District 6\Projects\D	67 2EDRXXXXXX Data\€ADsheets\D672E02-sht-04-t	predvieed -	STATE OF ILLINOIS	5TH STREET					7706	•	LOGAN	343	49
	PLOT SCALE = 10.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION			וכ	IH SIKEEI				CONTRACT	T NO. 7	2E02
Typical Sections	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS STA.	TO STA.		ILLINOIS FED. A			



\$\text{\$ STRINGER AVENUE}\$ \$\text{STRING VARIES}\$ \$\text{PROPOSED VARIES}\$ \$\text{SEE PLAN SHEETS}\$ MATCH EXISTING MATCH EXISTING A B 1 5 3 4 6 C TBR TBR TBR

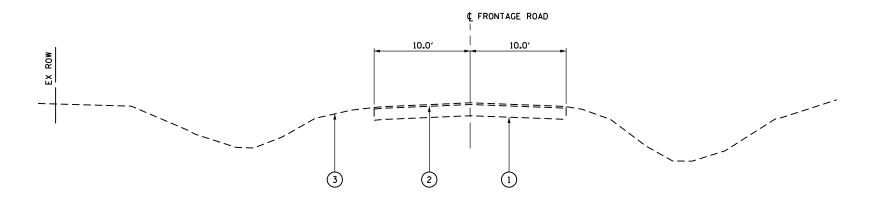
TYPICAL SECTION #54 STRINGER AVENUE



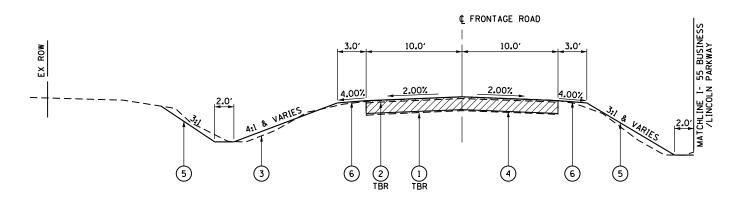
TYPICAL SECTION #55 STRINGER AVENUE

	TYPICAL SECTION LEGEND
A EXISTING PCC PAVEMENT 10"	1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")
B EXISTING HMA OVERLAY	2 PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"
© EXISTING CC&G TYPE M-6.06	3 PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.24
D EXISTING MEDIAN SURFACE 4"	4) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
TBR - TO BE REMOVED	5 PROPOSED INCIDENTAL HMA SURFACING
	6 PROPOSED SEEDING, CLASS 2

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -				TYPICAL S	FCTIONS		F.A.U.	SECTION	COUNTY	TOTAL SHE
pw:\\ILØ84EBIDINTEG.ıllınoıs.gov:PWIDOT\	Documents\IDOT Offices\District 6\Project	s\D672E DRXANMN Data\&ADsheets\D672E02-sht-04	typ1R4DVLE9ED -	STATE OF ILLINOIS						7706	•	LOGAN	343 50
	PLOT SCALE = 10.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION			STRINGER	AVENUE				CONTRACT	T NO. 72EO
Tupical Sections	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE.	SHEET	UE ZHE	FTS STA	TO STA		TILL THOSE FED.	AID DDO IECT	



TYPICAL SECTION #56 EXISTING FRONTAGE ROAD



TYPICAL SECTION #57
PATCHING FRONTAGE ROAD

TYPICAL SECTION LEGEND

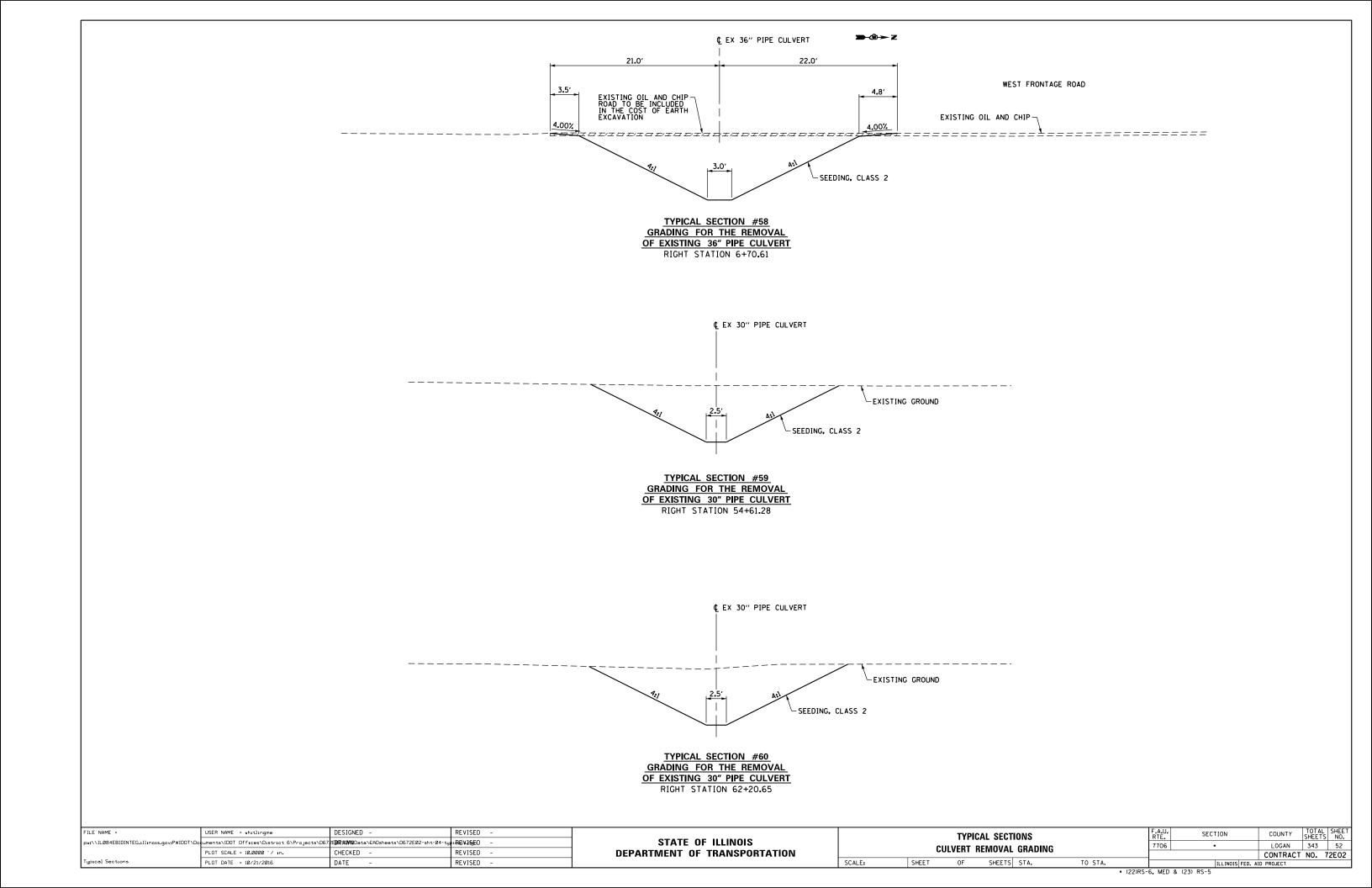
- 1 EXISTING GRAVEL BASE COURSE, 7 INCH
- 2 EXISTING BITUMINOUS SURFACE TREATMENT
- 3 EXISTING GROUND
- PAVEMENT PATCHING, TYPE III, 8 INCH OR PAVEMENT PATCHING, TYPE IV, 8 INCH
- 5 SEEDING, CLASS 2
- 6 EARTH SHOULDER

TBR - TO BE REMOVED

NOTES:

1. REMOVAL OF EXISTING FRONTAGE ROAD PAVEMENT SHALL BE INCLUDED IN THE COST OF PAVEMENT PATCHING.

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -				TYPICAL SE	CTIONS		F.A.U.	SECTION	COUNTY	TOTAL SHEET SHEETS NO.
pw:\\ILØ84EBIDINTEG.:111:no1s.gov:PWIDOT\Do	puments\IDOT Offices\District 6\Projects\D67	7 2EDRXAMAN Data\€ADsheets\D672E02-sht-04-tı	pıRÆDVLESÆD –	STATE OF ILLINOIS						7706	•	LOGAN	343 51
	PLOT SCALE = 10.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION			FRONTAGE	KUAD				CONTRACT	NO. 72E02
Typical Sections	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF SHEE	TS STA.	TO STA.		TILL INDIS FEE	AID PROJECT	



20100210 - TRI	EE REMOVAL (OVE	R 15 UNITS DIAMI	ETER)
	PPS# 6-00625-0000	(STP)	
STATION	OFFSET	SIDE	UNIT
16+92.66	58.9	LT	17
29+81.80	52.5	RT	34
31+08.26	106.9	LT	28
31+28.53	106.7	LT	23
31+61.43	107.3	LT	30
54+76.36	55.0	RT	35
62+27.93	97.0	RT	36
	PPS# 6-00625-0	000 (STP) TOTAL =	203

PPS# 6-00	625-0000 (STP)			
DESCRIPTION	STATION	OFFSET	SIDE	EAC
PROPOSED DUAL 18" PIPE	5+22.26	57.9	LT	1.0
EXISTING 18" PIPE	5+59.34	59.2	RT	1.0
EXISTING 36" PIPE	6+70.52	65.8	RT	1.0
PROPOSED TYPE 8 GRATE	6+71.18	53.2	LT	1.0
PROPOSED 3' X 2' BOX CULVERT	6+71.52	111.9	LT	1.0
PROPOSED 18" PIPE	7+26.91	54.1	LT	1.0
PROPOSED 2' X 2' BOX CULVERT	16+80.42	112.0	LT	1.0
PROPOSED TYPE 8 GRATE	16+80.64	44.1	LT	1.0
PROPOSED 18" PIPE	17+26.50	54.2	LT	1.0
PROPOSED TYPE 8 GRATE	31+71.16	54.2	LT	1.0
PROPOSED 2' X 2' BOX CULVERT	31+71.26	108.5	LT	1.0
PROPOSED INLET WITH TYPE 8 GRATE	33+00.01	55.7	LT	1.0
PROPOSED 18" PIPE	34+00.07	56.8	LT	1.0
PROPOSED 24" PIPE	47+74.85	57.5	RT	1.0
EXISTING 24" PIPE	47+76.44	54.4	RT	1.0
EXISTING 24" PIPE	49+11.19	154.0	LT	1.0
PROPOSED 2' X 2' BOX CULVERT	49+77.20	62.3	RT	1.0
EXISTING 30" PIPE	51+55.88	115.5	LT	1.0
PROPOSED TYPE 8 GRATE	54+60.74	57.3	RT	1.0
EXISTING 30" PIPE	54+61.30	59.8	RT	1.0
PROPOSED 18" PIPE	55+51.75	55.9	LT	1.0
PROPOSED 18" PIPE	61+24.45	54.1	LT	1.0
PROPOSED TYPE 8 GRATE	62+20.65	57.3	LT	1.0
EXISTING 2' X 2' BOX CULVERT	62+20.65	57.3	RT	1.0
PROPOSED 18" PIPE	63+01.35	54.5	LT	1.0
EXISTING 30" PIPE	66+11.42	57.9	LT	1.0
EXISTING 24" PIPE	66+97.93	47.3	RT	1.0
PROPOSED 15" PIPE	69+14.32	56.7	RT	1.0
EXISTING INLET	70+12.49	49.5	LT	1.0
EXISTING 12" PIPE	73+35.12	48.1	LT	1.0
PROPOSED TYPE 8 GRATE (IL 10)	135+93.83	58.5	LT	1.0
PROPOSED 15" PIPE CULVERT (IL 10)	136+01.836	58.3	LT	1.0
EXISTING 24" PIPE (IL 10)	139+01.82	68.0	LT	1.0
PROPOSED 18" PIPE (STRINGER AVE)	20+80.40	25.4	LT	1.0
PPS# 6-00625-0000 (STP) TOTAL	=			34

				PPS# 6-00	625-0000 (STP)			
						FC	OR INFORMATION ON	ILY
ROADWAY	SIDE	STATION	то	STATION	20200100 EARTH EXCAVATION CU YD	25% SHRINKAGE FACTOR APPLIED	EMBANKMENT CU YD	BALANCE WASTE (+) SHORTAGE (-
NB 155 BUSINESS/LINCOLN PARKWAY	LT	1+28.24	-	5+67.46	87	65	53	12
	LT	6+32.62	-	38+83.16	527	395	549	-154
	LT	40+39.97	-	48+52.74	289	217	19	198
	LT	48+64.29	-	67+83.47	594	446	299	147
	LT	69+24.94	-	72+78.86	126	95	10	85
	LT	73+26.33	-	76+90.11	74	56	0	56
SB I55 BUSINESS/LINCOLN PARKWAY	RT	1+32.21	-	5+72.07	121	91	3	88
	RT	6+29.56	-	18+65.16	417	313	88	225
	RT	20+83.61	-	38+47.84	383	287	86	201
	RT	41+07.75	-	47+96.61	187	140	44	96
	RT	48+07.86	-	72+03.54	747	560	281	279
	RT	72+86.01	-	76+12.85	46	35	7	28
FRONTAGE ROAD	LT	6+00.00	-	7+25.00	46	35	11	24
	LT	16+25.00	-	17+50.00	70	53	12	41
	LT	31+00.00	-	32+25.00	48	36	25	11
	LT	53+00.00	-	55+25.00	36	27	33	-6
	LT	62+00.00	-	62+50.00	3	2	15	-13
IL 10/121/WOODLAWN ROAD	LT	135+63.60	-	137+16.87	32	24	36	-12
	LT	138+53.03	-	141+65.89	12	9	30	-21
STRINGER AVENUE	LT	19+69.70	-	21+00.00	97	73	16	57
EXSITING 36" PIPE CULVERT REMOVAL	RT	6+70.61	-	N/A	89	66	0	66
EXISTING 30" PIPE CULVERT REMOVAL	RT	54+61.28	-	N/A	58	43	0	43
EXISTING 30" PIPE CULVERT REMOVAL	RT	62+20.65	-	N/A	58	44	0	44
		PPS #6-00625-000	0 (STP)	STAGE 1 TOTAL =	4147	3110	1617	1493

				EARTHWORK S	CHEDULE - STAGE 2			
				PPS# 6-006	525-0000 (STP)			
						FC	OR INFORMATION ON	ILY
					20200100			BALANCE
ROADWAY	SIDE	STATION	то	STATION	EARTH EXCAVATION	25% SHRINKAGE	EMBANKMENT	WASTE (+)
					CU YD	FACTOR APPLIED	CU YD	SHORTAGE (-
155 BUSINESS/LINCOLN PARKWAY	CENTER	9+63.40	-	18+56.74	176	132	418	-286
	CENTER	24+04.46	-	34+18.04	120	90	494	-404
	CENTER	44+87.15	-	48+23.50	73	55	90	-35
	CENTER	48+35.10	-	51+80.04	108	81	147	-66
	CENTER	52+39.71	-	72+56.51	240	180	1144	-964
	PPS #	6-00625-0000 (S	TP) STA	GE 2 SUB-TOTAL =	717	538	2293	-1755
				PPS# 6-004	77-0000 (HSIP)			
						FC	OR INFORMATION ON	ILY
					20200100			BALANCE
ROADWAY	SIDE	STATION	то	STATION	EARTH EXCAVATION	25% SHRINKAGE	EMBANKMENT	WASTE (+)
					CU YD	FACTOR APPLIED	CU YD	SHORTAGE (
I55 BUSINESS/LINCOLN PARKWAY	CENTER	0+49.48	-	5+75.62	111	83	5	78
	CENTER	6+25.03	-	9+63.40	62	47	21	26
I55 BUSINESS/LINCOLN PARKWAY &	CENTER	34+18.04	-	39+09.47	203	152	48	104
& 5TH STREET INTERSECTION	CENTER	40+23.12	-	44+87.15	228	171	0	171
	PPS #6	-00477-0000 (HS	IP) STAC	GE 2 SUB-TOTAL =	604	453	74	379
		•	-	STAGE 2 TOTAL =	1321	991	2367	-1376
CON	ADINIED STACE 1	(STP) AND STAG	E 2 /CTD	O LICID\ TOTAL -	5468	4101	3984	117

FILE N	NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -							F.A.U.	SECTION	COUNTY	TOTAL SHEET
pw:\\I	LØ84EBIDINTEG.:111:no:s.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67	PEDRXXXXX Data\€ADsheets\D672E02-sht-05-Sc	reRENASEON -	STATE OF ILLINOIS		:	SCHEDUL	E OF QUANTITIES		7706	•	LOGAN	343 53
		PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION								CONTRACT	
Defaul	t	PLOT DATE = 11/18/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS STA.	TO STA.		ILLINOIS FED. A		
										• (22)RS	5-6, MED	& (23) RS-5		

					50 - TRENCH B				
				PPS# (6+00477-0000	(HSIP)			
STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	TRENCH LENGTH	AVERAGE DEPTH	CU YE
2+70.99	2.2	RT		2+70.76	2.7	LT	5.0	2.6	1.0
2+70.81	2.4	LT		5+44.29	1.7	LT	279.0	2.6	54.1
5+49.29	1.7	LT		5+49.67	9.1	LT	7.4	2.6	1.4
5+49.29	1.7	LT		5+64.83	1.3	LT	15.6	2.6	3.0
6+39.11	0.8	RT		6+64.04	1.6	RT	25.0	2.6	4.9
6+51.64	9.5	RT		6+51.51	1.1	RT	8.4	2.6	1.6
6+77.70	1.8	RT		8+28.89	0.7	RT	151.2	2.6	29.3
							PPS #6-00	477-0000 (HSIP) TOTAL =	95.4

						SEEDIN	IG SCHEDULE					
							0625-0000 (STP)					
ROADWAY	STATION	SIDE	то	STATION	SIDE	25000200 SEEDING CLASS 2 (ACRE)	25000400 NITROGEN FERTILIZER NUTRIENTS (POUND)	25000500 PHOSPHORUS FERTILIZER NUTRIENTS (POUND)	25000600 POTASSIUM FERTILIZER NUTRIENTS (POUND)	25000700 AGRICULTURAL GROUND LIMESTONE (TON)	25100115 MULCH METHOD II (ACRE)	28000250 TEMPORARY EROSION CONTRO SEEDING (POUND)
NB I-55 BUSINESS/LINCOLN PARKWAY	1+28.24	LT	-	5+70.00	LT	0.19	17.5	17.5	17.5	0.39	0.19	19.4
	6+33.00	LT	-	38+84.00	LT	1.65	148.8	148.8	148.8	3.31	1.65	165.4
	40+11.40	LT	- 1	48+52.74	LT	0.52	46.9	46.9	46.9	1.04	0.52	52.2
	48+64.29	LT	- 1	67+97.95	LT	1.11	99.5	99.5	99.5	2.21	1.11	110.6
	68+95.00	LT	- 1	72+84.00	LT	0.41	36.7	36.7	36.7	0.82	0.41	40.8
	73+21.32	LT	- 1	76+92.00	LT	0.08	7.6	7.6	7.6	0.17	0.08	8.5
SB I-55 BUSINESS/LINCOLN PARKWAY	1+32.00	RT	- 1	5+74.14	RT	0.13	11.3	11.3	11.3	0.25	0.13	12.6
	6+27.11	RT	- 1	19+13.02	RT	0.59	53.3	53.3	53.3	1.18	0.59	59.2
	20+83.60	RT	- 1	38+52.91	RT	0.78	70.0	70.0	70.0	1.56	0.78	77.8
	41+00.00	RT	- 1	47+94.15	RT	0.27	24.7	24.7	24.7	0.55	0.27	27.4
	48+06.00	RT	- 1	51+99.96	RT	0.14	12.7	12.7	12.7	0.28	0.14	14.1
	52+20.96	RT	-	68+75.32	RT	0.87	78.1	78.1	78.1	1.73	0.87	86.7
	68+93.32	RT	- 1	72+30.69	RT	0.13	11.4	11.4	11.4	0.25	0.13	12.7
	72+76.96	RT	- 1	76+12.85	RT	0.04	3.8	3.8	3.8	0.09	0.04	4.3
FRONTAGE ROAD	6+00.00	LT		7+25.00	LT	0.07	6.4	6.4	6.4	0.14	0.07	7.1
	16+25.00	LT		17+50.00	LT	0.08	7.4	7.4	7.4	0.16	0.08	8.2
	31+00.00	LT	-	32+50.00	LT	0.07	6.7	6.7	6.7	0.15	0.07	7.5
	53+00.00	LT	-	55+25.00	LT	0.13	12.1	12.1	12.1	0.27	0.13	13.5
	62+00.00	LT	-	62+42.82	LT	0.03	2.9	2.9	2.9	0.06	0.03	3.2
	65+52.72	LT	-	66+08.42	LT	0.02	2.1	2.1	2.1	0.05	0.02	2.3
IL 10/121/WOODLAWN ROAD	135+63.60	LT	-	137+16.87	LT	0.06	5.6	5.6	5.6	0.12	0.06	6.2
	138+53.54	LT	-	141+65.89	LT	0.04	3.7	3.7	3.7	0.08	0.04	4.2
	PPS #	6-0062	25-00	00 (STP) TO	TALS =	7.4	669	669	669	15	7.4	744

280004	100 - PERIMET	TER EROSIO	N BARRI	ER SCH	IEDULE			
	PPS#	6-00625-000	0 (STP)					
ROADWAY	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOOT
SB I-55 BUSINESS/LINCOLN PARKWAY	15+49.99	80.6	RT	-	17+00.00	81.7	RT	152
SB I-55 BUSINESS/LINCOLN PARKWAY	29+50.00	60.0	RT	-	31+25.00	65.0	RT	177
NB I-55 BUSINESS/LINCOLN PARKWAY	53+00.00	141.7	LT	-	55+50.00	115.6	LT	252
					PPS# 6-0062	5-0000 (STP)	TOTAL =	581

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -								F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEET SHEETS NO.
pw:\\IL084EBIDINTEG.:1ll:nois.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67	EDRAMADota\€ADsheets\D672E02-sht-05-So		STATE OF ILLINOIS		;	SCHEDUL	E OF QU	JANTITIES		7706	•	LOGAN	343 54
	PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION									CONTRAC	T NO. 72E02
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		ILL INOIS F	ED. AID PROJECT	
										• (22)R	S-6, MED	& (23) RS-5		

28000305 - TEMPORARY DITCH (CHECKS SCHI	EDULE	
PPS# 6-00625-0000 (STP)		
DESCRIPTION	STATION	SIDE	FOOT
SB I-55 BUSINESS/LINCOLN PARKWAY	2+30.00	RT	10.0
	3+60.00	RT	10.0
	7+10.00	RT	10.0
	8+60.00	RT	10.0
	11+60.00	RT	10.0
	15+60.00	RT	10.0
	17+10.00	RT	10.0
	23+00.00	RT	10.0
	25+50.00	RT	10.0
	26+60.00	RT	10.0
	29+10.00	RT	10.0
	32+60.00	RT	10.0
	35+00.00	RT	10.0
	37+50.00	RT	10.0
	43+50.00	RT	10.0
	46+50.00	RT	10.0
	49+05.00	RT	10.0
	50+80.00	RT	10.0
	54+40.00	RT	10.0
	54+85.00	RT	10.0
	59+50.00	RT	10.0
	61+50.00	RT	10.0
	62+50.00	RT	10.0
	64+70.00	RT	10.0
	67+20.00	RT	10.0
	69+60.00	RT	10.0
	71+00.00	RT	10.0
NB I-55 BUSINESS/LINCOLN PARKWAY	3+90.00	LT	10.0
	8+30.00	LT	10.0
	10+80.00	LT	10.0
	13+30.00	LT	10.0
	15+80.00	LT	10.0
	17+60.00	LT	10.0
	20+10.00	LT	10.0
	22+60.00	LT	10.0
	25+10.00	LT	10.0
	27+60.00	LT	10.0
	29+60.00	LT	10.0
	31+15.00	LT	10.0
	34+40.00	LT	10.0
	36+90.00	LT	10.0
	42+90.00	LT	10.0
	45+40.00	LT	10.0
	47+40.00	LT	10.0
	50+00.00	LT	10.0
	50+60.00	LT	10.0
	51+50.00	LT	10.0
	52+30.00	LT ·-	10.0
	53+30.00	LT	10.0
	54+30.00	LT	10.0
	56+00.00	LT	10.0
	60+00.00	LT	10.0
	64+00.00	LT	10.0
	66+00.00	LT	10.0
	71+10.00	LT	10.0
PPS# 6-00625	73+65.00	LT	10.0

3110020	0 - SUBBASE GRA	ANULAR MA	TERIA	L, TYPE B 4"					
	PPS# 6-00	625-0000 (ST	P)						
ROADWAY	STATION	SIDE	то	STATION	SIDE	SQ YD			
I55 BUSINESS/LINCOLN PARKWAY	9+63.40	CENTER	-	18+57.59	CENTER	2077			
	14+58.26	RT	-	17+43.26	RT	131			
	24+04.46	CENTER	-	34+18.04	CENTER	2351			
	44+87.15	CENTER	-	48+18.87	CENTER	741			
	48+39.74	CENTER	-	72+56.51	CENTER	5375			
PPS# 6-00625-0000 (STP) TOTAL =									
	PPS# 6-004	177-0000 (HS	IP)						
ROADWAY	STATION	SIDE	TO	STATION	SIDE	SQ YD			
I55 BUSINESS/LINCOLN PARKWAY	1760+35.87	CENTER	-	1761+70.82	CENTER	105			
	0+49.58	CENTER	-	5+75.68	CENTER	559			
	6+25.02	CENTER	-	9+63.45	CENTER	500			
	34+18.04	CENTER	-	39+09.46	CENTER	748			
	40+23.14	CENTER	-	44+87.15	CENTER	659			
IL 10/121/WOODLAWN ROAD	135+72.94	CENTER	-	137+35.50	CENTER	69			
	138+39.10	CENTER	-	141+37.52	CENTER	131			
			PPS	#6-00477-0000 (H	SIP) TOTAL =	2770			
			S	TP & HSIP COMBI	NED TOTAL =	13445			

	35101800 - AGGREGATE BASE COURSE, TYPE B 6"												
PPS# 6-00625-0000 (STP)													
LOCATION	TYPE OF ENTRANCE	EX MATERIAL TYPE			AGGREGATE BASE COURSE, TYPE B 6"								
			(FT)	(FT)	(FT)	(FT)	(SQ. YD.)						
52+10.46 RT	FE - RURAL	AGG	21.00	59.00	59.00	17.00	45						
68+84.32 RT	FE - RURAL	EARTH	18.00	64.00	64.00	22.00	49						
			•	PPS# 6-0	0625-0000 (ST	ΓP) TOTAL =	94						

	ND CEMENT CON PPS# 6-00625-00			, _	
ROADWAY	STATION	то	STATION	LOCATION	SQ YI
155 BUSINESS/LINCOLN PARKWAY	24+04.49	-	24+68.18	CENTER	149
	51+80.04	-	52+39.71	CENTER	144
	63+72.32	-	72+56.51	CENTER	1965
		PPS#	6-00625-0000	(STP) TOTAL =	2257
	PPS# 6-00477-	0000			
	STATION	то	STATION	LOCATION	SQ Y
ROADWAY	SIATION				400
ROADWAY 155 BUSINESS/LINCOLN PARKWAY	6+25.03	-	9+63.40	CENTER	499.
		PPS #6		CENTER (HSIP) TOTAL =	499. 500

35300500 - PO	RTLAND CEMENT	CONCRET	E BASE COURSE 1	.0"						
	PPS# 6-00625	-0000 (STP)	1							
ROADWAY	STATION	то	STATION	LOCATION	SQ YI					
I55 BUSINESS/LINCOLN PARKWAY	15+20.75	-	18+57.59	CENTER	786					
STRINGER AVENUE	21+74.53	-	22+21.54	RT	43					
PPS# 6-00625-0000 (STP) TOTAL =										
PPS# 6-00477-0000 (HSIP)										
ROADWAY	STATION	то	STATION	LOCATION	SQ Y					
155 BUSINESS/LINCOLN PARKWAY	1760+54.93	-	1761+70.61	CENTER	39					
	0+49.48	-	5+75.62	CENTER	559					
	34+18.04	-	39+09.52	CENTER	748					
	40+23.13	-	44+87.15	CENTER	659					
IL 10/121/WOODLAWN ROAD	135+72.94	-	137+35.50	CENTER	69					
	138+39.10	-	141+37.52	CENTER	131					
	•	Р	PS #6-00477-0000	(HSIP) TOTAL =	220					
		ST	P AND HSIP COMB	INED TOTALS =	2991					

		4	4004250 -	PAVED	SHOU	JLDER REM	OVAL				
			PP	S# 6-006	525-00	00 (STP)					
ROADWAY	LOCATION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	LENGTH	WIDTH	SQ YD
NB I-55 BUSINESS/	OUTSIDE	4+77.42	42.0	LT	-	5+57.72	39.5	LT	80	VARIES	38
LINCOLN PARKWAY	OUTSIDE	6+42.63	34.0	LT	-	33+72.41	34.0	LT	2730	1.5	455
	OUTSIDE	33+72.38	34.0	LT	-	35+73.21	34.0	LT	201	VARIES	120
	OUTSIDE	45+15.17	34.0	LT	-	48+46.89	34.0	LT	332	1.5	55
	OUTSIDE	48+58.17	34.0	LT	-	67+62.55	34.0	LT	1904	1.5	317
	OUTSIDE	69+24.97	34.0	LT	-	72+76.03	34.0	LT	351	1.5	59
	OUTSIDE	73+26.33	34.0	LT	-	73+46.61	34.0	LT	20	VARIES	3
	OUTSIDE	73+46.61	34.0	LT	-	76+90.11	34.0	LT	344	VARIES	65
	INSIDE	9+63.40	4.0	LT	-	17+12.15	11.0	LT	749	1.5	125
	INSIDE	17+12.15	11.0	LT	-	18+56.74	11.0	LT	145	VARIES	87
	INSIDE	24+04.60	11.0	LT	-	25+13.89	11.0	LT	109	VARIES	59
	INSIDE	25+13.89	11.0	LT	-	34+18.04	11.0	LT	904	1.5	151
	INSIDE	44+87.15	10.0	LT	-	48+29.19	10.0	LT	342	1.5	57
	INSIDE	48+40.47	10.0	LT	-	72+56.51	10.0	LT	2416	1.5	403
SB I-55 BUSINESS/	INSIDE	9+63.40	10.0	RT	-	14+35.64	10.0	RT	472	1.5	79
LINCOLN PARKWAY	INSIDE	14+35.64	10.0	RT	-	18+56.68	10.0	RT	421	VARIES	244
	INSIDE	24+04.49	10.0	RT	-	25+13.87	10.0	RT	109	VARIES	60
	INSIDE	25+13.87	10.0	RT	-	34+18.04	10.0	RT	904	1.5	151
	INSIDE	44+87.15	10.0	RT	-	48+18.12	10.0	RT	331	1.5	55
	INSIDE	48+29.93	10.0	RT	-	72+56.51	10.0	RT	2427	1.5	404
	OUTSIDE	2+00.46	34.0	RT	-	5+67.86	34.0	RT	367	1.5	61
	OUTSIDE	6+42.57	34.0	RT	-	14+35.36	34.0	RT	793	1.5	132
	OUTSIDE	14+35.36	34.0	RT	-	17+64.57	34.0	RT	329	VARIES	162
	OUTSIDE	24+95.37	34.0	RT	-	25+13.70	34.0	RT	18	VARIES	11
	OUTSIDE	25+13.70	34.0	RT	-	33+70.56	34.0	RT	857	1.5	143
	OUTSIDE	44+60.29	34.0	RT	-	45+13.68	34.0	RT	53	VARIES	38
	OUTSIDE	45+13.68	34.0	RT	-	48+00.32	34.0	RT	287	1.5	48
	OUTSIDE	48+12.23	34.0	RT	-	71+94.74	34.0	RT	2383	1.5	397
	OUTSIDE	72+96.45	34.0	RT	-	73+39.64	34.0	RT	43	1.5	7
	OUTSIDE	73+39.64	34.0	RT	-	76+12.85	34.0	RT	273	VARIES	56
				•	•			PPS# 6-00	625-0000 (ST	P) TOTAL =	4003
									•		•
					_	00 (HSIP)			I	T	
ROADWAY	LOCATION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	LENGTH	WIDTH	SQ YD
NB I55 BUSINESS/	OUTSIDE	8+25.94	4.0	LT		9+63.45	11.0	LT	1.5	138	23
LINCOLN PARKWAY	OUTSIDE	34+18.04	11.0	LT		34+86.86	11.0	LT	1.5	69	11
	OUTSIDE	44+42.96	10.0	LT		44+87.15	10.0	LT	1.5	44	7
SB I55 BUSINESS/	OUTSIDE	8+24.62	10.0	RT		9+63.45	10.0	RT	1.5	139	23
LINCOLN PARKWAY	OUTSIDE	34+18.04	10.0	RT		34+88.15	10.0	RT	1.5	70	12
	OUTSIDE	44+43.53	10.0	RT		44+87.15	10.0	RT	1.5	44	7
							F	PPS #6-004	177-0000 (HS	IP) TOTAL =	84
								STP AND H	SIP COMBIN	ED TOTAL =	4087

35501318 - HOT-MIX ASPHALT BASE COURSE, 8 1/2"										
PPS# 6-00625-0000 (STP)										
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	SQ YD		
SB I55 BUSINESS/LINCOLN PARKWAY	14+58.26	31.6	RT	-	17+43.26	44.5	RT	99		
PPS# 6-00625-0000 (STP) TOTAL =										

Default	PLOT DATE = 10/21/2016	DATE -	REVISED -
	PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -
pw:\\ILØ84EBIDINTEG.:ll:nois.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67	PE DR'AUM Data\€ADsheets\D672E02-sht-05-Sc	- REVISED -
FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -

STATE	E 01	ILLINOIS
DEPARTMENT	OF	TRANSPORTATION

	SCHEDIII	E 0E 011	ANITITIES		F.A.U. RTE. 7706	SECTION		TOTAL SHEETS	
SCHEDULE OF QUANTITIES						•	LOGAN	343	55
							CONTRACT	NO.	2E02
SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FED. AI	D PROJECT		
				• (22)RS-	6, MED	& (23) RS-5			

	PPS# 6-00625	5-0000 (STP)				
ROADWAY	STATION	LANE	то	STATION	LANE	SQ Y
STALHUT DRIVE	38+70.18	EB & WB	-	1758+15.93	EB & WB	49
5TH STREET (WEST)	12+41.03	EB & WB	-	12+51.03	EB & WB	62
	16+05.08	EB & WB	-	16+15.08	EB & WB	53
FRONTAGE ROAD (NORTH)	15+20.53	NB & SB	-	15+50.99	NB & SB	34
FRONTAGE ROAD (SOUTH)	15+31.25	NB & SB	-	15+51.25	NB & SB	22
NB I-55 BUSINESS/LINCOLN PARKWAY	48+25.87	NB	-	48+37.81	NB	32
	48+49.29	NB	-	48+61.20	NB	32
SB I-55 BUSINESS/LINCOLN PARKWAY	47+97.30	SB	-	48+09.23	SB	32
	48+20.76	SB	-	48+32.68	SB	32
STRINGER AVENUE	19+89.75	EB & WB	-	19+99.72	EB & WB	26
KENWOOD DRIVE		EB & WB	-		EB & WB	33
SB & NB I-55 BUSINESS/LINCOLN PARKWAY	73+50.00	NB & SB	-	74+00.00	NB & SB	37
				PPS# 6-00625-0000	(STP) TOTAL =	78
	PPS # 6-00477	'-0000 (HSIP)				
ROADWAY	STATION	LANE	TO	STATION	LANE	SQ۱
155 BUSINESS/LINCOLN PARKWAY	1758+05.93	NB	-	1758+15.93	NB	39
	1758+05.93	SB	-	1758+15.93	SB	27
IL 10/121/WOODLAWN ROAD	133+70.43	EB	-	134+20.00	EB	14
	133+70.43	WB	-	134+20.00	WB	20
	143+41.32	EB & WB	-	143+91.32	EB & WB	31
5TH STREET	12+41.03	EB & WB	-	12+51.03	EB & WB	62
	·		PI	PS # 6-00477-0000	(HSIP) TOTAL =	78
			•	TP AND HSIP COM	RINED TOTAL -	157

	40600990 - TEMI	PORARY RA	AMP					
	PPS# 6-00625	-0000 (STP))					
ROADWAY	STATION	SIDE	то	STATION	SIDE	SQ YE		
NB I55 BUSINESS/LINCOLN PARKWAY	48+20.20	LT	-	48+50.72	LT	28		
	48+38.97	LT	-	48+69.38	LT	29		
	73+94.00	LT	-	74+00.00	LT	23		
SB I55 BUSINESS/LINCOLN PARKWAY	47+89.06	RT	-	48+17.04	RT	28		
	48+07.86	RT	-	48+38.30	RT	28		
	73+94.00	RT	-	74+00.00	RT	23		
STALHUT DRIVE	38+60.60	RT	-	38+66.61	RT	12		
	44+87.15	LT	-	74+00.00	LT	17		
PPS# 6-00625-0000 (STP) TOTAL =								
ROADWAY	PPS# 6-00477-	SIDE	TO	STATION	SIDE	SQ Y		
NB I-55 BUSINESS/LINCOLN PARKWAY	1758+05.93	LT	10	1758+11.91	LT	29		
SB I-55 BUSINESS/LINCOLN PARKWAY	1758+05.93	RT	_	1758+11.91	RT	18		
EB IL 10/121/WOODLAWN ROAD	133+70.43	RT	_	133+76.43	RT	18		
25 12 10, 121, 110052, 1111 110, 15	143+84.83	RT	-	143+91.32	RT	19		
WB IL 10/121/WOODLAWN ROAD	133+70.43	LT	-	133+76.43	LT	26		
··- ·,, ·· ··· ·- ·-	143+84.83	LT	-	143+91.32	LT	19		
5TH STREET	12+41.03	RT	-	12+47.03	RT	15		
	12+41.03	LT	-	12+47.03	LT	21		
	16+09.08	RT	-	16+15.08	RT	15		
	16+09.08	LT	-	16+15.08	LT	16		
	10.05.00							
	10103.00	· L	PPS #	6-00477-0000 (HSI	P) TOTAL =	196		

	42400800 - DETECTABLE WARNINGS										
PPS# 6-00625-0000 (STP)											
ROADWAY	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	SQ FT			
IL 10/121/WOODLAWN ROAD	138+53.32	72.5	LT	-	138+56.98	69.1	LT	10			
	PPS# 6-00625-0000 (STP) TOTAL =										

- 1	FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
١	pw:\\ILØ84EBIDINTEG.:ll:nois.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67	PE DR'AUM Data\€ADsheets\D672E02-sht-05-Sc	- REVESED -
١		PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -
ı	Default	PLOT DATE = 10/21/2016	DATE -	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

						F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEE
		SCHEDUL	E OF QUA	ANTITIES		7706	•	LOGAN	343	56
								CONTRACT	NO.	72E0
SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FED. A	ID PROJECT		
					• (22)RS-	6, MED	& (23) RS-5			

424	00100 - PORTLAN	D CEMENT CO	NCRETE S	IDEWA	ALK 4 INCH			
	PP	S# 6-00625-00	00 (STP)					
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	SQ FT
IL 10/121/WOODLAWN ROAD	135+63.60	50.0	LT	-	137+07.82	80.4	LT	769
NB 155 BUSINESS/LINCOLN PARKWAY	138+58.57	78.6	LT	-	141+65.89	49.9	LT	1589
					PPS# 6-0	0625-0000 (STP) TOTAL =	2358

		PPS# 6-00625	-0000 (STF	P)				
ROADWAY	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOO'
STRINGER AVENUE	20+43.44	14.1	LT		22+89.61	34.4	LT	446
	•				PPS #6-00	625-0000 (STI	P) TOTAL =	446
		PPS# 6-00477	-0000 (HSI	P)				
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOO'
I55 BUSINESS/LINCOLN PARKWAY	1760+35.87	19.3	LT		1761+70.79	27.1	LT	134
	1760+36.69	12.0	LT		1761+70.79	22.9	LT	134
	0+49.48	0.8	LT		5+75.62	8.0	LT	527
	0+49.48	10.4	RT		5+75.62	2.8	LT	527
	6+25.03	2.6	RT		8+25.90	2.5	LT	201
	6+25.03	7.1	RT		8+24.00	10.0	RT	199
	34+79.66	1.1	LT		39+08.93	5.5	LT	436
	34+79.56	3.1	RT		39+07.60	3.3	LT	436
	40+27.67	2.0	RT		44+49.17	3.9	LT	433
	40+25.86	7.1	RT		44+49.35	4.0	RT	433
IL 10/121/WOODLAWN ROAD	135+71.11	0.9	LT		137+08.74	1.1	LT	138
	135+71.11	10.9	LT		137+08.74	11.0	LT	138
					PPS #6-004	177-0000 (HSII	P) TOTAL =	3730

	440	000600 - SIDEV	VALK REM	10VAL				
		PPS# 6-00625	-0000 (STF	P)				
ROADWAY	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	SQ FT
IL 10/121/WOODLAWN ROAD	138+57.74	69.3	LT	-	138+57.74	69.3	LT	30
IL 10/121/WOODLAWN ROAD	141+55.48	49.5	LT	-	141+65.40	44.3	LT	41
					PPS# 6-00	625-0000 (STF) TOTAL =	71

	44003100	- MEDIAN REM	ΛΟνΑΙ	L		
	PPS# 6-	00477-0000 (H	SIP)			
ROADWAY	STATION	SIDE	то	STATION	SIDE	SQ FT
IL 10/121/WOODLAWN ROAD	137+08.76	CENTER	-	137+35.50	CENTER	222
	138+39.10	CENTER	-	142+19.96	CENTER	3250
			PF	PS #6-00477-0000	(HSIP) TOTAL =	3472

	440	00100 - PAVEI	VIENT REN	ЛОVAL	•					
PPS# 6-00625-0000 (STP)										
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	SQ YD		
SB I-55 BUSINESS/LINCOLN PARKWAY	14+58.26	31.7	RT	-	16+07.78	41.3	RT	25		
	16+80.51	44.5	RT	-	17+43.26	44.5	RT	10		
STRINGER AVENUE	21+04.09	12.0	LT	-	23+21.36	43.3	LT	275		
					PPS# 6-00	0625-0000 (STF) TOTAL =	311		

	44000200	- DRIVEWAY	PAVEMEN	IT REIV	IOVAL			
		PPS# 6-00625	-0000 (STF	P)				
ROADWAY	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	SQ YD
IL 10/121/WOODLAWN ROAD	140+09.50	49.2	LT	-	141+63.89	41.3	LT	141
					PPS# 6-00	0625-0000 (STF) TOTAL =	141

				PAVING (S	HOULDER) SCH	EDULE - (STP)				
					PPS #6-00625-00	000				
						48102100 AGGREGATE WEDGE SHOULDER, TYPE B	48203023 HOT-MIX ASPHALT SHOULDERS, 6 1/2"	48203100 HOT-MIX ASPHALT SHOULDERS	35501316 HOT-MIX ASPHALT BASE COURSE, 8"	40600290 BITUMINOUS MATERIALS (TACK COAT)
ROADWAY	STATION	SIDE	то	STATION	SIDE	TON	SQ YD	TON	SQ YD	POUND
NB I55 BUSINESS/LINCOLN PARKWAY	4+77.42	LT - OUTSIDE	-	5+63.03	LT - OUTSIDE	12	0	5	56	25
	6+32.62	LT - OUTSIDE	-	35+73.21	LT - OUTSIDE	286	0	266	2607	1173
	45+15.19	LT - OUTSIDE	-	48+52.74	LT - OUTSIDE	29	0	32	316	142
	48+64.29	LT - OUTSIDE	-	67+83.47	LT - OUTSIDE	155	0	148	1697	763
	69+24.94	LT - OUTSIDE	-	72+76.03	LT - OUTSIDE	36	0	32	310	140
	73+26.33	LT - OUTSIDE	-	76+90.11	LT - OUTSIDE	21	0	27	173	77
	9+63.40	LT - INSIDE	-	15+20.75	LT - INSIDE	0	248	21	0	111
	24+68.19	LT - INSIDE	-	34+18.04	LT - INSIDE	0	422	35	0	190
	44+87.15	LT - INSIDE	-	48+29.98	LT - INSIDE	0	152	13	0	68
	48+38.97	LT - INSIDE	-	51+80.04	LT - INSIDE	0	152	13	0	68
	52+39.71	LT - INSIDE	-	63+72.32	LT - INSIDE	0	504	42	0	227
SB I55 BUSINESS/LINCOLN PARKWAY	1+32.22	RT - OUTSIDE	-	5+72.07	RT - OUTSIDE	51	0	54	367	164
	6+29.56	RT - OUTSIDE	-	17+64.57	RT - OUTSIDE	87	0	80	877	395
	17+64.57	RT - OUTSIDE	-	18+65.16	RT - OUTSIDE	8	0	4	0	20
	20+83.61	RT - OUTSIDE	-	24+95.37	RT - OUTSIDE	34	0	34	0	189
	24+95.37	RT - OUTSIDE	-	33+70.56	RT - OUTSIDE	78	0	88	788	355
	44+60.29	RT - OUTSIDE	-	47+96.21	RT - OUTSIDE	38	0	44	301	135
	48+07.86	RT - OUTSIDE	-	72+03.54	RT - OUTSIDE	241	0	300	2121	955
	72+86.01	RT - OUTSIDE	-	76+12.85	RT - OUTSIDE	43	0	36	167	79
	9+63.40	RT - INSIDE	-	15+20.75	RT - INSIDE	0	248	21	0	111
	24+68.19	RT - INSIDE	-	34+18.04	RT - INSIDE	0	422	35	0	190
	44+88.09	RT - INSIDE	-	48+19.57	RT - INSIDE	0	147	13	0	66
	48+28.55	RT - INSIDE	-	51+80.04	RT - INSIDE	0	156	13	0	70
	52+39.71	RT - INSIDE	-	63+72.32	RT - INSIDE	0	503	42	0	227
				PPS #6-00625-000	0 (STP) TOTAL =	1119	2954	1398	9780	5940
				PAVING (S	HOULDER) SCH	EDULE - (HSIP)				
					PPS #6-00477-00	000				
						48102100 AGGREGATE WEDGE SHOULDER, TYPE B	48203023 HOT-MIX ASPHALT SHOULDERS, 6 1/2"	48203100 HOT-MIX ASPHALT SHOULDERS	35501316 HOT-MIX ASPHALT BASE COURSE, 8"	40600290 BITUMINOUS MATERIALS (TACK COAT)
ROADWAY	STATION	SIDE	то	STATION	SIDE	TON	SQ YD	TON	SQ YD	POUND
NB 155 BUSINESS/LINCOLN PARKWAY	1758+06.02	LT - OUTSIDE	-	1760+96.49	LT - OUTSIDE	0	0	30	0	113
	1+28.28	LT - OUTSIDE		4+77.42	LT - OUTSIDE	28	0	15	0	113
	35+73.21	LT - OUTSIDE	-	38+83.16	LT - OUTSIDE	26	0	58	0	146
	40+39.96	LT - OUTSIDE	-	45+15.19	LT - OUTSIDE	51	0	31	0	146
SB 155 BUSINESS/LINCOLN PARKWAY	1758+05.77	RT - OUTSIDE	-	1760+96.99	RT - OUTSIDE	0	0	17	0	74
	33+70.56	RT - OUTSIDE	-	38+47.84	RT - OUTSIDE	45	0	38	0	74
	41+07.75	RT - OUTSIDE	-	44+60.29	RT - OUTSIDE	39	0	37	0	191
			F	PS #6-00477-0000	(HSIP) TOTAL =	189	0	226	0	857
			9	TP AND HSIP CON	/IBINED TOTAL =	1308	2954	1624	9780	6797

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
pw:\\IL084EBIDINTEG.:1ll:nois.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67	PE DR'ANMN Data\€ADsheets\D672E02-sht-05-Sc	- REVESED -
	PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -

								F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			SCHEDULI	: OF QU	ANTITIES			7706	•	LOGAN	343	57
										CONTRACT	NO. 7	2E02
	SCALE:	SHEET	OF	SHEETS	STA.	TO STA.			ILLINOIS FED. AI	D PROJECT		
Τ							 (22)RS- 	6, MED	& (23) RS-5			

				PAVING SCHEDU	JLE - KOADW	/AY (PPS# 6-00625-0000	STP)				
						40600625 LEVELING BINDER (MACHINE METHOD) N50	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL	40603335 HMA SURFACE COURSE MIX "D", N50	40603535 POLYMERIZED HMA SURFACE COURSE MIX "D", N50	40800050 INCIDENTAL HMA SURFACING	40600290 BITUMINOU MATERIALS (TACK COAT
						(3/4", 1 1/4", 2 1/4")	(VAR. DEPTH)	(1 1/2")	(1 1/2")	(1 3/4")	
ROADWAY	STATION	SIDE	TO	STATION	SIDE	TON	TON	TON	TON	TON	POUND
NB 155 BUSINESS/LINCOLN PARKWAY	9+63.40	LT	-	18+75.00	LT	348	171	232	0	0	2486
	18+75.00	LT	-	20+82.79	LT	33	0	66	0	0	707
	20+82.79	LT	-	34+18.04	LT	489	167	326	0	0	3495
	44+87.16	LT	-	48+00.00	LT	106	94	71	0	0	756
	48+00.00	LT	-	48+45.64	LT	0	0	11	0	0	68
	48+41.52	LT	-	48+90.00	LT	0	0	12	0	0	73
	48+90.00	LT	-	74+00.00	LT	997	414	667	0	0	7121
SB I55 BUSINESS/LINCOLN PARKWAY	9+63.40	RT	-	18+75.00	RT	232	234	278	0	0	2981
	18+75.00	RT	-	20+82.79	RT	33	0	66	0	0	706
	20+82.79	RT	-	34+18.04	RT	280	253	336	0	0	3597
	44+87.16	RT	-	47+50.00	RT	49	105	59	0	0	631
	47+50.00	RT	-	48+17.04	RT	0	0	18	0	0	107
	48+12.97	RT	-	48+75.00	RT	0	0	16	0	0	98
	48+75.00	RT	-	74+00.00	RT	556	1213	667	0	0	7148
STALHUT DRIVE	38+60.00	LT & RT	-	39+66.18	LT & RT	46	0	91	0	0	731
5TH STREET	14+81.96	LT & RT	-	16+15.08	LT & RT	50	0	101	0	0	811
STRINGER AVENUE	19+89.75	LT & RT	-	22+36.77	LT & RT	0	0	0	0	100	462
KENWOOD DRIVE	71+94.63	RT	-	72+96.53	RT	0	0	0	0	31	140
CEMETARY ENTRANCE (EAST)	72+76.03	LT	-	73+26.33	RT	0	0	0	0	7	30
CROWN CORRECTION LINCOLN PARKWAY	15+20.75	CENTER	-	18+50.00	CENTER	12	0	0	0	0	0
	24+04.47	CENTER	-	24+68.19	CENTER	4	0	0	0	0	0
	63+72.32	CENTER	-	72+56.61	CENTER	40	0	0	0	0	0
			PP:	s #6-00625-0000 (s	STP) TOTAL =	3275	2651	3017	0	138	32148
				PAVING SCHEDU	F - ROADW	AY (PPS # 6+00477-0000	HCID)				
		1		7,111,10 0011200	I ROADW	·		40002225	40002525	40000000	40000000
				7.7.11.0	I NOADW	40600625 LEVELING BINDER (MACHINE METHOD) N50	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL	40603335 HMA SURFACE COURSE MIX "D", N50	40603535 POLYMERIZED HMA SURFACE COURSE MIX "D", N50	40800050 INCIDENTAL HMA SURFACING	40600290 BITUMINOU MATERIALS (TACK COAT
				THING SUITED	I NOADW	40600625 LEVELING BINDER (MACHINE METHOD)	X4060605 LEVELING BINDER (MACHINE METHOD)	HMA SURFACE COURSE	POLYMERIZED HMA SURFACE	INCIDENTAL HMA	BITUMINOL MATERIAL
ROADWAY	STATION	SIDE	то	STATION	SIDE	40600625 LEVELING BINDER (MACHINE METHOD) N50	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL	HMA SURFACE COURSE MIX "D", N50	POLYMERIZED HMA SURFACE COURSE MIX "D", N50	INCIDENTAL HMA SURFACING	BITUMINO MATERIAL
ROADWAY NB 155 BUSINESS/LINCOLN PARKWAY	STATION 1758+06.11	SIDE LT				40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4")	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH)	HMA SURFACE COURSE MIX "D", N50 (1 1/2")	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2")	INCIDENTAL HMA SURFACING (1 3/4")	BITUMINO MATERIAL (TACK COA
			то	STATION	SIDE	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON	INCIDENTAL HMA SURFACING (1 3/4") TON	BITUMINO MATERIAI (TACK COA POUND
	1758+06.11	LT	TO	STATION 1761+95.16	SIDE LT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON	INCIDENTAL HMA SURFACING (1 3/4") TON 0	BITUMINO MATERIAI (TACK COA POUND 869
	1758+06.11 0+35.50	LT LT	TO -	STATION 1761+95.16 1+32.14	SIDE LT LT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0	BITUMINO MATERIAI (TACK COA POUND 869 472
	1758+06.11 0+35.50 1+32.10	LT LT LT	TO -	STATION 1761+95.16 1+32.14 9+63.40	SIDE LT LT LT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0	POUND 869 472 3225
	1758+06.11 0+35.50 1+32.10 34+18.04	LT LT LT	TO	STATION 1761+95.16 1+32.14 9+63.40 38+00.00	SIDE LT LT LT LT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0	### BITUMINO MATERIAI (TACK COA) POUND 869 472 3225 1337
	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00	LT LT LT LT	TO	STATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00	SIDE LT LT LT LT LT LT LT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0	BITUMINO MATERIAI (TACK COA POUND 869 472 3225 1337 1081
NB 155 BUSINESS/LINCOLN PARKWAY	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00	LT LT LT LT LT	TO	STATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16	SIDE LT LT LT LT LT LT LT LT LT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356
NB 155 BUSINESS/LINCOLN PARKWAY	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01	LT LT LT LT LT LT RT	TO	STATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16	SIDE LT LT LT LT LT LT LT LT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329
NB 155 BUSINESS/LINCOLN PARKWAY	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50	LT LT LT LT LT LT RT RT	TO	STATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14	SIDE LT LT LT LT LT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247
NB 155 BUSINESS/LINCOLN PARKWAY	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14	LT LT LT LT LT LT RT RT RT	TO	STATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40	SIDE LT LT LT LT LT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 0 585	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247 2849
NB 155 BUSINESS/LINCOLN PARKWAY	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14 34+18.04	LT LT LT LT LT LT RT RT RT RT	TO	\$TATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40 38+00.00	SIDE LT LT LT LT LT RT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222 127	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 0 585	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266 153	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0 0 0	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247 2849 1636
NB 155 BUSINESS/LINCOLN PARKWAY	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14 34+18.04 38+00.00	LT LT LT LT LT LT RT RT RT RT RT RT	TO	STATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00	SIDE LT LT LT LT LT RT RT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222 127 60	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 0 585 120 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266 153 120	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247 2849 1636 960
NB 155 BUSINESS/LINCOLN PARKWAY SB 155 BUSINESS/LINCOLN PARKWAY	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14 34+18.04 38+00.00 41+60.00	LT LT LT LT LT LT RT RT RT RT RT RT RT	TO	STATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16	SIDE LT LT LT LT LT RT RT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222 127 60 86	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 0 585 120 0 46	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266 153 120 104	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247 2849 1636 960 1112
NB 155 BUSINESS/LINCOLN PARKWAY SB 155 BUSINESS/LINCOLN PARKWAY EB IL 10/121/WOODLAWN ROAD	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14 34+18.04 38+00.00 41+60.00 133+70.43	LT LT LT LT LT LT RT RT RT RT RT RT RT RT	TO	STATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 137+53.56	SIDE LT LT LT LT LT RT RT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222 127 60 86 84	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 585 120 0 46	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266 153 120 104 0	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247 2849 1636 960 1112 1358
NB 155 BUSINESS/LINCOLN PARKWAY SB 155 BUSINESS/LINCOLN PARKWAY EB IL 10/121/WOODLAWN ROAD	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14 34+18.04 38+00.00 41+60.00 133+70.43 137+53.56	LT LT LT LT LT LT RT RT RT RT RT RT RT RT RT	TO	STATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 137+53.56 138+19.80	SIDE LT LT LT LT LT RT RT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222 127 60 86 84 11	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 5885 120 0 46 0 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266 153 120 104 0	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 169 22	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247 2849 1636 960 1112 1358 173
NB 155 BUSINESS/LINCOLN PARKWAY SB 155 BUSINESS/LINCOLN PARKWAY EB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES)	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14 34+18.04 38+00.00 41+60.00 133+70.43 137+53.56 138+19.80	LT LT LT LT LT LT RT	TO	\$TATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 137+53.56 138+19.80 143+90.83	SIDE LT LT LT LT LT RT RT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222 127 60 86 84 11 75	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 585 120 0 46 0 0 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266 153 120 104 0 0 0	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 169 22	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247 2849 1636 960 1112 1358 173 1206
SB I55 BUSINESS/LINCOLN PARKWAY SB I55 BUSINESS/LINCOLN PARKWAY EB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) WB IL 10/121/WOODLAWN ROAD	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14 34+18.04 38+00.00 41+60.00 133+70.43 137+53.56 138+19.80 133+70.43	LT LT LT LT LT LT RT	TO	\$TATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 137+53.56 138+19.80 143+90.83 137+53.56	SIDE LT LT LT LT LT RT RT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222 127 60 86 84 11 75 54	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 585 120 0 46 0 0 0 0 0 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266 153 120 104 0 0 0 0	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 169 22 150	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247 2849 1636 960 1112 1358 173 1206 861
SB IS5 BUSINESS/LINCOLN PARKWAY SB IS5 BUSINESS/LINCOLN PARKWAY EB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) WB IL 10/121/WOODLAWN ROAD	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14 34+18.04 38+00.00 41+60.00 133+70.43 137+53.56 138+19.80 133+70.43 137+53.56	LT LT LT LT LT LT RT RT RT RT RT RT RT RT LT	TO	\$TATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 137+53.56 138+19.80 143+90.83 137+53.56 138+19.80	SIDE LT LT LT LT LT LT RT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222 127 60 86 84 11 75 54 11	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 585 120 0 46 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266 153 120 104 0 0 0 0 0	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 169 22 150 107	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247 2849 1636 960 1112 1358 173 1206 861 175
SB I55 BUSINESS/LINCOLN PARKWAY EB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) WB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) 5TH STREET (WEST)	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14 34+18.04 38+00.00 41+60.00 133+70.43 137+53.56 138+19.80 133+70.43 137+53.56 138+19.80	LT LT LT LT LT LT LT RT RT RT RT RT RT LT	TO	STATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 137+53.56 138+19.80 143+90.83 137+53.56 138+19.80 143+90.83 13+30.19	SIDE LT LT LT LT LT LT RT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222 127 60 86 84 11 75 54 11 119 49	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 585 120 0 46 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266 153 120 104 0 0 0 0 0 0 99	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 169 22 150 107 22 239	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247 2849 1636 960 1112 1358 173 1206 861 175 1918
SB I55 BUSINESS/LINCOLN PARKWAY EB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) WB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) 5TH STREET (WEST) 5TH STREET (EAST)	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14 34+18.04 38+00.00 41+60.00 133+70.43 137+53.56 138+19.80 133+70.43 137+53.56 138+19.80 12+41.03 14+00.19	LT LT LT LT LT LT RT RT RT RT	TO	STATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 137+53.56 138+19.80 143+90.83 137+53.56 138+19.80 143+90.83 13+30.19 14+81.96	SIDE LT LT LT LT LT LT RT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222 127 60 86 84 11 75 54 11 119	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 585 120 0 46 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266 153 120 104 0 0 0 0 0 0	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 169 22 150 107 22 239 0	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247 2849 1636 960 1112 1358 173 1206 861 175 1918 794
SB I55 BUSINESS/LINCOLN PARKWAY EB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) WB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) 5TH STREET (WEST) 5TH STREET (EAST) SIDE ROAD (EAST)	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14 34+18.04 38+00.00 41+60.00 133+70.43 137+53.56 138+19.80 133+70.43 137+53.56 138+19.80 12+41.03	LT LT LT LT LT LT LT RT RT RT RT RT RT LT	TO	\$TATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 137+53.56 138+19.80 143+90.83 137+53.56 138+19.80 143+90.83 13+30.19 14+81.96 6+42.63	SIDE LT LT LT LT LT LT RT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222 127 60 86 84 11 75 54 11 119 49 32	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 585 120 0 46 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266 153 120 104 0 0 0 0 0 0 0 99 64	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 169 22 150 107 22 239 0 0	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247 2849 1636 960 1112 1358 173 1206 861 175 1918
SB I55 BUSINESS/LINCOLN PARKWAY EB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) WB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) 5TH STREET (WEST) 5TH STREET (EAST) SIDE ROAD (EAST) SIDE ROAD (WEST)	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14 34+18.04 38+00.00 41+60.00 133+70.43 137+53.56 138+19.80 133+70.43 137+53.56 138+19.80 12+41.03 14+00.19 5+57.68 5+67.86	LT LT LT LT LT LT RT RT RT RT	TO	\$TATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 137+53.56 138+19.80 143+90.83 137+53.56 138+19.80 143+90.83 13+30.19 14+81.96 6+42.63 6+42.57	SIDE LT LT LT LT LT LT RT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222 127 60 86 84 11 75 54 11 119 49 32 0	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 585 120 0 46 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266 153 120 104 0 0 0 0 0 0 99 64 0 0 0	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 169 22 150 107 22 239 0 0 0	INCIDENTAL HMA SURFACING (1 3/4") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247 2849 1636 960 1112 1358 173 1206 861 175 1918 794 518
SB I55 BUSINESS/LINCOLN PARKWAY EB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) WB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) 5TH STREET (WEST) 5TH STREET (EAST) SIDE ROAD (EAST)	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14 34+18.04 38+00.00 41+60.00 133+70.43 137+53.56 138+19.80 133+70.43 137+53.56 138+19.80 12+41.03 14+00.19 5+57.68 5+67.86 6+25.03	LT LT LT LT LT LT LT RT RT RT RT RT RT LT RT LT RT LT RT LT RT	TO	\$TATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 137+53.56 138+19.80 143+90.83 137+53.56 138+19.80 143+90.83 13+30.19 14+81.96 6+42.63 6+42.57 9+63.40	SIDE LT LT LT LT LT LT RT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222 127 60 86 84 11 75 54 11 119 49 32 0 0 0 15	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 585 120 0 46 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266 153 120 104 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 169 22 150 107 22 239 0 0 0 0 0	INCIDENTAL HMA SURFACING (13/4") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247 2849 1636 960 1112 1358 173 1206 861 175 1918 794 518 130 135
SB I55 BUSINESS/LINCOLN PARKWAY EB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) WB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) 5TH STREET (WEST) 5TH STREET (EAST) SIDE ROAD (EAST) SIDE ROAD (WEST)	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14 34+18.04 38+00.00 41+60.00 133+70.43 137+53.56 138+19.80 133+70.43 137+53.56 138+19.80 12+41.03 14+00.19 5+57.68 5+67.86 6+25.03 34+18.04	LT LT LT LT LT LT LT RT RT RT RT RT RT LT	TO	\$TATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 137+53.56 138+19.80 143+90.83 137+53.56 138+19.80 143+90.83 13+30.19 14+81.96 6+42.63 6+42.57 9+63.40 38+00.00	SIDE LT LT LT LT LT LT RT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222 127 60 86 84 11 75 54 11 119 49 32 0 0 0 15	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 585 120 0 0 46 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266 153 120 104 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 169 22 150 107 22 239 0 0 0 0 0 0 0	INCIDENTAL HMA SURFACING (13/4") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POUND 869 472 3225 1337 1081 1356 1329 247 2849 1636 960 1112 1358 173 1206 861 175 1918 794 518 130 135 0
SB I55 BUSINESS/LINCOLN PARKWAY EB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) WB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES) 5TH STREET (WEST) 5TH STREET (EAST) SIDE ROAD (EAST) SIDE ROAD (WEST)	1758+06.11 0+35.50 1+32.10 34+18.04 38+00.00 41+60.00 1758+06.01 0+35.50 1+32.14 34+18.04 38+00.00 41+60.00 133+70.43 137+53.56 138+19.80 133+70.43 137+53.56 138+19.80 12+41.03 14+00.19 5+57.68 5+67.86 6+25.03	LT LT LT LT LT LT LT RT RT RT RT RT RT LT RT LT RT LT RT LT RT	TO	\$TATION 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 1761+95.16 1+32.14 9+63.40 38+00.00 41+60.00 44+87.16 137+53.56 138+19.80 143+90.83 137+53.56 138+19.80 143+90.83 13+30.19 14+81.96 6+42.63 6+42.57 9+63.40	SIDE LT LT LT LT LT LT RT RT RT RT	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON 54 29 452 187 67 190 83 15 222 127 60 86 84 11 75 54 11 119 49 32 0 0 0 15	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON 0 0 170 116 0 2 0 0 585 120 0 46 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 108 59 301 125 134 127 165 31 266 153 120 104 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 169 22 150 107 22 239 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	INCIDENTAL HMA SURFACING (13/4") TON 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	BITUMINOU MATERIAL (TACK COA) POUND 869 472 3225 1337 1081 1356 1329 247 2849 1636 960 1112 1358 173 1206 861 175 1918 794 518 130 0

	USER NAME = whitlingme	DESIGNED -	REVISED -							F.A.U. RTE.	SECTION	COUNTY	SHEETS	NO.
pw:\\ILØ84EBIDINTEG.:111:no:s.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67	72EDRAMMOData\£ADsheets\D672E02-sht-05-S		STATE OF ILLINOIS				E OF QUANTITIES		7706	•	LOGAN	343	58
	PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION								CONTRACT	T NO. 72	E02
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	0F	SHEETS STA.	TO STA.		ILLINOIS FED. A	ID PROJECT		
									• (22)RS-	6, MED &	(23) RS-5			

								P	AVEMENT I	PATCHING SCHEDULE					
									PPS #6-0	00625-0000 (STP)					
										44200094 PAVEMENT PATCHING TYPE II, 8 INCH	44200090 PAVEMENT PATCHING TYPE III, 8 INCH	44200101 PAVEMENT PATCHING TYPE IV, 8 INCH	44200120 PAVEMENT PATCHING TYPE II, 10 INCH	44200144 PAVEMENT PATCHING TYPE II, 12 INCH	44200148 PAVEMENT PATCHII TYPE III, 12 INCH
ROADWAY	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	LENGTH	WIDTH	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD
NB 155 BUSINESS/LINCOLN PARKWAY	5+70.17	50.2	LT	-	6+34.39	48.8	LT	VARIES	VARIES			102.0			
	47+75.00	10.0	LT	-	47+75.00	34.0	LT	24.0	5.0					13.3	
	49+50.00	10.0	LT	-	49+50.00	34.0	LT	24.0	4.5					12.0	
SB 155 BUSINESS/LINCOLN PARKWAY	47+75.00	10.0	RT	-	47+75.00	RT	LT	24.0	5.0					13.3	
POSTVILLE DRIVE (FRONTAGE ROAD)	6+71.26	67.5	LT	-	6+71.38	88.1	LT	20.4	7.7		17.5				
	16+80.51	67.5	LT	-	16+80.29	88.5	LT	21.1	7.6		17.8				
	31+71.27	66.1	LT	-	31+71.27	86.7	LT	20.6	6.6		15.1				
	54+61.72	69.3	LT	-	54+61.63	87.4	LT	18.0	8.7		17.4				
	62+20.65	69.3	LT	-	62+20.65	87.3	LT	18.0	6.7	13.4					
STRINGER AVENUE	20+81.19	11.9	RT	-	20+80.69	11.9	LT	24.3	5.5				14.9		
NB 155 BUSINESS/LINCOLN PARKWAY AS DIRECTED BY ENGINEER		LOCATIO	NS DET	ERMIN	IED BY ENGINEER			VARIES	VARIES					316.0	60.0
SB 155 BUSINESS/LINCOLN PARKWAY AS DIRECTED BY ENGINEER		LOCATIO	NS DET	ERMIN	IED BY ENGINEER			VARIES	VARIES					355.0	34.0
						ı	PPS #6-0	0625-0000 (STP) TOTAL =	13.4	67.8	102.0	15	710	94.0

50104400 - CONCRETE HEADWALL REMOVAL									
PPS	# 6-00625-0000 (ST	P)							
ROADWAY	STATION	OFFSET	SIDE	EACH					
I-55 BUSINESS/LINCOLN PARKWAY	25+08.15	1.7	RT	1.0					
	65+83.65	92.5	LT	1.0					
	66+10.62	60.2	LT	1.0					
	66+22.97	45.0	LT	1.0					
	66+96.81	43.6	RT	1.0					
IL 10/121/WOODLAWN ROAD	135+90.26	43.6	LT	1.0					
	PPS# 6-00625-0000 (STP) TOTAL =								

	55	5100500 - STOF	RM SEWER R	EMOVA	L 12"					
PPS# 6-00477-0000 (HSIP)										
ROADWAY STATION OFFSET SIDE TO STATION OFFSET SIDE										
155 BUSINESS/LINCOLN PARKWAY	2+70.99	2.2	RT		2+70.76	2.7	LT	5.0		
	2+70.81	2.4	LT		5+64.83	1.3	LT	294.0		
	5+49.26	1.7	LT		5+49.64	9.1	LT	7.0		
	6+39.11	0.8	RT		6+70.87	1.9	RT	32.0		
	6+51.51	1.1	RT		6+51.60	9.5	RT	9.0		
	6+70.77	1.8	RT		8+28.89	0.7	RT	158.0		
					PPS #	‡6-0047 7-000 0	(HSIP) TOTAL =	505		

6025550 - MANHOLES TO BE ADJUSTED										
PPS# 6-00625-0000 (STP)										
ROADWAY STATION OFFSET SIDE										
STALHUT DRIVE	LT	1.0								
5TH STREET	15+64.86	28.2	RT	1.0						
PPS# 6-00625-0000 (STP) TOTAL =										
PPS# 6-00477-0000 (HSIP)										
	PPS# 6-00477-000	0 (HSIP)								
ROADWAY	PPS# 6-00477-000 STATION	0 (HSIP) OFFSET	SIDE	EACH						
ROADWAY 5TH STREET		, ` 	SIDE RT	EACH 1.0						
	STATION	OFFSET								
5TH STREET	STATION 12+67.34	OFFSET 26.0	RT	1.0						
	STATION 12+67.34 14+31.59 136+38.88	26.0 26.5 44.0	RT LT	1.0						

60265700 - VALVE VAULTS TO BE ADJUSTED										
PPS# 6-00625-0000 (STP) ROADWAY STATION OFFSET SIDE										
ROADWAY	STATION	OFFSET	SIDE	EACH						
5TH STREET	15+10.53	35.2	LT	1.0						
5TH STREET	15+17.52	32.6	RT	1.0						
	PPS	# 6-00625-0000	(STP) TOTAL =	2						
PPS# 6-00477-0000 (HSIP)										
ROADWAY	STATION	OFFSET	SIDE	EAC						
		i e								
L 10/121/WOODLAWN ROAD	136+37.35	35.7	LT	1.0						
L 10/121/WOODLAWN ROAD	136+37.35 139+99.76	35.7 33.1	LT LT							
L 10/121/WOODLAWN ROAD				1.0 1.0 1.0						
- 10/121/WOODLAWN ROAD 5TH STREET	139+99.76	33.1	LT	1.0 1.0						
	139+99.76 140+14.03	33.1 30.5	LT LT	1.0 1.0 1.0						
L 10/121/WOODLAWN ROAD 5TH STREET	139+99.76 140+14.03 12+62.83 13+02.09	33.1 30.5 29.9	LT LT LT	1.0						

6050	00060 - REMOVIN	G INLETS		
P	PS# 6-00477-0000	(HSIP)		
ROADWAY	STATION	OFFSET	SIDE	EACH
155 BUSINESS/LINCOLN PARKWAY	2+70.79	2.7	LT	1.0
	2+70.96	2.1	RT	1.0
	5+49.67	9.1	LT	1.0
	5+49.29	1.8	LT	1.0
	6+51.51	1.1	RT	1.0
	6+51.64	9.5	RT	1.0
	8+28.89	0.7	RT	1.0
	PPS #	‡6-00477-0000	(HSIP) TOTAL =	7

60603800 - COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12												
PPS# 6-00625-0000 (STP)												
ROADWAY	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOOT				
NB I55 BUSINESS/LINCOLN PARKWAY	46+42.00	4.4	LT	-	47+42.00	4.4	LT	100				
	48+50.94	4.4	LT	-	50+17.00	4.4	LT	166				
SB I55 BUSINESS/LINCOLN PARKWAY	46+42.00	4.4	RT	-	48+07.70	4.4	RT	166				
	49+17.00	4.4	RT	-	50+17.00	4.4	RT	100				
PPS# 6-00625-0000 (STP) TOTAL =												

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
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	PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -

STATE OF	ILLINOIS
DEPARTMENT OF	TRANSPORTATION

						F.A.U. RTE.	SECT	ΓΙΟΝ		COUNTY	TOTAL SHEETS	SHEET NO.
ĺ		SCHEDULE	OF QUA	ANTITIES		7706		•		LOGAN	343	59
										CONTRACT	NO.	72E02
SCALE:	SHEET	OF	SHEETS	STA.	TO STA.			ILLINOIS	FED. AI	D PROJECT		

6060440	0 - COMBINATIO	N CONCRI	ETE CURB	AND	GUTTER, TYPE B-	6.18							
	PPS# 6-00625-0000 (STP)												
ROADWAY	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOOT					
STRINGER AVENUE	19+69.66	13.5	LT	-	20+84.08	13.4	LT	114					
	PPS# 6-00625-0000 (STP) TOTAL =												

606050	60605000 - COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24							
PPS# 6-00625-0000 (STP)								
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
IL 10/121/WOODLAWN ROAD	140+09.49	46.7	LT	-	140+63.05	46.6	LT	54
	141+11.33	43.4	LT	-	141+63.62	38.7	LT	53
STRINGER AVENUE	20+84.08	13.4	LT	-	22+37.35	5.2	RT	158
	PPS# 6-00625-0000 (STP) TOTAL =							

6060856	2 - COMBINATIO	N CONCRE	TE CURB	AND (GUTTER, TYPE M	-4.12			
PPS# 6-00625-0000 (STP)									
ROADWAY	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOOT	
NB 155 BUSINESS/LINCOLN PARKWAY	9+70.40	5.4	LT	-	15+13.75	5.4	LT	544	
	24+75.18	5.4	LT	-	34+11.04	5.4	LT	936	
	44+94.15	4.4	LT	-	44+42.00	4.4	LT	148	
	47+42.00	4.4	LT	-	48+13.39	4.4	LT	72	
	50+17.00	4.4	LT	-	51+73.04	0.8	LT	157	
	52+46.71	0.8	LT	-	63+65.32	4.4	LT	1119	
SB 155 BUSINESS/LINCOLN PARKWAY	9+70.40	4.4	RT	-	15+13.75	4.4	RT	544	
	24+75.18	4.4	RT	-	34+11.04	4.4	RT	936	
	44+94.15	4.4	RT	-	46+42.00	4.4	RT	148	
	48+45.22	4.4	RT	-	49+17.00	4.4	RT	72	
	50+17.00	4.4	RT	-	51+73.04	4.4	RT	156	
	52+46.71	4.4	RT	- 1	63+65.32	4.4	RT	1119	
					PPS# 6-00625	-0000 (STP) TOTAL =	5951	

60618300 - CONCRETE MEDIAN SURFACE, 4 INCH										
PPS# 6-00625-0000 (STP)										
ROADWAY	STATION	SIDE	то	STATION	SIDE	SQ FT				
155 BUSINESS/LINCOLN PARKWAY	9+70.40	CENTER	-	15+13.75	CENTER	5298				
155 BUSINESS/LINCOLN PARKWAY	24+75.18	CENTER	-	34+11.04	CENTER	9209				
155 BUSINESS/LINCOLN PARKWAY	44+94.15	CENTER	-	48+13.39	CENTER	2797				
I55 BUSINESS/LINCOLN PARKWAY	48+45.22	CENTER	-	51+73.04	CENTER	2853				
I55 BUSINESS/LINCOLN PARKWAY	52+46.71	CENTER	-	63+65.32	CENTER	9869				
	PPS# 6-00625-0000 (STP) TOTAL = 30026									

PPS# 6-00625-0000 (STP)									
ROADWAY STATION LOCATION TO STATION LOCATION									
155 BUSINESS/LINCOLN PARKWAY	9+63.40	CENTER	-	9+70.40	CENTER	91			
	15+13.75	CENTER	-	15+20.75	CENTER	91			
	24+68.19	CENTER	-	24+75.18	CENTER	147			
	34+11.04	CENTER	-	34+18.04	CENTER	91			
	44+87.15	CENTER	-	44+94.16	CENTER	84			
	48+06.69	CENTER	-	48+15.01	CENTER	100			
	48+35.86	CENTER	-	48+44.20	CENTER	100			
	51+73.04	CENTER	-	51+80.04	CENTER	47			
	52+39.71	CENTER	-	52+46.71	CENTER	47			
	63+65.32	CENTER	-	63+72.32	CENTER	84			

60622400 CONCRETE MEDIAN, TYPE SM-6.06								
PPS# 6-00477-0000 (HSIP)								
ROADWAY	STATION	ТО	STATION	LOCATION	SQ FT			
155 BUSINESS/LINCOLN PARKWAY	1760+36.69	-	1761.46.71	CENTER	603			
	PPS #6-00477-0000 (HSIP) TOTAL = 603							

60622800 CONCRETE MEDIAN, TYPE SM-6.12							
	PPS# 6-00477-0000 (HSIP)						
ROADWAY	STATION	то	STATION	LOCATION	SQ FT		
IL 10/121/WOODLAWN ROAD	135+71.11	-	137+09.59	CENTER	961		
IL 10/121/WOODLAWN ROAD	138+83.13	-	141+37.52	CENTER	1600		
PPS #6-00477-0000 (HSIP) TOTAL = 2561							

		- PIPE CULVERT		-					
ROADWAY	DESCRIPTION	S# 6-00625-000 (STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOOT
155 BUSINESS/LINCOLN PARKWAY	30" CMP	5+59.86	53.7	LT	-	6+39.99	52.0	LT	80
33 B33111233/ E11133E11 / 1111111711	36" CMP	6+71.16	58.6	LT	-	6+71.85	92.5	LT	34
	36" CMP	6+70.52	65.8	RT	-	6+69.82	99.8	RT	34
	12" PLASTIC	6+28.08	104.8	LT	-	6+67.50	98.0	LT	40
	18" CMP	6+97.55	110.3	LT	-	6+94.65	105.1	LT	6
	30" CMP	16+80.64	60.7	LT	-	16+79.89	93.8	LT	33
	12" RCP	16+80.53	2.2	LT	-	16+98.50	2.1	LT	12
	24" CONC	25+08.15	44.3	LT	-	25+08.08	49.3	RT	5
	15" CONC	31+58.16	0.5	RT	-	31+65.16	0.6	RT	7
	15" CONC	31+77.21	0.5	RT	-	31+84.20	0.9	RT	7
	30" CMP	31+71.42	61.7	LT	-	31+71.31	93.1	LT	32
	24" CMP	47+76.06	54.4	RT	-	47+82.07	45.2	RT	11
	24" CMP	48+10.15	2.1	RT	-	48+11.24	0.4	LT	2
	24" CMP	48+12.47	1.5	LT	-	48+13.56	3.2	LT	2
	24" CMP	48+41.94	46.7	LT	-	48+50.13	59.3	LT	15
	15" CMP	50+22.35	0.3	LT	-	50+33.35	0.0	LT	11
	15" CMP	50+45.76	0.3	RT	-	50+56.76	0.6	RT	11
	15" CMP	51+79.11	0.0	RT	-	52+39.19	0.3	LT	60
	30" CMP	54+61.28	61.9	LT	-	54+62.89	92.4	LT	32
	30" CMP	54+62.27	63.3	RT	-	54+63.69	93.3	RT	30
	15" CONC	61+85.54	0.1	RT	-	62+14.33	0.2	LT	29
	15" CONC	62+26.99	0.1	LT	-	62+55.99	0.2	LT	29
	30" CMP	62+20.44	61.8	RT	-	62+20.35	91.9	RT	30
	24" CMP	62+20.88	60.6	LT	-	62+20.32	93.6	LT	33
	15" CMP	68+30.88	0.3	LT	-	69+38.71	0.0	LT	108
	15" CMP	68+51.95	53.0	RT	-	69+11.84	52.9	RT	60
IL 10/121/WOODLAWN ROAD	15" CMP	135+88.70	36.9	LT	-	135+90.26	43.6	LT	7
						PPS# 6-006	25-000 (STF) TOTAL =	760

- 1	FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
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-		PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -
- [Default	PLOT DATE = 10/21/2016	DATE -	REVISED -

STATE	OF	- ILLINOIS
DEPARTMENT (0F	TRANSPORTATION

	SCHEDULE OF QUANTITIES							SE	CTION		COU
											LO
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	SHEET	OF	SHEETS	STA.	TO STA.				ILLINOIS	FED. Al	D PROJE
	•	•	_		•	(22)RS-6	, MED	& (23) RS-	5		

		PPS# 6-00625-	0000 (STP)						
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOO
NB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	9+63.40	22.5	LT	-	34+18.04	22.5	LT	246
	SHOULDER DIAGONAL	9+63.40	33.5	LT	-	34+18.04	33.5	LT	196
	YELLOW CENTERLINE	15+20.75	11.5	LT	-	19+60.75	11.5	LT	88
	TURN LANE	20+09.80	7.3	LT	_	21+99.62	11.5	LT	19
	YELLOW CENTERLINE	20+49.34	7.0	RT	-	24+68.19	11.5	LT	84
SB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	9+63.40	22.0	RT	-	34+18.04	22.0	RT	246
,,	SHOULDER DIAGONAL	9+63.40	33.5	RT	-	15+10.08	33.5	RT	44
	YELLOW CENTERLINE	15+20.75	10.5	RT	_	19+60.75	10.5	RT	88
	TURN LANE	16+58.26	335.5	RT	-	19+48379	33.5	RT	29
	YELLOW CENTERLINE	20+49.34	10.5	RT	-	24+68.19	10.5	RT	84
	SHOULDER DIAGONAL	21+75.76	33.5	RT	-	24+68.18	33.5	RT	23
NB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	44+87.15	22.0	LT	 -	74+00.00	22.0	LT	291
NO 133 DOSINESSY EINCOLIN I ANKWAT	SHOULDER DIAGONAL	44+87.15	34.0	LT	+-	74+00.00	34.0	LT	233
	YELLOW CENTERLINE	63+72.32	10.0	LT	+ -	68+13.03	10.0	LT	88
				LT	+-			LT	
	YELLOW CENTERLINE	69+18.56	10.0	LT	+	72+19.16	10.0		60
CD IEE DIICINECC/UNICOUN DARWAY	TURN LANE	73+14.08	9.0		-	74+49.51	9.0	LT	14
SB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	44+87.15	22.0	RT	-	74+00.00	22.0	RT	291
	SHOULDER DIAGONAL	44+87.15	34.0	RT	-	74+00.00	34.0	RT	233
	YELLOW CENTERLINE	63+72.32	10.0	RT	-	68+13.03	1.5	LT	88
	TURN LANE	66+02.25	10.5	RT	-	68+17.25	10.5	RT	22
	YELLOW CENTERLINE	69+56.39	10.5	RT	-	72+18.35	1.5	LT	53
	TURN LANE	71+07.82	10.5	RT	-	72+22.82	10.5	RT	12
	YELLOW CENTERLINE	73+14.08	6.0	RT	-	74+49.51	6.0	RT	28
STALHUT DRIVE	YELLOW CENTERLINE	38+70.37	0.0	RT	-	39+61.58	0.0	RT	18
5TH STREET	YELLOW CENTERLINE	15+75.08	5.0	RT	-	16+15.08	5.0	RT	8
	TURN LANE	15+75.08	6.0	LT	-	16+15.08	6.0	LT	4
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOO
NB I-55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	1758+05.90	43.0	LT	-	1761+79.62	43.0	LT	
		-							37
AT IL 10 INTERSECTION	I WHITE CENTERLINE	I 0+45.28	23.0	LT	_	9+63.40	22.5		
AT IL 10 INTERSECTION	WHITE CENTERLINE WHITE TURN LANE	0+45.28 1758+05.82	23.0 31.0	LT LT	-	9+63.40 1759+40.65	22.5 31.0	LT	92
AT IL 10 INTERSECTION	WHITE TURN LANE	1758+05.82	31.0	LT		1759+40.65	31.0	LT LT	92 14
AT IL 10 INTERSECTION	WHITE TURN LANE WHITE TURN LANE	1758+05.82 0+38.89	31.0 7.1	LT LT	-	1759+40.65 2+21.62	31.0 11.0	LT LT LT	92 14 18
AT IL 10 INTERSECTION	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE	1758+05.82 0+38.89 0+71.02	31.0 7.1 7.0	LT LT RT	-	1759+40.65 2+21.62 5+47.21	31.0 11.0 11.5	LT LT LT	92 14 18 95
AT IL 10 INTERSECTION	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE	1758+05.82 0+38.89 0+71.02 6+22.44	31.0 7.1 7.0 6.6	LT LT RT LT		1759+40.65 2+21.62 5+47.21 7+66.51	31.0 11.0 11.5 11.0	LT LT LT LT LT LT LT	92 14 18 95 14
AT IL 10 INTERSECTION	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05	31.0 7.1 7.0 6.6 7.6	LT LT RT LT RT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40	31.0 11.0 11.5 11.0 11.5	LT	92 14 18 95 14
AT IL 10 INTERSECTION	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93	31.0 7.1 7.0 6.6 7.6 55.0	LT LT RT LT RT LT	- - - -	1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46	31.0 11.0 11.5 11.0 11.5 55.0	LT	92 14 18 95 14 63 20
	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS SHOULDER DIAGONALS	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39	31.0 7.1 7.0 6.6 7.6 55.0 34.0	LT LT RT LT RT LT LT	- - -	1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40	31.0 11.0 11.5 11.0 11.5 55.0 34.0	LT L	92 14 18 95 14 63 20 26
NB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS SHOULDER DIAGONALS WHITE CENTERLINE	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5	LT LT RT LT RT LT LT LT LT LT	- - - - -	1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5	LT L	92 14 18 95 14 63 20 26
	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5	LT LT RT LT RT LT LT LT LT LT LT LT	- - - - -	1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5	LT L	92 14 18 95 14 63 20 26 50
NB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE YELLOW CENTERLINE SHOULDER DIAGONALS	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5	LT LT RT LT RT LT LT LT LT LT LT LT LT LT	- - - - -	1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5	LT L	92 14 18 95 14 63 20 26 50 96
NB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE WHITE CENTERLINE	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04 40+10.91	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5 23.0	LT LT RT LT RT LT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86 44+87.16	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5 22.0	LT	92 14 18 95 14 63 20 26 50 96 37
NB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE WHITE TURN LANE	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04 34+18.04 40+10.91 39+88.22	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5 23.0 36.0	LT LT RT LT RT LT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86 44+87.16 42+14.44	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5 22.0 36.0	LT	92 14 18 95 14 63 20 26 50 96 37 48
NB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE TURN LANE WHITE TURN LANE	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04 34+18.04 40+10.91 39+88.22 40+03.67	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5 23.0 36.0 6.1	LT LT RT LT RT LT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86 44+87.16 42+14.44 42+25.15	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5 22.0 36.0 11.0	LT	92 14 18 95 14 63 20 26 50 96 37 48 23
NB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION SB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE WHITE TURN LANE WHITE TURN LANE WHITE TURN LANE	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04 34+18.04 40+10.91 39+88.22 40+03.67 1758+04.88	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5 23.0 36.0 6.1 1.0	LT LT RT LT RT LT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86 44+87.16 42+14.44 42+25.15 1761+74.65	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5 22.0 36.0 11.0	LT	92 14 18 95 14 63 20 26 50 96 37 48 23 22
NB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE WHITE TURN LANE WHITE TURN LANE WHITE TURN LANE WHITE TURN LANE	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04 34+18.04 40+10.91 39+88.22 40+03.67 1758+04.88 1759+87.66	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5 23.0 36.0 6.1 1.0 13.0	LT LT RT LT RT LT LT LT LT LT LT LT LT LT RT RT RT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86 44+87.16 42+14.44 42+25.15 1761+74.65 1761+94.66	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5 22.0 36.0 11.0 1.0	LT	92 14 18 95 14 63 20 26 50 96 37 48 23 22
NB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION SB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE WHITE TURN LANE	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04 34+18.04 40+10.91 39+88.22 40+03.67 1758+04.88 1759+87.66 1760+46.08	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5 23.0 36.0 6.1 1.0 13.0	LT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86 44+87.16 42+14.44 42+25.15 1761+74.65 1761+94.66 1761+93.02	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5 22.0 36.0 11.0 1.0 13.0 14.3	LT	92 14 18 95 14 63 20 26 50 96 37 48 23 22 37 21
NB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION SB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE WHITE TURN LANE SHOULDER DIAGONALS	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04 34+18.04 40+10.91 39+88.22 40+03.67 1758+04.88 1759+87.66 1760+46.08 1758+05.90	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5 23.0 36.0 6.1 1.0 13.0 11.0	LT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86 44+87.16 42+14.44 42+25.15 1761+74.65 1761+94.66 1761+93.02 1760+97.78	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5 22.0 36.0 11.0 1.0 13.0 14.3 25.0	LT	92 14 18 95 14 63 20 26 50 96 37 48 23 22 37 21
NB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION SB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE WHITE TURN LANE SHOULDER DIAGONALS WHITE CENTERLINE	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04 34+18.04 40+10.91 39+88.22 40+03.67 1758+04.88 1759+87.66 1760+46.08 1758+05.90 0+35.41	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5 23.0 36.0 6.1 1.0 13.0 11.0 12.8 22.0	LT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86 44+87.16 42+14.44 42+25.15 1761+74.65 1761+94.66 1761+93.02 1760+97.78 9+63.40	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5 22.0 36.0 11.0 1.0 1.0 1.0 25.0 20.0	LT	92 14 18 95 14 63 20 26 50 96 37 48 23 22 37 21 15
NB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION SB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE YELLOW CENTERLINE WHITE CENTERLINE WHITE TURN LANE SHOULDER DIAGONALS WHITE CENTERLINE SHOULDER DIAGONALS	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04 34+18.04 40+10.91 39+88.22 40+03.67 1758+04.88 1759+87.66 1760+46.08 1758+05.90 0+35.41 1+33.22	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5 23.0 36.0 6.1 1.0 13.0 11.0 12.8 22.0 33.5	LT LT RT LT RT LT LT LT LT LT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86 44+87.16 42+14.44 42+25.15 1761+74.65 1761+94.66 1761+93.02 1760+97.78 9+63.40 5+72.07	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5 22.0 36.0 11.0 1.0 13.0 14.3 25.0 22.0 33.5	LT	92 14 18 95 14 63 20 26 50 96 37 48 23 22 37 21 15 23
NB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION SB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE YELLOW CENTERLINE WHITE CENTERLINE WHITE TURN LANE SHOULDER DIAGONALS WHITE CENTERLINE SHOULDER DIAGONALS SHOULDER DIAGONALS	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04 34+18.04 40+10.91 39+88.22 40+03.67 1758+04.88 1759+87.66 1760+46.08 1758+05.90 0+35.41 1+33.22 6+29.56	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5 23.0 36.0 6.1 1.0 13.0 11.0 12.8 22.0 33.5 33.5	LT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86 44+87.16 42+14.44 42+25.15 1761+74.65 1761+94.66 1761+93.02 1760+97.78 9+63.40 5+72.07 9+63.40	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5 22.0 36.0 11.0 1.0 13.0 14.3 25.0 22.0 33.5 33.5	LT	92 14 18 95 14 63 20 26 50 96 37 48 23 22 37 21 15 23 93
NB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION SB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE YELLOW CENTERLINE WHITE TURN LANE SHOULDER DIAGONALS WHITE CENTERLINE SHOULDER DIAGONALS SHOULDER DIAGONALS YELLOW CENTERLINE	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04 34+18.04 40+10.91 39+88.22 40+03.67 1758+04.88 1759+87.66 1760+46.08 1758+05.90 0+35.41 1+33.22 6+29.56 0+71.10	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5 23.0 36.0 6.1 1.0 12.8 22.0 33.5 33.5	LT LT RT LT RT LT LT LT LT LT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86 44+87.16 42+14.44 42+25.15 1761+74.65 1761+94.66 1761+93.02 1760+97.78 9+63.40 5+72.07 9+63.40 5+47.32	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5 22.0 36.0 11.0 1.0 13.0 14.3 25.0 22.0 33.5 7.0	LT	92 14 18 95 14 63 20 26 50 96 37 48 23 22 37 21 15 23 93 34 27
NB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION SB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE YELLOW CENTERLINE WHITE TURN LANE SHOULDER DIAGONALS WHITE SHOULDER DIAGONALS SHOULDER DIAGONALS YELLOW CENTERLINE WHITE TURN LANE	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04 40+10.91 39+88.22 40+03.67 1758+04.88 1759+87.66 1760+46.08 1758+05.90 0+35.41 1+33.22 6+29.56 0+71.10 4+18.06	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5 23.0 36.0 6.1 1.0 12.8 22.0 33.5 33.5 11.0 10.5	LT LT RT LT RT LT LT LT LT LT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86 44+87.16 42+14.44 42+25.15 1761+74.65 1761+94.66 1761+93.02 1760+97.78 9+63.40 5+72.07 9+63.40 5+47.32 5+83.74	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5 22.0 36.0 11.0 13.0 14.3 25.0 22.0 33.5 33.5 7.0 6.7	LT	92 14 18 95 14 63 20 26 50 96 37 48 23 22 37 21 15 23 93 34 27 95
NB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION SB I-55 BUSINESS/LINCOLN PARKWAY AT IL 10 INTERSECTION	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE WHITE TURN LANE SHOULDER DIAGONALS WHITE SHOULDER DIAGONALS WHITE CENTERLINE SHOULDER DIAGONALS SHOULDER DIAGONALS YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04 40+10.91 39+88.22 40+03.67 1758+04.88 1759+87.66 1760+46.08 1758+05.90 0+35.41 1+33.22 6+29.56 0+71.10 4+18.06 6+50.14	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5 23.0 36.0 6.1 1.0 12.8 22.0 33.5 33.5 11.0 10.5	LT LT RT LT RT LT LT LT LT LT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86 44+87.16 42+14.44 42+25.15 1761+74.65 1761+94.66 1761+93.02 1760+97.78 9+63.40 5+72.07 9+63.40 5+47.32 5+83.74 9+63.40	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5 22.0 36.0 11.0 13.0 14.3 25.0 22.0 33.5 33.5 7.0 6.7	LT	92 14 18 95 14 63 20 26 50 96 37 48 23 22 37 21 52 33 44 27 95
NB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION SB I-55 BUSINESS/LINCOLN PARKWAY AT IL 10 INTERSECTION	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE WHITE TURN LANE SHOULDER DIAGONALS WHITE CENTERLINE SHOULDER DIAGONALS SHOULDER DIAGONALS YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04 40+10.91 39+88.22 40+03.67 1758+04.88 1759+87.66 1760+46.08 1758+05.90 0+35.41 1+33.22 6+29.56 0+71.10 4+18.06 6+50.14 40+08.42	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5 23.0 36.0 6.1 1.0 12.8 22.0 33.5 33.5 11.0 10.5 22.0	LT LT RT LT RT LT LT LT LT LT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86 44+87.16 42+14.44 42+25.15 1761+74.65 1761+94.66 1761+93.02 1760+97.78 9+63.40 5+72.07 9+63.40 5+47.32 5+83.74 9+63.40 39+18.66	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5 22.0 36.0 11.0 13.0 14.3 25.0 22.0 33.5 33.5 7.0 6.7 10.5 22.0	LT	37 92 14 18 95 14 63 20 26 50 96 37 48 23 22 37 21 15 23 93 34 27 95
NB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION SB I-55 BUSINESS/LINCOLN PARKWAY AT IL 10 INTERSECTION	WHITE TURN LANE WHITE TURN LANE YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE YELLOW CENTERLINE SHOULDER DIAGONALS WHITE CENTERLINE WHITE TURN LANE SHOULDER DIAGONALS WHITE SHOULDER DIAGONALS WHITE CENTERLINE SHOULDER DIAGONALS SHOULDER DIAGONALS YELLOW CENTERLINE WHITE TURN LANE YELLOW CENTERLINE	1758+05.82 0+38.89 0+71.02 6+22.44 6+50.05 1758+05.93 6+32.39 34+18.04 34+18.04 40+10.91 39+88.22 40+03.67 1758+04.88 1759+87.66 1760+46.08 1758+05.90 0+35.41 1+33.22 6+29.56 0+71.10 4+18.06 6+50.14	31.0 7.1 7.0 6.6 7.6 55.0 34.0 22.5 11.5 33.5 23.0 36.0 6.1 1.0 12.8 22.0 33.5 33.5 11.0 10.5	LT LT RT LT RT LT LT LT LT LT		1759+40.65 2+21.62 5+47.21 7+66.51 9+63.40 1760+58.46 9+63.40 39+18.66 38+95.49 38+82.86 44+87.16 42+14.44 42+25.15 1761+74.65 1761+94.66 1761+93.02 1760+97.78 9+63.40 5+72.07 9+63.40 5+47.32 5+83.74 9+63.40	31.0 11.0 11.5 11.0 11.5 55.0 34.0 22.5 11.5 33.5 22.0 36.0 11.0 13.0 14.3 25.0 22.0 33.5 33.5 7.0 6.7	LT	92 14 18 95 14 63 20 26 50 96 37 48 23 22 37 21 15 23 93 34 27 95

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
pw:\\IL084EBIDINTEG.:1ll:no:s.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67	PE DR'AUM Data\€ADsheets\D672E02-sht-05-Sc	- REVISED -
	PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -

STATE O	F ILLINOIS
DEPARTMENT OF	TRANSPORTATION

SCHEDULI	OF QU	ANTITIES		F.A.U. RTE. 7706	SECTION •	LOGAN	TOTAL SHEETS 343	61
						CONTRACT	NO. 7	'2E02
OF	SHEETS	STA.	TO STA.		ILLINOIS FED. A	ID PROJECT		
			• (22)F	S-6. MED	& (23) RS-5			

	70300100 - SHC	ORT TERM PAVEM	ENT MARKI	NG - CONT	INUED				
EB IL 10/121/WOODLAWN DRIVE	WHITE CENTERLINE	133+66.35	23.0	RT	-	137+39.66	23.0	RT	37
	WHITE TURN LANE	135+34.84	34.0	RT	-	137+53.22	35.0	RT	22
	WHITE TURN LANE	135+73.27	11.0	RT	-	137+48.93	7.1	RT	18
	WHITE CENTERLINE	138+34.01	23.0	RT	1	143+90.84	17.2	RT	56
	YELLOW CENTERLINE	138+52.87	8.8	RT	-	138+86.13	9.2	RT	7
	YELLOW CENTERLINE	142+19.97	3.7	LT	-	143+90.72	6.4	LT	34
	WHITE TURN LANE	142+75.41	6.3	RT	-	143+90.84	6.3	RT	12
WB IL 10/121/WOODLAWN DRIVE	WHITE CENTERLINE	133+66.35	23.2	LT	-	137+36.34	23.0	LT	37
	WHITE TURN LANE	133+66.35	11.2	LT	-	134+63.24	11.1	LT	10
	WHITE CENTERLINE	138+34.02	22.4	LT	-	143+90.84	17.3	LT	56
	WHITE TURN LANE	138+24.93	6.9	LT	-	140+99.456	9.8	LT	27
	WHITE TURN LANE	138+20.06	34.4	LT	-	140+76.58	34.4	LT	26
5TH STREET	YELLOW CENTERLINE	12+41.03	6.0	LT	-	12+90.28	6.0	LT	10
	WHITE TURN LANE	12+41.03	6.0	RT	-	13+16.16	6.0	RT	8
	YELLOW CENTERLINE	14+33.99	6.0	RT	-	15+20.66	6.5	RT	17
	WHITE TURN LANE	14+19.18	6.0	LT	-	15+20.66	6.0	LT	10
	WHITE TURN LANE	14+19.18	18.0	LT	-	15+20.66	18.0	LT	10
		•			•	PPS #6-00	477-0000 (H	SIP) TOTAL =	1646
STP AND HSIP COMBINED TOTAL =								NED TOTAL =	4233

		PPS# 6-00625-	MENT MARI		-				
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	SQ I
NB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	9+63.40	22.5	LT	-	34+18.04	22.5	LT	81
	SHOULDER DIAGONAL	9+63.40	33.5	LT	- 1	34+18.04	33.5	LT	65
	YELLOW CENTERLINE	15+20.75	11.5	LT	- 1	19+60.75	11.5	LT	29
	TURN LANE	20+09.80	7.3	LT	-	21+99.62	11.5	LT	6
	YELLOW CENTERLINE	20+49.34	7.0	RT	- 1	24+68.19	11.5	LT	28
SB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	9+63.40	22.0	RT	-	34+18.04	22.0	RT	81
·	SHOULDER DIAGONAL	9+63.40	33.5	RT	- 1	15+10.08	33.5	RT	14
	YELLOW CENTERLINE	15+20.75	10.5	RT	-	19+60.75	10.5	RT	29
	TURN LANE	16+58.26	335.5	RT	-	19+48379	33.5	RT	10
	YELLOW CENTERLINE	20+49.34	10.5	RT	-	24+68.19	10.5	RT	28
	SHOULDER DIAGONAL	21+75.76	33.5	RT	-	24+68.18	33.5	RT	8
NB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	44+87.15	22.0	LT	- 1	74+00.00	22.0	LT	96
	SHOULDER DIAGONAL	44+87.15	34.0	LT	- 1	74+00.00	34.0	LT	77
	YELLOW CENTERLINE	63+72.32	10.0	LT	-	68+13.03	10.0	LT	29
	YELLOW CENTERLINE	69+18.56	10.0	LT	-	72+19.16	10.0	LT	20
	TURN LANE	73+14.08	9.0	LT	-	74+49.51	9.0	LT	4
SB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	44+87.15	22.0	RT	-	74+00.00	22.0	RT	96
	SHOULDER DIAGONAL	44+87.15	34.0	RT	-	74+00.00	34.0	RT	77
	YELLOW CENTERLINE	63+72.32	10.0	RT	-	68+13.03	1.5	LT	29
	TURN LANE	66+02.25	10.5	RT	-	68+17.25	10.5	RT	7
	YELLOW CENTERLINE	69+56.39	10.5	RT	-	72+18.35	1.5	LT	17
	TURN LANE	71+07.82	10.5	RT	-	72+22.82	10.5	RT	4
	YELLOW CENTERLINE	73+14.08	6.0	RT	-	74+49.51	6.0	RT	9
STALHUT DRIVE	YELLOW CENTERLINE	38+70.37	0.0	RT	-	39+61.58	0.0	RT	6
5TH STREET	YELLOW CENTERLINE	15+75.08	5.0	RT	-	16+15.08	5.0	RT	3
	TURN LANE	15+75.08	6.0	LT	-	16+15.08	6.0	LT	1
						PPS# 6-0	0625-0000 (S	TP) TOTAL =	854
		DDC# C 00477	2000 (HCIE)						
ROADWAY	DESCRIPTION	PPS# 6-00477-	OFFSET	SIDE	то	STATION	OFFSET	SIDE	SQ F
NB I-55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	1758+05.90	43.0	LT	- 10	1761+79.62	43.0	LT	12
AT IL 10 INTERSETION	WHITE CENTERLINE WHITE CENTERLINE	0+45.28	23.0	LT	+	9+63.40	22.5	LT	
AT IL 10 INTERSETION		1758+05.82	31.0	LT	-	9+63.40 1759+40.65		LT	30 4
	WHITE TURN LANE		7.1	LT	+		31.0	LT	6
	WHITE TURN LANE	0+38.89 0+71.02	7.1	RT	-	2+21.62	11.0	LT	
	YELLOW CENTERLINE WHITE TURN LANE	0+71.02 6+22.44	6.6	LT	-	5+47.21 7+66.51	11.5 11.0	LT	31 5
	WHITE TURN LANE	CONTINUED ON			-	7+00.51	11.0	LI	5

PLOT SCALE = 40.0000 '/ in. CHECKED - DEPARTMENT OF TRANSPORTATION CONTRACT	FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -								F.A.U.	SECTION	COUNTY	TOTAL SHEE
	pw:\\ILØ84EBIDINTEG.:ll:nois.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67	2EØRXWAN Data\€ADsheets\D672E02-sht-05-So	oneREWISED -	STATE OF ILLINOIS		5	SCHEDULI	E OF QUA	ANTITIES		7706	•	LOGAN	343 62
		PLOT SCALE = 40.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION									CONTRACT	NO. 72EO
	Default	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FED.		

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION			SCHEDULE	OF QUA	ANTITIES	
	SCALE:	SHEET	OF	SHEETS	STA.	ТО

	70300150 - SHORT TE	RM PAVEMENT	MARKING RI	MOVAL - (CONTIN	IUED			
	YELLOW CENTERLINE	6+50.05	7.6	RT	-	9+63.40	11.5	LT	21
	SHOULDER DIAGONALS	1758+05.93	55.0	LT	-	1760+58.46	55.0	LT	7
	SHOULDER DIAGONALS	6+32.39	34.0	LT	-	9+63.40	34.0	LT	9
NB I-55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	34+18.04	22.5	LT	-	39+18.66	22.5	LT	17
AT 5TH STREET INTERSECTION	YELLOW CENTERLINE	34+18.04	11.5	LT	-	38+95.49	11.5	LT	32
	SHOULDER DIAGONALS	34+18.04	33.5	LT		38+82.86	33.5	LT	12
	WHITE CENTERLINE	40+10.91	23.0	LT	-	44+87.16	22.0	LT	16
	WHITE TURN LANE	39+88.22	36.0	LT	-	42+14.44	36.0	LT	7
	WHITE TURN LANE	40+03.67	6.1	LT	-	42+25.15	11.0	LT	7
SB I-55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	1758+04.88	1.0	RT	-	1761+74.65	1.0	RT	12
AT IL 10 INTERSECTION	WHITE TURN LANE	1759+87.66	13.0	RT	-	1761+94.66	13.0	RT	7
	WHITE TURN LANE	1760+46.08	11.0	LT	-	1761+93.02	14.3	LT	5
	SHOULDER DIAGONALS	1758+05.90	12.8	RT		1760+97.78	25.0	RT	8
	WHITE CENTERLINE	0+35.41	22.0	RT	-	9+63.40	22.0	RT	31
	SHOULDER DIAGONALS	1+33.22	33.5	RT		5+72.07	33.5	RT	11
	SHOULDER DIAGONALS	6+29.56	33.5	RT		9+63.40	33.5	RT	9
	YELLOW CENTERLINE	0+71.10	11.0	RT	-	5+47.32	7.0	LT	31
	WHITE TURN LANE	4+18.06	10.5	RT	-	5+83.74	6.7	RT	5
	YELLOW CENTERLINE	6+50.14	10.5	RT	-	9+63.40	10.5	RT	21
SB I-55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	40+08.42	22.0	RT	-	39+18.66	22.0	RT	17
AT 5TH STREET INTERSECTION	SHOULDER DIAGONALS	41+06.57	33.5	LT		39+18.66	33.5	LT	10
	YELLOW CENTERLINE	40+32.54	10.5	RT	-	44+87.15	10.5	RT	30
EB IL 10/121/WOODLAWN DRIVE	WHITE CENTERLINE	133+66.35	23.0	RT	-	137+39.66	23.0	RT	12
	WHITE TURN LANE	135+34.84	34.0	RT	-	137+53.22	35.0	RT	7
	WHITE TURN LANE	135+73.27	11.0	RT	-	137+48.93	7.1	RT	6
	WHITE CENTERLINE	138+34.01	23.0	RT	-	143+90.84	17.2	RT	18
	YELLOW CENTERLINE	138+52.87	8.8	RT	-	138+86.13	9.2	RT	2
	YELLOW CENTERLINE	142+19.97	3.7	LT	-	143+90.72	6.4	LT	11
	WHITE TURN LANE	142+75.41	6.3	RT	-	143+90.84	6.3	RT	4
WB IL 10/121/WOODLAWN DRIVE	WHITE CENTERLINE	133+66.35	23.2	LT	-	137+36.34	23.0	LT	12
	WHITE TURN LANE	133+66.35	11.2	LT	-	134+63.24	11.1	LT	3
	WHITE CENTERLINE	138+34.02	22.4	LT	-	143+90.84	17.3	LT	18
	WHITE TURN LANE	138+24.93	6.9	LT	-	140+99.456	9.8	LT	9
	WHITE TURN LANE	138+20.06	34.4	LT	-	140+76.58	34.4	LT	8
5TH STREET	YELLOW CENTERLINE	12+41.03	6.0	LT	-	12+90.28	6.0	LT	3
	WHITE TURN LANE	12+41.03	6.0	RT	-	13+16.16	6.0	RT	2
	YELLOW CENTERLINE	14+33.99	6.0	RT	-	15+20.66	6.5	RT	6
	WHITE TURN LANE	14+19.18	6.0	LT	-	15+20.66	6.0	LT	3
	WHITE TURN LANE	14+19.18	18.0	LT	-	15+20.66	18.0	LT	3
		ı					477-0000 (HS		543
							HSIP COMBIN		1397

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
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	PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -

STATE OF ILLINOIS RTMENT OF TRANSPORTATION		SCH
	SCALE:	SHEET

					F.A.U. RTE.	SEC ⁻	ΓΙΟΝ	COUNTY	TOTAL SHEETS	SHEET NO.
CHEDUL	E OF QUA	ANTITIES			7706	•	•	LOGAN	343	63
								CONTRACT	NO. 7	2E02
OF	SHEETS	STA.	TO STA.				ILLINOIS FED. AI	D PROJECT		
				• (22)RS-6	6, MED	& (23) RS-5				

	PPS# 6-00625-0000 (STP)				
	· ·		STAGE 1		
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	SQ F
SB I-55 BUSINESS/LINCOLN PARKWAY	RIGHT TURN ARROW	17+48.60	31.0	RT	15.6
	RIGHT TURN ARROW	18+29.43	31.0	RT	15.6
	PPS	#6-00625-0000 (S	ΓΡ) STAGE 1	TOTAL =	31.2
			STAGE 2		
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	FOOT
NB I-55 BUSINESS/LINCOLN PARKWAY	LEFT TURN ARROW	20+35.68	17.0	LT	15.6
	LEFT TURN ARROW	21+35.68	17.0	LT	15.6
	PPS	#6-00625-0000 (S			31.2
			STAGE 3		
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	SQ F1
SB I-55 BUSINESS/LINCOLN PARKWAY	RIGHT TURN ARROW	17+48.60	31.0	RT	15.6
	RIGHT TURN ARROW	18+29.43	31.0	RT	15.6
	PPS	#6-00625-0000 (S	TP) STAGE 3	TOTAL =	31.2
70300210 - TEMPO	RARY PAVEMENT MARKING LE	TTERS AND SYMI	BOLS		
7,0500=10 1,511111 0	PPS# 6-00477-0000 (HSIP)				
	• •		STAGE 1		
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	SQ FT
NB I-55 BUSINESS/LINCOLN PARKWAY	RIGHT TURN ARROW	1+82.91	31.5	LT	15.6
	RIGHT TURN ARROW	2+82.89	31.5	LT	15.6
	RIGHT TURN ARROW	40+61.41	31.0	RT	15.6
	RIGHT TURN ARROW	41+61.32	31.0	RT	15.6
NB I-55 BUSINESS/LINCOLN PARKWAY	RIGHT TURN ARROW	36+95.96	31.0	LT	15.6
	RIGHT TURN ARROW	37+98.45	31.0	LT	15.6
	PPS	#6-00477-0000 (HS	IP) STAGE 1	TOTAL =	93.6
			STAGE 2		
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	F001
NB I-55 BUSINESS/LINCOLN PARKWAY	LEFT TURN ARROW	1758+09.88	34.0	LT	15.6
	LEFT TURN ARROW	1758+98.63	34.0	LT	15.6
	LEFT TURN ARROW	0+61.29	15.0	LT	15.6
	LEFT TURN ARROW	1+61.29	15.0	LT	15.6
	LEFT TURN ARROW	6+32.78	17.0	RT	15.6
	LEFT TURN ARROW	7+09.77	17.0	RT	15.6
	LEFT TURN ARROW	40+35.74	13.0	RT	15.6
CD LEE BUGINESS (CONCESSION TO THE CONCESSION TO	LEFT TURN ARROW	41+35.74	13.0	RT	15.6
SB I-55 BUSINESS/LINCOLN PARKWAY	LEFT TURN ARROW	1760+66.14	8.6	RT	15.6
	LEFT TURN ARROW	1761+59.56	8.6	RT	15.6
	LEFT TURN ARROW	4+57.05	16.5	RT	15.6
	LEFT TURN ARROW	5+59.89	16.5	RT	15.6
	LEFT TURN ARROW	37+79.15	14.5	RT	15.6
ED II 10/131/MOOD! AVAIN AVENUE	LEFT TURN ARROW	38+82.13	14.5	RT	15.6
EB IL 10/121/WOODLAWN AVENUE	LEFT TURN ARROW	135+90.58	14.4	RT	15.6
ER II 10/121/WOODI AWNI AVENUE	LEFT TURN ARROW	137+22.15	14.4	RT	15.6
EB IL 10/121/WOODLAWN AVENUE	LEFT TURN ARROW	133+71.87	14.0	LT	15.6
	LEFT TURN ARROW	134+46.58	14.0	LT	15.6
	LEFT TURN ARROW	138+61.99 140+41.33	14.8	LT LT	15.6
	LEFT TURN ARROW PPS	#6-00477-0000 (HS	14.8 P) STAGE 2		15.6 312. 0
	113	1 22 27 2300 (113	STAGE 3		
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	SQ F1
NB I-55 BUSINESS/LINCOLN PARKWAY	RIGHT TURN ARROW	1+82.91	31.5	LT	15.6
	RIGHT TURN ARROW	2+82.89	31.5	LT	15.6
	RIGHT TURN ARROW	40+61.41	31.0	RT	15.6
	RIGHT TURN ARROW	41+61.32	31.0	RT	15.6
NB I-55 BUSINESS/LINCOLN PARKWAY	RIGHT TURN ARROW	36+95.96	31.0	LT	15.6
IND 1-33 BUSHNESS/ LINCULIN FARRWAT					
NB 1-33 BOSINESS/ LINCOLIN PARKWAT	RIGHT TURN ARROW	37+98.45	31.0	LT	15.6

70300220 - TEMPORARY PAVI	MENT MARKING - LINE 4"	
PPS# 6-00625-	0000 (STP)	
STAGE 1		2800
STAGE 2		15343
STAGE 3		5000
PPS	# 6-00625-0000 (STP) TOTAL =	23143
PPS# 6-00477-0	0000 (HSIP)	
STAGE 1 TOTAL =	STAGE 1	2600
STAGE 2 TOTAL =	STAGE 2	6307
STAGE 3 TOTAL =	STAGE 3	5200
PPS	6-00477-0000 (HSIP) TOTAL =	14107
STP	AND HSIP COMBINED TOTAL =	37250

X0327979 - PAVEMENT MARKING REMOVAL - GRINDING	
PPS# 6-00625-0000 (STP)	SQ FT
STAGE 1	378
STAGE 2	724
STAGE 3	3905
STAGE 4	1450
PPS# 6-00625-0000 (STP) TOTAL =	6457
PPS# 6-00477-0000 (HSIP)	
STAGE 1	172
STAGE 2	688
STAGE 3	2060
STAGE 4	1516
PPS# 6-00477-0000 (HSIP) TOTAL =	2920
STP AND HSIP COMBINED TOTAL =	9377

SQ FT
84
262
1221
262
1829
84
388
457
388
929
2758

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	T	<u> </u>						F.A.U. RTE.	SECTION	COUNTY TOTAL SHEET
pw:\\ILØ84EBIDINTEG.:111:no:s.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67.	PE DRANAN Data\€ADsheets\D672E02-sht-05	i-ScheREWeiseGh -	STATE OF ILLINOIS	SCHEDULE OF QUANTITIES					7706	•	LOGAN 343 64	
	PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION					ı				CONTRACT NO. 72E02
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA.				TO STA.			. AID PROJECT	
										• (22)R	RS-6, MED & (2	3) K2-2	

	78000650 THERM			ARKING	- LINE 2	24"			
		PPS# 6-00625-							
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOOT
SB 155 BUSINESS/LINCOLN PARKWAY	STOP BAR	19+28.99	8.7	RT	-	19+28.99	34.0	RT	24
	RR WARNING SIGN	44+59.14	10.0	RT	-	44+59.14	34.0	RT	24
	RR WARNING SIGN	45+09.00	10.0	RT	-	45+09.00	34.0	RT	24
NB 155 BUSINESS/LINCOLN PARKWAY	STOP BAR	20+13.95	11.0	LT	-	20+13.95	34.0	LT	24
	STOP BAR	20+50.94	7.2	RT	-	20+50.40	4.8	LT	12
	RR WARNING SIGN	51+09.00	11.0	LT	-	51+09.00	34.0	LT	24
	RR WARNING SIGN	51+58.86	11.0	LT	-	51+58.86	34.0	LT	24
STALHUT DRIVE	STOP BAR	39+34.14	52.2	RT	-	39+46.97	39.5	RT	18
	STOP BAR	39+52.39	5.5	RT	-	39+52.63	18.0	RT	12
	STOP BAR	39+23.34	53.9	LT	-	39+36.10	41.2	LT	18
5TH STREET	STOP BAR	15+15.34	49.1	LT	-	15+35.00	45.9	LT	21
	STOP BAR	15+43.85	43.5	RT	-	15+58.34	43.4	RT	15
STRINGER RD	STOP BAR	21+17.87	58.7	RT	ı	21+35.74	56.6	RT	18
	STOP BAR	21+61.66	33.4	RT	ı	21+66.92	20.4	RT	14
						PPS# 6-00625	-0000 (STP)	TOTAL =	272
	78000650 - THERN	OPLASTIC PA	VEMENT M	ARKING	- LINE	24"			
		PPS# 6-00477-0	OUUU (MEIB)						
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOOT
SB I55 BUSINESS/LINCOLN PARKWAY	STOP BAR	17+61.91	27.3	LT	-	1761+64.29	15.9	LT	12
3B 133 BOSINESS/EINCOLN I ARRIVAT	STOP BAR	1761+74.65	11.0	LT		1761+74.65	13.0	RT	24
	STOP BAR	1761+64.25	30.0	RT		1761+53.15	44.1	RT	18
	STOP BAR	38+94.23	10.0	LT	_	38+94.66	2.0	RT	12
	STOP BAR	39+18.66	10.5	RT	-	39+18.66	35.0	RT	25
	STOP BAR	39+13.57	83.7	RT		39+27.35	72.3	RT	18
NB I55 BUSINESS/LINCOLN PARKWAY	STOP BAR	0+49.20	66.9	LT		0+62.95	78.1	LT	18
NB 133 BOSINESS/EINCOLN FARRWAT	STOP BAR	0+45.29	11.0	LT		0+02.93	35.0	LT	24
	STOP BAR	0+43.29	6.9	LT		0+43.29	4.5	LT	12
	STOP BAR	39+95.95	55.8	LT		40+10.15	66.9	LT	18
			11.0	LT	-	40+10.13	36.0	LT	24
	STOP BAR	40+10.91	9.4	RT		40+10.91	2.6	LT	12
ED II 10/131/MOODI AMAN BOAD	STOP BAR	40+33.63 137+22.24	7.1	LT		137+22.76	4.3	RT	12
EB IL10/121/WOODLAWN ROAD	STOP BAR								
	STOP BAR	137+39.66	11.0	RT	-	137+39.65	35.1	RT	24
NAME AND	STOP BAR	137+20.31	72.5	RT	-	137+33.37	60.2	RT	18
	STOP BAR	138+54.37	7.4	RT	-	138+51.10	4.1	LT	12
WBIL10/121/WOODLAWN ROAD	CTOD DAD	430.34.03		l LT l	-	138+34.02	34.4	LT	24
WBIL10/121/WOODLAWN ROAD	STOP BAR	138+34.02	10.4					1 1	
	STOP BAR	138+48.60	50.1	LT	-	138+59.58	64.3	LT	18
SIDE ROAD (EAST)	STOP BAR STOP BAR	138+48.60 5+71.14	50.1 48.8	LT LT	-	138+59.58 5+99.64	64.3 48.2	LT	29
SIDE ROAD (EAST) SIDE ROAD (WEST)	STOP BAR STOP BAR STOP BAR	138+48.60 5+71.14 6+02.47	50.1 48.8 50.1	LT LT RT		138+59.58 5+99.64 6+25.25	64.3 48.2 50.2	LT RT	29 24
SIDE ROAD (EAST)	STOP BAR STOP BAR STOP BAR STOP BAR	138+48.60 5+71.14 6+02.47 12+89.23	50.1 48.8 50.1 5.5	LT LT RT LT	-	138+59.58 5+99.64 6+25.25 12+89.38	64.3 48.2 50.2 6.0	LT RT RT	29 24 12
SIDE ROAD (EAST) SIDE ROAD (WEST)	STOP BAR STOP BAR STOP BAR STOP BAR STOP BAR	138+48.60 5+71.14 6+02.47 12+89.23 13+10.19	50.1 48.8 50.1 5.5 46.0	LT LT RT LT RT		138+59.58 5+99.64 6+25.25 12+89.38 12+96.15	64.3 48.2 50.2 6.0 57.3	LT RT RT RT	29 24 12 18
SIDE ROAD (EAST) SIDE ROAD (WEST)	STOP BAR STOP BAR STOP BAR STOP BAR STOP BAR STOP BAR	138+48.60 5+71.14 6+02.47 12+89.23 13+10.19 13+16.16	50.1 48.8 50.1 5.5 46.0 6.0	LT LT RT LT RT RT	-	138+59.58 5+99.64 6+25.25 12+89.38 12+96.15 13+16.16	64.3 48.2 50.2 6.0 57.3 17.8	LT RT RT RT RT	29 24 12 18 12
SIDE ROAD (EAST) SIDE ROAD (WEST)	STOP BAR	138+48.60 5+71.14 6+02.47 12+89.23 13+10.19 13+16.16 14+19.18	50.1 48.8 50.1 5.5 46.0 6.0	LT LT RT LT RT LT RT LT RT		138+59.58 5+99.64 6+25.25 12+89.38 12+96.15 13+16.16 14+19.18	64.3 48.2 50.2 6.0 57.3 17.8 38.5	LT RT RT RT RT LT	29 24 12 18 12 34
SIDE ROAD (EAST) SIDE ROAD (WEST)	STOP BAR STOP BAR STOP BAR STOP BAR STOP BAR STOP BAR	138+48.60 5+71.14 6+02.47 12+89.23 13+10.19 13+16.16	50.1 48.8 50.1 5.5 46.0 6.0	LT LT RT LT RT RT		138+59.58 5+99.64 6+25.25 12+89.38 12+96.15 13+16.16	64.3 48.2 50.2 6.0 57.3 17.8 38.5 6.0	LT RT RT RT RT LT LT	29 24 12 18 12

	PPS# 6-00625-0000 (STP)			
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	SQ FT
SB 155 BUSINESS/LINCOLN PARKWAY	RIGHT TURN ARROW	16+65.52	44.7	RT	15.6
	RIGHT TURN ARROW	17+73.94	44.9	RT	15.6
	RIGHT TURN ARROW	18+77.01	44.6	RT	15.6
	RR WARNING R	44+85.05	12.8	RT	3.6
	RR WARNING R	44+85.05	19.3	RT	3.6
	RR WARNING R	44+85.05	24.6	RT	3.6
	RR WARNING R	44+85.05	31.3	RT	3.6
	RR WARNING X	44+85.05	16.1	RT	54.0
	RR WARNING X	44+85.05	27.9	RT	54.0
	LEFT TURN ARROW	66+08.50	1.5	RT	15.6
	LEFT TURN ARROW	67+07.78	1.5	RT	15.6
	LEFT TURN ARROW	67+96.52	1.5	RT	15.6
	LEFT TURN ARROW	71+14.38	2.0	RT	15.6
ID 155 DUGUNEOU / 11 / 2 - 2 - 2 - 2 - 2 - 2 - 2 - 2 - 2 - 2	LEFT TURN ARROW	72+04.76	2.0	RT	
NB 155 BUSINESS/LINCOLN PARKWAY	LEFT TURN ARROW	20+72.61	2.1	RT	
	LEFT TURN ARROW	21+39.17	0.8	LT	
	LEFT TURN ARROW	21+94.89	3.4	LT	
	RR WARNING R	51+32.93	13.1	LT	
_	RR WARNING R	51+32.93	19.6	LT	
	RR WARNING R	51+32.93	24.3		
	RR WARNING R RR WARNING X	51+32.93 51+32.93	30.8		
	RR WARNING X	51+32.93			
	LEFT TURN ARROW	73+34.11			
	LEFT TURN ARROW	74+21.25			
5TH STREET	LEFT TURN ARROW	15+78.89	2.4		
JIII JINEET	LEFT TURN ARROW	16+10.68	2.4		
	RIGHT TURN /THRU ARROW	15+82.09	18.0		
STRINGER AVENUE	RIGHT TURN ARROW	68+08.11	77.2		
3111113211711102	LEFT TURN ARROW	68+40.41	83.1	LT	
		1			15.6 15.6 15.6 3.6 3.6 3.6 3.6 54.0 54.0 15.6 15.6 15.6 15.6 15.6 15.6 15.6 15.6
				0.8 LT 6.5 LT 7.6 LT 7.6 LT 0.0 LT 0.0 LT 2.4 RT 2.1 RT 8.0 LT 7.2 LT 3.1 LT 00000 (STP) TOTAL =	
	PPS# 6-00477-0000 (F				
ROADWAY	DESCRIPTION	STATION	OFFSET		SQ FT
NB 155 BUSINESS/LINCOLN PARKWAY	LEFT TURN ARROW	1758+12.82	23.0		
AT IL 10 INTERSECTION	LEFT TURN ARROW	1759+31.94	23.0		
AT IL 10 INTERSECTION			100	1 17 1	1
	LEFT TURN ARROW	1760+48.12			
	LEFT TURN ARROW	1761+38.69	23.0	LT	15.6
	LEFT TURN ARROW LEFT TURN ARROW	1761+38.69 1+96.72	23.0 2.0	LT LT	15.6 15.6
	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW	1761+38.69 1+96.72 0+97.11	23.0 2.0 45.8	LT LT LT	15.6 15.6 15.6
	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW	1761+38.69 1+96.72 0+97.11 1+97.31	23.0 2.0 45.8 42.7	LT LT LT	15.6 15.6 15.6 15.6
	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW	1761+38.69 1+96.72 0+97.11 1+97.31 2+97.31	23.0 2.0 45.8 42.7 42.7	LT LT LT LT LT LT	15.6 15.6 15.6 15.6 15.6
	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW LEFT TURN ARROW	1761+38.69 1+96.72 0+97.11 1+97.31 2+97.31 5+46.41	23.0 2.0 45.8 42.7 42.7 4.5	LT	15.6 15.6 15.6 15.6 15.6 15.6
	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW	1761+38.69 1+96.72 0+97.11 1+97.31 2+97.31 5+46.41 7+51.43	23.0 2.0 45.8 42.7 42.7 4.5 1.3	LT L	15.6 15.6 15.6 15.6 15.6 15.6 15.6
•	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW	1761+38.69 1+96.72 0+97.11 1+97.31 2+97.31 5+46.41 7+51.43 37+94.47	23.0 2.0 45.8 42.7 42.7 4.5 1.3	LT L	15.6 15.6 15.6 15.6 15.6 15.6 15.6
NB 155 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW LEFT TURN ARROW	1761+38.69 1+96.72 0+97.11 1+97.31 2+97.31 5+46.41 7+51.43 37+94.47 38+69.40	23.0 2.0 45.8 42.7 42.7 4.5 1.3 1.9 5.1	LT L	15.6 15.6 15.6 15.6 15.6 15.6 15.6 15.6
•	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW	1761+38.69 1+96.72 0+97.11 1+97.31 2+97.31 5+46.41 7+51.43 37+94.47 38+69.40 40+26.66	23.0 2.0 45.8 42.7 42.7 4.5 1.3 1.9 5.1 46.6	LT L	15.6 15.6 15.6 15.6 15.6 15.6 15.6 15.6
•	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW	1761+38.69 1+96.72 0+97.11 1+97.31 2+97.31 5+46.41 7+51.43 37+94.47 38+69.40 40+26.66 41+15.19	23.0 2.0 45.8 42.7 42.7 4.5 1.3 1.9 5.1 46.6 44.3	LT L	15.6 15.6 15.6 15.6 15.6 15.6 15.6 15.6
•	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW	1761+38.69 1+96.72 0+97.11 1+97.31 2+97.31 5+46.41 7+51.43 37+94.47 38+69.40 40+26.66 41+15.19 42+08.27	23.0 2.0 45.8 42.7 42.7 4.5 1.3 1.9 5.1 46.6 44.3 44.4	LT L	15.6 15.6 15.6 15.6 15.6 15.6 15.6 15.6
AT 5TH STREET INTERSECTION	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW LEFT TURN ARROW	1761+38.69 1+96.72 0+97.11 1+97.31 2+97.31 5+46.41 7+51.43 37+94.47 38+69.40 40+26.66 41+15.19 42+08.27 42+08.30	23.0 2.0 45.8 42.7 42.7 4.5 1.3 1.9 5.1 46.6 44.3 44.4 2.1	LT L	15.6 15.6 15.6 15.6 15.6 15.6 15.6 15.6
AT 5TH STREET INTERSECTION SB 155 BUSINESS/LINCOLN PARKWAY	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW	1761+38.69 1+96.72 0+97.11 1+97.31 2+97.31 5+46.41 7+51.43 37+94.47 38+69.40 40+26.66 41+15.19 42+08.27 42+08.30 1759+92.46	23.0 2.0 45.8 42.7 42.7 4.5 1.3 1.9 5.1 46.6 44.3 44.4 2.1 21.0	LT	15.6 15.6 15.6 15.6 15.6 15.6 15.6 15.6
AT 5TH STREET INTERSECTION	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW	1761+38.69 1+96.72 0+97.11 1+97.31 2+97.31 5+46.41 7+51.43 37+94.47 38+69.40 40+26.66 41+15.19 42+08.27 42+08.30 1759+92.46 1760+64.39	23.0 2.0 45.8 42.7 42.7 4.5 1.3 1.9 5.1 46.6 44.3 44.4 2.1 21.0 22.0	LT	15.6 15.6 15.6 15.6 15.6 15.6 15.6 15.6
AT 5TH STREET INTERSECTION SB 155 BUSINESS/LINCOLN PARKWAY	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW	1761+38.69 1+96.72 0+97.11 1+97.31 2+97.31 5+46.41 7+51.43 37+94.47 38+69.40 40+26.66 41+15.19 42+08.27 42+08.30 1759+92.46 1760+64.39 1761+34.64	23.0 2.0 45.8 42.7 42.7 4.5 1.3 1.9 5.1 46.6 44.3 44.4 2.1 21.0 22.0 25.0	LT	15.6 15.6 15.6 15.6 15.6 15.6 15.6 15.6
AT 5TH STREET INTERSECTION BB 155 BUSINESS/LINCOLN PARKWAY	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW	1761+38.69 1+96.72 0+97.11 1+97.31 2+97.31 5+46.41 7+51.43 37+94.47 38+69.40 40+26.66 41+15.19 42+08.27 42+08.30 1759+92.46 1760+64.39 1761+34.64 0+96.84	23.0 2.0 45.8 42.7 42.7 4.5 1.3 1.9 5.1 46.6 44.3 44.4 2.1 21.0 22.0 25.0 2.3	LT	15.6 15.6 15.6 15.6 15.6 15.6 15.6 15.6
5B 155 BUSINESS/LINCOLN PARKWAY	LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW RIGHT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW LEFT TURN ARROW RIGHT TURN ARROW	1761+38.69 1+96.72 0+97.11 1+97.31 2+97.31 5+46.41 7+51.43 37+94.47 38+69.40 40+26.66 41+15.19 42+08.27 42+08.30 1759+92.46 1760+64.39 1761+34.64	23.0 2.0 45.8 42.7 42.7 4.5 1.3 1.9 5.1 46.6 44.3 44.4 2.1 21.0 22.0 25.0	LT	15.6 15.6 15.6 15.6 15.6 15.6 15.6 15.6

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -		SCHEDULE OF QUANTITIES						F.A.U.	SECTION	COUNTY	TOTAL SHEET
pw:\\IL084EBIDINTEG.:ll:nois.gov:PWIDOT\[Ocuments\IDOT Offices\District 6\Projects\D6	72E DRXXXXX Data\€ADsheets\D672E02-sht-05-S	-	STATE OF ILLINOIS			SCHEDULE		ANTITIES		7706	•	LOGAN	343 65
	PLOT SCALE = 40.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION									CONTRAC	T NO. 72E02
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FED.	. AID PROJECT	

78003100 PREFORMED	PLASTIC PAVEMENT MARKING, TP	E B - LETTER AND	SYMBOLS -	CONTINUED	
SB I55 BUSINESS/LINCOLN PARKWAY	RIGHT TURN ARROW	36+77.28	42.8	RT	15.6
AT 5TH STREET INTERSECTION	RIGHT TURN ARROW	37+80.11	43.6	RT	15.6
	RIGHT TURN ARROW	38+71.82	43.6	RT	15.6
	LEFT TURN ARROW	37+19.54	1.4	RT	15.6
	LEFT TURN ARROW	40+58.44	4.5	RT	15.6
	LEFT TURN ARROW	41+33.37	1.2	RT	15.6
IL10/121/WOODLAWN ROAD	LEFT TURN ARROW	133+90.17	2.5	LT	15.6
	LEFT TURN ARROW	134+59.35	3.3	LT	15.6
	RIGHT TURN ARROW	135+40.67	43.7	RT	15.6
	RIGHT TURN ARROW	136+18.31	43.4	RT	15.6
	RIGHT TURN ARROW	136+93.84	45.8	RT	15.6
	LEFT TURN ARROW	135+97.51	1.7	RT	15.6
	LEFT TURN ARROW	136+97.41	2.8	LT	15.6
	LEFT TURN ARROW	138+79.31	4.2	RT	15.6
	LEFT TURN ARROW	139+79.28	1.8	RT	15.6
	LEFT TURN ARROW	140+79.25	0.6	LT	15.6
	RIGHT TURN ARROW	138+76.55	45.7	LT	15.6
	RIGHT TURN ARROW	139+66.61	42.9	LT	15.6
	RIGHT TURN ARROW	140+72.24	42.8	LT	15.6
	LEFT TURN ARROW	142+80.67	0.9	LT	15.6
	LEFT TURN ARROW	143+34.24	1.5	LT	15.6
	LEFT TURN ARROW	143+79.47	1.5	LT	15.6
5TH STREET	LEFT TURN ARROW	12+71.77	3.0	LT	15.6
	RIGHT TURN & THRU ARROW	12+47.71	13.3	RT	26.0
	THRU ARROW	13+05.52	12.6	RT	11.5
	RIGHT TURN ARROW	14+38.55	27.3	LT	15.6
	RIGHT TURN ARROW	15+03.06	27.5	LT	15.6
	THRU ARROW	14+34.06	12.2	LT	11.5
	THRU ARROW	15+11.28	11.9	LT	11.5
	LEFT TURN ARROW	14+59.07	2.4	RT	15.6
	LEFT TURN ARROW	15+14.44	1.8	RT	15.6
		PPS #6-00	477-0000 (HS	SIP) TOTAL =	825
		STP AND	HSIP COMBIN	IED TOTAL =	1361

	78003120 PREFORMED PLAS	STIC PAVEMENT	MARKING	, TYPE B	- LINE	5"			
	PPS#	6-00625-0000 (S	TP)						
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
SB I55 BUSINESS/LINCOLN PARKWAY	30' SKIP - 10' DASH	1747+18.82	0.0	RT	-	1754+61.15	0.0	RT	186
	30' SKIP - 10' DASH	9+63.40	22.0	RT	-	19+28.99	22.0	RT	242
	30' SKIP - 10' DASH	20+09.36	22.0	RT	-	34+18.04	22.0	RT	353
	30' SKIP - 10' DASH	44+87.15	22.0	RT	-	48+09.23	22.0	RT	81
	30' SKIP - 10' DASH	48+20.76	22.0	RT	-	74+00.00	22.0	RT	645
NB 155 BUSINESS/LINCOLN PARKWAY	30' SKIP - 10' DASH	9+63.40	22.5	LT	-	19+37.74	22.5	LT	244
	30' SKIP - 10' DASH	20+13.95	22.5	LT	-	34+18.04	22.5	LT	351
	30' SKIP - 10' DASH	44+87.16	22.0	LT	-	48+37.81	22.0	LT	88
	30' SKIP - 10' DASH	48+49.29	22.0	LT	-	74+00.00	22.0	LT	638
	30' SKIP - 10' DASH	80+23.13	22.0	LT	-	92+14.15	22.0	LT	300
		•				PPS# 6-00625-	0000 (STP)	TOTAL =	3128
	PPS#	6-00477-0000 (H	SIP)						
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
NB 155 BUSINESS/LINCOLN PARKWAY	SB CENTER SKIP DASH LINE	1758+04.88	1.0	RT	-	1761+74.65	1.0	RT	93
	SB CENTER SKIP DASH LINE	0+35.41	22.0	RT	-	9+63.40	22.0	RT	240
	SB CENTER SKIP DASH LINE	34+18.04	22.0	RT	-	39+18.66	22.0	RT	125
	SB CENTER SKIP DASH LINE	40+08.42	22.0	RT	-	44+87.15	22.0	RT	120
SB I55 BUSINESS/LINCOLN PARKWAY	NB CENTER SKIP DASH LINE	1758+05.90	43.0	LT	-	1761+79.62	43.0	LT	94
	NB CENTER SKIP DASH LINE	0+45.28	23.0	LT	-	9+63.40	22.5	LT	230
	NB CENTER SKIP DASH LINE	34+18.04	22.5	LT	-	39+18.66	22.5	LT	125
	NB CENTER SKIP DASH LINE	40+10.91	23.0	LT	-	44+87.16	22.2	LT	120
EB IL10/121/WOODLAWN ROAD	EB CENTER SKIP DASH LINE	133+66.35	23.1	RT	-	137+39.66	23.1	RT	93
	EB CENTER SKIP DASH LINE	138+34.01	23.1	RT	-	143+90.84	17.2	RT	140
WB IL10/121/WOODLAWN ROAD	WB CENTER SKIP DASH LINE	133+66.35	23.2	LT	-	137+36.34	23.0	LT	93
	WB CENTER SKIP DASH LINE	138+34.02	22.4	LT	-	143+90.84	17.3	LT	140
		•				PPS# 6-00477-0	000 (HSIP)	TOTAL =	1613

	78003130 PREFORMED PI	ASTIC PAVEME	NT MARK	ING, TYP	EB-LI	NE 6"			
	PI	PS# 6-00625-000	0 (STP)						
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
SB I55 BUSINESS/LINCOLN PARKWAY	6 ' SKIP- 2' DASH	14+58.26	33.5	RT	-	16+58.26	33.5	RT	50
	6 ' SKIP- 2' DASH	63+72.82	10.5	RT	-	66+02.25	10.5	RT	58
	6 ' SKIP- 2' DASH	69+56.39	10.5	RT	-	71+07.82	10.5	RT	38
NB 155 BUSINESS/LINCOLN PARKWAY	6 ' SKIP- 2' DASH	21+99.62	11.5	LT	1	24+68.19	11.5	LT	68
5TH STREET	6 ' SKIP- 2' DASH	15+20.66	18.0	LT	-	15.82.00	23.8	LT	16
						PPS# 6-00625	-0000 (STP) TOTAL =	230
	PF	S# 6-00477-000							
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
NB 155 BUSINESS/LINCOLN PARKWAY	WHITE 2' DASH 6' SKIP	1759+40.65	31.0	LT	-	1760+39.59	30.5	LT	24
	WHITE 2' DASH 6' SKIP	2+21.62	11.0	LT	-	4+18.06	10.5	RT	62
	WHITE 2' DASH 6' SKIP	2+21.62	11.0	LT	-	4+99.15	11.5	LT	70
	WHITE 2' DASH 6' SKIP	3+13.33	35.0	LT	-	5+13.29	33.5	LT	52
	WHITE 2' DASH 6' SKIP	7+66.51	11.0	LT	-	9+63.40	11.5	LT	50
	WHITE 2' DASH 6' SKIP	42+25.15	11.0	LT	-	44+87.15	10.5	LT	66
	WHITE 2' DASH 6' SKIP	42+14.44	36.0	LT	-	44+39.89	33.5	LT	58
SB 155 BUSINESS/LINCOLN PARKWAY	WHITE 2' DASH 6' SKIP	1758+26.31	12.6	RT	-	1759+87.66	13.0	RT	42
	WHITE 2' DASH 6' SKIP	1759+41.36	11.6	LT	-	1760+46.08	11.0	LT	28
	WHITE 2' DASH 6' SKIP	34+18.04	10.5	RT	-	37+09.07	10.0	RT	74
	WHITE 2' DASH 6' SKIP	34+50.78	33.5	RT	-	36+70.44	35.0	RT	56
EB IL10/121/WOODLAWN ROAD	WHITE 2' DASH 6' SKIP	133+79.82	34.6	RT	-	135+34.84	35.1	RT	40
	WHITE 2' DASH 6' SKIP	134+61.31	10.4	RT	-	135+71.65	11.1	RT	30
	WHITE 2' DASH 6' SKIP	140+82.65	11.7	RT	-	142+75.41	6.3	RT	50
WB IL10/121/WOODLAWN ROAD	WHITE 2' DASH 6' SKIP	134+63.24	11.1	LT	-	135+62.96	11.4	LT	26
	WHITE 2' DASH 6' SKIP	140+99.45	9.8	LT	-	142+20.62	6.8	LT	32
	WHITE 2' DASH 6' SKIP	140+76.58	34.4	LT	-	142+49.59	29.2	LT	44
						PPS# 6-00477-	0000 (HSIP) TOTAL =	804
						STP AND HSIP	COMBINE	TOTAL =	1034

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
pw:\\IL084EBIDINTEG.:ll:no:s.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67	PE DR'ANMN Data\€ADsheets\D672E02-sht-05-Sc	- REVISED -
	PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -

STATI	E OF	ILLINOIS
DEPARTMENT	0F	TRANSPORTATION

	SCHEDULE OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
					7706	•	LOGAN	343	66	
								CONTRACT	NO.	72E02
	SHEET	OF	SHEETS	SHEETS STA. TO STA. ILLINOIS FED. /						

	78009005 - MODIFIED URET	00625-0000 (STP)		-G - LIINE					
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOO
SB I-55 BUSINESS/LINCOLN PARKWAY	YELLOW INSIDE EDGE LINE	1747+77.22	12.0	LT	-	1753+50.24	12.0	LT	57
55 1 55 565 M255, EMCGEN 17 MM	WHITE OUTSIDE EDGE LINE	1747+18.82	12.0	RT	-	1751+65.87	12.0	RT	44
	WHITE OUTSIDE EDGE LINE	1751+68.19	26.4	RT	_	1754+61.15	12.0	RT	29
	YELLOW INSIDE EDGE LINE	9+63.40	10.5	RT	-	15+20.75	10.5	RT	55
	YELLOW DOUBLE MEDIAN LINE	15+20.75	10.5	RT	_	19+53.96	10.5	RT	88
	WHITE OUTSIDE EDGE LINE	9+63.40	33.5	RT	_	18+65.13	49.0	RT	90
	YELLOW DOUBLE MEDIAN LINE	20+51.01	10.5	RT	_	24+68.19	10.5	RT	83
	YELLOW INSIDE EDGE LINE	24+68.19	10.5	RT	_	34+18.04	10.5	RT	95
	WHITE OUTSIDE EDGE LINE	20+83.00	40.9	RT	_	34+18.04	33.5	RT	13
	YELLOW INSIDE EDGE LINE	44+87.15	10.5	RT	_	48+16.71	10.5	RT	33
	WHITE OUTSIDE EDGE LINE	44+87.15	33.5	RT	_	48+01.75	33.5	RT	3:
	YELLOW INSIDE EDGE LINE	48+28.22	10.5	RT	_	51+84.29	10.5	RT	36
	YELLOW INSIDE EDGE LINE	52+35.47	10.5	RT	_	63+72.32	10.5	RT	11
	YELLOW DOUBLE MEDIAN LINE	63+72.32	10.5	RT	_	68+13.03	1.5	LT	88
	YELLOW DOUBLE MEDIAN LINE	69+15.86	0.9	RT	_	72+18.35	1.5	LT	62
			9.0	RT			9.0	RT	29
	YELLOW DOUBLE MEDIAN LINE WHITE OUTSIDE EDGE LINE	73+14.08 48+13.30	33.5	RT	-	74+49.51 71+94.63	33.5	RT	23
		72+96.63	33.5	RT	-	71+94.63 76+12.80	33.5	RT	23 31
NB I-55 BUSINESS/LINCOLN PARKWAY	WHITE OUTSIDE EDGE LINE	9+63.40	33.5	LT	_		33.5	LT	24
ND 1-33 DUSINESS/LINCULIN PAKKWAY	WHITE OUTSIDE EDGE LINE				-	34+18.05 15+20.75			
	YELLOW DOUBLE MEDIAN LINE	9+63.40 15+20.75	11.5 11.5	LT LT	-	15+20.75	11.5 11.5	LT LT	55 87
	YELLOW DOUBLE MEDIAN LINE	+			-	19+20.04			
	YELLOW DOUBLE MEDIAN LINE	20+50.94	7.2	RT	-	24+68.19	11.5	LT	84
	YELLOW INSIDE EDGE LINE	24+68.19	11.5	LT	-	34+18.04	11.5	LT	9!
	YELLOW INSIDE EDGE LINE	44+87.15	10.5	LT	-	48+30.30	10.5	LT	34
	WHITE OUTSIDE EDGE LINE	44+87.15	33.5	LT	-	48+45.32	33.5	LT	3!
	YELLOW INSIDE EDGE LINE	48+41.85	10.5	LT	-	51+87.71	1.9	RT	34
	YELLOW INSIDE EDGE LINE	52+32.04	1.9	RT	-	63+72.32	10.5	LT	11
	YELLOW DOUBLE MEDIAN LINE	63+72.32	10.5	LT	-	68+13.06	10.5	LT	9:
	YELLOW DOUBLE MEDIAN LINE	69+18.56	10.5	LT	-	72+19.16	5.3	LT	63
	YELLOW DOUBLE MEDIAN LINE	73+14.08	3.0	RT	-	74+49.51	3.0	RT	2
	WHITE OUTSIDE EDGE LINE	48+56.73	33.5	LT	-	68+03.78	93.7	LT	19
	WHITE OUTSIDE EDGE LINE	69+42.62	33.5	LT	-	79+27.62	43.0	LT	99
	WHITE OUTSIDE EDGE LINE	79+91.96	43.0	LT	-	92+14.15	33.5	LT	12
STALHUT DRIVE	YELLOW DOUBLE MEDIAN LINE	38+70.37	6.0	RT	-	39+57.67	6.0	RT	19
	YELLOW DOUBLE MEDIAN LINE	38+70.37	6.5	LT	-	39+59.31	1.6	LT	18
5TH STREET	YELLOW DOUBLE CENTER LINE	15+43.44	44.0	RT	-	15+43.65	77.2	RT	6
	WHITE OUTSIDE EDGE LINE	15+58.19	67.2	RT	-	16+15.08	23.2	RT	9
	YELLOW DOUBLE CENTER LINE	15+75.08	5.4	RT	-	16+15.08	5.4	RT	8
	YELLOW DOUBLE CENTER LINE	15+35.07	45.9	LT	-	15+32.17	65.8	LT	4
	WHITE OUTSIDE EDGE LINE	15+52.05	66.8	LT	-	16+15.08	23.7	LT	9
STRINGER AVENUE	YELLOW DOUBLE CENTER LINE	68+58.35	68.1	LT	-	67+97.90	130.5	LT	1
						PPS# 6-00625-	0000 (STP)	TOTAL =	282
		0477-0000 (HSIP)					,		
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FO
SB 155 BUSINESS/LINCOLN PARKWAY	YELLOW INSIDE EDGE LINE	1747+77.22	11.5	LT	-	1753+50.24	11.5	LT	5
	WHITE OUTSIDE EDGE LINE	1747+18.82	11.5	RT	-	1751+65.87	11.5	RT	44
	WHITE OUTSIDE EDGE LINE	1751+68.19	26.4	LT	-	1751+68.19	30.5	LT	29
	YELLOW SINGLE MEDIAN LINE	1758+05.91	11.4	LT	-	1761+46.71	26.5	LT	34
	WHITE OUTSIDE EDGE LINE	1758+05.77	54.6	LT	-	143+90.35	27.6	LT	9:
	WHITE OUTSIDE EDGE LINE	6+42.57	33.5	RT	-	9+63.40	33.5	RT	3:
	WHITE OUTSIDE EDGE LINE	34+18.04	33.5	RT	-	12+41.03	30.2	LT	5
NB 155 BUSINESS/LINCOLN PARKWAY	YELLOW SINGLE MEDIAN LINE	1758+05.70	19.9	LT	-	1761+46.71	30.5	LT	3
	WHITE OUTSIDE EDGE LINE	1758+05.90	12.3	RT	-	133+66.35	34.6	LT	7:
	WHITE OUTSIDE EDGE LINE	6+42.63	33.5	LT	-	9+63.40	33.5	LT	32
	WHITE OUTSIDE EDGE LINE	34+18.05	33.5	LT	-	15+20.40	65.8	LT	63
				•			-		

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -							F.A.U. RTF.	SECTION	COUNTY	TOTAL SHEET SHEET NO.
pw:\\ILØ84EBIDINTEG.:ll:nois.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67	2E DRAMM Data\&ADsheets\D672E02-sht-05-Sc	- REVISED -	STATE OF ILLINOIS		5	SCHEDULI	E OF QUANTITIES		7706	•	LOGAN	343 67
	PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION								CONTRACT	T NO. 72EO2
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS STA.	TO STA.		ILLINOIS FED. A	ID PROJECT	
									• (22)RS-6	6, MED & (2	23) RS-5		

	78009005 - MODIFIED URET	HANE PAVEMEN	T MARKII	NG - LINE	5"				
NB & SB I55 BUSINESS/LINCOLN PARKWAY	YELLOW DOUBLE MEDIAN LINE	1761+46.71	30.5	LT	-	1761+68.43	30.5	LT	84
	YELLOW DOUBLE MEDIAN LINE	0+69.29	8.8	RT	-	5+49.53	9.3	LT	1926
	YELLOW DOUBLE MEDIAN LINE	6+48.63	9.1	RT	-	9+63.40	0.5	LT	1261
	YELLOW DOUBLE MEDIAN LINE	34+18.04	0.5	LT	-	38+95.49	11.0	LT	1879
IL10/121/WOODLAWN ROAD	WHITE OUTSIDE EDGE LINE	133+66.34	34.6	RT	-	5+68.02	33.5	LT	865
	YELLOW SINGLE MEDIAN LINE	133+70.62	5.6	RT	-	137+09.59	11.6	LT	342
	YELLOW SINGLE MEDIAN LINE	133+70.62	5.6	RT	-	137+09.61	6.6	LT	342
	YELLOW DOUBLE MEDIAN LINE	137+09.59	10.5	LT	-	137+24.42	8.6	LT	60
	YELLOW DOUBLE MEDIAN LINE	138+52.87	8.8	RT	-	138+86.13	9.2	RT	133
	YELLOW SINGLE MEDIAN LINE	138+86.11	6.6	RT	-	142+20.62	6.8	LT	335
	YELLOW SINGLE MEDIAN LINE	138+86.13	11.7	RT	-	142+20.70	0.7	LT	335
	YELLOW DOUBLE MEDIAN LINE	142+19.97	3.7	RLT	-	143+90.71	6.3	LT	429
	WHITE OUTSIDE EDGE LINE	143+91.31	37.7	RT	-	5+57.68	33.5	LT	1036
5TH STREET	WHITE OUTSIDE EDGE LINE	12+41.03	21.5	RT	-	44+87.15	33.5	RT	540
5TH STREET	WHITE OUTSIDE EDGE LINE	15+31.96	77.5	RT	-	44+87.16	33.5	LT	624
5TH STREET	YELLOW DOUBLE CENTER LINE	12+41.03	6.0	LT	-	12+90.28	6.0	LT	100
						PPS# 6-00477-0	000 (HSIP)	TOTAL =	15747
						STP AND HSIP C	OMBINED	TOTAL =	43995

		PPS# 6-00625-00	00 (STP)						
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOOT
SB I-55 BUSINESS/LINCOLN PARKWAY	WHITE CORNER ISLAND LINE	18+77.70	33.5	RT	-	19+48.79	33.5	RT	71
·	WHITE CORNER ISLAND LINE	18+77.70	33.5	RT	- 1	19+47.64	89.1	RT	90
	WHITE CORNER ISLAND LINE	19+48.79	33.5	RT	- 1	19+47.64	89.1	RT	56
	WHITE CORNER ISLAND LINE	19+87.80	33.5	RT	- 1	20+42.03	33.5	RT	54
	WHITE CORNER ISLAND LINE	19+87.80	33.5	RT	- 1	19+86.93	78.4	RT	45
	WHITE CORNER ISLAND LINE	19+86.93	78.4	RT	-	20+42.03	33.5	RT	72
STRINGER AVENUE	WHITE CORNER ISLAND LINE	67+97.44	33.5	LT	- 1	68+57.00	33.5	LT	60
	WHITE CORNER ISLAND LINE	67+97.44	33.5	LT	-	68+21.26	91.3	LT	63
	WHITE CORNER ISLAND LINE	68+21.26	91.3	LT	-	68+57.00	33.5	LT	72
	•					PPS# 6-0	0625-0000 (S	ΓP) TOTAL =	583
								•	
		PPS# 6-00477-00	00 (HSIP)						
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOOT
SB 155 BUSINESS/LINCOLN PARKWAY	SOLID WHITE	1761+33.17	13.0	RT	-	1761+78.66	13.0	RT	46
	SOLID WHITE	1761+84.66	13.0	RT	-	1761+94.66	13.0	RT	10
	SOLID WHITE	1761+33.17	13.0	RT	-	1761+69.61	34.5	RT	43
	SOLID WHITE	1761+73.99	38.6	RT	-	1761+93.73	65.7	RT	34
	SOLID WHITE	0+35.05	34.0	RT	-	0+90.83	34.0	RT	56
	SOLID WHITE	0+34.05	93.2	RT	-	0+90.83	34.0	RT	84
	SOLID WHITE	38+72.04	35.0	RT	-	39+40.19	35.0	RT	68
	SOLID WHITE	38+72.04	35.0	RT	-	39+40.17	91.3	RT	91
	SOLID WHITE	39+81.98	35.0	RT	-	40+43.61	35.0	RT	62
	SOLID WHITE	39+82.01	86.2	RT	-	40+43.61	35.0	RT	82
NB 155 BUSINESS/LINCOLN PARKWAY	SOLID WHITE	1761+44.45	55.0	LT	-	1761+79.62	55.0	LT	35
	SOLID WHITE	1761+85.83	55.0	LT	-	1761+95.91	55.0	LT	10
	SOLID WHITE	1761+44.45	55.0	LT	-	1761+72.64	74.8	LT	35
	SOLID WHITE	1761+76.78	79.1	LT	-	1761+96.91	112.6	LT	39
	SOLID WHITE	1762+29.37	56.0	LT	-	1762+29.37	55.9	LT	58
	SOLID WHITE	1762+29.37	106.3	LT	-	1762+29.37	55.9	LT	78
	SOLID WHITE	39+88.22	36.0	LT	-	40+20.73	36.0	LT	33
	SOLID WHITE	39+88.23	68.3	LT	-	40+43.61	35.0	RT	47
EB IL 10/121/WOODLAWN ROAD	SOLID WHITE	136+94.02	35.1	RT	-	137+53.22	35.1	RT	59
	SOLID WHITE	138+22.23	35.1	RT	-	138+72.83	35.1	RT	51
WB IL 10/121/WOODLAWN ROAD	SOLID WHITE	136+99.38	34.5	LT	-	137+52.05	34.5	LT	53
	SOLID WHITE	138+20.06	34.4	LT	-	138+77.66	34.4	LT	58
EB 5TH STREET	SOLID WHITE	12+78.03	17.8	RT	-	13+29.19	17.8	RT	51
	SOLID WHITE	14+00.19	24.0	RT	-	14+32.50	24.0	RT	32
WB 5TH STREET	SOLID WHITE	12+72.92	24.0	LT	-	13+29.17	24.0	LT	56
						PPS #6-00	477-0000 (HS	IP) TOTAL =	1271

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -							F.A.U.	SECTION	COUNTY	SHEETS NO.	Т
pw:\\IL084EBIDINTEG.:llinois.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67	PE DR XUMU Data\&ADsheets\D672E02-sht-05-Sc	- REWISED -	STATE OF ILLINOIS				OF QUANTITIES		7706	•	LOGAN	343 68	1
	PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION								CONTRACT	NO. 72E02	1
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS STA.	TO STA.		ILLINOIS FED. AI	D PROJECT		1
									• (22)RS-6	, MED & (2	3) RS-5			•

F.A.U. RTE.	SEC.	rion		COUNTY	TOTAL SHEETS	SHEET NO.
7706		•		LOGAN	343	69
				CONTRACT	NO. 7	2E02
		ILLINOIS	FED. AII	PROJECT		
	& (23) RS-5					

TO STA.

SCHEDULE OF QUANTITIES

OF SHEETS STA.

SCALE:

SHEET

		PPS# 6-00625	` ` `						
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOOT
SB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE LINE	16+58.26	33.5	RT	-	18+77.70	33.5	RT	220
	WHITE TURN LANE LINE	66+02.25	10.5	RT	-	68+17.25	10.5	RT	215
	WHITE TURN LANE LINE	71+07.82	10.5	RT	-	72+22.82	10.5	RT	115
NB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE LINE	20+14.13	3.2	LT	-	21+99.62	11.5	LT	186
	WHITE TURN LANE LINE	20+13.95	11.5	LT	-	21+99.62	11.5	LT	199
	WHITE TURN LANE LINE	73+14.08	9.0	LT	-	74+49.51	9.0	LT	136
5TH STREET		15+75.08	6.0	LT	-	16+15.08	6.0	LT	40
						PPS# 6-0	0625-0000 (S	TP) TOTAL =	1111
		PPS# 6-00477	-0000 (HSIP)						
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOOT
NB 155 BUSINESS/LINCOLN PARKWAY	SOLID WHITE	1758+05.82	31.0	LT	-	1759+40.65	31.0	LT	135
	SOLID WHITE	0+42.83	3.3	LT	-	2+21.62	11.0	LT	191
	SOLID WHITE	0+42.66	11.0	LT	-	2+21.62	11.0	LT	179
	SOLID WHITE	0+94.37	35.0	LT	-	3+13.33	35.0	LT	219
	SOLID WHITE	6+26.94	2.5	LT	-	3+13.33	35.0	LT	154
	SOLID WHITE	6+26.68	11.0	LT	-	5+99.64	48.2	LT	140
	SOLID WHITE	40+08.62	1.5	LT	-	42+25.15	11.0	RT	232
	SOLID WHITE	40+08.42	11.0	LT	-	42+25.15	11.0	RT	217
	SOLID WHITE	40+20.73	36.0	LT	-	42+14.44	36.0	LT	194
SB I55 BUSINESS/LINCOLN PARKWAY	SOLID WHITE	1759+87.66	13.0	RT	-	1761+33.17	13.0	RT	146
•	SOLID WHITE	1760+46.08	11.0	LT	-	1761+89.62	17.5	LT	148
	SOLID WHITE	1760+46.08	11.0	LT	-	1761+89.77	11.0	LT	138
	SOLID WHITE	1761+56.27	46.7	RT	-	1761+77.37	27.3	RT	29
	SOLID WHITE	1761+60.71	50.8	RT	-	1761+81.43	31.8	RT	29
	SOLID WHITE	4+18.06	10.5	LT	-	5+79.82	3.0	RT	174
	SOLID WHITE	4+18.06	10.5	LT	-	5+79.96	10.5	RT	162
	SOLID WHITE	37+09.07	10.0	RT	-	39+18.56	1.0	RT	224
	SOLID WHITE	37+09.07	10.0	RT	-	39+18.75	10.0	RT	210
	SOLID WHITE	36+70.44	35.0	RT	-	38+72.04	35.0	RT	202
B & SB I55 BUSINESS/LINCOLN PARKWAY	SOLID WHITE	1761+78.55	18.9	RT	_	1761+79.91	50.1	LT	79
S & SD 133 BOSHNESS, EMPOSEIN TAMMONT	SOLID WHITE	1761+84.56	19.0	RT	-	1761+85.91	60.0	LT	
WB IL 10/121/WOODLAWN ROAD	SOLID WHITE	133+66.35	11.2	LT	-	134+63.24	11.1	LT	97
io, izi, woodlawa noad	SOLID WHITE	138+28.47	3.5	LT	-	140+99.45	9.8	LT	282
	SOLID WHITE	138+28.39	10.4	LT	-	140+99.45	9.8	LT	271
	SOLID WHITE	138+77.66	34.4	LT	-	140+76.58	34.4	LT	199
	SOLID WHITE	138+52.01	70.9	LT	-	138+32.91	51.3	LT	28
	SOLID WHITE	138+37.21	47.1	LT	-	138+56.45	66.9	LT	28
	SOLID WHITE	135+34.84	35.1	RT	-	136+94.02	35.1	RT	160
	SOLID WHITE	135+73.27	11.0	RT	-	137+45.09	11.0	RT	172
	SOLID WHITE	135+73.27	11.0	RT	-	137+44.92	3.3	RT	184
	SOLID WHITE	142+75.41	6.3	RT	-	143+90.84	6.3	RT	116
ER STU STREET	SOLID WHITE								
EB 5TH STREET	SOLID WHITE	12+41.03	6.0	RT	-	13+16.16	6.0	RT	75
WB 5TH STREET		14+19.18	6.0	LT	-	15+20.66	6.0	LT	102
	SOLID WHITE	14+19.18	18.0	RT	-	15+20.66	18.0	LT LT	102
							HSIP COMBIN		5097 6208

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

FILE NAME =

USER NAME = whitlingme

PLOT DATE = 10/21/2016

PLOT SCALE = 40.0000 ' / in.

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DESIGNED -

CHECKED -

DATE

REVISED -

REVISED -

REVISED -

	78009012 MODIFIED URET	HANE PAVEMENT	MARKING -	LINE 12"					
	PPS# 6	-00625-0000 (STP)							
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOOT
NB & SB I-55 BUSINESS/LINCOLN PARKWAY	YELLOW DIAGONALS CENTER MEDIAN	15+20.75	0.0	RT	-	19+52.07	0.0	RT	392
	YELLOW DIAGONALS CENTER MEDIAN	20+50.43	8.8	RT	-	24+68.18	0.0	RT	207
	YELLOW DIAGONALS CENTER MEDIAN	51+80.04	0.0	RT		51+88.93	0.0	RT	19
	YELLOW DIAGONALS CENTER MEDIAN	52+30.82	0.0	RT		52+39.71	0.0	RT	19
	YELLOW DIAGONALS CENTER MEDIAN	63+72.32	0.0	RT	-	68+16.44	6.0	LT	296
	YELLOW DIAGONALS CENTER MEDIAN	69+16.57	0.0	RT	-	72+19.24	3.3	LT	223
SB I-55 BUSINESS/LINCOLN PARKWAY	WHITE DIAGONALS ISLAND	18+77.70	33.5	RT	-	19+48.79	33.5	RT	88
	WHITE DIAGONALS ISLAND	19+87.80	33.5	RT	-	20+42.03	33.5	RT	60
	YELLOW DIAGONALS CENTER MEDIAN	73+11.66	6.0	RT	-	74+49.51	9.0	RT	29
NB I-55 BUSINESS/LINCOLN PARKWAY	WHITE CHEVRONS TURN LANE MEDIAN	20+09.80	7.3	LT	-	21+99.62	11.5	LT	44
	WHITE DIAGONALS ISLAND	67+97.44	33.5	LT	-	68+57.00	33.5	LT	79
STALHUT DRIVE	YELLOW DIAGONALS CENTER MEDIAN	19+67.94	129.7	RT	-	16+69.69	38.9	RT	86
		•	•			PPS# 6-0	0625-0000 (S	TP) TOTAL =	1541
							-	•	
	PPS# 6	-00477-0000 (HSIP))						
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	FOOT
SB I55 BUSINESS/LINCOLN PARKWAY	LEFT TURN LANE CHEVRONS	1760+59.20	11.3	LT	-	1761+86.45	14.2	LT	35
	LEFT TURN LANE CHEVRONS	4+31.17	10.2	RT	-	5+76.01	6.5	RT	44
	CENTER MEDIAN DIAGONALS	0+83.26	8.5	RT	-	5+46.36	9.2	LT	112
	NW CORNER ISLAND	1761+33.17	13.0	RT	- 1	1761+94.66	13.0	RT	61
	SW CORNER ISLAND	0+35.05	34.0	RT	-	0+90.83	34.0	RT	61
	LEFT TURN LANE CHEVRONS	37+09.07	10.0	RT	- 1	39+23.25	5.4	RT	70
	NW CORNER ISLAND	38+72.04	35.0	RT	-	39+40.19	35.0	RT	71
	SW CORNER ISLAND	39+81.98	35.0	RT	-	40+43.51	35.0	RT	63
NB I55 BUSINESS/LINCOLN PARKWAY	LEFT TURN LANE CHEVRONS	0+46.66	7.5	LT	-	2+06.66	10.7	LT	53
·	LEFT TURN LANE CHEVRONS	6+31.26	6.9	LT	-	7+54.78	10.6	LT	46
	NE CORNER ISLAND	1761+44.45	55.0	LT	-	1761+95.91	55.0	LT	54
	SE CORNER ISLAND	0+36.20	35.0	LT	_	0+94.37	35.0	LT	57
	LEFT TURN LANE CHEVRONS	40+03.67	6.1	LT	-	42+25.15	11.0	LT	73
	SE CORNER ISLAND	39+88.22	36.0	LT	-	40+20.73	36.0	LT	36
6B & NB 155 BUSINESS/LINCOLN PARKWAY	CENTER MEDIAN DIAGONALS	1761+46.71	28.5	LT	-	1761+67.35	29.0	LT	6
,	CENTER MEDIAN DIAGONALS	6+64.73	8.6	RT	-	9+39.51	0.5	RT	151
	CENTER MEDIAN DIAGONALS	34+18.04	0.5	LT	-	38+79.15	10.4	LT	219
	CENTER MEDIAN DIAGONALS	40+58.21	9.4	RT	-	44+87.15	0.0	LT	195
EB IL 10/121/WOODLAWN ROAD	LEFT TURN LANE CHEVRONS	135+92.72	10.6	RT	1 - 1	137+41.18	7.3	RT	48
WB IL 10/121/WOODLAWN ROAD	LEFT TURN LANE CHEVRONS	138+31.89	7.0	LT	-	140+86.30	9.8	LT	72
EB & WB IL 10/121/WOODLAWN ROAD	CENTER MEDIAN DIAGONALS	137+09.59	9.1	LT	- 1	137+23.34	8.7	LT	5
,	CENTER MEDIAN DIAGONALS	138+53.95	8.9	RT	1 - 1	138+86.13	9.2	RT	16
	CENTER MEDIAN DIAGONALS	142+19.97	3.7	LT	-	142+63.93	5.7	LT	10
			- * *		1 1		1477-0000 (HS		1557
							(,	

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -								F.A.U.	SECTION	COUNTY	TOTAL SHEET
pw:\\IL084EBIDINTEG.:llinois.gov:PWIDOT\De	cuments\IDOT Offices\District 6\Projects\D67	ZE DRXXXXX Data\€ADsheets\D672E02-sht-05-S	- REVISED -	STATE OF ILLINOIS		;	SCHEDU	LE OF QU	JANTITIES		7706	•	LOGAN	343 70
	PLOT SCALE = 40.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION									CONTRACT	NO. 72E02
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FED.	AID PROJECT	
										• (22)RS	-6, MED &	(23) RS-5		

	PPS# 6-	00625-0000 (9	TP)			
ROADWAY	STATION	SIDE	Το	STATION	SIDE	EACH
NB I55 BUSINESS/LINCOLN PARKWAY	9+63.40	LT	1 - 1	34+18.06	LT	68.0
NB I55 BUSINESS/LINCOLN PARKWAY	44+87.16	LT	1 - 1	74+50.00	LT	75.0
NB I55 BUSINESS/LINCOLN PARKWAY	80+23.13	LT	- 1	92+14.15	LT	15.0
SB I55 BUSINESS/LINCOLN PARKWAY	1747+18.82	RT	- 1	1754+16.00	RT	12.0
SB I55 BUSINESS/LINCOLN PARKWAY	9+63.40	RT	- 1	34+18.06	RT	61.0
SB I55 BUSINESS/LINCOLN PARKWAY	44+87.16	RT	- 1	74+50.00	RT	72.0
			P	PS# 6-00625-0000	(STP) TOTAL =	303
	DDS# 6 (00477-0000 (H	CID)			
ROADWAY	STATION	LANES	TO	STATION	LANES	EACH
SB I55 BUSINESS/LINCOLN PARKWAY	1747+18.82	RT		1754+61.15	LT	11.0
•	1758+04.88	RT		1761+74.65	RT	21.0
	0+35.41	RT		9+63.40	RT	43.0
	34+18.06	RT		39+18.66	RT	42.0
	40+08.42	RT		44+87.16	RT	18.0
NB I55 BUSINESS/LINCOLN PARKWAY	1758+05.90	LT		1761+79.62	LT	11.0
	0+45.28	LT		9+63.40	LT	60.0
	34+18.06	LT		39+18.66	LT	19.0
	40+08.42	LT		44+87.16	LT	37.0
WB IL 10/121/WOODLAWN ROAD	133+66.35	LT		137+36.34	LT	9.0
	138+34.02	LT		143+90.84	LT	38.0
EB IL 10/121/WOODLAWN ROAD	133+66.35	RT		137+39.66	RT	22.0
	138+34.01	RT		143+90.84	RT	20.0
5TH STREET	12+41.08	EB		13+30.69	EB	4.0
	14+39.99	EB		15+20.66	EB	8.0
	12+41.03	WB		12+90.28	WB	4.0
	14+19.18	WB		15+20.66	WB	6.0
			P	PS# 6-0047-0000 (HSIP) TOTAL =	373
			ST	P AND HSIP COMB	INED TOTAL =	676

	PPS# 6	-00625	-0000 (STP)		
ROADWAY	STATION	то	STATION	LOCATION	EACH
I55 BUSINESS/LINCOLN PARKWAY	9+63.40	- 1	15+20.75	CENTER MEDIAN	36.0
	24+68.19	- 1	34+18.04	CENTER MEDIAN	56.0
	44+87.15	T - 1	48+22.74	CENTER MEDIAN	24.0
	48+43.63	-	51+80.04	CENTER MEDIAN	24.0
	52+39.71	-	63+72.32	CENTER MEDIAN	64.0
STALHUT DRIVE	39+39.96	-	39+57.10	SW CORNER ISLAND	11.0
	39+40.19	T - I	39+55.23	NW CORNER ISLAND	12.0
STRINGER AVENUE	21+32.61	-	21+71.65	CORNER ISLAND	15.0
			PPS	# 6-00625-0000 (STP) TOTAL =	242
DOADWAY		1	77-0000	LOCATION	5400
ROADWAY	STATION	то	STATION	LOCATION	EACH
155 BUSINESS/LINCOLN PARKWAY	1758+06.93	1 - 1	1761.33.31		
iss sources, Entered Livin in the transfer		+	1761+32.31	NORTH CENTER MEDIAN	32
155 DOSINESS, EINOCENT, WINNERN	1761+73.27	-	1761+93.90	NW MEDIAN ISLAND	9
	1761+73.27 1761+75.23	-	1761+93.90 1761+95.78	NW MEDIAN ISLAND NE CORNER ISLAND	9
100 000111200, 21110022117, 111111711	1761+73.27	-	1761+93.90	NW MEDIAN ISLAND	9
	1761+73.27 1761+75.23	-	1761+93.90 1761+95.78	NW MEDIAN ISLAND NE CORNER ISLAND	9
	1761+73.27 1761+75.23 0+34.93	-	1761+93.90 1761+95.78 0+56.40	NW MEDIAN ISLAND NE CORNER ISLAND SW CORNER ISLAND	9 10 10
	1761+73.27 1761+75.23 0+34.93 0+36.50	-	1761+93.90 1761+95.78 0+56.40 0+57.57	NW MEDIAN ISLAND NE CORNER ISLAND SW CORNER ISLAND SE CORNER ISLAND	9 10 10 10
IL 10/121/WOODLAWN ROAD	1761+73.27 1761+75.23 0+34.93 0+36.50 39+11.00	-	1761+93.90 1761+95.78 0+56.40 0+57.57 39+36.00	NW MEDIAN ISLAND NE CORNER ISLAND SW CORNER ISLAND SE CORNER ISLAND NW CORNER ISLAND	9 10 10 10 10
,	1761+73.27 1761+75.23 0+34.93 0+36.50 39+11.00 39+81.98	-	1761+93.90 1761+95.78 0+56.40 0+57.57 39+36.00 40+07.10	NW MEDIAN ISLAND NE CORNER ISLAND SW CORNER ISLAND SE CORNER ISLAND NW CORNER ISLAND SW CORNER ISLAND	9 10 10 10 10 12

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	PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -	l
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -	

STATE OF ILLINOIS	
DEPARTMENT OF TRANSPORTA	TION

	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEE		
SCHEDULE OF QUANTITIES	7706	•	LOGAN	343	71		
			CONTRACT	NO.	72E0		
SCALE: SHEET OF SHEETS STA. TO STA.		ILLINOIS FED. A	ID PROJECT				
• (22)RS-6, MED & (23) RS-5							

STP AND HSIP COMBINED TOTAL =

30189

			Z0033	700 - LONGITUDINAL JOINT SEAL	
				PPS# 6-00625-0000 (STP)	
ROADWAY	STATION	то	STATION	LOCATION	QUANTITY (FOOT)
S.B. I-55 BUSINESS/	9+63.42	-	34+18.07	S.B. THRU LANES CENTERLINE	2454.7
LINCOLN PARKWAY	14+58.26	-	18+77.70	S.B. RIGHT TURN LANE LINE	419.4
	19+55.62	-	20+57.67	S.B. MEDIAN LINE (10.3' RT OFFSET FROM C.L. RDWY.)	102.1
	19+53.88	-	22+35.31	MEDIAN CENTERLINE (C.L. OF RDWY.)	281.4
	44+87.18	-	48+09.23	S.B. THRU LANES CENTERLINE	322.0
	48+20.76	-	74+00.00	S.B. THRU LANES CENTERLINE	2579.2
	63+72.32	-	73+13.31	S.B. LEFT TURN LANE, OUTSIDE LINE	941.0
	65+73.51	-	69+16.58	S.B. LEFT TURN LANE LINE (C.L. OF RDWY.)	343.1
	71+00.00	 	74+00.00	S.B. LEFT TURN LANE LINE (C.L. OF RDWY.)	300.0
N.B. I-55 BUSINESS/	9+63.42	<u> </u>	34+18.07	N.B. THRU LANES CENTERLINE	2454.7
LINCOLN PARKWAY	19+20.04	-	24+68.19	N.B. LEFT TURN LANE LINE	548.2
LINCOLINTALLICATION	44+87.18	-	48+38.26	N.B. THRU LANES CENTERLINE	351.1
	48+49.73	 	74+00.00	N.B. THRU LANES CENTERLINE	2550.3
	68+15.09	-	69+16.16	N.B. MEDIAN LANE (10.0' LT OFFSET FROM C.L. RDWY.)	101.1
	72+08.12	<u> </u>	74+00.00	N.B. LEFT TURN / MEDIAN LINE (9.0' LT OFFSET FROM C.L. RDWY.)	191.9
	72100.12		74100.00	PPS #6-00625-0000 (STP) TOTAL =	13940
				773 #0-00023-0000 (S17) TOTAL -	13340
				PPS# 6-00477-0000 (HSIP)	
				113# 0-00477-0000 (113#)	
ROADWAY	STATION	то	STATION	LOCATION	QUANTITY (FOOT)
S.B. I-55 BUSINESS/	1758+05.91	-	1761+94.88	S.B. THRU LANES CENTERLINE	389.0
LINCOLN PARKWAY	1758+26.31	-	1761+94.64	S.B. RIGHT TURN LANE LINE	368.3
	1759+41.36	-	1761+95.09	S.B. LEFT TURN LANE LINE	253.7
	STATION EQUA	TION:	STA. 1762+29.37	(BACK) = STA. 0+00.49 (AHEAD)	
	+35.27	-	9+63.42	S.B. THRU LANES CENTERLINE	928.2
	+35.47	-	6+49.33	S.B. LEFT TURN LANE LINE (10.4' RT OFFSET FROM C.L. RDWY.)	613.9
	+35.04	-	2+15.48	S.B. OUTSIDE THRU LANE LINE	180.4
	34+18.07	-	44+87.18	S.B. THRU LANES CENTERLINE	1069.1
	34+18.07	-	40+32.54	S.B. LEFT TURN LANE	614.5
	34+50.78	-	41+44.53	S.B. RIGHT TURN LANE / OUTSIDE THRU LANE LINE	693.8
N.B. I-55 BUSINESS/	1758+05.91	-	1761+95.67	N.B. THRU LANES CENTERLINE	389.8
LINCOLN PARKWAY	1758+05.91	-	1760+39.59	N.B LEFT TURN LANE LINE	233.7
	1760+58.92	-	1761+95.91	N.B. OUTSIDE THRU LANE LINE	137.0
	STATION EQUA	TION:	STA. 1762+29.37	(BACK) = STA. 0+00.49 (AHEAD)	
	+36.08	-	9+63.42	N.B. THRU LANES CENTERLINE	927.3
	+35.67	-	9+63.42	N.B. LEFT TURN LANE LINE (10.5' LT OFFSET FROM C.L. RDWY.)	927.8
	+35.66	-	2+32.49	ROADWAY CENTERLINE	196.8
	+36.30	-	5+13.29	N.B. RIGHT TURN LANE LINE	477.0
	4+04.64	-	7+65.95	ROADWAY CENTERLINE	361.3
	34+18.07	-	44+87.18	N.B. THRU LANES CENTERLINE	1069.1
	38+82.65	-	44+39.89	N.B. RIGHT TURN LANE LINE	557.2
	38+95.56	-	44+87.18	N.B. INSIDE THRU LANE / LEFT TURN LANE LINE	591.6
	36+62.24	-	42+72.95	ROADWAY CENTERLINE	610.7
E.B. IL 10/121/	133+70.22	-	143+91.13	E.B. THRU LANES CENTERLINE	1020.9
WOODLAWN ROAD	133+79.82	-	139+61.51	E.B. RIGHT TURN LANE / OUTSIDE THRU LANE LINE	581.7
	134+67.41	-	138+55.34	E.B. LEFT TURN LANE / INSIDE THRU LANE LINE	387.9
	140+82.65	-	143+90.94	E.B.LEFT TURN LANE / INSIDE THRU LANE LINE	308.3
W.B. IL 10/121/	133+69.40	-	143+90.53	W.B. THRU LANES CENTERLINE	1021.1
WOODLAWN ROAD	133+69.61	-	135+62.96	W.B. LEFT TURN LANE LINE	193.3
	136+02.73	-	142+49.59	W.B. OUTSIDE THRU LANE / RIGHT TURN LANE LINE	646.9
	137+21.83	-	142+20.62	W.B. INSIDE THRU LANE / LEFT TURN LANE LINE	498.8
	•	•	•	PPS #6-00477-0000 (HSIP) TOTAL =	16249
				STP AND HSIP COMBINED TOTAL =	20190

	HOT-MIX ASPHALT SURFA		***************************************			
	I-55 BUSINESS/LINCO					
	PPS# 6-00625-00	, , , , , , , , , , , , , , , , , , , 				
DOADWAY	LANE DEPTH				CTATION	CO VE
ROADWAY	DESCRIPTION	INCHES	STATION	то	STATION	SQ YE
NBI-55 BUSINESS/LINCOLN PARKWAY	OUTSIDE SHOULDER	VARIABLE	4+77.42	-	5+63.30	56
	OUTSIDE SHOULDER	VARIABLE	6+32.39	-	35+73.21	2605
	DRIVING	3	9+63.40	-	17+60.00	1019
	PASSING	VARIABLE	9+63.40	-	17+60.00	1017
	DRIVING & PASSING	VARIABLE	17+60.00	-	18+75.00	315
	DRIVING & PASSING	2	18+75.00	-	20+82.79	785
	DRIVING & PASSING	VARIABLE	20+82.79	-	21+75.00	348
	DRIVING	3	21+75.00	-	34+18.05	1565
	PASSING & TURN LANE	VARIABLE	21+75.00	-	24+04.46	577
	PASSING	VARIABLE	24+04.46	-	34+18.05	1315
	DRIVING	3	44+87.16	-	47+00.00	292
	PASSING	VARIABLE	44+87.16	-	47+00.00	281
	OUTSIDE SHOULDER	VARIABLE	45+15.19	-	48+50.72	316
	DRIVING & PASSING	VARIABLE	47+00.00	-	48+00.00	267
	DRIVING & PASSING	2	48+00.00	-	48+33.70	69
	DRIVING & PASSING	2	48+53.44	-	48+90.00	77
	OUTSIDE SHOULDER	VARIABLE	48+62.23	-	67+77.60	1697
	DRIVING & PASSING	VARIABLE	48+90.00	-	49+90.00	267
	DRIVING	3	49+90.00	-	72+56.51	2975
	PASSING	3	49+90.00	-	72+56.51	3069
	OUTSIDE SHOULDER	VARIABLE	69+24.94	-	72+76.03	310
	OUTSIDE SHOULDER	VARIABLE	73+26.33	-	76+90.11	324
	DRIVING & PASSING	VARIABLE	72+56.51	-	73+50.00	353
SB I-55 BUSINESS/LINCOLN PARKWAY	OUTSIDE SHOULDER	VARIABLE	1+32.21	-	5+72.07	367
,	OUTSIDE SHOULDER	VARIABLE	6+29.56	-	17+64.57	877
	DRIVING & TURN LANE	2	9+63.40	-	17+60.00	1274
	PASSING	VARIABLE	9+63.40	-	17+60.00	1061
	DRIVING & PASSING	VARIABLE	17+60.00	-	18+75.00	504
	DRIVING & PASSING	2	18+75.00	-	20+82.79	1820
	DRIVING & PASSING	VARIABLE	20+82.79	-	21+75.00	348
	DRIVING	2	21+75.00	-	34+18.05	1669
	PASSING & TURN LANE	VARIABLE	21+75.00	- 1	24+04.46	563
	PASSING	VARIABLE	24+04.46	- 1	34+18.05	1346
	OUTSIDE SHOULDER	VARIABLE	24+95.37	-	33+70.56	788
	OUTSIDE SHOULDER	VARIABLE	44+60.29	- 1	48+01.42	301
	DRIVING	2	44+87.16	-	46+50.00	222
	PASSING	VARIABLE	44+87.16	- 1	46+50.00	213
	DRIVING & PASSING	VARIABLE	46+50.00	- 1	47+50.00	267
	DRIVING & PASSING	2	47+50.00	- 1	48+05.11	126
	OUTSIDE SHOULDER	VARIABLE	48+07.86	- 1	72+03.54	2121
	DRIVING & PASSING	2	48+24.90	- 1	48+75.00	113
	DRIVING & PASSING	VARIABLE	48+75.00	- 1	49+75.00	267
	DRIVING	2	49+75.00	- 1	72+56.51	3175
	PASSING	VARIABLE	49+75.00	- 1	72+56.51	2909
	DRIVING & PASSING	VARIABLE	72+56.51	- 1	73+50.00	353
	OUTSIDE SHOULDER	VARIABLE	72+86.01	- 1	76+12.80	283
5TH STREET	ALL LANES	1.5	14+81.96	- 1	16+05.08	1092
STRINGER AVENUE	ALL LANES	1.5	19+99.72	- 1	22+56.24	1000
KENWOOD DRIVE	ALL LANES	1.5	71+94.63	-	72+96.53	279
CEMETARY ENTRANCE (EAST SIDE)	ALL LANES	1.5	,1154.05	\vdash	, 2130.33	66
SEIVIETANT ENTINAINCE (LAST SIDE)	ALL	1 1.5		\Box		- 00

	PPS# 6-00477-000	00 (HSIP)				
	LANE	DEPTH				
ROADWAY	DESCRIPTION	INCHES	STATION	то	STATION	s
INTERSECTION I-55 BUSINESS		2	1760+98.00		1+31.82	
AND IL 10	PASSING & DRIVING	2	136+50.00	-	139+50.00	į
NB I-55 BUS/LINCOLN PARKWAY	PASSING & DRIVING	2	1758+06.00	-	1760+98.00	
	EX OUTSIDE SHOULDER	2	1758+06.04	-	1760+96.49	
	EX OUTSIDE SHOULDER	2	1+28.32	-	4+77.42	
	PASSING & DRIVING	VARIABLE	1+32.14	-	2+20.00	
	DRIVING	3	2+20.00	-	9+63.40	
	PASSING & TURN LANES	VARIABLE	2+20.00	-	9+63.40	1
	DRIVING	3	34+18.05	-	37+00.00	
	PASSING	VARIABLE	34+18.05	-	37+00.00	
	NB OUTSIDE SHOULDER	2	35+73.21	_	38+83.15	
	PASSING & DRIVING	VARIABLE	37+00.00	-	38+40.70	
	NB OUTSIDE SHOULDER	2	40+39.40	-	45+15.20	
	PASSING & DRIVING	VARIABLE	41+60.00	-	42+50.00	
	DRIVING	3	42+50.00	-	44+87.14	
	PASSING	VARIABLE	42+50.00	-	44+87.14	
SB I-55 BUS/LINCOLN PARKWAY	PASSING & DRIVING	2	1758+06.00	-	1760+98.00	:
	EX OUTSIDE SHOULDER	2	1758+06.04	-	1760+97.78	
	PASSING & DRIVING	VARIABLE	1+32.14	-	2+20.00	
	DRIVING	2	2+20.00	-	9+63.40	
	PASSING & TURN LANES	VARIABLE	2+20.00	-	9+63.40	:
	DRIVING	2	34+18.05	-	37+00.00	
	PASSING & TURN LANE	VARIABLE	34+18.05	-	37+00.00	
	SB OUTSIDE SHOULDER	2	33+70.56	-	38+48.19	
	PASSING & DRIVING	VARIABLE	37+00.00	-	38+40.70	
	SB OUTSIDE SHOULDER	2	41+06.35	-	44+60.29	
	PASSING & DRIVING	VARIABLE	41+60.00	-	42+50.00	
	DRIVING	2	42+50.00	-	44+87.14	
	PASSING	VARIABLE	42+50.00	-	44+87.14	
EB IL 10/121/WOODLAWN ROAD	PASSING & DRIVING	2	134+19.24	-	136+50.00	:
	PASSING & DRIVING	2	139+50.00	-	143+40.82	1
WB IL 10/121/WOODLAWN ROAD	PASSING & DRIVING	2	134+19.24	-	136+50.00	
	PASSING & DRIVING	2	139+50.00	-	143+40.82	1
5TH STREET INTERSECTION	ALL	2	38+00.00	-	42+50.00	4
SIDE ROAD (EAST)	ALL	1 1/2	5+57.68	-	6+42.63	
SIDE ROAD (WEST)	ALL	1 1/2	5+67.86	-	6+42.57	
	•	•	PPS# 6-0047-	-0000	(HSIP) TOTAL =	2

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
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	PLOT SCALE = 40.0000 '/ in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -

STATE	E OF	ILLINOIS
DEPARTMENT	0F	TRANSPORTATION

			COULDING OF CHANTIFE				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHE
	SCHEDULE OF QUANTITIES				7706	•	LOGAN	343	72		
								CONTRACT	NO.	72E0	
	SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FED. A	ID PROJECT		
Τ	• (22)RS-6, MED & (23) RS-5										

		X0326694	PLUG E	XISTIN	IG STORM SEV	WERS				
		F	PPS# 6-00	477-0	000 (HSIP)					
ROADWAY	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	LENGTH	PIPE AREA	CU YD
155 BUSINESS/LINCOLN PARKWAY	5+64.83	1.3	LT	-	6+39.11	0.8	RT	75.0	0.79	2.2
							PPS# 6-	00477-0000	(HSIP) TOTAL =	2.2

	X0327371 - PLUG EXISTI	NG PIPE			
	PPS# 6-00625-0000 (S	TP)			
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	CU YD
155 BUSINESS/LINCOLN PARKWAY	12" STORM SEWER	6+64.04	1.6	RT	0.3
	12" STORM SEWER	6+77.70	1.8	RT	0.3
	12" RCP	16+80.53	2.2	LT	0.3
	15" CONC	31+65.16	0.6	RT	0.4
	15" CONC	31+77.20	0.8	RT	0.4
	15" CONC	50+33.35	0.1	LT	0.4
	15" CONC	50+46.51	0.4	RT	0.4
	15" CONC	54+68.10	0.1	RT	0.4
	15" CONC	62+14.54	0.1	RT	0.4
	15" CONC	62+26.99	0.1	LT	0.4
		PPS# 6-0062	5-0000 (ST	P) TOTAL =	3.7

Х4	402020 CONCRETI	E MEDIAN SU	RFACE	REMOVAL		
	PPS # 6-	00477-0000 (H	SIP)			
ROADWAY	STATION	SIDE	то	STATION	SIDE	SQ FT
155 BUSINESS/LINCOLN PARKWAY	1760+35.98	CENTER	-	1761+64.77	CENTER	631
	0+77.66	CENTER	-	5+72.29	CENTER	3521
	6+25.95	CENTER	-	8+25.26	CENTER	1526
	34+80.22	CENTER	-	39+02.23	CENTER	4435
	40+30.40	CENTER	-	44+48.94	CENTER	4213
IL 10/121/WOODLAWN ROAD	135+71.11	CENTER	-	137+08.62	CENTER	942
			PF	PS# 6-00477-0000	(HSIP) TOTAL =	15268

	Z0023	3500 - FILLING	EXISTING (CULVERT					
		PPS# 6-0062	5-0000 (STP)					
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	то	STATION	OFFSET	SIDE	CU YD
I55 BUSINESS/LINCOLN PARKWAY	24" CONC	25+08.15	1.7	RT	-	25+08.15	44.3	RT	5.1
I55 BUSINESS/LINCOLN PARKWAY	24" CMP	47+82.07	45.2	RT	-	48+10.15	2.1	RT	6.3
I55 BUSINESS/LINCOLN PARKWAY	24" CMP	48+13.56	3.2	LT	-	48+41.94	46.7	LT	6.3
I55 BUSINESS/LINCOLN PARKWAY	30" CMP	65+83.65	92.5	LT	-	66+10.62	60.2	LT	8.0
155 BUSINESS/LINCOLN PARKWAY	24" RCP	66+22.97	45.0	LT	-	66+96.81	43.9	LT	13.7
						PPS# 6-0062	5-0000 (STF) TOTAL =	39.4

X6015010 - REMOVE AND REPLACE	CONCRETE HEADWA	ALLS FOR P	IPE DRAIN	IS
PPS# 6-0	0625-0000 (STP)			
ROADWAY	STATION	OFFSET	SIDE	EACH
155 BUSINESS/LINCOLN PARKWAY	31+77.18	103.3	LT	1.0
	32+20.37	102.1	LT	1.0
	PPS# 6-0062	25-0000 (STI	P) TOTAL =	2.0

			SIGNIN	G SCHEDULE POSTV	ILLE DRIVE (FRONTAG	E ROAD)	
				PPS# 6-006	25-0000 (STP)		
STA	O/S	Direction Sign Facing	Existing Sign	REMOVE SIGN PANEL TYPE 1, SQ FT	RELOCATE SIGN PANEL TYPE 1, SQ FT	Mounting	TELESCOPING STEEL SIGN SUPPORT
10+80	14 FT LT	South	R1-2 36x36 Yield	5	5	GROUND MOUNT - TELESPAR	14
12+51	19 FT RT	North	R2-1 (35) 24x36 35 MPH	6	6	GROUND MOUNT - TELESPAR	14
24+45	19 FT RT	North	R2-1 (35) 24x36 35 MPH	6	6	GROUND MOUNT - TELESPAR	14
		PP.	S# 6-00625-0000 (STP) TOTAL =	17	17		42

					JLE IL 10/121/WOODL					
		1		PPS# 6-00	0625-0000 (STP - SIDEWA	ALK)	T .		ı	
STA	O/S	DIRECTION SIGN FACING	EXISTING SIGN	REMOVE SIGN PANEL ASSEMBLY TYPE A, EACH	REMOVE SIGN PANEL ASSEMBLY TYPE B, EACH	RELOCATE SIGN PANEL ASSEMBLY TYPE A, EACH	RELOCATE SIGN PANEL ASSEMBLY TYPE B, EACH	MOUNTING	TELESCOPING STEEL SIGN SUPPORT, FT	BASE FOR TELESCOPING STEEL SIGN SUPPORT, EACH
141+01	50 FT LT	WEST	SPECIAL GREEN/WHITE 48X12 "^ MASON CITY"							
141+01	50 FT LT	WEST	SPECIAL GREEN/WHITE 48X12 "^ PEORIA"	1						
141+01	50 FT LT	WEST	SPECIAL GREEN/WHITE 48X18 "<- CORRECTIONAL CENTER"	1						
141+01	50 FT LT	WEST	M4-5 (BL) 24X12 TO	1						
141+01	50 FT LT	WEST	M1-1 (55) 24X24 INTERSTATE 55 SHIELD	1	1		4	GROUND MOUNT - TELESPAR	20	
141+01	50 FT LT	WEST	M1-1 (155) 30X24 INTERSTATE 155 SHIELD	1	1		1	GROUND MOUNT - TELESPAR	28	
141+01	50 FT LT	WEST	M6-3 (BL) 21X15 UP ARROW							
141+01	50 FT LT	WEST	M3-1 24X12 NORTH	1						
141+01	50 FT LT	WEST	M1-I100 (121) 30X24 IL 121	1						
141+01	50 FT LT	WEST	M6-3 21X15 UP ARROW							
			PPS# 6-00625-0000 (STP) TOTAL =	0	1	0	1		28	0
				PPS	# 6-00477-0000 (HSIP)					
STA	O/S	DIRECTION SIGN FACING	EXISTING SIGN	REMOVE SIGN PANEL ASSEMBLY TYPE A, EACH	REMOVE SIGN PANEL ASSEMBLY TYPE B, EACH	RELOCATE SIGN PANEL ASSEMBLY TYPE A, EACH	RELOCATE SIGN PANEL ASSEMBLY TYPE B, EACH	MOUNTING	TELESCOPING STEEL SIGN SUPPORT, FT	BASE FOR TELESCOPING STEEL SIGN SUPPORT, EACH
137+27	5 FT LT	EAST	R4-7 24X30 KEEP RIGHT	1		4		CUREACE MOUNT TELECRAR	4.4	1
137+27	5 FT LT	EAST	OM 1-3 18X18 OBJECT MARKER	1		1		SURFACE MOUNT - TELESPAR	14	1
138+49	5 FT RT	WEST	R4-7 24X30 KEEP RIGHT	4		4		CUREACE MOUNT TELECRAR	4.4	1
138+49	5 FT RT	WEST	OM 1-3 18X18 OBJECT MARKER	1		1		SURFACE MOUNT - TELESPAR	14	1
140+36	5 FT RT	NORTH	R6-2 (LEFT) 30X36 ONE WAY	1				REMOVAL ONLY		
141+24	4 FT RT	EAST	R3-I100 24X24 LEFT TURN LANE							
141+24	4 FT RT	EAST	M6-2 (LEFT) 21X15 ANGLE LEFT ARROW	1						
141+24	4 FT RT	EAST	D9-2 24X24 HOSPITAL]	1		1	SURFACE MOUNT - TELESPAR	14	1
141+24	4 FT RT	EAST	M6-2 (LEFT) (BL) 21X15 ANGLE LEFT ARROW]	1			SUNFACE MIDDINI - TELESPAR	14	
141+24	4 FT RT	WEST	R3-I100 24X24 LEFT TURN LANE]						
141+24	4 FT RT	WEST	M6-2 (LEFT) 21X15 ANGLE LEFT ARROW]						<u> </u>
			PPS# 6-00477-0000 (HSIP) TOTAL =	3	1	2	1		42	3

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
pw:\\IL084EBIDINTEG.:ll:no:s.gov:PWID0T\Do	cuments\IDOT Offices\District 6\Projects\D67	PE DRXXXXX Data\€ADsheets\D672E02-sht-05-Sc	- REVISED -
	PLOT SCALE = 40.0000 ' / 10.	CHECKED -	REVISED -
Default	PLOT DATE = 11/18/2016	DATE -	REVISED -

STATE OF	ILLINOIS
DEPARTMENT OF	TRANSPORTATION

						F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		SCHEDUL	E OF QUA	ANTITIES		7706	•	LOGAN	343	73
								CONTRACT	NO.	72E02
SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FED. A	ID PROJECT		

(22)RS-6,	MED	&	(23)	RS-5	

						625-0000 (STP)	NESS LOOP/LINCOLN PA					1
STA	O/S	DIRECTION SIGN FACING	EXISTING SIGN	REMOVE SIGN PANEL TYPE 1, SQ FT	REMOVE SIGN PANEL TYPE 2, SQ FT	PANEL	REMOVE SIGN PANEL ASSEMBLY TYPE B, EACH	1 DANEL TYPE 2 PANEL	RELOCATE SIGN PANEL ASSEMBLY TYPE B, EACH	Mounting	TELESCOPING STEEL SIGN SUPPORT, FT	BASE FOR TELESCOPING STEEL SIGN SUPPORT, EAC
2+34	48 FT RT	NORTH	XM-21 24X30 HISTORIC IL-US 66	5			5			GROUND MOUNT - TELESPAR	14	
3+70	51 FT LT	SOUTH	SPECIAL GREEN/WHITE 48X12 "<- MASON CITY"									
3+70	51 FT LT	SOUTH	SPECIAL GREEN/WHITE 48X12 "CLINTON ->"									
3+70	51 FT LT	SOUTH	SPECIAL GREEN/WHITE48X24 "LINCOLN CHRISTIAN UNIVERSITY ->"	4						00011115 1401111T TELEGRAD		
3+70 3+70	51 FT LT 51 FT LT	SOUTH SOUTH	M4-5 (BL) 24X12 TO M1-1 (55) 24X24 INTERSTATE 55 SHIELD	\dashv			1		1	GROUND MOUNT - TELESPAR	28	
3+70	51 FT LT	SOUTH	M1-1 (155) 30X24 INTERSTATE 155 SHIELD	7								
3+70	51 FT LT	SOUTH	M6-1 (BL) 21-15 LEFT ARROW									
4+39	45 FT RT	NORTH	R2-1 30X36 45 MPH		14			14		GROUND MOUNT - TELESPAR	14	
4+39	45 FT RT	SOUTH	R5-1A 36X24 WRONG WAY		14			17		GROOMS WOOM TELESTAN	17	
5+30	45 FT RT	NORTH	D4-I100 30X24 FRONTAGE ROAD ENTRANCE	4			1		1	CDOLIND MOUNT TELECOAD	1.4	
5+30 5+30	45 FT RT 45 FT RT	NORTH SOUTH	M6-4 (GR) 21X15 TWO WAY ARROW R5-1 36X36 DO NOT ENTER	\dashv			1		1	GROUND MOUNT - TELESPAR	14	
5+35	65 FT LT	NORTH	R1-2 36X36 VIELD	5			5			GROUND MOUNT - TELESPAR	14	
5+55	45 FT LT	EAST	R1-1 36X36 STOP				-					
5+55	45 FT LT	EAST	R6-2 (LEFT) 30X36 ONE WAY				1		1	GROUND MOUNT - TELESPAR	14	
5+55	45 FT LT	WEST	R6-2 (LEFT) 30X36 ONE WAY									
6+35	45 FT RT	WEST	R1-1 36X36 STOP	4						CDOLIND MOUNT TELECOMS	1.4	
6+35 6+35	45 FT RT 45 FT RT	WEST EAST	R6-2 (LEFT) 30X36 ONE WAY R6-2 (LEFT) 30X36 ONE WAY	-			1		1	GROUND MOUNT - TELESPAR	14	
6+50	79 FT RT	NORTH	OM 4-3 18X18 OBJECT MARKER	2			2	+ + + + + + + + + + + + + + + + + + + +	+	GROUND MOUNT - TELESPAR	14	<u> </u>
6+51	84 FT RT	NORTH	R11-I100 36X30 ROAD ENDS	8			8			GROUND MOUNT - TELESPAR	14	1
6+52	89 FT RT	NORTH	OM 4-3 18X18 OBJECT MARKER	2			2			GROUND MOUNT - TELESPAR	14	
6+77	42 FT LT	SOUTH	D4-I100 30X24 FRONTAGE ROAD ENTRANCE						1			
6+77	42 FT LT	SOUTH	M6-4 (GR) 21X15 TWO WAY ARROW	4			1		-	GROUND MOUNT - TELESPAR	14	
6+77 7+60	42 FT LT	NORTH NORTH	R5-1 36X36 DO NOT ENTER					+	+	CPOLIND MOUNT TELECOAR	1.4	
7+69 15+46	43 FT LT 45 FT RT	NORTH NORTH	R5-1A 36X24 WRONG WAY R3-I100A 24X24 RIGHT TURN LANE	6	+		6	+	+	GROUND MOUNT - TELESPAR	14	+
15+46	45 FT RT	NORTH	M6-2 (RIGHT) 21X15 RIGHT ARROW	+								
15+46	45 FT RT	NORTH	D9-2 24X24 HOSPITAL	1					1	GROUND MOUNT - TELESPAR	28	
15+46	45 FT RT	NORTH	M6-2 (RIGHT) (BL) 21X15 RIGHT ARROW									
15+53	42 FT LT	SOUTH	R2-1 30X36 45 MPH	8			8			GROUND MOUNT - TELESPAR	14	
19+73	42 FT LT	WEST	W1-6 (LEFT) 48X24 LEFT ARROW	4						CROUND MOUNT TO TOO		
19+73 19+73	42 FT LT 42 FT LT	WEST EAST	R6-2 (LEFT) 30X36 ONE WAY W1-7 48X24 DOUBLE ARROW	-			1		1	GROUND MOUNT - TELESPAR	14	
24+08	42 FT LT	SOUTH	R3-I100 24X24 LEFT TURN LANE	1					+		+	1
24+08	4 FT LT	SOUTH	M6-2 (LEFT) 21X15 ANGLE LEFT ARROW	1						CUREA CE LACUNITA		
24+08	4 FT LT	SOUTH	D9-2 24X24 HOSPITAL]					1	SURFACE MOUNT - TELESPAR	14	1
24+08	4 FT LT	SOUTH	M6-2 (LEFT) (BL) 21X15 ANGLE LEFT ARROW									
28+96	46 FT RT	NORTH	M2-1 (GR) 21X15 JCT	4		1				GROUND MOUNT - TELESPAR	14	
28+96	46 FT RT	NORTH	M1-2 (55) 24X24 BUS 55 ROUTE SHIELD			_						
32+74 32+74	43 FT RT 43 FT RT	NORTH NORTH	W3-3 36X36 SIGNAL AHEAD SPECIAL YELLOW/BLACK 24X9 "5TH STREET"	-			1		1	GROUND MOUNT - TELESPAR	14	
32+74	43 FT RT	NORTH	BUSINESS LOGO SIGN	1						SUCCIAD MICOIAL - LETERAN	14	
34+84	52 FT RT	NORTH	SPECIAL GREEN/WHITE 48X18 "<- POSTVILLE COURTHOUSE STATE HISTORIC SITE"						 			
34+84	52 FT RT	NORTH	SPECIAL GREEN/WHITE 48X18 "<- COURTHOUSE HISTORIC DISTRICT"									
34+84	52 FT RT	NORTH	SPECIAL GREEN/WHITE 48X18 "<- LOGAN COUNTY HEALTH DEPARTMENT"	1								
34+84	52 FT RT	NORTH	I-7 24X24 RAIL STATION	4			1		1	GROUND MOUNT - TELESPAR	28	
34+84	52 FT RT	NORTH NORTH	M6-1 (RIGHT) (GR) 21X15 RIGHT ARROW	4								
34+84 34+84	52 FT RT 52 FT RT	NORTH NORTH	D9-I104 36X18 TOURIST INFORMATION M6-1 (BL) 21-15 LEFT ARROW	+								
34+84	52 FT RT	NORTH	BUSINESS LOGO SIGN	+								
35+32	52 FT LT	SOUTH	R2-1 30X36 45 MPH	8			8			GROUND MOUNT - TELESPAR	14	
37+74	49 FT LT	SOUTH	ADOPT A HIGHWAY ASSEMBLY				1		1	GROUND MOUNT - TELESPAR	14	
40+25	57 FT RT	NORTH	M3-3 (GR) (SOUTH) 24X12 SOUTH PLAQUE									
40+25	57 FT RT	NORTH	M1-2 (55) 24X24 BUS 55 ROUTE SHIELD	4			1		1	GROUND MOUNT - TELESPAR	14	
40+25 42+04	57 FT RT 52 FT RT	NORTH NORTH	XM-21 24X30 HISTORIC IL-US 66 ADOPT A HIGHWAY ASSEMBLY	1			1	+	1	GROUND MOUNT - TELESPAR	14	-
42+04	52 FT KT	SOUTH	SPECIAL GREEN/WHITE 48X18 "<- POSTVILLE COURTHOUSE STATE HISTORIC SITE"	+			<u> </u>	+	+ +	SKOOND WOONT - TELESPAR	14	<u> </u>
42+13	58 FT LT	SOUTH	SPECIAL GREEN/WHITE 48X18 "<- COURTHOUSE HISTORIC DISTRICT"	1								
42+13	58 FT LT	SOUTH	D4-I101 30X18 BUSINESS DISTRICT]						CPOLIND MOUNT TELECOAR	20	
42+13	58 FT LT	SOUTH	M6-1 (GR) (RIGHT) 21X15 RIGHT ARROW				1		1	GROUND MOUNT - TELESPAR	28	
42+13	58 FT LT	SOUTH	D9-I104 36X18 TOURIST INFORMATION (LEFT ARROW)	4								
42+13	58 FT LT	SOUTH	M6-1 (BL) 21-15 LEFT ARROW		CONTINUES	ON NEVT CHEST						1
					CONTINUED	ON NEXT SHEET						
	IISEE	R NAME = whitlingme	DESIGNED - REVISED -				T			F.A.IJ.	SECTION	COUNTY
TEG.:ll:no:s.gc			ojects\D672E0RXYM\Data\6ADsheets\D672E02-sht-05-ScheREM4SEQh -		STATE OF	ILLINOIS		SCHEDULE	OF QUANTITIES	F.A.U. RTE. 7706	SECTION	COUNTY
_		T SCALE = 40.0000 '/ in.	CHECKED - REVISED -	DED 4 D	TMENT OF		FATION			1106	•	CONTRACT

					PP3# 0-U	10625-0000 (STF	·)							
STA	O/S	DIRECTION SIGN FACING	EXISTING SIGN	REMOVE SIGN PANEL TYPE 1, SQ FT			REMOVE SIGN PANEL ASSEMBLY TYPE B, EACH	RELOCATE SIGN PANEL TYPE 1, SQ FT	RELOCATE SIGN PANEL TYPE 2, SQ FT	PANEL	RELOCATE SIGN PANEL ASSEMBLY TYPE B, EACH	MOUNTING	TELESCOPING STEEL SIGN SUPPORT, FT	BASE FOR TELESCOPING STEE SIGN SUPPORT, EAC
43+98	52 FT LT	SOUTH	D9-2 24X24 HOSPITAL											
43+98	52 FT LT	SOUTH	D9-2 24X24 HOSPITAL											
43+98	52 FT LT	SOUTH	M6-3 (BL) 21X15 UP ARROW											
43+98	52 FT LT	SOUTH	I-7 24X24 RAIL STATION	1			1				1	GROUND MOUNT - TELESPAR	28	
43+98	52 FT LT	SOUTH	M6-1 (GR) (RIGHT) 21X15 RIGHT ARROW	1										
43+98	52 FT LT	SOUTH	BUSINESS LOGO SIGN											
43+98	52 FT LT	SOUTH	BUSINESS LOGO SIGN											
45+75	51 FT RT	NORTH	R2-1 30X36 45 MPH	8				8				GROUND MOUNT - TELESPAR	14	
45+88	43 FT LT	SOUTH	W3-3 36X36 SIGNAL AHEAD				1				1	GROUND MOUNT - TELESPAR	14	
45+88	43 FT LT	SOUTH	SPECIAL YELLOW/BLACK 24X9 "5TH STREET"				1				1	GROUND MOUNT - TEELSFAR	14	
55+00	4 FT LT	SOUTH	W10-1 36X36 RR CROSSING	9				9				GROUND MOUNT - TELESPAR	14	
55+00	42 FT LT	SOUTH	W10-1 36X36 RR CROSSING	9				9				GROUND MOUNT - TELESPAR	14	
59+36	48 FT LT	SOUTH	R2-1 30X36 45 MPH	8				8				GROUND MOUNT - TELESPAR	14	
67+78	49 FT LT	EAST	R1-1 36X36 STOP	9				9				GROUND MOUNT - TELESPAR	14	
68+76	73 FT LT	NORTH	R4-7 24X30 KEEP RIGHT			1						REMOVAL ONLY		
68+76	73 FT LT	NORTH	OM 1-3 18X18 OBJECT MARKER			1						REMOVALONLY		
69+93	48 FT LT	NORTH	R5-1 36X36 DO NOT ENTER	9				9				GROUND MOUNT - TELESPAR	14	
70+00	49 FT RT	NORTH	R2-1 30X36 45 MPH	8				8				GROUND MOUNT - TELESPAR	14	
72+08	46 FT LT	SOUTH	SPECIAL 24X24 "LINCOLN POP XXXXX"											
72+08	46 FT LT	SOUTH	R2-1 30X36 45 MPH	1			4					CROUND MOUNT TELECOAD	20	
72+08	46 FT LT	SOUTH	SPECIAL 36X36 "AMTRAK COMMUNITY"	1			1				1	GROUND MOUNT - TELESPAR	28	
72+08	46 FT LT	SOUTH	SPECIAL 48X24 "IDOT CHAMPIONSHIP SIGN"	1										
72+70	88 FT LT	NORTH	R1-1 30X30 STOP	6								REMOVAL ONLY		
76+65	46 FT LT	SOUTH	SPECIAL 48X24 "IDOT CHAMPIONSHIP SIGN"											
76+65	46 FT LT	SOUTH	SPECIAL 48X24 "IDOT CHAMPIONSHIP SIGN"	1			1				1	GROUND MOUNT - TELESPAR	28	
76+65	46 FT LT	SOUTH	SPECIAL 48X24 "IDOT CHAMPIONSHIP SIGN"											
			PPS# 6-00625-0000 (STP) TOTAL =	110	14	2	18	104	14	1	18		588	1
					PPS # 6-004	177-0000 (HSIP)								
										RELOCATE SIGN	RELOCATE SIGN		TELECCODING	BASE FOR
STA	O/S	DIRECTION SIGN FACING	EXISTING SIGN	REMOVE SIGN PANEL TYPE 1, SQ FT	DANIEL TYPE 2	REMOVE SIGN PANEL ASSEMBLY TYPE A, EACH	REMOVE SIGN PANEL ASSEMBLY TYPE B, EACH		RELOCATE SIGN PANEL TYPE 2, SQ FT		PANEL ASSEMBLY TYPE B, EACH	Mounting	TELESCOPING STEEL SIGN SUPPORT, FT	
STA 1761+60	 		EXISTING SIGN R4-7 24X30 KEEP RIGHT	PANEL TYPE 1,	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH	ASSEMBLY TYPE	PANEL TYPE I,	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH	PANEL ASSEMBLY TYPE		STEEL SIGN SUPPORT, FT	
	O/S 25 FT LT 25 FT LT	SIGN FACING		PANEL TYPE 1,	PANEL TYPE 2,	PANEL ASSEMBLY TYPE	ASSEMBLY TYPE	PANEL TYPE I,	PANEL TYPE 2,	ASSEMBLY TYPE	PANEL ASSEMBLY TYPE	Mounting SURFACE MOUNT - TELESPAR	STEEL SIGN	
1761+60	25 FT LT	SIGN FACING SOUTH	R4-7 24X30 KEEP RIGHT	PANEL TYPE 1,	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH	ASSEMBLY TYPE	PANEL TYPE I,	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH 1	PANEL ASSEMBLY TYPE	SURFACE MOUNT - TELESPAR	STEEL SIGN SUPPORT, FT 14	SIGN SUPPORT, EAC
1761+60 1761+60	25 FT LT 25 FT LT	SIGN FACING SOUTH SOUTH	R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER	PANEL TYPE 1,	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH	ASSEMBLY TYPE	PANEL TYPE I,	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH	PANEL ASSEMBLY TYPE		STEEL SIGN SUPPORT, FT	
1761+60 1761+60 0+66	25 FT LT 25 FT LT 5 FT RT	SIGN FACING SOUTH SOUTH NORTH	R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R4-7 24X30 KEEP RIGHT	PANEL TYPE 1,	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH	ASSEMBLY TYPE	PANEL TYPE I,	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH 1	PANEL ASSEMBLY TYPE	SURFACE MOUNT - TELESPAR	STEEL SIGN SUPPORT, FT 14	SIGN SUPPORT, EAC
1761+60 1761+60 0+66 0+66	25 FT LT 25 FT LT 5 FT RT 5 FT RT	SIGN FACING SOUTH SOUTH NORTH NORTH	R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R3-I100 24X24 LEFT TURN LANE	PANEL TYPE 1,	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH 1	ASSEMBLY TYPE	PANEL TYPE I,	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH 1	PANEL ASSEMBLY TYPE	SURFACE MOUNT - TELESPAR SURFACE MOUNT - TELESPAR	STEEL SIGN SUPPORT, FT 14 14	SIGN SUPPORT, EAC
1761+60 1761+60 0+66 0+66 2+74	25 FT LT 25 FT LT 5 FT RT 5 FT RT 1 FT LT	SIGN FACING SOUTH SOUTH NORTH NORTH SOUTH	R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER	PANEL TYPE 1,	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH	ASSEMBLY TYPE	PANEL TYPE I,	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH 1	PANEL ASSEMBLY TYPE	SURFACE MOUNT - TELESPAR	STEEL SIGN SUPPORT, FT 14	SIGN SUPPORT, EAC
1761+60 1761+60 0+66 0+66 2+74 2+74	25 FT LT 25 FT LT 5 FT RT 5 FT RT 1 FT LT	SIGN FACING SOUTH SOUTH NORTH NORTH SOUTH SOUTH SOUTH	R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R3-I100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW	PANEL TYPE 1,	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH 1	ASSEMBLY TYPE	PANEL TYPE I,	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH 1	PANEL ASSEMBLY TYPE	SURFACE MOUNT - TELESPAR SURFACE MOUNT - TELESPAR	STEEL SIGN SUPPORT, FT 14 14	SIGN SUPPORT, EAC
1761+60 1761+60 0+66 0+66 2+74 2+74 2+74	25 FT LT 25 FT LT 5 FT RT 5 FT RT 1 FT LT 1 FT LT 1 FT LT	SIGN FACING SOUTH SOUTH NORTH NORTH SOUTH SOUTH SOUTH NORTH	R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R3-I100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-I100 24X24 LEFT TURN LANE	PANEL TYPE 1,	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH 1	ASSEMBLY TYPE	PANEL TYPE I,	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH 1	PANEL ASSEMBLY TYPE	SURFACE MOUNT - TELESPAR SURFACE MOUNT - TELESPAR SURFACE MOUNT - TELESPAR	STEEL SIGN SUPPORT, FT 14 14 14	SIGN SUPPORT, EAC
1761+60 1761+60 0+66 0+66 2+74 2+74 2+74 2+74	25 FT LT 25 FT LT 5 FT RT 5 FT RT 1 FT LT 1 FT LT 1 FT LT 1 FT LT	SIGN FACING SOUTH SOUTH NORTH NORTH SOUTH SOUTH SOUTH NORTH NORTH	R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R3-I100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-I100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW	PANEL TYPE 1,	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH 1	ASSEMBLY TYPE	PANEL TYPE I,	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH 1	PANEL ASSEMBLY TYPE	SURFACE MOUNT - TELESPAR SURFACE MOUNT - TELESPAR	STEEL SIGN SUPPORT, FT 14 14	SIGN SUPPORT, EAC
1761+60 1761+60 0+66 0+66 2+74 2+74 2+74 2+74 2+74 8+05 8+05	25 FT LT 25 FT LT 5 FT RT 5 FT RT 1 FT LT 1 FT LT 1 FT LT 1 FT LT 1 FT RT 1 FT RT	SIGN FACING SOUTH SOUTH NORTH NORTH SOUTH SOUTH NORTH NORTH NORTH NORTH SOUTH SOUTH SOUTH	R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW	PANEL TYPE 1,	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH 1 1 1	ASSEMBLY TYPE	PANEL TYPE I,	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH 1 1 1	PANEL ASSEMBLY TYPE	SURFACE MOUNT - TELESPAR SURFACE MOUNT - TELESPAR SURFACE MOUNT - TELESPAR SURFACE MOUNT - TELESPAR	STEEL SIGN SUPPORT, FT 14 14 14 14	1
1761+60 1761+60 0+66 0+66 2+74 2+74 2+74 2+74 8+05	25 FT LT 25 FT LT 5 FT RT 5 FT RT 1 FT LT	SIGN FACING SOUTH SOUTH NORTH NORTH SOUTH SOUTH NORTH NORTH NORTH NORTH NORTH SOUTH	R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE	PANEL TYPE 1,	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH 1	ASSEMBLY TYPE	PANEL TYPE I,	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH 1	PANEL ASSEMBLY TYPE	SURFACE MOUNT - TELESPAR SURFACE MOUNT - TELESPAR SURFACE MOUNT - TELESPAR	STEEL SIGN SUPPORT, FT 14 14 14	SIGN SUPPORT, EAC
1761+60 1761+60 0+66 0+66 2+74 2+74 2+74 2+74 2+74 8+05 8+05 35+36	25 FT LT 25 FT LT 5 FT RT 5 FT RT 1 FT LT 1 FT LT 1 FT LT 1 FT LT 1 FT RT 1 FT RT 3 FT RT	SIGN FACING SOUTH SOUTH NORTH NORTH SOUTH SOUTH NORTH NORTH NORTH NORTH SOUTH NORTH SOUTH NORTH NORTH NORTH NORTH NORTH	R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW	PANEL TYPE 1,	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH 1 1 1	ASSEMBLY TYPE	PANEL TYPE I,	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH 1 1 1	PANEL ASSEMBLY TYPE	SURFACE MOUNT - TELESPAR SURFACE MOUNT - TELESPAR SURFACE MOUNT - TELESPAR SURFACE MOUNT - TELESPAR	STEEL SIGN SUPPORT, FT 14 14 14 14	1 1 1 1
1761+60 1761+60 0+66 0+66 2+74 2+74 2+74 2+74 2+74 8+05 8+05 35+36	25 FT LT 25 FT LT 5 FT RT 5 FT RT 1 FT LT 1 FT LT 1 FT LT 1 FT LT 1 FT RT 1 FT RT 3 FT RT 3 FT RT	SIGN FACING SOUTH SOUTH NORTH NORTH SOUTH SOUTH NORTH NORTH NORTH NORTH SOUTH SOUTH NORTH SOUTH NORTH SOUTH	R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE	PANEL TYPE 1, SQ FT	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH 1 1 1	ASSEMBLY TYPE	SQ FT	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH 1 1 1	PANEL ASSEMBLY TYPE	SURFACE MOUNT - TELESPAR	\$TEEL SIGN SUPPORT, FT 14 14 14 14 14 14	1 1 1 1
1761+60 1761+60 0+66 0+66 2+74 2+74 2+74 2+74 8+05 8+05 35+36 42+86	25 FT LT 25 FT LT 5 FT RT 5 FT RT 1 FT LT 1 FT LT 1 FT LT 1 FT LT 1 FT RT 1 FT RT 3 FT RT 3 FT RT 4 FT RT	SIGN FACING SOUTH SOUTH NORTH NORTH SOUTH SOUTH NORTH NORTH NORTH NORTH SOUTH SOUTH NORTH NORTH NORTH NORTH NORTH NORTH NORTH NORTH	R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW W10-1 36X36 RR CROSSING	PANEL TYPE 1, SQ FT	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH 1 1 1 1	ASSEMBLY TYPE	SQ FT	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH 1 1 1 1 1	PANEL ASSEMBLY TYPE	SURFACE MOUNT - TELESPAR GROUND MOUNT - TELESPAR GROUND MOUNT - TELESPAR	STEEL SIGN SUPPORT, FT 14 14 14 14 14 14 14 14 14 1	1 1 1 1
1761+60 1761+60 0+66 0+66 2+74 2+74 2+74 2+74 8+05 8+05 35+36 35+36 42+86 44+02	25 FT LT 25 FT LT 5 FT RT 5 FT RT 1 FT LT 1 FT LT 1 FT LT 1 FT RT 1 FT RT 3 FT RT 3 FT RT 4 FT RT 48 FT RT 5 FT LT	SIGN FACING SOUTH SOUTH NORTH NORTH SOUTH SOUTH NORTH NORTH NORTH NORTH SOUTH SOUTH NORTH	R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW W10-1 36X36 RR CROSSING W10-1 36X36 RR CROSSING R3-1100 24X24 LEFT TURN LANE	PANEL TYPE 1, SQ FT	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH 1 1 1	ASSEMBLY TYPE	SQ FT	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH 1 1 1	PANEL ASSEMBLY TYPE	SURFACE MOUNT - TELESPAR GROUND MOUNT - TELESPAR	\$TEEL SIGN SUPPORT, FT 14 14 14 14 14 14 14 14 14	1 1 1 1
1761+60 1761+60 0+66 0+66 2+74 2+74 2+74 2+74 8+05 8+05 35+36 35+36 42+86 42+86	25 FT LT 25 FT LT 5 FT RT 5 FT RT 1 FT LT 1 FT LT 1 FT LT 1 FT LT 1 FT RT 1 FT RT 3 FT RT 3 FT RT 4 FT RT	SIGN FACING SOUTH SOUTH NORTH NORTH SOUTH SOUTH NORTH NORTH NORTH NORTH SOUTH SOUTH NORTH NORTH NORTH NORTH NORTH NORTH	R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R4-7 24X30 KEEP RIGHT OM 1-3 18X18 OBJECT MARKER R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW R3-1100 24X24 LEFT TURN LANE M6-2 (LEFT) 21X15 ANGLE LEFT ARROW W10-1 36X36 RR CROSSING W10-1 36X36 RR CROSSING	PANEL TYPE 1, SQ FT	PANEL TYPE 2,	PANEL ASSEMBLY TYPE A, EACH 1 1 1 1	ASSEMBLY TYPE	SQ FT	PANEL TYPE 2,	ASSEMBLY TYPE A, EACH 1 1 1 1 1	PANEL ASSEMBLY TYPE	SURFACE MOUNT - TELESPAR GROUND MOUNT - TELESPAR GROUND MOUNT - TELESPAR	STEEL SIGN SUPPORT, FT 14 14 14 14 14 14 14 14 14 1	1 1 1 1

SIGNING SCHEDULE LINCOLN PARKWAY AND I-55 BUSINESS LOOP/LINCOLN PARKWAY

		19+88	28 FT LT	SOUTH	R1-1 30x30 STOP	1			REMOVAL ONLY									
				PPS# (6-00625-0000 (STP) TOTAL =	1												
51.55	Luces wife	DECIONED		DEWISED											I F A II			TOTAL SHEET
FILE NAME =	USER NAME = whitlingme	DESIGNED	-	REVISED	-										RTF.	SECTION	COUNTY	SHEETS NO.
pw:\\ILØ84EBIDINTEG.:111:no:s.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67	2E 0012:X00W100 Data\	\€ADsheets\D6728	02-sht-05-Sche RE14SE	n -	STATE	OF ILLINOIS			S	CHEDULI	E OF QU	ANTITIES		7706	•	LOGAN	343 75
	PLOT SCALE = 40.0000 '/ in.	CHECKED	-	REVISED	-	DEPARTMENT	OF TRANSPORTATION	N									CONTRAC	T NO. 72E02
Default	PLOT DATE = 11/18/2016	DATE	-	REVISED	-				SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS I	ED. AID PROJECT	

SIGNING SCHEDULE STRINGER AVENUE PPS# 6-00625-0000 (STP)

REMOVE SIGN PANEL TYPE 1, SQ FT 1, SQ FT

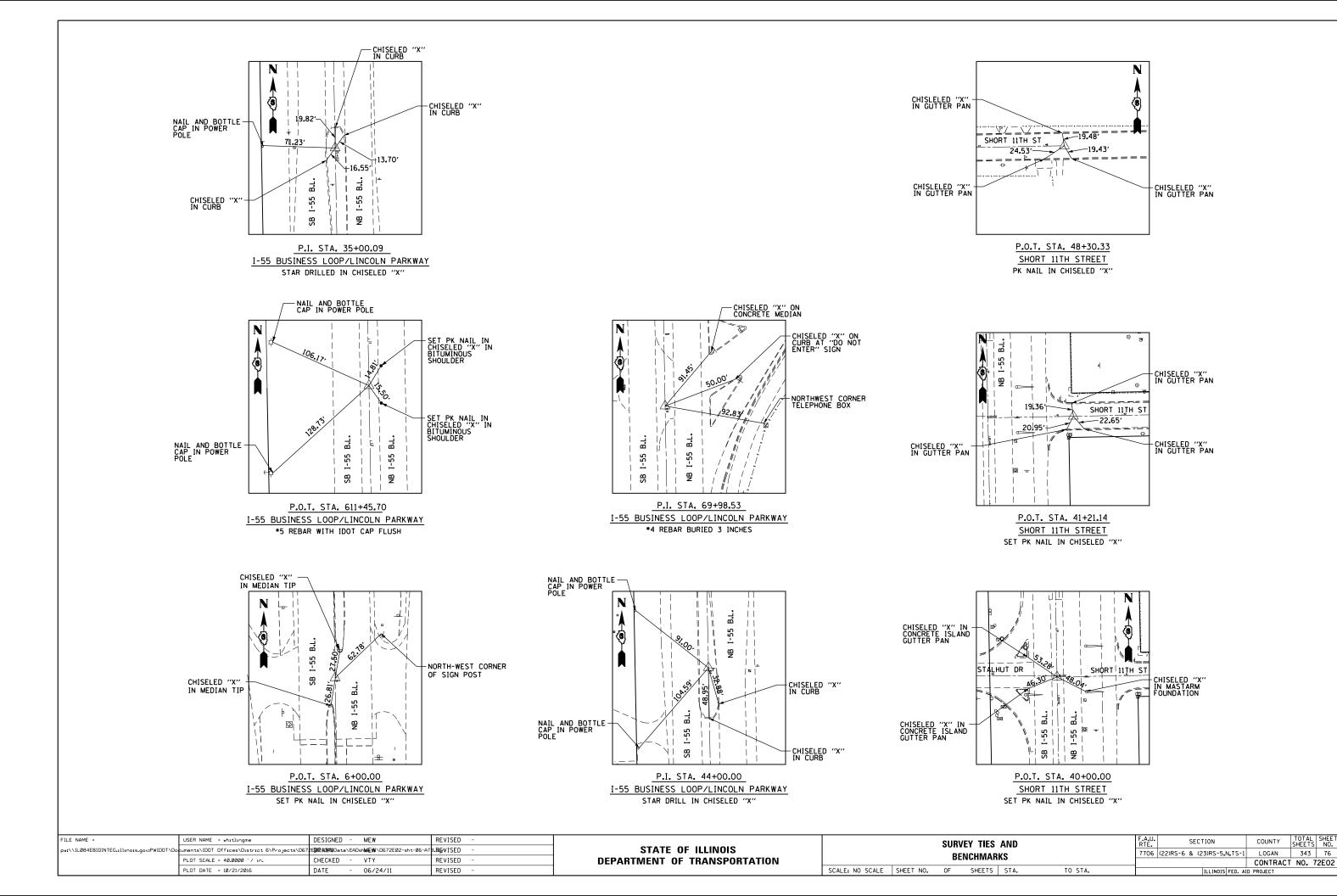
DIRECTION SIGN FACING

EXISTING SIGN

STA

TELESCOPING STEEL SIGN SUPPORT

MOUNTING



CHISELED "X" IN TOP OF CURB I-55 SB 貿 SHORT 11TH ST CHISELED "X" IN MASTARM FOUNDATION -CHISELED "X" IN TOP OF CURB P.O.T. STA. 23+69.28 POSTVILLE DRIVE PK NAIL SET IN CHISELED "X" -CHISELED "X" IN TOP OF CURB ___ 1-55 SB 1-55 罗 -NOTCH IN 10TH H POST -NAIL AND BOTTLE CAP IN POWER POLE 42.62 CHISELED "X" IN MASTARM FOUNDATION -CHISELED "X" IN TOP OF CURB - CHISELED "X" IN CONCRETE ENTRANCE SHORT 11TH ST P.I. STA. 22+45.44 P.I. STA. 37+18.97 POSTVILLE DRIVE POSTVILLE DRIVE PK NAIL IN CHISELED "X" PK NAIL IN CHISELED "X" -SOUTHWEST CORNER OF YIELD SIGN POST SHORT 11TH ST CHISELED "X" IN MASTARM FOUNDATION CHISELED "X" NORTHEAST CORNER OF 35 MPH SIGN POST SOUTHEAST CORNER OF NO OUTLET SIGN POST SOUTH INSIDE CORNER OF CONCRETE HEADWALL OSTVILLE DRIV ______ ______ I-55 B.L. B.L. _-I-55 SB. B. P.O.T. STA. 10+00.00 P.I. STA. 24+65.41 POSTVILLE DRIVE POSTVILLE DRIVE PK NAIL IN CHISELED "X" PK NAIL IN CHISELED "X"

BENCHMARKS

BENCHMARK 17

CHISELED SOUARE ON EAST HEADWALL
OF 2' X 2' BOX CULVERT FOR BUSINESS
55. 0.3 MILE SOUTH OF ILLINOIS 10.
STATION 16+80.60 @ 44.10' LT
ELEVATION = 581.040

BENCHMARK 18

SPIKE IN LIGHT POLE ON EAST SIDE
OF BUSINESS 55, IN FRONT OF
CHRISTIAN HOME OFFICE.
STATION 24+37.30 e 118.10' LT
ELEVATION = 588.123

BENCHMARK 19

CHISELED SOUARE ON EAST HEADWALL OF 2' X 2' BOX CULVERT FOR BUSINESS 55, 0.65 MILE SOUTH OF ILLINOIS 10.
STATION 31+71.10 @ 44.40' LT ELEVATION = 591.451

BENCHMARK 20

RAIL ROAD SPIKE IN POWER POLE EAST OF BUSINESS 55 AND SOUTH OF 5TH STREET ROAD, 0.8 MILE SOUTH OF ILLINOIS 10. ELEVATION = 595.148

BENCHMARK 21

CHISELED SQUARE ON EAST HEADWALL
OF 2' X 2' BOX CULVERT FOR BUSINESS
55, 1.05 NUKE SOUTH OF ILLINOIS 10.
STATION 54+61.50 @ 50.10' LT
ELEVATION = 594.325

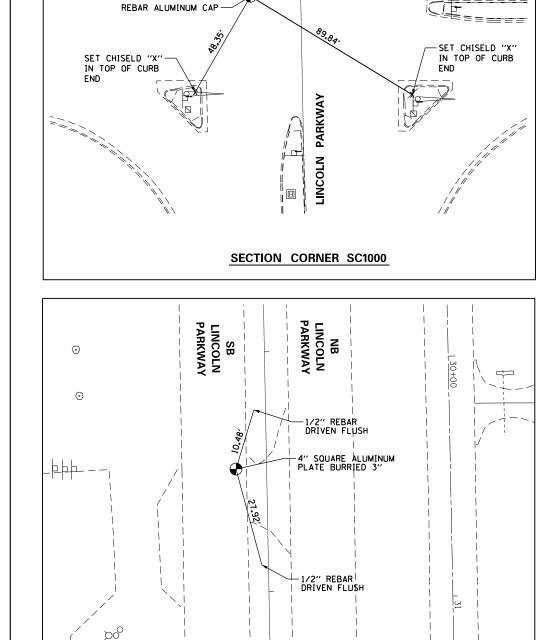
BENCHMARK 22

CHISELED SOUARE ON EAST HEADWALL
OF 24" CULVERT FOR BUSINESS 55.
1.25 MILE SOUTH OF ILLINOIS 10.
STATION 66+22.64 @ 45.50' LT
ELEVATION = 596.681

FILE NAME =	USER NAME = whitlingme	DESIGNED -	MEW	REVISED	-
pw:\\IL084EBIDINTEG.:ll:nois.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67	PE DRAMAN Data\EAD)sh ME# \D672E02-sht-06-AT	в .R GEVISED	-
	PLOT SCALE = 40.0000 '/ in.	CHECKED -	VTY	REVISED	-
	PLOT DATE = 10/21/2016	DATE -	06/24/11	REVISED	-

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

			SUR	VEY TIES	AND		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ı			RI	ENCHMAR	κc		7706	(22)RS-6 & (23)RS-5,N,TS-1	LOGAN	343	77
ı				LINGIIIVIAII	NO .				CONTRACT	NO.	72E02
ı	SCALE: NO SCALE	SHEET NO.	OF	SHEETS	STA.	TO STA.		ILLINOIS FED. AI	D PROJECT		



SECTION CORNER SC1001

uments\IDOT Offices\District 6\Projects\D67**2EDRXNAN**Data\EADsh**WE**\D672E02-sht-06-AT

USER NAME = whitlingme

PLOT DATE = 10/21/2016

DESIGNED - MEW

VTY

06/24/11

CHECKED

DATE

REVISED

REVISED

REVISED

B.R.F.VISED

- SET CHISELD "X" IN TOP OF CURB END

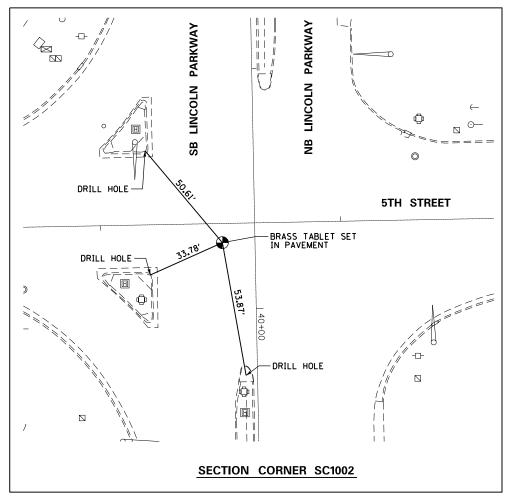
IL 10/121/WOODLAWN RD

SET CHISELD "X" IN TOP OF CURB END

IL 10/121/WOODLAWN RD

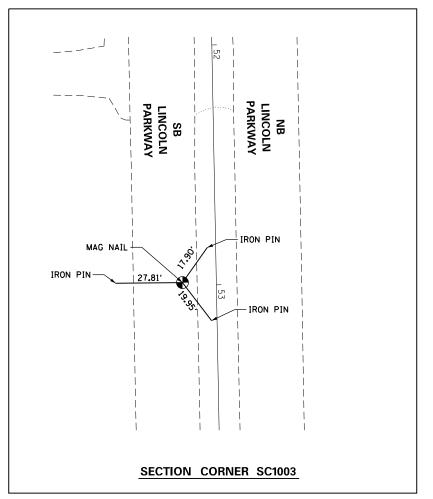
FILE NAME =

ow:\\IL084EBIDINTEG.:ll:nois.gov:PWIDOT\D



STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION



 F.A.U. RTE.
 SECTION
 COUNTY
 TOTAL SHEETS NO.

 7706
 (22)RS-6
 & (23)RS-5,N,TS-1
 LOGAN
 343
 78

 CONTRACT NO. 72EO2

				SEC	CTION CORNE	RS		
POINT	NORTHING	EASTING	ADJUSTMENT	STATION	OFFSET	DESCRIPTION	TYPE	TIF
SC1000	1,272,281.733	2,514,536.942	NAD83/2007 ADJ	137+65.73	0.062 RT	E 1/4 CORNER SECT 26, T20N, R3W. 3RD P.M	1	3446
SC1001	1,269,633.956	2,514,595,415	NAD83/2007 ADJ	26+48.61	12.756 RT	N.E. CORNER SECT. 35, T20N, R3W,, 3TH P.M.	1	
SC1002	1,268,310.588	2,514,619.861	NAD83/2007 ADJ	39+72.20	13.609 RT	SW COR NW 1/4 NW 1/4 SECT 36, T20N, R3W, 3TH P.M.	1	3109
SC1003	1,266,984.229	2,514,644.593	NAD83/2007 ADJ	52+98.80	14.14 RT	EAST 1/4 CORNER SECT. 35, T20N, R3W, 3RD P.M.	1	

SCALE: NO SCALE SHEET NO. OF SHEETS STA.

SECTION CORNERS

TO STA.

POINT	CURVE		GRID COC	RDINATES						
NAME	NAME		STATION	NORTHING	EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
TM11301			0+00.49	1,272,281.84	2,514,557.84					
TM136			11+45.70	1,271,136.84	2,514,579.52					
TM137			35+00.02	1,268,782.95	2,514,624.40					
138			44+00.00	1,267,883.13	2,514,641.69					
307			69+98.54	1,265,285.06	2,514,690.97					
		PC	75+86.65	1,264,697.06	2,514,702.16					
	103	PI	91+15.85	1,263,168.13	2,514,731.26	43° 36′ 41.60″ RT	3,822.15	1,529.20'	2,909.29	294.56
		PT	104+95.94	1,262,041.07	2,513,697.73					
		CC		1,264,624.33	2,510,880.70					
		PC	112+84.29	1,261,460.03	2,513,164.91					
	100	PI	120+34.99	1,260,906.75	2,512,657.54	10° 05′ 14.07′′ LT	8,505,95'	750.70′	1,497.52	33.06′
		PT	127+81.81	1,260,273.15	2,512,254.92					
		СС		1,255,711.18	2,519,434.03					

POINT	CURVE	GRID	COORDINATES						
NAME	NAME	STATIO	I NORTHING	EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
RK1004		1746+30.	0 1,273,880.40	2,514,509.87					
		PC 1753+29.	0 1,273,181.36	2,514,524.84					
	8	PI 1754+53.	1,273,057.79	2,514,527.48	1° 28′ 58.77″ RT	9,550.10	123.60'	247.19	0.80′
		PRC 1755+76.	39 1,272,934.19	2,514,526.93					
		CC	1,272,976.91	2,504,976.93					
		PRC 1755+76.	39 1,272,934.19	2,514,526.93					
	9	PI 1756+90.	79 1,272,820.29	2,514,526.42	1° 22′ 00.53″ LT	9,548.75	113.90'	227.79′	0.68'
		PT 1758+04.	1,272,706.41	2,514,528.63					
		СС	1,272,891.48	2,524,075.58					
1300		1762+29.	37 1,272,281.80	2,514,536.86					

POINT	CURVE		GRID COC	RDINATES						
NAME	NAME		STATION	NORTHING	EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
H1005			24+61.29	1,270,597.89	2,513,295.53					
		PC	24+61.30	1,270,597.90	2,513,295.53					
	1	PI	31+17.84	1,269,941.47	2,513,307.41	117° 17′ 43.26″ LT	400.00'	656.54′	818.88′	368.79
		PT	32+80.18	1,270,253.05	2,513,885.30					
		CC		1,270,605.14	2,513,695.47					
		PC	32+80.20	1,270,253.05	2,513,885.29					
	2	PI	34+05.20	1,270,312.37	2,513,995.32	28° 20′ 37.10″ RT	495.02'	125.00'	244.88′	15.54
		PT	35+25.08	1,270,312.35	2,514,120.32					
		СС		1,269,817.33	2,514,120.22					
409			40+00,00	1,270,312,25	2,514,595,24					

¢ EXISTI	NG SHORT E	ELEVENTH STREET							
POINT	CURVE	GRID COO	RDINATES						
NAME	NAME	STATION	NORTHING	EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
TM1000		40+00.00	1,270,310.19	2,514,595.28					
14866		41+27.14	1,270,312.59	2,514,722.40					
14793		48+30.33	1,270,325.87	2,515,425.47					

¢ EXISTIN	NG POSTVIL	LE DRIVE (FRONT	AGE ROAD)						
POINT	CURVE	GRID COO	RDINATES						
NAME	NAME	STATION	NORTHING	EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
75		10+00.00	1,265,532.01	2,514,845.20					
AL I2230		10+54.70	1,265,572.92	2,514,808.89					
AL12231 10+85.10 1,265,595.66 2,514,788.71									

¢ EXISTI	NG 5TH STE	REET							
POINT	CURVE	GRID COO	RDINATES						
NAME	NAME	STATION	NORTHING	EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
15004		3+50.53	1,268,300.78	2,513,619.82					
3050		10+49.98	1,268,312.91	2,514,319.17					
305		13+64.18	1,268,318.83	2,514,633.31					
3650	·	16+49.98	1,268,324.21	2,514,919.06				•	
15003		27+63.31	1,268,345.96	2,516,032.18				_	

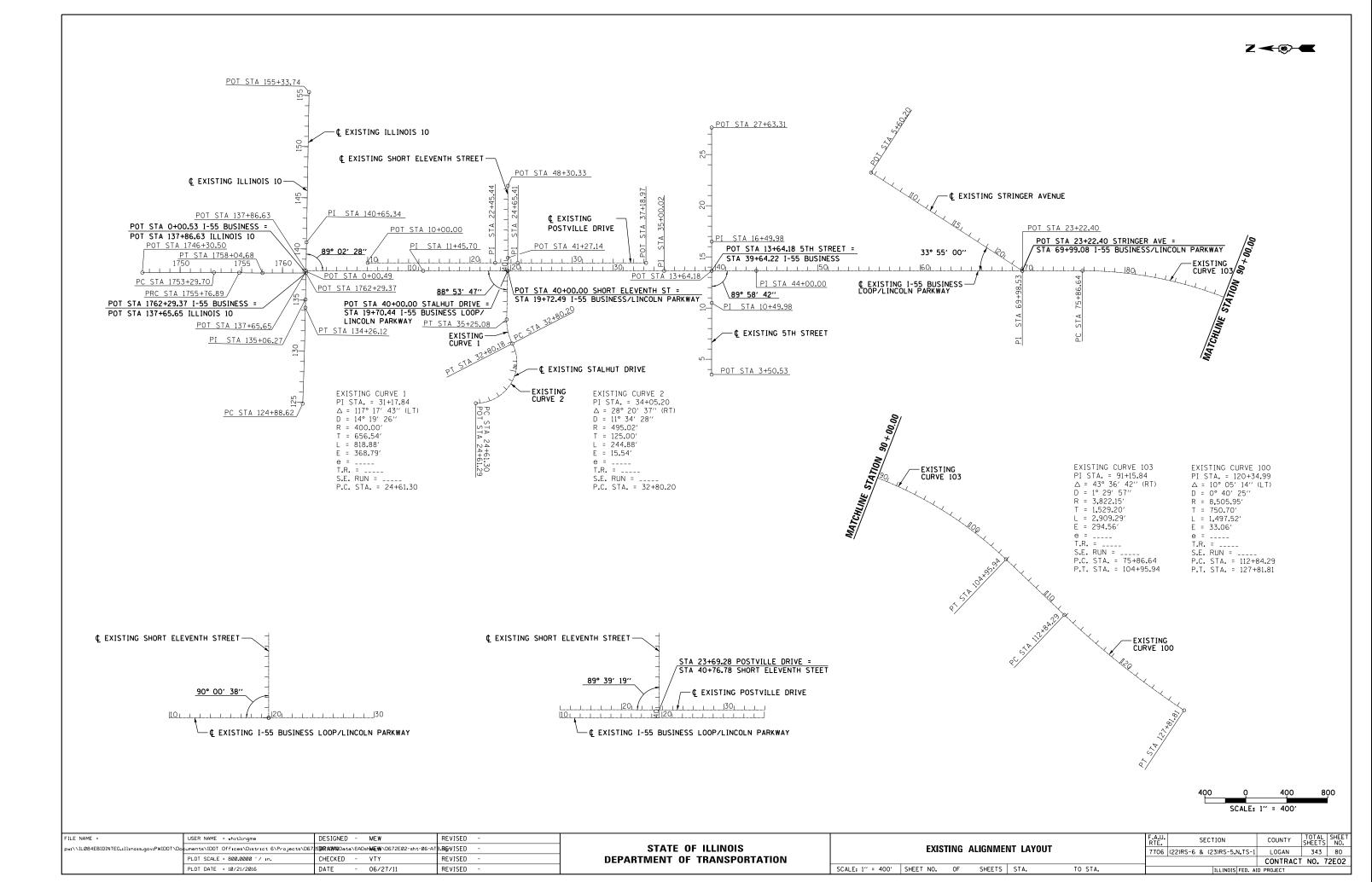
¢ EXISTII	NG STRINGE	R AVENUE							
POINT	CURVE	GRID COO	RDINATES						
NAME	NAME	STATION	NORTHING	EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
660		5+60.20	1,266,780.12	2,515,622.94					
68		23+22.40	1,265,284.52	2,514,690.98					
			1,200,20 ,102				1		

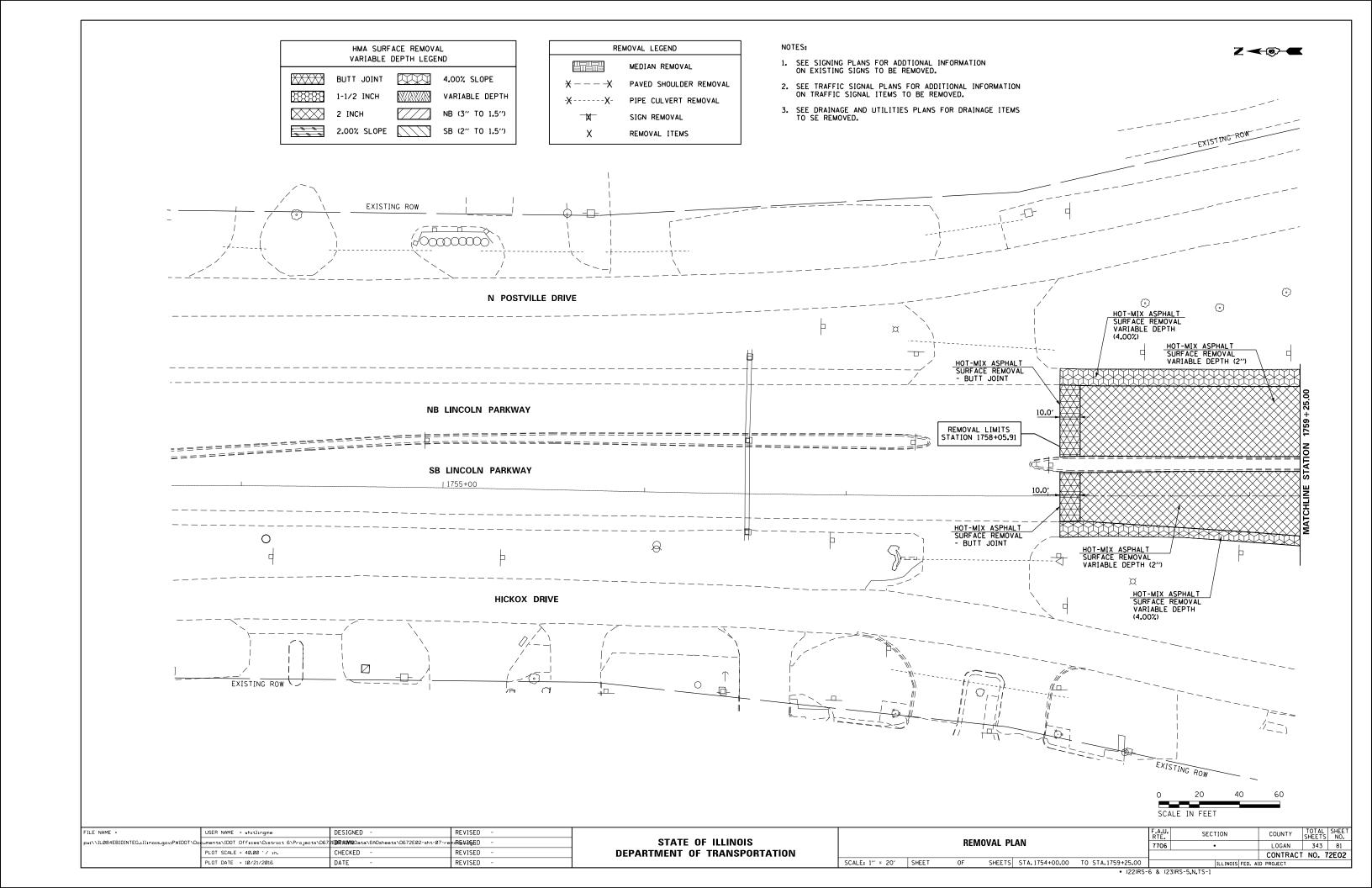
¢ EXISTING IL 10/121/WOODWAWN ROAD												
POINT	CURVE		GRID COO	RDINATES								
NAME	NAME		STATION	NORTHING	EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL		
		PC	124+88.62	1,272,289.29	2,513,259.89							
	C1112	PI	129+57.39	1,272,279.81	2,513,728.56	1° 18′ 25.14″ LT	41,098.23	468.77′	937.50′	2.67'		
		PT	134+26.12	1,272,281.02	2,514,197.33							
		CC		1,313,379.11	2,514,091.16							
161			135+06.27	1,272,281.23	2,514,277.48							
300			137+65.65	1,272,281.79	2,514,536.86							
301			137+86.63	1,272,281.84	2,514,557.84							
162			140+65.34	1,272,282.45	2,514,836.55							
115			155+33.74	1,272,284.81	2,516,304.95							

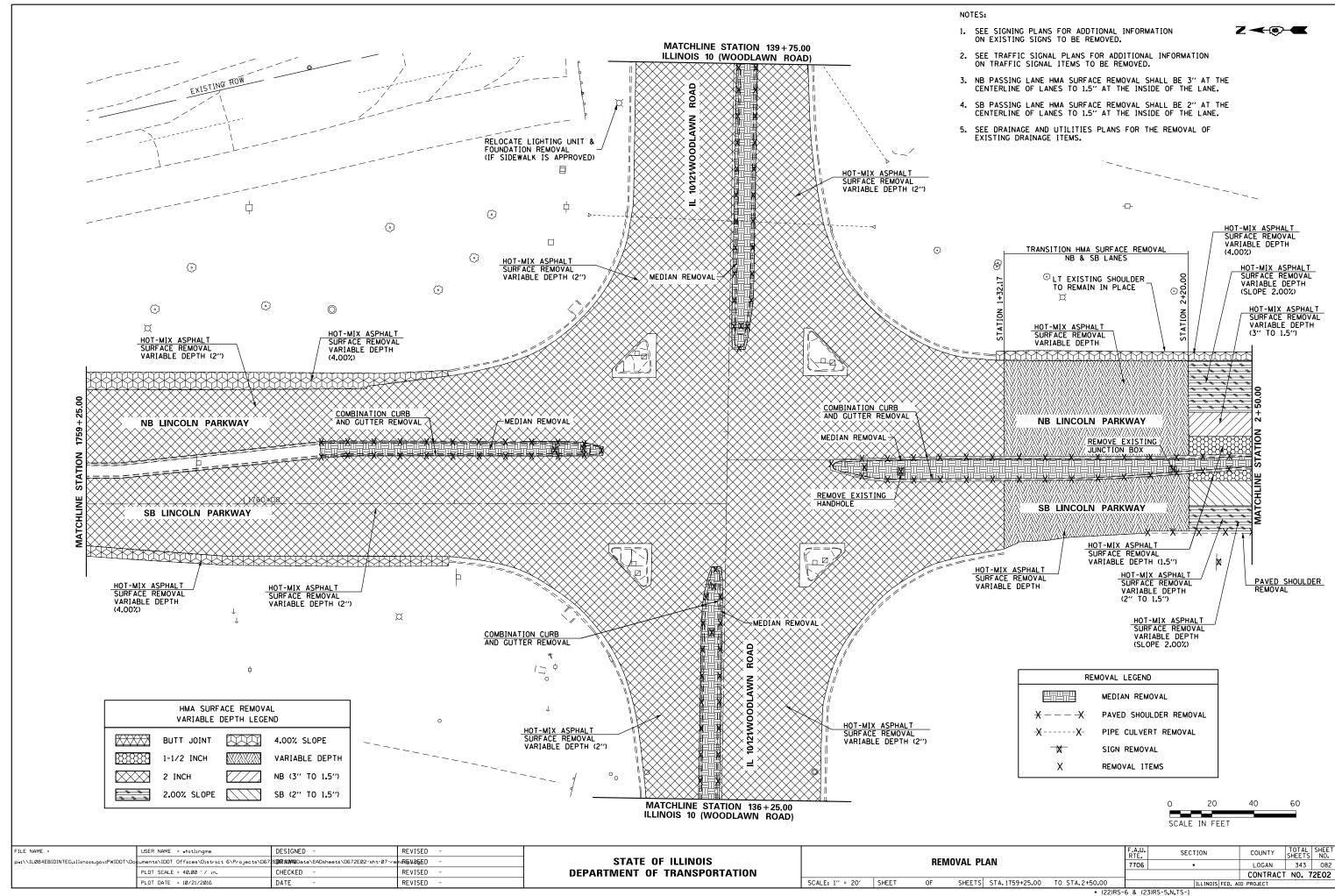
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pw:\\ILØ84EBIDINTEG.:ll:nois.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67	2E 0012/AUWAN OData\EA	Dsh M€₩ \D672E02-sht-06-AT	в .R G⊬VISED	-
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	PLOT DATE = 10/21/2016	DATE -	06/24/11	REVISED	-

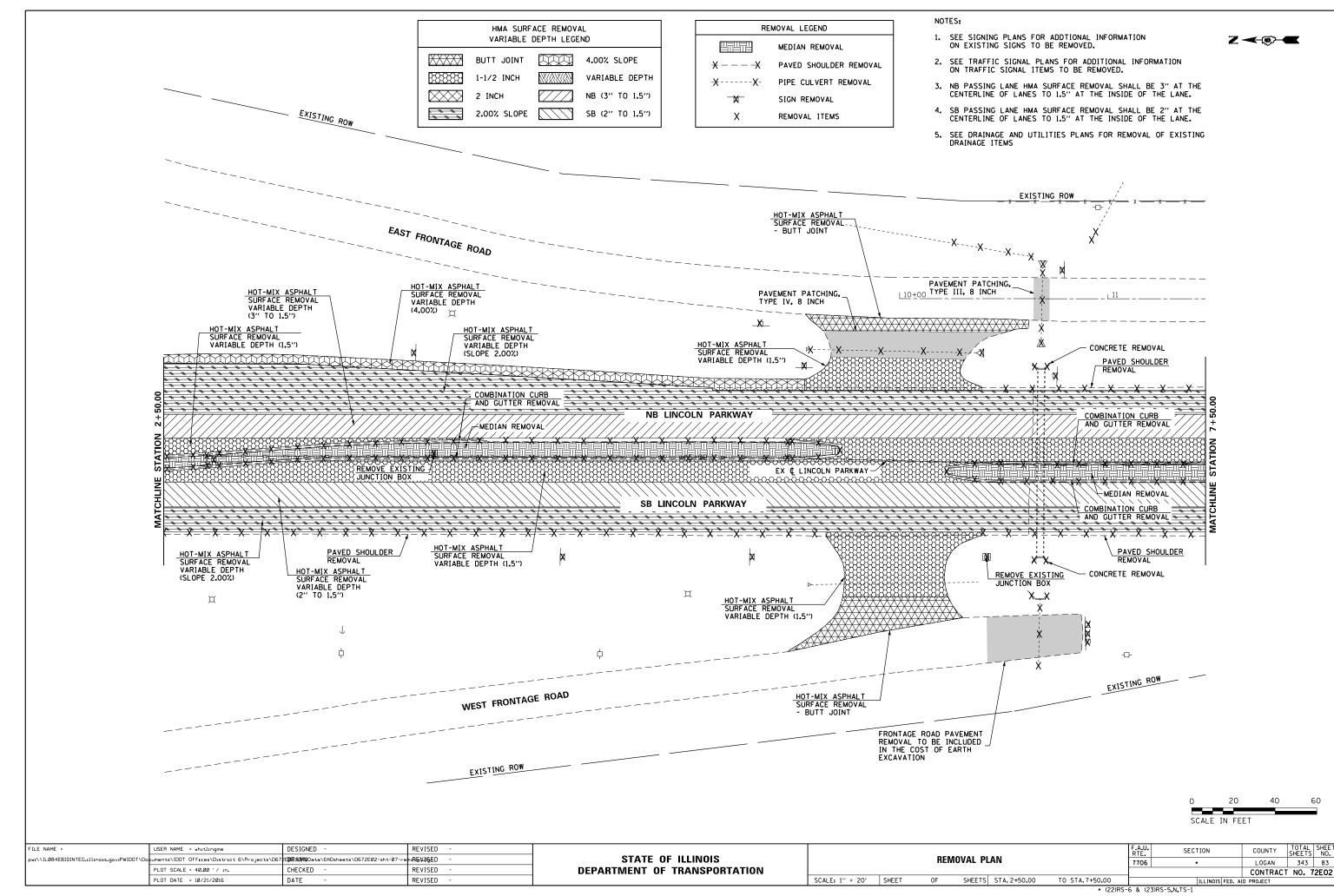
STATE OF ILLINOIS							
DEPARTMENT	OF TRANSPORTATION						

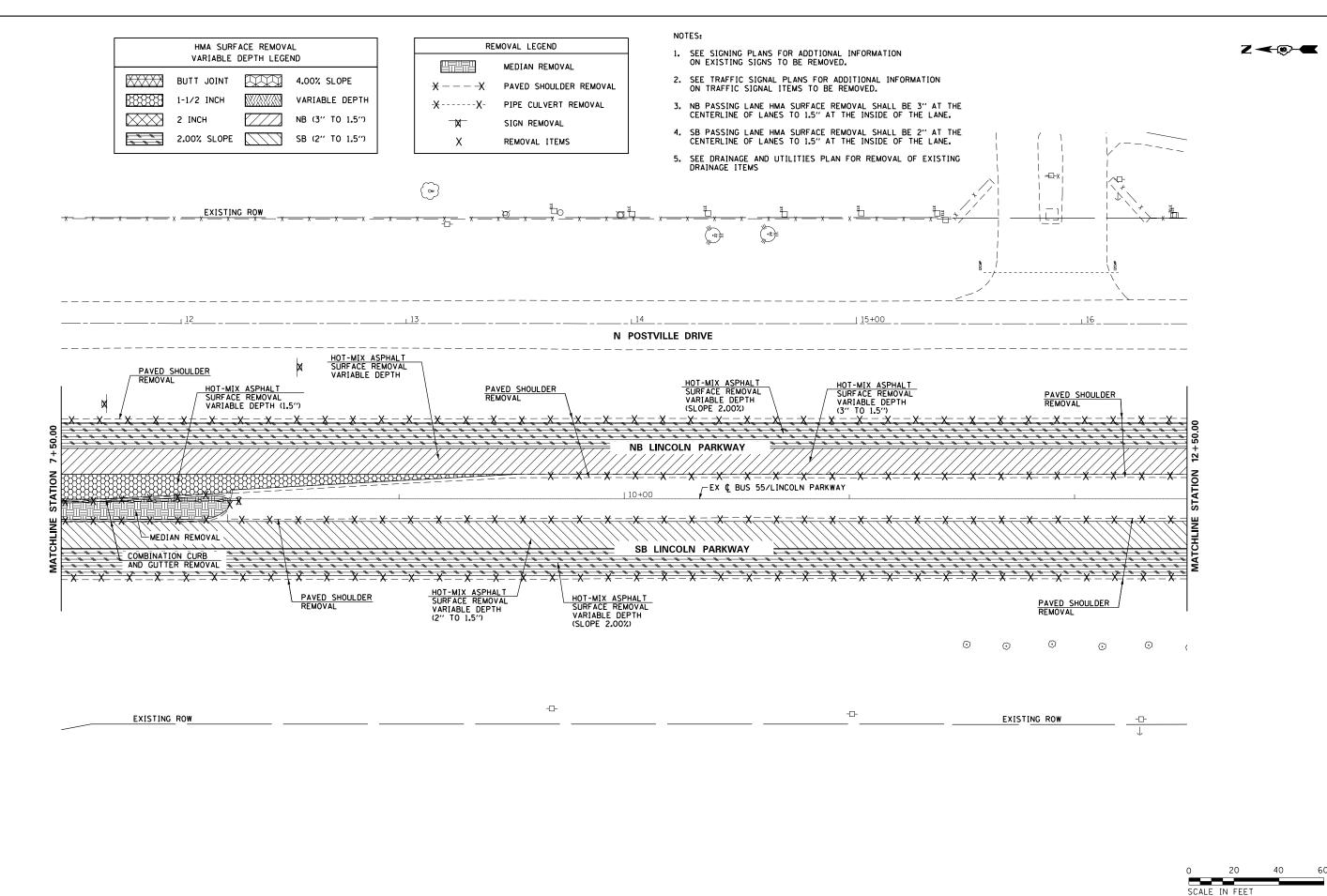
ALIGNMENT DATA							SECTION	COUNTY	TOTAL SHEETS	AL SHEET ETS NO.	
							(22)RS-6 & (23)RS-5,N,TS-1	LOGAN	343	79	
								CONTRACT	NO. 7	72E02	
SCALE: NO SCALE	SHEET NO.	OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT					





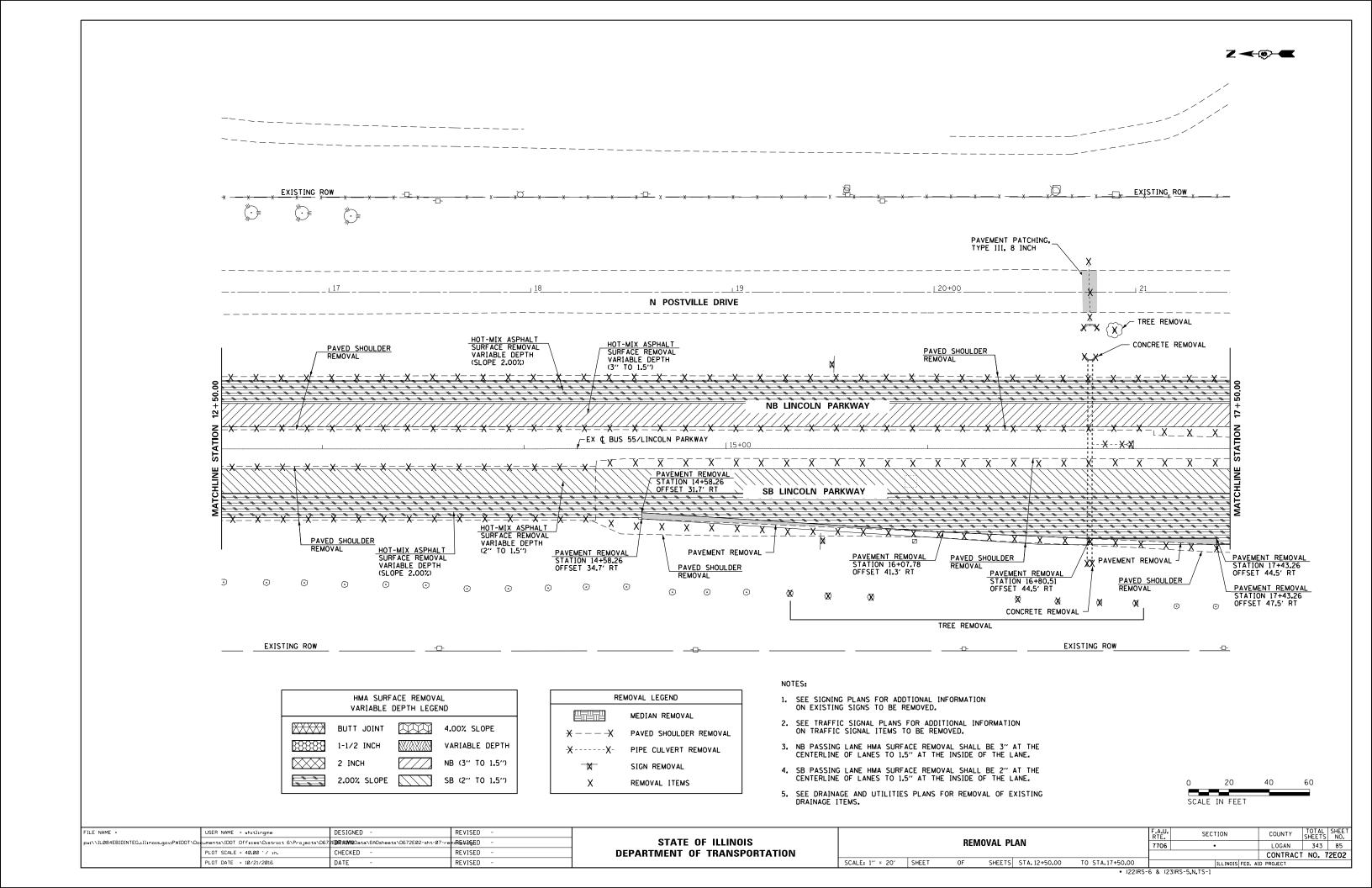


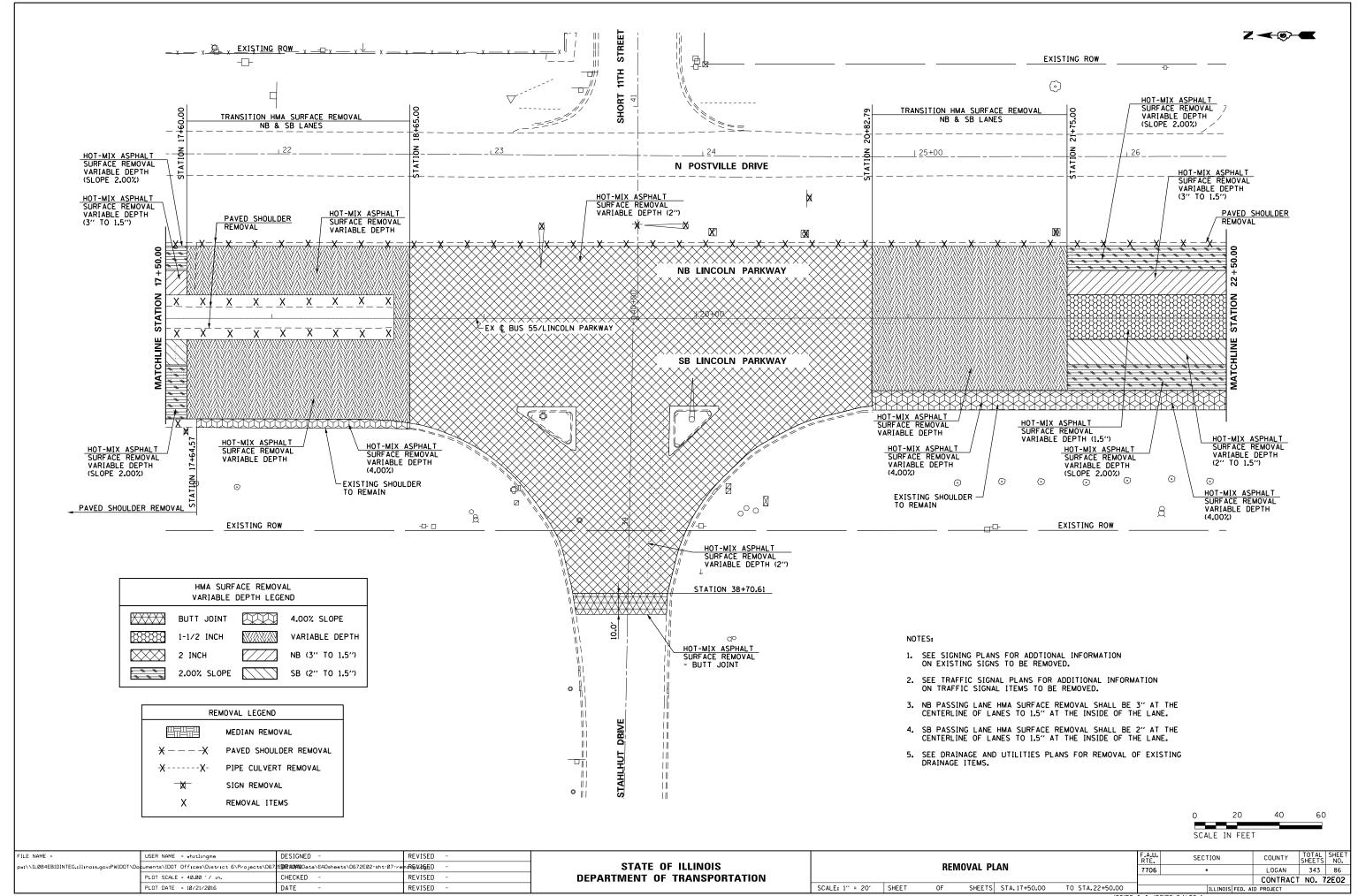


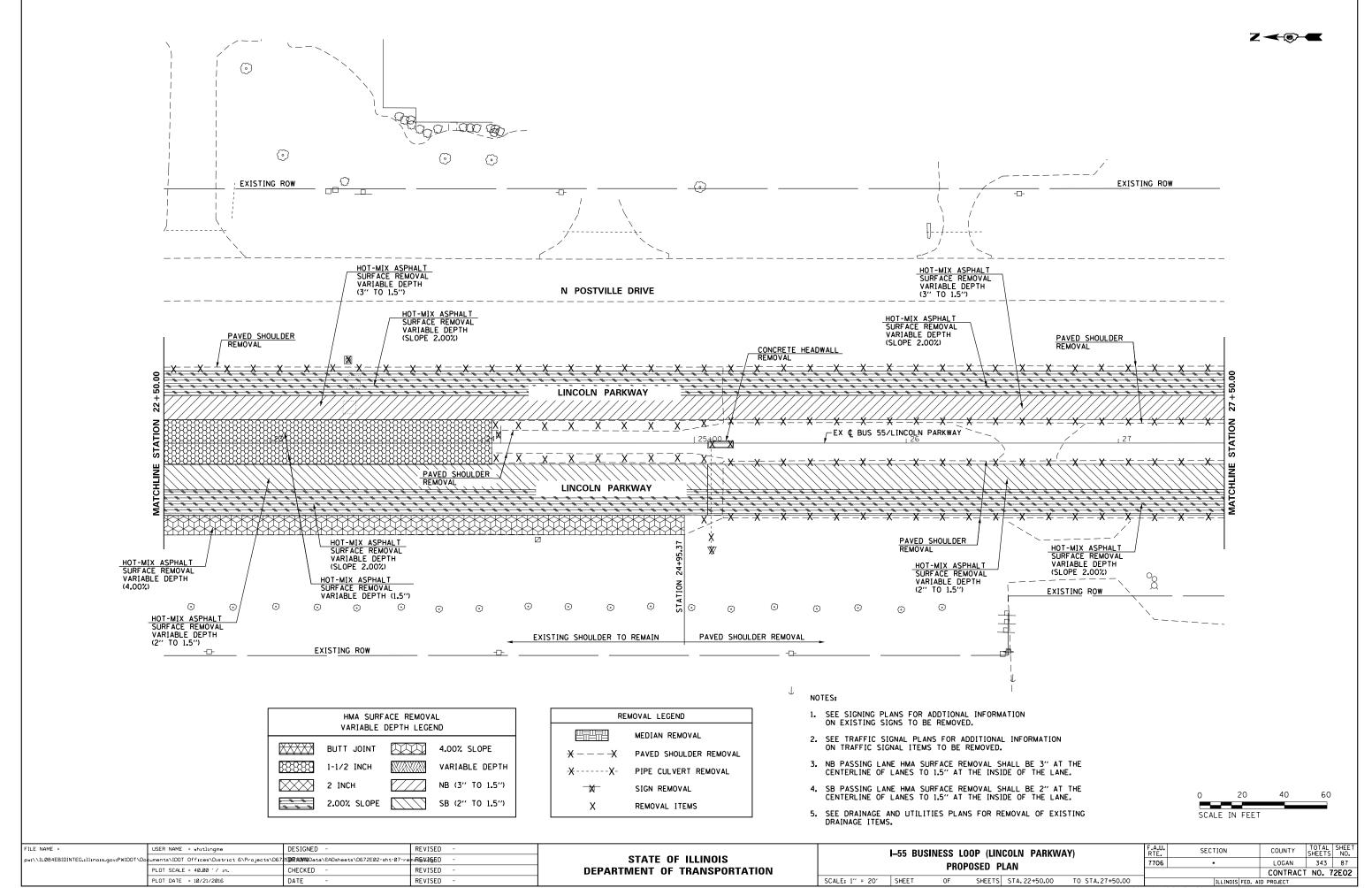


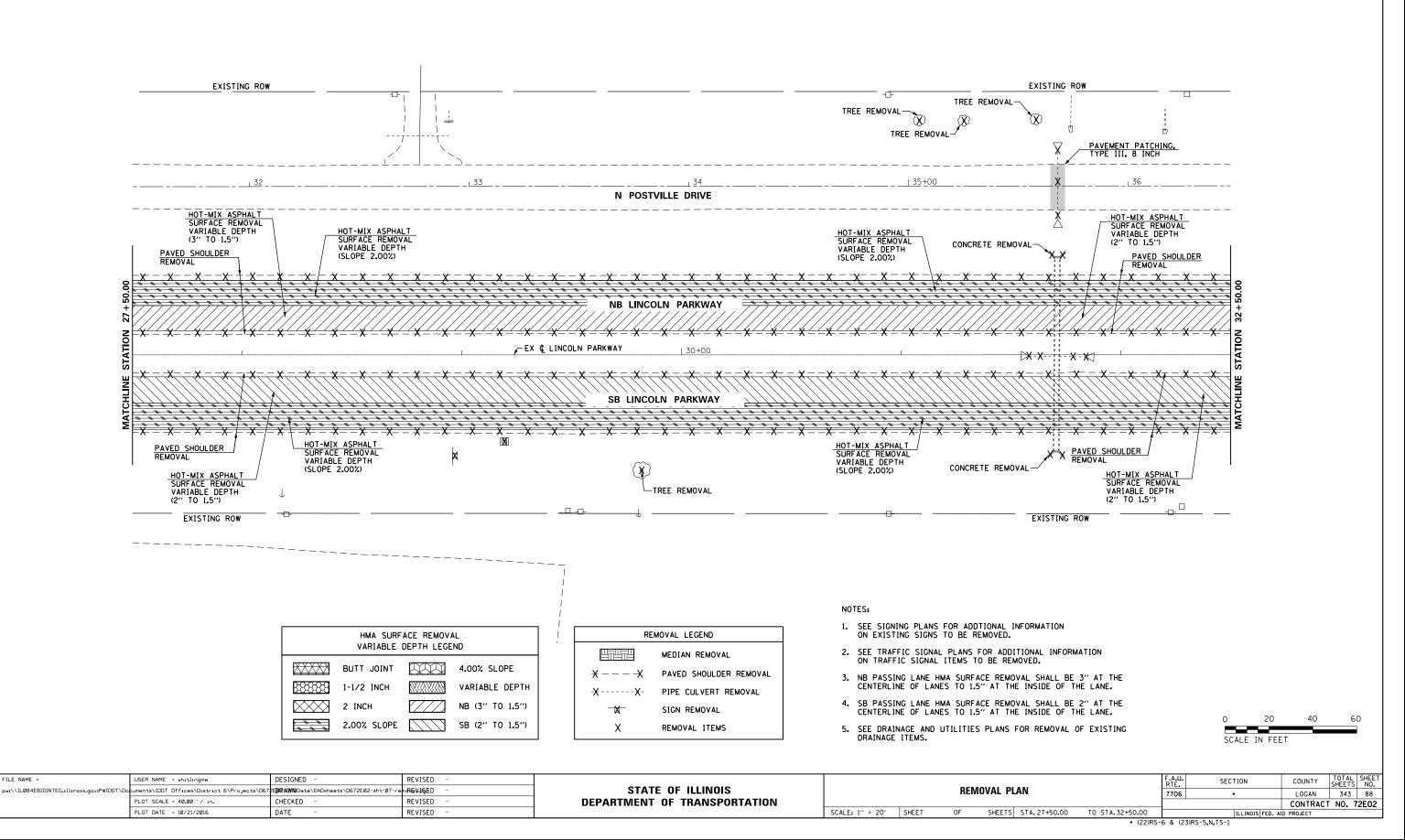


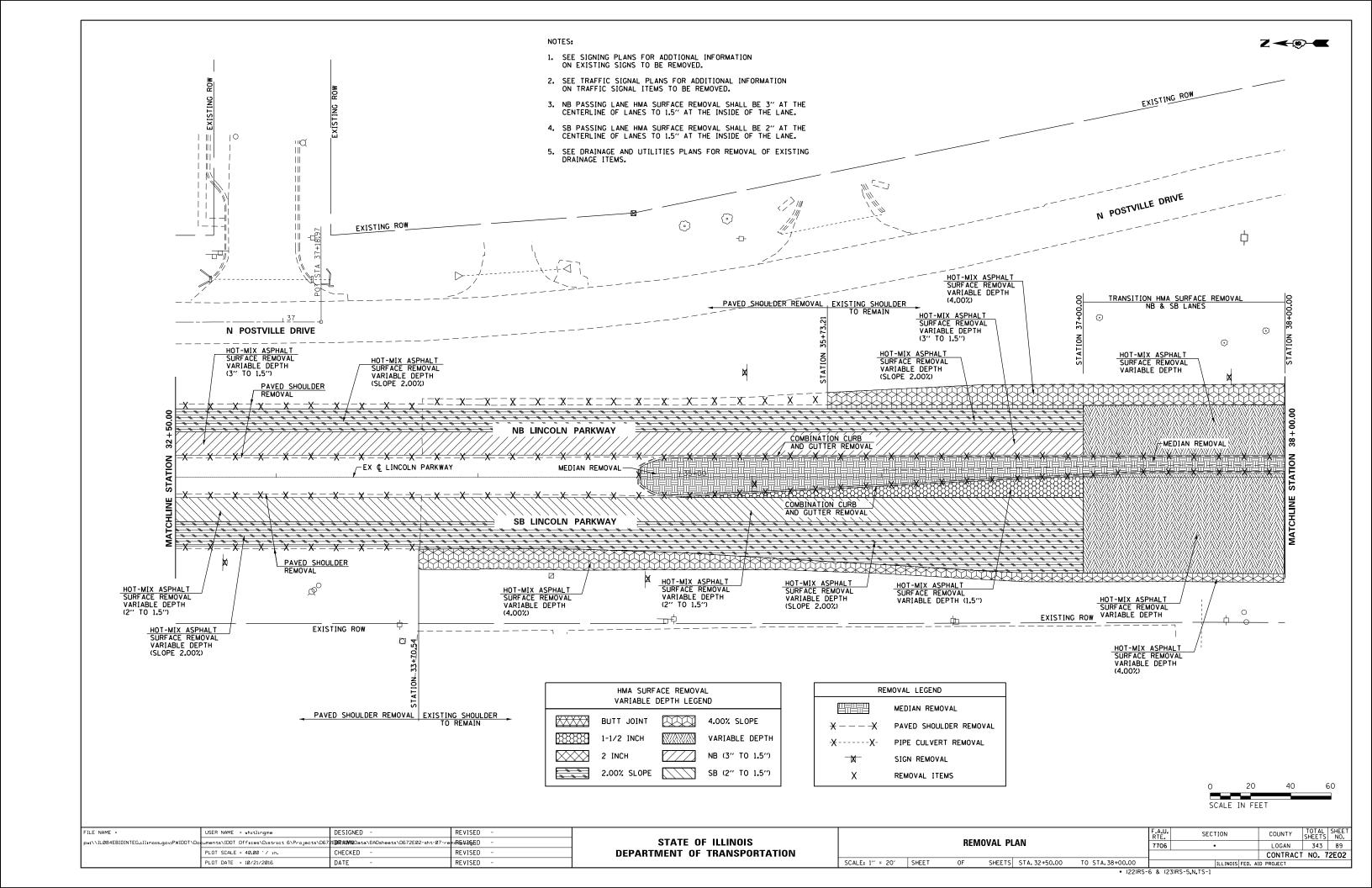
FILE NAME = DESIGNED REVISED USER NAME = whitlingme STATE OF ILLINOIS ow:\\ILØ84EBIDINTEG.:ll:no: uments\IDOT Offices\District 6\Projects\D672E**DRAMM9**Data\GADsheets\D672E02-sht-07-i ndR6¥&SFD REMOVAL PLAN LOGAN 343 84 7706 CHECKED REVISED **DEPARTMENT OF TRANSPORTATION** CONTRACT NO. 72E02 SCALE: 1" = 20' SHEET SHEETS STA. 7+50.00 PLOT DATE = 10/21/2016 DATE REVISED TO STA.12+50.00

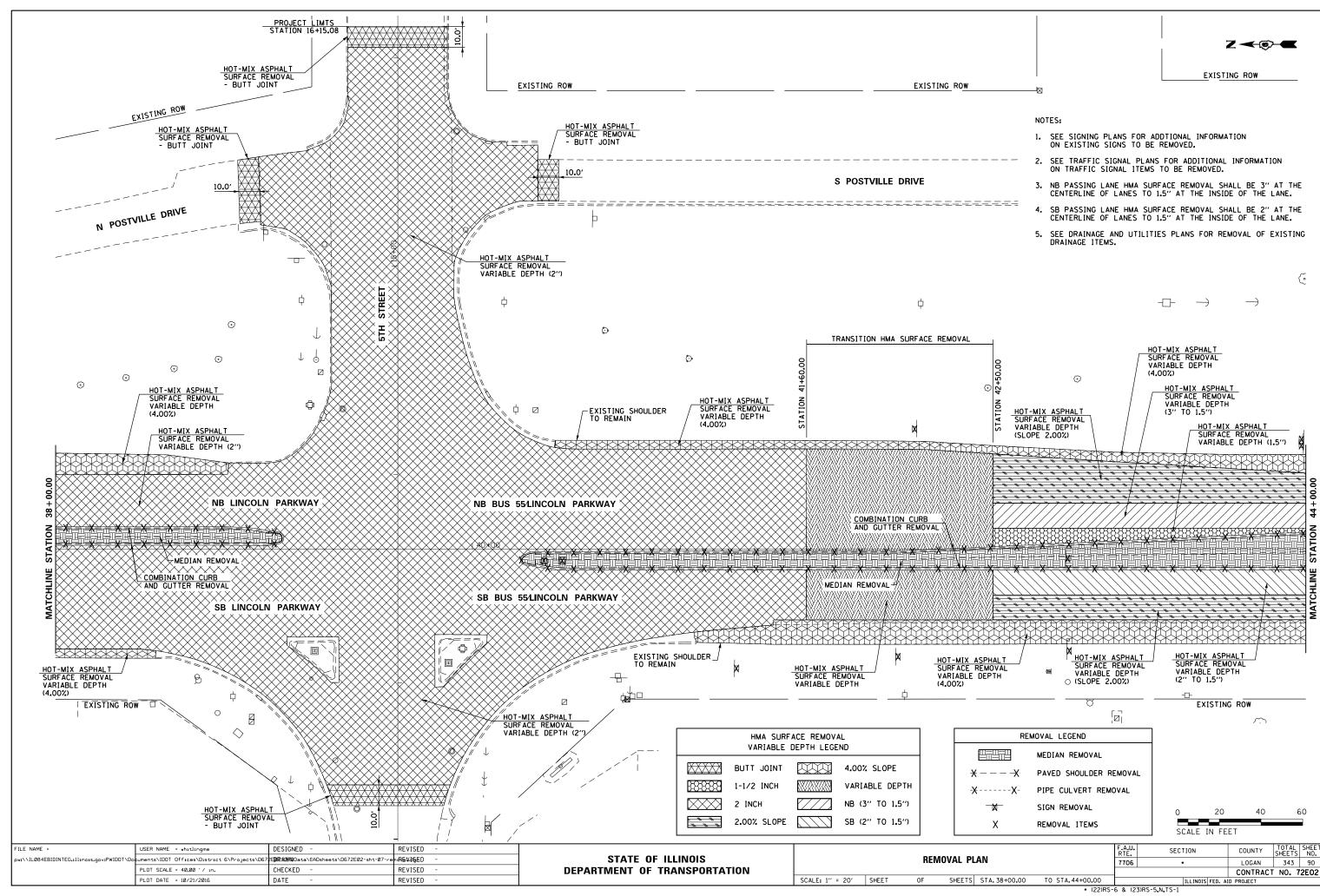


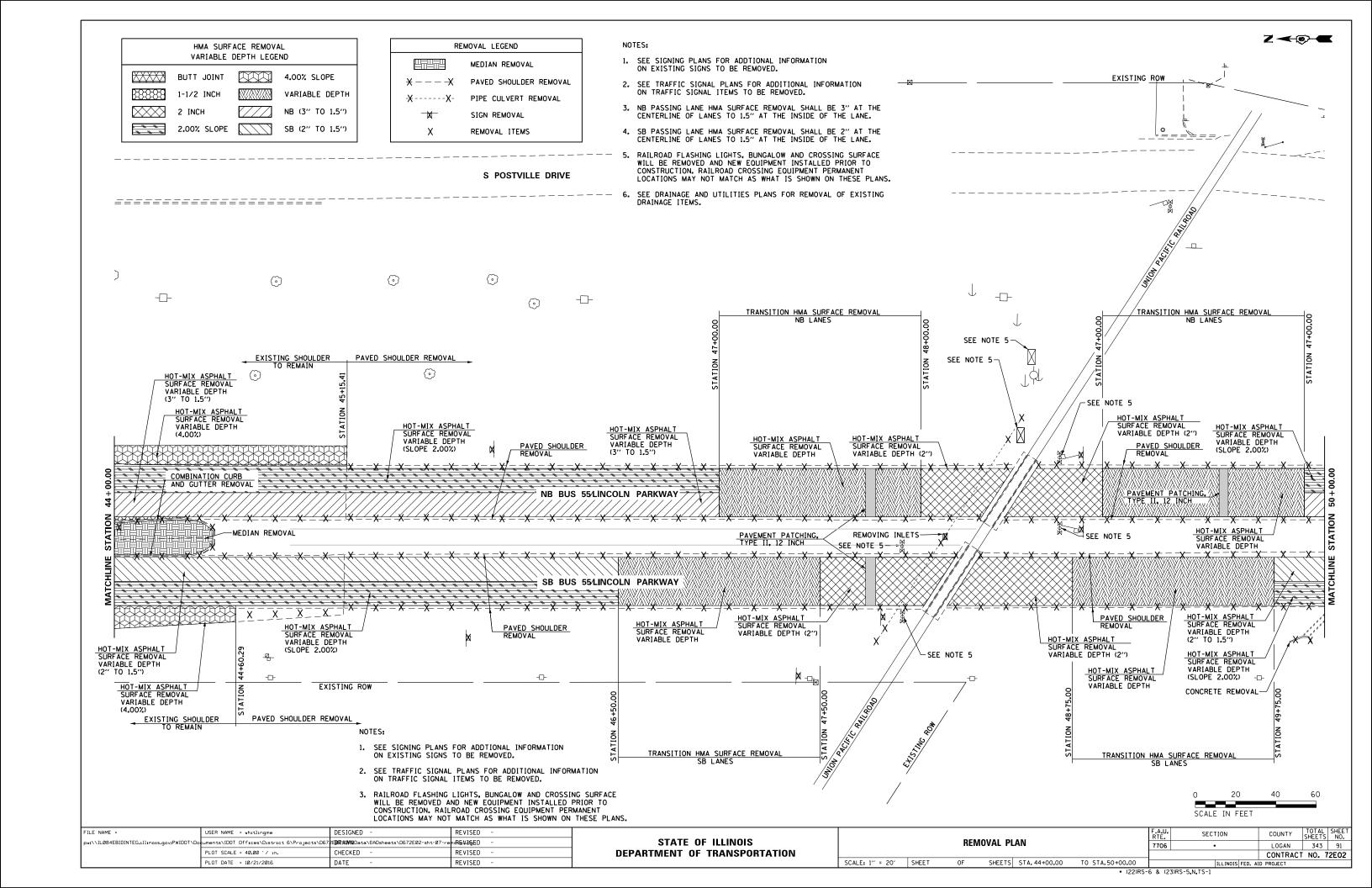


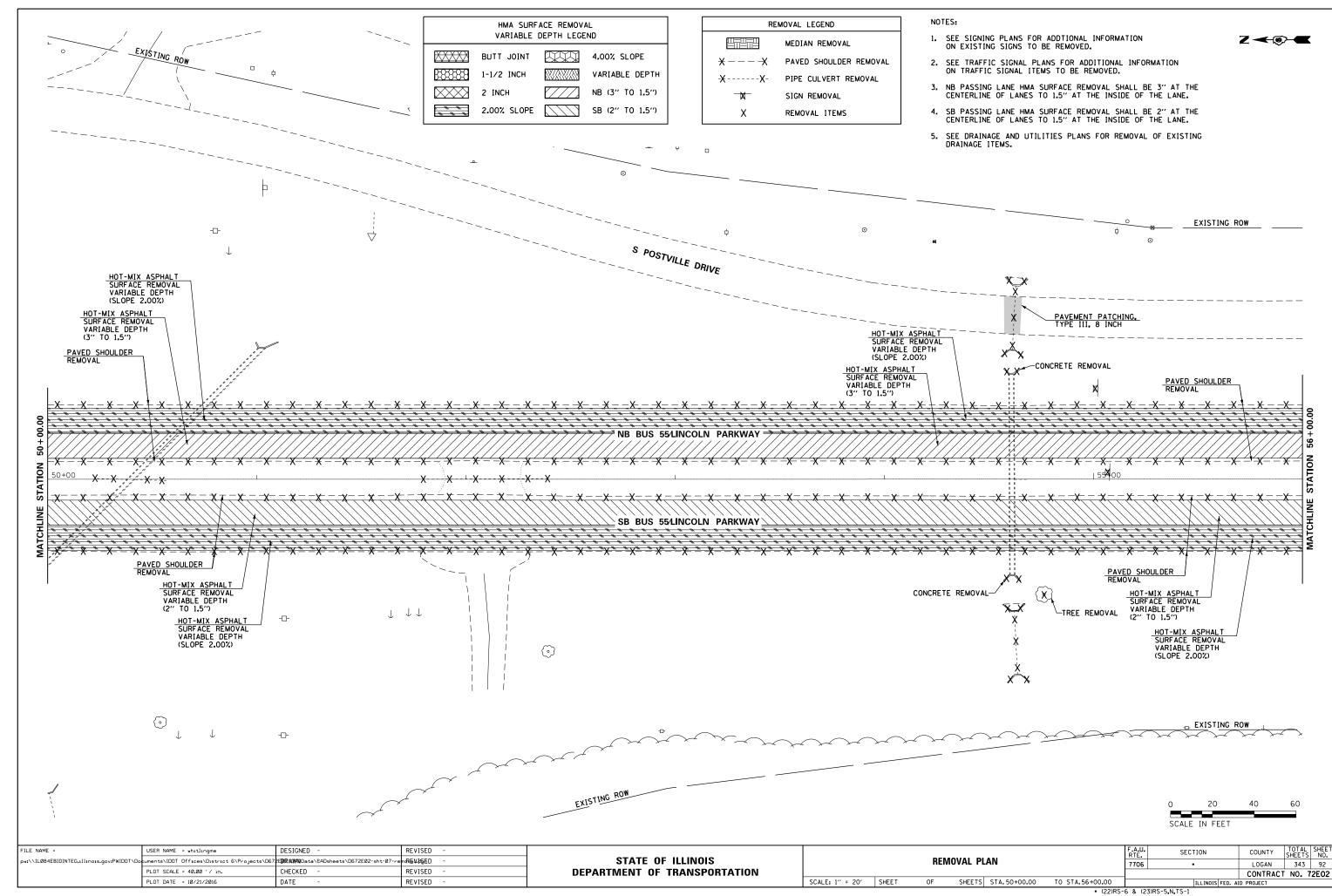


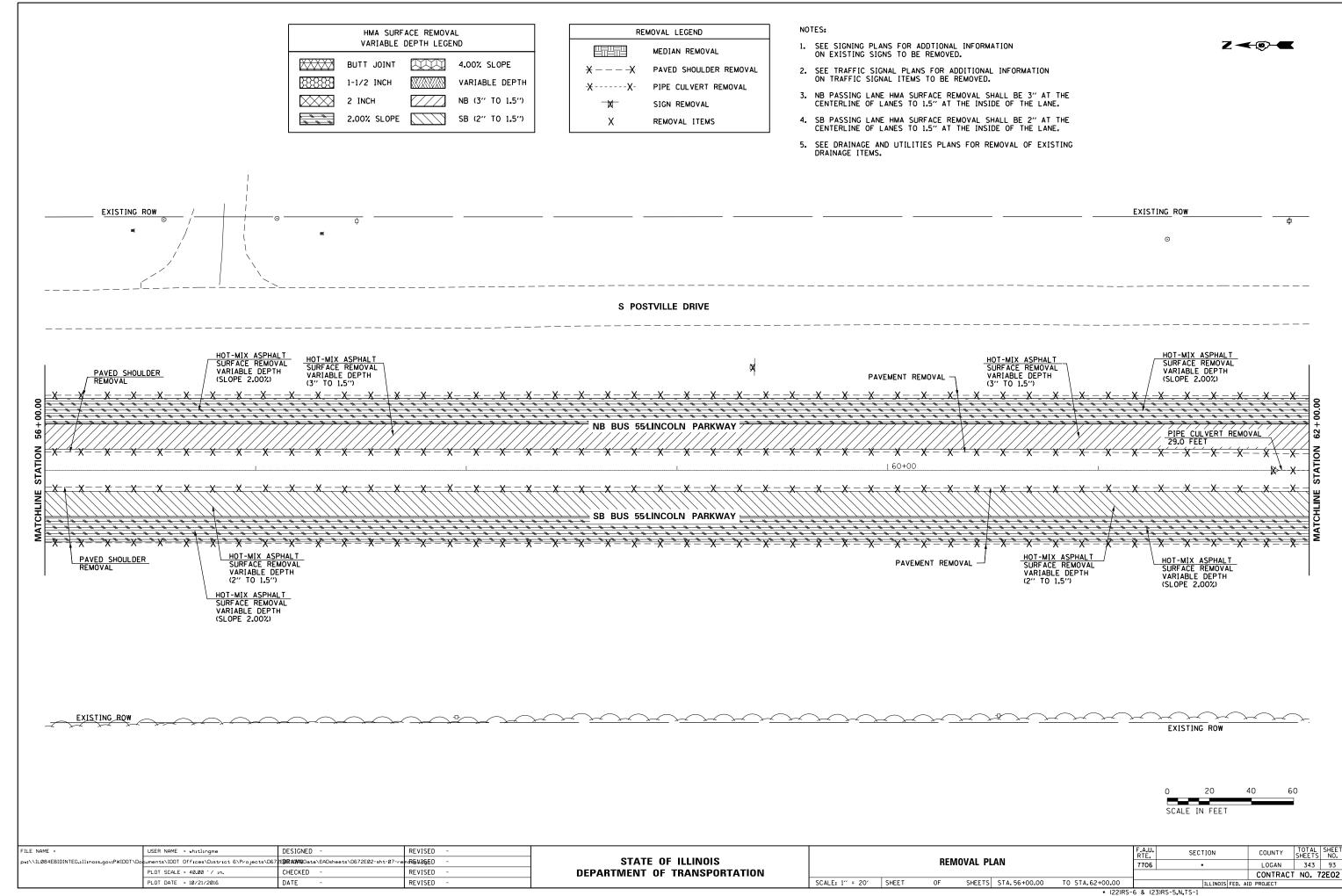


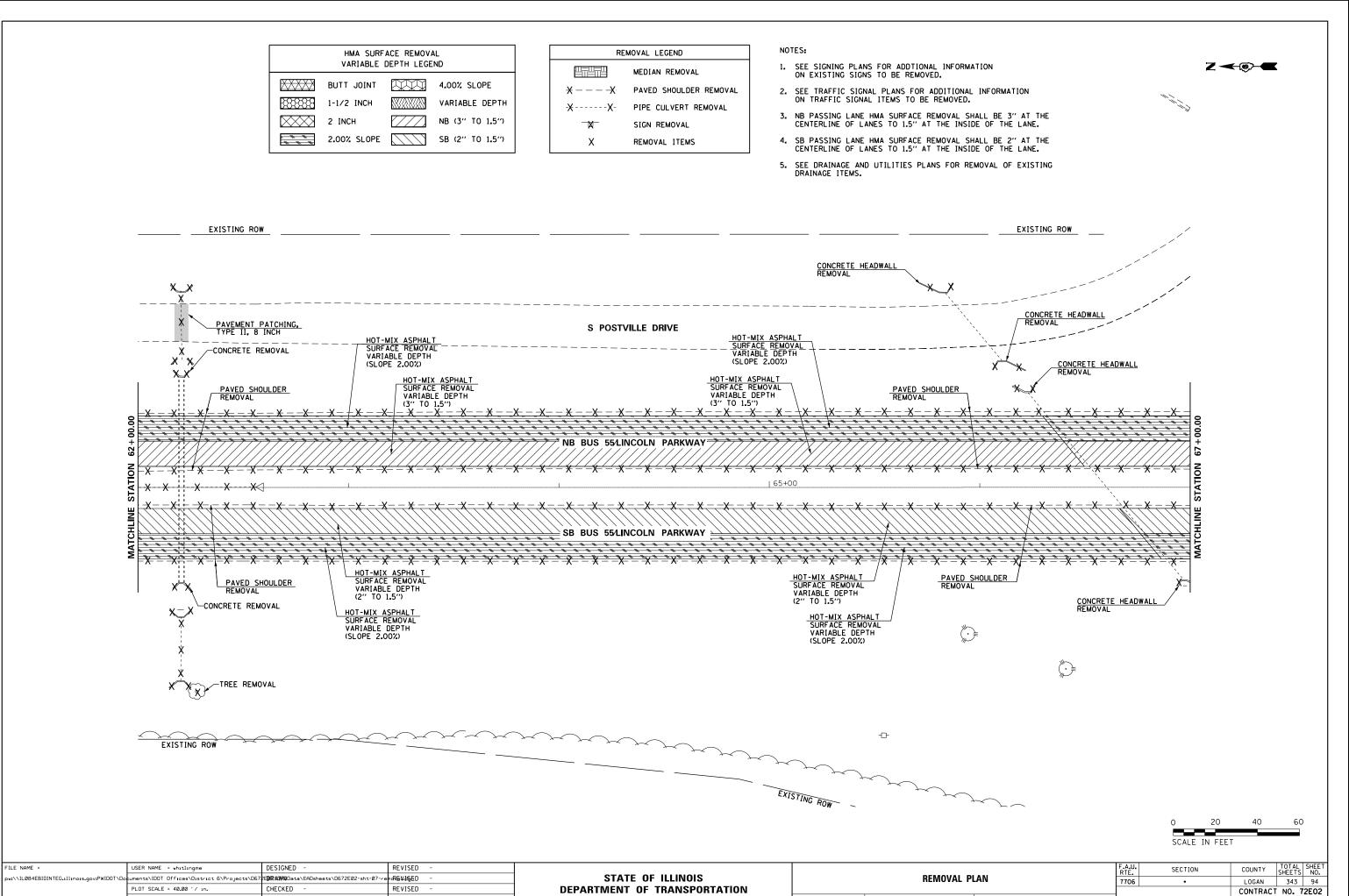












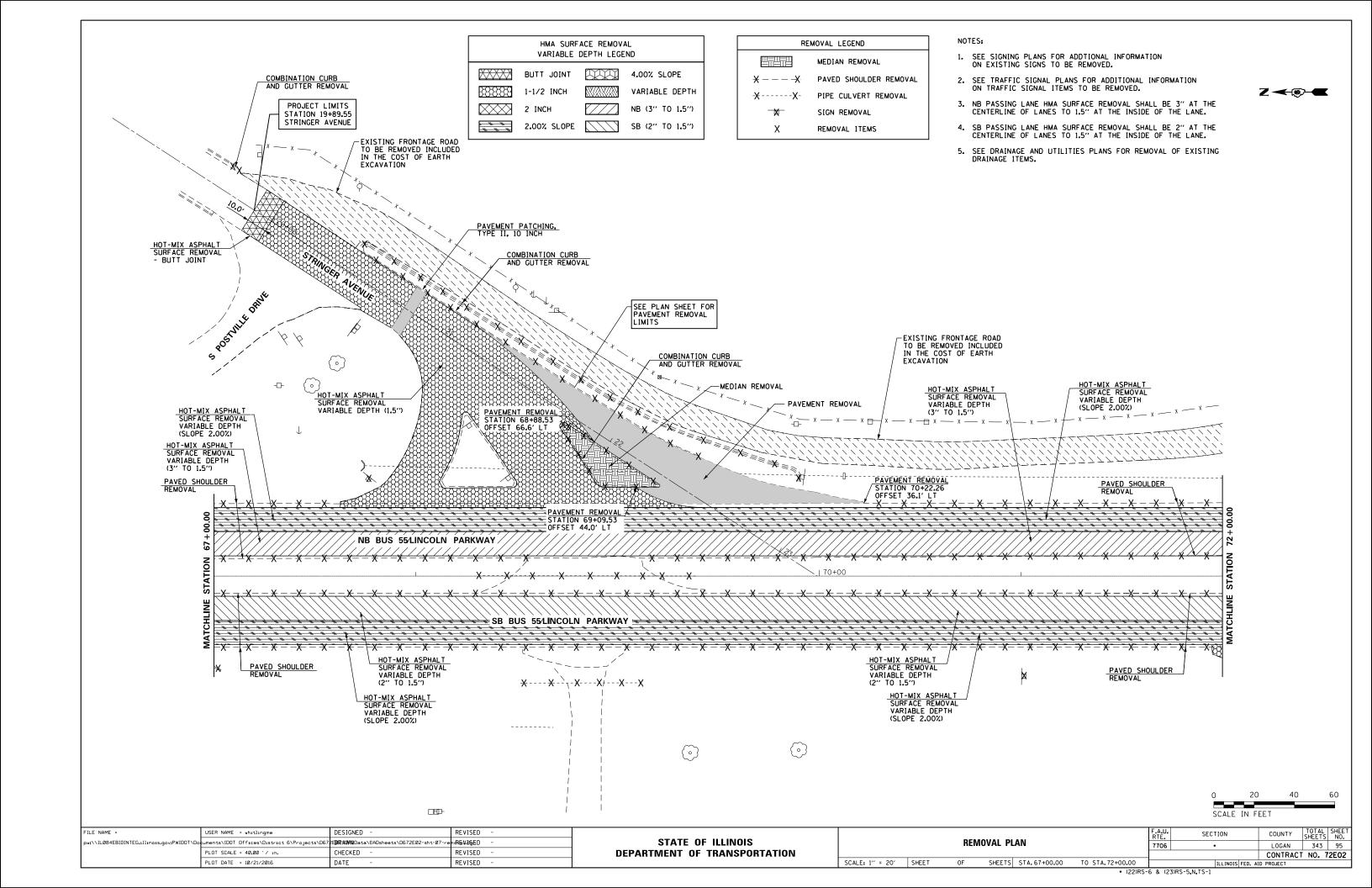
REVISED

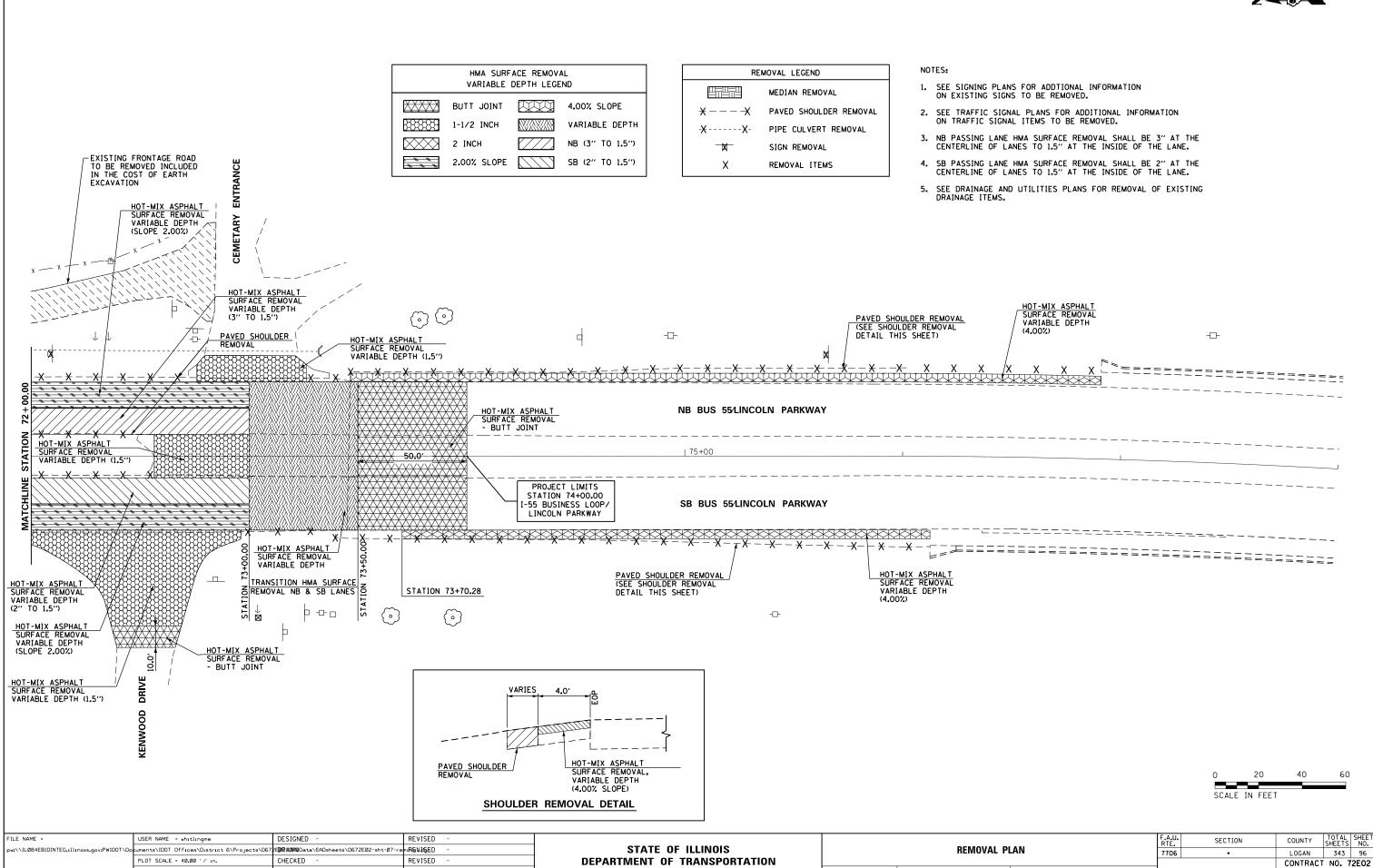
PLOT DATE = 10/21/2016

DATE

SCALE: 1" = 20' SHEET

SHEETS STA. 62+00.00 TO STA. 67+00.00





PLOT DATE = 10/21/2016

DATE

REVISED

SCALE: 1" = 20' SHEET

SHEETS STA. 72+00.00 TO STA. 78+00.00

HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND

2 INCH

BUTT JOINT 1-1/2 INCH

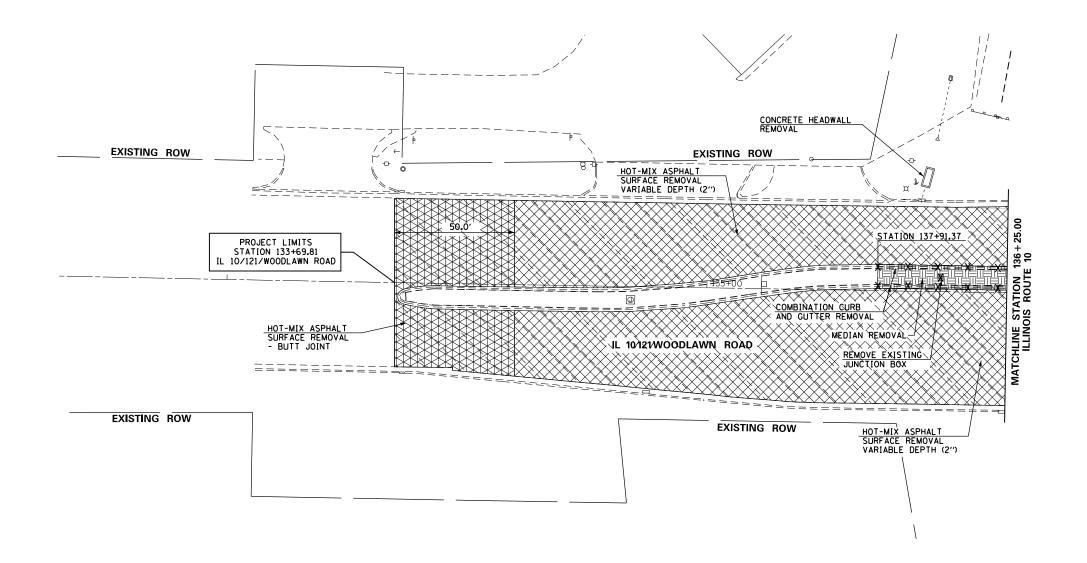
4.00% SLOPE VARIABLE DEPTH

NB (3" TO 1.5") 2.00% SLOPE SB (2" TO 1.5")

REMOVAL LEGEND MEDIAN REMOVAL PAVED SHOULDER REMOVAL PIPE CULVERT REMOVAL SIGN REMOVAL Χ REMOVAL ITEMS

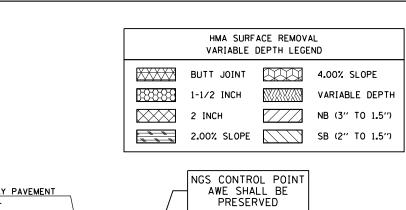
NOTES:

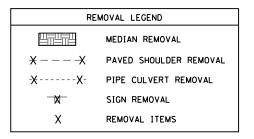
- 1. SEE SIGNING PLANS FOR ADDTIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
- SEE TRAFFIC SIGNAL PLANS FOR ADDITIONAL INFORMATION ON TRAFFIC SIGNAL ITEMS TO BE REMOVED.
- 3. SEE DRAINAGE AND UTILITIES PLANS FOR REMOVAL OF EXISTING DRAINAGE ITEMS.





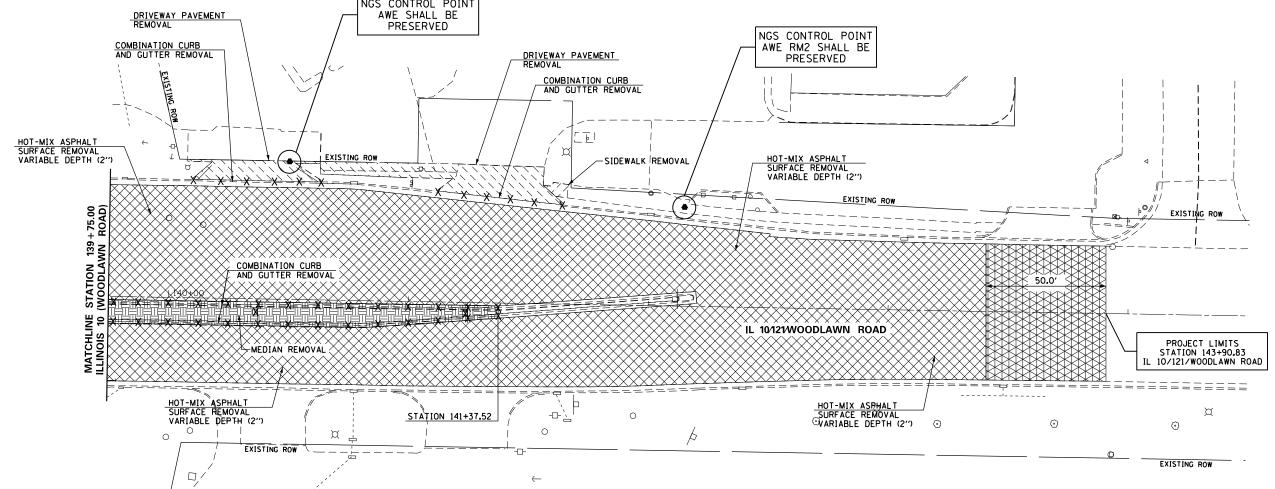
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pm:\\L084EBIDINTEG.illinois.gov:PWiDOT\Documents\IDOT Offices\District 6\Projects\D67#EBRAMMOata\E6Dsheets\D67ZE02-sht-07-remA6\L66ED -				STATE OF ILLINOIS	REMOVAL PLAN			7706	•	LOGAN	343 97		
		PLOT SCALE = 40.00 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION							CONTRACT	T NO. 72EO2
		PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE: 1" = 20' SHEET	OF	SHEETS STA. 132	-00.00 TO STA.136+50.00		ILL INOIS FED.	AID PROJECT	





NOTES:

- 1. SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
- 2. SEE TRAFFIC SIGNAL PLANS FOR ADDITIONAL INFORMATION ON TRAFFIC SIGNAL ITEMS TO BE REMOVED.
- 3. SEE DRAINAGE AND UTILITIES PLANS FOR REMOVAL OF EXISTING DRAINAGE ITEMS.



NGS CONTROL POINT	STATION	OFFSET	ELEVATION
AWE	140+49.659	57.472 LT	582.036
AWE RM2	142+14.535	41.308 LT	583.230



FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -						F.A.U.	SECTION	COUNTY	TOTAL SHEET
pw:\\IL084EBIDINTEG.:llinois.gov:PWIDOT\Do	cuments\IDOT Offices\District 6\Projects\D67	PE DRAMMN Data\EADsheets\D672E02-sht-07-re	r-R-6¥-16GED -	STATE OF ILLINOIS			MOVAL PLAN		7706	•	LOGAN	343 98
	PLOT SCALE = 40.00 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION							CONTRACT	T NO. 72EO2
	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE: 1" = 20' SHEET OF SHEETS STA.139+75.00 TO STA.144+00.00			ILLINOIS FED. AID PROJE				

