

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

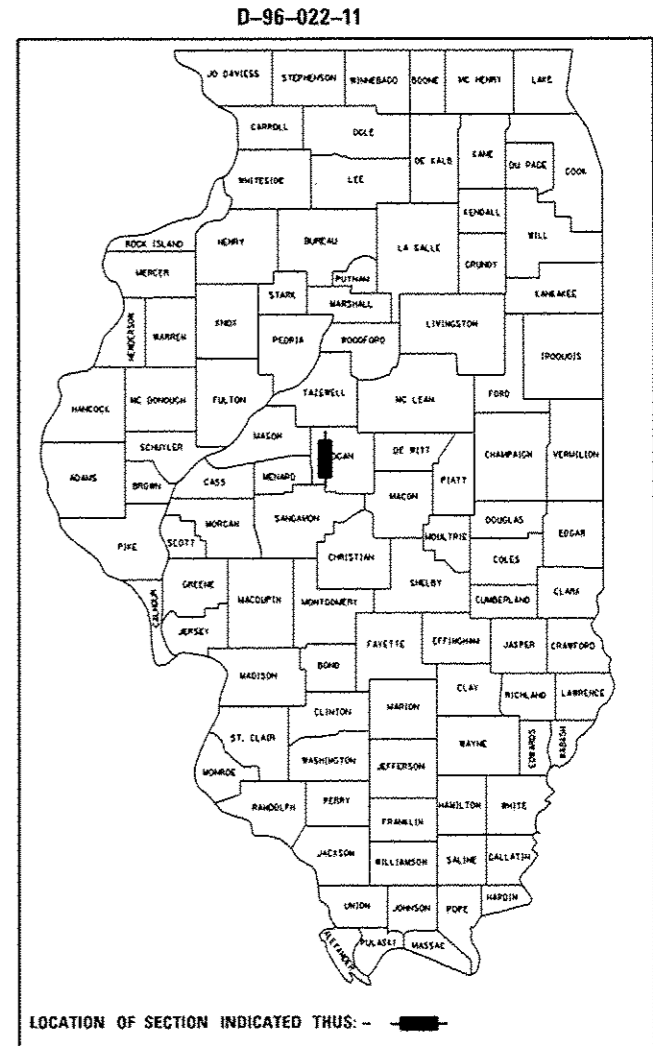
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		ILLINOIS	343	1
			CONTRACT NO. 72E02	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROPOSED HIGHWAY PLANS

F.A.U. ROUTE 7706
(I55 BUS, OLD US 66, LINCOLN PARKWAY)
SECTION (22) RS-6, MED & (23) RS-5
PROJECT ACHSIP-ACM-7706(005)
TYPE of IMPROVEMENT 3R & SAFETY
LOGAN COUNTY

C-96-022-11



ROADWAY CLASSIFICATION

MINOR ARTERIAL

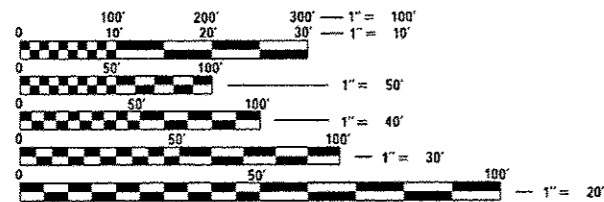
AVERAGE DAILY TRAFFIC (2013)

IL 10 WOODLAWN ROAD TO 5TH STREET

7,500 (2013)
PV = 94.90%
SU = 3.77%
MU = 1.33%

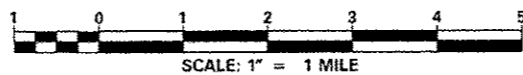
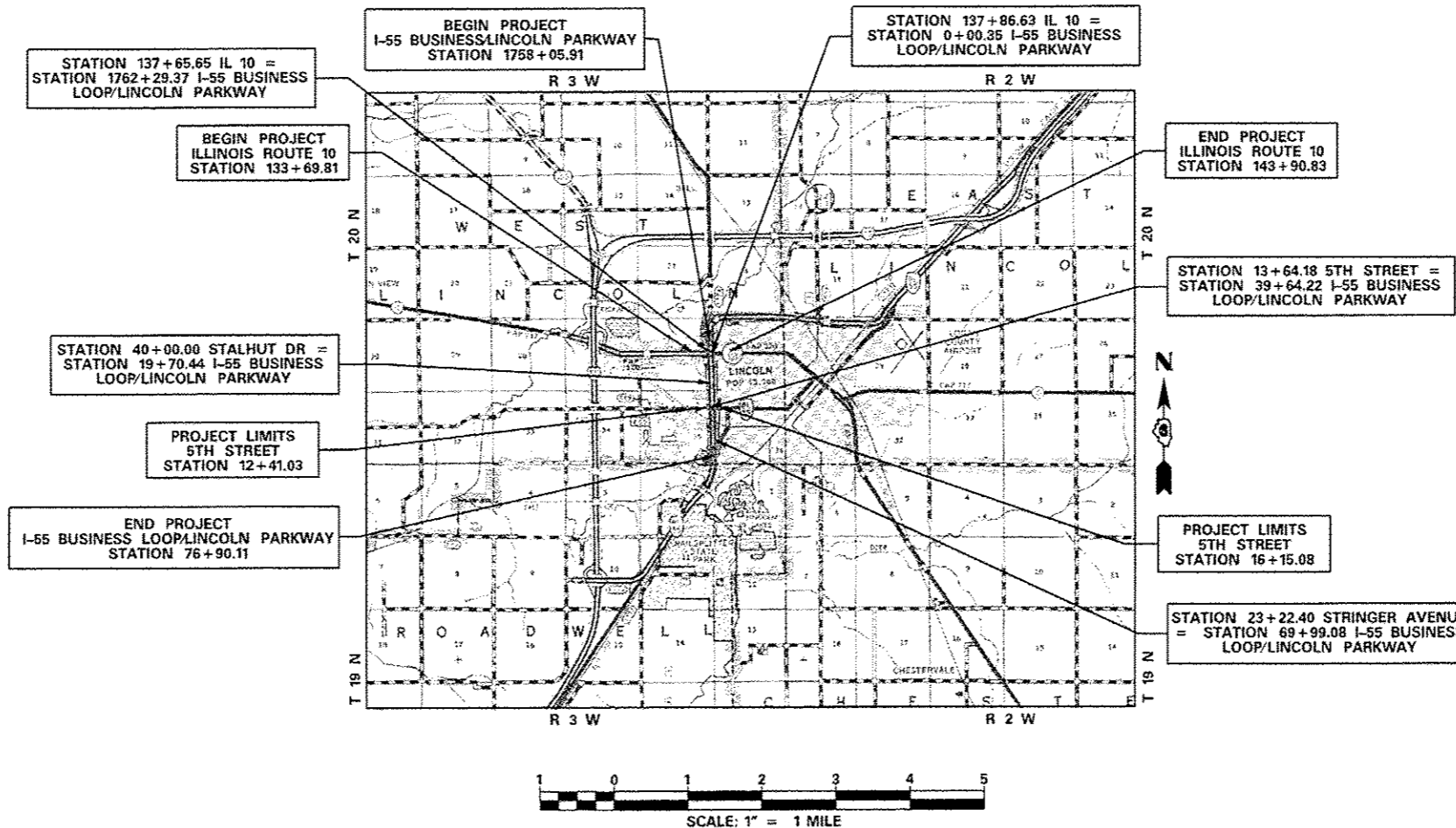
5TH STREET TO STRINGER AVENUE

4,000 (2013)
PV = 90.25%
SU = 6.25%
MU = 3.50%



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811



GROSS LENGTH = 8114.35 FT. = 1.536 MILE
NET LENGTH = 8114.35 FT. = 1.536 MILE

PROJECT ENGINEER JAY WAVERING 217-785-9046
PROJECT MANAGER MICHAEL WHITLING 217-524-0239

CONTRACT NO. 72E02

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED *October 20, 2016*

Jeffrey M. South, JPM
REGIONAL ENGINEER

Dec 9 2016
Maureen M. Addis, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

Dec 9 2016
Amelia [Signature]
DIRECTOR OF PROGRAM DEVELOPMENT

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OF THE STATE OF ILLINOIS

GENERAL NOTES

EXCAVATION IN OLD GRAVEL OR EXISTING OIL AND CHIP ROADWAYS IS TO BE INCLUDED IN THE COST OF EARTH EXCAVATION.

GRADING SHALL BE DONE BY HAND AROUND LIGHT POLES, UTILITY POLES, SIGN POSTS, SHRUBS, TREES OR OTHER NATURAL OR MAN-MADE OBJECTS WHERE SHALLOW FILLS OR CUTS ARE ADJACENT TO THE ITEMS.

THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR EXISTING ITEMS WHICH WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT THE CONTRACTOR'S EXPENSE.

AT ANY AREA WHERE GRADING IS NOT PROPOSED, THE EXISTING GROUND COVER SHALL NOT BE DISTURBED.

ONLY THOSE TREES DESIGNATED BY THE ENGINEER OR LISTED IN THE TREE REMOVAL SCHEDULE SHALL BE REMOVED. THE CONTRACTOR SHALL PROTECT ALL REMAINING TREES FROM DAMAGE DUE TO HIS OPERATIONS.

EXISTING PAVEMENT DAMAGED DUE TO THE CONTRACTOR'S OPERATIONS, AND NOT OTHER WISE NECESSARY TO REPLACE, SHALL BE REPLACED AT THE EXPENSE OF THE CONTRACTOR.

SAW CUTS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

ALL DRAINAGE STRUCTURES SHALL BE FREE OF SILT, DEBRIS, OR OTHER SUCH OBSTRUCTIONS AT THE TIME OF FINAL INSPECTION, THE CLEANING OF THESE DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE VARIOUS PAY ITEMS INVOLVED. THE WORK AREA SHALL BE POSITIVELY DRAINED DURING CONSTRUCTION. FINAL GRADES SHALL BE PROTECTED AGAINST DAMAGE FROM EROSION, SEDIMENTATION, AND TRAFFIC.

LENGTHS AND SIZES OF STORM SEWERS AS SHOWN ON THE PLANS AND DRAINAGE STRUCTURE ELEVATIONS SHALL BE VERIFIED BY THE CONTRACTOR IN THE FIELD PRIOR TO ORDERING AND INSTALLATION OF DRAINAGE ITEMS. THE INVERTS OF THE PROPOSED DRAINAGE STRUCTURES MAY REQUIRE REVISIONS TO MEET EXISTING FIELD CONDITIONS. ANY ADJUSTMENTS SHALL BE AS DIRECTED BY THE ENGINEER.

FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.

THE CONTRACTOR WILL BE RESPONSIBLE FOR THE TRAFFIC CONTROL DEVICES AT ALL TIMES DURING CONSTRUCTION ACTIVITIES AND SHALL COORDINATE THE ITEMS OF WORK IN ORDER TO KEEP HAZARD TRAFFIC INCONVENIENCES TO A MINIMUM.

SEEDING SHALL BE DONE ON ALL AREAS THAT ARE DISTURBED BY CONSTRUCTION OPERATIONS AS DIRECTED BY THE ENGINEER. ALL AREAS DISTURBED BY THE CONTRACTOR OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE SEEDED, AS DIRECTED BY THE ENGINEER, AT THE CONTRACTOR'S EXPENSE. SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION.

ALL PRECAST CONCRETE BOX CULVERT SEGMENTS WITH PIPE CULVERTS INTERSECTING THE SIDEWALLS SHALL BE DETAILED AS PER THE REQUIREMENTS OF ASTM C1577, EXCEPT THAT THE AS2 AND AS5 REINFORCEMENT AREAS SHALL BE INCREASED TO 0.40 SQ. IN. PER FOOT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ADEQUATE SIGNS, TRAFFIC CONTROL DEVICES, AND WARNING DEVICES TO INFORM AND PROTECT THE PUBLIC DURING ALL PHASES OF CONSTRUCTION.

ALL PAVEMENT MARKINGS SHALL BE VERIFIED BY RESIDENT/FIELD ENGINEER PRIOR TO THEIR PLACEMENT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.

THE CONTRACTOR SHALL CONTACT J.U.L.I.E. PRIOR TO ANY EXCAVATION TO HAVE ALL PUBLIC AND PRIVATE UTILITIES LOCATED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.26 OF THE STANDARD SPECIFICATIONS. THE J.U.L.I.E. NUMBER IS (800) 892-0123. A MINIMUM OF 48 HOURS ADVANCE NOTICE IS REQUIRED.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUB-NUMBER LISTED ON THE INDEX OF SHEETS OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.

ALL ELEVATIONS REFER TO USGS MEAN SEA LEVEL DATUM.

ALL PAVEMENT MARKING LINES THAT CONFLICT WITH STAGE CONSTRUCTION THAT IS LOCATED OFF THE PROJECT SHALL BE REMOVED BY WATER BLASTING AND SHALL BE PAID FOR AS PAVEMENT MARKING REMOVAL - WATER BLASTING.

UTILITES ARE SHOWN IN ACCORDANCE WITH THE BEST AVAILABLE INFORMATION, AND THEIR TRUE LOCATION IS NOT GUARANTEED TO BE AS SHOWN ON THE PLANS. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE EXACT LOCATION OF ALL UTILITIES AND CARRY OUT HIS/HER OPERATIONS ACCORDINGLY. MEMBERS OF J.U.L.I.E. KNOWN TO BE WITHIN THE LIMITS OF THE IMPROVEMENT ARE:

- AMEREN CILCO - GAS
- ILLINOIS AMERICAN WATER - WATER
- FRONTIER COMMUNICATIONS - FIBER OPTICS
- METRO COMMUNICATIONS COMPANY INC. - COMMUNICATIONS
- COMCAST COMMUNICATIONS - CABLE
- AMEREN/CENTRAL ILLINOIS - ELECTRIC
- PANHANDLE EASTERN PIPELINE CO. - PIPELINE
- FIBER UTILITY GROUP - FIBER OPTIC
- CITY OF LINCOLN - SEWER
- CITY OF LINCOLN (STREET DEPT.) - WATER
- CMS FIBER - FIBER OPTIC

FILE NAME *	USER NAME * whittingma	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pu\11084EBIDINTEG\Illinois.gov\PWIDOT\Documents\1007 Offices\District 6\Projects\0672E02\0672E02-02-ge-REVISED.dgn	PLOT SCALE * 48.0000' / in.	CHECKED -	REVISED -			7706	*	LOGAN	343	3	
General Notes	PLOT DATE * 10/21/2016	DATE -	REVISED -			SCALE: SHEET OF SHEETS STA. TO STA.		ILLINOIS FED. AID PROJECT			
								CONTRACT NO. 72E02			

CONSTRUCTION CODE

CODE NO	ITEM	UNIT	TOTAL QUANTITY	PPS# 6-00625-0000	PPS# 6-00477-0000	PPS# 6-00625-0000	PPS# 6-00625-0000	PPS# 6-00625-0000	PPS# 6-00477-0000	PPS# 6-00625-0000	PPS# 6-00625-0000	PPS# 6-00625-0000
				80%FED/20%STATE	90%FED/10%STATE	IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE	STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL	5TH STREET INTERSECTION 80%FED/15%STATE/ 5%LOCAL	5TH STREET INTERSECTION 90%FED/10%STATE	80%FED/20%LOCAL	100%LOCAL	
				STP ROADWAY 0004 URBAN	HSIP ROADWAY 0004 URBAN	STP TRAFFIC SIGNALS 0021 URBAN	STP TRAFFIC SIGNALS 0021 URBAN	STP TRAFFIC SIGNALS 0021 URBAN	HSIP TRAFFIC SIGNALS 0021 URBAN	STP SIDEWALK 0020 URBAN	STP LIGHTING 0021 URBAN	
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	203	203								
20200100	EARTH EXCAVATION	CU YD	5468	4820	604					44		
20800150	TRENCH BACKFILL	CU YD	202.9	102.7	95.4					4.8		
25000200	SEEDING, CLASS 2	ACRE	7.4	7.4								
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	669	669								
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	669	669								
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	669	669								
25000700	AGRICULTURAL GROUND LIMESTONE	TON	15	15								
25100115	MULCH, METHOD 2	ACRE	7.4	7.4								
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	744	744								
28000305	TEMPORARY DITCH CHECKS	FOOT	560	560								
28000400	PERIMETER EROSION BARRIER	FOOT	581	581								
28000500	INLET AND PIPE PROTECTION	EACH	34	34								
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	13445	10675	2770							

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FILE NAME =	USER NAME = whittinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Summary of Quantities	PLT SCALE = 100,0000' / 1"	CHECKED -	REVISED -						7706	*	LOGAN	343	4
	PLT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT CONTRACT NO. 72E02	

CODE NO	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE								
				PPS# 6-00625-0000 80%FED/20%STATE STP ROADWAY URBAN	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE HSIP ROADWAY URBAN	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE STP TRAFFIC SIGNALS URBAN	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL STP TRAFFIC SIGNALS URBAN	PPS# 6-00625-0000 5TH STREET INTERSECTION 80%FED/15%STATE/ 5%LOCAL TRAFFIC SIGNALS URBAN	PPS# 6-00477-0000 5TH STREET INTERSECTION 90%FED/10%STATE TRAFFIC SIGNALS URBAN	PPS# 6-00625-0000 80%FED/20%LOCAL STP SIDEWALK URBAN	PPS# 6-00625-0000 100%LOCAL STP LIGHTING URBAN	
44000100	PAVEMENT REMOVAL	SO YD	311	311								
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	141	141								
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	4182	446	3736							
44000600	SIDEWALK REMOVAL	SO FT	71							71		
44003100	MEDIAN REMOVAL	SO FT	3472		3472							
44004250	PAVED SHOULDER REMOVAL	SO YD	4087	4003	84							
44200094	PAVEMENT PATCHING, TYPE II, 8 INCH	SO YD	13.4	13.4								
44200099	PAVEMENT PATCHING, TYPE III, 8 INCH	SO YD	67.8	67.8								
44200101	PAVEMENT PATCHING, TYPE IV, 8 INCH	SO YD	102	102								
44200120	PAVEMENT PATCHING, TYPE II, 10 INCH	SO YD	15	15								
44200144	PAVEMENT PATCHING, TYPE II, 12 INCH	SO YD	710	710								
44200148	PAVEMENT PATCHING, TYPE III, 12 INCH	SO YD	94	94								
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	1308	1119	189							
48203023	HOT-MIX ASPHALT SHOULDERS, 6 1/2"	SO YD	2954	2954								

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FILE NAME :	USER NAME : whittinge	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Summary of Quantities									7706		LOGAN	343	6
PLT SCALE = 100.0000 / 1 in.	CHECKED -	REVISED -	SCALE:		SHEET	OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 72E02		
PLT DATE = 10/21/2016	DATE -	REVISED -											

CONSTRUCTION CODE

CODE NO	ITEM	UNIT	TOTAL QUANTITY	PPS# 6-00625-0000	PPS# 6-00477-0000	PPS# 6-00625-0000	PPS# 6-00625-0000	PPS# 6-00625-0000	PPS# 6-00477-0000	PPS# 6-00625-0000	PPS# 6-00625-0000
				80%FED/20%STATE	90%FED/10%STATE	IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE	STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL	5TH STREET INTERSECTION 80%FED/15%STATE/ 5%LOCAL	5TH STREET INTERSECTION 90%FED/10%STATE	80%FED/20%LOCAL	100%LOCAL
				STP ROADWAY	HSIP ROADWAY	STP TRAFFIC SIGNALS	STP TRAFFIC SIGNALS	STP TRAFFIC SIGNALS	HSIP TRAFFIC SIGNALS	STP SIDEWALK	STP LIGHTING
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
542A0220	PIPE CULVERTS, CLASS A, TYPE 1 15"	FOOT	85	60						25	
542D0220	PIPE CULVERTS, CLASS D, TYPE 1 15"	FOOT	4							4	
54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"	EACH	10	10							
54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	2	2							
54248510	CONCRETE COLLAR	CU YD	6.5	6.5							
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	64	64							
550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	94	94							
550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	928	928							
550A0120	STORM SEWERS, CLASS A, TYPE 1 24"	FOOT	88	88							
55100500	STORM SEWER REMOVAL 12"	FOOT	505		505						
60218500	MANHOLES, TYPE A, 4' -DIAMETER, TYPE 3 FRAME AND GRATE	EACH	2	2							
60219000	MANHOLES, TYPE A, 4' -DIAMETER, TYPE 8 GRATE	EACH	1							1	
60219400	MANHOLES, TYPE A, 4' -DIAMETER, TYPE 12 FRAME AND GRATE	EACH	1	1							
60235700	INLETS, TYPE A, TYPE 3 FRAME AND GRATE	EACH	2	2							

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FILE NAME :	USER NAME : whitingm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\1\1084EBID\INTEG\illinois.gov\PI1001\Documents\DOT Offices\District 6\Projects\06722EB2-ht-03-509\REVISED -	DESIGNED -	REVISED -	7706						*	LOGAN	343	8	
Summary of Quantities	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT	
	PLOT DATE = 10/21/2016	DATE -	REVISED -										

CODE NO	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE								
				PPS# 6-00625-0000 80%FED/20%STATE STP ROADWAY URBAN	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE HSIP ROADWAY URBAN	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE STP TRAFFIC SIGNALS URBAN	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL STP TRAFFIC SIGNALS URBAN	PPS# 6-00625-0000 5TH STREET INTERSECTION 80%FED/15%STATE/ 5%LOCAL STP TRAFFIC SIGNALS URBAN	PPS# 6-00477-0000 5TH STREET INTERSECTION 90%FED/10%STATE HSIP TRAFFIC SIGNALS URBAN	PPS# 6-00625-0000 80%FED/20%LOCAL STP SIDEWALK URBAN	PPS# 6-00625-0000 100%LOCAL STP LIGHTING URBAN	
60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	1	1								
60236900	INLETS, TYPE A, TYPE 12 FRAME AND GRATE	EACH	1	1								
60240301	INLETS, TYPE B, TYPE 8 GRATE	EACH	1	1								
60240315	INLETS, TYPE B, TYPE 12 FRAME AND GRATE	EACH	1	1								
60255500	MANHOLES TO BE ADJUSTED	EACH	5	2	3							
60262700	INLETS TO BE RECONSTRUCTED	EACH	1							1		
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	7	2	5							
60402210	GRATES, TYPE 8	EACH	5	5								
60500060	REMOVING INLETS	EACH	7		7							
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	532	532								
60604400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18	FOOT	114	114								
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	265	265								
60608562	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12	FOOT	5951	5951								
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SO FT	30026	30026								

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FILE NAME =	USER NAME = whittingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.U. RTE. 7706	SECTION •	COUNTY LOGAN	TOTAL SHEETS 343	SHEET NO. 9
pw\1\1804EBIDINTEG\Illinois.gov\PHI00T\Documents\DOT Offices\District 6\Projects\06722\Drawings\CAD\Sheets\06722E02-sh1-03-S02	PLLOT SCALE = 100.0000' / in	CHECKED -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		
Summary of Quantities	PLLOT DATE = 10/21/2016	DATE -	REVISED -		CONTRACT NO. 72E02								
• (22)RS-6, MED & (23) RS-5													

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70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	37250	23143	14107							
*72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	11	2	9							
*72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	19	18						1		
*72400310	REMOVE SIGN PANEL - TYPE 1	SQ FT	146	128	18							
*72400320	REMOVE SIGN PANEL - TYPE 2	SQ FT	14	14								
*72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	9	1	8							
*72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	EACH	19	18						1		
*72400710	RELOCATE SIGN PANEL - TYPE 1	SQ FT	139	121	18							
*72400720	RELOCATE SIGN PANEL - TYPE 2	SQ FT	14	14								
*72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	812	630	154					28		
*73100100	BASE FOR TELESCOPING STEEL SIGN SUPPORT	EACH	10	1	9							
*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	738	272	466							
*78003100	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LETTERS AND SYMBOLS	SQ FT	1361	536	825							
*78003120	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 5"	FOOT	4741	3128	1613							

* SPECIALTY ITEM

FILE NAME =	USER NAME = whitingma	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\ill884EBIDINTEG\Illinois.gov\PMIDOT\Documents\IDOT Office\District 6\Projects\06722E02-ht-83-S01\06722E02-ht-83-S01.dwg	PROJECT DATA\CAD\Sheets\06722E02-ht-83-S01.dwg	CHECKED -	REVISED -						7706		LOGAN	343	11
Summary of Quantities	PLOT SCALE = 1/8"=1'-0"	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 72E02		
	PLOT DATE = 10/21/2016				ILLINOIS FED. AID PROJECT								

CODE NO	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE								
				PPS# 6-00625-0000 80%FED/20%STATE STP ROADWAY URBAN	PPS# 6-00477-0000 IL 10 INTERSECTION 90%FED/10%STATE HSIP ROADWAY URBAN	PPS# 6-00625-0000 IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE STP TRAFFIC SIGNALS URBAN	PPS# 6-00625-0000 STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL STP TRAFFIC SIGNALS URBAN	PPS# 6-00625-0000 5TH STREET INTERSECTION 80%FED/15%STATE/ 5%LOCAL STP TRAFFIC SIGNALS URBAN	PPS# 6-00477-0000 5TH STREET INTERSECTION 90%FED/10%STATE HSIP TRAFFIC SIGNALS URBAN	PPS# 6-00625-0000 80%FED/20%LOCAL STP SIDEWALK URBAN	PPS# 6-00625-0000 100%LOCAL STP LIGHTING URBAN	
*78003130	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 6"	FOOT	1034	230	804							
*78009005	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	FOOT	43995	28248	15747							
*78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	6208	1111	5097							
*78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	1854	583	1271							
*78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	3098	1541	1557							
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	676	303	373							
*78200020	CURB REFLECTORS	EACH	403	242	161							
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	676	303	373							
*81028350	UNDERGROUND CONDUIT, PVC, 2" DIA.	FOOT	1748			1748						
*81028370	UNDERGROUND CONDUIT, PVC, 3" DIA.	FOOT	71				71					
*81028390	UNDERGROUND CONDUIT, PVC, 4" DIA.	FOOT	272				131		141			
*81400100	HANDHOLE	EACH	2				2					
*81500100	GULFBOX JUNCTION	EACH	8			8						
*81500130	GULFBOX JUNCTION REMOVAL	EACH	9			9						

* SPECIALTY ITEM

FILE NAME : p:\11\004EBID\INTEG\Illinois.gov\PI\DOT\Do	USER NAME : whistling	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.U. RTE: 7706	SECTION *	COUNTY LOGAN	TOTAL SHEETS 343	SHEET NO. 12
PLOT SCALE = 100.0000' / 1" =		CHECKED -	REVISED -						SCALE: SHEET OF SHEETS STA. TO STA.				ILLINOIS FED. AID PROJECT
PLOT DATE = 10/21/2016		DATE -	REVISED -										
Summary of Quantities													

CODE NO	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE								
				PPS# 6-00625-0000	PPS# 6-00477-0000	PPS# 6-00625-0000	PPS# 6-00625-0000	PPS# 6-00625-0000	PPS# 6-00477-0000	PPS# 6-00625-0000	PPS# 6-00625-0000	
				80%FED/20%STATE	90%FED/10%STATE	IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE	STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL	5TH STREET INTERSECTION 80%FED/ 15%STATE/ 5%LOCAL	5TH STREET INTERSECTION 90%FED/10%STATE	80%FED/20%LOCAL	100%LOCAL	
				STP ROADWAY	HSIP ROADWAY	STP TRAFFIC SIGNALS	STP TRAFFIC SIGNALS	STP TRAFFIC SIGNALS	HSIP TRAFFIC SIGNALS	STP SIDEWALK	STP LIGHTING	
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	
*81702130	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6	FOOT	1935								1935	
*83600300	LIGHT POLE FOUNDATION, 30" DIAMETER	FOOT	6.5								6.5	
*83800650	BREAKAWAY DEVICE, COUPLING WITH STAINLESS STEEL SCREEN	EACH	1								1	
*84200804	REMOVAL OF POLE FOUNDATION	EACH	1								1	
*84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	1								1	
*85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	3			1	1	1				
*87300215	ELECTRIC CABLE IN TRENCH, SIGNAL, NO. 14 2C	FOOT	654			654						
*87300225	ELECTRIC CABLE IN TRENCH, SIGNAL, NO. 14 3C	FOOT	654			654						
*87301705	ELECTRIC CABLE IN CONDUIT, COMMUNICATION NO. 18 3 PAIR	FOOT	2973			1052	691	1230				
*87500600	TRAFFIC SIGNAL POST, 10 FT.	EACH	2			2						
*87800100	CONCRETE FOUNDATION, TYPE A	FOOT	6			6						
*87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	23.5				23.5					
*87900200	DRILL EXISTING HANDHOLE	EACH	20			16	2		2			
*88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2			2						

* SPECIALTY ITEM

FILE NAME :	USER NAME : whitingm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Summary of Quantities									7706	.	LOGAN	343	13
PLOT SCALE = 1/8" = 1'-0"					CHECKED -	REVISED -	SCALE:		SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT		
PLOT DATE = 10/21/2016					DATE -	REVISED -	CONTRACT NO. 72E02						

2016.10

CONSTRUCTION CODE

CODE NO	ITEM	UNIT	TOTAL QUANTITY	PPS# 6-00625-0000	PPS# 6-00477-0000	PPS# 6-00625-0000	PPS# 6-00625-0000	PPS# 6-00625-0000	PPS# 6-00477-0000	PPS# 6-00625-0000	PPS# 6-00625-0000
				80%FED/20%STATE	90%FED/10%STATE	IL 10 INTERSECTION & INTERCONNECT 80%FED/20%STATE	STAHLHUT DRIVE INTERSECTION 80%FED/13.33%STATE/ 6.67%LOCAL	5TH STREET INTERSECTION 80%FED/15%STATE/ 5%LOCAL	5TH STREET INTERSECTION 90%FED/10%STATE	80%FED/20%LOCAL	100%LOCAL
				STP ROADWAY	HSIP ROADWAY	STP TRAFFIC SIGNALS	STP TRAFFIC SIGNALS	STP TRAFFIC SIGNALS	HSIP TRAFFIC SIGNALS	STP SIDEWALK	STP LIGHTING
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
Z0023602	GRANULAR CULVERT BACKFILL	CU YD	138	138							
Z0033072	VIDEO VEHICLE DETECTION SYSTEM	EACH	3			1	1	1			
Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	30189	13940	16249						
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	LSUM	1	1							
Z0076600	TRAINEES	HOUR	4000	4000							

* SPECIALTY ITEM

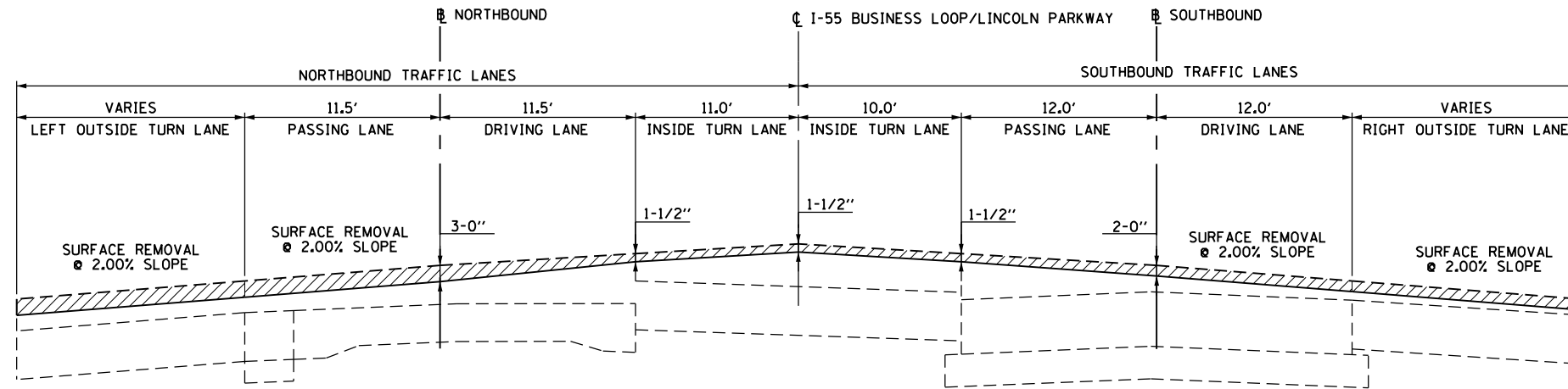
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	PLOT SCALE :	CHECKED :	REVISED :
	100.0000 1/16"		
	PLOT DATE :	DATE :	REVISED :
	10/21/2016		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

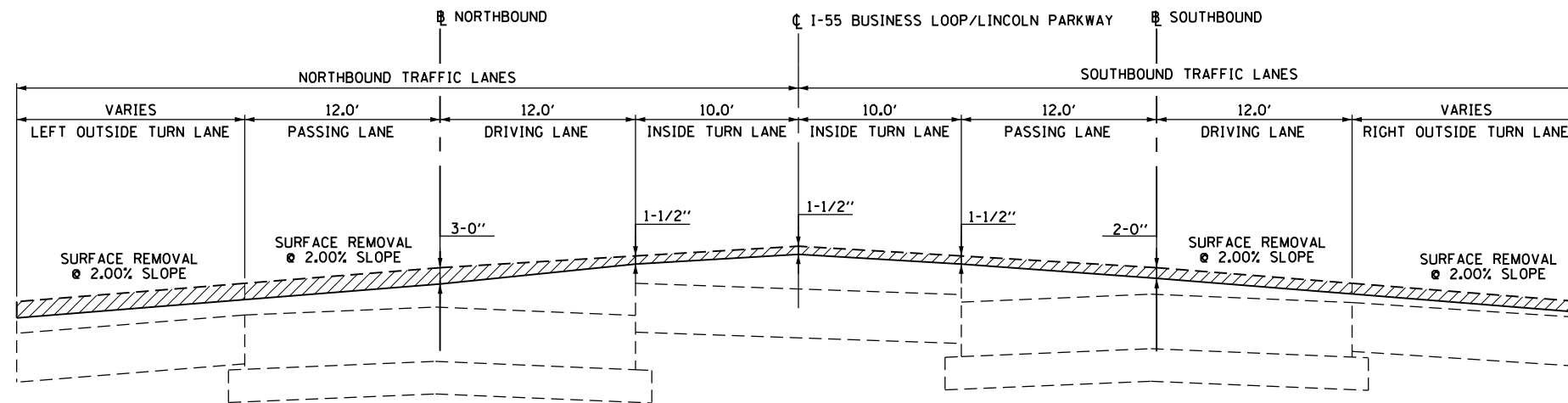
SUMMARY OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	*	LOGAN	343	16
			CONTRACT NO. 72E02	
ILLINOIS FED. AID PROJECT				



SURFACE REMOVAL, VARIABLE DEPTH DETAIL
I-55 BUSINESS LOOP/LINCOLN PARKWAY
 STATION 1+32.17 TO STATION 17+60.00
 STATION 21+75.00 TO RIGHT STATION 37+00.00



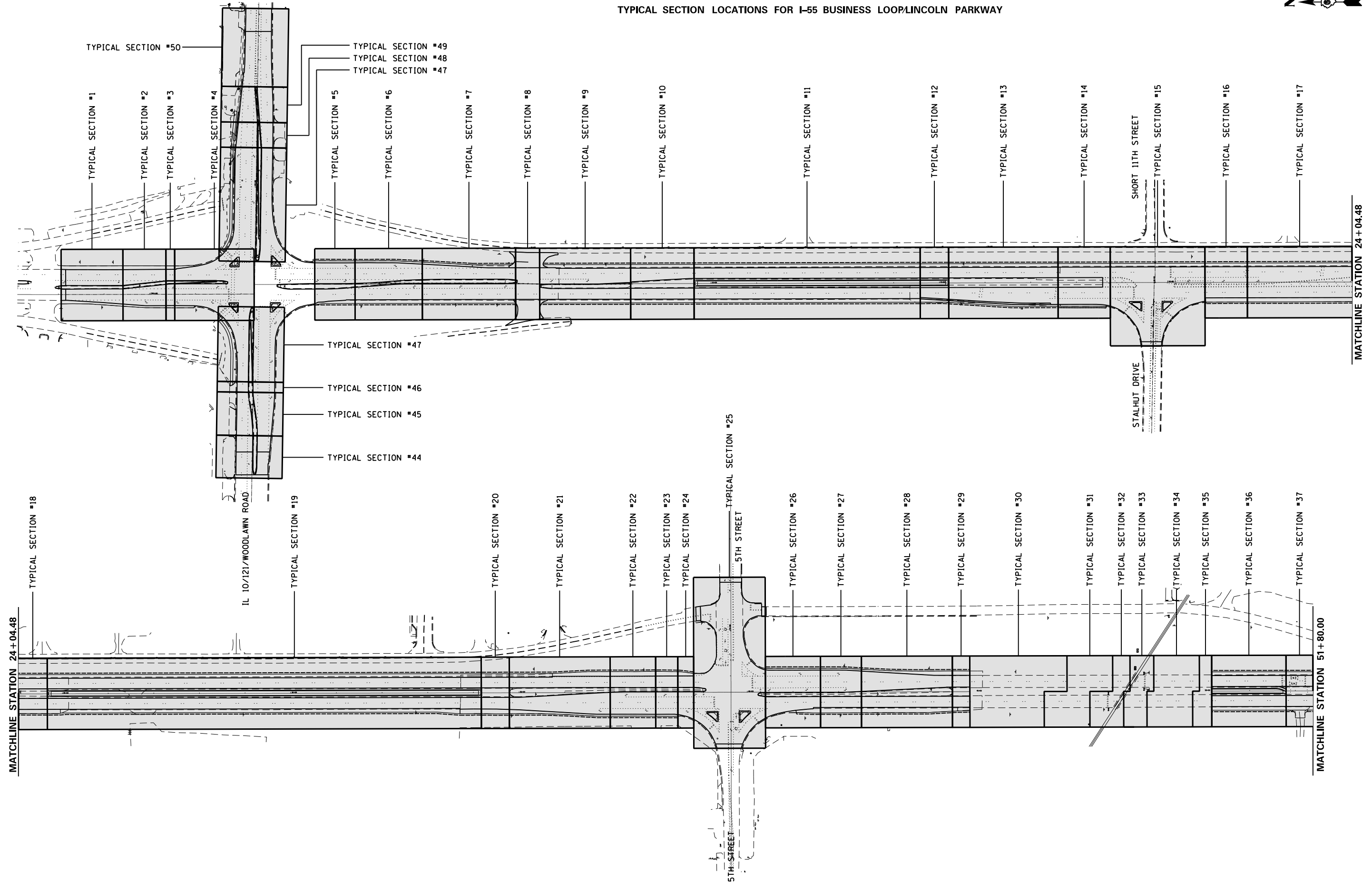
SURFACE REMOVAL, VARIABLE DEPTH DETAIL
I-55 BUSINESS LOOP/LINCOLN PARKWAY
 RIGHT STATION 42+50.00 TO RIGHT STATION 46+50.00
 RIGHT STATION 49+75.00 TO RIGHT STATION 73+50.00
 LEFT STATION 42+50.00 TO LEFT STATION 47+00.00
 LEFT STATION 49+00.00 TO LEFT STATION 73+50.00

INTERSECTIONS
IL 10/121 WOODLAWN ROAD
STALHUT DRIVE
5TH STREET

ALL LANES SHALL BE 2" SURFACE REMOVAL

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS MILLING DETAIL				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\IL\084EBID\INTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 6\Projects\0672\Drawings\EA\Sheets\0672E02-sht-04-typ	DRAWN	REVISION	REVISION						7706	.	LOGAN	343	17
Typical Sections	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -		SCALE:	SHEET	OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 72E02		
	PLOT DATE = 10/21/2016	DATE -	REVISED -										

TYPICAL SECTION LOCATIONS FOR I-55 BUSINESS LOOP/LINCOLN PARKWAY



FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
p:\11\084EBID\INTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 6\Projects\0672ED\Drawings\60\Sheets\0672E02-sht-04-typ		DRAWN -	REVISED -
Typical Sections	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
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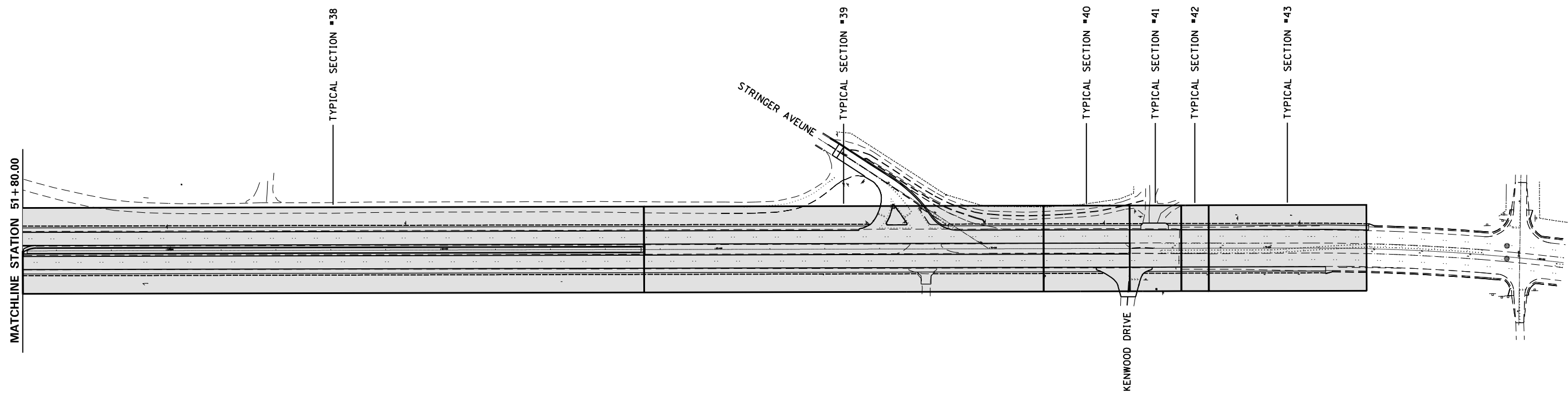
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
LOCATION MAP

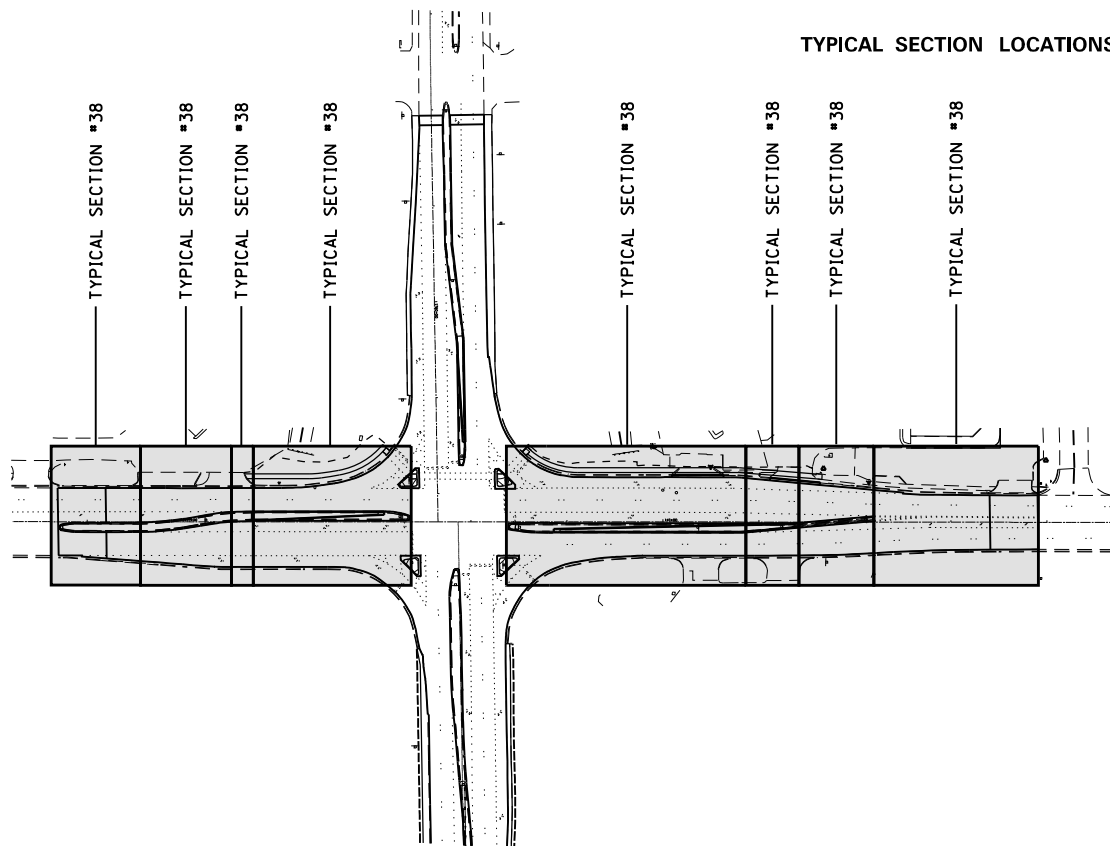
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	*	LOGAN	343	18
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72E02	

TYPICAL SECTION LOCATIONS FOR I-55 BUSINESS LOOP/LINCOLN PARKWAY



TYPICAL SECTION LOCATIONS FOR IL 1012/WOODLAWN ROAD



FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
p:\11\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 6\Projects\0672EDRAWING\0672E02-sht-04-typ		DRAWN -	REVISED -
Typical Sections	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/21/2016	DATE -	REVISED -

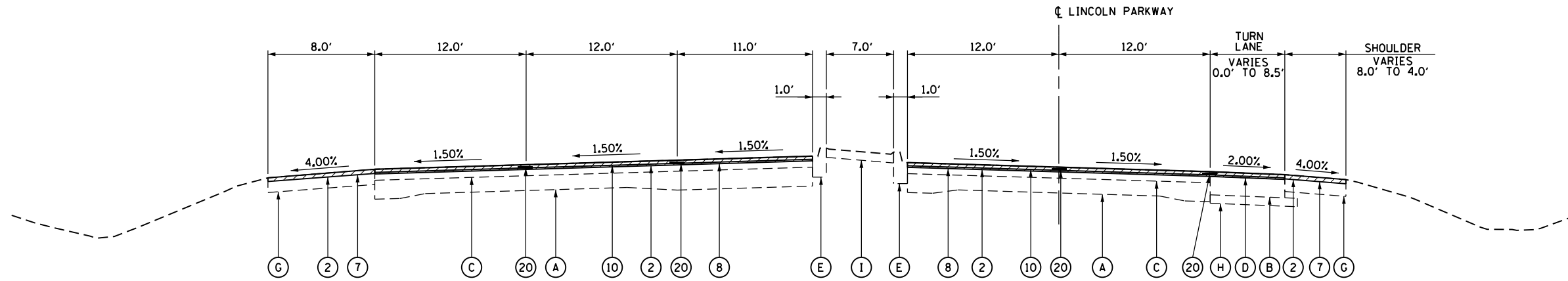
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
LOCATION MAP

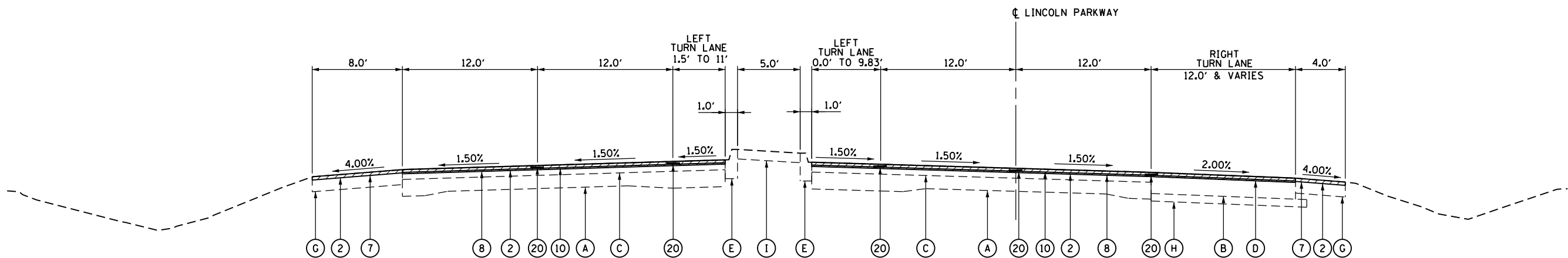
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	*	LOGAN	343	19
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72E02	

* (22)RS-6, MED & (23) RS-5



TYPICAL SECTION #1
I-55 BUSINESS LOOP/LINCOLN PARKWAY
NORTH OF IL 10 INTERSECTION
 STATION 1758+05.85 TO STATION 1759+41.28

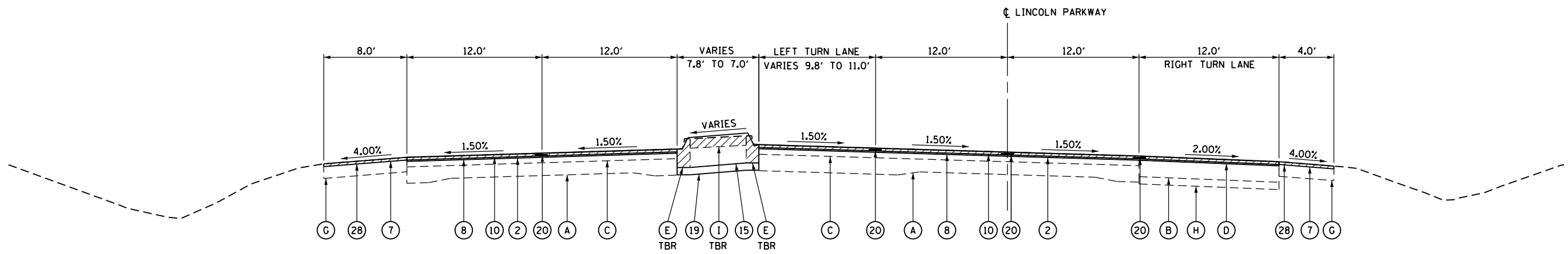


TYPICAL SECTION #2
I-55 BUSINESS LOOP/LINCOLN PARKWAY
NORTH OF IL 10 INTERSECTION
 STATION 1759+41.28 TO STATION 1760+36.63

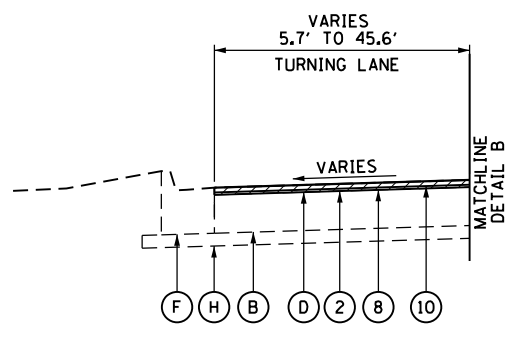
TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

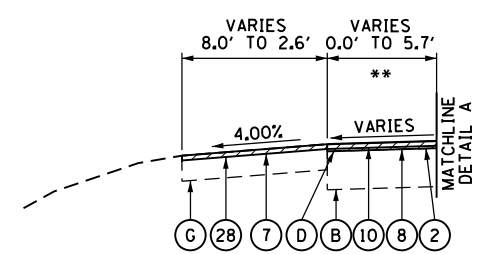
FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS I-55 BUSINESS LOOP/LINCOLN PARKWAY			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 6\Projects\0672ED\Drawings\EA0\Sheets\0672E02-sht-04-typ		CHECKED -	REVISED -		7706	.	LOGAN	343	20			
Typical Sections	PLOT SCALE = 10.0000' / in.	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 72E02				



TYPICAL SECTION #3
I-55 BUSINESS LOOP/LINCOLN PARKWAY
NORTH OF IL 10 INTERSECTION
 STATION 1760+36.63 TO STATION 1760+54.93

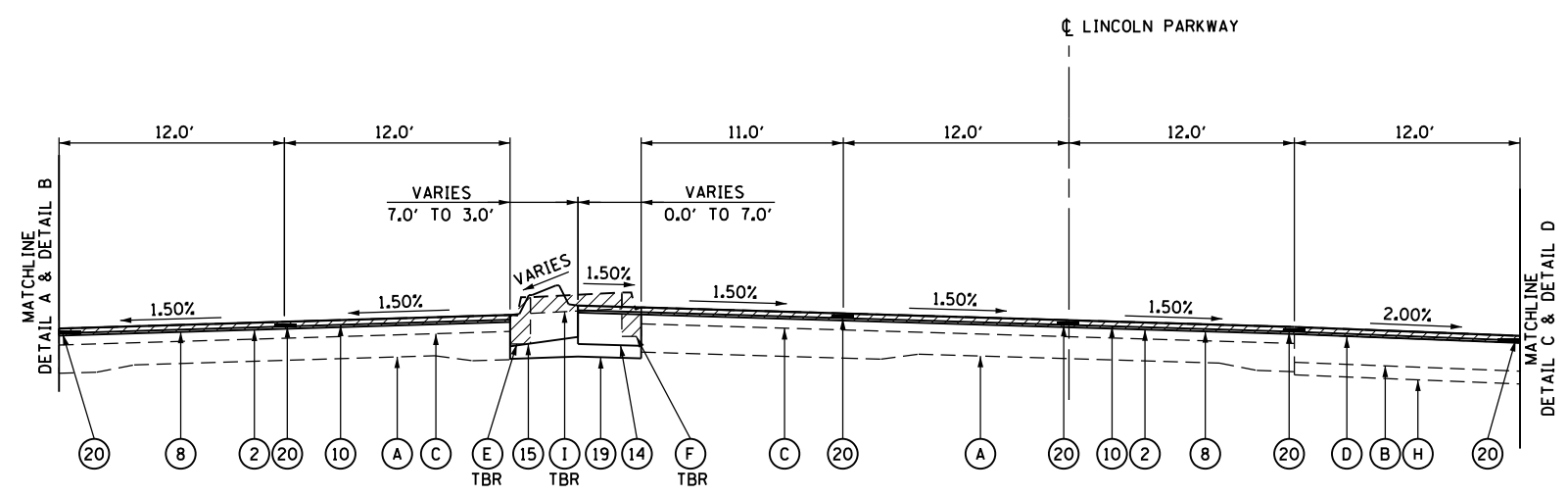


DETAIL B
 STATION 1760+96.87 TO STATION 1761+70.82

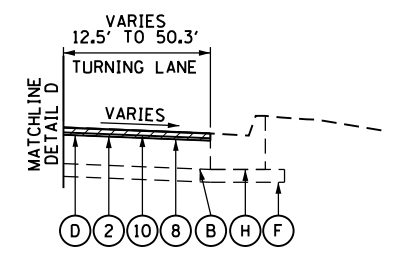


** TURN LANE STATION 1760+58.46 TO STATION 1760+96.87

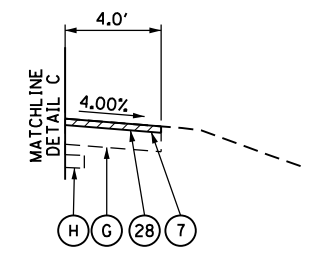
DETAIL A
 STATION 1760+54.93 TO STATION 1760+96.87



TYPICAL SECTION #4
I-55 BUSINESS LOOP/LINCOLN PARKWAY
NORTH OF IL 10 INTERSECTION
 STATION 1760+54.93 TO STATION 1762+29.37



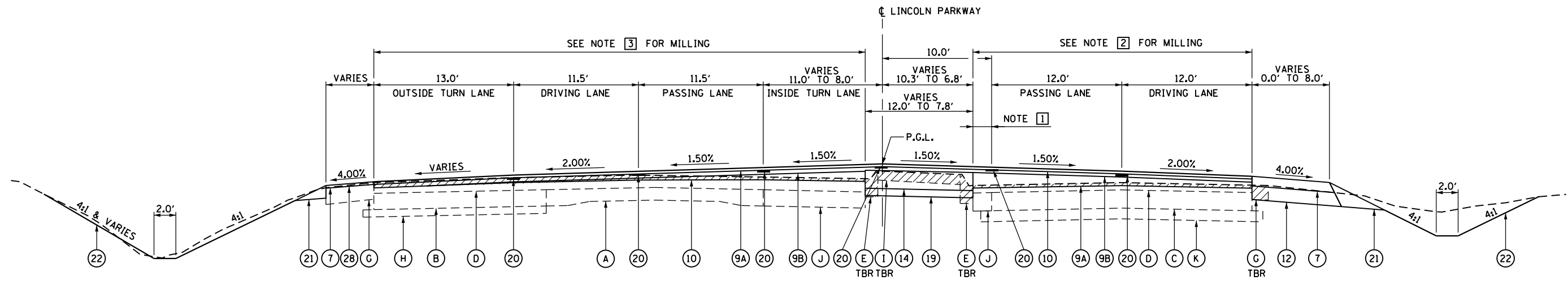
DETAIL D
 STATION 1760+97.52 TO STATION 1761+70.82



DETAIL D
 STATION 1760+54.93 TO STATION 1760+97.52

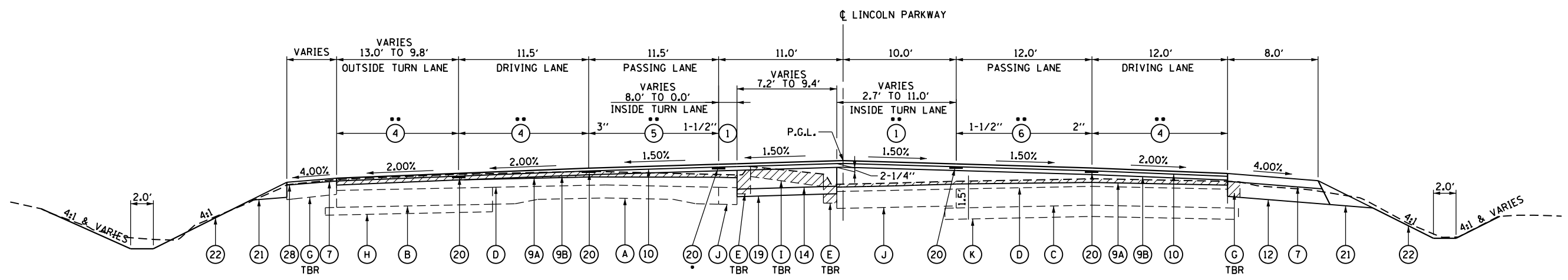
TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)



TYPICAL SECTION #5
I-55 BUSINESS LOOP/LINCOLN PARKWAY
TRANSITION FROM IL 10121/WOODLAWN ROAD INTERSECTION
 STATION 1+32.17 TO STATION 2+20.00

- NOTES:
- 1 TURN LANE - STA 1+82.16, 0.0' TO STA. 2+20.00, 2.7'
 - 2 SB SURFACE REMOVAL MAINTAIN 2" STATION 2+20.00 TO STATION 1+32.17
 - 3 NB SURFACE REMOVAL TRANSITION FROM 3" STATION 2+20.00 TO 2" STATION 1+32.17



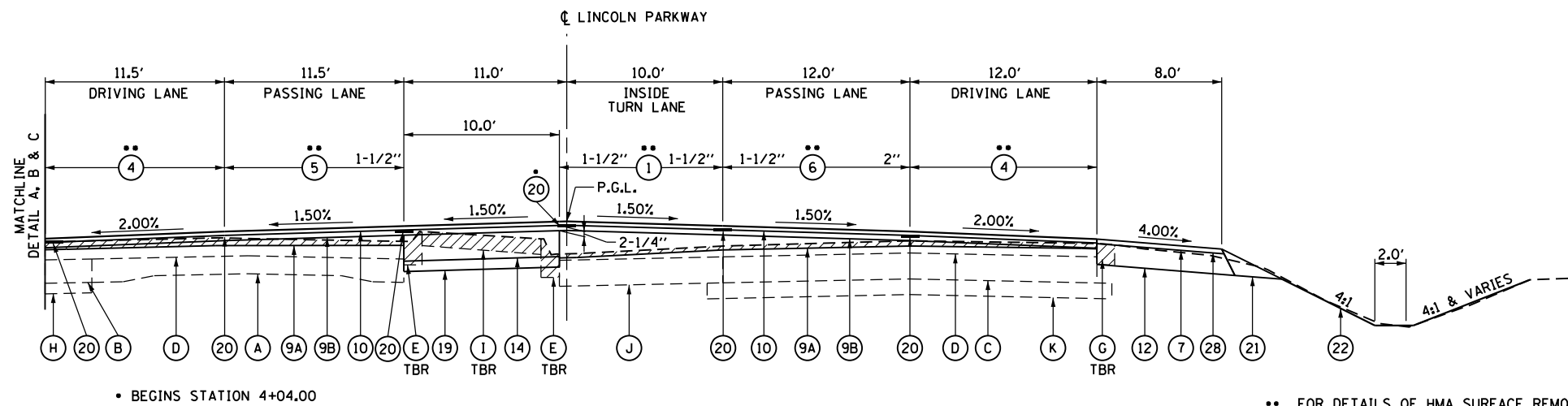
• ENDS AT STATION 2+32.49

TYPICAL SECTION #6
I-55 BUSINESS LOOP/LINCOLN PARKWAY
 STATION 2+20.00 TO STATION 3+68.00

•• FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL

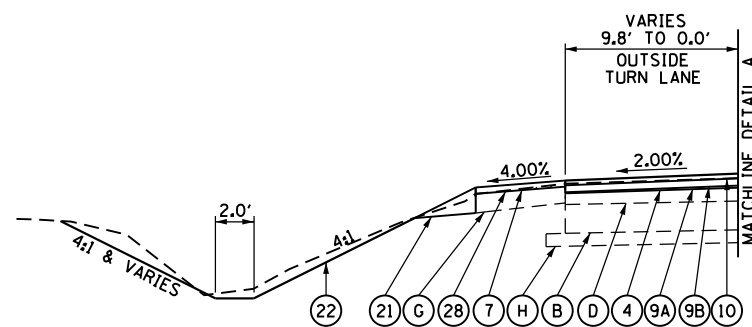
TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

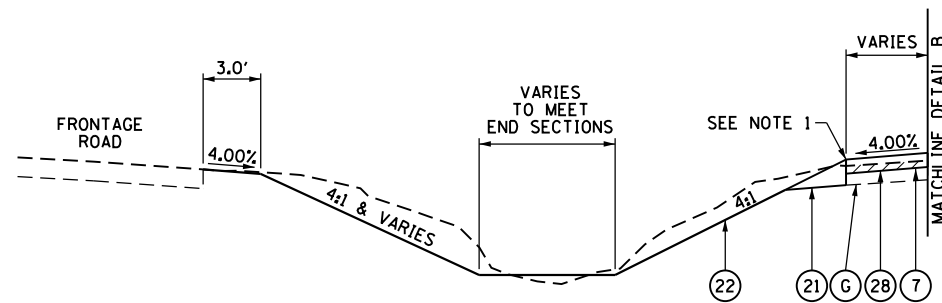


TYPICAL SECTION #7
I-55 BUSINESS LOOP/LINCOLN PARKWAY
 STATION 3+68.00 TO STATION 5+72.62

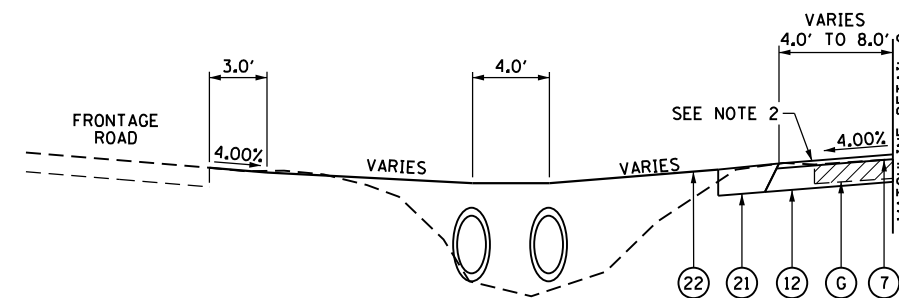
•• FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL



DETAIL A
 STATION 3+68.00 TO STATION 5+13.30



DETAIL B
 STATION 5+13.30 TO STATION 5+25.00



DETAIL C
 STATION 5+25.00 TO STATION 5+72.62

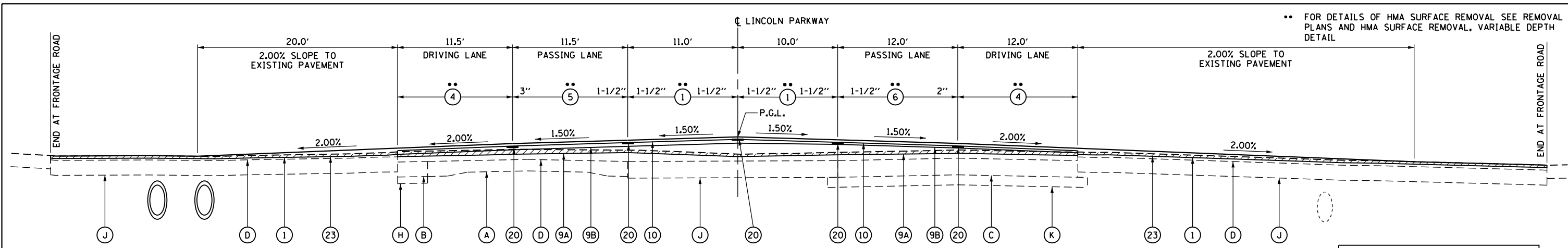
NOTES:

- EXISTING SHOULDER TO REMAIN STATION 3+68.00 TO STATION 4+77.42.
- PROPOSED SHOULDER STATION 4+77.42 TO STATION 5+57.68

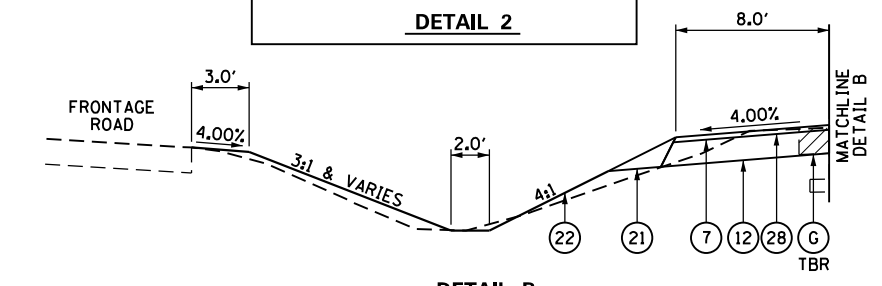
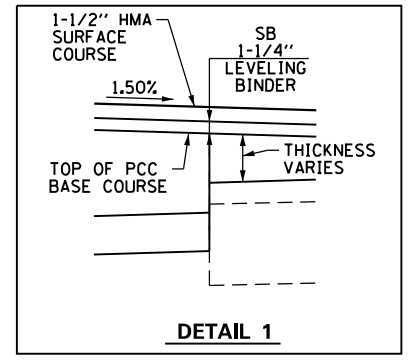
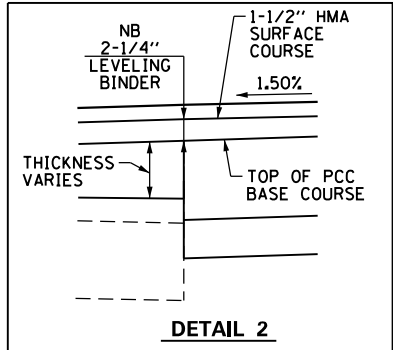
TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

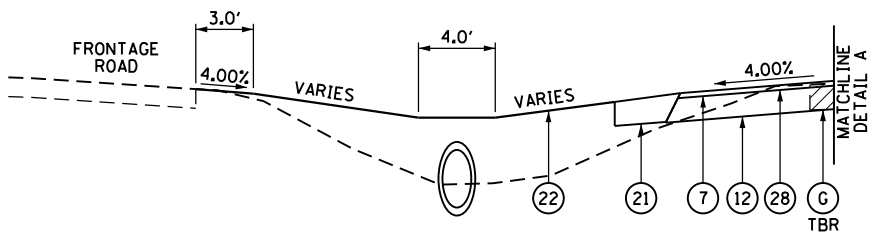
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pw:\IL\084EBID\INTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 6\Projects\0672\Drawings\EA0\Sheets\0672E02-sht-04-typ\Typical Sections	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -			7706		LOGAN	343	23
	PLOT DATE = 10/21/2016	DATE -	REVISED -			SCALE: SHEET OF SHEETS STA. TO STA.		ILLINOIS FED. AID PROJECT CONTRACT NO. 72E02		



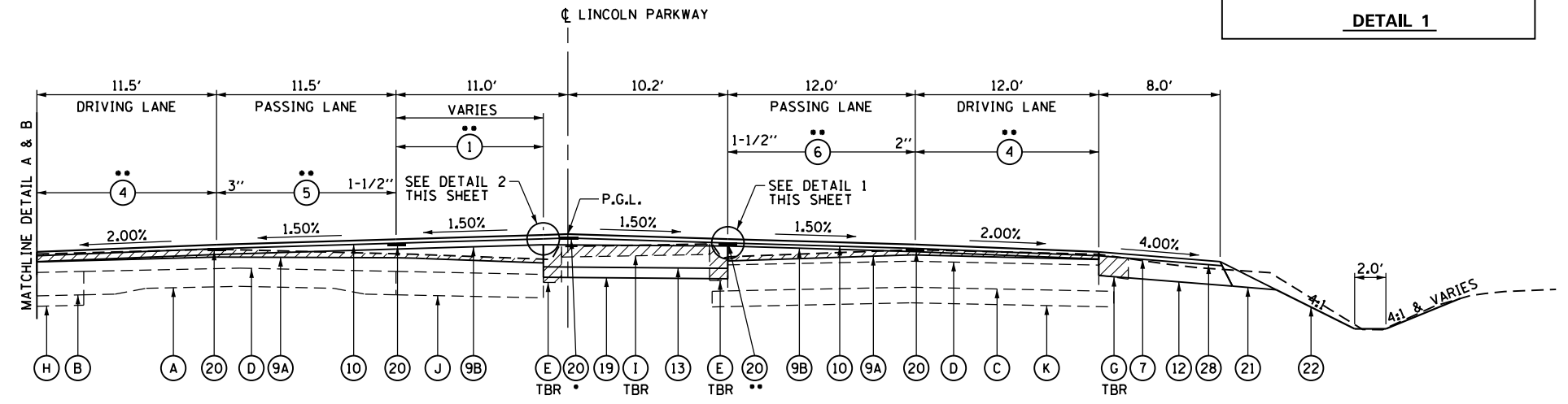
TYPICAL SECTION #8
I-55 BUSINESS LOOP LINCOLN PARKWAY
CROSSOVER AREA
 STATION 5+72.62 TO STATION 6+25.03



DETAIL B
 STATION 7+25.00 TO STATION 8+25.89



DETAIL A
 STATION 6+25.03 TO STATION 7+25.00



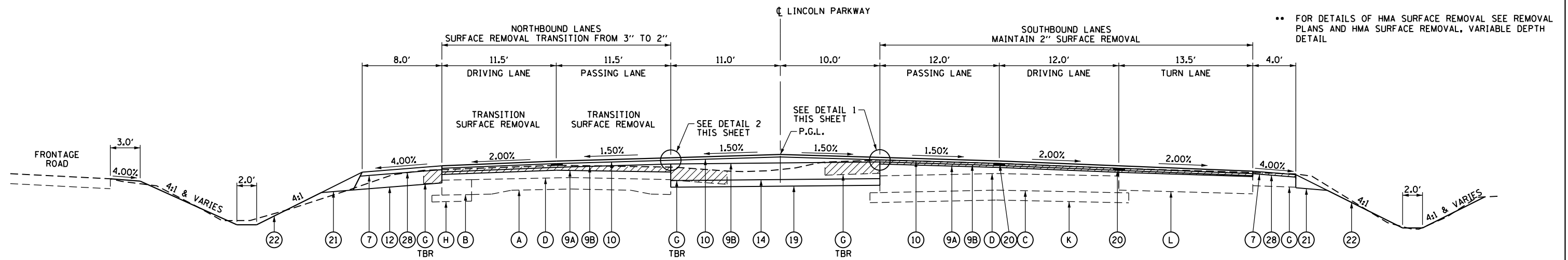
TYPICAL SECTION #9
I-55 BUSINESS LOOP LINCOLN PARKWAY
 STATION 6+25.03 TO STATION 8+25.89

• ENDS AT STATION 7+66.00
 •• ENDS AT STATION 6+50.14

•• FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL

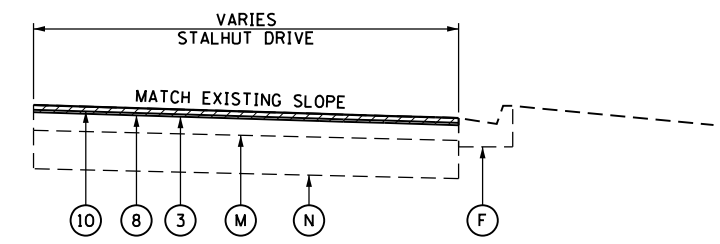
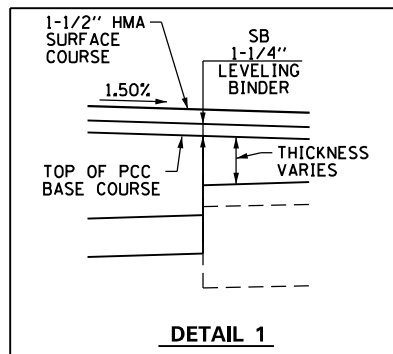
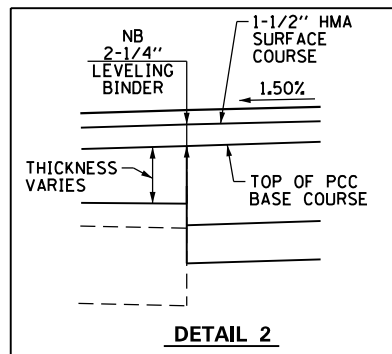
TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

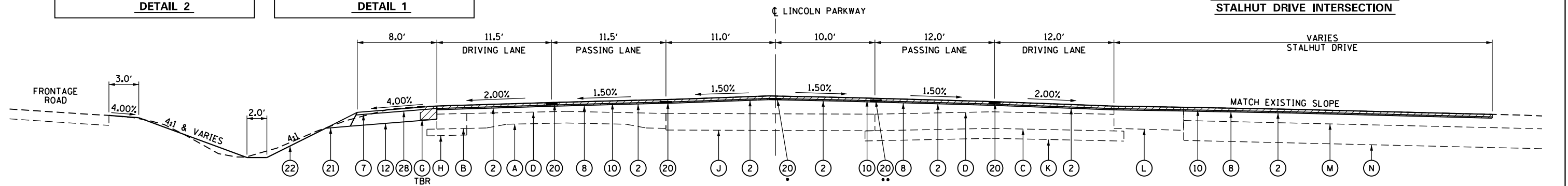


•• FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL

TYPICAL SECTION #14
I-55 BUSINESS LOOP/LINCOLN PARKWAY
TRANSITION TO STALHUT DRIVE INTERSECTION
 STATION 17+60.00 TO STATION 18+75.00



CURB & GUTTER END SECTION
STALHUT DRIVE INTERSECTION



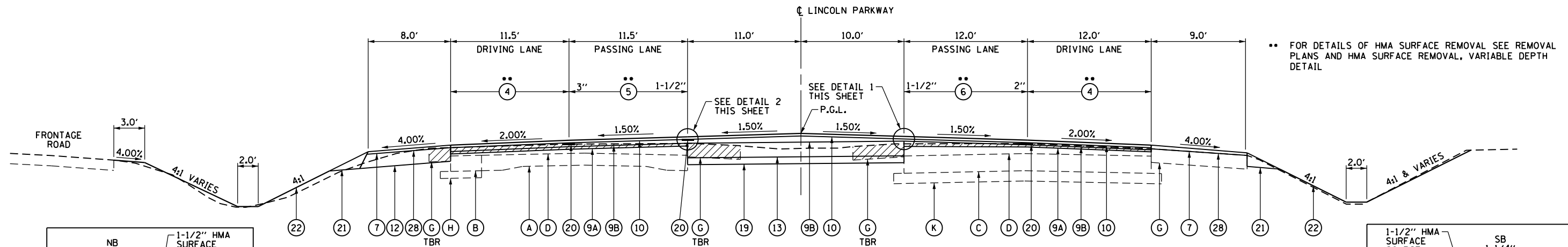
• BEGINS STATION 19+50.40
 •• BEGINS STATION 19+55.21

TYPICAL SECTION #15
I-55 BUSINESS LOOP/LINCOLN PARKWAY
STALHUT DRIVE INTERSECTION
 STATION 18+75.00 TO STATION 20+82.79

TYPICAL SECTION LEGEND

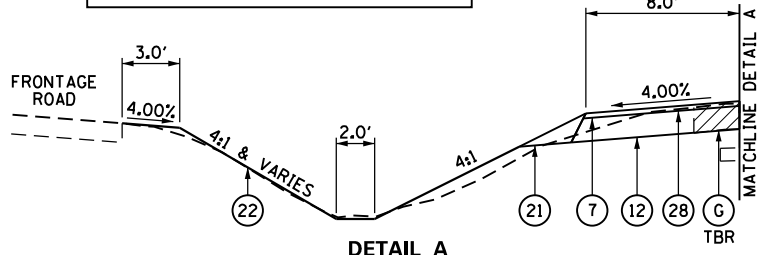
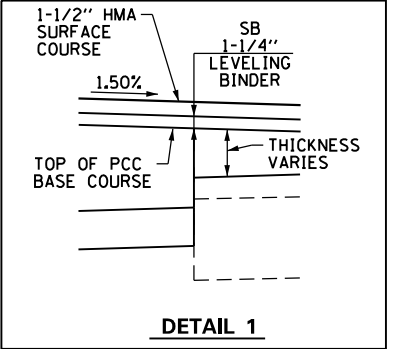
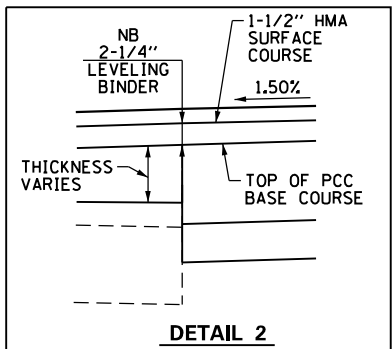
(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

• (22)RS-6, MED & (23) RS-5

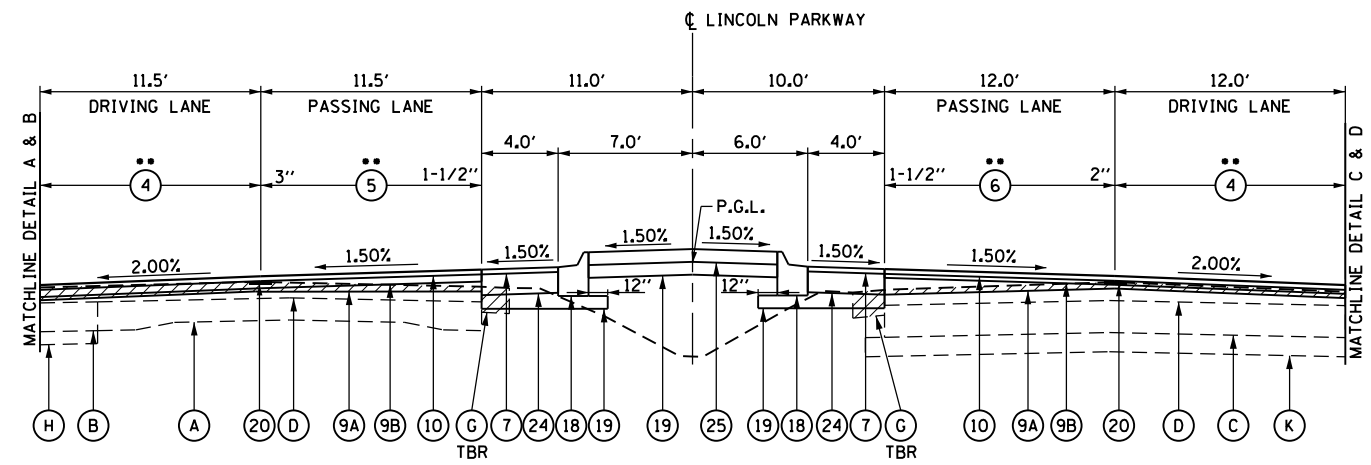


•• FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL

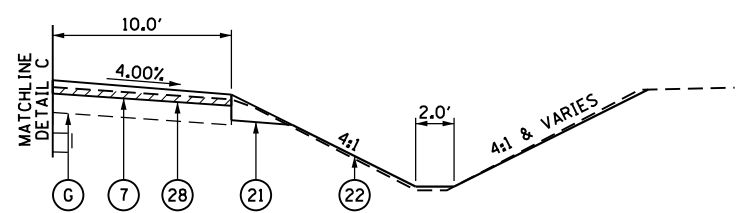
TYPICAL SECTION #18
I-55 BUSINESS LOOP/LINCOLN PARKWAY
 STATION 24+04.48 TO STATION 24+68.16



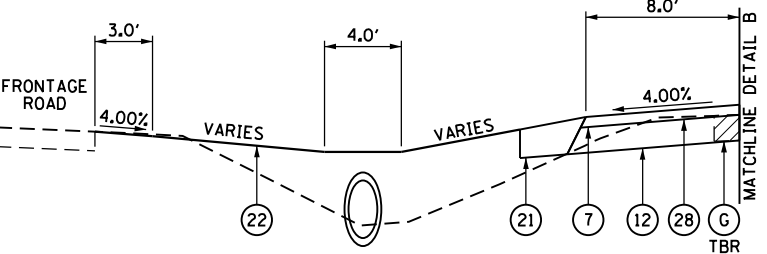
DETAIL A
 STATION 24+68.16 TO STATION 31+70.00
 STATION 34+00.00 TO STATION 34+18.04



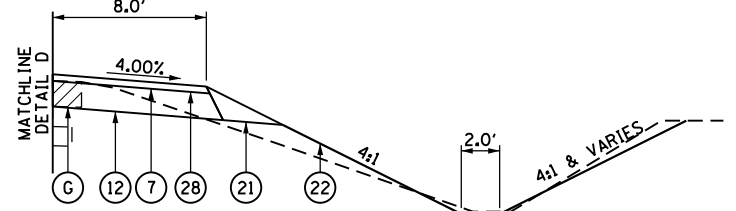
TYPICAL SECTION #19
I-55 BUSINESS LOOP/LINCOLN PARKWAY
 STATION 24+68.16 TO STATION 34+18.04



DETAIL C
 STATION 24+68+16 TO STATION 24+95.37
 STATION 33+70.56 TO STATION 34+18.04



DETAIL B
 STATION 31+00.00 TO STATION 34+00.00

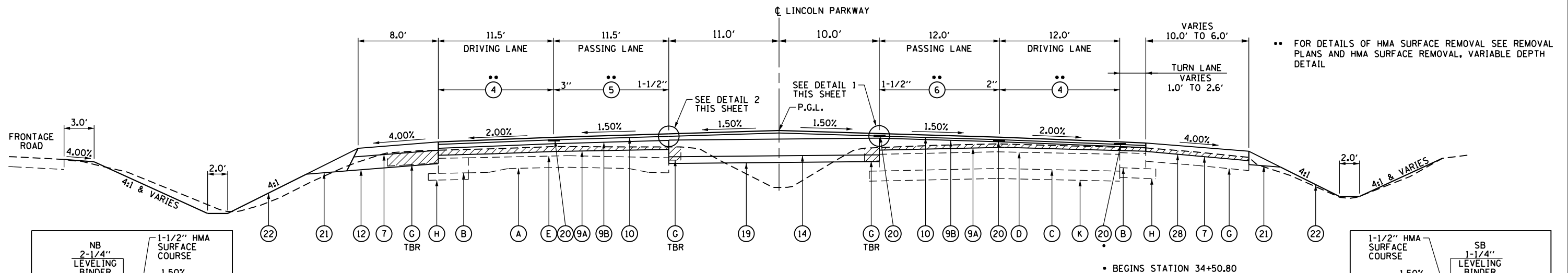


DETAIL D
 STATION 24+95.37 TO STATION 33+70.56

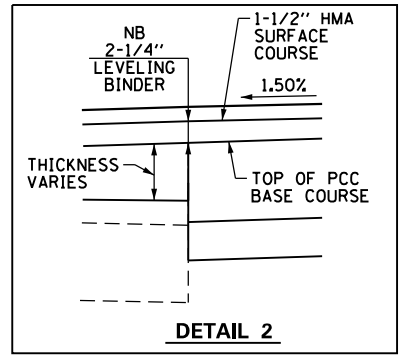
•• FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL

TYPICAL SECTION LEGEND

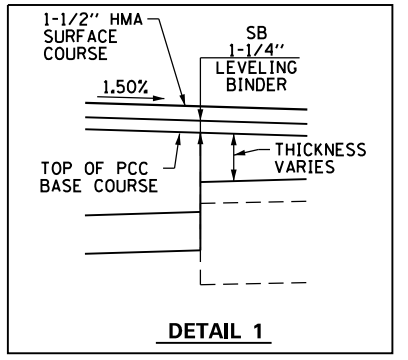
(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)



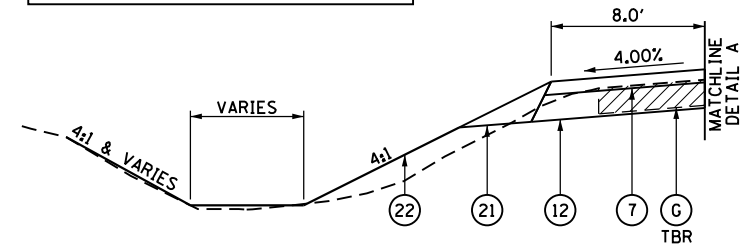
TYPICAL SECTION #20
I-55 BUSINESS LOOP/LINCOLN PARKWAY
 STATION 34+18.04 TO STATION 34+79.17



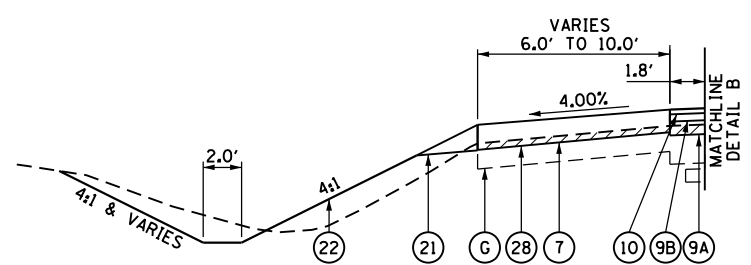
DETAIL 2



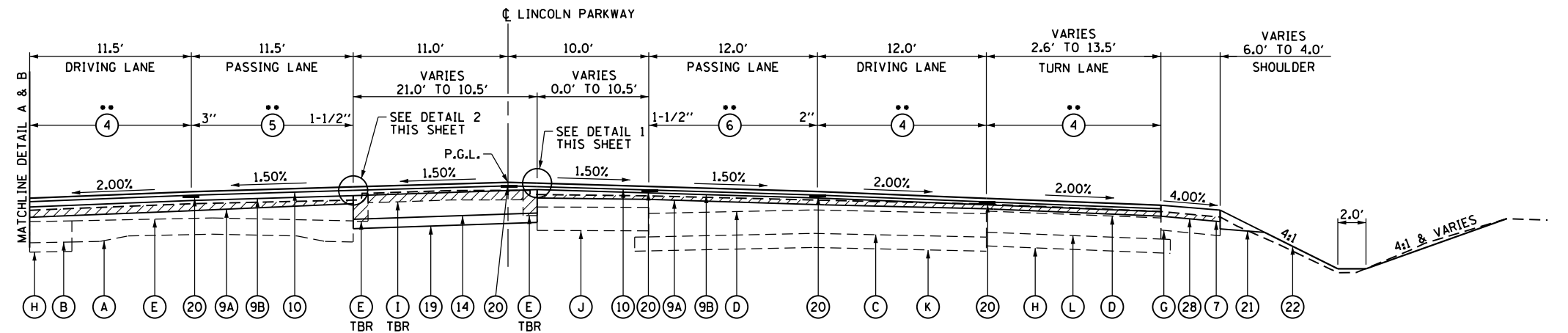
DETAIL 1



DETAIL A
 STATION 34+79.17 TO STATION 35+73.21



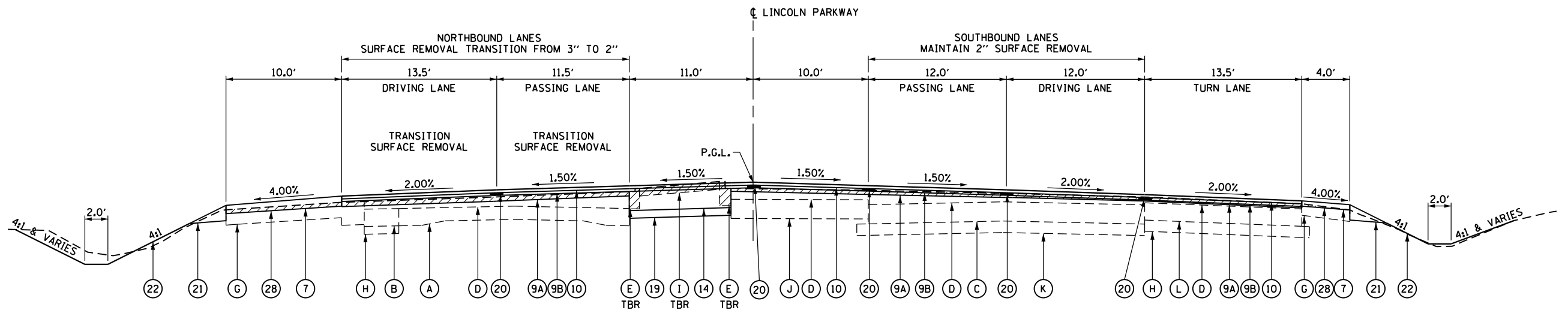
DETAIL B
 STATION 35+73.21 TO STATION 37+00.00



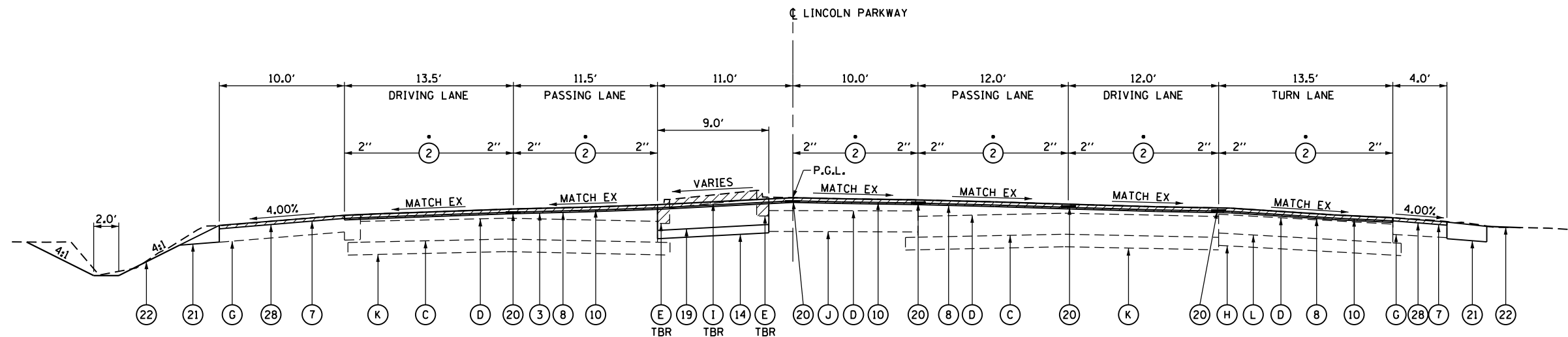
TYPICAL SECTION #21
I-55 BUSINESS LOOP/LINCOLN PARKWAY
 STATION 34+79.17 TO STATION 37+00.00

TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)



TYPICAL SECTION #22
I-55 BUSINESS LOOP/LINCOLN PARKWAY
TRANSITION TO 5TH STREET INTERSECTION
 STATION 37+00.00 TO STATION 38+00.00



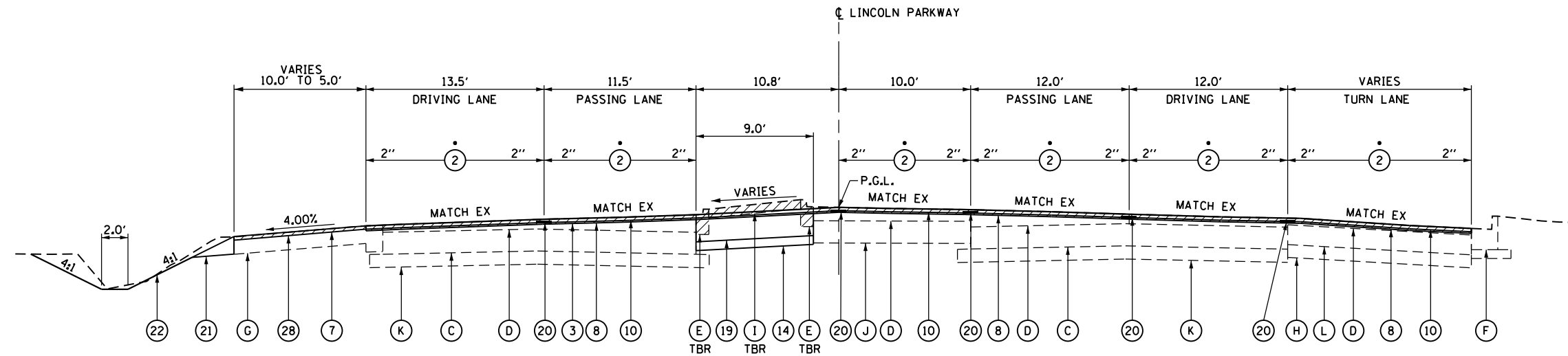
• SURFACE REMOVAL SHALL BE 2" FOR ALL LANES OF TRAFFIC

TYPICAL SECTION #23
I-55 BUSINESS LOOP/LINCOLN PARKWAY
5TH STREET INTERSECTION
 STATION 38+00.00 TO STATION 38+48.18

TYPICAL SECTION LEGEND

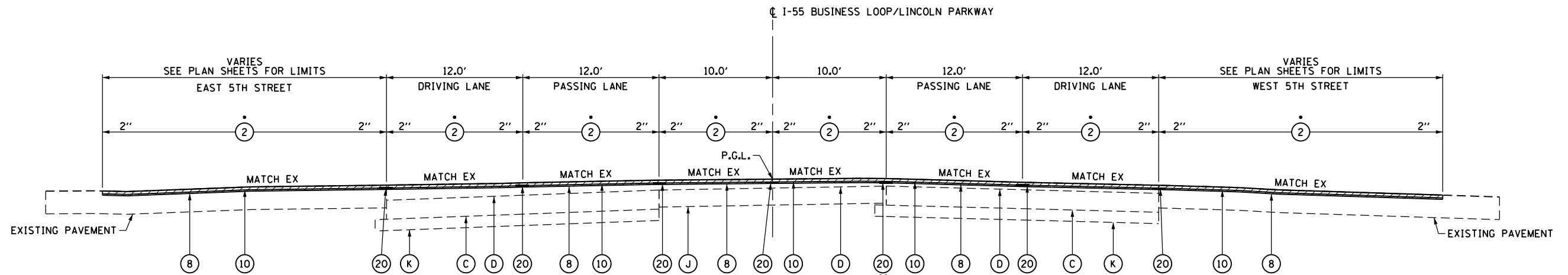
(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS I-55 BUSINESS LOOP/LINCOLN PARKWAY			F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 6\Projects\0672EDR\Drawings\EA0\Sheets\0672E02-sht-04-typ		CHECKED -	REVISED -		7706	•	LOGAN	343	31			
Typical Sections	PLOT SCALE = 10.0000' / in.	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 72E02				
	PLOT DATE = 10/21/2016							ILLINOIS FED. AID PROJECT				



• SURFACE REMOVAL SHALL BE 2" FOR ALL LANES OF TRAFFIC

TYPICAL SECTION #24
I-55 BUSINESS LOOP/LINCOLN PARKWAY
5TH STREET INTERSECTION
 STATION 38+48.18 TO STATION 38+83.08



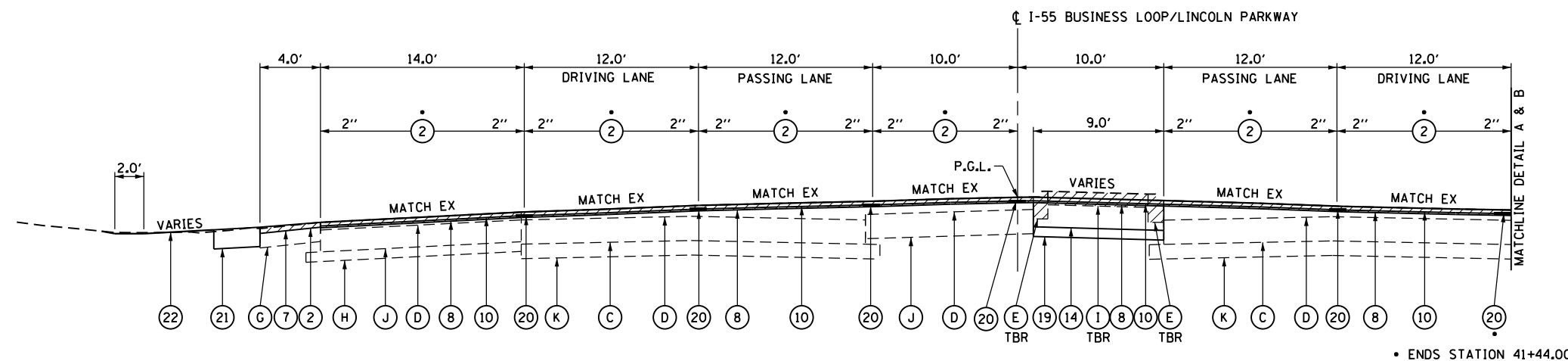
• BEGINS STATION 38+95.56 •• ENDS STATION 40+32.54

• SURFACE REMOVAL SHALL BE 2" FOR ALL LANES OF TRAFFIC
 5TH STREET INCLUDED

TYPICAL SECTION #25
I-55 BUSINESS LOOP/LINCOLN PARKWAY
5TH STREET INTERSECTION
 STATION 38+83.08 TO STATION 40+39.89

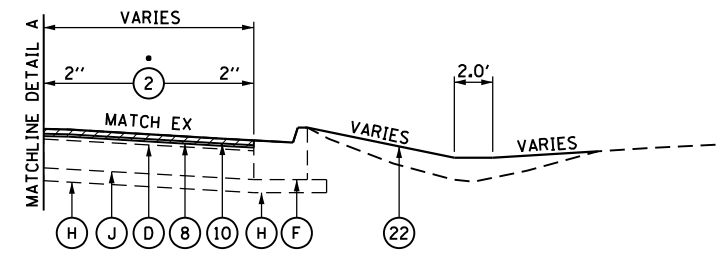
TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

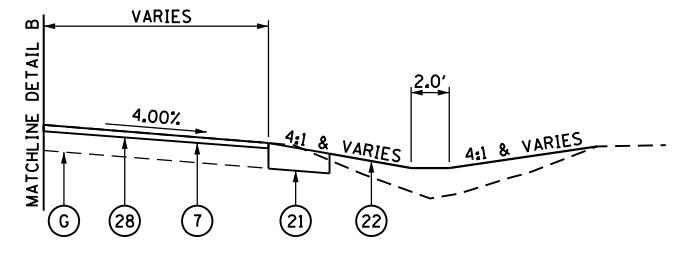


TYPICAL SECTION #26
I-55 BUSINESS LOOP/LINCOLN PARKWAY
5TH STREET INTERSECTION
 STATION 40+39.89 TO STATION 41+60.00

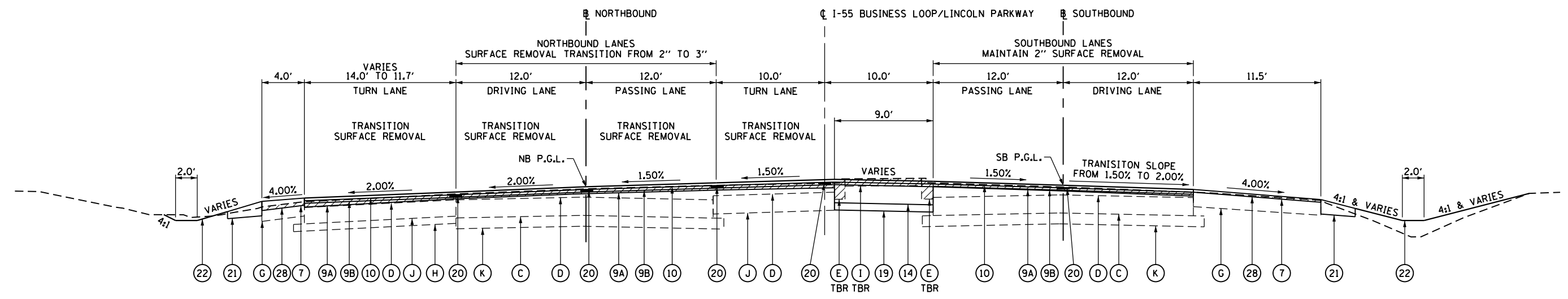
• SURFACE REMOVAL SHALL BE 2" FOR ALL LANES OF TRAFFIC



DETAIL A
 STATION 40+39.89 TO STATION 41+06.32



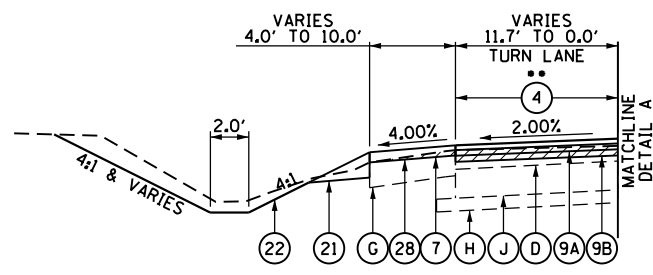
DETAIL B
 STATION 41+06.32 TO STATION 41+60.00



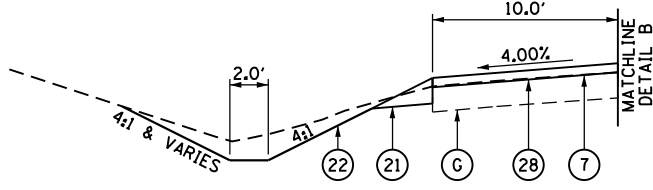
TYPICAL SECTION #27
I-55 BUSINESS LOOP/LINCOLN PARKWAY
TRANSITION FROM 5TH STREET INTERSECTION
 STATION 41+60.00 TO STATION 42+50.00

TYPICAL SECTION LEGEND

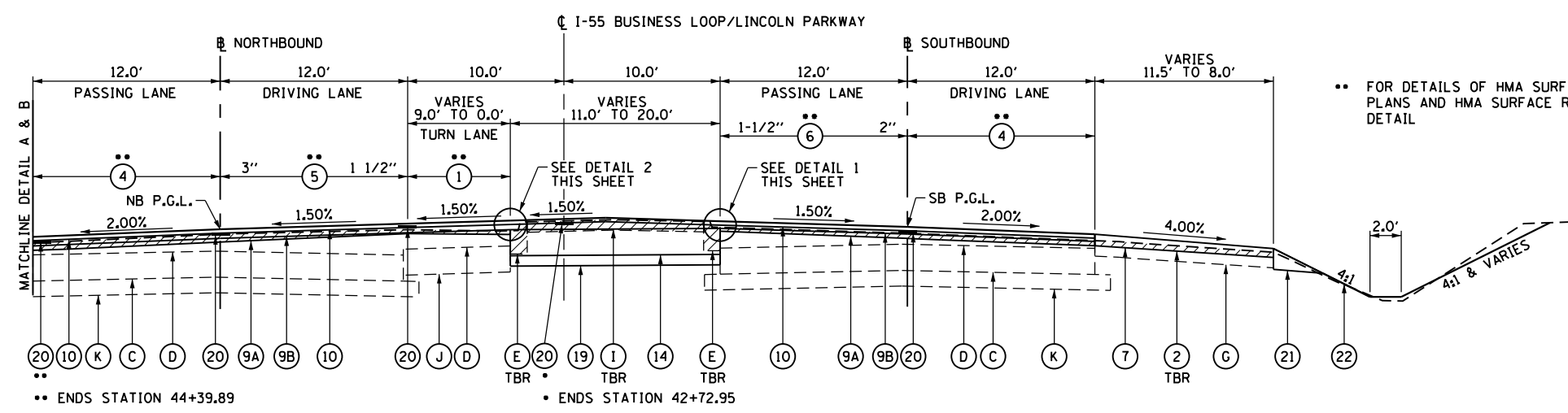
(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)



DETAIL A
STATION 42+50.00 TO STATION 44+08.75



DETAIL B
STATION 44+08.75 TO STATION 44+50.00

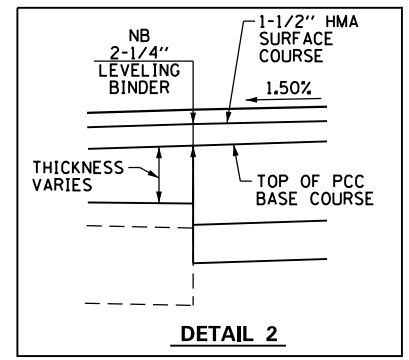


TYPICAL SECTION #28
I-55 BUSINESS LOOP/LINCOLN PARKWAY
STATION 42+50.00 TO STATION 44+50.00

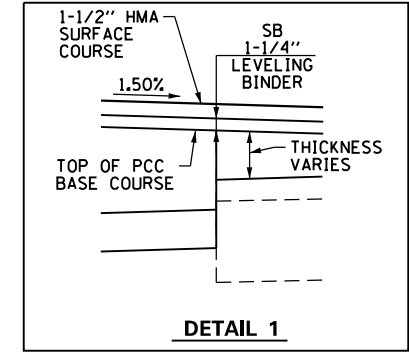
•• FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL

•• ENDS STATION 44+39.89

• ENDS STATION 42+72.95

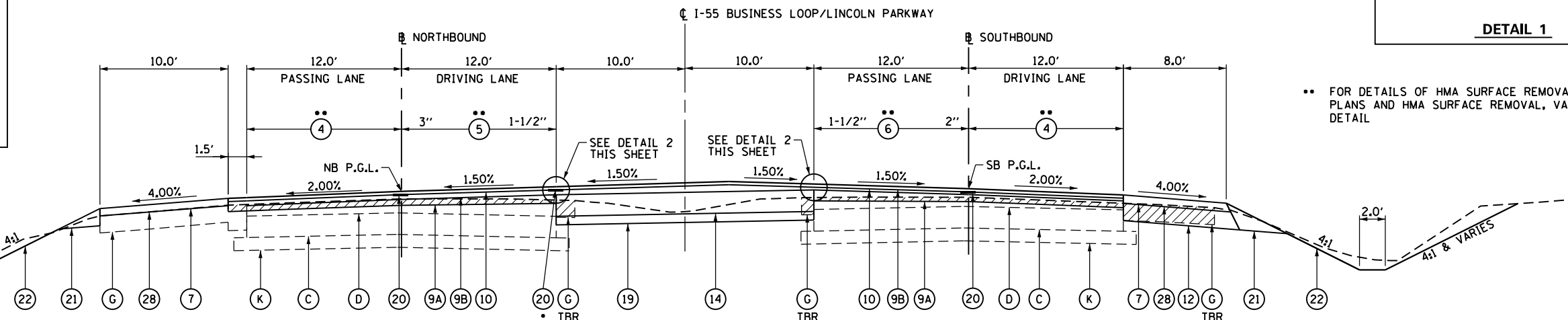


DETAIL 2



DETAIL 1

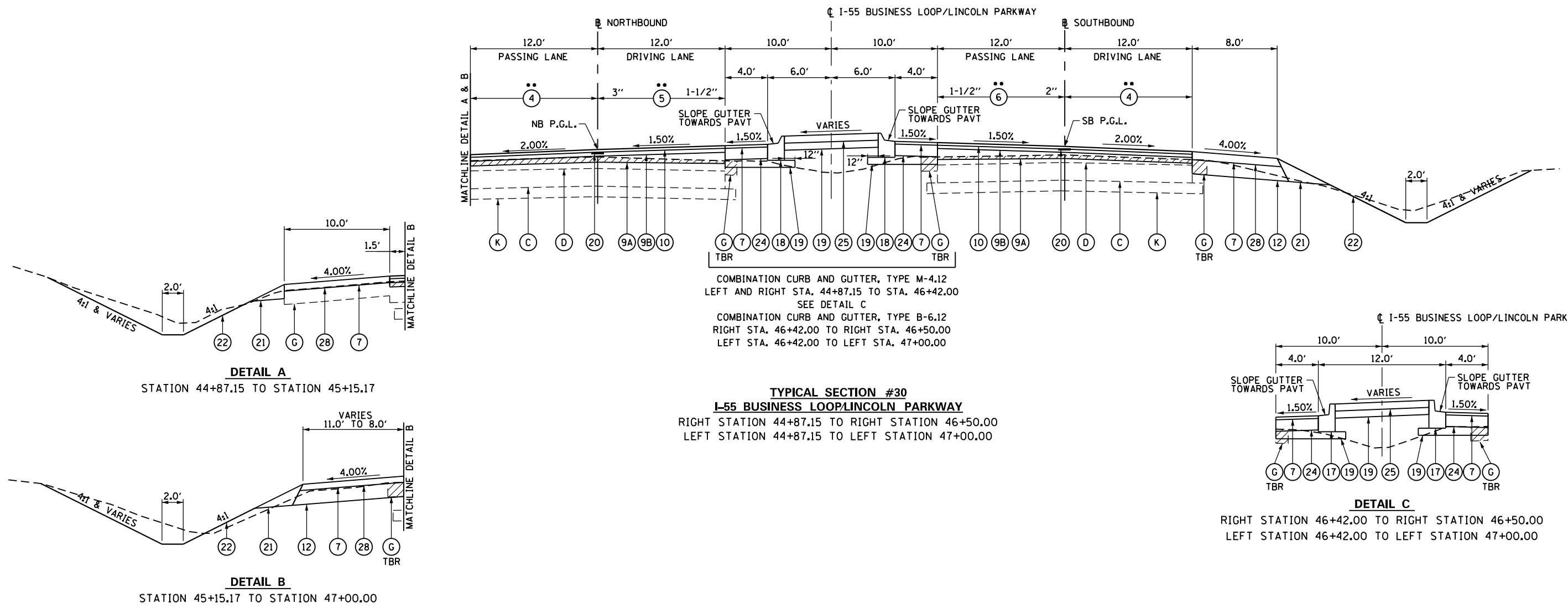
•• FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL



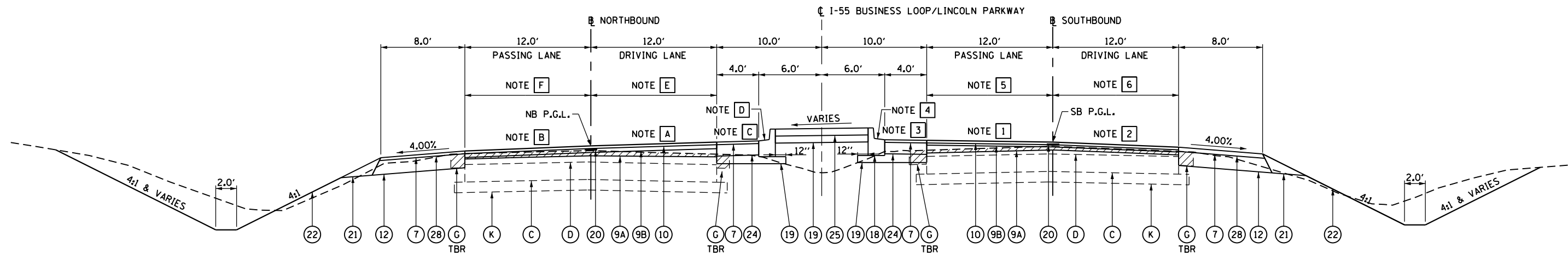
TYPICAL SECTION #29
I-55 BUSINESS LOOP/LINCOLN PARKWAY
STATION 44+50.00 TO STATION 44+87.15

TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)



TYPICAL SECTION LEGEND							
(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"			
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL			
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B			
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2			
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING			
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"			
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH			
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"			
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL			
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)			



TYPICAL SECTION #31
I-55 BUSINESS LOOP/LINCOLN PARKWAY
TRANSITION IN TO RAILROAD CROSSING
 RIGHT STATION 46+50.00 TO RIGHT STATION 47+50.00
 LEFT STATION 47+00.00 TO LEFT STATION 48+00.00

LEFT SIDE NOTES:

- NOTE **A** LEFT PASSING LANE SLOPE - TRANSITION SLOPE STATION 47+00.00 (-1.50%) TO STATION 48+00.00 (MATCH EXISTING)
- NOTE **B** LEFT DRIVING LANE SLOPE - TRANSITION SLOPE STATION 47+00.00 (-2.00%) TO STATION 48+00.00 (MATCH EXISTING)
- NOTE **C** LEFT INSIDE SHOULDER SLOPE - SHALL BE THE SAME AS THE PASSING LANE SLOPE
- NOTE **D** LEFT CC&G TYPE - LEFT STATION 47+00.00 TO LEFT STATION 47+42.00 - TYPE B-6.12
 LEFT STATION 47+42.00 TO LEFT STATION 48+00.00 - TYPE M-4.12
 LEFT GUTTER SLOPE - LEFT STATION 47+00.00 TO LEFT STATION 47+65.00 GUTTER SHALL SLOPE TOWARDS PAVEMENT.
 LEFT STATION 47+65.00 TO LEFT STATION 47+75.00 GUTTER SHALL TRANSITION TOWARDS PAVEMENT TO AWAY FROM PAVEMENT.
 LEFT STATION 47+75.00 TO LEFT STATION 48+00.00 GUTTER SHALL SLOPE AWAY FROM PAVEMENT.
- NOTE **E** LEFT PASSING LANE MILLING - TRANSITION FROM VARIABLE DEPTH SURFACE REMOVAL LEFT STATION 47+00.00 TO 2" SURFACE REMOVAL LEFT STATION 48+00.00
- NOTE **F** LEFT DRIVING LANE MILLING - TRANSITION FROM 2.00% SLOPED SURFACE REMOVAL LEFT STATION 47+00.00 TO 2" SURFACE REMOVAL LEFT STATION 48+00.00

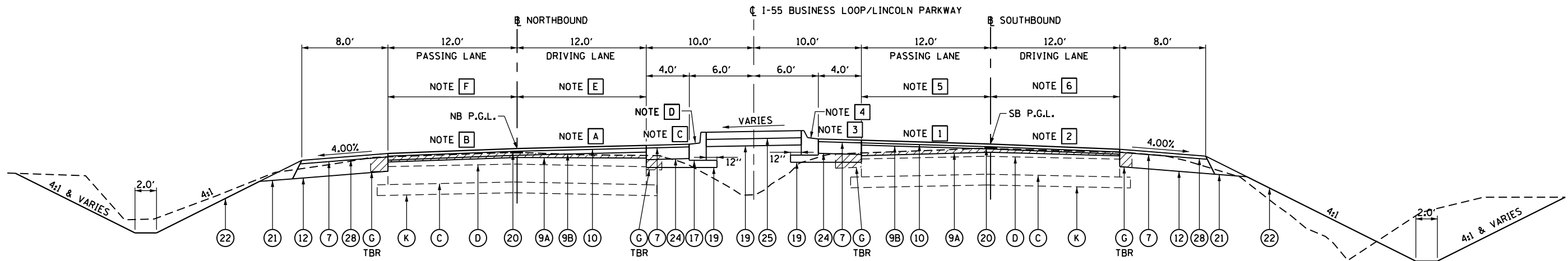
RIGHT SIDE NOTES:

- NOTE **1** RIGHT PASSING LANE SLOPE - TRANSITION SLOPE STATION 46+50.00 (-1.50%) TO STATION 47+50.00 (MATCH EXISTING)
- NOTE **2** RIGHT DRIVING LANE SLOPE - TRANSITION SLOPE STATION 46+50.00 (-2.00%) TO STATION 47+50.00 (MATCH EXISTING)
- NOTE **3** RIGHT INSIDE SHOULDER SLOPE - SHALL BE THE SAME AS THE PASSING LANE SLOPE
- NOTE **4** RIGHT CC&G TYPE - RIGHT STATION 46+50.00 TO RIGHT STATION 47+50.00 - TYPE B-6.12
 RIGHT GUTTER SLOPE - RIGHT STATION 46+50.00 TO RIGHT STATION 46+90.00 GUTTER SHALL SLOPE TOWARDS PAVEMENT.
 RIGHT STATION 46+90.00 TO RIGHT STATION 47+00.00 GUTTER SHALL TRANSITION TOWARDS PAVEMENT TO AWAY FROM PAVEMENT.
 RIGHT STATION 47+00.00 TO RIGHT STATION 47+50.00 GUTTER SHALL SLOPE AWAY FROM PAVEMENT.
- NOTE **5** RIGHT PASSING LANE MILLING - TRANSITION FROM VARIABLE DEPTH SURFACE REMOVAL RIGHT STATION 46+50.00 TO 2" SURFACE REMOVAL RIGHT STATION 47+50.00
- NOTE **6** RIGHT DRIVING LANE MILLING - TRANSITION FROM 2.00% SLOPED SURFACE REMOVAL TO 2" SURFACE REMOVAL FROM RIGHT STATION 46+50.00 TO RIGHT STATION 47+50.00

TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TYPICALS I-55 BUSINESS LOOP/LINCOLN PARKWAY	F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIDOT\Documents\IDOT Offices\District 6\Projects\0672EDR\Drawings\EA0\Sheets\0672E02-sht-04-typ	PLotted	CHECKED -	REVISED -			7706	.	LOGAN	343	36
Typical Sections	PLOT SCALE = 10.0000' / in.	DATE -	REVISED -			SCALE: SHEET OF SHEETS STA. TO STA.		ILLINOIS FED. AID PROJECT		CONTRACT NO. 72E02



TYPICAL SECTION #34
I-55 BUSINESS LOOP/LINCOLN PARKWAY
TRANSITION OUT FROM RAILROAD CROSSING
 RIGHT STATION 48+75.00 TO RIGHT STATION 49+75.00
 LEFT STATION 48+90.00 TO LEFT STATION 49+90.00

LEFT SIDE NOTES:

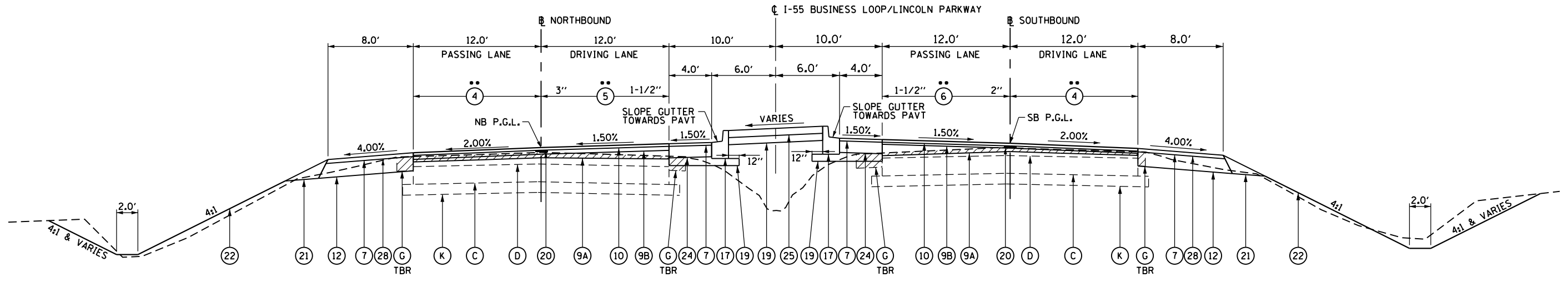
- NOTE [A] LEFT PASSING LANE SLOPE - TRANSITION SLOPE STATION 48+90.00 (MATCH EXISTING) TO STATION 49+90.00 (-1.50%)
- NOTE [B] LEFT DRIVING LANE SLOPE - TRANSITION SLOPE STATION 48+90.00 (MATCH EXISTING) TO STATION 49+90.00 (-2.00%)
- NOTE [C] LEFT INSIDE SHOULDER SLOPE - SHALL BE THE SAME AS THE PASSING LANE SLOPE
- NOTE [D] LEFT CC&G TYPE - LEFT STATION 48+90.00 TO LEFT STATION 49+90.00 - TYPE B-6.12
LEFT GUTTER SLOPE - LEFT STATION 48+90.00 TO LEFT STATION 49+90.00 GUTTER SHALL SLOPE TOWARDS PAVEMENT.
- NOTE [E] LEFT PASSING LANE MILLING - TRANSITION FROM 2" SURFACE REMOVAL LEFT STATION 48+90.00 TO VARIABLE DEPTH SURFACE REMOVAL LEFT STATION 49+90.00
- NOTE [F] LEFT DRIVING LANE MILLING - TRANSITION FROM 2" SURFACE REMOVAL LEFT STATION 48+90.00 TO 2.00% SURFACE REMOVAL LEFT STATION 49+90.00

RIGHT SIDE NOTES:

- NOTE [1] RIGHT PASSING LANE SLOPE - TRANSITION SLOPE STATION 48+75.00 (MATCH EXISTING) TO STATION 49+75.00 (-1.50%)
- NOTE [2] RIGHT DRIVING LANE SLOPE - TRANSITION SLOPE STATION 48+75.00 (MATCH EXISTING) TO STATION 49+75.00 (-2.00%)
- NOTE [3] RIGHT INSIDE SHOULDER SLOPE - SHALL BE THE SAME AS THE PASSING LANE SLOPE
- NOTE [4] RIGHT CC&G TYPE - RIGHT STATION 48+75.00 TO RIGHT STATION 49+17.00 - TYPE M-4.12
RIGHT STATION 49+17.00 TO RIGHT STATION 49+75.00 - TYPE B-6.12
RIGHT GUTTER SLOPE - RIGHT STATION 48+75.00 TO RIGHT STATION 46+15.00 GUTTER SHALL SLOPE AWAY FROM PAVEMENT.
RIGHT STATION 49+15.00 TO RIGHT STATION 49+25.00 GUTTER SHALL TRANSITION AWAY FROM PAVEMENT TOWARDS PAVEMENT.
RIGHT STATION 49+25.00 TO RIGHT STATION 49+75.00 GUTTER SHALL SLOPE TOWARDS PAVEMENT.
- NOTE [5] RIGHT PASSING LANE MILLING - TRANSITION FROM 2" SURFACE REMOVAL RIGHT STATION 48+75.00 TO VARIABLE DEPTH SURFACE REMOVAL RIGHT STATION 49+75.00
- NOTE [6] RIGHT DRIVING LANE MILLING - TRANSITION FROM 2" SURFACE REMOVAL TO 2.00% VARIABLE DEPTH FROM RIGHT STATION 48+75.00 TO RIGHT STATION 49+75.00

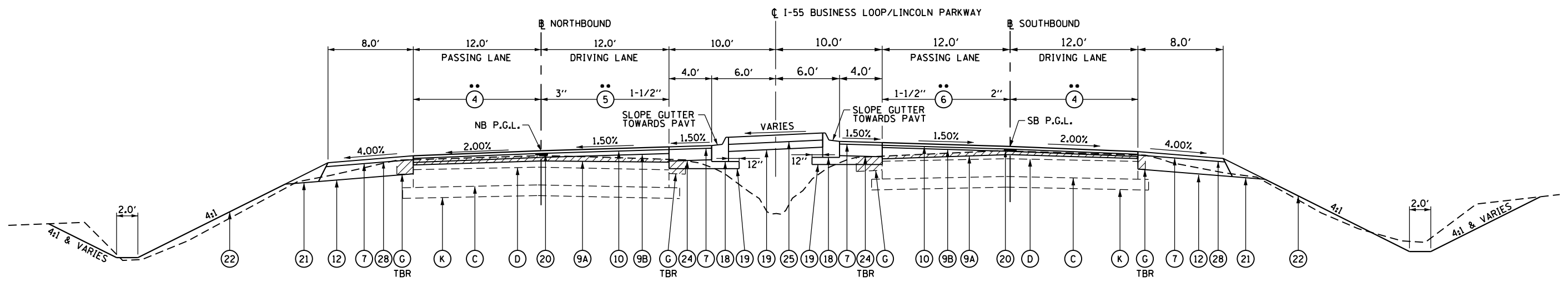
TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)



TYPICAL SECTION #35
I-55 BUSINESS LOOP/LINCOLN PARKWAY
 RIGHT STATION 49+75.00 TO RIGHT STATION 50+17.00
 LEFT STATION 49+90.00 TO LEFT STATION 50+17.00

•• FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL

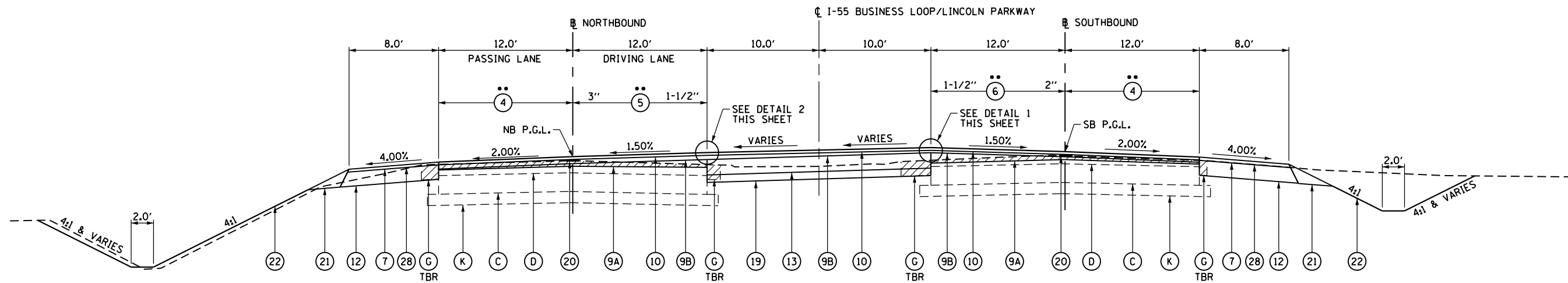


TYPICAL SECTION #36
I-55 BUSINESS LOOP/LINCOLN PARKWAY
 STATION 51+17.00 TO STATION 51+80.00

•• FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL

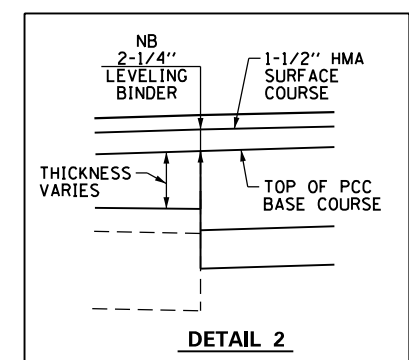
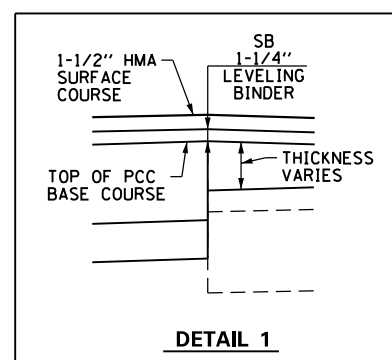
TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)



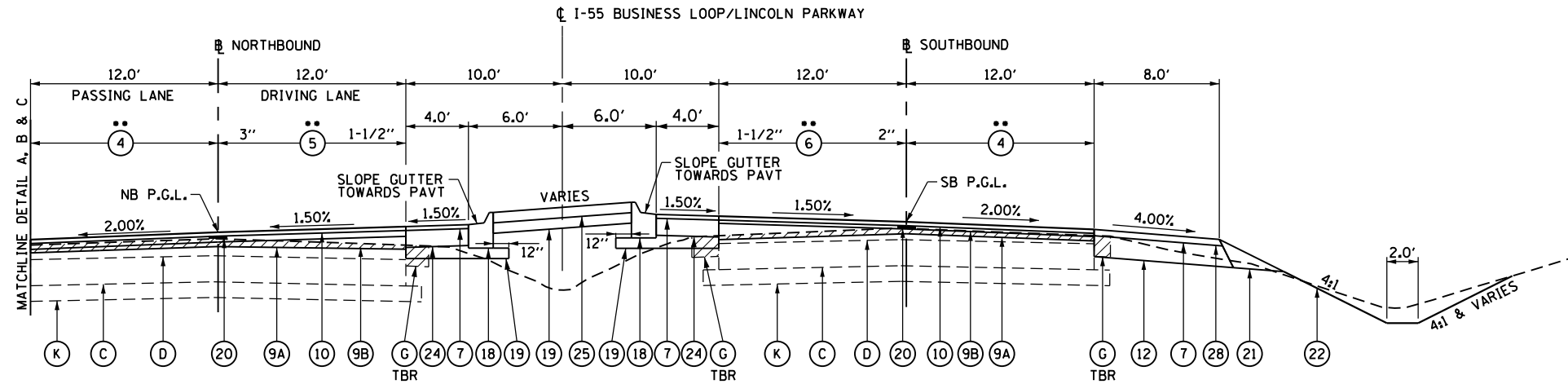
•• FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL

TYPICAL SECTION #37
I-55 BUSINESS LOOP/LINCOLN PARKWAY
 STATION 51+80.00 TO STATION 52+40.00

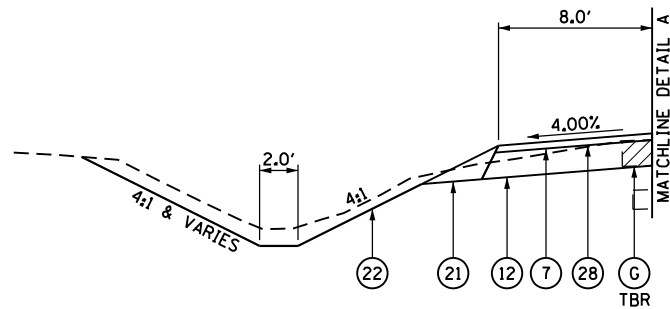


TYPICAL SECTION LEGEND

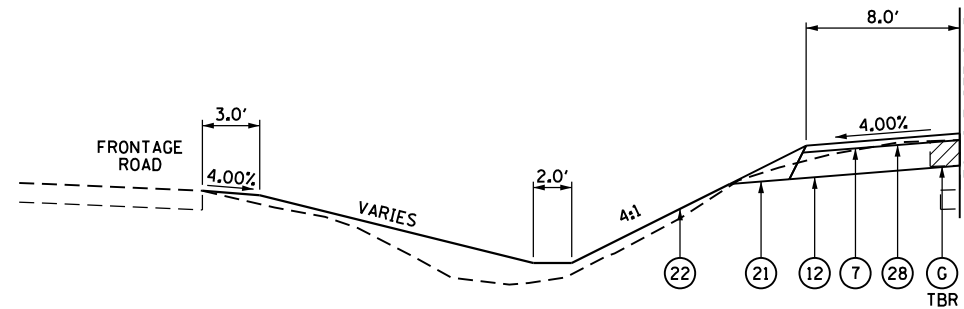
(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)



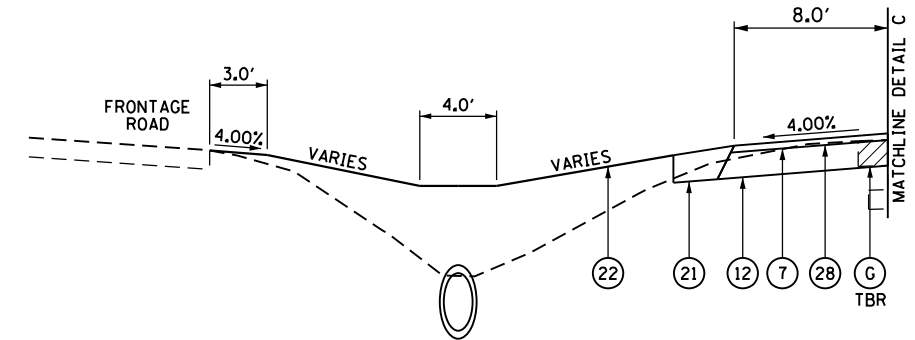
TYPICAL SECTION #38
I-55 BUSINESS LOOP/LINCOLN PARKWAY
 STATION 52+40.00 TO STATION 63+72.32



DETAIL A
 STATION 52+40.00 TO STATION 54+25.00
 STATION 55+75.00 TO STATION 61+50.00



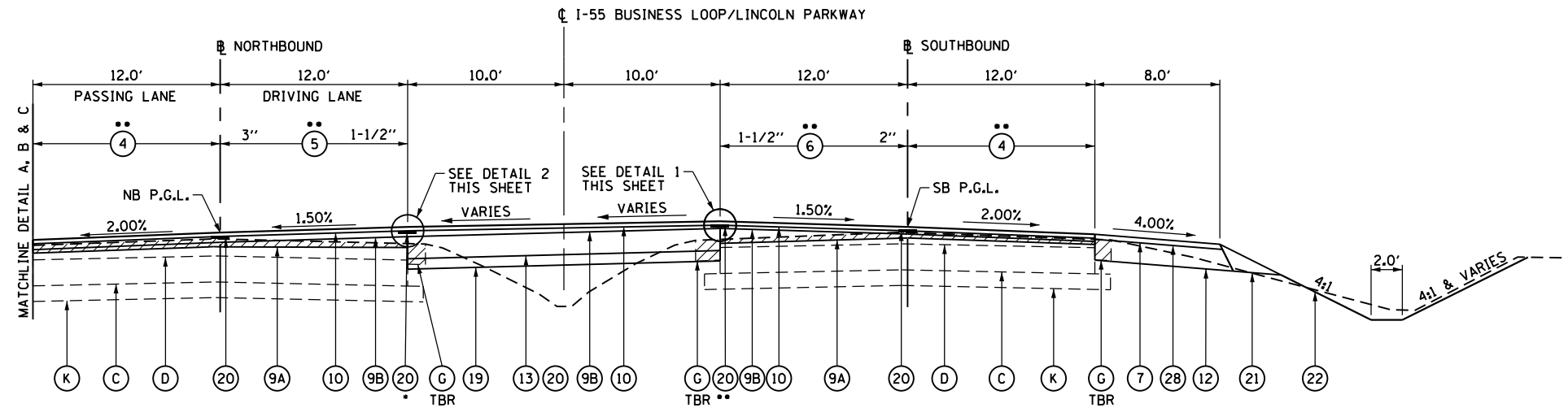
DETAIL B
 STATION 54+25.00 TO STATION 54+61.28
 STATION 55+00.00 TO STATION 55+75.00
 STATION 63+00.00 TO STATION 63+72.32



DETAIL C
 STATION 54+61.28 TO STATION 55+00.00
 STATION 61+50.00 TO STATION 63+00.00

TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

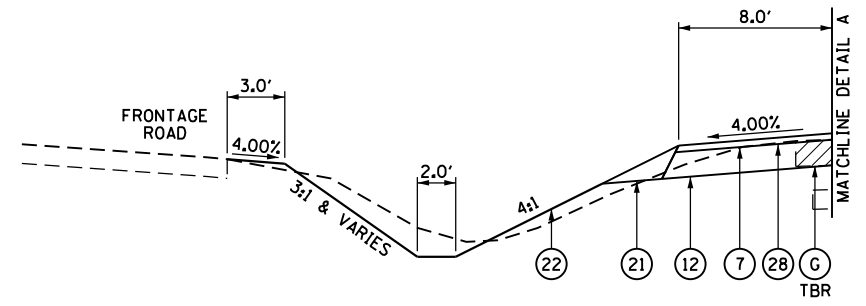
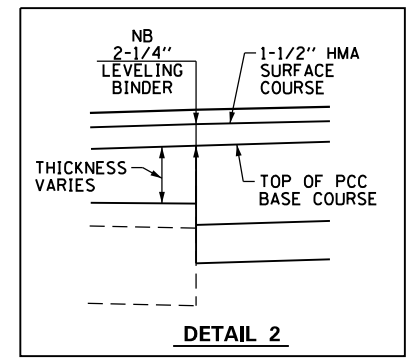
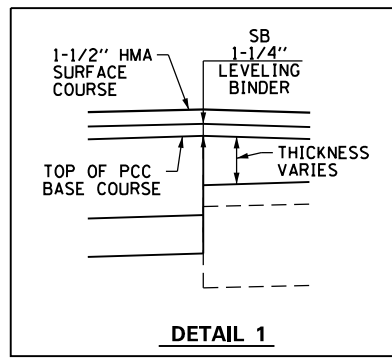


• BEGINS STATION 68+15.09
ENDS STATION 69+16.16

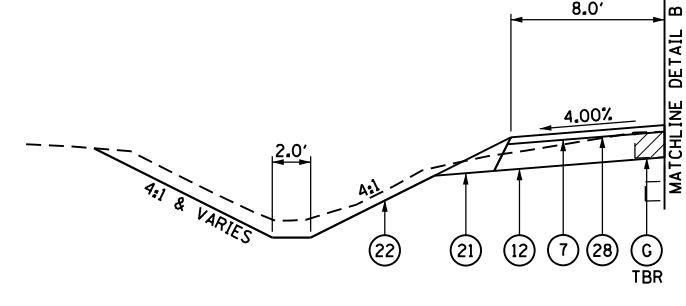
•• BEGINS STATION 63+72.32

•• FOR DETAILS OF HMA SURFACE REMOVAL SEE REMOVAL PLANS AND HMA SURFACE REMOVAL, VARIABLE DEPTH DETAIL

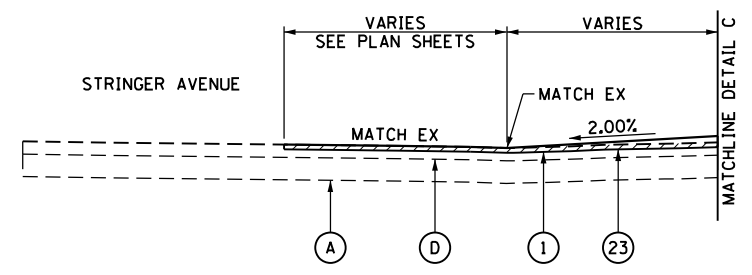
TYPICAL SECTION #39
I-55 BUSINESS LOOP/LINCOLN PARKWAY
STATION 63+72.32 TO STATION 71+00.00



DETAIL A
STATION 63+72.32 TO STATION 66+25.00



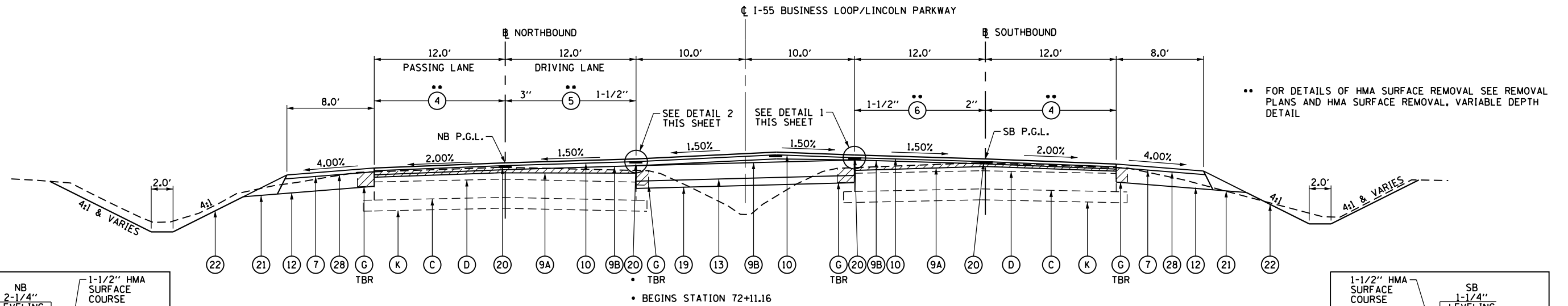
DETAIL B
STATION 66+25.00 TO STATION 67+62.55
STATION 70+22.26 TO STATION 71+00.00



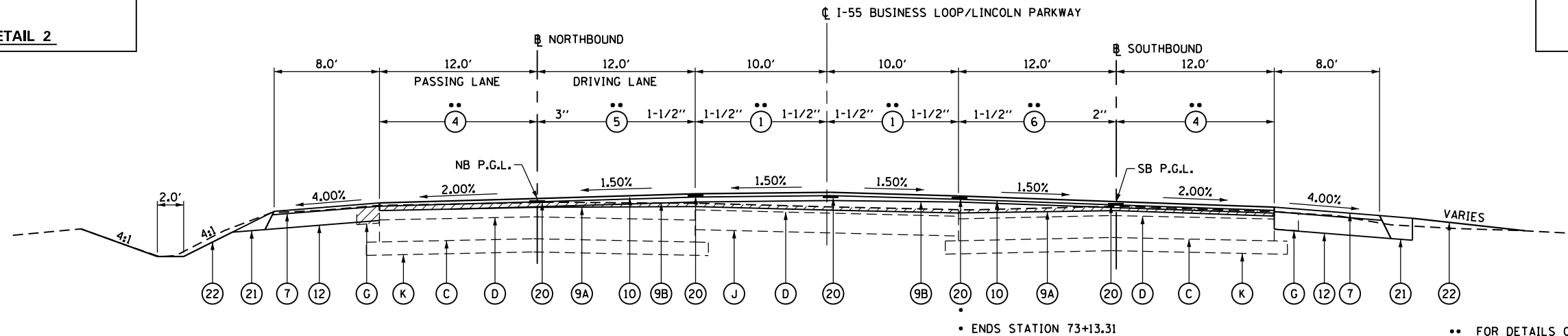
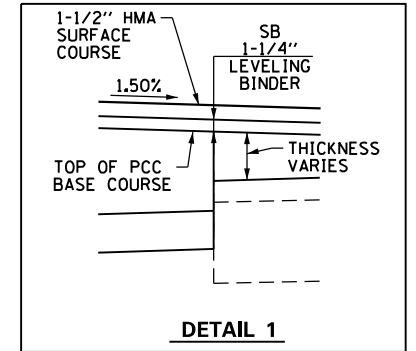
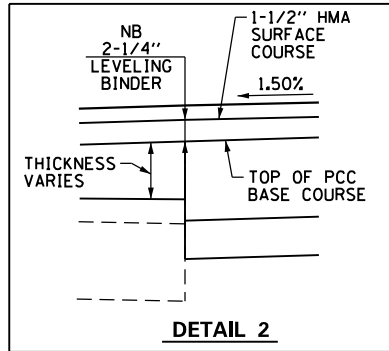
DETAIL C
STATION 67+62.55 TO STATION 70+22.26
STRINGER AVENUE

TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)



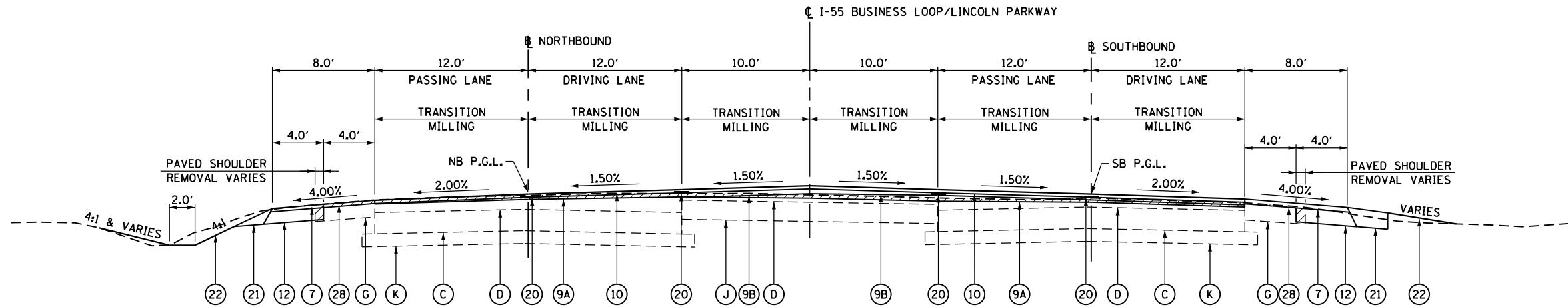
TYPICAL SECTION #40
I-55 BUSINESS LOOP/LINCOLN PARKWAY
 STATION 71+00.00 TO STATION 72+56.51



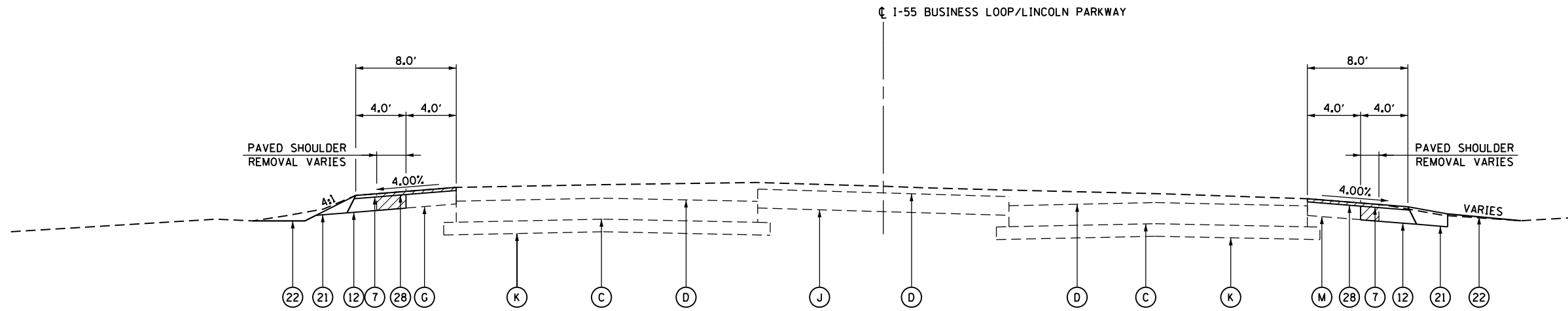
TYPICAL SECTION #41
I-55 BUSINESS LOOP/LINCOLN PARKWAY
 STATION 72+56.51 TO STATION 73+50.00

TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)



TYPICAL SECTION #42
I-55 BUSINESS LOOP/LINCOLN PARKWAY
BUTT JOINT
 STATION 73+50.00 TO STATION 74+00.00

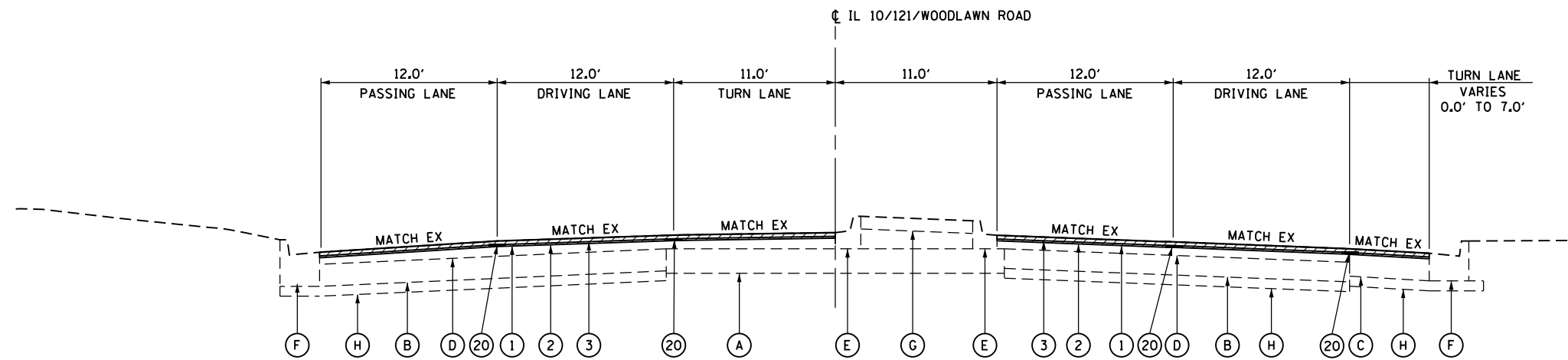


TYPICAL SECTION #43
I-55 BUSINESS LOOP/LINCOLN PARKWAY
 RIGHT STATION 74+00.00 TO RIGHT STATION 76+12.80
 LEFT STATION 74+00.00 TO LEFT STATION 76+90.00

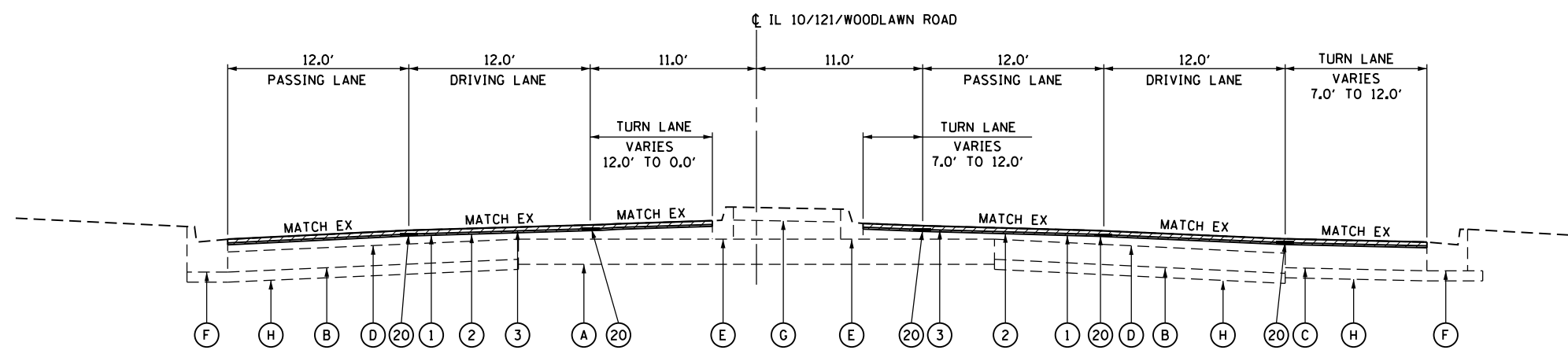
TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 9"-7"-9"	(I) EXISTING MEDIAN SURFACE 4"	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2")	(10) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(19) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE WIDENING 9"	(J) EXISTING HMA BASE COURSE 9"	(2) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(11) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (2")	(20) LONGITUDINAL JOINT SEAL
(C) EXISTING PCC PAVEMENT 10"	(K) EXISTING GRANULAR BORROW 6"	(3) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2-1/4")	(12) PROPOSED HMA BASE COURSE, 8"	(21) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
(D) EXISTING HMA OVERLAY	(L) EXISTING HMA PAVEMENT 12-1/2"	(4) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2.00% SLOPE)	(13) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 8-1/2"	(22) PROPOSED SEEDING, CLASS 2
(E) EXISTING CC&G TYPE M-6.06	(M) EXISTING HMA PAVEMENT 8"	(5) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (3" TO 1-1/2")	(14) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	(23) PROPOSED INCIDENTAL HMA SURFACING
(F) EXISTING CC&G TYPE B-6.24	(N) EXISTING STABILIZED SOIL 12"	(6) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2" TO 1-1/2")	(15) PROPOSED CONCRETE MEDIAN, TYPE SM-6.06	(24) PROPOSED HMA SHOULDER, 6-1/2"
(G) EXISTING HMA SHOULDER 8"	TBR - TO BE REMOVED	(7) PROPOSED HMA SHOULDERS	(16) PROPOSED CONCRETE MEDIAN, TYPE SM-4.12	(25) PROPOSED CONCRETE MEDIAN SURFACE, 4 INCH
(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"		(8) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(17) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.12	(26) PROPOSED HMA BASE COURSE, 8 1/2"
		(9A) PROPOSED LEVELING BINDER (MACHINE METHOD), SPECIAL (VARIES)	(18) PROPOSED COMBINATION CURB AND GUTTER, TYPE M-4.12	(27) PROPOSED PAVEMENT REMOVAL
		(9B) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (2-1/4" & 1-1/4")		(28) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (4.00% SLOPE)

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS I-55 BUSINESS LOOP/LINCOLN PARKWAY			F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11084EBID\INTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 6\Projects\0672\Drawings\EA0\Drawings\0672E02-sht-04-typ		CHECKED -	REVISED -		7706		LOGAN	343	44			
Typical Sections	PLOT SCALE = 10.0000" / in.	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 72E02				



TYPICAL SECTION #44
IL 10/121/WOODLAWN ROAD
 STATION 133+62.35 TO STATION 134+55.57

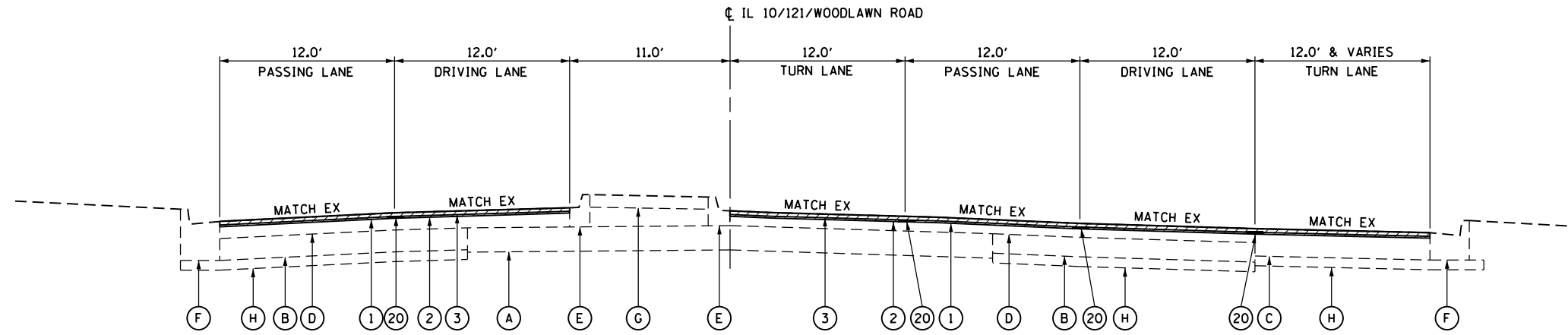


TYPICAL SECTION #45
IL 10/121/WOODLAWN ROAD
 STATION 134+55.57 TO STATION 135+50.50

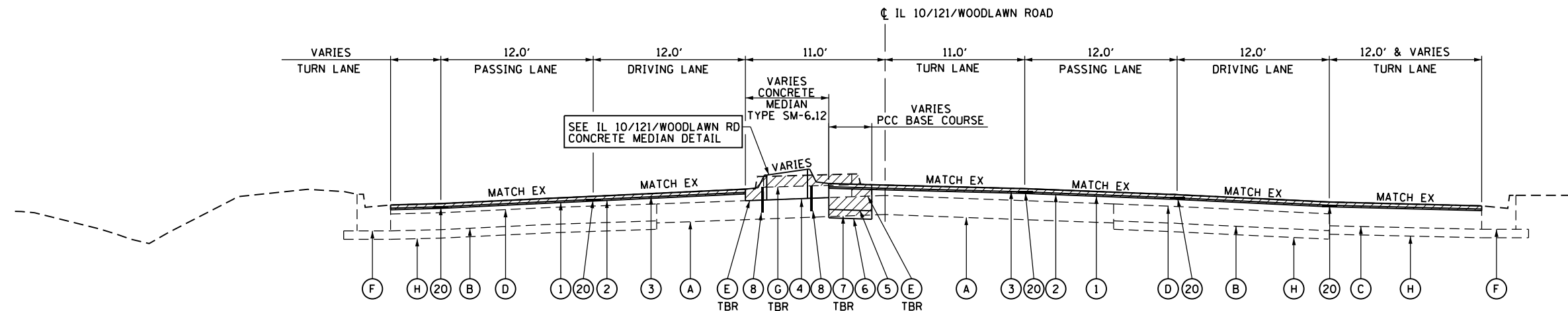
TYPICAL SECTION LEGEND

(A) EXISTING PCC PAVEMENT 10"	(F) EXISTING CC&G TYPE B-6.24	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(6) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE 8"	(G) EXISTING MEDIAN SURFACE 4"	(2) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(7) PROPOSED PAVEMENT REMOVAL
(C) EXISTING PCC PAVEMENT 9"	(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"	(3) PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(8) PROPOSED DOWELS
(D) EXISTING HMA OVERLAY	(I) EXISTING CONCRETE MEDIAN, TYPE SM-6.12	(4) PROPOSED CONCRETE MEDIAN, TYPE SM-6.12	
(E) EXISTING CC&G TYPE M-6.12	TBR - TO BE REMOVED	(5) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	

• (22)RS-6, MED & (23) RS-5



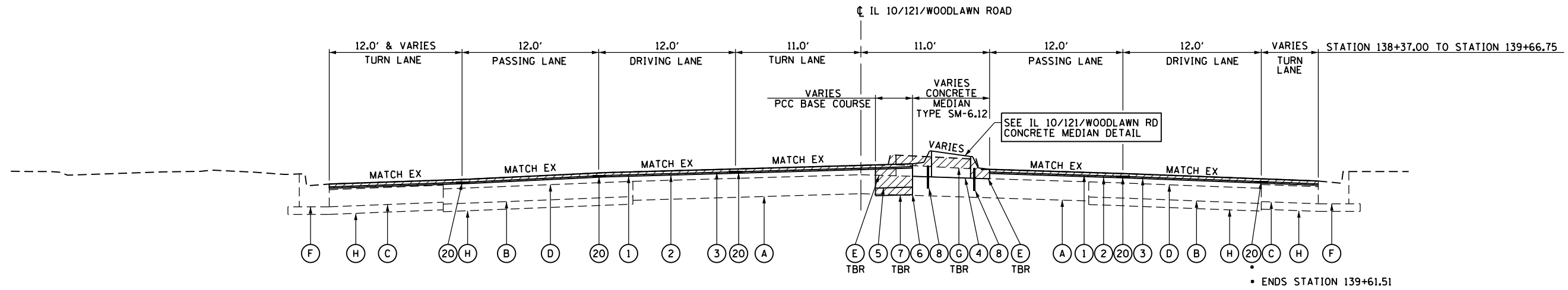
TYPICAL SECTION #46
IL 10/121/WOODLAWN ROAD
 STATION 135+50.50 TO STATION 135+71.11



TYPICAL SECTION #47
IL 10/121/WOODLAWN ROAD
 STATION 135+71.11 TO STATION 137+37.49

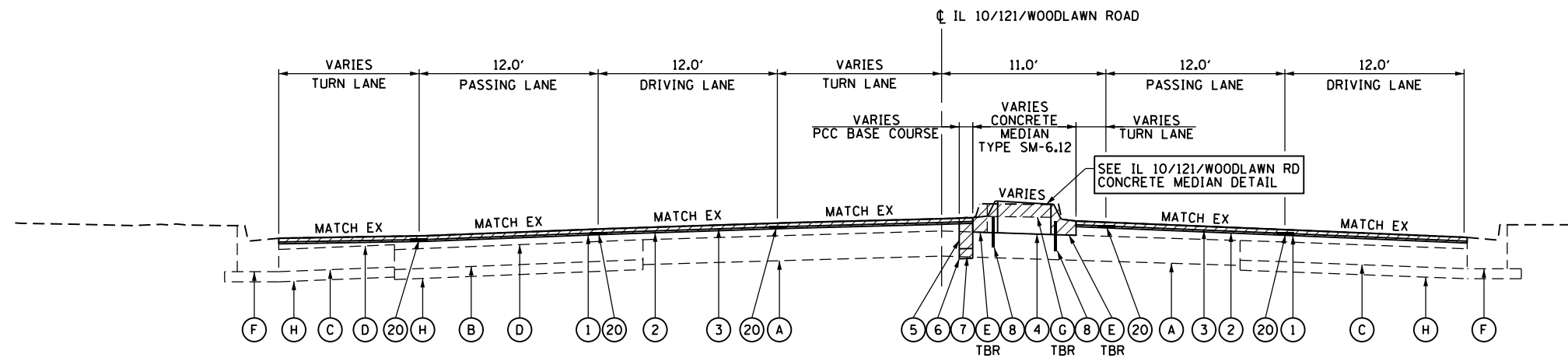
NOTES:
 THE ENGINEER SHALL DETERMINE WHETHER THE EXISTING PCC PAVEMENT SHALL BE REMOVED FOR THE PLACEMENT OF THE BASE COURSE IN THE CENTER MEDIAN.
 THE COST OF THE DOWELS SHALL BE INCLUDED IN CONCRETE MEDIAN, TYPE SM-6.12

TYPICAL SECTION LEGEND			
(A) EXISTING PCC PAVEMENT 10"	(F) EXISTING CC&G TYPE B-6.24	(1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2")	(6) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
(B) EXISTING PCC BASE COURSE 8"	(G) EXISTING MEDIAN SURFACE 4"	(2) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")	(7) PROPOSED PAVEMENT REMOVAL
(C) EXISTING PCC PAVEMENT 9"	(H) EXISTING SUB-BASE GRANULAR MATERIAL 4"	(3) PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "D", N50 (1-1/2")	(8) PROPOSED DOWELS
(D) EXISTING HMA OVERLAY	(I) EXISTING CONCRETE MEDIAN, TYPE SM-6.12	(4) PROPOSED CONCRETE MEDIAN, TYPE SM-6.12	
(E) EXISTING CC&G TYPE M-6.12	TBR - TO BE REMOVED	(5) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10"	



TYPICAL SECTION #48
IL 10/121/WOODLAWN ROAD
 STATION 138+37.00 TO STATION 140+86.50

NOTES:
 THE ENGINEER SHALL DETERMINE WHETHER THE EXISTING PCC PAVEMENT SHALL BE REMOVED FOR THE PLACEMENT OF THE BASE COURSE IN THE CENTER MEDIAN.
 THE COST OF THE DOWELS SHALL BE INCLUDED IN CONCRETE MEDIAN, TYPE SM-6.12

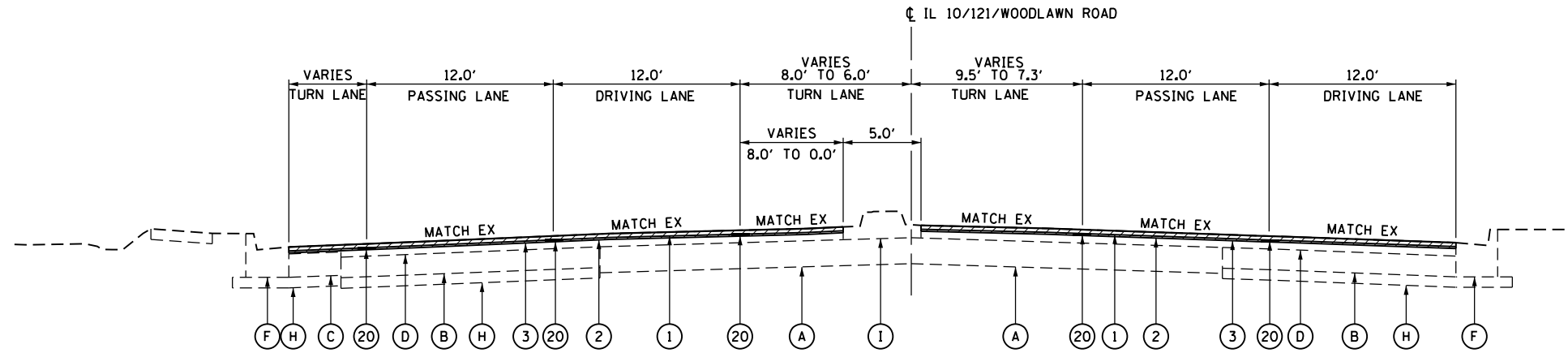


TYPICAL SECTION #49
IL 10/121/WOODLAWN ROAD
 STATION 140+86.50 TO STATION 141+41.71

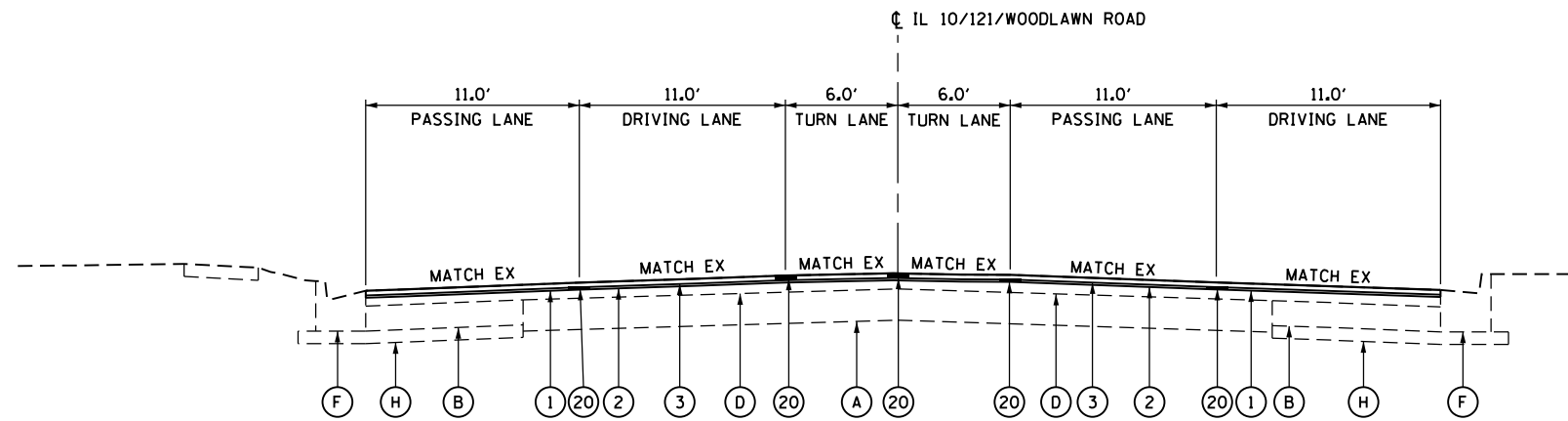
TYPICAL SECTION LEGEND

- | | | | |
|---------------------------------|--|--|---|
| (A) EXISTING PCC PAVEMENT 10" | (F) EXISTING CC&G TYPE B-6.24 | (1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2") | (6) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4" |
| (B) EXISTING PCC BASE COURSE 8" | (G) EXISTING MEDIAN SURFACE 4" | (2) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4") | (7) PROPOSED PAVEMENT REMOVAL |
| (C) EXISTING PCC PAVEMENT 9" | (H) EXISTING SUB-BASE GRANULAR MATERIAL 4" | (3) PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "D", N50 (1-1/2") | (8) PROPOSED DOWELS |
| (D) EXISTING HMA OVERLAY | (I) EXISTING CONCRETE MEDIAN, TYPE SM-6.12 | (4) PROPOSED CONCRETE MEDIAN, TYPE SM-6.12 | |
| (E) EXISTING CC&G TYPE M-6.12 | TBR - TO BE REMOVED | (5) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10" | |

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS IL 10/121/WOODLAWN ROAD			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 6\Projects\0672\Drawings\EA\Sheets\0672E02-sht-04-typ		DRAWN -	REVISED -		7706	.	LOGAN	343	47			
Typical Sections	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 72E02				
	PLOT DATE = 10/21/2016	DATE -	REVISED -				ILLINOIS FED. AID PROJECT					



TYPICAL SECTION #50
IL 10/121/WOODLAWN ROAD
 STATION 141+41.71 TO STATION 142+19.73



• ENDS STATION 142+49.59

TYPICAL SECTION #51
IL 10/121/WOODLAWN ROAD
 STATION 142+19.73 TO STATION 143+91.32

TYPICAL SECTION LEGEND

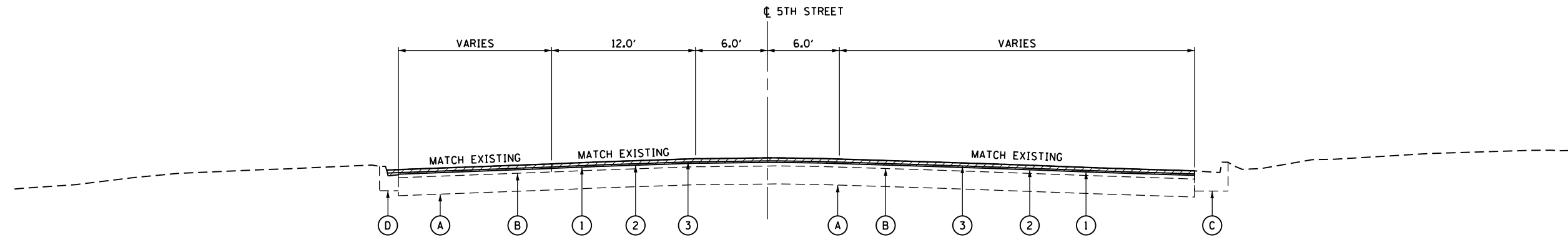
- | | | | |
|---------------------------------|--|--|---|
| (A) EXISTING PCC PAVEMENT 10" | (F) EXISTING CC&G TYPE B-6.24 | (1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2") | (6) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4" |
| (B) EXISTING PCC BASE COURSE 8" | (G) EXISTING MEDIAN SURFACE 4" | (2) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4") | (7) PROPOSED PAVEMENT REMOVAL |
| (C) EXISTING PCC PAVEMENT 9" | (H) EXISTING SUB-BASE GRANULAR MATERIAL 4" | (3) PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "D", N50 (1-1/2") | (8) PROPOSED DOWELS |
| (D) EXISTING HMA OVERLAY | (I) EXISTING CONCRETE MEDIAN, TYPE SM-6.12 | (4) PROPOSED CONCRETE MEDIAN, TYPE SM-6.12 | |
| (E) EXISTING CC&G TYPE M-6.12 | TBR - TO BE REMOVED | (5) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10" | |

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
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Typical Sections	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/21/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS			
IL 10/121/WOODLAWN ROAD			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	•	LOGAN	343	48
			CONTRACT NO. 72E02	
ILLINOIS FED. AID PROJECT				

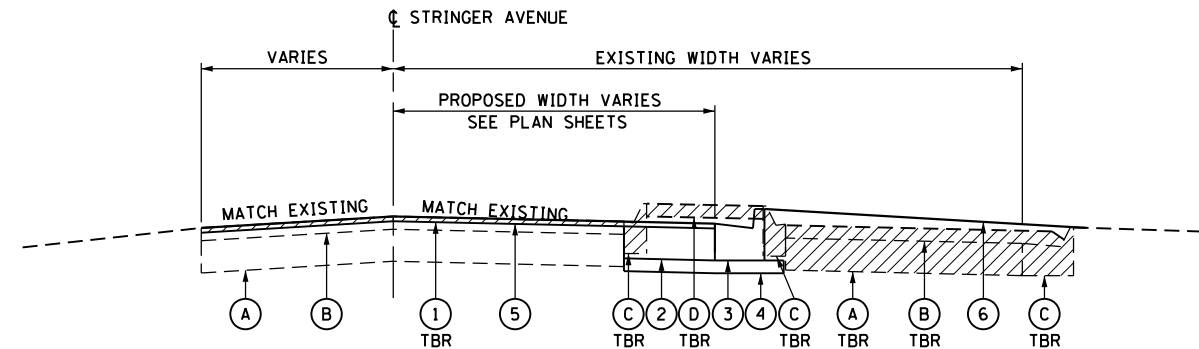


TYPICAL SECTION #52
5TH STREET

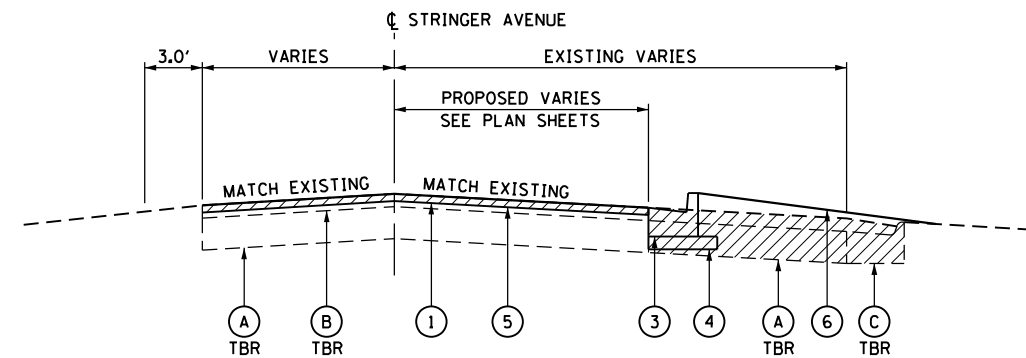
TYPICAL SECTION LEGEND

- | | |
|-------------------------------|---|
| (A) EXISTING PCC PAVEMENT 10" | (1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (2") |
| (B) EXISTING HMA OVERLAY | (2) PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4") |
| (C) EXISTING CC&G TYPE B-6.24 | (3) PROPOSED HMA SURFACE COURSE, MIX "D", N50 (1-1/2") |
| (D) EXISTING CC&G TYPE B-6.12 | (4) PROPOSED SEEDING, CLASS 2 |
| TBR - TO BE REMOVED | |

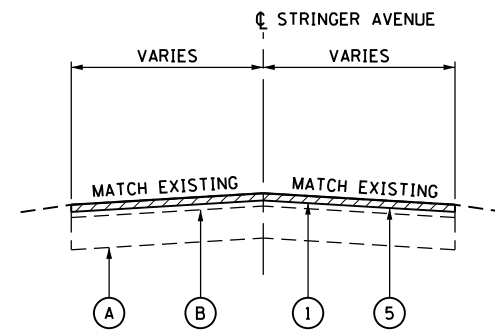
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Typical Sections	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 72E02		ILLINOIS FED. AID PROJECT		
	PLOT DATE = 10/21/2016	DATE -	REVISED -		• (22)RS-6, MED & (23) RS-5							



TYPICAL SECTION #53
STRINGER AVENUE



TYPICAL SECTION #54
STRINGER AVENUE



TYPICAL SECTION #55
STRINGER AVENUE

TYPICAL SECTION LEGEND

- | | |
|--------------------------------|---|
| (A) EXISTING PCC PAVEMENT 10" | (1) PROPOSED HMA SURFACE REMOVAL, VARIABLE DEPTH (1-1/2") |
| (B) EXISTING HMA OVERLAY | (2) PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 10" |
| (C) EXISTING CC&G TYPE M-6.06 | (3) PROPOSED COMBINATION CURB AND GUTTER, TYPE B-6.24 |
| (D) EXISTING MEDIAN SURFACE 4" | (4) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4" |
| TBR - TO BE REMOVED | (5) PROPOSED INCIDENTAL HMA SURFACING |
| | (6) PROPOSED SEEDING, CLASS 2 |

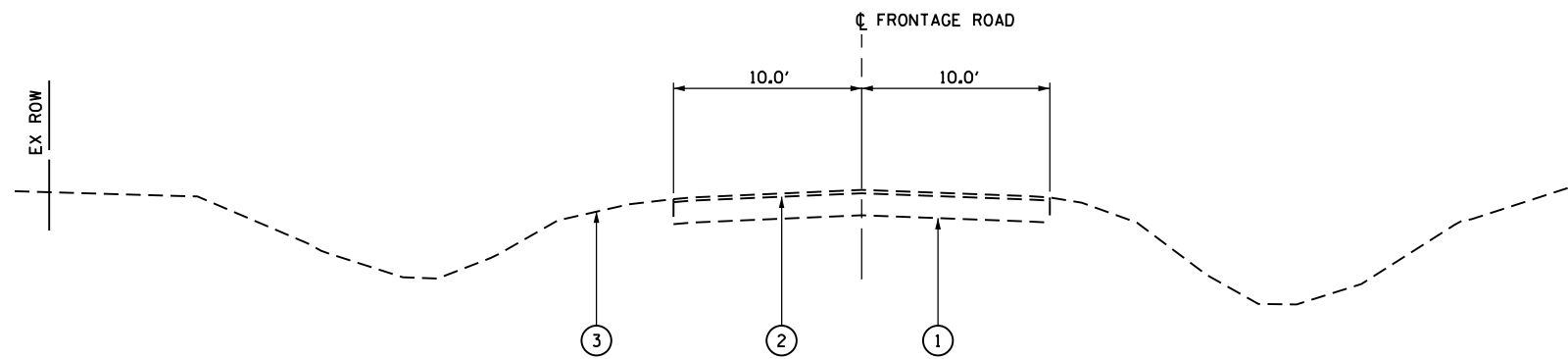
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Typical Sections	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/21/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
STRINGER AVENUE

SCALE: SHEET OF SHEETS STA. TO STA.

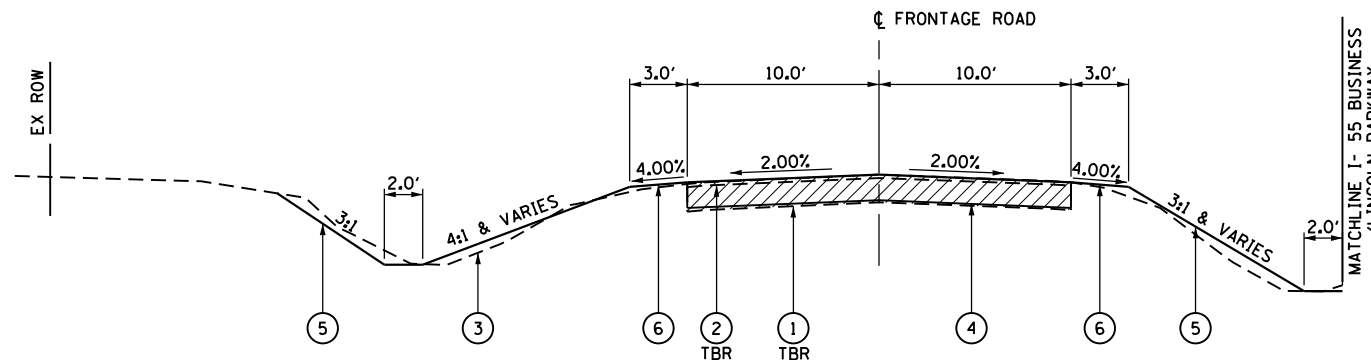
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	*	LOGAN	343	50
CONTRACT NO. 72E02			ILLINOIS FED. AID PROJECT	



TYPICAL SECTION #56
EXISTING FRONTAGE ROAD

- TYPICAL SECTION LEGEND**
- ① EXISTING GRAVEL BASE COURSE, 7 INCH
 - ② EXISTING BITUMINOUS SURFACE TREATMENT
 - ③ EXISTING GROUND
 - ④ PAVEMENT PATCHING, TYPE III, 8 INCH OR PAVEMENT PATCHING, TYPE IV, 8 INCH
 - ⑤ SEEDING, CLASS 2
 - ⑥ EARTH SHOULDER

TBR - TO BE REMOVED



TYPICAL SECTION #57
PATCHING FRONTAGE ROAD

NOTES:

1. REMOVAL OF EXISTING FRONTAGE ROAD PAVEMENT SHALL BE INCLUDED IN THE COST OF PAVEMENT PATCHING.

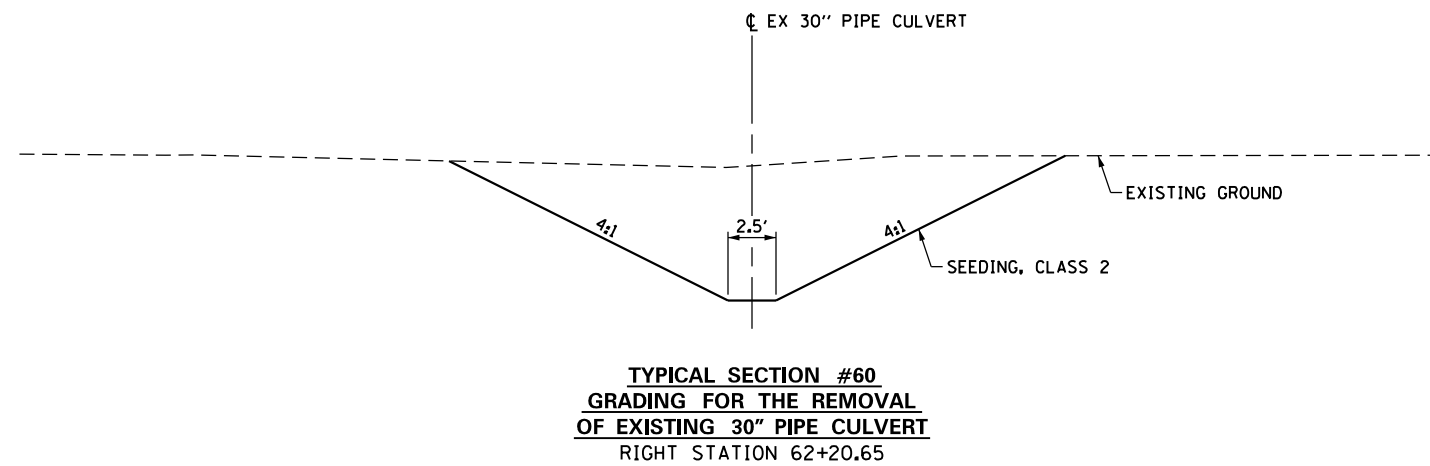
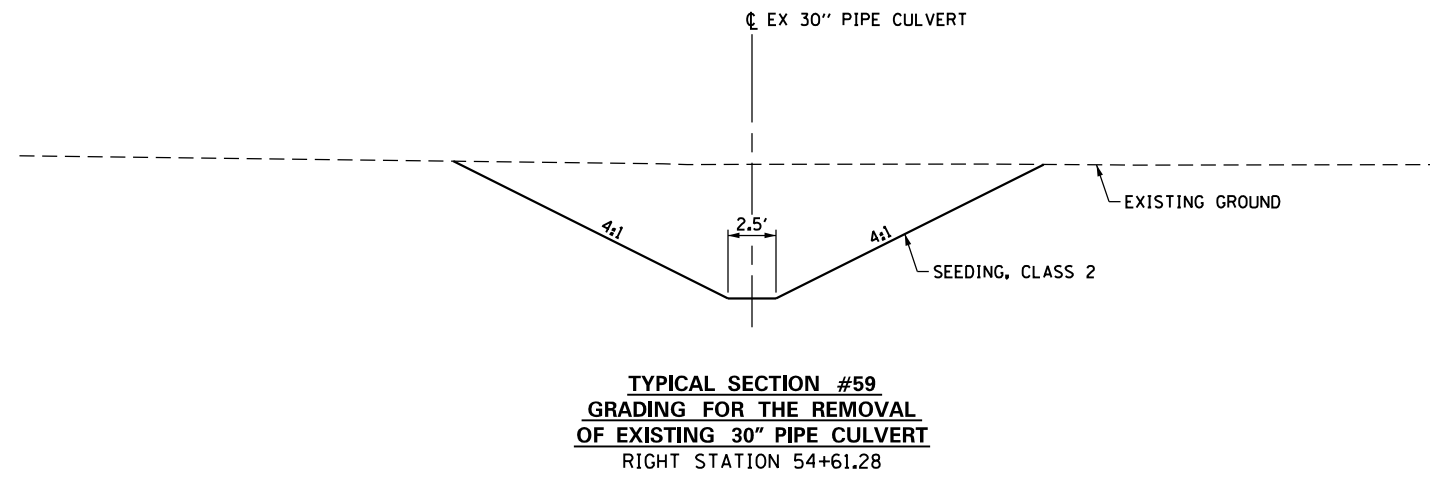
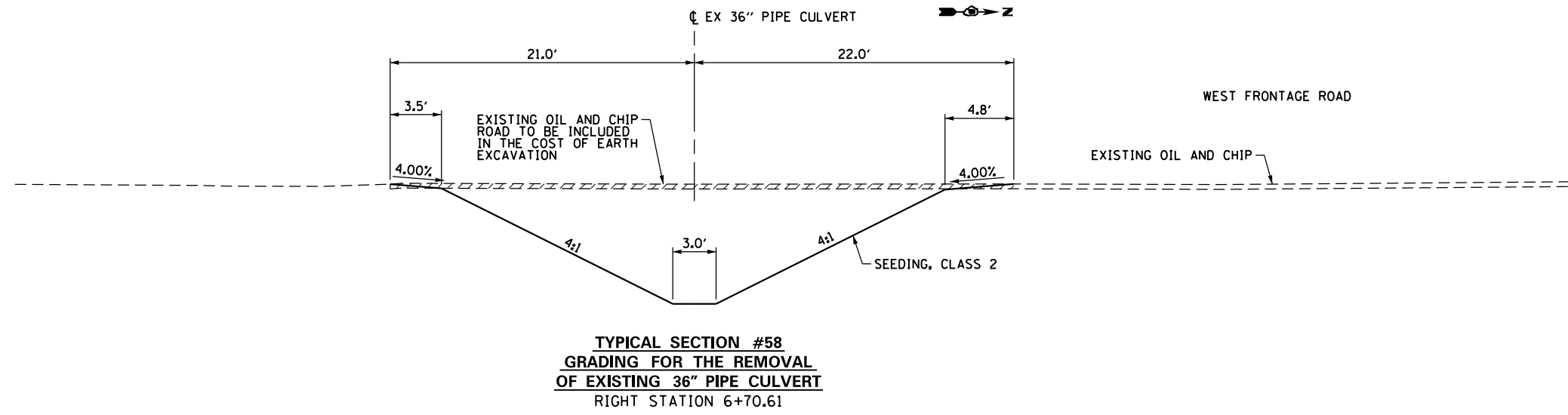
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Typical Sections	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/21/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
FRONTAGE ROAD

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	*	LOGAN	343	51
			CONTRACT NO. 72E02	
ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS CULVERT REMOVAL GRADING			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 6\Projects\0672EDR\Drawings\EA0\Sheets\0672E02-sht-04-typ					SCALE: SHEET OF SHEETS STA. TO STA.			7706	•	LOGAN	343	52
Typical Sections								CONTRACT NO. 72E02				
PLOT SCALE = 10.0000' / in.								ILLINOIS FED. AID PROJECT				
PLOT DATE = 10/21/2016							• (22)RS-6, MED & (23) RS-5					

20100210 - TREE REMOVAL (OVER 15 UNITS DIAMETER)			
PPS# 6-00625-0000 (STP)			
STATION	OFFSET	SIDE	UNIT
16+92.66	58.9	LT	17
29+81.80	52.5	RT	34
31+08.26	106.9	LT	28
31+28.53	106.7	LT	23
31+61.43	107.3	LT	30
54+76.36	55.0	RT	35
62+27.93	97.0	RT	36
PPS# 6-00625-0000 (STP) TOTAL =			203

28000500 - INLET AND PIPE PROTECTION SCHEDULE				
PPS# 6-00625-0000 (STP)				
DESCRIPTION	STATION	OFFSET	SIDE	EACH
PROPOSED DUAL 18" PIPE	5+22.26	57.9	LT	1.0
EXISTING 18" PIPE	5+59.34	59.2	RT	1.0
EXISTING 36" PIPE	6+70.52	65.8	RT	1.0
PROPOSED TYPE 8 GRATE	6+71.18	53.2	LT	1.0
PROPOSED 2' X 2' BOX CULVERT	6+71.52	111.9	LT	1.0
PROPOSED 18" PIPE	7+26.91	54.1	LT	1.0
PROPOSED 2' X 2' BOX CULVERT	16+80.42	112.0	LT	1.0
PROPOSED TYPE 8 GRATE	16+80.64	44.1	LT	1.0
PROPOSED 18" PIPE	17+26.50	54.2	LT	1.0
PROPOSED TYPE 8 GRATE	31+71.16	54.2	LT	1.0
PROPOSED 2' X 2' BOX CULVERT	31+71.26	108.5	LT	1.0
PROPOSED INLET WITH TYPE 8 GRATE	33+00.01	55.7	LT	1.0
PROPOSED 18" PIPE	34+00.07	56.8	LT	1.0
PROPOSED 24" PIPE	47+74.85	57.5	RT	1.0
EXISTING 24" PIPE	47+76.44	54.4	RT	1.0
EXISTING 24" PIPE	49+11.19	154.0	LT	1.0
PROPOSED 2' X 2' BOX CULVERT	49+77.20	62.3	RT	1.0
EXISTING 30" PIPE	51+55.88	115.5	LT	1.0
PROPOSED TYPE 8 GRATE	54+60.74	57.3	RT	1.0
EXISTING 30" PIPE	54+61.30	59.8	RT	1.0
PROPOSED 18" PIPE	55+51.75	55.9	LT	1.0
PROPOSED 18" PIPE	61+24.45	54.1	LT	1.0
PROPOSED TYPE 8 GRATE	62+20.65	57.3	LT	1.0
EXISTING 2' X 2' BOX CULVERT	62+20.65	57.3	RT	1.0
PROPOSED 18" PIPE	63+01.35	54.5	LT	1.0
EXISTING 30" PIPE	66+11.42	57.9	LT	1.0
EXISTING 24" PIPE	66+97.93	47.3	RT	1.0
PROPOSED 15" PIPE	69+14.32	56.7	RT	1.0
EXISTING INLET	70+12.49	49.5	LT	1.0
EXISTING 12" PIPE	73+35.12	48.1	LT	1.0
PROPOSED TYPE 8 GRATE (IL 10)	135+93.83	58.5	LT	1.0
PROPOSED 15" PIPE CULVERT (IL 10)	136+01.836	58.3	LT	1.0
EXISTING 24" PIPE (IL 10)	139+01.82	68.0	LT	1.0
PROPOSED 18" PIPE (STRINGER AVE)	20+80.40	25.4	LT	1.0
PPS# 6-00625-0000 (STP) TOTAL =				34

EARTHWORK SCHEDULE - STAGE 1								
PPS# 6-00625-0000 (STP)								
ROADWAY	SIDE	STATION	TO	STATION	20200100 EARTH EXCAVATION CU YD	FOR INFORMATION ONLY		
						25% SHRINKAGE FACTOR APPLIED	EMBANKMENT CU YD	BALANCE WASTE (+) SHORTAGE (-)
NB 155 BUSINESS/LINCOLN PARKWAY	LT	1+28.24	-	5+67.46	87	65	53	12
	LT	6+32.62	-	38+83.16	527	395	549	-154
	LT	40+39.97	-	48+52.74	289	217	19	198
	LT	48+64.29	-	67+83.47	594	446	299	147
	LT	69+24.94	-	72+78.86	126	95	10	85
	LT	73+26.33	-	76+90.11	74	56	0	56
SB 155 BUSINESS/LINCOLN PARKWAY	RT	1+32.21	-	5+72.07	121	91	3	88
	RT	6+29.56	-	18+65.16	417	313	88	225
	RT	20+83.61	-	38+47.84	383	287	86	201
	RT	41+07.75	-	47+96.61	187	140	44	96
	RT	48+07.86	-	72+03.54	747	560	281	279
	RT	72+86.01	-	76+12.85	46	35	7	28
FRONTAGE ROAD	LT	6+00.00	-	7+25.00	46	35	11	24
	LT	16+25.00	-	17+50.00	70	53	12	41
	LT	31+00.00	-	32+25.00	48	36	25	11
	LT	53+00.00	-	55+25.00	36	27	33	-6
IL 10/121/WOODLAWN ROAD	LT	135+63.60	-	137+16.87	32	24	36	-12
	LT	138+53.03	-	141+65.89	12	9	30	-21
STRINGER AVENUE	LT	19+69.70	-	21+00.00	97	73	16	57
EXSITING 36" PIPE CULVERT REMOVAL	RT	6+70.61	-	N/A	89	66	0	66
EXISTING 30" PIPE CULVERT REMOVAL	RT	54+61.28	-	N/A	58	43	0	43
EXISTING 30" PIPE CULVERT REMOVAL	RT	62+20.65	-	N/A	58	44	0	44
PPS #6-00625-0000 (STP) STAGE 1 TOTAL =					4147	3110	1617	1493

EARTHWORK SCHEDULE - STAGE 2								
PPS# 6-00625-0000 (STP)								
ROADWAY	SIDE	STATION	TO	STATION	20200100 EARTH EXCAVATION CU YD	FOR INFORMATION ONLY		
						25% SHRINKAGE FACTOR APPLIED	EMBANKMENT CU YD	BALANCE WASTE (+) SHORTAGE (-)
155 BUSINESS/LINCOLN PARKWAY	CENTER	9+63.40	-	18+56.74	176	132	418	-286
	CENTER	24+04.46	-	34+18.04	120	90	494	-404
	CENTER	44+87.15	-	48+23.50	73	55	90	-35
	CENTER	48+35.10	-	51+80.04	108	81	147	-66
	CENTER	52+39.71	-	72+56.51	240	180	1144	-964
PPS #6-00625-0000 (STP) STAGE 2 SUB-TOTAL =					717	538	2293	-1755
PPS# 6-00477-0000 (HSIP)								
ROADWAY	SIDE	STATION	TO	STATION	20200100 EARTH EXCAVATION CU YD	FOR INFORMATION ONLY		
						25% SHRINKAGE FACTOR APPLIED	EMBANKMENT CU YD	BALANCE WASTE (+) SHORTAGE (-)
155 BUSINESS/LINCOLN PARKWAY	CENTER	0+49.48	-	5+75.62	111	83	5	78
	CENTER	6+25.03	-	9+63.40	62	47	21	26
155 BUSINESS/LINCOLN PARKWAY & & 5TH STREET INTERSECTION	CENTER	34+18.04	-	39+09.47	203	152	48	104
	CENTER	40+23.12	-	44+87.15	228	171	0	171
PPS #6-00477-0000 (HSIP) STAGE 2 SUB-TOTAL =					604	453	74	379
STAGE 2 TOTAL =					1321	991	2367	-1376
COMBINED STAGE 1 (STP) AND STAGE 2 (STP & HSIP) TOTAL =					5468	4101	3984	117

20800150 - TRENCH BACKFILL									
PPS# 6+00477-0000 (HSIP)									
STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	TRENCH LENGTH	AVERAGE DEPTH	CU YD
2+70.99	2.2	RT		2+70.76	2.7	LT	5.0	2.6	1.0
2+70.81	2.4	LT		5+44.29	1.7	LT	279.0	2.6	54.1
5+49.29	1.7	LT		5+49.67	9.1	LT	7.4	2.6	1.4
5+49.29	1.7	LT		5+64.83	1.3	LT	15.6	2.6	3.0
6+39.11	0.8	RT		6+64.04	1.6	RT	25.0	2.6	4.9
6+51.64	9.5	RT		6+51.51	1.1	RT	8.4	2.6	1.6
6+77.70	1.8	RT		8+28.89	0.7	RT	151.2	2.6	29.3
PPS #6-00477-0000 (HSIP) TOTAL =									95.4

28000305 - TEMPORARY DITCH CHECKS SCHEDULE			
PPS# 6-00625-0000 (STP)			
DESCRIPTION	STATION	SIDE	FOOT
SB I-55 BUSINESS/LINCOLN PARKWAY	2+30.00	RT	10.0
	3+60.00	RT	10.0
	7+10.00	RT	10.0
	8+60.00	RT	10.0
	11+60.00	RT	10.0
	15+60.00	RT	10.0
	17+10.00	RT	10.0
	23+00.00	RT	10.0
	25+50.00	RT	10.0
	26+60.00	RT	10.0
	29+10.00	RT	10.0
	32+60.00	RT	10.0
	35+00.00	RT	10.0
	37+50.00	RT	10.0
	43+50.00	RT	10.0
	46+50.00	RT	10.0
	49+05.00	RT	10.0
	50+80.00	RT	10.0
	54+40.00	RT	10.0
	54+85.00	RT	10.0
59+50.00	RT	10.0	
61+50.00	RT	10.0	
62+50.00	RT	10.0	
64+70.00	RT	10.0	
67+20.00	RT	10.0	
69+60.00	RT	10.0	
71+00.00	RT	10.0	
NB I-55 BUSINESS/LINCOLN PARKWAY	3+90.00	LT	10.0
	8+30.00	LT	10.0
	10+80.00	LT	10.0
	13+30.00	LT	10.0
	15+80.00	LT	10.0
	17+60.00	LT	10.0
	20+10.00	LT	10.0
	22+60.00	LT	10.0
	25+10.00	LT	10.0
	27+60.00	LT	10.0
	29+60.00	LT	10.0
	31+15.00	LT	10.0
	34+40.00	LT	10.0
	36+90.00	LT	10.0
	42+90.00	LT	10.0
	45+40.00	LT	10.0
	47+40.00	LT	10.0
	50+00.00	LT	10.0
	50+60.00	LT	10.0
	51+50.00	LT	10.0
52+30.00	LT	10.0	
53+30.00	LT	10.0	
54+30.00	LT	10.0	
56+00.00	LT	10.0	
60+00.00	LT	10.0	
64+00.00	LT	10.0	
66+00.00	LT	10.0	
71+10.00	LT	10.0	
73+65.00	LT	10.0	
PPS# 6-00625-0000 (STP) TOTAL =			560

SEEDING SCHEDULE												
PPS# 6-00625-0000 (STP)												
ROADWAY	STATION	SIDE	TO	STATION	SIDE	25000200 SEEDING CLASS 2 (ACRE)	25000400 NITROGEN FERTILIZER NUTRIENTS (POUND)	25000500 PHOSPHORUS FERTILIZER NUTRIENTS (POUND)	25000600 POTASSIUM FERTILIZER NUTRIENTS (POUND)	25000700 AGRICULTURAL GROUND LIMESTONE (TON)	25100115 MULCH METHOD II (ACRE)	28000250 TEMPORARY EROSION CONTROL SEEDING (POUND)
NB I-55 BUSINESS/LINCOLN PARKWAY	1+28.24	LT	-	5+70.00	LT	0.19	17.5	17.5	17.5	0.39	0.19	19.4
	6+33.00	LT	-	38+84.00	LT	1.65	148.8	148.8	148.8	3.31	1.65	165.4
	40+11.40	LT	-	48+52.74	LT	0.52	46.9	46.9	46.9	1.04	0.52	52.2
	48+64.29	LT	-	67+97.95	LT	1.11	99.5	99.5	99.5	2.21	1.11	110.6
	68+95.00	LT	-	72+84.00	LT	0.41	36.7	36.7	36.7	0.82	0.41	40.8
	73+21.32	LT	-	76+92.00	LT	0.08	7.6	7.6	7.6	0.17	0.08	8.5
SB I-55 BUSINESS/LINCOLN PARKWAY	1+32.00	RT	-	5+74.14	RT	0.13	11.3	11.3	11.3	0.25	0.13	12.6
	6+27.11	RT	-	19+13.02	RT	0.59	53.3	53.3	53.3	1.18	0.59	59.2
	20+83.60	RT	-	38+52.91	RT	0.78	70.0	70.0	70.0	1.56	0.78	77.8
	41+00.00	RT	-	47+94.15	RT	0.27	24.7	24.7	24.7	0.55	0.27	27.4
	48+06.00	RT	-	51+99.96	RT	0.14	12.7	12.7	12.7	0.28	0.14	14.1
	52+20.96	RT	-	68+75.32	RT	0.87	78.1	78.1	78.1	1.73	0.87	86.7
	68+93.32	RT	-	72+30.69	RT	0.13	11.4	11.4	11.4	0.25	0.13	12.7
	72+76.96	RT	-	76+12.85	RT	0.04	3.8	3.8	3.8	0.09	0.04	4.3
FRONTAGE ROAD	6+00.00	LT	-	7+25.00	LT	0.07	6.4	6.4	6.4	0.14	0.07	7.1
	16+25.00	LT	-	17+50.00	LT	0.08	7.4	7.4	7.4	0.16	0.08	8.2
	31+00.00	LT	-	32+50.00	LT	0.07	6.7	6.7	6.7	0.15	0.07	7.5
	53+00.00	LT	-	55+25.00	LT	0.13	12.1	12.1	12.1	0.27	0.13	13.5
	62+00.00	LT	-	62+42.82	LT	0.03	2.9	2.9	2.9	0.06	0.03	3.2
	65+52.72	LT	-	66+08.42	LT	0.02	2.1	2.1	2.1	0.05	0.02	2.3
IL 10/121/WOODLAWN ROAD	135+63.60	LT	-	137+16.87	LT	0.06	5.6	5.6	5.6	0.12	0.06	6.2
	138+53.54	LT	-	141+65.89	LT	0.04	3.7	3.7	3.7	0.08	0.04	4.2
PPS #6-00625-0000 (STP) TOTALS =						7.4	669	669	669	15	7.4	744

28000400 - PERIMETER EROSION BARRIER SCHEDULE								
PPS# 6-00625-0000 (STP)								
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
SB I-55 BUSINESS/LINCOLN PARKWAY	15+49.99	80.6	RT	-	17+00.00	81.7	RT	152
SB I-55 BUSINESS/LINCOLN PARKWAY	29+50.00	60.0	RT	-	31+25.00	65.0	RT	177
NB I-55 BUSINESS/LINCOLN PARKWAY	53+00.00	141.7	LT	-	55+50.00	115.6	LT	252
PPS# 6-00625-0000 (STP) TOTAL =								581

31100200 - SUBBASE GRANULAR MATERIAL, TYPE B 4"						
PPS# 6-00625-0000 (STP)						
ROADWAY	STATION	SIDE	TO	STATION	SIDE	SQ YD
I55 BUSINESS/LINCOLN PARKWAY	9+63.40	CENTER	-	18+57.59	CENTER	2077
	14+58.26	RT	-	17+43.26	RT	131
	24+04.46	CENTER	-	34+18.04	CENTER	2351
	44+87.15	CENTER	-	48+18.87	CENTER	741
	48+39.74	CENTER	-	72+56.51	CENTER	5375
PPS# 6-00625-0000 (STP) TOTAL =						10675
PPS# 6-00477-0000 (HSIP)						
ROADWAY	STATION	SIDE	TO	STATION	SIDE	SQ YD
I55 BUSINESS/LINCOLN PARKWAY	1760+35.87	CENTER	-	1761+70.82	CENTER	105
	0+49.58	CENTER	-	5+75.68	CENTER	559
	6+25.02	CENTER	-	9+63.45	CENTER	500
	34+18.04	CENTER	-	39+09.46	CENTER	748
	40+23.14	CENTER	-	44+87.15	CENTER	659
IL 10/121/WOODLAWN ROAD	135+72.94	CENTER	-	137+35.50	CENTER	69
	138+39.10	CENTER	-	141+37.52	CENTER	131
PPS# 6-00477-0000 (HSIP) TOTAL =						2770
STP & HSIP COMBINED TOTAL =						13445

35101800 - AGGREGATE BASE COURSE, TYPE B 6"							
PPS# 6-00625-0000 (STP)							
LOCATION	TYPE OF ENTRANCE	EX MATERIAL TYPE	WIDTH (FT)	RT OFFSET (FT)	LT OFFSET (FT)	LENGTH (FT)	AGGREGATE BASE COURSE, TYPE B 6" (SQ. YD.)
52+10.46 RT	FE - RURAL	AGG	21.00	59.00	59.00	17.00	45
68+84.32 RT	FE - RURAL	EARTH	18.00	64.00	64.00	22.00	49
PPS# 6-00625-0000 (STP) TOTAL =							94

35300310 PORTLAND CEMENT CONCRETE BASE COURSE 8 1/2"						
PPS# 6-00625-0000 (STP)						
ROADWAY	STATION	TO	STATION	LOCATION	SQ YD	
I55 BUSINESS/LINCOLN PARKWAY	24+04.49	-	24+68.18	CENTER	149	
	51+80.04	-	52+39.71	CENTER	144	
	63+72.32	-	72+56.51	CENTER	1965	
PPS# 6-00625-0000 (STP) TOTAL =						2257
PPS# 6-00477-0000 (HSIP)						
ROADWAY	STATION	TO	STATION	LOCATION	SQ YD	
I55 BUSINESS/LINCOLN PARKWAY	6+25.03	-	9+63.40	CENTER	499.6	
PPS# 6-00477-0000 (HSIP) TOTAL =						500
STP AND HSIP COMBINED TOTAL =						2757

35300500 - PORTLAND CEMENT CONCRETE BASE COURSE 10"						
PPS# 6-00625-0000 (STP)						
ROADWAY	STATION	TO	STATION	LOCATION	SQ YD	
I55 BUSINESS/LINCOLN PARKWAY	15+20.75	-	18+57.59	CENTER	786	
STRINGER AVENUE	21+74.53	-	22+21.54	RT	43	
PPS# 6-00625-0000 (STP) TOTAL =						786
PPS# 6-00477-0000 (HSIP)						
ROADWAY	STATION	TO	STATION	LOCATION	SQ YD	
I55 BUSINESS/LINCOLN PARKWAY	1760+54.93	-	1761+70.61	CENTER	39	
	0+49.48	-	5+75.62	CENTER	559	
	34+18.04	-	39+09.52	CENTER	748	
	40+23.13	-	44+87.15	CENTER	659	
IL 10/121/WOODLAWN ROAD	135+72.94	-	137+35.50	CENTER	69	
	138+39.10	-	141+37.52	CENTER	131	
PPS# 6-00477-0000 (HSIP) TOTAL =						2205
STP AND HSIP COMBINED TOTALS =						2991

44004250 - PAVED SHOULDER REMOVAL											
PPS# 6-00625-0000 (STP)											
ROADWAY	LOCATION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	LENGTH	WIDTH	SQ YD
NB I-55 BUSINESS/LINCOLN PARKWAY	OUTSIDE	4+77.42	42.0	LT	-	5+57.72	39.5	LT	80	VARIES	38
	OUTSIDE	6+42.63	34.0	LT	-	33+72.41	34.0	LT	2730	1.5	455
	OUTSIDE	33+72.38	34.0	LT	-	35+73.21	34.0	LT	201	VARIES	120
	OUTSIDE	45+15.17	34.0	LT	-	48+46.89	34.0	LT	332	1.5	55
	OUTSIDE	48+58.17	34.0	LT	-	67+62.55	34.0	LT	1904	1.5	317
	OUTSIDE	69+24.97	34.0	LT	-	72+76.03	34.0	LT	351	1.5	59
	OUTSIDE	73+26.33	34.0	LT	-	73+46.61	34.0	LT	20	VARIES	3
	OUTSIDE	73+46.61	34.0	LT	-	76+90.11	34.0	LT	344	VARIES	65
	INSIDE	9+63.40	4.0	LT	-	17+12.15	11.0	LT	749	1.5	125
	INSIDE	17+12.15	11.0	LT	-	18+56.74	11.0	LT	145	VARIES	87
	INSIDE	24+04.60	11.0	LT	-	25+13.89	11.0	LT	109	VARIES	59
	INSIDE	25+13.89	11.0	LT	-	34+18.04	11.0	LT	904	1.5	151
	INSIDE	44+87.15	10.0	LT	-	48+29.19	10.0	LT	342	1.5	57
	INSIDE	48+40.47	10.0	LT	-	72+56.51	10.0	LT	2416	1.5	403
	SB I-55 BUSINESS/LINCOLN PARKWAY	INSIDE	9+63.40	10.0	RT	-	14+35.64	10.0	RT	472	1.5
INSIDE		14+35.64	10.0	RT	-	18+56.68	10.0	RT	421	VARIES	244
INSIDE		24+04.49	10.0	RT	-	25+13.87	10.0	RT	109	VARIES	60
INSIDE		25+13.87	10.0	RT	-	34+18.04	10.0	RT	904	1.5	151
INSIDE		44+87.15	10.0	RT	-	48+18.12	10.0	RT	331	1.5	55
INSIDE		48+29.93	10.0	RT	-	72+56.51	10.0	RT	2427	1.5	404
OUTSIDE		2+00.46	34.0	RT	-	5+67.86	34.0	RT	367	1.5	61
OUTSIDE		6+42.57	34.0	RT	-	14+35.36	34.0	RT	793	1.5	132
OUTSIDE		14+35.36	34.0	RT	-	17+64.57	34.0	RT	329	VARIES	162
OUTSIDE		24+95.37	34.0	RT	-	25+13.70	34.0	RT	18	VARIES	11
OUTSIDE		25+13.70	34.0	RT	-	33+70.56	34.0	RT	857	1.5	143
OUTSIDE		44+60.29	34.0	RT	-	45+13.68	34.0	RT	53	VARIES	38
OUTSIDE		45+13.68	34.0	RT	-	48+00.32	34.0	RT	287	1.5	48
OUTSIDE		48+12.23	34.0	RT	-	71+94.74	34.0	RT	2383	1.5	397
OUTSIDE		72+96.45	34.0	RT	-	73+39.64	34.0	RT	43	1.5	7
OUTSIDE	73+39.64	34.0	RT	-	76+12.85	34.0	RT	273	VARIES	56	
PPS# 6-00625-0000 (STP) TOTAL =											4003
PPS# 6-00477-0000 (HSIP)											
ROADWAY	LOCATION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	LENGTH	WIDTH	SQ YD
NB I55 BUSINESS/LINCOLN PARKWAY	OUTSIDE	8+25.94	4.0	LT	-	9+63.45	11.0	LT	1.5	138	23
	OUTSIDE	34+18.04	11.0	LT	-	34+86.86	11.0	LT	1.5	69	11
	OUTSIDE	44+42.96	10.0	LT	-	44+87.15	10.0	LT	1.5	44	7
SB I55 BUSINESS/LINCOLN PARKWAY	OUTSIDE	8+24.62	10.0	RT	-	9+63.45	10.0	RT	1.5	139	23
	OUTSIDE	34+18.04	10.0	RT	-	34+88.15	10.0	RT	1.5	70	12
	OUTSIDE	44+43.53	10.0	RT	-	44+87.15	10.0	RT	1.5	44	7
PPS# 6-00477-0000 (HSIP) TOTAL =											84
STP AND HSIP COMBINED TOTAL =											4087

35501318 - HOT-MIX ASPHALT BASE COURSE, 8 1/2"										
PPS# 6-00625-0000 (STP)										
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	LENGTH	WIDTH	SQ YD
SB I55 BUSINESS/LINCOLN PARKWAY	14+58.26	31.6	RT	-	17+43.26	44.5	RT	44.5	RT	99
PPS# 6-00625-0000 (STP) TOTAL =										99

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\IL\084EBIDINTEG\Illinois.gov\PI\DOT\Documents\DOT Offices\District 6\Projects\0672ED\Drawings\60\Sheets\0672E02-sht-05-Sched		CHECKED -	REVISED -		7706	*	LOGAN	343	55				
Default	PLOT SCALE = 40.0000' / in.	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 72E02				
	PLOT DATE = 10/21/2016	DATE -	REVISED -						ILLINOIS FED. AID PROJECT				

40600982 - HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT						
PPS# 6-00625-0000 (STP)						
ROADWAY	STATION	LANE	TO	STATION	LANE	SQ YD
STALHUT DRIVE	38+70.18	EB & WB	-	1758+15.93	EB & WB	49
5TH STREET (WEST)	12+41.03	EB & WB	-	12+51.03	EB & WB	62
	16+05.08	EB & WB	-	16+15.08	EB & WB	53
FRONTAGE ROAD (NORTH)	15+20.53	NB & SB	-	15+50.99	NB & SB	34
FRONTAGE ROAD (SOUTH)	15+31.25	NB & SB	-	15+51.25	NB & SB	22
NB I-55 BUSINESS/LINCOLN PARKWAY	48+25.87	NB	-	48+37.81	NB	32
	48+49.29	NB	-	48+61.20	NB	32
SB I-55 BUSINESS/LINCOLN PARKWAY	47+97.30	SB	-	48+09.23	SB	32
	48+20.76	SB	-	48+32.68	SB	32
STRINGER AVENUE	19+89.75	EB & WB	-	19+99.72	EB & WB	26
KENWOOD DRIVE		EB & WB	-		EB & WB	33
SB & NB I-55 BUSINESS/LINCOLN PARKWAY	73+50.00	NB & SB	-	74+00.00	NB & SB	378
PPS# 6-00625-0000 (STP) TOTAL =						784
PPS # 6-00477-0000 (HSIP)						
ROADWAY	STATION	LANE	TO	STATION	LANE	SQ YD
155 BUSINESS/LINCOLN PARKWAY	1758+05.93	NB	-	1758+15.93	NB	39
	1758+05.93	SB	-	1758+15.93	SB	27
IL 10/121/WOODLAWN ROAD	133+70.43	EB	-	134+20.00	EB	144
	133+70.43	WB	-	134+20.00	WB	203
	143+41.32	EB & WB	-	143+91.32	EB & WB	314
5TH STREET	12+41.03	EB & WB	-	12+51.03	EB & WB	62
PPS # 6-00477-0000 (HSIP) TOTAL =						789
STP AND HSIP COMBINED TOTAL =						1574

40600990 - TEMPORARY RAMP						
PPS# 6-00625-0000 (STP)						
ROADWAY	STATION	SIDE	TO	STATION	SIDE	SQ YD
NB I55 BUSINESS/LINCOLN PARKWAY	48+20.20	LT	-	48+50.72	LT	28
	48+38.97	LT	-	48+69.38	LT	29
	73+94.00	LT	-	74+00.00	LT	23
SB I55 BUSINESS/LINCOLN PARKWAY	47+89.06	RT	-	48+17.04	RT	28
	48+07.86	RT	-	48+38.30	RT	28
	73+94.00	RT	-	74+00.00	RT	23
STALHUT DRIVE	38+60.60	RT	-	38+66.61	RT	12
	44+87.15	LT	-	74+00.00	LT	17
PPS# 6-00625-0000 (STP) TOTAL =						188
PPS# 6-00477-0000 (HSIP)						
ROADWAY	STATION	SIDE	TO	STATION	SIDE	SQ YD
NB I-55 BUSINESS/LINCOLN PARKWAY	1758+05.93	LT	-	1758+11.91	LT	29
SB I-55 BUSINESS/LINCOLN PARKWAY	1758+05.93	RT	-	1758+11.91	RT	18
EB IL 10/121/WOODLAWN ROAD	133+70.43	RT	-	133+76.43	RT	18
	143+84.83	RT	-	143+91.32	RT	19
WB IL 10/121/WOODLAWN ROAD	133+70.43	LT	-	133+76.43	LT	26
	143+84.83	LT	-	143+91.32	LT	19
5TH STREET	12+41.03	RT	-	12+47.03	RT	15
	12+41.03	LT	-	12+47.03	LT	21
	16+09.08	RT	-	16+15.08	RT	15
	16+09.08	LT	-	16+15.08	LT	16
PPS # 6-00477-0000 (HSIP) TOTAL =						196
STP AND HSIP COMBINED TOTAL =						384

42400800 - DETECTABLE WARNINGS							
PPS# 6-00625-0000 (STP)							
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SQ FT
IL 10/121/WOODLAWN ROAD	138+53.32	72.5	LT	-	138+56.98	69.1	10
PPS# 6-00625-0000 (STP) TOTAL =							10

42400100 - PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH									
PPS# 6-00625-0000 (STP)									
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	SQ FT	
IL 10/121/WOODLAWN ROAD	135+63.60	50.0	LT	-	137+07.82	80.4	LT	769	
NB I55 BUSINESS/LINCOLN PARKWAY	138+58.57	78.6	LT	-	141+65.89	49.9	LT	1589	
PPS# 6-00625-0000 (STP) TOTAL =									2358

44000500 - COMBINATION CURB AND GUTTER REMOVAL								
PPS# 6-00625-0000 (STP)								
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
STRINGER AVENUE	20+43.44	14.1	LT		22+89.61	34.4	LT	446
PPS #6-00625-0000 (STP) TOTAL =								446

PPS# 6-00477-0000 (HSIP)								
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
155 BUSINESS/LINCOLN PARKWAY	1760+35.87	19.3	LT		1761+70.79	27.1	LT	134
	1760+36.69	12.0	LT		1761+70.79	22.9	LT	134
	0+49.48	0.8	LT		5+75.62	8.0	LT	527
	0+49.48	10.4	RT		5+75.62	2.8	LT	527
	6+25.03	2.6	RT		8+25.90	2.5	LT	201
	6+25.03	7.1	RT		8+24.00	10.0	RT	199
	34+79.66	1.1	LT		39+08.93	5.5	LT	436
	34+79.56	3.1	RT		39+07.60	3.3	LT	436
	40+27.67	2.0	RT		44+49.17	3.9	LT	433
	40+25.86	7.1	RT		44+49.35	4.0	RT	433
IL 10/121/WOODLAWN ROAD	135+71.11	0.9	LT		137+08.74	1.1	LT	138
	135+71.11	10.9	LT		137+08.74	11.0	LT	138
PPS #6-00477-0000 (HSIP) TOTAL =								3736
STP AND HSIP COMBINED TOTAL =								4182

44000600 - SIDEWALK REMOVAL								
PPS# 6-00625-0000 (STP)								
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	SQ FT
IL 10/121/WOODLAWN ROAD	138+57.74	69.3	LT	-	138+57.74	69.3	LT	30
IL 10/121/WOODLAWN ROAD	141+55.48	49.5	LT	-	141+65.40	44.3	LT	41
PPS# 6-00625-0000 (STP) TOTAL =								71

44003100 - MEDIAN REMOVAL						
PPS# 6-00477-0000 (HSIP)						
ROADWAY	STATION	SIDE	TO	STATION	SIDE	SQ FT
IL 10/121/WOODLAWN ROAD	137+08.76	CENTER	-	137+35.50	CENTER	222
	138+39.10	CENTER	-	142+19.96	CENTER	3250
PPS #6-00477-0000 (HSIP) TOTAL =						3472

44000100 - PAVEMENT REMOVAL								
PPS# 6-00625-0000 (STP)								
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	SQ YD
SB I-55 BUSINESS/LINCOLN PARKWAY	14+58.26	31.7	RT	-	16+07.78	41.3	RT	25
	16+80.51	44.5	RT	-	17+43.26	44.5	RT	10
STRINGER AVENUE	21+04.09	12.0	LT	-	23+21.36	43.3	LT	275
PPS# 6-00625-0000 (STP) TOTAL =								311

44000200 - DRIVEWAY PAVEMENT REMOVAL								
PPS# 6-00625-0000 (STP)								
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	SQ YD
IL 10/121/WOODLAWN ROAD	140+09.50	49.2	LT	-	141+63.89	41.3	LT	141
PPS# 6-00625-0000 (STP) TOTAL =								141

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
pw:\IL084EBIDINTEG\Illinois.gov\PIDOT\Documents\IDOT Offices\District 6\Projects\0672EDR\Drawings\EA0\Sheets\0672E02-sht-05-Sch-REMOVED		CHECKED -	REVISED -
Default	PLOT SCALE = 40.0000' / in.	DATE -	REVISED -
	PLOT DATE = 10/21/2016		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	*	LOGAN	343	56
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72E02	

PAVING (SHOULDER) SCHEDULE - (STP)										
PPS #6-00625-0000										
ROADWAY	STATION	SIDE	TO	STATION	SIDE	48102100 AGGREGATE WEDGE SHOULDER, TYPE B TON	48203023 HOT-MIX ASPHALT SHOULDERS, 6 1/2" SQ YD	48203100 HOT-MIX ASPHALT SHOULDERS TON	35501316 HOT-MIX ASPHALT BASE COURSE, 8" SQ YD	40600290 BITUMINOUS MATERIALS (TACK COAT) POUND
NB I55 BUSINESS/LINCOLN PARKWAY	4+77.42	LT - OUTSIDE	-	5+63.03	LT - OUTSIDE	12	0	5	56	25
	6+32.62	LT - OUTSIDE	-	35+73.21	LT - OUTSIDE	286	0	266	2607	1173
	45+15.19	LT - OUTSIDE	-	48+52.74	LT - OUTSIDE	29	0	32	316	142
	48+64.29	LT - OUTSIDE	-	67+83.47	LT - OUTSIDE	155	0	148	1697	763
	69+24.94	LT - OUTSIDE	-	72+76.03	LT - OUTSIDE	36	0	32	310	140
	73+26.33	LT - OUTSIDE	-	76+90.11	LT - OUTSIDE	21	0	27	173	77
	9+63.40	LT - INSIDE	-	15+20.75	LT - INSIDE	0	248	21	0	111
	24+68.19	LT - INSIDE	-	34+18.04	LT - INSIDE	0	422	35	0	190
	44+87.15	LT - INSIDE	-	48+29.98	LT - INSIDE	0	152	13	0	68
	48+38.97	LT - INSIDE	-	51+80.04	LT - INSIDE	0	152	13	0	68
52+39.71	LT - INSIDE	-	63+72.32	LT - INSIDE	0	504	42	0	227	
SB I55 BUSINESS/LINCOLN PARKWAY	1+32.22	RT - OUTSIDE	-	5+72.07	RT - OUTSIDE	51	0	54	367	164
	6+29.56	RT - OUTSIDE	-	17+64.57	RT - OUTSIDE	87	0	80	877	395
	17+64.57	RT - OUTSIDE	-	18+65.16	RT - OUTSIDE	8	0	4	0	20
	20+83.61	RT - OUTSIDE	-	24+95.37	RT - OUTSIDE	34	0	34	0	189
	24+95.37	RT - OUTSIDE	-	33+70.56	RT - OUTSIDE	78	0	88	788	355
	44+60.29	RT - OUTSIDE	-	47+96.21	RT - OUTSIDE	38	0	44	301	135
	48+07.86	RT - OUTSIDE	-	72+03.54	RT - OUTSIDE	241	0	300	2121	955
	72+86.01	RT - OUTSIDE	-	76+12.85	RT - OUTSIDE	43	0	36	167	79
	9+63.40	RT - INSIDE	-	15+20.75	RT - INSIDE	0	248	21	0	111
	24+68.19	RT - INSIDE	-	34+18.04	RT - INSIDE	0	422	35	0	190
	44+88.09	RT - INSIDE	-	48+19.57	RT - INSIDE	0	147	13	0	66
	48+28.55	RT - INSIDE	-	51+80.04	RT - INSIDE	0	156	13	0	70
	52+39.71	RT - INSIDE	-	63+72.32	RT - INSIDE	0	503	42	0	227
PPS #6-00625-0000 (STP) TOTAL =						1119	2954	1398	9780	5940

PAVING (SHOULDER) SCHEDULE - (HSIP)										
PPS #6-00477-0000										
ROADWAY	STATION	SIDE	TO	STATION	SIDE	48102100 AGGREGATE WEDGE SHOULDER, TYPE B TON	48203023 HOT-MIX ASPHALT SHOULDERS, 6 1/2" SQ YD	48203100 HOT-MIX ASPHALT SHOULDERS TON	35501316 HOT-MIX ASPHALT BASE COURSE, 8" SQ YD	40600290 BITUMINOUS MATERIALS (TACK COAT) POUND
NB I55 BUSINESS/LINCOLN PARKWAY	1758+06.02	LT - OUTSIDE	-	1760+96.49	LT - OUTSIDE	0	0	30	0	113
	1+28.28	LT - OUTSIDE	-	4+77.42	LT - OUTSIDE	28	0	15	0	113
	35+73.21	LT - OUTSIDE	-	38+83.16	LT - OUTSIDE	26	0	58	0	146
	40+39.96	LT - OUTSIDE	-	45+15.19	LT - OUTSIDE	51	0	31	0	146
SB I55 BUSINESS/LINCOLN PARKWAY	1758+05.77	RT - OUTSIDE	-	1760+96.99	RT - OUTSIDE	0	0	17	0	74
	33+70.56	RT - OUTSIDE	-	38+47.84	RT - OUTSIDE	45	0	38	0	74
	41+07.75	RT - OUTSIDE	-	44+60.29	RT - OUTSIDE	39	0	37	0	191
PPS #6-00477-0000 (HSIP) TOTAL =						189	0	226	0	857
STP AND HSIP COMBINED TOTAL =						1308	2954	1624	9780	6797

PAVING SCHEDULE - ROADWAY (PPS# 6-00625-0000 STP)											
ROADWAY	STATION	SIDE	TO	STATION	SIDE	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON	40603335 HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON	40603535 POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON	40800050 INCIDENTAL HMA SURFACING (1 3/4") TON	40600290 BITUMINOUS MATERIALS (TACK COAT) POUND
NB I55 BUSINESS/LINCOLN PARKWAY	9+63.40	LT	-	18+75.00	LT	348	171	232	0	0	2486
	18+75.00	LT	-	20+82.79	LT	33	0	66	0	0	707
	20+82.79	LT	-	34+18.04	LT	489	167	326	0	0	3495
	44+87.16	LT	-	48+00.00	LT	106	94	71	0	0	756
	48+00.00	LT	-	48+45.64	LT	0	0	11	0	0	68
	48+41.52	LT	-	48+90.00	LT	0	0	12	0	0	73
SB I55 BUSINESS/LINCOLN PARKWAY	9+63.40	RT	-	18+75.00	RT	232	234	278	0	0	2981
	18+75.00	RT	-	20+82.79	RT	33	0	66	0	0	706
	20+82.79	RT	-	34+18.04	RT	280	253	336	0	0	3597
	44+87.16	RT	-	47+50.00	RT	49	105	59	0	0	631
	47+50.00	RT	-	48+17.04	RT	0	0	18	0	0	107
	48+12.97	RT	-	48+75.00	RT	0	0	16	0	0	98
STALHUT DRIVE	38+60.00	LT & RT	-	39+66.18	LT & RT	46	0	91	0	0	731
5TH STREET	14+81.96	LT & RT	-	16+15.08	LT & RT	50	0	101	0	0	811
STRINGER AVENUE	19+89.75	LT & RT	-	22+36.77	LT & RT	0	0	0	0	100	462
KENWOOD DRIVE	71+94.63	RT	-	72+96.53	RT	0	0	0	0	31	140
CEMETARY ENTRANCE (EAST)	72+76.03	LT	-	73+26.33	RT	0	0	0	0	7	30
CROWN CORRECTION LINCOLN PARKWAY	15+20.75	CENTER	-	18+50.00	CENTER	12	0	0	0	0	0
	24+04.47	CENTER	-	24+68.19	CENTER	4	0	0	0	0	0
	63+72.32	CENTER	-	72+56.61	CENTER	40	0	0	0	0	0
PPS #6-00625-0000 (STP) TOTAL =						3275	2651	3017	0	138	32148
PAVING SCHEDULE - ROADWAY (PPS # 6+00477-0000 HSIP)											
ROADWAY	STATION	SIDE	TO	STATION	SIDE	40600625 LEVELING BINDER (MACHINE METHOD) N50 (3/4", 1 1/4", 2 1/4") TON	X4060605 LEVELING BINDER (MACHINE METHOD) SPECIAL (VAR. DEPTH) TON	40603335 HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON	40603535 POLYMERIZED HMA SURFACE COURSE MIX "D", N50 (1 1/2") TON	40800050 INCIDENTAL HMA SURFACING (1 3/4") TON	40600290 BITUMINOUS MATERIALS (TACK COAT) POUND
NB I55 BUSINESS/LINCOLN PARKWAY	1758+06.11	LT	-	1761+95.16	LT	54	0	108	0	0	869
	0+35.50	LT	-	1+32.14	LT	29	0	59	0	0	472
	1+32.10	LT	-	9+63.40	LT	452	170	301	0	0	3225
	34+18.04	LT	-	38+00.00	LT	187	116	125	0	0	1337
	38+00.00	LT	-	41+60.00	LT	67	0	134	0	0	1081
SB I55 BUSINESS/LINCOLN PARKWAY	1758+06.01	RT	-	1761+95.16	RT	83	0	165	0	0	1329
	0+35.50	RT	-	1+32.14	RT	15	0	31	0	0	247
	1+32.14	RT	-	9+63.40	RT	222	585	266	0	0	2849
	34+18.04	RT	-	38+00.00	RT	127	120	153	0	0	1636
	38+00.00	RT	-	41+60.00	RT	60	0	120	0	0	960
EB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES)	133+70.43	RT	-	137+53.56	RT	84	0	0	169	0	1358
	137+53.56	RT	-	138+19.80	RT	11	0	0	22	0	173
	138+19.80	RT	-	143+90.83	RT	75	0	0	150	0	1206
WB IL 10/121/WOODLAWN ROAD (INCLUDES RIGHT TURN LANES)	133+70.43	LT	-	137+53.56	LT	54	0	0	107	0	861
	137+53.56	LT	-	138+19.80	LT	11	0	0	22	0	175
	138+19.80	LT	-	143+90.83	LT	119	0	0	239	0	1918
5TH STREET (WEST)	12+41.03	LT & RT	-	13+30.19	LT & RT	49	0	99	0	0	794
5TH STREET (EAST)	14+00.19	LT & RT	-	14+81.96	LT & RT	32	0	64	0	0	518
SIDE ROAD (EAST)	5+57.68	LT	-	6+42.63	LT	0	0	0	0	55	130
SIDE ROAD (WEST)	5+67.86	RT	-	6+42.57	RT	0	0	0	0	46	135
CROWN CORRECTION LINCOLN PARKWAY	6+25.04	CENTER	-	9+63.40	CENTER	15	0	0	0	0	0
	34+18.04	CENTER	-	38+00.00	CENTER	10	0	0	0	0	0
	41+00.00	CENTER	-	44+87.50	CENTER	25	0	0	0	0	0
PPS #6-00477-0000 (HSIP) TOTAL =						2057	1039	1856	709	101	23741
STP AND HSIP COMBINED TOTAL =						5332	3690	4873	709	239	55889

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBIDINTEG\Illinois.gov\PIDOT\Documents\DOT Offices\District 6\Projects\0672EDRAWING\0672E02-sht-05-Sched	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		7706	*	LOGAN	343	58				
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	CONTRACT NO. 72E02	

PAVEMENT PATCHING SCHEDULE															
PPS #6-00625-0000 (STP)															
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	LENGTH	WIDTH	44200094 PAVEMENT PATCHING TYPE II, 8 INCH SQ YD	44200090 PAVEMENT PATCHING TYPE III, 8 INCH SQ YD	44200101 PAVEMENT PATCHING TYPE IV, 8 INCH SQ YD	44200120 PAVEMENT PATCHING TYPE II, 10 INCH SQ YD	44200144 PAVEMENT PATCHING TYPE II, 12 INCH SQ YD	44200148 PAVEMENT PATCHING TYPE III, 12 INCH SQ YD
NB 155 BUSINESS/LINCOLN PARKWAY	5+70.17	50.2	LT	-	6+34.39	48.8	LT	VARIES	VARIES			102.0			
	47+75.00	10.0	LT	-	47+75.00	34.0	LT	24.0	5.0					13.3	
	49+50.00	10.0	LT	-	49+50.00	34.0	LT	24.0	4.5					12.0	
SB 155 BUSINESS/LINCOLN PARKWAY	47+75.00	10.0	RT	-	47+75.00	RT	LT	24.0	5.0					13.3	
	6+71.26	67.5	LT	-	6+71.38	88.1	LT	20.4	7.7		17.5				
POSTVILLE DRIVE (FRONTAGE ROAD)	16+80.51	67.5	LT	-	16+80.29	88.5	LT	21.1	7.6		17.8				
	31+71.27	66.1	LT	-	31+71.27	86.7	LT	20.6	6.6		15.1				
	54+61.72	69.3	LT	-	54+61.63	87.4	LT	18.0	8.7		17.4				
	62+20.65	69.3	LT	-	62+20.65	87.3	LT	18.0	6.7	13.4					
STRINGER AVENUE	20+81.19	11.9	RT	-	20+80.69	11.9	LT	24.3	5.5			14.9			
NB 155 BUSINESS/LINCOLN PARKWAY AS DIRECTED BY ENGINEER	LOCATIONS DETERMINED BY ENGINEER							VARIES	VARIES					316.0	60.0
SB 155 BUSINESS/LINCOLN PARKWAY AS DIRECTED BY ENGINEER	LOCATIONS DETERMINED BY ENGINEER							VARIES	VARIES					355.0	34.0
PPS #6-00625-0000 (STP) TOTAL =										13.4	67.8	102.0	15	710	94.0

50104400 - CONCRETE HEADWALL REMOVAL				
PPS# 6-00625-0000 (STP)				
ROADWAY	STATION	OFFSET	SIDE	EACH
I-55 BUSINESS/LINCOLN PARKWAY	25+08.15	1.7	RT	1.0
	65+83.65	92.5	LT	1.0
	66+10.62	60.2	LT	1.0
	66+22.97	45.0	LT	1.0
	66+96.81	43.6	RT	1.0
IL 10/121/WOODLAWN ROAD	135+90.26	43.6	LT	1.0
PPS# 6-00625-0000 (STP) TOTAL =				6.0

60265700 - VALVE VAULTS TO BE ADJUSTED				
PPS# 6-00625-0000 (STP)				
ROADWAY	STATION	OFFSET	SIDE	EACH
5TH STREET	15+10.53	35.2	LT	1.0
5TH STREET	15+17.52	32.6	RT	1.0
PPS# 6-00625-0000 (STP) TOTAL =				2
PPS# 6-00477-0000 (HSIP)				
ROADWAY	STATION	OFFSET	SIDE	EACH
IL 10/121/WOODLAWN ROAD	136+37.35	35.7	LT	1.0
	139+99.76	33.1	LT	1.0
	140+14.03	30.5	LT	1.0
5TH STREET	12+62.83	29.9	LT	1.0
	13+02.09	41.4	LT	1.0
PPS# 6-00477-0000 (HSIP) TOTAL =				5
STP AND HSIP COMBINED TOTAL =				7

55100500 - STORM SEWER REMOVAL 12"								
PPS# 6-00477-0000 (HSIP)								
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
I55 BUSINESS/LINCOLN PARKWAY	2+70.99	2.2	RT		2+70.76	2.7	LT	5.0
	2+70.81	2.4	LT		5+64.83	1.3	LT	294.0
	5+49.26	1.7	LT		5+49.64	9.1	LT	7.0
	6+39.11	0.8	RT		6+70.87	1.9	RT	32.0
	6+51.51	1.1	RT		6+51.60	9.5	RT	9.0
	6+70.77	1.8	RT		8+28.89	0.7	RT	158.0
PPS# 6-00477-0000 (HSIP) TOTAL =								505

60500060 - REMOVING INLETS				
PPS# 6-00477-0000 (HSIP)				
ROADWAY	STATION	OFFSET	SIDE	EACH
I55 BUSINESS/LINCOLN PARKWAY	2+70.79	2.7	LT	1.0
	2+70.96	2.1	RT	1.0
	5+49.67	9.1	LT	1.0
	5+49.29	1.8	LT	1.0
	6+51.51	1.1	RT	1.0
	6+51.64	9.5	RT	1.0
	8+28.89	0.7	RT	1.0
PPS# 6-00477-0000 (HSIP) TOTAL =				7

6025550 - MANHOLES TO BE ADJUSTED				
PPS# 6-00625-0000 (STP)				
ROADWAY	STATION	OFFSET	SIDE	EACH
STALHUT DRIVE	39+04.26	23.5	LT	1.0
5TH STREET	15+64.86	28.2	RT	1.0
PPS# 6-00625-0000 (STP) TOTAL =				2
PPS# 6-00477-0000 (HSIP)				
ROADWAY	STATION	OFFSET	SIDE	EACH
5TH STREET	12+67.34	26.0	RT	1.0
	14+31.59	26.5	LT	1.0
IL 10/121/WOODLAWN ROAD	136+38.88	44.0	RT	1.0
PPS# 6-00477-0000 (HSIP) TOTAL =				3
STP AND HSIP COMBINED TOTAL =				5

60603800 - COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12								
PPS# 6-00625-0000 (STP)								
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
NB 155 BUSINESS/LINCOLN PARKWAY	46+42.00	4.4	LT	-	47+42.00	4.4	LT	100
	48+50.94	4.4	LT	-	50+17.00	4.4	LT	166
SB 155 BUSINESS/LINCOLN PARKWAY	46+42.00	4.4	RT	-	48+07.70	4.4	RT	166
	49+17.00	4.4	RT	-	50+17.00	4.4	RT	100
PPS# 6-00625-0000 (STP) TOTAL =								532

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
p:\IL\084EBID\INTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 6\Projects\0672\DRAWING\DATA\EA0\Sheets\0672E02-sht-05-Sched		DRAWN -	REVISED -
Default	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/21/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	*	LOGAN	343	59
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72E02	

60604400 - COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18								
PPS# 6-00625-0000 (STP)								
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
STRINGER AVENUE	19+69.66	13.5	LT	-	20+84.08	13.4	LT	114
PPS# 6-00625-0000 (STP) TOTAL =								114

60605000 - COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24								
PPS# 6-00625-0000 (STP)								
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
IL 10/121/WOODLAWN ROAD	140+09.49	46.7	LT	-	140+63.05	46.6	LT	54
	141+11.33	43.4	LT	-	141+63.62	38.7	LT	53
STRINGER AVENUE	20+84.08	13.4	LT	-	22+37.35	5.2	RT	158
PPS# 6-00625-0000 (STP) TOTAL =								265

60608562 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12								
PPS# 6-00625-0000 (STP)								
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
NB 155 BUSINESS/LINCOLN PARKWAY	9+70.40	5.4	LT	-	15+13.75	5.4	LT	544
	24+75.18	5.4	LT	-	34+11.04	5.4	LT	936
	44+94.15	4.4	LT	-	44+42.00	4.4	LT	148
	47+42.00	4.4	LT	-	48+13.39	4.4	LT	72
	50+17.00	4.4	LT	-	51+73.04	0.8	LT	157
	52+46.71	0.8	LT	-	63+65.32	4.4	LT	1119
SB 155 BUSINESS/LINCOLN PARKWAY	9+70.40	4.4	RT	-	15+13.75	4.4	RT	544
	24+75.18	4.4	RT	-	34+11.04	4.4	RT	936
	44+94.15	4.4	RT	-	46+42.00	4.4	RT	148
	48+45.22	4.4	RT	-	49+17.00	4.4	RT	72
	50+17.00	4.4	RT	-	51+73.04	4.4	RT	156
	52+46.71	4.4	RT	-	63+65.32	4.4	RT	1119
PPS# 6-00625-0000 (STP) TOTAL =								5951

60618300 - CONCRETE MEDIAN SURFACE, 4 INCH						
PPS# 6-00625-0000 (STP)						
ROADWAY	STATION	SIDE	TO	STATION	SIDE	SQ FT
155 BUSINESS/LINCOLN PARKWAY	9+70.40	CENTER	-	15+13.75	CENTER	5298
155 BUSINESS/LINCOLN PARKWAY	24+75.18	CENTER	-	34+11.04	CENTER	9209
155 BUSINESS/LINCOLN PARKWAY	44+94.15	CENTER	-	48+13.39	CENTER	2797
155 BUSINESS/LINCOLN PARKWAY	48+45.22	CENTER	-	51+73.04	CENTER	2853
155 BUSINESS/LINCOLN PARKWAY	52+46.71	CENTER	-	63+65.32	CENTER	9869
PPS# 6-00625-0000 (STP) TOTAL =						30026

60622305 - CONCRETE MEDIAN, TYPE SM-4.12						
PPS# 6-00625-0000 (STP)						
ROADWAY	STATION	LOCATION	TO	STATION	LOCATION	SQ FT
155 BUSINESS/LINCOLN PARKWAY	9+63.40	CENTER	-	9+70.40	CENTER	91
	15+13.75	CENTER	-	15+20.75	CENTER	91
	24+68.19	CENTER	-	24+75.18	CENTER	147
	34+11.04	CENTER	-	34+18.04	CENTER	91
	44+87.15	CENTER	-	44+94.16	CENTER	84
	48+06.69	CENTER	-	48+15.01	CENTER	100
	48+35.86	CENTER	-	48+44.20	CENTER	100
	51+73.04	CENTER	-	51+80.04	CENTER	47
	52+39.71	CENTER	-	52+46.71	CENTER	47
63+65.32	CENTER	-	63+72.32	CENTER	84	
PPS# 6-00625-0000 (STP) TOTAL =						882

60622400 CONCRETE MEDIAN, TYPE SM-6.06					
PPS# 6-00477-0000 (HSIP)					
ROADWAY	STATION	TO	STATION	LOCATION	SQ FT
155 BUSINESS/LINCOLN PARKWAY	1760+36.69	-	1761.46.71	CENTER	603
PPS #6-00477-0000 (HSIP) TOTAL =					603

60622800 CONCRETE MEDIAN, TYPE SM-6.12					
PPS# 6-00477-0000 (HSIP)					
ROADWAY	STATION	TO	STATION	LOCATION	SQ FT
IL 10/121/WOODLAWN ROAD	135+71.11	-	137+09.59	CENTER	961
IL 10/121/WOODLAWN ROAD	138+83.13	-	141+37.52	CENTER	1600
PPS #6-00477-0000 (HSIP) TOTAL =					2561

50105220 - PIPE CULVERT REMOVAL										
PPS# 6-00625-000 (STP)										
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT	
155 BUSINESS/LINCOLN PARKWAY	30" CMP	5+59.86	53.7	LT	-	6+39.99	52.0	LT	80	
	36" CMP	6+71.16	58.6	LT	-	6+71.85	92.5	LT	34	
	36" CMP	6+70.52	65.8	RT	-	6+69.82	99.8	RT	34	
	12" PLASTIC	6+28.08	104.8	LT	-	6+67.50	98.0	LT	40	
	18" CMP	6+97.55	110.3	LT	-	6+94.65	105.1	LT	6	
	30" CMP	16+80.64	60.7	LT	-	16+79.89	93.8	LT	33	
	12" RCP	16+80.53	2.2	LT	-	16+98.50	2.1	LT	12	
	24" CONC	25+08.15	44.3	LT	-	25+08.08	49.3	RT	5	
	15" CONC	31+58.16	0.5	RT	-	31+65.16	0.6	RT	7	
	15" CONC	31+77.21	0.5	RT	-	31+84.20	0.9	RT	7	
	30" CMP	31+71.42	61.7	LT	-	31+71.31	93.1	LT	32	
	24" CMP	47+76.06	54.4	RT	-	47+82.07	45.2	RT	11	
	24" CMP	48+10.15	2.1	RT	-	48+11.24	0.4	LT	2	
	24" CMP	48+12.47	1.5	LT	-	48+13.56	3.2	LT	2	
	24" CMP	48+41.94	46.7	LT	-	48+50.13	59.3	LT	15	
	15" CMP	50+22.35	0.3	LT	-	50+33.35	0.0	LT	11	
	15" CMP	50+45.76	0.3	RT	-	50+56.76	0.6	RT	11	
	15" CMP	51+79.11	0.0	RT	-	52+39.19	0.3	LT	60	
	30" CMP	54+61.28	61.9	LT	-	54+62.89	92.4	LT	32	
	30" CMP	54+62.27	63.3	RT	-	54+63.69	93.3	RT	30	
	15" CONC	61+85.54	0.1	RT	-	62+14.33	0.2	LT	29	
	15" CONC	62+26.99	0.1	LT	-	62+55.99	0.2	LT	29	
	30" CMP	62+20.44	61.8	RT	-	62+20.35	91.9	RT	30	
	24" CMP	62+20.88	60.6	LT	-	62+20.32	93.6	LT	33	
	15" CMP	68+30.88	0.3	LT	-	69+38.71	0.0	LT	108	
	15" CMP	68+51.95	53.0	RT	-	69+11.84	52.9	RT	60	
	15" CMP	135+88.70	36.9	LT	-	135+90.26	43.6	LT	7	
	IL 10/121/WOODLAWN ROAD	15" CMP	135+88.70	36.9	LT	-	135+90.26	43.6	LT	7
	PPS# 6-00625-000 (STP) TOTAL =									760

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
p:\11\084EBID\INTEG\Illinois.gov\PI\DOT\Documents\DOT Offices\District 6\Projects\0672E02\Drawings\0672E02-sht-05-Schedule of Quantities.dwg		CHECKED -	REVISED -
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	*	LOGAN	343	60
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72E02	

70300100 - SHORT TERM PAVEMENT MARKING									
PPS# 6-00625-0000 (STP)									
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
NB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	9+63.40	22.5	LT	-	34+18.04	22.5	LT	246
	SHOULDER DIAGONAL	9+63.40	33.5	LT	-	34+18.04	33.5	LT	196
	YELLOW CENTERLINE	15+20.75	11.5	LT	-	19+60.75	11.5	LT	88
	TURN LANE	20+09.80	7.3	LT	-	21+99.62	11.5	LT	19
	YELLOW CENTERLINE	20+49.34	7.0	RT	-	24+68.19	11.5	LT	84
SB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	9+63.40	22.0	RT	-	34+18.04	22.0	RT	246
	SHOULDER DIAGONAL	9+63.40	33.5	RT	-	15+10.08	33.5	RT	44
	YELLOW CENTERLINE	15+20.75	10.5	RT	-	19+60.75	10.5	RT	88
	TURN LANE	16+58.26	335.5	RT	-	19+48379	33.5	RT	29
	YELLOW CENTERLINE	20+49.34	10.5	RT	-	24+68.19	10.5	RT	84
	SHOULDER DIAGONAL	21+75.76	33.5	RT	-	24+68.18	33.5	RT	23
NB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	44+87.15	22.0	LT	-	74+00.00	22.0	LT	291
	SHOULDER DIAGONAL	44+87.15	34.0	LT	-	74+00.00	34.0	LT	233
	YELLOW CENTERLINE	63+72.32	10.0	LT	-	68+13.03	10.0	LT	88
	YELLOW CENTERLINE	69+18.56	10.0	LT	-	72+19.16	10.0	LT	60
	TURN LANE	73+14.08	9.0	LT	-	74+49.51	9.0	LT	14
SB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	44+87.15	22.0	RT	-	74+00.00	22.0	RT	291
	SHOULDER DIAGONAL	44+87.15	34.0	RT	-	74+00.00	34.0	RT	233
	YELLOW CENTERLINE	63+72.32	10.0	RT	-	68+13.03	1.5	LT	88
	TURN LANE	66+02.25	10.5	RT	-	68+17.25	10.5	RT	22
	YELLOW CENTERLINE	69+56.39	10.5	RT	-	72+18.35	1.5	LT	53
	TURN LANE	71+07.82	10.5	RT	-	72+22.82	10.5	RT	12
STALHUT DRIVE	YELLOW CENTERLINE	73+14.08	6.0	RT	-	74+49.51	6.0	RT	28
	YELLOW CENTERLINE	38+70.37	0.0	RT	-	39+61.58	0.0	RT	18
5TH STREET	YELLOW CENTERLINE	15+75.08	5.0	RT	-	16+15.08	5.0	RT	8
	TURN LANE	15+75.08	6.0	LT	-	16+15.08	6.0	LT	4
PPS# 6-00625-0000 (STP) TOTAL =									2587

PPS# 6-00477-0000 (HSIP)									
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
NB I-55 BUSINESS/LINCOLN PARKWAY AT IL 10 INTERSECTION	WHITE CENTERLINE	1758+05.90	43.0	LT	-	1761+79.62	43.0	LT	37
	WHITE CENTERLINE	0+45.28	23.0	LT	-	9+63.40	22.5	LT	92
	WHITE TURN LANE	1758+05.82	31.0	LT	-	1759+40.65	31.0	LT	14
	WHITE TURN LANE	0+38.89	7.1	LT	-	2+21.62	11.0	LT	18
	YELLOW CENTERLINE	0+71.02	7.0	RT	-	5+47.21	11.5	LT	95
	WHITE TURN LANE	6+22.44	6.6	LT	-	7+66.51	11.0	LT	14
	YELLOW CENTERLINE	6+50.05	7.6	RT	-	9+63.40	11.5	LT	63
	SHOULDER DIAGONALS	1758+05.93	55.0	LT	-	1760+58.46	55.0	LT	20
NB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION	SHOULDER DIAGONALS	6+32.39	34.0	LT	-	9+63.40	34.0	LT	26
	WHITE CENTERLINE	34+18.04	22.5	LT	-	39+18.66	22.5	LT	50
	YELLOW CENTERLINE	34+18.04	11.5	LT	-	38+95.49	11.5	LT	96
	SHOULDER DIAGONALS	34+18.04	33.5	LT	-	38+82.86	33.5	LT	37
	WHITE CENTERLINE	40+10.91	23.0	LT	-	44+87.16	22.0	LT	48
	WHITE TURN LANE	39+88.22	36.0	LT	-	42+14.44	36.0	LT	23
SB I-55 BUSINESS/LINCOLN PARKWAY AT IL 10 INTERSECTION	WHITE TURN LANE	40+03.67	6.1	LT	-	42+25.15	11.0	LT	22
	WHITE CENTERLINE	1758+04.88	1.0	RT	-	1761+74.65	1.0	RT	37
	WHITE TURN LANE	1759+87.66	13.0	RT	-	1761+94.66	13.0	RT	21
	WHITE TURN LANE	1760+46.08	11.0	LT	-	1761+93.02	14.3	LT	15
	SHOULDER DIAGONALS	1758+05.90	12.8	RT	-	1760+97.78	25.0	RT	23
	WHITE CENTERLINE	0+35.41	22.0	RT	-	9+63.40	22.0	RT	93
	SHOULDER DIAGONALS	1+33.22	33.5	RT	-	5+72.07	33.5	RT	34
	SHOULDER DIAGONALS	6+29.56	33.5	RT	-	9+63.40	33.5	RT	27
	YELLOW CENTERLINE	0+71.10	11.0	RT	-	5+47.32	7.0	LT	95
	WHITE TURN LANE	4+18.06	10.5	RT	-	5+83.74	6.7	RT	17
SB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION	YELLOW CENTERLINE	6+50.14	10.5	RT	-	9+63.40	10.5	RT	63
	WHITE CENTERLINE	40+08.42	22.0	RT	-	39+18.66	22.0	RT	50
	SHOULDER DIAGONALS	41+06.57	33.5	LT	-	39+18.66	33.5	LT	30
	YELLOW CENTERLINE	40+32.54	10.5	RT	-	44+87.15	10.5	RT	91

CONTINUED ON NEXT SHEET

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG\Illinois.gov\PIDOT\Documents\DOT Offices\District 6\Projects\0672\Drawings\EA0\Sheets\0672E02-sht-05-Sch-REVISED	PLLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -						7706	*	LOGAN	343	61
Default	PLLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	CONTRACT NO. 72E02	
											ILLINOIS FED. AID PROJECT		

70300100 - SHORT TERM PAVEMENT MARKING - CONTINUED									
EB IL 10/121/WOODLAWN DRIVE	WHITE CENTERLINE	133+66.35	23.0	RT	-	137+39.66	23.0	RT	37
	WHITE TURN LANE	135+34.84	34.0	RT	-	137+53.22	35.0	RT	22
	WHITE TURN LANE	135+73.27	11.0	RT	-	137+48.93	7.1	RT	18
	WHITE CENTERLINE	138+34.01	23.0	RT	-	143+90.84	17.2	RT	56
	YELLOW CENTERLINE	138+52.87	8.8	RT	-	138+86.13	9.2	RT	7
	YELLOW CENTERLINE	142+19.97	3.7	LT	-	143+90.72	6.4	LT	34
WB IL 10/121/WOODLAWN DRIVE	WHITE TURN LANE	142+75.41	6.3	RT	-	143+90.84	6.3	RT	12
	WHITE CENTERLINE	133+66.35	23.2	LT	-	137+36.34	23.0	LT	37
	WHITE TURN LANE	133+66.35	11.2	LT	-	134+63.24	11.1	LT	10
	WHITE CENTERLINE	138+34.02	22.4	LT	-	143+90.84	17.3	LT	56
	WHITE TURN LANE	138+24.93	6.9	LT	-	140+99.456	9.8	LT	27
	WHITE TURN LANE	138+20.06	34.4	LT	-	140+76.58	34.4	LT	26
5TH STREET	YELLOW CENTERLINE	12+41.03	6.0	LT	-	12+90.28	6.0	LT	10
	WHITE TURN LANE	12+41.03	6.0	RT	-	13+16.16	6.0	RT	8
	YELLOW CENTERLINE	14+33.99	6.0	RT	-	15+20.66	6.5	RT	17
	WHITE TURN LANE	14+19.18	6.0	LT	-	15+20.66	6.0	LT	10
	WHITE TURN LANE	14+19.18	18.0	LT	-	15+20.66	18.0	LT	10
PPS #6-00477-0000 (HSIP) TOTAL =									1646
STP AND HSIP COMBINED TOTAL =									4233

70300150 - SHORT TERM PAVEMENT MARKING REMOVAL									
PPS# 6-00625-0000 (STP)									
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	SQ FT
NB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	9+63.40	22.5	LT	-	34+18.04	22.5	LT	81
	SHOULDER DIAGONAL	9+63.40	33.5	LT	-	34+18.04	33.5	LT	65
	YELLOW CENTERLINE	15+20.75	11.5	LT	-	19+60.75	11.5	LT	29
	TURN LANE	20+09.80	7.3	LT	-	21+99.62	11.5	LT	6
	YELLOW CENTERLINE	20+49.34	7.0	RT	-	24+68.19	11.5	LT	28
SB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	9+63.40	22.0	RT	-	34+18.04	22.0	RT	81
	SHOULDER DIAGONAL	9+63.40	33.5	RT	-	15+10.08	33.5	RT	14
	YELLOW CENTERLINE	15+20.75	10.5	RT	-	19+60.75	10.5	RT	29
	TURN LANE	16+58.26	335.5	RT	-	19+48379	33.5	RT	10
	YELLOW CENTERLINE	20+49.34	10.5	RT	-	24+68.19	10.5	RT	28
	SHOULDER DIAGONAL	21+75.76	33.5	RT	-	24+68.18	33.5	RT	8
NB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	44+87.15	22.0	LT	-	74+00.00	22.0	LT	96
	SHOULDER DIAGONAL	44+87.15	34.0	LT	-	74+00.00	34.0	LT	77
	YELLOW CENTERLINE	63+72.32	10.0	LT	-	68+13.03	10.0	LT	29
	YELLOW CENTERLINE	69+18.56	10.0	LT	-	72+19.16	10.0	LT	20
	TURN LANE	73+14.08	9.0	LT	-	74+49.51	9.0	LT	4
SB I55 BUSINESS/LINCOLN PARKWAY	WHITE CENTERLINE	44+87.15	22.0	RT	-	74+00.00	22.0	RT	96
	SHOULDER DIAGONAL	44+87.15	34.0	RT	-	74+00.00	34.0	RT	77
	YELLOW CENTERLINE	63+72.32	10.0	RT	-	68+13.03	1.5	LT	29
	TURN LANE	66+02.25	10.5	RT	-	68+17.25	10.5	RT	7
	YELLOW CENTERLINE	69+56.39	10.5	RT	-	72+18.35	1.5	LT	17
	TURN LANE	71+07.82	10.5	RT	-	72+22.82	10.5	RT	4
STALHUT DRIVE	YELLOW CENTERLINE	73+14.08	6.0	RT	-	74+49.51	6.0	RT	9
	YELLOW CENTERLINE	38+70.37	0.0	RT	-	39+61.58	0.0	RT	6
5TH STREET	YELLOW CENTERLINE	15+75.08	5.0	RT	-	16+15.08	5.0	RT	3
	TURN LANE	15+75.08	6.0	LT	-	16+15.08	6.0	LT	1
PPS# 6-00625-0000 (STP) TOTAL =									854
PPS# 6-00477-0000 (HSIP)									
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	SQ FT
NB I-55 BUSINESS/LINCOLN PARKWAY AT IL 10 INTERSECTION	WHITE CENTERLINE	1758+05.90	43.0	LT	-	1761+79.62	43.0	LT	12
	WHITE CENTERLINE	0+45.28	23.0	LT	-	9+63.40	22.5	LT	30
	WHITE TURN LANE	1758+05.82	31.0	LT	-	1759+40.65	31.0	LT	4
	WHITE TURN LANE	0+38.89	7.1	LT	-	2+21.62	11.0	LT	6
	YELLOW CENTERLINE	0+71.02	7.0	RT	-	5+47.21	11.5	LT	31
	WHITE TURN LANE	6+22.44	6.6	LT	-	7+66.51	11.0	LT	5
CONTINUED ON NEXT SHEET									

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG\Illinois.gov\PI\DOT\Documents\DOT Offices\District 6\Projects\0672\DRAWING\DATA\EA0\Sheets\0672E02-sht-05-Sch	DESIGNED -	REVISED -	7706						*	LOGAN	343	62	
Default	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 72E02								
	PLOT DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT	

70300150 - SHORT TERM PAVEMENT MARKING REMOVAL - CONTINUED									
	YELLOW CENTERLINE	6+50.05	7.6	RT	-	9+63.40	11.5	LT	21
	SHOULDER DIAGONALS	1758+05.93	55.0	LT	-	1760+58.46	55.0	LT	7
	SHOULDER DIAGONALS	6+32.39	34.0	LT	-	9+63.40	34.0	LT	9
NB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION	WHITE CENTERLINE	34+18.04	22.5	LT	-	39+18.66	22.5	LT	17
	YELLOW CENTERLINE	34+18.04	11.5	LT	-	38+95.49	11.5	LT	32
	SHOULDER DIAGONALS	34+18.04	33.5	LT	-	38+82.86	33.5	LT	12
	WHITE CENTERLINE	40+10.91	23.0	LT	-	44+87.16	22.0	LT	16
	WHITE TURN LANE	39+88.22	36.0	LT	-	42+14.44	36.0	LT	7
	WHITE TURN LANE	40+03.67	6.1	LT	-	42+25.15	11.0	LT	7
SB I-55 BUSINESS/LINCOLN PARKWAY AT IL 10 INTERSECTION	WHITE CENTERLINE	1758+04.88	1.0	RT	-	1761+74.65	1.0	RT	12
	WHITE TURN LANE	1759+87.66	13.0	RT	-	1761+94.66	13.0	RT	7
	WHITE TURN LANE	1760+46.08	11.0	LT	-	1761+93.02	14.3	LT	5
	SHOULDER DIAGONALS	1758+05.90	12.8	RT	-	1760+97.78	25.0	RT	8
	WHITE CENTERLINE	0+35.41	22.0	RT	-	9+63.40	22.0	RT	31
	SHOULDER DIAGONALS	1+33.22	33.5	RT	-	5+72.07	33.5	RT	11
	SHOULDER DIAGONALS	6+29.56	33.5	RT	-	9+63.40	33.5	RT	9
	YELLOW CENTERLINE	0+71.10	11.0	RT	-	5+47.32	7.0	LT	31
	WHITE TURN LANE	4+18.06	10.5	RT	-	5+83.74	6.7	RT	5
	YELLOW CENTERLINE	6+50.14	10.5	RT	-	9+63.40	10.5	RT	21
	WHITE CENTERLINE	40+08.42	22.0	RT	-	39+18.66	22.0	RT	17
	SB I-55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION	SHOULDER DIAGONALS	41+06.57	33.5	LT	-	39+18.66	33.5	LT
YELLOW CENTERLINE		40+32.54	10.5	RT	-	44+87.15	10.5	RT	30
WHITE CENTERLINE		133+66.35	23.0	RT	-	137+39.66	23.0	RT	12
EB IL 10/121/WOODLAWN DRIVE	WHITE TURN LANE	135+34.84	34.0	RT	-	137+53.22	35.0	RT	7
	WHITE TURN LANE	135+73.27	11.0	RT	-	137+48.93	7.1	RT	6
	WHITE CENTERLINE	138+34.01	23.0	RT	-	143+90.84	17.2	RT	18
	YELLOW CENTERLINE	138+52.87	8.8	RT	-	138+86.13	9.2	RT	2
	YELLOW CENTERLINE	142+19.97	3.7	LT	-	143+90.72	6.4	LT	11
	WHITE TURN LANE	142+75.41	6.3	RT	-	143+90.84	6.3	RT	4
WB IL 10/121/WOODLAWN DRIVE	WHITE CENTERLINE	133+66.35	23.2	LT	-	137+36.34	23.0	LT	12
	WHITE TURN LANE	133+66.35	11.2	LT	-	134+63.24	11.1	LT	3
	WHITE CENTERLINE	138+34.02	22.4	LT	-	143+90.84	17.3	LT	18
	WHITE TURN LANE	138+24.93	6.9	LT	-	140+99.456	9.8	LT	9
	WHITE TURN LANE	138+20.06	34.4	LT	-	140+76.58	34.4	LT	8
5TH STREET	YELLOW CENTERLINE	12+41.03	6.0	LT	-	12+90.28	6.0	LT	3
	WHITE TURN LANE	12+41.03	6.0	RT	-	13+16.16	6.0	RT	2
	YELLOW CENTERLINE	14+33.99	6.0	RT	-	15+20.66	6.5	RT	6
	WHITE TURN LANE	14+19.18	6.0	LT	-	15+20.66	6.0	LT	3
	WHITE TURN LANE	14+19.18	18.0	LT	-	15+20.66	18.0	LT	3
PPS #6-00477-0000 (HSIP) TOTAL =									543
STP AND HSIP COMBINED TOTAL =									1397

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 6\Projects\0672\Drawings\EA0\Sheets\0672E02-sht-05-Sch	DRAWN -	CHECKED -	REVISED -						7706	•	LOGAN	343	63
Default	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 72E02				
	PLOT DATE = 10/21/2016	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

70300210 - TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS					
PPS# 6-00625-0000 (STP)					
ROADWAY	DESCRIPTION	STAGE 1			
		STATION	OFFSET	SIDE	SQ FT
SB I-55 BUSINESS/LINCOLN PARKWAY	RIGHT TURN ARROW	17+48.60	31.0	RT	15.6
	RIGHT TURN ARROW	18+29.43	31.0	RT	15.6
PPS #6-00625-0000 (STP) STAGE 1 TOTAL =					31.2
ROADWAY	DESCRIPTION	STAGE 2			
		STATION	OFFSET	SIDE	FOOT
NB I-55 BUSINESS/LINCOLN PARKWAY	LEFT TURN ARROW	20+35.68	17.0	LT	15.6
	LEFT TURN ARROW	21+35.68	17.0	LT	15.6
PPS #6-00625-0000 (STP) STAGE 2 TOTAL =					31.2
ROADWAY	DESCRIPTION	STAGE 3			
		STATION	OFFSET	SIDE	SQ FT
SB I-55 BUSINESS/LINCOLN PARKWAY	RIGHT TURN ARROW	17+48.60	31.0	RT	15.6
	RIGHT TURN ARROW	18+29.43	31.0	RT	15.6
PPS #6-00625-0000 (STP) STAGE 3 TOTAL =					31.2
70300210 - TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS					
PPS# 6-00477-0000 (HSIP)					
ROADWAY	DESCRIPTION	STAGE 1			
		STATION	OFFSET	SIDE	SQ FT
NB I-55 BUSINESS/LINCOLN PARKWAY	RIGHT TURN ARROW	1+82.91	31.5	LT	15.6
	RIGHT TURN ARROW	2+82.89	31.5	LT	15.6
	RIGHT TURN ARROW	40+61.41	31.0	RT	15.6
	RIGHT TURN ARROW	41+61.32	31.0	RT	15.6
NB I-55 BUSINESS/LINCOLN PARKWAY	RIGHT TURN ARROW	36+95.96	31.0	LT	15.6
	RIGHT TURN ARROW	37+98.45	31.0	LT	15.6
PPS #6-00477-0000 (HSIP) STAGE 1 TOTAL =					93.6
ROADWAY	DESCRIPTION	STAGE 2			
		STATION	OFFSET	SIDE	FOOT
NB I-55 BUSINESS/LINCOLN PARKWAY	LEFT TURN ARROW	1758+09.88	34.0	LT	15.6
	LEFT TURN ARROW	1758+98.63	34.0	LT	15.6
	LEFT TURN ARROW	0+61.29	15.0	LT	15.6
	LEFT TURN ARROW	1+61.29	15.0	LT	15.6
	LEFT TURN ARROW	6+32.78	17.0	RT	15.6
	LEFT TURN ARROW	7+09.77	17.0	RT	15.6
	LEFT TURN ARROW	40+35.74	13.0	RT	15.6
	LEFT TURN ARROW	41+35.74	13.0	RT	15.6
SB I-55 BUSINESS/LINCOLN PARKWAY	LEFT TURN ARROW	1760+66.14	8.6	RT	15.6
	LEFT TURN ARROW	1761+59.56	8.6	RT	15.6
	LEFT TURN ARROW	4+57.05	16.5	RT	15.6
	LEFT TURN ARROW	5+59.89	16.5	RT	15.6
	LEFT TURN ARROW	37+79.15	14.5	RT	15.6
	LEFT TURN ARROW	38+82.13	14.5	RT	15.6
EB IL 10/121/WOODLAWN AVENUE	LEFT TURN ARROW	135+90.58	14.4	RT	15.6
	LEFT TURN ARROW	137+22.15	14.4	RT	15.6
EB IL 10/121/WOODLAWN AVENUE	LEFT TURN ARROW	133+71.87	14.0	LT	15.6
	LEFT TURN ARROW	134+46.58	14.0	LT	15.6
	LEFT TURN ARROW	138+61.99	14.8	LT	15.6
	LEFT TURN ARROW	140+41.33	14.8	LT	15.6
	LEFT TURN ARROW	140+41.33	14.8	LT	15.6
PPS #6-00477-0000 (HSIP) STAGE 2 TOTAL =					312.0
ROADWAY	DESCRIPTION	STAGE 3			
		STATION	OFFSET	SIDE	SQ FT
NB I-55 BUSINESS/LINCOLN PARKWAY	RIGHT TURN ARROW	1+82.91	31.5	LT	15.6
	RIGHT TURN ARROW	2+82.89	31.5	LT	15.6
	RIGHT TURN ARROW	40+61.41	31.0	RT	15.6
	RIGHT TURN ARROW	41+61.32	31.0	RT	15.6
NB I-55 BUSINESS/LINCOLN PARKWAY	RIGHT TURN ARROW	36+95.96	31.0	LT	15.6
	RIGHT TURN ARROW	37+98.45	31.0	LT	15.6
PPS #6-00477-0000 (HSIP) STAGE 3 TOTAL =					93.6
STP AND HSIP COMBINED TOTAL =					593

70300220 - TEMPORARY PAVEMENT MARKING - LINE 4"		
PPS# 6-00625-0000 (STP)		
STAGE 1		2800
STAGE 2		15343
STAGE 3		5000
PPS# 6-00625-0000 (STP) TOTAL =		23143
PPS# 6-00477-0000 (HSIP)		
STAGE 1 TOTAL =	STAGE 1	2600
STAGE 2 TOTAL =	STAGE 2	6307
STAGE 3 TOTAL =	STAGE 3	5200
PPS# 6-00477-0000 (HSIP) TOTAL =		14107
STP AND HSIP COMBINED TOTAL =		37250

X0327979 - PAVEMENT MARKING REMOVAL - GRINDING		
PPS# 6-00625-0000 (STP)		
	SQ FT	
STAGE 1	378	
STAGE 2	724	
STAGE 3	3905	
STAGE 4	1450	
PPS# 6-00625-0000 (STP) TOTAL =		6457
PPS# 6-00477-0000 (HSIP)		
STAGE 1	172	
STAGE 2	688	
STAGE 3	2060	
STAGE 4	1516	
PPS# 6-00477-0000 (HSIP) TOTAL =		2920
STP AND HSIP COMBINED TOTAL =		9377

X0327980 - PAVEMENT MARKING REMOVAL - WATER BLASTING		
PPS# 6-00625-0000 (STP)		
	SQ FT	
STAGE 1 - NB LANES SOUTH END	84	
STAGE 2 - NB LANES SOUTH END	262	
STAGE 3 - NB LANES SOUTH END	1221	
STAGE 4 - NB LANES SOUTH END	262	
PPS# 6-00625-0000 (STP) TOTAL =		1829
PPS# 6-00477-0000 (HSIP)		
STAGE 1 - SB LANES NORTH END	84	
STAGE 2 - SB LANES NORTH END	388	
STAGE 3 - SB LANES NORTH END	457	
STAGE 4 - SB LANES NORTH END	388	
PPS# 6-00477-0000 (HSIP) TOTAL =		929
STP AND HSIP COMBINED TOTAL =		2758

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
pw\11084EBIDINTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 6\Projects\0672\Drawings\0672\0672E02-sht-05-Schedule of Quantities		CHECKED -	REVISED -
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	*	LOGAN	343	64
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72E02	

78000650 THERMOPLASTIC PAVEMENT MARKING - LINE 24"									
PPS# 6-00625-0000 (STP)									
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
SB I55 BUSINESS/LINCOLN PARKWAY	STOP BAR	19+28.99	8.7	RT	-	19+28.99	34.0	RT	24
	RR WARNING SIGN	44+59.14	10.0	RT	-	44+59.14	34.0	RT	24
	RR WARNING SIGN	45+09.00	10.0	RT	-	45+09.00	34.0	RT	24
NB I55 BUSINESS/LINCOLN PARKWAY	STOP BAR	20+13.95	11.0	LT	-	20+13.95	34.0	LT	24
	STOP BAR	20+50.94	7.2	RT	-	20+50.40	4.8	LT	12
	RR WARNING SIGN	51+09.00	11.0	LT	-	51+09.00	34.0	LT	24
STALHUT DRIVE	RR WARNING SIGN	51+58.86	11.0	LT	-	51+58.86	34.0	LT	24
	STOP BAR	39+34.14	52.2	RT	-	39+46.97	39.5	RT	18
	STOP BAR	39+52.39	5.5	RT	-	39+52.63	18.0	RT	12
5TH STREET	STOP BAR	39+23.34	53.9	LT	-	39+36.10	41.2	LT	18
	STOP BAR	15+15.34	49.1	LT	-	15+35.00	45.9	LT	21
	STOP BAR	15+43.85	43.5	RT	-	15+58.34	43.4	RT	15
STRINGER RD	STOP BAR	21+17.87	58.7	RT	-	21+35.74	56.6	RT	18
	STOP BAR	21+61.66	33.4	RT	-	21+66.92	20.4	RT	14
PPS# 6-00625-0000 (STP) TOTAL =									272

78000650 - THERMOPLASTIC PAVEMENT MARKING - LINE 24"									
PPS# 6-00477-0000 (HSIP)									
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
SB I55 BUSINESS/LINCOLN PARKWAY	STOP BAR	17+61.91	27.3	LT	-	1761+64.29	15.9	LT	12
	STOP BAR	1761+74.65	11.0	LT	-	1761+74.65	13.0	RT	24
	STOP BAR	1761+64.25	30.0	RT	-	1761+53.15	44.1	RT	18
	STOP BAR	38+94.23	10.0	LT	-	38+94.66	2.0	RT	12
	STOP BAR	39+18.66	10.5	RT	-	39+18.66	35.0	RT	25
	STOP BAR	39+13.57	83.7	RT	-	39+27.35	72.3	RT	18
NB I55 BUSINESS/LINCOLN PARKWAY	STOP BAR	0+49.20	66.9	LT	-	0+62.95	78.1	LT	18
	STOP BAR	0+45.29	11.0	LT	-	0+45.29	35.0	LT	24
	STOP BAR	0+72.25	6.9	LT	-	0+71.54	4.5	LT	12
	STOP BAR	39+95.95	55.8	LT	-	40+10.15	66.9	LT	18
	STOP BAR	40+10.91	11.0	LT	-	40+10.91	36.0	LT	24
	STOP BAR	40+33.63	9.4	RT	-	40+33.11	2.6	LT	12
EB IL10/121/WOODLAWN ROAD	STOP BAR	137+22.24	7.1	LT	-	137+22.76	4.3	RT	12
	STOP BAR	137+39.66	11.0	RT	-	137+39.65	35.1	RT	24
	STOP BAR	137+20.31	72.5	RT	-	137+33.37	60.2	RT	18
WBIL10/121/WOODLAWN ROAD	STOP BAR	138+54.37	7.4	RT	-	138+51.10	4.1	LT	12
	STOP BAR	138+34.02	10.4	LT	-	138+34.02	34.4	LT	24
	STOP BAR	138+48.60	50.1	LT	-	138+59.58	64.3	LT	18
SIDE ROAD (EAST)	STOP BAR	5+71.14	48.8	LT	-	5+99.64	48.2	LT	29
SIDE ROAD (WEST)	STOP BAR	6+02.47	50.1	RT	-	6+25.25	50.2	RT	24
5TH STREET	STOP BAR	12+89.23	5.5	LT	-	12+89.38	6.0	RT	12
	STOP BAR	13+10.19	46.0	RT	-	12+96.15	57.3	RT	18
	STOP BAR	13+16.16	6.0	RT	-	13+16.16	17.8	RT	12
	STOP BAR	14+19.18	6.0	LT	-	14+19.18	38.5	LT	34
	STOP BAR	14+34.43	6.0	RT	-	14+34.43	6.0	LT	12
PPS# 6-00477-0000 (HSIP) TOTAL =									466
STP AND HSIP COMBINED TOTAL =									738

78003100 PREFORMED PLASTIC PAVEMENT MARKING, TPE B - LETTER AND SYMBOLS						
PPS# 6-00625-0000 (STP)						
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	SQ FT	
SB I55 BUSINESS/LINCOLN PARKWAY	RIGHT TURN ARROW	16+65.52	44.7	RT	15.6	
	RIGHT TURN ARROW	17+73.94	44.9	RT	15.6	
	RIGHT TURN ARROW	18+77.01	44.6	RT	15.6	
	RR WARNING R	44+85.05	12.8	RT	3.6	
	RR WARNING R	44+85.05	19.3	RT	3.6	
	RR WARNING R	44+85.05	24.6	RT	3.6	
	RR WARNING R	44+85.05	31.3	RT	3.6	
	RR WARNING X	44+85.05	16.1	RT	54.0	
	RR WARNING X	44+85.05	27.9	RT	54.0	
	LEFT TURN ARROW	66+08.50	1.5	RT	15.6	
	LEFT TURN ARROW	67+07.78	1.5	RT	15.6	
	LEFT TURN ARROW	67+96.52	1.5	RT	15.6	
	LEFT TURN ARROW	71+14.38	2.0	RT	15.6	
	LEFT TURN ARROW	72+04.76	2.0	RT	15.6	
	NB I55 BUSINESS/LINCOLN PARKWAY	LEFT TURN ARROW	20+72.61	2.1	RT	15.6
		LEFT TURN ARROW	21+39.17	0.8	LT	15.6
		LEFT TURN ARROW	21+94.89	3.4	LT	15.6
		RR WARNING R	51+32.93	13.1	LT	3.6
RR WARNING R		51+32.93	19.6	LT	3.6	
RR WARNING R		51+32.93	24.3	LT	3.6	
RR WARNING R		51+32.93	30.8	LT	3.6	
RR WARNING X		51+32.93	16.5	LT	54.0	
RR WARNING X		51+32.93	27.6	LT	54.0	
LEFT TURN ARROW		73+34.11	0.0	LT	15.6	
LEFT TURN ARROW		74+21.25	0.0	LT	15.6	
5TH STREET		LEFT TURN ARROW	15+78.89	2.4	RT	15.6
	LEFT TURN ARROW	16+10.68	2.1	RT	15.6	
STRINGER AVENUE	RIGHT TURN /THRU ARROW	15+82.09	18.0	LT	26.0	
	RIGHT TURN ARROW	68+08.11	77.2	LT	15.6	
	LEFT TURN ARROW	68+40.41	83.1	LT	15.6	
PPS# 6-00625-0000 (STP) TOTAL =					536	

PPS# 6-00477-0000 (HSIP)						
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	SQ FT	
NB I55 BUSINESS/LINCOLN PARKWAY AT IL 10 INTERSECTION	LEFT TURN ARROW	1758+12.82	23.0	LT	15.6	
	LEFT TURN ARROW	1759+31.94	23.0	LT	15.6	
	LEFT TURN ARROW	1760+48.12	19.0	LT	15.6	
	LEFT TURN ARROW	1761+38.69	23.0	LT	15.6	
	LEFT TURN ARROW	1+96.72	2.0	LT	15.6	
	RIGHT TURN ARROW	0+97.11	45.8	LT	15.6	
	RIGHT TURN ARROW	1+97.31	42.7	LT	15.6	
	RIGHT TURN ARROW	2+97.31	42.7	LT	15.6	
	LEFT TURN ARROW	5+46.41	4.5	LT	15.6	
	LEFT TURN ARROW	7+51.43	1.3	LT	15.6	
	NB I55 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION	LEFT TURN ARROW	37+94.47	1.9	LT	15.6
		LEFT TURN ARROW	38+69.40	5.1	LT	15.6
RIGHT TURN ARROW		40+26.66	46.6	LT	15.6	
RIGHT TURN ARROW		41+15.19	44.3	LT	15.6	
RIGHT TURN ARROW		42+08.27	44.4	LT	15.6	
LEFT TURN ARROW		42+08.30	2.1	LT	15.6	
SB I55 BUSINESS/LINCOLN PARKWAY AT IL 10 INTERSECTION	RIGHT TURN ARROW	1759+92.46	21.0	RT	15.6	
	RIGHT TURN ARROW	1760+64.39	22.0	RT	15.6	
	RIGHT TURN ARROW	1761+34.64	25.0	RT	15.6	
	LEFT TURN ARROW	0+96.84	2.3	RT	15.6	
	LEFT TURN ARROW	4+46.52	0.1	RT	15.6	
	LEFT TURN ARROW	6+51.61	4.8	RT	15.6	
CONTINUED ON NEXT SHEET						

78003100 PREFORMED PLASTIC PAVEMENT MARKING, TPE B - LETTER AND SYMBOLS - CONTINUED					
SB 155 BUSINESS/LINCOLN PARKWAY AT 5TH STREET INTERSECTION	RIGHT TURN ARROW	36+77.28	42.8	RT	15.6
	RIGHT TURN ARROW	37+80.11	43.6	RT	15.6
	RIGHT TURN ARROW	38+71.82	43.6	RT	15.6
	LEFT TURN ARROW	37+19.54	1.4	RT	15.6
	LEFT TURN ARROW	40+58.44	4.5	RT	15.6
IL10/121/WOODLAWN ROAD	LEFT TURN ARROW	41+33.37	1.2	RT	15.6
	LEFT TURN ARROW	133+90.17	2.5	LT	15.6
	LEFT TURN ARROW	134+59.35	3.3	LT	15.6
	RIGHT TURN ARROW	135+40.67	43.7	RT	15.6
	RIGHT TURN ARROW	136+18.31	43.4	RT	15.6
	RIGHT TURN ARROW	136+93.84	45.8	RT	15.6
	LEFT TURN ARROW	135+97.51	1.7	RT	15.6
	LEFT TURN ARROW	136+97.41	2.8	LT	15.6
	LEFT TURN ARROW	138+79.31	4.2	RT	15.6
	LEFT TURN ARROW	139+79.28	1.8	RT	15.6
	LEFT TURN ARROW	140+79.25	0.6	LT	15.6
	RIGHT TURN ARROW	138+76.55	45.7	LT	15.6
	RIGHT TURN ARROW	139+66.61	42.9	LT	15.6
	RIGHT TURN ARROW	140+72.24	42.8	LT	15.6
	LEFT TURN ARROW	142+80.67	0.9	LT	15.6
5TH STREET	LEFT TURN ARROW	143+34.24	1.5	LT	15.6
	LEFT TURN ARROW	143+79.47	1.5	LT	15.6
	LEFT TURN ARROW	12+71.77	3.0	LT	15.6
	RIGHT TURN & THRU ARROW	12+47.71	13.3	RT	26.0
	THRU ARROW	13+05.52	12.6	RT	11.5
	RIGHT TURN ARROW	14+38.55	27.3	LT	15.6
	RIGHT TURN ARROW	15+03.06	27.5	LT	15.6
	THRU ARROW	14+34.06	12.2	LT	11.5
	THRU ARROW	15+11.28	11.9	LT	11.5
	LEFT TURN ARROW	14+59.07	2.4	RT	15.6
LEFT TURN ARROW	15+14.44	1.8	RT	15.6	
PPS #6-00477-0000 (HSIP) TOTAL =					825
STP AND HSIP COMBINED TOTAL =					1361

78003120 PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 5"										
PPS# 6-00625-0000 (STP)										
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT	
SB 155 BUSINESS/LINCOLN PARKWAY	30' SKIP - 10' DASH	1747+18.82	0.0	RT	-	1754+61.15	0.0	RT	186	
	30' SKIP - 10' DASH	9+63.40	22.0	RT	-	19+28.99	22.0	RT	242	
	30' SKIP - 10' DASH	20+09.36	22.0	RT	-	34+18.04	22.0	RT	353	
	30' SKIP - 10' DASH	44+87.15	22.0	RT	-	48+09.23	22.0	RT	81	
	30' SKIP - 10' DASH	48+20.76	22.0	RT	-	74+00.00	22.0	RT	645	
NB 155 BUSINESS/LINCOLN PARKWAY	30' SKIP - 10' DASH	9+63.40	22.5	LT	-	19+37.74	22.5	LT	244	
	30' SKIP - 10' DASH	20+13.95	22.5	LT	-	34+18.04	22.5	LT	351	
	30' SKIP - 10' DASH	44+87.16	22.0	LT	-	48+37.81	22.0	LT	88	
	30' SKIP - 10' DASH	48+49.29	22.0	LT	-	74+00.00	22.0	LT	638	
	30' SKIP - 10' DASH	80+23.13	22.0	LT	-	92+14.15	22.0	LT	300	
PPS# 6-00625-0000 (STP) TOTAL =										3128

PPS# 6-00477-0000 (HSIP)										
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT	
NB 155 BUSINESS/LINCOLN PARKWAY	SB CENTER SKIP DASH LINE	1758+04.88	1.0	RT	-	1761+74.65	1.0	RT	93	
	SB CENTER SKIP DASH LINE	0+35.41	22.0	RT	-	9+63.40	22.0	RT	240	
	SB CENTER SKIP DASH LINE	34+18.04	22.0	RT	-	39+18.66	22.0	RT	125	
	SB CENTER SKIP DASH LINE	40+08.42	22.0	RT	-	44+87.15	22.0	RT	120	
SB 155 BUSINESS/LINCOLN PARKWAY	NB CENTER SKIP DASH LINE	1758+05.90	43.0	LT	-	1761+79.62	43.0	LT	94	
	NB CENTER SKIP DASH LINE	0+45.28	23.0	LT	-	9+63.40	22.5	LT	230	
	NB CENTER SKIP DASH LINE	34+18.04	22.5	LT	-	39+18.66	22.5	LT	125	
	NB CENTER SKIP DASH LINE	40+10.91	23.0	LT	-	44+87.16	22.2	LT	120	
EB IL10/121/WOODLAWN ROAD	EB CENTER SKIP DASH LINE	133+66.35	23.1	RT	-	137+39.66	23.1	RT	93	
	EB CENTER SKIP DASH LINE	138+34.01	23.1	RT	-	143+90.84	17.2	RT	140	
WB IL10/121/WOODLAWN ROAD	WB CENTER SKIP DASH LINE	133+66.35	23.2	LT	-	137+36.34	23.0	LT	93	
	WB CENTER SKIP DASH LINE	138+34.02	22.4	LT	-	143+90.84	17.3	LT	140	
PPS# 6-00477-0000 (HSIP) TOTAL =										1613
STP AND HSIP COMBINED TOTAL =										4741

78003130 PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 6"										
PPS# 6-00625-0000 (STP)										
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT	
SB 155 BUSINESS/LINCOLN PARKWAY	6' SKIP- 2' DASH	14+58.26	33.5	RT	-	16+58.26	33.5	RT	50	
	6' SKIP- 2' DASH	63+72.82	10.5	RT	-	66+02.25	10.5	RT	58	
	6' SKIP- 2' DASH	69+56.39	10.5	RT	-	71+07.82	10.5	RT	38	
NB 155 BUSINESS/LINCOLN PARKWAY	6' SKIP- 2' DASH	21+99.62	11.5	LT	-	24+68.19	11.5	LT	68	
5TH STREET	6' SKIP- 2' DASH	15+20.66	18.0	LT	-	15.82.00	23.8	LT	16	
PPS# 6-00625-0000 (STP) TOTAL =										230

PPS# 6-00477-0000 (HSIP)										
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT	
NB 155 BUSINESS/LINCOLN PARKWAY	WHITE 2' DASH 6' SKIP	1759+40.65	31.0	LT	-	1760+39.59	30.5	LT	24	
	WHITE 2' DASH 6' SKIP	2+21.62	11.0	LT	-	4+18.06	10.5	RT	62	
	WHITE 2' DASH 6' SKIP	2+21.62	11.0	LT	-	4+99.15	11.5	LT	70	
	WHITE 2' DASH 6' SKIP	3+13.33	35.0	LT	-	5+13.29	33.5	LT	52	
	WHITE 2' DASH 6' SKIP	7+66.51	11.0	LT	-	9+63.40	11.5	LT	50	
	WHITE 2' DASH 6' SKIP	42+25.15	11.0	LT	-	44+87.15	10.5	LT	66	
	WHITE 2' DASH 6' SKIP	42+14.44	36.0	LT	-	44+39.89	33.5	LT	58	
SB 155 BUSINESS/LINCOLN PARKWAY	WHITE 2' DASH 6' SKIP	1758+26.31	12.6	RT	-	1759+87.66	13.0	RT	42	
	WHITE 2' DASH 6' SKIP	1759+41.36	11.6	LT	-	1760+46.08	11.0	LT	28	
	WHITE 2' DASH 6' SKIP	34+18.04	10.5	RT	-	37+09.07	10.0	RT	74	
EB IL10/121/WOODLAWN ROAD	WHITE 2' DASH 6' SKIP	34+50.78	33.5	RT	-	36+70.44	35.0	RT	56	
	WHITE 2' DASH 6' SKIP	133+79.82	34.6	RT	-	135+34.84	35.1	RT	40	
	WHITE 2' DASH 6' SKIP	134+61.31	10.4	RT	-	135+71.65	11.1	RT	30	
WB IL10/121/WOODLAWN ROAD	WHITE 2' DASH 6' SKIP	140+82.65	11.7	RT	-	142+75.41	6.3	RT	50	
	WHITE 2' DASH 6' SKIP	134+63.24	11.1	LT	-	135+62.96	11.4	LT	26	
	WHITE 2' DASH 6' SKIP	140+99.45	9.8	LT	-	142+20.62	6.8	LT	32	
	WHITE 2' DASH 6' SKIP	140+76.58	34.4	LT	-	142+49.59	29.2	LT	44	
PPS# 6-00477-0000 (HSIP) TOTAL =										804
STP AND HSIP COMBINED TOTAL =										1034

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
p:\11084EBIDINTEG\Illinois.gov\PI\DOT\Documents\DOT Offices\District 6\Projects\0672EDRAWING\0672ED\0672ED-05-Sch-REVISIONS		CHECKED -	REVISED -
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	*	LOGAN	343	66
CONTRACT NO. 72E02			ILLINOIS FED. AID PROJECT	

78009005 - MODIFIED URETHANE PAVEMENT MARKING - LINE 5"									
PPS# 6-00625-0000 (STP)									
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
SB I-55 BUSINESS/LINCOLN PARKWAY	YELLOW INSIDE EDGE LINE	1747+77.22	12.0	LT	-	1753+50.24	12.0	LT	573
	WHITE OUTSIDE EDGE LINE	1747+18.82	12.0	RT	-	1751+65.87	12.0	RT	447
	WHITE OUTSIDE EDGE LINE	1751+68.19	26.4	RT	-	1754+61.15	12.0	RT	294
	YELLOW INSIDE EDGE LINE	9+63.40	10.5	RT	-	15+20.75	10.5	RT	558
	YELLOW DOUBLE MEDIAN LINE	15+20.75	10.5	RT	-	19+53.96	10.5	RT	880
	WHITE OUTSIDE EDGE LINE	9+63.40	33.5	RT	-	18+65.13	49.0	RT	902
	YELLOW DOUBLE MEDIAN LINE	20+51.01	10.5	RT	-	24+68.19	10.5	RT	834
	YELLOW INSIDE EDGE LINE	24+68.19	10.5	RT	-	34+18.04	10.5	RT	950
	WHITE OUTSIDE EDGE LINE	20+83.00	40.9	RT	-	34+18.04	33.5	RT	1335
	YELLOW INSIDE EDGE LINE	44+87.15	10.5	RT	-	48+16.71	10.5	RT	330
	WHITE OUTSIDE EDGE LINE	44+87.15	33.5	RT	-	48+01.75	33.5	RT	315
	YELLOW INSIDE EDGE LINE	48+28.22	10.5	RT	-	51+84.29	10.5	RT	368
	YELLOW INSIDE EDGE LINE	52+35.47	10.5	RT	-	63+72.32	10.5	RT	1149
	YELLOW DOUBLE MEDIAN LINE	63+72.32	10.5	RT	-	68+13.03	1.5	LT	882
	YELLOW DOUBLE MEDIAN LINE	69+15.86	0.9	RT	-	72+18.35	1.5	LT	620
	YELLOW DOUBLE MEDIAN LINE	73+14.08	9.0	RT	-	74+49.51	9.0	RT	290
	WHITE OUTSIDE EDGE LINE	48+13.30	33.5	RT	-	71+94.63	33.5	RT	2381
	WHITE OUTSIDE EDGE LINE	72+96.63	33.5	RT	-	76+12.80	33.5	RT	316
NB I-55 BUSINESS/LINCOLN PARKWAY	WHITE OUTSIDE EDGE LINE	9+63.40	33.5	LT	-	34+18.05	33.5	LT	2455
	YELLOW INSIDE EDGE LINE	9+63.40	11.5	LT	-	15+20.75	11.5	LT	557
	YELLOW DOUBLE MEDIAN LINE	15+20.75	11.5	LT	-	19+20.04	11.5	LT	877
	YELLOW DOUBLE MEDIAN LINE	20+50.94	7.2	RT	-	24+68.19	11.5	LT	846
	YELLOW INSIDE EDGE LINE	24+68.19	11.5	LT	-	34+18.04	11.5	LT	950
	YELLOW INSIDE EDGE LINE	44+87.15	10.5	LT	-	48+30.30	10.5	LT	343
	WHITE OUTSIDE EDGE LINE	44+87.15	33.5	LT	-	48+45.32	33.5	LT	358
	YELLOW INSIDE EDGE LINE	48+41.85	10.5	LT	-	51+87.71	1.9	RT	349
	YELLOW INSIDE EDGE LINE	52+32.04	1.9	RT	-	63+72.32	10.5	LT	1144
	YELLOW DOUBLE MEDIAN LINE	63+72.32	10.5	LT	-	68+13.06	10.5	LT	910
	YELLOW DOUBLE MEDIAN LINE	69+18.56	10.5	LT	-	72+19.16	5.3	LT	634
	YELLOW DOUBLE MEDIAN LINE	73+14.08	3.0	RT	-	74+49.51	3.0	RT	271
	WHITE OUTSIDE EDGE LINE	48+56.73	33.5	LT	-	68+03.78	93.7	LT	1984
	WHITE OUTSIDE EDGE LINE	69+42.62	33.5	LT	-	79+27.62	43.0	LT	990
WHITE OUTSIDE EDGE LINE	79+91.96	43.0	LT	-	92+14.15	33.5	LT	1235	
STALHUT DRIVE	YELLOW DOUBLE MEDIAN LINE	38+70.37	6.0	RT	-	39+57.67	6.0	RT	196
	YELLOW DOUBLE MEDIAN LINE	38+70.37	6.5	LT	-	39+59.31	1.6	LT	180
5TH STREET	YELLOW DOUBLE CENTER LINE	15+43.44	44.0	RT	-	15+43.65	77.2	RT	66
	WHITE OUTSIDE EDGE LINE	15+58.19	67.2	RT	-	16+15.08	23.2	RT	90
	YELLOW DOUBLE CENTER LINE	15+75.08	5.4	RT	-	16+15.08	5.4	RT	80
	YELLOW DOUBLE CENTER LINE	15+35.07	45.9	LT	-	15+32.17	65.8	LT	40
WHITE OUTSIDE EDGE LINE	15+52.05	66.8	LT	-	16+15.08	23.7	LT	93	
STRINGER AVENUE	YELLOW DOUBLE CENTER LINE	68+58.35	68.1	LT	-	67+97.90	130.5	LT	176
PPS# 6-00625-0000 (STP) TOTAL =									28248
PPS# 6-00477-0000 (HSIP)									
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
SB I55 BUSINESS/LINCOLN PARKWAY	YELLOW INSIDE EDGE LINE	1747+77.22	11.5	LT	-	1753+50.24	11.5	LT	573
	WHITE OUTSIDE EDGE LINE	1747+18.82	11.5	RT	-	1751+65.87	11.5	RT	447
	WHITE OUTSIDE EDGE LINE	1751+68.19	26.4	LT	-	1751+68.19	30.5	LT	294
	YELLOW SINGLE MEDIAN LINE	1758+05.91	11.4	LT	-	1761+46.71	26.5	LT	342
	WHITE OUTSIDE EDGE LINE	1758+05.77	54.6	LT	-	143+90.35	27.6	LT	910
	WHITE OUTSIDE EDGE LINE	6+42.57	33.5	RT	-	9+63.40	33.5	RT	321
	WHITE OUTSIDE EDGE LINE	34+18.04	33.5	RT	-	12+41.03	30.2	LT	552
NB I55 BUSINESS/LINCOLN PARKWAY	YELLOW SINGLE MEDIAN LINE	1758+05.70	19.9	LT	-	1761+46.71	30.5	LT	342
	WHITE OUTSIDE EDGE LINE	1758+05.90	12.3	RT	-	133+66.35	34.6	LT	718
	WHITE OUTSIDE EDGE LINE	6+42.63	33.5	LT	-	9+63.40	33.5	LT	321
	WHITE OUTSIDE EDGE LINE	34+18.05	33.5	LT	-	15+20.40	65.8	LT	636

CONTINUED ON NEXT SHEET

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 6\Projects\0672EDRAWING\DATA\EA0\Sheets\0672E02-sht-05-Sched	PLotted SCALE = 40.0000' / in.	CHECKED -	REVISED -						7706		LOGAN	343	67
Default	PLotted DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT CONTRACT NO. 72E02	

78009005 - MODIFIED URETHANE PAVEMENT MARKING - LINE 5"									
NB & SB I55 BUSINESS/LINCOLN PARKWAY	YELLOW DOUBLE MEDIAN LINE	1761+46.71	30.5	LT	-	1761+68.43	30.5	LT	84
	YELLOW DOUBLE MEDIAN LINE	0+69.29	8.8	RT	-	5+49.53	9.3	LT	1926
	YELLOW DOUBLE MEDIAN LINE	6+48.63	9.1	RT	-	9+63.40	0.5	LT	1261
	YELLOW DOUBLE MEDIAN LINE	34+18.04	0.5	LT	-	38+95.49	11.0	LT	1879
IL10/121/WOODLAWN ROAD	WHITE OUTSIDE EDGE LINE	133+66.34	34.6	RT	-	5+68.02	33.5	LT	865
	YELLOW SINGLE MEDIAN LINE	133+70.62	5.6	RT	-	137+09.59	11.6	LT	342
	YELLOW SINGLE MEDIAN LINE	133+70.62	5.6	RT	-	137+09.61	6.6	LT	342
	YELLOW DOUBLE MEDIAN LINE	137+09.59	10.5	LT	-	137+24.42	8.6	LT	60
	YELLOW DOUBLE MEDIAN LINE	138+52.87	8.8	RT	-	138+86.13	9.2	RT	133
	YELLOW SINGLE MEDIAN LINE	138+86.11	6.6	RT	-	142+20.62	6.8	LT	335
	YELLOW SINGLE MEDIAN LINE	138+86.13	11.7	RT	-	142+20.70	0.7	LT	335
	YELLOW DOUBLE MEDIAN LINE	142+19.97	3.7	RLT	-	143+90.71	6.3	LT	429
	WHITE OUTSIDE EDGE LINE	143+91.31	37.7	RT	-	5+57.68	33.5	LT	1036
5TH STREET 5TH STREET 5TH STREET	WHITE OUTSIDE EDGE LINE	12+41.03	21.5	RT	-	44+87.15	33.5	RT	540
	WHITE OUTSIDE EDGE LINE	15+31.96	77.5	RT	-	44+87.16	33.5	LT	624
	YELLOW DOUBLE CENTER LINE	12+41.03	6.0	LT	-	12+90.28	6.0	LT	100
PPS# 6-00477-0000 (HSIP) TOTAL =									15747
STP AND HSIP COMBINED TOTAL =									43995

78009008 MODIFIED URETHANE PAVEMENT MARKING - LINE 8"									
PPS# 6-00625-0000 (STP)									
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
SB I-55 BUSINESS/LINCOLN PARKWAY	WHITE CORNER ISLAND LINE	18+77.70	33.5	RT	-	19+48.79	33.5	RT	71
	WHITE CORNER ISLAND LINE	18+77.70	33.5	RT	-	19+47.64	89.1	RT	90
	WHITE CORNER ISLAND LINE	19+48.79	33.5	RT	-	19+47.64	89.1	RT	56
	WHITE CORNER ISLAND LINE	19+87.80	33.5	RT	-	20+42.03	33.5	RT	54
	WHITE CORNER ISLAND LINE	19+87.80	33.5	RT	-	19+86.93	78.4	RT	45
	WHITE CORNER ISLAND LINE	19+86.93	78.4	RT	-	20+42.03	33.5	RT	72
STRINGER AVENUE	WHITE CORNER ISLAND LINE	67+97.44	33.5	LT	-	68+57.00	33.5	LT	60
	WHITE CORNER ISLAND LINE	67+97.44	33.5	LT	-	68+21.26	91.3	LT	63
	WHITE CORNER ISLAND LINE	68+21.26	91.3	LT	-	68+57.00	33.5	LT	72
PPS# 6-00625-0000 (STP) TOTAL =									583
PPS# 6-00477-0000 (HSIP)									
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
SB I55 BUSINESS/LINCOLN PARKWAY	SOLID WHITE	1761+33.17	13.0	RT	-	1761+78.66	13.0	RT	46
	SOLID WHITE	1761+84.66	13.0	RT	-	1761+94.66	13.0	RT	10
	SOLID WHITE	1761+33.17	13.0	RT	-	1761+69.61	34.5	RT	43
	SOLID WHITE	1761+73.99	38.6	RT	-	1761+93.73	65.7	RT	34
	SOLID WHITE	0+35.05	34.0	RT	-	0+90.83	34.0	RT	56
	SOLID WHITE	0+34.05	93.2	RT	-	0+90.83	34.0	RT	84
	SOLID WHITE	38+72.04	35.0	RT	-	39+40.19	35.0	RT	68
	SOLID WHITE	38+72.04	35.0	RT	-	39+40.17	91.3	RT	91
	SOLID WHITE	39+81.98	35.0	RT	-	40+43.61	35.0	RT	62
	SOLID WHITE	39+82.01	86.2	RT	-	40+43.61	35.0	RT	82
NB I55 BUSINESS/LINCOLN PARKWAY	SOLID WHITE	1761+44.45	55.0	LT	-	1761+79.62	55.0	LT	35
	SOLID WHITE	1761+85.83	55.0	LT	-	1761+95.91	55.0	LT	10
	SOLID WHITE	1761+44.45	55.0	LT	-	1761+72.64	74.8	LT	35
	SOLID WHITE	1761+76.78	79.1	LT	-	1761+96.91	112.6	LT	39
	SOLID WHITE	1762+29.37	56.0	LT	-	1762+29.37	55.9	LT	58
	SOLID WHITE	1762+29.37	106.3	LT	-	1762+29.37	55.9	LT	78
	SOLID WHITE	39+88.22	36.0	LT	-	40+20.73	36.0	LT	33
	SOLID WHITE	39+88.23	68.3	LT	-	40+43.61	35.0	RT	47
EB IL 10/121/WOODLAWN ROAD	SOLID WHITE	136+94.02	35.1	RT	-	137+53.22	35.1	RT	59
	SOLID WHITE	138+22.23	35.1	RT	-	138+72.83	35.1	RT	51
WB IL 10/121/WOODLAWN ROAD	SOLID WHITE	136+99.38	34.5	LT	-	137+52.05	34.5	LT	53
	SOLID WHITE	138+20.06	34.4	LT	-	138+77.66	34.4	LT	58
EB 5TH STREET	SOLID WHITE	12+78.03	17.8	RT	-	13+29.19	17.8	RT	51
	SOLID WHITE	14+00.19	24.0	RT	-	14+32.50	24.0	RT	32
WB 5TH STREET	SOLID WHITE	12+72.92	24.0	LT	-	13+29.17	24.0	LT	56
PPS #6-00477-0000 (HSIP) TOTAL =									1271
STP AND HSIP COMBINED TOTAL =									1854

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\IL\084EBID\INTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 6\Projects\0672ED\Drawings\EA0\Sheets\0672E02-sht-05-Sched	PLotted SCALE = 40.0000' / in.	CHECKED -	REVISED -						7706	*	LOGAN	343	68
Default	DATE = 10/21/2016	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT CONTRACT NO. 72E02	

78009006 MODIFIED URETHANE PAVEMENT MARKINGS - LINE 6"									
PPS# 6-00625-0000 (STP)									
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
SB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE LINE	16+58.26	33.5	RT	-	18+77.70	33.5	RT	220
	WHITE TURN LANE LINE	66+02.25	10.5	RT	-	68+17.25	10.5	RT	215
	WHITE TURN LANE LINE	71+07.82	10.5	RT	-	72+22.82	10.5	RT	115
NB I-55 BUSINESS/LINCOLN PARKWAY	WHITE TURN LANE LINE	20+14.13	3.2	LT	-	21+99.62	11.5	LT	186
	WHITE TURN LANE LINE	20+13.95	11.5	LT	-	21+99.62	11.5	LT	199
	WHITE TURN LANE LINE	73+14.08	9.0	LT	-	74+49.51	9.0	LT	136
5TH STREET		15+75.08	6.0	LT	-	16+15.08	6.0	LT	40
PPS# 6-00625-0000 (STP) TOTAL =									1111
PPS# 6-00477-0000 (HSIP)									
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
NB I55 BUSINESS/LINCOLN PARKWAY	SOLID WHITE	1758+05.82	31.0	LT	-	1759+40.65	31.0	LT	135
	SOLID WHITE	0+42.83	3.3	LT	-	2+21.62	11.0	LT	191
	SOLID WHITE	0+42.66	11.0	LT	-	2+21.62	11.0	LT	179
	SOLID WHITE	0+94.37	35.0	LT	-	3+13.33	35.0	LT	219
	SOLID WHITE	6+26.94	2.5	LT	-	3+13.33	35.0	LT	154
	SOLID WHITE	6+26.68	11.0	LT	-	5+99.64	48.2	LT	140
	SOLID WHITE	40+08.62	1.5	LT	-	42+25.15	11.0	RT	232
	SOLID WHITE	40+08.42	11.0	LT	-	42+25.15	11.0	RT	217
SB I55 BUSINESS/LINCOLN PARKWAY	SOLID WHITE	40+20.73	36.0	LT	-	42+14.44	36.0	LT	194
	SOLID WHITE	1759+87.66	13.0	RT	-	1761+33.17	13.0	RT	146
	SOLID WHITE	1760+46.08	11.0	LT	-	1761+89.62	17.5	LT	148
	SOLID WHITE	1760+46.08	11.0	LT	-	1761+89.77	11.0	LT	138
	SOLID WHITE	1761+56.27	46.7	RT	-	1761+77.37	27.3	RT	29
	SOLID WHITE	1761+60.71	50.8	RT	-	1761+81.43	31.8	RT	29
	SOLID WHITE	4+18.06	10.5	LT	-	5+79.82	3.0	RT	174
	SOLID WHITE	4+18.06	10.5	LT	-	5+79.96	10.5	RT	162
	SOLID WHITE	37+09.07	10.0	RT	-	39+18.56	1.0	RT	224
	SOLID WHITE	37+09.07	10.0	RT	-	39+18.75	10.0	RT	210
NB & SB I55 BUSINESS/LINCOLN PARKWAY	SOLID WHITE	36+70.44	35.0	RT	-	38+72.04	35.0	RT	202
	SOLID WHITE	1761+78.55	18.9	RT	-	1761+79.91	50.1	LT	79
WB IL 10/121/WOODLAWN ROAD	SOLID WHITE	1761+84.56	19.0	RT	-	1761+85.91	60.0	LT	79
	SOLID WHITE	133+66.35	11.2	LT	-	134+63.24	11.1	LT	97
	SOLID WHITE	138+28.47	3.5	LT	-	140+99.45	9.8	LT	282
	SOLID WHITE	138+28.39	10.4	LT	-	140+99.45	9.8	LT	271
	SOLID WHITE	138+77.66	34.4	LT	-	140+76.58	34.4	LT	199
	SOLID WHITE	138+52.01	70.9	LT	-	138+32.91	51.3	LT	28
	SOLID WHITE	138+37.21	47.1	LT	-	138+56.45	66.9	LT	28
	SOLID WHITE	135+34.84	35.1	RT	-	136+94.02	35.1	RT	160
	SOLID WHITE	135+73.27	11.0	RT	-	137+45.09	11.0	RT	172
	SOLID WHITE	135+73.27	11.0	RT	-	137+44.92	3.3	RT	184
EB 5TH STREET	SOLID WHITE	142+75.41	6.3	RT	-	143+90.84	6.3	RT	116
WB 5TH STREET	SOLID WHITE	12+41.03	6.0	RT	-	13+16.16	6.0	RT	75
	SOLID WHITE	14+19.18	6.0	LT	-	15+20.66	6.0	LT	102
	SOLID WHITE	14+19.18	18.0	RT	-	15+20.66	18.0	LT	102
PPS #6-00477-0000 (HSIP) TOTAL =									5097
STP AND HSIP COMBINED TOTAL =									6208

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 6\Projects\0672\Drawings\EA0\Sheets\0672E02-sht-05-Sched	DESIGNED -	REVISED -	7706						•	LOGAN	343	69	
Default	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT CONTRACT NO. 72E02	
	PLOT DATE = 10/21/2016	DATE -	REVISED -	(22)RS-6, MED & (23) RS-5									

78009012 MODIFIED URETHANE PAVEMENT MARKING - LINE 12"									
PPS# 6-00625-0000 (STP)									
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
NB & SB I-55 BUSINESS/LINCOLN PARKWAY	YELLOW DIAGONALS CENTER MEDIAN	15+20.75	0.0	RT	-	19+52.07	0.0	RT	392
	YELLOW DIAGONALS CENTER MEDIAN	20+50.43	8.8	RT	-	24+68.18	0.0	RT	207
	YELLOW DIAGONALS CENTER MEDIAN	51+80.04	0.0	RT		51+88.93	0.0	RT	19
	YELLOW DIAGONALS CENTER MEDIAN	52+30.82	0.0	RT		52+39.71	0.0	RT	19
	YELLOW DIAGONALS CENTER MEDIAN	63+72.32	0.0	RT	-	68+16.44	6.0	LT	296
SB I-55 BUSINESS/LINCOLN PARKWAY	YELLOW DIAGONALS CENTER MEDIAN	69+16.57	0.0	RT	-	72+19.24	3.3	LT	223
	WHITE DIAGONALS ISLAND	18+77.70	33.5	RT	-	19+48.79	33.5	RT	88
	WHITE DIAGONALS ISLAND	19+87.80	33.5	RT	-	20+42.03	33.5	RT	60
NB I-55 BUSINESS/LINCOLN PARKWAY	YELLOW DIAGONALS CENTER MEDIAN	73+11.66	6.0	RT	-	74+49.51	9.0	RT	29
	WHITE CHEVRONS TURN LANE MEDIAN	20+09.80	7.3	LT	-	21+99.62	11.5	LT	44
STALHUT DRIVE	WHITE DIAGONALS ISLAND	67+97.44	33.5	LT	-	68+57.00	33.5	LT	79
	YELLOW DIAGONALS CENTER MEDIAN	19+67.94	129.7	RT	-	16+69.69	38.9	RT	86
PPS# 6-00625-0000 (STP) TOTAL =									1541
PPS# 6-00477-0000 (HSIP)									
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	FOOT
SB I55 BUSINESS/LINCOLN PARKWAY	LEFT TURN LANE CHEVRONS	1760+59.20	11.3	LT	-	1761+86.45	14.2	LT	35
	LEFT TURN LANE CHEVRONS	4+31.17	10.2	RT	-	5+76.01	6.5	RT	44
	CENTER MEDIAN DIAGONALS	0+83.26	8.5	RT	-	5+46.36	9.2	LT	112
	NW CORNER ISLAND	1761+33.17	13.0	RT	-	1761+94.66	13.0	RT	61
	SW CORNER ISLAND	0+35.05	34.0	RT	-	0+90.83	34.0	RT	61
	LEFT TURN LANE CHEVRONS	37+09.07	10.0	RT	-	39+23.25	5.4	RT	70
	NW CORNER ISLAND	38+72.04	35.0	RT	-	39+40.19	35.0	RT	71
	SW CORNER ISLAND	39+81.98	35.0	RT	-	40+43.51	35.0	RT	63
NB I55 BUSINESS/LINCOLN PARKWAY	LEFT TURN LANE CHEVRONS	0+46.66	7.5	LT	-	2+06.66	10.7	LT	53
	LEFT TURN LANE CHEVRONS	6+31.26	6.9	LT	-	7+54.78	10.6	LT	46
	NE CORNER ISLAND	1761+44.45	55.0	LT	-	1761+95.91	55.0	LT	54
	SE CORNER ISLAND	0+36.20	35.0	LT	-	0+94.37	35.0	LT	57
	LEFT TURN LANE CHEVRONS	40+03.67	6.1	LT	-	42+25.15	11.0	LT	73
SB & NB I55 BUSINESS/LINCOLN PARKWAY	SE CORNER ISLAND	39+88.22	36.0	LT	-	40+20.73	36.0	LT	36
	CENTER MEDIAN DIAGONALS	1761+46.71	28.5	LT	-	1761+67.35	29.0	LT	6
	CENTER MEDIAN DIAGONALS	6+64.73	8.6	RT	-	9+39.51	0.5	RT	151
	CENTER MEDIAN DIAGONALS	34+18.04	0.5	LT	-	38+79.15	10.4	LT	219
EB IL 10/121/WOODLAWN ROAD	CENTER MEDIAN DIAGONALS	40+58.21	9.4	RT	-	44+87.15	0.0	LT	195
	LEFT TURN LANE CHEVRONS	135+92.72	10.6	RT	-	137+41.18	7.3	RT	48
WB IL 10/121/WOODLAWN ROAD	LEFT TURN LANE CHEVRONS	138+31.89	7.0	LT	-	140+86.30	9.8	LT	72
EB & WB IL 10/121/WOODLAWN ROAD	CENTER MEDIAN DIAGONALS	137+09.59	9.1	LT	-	137+23.34	8.7	LT	5
	CENTER MEDIAN DIAGONALS	138+53.95	8.9	RT	-	138+86.13	9.2	RT	16
	CENTER MEDIAN DIAGONALS	142+19.97	3.7	LT	-	142+63.93	5.7	LT	10
PPS #6-00477-0000 (HSIP) TOTAL =									1557
STP AND HSIP COMBINED TOTAL =									3098

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 6\Projects\0672E02\Drawings\0672E02-sht-05-Schedule of Quantities.dwg	PLotted	CHECKED -	REVISED -						7706		LOGAN	343	70
Default	PLOT SCALE = 40.0000' / in.	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT CONTRACT NO. 72E02	

78100100 RAISED REFLECTIVE PAVEMENT MARKER						
PPS# 6-00625-0000 (STP)						
ROADWAY	STATION	SIDE	TO	STATION	SIDE	EACH
NB I55 BUSINESS/LINCOLN PARKWAY	9+63.40	LT	-	34+18.06	LT	68.0
NB I55 BUSINESS/LINCOLN PARKWAY	44+87.16	LT	-	74+50.00	LT	75.0
NB I55 BUSINESS/LINCOLN PARKWAY	80+23.13	LT	-	92+14.15	LT	15.0
SB I55 BUSINESS/LINCOLN PARKWAY	1747+18.82	RT	-	1754+16.00	RT	12.0
SB I55 BUSINESS/LINCOLN PARKWAY	9+63.40	RT	-	34+18.06	RT	61.0
SB I55 BUSINESS/LINCOLN PARKWAY	44+87.16	RT	-	74+50.00	RT	72.0
PPS# 6-00625-0000 (STP) TOTAL =						303
PPS# 6-00477-0000 (HSIP)						
ROADWAY	STATION	LANES	TO	STATION	LANES	EACH
SB I55 BUSINESS/LINCOLN PARKWAY	1747+18.82	RT		1754+61.15	LT	11.0
	1758+04.88	RT		1761+74.65	RT	21.0
	0+35.41	RT		9+63.40	RT	43.0
	34+18.06	RT		39+18.66	RT	42.0
	40+08.42	RT		44+87.16	RT	18.0
NB I55 BUSINESS/LINCOLN PARKWAY	1758+05.90	LT		1761+79.62	LT	11.0
	0+45.28	LT		9+63.40	LT	60.0
	34+18.06	LT		39+18.66	LT	19.0
	40+08.42	LT		44+87.16	LT	37.0
WB IL 10/121/WOODLAWN ROAD	133+66.35	LT		137+36.34	LT	9.0
	138+34.02	LT		143+90.84	LT	38.0
EB IL 10/121/WOODLAWN ROAD	133+66.35	RT		137+39.66	RT	22.0
	138+34.01	RT		143+90.84	RT	20.0
5TH STREET	12+41.08	EB		13+30.69	EB	4.0
	14+39.99	EB		15+20.66	EB	8.0
	12+41.03	WB		12+90.28	WB	4.0
	14+19.18	WB		15+20.66	WB	6.0
PPS# 6-0047-0000 (HSIP) TOTAL =						373
STP AND HSIP COMBINED TOTAL =						676

78200020 - CURB REFLECTORS					
PPS# 6-00625-0000 (STP)					
ROADWAY	STATION	TO	STATION	LOCATION	EACH
I55 BUSINESS/LINCOLN PARKWAY	9+63.40	-	15+20.75	CENTER MEDIAN	36.0
	24+68.19	-	34+18.04	CENTER MEDIAN	56.0
	44+87.15	-	48+22.74	CENTER MEDIAN	24.0
	48+43.63	-	51+80.04	CENTER MEDIAN	24.0
	52+39.71	-	63+72.32	CENTER MEDIAN	64.0
STALHUT DRIVE	39+39.96	-	39+57.10	SW CORNER ISLAND	11.0
	39+40.19	-	39+55.23	NW CORNER ISLAND	12.0
STRINGER AVENUE	21+32.61	-	21+71.65	CORNER ISLAND	15.0
PPS# 6-00625-0000 (STP) TOTAL =					242
PPS# 6-00477-0000					
ROADWAY	STATION	TO	STATION	LOCATION	EACH
I55 BUSINESS/LINCOLN PARKWAY	1758+06.93	-	1761+32.31	NORTH CENTER MEDIAN	32
	1761+73.27	-	1761+93.90	NW MEDIAN ISLAND	9
	1761+75.23	-	1761+95.78	NE CORNER ISLAND	10
	0+34.93	-	0+56.40	SW CORNER ISLAND	10
	0+36.50	-	0+57.57	SE CORNER ISLAND	10
	39+11.00	-	39+36.00	NW CORNER ISLAND	12
	39+81.98	-	40+07.10	SW CORNER ISLAND	10
IL 10/121/WOODLAWN ROAD	133+72.25	-	137+09.59	WEST CENTER MEDIAN	33
	138+86.13	-	142+20.19	EAST CENTER MEDIAN	35
PPS# 6-00477-000 (HSIP) TOTAL =					161
STP AND HSIP COMBINED TOTAL =					403

Z0033700 - LONGITUDINAL JOINT SEAL						
PPS# 6-00625-0000 (STP)						
ROADWAY	STATION	TO	STATION	LOCATION	QUANTITY (FOOT)	
S.B. I-55 BUSINESS/LINCOLN PARKWAY	9+63.42	-	34+18.07	S.B. THRU LANES CENTERLINE	2454.7	
	14+58.26	-	18+77.70	S.B. RIGHT TURN LANE LINE	419.4	
	19+55.62	-	20+57.67	S.B. MEDIAN LINE (10.3' RT OFFSET FROM C.L. RDWY.)	102.1	
	19+53.88	-	22+35.31	MEDIAN CENTERLINE (C.L. OF RDWY.)	281.4	
	44+87.18	-	48+09.23	S.B. THRU LANES CENTERLINE	322.0	
	48+20.76	-	74+00.00	S.B. THRU LANES CENTERLINE	2579.2	
	63+72.32	-	73+13.31	S.B. LEFT TURN LANE, OUTSIDE LINE	941.0	
	65+73.51	-	69+16.58	S.B. LEFT TURN LANE LINE (C.L. OF RDWY.)	343.1	
	71+00.00	-	74+00.00	S.B. LEFT TURN LANE LINE (C.L. OF RDWY.)	300.0	
	N.B. I-55 BUSINESS/LINCOLN PARKWAY	9+63.42	-	34+18.07	N.B. THRU LANES CENTERLINE	2454.7
19+20.04		-	24+68.19	N.B. LEFT TURN LANE LINE	548.2	
44+87.18		-	48+38.26	N.B. THRU LANES CENTERLINE	351.1	
48+49.73		-	74+00.00	N.B. THRU LANES CENTERLINE	2550.3	
68+15.09		-	69+16.16	N.B. MEDIAN LANE (10.0' LT OFFSET FROM C.L. RDWY.)	101.1	
72+08.12		-	74+00.00	N.B. LEFT TURN / MEDIAN LINE (9.0' LT OFFSET FROM C.L. RDWY.)	191.9	
PPS #6-00625-0000 (STP) TOTAL =					13940	
PPS# 6-00477-0000 (HSIP)						
ROADWAY	STATION	TO	STATION	LOCATION	QUANTITY (FOOT)	
S.B. I-55 BUSINESS/LINCOLN PARKWAY	1758+05.91	-	1761+94.88	S.B. THRU LANES CENTERLINE	389.0	
	1758+26.31	-	1761+94.64	S.B. RIGHT TURN LANE LINE	368.3	
	1759+41.36	-	1761+95.09	S.B. LEFT TURN LANE LINE	253.7	
	STATION EQUATION: STA. 1762+29.37 (BACK) = STA. 0+00.49 (AHEAD)					
	+35.27	-	9+63.42	S.B. THRU LANES CENTERLINE	928.2	
	+35.47	-	6+49.33	S.B. LEFT TURN LANE LINE (10.4' RT OFFSET FROM C.L. RDWY.)	613.9	
	+35.04	-	2+15.48	S.B. OUTSIDE THRU LANE LINE	180.4	
	34+18.07	-	44+87.18	S.B. THRU LANES CENTERLINE	1069.1	
	34+18.07	-	40+32.54	S.B. LEFT TURN LANE	614.5	
	34+50.78	-	41+44.53	S.B. RIGHT TURN LANE / OUTSIDE THRU LANE LINE	693.8	
N.B. I-55 BUSINESS/LINCOLN PARKWAY	1758+05.91	-	1761+95.67	N.B. THRU LANES CENTERLINE	389.8	
	1758+05.91	-	1760+39.59	N.B. LEFT TURN LANE LINE	233.7	
	1760+58.92	-	1761+95.91	N.B. OUTSIDE THRU LANE LINE	137.0	
	STATION EQUATION: STA. 1762+29.37 (BACK) = STA. 0+00.49 (AHEAD)					
	+36.08	-	9+63.42	N.B. THRU LANES CENTERLINE	927.3	
	+35.67	-	9+63.42	N.B. LEFT TURN LANE LINE (10.5' LT OFFSET FROM C.L. RDWY.)	927.8	
	+35.66	-	2+32.49	ROADWAY CENTERLINE	196.8	
	+36.30	-	5+13.29	N.B. RIGHT TURN LANE LINE	477.0	
	4+04.64	-	7+65.95	ROADWAY CENTERLINE	361.3	
	34+18.07	-	44+87.18	N.B. THRU LANES CENTERLINE	1069.1	
E.B. IL 10/121/WOODLAWN ROAD	38+82.65	-	44+39.89	N.B. RIGHT TURN LANE LINE	557.2	
	38+95.56	-	44+87.18	N.B. INSIDE THRU LANE / LEFT TURN LANE LINE	591.6	
	36+62.24	-	42+72.95	ROADWAY CENTERLINE	610.7	
	133+70.22	-	143+91.13	E.B. THRU LANES CENTERLINE	1020.9	
	133+79.82	-	139+61.51	E.B. RIGHT TURN LANE / OUTSIDE THRU LANE LINE	581.7	
	134+67.41	-	138+55.34	E.B. LEFT TURN LANE / INSIDE THRU LANE LINE	387.9	
	140+82.65	-	143+90.94	E.B. LEFT TURN LANE / INSIDE THRU LANE LINE	308.3	
	W.B. IL 10/121/WOODLAWN ROAD	133+69.40	-	143+90.53	W.B. THRU LANES CENTERLINE	1021.1
		133+69.61	-	135+62.96	W.B. LEFT TURN LANE LINE	193.3
		136+02.73	-	142+49.59	W.B. OUTSIDE THRU LANE / RIGHT TURN LANE LINE	646.9
	137+21.83	-	142+20.62	W.B. INSIDE THRU LANE / LEFT TURN LANE LINE	498.8	
PPS #6-00477-0000 (HSIP) TOTAL =					16249	
STP AND HSIP COMBINED TOTAL =					30189	

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
pw\ill084EBIDINTEG\illinois.gov\PIDOT\Documents\IDOT Offices\District 6\Projects\0672\DRAWING\DATA\EA0\Sheets\0672E02-sht-05-Sch-REVISED		CHECKED -	REVISED -
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	*	LOGAN	343	71
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72E02	

X4401198 - HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH						
I-55 BUSINESS/LINCOLN PARKWAY						
PPS# 6-00625-0000 (STP)						
ROADWAY	LANE DESCRIPTION	DEPTH INCHES	STATION	TO	STATION	SQ YD
NBI-55 BUSINESS/LINCOLN PARKWAY	OUTSIDE SHOULDER	VARIABLE	4+77.42	-	5+63.30	56
	OUTSIDE SHOULDER	VARIABLE	6+32.39	-	35+73.21	2605
	DRIVING	3	9+63.40	-	17+60.00	1019
	PASSING	VARIABLE	9+63.40	-	17+60.00	1017
	DRIVING & PASSING	VARIABLE	17+60.00	-	18+75.00	315
	DRIVING & PASSING	2	18+75.00	-	20+82.79	785
	DRIVING & PASSING	VARIABLE	20+82.79	-	21+75.00	348
	DRIVING	3	21+75.00	-	34+18.05	1565
	PASSING & TURN LANE	VARIABLE	21+75.00	-	24+04.46	577
	PASSING	VARIABLE	24+04.46	-	34+18.05	1315
	DRIVING	3	44+87.16	-	47+00.00	292
	PASSING	VARIABLE	44+87.16	-	47+00.00	281
	OUTSIDE SHOULDER	VARIABLE	45+15.19	-	48+50.72	316
	DRIVING & PASSING	VARIABLE	47+00.00	-	48+00.00	267
	DRIVING & PASSING	2	48+00.00	-	48+33.70	69
	DRIVING & PASSING	2	48+53.44	-	48+90.00	77
	OUTSIDE SHOULDER	VARIABLE	48+62.23	-	67+77.60	1697
	DRIVING & PASSING	VARIABLE	48+90.00	-	49+90.00	267
	DRIVING	3	49+90.00	-	72+56.51	2975
	PASSING	3	49+90.00	-	72+56.51	3069
	OUTSIDE SHOULDER	VARIABLE	69+24.94	-	72+76.03	310
	OUTSIDE SHOULDER	VARIABLE	73+26.33	-	76+90.11	324
	DRIVING & PASSING	VARIABLE	72+56.51	-	73+50.00	353
	SB I-55 BUSINESS/LINCOLN PARKWAY	OUTSIDE SHOULDER	VARIABLE	1+32.21	-	5+72.07
OUTSIDE SHOULDER		VARIABLE	6+29.56	-	17+64.57	877
DRIVING & TURN LANE		2	9+63.40	-	17+60.00	1274
PASSING		VARIABLE	9+63.40	-	17+60.00	1061
DRIVING & PASSING		VARIABLE	17+60.00	-	18+75.00	504
DRIVING & PASSING		2	18+75.00	-	20+82.79	1820
DRIVING & PASSING		VARIABLE	20+82.79	-	21+75.00	348
DRIVING		2	21+75.00	-	34+18.05	1669
PASSING & TURN LANE		VARIABLE	21+75.00	-	24+04.46	563
PASSING		VARIABLE	24+04.46	-	34+18.05	1346
OUTSIDE SHOULDER		VARIABLE	24+95.37	-	33+70.56	788
OUTSIDE SHOULDER		VARIABLE	44+60.29	-	48+01.42	301
DRIVING		2	44+87.16	-	46+50.00	222
PASSING		VARIABLE	44+87.16	-	46+50.00	213
DRIVING & PASSING		VARIABLE	46+50.00	-	47+50.00	267
DRIVING & PASSING		2	47+50.00	-	48+05.11	126
OUTSIDE SHOULDER		VARIABLE	48+07.86	-	72+03.54	2121
DRIVING & PASSING		2	48+24.90	-	48+75.00	113
DRIVING & PASSING		VARIABLE	48+75.00	-	49+75.00	267
DRIVING		2	49+75.00	-	72+56.51	3175
PASSING		VARIABLE	49+75.00	-	72+56.51	2909
DRIVING & PASSING		VARIABLE	72+56.51	-	73+50.00	353
OUTSIDE SHOULDER		VARIABLE	72+86.01	-	76+12.80	283
5TH STREET		ALL LANES	1.5	14+81.96	-	16+05.08
STRINGER AVENUE	ALL LANES	1.5	19+99.72	-	22+56.24	1000
KENWOOD DRIVE	ALL LANES	1.5	71+94.63	-	72+96.53	279
CEMETARY ENTRANCE (EAST SIDE)	ALL	1.5				66
PPS# 6-00625-0000 (STP) TOTAL =						43300

X4401198 - HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH - CONTINUED						
PPS# 6-00477-0000 (HSIP)						
ROADWAY	LANE DESCRIPTION	DEPTH INCHES	STATION	TO	STATION	SQ YD
INTERSECTION I-55 BUSINESS AND IL 10	PASSING & DRIVING	2	1760+98.00	-	1+31.82	5148
	PASSING & DRIVING	2	136+50.00	-	139+50.00	5148
NB I-55 BUS/LINCOLN PARKWAY	PASSING & DRIVING	2	1758+06.00	-	1760+98.00	984
	EX OUTSIDE SHOULDER	2	1758+06.04	-	1760+96.49	250
	EX OUTSIDE SHOULDER	2	1+28.32	-	4+77.42	175
	PASSING & DRIVING	VARIABLE	1+32.14	-	2+20.00	442
	DRIVING	3	2+20.00	-	9+63.40	1253
	PASSING & TURN LANES	VARIABLE	2+20.00	-	9+63.40	1416
	DRIVING	3	34+18.05	-	37+00.00	388
	PASSING	VARIABLE	34+18.05	-	37+00.00	358
	NB OUTSIDE SHOULDER	2	35+73.21	-	38+83.15	322
	PASSING & DRIVING	VARIABLE	37+00.00	-	38+40.70	276
	NB OUTSIDE SHOULDER	2	40+39.40	-	45+15.20	310
	PASSING & DRIVING	VARIABLE	41+60.00	-	42+50.00	480
	DRIVING	3	42+50.00	-	44+87.14	449
	PASSING	VARIABLE	42+50.00	-	44+87.14	413
SB I-55 BUS/LINCOLN PARKWAY	PASSING & DRIVING	2	1758+06.00	-	1760+98.00	1160
	EX OUTSIDE SHOULDER	2	1758+06.04	-	1760+97.78	164
	PASSING & DRIVING	VARIABLE	1+32.14	-	2+20.00	267
	DRIVING	2	2+20.00	-	9+63.40	979
	PASSING & TURN LANES	VARIABLE	2+20.00	-	9+63.40	1431
	DRIVING	2	34+18.05	-	37+00.00	502
	PASSING & TURN LANE	VARIABLE	34+18.05	-	37+00.00	588
	SB OUTSIDE SHOULDER	2	33+70.56	-	38+48.19	328
	PASSING & DRIVING	VARIABLE	37+00.00	-	38+40.70	549
	SB OUTSIDE SHOULDER	2	41+06.35	-	44+60.29	424
	PASSING & DRIVING	VARIABLE	41+60.00	-	42+50.00	241
	DRIVING	2	42+50.00	-	44+87.14	319
	PASSING	VARIABLE	42+50.00	-	44+87.14	314
	EB IL 10/121/WOODLAWN ROAD	PASSING & DRIVING	2	134+19.24	-	136+50.00
PASSING & DRIVING		2	139+50.00	-	143+40.82	1205
WB IL 10/121/WOODLAWN ROAD	PASSING & DRIVING	2	134+19.24	-	136+50.00	748
	PASSING & DRIVING	2	139+50.00	-	143+40.82	1740
5TH STREET INTERSECTION	ALL	2	38+00.00	-	42+50.00	4668
SIDE ROAD (EAST)	ALL	1 1/2	5+57.68	-	6+42.63	288
SIDE ROAD (WEST)	ALL	1 1/2	5+67.86	-	6+42.57	300
PPS# 6-0047-0000 (HSIP) TOTAL =						29940
STP AND HSIP COMBINED TOTAL =						73240

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES			F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG\Illinois.gov\PIWIDOT\Documents\DOT Offices\District 6\Projects\0672\Drawings\0672\0672E02-sht-05-Sched	PLotted Scale = 40.0000' / in.	CHECKED -	REVISED -					7706	*	LOGAN	343	72
Default	PLOT DATE = 10/21/2016	DATE -	REVISED -					SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 72E02	

X0326694 PLUG EXISTING STORM SEWERS										
PPS# 6-00477-0000 (HSIP)										
ROADWAY	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	LENGTH	PIPE AREA	CU YD
155 BUSINESS/LINCOLN PARKWAY	5+64.83	1.3	LT	-	6+39.11	0.8	RT	75.0	0.79	2.2
PPS# 6-00477-0000 (HSIP) TOTAL =										2.2

Z0023500 - FILLING EXISTING CULVERT										
PPS# 6-00625-0000 (STP)										
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	TO	STATION	OFFSET	SIDE	CU YD	
155 BUSINESS/LINCOLN PARKWAY	24" CONC	25+08.15	1.7	RT	-	25+08.15	44.3	RT	5.1	
155 BUSINESS/LINCOLN PARKWAY	24" CMP	47+82.07	45.2	RT	-	48+10.15	2.1	RT	6.3	
155 BUSINESS/LINCOLN PARKWAY	24" CMP	48+13.56	3.2	LT	-	48+41.94	46.7	LT	6.3	
155 BUSINESS/LINCOLN PARKWAY	30" CMP	65+83.65	92.5	LT	-	66+10.62	60.2	LT	8.0	
155 BUSINESS/LINCOLN PARKWAY	24" RCP	66+22.97	45.0	LT	-	66+96.81	43.9	LT	13.7	
PPS# 6-00625-0000 (STP) TOTAL =										39.4

X0327371 - PLUG EXISTING PIPE					
PPS# 6-00625-0000 (STP)					
ROADWAY	DESCRIPTION	STATION	OFFSET	SIDE	CU YD
155 BUSINESS/LINCOLN PARKWAY	12" STORM SEWER	6+64.04	1.6	RT	0.3
	12" STORM SEWER	6+77.70	1.8	RT	0.3
	12" RCP	16+80.53	2.2	LT	0.3
	15" CONC	31+65.16	0.6	RT	0.4
	15" CONC	31+77.20	0.8	RT	0.4
	15" CONC	50+33.35	0.1	LT	0.4
	15" CONC	50+46.51	0.4	RT	0.4
	15" CONC	54+68.10	0.1	RT	0.4
	15" CONC	62+14.54	0.1	RT	0.4
	15" CONC	62+26.99	0.1	LT	0.4
PPS# 6-00625-0000 (STP) TOTAL =					3.7

X6015010 - REMOVE AND REPLACE CONCRETE HEADWALLS FOR PIPE DRAINS				
PPS# 6-00625-0000 (STP)				
ROADWAY	STATION	OFFSET	SIDE	EACH
155 BUSINESS/LINCOLN PARKWAY	31+77.18	103.3	LT	1.0
	32+20.37	102.1	LT	1.0
PPS# 6-00625-0000 (STP) TOTAL =				2.0

X4402020 CONCRETE MEDIAN SURFACE REMOVAL						
PPS # 6-00477-0000 (HSIP)						
ROADWAY	STATION	SIDE	TO	STATION	SIDE	SQ FT
155 BUSINESS/LINCOLN PARKWAY	1760+35.98	CENTER	-	1761+64.77	CENTER	631
	0+77.66	CENTER	-	5+72.29	CENTER	3521
	6+25.95	CENTER	-	8+25.26	CENTER	1526
	34+80.22	CENTER	-	39+02.23	CENTER	4435
	40+30.40	CENTER	-	44+48.94	CENTER	4213
IL 10/121/WOODLAWN ROAD	135+71.11	CENTER	-	137+08.62	CENTER	942
PPS# 6-00477-0000 (HSIP) TOTAL =						15268

SIGNING SCHEDULE POSTVILLE DRIVE (FRONTAGE ROAD)							
PPS# 6-00625-0000 (STP)							
STA	O/S	Direction Sign Facing	Existing Sign	REMOVE SIGN PANEL TYPE 1, SQ FT	RELOCATE SIGN PANEL TYPE 1, SQ FT	Mounting	TELESCOPING STEEL SIGN SUPPORT
10+80	14 FT LT	South	R1-2 36x36 Yield	5	5	GROUND MOUNT - TELES PAR	14
12+51	19 FT RT	North	R2-1 (35) 24x36 35 MPH	6	6	GROUND MOUNT - TELES PAR	14
24+45	19 FT RT	North	R2-1 (35) 24x36 35 MPH	6	6	GROUND MOUNT - TELES PAR	14
PPS# 6-00625-0000 (STP) TOTAL =				17	17		42

SIGNING SCHEDULE IL 10/121/WOODLAWN DRIVE										
PPS# 6-00625-0000 (STP - SIDEWALK)										
STA	O/S	DIRECTION SIGN FACING	EXISTING SIGN	REMOVE SIGN PANEL ASSEMBLY TYPE A, EACH	REMOVE SIGN PANEL ASSEMBLY TYPE B, EACH	RELOCATE SIGN PANEL ASSEMBLY TYPE A, EACH	RELOCATE SIGN PANEL ASSEMBLY TYPE B, EACH	MOUNTING	TELESCOPING STEEL SIGN SUPPORT, FT	BASE FOR TELESCOPING STEEL SIGN SUPPORT, EACH
141+01	50 FT LT	WEST	SPECIAL GREEN/WHITE 48X12 "A MASON CITY"							
141+01	50 FT LT	WEST	SPECIAL GREEN/WHITE 48X12 "A PEORIA"							
141+01	50 FT LT	WEST	SPECIAL GREEN/WHITE 48X18 "C- CORRECTIONAL CENTER"							
141+01	50 FT LT	WEST	M4-5 (BL) 24X12 TO							
141+01	50 FT LT	WEST	M1-1 (55) 24X24 INTERSTATE 55 SHIELD							
141+01	50 FT LT	WEST	M1-1 (155) 30X24 INTERSTATE 155 SHIELD		1		1	GROUND MOUNT - TELES PAR	28	
141+01	50 FT LT	WEST	M6-3 (BL) 21X15 UP ARROW							
141+01	50 FT LT	WEST	M3-1 24X12 NORTH							
141+01	50 FT LT	WEST	M1-1100 (121) 30X24 IL 121							
141+01	50 FT LT	WEST	M6-3 21X15 UP ARROW							
PPS# 6-00625-0000 (STP) TOTAL =				0	1	0	1		28	0
PPS # 6-00477-0000 (HSIP)										
STA	O/S	DIRECTION SIGN FACING	EXISTING SIGN	REMOVE SIGN PANEL ASSEMBLY TYPE A, EACH	REMOVE SIGN PANEL ASSEMBLY TYPE B, EACH	RELOCATE SIGN PANEL ASSEMBLY TYPE A, EACH	RELOCATE SIGN PANEL ASSEMBLY TYPE B, EACH	MOUNTING	TELESCOPING STEEL SIGN SUPPORT, FT	BASE FOR TELESCOPING STEEL SIGN SUPPORT, EACH
137+27	5 FT LT	EAST	R4-7 24X30 KEEP RIGHT	1		1		SURFACE MOUNT - TELES PAR	14	1
137+27	5 FT LT	EAST	OM 1-3 18X18 OBJECT MARKER							
138+49	5 FT RT	WEST	R4-7 24X30 KEEP RIGHT	1		1		SURFACE MOUNT - TELES PAR	14	1
138+49	5 FT RT	WEST	OM 1-3 18X18 OBJECT MARKER							
140+36	5 FT RT	NORTH	R6-2 (LEFT) 30X36 ONE WAY	1				REMOVAL ONLY		
141+24	4 FT RT	EAST	R3-1100 24X24 LEFT TURN LANE							
141+24	4 FT RT	EAST	M6-2 (LEFT) 21X15 ANGLE LEFT ARROW							
141+24	4 FT RT	EAST	D9-2 24X24 HOSPITAL							
141+24	4 FT RT	EAST	M6-2 (LEFT) (BL) 21X15 ANGLE LEFT ARROW		1		1	SURFACE MOUNT - TELES PAR	14	1
141+24	4 FT RT	WEST	R3-1100 24X24 LEFT TURN LANE							
141+24	4 FT RT	WEST	M6-2 (LEFT) 21X15 ANGLE LEFT ARROW							
PPS# 6-00477-0000 (HSIP) TOTAL =				3	1	2	1		42	3

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIDOT\Documents\IDOT Offices\District 6\Projects\0672EDRAWING\0672ED\Drawings\0672E02-sht-05-Sch		CHECKED -	REVISED -		7706	*	LOGAN	343	73				
Default	PLOT SCALE = 40.0000' / in.	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	CONTRACT NO. 72E02	

SIGNING SCHEDULE LINCOLN PARKWAY AND I-55 BUSINESS LOOP/LINCOLN PARKWAY

PPS# 6-00625-0000 (STP)

STA	O/S	DIRECTION SIGN FACING	EXISTING SIGN	REMOVE SIGN PANEL TYPE 1, SQ FT	REMOVE SIGN PANEL TYPE 2, SQ FT	REMOVE SIGN PANEL ASSEMBLY TYPE A, EACH	REMOVE SIGN PANEL ASSEMBLY TYPE B, EACH	RELOCATE SIGN PANEL TYPE 1, SQ FT	RELOCATE SIGN PANEL TYPE 2, SQ FT	RELOCATE SIGN PANEL ASSEMBLY TYPE A, EACH	RELOCATE SIGN PANEL ASSEMBLY TYPE B, EACH	Mounting	TELESCOPING STEEL SIGN SUPPORT, FT	BASE FOR TELESCOPING STEEL SIGN SUPPORT, EACH
2+34	48 FT RT	NORTH	XM-21 24X30 HISTORIC IL-US 66	5				5				GROUND MOUNT - TELES PAR	14	
3+70	51 FT LT	SOUTH	SPECIAL GREEN/WHITE 48X12 "<- MASON CITY"											
3+70	51 FT LT	SOUTH	SPECIAL GREEN/WHITE 48X12 "CLINTON ->"											
3+70	51 FT LT	SOUTH	SPECIAL GREEN/WHITE 48X24 "LINCOLN CHRISTIAN UNIVERSITY ->"											
3+70	51 FT LT	SOUTH	M4-5 (BL) 24X12 TO				1				1	GROUND MOUNT - TELES PAR	28	
3+70	51 FT LT	SOUTH	M1-1 (55) 24X24 INTERSTATE 55 SHIELD											
3+70	51 FT LT	SOUTH	M1-1 (155) 30X24 INTERSTATE 155 SHIELD											
3+70	51 FT LT	SOUTH	M6-1 (BL) 21-15 LEFT ARROW											
4+39	45 FT RT	NORTH	R2-1 30X36 45 MPH						14			GROUND MOUNT - TELES PAR	14	
4+39	45 FT RT	SOUTH	R5-1A 36X24 WRONG WAY											
5+30	45 FT RT	NORTH	D4-I100 30X24 FRONTAGE ROAD ENTRANCE											
5+30	45 FT RT	NORTH	M6-4 (GR) 21X15 TWO WAY ARROW				1				1	GROUND MOUNT - TELES PAR	14	
5+30	45 FT RT	SOUTH	R5-1 36X36 DO NOT ENTER											
5+35	65 FT LT	NORTH	R1-2 36X36 YIELD	5				5				GROUND MOUNT - TELES PAR	14	
5+55	45 FT LT	EAST	R1-1 36X36 STOP											
5+55	45 FT LT	EAST	R6-2 (LEFT) 30X36 ONE WAY				1				1	GROUND MOUNT - TELES PAR	14	
5+55	45 FT LT	WEST	R6-2 (LEFT) 30X36 ONE WAY											
6+35	45 FT RT	WEST	R1-1 36X36 STOP											
6+35	45 FT RT	WEST	R6-2 (LEFT) 30X36 ONE WAY				1				1	GROUND MOUNT - TELES PAR	14	
6+35	45 FT RT	EAST	R6-2 (LEFT) 30X36 ONE WAY											
6+50	79 FT RT	NORTH	OM 4-3 18X18 OBJECT MARKER	2				2				GROUND MOUNT - TELES PAR	14	
6+51	84 FT RT	NORTH	R11-I100 36X30 ROAD ENDS	8				8				GROUND MOUNT - TELES PAR	14	
6+52	89 FT RT	NORTH	OM 4-3 18X18 OBJECT MARKER	2				2				GROUND MOUNT - TELES PAR	14	
6+77	42 FT LT	SOUTH	D4-I100 30X24 FRONTAGE ROAD ENTRANCE											
6+77	42 FT LT	SOUTH	M6-4 (GR) 21X15 TWO WAY ARROW				1				1	GROUND MOUNT - TELES PAR	14	
6+77	42 FT LT	NORTH	R5-1 36X36 DO NOT ENTER											
7+69	43 FT LT	NORTH	R5-1A 36X24 WRONG WAY					6				GROUND MOUNT - TELES PAR	14	
15+46	45 FT RT	NORTH	R3-I100A 24X24 RIGHT TURN LANE											
15+46	45 FT RT	NORTH	M6-2 (RIGHT) 21X15 RIGHT ARROW				1				1	GROUND MOUNT - TELES PAR	28	
15+46	45 FT RT	NORTH	D9-2 24X24 HOSPITAL											
15+46	45 FT RT	NORTH	M6-2 (RIGHT) (BL) 21X15 RIGHT ARROW											
15+53	42 FT LT	SOUTH	R2-1 30X36 45 MPH	8				8				GROUND MOUNT - TELES PAR	14	
19+73	42 FT LT	WEST	W1-6 (LEFT) 48X24 LEFT ARROW											
19+73	42 FT LT	WEST	R6-2 (LEFT) 30X36 ONE WAY				1				1	GROUND MOUNT - TELES PAR	14	
19+73	42 FT LT	EAST	W1-7 48X24 DOUBLE ARROW											
24+08	4 FT LT	SOUTH	R3-I100 24X24 LEFT TURN LANE											
24+08	4 FT LT	SOUTH	M6-2 (LEFT) 21X15 ANGLE LEFT ARROW				1				1	SURFACE MOUNT - TELES PAR	14	1
24+08	4 FT LT	SOUTH	D9-2 24X24 HOSPITAL											
24+08	4 FT LT	SOUTH	M6-2 (LEFT) (BL) 21X15 ANGLE LEFT ARROW											
28+96	46 FT RT	NORTH	M2-1 (GR) 21X15 JCT											
28+96	46 FT RT	NORTH	M1-2 (55) 24X24 BUS 55 ROUTE SHIELD			1				1		GROUND MOUNT - TELES PAR	14	
32+74	43 FT RT	NORTH	W3-3 36X36 SIGNAL AHEAD											
32+74	43 FT RT	NORTH	SPECIAL YELLOW/BLACK 24X9 "5TH STREET"				1				1	GROUND MOUNT - TELES PAR	14	
32+74	43 FT RT	NORTH	BUSINESS LOGO SIGN											
34+84	52 FT RT	NORTH	SPECIAL GREEN/WHITE 48X18 "<- POSTVILLE COURTHOUSE STATE HISTORIC SITE"											
34+84	52 FT RT	NORTH	SPECIAL GREEN/WHITE 48X18 "<- COURTHOUSE HISTORIC DISTRICT"											
34+84	52 FT RT	NORTH	SPECIAL GREEN/WHITE 48X18 "<- LOGAN COUNTY HEALTH DEPARTMENT"											
34+84	52 FT RT	NORTH	I-7 24X24 RAIL STATION											
34+84	52 FT RT	NORTH	M6-1 (RIGHT) (GR) 21X15 RIGHT ARROW				1				1	GROUND MOUNT - TELES PAR	28	
34+84	52 FT RT	NORTH	D9-I104 36X18 TOURIST INFORMATION											
34+84	52 FT RT	NORTH	M6-1 (BL) 21-15 LEFT ARROW											
34+84	52 FT RT	NORTH	BUSINESS LOGO SIGN											
35+32	52 FT LT	SOUTH	R2-1 30X36 45 MPH	8				8				GROUND MOUNT - TELES PAR	14	
37+74	49 FT LT	SOUTH	ADOPT A HIGHWAY ASSEMBLY				1				1	GROUND MOUNT - TELES PAR	14	
40+25	57 FT RT	NORTH	M3-3 (GR) (SOUTH) 24X12 SOUTH PLAQUE											
40+25	57 FT RT	NORTH	M1-2 (55) 24X24 BUS 55 ROUTE SHIELD				1				1	GROUND MOUNT - TELES PAR	14	
40+25	57 FT RT	NORTH	XM-21 24X30 HISTORIC IL-US 66											
42+04	52 FT RT	NORTH	ADOPT A HIGHWAY ASSEMBLY				1				1	GROUND MOUNT - TELES PAR	14	
42+13	58 FT LT	SOUTH	SPECIAL GREEN/WHITE 48X18 "<- POSTVILLE COURTHOUSE STATE HISTORIC SITE"											
42+13	58 FT LT	SOUTH	SPECIAL GREEN/WHITE 48X18 "<- COURTHOUSE HISTORIC DISTRICT"											
42+13	58 FT LT	SOUTH	D4-I101 30X18 BUSINESS DISTRICT											
42+13	58 FT LT	SOUTH	M6-1 (GR) (RIGHT) 21X15 RIGHT ARROW				1				1	GROUND MOUNT - TELES PAR	28	
42+13	58 FT LT	SOUTH	D9-I104 36X18 TOURIST INFORMATION (LEFT ARROW)											
42+13	58 FT LT	SOUTH	M6-1 (BL) 21-15 LEFT ARROW											

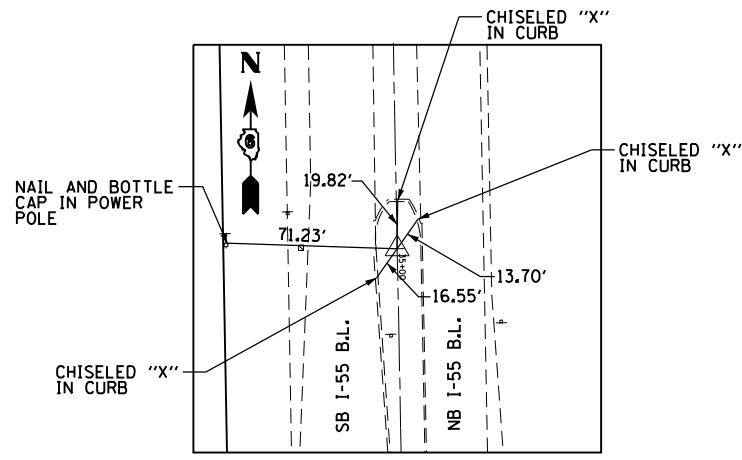
CONTINUED ON NEXT SHEET

FILE NAME =	USER NAME =	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\1\084EBID\INTEG\Illinois.gov\PI\DOT\Documents\DOT Offices\District 6\Projects\0672\DRAWING\0672\02-sht-05-Sc\REVISED -	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -						7706	*	LOGAN	343	74
Default	PLOT DATE = 11/18/2016	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				ILLINOIS FED. AID PROJECT		CONTRACT NO. 72E02		
* (22)RS-6, MED & (23) RS-5													

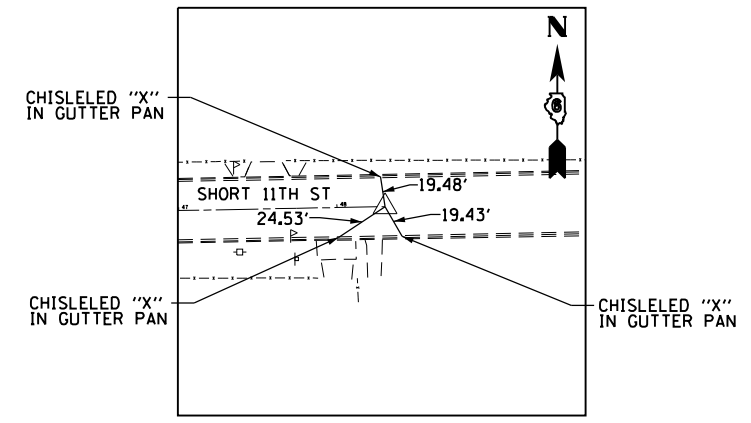
SIGNING SCHEDULE LINCOLN PARKWAY AND I-55 BUSINESS LOOP/LINCOLN PARKWAY														
PPS# 6-00625-0000 (STP)														
STA	O/S	DIRECTION SIGN FACING	EXISTING SIGN	REMOVE SIGN PANEL TYPE 1, SQ FT	REMOVE SIGN PANEL TYPE 2, SQ FT	REMOVE SIGN PANEL ASSEMBLY TYPE A, EACH	REMOVE SIGN PANEL ASSEMBLY TYPE B, EACH	RELOCATE SIGN PANEL TYPE 1, SQ FT	RELOCATE SIGN PANEL TYPE 2, SQ FT	RELOCATE SIGN PANEL ASSEMBLY TYPE A, EACH	RELOCATE SIGN PANEL ASSEMBLY TYPE B, EACH	MOUNTING	TELESCOPING STEEL SIGN SUPPORT, FT	BASE FOR TELESCOPING STEEL SIGN SUPPORT, EACH
43+98	52 FT LT	SOUTH	D9-2 24X24 HOSPITAL											
43+98	52 FT LT	SOUTH	D9-2 24X24 HOSPITAL											
43+98	52 FT LT	SOUTH	M6-3 (BL) 21X15 UP ARROW											
43+98	52 FT LT	SOUTH	I-7 24X24 RAIL STATION				1				1	GROUND MOUNT - TELES PAR	28	
43+98	52 FT LT	SOUTH	M6-1 (GR) (RIGHT) 21X15 RIGHT ARROW											
43+98	52 FT LT	SOUTH	BUSINESS LOGO SIGN											
43+98	52 FT LT	SOUTH	BUSINESS LOGO SIGN											
45+75	51 FT RT	NORTH	R2-1 30X36 45 MPH	8				8				GROUND MOUNT - TELES PAR	14	
45+88	43 FT LT	SOUTH	W3-3 36X36 SIGNAL AHEAD				1				1	GROUND MOUNT - TELES PAR	14	
45+88	43 FT LT	SOUTH	SPECIAL YELLOW/BLACK 24X9 "5TH STREET"											
55+00	4 FT LT	SOUTH	W10-1 36X36 RR CROSSING	9				9				GROUND MOUNT - TELES PAR	14	
55+00	42 FT LT	SOUTH	W10-1 36X36 RR CROSSING	9				9				GROUND MOUNT - TELES PAR	14	
59+36	48 FT LT	SOUTH	R2-1 30X36 45 MPH	8				8				GROUND MOUNT - TELES PAR	14	
67+78	49 FT LT	EAST	R1-1 36X36 STOP	9				9				GROUND MOUNT - TELES PAR	14	
68+76	73 FT LT	NORTH	R4-7 24X30 KEEP RIGHT											
68+76	73 FT LT	NORTH	OM 1-3 18X18 OBJECT MARKER			1						REMOVAL ONLY		
69+93	48 FT LT	NORTH	R5-1 36X36 DO NOT ENTER	9				9				GROUND MOUNT - TELES PAR	14	
70+00	49 FT RT	NORTH	R2-1 30X36 45 MPH	8				8				GROUND MOUNT - TELES PAR	14	
72+08	46 FT LT	SOUTH	SPECIAL 24X24 "LINCOLN POP XXXX"											
72+08	46 FT LT	SOUTH	R2-1 30X36 45 MPH				1				1	GROUND MOUNT - TELES PAR	28	
72+08	46 FT LT	SOUTH	SPECIAL 36X36 "AMTRAK COMMUNITY"											
72+08	46 FT LT	SOUTH	SPECIAL 48X24 "IDOT CHAMPIONSHIP SIGN"											
72+70	88 FT LT	NORTH	R1-1 30X30 STOP	6								REMOVAL ONLY		
76+65	46 FT LT	SOUTH	SPECIAL 48X24 "IDOT CHAMPIONSHIP SIGN"				1				1	GROUND MOUNT - TELES PAR	28	
76+65	46 FT LT	SOUTH	SPECIAL 48X24 "IDOT CHAMPIONSHIP SIGN"											
76+65	46 FT LT	SOUTH	SPECIAL 48X24 "IDOT CHAMPIONSHIP SIGN"											
PPS# 6-00625-0000 (STP) TOTAL =				110	14	2	18	104	14	1	18		588	1

SIGNING SCHEDULE STRINGER AVENUE														
PPS# 6-00477-0000 (HSIP)														
STA	O/S	DIRECTION SIGN FACING	EXISTING SIGN	REMOVE SIGN PANEL TYPE 1, SQ FT	REMOVE SIGN PANEL TYPE 2, SQ FT	REMOVE SIGN PANEL ASSEMBLY TYPE A, EACH	REMOVE SIGN PANEL ASSEMBLY TYPE B, EACH	RELOCATE SIGN PANEL TYPE 1, SQ FT	RELOCATE SIGN PANEL TYPE 2, SQ FT	RELOCATE SIGN PANEL ASSEMBLY TYPE A, EACH	RELOCATE SIGN PANEL ASSEMBLY TYPE B, EACH	Mounting	TELESCOPING STEEL SIGN SUPPORT, FT	BASE FOR TELESCOPING STEEL SIGN SUPPORT, EACH
1761+60	25 FT LT	SOUTH	R4-7 24X30 KEEP RIGHT				1				1	SURFACE MOUNT - TELES PAR	14	1
1761+60	25 FT LT	SOUTH	OM 1-3 18X18 OBJECT MARKER											
0+66	5 FT RT	NORTH	R4-7 24X30 KEEP RIGHT				1				1	SURFACE MOUNT - TELES PAR	14	1
0+66	5 FT RT	NORTH	OM 1-3 18X18 OBJECT MARKER											
2+74	1 FT LT	SOUTH	R3-1100 24X24 LEFT TURN LANE											
2+74	1 FT LT	SOUTH	M6-2 (LEFT) 21X15 ANGLE LEFT ARROW				1				1	SURFACE MOUNT - TELES PAR	14	1
2+74	1 FT LT	NORTH	R3-1100 24X24 LEFT TURN LANE											
2+74	1 FT LT	NORTH	M6-2 (LEFT) 21X15 ANGLE LEFT ARROW											
8+05	1 FT RT	SOUTH	R3-1100 24X24 LEFT TURN LANE				1				1	SURFACE MOUNT - TELES PAR	14	1
8+05	1 FT RT	SOUTH	M6-2 (LEFT) 21X15 ANGLE LEFT ARROW											
35+36	3 FT RT	NORTH	R3-1100 24X24 LEFT TURN LANE				1				1	SURFACE MOUNT - TELES PAR	14	1
35+36	3 FT RT	NORTH	M6-2 (LEFT) 21X15 ANGLE LEFT ARROW											
42+86	4 FT RT	NORTH	W10-1 36X36 RR CROSSING	9				9				GROUND MOUNT - TELES PAR	14	
42+86	48 FT RT	NORTH	W10-1 36X36 RR CROSSING	9				9				GROUND MOUNT - TELES PAR	14	
44+02	5 FT LT	SOUTH	R3-1100 24X24 LEFT TURN LANE				1				1	SURFACE MOUNT - TELES PAR	14	1
44+02	5 FT LT	SOUTH	M6-2 (LEFT) 21X15 ANGLE LEFT ARROW											
PPS# 6-00477-0000 (HSIP) TOTAL =				18	0	6	0	18	0	6	0		112	6
STP AND HSIP COMBINED TOTAL =				128	14	8	18	122	14	7	18			

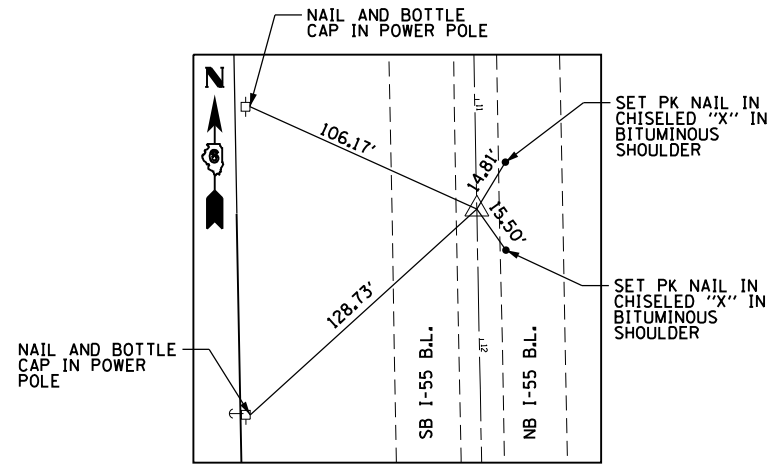
SIGNING SCHEDULE STRINGER AVENUE						
PPS# 6-00625-0000 (STP)						
STA	O/S	DIRECTION SIGN FACING	EXISTING SIGN	REMOVE SIGN PANEL TYPE 1, SQ FT	RELOCATE SIGN PANEL TYPE 1, SQ FT	TELESCOPING STEEL SIGN SUPPORT
19+88	28 FT LT	SOUTH	R1-1 30x30 STOP	1		
PPS# 6-00625-0000 (STP) TOTAL =				1		



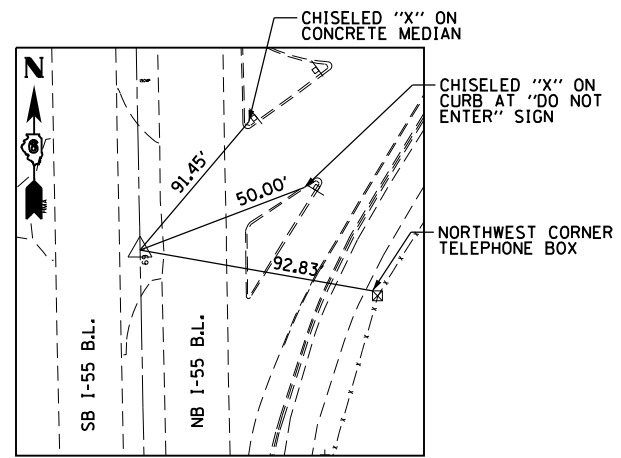
P.I. STA. 35+00.09
I-55 BUSINESS LOOP/LINCOLN PARKWAY
STAR DRILLED IN CHISELED "X"



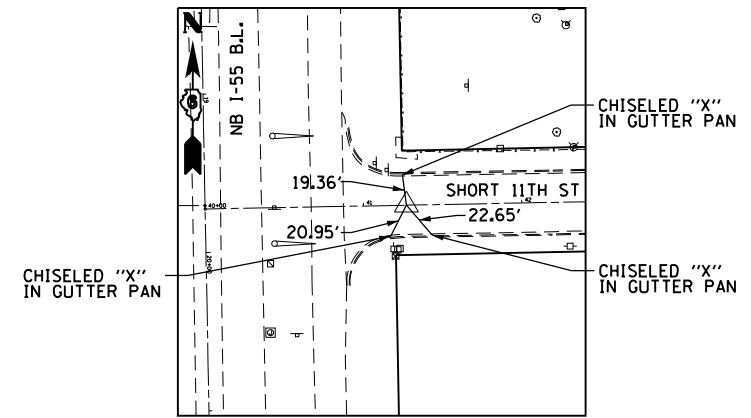
P.O.T. STA. 48+30.33
SHORT 11TH STREET
PK NAIL IN CHISELED "X"



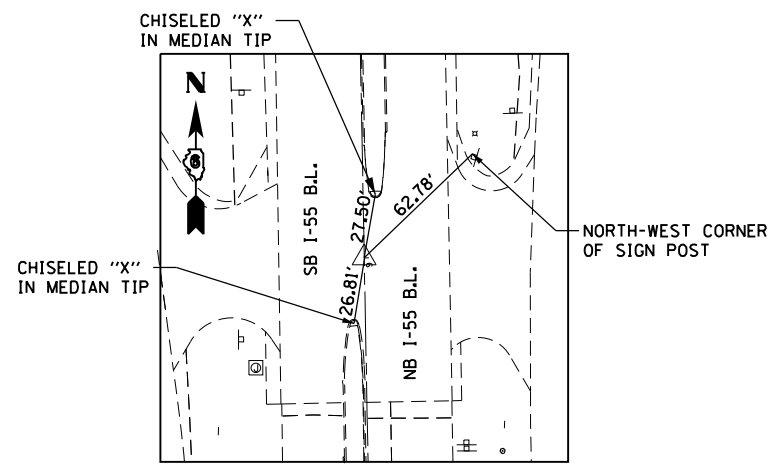
P.O.T. STA. 611+45.70
I-55 BUSINESS LOOP/LINCOLN PARKWAY
*5 REBAR WITH IDOT CAP FLUSH



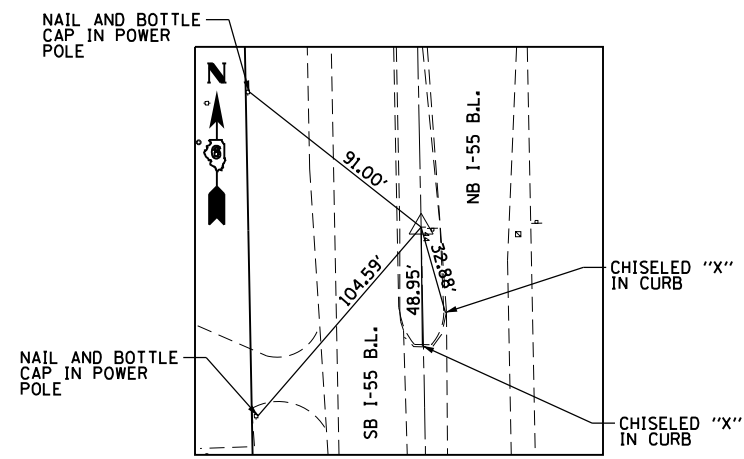
P.I. STA. 69+98.53
I-55 BUSINESS LOOP/LINCOLN PARKWAY
*4 REBAR BURIED 3 INCHES



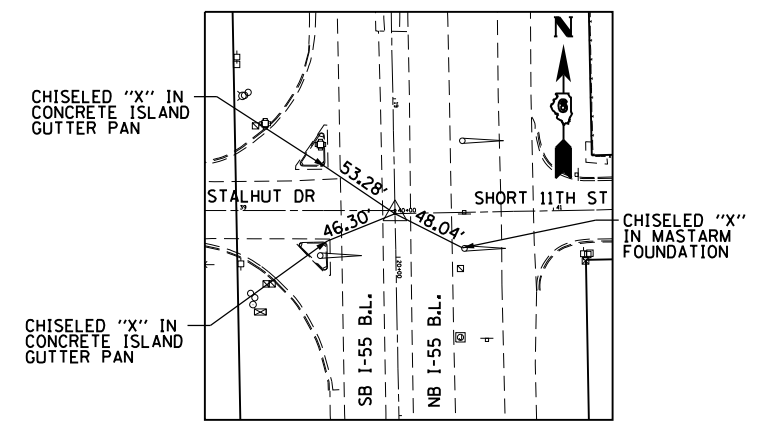
P.O.T. STA. 41+21.14
SHORT 11TH STREET
SET PK NAIL IN CHISELED "X"



P.O.T. STA. 6+00.00
I-55 BUSINESS LOOP/LINCOLN PARKWAY
SET PK NAIL IN CHISELED "X"



P.I. STA. 44+00.00
I-55 BUSINESS LOOP/LINCOLN PARKWAY
STAR DRILL IN CHISELED "X"



P.O.T. STA. 40+00.00
SHORT 11TH STREET
SET PK NAIL IN CHISELED "X"

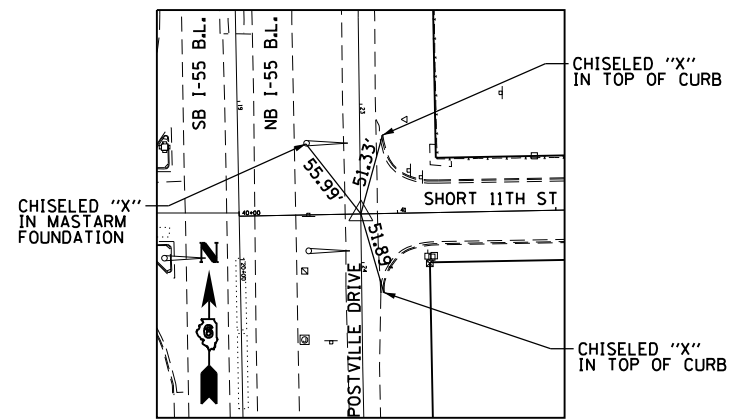
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		DATE - 06/24/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

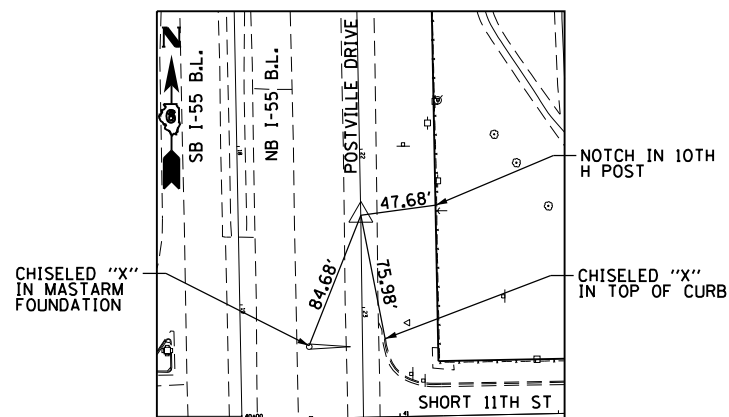
SURVEY TIES AND
BENCHMARKS

SCALE: NO SCALE SHEET NO. OF SHEETS STA. TO STA.

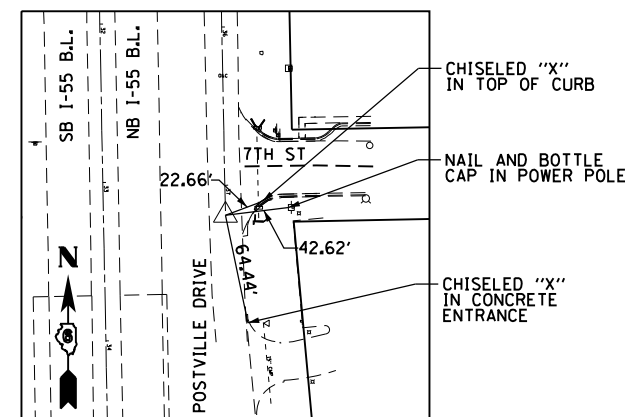
F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	(22)RS-6 & (23)RS-5,N,TS-1	LOGAN	343	76
CONTRACT NO. 72E02				
ILLINOIS FED. AID PROJECT				



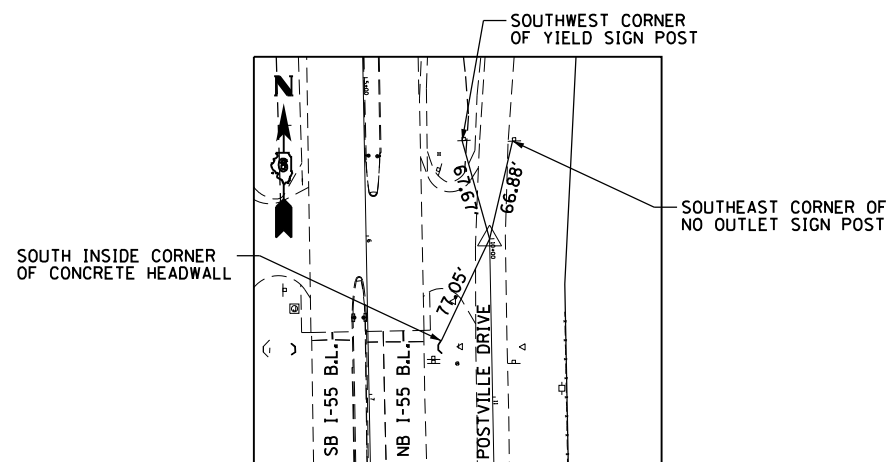
P.O.T. STA. 23+69.28
 POSTVILLE DRIVE
 PK NAIL SET IN CHISELED "X"



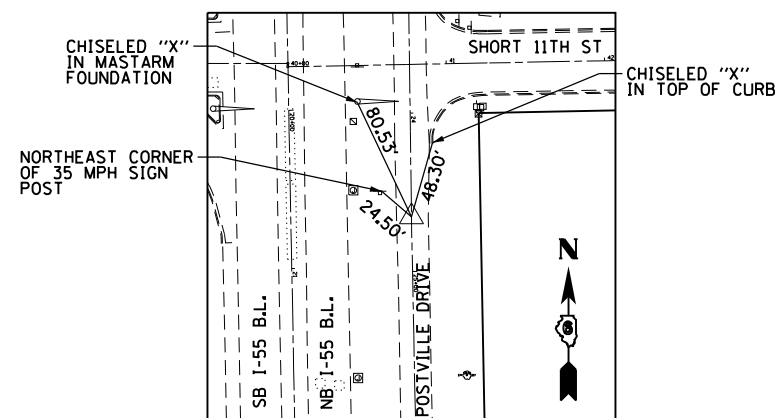
P.I. STA. 22+45.44
 POSTVILLE DRIVE
 PK NAIL IN CHISELED "X"



P.I. STA. 37+18.97
 POSTVILLE DRIVE
 PK NAIL IN CHISELED "X"



P.O.T. STA. 10+00.00
 POSTVILLE DRIVE
 PK NAIL IN CHISELED "X"



P.I. STA. 24+65.41
 POSTVILLE DRIVE
 PK NAIL IN CHISELED "X"

BENCHMARKS	
BENCHMARK 17	CHISELED SQUARE ON EAST HEADWALL OF 2' X 2' BOX CULVERT FOR BUSINESS 55, 0.3 MILE SOUTH OF ILLINOIS 10. STATION 16+80.60 @ 44.10' LT ELEVATION = 581.040
BENCHMARK 18	SPIKE IN LIGHT POLE ON EAST SIDE OF BUSINESS 55, IN FRONT OF CHRISTIAN HOME OFFICE. STATION 24+37.30 @ 118.10' LT ELEVATION = 588.123
BENCHMARK 19	CHISELED SQUARE ON EAST HEADWALL OF 2' X 2' BOX CULVERT FOR BUSINESS 55, 0.65 MILE SOUTH OF ILLINOIS 10. STATION 31+71.10 @ 44.40' LT ELEVATION = 591.451
BENCHMARK 20	RAIL ROAD SPIKE IN POWER POLE EAST OF BUSINESS 55 AND SOUTH OF 5TH STREET ROAD, 0.8 MILE SOUTH OF ILLINOIS 10. ELEVATION = 595.148
BENCHMARK 21	CHISELED SQUARE ON EAST HEADWALL OF 2' X 2' BOX CULVERT FOR BUSINESS 55, 1.05 MILE SOUTH OF ILLINOIS 10. STATION 54+61.50 @ 50.10' LT ELEVATION = 594.325
BENCHMARK 22	CHISELED SQUARE ON EAST HEADWALL OF 24" CULVERT FOR BUSINESS 55, 1.25 MILE SOUTH OF ILLINOIS 10. STATION 66+22.64 @ 45.50' LT ELEVATION = 596.681

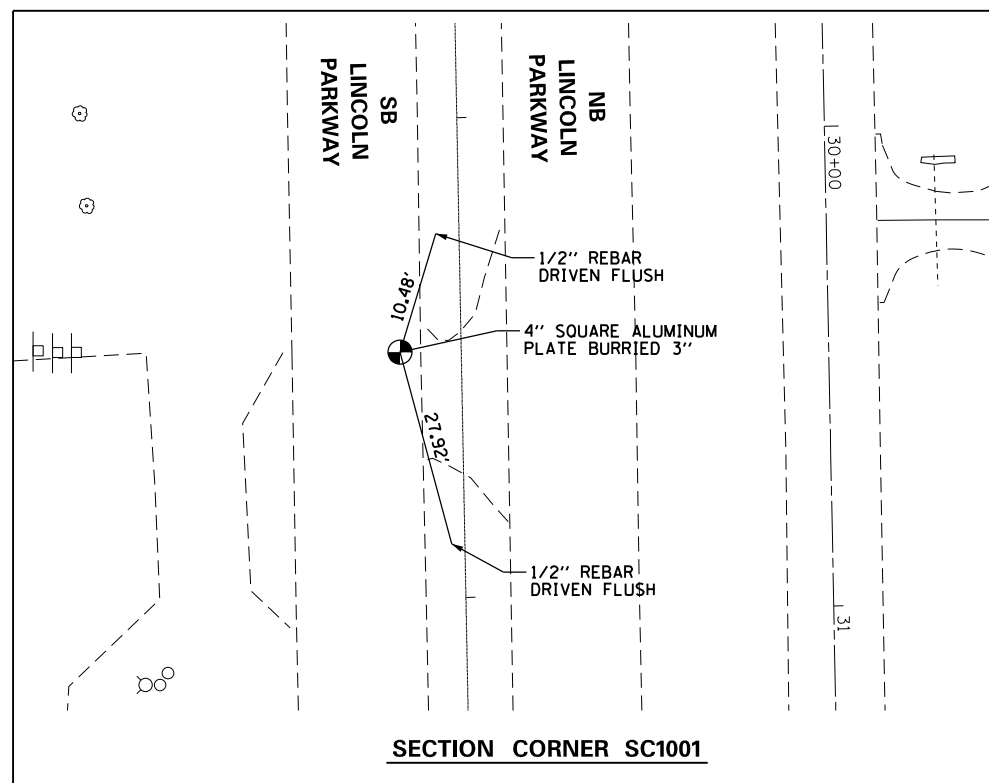
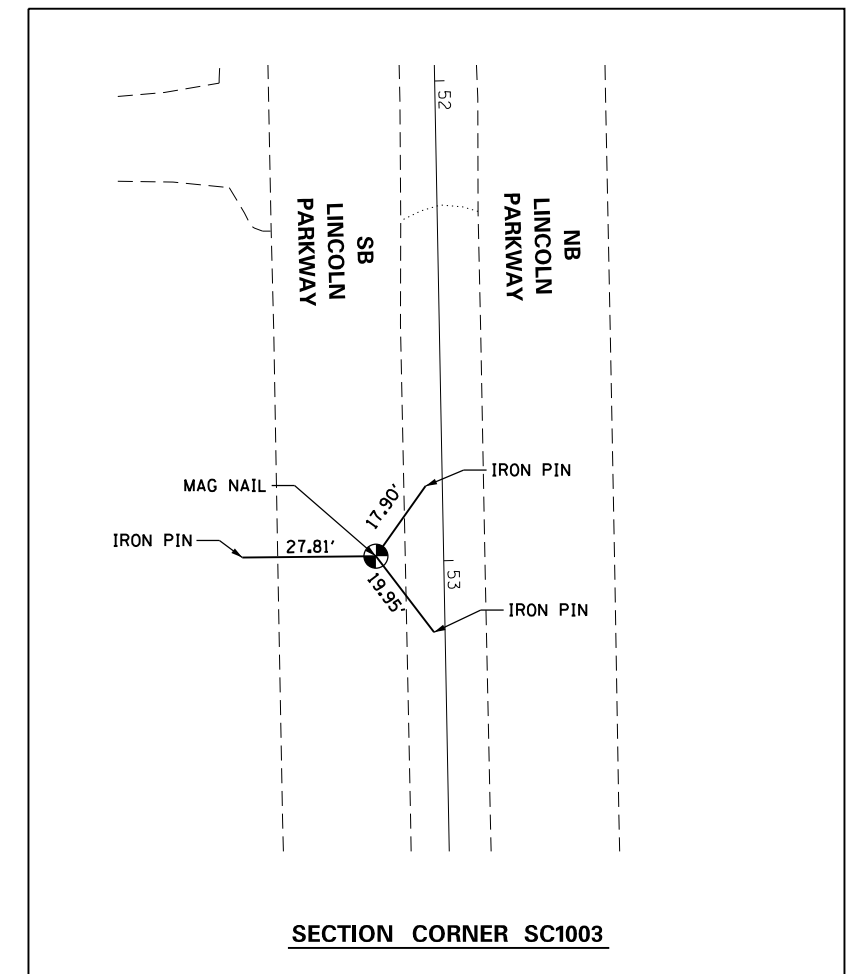
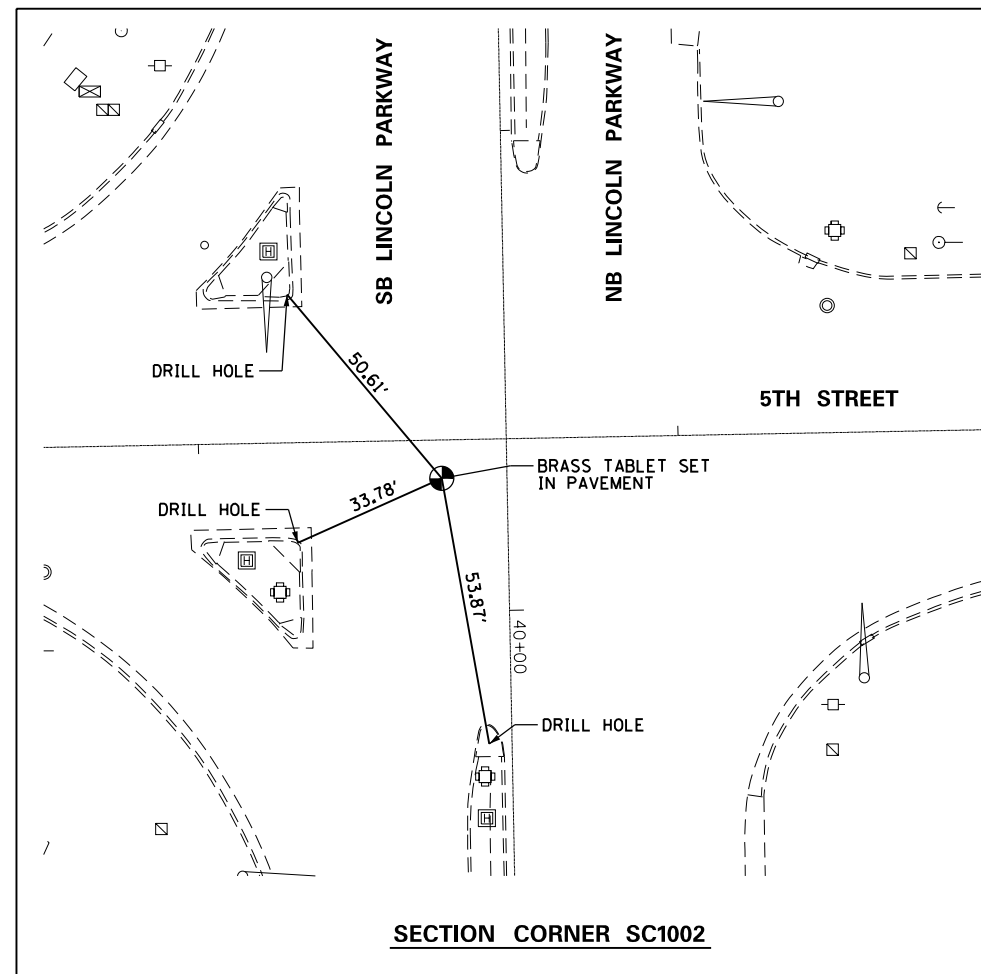
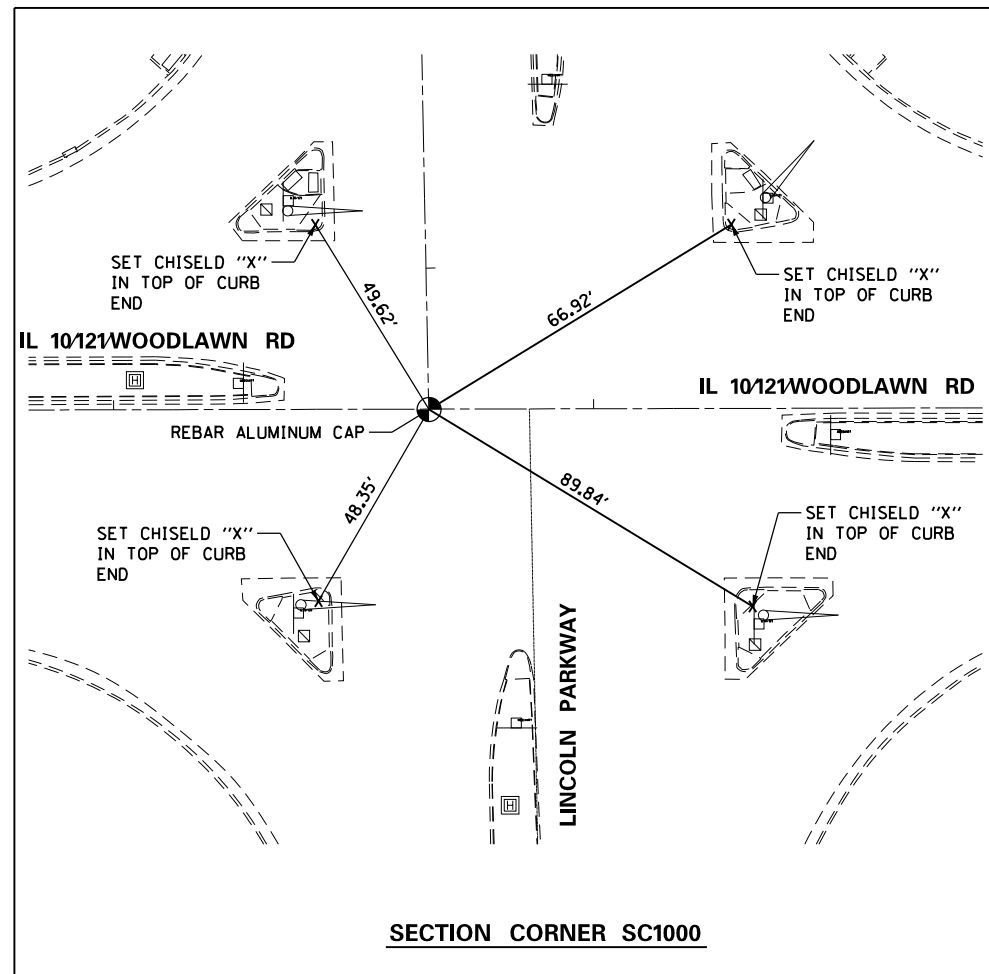
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		DATE - 06/24/11	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SURVEY TIES AND
 BENCHMARKS

SCALE: NO SCALE SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RT.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	(22)RS-6 & (23)RS-5,N,TS-1	LOGAN	343	77
				CONTRACT NO. 72E02
ILLINOIS FED. AID PROJECT				



SECTION CORNERS								
POINT	NORTHING	EASTING	ADJUSTMENT	STATION	OFFSET	DESCRIPTION	TYPE	TIF
SC1000	1,272,281.733	2,514,536.942	NAD83/2007 ADJ	137+65.73	0.062 RT	E 1/4 CORNER SECT 26, T20N, R3W, 3RD P.M	1	3446
SC1001	1,269,633.956	2,514,595.415	NAD83/2007 ADJ	26+48.61	12.756 RT	N.E. CORNER SECT. 35, T20N, R3W., 3TH P.M.	1	
SC1002	1,268,310.588	2,514,619.861	NAD83/2007 ADJ	39+72.20	13.609 RT	SW COR NW 1/4 NW 1/4 SECT 36, T20N, R3W, 3TH P.M.	1	3109
SC1003	1,266,984.229	2,514,644.593	NAD83/2007 ADJ	52+98.80	14.14 RT	EAST 1/4 CORNER SECT. 35, T20N, R3W, 3RD P.M.	1	

FILE NAME =	USER NAME = whitlingme	DESIGNED - MEW	REVISED -
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		DATE - 06/24/11	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SECTION CORNERS			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.
NO SCALE			

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	(22)RS-6 & (23)RS-5,N,TS-1	LOGAN	343	78
CONTRACT NO. 72E02				
ILLINOIS FED. AID PROJECT				

☒ EXISTING I-55 BUSINESS LOOP/LINCOLN PARKWAY (SOUTH OF IL10/121/WOODLAWN ROAD INTERSECTION)									
POINT NAME	CURVE NAME	GRID COORDINATES		EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
		STATION	NORTHING						
TM11301		0+00.49	1,272,281.84	2,514,557.84					
TM136		11+45.70	1,271,136.84	2,514,579.52					
TM137		35+00.02	1,268,782.95	2,514,624.40					
138		44+00.00	1,267,883.13	2,514,641.69					
307		69+98.54	1,265,285.06	2,514,690.97					
	103	PC 75+86.65	1,264,697.06	2,514,702.16					
		PI 91+15.85	1,263,168.13	2,514,731.26	43° 36' 41.60" RT	3,822.15'	1,529.20'	2,909.29'	294.56'
		PT 104+95.94	1,262,041.07	2,513,697.73					
		CC 112+84.29	1,261,460.03	2,513,164.91					
	100	PC 120+34.99	1,260,906.75	2,512,657.54	10° 05' 14.07" LT	8,505.95'	750.70'	1,497.52'	33.06'
		PI 127+81.81	1,260,273.15	2,512,254.92					
		CC 127+81.81	1,255,711.18	2,519,434.03					

☒ EXISTING POSTVILLE DRIVE (FRONTAGE ROAD)									
POINT NAME	CURVE NAME	GRID COORDINATES		EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
		STATION	NORTHING						
75		10+00.00	1,265,532.01	2,514,845.20					
AL12230		10+54.70	1,265,572.92	2,514,808.89					
AL12231		10+85.10	1,265,595.66	2,514,788.71					

☒ EXISTING 5TH STREET									
POINT NAME	CURVE NAME	GRID COORDINATES		EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
		STATION	NORTHING						
15004		3+50.53	1,268,300.78	2,513,619.82					
3050		10+49.98	1,268,312.91	2,514,319.17					
305		13+64.18	1,268,318.83	2,514,633.31					
3650		16+49.98	1,268,324.21	2,514,919.06					
15003		27+63.31	1,268,345.96	2,516,032.18					

☒ EXISTING I-55 BUSINESS LOOP/LINCOLN PARKWAY (NORTH OF IL10/121/WOODLAWN ROAD INTERSECTION)									
POINT NAME	CURVE NAME	GRID COORDINATES		EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
		STATION	NORTHING						
RK1004		1746+30.50	1,273,880.40	2,514,509.87					
		PC 1753+29.70	1,273,181.36	2,514,524.84					
	8	PI 1754+53.30	1,273,057.79	2,514,527.48	1° 28' 58.77" RT	9,550.10'	123.60'	247.19'	0.80'
		PRC 1755+76.89	1,272,934.19	2,514,526.93					
		CC 1755+76.89	1,272,976.91	2,504,976.93					
		PRC 1755+76.89	1,272,934.19	2,514,526.93					
	9	PI 1756+90.79	1,272,820.29	2,514,526.42	1° 22' 00.53" LT	9,548.75'	113.90'	227.79'	0.68'
		PT 1758+04.68	1,272,706.41	2,514,528.63					
		CC 1758+04.68	1,272,891.48	2,524,075.58					
1300		1762+29.37	1,272,281.80	2,514,536.86					

☒ EXISTING STRINGER AVENUE									
POINT NAME	CURVE NAME	GRID COORDINATES		EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
		STATION	NORTHING						
660		5+60.20	1,266,780.12	2,515,622.94					
68		23+22.40	1,265,284.52	2,514,690.98					

☒ EXISTING IL 10/121/WOODLAWN ROAD									
POINT NAME	CURVE NAME	GRID COORDINATES		EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
		STATION	NORTHING						
		PC 124+88.62	1,272,289.29	2,513,259.89					
	C1112	PI 129+57.39	1,272,279.81	2,513,728.56	1° 18' 25.14" LT	41,098.23'	468.77'	937.50'	2.67'
		PT 134+26.12	1,272,281.02	2,514,197.33					
		CC 134+26.12	1,313,379.11	2,514,091.16					
161		135+06.27	1,272,281.23	2,514,277.48					
300		137+65.65	1,272,281.79	2,514,536.86					
301		137+86.63	1,272,281.84	2,514,557.84					
162		140+65.34	1,272,282.45	2,514,836.55					
115		155+33.74	1,272,284.81	2,516,304.95					

☒ EXISTING STALHUT DRIVE)									
POINT NAME	CURVE NAME	GRID COORDINATES		EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
		STATION	NORTHING						
HI005		24+61.29	1,270,597.89	2,513,295.53					
		PC 24+61.30	1,270,597.90	2,513,295.53					
	1	PI 31+17.84	1,269,941.47	2,513,307.41	117° 17' 43.26" LT	400.00'	656.54'	818.88'	368.79'
		PT 32+80.18	1,270,253.05	2,513,885.30					
		CC 32+80.18	1,270,605.14	2,513,695.47					
		PC 32+80.20	1,270,253.05	2,513,885.29					
	2	PI 34+05.20	1,270,312.37	2,513,995.32	28° 20' 37.10" RT	495.02'	125.00'	244.88'	15.54'
		PT 35+25.08	1,270,312.35	2,514,120.32					
		CC 35+25.08	1,269,817.33	2,514,120.22					
409		40+00.00	1,270,312.25	2,514,595.24					

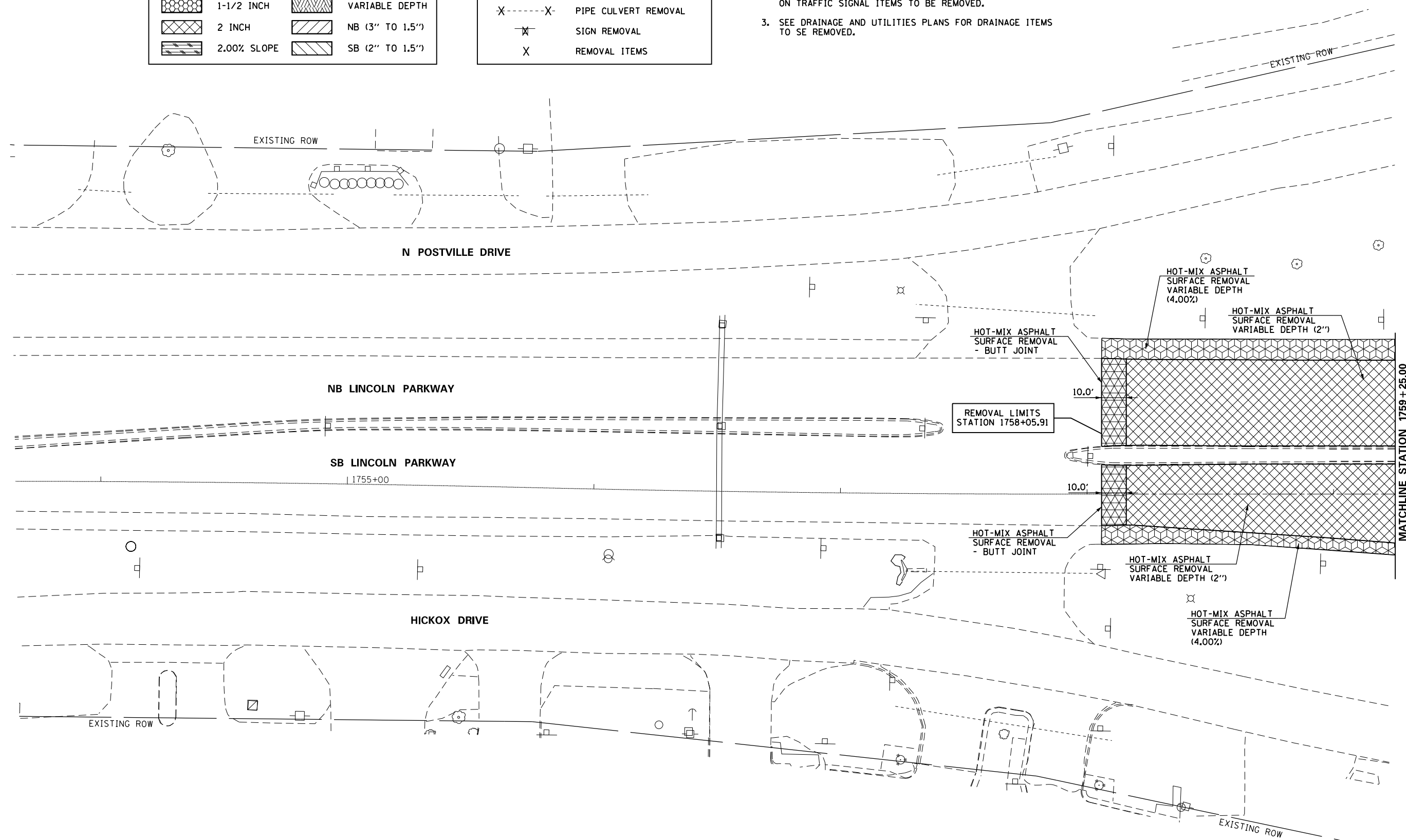
☒ EXISTING SHORT ELEVENTH STREET									
POINT NAME	CURVE NAME	GRID COORDINATES		EASTING	DELTA	RADIUS	TANGENT	LENGTH	EXTERNAL
		STATION	NORTHING						
TM1000		40+00.00	1,270,310.19	2,514,595.28					
14866		41+27.14	1,270,312.59	2,514,722.40					
14793		48+30.33	1,270,325.87	2,515,425.47					

HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND			
	BUTT JOINT		4.00% SLOPE
	1-1/2 INCH		VARIABLE DEPTH
	2 INCH		NB (3" TO 1.5")
	2.00% SLOPE		SB (2" TO 1.5")

REMOVAL LEGEND	
	MEDIAN REMOVAL
	PAVED SHOULDER REMOVAL
	PIPE CULVERT REMOVAL
	SIGN REMOVAL
	REMOVAL ITEMS

NOTES:

1. SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
2. SEE TRAFFIC SIGNAL PLANS FOR ADDITIONAL INFORMATION ON TRAFFIC SIGNAL ITEMS TO BE REMOVED.
3. SEE DRAINAGE AND UTILITIES PLANS FOR DRAINAGE ITEMS TO SE REMOVED.



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		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

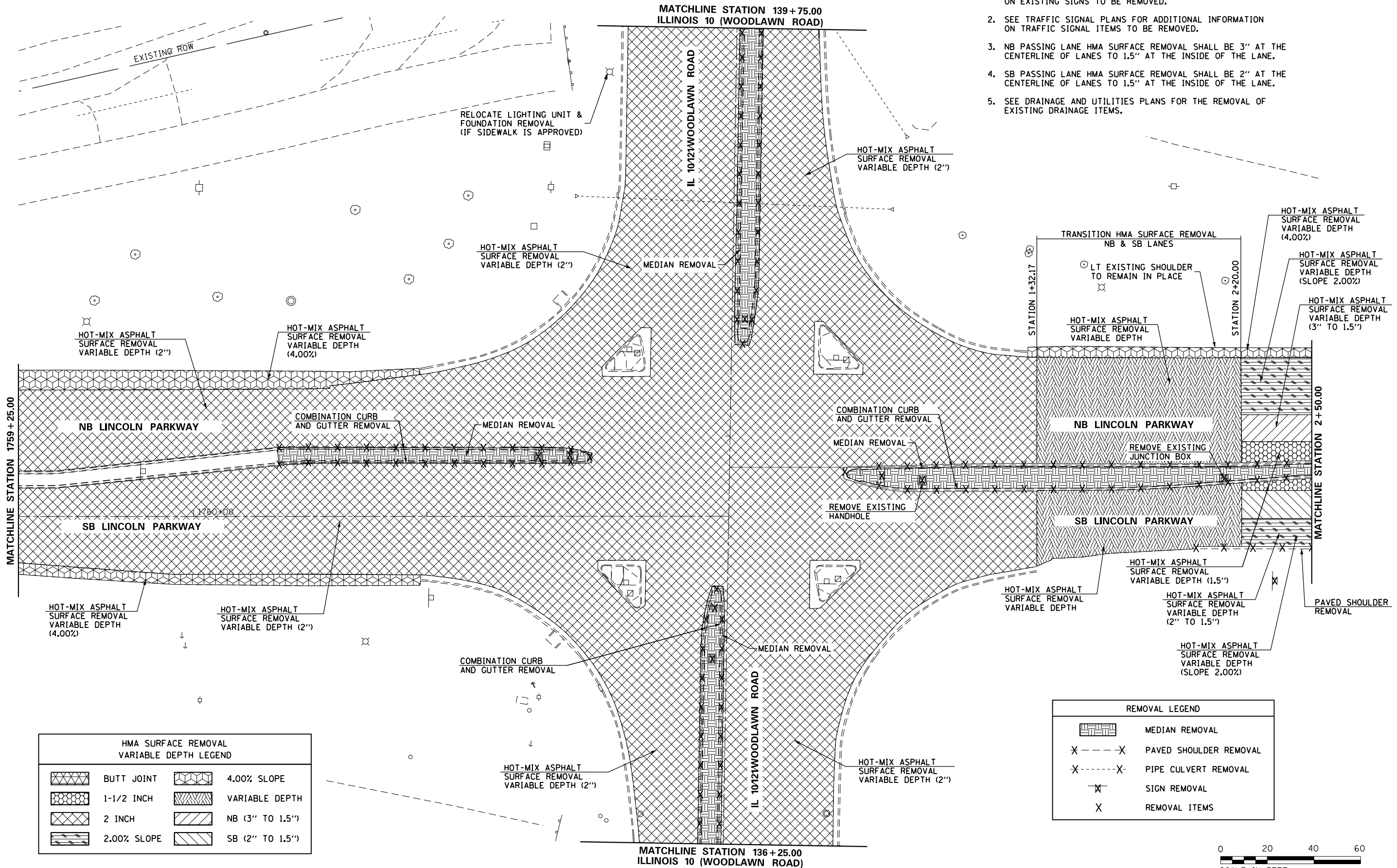
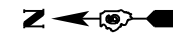
REMOVAL PLAN

SCALE: 1" = 20' SHEET OF SHEETS STA. 1754+00.00 TO STA. 1759+25.00

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	•	LOGAN	343	81
CONTRACT NO. 72E02				
ILLINOIS FED. AID PROJECT				

NOTES:

1. SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
2. SEE TRAFFIC SIGNAL PLANS FOR ADDITIONAL INFORMATION ON TRAFFIC SIGNAL ITEMS TO BE REMOVED.
3. NB PASSING LANE HMA SURFACE REMOVAL SHALL BE 3" AT THE CENTERLINE OF LANES TO 1.5" AT THE INSIDE OF THE LANE.
4. SB PASSING LANE HMA SURFACE REMOVAL SHALL BE 2" AT THE CENTERLINE OF LANES TO 1.5" AT THE INSIDE OF THE LANE.
5. SEE DRAINAGE AND UTILITIES PLANS FOR THE REMOVAL OF EXISTING DRAINAGE ITEMS.



HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND	
	BUTT JOINT
	1-1/2 INCH
	2 INCH
	2.00% SLOPE
	4.00% SLOPE
	VARIABLE DEPTH
	NB (3" TO 1.5")
	SB (2" TO 1.5")

REMOVAL LEGEND	
	MEDIAN REMOVAL
	PAVED SHOULDER REMOVAL
	PIPE CULVERT REMOVAL
	SIGN REMOVAL
	REMOVAL ITEMS

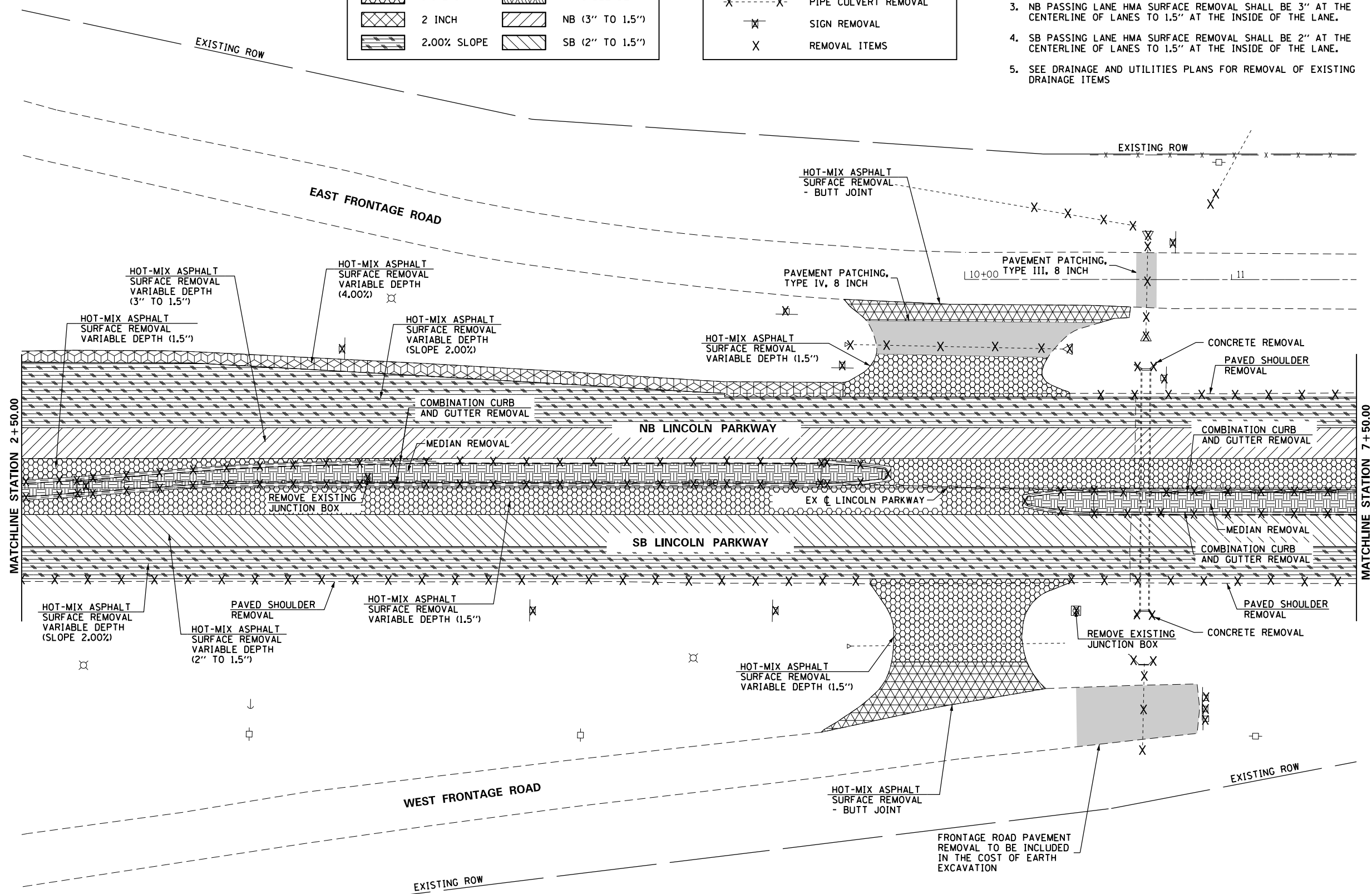
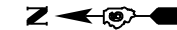


HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND			
	BUTT JOINT		4.00% SLOPE
	1-1/2 INCH		VARIABLE DEPTH
	2 INCH		NB (3'' TO 1.5'')
	2.00% SLOPE		SB (2'' TO 1.5'')

REMOVAL LEGEND	
	MEDIAN REMOVAL
	PAVED SHOULDER REMOVAL
	PIPE CULVERT REMOVAL
	SIGN REMOVAL
	REMOVAL ITEMS

NOTES:

1. SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
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5. SEE DRAINAGE AND UTILITIES PLANS FOR REMOVAL OF EXISTING DRAINAGE ITEMS



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	PLOT SCALE = 40.00' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/21/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVAL PLAN

SCALE: 1" = 20' SHEET OF SHEETS STA. 2+50.00 TO STA. 7+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	.	LOGAN	343	83
CONTRACT NO. 72E02				
ILLINOIS FED. AID PROJECT				

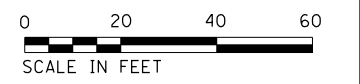
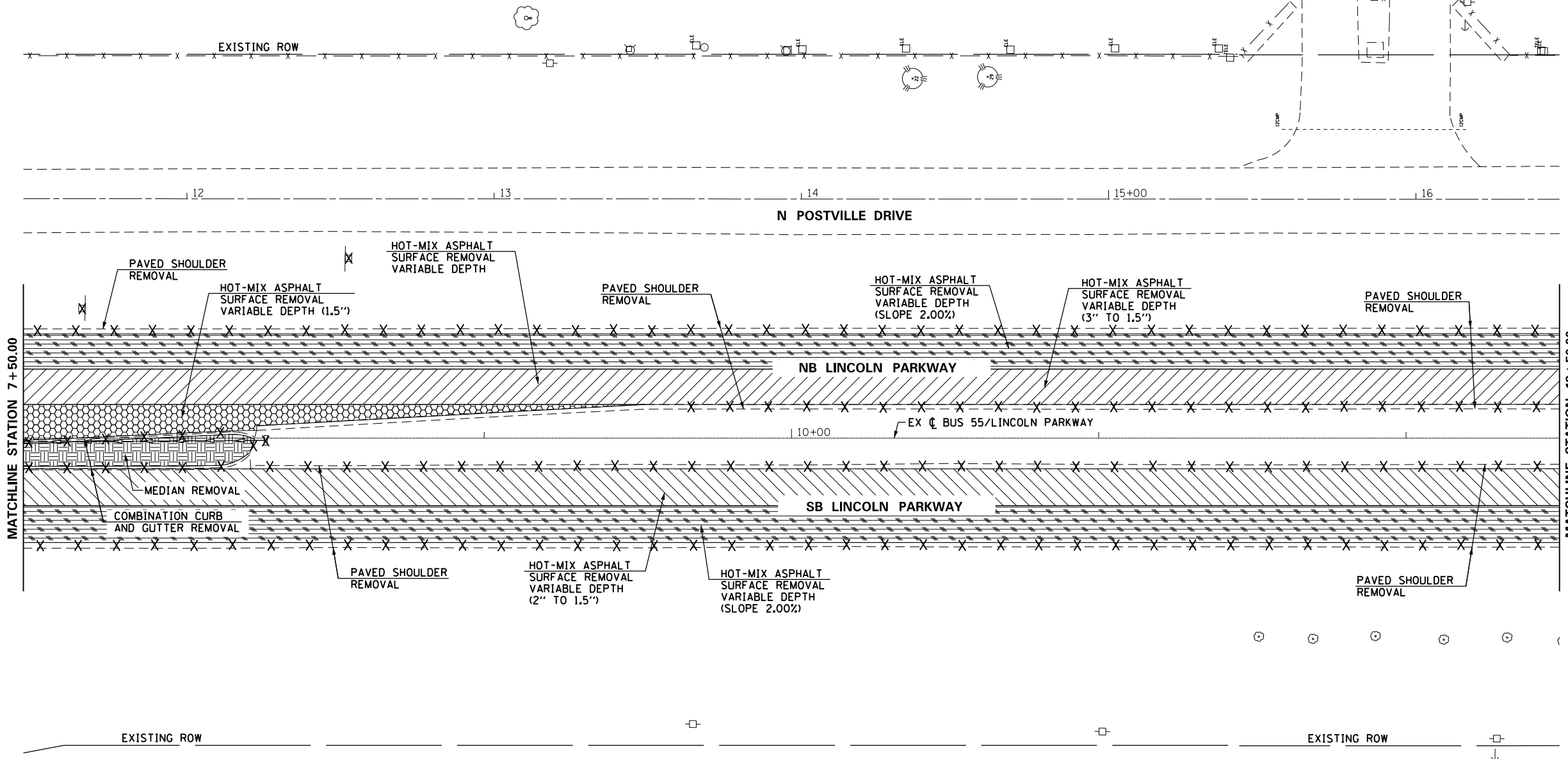


HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND			
	BUTT JOINT		4.00% SLOPE
	1-1/2 INCH		VARIABLE DEPTH
	2 INCH		NB (3" TO 1.5")
	2.00% SLOPE		SB (2" TO 1.5")

REMOVAL LEGEND	
	MEDIAN REMOVAL
	PAVED SHOULDER REMOVAL
	PIPE CULVERT REMOVAL
	SIGN REMOVAL
	REMOVAL ITEMS

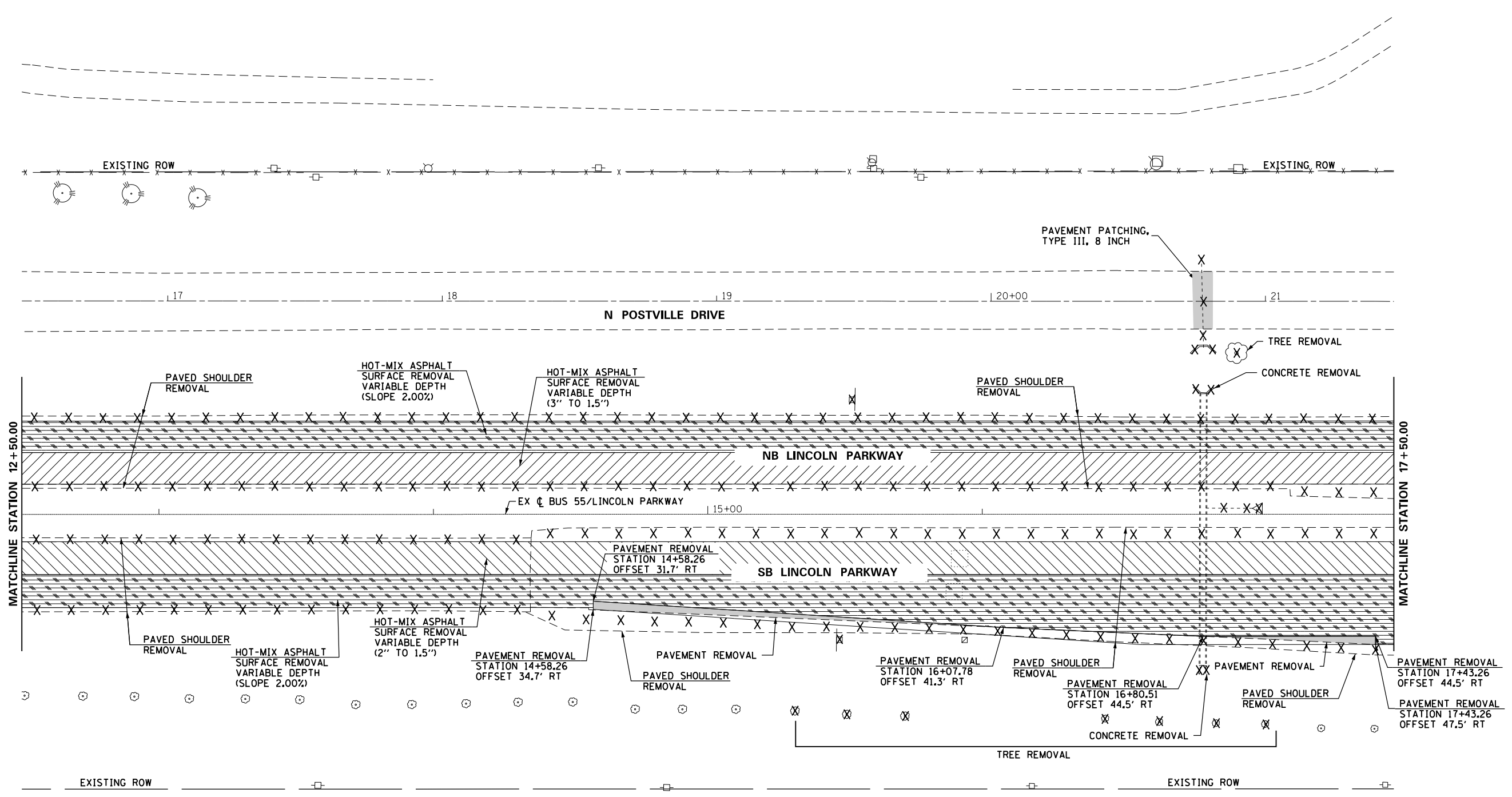
NOTES:

1. SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
2. SEE TRAFFIC SIGNAL PLANS FOR ADDITIONAL INFORMATION ON TRAFFIC SIGNAL ITEMS TO BE REMOVED.
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4. SB PASSING LANE HMA SURFACE REMOVAL SHALL BE 2" AT THE CENTERLINE OF LANES TO 1.5" AT THE INSIDE OF THE LANE.
5. SEE DRAINAGE AND UTILITIES PLAN FOR REMOVAL OF EXISTING DRAINAGE ITEMS



FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVAL PLAN				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\1\084EBIDINTEG.illinois.gov\PIWIDOTDocuments\IDOT Offices\District 6\Projects\0672EDR\Drawings\EAD\Sheets\0672E02-sht-07-rev		DRAWN -	REVISED -		7706	.	LOGAN	343	84				
PLOT SCALE = 40.00' / in.		CHECKED -	REVISED -		SCALE: 1" = 20'				SHEET OF SHEETS STA. 7+50.00 TO STA. 12+50.00				
PLOT DATE = 10/21/2016		DATE -	REVISED -		ILLINOIS FED. AID PROJECT				CONTRACT NO. 72E02				

• (22)RS-6 & (23)RS-5,N,TS-1

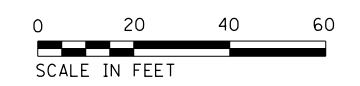


HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND			
	BUTT JOINT		4.00% SLOPE
	1-1/2 INCH		VARIABLE DEPTH
	2 INCH		NB (3" TO 1.5")
	2.00% SLOPE		SB (2" TO 1.5")

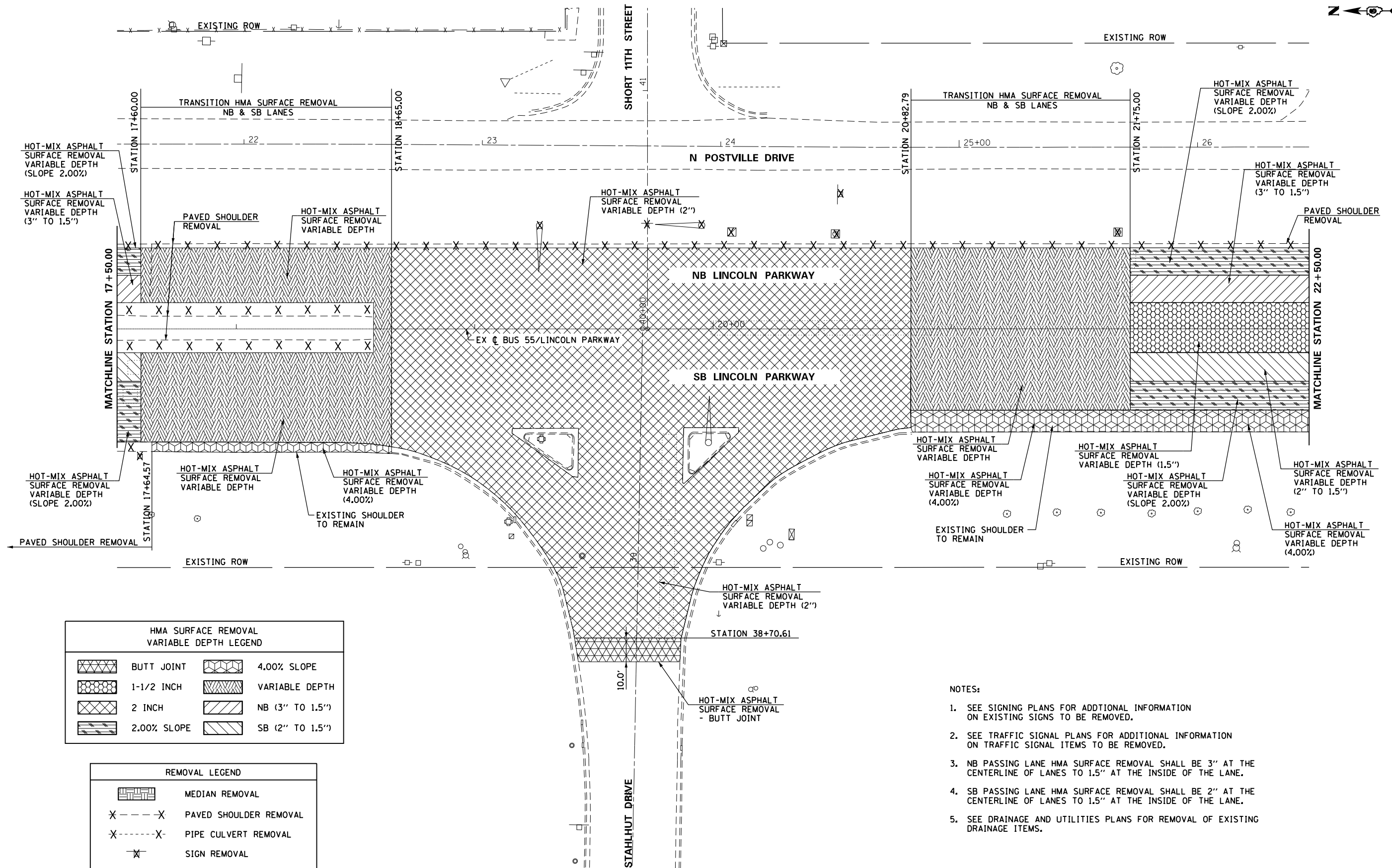
REMOVAL LEGEND	
	MEDIAN REMOVAL
	PAVED SHOULDER REMOVAL
	PIPE CULVERT REMOVAL
	SIGN REMOVAL
	REMOVAL ITEMS

NOTES:

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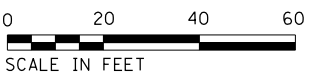
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PLOT SCALE = 40.00' / in.		DATE -	REVISED -					CONTRACT NO. 72E02				

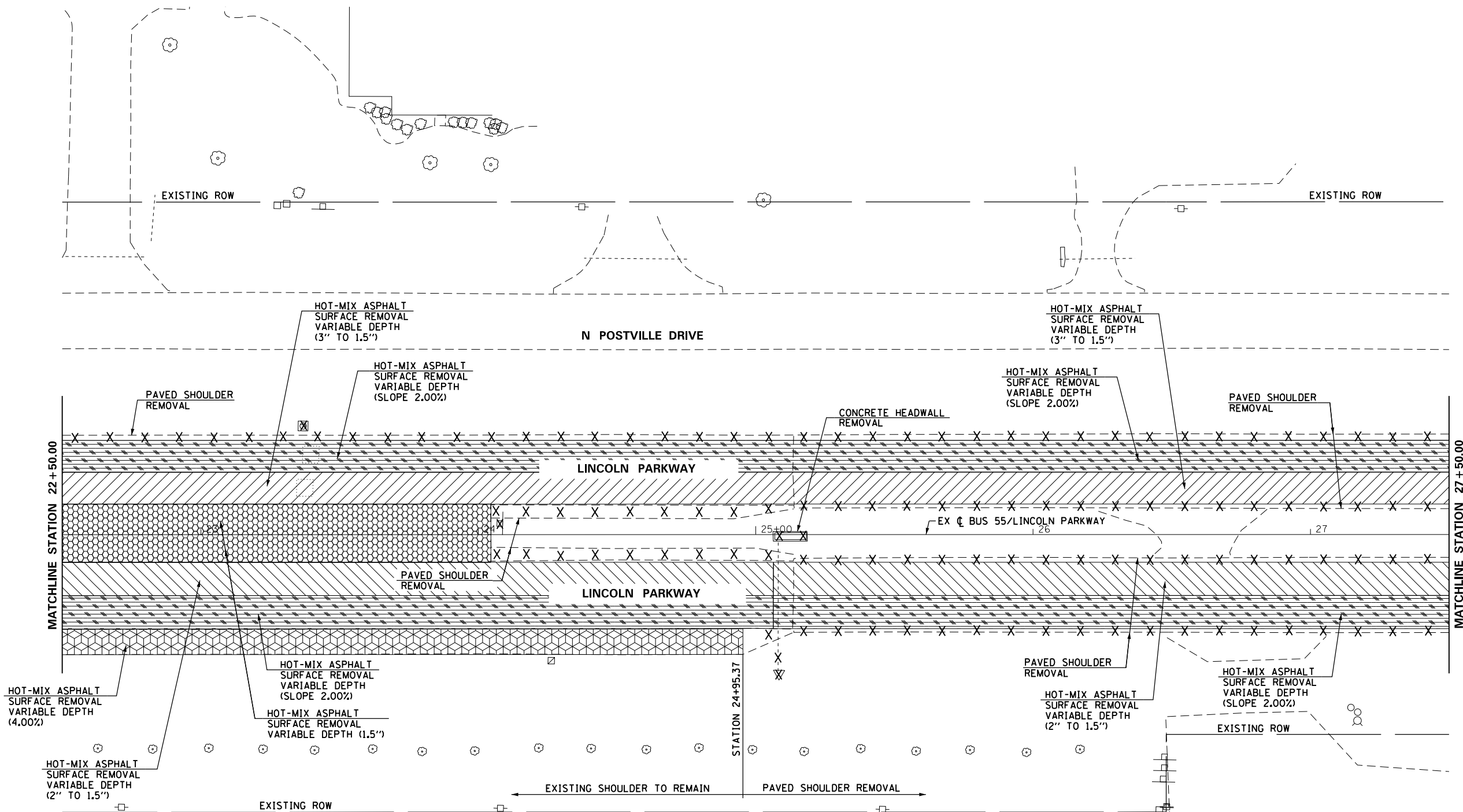


HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND			
	BUTT JOINT		4.00% SLOPE
	1-1/2 INCH		VARIABLE DEPTH
	2 INCH		NB (3" TO 1.5")
	2.00% SLOPE		SB (2" TO 1.5")

REMOVAL LEGEND	
	MEDIAN REMOVAL
	PAVED SHOULDER REMOVAL
	PIPE CULVERT REMOVAL
	SIGN REMOVAL
	REMOVAL ITEMS

- NOTES:
- SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
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- NOTES:
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HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND			
	BUTT JOINT		4.00% SLOPE
	1-1/2 INCH		VARIABLE DEPTH
	2 INCH		NB (3" TO 1.5")
	2.00% SLOPE		SB (2" TO 1.5")

REMOVAL LEGEND	
	MEDIAN REMOVAL
	PAVED SHOULDER REMOVAL
	PIPE CULVERT REMOVAL
	SIGN REMOVAL
	REMOVAL ITEMS

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		DATE -	REVISED -

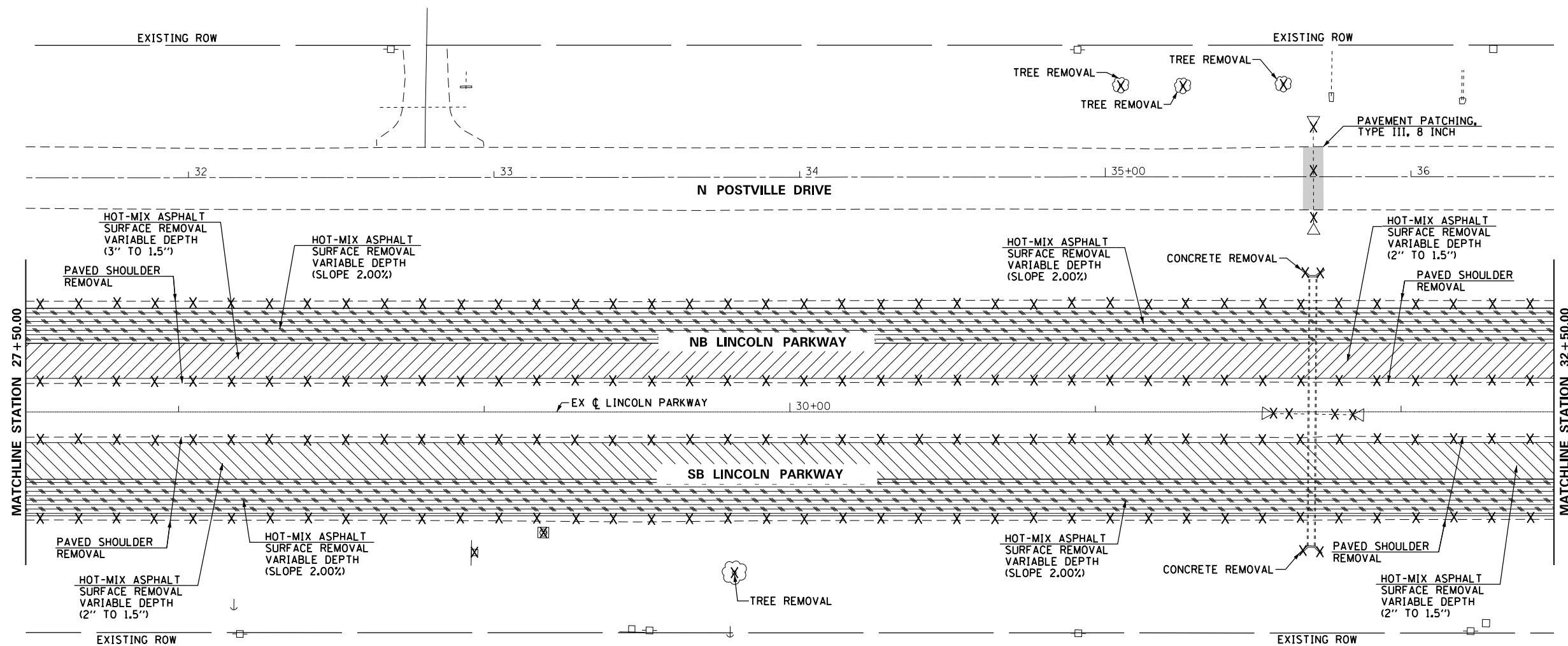
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-55 BUSINESS LOOP (LINCOLN PARKWAY)
PROPOSED PLAN**

SCALE: 1" = 20' SHEET OF SHEETS STA. 22+50.00 TO STA. 27+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	.	LOGAN	343	87
CONTRACT NO. 72E02				
ILLINOIS FED. AID PROJECT				

(22)RS-6 & (23)RS-5,N,TS-1



HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND			
	BUTT JOINT		4.00% SLOPE
	1-1/2 INCH		VARIABLE DEPTH
	2 INCH		NB (3" TO 1.5")
	2.00% SLOPE		SB (2" TO 1.5")

REMOVAL LEGEND	
	MEDIAN REMOVAL
	PAVED SHOULDER REMOVAL
	PIPE CULVERT REMOVAL
	SIGN REMOVAL
	REMOVAL ITEMS

NOTES:

- SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
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		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

REMOVAL PLAN

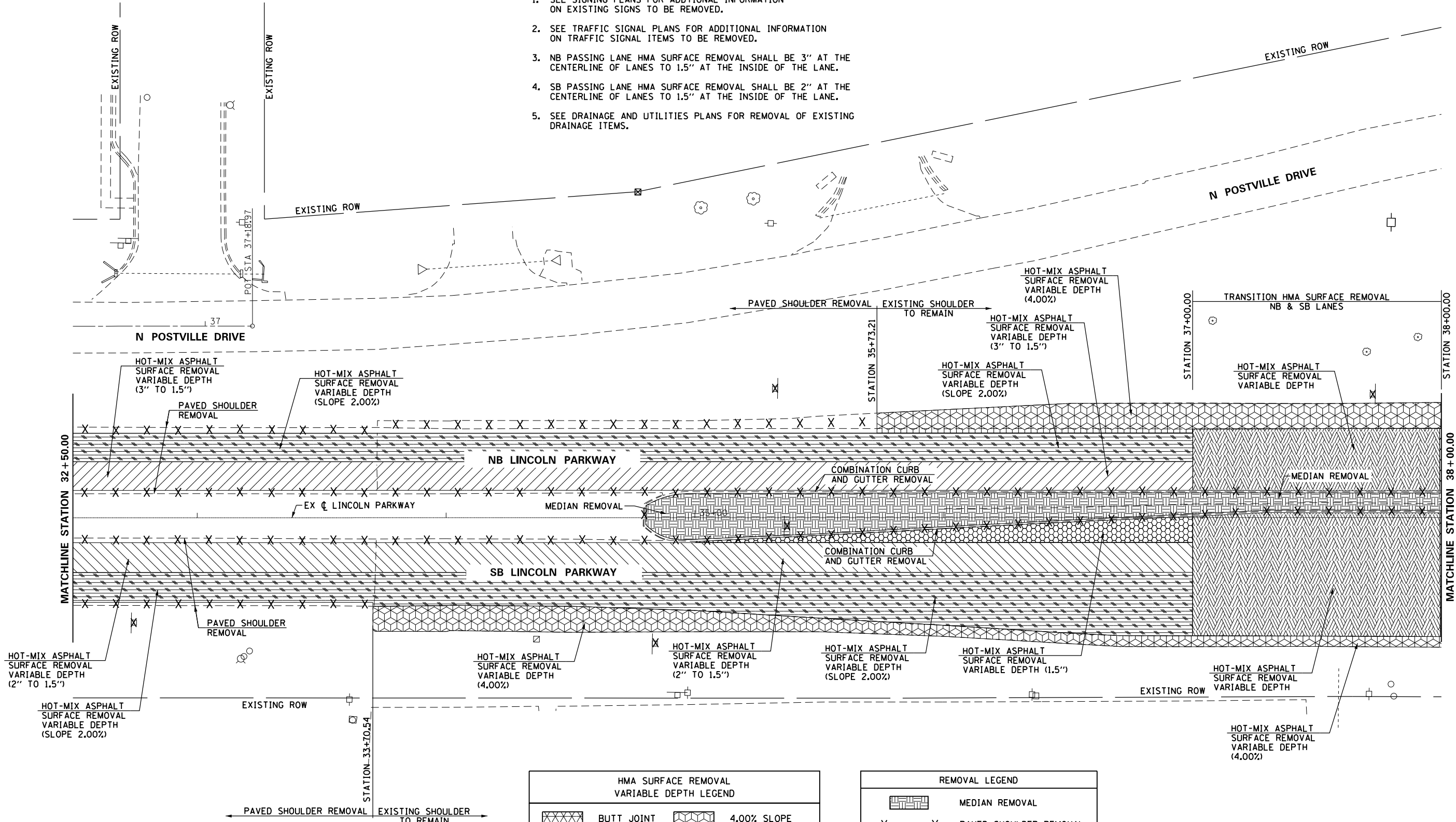
SCALE: 1" = 20' SHEET OF SHEETS STA. 27+50.00 TO STA. 32+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	.	LOGAN	343	88
CONTRACT NO. 72E02				
ILLINOIS FED. AID PROJECT				



NOTES:

1. SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
2. SEE TRAFFIC SIGNAL PLANS FOR ADDITIONAL INFORMATION ON TRAFFIC SIGNAL ITEMS TO BE REMOVED.
3. NB PASSING LANE HMA SURFACE REMOVAL SHALL BE 3" AT THE CENTERLINE OF LANES TO 1.5" AT THE INSIDE OF THE LANE.
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5. SEE DRAINAGE AND UTILITIES PLANS FOR REMOVAL OF EXISTING DRAINAGE ITEMS.



HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND			
	BUTT JOINT		4.00% SLOPE
	1-1/2 INCH		VARIABLE DEPTH
	2 INCH		NB (3" TO 1.5")
	2.00% SLOPE		SB (2" TO 1.5")

REMOVAL LEGEND	
	MEDIAN REMOVAL
	PAVED SHOULDER REMOVAL
	PIPE CULVERT REMOVAL
	SIGN REMOVAL
	REMOVAL ITEMS



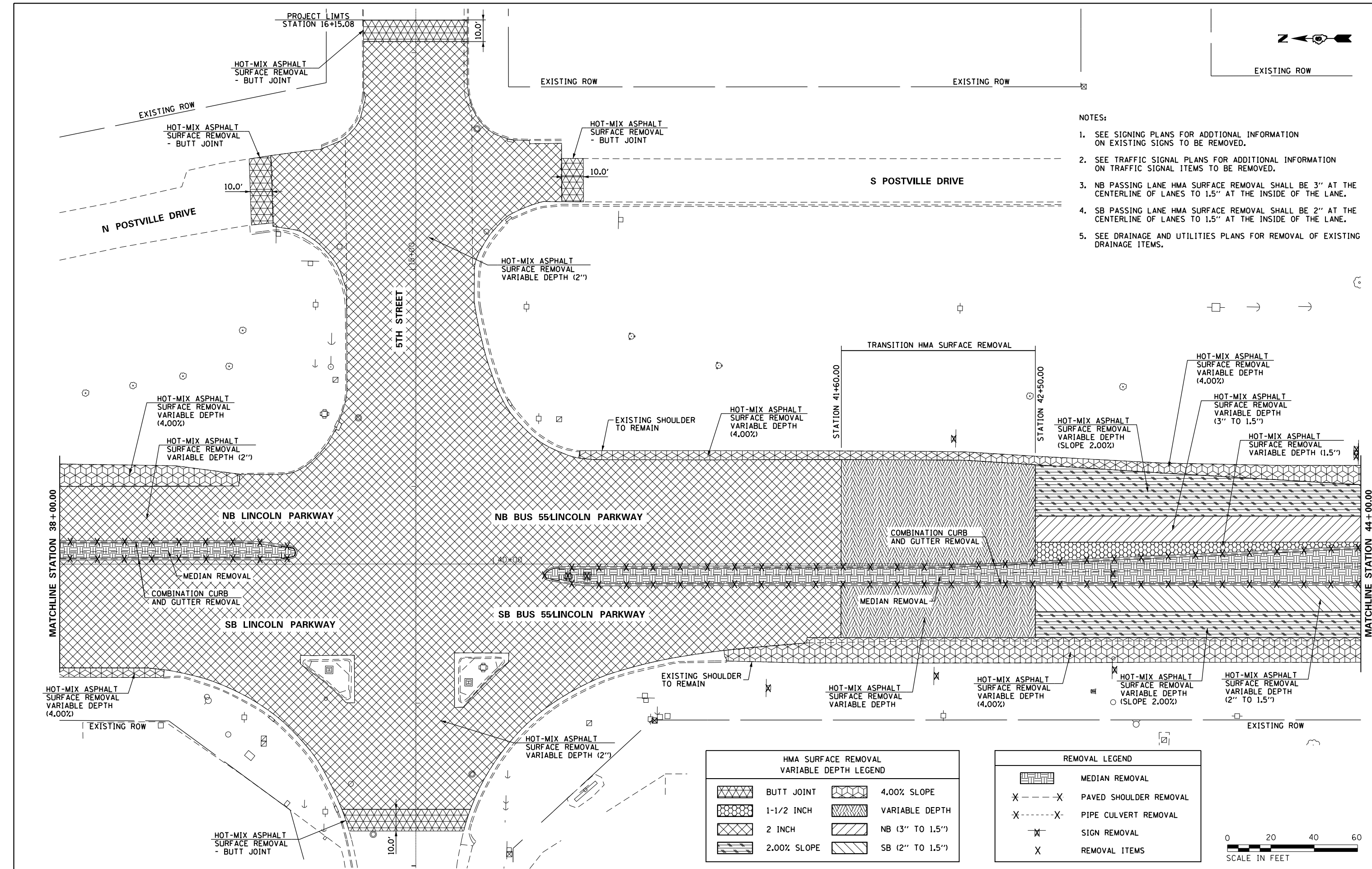
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PLOT SCALE = 40.00' / in.		CHECKED -	REVISED -		CONTRACT NO. 72E02							
PLOT DATE = 10/21/2016		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

(22)RS-6 & (23)RS-5,N,TS-1



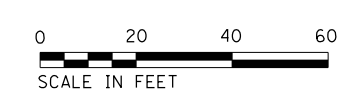
EXISTING ROW

- NOTES:
1. SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
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HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND	
	BUTT JOINT
	1-1/2 INCH
	2 INCH
	2.00% SLOPE
	4.00% SLOPE
	VARIABLE DEPTH
	NB (3" TO 1.5")
	SB (2" TO 1.5")

REMOVAL LEGEND	
	MEDIAN REMOVAL
	PAVED SHOULDER REMOVAL
	PIPE CULVERT REMOVAL
	SIGN REMOVAL
	REMOVAL ITEMS



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	PLOT DATE = 10/21/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

REMOVAL PLAN			
SCALE: 1" = 20'	SHEET	OF	SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	.	LOGAN	343	90
CONTRACT NO. 72E02				
ILLINOIS FED. AID PROJECT				

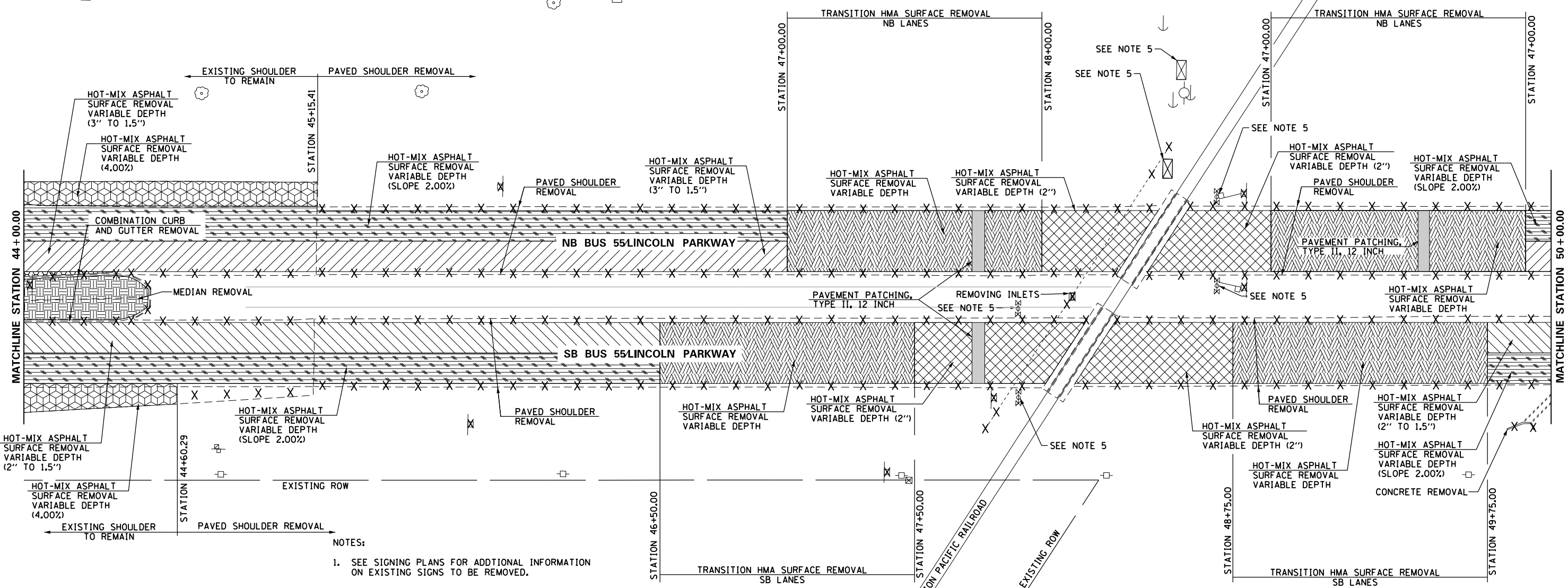
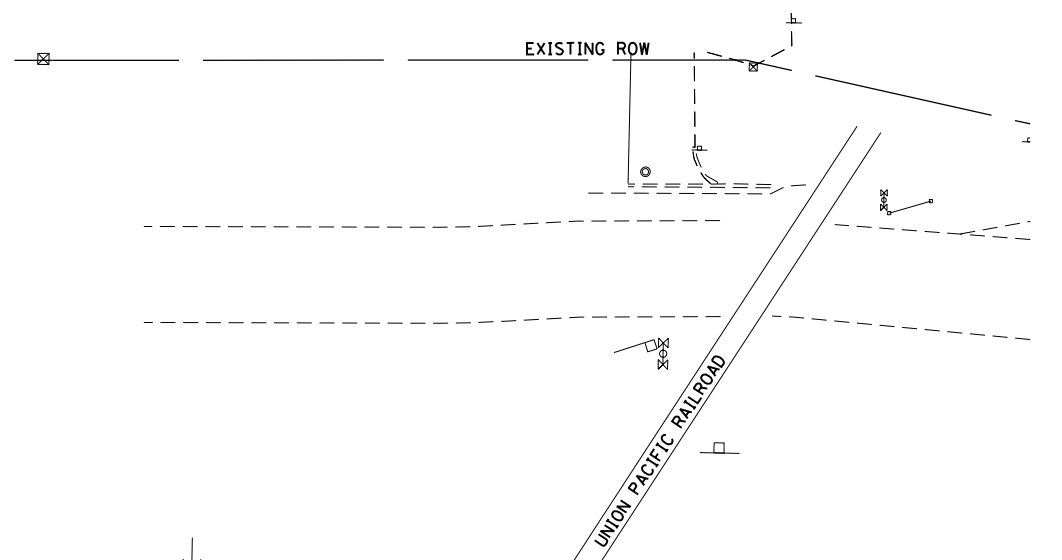
(22)RS-6 & (23)RS-5,N,TS-1



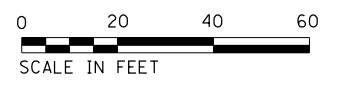
HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND	
	BUTT JOINT
	1-1/2 INCH
	2 INCH
	2.00% SLOPE
	4.00% SLOPE
	VARIABLE DEPTH
	NB (3" TO 1.5")
	SB (2" TO 1.5")

REMOVAL LEGEND	
	MEDIAN REMOVAL
X - - - X	PAVED SHOULDER REMOVAL
X - - - - - X	PIPE CULVERT REMOVAL
	SIGN REMOVAL
X	REMOVAL ITEMS

- NOTES:
- SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
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 - NB PASSING LANE HMA SURFACE REMOVAL SHALL BE 3" AT THE CENTERLINE OF LANES TO 1.5" AT THE INSIDE OF THE LANE.
 - SB PASSING LANE HMA SURFACE REMOVAL SHALL BE 2" AT THE CENTERLINE OF LANES TO 1.5" AT THE INSIDE OF THE LANE.
 - RAILROAD FLASHING LIGHTS, BUNGALOW AND CROSSING SURFACE WILL BE REMOVED AND NEW EQUIPMENT INSTALLED PRIOR TO CONSTRUCTION. RAILROAD CROSSING EQUIPMENT PERMANENT LOCATIONS MAY NOT MATCH AS WHAT IS SHOWN ON THESE PLANS.
 - SEE DRAINAGE AND UTILITIES PLANS FOR REMOVAL OF EXISTING DRAINAGE ITEMS.



- NOTES:
- SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
 - SEE TRAFFIC SIGNAL PLANS FOR ADDITIONAL INFORMATION ON TRAFFIC SIGNAL ITEMS TO BE REMOVED.
 - RAILROAD FLASHING LIGHTS, BUNGALOW AND CROSSING SURFACE WILL BE REMOVED AND NEW EQUIPMENT INSTALLED PRIOR TO CONSTRUCTION. RAILROAD CROSSING EQUIPMENT PERMANENT LOCATIONS MAY NOT MATCH AS WHAT IS SHOWN ON THESE PLANS.



FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVAL PLAN			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\1\084EBIDINTEG\Illinois.gov\PI\DOT\Documents\DOT Offices\District 6\Projects\0672ED\Drawings\EA0\Sheets\0672E02-sht-07-rev-01.dwg	PLOT SCALE = 40.00' / in.	CHECKED -	REVISED -		7706	.	LOGAN	343	91			
PLOT DATE = 10/21/2016	DATE -	REVISED -	REVISED -		CONTRACT NO. 72E02			ILLINOIS FED. AID PROJECT				

SCALE: 1" = 20' SHEET OF SHEETS STA. 44+00.00 TO STA. 50+00.00

(22)RS-6 & (23)RS-5,N,TS-1

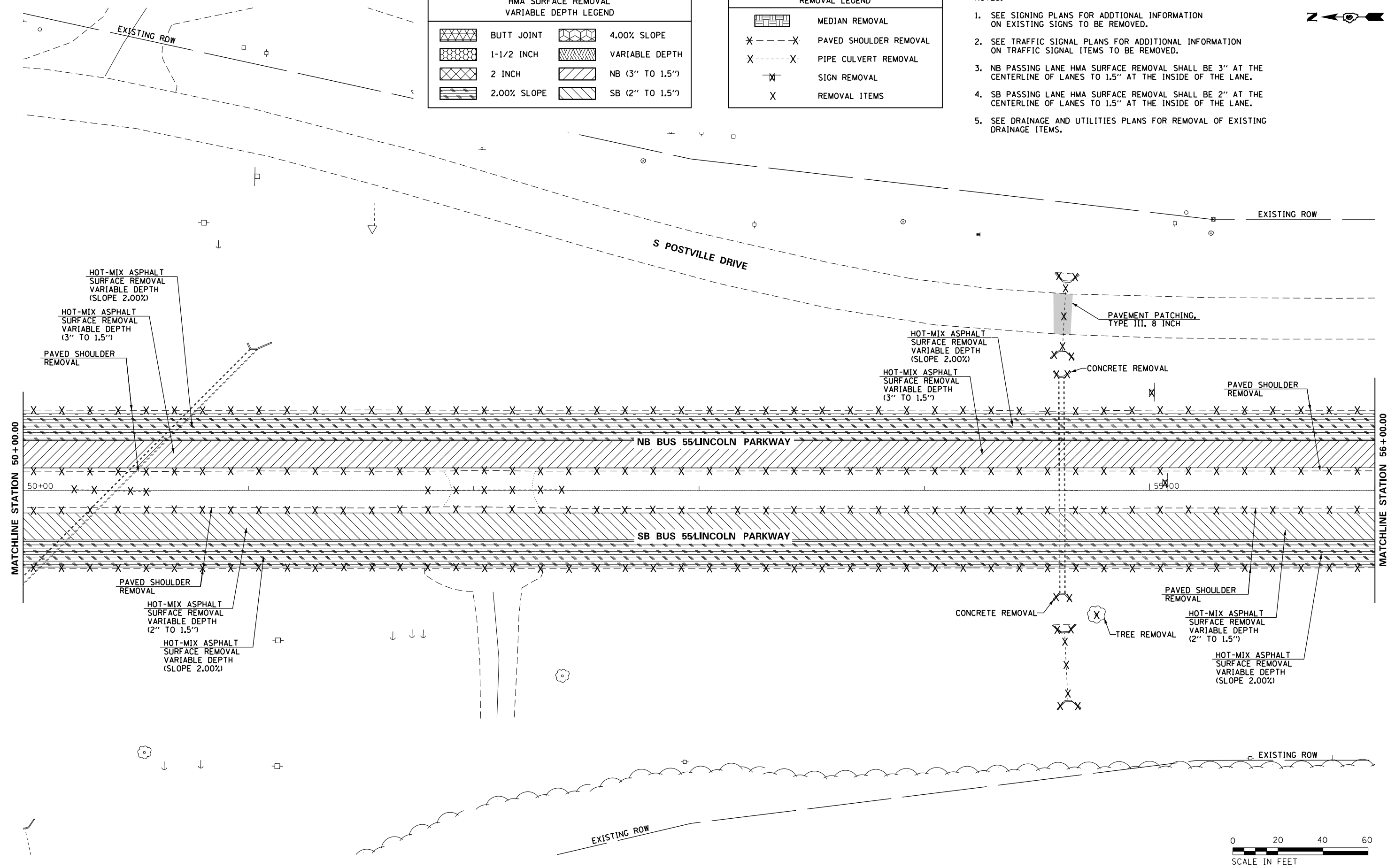


HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND			
	BUTT JOINT		4.00% SLOPE
	1-1/2 INCH		VARIABLE DEPTH
	2 INCH		NB (3" TO 1.5")
	2.00% SLOPE		SB (2" TO 1.5")

REMOVAL LEGEND	
	MEDIAN REMOVAL
	PAVED SHOULDER REMOVAL
	PIPE CULVERT REMOVAL
	SIGN REMOVAL
	REMOVAL ITEMS

NOTES:

1. SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
2. SEE TRAFFIC SIGNAL PLANS FOR ADDITIONAL INFORMATION ON TRAFFIC SIGNAL ITEMS TO BE REMOVED.
3. NB PASSING LANE HMA SURFACE REMOVAL SHALL BE 3" AT THE CENTERLINE OF LANES TO 1.5" AT THE INSIDE OF THE LANE.
4. SB PASSING LANE HMA SURFACE REMOVAL SHALL BE 2" AT THE CENTERLINE OF LANES TO 1.5" AT THE INSIDE OF THE LANE.
5. SEE DRAINAGE AND UTILITIES PLANS FOR REMOVAL OF EXISTING DRAINAGE ITEMS.



FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
p:\11\084EBIDINTEG\Illinois.gov\PI\DOT\Documents\DOT Offices\District 6\Projects\0672\Drawings\EA0\Sheets\0672E02-sht-07-rev		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

REMOVAL PLAN

SCALE: 1" = 20' SHEET OF SHEETS STA. 50+00.00 TO STA. 56+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	.	LOGAN	343	92
CONTRACT NO. 72E02				
ILLINOIS FED. AID PROJECT				

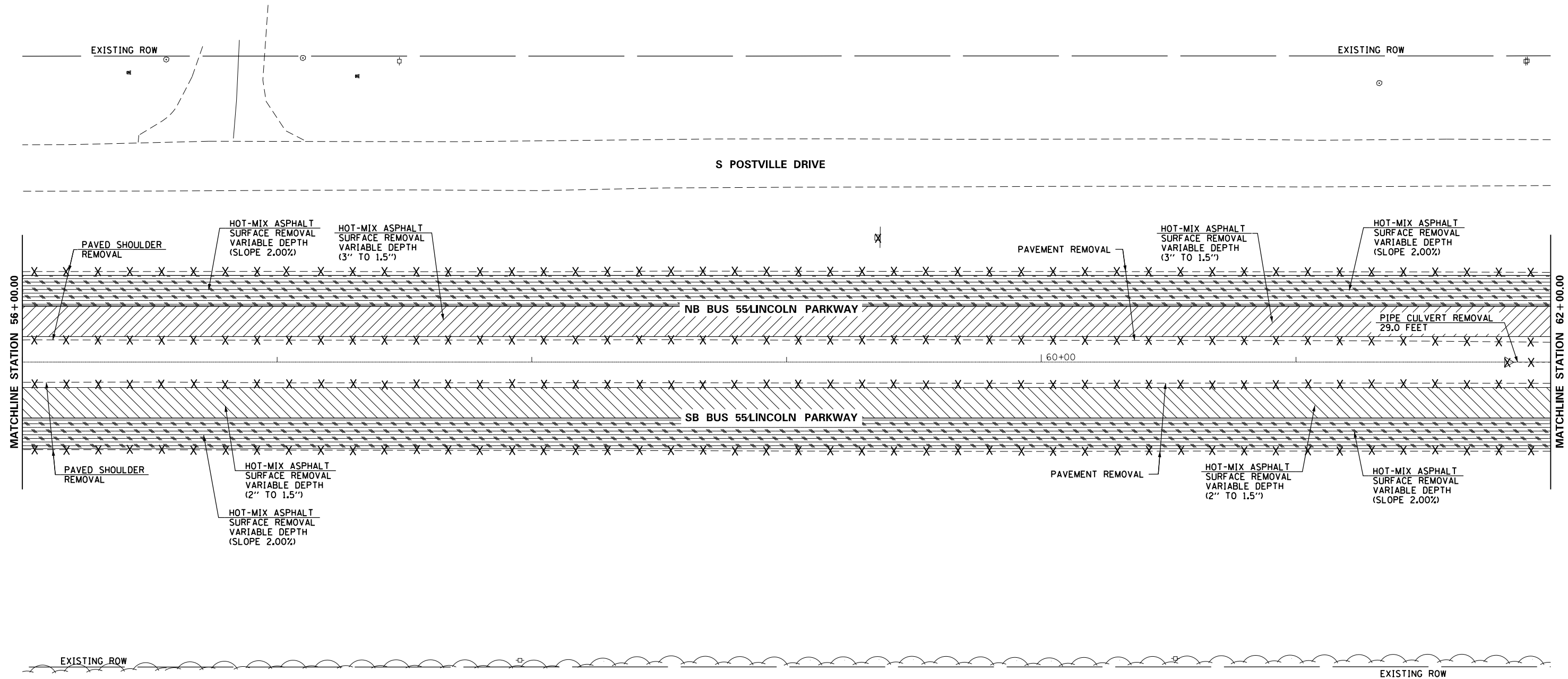
• (22)RS-6 & (23)RS-5,N,TS-1

HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND			
	BUTT JOINT		4.00% SLOPE
	1-1/2 INCH		VARIABLE DEPTH
	2 INCH		NB (3'' TO 1.5'')
	2.00% SLOPE		SB (2'' TO 1.5'')

REMOVAL LEGEND	
	MEDIAN REMOVAL
	PAVED SHOULDER REMOVAL
	PIPE CULVERT REMOVAL
	SIGN REMOVAL
	REMOVAL ITEMS

NOTES:

- SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
- SEE TRAFFIC SIGNAL PLANS FOR ADDITIONAL INFORMATION ON TRAFFIC SIGNAL ITEMS TO BE REMOVED.
- NB PASSING LANE HMA SURFACE REMOVAL SHALL BE 3" AT THE CENTERLINE OF LANES TO 1.5" AT THE INSIDE OF THE LANE.
- SB PASSING LANE HMA SURFACE REMOVAL SHALL BE 2" AT THE CENTERLINE OF LANES TO 1.5" AT THE INSIDE OF THE LANE.
- SEE DRAINAGE AND UTILITIES PLANS FOR REMOVAL OF EXISTING DRAINAGE ITEMS.



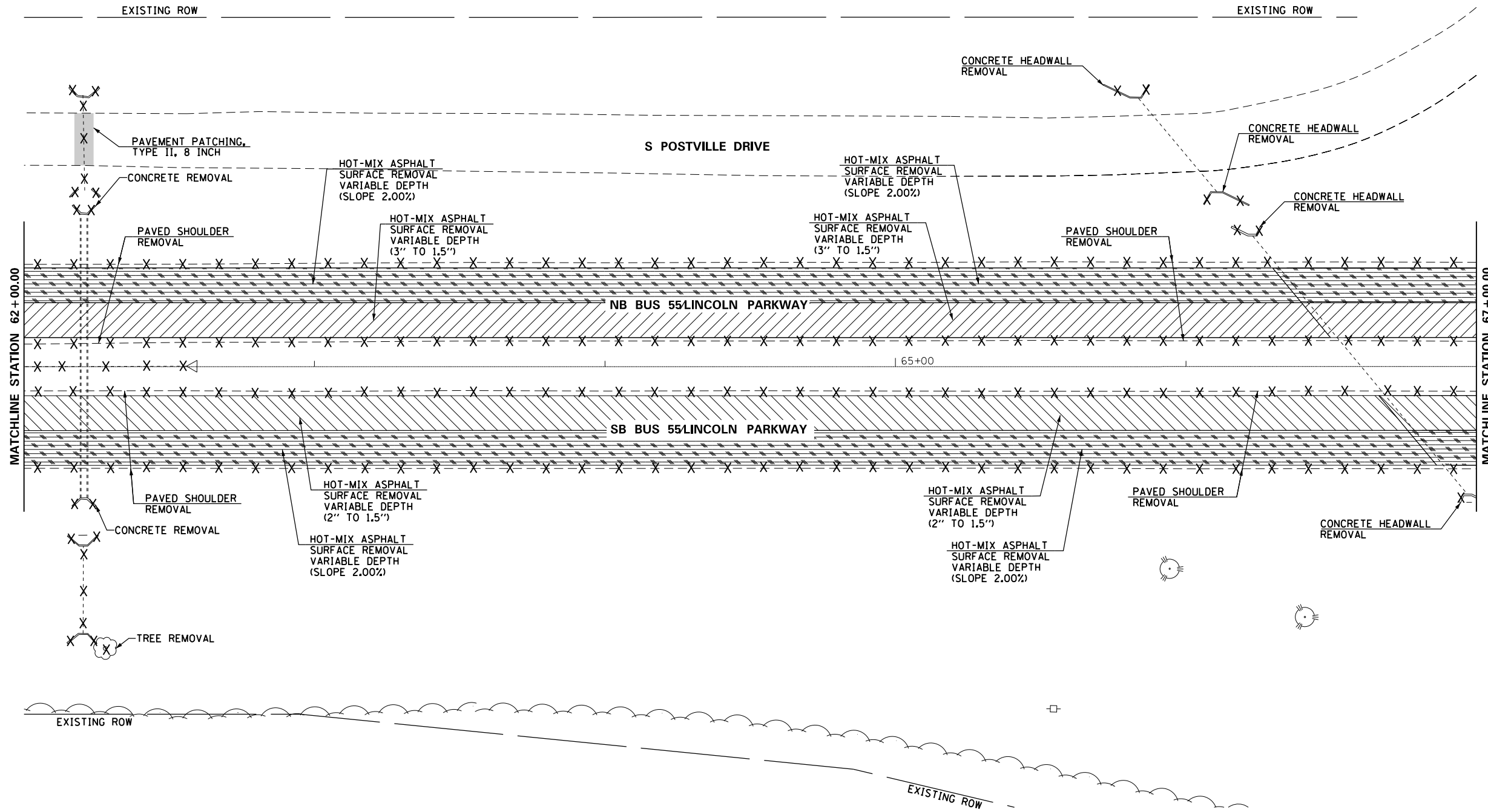
FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVAL PLAN			F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
DRAWN					REVISED -	SCALE: 1" = 20'	SHEET	OF	SHEETS	STA. 56+00.00	TO STA. 62+00.00	LOGAN	343	93
PLOT SCALE = 40.00' / in.					CHECKED -	REVISED -	CONTRACT NO. 72E02							
PLOT DATE = 10/21/2016					DATE -	REVISED -	ILLINOIS FED. AID PROJECT							

HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND			
	BUTT JOINT		4.00% SLOPE
	1-1/2 INCH		VARIABLE DEPTH
	2 INCH		NB (3" TO 1.5")
	2.00% SLOPE		SB (2" TO 1.5")

REMOVAL LEGEND	
	MEDIAN REMOVAL
	PAVED SHOULDER REMOVAL
	PIPE CULVERT REMOVAL
	SIGN REMOVAL
	REMOVAL ITEMS

NOTES:

1. SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
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FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVAL PLAN			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 6\Projects\0672EDR\Drawings\EA0\Sheets\0672E02-sht-07-rev-01.dwg		CHECKED -	REVISED -		7706	.	LOGAN	343	94			
PLOT SCALE = 48.00' / in.		DATE -	REVISED -		CONTRACT NO. 72E02							
PLOT DATE = 10/21/2016					ILLINOIS FED. AID PROJECT							

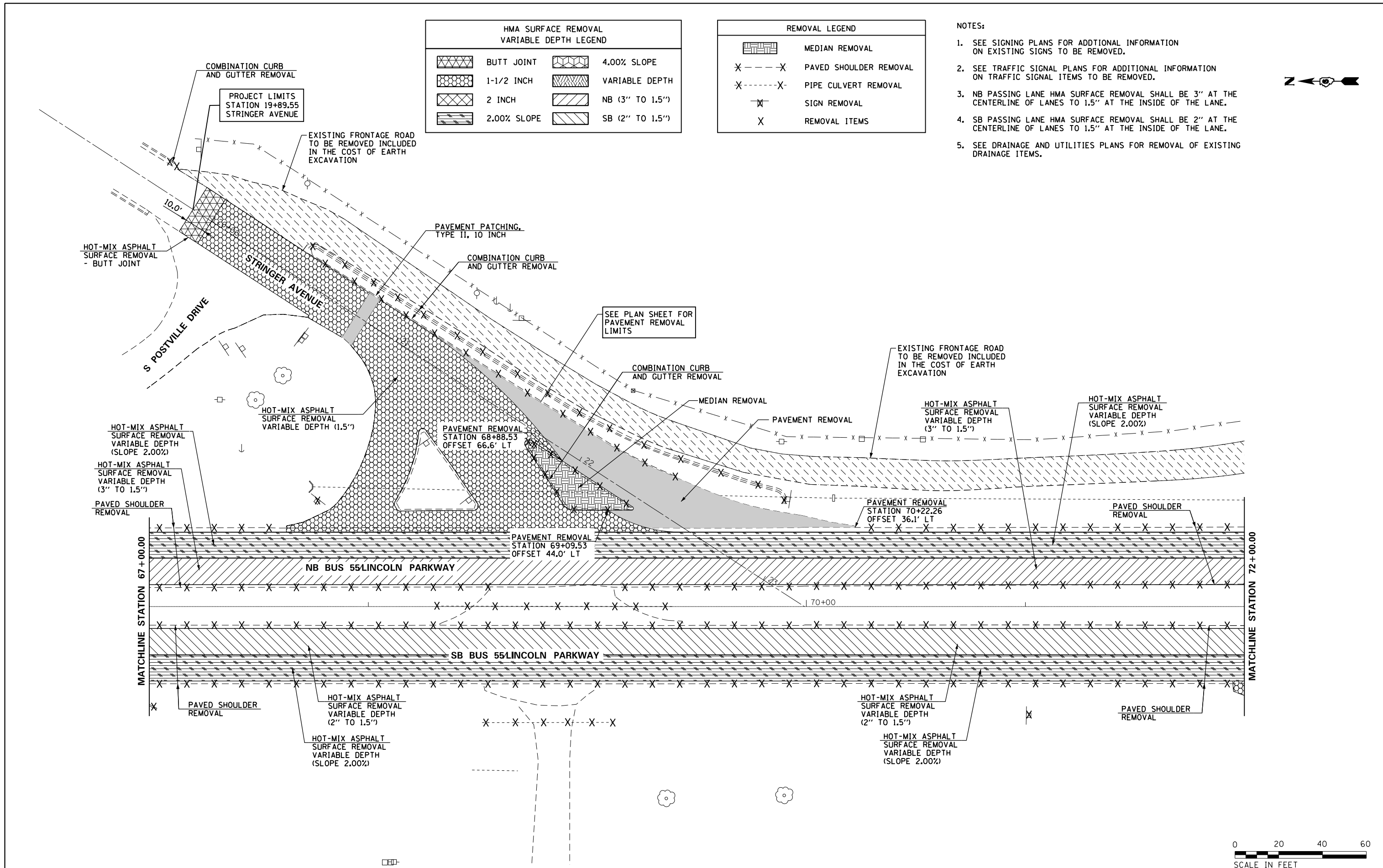
(22)RS-6 & (23)RS-5,N,TS-1

HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND			
	BUTT JOINT		4.00% SLOPE
	1-1/2 INCH		VARIABLE DEPTH
	2 INCH		NB (3" TO 1.5")
	2.00% SLOPE		SB (2" TO 1.5")

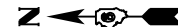
REMOVAL LEGEND	
	MEDIAN REMOVAL
X - - - X	PAVED SHOULDER REMOVAL
X - - - - X	PIPE CULVERT REMOVAL
	SIGN REMOVAL
X	REMOVAL ITEMS

NOTES:

1. SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
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FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVAL PLAN	F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\1\084EBIDINTEG.illinois.gov\PIWIDT\Documents\DOT Offices\District 6\Projects\0672EDR\Drawings\672E02-sht-07-rev-01.dwg	DRAWN -	CHECKED -	REVISED -			7706	.	LOGAN	343	95	
PLOT SCALE = 40.00' / in.		DATE -	REVISED -			CONTRACT NO. 72E02					
PLOT DATE = 10/21/2016						ILLINOIS FED. AID PROJECT					

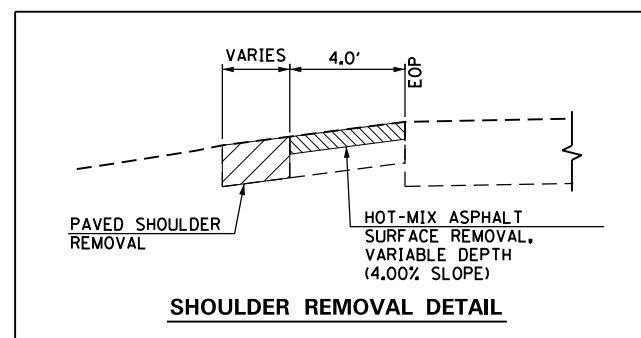
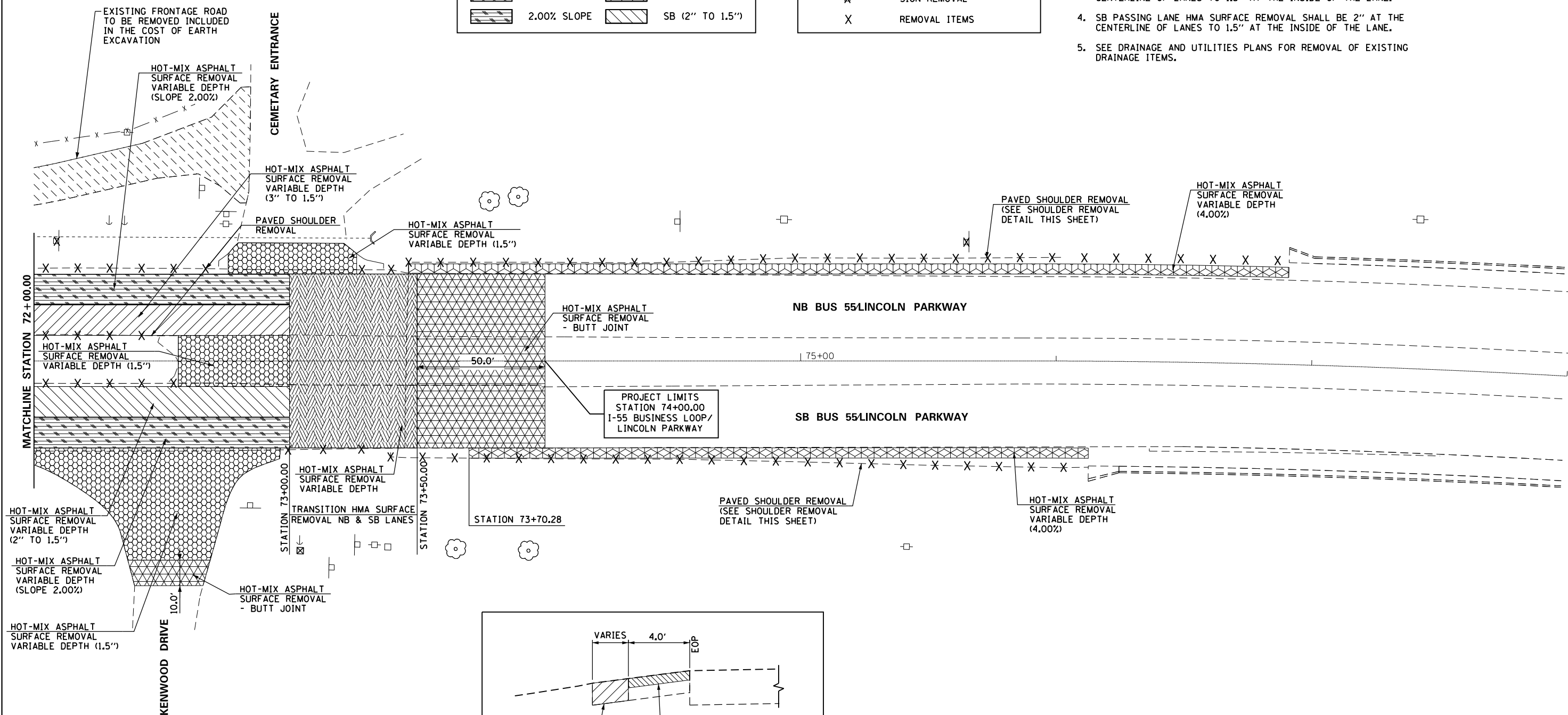


HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND			
	BUTT JOINT		4.00% SLOPE
	1-1/2 INCH		VARIABLE DEPTH
	2 INCH		NB (3" TO 1.5")
	2.00% SLOPE		SB (2" TO 1.5")

REMOVAL LEGEND	
	MEDIAN REMOVAL
	PAVED SHOULDER REMOVAL
	PIPE CULVERT REMOVAL
	SIGN REMOVAL
	REMOVAL ITEMS

NOTES:

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FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVAL PLAN			F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 6\Projects\0672EDR\Drawings\EA0\Sheets\0672E02-sht-07-rev-01.dwg		CHECKED -	REVISED -		7706		LOGAN	343	96			
PLOT SCALE = 40.00' / in.		DATE -	REVISED -		CONTRACT NO. 72E02							
PLOT DATE = 10/21/2016					ILLINOIS FED. AID PROJECT							

SCALE: 1" = 20' SHEET OF SHEETS STA. 72+00.00 TO STA. 78+00.00

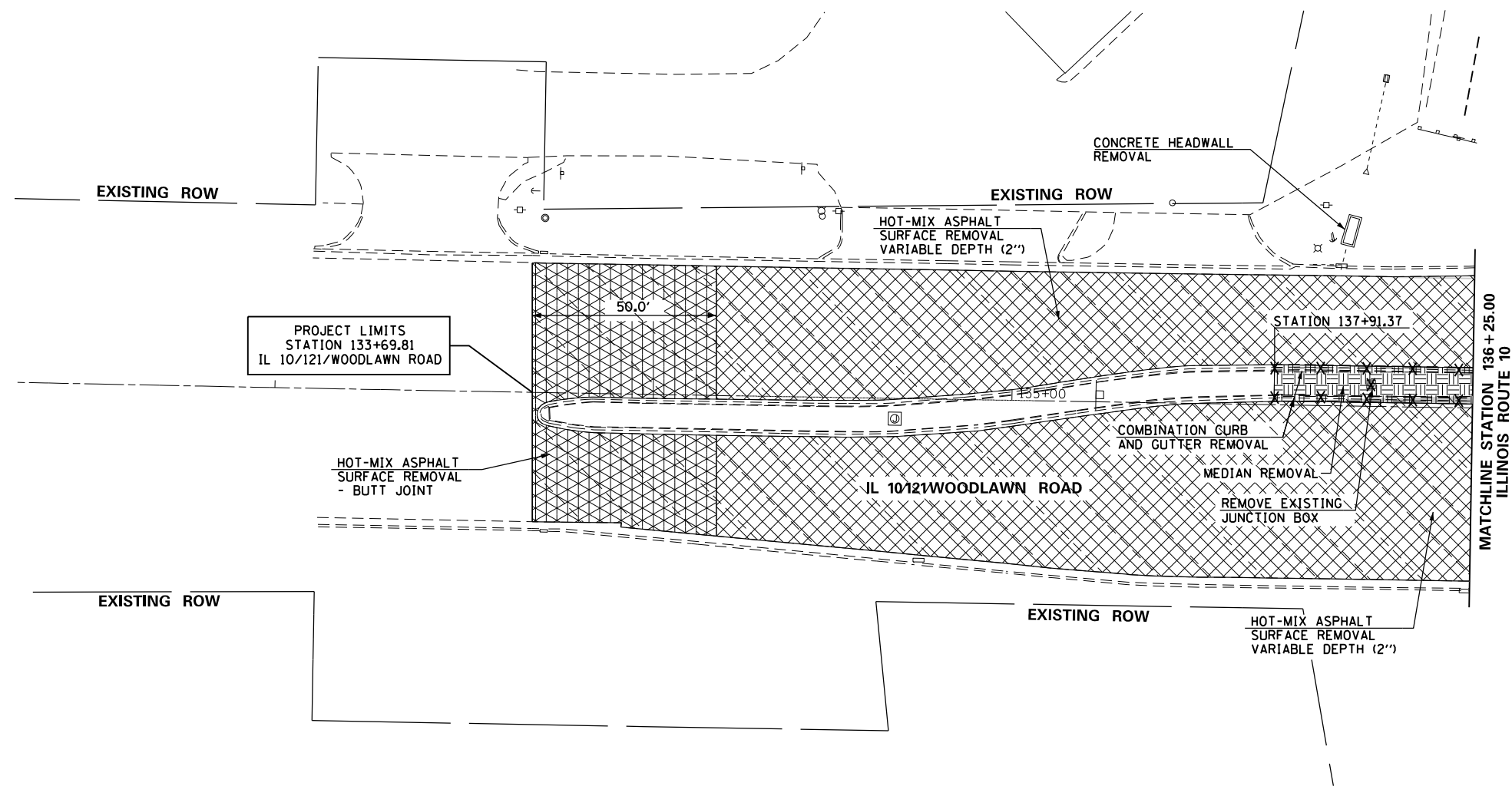


HMA SURFACE REMOVAL VARIABLE DEPTH LEGEND			
	BUTT JOINT		4.00% SLOPE
	1-1/2 INCH		VARIABLE DEPTH
	2 INCH		NB (3" TO 1.5")
	2.00% SLOPE		SB (2" TO 1.5")

REMOVAL LEGEND	
	MEDIAN REMOVAL
X - - - - X	PAVED SHOULDER REMOVAL
X - - - - - X	PIPE CULVERT REMOVAL
	SIGN REMOVAL
X	REMOVAL ITEMS

NOTES:

1. SEE SIGNING PLANS FOR ADDITIONAL INFORMATION ON EXISTING SIGNS TO BE REMOVED.
2. SEE TRAFFIC SIGNAL PLANS FOR ADDITIONAL INFORMATION ON TRAFFIC SIGNAL ITEMS TO BE REMOVED.
3. SEE DRAINAGE AND UTILITIES PLANS FOR REMOVAL OF EXISTING DRAINAGE ITEMS.



FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -
p:\IL\084EBID\INTEG\Illinois.gov\PWIDOT\Documents\IDOT Offices\District 6\Projects\0672\Drawings\EA0\Sheets\0672E02-sht-07-rev		DRAWN -	REVISED -
	PLOT SCALE = 40.00' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/21/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVAL PLAN

SCALE: 1" = 20' SHEET OF SHEETS STA. 132+00.00 TO STA. 136+50.00

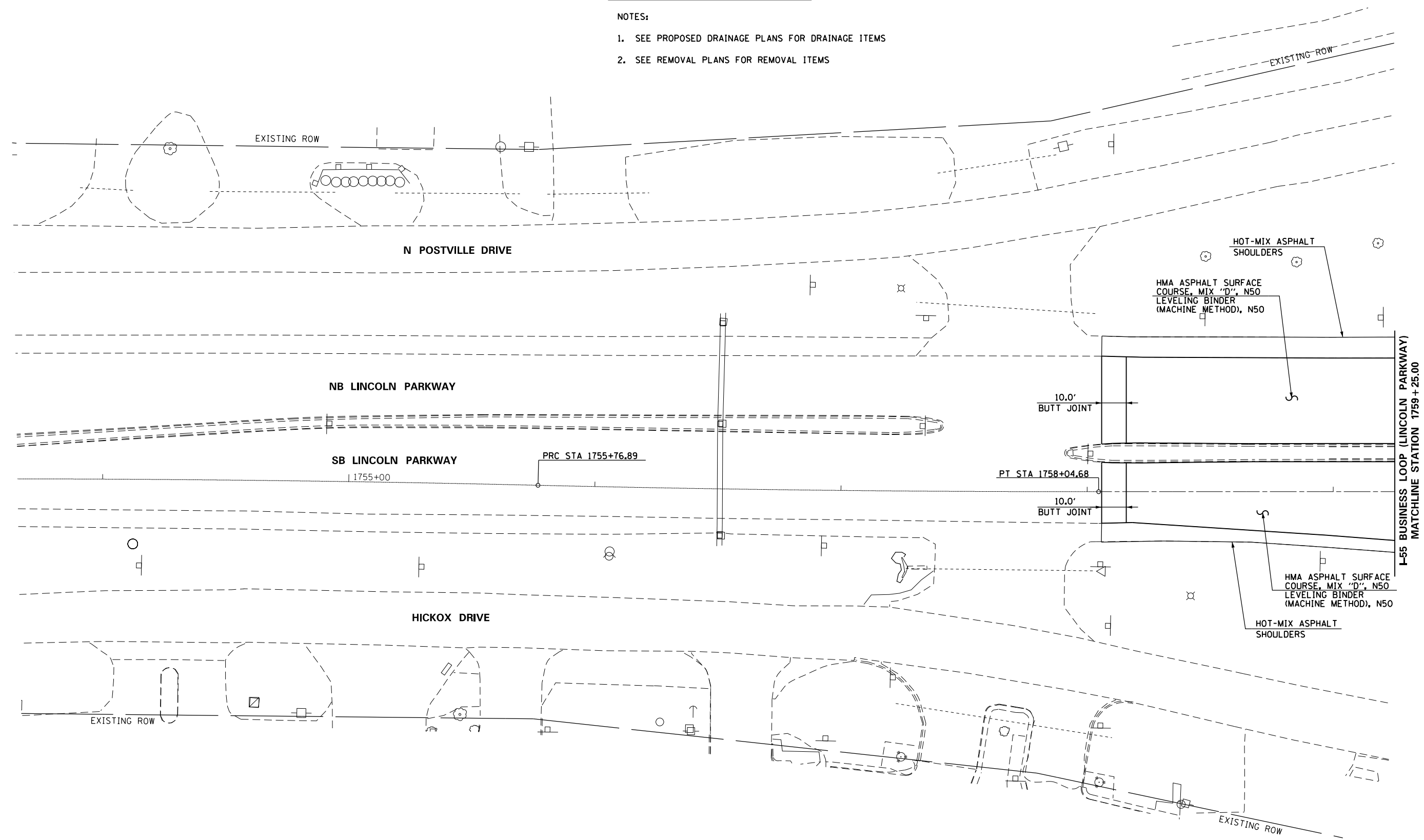
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7706	.	LOGAN	343	97
CONTRACT NO. 72E02			ILLINOIS FED. AID PROJECT	



LEGEND

BASE COURSE WIDENING

- NOTES:**
1. SEE PROPOSED DRAINAGE PLANS FOR DRAINAGE ITEMS
 2. SEE REMOVAL PLANS FOR REMOVAL ITEMS



FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN I-55 BUSINESS LOOP (LINCOLN PARKWAY)	F.A.U. R.E. =	SECTION =	COUNTY =	TOTAL SHEETS =	SHEET NO. =
pw:\IL\084EBIDINTEG.illinois.gov\PWIDOT\Documents\IDOT Offices\District 6\Projects\0672EDR\Drawings\EA0\Drawings\0672E02-sht-08A-plan.dwg	DESIGNED -	REVISED -	7706			.	LOGAN	343	99	
PLOT SCALE = 40.00' / in.	CHECKED -	REVISED -	CONTRACT NO. 72E02							
PLOT DATE = 10/21/2016	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							

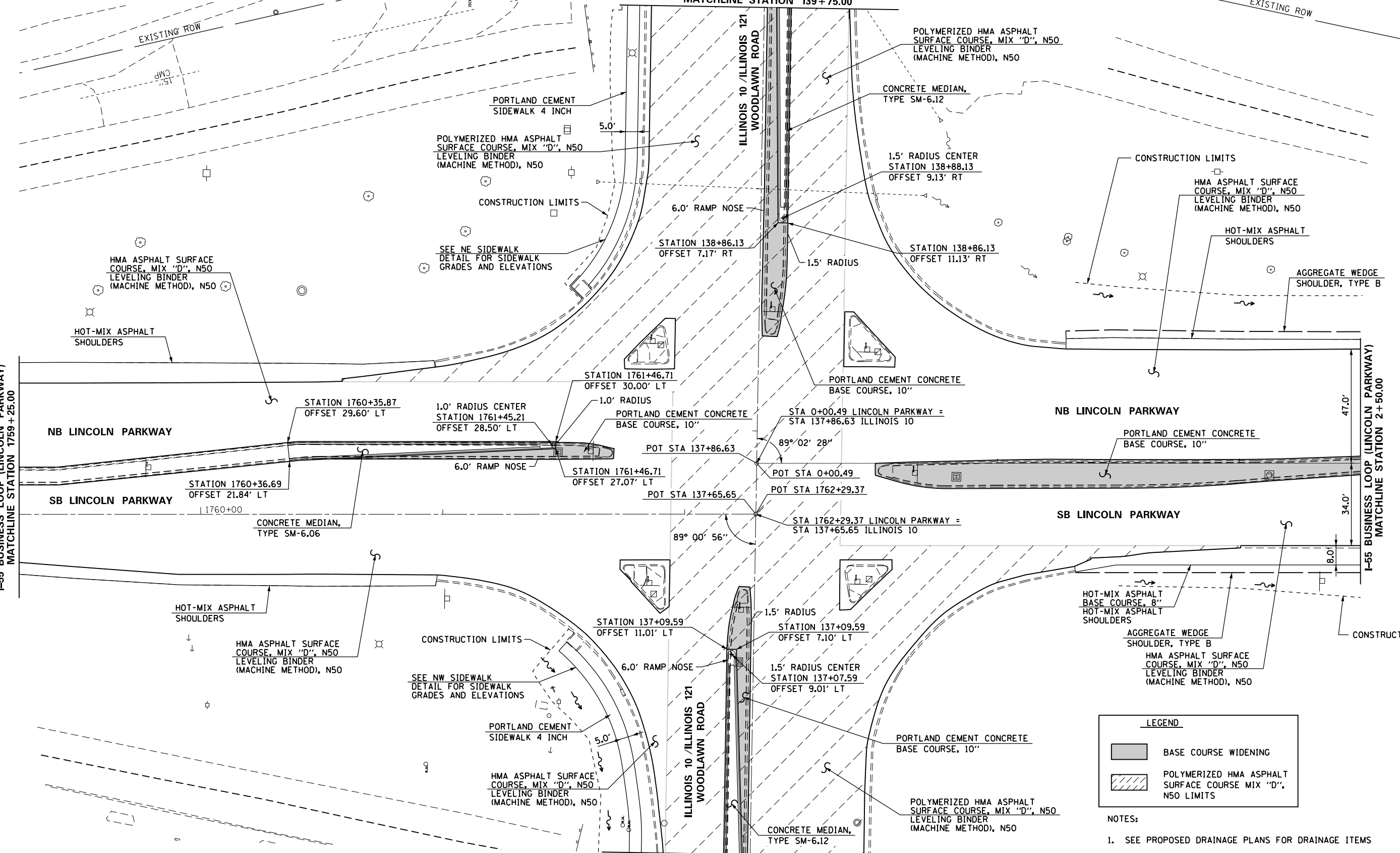
SCALE: 1" = 20' SHEET OF SHEETS STA. 1754+00.00 TO STA. 1759+25.00

(22)RS-6 & (23)RS-5,N,TS-1



IL 10 /IL 121 /WOODLAWN ROAD
MATCHLINE STATION 139+75.00

EXISTING ROW



I-55 BUSINESS LOOP (LINCOLN PARKWAY)
MATCHLINE STATION 1759+25.00

I-55 BUSINESS LOOP (LINCOLN PARKWAY)
MATCHLINE STATION 2+50.00

LEGEND

- BASE COURSE WIDENING
- POLYMERIZED HMA ASPHALT SURFACE COURSE MIX "D", N50 LIMITS

- NOTES:**
- SEE PROPOSED DRAINAGE PLANS FOR DRAINAGE ITEMS
 - SEE REMOVAL PLANS FOR REMOVAL ITEMS



IL 10 /IL 121 /WOODLAWN ROAD
MATCHLINE STATION 136+25.00

FILE NAME =	USER NAME = whitlingme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN I-55 BUSINESS LOOP (LINCOLN PARKWAY)			F.A.U. RTE. 7706	SECTION .	COUNTY LOGAN	TOTAL SHEETS 343	SHEET NO. 100
DRAWN -					SCALE: 1" = 20'			SHEET OF SHEETS STA. 1759+25.00 TO STA. 2+50.00			CONTRACT NO. 72E02	
PLOT SCALE = 40.00' / in.								ILLINOIS FED. AID PROJECT				

(22)RS-6, MED & (23) RS-5