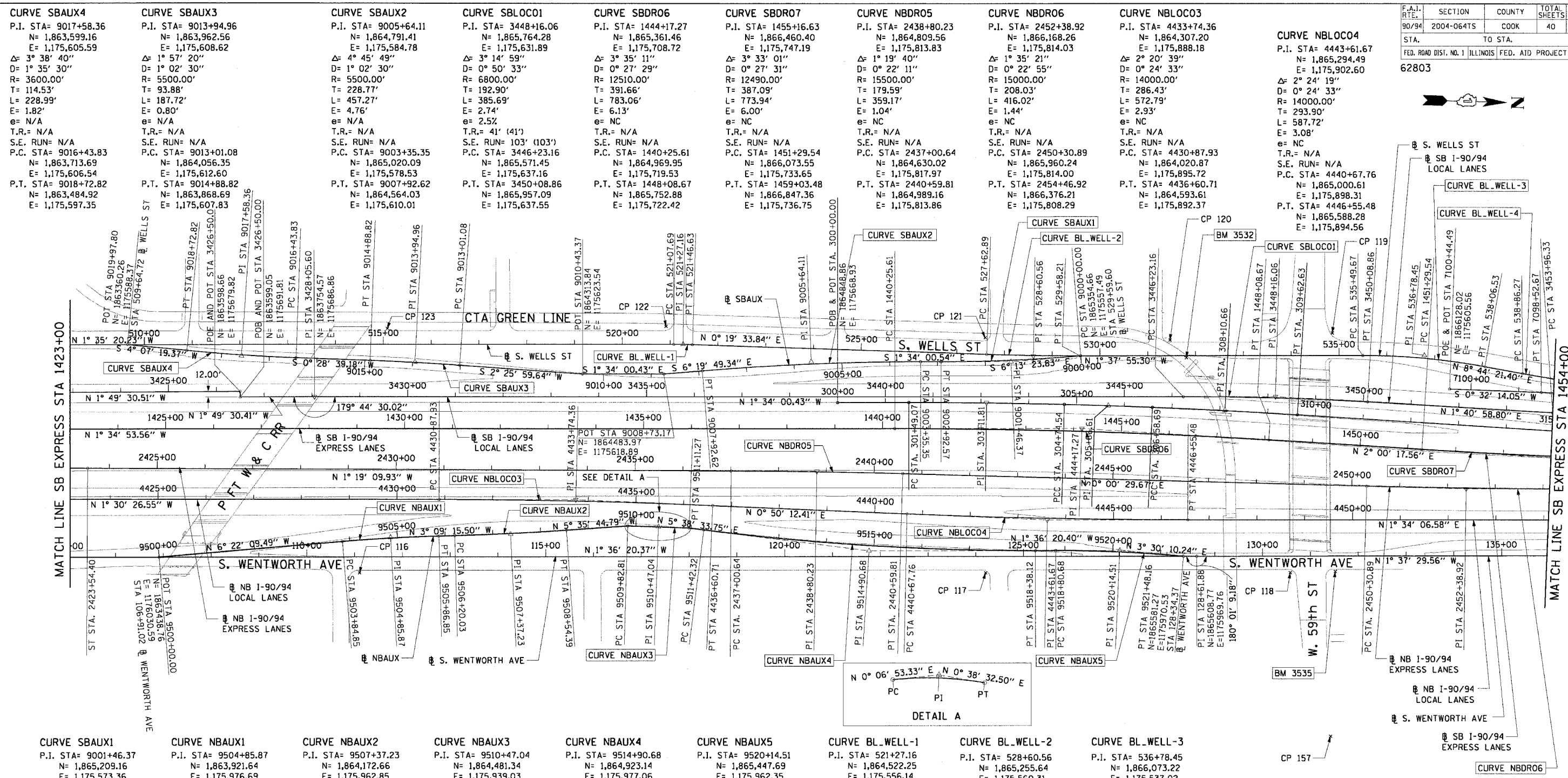


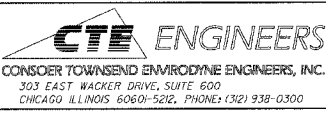
F.A.I. RT#	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2004-064TS	COOK	40	11
STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		
62803				



CURVE SBAUX1 P.I. STA= 9001+46.37 N= 1,865,209.16 E= 1,175,573.36 Δ= 4° 39' 23" D= 1° 35' 30" R= 3600.00' T= 146.37' L= 292.57' E= 2.97' e= N/A T.R.= N/A S.E. RUN= N/A P.C. STA= 9000+00.00 N= 1,865,354.66 E= 1,175,557.49 P.T. STA= 9002+92.57 N= 1,865,062.84 E= 1,175,577.36	CURVE NBAUX1 P.I. STA= 9504+85.87 N= 1,863,921.64 E= 1,175,976.69 Δ= 3° 12' 54" D= 1° 35' 30" R= 3600.00' T= 101.03' L= 202.00' E= 1.42' e= N/A T.R.= N/A S.E. RUN= N/A P.C. STA= 9503+84.85 N= 1,864,821.23 E= 1,175,987.89 P.T. STA= 9505+86.85 N= 1,864,022.51 E= 1,175,971.13	CURVE NBAUX2 P.I. STA= 9507+37.23 N= 1,864,172.66 E= 1,175,962.85 Δ= 2° 26' 29" D= 1° 02' 30" R= 5500.00' T= 117.20' L= 234.36' E= 1.25' e= N/A T.R.= N/A S.E. RUN= N/A P.C. STA= 9506+20.03 N= 1,864,055.64 E= 1,175,969.30 P.T. STA= 9508+54.39 N= 1,864,289.30 E= 1,175,951.42	CURVE NBAUX3 P.I. STA= 9510+47.04 N= 1,864,481.34 E= 1,175,939.03 Δ= 0° 31' 39" D= 0° 24' 30" R= 13952.00' T= 64.23' L= 128.46' E= 0.15' e= N/A T.R.= N/A S.E. RUN= N/A P.C. STA= 9509+82.81 N= 1,864,417.11 E= 1,175,938.90 P.T. STA= 9511+11.27 N= 1,864,271.36 E= 1,175,939.75	CURVE NBAUX4 P.I. STA= 9514+90.68 N= 1,864,923.14 E= 1,175,977.06 Δ= 7° 14' 54" D= 0° 24' 30" R= 5500.00' T= 348.36' L= 695.79' E= 11.02' e= N/A T.R.= N/A S.E. RUN= N/A P.C. STA= 9511+42.32 N= 1,864,576.47 E= 1,175,942.80 P.T. STA= 9518+38.12 N= 1,865,271.36 E= 1,175,967.30	CURVE NBAUX5 P.I. STA= 9520+14.51 N= 1,865,447.69 E= 1,175,962.35 Δ= 5° 06' 31" D= 1° 54' 35" R= 3000.00' T= 133.83' L= 267.48' E= 2.98' e= 2.4 T.R.= 11' (0') S.E. RUN= 66' (0') P.C. STA= 9518+80.68 N= 1,865,313.91 E= 1,175,966.10 P.T. STA= 9521+48.16 N= 1,865,581.27 E= 1,175,970.53	CURVE BL_WELL-1 P.I. STA= 521+27.16 N= 1,864,522.25 E= 1,175,556.14 Δ= 1° 54' 54" D= 4° 55' 05" R= 1165.00' T= 19.47' L= 38.94' E= 0.16' e= N/A T.R.= N/A S.E. RUN= N/A P.C. STA= 521+07.69 N= 1,864,502.79 E= 1,175,556.68 P.T. STA= 521+46.63 N= 1,865,541.72 E= 1,175,556.25	CURVE BL_WELL-2 P.I. STA= 528+60.56 N= 1,865,255.64 E= 1,175,560.31 Δ= 1° 57' 29" D= 1° 00' 09" R= 5715.00' T= 97.67' L= 195.31' E= 0.83' e= N/A T.R.= N/A S.E. RUN= N/A P.C. STA= 527+62.89 N= 1,865,157.98 E= 1,175,559.75 P.T. STA= 529+58.21 N= 1,865,353.27 E= 1,175,557.53	CURVE BL_WELL-3 P.I. STA= 536+78.45 N= 1,866,073.22 E= 1,175,537.02 Δ= 10° 22' 17" D= 4° 02' 16" R= 1419.00' T= 128.78' L= 256.86' E= 5.83' e= N/A T.R.= N/A S.E. RUN= N/A P.C. STA= 535+49.67 N= 1,865,944.49 E= 1,175,540.68 P.T. STA= 538+06.53 N= 1,866,200.51 E= 1,175,556.58
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NOTES:

- AVERAGE GRID TO GROUND CONVERSION FACTOR = 1.000010988654360
- FOR BENCH MARK INFORMATION, SEE SHEET 1 OF ALIGNMENT PLAN SHEETS.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
 TRAFFIC SIGNALS AT 51ST STREET AND
 WENTWORTH AVENUE / WELLS STREET

ALIGNMENT PLAN

10/25/2004 03:27:42 PM