

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2004-064TS	COOK	40	13
STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		
62803				



**CURVE C4701**  
 P.I. STA= 8068+34.90  
 N= 1,872,302.10  
 E= 1,175,769.16  
 $\Delta$ = 2° 04' 05"  
 D= 1° 08' 45"  
 R= 5000.00'  
 T= 90.24'  
 L= 180.47'  
 E= 0.81'  
 e= N/A  
 T.R.= N/A  
 S.E. RUN= N/A  
 P.C. STA= 8067+44.66  
 N= 1,872,211.86  
 E= 1,175,768.33  
 P.T. STA= 8069+25.12  
 N= 1,872,392.31  
 E= 1,175,766.72

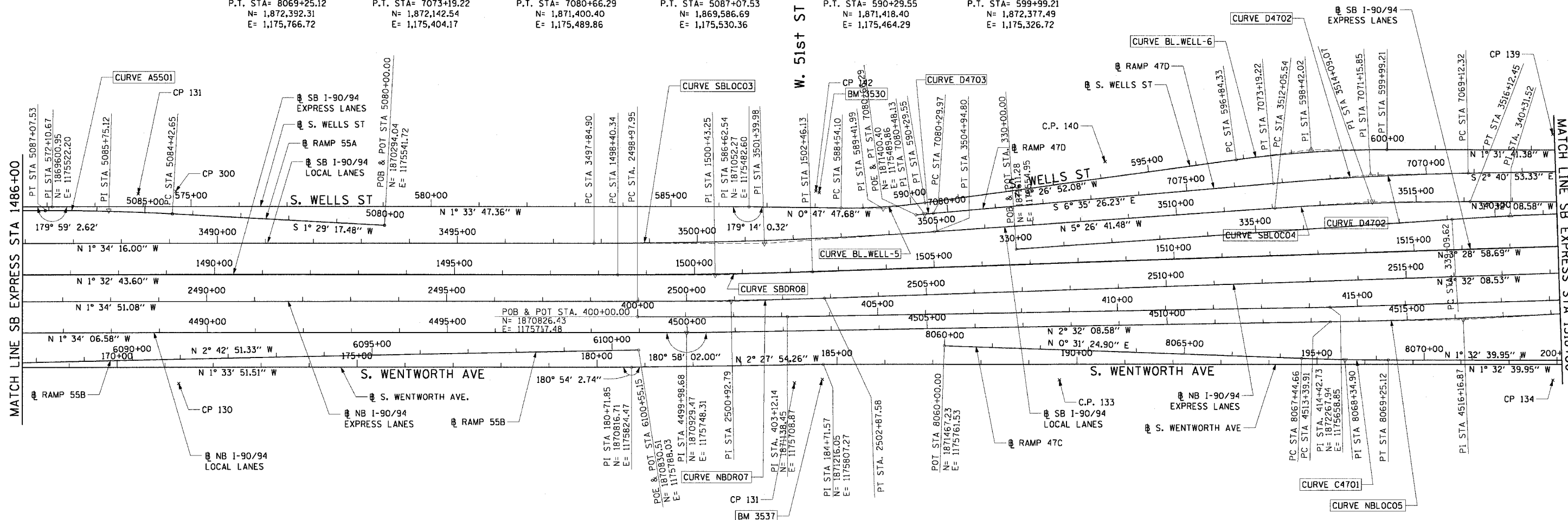
**CURVE D4702**  
 P.I. STA= 7071+15.85  
 N= 1,872,344.72  
 E= 1,175,380.81  
 $\Delta$ = 3° 54' 33"  
 D= 0° 57' 38"  
 R= 5964.00'  
 T= 203.53'  
 L= 406.91'  
 E= 3.47'  
 e= N/A  
 T.R.= N/A  
 S.E. RUN= N/A  
 P.C. STA= 7069+12.32  
 N= 1,872,548.03  
 E= 1,175,371.29  
 P.T. STA= 7073+19.22  
 N= 1,872,142.54  
 E= 1,175,404.17

**CURVE D4703**  
 P.I. STA= 7080+48.13  
 N= 1,871,418.45  
 E= 1,175,487.83  
 $\Delta$ = 0° 11' 56"  
 D= 0° 32' 51"  
 R= 10464.00'  
 T= 18.16'  
 L= 36.32'  
 E= 0.02'  
 e= N/A  
 T.R.= N/A  
 S.E. RUN= N/A  
 P.C. STA= 7080+29.97  
 N= 1,871,436.49  
 E= 1,175,485.75  
 P.T. STA= 7080+66.29  
 N= 1,871,400.40  
 E= 1,175,489.86

**CURVE A5501**  
 P.I. STA= 5085+75.12  
 N= 1,869,719.11  
 E= 1,175,526.79  
 $\Delta$ = 3° 02' 07"  
 D= 1° 08' 45"  
 R= 5000.00'  
 T= 132.48'  
 L= 264.89'  
 E= 1.75'  
 e= N/A  
 T.R.= N/A  
 S.E. RUN= N/A  
 P.C. STA= 5084+42.65  
 N= 1,869,851.55  
 E= 1,175,530.23  
 P.T. STA= 5087+07.53  
 N= 1,869,586.69  
 E= 1,175,530.36

**CURVE BL.WELL-5**  
 P.I. STA= 589+41.99  
 N= 1,871,331.70  
 E= 1,175,478.71  
 $\Delta$ = 8° 39' 04"  
 D= 4° 55' 51"  
 R= 1162.00'  
 T= 87.89'  
 L= 175.45'  
 E= 3.32'  
 e= N/A  
 T.R.= N/A  
 S.E. RUN= N/A  
 P.C. STA= 588+54.10  
 N= 1,871,243.82  
 E= 1,175,479.94  
 P.T. STA= 590+29.55  
 N= 1,871,418.40  
 E= 1,175,464.29

**CURVE BL.WELL-6**  
 P.I. STA= 598+42.02  
 N= 1,872,219.86  
 E= 1,175,330.92  
 $\Delta$ = 7° 55' 11"  
 D= 2° 30' 55"  
 R= 2278.00'  
 T= 157.69'  
 L= 314.87'  
 E= 5.45'  
 e= N/A  
 T.R.= N/A  
 S.E. RUN= N/A  
 P.C. STA= 596+84.33  
 N= 1,872,064.31  
 E= 1,175,356.80  
 P.T. STA= 599+99.21  
 N= 1,872,377.49  
 E= 1,175,326.72



**CURVE SBLOC03**  
 P.I. STA= 3501+39.98  
 N= 1,871,086.59  
 E= 1,175,557.00  
 $\Delta$ = 3° 52' 25"  
 D= 0° 32' 44"  
 R= 10500.00'  
 T= 355.09'  
 L= 709.90'  
 E= 6.00'  
 e= RC  
 T.R.= 96' (96')  
 S.E. RUN= 62' (62')  
 P.C. STA= 3497+84.90  
 N= 1,870,731.64  
 E= 1,175,566.73  
 P.T. STA= 3504+94.80  
 N= 1,871,440.08  
 E= 1,175,523.31

**CURVE SBLOC04**  
 P.I. STA= 3514+09.07  
 N= 1,872,350.23  
 E= 1,175,436.55  
 $\Delta$ = 3° 54' 33"  
 D= 0° 57' 39"  
 R= 5964.00'  
 T= 203.53'  
 L= 406.91'  
 E= 3.47'  
 e= 2.8%  
 T.R.= 41' (41')  
 S.E. RUN= 115' (115')  
 P.C. STA= 3512+05.54  
 N= 1,872,147.61  
 E= 1,175,455.86  
 P.T. STA= 3516+12.45  
 N= 1,872,553.69  
 E= 1,175,431.10

**CURVE SBDR08**  
 P.I. STA= 1500+43.25  
 N= 1,870,985.63  
 E= 1,175,625.10  
 $\Delta$ = 1° 56' 15"  
 D= 0° 28' 39"  
 R= 12000.00'  
 T= 202.92'  
 L= 405.79'  
 E= 1.72'  
 e= NC  
 T.R.= N/A  
 S.E. RUN= N/A  
 P.C. STA= 1498+40.34  
 N= 1,870,782.79  
 E= 1,175,630.57  
 P.T. STA= 1502+46.13  
 N= 1,871,188.17  
 E= 1,175,612.77

**CURVE NBDRO7**  
 P.I. STA= 2500+92.79  
 N= 1,871,020.31  
 E= 1,175,680.12  
 $\Delta$ = 1° 57' 17"  
 D= 0° 30' 06"  
 R= 11420.00'  
 T= 194.84'  
 L= 389.63'  
 E= 1.66'  
 e= NC  
 T.R.= N/A  
 S.E. RUN= N/A  
 P.C. STA= 2498+97.95  
 N= 1,870,825.55  
 E= 1,175,685.50  
 P.T. STA= 2502+87.58  
 N= 1,871,214.78  
 E= 1,175,668.10

**CURVE NBLOC05**  
 P.I. STA= 4516+16.87  
 N= 1,872,546.08  
 E= 1,175,676.71  
 $\Delta$ = 2° 26' 28"  
 D= 0° 26' 27"  
 R= 13000.00'  
 T= 276.96'  
 L= 553.84'  
 E= 2.95'  
 e= NC  
 T.R.= N/A  
 S.E. RUN= N/A  
 P.C. STA= 4513+39.91  
 N= 1,872,269.39  
 E= 1,175,688.97  
 P.T. STA= 4518+93.75  
 N= 1,872,822.00  
 E= 1,175,652.69

**NOTES:**

1. AVERAGE GRID TO GROUND CONVERSION FACTOR = 1.000010988654360
2. FOR BENCH MARK INFORMATION, SEE SHEET 1 OF ALIGNMENT PLAN SHEETS.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 90/94 (DAN RYAN EXPRESSWAY)  
 TRAFFIC SIGNALS AT 51ST STREET AND  
 WENTWORTH AVENUE / WELLS STREET

**ALIGNMENT PLAN**

SCALE: 1"=100'  
 DATE: October 29, 2004

DRAWN BY: NJH/AMM  
 CHECKED BY: JAL/MS