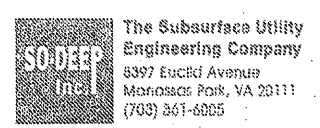


| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|-------------|--------|---------------------------|-----------|
| 704 | (LN & TS-1) | McLEAN | 497 | 201 |
| STA. N/A | | | TO STA. N/A | |
| FED. ROAD DIST. NO. | | | ILLINOIS FED. AID PROJECT | |
| | | | CONTRACT NO. 70514 | |



So-Deep Test Hole Certification Form - Metric

The Subsurface Utility Engineering Company
6397 Euclid Avenue
Manassas Park, VA 20111
(703) 861-6005

City, County, State: **McLEAN CO., IL**
Gen. Loc: **MORRIS AVE, S. OF SIX POINTS RD.**
Recorded Size/Material Type: **(1) 115MM STEEL GAS LINE**
Foreman/Truck#/Form By: **J. HARLIN / 221 JM. LOBE**

Control # **SILA663**
Test Hole # **19**
Plan Scale **1:250**
Street # **3**
Proposed **STORM X-ING**
Date **OCTOBER 18, 2006**

Condition of paving prior to work: **NO PAVING**

B.M. 1 Elev. = **251.463M**
is **GIVEN**

B.M. 2 Elev. = **250.891M**
is **CALCULATED**

Benchmarks check **BY B.865M**
Elevations are referenced to **B.M.#2**

Recorded Size/Type of utility **WAS FOUND**

There **WERE NO** additional utilities in the test hole

The utility **WAS** in good condition.

Paving Thickness and type **NO PAVE**

Color of ribbon installed **YELLOW**

Soil Type **DRY, HARD CLAY**

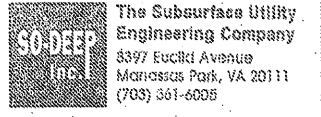
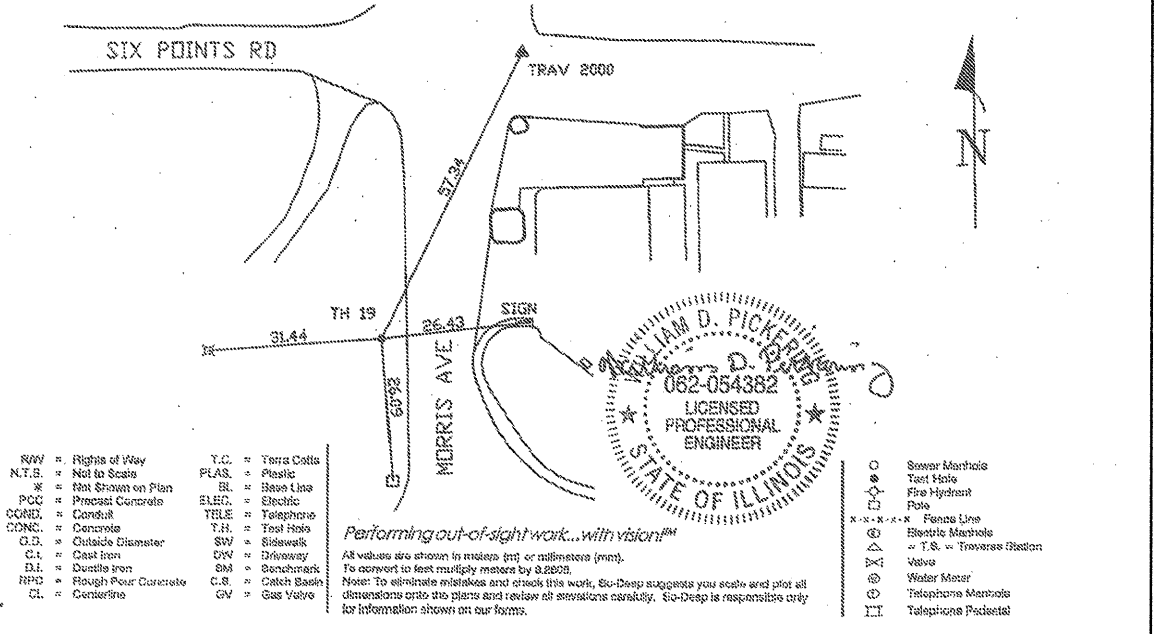
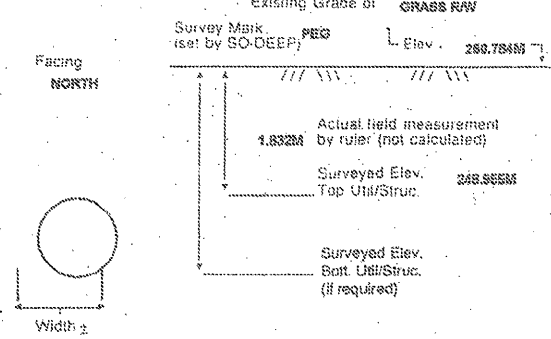
Field Condition **GRASS R/W**

T.H. tied to **PEG**

(1) **115MM COATED STEEL GAS LINE**
Size/Material Type
Portion of pipe exposed for O.D. measurement:
FULL

Remarks: **NONE**

So-Deep will attempt to use the SMH most applicable to your design. If however, B.Ms. differ by more than .05, resulting differences could cause design conflicts.



So-Deep Test Hole Certification Form - Metric

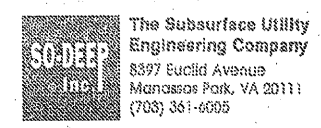
The Subsurface Utility Engineering Company
6397 Euclid Avenue
Manassas Park, VA 20111
(703) 861-6005

City, County, State: **McLEAN CO., IL**
Gen. Loc: **SPRINGFIELD AVE; E. OF HOUSE #2223**
Recorded Size/Material Type: **81MM STEEL GAS LINE**
Foreman/Truck#/Form By: **J. HARLIN / 221 JM. LOBE**

Control # **SILA033**
Test Hole # **2**

| | | | | |
|---------------------|------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (JN & TS-1 | MCLEAN | 497 | 202 |
| STA. N/A | | TO STA. N/A | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 70514



So-Deep Test Hole Certification Form - Metric

© So-Deep, Inc. 1988, 1994

Control # SILA063
 Test Hole # 21
 Plan Scale 1:250
 Sheet # 17
 Proposed GRADE CHANGE
 Date OCTOBER 20, 2009

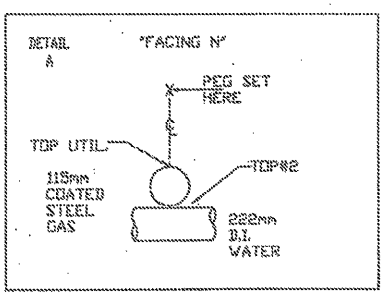
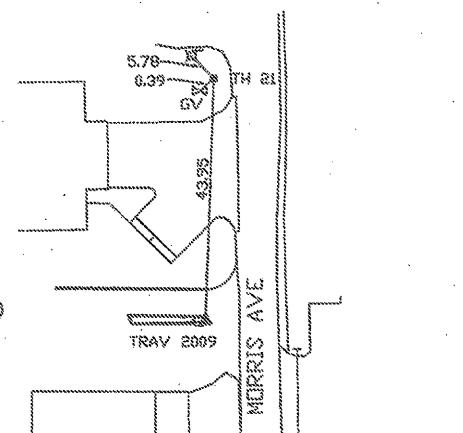
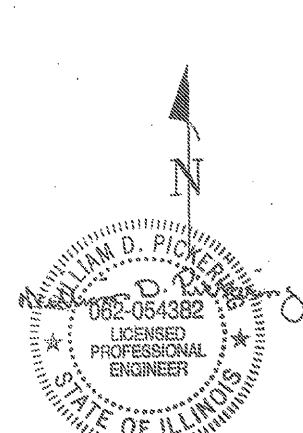
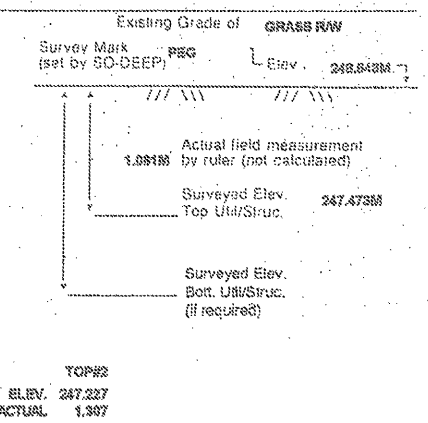
City, County, State MCLEAN CO., IL
 Gen. Loc. MORRIS AVE., N. OF GOOSE CREEK RD.
 Recorded Size/Material/Type 102MM STEEL GAS LINE & 203MM UNK TYPE WATER LINE
 Foreman/Truck/Form By J. HARLIN /221 JM. LOSE

B.M. 1 Elev. = 281.835M
 IS GIVEN
 B.M. 2 Elev. = 281.463M
 IS GIVEN
 Benchmarks check BY G.0898M
 Elevations are referenced to B.M.#1

Description: (TBM 13) TOP NUT OF HYDRANT; 7.50MS RT OF STA. 0+404.00 ±, S. MORRIS AVE.
 Description: (TRAV 2009) TOP OF RR SPIKE FOUND; 20.40M ± RT OF STA. 0+402.80 ± S. MORRIS AVE.

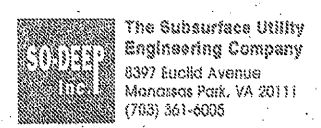
SO-DEEP will attempt to use the BMM#1 most applicable to your design. If however, 2Ms differ by more than .05, resulting differences could cause design conflicts.

Recorded Size/Type of utility WAS FOUND
 There WERE additional utilities in the test hole
 The utility WAS in good condition.
 Paving Thickness and type NO PAVE
 Color of ribbon installed BLUE, YELLOW
 Soil Type DRY, HARD
 Field Condition GRASS RW
 T.H. tied to PEG
 115MM COATED STEEL GAS & 223MM D.I. WATER
 Size/Material/Type
 Portion of pipe exposed for O.D. measurement:
 N/A
 Remarks: NONE



- R/W = Rights of Way
- N.T.S. = Not to Scale
- * = Not Shown on Plan
- P.C.C. = Precast Concrete
- COND. = Conduit
- CONC. = Concrete
- C.D. = Outside Diameter
- D.I. = Ductile Iron
- H.P.C. = Rough Four Concrete
- CL = Centerline
- T.C. = Terra Cotta
- PLAS. = Plastic
- BL = Base Line
- ELEC. = Electric
- TELE = Telephone
- T.H. = Test Hole
- SW = Sillwork
- DW = Driveway
- BM = Benchmark
- C.S. = Catch Basin
- GV = Gas Valve
- = Sewer Manhole
- = Test Hole
- = Fire Hydrant
- = Pole
- = Fence Line
- ⊙ = Electric Manhole
- ⊙ = T.S. = Traverse Station
- ∇ = Valve
- ⊙ = Water Meter
- ⊙ = Telephone Manhole
- ⊙ = Telephone Pedestal

Performing out-of-sight work...with vision!SM
 All values are shown in meters (m) or millimeters (mm).
 To convert to feet multiply meters by 3.2808.
 Note: To eliminate mistakes and check this work, So-Deep suggests you scale and plot all dimensions onto the plans and review all elevations carefully. So-Deep is responsible only for information shown on our forms.



So-Deep Test Hole Certification Form - Metric

© So-Deep, Inc. 1988, 1994

Control # SILA063
 Test Hole # 22
 Plan Scale 1:250
 Sheet # 17
 Proposed GRADE CHANGE
 Date OCTOBER 20, 2009

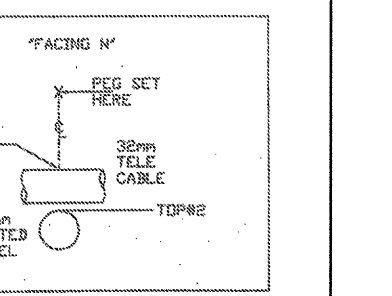
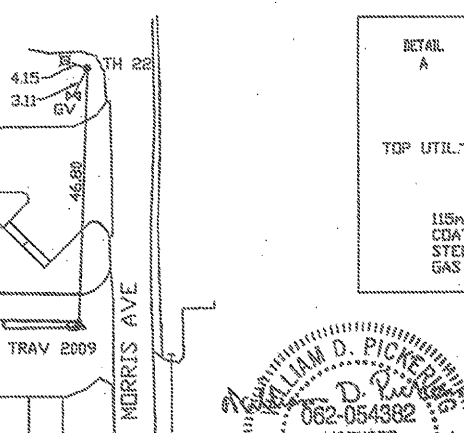
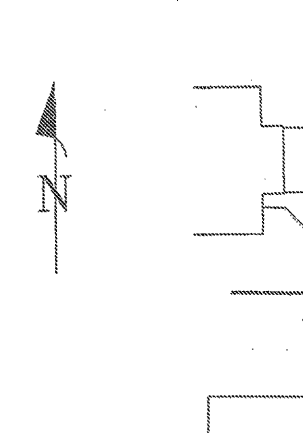
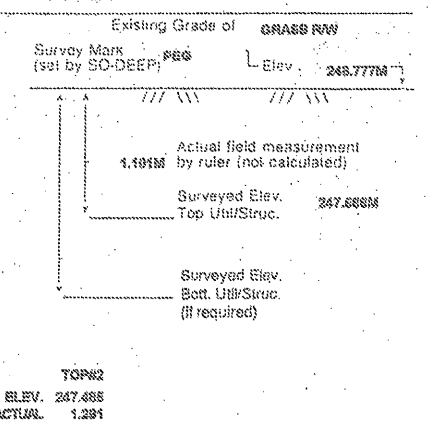
City, County, State MCLEAN CO., IL
 Gen. Loc. MORRIS AVE., N. OF GOOSE CREEK RD.
 Recorded Size/Material/Type (1) UNK. SIZE TELE. CABLE & 102MM STEEL GAS LINE
 Foreman/Truck/Form By J. HARLIN /221 JM. LOSE

B.M. 1 Elev. = 281.685M
 IS GIVEN
 B.M. 2 Elev. = 281.463M
 IS GIVEN
 Benchmarks check BY G.0898M
 Elevations are referenced to B.M.#1

Description: (TBM 13) TOP NUT OF HYDRANT; 7.50MS RT OF STA. 0+404.00 ±, S. MORRIS AVE.
 Description: (TRAV 2009) TOP OF RR SPIKE FOUND; 20.40M ± RT OF STA. 0+402.80 ± S. MORRIS AVE.

SO-DEEP will attempt to use the BMM#1 most applicable to your design. If however, 2Ms differ by more than .05, resulting differences could cause design conflicts.

Recorded Size/Type of utility WAS FOUND
 There WERE additional utilities in the test hole
 The utility WAS in good condition.
 Paving Thickness and type NO PAVE
 Color of ribbon installed ORANGE, YELLOW
 Soil Type DRY, HARD
 Field Condition GRASS RW
 T.H. tied to PEG
 (1) 32MM TELE. CABLE & 115MM COATED ST. GAS
 Size/Material/Type
 Portion of pipe exposed for O.D. measurement:
 N/A
 Remarks: NONE



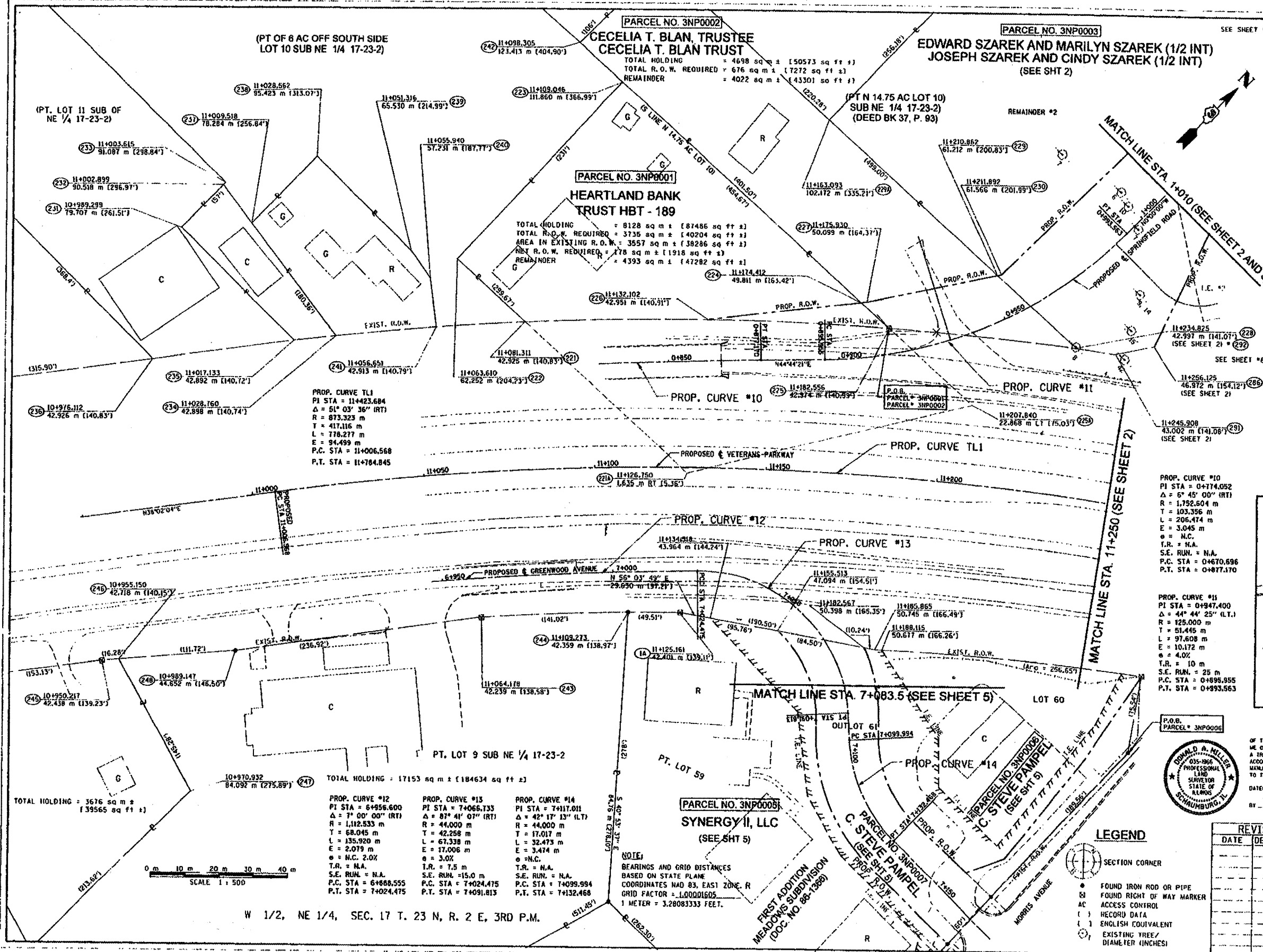
- R/W = Rights of Way
- N.T.S. = Not to Scale
- * = Not Shown on Plan
- P.C.C. = Precast Concrete
- COND. = Conduit
- CONC. = Concrete
- C.D. = Outside Diameter
- C.I. = Cast Iron
- D.I. = Ductile Iron
- R.P.C. = Rough Four Concrete
- CL = Centerline
- T.C. = Terra Cotta
- PLAS. = Plastic
- BL = Base Line
- ELEC. = Electric
- TELE = Telephone
- T.H. = Test Hole
- SW = Sillwork
- DW = Driveway
- BM = Benchmark
- C.S. = Catch Basin
- GV = Gas Valve
- = Sewer Manhole
- = Test Hole
- = Fire Hydrant
- = Pole
- = Fence Line
- ⊙ = Electric Manhole
- ⊙ = T.S. = Traverse Station
- ∇ = Valve
- ⊙ = Water Meter
- ⊙ = Telephone Manhole
- ⊙ = Telephone Pedestal

Performing out-of-sight work...with vision!SM
 All values are shown in meters (m) or millimeters (mm).
 To convert to feet multiply meters by 3.2808.
 Note: To eliminate mistakes and check this work, So-Deep suggests you scale and plot all dimensions onto the plans and review all elevations carefully. So-Deep is responsible only for information shown on our forms.

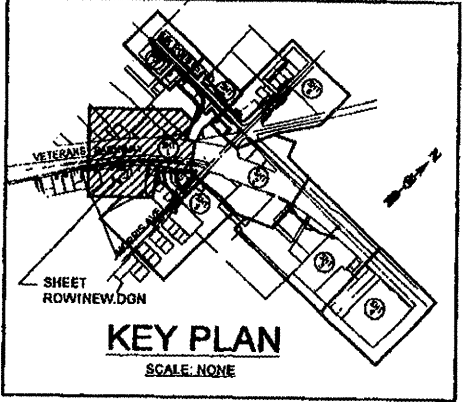
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SUBSURFACE UTILITY
 ENGINEERING DETAILS
 SHEET 11 OF 11
 DRAWN BY SAM
 CHECKED BY PMH
 DATE 5/27/09

RECORDED AS DOCUMENT NO.



| | | | |
|------|--------|---------------------------|--------------------------|
| (19) | (230) | S 01° 09' 45" W | 52.945 m (173.70') |
| (20) | (222) | S 88° 11' 52" W | 26.938 m (88.38') |
| (21) | (221A) | N 88° 11' 52" E | 64.400 m (211.29') |
| (22) | (223) | N 01° 31' 16" W | 70.394 m (230.95') |
| (23) | (224) | N 87° 52' 26" E | 94.565 m (310.25') |
| (24) | (225) | S 87° 52' 26" W | 16.793 m (55.10') |
| (25) | (226) | N 87° 52' 26" E | 10.968 m (35.99') |
| (26) | (227) | S 38° 54' 24" W | 45.077 m (147.89') |
| (27) | (228) | N 38° 55' 43" E | 1.631 m (5.35') |
| (28) | (229) | N 87° 52' 26" E | 32.970 m (108.17') |
| (29) | (230) | N 33° 38' 33" E | 38.779 m (127.23') |
| (30) | (231) | S 87° 52' 23" W | 31.250 m (102.53') |
| (31) | (232) | N 33° 40' 35" E | 1.160 m (3.81') |
| (32) | (233) | S 87° 52' 23" W | 66.480 m (218.11') |
| (33) | (234) | N 00° 26' 53" W | 17.374 m (57.00') |
| (34) | (235) | S 89° 30' 56" E | 46.520 m (152.62') |
| (35) | (236) | N 00° 26' 53" W | 0.914 m (3.00') |
| (36) | (237) | S 81° 19' 08" C | 14.042 m (46.07') |
| (37) | (238) | N 81° 19' 08" W | 40.932 m (134.29') |
| (38) | (239) | N 00° 26' 53" W | 27.057 m (88.77') |
| (39) | (240) | S 89° 30' 56" E | 38.871 m (127.53') |
| (40) | (241) | S 79° 41' 02" E | 9.662 m (31.70') |
| (41) | (242) | S 51° 42' 41" E | 14.338 m (47.04') |
| (42) | (243) | S 37° 33' 15" W | 72.207 m (236.90') |
| (43) | (244) | N 41° 17' 20" E | 4.941 m (16.21') |
| (44) | (245) | S 72° 50' 43" E | 44.281 m (145.28') |
| (45) | (246) | N 41° 17' 20" E | 34.052 m (111.72') |
| (46) | (247) | N 31° 58' 00" E | 85.615 m (280.89') |
| (47) | (248) | S 00° 54' 06" E | 78.083 m (256.18') |
| (48) | (249) | N 44° 34' 35" E | Lo = 53.280 m (174.80') |
| (49) | (250) | Ro = 915.862 m (3004.79') | Arc = 53.281 m (174.83') |
| (50) | (251) | S 42° 05' 03" E | Lo = 25.871 m (84.88') |
| (51) | (252) | Ro = 915.862 m (3004.79') | Arc = 25.872 m (84.88') |
| (52) | (253) | S 47° 54' 01" W | Lo = 52.929 m (173.65') |
| (53) | (254) | Ro = 915.862 m (3004.79') | Arc = 52.936 m (173.67') |
| (54) | (255) | N 51° 16' 15" E | Lo = 54.834 m (179.90') |
| (55) | (256) | Ro = 915.862 m (3004.79') | Arc = 54.842 m (179.93') |
| (56) | (257) | S 39° 04' 46" W | Lo = 12.198 m (40.02') |
| (57) | (258) | Ro = 915.862 m (3004.79') | Arc = 12.198 m (40.02') |
| (58) | (259) | N 40° 22' 35" E | Lo = 29.261 m (96.00') |
| (59) | (260) | Ro = 915.862 m (3004.79') | Arc = 29.262 m (96.00') |
| (60) | (261) | S 38° 21' 11" W | Lo = 10.918 m (35.82') |
| (61) | (262) | Ro = 915.862 m (3004.79') | Arc = 10.918 m (35.82') |
| (62) | (263) | N 43° 58' 28" E | Lo = 58.016 m (190.34') |
| (63) | (264) | Ro = 830.518 m (2724.79') | Arc = 58.028 m (190.38') |



SURVEYOR'S STATEMENT

I, DONALD A. MILLER, HEREBY STATE THAT I AM A PROFESSIONAL LAND SURVEYOR OF THE STATE OF ILLINOIS; THAT THE SURVEY OR PROPOSED MAP ROUTE TOWAS MADE BY ME OR UNDER MY DIRECT SUPERVISION; THAT THE LAND CORNER TIES AS SHOWN HEREON, ARE A TRUE AND CORRECT REPRESENTATION OF SAID SURVEY AND THAT THE PLAT HEREON IS IN ACCORDANCE WITH APPLICABLE SECTIONS OF THE LAND ACQUISITION POLICIES AND PROCEDURES MANUAL OF THE DEPARTMENT'S DISTRICT POLICIES. THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

DATED MAY 10, 2007

BY: *Donald A. Miller*
DONALD A. MILLER ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-1966
LICENSE EXPIRES DATE 11-30-08

| REVISION | | ILLINOIS DEPT. OF TRANSPORTATION RIGHT OF WAY PLANS |
|----------|-------------|--|
| DATE | DESCRIPTION | |
| | | ROUTE FAP. 704 (I-55 BL. VETERAN'S PARKWAY) |
| | | SECTION (1) N. & 15- |
| | | PROJECT |
| | | COUNTY McLEAN |
| | | JOB NUMBER R-93-005-98 |
| | | STATION 10+950 TO 11+250 |
| | | SCALE 1:500 SHEET 1 OF 8 |



- LEGEND**
- SECTION CORNER
 - FOUND IRON ROD OR PIPE
 - AC ACCESS CONTROL
 - RECORD DATA
 - ENGLISH EQUIVALENT
 - EXISTING TREE/ DIAMETER (INCHES)

PARCEL NO. 3NP0002
CECELIA T. BLAN, TRUSTEE
CECELIA T. BLAN TRUST
TOTAL HOLDING = 4698 sq m ± (15073 sq ft ±)
TOTAL R.O.W. REQUIRED = 676 sq m ± (1722 sq ft ±)
REMAINDER = 4022 sq m ± (13301 sq ft ±)

PARCEL NO. 3NP0003
EDWARD SZAREK AND MARILYN SZAREK (1/2 INT)
JOSEPH SZAREK AND CINDY SZAREK (1/2 INT)
(SEE SHT 2)

PARCEL NO. 3NP0001
HEARTLAND BANK
TRUST HBT - 189
TOTAL HOLDING = 8128 sq m ± (87486 sq ft ±)
TOTAL R.O.W. REQUIRED = 3735 sq m ± (40204 sq ft ±)
AREA IN EXISTING R.O.W. = 3557 sq m ± (38286 sq ft ±)
NET R.O.W. REQUIRED = 178 sq m ± (1918 sq ft ±)
REMAINDER = 4393 sq m ± (47282 sq ft ±)

PROP. CURVE #10
PI STA = 11+423.684
Δ = 51° 03' 36" (RT)
R = 873.323 m
T = 417.116 m
L = 778.277 m
E = 94.499 m
P.C. STA = 11+006.568
P.T. STA = 11+784.845

PROP. CURVE #11
PI STA = 11+947.400
Δ = 44° 44' 25" (LT.)
R = 125.000 m
T = 51.445 m
L = 97.608 m
E = 10.172 m
e = 4.02'
T.R. = 10 m
S.E. RUN = 25 m
P.C. STA = 0+895.955
P.T. STA = 0+877.170

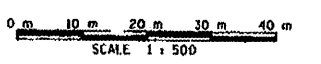
PROP. CURVE #12
PI STA = 7+066.733
Δ = 87° 41' 07" (RT)
R = 44,000 m
T = 42,258 m
L = 67,338 m
E = 17,008 m
e = 3.02'
T.R. = 7.5 m
S.E. RUN = 15.0 m
P.C. STA = 7+024.475
P.T. STA = 7+091.813

PROP. CURVE #13
PI STA = 7+117.011
Δ = 42° 17' 13" (LT.)
R = 44,000 m
T = 17,017 m
L = 32,473 m
E = 3,474 m
e = N.C.
T.R. = N.A.
S.E. RUN = N.A.
P.C. STA = 7+099.994
P.T. STA = 7+132.468

PROP. CURVE #14
PI STA = 0+947.400
Δ = 44° 44' 25" (LT.)
R = 125.000 m
T = 51.445 m
L = 97.608 m
E = 10.172 m
e = 4.02'
T.R. = 10 m
S.E. RUN = 25 m
P.C. STA = 0+895.955
P.T. STA = 0+877.170

PROP. CURVE #12
PI STA = 6+956.600
Δ = 7° 00' 00" (RT)
R = 1,112.533 m
T = 68.045 m
L = 135.920 m
E = 2.079 m
e = N.C. 2.0X
T.R. = N.A.
S.E. RUN = N.A.
P.C. STA = 6+888.555
P.T. STA = 7+024.475

NOTE:
BEARINGS AND GRID DISTANCES
BASED ON STATE PLANE
COORDINATES NAD 83, EAST ZONE. R
GRID FACTOR = 1.00001605
1 METER = 3.28083333 FEET.



W 1/2, NE 1/4, SEC. 17 T. 23 N, R. 2 E, 3RD P.M.

SE 1/4 SEC. 8 T. 23 N, R. 2 E, 3RD P.M.

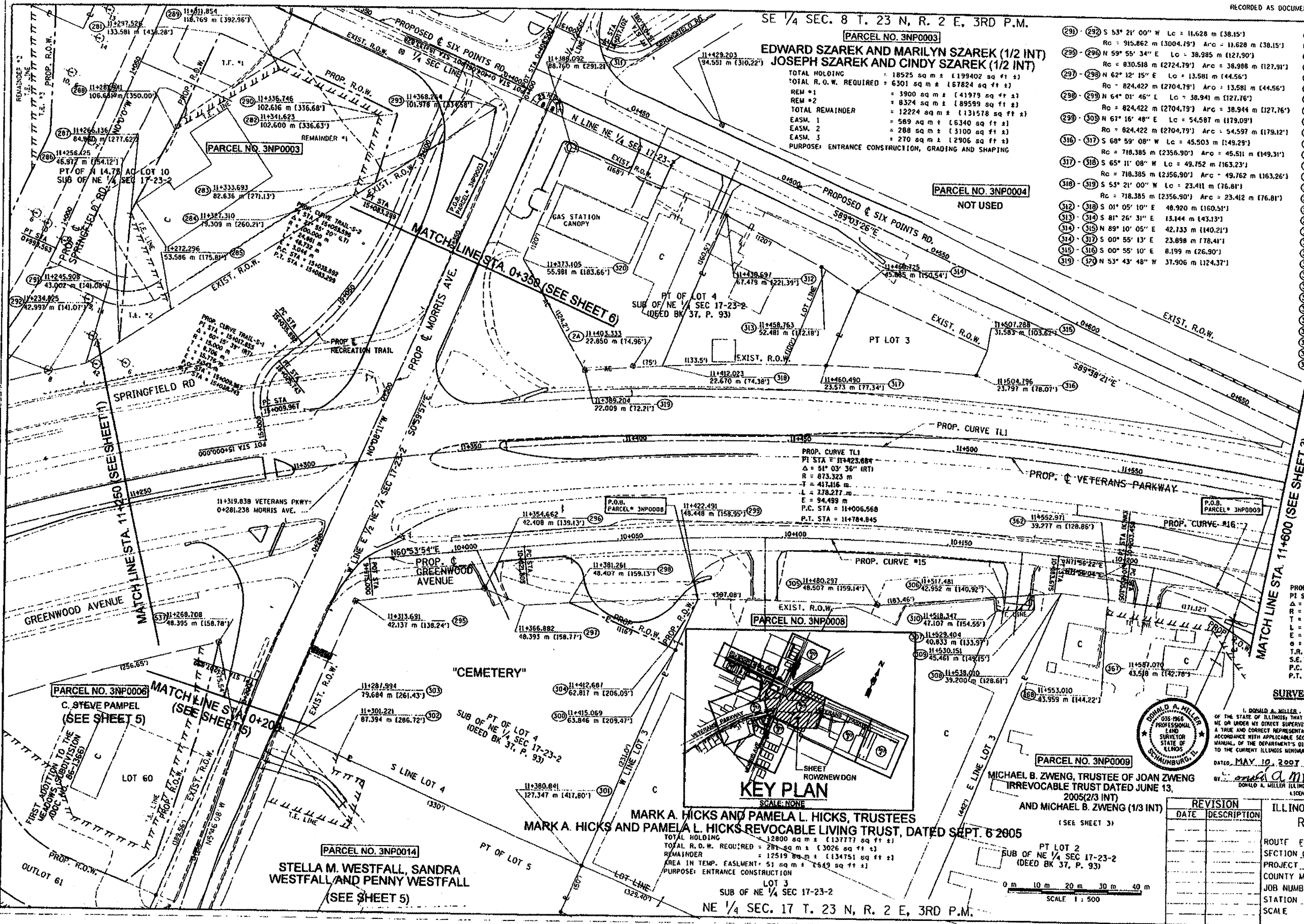
PARCEL NO. 3NP0003
EDWARD SZAREK AND MARILYN SZAREK (1/2 INT)
JOSEPH SZAREK AND CINDY SZAREK (1/2 INT)

TOTAL HOLDING = 18525 sq m ± (199402 sq ft ±)
 TOTAL R.O.W. REQUIRED = 6301 sq m ± (67824 sq ft ±)
 REM #1 = 3900 sq m ± (41979 sq ft ±)
 REM #2 = 8324 sq m ± (89599 sq ft ±)
 TOTAL REMAINDER = 12224 sq m ± (131578 sq ft ±)
 EASM. 1 = 589 sq m ± (6340 sq ft ±)
 EASM. 2 = 288 sq m ± (3100 sq ft ±)
 EASM. 3 = 270 sq m ± (2905 sq ft ±)
 PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

- (291) (292) S 53° 21' 00" W Lc = 11,628 m (38.15')
- Rc = 915,862 m (3004.79') Arc = 11,628 m (38.15')
- (293) (294) N 59° 55' 34" E Lc = 38,985 m (127.90')
- Rc = 830,518 m (2724.79') Arc = 38,988 m (127.91')
- (295) (296) N 62° 12' 15" E Lc = 13,581 m (44.56')
- Rc = 824,422 m (2704.79') Arc = 13,581 m (44.56')
- (297) (298) N 64° 01' 46" E Lc = 58,941 m (193.31')
- Rc = 824,422 m (2704.79') Arc = 38,944 m (127.76')
- (299) (300) N 61° 16' 48" E Lc = 54,587 m (179.09')
- Rc = 824,422 m (2704.79') Arc = 54,597 m (179.12')
- (301) (302) S 68° 59' 08" W Lc = 45,503 m (149.29')
- Rc = 718,385 m (2356.90') Arc = 45,511 m (149.31')
- (303) (304) S 65° 11' 08" W Lc = 49,752 m (163.23')
- Rc = 718,385 m (2356.90') Arc = 49,762 m (163.26')
- (305) (306) S 53° 21' 00" W Lc = 23,411 m (76.81')
- Rc = 718,385 m (2356.90') Arc = 23,412 m (76.81')
- (307) (308) S 01° 05' 10" E Lc = 48,920 m (160.51')
- (309) (310) S 81° 26' 31" E Lc = 13,144 m (43.13')
- (311) (312) N 89° 10' 05" E Lc = 42,133 m (138.21')
- (313) (314) S 00° 55' 13" E Lc = 23,898 m (78.41')
- (315) (316) S 00° 55' 10" E Lc = 8,199 m (26.90')
- (317) (318) N 53° 43' 48" W Lc = 37,906 m (124.37')

- (281) (282) S 89° 41' 01" E 58,863 m (193.13')
- (283) (284) S 00° 00' 00" W 32,216 m (105.70')
- (285) (286) S 06° 31' 30" E 21,806 m (71.54')
- (287) (288) S 33° 47' 35" W 1,728 m (5.65')
- (289) (290) N 33° 47' 35" E 140,654 m (461.54')
- (291) (292) S 33° 47' 35" W 64,532 m (211.71')
- (293) (294) N 06° 31' 37" W 25,529 m (83.74')
- (295) (296) S 33° 47' 35" W 18,336 m (60.14')
- (297) (298) N 46° 49' 33" W 31,738 m (104.10')
- (299) (300) N 19° 18' 29" W 39,156 m (128.43')
- (301) (302) S 33° 47' 35" W 11,454 m (37.57')
- (303) (304) N 04° 03' 49" E 28,181 m (92.51')
- (305) (306) N 35° 54' 40" E 36,235 m (118.85')
- (307) (308) S 89° 41' 05" E 32,887 m (107.89')
- (309) (310) S 00° 10' 27" E 44,508 m (146.02')
- (311) (312) N 88° 35' 39" E 13,040 m (42.77')
- (313) (314) N 89° 44' 11" E 32,761 m (107.48')
- (315) (316) S 07° 54' 26" W 17,051 m (55.94')
- (317) (318) S 00° 38' 38" E 10,426 m (34.38')
- (319) (320) S 89° 44' 03" W 2,437 m (7.97')
- (321) (322) S 89° 44' 08" W 80,411 m (263.80')
- (323) (324) S 89° 44' 08" W 14,231 m (46.69')
- (325) (326) N 61° 22' 21" E 35,670 m (117.03')
- (327) (328) N 61° 22' 21" E 11,547 m (37.88')
- (329) (330) S 26° 35' 29" E 4,235 m (13.89')
- (331) (332) N 61° 22' 21" E 8,312 m (27.27')
- (333) (334) S 26° 21' 26" E 4,683 m (15.36')
- (335) (336) S 63° 37' 40" W 11,299 m (37.07')
- (337) (338) N 89° 05' 20" E 50,055 m (164.51')
- (339) (340) S 01° 05' 10" E 36,576 m (120.00')
- (341) (342) S 86° 11' 06" E 32,776 m (107.54')

PARCEL NO. 3NP0004 NOT USED



LEGEND

- SECTION CORNER
- FOUND IRON ROD OR PIPE
- FOUND RIGHT OF WAY MARKER
- ACCESS CONTROL
- RECORD DATA
- ENGLISH EQUIVALENT
- EXISTING TREE / DIAMETER (INCHES)

NOTE:
 BEARINGS AND GRID DISTANCES
 BASED ON STATE PLANE
 COORDINATES NAD 83, EAST ZONE.
 GRID FACTOR = 1.00001605.
 1 METER = 3.28083333 FEET.

PROP. CURVE #15
 PI STA = 10+102.992
 $\Delta = 11° 02' 28"$ (RT.)
 R = 840,000 m
 T = 81,186 m
 L = 161,870 m
 E = 3,914 m
 o = N.C.
 T.R. = N.A.
 S.E. RUN = N.A.
 P.C. STA = 10+021.805
 P.T. STA = 10+183.675

PROP. CURVE #16
 PI STA = 10+229.362
 $\Delta = 34° 02' 16"$ (RT.)
 R = 79,248 m
 T = 24,257 m
 L = 47,079 m
 E = 3,629 m
 o = N.C.
 T.R. = N.A.
 S.E. RUN = N.A.
 P.C. STA = 10+025.105
 P.T. STA = 10+282.184

SURVEYOR'S STATEMENT

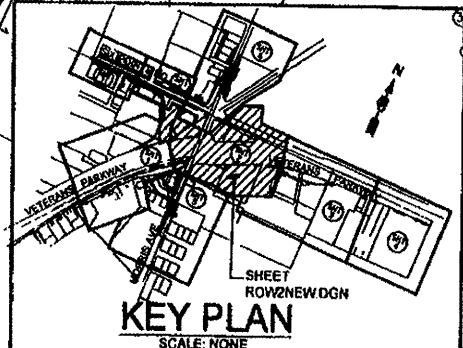
I, DONALD A. MILLER, HEREBY STATE THAT I AM A PROFESSIONAL LAND SURVEYOR OF THE STATE OF ILLINOIS THAT THE SURVEY OF PROPOSED F&P ROUTE 704 WAS MADE BY ME OR UNDER MY DIRECT SUPERVISION THAT THE LAND CORNER TIES AS SHOWN HEREON, ARE A TRUE AND CORRECT REPRESENTATION OF SAID SURVEY, AND THAT THE PLAY HEREON IS IN ACCORDANCE WITH APPLICABLE SECTIONS OF THE LAND ACQUISITION POLICIES AND PROCEDURES MANUAL OF THE DEPARTMENT'S DISTRICT OFFICES. THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

DATE: MAY 10, 2007
 BY: *Donald A. Miller*
 DONALD A. MILLER ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-1966
 LICENSE EXPIRES DATE 11-30-08



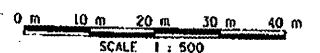
MICHAEL B. ZWENG, TRUSTEE OF JOAN ZWENG IRREVOCABLE TRUST DATED JUNE 13, 2005 (2/3 INT) AND MICHAEL B. ZWENG (1/3 INT) (SEE SHEET 3)

KEY PLAN



MARK A. HICKS AND PAMELA L. HICKS, TRUSTEES
 MARK A. HICKS AND PAMELA L. HICKS REVOCABLE LIVING TRUST, DATED SEPT. 6 2005

TOTAL HOLDING = 12800 sq m ± (137777 sq ft ±)
 TOTAL R.O.W. REQUIRED = 281 sq m ± (3026 sq ft ±)
 REMAINDER = 12519 sq m ± (134751 sq ft ±)
 AREA IN TEMP. EASMENT = 51 sq m ± (549 sq ft ±)
 PURPOSE: ENTRANCE CONSTRUCTION

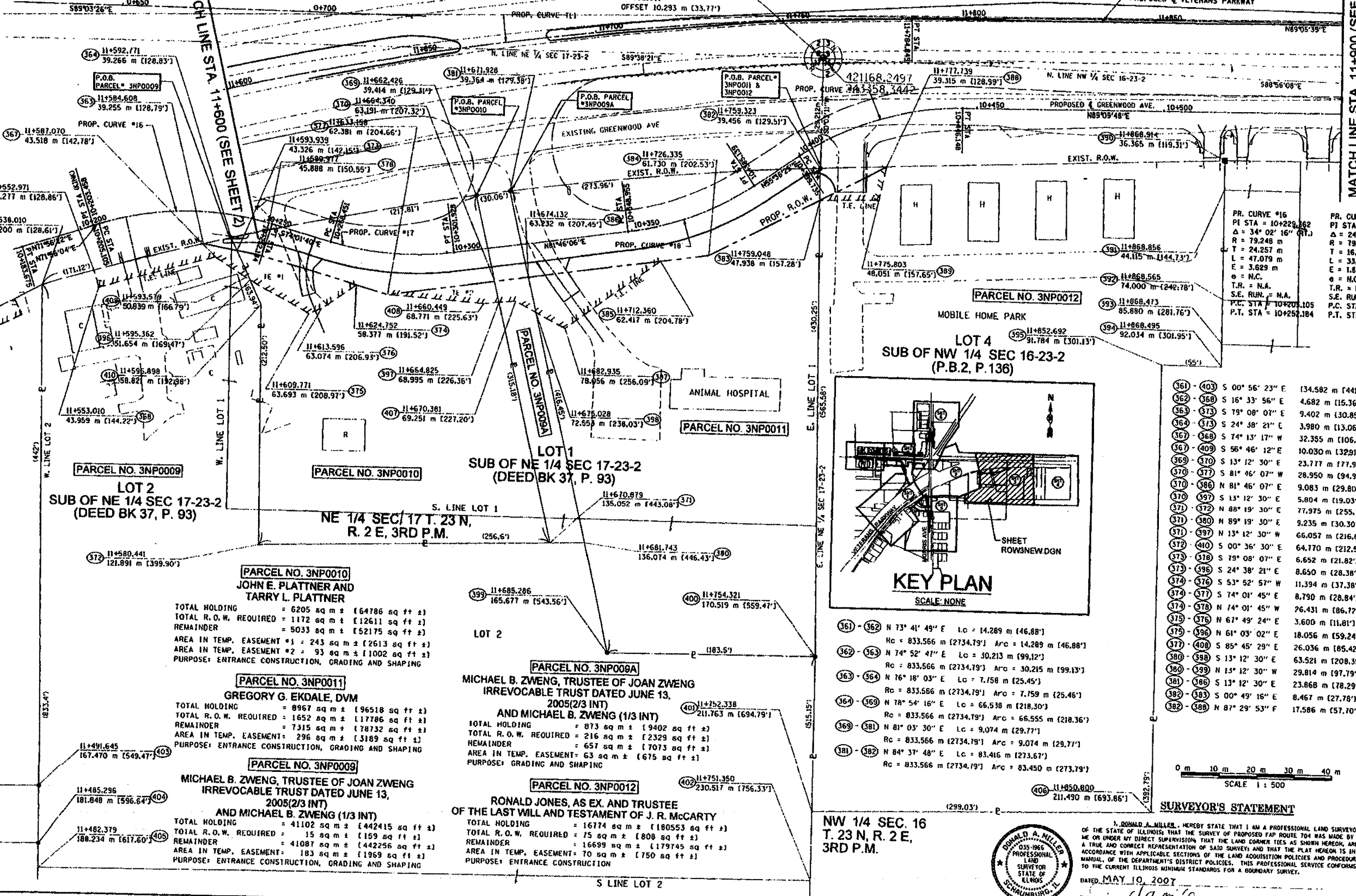


| REVISION | DATE | DESCRIPTION |
|----------|------|-------------|
| | | |
| | | |
| | | |
| | | |

ILLINOIS DEPT. OF TRANSPORTATION
 RIGHT OF WAY PLANS
 ROUTE EAP 704 (I-55 BL. VETERAN'S PARKWAY)
 SECTION (1) N. & IS-1
 PROJECT
 COUNTY McLEAN
 JOB NUMBER R-93-005-98
 STATION 11+250 TO 11+600
 SCALE 1:500 SHEET 2 OF 8

PI STA = 11+423.684
Δ = 51° 03' 36" (RT)
R = 873.323 m
T = 417.116 m
L = 778.277 m
E = 94.499 m

P.C. STA = 11+006.568
P.T. STA = 11+784.845
PROPOSED SIX POINTS ROAD

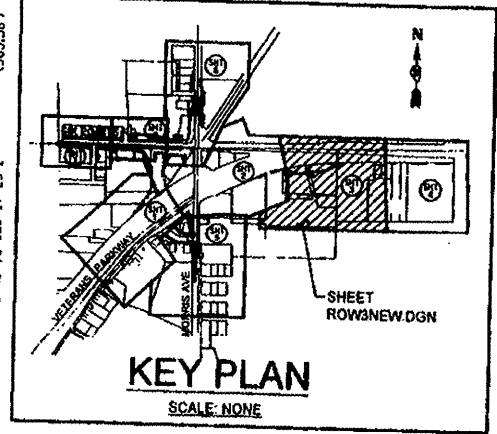


LEGEND

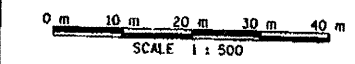
- SECTION CORNER
- FOUND IRON ROD OR PIPE
- FOUND RIGHT OF WAY MARKER
- ACCESS CONTROL
- RECORD DATA
- ENGLISH EQUIVALENT
- EXISTING TREE/DIAMETER (INCHES)

NOTE:
BEARINGS AND GRID DISTANCES
BASED ON STATE PLANE
COORDINATES NAD 83, EAST ZONE.
GRID FACTOR = 1.00001605
1 METER = 3.28083333 FEET.

| PR. CURVE #16 | PR. CURVE #17 | PR. CURVE #18 | PR. CURVE #19 |
|--|---|---|---|
| PI STA = 10+229.662 Δ = 34° 02' 16" (RT) R = 79.248 m T = 24.257 m L = 47.079 m E = 3.629 m θ = N.C. T.R. = N.A. S.E. RUN = N.A. P.C. STA = 10+202.105 P.T. STA = 10+252.384 | PI STA = 10+285.443 Δ = 26° 09' 41" (LT.) R = 79.248 m T = 16.992 m L = 33.477 m E = 1.801 m θ = N.C. T.R. = N.A. S.E. RUN = N.A. P.C. STA = 10+268.451 P.T. STA = 10+301.928 | PI STA = 10+367.368 Δ = 26° 09' 41" (LT.) R = 79.248 m T = 18.413 m L = 36.185 m E = 2.111 m θ = N.C. T.R. = N.A. S.E. RUN = N.A. P.C. STA = 10+348.955 P.T. STA = 10+385.139 | PI STA = 10+423.629 Δ = 33° 33' 22" (RT.) R = 79.248 m T = 23.893 m L = 46.413 m E = 3.524 m θ = N.C. T.R. = N.A. S.E. RUN = N.A. P.C. STA = 10+399.735 P.T. STA = 10+446.148 |



| | | | | | |
|---------|-----------------|---------------------|---------|-----------------|---------------------|
| 361-403 | S 00° 56' 23" E | 134.582 m [441.54'] | 383-384 | S 62° 06' 41" W | 33.615 m [110.29'] |
| 362-368 | S 16° 33' 56" E | 4.682 m [15.36'] | 383-388 | N 62° 06' 41" E | 19.910 m [65.32'] |
| 363-373 | S 79° 08' 07" E | 9.402 m [30.85'] | 383-389 | N 88° 21' 34" E | 15.834 m [51.95'] |
| 364-373 | S 24° 38' 21" E | 3.980 m [13.06'] | 383-400 | S 00° 49' 16" E | 122.650 m [402.42'] |
| 365-368 | S 74° 13' 17" W | 32.355 m [106.15'] | 384-385 | S 81° 46' 07" W | 13.000 m [42.65'] |
| 367-409 | S 56° 46' 12" E | 10.030 m [32.91'] | 385-386 | S 81° 46' 07" W | 35.485 m [116.42'] |
| 369-370 | S 13° 12' 30" E | 23.777 m [77.99'] | 385-387 | S 53° 20' 55" W | 31.251 m [102.53'] |
| 370-377 | S 81° 46' 07" W | 28.950 m [94.98'] | 386-388 | S 13° 12' 30" E | 9.321 m [30.57'] |
| 370-386 | N 81° 46' 07" E | 9.083 m [29.80'] | 387-389 | N 60° 33' 15" W | 9.082 m [29.80'] |
| 370-397 | S 13° 12' 30" E | 5.804 m [19.03'] | 388-389 | S 11° 17' 51" W | 9.035 m [29.64'] |
| 371-372 | N 88° 19' 30" E | 77.975 m [255.76'] | 388-390 | N 87° 13' 03" E | 90.904 m [298.24'] |
| 371-380 | N 89° 19' 30" E | 9.235 m [30.30'] | 389-391 | S 00° 27' 36" E | 1.765 m [5.78'] |
| 371-397 | N 13° 12' 30" W | 66.057 m [216.67'] | 389-392 | S 00° 27' 36" E | 29.871 m [98.00'] |
| 372-410 | S 00° 36' 30" E | 64.170 m [212.50'] | 389-393 | S 00° 27' 36" E | 11.880 m [38.98'] |
| 373-378 | S 79° 08' 07" E | 6.652 m [21.82'] | 389-394 | S 00° 27' 36" E | 6.096 m [20.00'] |
| 373-396 | S 24° 38' 21" E | 8.650 m [28.38'] | 389-395 | N 90° 00' 00" E | 15.805 m [51.85'] |
| 374-376 | S 53° 52' 57" W | 11.394 m [37.38'] | 389-406 | S 00° 00' 00" W | 119.721 m [392.79'] |
| 374-377 | S 74° 01' 45" E | 8.790 m [28.84'] | 389-409 | N 61° 13' 25" W | 1.265 m [4.15'] |
| 374-378 | N 74° 01' 45" W | 26.431 m [86.72'] | 389-410 | N 24° 38' 21" W | 6.980 m [22.89'] |
| 375-376 | N 61° 49' 24" E | 3.600 m [11.81'] | 389-410 | N 85° 08' 13" E | 4.431 m [14.54'] |
| 375-396 | N 61° 03' 02" E | 18.056 m [59.24'] | 389-410 | S 83° 39' 19" W | 4.206 m [13.80'] |
| 377-400 | S 85° 45' 29" E | 26.036 m [85.42'] | 389-410 | N 60° 33' 15" W | 5.397 m [17.71'] |
| 380-398 | S 13° 12' 30" W | 63.521 m [208.35'] | 389-410 | S 89° 47' 31" W | 55.403 m [181.72'] |
| 380-399 | N 13° 12' 30" W | 29.814 m [97.79'] | 400-401 | S 00° 49' 16" E | 41.273 m [135.42'] |
| 381-385 | S 13° 12' 30" W | 23.868 m [78.29'] | 401-402 | S 00° 49' 16" E | 18.769 m [61.58'] |
| 382-383 | S 00° 49' 16" E | 8.467 m [27.78'] | 401-405 | N 88° 52' 57" E | 91.170 m [299.11'] |
| 382-388 | N 87° 29' 53" F | 17.586 m [57.70'] | 402-405 | S 89° 40' 00" W | 207.921 m [682.15'] |
| | | | 403-404 | S 00° 53' 34" E | 15.248 m [50.03'] |
| | | | 404-405 | S 00° 51' 18" E | 6.788 m [22.27'] |



SURVEYOR'S STATEMENT



NW 1/4 SEC 16
T. 23 N, R. 2 E,
3RD P.M.

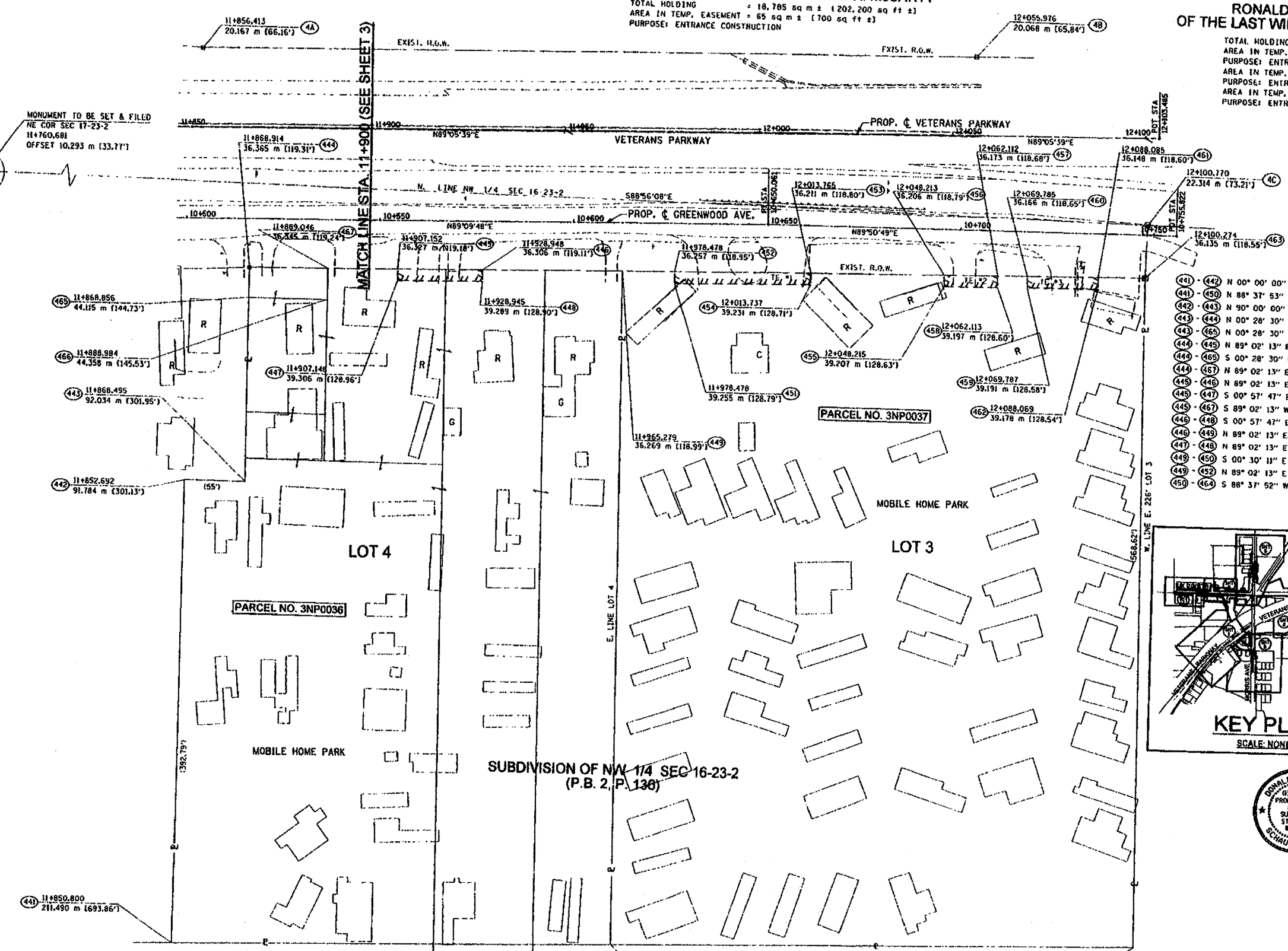
I, DONALD A. MILLER, HEREBY STATE THAT I AM A PROFESSIONAL LAND SURVEYOR OF THE STATE OF ILLINOIS THAT THE SURVEY OF PROPOSED PARCELS SHOWN HEREON WAS MADE BY ME OR UNDER MY DIRECT SUPERVISION THAT THE LAND CORNER TIES AS SHOWN HEREON ARE A TRUE AND CORRECT REPRESENTATION OF THE LAND ACQUISITION POLICIES AND PROCEDURES IN ACCORDANCE WITH APPLICABLE SECTIONS OF THE LAND ACQUISITION POLICIES AND PROCEDURES TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
DATED MAY 10, 2007
BY: *Donald A. Miller*
DONALD A. MILLER PROFESSIONAL LAND SURVEYOR NO. 035-1965
LICENSE EXPIRES DATE 11-30-09

ILLINOIS DEPT. OF TRANSPORTATION
RIGHT OF WAY PLANS

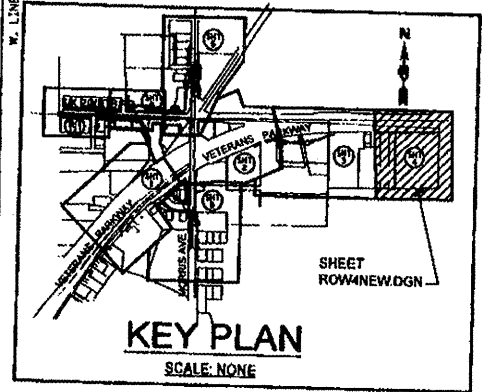
ROUTE EAP 704 (U.S. 55 BL. VETERAN'S PARKWAY)
SECTION (U.N. & TS-1)
PROJECT
COUNTY McLEAN
JOB NUMBER R-93-005-98
STATION 11+600 TO 11+900
SCALE 1:500 SHEET 3 OF 8

PARCEL NO. 3NP0036
RONALD JONES, AS EX. AND TRUSTEE
OF THE LAST WILL AND TESTAMENT OF J. R. McCARTY
 TOTAL HOLDING = 18,785 sq m ± (202,200 sq ft ±)
 AREA IN TEMP. EASEMENT = 65 sq m ± (700 sq ft ±)
 PURPOSE: ENTRANCE CONSTRUCTION

PARCEL NO. 3NP0037
RONALD JONES, AS EX. AND TRUSTEE
OF THE LAST WILL AND TESTAMENT OF J. R. McCARTY
 TOTAL HOLDING = 23,468 sq m ± (252,605 sq ft ±)
 AREA IN TEMP. EASEMENT 1 = 106 sq m ± (1141 sq ft ±)
 PURPOSE: ENTRANCE CONSTRUCTION
 AREA IN TEMP. EASEMENT 2 = 42 sq m ± (452 sq ft ±)
 PURPOSE: ENTRANCE CONSTRUCTION
 AREA IN TEMP. EASEMENT 3 = 65 sq m ± (1591 sq ft ±)
 PURPOSE: ENTRANCE CONSTRUCTION



| | | | | | | | |
|-----|-----|-----------------|---------------------|-----|-----|-----------------|---------------------|
| 441 | 442 | N 00° 00' 00" E | 119.721 m (392.79') | 451 | 452 | N 00° 57' 47" W | 2.998 m (9.84') |
| 442 | 443 | N 88° 37' 53" E | 113.257 m (371.50') | 452 | 453 | N 89° 03' 15" E | 35.259 m (115.96') |
| 443 | 444 | N 90° 00' 00" E | 15.805 m (51.85') | 453 | 454 | N 89° 02' 13" E | 35.286 m (115.95') |
| 444 | 445 | N 00° 28' 30" W | 55.670 m (182.64') | 454 | 455 | S 00° 22' 55" E | 3.020 m (9.88') |
| 445 | 446 | N 00° 28' 30" W | 47.921 m (157.22') | 455 | 456 | N 89° 02' 13" E | 34.449 m (112.74') |
| 446 | 447 | N 89° 02' 13" E | 38.238 m (125.45') | 456 | 457 | N 00° 56' 09" W | 3.000 m (9.84') |
| 447 | 448 | S 00° 28' 30" E | 7.749 m (25.42') | 457 | 458 | N 89° 03' 16" E | 13.899 m (45.60') |
| 448 | 449 | N 89° 02' 13" E | 20.132 m (66.05') | 458 | 459 | N 89° 02' 13" E | 13.898 m (45.60') |
| 449 | 450 | N 89° 02' 13" E | 21.796 m (71.51') | 459 | 460 | S 00° 57' 47" E | 3.023 m (9.92') |
| 450 | 451 | S 00° 57' 47" E | 2.978 m (9.77') | 460 | 461 | N 89° 02' 13" E | 7.673 m (25.17') |
| 451 | 452 | S 89° 02' 13" W | 18.106 m (59.40') | 461 | 462 | N 00° 57' 47" W | 3.026 m (9.93') |
| 452 | 453 | S 00° 57' 47" E | 2.984 m (9.79') | 462 | 463 | N 89° 03' 12" E | 18.281 m (59.98') |
| 453 | 454 | N 89° 02' 13" E | 36.331 m (119.20') | 463 | 464 | N 89° 02' 13" E | 18.301 m (60.04') |
| 454 | 455 | N 89° 02' 13" E | 21.796 m (71.51') | 464 | 465 | S 00° 35' 31" W | 3.032 m (9.95') |
| 455 | 456 | S 00° 30' 11" E | 174.305 m (571.86') | 465 | 466 | N 89° 02' 13" E | 12.189 m (39.99') |
| 456 | 457 | N 89° 02' 13" E | 13.199 m (43.30') | 466 | 467 | S 00° 30' 46" E | 173.320 m (568.63') |
| 457 | 458 | S 88° 37' 52" W | 135.036 m (443.03') | 467 | 468 | N 89° 47' 09" E | 20.132 m (66.05') |
| 458 | 459 | | | 468 | 469 | N 00° 28' 30" W | 8.013 m (26.29') |



- LEGEND**
- SECTION CORNER
 - FOUND IRON ROD OR PIPE
 - FOUND RIGHT OF WAY MARKER
 - ACCESS CONTROL
 - RECORD DATA
 - ENGLISH EQUIVALENT
 - EXISTING TREE / DIAMETER (INCHES)

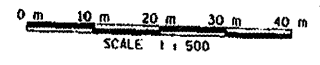
NOTE:
 BEARINGS AND GRID DISTANCES
 BASED ON STATE PLANE
 COORDINATES NAD 83, EAST ZONE.
 GRID FACTOR = 1.00001605
 1 METER = 3.28083333 FEET.

SURVEYOR'S STATEMENT

I, DONALD A. MILLER, HEREBY STATE THAT I AM A PROFESSIONAL LAND SURVEYOR OF THE STATE OF ILLINOIS. THAT THE SURVEY OF PROPOSED EAP ROUTE FOR WAS MADE BY ME OR UNDER MY DIRECT SUPERVISION THAT THE LAND CORNER TIES AS SHOWN HEREON ARE ACCORDANCE WITH APPLICABLE SECTIONS OF THE LAND ACQUISITION POLICIES AND PROCEDURES MANUAL, OF THE DEPARTMENT'S DISTRICT POLICIES. THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
 DATED: MAY 10, 2007
 BY: *Donald A. Miller*
 DONALD A. MILLER ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-1966
 LICENSE EXPIRES DATE 11-30-08



ILLINOIS DEPT. OF TRANSPORTATION
RIGHT OF WAY PLANS
 ROUTE: EAP 104 (I-55 BL - VETERANS PARKWAY)
 SECTION: (11 N. & T.S.)
 PROJECT: _____
 COUNTY: McLEAN
 JOB NUMBER: R-93-005-98
 STATION: 11+900 TO 12+103.485
 SCALF: 1:500 SHEET: 4 OF 8



NW 1/4 SEC. 16 T. 23 N., R. 2 E., 3RD P.M.

PARCEL NO. 3NP0005
SYNERGY II, L.L.C.
 TOTAL HOLDING = 7964 sq m ± (85129 sq ft ±)
 TOTAL R.O.W. REQUIRED = 30 sq m ± (324 sq ft ±)
 REMAINDER = 7934 sq m ± (85405 sq ft ±)
 AREA IN TEMP. EASEMENT = 731 sq m ± (1872 sq ft ±)
 PURPOSE: GRADING AND SHAPING

PARCEL NO. 3NP0007
C. STEVE PAMPEL
 TOTAL HOLDING = 1760 sq m ± (18946 sq ft ±)
 TOTAL R.O.W. REQUIRED = 1760 sq m ± (18946 sq ft ±)
 REMAINDER = 0000 sq m ± (0000 sq ft ±)

PARCEL NO. 3NP0008
C. STEVE PAMPEL
 TOTAL HOLDING = 3723 sq m ± (40070 sq ft ±)
 TOTAL R.O.W. REQUIRED = 485 sq m ± (5217 sq ft ±)
 REMAINDER = 3238 sq m ± (34853 sq ft ±)
 AREA IN TEMP. EASEMENT = 1621 sq m ± (17450 sq ft ±)
 PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

PROF. CURVE #13
 P.I. STA = 7+066.733
 $\Delta = 87^\circ 41' 07''$ (RT)
 R = 44,000 m
 T = 42,258 m
 L = 67,338 m
 E = 17,006 m
 $\phi = 3.0\%$
 T.R. = 7.5 m
 S.E. RUN = 15.0 m
 P.C. STA = 7+024.475
 P.T. STA = 7+091.813

PROF. CURVE #14
 P.I. STA = 7+117.011
 $\Delta = 42^\circ 17' 13''$ (LT)
 R = 44,000 m
 T = 17,017 m
 L = 32,473 m
 E = 3,474 m
 $\phi = N.C.$
 T.R. = N.A.
 S.E. RUN = N.A.
 P.C. STA = 7+099.994
 P.T. STA = 7+132.468

| | | | | | |
|-------------|--|--------------------|-------------|-----------------|--------------------|
| (521)-(522) | N 56° 03' 19" E | 20.144 m (66.09') | (528)-(529) | S 88° 50' 59" W | 79.903 m (262.15') |
| (521)-(530) | S 81° 26' 46" E | 37.928 m (124.44') | (530)-(531) | S 51° 31' 16" E | 24.718 m (81.10') |
| (522)-(523) | N 56° 03' 49" E | 25.756 m (84.50') | (531)-(532) | S 73° 33' 30" E | 19.184 m (62.94') |
| (523)-(524) | N 88° 50' 17" E | 26.799 m (87.86') | (532)-(533) | N 89° 57' 55" E | 33.020 m (108.33') |
| (524)-(525) | S 14° 00' 09" E | 16.665 m (54.68') | (533)-(534) | N 56° 03' 49" E | 3.126 m (10.26') |
| (525)-(526) | N 88° 50' 17" E | 3.701 m (12.14') | (534)-(535) | S 76° 55' 55" E | 31.003 m (101.72') |
| (526)-(527) | S 01° 09' 41" E | 1.599 m (5.25') | (535)-(536) | S 18° 15' 40" E | 17.498 m (57.41') |
| (527)-(528) | N 01° 09' 41" W | 16.529 m (54.21') | (536)-(537) | S 11° 05' 24" E | 29.605 m (97.13') |
| (528)-(529) | S 01° 09' 41" E | 42.138 m (138.25') | (537)-(538) | S 34° 52' 29" E | 23.095 m (75.77') |
| (529)-(530) | N 29° 33' 00" W | 15.563 m (51.06') | (538)-(539) | S 05° 46' 07" E | 37.309 m (122.40') |
| (530)-(531) | S 66° 41' 23" E | 30.066 m (98.64') | (539)-(540) | S 25° 17' 57" W | 11.827 m (38.80') |
| (531)-(532) | Rc = 35.052 m (115.00') Arc = 31.074 m (101.95') | | (540)-(541) | S 88° 50' 17" W | 20.501 m (67.26') |
| (532)-(533) | S 66° 13' 01" E | 44.991 m (147.61') | (541)-(542) | N 88° 50' 17" E | 9.979 m (32.74') |
| (533)-(534) | Rc = 53.340 m (175.00') Arc = 46.445 m (152.38') | | (542)-(543) | S 05° 46' 30" E | 45.219 m (148.36') |
| (534)-(535) | S 54° 55' 46" E | 23.959 m (78.60') | (543)-(544) | N 90° 00' 00" W | 23.940 m (78.54') |
| (535)-(536) | Rc = 53.340 m (175.00') Arc = 24.166 m (79.28') | | (544)-(545) | N 45° 08' 28" W | 14.347 m (47.07') |
| (536)-(537) | N 50° 40' 02" E | 70.173 m (230.23') | (545)-(546) | N 01° 09' 41" W | 57.912 m (190.00') |
| (537)-(538) | Rc = 824.422 m (2704.79') Arc = 70.194 m (230.29') | | (546)-(547) | N 89° 31' 48" E | 4.182 m (13.72') |
| (538)-(539) | S 48° 02' 19" W | 2.119 m (6.95') | (547)-(548) | S 89° 37' 48" W | 6.204 m (20.35') |
| (539)-(540) | Rc = 824.422 m (2704.79') Arc = 2.119 m (6.95') | | (548)-(549) | N 89° 39' 41" E | 4.664 m (15.30') |
| (540)-(541) | N 53° 18' 04" E | 5.862 m (19.23') | (549)-(550) | N 01° 09' 41" W | 63.610 m (208.69') |
| (541)-(542) | Rc = 824.422 m (2704.79') Arc = 5.862 m (19.23') | | (550)-(551) | S 89° 39' 41" W | 6.134 m (20.12') |
| (542)-(543) | N 65° 44' 08" W | 30.099 m (98.75') | (551)-(552) | N 89° 39' 41" E | 91.202 m (299.22') |
| (543)-(544) | Rc = 35.052 m (115.00') Arc = 31.110 m (102.07') | | (552)-(553) | S 02° 09' 28" E | 29.000 m (95.14') |
| (544)-(545) | | | (553)-(554) | S 01° 05' 34" E | 34.639 m (113.64') |
| (545)-(546) | | | (554)-(555) | S 01° 05' 34" E | 23.220 m (76.18') |
| (546)-(547) | | | (555)-(556) | S 89° 31' 48" W | 60.938 m (199.93') |
| (547)-(548) | | | (556)-(557) | S 89° 37' 48" W | 21.847 m (71.83') |
| (548)-(549) | | | (557)-(558) | S 89° 37' 48" W | 2.829 m (9.28') |
| (549)-(550) | | | (558)-(559) | N 01° 04' 26" W | 18.613 m (61.25') |
| (550)-(551) | | | (559)-(560) | N 88° 51' 15" E | 8.002 m (26.25') |
| (551)-(552) | | | (560)-(561) | N 01° 42' 56" W | 10.355 m (33.97') |
| (552)-(553) | | | (561)-(562) | S 87° 51' 16" W | 6.945 m (22.79') |
| (553)-(554) | | | (562)-(563) | N 01° 09' 41" W | 21.117 m (69.28') |
| (554)-(555) | | | (563)-(564) | N 84° 13' 44" E | 25.866 m (84.86') |
| (555)-(556) | | | (564)-(565) | N 89° 44' 08" E | 14.231 m (46.69') |
| (556)-(557) | | | (565)-(566) | S 89° 44' 08" W | 5.942 m (19.49') |
| (557)-(558) | | | (566)-(567) | N 89° 44' 08" E | 80.411 m (263.82') |
| (558)-(559) | | | (567)-(568) | S 05° 48' 50" E | 4.662 m (15.30') |
| (559)-(560) | | | (568)-(569) | S 00° 12' 35" E | 15.231 m (49.97') |
| (560)-(561) | | | (569)-(570) | N 84° 11' 53" E | 9.991 m (32.78') |
| (561)-(562) | | | (570)-(571) | S 05° 48' 20" E | 15.027 m (49.30') |
| (562)-(563) | | | (571)-(572) | S 01° 09' 41" E | 57.097 m (187.33') |
| (563)-(564) | | | (572)-(573) | S 01° 09' 41" E | 18.288 m (60.00') |
| (564)-(565) | | | (573)-(574) | N 01° 05' 34" W | 57.914 m (190.00') |
| (565)-(566) | | | (574)-(575) | N 01° 10' 10" W | 84.734 m (278.00') |

LEGEND

- SECTION CORNER
- FOUND IRON ROD OR PIPE
- FOUND RIGHT OF WAY MARKER
- ACCESS CONTROL
- RECORD DATA
- ENGLISH EQUIVALENT
- EXISTING TREE
- DIAMETER (INCHES)

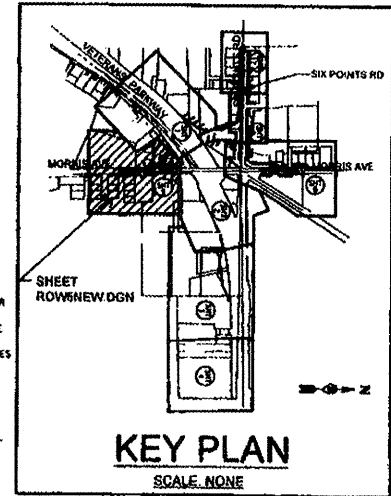
NOTE:
 BEARINGS AND GRID DISTANCES
 BASED ON STATE PLANE
 COORDINATES NAD 83, EAST ZONE.
 GRID FACTOR = 1.00001605
 1 METER = 3.28083333 FEET.

SURVEYOR'S STATEMENT

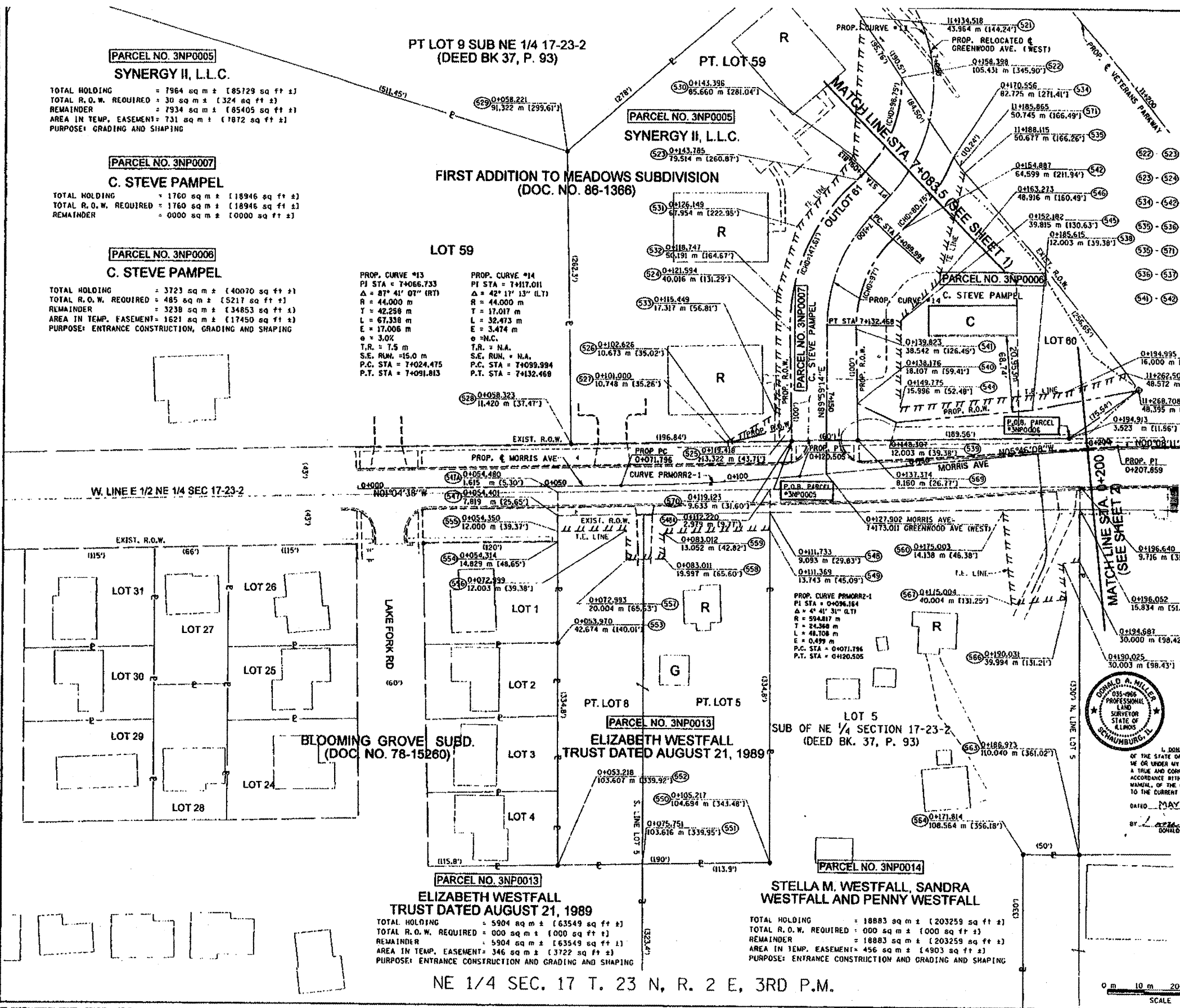
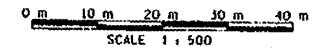
I, DONALD A. MILLER, HEREBY STATE THAT I AM A PROFESSIONAL LAND SURVEYOR OF THE STATE OF ILLINOIS THAT THE SURVEY OF PROPOSED FAP ROUTE TO A WAS MADE BY ME OR UNDER MY DIRECT SUPERVISION THAT THE LAND CORNER TIES AS SHOWN HEREON ARE A TRUE AND CORRECT REPRESENTATION OF SAID SURVEY AND THAT THE PLAT HEREON IS IN ACCORDANCE WITH APPLICABLE SECTIONS OF THE LAND ACQUISITION POLICIES AND PROCEDURES MANUAL OF THE DEPARTMENT'S DISTRICT OFFICES. THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS SURVEYING STANDARDS FOR A BOUNDARY SURVEY.

DATED: MAY 10, 2007

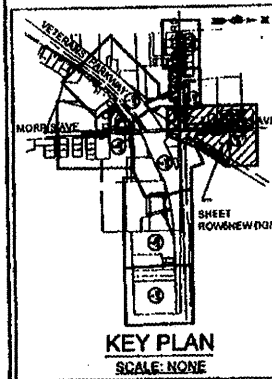
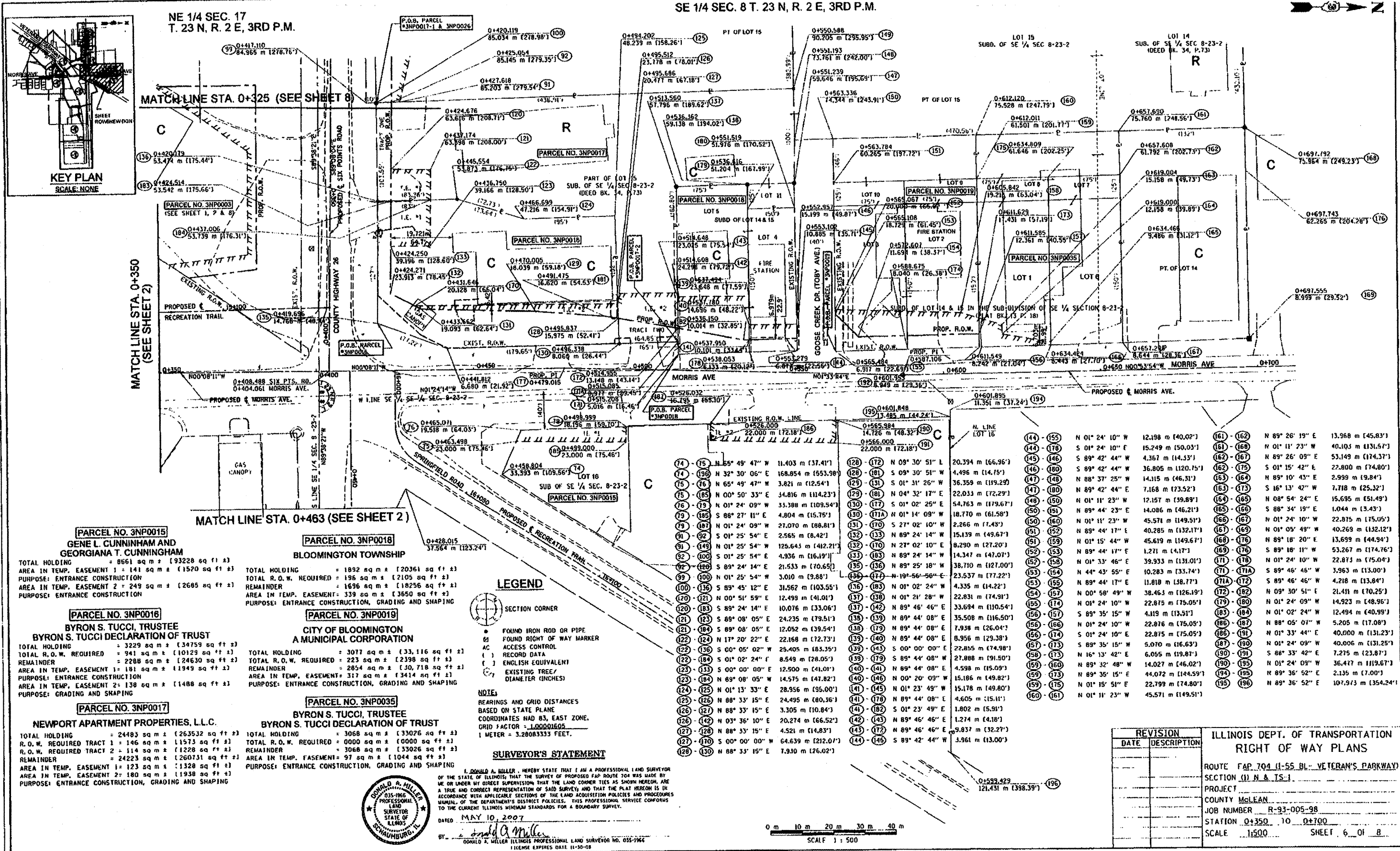
BY: *Donald A. Miller*
 DONALD A. MILLER, ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-1966
 LICENSE EXPIRES DATE 11-30-08



| REVISION | | ILLINOIS DEPT. OF TRANSPORTATION RIGHT OF WAY PLANS |
|----------|-------------|--|
| DATE | DESCRIPTION | |
| | | ROUTE FAP 104 (I-55 BL - VETERAN'S PARKWAY) |
| | | SECTION (11 N & TS-1) |
| | | PROJECT |
| | | COUNTY McLEAN |
| | | JOB NUMBER R-93-005-98 |
| | | STATION 0+000 TO 0+200 |
| | | SCALE 1:500 SHEET 5 OF 8 |



SE 1/4 SEC. 8 T. 23 N, R. 2 E, 3RD P.M.



NE 1/4 SEC. 17
T. 23 N, R. 2 E, 3RD P.M.

MATCH LINE STA. 0+325 (SEE SHEET 8)

MATCH LINE STA. 0+350
(SEE SHEET 2)

MATCH LINE STA. 0+463 (SEE SHEET 2)

PARCEL NO. 3NP0015
GENE L. CUNNINGHAM AND
GEORGIANA T. CUNNINGHAM
TOTAL HOLDING = 8661 sq m ± [93228 sq ft ±]
AREA IN TEMP. EASEMENT 1 = 141 sq m ± [1570 sq ft ±]
PURPOSE: ENTRANCE CONSTRUCTION
AREA IN TEMP. EASEMENT 2 = 249 sq m ± [2685 sq ft ±]
PURPOSE: ENTRANCE CONSTRUCTION

PARCEL NO. 3NP0016
BYRON S. TUCCI, TRUSTEE
BYRON S. TUCCI DECLARATION OF TRUST
TOTAL HOLDING = 3229 sq m ± [34759 sq ft ±]
TOTAL R.O.W. REQUIRED = 941 sq m ± [10129 sq ft ±]
REMAINDER = 2288 sq m ± [24630 sq ft ±]
AREA IN TEMP. EASEMENT 1 = 181 sq m ± [1949 sq ft ±]
PURPOSE: ENTRANCE CONSTRUCTION
AREA IN TEMP. EASEMENT 2 = 138 sq m ± [1488 sq ft ±]
PURPOSE: GRADING AND SHAPING

PARCEL NO. 3NP0017
NEWPORT APARTMENT PROPERTIES, L.L.C.
TOTAL HOLDING = 24483 sq m ± [263532 sq ft ±]
R.O.W. REQUIRED TRACT 1 = 146 sq m ± [1573 sq ft ±]
R.O.W. REQUIRED TRACT 2 = 114 sq m ± [1228 sq ft ±]
REMAINDER = 24223 sq m ± [260731 sq ft ±]
AREA IN TEMP. EASEMENT 1 = 123 sq m ± [1328 sq ft ±]
AREA IN TEMP. EASEMENT 2 = 180 sq m ± [1938 sq ft ±]
PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

PARCEL NO. 3NP0018
BLOOMINGTON TOWNSHIP
TOTAL HOLDING = 1892 sq m ± [20361 sq ft ±]
TOTAL R.O.W. REQUIRED = 196 sq m ± [2105 sq ft ±]
REMAINDER = 1696 sq m ± [18256 sq ft ±]
AREA IN TEMP. EASEMENT = 339 sq m ± [3650 sq ft ±]
PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

PARCEL NO. 3NP0019
CITY OF BLOOMINGTON
A MUNICIPAL CORPORATION
TOTAL HOLDING = 3077 sq m ± [33,116 sq ft ±]
TOTAL R.O.W. REQUIRED = 223 sq m ± [2398 sq ft ±]
REMAINDER = 2854 sq m ± [30,718 sq ft ±]
AREA IN TEMP. EASEMENT = 317 sq m ± [3414 sq ft ±]
PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

PARCEL NO. 3NP0035
BYRON S. TUCCI, TRUSTEE
BYRON S. TUCCI DECLARATION OF TRUST
TOTAL HOLDING = 3068 sq m ± [33076 sq ft ±]
TOTAL R.O.W. REQUIRED = 0000 sq m ± [0000 sq ft ±]
REMAINDER = 3068 sq m ± [33076 sq ft ±]
AREA IN TEMP. EASEMENT = 97 sq m ± [1044 sq ft ±]
PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

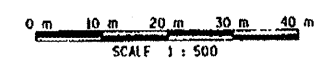
LEGEND

- SECTION CORNER
- FOUND IRON ROD OR PIPE
- FOUND RIGHT OF WAY MARKER
- ACCESS CONTROL
- RECORD DATA
- ENGLISH EQUIVALENT
- EXISTING TREE / DIAMETER (INCHES)

NOTE:
BEARINGS AND GRID DISTANCES
BASED ON STATE PLANE
COORDINATES HAD 83, EAST ZONE.
GRID FACTOR = 1.00001605
1 METER = 3.28083333 FEET.

SURVEYOR'S STATEMENT
I, DONALD A. MILLER, HEREBY STATE THAT I AM A PROFESSIONAL LAND SURVEYOR OF THE STATE OF ILLINOIS, THAT THE SURVEY OF PROPOSED FAP ROUTE 704 WAS MADE BY ME OR UNDER MY DIRECT SUPERVISION THAT THE LAND CORNER TIES AS SHOWN HEREON, ARE A TRUE AND CORRECT REPRESENTATION OF SAID SURVEY, AND THAT THE PLAT HEREON IS IN ACCORDANCE WITH APPLICABLE SECTIONS OF THE LAND ACQUISITION POLICIES AND PROCEDURES MANUAL OF THE DEPARTMENT'S DISTRICT POLICIES. THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
DATED MAY 10, 2007
BY: *Donald A. Miller*
DONALD A. MILLER ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-1966
LICENSE EXPIRES DATE 11-30-08

| | | | | | | | | | | | |
|--------------|-----------------|---------------------|---------------|-----------------|--------------------|---------------|-----------------|--------------------|----------------|-----------------|---------------------|
| (14) - (15) | N 55° 49' 47" W | 11.403 m [37.41'] | (128) - (127) | N 09° 30' 51" L | 20.394 m [66.96'] | (144) - (155) | N 01° 24' 10" W | 12.198 m [40.02'] | (161) - (162) | N 89° 26' 19" E | 13.968 m [45.83'] |
| (14) - (15) | N 32° 30' 06" E | 168.854 m [553.98'] | (128) - (121) | S 09° 30' 51" W | 4.496 m [14.75'] | (144) - (176) | N 01° 24' 10" E | 15.249 m [50.03'] | (161) - (160) | N 01° 11' 23" W | 40.105 m [131.57'] |
| (15) - (16) | N 65° 49' 47" W | 3.821 m [12.54'] | (129) - (131) | S 01° 31' 26" W | 36.359 m [119.29'] | (145) - (146) | S 89° 42' 44" W | 4.367 m [14.33'] | (162) - (161) | N 89° 26' 09" E | 53.149 m [174.37'] |
| (15) - (16) | N 00° 50' 33" E | 34.816 m [114.23'] | (129) - (181) | N 04° 32' 17" E | 22.033 m [72.29'] | (146) - (180) | S 89° 42' 44" W | 36.805 m [120.75'] | (162) - (175) | S 01° 15' 42" E | 22.800 m [74.80'] |
| (16) - (19) | N 01° 24' 09" W | 33.388 m [109.54'] | (130) - (177) | S 01° 02' 25" E | 54.763 m [179.67'] | (147) - (148) | N 88° 37' 25" W | 14.115 m [46.31'] | (163) - (164) | N 89° 10' 43" E | 2.999 m [9.84'] |
| (19) - (181) | S 88° 27' 11" E | 4.804 m [15.75'] | (130) - (171) | N 01° 14' 09" W | 18.770 m [61.58'] | (148) - (150) | N 89° 42' 44" E | 7.168 m [23.52'] | (163) - (173) | S 16° 13' 42" W | 7.718 m [25.32'] |
| (19) - (181) | N 01° 24' 09" W | 27.070 m [88.81'] | (131) - (170) | S 27° 02' 10" W | 2.266 m [7.43'] | (150) - (191) | N 01° 11' 23" W | 12.157 m [39.89'] | (164) - (165) | N 08° 54' 24" E | 15.635 m [51.49'] |
| (19) - (181) | S 01° 25' 54" E | 2.565 m [8.42'] | (132) - (133) | N 89° 24' 14" W | 15.139 m [49.67'] | (151) - (152) | N 89° 44' 23" E | 14.086 m [46.21'] | (165) - (166) | S 88° 34' 19" E | 1.044 m [3.43'] |
| (19) - (181) | S 01° 25' 54" E | 125.643 m [412.21'] | (132) - (170) | N 27° 02' 10" E | 8.290 m [27.20'] | (152) - (153) | N 01° 11' 23" W | 45.571 m [149.51'] | (166) - (167) | N 01° 24' 10" W | 22.875 m [75.05'] |
| (19) - (181) | S 01° 25' 54" E | 4.936 m [16.19'] | (133) - (183) | N 89° 24' 14" W | 14.347 m [47.07'] | (153) - (154) | N 89° 44' 17" E | 40.285 m [132.17'] | (167) - (169) | N 01° 05' 49" W | 40.269 m [132.12'] |
| (19) - (181) | S 89° 24' 14" E | 21.533 m [70.65'] | (133) - (136) | N 89° 25' 18" W | 38.710 m [127.00'] | (154) - (155) | N 89° 44' 17" E | 45.619 m [149.67'] | (168) - (176) | N 89° 18' 20" E | 13.699 m [44.94'] |
| (19) - (181) | N 01° 25' 54" W | 3.010 m [9.88'] | (134) - (177) | N 19° 56' 56" E | 23.537 m [77.22'] | (155) - (156) | N 01° 33' 46" E | 1.271 m [4.17'] | (169) - (176) | S 89° 18' 11" W | 53.267 m [174.76'] |
| (19) - (181) | S 89° 45' 12" E | 31.562 m [103.55'] | (136) - (183) | N 01° 02' 24" W | 4.335 m [14.22'] | (156) - (158) | N 89° 44' 17" E | 39.933 m [131.01'] | (171) - (178) | N 01° 24' 10" W | 22.873 m [75.04'] |
| (19) - (181) | N 00° 51' 59" E | 12.499 m [41.01'] | (137) - (138) | N 01° 21' 28" W | 22.831 m [74.91'] | (157) - (158) | N 44° 43' 55" E | 10.283 m [33.74'] | (171) - (171A) | S 89° 46' 46" W | 3.963 m [13.00'] |
| (19) - (181) | S 89° 24' 14" E | 10.076 m [33.06'] | (137) - (142) | N 89° 46' 46" E | 33.694 m [110.54'] | (158) - (159) | N 89° 44' 17" E | 11.818 m [38.77'] | (171A) - (172) | N 01° 24' 10" W | 22.873 m [75.04'] |
| (19) - (181) | S 89° 08' 05" E | 24.235 m [79.51'] | (138) - (139) | N 89° 44' 08" E | 35.508 m [116.50'] | (159) - (160) | N 01° 24' 10" W | 22.875 m [75.05'] | (172) - (182) | N 09° 30' 51" E | 21.411 m [70.25'] |
| (19) - (181) | S 89° 08' 05" E | 12.052 m [39.54'] | (138) - (179) | N 89° 44' 08" E | 7.938 m [26.04'] | (160) - (161) | N 01° 24' 10" W | 22.875 m [75.05'] | (172) - (182) | N 01° 02' 24" W | 12.494 m [40.99'] |
| (19) - (181) | N 17° 20' 22" E | 22.168 m [72.73'] | (139) - (140) | N 89° 44' 08" E | 8.956 m [29.38'] | (161) - (162) | S 89° 35' 15" W | 5.070 m [16.63'] | (173) - (180) | N 01° 24' 09" W | 14.923 m [48.96'] |
| (19) - (181) | S 00° 05' 02" W | 25.405 m [83.35'] | (139) - (143) | S 00° 00' 00" E | 22.855 m [74.98'] | (162) - (163) | N 01° 24' 10" W | 22.875 m [75.05'] | (183) - (184) | N 01° 02' 24" W | 12.494 m [40.99'] |
| (19) - (181) | S 01° 02' 24" E | 8.549 m [28.05'] | (139) - (179) | N 89° 44' 08" E | 27.888 m [91.50'] | (163) - (164) | N 01° 24' 10" W | 22.876 m [75.05'] | (185) - (187) | N 88° 05' 07" W | 5.205 m [17.08'] |
| (19) - (181) | S 00° 00' 00" E | 12.500 m [41.01'] | (140) - (141) | N 89° 44' 08" E | 4.598 m [15.09'] | (164) - (165) | S 89° 35' 15" W | 5.070 m [16.63'] | (187) - (190) | N 01° 24' 09" W | 40.006 m [131.25'] |
| (19) - (181) | N 89° 08' 05" W | 14.575 m [47.82'] | (140) - (146) | N 00° 20' 09" W | 15.186 m [49.82'] | (165) - (166) | N 16° 13' 42" E | 6.055 m [19.87'] | (190) - (191) | S 88° 33' 42" E | 7.275 m [23.87'] |
| (19) - (181) | N 01° 13' 33" E | 28.956 m [95.00'] | (141) - (145) | N 01° 23' 49" W | 15.178 m [49.80'] | (166) - (167) | N 89° 32' 48" W | 14.027 m [46.02'] | (190) - (195) | N 01° 24' 09" W | 36.477 m [119.67'] |
| (19) - (181) | N 88° 33' 15" E | 24.495 m [80.36'] | (141) - (178) | N 89° 44' 08" E | 4.605 m [15.11'] | (167) - (168) | N 89° 35' 15" E | 44.072 m [144.59'] | (194) - (195) | N 89° 36' 52" E | 2.135 m [7.00'] |
| (19) - (181) | N 03° 36' 10" E | 20.274 m [66.52'] | (142) - (143) | N 89° 46' 46" E | 1.802 m [5.91'] | (168) - (169) | N 01° 15' 51" E | 22.799 m [74.80'] | (195) - (196) | N 89° 36' 52" E | 107.913 m [354.24'] |
| (19) - (181) | N 88° 33' 15" E | 4.521 m [14.83'] | (143) - (172) | N 89° 46' 46" E | 9.837 m [32.27'] | (169) - (170) | N 01° 11' 23" W | 45.571 m [149.51'] | | | |
| (19) - (181) | S 00° 00' 00" W | 64.639 m [212.07'] | (144) - (146) | S 89° 42' 44" W | 3.961 m [13.00'] | | | | | | |
| (19) - (181) | N 88° 33' 15" E | 7.930 m [26.02'] | | | | | | | | | |



| REVISION | | ILLINOIS DEPT. OF TRANSPORTATION RIGHT OF WAY PLANS |
|----------|-------------|--|
| DATE | DESCRIPTION | |
| | | ROUTE FAP 704 (I-55 BL - VETERAN'S PARKWAY) |
| | | SECTION (1) N & TS-1 |
| | | PROJECT |
| | | COUNTY McLEAN |
| | | JOB NUMBER R-93-005-98 |
| | | STATION 0+350 TO 0+700 |
| | | SCALE 1:500 SHEET 6 OF 8 |

"MONUMENT RECORD" 95-12491
 LAND SURVEY MONUMENT SITUATED IN SECTION 8, TOWNSHIP 23 NORTH, RANGE 2 EAST, OF THE 3RD P.M., MOLEMAN COUNTY, ILLINOIS.
 RUTH WEBER, Recorder
 McLean County, IL
 FILED

SPACE RESERVED FOR RECORDING OFFICER

RECOVER THE DRAWING

RECOVER THE DRAWING

RECOVER THE DRAWING

STATE OF ILLINOIS
 COUNTY OF MOLEMAN

"MONUMENT RECORD" 91 14123
 LAND SURVEY MONUMENT SITUATED IN SECTION 8, TOWNSHIP 23 NORTH, RANGE 2 EAST, OF THE 3RD P.M., MOLEMAN COUNTY, ILLINOIS.
 MONUMENT DESCRIPTION AND REMARKS: SET 3" IRON PIPE W/CHD CAP FROM TIES ON PLAT OF SZAREK SUB. CORRECTS MON RECORD PREVIOUSLY RECORDED ENTRANCE SETTING PI NAIL IN ROAD AS CORNER

RECOVER THE DRAWING

RECOVER THE DRAWING

STATE OF ILLINOIS
 COUNTY OF MOLEMAN

PARCEL NO. 3NP0020
RONALD E. FOGGY AND CHERYL A. FOGGY
 TOTAL HOLDING = 980 sq m ± [10549 sq ft ±]
 TOTAL R.O.W. REQUIRED = 83 sq m ± [893 sq ft ±]
 REMAINDER = 897 sq m ± [9656 sq ft ±]
 AREA IN TEMP. EASEMENT = 98 sq m ± [1054 sq ft ±]
 PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

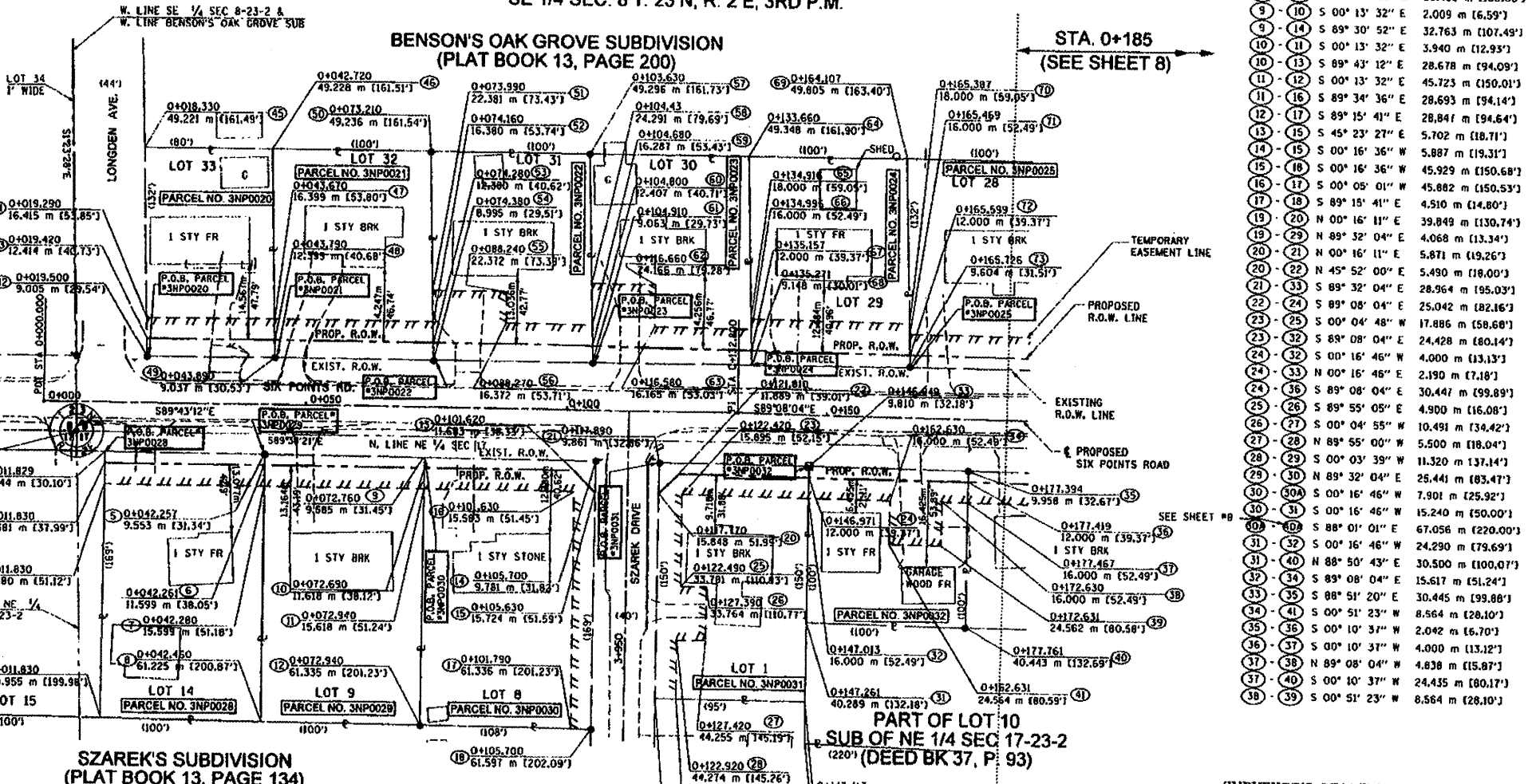
PARCEL NO. 3NP0021
ROB GORHAM AND FRANCES GORHAM
 TOTAL HOLDING = 1276 sq m ± [13199 sq ft ±]
 TOTAL R.O.W. REQUIRED = 103 sq m ± [1107 sq ft ±]
 REMAINDER = 1173 sq m ± [12092 sq ft ±]
 AREA IN TEMP. EASEMENT = 122 sq m ± [1313 sq ft ±]
 PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

PARCEL NO. 3NP0022
ROBERT E. GASS AND LINDA L. GASS
 TOTAL HOLDING = 1226 sq m ± [13199 sq ft ±]
 TOTAL R.O.W. REQUIRED = 101 sq m ± [1085 sq ft ±]
 REMAINDER = 1125 sq m ± [12114 sq ft ±]
 AREA IN TEMP. EASEMENT = 208 sq m ± [2238 sq ft ±]
 PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

PARCEL NO. 3NP0023
LARRY JAMES BROWN AND JACQUELINE J. BROWN
 TOTAL HOLDING = 1227 sq m ± [13204 sq ft ±]
 TOTAL R.O.W. REQUIRED = 93 sq m ± [996 sq ft ±]
 REMAINDER = 1134 sq m ± [12208 sq ft ±]
 AREA IN TEMP. EASEMENT = 219 sq m ± [2352 sq ft ±]
 PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

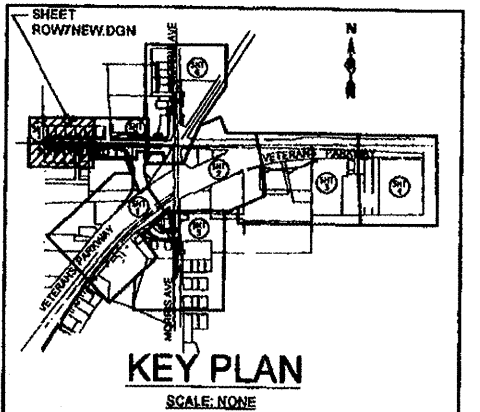
PARCEL NO. 3NP0024
DEAN A. SEELY AND BECKY A. SEELY
 TOTAL HOLDING = 1225 sq m ± [13185 sq ft ±]
 TOTAL R.O.W. REQUIRED = 80 sq m ± [861 sq ft ±]
 REMAINDER = 1145 sq m ± [12324 sq ft ±]
 AREA IN TEMP. EASEMENT = 183 sq m ± [1970 sq ft ±]
 PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

PARCEL NO. 3NP0025
ANDRE TAYLOR AND ELIZABETH A. SCHNEIDER
 (SEE DATA ON SHT 8)



| | | | |
|----|----|-----------------|--------------------|
| 1 | 2 | N 00° 16' 42" E | 45.375 m (148.87') |
| 1 | 8 | S 89° 12' 56" E | 30.636 m (100.51') |
| 2 | 3 | N 00° 16' 42" E | 4.000 m (13.12') |
| 2 | 7 | S 89° 41' 02" E | 30.452 m (99.91') |
| 3 | 4 | N 00° 02' 57" W | 2.136 m (7.01') |
| 3 | 6 | S 89° 43' 12" E | 30.437 m (99.86') |
| 4 | 5 | S 89° 30' 52" E | 30.428 m (99.82') |
| 5 | 9 | S 89° 30' 52" E | 2.100 m (6.89') |
| 5 | 8 | S 89° 30' 52" E | 30.684 m (100.67') |
| 6 | 1 | S 00° 02' 57" W | 3.950 m (12.96') |
| 6 | 10 | S 89° 43' 12" E | 30.670 m (100.62') |
| 7 | 11 | S 00° 02' 57" W | 45.625 m (149.69') |
| 7 | 12 | S 89° 41' 05" E | 30.660 m (100.59') |
| 8 | 13 | S 89° 30' 52" E | 30.480 m (100.00') |
| 9 | 14 | S 00° 13' 32" E | 2.009 m (6.59') |
| 9 | 15 | S 89° 30' 52" E | 32.763 m (107.49') |
| 10 | 16 | S 00° 13' 32" E | 3.940 m (12.93') |
| 10 | 17 | S 89° 43' 12" E | 28.678 m (94.09') |
| 11 | 18 | S 00° 13' 32" E | 45.723 m (150.01') |
| 11 | 19 | S 89° 34' 36" E | 28.693 m (94.14') |
| 12 | 20 | S 89° 15' 41" E | 28.847 m (94.64') |
| 13 | 21 | S 45° 23' 27" E | 5.702 m (18.71') |
| 14 | 22 | S 00° 16' 36" W | 5.887 m (19.31') |
| 15 | 23 | S 00° 16' 36" W | 45.929 m (150.68') |
| 16 | 24 | S 00° 09' 01" W | 45.882 m (150.53') |
| 17 | 25 | S 89° 15' 41" E | 4.510 m (14.80') |
| 18 | 26 | N 00° 16' 11" E | 39.849 m (130.74') |
| 19 | 27 | N 89° 32' 04" E | 4.068 m (13.34') |
| 20 | 28 | N 00° 16' 11" E | 5.871 m (19.26') |
| 20 | 29 | N 45° 52' 00" E | 5.490 m (18.00') |
| 21 | 30 | S 89° 32' 04" E | 28.964 m (95.03') |
| 22 | 31 | S 89° 08' 04" E | 25.042 m (82.16') |
| 23 | 32 | S 00° 04' 48" W | 17.886 m (58.68') |
| 24 | 33 | S 89° 08' 04" E | 24.428 m (80.14') |
| 24 | 34 | S 00° 16' 46" W | 4.000 m (13.13') |
| 25 | 35 | N 00° 16' 46" E | 2.190 m (7.18') |
| 26 | 36 | S 89° 08' 04" E | 30.447 m (99.89') |
| 27 | 37 | S 89° 55' 05" E | 4.900 m (16.08') |
| 28 | 38 | S 00° 04' 55" W | 10.491 m (34.42') |
| 29 | 39 | N 89° 55' 00" W | 5.500 m (18.04') |
| 30 | 40 | S 00° 03' 39" W | 11.320 m (37.14') |
| 31 | 41 | N 89° 32' 04" E | 25.441 m (83.47') |
| 32 | 42 | S 00° 16' 46" W | 7.901 m (25.92') |
| 33 | 43 | S 00° 16' 46" W | 15.240 m (50.00') |
| 34 | 44 | S 89° 08' 04" E | 67.056 m (220.00') |
| 35 | 45 | S 00° 16' 46" W | 24.290 m (79.69') |
| 36 | 46 | N 89° 50' 43" E | 30.500 m (100.07') |
| 37 | 47 | S 89° 08' 04" E | 15.617 m (51.24') |
| 38 | 48 | S 00° 51' 20" E | 30.445 m (99.88') |
| 39 | 49 | S 00° 10' 31" W | 2.042 m (6.70') |
| 40 | 50 | S 00° 10' 31" W | 4.000 m (13.12') |
| 41 | 51 | N 89° 08' 04" E | 4.838 m (15.87') |
| 42 | 52 | S 00° 10' 31" W | 24.435 m (80.17') |
| 43 | 53 | S 00° 51' 23" W | 8.564 m (28.10') |

| | | | |
|----|----|---------------------|--------------------|
| 39 | 41 | N 89° 08' 47" W | 10.000 m (32.81') |
| 40 | 42 | S 89° 01' 23' 31" W | 3.411 m (11.19') |
| 41 | 43 | S 89° 47' 39" E | 24.383 m (80.00') |
| 42 | 44 | N 01° 23' 31" W | 4.001 m (13.13') |
| 43 | 45 | S 89° 43' 12" E | 24.385 m (80.00') |
| 44 | 46 | N 01° 23' 31" W | 32.822 m (107.69') |
| 45 | 47 | S 89° 41' 05" E | 24.386 m (80.00') |
| 46 | 48 | S 89° 44' 06" E | 24.393 m (80.03') |
| 47 | 49 | S 01° 22' 44" E | 32.843 m (107.75') |
| 48 | 50 | S 89° 44' 06" E | 30.492 m (100.04') |
| 49 | 51 | S 01° 22' 44" E | 4.001 m (13.13') |
| 50 | 52 | S 89° 41' 05" E | 30.492 m (100.04') |
| 51 | 53 | S 01° 22' 44" E | 3.364 m (11.04') |
| 52 | 54 | S 89° 43' 12" E | 30.491 m (100.04') |
| 53 | 55 | S 89° 38' 35" E | 30.493 m (100.04') |
| 54 | 56 | S 01° 22' 39" E | 26.867 m (88.15') |
| 55 | 57 | S 89° 50' 01" E | 30.418 m (99.80') |
| 56 | 58 | S 01° 22' 39" E | 6.003 m (19.69') |
| 57 | 59 | S 89° 41' 05" E | 14.251 m (46.76') |
| 58 | 60 | S 01° 22' 39" E | 4.001 m (13.13') |
| 59 | 61 | S 01° 22' 39" E | 3.386 m (11.11') |
| 60 | 62 | S 89° 43' 12" E | 30.525 m (100.15') |
| 61 | 63 | S 89° 50' 51" E | 30.529 m (100.16') |
| 62 | 64 | S 00° 04' 01" W | 6.000 m (19.68') |
| 63 | 65 | S 89° 25' 26" E | 16.414 m (53.85') |
| 64 | 66 | S 01° 32' 10" E | 25.019 m (82.08') |
| 65 | 67 | N 89° 47' 43" E | 30.492 m (100.04') |
| 66 | 68 | S 01° 32' 10" E | 8.007 m (26.27') |
| 67 | 69 | S 89° 08' 04" E | 12.238 m (40.15') |
| 68 | 70 | S 01° 32' 10" E | 4.003 m (13.13') |
| 69 | 71 | S 01° 32' 10" E | 3.224 m (10.58') |
| 70 | 72 | S 89° 08' 04" E | 30.477 m (99.99') |
| 71 | 73 | S 89° 49' 47" E | 30.459 m (99.93') |
| 72 | 74 | S 00° 51' 34" W | 6.001 m (26.25') |
| 73 | 75 | S 89° 08' 04" E | 18.581 m (60.96') |
| 74 | 76 | S 01° 29' 21" E | 31.375 m (102.54') |
| 75 | 77 | S 89° 59' 36" E | 30.492 m (100.04') |
| 76 | 78 | S 01° 29' 21" E | 2.002 m (6.57') |
| 77 | 79 | S 89° 08' 04" E | 30.471 m (99.97') |
| 78 | 80 | S 01° 29' 21" E | 4.003 m (13.13') |
| 79 | 81 | S 89° 50' 30" E | 30.458 m (99.93') |
| 80 | 82 | S 89° 59' 36" E | 31.829 m (104.43') |
| 81 | 83 | S 01° 26' 25" E | 2.000 m (6.56') |
| 82 | 84 | S 01° 26' 25" E | 4.000 m (13.12') |
| 83 | 85 | S 01° 26' 25" E | 2.397 m (7.88') |



PARCEL NO. 3NP0028
PAUL D. MILLER AND NORMA E. MILLER
 TOTAL HOLDING = 1515 sq m ± [16955 sq ft ±]
 TOTAL R.O.W. REQUIRED = 64 sq m ± [694 sq ft ±]
 REMAINDER = 1511 sq m ± [16261 sq ft ±]
 AREA IN TEMP. EASEMENT = 121 sq m ± [1301 sq ft ±]
 PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

PARCEL NO. 3NP0030
DEBORAH A. MITCHELL AND BRUCE E. MITCHELL
 TOTAL HOLDING = 1695 sq m ± [18247 sq ft ±]
 TOTAL R.O.W. REQUIRED = 71 sq m ± [764 sq ft ±]
 REMAINDER = 1624 sq m ± [17483 sq ft ±]
 AREA IN TEMP. EASEMENT = 304 sq m ± [3273 sq ft ±]
 PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

PARCEL NO. 3NP0032
GEORGE M. WILLARD, AS TO A LIFE ESTATE, WITH THE REMAINDER TO KENT WAYNE WILLARD, DEBORAH SUSAN PUMPHREY AND SHERILYN ROSE MALONE, AS TENANTS IN COMMON

PARCEL NO. 3NP0029
BRUCE E. MITCHELL AND DEBORAH A. MITCHELL
 TOTAL HOLDING = 1580 sq m ± [17009 sq ft ±]
 TOTAL R.O.W. REQUIRED = 61 sq m ± [660 sq ft ±]
 REMAINDER = 1519 sq m ± [16349 sq ft ±]
 AREA IN TEMP. EASEMENT = 123 sq m ± [1320 sq ft ±]
 PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

PARCEL NO. 3NP0031
WILLIAM J. DODSON AND MARGARET A. DODSON
 TOTAL HOLDING = 1324 sq m ± [14252 sq ft ±]
 TOTAL R.O.W. REQUIRED = 68 sq m ± [733 sq ft ±]
 REMAINDER = 1256 sq m ± [13519 sq ft ±]
 AREA IN TEMP. EASEMENT = 536 sq m ± [5621 sq ft ±]
 PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

PARCEL NO. 3NP0033
 TOTAL HOLDING = 929 sq m ± [9979 sq ft ±]
 TOTAL R.O.W. REQUIRED = 64 sq m ± [694 sq ft ±]
 REMAINDER = 865 sq m ± [9285 sq ft ±]
 AREA IN TEMP. EASEMENT = 207 sq m ± [2233 sq ft ±]
 PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

SURVEYOR'S STATEMENT
 I, DONALD A. MILLER, HEREBY STATE THAT I AM A PROFESSIONAL LAND SURVEYOR OF THE STATE OF ILLINOIS THAT THE SURVEY OF PROPOSED FAP ROUTE 704 WAS MADE BY ME OR UNDER MY DIRECT SUPERVISION THAT THE LAND CORNER TIES AS SHOWN HEREON ARE A TRUE AND CORRECT REPRESENTATION OF SAID SURVEY; AND THAT THE PLAT HEREON IS IN ACCORDANCE WITH APPLICABLE SECTIONS OF THE LAND ACQUISITION POLICIES AND PROCEDURES TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
 DATED MAY 10, 2007
 DONALD A. MILLER ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-1966
 LICENSE EXPIRES DATE 11-30-08

LEGEND

- SECTION CORNER
- FOUND IRON ROD OR PIPE
- FOUND RIGHT OF WAY MARKER
- ACCESS CONTROL
- RECORD DATA
- ENGLISH EQUIVALENT
- EXISTING TREE/DIAMETER (INCHES)

NOTE:
 BEARINGS AND GRID DISTANCES BASED ON STATE PLANE COORDINATES NAD 83, EAST ZONE. GRID FACTOR = 1.00001605. 1 METER = 3.28083333 FEET.

0 m 10 m 20 m 30 m 40 m
 SCALE 1:500

ILLINOIS DEPT. OF TRANSPORTATION
RIGHT OF WAY PLANS
 ROUTE FAP 704 (I-55 BL - VETERAN'S PARKWAY)
 SECTION (J) N & T-1
 PROJECT
 COUNTY McLEAN
 JOB NUMBER R-93-005-98
 STATION 0+000 TO 0+185
 SCALE 1:500 SHEET 7 OF 8

PARCEL NO. 3NP0024
(SEE DATA ON SHEET 7)

PARCEL NO. 3NP0025
ANDRE TAYLOR AND ELIZABETH A. SCHNEIDER
TOTAL HOLDING = 1226 sq m ± [13197 sq ft ±]
TOTAL R.O.W. REQUIRED = 89 sq m ± [958 sq ft ±]
REMAINDER = 1137 sq m ± [12239 sq ft ±]
AREA IN TEMP. EASEMENT = 231 sq m ± [2486 sq ft ±]
PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

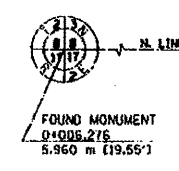
PARCEL NO. 3NP0026
KENNETH H. BUSH
TOTAL HOLDING = 14982 sq m ± [161266 sq ft ±]
TOTAL R.O.W. REQUIRED = 981 sq m ± [10564 sq ft ±]
REMAINDER = 14001 sq m ± [150702 sq ft ±]
AREA IN TEMP. EASEMENT = 1044 sq m ± [11232 sq ft ±]
PURPOSE: GRADING AND SHAPING

PARCEL NO. 3NP0027
ROSE MARIE SZAREK
TOTAL HOLDING = 339 sq m ± [3650 sq ft ±]
TOTAL R.O.W. REQUIRED = 339 sq m ± [3650 sq ft ±]
REMAINDER = 0000 sq m ± [0000 sq ft ±]

BENSON'S OAK GROVE SUBDIVISION
(PLAT BK. 13, P. 200)

LOT 15
SUB OF SE 1/4 SEC 8-23-2
(DEED BOOK 34, P. 73)

"MONUMENT RECORD" 91 14123
LAND SURVEY MONUMENT SITUATED IN SECTION 8, TOWNSHIP 23 NORTH, RANGE 2 EAST, OF THE 13RD PRINCIPAL MERIDIAN, McLEAN COUNTY, ILLINOIS
MONUMENT DESCRIPTION AND REMARKS: SET 3" IRON PIPE MONUMENT FROM TIES ON PLAT OF SZAREK SUB. CORRECTS MONUMENT PREVIOUSLY RECORDED ERROREOUS SETTING PIV MAIL IN ROAD AS CORNER
RECOVERED TIE DRAWING
FO PIPE NORTH 45° 0'
SIX POINTS RD.
MAIL IN CVD TAG IN P. POLE 530' E ROADS
MAIL IN CVD TAG IN P. POLE 530' E ROADS
STATE OF ILLINOIS
COUNTY OF McLEAN
SPACE RESERVED FOR RECORDING OFFICER INDEX CODE C-4
FILED
McLEAN COUNTY, ILLINOIS
AT: McLEAN



PARCEL NO. 3NP0032
(SEE DATA ON SHEET 7)

PARCEL NO. 3NP0033
EDWARD STANLEY SZAREK AND MARILYN JOAN SZAREK
TOTAL HOLDING = 1618 sq m ± [17413 sq ft ±]
TOTAL R.O.W. REQUIRED = 114 sq m ± [1230 sq ft ±]
REMAINDER = 1504 sq m ± [16183 sq ft ±]
AREA IN TEMP. EASEMENT = 229 sq m ± [2468 sq ft ±]
PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

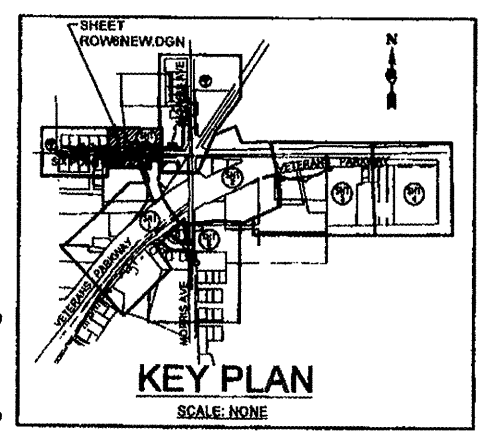
PARCEL NO. 3NP0034
EDWARD STANLEY SZAREK AND MARILYN JOAN SZAREK
TOTAL HOLDING = 1500 sq m ± [16144 sq ft ±]
TOTAL R.O.W. REQUIRED = 646 sq m ± [6953 sq ft ±]
REMAINDER = 854 sq m ± [9191 sq ft ±]
AREA IN TEMP. EASEMENT = 299 sq m ± [3217 sq ft ±]
PURPOSE: ENTRANCE CONSTRUCTION, GRADING AND SHAPING

NE 1/4 SEC. 17 T. 23 N, R. 2 E, 3RD P.M.

PARCEL NO. 3NP0003
EDWARD SZAREK AND MARILYN SZAREK AND JOSEPH SZAREK
(SEE DATA ON SHEET 2)

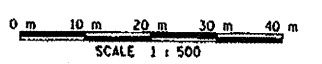


SURVEYOR'S STATEMENT
I, DONALD A. MILLER, HEREBY STATE THAT I AM A PROFESSIONAL LAND SURVEYOR OF THE STATE OF ILLINOIS THAT THE SURVEY OF PROPOSED FAP ROUTE 704 WAS MADE BY ME OR UNDER MY DIRECT SUPERVISION THAT THE SURVEY OF PROPOSED FAP ROUTE 704 WAS MADE BY A TRUE AND CORRECT REPRESENTATION OF SAID SURVEY AND THAT THE PLAT HEREON IS IN ACCORDANCE WITH APPLICABLE SECTIONS OF THE LAND ACQUISITION POLICIES AND PROCEDURES MANUAL OF THE DEPARTMENT'S DISTRICT POLICIES, THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS STATUTE STANDARDS FOR A BOUNDARY SURVEY.
DATED MAY 10, 2007
BY: Donald A. Miller
DONALD A. MILLER, ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-966
LICENSE EXPIRES DATE 11-30-08



LEGEND

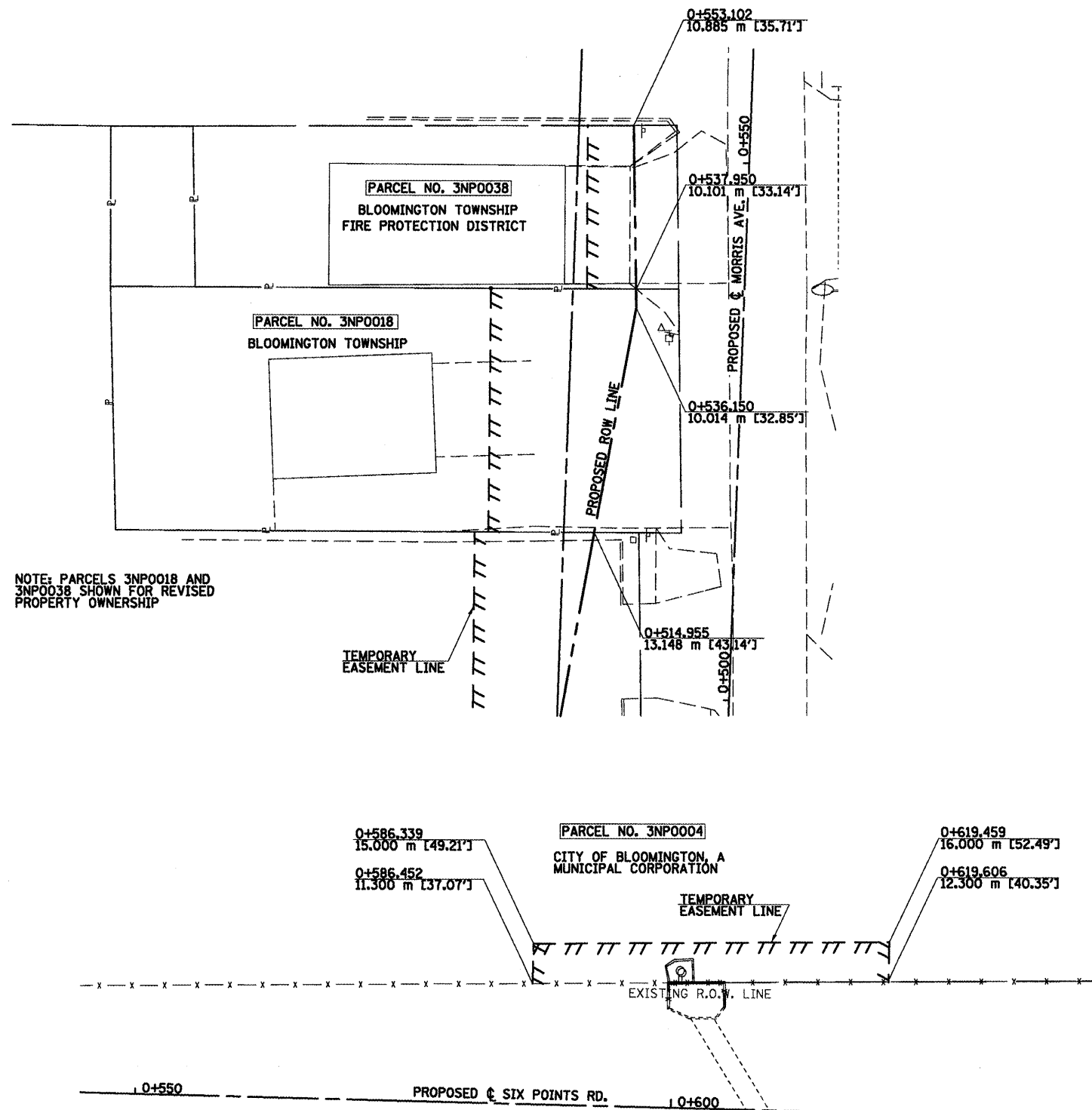
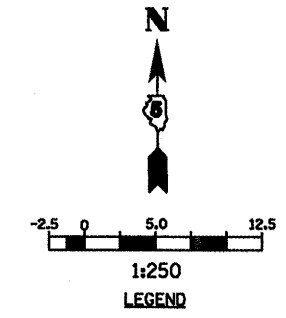
- SECTION CORNER
 - FOUND IRON ROD OR PIPE
 - FOUND RIGHT OF WAY MARKER
 - ACCESS CONTROL
 - RECORD DATA
 - ENGLISH EQUIVALENT
 - EXISTING TREE/DIAMETER (INCHES)
- NOTE:
BEARINGS AND GRID DISTANCES BASED ON STATE PLANE COORDINATES NAD 83, EAST ZONE. GRID FACTOR = 1.00001605. 1 METER = 3.28083333 FEET.



ILLINOIS DEPT. OF TRANSPORTATION
RIGHT OF WAY PLANS
ROUTE FAP 704 (I-55 BL. VETERAN'S PARKWAY)
SECTION (11 N & TS-1)
PROJECT
COUNTY McLEAN
JOB NUMBER R-93-005-98
STATION 0+160 TO 0+325
SCALE 1:500 SHEET 8 OF 8

| | | | |
|-----|-----|-----------------|---------------------|
| 12 | 86 | S 89° 08' 04" E | 24.401 m [80.06'] |
| 13 | 83 | S 89° 23' 27" E | 30.462 m [99.94'] |
| 80 | 81 | S 01° 23' 49" E | 11.379 m [37.33'] |
| 81 | 82 | S 01° 23' 49" E | 20.724 m [67.99'] |
| 81 | 84 | N 89° 55' 29" W | 5.333 m [17.50'] |
| 82 | 83 | S 01° 23' 49" E | 8.132 m [26.68'] |
| 82 | 86 | S 45° 51' 52" W | 8.295 m [27.21'] |
| 84 | 85 | S 00° 04' 17" W | 22.800 m [73.82'] |
| 87 | 88 | S 89° 55' 37" E | 7.065 m [23.18'] |
| 87 | 96 | S 01° 14' 31" E | 37.328 m [122.47'] |
| 87 | 120 | N 01° 14' 31" W | 43.923 m [144.10'] |
| 88 | 89 | S 00° 04' 16" W | 23.402 m [76.78'] |
| 89 | 90 | S 89° 08' 04" E | 62.952 m [206.54'] |
| 90 | 91 | S 76° 05' 33" E | 44.031 m [144.46'] |
| 91 | 92 | S 01° 25' 54" E | 2.565 m [8.42'] |
| 91 | 123 | N 01° 25' 54" W | 125.644 m [412.22'] |
| 92 | 93 | N 89° 08' 04" W | 32.998 m [108.26'] |
| 92 | 100 | S 01° 25' 54" E | 4.936 m [16.19'] |
| 93 | 117 | N 73° 18' 56" W | 31.181 m [102.30'] |
| 94 | 95 | S 45° 51' 55" W | 14.142 m [46.40'] |
| 94 | 117 | S 89° 08' 04" E | 20.000 m [65.62'] |
| 96 | 96 | N 89° 08' 04" W | 18.964 m [62.22'] |
| 96 | 97 | S 01° 14' 31" E | 5.620 m [18.44'] |
| 97 | 98 | S 01° 14' 31" E | 3.046 m [9.99'] |
| 97 | 98 | N 89° 44' 50" E | 111.978 m [367.38'] |
| 98 | 99 | S 89° 44' 53" E | 111.999 m [367.45'] |
| 99 | 100 | N 01° 25' 54" W | 3.010 m [9.88'] |
| 101 | 102 | S 00° 32' 50" W | 2.258 m [7.41'] |
| 101 | 110 | S 89° 22' 02" E | 49.085 m [161.04'] |
| 102 | 103 | S 00° 32' 50" W | 6.971 m [22.87'] |
| 102 | 107 | S 89° 08' 04" E | 14.349 m [47.08'] |
| 103 | 104 | S 00° 32' 50" W | 21.255 m [69.73'] |
| 103 | 105 | N 89° 55' 00" W | 0.688 m [2.26'] |
| 103 | 109 | S 89° 59' 56" E | 12.274 m [40.27'] |
| 104 | 116 | N 89° 22' 15" E | 31.909 m [104.69'] |
| 105 | 108 | N 72° 32' 21" W | 10.435 m [34.24'] |
| 107 | 109 | S 55° 32' 46" E | 18.077 m [59.31'] |
| 108 | 114 | S 00° 51' 14" E | 2.281 m [7.48'] |
| 109 | 111 | S 31° 19' 28" E | 18.660 m [61.22'] |
| 110 | 113 | S 00° 10' 37" W | 30.480 m [100.00'] |
| 110 | 293 | S 89° 22' 02" E | 122.910 m [403.25'] |
| 111 | 112 | S 21° 47' 34" E | 2.451 m [8.06'] |
| 112 | 115 | N 89° 22' 15" E | 9.070 m [29.76'] |
| 112 | 116 | S 89° 22' 15" W | 8.071 m [26.48'] |
| 112 | 118 | S 21° 48' 06" E | 35.240 m [115.62'] |
| 114 | 115 | S 89° 08' 25" E | 10.729 m [35.20'] |
| 115 | 116 | S 24° 36' 13" E | 21.077 m [69.15'] |
| 116 | 119 | S 24° 36' 17" E | 50.117 m [164.62'] |
| 118 | 119 | S 01° 09' 45" W | 12.816 m [42.05'] |
| 119 | 230 | S 01° 09' 45" W | 52.945 m [173.70'] |
| 120 | 121 | N 07° 30' 29" W | 45.611 m [149.68'] |
| 121 | 122 | N 11° 25' 51" W | 2.535 m [8.32'] |
| 122 | 123 | N 89° 29' 48" E | 116.750 m [383.04'] |
| 281 | 288 | N 00° 00' 00" W | 30.060 m [98.62'] |
| 287 | 288 | N 03° 46' 30" E | 30.332 m [99.51'] |

| | | | | |
|---------------------|------------|------------------|--------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| T04 | (DN & TS-1 | McLEAN | 497 | 211 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | CONTRACT NO. 70514 | |



| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

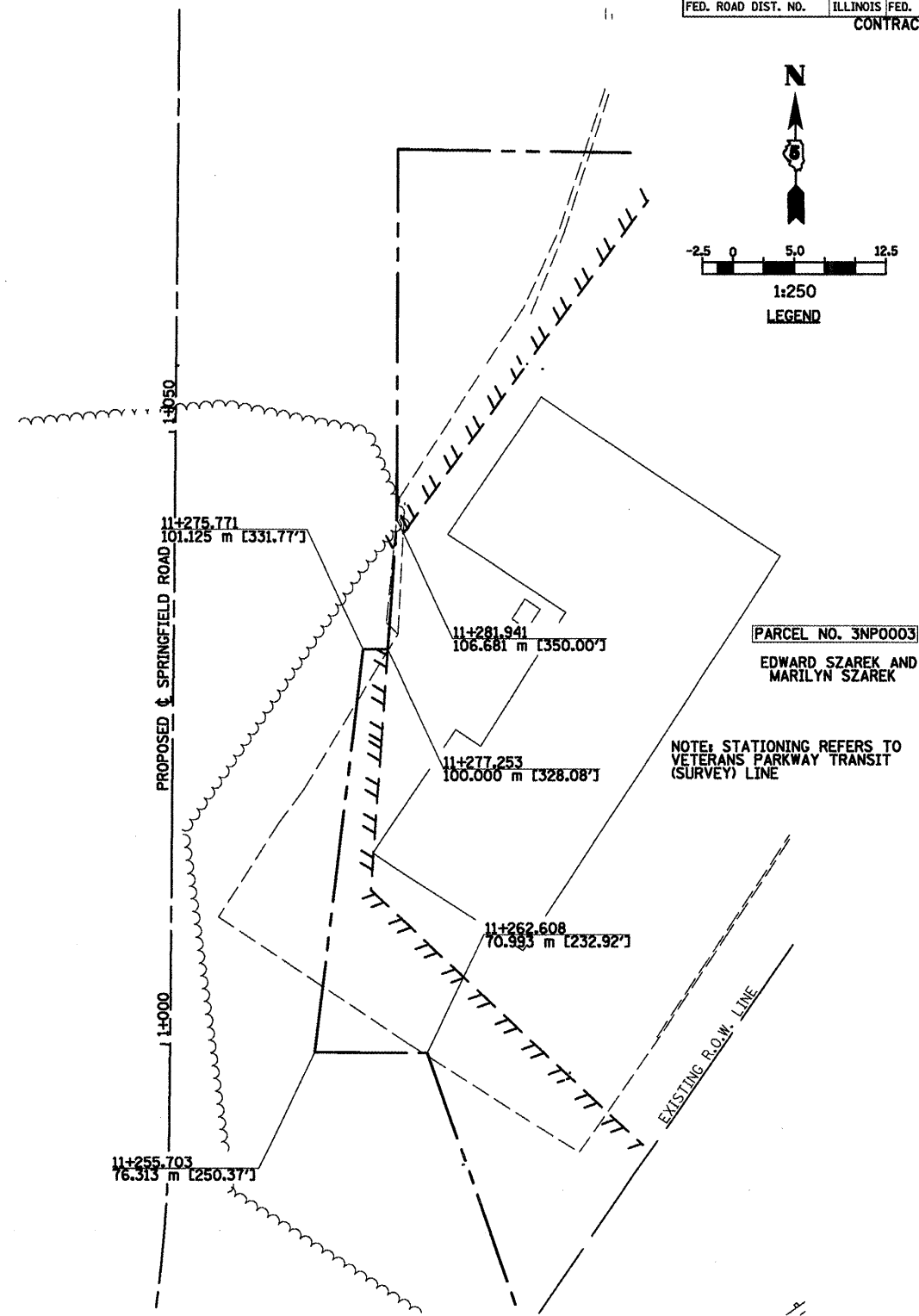
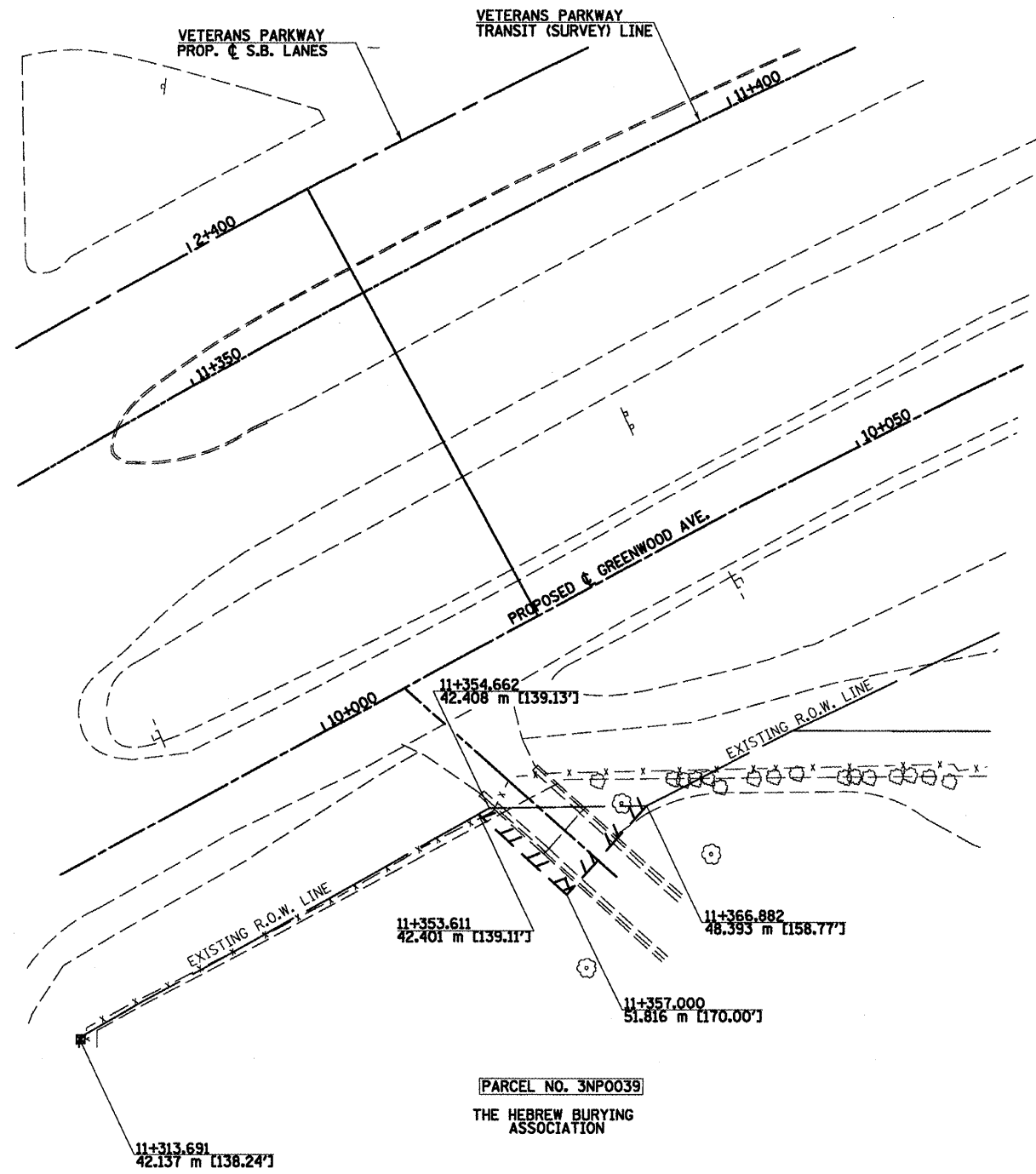
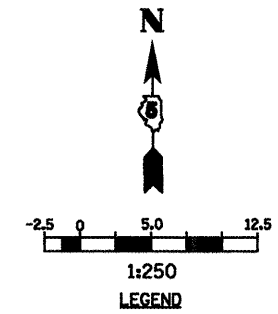
RIGHT OF WAY

SHEET 1 OF 4 DRAWN BY PMH

DATE 05-27-09 CHECKED BY GJS

RIGHT OF WAY REVISIONS

| | | | | |
|---------------------|-------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1)N & TS-1 | McLEAN | 497 | 212 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

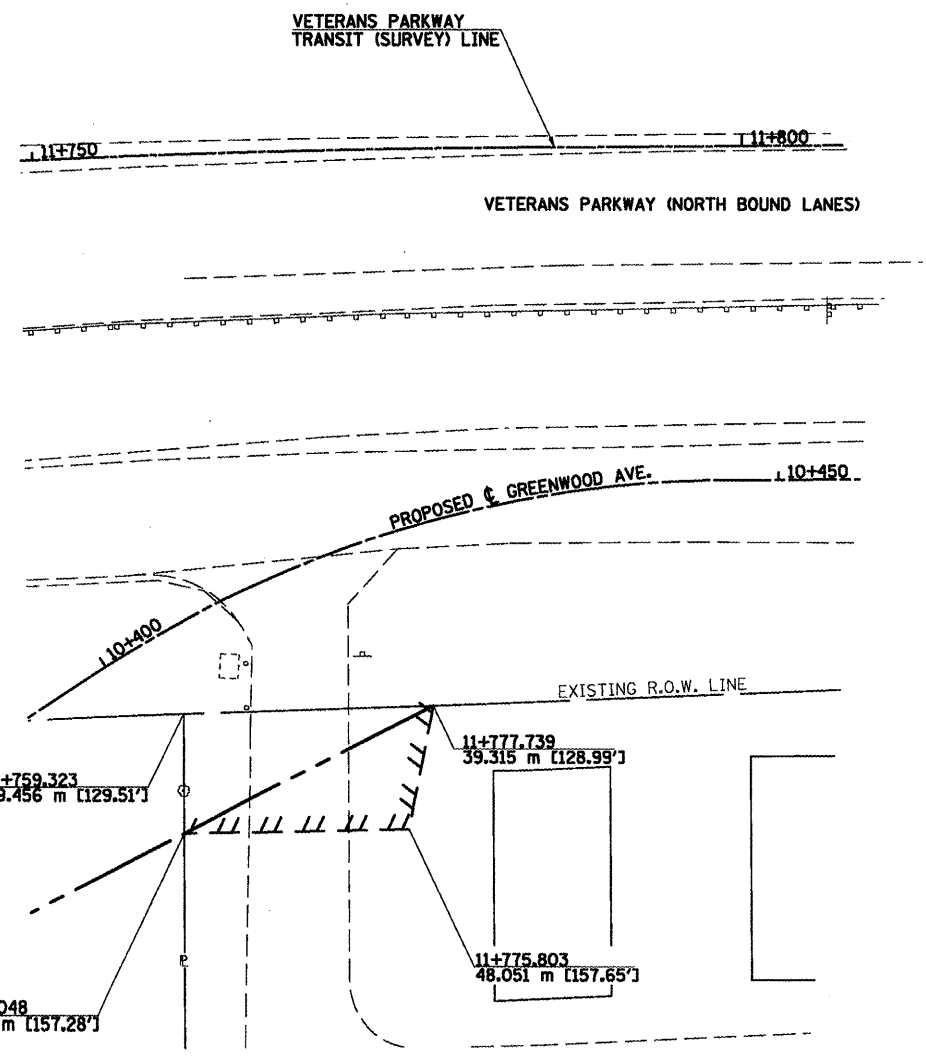
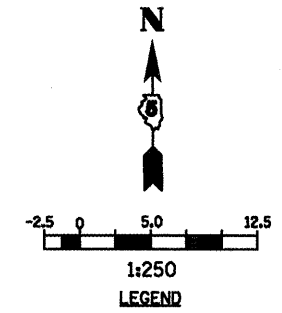
ILLINOIS DEPARTMENT OF TRANSPORTATION

RIGHT OF WAY

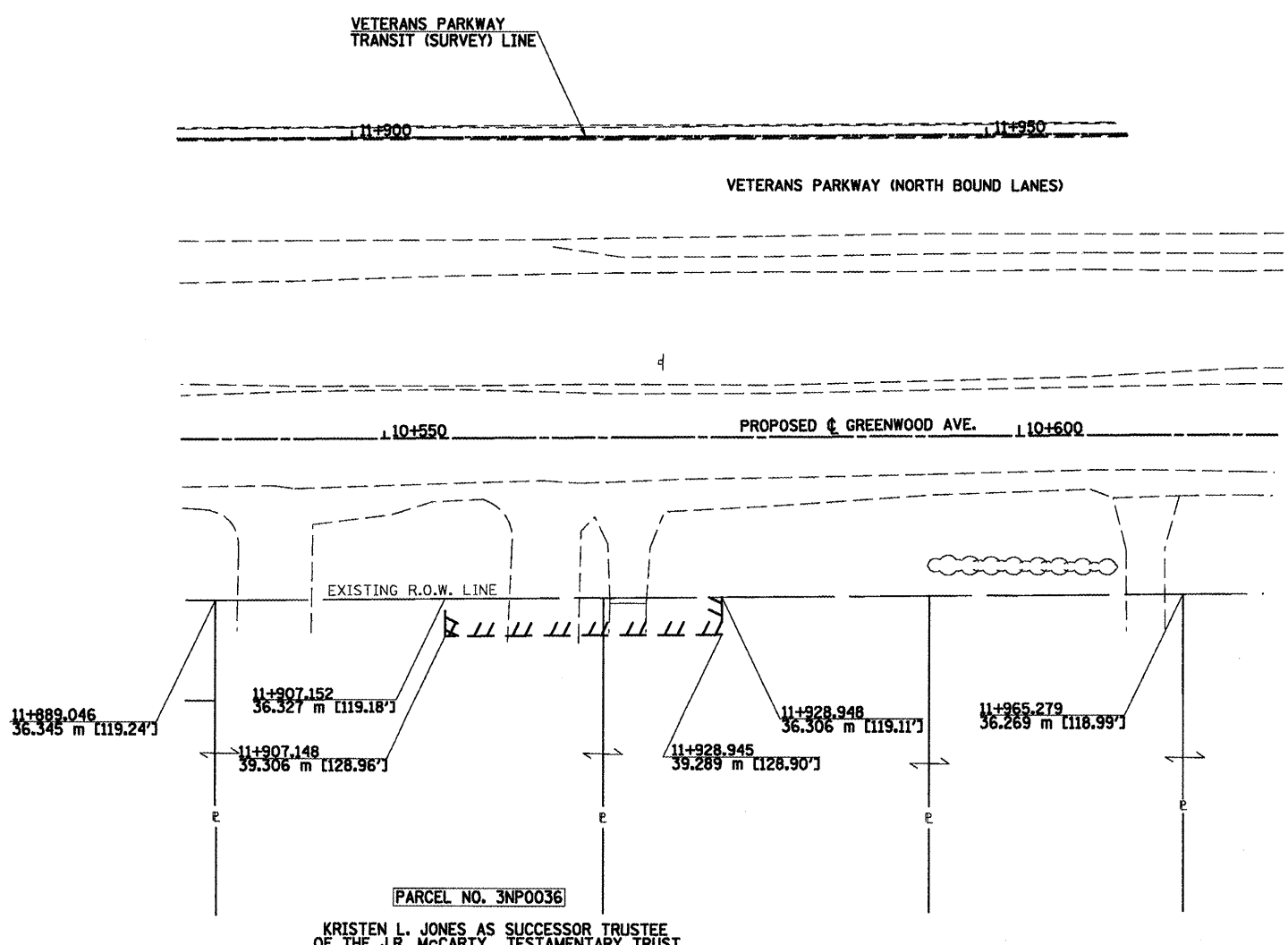
SHEET 2 OF 4 DRAWN BY PMH
DATE 05-27-09 CHECKED BY GJS

RIGHT OF WAY REVISIONS

| | | | | |
|---------------------|------------|------------------|--------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (DN & TS-1 | MCLEAN | 497 | 213 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | CONTRACT NO. 70514 | |



PARCEL NO. 3NP0012
 KRISTEN L. JONES AS SUCCESSOR TRUSTEE
 OF THE J.R. McCARTY TESTAMENTARY TRUST
 CREATED UNDER THE LAST WILL AND
 TESTAMENT OF J.R. McCARTY



PARCEL NO. 3NP0036
 KRISTEN L. JONES AS SUCCESSOR TRUSTEE
 OF THE J.R. McCARTY TESTAMENTARY TRUST
 CREATED UNDER THE LAST WILL AND
 TESTAMENT OF J.R. McCARTY

NOTE: PARCELS 3NP0012 AND
 3NP0036 SHOWN FOR REVISED
 PROPERTY OWNERSHIP

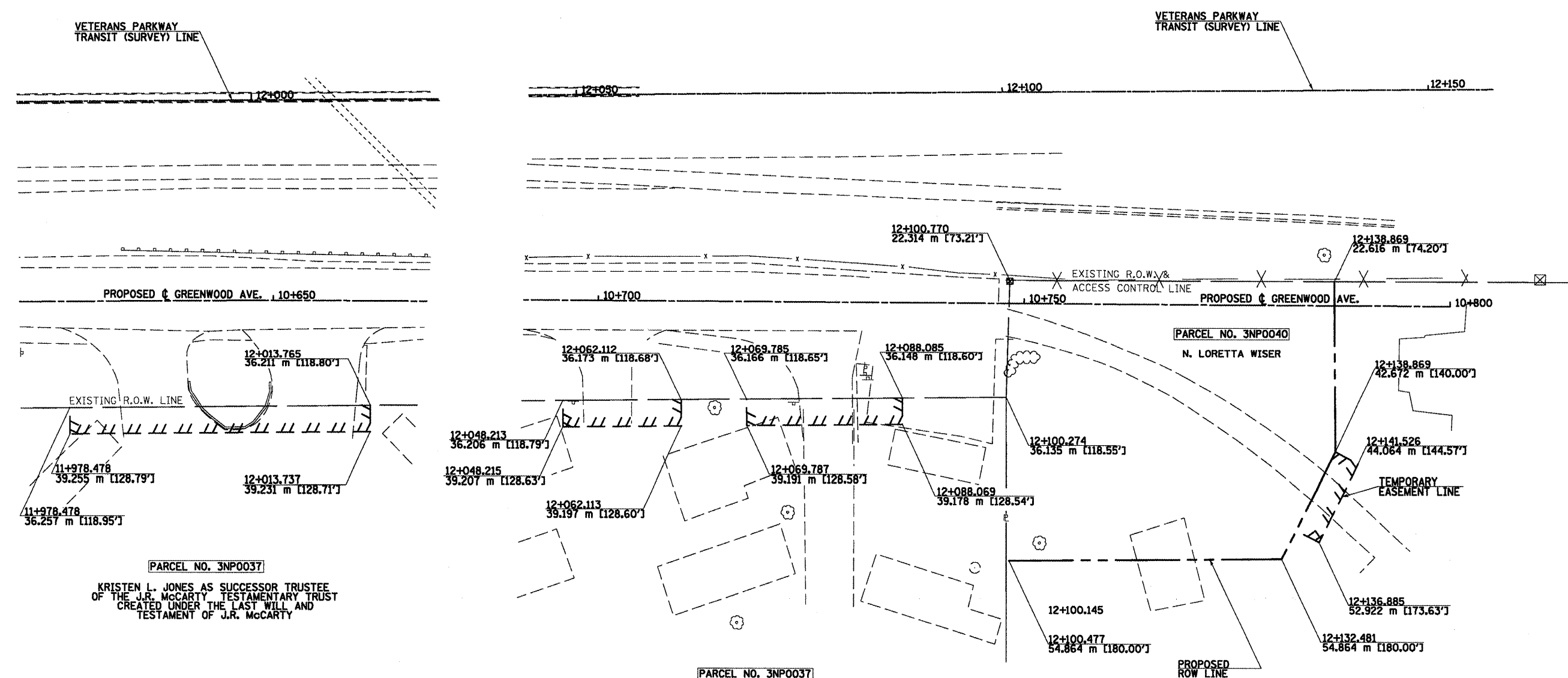
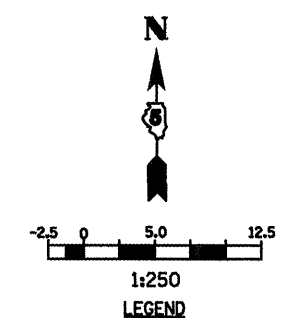
| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---------------------------------------|
| NAME | DATE | |
| | | RIGHT OF WAY |
| | | |
| | | |
| | | |
| | | |
| | | |

SHEET 3 OF 4
 DATE 05-27-09

DRAWN BY PMH
 CHECKED BY GJS

RIGHT OF WAY REVISIONS

| | | | | |
|---------------------|-------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (DN & TS-1) | McLEAN | 497 | 214 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



PARCEL NO. 3NP0037
 KRISTEN L. JONES AS SUCCESSOR TRUSTEE
 OF THE J.R. McCARTY TESTAMENTARY TRUST
 CREATED UNDER THE LAST WILL AND
 TESTAMENT OF J.R. McCARTY

PARCEL NO. 3NP0037
 KRISTEN L. JONES AS SUCCESSOR TRUSTEE
 OF THE J.R. McCARTY TESTAMENTARY TRUST
 CREATED UNDER THE LAST WILL AND
 TESTAMENT OF J.R. McCARTY

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

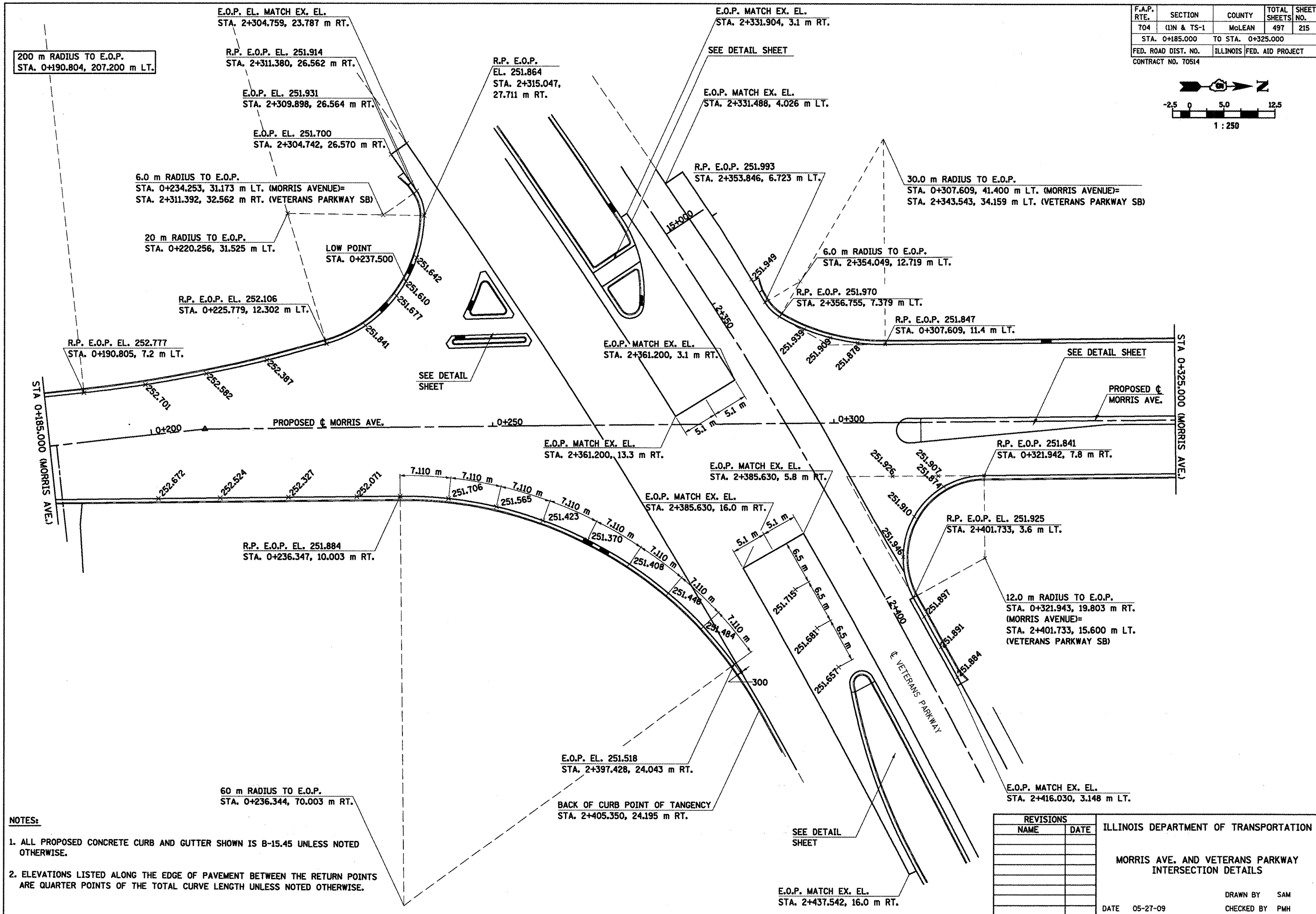
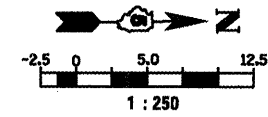
ILLINOIS DEPARTMENT OF TRANSPORTATION

 RIGHT OF WAY

 SHEET 4 OF 4
 DATE 05-27-09
 DRAWN BY PMH
 CHECKED BY GJS

RIGHT OF WAY REVISIONS

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|-------------|---------------------------|--------------|-----------|
| 704 | (DN & TS-1) | McLEAN | 497 | 215 |
| STA. 0+185.000 | | TO STA. 0+325.000 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



- NOTES:**
1. ALL PROPOSED CONCRETE CURB AND GUTTER SHOWN IS B-15.45 UNLESS NOTED OTHERWISE.
 2. ELEVATIONS LISTED ALONG THE EDGE OF PAVEMENT BETWEEN THE RETURN POINTS ARE QUARTER POINTS OF THE TOTAL CURVE LENGTH UNLESS NOTED OTHERWISE.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

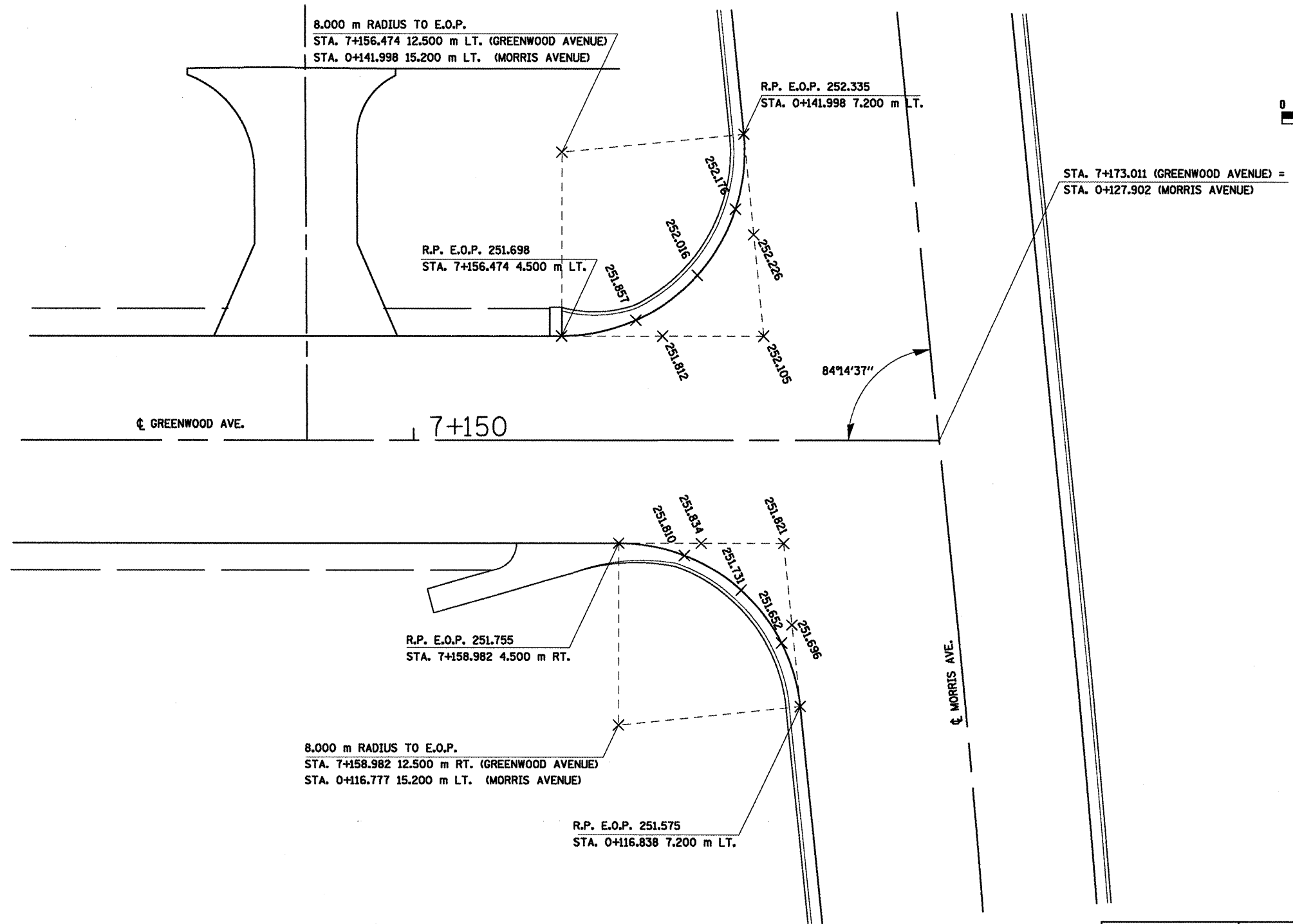
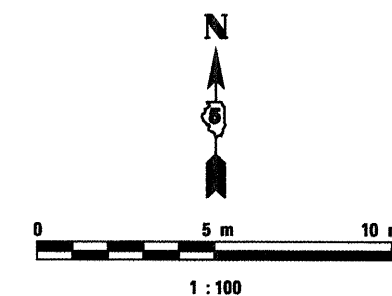
ILLINOIS DEPARTMENT OF TRANSPORTATION

MORRIS AVE. AND VETERANS PARKWAY INTERSECTION DETAILS

DRAWN BY SAM
CHECKED BY PMH

DATE 05-27-09

| | | | | |
|---------------------|------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (DN & TS-1 | McLEAN | 497 | 216 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |

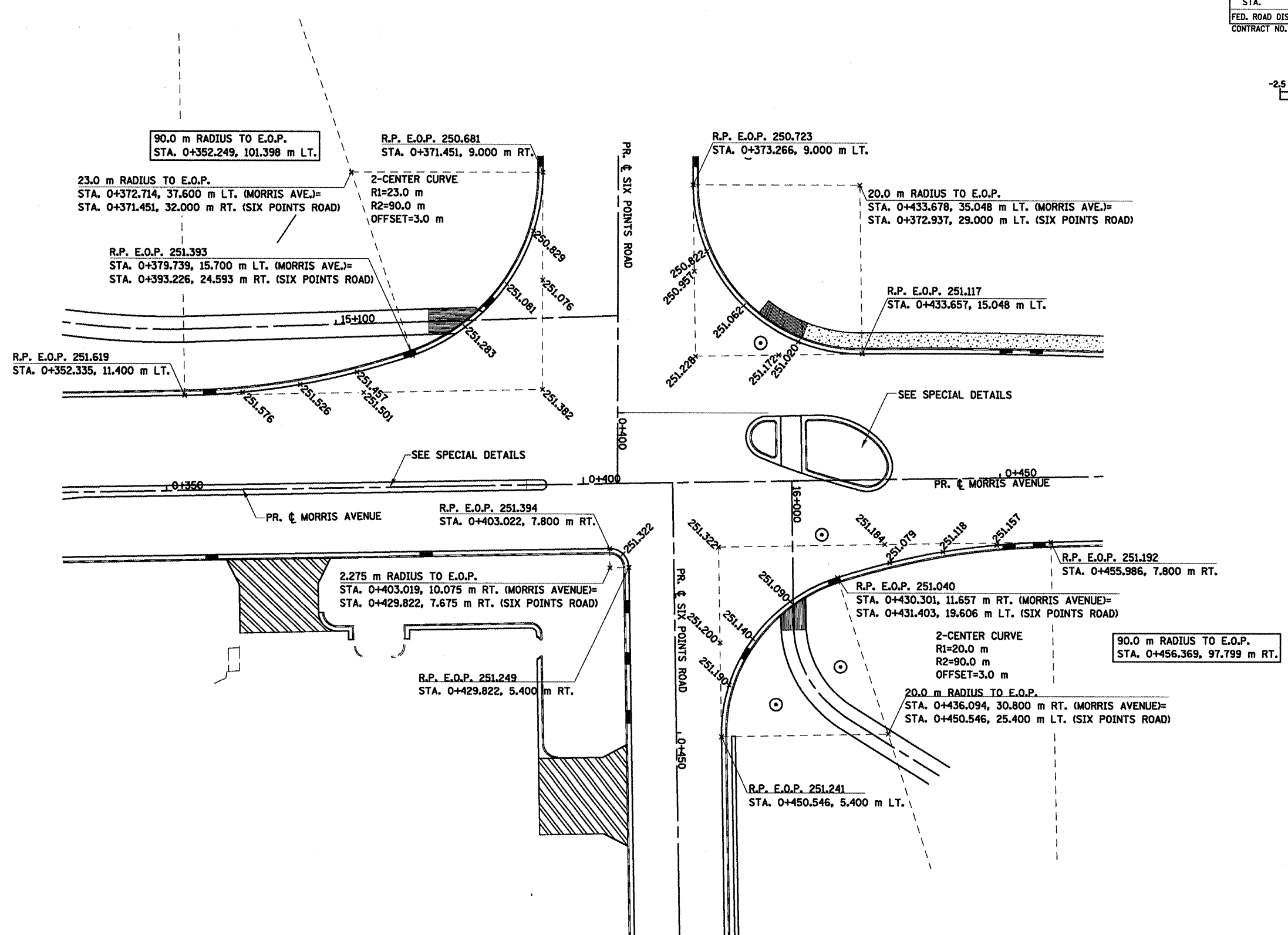
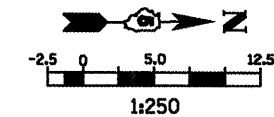


NOTES:

1. ALL PROPOSED CONCRETE CURB AND GUTTER SHOWN IS 8-15.45 UNLESS NOTED OTHERWISE.
2. ELEVATIONS LISTED ALONG THE EDGE OF PAVEMENT BETWEEN THE RETURN POINTS ARE QUARTER POINTS OF THE TOTAL CURVE LENGTH.

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|----------|---|
| NAME | DATE | |
| | | MORRIS AVENUE AND GREENWOOD AVENUE INTERSECTION DETAILS |
| | | |
| | | |
| | | |
| | | |
| | | |
| DATE | 05-27-09 | DRAWN BY BER CHECKED BY PMH |

| | | | | |
|---------------------|-------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (LN & TS-1) | McLEAN | 497 | 217 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |

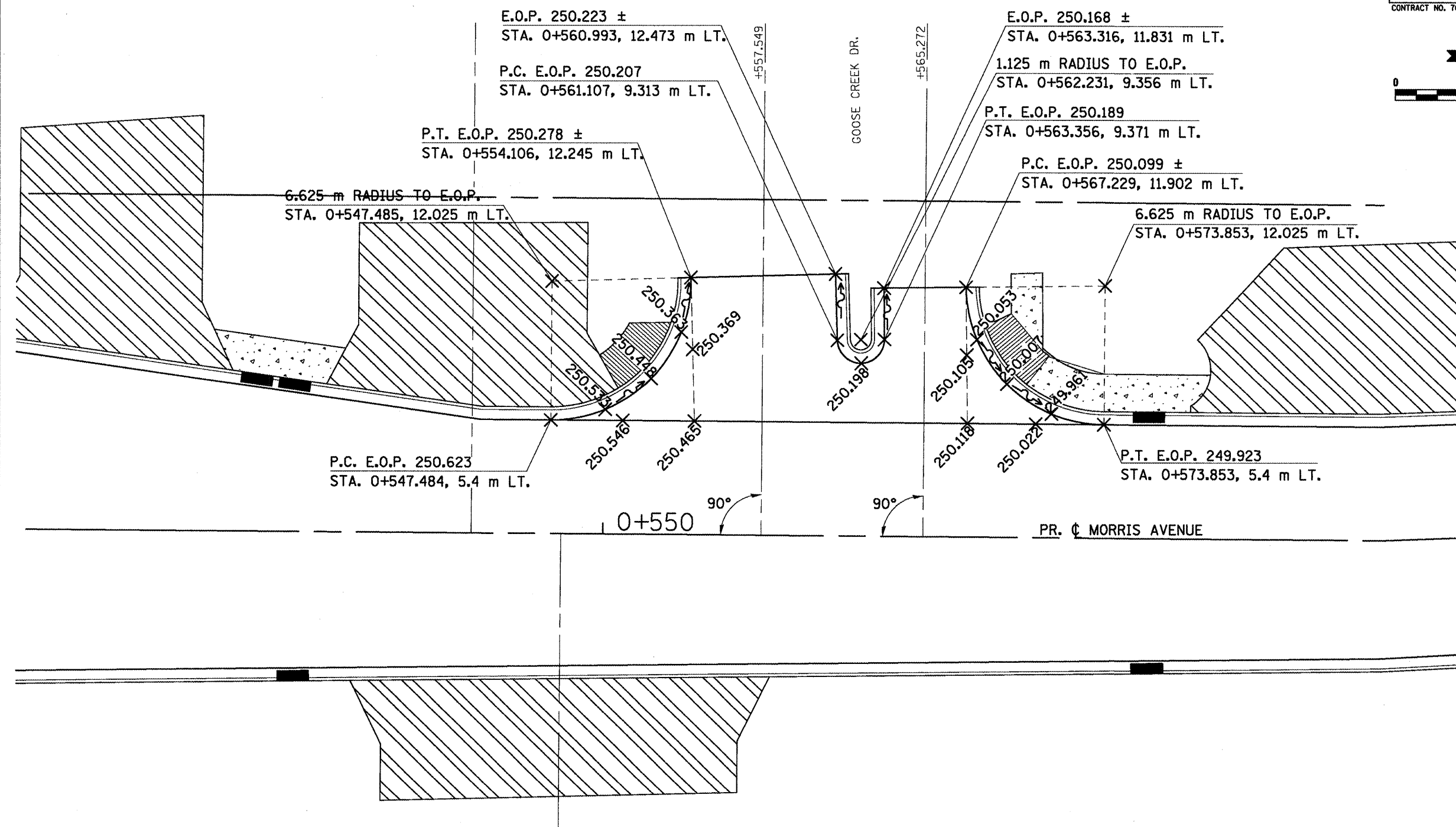
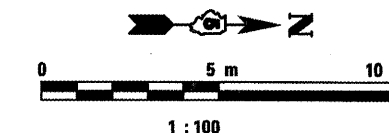


- NOTES:**
1. ALL PROPOSED CONCRETE CURB AND GUTTER SHOWN IS B-15.45 UNLESS NOTED OTHERWISE.
 2. ELEVATIONS LISTED ALONG THE EDGE OF PAVEMENT BETWEEN THE RETURN POINTS ARE QUARTER POINTS OF THE TOTAL RADIUS LENGTH.

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---|
| NAME | DATE | |
| | | MORRIS AVE. AND SIX POINTS RD. INTERSECTION DETAILS DRAWN BY SAM CHECKED BY PMH DATE 05-27-09 |
| | | |
| | | |
| | | |
| | | |

MORRIS AVE. AND SIX POINTS RD. INTERSECTION DETAILS

| | | | | |
|---------------------|-------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1)N & TS-1 | McLEAN | 497 | 218 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



NOTES:

1. ALL PROPOSED CONCRETE CURB AND GUTTER SHOWN IS B-15.45 UNLESS NOTED OTHERWISE.
2. ELEVATIONS LISTED ALONG THE EDGE OF PAVEMENT BETWEEN THE RETURN POINTS ARE QUARTER POINTS OF THE TOTAL RADIUS LENGTH.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

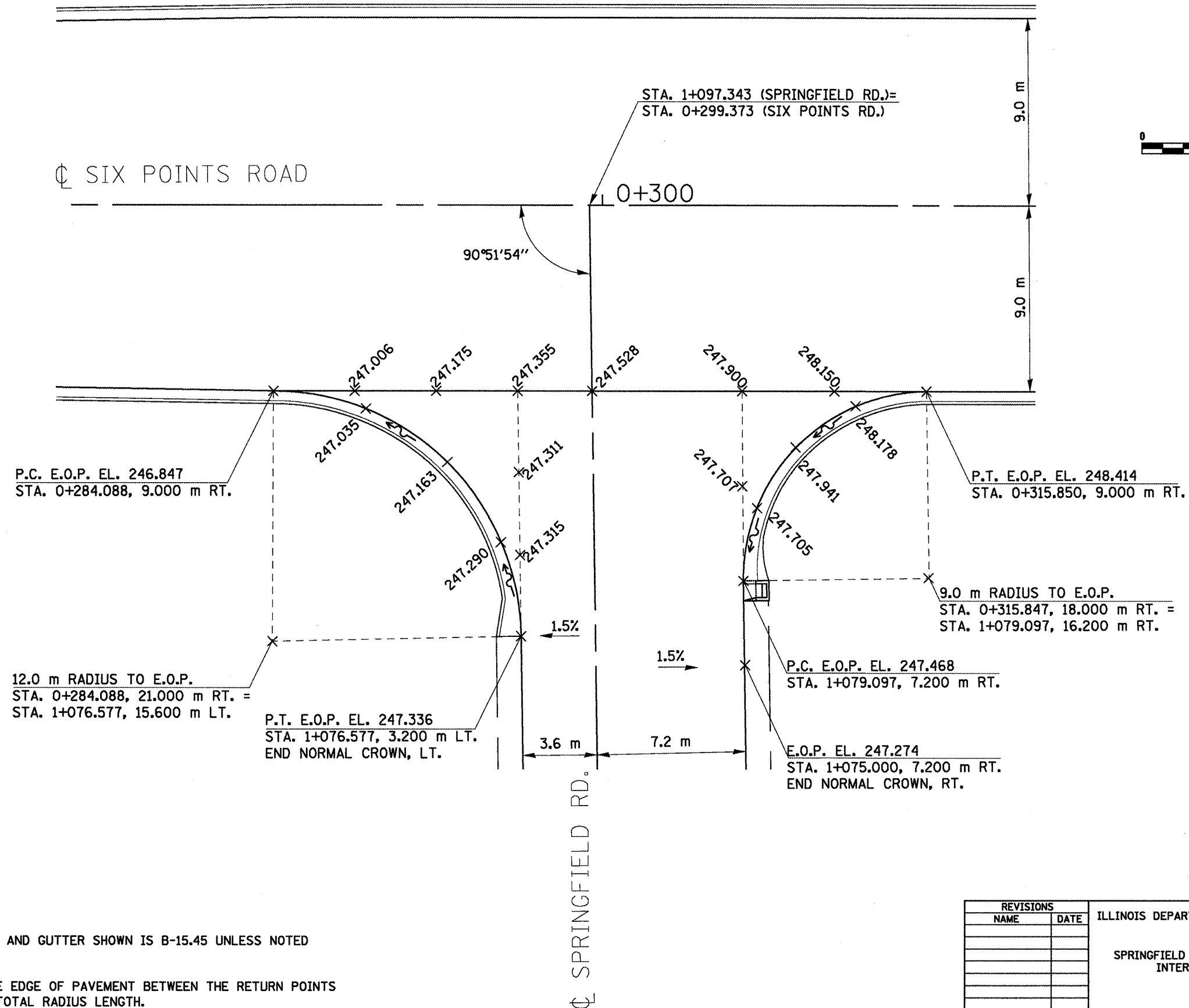
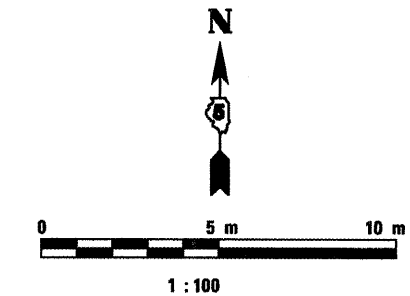
ILLINOIS DEPARTMENT OF TRANSPORTATION

GOOSE CREEK DR. AND MORRIS AVE.
INTERSECTION DETAILS

DRAWN BY SAM
CHECKED BY PMH
DATE 05-27-09

GOOSE CREEK DR. AND MORRIS AVE. INTERSECTION DETAILS

| | | | | |
|---------------------|------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (LN & TS-1 | McLEAN | 497 | 219 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



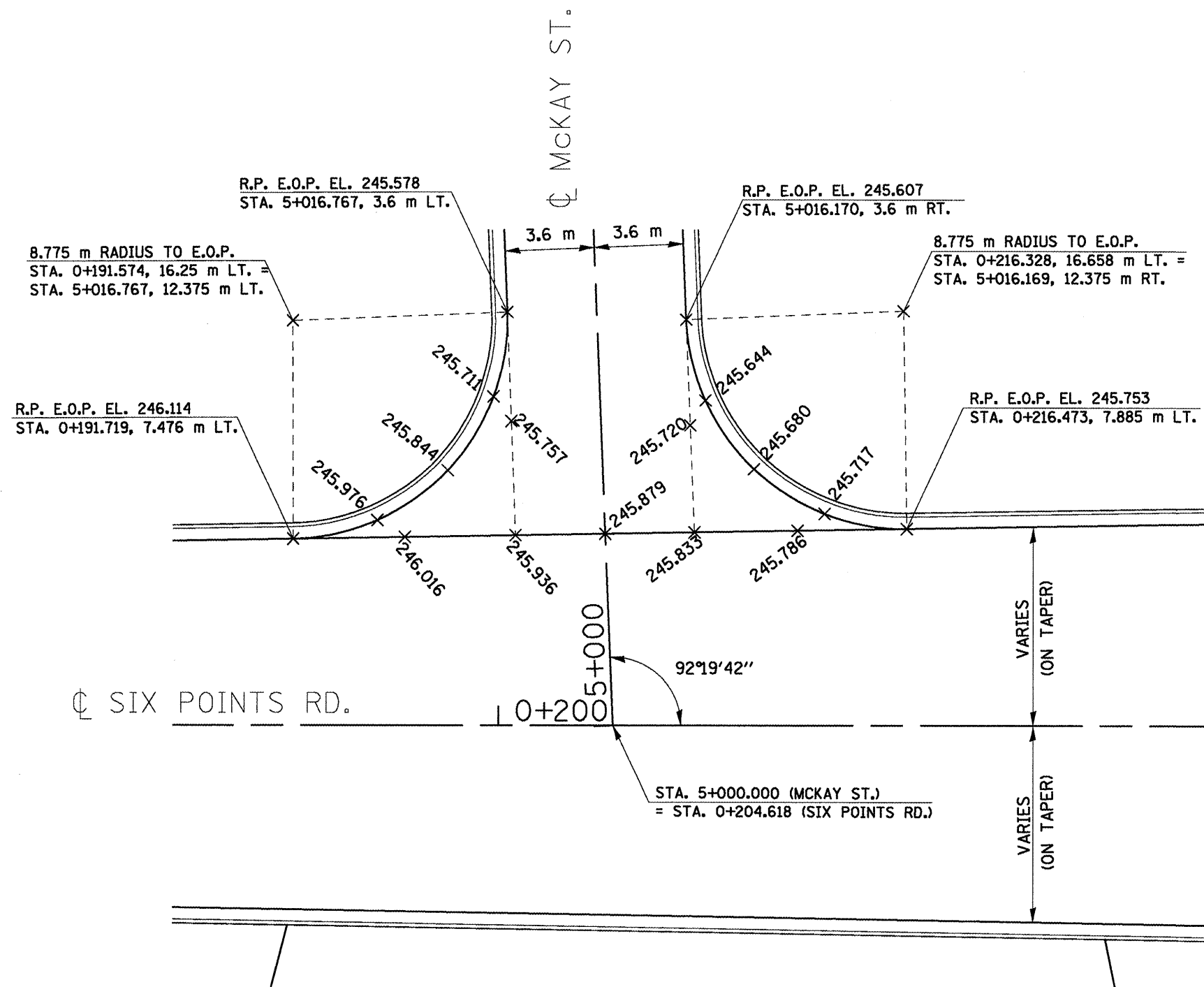
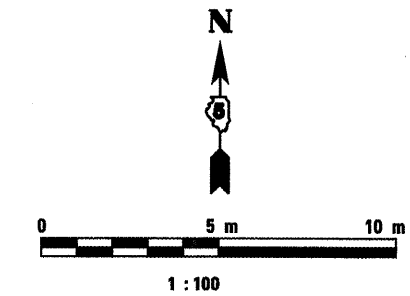
NOTES:

1. ALL PROPOSED CONCRETE CURB AND GUTTER SHOWN IS B-15.45 UNLESS NOTED OTHERWISE.
2. ELEVATIONS LISTED ALONG THE EDGE OF PAVEMENT BETWEEN THE RETURN POINTS ARE QUARTER POINTS OF THE TOTAL RADIUS LENGTH.

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|---------------|------|---|
| NAME | DATE | |
| | | SPRINGFIELD RD. AND SIX POINTS RD INTERSECTION DETAILS |
| | | |
| | | |
| | | |
| | | |
| DATE 05-27-09 | | DRAWN BY SAM CHECKED BY PMH |

SPRINGFIELD RD. AND SIX POINTS RD. INTERSECTION DETAILS

| | | | | |
|---------------------|------------|---------------------------|--------------|-----------|
| F.A.P. RT.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (IN & TS-1 | MOLEAN | 497 | 220 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



NOTES:

1. ALL PROPOSED CONCRETE CURB AND GUTTER SHOWN IS B-15.45 UNLESS NOTED OTHERWISE.
2. ELEVATIONS LISTED ALONG THE EDGE OF PAVEMENT BETWEEN THE RETURN POINTS ARE QUARTER POINTS OF THE TOTAL CURVE LENGTH.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

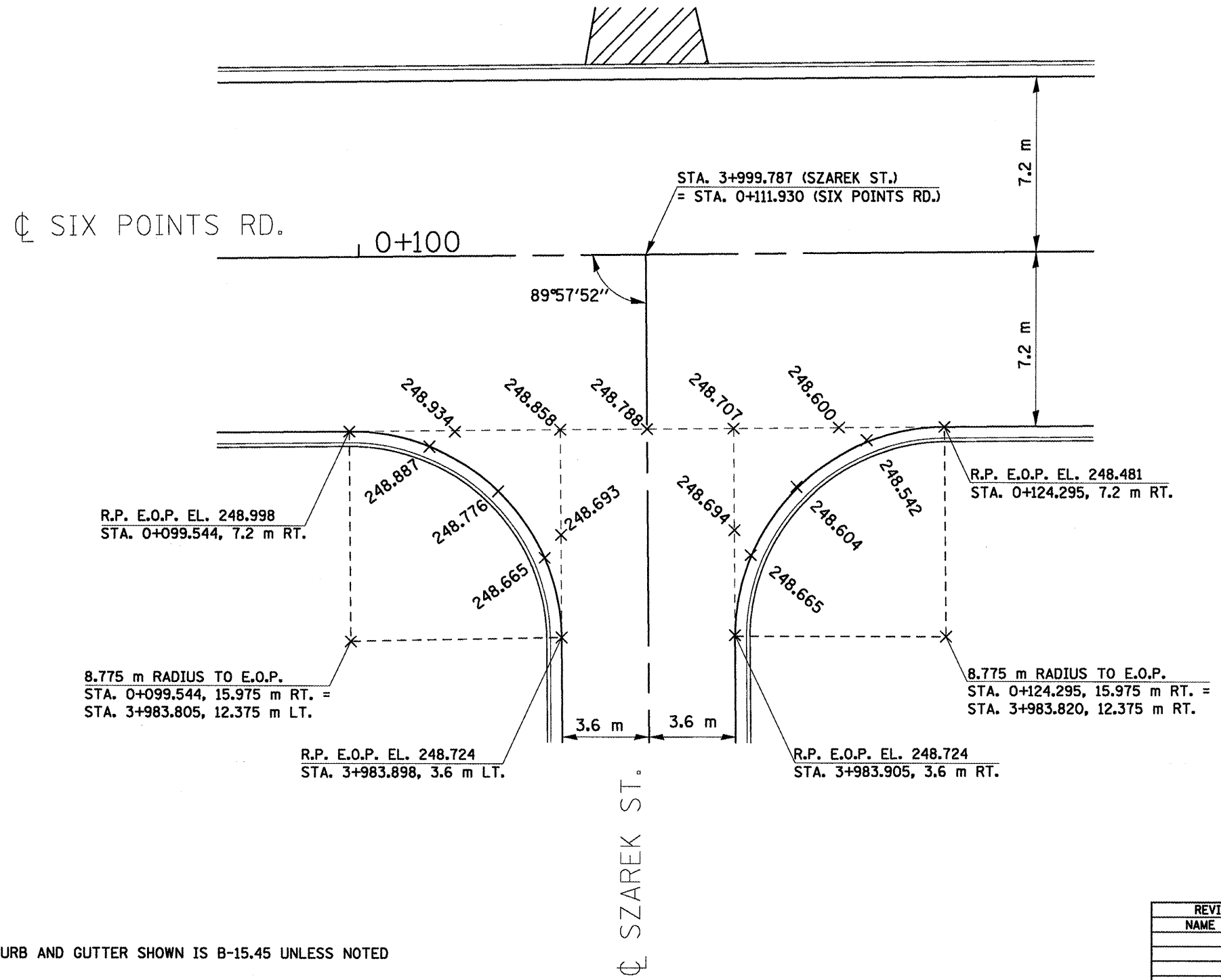
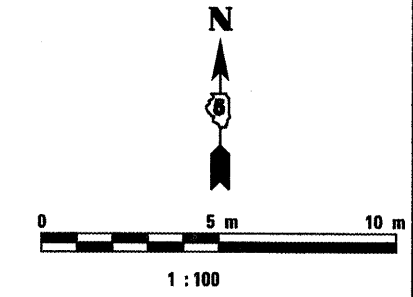
MCKAY ST. AND SIX POINTS RD.
INTERSECTION DETAILS

DRAWN BY SAM
CHECKED BY PMH

DATE 05-27-09

MCKAY ST. AND SIX POINTS RD. INTERSECTION DETAILS

| | | | | |
|---------------------|-------------|---------------------------|--------------|-----------|
| F.A.P. RT.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (IN & TS-1) | McLEAN | 70514 | 221 |
| STA. N/A | | TO STA. N/A | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



NOTES:

1. ALL PROPOSED CONCRETE CURB AND GUTTER SHOWN IS B-15.45 UNLESS NOTED OTHERWISE.
2. ELEVATIONS LISTED ALONG THE EDGE OF PAVEMENT BETWEEN THE RETURN POINTS ARE QUARTER POINTS OF THE TOTAL CURVE LENGTH.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

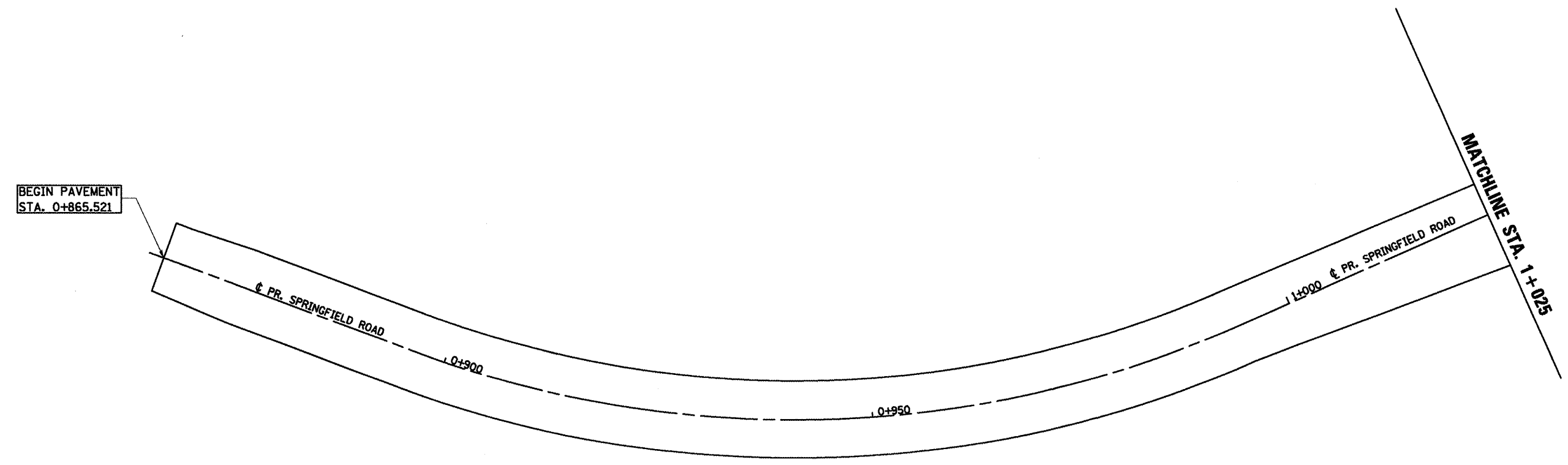
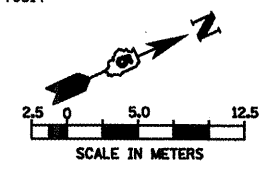
SZAREK ST. AND SIX POINTS RD.
INTERSECTION DETAILS

DRAWN BY SAM
CHECKED BY PMH

DATE 05-27-09

SZAREK ST. AND SIX POINTS RD. - INTERSECTION DETAILS

| | | | | |
|---------------------|-------------|---------------------------|-------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1)N & TS-1 | McLEAN | 497 | 223 |
| STA. 0+865.521 | | | TO STA. 1+025.000 | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |

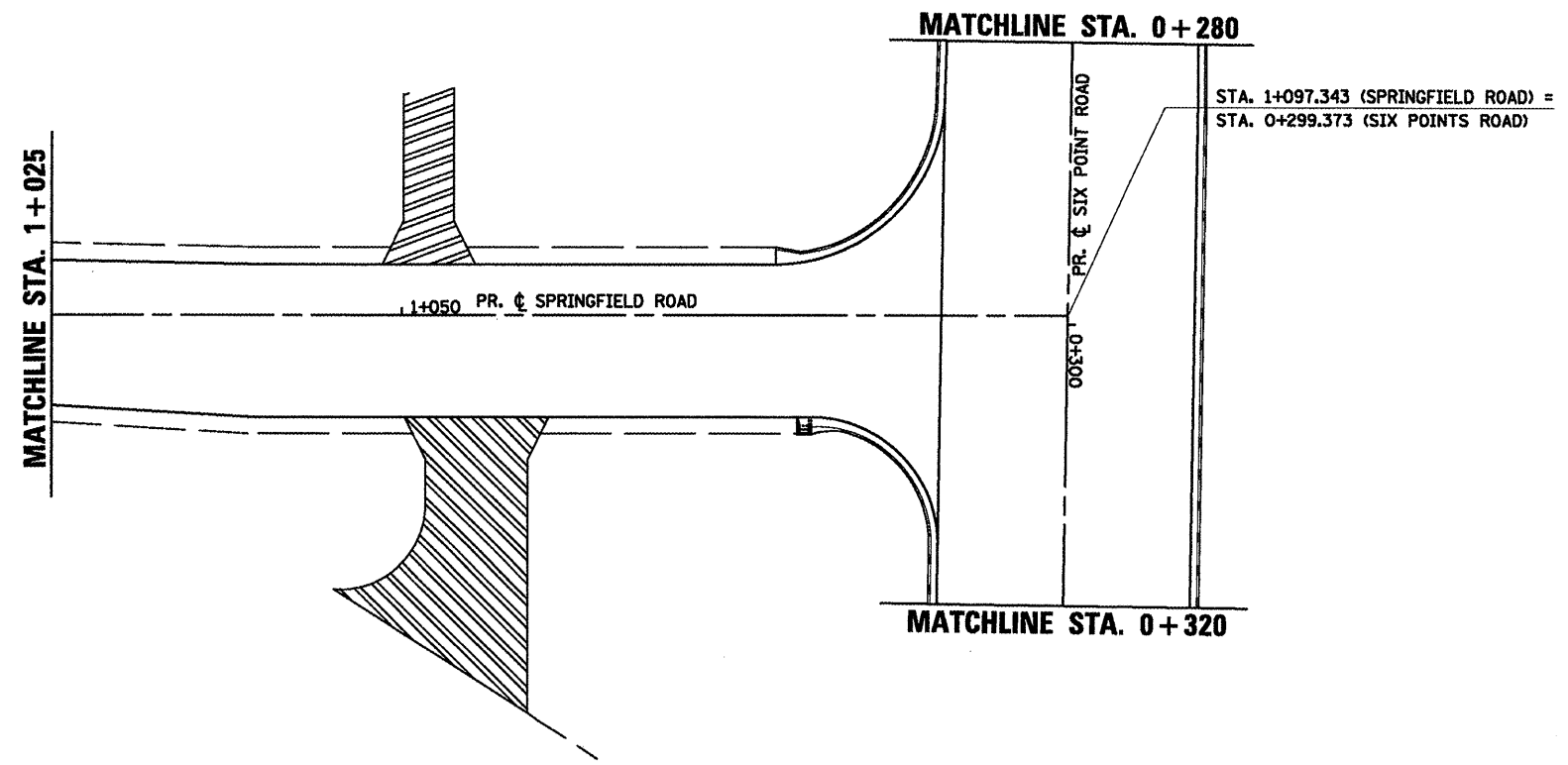
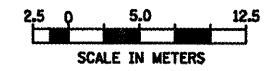


| STATION | LEFT | | | CENTERLINE ELEVATION | RIGHT | | |
|-----------|-------------|-------------|----------------|----------------------|-------------|--------------|-----------------|
| | LEFT RATE | LEFT OFFSET | LEFT ELEVATION | | RIGHT RATE | RIGHT OFFSET | RIGHT ELEVATION |
| | % | m | m | m | % | m | m |
| 0+865.521 | MATCH EXIST | 4.228 | MATCH EXIST | 253.702 | MATCH EXIST | 4.027 | MATCH EXIST |
| 0+870.000 | -1.50 | 4.305 | 253.468 | 253.533 | -1.39 | 4.252 | 253.473 |
| 0+880.000 | -1.50 | 4.499 | 253.087 | 253.154 | 0.11 | 4.499 | 253.159 |
| 0+890.000 | -1.72 | 4.500 | 252.699 | 252.776 | 1.72 | 4.500 | 252.853 |
| 0+900.000 | -3.32 | 4.500 | 252.248 | 252.398 | 3.32 | 4.500 | 252.547 |
| 0+910.000 | -4.00 | 4.500 | 251.839 | 252.019 | 4.00 | 4.500 | 252.199 |
| 0+920.000 | -4.00 | 4.500 | 251.461 | 251.641 | 4.00 | 4.500 | 251.821 |
| 0+930.000 | -4.00 | 4.500 | 251.083 | 251.263 | 4.00 | 4.500 | 251.443 |
| 0+940.000 | -4.00 | 4.500 | 250.715 | 250.895 | 4.00 | 4.500 | 251.075 |
| 0+950.000 | -4.00 | 4.500 | 250.369 | 250.549 | 4.00 | 4.500 | 250.729 |
| 0+960.000 | -4.00 | 4.500 | 250.045 | 250.225 | 4.00 | 4.500 | 250.405 |
| 0+970.000 | -4.00 | 4.500 | 249.743 | 249.923 | 4.00 | 4.500 | 250.103 |
| 0+980.000 | -4.00 | 4.500 | 249.451 | 249.631 | 4.00 | 4.500 | 249.811 |
| 0+990.000 | -3.24 | 4.500 | 249.193 | 249.339 | 3.24 | 4.500 | 249.485 |
| 1+000.000 | -1.64 | 4.373 | 248.976 | 249.048 | 1.64 | 4.882 | 249.128 |
| 1+010.000 | -1.50 | 4.175 | 248.693 | 248.756 | 0.04 | 5.474 | 248.758 |
| 1+020.000 | -1.50 | 3.978 | 248.405 | 248.464 | -1.47 | 6.067 | 248.375 |
| 1+020.230 | -1.50 | 3.973 | 248.398 | 248.458 | -1.50 | 6.081 | 248.366 |

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|---------------|------|--|
| NAME | DATE | |
| | | PAVEMENT ELEVATIONS SPRINGFIELD ROAD (WEST) |
| | | |
| | | |
| | | |
| | | |
| | | |
| DATE 05-27-09 | | DRAWN BY SAM CHECKED BY PMH |

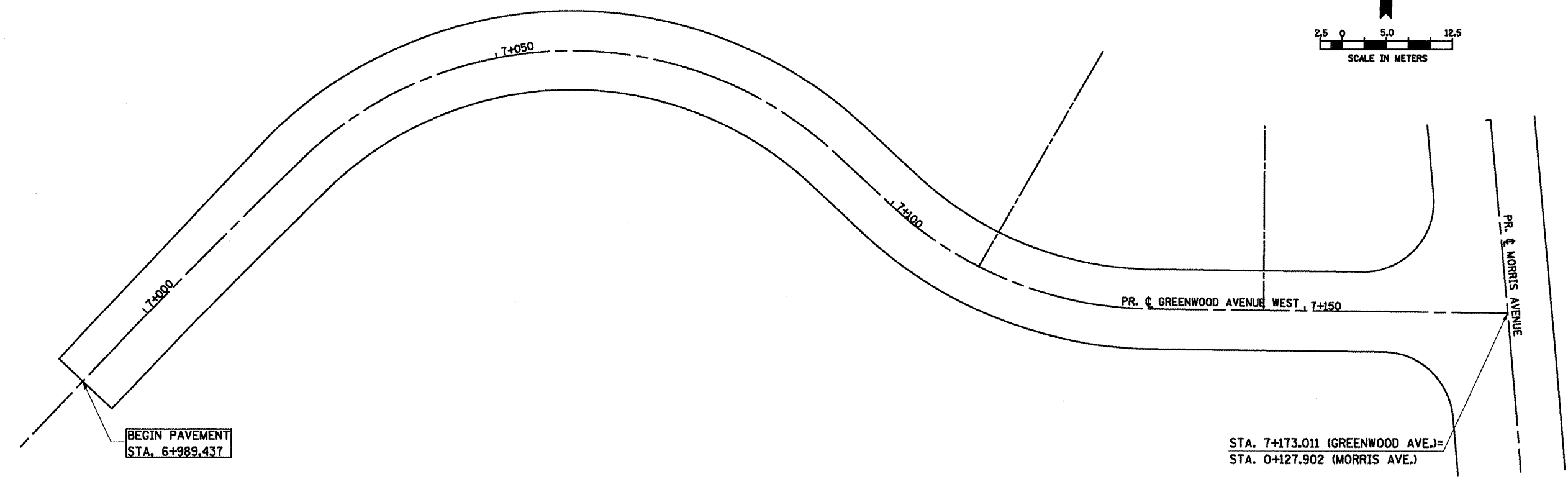
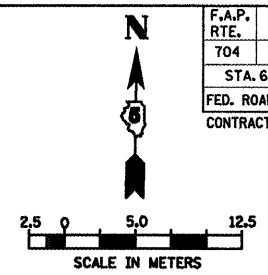
SPRINGFIELD ROAD, PAVEMENT ELEVATION, STA. 0 + 865.521 TO STA. 1 + 025

| | | | | |
|------------------------|-------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1)N & TS-1 | MCLEAN | 497 | 224 |
| STA. 1+025.000 TO STA. | | | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|----------|--|
| NAME | DATE | |
| | | PAVEMENT ELEVATIONS SPRINGFIELD ROAD (WEST) |
| | | |
| | | |
| | | |
| | | |
| | | |
| DATE | 03-31-09 | DRAWN BY SAM CHECKED BY PMH |

| | | | | |
|---------------------|-------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1)N & TS-1 | McLEAN | 497 | 225 |
| STA. 6+989.437 | | TO STA. 7+173.011 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



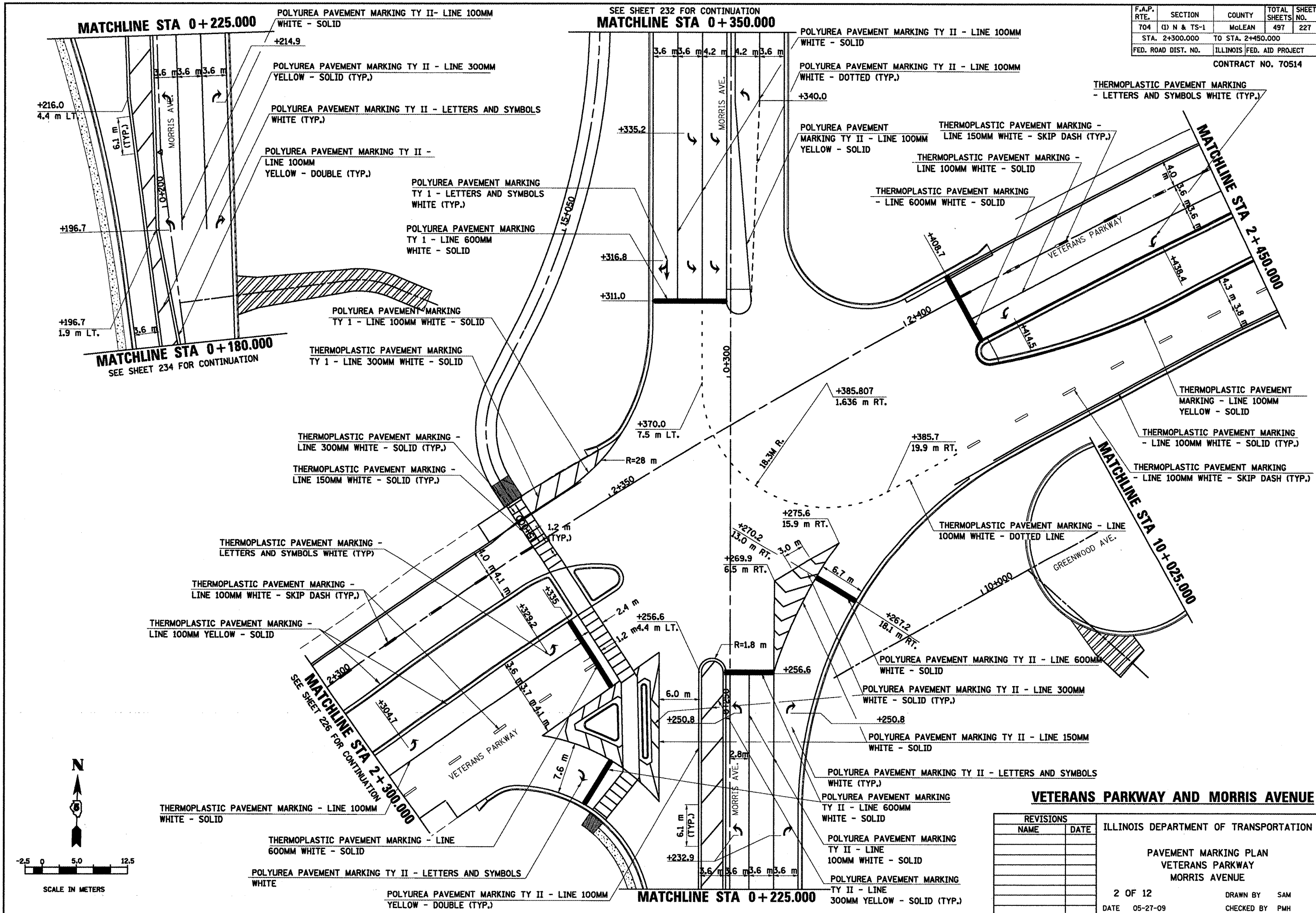
| STATIONS | LEFT | | | CENTERLINE ELEVATION m | RIGHT | | |
|-----------|-----------|-------------|----------------|------------------------------|------------|--------------|-----------------|
| | LEFT RATE | LEFT OFFSET | LEFT ELEVATION | | RIGHT RATE | RIGHT OFFSET | RIGHT ELEVATION |
| | % | m | m | | % | m | m |
| 6+989.437 | 2.00 | 3.660 | 254.011 | 254.938 | -2.00 | 4.733 | 254.843 |
| 6+990.000 | 2.00 | 3.673 | 254.986 | 254.913 | -2.00 | 4.720 | 254.819 |
| 7+000.000 | 2.00 | 3.913 | 254.547 | 254.469 | -2.00 | 4.500 | 254.379 |
| 7+010.000 | 2.00 | 4.153 | 254.109 | 254.026 | -2.00 | 4.500 | 253.936 |
| 7+020.000 | 2.25 | 4.393 | 253.658 | 253.559 | -2.25 | 4.500 | 253.458 |
| 7+030.000 | 3.00 | 4.500 | 253.207 | 253.072 | -3.00 | 4.500 | 252.937 |
| 7+040.000 | 3.00 | 4.500 | 252.703 | 252.568 | -3.00 | 4.500 | 252.433 |
| 7+050.000 | 3.00 | 4.500 | 252.196 | 252.061 | -3.00 | 4.500 | 251.926 |
| 7+060.000 | 3.00 | 4.500 | 251.697 | 251.562 | -3.00 | 4.500 | 251.427 |
| 7+070.000 | 3.00 | 4.500 | 251.290 | 251.155 | -3.00 | 4.500 | 251.020 |
| 7+080.000 | 3.00 | 4.500 | 250.993 | 250.858 | -3.00 | 4.500 | 250.723 |
| 7+090.000 | 2.16 | 4.500 | 250.769 | 250.672 | -2.16 | 4.500 | 250.575 |
| 7+100.000 | 0.16 | 4.500 | 250.603 | 250.596 | -1.50 | 4.500 | 250.528 |
| 7+110.000 | -1.50 | 4.500 | 250.564 | 250.632 | -1.50 | 4.500 | 250.564 |
| 7+120.000 | -1.50 | 4.500 | 250.710 | 250.777 | -1.50 | 4.500 | 250.710 |
| 7+130.000 | -1.50 | 4.500 | 250.965 | 251.033 | -1.50 | 4.500 | 250.965 |
| 7+140.000 | -1.50 | 4.500 | 251.265 | 251.333 | -1.50 | 4.500 | 251.265 |

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|---------------|------|--|
| NAME | DATE | |
| | | PAVEMENT ELEVATIONS GREENWOOD AVENUE (WEST) |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| DATE 05-27-09 | | DRAWN BY SAM CHECKED BY PMH |

GREENWOOD AVENUE, PAVEMENT ELEVATION, STA. 6 + 989.437 TO STA. 7 + 173.011

| | | | |
|---------------------|--------------|---------------------------|------------------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS NO. |
| 704 | (1) N & TS-1 | McLEAN | 497 227 |
| STA. 2+300.000 | | TO STA. 2+450.000 | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | |

CONTRACT NO. 70514



VETERANS PARKWAY AND MORRIS AVENUE

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |

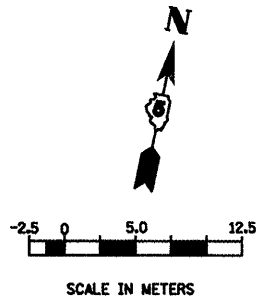
ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
VETERANS PARKWAY
MORRIS AVENUE

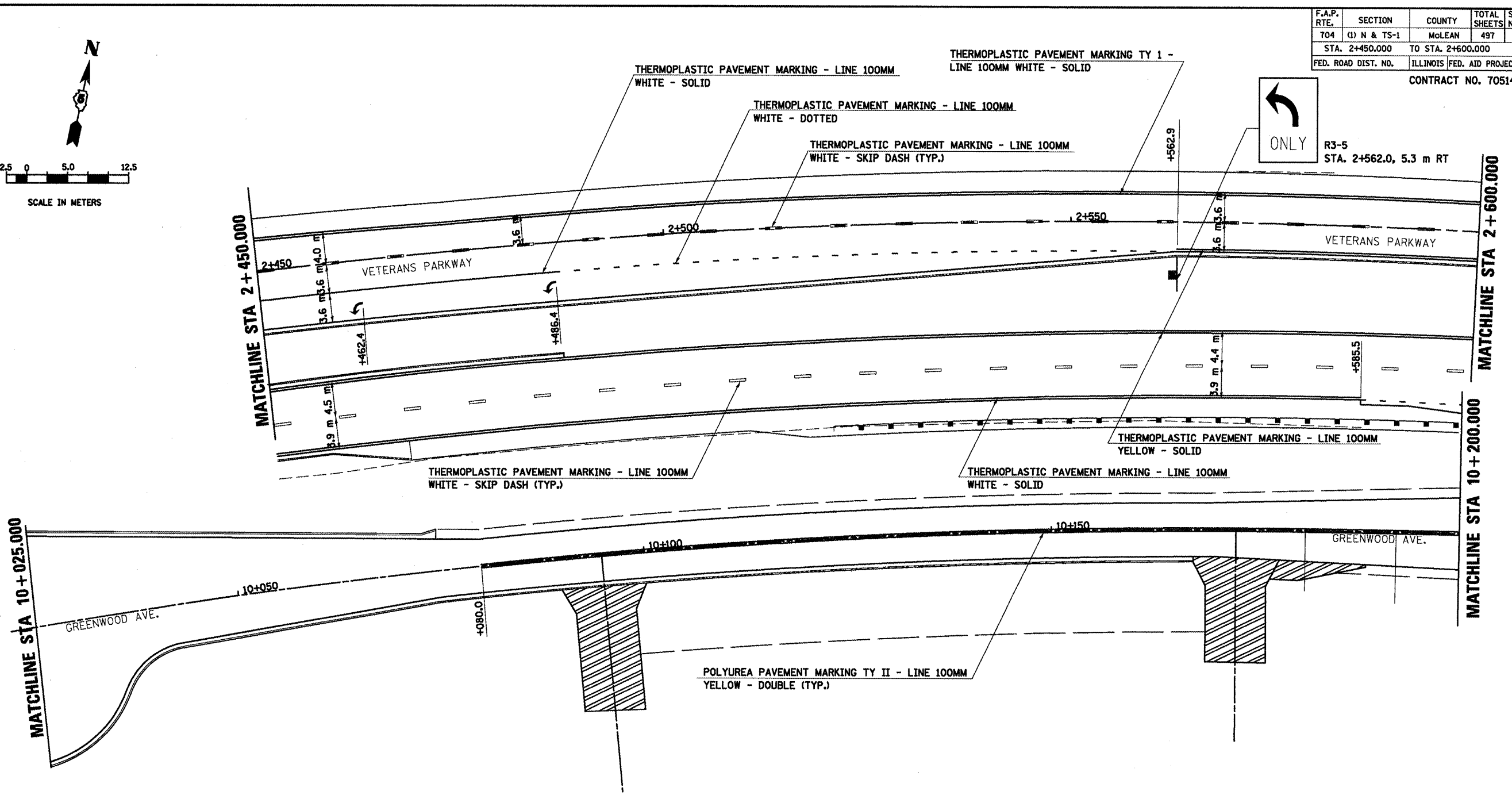
2 OF 12

DATE 05-27-09

DRAWN BY SAM
CHECKED BY PMH



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|----------------------------------|--------------|---------------------------|--------------|-----------|
| 704 | (1) N & TS-1 | McLEAN | 497 | 228 |
| STA. 2+450.000 TO STA. 2+600.000 | | | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |

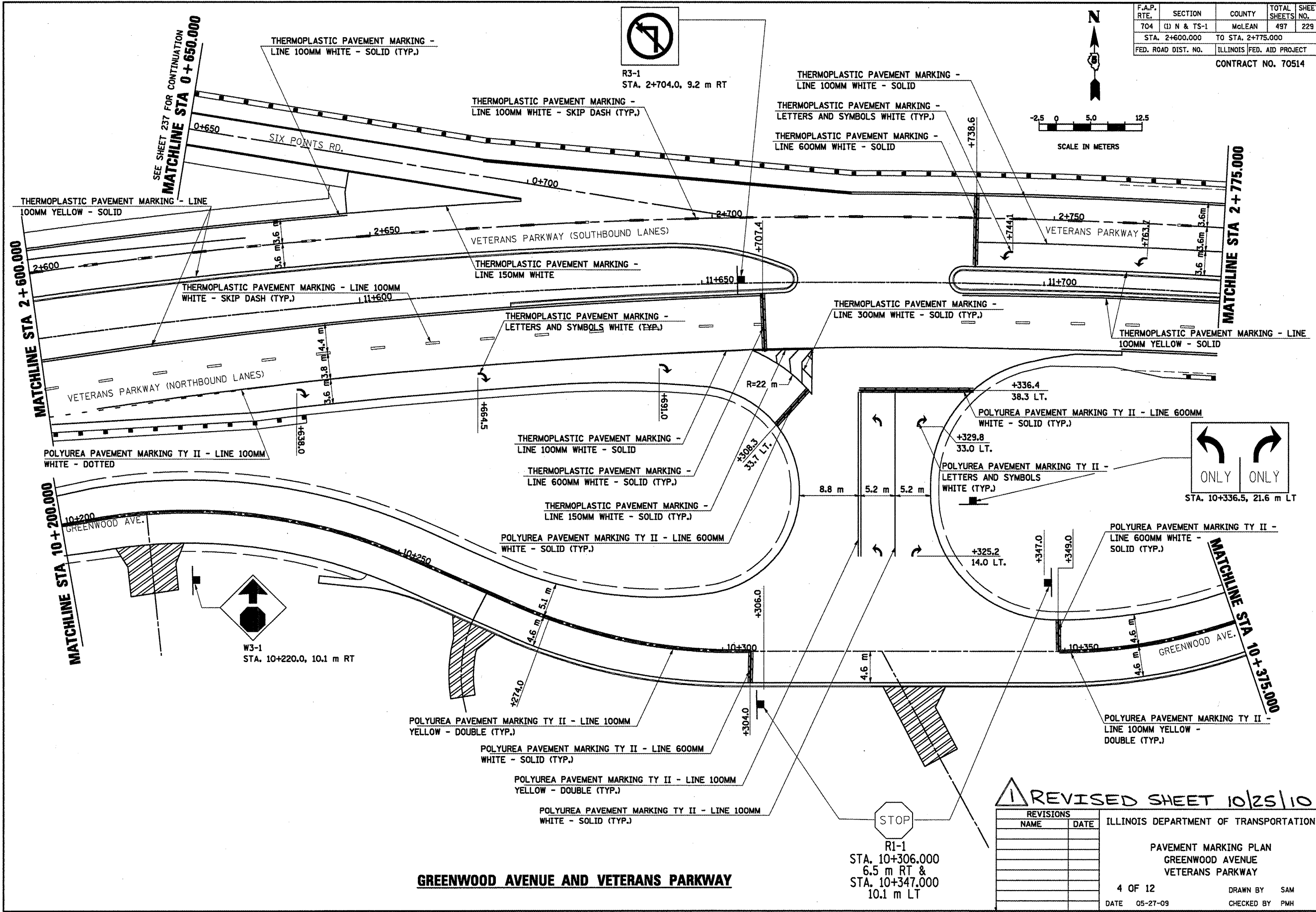
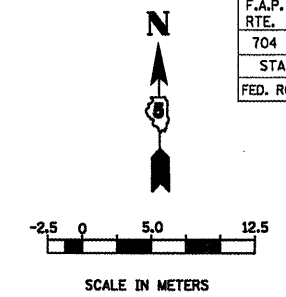


GREENWOOD AVENUE AND VETERANS PARKWAY

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|---------------|------|---|
| NAME | DATE | |
| | | PAVEMENT MARKING PLAN GREENWOOD AVENUE VETERANS PARKWAY |
| | | |
| | | |
| | | |
| | | |
| 3 OF 12 | | DRAWN BY SAM |
| DATE 03-31-09 | | CHECKED BY PMH |

| | | | | |
|---------------------|--------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (I) N & TS-1 | McLEAN | 497 | 229 |
| STA. 2+600.000 | | TO STA. 2+775.000 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

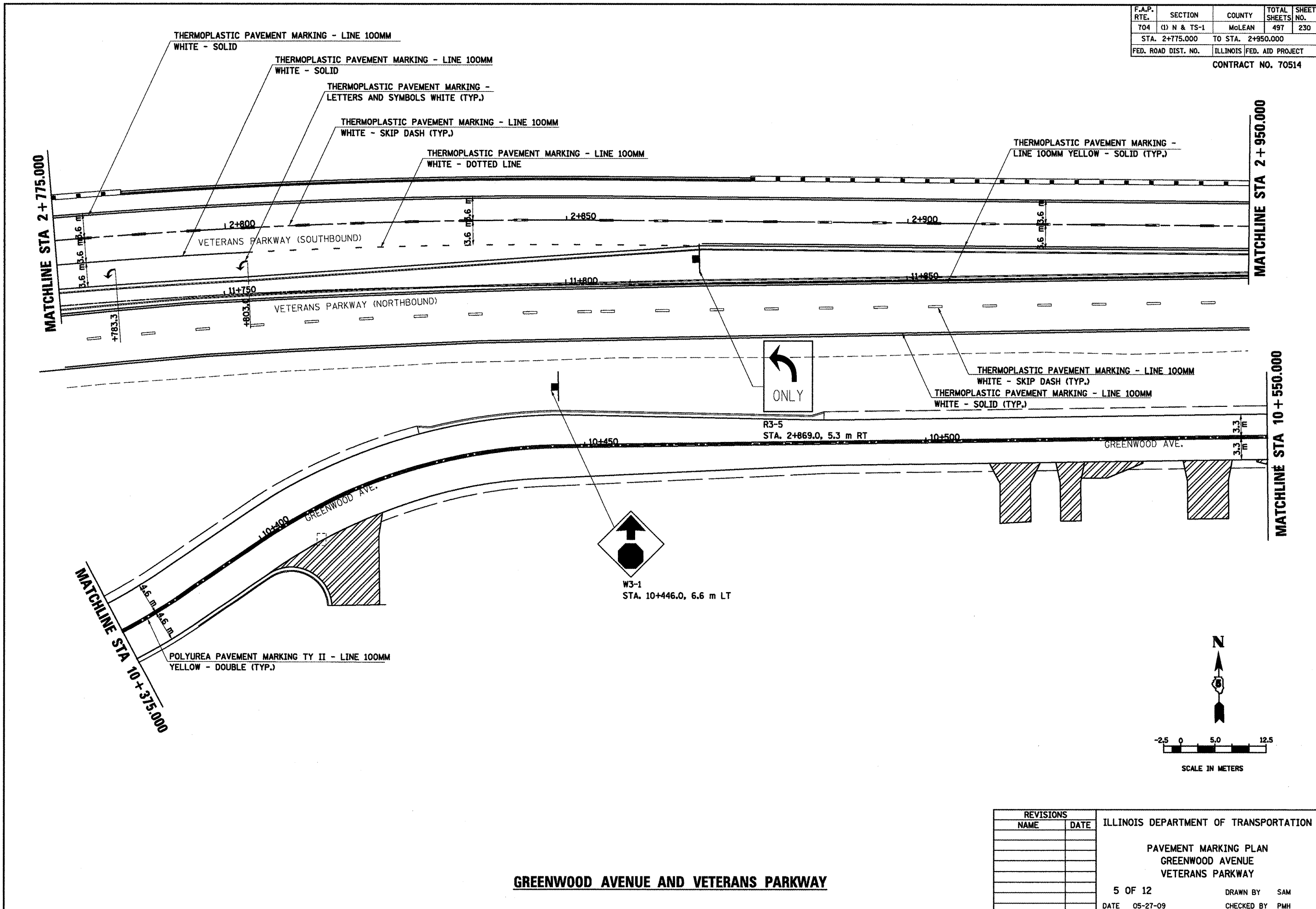
CONTRACT NO. 70514



GREENWOOD AVENUE AND VETERANS PARKWAY

PAVEMENT MARKING PLAN

| | | | | |
|---------------------|--------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| T04 | (1) N & TS-1 | McLEAN | 497 | 230 |
| STA. 2+775.000 | | TO STA. 2+950.000 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |

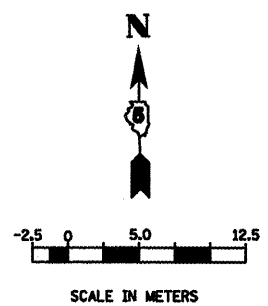
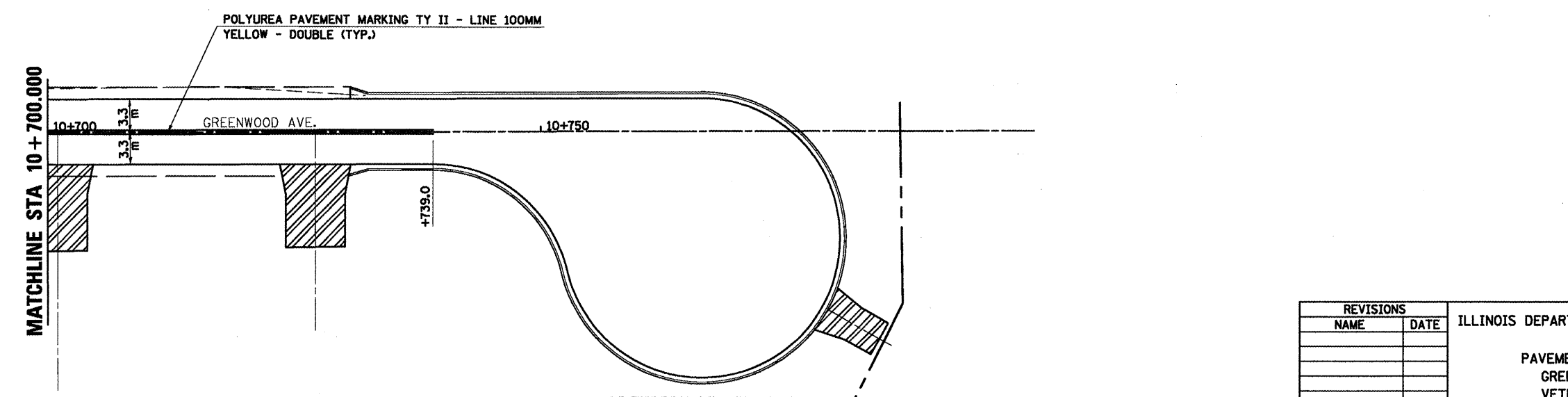
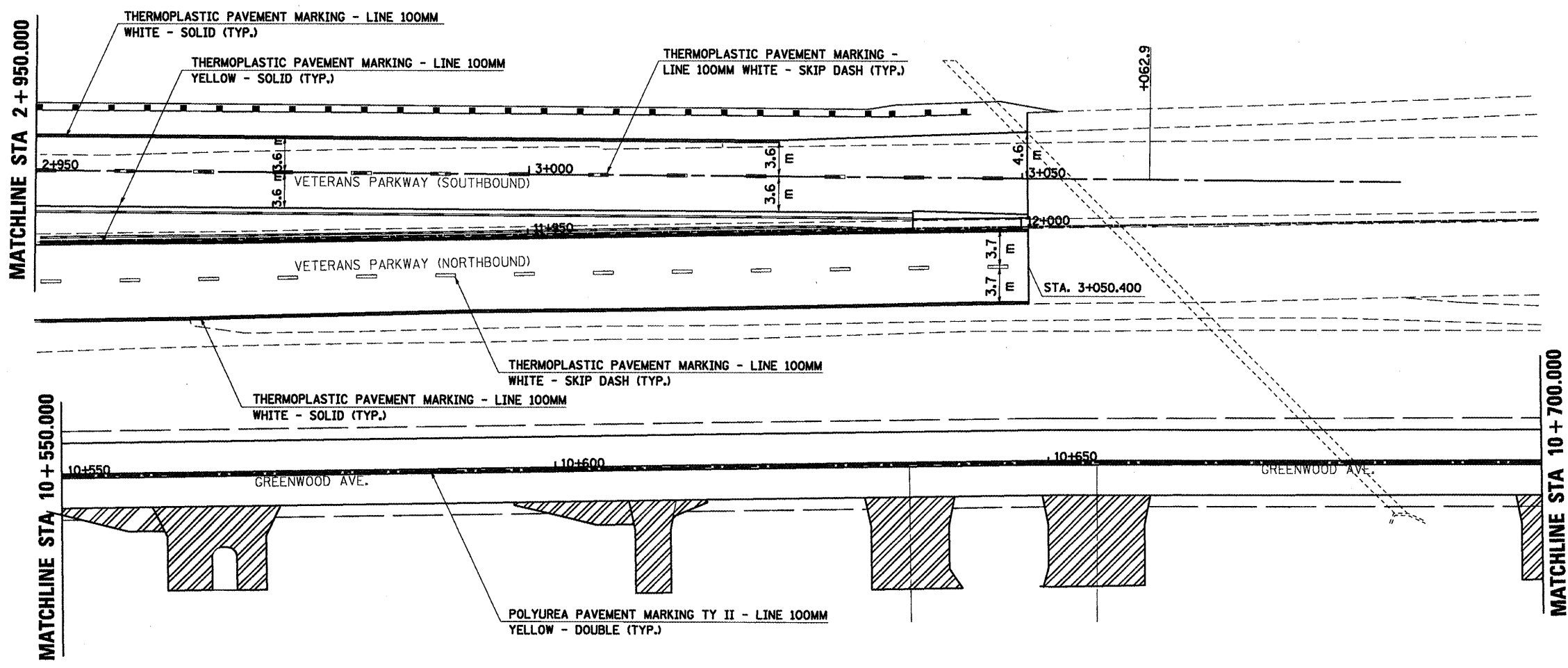
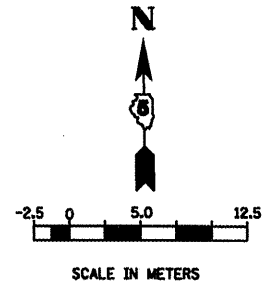


GREENWOOD AVENUE AND VETERANS PARKWAY

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|---------------|------|---|
| NAME | DATE | |
| | | PAVEMENT MARKING PLAN GREENWOOD AVENUE VETERANS PARKWAY |
| | | |
| | | |
| | | |
| | | |
| 5 OF 12 | | DRAWN BY SAM |
| DATE 05-27-09 | | CHECKED BY PMH |

| | | | | |
|---------------------|--------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1) N & TS-1 | McLEAN | 497 | 231 |
| STA. 2+950.000 | | TO STA. 3+050.400 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 70514



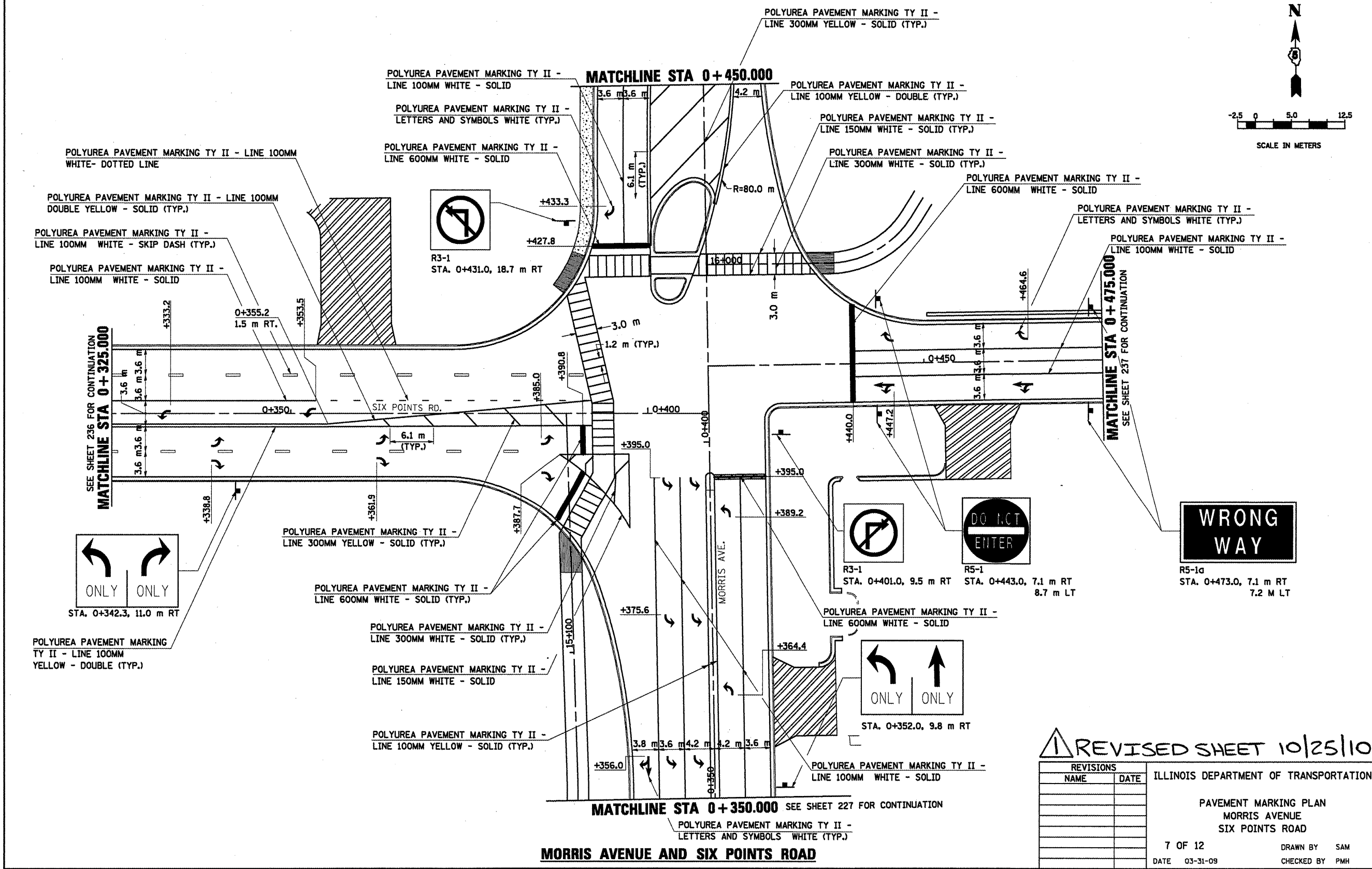
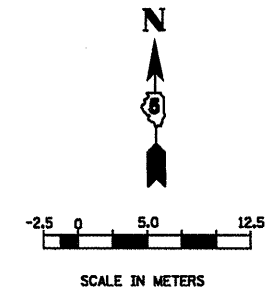
GREENWOOD AVENUE AND VETERANS PARKWAY

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
 PAVEMENT MARKING PLAN
 GREENWOOD AVENUE
 VETERANS PARKWAY
 6 OF 12
 DATE 05-27-09
 DRAWN BY SAM
 CHECKED BY PMH

| | | | | |
|---------------------|--------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (I) N & TS-1 | McLEAN | 497 | 232 |
| STA. 0+350.000 | | TO STA. 0+450.000 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 70514



REVISIED SHEET 10/25/10

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
MORRIS AVENUE
SIX POINTS ROAD

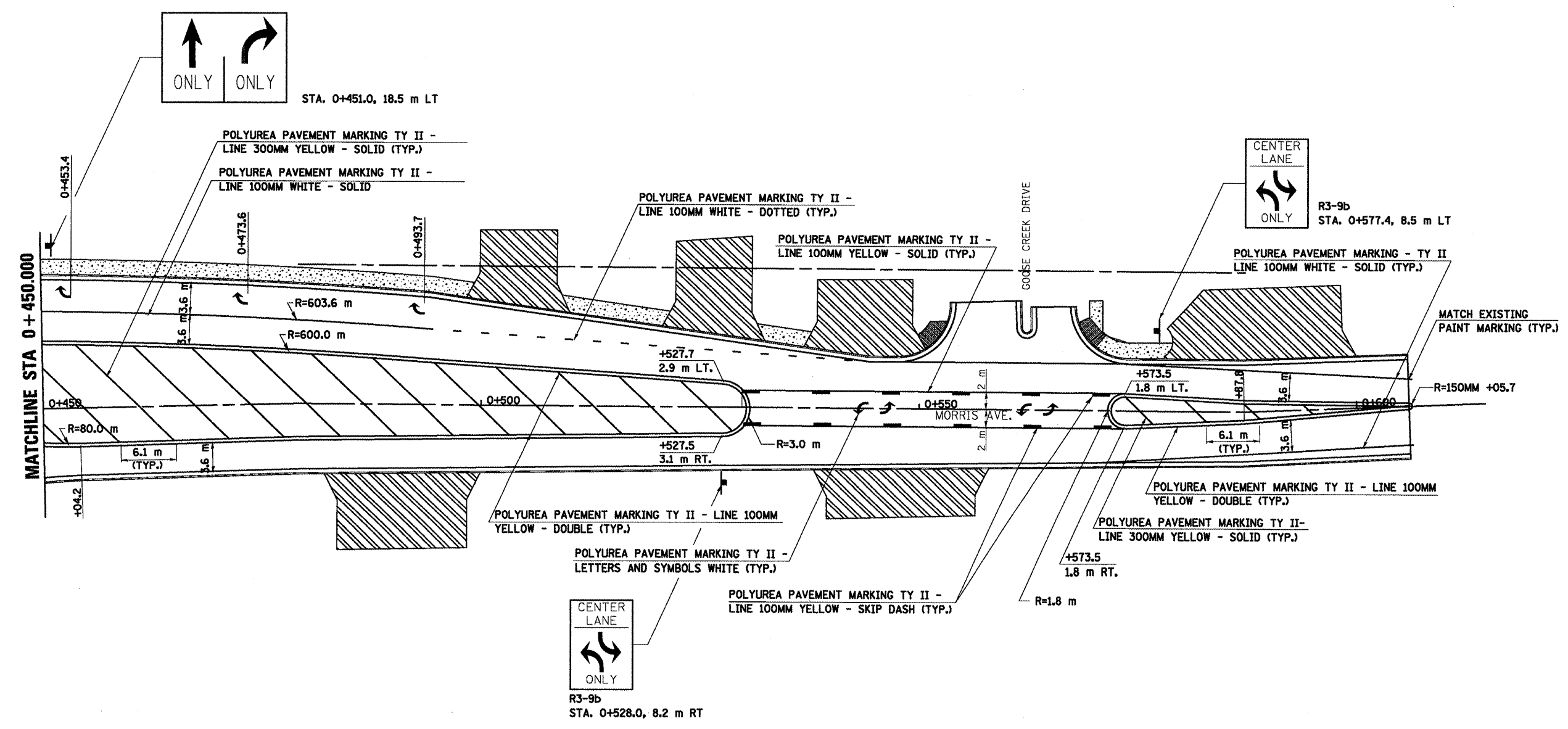
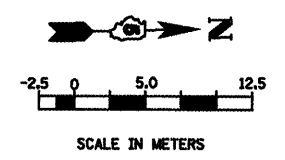
7 OF 12
DATE 03-31-09

DRAWN BY SAM
CHECKED BY PMH

MORRIS AVENUE AND SIX POINTS ROAD

| | | | | |
|---------------------|--------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1) N & TS-1 | MOLEAN | 497 | 233 |
| STA. 0+450.000 | | TO STA. 0+606.000 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 70514



MORRIS AVENUE

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
MORRIS AVENUE

8 OF 12

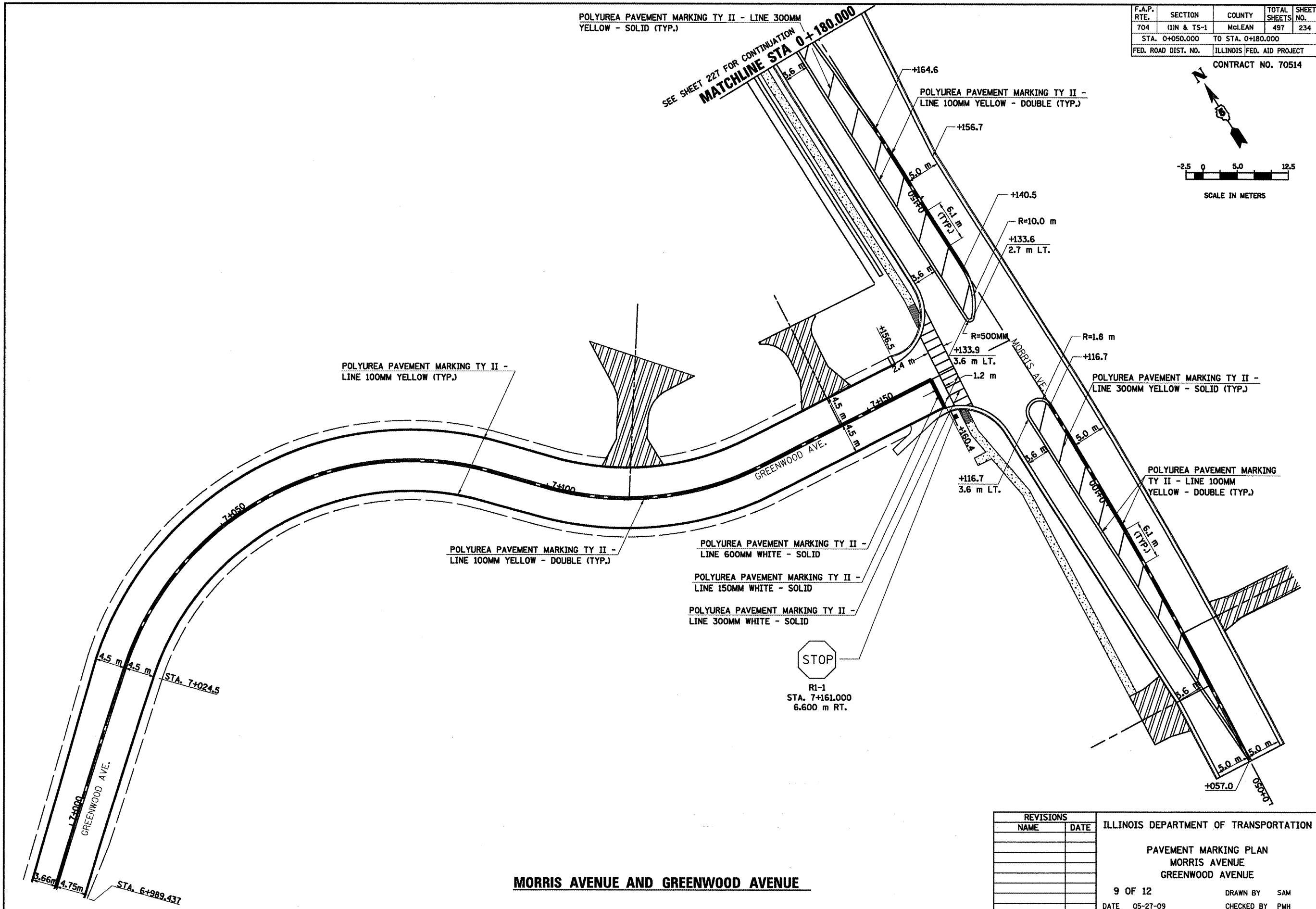
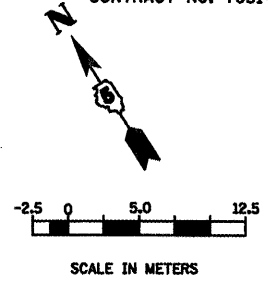
DATE 03-31-09

DRAWN BY SAM
CHECKED BY PMH

POLYUREA PAVEMENT MARKING TY II - LINE 300MM YELLOW - SOLID (TYP.)

| | | | | |
|---------------------|------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | 11N & TS-1 | McLEAN | 497 | 234 |
| STA. 0+050.000 | | TO STA. 0+180.000 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 70514



POLYUREA PAVEMENT MARKING TY II - LINE 100MM YELLOW (TYP.)

POLYUREA PAVEMENT MARKING TY II - LINE 100MM YELLOW - DOUBLE (TYP.)

POLYUREA PAVEMENT MARKING TY II - LINE 60MM WHITE - SOLID

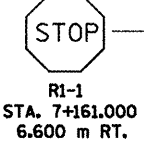
POLYUREA PAVEMENT MARKING TY II - LINE 150MM WHITE - SOLID

POLYUREA PAVEMENT MARKING TY II - LINE 300MM WHITE - SOLID

POLYUREA PAVEMENT MARKING TY II - LINE 100MM YELLOW - DOUBLE (TYP.)

POLYUREA PAVEMENT MARKING TY II - LINE 300MM YELLOW - SOLID (TYP.)

POLYUREA PAVEMENT MARKING TY II - LINE 100MM YELLOW - DOUBLE (TYP.)



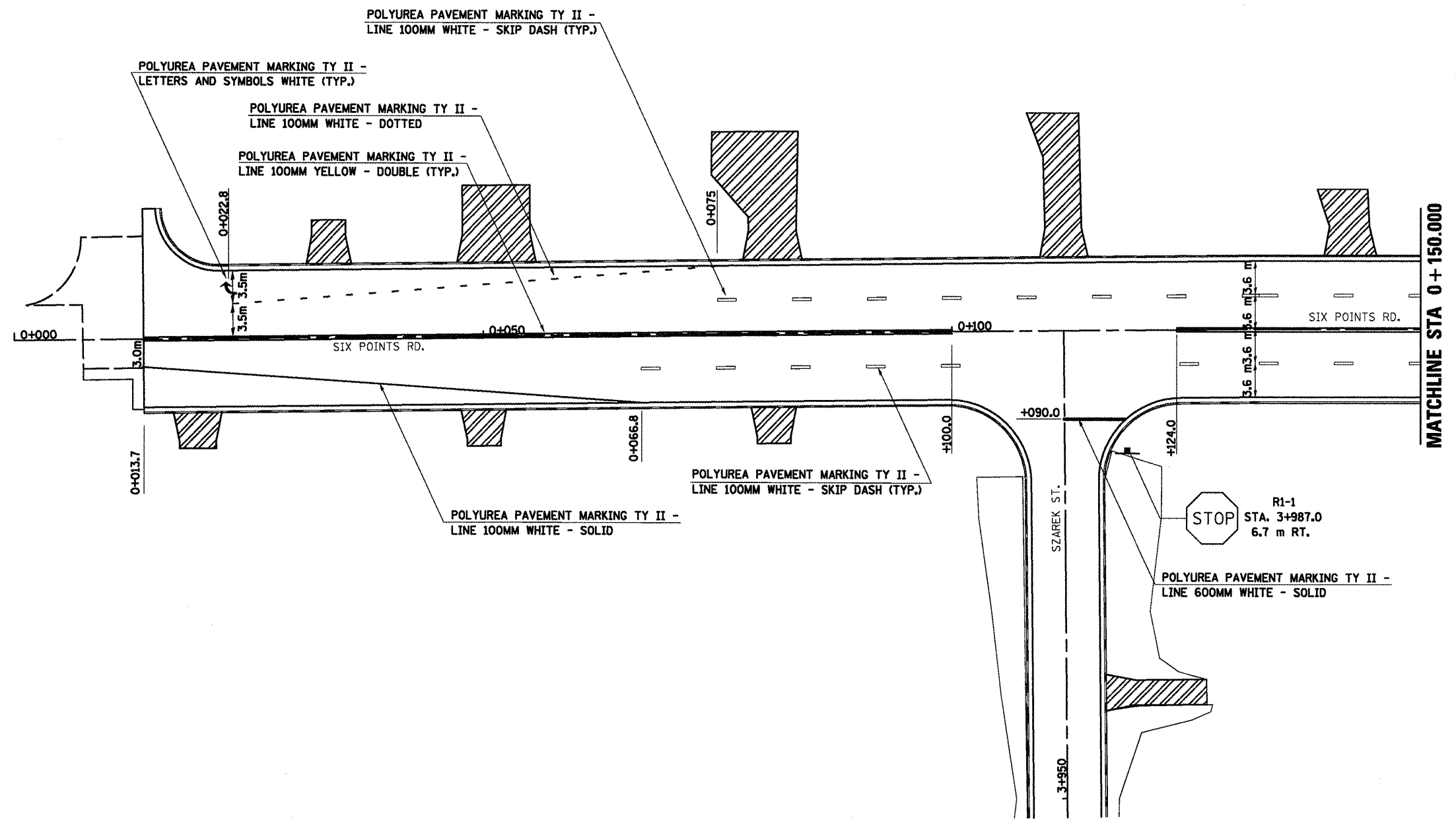
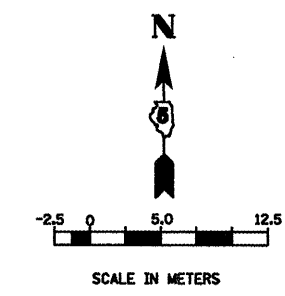
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
 PAVEMENT MARKING PLAN
 MORRIS AVENUE
 GREENWOOD AVENUE
 9 OF 12
 DATE 05-27-09
 DRAWN BY SAM
 CHECKED BY PMH

MORRIS AVENUE AND GREENWOOD AVENUE

| | | | | |
|---------------------|--------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| T04 | (1) N & TS-1 | McLEAN | 497 | 235 |
| STA. 0+000.000 | | TO STA. 0+150.000 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 70514



SIX POINTS ROAD

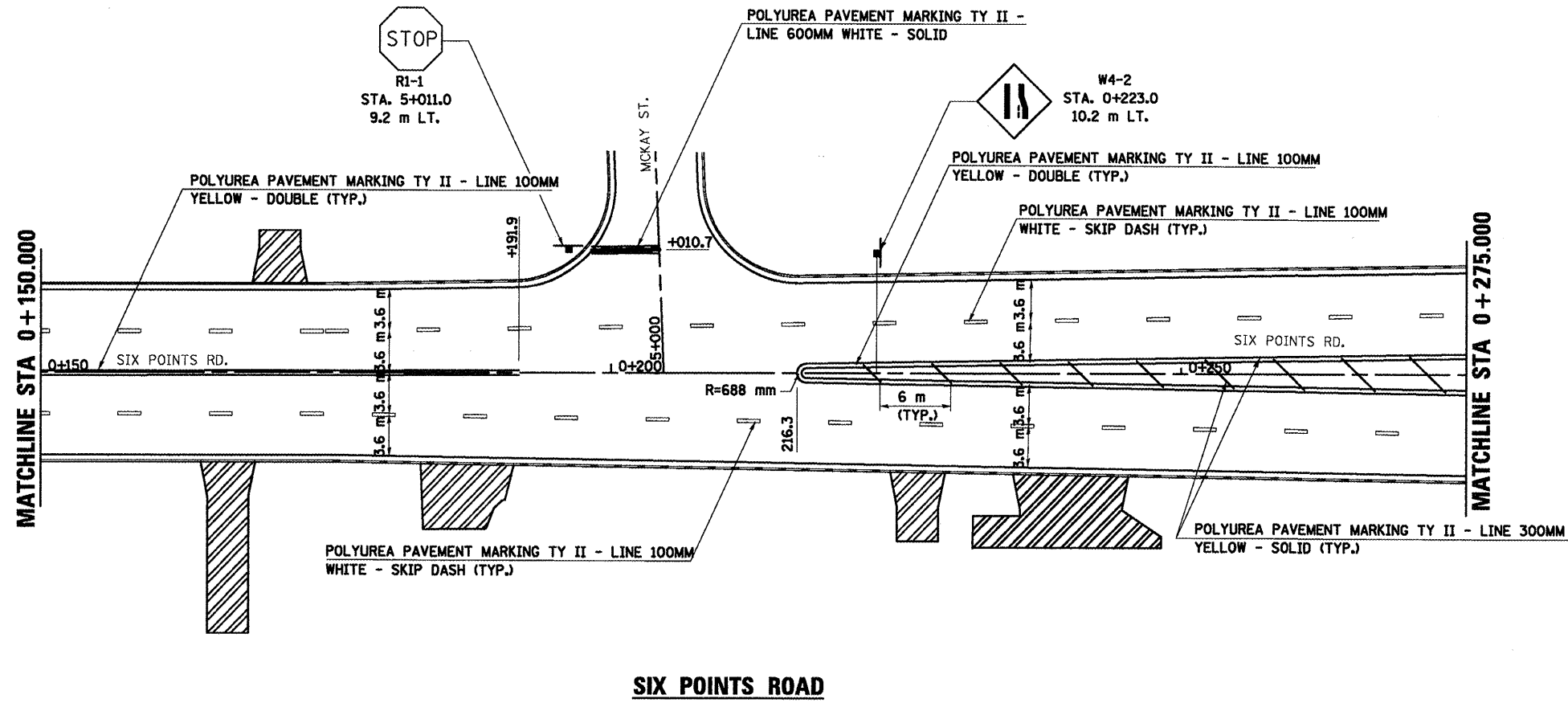
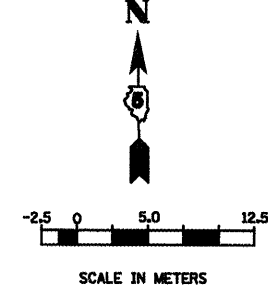
| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---------------------------------------|
| NAME | DATE | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |

**PAVEMENT MARKING PLAN
SIX POINTS ROAD**

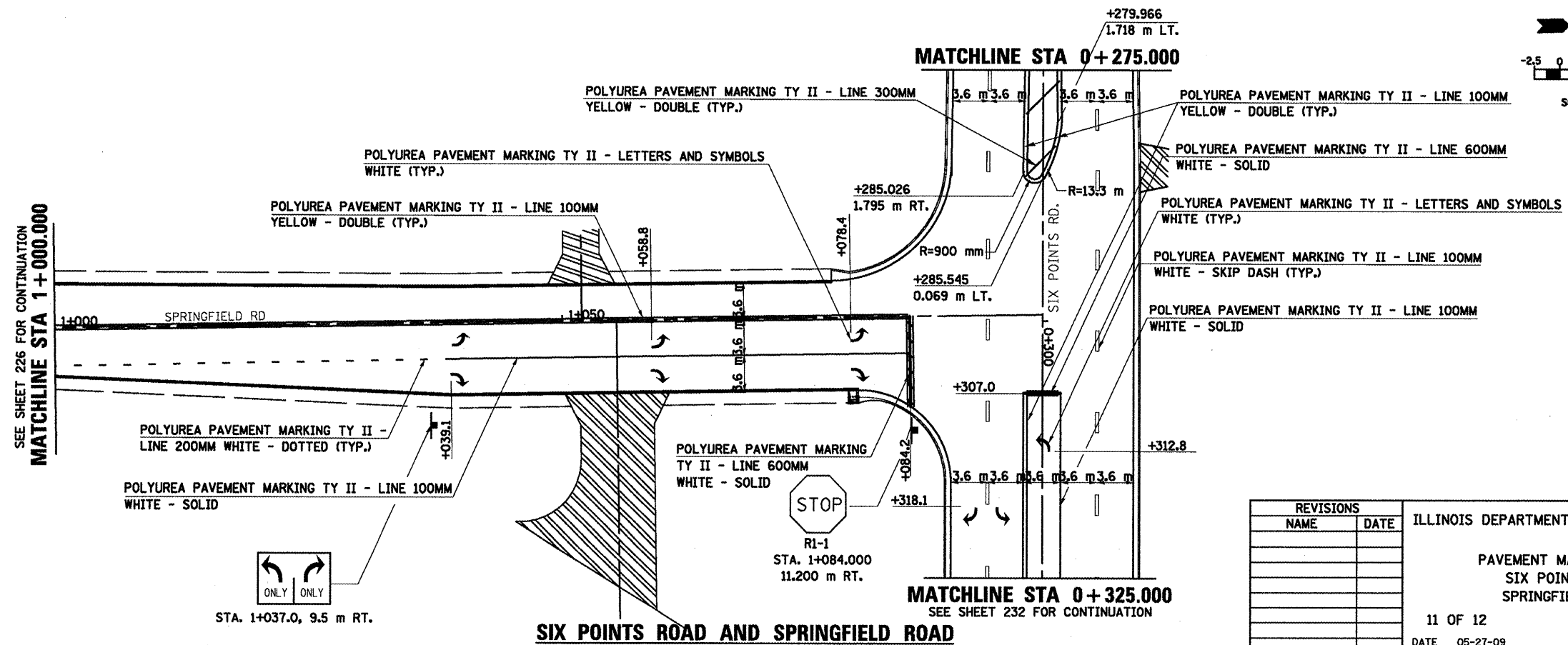
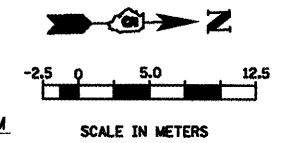
10 OF 12 DRAWN BY SAM
DATE 03-31-09 CHECKED BY PMH

| | | | | |
|---------------------|--------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1) N & TS-1 | McLEAN | 497 | 236 |
| STA. 0+150.000 | | TO STA. 0+325.000 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 70514



SIX POINTS ROAD

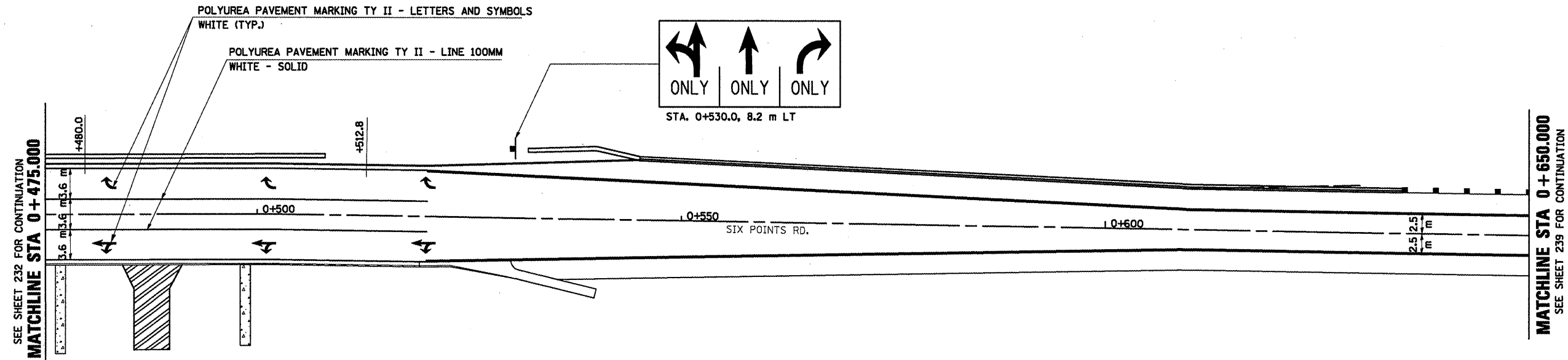
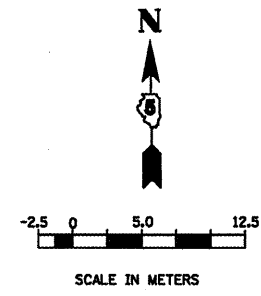


SIX POINTS ROAD AND SPRINGFIELD ROAD

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
 PAVEMENT MARKING PLAN
 SIX POINTS ROAD
 SPRINGFIELD ROAD
 11 OF 12
 DATE 05-27-09
 DRAWN BY SAM
 CHECKED BY PMH

| | | | | |
|---------------------|--------------|-------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1) N & TS-1 | McLEAN | 497 | 237 |
| STA. 0+475.000 | | TO STA. 0+650.000 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



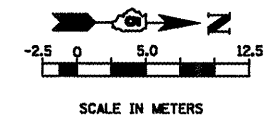
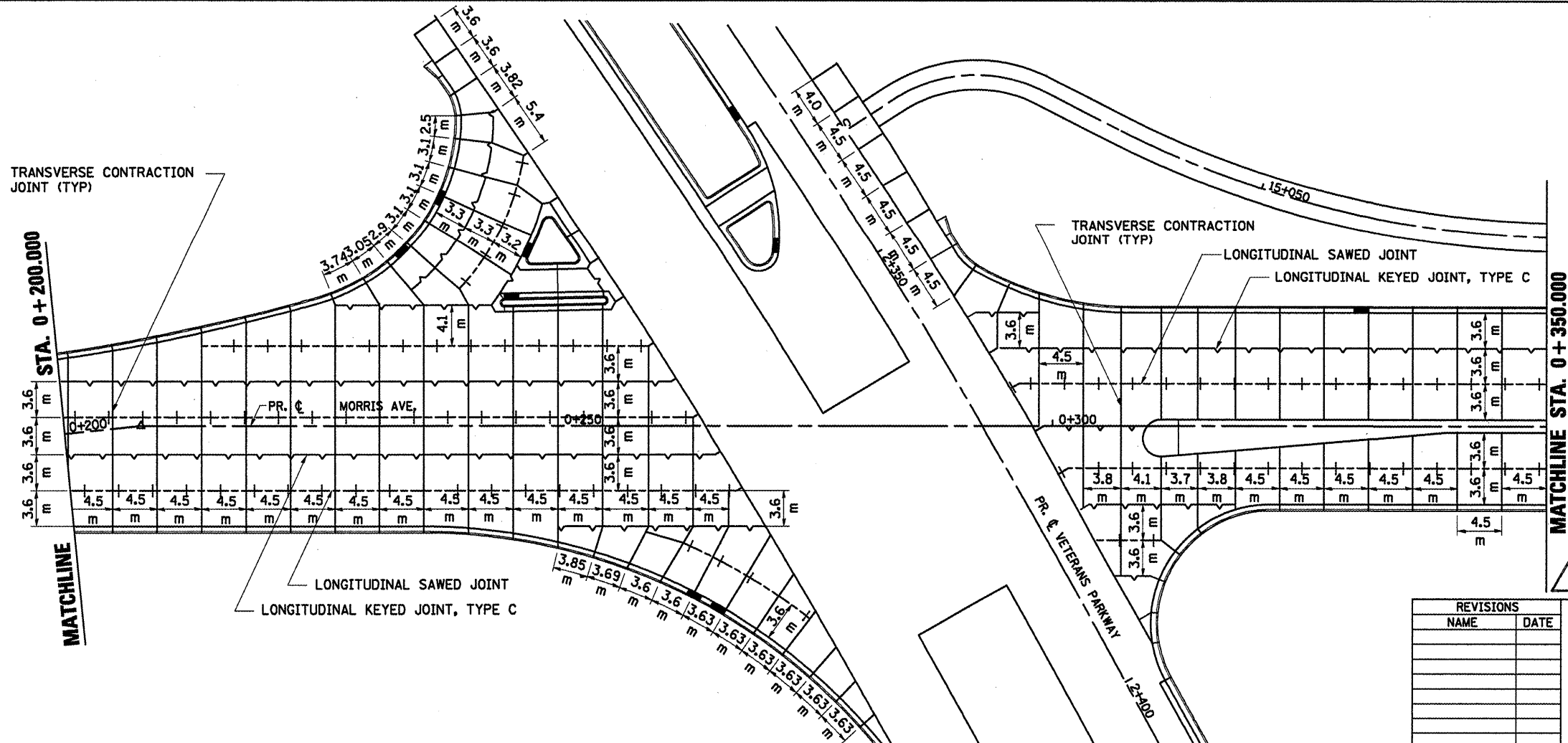
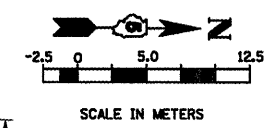
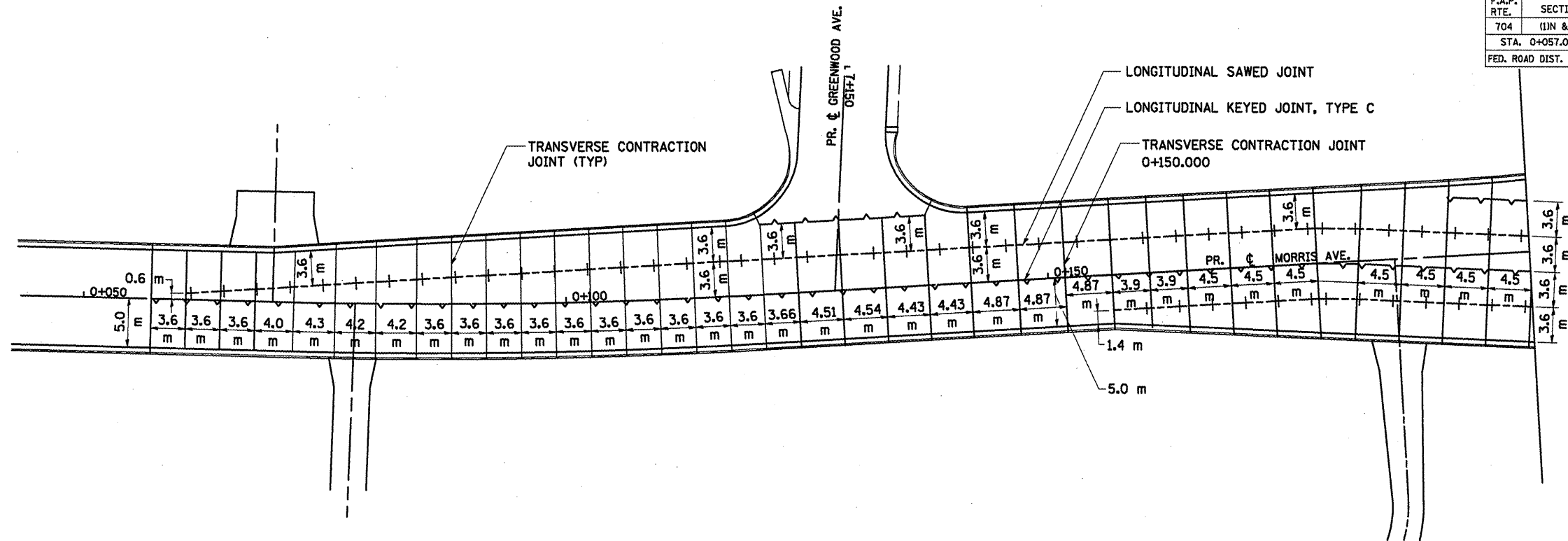
SIX POINTS ROAD

REVISED SHEET 10/25/10

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---|
| NAME | DATE | |
| | | PAVEMENT MARKING PLAN SIX POINTS ROAD 12 OF 12 DATE 03-31-09 |
| | | |
| | | |
| | | |
| | | |
| | | |

DRAWN BY SAM
 CHECKED BY PMH

| | | | | |
|---------------------|--------------|-------------------|------------------|--------------------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS NO. | SHEET NO. |
| 704 | (11N & TS-1) | McLEAN | 497 | 238 |
| STA. 0+057.000 | | TO STA. 0+350.000 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | CONTRACT NO. 70514 |



REVISIONS SHEET 10/25/10

| REVISIONS | NAME | DATE |
|-----------|------|------|
| | | |
| | | |
| | | |
| | | |
| | | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

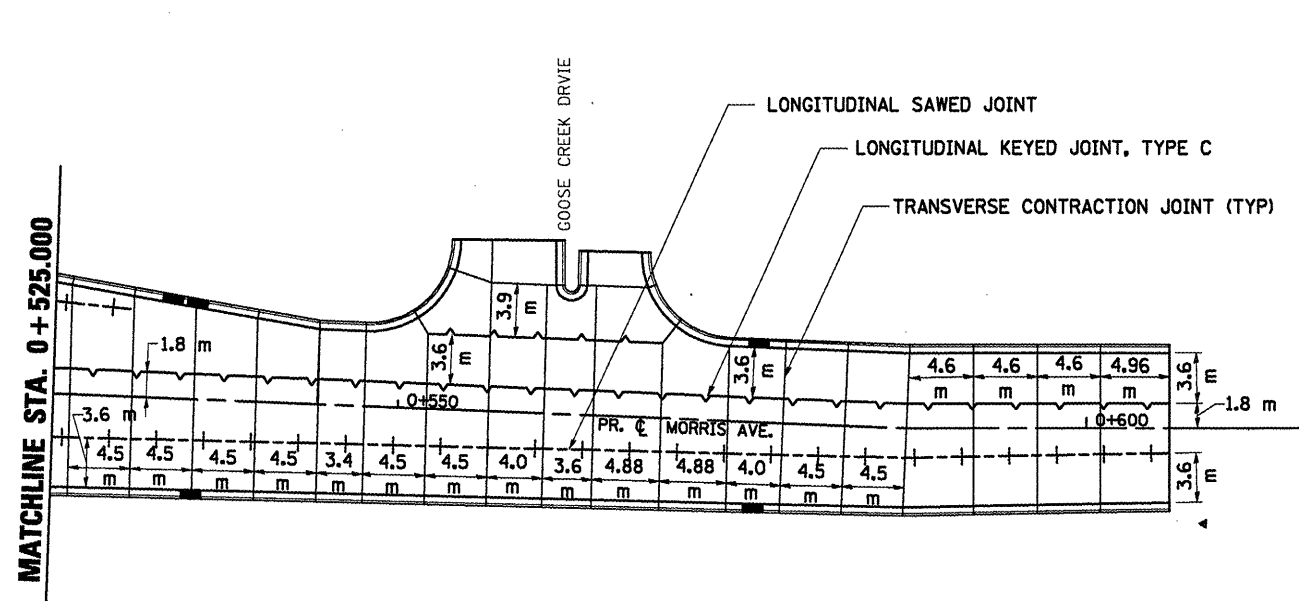
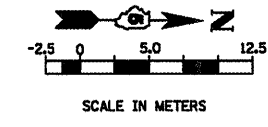
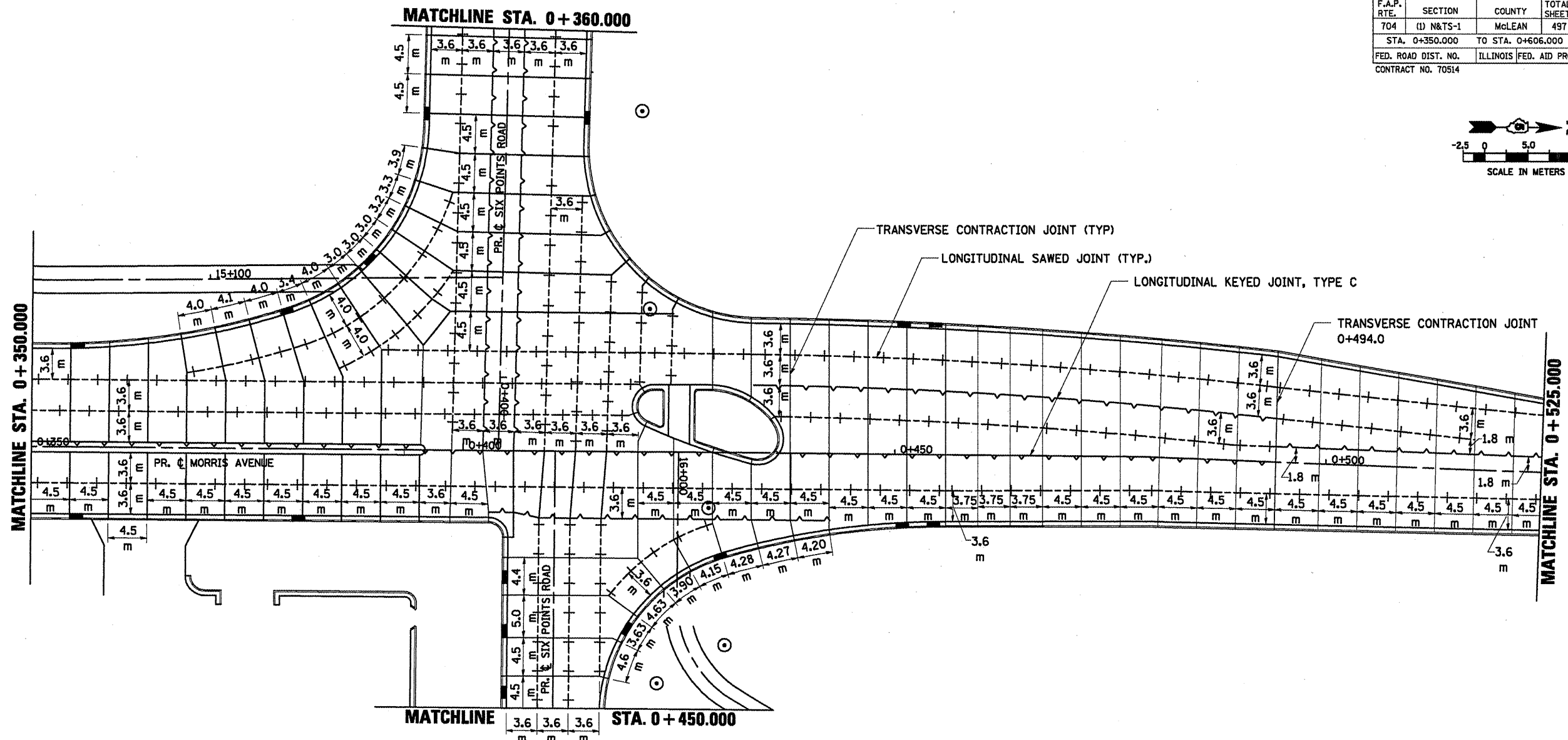
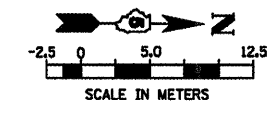
PAVEMENT JOINTING PLAN
MORRIS AVENUE

DRAWN BY SAM
CHECKED BY PMH

DATE 05-27-09

MORRIS AVENUE PAVEMENT JOINTING PLAN STA. 0+057.000 TO STA. 0+350.000

| | | | | |
|---------------------|------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1) N&TS-1 | McLEAN | 497 | 239 |
| STA. 0+350.000 | | TO STA. 0+606.000 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |

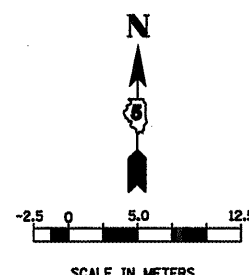
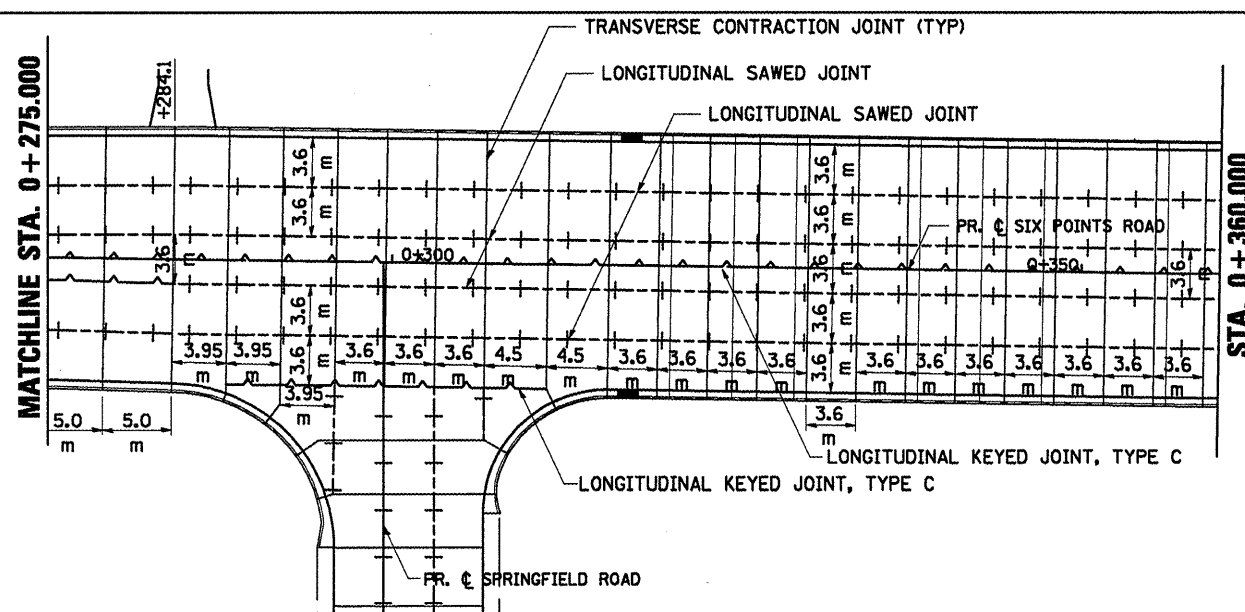
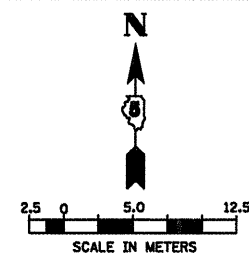
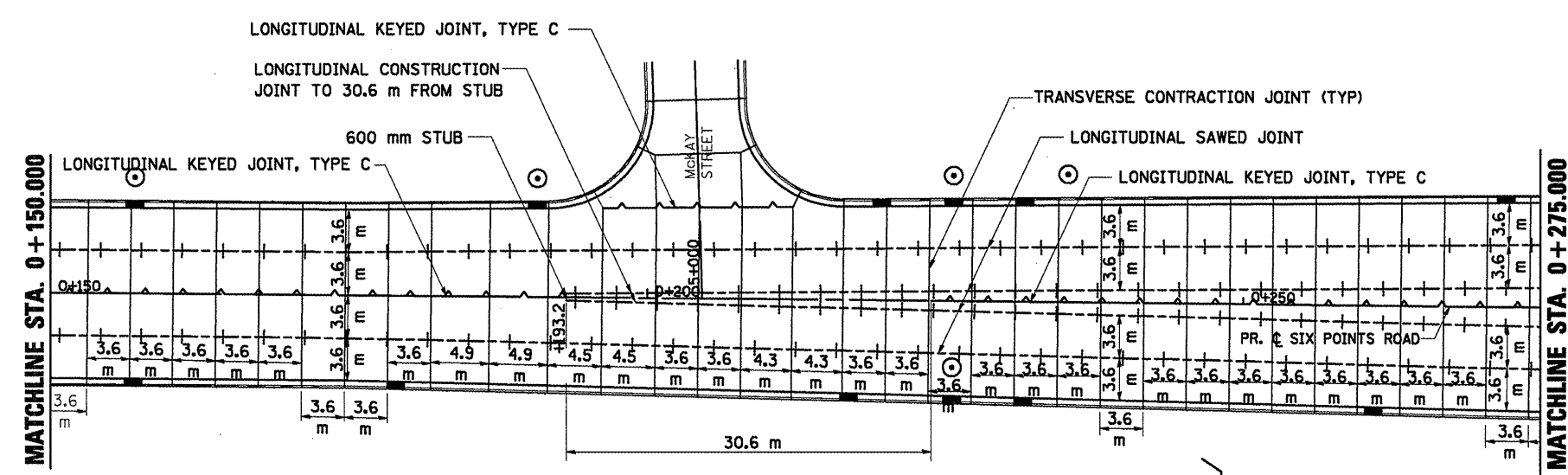
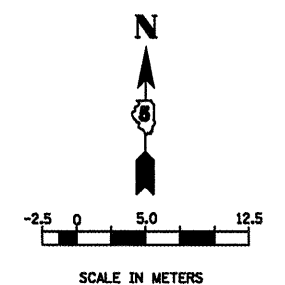
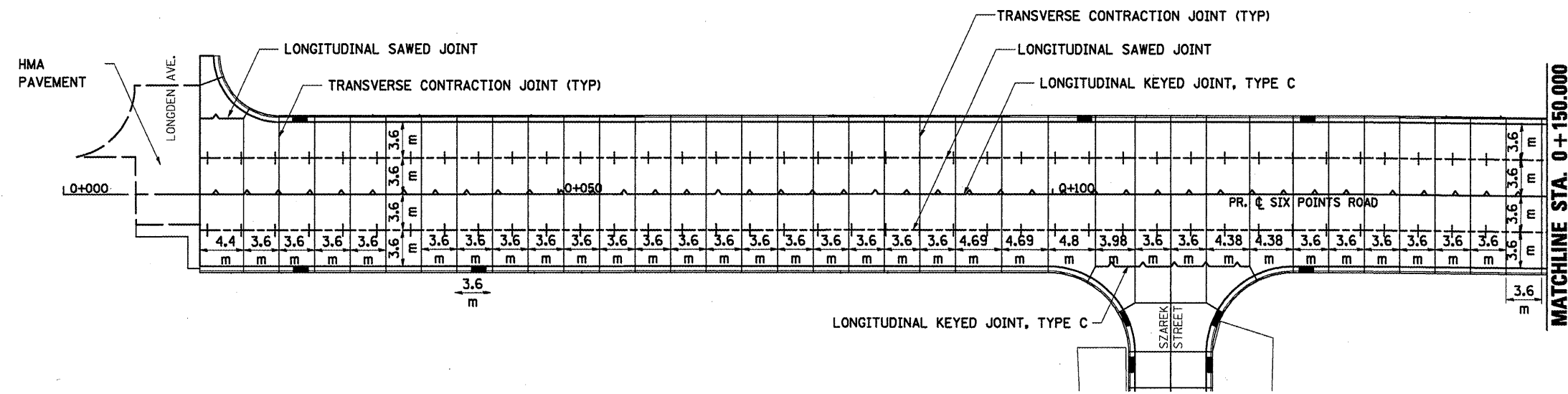


REVISIONS **REVISOR** **DATE** **ILLINOIS DEPARTMENT OF TRANSPORTATION**

| REVISIONS | REVISOR | DATE |
|-----------|---------|------|
| | | |
| | | |
| | | |
| | | |
| | | |

PAVEMENT JOINTING PLAN
MORRIS AVENUE
 DRAWN BY SAM
 CHECKED BY PMH
 DATE 05-27-09

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|------------|---------------------------|--------------|-----------|
| 704 | 11N & TS-1 | McLEAN | 497 | 240 |
| STA. 0+013.771 | | TO STA. 0+360.000 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



REVISIED SHEET 10/25/10

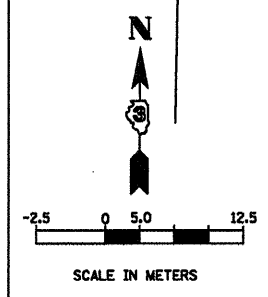
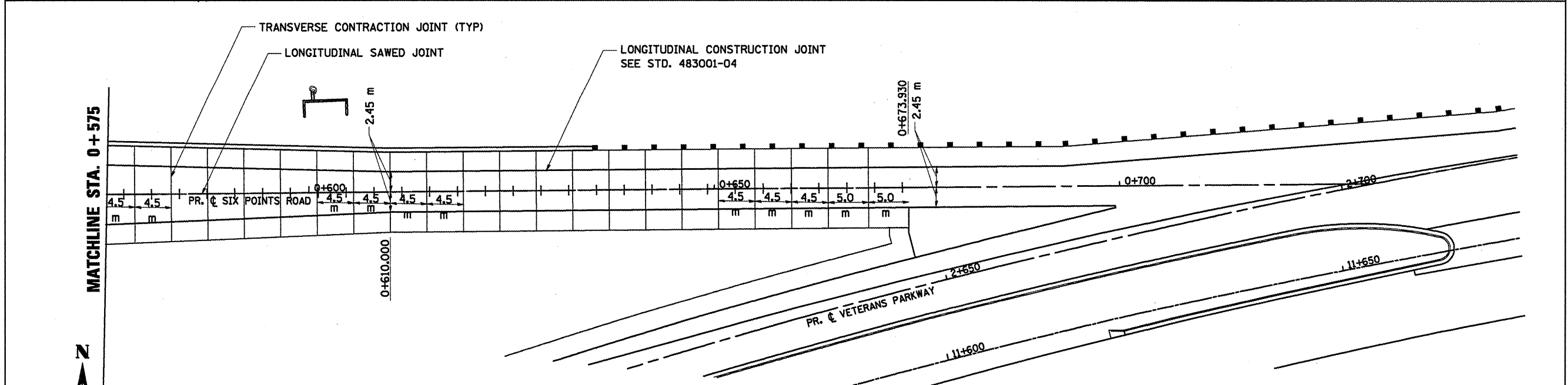
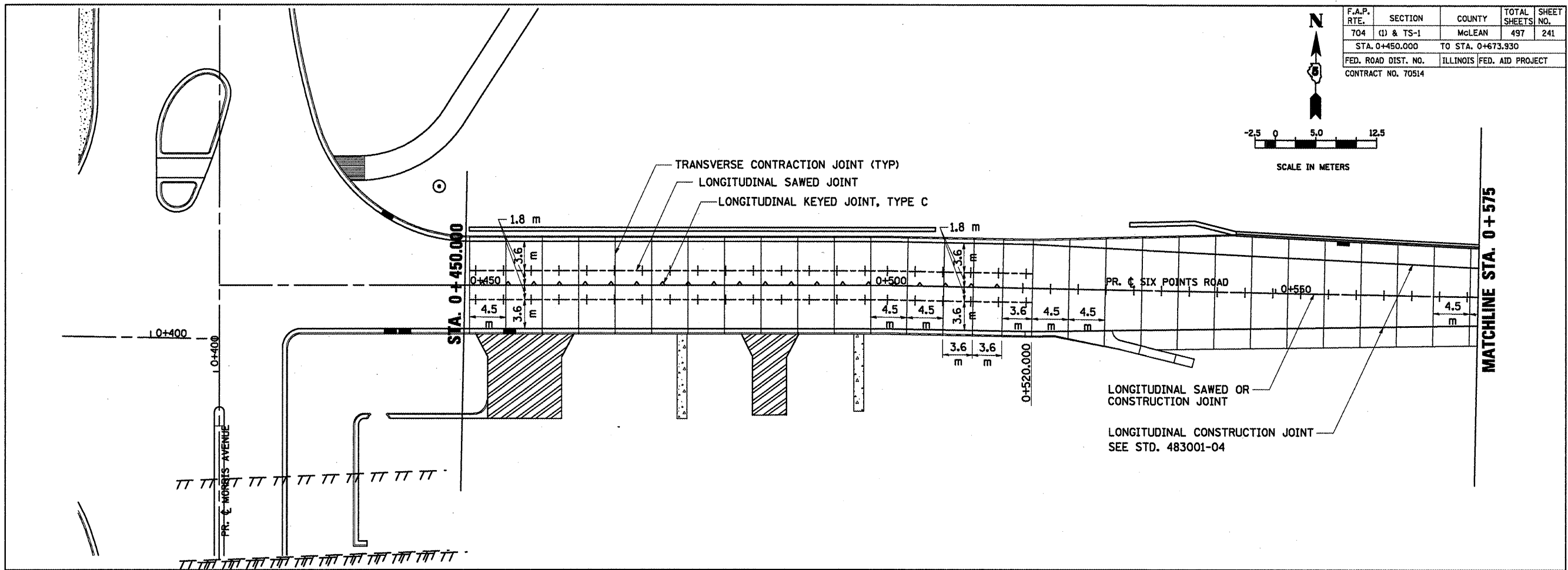
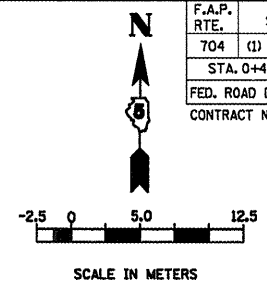
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT JOINTING PLAN
SIX POINTS ROAD (WEST)

DRAWN BY SAM
CHECKED BY PMH
DATE 05-27-09

| | | | | |
|---------------------|------------|-------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (I) & TS-1 | McLEAN | 497 | 241 |
| STA. 0+450.000 | | TO STA. 0+673.930 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

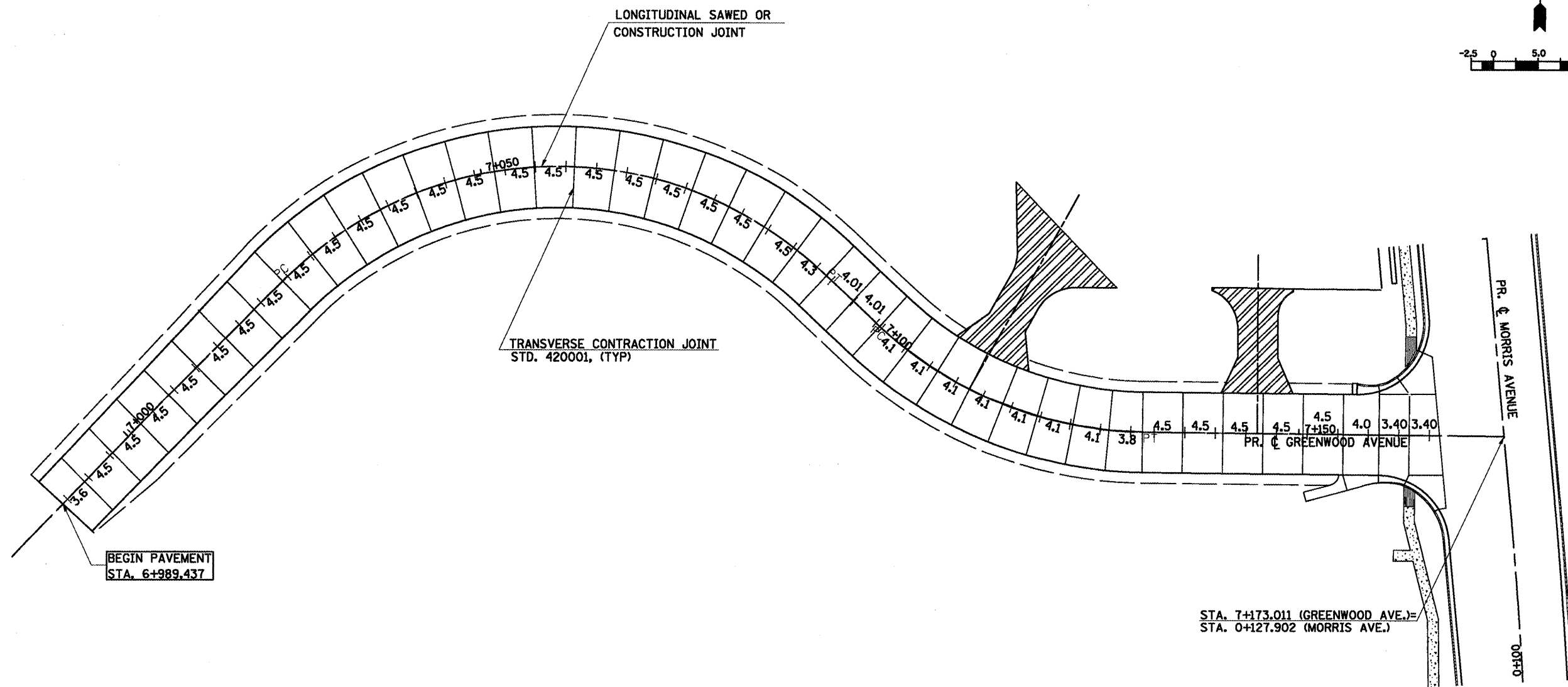
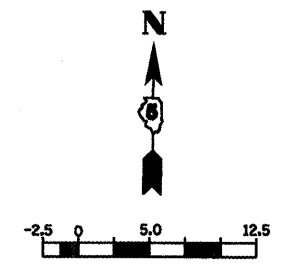
PAVEMENT JOINTING PLAN
SIX POINTS ROAD (EAST)

DATE 05-27-09

DRAWN BY SAM
CHECKED BY PMH

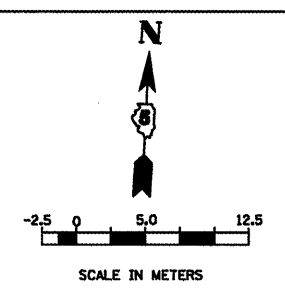
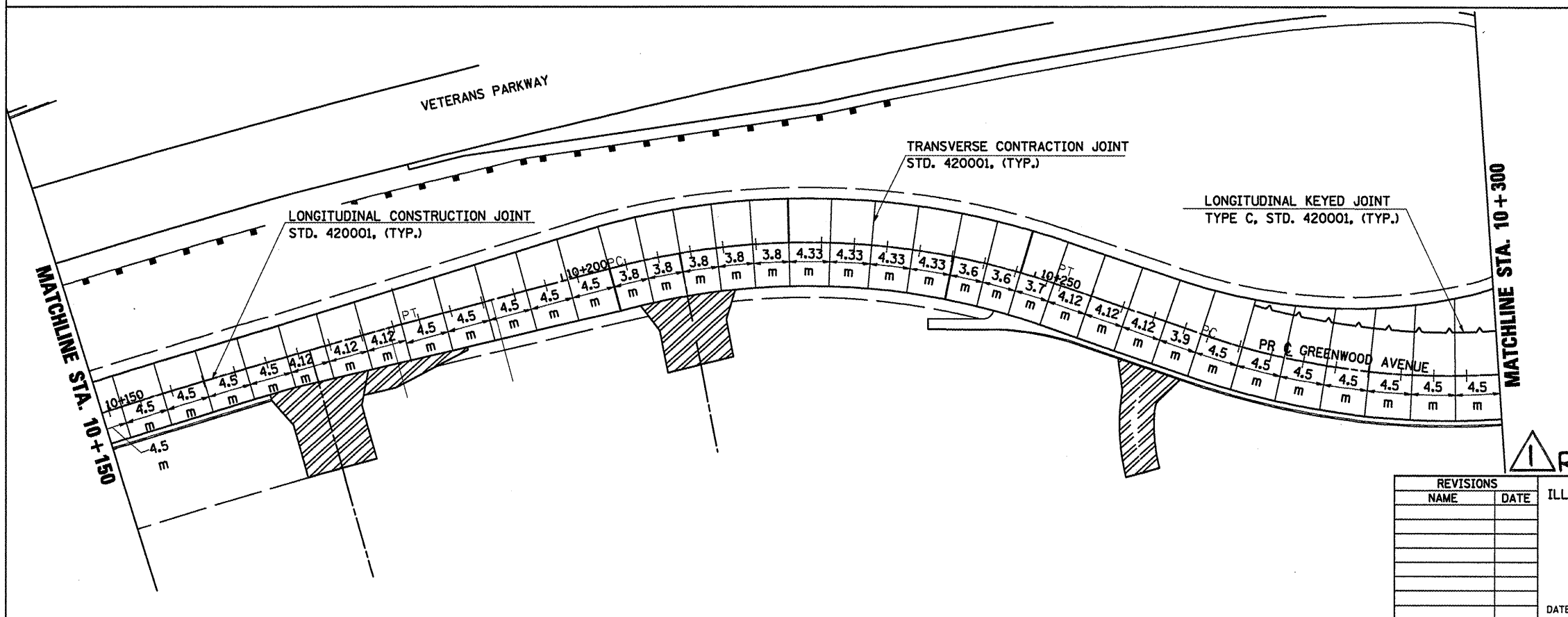
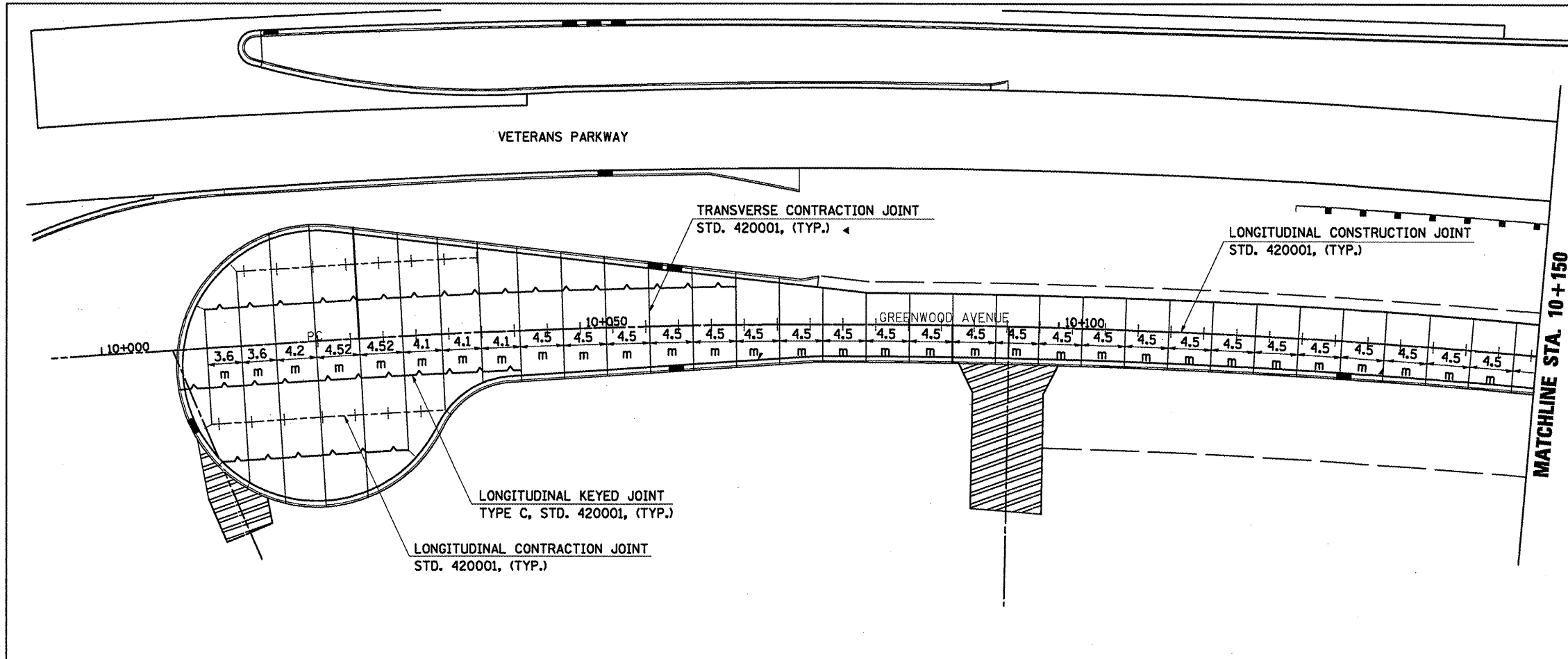
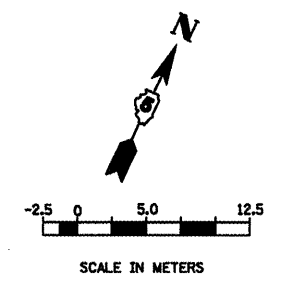
REVISIED SHEET 10/25/10

| | | | | |
|---------------------|---------------------------|-------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1)N & TS-1 | McLEAN | 497 | 242 |
| STA. 6+989.437 | | TO STA. 7+173.011 | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |
| CONTRACT NO. 70514 | | | | |



| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|----------|---|
| NAME | DATE | |
| | | PAVEMENT JOINTING PLAN GREENWOOD AVENUE (WEST) |
| | | |
| | | |
| | | |
| | | |
| | | |
| DATE | 05-27-09 | DRAWN BY SAM CHECKED BY PMH |

| | | | | |
|---------------------|------------|--------------------|--------------|--------------------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (DN & TS-1 | McLEAN | 497 | 243 |
| STA. 10+000.000 | | TO STA. 10+300.000 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | CONTRACT NO. 70514 |



REVISIED SHEET 10/25/10

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

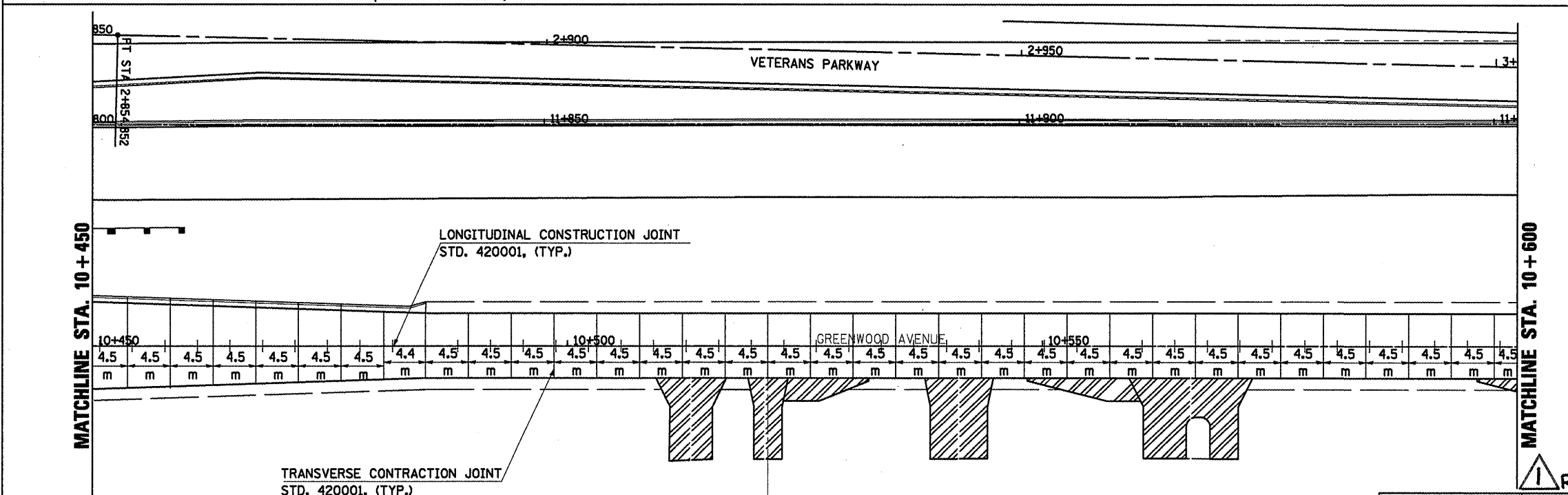
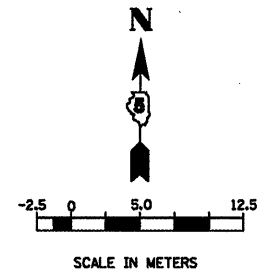
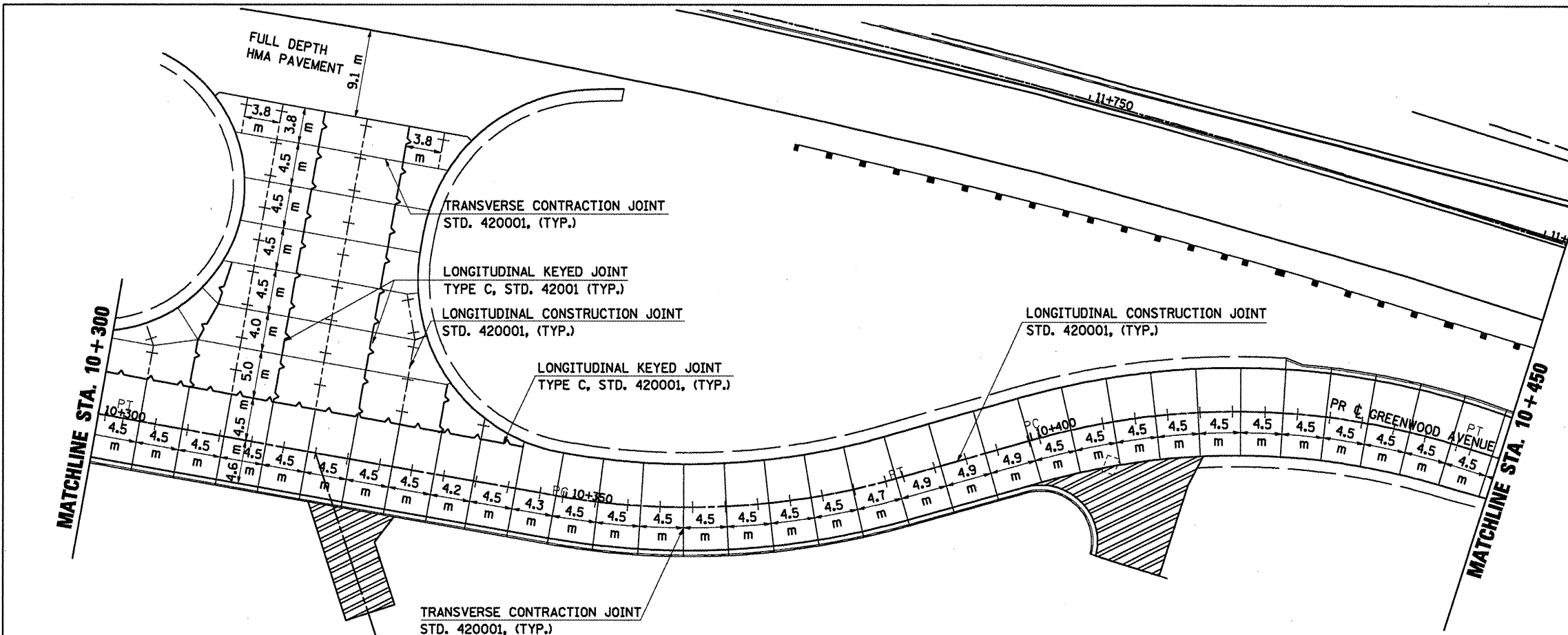
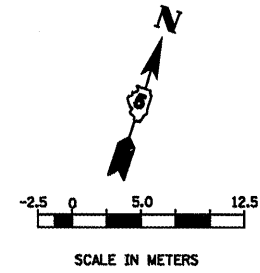
ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT JOINTING PLAN
GREENWOOD AVENUE (EAST)

DRAWN BY SAM
CHECKED BY PMH
DATE 05-27-09

GREENWOOD AVENUE PAVEMENT JOINTING PLAN STA. 10+000.000 TO STA. 10+300.000

| | | | | |
|---------------------|-------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (IN & TS-1) | McLEAN | 497 | 244 |
| STA. 10+300.000 | | TO STA. 10+600.000 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



REVISIED SHEET 10/25/10

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

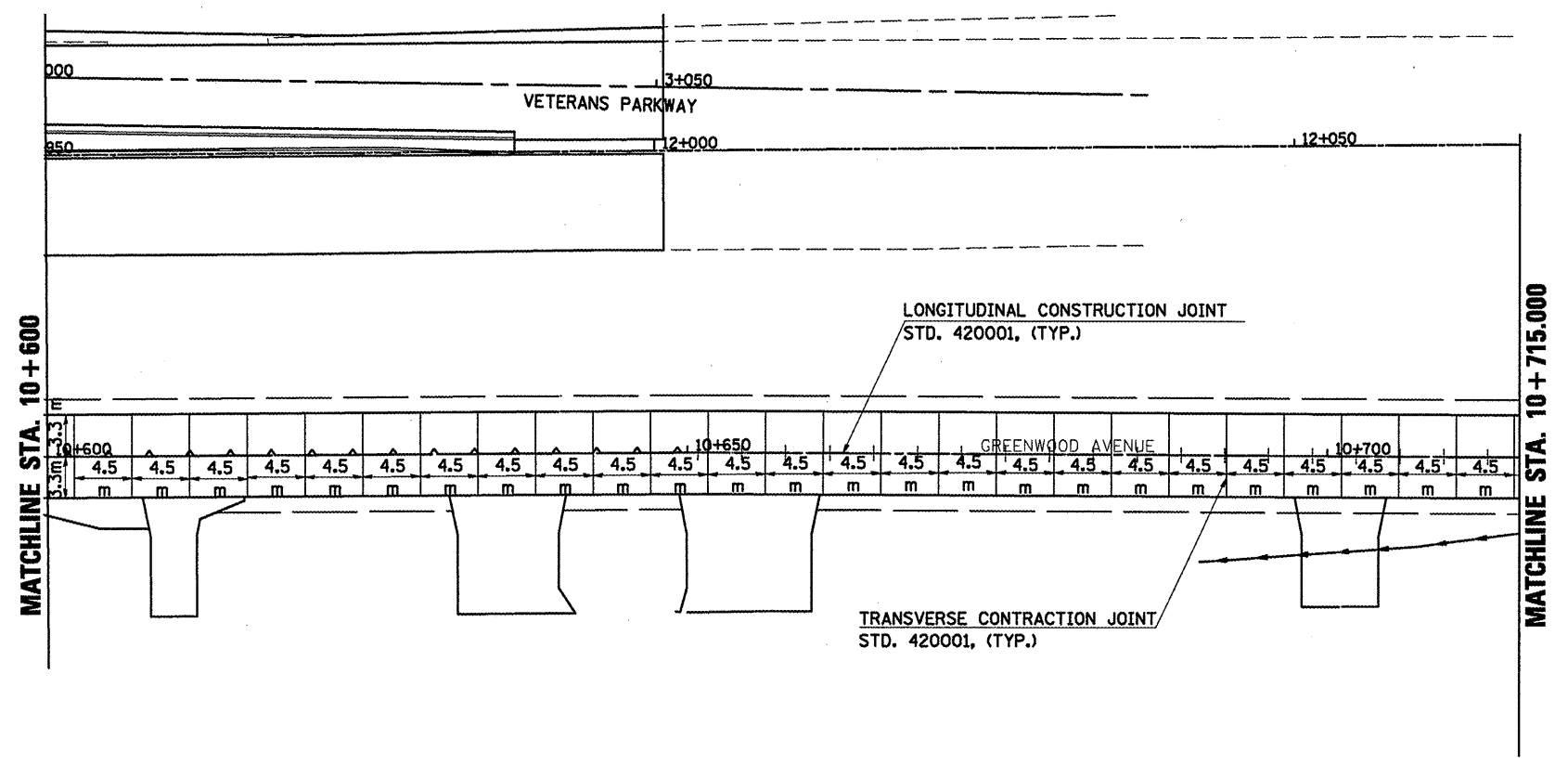
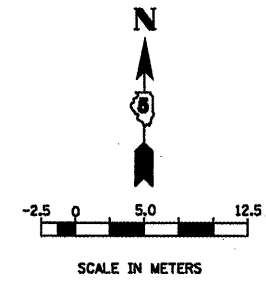
ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT JOINTING PLAN
GREENWOOD AVENUE (EAST)

DRAWN BY SAM
CHECKED BY PMH
DATE 05-27-09

GREENWOOD AVENUE PAVEMENT JOINTING PLAN STA. 10+300.000 TO STA. 10+600.000

| | | | | |
|---------------------|-------------|--------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1)N & TS-1 | McLEAN | 497 | 245 |
| STA. 10+600.000 | | TO STA. 10+715.000 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



REVISIED SHEET 10/25/10

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

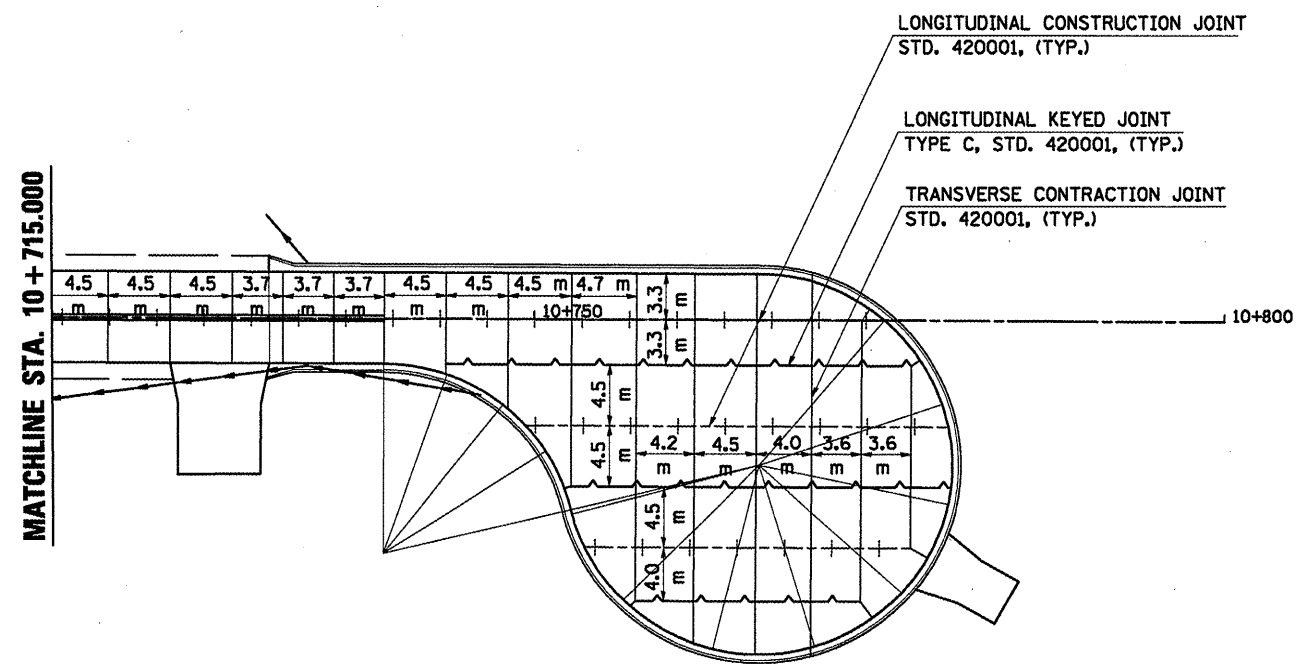
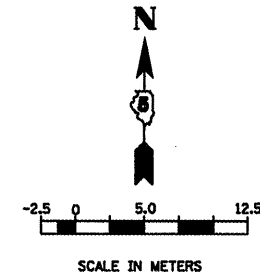
ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT JOINTING PLAN
GREENWOOD AVENUE (EAST)

DRAWN BY SAM
CHECKED BY PMH

DATE 05-27-09

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|------------|--------------------|--------------|-----------|
| 704 | 11N & TS-1 | McLEAN | 497 | 246 |
| STA. 10+715.000 | | TO STA. 10+800.000 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |

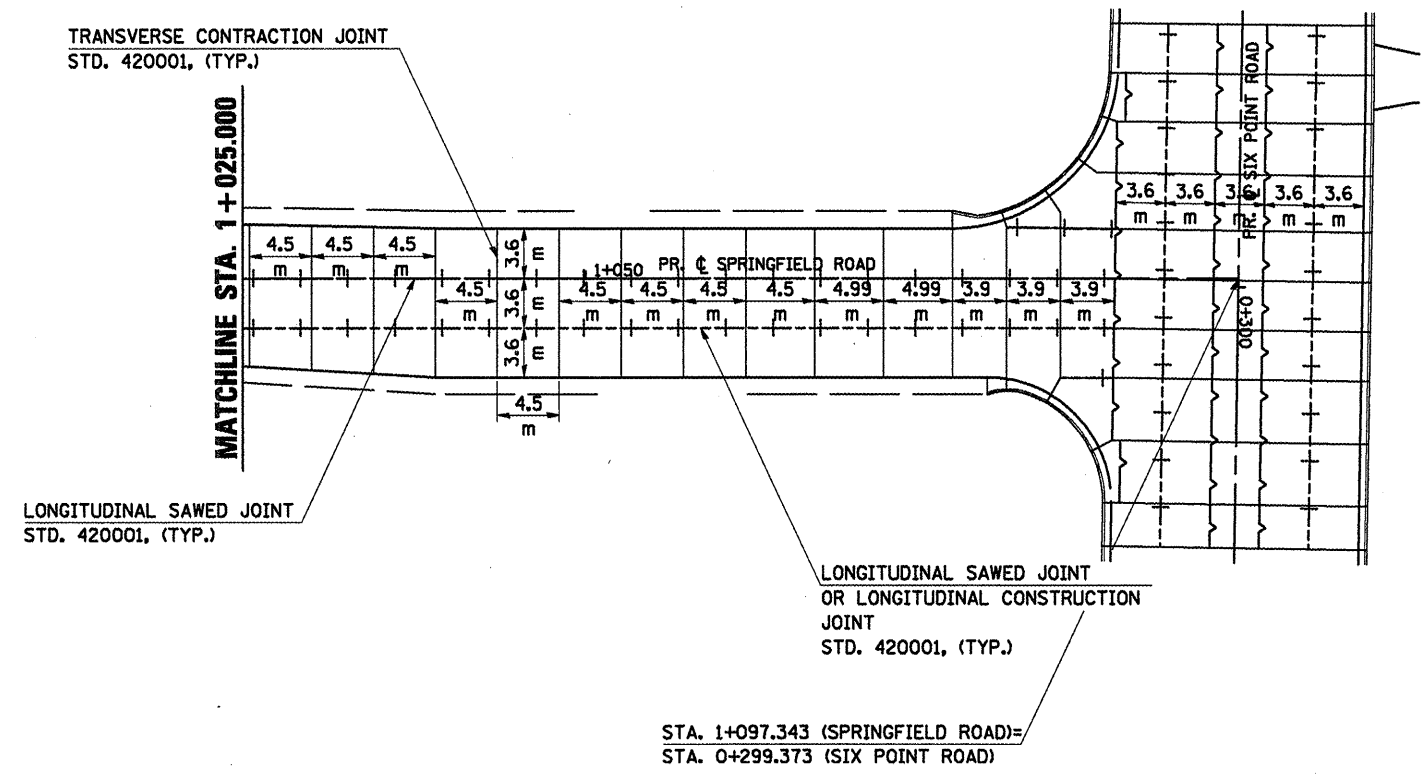
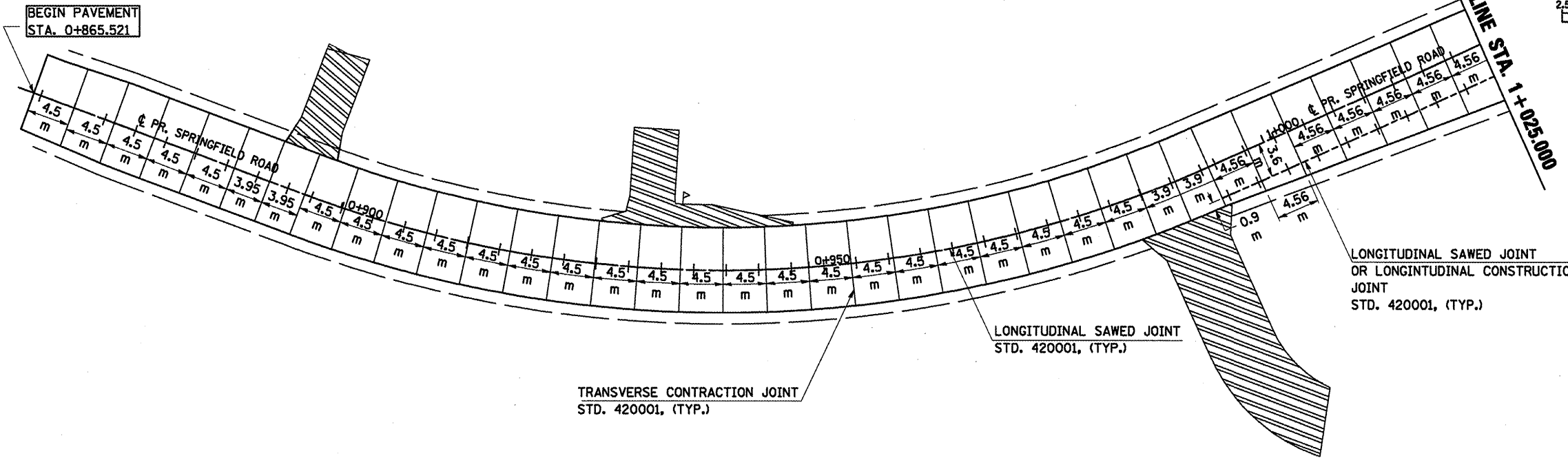
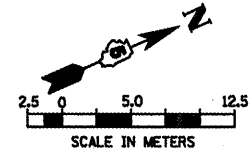


REVISIED SHEET 10/25/10

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---|
| NAME | DATE | |
| | | PAVEMENT JOINTING PLAN GREENWOOD AVENUE (EAST) |
| | | |
| | | |
| | | |
| | | |
| | | |

DRAWN BY SAM
CHECKED BY PMH
DATE 05-27-09

| | | | | |
|---------------------|-------------|-------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (DN & TS-1) | McLEAN | 497 | 248 |
| STA. 0+865.521 | | TO STA. 1+025.000 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



REVISIONS SHEET 10/25/10

| REVISIONS | NAME | DATE |
|-----------|------|------|
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

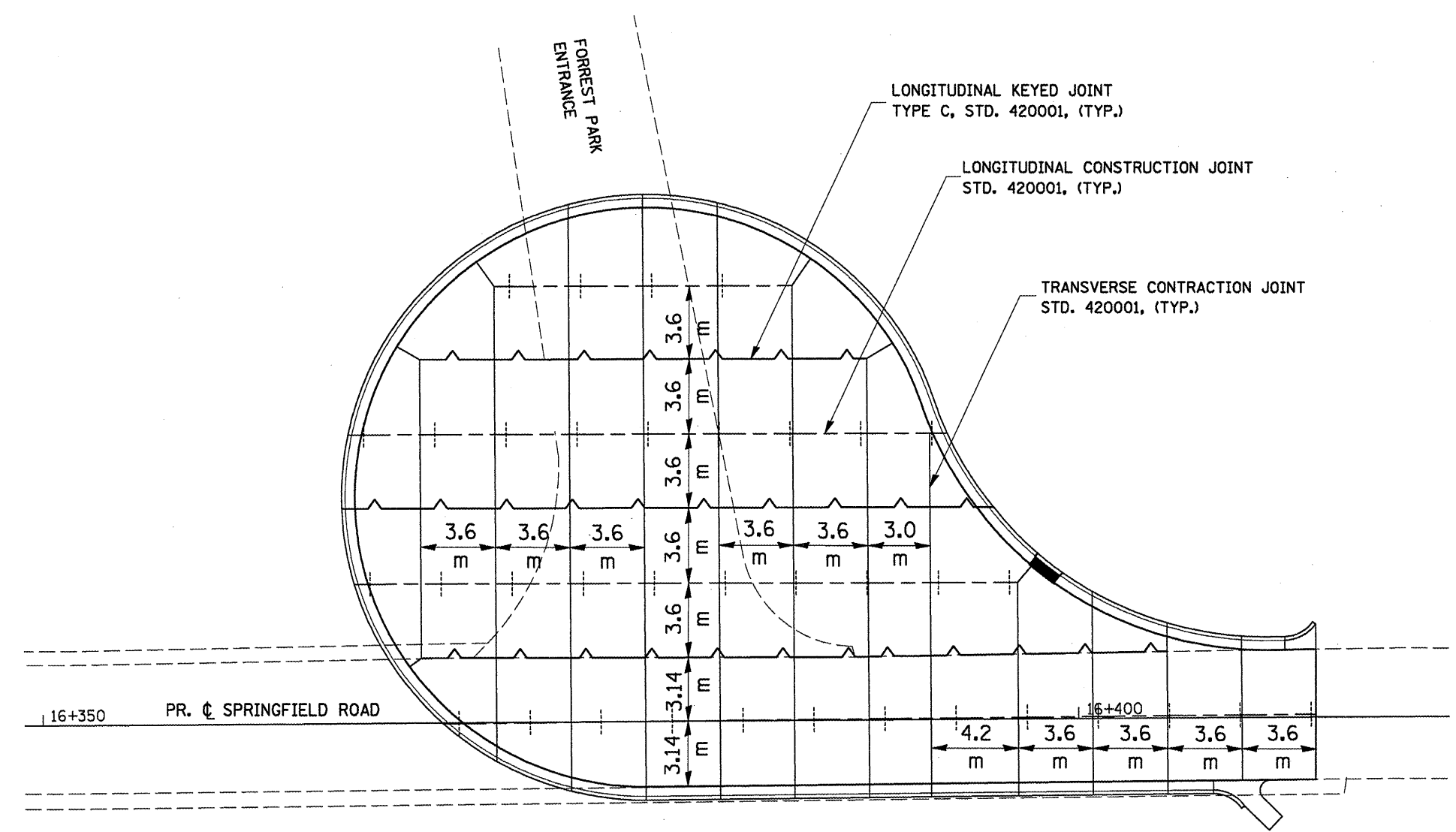
PAVEMENT JOINT PLANS
SPRINGFIELD ROAD (WEST)

DRAWN BY SAM
CHECKED BY PMH

DATE 05-27-09

CONTRACT NO. 70514

| | | | | |
|---------------------|---------------------------|--------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (LN & TS-1D) | McLEAN | 497 | 249 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |



PAVEMENT JOINTING NOTES:
 LONGITUDINAL CONSTRUCTION JOINTS AND
 TRANSVERSE CONTRACTION JOINTS MAY BE
 SUBSTITUTED DEPENDING ON POURING
 SEQUENCE.

1 REVISED SHEET 10/25/10

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVEMENT JOINTING PLAN
SPRINGFIELD ROAD (EAST)
 DRAWN BY RC
 CHECKED BY KJT
 DATE 6/29/2010

PLOT DATE = 10/19/2010
 FILE NAME = c:\pwworkspace\craig\proj\1010\1010\1010.dwg
 PLOT SCALE = 6.7588 m / in.
 USER NAME = craig

BILL OF MATERIALS - TRAFFIC SIGNALS

| | | | | |
|---------------------|-------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (DN & TS-1) | McLean | 497 | 250 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 70514

| ITEM | UNIT | TOTAL QUANTITY | VETERANS PKWY. & MORRIS AVE | MORRIS AVE & SIX POINT RD | VETERANS PKWY. & GREENWOOD | INTERCONNECT |
|---|-------|----------------|-----------------------------|---------------------------|----------------------------|--------------|
| HANDHOLE | EACH | 25 | 9 | 7 | 5 | 4 |
| HEAVY-DUTY HANDHOLE | EACH | 4 | 2 | 1 | 1 | 0 |
| DOUBLE HANDHOLE | EACH | 3 | 1 | 1 | 1 | 0 |
| FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL MASTER CONTROLLER | EACH | 3 | 1 | 1 | 1 | 0 |
| TRANSCIEVER - FIBER OPTIC | EACH | 1 | 0 | 0 | 0 | 1 |
| DRILL EXISTING HANDHOLE | EACH | 0 | 0 | 0 | 0 | 3 |
| TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC | EACH | 1 | 0 | 0 | 0 | 1 |
| INDUCTIVE LOOP DETECTOR | EACH | 51 | 22 | 17 | 12 | 0 |
| PEDESTRIAN PUSH-BUTTON | EACH | 53 | 18 | 22 | 13 | 0 |
| TEMPORARY TRAFFIC SIGNAL INSTALLATION | EACH | 7 | 3 | 4 | 0 | 0 |
| REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT | EACH | 2 | 1 | 1 | 0 | 0 |
| REMOVE EXISTING HANDHOLE | EACH | 2 | 1 | 0 | 0 | 0 |
| REMOVE EXISTING CONCRETE FOUNDATION | EACH | 12 | 8 | 4 | 0 | 0 |
| SIGN PANEL-TYPE 1 | SQ M | 12 | 8 | 4 | 0 | 0 |
| SIGN PANEL-TYPE 2 | SQ M | 6 | 4 | 1 | 1 | 0 |
| CONDUIT IN TRENCH, 50MM DIA., PVC | METER | 9 | 2 | 4 | 3 | 0 |
| CONDUIT IN TRENCH, 65MM DIA., PVC | METER | 1467 | 233 | 224 | 222 | 788 |
| CONDUIT IN TRENCH, 100MM DIA., PVC | METER | 98 | 20 | 26 | 52 | 0 |
| CONDUIT PUSHED, 50MM DIA., GALVANIZED STEEL | METER | 62 | 44 | 14 | 4 | 0 |
| CONDUIT PUSHED, 75MM DIA., GALVANIZED STEEL | METER | 132 | 14 | 18 | 0 | 100 |
| CONDUIT PUSHED, 100MM DIA., GALVANIZED STEEL | METER | 137 | 51 | 35 | 51 | 0 |
| TRENCH AND BACKFILL FOR ELECTRICAL WORK | METER | 213 | 107 | 69 | 37 | 0 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C | METER | 1777 | 357 | 321 | 311 | 788 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C | METER | 4883 | 1888 | 1985 | 1010 | 0 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | METER | 731 | 457 | 274 | 0 | 0 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | METER | 3706 | 1784 | 958 | 964 | 0 |
| ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR | METER | 486 | 105 | 285 | 96 | 0 |
| ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C | METER | 1018 | 368 | 342 | 308 | 0 |
| TRAFFIC SIGNAL POST, GALVANIZED STEEL 3.0 METER | EACH | 72 | 24 | 12 | 36 | 0 |
| TRAFFIC SIGNAL POST, GALVANIZED STEEL 3.65 METER | EACH | 2 | 2 | 0 | 0 | 0 |
| TRAFFIC SIGNAL POST, GALVANIZED STEEL 4.85 METER | EACH | 4 | 3 | 1 | 0 | 0 |
| STEEL MAST ARM ASSEMBLY AND POLE, 6.09 METER | EACH | 4 | 1 | 2 | 1 | 0 |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 8.53 METER | EACH | 1 | 0 | 1 | 0 | 0 |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 14.63 METER | EACH | 1 | 0 | 0 | 1 | 0 |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 11.58 METER | EACH | 2 | 0 | 0 | 2 | 0 |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 12.19 METER | EACH | 1 | 0 | 1 | 0 | 0 |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 12.80 METER | EACH | 1 | 1 | 0 | 0 | 0 |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 13.41 METER | EACH | 1 | 0 | 1 | 0 | 0 |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 14.02 METER | EACH | 1 | 0 | 1 | 0 | 0 |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 15.24 METER | EACH | 1 | 1 | 0 | 0 | 0 |
| CONCRETE FOUNDATION, TYPE A | METER | 2 | 2 | 0 | 0 | 0 |
| CONCRETE FOUNDATION, TYPE D | METER | 12 | 7.2 | 3.6 | 1.2 | 0 |
| CONCRETE FOUNDATION, TYPE E, 600MM DIAMETER | METER | 3.6 | 1.2 | 1.2 | 1.2 | 0 |
| CONCRETE FOUNDATION, TYPE E, 750MM DIAMETER | METER | 9 | 0 | 9 | 0 | 0 |
| DETECTOR LOOP, TYPE I | METER | 50.6 | 18.4 | 18.4 | 13.8 | 0 |
| ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C | METER | 549 | 160 | 227 | 162 | 0 |
| FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F | METER | 4222 | 1308 | 1275 | 839 | 800 |
| ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C | METER | 3600 | 1700 | 600 | 300 | 1000 |
| SERVICE INSTALLATION - TYPE A | EACH | 610 | 265 | 164 | 181 | 0 |
| LIGHT POLE, WOOD, 1067 METERS, CLASS 3 | EACH | 3 | 1 | 1 | 1 | 0 |
| SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED | EACH | 3 | 1 | 1 | 1 | 0 |
| SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED | EACH | 30 | 12 | 9 | 9 | 0 |
| SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 7 | 5 | 1 | 1 | 0 |
| SIGNAL HEAD, LED, 1-FACE, 4-SECTION, MAST ARM MOUNTED | EACH | 0 | 0 | 0 | 0 | 0 |
| SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED | EACH | 1 | 0 | 1 | 0 | 0 |
| SIGNAL HEAD, LED, 2-FACE, 3 SECTION, BRACKET MOUNTED | EACH | 4 | 1 | 2 | 1 | 0 |
| SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED | EACH | 3 | 1 | 2 | 0 | 0 |
| PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER | EACH | 3 | 1 | 1 | 1 | 0 |
| LIGHT DETECTOR | EACH | 8 | 4 | 4 | 0 | 0 |
| LIGHT DETECTOR AMPLIFIER | EACH | 6 | 2 | 2 | 2 | 0 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 20 3/C TWISTED SHIELDED | EACH | 3 | 1 | 1 | 1 | 0 |
| TRAFFIC SIGNAL BATTERY BACKUP SYSTEM | METER | 437 | 121 | 121 | 195 | 0 |
| | EACH | 3 | 1 | 1 | 1 | 0 |

- EMERGENCY VEHICLE PREEMPTION (100% COST TO THE CITY OF BLOOMINGTON FIRE DEPARTMENT)

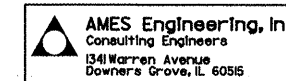
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SUMMARY OF QUANTITIES
TRAFFIC SIGNALS**

SCALE: NONE
DATE: MARCH, 2009

DRAWN BY: RV
DESIGNED BY: AS/SM
CHECKED BY: MSA



\$FILE\$ 10/14/2010 \$TIME\$

TS-01

| | | | | |
|---------------------|-------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1)N & TS-1 | McLean | 497 | 251 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 70514

ELECTRICAL LOAD FOR VETERANS PARKWAY & MORRIS AVENUE

| VETERANS PARKWAY (EB) | NUMBER | WATTAGE EACH | BURN TIME % |
|-------------------------|--------|--------------|-------------|
| RED | 3 | 10 | 64 |
| YELLOW | 3 | 19 | 5 |
| GREEN | 3 | 14 | 31 |
| RIGHT/LEFT YELLOW ARROW | 2 | 9 | 3 |
| RIGHT/LEFT GREEN ARROW | 2 | 7 | 16 |
| RIGHT/LEFT RED ARROW | 2 | 8 | 81 |
| PEDESTRIAN SIGNAL | - | 25 | 100 |

| VETERANS PARKWAY (WB) | NUMBER | WATTAGE EACH | BURN TIME % |
|-----------------------|--------|--------------|-------------|
| RED | 4 | 10 | 64 |
| YELLOW | 4 | 19 | 5 |
| GREEN | 4 | 14 | 31 |
| LEFT YELLOW ARROW | 1 | 9 | 3 |
| LEFT GREEN ARROW | 1 | 7 | 16 |
| LEFT RED ARROW | 1 | 8 | 81 |
| PEDESTRIAN SIGNAL | - | 25 | 100 |

| MORRIS AVENUE (NB) | NUMBER | WATTAGE EACH | BURN TIME % |
|--------------------|--------|--------------|-------------|
| RED | 2 | 10 | 82 |
| YELLOW | 2 | 19 | 5 |
| GREEN | 2 | 14 | 13 |
| LEFT YELLOW ARROW | 2 | 9 | 3 |
| LEFT GREEN ARROW | 2 | 7 | 12 |
| LEFT RED ARROW | 2 | 8 | 85 |
| PEDESTRIAN SIGNAL | 4 | 25 | 100 |

| MORRIS AVENUE (SB) | NUMBER | WATTAGE EACH | BURN TIME % |
|--------------------|--------|--------------|-------------|
| RED | 4 | 10 | 82 |
| YELLOW | 4 | 19 | 5 |
| GREEN | 4 | 14 | 13 |
| LEFT YELLOW ARROW | 2 | 9 | 3 |
| LEFT GREEN ARROW | 2 | 7 | 12 |
| LEFT RED ARROW | 2 | 8 | 85 |
| PEDESTRIAN SIGNAL | 4 | 25 | 100 |

ELECTRICAL LOAD FOR VETERANS PARKWAY & GREENWOOD AVE

| VETERANS PARKWAY (EB) | NUMBER | WATTAGE EACH | BURN TIME % |
|-----------------------|--------|--------------|-------------|
| RED | 3 | 10 | 33 |
| YELLOW | 3 | 19 | 4 |
| GREEN | 3 | 14 | 63 |
| RIGHT YELLOW ARROW | 2 | 9 | 4 |
| RIGHT GREEN ARROW | 2 | 7 | 63 |
| RIGHT RED ARROW | - | 8 | 33 |
| RED SIGNAL | - | 25 | 100 |

| VETERANS PARKWAY (WB) | NUMBER | WATTAGE EACH | BURN TIME % |
|-----------------------|--------|--------------|-------------|
| RED | 3 | 10 | 85 |
| YELLOW | 3 | 19 | 3 |
| GREEN | 3 | 14 | 12 |
| LEFT YELLOW ARROW | 2 | 9 | 3 |
| LEFT GREEN ARROW | 2 | 7 | 12 |
| LEFT RED ARROW | 2 | 8 | 85 |
| PEDESTRIAN SIGNAL | - | 25 | 100 |

| GREENWOOD AVENUE (NB) | NUMBER | WATTAGE EACH | BURN TIME % |
|---------------------------|--------|--------------|-------------|
| RIGHT & LEFT YELLOW ARROW | 4 | 9 | 3 |
| RIGHT & LEFT GREEN ARROW | 4 | 7 | 15 |
| RIGHT & LEFT RED ARROW | 4 | 8 | 82 |

ELECTRICAL LOAD FOR SIX POINTS ROAD & MORRIS AVENUE

| SIX POINTS ROAD (EB) | NUMBER | WATTAGE EACH | BURN TIME % |
|---------------------------|--------|--------------|-------------|
| RIGHT & LEFT YELLOW ARROW | 5 | 9 | 5 |
| RIGHT & LEFT GREEN ARROW | 5 | 7 | 20 |
| RIGHT & LEFT RED ARROW | 5 | 8 | 75 |
| PEDESTRIAN SIGNAL | 4 | 25 | 100 |

| SIX POINTS ROAD (WB) | NUMBER | WATTAGE EACH | BURN TIME % |
|----------------------|--------|--------------|-------------|
| RED | 2 | 10 | 70 |
| YELLOW | 2 | 19 | 5 |
| GREEN | 2 | 14 | 25 |
| LEFT GREEN ARROW | 1 | 7 | 25 |
| PEDESTRIAN SIGNAL | 4 | 25 | 100 |

| MORRIS AVENUE (NB) | NUMBER | WATTAGE EACH | BURN TIME % |
|--------------------|--------|--------------|-------------|
| RED | 3 | 10 | 80 |
| YELLOW | 3 | 19 | 5 |
| GREEN | 3 | 14 | 15 |
| LEFT YELLOW ARROW | 2 | 9 | 5 |
| LEFT GREEN ARROW | 2 | 7 | 15 |
| PEDESTRIAN SIGNAL | 2 | 25 | 100 |

| MORRIS AVENUE (SB) | NUMBER | WATTAGE EACH | BURN TIME % |
|--------------------|--------|--------------|-------------|
| RED | 3 | 10 | 75 |
| YELLOW | 3 | 19 | 5 |
| GREEN | 3 | 14 | 20 |
| RIGHT YELLOW ARROW | 2 | 9 | 5 |
| RIGHT GREEN ARROW | 2 | 7 | 20 |

NOTE: THE CONTRACTOR SHALL PAY FOR ALL ENERGY CHARGES UNTIL THE PROJECT IS ACCEPTED. BILLING SHALL THEN BE TRANSFERRED TO THE CITY OF BLOOMINGTON

GENERAL NOTES:

- THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION OF ALL UNDERGROUND UTILITIES PRIOR TO BEGINNING OF CONSTRUCTION. (J.U.L.I.E. 800-892-0123)
- ALL SIGNAL BASES SHALL BE LOCATED A MINIMUM OF 1.8 METERS (6 FEET) FROM THE FACE OF CURB UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- A SINGLE 12 GAUGE THHN CONDUCTOR SHALL BE FURNISHED AND LEFT IN PLACE IN ALL CONDUITS BETWEEN HANDHOLES AND FOUNDATIONS OR CONTROLLER AS INCIDENTAL TO THE RESPECTIVE CONDUIT PAY ITEM.
- THE PROPOSED TRAFFIC SIGNAL CONTROL, CABINET SHALL BE FURNISHED WITH A MANUAL CONTROL SWITCH AND MANUAL CONTROL CORD WITHIN THE POLICE DOOR COMPARTMENT AS INCIDENTAL TO THE CONTROL CABINET PAY ITEM.
- THE CONTRACTOR SHALL ARRANGE FOR A FACTORY OR SUPPLIER REPRESENTATIVE TO BE PRESENT AT THE INTERSECTION WHEN THE SIGNAL IS TURNED ON, INCIDENTAL TO THE CONTROLLER PAY ITEM.
- THE DEPARTMENT OF TRANSPORTATION TRAFFIC SIGNAL SYSTEMS ENGINEER (217) 466-7383 SHALL BE NOTIFIED AT LEAST 72 HOURS PRIOR TO THE TURNING ON OF THE TRAFFIC SIGNALS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ELECTRICAL SERVICE FOR THE TRAFFIC SIGNAL AND STREET LIGHTING, THE CONTRACTOR SHALL CONTACT THE UTILITY COMPANY PRIOR TO BEGINNING WORK TO OBTAIN THE UTILITY COMPANY REQUIREMENTS FOR THE SERVICE INSTALLATION.
- NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR PLACING CONDUIT AT GREATER THAN 0.60 METERS (2 FEET) MINIMUM DEPTH TO AVOID OBSTACLES SUCH AS UNDERGROUND UTILITIES.
- THE DOUBLE HANDHOLE SHALL HAVE 4 METERS (13 FEET) OF SLACK IN EACH CABLE NEATLY WOUND ON THE HOOKS. THE CABLE SHALL BE PAID FOR AT ITS INDIVIDUAL UNIT PRICE.
- ALL MAST ARM MOUNTED SIGNAL HEADS ON AN INDIVIDUAL MAST ARM SHALL BE MOUNTED SO THAT THE RED INDICATIONS ARE LEVEL WITH EACH OTHER.
- THE ELECTRICAL CONDUCTORS FOR ALL TRAFFIC SIGNAL HEADS SHALL BE SOLID, SOFT COPPER.
- THE DOUBLE HANDHOLE SHALL BE FURNISHED WITH RECESSED, INTEGRAL, HINGED LIDS.
- THE CONTRACTOR IS RESPONSIBLE FOR THE COST OF UNCOVERING OR HAND DIGGING AROUND UTILITIES AS NECESSARY, INCIDENTAL TO THE CONDUIT PAY ITEM.
- ALL THREADS OF BOLTS USED IN ASSEMBLY OF TRAFFIC SIGNAL COMPONENTS SHALL BE COATED WITH A NON-LEAD BASED ANTI-SIEZE COMPOUND, SIMILAR TO LEAD PLATE, PRIOR TO ASSEMBLY.
- ALL MAST ARM POLE BASES SHALL BE PROTECTED BY A STAINLESS STEEL MESH SCREENING AROUND THE BASEBOLTS TO PREVENT RODENT ENTRY, THE MESH SHALL BE SECURED TO THE BASE BY STAINLESS STEEL, BANDING AS INCIDENTAL TO THE INDIVIDUAL MAST ARM ASSEMBLY PAY ITEM.
- POLYCARBONATE BACKPLATES SHALL BE LOUVERED.
- ALL CONDUIT IN TRENCH SHALL BE PVC. ALL CONDUIT PUSHED MAY BE GALVANIZED STEEL OR PVC.
- ALL SIGNAL CABLE SHALL BE MARKED WITH ELECTRIC TAPE FOR IDENTIFICATION PURPOSES AS SHOWN ON THE WIRING DIAGRAM. THIS IDENTIFICATION SHALL BE DONE AT THE CONTROLLER AND IN THE SIGNAL HEAD.
- THE EXACT LOCATION OF THE MAST ARM ASSEMBLY AND POLE SHALL BE VERIFIED IN THE FIELD WITH THE ENGINEER.
- ALL SALVAGED ITEMS SHALL BECOME THE PROPERTY OF THE CITY OF BLOOMINGTON, THE CONTRACTOR SHALL GIVE THE RESIDENT ENGINEER A COPY OF A SIGNED RECEIPT FOR ALL ITEMS DELIVERED TO THE CITY.

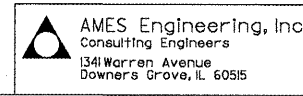
- THE CONTRACTOR SHALL GIVE THE CITY OF BLOOMINGTON AND STATE TRAFFIC OFFICE 72 HOURS NOTICE AND BE PRESENT FOR ANY CLASS OR DEMONSTRATION OF FIBER OPTIC SPLICING AND/OR INSTALLATION.
- EACH PROPOSED TRAFFIC SIGNAL CABINET SHALL BE PROVIDED WITH A DOOR SWITCH, CONFLICT FLASH AND MANUAL FLASH INPUTS WIRED TO THE APPROPRIATE CONTROLLER "D" CONNECTOR INPUTS.
- MOUNT A SURGE SUPPRESSOR WITH A 3-POSITION TERMINAL BLOCK ON AN ALUMINUM PLATE IN THE TRAFFIC SIGNAL CABINET BELOW THE POWER DISTRIBUTION PANEL. SURGE SUPPRESSOR SHALL BE MODEL HS-P-SP-120-30A-RJ AS MANUFACTURED BY INNOVATIVE TECHNOLOGY INC OR APPROVED EQUAL. INCOMING POWER SHALL CONNECT TO THE TERMINAL BLOCK AND FEED THE SURGE SUPPRESSOR THROUGH NO. 10 AWG SOLID COPPER WIRE (AC+, AC-, GND) WITH APPROXIMATELY TEN 1/2 TO 2 INCH COILS IN THE AC+ AND AC- LINES.
- CONFIRMATION BEACONS WILL NOT BE INSTALLED WITH THE PROPOSED LIGHT DETECTORS.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
VETERANS PARKWAY & MORRIS AVENUE
VETERANS PARKWAY & GREENWOOD AVENUE
SIX POINTS ROAD & MORRIS AVENUE
TRAFFIC SIGNAL NOTES

SCALE: _____
DATE: MARCH, 2009

DRAWN BY: RV
DESIGNED BY: AS
CHECKED BY: MSA



\$FILE\$ 8/19/2010 \$TIME\$

TEMPORARY TRAFFIC SIGNAL LEGEND

- ⊗ TEMPORARY WOOD POLE
- +▲ TEMPORARY TRAFFIC SIGNAL HEAD WITH BACKPLATE
- ▲▲ TEMPORARY TRAFFIC SIGNAL HEAD WITH BACK PLATE
- ▲ SPAN WIRE MOUNTED (SECONDARY LOCATION)
- GUY WIRE
- ⊠ TEMPORARY TRAFFIC SIGNAL CONTROLLER CABINET
- TEMPORARY SERVICE INSTALLATION

EXISTING EQUIPMENT TO BE REMOVED LEGEND

- ▲ EXISTING SIGNAL HEAD TO BE REMOVED
- EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED
- ▲ EXISTING MAST ARM POLE AND FOUNDATION TO BE REMOVED
- ⊠ EXISTING CONTROLLER TO BE REMOVED, FOUNDATION TO REMAIN
- ▲ EXISTING HANDHOLE TO BE REMOVED
- EXISTING HANDHOLE TO BE REMAIN
- ▲ SIGNAL HEAD, PEDESTRIAN
- ⊠ EXISTING JUNCTION BOX TO BE REMOVED
- ▲ EXISTING MAGNETIC DETECTOR TO BE REMOVED
- ▲ EMERGENCY VEHICLE LIGHT DETECTOR
- EXISTING PEDESTRIAN PUSHBUTTON TO BE REMOVED

TEMPORARY LIGHTING LEGEND

- ⊠ TEMPORARY LIGHTING CONTROLLER
- ▲ TEMPORARY SPAN WIRE, TETHER WIRE AND POWER CABLE
- ▲ 400W H.P.S. LUMINAIRE MOUNTED 3.5 m LONG MAST ARM ASSEMBLY
- EXISTING LIGHTING EQUIPMENT TO BE REMOVED

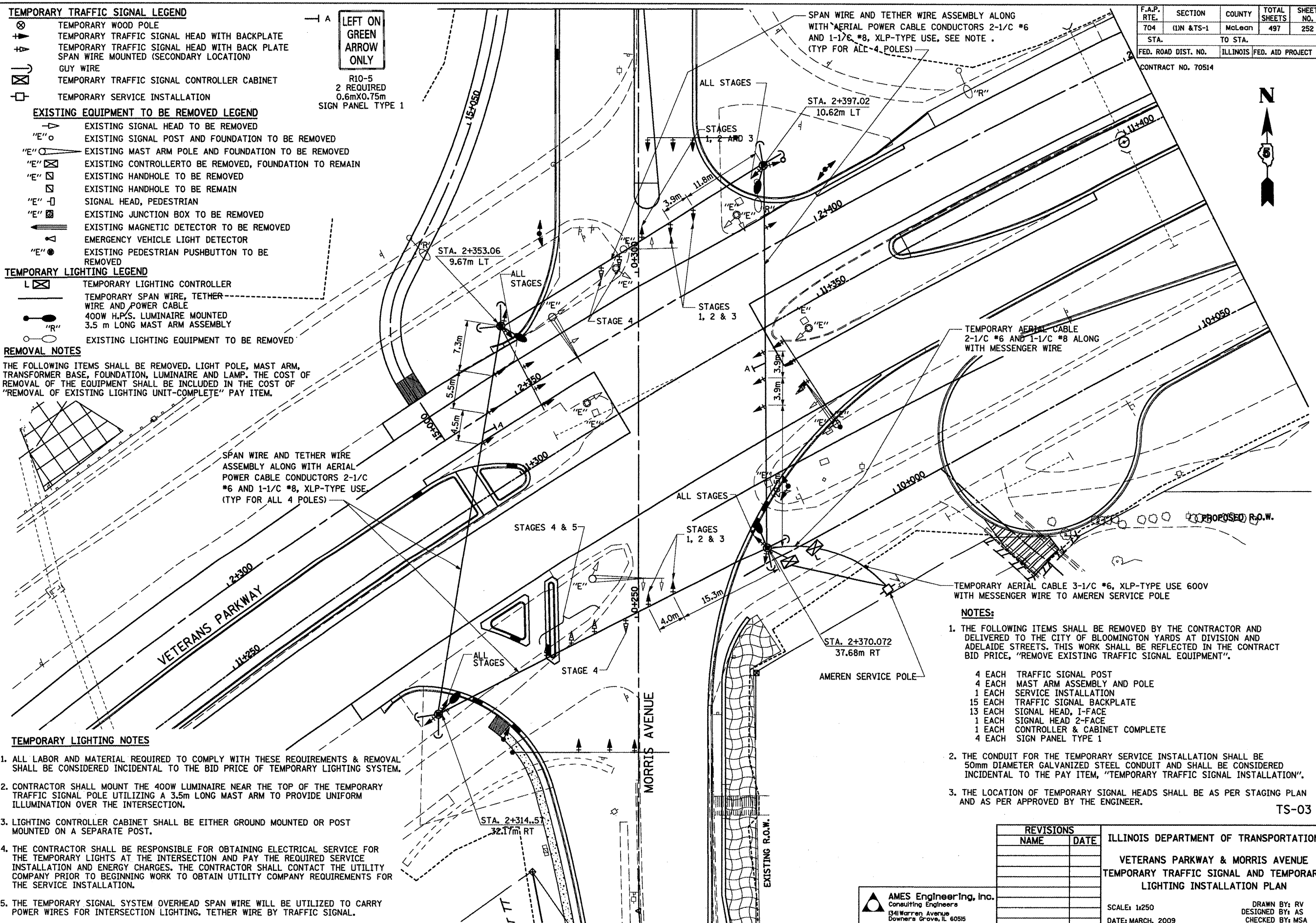
REMOVAL NOTES

THE FOLLOWING ITEMS SHALL BE REMOVED. LIGHT POLE, MAST ARM, TRANSFORMER BASE, FOUNDATION, LUMINAIRE AND LAMP. THE COST OF REMOVAL OF THE EQUIPMENT SHALL BE INCLUDED IN THE COST OF "REMOVAL OF EXISTING LIGHTING UNIT-COMPLETE" PAY ITEM.

TEMPORARY LIGHTING NOTES

1. ALL LABOR AND MATERIAL REQUIRED TO COMPLY WITH THESE REQUIREMENTS & REMOVAL SHALL BE CONSIDERED INCIDENTAL TO THE BID PRICE OF TEMPORARY LIGHTING SYSTEM.
2. CONTRACTOR SHALL MOUNT THE 400W LUMINAIRE NEAR THE TOP OF THE TEMPORARY TRAFFIC SIGNAL POLE UTILIZING A 3.5m LONG MAST ARM TO PROVIDE UNIFORM ILLUMINATION OVER THE INTERSECTION.
3. LIGHTING CONTROLLER CABINET SHALL BE EITHER GROUND MOUNTED OR POST MOUNTED ON A SEPARATE POST.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ELECTRICAL SERVICE FOR THE TEMPORARY LIGHTS AT THE INTERSECTION AND PAY THE REQUIRED SERVICE INSTALLATION AND ENERGY CHARGES. THE CONTRACTOR SHALL CONTACT THE UTILITY COMPANY PRIOR TO BEGINNING WORK TO OBTAIN UTILITY COMPANY REQUIREMENTS FOR THE SERVICE INSTALLATION.
5. THE TEMPORARY SIGNAL SYSTEM OVERHEAD SPAN WIRE WILL BE UTILIZED TO CARRY POWER WIRES FOR INTERSECTION LIGHTING. TETHER WIRE BY TRAFFIC SIGNAL.

← A
LEFT ON GREEN ARROW ONLY
 R10-5
 2 REQUIRED
 0.6mX0.75m
 SIGN PANEL TYPE 1



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|------------|---------------------------|--------------|-----------|
| 704 | (DN & TS-1 | McLean | 497 | 252 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 70514



- NOTES:**
1. THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND DELIVERED TO THE CITY OF BLOOMINGTON YARDS AT DIVISION AND ADELAIDE STREETS. THIS WORK SHALL BE REFLECTED IN THE CONTRACT BID PRICE, "REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT".
 - 4 EACH TRAFFIC SIGNAL POST
 - 4 EACH MAST ARM ASSEMBLY AND POLE
 - 1 EACH SERVICE INSTALLATION
 - 15 EACH TRAFFIC SIGNAL BACKPLATE
 - 13 EACH SIGNAL HEAD, I-FACE
 - 1 EACH SIGNAL HEAD 2-FACE
 - 1 EACH CONTROLLER & CABINET COMPLETE
 - 4 EACH SIGN PANEL TYPE 1
 2. THE CONDUIT FOR THE TEMPORARY SERVICE INSTALLATION SHALL BE 50mm DIAMETER GALVANIZED STEEL CONDUIT AND SHALL BE CONSIDERED INCIDENTAL TO THE PAY ITEM, "TEMPORARY TRAFFIC SIGNAL INSTALLATION".
 3. THE LOCATION OF TEMPORARY SIGNAL HEADS SHALL BE AS PER STAGING PLAN AND AS PER APPROVED BY THE ENGINEER.

TS-03

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|--|
| NAME | DATE | |
| | | VETERANS PARKWAY & MORRIS AVENUE TEMPORARY TRAFFIC SIGNAL AND TEMPORARY LIGHTING INSTALLATION PLAN |
| | | |
| | | |
| | | |
| | | |

AMES Engineering, Inc.
 Consulting Engineers
 1341 Warren Avenue
 Downers Grove, IL 60515

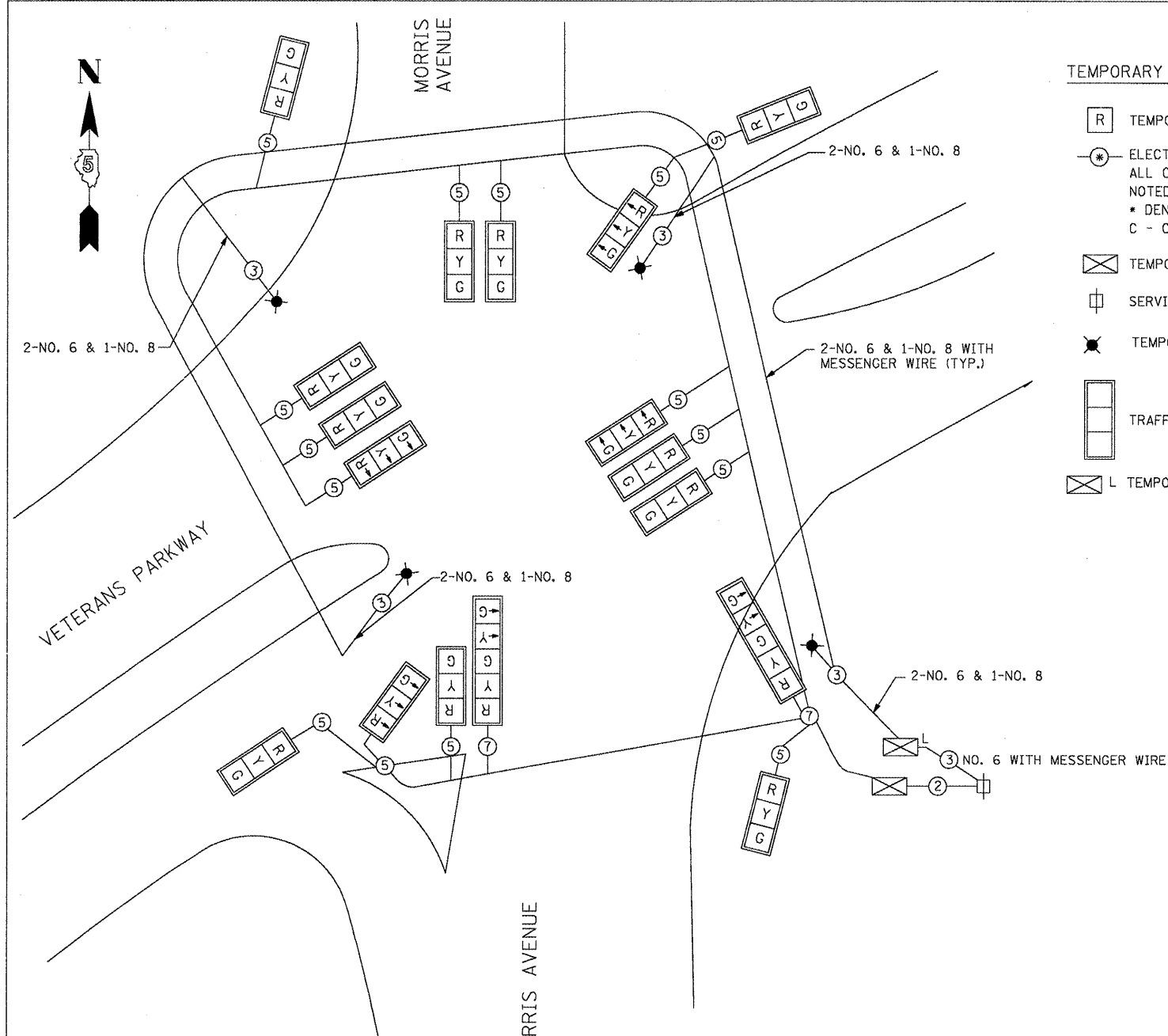
SCALE: 1/250
 DATE: MARCH, 2009

DRAWN BY: RV
 DESIGNED BY: AS
 CHECKED BY: MSA

FILE# 10/14/2010 \$TIME\$

| | | | | |
|---------------------|-------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1)N & TS-1 | McLean | 497 | 253 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 70514
OF ALL UNDERGROUND UTILITIES PRIOR TO BEGINNING CONSTRUCTION (J.U.L.I.E. 800-892-0123).



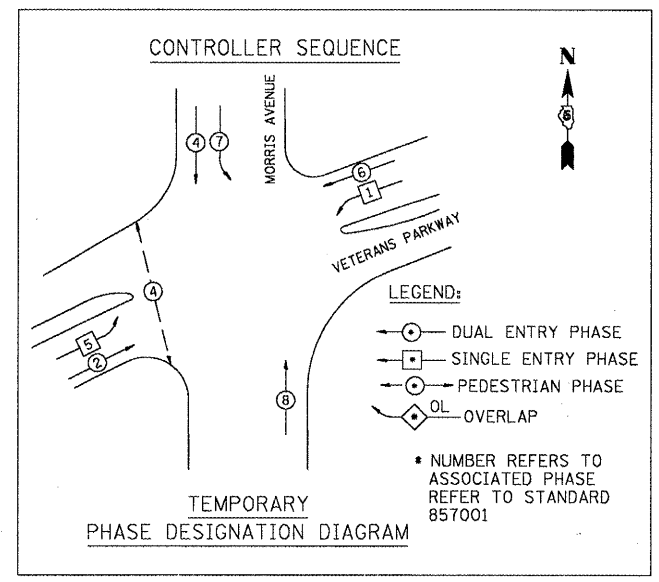
TEMPORARY CABLE PLAN LEGEND:

- [R] TEMPORARY TRAFFIC SIGNAL SECTION
- ELECTRICAL CABLE AERIAL SUSPENDED. ALL CABLE IS NO. 14 AWG EXCEPT AS NOTED.
* DENOTES NUMBER OF CONDUCTORS, C - COAXIAL CABLE
- ⊠ TEMPORARY CONTROLLER CABINET
- SERVICE INSTALLATION
- ⊛ TEMPORARY 400W HPS LUMINAIRE
- ▭ TRAFFIC SIGNAL BACKPLATE
- ⊠ L TEMPORARY LIGHTING CONTROLLER

GENERAL NOTES:

1. THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION OF ALL UNDERGROUND UTILITIES PRIOR TO BEGINNING CONSTRUCTION (J.U.L.I.E. 800-892-0123).
2. ALL TRAFFIC SIGNAL HEADS SHALL BE 300MM (12").
3. ALL WOOD SUPPORT POLES SHALL BE INSTALLED SO THAT A MINIMUM OF 10.7 METERS (35 FEET) OF POLE IS ABOVE THE EXISTING PAVEMENT ELEVATION ADJACENT TO THE POLE. A SUFFICIENT LENGTH OF POLE SHALL BE BURIED AND BACK GUYED TO ALLOW THE INSTALLATION TO WITHSTAND 80 K.P.H. SUSTAINED WIND LOADING.
4. THE PROPOSED TRAFFIC SIGNAL CONTROL CABINET SHALL BE FURNISHED WITH A MANUAL CONTROL SWITCH AND MANUAL CONTROL CORD WITHIN THE POLICE DOOR COMPARTMENT AS INCIDENTAL TO THE TEMPORARY TRAFFIC SIGNAL INSTALLATION PAY ITEM.
5. THE CONTRACTOR SHALL ARRANGE FOR A FACTORY OR SUPPLIER REPRESENTATIVE TO BE PRESENT AT THE INTERSECTION WHEN THE CONTROLLER PLACED IN OPERATION AS INCIDENTAL TO THE TEMPORARY TRAFFIC SIGNAL INSTALLATION PAY ITEM. CONTRACT SHALL MAKE CERTAIN THAT THE CONTROL EQUIPMENT ARE WORKING TO THE SATISFACTION OF THE ENGINEER.
6. THE ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SIGNAL SYSTEM ENGINEER (217-466-7383) SHALL BE NOTIFIED AT LEAST 72 HOURS PRIOR TO PLACING THE SIGNALS IN OPERATION.
7. ALL SPAN WIRE MOUNTED SIGNAL HEADS ON AN INDIVIDUAL SPAN WIRE SHALL BE MOUNTED SO THAT THE RED INDICATIONS ARE LEVEL WITH EACH OTHER.
8. THE ELECTRICAL CONDUCTORS FOR ALL TRAFFIC SIGNAL HEADS SHALL BE SOLID, SOFT COPPER.
9. THE CONTRACTOR IS RESPONSIBLE FOR THE COST OF UNCOVERING OR HAND DIGGING AROUND UTILITIES AS NECESSARY, INCIDENTAL TO THE TEMPORARY TRAFFIC SIGNAL INSTALLATION PAY ITEM.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ELECTRICAL SERVICE FOR THE TEMPORARY TRAFFIC SIGNALS CONTRACTOR SHALL CONTACT AMEREN (ILLINOIS POWER CO.), PRIOR TO BEGINNING WORK TO OBTAIN THE UTILITY COMPANY REQUIREMENTS FOR THE SERVICE INSTALLATION AND TO ARRANGE FOR ELECTRICAL SERVICE INSTALLATION.
11. IT IS ANTICIPATED THAT THE TEMPORARY CONTROLLER WILL BE LOCATED IN THE SOUTHEAST QUADRANT. A CABINET OF SUFFICIENT SIZE SHALL BE INSTALLED BY THE CONTRACTOR AT A LOCATION ACCEPTABLE TO THE ENGINEER.
12. THE CONTRACTOR SHALL FURNISH SUFFICIENT CABLE SLACK TO ALLOW REPOSITIONING OF EACH SIGNAL HEAD BETWEEN CONSTRUCTION PHASES AND TO ANY POSITION ON THE SPAN WIRE.
13. RELOCATE EXISTING STREET NAME SIGNS TO TEMPORARY SPAN WIRE POLES AS DIRECTED BY THE ENGINEER.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF THE TEMPORARY TRAFFIC SIGNAL INSTALLATIONS FOR THE DURATION OF THE SIGNAL WORK.
15. TEMPORARY TRAFFIC SIGNAL HEADS SHALL BE PROPERLY COVERED PRIOR TO TURN-ON AS DIRECTED BY THE ENGINEER. THE COST SHALL BE INCLUDED WITH THE COST OF THE TEMPORARY SIGNAL INSTALLATION PAY ITEM.
16. TEMPORARY SIGNALS TO BE IN OPERATION BEFORE EXISTING SIGNALS ARE REMOVED.

TEMPORARY CABLE PLAN
NOT TO SCALE



| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
VETERANS PARKWAY & MORRIS AVENUE
TEMPORARY PHASE DESIGNATION DIAGRAM
AND TEMPORARY CABLE PLAN

SCALE:
DATE: MARCH, 2009

DRAWN BY: RV
DESIGNED BY: AS
CHECKED BY: MSA

AMES Engineering, Inc.
Consulting Engineers
1341 Warren Avenue
Downers Grove, IL 60515

\$FILE\$ 10/14/2010 \$TIME\$

TS-04

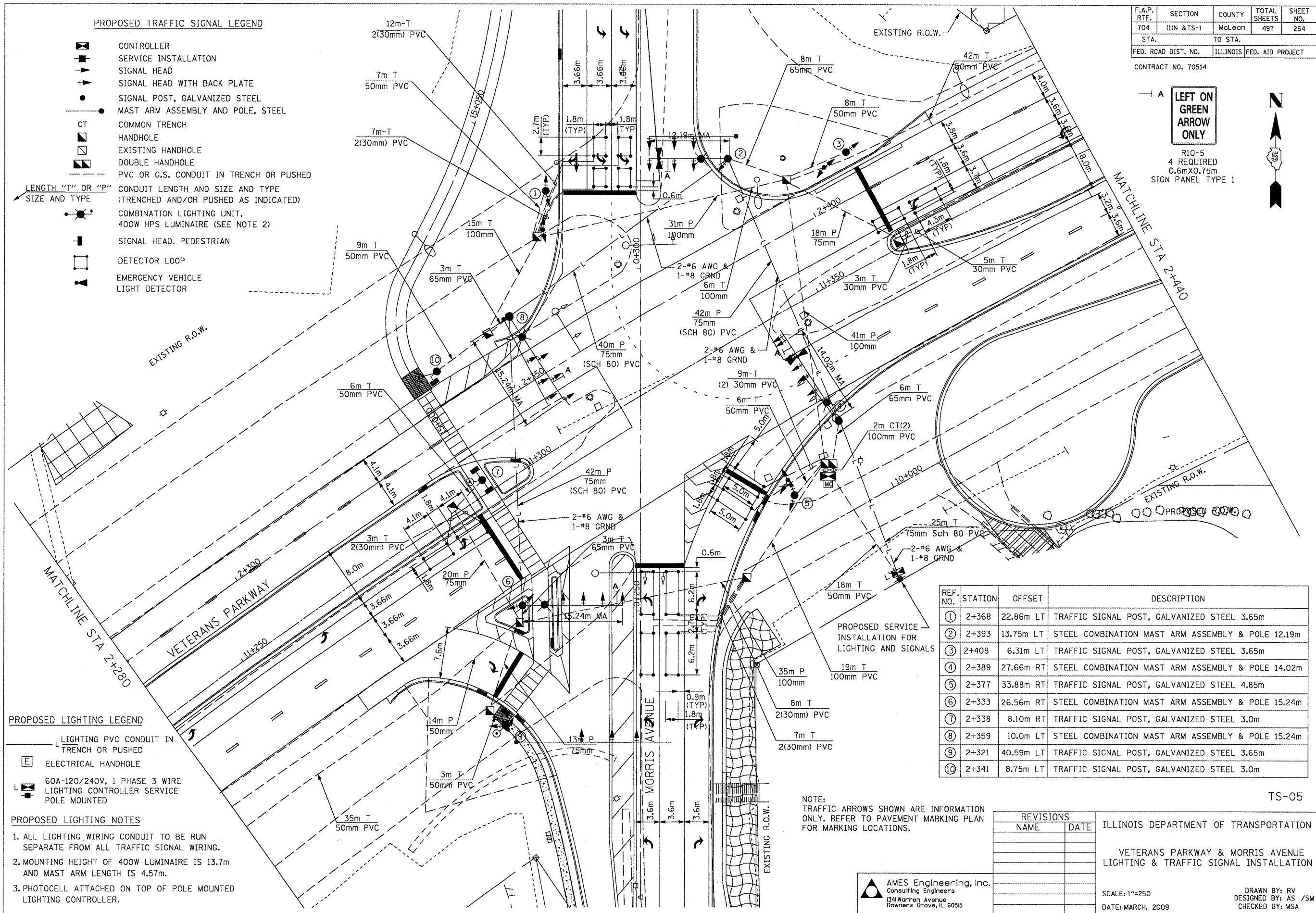
PROPOSED TRAFFIC SIGNAL LEGEND

- CONTROLLER
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD WITH BACK PLATE
- SIGNAL POST, GALVANIZED STEEL
- MAST ARM ASSEMBLY AND POLE, STEEL
- COMMON TRENCH
- HANDHOLE
- EXISTING HANDHOLE
- DOUBLE HANDHOLE
- PVC OR G.S. CONDUIT IN TRENCH OR PUSHED
- LENGTH "T" OR "P" SIZE AND TYPE
- CONDUIT LENGTH AND SIZE AND TYPE (TRENCHED AND/OR PUSHED AS INDICATED)
- COMBINATION LIGHTING UNIT, 400W HPS LUMINAIRE (SEE NOTE 2)
- SIGNAL HEAD, PEDESTRIAN
- DETECTOR LOOP
- EMERGENCY VEHICLE LIGHT DETECTOR

| | | | | |
|---------------------|-------------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1)N & TS-1 | McLean | 497 | 254 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |

LEFT ON GREEN ARROW ONLY

R10-5
4 REQUIRED
0.6mX0.75m
SIGN PANEL TYPE 1



PROPOSED LIGHTING LEGEND

- LIGHTING PVC CONDUIT IN TRENCH OR PUSHED
- ELECTRICAL HANDHOLE
- 60A-120/240V, 1 PHASE 3 WIRE LIGHTING CONTROLLER SERVICE POLE MOUNTED

PROPOSED LIGHTING NOTES

1. ALL LIGHTING WIRING CONDUIT TO BE RUN SEPARATE FROM ALL TRAFFIC SIGNAL WIRING.
2. MOUNTING HEIGHT OF 400W LUMINAIRE IS 13.7m AND MAST ARM LENGTH IS 4.57m.
3. PHOTOCELL ATTACHED ON TOP OF POLE MOUNTED LIGHTING CONTROLLER.

| REF. NO. | STATION | OFFSET | DESCRIPTION |
|----------|---------|-----------|---|
| 1 | 2+368 | 22.86m LT | TRAFFIC SIGNAL POST, GALVANIZED STEEL 3.65m |
| 2 | 2+393 | 13.75m LT | STEEL COMBINATION MAST ARM ASSEMBLY & POLE 12.19m |
| 3 | 2+408 | 6.31m LT | TRAFFIC SIGNAL POST, GALVANIZED STEEL 3.65m |
| 4 | 2+389 | 27.66m RT | STEEL COMBINATION MAST ARM ASSEMBLY & POLE 14.02m |
| 5 | 2+377 | 33.88m RT | TRAFFIC SIGNAL POST, GALVANIZED STEEL 4.85m |
| 6 | 2+333 | 26.56m RT | STEEL COMBINATION MAST ARM ASSEMBLY & POLE 15.24m |
| 7 | 2+338 | 8.10m RT | TRAFFIC SIGNAL POST, GALVANIZED STEEL 3.0m |
| 8 | 2+359 | 10.0m LT | STEEL COMBINATION MAST ARM ASSEMBLY & POLE 15.24m |
| 9 | 2+321 | 40.59m LT | TRAFFIC SIGNAL POST, GALVANIZED STEEL 3.65m |
| 10 | 2+341 | 8.75m LT | TRAFFIC SIGNAL POST, GALVANIZED STEEL 3.0m |

NOTE: TRAFFIC ARROWS SHOWN ARE INFORMATION ONLY. REFER TO PAVEMENT MARKING PLAN FOR MARKING LOCATIONS.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
VETERANS PARKWAY & MORRIS AVENUE
LIGHTING & TRAFFIC SIGNAL INSTALLATION

SCALE: 1"=250
DATE: MARCH, 2009

















DRAWN BY: RV
DESIGNED BY: AS /SM
CHECKED BY: MSA

AMES Engineering, Inc.
Consulting Engineers
1341 Warren Avenue
Downers Grove, IL 60515

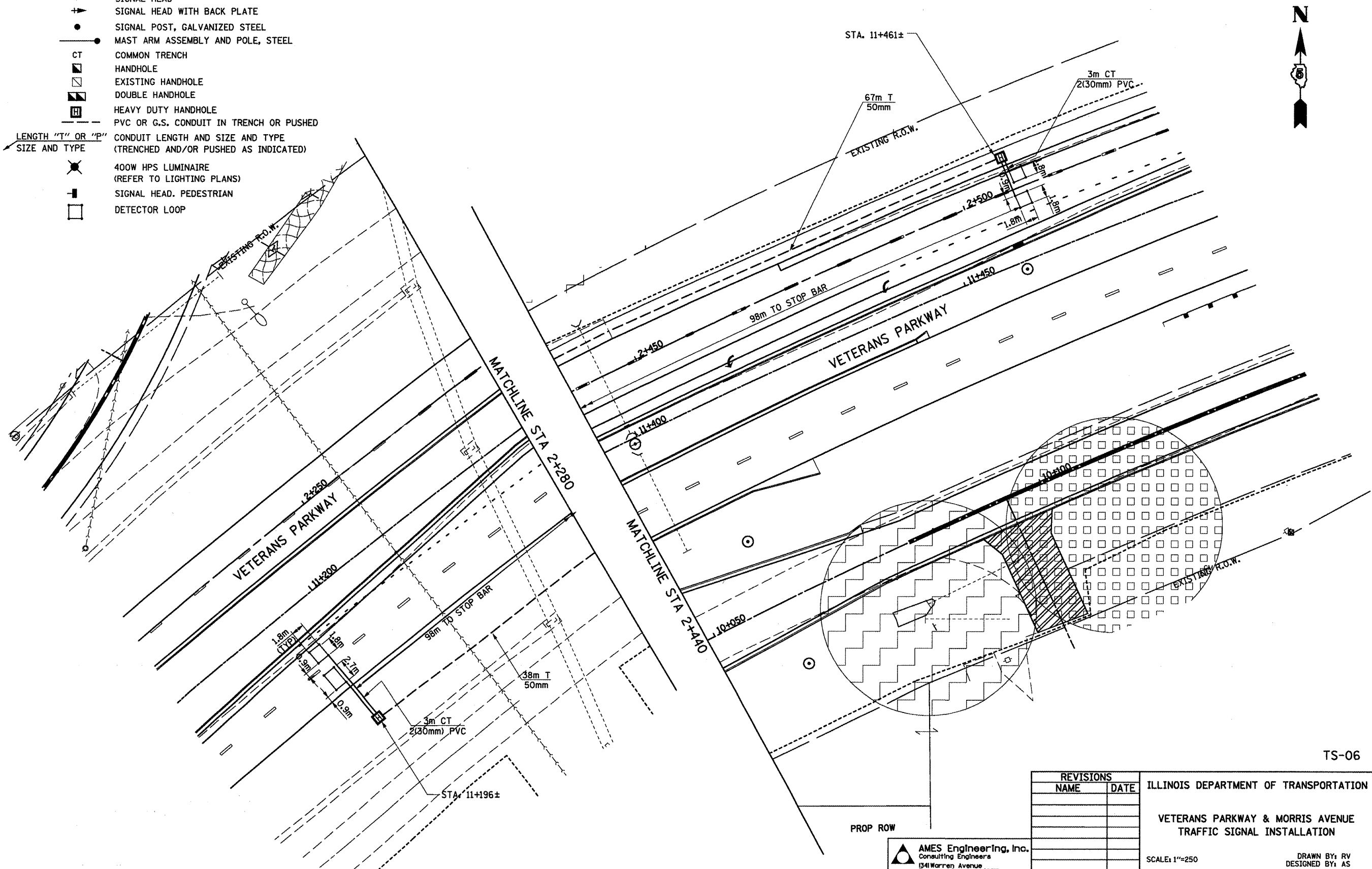
FILES 10/14/2010 \$TIME\$

| | | | | |
|---------------------|------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | 11N & TS-1 | McLean | 497 | 255 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |

PROPOSED TRAFFIC SIGNAL LEGEND

-  CONTROLLER LOCATED ON EXISTING FOUNDATION
-  SERVICE INSTALLATION
-  SIGNAL HEAD
-  SIGNAL HEAD WITH BACK PLATE
-  SIGNAL POST, GALVANIZED STEEL
-  MAST ARM ASSEMBLY AND POLE, STEEL
-  COMMON TRENCH
-  HANDHOLE
-  EXISTING HANDHOLE
-  DOUBLE HANDHOLE
-  HEAVY DUTY HANDHOLE
-  PVC OR G.S. CONDUIT IN TRENCH OR PUSHED
-  CONDUIT LENGTH AND SIZE AND TYPE (TRENCHED AND/OR PUSHED AS INDICATED)
-  400W HPS LUMINAIRE (REFER TO LIGHTING PLANS)
-  SIGNAL HEAD, PEDESTRIAN
-  DETECTOR LOOP

LENGTH "T" OR "P" SIZE AND TYPE



TS-06

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
**VETERANS PARKWAY & MORRIS AVENUE
TRAFFIC SIGNAL INSTALLATION**
SCALE: 1"=250
DATE: MARCH, 2009
DRAWN BY: RV
DESIGNED BY: AS
CHECKED BY: MSA

AMES Engineering, Inc.
Consulting Engineers
1341 Warren Avenue
Downers Grove, IL 60515

\$FILES 10/14/2010 \$TIME\$

TEMPORARY TRAFFIC SIGNAL LEGEND

- ⊗ TEMPORARY WOOD POLE
- TEMPORARY TRAFFIC SIGNAL HEAD WITH BACKPLATE
- ↪ GUY WIRE
- ⊠ TEMPORARY CONTROLLER CABINET
- TEMPORARY SERVICE INSTALLATION

EXISTING EQUIPMENT TO BE REMOVED LEGEND

- ⊠ EXISTING SIGNAL HEAD TO BE REMOVED
- ⊠ EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED
- ⊠ EXISTING MAST ARM POLE AND FOUNDATION TO BE REMOVED
- ⊠ EXISTING CONTROLLER TO BE REMOVED, FOUNDATION TO REMAIN
- ⊠ EXISTING HANDHOLE TO BE REMOVED
- ⊠ EXISTING HANDHOLE TO BE REMAIN
- ⊠ SIGNAL HEAD, PEDESTRIAN
- ⊠ EXISTING JUNCTION BOX TO BE REMOVED
- ⊠ EXISTING MAGNETIC DETECTOR TO BE REMOVED
- ⊠ EMERGENCY VEHICLE LIGHT DETECTOR
- ⊠ EXISTING PEDESTRIAN PUSHBUTTON TO BE REMOVED

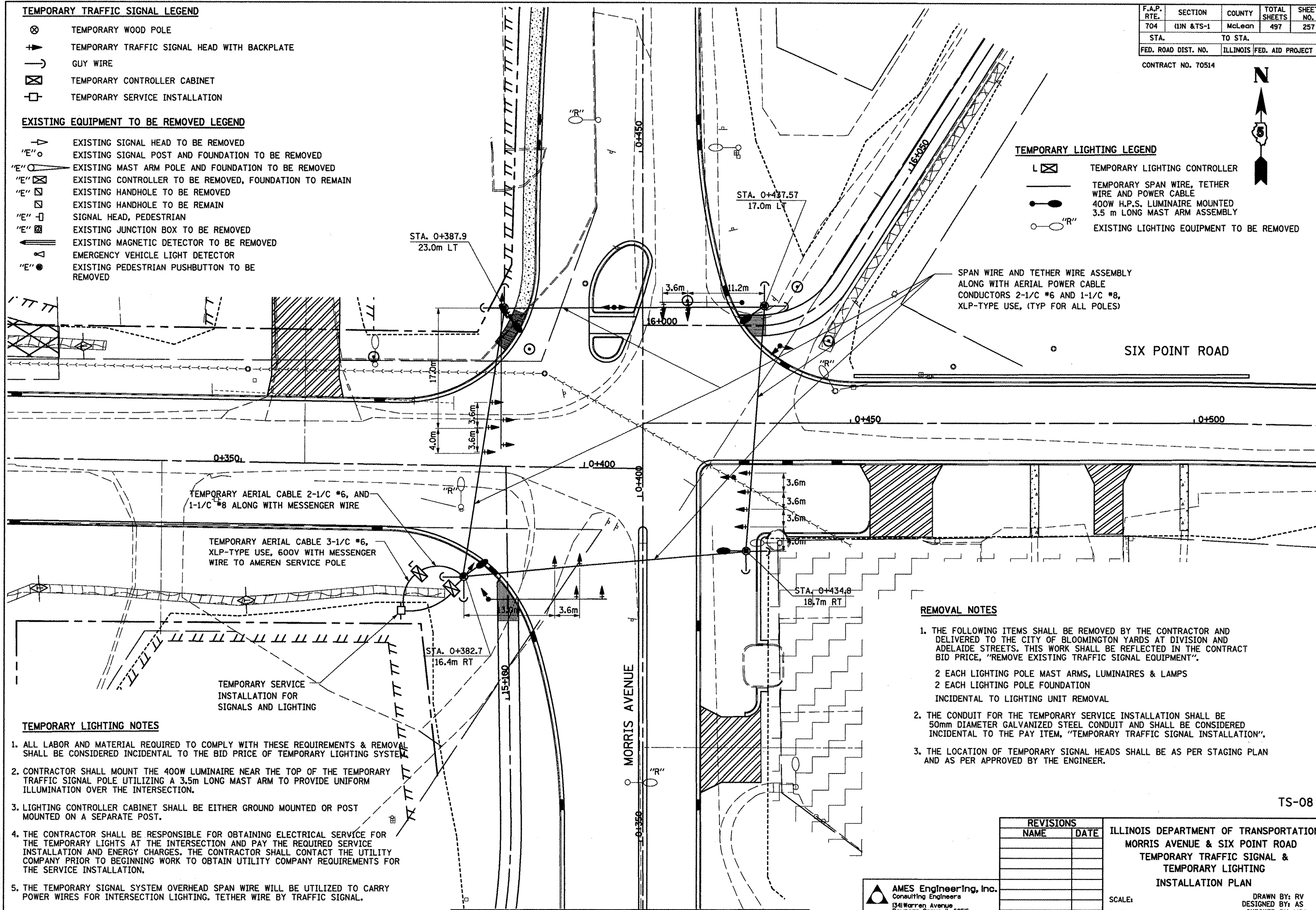
| | | | | |
|---------------------|---------------------------|--------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (11N & TS-1) | McLean | 497 | 257 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |

CONTRACT NO. 70514



TEMPORARY LIGHTING LEGEND

- ⊠ TEMPORARY LIGHTING CONTROLLER
- TEMPORARY SPAN WIRE, TETHER WIRE AND POWER CABLE
- 400W H.P.S. LUMINAIRE MOUNTED 3.5 m LONG MAST ARM ASSEMBLY
- "R" EXISTING LIGHTING EQUIPMENT TO BE REMOVED



SPAN WIRE AND TETHER WIRE ASSEMBLY ALONG WITH AERIAL POWER CABLE CONDUCTORS 2-1/C #6 AND 1-1/C #8, XLP-TYPE USE, (TYP FOR ALL POLES)

TEMPORARY AERIAL CABLE 2-1/C #6, AND 1-1/C #8 ALONG WITH MESSENGER WIRE

TEMPORARY AERIAL CABLE 3-1/C #6, XLP-TYPE USE, 600V WITH MESSENGER WIRE TO AMEREN SERVICE POLE

TEMPORARY SERVICE INSTALLATION FOR SIGNALS AND LIGHTING

REMOVAL NOTES

1. THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND DELIVERED TO THE CITY OF BLOOMINGTON YARDS AT DIVISION AND ADELAIDE STREETS. THIS WORK SHALL BE REFLECTED IN THE CONTRACT BID PRICE, "REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT".
- 2 EACH LIGHTING POLE MAST ARMS, LUMINAIRES & LAMPS
2 EACH LIGHTING POLE FOUNDATION
INCIDENTAL TO LIGHTING UNIT REMOVAL
2. THE CONDUIT FOR THE TEMPORARY SERVICE INSTALLATION SHALL BE 50mm DIAMETER GALVANIZED STEEL CONDUIT AND SHALL BE CONSIDERED INCIDENTAL TO THE PAY ITEM, "TEMPORARY TRAFFIC SIGNAL INSTALLATION".
3. THE LOCATION OF TEMPORARY SIGNAL HEADS SHALL BE AS PER STAGING PLAN AND AS PER APPROVED BY THE ENGINEER.

TEMPORARY LIGHTING NOTES

1. ALL LABOR AND MATERIAL REQUIRED TO COMPLY WITH THESE REQUIREMENTS & REMOVAL SHALL BE CONSIDERED INCIDENTAL TO THE BID PRICE OF TEMPORARY LIGHTING SYSTEM.
2. CONTRACTOR SHALL MOUNT THE 400W LUMINAIRE NEAR THE TOP OF THE TEMPORARY TRAFFIC SIGNAL POLE UTILIZING A 3.5m LONG MAST ARM TO PROVIDE UNIFORM ILLUMINATION OVER THE INTERSECTION.
3. LIGHTING CONTROLLER CABINET SHALL BE EITHER GROUND MOUNTED OR POST MOUNTED ON A SEPARATE POST.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ELECTRICAL SERVICE FOR THE TEMPORARY LIGHTS AT THE INTERSECTION AND PAY THE REQUIRED SERVICE INSTALLATION AND ENERGY CHARGES. THE CONTRACTOR SHALL CONTACT THE UTILITY COMPANY PRIOR TO BEGINNING WORK TO OBTAIN UTILITY COMPANY REQUIREMENTS FOR THE SERVICE INSTALLATION.
5. THE TEMPORARY SIGNAL SYSTEM OVERHEAD SPAN WIRE WILL BE UTILIZED TO CARRY POWER WIRES FOR INTERSECTION LIGHTING. TETHER WIRE BY TRAFFIC SIGNAL.

\$FILE\$ 10/14/2010 \$TIME\$

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |

AMES Engineering, Inc.
Consulting Engineers
1341 Warren Avenue
Downers Grove, IL 60515

ILLINOIS DEPARTMENT OF TRANSPORTATION
MORRIS AVENUE & SIX POINT ROAD
TEMPORARY TRAFFIC SIGNAL &
TEMPORARY LIGHTING
INSTALLATION PLAN

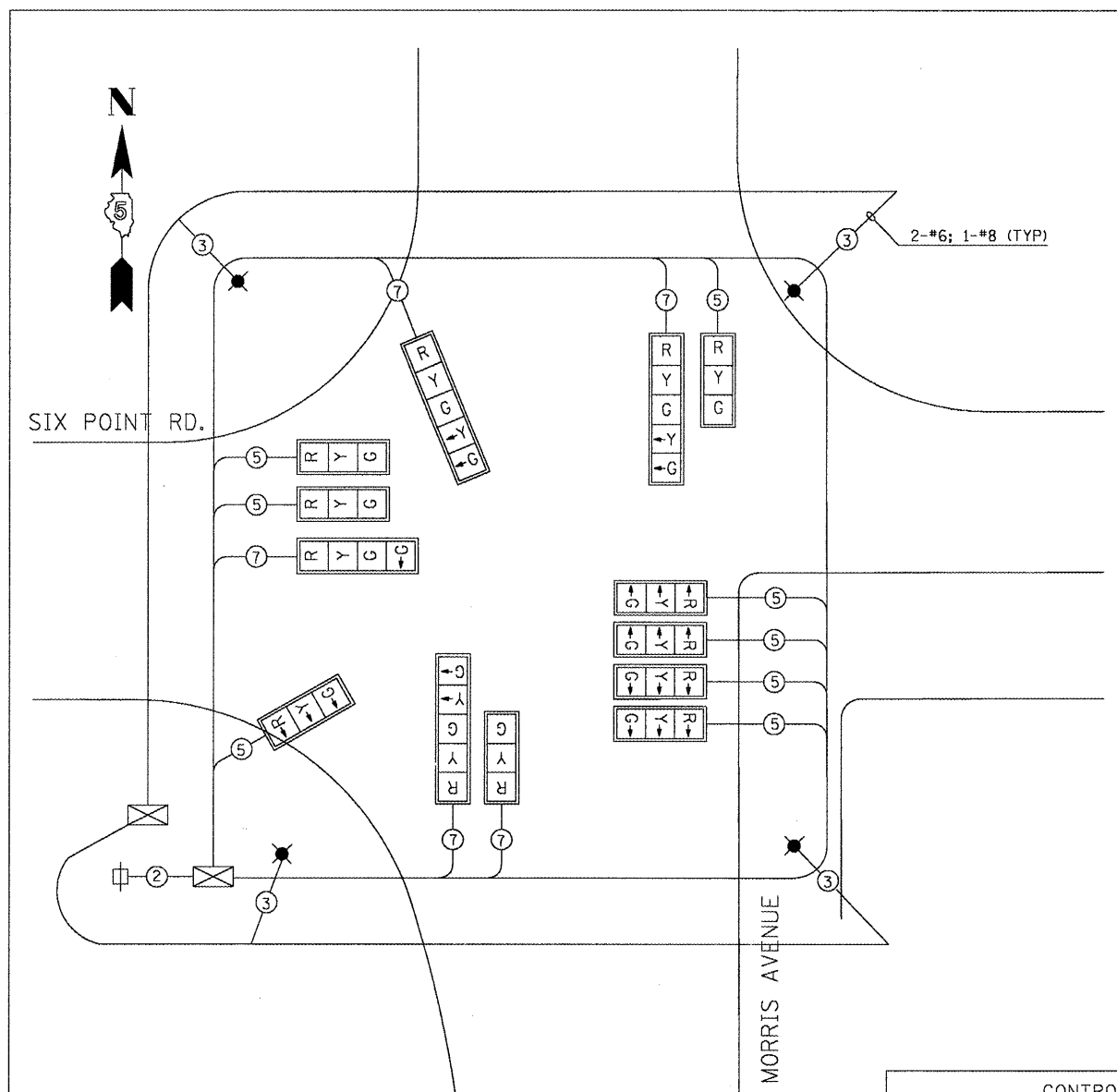
SCALE: _____
DATE: MARCH, 2009

DRAWN BY: RV
DESIGNED BY: AS
CHECKED BY: MSA

TS-08

| | | | | |
|---------------------|-------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1)N & TS-1 | McLean | 497 | 258 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 70514
 THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION OF ALL UNDERGROUND UTILITIES PRIOR TO BEGINNING CONSTRUCTION (J.U.L.I.E. 800-892-0123).



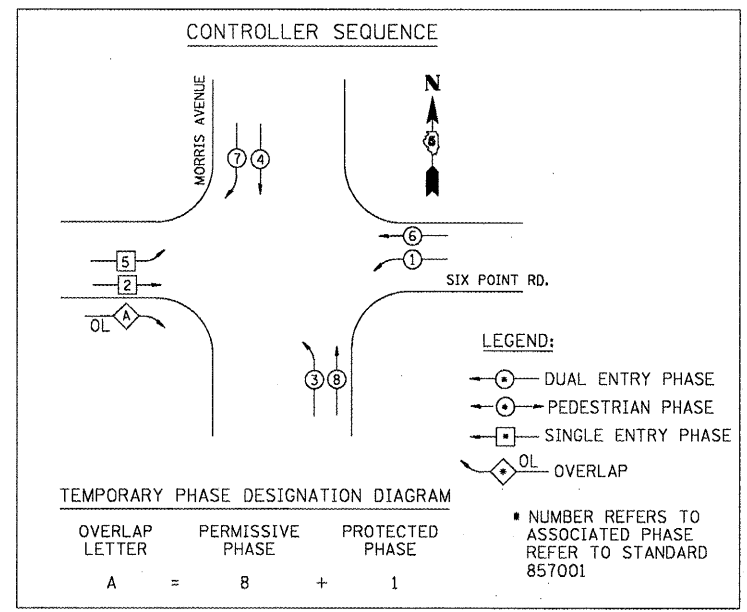
TEMPORARY CABLE PLAN
 NOT TO SCALE

TEMPORARY CABLE PLAN LEGEND:

- [R] TEMPORARY TRAFFIC SIGNAL SECTION
- ⑤ ELECTRICAL CABLE AERIAL SUSPENDED. ALL CABLE IS NO. 14 AWG EXCEPT AS NOTED.
 - DENOTES NUMBER OF CONDUCTORS, C - COAXIAL CABLE
- ⊠ TEMPORARY CONTROLLER CABINET
- ⊞ SERVICE INSTALLATION
- 📹 WIDE AREA VIDEO VEHICLE DETECTION SYSTEM CAMERA
- ☼ TEMPORARY 400W HPS LUMINAIRE
- [R
Y
G] TRAFFIC SIGNAL BACKPLATE

GENERAL NOTES:

1. THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION OF ALL UNDERGROUND UTILITIES PRIOR TO BEGINNING CONSTRUCTION (J.U.L.I.E. 800-892-0123).
2. ALL TRAFFIC SIGNAL HEADS SHALL BE 300MM (12").
3. ALL WOOD SUPPORT POLES SHALL BE INSTALLED SO THAT A MINIMUM OF 10.7 METERS (35 FEET) OF POLE IS ABOVE THE EXISTING PAVEMENT ELEVATION ADJACENT TO THE POLE. A SUFFICIENT LENGTH OF POLE SHALL BE BURIED AND BACK GUYED TO ALLOW THE INSTALLATION TO WITHSTAND 80 K.P.H. SUSTAINED WIND LOADING.
4. THE PROPOSED TRAFFIC SIGNAL CONTROL CABINET SHALL BE FURNISHED WITH A MANUAL CONTROL SWITCH AND MANUAL CONTROL CORD WITHIN THE POLICE DOOR COMPARTMENT AS INCIDENTAL TO THE TEMPORARY TRAFFIC SIGNAL INSTALLATION PAY ITEM.
5. THE CONTRACTOR SHALL ARRANGE FOR A FACTORY OR SUPPLIER REPRESENTATIVE TO BE PRESENT AT THE INTERSECTION WHEN THE CONTROLLER AND VIDEO DETECTION SYSTEM IS PLACED IN OPERATION AS INCIDENTAL TO THE TEMPORARY TRAFFIC SIGNAL INSTALLATION PAY ITEM, CONTRACT SHALL MAKE CERTAIN THAT THE CONTROL EQUIPMENT AND VIDEO DETECTION SYSTEM ARE WORKING TO THE SATISFACTION OF THE ENGINEER.
6. THE ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SIGNAL SYSTEMS ENGINEER (217) 466-7383 SHALL BE NOTIFIED AT LEAST 72 HOURS PRIOR TO PLACING THE SIGNALS IN OPERATION.
7. ALL SPAN WIRE MOUNTED SIGNAL HEADS ON AN INDIVIDUAL SPAN WIRE SHALL BE MOUNTED SO THAT THE RED INDICATIONS ARE LEVEL WITH EACH OTHER.
8. THE ELECTRICAL CONDUCTORS FOR ALL TRAFFIC SIGNAL HEADS SHALL BE SOLID, SOFT COPPER.
9. THE CONTRACTOR IS RESPONSIBLE FOR THE COST OF UNCOVERING OR HAND DIGGING AROUND UTILITIES AS NECESSARY, INCIDENTAL TO THE TEMPORARY TRAFFIC SIGNAL INSTALLATION PAY ITEM.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ELECTRICAL SERVICE FOR THE TEMPORARY TRAFFIC SIGNALS. THE CONTRACTOR SHALL CONTACT AMEREN (ILLINOIS POWER CO.) PRIOR TO BEGINNING WORK TO OBTAIN THE UTILITY COMPANY REQUIREMENTS FOR THE SERVICE INSTALLATION AND TO ARRANGE FOR ELECTRICAL SERVICE INSTALLATION.
11. IT IS ANTICIPATED THAT THE TEMPORARY CONTROLLER WILL BE LOCATED IN THE SOUTHWEST QUADRANT. A CABINET OF SUFFICIENT SIZE SHALL BE INSTALLED BY THE CONTRACTOR AT A LOCATION ACCEPTABLE TO THE ENGINEER.
12. A MINIMUM 6 METERS OF CABLE SLACK SHALL BE PROVIDED TO ALLOW REPOSITIONING OF EACH SIGNAL HEAD BETWEEN CONSTRUCTION PHASES.
13. THE CONTRACTOR SHALL ASSURE THAT THE TEMPORARY CONTROLLER AND CABINET ARE COMPATIBLE WITH ALL WIDE AREA VIDEO VEHICLE DETECTION SYSTEM EQUIPMENT.
14. RELOCATE EXISTING STREET NAME SIGNS TO TEMPORARY SPAN WIRE POLES AS DIRECTED BY THE ENGINEER.
15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF THE TEMPORARY TRAFFIC SIGNAL INSTALLATIONS FOR THE DURATION OF THE SIGNAL WORK.
16. TEMPORARY TRAFFIC SIGNAL HEADS SHALL BE PROPERLY COVERED PRIOR TO TURN-ON AS DIRECTED BY THE ENGINEER. THE COST SHALL BE INCLUDED WITH THE COST OF THE TEMPORARY SIGNAL INSTALLATION PAY ITEM.
17. TEMPORARY SIGNALS TO BE IN OPERATION BEFORE EXISTING SIGNALS ARE REMOVED.



\$FILE\$ 10/14/2010 \$TIME\$

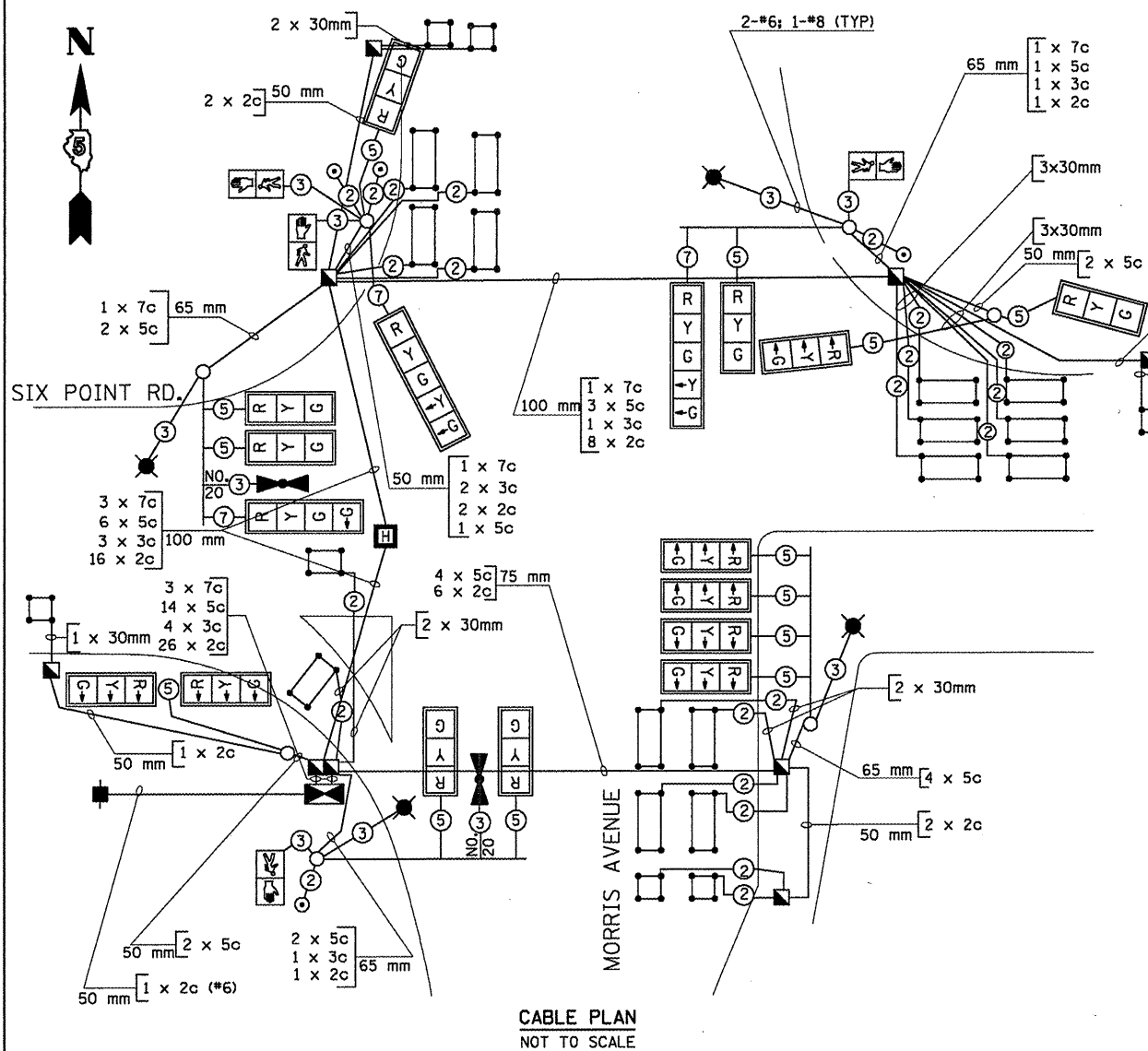
AMES Engineering, Inc.
 Consulting Engineers
 1341 Warren Avenue
 Downers Grove, IL 60515

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
 MORRIS AVENUE & SIX POINT ROAD
 TEMPORARY WIRING PLAN
 TEMPORARY PHASE DESIGNATION DIAGRAM
 SCALE: _____
 DATE: MARCH, 2009
 DRAWN BY: RV
 DESIGNED BY: AS
 CHECKED BY: MSA

TS-09

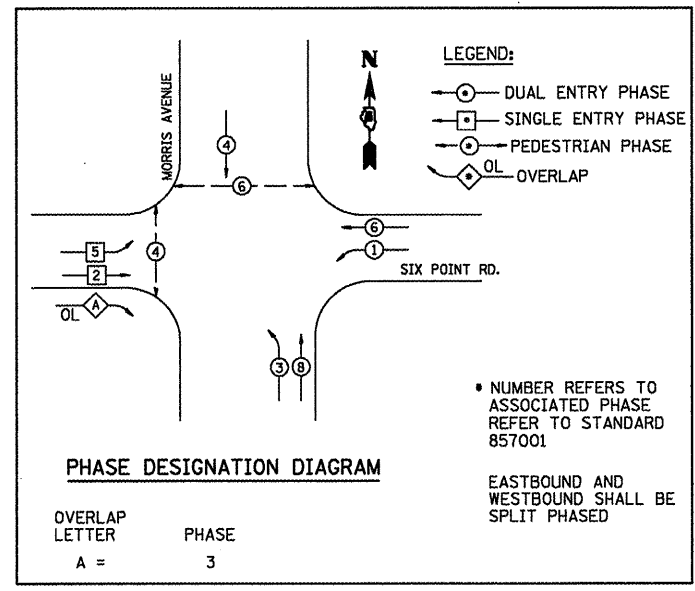
| | | | | |
|---------------------|-------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (LN & TS-1) | McLean | 497 | 260 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |



- CABLE PLAN LEGEND:**
- SERVICE INSTALLATION
 - ⊠ CONTROLLER
 - ⊡ HANDHOLE
 - ⊞ HEAVY DUTY HANDHOLE
 - ⊞ DOUBLE HANDHOLE
 - MAST ARM OR SIGNAL POST
 - ⊞ YELLOW LEFT TURN ARROW
 - ⊞ RED BALL SECTION
 - ⊞ SIGNAL HEAD BACKPLATE
 - ⑤ NUMBER OF CONDUCTORS IN CABLE
- CONDUIT SIZE
- 50 mm 3 x 2c
 - 50 mm 1 x 3c
- NUMBER OF CABLES IN CONDUIT
- ⑤ NUMBER OF CONDUCTORS IN EACH CABLE
- ⊞ 12" (300mm) PEDESTRIAN SIGNAL SECTION
 - ⊞ PEDESTRIAN PUSHBUTTON
 - ⊞ DETECTOR LOOP
 - ⊞ EMERGENCY VEHICLE LIGHT DETECTOR

BILL OF MATERIALS

| ITEM | UNIT | QNTY. |
|---|-------|-------|
| HANDHOLE | EACH | 7 |
| HEAVY-DUTY HANDHOLE | EACH | 1 |
| DOUBLE HANDHOLE | EACH | 1 |
| FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL | EACH | 1 |
| TRANSCEIVER - FIBER OPTIC | EACH | 1 |
| TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC | EACH | 17 |
| INDUCTIVE LOOP DETECTOR | EACH | 22 |
| PEDESTRIAN PUSH-BUTTON | EACH | 4 |
| TEMPORARY TRAFFIC SIGNAL INSTALLATION | EACH | 1 |
| REMOVE EXISTING HANDHOLE | EACH | 4 |
| REMOVE EXISTING CONCRETE FOUNDATION | EACH | 4 |
| REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT | EACH | 0 |
| SIGN PANEL TYPE I | SQ M | 1 |
| SIGN PANEL TYPE II | SQ M | 4 |
| CONDUIT IN TRENCH, 50MM DIA., PVC | METER | 224 |
| CONDUIT IN TRENCH, 65MM DIA., PVC | METER | 26 |
| CONDUIT IN TRENCH, 100MM DIA., PVC | METER | 14 |
| CONDUIT PUSHED, 50MM DIA., GALVANIZED STEEL | METER | 18 |
| CONDUIT PUSHED, 75MM DIA., GALVANIZED STEEL | METER | 35 |
| CONDUIT PUSHED, 100MM DIA., GALVANIZED STEEL | METER | 69 |
| TRENCH AND BACKFILL FOR ELECTRICAL WORK | METER | 321 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C | METER | 1985 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C | METER | 274 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | METER | 958 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | METER | 285 |
| ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR | METER | 342 |
| ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C | METER | 12 |
| TRAFFIC SIGNAL POST, GALVANIZED STEEL 3.65 METER | EACH | 1 |
| TRAFFIC SIGNAL POST, GALVANIZED STEEL 4.85 METER | EACH | 2 |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 11.58 METER | EACH | 1 |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 12.80 METER | EACH | 1 |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 13.41 METER | EACH | 1 |
| CONCRETE FOUNDATION, TYPE A | METER | 3.6 |
| CONCRETE FOUNDATION, TYPE D | METER | 1.2 |
| CONCRETE FOUNDATION, TYPE E, 600MM DIAMETER | METER | 4.6 |
| CONCRETE FOUNDATION, TYPE E, 750MM DIAMETER | METER | 18.4 |
| DETECTOR LOOP, TYPE I | METER | 227 |
| ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C | METER | 1275 |
| FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125 MM12F & SM12F | METER | 600 |
| ELECTRIC CABLE IN CONDUIT, GROUNDING NO. 6 1C | METER | 164 |
| SERVICE INSTALLATION, TYPE A | EACH | 1 |
| LIGHT POLE, WOOD, 10.67 M, CLASS 3 | EACH | 1 |
| SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED | EACH | 9 |
| SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED | EACH | 1 |
| SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED | EACH | 2 |
| SIGNAL HEAD, LED, 1-FACE, 4-SECTION, MAST ARM MOUNTED | EACH | 1 |
| SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED | EACH | 1 |
| SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED | EACH | 2 |
| LIGHT DETECTOR | EACH | 2 |
| LIGHT DETECTOR AMPLIFIER | EACH | 1 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 20 3C TWISTED SHIELDED | EACH | 121 |
| PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED W/CDT | EACH | 4 |
| BATTERY BACKUP | EACH | 1 |
| STEEL MAST ARM ASSEMBLY AND POLE, 6.09 METER | EACH | 1 |
| PEDESTRIAN SIGNAL HEAD LED, 1-FACE BRACKET MOUNTED | EACH | 1 |



NOTE: LIGHTING CABLE SHALL CONSIST OF 2-#6 (XLP-TYPE USE) 1-#8 (XLP-TYPE USE) AND SHALL BE INSTALLED IN THE SEPARATE LIGHTING CONDUIT AS SHOWN ON THE PLAN SHEET. SEE BILL OF MATERIALS FOR LIGHTING.

100% COST TO CITY OF BLOOMINGTON FIRE DEPARTMENT

REVISED SHEET 10/25/09 JS-11

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---|
| NAME | DATE | |
| | | MORRIS AVENUE & SIX POINT ROAD SCHEDULE OF QUANTITIES, CABLE PLAN AND PHASE DESIGNATION DIAGRAM |
| | | |
| | | |
| | | |
| | | |

AMES Engineering, Inc.
Consulting Engineers
1341 Warren Avenue
Downers Grove, IL 60515

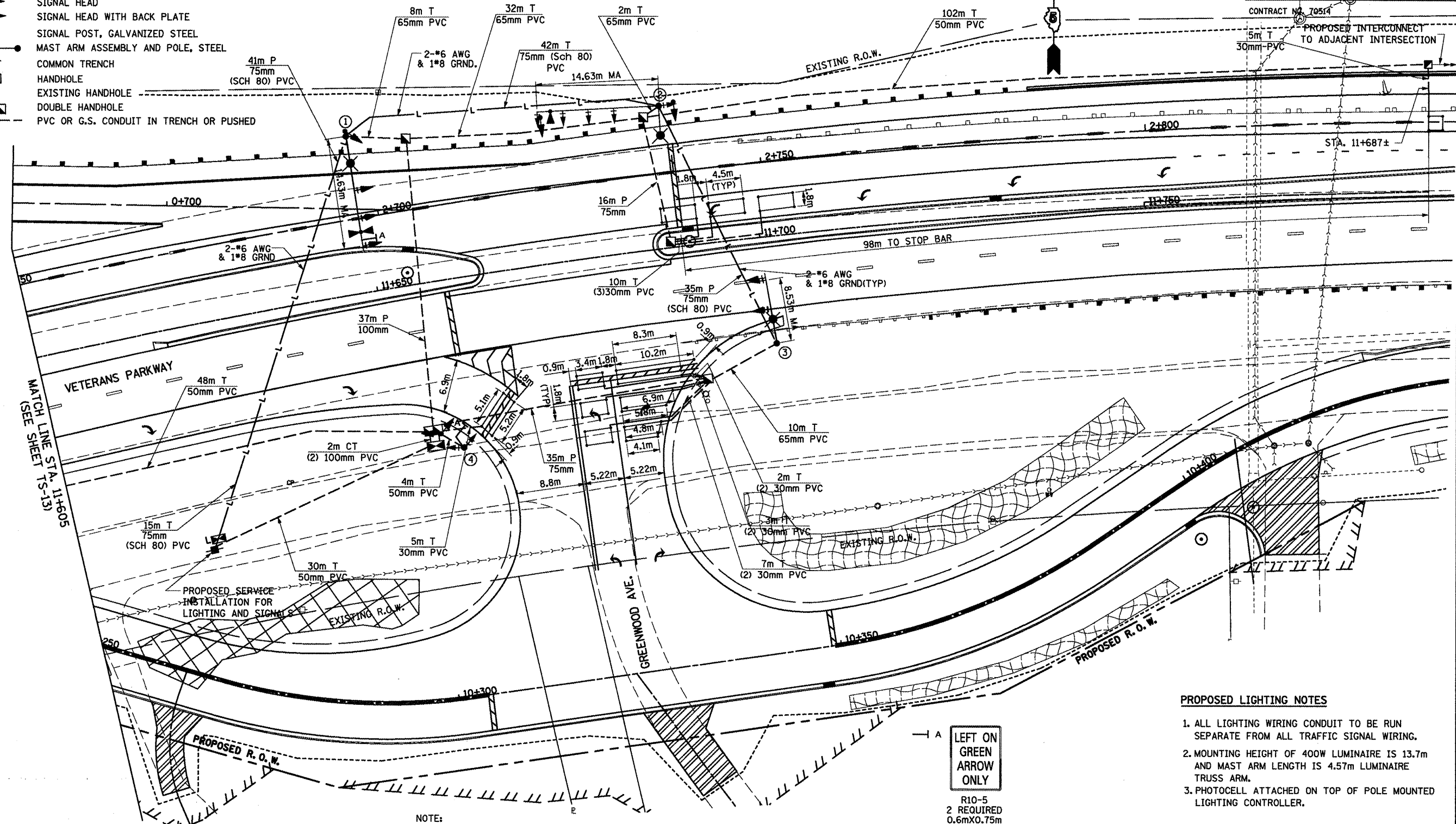
SCALE: NONE
DATE: MARCH, 2009
DRAWN BY: RV
DESIGNED BY: AS
CHECKED BY: MSA

\$FILE\$ 10/14/2010 \$TIME\$

PROPOSED TRAFFIC SIGNAL LEGEND

- CONTROLLER LOCATED ON EXISTING FOUNDATION
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD WITH BACK PLATE
- SIGNAL POST, GALVANIZED STEEL
- MAST ARM ASSEMBLY AND POLE, STEEL
- COMMON TRENCH
- HANDHOLE
- EXISTING HANDHOLE
- DOUBLE HANDHOLE
- PVC OR G.S. CONDUIT IN TRENCH OR PUSHED

| | | | | |
|---------------------|-------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1N & TS-1) | McLean | 497 | 261 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



PROPOSED LIGHTING LEGEND

- LIGHTING PVC CONDUIT IN TRENCH OR PUSHED
- 60A-120/240V, 1 PHASE 3 WIRE LIGHTING CONTROLLER SERVICE POLE MOUNTED
- COMBINATION LIGHTING UNIT, 400W HPS LUMINAIRE (SEE NOTE 2)

NOTE: TRAFFIC ARROWS SHOWN ARE INFORMATION ONLY. REFER TO PAVEMENT MARKING PLAN FOR MARKING LOCATIONS.

| REF. NO. | STATION | OFFSET | DESCRIPTION |
|----------|----------|----------|---|
| ① | 2+697.50 | 11.4m LT | STEEL COMBINATION MAST ARM ASSEMBLY & POLE 14.41m |
| ② | 2+738.0 | 8.6m LT | STEEL COMBINATION MAST ARM ASSEMBLY & POLE 14.41m |
| ③ | 2+749.0 | 24.0m RT | STEEL COMBINATION MAST ARM ASSEMBLY & POLE 8.53m |
| ④ | 2+707.0 | 31.6m RT | TRAFFIC SIGNAL POST, GALVANIZED STEEL 4.85m |

PROPOSED LIGHTING NOTES

1. ALL LIGHTING WIRING CONDUIT TO BE RUN SEPARATE FROM ALL TRAFFIC SIGNAL WIRING.
2. MOUNTING HEIGHT OF 400W LUMINAIRE IS 13.7m AND MAST ARM LENGTH IS 4.57m LUMINAIRE TRUSS ARM.
3. PHOTOCELL ATTACHED ON TOP OF POLE MOUNTED LIGHTING CONTROLLER.

← A
LEFT ON GREEN ARROW ONLY
R10-5
2 REQUIRED
0.6mX0.75m
SIGN PANEL TYPE 1

TS-12

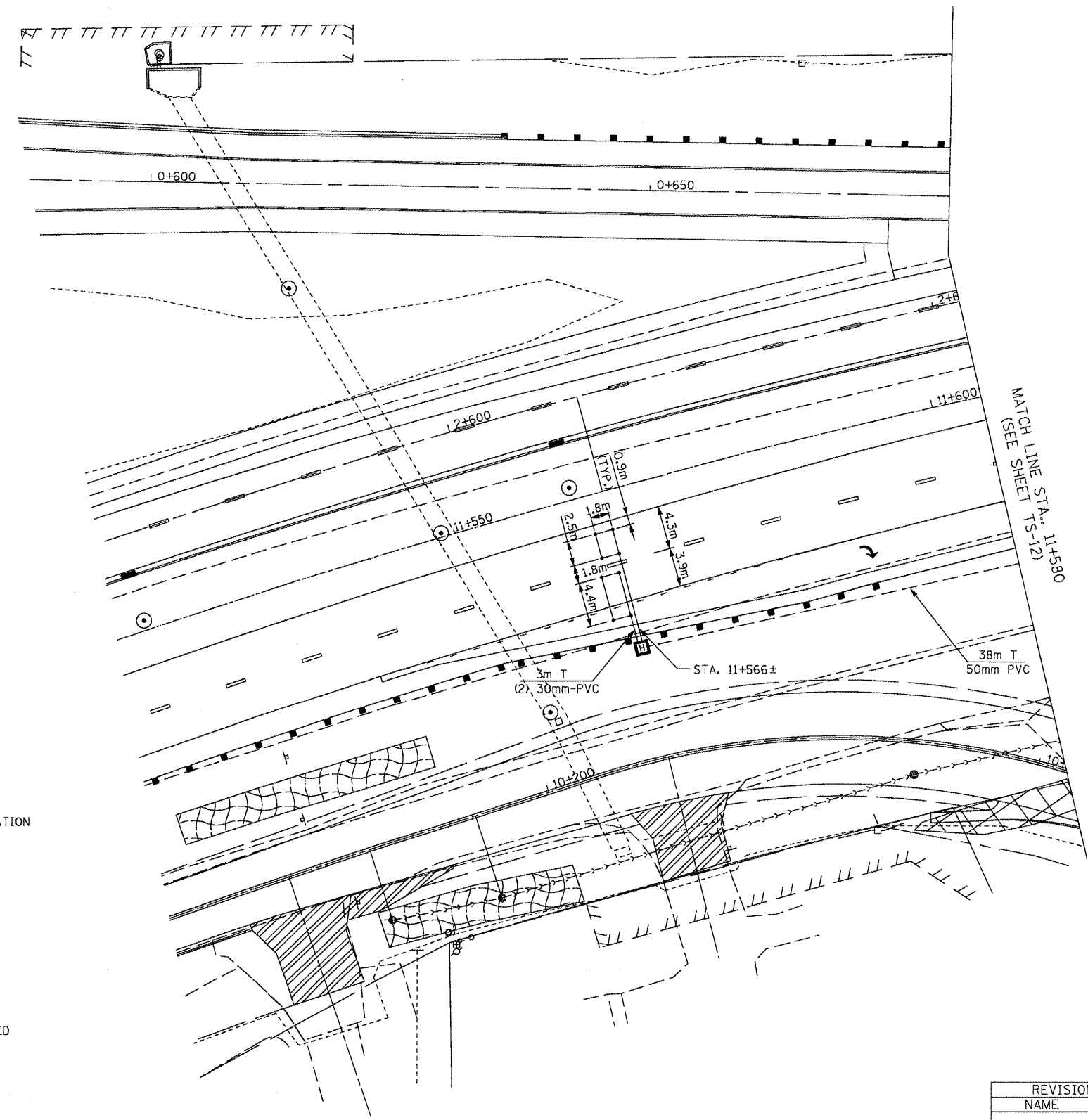
\$FILE\$ 10/14/2010 \$TIME\$

AMES Engineering, Inc.
Consulting Engineers
134 Warren Avenue
Downers Grove, IL 60555

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
**VETERANS PARKWAY AND GREENWOOD AVE.
LIGHTING & TRAFFIC SIGNAL INSTALLATION**
SCALE: 1" = 250'
DATE: MARCH, 2009
DRAWN BY: RV
DESIGNED BY: AS
CHECKED BY: MSA

| | | | | |
|---------------------|-------------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1)N & TS-1 | McLean | 497 | 262 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 70514 | | | | |



PROPOSED TRAFFIC SIGNAL LEGEND

- CONTROLLER LOCATED ON EXISTING FOUNDATION
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD WITH BACK PLATE
- SIGNAL POST, GALVANIZED STEEL
- MAST ARM ASSEMBLY AND POLE, STEEL
- COMMON TRENCH
- HANDHOLE
- EXISTING HANDHOLE
- DOUBLE HANDHOLE
- HEAVY DUTY HANDHOLE
- PVC OR G.S. CONDUIT IN TRENCH OR PUSHED
- CONDUIT LENGTH AND SIZE AND TYPE (TRENCHED AND/OR PUSHED AS INDICATED)
- 400W HPS LUMINAIRE (REFER TO LIGHTING PLANS)
- SIGNAL HEAD, PEDESTRIAN
- DETECTOR LOOP

\$FILE\$ 10/14/2010 \$TIME\$

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

AMES Engineering, Inc.
 Consulting Engineers
 1341 Warren Avenue
 Downers Grove, IL 60515

ILLINOIS DEPARTMENT OF TRANSPORTATION

 VETERANS PARKWAY AND GREENWOOD AVE.
 TRAFFIC SIGNAL INSTALLATION-2

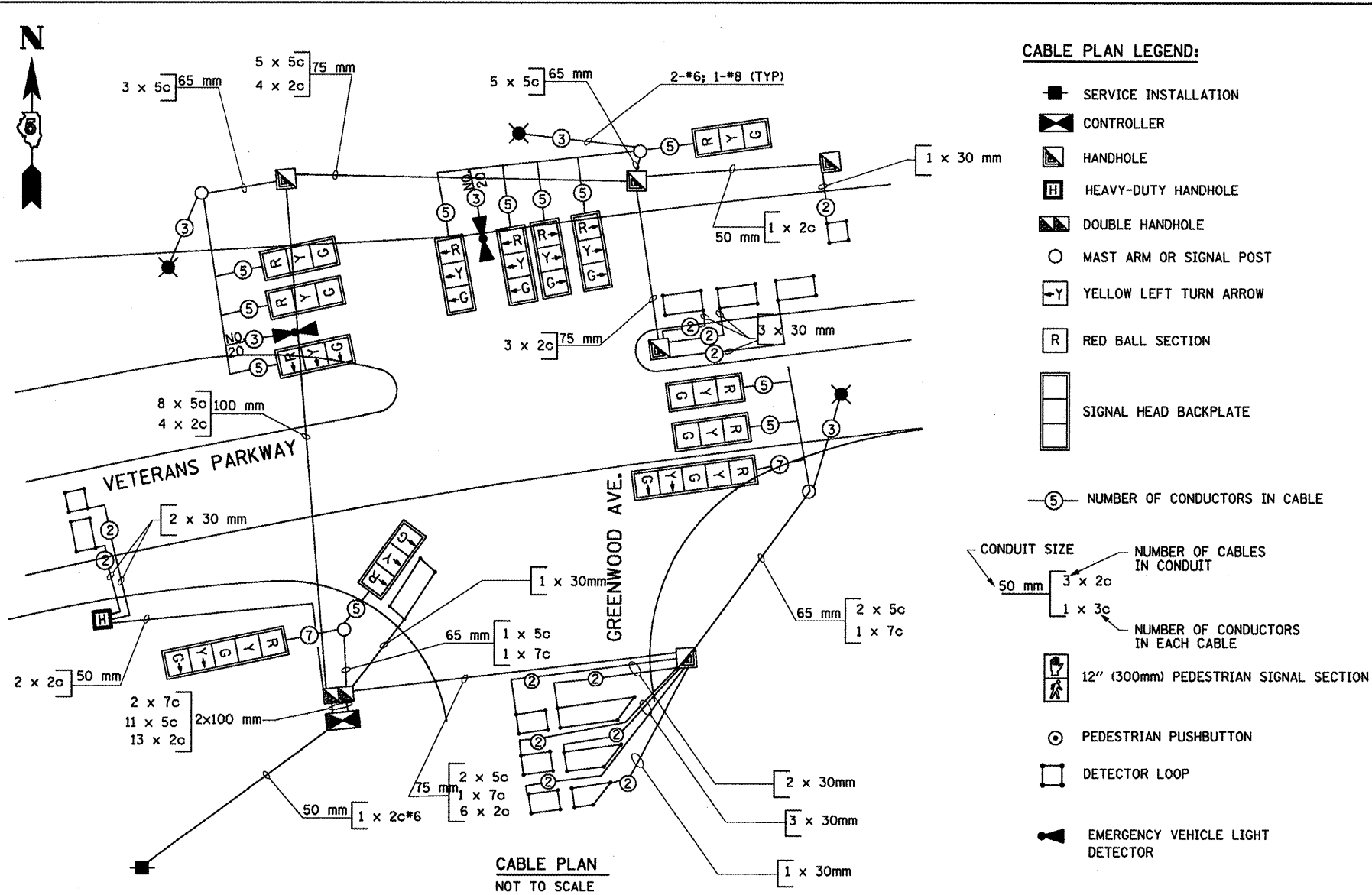
 SCALE:
 DATE: MARCH, 2009

 DRAWN BY: RV
 DESIGNED BY: AS
 CHECKED BY: MSA

TS-13

| | | | | |
|---------------------|------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (JN & TS-1 | McLean | 497 | 263 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 70514

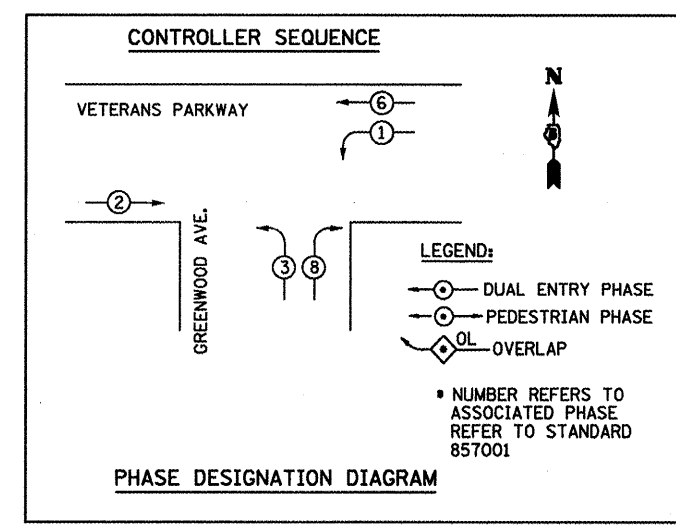


- CABLE PLAN LEGEND:**
- SERVICE INSTALLATION
 - ◻ CONTROLLER
 - ◻ HANDHOLE
 - ◻ HEAVY-DUTY HANDHOLE
 - ◻ DOUBLE HANDHOLE
 - MAST ARM OR SIGNAL POST
 - Y YELLOW LEFT TURN ARROW
 - R RED BALL SECTION
 - ◻ SIGNAL HEAD BACKPLATE
 - ⑤ NUMBER OF CONDUCTORS IN CABLE
- CONDUIT SIZE
- 50 mm
- 3 x 2c
 - 1 x 3c
- NUMBER OF CABLES IN CONDUIT
- NUMBER OF CONDUCTORS IN EACH CABLE
- 12" (300mm) PEDESTRIAN SIGNAL SECTION
 - ⊙ PEDESTRIAN PUSHBUTTON
 - ◻ DETECTOR LOOP
 - ▶ EMERGENCY VEHICLE LIGHT DETECTOR

BILL OF MATERIALS

| ITEM | UNIT | QNTY. |
|---|-------|-------|
| HANDHOLE | EACH | 5 |
| HEAVY DUTY HANDHOLE | EACH | 1 |
| DOUBLE HANDHOLE | EACH | 1 |
| FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL | EACH | 1 |
| TRANSCEIVER - FIBER OPTIC | EACH | 1 |
| TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC | EACH | 12 |
| INDUCTIVE LOOP DETECTOR | EACH | 13 |
| SIGN PANEL TYPE I | SQ M | 1 |
| SIGN PANEL TYPE II | SQ M | 3 |
| CONDUIT IN TRENCH, 50MM DIA., PVC | METER | 222 |
| CONDUIT IN TRENCH, 65MM DIA., PVC | METER | 52 |
| CONDUIT IN TRENCH, 100MM DIA., PVC | METER | 4 |
| CONDUIT PUSHED, 75MM DIA., GALVANIZED STEEL | METER | 51 |
| CONDUIT PUSHED, 100MM DIA., GALVANIZED STEEL | METER | 37 |
| TRENCH AND BACKFILL FOR ELECTRICAL WORK | METER | 311 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C | METER | 1010 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | METER | 964 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | METER | 96 |
| ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR | METER | 308 |
| ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C | METER | 36 |
| TRAFFIC SIGNAL POST, GALVANIZED STEEL 4.85 METER | EACH | 1 |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 8.53 METER | EACH | 1 |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 14.63 METER | EACH | 2 |
| CONCRETE FOUNDATION, TYPE A | METER | 1.2 |
| CONCRETE FOUNDATION, TYPE D | METER | 1.2 |
| CONCRETE FOUNDATION, TYPE E, 600MM DIAMETER | METER | 4 |
| CONCRETE FOUNDATION, TYPE E, 750MM DIAMETER | METER | 13.8 |
| DETECTOR LOOP, TYPE I | METER | 162 |
| ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C | METER | 839 |
| FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125 MM12F & SM12F | METER | 300 |
| ELECTRIC CABLE IN CONDUIT, GROUNDING NO. 6 1C | METER | 181 |
| SERVICE INSTALLATION, TYPE A | EACH | 1 |
| LIGHT POLE, WOOD, 10.67 METERS, CLASS 3 | EACH | 1 |
| SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED | EACH | 9 |
| SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED | EACH | 1 |
| SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED | EACH | 1 |
| SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED | EACH | 1 |
| LIGHT DETECTOR | EACH | 2 |
| LIGHT DETECTOR AMPLIFIER | EACH | 1 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 20 3C TWISTED SHIELDED | EACH | 195 |
| BATTERY BACKUP | EACH | 1 |

•• 100% COST TO CITY OF BLOOMINGTON FIRE DEPARTMENT.



NOTE: LIGHTING CABLE SHALL CONSIST OF 2-*6 (XLP-TYPE USE) 1-*8 (XLP-TYPE USE) AND SHALL BE INSTALLED IN THE SEPARATE LIGHTING CONDUIT AS SHOWN ON THE PLAN SHEET. SEE BILL OF MATERIALS FOR LIGHTING.

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---|
| NAME | DATE | |
| | | VETERANS PARKWAY AND GREENWOOD AVE. SCHEDULE OF QUANTITIES CABLE PLAN AND PHASE DESIGNATION DIAGRAM |
| | | |
| | | |
| | | |
| | | |

SCALE: NONE
DATE: MARCH, 2009

DRAWN BY: RV
DESIGNED BY: AS
CHECKED BY: MSA

AMES Engineering, Inc.
Consulting Engineers
1341 Warren Avenue
Downers Grove, IL 60555

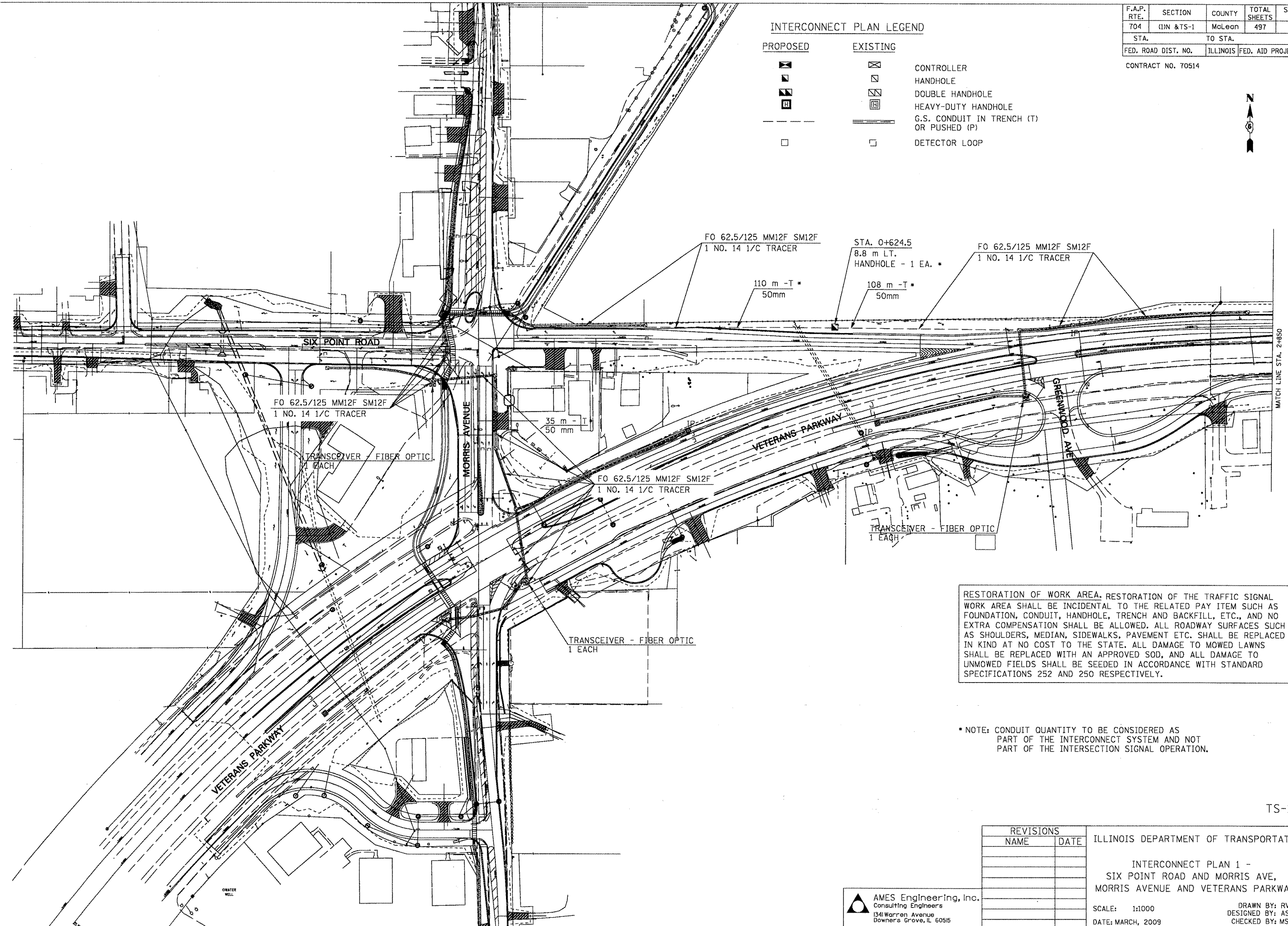
FILE# 10/14/2010 TIME#

TS-14

| | | | | |
|---------------------|-------------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1)N & TS-1 | McLean | 497 | 264 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 70514 | | | | |

INTERCONNECT PLAN LEGEND

| PROPOSED | EXISTING | |
|----------|----------|--|
| | | CONTROLLER |
| | | HANDHOLE |
| | | DOUBLE HANDHOLE |
| | | HEAVY-DUTY HANDHOLE |
| | | G.S. CONDUIT IN TRENCH (T) OR PUSHED (P) |
| | | DETECTOR LOOP |



RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND AT NO COST TO THE STATE. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

* NOTE: CONDUIT QUANTITY TO BE CONSIDERED AS PART OF THE INTERCONNECT SYSTEM AND NOT PART OF THE INTERSECTION SIGNAL OPERATION.

TS-15

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

INTERCONNECT PLAN 1 -
SIX POINT ROAD AND MORRIS AVE,
MORRIS AVENUE AND VETERANS PARKWAY

SCALE: 1:1000
DATE: MARCH, 2009

DRAWN BY: RV
DESIGNED BY: AS
CHECKED BY: MSA

AMES Engineering, Inc.
Consulting Engineers
1341 Warren Avenue
Downers Grove, IL 60515

\$FILES\$
8/19/2010 \$TIMES\$

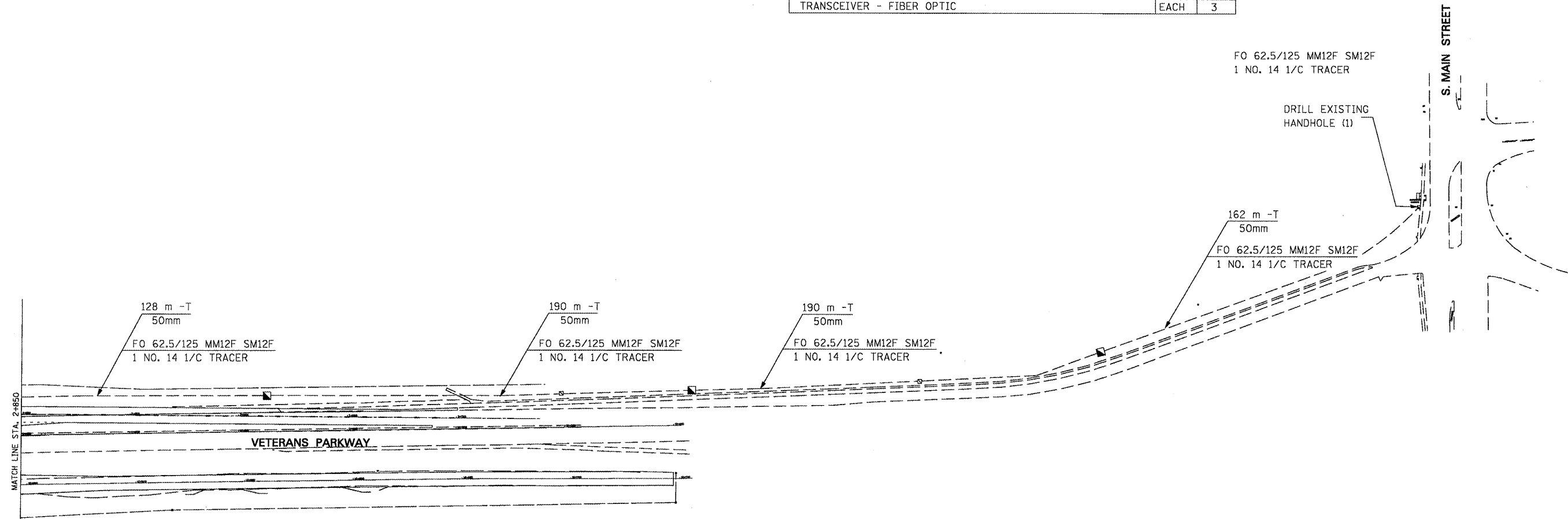
FIBER OPTIC INTERCONNECT

BILL OF MATERIALS

| | | | | |
|---------------------|-------------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1)N & TS-1 | McLean | 497 | 265 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

CONTRACT NO. 70514

| ITEM | UNIT | QNTY. |
|--|-------|-------|
| HANDHOLE | EACH | 4 |
| CONDUIT IN TRENCH, 50MM DIA., PVC | METER | 300 |
| CONDUIT PUSHED, 50MM DIA., GALVANIZED STEEL | METER | 100 |
| TRENCH AND BACKFILL FOR ELECTRICAL WORK | METER | 300 |
| MASTER CONTROLLER | EACH | 1 |
| ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C | METER | 800 |
| FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125 MM12F & SM12F | METER | 1000 |
| DRILL EXISTING HAND HOLE | METER | 1 |
| TRANSCEIVER - FIBER OPTIC | EACH | 3 |



NOTE: CONDUIT & FIBER QUANTITY TO BE CONSIDERED AS PART OF THE INTERCONNECT SYSTEM AND NOT PART OF THE INTERSECTION SIGNAL OPERATION.
HANDHOLES TO BE LOCATED PER THE ENGINEER.

INTERCONNECT PLAN LEGEND

| PROPOSED | EXISTING | |
|----------|----------|--|
| | | CONTROLLER |
| | | HANDHOLE |
| | | DOUBLE HANDHOLE |
| | | HEAVY-DUTY HANDHOLE |
| | | G.S. CONDUIT IN TRENCH (T) OR PUSHED (P) |
| | | DETECTOR LOOP |

TS-16

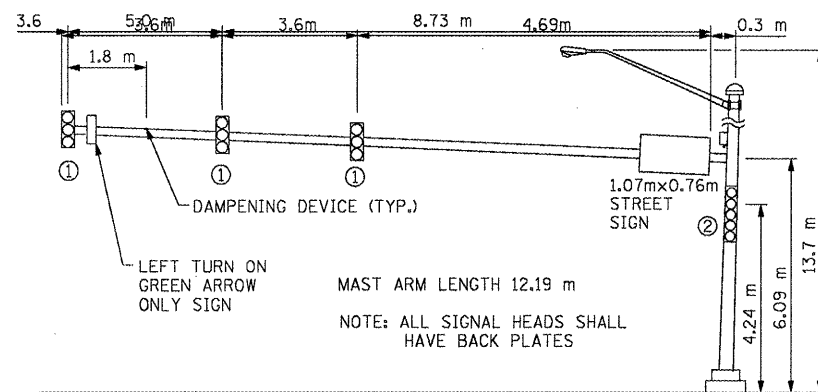
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
INTERCONNECT PLAN - 2
VETERANS PARKWAY
SCALE: 1:1000
DATE: MARCH, 2009
DRAWN BY: RV
DESIGNED BY: AS
CHECKED BY: MSA

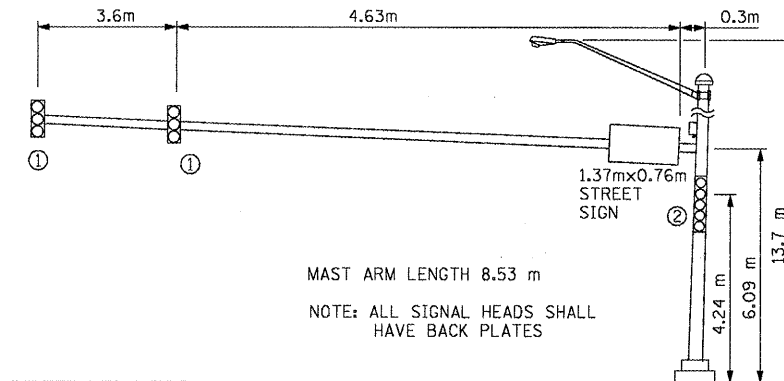
AMES Engineering, Inc.
Consulting Engineers
1341 Warren Avenue
Downers Grove, IL 60515

\$FILES 8/19/2010 \$TIME\$

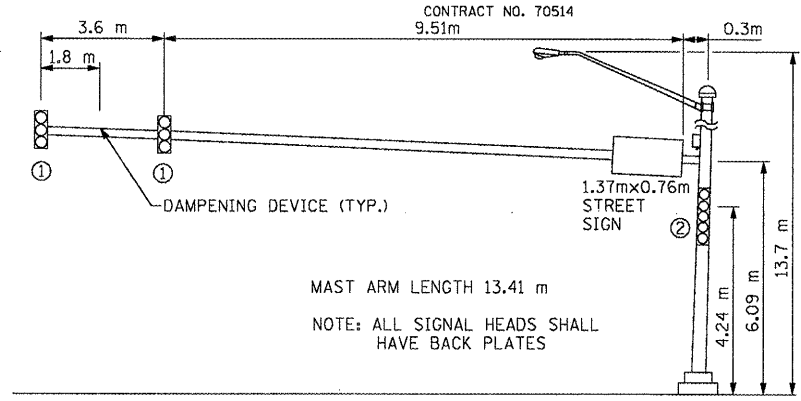
| | | | | |
|---------------------|------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1N & TS-1 | McLean | 497 | 266 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



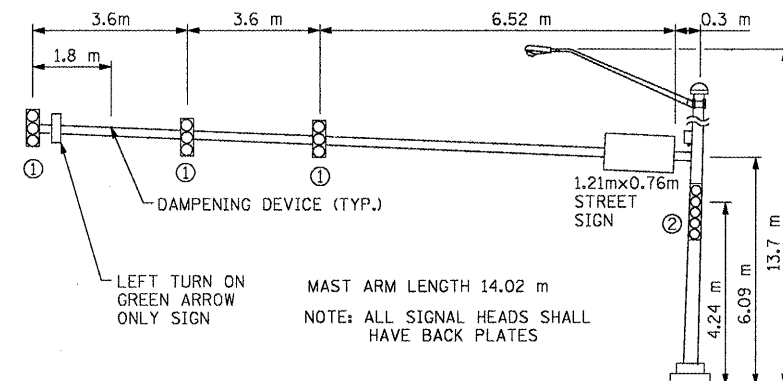
VETERANS PARKWAY AND MORRIS AVENUE (NORTHBOUND)



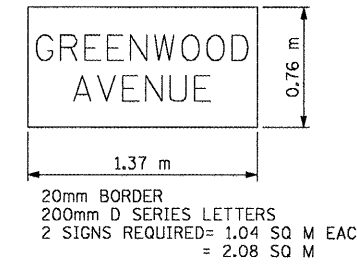
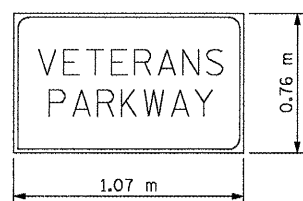
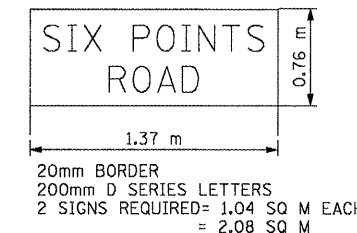
VETERANS PARKWAY AND GREENWOOD AVENUE (EASTBOUND)



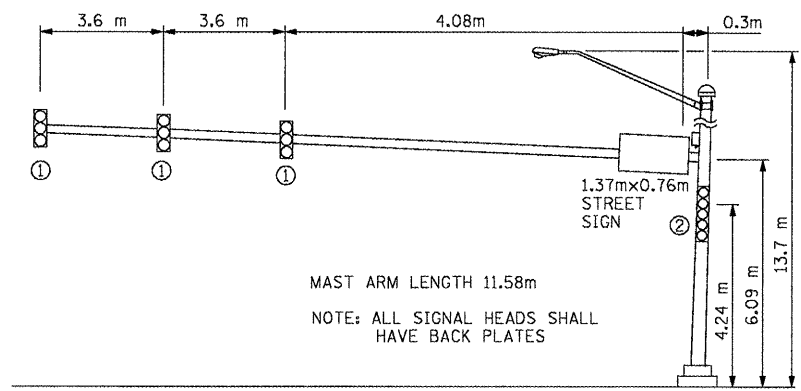
SIX POINTS ROAD MORRIS AVENUE (NORTHBOUND)



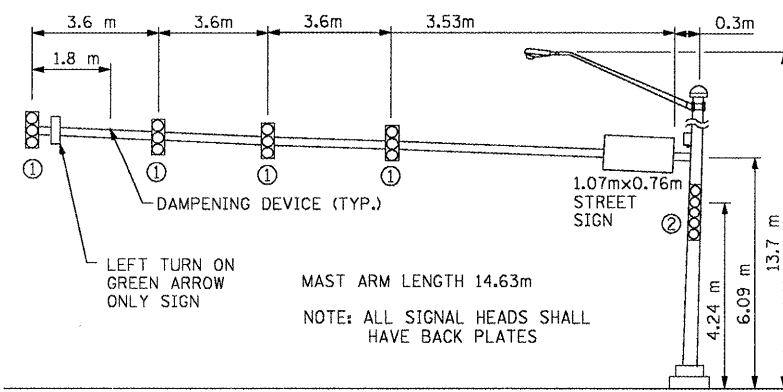
VETERANS PARKWAY AND MORRIS AVENUE (EASTBOUND)



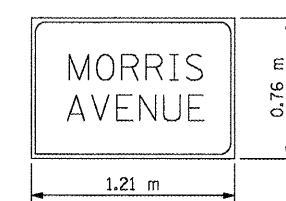
NOTE: REFER TO I.D.O.T. STANDARD 720016



SIX POINTS ROAD MORRIS AVENUE (WESTBOUND)



VETERANS PARKWAY AND GREENWOOD AVENUE (NORTHBOUND)



NOTE: REFER TO I.D.O.T. STANDARD 720016

| VETERANS PARWAY/MORRIS | MAST ARM LENGTH | FOUNDATION DIAMETER | FOUNDATION DEPTH | MIN. DEPTH INTO BED ROCK |
|---------------------------|-----------------|---------------------|------------------|--------------------------|
| N.E. | 12.19m | 750mm | 4.6m | 0.0 |
| N.W. | 15.24m | 750mm | 5.2m | 0.0 |
| S.E. | 14.02m | 750mm | 4.6m | 0.0 |
| S.W. | 15.24m | 750mm | 5.2m | 0.0 |
| VETERANS PARWAY/GREENWOOD | MAST ARM LENGTH | FOUNDATION DIAMETER | FOUNDATION DEPTH | MIN. DEPTH INTO BED ROCK |
| N.E. | 14.63m | 750mm | 4.6m | 0.0 |
| N.W. | 14.63m | 750mm | 4.6m | 0.0 |
| S.E. | 8.53m | 750mm | 4.6m | 0.0 |
| SIX POINTS ROAD/MORRIS | MAST ARM LENGTH | FOUNDATION DIAMETER | FOUNDATION DEPTH | MIN. DEPTH INTO BED ROCK |
| N.E. | 13.41m | 750mm | 4.6m | 0.0 |
| N.W. | 11.58m | 750mm | 4.6m | 0.0 |
| S.E. | 6.09m | 750mm | 4.6m | 0.0 |
| S.W. | 12.8m | 750mm | 4.6m | 0.0 |



R10-5L (0.6mX0.7m)
8 SIGNS REQUIRED - 0.45 SQ.M EACH
- 0.45 SQ.M EACH
TYPE A SHEETING REQUIRED
THIS SIGN SHALL BR LOCATED 0.15m TO 0.3m TO THE RIGHT OF EACH 3 SECTION MAST ARM MOUNTED LEFT TURN SIGNAL. PLUS ADJACENT TO EACH POST MOUNTED 3-SECTION LEFT TURN LANE

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
VETERANS PARKWAY & MORRIS AVENUE
VETERANS PARKWAY & GREENWOOD AVENUE
SIX POINTS ROAD & MORRIS AVENUE
MAST ARM DETAILS

SCALE: NONE
DATE: MARCH, 2009

DRAWN BY: RV
DESIGNED BY: AS
CHECKED BY: MSA

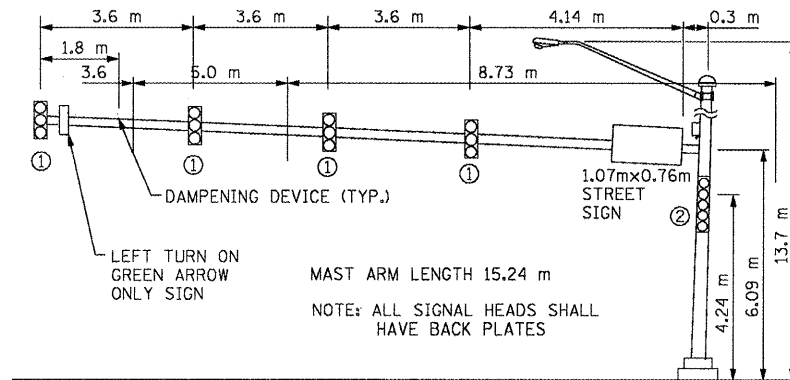
AMES Engineering, Inc.
Consulting Engineers
1341 Warren Avenue
Downers Grove, IL 60515

FILES\$ 10/14/2010 \$TIME\$

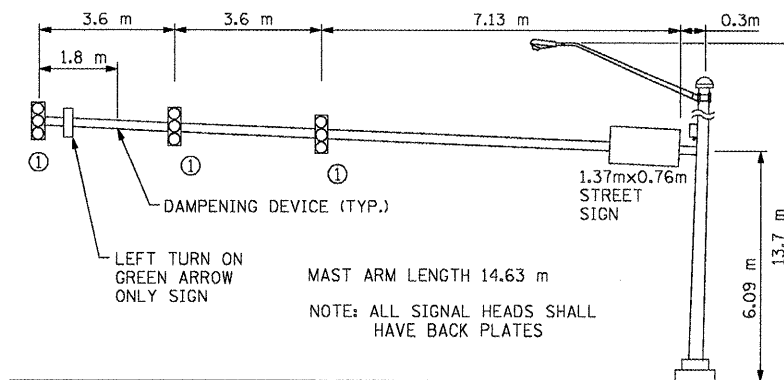
TS-17

| | | | | |
|---------------------|------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1N & TS-1 | McLean | 497 | 267 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

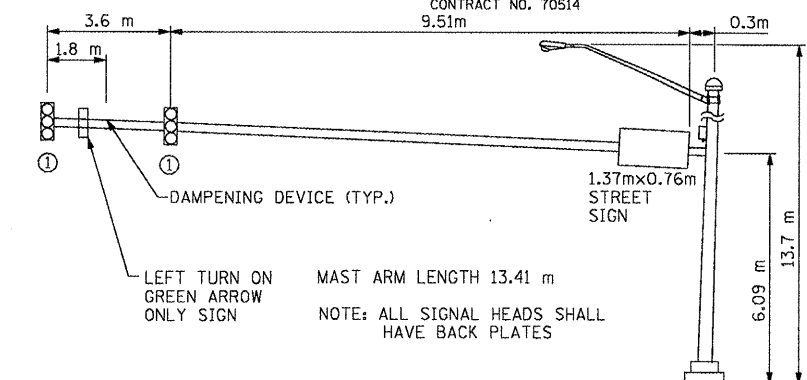
CONTRACT NO. 70514
9.51m



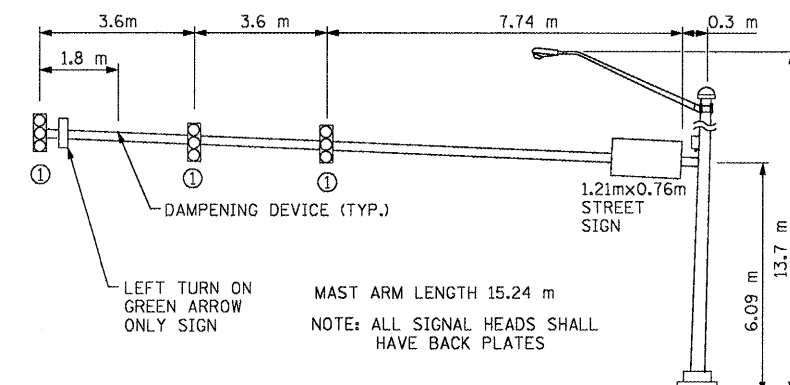
VETERANS PARKWAY AND MORRIS AVENUE (SOUTHBOUND)



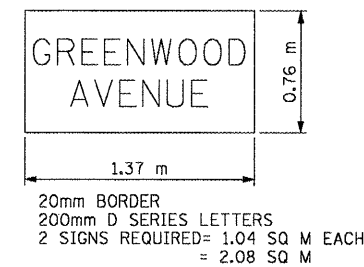
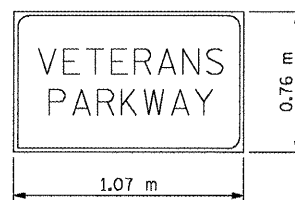
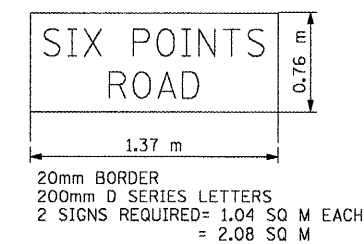
VETERANS PARKWAY AND GREENWOOD AVENUE (WESTBOUND)



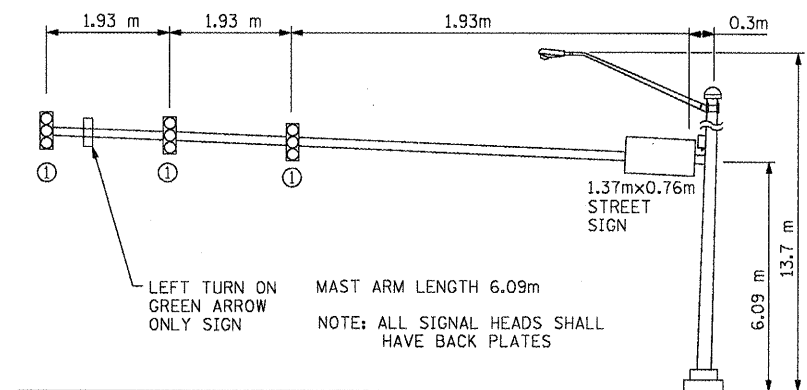
SIX POINTS ROAD MORRIS AVENUE (SOUTHBOUND)



VETERANS PARKWAY AND MORRIS AVENUE (WESTBOUND)

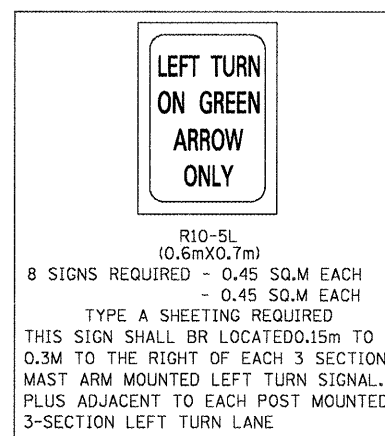


NOTE: REFER TO I.D.O.T. STANDARD 720016



SIX POINTS ROAD MORRIS AVENUE (EASTBOUND)

| VETERANS PARWAY/MORRIS | MAST ARM LENGTH | FOUNDATION DIAMETER | FOUNDATION DEPTH | MIN. DEPTH INTO BED ROCK |
|---------------------------|-----------------|---------------------|------------------|--------------------------|
| N.E. | 12.19m | 750mm | 4.6m | 0.0 |
| N.W. | 15.24m | 750mm | 5.2m | 0.0 |
| S.E. | 14.02m | 750mm | 4.6m | 0.0 |
| S.W. | 15.24m | 750mm | 5.2m | 0.0 |
| VETERANS PARWAY/GREENWOOD | | | | |
| N.E. | 14.63m | 750mm | 4.6m | 0.0 |
| N.W. | 14.63m | 750mm | 4.6m | 0.0 |
| S.E. | 8.53m | 750mm | 4.6m | 0.0 |
| SIX POINTS ROAD/MORRIS | | | | |
| N.E. | 13.41m | 750mm | 4.6m | 0.0 |
| N.W. | 11.58m | 750mm | 4.6m | 0.0 |
| S.E. | 6.09m | 750mm | 4.6m | 0.0 |
| S.W. | 12.8m | 750mm | 4.6m | 0.0 |



| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
VETERANS PARKWAY & MORRIS AVENUE
VETERANS PARKWAY & GREENWOOD AVENUE
SIX POINTS ROAD & MORRIS AVENUE
MAST ARM DETAILS

SCALE: NONE
DATE: MARCH, 2009

DRAWN BY: RV
DESIGNED BY: BS
CHECKED BY: MSA

AMES Engineering, Inc.
Consulting Engineers
1341 Warren Avenue
Downers Grove, IL 60515

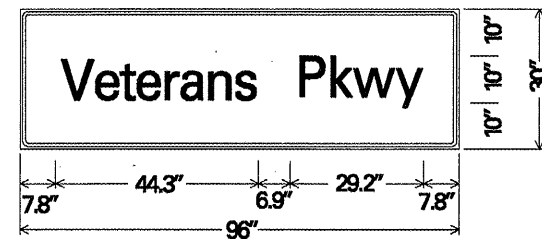
TS-18

FILE# 10/14/2010 \$TIME\$

| | | | | |
|---------------------|-------------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1N & TS-1) | McLean | 497 | 268 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

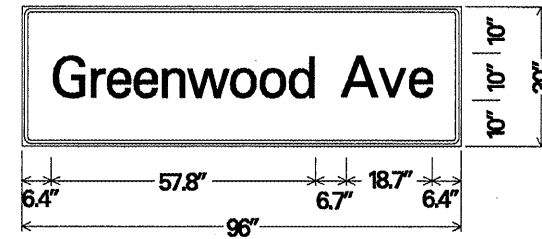
CONTRACT NO. 70514

PANEL SIGN DESIGN TYPE 2



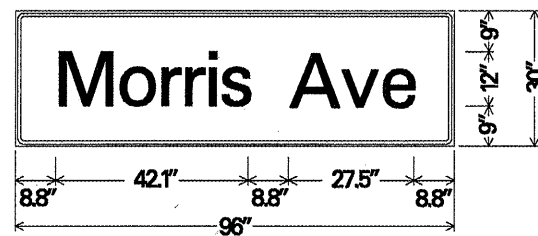
1.9" Radius, 0.8" Border, 0.5" Indent, White on Green;
[Veterans Pkwy] T2000HWYC;

4 EA., 20 Sq. Ft. Each



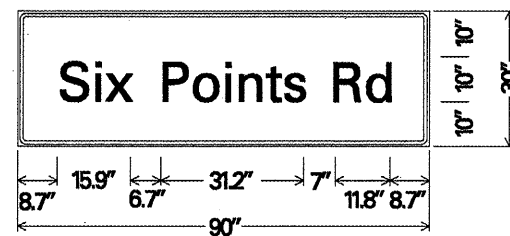
1.9" Radius, 0.8" Border, 0.5" Indent, White on Green;
[Greenwood Ave] T2000HWYC;

2 EA., 20 Sq. Ft. Each



1.9" Radius, 0.8" Border, 0.5" Indent, White on Green;
[Morris Ave] T2000HWYD;

4 EA., 20 Sq. Ft. Each



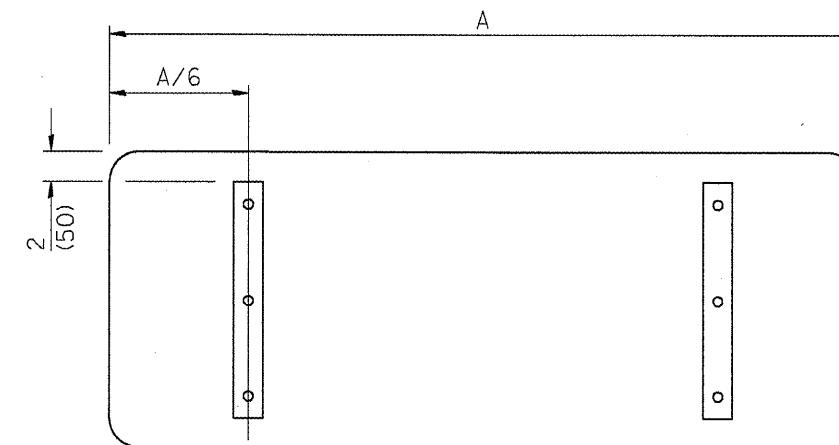
1.9" Radius, 0.8" Border, 0.5" Indent, White on Green;
[Six Points Rd] T2000HWYC;

2 EA., 18.75 Sq. Ft. Each

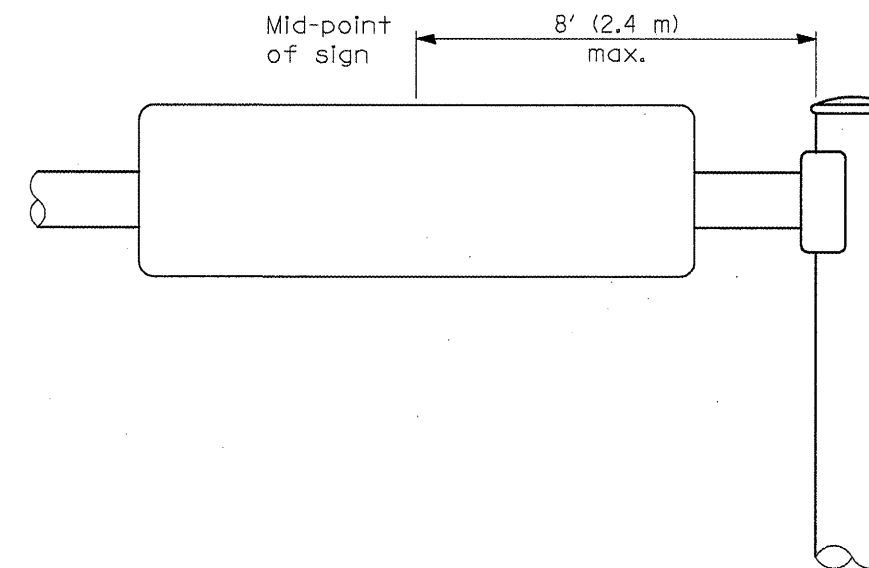
NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS

GENERAL NOTES

1. ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE ZZ SHEETING.
2. THE SIGN PANELS SHALL BE MOUNTED AS SHOWN ON STANDARD 720001-01.



SUPPORTING CHANNELS



MOUNTING LOCATION

TS-19

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

MAST ARM MOUNTED
STREET NAME SIGNS

SCALE: NONE
DATE: MARCH, 2009

DRAWN BY: RDB
DESIGNED BY: JHE
CHECKED BY: DAD

AMES Engineering, Inc.
Consulting Engineers
1341 Warren Avenue
Downers Grove, IL 60515

| | | | | |
|---------------------|------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | 11N & TS-1 | McLean | 497 | 269 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 70514 | | | | |

SCHEDULE OF QUANTITIES LIGHTING ITEMS

| CODE NO. | ITEM | UNIT | TOTAL QTY | VETERANS PKWY & MORRIS AVE | VETERANS PKWY & GREENWOOD AVE | MORRIS AVE & SIX POINTS RD |
|----------|---|-------|-----------|----------------------------|-------------------------------|----------------------------|
| 80400100 | ELECTRIC SERVICE INSTALLATION | EACH | 3 | 1 | 1 | 1 |
| 82102400 | LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, CONTROL 400 WATT | EACH | 10 | 4 | 3 | 3 |
| | CONDUIT PUSHED, 75MM DIA., PVC SCHEDULE 80 | METER | 345 | 140 | 80 | 125 |
| M8170040 | ELECTRICAL CABLE IN CONDUIT, 600V (XLP-TYPE USE), 1/C NO. 6 | METER | 990 | 360 | 290 | 340 |
| M8170030 | ELECTRICAL CABLE IN CONDUIT, 600V (XLP-TYPE USE), 1/C NO. 8 | METER | 495 | 180 | 145 | 170 |
| M8190200 | TRENCH AND BACKFILL FOR ELECTRICAL WORK | METER | 93 | 25 | 57 | 11 |
| MX810070 | CONDUIT IN TRENCH, 75MM DIA., PVC, SCHEDULE 80 | METER | 93 | 25 | 57 | 11 |
| 82500310 | LIGHTING CONTROLLER, POLE MOUNTED, 240 VOLT, 60 AMP | EACH | 3 | 1 | 1 | 1 |
| X8410105 | TEMPORARY LIGHTING SYSTEM | EACH | 2 | 1 | 0 | 1 |
| XX002059 | REMOVAL OF EXISTING LIGHTING UNIT - COMPLETE | EACH | 6 | 2 | 0 | 4 |

FILE\$ 10/14/2010 \$TIME\$

▲ REVISION SHEET 10/25/10 TS-20

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCALE: NONE
DATE: MARCH, 2009

DRAWN BY: RV
DESIGNED BY: AS/SM
CHECKED BY: MSA

▲ AMES Engineering, Inc.
Consulting Engineers
134 Warren Avenue
Downers Grove, IL 60515

BENCHMARK

Square cut in the northwest corner of a concrete intake on the south side of Greenwood Avenue East at the west edge of the mobile home park. Station 10+407.6 3.4 m Right of the proposed Greenwood Avenue East alignment. Elevation 249.710.

EXISTING STRUCTURE

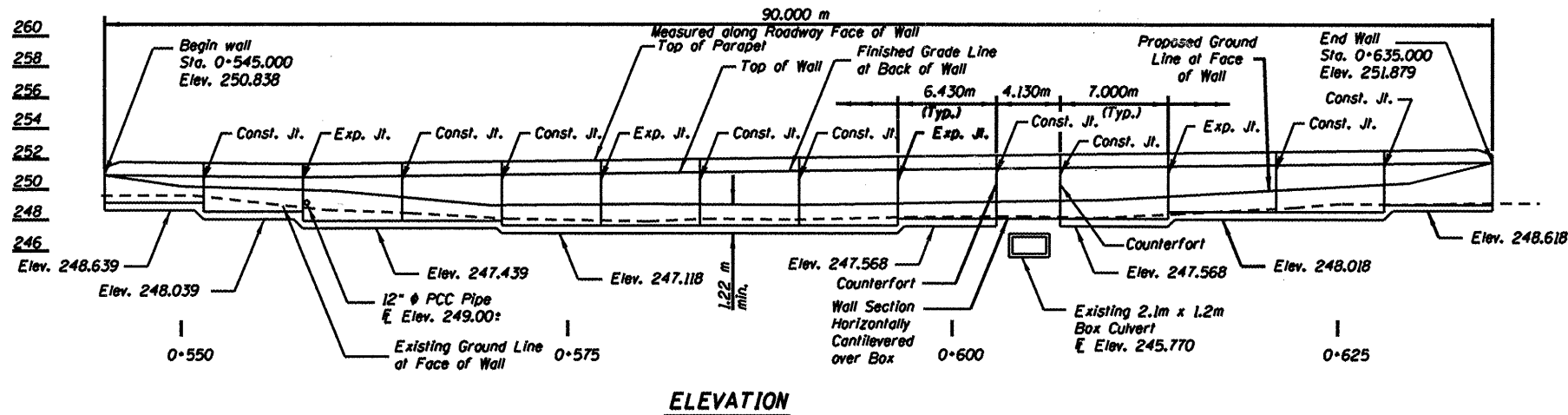
No existing structure.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | |
|--------------------|---------|--------|---------|-------------|
| ROUTE NO. | SECTION | COUNTY | SHEET | SHEET NO. 1 |
| F.A.P. 704 | (1) | MCLEAN | 497 270 | 11 SHEETS |
| CONTRACT NO. 70514 | | | | |

TOTAL BILL OF MATERIAL

| ITEM | UNIT | TOTAL |
|--|------|--------|
| Porous Granular Embankment, Special | Cu M | 300 |
| Structure Excavation | Cu M | 381 |
| Concrete Structures | Cu M | 258 |
| Reinforcement Bars, Epoxy Coated | Kg | 21,190 |
| Geocomposite Wall Drain | Sq M | 259 |
| Pipe Under Drains for Structures, 100mm | M | 90.0 |
| Protective Coat | Sq M | 109 |
| Porous Granular Embankment, Special CA-6 | Cu M | 54 |



Six Points
O/S To
Front Face of Walls

| Station | Top of Wall Elev. (Meters) | Finished Grade F.F. of Wall Elev. |
|-----------|----------------------------|-----------------------------------|
| 0+545.000 | 7.482 | 250.838 |
| 0+550.000 | 7.318 | 250.817 |
| 0+560.000 | 6.990 | 250.807 |
| 0+570.000 | 6.662 | 250.841 |
| 0+580.000 | 6.335 | 250.920 |
| 0+590.000 | 6.007 | 251.041 |
| 0+600.000 | 5.679 | 251.207 |
| 0+610.000 | 5.351 | 251.408 |
| 0+620.000 | 5.350 | 251.607 |
| 0+630.000 | 5.350 | 251.793 |
| 0+635.000 | 5.350 | 251.880 |

** Top of wall to match proposed edge of shoulder elevations for Six Points Road.

DESIGN SPECIFICATIONS

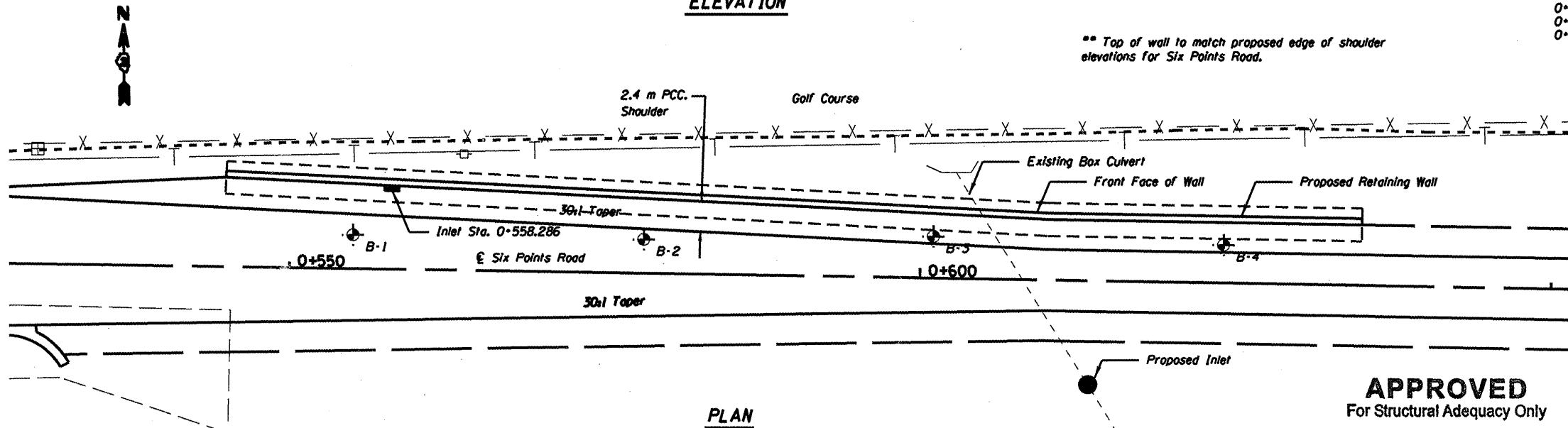
AASHTO 2002

DESIGN STRESSES

FIELD UNITS
f_c = 24 MPa
f_y = 420 MPa (Reinf.)

INDEX OF SHEETS

- GENERAL PLAN
- 5. ELEVATION
- 7. SECTIONS
- DETAILS AND SECTIONS
- DETAILS
- 11. SOIL BORINGS

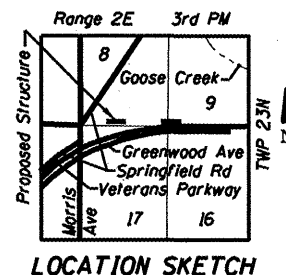


APPROVED
For Structural Adequacy Only

Ralph E. Anderson (SIT)
Engineer of Bridges & Structures

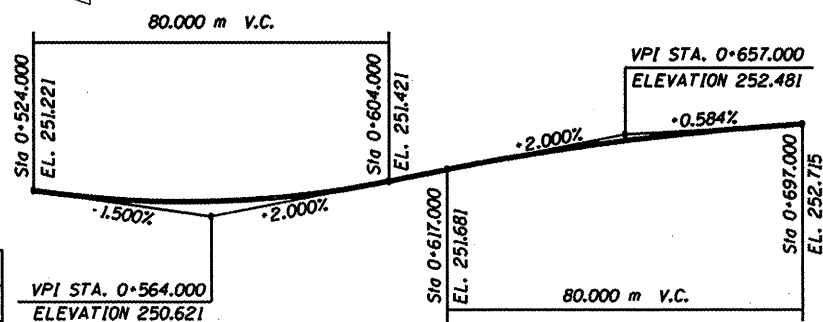


Signature: *William C. Thompson, Jr.*
Date: 8/19/10
Exp. Date: 11/30/2010



GENERAL NOTES

- Protective coat shall be applied to the top and exposed inside faces of wall.
- See sheet 6 thru 8 of 11 for wall thru sections and details.
- ⊕ Denotes soil boring location
- Reinforcement bars shall conform to the requirements of ASTM A 706M GR. 420. See Special Provisions.
- All exposed edges of the concrete facing shall be chamfered 20mm.
- All dimensions are in millimeters (mm) except as noted.
- The footing excavation shall be cleaned of loose material and replaced with Porous Granular Embankment, Special CA-6 prior to pouring the concrete.



SIX POINTS EAST ROADWAY PROFILE

SHIVE HATTERY

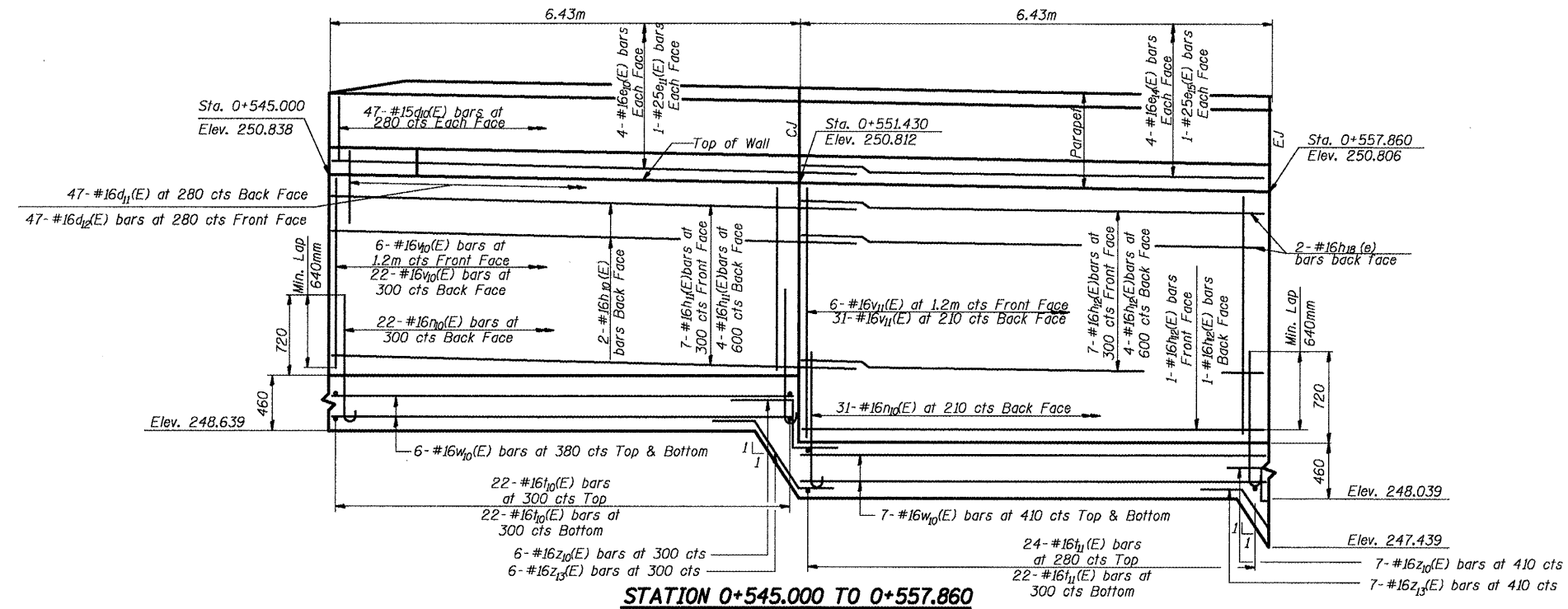
| | |
|----------|-----|
| DESIGNED | JDA |
| CHECKED | WCT |
| DRAWN | MAC |
| CHECKED | WCT |

DATE: 06-25-10

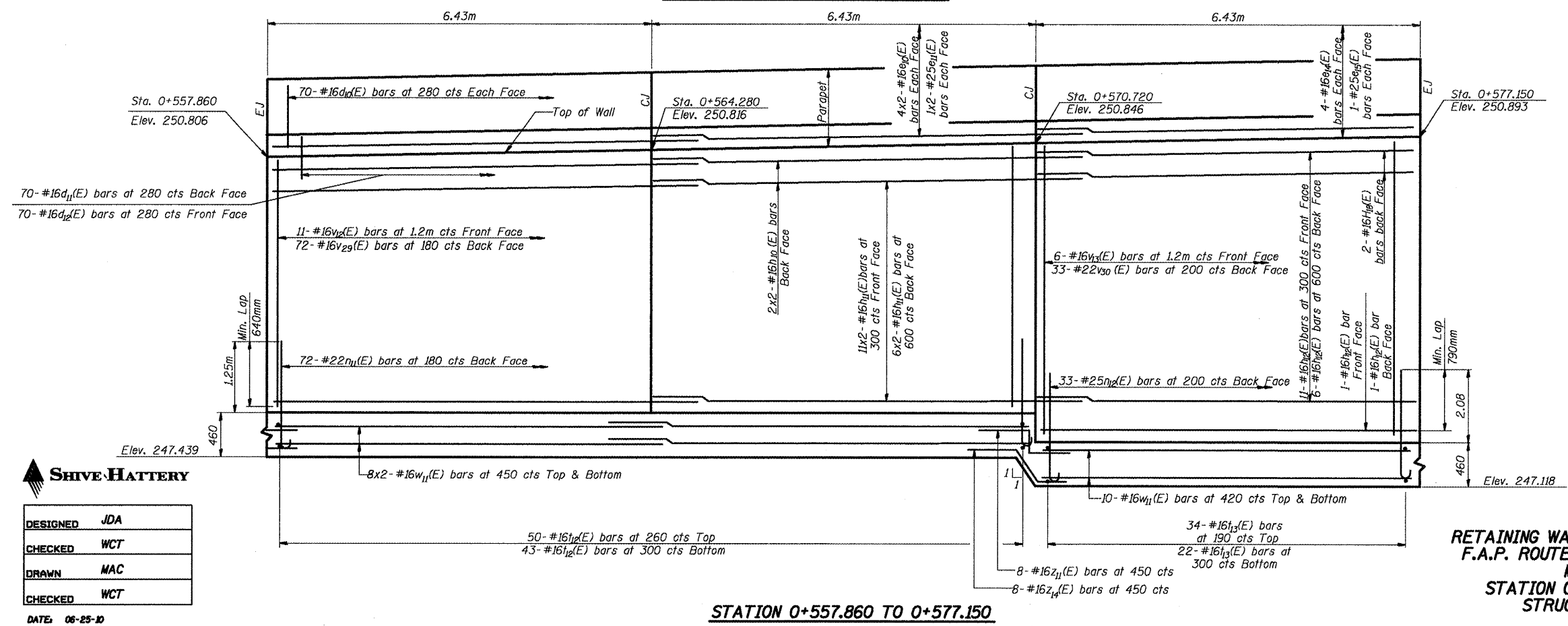
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | |
|-------------|----------|--------|---------|-----------|
| PROJECT NO. | DISTRICT | COUNTY | SECTION | SHEET NO. |
| P.A.P. 704 | CDI | MCLEAN | 497 | 271 |
| DESIGNED | | DRAWN | | DATE |
| JDA | | MAC | | 06-25-10 |

CONTRACT NO. 70514



NOTE: CJ and EJ denote Construction Joint and Expansion Joint respectively



ELEVATION
RETAINING WALL ALONG SIX POINTS ROAD
F.A.P. ROUTE 704 SECTION (D) N&TS-1
MCLEAN COUNTY
STATION 0+545.000 TO 0+635.000
STRUCTURE NO. 057-W001

SHIVE HATTERY

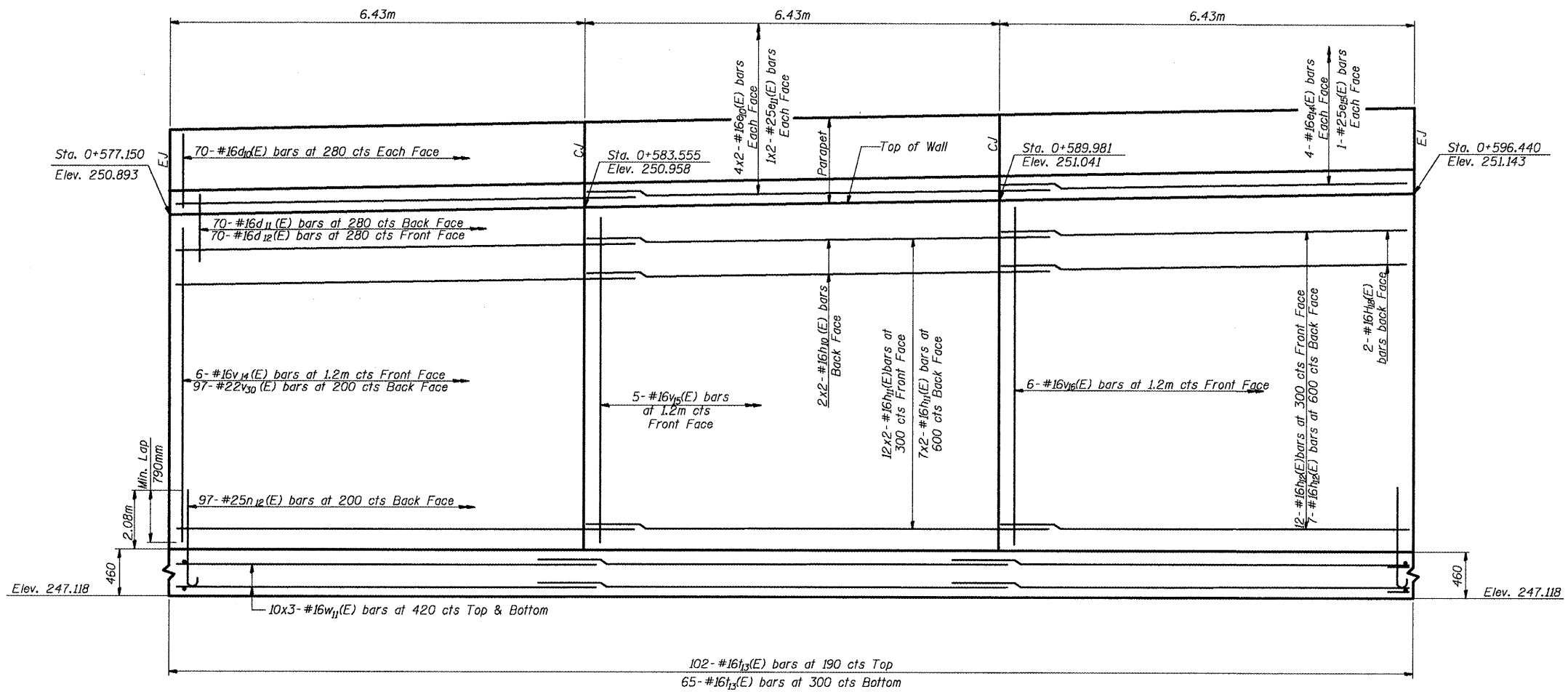
| | |
|----------|-----|
| DESIGNED | JDA |
| CHECKED | WCT |
| DRAWN | MAC |
| CHECKED | WCT |

DATE: 06-25-10

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | | |
|-------------|---------|---------|------|---------|-------------|
| PROJECT NO. | SECTION | SUBJECT | DATE | BY | SHEET NO. 3 |
| F.A.P. 704 | CDI | McLEAN | 497 | 272 | 11 SHEETS |
| DESIGNED | | DRAWN | | CHECKED | |
| DATE | | DATE | | DATE | |

CONTRACT NO. 70514



NOTE: CJ and EJ denote Construction Joint and Expansion Joint respectively

STATION 0+577.150 TO 0+596.440

SHIVE HATTERY

| | |
|----------|-----|
| DESIGNED | JDA |
| CHECKED | WCT |
| DRAWN | MAC |
| CHECKED | WCT |

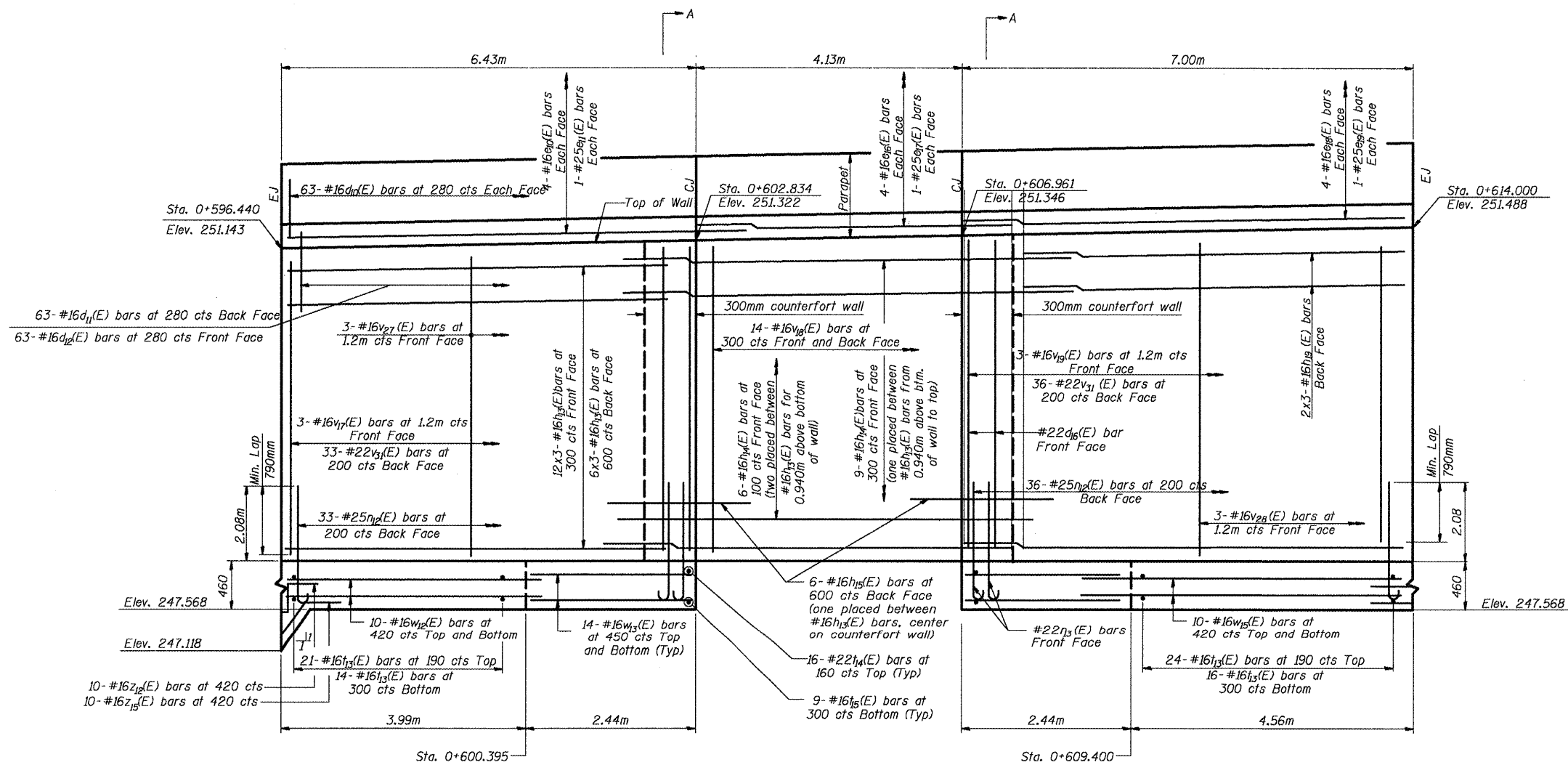
DATE: 06-25-10

ELEVATION
RETAINING WALL ALONG SIX POINTS ROAD
F.A.P. ROUTE 704 SECTION (D) N&TS-1
McLEAN COUNTY
STATION 0+545.000 TO 0+635.000
STRUCTURE NO. 057-W001

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | |
|-------------|----------|-----------|---------|-----------|
| PROJECT NO. | DISTRICT | COUNTY | SECTION | SHEET NO. |
| F.A.P. 704 | DI | MCLEAN | 497 | 273 |
| DESIGNED | | PUBLISHED | | DATE |
| | | | | 06-25-10 |

CONTRACT NO. 70514



NOTE: CJ and EJ denote Construction
Joint and Expansion Joint respectively

STATION 0+596.440 TO 0+614.000



| | |
|----------|-----|
| DESIGNED | JDA |
| CHECKED | WCT |
| DRAWN | MAC |
| CHECKED | WCT |

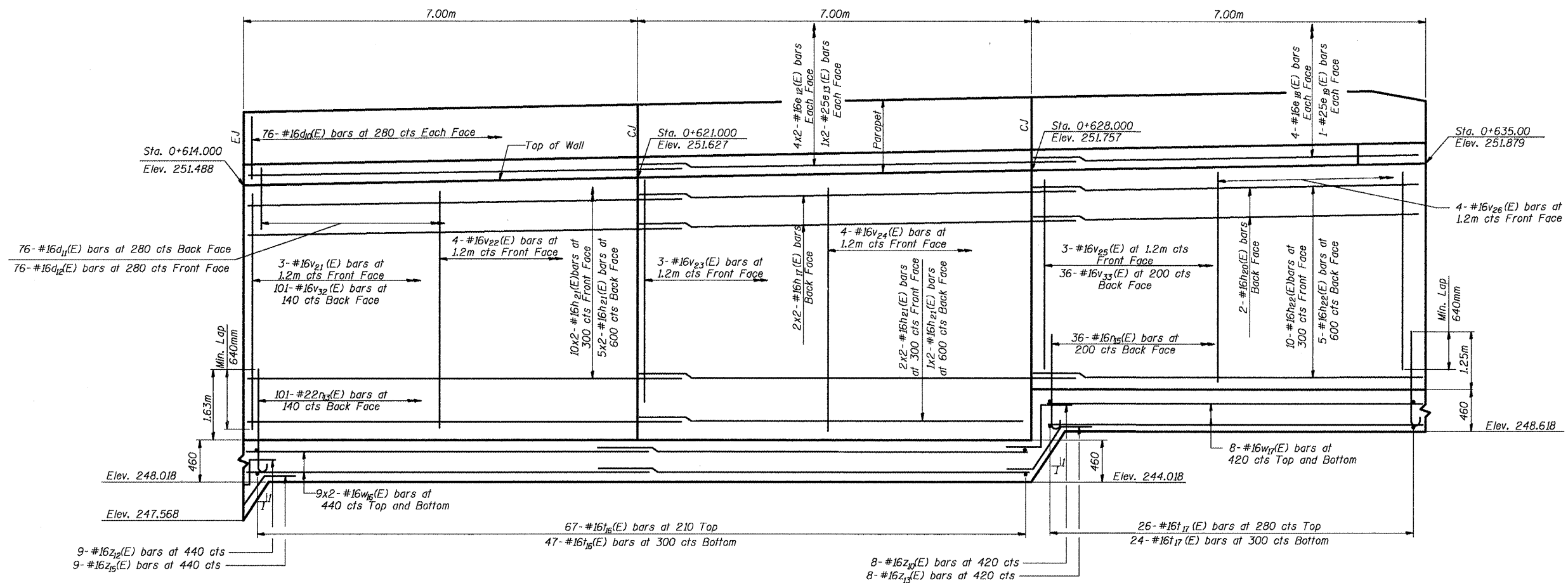
DATE: 06-25-10

ELEVATION
RETAINING WALL ALONG SIX POINTS ROAD
F.A.P. ROUTE 704 SECTION (I) N&TS-1
MCLEAN COUNTY
STATION 0+545.000 TO 0+635.000
STRUCTURE NO. 057-W001

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | | |
|-------------------|----------------|----------------|-----|---------|-------------|
| DESIGN NO. | SECTION | DATE | BY | CHECKED | SHEET NO. 5 |
| P.A.P. 704 | 01 | MCLEAN | 497 | 274 | 11 SHEETS |
| PROJECT DIST. NO. | CLASSIFICATION | PROJECT NUMBER | | | |

CONTRACT NO. 70514



NOTE: CJ and EJ denote Construction Joint and Expansion Joint respectively

STATION 0+614.000 TO 0+635.000



| | |
|----------|-----|
| DESIGNED | JDA |
| CHECKED | WCT |
| DRAWN | MAC |
| CHECKED | WCT |

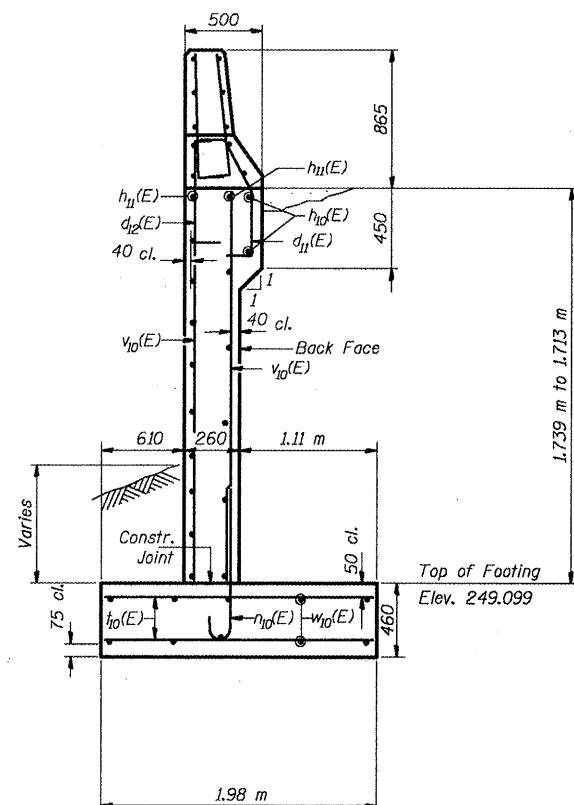
DATE: 06-25-10

ELEVATION
RETAINING WALL ALONG SIX POINTS ROAD
F.A.P. ROUTE 704 SECTION (I) N&TS-1
MCLEAN COUNTY
STATION 0+545.000 TO 0+635.000
STRUCTURE NO. 057-W001

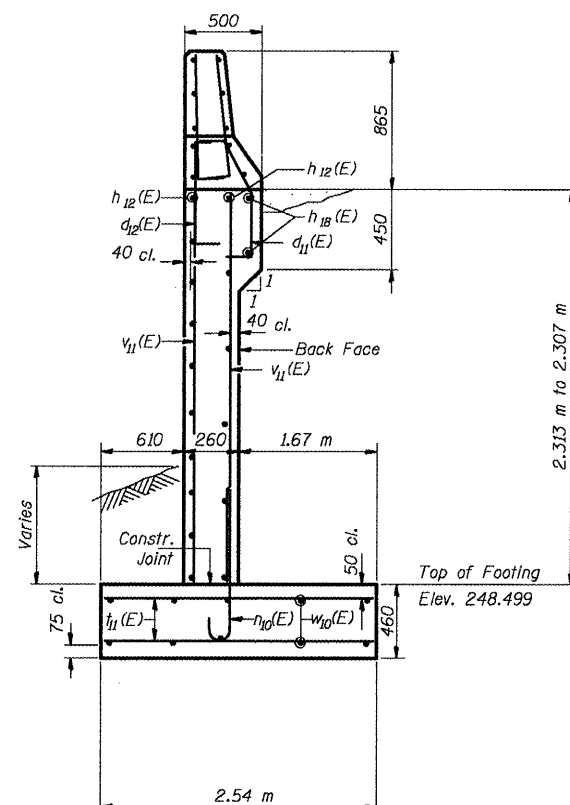
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | |
|-----------------|----------|------------------|---------|-----------|
| PROJECT NO. | DISTRICT | COUNTY | JOB NO. | SHEET NO. |
| F.A.P. 704 | 001 | McLEAN | 497 | 275 |
| PROJ. DIST. NO. | SECTION | FILE NO. PROJECT | | |

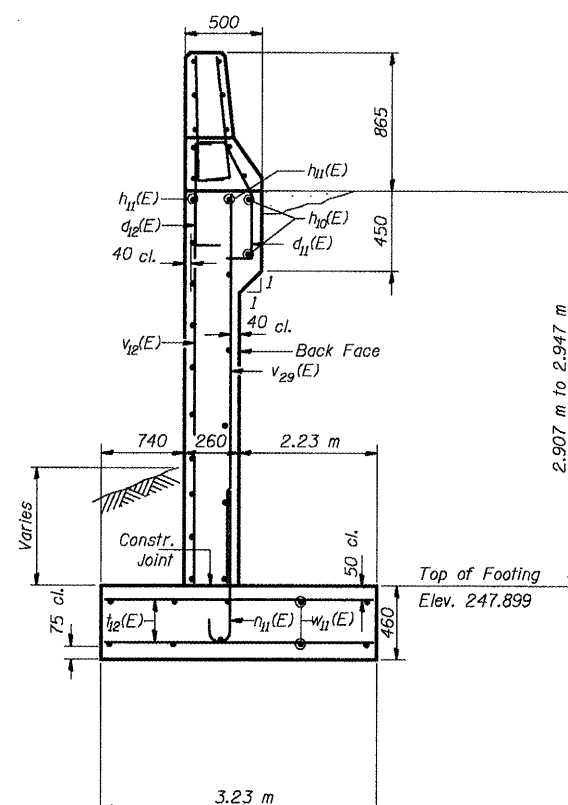
CONTRACT NO. 70514



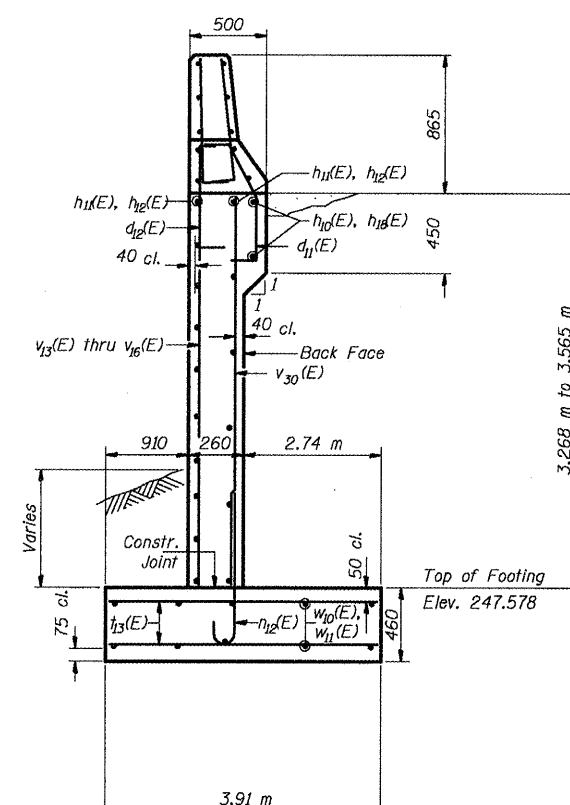
STATION 0+545.000 - 0+551.430



STATION 0+551.430 - 0+557.860



STATION 0+557.860 - 0+570.720



STATION 0+570.720 - 0+596.440



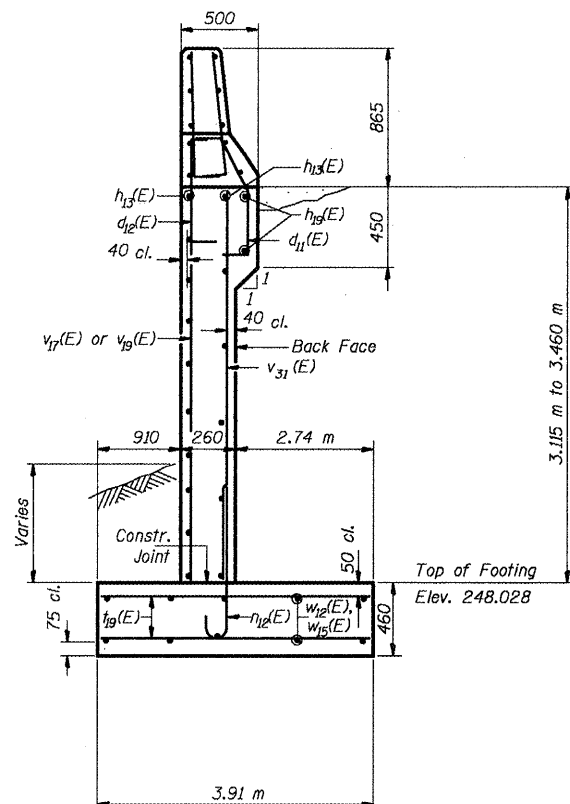
| | |
|----------|-----|
| DESIGNED | JDA |
| CHECKED | WCT |
| DRAWN | MAC |
| CHECKED | WCT |

DATE: 06-25-10

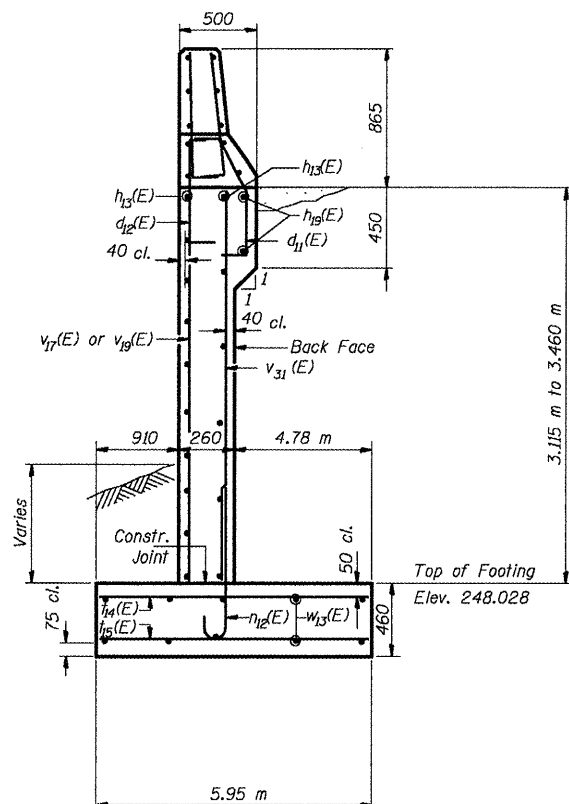
SECTIONS
RETAINING WALL ALONG SIX POINTS ROAD
F.A.P. ROUTE 704 SECTION (I) N&TS-1
McLEAN COUNTY
STATION 0+545.000 TO 0+635.000
STRUCTURE NO. 057-W001

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

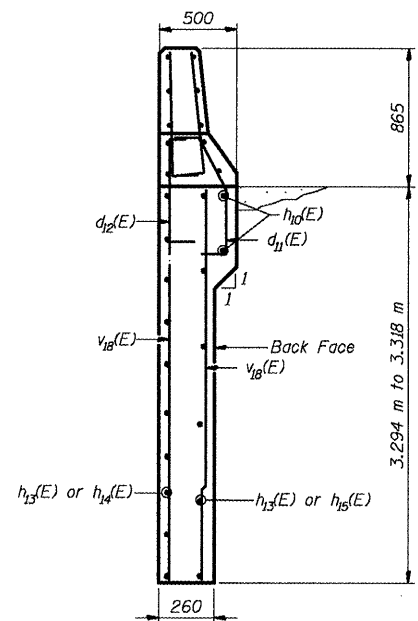
| | | | | |
|-------------------|---------|----------------|--------------------|-----------|
| DESIGN NO. | SECTION | COUNTY | DATE | SHEET NO. |
| P.A.P. 704 | DI | MCLEAN | 497 | 276 |
| PROJECT DIST. NO. | BLANKS | PROJECT NUMBER | CONTRACT NO. 70514 | |



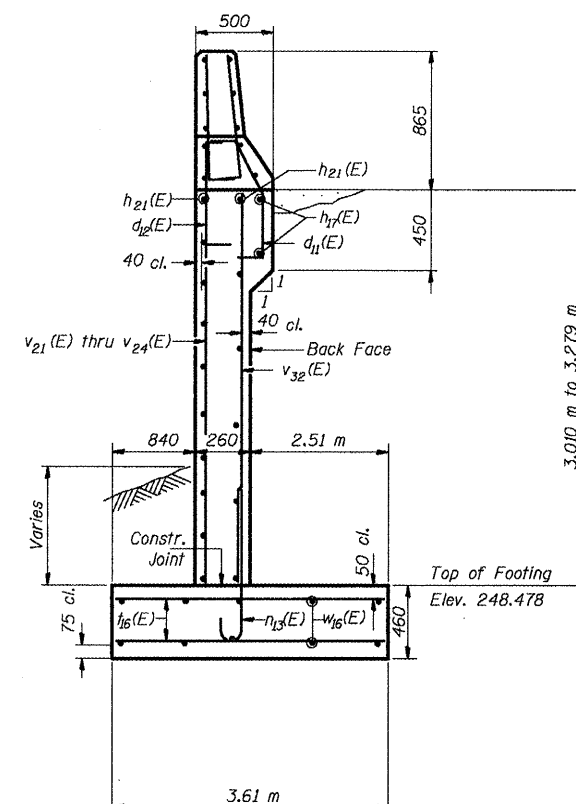
STATION 0+596.440 - 0+600.395
STATION 0+609.400 - 0+614.000



STATION 0+600.395 - 0+602.834
STATION 0+606.961 - 0+609.400



STATION 0+602.834 - 0+606.961



STATION 0+614.000 - 0+628.000

SHIVE HATTERY

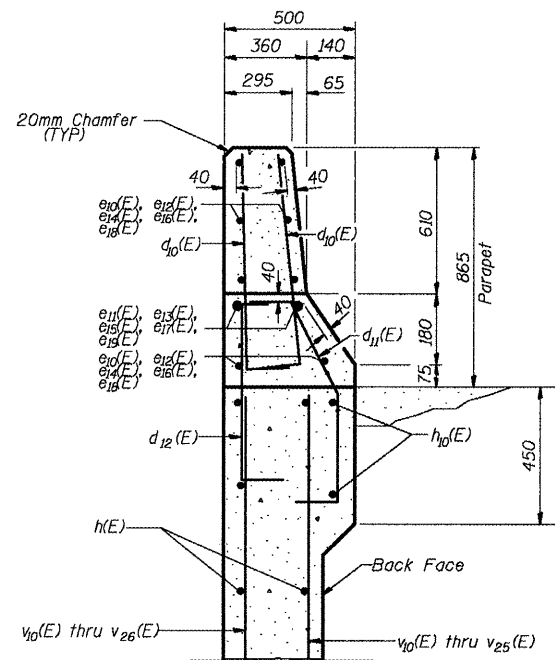
| | |
|----------|-----|
| DESIGNED | JDA |
| CHECKED | WCT |
| DRAWN | MAC |
| CHECKED | WCT |

DATE: 06-25-10

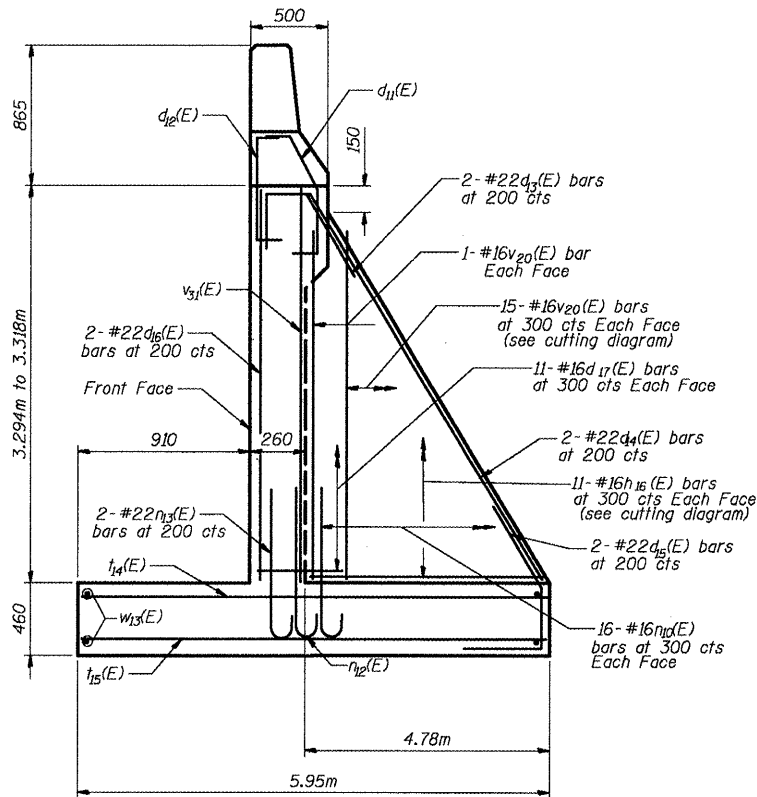
SECTIONS
RETAINING WALL ALONG SIX POINTS ROAD
F.A.P. ROUTE 704 SECTION (I) N&TS-1
MCLEAN COUNTY
STATION 0+545.000 TO 0+635.000
STRUCTURE NO. 057-W001

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

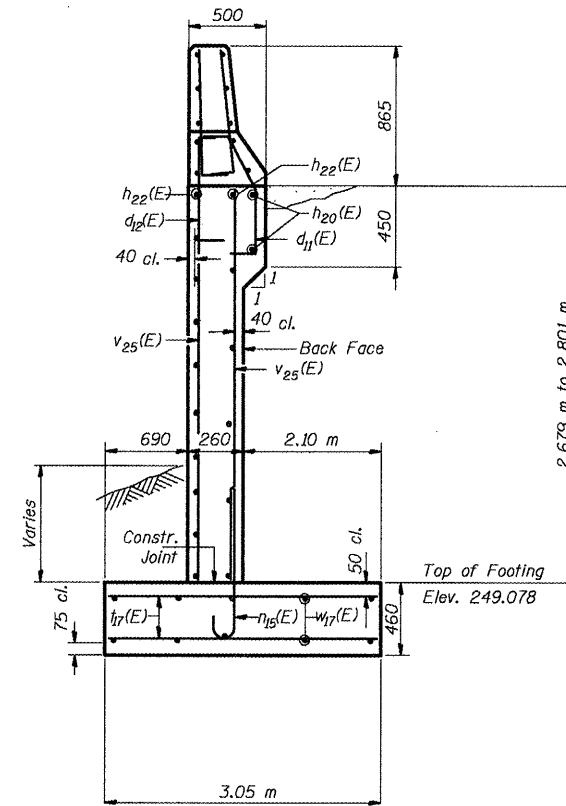
| | | | | | |
|--------------------|---------|----------|------|-----|-------------|
| DESIGN NO. | SECTION | QUANTITY | DATE | BY | SHEET NO. 8 |
| P.A.P. 704 | CDI | McLEAN | 497 | 277 | 11 SHEETS |
| CONTRACT NO. 70514 | | | | | |



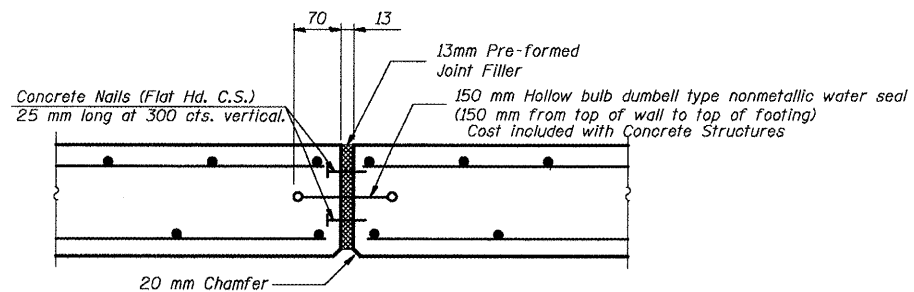
RAIL DETAIL



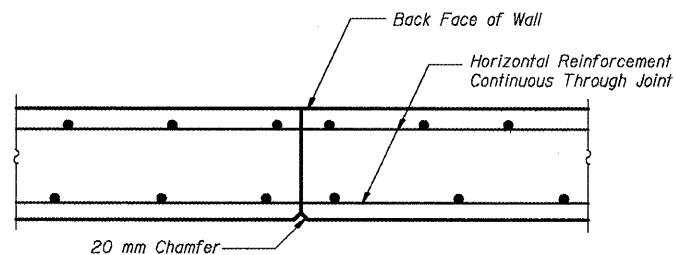
Section A



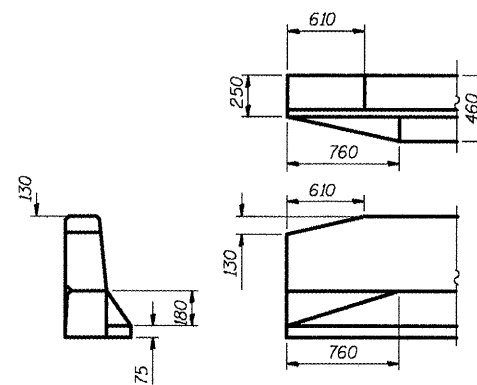
STATION 0+628.000 - 0+635.000



EXPANSION JOINT (TYPICAL)



CONSTRUCTION JOINT (TYPICAL)



PARAPET RAIL END SECTION DETAILS

SHIVE HATTERY

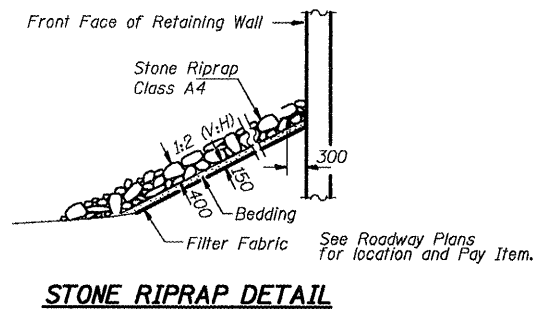
| | |
|----------|-----|
| DESIGNED | JDA |
| CHECKED | WCT |
| DRAWN | MAC |
| CHECKED | WCT |

DATE: 06-25-10

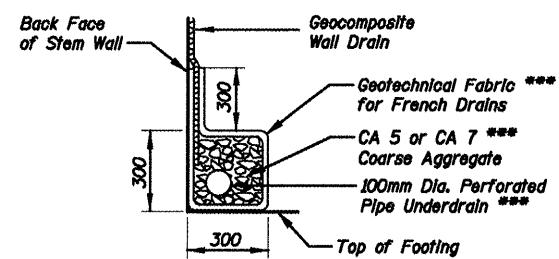
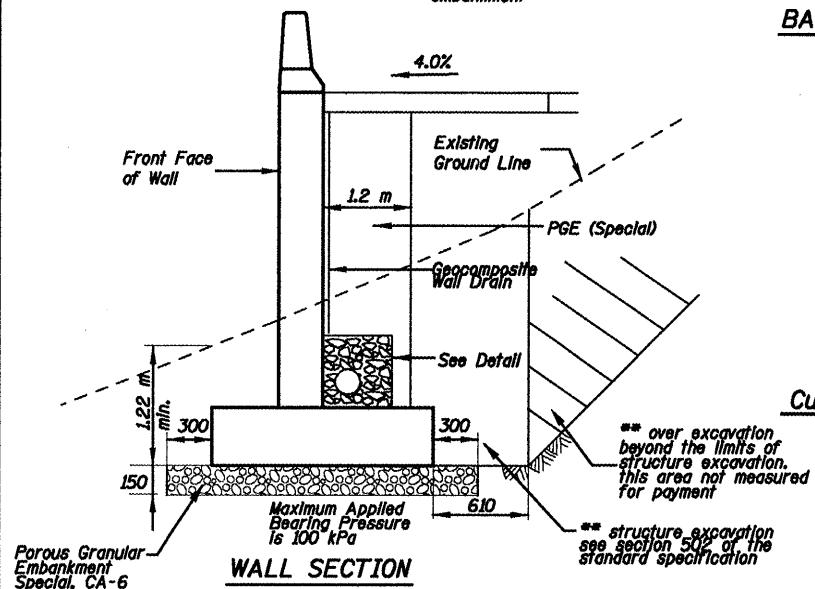
DETAILS AND SECTIONS
RETAINING WALL ALONG SIX POINTS ROAD
F.A.P. ROUTE 704 SECTION (1) N&TS-1
McLEAN COUNTY
STATION 0+545.000 TO 0+635.000
STRUCTURE NO. 057-W001

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | |
|--------------------|---------|-----------|------|-----------|
| PROJECT NO. | SECTION | QUALITY | DATE | SHEET NO. |
| 704 | CDI | McLEAN | 497 | 278 |
| SHEET NO. 9 | | 11 SHEETS | | |
| CONTRACT NO. 70514 | | | | |



** backfill remainder of structure, excavation and over excavation with same material specified for roadway embankment

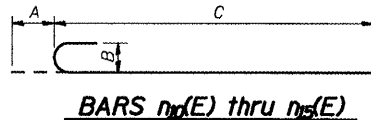
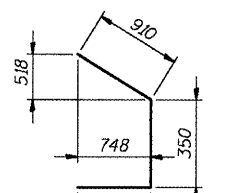
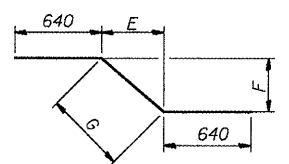
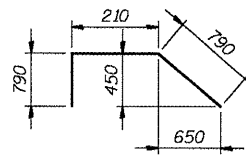
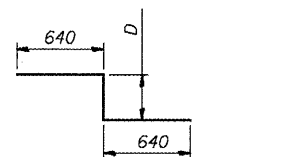
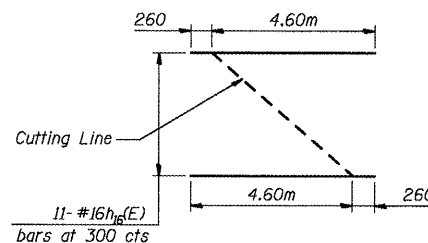
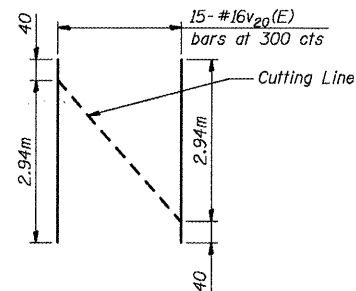
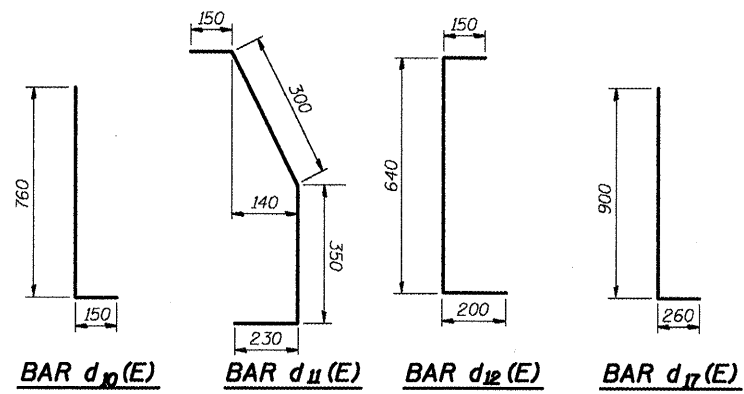


*** Cast Included with "Pipe Underdrains for Structures".

SHIVE HATTERY

| | |
|----------|-----|
| DESIGNED | JDA |
| CHECKED | WCT |
| DRAWN | MAC |
| CHECKED | WCT |

DATE: 06-25-10



BILL OF MATERIAL

| Bar | No. | Size | Length | Shape |
|-------------|-----|------|--------|-------|
| $d_{10}(E)$ | 652 | #16 | 0.910m | U |
| $d_{11}(E)$ | 326 | #16 | 1.030m | U |
| $d_{12}(E)$ | 326 | #16 | 0.990m | U |
| $d_{13}(E)$ | 4 | #22 | 1.890m | U |
| $d_{14}(E)$ | 4 | #22 | 5.740m | U |
| $d_{15}(E)$ | 4 | #22 | 2.050m | U |
| $d_{16}(E)$ | 4 | #22 | 3.210m | U |
| $d_{17}(E)$ | 44 | #16 | 1.160m | U |
| $h_{10}(E)$ | 48 | #16 | 7.070m | U |
| $h_{11}(E)$ | 12 | #25 | 7.750m | U |
| $h_{12}(E)$ | 16 | #16 | 7.640m | U |
| $h_{13}(E)$ | 4 | #25 | 8.320m | U |
| $h_{14}(E)$ | 24 | #16 | 6.350m | U |
| $h_{15}(E)$ | 6 | #25 | 6.350m | U |
| $h_{16}(E)$ | 8 | #16 | 4.770m | U |
| $h_{17}(E)$ | 2 | #25 | 5.450m | U |
| $h_{18}(E)$ | 16 | #16 | 6.920m | U |
| $h_{19}(E)$ | 4 | #25 | 6.920m | U |
| $h_{20}(E)$ | 10 | #16 | 7.070m | U |
| $h_{21}(E)$ | 83 | #16 | 7.070m | U |
| $h_{22}(E)$ | 51 | #16 | 6.350m | U |
| $h_{23}(E)$ | 54 | #16 | 6.260m | U |
| $h_{24}(E)$ | 15 | #16 | 5.710m | U |
| $h_{25}(E)$ | 12 | #16 | 3.000m | U |
| $h_{26}(E)$ | 22 | #16 | 4.860m | U |
| $h_{27}(E)$ | 4 | #16 | 7.640m | U |
| $h_{28}(E)$ | 6 | #16 | 6.350m | U |
| $h_{29}(E)$ | 6 | #16 | 6.260m | U |
| $h_{30}(E)$ | 2 | #16 | 6.920m | U |
| $h_{31}(E)$ | 36 | #16 | 7.640m | U |
| $h_{32}(E)$ | 15 | #16 | 6.920m | U |
| $n_{10}(E)$ | 117 | #16 | 1.290m | U |
| $n_{11}(E)$ | 72 | #22 | 1.890m | U |
| $n_{12}(E)$ | 199 | #25 | 2.750m | U |
| $n_{13}(E)$ | 105 | #22 | 2.270m | U |
| $n_{14}(E)$ | 36 | #16 | 1.820m | U |
| $n_{15}(E)$ | 44 | #16 | 1.900m | U |
| $n_{16}(E)$ | 46 | #16 | 2.460m | U |
| $n_{17}(E)$ | 93 | #16 | 3.150m | U |
| $n_{18}(E)$ | 298 | #16 | 3.830m | U |
| $n_{19}(E)$ | 32 | #22 | 5.870m | U |
| $n_{20}(E)$ | 18 | #16 | 5.870m | U |
| $n_{21}(E)$ | 114 | #16 | 3.530m | U |
| $n_{22}(E)$ | 50 | #16 | 2.970m | U |
| $v_{10}(E)$ | 28 | #16 | 1.670m | U |
| $v_{11}(E)$ | 37 | #16 | 2.260m | U |
| $v_{12}(E)$ | 11 | #16 | 2.860m | U |
| $v_{13}(E)$ | 6 | #16 | 3.220m | U |
| $v_{14}(E)$ | 6 | #16 | 3.270m | U |
| $v_{15}(E)$ | 5 | #16 | 3.340m | U |
| $v_{16}(E)$ | 6 | #16 | 3.420m | U |
| $v_{17}(E)$ | 3 | #16 | 3.070m | U |
| $v_{18}(E)$ | 28 | #16 | 3.210m | U |
| $v_{19}(E)$ | 3 | #16 | 3.270m | U |
| $v_{20}(E)$ | 34 | #16 | 2.980m | U |
| $v_{21}(E)$ | 3 | #16 | 2.970m | U |
| $v_{22}(E)$ | 4 | #16 | 3.040m | U |
| $v_{23}(E)$ | 3 | #16 | 3.110m | U |
| $v_{24}(E)$ | 4 | #16 | 3.170m | U |
| $v_{25}(E)$ | 3 | #16 | 2.640m | U |
| $v_{26}(E)$ | 4 | #16 | 2.700m | U |
| $v_{27}(E)$ | 3 | #16 | 3.160m | U |
| $v_{28}(E)$ | 3 | #16 | 3.350m | U |
| $v_{29}(E)$ | 72 | #16 | 2.450m | U |
| $v_{30}(E)$ | 150 | #22 | 2.770m | U |
| $v_{31}(E)$ | 69 | #22 | 2.660m | U |
| $v_{32}(E)$ | 101 | #16 | 2.400m | U |
| $v_{33}(E)$ | 36 | #16 | 2.300m | U |

BILL OF MATERIAL CONT'D

| Bar | No. | Size | Length | Shape |
|----------------------------------|-----|----------------|--------|-------|
| $w_{10}(E)$ | 26 | #16 | 6.350m | U |
| $w_{11}(E)$ | 112 | #16 | 6.890m | U |
| $w_{12}(E)$ | 20 | #16 | 4.590m | U |
| $w_{13}(E)$ | 56 | #16 | 2.360m | U |
| $w_{14}(E)$ | 20 | #16 | 5.160m | U |
| $w_{15}(E)$ | 36 | #16 | 7.280m | U |
| $w_{16}(E)$ | 16 | #16 | 6.920m | U |
| $z_{10}(E)$ | 21 | #16 | 1.880m | U |
| $z_{11}(E)$ | 8 | #16 | 1.600m | U |
| $z_{12}(E)$ | 19 | #16 | 1.730m | U |
| $z_{13}(E)$ | 21 | #16 | 2.130m | U |
| $z_{14}(E)$ | 8 | #16 | 1.730m | U |
| $z_{15}(E)$ | 19 | #16 | 1.920m | U |
| Concrete Structures | | m ³ | 258 | |
| Reinforcement Bars, Epoxy Coated | | kg | 21,190 | |

Bars Indicated thus 12 x 4-#15 etc. Indicates 12 lines of bars with 4 lengths per line.

Reinforcement bars designated (E) shall be epoxy coated. All construction joints shall be bonded.

BARS $n_{10}(E)$ thru $n_{15}(E)$ Dimensions

| | A | B | C |
|-------------|-----|-----|--------|
| $n_{10}(E)$ | 180 | 130 | 1.110m |
| $n_{11}(E)$ | 250 | 180 | 1.640m |
| $n_{12}(E)$ | 280 | 200 | 2.470m |
| $n_{13}(E)$ | 250 | 180 | 2.020m |
| $n_{14}(E)$ | 180 | 130 | 1.640m |

BARS $z_{10}(E)$ thru $z_{15}(E)$ Dimensions

| | D | E | F | G |
|-------------|-----|-----|-----|-----|
| $z_{10}(E)$ | 600 | --- | --- | --- |
| $z_{11}(E)$ | 320 | --- | --- | --- |
| $z_{12}(E)$ | 450 | --- | --- | --- |
| $z_{13}(E)$ | --- | 600 | 600 | 849 |
| $z_{14}(E)$ | --- | 320 | 320 | 454 |
| $z_{15}(E)$ | --- | 450 | 450 | 636 |

MIN. BAR LAP
#16 640mm
#25 1320mm

DETAILS
RETAINING WALL ALONG SIX POINTS ROAD
F.A.P. ROUTE 704 SECTION (I) N&TS-1
McLEAN COUNTY
STATION 0+545.000 TO 0+635.000
STRUCTURE NO. 057-W001

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | |
|---------------------|----------|------------------|-----------|--------------|
| ROUTE NO. | SECTION | COUNTY | SHEET NO. | TOTAL SHEETS |
| F.A.P. 704 | (1) | McLEAN | 497 | 280 |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

Contract No. 70514

Page 1 of 1

SOIL BORING LOG

Date 10/16/02

ROUTE FAP 704(L-568L) DESCRIPTION RETAINING WALL #1: STA 0+549 TO 0+630 LOGGED BY IDOT-LM

SECTION (1) LOCATION NE 14, SEC. 17, TWP. 23N, RNG. 2E, 3rd PM

COUNTY McLEAN DRILLING METHOD Hollow Stem Auger HAMMER TYPE AUTOMATIC

STRUCT. NO. _____
Station _____

BORING NO. 03 6 PTS. EXTEN.
Station 601+09
Offset 3.38m LT CL PROP.
Ground Surface Elev. 247.00 m (m) (mm) (kPa) (%)

| DEPTH (m) | DEPTH (ft) | SOIL DESCRIPTION | UCS (kPa) | Failure Mode | SPT (blows) |
|-----------|------------|--|-----------|--------------|-------------|
| 0.0 | 0.0 | Surface Water Elev. _____ m | | | |
| 0.0 | 0.0 | Stream Bed Elev. _____ m | | | |
| 0.0 | 0.0 | Groundwater Elev. _____ m | | | |
| 0.0 | 0.0 | First Encounter _____ m | | | |
| 0.0 | 0.0 | Upon Completion <u>243.9</u> m | | | |
| 0.0 | 0.0 | After _____ Hrs. | | | |
| 0.0 | 0.0 | 240.90 | 8 | | 16.0 |
| 0.0 | 0.0 | 21 | 287 | P | |
| 0.0 | 0.0 | 21 | | | |
| 0.0 | 0.0 | 246.24 | 3 | | |
| 0.0 | 0.0 | Very Stiff Brown SILTY CLAY LOAM (FILL) | | | |
| 0.0 | 0.0 | 246.24 | 3 | | |
| 0.0 | 0.0 | Very Stiff Brown SILTY CLAY LOAM (FILL) | | | |
| 0.0 | 0.0 | 4 | 192 | 21.0 | |
| 0.0 | 0.0 | 4 | | | |
| 0.0 | 0.0 | 239.99 | | | |
| 0.0 | 0.0 | Hard Gray SANDY CLAY LOAM TILL | | | |
| 0.0 | 0.0 | -1.6 | | | |
| 0.0 | 0.0 | 4 | | | |
| 0.0 | 0.0 | 3 | 240 | 28.0 | |
| 0.0 | 0.0 | 4 | | | |
| 0.0 | 0.0 | 245.02 | | | |
| 0.0 | 0.0 | Soft Gray SILTY LOAM to SILT | | | |
| 0.0 | 0.0 | 2 | | | |
| 0.0 | 0.0 | 2 | 48 | 26.0 | |
| 0.0 | 0.0 | 3 | | | |
| 0.0 | 0.0 | 2 | | | |
| 0.0 | 0.0 | 2 | 24 | 28.0 | |
| 0.0 | 0.0 | 4 | | | |
| 0.0 | 0.0 | 2 | | | |
| 0.0 | 0.0 | 1 | 48 | 28.0 | |
| 0.0 | 0.0 | 3 | | | |
| 0.0 | 0.0 | 242.58 | | | |
| 0.0 | 0.0 | Very Stiff Brown CLAY LOAM TILL | | | |
| 0.0 | 0.0 | 2 | | | |
| 0.0 | 0.0 | 4 | 316 | 14.0 | |
| 0.0 | 0.0 | 8 | | | |
| 0.0 | 0.0 | 241.82 | | | |
| 0.0 | 0.0 | Very Stiff Gray SILTY LOAM with SILT Ribbons | | | |
| 0.0 | 0.0 | 9 | | | |
| 0.0 | 0.0 | 14 | 268 | 18.0 | |
| 0.0 | 0.0 | 16 | | | |
| 0.0 | 0.0 | -6.0 | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T208)
BBS, from 137 (Rev. 8-99)

Page 1 of 1

SOIL BORING LOG

Date 10/16/02

ROUTE FAP 704(L-568L) DESCRIPTION RETAINING WALL #1: STA 0+549 TO 0+630 LOGGED BY IDOT-LM

SECTION (1) LOCATION NE 14, SEC. 17, TWP. 23N, RNG. 2E, 3rd PM

COUNTY McLEAN DRILLING METHOD Hollow Stem Auger HAMMER TYPE AUTOMATIC

STRUCT. NO. _____
Station _____

BORING NO. 04 6 PTS. EXTEN.
Station 624+09
Offset 3.05m LT CL PROP.
Ground Surface Elev. 248.27 m (m) (mm) (kPa) (%)

| DEPTH (m) | DEPTH (ft) | SOIL DESCRIPTION | UCS (kPa) | Failure Mode | SPT (blows) |
|-----------|------------|--|-----------|--------------|-------------|
| 0.0 | 0.0 | Surface Water Elev. _____ m | | | |
| 0.0 | 0.0 | Stream Bed Elev. _____ m | | | |
| 0.0 | 0.0 | Groundwater Elev. _____ m | | | |
| 0.0 | 0.0 | First Encounter _____ m | | | |
| 0.0 | 0.0 | Upon Completion <u>NONE</u> m | | | |
| 0.0 | 0.0 | After _____ Hrs. | | | |
| 0.0 | 0.0 | 240.90 | 4 | | 11.0 |
| 0.0 | 0.0 | 5 | 316 | B | |
| 0.0 | 0.0 | 8 | | | |
| 0.0 | 0.0 | 247.61 | | | |
| 0.0 | 0.0 | Hard Brown SILTY CLAY LOAM (FILL) | | | |
| 0.0 | 0.0 | 3 | | | |
| 0.0 | 0.0 | 4 | 431 | 17.0 | |
| 0.0 | 0.0 | 6 | | | |
| 0.0 | 0.0 | 247.00 | | | |
| 0.0 | 0.0 | Soft Gray CLAY LOAM | | | |
| 0.0 | 0.0 | -1.6 | | | |
| 0.0 | 0.0 | 3 | | | |
| 0.0 | 0.0 | 1 | 29 | 25.0 | |
| 0.0 | 0.0 | 3 | | | |
| 0.0 | 0.0 | 246.24 | | | |
| 0.0 | 0.0 | Very Stiff Brown-Gray CLAY LOAM | | | |
| 0.0 | 0.0 | 2 | | | |
| 0.0 | 0.0 | 5 | 211 | 17.0 | |
| 0.0 | 0.0 | 7 | | | |
| 0.0 | 0.0 | 245.32 | | | |
| 0.0 | 0.0 | Very Stiff Brown-Gray SANDY CLAY LOAM TILL | | | |
| 0.0 | 0.0 | 2 | | | |
| 0.0 | 0.0 | 6 | 259 | 14.0 | |
| 0.0 | 0.0 | 9 | | | |
| 0.0 | 0.0 | 244.71 | | | |
| 0.0 | 0.0 | Hard Brown-Gray SANDY CLAY LOAM TILL | | | |
| 0.0 | 0.0 | 4 | | | |
| 0.0 | 0.0 | 8 | 402 | 10.0 | |
| 0.0 | 0.0 | 12 | | | |
| 0.0 | 0.0 | 243.95 | | | |
| 0.0 | 0.0 | Very Stiff Brown SANDY CLAY LOAM TILL | | | |
| 0.0 | 0.0 | 4 | | | |
| 0.0 | 0.0 | 7 | 384 | 12.0 | |
| 0.0 | 0.0 | 12 | | | |
| 0.0 | 0.0 | -4.5 | | | |
| 0.0 | 0.0 | 240.29 | | | |
| 0.0 | 0.0 | End of Boring | | | |
| 0.0 | 0.0 | 2 | | | |
| 0.0 | 0.0 | 5 | 211 | 17.0 | |
| 0.0 | 0.0 | 7 | | | |
| 0.0 | 0.0 | 245.32 | | | |
| 0.0 | 0.0 | Very Stiff Brown-Gray SANDY CLAY LOAM TILL | | | |
| 0.0 | 0.0 | 2 | | | |
| 0.0 | 0.0 | 6 | 259 | 14.0 | |
| 0.0 | 0.0 | 9 | | | |
| 0.0 | 0.0 | 244.71 | | | |
| 0.0 | 0.0 | Hard Brown-Gray SANDY CLAY LOAM TILL | | | |
| 0.0 | 0.0 | 4 | | | |
| 0.0 | 0.0 | 8 | 402 | 10.0 | |
| 0.0 | 0.0 | 12 | | | |
| 0.0 | 0.0 | 243.95 | | | |
| 0.0 | 0.0 | Very Stiff Brown SANDY CLAY LOAM TILL | | | |
| 0.0 | 0.0 | 4 | | | |
| 0.0 | 0.0 | 7 | 384 | 12.0 | |
| 0.0 | 0.0 | 12 | | | |
| 0.0 | 0.0 | -4.5 | | | |
| 0.0 | 0.0 | 240.29 | | | |
| 0.0 | 0.0 | Very Stiff Gray SANDY CLAY LOAM TILL | | | |
| 0.0 | 0.0 | 4 | | | |
| 0.0 | 0.0 | 5 | 259 | 10.0 | |
| 0.0 | 0.0 | 9 | | | |
| 0.0 | 0.0 | 243.04 | | | |
| 0.0 | 0.0 | Very Stiff Gray SANDY CLAY LOAM TILL | | | |
| 0.0 | 0.0 | 4 | | | |
| 0.0 | 0.0 | 5 | 259 | 10.0 | |
| 0.0 | 0.0 | 9 | | | |
| 0.0 | 0.0 | -6.0 | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T208)
BBS, from 137 (Rev. 8-99)



| | |
|----------|-----|
| DESIGNED | JDA |
| CHECKED | WCT |
| DRAWN | MAC |
| CHECKED | WCT |

DATE: 05-05-06

SOIL BORINGS 2
RETAINING WALL ALONG SIX POINTS ROAD
F.A.P. ROUTE 704 SECTION (1) N&TS-1
McLEAN COUNTY
STATION 0+545.000 TO 0+635.000
STRUCTURE NO. 057-W001

BENCHMARK

Square cut in the northwest corner of a concrete intake on the south side of Greenwood Avenue East at the west edge of the mobile home park. Station 10+407.6 3.4 m Right of the proposed Greenwood Avenue East alignment. Elevation 249.710.

EXISTING STRUCTURE

No existing structure.

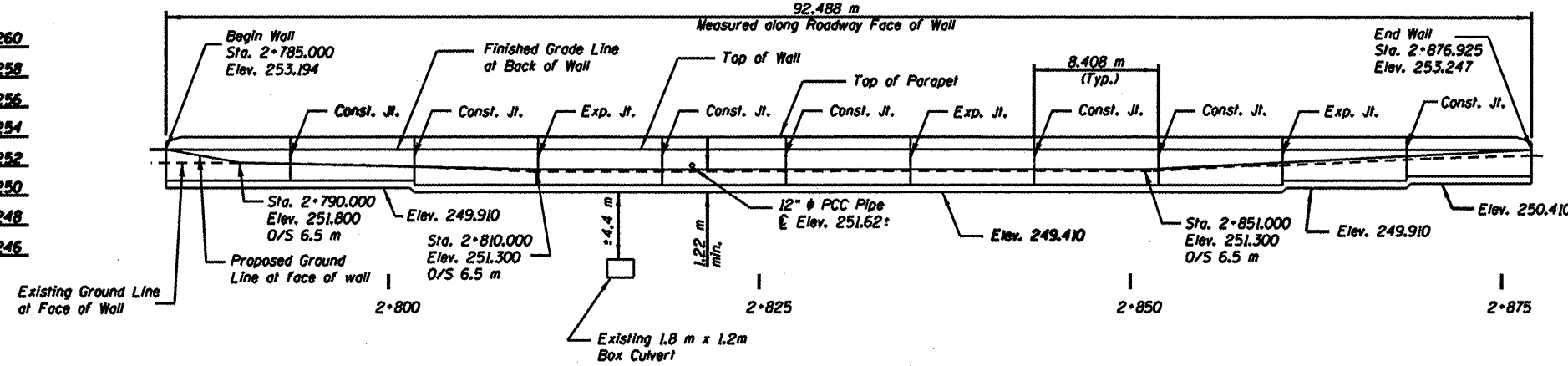
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | | |
|------------------|---------|--------|-------|-------|-------------------|
| ROUTE NO. | SECTION | COUNTY | DIST. | SHEET | SHEET NO. 1 |
| F.A.P. 704 | UI | MCLEAN | 497 | 381 | 7 SHEETS |
| ILLINOIS PROJECT | | | | | CONTRACT NO. 7054 |

TOTAL BILL OF MATERIAL

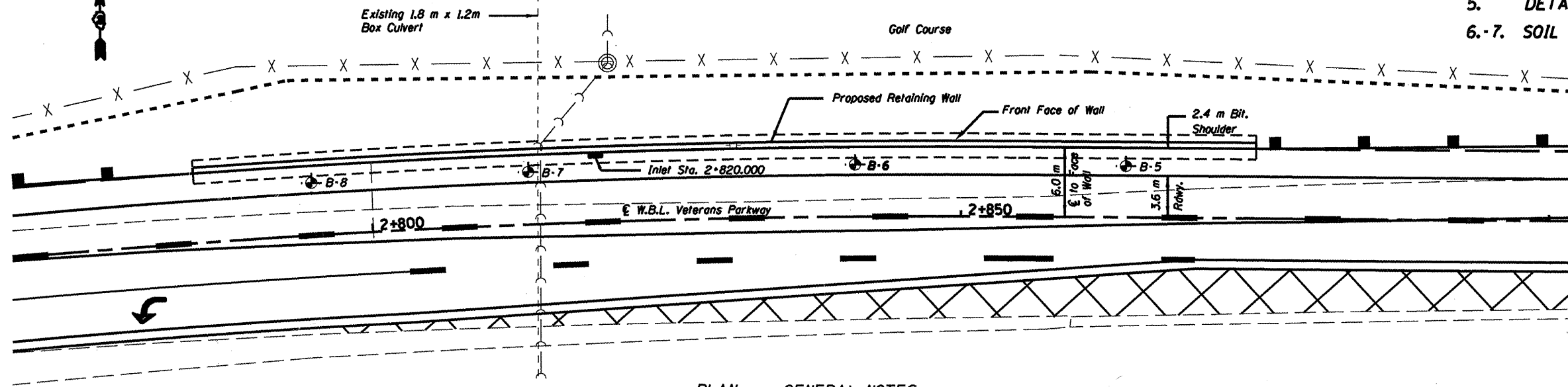
| ITEM | UNIT | TOTAL |
|--|------|--------|
| Porous Granular Embankment, Special | Cu M | 312 |
| Structure Excavation | Cu M | 908 |
| Concrete Structures | Cu M | 259 |
| Reinforcement Bars, Epoxy Coated | Kg | 20,340 |
| Geocomposite Wall Drain | Sq M | 267 |
| Pipe Under Drains for Structures, 100mm | M | 92.5 |
| Protective Coat | Sq M | 112 |
| Porous Granular Embankment, Special CA-6 | Cu M | 54 |

- 260
- 258
- 256
- 254
- 252
- 250
- 248
- 246



ELEVATION

* Top of wall to match proposed edge of shoulder elevations for widening of veterans W.B.L.



INDEX OF SHEETS

1. GENERAL PLAN
- 2.-3. ELEVATION
4. SECTIONS AND DETAILS
5. DETAILS
- 6.-7. SOIL BORINGS

CURVE DATA

F.A.P. Route 704 (I-55 BL.)
CVSB - 3
Δ = 22°45'00" (RT)
R = 882.258 m
T = 177.494 m
L = 350.312 m
E = 17.677 m
PC = 2+504.540
PI = 2+682.034
PT = 2+854.852

| Station | Top of Wall Elev. (Meters) | Top of Wall Elev. |
|-----------|----------------------------|-------------------|
| 2+785.000 | 6.500 | 253.194 |
| 2+790.000 | 6.500 | 253.202 |
| 2+800.000 | 6.500 | 253.192 |
| 2+810.000 | 6.500 | 253.157 |
| 2+820.000 | 6.500 | 253.152 |
| 2+830.000 | 6.500 | 253.125 |
| 2+840.000 | 6.500 | 253.139 |
| 2+850.000 | 6.500 | 253.185 |
| 2+860.000 | 6.500 | 253.229 |
| 2+870.000 | 6.500 | 253.236 |
| 2+877.000 | 6.500 | 253.247 |

APPROVED PLAN
For Structural Adequacy Only

Ralph E. Anderson
Engineer of Bridges & Structures

GENERAL NOTES

Protective coat shall be applied to the top and exposed inside faces of wall.
See sheet 4 of 7 for wall thru sections and details.
⊕ Denotes soil boring location

Reinforcement bars shall conform to the requirements of ASTM A 706M GR 420. See Special Provisions.

All exposed edges of the concrete facing shall be chamfered 20mm.

All dimensions are in millimeters (mm) except as noted

The footing excavation shall be cleaned of loose material and replaced with Porous Granular Embankment, Special CA-6 prior to pouring the concrete.

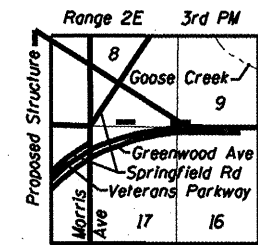
DESIGN SPECIFICATIONS

AASHTO 2002

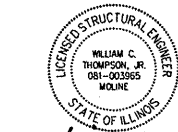
DESIGN STRESSES

FIELD UNITS

$f_c = 24 \text{ MPa}$
 $f_y = 420 \text{ MPa (Reinf.)}$



LOCATION SKETCH



Signature: *William C. Thompson, Jr.*
Date: 8/19/10
Exp. Date: 11/30/2010

GENERAL PLAN
RETAINING WALL ALONG
W.B.L. VETERANS PARKWAY
F.A.P. ROUTE 704 SECTION (I) N&TS-1
MCLEAN COUNTY
STATION 2+785.000 TO 2+877.000
STRUCTURE NO. 057-W002

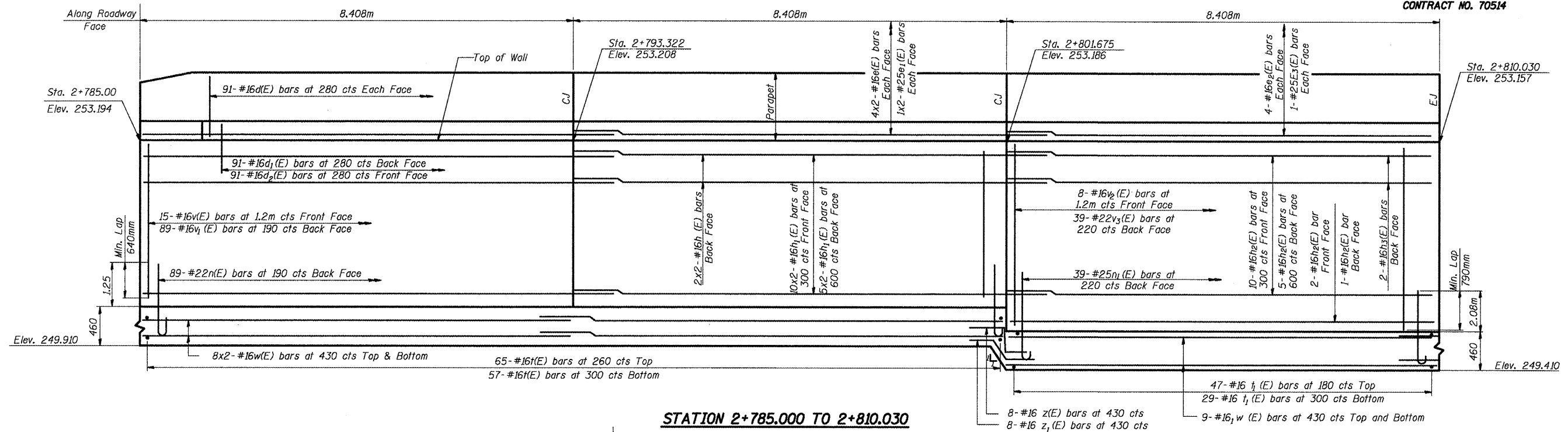
SHIVE-HATTERY

| | |
|----------|-----|
| DESIGNED | JDA |
| CHECKED | WCT |
| DRAWN | MAC |
| CHECKED | WCT |

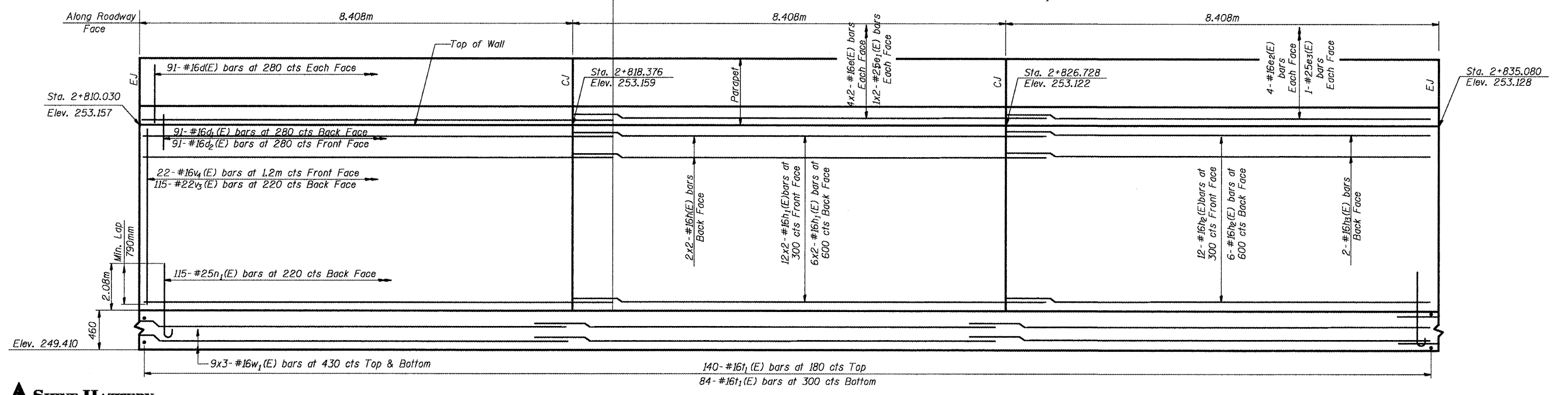
DATE: 06-25-10

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | | |
|--------------------|---------|----------|------|------------------|-------------|
| PROJECT NO. | SECTION | QUALITY | DATE | BY | SHEET NO. 2 |
| P.A.N. 704 | CDI | McLEAN | 497 | 282 | 7 SHEETS |
| FUNDING DIST. NO. | | ILLINOIS | | FED. AID PROJECT | |
| CONTRACT NO. 70514 | | | | | |



STATION 2+785.000 TO 2+810.030



STATION 2+810.030 TO 2+835.080

SHIVE HATTERY

| | |
|----------|-----|
| DESIGNED | Jda |
| CHECKED | WCT |
| DRAWN | mac |
| CHECKED | WCT |

DATE: 06-25-10

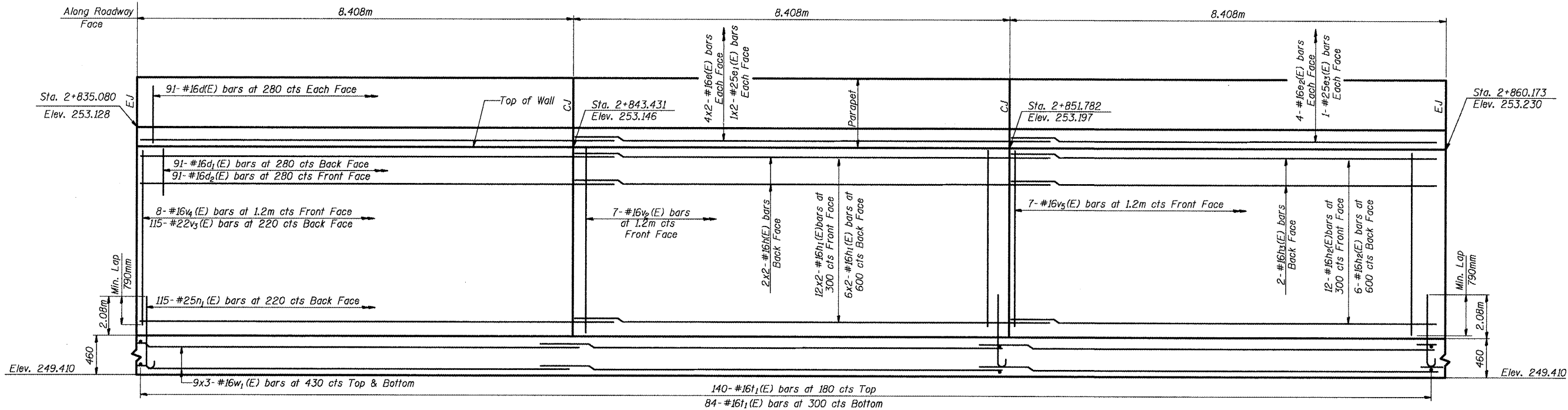
NOTE: CJ and EJ denote Construction Joint and Expansion Joint respectively

ELEVATION
RETAINING WALL ALONG
W.B.L. VETERANS PARKWAY
F.A.P. ROUTE 704 SECTION (I) N&TS-1
McLEAN COUNTY
STATION 2+785.000 TO 2+877.000
STRUCTURE NO. 057-W002

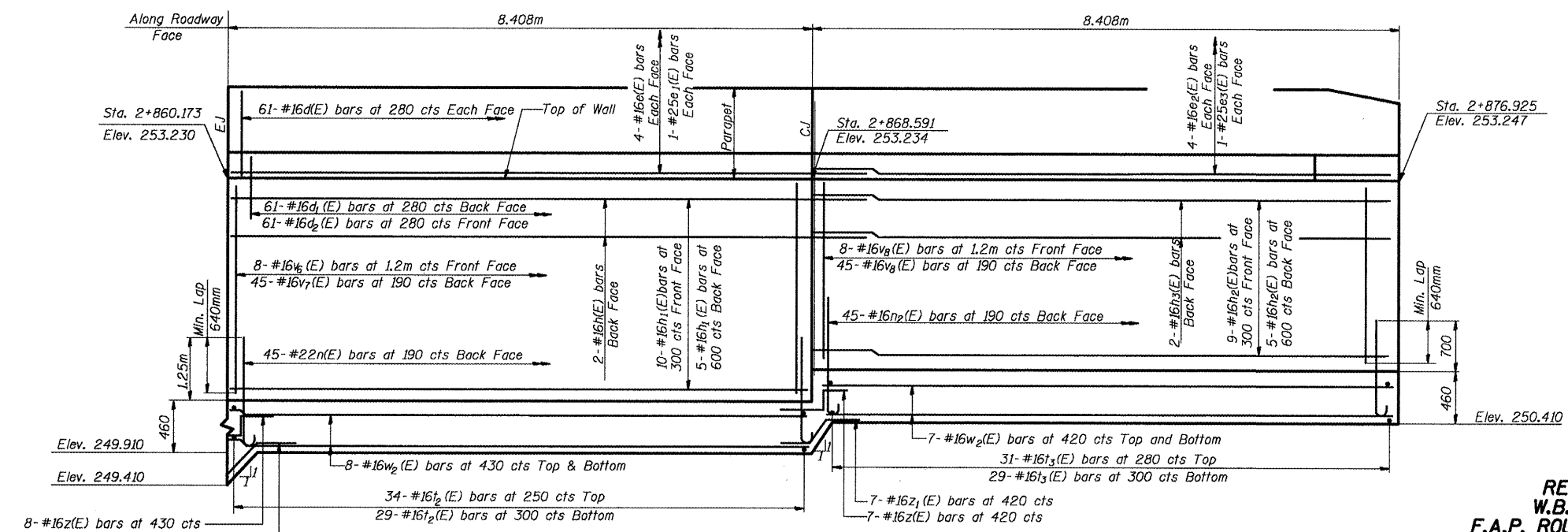
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | |
|-------------|----------|----------|---------|-----------|
| PROJECT NO. | DISTRICT | COUNTY | SECTION | SHEET NO. |
| 704 | CDI | McLEAN | 497 | 283 |
| SHEET NO. 3 | | 7 SHEETS | | |

CONTRACT NO. 70514



STATION 2+835.080 TO 2+860.173



STATION 2+860.125 TO 2+876.925



| | |
|----------|-----|
| DESIGNED | Jda |
| CHECKED | WCT |
| DRAWN | mac |
| CHECKED | WCT |

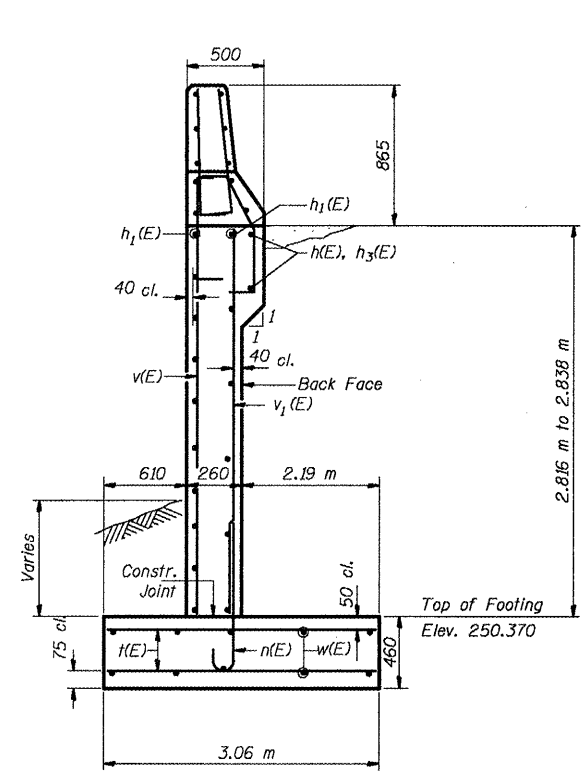
DATE: 06-25-10

NOTE: CJ and EJ denote Construction Joint and Expansion Joint respectively

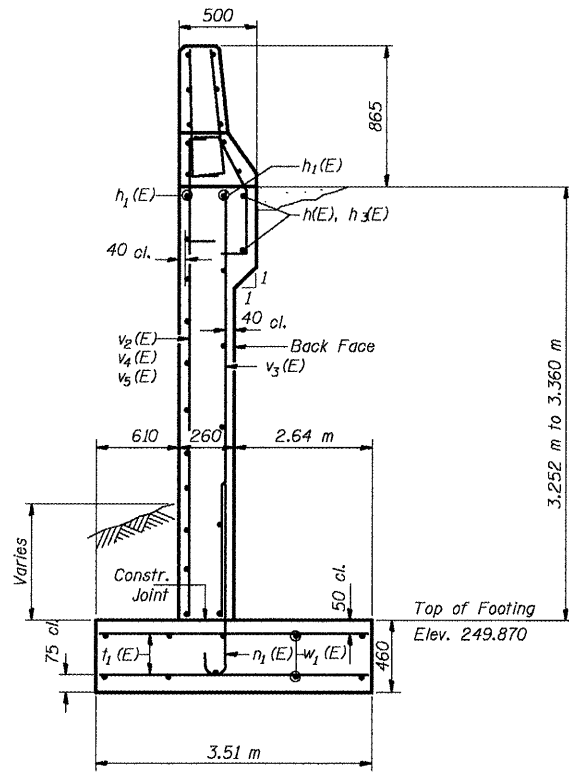
ELEVATION
RETAINING WALL ALONG
W.B.L. VETERANS PARKWAY
F.A.P. ROUTE 704 SECTION (I) N&TS-1
McLEAN COUNTY
STATION 2+785.000 TO 2+877.000
STRUCTURE NO. 057-W002

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

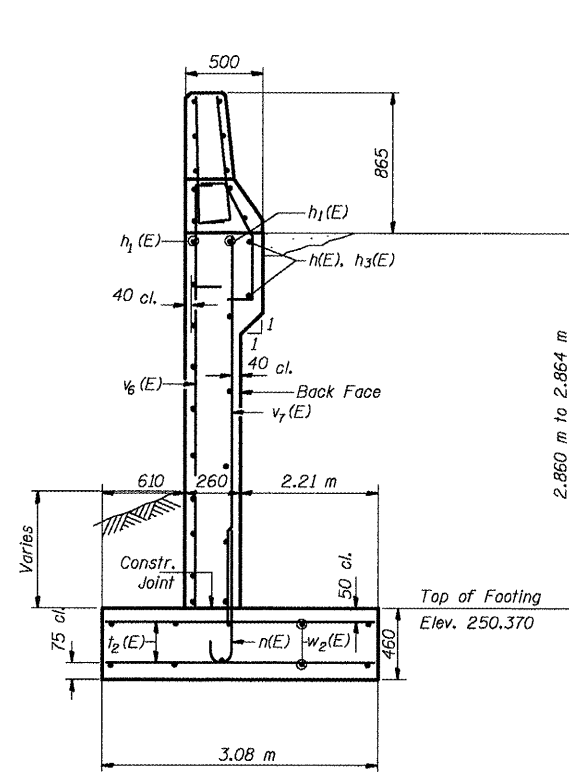
| | | | | | |
|--------------------|---------|----------|------|-----|-------------|
| ROUTE NO. | SECTION | QUANTITY | DATE | BY | SHEET NO. 4 |
| 704 | 01 | McLEAN | 497 | 294 | 7 SHEETS |
| CONTRACT NO. 70514 | | | | | |



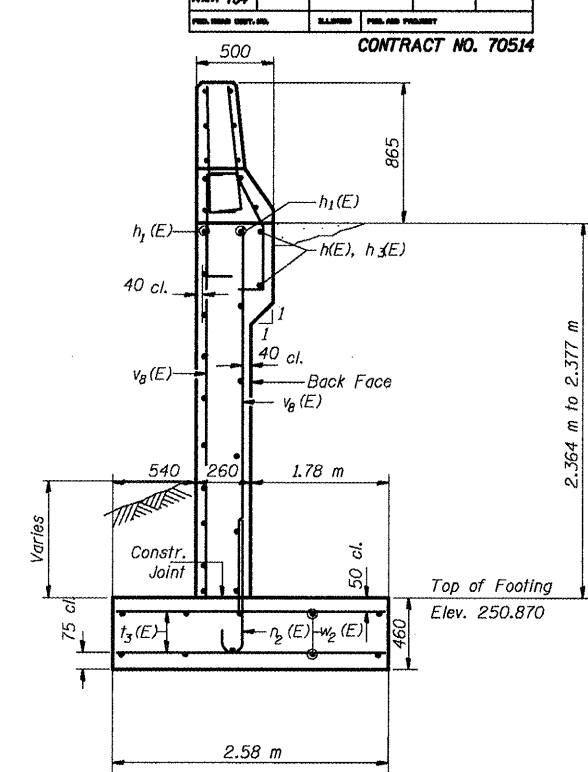
STATION 2+785.000 - 2+801.675



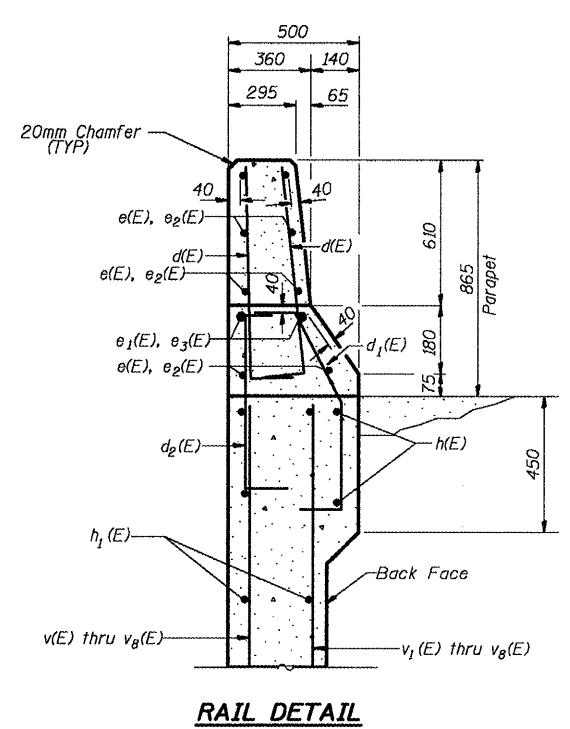
STATION 2+801.675 - 2+860.173



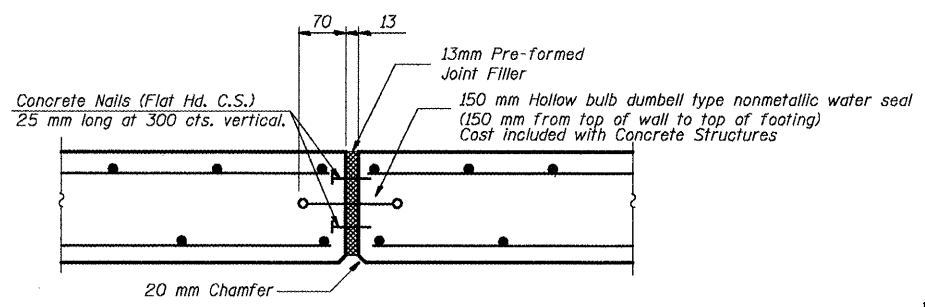
STATION 2+860.173 - 2+868.591



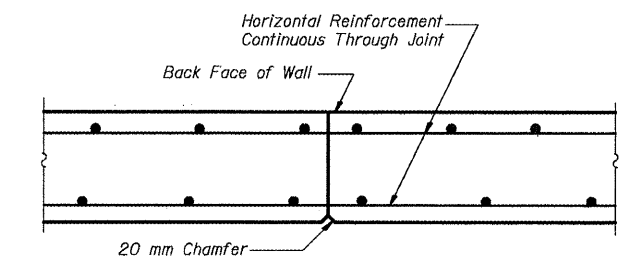
STATION 2+868.591 - 2+876.925



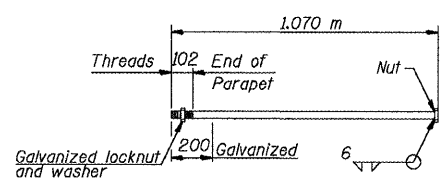
RAIL DETAIL



EXPANSION JOINT (TYPICAL)

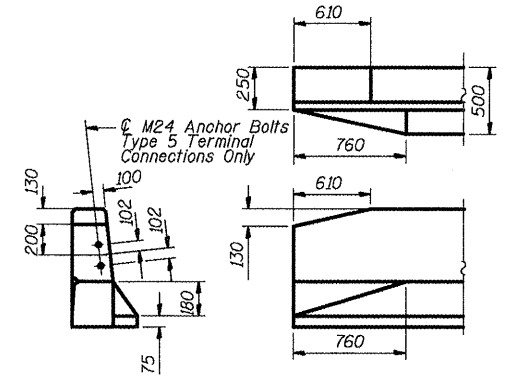


CONSTRUCTION JOINT (TYPICAL)



M24 ANCHOR BOLT
(Cost included with Concrete Structures)

Type 5 terminal connections only. West end of retaining wall. see Roadway Plans.



PARAPET RAIL END SECTION DETAILS

SECTIONS AND DETAILS
RETAINING WALL ALONG
W.B.L. VETERANS PARKWAY
F.A.P. ROUTE 704 SECTION (1) N&TS-1
McLEAN COUNTY
STATION 2+785.000 TO 2+877.000
STRUCTURE NO. 057-W002



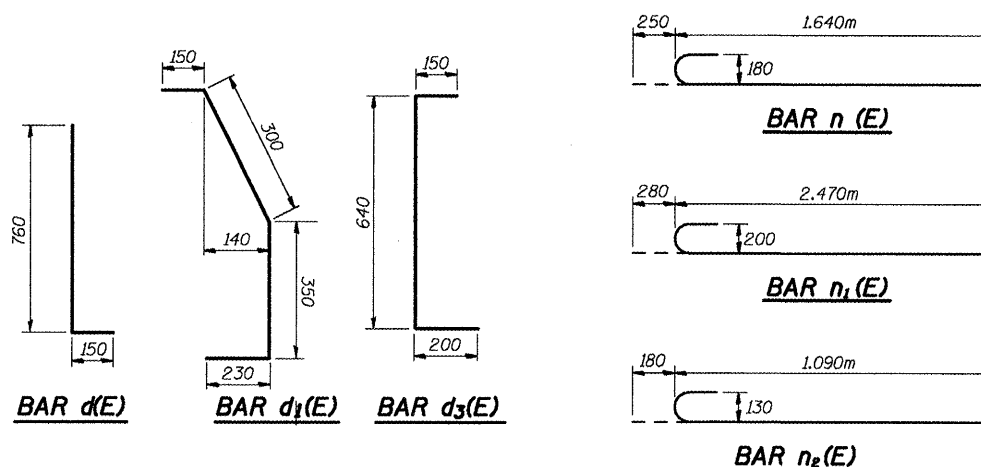
| | |
|----------|-----|
| DESIGNED | Jda |
| CHECKED | WCT |
| DRAWN | mac |
| CHECKED | WCT |

DATE: 06-25-10

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

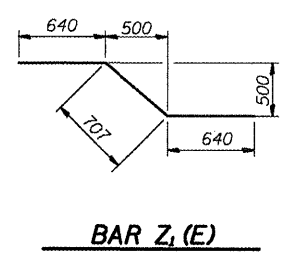
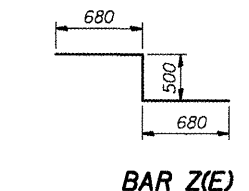
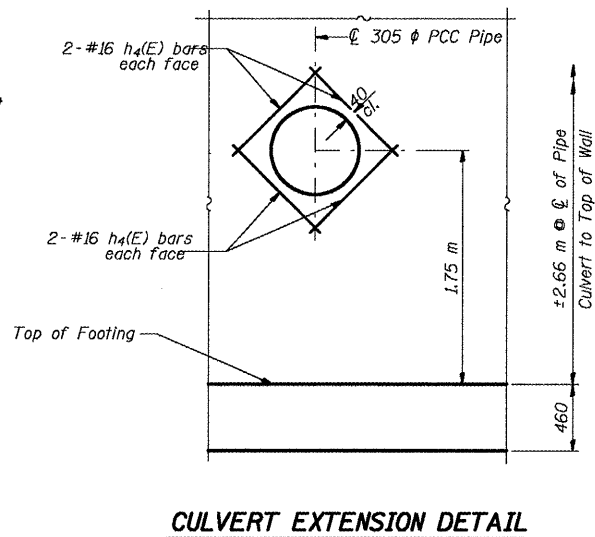
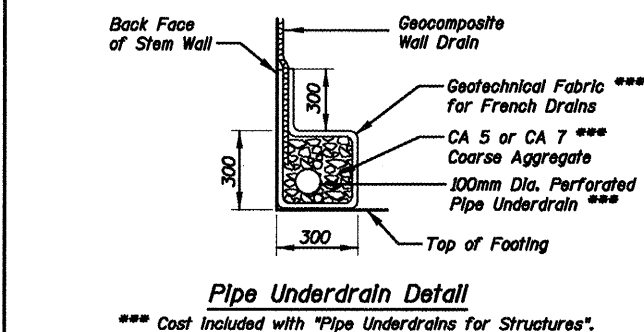
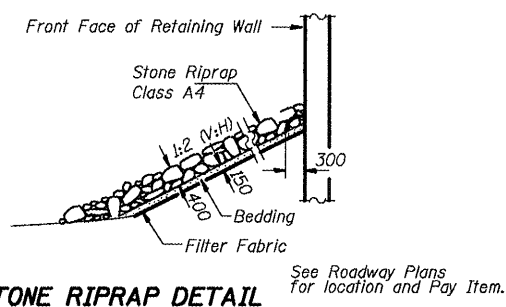
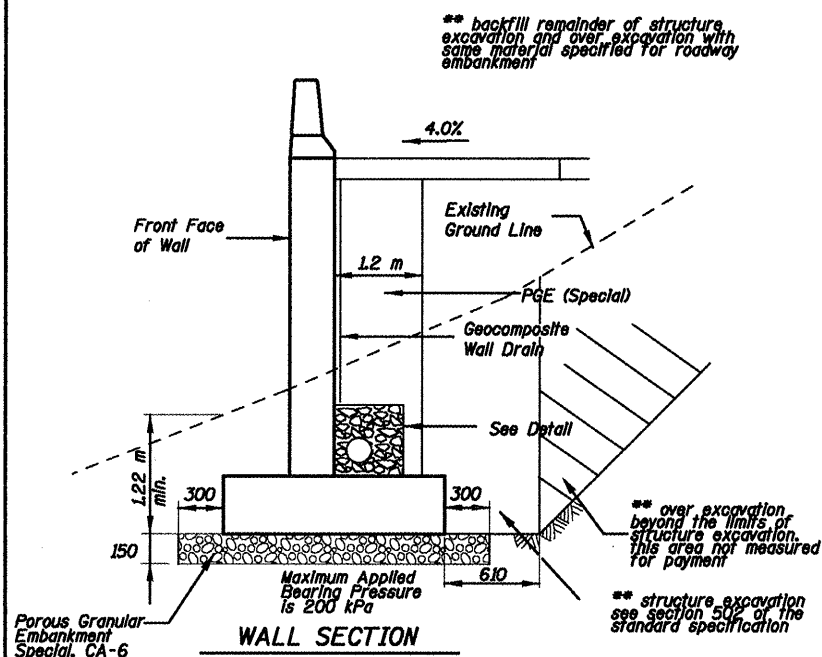
| | | | |
|-----------|----------|--------------|-----------|
| ROUTE NO. | POSTMILE | CONTRACT NO. | SHEET NO. |
| 704 | 071 | 70514 | 5 |
| DRAWN BY | | DATE | 7 SHEETS |
| MCM | | 06-25-10 | |

CONTRACT NO. 70514



BILL OF MATERIAL

| Bar | No. | Size | Length | Shape |
|----------------------------------|-----|----------------|--------|-------|
| d(E) | 668 | #16 | 0.910m | L |
| d1(E) | 334 | #16 | 1.030m | L |
| d2(E) | 334 | #16 | 0.990m | L |
| e(E) | 56 | #16 | 9.050m | — |
| e1(E) | 14 | #25 | 9.730m | — |
| e2(E) | 32 | #16 | 8.330m | — |
| e3(E) | 8 | #25 | 8.330m | — |
| h(E) | 14 | #16 | 9.050m | — |
| h1(E) | 117 | #16 | 9.050m | — |
| h2(E) | 68 | #16 | 8.330m | — |
| h3(E) | 8 | #16 | 8.330m | — |
| h4(E) | 8 | #16 | 0.790m | — |
| n(E) | 134 | #22 | 1.890m | U |
| n1(E) | 269 | #25 | 2.750m | U |
| n2(E) | 45 | #16 | 1.270m | U |
| t(E) | 122 | #16 | 2.98m | — |
| t1(E) | 524 | #16 | 3.43m | — |
| t2(E) | 63 | #16 | 3.00m | — |
| t3(E) | 60 | #16 | 2.50m | — |
| v(E) | 15 | #16 | 2.780m | — |
| v1(E) | 89 | #16 | 2.340m | — |
| v2(E) | 15 | #16 | 3.240m | — |
| v3(E) | 269 | #22 | 2.560m | — |
| v4(E) | 30 | #16 | 3.210m | — |
| v5(E) | 7 | #16 | 3.290m | — |
| v6(E) | 8 | #16 | 2.820m | — |
| v7(E) | 45 | #16 | 2.370m | — |
| v8(E) | 53 | #16 | 2.320m | — |
| w(E) | 32 | #16 | 8.690m | — |
| w1(E) | 126 | #16 | 8.950m | — |
| w2(E) | 30 | #16 | 8.330m | — |
| z(E) | 23 | #16 | 1.860m | L |
| z1(E) | 23 | #16 | 1.990m | L |
| Concrete Structures | | m ³ | 259 | |
| Reinforcement Bars, Epoxy Coated | | kg | 20,350 | |



MIN. BAR LAP

| | |
|-----|--------|
| #16 | 640mm |
| #25 | 1.320m |

Bars indicated thus 12 x 4-#15 etc. indicates 12 lines of bars with 4 lengths per line.

Reinforcement bars designated (E) shall be epoxy coated. All construction joints shall be bonded.

SHIVE HATTERY

| | |
|----------|-----|
| DESIGNED | Jda |
| CHECKED | WCT |
| DRAWN | mac |
| CHECKED | WCT |

DATE: 06-25-10

DETAILS
RETAINING WALL ALONG
W.B.L. VETERANS PARKWAY
F.A.P. ROUTE 704 SECTION (D) N&TS-1
McLEAN COUNTY
STATION 2+785.000 TO 2+877.000
STRUCTURE NO. 057-W002

| | | | | |
|---------------------|------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (DN & TS-1 | McLEAN | 497 | 288 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

Illinois Department of Transportation
Division of Highways
Illinois Department of Transportation

SOIL BORING LOG

Page 1 of 1
Date 1/7/02

ROUTE FAP 704(I-55BL) DESCRIPTION MAST ARM FOUNDATION AT SIX POINTS ROAD & MORRIS AVENUE IN BLOOMINGTON LOGGED BY K.W.

SECTION (11) SIX POINTS ROAD LOCATION NE 14, SEC. 17, TWP. 23N, RNG. 2E, 3rd PM

COUNTY MCLEAN DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

STRUCT. NO. _____
Station _____

BORING NO. 1 NW QUAD.
Station _____
Offset _____
Ground Surface Elev. _____ m (m) (mm) (kPa) (%)

| DEPTHS | SOIL DESCRIPTION | Surface Water Elev. m | Stream Bed Elev. m | Groundwater Elev. Upon Completion After Hrs. | First Encounter m | UPON COMPLETION | After Hrs. |
|--------|---------------------------------------|-----------------------|--------------------|--|-------------------|-----------------|------------|
| 5 | Stiff Gray LOAM TILL (continued) | | | | | 144 | 11.0 |
| 7 | | | | | | S | |
| 9 | | | | | | | |
| 8 | Hard Brown SILTY CLAY LOAM TILL | | | | | 240 | 14.0 |
| 12 | | | | | | B | |
| -1.5 | | | | | | | |
| 5 | Very Stiff Brown SILTY CLAY LOAM TILL | | | | | 211 | 14.0 |
| 8 | | | | | | B | |
| 9 | | | | | | | |
| 5 | Very Stiff Gray SILTY CLAY LOAM TILL | | | | | 240 | 12.0 |
| 8 | | | | | | B | |
| 7 | | | | | | | |
| 4 | Hard Gray SILTY CLAY LOAM TILL | | | | | 431 | 13.0 |
| 6 | | | | | | B | |
| 7 | | | | | | | |
| 8 | | | | | | | |
| 9 | | | | | | | |
| -8.0 | | | | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)

Illinois Department of Transportation
Division of Highways
Illinois Department of Transportation

SOIL BORING LOG

Page 1 of 1
Date 1/7/02

ROUTE FAP 704(I-55BL) DESCRIPTION MAST ARM FOUNDATION AT SIX POINTS ROAD & MORRIS AVENUE LOGGED BY K.W.

SECTION (11) SIX POINTS ROAD LOCATION NE 14, SEC. 17, TWP. 23N, RNG. 2E, 3rd PM

COUNTY MCLEAN DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

STRUCT. NO. _____
Station _____

BORING NO. 2 SW QUAD.
Station _____
Offset _____
Ground Surface Elev. _____ m (m) (mm) (kPa) (%)

| DEPTHS | SOIL DESCRIPTION | Surface Water Elev. m | Stream Bed Elev. m | Groundwater Elev. Upon Completion After Hrs. | First Encounter m | UPON COMPLETION | After Hrs. |
|--------|--|-----------------------|--------------------|--|-------------------|-----------------|------------|
| 5 | Stiff Gray SILTY LOAM TILL (continued) | | | | | 584 | 14.0 |
| 6 | | | | | | S | |
| 7 | | | | | | | |
| 10 | | | | | | | |
| 4 | Mix of Stiff Brown SILTY CLAY LOAM TILL, GRAVEL & Old BITUMINOUS CONCRETE (FILL) | | | | | 240 | 13.0 |
| 9 | | | | | | B | |
| 16 | | | | | | | |
| -1.5 | | | | | | | |
| 3 | Stiff to Very Stiff Brown SILTY CLAY LOAM TILL | | | | | 335 | 13.0 |
| 4 | | | | | | S | |
| 7 | | | | | | | |
| 3 | Hard Brown SILTY CLAY LOAM TILL | | | | | 422 | 14.0 |
| 7 | | | | | | B | |
| 8 | | | | | | | |
| 4 | | | | | | 389 | 14.0 |
| 6 | | | | | | B | |
| 8 | | | | | | | |
| -4.6 | | | | | | | |
| 5 | Stiff Gray SILTY LOAM TILL | | | | | 211 | 13.0 |
| 5 | | | | | | B | |
| 7 | | | | | | | |
| 3 | | | | | | 172 | 14.0 |
| 6 | | | | | | B | |
| 8 | | | | | | | |
| -6.0 | | | | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)

Illinois Department of Transportation
Division of Highways
Illinois Department of Transportation

SOIL BORING LOG

Page 1 of 1
Date 1/7/02

ROUTE FAP 704(I-55BL) DESCRIPTION MAST ARM FOUNDATION AT SIX POINTS ROAD & MORRIS AVENUE LOGGED BY K.W.

SECTION (11) SIX POINTS ROAD LOCATION NE 14, SEC. 17, TWP. 23N, RNG. 2E, 3rd PM

COUNTY MCLEAN DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

STRUCT. NO. _____
Station _____

BORING NO. 3 NE QUAD.
Station _____
Offset _____
Ground Surface Elev. _____ m (m) (mm) (kPa) (%)

| DEPTHS | SOIL DESCRIPTION | Surface Water Elev. m | Stream Bed Elev. m | Groundwater Elev. Upon Completion After Hrs. | First Encounter m | UPON COMPLETION | After Hrs. |
|--------|--|-----------------------|--------------------|--|-------------------|-----------------|------------|
| 4 | Very Stiff Gray SILTY CLAY LOAM TILL (continued) | | | | | 318 | 13.0 |
| 5 | | | | | | B | |
| 7 | | | | | | | |
| 3 | Hard Brown SILTY CLAY LOAM TILL | | | | | 259 | 14.0 |
| 7 | | | | | | B | |
| 8 | | | | | | | |
| -1.5 | | | | | | | |
| 3 | Very Stiff Gray SILTY CLAY LOAM TILL | | | | | 383 | 15.0 |
| 4 | | | | | | B | |
| 6 | | | | | | 402 | 15.0 |
| 7 | | | | | | B | |
| 8 | | | | | | | |
| 9 | | | | | | | |
| 4 | | | | | | 656 | 14.0 |
| 7 | | | | | | B | |
| 9 | | | | | | | |
| 6 | Very Stiff Gray SILTY CLAY LOAM TILL | | | | | 297 | 13.0 |
| 5 | | | | | | B | |
| 7 | | | | | | | |
| -4.6 | | | | | | | |
| 3 | | | | | | 259 | 14.0 |
| 4 | | | | | | B | |
| 7 | | | | | | | |
| 4 | | | | | | 201 | 14.0 |
| 4 | | | | | | B | |
| 7 | | | | | | | |
| -6.0 | | | | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|--|
| NAME | DATE | |
| | | BORING LOGS SIX POINT ROAD TRAFFIC SIGNAL |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |

1 OF 10
DATE 10-16-06
DRAWN BY
CHECKED BY PMH

SIX POINT ROAD TRAFFIC SIGNAL, BORING LOGS

| | | | | |
|---------------------|------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (DN & TS-1 | McLEAN | 497 | 292 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

Page 1 of 1

Illinois Department of Transportation
Division of Highways
Soil Boring Log

SOIL BORING LOG

Date 12202

ROUTE FAP 704(I-55BL) DESCRIPTION BOX CULVERT ON SIX POINTS ROAD RECONSTRUCTION IN BLOOMINGTON LOGGED BY B.W.

SECTION (11) (SIX POINTS ROAD) LOCATION NE 14, SEC. 17, TWP. 23N, RNG. 2E, 3rd PM

COUNTY McLEAN DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

STRUCT. NO. _____
Station _____

BORING NO. 1 WBL
Station 0+285.5
Offset 3.05m LT
Ground Surface Elev. 244.48 m (m) (mm) (kPa) (%)

| SOIL DESCRIPTION | DEPTH (m) | DEPTH (mm) | UCS (kPa) | Failure Mode (%) | SOIL DESCRIPTION | DEPTH (m) | DEPTH (mm) | UCS (kPa) | Failure Mode (%) |
|--|-----------|------------|-----------|------------------|--|-----------|------------|-----------|------------------|
| AUGERED BITUMINOUS PAVEMENT Over CA06 GRAVEL BASE Over Brown SANDY LOAM TILL (FILL) | | | | | Very Stiff Gray CLAY LOAM TILL (continued) | 2 | | 192 | 13.0 |
| | | | | | | 7 | | B | |
| Stiff Brown SANDY LOAM TILL Mixed with Some CA06 (FILL) | 243.72 | 11 | | | | 2 | | | |
| A-3 Road Surface Over CA06 GRAVEL BASE | 243.57 | 11 | 4.0 | | | 6 | 192 | 13.0 | |
| | | | | | | 8 | | B | |
| Soft Greenish Gray CLAY LOAM TILL Weathered & Reworked with Pieces of Brown Till in Stream Valley Colluvial Material | 243.11 | -1.5 | | | | 3 | | | |
| | | | | | | 7 | 345 | 12.0 | |
| | | | | | | 8 | | B | |
| | | | | | | 1 | | | |
| | | | | | | 2 | 29 | 25.0 | |
| | | | | | | 2 | | S | |
| Stiff Mottled Brown Gray SILTY CLAY LOAM with Black & Brown 5mm SAND Inclusions in Colluvial Material | 241.58 | -3.0 | | | | 3 | | | |
| | | | | | | 1 | 125 | 25.0 | |
| | | | | | | 2 | | B | |
| Soft Brown CLAY LOAM TILL with Wet SAND Pockets | 240.82 | | | | End of Boring | | | | |
| | | | | | | 2 | 29 | 22.0 | |
| | | | | | | 1 | | P | |
| Stiff Brown CLAY LOAM TILL | 240.06 | -4.5 | | | | 3 | 125 | 15.0 | |
| | | | | | | 4 | | B | |
| Very Stiff Brown CLAY LOAM TILL | 239.30 | | | | | 1 | | | |
| | | | | | | 3 | 230 | 14.0 | |
| | | | | | | 5 | | B | |
| | | | | | | 238.54 | -6.0 | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)

Page 1 of 1

Illinois Department of Transportation
Division of Highways
Soil Boring Log

SOIL BORING LOG

Date 32103

ROUTE FAP 704(I-55BL) DESCRIPTION SPRINGFIELD ROAD RELOCATION TO SIX POINTS ROAD LOGGED BY _____

SECTION (11) LOCATION SEC. TWP. RNG.

COUNTY McLEAN DRILLING METHOD Hollow Stem Auger HAMMER TYPE AUTOMATIC

STRUCT. NO. _____
Station _____

BORING NO. T1
Station 1+058
Offset 1.52m LT
Ground Surface Elev. 248.75 m (m) (mm) (kPa) (%)

| SOIL DESCRIPTION | DEPTH (m) | DEPTH (mm) | UCS (kPa) | Failure Mode (%) | SOIL DESCRIPTION | DEPTH (m) | DEPTH (mm) | UCS (kPa) | Failure Mode (%) |
|--|-----------|------------|-----------|------------------|--|-----------|------------|-----------|------------------|
| AUGER SAMPLE CA06 STONE | | | | | Very Soft Dark Gray CLAY LOAM (continued) | | | | |
| | | | | | | 1 | 29 | 22.0 | |
| | | | | | | 3 | | P | |
| CA06 STONE | 247.84 | | | | LOOSE (Very Soft) Gray & Brown SANDY LOAM with GRAVEL & FREE WATER | | | | |
| Very Stiff Brown SILTY CLAY LOAM | 247.68 | 4 | 192 | 24.0 | | 1 | 38 | 59.0 | |
| | | | | | | | | P | |
| Loose Black Buried BITUMINOUS CONCRETE | 247.32 | -1.5 | | | | | | | |
| | | | | | | 3 | | | |
| | | | | | | 3 | 8.0 | | |
| | | | | | Soft Brown & Gray CLAY LOAM TILL | 1 | 48 | 23.0 | |
| | | | | | | 3 | | P | |
| | | | | | End of Boring | | | | |
| Broken BRICK & SANDY LOAM | 246.46 | | | | | | | | |
| Very Stiff Brown SILTY CLAY LOAM | 246.31 | 6 | | | | | | | |
| | | | | | | 3 | 192 | 16.0 | |
| | | | | | | 3 | | | |
| Soft Black & Brown Mix of CLAY LOAM, CA06, & BITUMINOUS CONCRETE | 245.85 | -3.0 | | | | | | | |
| | | | | | | 7 | 48 | 19.0 | |
| BROKEN BRICKS & SAND | 245.55 | | | | | 4 | | | |
| | | | | | | 4 | | | |
| | | | | | | 3 | | | |
| | | | | | | 4 | | | |
| | | | | | | 3 | | | |
| | | | | | | 4 | | | |
| | | | | | | 3 | | | |
| Broken BRICK & Loose Black CINDERS & ASH | 244.18 | -4.5 | | | | | | | |
| | | | | | | 1 | | | |
| | | | | | | 1 | | | |
| | | | | | | 1 | | | |
| Stiff Brown SILTY CLAY LOAM | 243.57 | | | | | | | | |
| | | | | | | 1 | | | |
| | | | | | | 2 | 96 | 26.0 | |
| | | | | | | 3 | | P | |
| | | | | | | 242.81 | -6.0 | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---|
| NAME | DATE | |
| | | BORING LOGS SPRINGFIELD ROAD BOX CULVERT |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |

5 OF 10
DATE 10-16-06

DRAWN BY
CHECKED BY PMH

SPRINGFIELD ROAD CULVERT, BORING LOGS

| | | | | |
|---------------------|------------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (DN & TS-1 | McLEAN | 497 | 293 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

Page 1 of 1

Illinois Department of Transportation
Division of Highways
Illinois Department of Transportation

SOIL BORING LOG

Date 10/9/03

ROUTE FAP 704(L-55BL) DESCRIPTION RETAINING WALL #1; STA 0+549 TO 0+630 LOGGED BY IDOT-LM

SECTION (1) LOCATION NE 14, SEC. 17, TWP. 23N, RNG. 2E, 3rd PM

COUNTY McLEAN DRILLING METHOD Hollow Stem Auger HAMMER TYPE AUTOMATIC

STRUCT. NO. _____
Station _____

BORING NO. 01 6 PTS EXTEN.
Station 555+09
Offset 2.74m LT CL PROP.
Ground Surface Elev. 249.13 m (m) (150 mm) (kPa) (%)

| DEPTH (m) | DEPTH (ft) | SOIL DESCRIPTION | WATER ELEV. (m) | WATER ELEV. (ft) | TEMP. (C) | TEMP. (F) | UNSAT. W. (mm) | UNSAT. W. (%) | UNSAT. W. (mm) | UNSAT. W. (%) |
|-----------|------------|---------------------------------------|-----------------|------------------|-----------|-----------|----------------|---------------|----------------|---------------|
| 0.0 | 0.0 | Augered Brown SILTY CLAY LOAM (FILL) | 246.97 | 812.8 | | | | | | |
| 0.3 | 1.0 | Very Stiff Brown SILTY CLAY | | | | | | | | |
| 0.4 | 1.3 | | | | | | | | | |
| 0.5 | 1.6 | Medium Brown SILTY CLAY | | | | | | | | |
| 0.6 | 1.9 | | | | | | | | | |
| 0.7 | 2.3 | Very Stiff Brown SANDY CLAY LOAM TILL | | | | | | | | |
| 0.8 | 2.6 | | | | | | | | | |
| 0.9 | 2.9 | | | | | | | | | |
| 1.0 | 3.3 | | | | | | | | | |
| 1.1 | 3.6 | | | | | | | | | |
| 1.2 | 3.9 | | | | | | | | | |
| 1.3 | 4.3 | | | | | | | | | |
| 1.4 | 4.6 | | | | | | | | | |
| 1.5 | 4.9 | | | | | | | | | |
| 1.6 | 5.2 | | | | | | | | | |
| 1.7 | 5.6 | | | | | | | | | |
| 1.8 | 5.9 | | | | | | | | | |
| 1.9 | 6.2 | | | | | | | | | |
| 2.0 | 6.6 | | | | | | | | | |
| 2.1 | 6.9 | | | | | | | | | |
| 2.2 | 7.2 | | | | | | | | | |
| 2.3 | 7.5 | | | | | | | | | |
| 2.4 | 7.9 | | | | | | | | | |
| 2.5 | 8.2 | | | | | | | | | |
| 2.6 | 8.5 | | | | | | | | | |
| 2.7 | 8.9 | | | | | | | | | |
| 2.8 | 9.2 | | | | | | | | | |
| 2.9 | 9.5 | | | | | | | | | |
| 3.0 | 9.8 | | | | | | | | | |
| 3.1 | 10.1 | | | | | | | | | |
| 3.2 | 10.4 | | | | | | | | | |
| 3.3 | 10.8 | | | | | | | | | |
| 3.4 | 11.1 | | | | | | | | | |
| 3.5 | 11.4 | | | | | | | | | |
| 3.6 | 11.8 | | | | | | | | | |
| 3.7 | 12.1 | | | | | | | | | |
| 3.8 | 12.4 | | | | | | | | | |
| 3.9 | 12.8 | | | | | | | | | |
| 4.0 | 13.1 | | | | | | | | | |
| 4.1 | 13.4 | | | | | | | | | |
| 4.2 | 13.8 | | | | | | | | | |
| 4.3 | 14.1 | | | | | | | | | |
| 4.4 | 14.4 | | | | | | | | | |
| 4.5 | 14.8 | | | | | | | | | |
| 4.6 | 15.1 | | | | | | | | | |
| 4.7 | 15.4 | | | | | | | | | |
| 4.8 | 15.8 | | | | | | | | | |
| 4.9 | 16.1 | | | | | | | | | |
| 5.0 | 16.4 | | | | | | | | | |
| 5.1 | 16.8 | | | | | | | | | |
| 5.2 | 17.1 | | | | | | | | | |
| 5.3 | 17.4 | | | | | | | | | |
| 5.4 | 17.8 | | | | | | | | | |
| 5.5 | 18.1 | | | | | | | | | |
| 5.6 | 18.4 | | | | | | | | | |
| 5.7 | 18.8 | | | | | | | | | |
| 5.8 | 19.1 | | | | | | | | | |
| 5.9 | 19.4 | | | | | | | | | |
| 6.0 | 19.8 | | | | | | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T208)
BBS, from 137 (Rev. 8-99)

Page 1 of 1

Illinois Department of Transportation
Division of Highways
Illinois Department of Transportation

SOIL BORING LOG

Date 10/9/03

ROUTE FAP 704(L-55BL) DESCRIPTION RETAINING WALL #1; STA 0+549 TO 0+630 LOGGED BY IDOT-LM

SECTION (1) LOCATION NE 14, SEC. 17, TWP. 23N, RNG. 2E, 3rd PM

COUNTY McLEAN DRILLING METHOD Hollow Stem Auger HAMMER TYPE AUTOMATIC

STRUCT. NO. _____
Station _____

BORING NO. 02 6 PTS EXTEN.
Station 578+09
Offset 2.74m LT CL PROP.
Ground Surface Elev. 247.73 m (m) (150 mm) (kPa) (%)

| DEPTH (m) | DEPTH (ft) | SOIL DESCRIPTION | WATER ELEV. (m) | WATER ELEV. (ft) | TEMP. (C) | TEMP. (F) | UNSAT. W. (mm) | UNSAT. W. (%) | UNSAT. W. (mm) | UNSAT. W. (%) |
|-----------|------------|---------------------------------------|-----------------|------------------|-----------|-----------|----------------|---------------|----------------|---------------|
| 0.0 | 0.0 | Augered Brown SILTY CLAY LOAM (FILL) | 246.97 | 812.8 | | | | | | |
| 0.3 | 1.0 | Very Stiff Brown SILTY CLAY LOAM | | | | | | | | |
| 0.4 | 1.3 | | | | | | | | | |
| 0.5 | 1.6 | Stiff Brown SILTY CLAY | | | | | | | | |
| 0.6 | 1.9 | | | | | | | | | |
| 0.7 | 2.3 | Medium Brown SANDY LOAM | | | | | | | | |
| 0.8 | 2.6 | | | | | | | | | |
| 0.9 | 2.9 | | | | | | | | | |
| 1.0 | 3.3 | Very Stiff Brown SANDY CLAY LOAM TILL | | | | | | | | |
| 1.1 | 3.6 | | | | | | | | | |
| 1.2 | 3.9 | | | | | | | | | |
| 1.3 | 4.3 | | | | | | | | | |
| 1.4 | 4.6 | | | | | | | | | |
| 1.5 | 4.9 | | | | | | | | | |
| 1.6 | 5.2 | | | | | | | | | |
| 1.7 | 5.6 | | | | | | | | | |
| 1.8 | 5.9 | | | | | | | | | |
| 1.9 | 6.2 | | | | | | | | | |
| 2.0 | 6.6 | | | | | | | | | |
| 2.1 | 6.9 | | | | | | | | | |
| 2.2 | 7.2 | | | | | | | | | |
| 2.3 | 7.5 | | | | | | | | | |
| 2.4 | 7.9 | | | | | | | | | |
| 2.5 | 8.2 | | | | | | | | | |
| 2.6 | 8.5 | | | | | | | | | |
| 2.7 | 8.9 | | | | | | | | | |
| 2.8 | 9.2 | | | | | | | | | |
| 2.9 | 9.5 | | | | | | | | | |
| 3.0 | 9.8 | | | | | | | | | |
| 3.1 | 10.1 | | | | | | | | | |
| 3.2 | 10.4 | | | | | | | | | |
| 3.3 | 10.8 | | | | | | | | | |
| 3.4 | 11.1 | | | | | | | | | |
| 3.5 | 11.4 | | | | | | | | | |
| 3.6 | 11.8 | | | | | | | | | |
| 3.7 | 12.1 | | | | | | | | | |
| 3.8 | 12.4 | | | | | | | | | |
| 3.9 | 12.8 | | | | | | | | | |
| 4.0 | 13.1 | | | | | | | | | |
| 4.1 | 13.4 | | | | | | | | | |
| 4.2 | 13.8 | | | | | | | | | |
| 4.3 | 14.1 | | | | | | | | | |
| 4.4 | 14.4 | | | | | | | | | |
| 4.5 | 14.8 | | | | | | | | | |
| 4.6 | 15.1 | | | | | | | | | |
| 4.7 | 15.4 | | | | | | | | | |
| 4.8 | 15.8 | | | | | | | | | |
| 4.9 | 16.1 | | | | | | | | | |
| 5.0 | 16.4 | | | | | | | | | |
| 5.1 | 16.8 | | | | | | | | | |
| 5.2 | 17.1 | | | | | | | | | |
| 5.3 | 17.4 | | | | | | | | | |
| 5.4 | 17.8 | | | | | | | | | |
| 5.5 | 18.1 | | | | | | | | | |
| 5.6 | 18.4 | | | | | | | | | |
| 5.7 | 18.8 | | | | | | | | | |
| 5.8 | 19.1 | | | | | | | | | |
| 5.9 | 19.4 | | | | | | | | | |
| 6.0 | 19.8 | | | | | | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T208)
BBS, from 137 (Rev. 8-99)

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---------------------------------------|
| NAME | DATE | |
| | | BORING LOGS RETAINING WALLS |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |

6 OF 10 DRAWN BY
DATE 10-16-06 CHECKED BY PMH

RETAINING WALLS, BORING LOGS

| | | | | |
|---------------------|-------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (1)N & TS-1 | McLEAN | 497 | 295 |
| STA. | | | TO STA. | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

Page 1 of 1

Illinois Department of Transportation SOIL BORING LOG

Date 10/20/03

ROUTE FAP 704(I-55BL) DESCRIPTION RETAINING WALL #2; STA 2+785 TO 2+875 LOGGED BY IDOT-LM

SECTION (1)I LOCATION NE 14, SEC. 17, TWP. 23N, RNG. 2E, 3rd PM

COUNTY MCLEAN DRILLING METHOD Hollow Stem Auger HAMMER TYPE AUTOMATIC

| STRUCT. NO. | D E L C O S I | B L C O S I | U C S I | M O I S T | Surface Water Elev. | D E L C O S I | B L C O S I | U C S I | M O I S T |
|--|---------------------------------|----------------------------|------------------|-----------------------|--|---------------------------------|----------------------------|------------------|-----------------------|
| Station _____ | | | | | _____ m | | | | |
| BORING NO. <u>06 BY VETS SBL</u> | | | | | Groundwater Elev.: | | | | |
| Station <u>984+00</u> | | | | | First Encounter _____ m | | | | |
| Offset <u>0.91m LT CL SBL</u> | | | | | Upon Completion <u>244.4</u> m | | | | |
| Ground Surface Elev. <u>253.25</u> m | (m) | (150 mm) | (kPa) | (%) | After _____ Hrs. | (m) | (150 mm) | (kPa) | (%) |
| AUGERED 0.3 meter SHOULDER STONE; 0.9 meter Brown CA06 FILL Over Gray SANDY CLAY LOAM FILL | | | | | Very Stiff Gray SILTY CLAY TILL (FILL) (continued) | | | | |
| | | | | | | 2 | | | |
| | | | | | | 4 | 278 | 23.0 | |
| | | | | | | 6 | S | | |
| | | | | | | 2 | | | |
| | | | | | | 3 | 211 | 23.0 | |
| | | | | | | 4 | P | | |
| | | | | | 251.73 -1.5 | | | | |
| Very Stiff Gray SANDY CLAY LOAM & SILTY CLAY LOAM (FILL) | 6 | | | | Very Stiff Brown SANDY CLAY LOAM TILL | | | | |
| | 21 | 287 | 17.0 | | | 3 | | | |
| | 8 | P | | | | 4 | 192 | 21.0 | |
| | | | | | | 7 | B | | |
| | | | | | 245.02 | | | | |
| | | | | | Hard Brown SANDY CLAY LOAM TILL | | | | |
| | | | | | | 4 | | | |
| | | | | | | 5 | 431 | 19.0 | |
| | | | | | | 10 | B | | |
| | | | | | -3.0 | | | | |
| | 4 | | | | | 3 | | | |
| | 5 | 240 | 15.0 | | | 7 | 383 | 13.0 | |
| | 8 | P | | | | 10 | B | | |
| | | | | | 249.59 | | | | |
| Hard Brown SANDY CLAY LOAM (FILL) | 4 | | | | | 5 | | | |
| | 5 | 402 | 13.0 | | | 10 | 508 | 12.0 | |
| | 6 | S | | | | 13 | B | | |
| | | | | | 248.83 | | | | |
| Very Stiff Brown SANDY CLAY LOAM & SILTY CLAY (FILL) | 3 | | | | Hard Gray SANDY CLAY LOAM TILL | | | | |
| | 3 | 192 | 11.0 | | | 5 | | | |
| | 6 | P | | | | 8 | 450 | 13.0 | |
| | | | | | 248.07 | | | | |
| Very Stiff Gray & Brown SILTY CLAY LOAM (FILL) | 4 | | | | End of Boring | 12 | B | | |
| | 4 | 201 | 20.0 | | | | | | |
| | 6 | S | | | | | | | |
| | | | | | 247.31 | | | | |
| | | | | | | | | | |
| | | | | | -6.0 | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T298)
BBS, from 137 (Rev. 8-99)

Page 1 of 1

Illinois Department of Transportation SOIL BORING LOG

Date 10/20/03

ROUTE FAP 704(I-55BL) DESCRIPTION RETAINING WALL #2; STA 2+785 TO 2+875 LOGGED BY IDOT-LM

SECTION (1)I LOCATION NE 14, SEC. 17, TWP. 23N, RNG. 2E, 3rd PM

COUNTY MCLEAN DRILLING METHOD Hollow Stem Auger HAMMER TYPE AUTOMATIC

| STRUCT. NO. | D E L C O S I | B L C O S I | U C S I | M O I S T | Surface Water Elev. | D E L C O S I | B L C O S I | U C S I | M O I S T |
|---|---------------------------------|----------------------------|------------------|-----------------------|---|---------------------------------|----------------------------|------------------|-----------------------|
| Station _____ | | | | | _____ m | | | | |
| BORING NO. <u>06 BY VETS SBL</u> | | | | | Groundwater Elev.: | | | | |
| Station <u>981+00</u> | | | | | First Encounter _____ m | | | | |
| Offset <u>0.91m LT CL SBL</u> | | | | | Upon Completion _____ m | | | | |
| Ground Surface Elev. <u>253.21</u> m | (m) | (150 mm) | (kPa) | (%) | After _____ Hrs. | (m) | (150 mm) | (kPa) | (%) |
| AUGERED SHOULDER STONE & CA06 Over Gray SANDY CLAY LOAM | | | | | Stiff Gray SILTY CLAY (FILL) (continued) | | | | |
| | | | | | | 1 | | | |
| | | | | | | 3 | 96 | 25.0 | |
| | | | | | | 3 | B | | |
| | | | | | | 1 | | | |
| | | | | | | 3 | 105 | 27.0 | |
| | | | | | | 3 | B | | |
| | | | | | 251.69 -1.5 | | | | |
| Very Stiff Gray SANDY CLAY LOAM (FILL) | 4 | | | | Stiff Gray SANDY CLAY LOAM & SILTY CLAY LOAM (FILL) | | | | |
| | 4 | 211 | 16.0 | | | 2 | | | |
| | 7 | S | | | | 3 | 192 | 20.0 | |
| | | | | | | 5 | B | | |
| | | | | | 245.74 -7.5 | | | | |
| | | | | | | 1 | | | |
| | | | | | | 2 | 153 | 28.0 | |
| | | | | | | 3 | B | | |
| | | | | | -9.0 | | | | |
| | | | | | | 2 | | | |
| | | | | | | 2 | 96 | 17.0 | |
| | | | | | | 3 | B | | |
| | | | | | 260.01 | | | | |
| Hard Brown SANDY CLAY LOAM (FILL) | 5 | 383 | 12.0 | | | 2 | | | |
| | 6 | P | | | | 3 | 96 | 17.0 | |
| | | | | | | 3 | B | | |
| | | | | | 249.25 | | | | |
| Hard Gray & Brown SANDY CLAY LOAM (FILL) | 3 | | | | Hard Brown SANDY CLAY LOAM TILL | | | | |
| | 5 | 469 | 12.0 | | | 5 | 469 | 12.0 | |
| | 7 | S | | | | 7 | S | | |
| | | | | | -10.5 | | | | |
| | | | | | | 4 | | | |
| | | | | | | 4 | 383 | 14.0 | |
| | | | | | | 11 | 536 | 12.0 | |
| | | | | | | 14 | S | | |
| | | | | | 243.46 | | | | |
| | | | | | | | | | |
| | | | | | 242.08 | | | | |
| | | | | | | | | | |
| | | | | | | | | | |
| | | | | | -12.0 | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T298)
BBS, from 137 (Rev. 8-99)

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---------------------------------------|
| NAME | DATE | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |

BORING LOGS
RETAINING WALLS

8 OF 10
DATE 10-16-06

DRAWN BY
CHECKED BY PMH

RETAINING WALLS, BORING LOGS

| | | | | |
|---------------------|-----------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | UN & TS-1 | McLEAN | 497 | 297 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

Illinois Department of Transportation
Division of Highways
Illinois Department of Transportation

SOIL BORING LOG

Page 1 of 1

Date 10/22/03

ROUTE FAP 704(I-55BL) DESCRIPTION RETAINING WALL #3; STA 0+201 TO 0+245 AT CEMETARY LOGGED BY IDOT-LM

SECTION (11) LOCATION NE 14, SEC. 17, TWP. 23N, RNG. 2E, 3rd PM

COUNTY MCLEAN DRILLING METHOD Hollow Stem Auger HAMMER TYPE AUTOMATIC

STRUCT. NO. _____ Station _____

BORING NO. 09 MORRIS AVE.
Station 210+00
Offset 7.92m RT CL PROP.
Ground Surface Elev. 253.55 m (m) (160 mm) (kPa) (%)

| DEPTH (m) | SOIL DESCRIPTION | U | M | Surface Water Elev. (m) | Stream Bed Elev. (m) | Groundwater Elev. (m) | First Encounter (m) | Upon Completion (m) | After (Hrs.) |
|-----------|---|---|---|-------------------------|----------------------|-----------------------|---------------------|---------------------|--------------|
| 0.0 | AUGERED BITUMINOUS PAVEMENT Over Brown CA06 | | | | | | | | |
| 252.79 | Hard Brown SANDY CLAY LOAM TILL | 6 | | | | | | | |
| 252.18 | Very Stiff Brown SANDY CLAY LOAM TILL | 5 | | | | | | | |
| 249.89 | Very Stiff Gray SANDY CLAY LOAM TILL | 3 | | | | | | | |
| 249.13 | Stiff Gray SANDY CLAY LOAM | 3 | | | | | | | |
| 248.52 | End of Boring | | | | | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)

Illinois Department of Transportation
Division of Highways
Illinois Department of Transportation

SOIL BORING LOG

Page 1 of 1

Date 10/22/03

ROUTE FAP 704(I-55BL) DESCRIPTION RETAINING WALL #3; STA 0+201 TO 0+245 AT CEMETARY LOGGED BY IDOT-LM

SECTION (11) LOCATION NE 14, SEC. 17, TWP. 23N, RNG. 2E, 3rd PM

COUNTY MCLEAN DRILLING METHOD Hollow Stem Auger HAMMER TYPE AUTOMATIC

STRUCT. NO. _____ Station _____

BORING NO. 10 MORRIS AVE.
Station 240+00
Offset 7.92m RT CL PROP.
Ground Surface Elev. 252.75 m (m) (160 mm) (kPa) (%)

| DEPTH (m) | SOIL DESCRIPTION | U | M | Surface Water Elev. (m) | Stream Bed Elev. (m) | Groundwater Elev. (m) | First Encounter (m) | Upon Completion (m) | After (Hrs.) |
|-----------|---|---|---|-------------------------|----------------------|-----------------------|---------------------|---------------------|--------------|
| 0.0 | AUGERED BITUMINOUS PAVEMENT Over Brown CA06 | | | | | | | | |
| 251.99 | Very Stiff Brown SANDY CLAY LOAM TILL | 4 | | | | | | | |
| 249.85 | Hard Brown SANDY CLAY LOAM TILL | 5 | | | | | | | |
| 248.79 | Stiff Gray SANDY CLAY LOAM TILL | 3 | | | | | | | |
| 247.72 | End of Boring | | | | | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---------------------------------------|
| NAME | DATE | |
| | | BORING LOGS RETAINING WALLS |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |

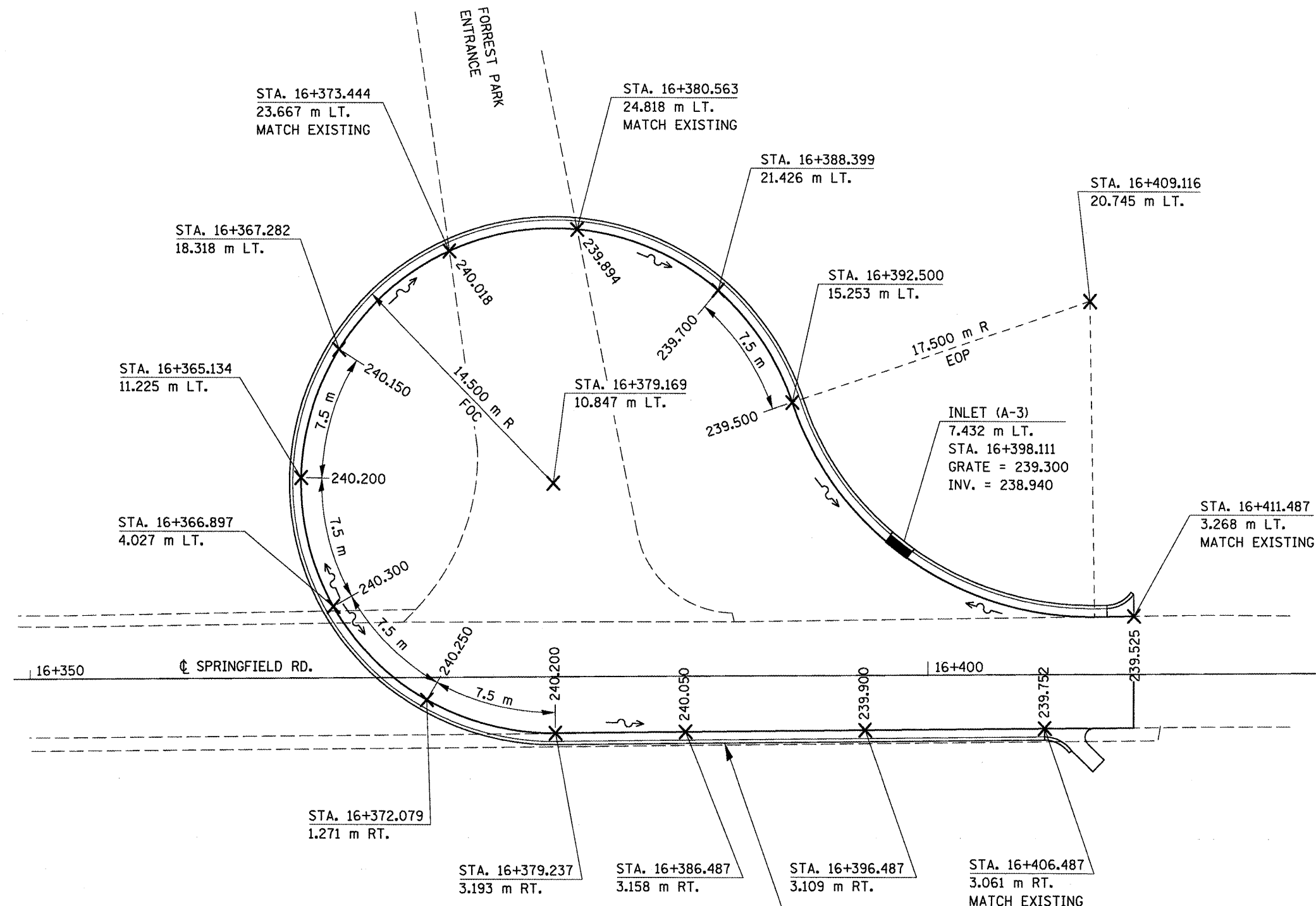
10 OF 10
DATE 10-16-06

DRAWN BY
CHECKED BY PMH

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|--------------|---------------------------|--------------|-----------|
| 704 | (1)N & TS-1D | McLEAN | 497 | 298 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



NOT TO SCALE



LEGEND
 ~~~~~ DIRECTION OF FLOW  
 X 239.500 TOP OF PAVEMENT ELEVATION

**NOTE:**  
 ALL PROPOSED CURB & GUTTER IS B-15.45 UNLESS NOTED OTHERWISE.

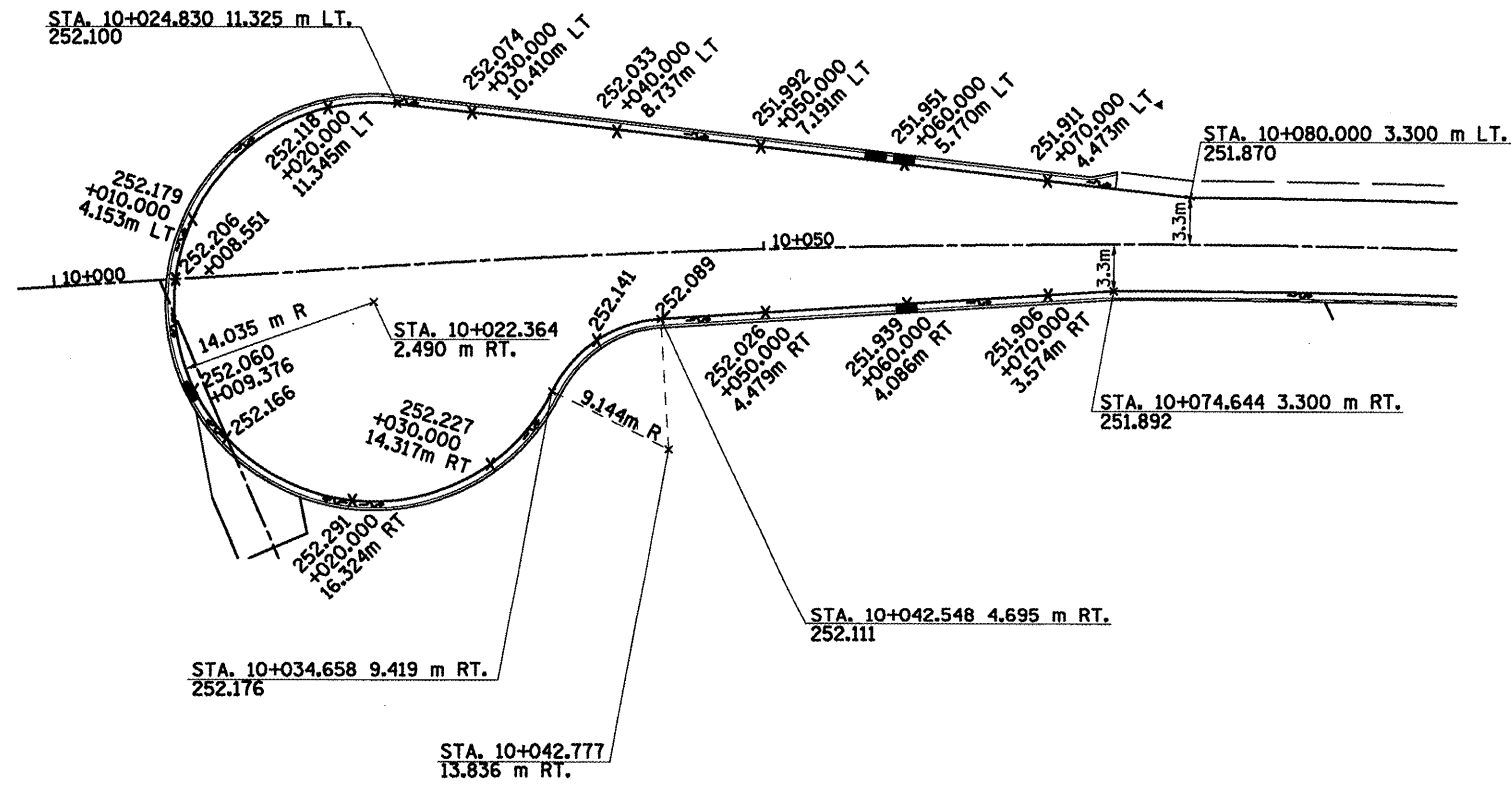
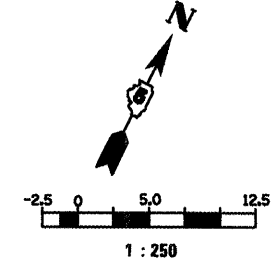
CONC. C&G  
 TY. M-10.45  
 STA. 16+379.237 TO  
 STA. 16+406.487 RT.

| REVISIONS |      |
|-----------|------|
| NAME      | DATE |
|           |      |
|           |      |
|           |      |
|           |      |
|           |      |
|           |      |
|           |      |

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**SPRINGFIELD ROAD  
 CUL DE SAC DETAIL**  
 DRAWN BY RC  
 CHECKED BY KJT  
 DATE 6/29/2010

PLOT DATE = 8/1/2010  
 FILE NAME = c:\pwworkspace\projects\70514\70514-ahc\layout.dgn  
 PLOT SCALE = 6.7500 m / IN.  
 USER NAME = oragie

|                     |            |                           |              |           |
|---------------------|------------|---------------------------|--------------|-----------|
| F.A.P. RTE.         | SECTION    | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| 704                 | (LN & TS-1 | McLEAN                    | 497          | 299       |
| STA. 10+000.000     |            | TO STA. 10+080.255        |              |           |
| FED. ROAD DIST. NO. |            | ILLINOIS FED. AID PROJECT |              |           |
| CONTRACT NO. 70514  |            |                           |              |           |



**LEGEND:**

- ~ DIRECTION OF FLOW
- x252.026 TOP EDGE OF PAVEMENT ELEVATION

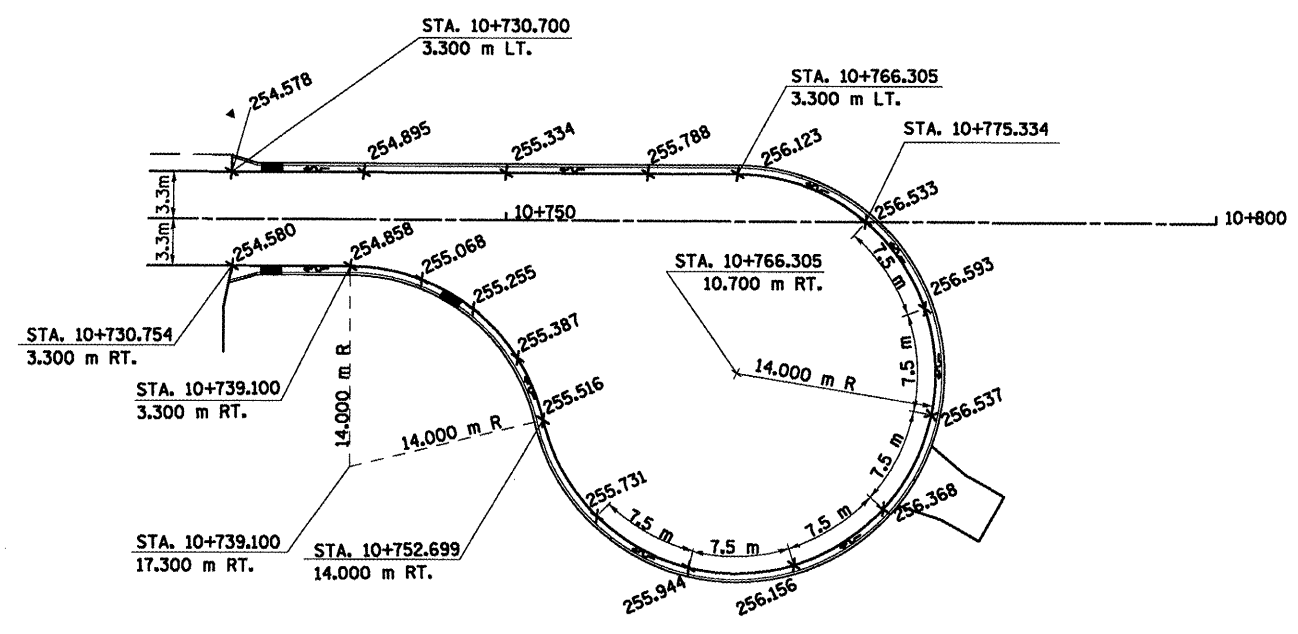
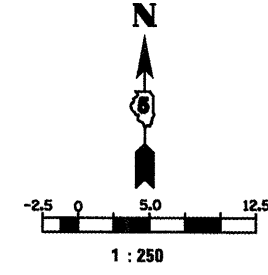
**NOTES:**

1. ALL PROPOSED CONCRETE CURB AND GUTTER SHOWN IS B-15.45 UNLESS NOTED OTHERWISE.

| REVISIONS |          | ILLINOIS DEPARTMENT OF TRANSPORTATION  |
|-----------|----------|----------------------------------------|
| NAME      | DATE     |                                        |
|           |          | GREENWOOD AVENUE<br>CUL DE SAC DETAILS |
|           |          |                                        |
|           |          |                                        |
|           |          |                                        |
|           |          |                                        |
|           |          |                                        |
| 1 OF 2    |          | DRAWN BY SAM                           |
| DATE      | 05-27-09 | CHECKED BY PMH                         |

**GREENWOOD AVENUE CUL DE SAC DETAILS**

|                     |           |                    |                  |           |
|---------------------|-----------|--------------------|------------------|-----------|
| F.A.P. RTE.         | SECTION   | COUNTY             | TOTAL SHEETS     | SHEET NO. |
| 704                 | DN & TS-1 | McLEAN             | 497              | 300       |
| STA. 10+730.700     |           | TO STA. 10+775.334 |                  |           |
| FED. ROAD DIST. NO. |           | ILLINOIS           | FED. AID PROJECT |           |
| CONTRACT NO. 70514  |           |                    |                  |           |



**LEGEND:**  
 DIRECTION OF FLOW  
 TOP EDGE OF PAVEMENT ELEVATION

**NOTES:**  
 1. ALL PROPOSED CONCRETE CURB AND GUTTER SHOWN IS B-15.45 UNLESS NOTED OTHERWISE.

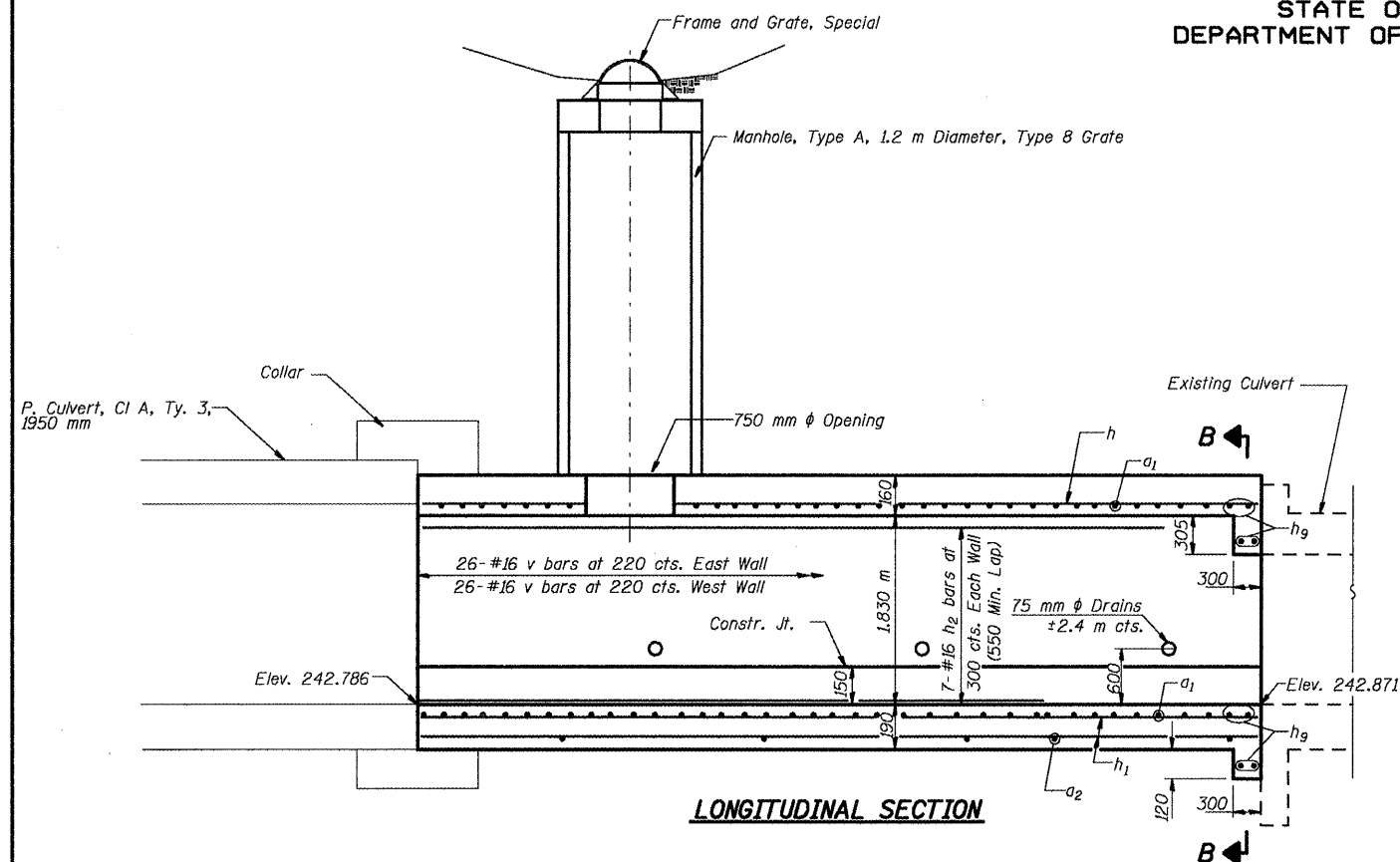
| REVISIONS |      | ILLINOIS DEPARTMENT OF TRANSPORTATION  |
|-----------|------|----------------------------------------|
| NAME      | DATE |                                        |
|           |      | GREENWOOD AVENUE<br>CUL DE SAC DETAILS |
|           |      |                                        |
|           |      |                                        |
|           |      |                                        |
|           |      |                                        |
|           |      |                                        |

DRAWN BY SAM  
 CHECKED BY PMH  
 DATE 05-27-09

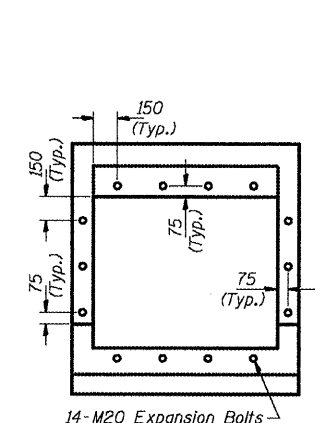
**GREENWOOD AVENUE CUL DE SAC DETAILS**

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

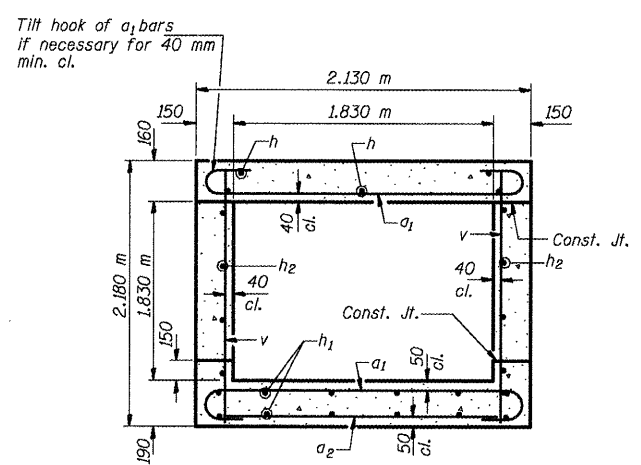
|                  |          |         |      |       |
|------------------|----------|---------|------|-------|
| PROJECT NO.      | CONTRACT | CITY    | DATE | SCALE |
| F.A.P. 704       | CDI      | MCLEAN  | 4/97 | 300A  |
| DESIGN DIST. NO. | DRAWN    | PROJECT |      |       |



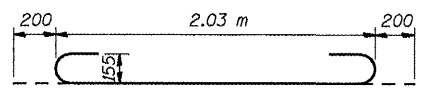
**LONGITUDINAL SECTION**



**SECTION B-B**  
Expansion bolts shall be M20 hooked bolts. Hooked bolts shall extend a minimum of 230 mm into new concrete.



**SECTION THRU BARREL**



**BAR a1**

**DESIGN STRESSES**  
 $f_y = 420 \text{ MPa}$   
 $f'_c = 24 \text{ MPa}$

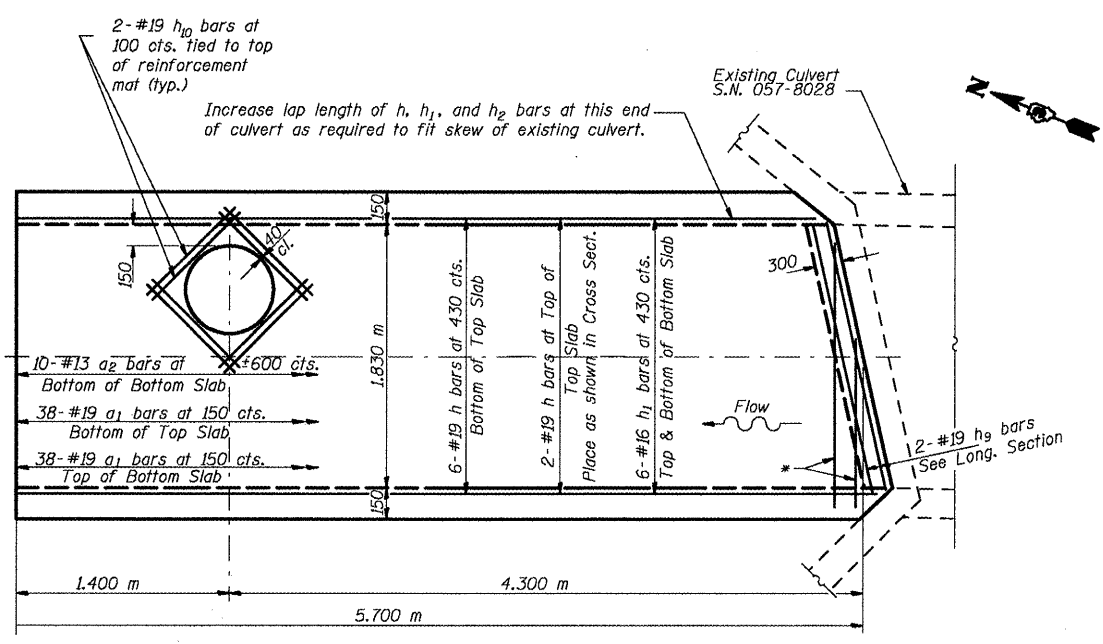
**LOADING MS18 & ALT.**

**BILL OF MATERIAL**

| Bar                   | No. | Size | Length (m)     | Shape |
|-----------------------|-----|------|----------------|-------|
| a1                    | 76  | #19  | 2.43           | U     |
| a2                    | 10  | #13  | 1.91           | —     |
| h                     | 8   | #19  | 5.62           | —     |
| h1                    | 12  | #16  | 5.62           | —     |
| h2                    | 14  | #16  | 5.62           | —     |
| h9                    | 8   | #19  | 1.89           | —     |
| h10                   | 8   | #19  | 1.09           | —     |
| v                     | 52  | #16  | 2.08           | —     |
| Concrete Box Culverts |     |      | m <sup>3</sup> | 7.6   |
| Reinforcement Bars    |     |      | kg             | 1151  |
| M20 Expansion Bolts   |     |      | each           | 14    |

**SHIVE HATTERY**

|          |
|----------|
| DESIGNED |
| CHECKED  |
| DRAWN    |
| CHECKED  |



**PLAN**

\* a bars in skew portion of slab shall be ordered full length & cut to fit.

**NOTES**

Reinforcement Bars shall conform to the requirements of ASTM A706M Grade 420.  
All dimensions are in millimeters (mm) except as noted.  
All construction joints shall be bonded.

**CULVERT DETAILS**  
SPRINGFIELD ROAD  
STA. 0+972.395  
22.220 m RT.  
F.A.P. ROUTE 704 SECTION (D)  
MCLEAN COUNTY