

STATE OF ILLINOIS
WINNEBAGO COUNTY HIGHWAY DEPARTMENT
 PLANS FOR PROPOSED FEDERAL AID URBAN IMPROVEMENT



INDEX OF SHEETS

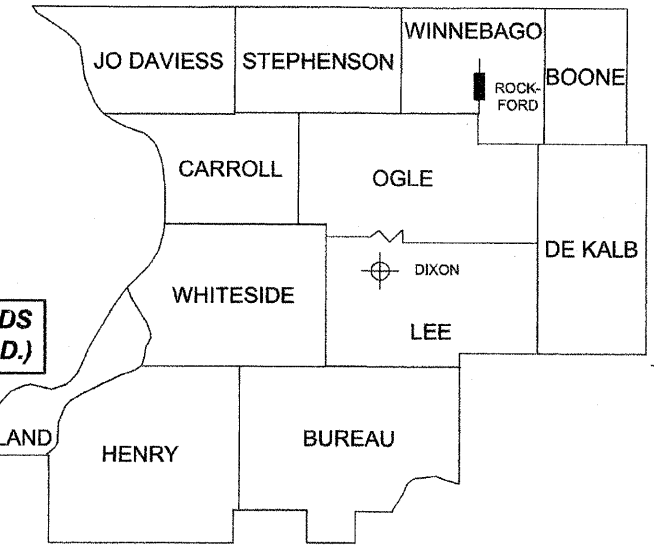
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FAU ROUTE 5097
FAS ROUTE 1041
SECTION: 06-00400-00-RS
PROJECT NO.: M-5099 (76)

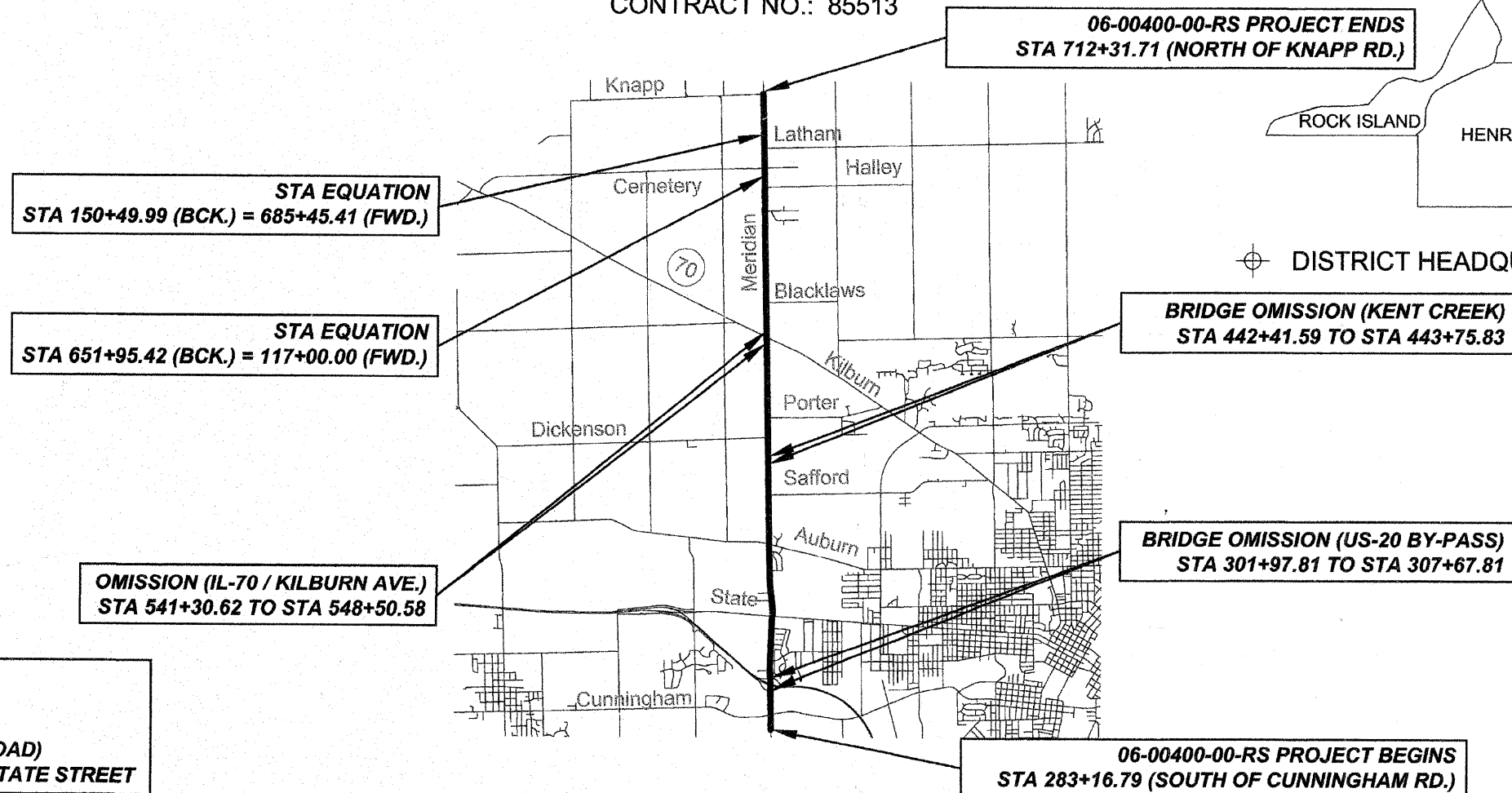
RESURFACING ON MERIDIAN ROAD (C.H. 24)

JOB NO.: C-92-074-07

CONTRACT NO.: 85513



DISTRICT HEADQUARTER
 PROJECT LOCATION



ADT: 8,250 (2009) 10% TRUCKS
MAJOR COLLECTOR & MINOR ARTERIAL
DESIGN SPEED: 50 MPH
3R POLICY (STATE STREET TO KNAPP ROAD)
LAPP POLICY (CUNNINGHAM ROAD TO STATE STREET)

SCALES:	
CROSS-SECTIONS	PLAN & PROFILE
FULL SIZE	FULL SIZE
HORIZONTAL: 1" = 20'	HORIZONTAL: 1" = 50'
VERTICAL: 1" = 10'	VERTICAL: 1" = 10'
1/4 SIZE	1/4 SIZE
HORIZONTAL: 1" = 40'	HORIZONTAL: 1" = 100'
VERTICAL: 1" = 20'	VERTICAL: 1" = 20'

MERIDIAN ROAD: 41,560.95 FT = 7.87 MILES
 SIDE ROAD IMPROVEMENTS: 4,773.34 FT = 0.90 MILES
 TOTAL LENGTH OF IMPROVEMENTS = 46,334.29 FT = 8.77 MILES


CALL J.U.L.I.E.
BEFORE YOU DIG
 1-800-892-0123

WINNEBAGO TWP (T26N- R11E)
 SEC. - 13, 12, 1

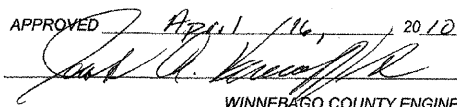
BURRITT TWP (T27N- R11E)
 SEC. - 36, 25, 24, 13, 12, 1

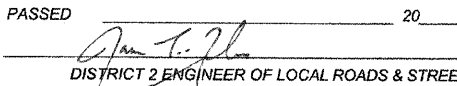
ROCKFORD TWP (T44N- R1E)
 SEC. - 31, 30, 19, 18, 7, 6

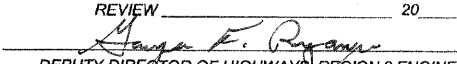
OWEN TWP (T45N- R1E)
 SEC. - 31, 30, 19, 18, 7


 4/16/10
 EXP 11/30/11

THESE PLANS PREPARED BY:
WINNEBAGO COUNTY HIGHWAY DEPARTMENT

APPROVED:  4/16/10
 WINNEBAGO COUNTY ENGINEER

PASSED: _____ 20_____

 DISTRICT 2 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID
 BASED ON LIMITED
 REVIEW _____ 20_____

 DEPUTY DIRECTOR OF HIGHWAYS, REGION 2 ENGINEER



SUMMARY OF QUANTITIES CONT.)

ITEM NO.	PAY CODE NUMBER	ITEMS	UNIT	QUANTITIES		
				06-00400-00-RS		TOTAL
				I000	Y031-1F	
* 155	87702980	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 50 FT.	EA		2.00	2.00
* 156	87703030	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 60 FT.	EA		1.00	1.00
* 157	87800100	CONCRETE FOUNDATION, TYPE A	FT		3.00	3.00
* 158	87800150	CONCRETE FOUNDATION, TYPE C	FT		3.00	3.00
* 159	87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FT		43.00	43.00
* 160	87800420	CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER	FT		21.00	21.00
* 161	88040090	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3 SECTION, MAST ARM MOUNTED	EA		6.00	6.00
* 162	88040150	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5 SECTION, BRACKET MOUNTED	EA		5.00	5.00
* 163	88040160	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5 SECTION, MAST ARM MOUNTED	EA		5.00	5.00
* 164	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EA		1.00	1.00
* 165	89502380	REMOVE EXISTING HANDHOLE	EA		6.00	6.00
* 166	89502385	REMOVE EXISTING CONCRETE FOUNDATION	EA		8.00	8.00
167	X0322903	SAW CUTTING, (FULL DEPTH)	FT	4,236.00		4,236.00
168	X0322923	SEGMENTAL CONCRETE BLOCK WALL	SF	412.00		412.00
* 169	X0323481	VIDEO VEHICLE DETECTION, 4 CAMERAS	EA		1.00	1.00
170	X6060500	CORRUGATED MEDIAN REMOVAL	SF	2,439.00		2,439.00
171	X6064201	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06	FT	279.00		279.00
172	X6063600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24	FT	7,096.00		7,096.00
* 173	XX003163	EMERGENCY VEHICLE PRIORITY SYSTEM	EA		1.00	1.00
174	X0322936	REMOVE EXISTING FLARED END SECTION	EA	1.00		1.00
175	X2111100	TOPSOIL EXCAVATION AND PLACEMENT, SPECIAL	CY	8,115.30		8,115.30
176	Z0013798	CONSTRUCTION LAYOUT	LSUM	1.00		1.00
* 177	Z0033084	ELECTRIC CABLE IN CONDUIT, GROUND, NO. 6 1C (GREEN)	FT		536.00	536.00
178	Z0055300	RUMBLE STRIP	EA	8.00		8.00
179	Z0075310	TIE BARS 3/4"	EA	956.00		956.00

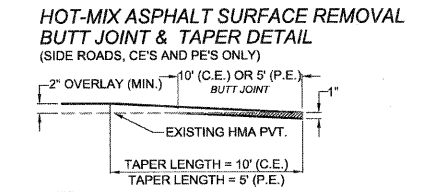
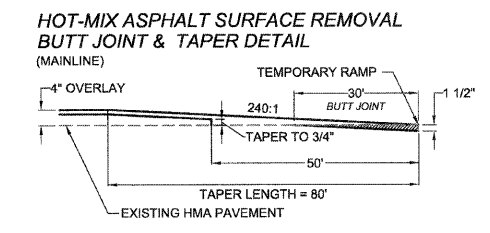
* SPECIALTY ITEMS



HMA MIXTURE CHART

MIXTURE USE (S):	SURFACE, TOP LIFT FULL DEPTH	LEVEL BINDER	BINDER, BASE COURSE, & FULL DEPTH	TOP SHOULDER	BOTTOM SHOULDER
PG:	PG 64-22	PG 64-22	PG 64-22	PG 58-22	PG 58-22
DESIGN AIR VOIDS:	4.0% @ N70	4.0% @ N70	4.0% @ N70	3.0% @ N50	2.0% @ N50
MIXTURE COMPOSITION (GRADATION MIXTURE):	IL-9.5	IL-9.5FG	IL-19.0	IL-9.5	BAM OR IL-19.0
FRICTION AGGREGATE:	MIX D	N/A	N/A	MIX C	N/A
20 YEAR ESAL:	VARIES TO 3.0	VARIES TO 3.0	VARIES TO 3.0	N/A	N/A
MIX UNIT WEIGHT:	112 LBS / SY / IN			112 LBS / SY / IN	

DETAILS

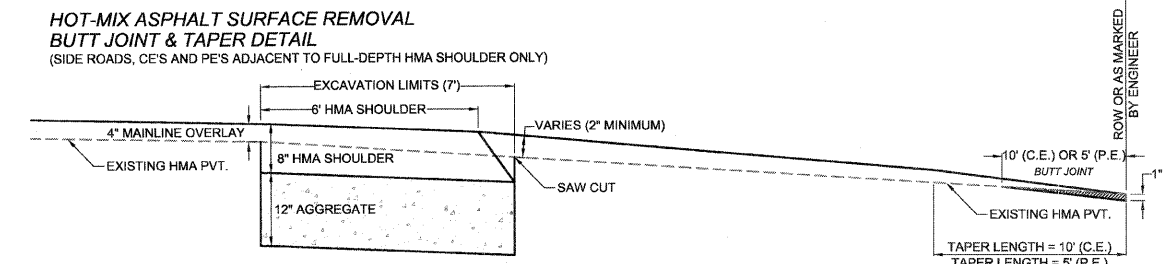


NOTE: THE THICKNESS OF INCIDENTAL BITUMINOUS SURFACING FOR ALL DRIVES SHALL BE 2" MINIMUM UNLESS OTHERWISE NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER.

EXISTING AGGREGATE DRIVEWAYS SHALL BE PREPARED USING AGGREGATE BASE, TYPE B TO BRING THE DRIVEWAY TO PROPER WIDTH AND GRADE BEFORE PLACING THE INCIDENTAL BITUMINOUS SURFACING.

PAVEMENT DESIGN (OVERLAY - 06-00400-00-RS)

DATE: JUNE 26, 2008	ROUTE: C.H. 24 (MERIDIAN ROAD)
CALCULATIONS BY: TESTING SERVICE CORP.	SECTION: 06-00400-00-RS
CHECKED BY: _____	COUNTY: WINNEBAGO
CLASS II ROADS AND STREETS	LOCATION: FROM CUNNINGHAM TO KNAPP
LIMITS OF ANALYSIS:	PAVEMENT DESIGN:
STATION 283+16.79 TO	SUBGRADE SUPPORT RATING (SSR):
STATION 712+31.71	POOR (FAIR, POOR, OR GRANULAR)
LENGTH: 42,914.92 FT 8.13 MILES	FLEXIBLE TRAFFIC FACTOR: SEE CHART
STRUCTURAL DESIGN TRAFFIC:	SELECTED DESIGN PG BINDER: PG 64-22
SEGMENT	1 2 3 4 5
STA RANGE	276+13 to 291+33 291+33 to 317+83 340+72 to 353+47 353+47 to 541+31 548+51 to 712+32
STA RANGE CONT.)	317+83 to 340+72
ADT (10 Yr.)	11,145 11,315 9,960 7,755 5,380
PV	9,808 9,777 8,765 6,824 4,734
SU	780 972 697 543 377
MU	557 566 498 388 269
FLEX. TF	3.035 3.084 2.713 2.114 1.466
OVERLAY THICKNESS	3 1/4" 3 1/4" 4" 4" 4"
FULL-DEPTH THICKNESS	N/A N/A 12" 12" 11"

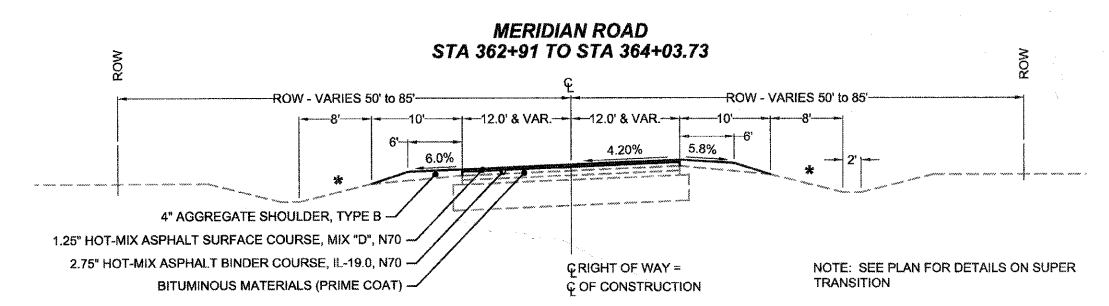
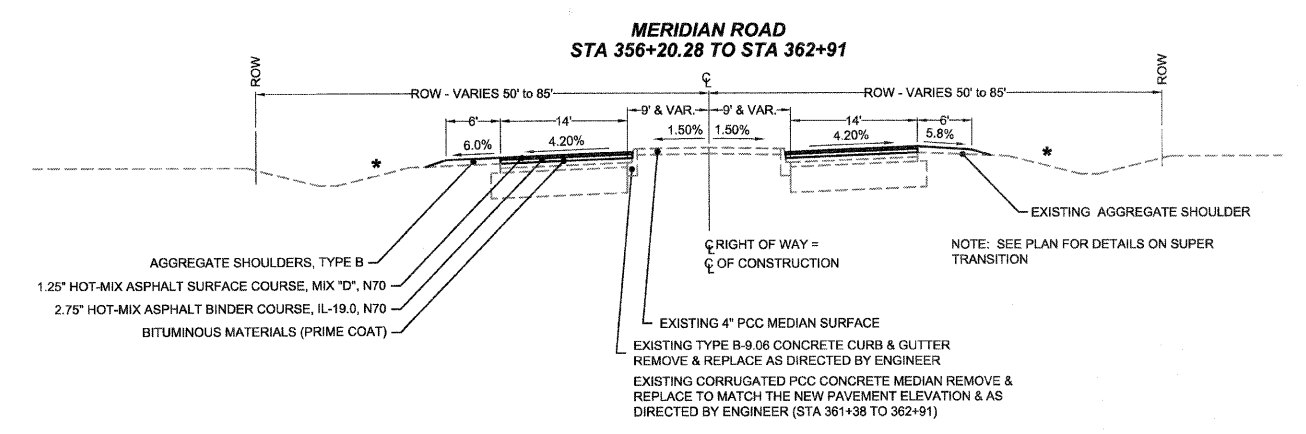
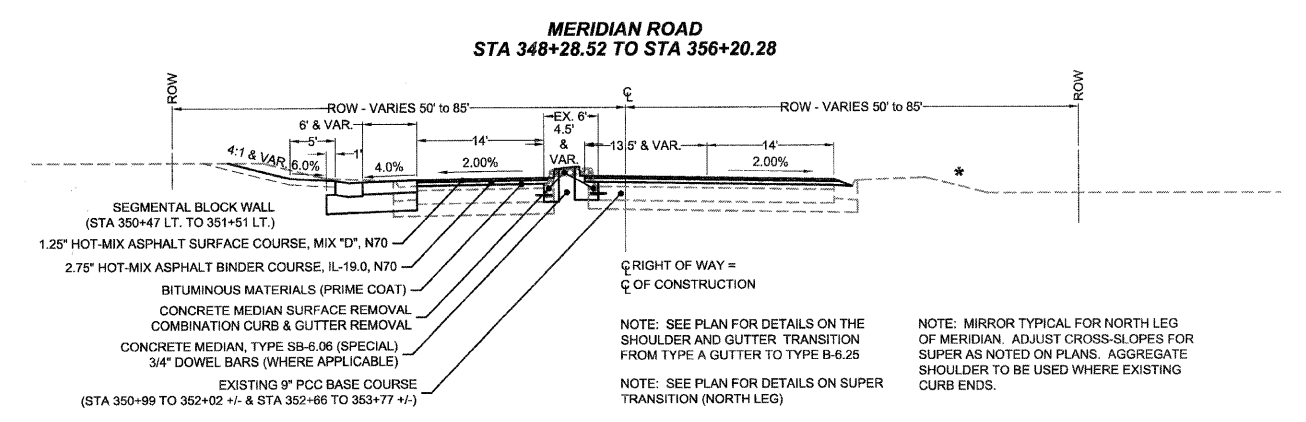
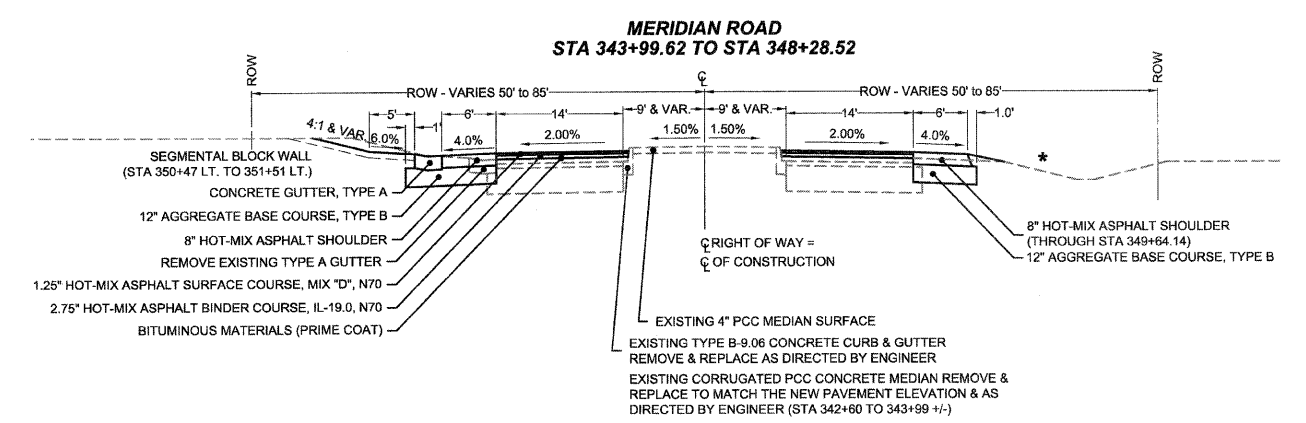
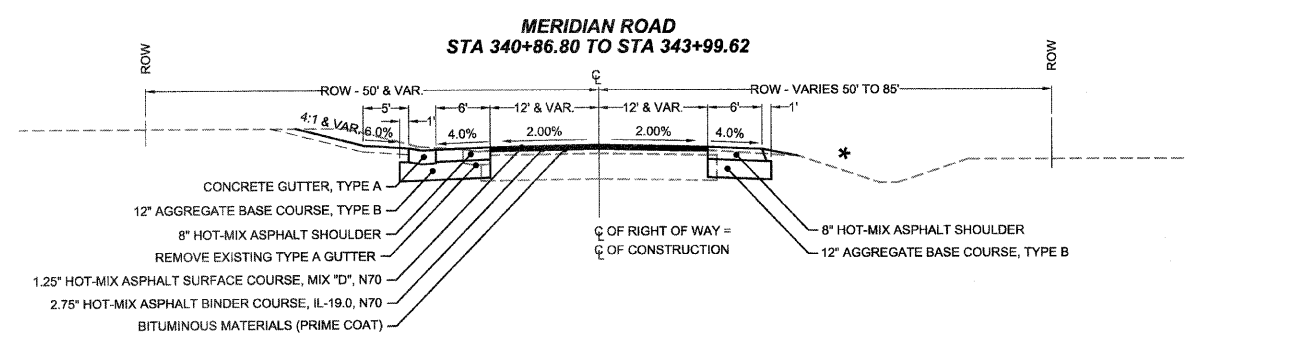
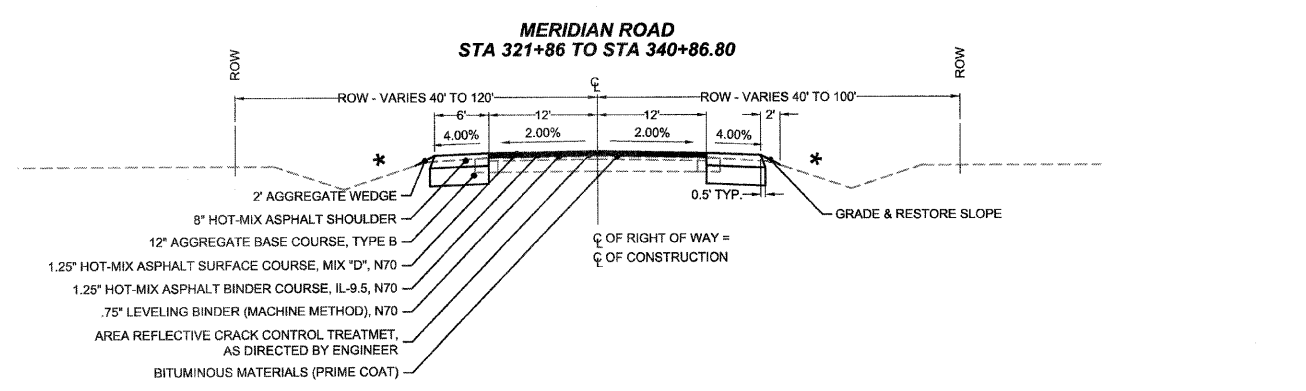
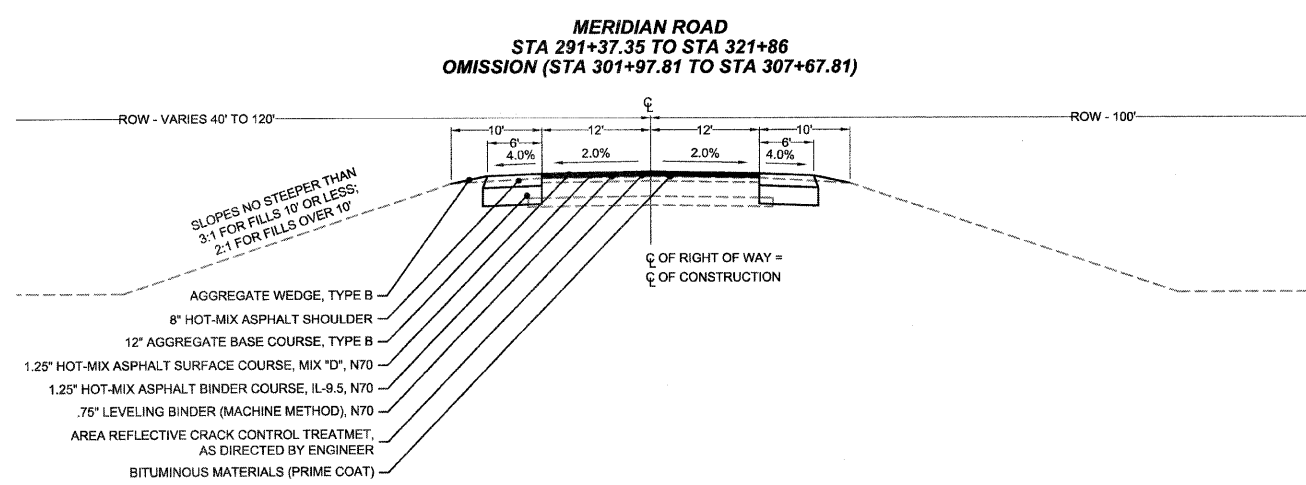
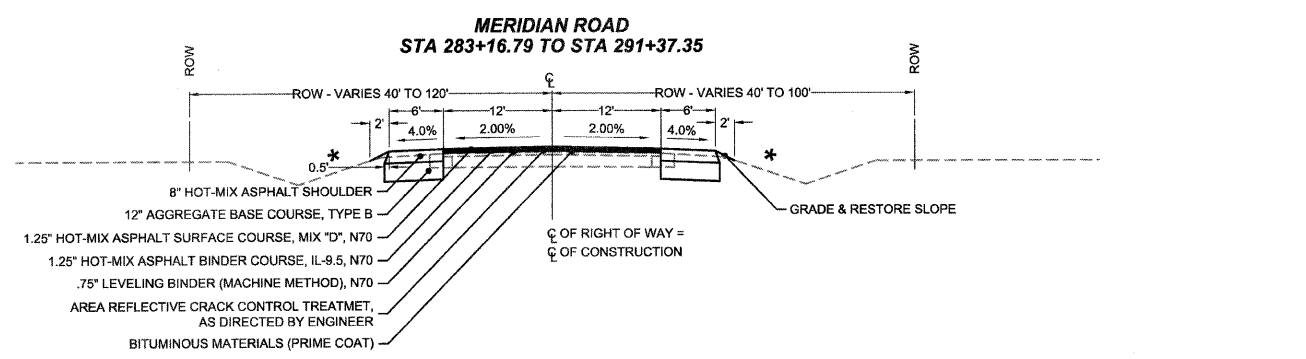


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EXISTING AGGREGATE DRIVEWAYS SHALL BE PREPARED USING AGGREGATE BASE, TYPE B TO BRING THE DRIVEWAY TO PROPER WIDTH AND GRADE BEFORE PLACING THE INCIDENTAL BITUMINOUS SURFACING.



TYPICAL SECTIONS

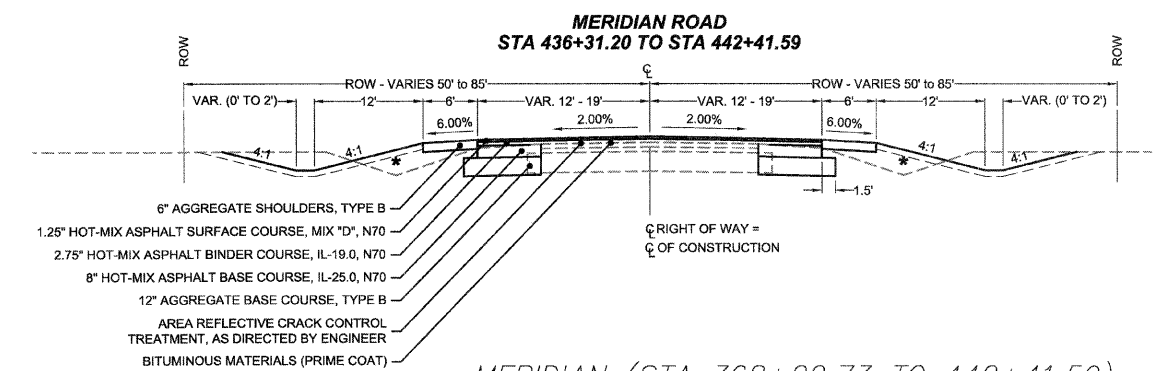
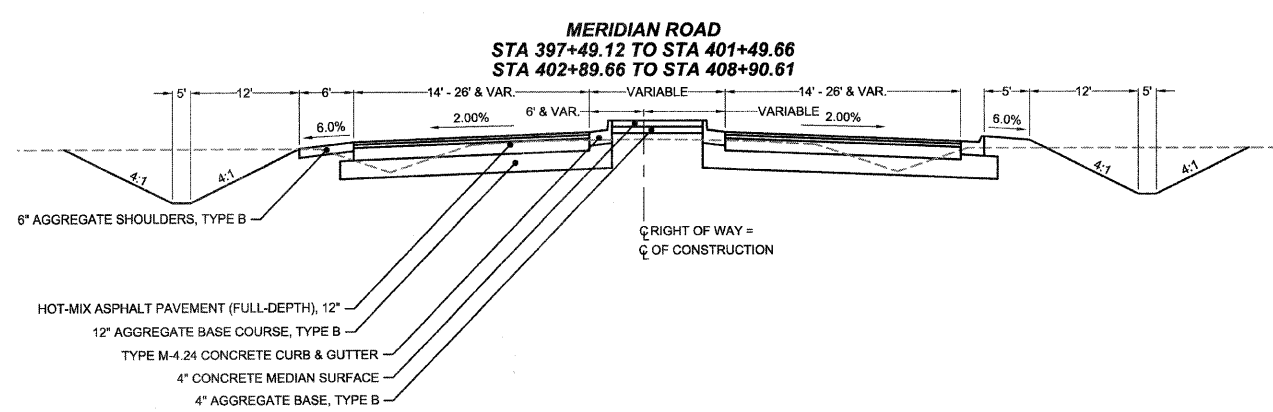
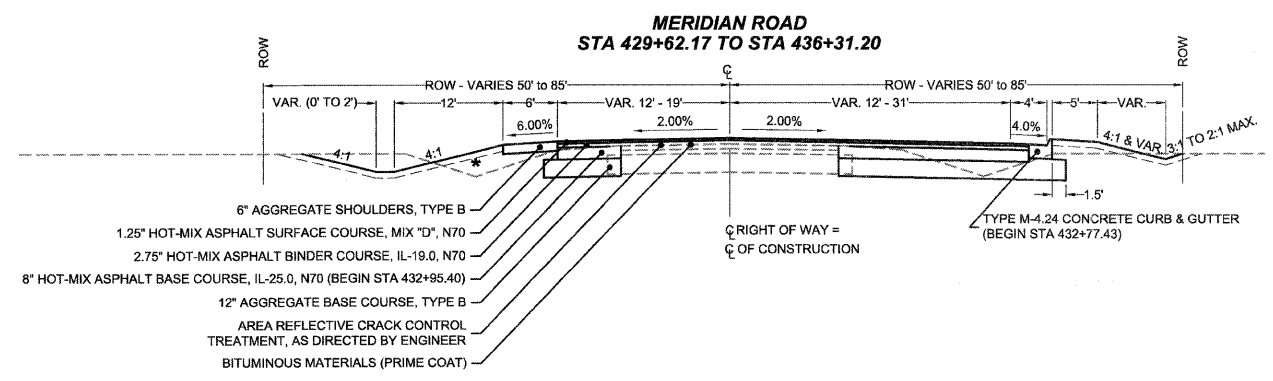
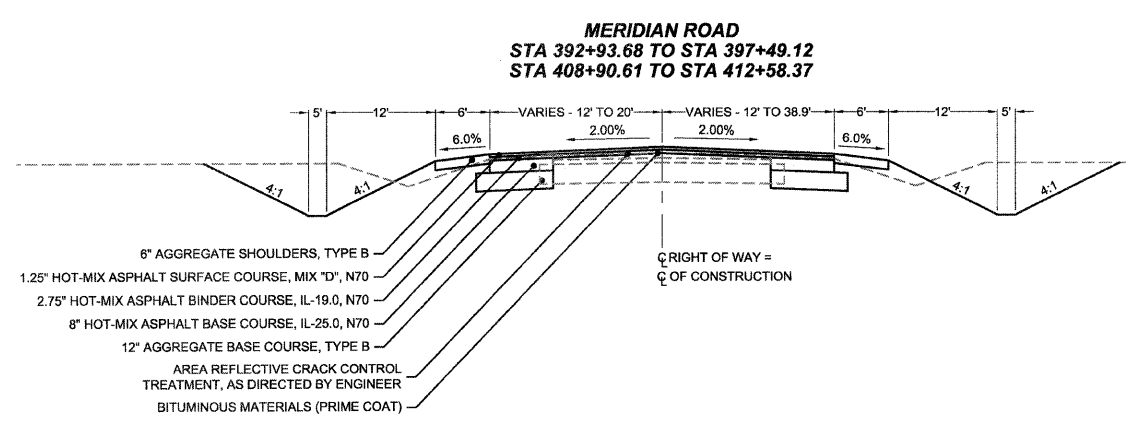
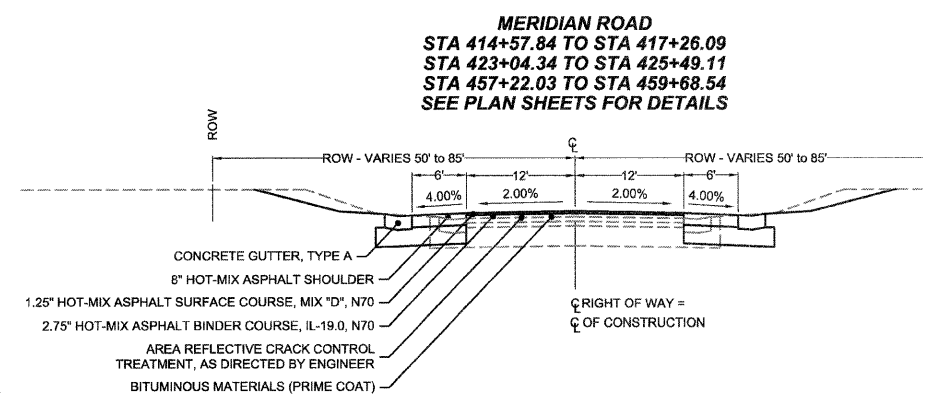
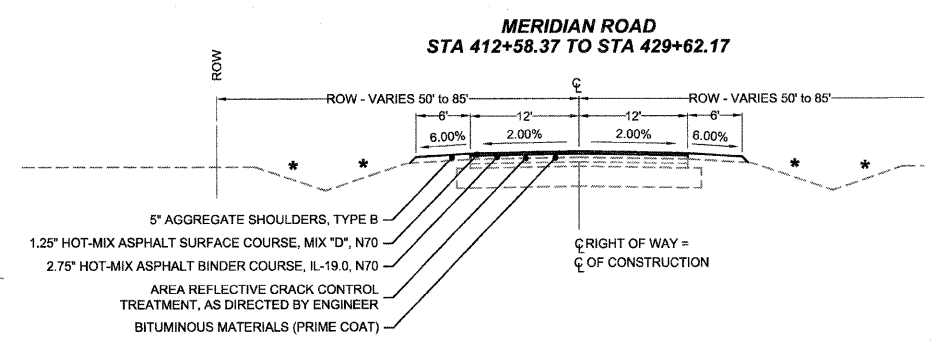
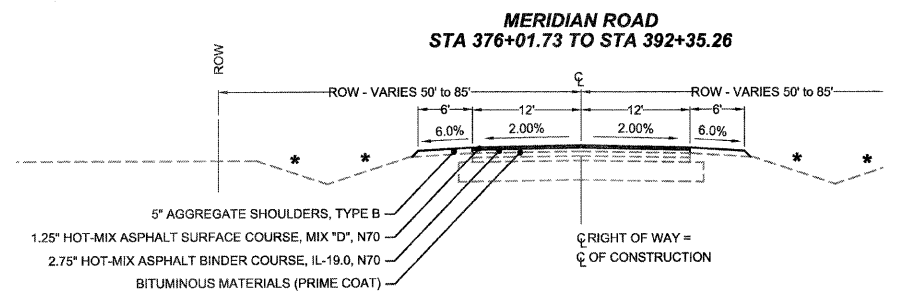
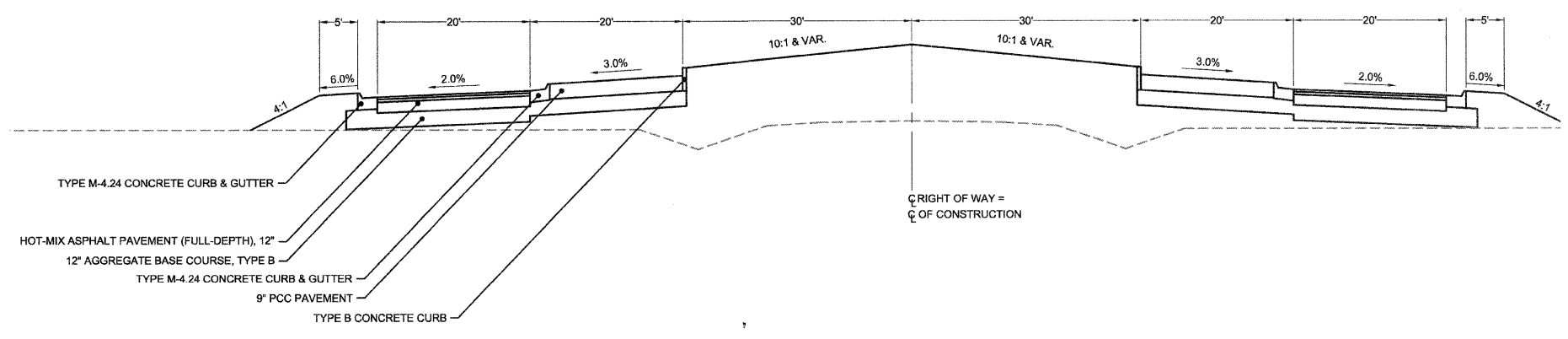
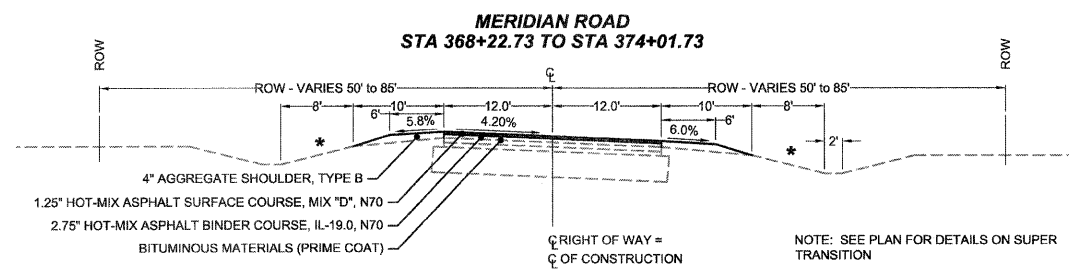


NOTE: * - MAINTAIN EXISTING ROADSIDE SLOPES. EXISTING SLOPES ESTIMATED BETWEEN 3:1 AND 4:1.



TYPICAL SECTIONS

ROUNDBABOUT TYPICAL

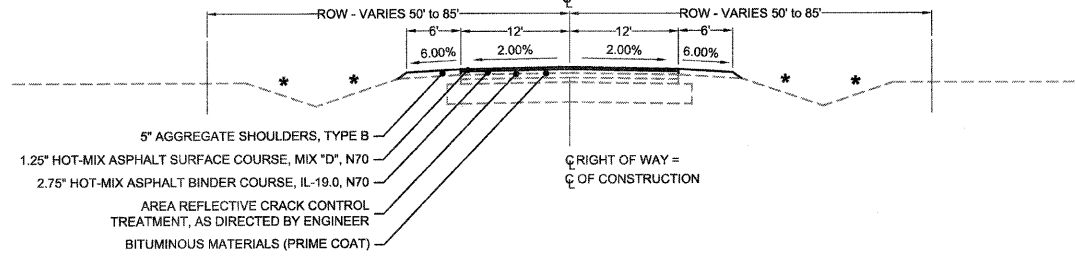


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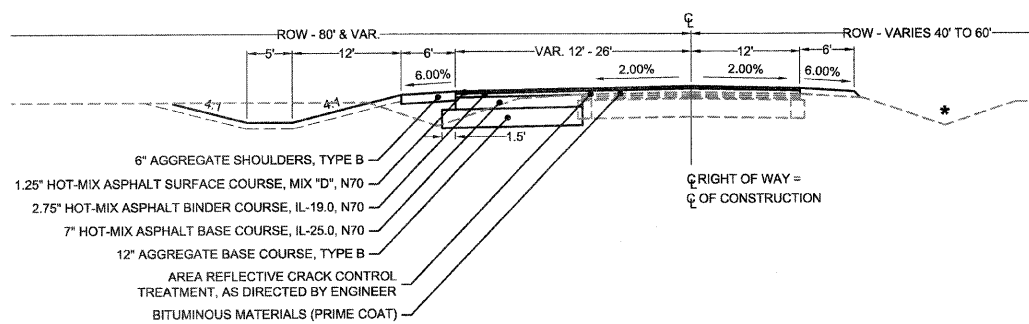


TYPICAL SECTIONS

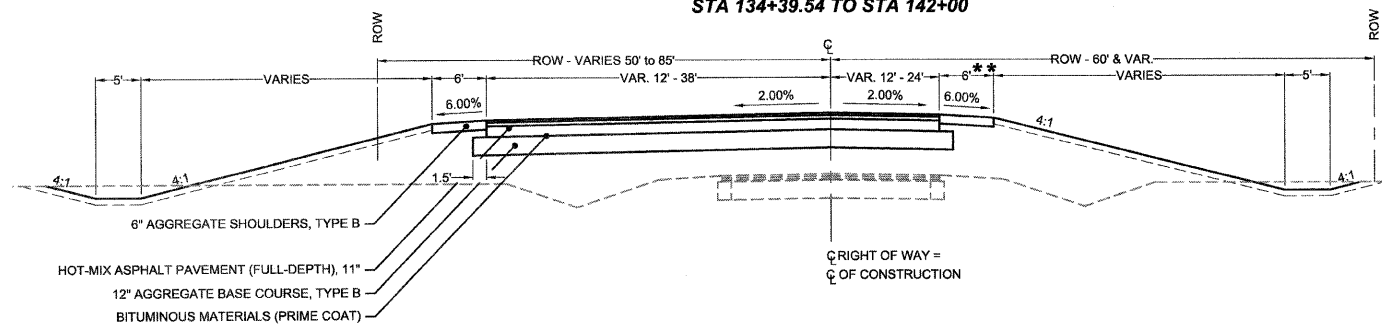
MERIDIAN ROAD
 STA 442+41.59 TO STA 469+50.49
 STA 485+00.97 TO STA 117+26.02
 STA 150+27.47 TO STA 712+31.71
 (STA EQUATION AT 150+49.99 BCK = STA 685+45.41 FWD)
 OMISSION STA 442+41.59 TO 443+75.83 - BRIDGE
 OMISSION STA 541+30.62 TO 548+50.58 - IL-70



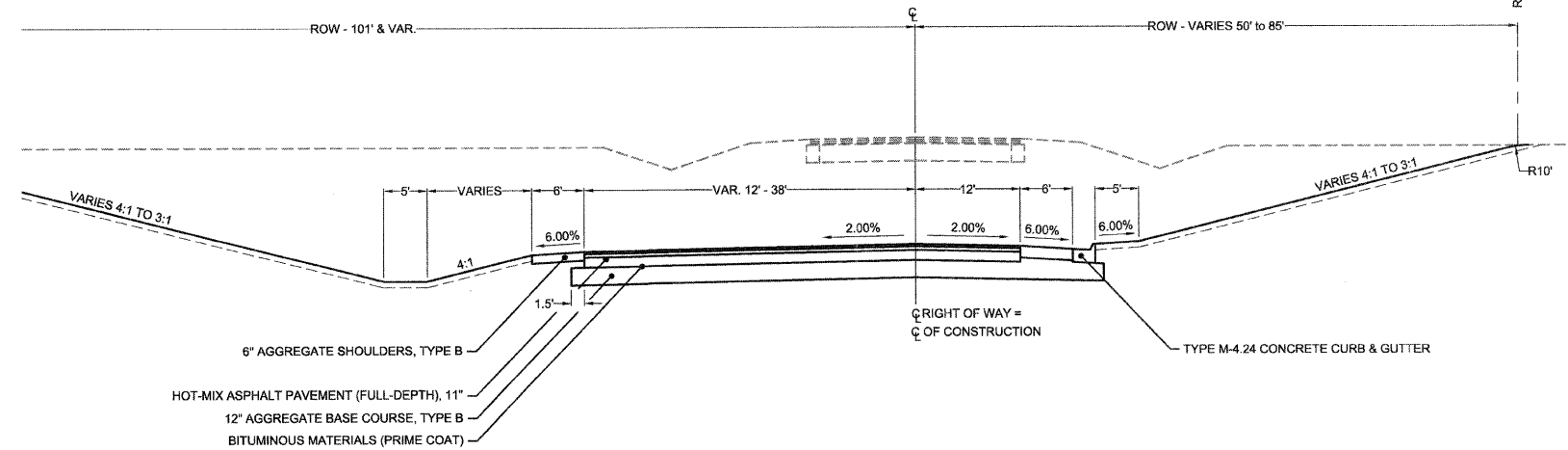
MERIDIAN ROAD
 STA 469+50.49 TO STA 485+00.97
 STA 117+26.02 TO STA 118+36.56
 STA 142+00 TO STA 150+27.47



MERIDIAN ROAD
 STA 118+36.56 TO STA 129+68.73
 STA 134+39.54 TO STA 142+00



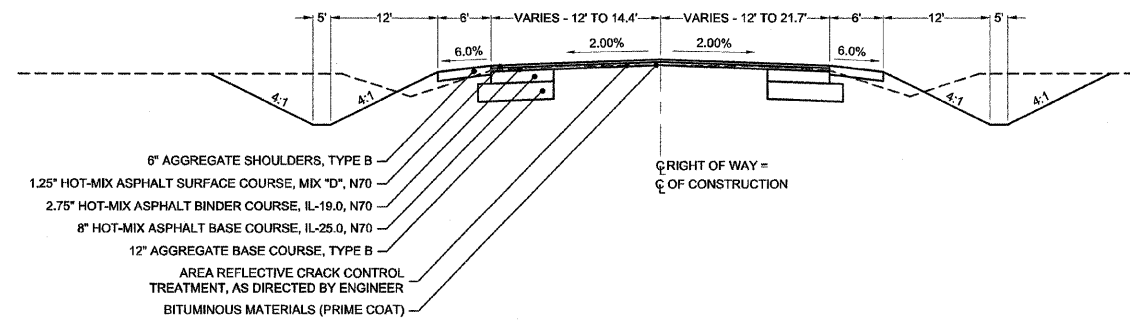
MERIDIAN ROAD
 STA 129+68.73 TO STA 134+39.54



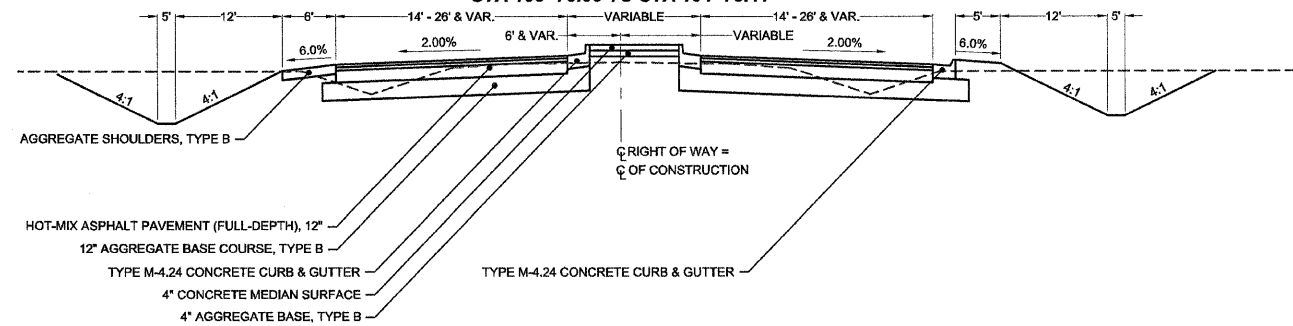


TYPICAL SECTIONS

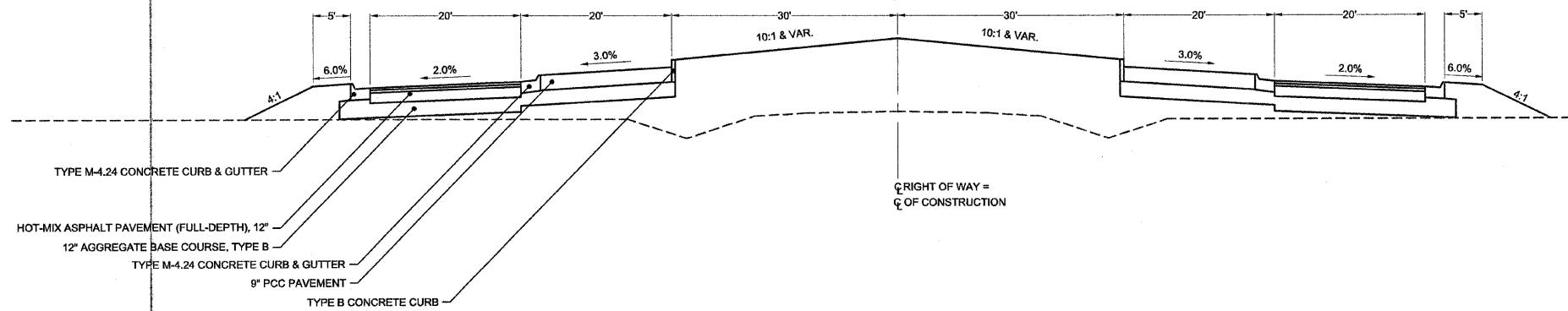
**AUBURN ROAD
STA 89+60.74 TO STA 93+27.99**



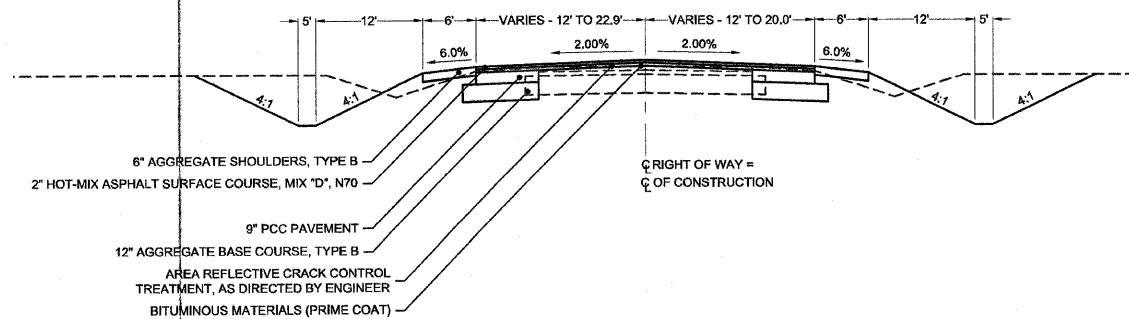
**AUBURN ROAD
STA 93+27.99 TO STA 99+30.00
STA 100+70.00 TO STA 104+75.41**



ROUNDABOUT TYPICAL



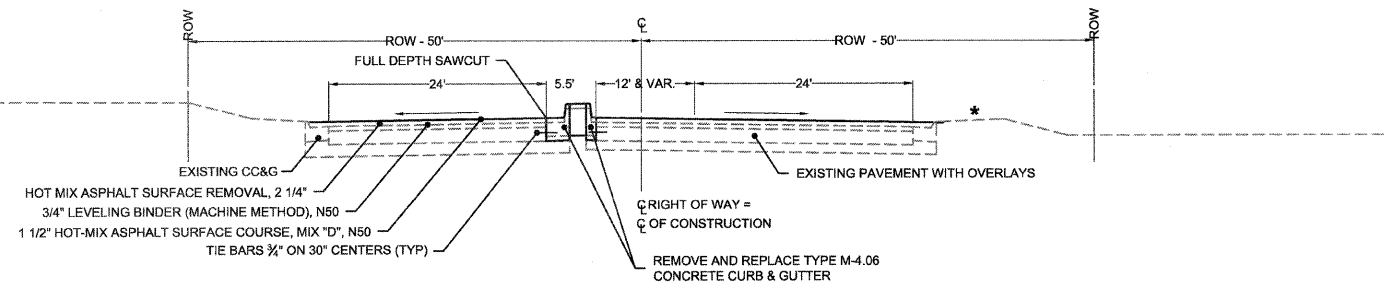
**AUBURN ROAD
STA 104+75.41 TO STA 110+76.45**



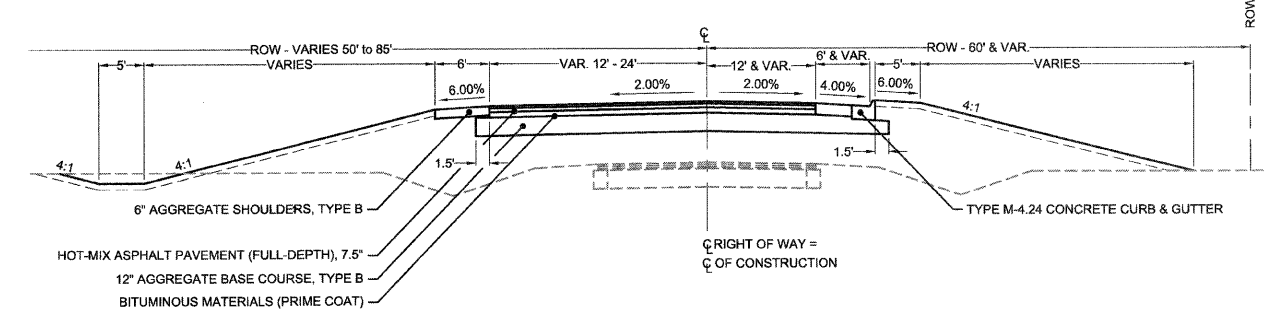


TYPICAL SECTIONS

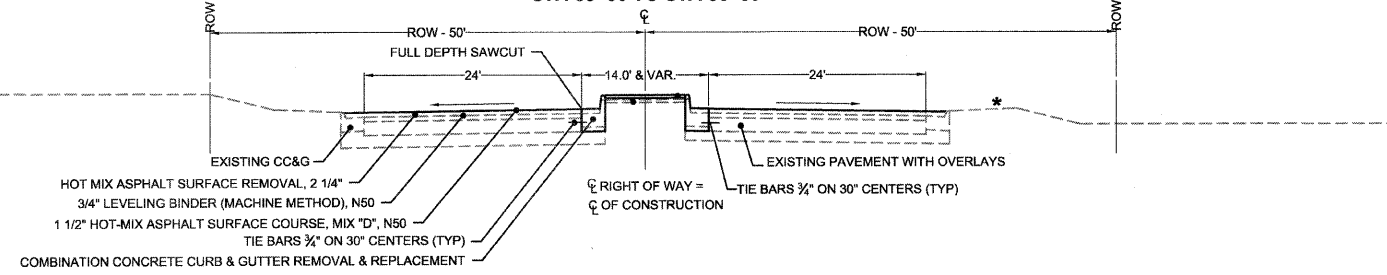
**WEST STATE STREET
STA 87+44 TO STA 92+00**



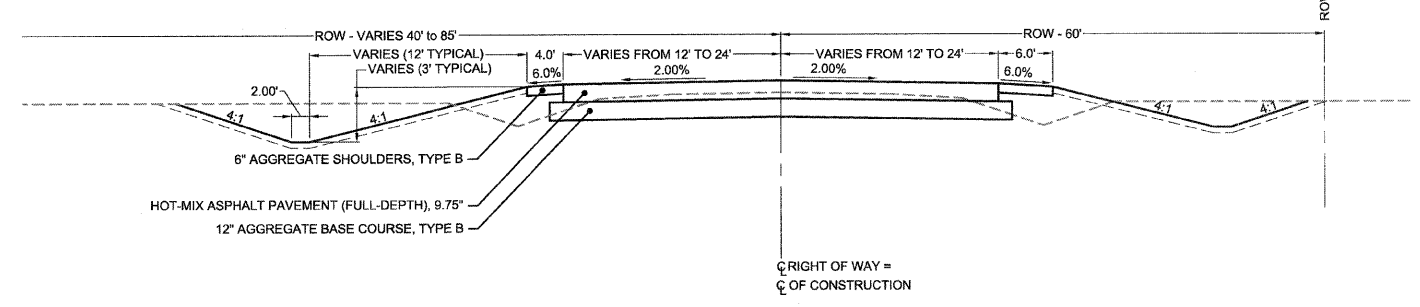
**CEMETERY ROAD
STA 196+75 TO STA 201+16**



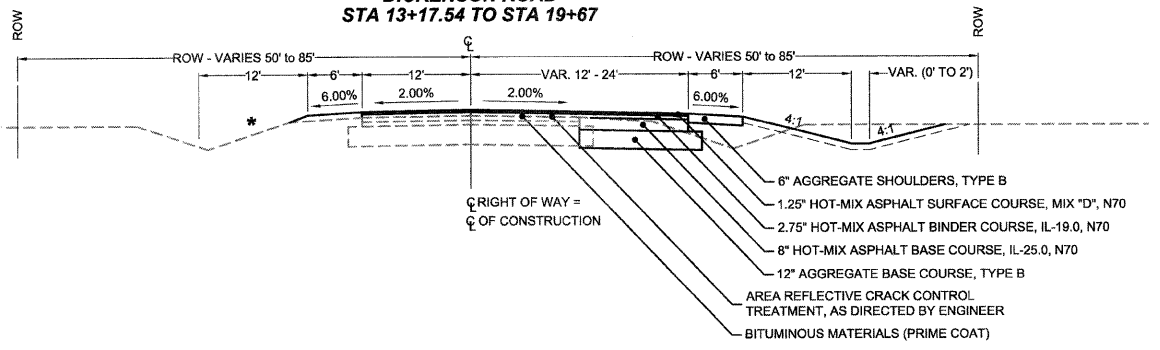
**WEST STATE STREET
STA 83+00 TO STA 86+80**



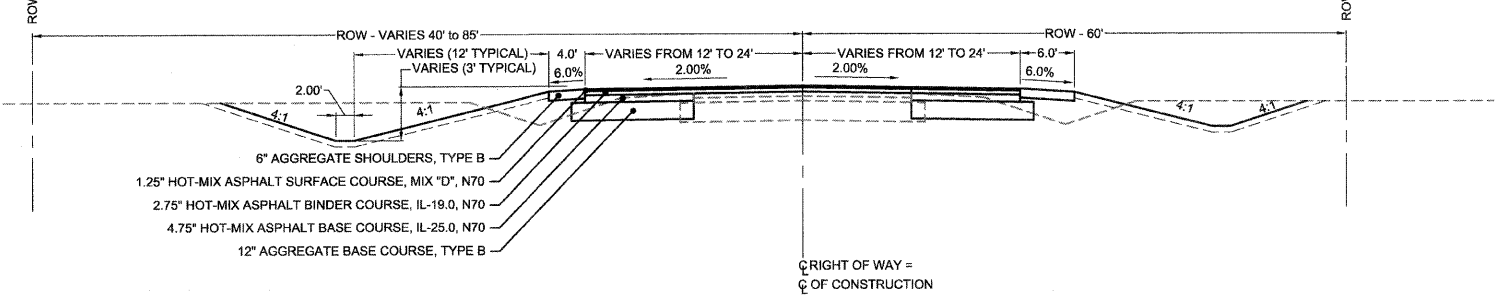
**LATHAM ROAD
STA 0+17.60 TO STA 1+75**



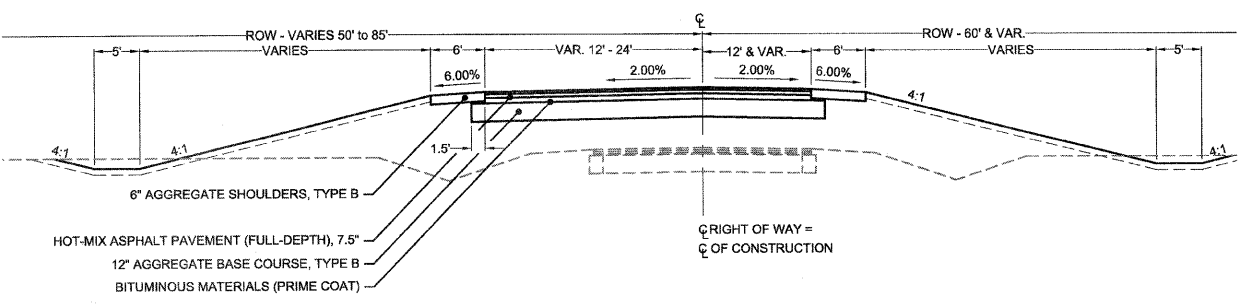
**DICKENSON ROAD
STA 13+17.54 TO STA 19+67**



**LATHAM ROAD
STA 1+75 TO STA 6+57.91**



**CEMETERY ROAD
STA 194+57.03 TO STA 196+75
STA 201+69 TO STA 205+50.43**



NOTE: * - MAINTAIN EXISTING ROADSIDE SLOPES. EXISTING SLOPES ESTIMATED BETWEEN 3:1 AND 4:1.



HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT (SY)

Table with 4 columns: STA, O/S, TYPE, HMA SURF REM BUTT JT (SY). Lists stationing and quantities for hot-mix asphalt surface removal along Meridian Road.

HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT (SY)

Table with 4 columns: STA, O/S, TYPE, HMA SURF REM BUTT JT (SY). Lists stationing and quantities for hot-mix asphalt surface removal along Meridian Road.

TEMPORARY RAMP

Table with 4 columns: STA, O/S, TYPE, TEMPORARY RAMP (SY). Lists quantities for temporary ramps.

HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70

Table with 4 columns: STA, O/S, TYPE, HMA BC IL-19.0 N70 (TON). Lists quantities for hot-mix asphalt binder course.

HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 (TON)

Table with 4 columns: STA, O/S, TYPE, HMA BC IL-19.0 N70 (TON). Lists quantities for hot-mix asphalt binder course.

HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 (TON)

Table with 4 columns: STA, O/S, TYPE, HMA BC IL-19.0 N70 (TON). Lists quantities for hot-mix asphalt binder course.

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70

Table with 4 columns: STA, O/S, TYPE, HMA SC "D" N70 (TON). Lists quantities for hot-mix asphalt surface course.

Table with 4 columns: STA, O/S, TYPE, HMA SC "D" N70 (TON). Lists quantities for hot-mix asphalt surface course.

Table with 4 columns: STA, O/S, TYPE, HMA SC "D" N70 (TON). Lists quantities for hot-mix asphalt surface course.

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70

Table with 4 columns: STA, O/S, TYPE, HMA SC "D" N70 (TON). Lists quantities for hot-mix asphalt surface course.

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50

Table with 4 columns: STA, O/S, TYPE, HMA SC "D" N50 (TON). Lists quantities for hot-mix asphalt surface course.

HOT-MIX ASPHALT PAVEMENT (FULL - DEPTH), 7 1/2"

Table with 4 columns: STA, O/S, TYPE, HMA PAVT FD 7 1/2 (SY). Lists quantities for hot-mix asphalt pavement.

HOT-MIX ASPHALT PAVEMENT (FULL - DEPTH), 9 3/4"

Table with 4 columns: STA, O/S, TYPE, HMA PAVT FD 9 3/4 (SY). Lists quantities for hot-mix asphalt pavement.

HOT-MIX ASPHALT PAVEMENT (FULL - DEPTH), 11"

Table with 4 columns: STA, O/S, TYPE, HMA PAVT FD 11 (SY). Lists quantities for hot-mix asphalt pavement.

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70

Table with 4 columns: STA, O/S, TYPE, HMA SC "D" N70 (TON). Lists quantities for hot-mix asphalt surface course.

PORTLAND CEMENT CONCRETE PAVEMENT 9"

Table with 4 columns: STA, O/S, TYPE, PCC PVT 9 (SY). Lists quantities for portland cement concrete pavement.

COMBINATION CURB AND GUTTER REMOVAL

Table with 4 columns: STA, O/S, COMB CURB GUTTER REM (FT). Lists quantities for curb and gutter removal.

Table with 4 columns: STA, O/S, COMB CURB GUTTER REM (FT). Lists quantities for curb and gutter removal.

Table with 4 columns: STA, O/S, COMB CURB GUTTER REM (FT). Lists quantities for curb and gutter removal.

Table with 4 columns: STA, O/S, COMB CURB GUTTER REM (FT). Lists quantities for curb and gutter removal.

Table with 4 columns: STA, O/S, COMB CURB GUTTER REM (FT). Lists quantities for curb and gutter removal.

Table with 4 columns: STA, O/S, COMB CURB GUTTER REM (FT). Lists quantities for curb and gutter removal.

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

Table with 4 columns: STA, O/S, COMB C C&G REM & REPL (FT). Lists quantities for concrete curb and gutter removal and replacement.

NOTE: A CONTINGENCY QUANTITY HAS BEEN PROVIDED FOR THE ENGINEER TO USE ON MERIDIAN ROAD AT STATE STREET TO REPLACE SECTIONS OF BAD CURB AS IDENTIFIED AND MARKED IN THE FIELD BY THE ENGINEER.

CONCRETE MEDIAN SURFACE REMOVAL

Table with 4 columns: STA, O/S, CONC MEDIAN SURF REM (SF). Lists quantities for concrete median surface removal.

MEDIAN REMOVAL

Table with 4 columns: STA, O/S, MEDIAN REMOVAL (SF). Lists quantities for median removal.

MEDIAN SURFACE REMOVAL AND REPLACEMENT

Table with 4 columns: STA, O/S, MEDIAN SURF R&R (SF). Lists quantities for median surface removal and replacement.

NOTE: A CONTINGENCY QUANTITY HAS BEEN PROVIDED FOR THE ENGINEER TO USE ON MERIDIAN ROAD AT STATE STREET TO REPLACE SECTIONS OF BAD MEDIAN SURFACE AS IDENTIFIED AND MARKED IN THE FIELD BY THE ENGINEER.

AREA REFLECTIVE CRACK CONTROL TREATMENT

Table with 4 columns: STA, AREA REF CR CON TREAT (SY). Lists quantities for area reflective crack control treatment.

Table with 4 columns: STA, AREA REF CR CON TREAT (SY). Lists quantities for area reflective crack control treatment.

Table with 4 columns: STA, AREA REF CR CON TREAT (SY). Lists quantities for area reflective crack control treatment.

FIBER GLASS FABRIC REPAIR SYSTEM

Table with 4 columns: STA, FIBERGLASS FAB REP SY (SY). Lists quantities for fiber glass fabric repair system.

AGGREGATE SHOULDERS, TYPE B

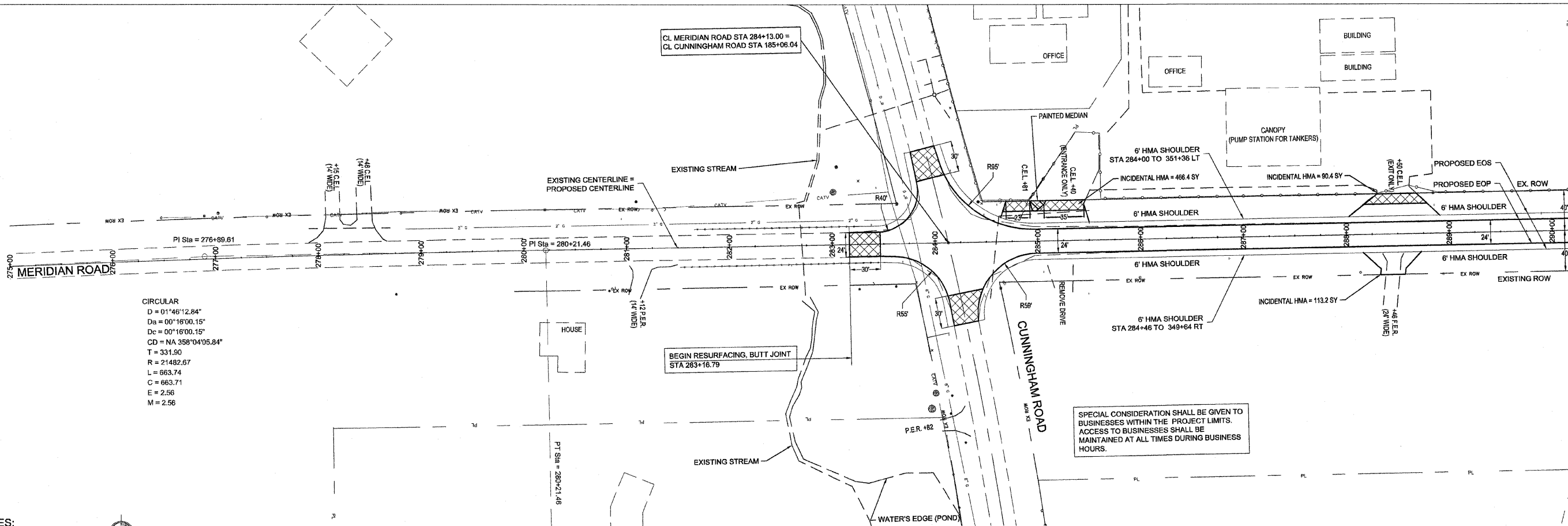
Table with 4 columns: STA, AGGREGATE SHOULDERS, TYPE B (TON). Lists quantities for aggregate shoulders.

Table with 4 columns: STA, AGGREGATE SHOULDERS, TYPE B (TON). Lists quantities for aggregate shoulders.

Table with 4 columns: STA, AGGREGATE SHOULDERS, TYPE B (TON). Lists quantities for aggregate shoulders.

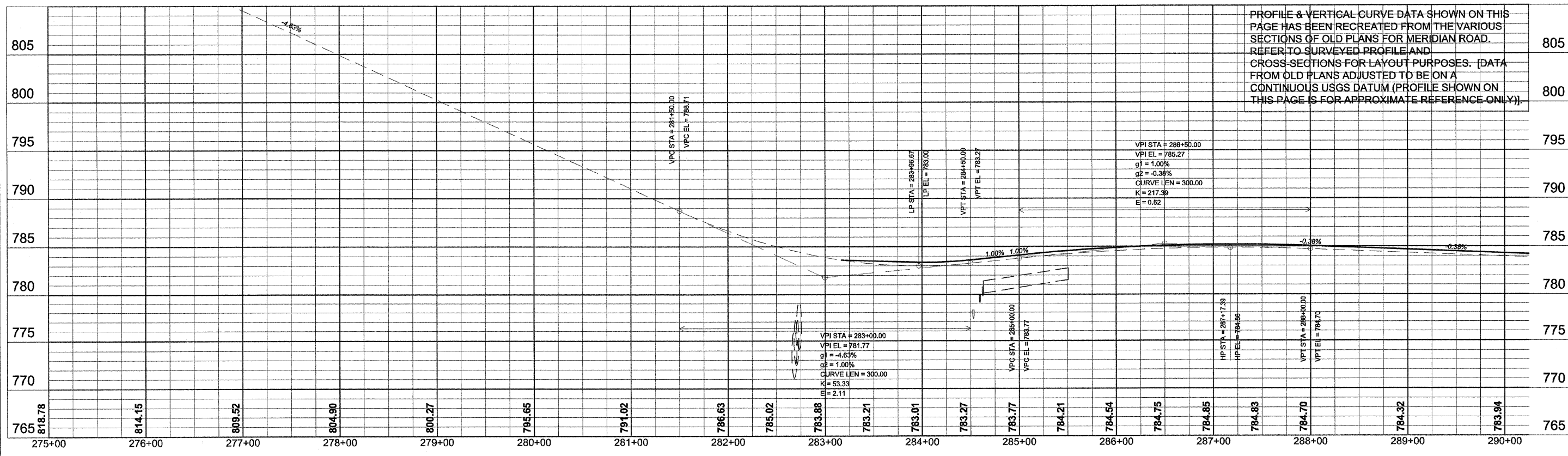
Table with 4 columns: STA, AGGREGATE SHOULDERS, TYPE B, 6" (TON). Lists quantities for aggregate shoulders.

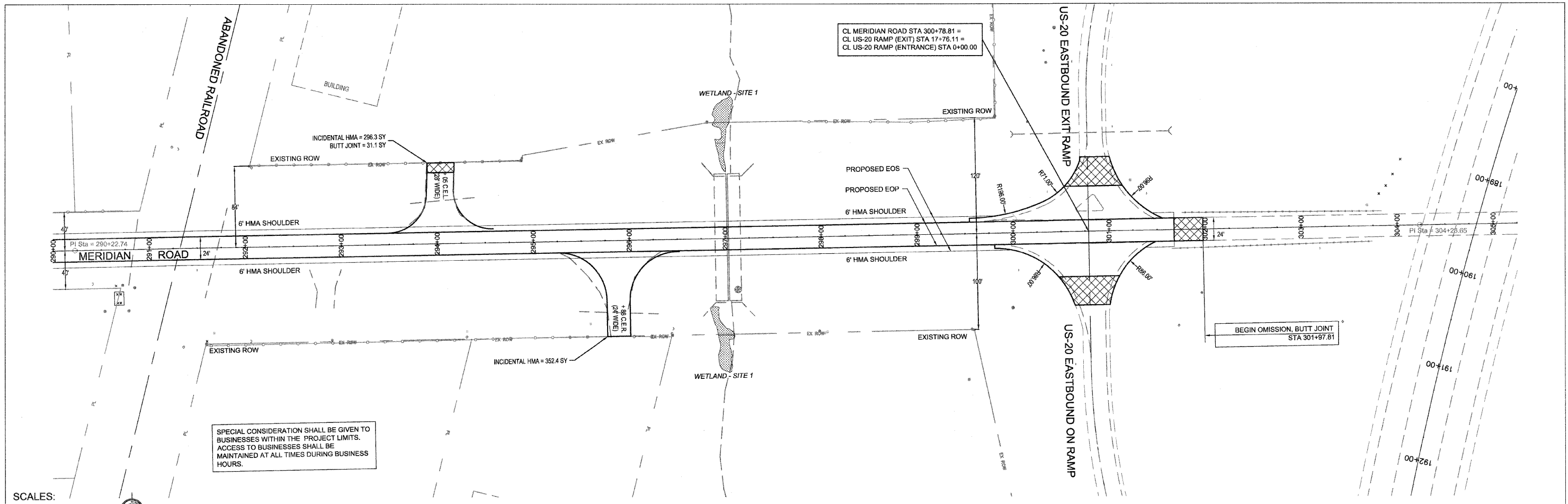
Table with 4 columns: STA, AGGREGATE SHOULDERS, TYPE B, 6" (TON). Lists quantities for aggregate shoulders.



CIRCULAR
 D = 01°46'12.84"
 Da = 00°16'00.15"
 Dc = 00°16'00.15"
 CD = NA 358°04'05.84"
 T = 331.90
 R = 21482.67
 L = 663.74
 C = 663.71
 E = 2.56
 M = 2.56

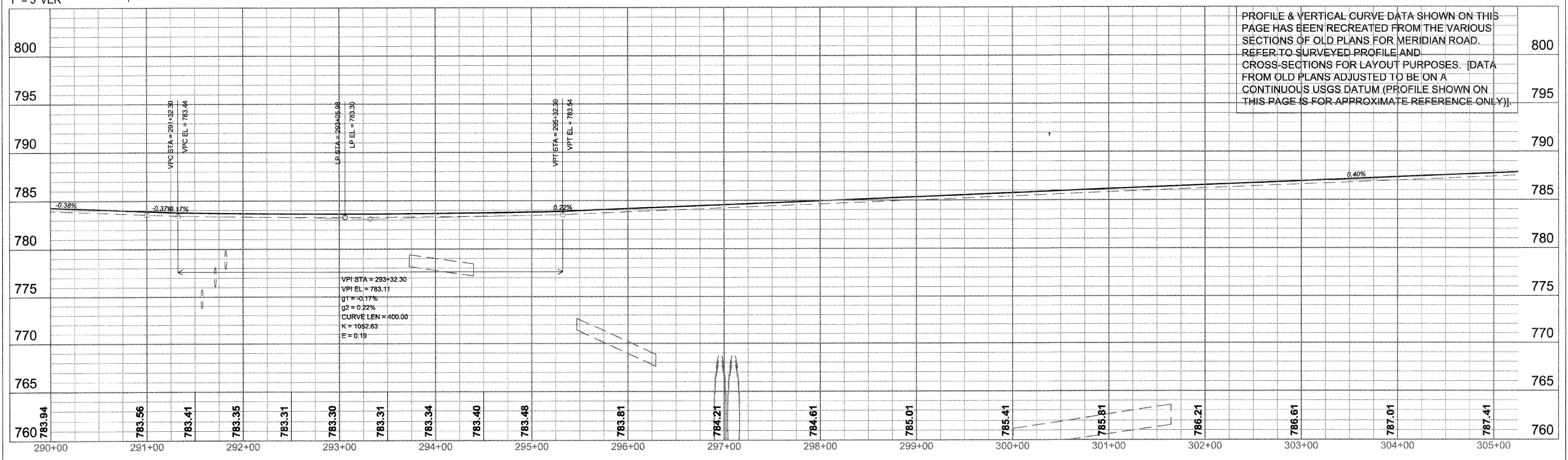
SCALES:
 1" = 50' HOR
 1" = 5' VER

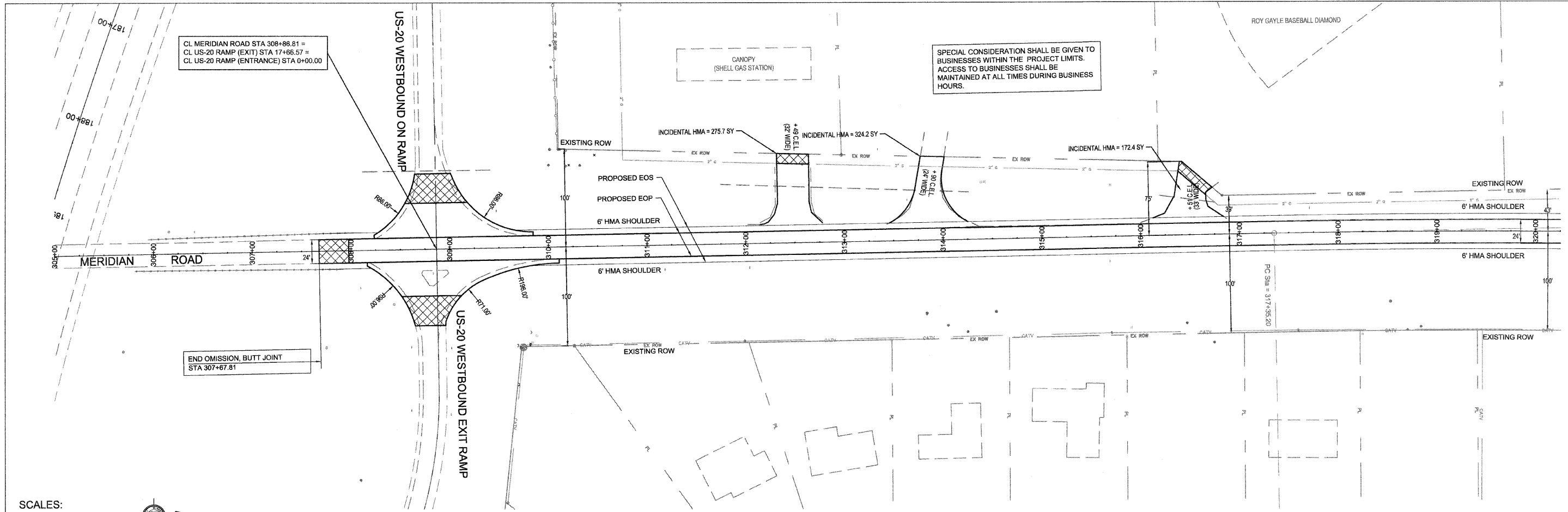




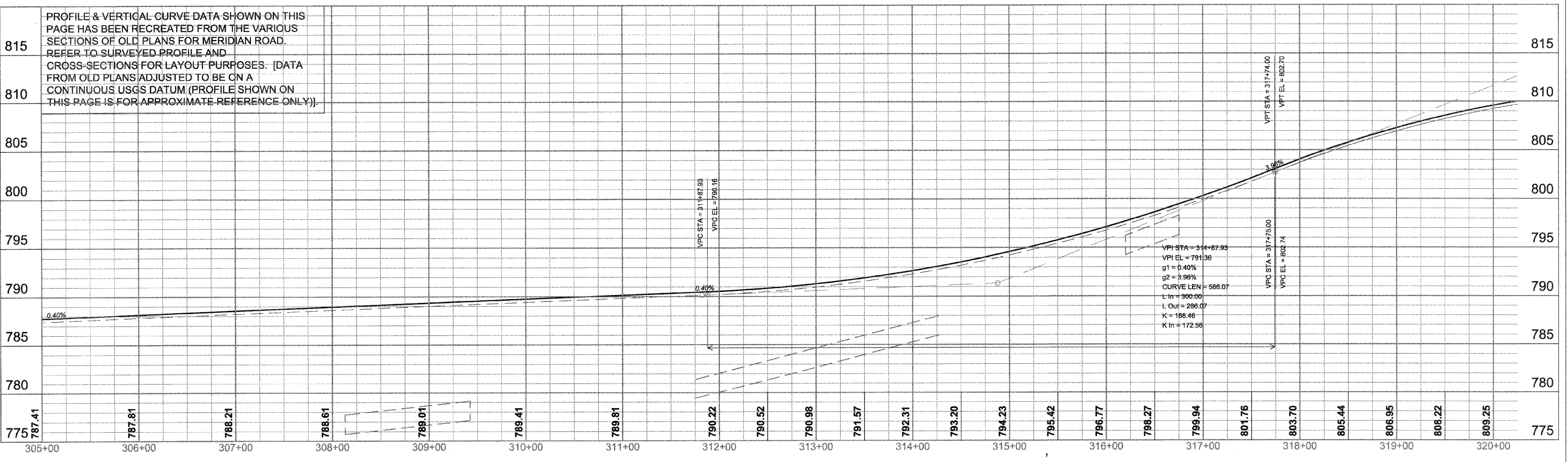
SPECIAL CONSIDERATION SHALL BE GIVEN TO BUSINESSES WITHIN THE PROJECT LIMITS. ACCESS TO BUSINESSES SHALL BE MAINTAINED AT ALL TIMES DURING BUSINESS HOURS.

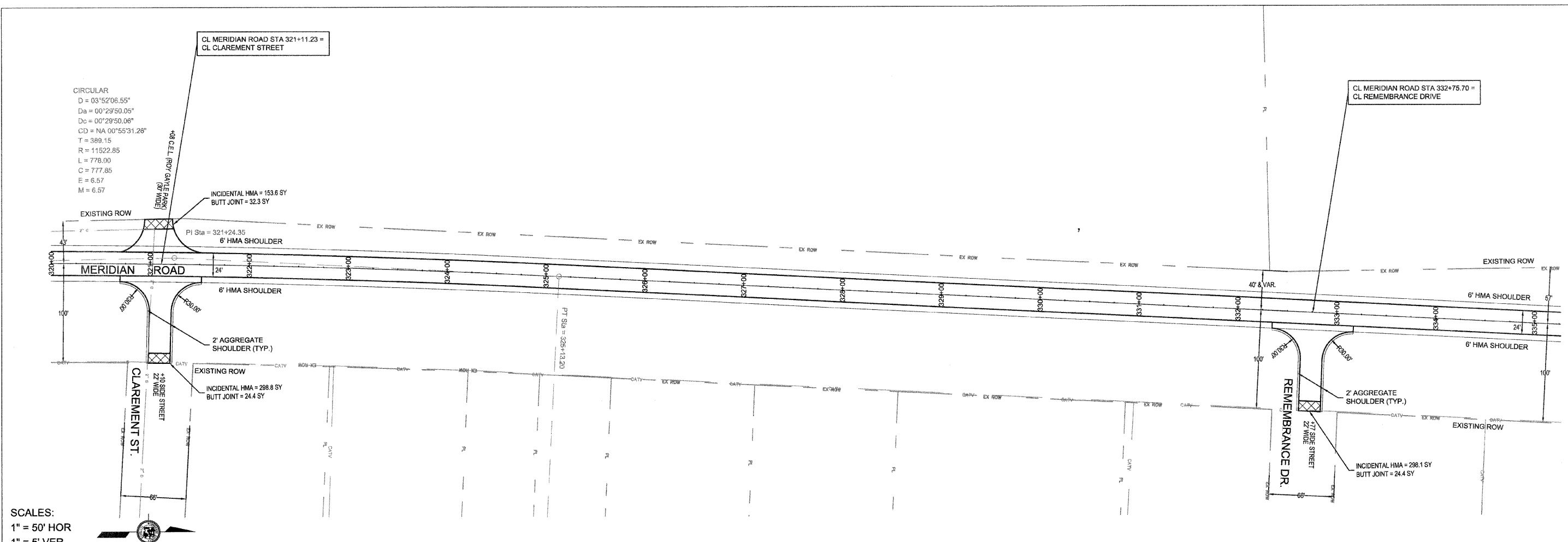
SCALES:
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 1" = 5' VER





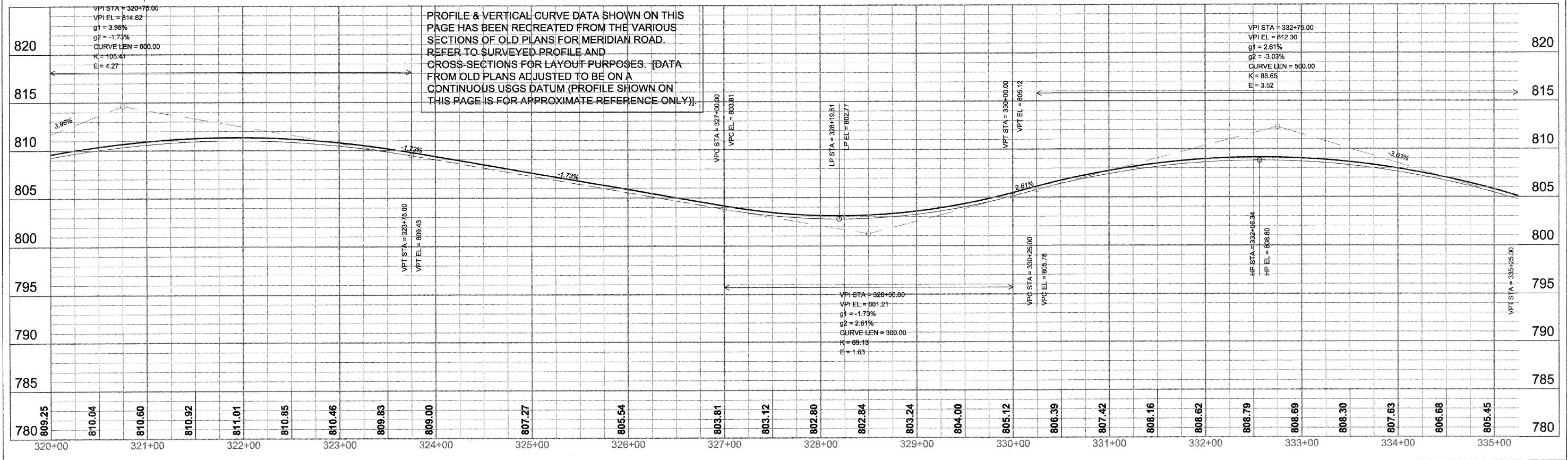
SCALES:
 1" = 50' HOR
 1" = 5' VER

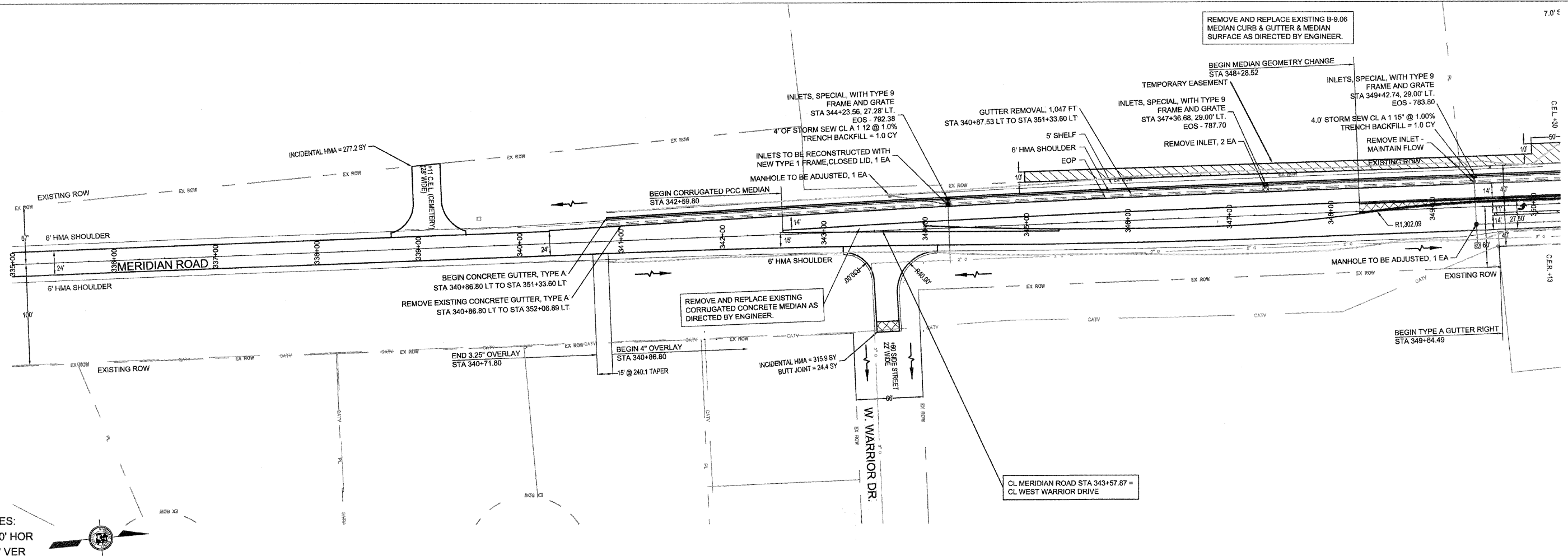




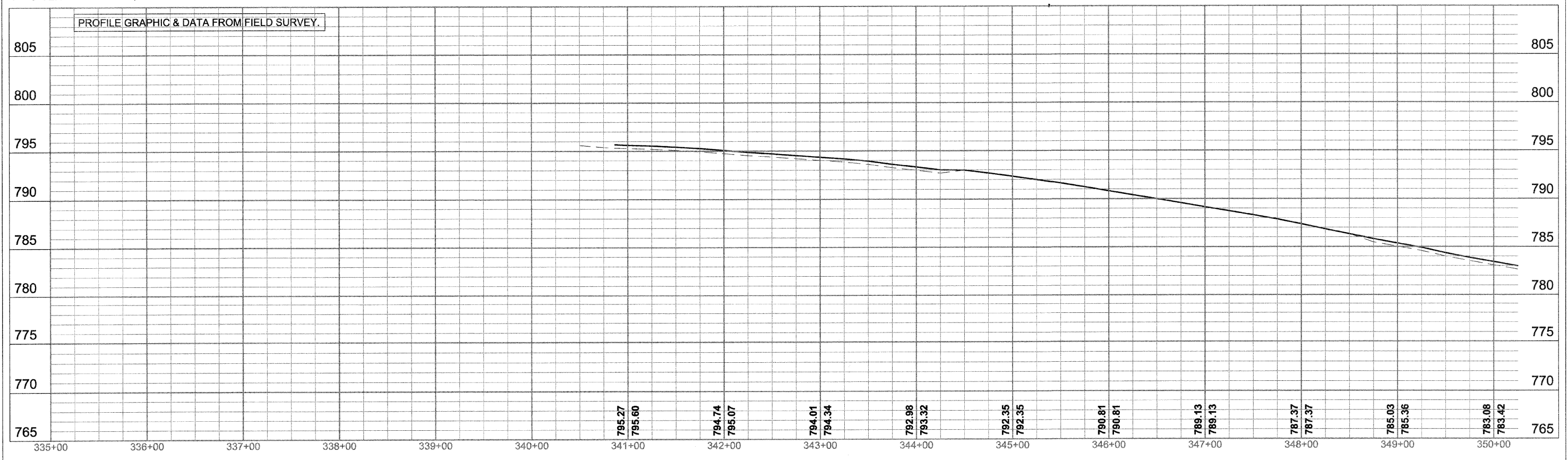
CIRCULAR
 D = 03°52'06.55"
 Da = 00°29'50.05"
 Dc = 00°29'50.06"
 CD = NA 00°55'31.26"
 T = 389.15
 R = 11522.85
 L = 778.00
 C = 777.85
 E = 6.57
 M = 6.57

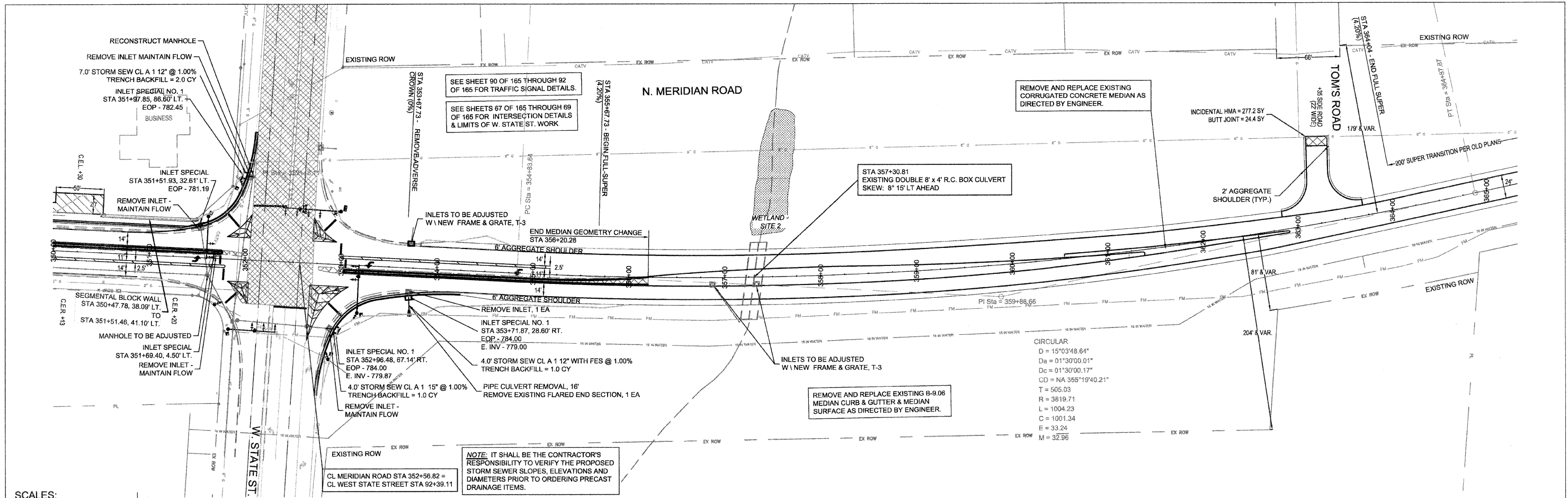
SCALES:
 1" = 50' HOR
 1" = 5' VER



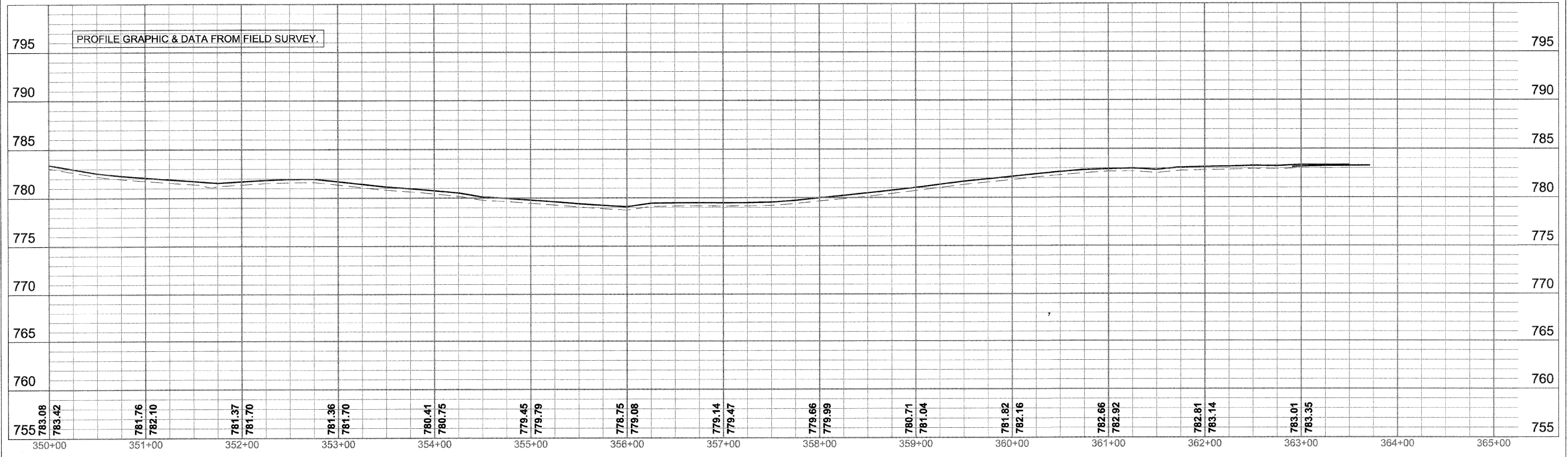


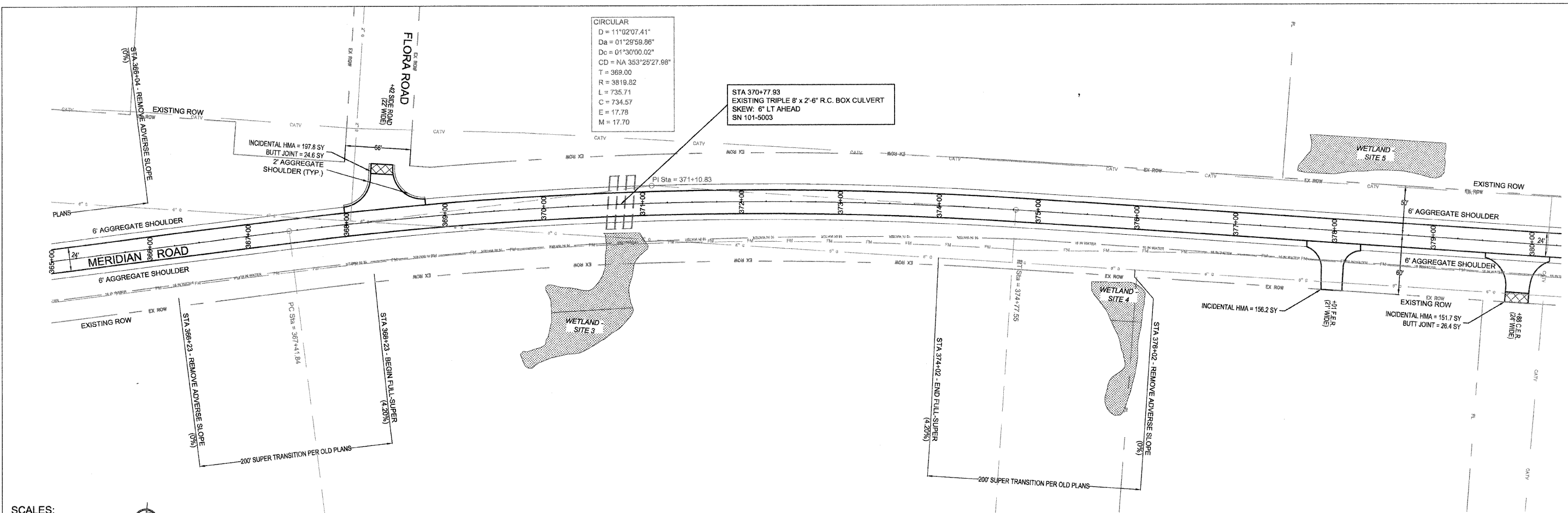
SCALES:
 1" = 50' HOR
 1" = 5' VER



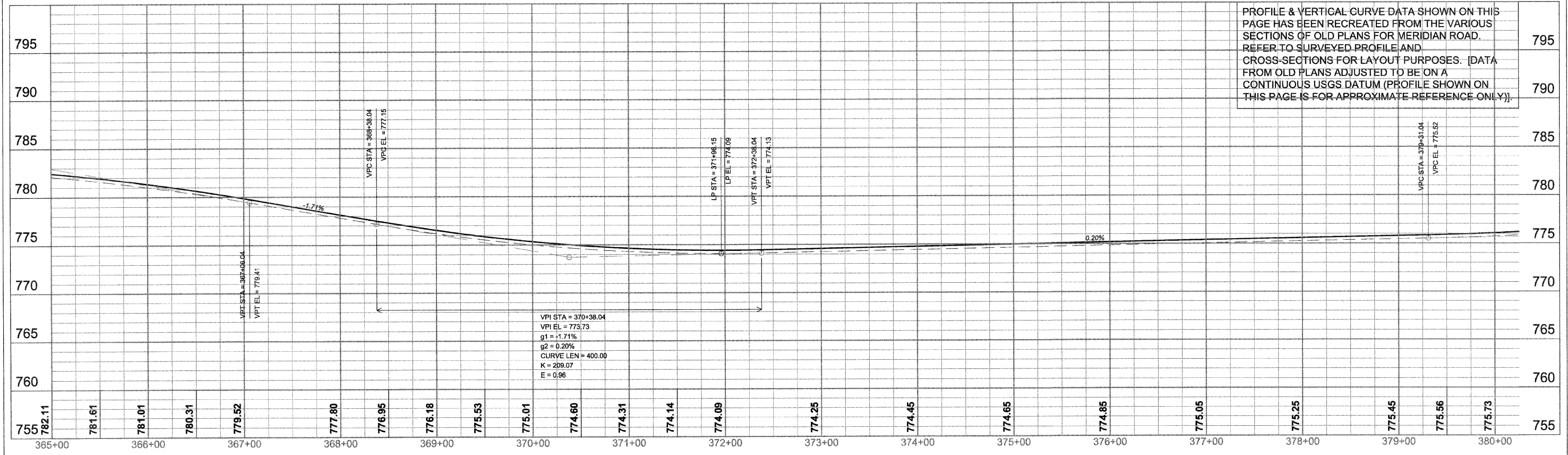


SCALES:
 1" = 50' HOR
 1" = 5' VER

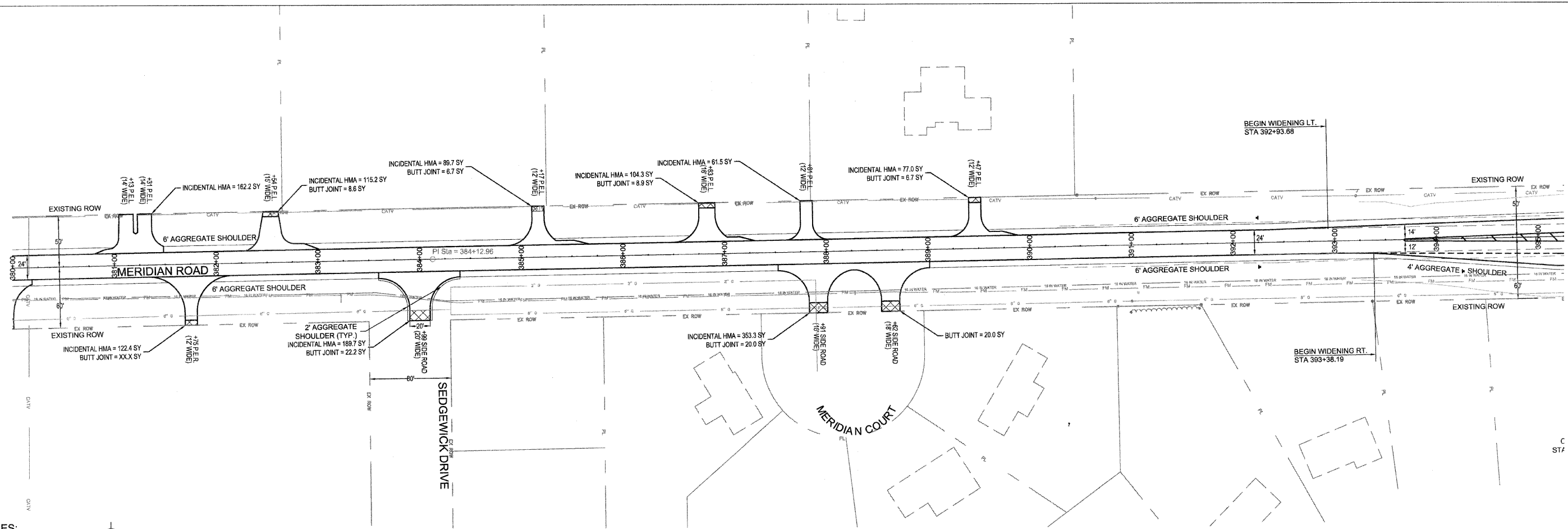




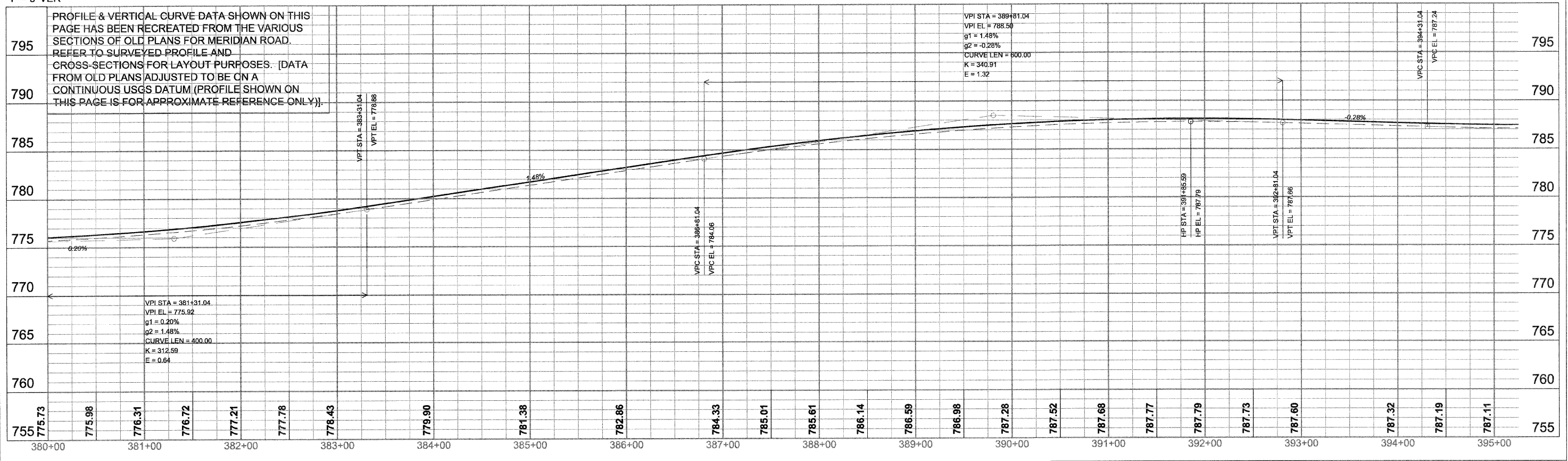
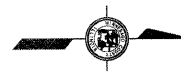
SCALES:
 1" = 50' HOR
 1" = 5' VER

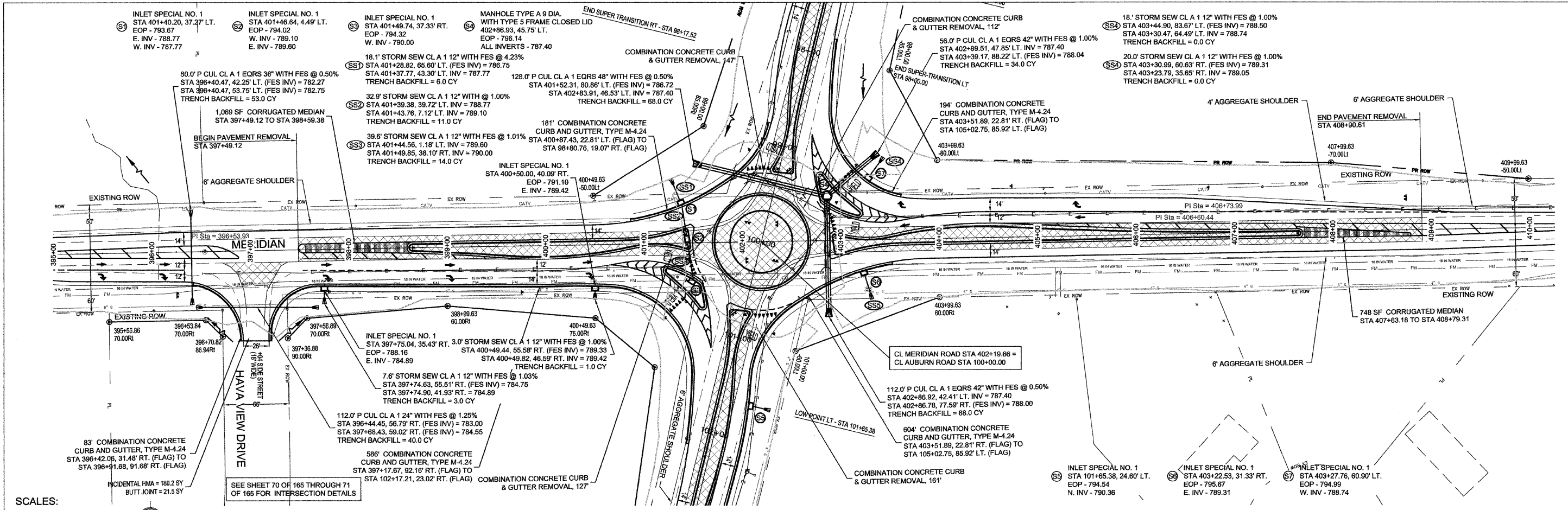


PROFILE & VERTICAL CURVE DATA SHOWN ON THIS PAGE HAS BEEN RECREATED FROM THE VARIOUS SECTIONS OF OLD PLANS FOR MERIDIAN ROAD. REFER TO SURVEYED PROFILE AND CROSS-SECTIONS FOR LAYOUT PURPOSES. [DATA FROM OLD PLANS ADJUSTED TO BE ON A CONTINUOUS USGS DATUM (PROFILE SHOWN ON THIS PAGE IS FOR APPROXIMATE REFERENCE ONLY)].

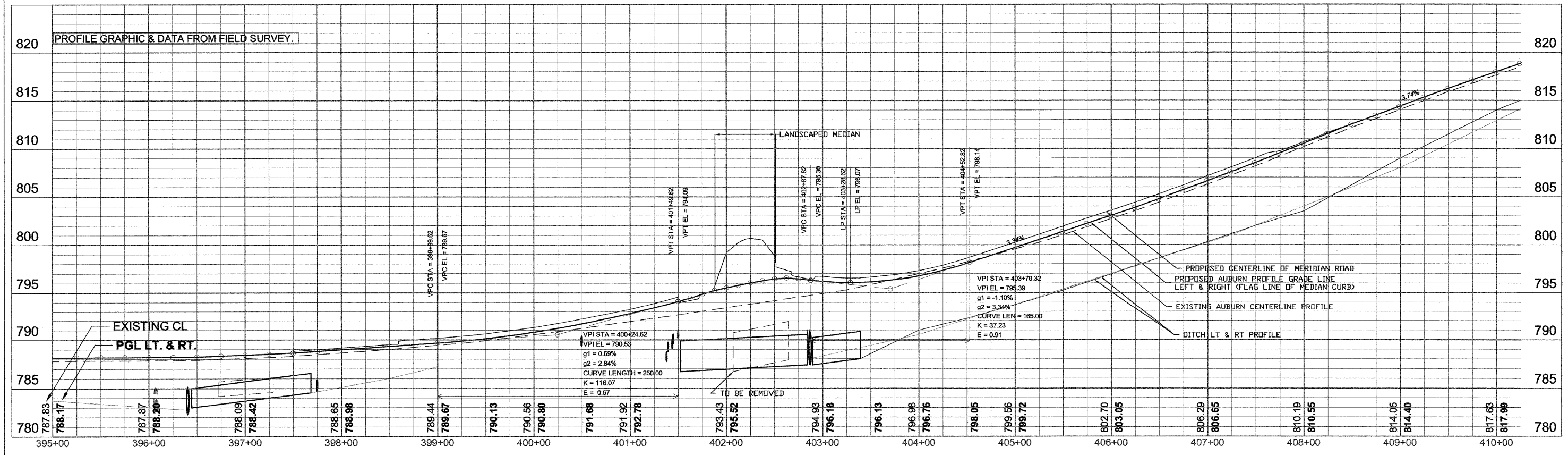


SCALES:
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 1" = 5' VER



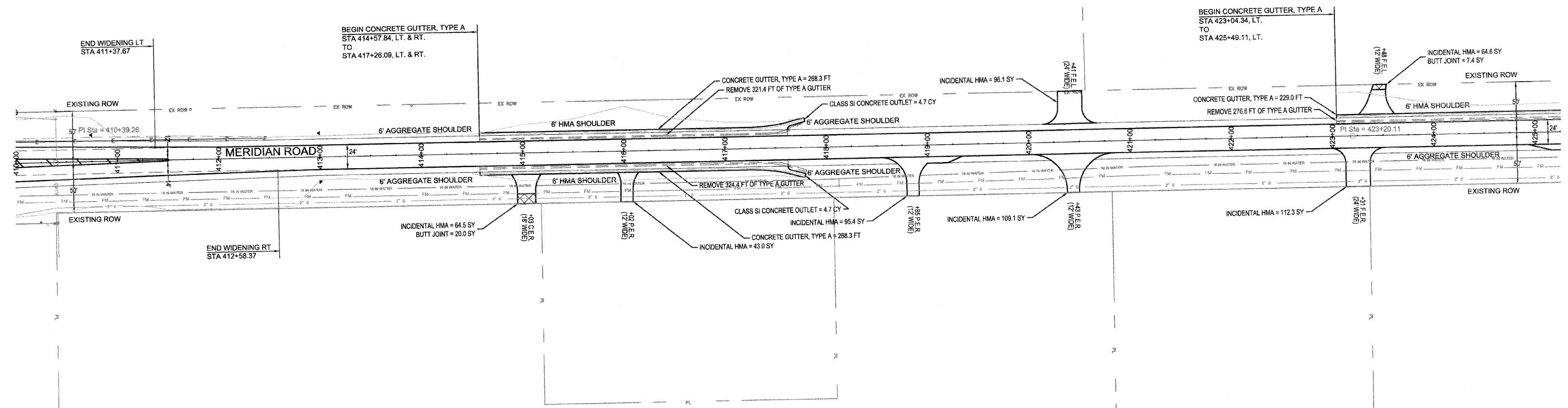


SCALES:
1" = 50' HOR
1" = 5' VER

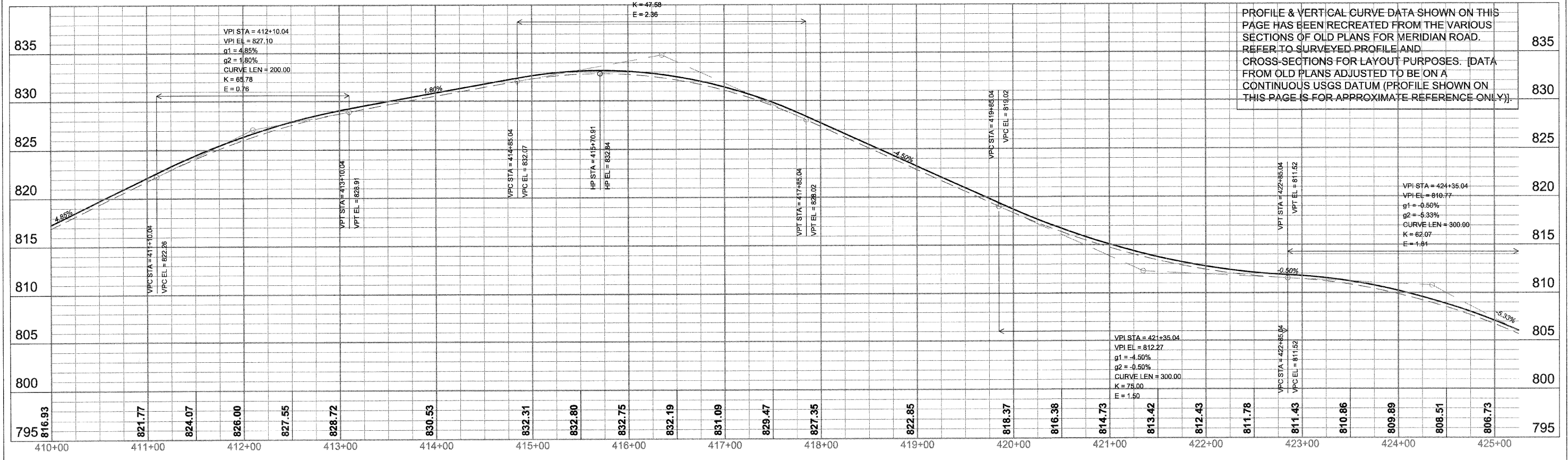


SEE CROSS-SECTION SHEET ON PAGE 120 OF 165 FOR DETAILS. ELEVATIONS ARE TO BE VERIFIED WITH ENGINEER PRIOR TO PLACING THE CONCRETE GUTTER.

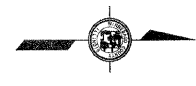
SEE CROSS-SECTION SHEET ON PAGE 121 OF 165 FOR DETAILS. ELEVATIONS ARE TO BE VERIFIED WITH ENGINEER PRIOR TO PLACING THE CONCRETE GUTTER.



SCALES:
 1" = 50' HOR
 1" = 5' VER



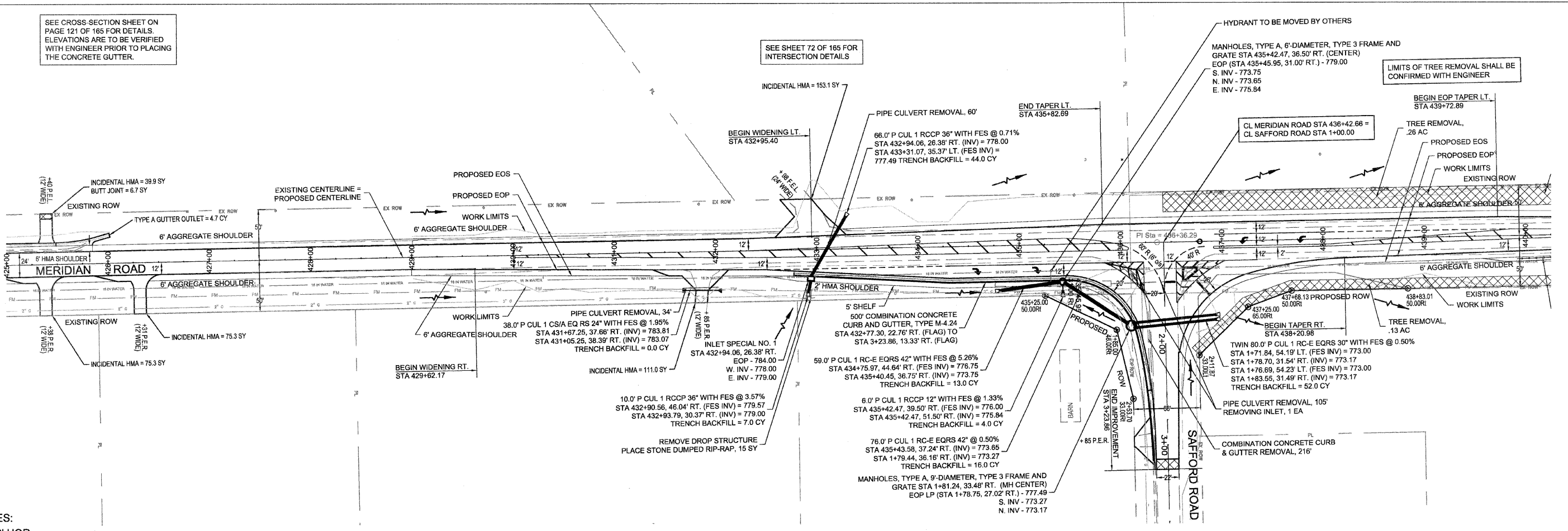
PROFILE & VERTICAL CURVE DATA SHOWN ON THIS PAGE HAS BEEN RECREATED FROM THE VARIOUS SECTIONS OF OLD PLANS FOR MERIDIAN ROAD. REFER TO SURVEYED PROFILE AND CROSS-SECTIONS FOR LAYOUT PURPOSES. [DATA FROM OLD PLANS ADJUSTED TO BE ON A CONTINUOUS USGS DATUM (PROFILE SHOWN ON THIS PAGE IS FOR APPROXIMATE REFERENCE ONLY)].



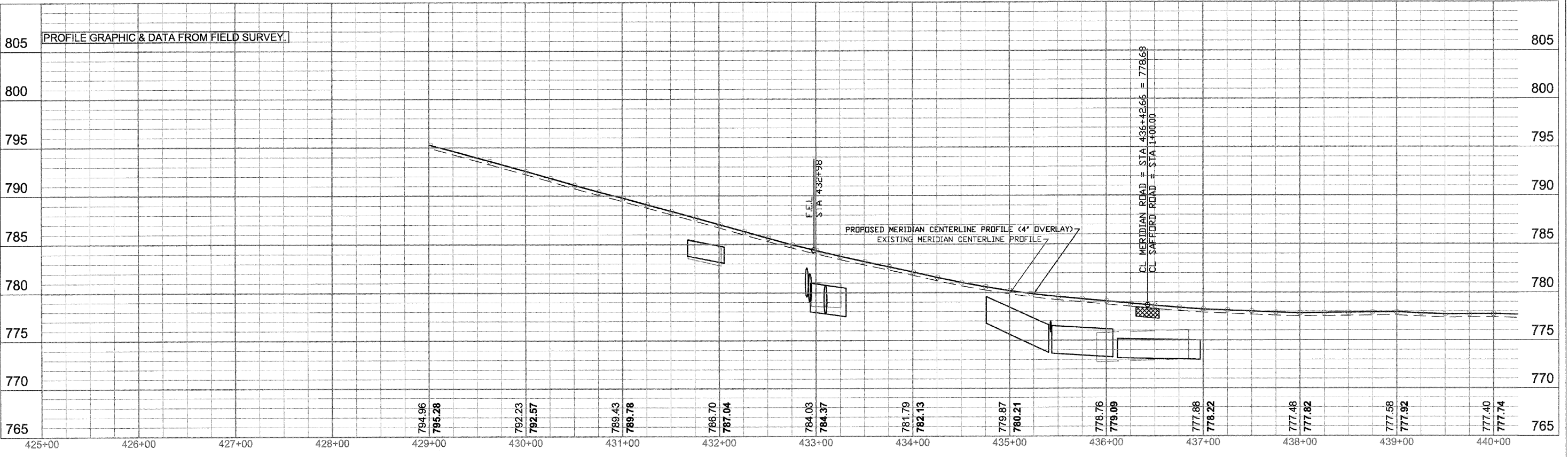
SEE CROSS-SECTION SHEET ON PAGE 121 OF 165 FOR DETAILS. ELEVATIONS ARE TO BE VERIFIED WITH ENGINEER PRIOR TO PLACING THE CONCRETE GUTTER.

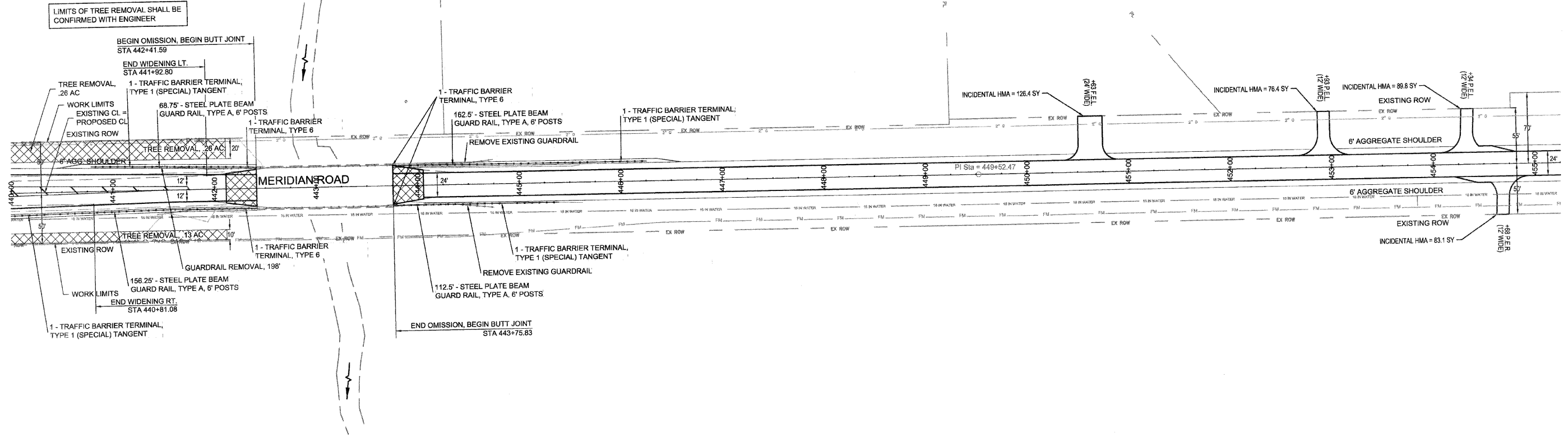
SEE SHEET 72 OF 165 FOR INTERSECTION DETAILS

LIMITS OF TREE REMOVAL SHALL BE CONFIRMED WITH ENGINEER

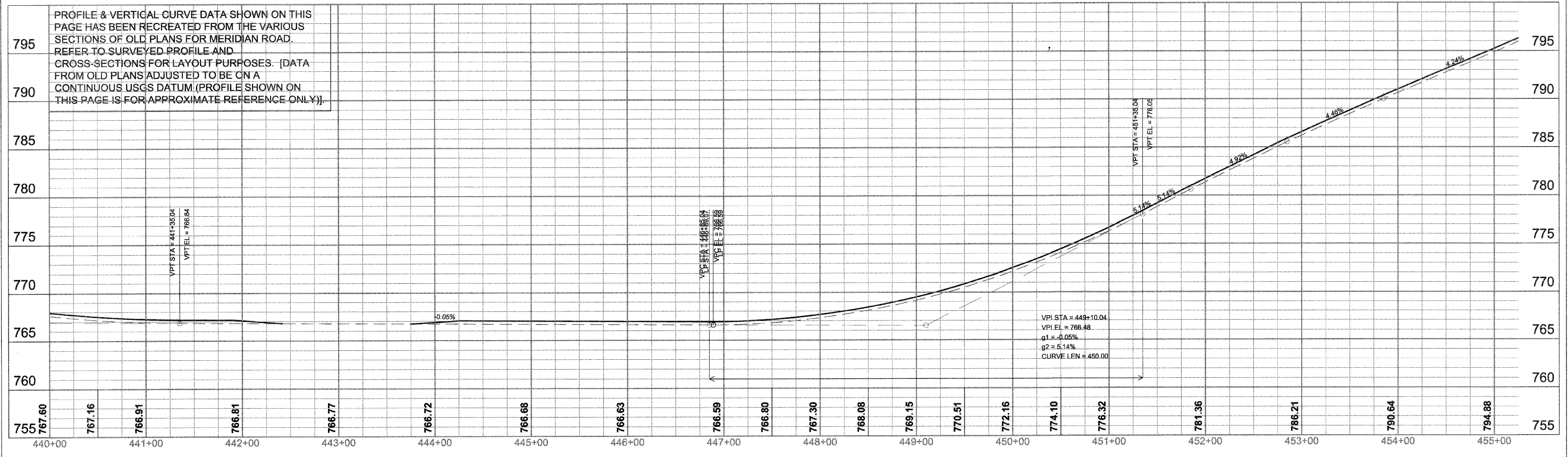


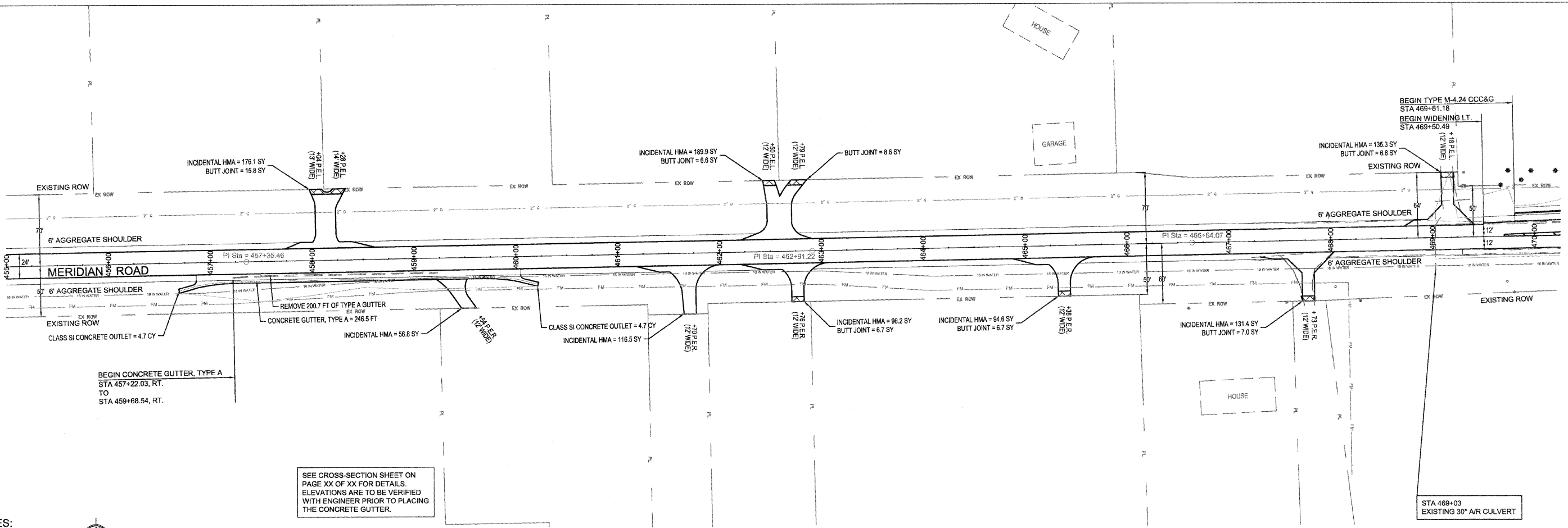
SCALES:
 1" = 50' HOR
 1" = 5' VER





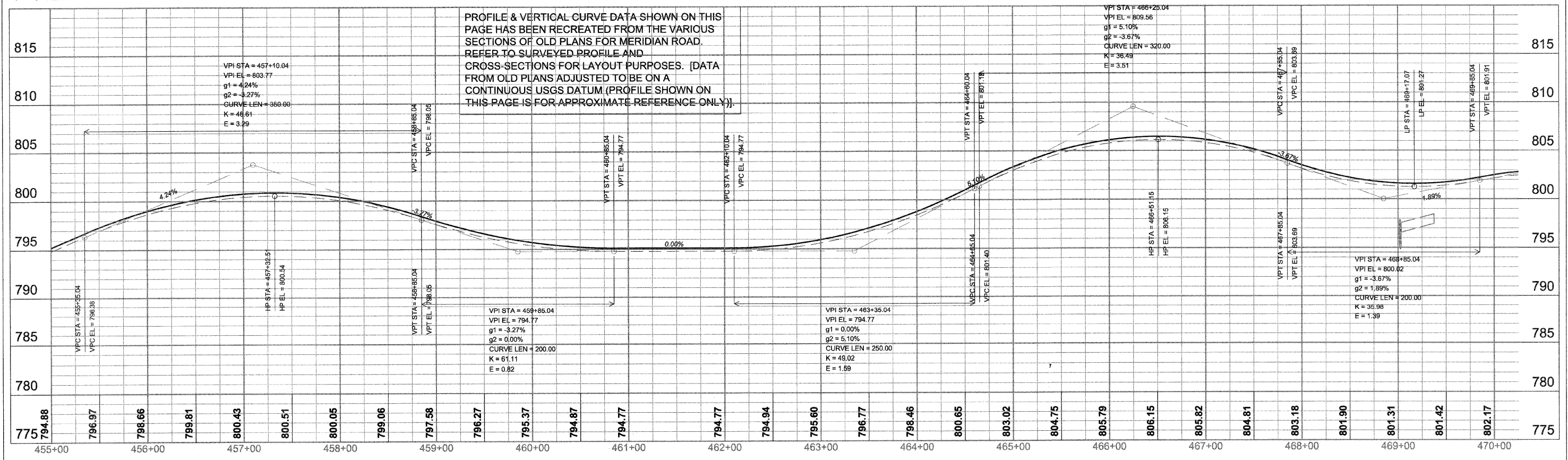
SCALES:
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 1" = 5' VER

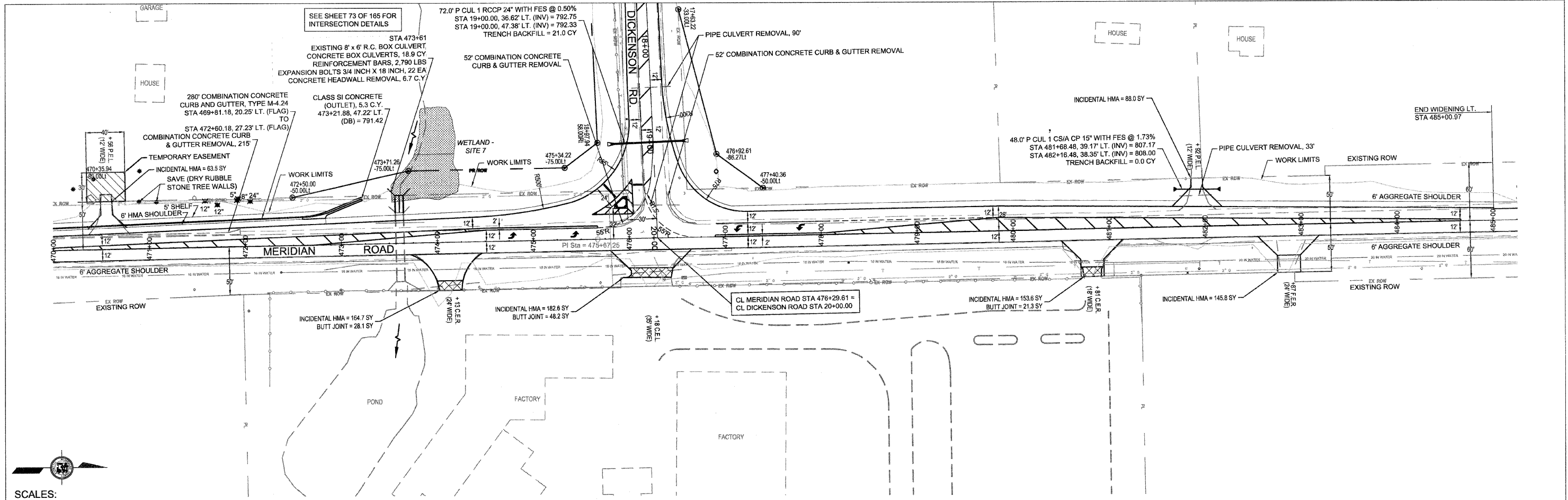




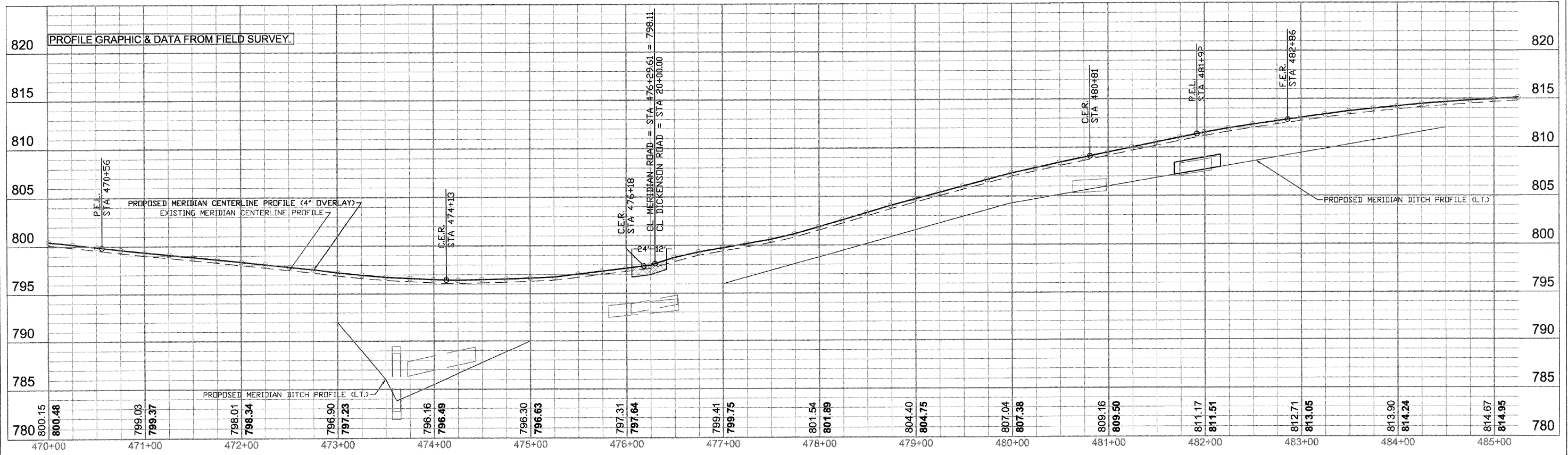
SEE CROSS-SECTION SHEET ON PAGE XX OF XX FOR DETAILS. ELEVATIONS ARE TO BE VERIFIED WITH ENGINEER PRIOR TO PLACING THE CONCRETE GUTTER.

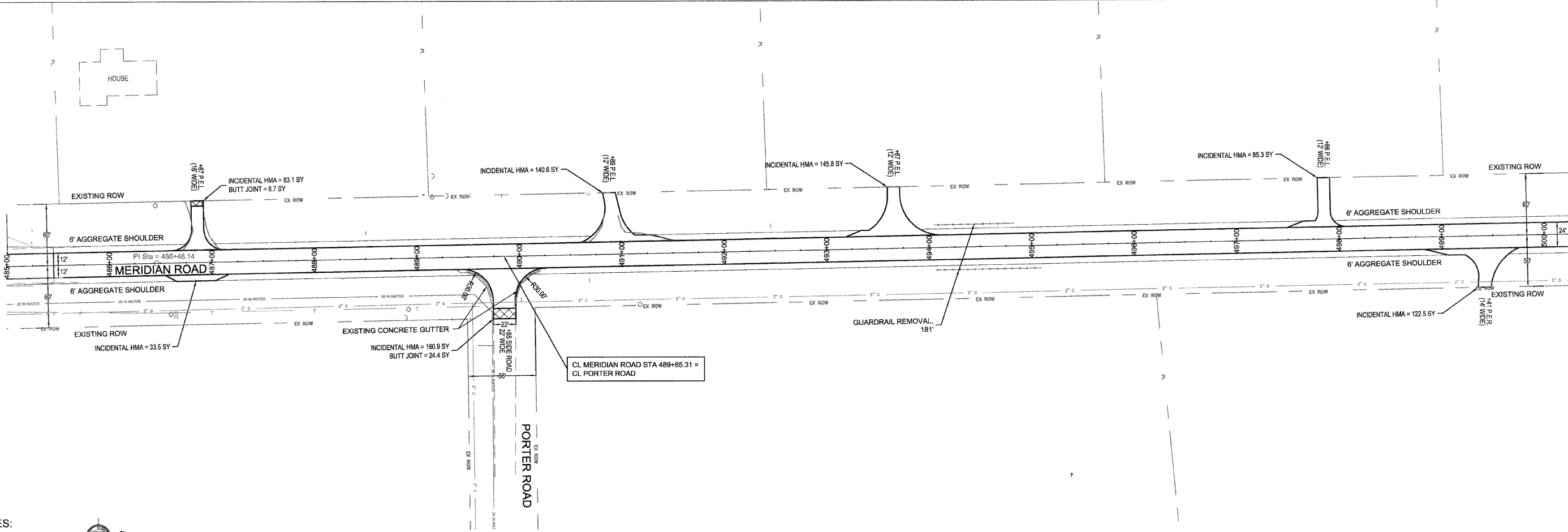
SCALES:
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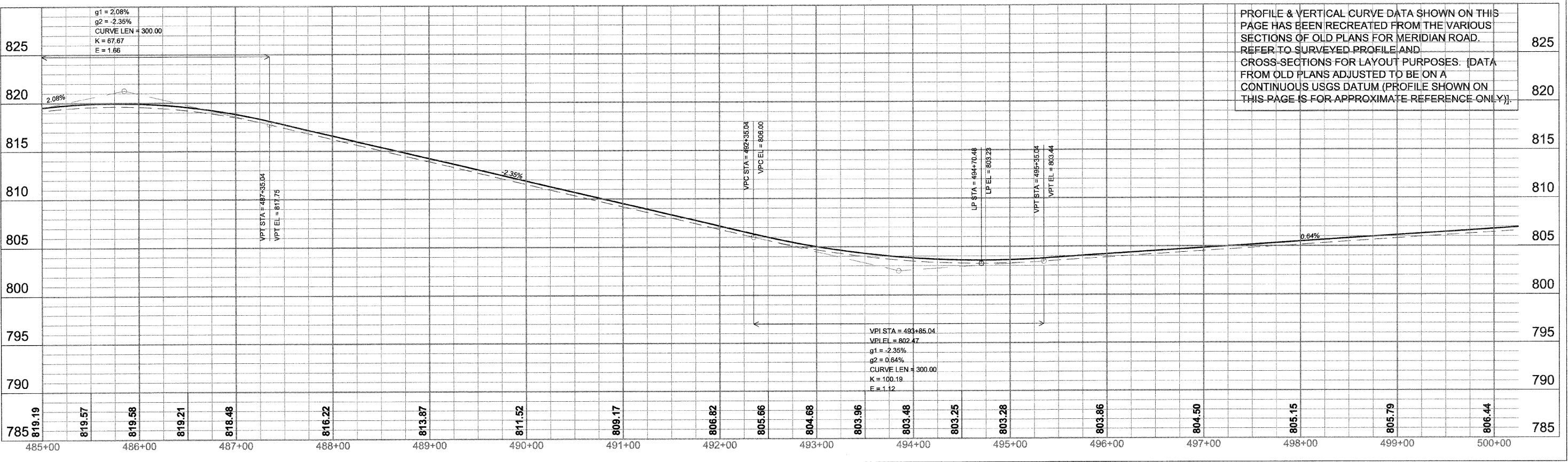


SCALES:
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 1" = 5' VER

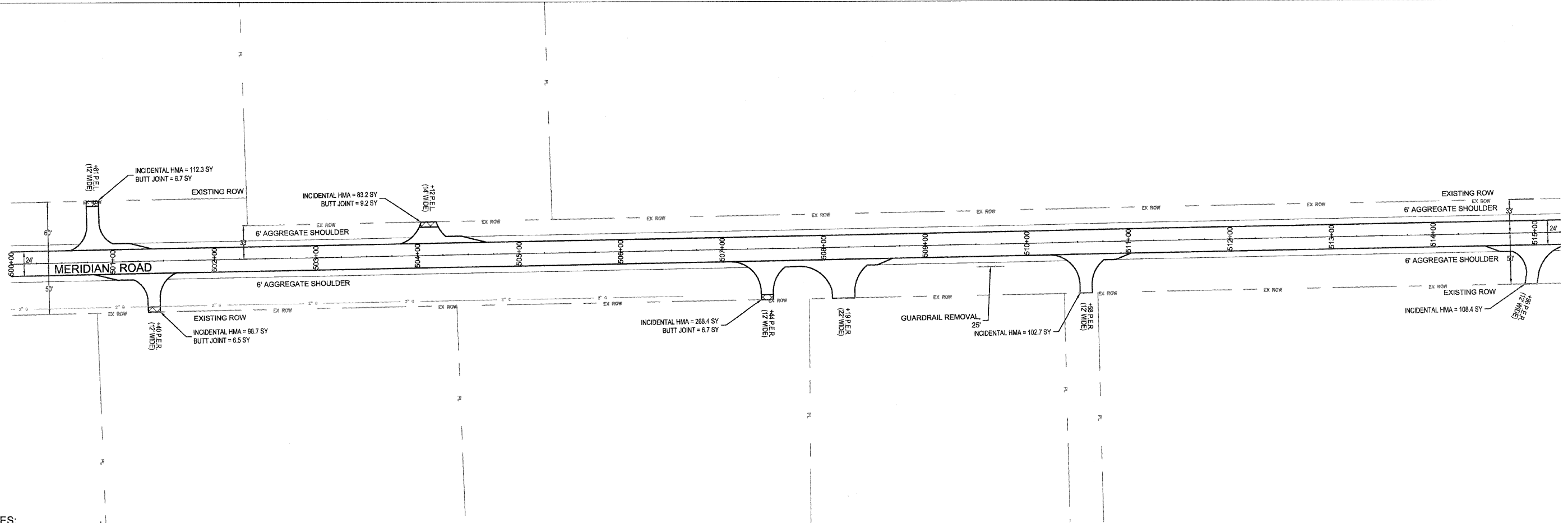




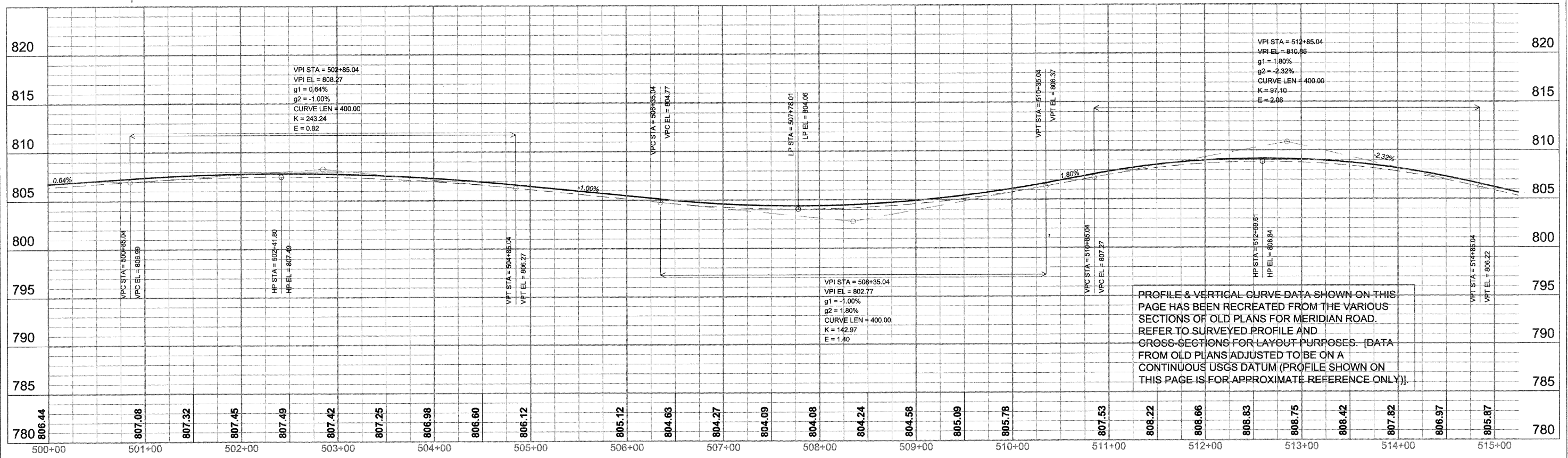
SCALES:
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 1" = 5' VER



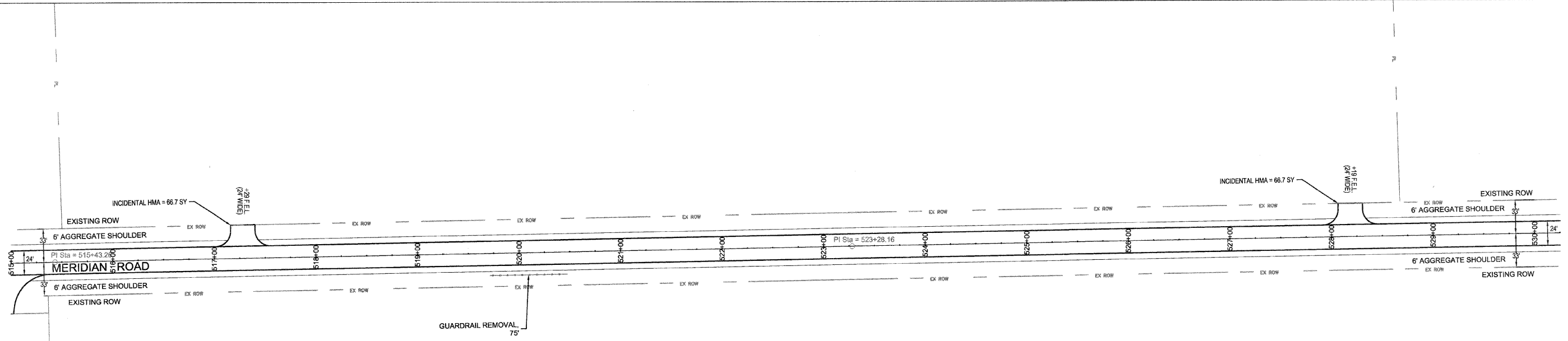
PROFILE & VERTICAL CURVE DATA SHOWN ON THIS PAGE HAS BEEN RECREATED FROM THE VARIOUS SECTIONS OF OLD PLANS FOR MERIDIAN ROAD. REFER TO SURVEYED PROFILE AND CROSS-SECTIONS FOR LAYOUT PURPOSES. [DATA FROM OLD PLANS ADJUSTED TO BE ON A CONTINUOUS USGS DATUM (PROFILE SHOWN ON THIS PAGE IS FOR APPROXIMATE REFERENCE ONLY)].



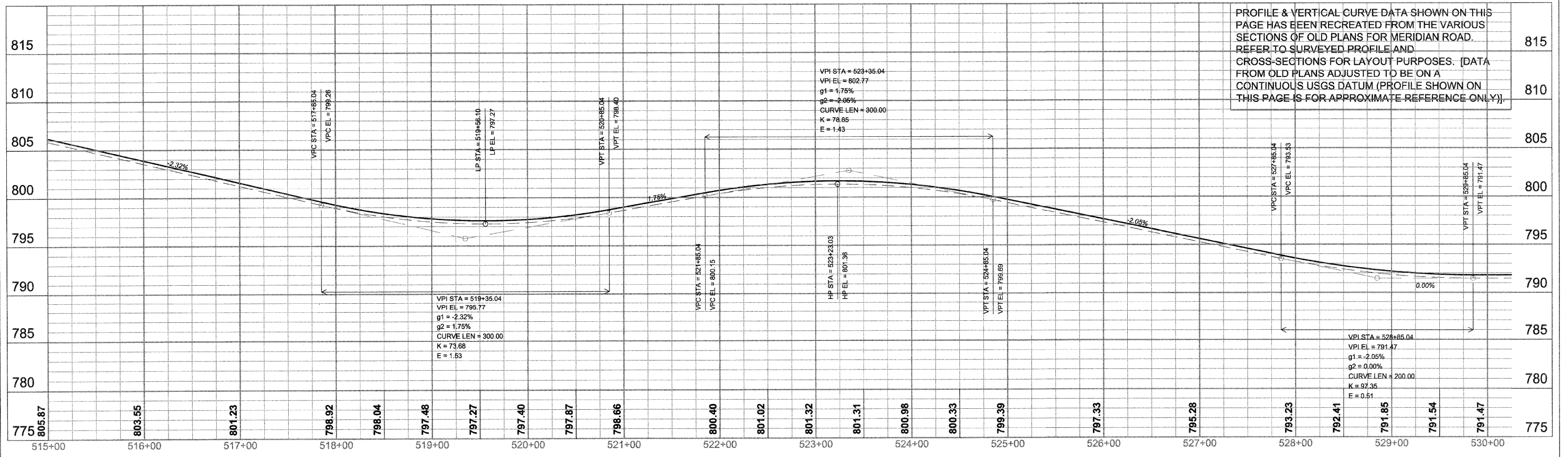
SCALES:
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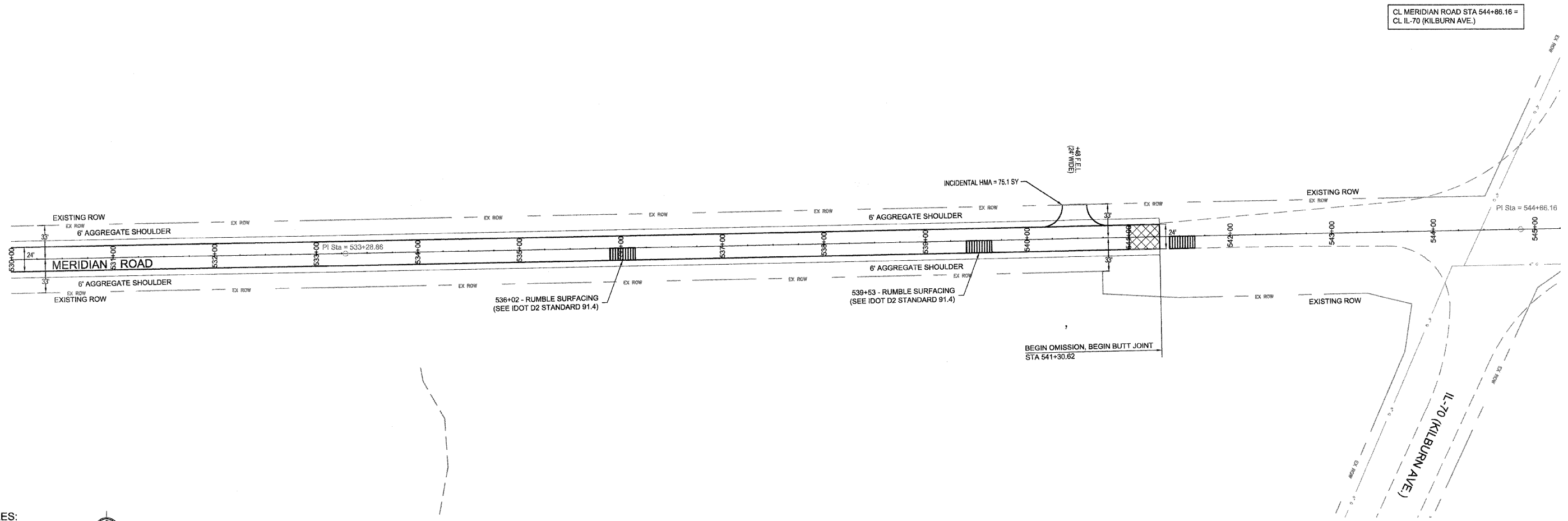
PROFILE & VERTICAL CURVE DATA SHOWN ON THIS PAGE HAS BEEN RECREATED FROM THE VARIOUS SECTIONS OF OLD PLANS FOR MERIDIAN ROAD. REFER TO SURVEYED PROFILE AND CROSS-SECTIONS FOR LAYOUT PURPOSES. [DATA FROM OLD PLANS ADJUSTED TO BE ON A CONTINUOUS USGS DATUM (PROFILE SHOWN ON THIS PAGE IS FOR APPROXIMATE REFERENCE ONLY)].



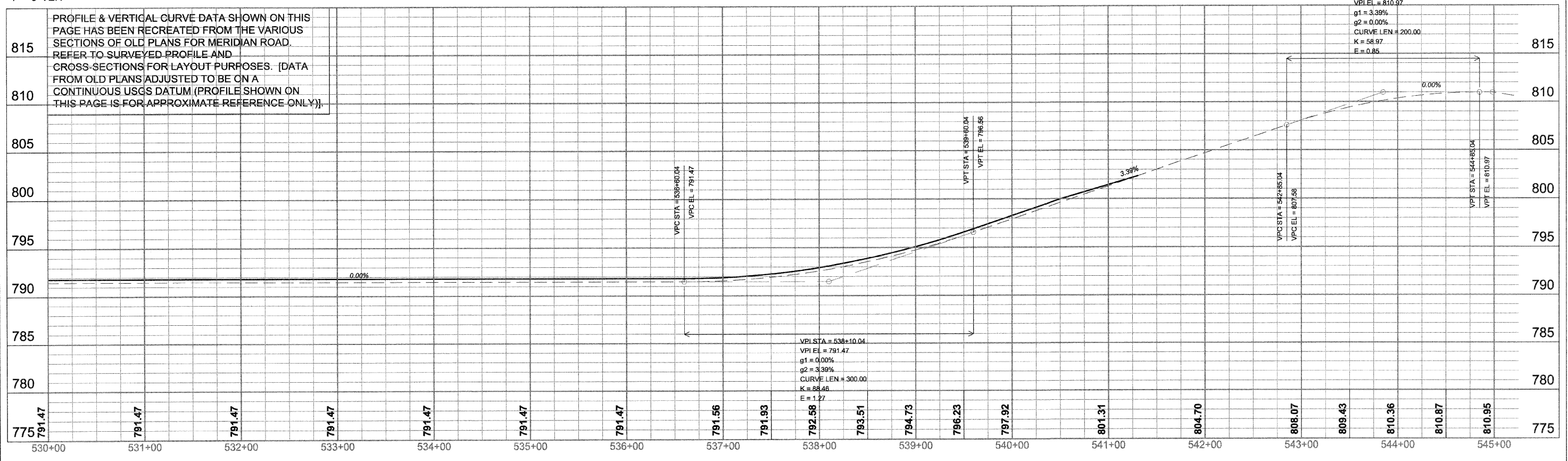
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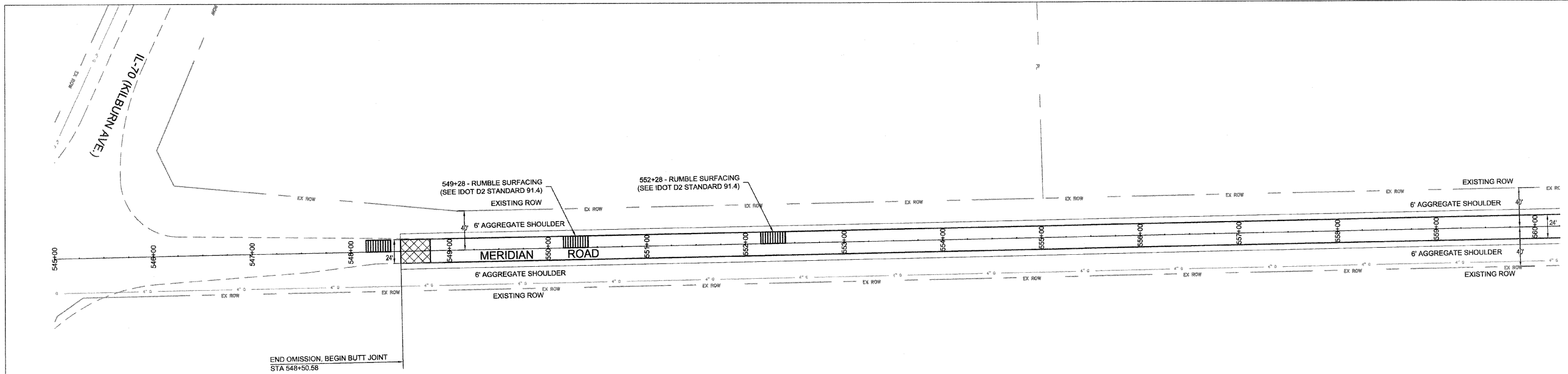


PROFILE & VERTICAL CURVE DATA SHOWN ON THIS PAGE HAS BEEN RECREATED FROM THE VARIOUS SECTIONS OF OLD PLANS FOR MERIDIAN ROAD. REFER TO SURVEYED PROFILE AND CROSS-SECTIONS FOR LAYOUT PURPOSES. [DATA FROM OLD PLANS ADJUSTED TO BE ON A CONTINUOUS USGS DATUM (PROFILE SHOWN ON THIS PAGE IS FOR APPROXIMATE REFERENCE ONLY)].

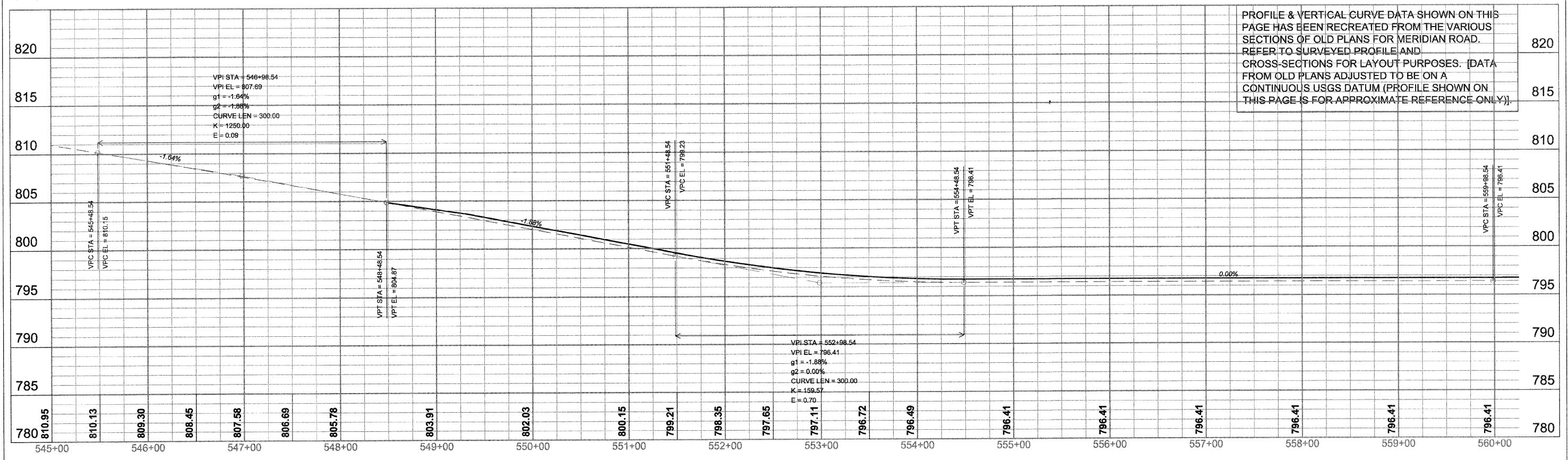


SCALES:
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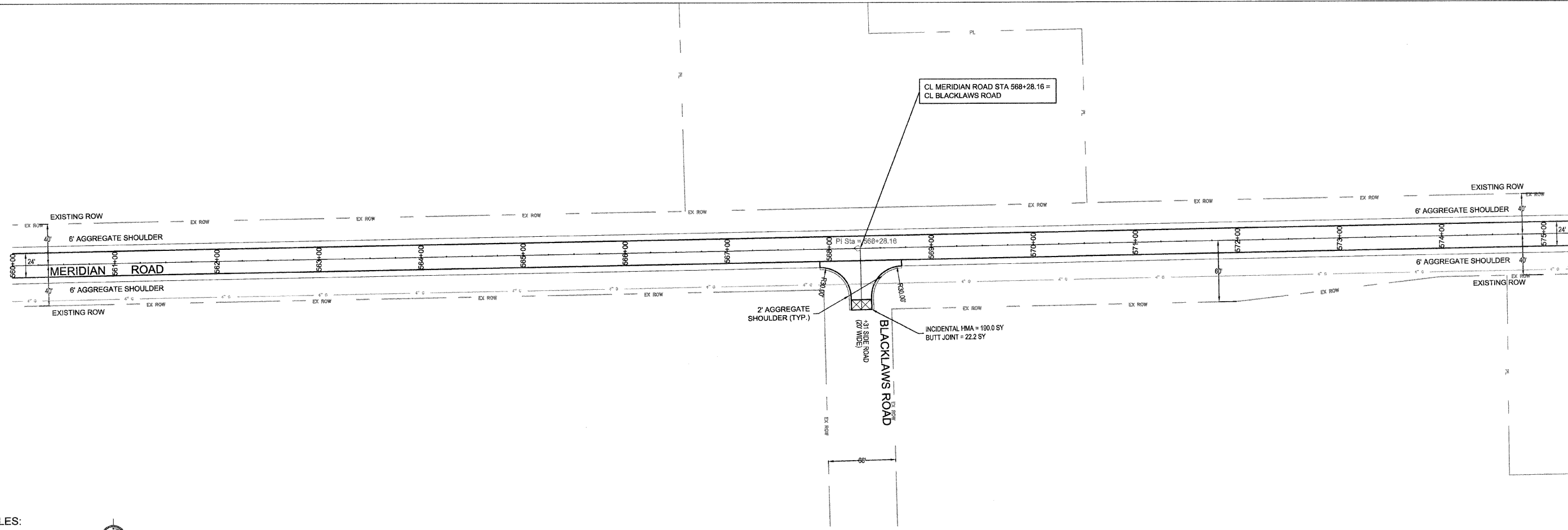




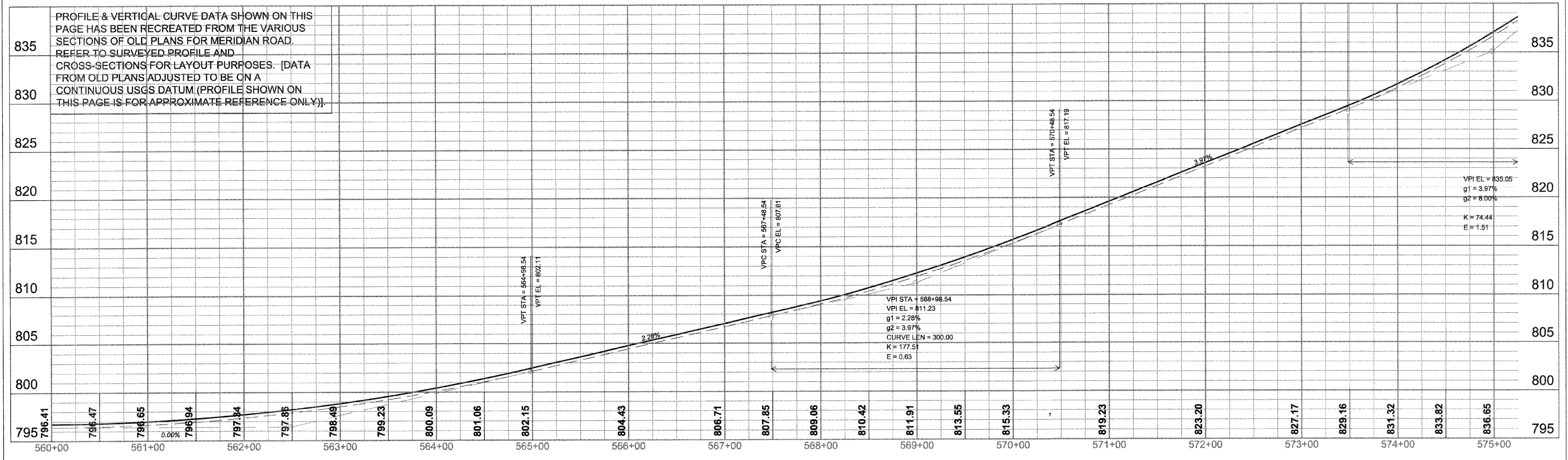
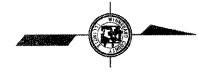
CL MERIDIAN ROAD STA 544+86.16 = CL PORTER ROAD
 SCALES:
 1" = 50' HOR
 1" = 5' VER

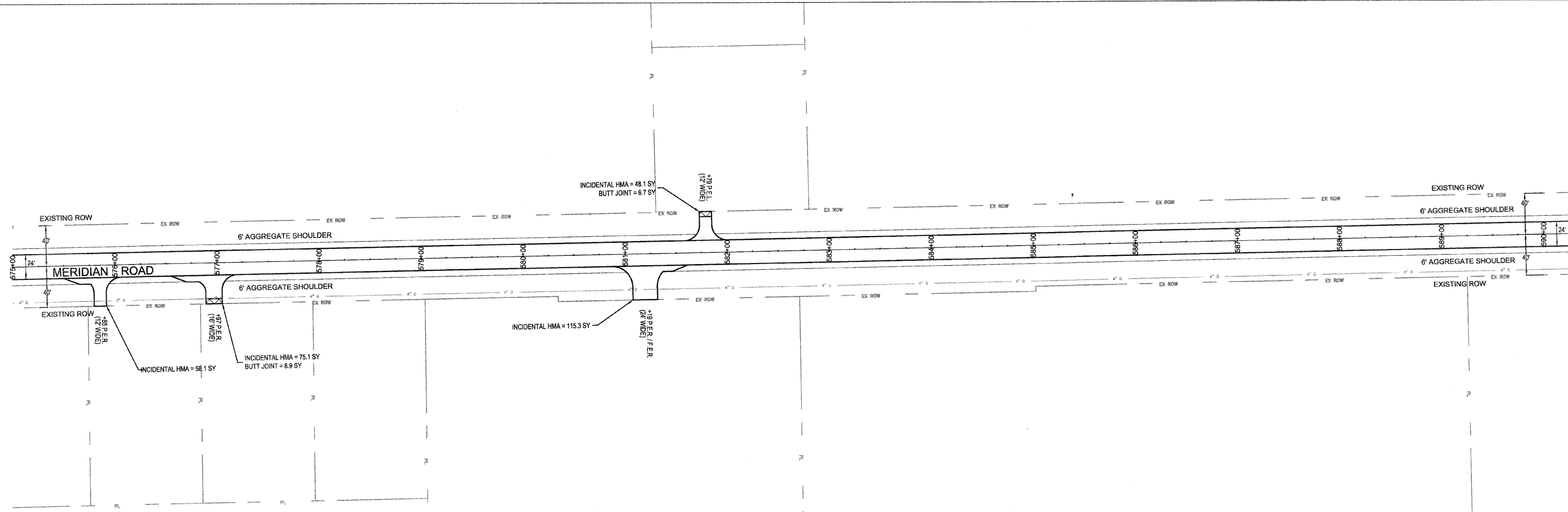


PROFILE & VERTICAL CURVE DATA SHOWN ON THIS PAGE HAS BEEN RECREATED FROM THE VARIOUS SECTIONS OF OLD PLANS FOR MERIDIAN ROAD. REFER TO SURVEYED PROFILE AND CROSS-SECTIONS FOR LAYOUT PURPOSES. [DATA FROM OLD PLANS ADJUSTED TO BE ON A CONTINUOUS USGS DATUM (PROFILE SHOWN ON THIS PAGE IS FOR APPROXIMATE REFERENCE ONLY)].

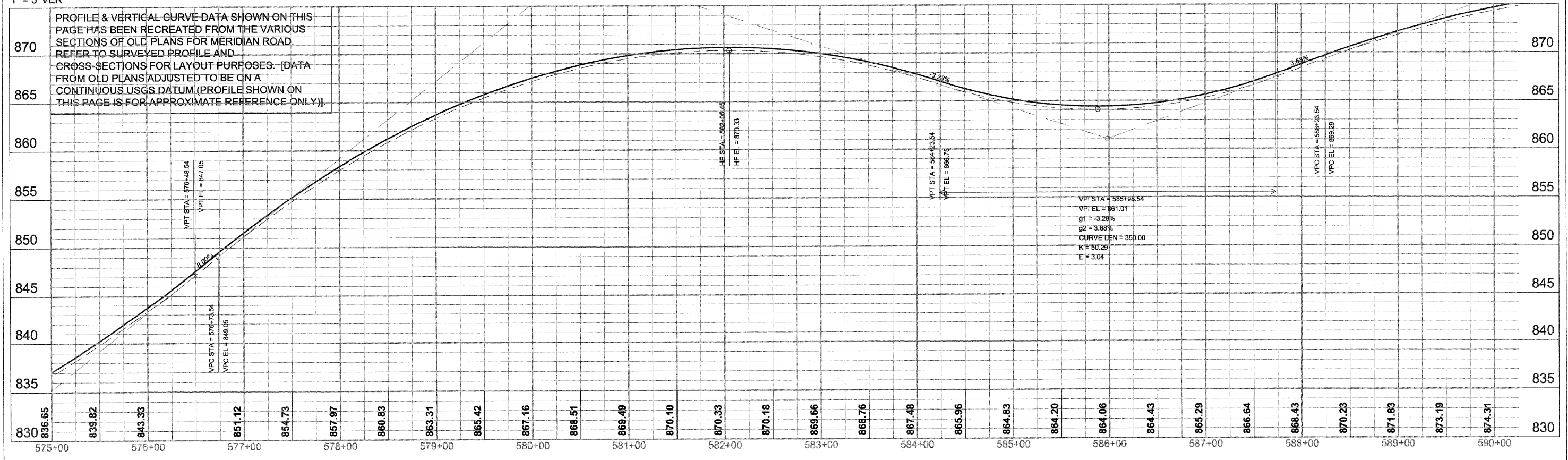


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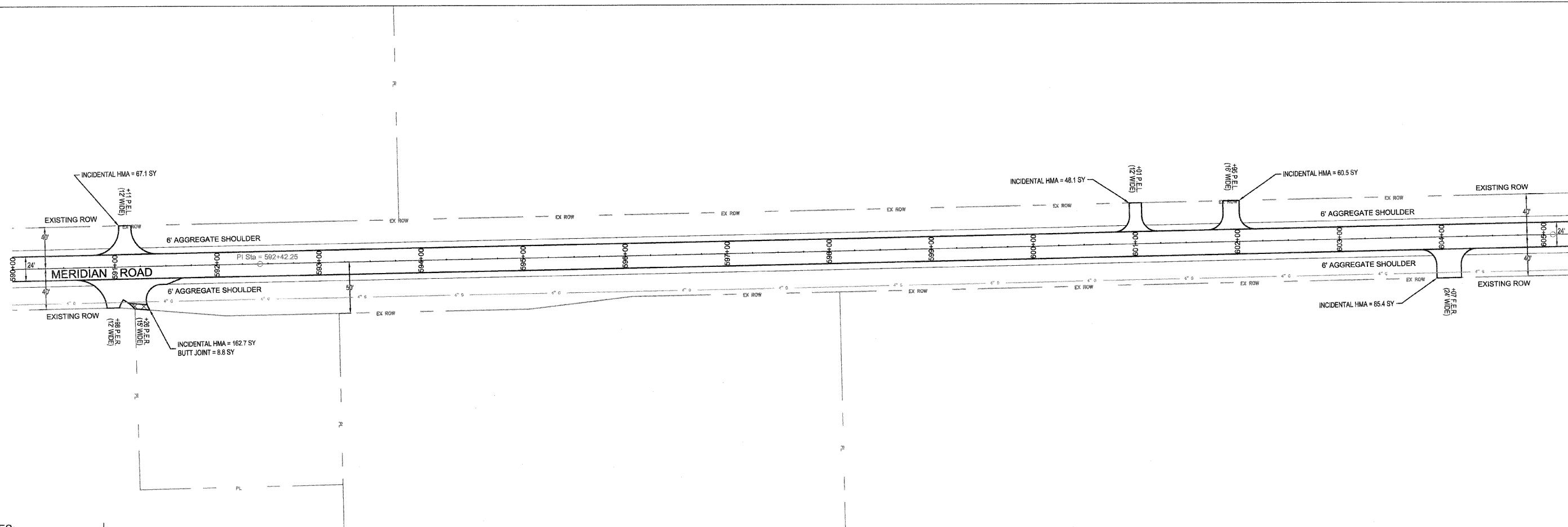




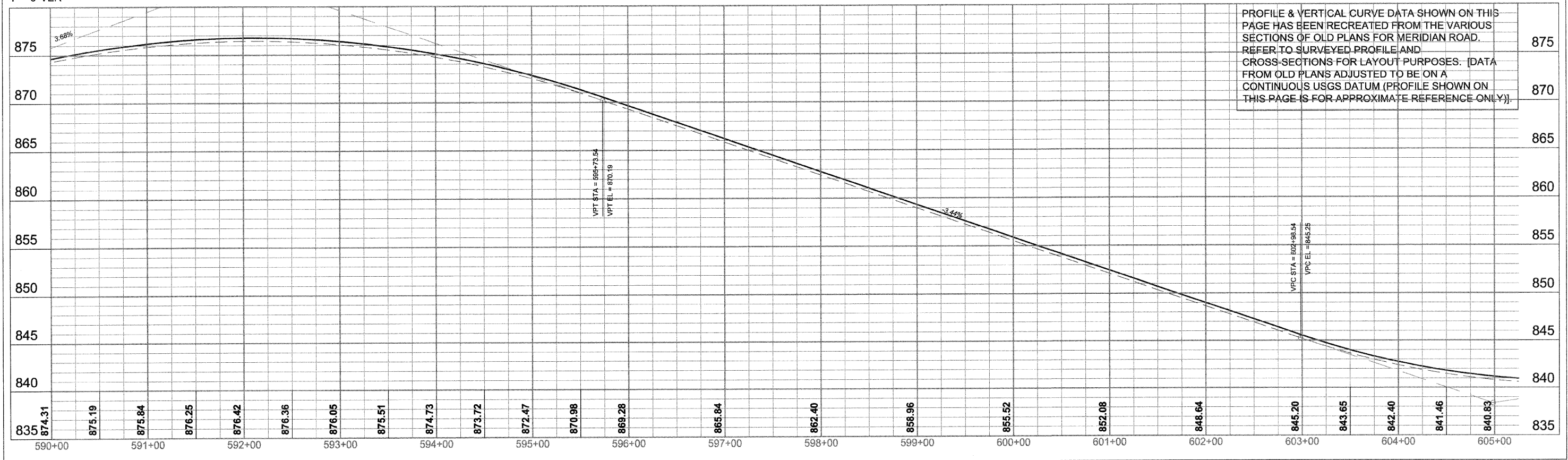
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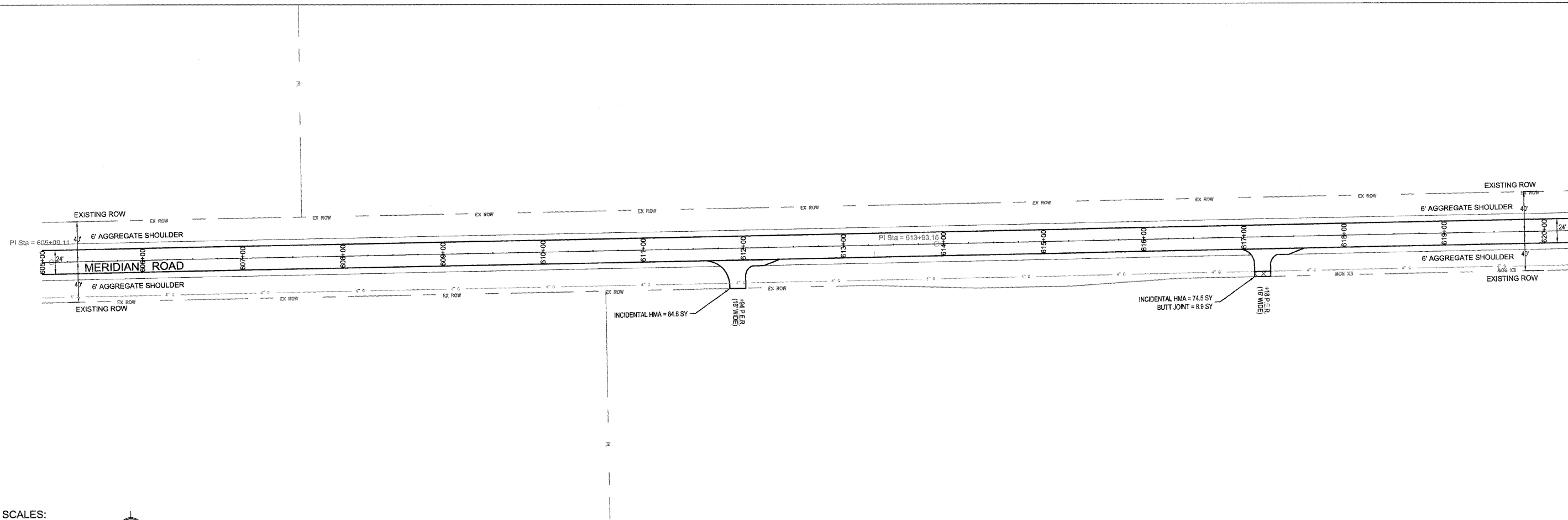
PROFILE & VERTICAL CURVE DATA SHOWN ON THIS PAGE HAS BEEN RECREATED FROM THE VARIOUS SECTIONS OF OLD PLANS FOR MERIDIAN ROAD. REFER TO SURVEYED PROFILE AND CROSS-SECTIONS FOR LAYOUT PURPOSES. [DATA FROM OLD PLANS ADJUSTED TO BE ON A CONTINUOUS USGS DATUM (PROFILE SHOWN ON THIS PAGE IS FOR APPROXIMATE REFERENCE ONLY)].



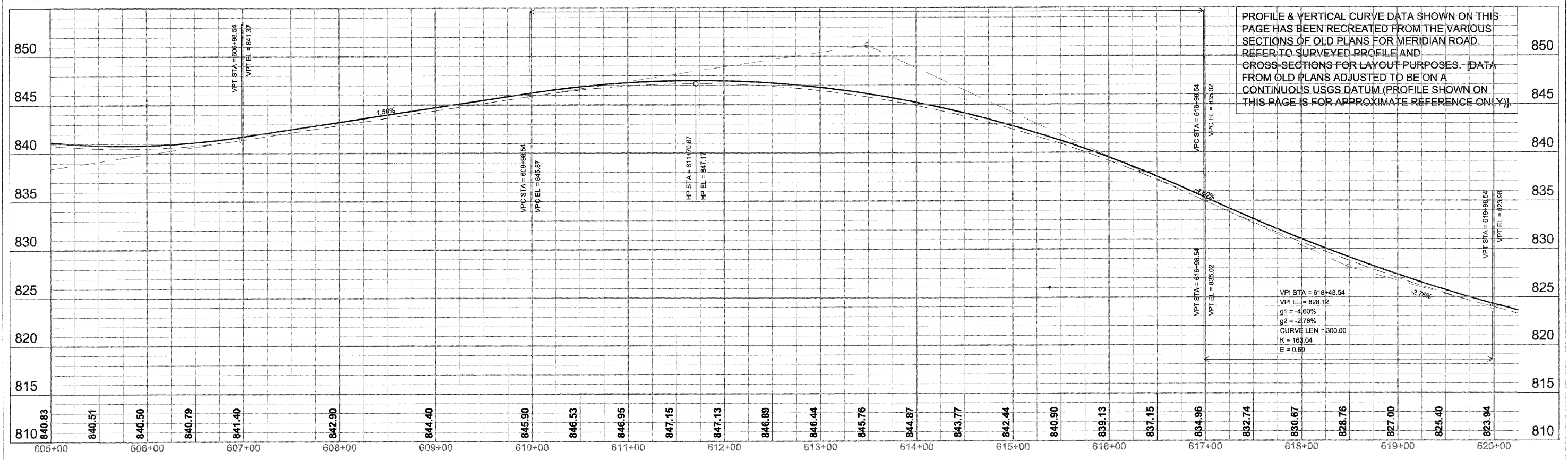
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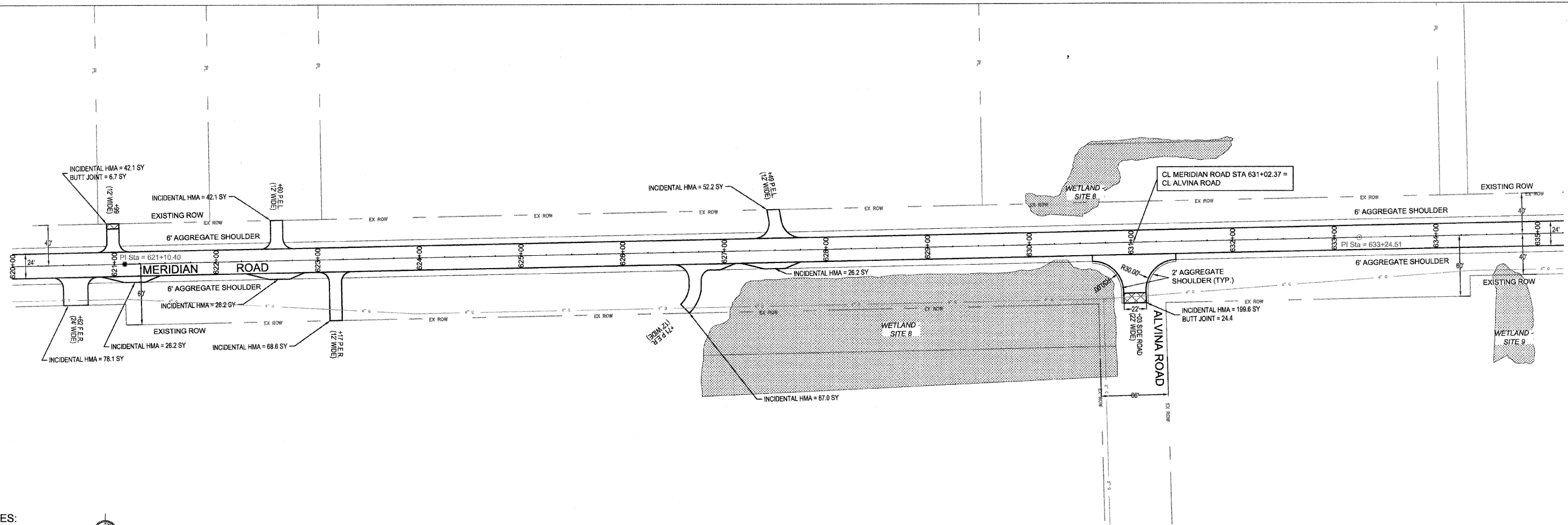
PROFILE & VERTICAL CURVE DATA SHOWN ON THIS PAGE HAS BEEN RECREATED FROM THE VARIOUS SECTIONS OF OLD PLANS FOR MERIDIAN ROAD. REFER TO SURVEYED PROFILE AND CROSS-SECTIONS FOR LAYOUT PURPOSES. [DATA FROM OLD PLANS ADJUSTED TO BE ON A CONTINUOUS USGS DATUM (PROFILE SHOWN ON THIS PAGE IS FOR APPROXIMATE REFERENCE ONLY)].



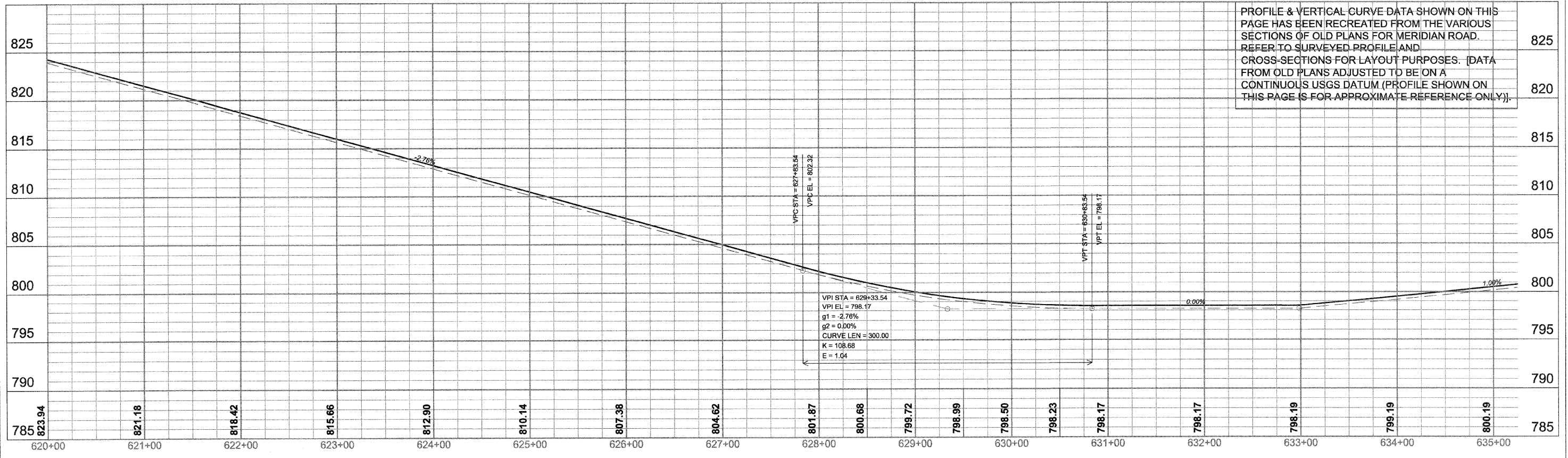
SCALES:
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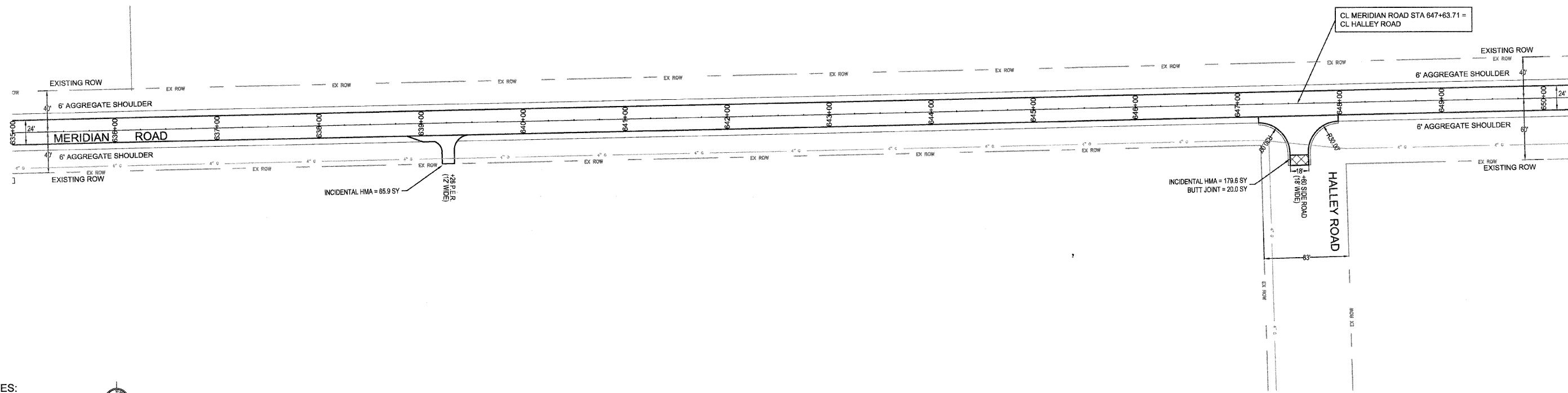
PROFILE & VERTICAL CURVE DATA SHOWN ON THIS PAGE HAS BEEN RECREATED FROM THE VARIOUS SECTIONS OF OLD PLANS FOR MERIDIAN ROAD. REFER TO SURVEYED PROFILE AND CROSS-SECTIONS FOR LAYOUT PURPOSES. [DATA FROM OLD PLANS ADJUSTED TO BE ON A CONTINUOUS USGS DATUM (PROFILE SHOWN ON THIS PAGE IS FOR APPROXIMATE REFERENCE ONLY)].



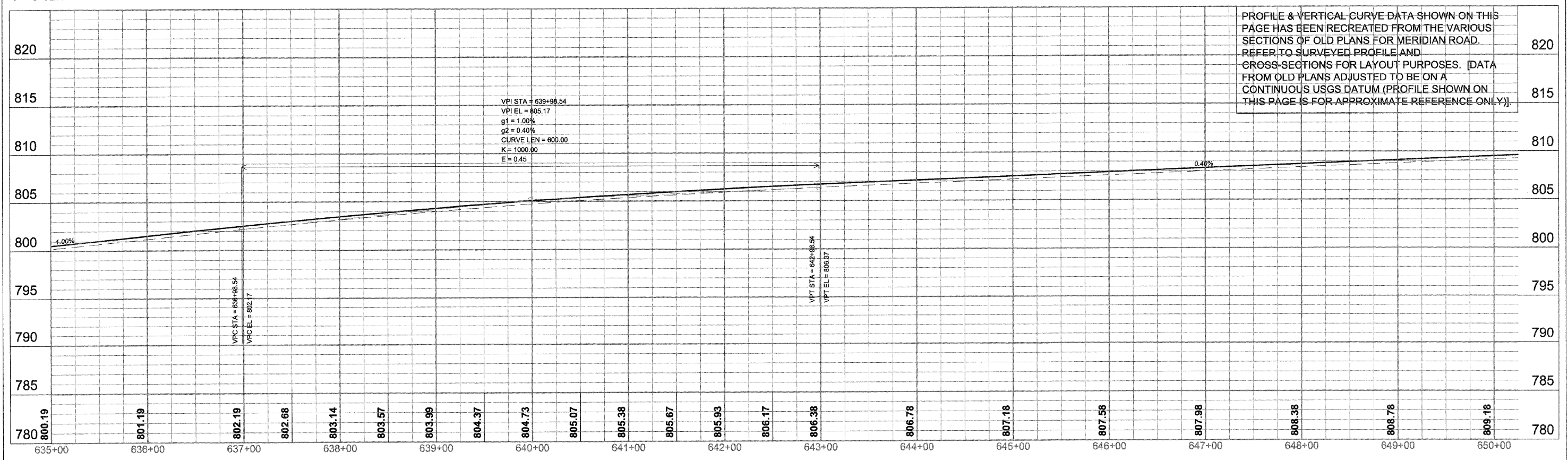
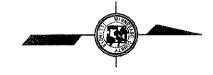
SCALES:
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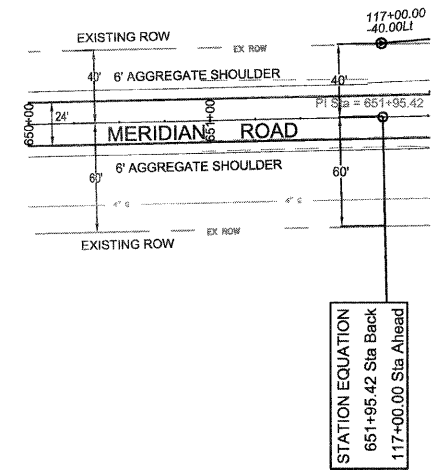


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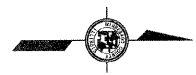


SCALES:
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 1" = 5' VER

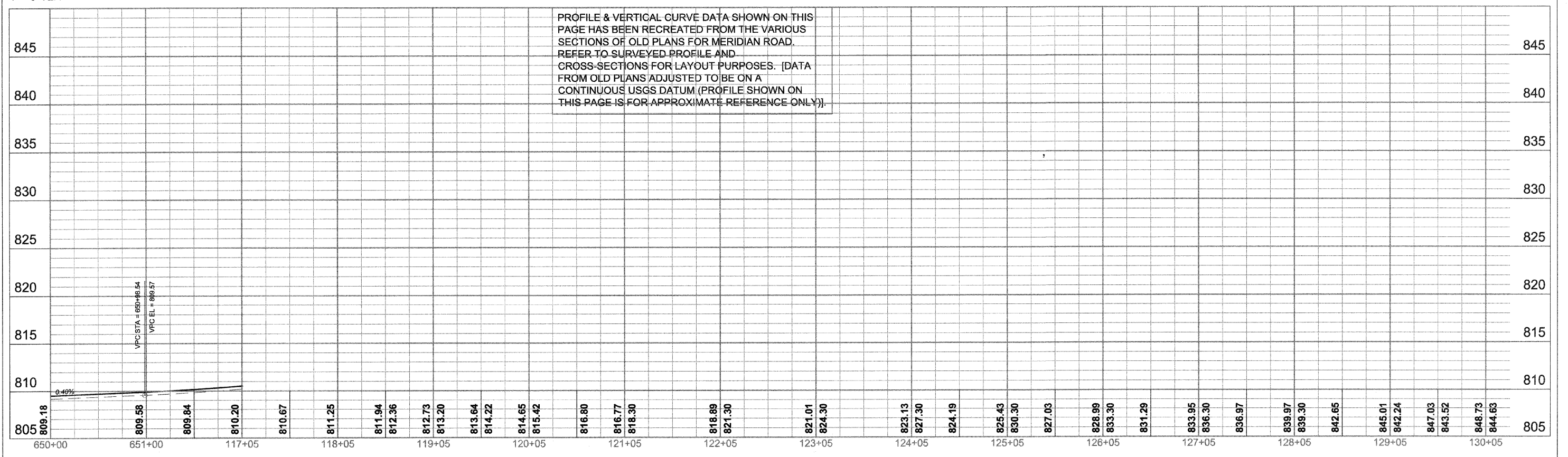


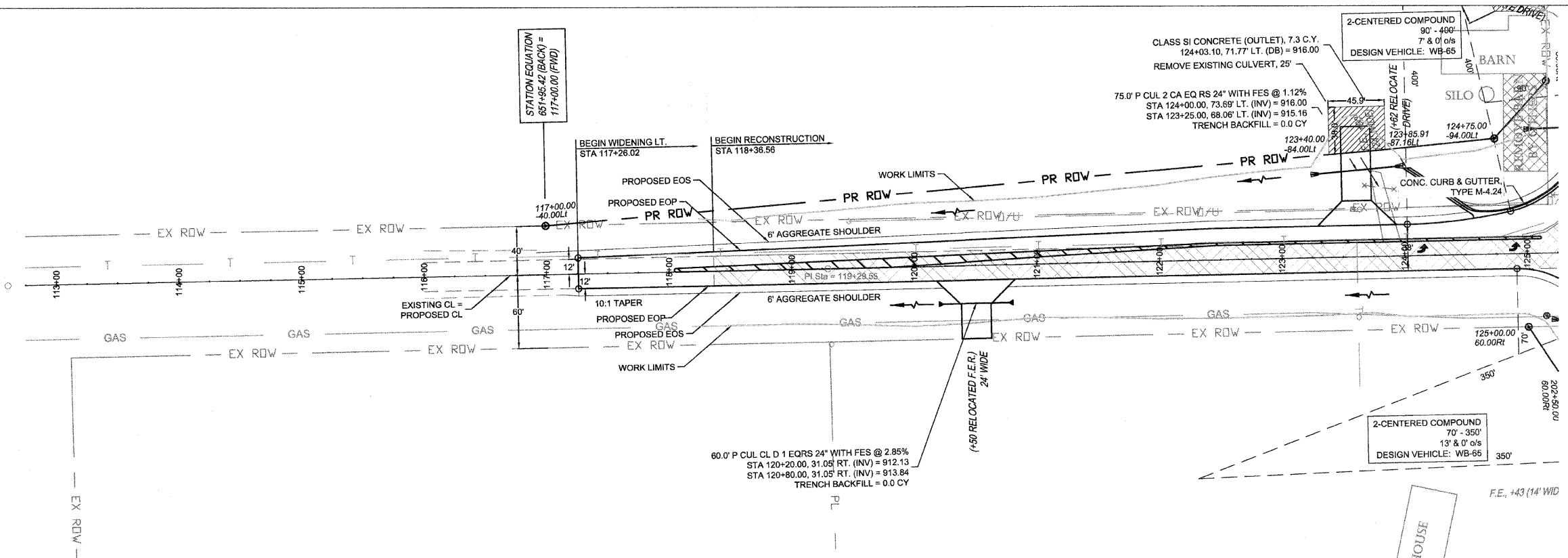


SCALES:
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1" = 5' VER

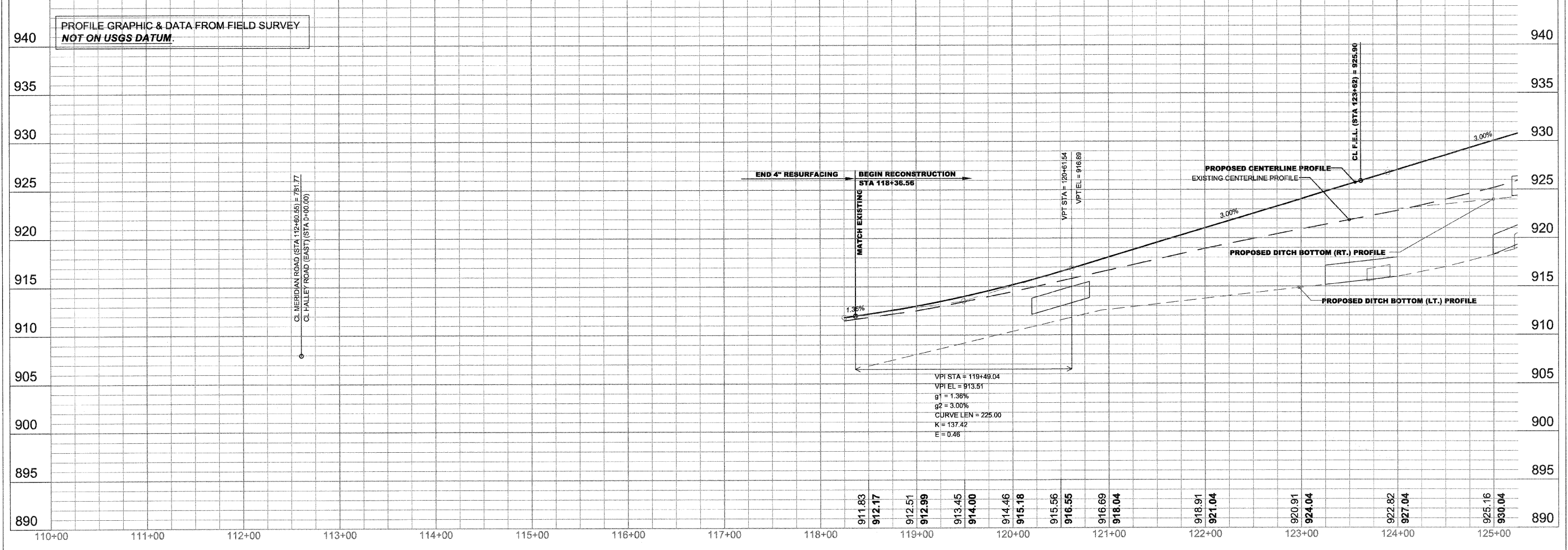


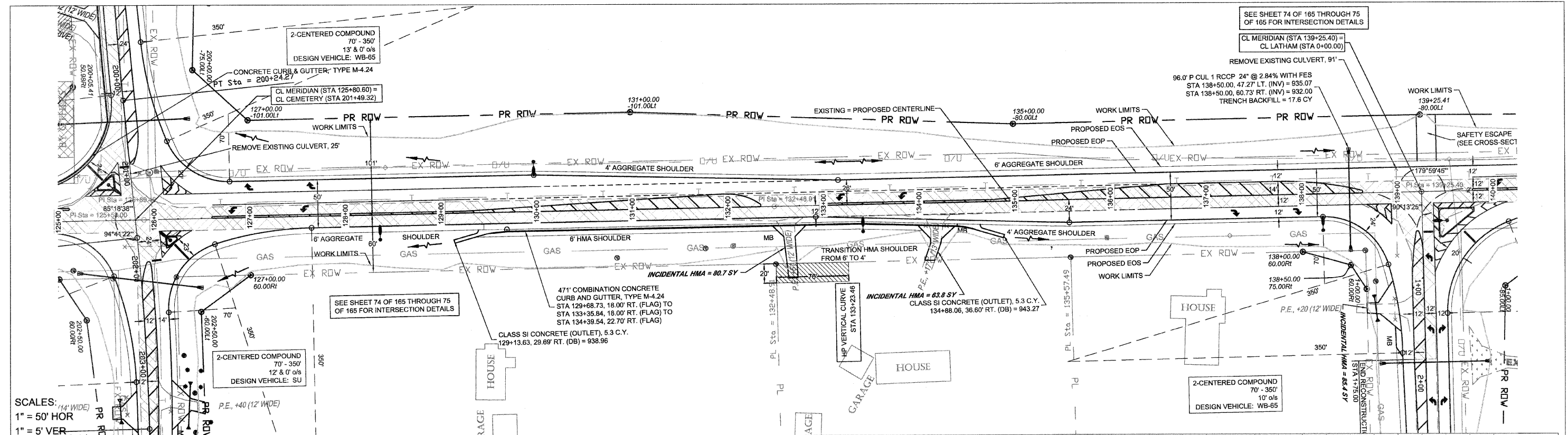
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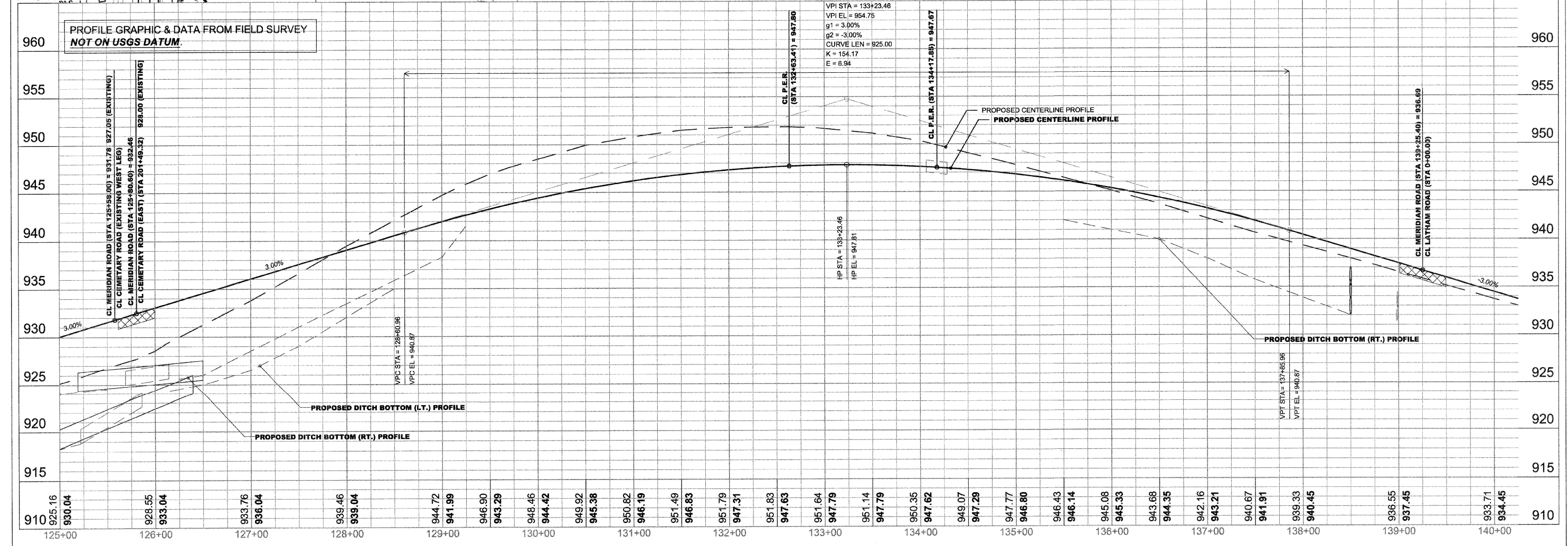


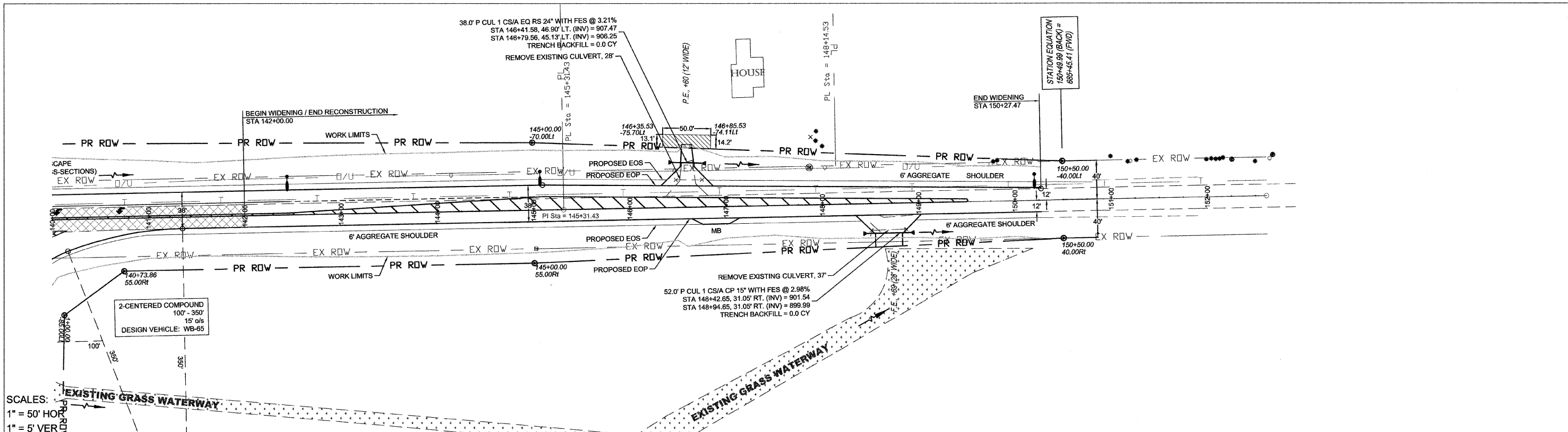
SCALES:
1" = 50' HOR
1" = 5' VER



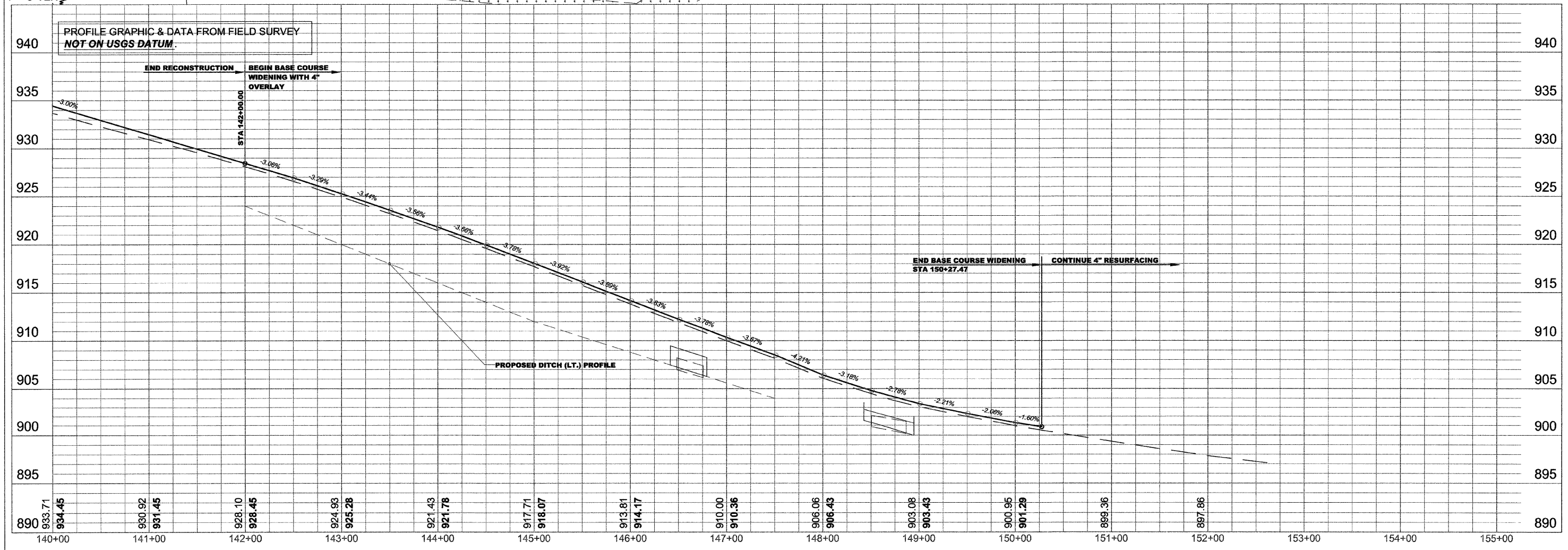


SCALES:
 1" = 50' HOR
 1" = 5' VER

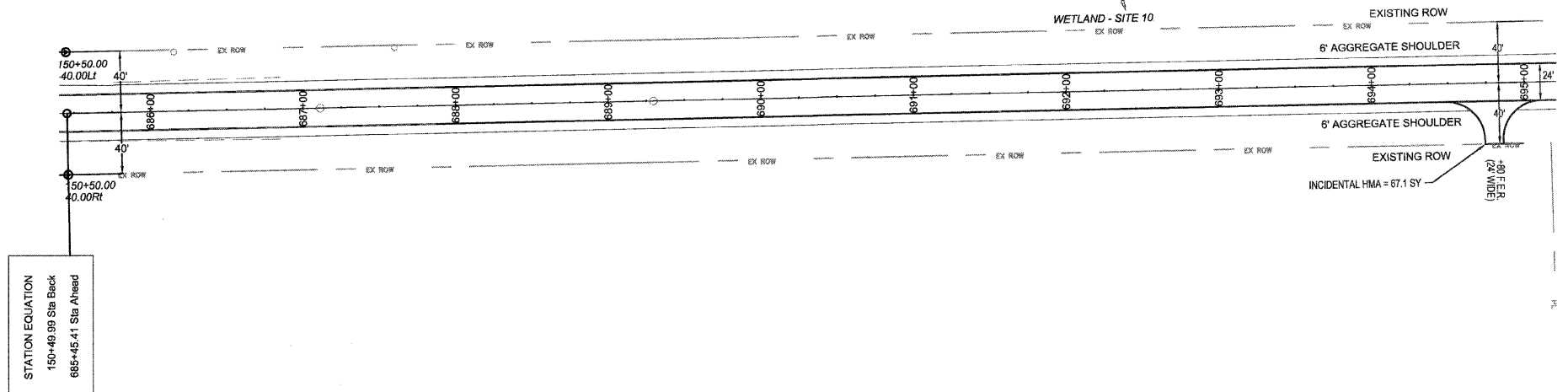




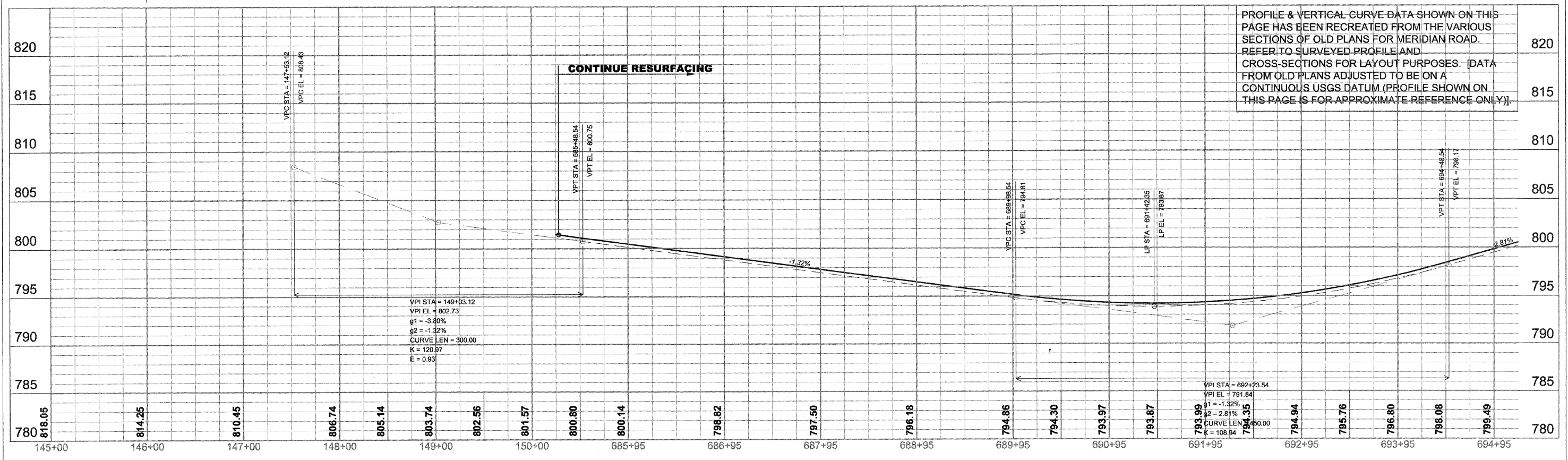
SCALES:
 1" = 50' HOR
 1" = 5' VER

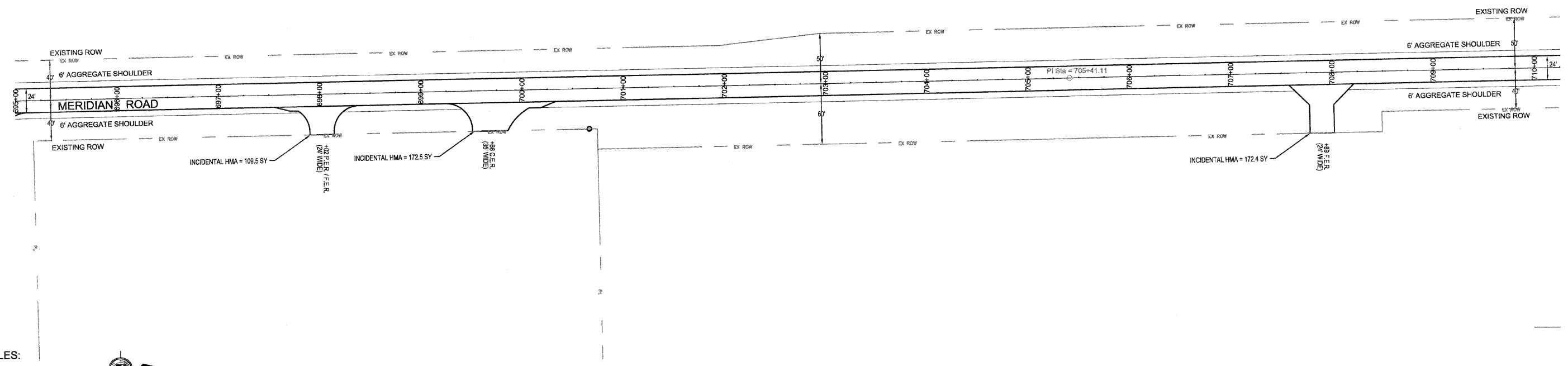


PI Sta = 150+49.99 PI Sta = 687+11.28 PI Sta = 689+29.67

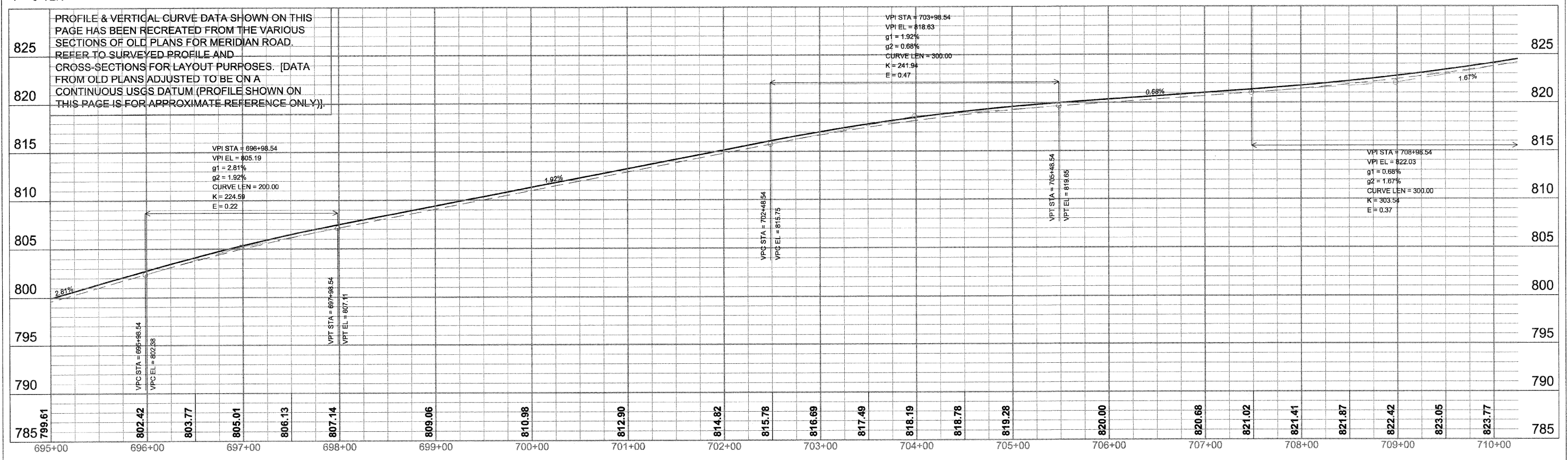
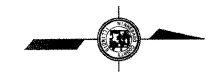


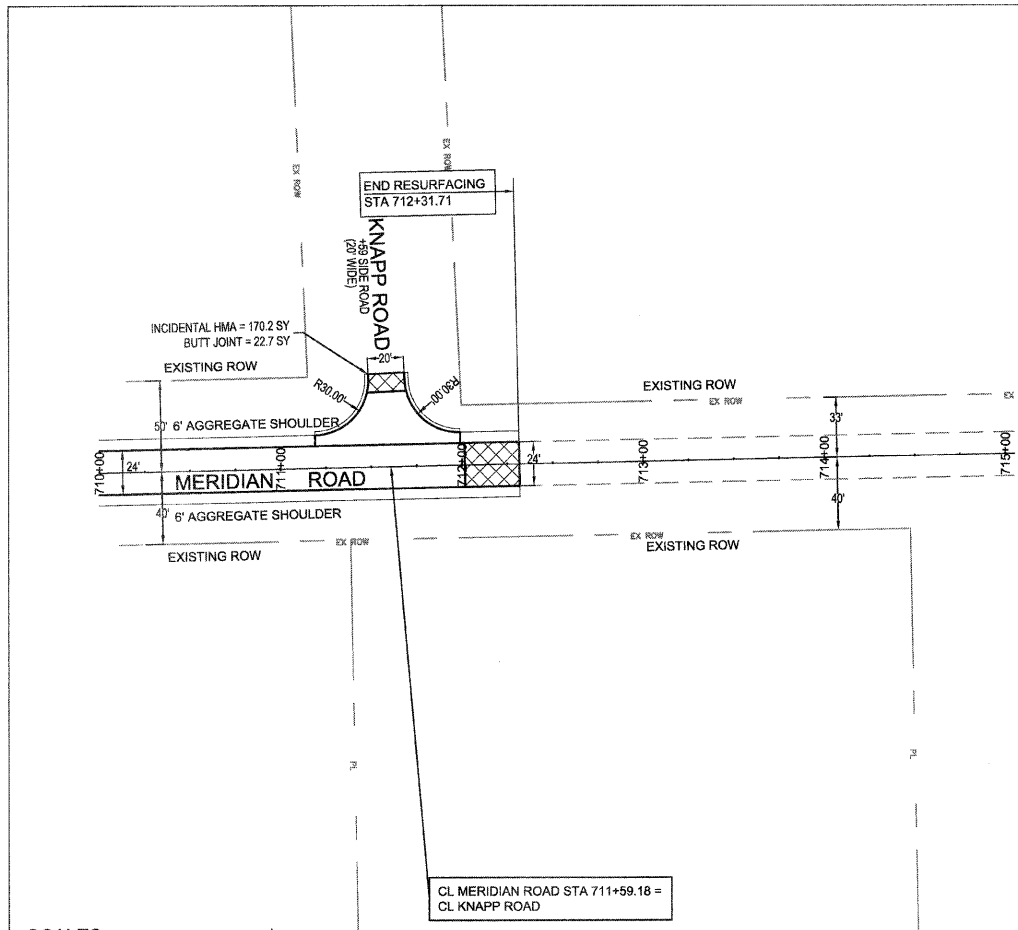
SCALES:
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 1" = 5' VER



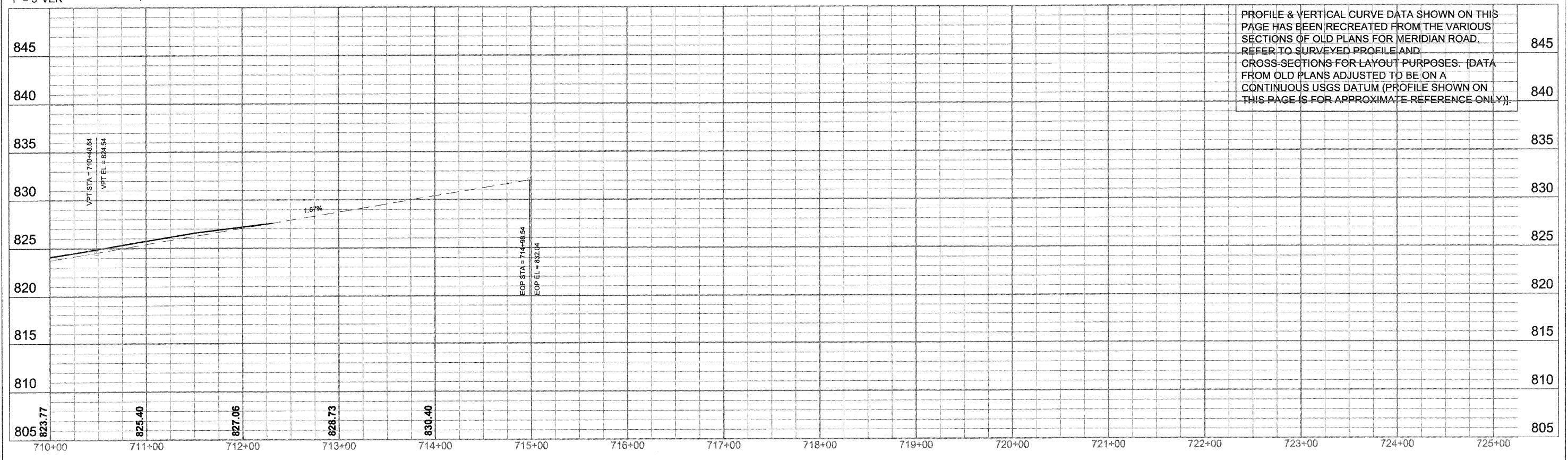


SCALES:
 1" = 50' HOR
 1" = 5' VER

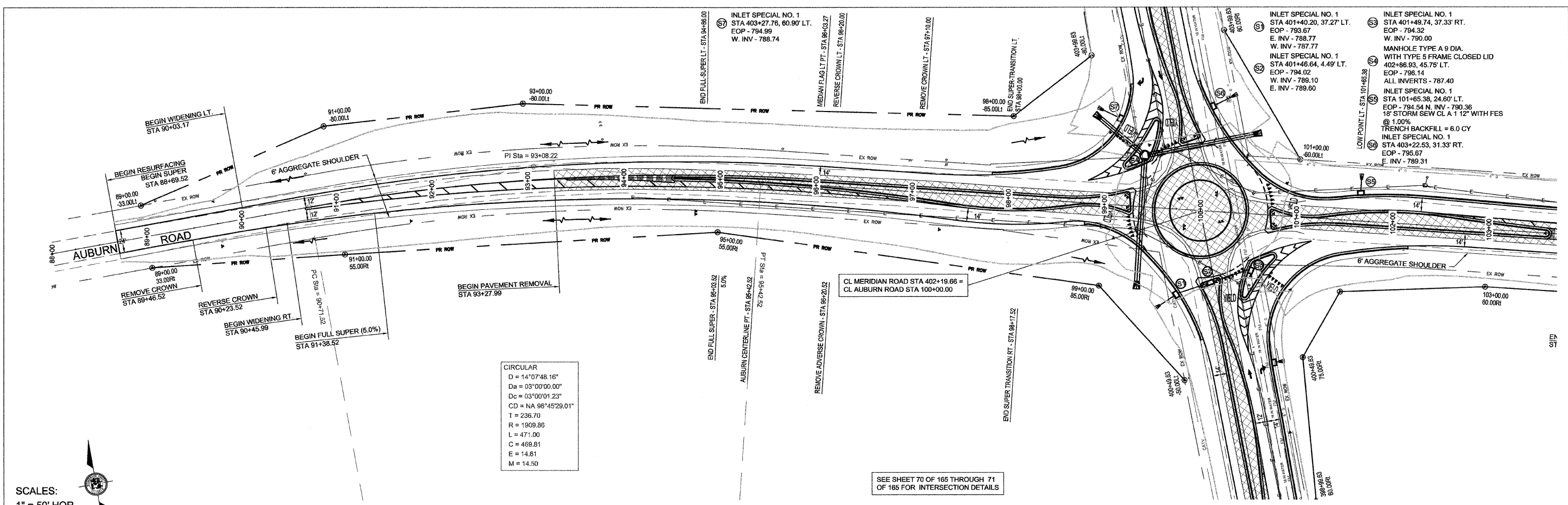




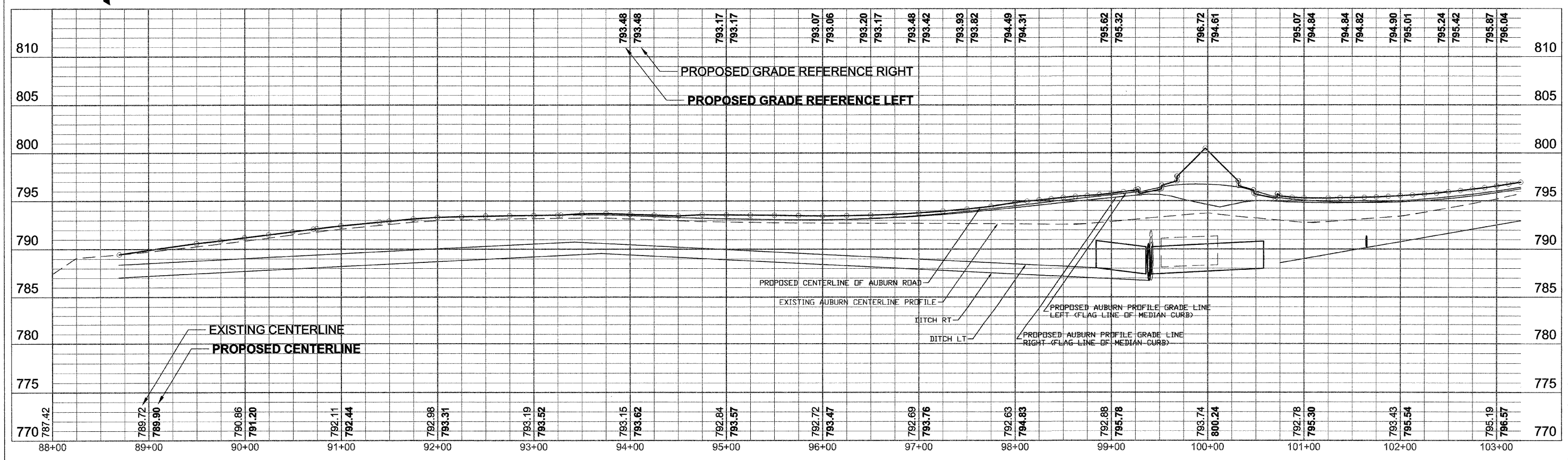
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 1" = 50' HOR
 1" = 5' VER

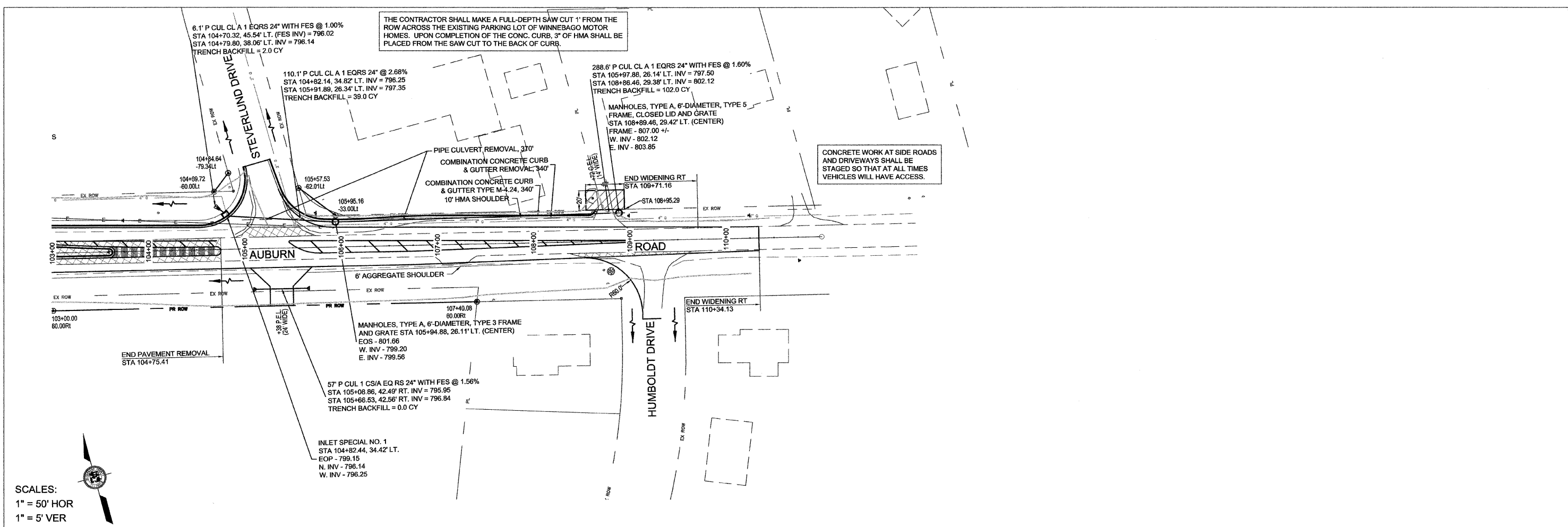


PROFILE & VERTICAL CURVE DATA SHOWN ON THIS PAGE HAS BEEN RECREATED FROM THE VARIOUS SECTIONS OF OLD PLANS FOR MERIDIAN ROAD. REFER TO SURVEYED PROFILE AND CROSS-SECTIONS FOR LAYOUT PURPOSES. [DATA FROM OLD PLANS ADJUSTED TO BE ON A CONTINUOUS USGS DATUM (PROFILE SHOWN ON THIS PAGE IS FOR APPROXIMATE REFERENCE ONLY)].

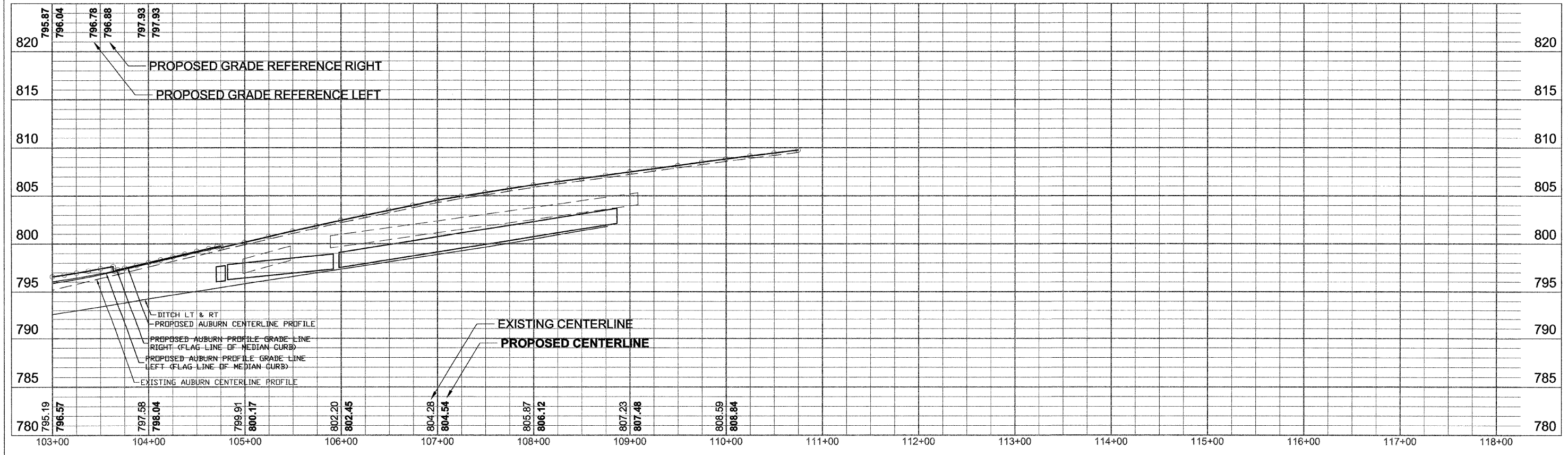


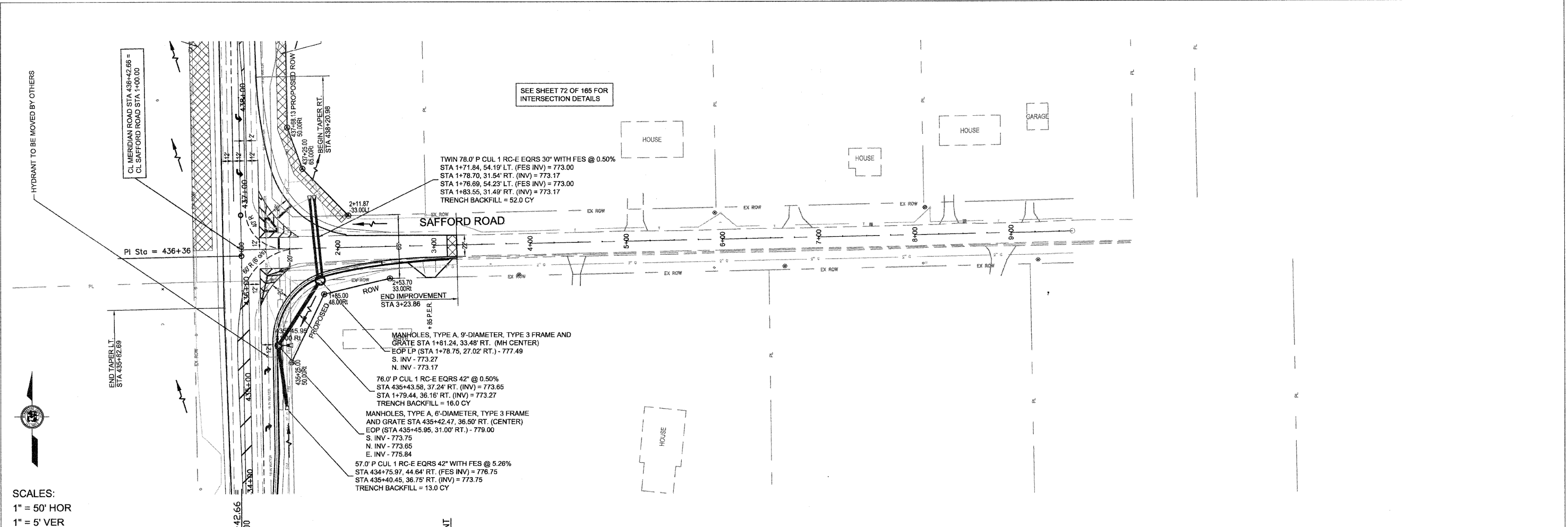
SCALES:
1" = 50' HOR
1" = 5' VER



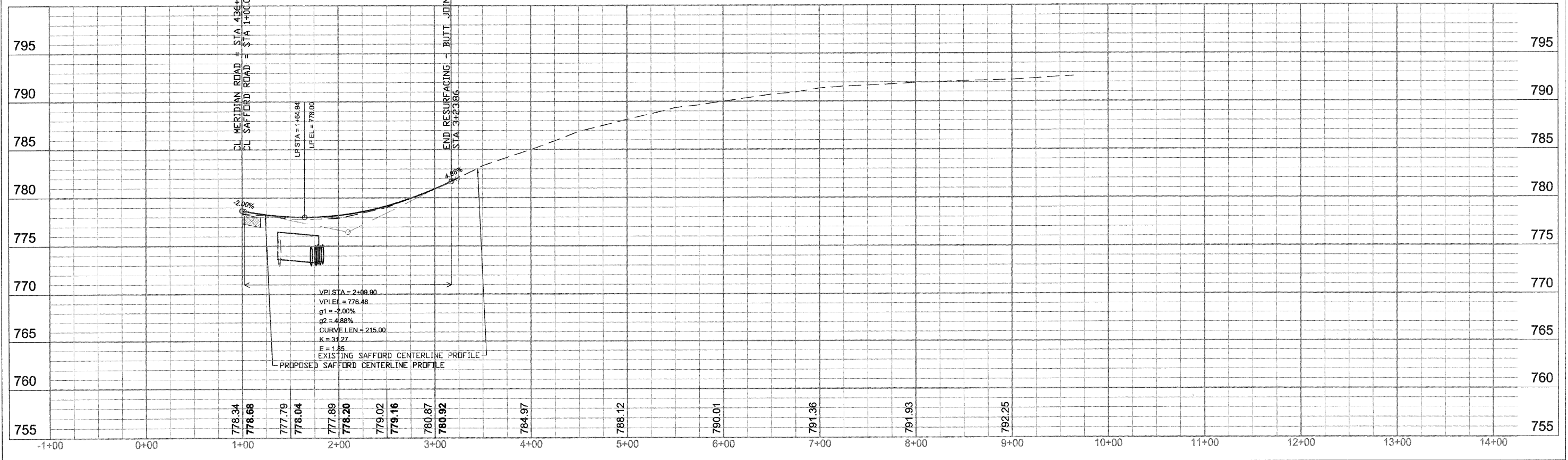


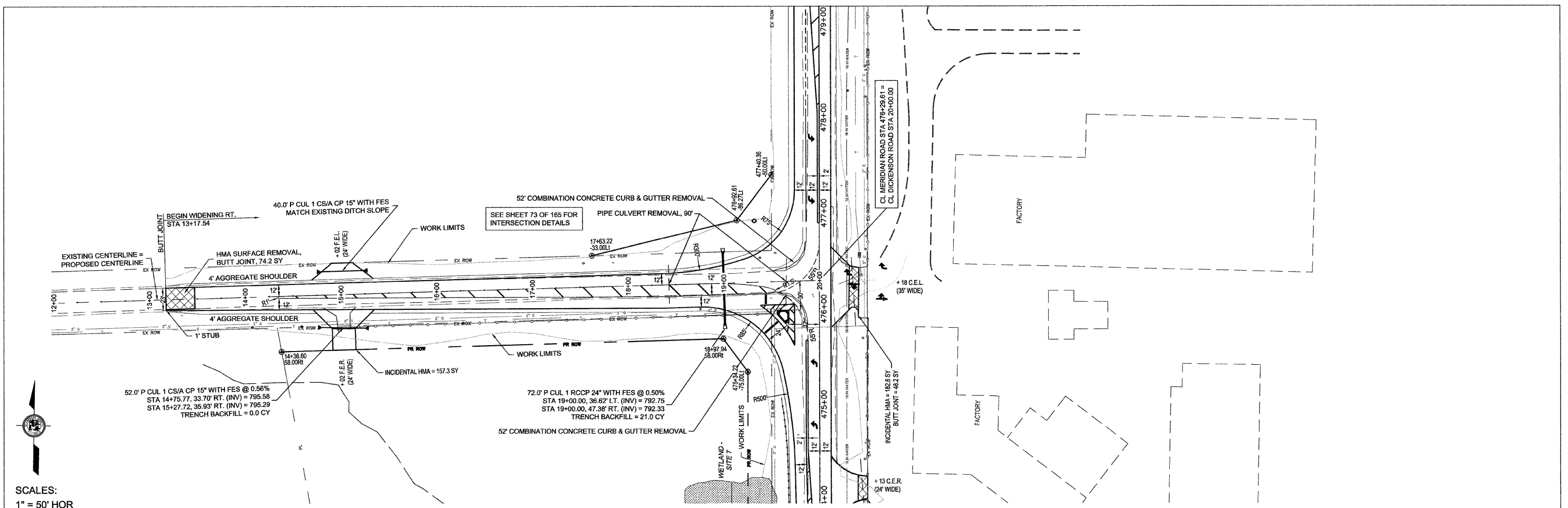
SCALES:
1" = 50' HOR
1" = 5' VER



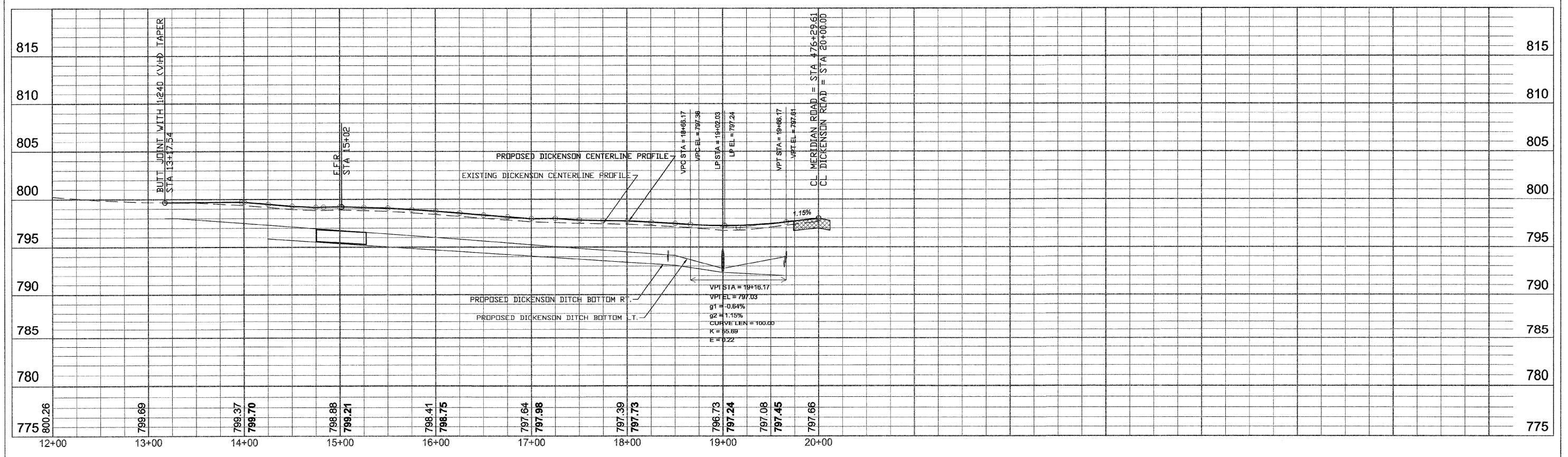


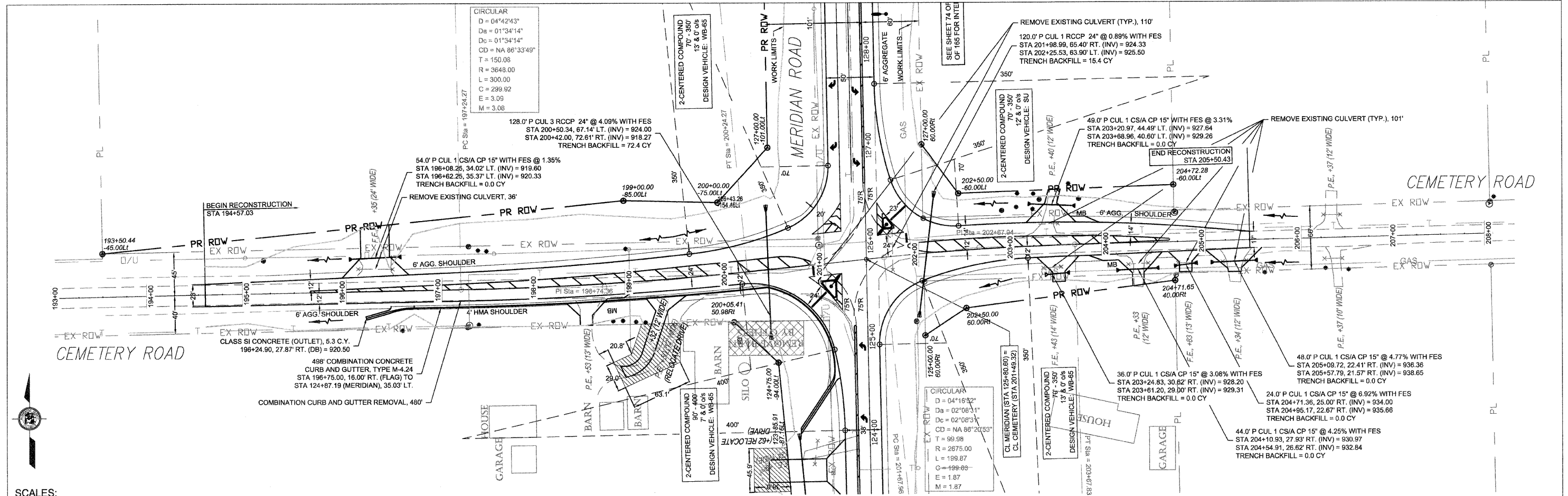
SCALES:
 1" = 50' HOR
 1" = 5' VER



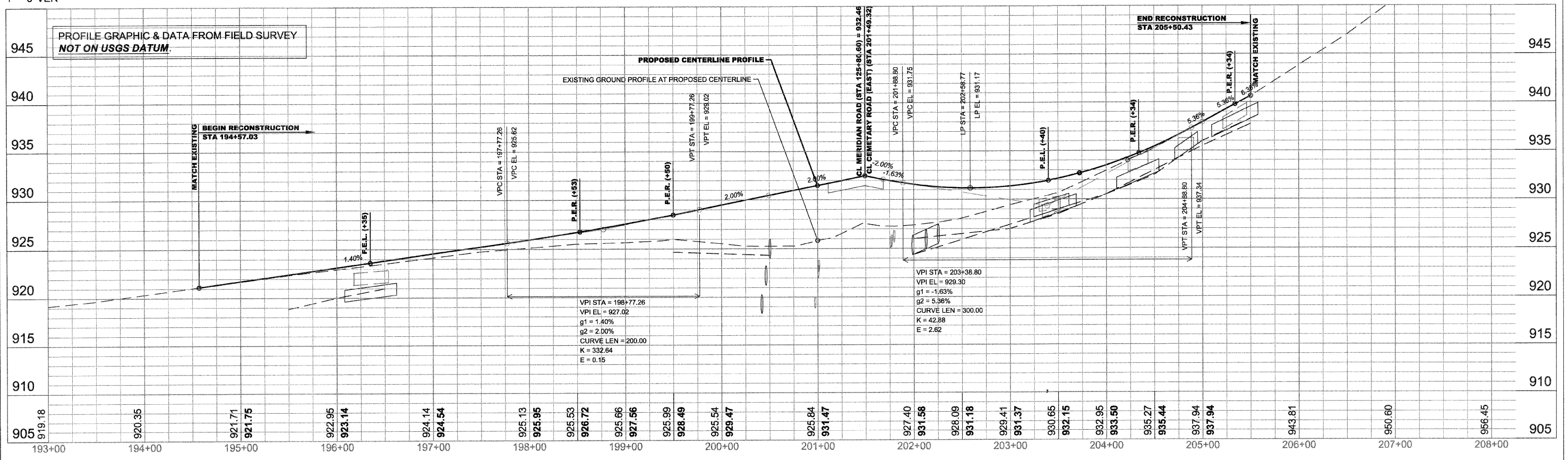


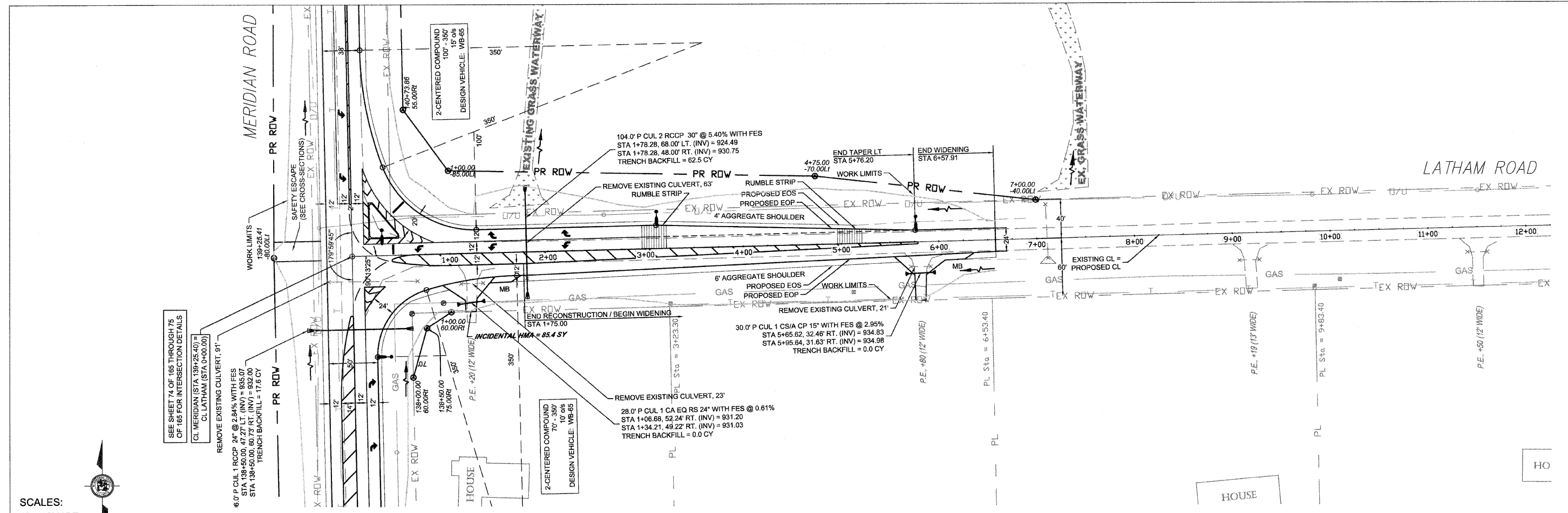
SCALES:
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1" = 5' VER



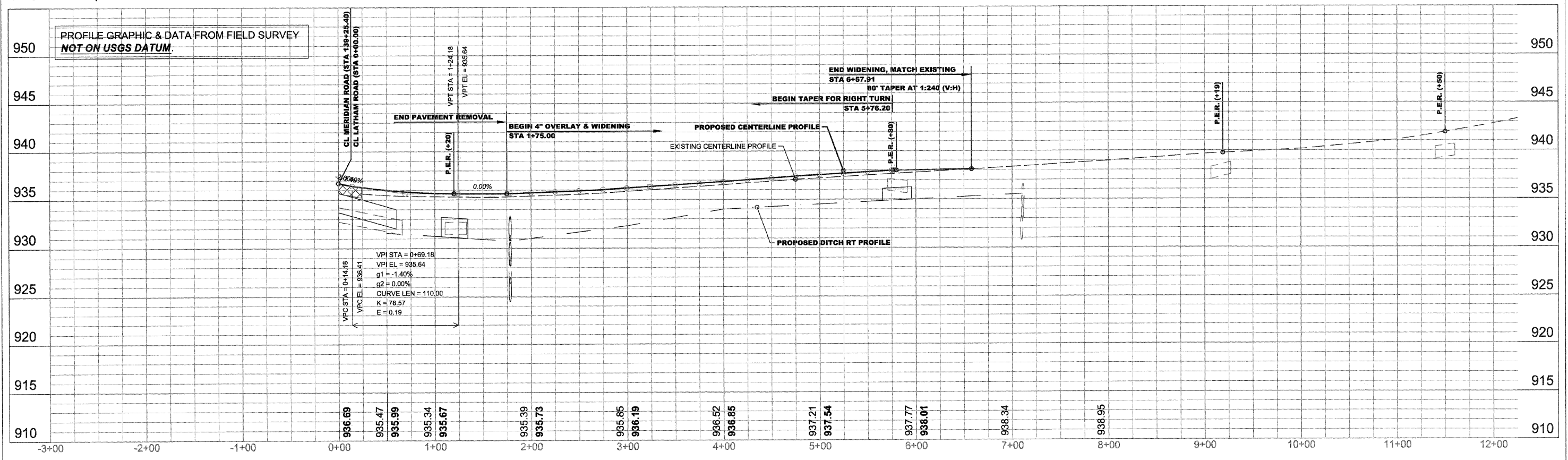



SCALES: 1" = 50' HOR 1" = 5' VER





SCALES:
1" = 50' HOR
1" = 5' VER



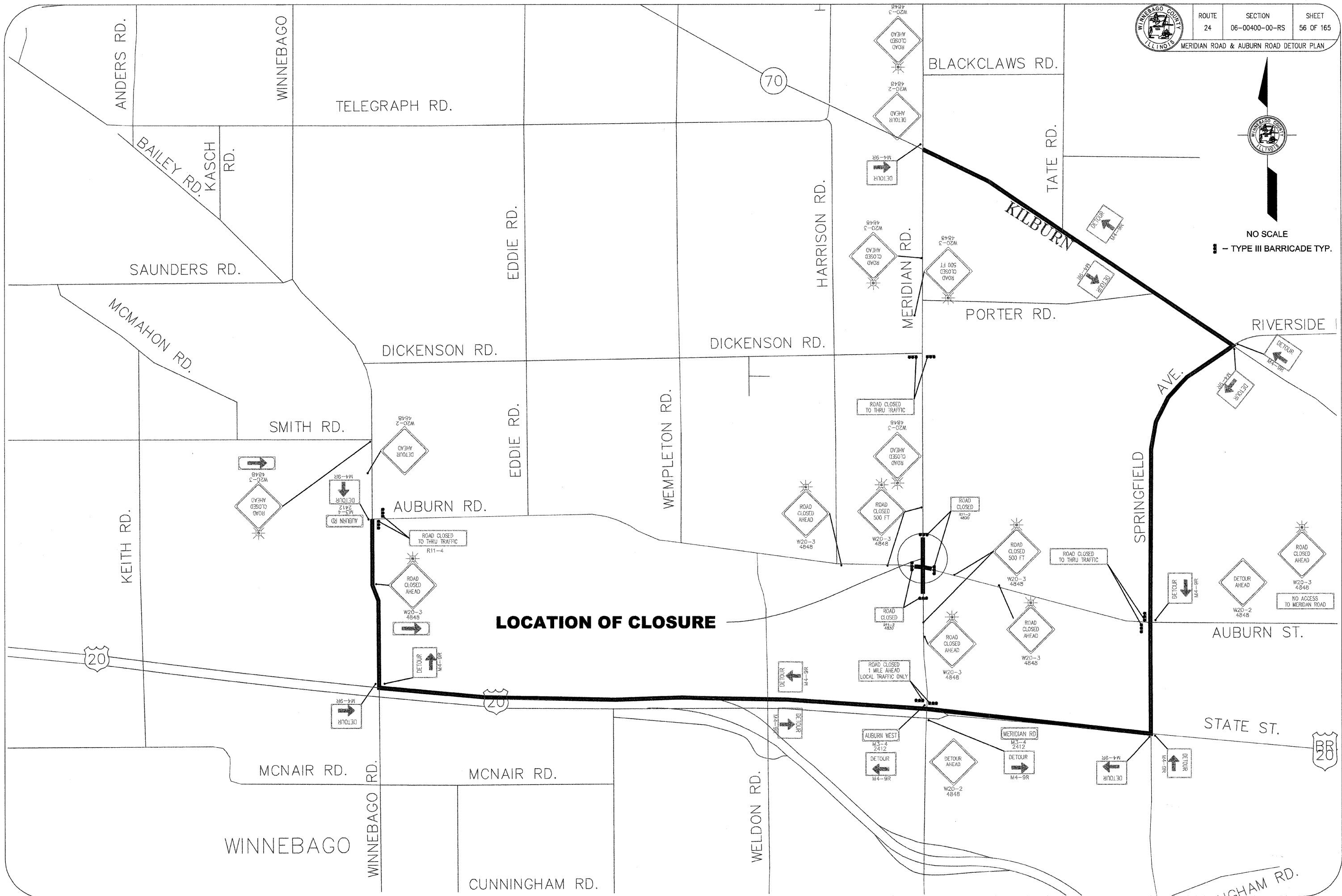
	ROUTE	SECTION	SHEET
	24	06-00400-00-RS	56 OF 165

MERIDIAN ROAD & AUBURN ROAD DETOUR PLAN



NO SCALE

— TYPE III BARRICADE TYP.

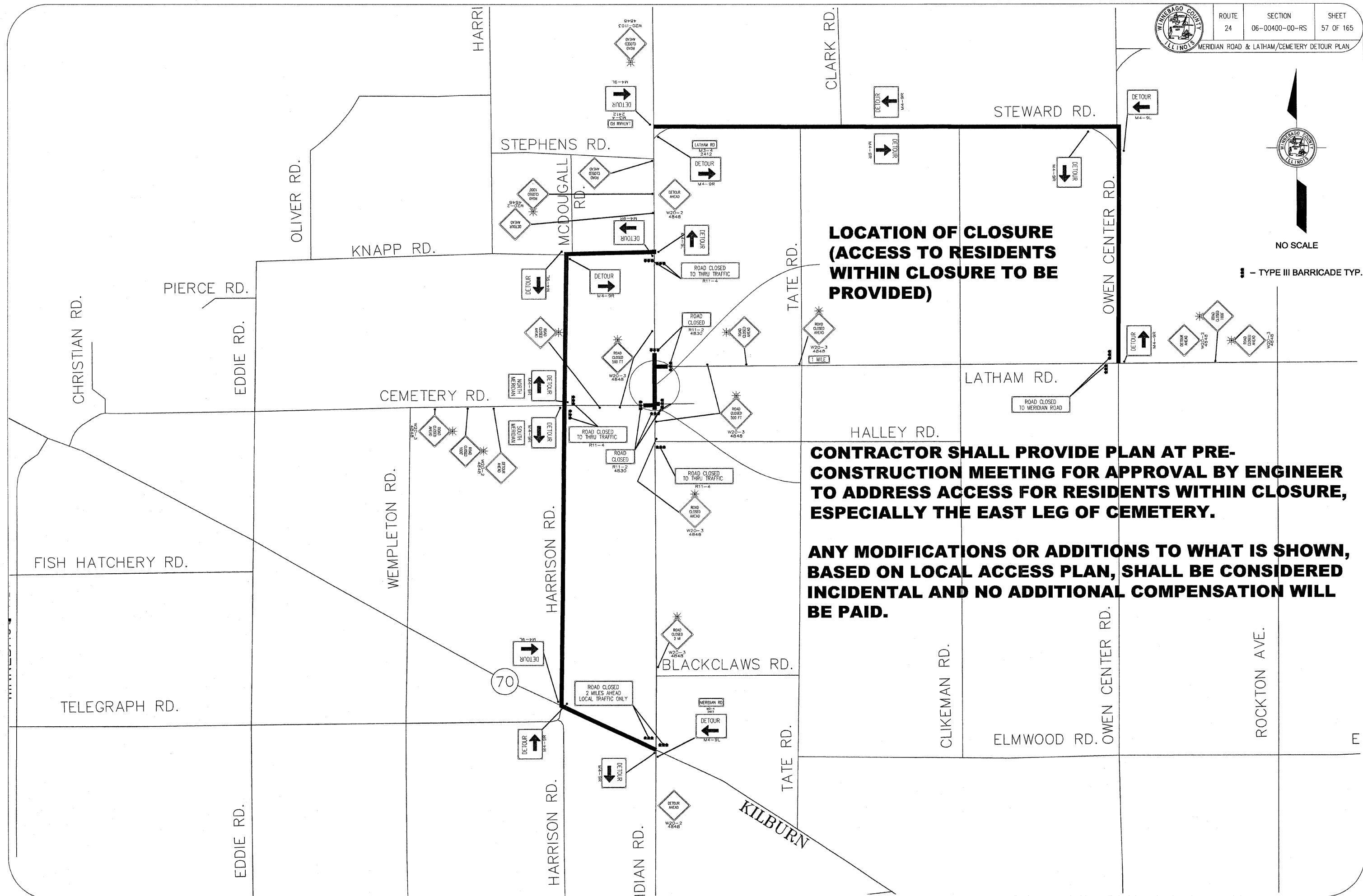


LOCATION OF CLOSURE



NO SCALE

— TYPE III BARRICADE TYP.



**LOCATION OF CLOSURE
(ACCESS TO RESIDENTS
WITHIN CLOSURE TO BE
PROVIDED)**

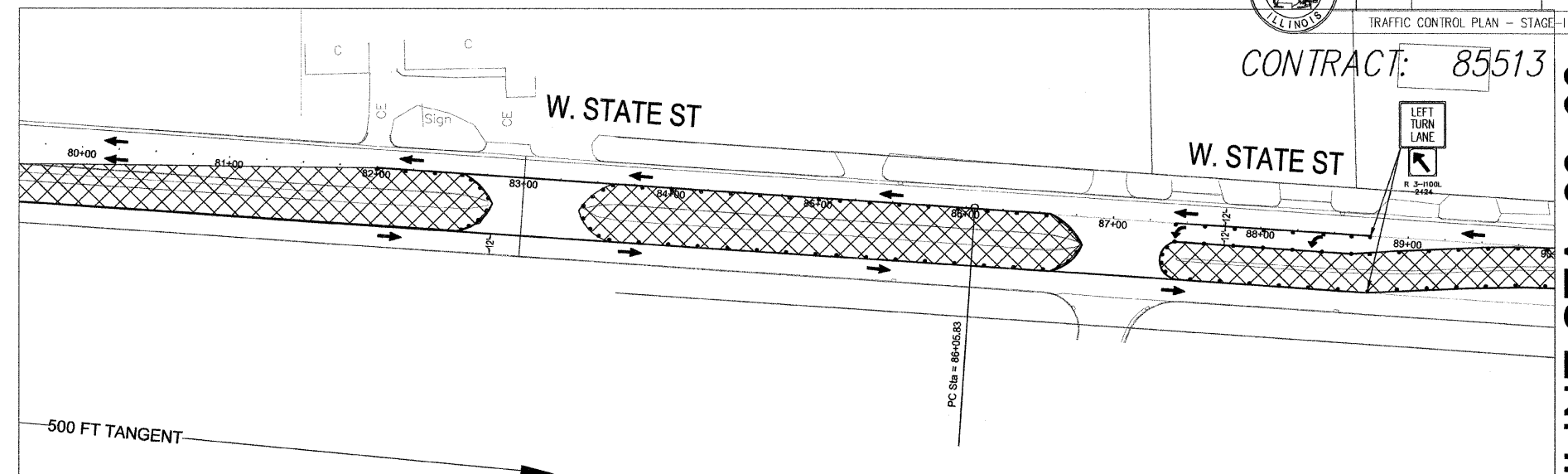
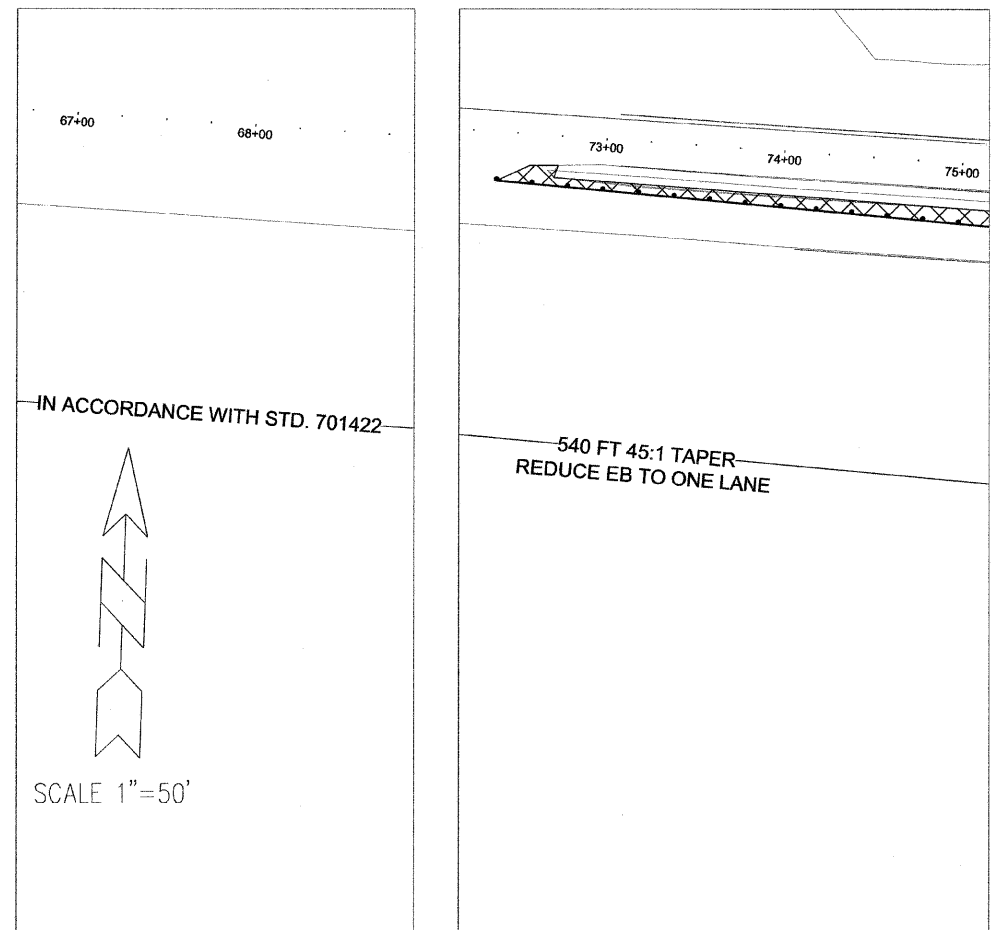
**CONTRACTOR SHALL PROVIDE PLAN AT PRE-
CONSTRUCTION MEETING FOR APPROVAL BY ENGINEER
TO ADDRESS ACCESS FOR RESIDENTS WITHIN CLOSURE,
ESPECIALLY THE EAST LEG OF CEMETERY.**

**ANY MODIFICATIONS OR ADDITIONS TO WHAT IS SHOWN,
BASED ON LOCAL ACCESS PLAN, SHALL BE CONSIDERED
INCIDENTAL AND NO ADDITIONAL COMPENSATION WILL
BE PAID.**



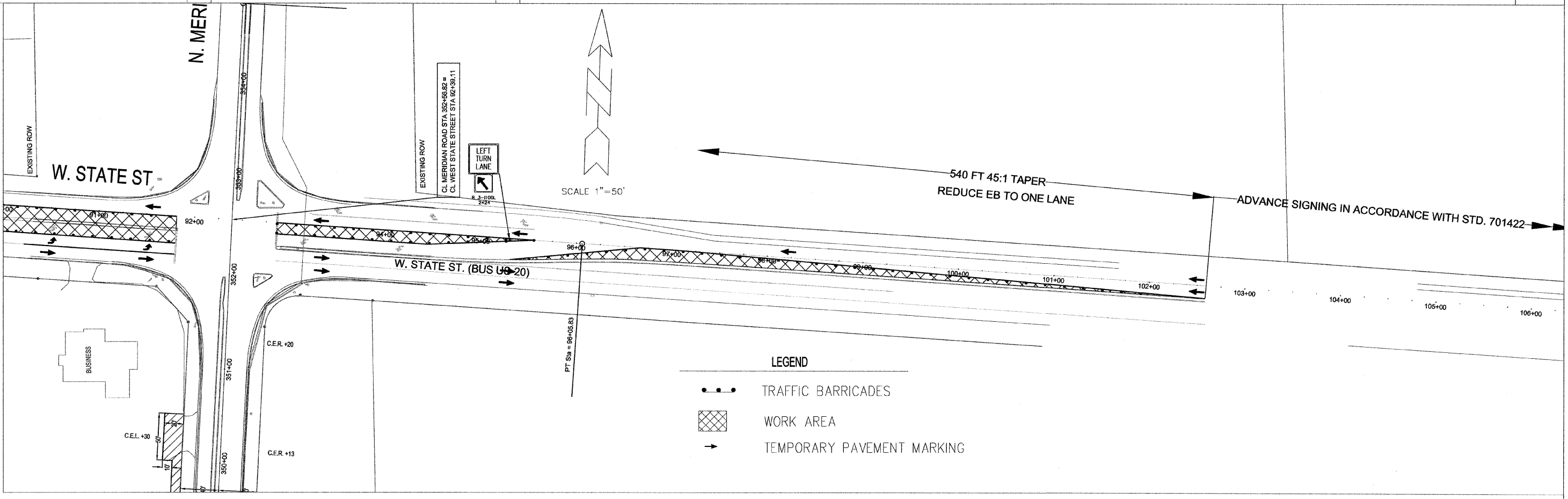
CONTRACT: 85513

MATCHLINE STA. 90+00



- STAGE I
1. RELOCATE OR COVER EXISTING REGULATORY SIGNS AS APPLICABLE
 2. PLACE SIGNING, FLAGGERS AND TRAFFIC CONTROL DEVICES PER STD. 701422, 701701, 701901, 701922 AND IDOT DIST II DETAIL # 94.2 (ALL TEMPORARY TRAFFIC CONTROL PLACEMENT BASED ON 45 MPH DESIGN)
 3. REMOVE & RECONSTRUCT MEDIAN IN AREAS SHOWN, CLOSE SIDE ROADS AS DIRECTED BY ENGINEER
 4. POST AN ADVISORY SPEED LIMIT OF 35 MPH THROUGH CONSTRUCTION AREA

MATCHLINE STA. 90+00



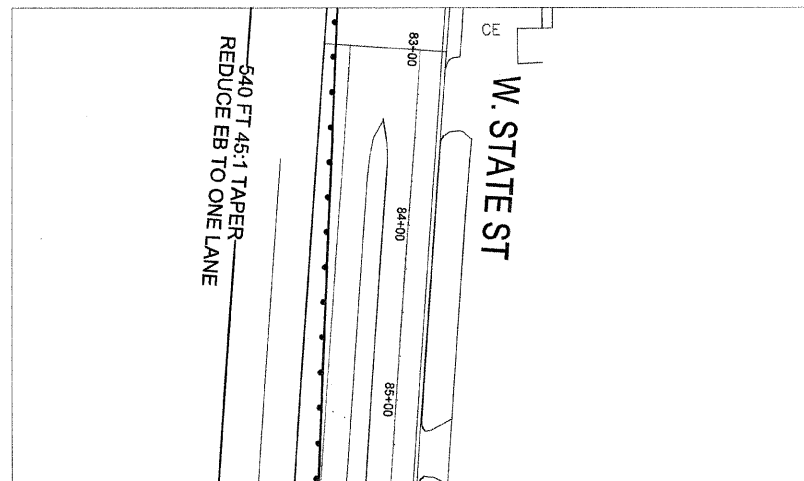
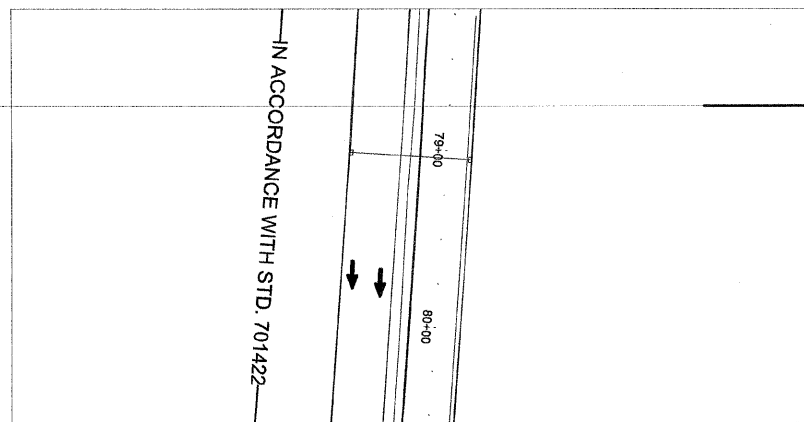
- LEGEND
- ● ● TRAFFIC BARRICADES
 - ▨ WORK AREA
 - TEMPORARY PAVEMENT MARKING



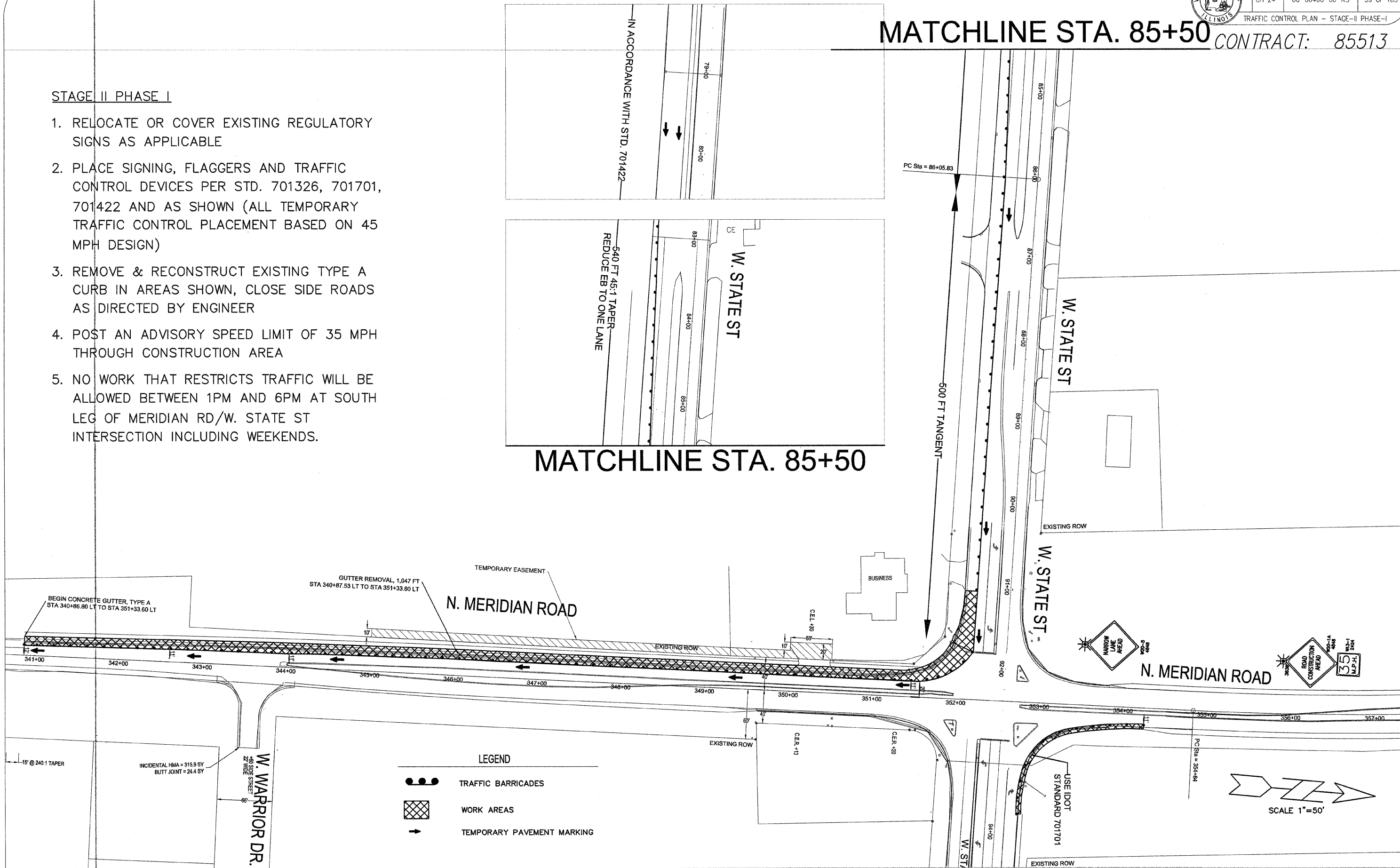
MATCHLINE STA. 85+50 CONTRACT: 85513

STAGE II PHASE I

1. RELOCATE OR COVER EXISTING REGULATORY SIGNS AS APPLICABLE
2. PLACE SIGNING, FLAGGERS AND TRAFFIC CONTROL DEVICES PER STD. 701326, 701701, 701422 AND AS SHOWN (ALL TEMPORARY TRAFFIC CONTROL PLACEMENT BASED ON 45 MPH DESIGN)
3. REMOVE & RECONSTRUCT EXISTING TYPE A CURB IN AREAS SHOWN, CLOSE SIDE ROADS AS DIRECTED BY ENGINEER
4. POST AN ADVISORY SPEED LIMIT OF 35 MPH THROUGH CONSTRUCTION AREA
5. NO WORK THAT RESTRICTS TRAFFIC WILL BE ALLOWED BETWEEN 1PM AND 6PM AT SOUTH LEG OF MERIDIAN RD/W. STATE ST INTERSECTION INCLUDING WEEKENDS.



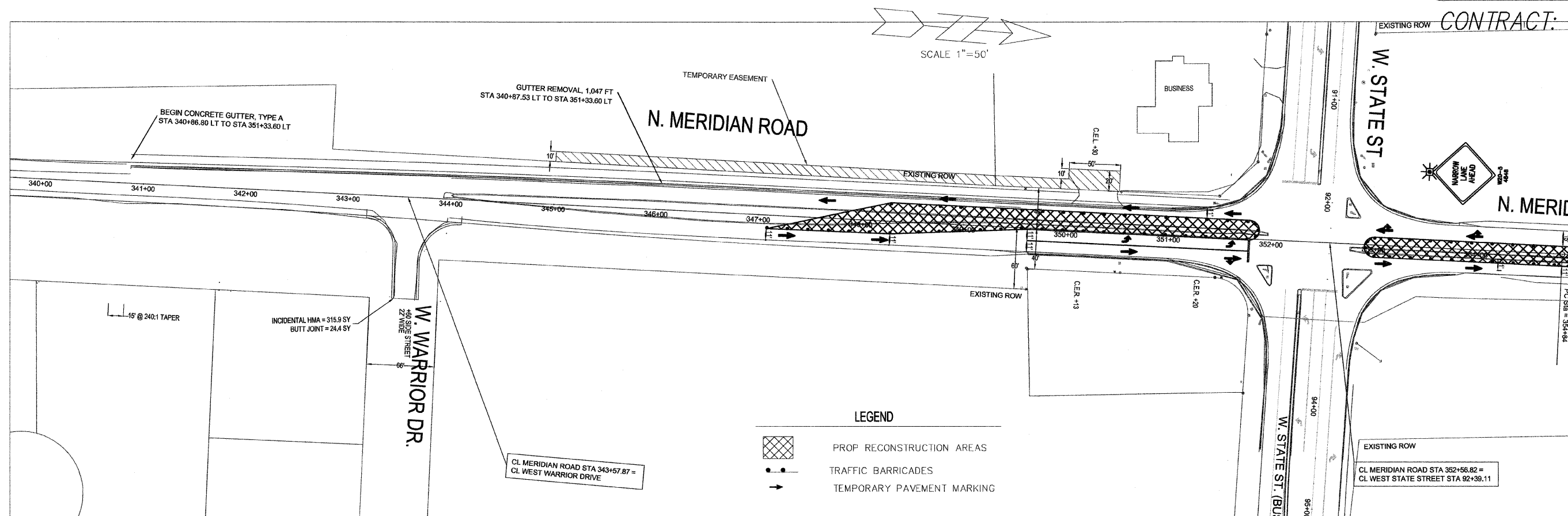
MATCHLINE STA. 85+50



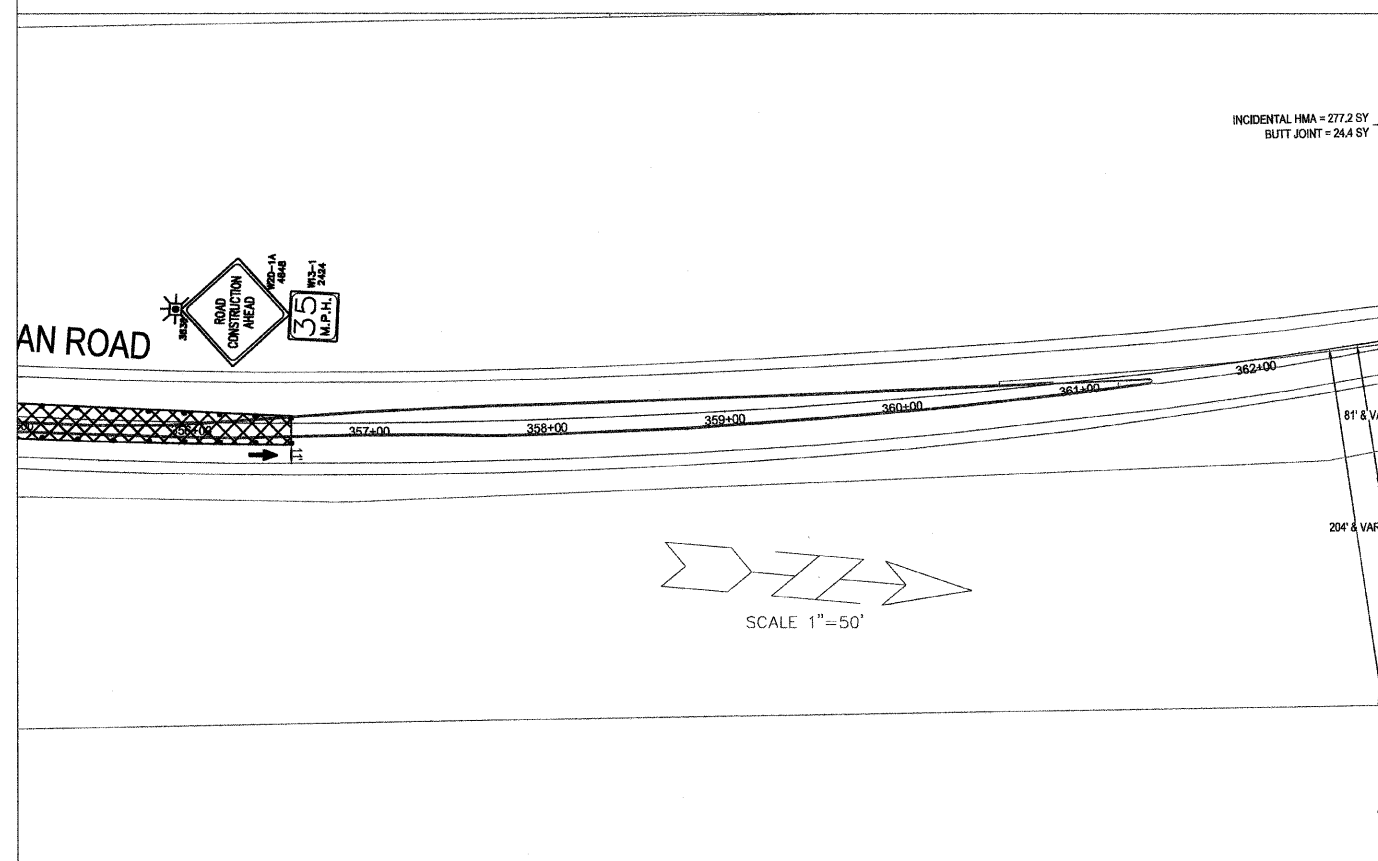


CONTRACT: 85513

MATCHLINE STA. 355+00



MATCHLINE STA. 355+00

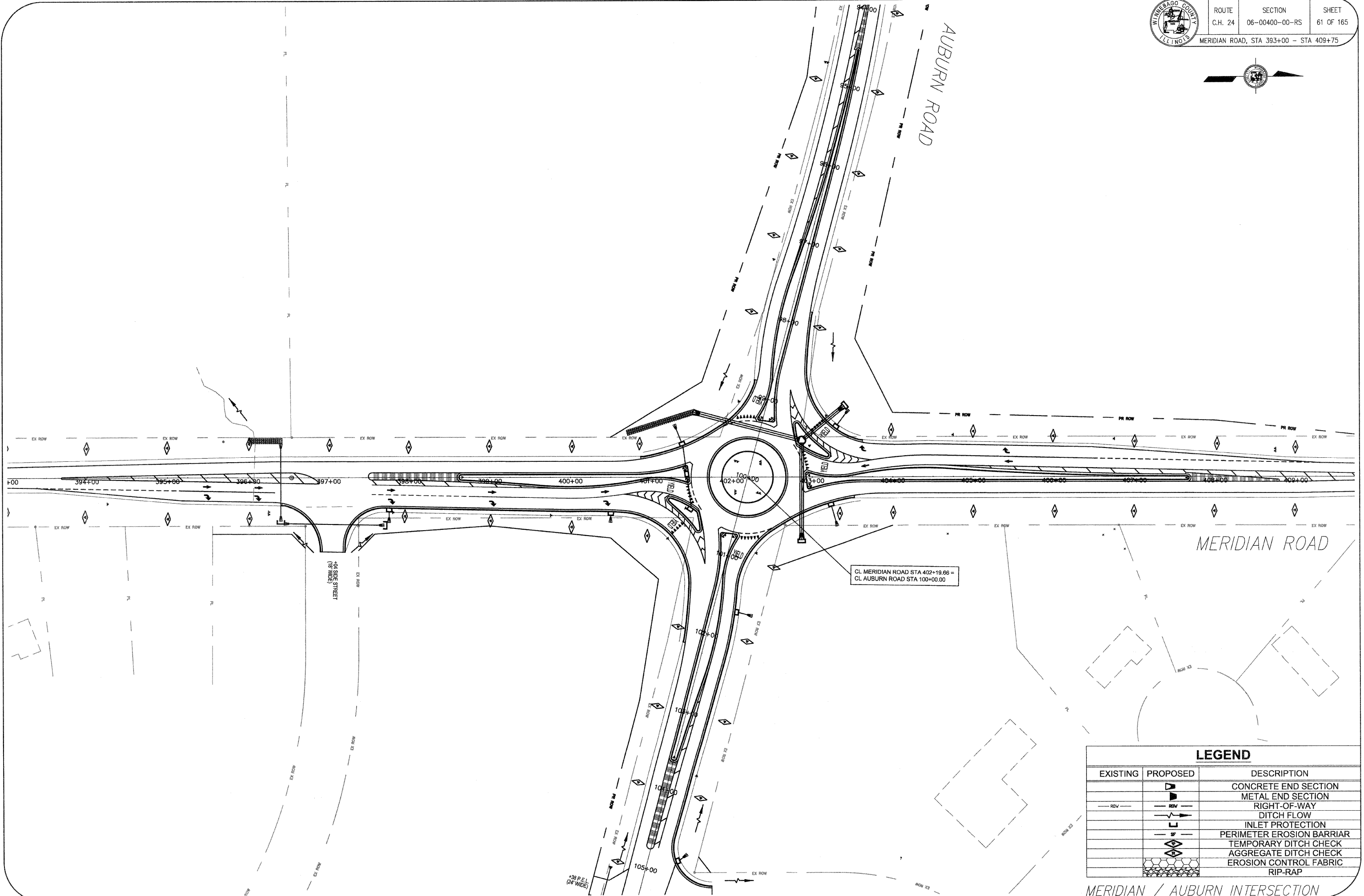
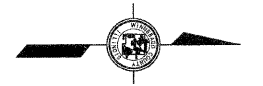


STAGE II PHASE II

1. RELOCATE OR COVER EXISTING REGULATORY SIGNS AS APPLICABLE
2. PLACE SIGNING, FLAGGERS AND TRAFFIC CONTROL DEVICES PER STD. 701701 AND AS SHOWN (ALL TEMPORARY TRAFFIC CONTROL PLACEMENT BASED ON 45 MPH DESIGN)
3. REMOVE & RECONSTRUCT EXISTING MEDIAN CURB IN AREAS SHOWN, CLOSE SIDE ROADS AS DIRECTED BY ENGINEER
4. POST AN ADVISORY SPEED LIMIT OF 35 MPH THROUGH CONSTRUCTION AREA

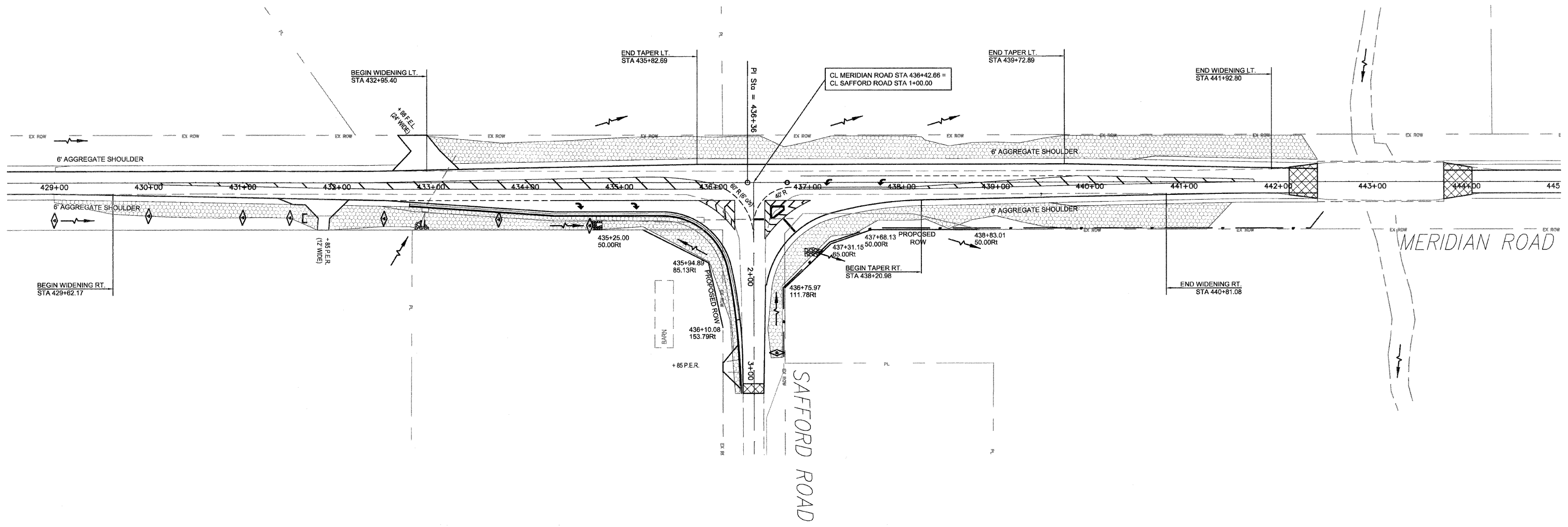
STAGE III

1. RELOCATE OR COVER EXISTING REGULATORY SIGNS AS APPLICABLE
2. PLACE SIGNING, FLAGGERS AND TRAFFIC CONTROL DEVICES PER STD. 701701
3. REMOVE CORNER ISLANDS AND EXISTING TRAFFIC SIGNALS AT THE INTERSECTION USING STD. 701701.
4. POST AN ADVISORY SPEED LIMIT OF 35 MPH THROUGH CONSTRUCTION AREA



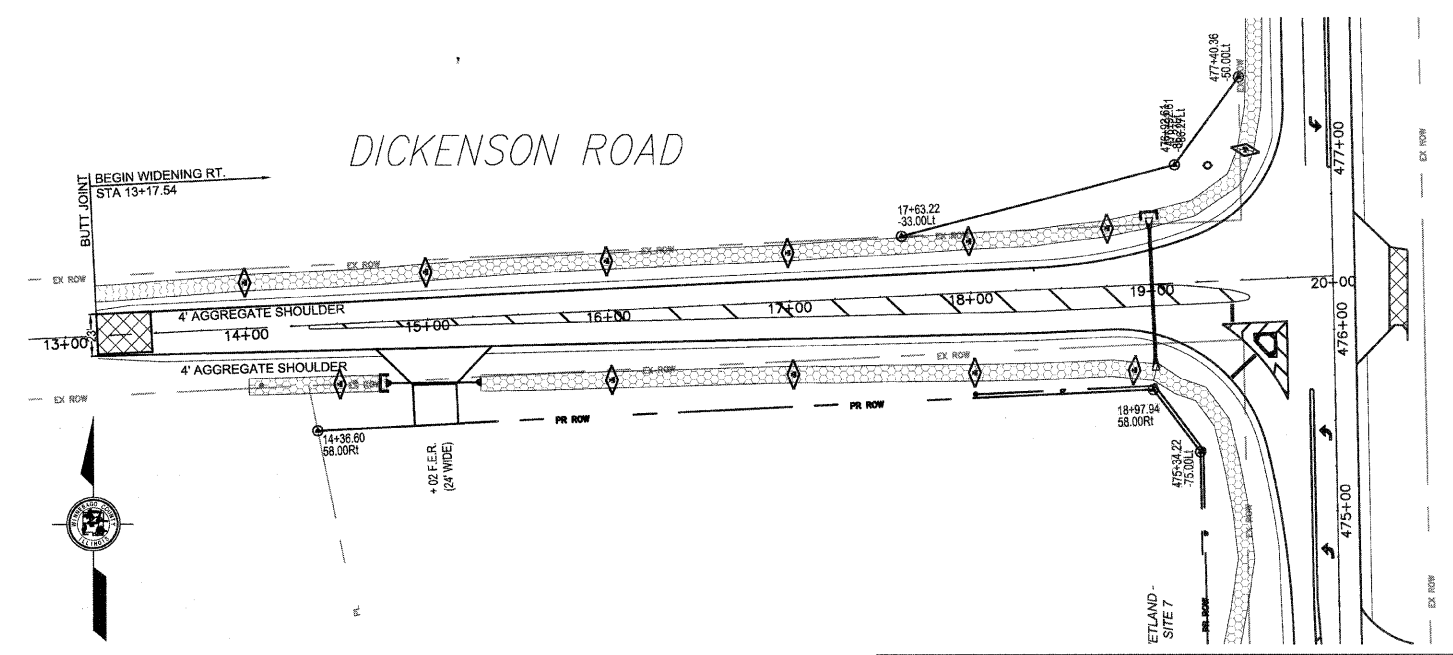
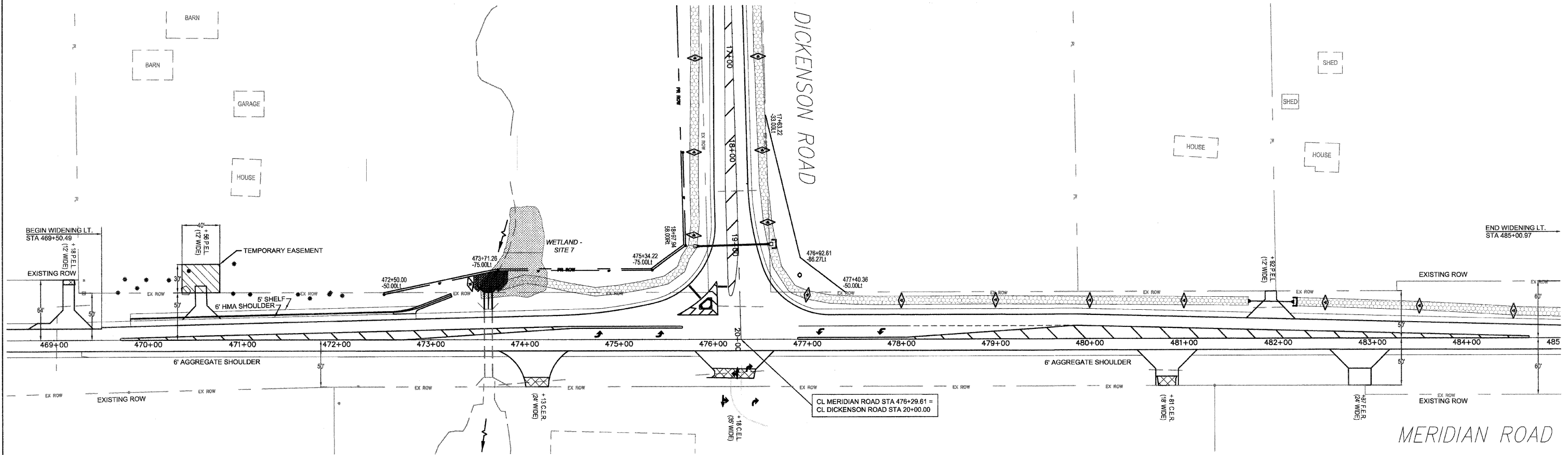
LEGEND		
EXISTING	PROPOSED	DESCRIPTION
		CONCRETE END SECTION
		METAL END SECTION
		RIGHT-OF-WAY
		DITCH FLOW
		INLET PROTECTION
		PERIMETER EROSION BARRIAR
		TEMPORARY DITCH CHECK
		AGGREGATE DITCH CHECK
		EROSION CONTROL FABRIC
		RIP-RAP

MERIDIAN / AUBURN INTERSECTION
EROSION CONTROL PLAN



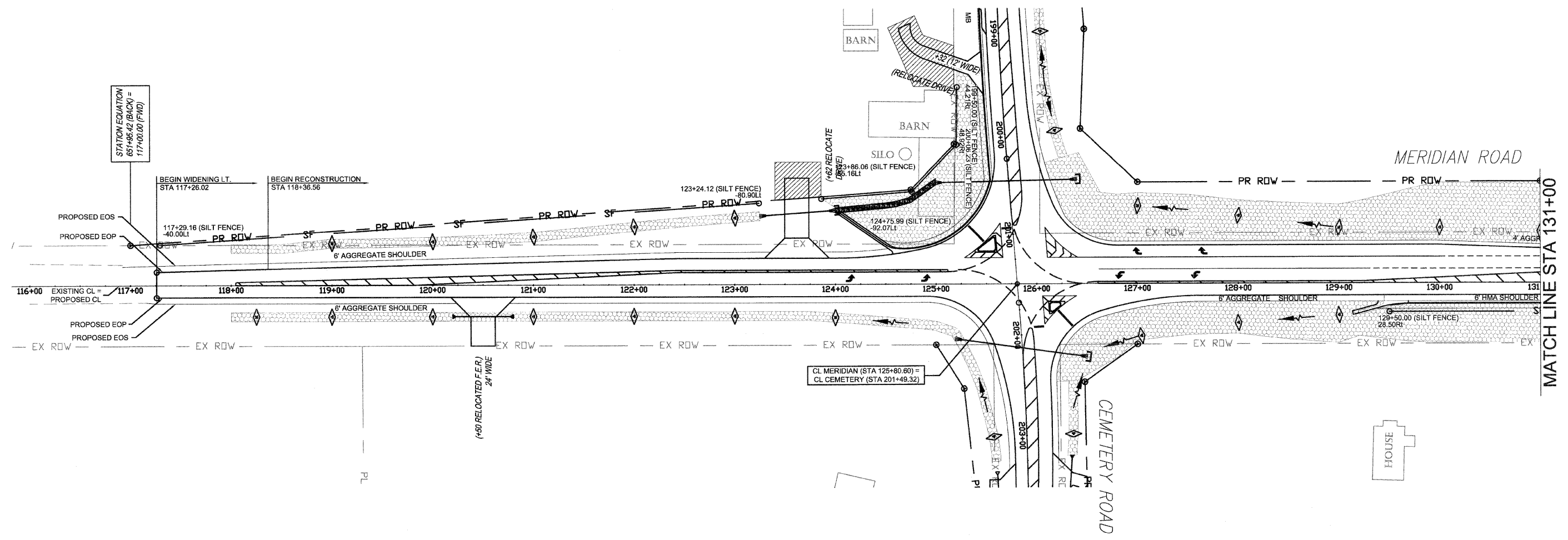
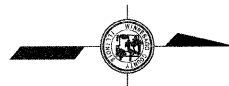
LEGEND		
EXISTING	PROPOSED	DESCRIPTION
		CONCRETE END SECTION
		METAL END SECTION
		RIGHT-OF-WAY
		DITCH FLOW
		INLET PROTECTION
		PERIMETER EROSION BARRIAR
		TEMPORARY DITCH CHECK
		AGGREGATE DITCH CHECK
		EROSION CONTROL FABRIC
		RIP-RAP

MERIDIAN / SAFFORD INTERSECTION
EROSION CONTROL PLAN

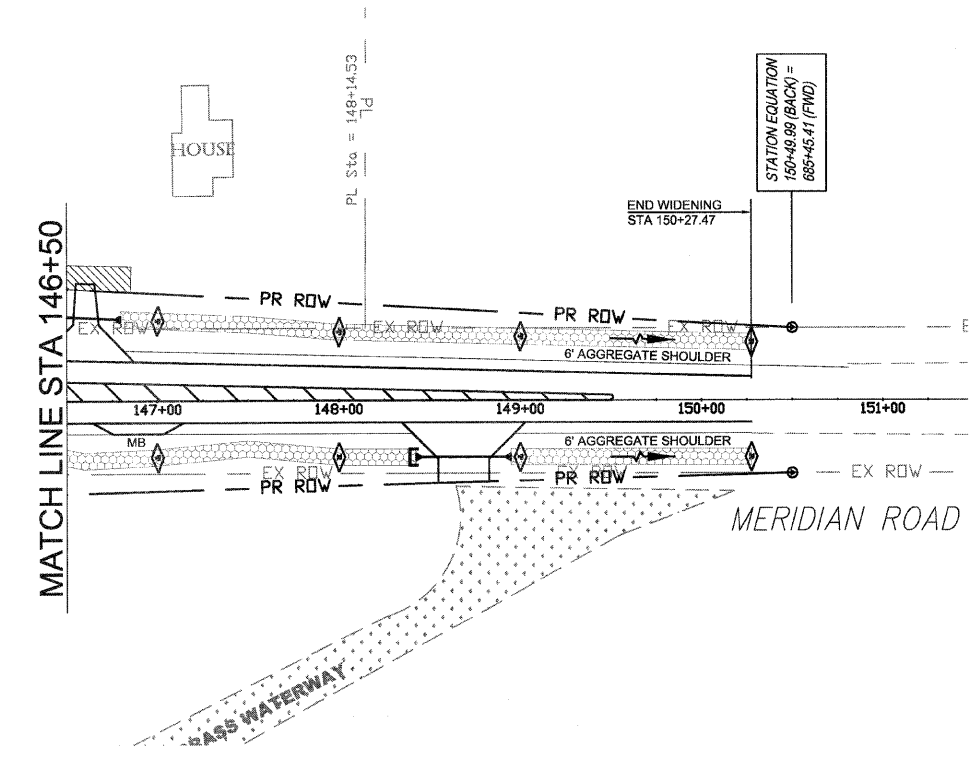
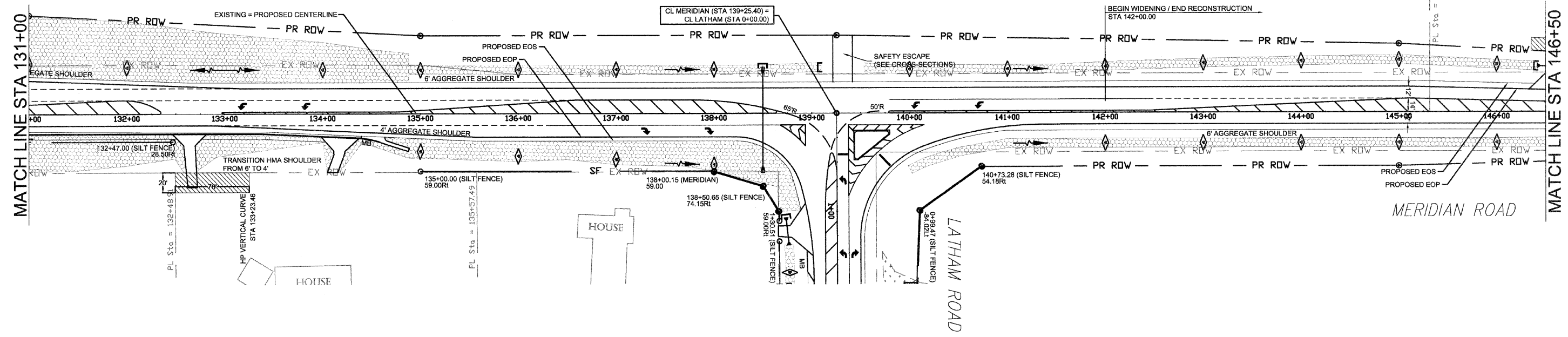


LEGEND		
EXISTING	PROPOSED	DESCRIPTION
		CONCRETE END SECTION
		METAL END SECTION
		RIGHT-OF-WAY
		DITCH FLOW
		INLET PROTECTION
		PERIMETER EROSION BARRIER
		TEMPORARY DITCH CHECK
		AGGREGATE DITCH CHECK
		EROSION CONTROL FABRIC
		RIP-RAP

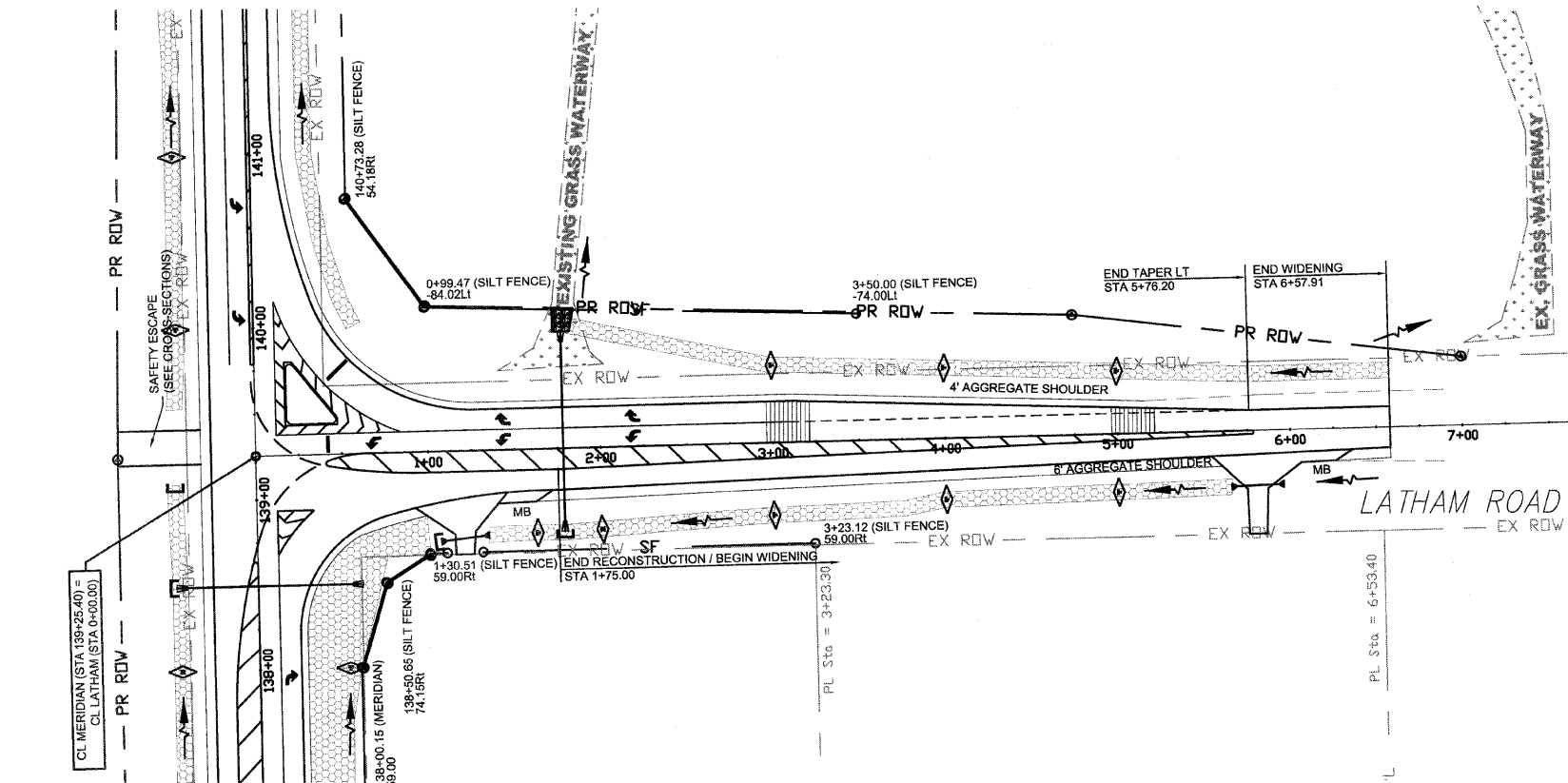
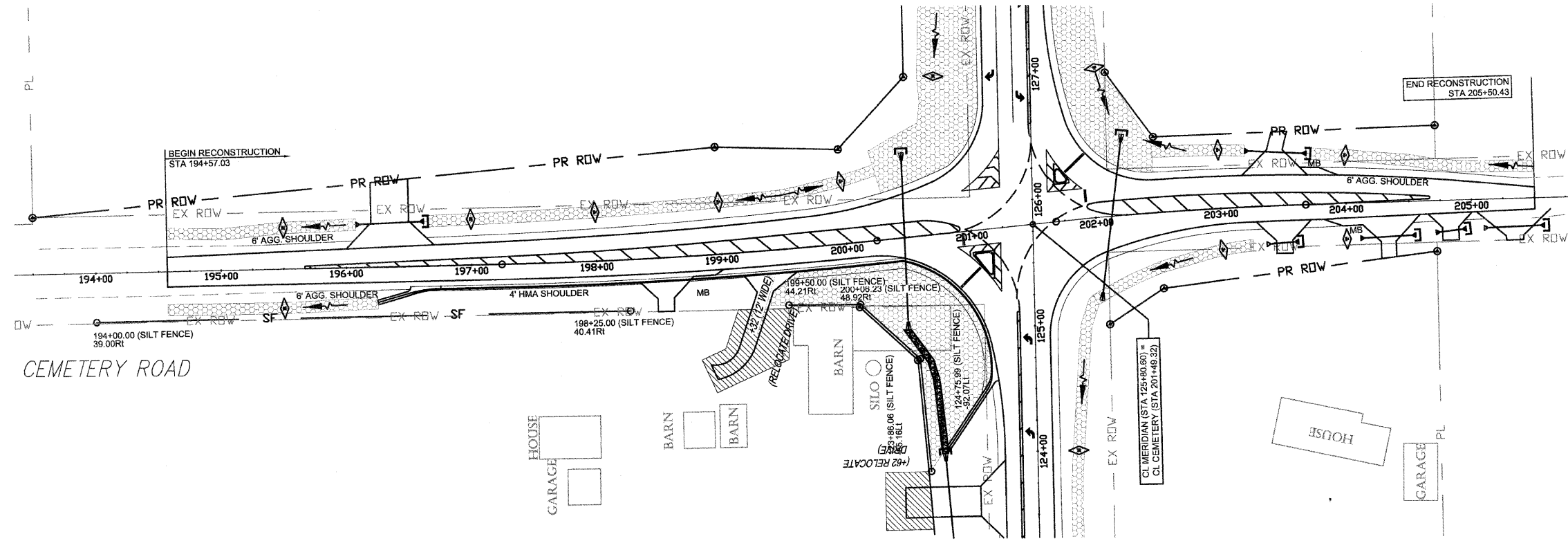
MERIDIAN / DICKENSON INTERSECTION
 EROSION CONTROL PLAN



LEGEND		
EXISTING	PROPOSED	DESCRIPTION
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		METAL END SECTION
		RIGHT-OF-WAY
		DITCH FLOW
		INLET PROTECTION
		PERIMETER EROSION BARRIAR
		TEMPORARY DITCH CHECK
		AGGREGATE DITCH CHECK
		EROSION CONTROL FABRIC
		RIP-RAP

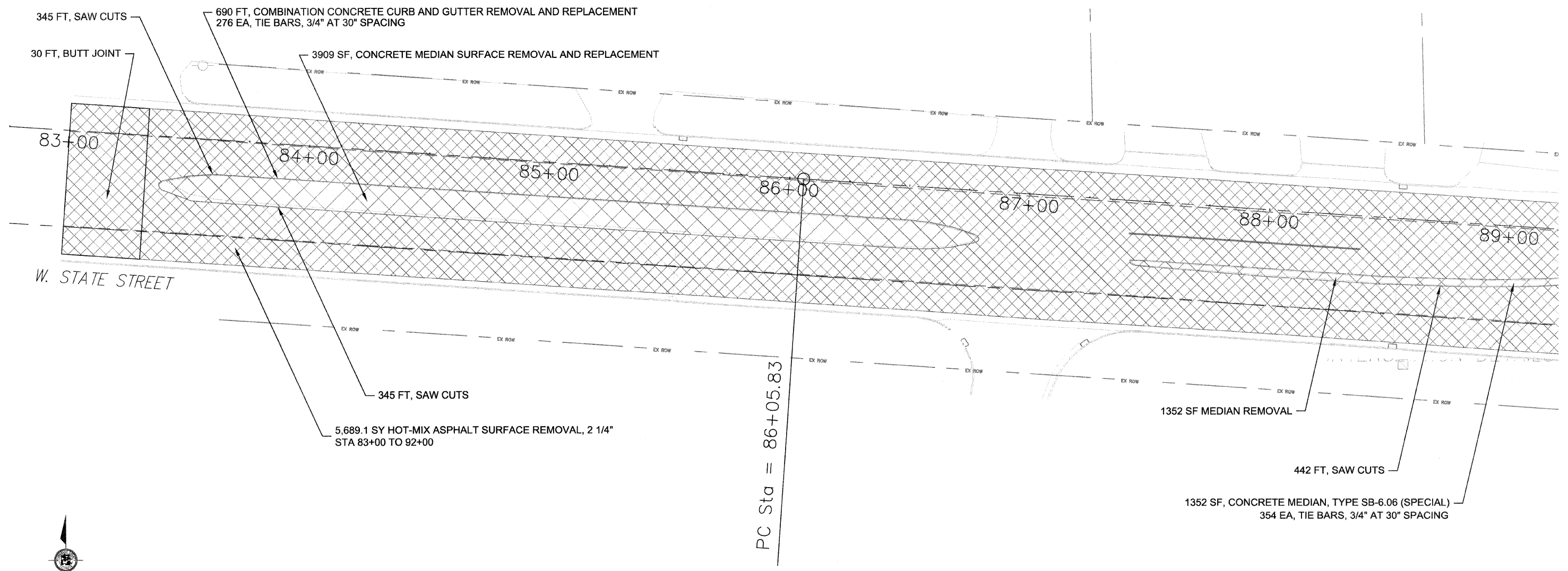


LEGEND		
EXISTING	PROPOSED	DESCRIPTION
		CONCRETE END SECTION
		METAL END SECTION
		RIGHT-OF-WAY
		DITCH FLOW
		INLET PROTECTION
		PERIMETER EROSION BARRIER
		TEMPORARY DITCH CHECK
		AGGREGATE DITCH CHECK
		EROSION CONTROL FABRIC
		RIP-RAP



LEGEND		
EXISTING	PROPOSED	DESCRIPTION
		CONCRETE END SECTION
		METAL END SECTION
		RIGHT-OF-WAY
		DITCH FLOW
		INLET PROTECTION
		PERIMETER EROSION BARRIER
		TEMPORARY DITCH CHECK
		AGGREGATE DITCH CHECK
		EROSION CONTROL FABRIC
		RIP-RAP

ELEVATIONS FOR THE PROPOSED MEDIAN WORK ON STATE STREET HAVE NOT BEEN PROVIDED. THE CONTRACTOR SHALL USE THE EXISTING PAVEMENT ELEVATION TO CONSTRUCT THE CURB.

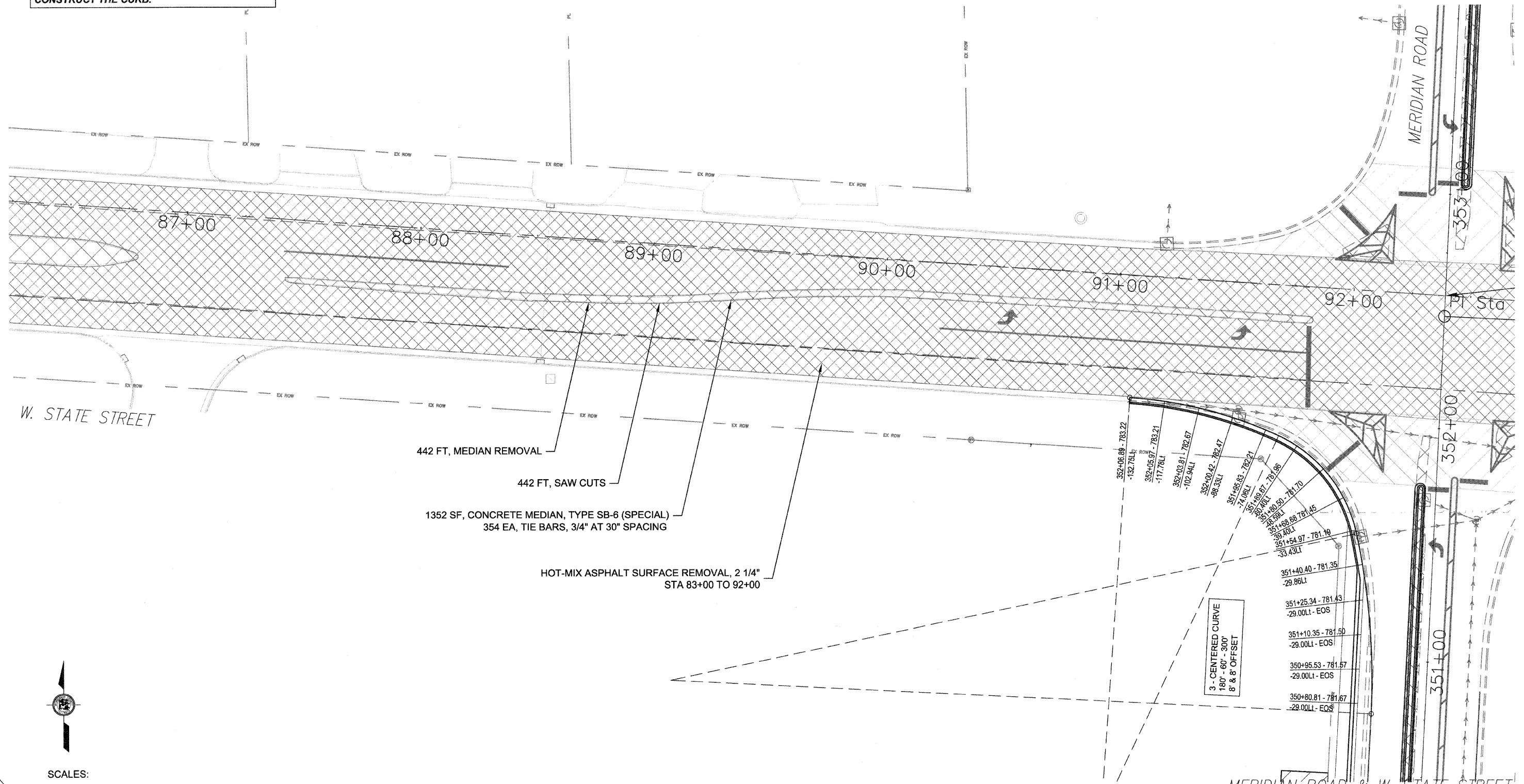


SCALES:
1" = 20' HOR

SEE SHEET 90 OF 165 THROUGH 92 OF 165 FOR TRAFFIC SIGNAL DETAILS

ELEVATIONS SHOWN FOR PROPOSED CURB ARE BASED ON EXISTING CURB ELEVATIONS. THE CONTRACT SHALL CONFIRM ELEVATIONS WITH THE ENGINEER PRIOR TO PLACING THE PROPOSED CONCRETE CURB

ELEVATIONS FOR THE PROPOSED MEDIAN WORK ON STATE STREET HAVE NOT BEEN PROVIDED. THE CONTRACTOR SHALL USE THE EXISTING PAVEMENT ELEVATION TO CONSTRUCT THE CURB.



SCALES:
1" = 20' HOR

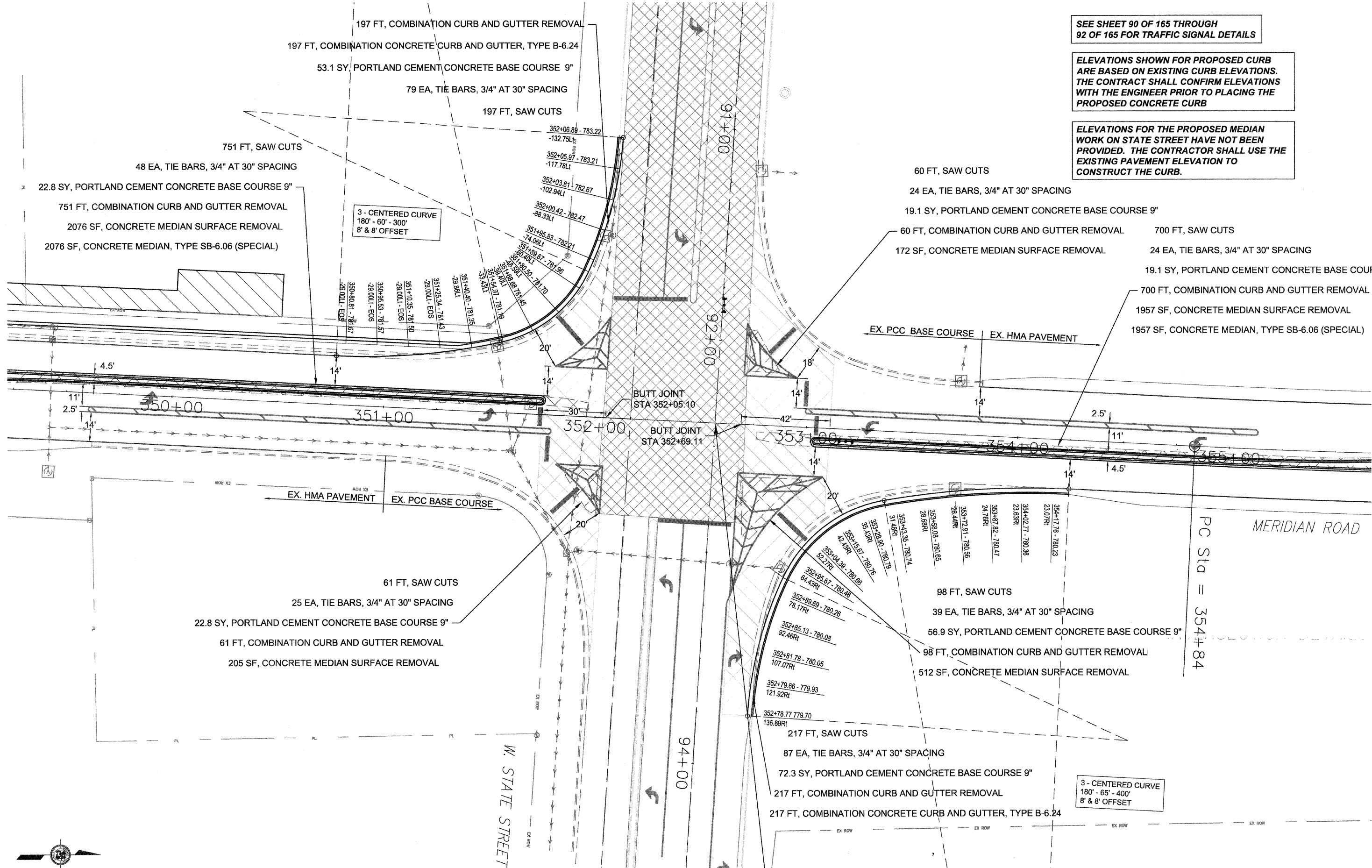
MERIDIAN ROAD & W. STATE STREET INTERSECTION DETAILS



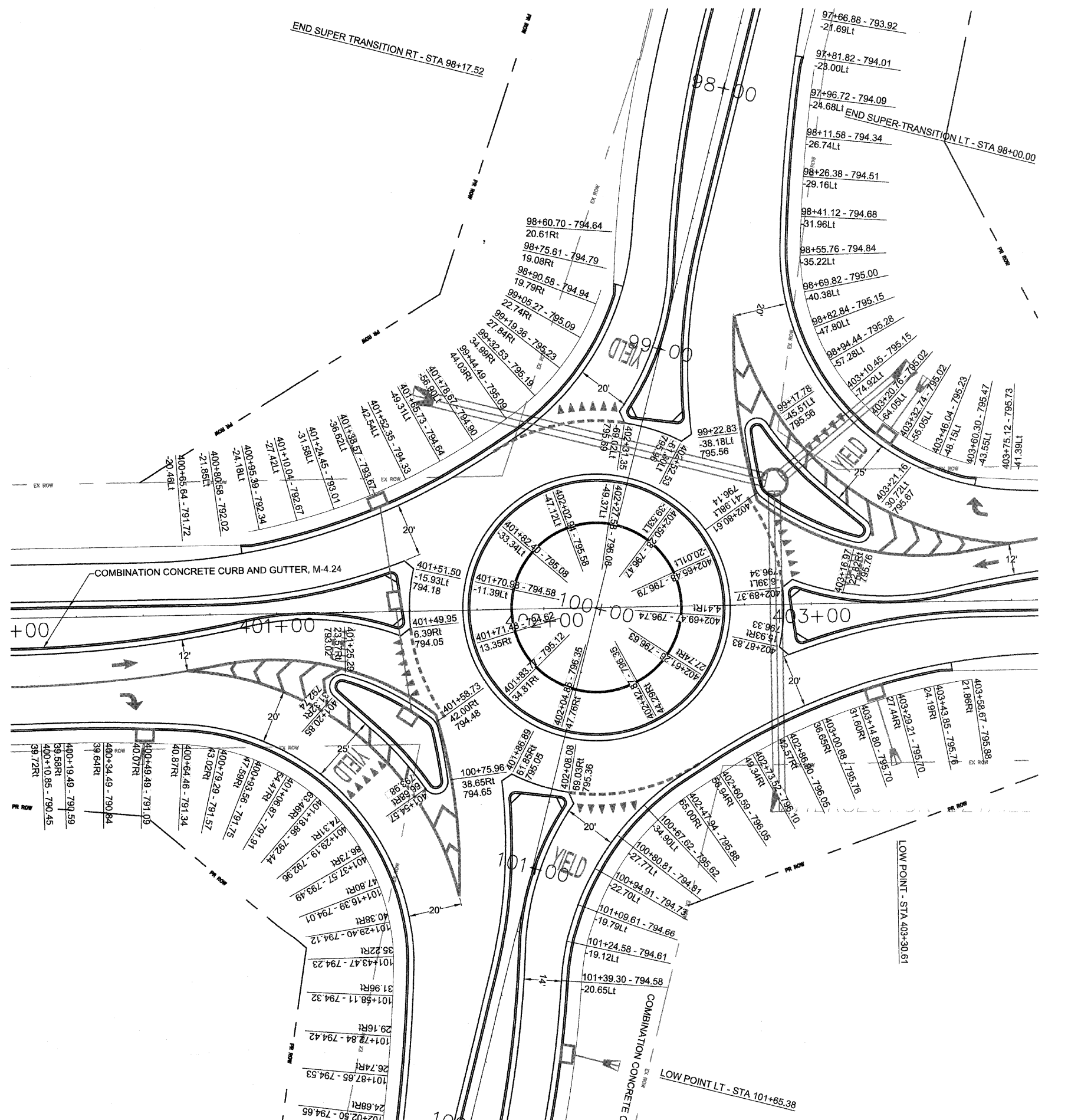
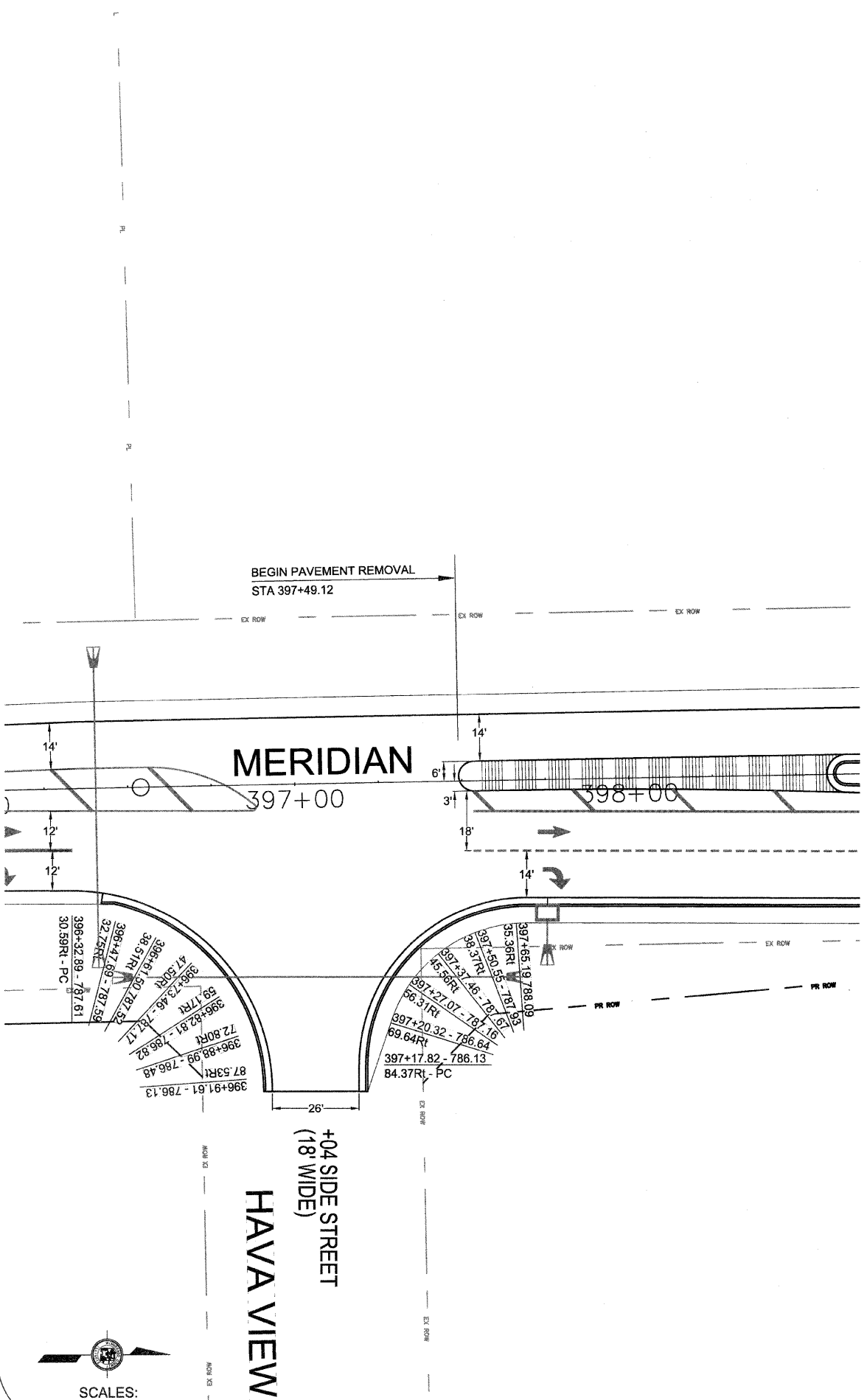
SEE SHEET 90 OF 165 THROUGH 92 OF 165 FOR TRAFFIC SIGNAL DETAILS

ELEVATIONS SHOWN FOR PROPOSED CURB ARE BASED ON EXISTING CURB ELEVATIONS. THE CONTRACTOR SHALL CONFIRM ELEVATIONS WITH THE ENGINEER PRIOR TO PLACING THE PROPOSED CONCRETE CURB

ELEVATIONS FOR THE PROPOSED MEDIAN WORK ON STATE STREET HAVE NOT BEEN PROVIDED. THE CONTRACTOR SHALL USE THE EXISTING PAVEMENT ELEVATION TO CONSTRUCT THE CURB.

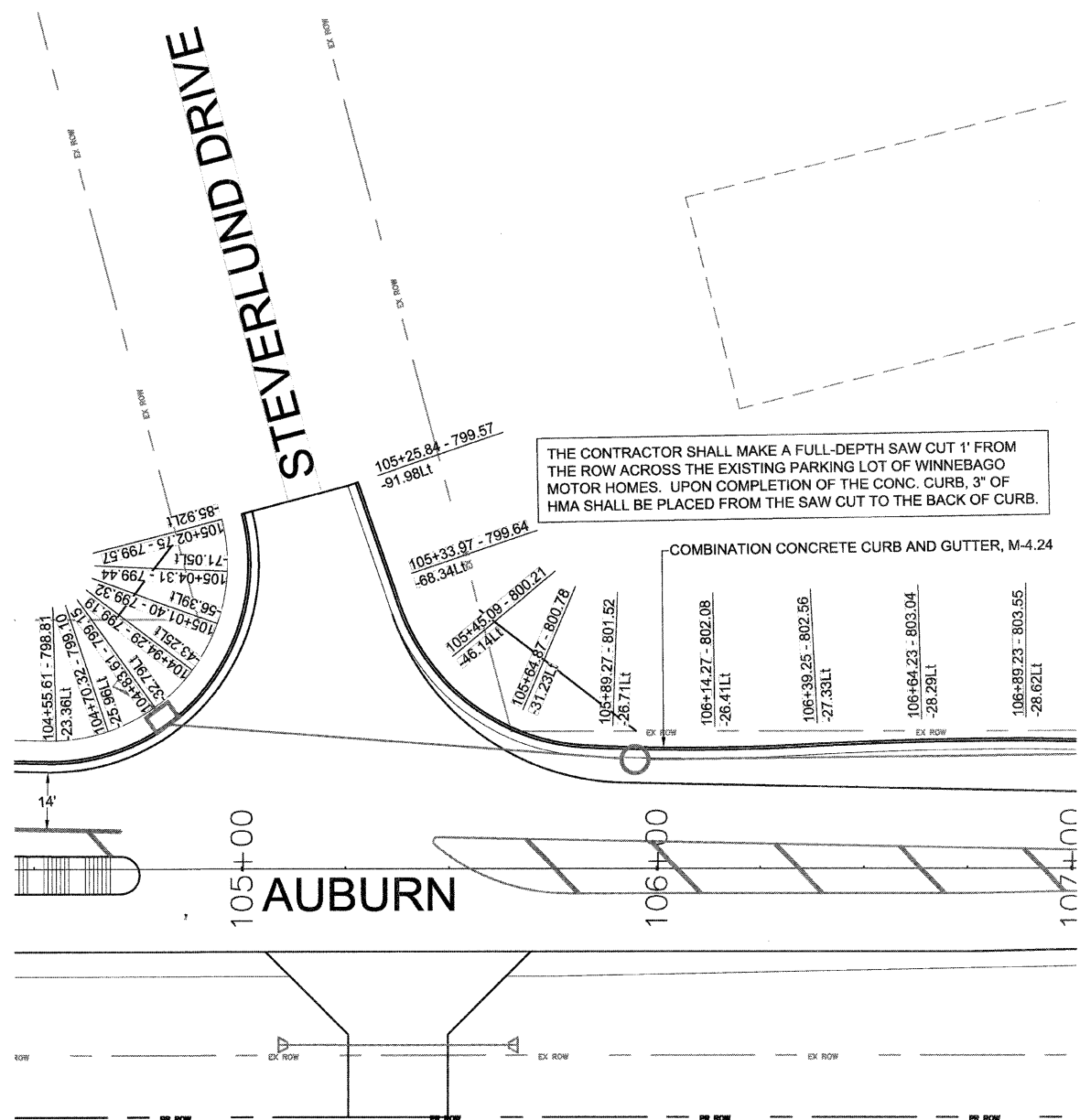
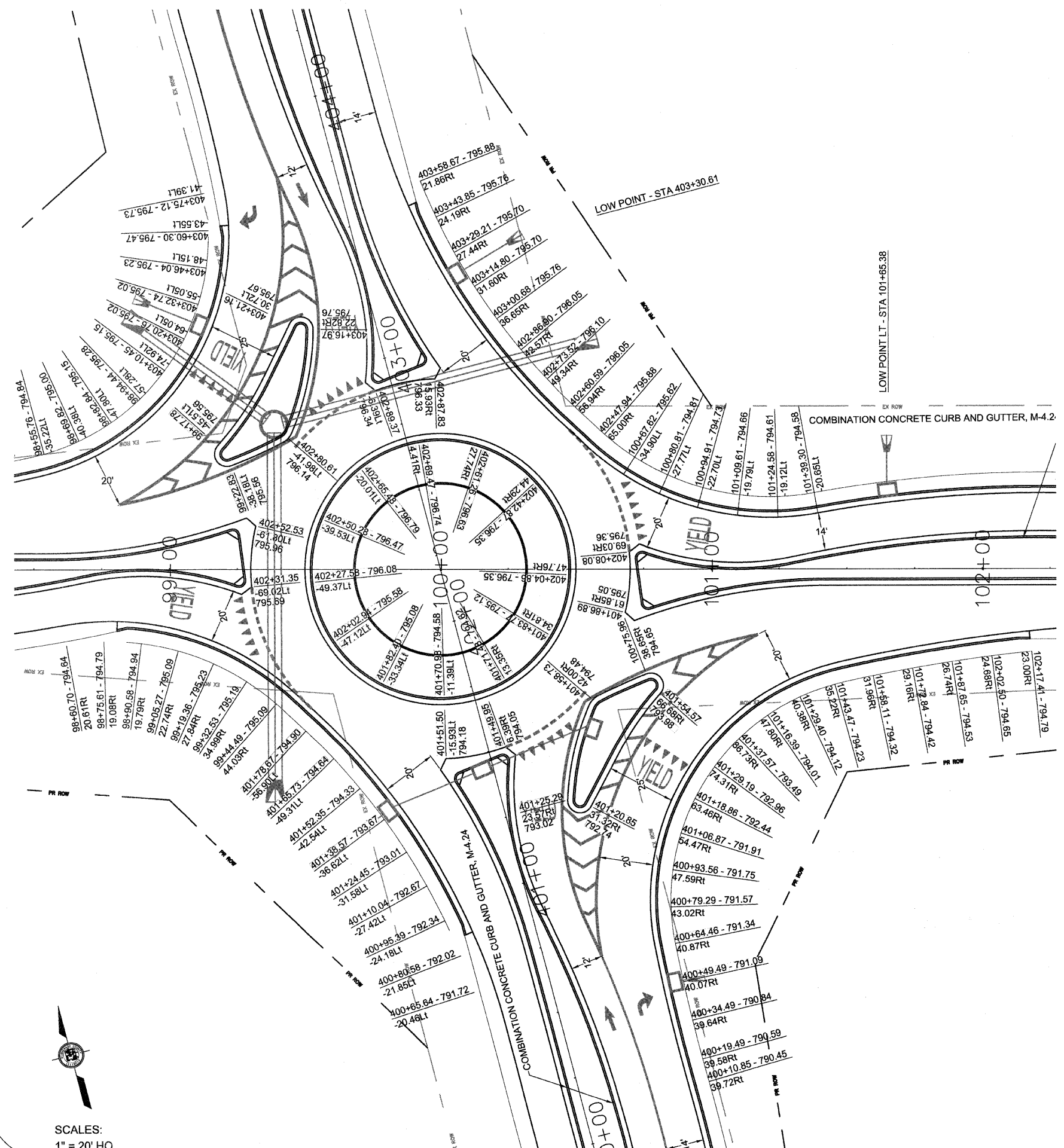


SCALES:
1" = 20' HOR



SCALES:
1" = 20.0' HOR

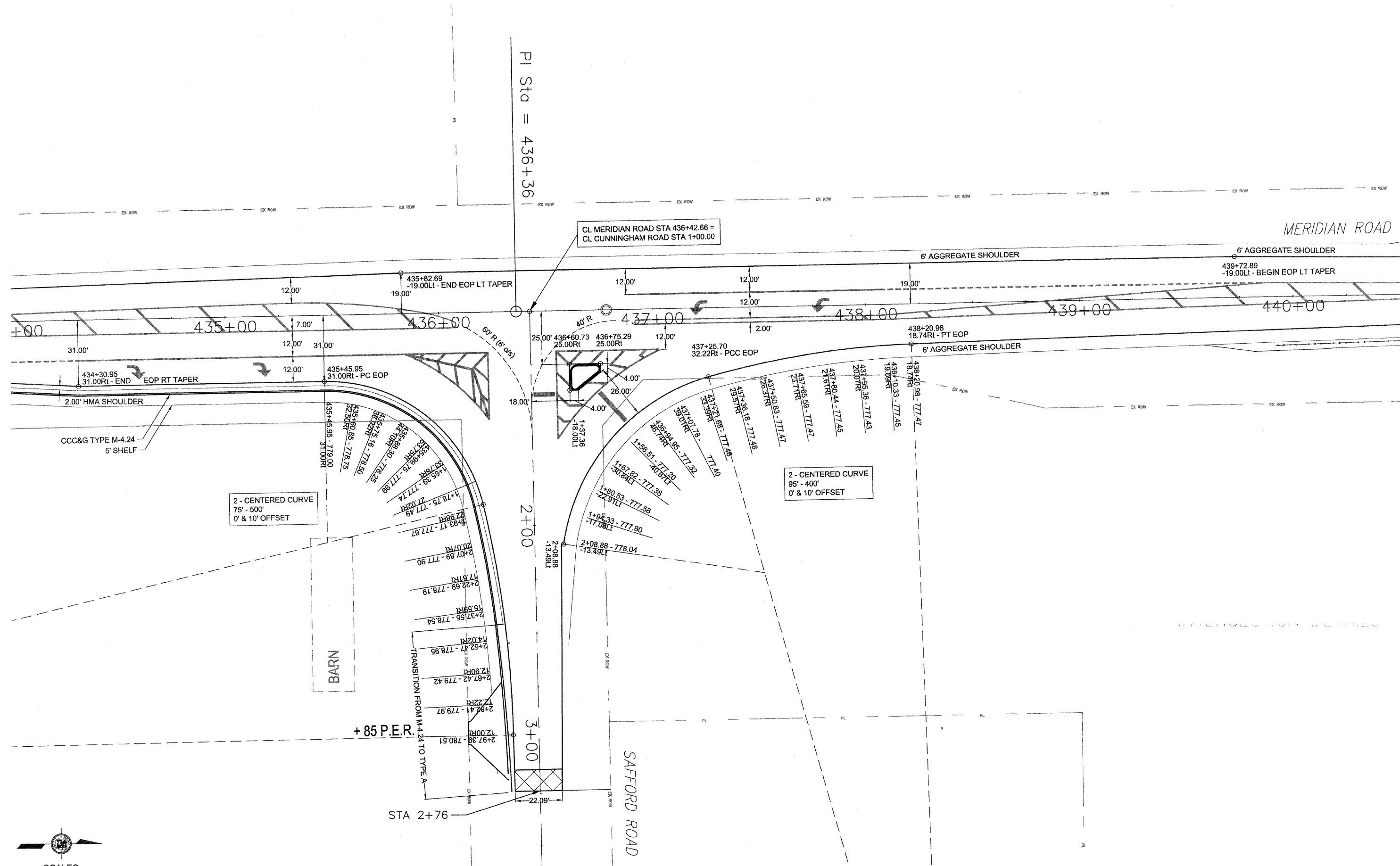
MERIDIAN ROAD & AUBURN ROAD INTERSECTION DETAILS



THE CONTRACTOR SHALL MAKE A FULL-DEPTH SAW CUT 1' FROM THE ROW ACROSS THE EXISTING PARKING LOT OF WINNEBAGO MOTOR HOMES. UPON COMPLETION OF THE CONC. CURB, 3" OF HMA SHALL BE PLACED FROM THE SAW CUT TO THE BACK OF CURB.

CONCRETE WORK AT SIDE ROADS AND DRIVEWAYS SHALL BE STAGED SO THAT AT ALL TIMES VEHICLES WILL HAVE ACCESS.

SCALES:
1" = 20' HO



CL MERIDIAN ROAD STA 436+42.66 =
 CL CUNNINGHAM ROAD STA 1+00.00

2 - CENTERED CURVE
 95' - 400'
 0' & 10' OFFSET

2 - CENTERED CURVE
 75' - 500'
 0' & 10' OFFSET

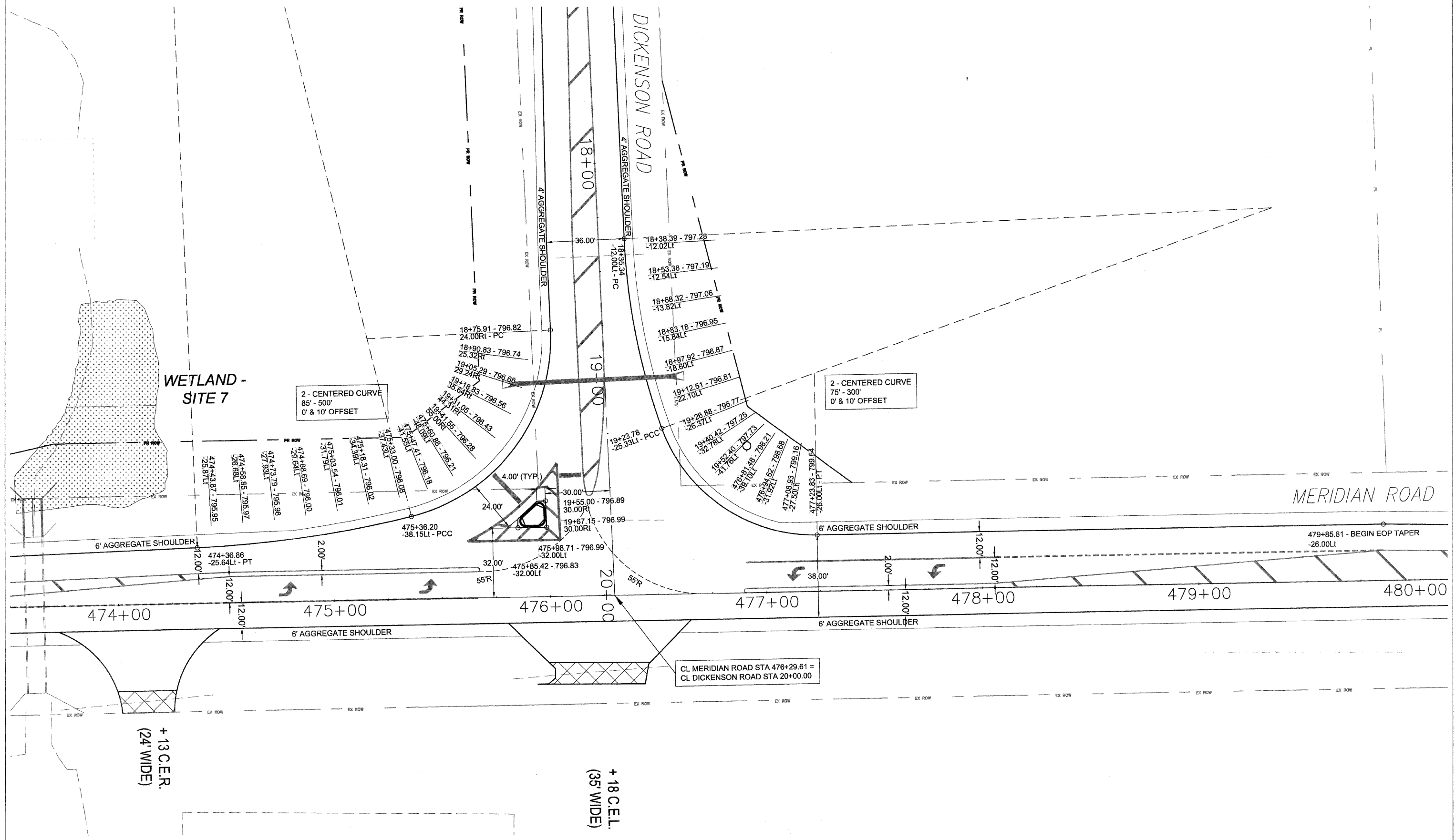
BARN

SAFFORD ROAD

MERIDIAN ROAD

SCALES:
 1" = 20' HOR

MERIDIAN ROAD & SAFFORD ROAD
 INTERSECTION DETAILS



CL MERIDIAN ROAD STA 476+29.61 =
 CL DICKENSON ROAD STA 20+00.00

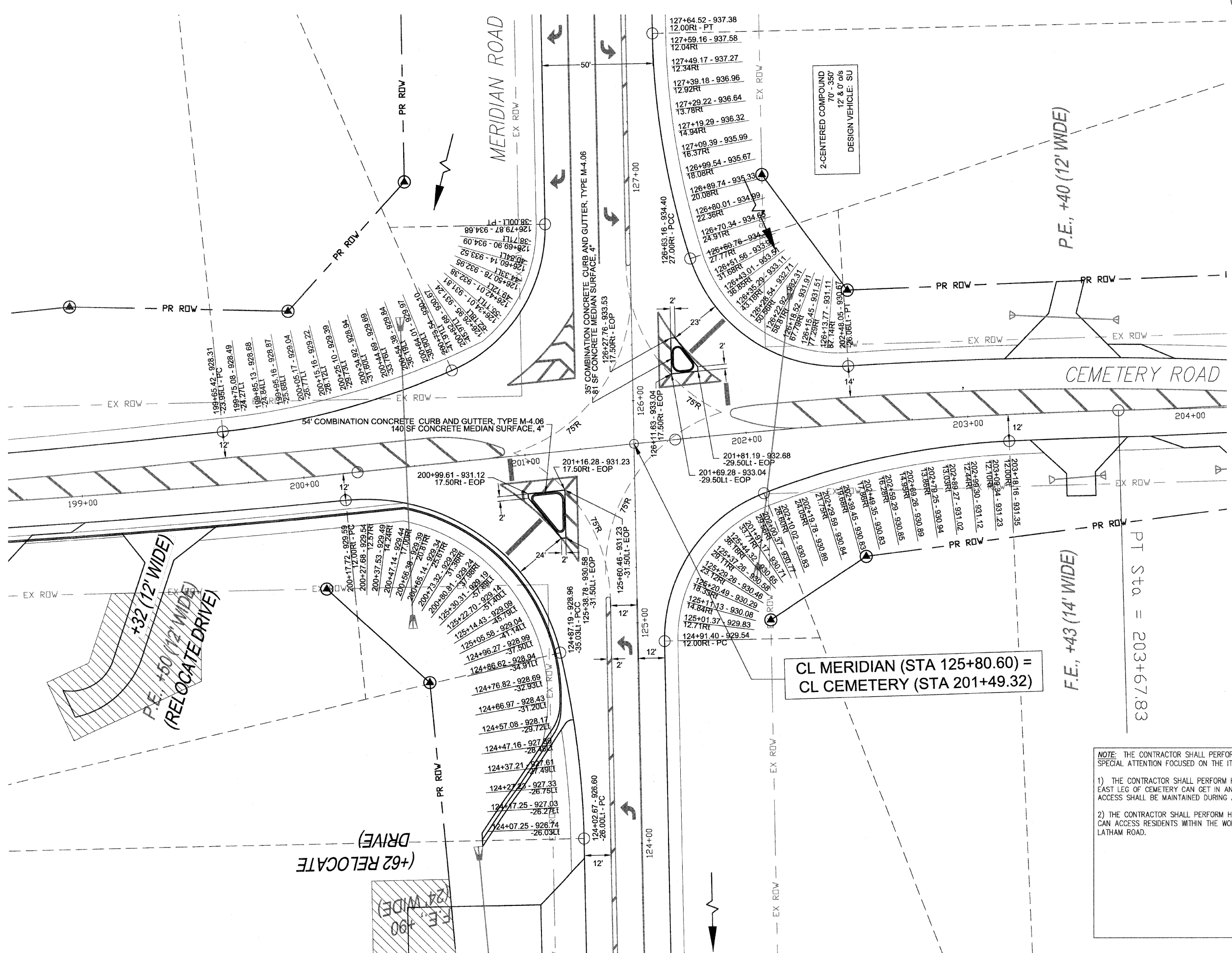
+ 13' C.E.R.
 (24' WIDE)

+ 18' C.E.L.
 (35' WIDE)

SCALES:
 1" = 20'



SCALE: 1"=20'



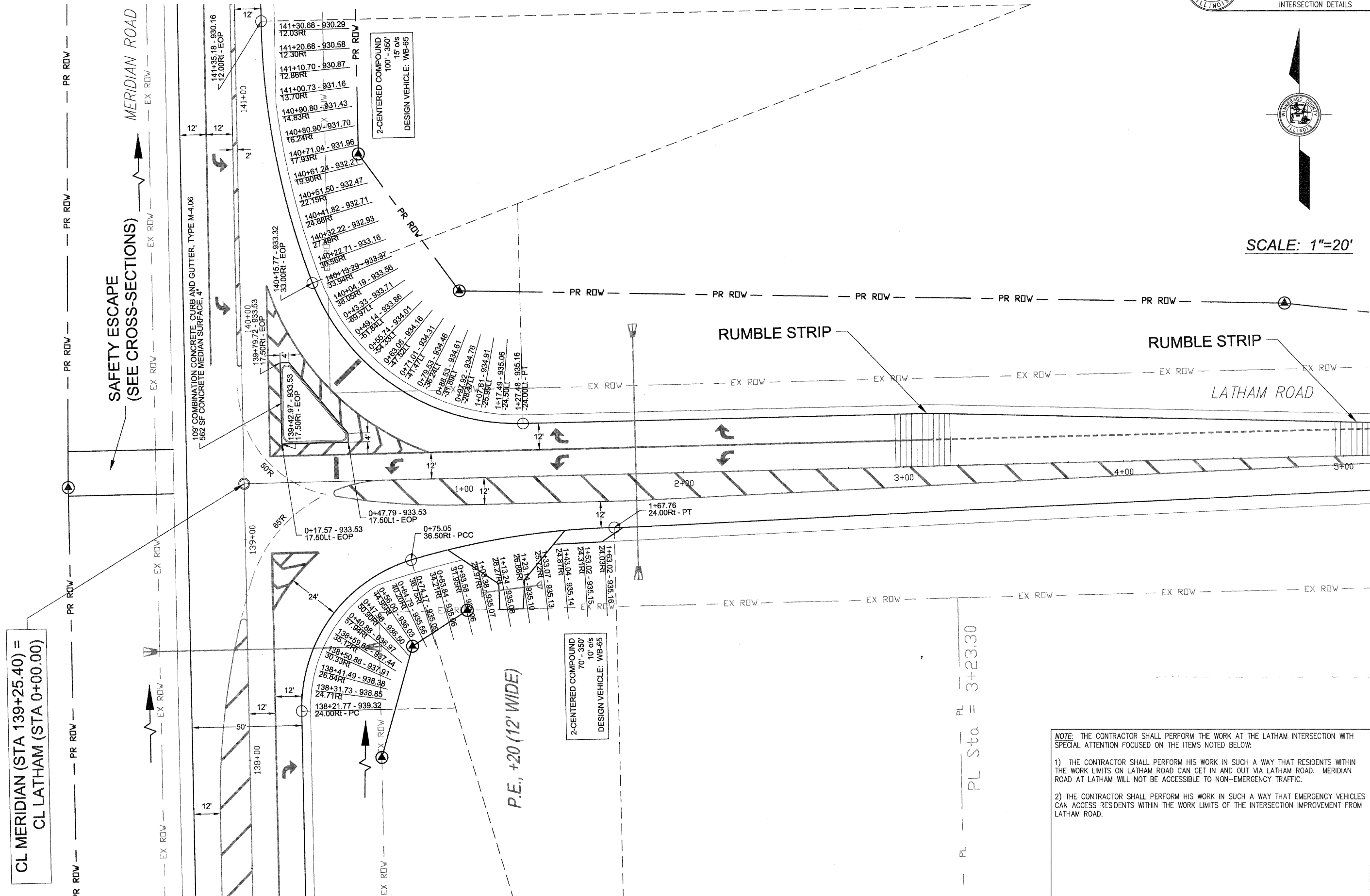
CL MERIDIAN (STA 125+80.60) = CL CEMETERY (STA 201+49.32)

NOTE: THE CONTRACTOR SHALL PERFORM THE WORK AT THE CEMETERY INTERSECTION WITH SPECIAL ATTENTION FOCUSED ON THE ITEMS NOTED BELOW:

- 1) THE CONTRACTOR SHALL PERFORM HIS WORK IN SUCH A WAY THAT RESIDENTS ON THE EAST LEG OF CEMETERY CAN GET IN AND OUT VIA THE SOUTH LEG OF MERIDIAN. THIS ACCESS SHALL BE MAINTAINED DURING AND AFTER WORKING HOURS.
- 2) THE CONTRACTOR SHALL PERFORM HIS WORK IN SUCH A WAY THAT EMERGENCY VEHICLES CAN ACCESS RESIDENTS WITHIN THE WORK LIMITS OF THE INTERSECTION IMPROVEMENT FROM LATHAM ROAD.



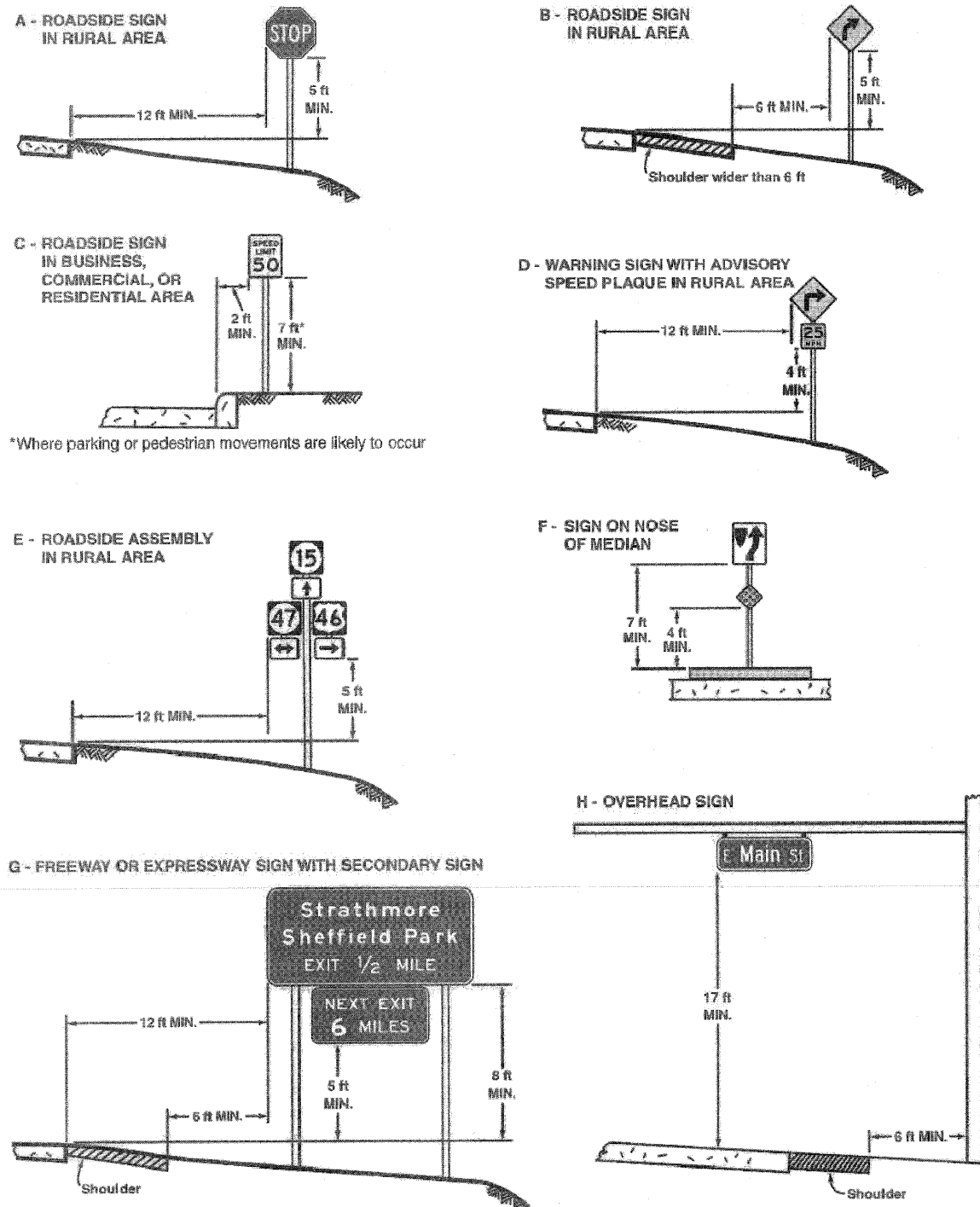
SCALE: 1"=20'



NOTE: THE CONTRACTOR SHALL PERFORM THE WORK AT THE LATHAM INTERSECTION WITH SPECIAL ATTENTION FOCUSED ON THE ITEMS NOTED BELOW:

- 1) THE CONTRACTOR SHALL PERFORM HIS WORK IN SUCH A WAY THAT RESIDENTS WITHIN THE WORK LIMITS ON LATHAM ROAD CAN GET IN AND OUT VIA LATHAM ROAD. MERIDIAN ROAD AT LATHAM WILL NOT BE ACCESSIBLE TO NON-EMERGENCY TRAFFIC.
- 2) THE CONTRACTOR SHALL PERFORM HIS WORK IN SUCH A WAY THAT EMERGENCY VEHICLES CAN ACCESS RESIDENTS WITHIN THE WORK LIMITS OF THE INTERSECTION IMPROVEMENT FROM LATHAM ROAD.

Figure 2A-2. Examples of Heights and Lateral Locations of Sign Installations

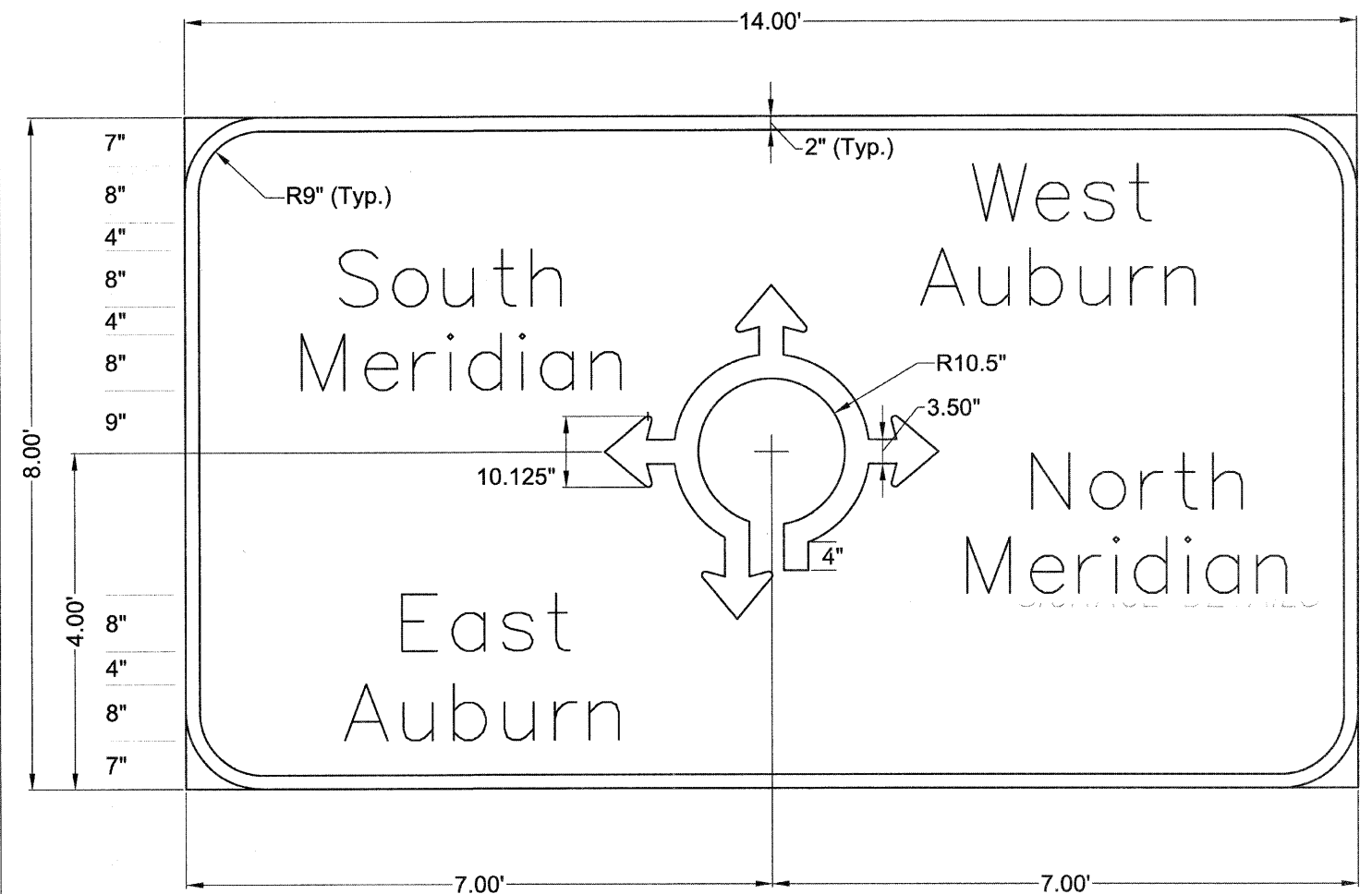


*Where parking or pedestrian movements are likely to occur

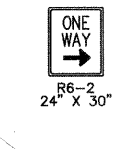
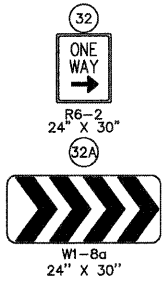
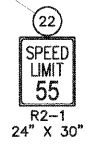
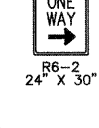
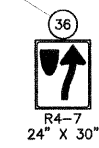
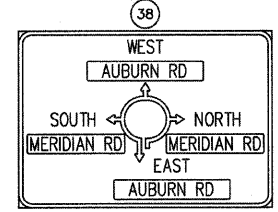
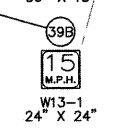
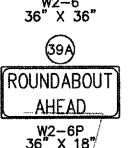
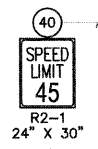
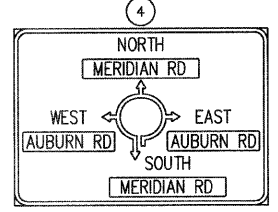
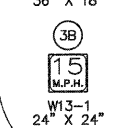
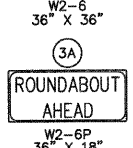
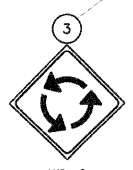
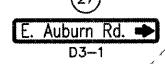
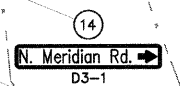
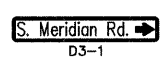
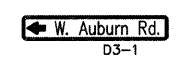
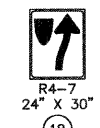
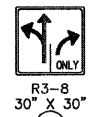
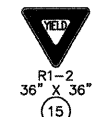
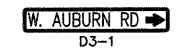
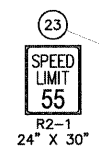
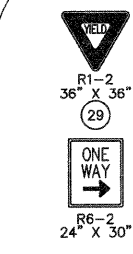
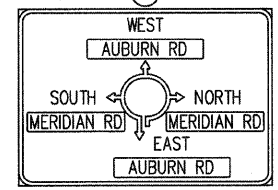
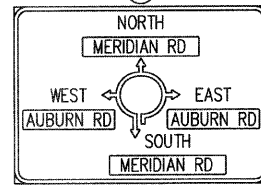
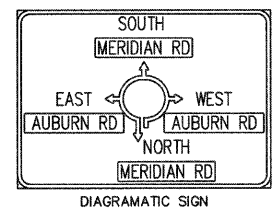
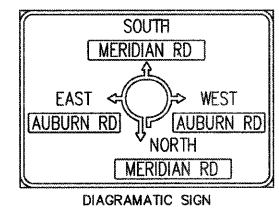
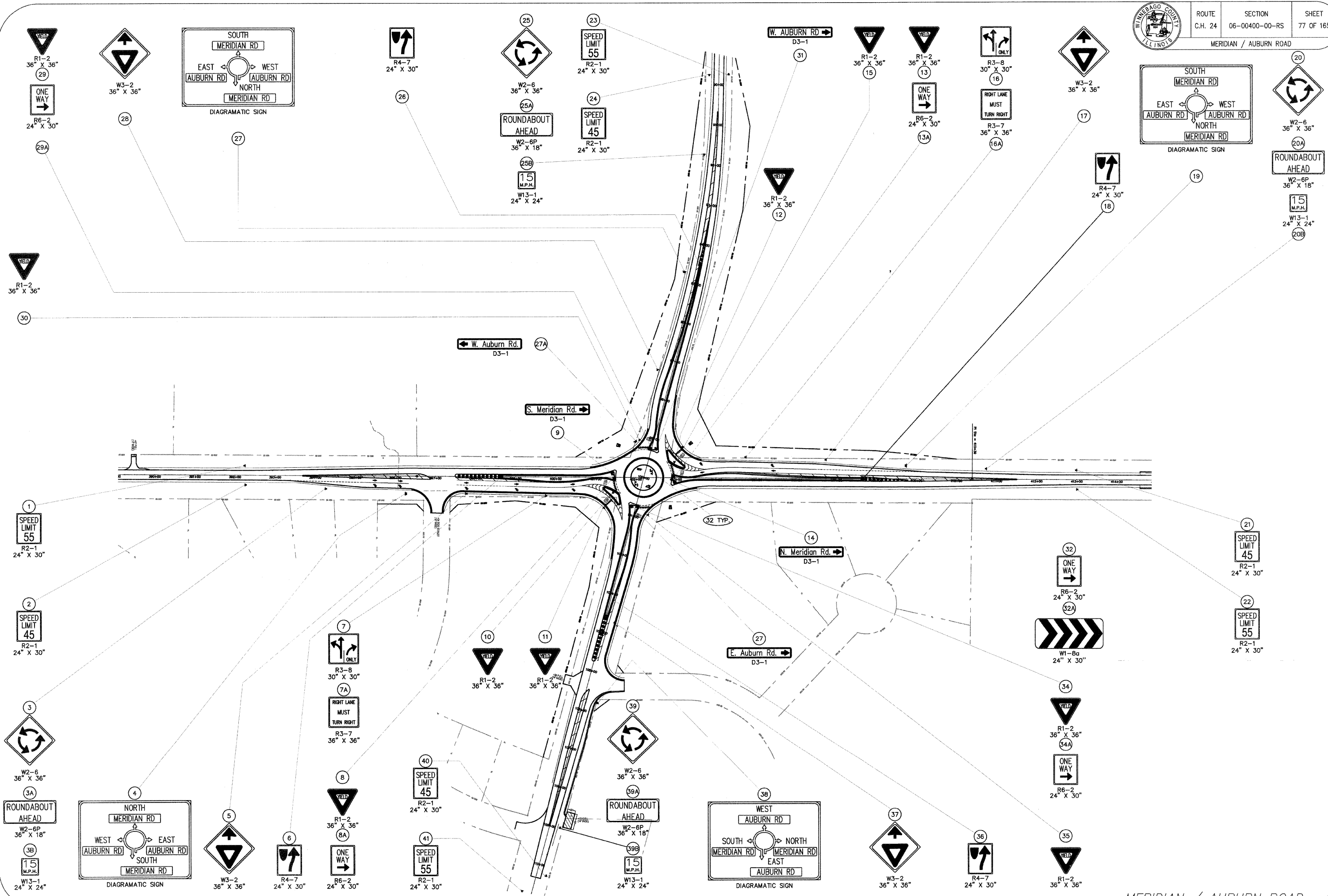
Note:
 See Section 2A.19 for reduced lateral offset distances that may be used in areas where lateral offsets are limited, and in business, commercial, or residential areas where sidewalk width is limited or where existing poles are close to the curb.

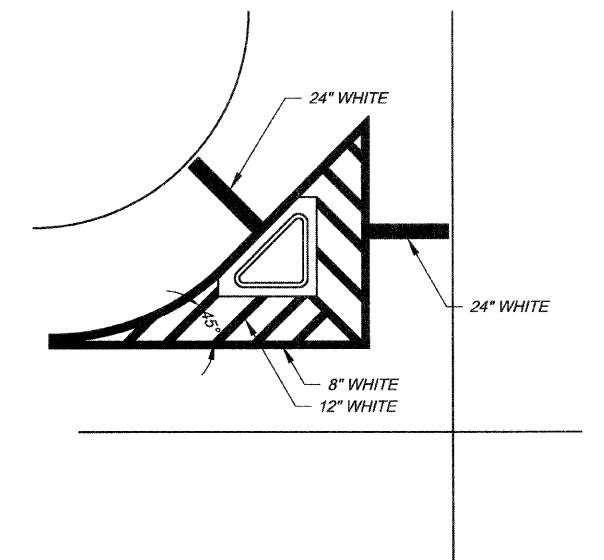
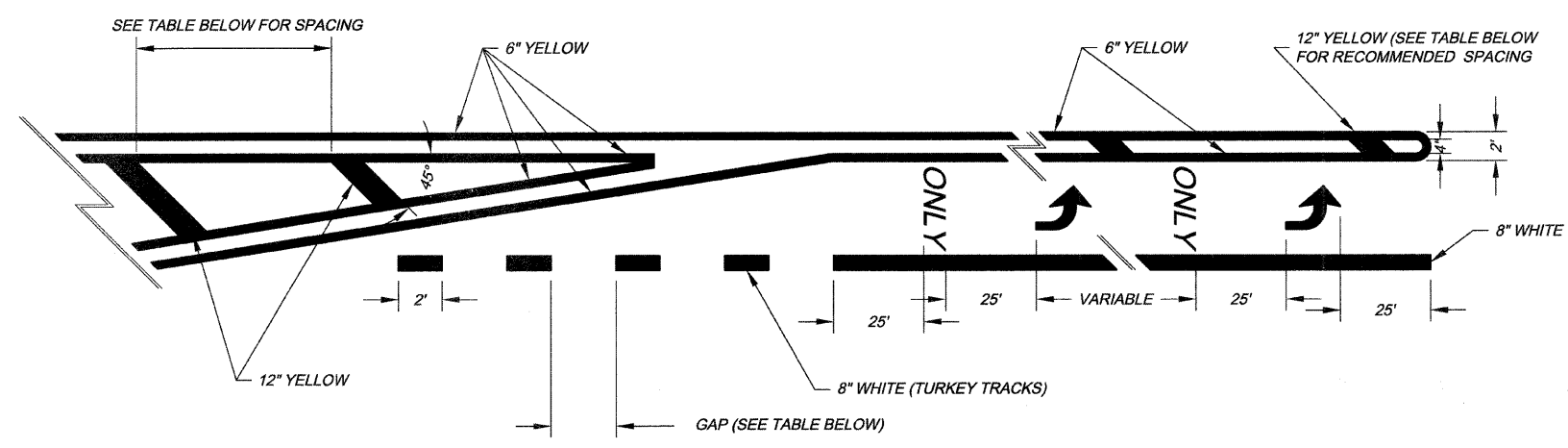
GENERAL NOTES

1. CALL J. U. I. L. E. AT (800) 892-0123 AT LEAST 48 HRS PRIOR TO BEGINNING INSTALLATION. ALL UTILITIES SHALL BE LOCATED IN THE FIELD AND THE ENGINEER SHALL BE NOTIFIED OF ANY CONFLICTS PRIOR TO BEGINNING CONSTRUCTION.
2. ALL SIGNING, STRIPING, TRAFFIC SIGNALS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.) CURRENT EDITION AND TO THE STANDARDS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.
3. SEE ADJACENT MUTCD DIAGRAM FOR INSTALLATION HEIGHTS AND OFFSETS.
4. SHOP DRAWINGS FOR THE SIGNS LABELED AS "DIAGRAMMATIC" SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL. FABRICATION SHALL NOT BEGIN UNTIL APPROVAL FROM THE ENGINEER HAS BEEN GIVEN. ALL DIAGRAMMATIC SIGNS SHALL HAVE A GREEN BACKGROUND WITH A WHITE MESSAGE AND WILL BE MOUNTED TO 4 X 6 WOOD POSTS AS SHOWN IN IDOT STANDARD 720021.
5. WHERE SIGNS ARE TO BE ERECTED IN THE CONCRETE MEDIAN SURFACE, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CASTING HOLES THROUGH THE CONCRETE MEDIAN SURFACE. THE HOLE SHALL BE FORMED USING A CONDUIT OF ADEQUATE DIAMETER FOR THE POST CALLED FOR AND CUT TO MATCH THE MEDIAN SURFACE THICKNESS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LAYING OUT THE NECESSARY HOLES.



TYPICAL DIAGRAMMATIC SIGN





TYPICAL MARKING FOR CORNER ISLAND OFFSET SHOULDER WIDTH

SKIP DASH SPACING (FEET):

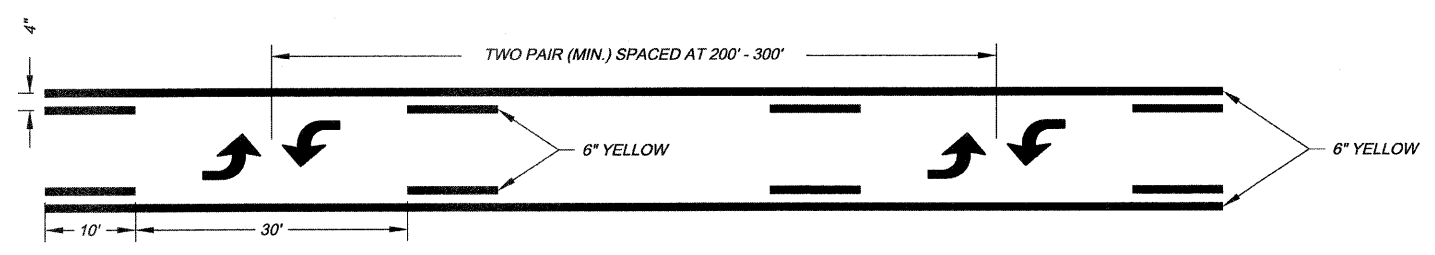
SPEED LIMIT RANGE	SPACING
LESS THAN 45 MPH	4 FT GAP
45 MPH TO 55 MPH	6 FT GAP
OVER 55 MPH	8 FT GAP

RECOMMENDED SPACING BETWEEN DIAGONALS (FEET):

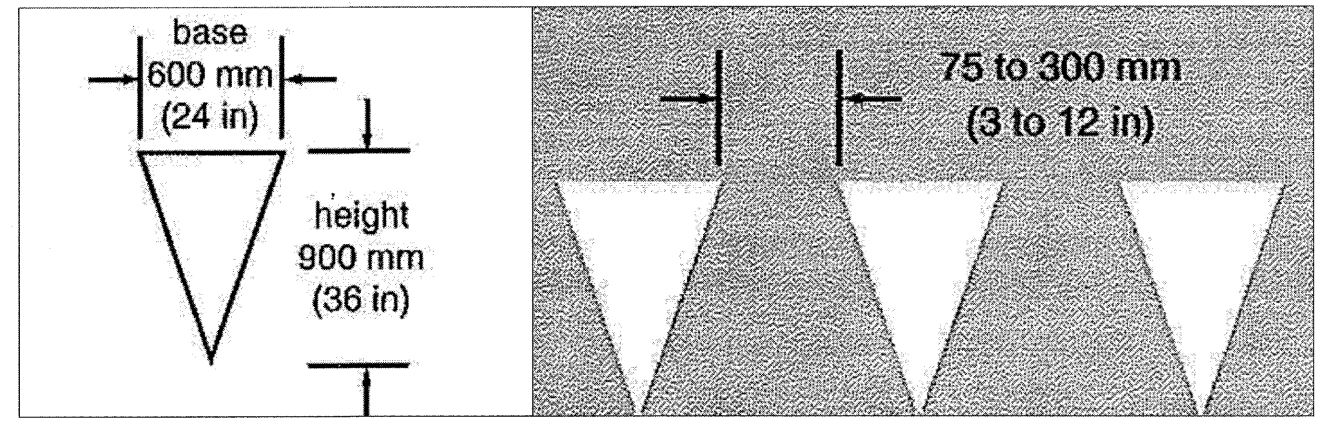
SPEED LIMIT RANGE	CONTINUOUS MEDIAN AREA	INTERSECTION CHANNELIZATION	OBJECTS (ISLANDS)
LESS THAN 30 MPH	50 FT	15 FT	10 FT
30 MPH TO 40 MPH	75 FT	20 FT	15 FT
45 MPH & OVER	75 FT	30 FT	20 FT

NOTE: IF THE SPACING RECOMMENDED IN THE TABLE DOES NOT PERMIT AT LEAST FIVE DIAGONAL LINES IN THE AREA BEING MARKED, THE SPACING FROM THE NEXT LOWEST SPEED RANGE SHOULD BE USED. THE RECOMMENDED SPACING IS MEASURED PARALLEL TO THE PAVEMENT CENTERLINE.

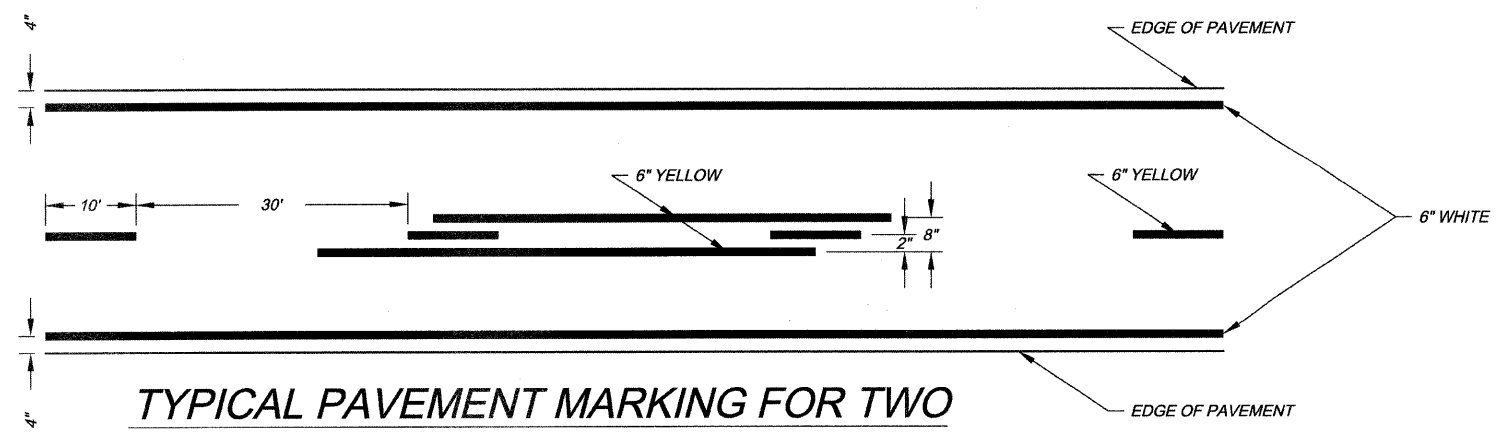
TYPICAL MARKING FOR FLUSH MEDIAN



MEDIAN PAVEMENT MARKING



TYPICAL YIELD LINE LAYOUTS

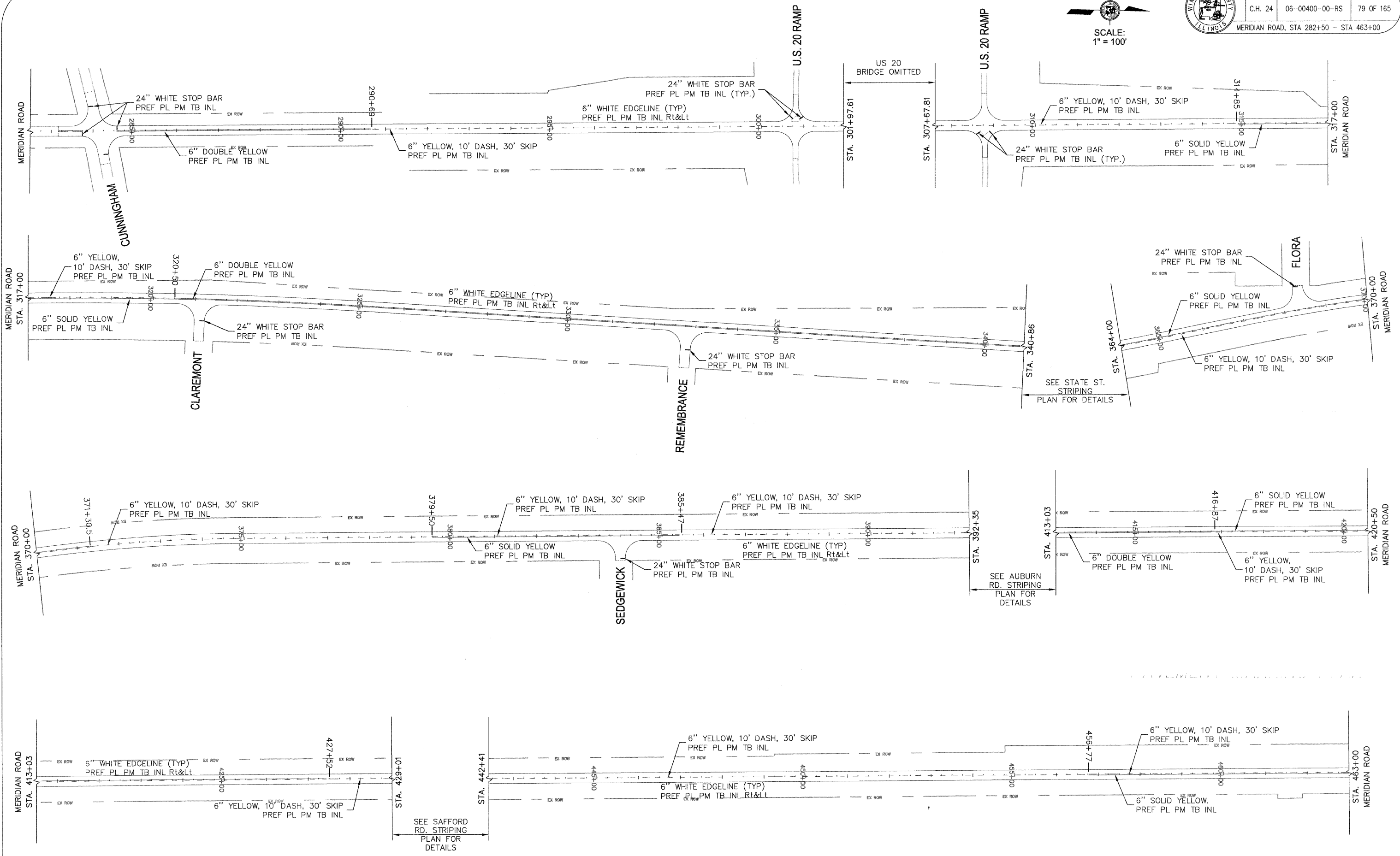
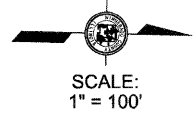


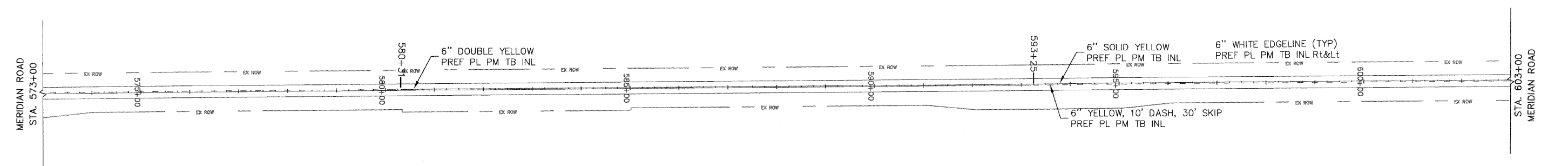
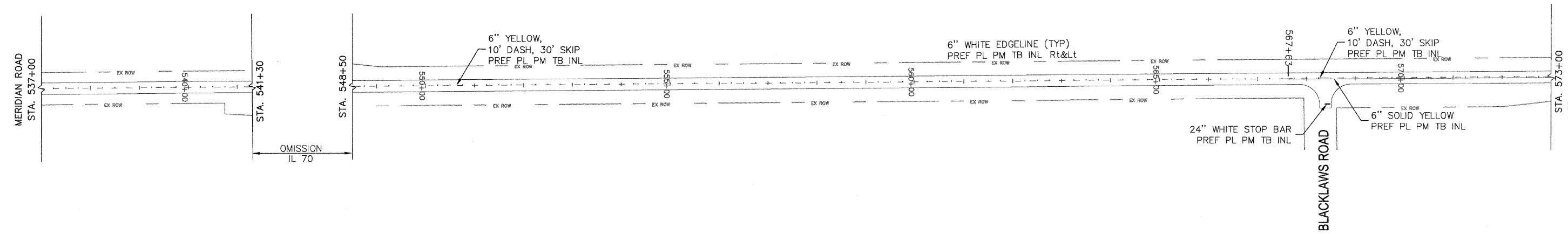
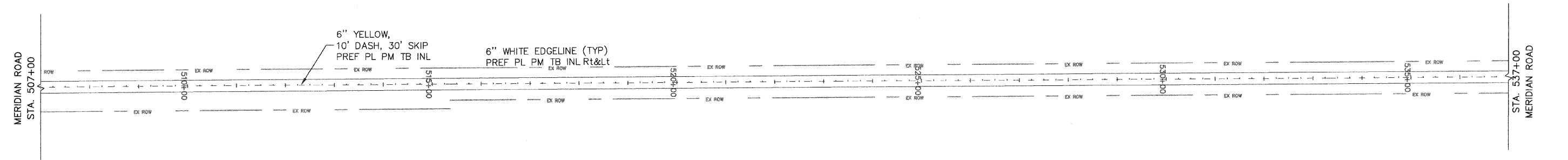
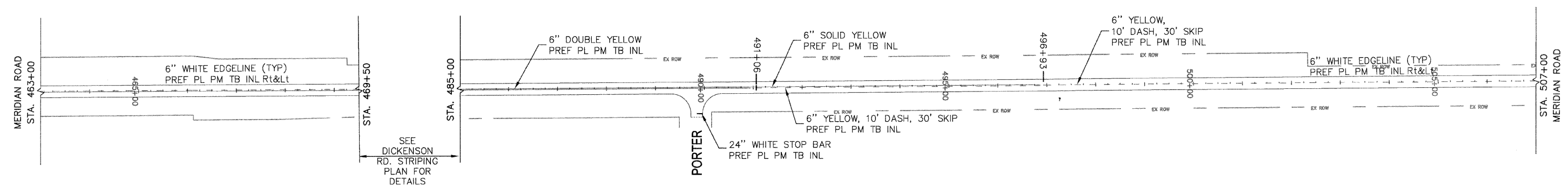
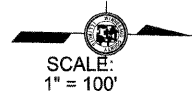
TYPICAL PAVEMENT MARKING FOR TWO LANE SECTION - NO PASSING ZONES

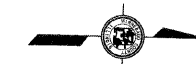
GENERAL NOTES

PAVEMENT MARKING

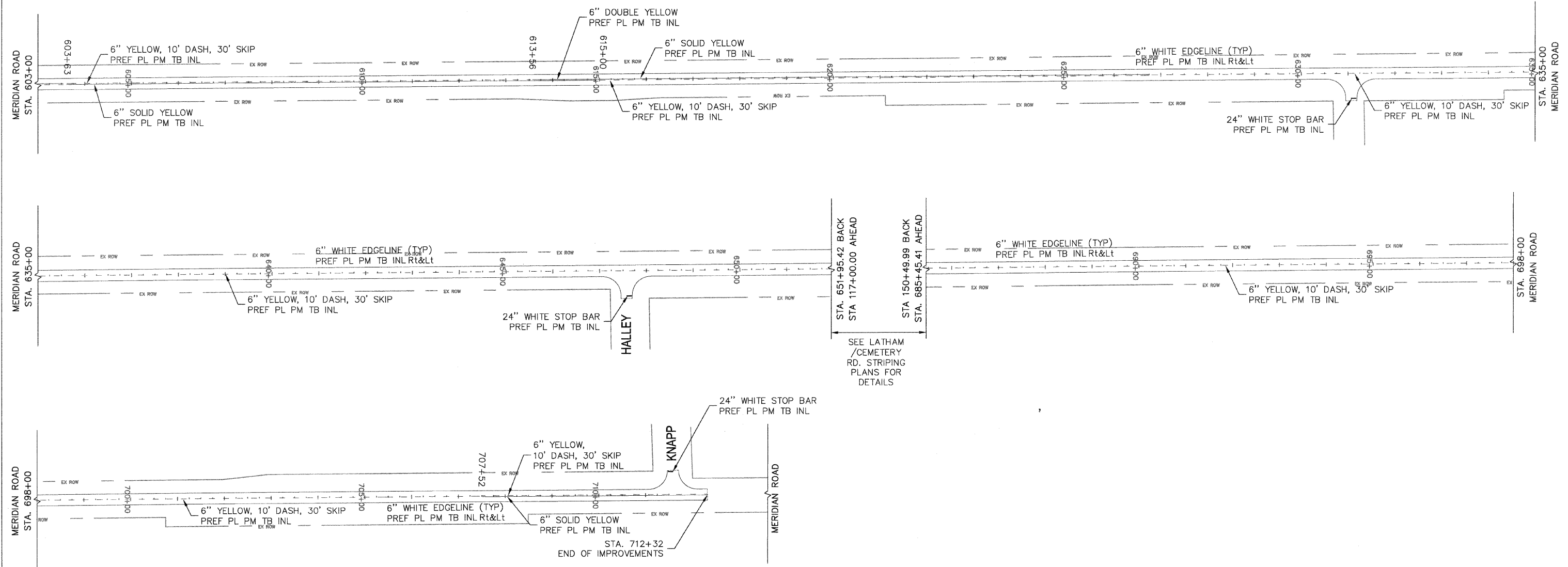
- ALL PAVEMENT MARKING SHALL BE DONE ACCORDING TO STANDARD 780001, EXCEPT AS FOLLOWS:
- 1) ALL WORDS, SUCH AS "ONLY" SHALL BE 8" HIGH.
 - 2) ALL ARROWS SHALL BE THE LARGE SIZE.
 - 3) THE DISTANCE BETWEEN YELLOW NO-PASSING LINES SHALL BE 8", NOT 7" AS SHOWN IN STANDARD 780001.

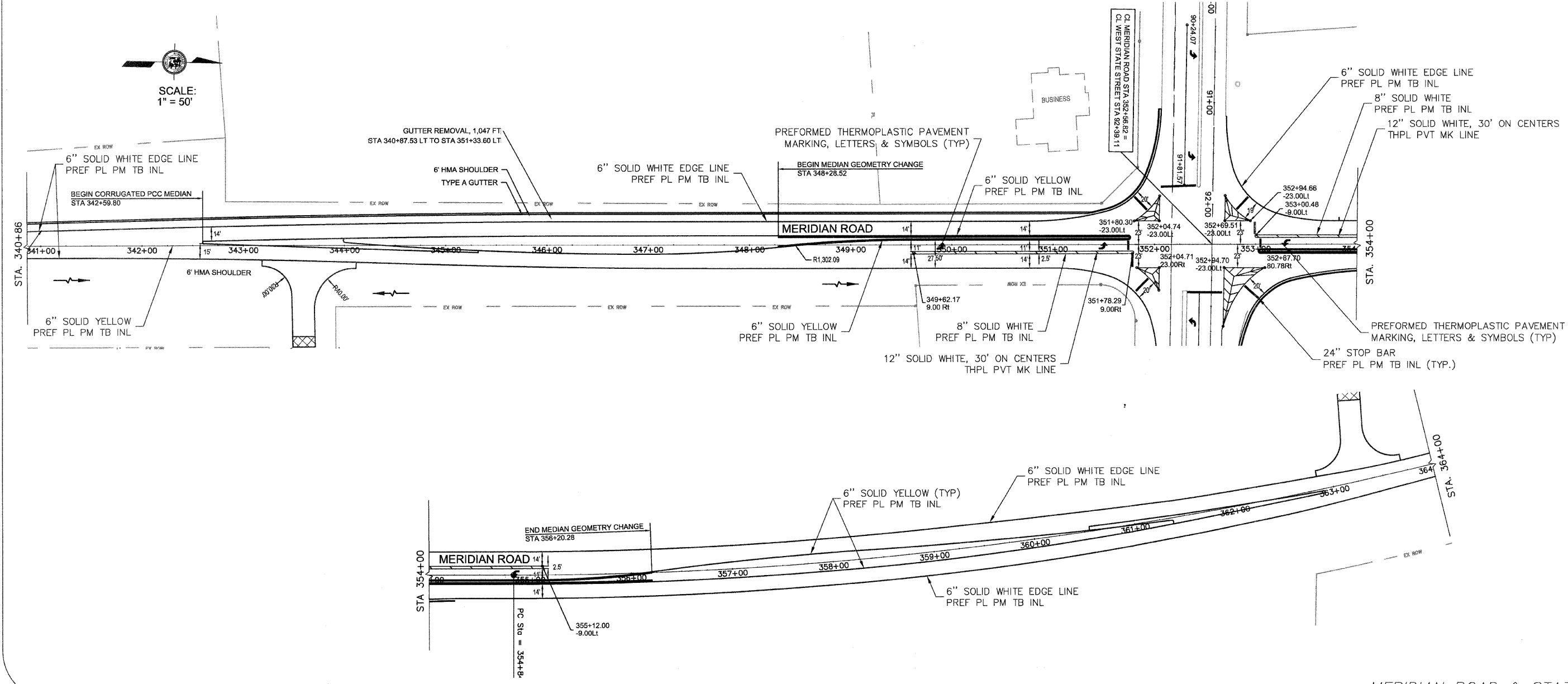
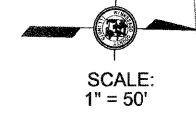
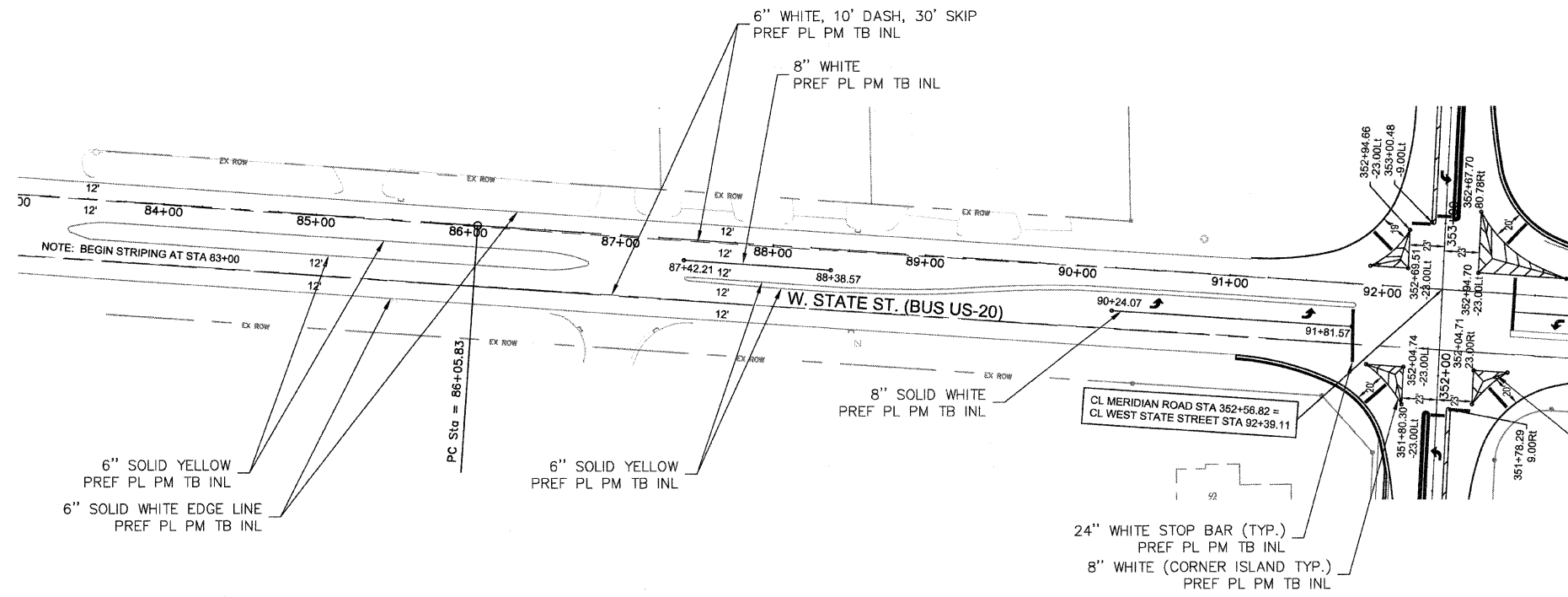




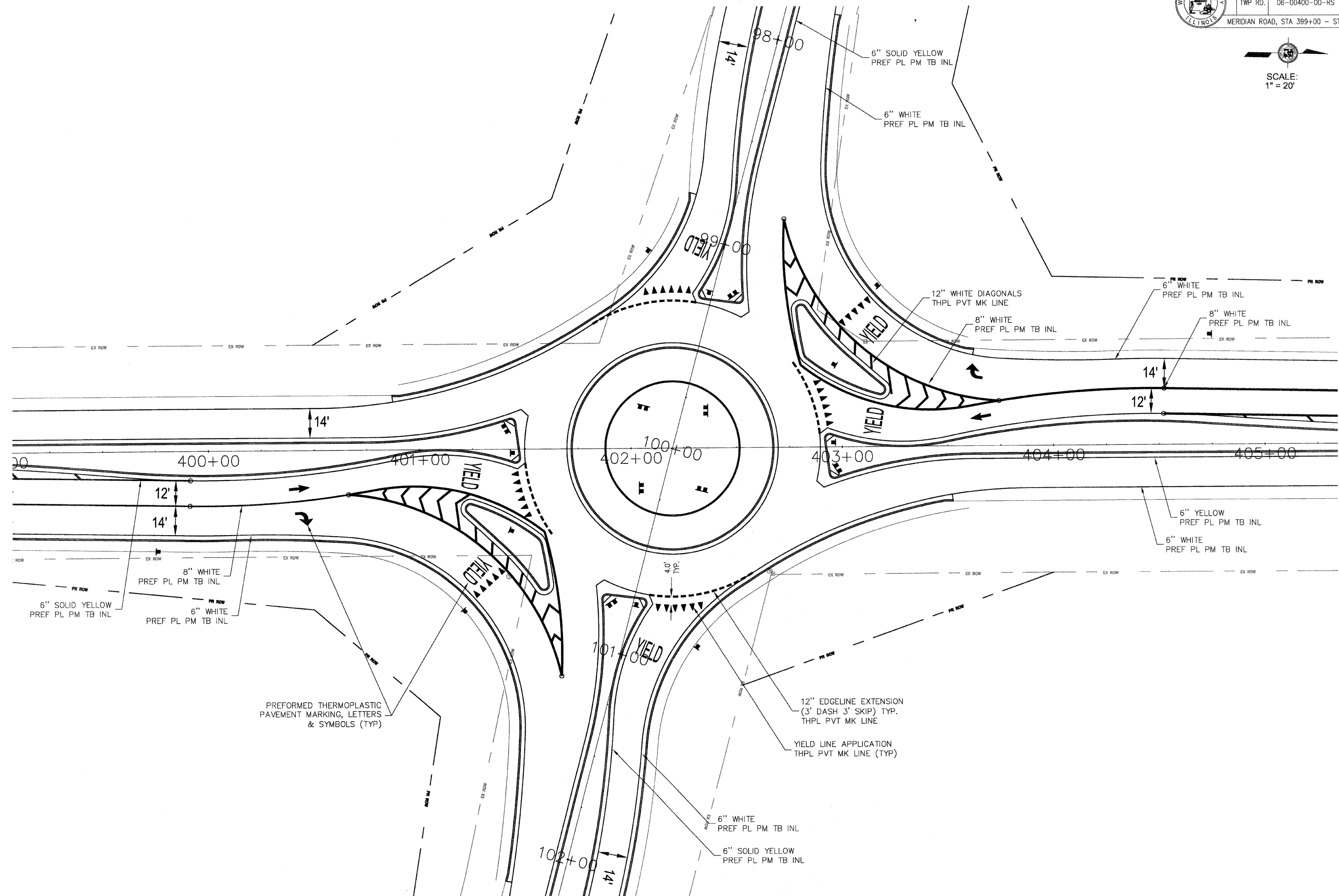
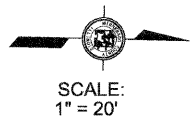


SCALE:
1" = 100'





MERIDIAN ROAD & STATE STREET
 PAVEMENT MARKING PLAN



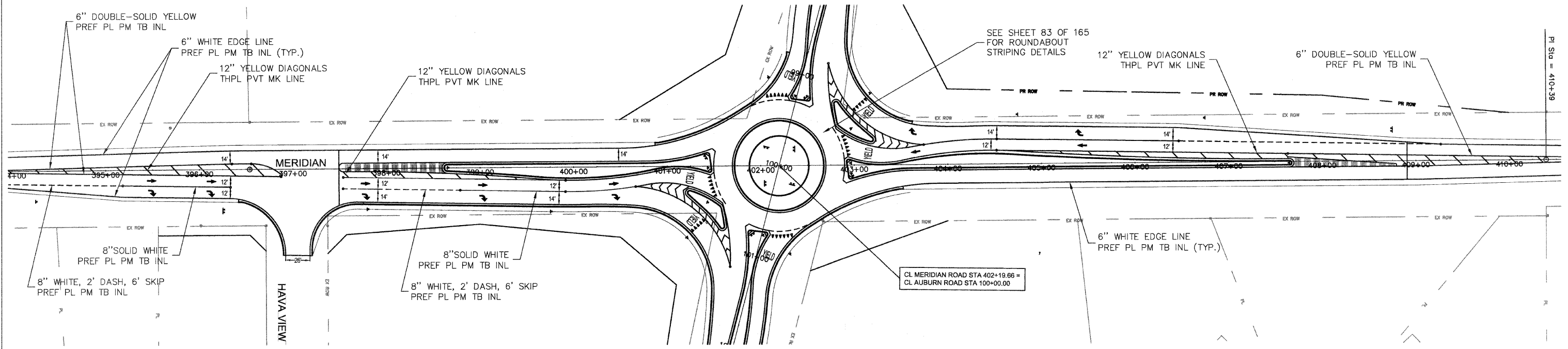
PREFORMED THERMOPLASTIC
PAVEMENT MARKING, LETTERS
& SYMBOLS (TYP)

12" EDGELINE EXTENSION
(3' DASH 3' SKIP) TYP.
THPL PVT MK LINE

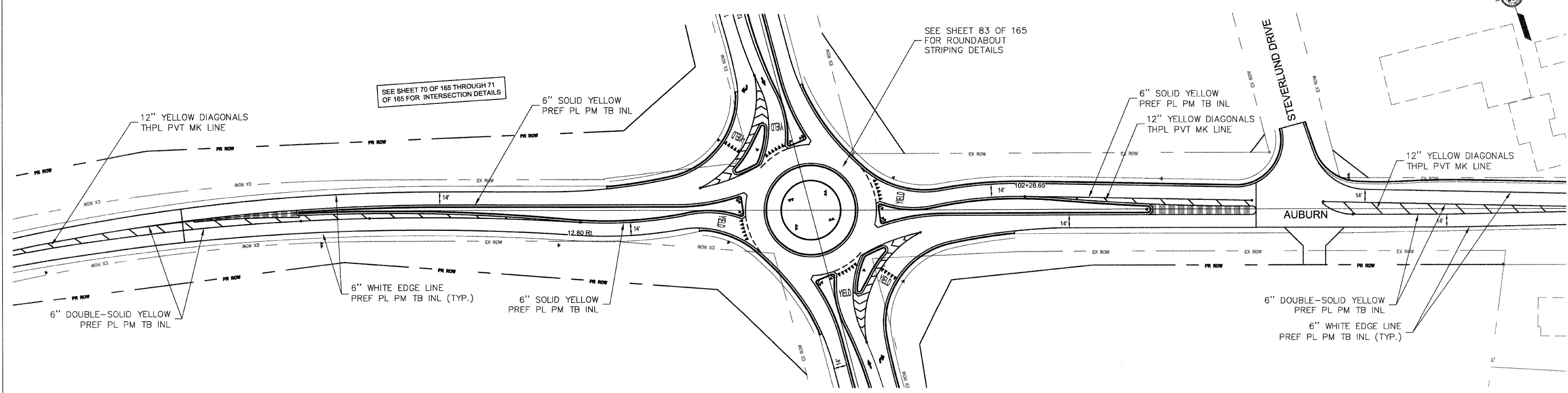
YIELD LINE APPLICATION
THPL PVT MK LINE (TYP)

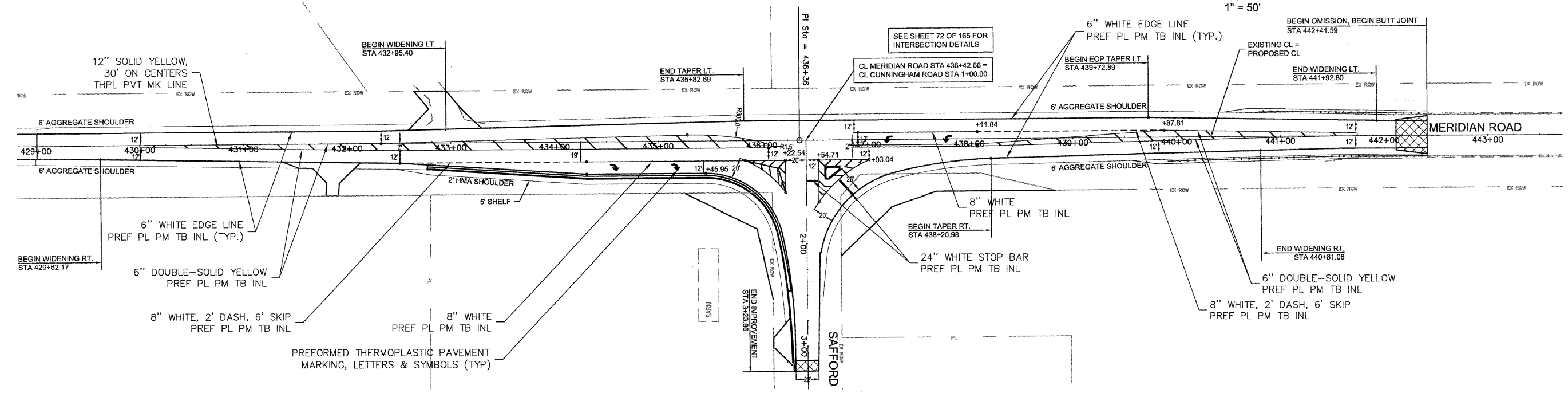
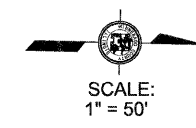
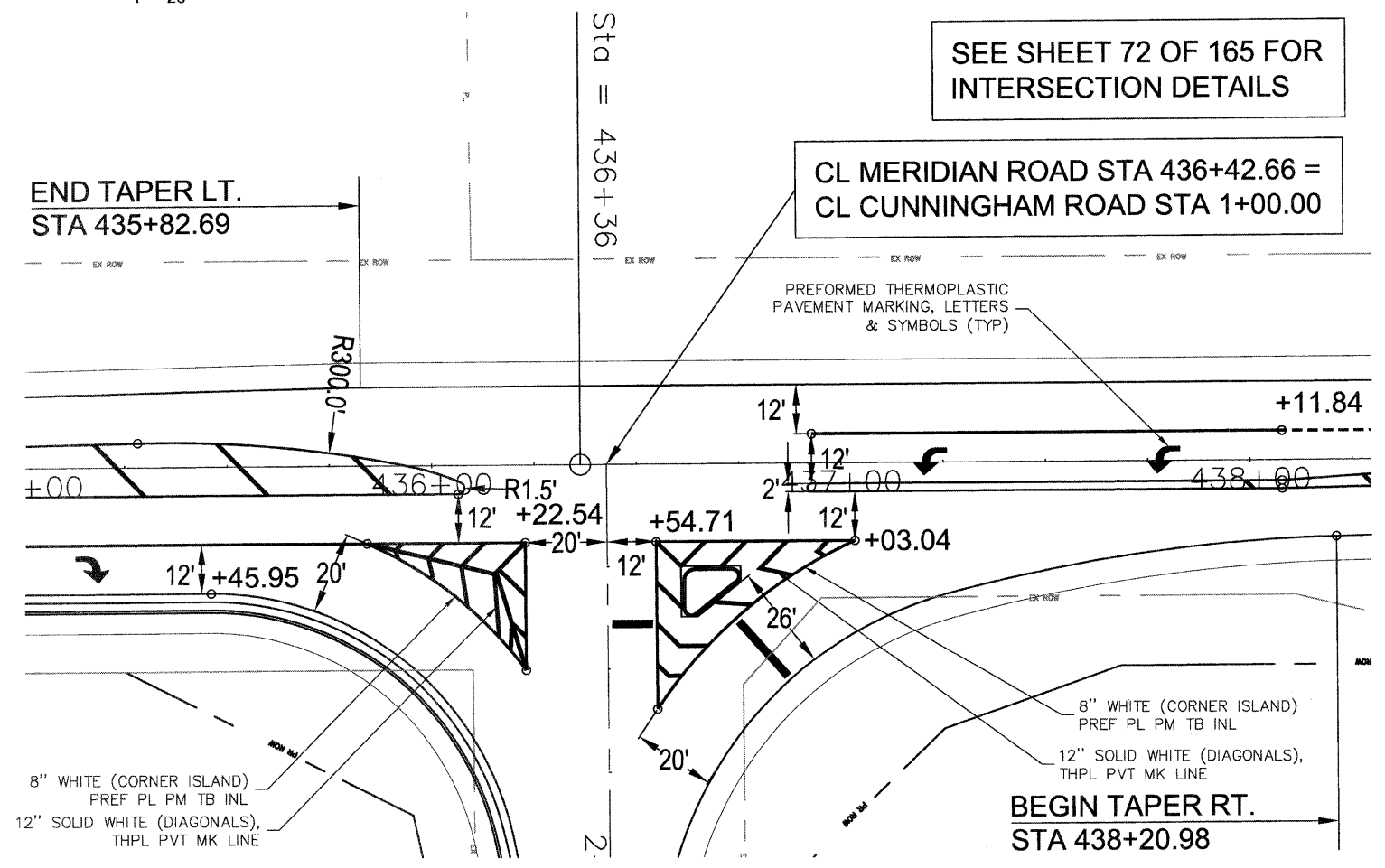
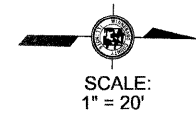


MERIDIAN RD.



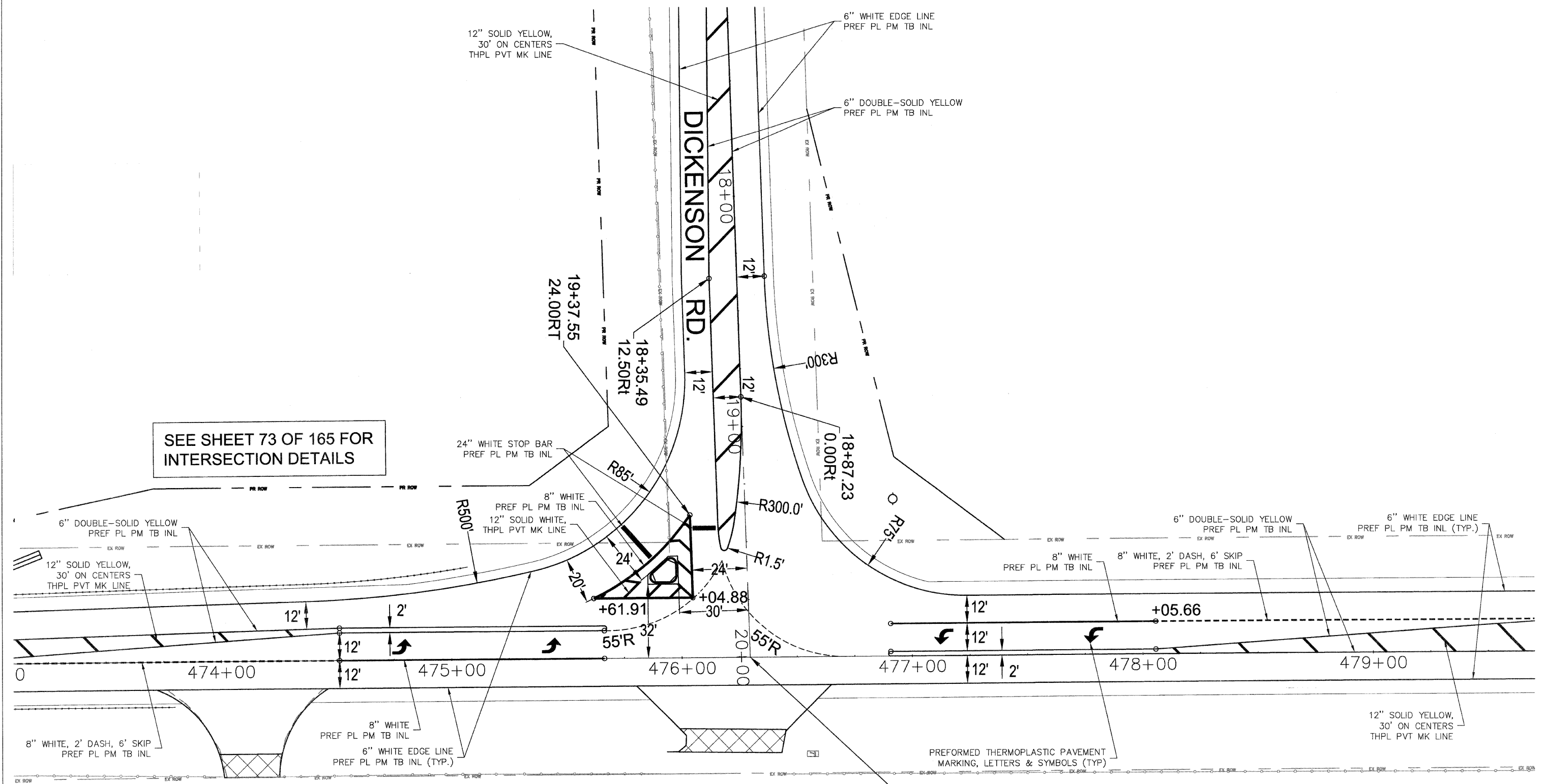
AUBURN RD.







SCALE:
1" = 20'

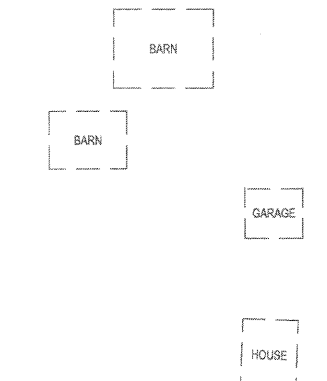
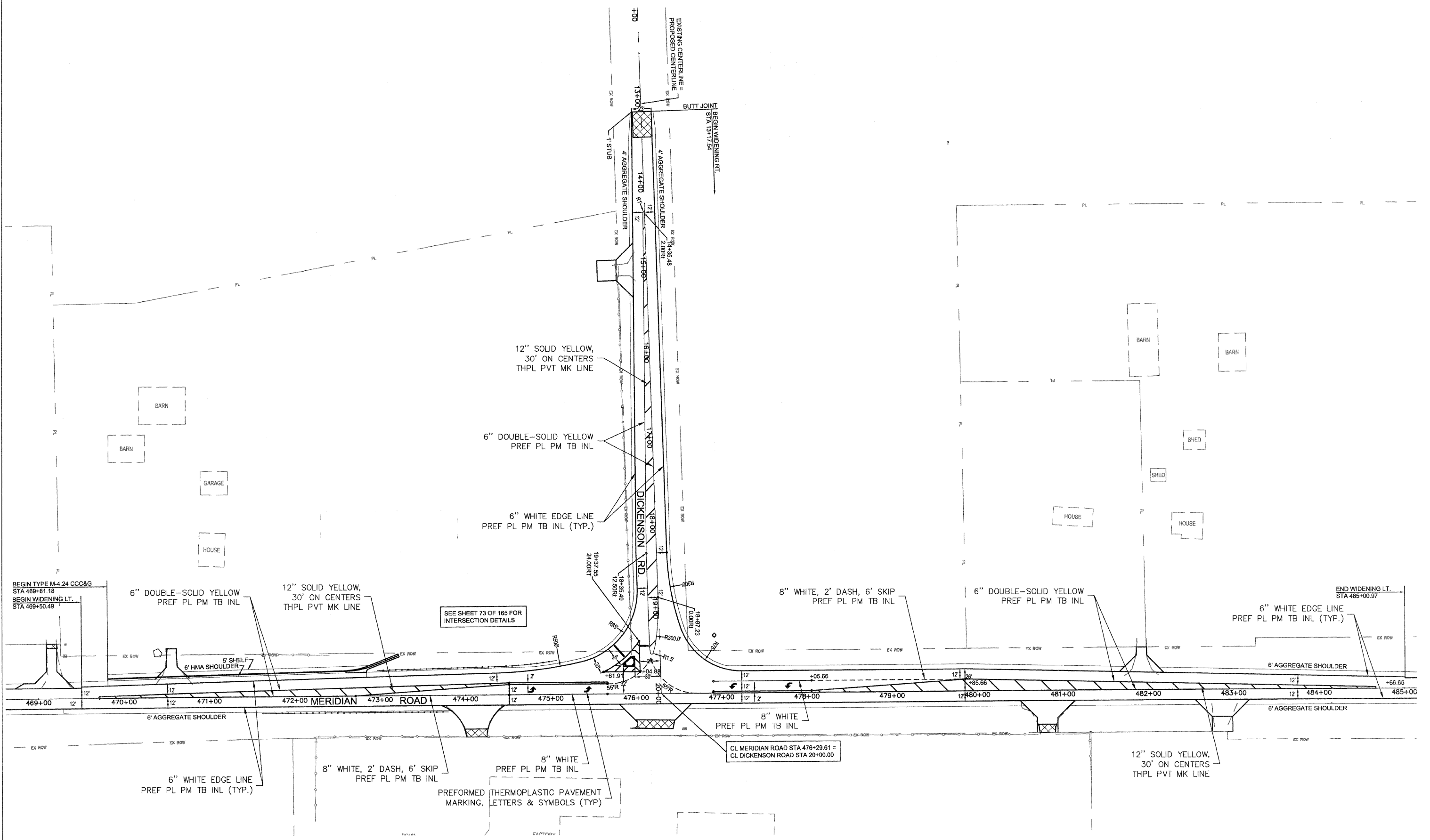


SEE SHEET 73 OF 165 FOR INTERSECTION DETAILS

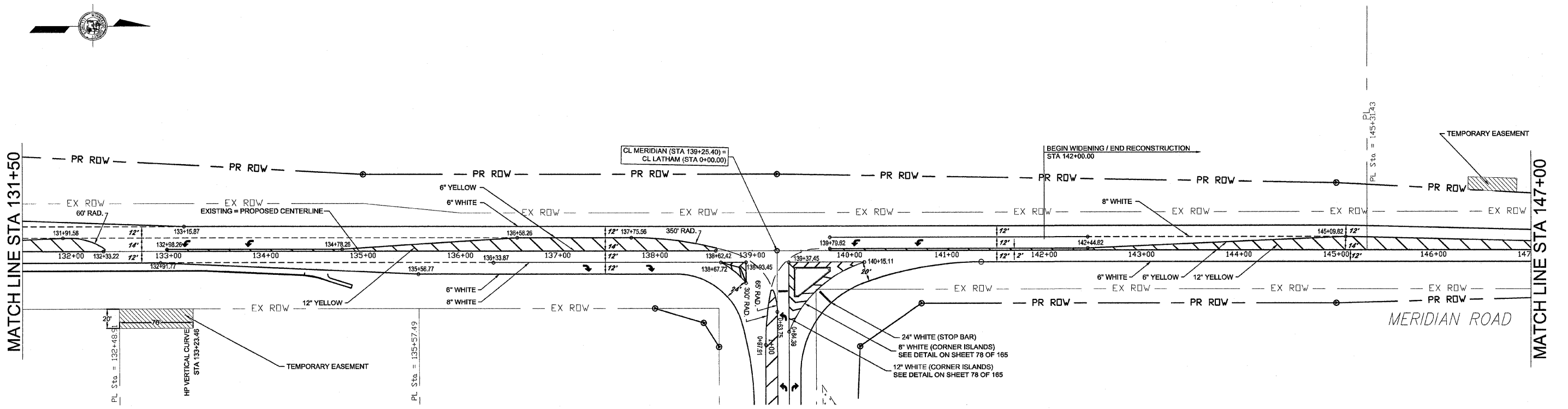
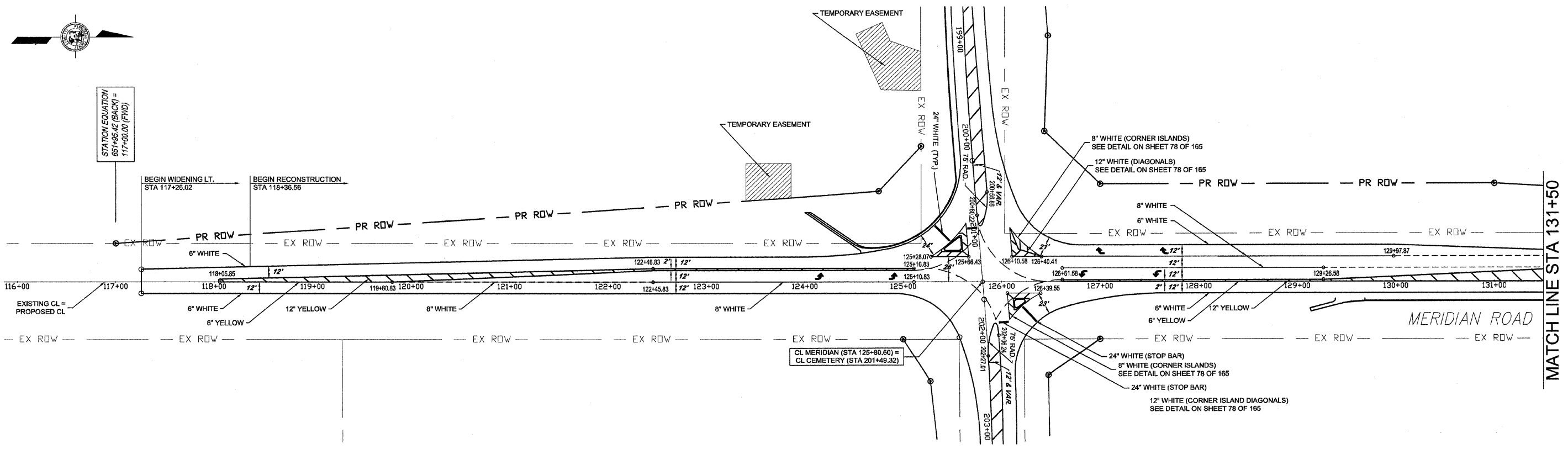
CL MERIDIAN ROAD STA 476+29.61 =
CL DICKENSON ROAD STA 20+00.00



SCALE:
1" = 50'

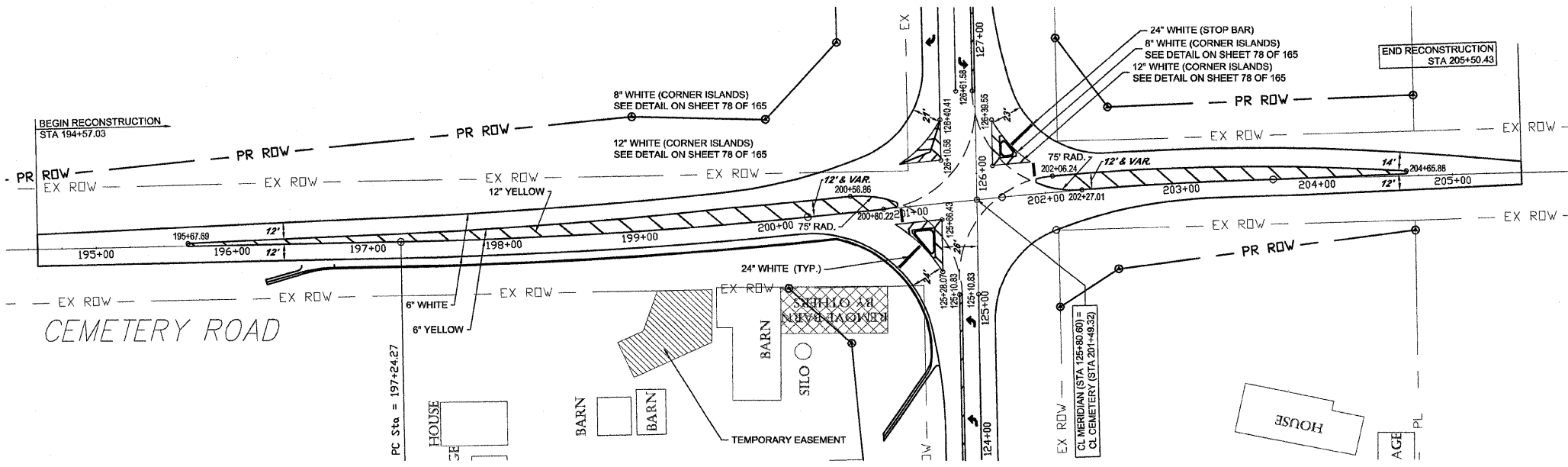
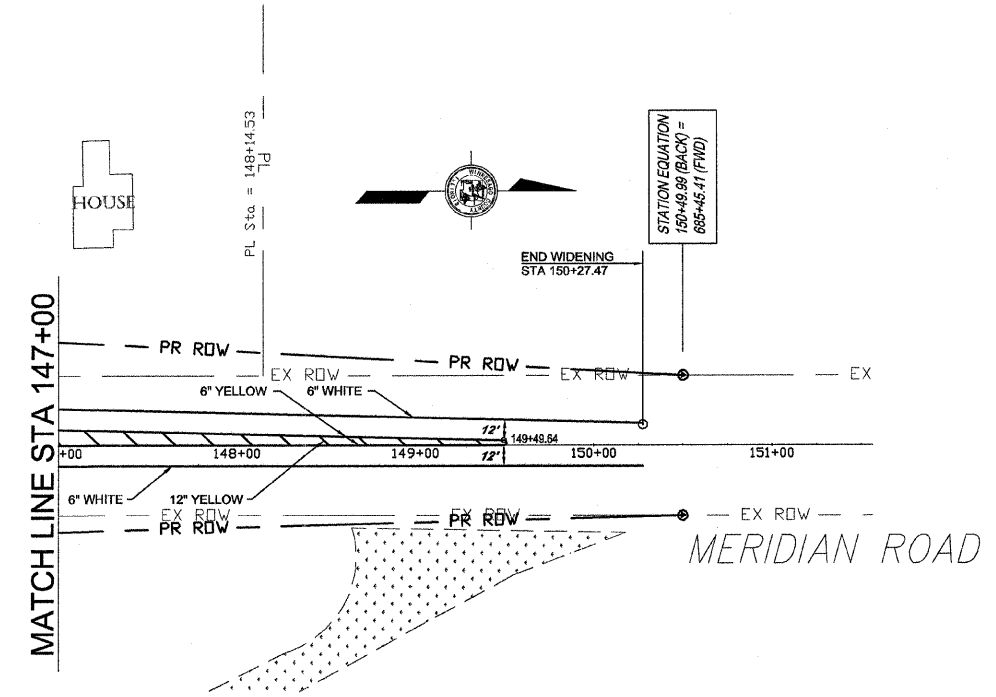
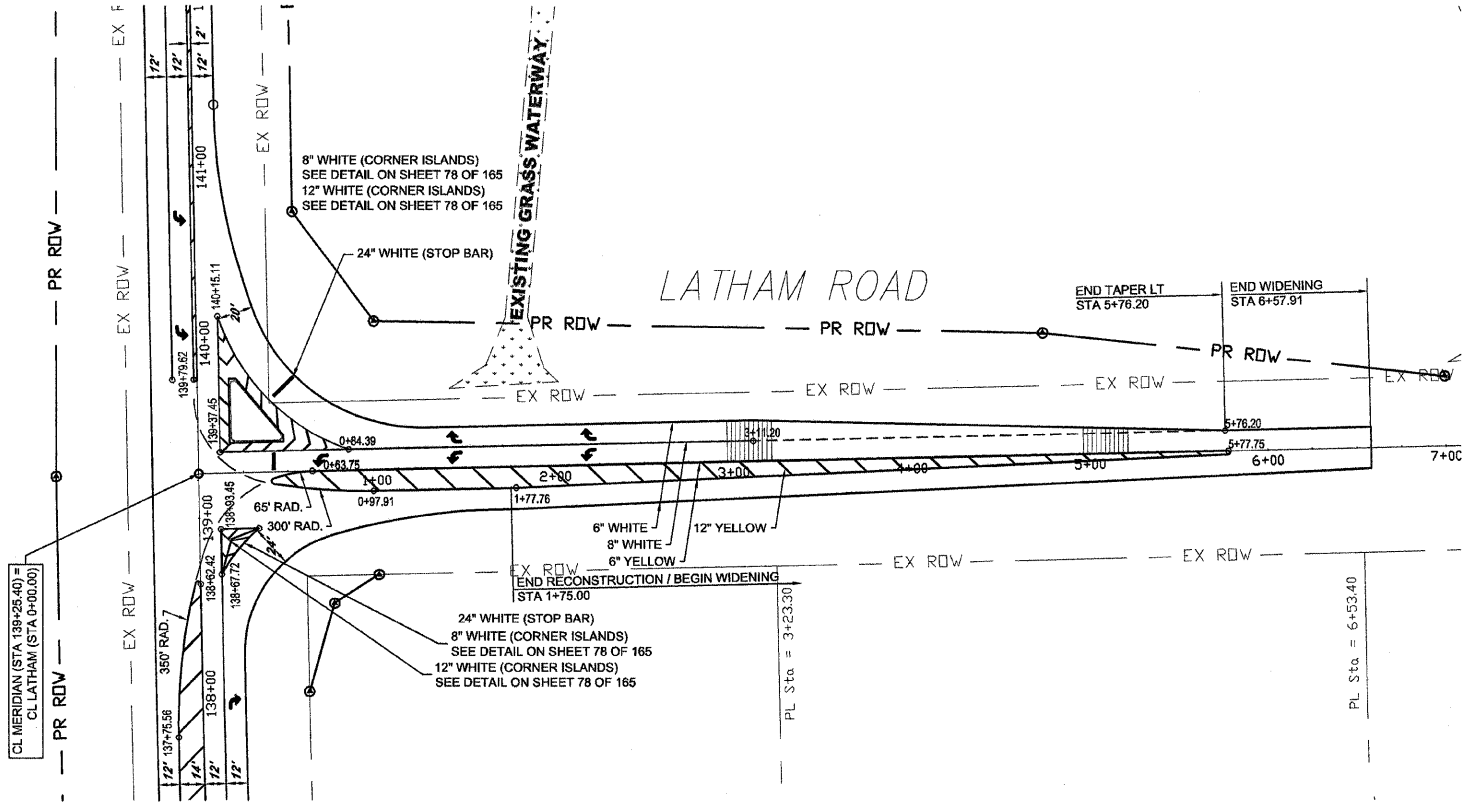


MERIDIAN ROAD & DICKENSON ROAD
PAVEMENT MARKING PLAN





LATHAM ROAD, CEMETERY ROAD & MERIDIAN ROAD





SUMMARY OF SIGNAL QUANTITIES

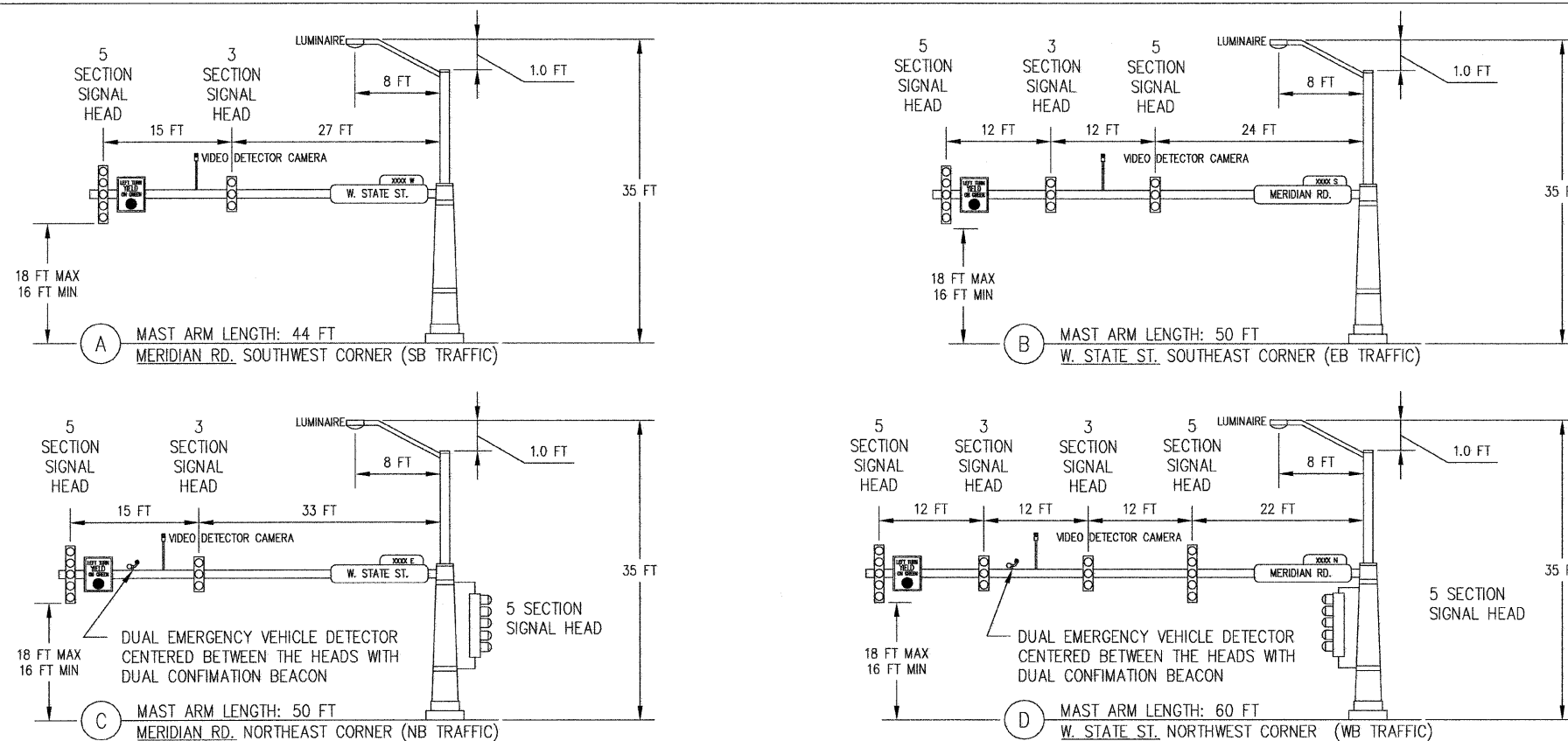
ITEM NO.	PAY CODE NUMBER	ITEMS	UNIT	QUANTITY
				Y031-1F
1	72000200	SIGN PANEL - TYPE 2	SF	40.00
2	80500200	SERVICE INSTALLATION, TYPE B	EA	1.00
3	81012300	CONDUIT IN TRENCH, 1" DIA., PVC	FT	10.00
4	81012600	CONDUIT IN TRENCH, 2" DIA., PVC	FT	59.00
5	81012700	CONDUIT IN TRENCH, 2 1/2" DIA., PVC	FT	52.00
6	81012800	CONDUIT IN TRENCH, 3" DIA., PVC	FT	5.00
7	81013000	CONDUIT IN TRENCH, 4" DIA., PVC	FT	65.00
8	81021370	CONDUIT PUSHED, 4" DIA., PVC	FT	335.00
9	81400700	HANDHOLE, PORTLAND CEMENT CONCRETE	EA	5.00
10	81400720	DOUBLE HANDHOLE, PORTLAND CEMENT CONCRETE	EA	1.00
11	81702110	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 10	FT	1,786.00
12	82103400	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, PHOTO-CELL CONTROL, 400 WATT	EA	4.00
13	85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EA	1.00
14	86200200	UNINTERRUPTIBLE POWER SUPPLY, STANDARD	EA	1.00
15	87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FT	1,407.00
16	87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FT	2,095.00
17	87301815	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 3 C	FT	15.00
18	87502480	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EA	3.00
19	87702950	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 44 FT.	EA	1.00
20	87702980	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 50 FT.	EA	2.00
21	87703030	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 60 FT.	EA	1.00
22	87800100	CONCRETE FOUNDATION, TYPE A	FT	3.00
23	87800150	CONCRETE FOUNDATION, TYPE C	FT	3.00
24	87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FT	43.00
25	87800420	CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER	FT	21.00
26	88040090	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3 SECTION, MAST ARM MOUNTED	EA	6.00
27	88040150	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5 SECTION, BRACKET MOUNTED	EA	5.00
28	88040160	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5 SECTION, MAST ARM MOUNTED	EA	5.00
29	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EA	1.00
30	89502380	REMOVE EXISTING HANDHOLE	EA	6.00
31	89502385	REMOVE EXISTING CONCRETE FOUNDATION	EA	8.00
32	XX003163	EMERGENCY VEHICLE PRIORITY SYSTEM	EA	1.00
33	X0323481	VIDEO VEHICLE DETECTION, 4 CAMERAS	EA	1.00
34	Z0033084	ELECTRIC CABLE IN CONDUIT, GROUND, NO. 6 1C (GREEN)	FT	536.00

GENERAL NOTES

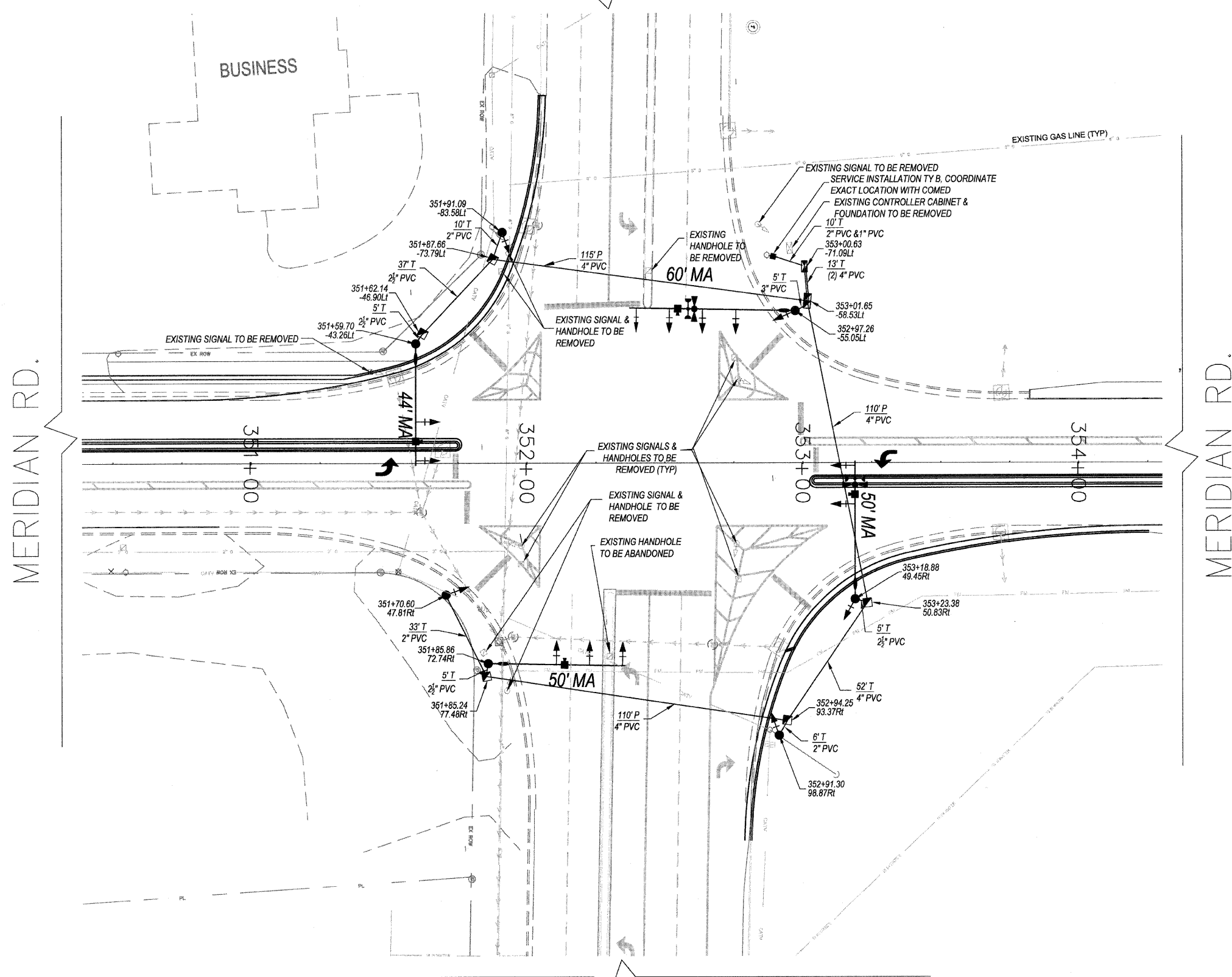
- CALL J. U. I. L. E. AT (800) 892-0123 AT LEAST 48 HRS PRIOR TO BEGINNING CONSTRUCTION. ALL UTILITIES SHALL BE LOCATED IN THE FIELD AND THE ENGINEER SHALL BE NOTIFIED OF ANY CONFLICTS PRIOR TO BEGINNING CONSTRUCTION.
- ALL SIGNING, STRIPING, TRAFFIC SIGNALS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.) CURRENT EDITION AND TO THE STANDARDS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.
- MAST ARM ASSEMBLIES AND TRAFFIC SIGNAL POSTS SHALL BE GALVANIZED STEEL. SIGNAL HEADS SHALL BE POLYCARBONATE WITH 12" LENSES.
- THE EXISTING AND PROPOSED CONTROLLER CABINET IS LOCATED IN THE NORTHWEST QUADRANT OF THE INTERSECTION. THE CONTRACTOR SHALL COORDINATE WITH COMED ALL WORK ASSOCIATED WITH SERVICE INSTALLATION, TYPE B. ALL COSTS ASSOCIATED WITH ENERGIZING THE NEW CONTROLLER, INCLUDING ANY COMED FEES FOR TRANSFORMER UPGRADES / CHANGES SHALL BE CONSIDERED INCIDENTAL TO THE SERVICE INSTALLATION, TYPE B.
- THE CONTRACTOR SHALL USE EAGLE BRAND EQUIPMENT OR EQUIPMENT COMPATIBLE WITH EAGLE BRAND.
- THE EMERGENCY VEHICLE PRIORITY SYSTEM (EVPS) SHALL BE THE "TOMAR/OPTRONX OPTICAL PRE-EMPTION SYSTEM". THIS TRAFFIC SIGNAL INSTALLATION SHALL INCLUDE DUAL EMERGENCY VEHICLE DETECTORS WITH DUAL CONFIRMATION BEACONS FOR EACH ROADWAY. CONTACT BROWN TRAFFIC PRODUCTS AT 1-800-888-7078 FOR FURTHER INFORMATION IN REGARDS TO THE MANUFACTURER'S INSTALLATION REQUIREMENTS. THE CONTRACTOR SHALL CONFIRM WITH THE JURISDICTIONAL EMERGENCY AGENCY (WEST SUBURBAN FIRE PROTECTION FIRE DISTRICT (815) 964-3441) THAT THE SYSTEM IS COMPATIBLE WITH EXISTING FIRE DISTRICT EMERGENCY EQUIPMENT PRIOR TO INSTALLATION. SEE SPECIAL PROVISIONS FOR ADDITIONAL REQUIREMENTS FOR EVPS.
- THE FOLLOWING VIDEO DETECTION SYSTEMS ARE APPROVED FOR USE IN DISTRICT 2: 1) ITERIS VANTAGE EDGE 2 (4 CAMERA SYSTEM), AND 2) AUTOSCOPE SOLO PRO (4 CAMERA SYSTEM). SEE SPECIAL PROVISIONS FOR ADDITIONAL VIDEO DETECTION SYSTEM REQUIREMENTS. CONTACT BROWN TRAFFIC PRODUCTS AT -800-888-7078 FOR FURTHER INFORMATION REGARDING THE MANUFACTURER'S INSTALLATION REQUIREMENTS.
- PROVIDE TWO CONDUITS FROM THE CONTROLLER TO THE ADJACENT SERVICE POLE.. ONE 2" CONDUIT SHALL BE USED FOR POWER AND ONE 1" CONDUIT SHALL BE RESERVED FOR THE TELEPHONE SERVICE.
- SIGN PANELS FOR STREET NAMES SHALL BE 2.5' X 8' AND SHALL BE MOUNTED AS SHOWN ON STANDARD 877001-04.
- FOUNDATIONS SHALL BE CONSTRUCTED IN ACCORDANCE WITH STANDARD 878001-08.
- SALVAGE EXISTING STREET NAME SIGNS FROM EXISTING MAST ARMS AND RE-INSTALL ON NEW MAST ARMS. THE COST OF THIS WORK SHALL BE CONSIDERED INCIDENTAL TO REMOVING EXISTING TRAFFIC SIGNAL EQUIPMENT. IF THE EXISTING SIGNS BECOME DAMAGED, THE CONTRACTOR SHALL REPLACE THEM AT NO ADDITIONAL COST TO THE CONTRACT.
- THE CONTRACTOR SHALL NOT BEGIN THE REMOVAL OF THE CORNER ISLANDS AND MEDIANS UNTIL ALL NEW TRAFFIC SIGNAL EQUIPMENT AND COMPONENTS ARE DELIVERED AND ON SITE.

MAST ARM EQUIPMENT MOUNTING DETAIL

NO SCALE



WEST STATE ST.



NOTE:
 THE CONTRACTOR SHALL CONDUCT THIS WORK IN SUCH A WAY THAT THE EXISTING SIGNALS REMAIN IN OPERATION UNTIL SUCH TIME AS THE PROPOSED SIGNALS CAN BE TURNED ON AND BECOME OPERATIONAL. THE CONTRACTOR SHALL COORDINATE THE TURN ON OF THE PROPOSED SIGNALS WITH THE ENGINEER.

THE CONTRACTOR SHALL NOT BEGIN THE REMOVAL OF THE CORNER ISLANDS AND MEDIANS UNTIL ALL NEW TRAFFIC SIGNAL EQUIPMENT AND COMPONENTS ARE DELIVERED AND ON SITE.

TRAFFIC SIGNAL LEGEND

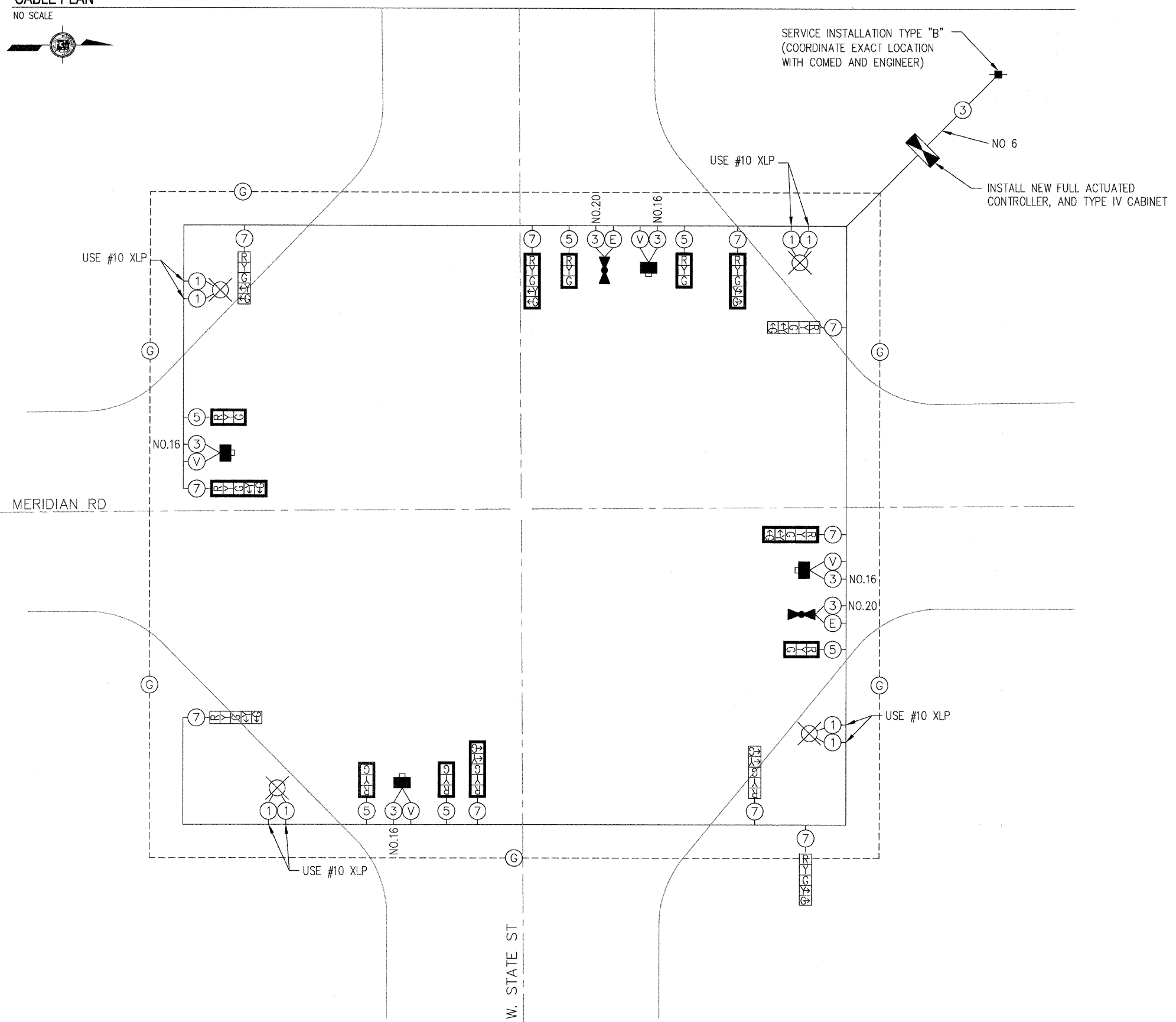
EXISTING	PROPOSED

SCALES:
 1" = 20' HOR

WEST STATE ST.



CABLE PLAN
NO SCALE

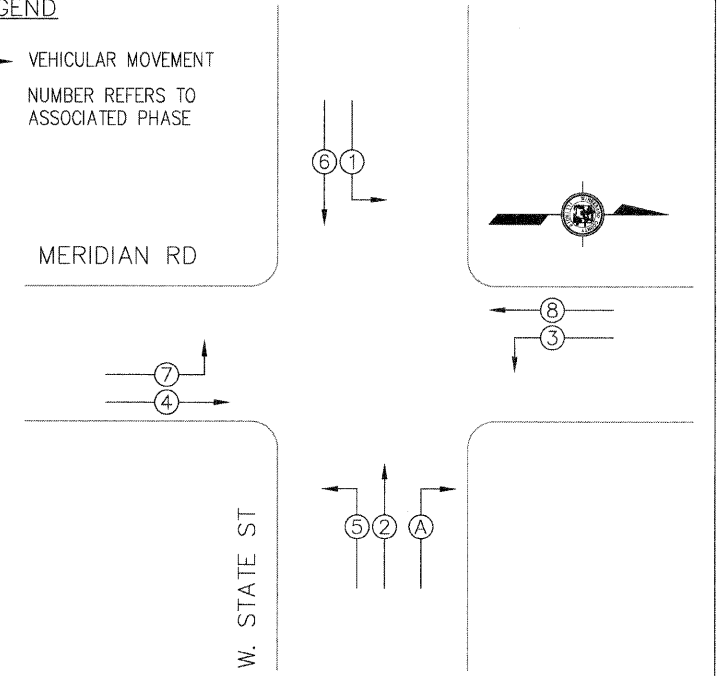


PHASE DESIGNATION DIAGRAM
NO SCALE

REFER TO I.D.O.T. STANDARD 857001-01

LEGEND

- (*)— VEHICULAR MOVEMENT
- * NUMBER REFERS TO ASSOCIATED PHASE



CABLE PLAN LEGEND

- SERVICE INSTALLATION
- ⑤ CABLE WITH NO. OF CONDUCTORS
- ⌈ R Y G ⌋ SIGNAL HEAD WITH BACKPLATE (12" LENSES)
- ⌈ R Y G ⌋ SIGNAL HEAD WITHOUT BACKPLATE (12" LENSES)
- ⊗ HIGH PRESSURE SODIUM LUMINAIRE (400 WATTS)
- ⓔ EMERGENCY VEHICLE PRIORITY SYTEM LEAD-IN CABLE (3/C NO. 20)
- ⊕ EMERGENCY VEHICLE DOUBLE OPTICAL DETECTOR WITH DOUBLE CONFIRMATION BEACON
- ⊞ CONTROLLER CABINET
- VIDEO DETECTOR CAMERA
- Ⓥ VIDEO DETECTOR CABLE (SEE SPECIAL PROVISIONS)
- ⓐ GROUNDING CIRCUIT AS PER STD 873001-02 (1/C NO. 6)



CONTRACT: 85513

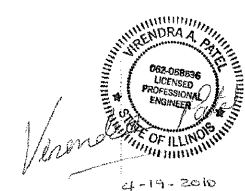
HIGHWAY LIGHTING - SUMMARY OF QUANTITY

PAY ITEM	DESCRIPTION	UNIT	QUANTITY			
			AUBURN	LATHAM	CEMETARY	TOTAL
80400100	ELECTRIC SERVICE INSTALLATION	EACH	1	1	1	3
81000800	CONDUIT IN TRENCH, 3" DIA, GALVANIZED STEEL	FOOT	295	188	167	650
81603010	UNIT DUCT, 600V, 2-1C # 10, 1-C #10 GROUND, XLP-TYPE USE, 3/4" DIA POLYETHYLENE	FOOT	3500	401	631	4532
81900200	TRENCH & BACKFILL FOR ELECTRICAL WORK	FOOT	2140	123	297	2560
82102250	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 250 WATTS	EACH	16	2	2	20
82500350	LIGHTING CONTROLLER BASE MOUNTED 240 VOLTS, 100 AMPS	EACH	1	1	1	3
83002200	LIGHT POLE, ALUMINIUM, 40 FT MH, 6 FT DAVIT ARM	EACH	10	-	1	11
83600300	LIGHT POLE, FOUNDATION, 30" DIAMETER	FOOT	96	12	12	120
83002600	LIGHT POLE, ALUMINIUM, 40 FT MH, 15 FT DAVIT ARM	EACH	6	2	1	9
83800505	BREAKAWAY DEVICE, COUPLING WITH ALUMINIUM SKIRT	EACH	64	8	8	80

ILLINOIS DEPARTMENT OF TRANSPORTATION LUMINAIRE PERFORMANCE TABLE		
GIVEN CONDITIONS		
ROADWAY DATA:	Pavement Width	60 FT
	Number of Lanes	2
	Median Width	20 FT
	IES Surface Classification	R3
	Q-Zero Value	0.07
LIGHT POLE DATA:	Mounting Height	40 FT
	Mast Arm Length	6 FT & 15 FT
	Pole Set-Back From Edge of Pavement	14 FT
	Pole Set-Back From Back of Curb	5 FT
LUMINAIRE DATA:	Lamp Type	HPS
	Lamp Lumens	28000
	IES Verticle Distribution	M
	IES Control of Distribution	E
	IES Lateral Distribution	3
	Total Light Loss Factor	0.85
LAYOUT DATA:	Spacing	260 FT
	Configuration	STG
	Luminaire Overhang Over Edge of Pavement Lane	0 FT
NOTE: Variations from the above specified IES distribution pattern may be requested and acceptance of variations will be subject to review by the Engineer based on how well the performance requirements are met.		
PERFORMANCE REQUIRMENTS		
NOTE: These performance requirements shall be the minimum acceptable standards of photometric performance for the luminaire, based on the given conditions listed above.		
ILLUMINATION:	Average Horizontal Illumination, (Eavg)	6.0 Lux
	Uniformity Ratio, (Eave/Emin)	4.0
LUMINANCE:	Average Luminance: (Lave)	0.4 Cd/m2
	Uniformity Ratios: (Lave/Lmin)	4.0
	(Lmax/Lmin)	8.0
	Maximum Veiling Luminance Ratio: (Lv/Lave)	0.4

GENERAL NOTE:

- ALL LIGHTING POLES C1 THROUGH C10 ARE LOCATED 6' BEHIND THE CURB. ALL LIGHTING POLES S1 THROUGH S6 ARE LOCATED 15' FROM THE EDGE OF PAVEMENT
- ALL POLES HAVE A 40' MOUNTING HEIGHT
- USE IDOT STANDARD # 836001 FOR THE LIGHT POLE FOUNDATION DETAIL
- USE IDOT DISTRICT-1 STANDARD - BE 411 FOR THE DAVIT LIGHT POLE DETAIL
- USE IDOT STANDARD # 825021 FOR THE INSTALLATION OF LIGHTING CONTROLLER
- ELECTRIC CABLE IN CONDUIT, SERVICE, #6 3/C AND CONDUIT IN TRENCH BETWEEN THE SERVICE POLE AND LIGHTING CONTROLLER SHALL NOT BE PAID SEPARATELY AND SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF ELECTRIC SERVICE INSTALLATION. CHANGE IN LOCATION OF LIGHTING CONTROLLER, IS PERMITTED ONLY WITH THE ENGINEER'S APPROVAL.



4-19-2010

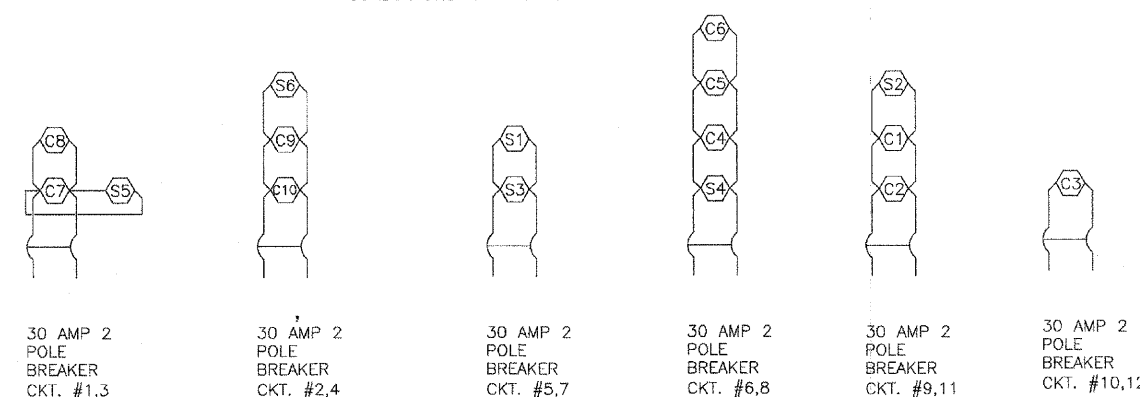


CONTRACT: 85513

POLE FEDER SIZES

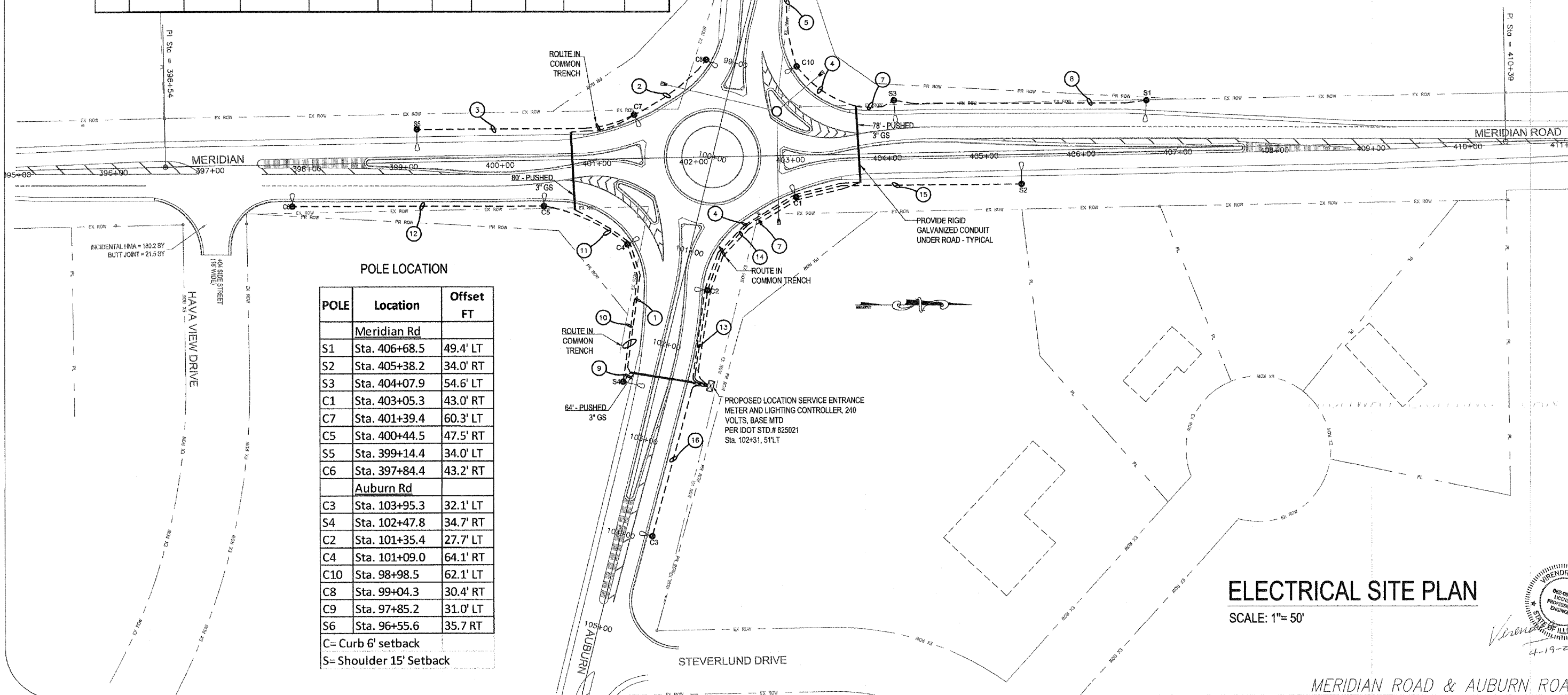
Branch Circuit #	panel Circuit	CU Wire Size	Leaving Device	Load Device	Total Remaining Load (Amps)	Distance (Feet)	Wire Resistance Ohms/1000 ft	Voltage Drop	Cummulative Voltage Drop	% Drop at Applied Voltage
1	1,3	10	Panel	Pole C7	4.20	434.00	0.0012899	4.70	4.70	1.96%
2	1,3	10	Pole C7	Pole C8	1.40	95.00	0.0012899	0.34	5.05	2.10%
3	1,3	10	Pole C7	Pole S5	1.40	227.00	0.0012899	0.82	5.87	2.44%
4	2,4	10	Panel	Pole C10	4.20	473.00	0.0012899	5.13	5.13	2.14%
5	2,4	10	Pole C10	Pole C9	2.80	110.00	0.0012899	0.79	5.92	2.47%
6	2,4	10	Pole C9	Pole S6	1.40	211.00	0.0012899	0.76	6.68	2.78%
7	5,7	10	Panel	Pole S3	2.80	439.00	0.0012899	3.17	3.17	1.32%
8	5,7	10	Pole S3	Pole S1	1.40	261.00	0.0012899	0.94	4.11	1.71%
9	6,8	10	Panel	Pole S4	5.60	92.00	0.0012899	1.33	1.33	0.55%
10	6,8	10	Pole S4	Pole C4	4.20	145.00	0.0012899	1.57	2.90	1.21%
11	6,8	10	Pole C4	Pole C5	2.80	96.00	0.0012899	0.69	3.59	1.50%
12	6,8	10	Pole C5	Pole C6	1.40	261.00	0.0012899	0.94	4.54	1.89%
13	9,11	10	Panel	Pole C2	4.20	108.00	0.0012899	1.17	1.17	0.49%
14	9,11	10	Pole C2	Pole C1	2.80	139.00	0.0012899	1.00	2.17	0.91%
15	9,11	10	Pole C1	Pole S2	1.40	234.00	0.0012899	0.85	3.02	1.26%
16	10,12	10	Panel	Pole C3	1.40	175.00	0.0012899	0.63	0.63	0.26%

ALL WIRING TO BE #10 AWG.
PROVIDE A #10 CU GROUNDING
CONDUCTORS WITH ALL CIRCUITS.



WIRING SCHEMATIC

NO SCALE/PURELY DIAGRAMMATIC

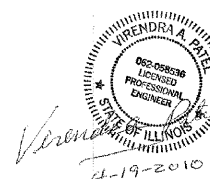


POLE LOCATION

POLE	Location	Offset FT
Meridian Rd		
S1	Sta. 406+68.5	49.4' LT
S2	Sta. 405+38.2	34.0' RT
S3	Sta. 404+07.9	54.6' LT
C1	Sta. 403+05.3	43.0' RT
C7	Sta. 401+39.4	60.3' LT
C5	Sta. 400+44.5	47.5' RT
S5	Sta. 399+14.4	34.0' LT
C6	Sta. 397+84.4	43.2' RT
Auburn Rd		
C3	Sta. 103+95.3	32.1' LT
S4	Sta. 102+47.8	34.7' RT
C2	Sta. 101+35.4	27.7' LT
C4	Sta. 101+09.0	64.1' RT
C10	Sta. 98+98.5	62.1' LT
C8	Sta. 99+04.3	30.4' RT
C9	Sta. 97+85.2	31.0' LT
S6	Sta. 96+55.6	35.7 RT
C= Curb 6' setback		
S= Shoulder 15' Setback		

ELECTRICAL SITE PLAN

SCALE: 1"= 50'





CONTRACT: 85513

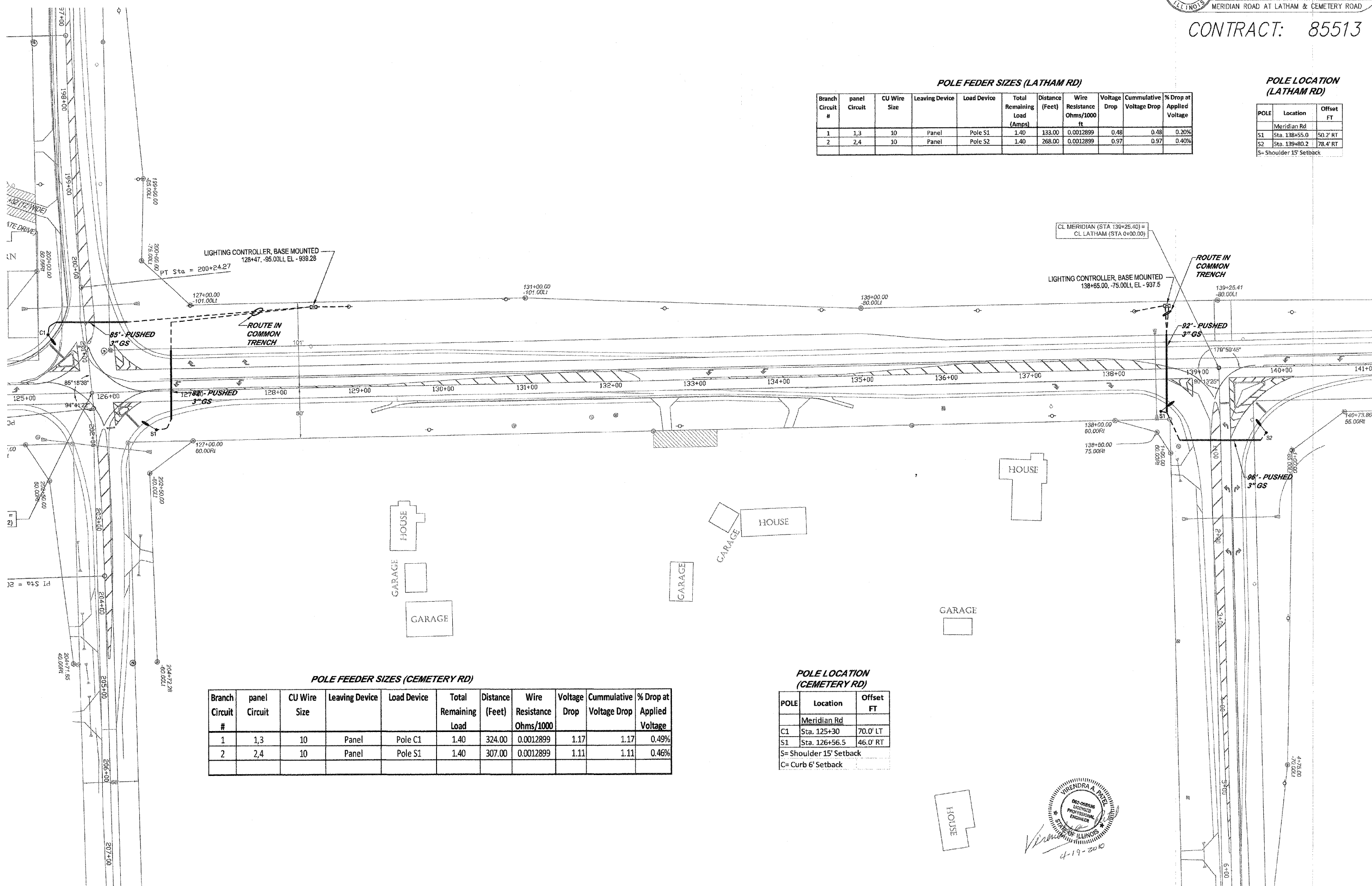
POLE FEDER SIZES (LATHAM RD)

Branch Circuit #	panel Circuit	CU Wire Size	Leaving Device	Load Device	Total Remaining Load (Amps)	Distance (Feet)	Wire Resistance Ohms/1000 ft	Voltage Drop	Cummulative Voltage Drop	% Drop at Applied Voltage
1	1,3	10	Panel	Pole S1	1.40	133.00	0.0012899	0.48	0.48	0.20%
2	2,4	10	Panel	Pole S2	1.40	268.00	0.0012899	0.97	0.97	0.40%

POLE LOCATION (LATHAM RD)

POLE	Location	Offset FT
	Meridian Rd	
S1	Sta. 138+55.0	50.2' RT
S2	Sta. 139+80.2	78.4' RT

S= Shoulder 15' Setback



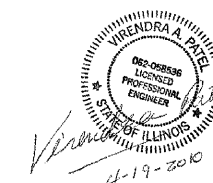
POLE FEDER SIZES (CEMETERY RD)

Branch Circuit #	panel Circuit	CU Wire Size	Leaving Device	Load Device	Total Remaining Load	Distance (Feet)	Wire Resistance Ohms/1000	Voltage Drop	Cummulative Voltage Drop	% Drop at Applied Voltage
1	1,3	10	Panel	Pole C1	1.40	324.00	0.0012899	1.17	1.17	0.49%
2	2,4	10	Panel	Pole S1	1.40	307.00	0.0012899	1.11	1.11	0.46%

POLE LOCATION (CEMETERY RD)

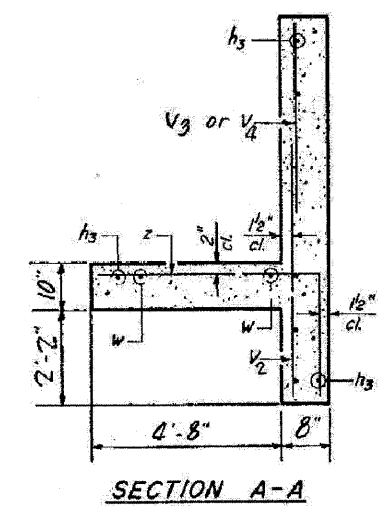
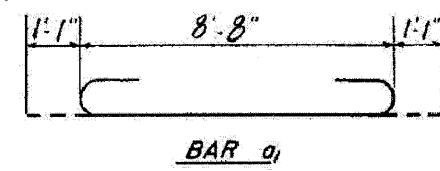
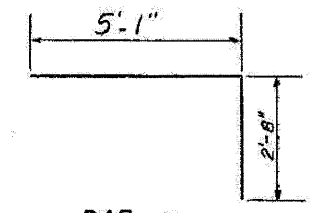
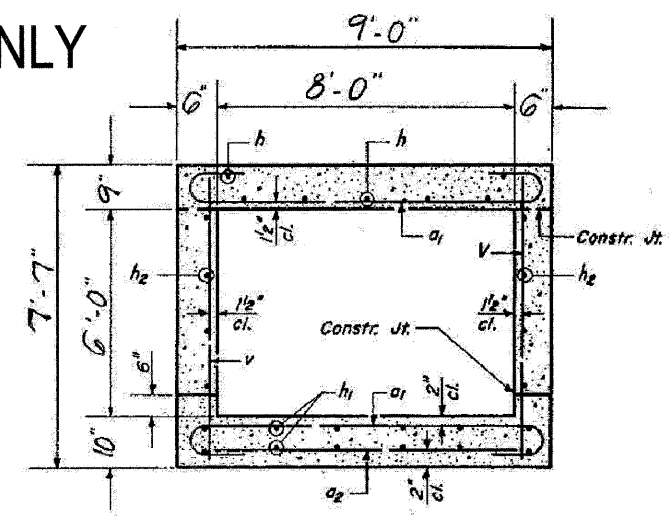
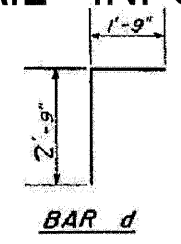
POLE	Location	Offset FT
	Meridian Rd	
C1	Sta. 125+30	70.0' LT
S1	Sta. 126+56.5	46.0' RT

S= Shoulder 15' Setback
 C= Curb 6' Setback



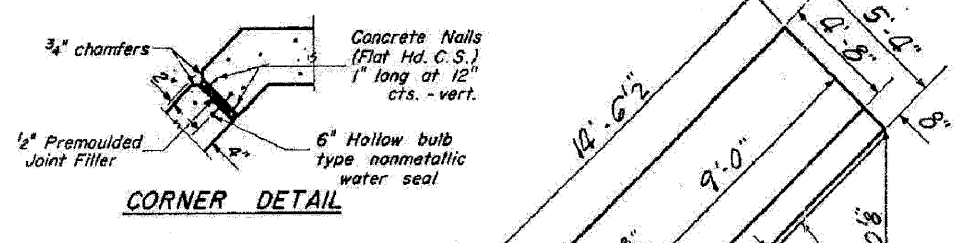
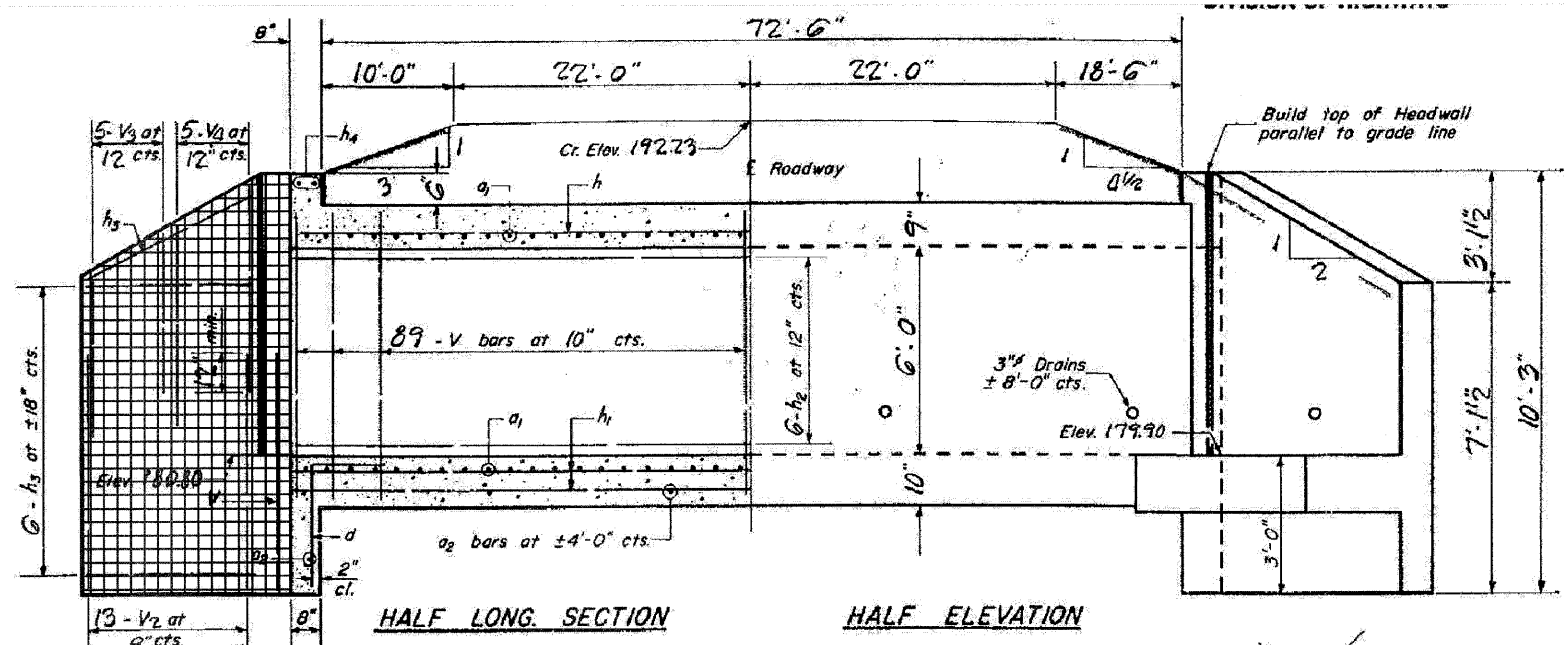
CONTRACT: 85513

EXISTING CULVERT DETAIL - INFO. ONLY

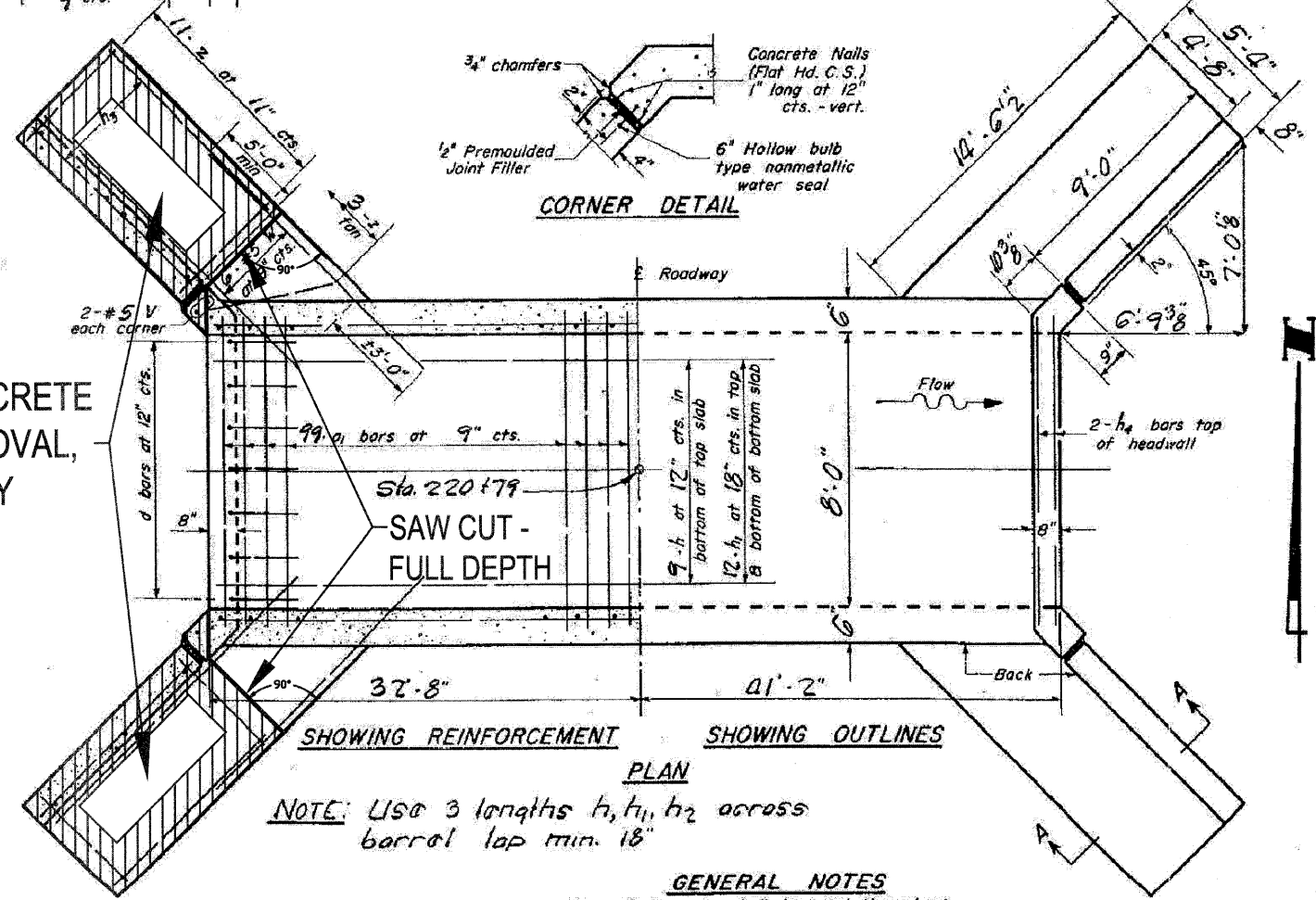


BILL OF MATERIAL

Bar	No.	Size	Length
a1	198	#8	10'-10"
	21	#4	8'-0"
d	18	#4	6'-0"
h	33	#6	5'-9"
h1	36	#5	25'-9"
h2	6	#5	25'-9"
h3	3	#5	8'-9"
h4	4	#5	8'-3"
v	186	#5	7'-4"
v2	52	#4	5'-9"
v3	20	#4	4'-0"
v4	2	#4	5'-6"
w	24	#5	3'-6"
z	56	#5	9'-0"
Class X Concrete	Cu. Yds.		7.1
Reinforcement Bars	Lbs.		11910



CONCRETE
REMOVAL,
6.7 CY



GENERAL NOTES
Class X Concrete shall be used throughout.
Exposed edges shall be beveled 3/4".
For backfilling and embankment see Std. Spec's.
All bars shall be lapped 20 diameters unless otherwise specified.
The top of the culvert, the backs of the sidewalls above the lower construction joint and backs of the wings above the tops of the footings shall be waterproofed in accordance with Art. 51.21 of the Std. Spec's.
Nonmetallic water seal used in the wingwall joints shall extend from the top of the footing to within 6" of the top of the headwall.
Tilt hook of "a1" bars, if necessary, to obtain 1/2" minimum clearance at top of hook.

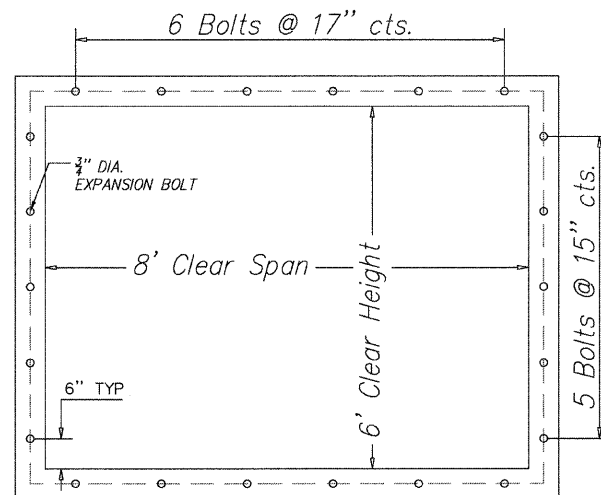
DESIGNED	19
CHECKED	EXAMINED
DRAWN	PASSED
CHECKED	APPROVED

fs = 20,000 psi.
fc = 1400 psi. barrel
fc = 1000 psi. wings
v = 90 psi. barrel
v = 75 psi. footing
n = 10
LOADING HS-20

STA. 220+79
MERIDIAN ROAD - C.H. NO. 24
SEC. 113-2B-MFT
WINNEBAGO, COUNTY

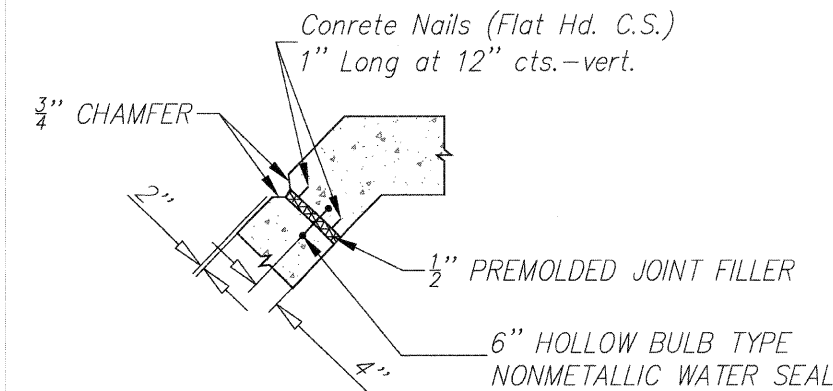
BILL OF MATERIALS

Bar	No.	Size	Length
a1	40	#8	10'-10"
a2	5	#4	8'-3"
d	9	#4	4'-6"
h	9	#6	14'-5"
h1	12	#5	14'-5"
h2	12	#5	14'-5"
h3	18	#4	8'-9"
h4	6	#6	8'-3"
v	38	#5	7'-4"
v2	26	#4	5'-9"
v3	10	#4	4'-0"
v4	10	#4	5'-6"
w	12	#5	13'-6"
z	28	#5	7'-9"
CONCRETE HEADWALL REMOVAL			CY 6.7
3/4" DIA. EXPANSION BOLTS			EACH 22
CONCRETE BOX CULVERT			CY 18.9
REINFORCEMENT BARS			LBS 2,790

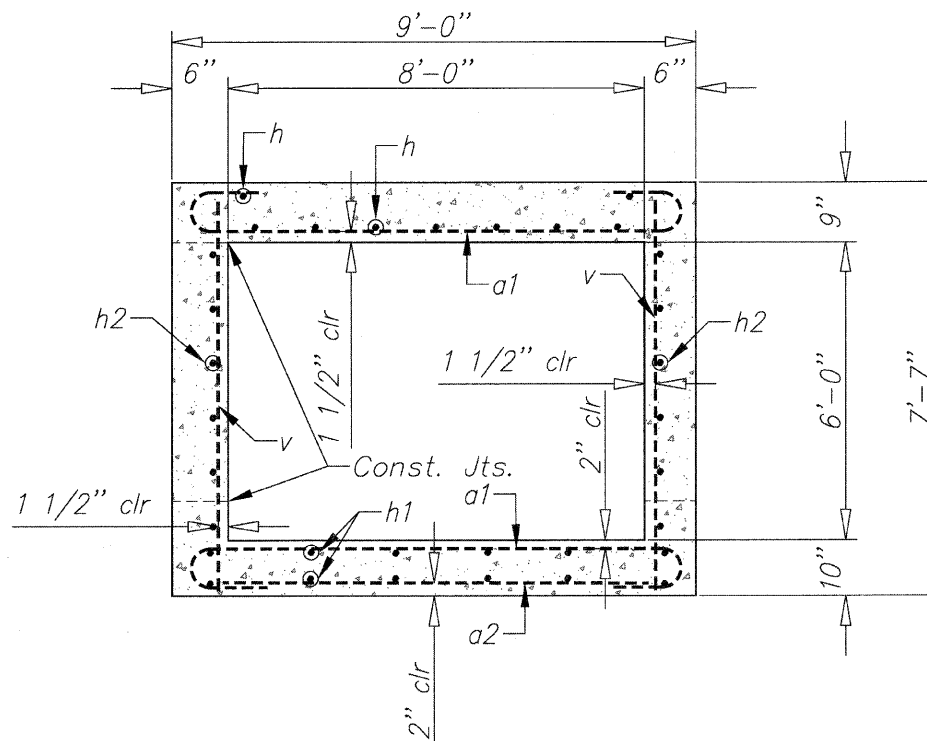


**EXPANSION BOLTS
LAYOUT DETAIL**

NOTE:
Expansion Bolts shall be $\frac{3}{4}$ " diameter hooked bolts. Hooked bolts shall extend a minimum of 9" into new concrete.



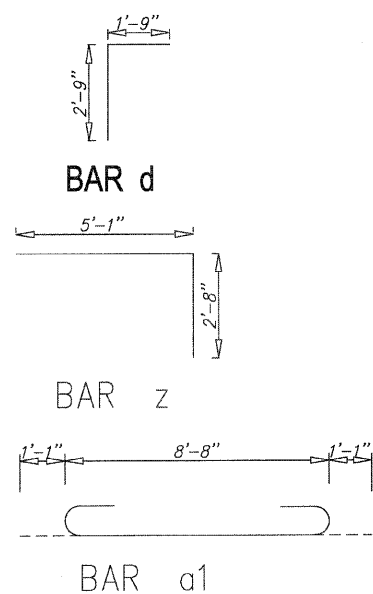
CORNER DETAIL



SECTION THRU BARREL

$f_s = 20,000$ psi
 $f_c = 1400$ psi barrel
 $f_c = 1000$ psi wings
 $v = 90$ psi barrel
 $v = 75$ psi footing
 $n = 10$

LOADING HS-20

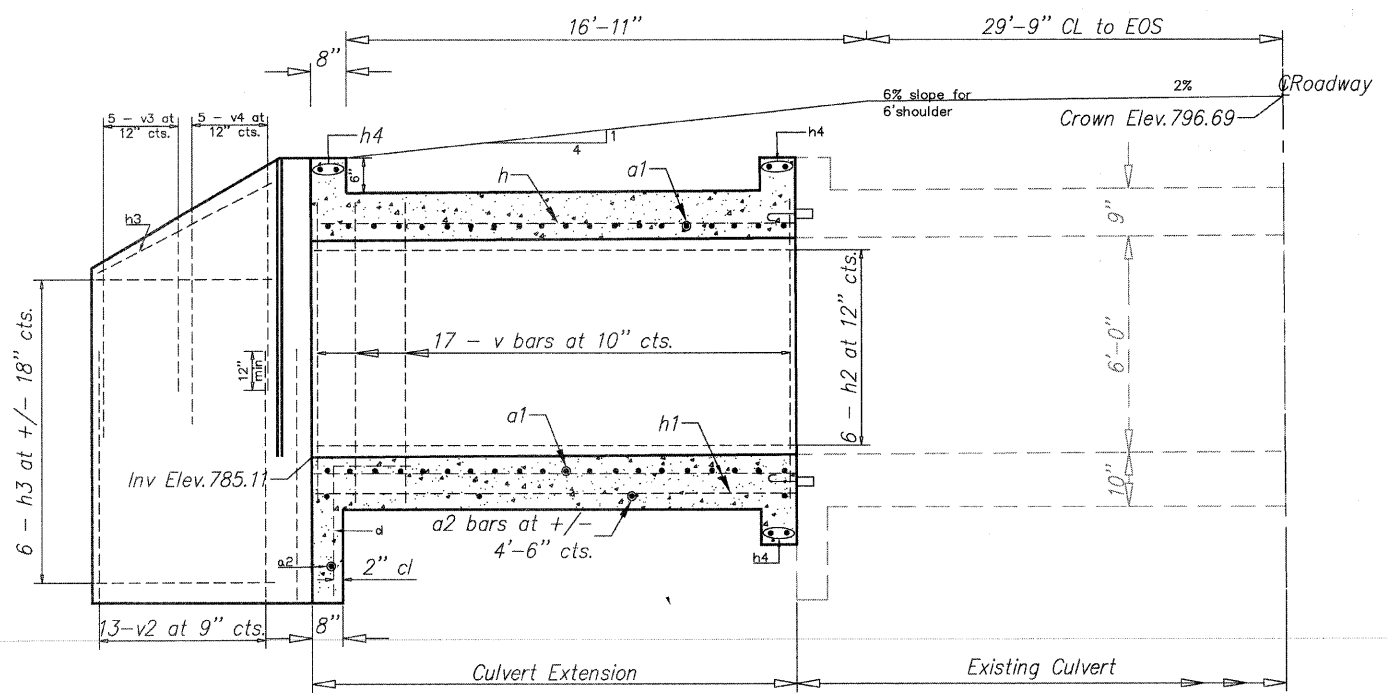


GENERAL NOTES:

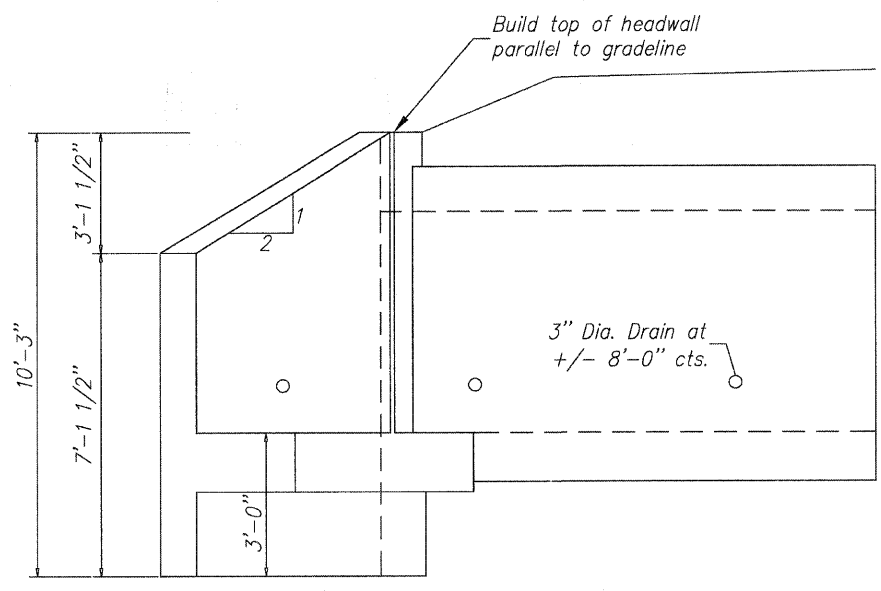
- Class Sl Concrete shall be used throughout.
- Exposed edges shall be beveled $\frac{3}{4}$ ".
- For backfilling and embankment see section 502 of St'd. Specs. Structure Excavation will not be paid separately and will be incidental to CONCRETE BOX CULVERT.
- All bars shall be lapped 20 diameters unless otherwise specified.
- Nonmetallic water seal used in the wing wall joints shall extend from the top of the footing to within 6" of the top of the headwall.
- Tilt hook of "a1" bars, if necessary, to obtain $1 \frac{1}{2}$ " minimum clearance at top of hook.



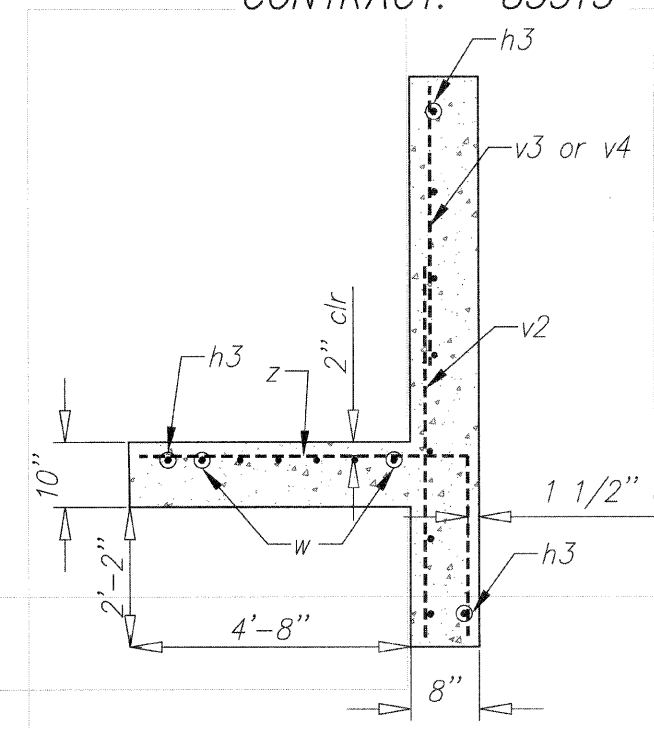
CONTRACT: 85513



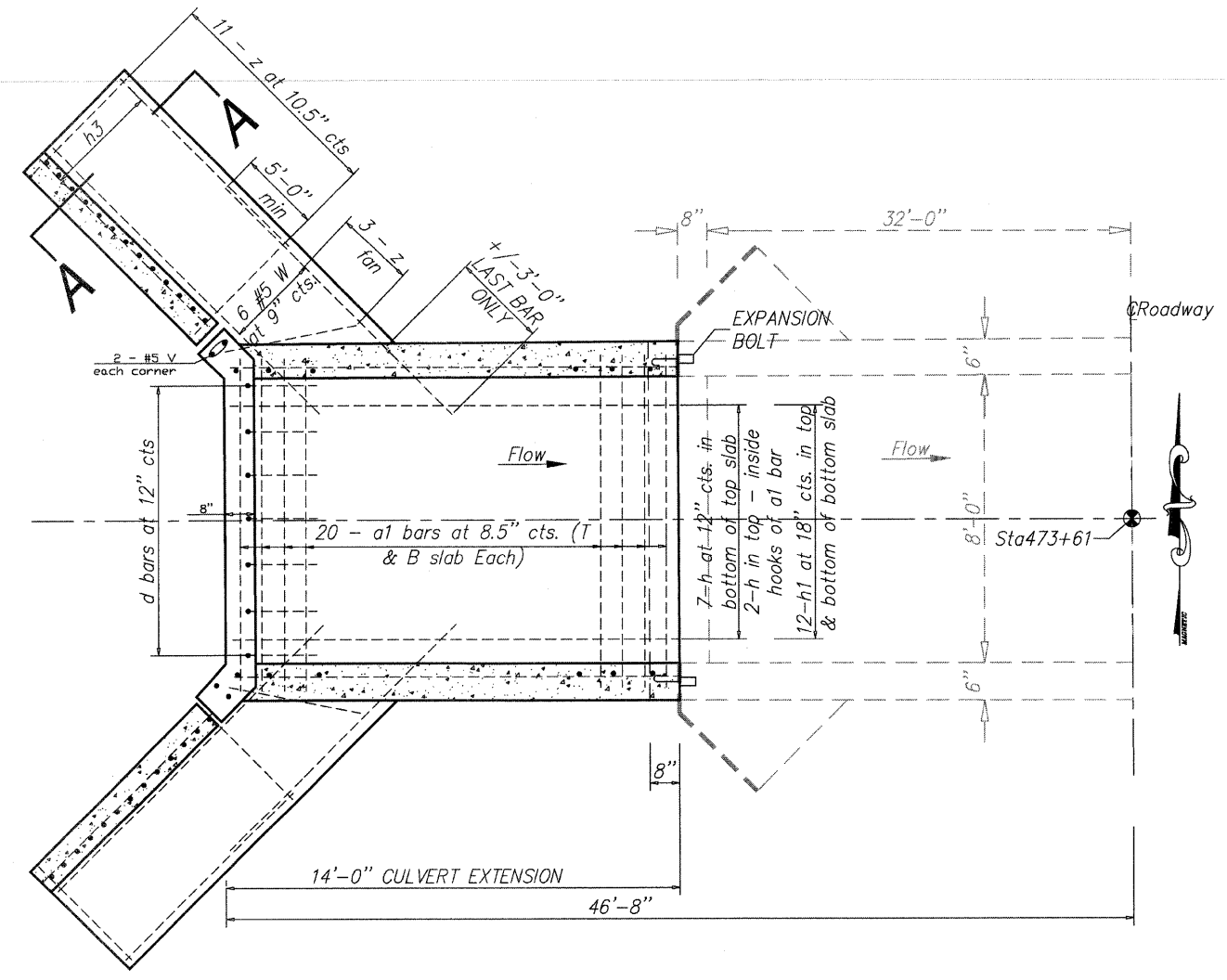
LONG. SECTION



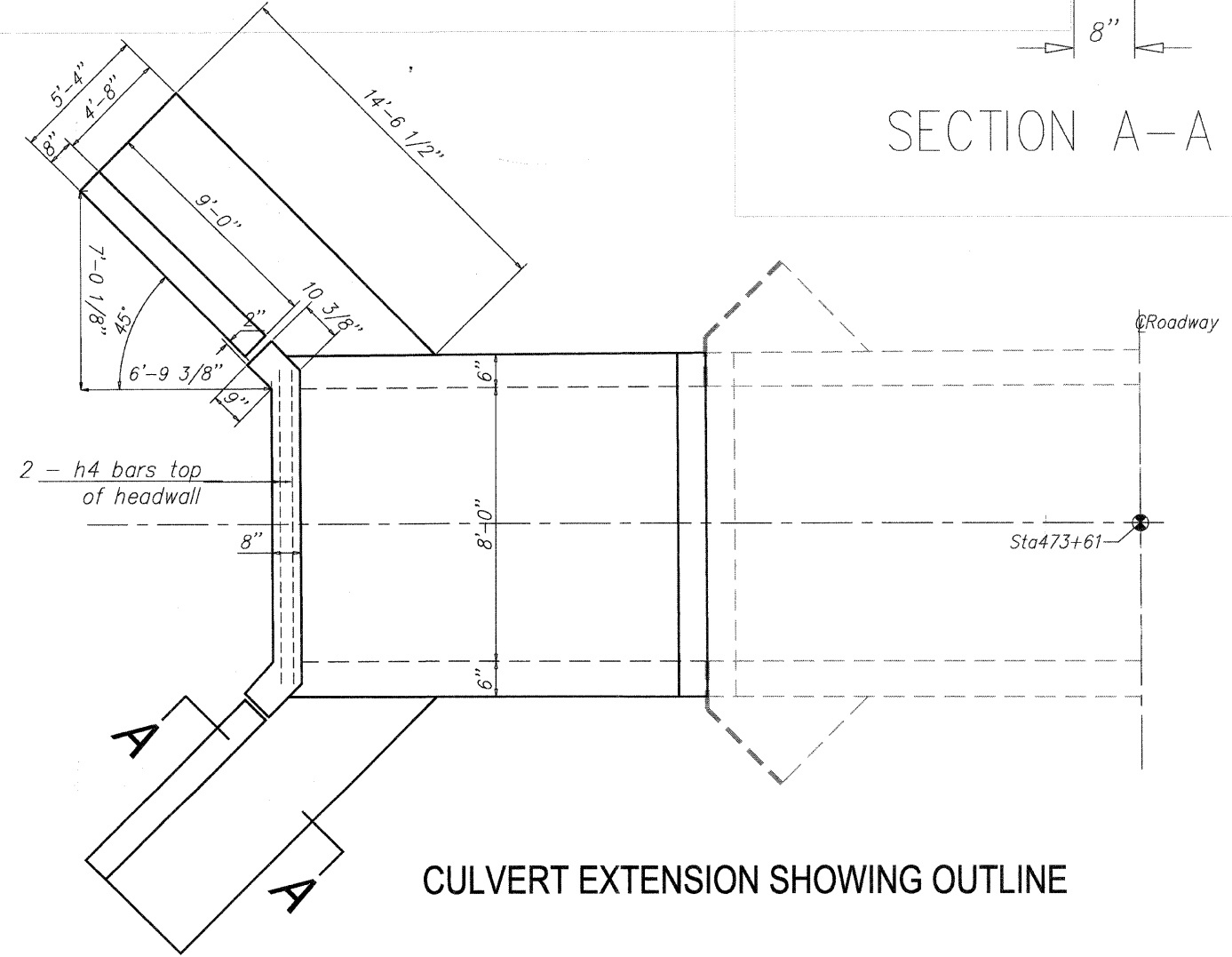
ELEVATION



SECTION A-A

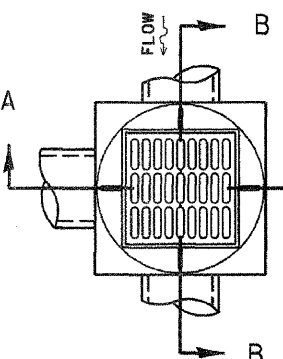
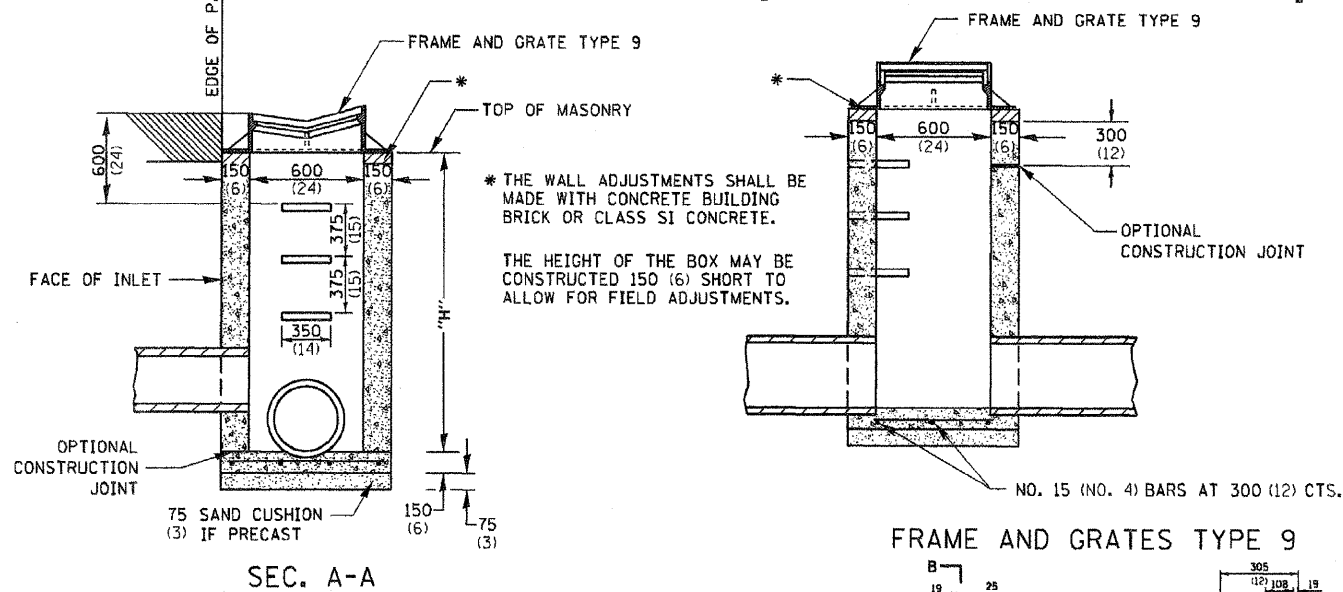


CULVERT EXTENSION SHOWING REINFORCEMENT



CULVERT EXTENSION SHOWING OUTLINE

INLET SPECIAL (TYPE A GUTTER)



NOTES

SEE STANDARD 602701 FOR DETAILS OF STEPS.

EXCEPT AS NOTED HEREON INLET SPECIAL SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 602 OF THE STANDARD SPECIFICATIONS.

THE SIDE WALLS MAY BE BUILT AS PRECAST SEGMENTED SECTIONS.

ALL VOIDS AROUND PIPE ENTRANCE, BOTH INSIDE AND OUTSIDE, SHALL BE SEALED WITH MORTAR.

WEIGHT OF CAST IRON FRAME & GRATE = 200kg (440 LBS.).

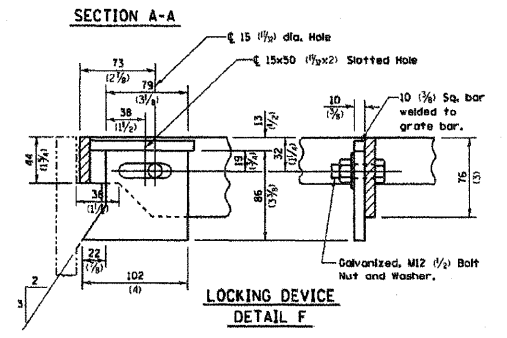
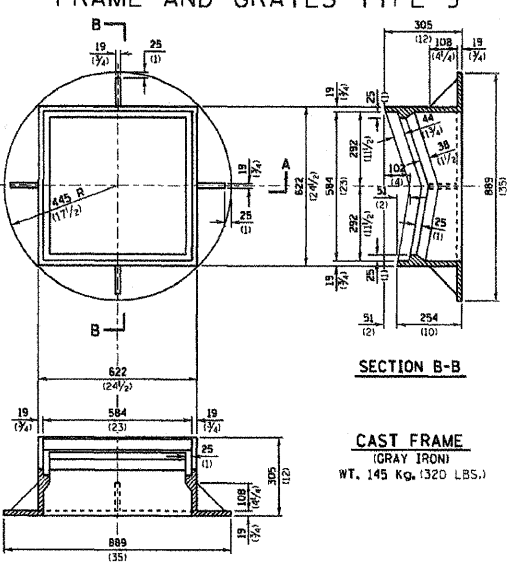
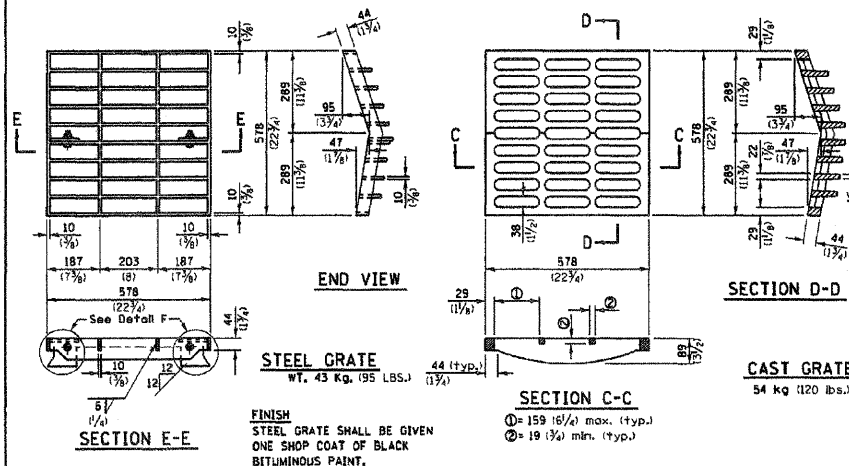
STEPS SHALL BE OMITTED WHEN DEPTH OF "H" IS LESS THAN 1524 (60").

DETAIL OF FRAME & GRATE

NOTES

CLASS SI CONCRETE OR PRECAST CONCRETE SHALL BE USED THROUGHOUT. PRECAST CONCRETE SHALL BE IN ACCORDANCE WITH SECTION 504.01 THRU 504.05 OF THE STANDARD SPECIFICATIONS EXCEPT THAT CONCRETE STRENGTH SHALL BE 27.5 MPa (4,000 PSI) AFTER 28 DAYS.

THE CONTRACT UNIT PRICE EACH FOR INLET SPECIAL SHALL INCLUDE THE COST OF CONSTRUCTING THE INLET BOX, FURNISHING AND INSTALLING THE FRAME AND GRATE, THE CAST IRON STEPS (IF USED), THE PRECAST FLOOR SLAB, SAND CUSHION (WHEN USED) AND REINFORCEMENT BARS.



GENERAL NOTES

THE MATERIAL FOR STEEL GRATE SHALL CONFORM TO ARTICLE 1006.04 OF THE STANDARD SPECIFICATIONS.

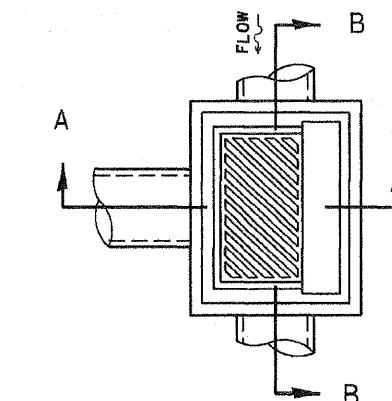
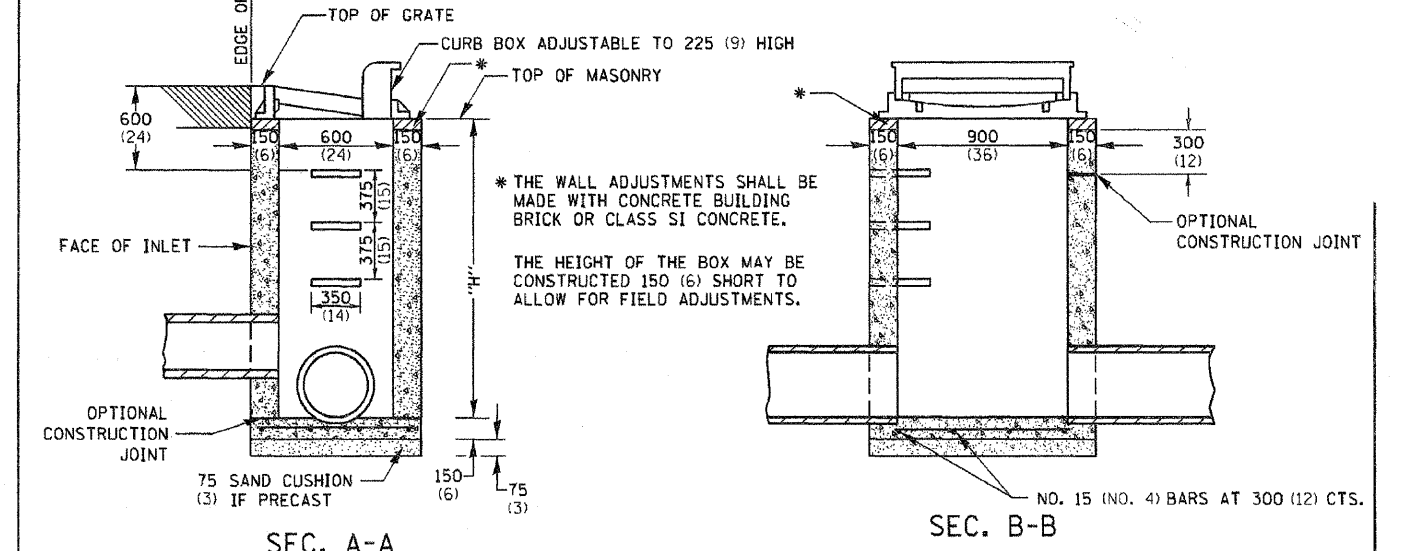
THE USE OF EITHER A CAST GRATE OR A STEEL GRATE WITH THE CAST FRAME SHALL BE THE OPTION OF THE CONTRACTOR.

THE CAST GRATE MAY BE MADE OF EITHER GRAY IRON OR DUCTILE IRON CONFORMING TO THE STANDARD SPECIFICATIONS. DUCTILE IRON CASTING SHALL BE GRADE 65-45-12

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

REVISED - 1-27-00	REGION 2 / DISTRICT 2 STANDARD				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED -									
REVISED -									
REVISED -	SCALE: 1:8000	SHEET NO.	OF SHEETS	STA.	CONTRACT NO.				
PLOT DATE = Fri Sep 19 09:30:27 2009					ILLINOIS FED. AID PROJECT				

INLET SPECIAL



NOTES

SEE STANDARD 602701 FOR DETAILS OF STEPS.

EXCEPT AS NOTED HEREON INLET SPECIAL SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 602 OF THE STANDARD SPECIFICATIONS.

THE SIDE WALLS MAY BE BUILT AS PRECAST SEGMENTED SECTIONS.

ALL VOIDS AROUND PIPE ENTRANCE, BOTH INSIDE AND OUTSIDE, SHALL BE SEALED WITH MORTAR.

WEIGHT OF CAST IRON FRAME & GRATE = 240 kg (530 lbs.) ±.

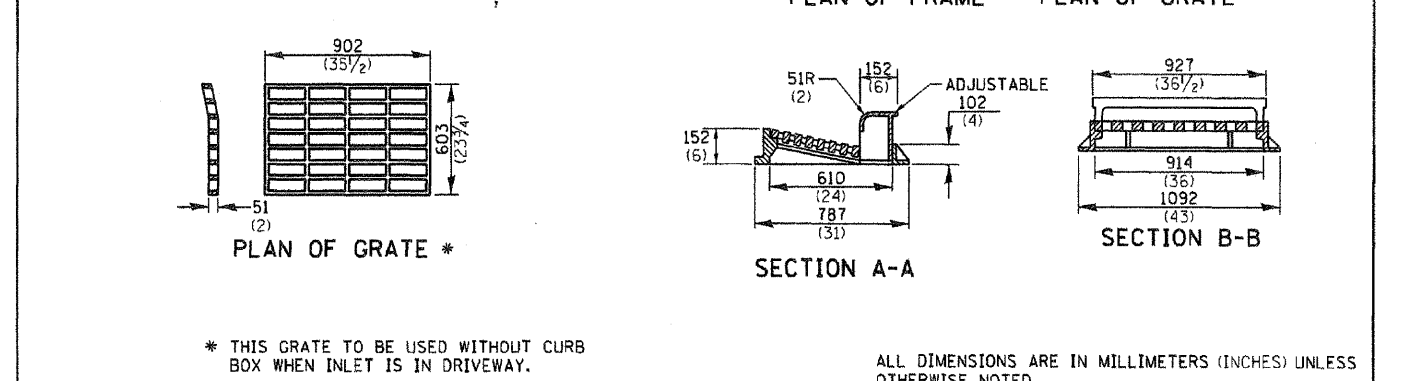
STEPS SHALL BE OMITTED WHEN DEPTH OF "H" IS LESS THAN 1.5 m (5 ft).

DETAIL OF FRAME & GRATE

NOTES

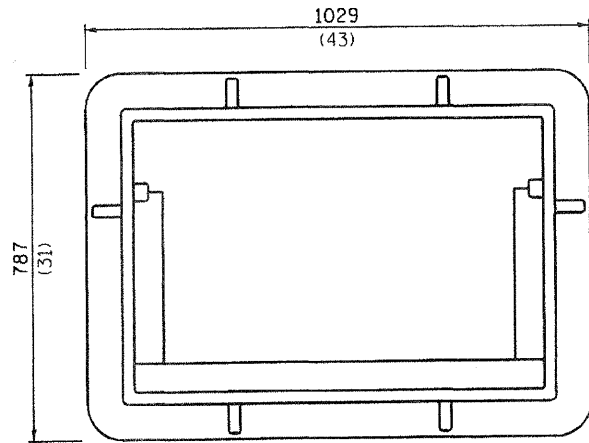
CLASS SI CONCRETE OR PRECAST CONCRETE SHALL BE USED THROUGHOUT. PRECAST CONCRETE SHALL BE IN ACCORDANCE WITH SECTION 504.01 THRU 504.05 OF THE STANDARD SPECIFICATIONS EXCEPT THAT CONCRETE STRENGTH SHALL BE 27.5 MPa (4,000 PSI) AFTER 28 DAYS.

THE CONTRACT UNIT PRICE EACH FOR INLET SPECIAL SHALL INCLUDE THE COST OF CONSTRUCTING THE INLET BOX, FURNISHING AND INSTALLING THE FRAME AND GRATE, THE CAST IRON STEPS (IF USED), THE PRECAST FLOOR SLABS, SAND CUSHION (WHEN USED) AND REINFORCEMENT BARS.

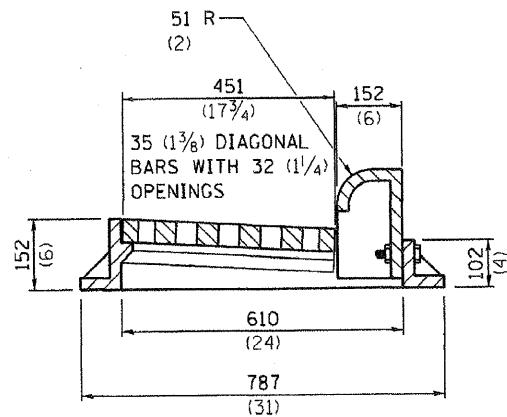


REVISED - 11-10-94	REGION 2 / DISTRICT 2 STANDARD				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED -									
REVISED -									
REVISED -	SCALE: 1:8000	SHEET NO.	OF SHEETS	STA.	CONTRACT NO.				
PLOT DATE = Fri Sep 18 09:30:25 2009					ILLINOIS FED. AID PROJECT				

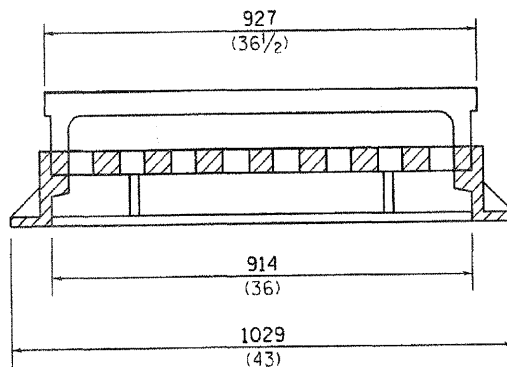
FRAME AND GRATE FOR INLET SPECIAL



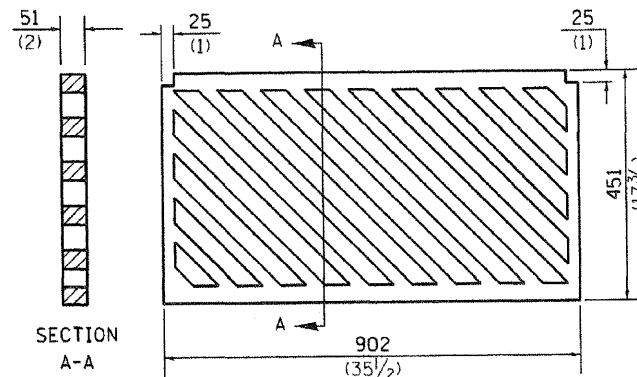
PLAN OF FRAME
WITHOUT GRATE AND CURB BOX



TRANSVERSE SECTION



LONGITUDINAL SECTION



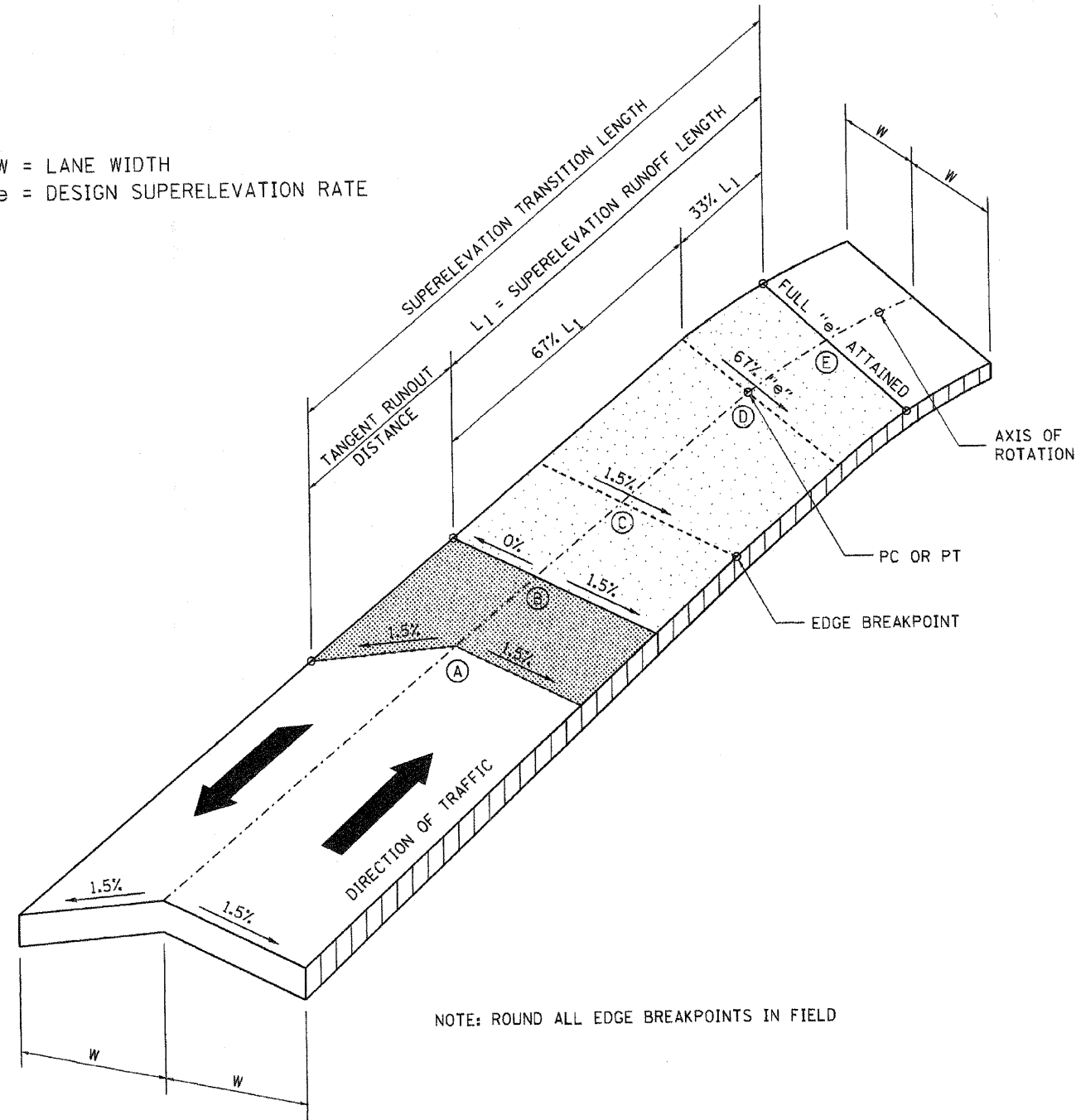
PLAN OF GRATE

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

R 3067
APPROXIMATE WEIGHT - 231 Kg. (510 LBS.)

SUPERELEVATION TRANSITION ON TWO-LANE HIGHWAY

W = LANE WIDTH
e = DESIGN SUPERELEVATION RATE



NOTE: ROUND ALL EDGE BREAKPOINTS IN FIELD

TRANSITION CURVE TABLE

CURVE PI STA.	SUPERELEVATION "e"	W	SUPERELEVATION TRANSITION LENGTH	TANGENT RUNOUT DISTANCE	SUPERELEVATION RUNOFF LENGTH

REVISED - 9-25-92	REGION 2 / DISTRICT 2 STANDARD		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED -			CONTRACT NO.				
REVISED -	SCALE: 1:8000	SHEET NO.	OF	SHEETS	STA.	TO	STA.
REVISED -			FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

REVISED - 11-09-06	REGION 2 / DISTRICT 2 STANDARD		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED -			CONTRACT NO.				
REVISED -	SCALE: 1:8000	SHEET NO.	OF	SHEETS	STA.	TO	STA.
REVISED -			FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

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