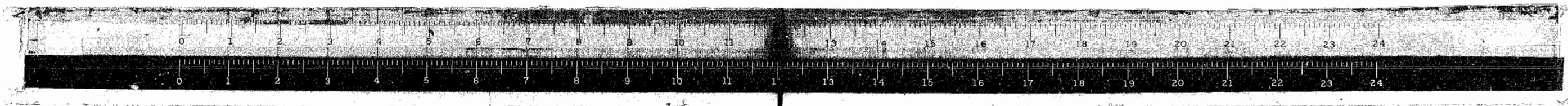


RT. FAI-55

SEC. 57-1HB
57-1(1)



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TYPICAL SECTIONS & SHLD.DETAILS	2,2A
SUMMARY OF QUANTITIES & GENERAL NOTES	3
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PLAN & PROFILE FAI-55	7
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CROSS SECTIONS FAI-55	60-64
CROSS SECTIONS RAMPS	65-68

- STANDARDS:
- 1686-3, 1744-2, 1683-2
 - 1690
 - 1766-7
 - 1976 , 2113-1
 - 2115-3
 - 2122-4
 - 2130-3
 - 2135 2153-9
 - 2179-4 2228-2, 2217-2
 - 2237-6
 - 2240-2
 - 2248-1
 - 2262-1
 - 2263-2
 - 2298-3
 - 2299-4
 - 2300
 - 2303-3
 - 2323 2327-1
 - 2302-2
 - 2304-2
 - 2305-2
 - 2306-3
 - 2307-3
 - 2310-2
 - 2311-3
 - 2315-3

SCALES

PLAN	1" = 100'
PROFILE	HORZ. 1" = 100'
	VERT. 1" = 10'
CROSS SECTIONS	HORZ. 1" = 10'
	VERT. 1" = 5'

HIGHWAY CLASSIFICATION

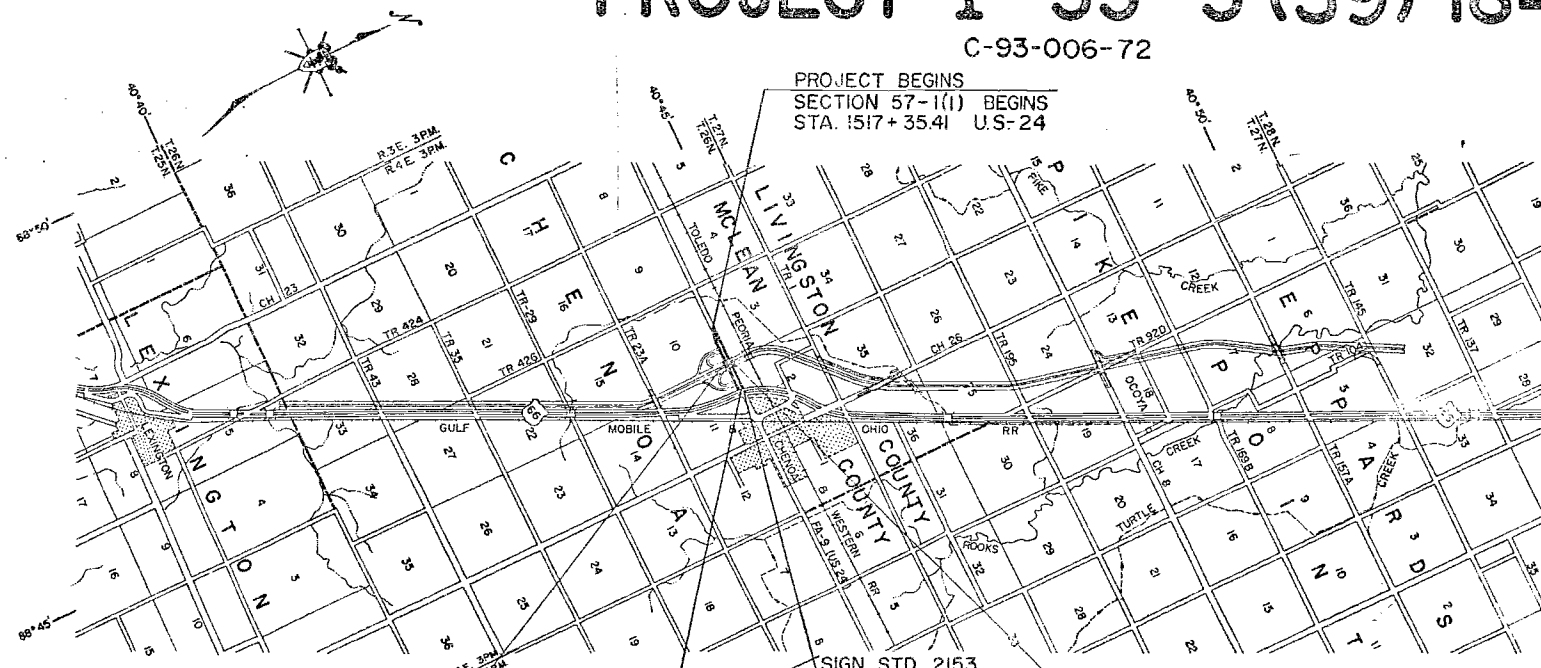
FAI-55	3138(93)	TRUNK	20.27	(PCC-20)
FA-9	1040(92)	MAJOR	312	(PCC-20)

CONTRACT NO. 20202

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED
FEDERAL AID INTERSTATE HIGHWAY
FAI ROUTE 55
SECTION 57-IHB & 57-1(I)
MCLEAN COUNTY
PROJECT I-55-5 (39) 184

C-93-006-72



PROJECT I-55-5(39)184 BEGINS
SECTION 57-IHB BEGINS
STA. 397+63.79

PROJECT ENDS
SECTION 57-1(I) ENDS
STA. 1563 + 61.55 U.S. 24

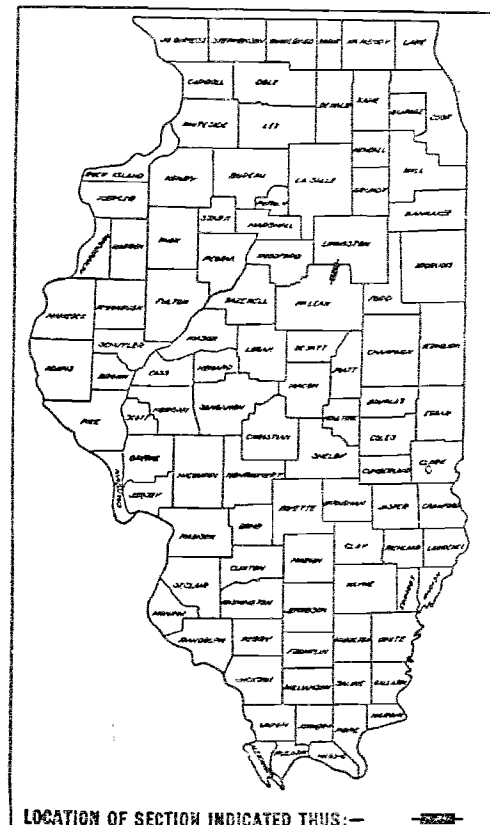
PROJECT I-55-5(39)184 ENDS
SECTION 57-IHB ENDS
STA. 399 + 50.21

APPROXIMATE SCALE

PROJECT LENGTH	186.42	FEET	0.035	MILES
NET LENGTH	4626.14	FEET	0.876	MILES
SECTION 57-1(I)				
SECTION 57-IHB				

DISTRICT	SEC.	COUNTY	SHEET NO.	TOTAL SHEETS
FAI-55	57-1(I)	MCLEAN	88	1

P-93-032-68



LOCATION OF SECTION INDICATED THUS:—

SECTION 57-IHB INCLUDES
SPECIAL BRIDGE DESIGN
DUAL STRUCTURES CARRYING
1-55 OVER U.S. ROUTE 24.
REINFORCED CONCRETE DECK
ON W36 BEAMS. 2-MAIN SPANS
AT 66'-3 1/2" AND 36" PRECAST,
PRESTRESSED CONCRETE I-BEAMS.
2-APPROACH SPANS AT 26'-7 1/2"
ON REINFORCED CONCRETE PIER
AND VAULTED ABUTMENTS.

Carter Jenkins
CARTER JENKINS
CONSULTING ENGINEER

Charles H. Merchant
CHARLES H. MERCHANT
STRUCTURAL ENGINEER

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED: *[Signature]*
EXAMINED: *[Signature]*
DATE: *10/27/72*

APPROVED: *[Signature]*
UNDER SECRETARY, CHIEF TRANSPORTATION ENGINEER

APPROVED: *[Signature]*
SECRETARY

DATE: *June 26, 1972*

PLANS PREPARED BY
JENKINS, MERCHANT & NANKIVIL
CONSULTING ENGINEERS
SPRINGFIELD, ILLINOIS

DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: *C. S. Monnier*
DIVISION ENGINEER

DATE: *10/9/72*



INDEX OF SHEETS

COVER SHEET	1
TYPICAL SECTIONS & SHLD.DETAILS	2,2A
SUMMARY OF QUANTITIES & GENERAL NOTES	3
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CROSS SECTIONS FAI.-55	60-64
CROSS SECTIONS RAMPS	65-68

STANDARDS:

1686-3, 1744-2, 1683-2
1690
1766-7
1976, 2113-1
2115-3
2122-4
2130-3
2135, 2153-9
2179-4, 2228-2, 2217-2
2237-6
2240-2
2248-1
2262-1
2263-2
2298-3
2299-4
2300
2307-3
2323, 2327-1
2302-2
2304-2
2305-2
2306-3
2307-3
2310-2
2311-3
2315-3

SCALES

PLAN	1" = 100'
PROFILE	HORZ. 1" = 100'
	VERT. 1" = 10'
CROSS SECTIONS	HORZ. 1" = 10'
	VERT. 1" = 5'

HIGHWAY CLASSIFICATION

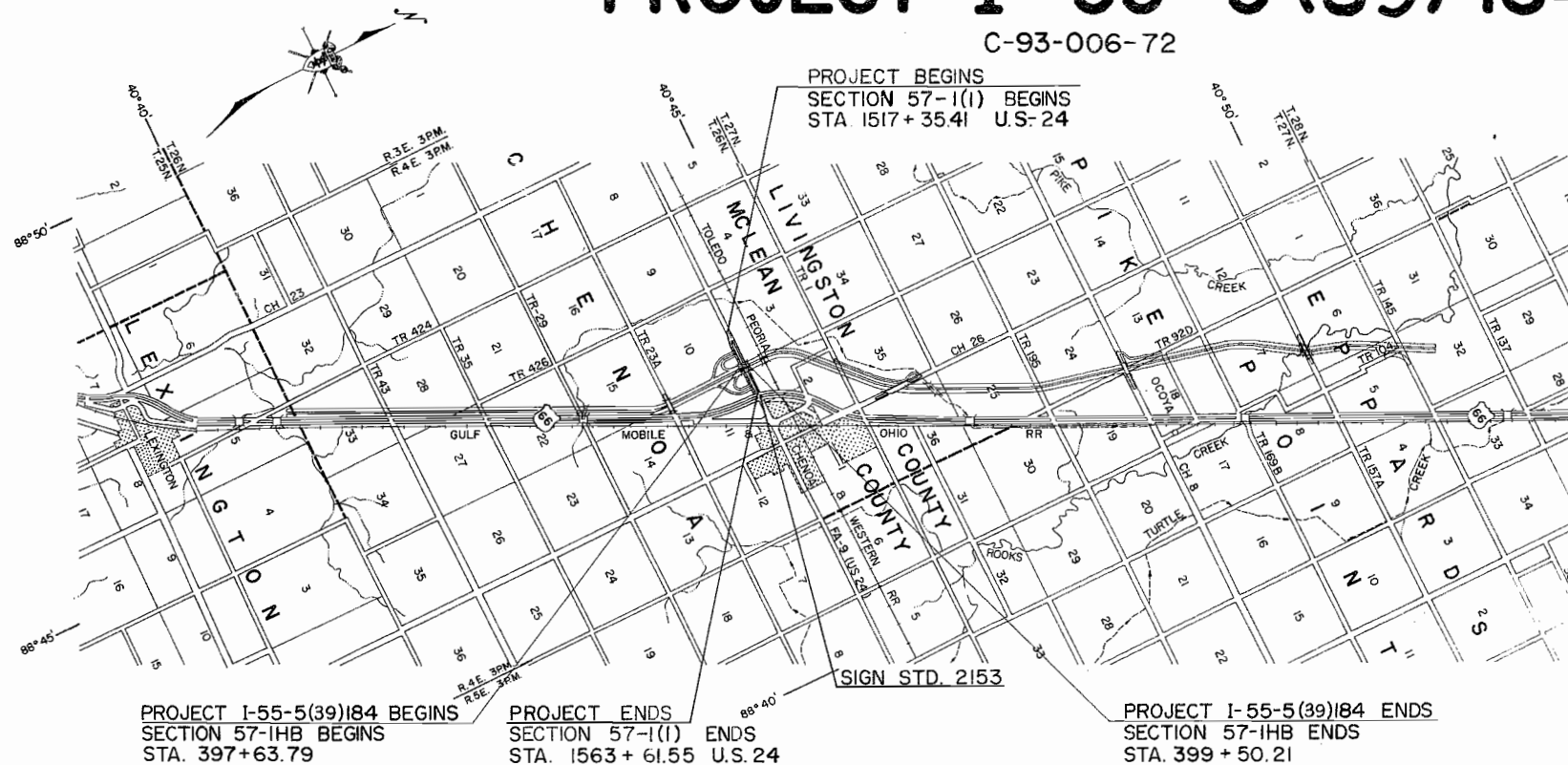
FAI-55	3138(93)	TRUNK	20.27	(PCC-20)
F.A.-9	1040(92)	MAJOR	312	(PCC-20)

CONTRACT NO. 29202

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PLANS FOR PROPOSED
FEDERAL AID INTERSTATE HIGHWAY
FAI ROUTE 55
SECTION 57-IHB & 57-1(I)
MCLEAN COUNTY
PROJECT I-55-5 (39) 184**

C-93-006-72



APPROXIMATE SCALE

0	1	2	3	4	
MILES					
PROJECT LENGTH	186.42	FEET	0.035	MILES	
SECTION 57-1(I)	NET LENGTH	4626.14	FEET	0.876	MILES
SECTION 57-IHB	NET LENGTH	186.42	FEET	0.035	MILES

FEDERAL AID ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-55	57-1(I)	MCLEAN	68	1

P-93-032-68



SECTION 57-IHB INCLUDES SPECIAL BRIDGE DESIGN DUAL STRUCTURES CARRYING I-55 OVER U.S. ROUTE 24. REINFORCED CONCRETE DECK ON W36 BEAMS. 2-MAIN SPANS AT 66'-3 1/2" AND 36" PRECAST, PRESTRESSED CONCRETE I-BEAMS. 2-APPROACH SPANS AT 26'-7 1/2" ON REINFORCED CONCRETE PIER AND VAULTED ABUTMENTS.

Carter Jenkins
CARTER JENKINS
CONSULTING ENGINEER

Charles H. Merchant
CHARLES H. MERCHANT
STRUCTURAL ENGINEER

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED: 7-12-72

EXAMINED: 8-28-72

PASSED: 8-28-72

APPROVED: [Signature]

UNDER SECRETARY, CHIEF TRANSPORTATION ENGINEER

APPROVED: [Signature]

SECRETARY

DATE: June 26, 1972

PLANS PREPARED BY
JENKINS, MERCHANT & NANKIVIL
CONSULTING ENGINEERS
SPRINGFIELD, ILLINOIS

DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

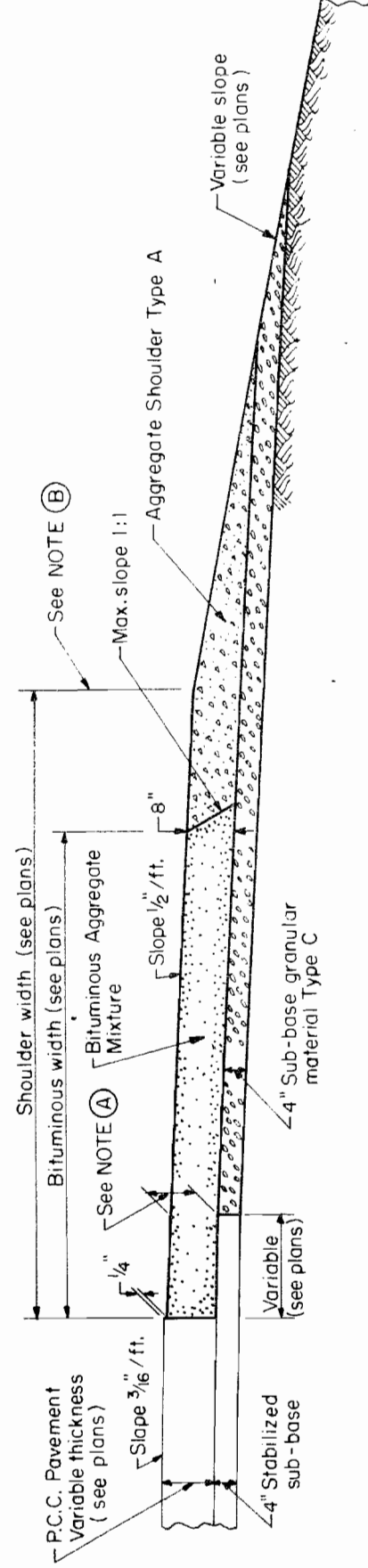
APPROVED

DIVISION ENGINEER

DATE

SHOULDER DETAILS

FAI.55 57-1HB M=1.00 68 2A

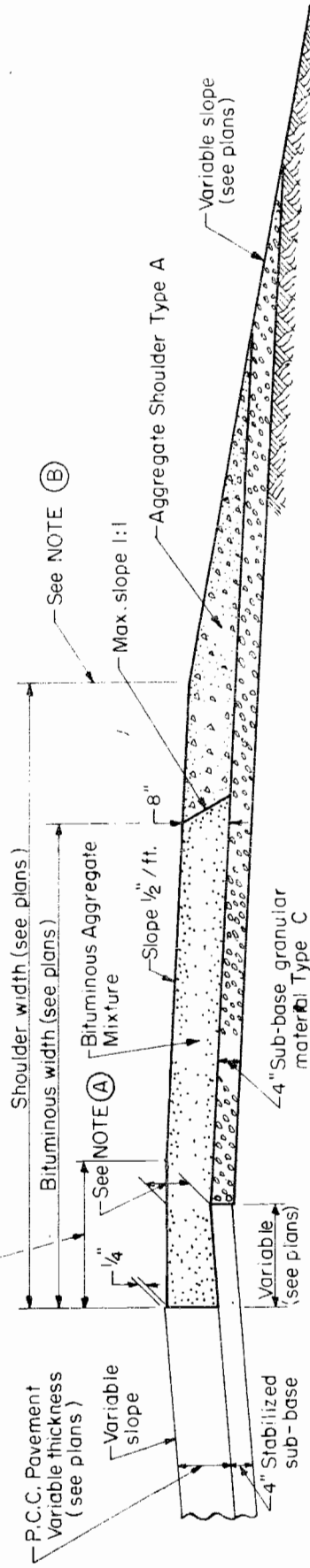


SHOULDER FOR TANGENT PAVEMENT

NOTE "A"

This thickness will vary with the thickness of pavement, extended length of sub-base, and the slope of pavement. When this thickness is less than 8 inches the stabilized shoulder and sub-base shall be stepped down at this line to provide a 8 inch minimum thick stabilized shoulder.

When the superlevation of pavement exceeds 0.04 ft./ft. a 2 foot portion of the shoulder that is adjacent to pavement shall be sloped so that the algebraic difference between pavement and shoulder will not be greater than 0.08 ft./ft.

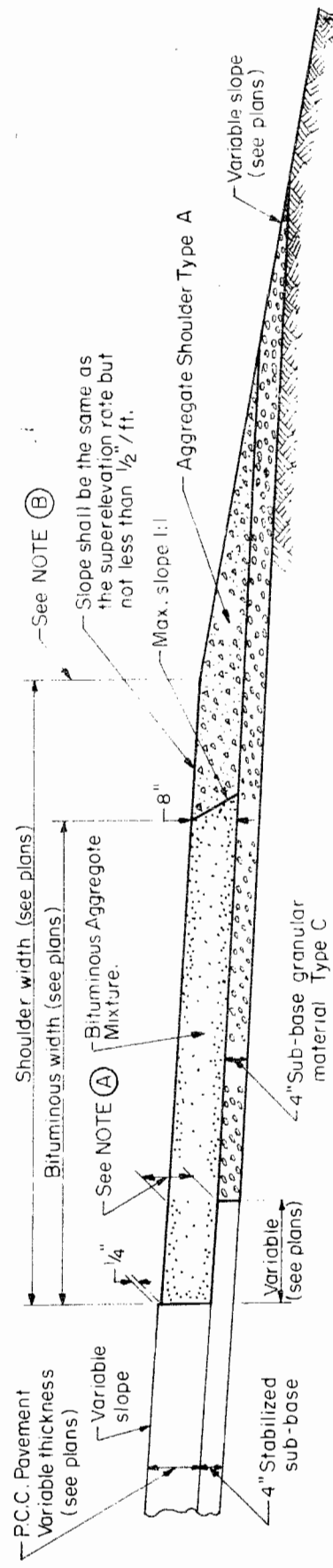


SHOULDER FOR SUPERELEVATED PAVEMENT

(OUTSIDE OF CURVE)

NOTE "B"

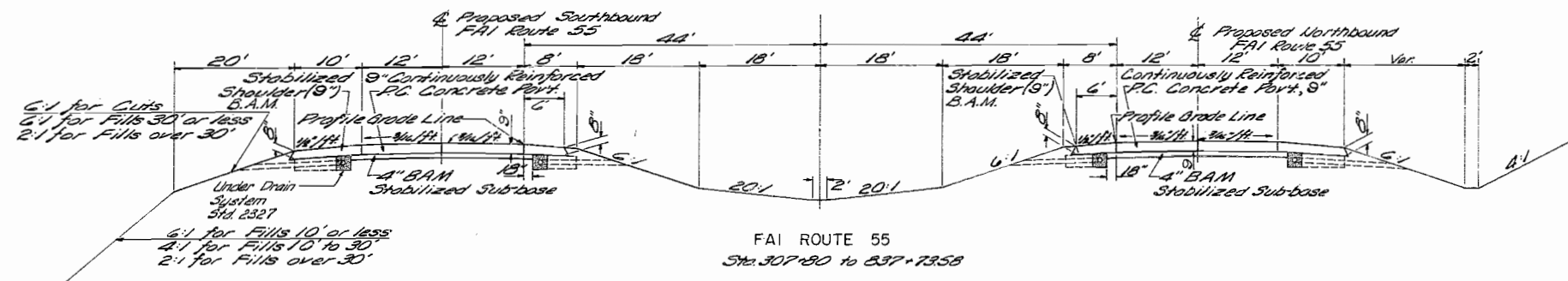
When the plans specify the shoulder to be stabilized full width the BAM shall be extended to this line



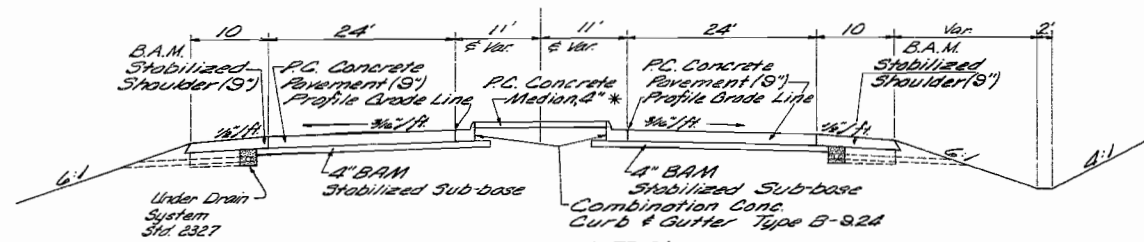
SHOULDER FOR SUPERELEVATED PAVEMENT

(INSIDE OF CURVE)

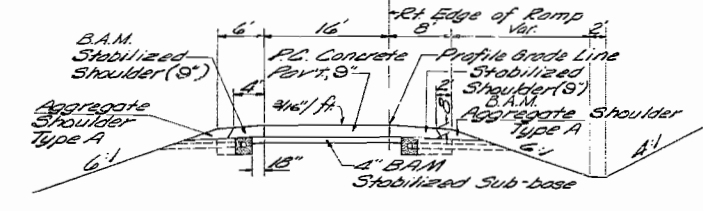
SECTION NO.	DATE	COUNTY	TOTAL SHEETS
FAI 55-57-11B	5/7/78	McLean	25
STA.	TO STA.		
874.832 NO. 4	874.832	874.832	874.832



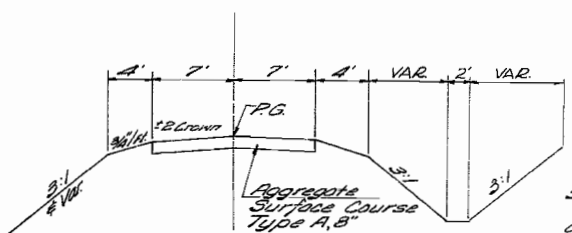
FAI ROUTE 55
Sta. 307+80 to 337+73.58



U.S.-24 (FA. ROUTE 9)
STA. 1517+35.41 to STA. 1559+41.71
SEE DETAILS AND PLANS FOR MEDIAN VARIATIONS.



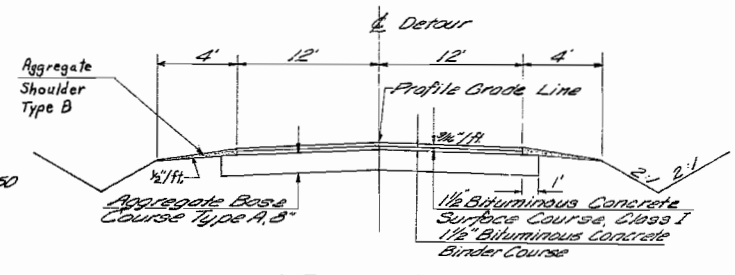
INTERCHANGE RAMP



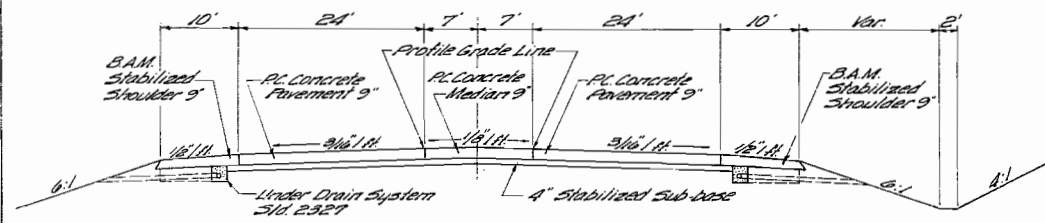
ACCESS ROAD
PROVIDE A-3 SURFACE TREATMENT FROM EDGE OF U.S.-24 PAVEMENT TO 3 FEET OUTSIDE OF SHOULDER.

TRAFFIC DESIGN DATA

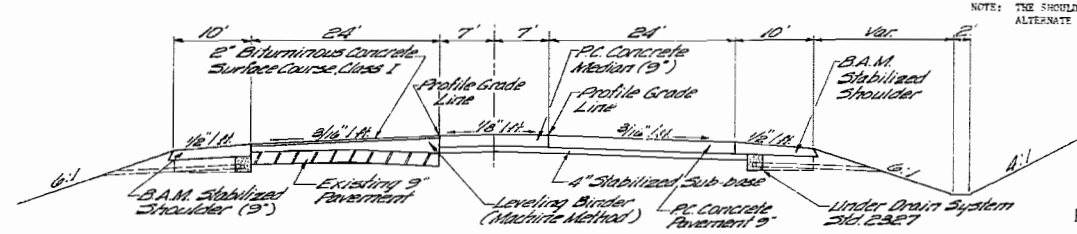
Structural Design Traffic: Year 1985 PC-6159
 Class II Roads & Streets SU=885 MU=655
 Minimum Soil Support: CBR=3.0
 Per Cent of S.D.T. in Design Lane: U.P.=90 U.S.=50 U.M.=50
 Pavement Structure Materials:
 Surface Course Type: 9" SRPCC a₁=0.50
 Subbase Type: 4" B.A.M. a₂=0.28



DETOUR ROAD NO. 1



U.S.-24 (FA. ROUTE 9)
STA. 1559+41.71 TO STA. 1560+70.00



U.S.-24 (FA. ROUTE 9)
Sta. 1560+70 TO STA. 1562+11.89

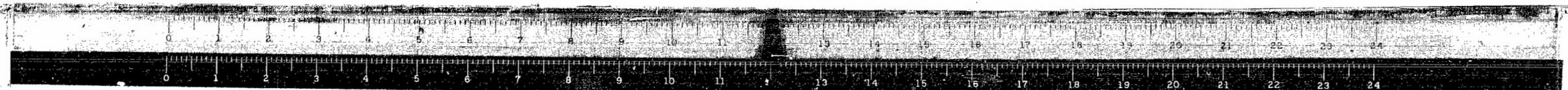
NOTE: THE SHOULDER DESIGN SHOWN ON THIS TYPICAL SECTION SHEET REFLECTS ALTERNATE A. FOR ALTERNATE B, SEE SHEET NO. 2A.

State of Illinois
Department of Transportation
District Three

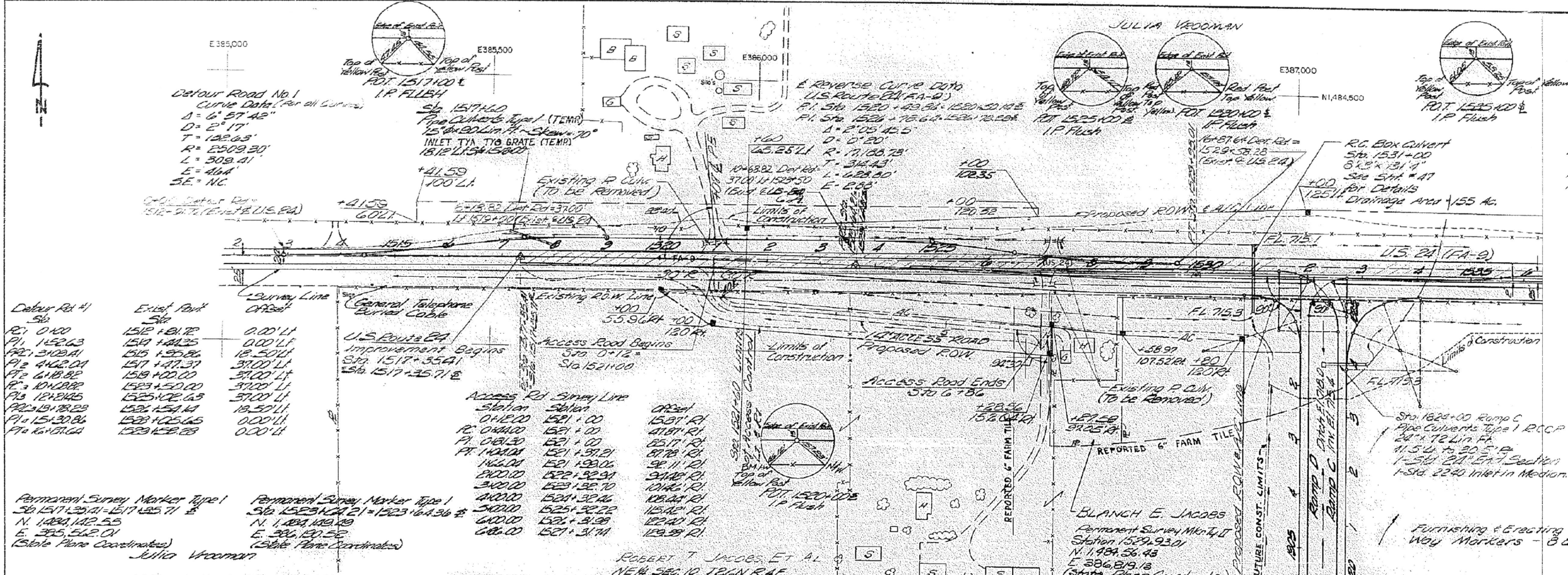
Reviewed By: _____
District Engineer of Design
Date: _____
Examined By: _____
District Engineer of Construction
District Engineer of Maintenance
District Engineer of Materials
District Engineer of Traffic

TYPICAL SECTIONS
FAI-55 SEC. 57-11B & 57-11(I)
McLEAN COUNTY

ON STANDARD 2237 ONLY
THE DETAILS FOR B.A.M.
STABILIZATION SHALL APPLY.



PROJECT NO.	23110	COUNTY	McLean	TOTAL SHEETS	68	SHEET	4
FILE NO.	PA-5560-103	DATE	1517+25.51	TO STA.	1536+00		
DATE	1517+25.51	PROJECT					



Left Turn Lane
Curve Data
PI# 100.00 Sta 1535+00.27
PI# 2 110.00 Sta 1536+14.56
D=5° 25' 24"
T=57.11'
R=105.08'
L=115.21'
E=150'

Note:
See Sht #17 for Limits
Of Construction of Ramps
On Section 57-1(1)

Detour Rd #1

Station	Exist. Point	Offset
PC 0+00	1512+81.2	0.00' LH
PI 1+52.63	1514+84.25	0.00' LH
P2 3+02.01	1515+125.86	18.50' LH
PT 4+02.01	1517+37.37	37.00' LH
P2 6+18.82	1519+40.00	37.00' LH
PC 10+00.00	1523+50.00	37.00' LH
P2 12+00.00	1525+42.63	37.00' LH
P2 14+00.00	1527+45.14	18.50' LH
PI 15+30.86	1528+42.63	0.00' LH
PT 16+51.64	1529+56.88	0.00' LH

U.S. Route 24
Improvement Begins
Sta 1517+35.41
Sta 1517+35.71 E

Station	Offset
0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00

Access Rd. Survey Line

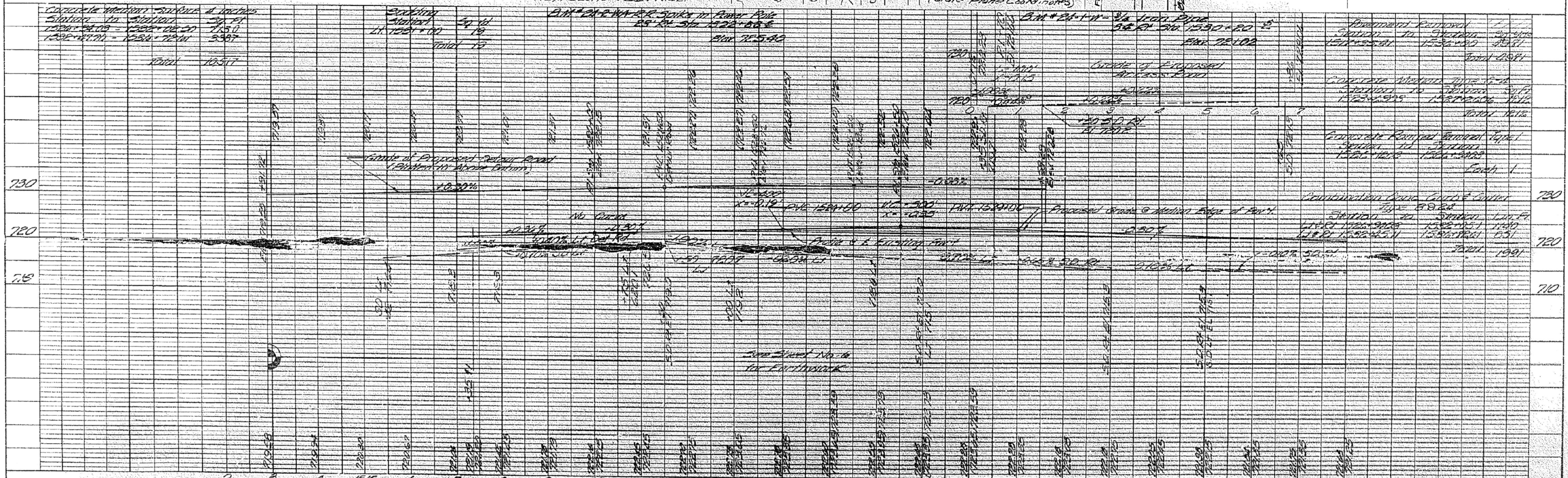
Station	Offset
0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00

Permanent Survey Marker Type 1

Station	Offset
0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00

Permanent Survey Marker Type 1

Station	Offset
0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00
PC 0+00	1521+00
PI 0+00	1521+00
P2 0+00	1521+00
PT 0+00	1521+00
P2 0+00	1521+00



PLAN	DATE
REVISIONS	
1. CHANGED	
2. CHANGED	
3. CHANGED	
4. CHANGED	
5. CHANGED	
6. CHANGED	
7. CHANGED	
8. CHANGED	
9. CHANGED	
10. CHANGED	

PROFILE	DATE
REVISIONS	
1. CHANGED	
2. CHANGED	
3. CHANGED	
4. CHANGED	
5. CHANGED	
6. CHANGED	
7. CHANGED	
8. CHANGED	
9. CHANGED	
10. CHANGED	

CHICAGO AERIAL SURVEY
10265 FRANKLIN AVE.
FRANKLIN PARK, ILLINOIS
DATE OF PHOTOGRAPHY
ELEVATIONS BASED ON MEAN SEA LEVEL DATUM

COMPILED BY STEREO-PHOTOGRAMMETRIC MEANS

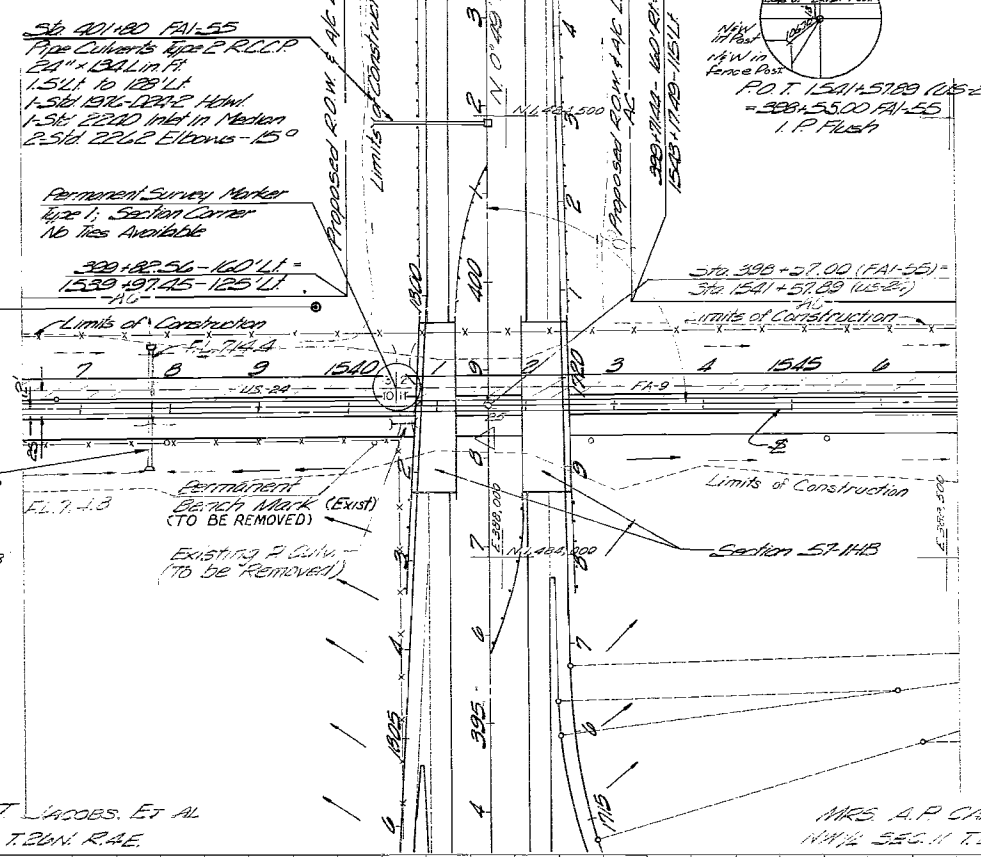
DESIGN AND PLOT NO.	BY	CHECKED	TOTAL SHEETS	SHEET NO.
PAI-55	McLEOD	McLEOD	13	5
STA.	TO STA.			
1533+85	1545+00			
S.P. & S.D. NO. 4	BLUESKY PROJECT			

SE 1/4 SEC. 3 T. 20N. R. 4E.
JULIA VROOMAN

SW 1/4 SEC. 2 T. 20N. R. 4E.
JULIA VROOMAN

ROBERT T. JACOBS, ET AL
NE 1/4 SEC. 10 T. 20N. R. 4E.

MRS. A.P. CARRICK
NW 1/4 SEC. 11 T. 20N. R. 4E.



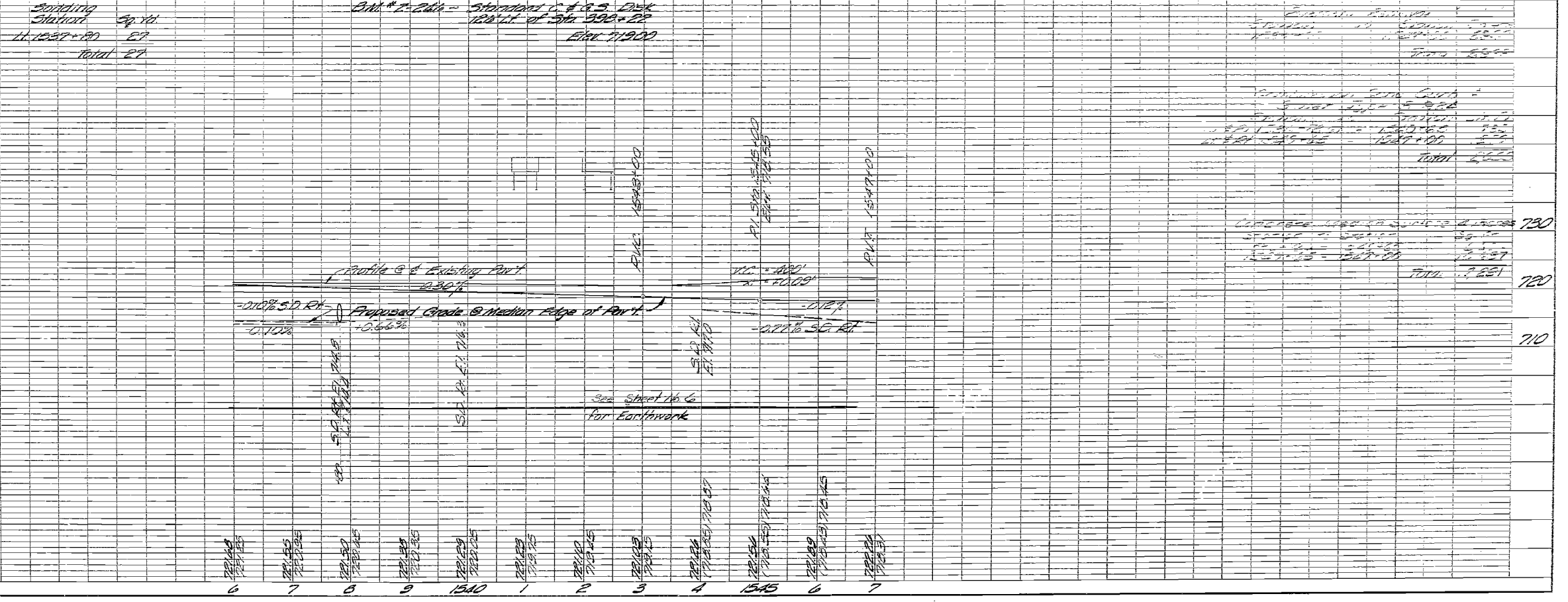
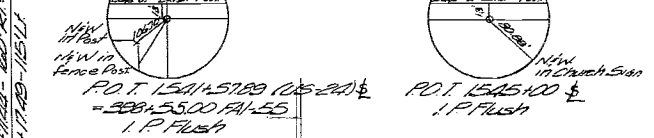
Sta 401+80 PAI-55
Pipe Culverts Type 2 RCCP
24" x 134 Lm. Ft.
1.54' to 1.82' LT
1.54' 192'-0" DEPT. Head
1.54' 222'0" Mid. in Median
2.54' 222'2" Elbows - 15'

Permanent Survey Marker
Type 1, Section Corner
No. Ties Available

300+82.56 - 160' LT.
1539+49.45 - 125' LT.
-110'

PROPOSED PERMANENT
BENCH MARK 123' LT.
STA. 1539+85 ±

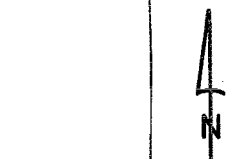
Sta 1537+80
Pipe Culverts Type 1 RCCP
36" x 132 Lm. Ft.
66' LT. to 66' RT.
2-36" End Sections
Remove Existing 36" x 61/3
Concrete Culvert
Drainage Area = 27 Ac.



PLAN	DATE
BY	
NO.	

PROFILE	DATE
BY	
NO.	

CHICAGO AERIAL SURVEY
10285 FRANKLIN AVE.
FRANKLIN PARK, ILLINOIS
DATE OF PHOTOGRAPHY
ELEVATIONS BASED ON MEAN SEA LEVEL DATUM



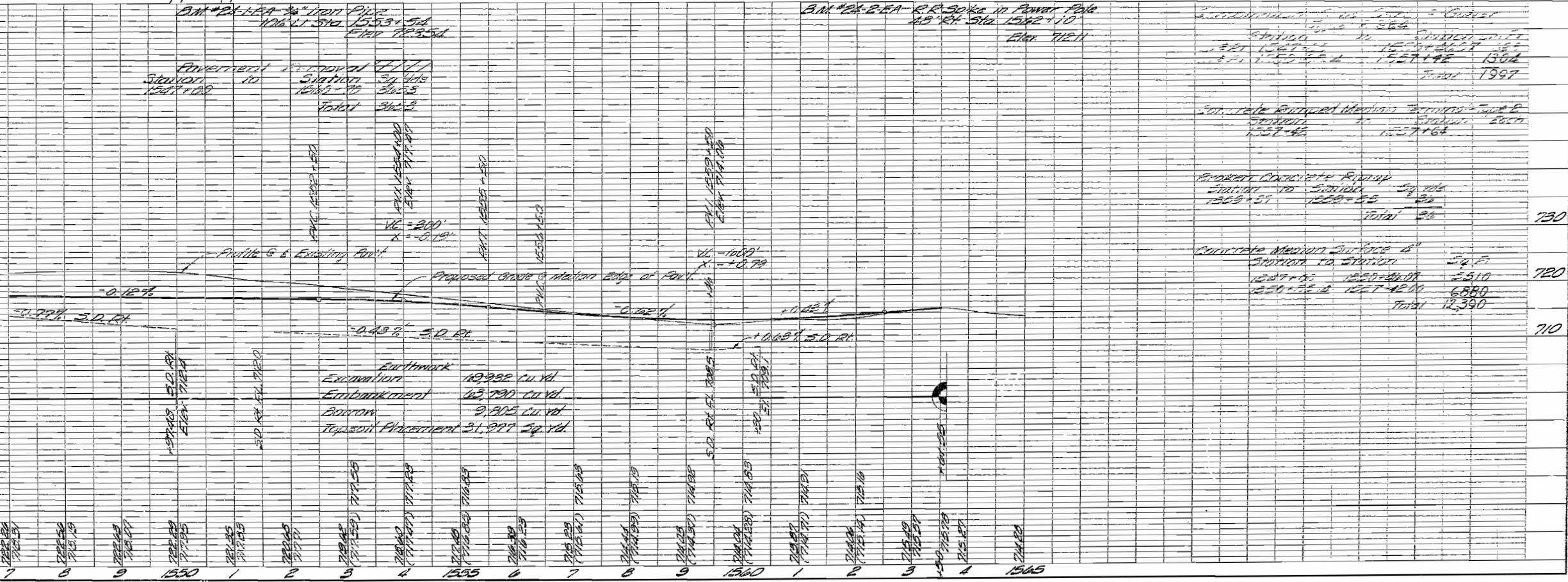
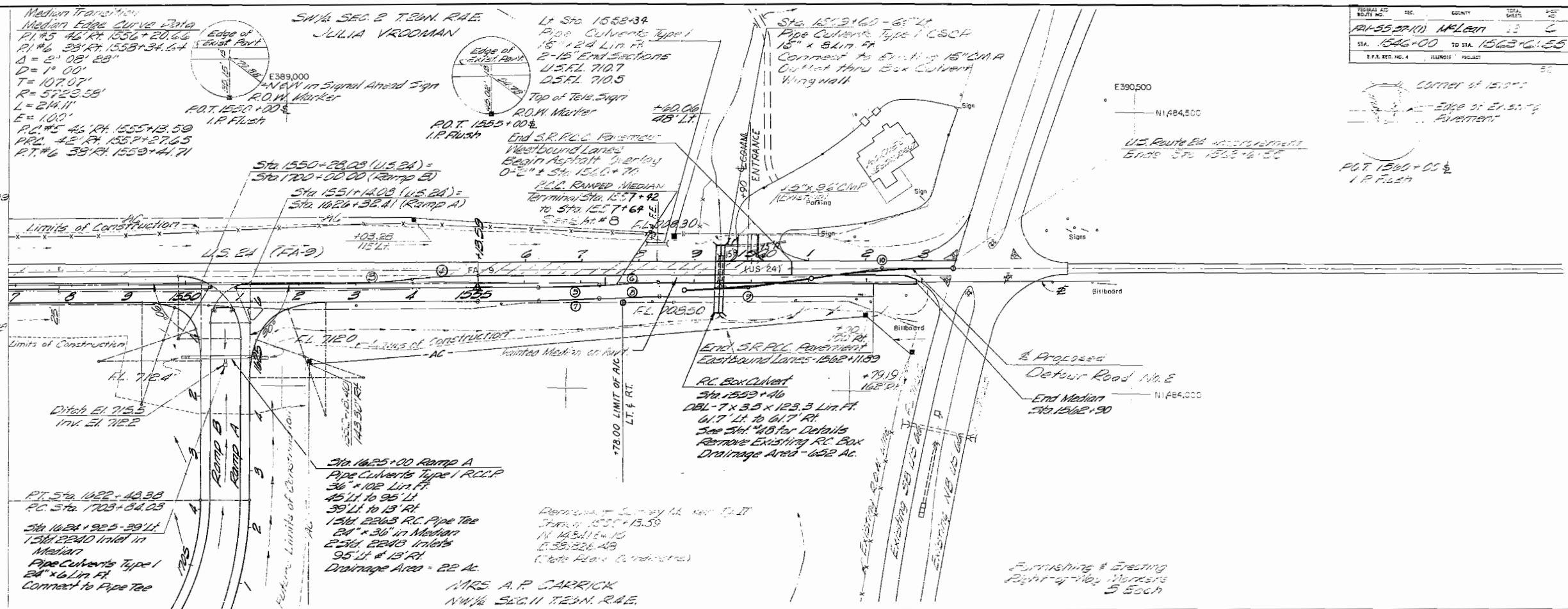
LEFT TURN LANE CURVE DATA
 PI #3 = 100' RT Sta. 1553+41.25
 PI #4 = 110' LT Sta. 1554+20.25
 $\Delta = 5^{\circ} 58' 24''$
 $D = 5^{\circ} 11' 05''$
 $T = 57.66'$
 $R = 1105.08'$
 $L = 115.21'$
 $E = 1.50'$
 PC #3 = 100' RT Sta. 1552+88.29
 PRC = 500' LT Sta. 1553+20.25
 PT #4 = 110' LT Sta. 1555+13.59

Median Transition
Median Edge Curve Data
 PI #5 = 46' RT Sta. 1556+20.66
 PI #6 = 38' RT Sta. 1558+34.64
 $\Delta = 2^{\circ} 08' 28''$
 $D = 1^{\circ} 00'$
 $T = 107.07'$
 $R = 5723.58'$
 $L = 214.11'$
 $E = 1.00'$
 PC #5 = 46' RT Sta. 1555+13.59
 PRC = 42' RT Sta. 1557+27.65
 PT #6 = 38' RT Sta. 1559+41.71

Median Transition
Outside Edge Pavement Curve Data
 PI #7 = 70' RT Sta. 1555+51.10
 PI #8 = 52' RT Sta. 1558+35.09
 $\Delta = 2^{\circ} 08' 28''$
 $D = 0^{\circ} 59' 45''$
 $T = 107.51'$
 $R = 3753.58'$
 $L = 215.00'$
 $E = 1.00'$
 PC #7 = 70' RT Sta. 1555+13.59
 PRC = 500' LT Sta. 1553+20.25
 PT #8 = 52' RT Sta. 1559+41.71

Detour Road No. 2 Curve Data
 PI #9 = 48' RT Sta. 1553+31.25
 PI #10 = 10' RT Sta. 1562+30.25
 $\Delta = 7^{\circ} 47' 35''$
 $D = 3^{\circ} 30'$
 $T = 14.50'$
 $R = 1637.02'$
 $L = 222.66'$
 $E = 3.79'$
 PC #9 = 510' RT Sta. 1558+34.47
 PRC = 3168' RT Sta. 1563+59.17

Note:
 See Sht. # 13 for Limits of Construction of Ramps at Sta. 57+1(1)



DATE	
BY	
PLAN	
SURVEYED	
NOTE BOOK	
ALIGNMENT CHECKED	
RT. OF WAY CHECKED	
NO.	

DATE	
BY	
PROFILE	
SURVEYED	
NOTE BOOK	
GRADES CHECKED	
B. M. NOTED	
STRUCTURE NOTED	
NO.	

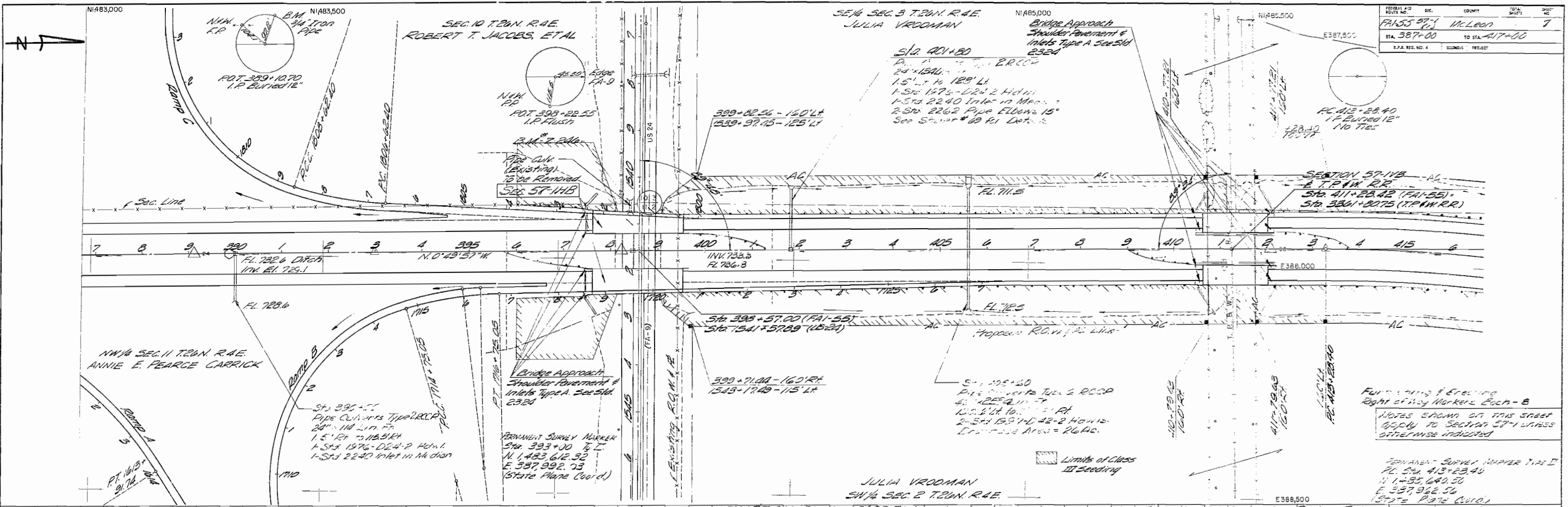
COMPILED BY STEREO-PHOTOGRAMMETRIC MEANS
 CHICAGO AERIAL SURVEY
 10265 FRANKLIN AVE.
 FRANKLIN PARK, ILL. IND. 5
 DATE OF PHOTOGRAPHING
 ELEVATION BASED ON MEAN SEA LEVEL DATUM

PLANNING BY DATE
 CHECKED BY DATE
 PLOTTED BY DATE
 NOTE BOOK NO.
 ALIGNMENT CHECKED
 RT OF WAY CHECKED

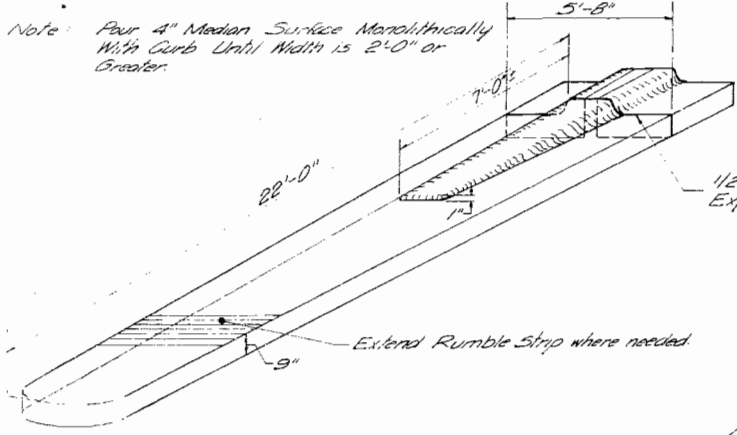
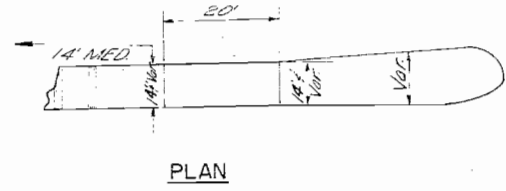
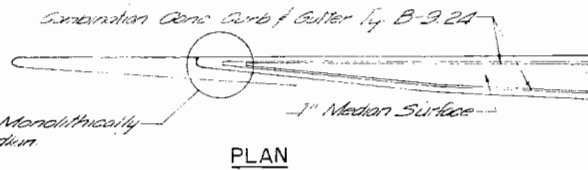
COMPILED BY STEREO-PHOTOGRAMMETRIC MEANS
 CHICAGO AERIAL SURVEY
 10265 FRANKLIN AVE.
 FRANKLIN PARK, ILL. 60064
 DATE OF PHOTOGRAPHY
 ELEVATIONS BASED ON MEAN SEA LEVEL DATUM

PROFILE SURVEY BY DATE
 NOTE BOOK NO.
 GRADES CHECKED
 B.M. NOTED
 STRUCTURE MARKS CHECKED

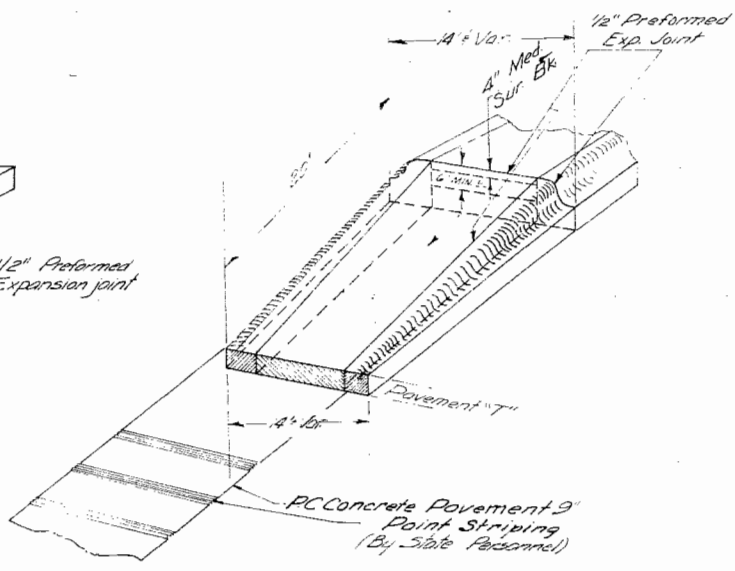
PROJECT NO.	SHEET	COUNT	TOTAL SHEETS
FA55-57-1	McLean	7	7
STA. 387+00	TO STA. 417+00		



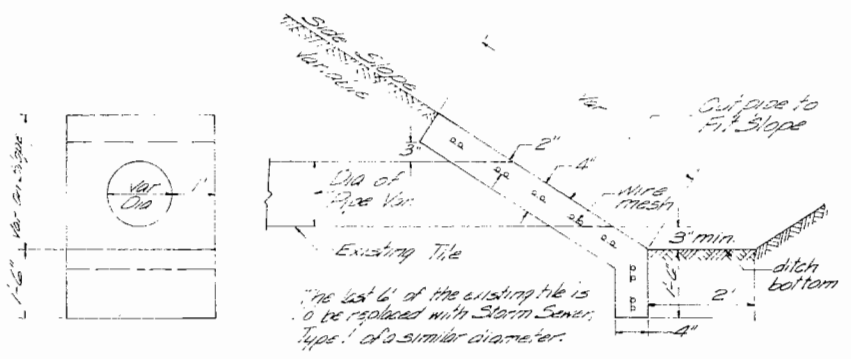
B.M. #143 - 3/4" Iron Pipe Flush with ground in north & south fence 1026-27 West of Sta. 392+10.70 Elev. 718.08			Steel Plate Beam Guard Rail - Single Rail			B.M. #2-246 - Standard C.O.S. Disk 1/2" Lt. of Sta. 398+22 Elev. 719.09			B.M. #MA-8 - RP Spike in Power Pole 260' Lt. Sta. 411+73 Elev. 719.1		
Station	To Station	Limit	Station	To Station	Limit	Station	To Station	Limit	Station	To Station	Limit
LI 396+52.3	397+52.3	150	LI 396+52.3	397+52.3	150	Med 395+77.3	396+77.3	25	LI 395+22.3	396+22.3	25
RI 396+52.3	397+52.3	25	RI 396+52.3	397+52.3	25	Med 400+26.3	401+26.3	25	RI 395+71.2	400+71.2	100
LI 397+41.3	401+41.3	1082.5	LI 397+41.3	401+41.3	1082.5	Med 400+26.3	401+26.3	25	RI 401+81.6	417+81.6	300
Med 397+41.3	401+41.3	150	Med 397+41.3	401+41.3	150	Med 400+26.3	401+26.3	25	RI 411+81.6	417+81.6	300
RI 397+41.3	401+41.3	1082.5	RI 397+41.3	401+41.3	1082.5	Med 413+35.3	413+35.3	150			
Med 397+41.3	401+41.3	150	Med 397+41.3	401+41.3	150	Total		100			
LI 402+13.3	417+00	486.7	LI 402+13.3	417+00	486.7						
Med 402+13.3	417+00	150	Med 402+13.3	417+00	150						
RI 402+13.3	417+00	486.7	RI 402+13.3	417+00	486.7						
Med 402+13.3	417+00	150	Med 402+13.3	417+00	150						
LI 412+13.3	417+00	486.7	LI 412+13.3	417+00	486.7						
Med 412+13.3	417+00	150	Med 412+13.3	417+00	150						
RI 412+13.3	417+00	486.7	RI 412+13.3	417+00	486.7						
Med 412+13.3	417+00	150	Med 412+13.3	417+00	150						



P.C.C. RAMPED MEDIAN TERMINAL - TYPE 1
± 13.0 C. 105

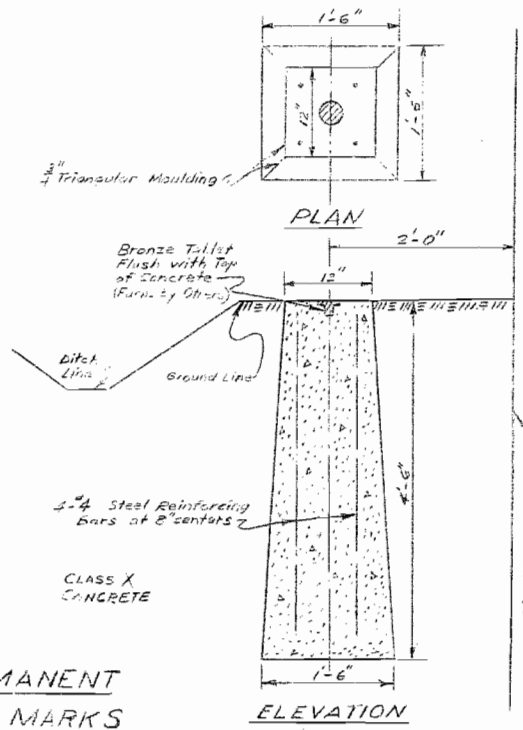


P.C.C. RAMPED MEDIAN TERMINAL - TYPE 2
± 13.0 C. 105



SPECIAL SLOPE HEADWALL

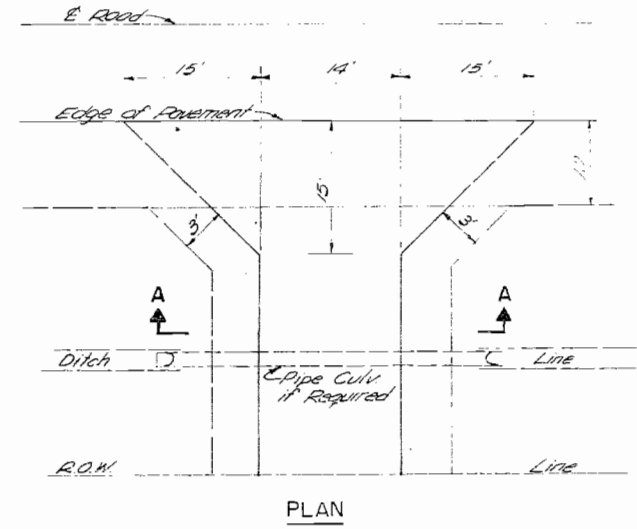
At locations where an existing tile empties into a ditch.



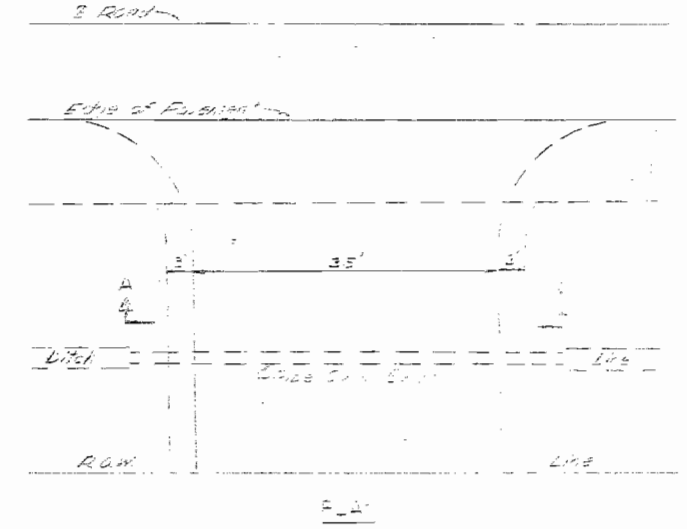
PERMANENT BENCH MARKS

123' Lt Sta. 1538-R52

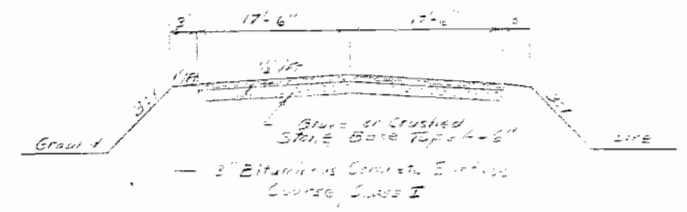
METHOD OF RESETTING U.S. GEOLOGICAL SURVEY AND U.S. COAST AND GEODETIC SURVEY BENCHMARKS



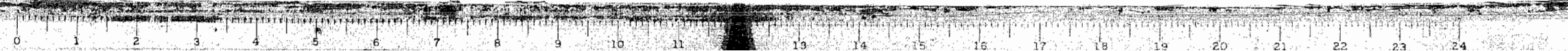
DETAIL OF FIELD AND PRIVATE ENTRANCE SECTION A-A



DETAIL OF COMMERCIAL ENTRANCE SECTION A-A



MISCELLANEOUS DETAILS
FAI ROUTE 55 SEC 5
McLEAN COUNTY



OFFSETS FROM & MEDIAN

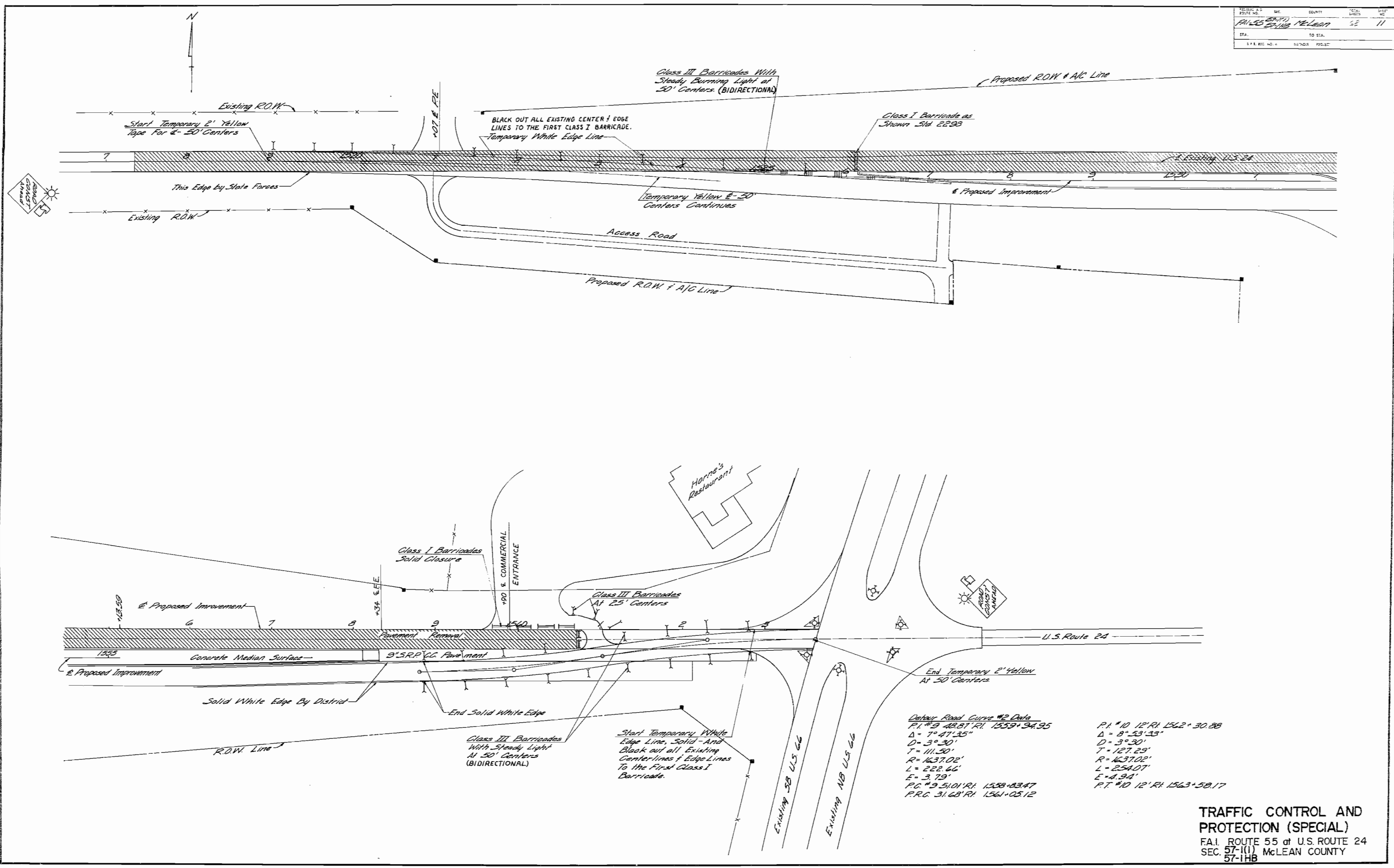
OFFSETS FROM & MEDIAN

FAI-55 57-113 57-114 McLEAN CO 3/11/10

Control Point	Dist	Sta	Edge Pavt.	Elev.	Median Elev.	Island Elev.	Left Elev.	Right Island Elev.	Median Elev.	Edge Pavt.	Elev.
		1538+75	3500	720.07		11.00	720.43	11.00	720.43	3500	720.07
		1539+00		719.99			720.35		720.35		719.99
		+25		719.92			720.28		720.28		719.92
		+50		719.84			720.20		720.20		719.84
		+75		719.77			720.13		720.13		719.77
		1540+00		719.69			720.05		720.05		719.69
		+25		719.62			719.98		719.98		719.62
		+50		719.54			719.90		719.90		719.54
		+75		719.47			719.83		719.83		719.47
		1541+00		719.39			719.75		719.75		719.39
		+25		719.32			719.68		719.68		719.32
		+50		719.24			719.60		719.60		719.24
		+75		719.17			719.53		719.53		719.17
		1542+00		719.09			719.45		719.45		719.09
		+25		719.02			719.38		719.38		719.02
		+50		718.94			719.30		719.30		718.94
		+75		718.87			719.23		719.23		718.87
		1543+00		718.79			719.15		719.15		718.79
		+25		718.72			719.08		719.08		718.72
		+50		718.65			719.01		719.01		718.65
		+75		718.58			718.94		718.94		718.58
		1544+00		718.51			718.87		718.87		718.51
		+25		718.45			718.81		718.81		718.45
		+50		718.39			718.75		718.75		718.39
		+75		718.33			718.69		718.69		718.33
		1545+00		718.28			718.64		718.64		718.28
		+25		718.23			718.59		718.59		718.23
		+50		718.18			718.54		718.54		718.18
		+75		718.14			718.50		718.50		718.14
		1546+00		718.09			718.45		718.45		718.09
		+25		718.05			718.41		718.41		718.05
		+50		718.02			718.38		718.38		718.02
		+75		717.98			718.34		718.34		717.98
		1547+00		717.95			718.31		718.31		717.95
		+25		717.92			718.28		718.28		717.92
		+50		717.89			718.25		718.25		717.89
		+75		717.86			718.22		718.22		717.86
		1548+00		717.83			718.19		718.19		717.83
		+25		717.80			718.16		718.16		717.80
		+50		717.77			718.13		718.13		717.77
		+75		717.74			718.10		718.10		717.74
		1549+00		717.71			718.07		718.07		717.71
		+25		717.68			718.04		718.04		717.68
		+50		717.65			718.01		718.01		717.65
		+75		717.62			717.98		717.98		717.62
		+8808		717.60	11.00	717.96	11.00	717.96			717.60
		1550+00		717.59		10.35	717.95		717.95		717.59
		+25		717.56		4.61	717.92		717.92		717.56
		+42.57		717.54		-3.45	717.90		717.90		717.54
		+44.36		717.54		-4.49	718.00		717.90		717.54
		+50		717.53			717.89		717.89		717.53
		+75		717.50			717.86		717.86		717.50
		+87.64		717.48		-6.18	717.91		717.84		717.48
		+88.53		717.48		-5.81	717.92		717.84		717.48
		1551+00		717.47		-2.44	717.96		717.83		717.47
		+1408		717.45		-1.00	717.99		717.81		717.45
		+25		717.44			717.98		717.80		717.44
		+50		717.41			717.95		717.77		717.41
		+75		717.38			717.92		717.74		717.38
		1552+00		717.35			717.89		717.71		717.35
		+25		717.32			717.86		717.68		717.32
		+50		717.29			717.83		717.65		717.29
		+75		717.24			717.80		717.61		717.24
PC		+83.59		717.23		-1.00	717.79		717.60		717.23
		1553+00	3500	717.21	11.00	717.58	-0.87	717.77	11.00	717.58	717.21

Control Point	Dist.	Sta.	Edge Pavt.	Elev.	Median Elev.	Island	Left Elev.	Right Island Elev.	Median Elev.	Edge Pavt.	Elev.	
		1553+25	3500	717.14	11.00	717.51	-0.22	717.69	11.00	717.51	3500	717.14
		+50		717.07		717.45	1.00	717.61		717.45		717.07
		+75		717.00		717.37	3.79	717.45		717.37		717.00
ARC		+83.59		716.91		717.29	5.00	717.38		717.29		716.91
		1554+00		716.91		717.28	5.14	717.39		717.28		716.91
		+25		716.81		717.18	7.44	717.24		717.18		716.81
		+50		716.70		717.08	9.16	717.11		717.08		716.70
		+75		716.58		716.96	10.92	717.00		716.96		716.58
		1555+00		716.46		716.83	10.91	716.83		716.83		716.46
		1555+859	3500	716.38	11.00	716.75	11.00(LT)	716.75	11.00	716.75	3500	716.38
				OFFSETS FROM LL EDGE PAVEMENT								
L+1/4 PT PC		1555+13.59	0	716.38		2400	716.75	4600	716.75		7000	716.38
		+25		716.31			716.69	4599	716.69		6999	716.31
		+50		716.17			716.54	4588	716.54		6988	716.17
		+75		716.01			716.39	4567	716.39		6967	716.01
		1556+00		715.86			716.23	4535	716.23		6935	715.86
		+25		715.70			716.08	4492	716.08		6892	715.70
		+50		715.55			715.92	4437	715.92		6839	715.55
		+75		715.40			715.77	4372	715.77		6773	715.40
		1557+00		715.26			715.63	4297	715.63		6698	715.26
		+25		715.13			715.50	4210	715.50		6611	715.13
		+27.65		715.12			715.49	4200	715.49		6600	715.12
		+50		715.01			715.39	4121	715.39		6522	715.01
		+75		714.91			715.28	4042	715.28		6444	714.91
		1558+00		714.82			715.19	3975	715.19		6376	714.82
		+25		714.73			715.10	3919	715.10		6319	714.73
		+50		714.66			715.03	3873	715.03		6274	714.66
		+75		714.59			714.97	3839	714.97		6239	714.59
		1559+00		714.55			714.92	3815	714.92		6215	714.55
		+25		714.50			714.88	3802	714.88		6202	714.50
PT		+41.71	0	714.48		2400	714.86	3800	714.86		6200	714.48

STATIONS & OFFSETS
FAI ROUTE 55
AT U.S. ROUTE 24
SECTION 57-1(I), 57-IHB
McLEAN COUNTY

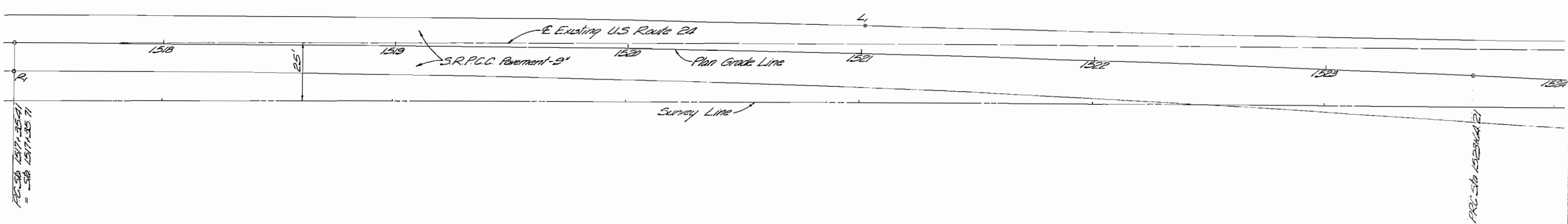


Detour Road Curve #2 Data
 P.I. #9 48.87 R.I. 1559+34.95
 $\Delta = 7^{\circ} 47' 35''$
 $D = 3^{\circ} 30'$
 $T = 111.50'$
 $R = 1637.02'$
 $L = 222.66'$
 $E = 3.79'$
 P.C. #9 51.01 R.I. 1558+83.47
 P.R.C. 31.68 R.I. 1561+05.12

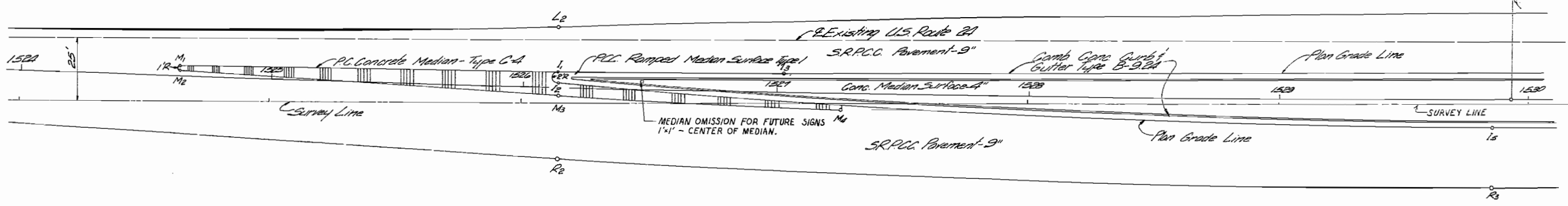
P.I. #10 12' R.I. 1562+30.88
 $\Delta = 8^{\circ} 53' 33''$
 $D = 3^{\circ} 30'$
 $T = 127.25'$
 $R = 1637.02'$
 $L = 254.07'$
 $E = 4.94'$
 P.T. #10 12' R.I. 1563+58.17

TRAFFIC CONTROL AND PROTECTION (SPECIAL)
 FAI ROUTE 55 at U.S. ROUTE 24
 SEC. 57-1(1) MCGLEAN COUNTY
 57-1HB

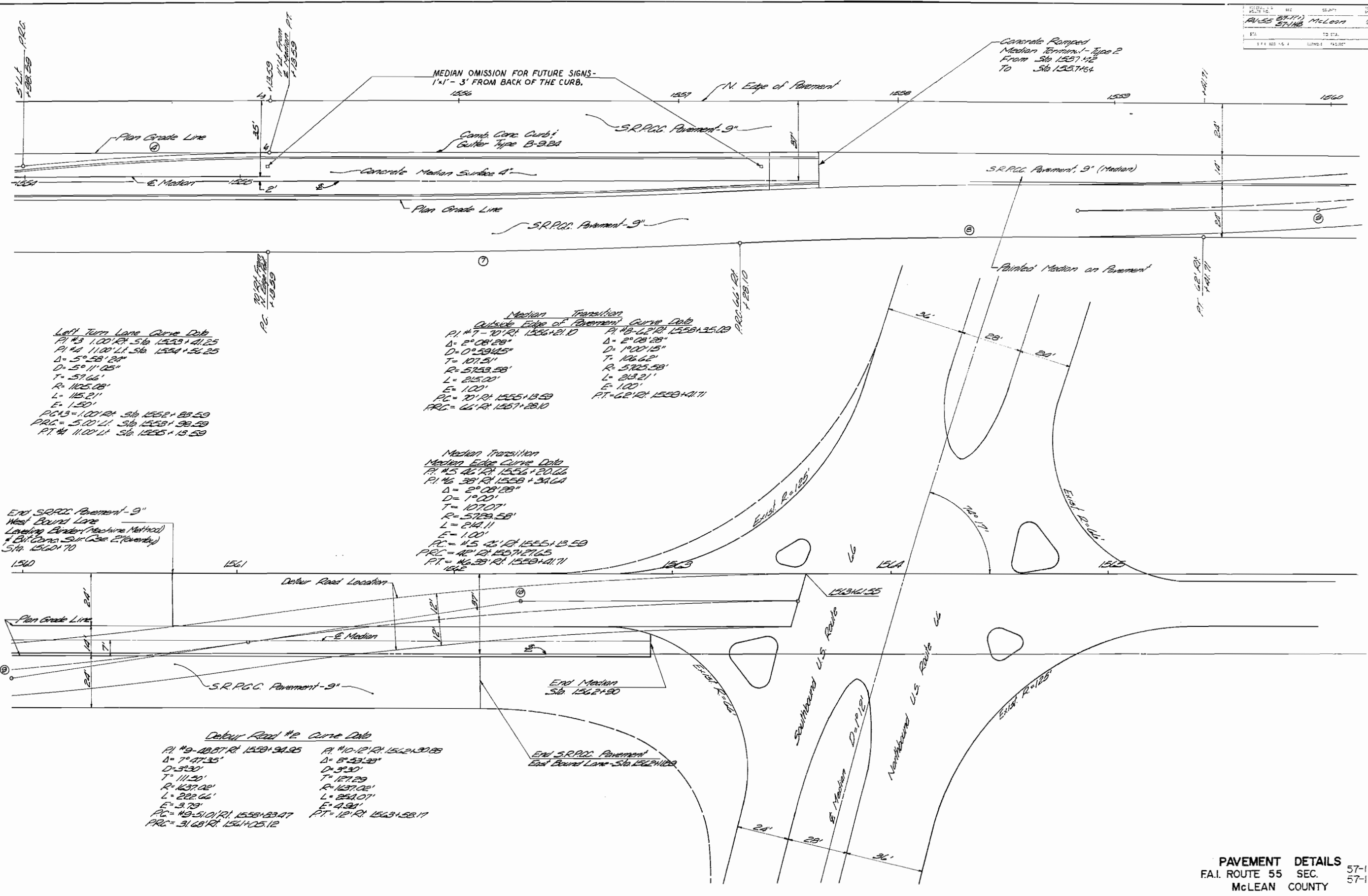
TOTAL AS	DATE	COUNT	DATE	DATE
REVISED	BY			
FAI-55-24-101	McLean	35	12	
STA.	TO STA.			
1+1.00	1+1.00			



Note:
 Channelization Approach is adjusted to
 Reverse curve centerline.
 Control points are as shown.
 Curves are not simple circular.
 See table of offsets for layout.



PAVEMENT DETAILS
 FAI. ROUTE 55 SEC. 57-1(1)
 MCLEAN COUNTY 57-1HB



Left Turn Lane Curve Data
 PI #3 1.00' RT Sta. 1553+41.25
 PI #4 11.00' LT Sta. 1554+56.25
 $\Delta = 5^{\circ} 58' 24''$
 $D = 5^{\circ} 11' 05''$
 $T = 57.66'$
 $R = 1025.08'$
 $L = 115.21'$
 $E = 1.50'$
 PCH = 1.00' RT Sta. 1552+89.59
 PRC = 5.00' LT Sta. 1553+98.59
 PT #1 11.00' LT Sta. 1555+13.59

Median Transition Outside Edge of Pavement Curve Data
 PI #7 20' RT Sta. 1556+21.10
 $\Delta = 2^{\circ} 08' 28''$
 $D = 0^{\circ} 59' 45''$
 $T = 107.51'$
 $R = 5723.58'$
 $L = 215.00'$
 $E = 1.00'$
 PC = 20' RT Sta. 1555+13.59
 PRC = 66' RT Sta. 1557+28.10

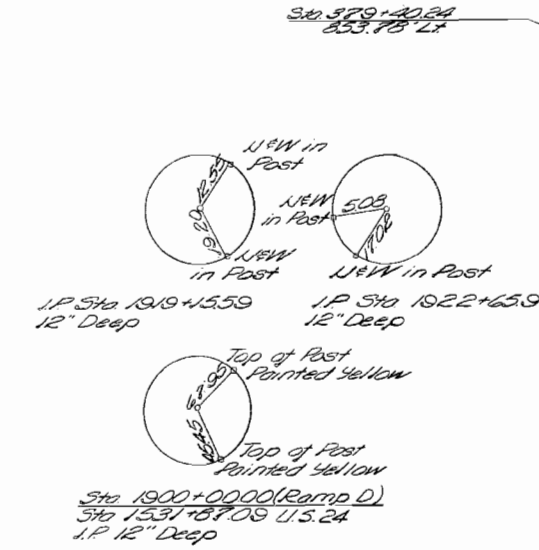
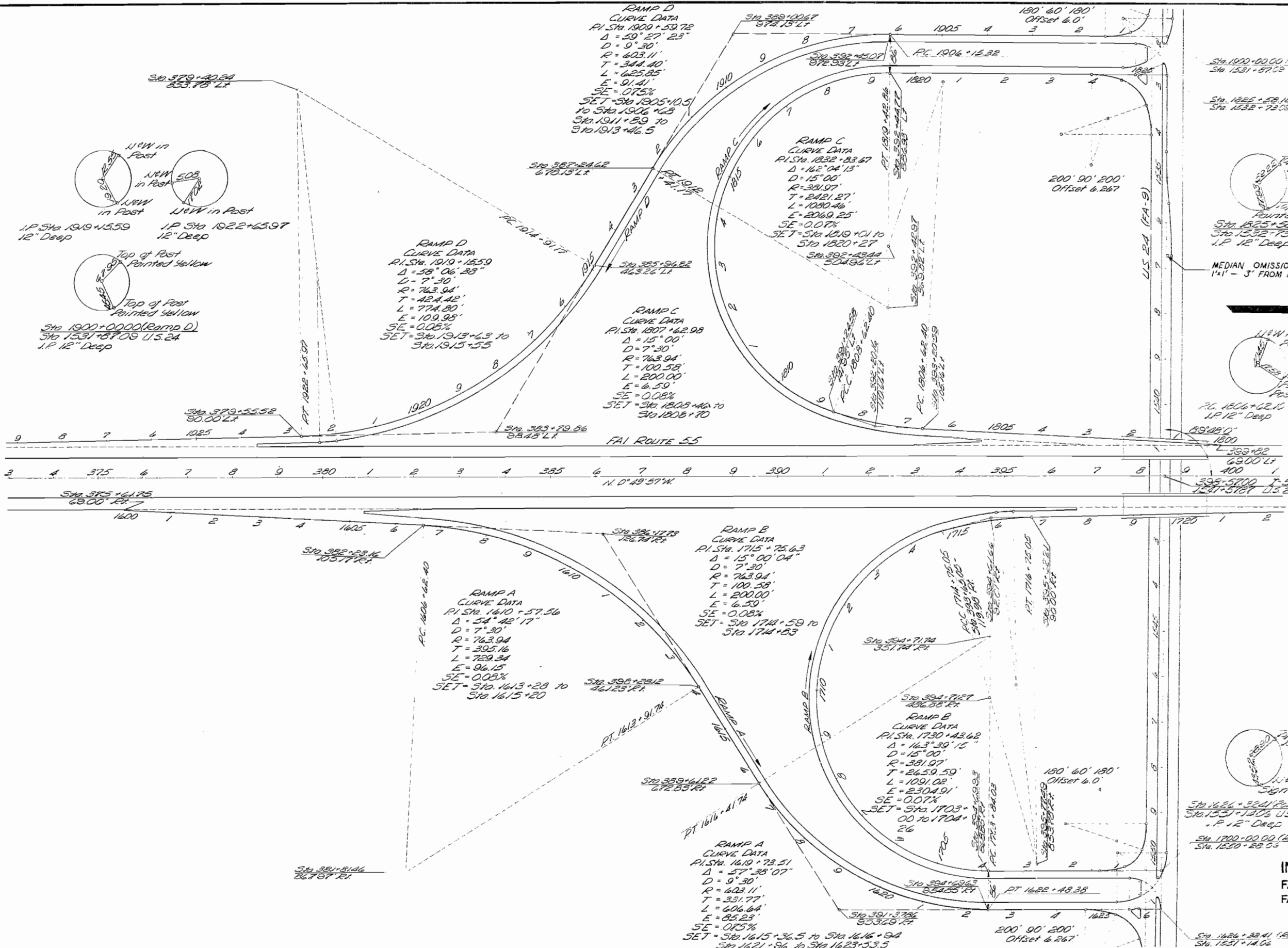
Median Transition Inside Edge of Pavement Curve Data
 PI #8 62' RT Sta. 1559+35.09
 $\Delta = 2^{\circ} 08' 28''$
 $D = 1^{\circ} 00' 15''$
 $T = 126.62'$
 $R = 5723.58'$
 $L = 233.21'$
 $E = 1.00'$
 PT = 62' RT Sta. 1559+41.71

Median Transition Median Edge Curve Data
 PI #5 46' RT Sta. 1556+20.66
 PI #6 38' RT Sta. 1558+34.64
 $\Delta = 2^{\circ} 08' 28''$
 $D = 1^{\circ} 00' 15''$
 $T = 107.07'$
 $R = 5723.58'$
 $L = 214.11'$
 $E = 1.00'$
 PC = 45' RT Sta. 1555+13.59
 PRC = 40' RT Sta. 1557+27.65
 PT = 46' RT Sta. 1559+41.71

End S.R.P.C.C. Pavement - 9"
 West Bound Lane
 Leveling Boarder Measure Method
 # Bull Corner Sur. Case 2 (overlapping)
 Sta. 1560+70

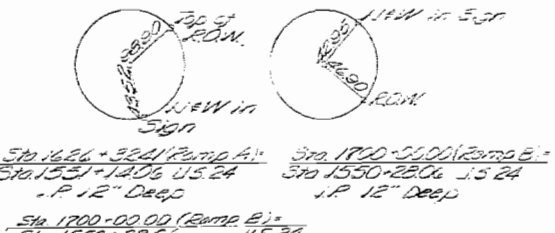
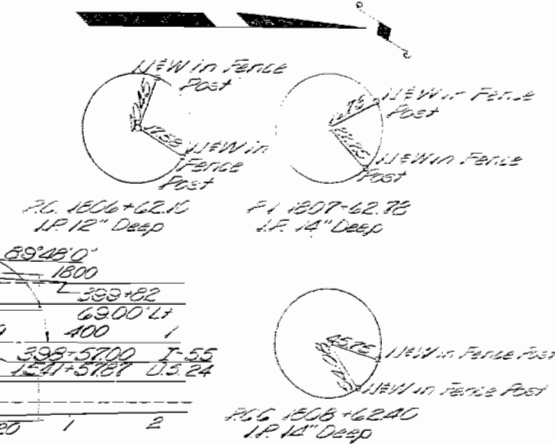
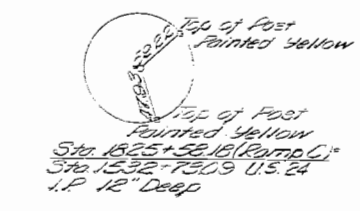
Debur Road #2 Curve Data

PI #9 48.87' RT Sta. 1559+94.95	PI #10 12' RT Sta. 1562+30.88
$\Delta = 7^{\circ} 47' 35''$	$\Delta = 8^{\circ} 53' 33''$
$D = 3^{\circ} 30'$	$D = 3^{\circ} 30'$
$T = 111.50'$	$T = 127.29'$
$R = 1637.02'$	$R = 1637.02'$
$L = 222.66'$	$L = 250.07'$
$E = 3.79'$	$E = 4.91'$
PC = 49' 51.01' RT Sta. 1558+83.47	PT = 12' RT Sta. 1563+58.17
PRC = 31.68' RT Sta. 1561+05.12	



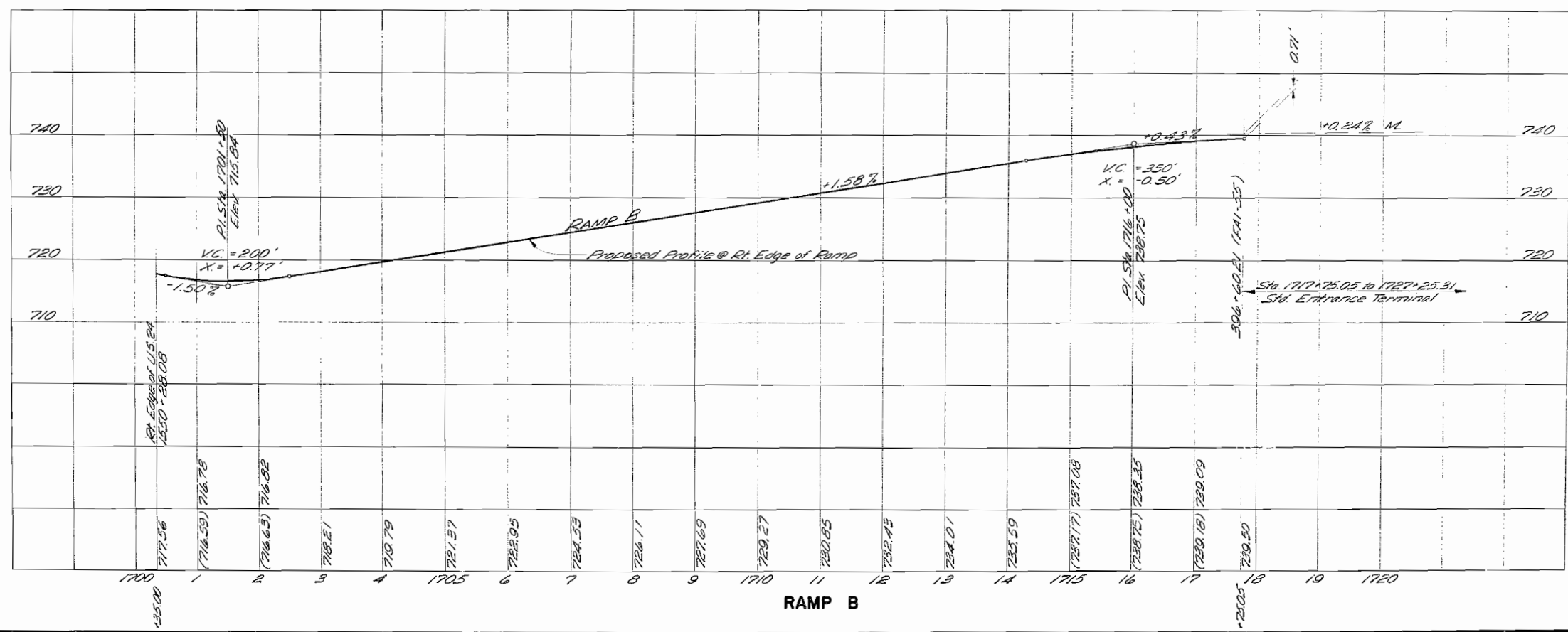
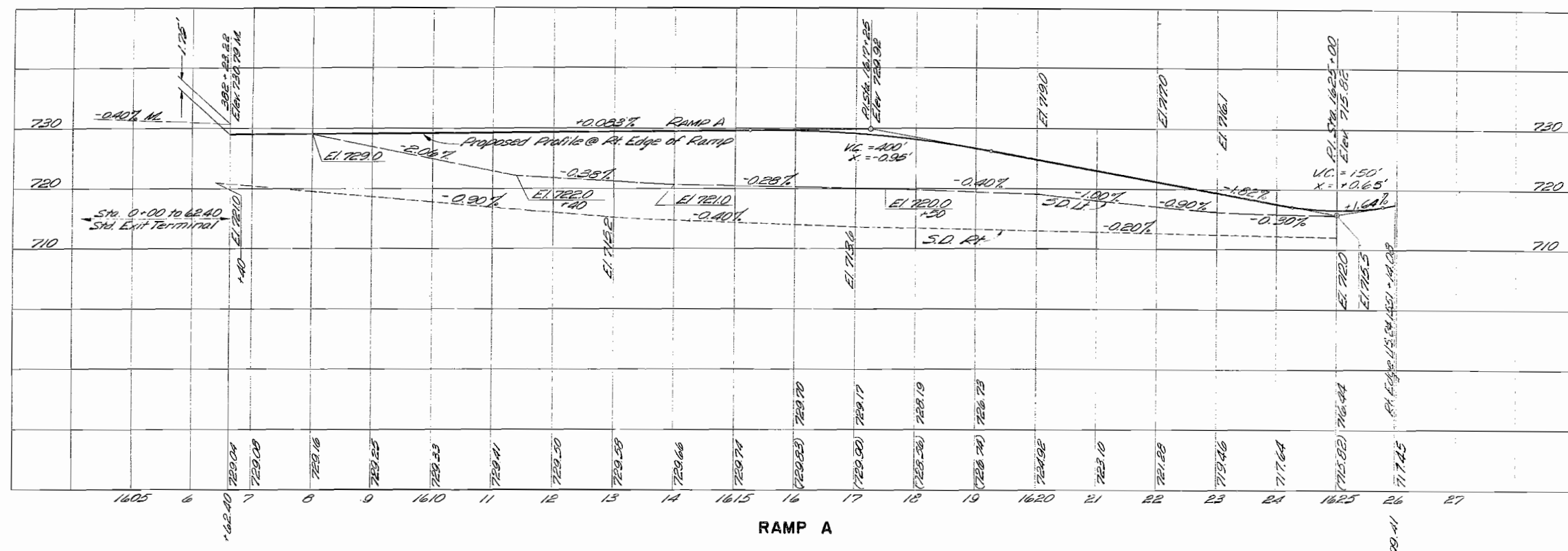
Sta. 1900+00.00 (Ramp D) =
Sta. 1531+87.09 U.S. 24

Sta. 1922+65.97 (Ramp C) =
Sta. 1532+75.09 U.S. 24

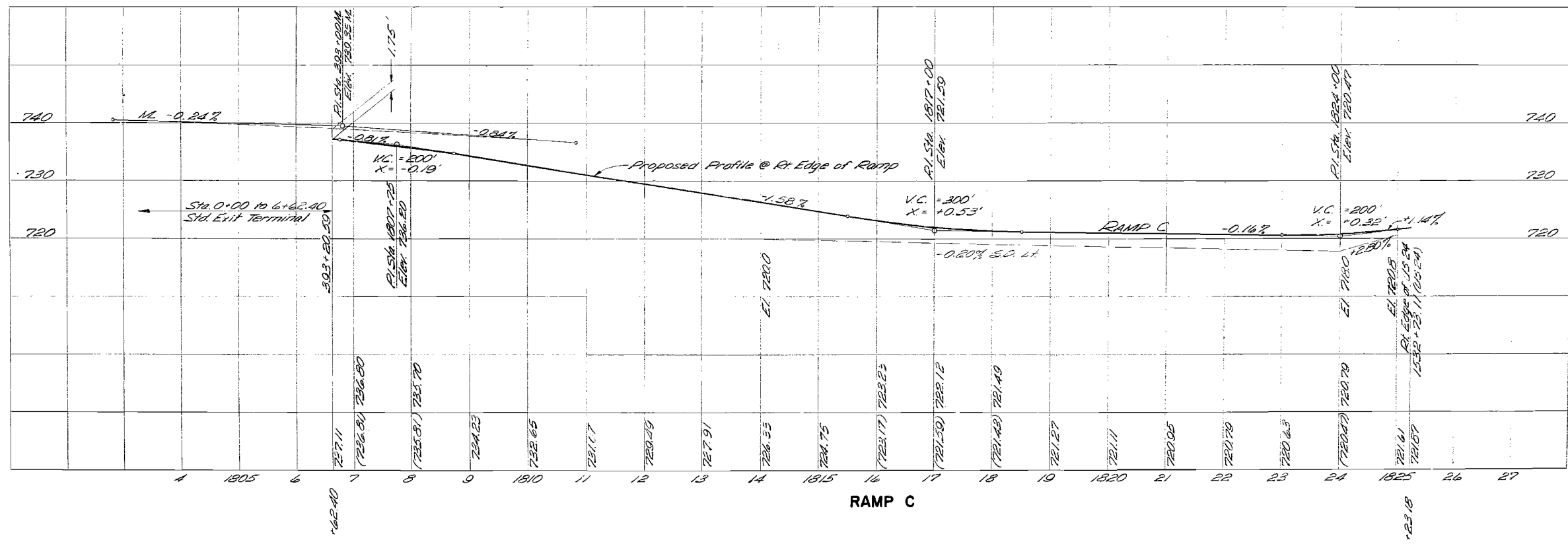
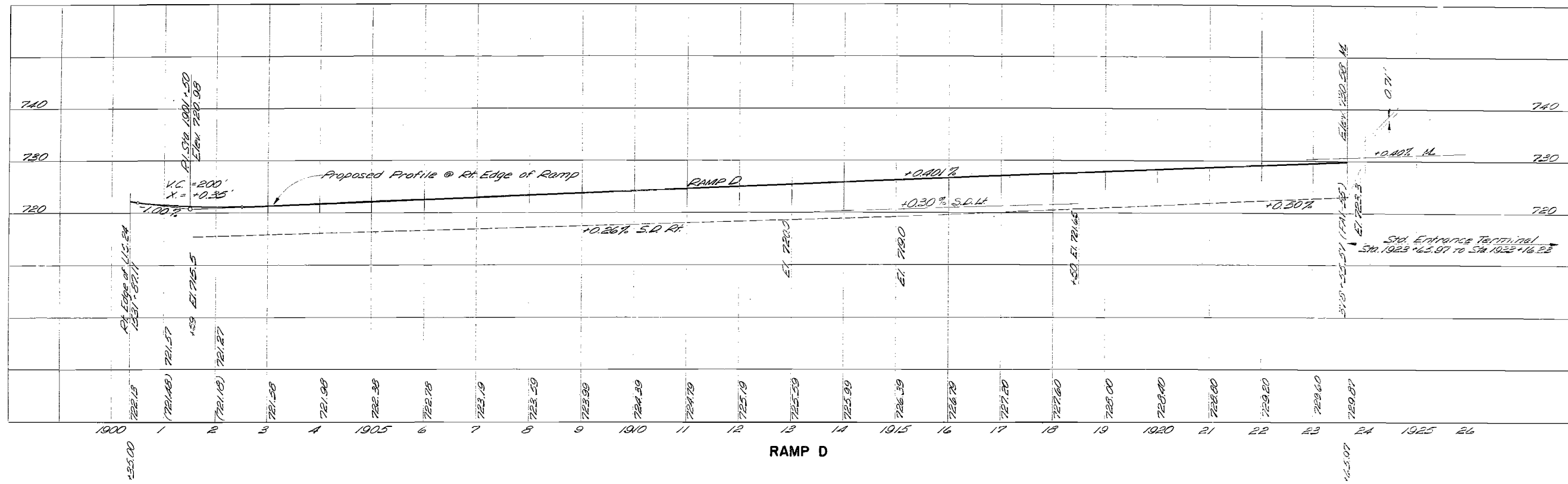


INTERCHANGE LAYOUT
FAI-55 AT U.S.-24
FAI ROUTE 55 SECTION 57-1(1)
MCLEAN COUNTY 57-11B

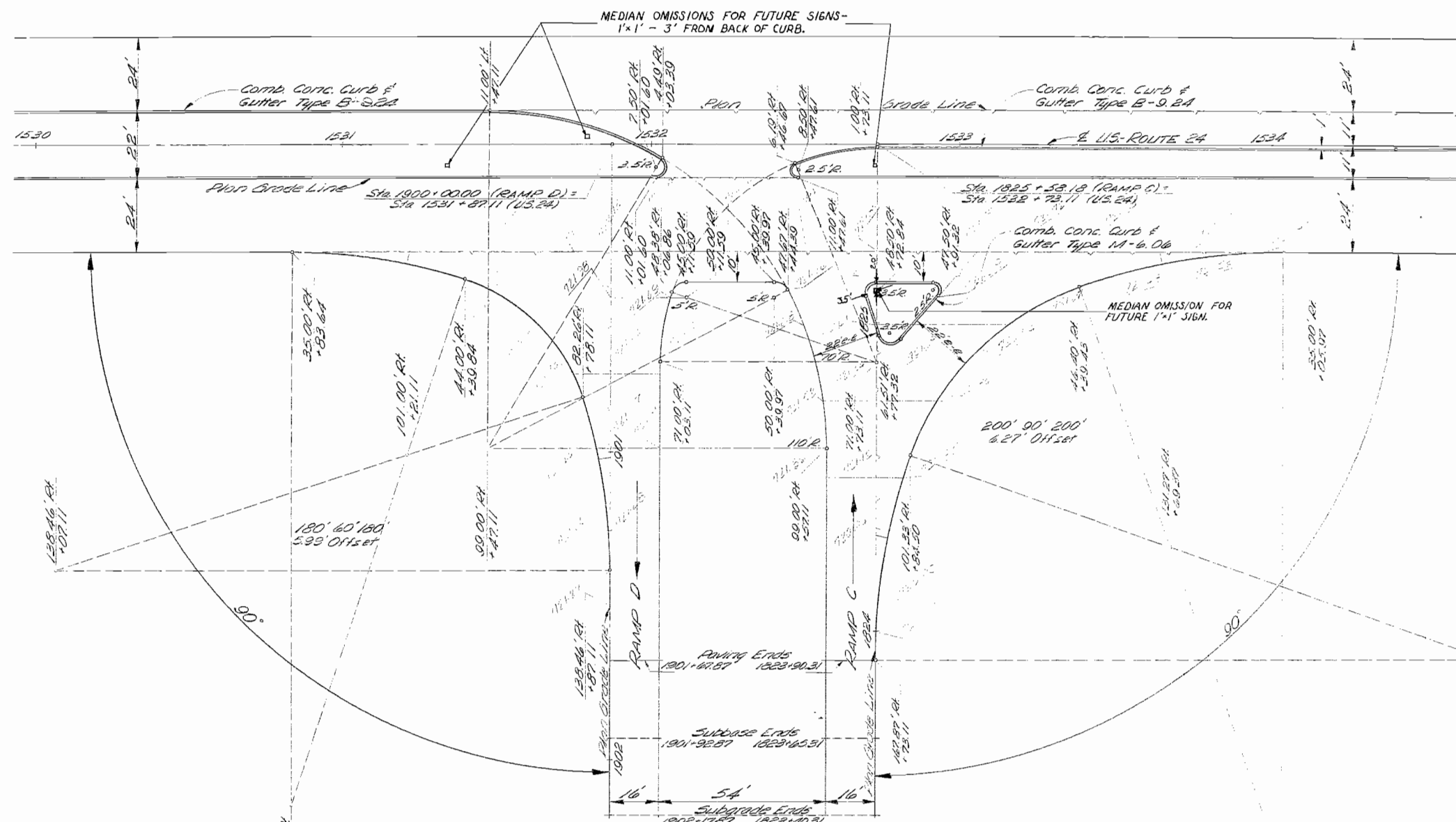
Sta. 1626+32.41 (Ramp A) =
Sta. 1531+14.06 U.S. 24



RAMP PROFILES
CHENOA INTERCHANGE
 FAI ROUTE 55 SECTION 57-1(1)
 McLEAN COUNTY 57-1HB

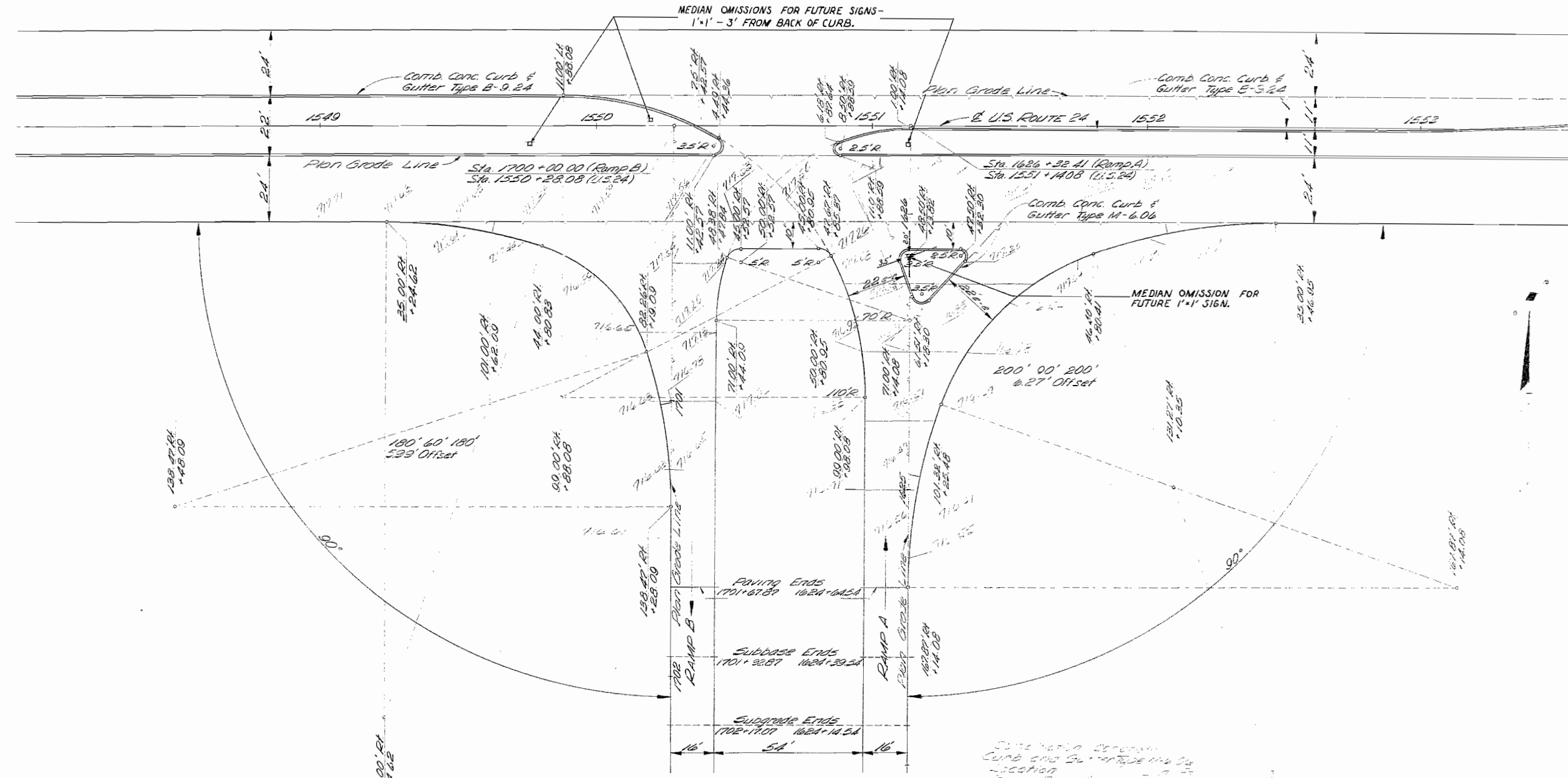


RAMP PROFILES
CHENOA INTERCHANGE
 FAI ROUTE 55 SECTION 57-1(1)
 McLEANS COUNTY 57-1HB



Concrete Median Surface Area
 Location 54.77
 Island Ramp C 250
 Total 250

INTERSECTION DETAILS
U.S. ROUTE 24 WITH RAMPS
 FAI ROUTE 55 SECTION 57-1(1)
 MCLEAN COUNTY 57-1HB
 SCALE: 1" = 20'



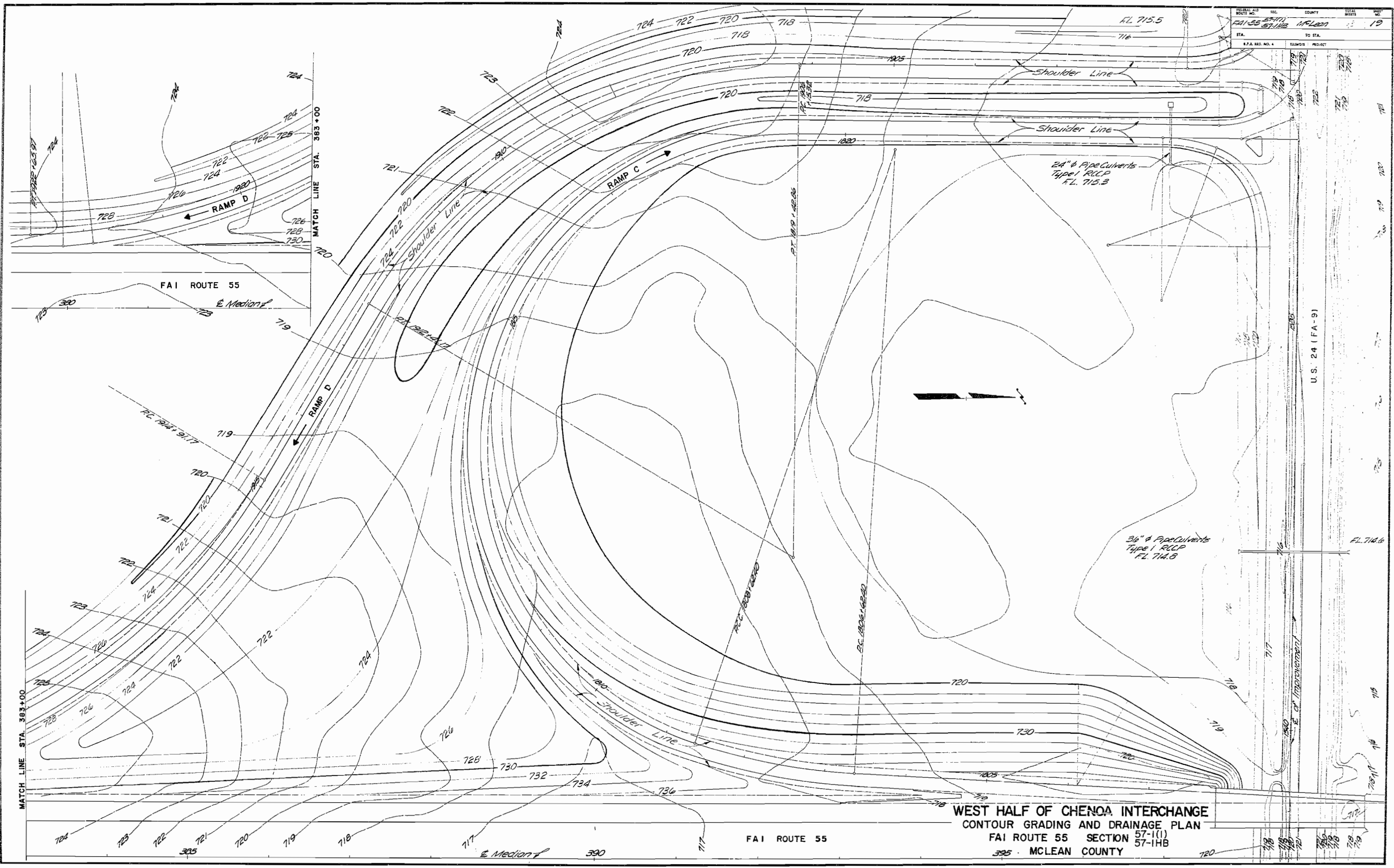
Note: Stations and Offsets are from E. U.S. 24

Concrete Median Surface Area
Location
Island Ramp A
Total

59.13
250
250

INTERSECTION DETAILS
U.S. ROUTE 24 WITH RAMPS
 ROUTE SECTION 57-1(1)
 McLEAN COUNTY 57-1HB
 SCALE 1" = 20'

FEDERAL AID ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS
FAI-55	57-11B	McLean	19
STA.	TO STA.		
87.4	87.4		
SHEET NO.	PROJECT		
4	WEST HALF OF CHENOA INTERCHANGE		



WEST HALF OF CHENOA INTERCHANGE
 CONTOUR GRADING AND DRAINAGE PLAN
 FAI ROUTE 55 SECTION 57-1(1)
 57-11B
 395 · MCLEAN COUNTY

U.S. 24 (FA-9)

FAI ROUTE 55
E. Median

FAI ROUTE 55

MATCH LINE STA. 383+00

MATCH LINE STA. 383+00

720

715

716

717

718

719

720

721

722

723

724

725

726

727

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729

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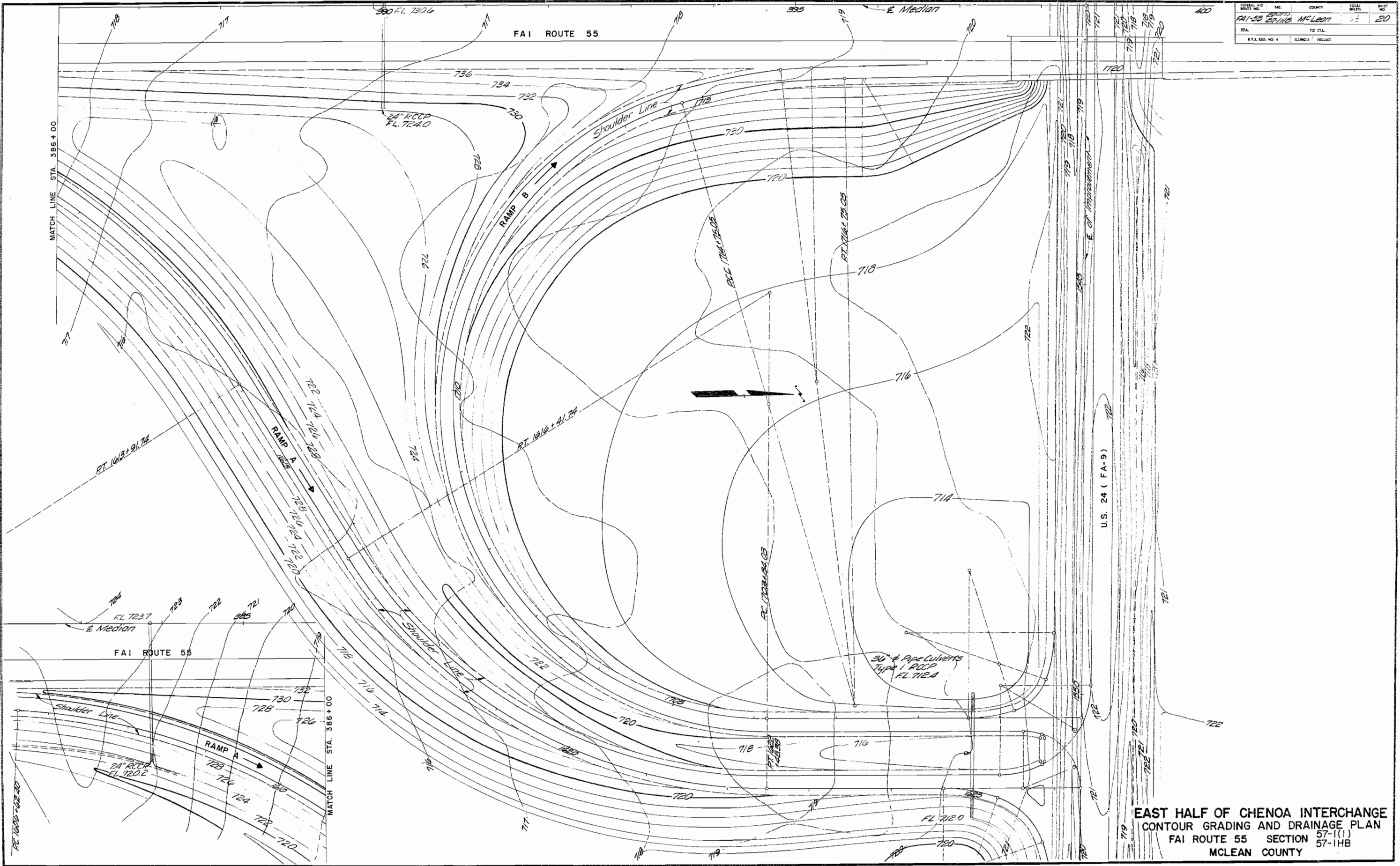
797

798

799

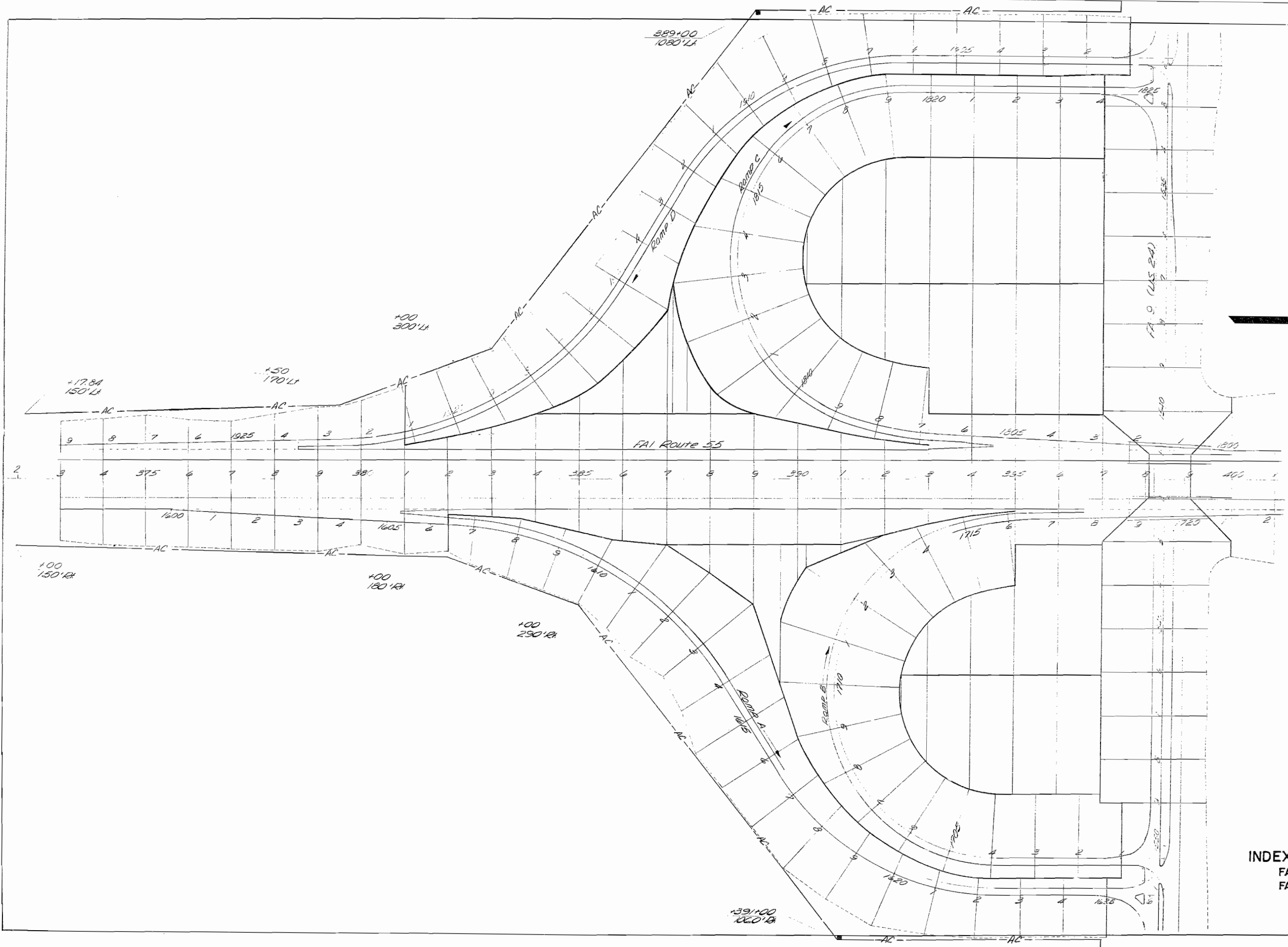
800

FEDERAL AID DISTRICT NO.	STATE	COUNTY	SHEET NO.	TOTAL SHEETS
FAI-55	ILLINOIS	MCLEAN	20	20
STA.	TO STA.		PROJECT	



EAST HALF OF CHENOA INTERCHANGE
CONTOUR GRADING AND DRAINAGE PLAN
 FAI ROUTE 55 SECTION 57-1(1)
 MCLEAN COUNTY 57-1HB

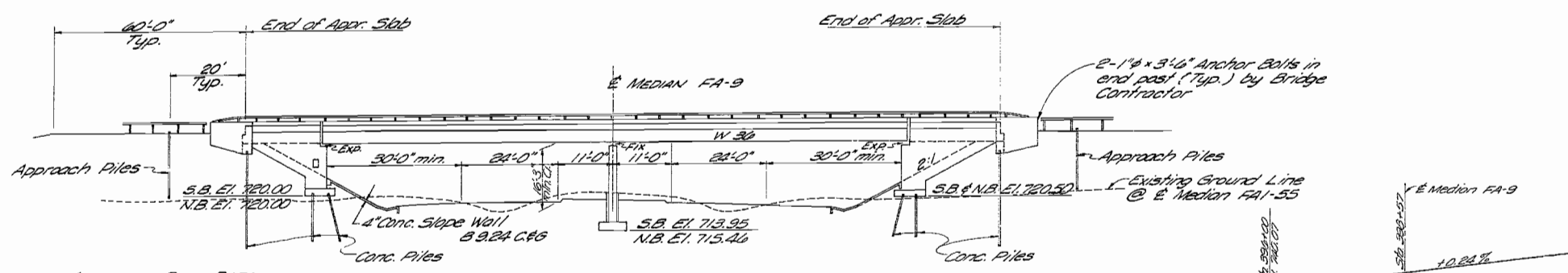
FEDERAL AID ROUTE NO.	SAC	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-55	2471	McLean	33	21
STA.	TO STA.			
S.P.A. REG. NO. 1	ILLINOIS	PROJECT		



Section 57-1
 Finishing & Erecting
 Right-of-Way Markers
 Each = 2

INDEX OF CROSS SECTIONS
 FAI-55 AT U.S.-24
 FAI ROUTE 55 SECTION 57-1(1)
 McLEAN COUNTY 57-1HB

B.M. E-246 - Standard C. & G.S. Disk
126' West of Pk. 25
Elev. 719.00



ELEVATION

APPROACH PILE DATA
Type - Crowsfoot
Length Req'd. - 19'
No. Req'd. - 37

GENERAL NOTES

All reinforcement bars shall be lapped 24 diameters unless otherwise shown.
Fasteners shall be high strength bolts. Bolts 3/4" Ø, open holes 1 1/8" Ø unless noted.
Calculated weight of Structural Steel = 439,470 Lbs.
The Basic Lead Silico Chromate paint system shall be used for shop and field painting of structural steel.
Field welding of construction accessories will not be permitted to the bottom flange of beams or girders nor to the top flange for a distance equal to one-fourth the span length each way from the pier supports. Field welding in other areas will be permitted only when approved by the Engineer.
Anchor bolts shall be set before bolting diaphragms over supports. The embankment configuration shown shall be the minimum embankment that must be constructed prior to the construction of the abutments.
The concrete rail section above the mandatory construction joint at the top of the slab shall be constructed of Class X Concrete, except the aggregates shall conform to the requirements of Handrail Concrete.
The contractor shall drive 3 test piles in permanent locations. (See Sheets 16-21) as directed by the Engineer before ordering the remainder of the piles.
Protective Coat shall not be applied to surfaces to which Cool Tar Interlayer Protective Coat is applied.
Slope Wall shall be reinforced with welded wire fabric 6"x6" mesh, weighing 50# per 100 sq. ft.
Concrete Piles @ Abut. Berms shall be driven in holes prepared through the embankment in accordance with Article 513.09(c) of the Standard Specifications.

For Footing Layout See Sheet 15 of 25

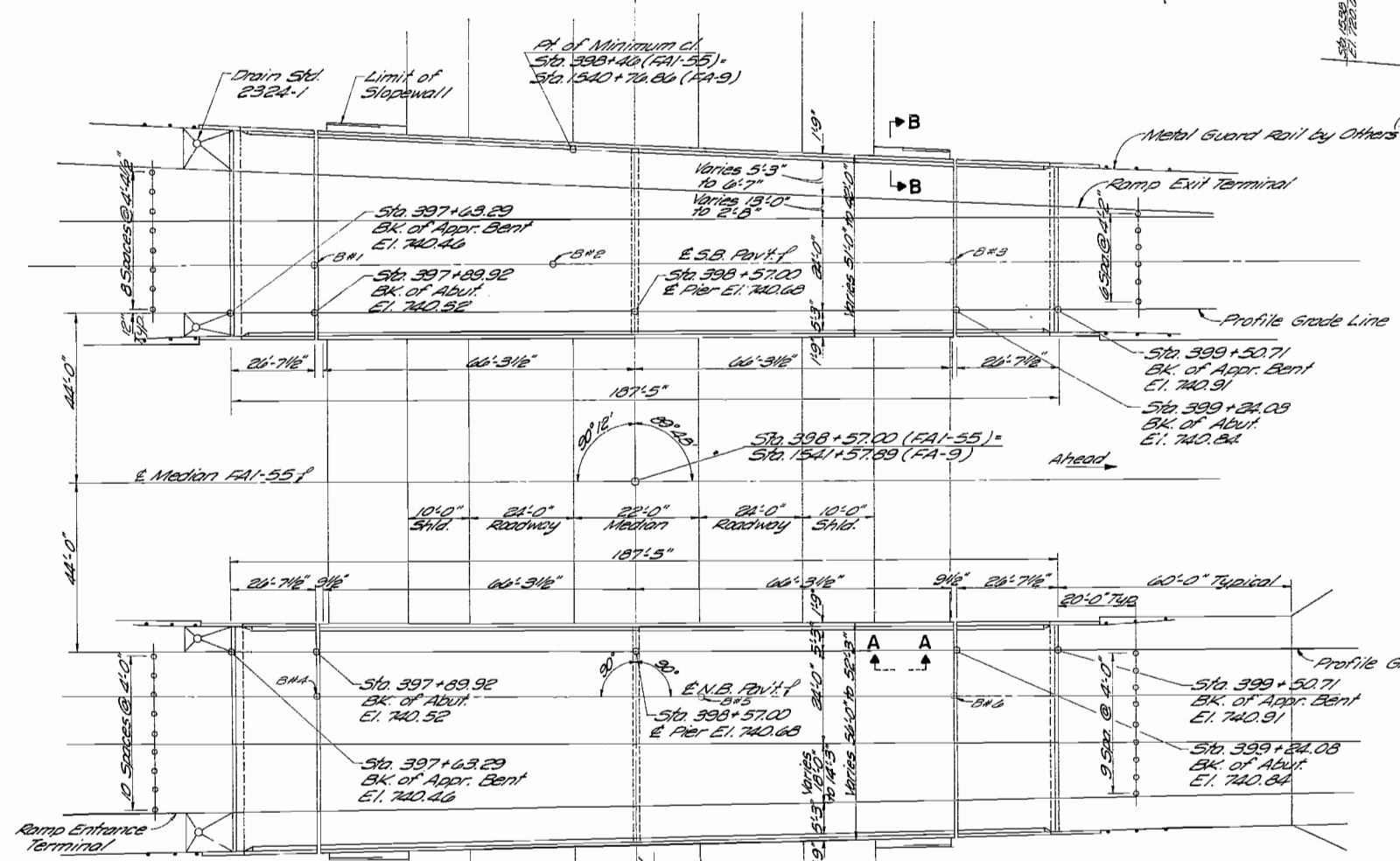
STA. 398 + 57.00
BUILT 197 BY
STATE OF ILLINOIS
FAI RTE. 55 SEC. 57-1HB
FA PROJECT I-55-5(39)
LOADING HS 20

NAME PLATE
See Std. 2113 (2 Req'd.)

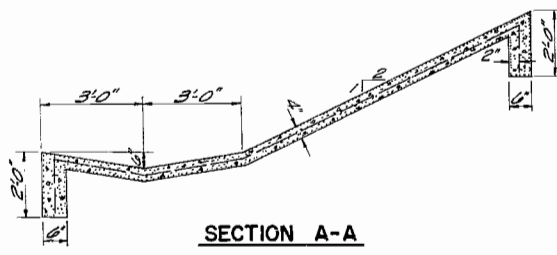
TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
* Bit Conc. Surface Course, C.I. I	Tons	160		160
Structure Excavation	Cu. Yd.		208	208
Class X Concrete	Cu. Yd.	579.0	608.7	1187.7
Precast Prest. Conc. I-Bms. 36"	Lin. Ft.		602	602
Aluminum Siding	Lin. Ft.		782	782
Crowsfoot Piles 19 Ft.	Lin. Ft.		703	703
Concrete Piles	Lin. Ft.		4878	4878
Test Pile (Concrete)	EA		2	2
Name Plate	EA		2	2
Slope Wall, 4"	Sq. Yd.			1075
* Cool Tar Interlayer Protect. Coat	Sq. Yd.	1940		1940
Reinforcement Bars	Lbs.	155,290	68,150	223,440
Structural Steel	L.S.	7.5		7.5
Preformed Joint Sealer 2 1/2"	Lin. Ft.	201		201
Protective Coat	Sq. Yd.	286		286

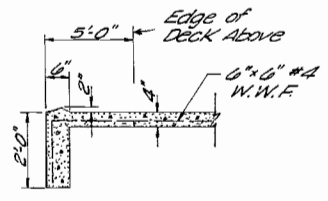
* For Information Only



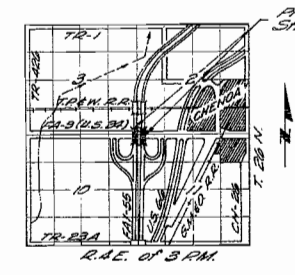
PLAN



SECTION A-A



SECTION B-B



LOCATION PLAN

DESIGN STRESSES

FIELD UNITS	PRECAST-PRESTRESSED UNITS
$f_c = 1,200$ p.s.i. (Deck Slab)	$f_c = 5,000$ p.s.i.
$f_c = 1,400$ p.s.i. (Sub. Curb & Approach)	$f_c = 4,000$ p.s.i.
$f_s = 20,000$ p.s.i. (Reinf.)	$f_s = 248,000$ p.s.i.
$f_s = 20,000$ p.s.i. (Struct. A-36)	$f_s = 173,600$ p.s.i.
$v_c = 75$ p.s.i. (Flgs.)	
$n = 10$	

25% I_a included in D.L. for future wearing surface
Allowable LL Defl. 4/1000

LOADING HS 20 - 44 & ALTERNATE
Design Specs. 1969 AASHTO as applicable

APPROVED
[Signature]
[Title]

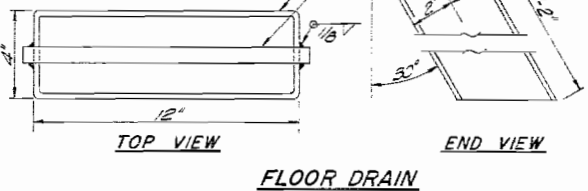
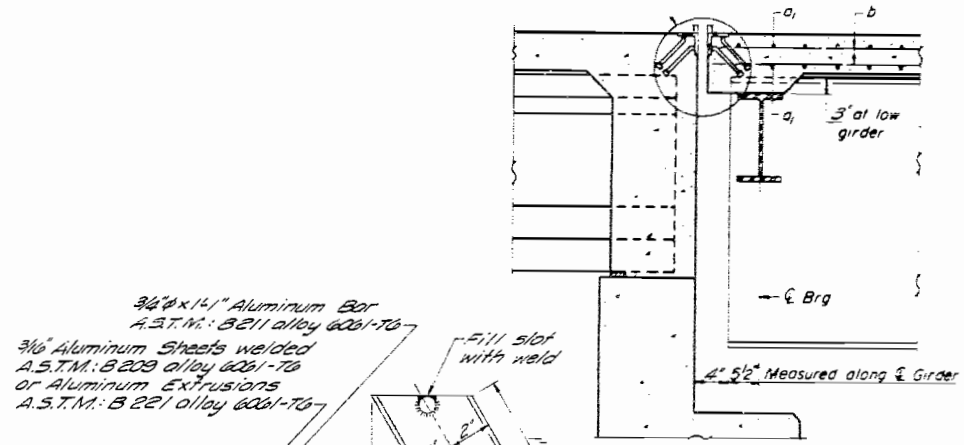
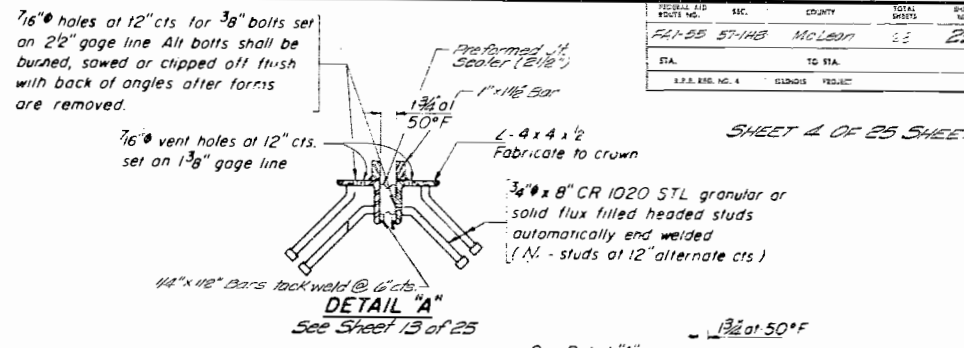
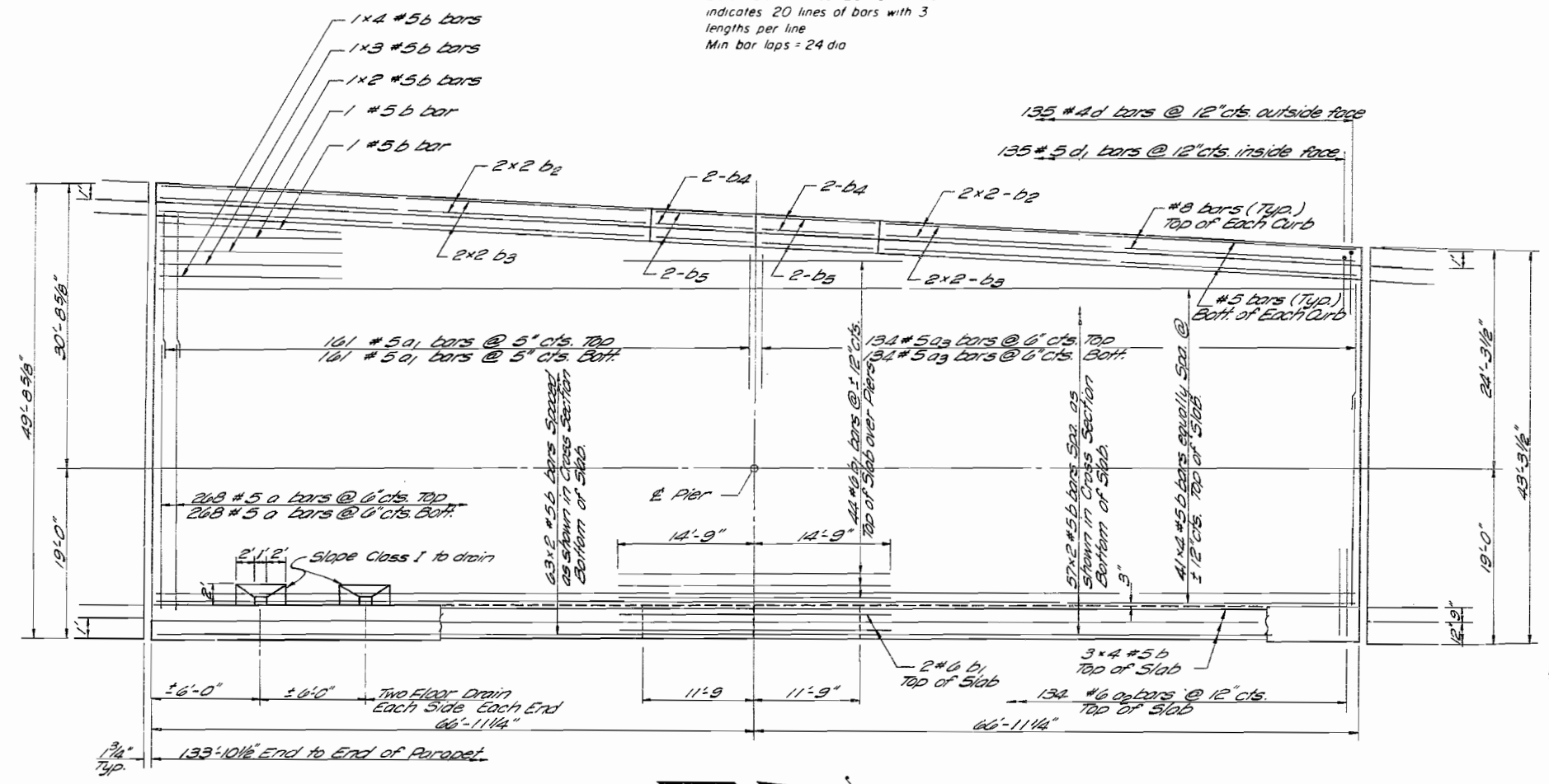
Robert S. Green 7/6/54

GENERAL PLAN & ELEVATION
FAI-55 OVER U.S. RTE. 24 (FA-9)
PROJECT I-55-5 (39) 184
FAI ROUTE 55 SECTION 57-1HB
MCLEAN COUNTY
STATION 398 + 57.00 (FAI-55)

PROJECT NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-85-57-1HB	57-1HB	McLean	25	25
STA.	TO STA.			
S.P.A. NO. 4	GLDHS	PRJCT		

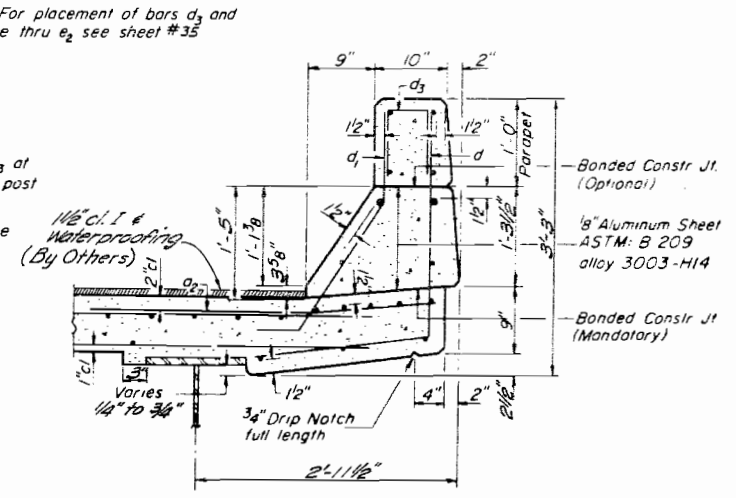
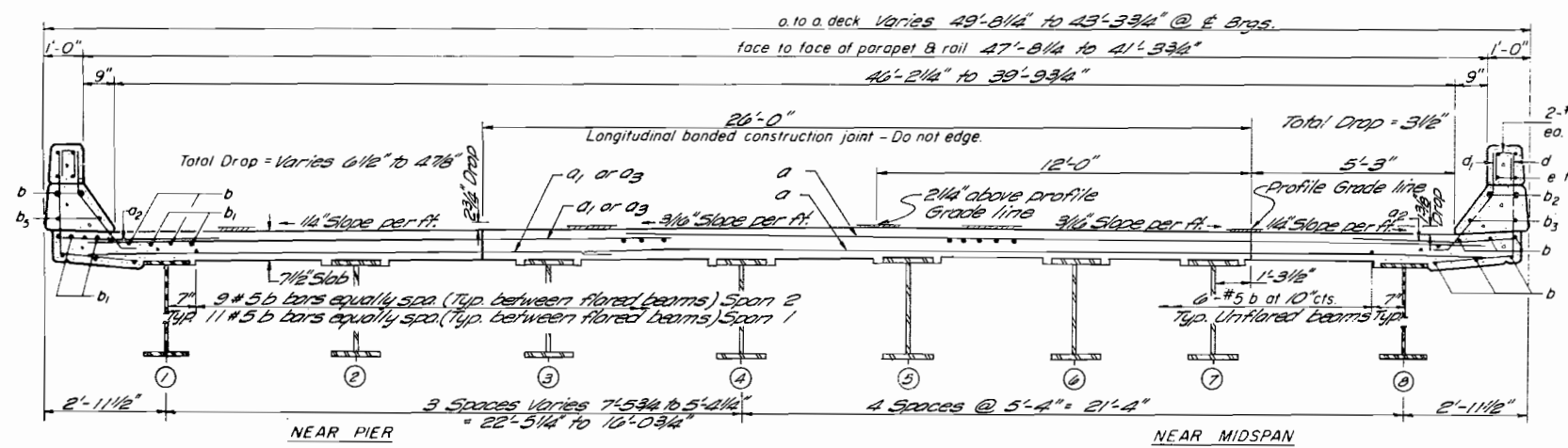
SHEET 4 OF 25 SHEETS

NOTE
 Bars indicated thus 20 x 3 #5 etc indicates 20 lines of bars with 3 lengths per line
 Min bar laps = 24 dia



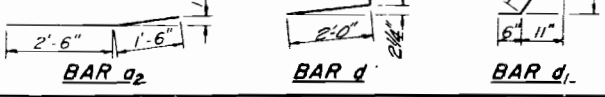
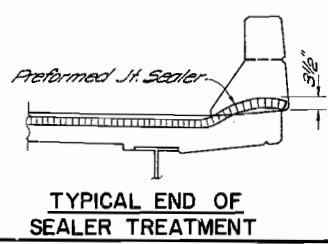
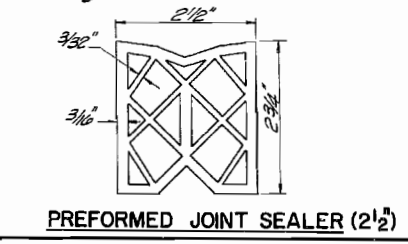
BILL OF MATERIAL

Bar	No	Size	Length	Shape
a	536	#5	27'-9"	
a ₁	322	#5	22'-0"	
a ₂	268	#6	4'-0"	
a ₃	268	#5	19'-0"	
b	459	#5	34'-9"	
b ₁	28	#6	29'-6"	
b ₂	16	#8	28'-9"	
b ₃	16	#5	28'-6"	
b ₄	8	#8	11'-0"	
b ₅	8	#5	11'-6"	
d	270	#4	4'-7"	J
d ₁	270	#5	3'-5"	J
Reinforcement Bars		Lbs	51,650	
Class X Concrete		Cu Yds	171.5	



CROSS SECTION Looking North

CURB SECTION



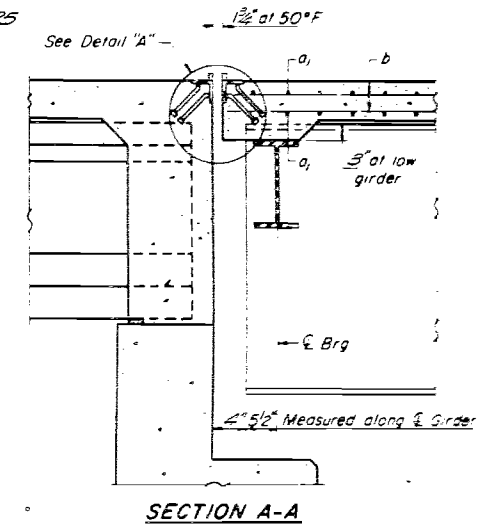
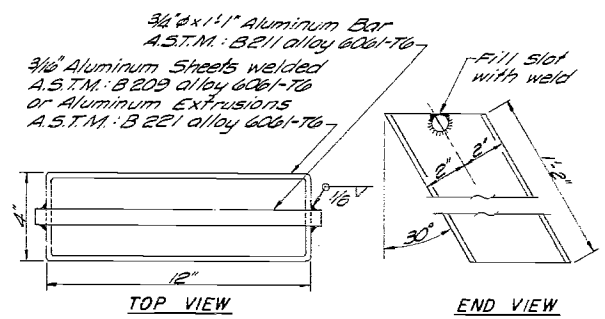
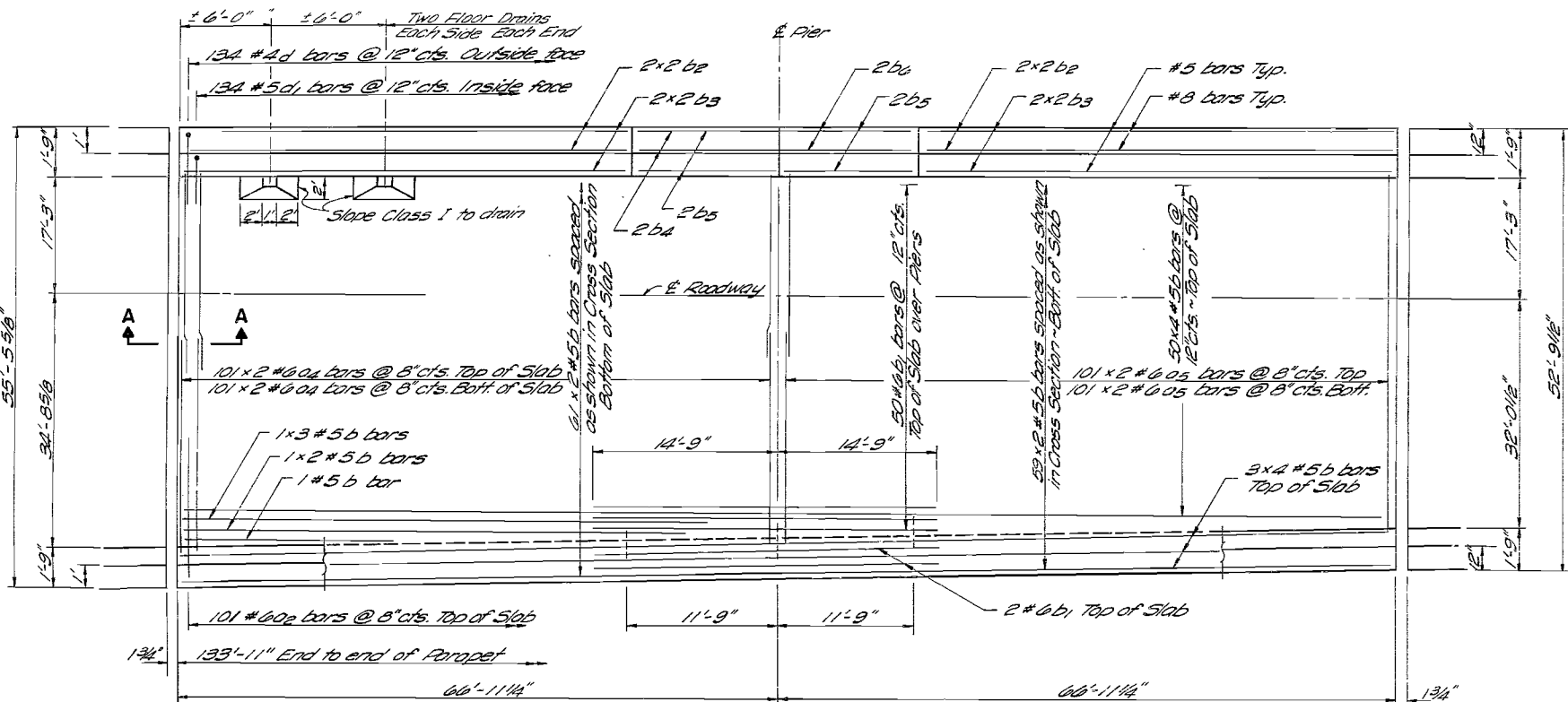
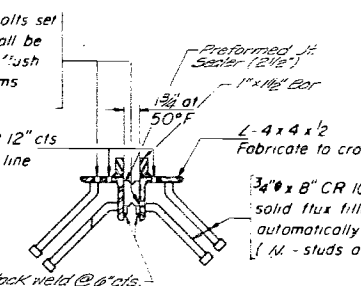
Cost of Aluminum Sheets shall be incidental to Class X Concrete

Parapet Reinforcement and Class X Concrete are billed on sheet # 14

SUPERSTRUCTURE
 SOUTHBOUND - STRUCTURE
 FAI ROUTE 55 SEC. 57-1HB
 MC LEAN COUNTY
 STATION 398+57.00

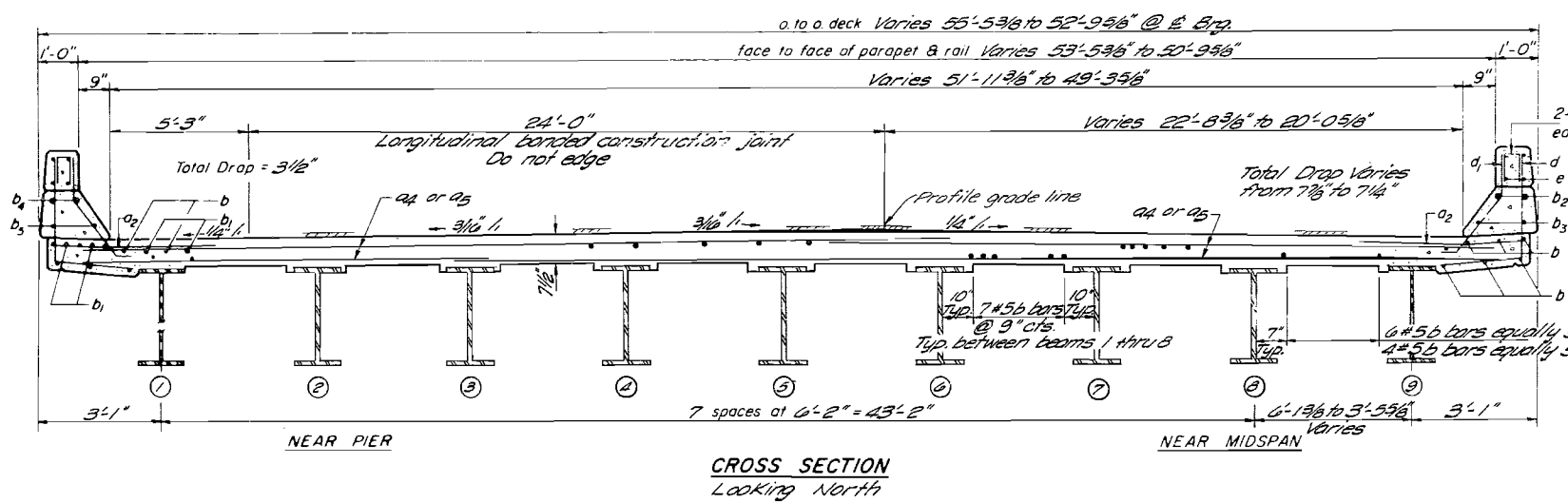
NOTE
 Bars indicated thus 20 x 3 #5 etc
 indicates 20 lines of bars with 3
 lengths per line
 Min bar laps = 24 dia

1/6" holes at 12" cts for 3/8" bolts set
 on 2" gage line. All bolts shall be
 burned, sawed or clipped off flush
 with back of angles after forms
 are removed.

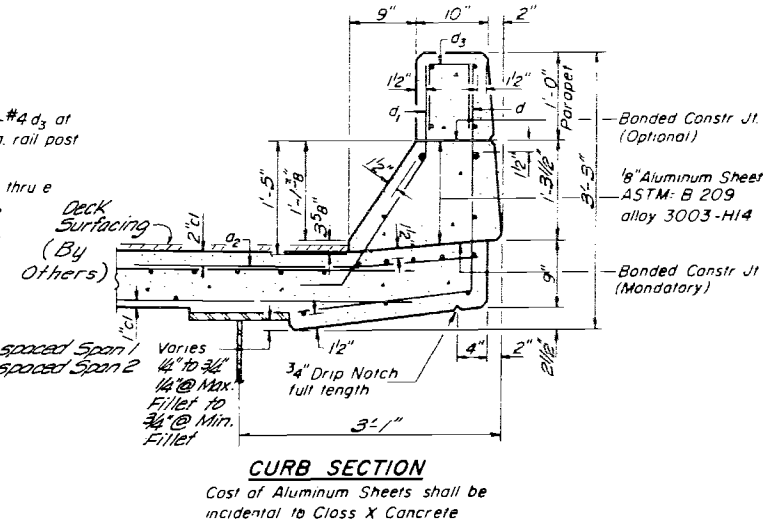


BILL OF MATERIAL

Bar	No	Size	Length	Shape	
a4	404	#6	29'-3"		
a2	222	#6	4'-0"		
a5	404	#6	28'-9"		
b	454	#5	34'-9"		
b1	54	#6	29'-6"		
b2	16	#8	28'-9"		
b3	16	#5	28'-6"		
b4	8	#8	11'-6"		
b5	8	#5	11'-6"		
d	208	#4	4'-7"	J	
d1	208	#5	3'-5"	J	
Reinforcement Bars				Lbs	53,280
Class X Concrete				Cu Yds	195.9



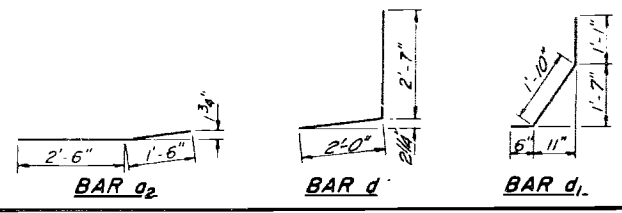
NOTE: For placement of bars d3 and
 e thru e2 see sheet #35

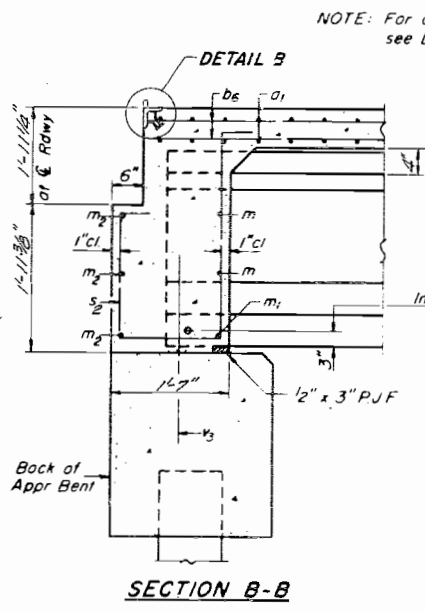
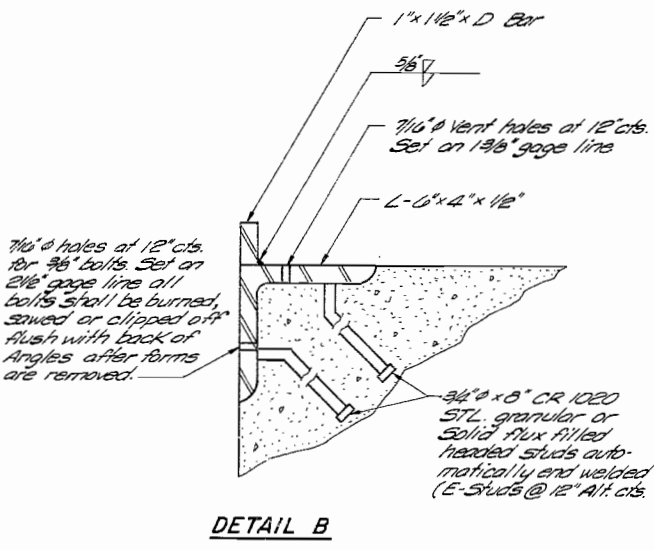
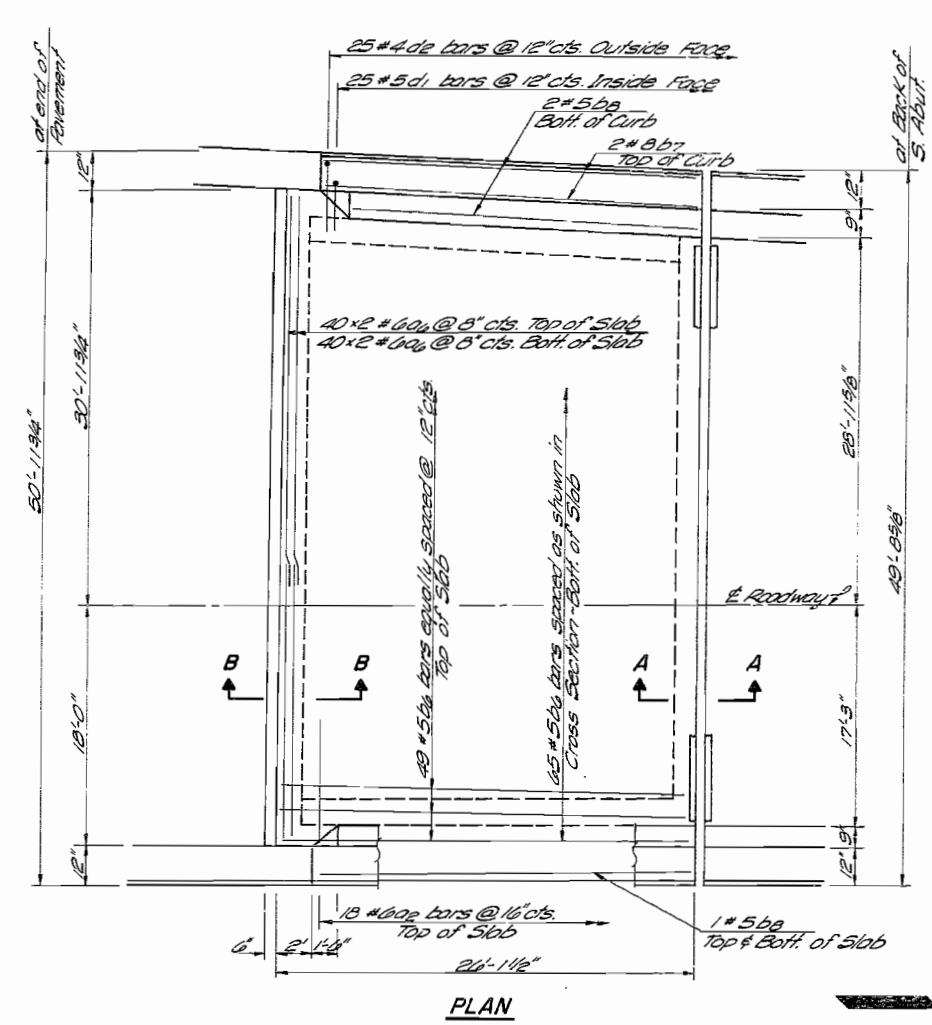


Parapet Reinforcement and Class X
 Concrete are billed on sheet # 14

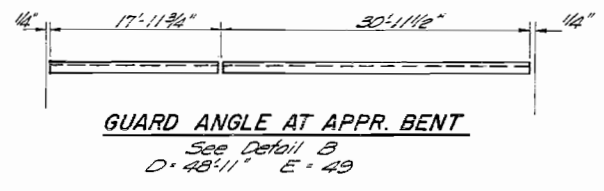
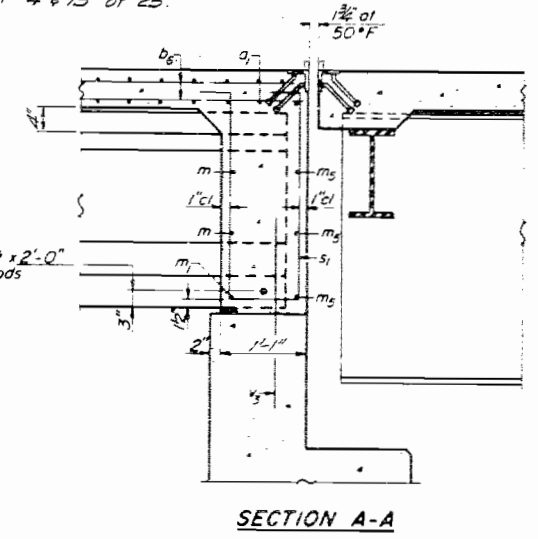
SUPERSTRUCTURE
NORTHBOUND - STRUCTURE
 FAI ROUTE 55 SEC. 57-1HB
 MC LEAN COUNTY
 STATION 398+57.00

Note:
 Preformed joint sealer & sealer treatment (See Details-Sht 4)





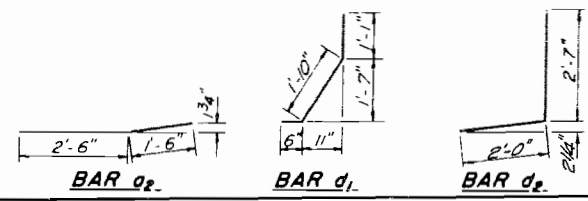
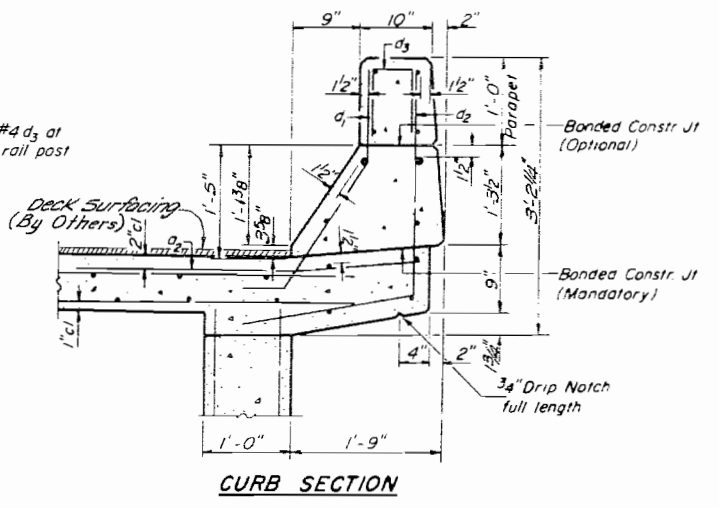
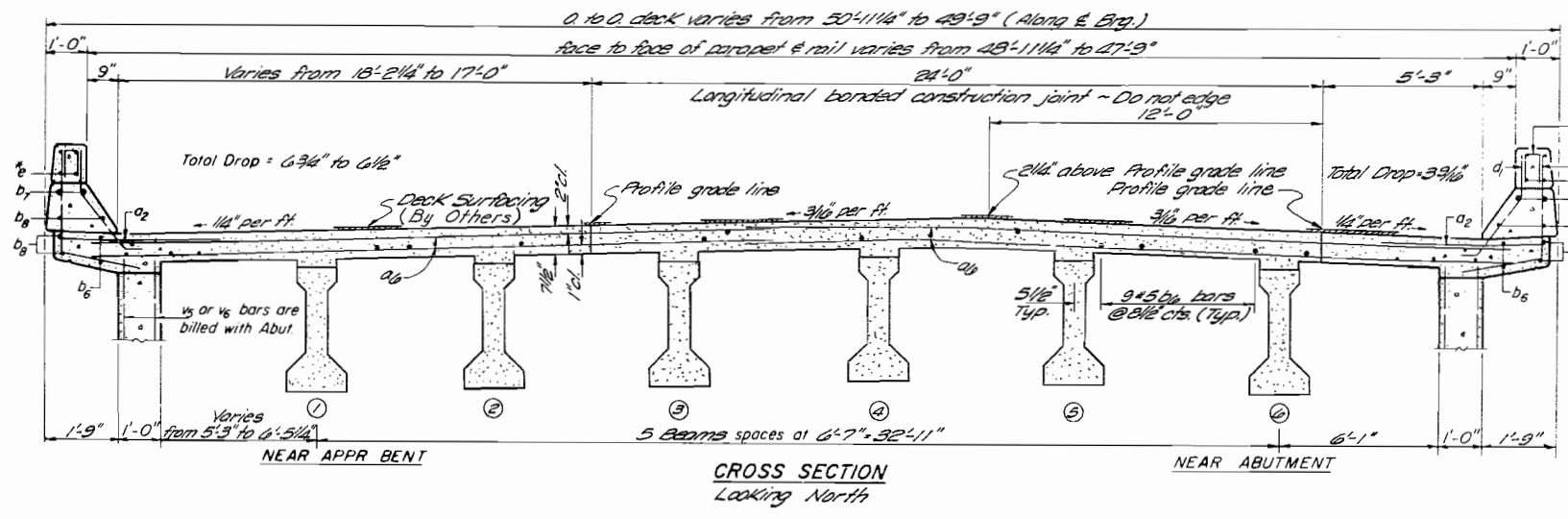
NOTE: For details of expansion angles at Abut see Detail "A" sheet #4 & 13 of 25.



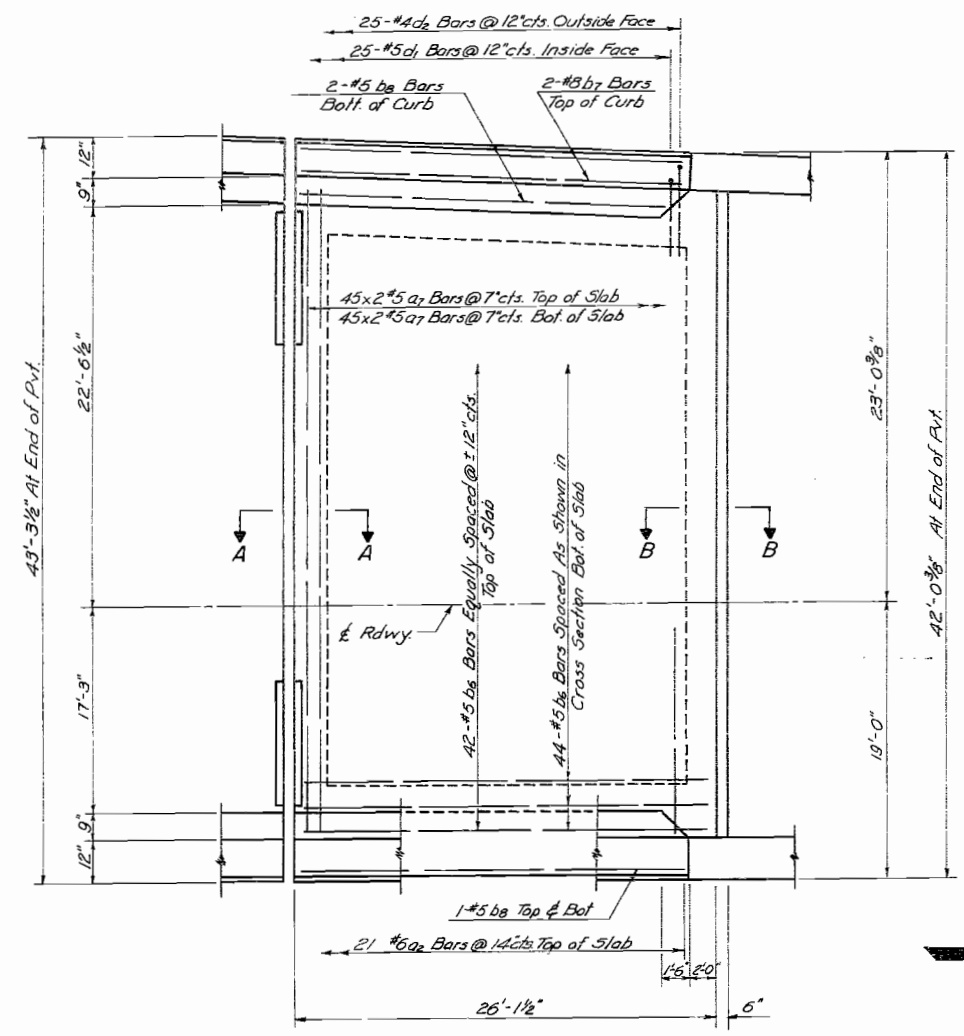
ONE APPR. SPAN
BILL OF MATERIAL

Bar	No	Size	Length	Shape
a ₂	120	#6	25'-9"	
a ₂	36	#6	4'-0"	
b ₆	114	#5	25'-9"	
b ₇	4	#8	23'-9"	
b ₈	8	#5	22'-9"	
d ₁	50	#5	3'-5"	
d ₂	50	#4	4'-7"	
m	28	#4	5'-5"	
m	14	#5	4'-11"	
m ₂	6	#5	25'-0"	
s ₁	42	#4	8'-9"	
s ₂	42	#4	7'-9"	
Reinforcement Bars		Lbs	11,170	
Class X Concrete		Cu Yds	43.6	

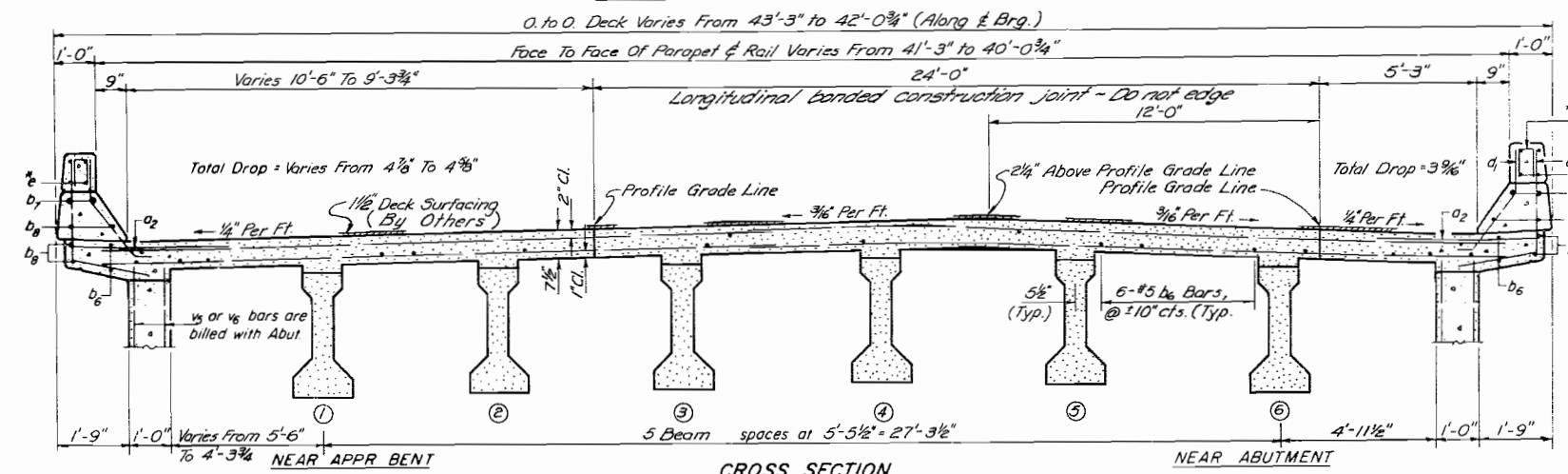
*Parapet Reinforcement and Class X Concrete are billed on sheet # 14
For placement and details of bars m thru m₂ and s₁ thru s₂ see sheet # 11



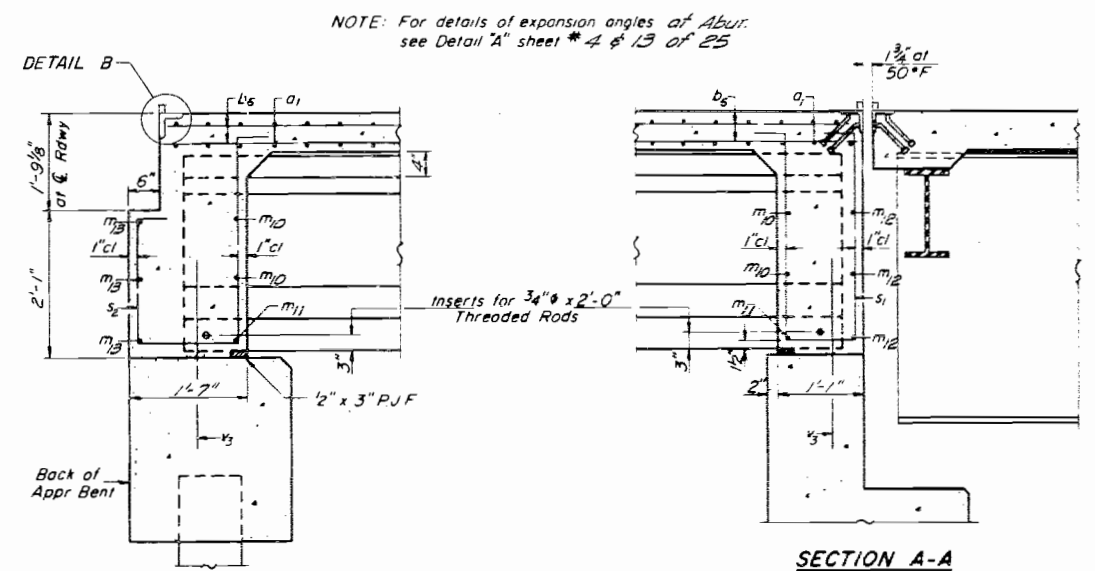
SUPERSTRUCTURE
SOUTH APPROACH SPAN
SOUTHBOUND ROADWAY
FAI ROUTE 55 SEC. 57-1HB
MCLEAN COUNTY
STA. 398 + 57.00



PLAN

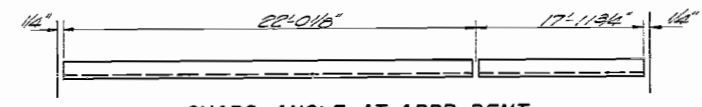


CROSS SECTION Looking North



SECTION B-B
Note: For Detail B See Sheet 6 of 25

SECTION A-A

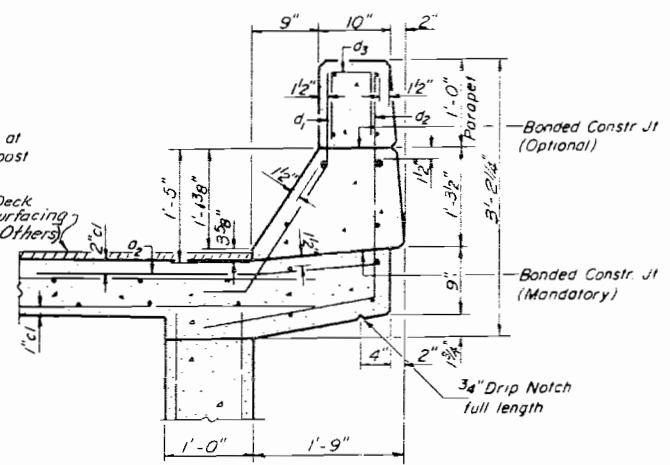


GUARD ANGLE AT APPR. BENT
See Detail B Sheet 6 of 25
D = 39.41" E = 40

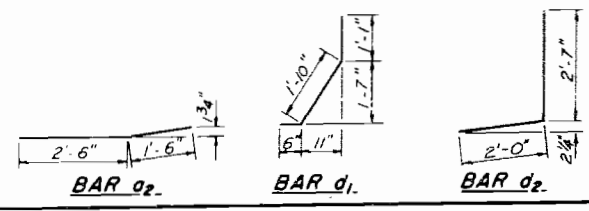
ONE APPR. SPAN
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a ₁	180	#5	21'-9"	—
a ₂	42	#6	4'-0"	—
b ₆	86	#5	25'-9"	—
b ₇	4	#8	23'-9"	—
b ₈	8	#5	22'-9"	—
d ₁	50	#5	3'-5"	⌋
d ₂	50	#4	4'-7"	⌋
m ₆	2	#4	5'-0"	—
m ₁₀	24	#4	4'-6"	—
m ₁₁	12	#5	3'-5"	—
m ₁₂	3	#5	15'-6"	—
m ₁₃	6	#5	20'-6"	—
m ₁₄	2	#4	3'-9"	—
m ₁₅	1	#5	3'-5"	—
m ₇	1	#5	4'-6"	—
s ₂	35	#4	7'-9"	⌋
s ₁	35	#4	8'-9"	⌋
Reinforcement Bars			Lbs	5,200
Class X Concrete			Cu Yds	42.1

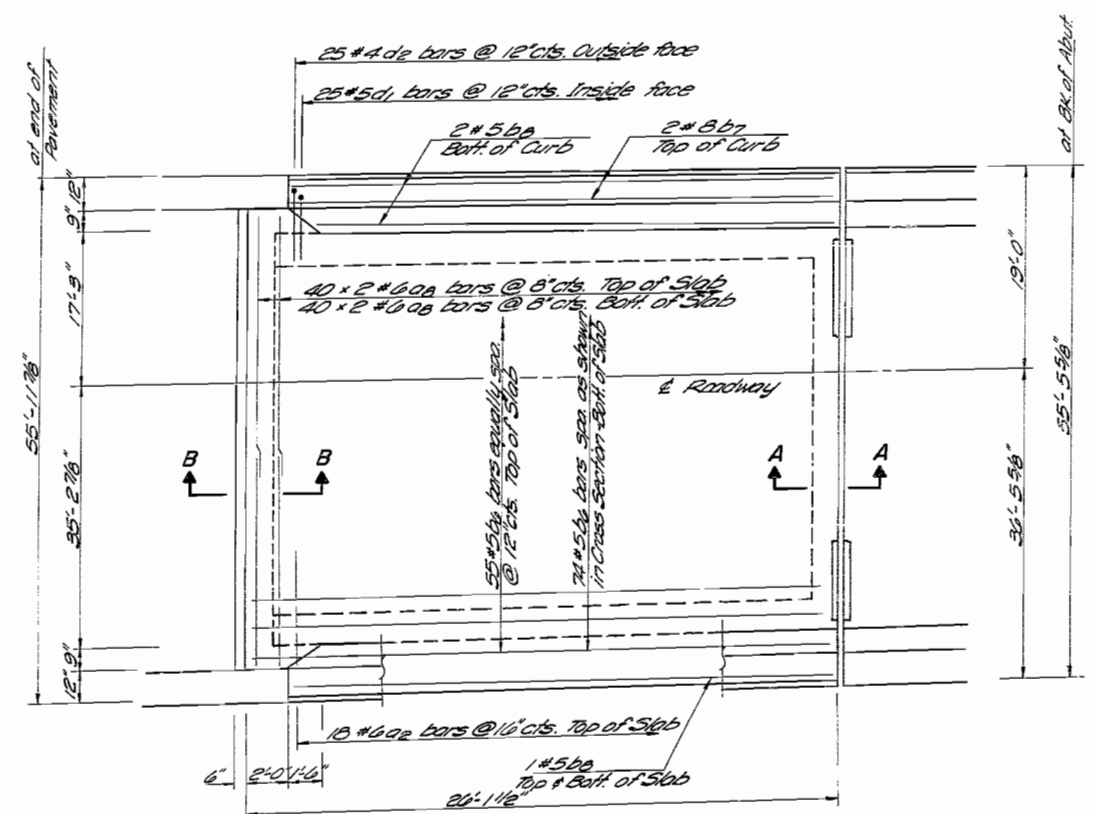
* Parapet Reinforcement and Class X Concrete are billed on sheet # 14
For placement and details of bars m₁₀ thru m₁₅ and s₁ thru s₂ see sheet # 11



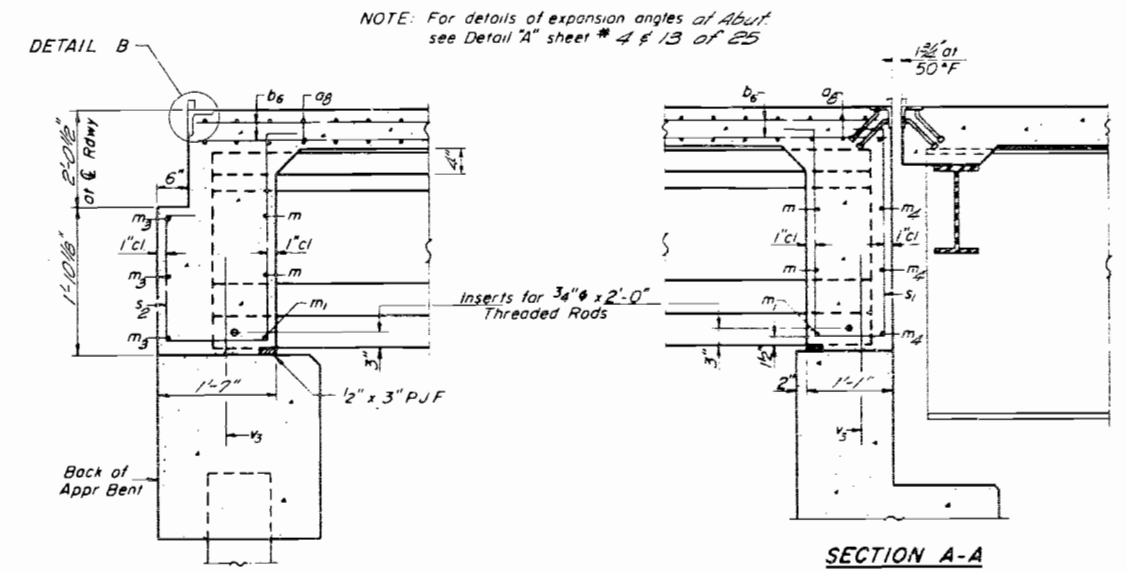
CURB SECTION



SUPERSTRUCTURE
NORTH APPROACH SPAN
SOUTHBOUND ROADWAY
FAI ROUTE 55 SEC. 57-1HB
MCLEAN COUNTY
STA. 398 + 57.00

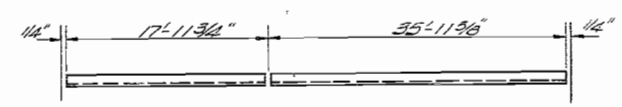


PLAN

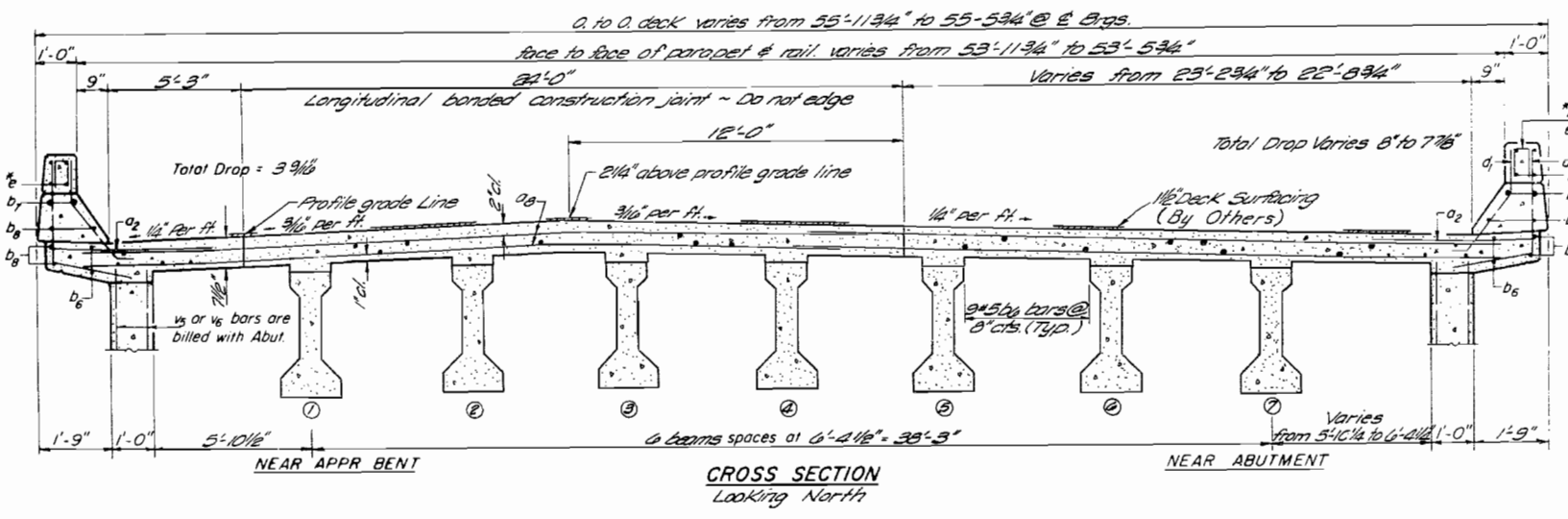


SECTION B-B

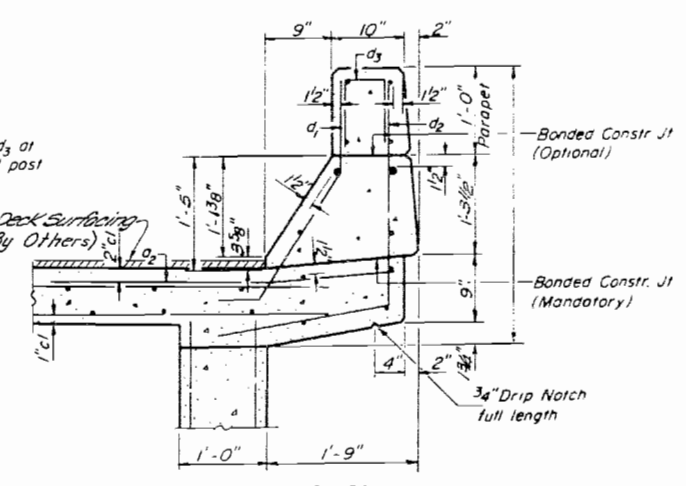
Note: For Detail B See Sheet 6 of 25



GUARD ANGLE ON APPR. BENT
 See Detail B Sheet 6 of 25
 D = 53'-11" E = 54'



CROSS SECTION
 Looking North

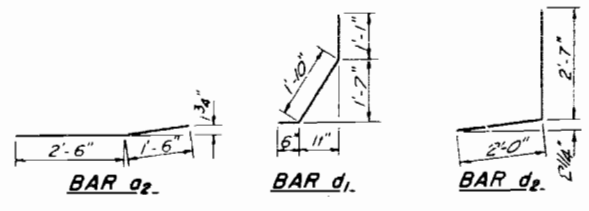


CURB SECTION

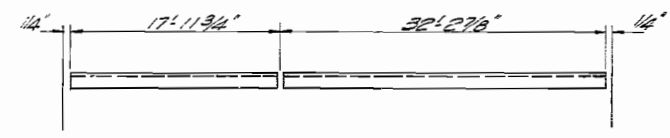
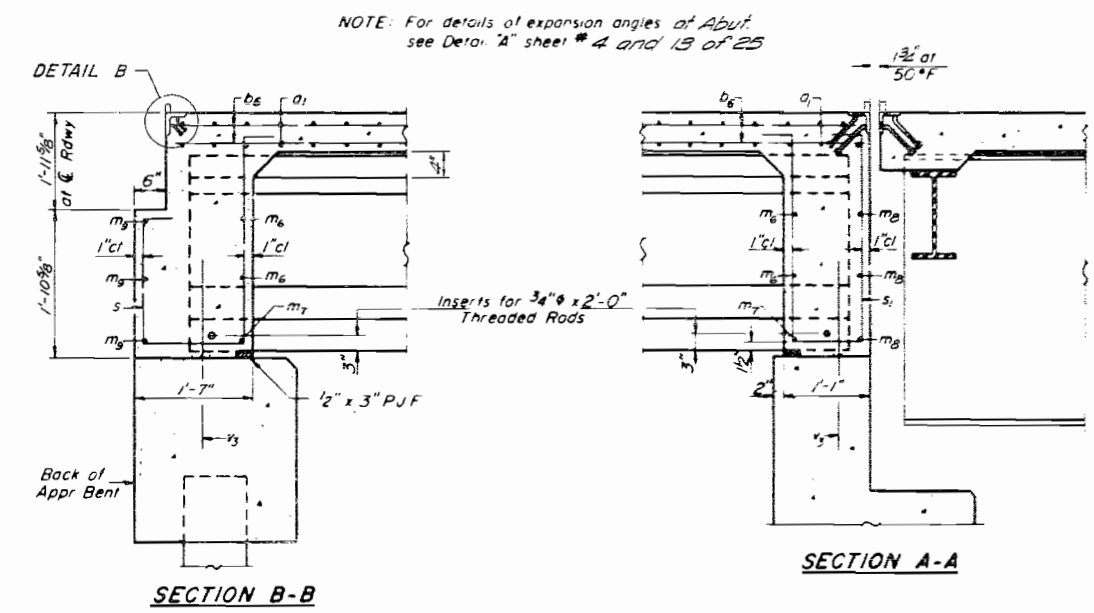
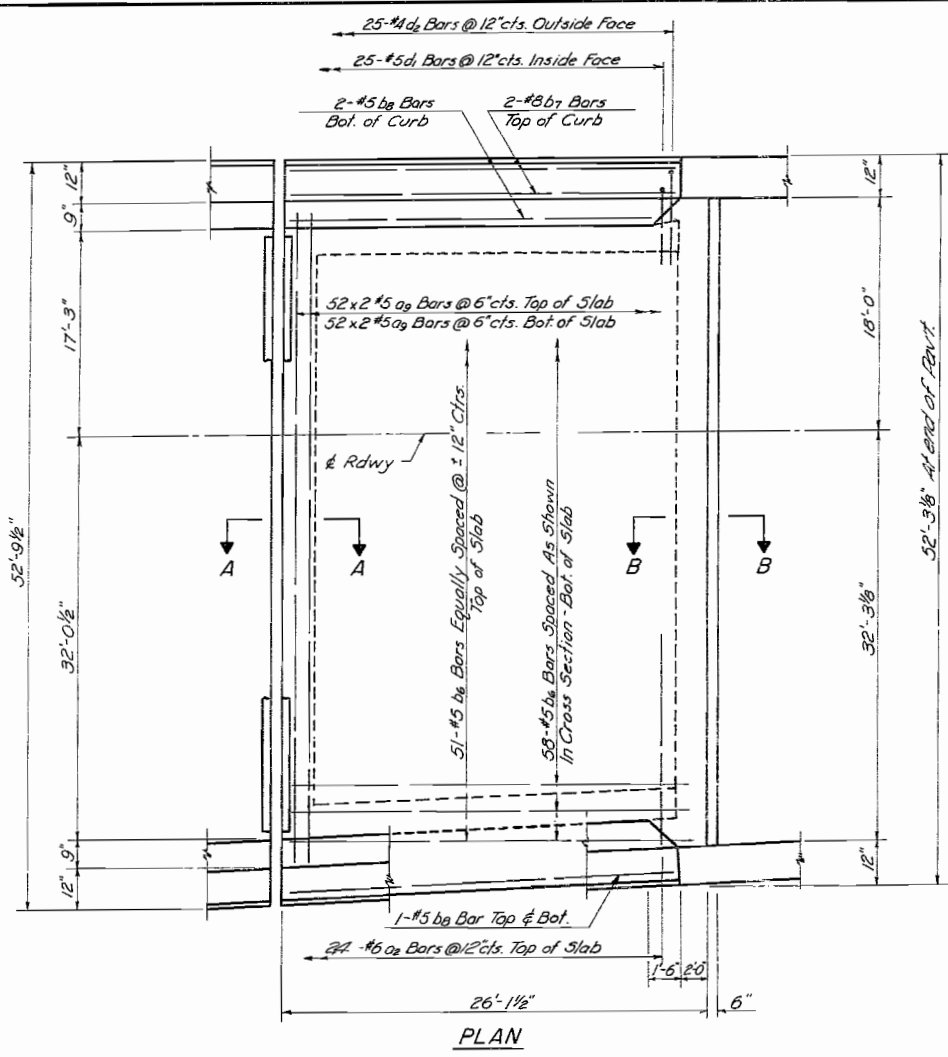
ONE APPR. SPAN
 BILL OF MATERIAL

Bar	No	Size	Length	Shape
a ₁	100	#6	29'-3"	
a ₂	36	#6	4'-0"	
b ₆	129	#5	25'-9"	
b ₇	4	#8	23'-9"	
b ₈	8	#5	22'-9"	
d ₁	50	#5	3'-5"	
d ₂	50	#4	4'-7"	
m	32	#4	5'-5"	
m ₁	16	#5	4'-1"	
m ₃	6	#5	27'-6"	
m ₄	6	#5	25'-6"	
s ₁	43	#4	8'-9"	
s ₂	43	#4	7'-9"	
Reinforcement Bars			Lbs	12,290
Class X Concrete			Cu Yds	32.7

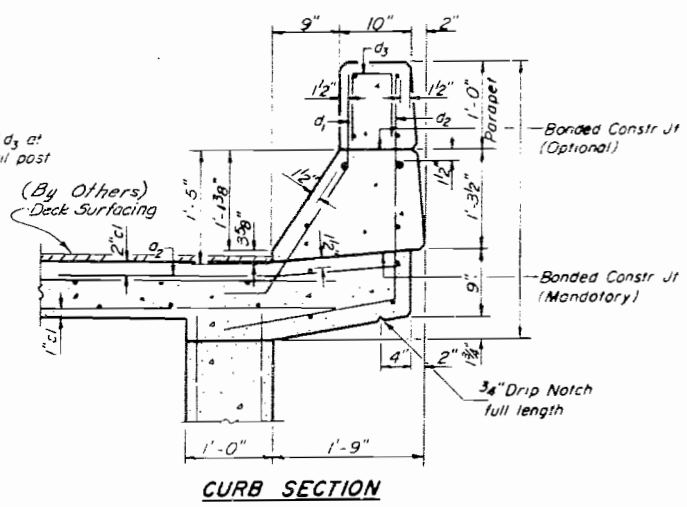
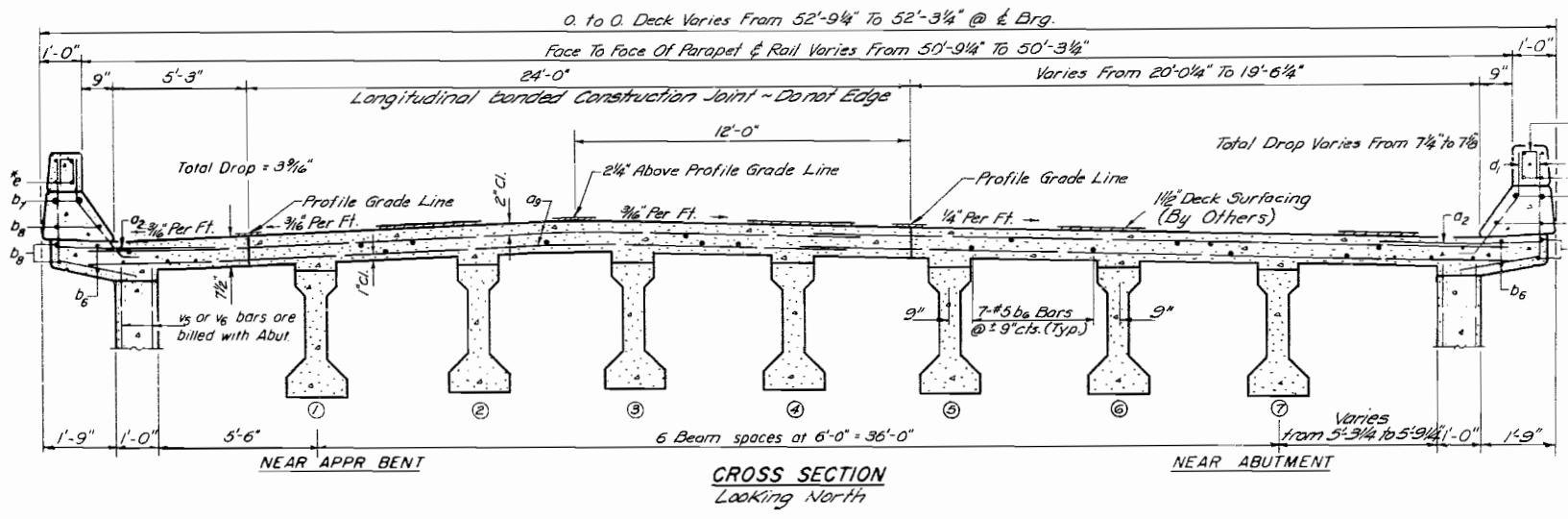
* Parapet Reinforcement and Class X Concrete are billed on sheet # 14
 For placement and details of bars m thru m₂ and s₁ thru s₂ see sheet # 11



SUPERSTRUCTURE
 SOUTH APPROACH SPAN
 NORTHBOUND ROADWAY
 FAI ROUTE 55 SEC. 57-IHB
 McLEAN COUNTY
 STA. 398 + 57.00



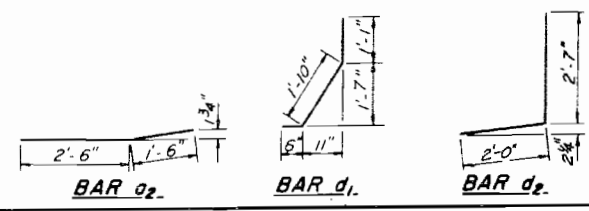
GUARD ANGLE AT APPR. BENT
 See Detail B Sheet 6 of 25
 D=50'-2" E=50



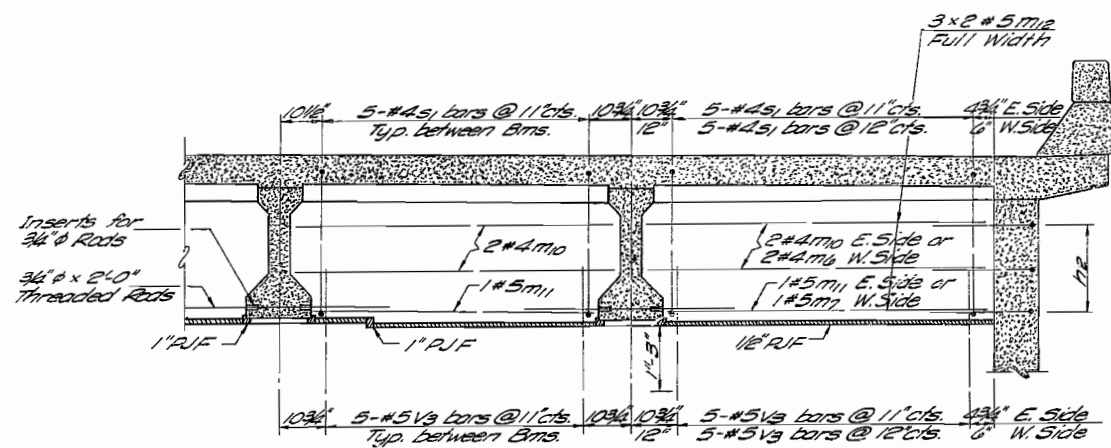
ONE APPR. SPAN
 BILL OF MATERIAL

Bar	No	Size	Length	Shape
a ₁	208	#5	26'-9"	—
a ₂	28	#6	4'-0"	—
b ₆	109	#5	25'-9"	—
b ₇	4	#8	23'-9"	—
b ₈	8	#5	22'-9"	—
d ₁	50	#5	3'-5"	—
d ₂	50	#4	4'-7"	—
m ₆	32	#4	5'-0"	—
m ₇	16	#5	4'-6"	—
m ₈	6	#5	24'-0"	—
m ₉	6	#5	26'-0"	—
s ₂	40	#4	7'-9"	—
s ₁	40	#4	8'-9"	—
Reinforcement Bars			Lbs	11,730
Class X Concrete			Cu Yds	49.3

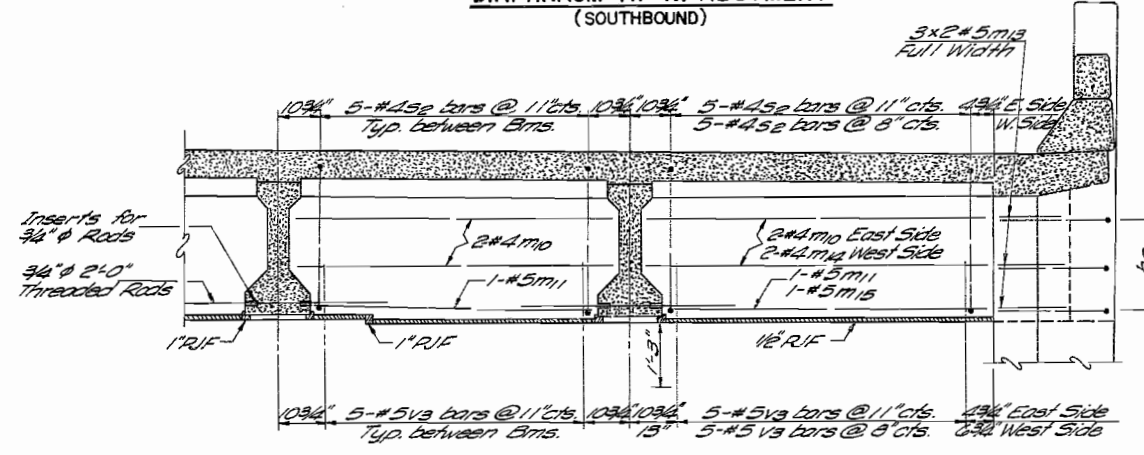
*Parapet Reinforcement and Class X Concrete are billed on sheet #14.
 For placement and details of bars m₆ thru m₉ and s₁ thru s₂ see sheet #10



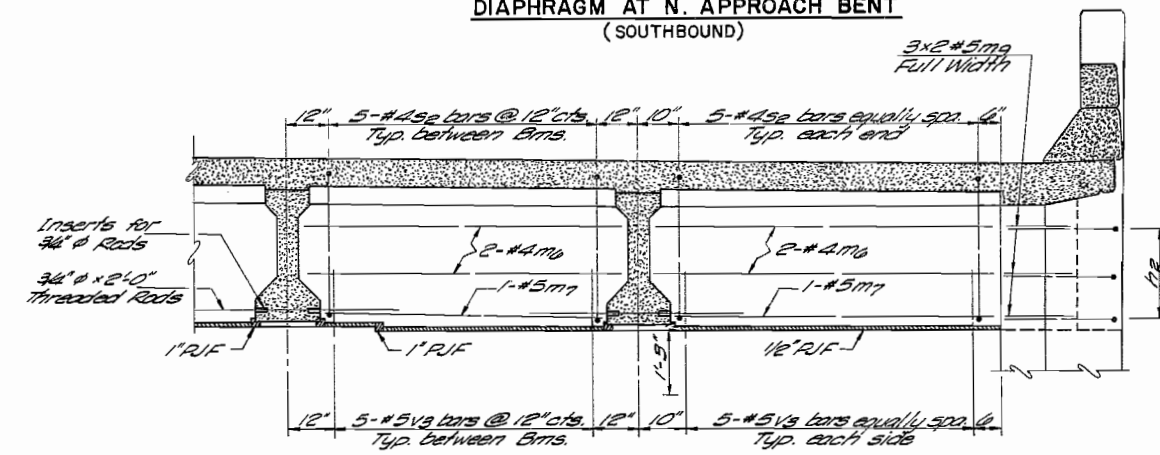
SUPERSTRUCTURE
 NORTH APPROACH SPAN
 NORTHBOUND ROADWAY
 FAI ROUTE 55 SEC. 57-1HB
 MCLEAN COUNTY
 STA. 398 + 57.00



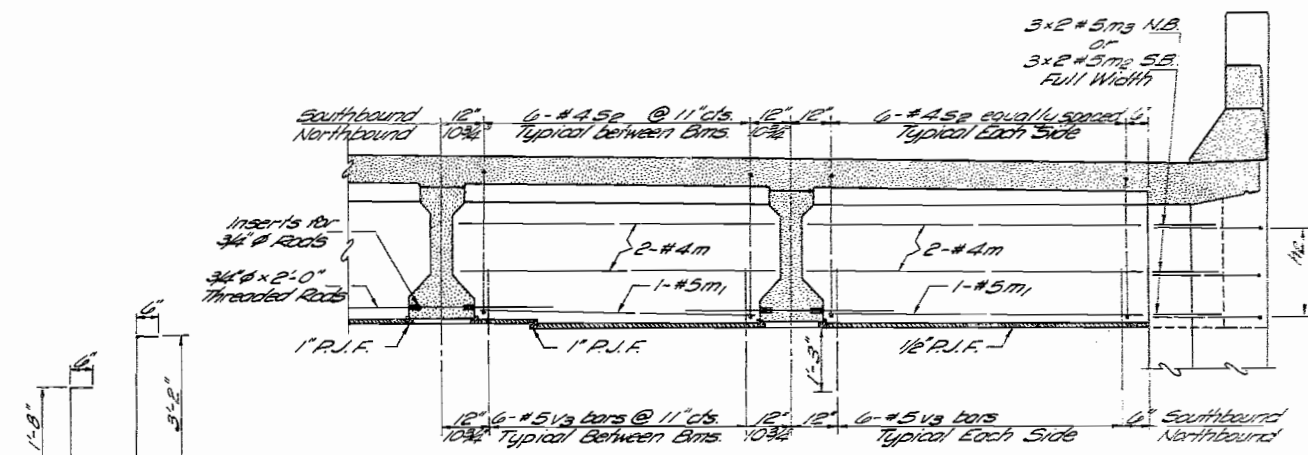
DIAPHRAGM AT N. ABUTMENT
(SOUTHBOUND)



DIAPHRAGM AT N. APPROACH BENT
(SOUTHBOUND)

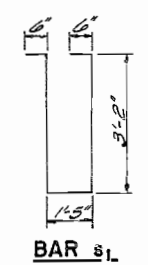


DIAPHRAGM AT N. APPROACH BENT
(NORTHBOUND)

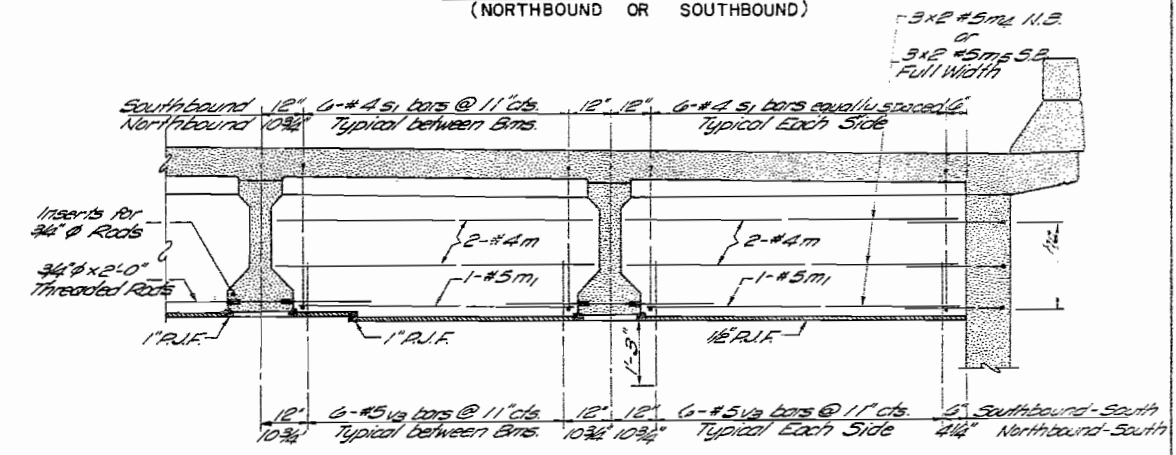


DIAPHRAGM AT S. APPROACH BENT
(NORTHBOUND OR SOUTHBOUND)

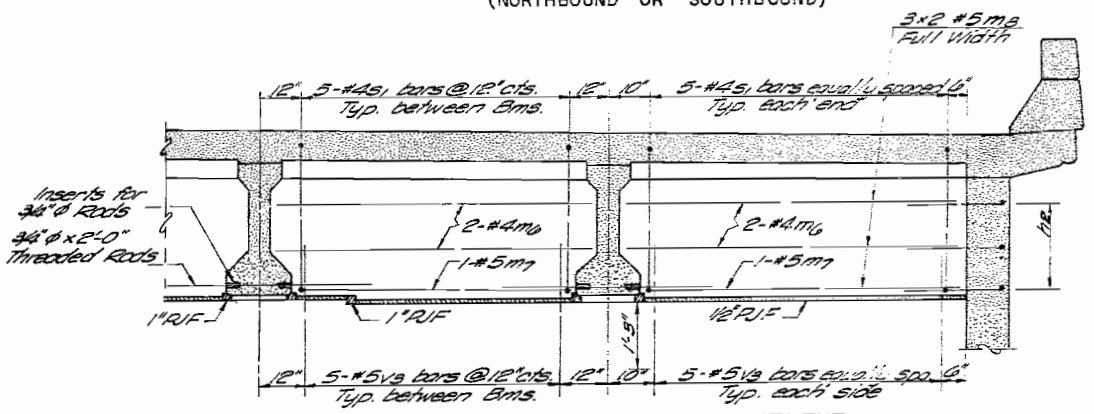
BAR s2



BAR s1



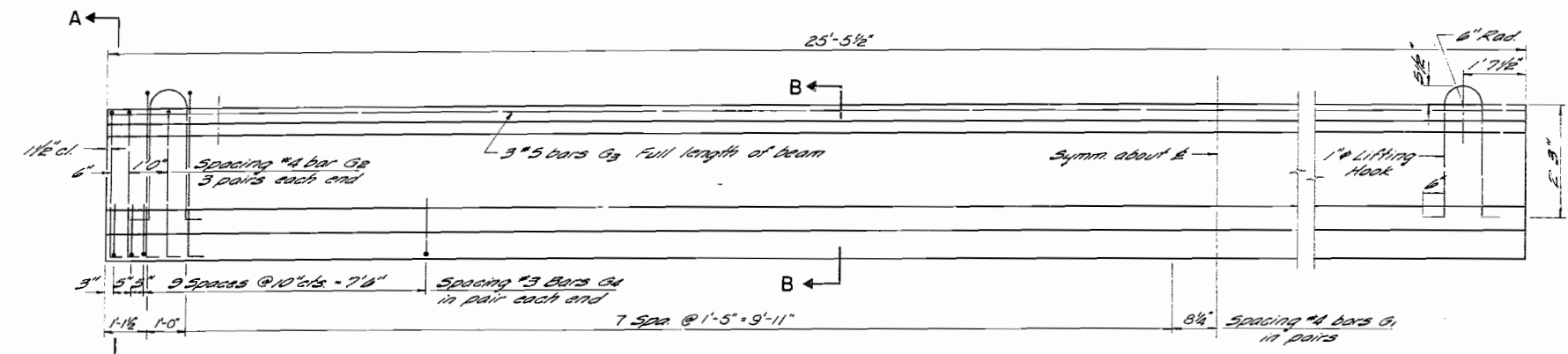
DIAPHRAGM AT S. ABUTMENT
(NORTHBOUND OR SOUTHBOUND)



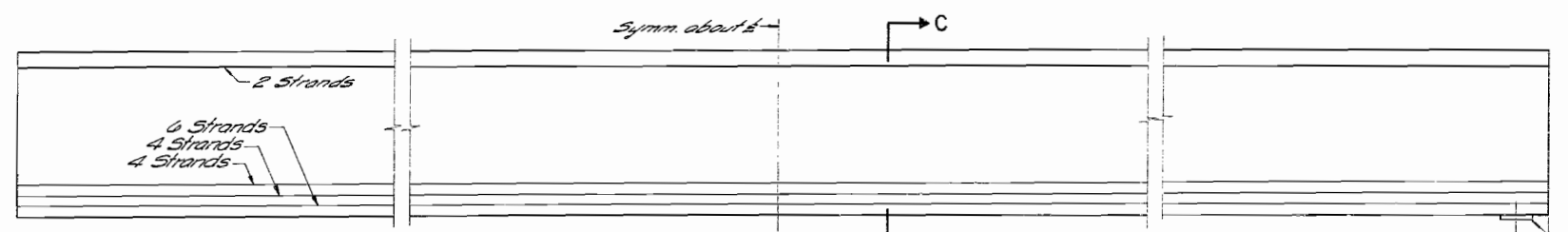
DIAPHRAGM AT N. ABUTMENT
(NORTHBOUND)

Notes:
 Bars s1 thru s2 and m thru m2 are billed with Approach Slab Bill of Material on Sheets # 6 thru 9
 Bars m1, m2, m3 & v2 are billed with Abutment Bill of Material on Sheet # 18 thru 21
 See Sheets # 18 thru 21 for sections thru abutment and approach bent diaphragms.

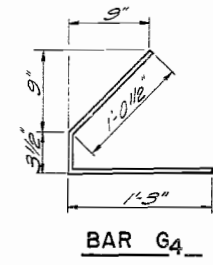
DIAPHRAGM & BEARING DETAILS
 FAI ROUTE 55 SEC. 57-1HB
 MCLEAN COUNTY
 STA. 398 + 57.00



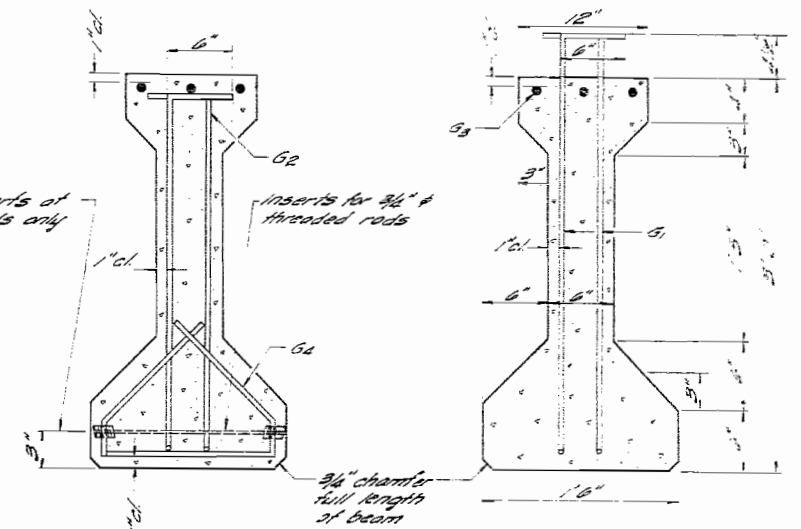
ELEVATION OF BEAM
 Showing Reinforcement & Dimensions



ELEVATION OF BEAM
 Showing Prestressing Steel

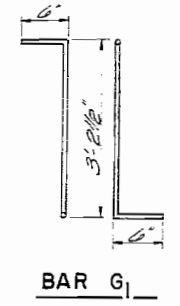


BAR G4

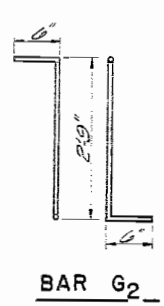


SECTION A-A

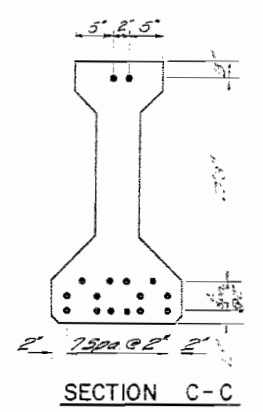
SECTION B-B



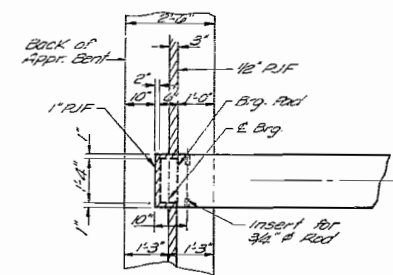
BAR G1



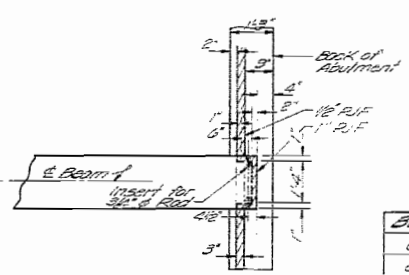
BAR G2



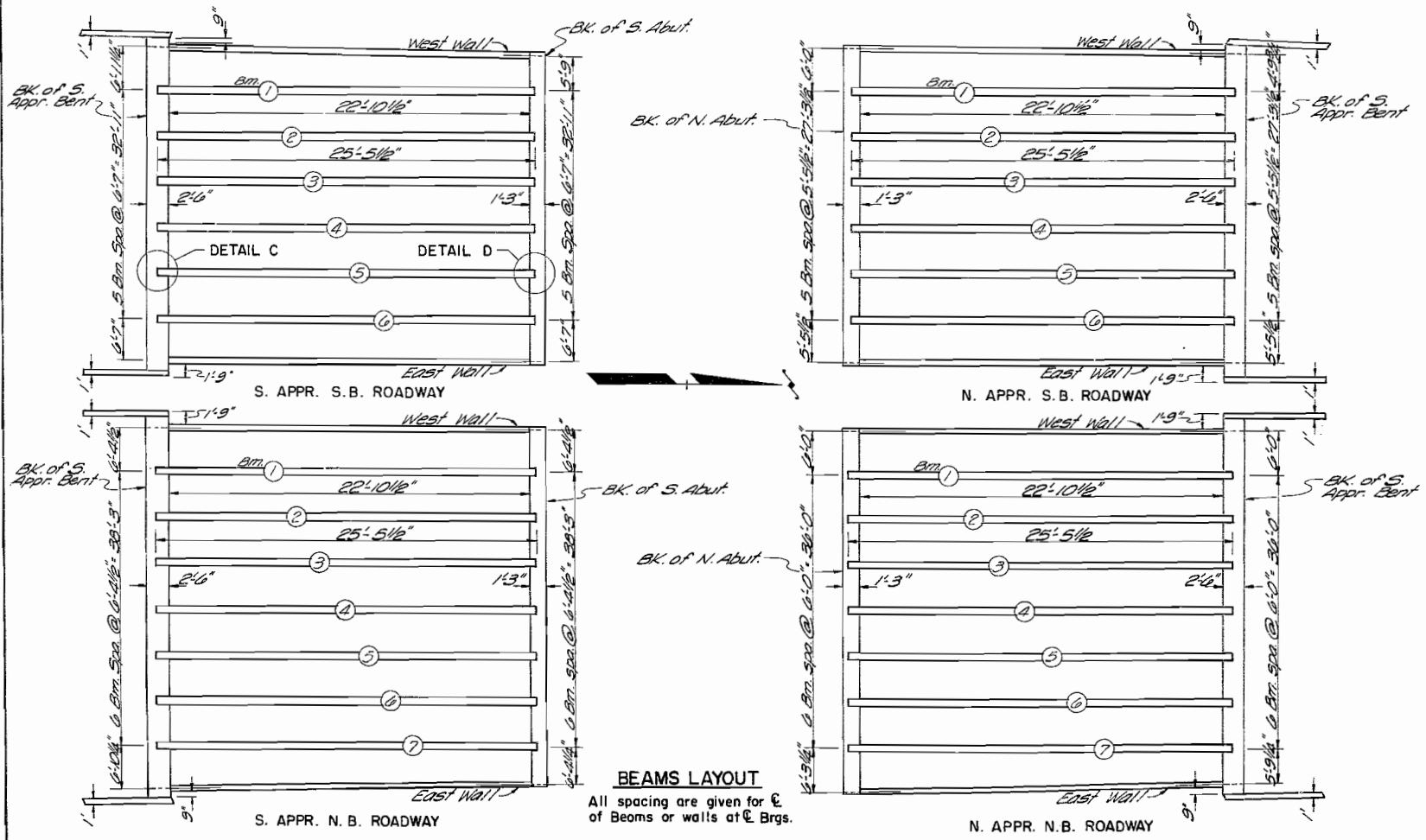
SECTION C-C



DETAIL C
 Typ. to all beams all Appr. Bents



DETAIL D
 Typ. to all beams all abuts.



BEAMS LAYOUT
 All spacing are given for \bar{c} of Beams or walls at \bar{c} Brgs.

*** BAR LIST**

Bar	No	Size	Length	Shape
G1	36	#4	2'-2 1/2"	TL
G2	12	#4	3'-9"	TL
G3	3	#5	25'-3"	—
G4	48	#3	2'-7"	L

* For one beam only

BILL OF MATERIAL

Item	Unit	Total
Furnishing & Erecting Precast Prestressed Concrete I-Beams, 36"	Lin Ft.	662

NOTES

All inserts and threaded rods for inserts, reinforcing and Prestressing Steel, and other items which are cast into the Precast Concrete I-Beams shall be included in the contract unit price per linear foot of "Furnishing And Erecting Precast Prestressed Concrete I-Beams, 36 in."

See Standard Specifications for additional information regarding materials, Prestressing equipment, construction and handling methods and other requirements for Precast Prestressed Concrete I-Beams. Prestressing Steel shall have a nominal diameter of 3/16".

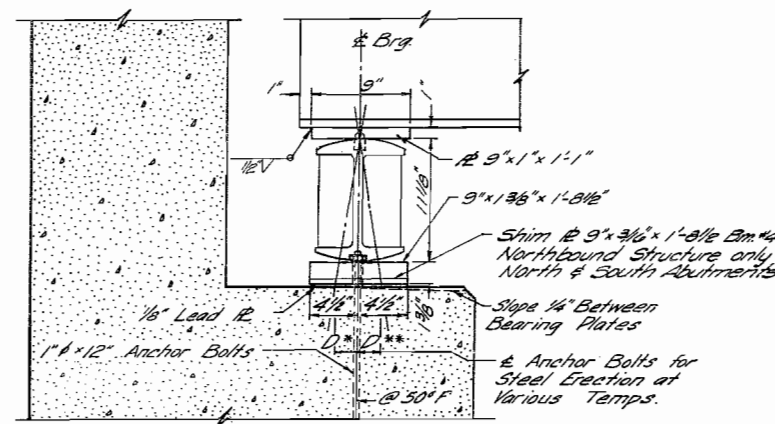
Inserts for 3/4" # Threaded rods are to be two strut, coil type for interior I-Beams and single coil, flared loop type for exterior I-Beams.

Steel for lifting hooks shall be ASTM A-306 Grade 70-80.

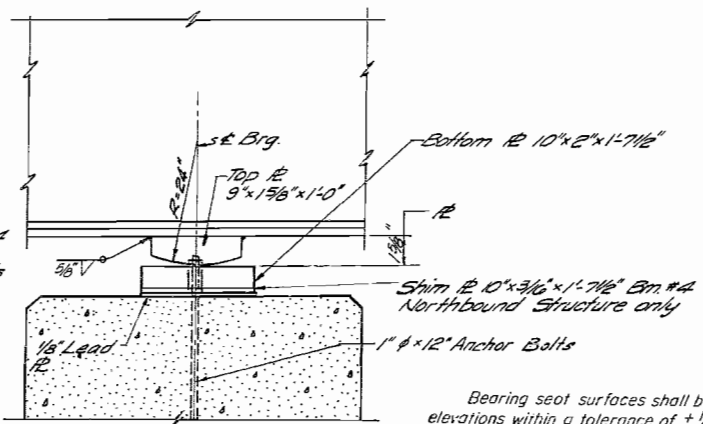
An alternate strand pattern using Extra High Strength Prestressing strand (270 ksi) is permitted. See Special Provisions.

BEAMS LAYOUT AND PRESTRESSED I-BM. DETAILS

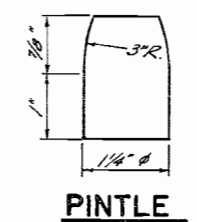
F.A.I. RT. 55 SEC. 57-1HB
 McLEAN COUNTY
 STATION 398+57.00



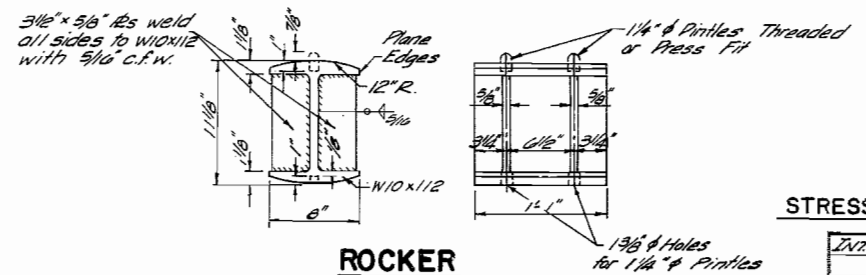
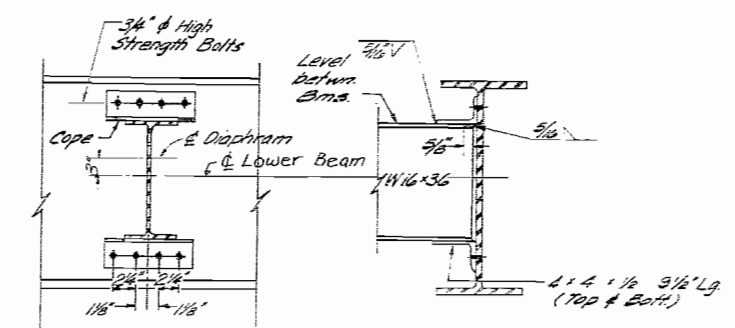
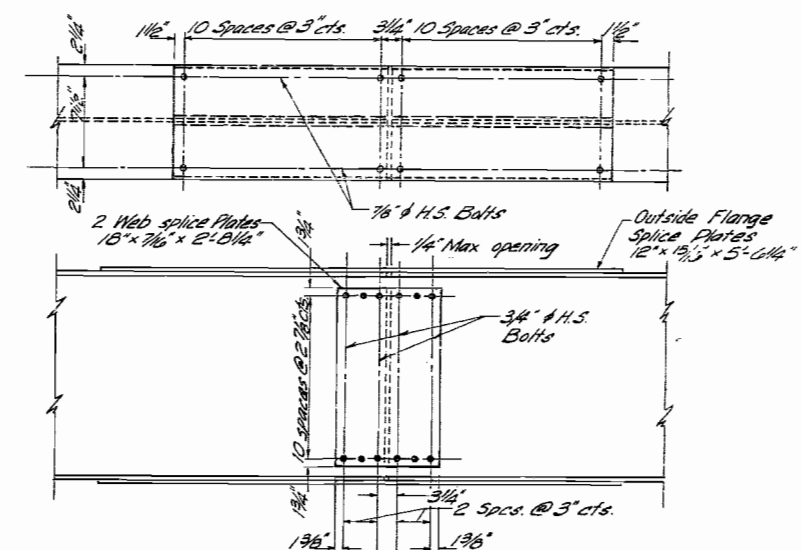
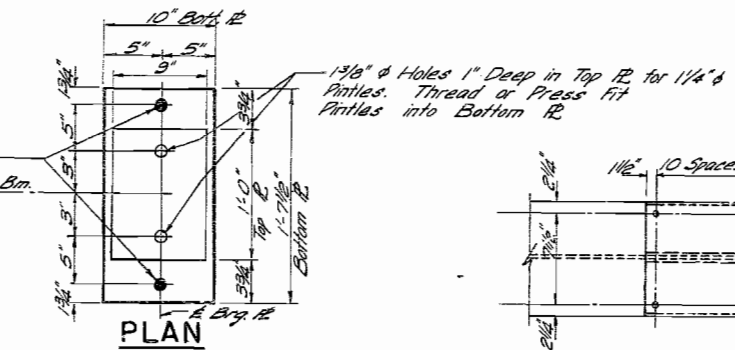
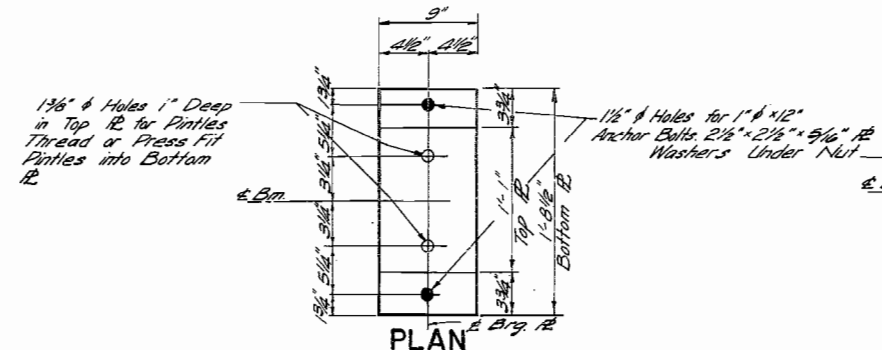
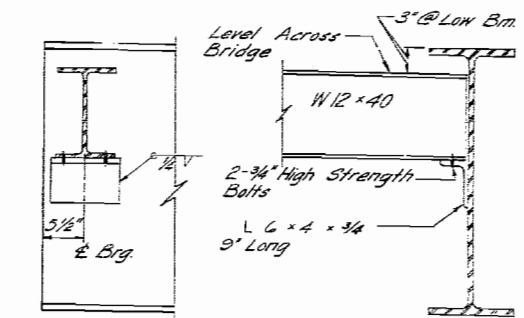
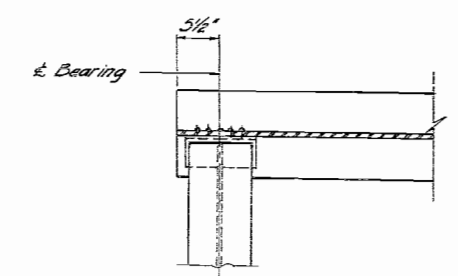
ELEVATION ABUTMENTS



ELEVATION PIERS



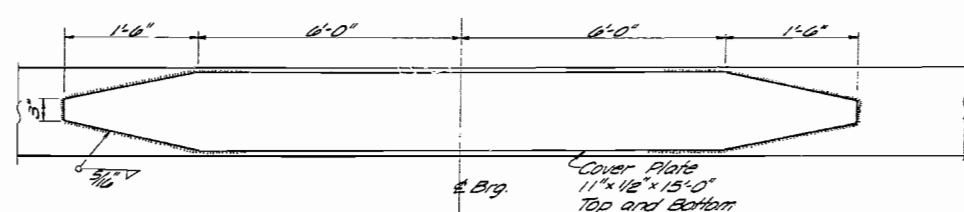
Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of ± 1/8 inch. Adjustment shall be made either by grinding the surface or by shimming the bearing. Two 1/8" adjusting shims, of the dimensions of the bottom bearing plate, shall be provided for each bearing in addition to all other plates or shims.



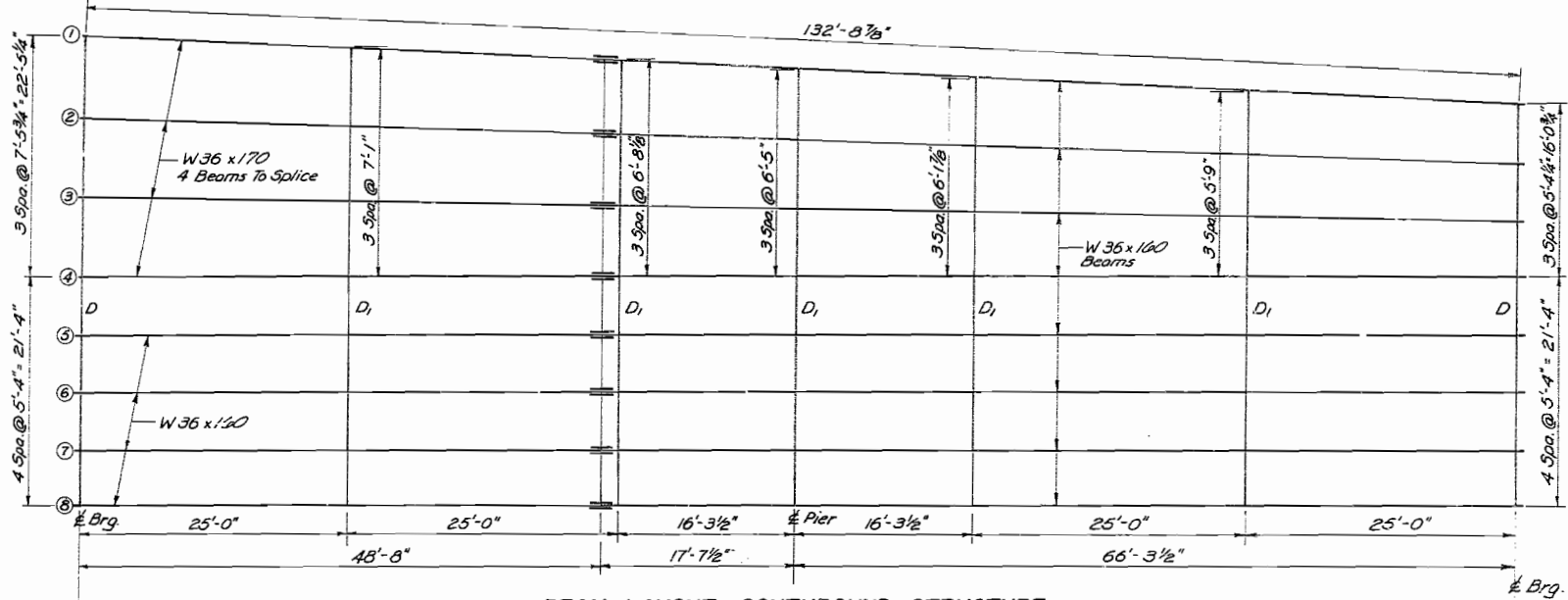
STRESS TABLE FOR N.B. BRIDGE

INT. BEAM MOMENT TABLE		
	0.4 Span	Pier
I ₂ (in ⁴)	976.0	13,423
S ₂ (in ³)	542	725.6
DL (Kil)	11.97	11.97
MDL (Kil)	350.3	-699.6
MLL (Kil)	407.5	362.4
MIMP (Kil)	100.5	94.7
M Total (Kil)	868.3	1150.7
R Total (Kil)	19.14	19.1

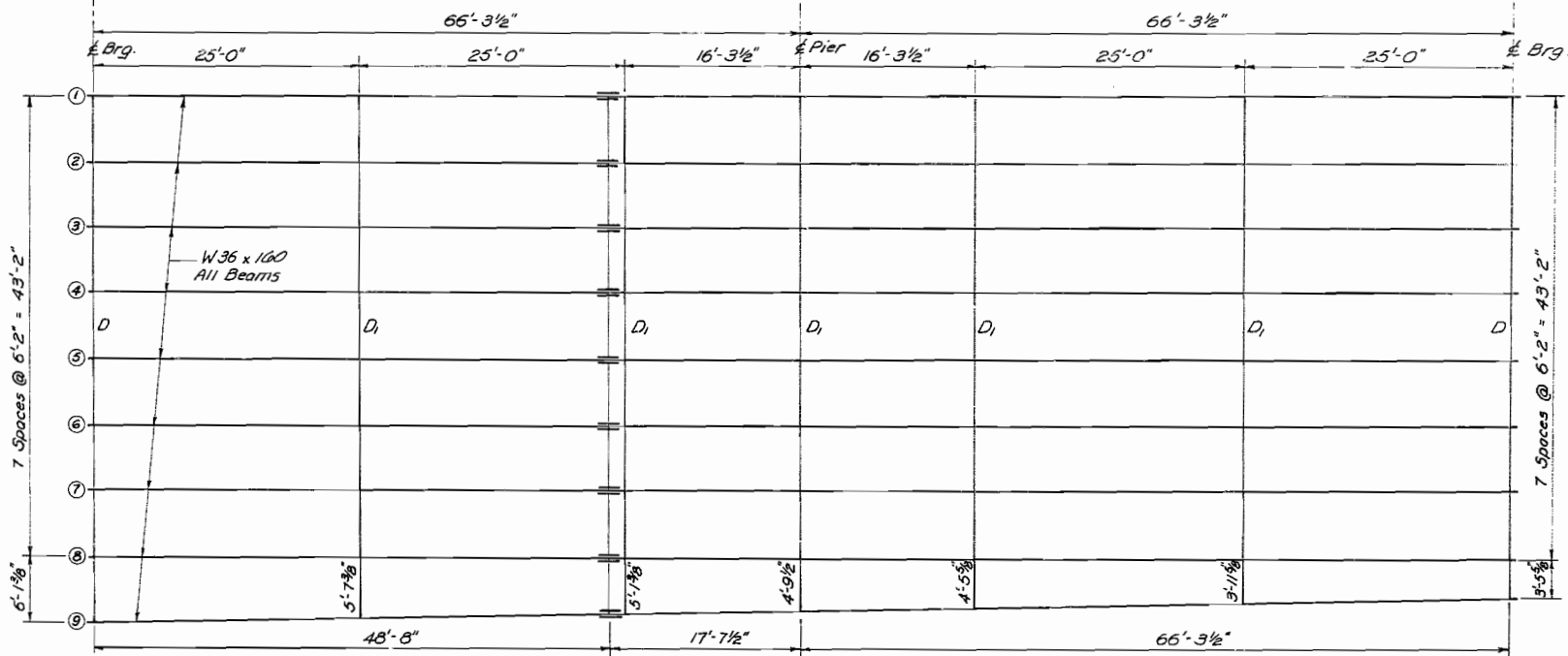
INT. BEAM REACTION TABLE		
	Abut	Pier
RDL (K)	29.1	100.5
RLl (K)	33.3	39.2
IMP (K)	8.7	10.3
R Total (K)	71.1	150.0



NOTES FOR SETTING OF ANCHOR BOLTS AT EXPANSION BEARINGS:
 a. D' (Side of brg away from fixed brg.)
 D' = 1/8" per each 100' of Expansion for every 15° fall below the normal temp. of 50°F
 D'' (Side of brg toward fixed brg.)
 D'' = 1/8" per each 100' of Expansion for every 15° rise above the normal temp. of 50°F
 b. After beams have been erected and dimensions D' & D'' determined, holes shall be drilled and anchor bolts shall be grouted in place. All fixed anchor bolts may be built into the masonry.



BEAM LAYOUT - SOUTHBOUND STRUCTURE



BEAM LAYOUT - NORTHBOUND STRUCTURE

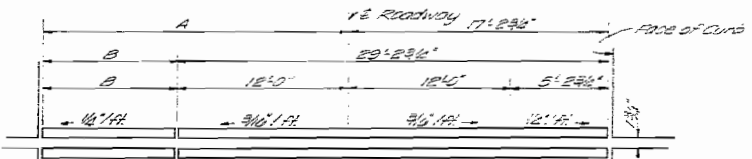
TOP OF BEAM ELEVATIONS*

BEAM NO.	SOUTH ABUT.	SPLICE	PIER	NORTH ABUT.
1	739.327	739.492	739.552	739.777
2	739.483	739.632	739.686	739.889
3	739.639	739.769	739.816	739.992
4	739.754	739.873	739.916	740.077
5	739.840	739.957	739.999	740.158
6	739.757	739.874	739.916	740.075
7	739.673	739.790	739.832	739.991
8	739.569	739.686	739.728	739.887
9	739.575	739.689	739.735	739.895
2	739.684	739.805	739.847	740.011
3	739.785	739.901	739.945	740.105
4	739.801	739.917	739.961	740.121
5	739.705	739.820	739.864	740.023
6	739.590	739.709	739.749	739.908
7	739.465	739.581	739.624	739.793
8	739.330	739.453	739.494	739.658
9	739.205	739.345	739.395	739.585

* For Fabrication Only.

LENGTHS OF FLARED BEAMS
 @ BRG. OF SOUTH ABUT. TO @ BRG. NORTH ABUT.

N.B. STR.	S.B. STR.			N.B. & S.B. STR.
BEAM 9	BEAM 1	BEAM 2	BEAM 3	ALL UNFLARED BEAMS
132'-7 1/8"	132'-8 7/8"	132'-7 7/8"	132'-7 1/2"	132'-7"

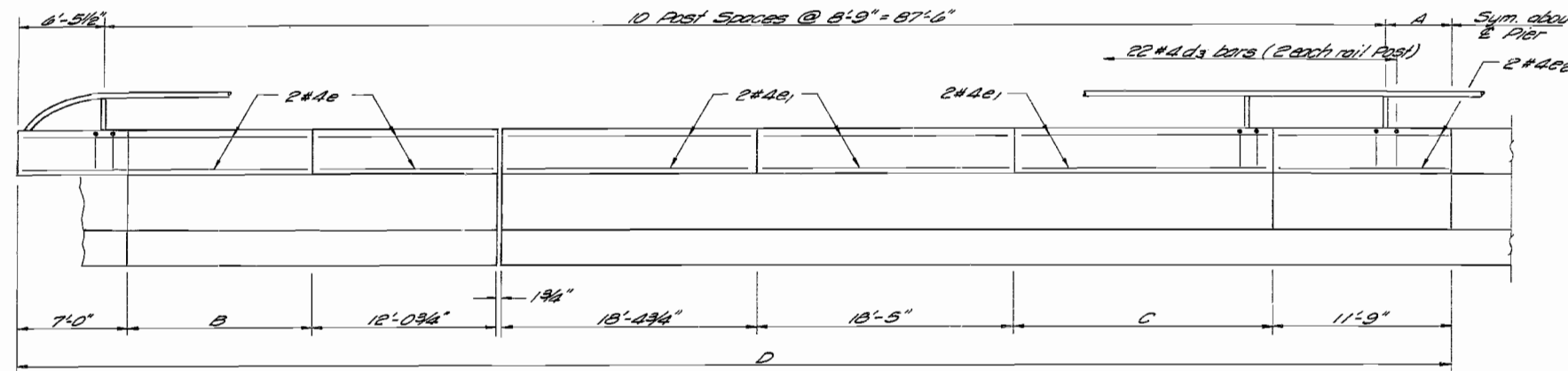


GUARD ANGLE DETAIL "A"

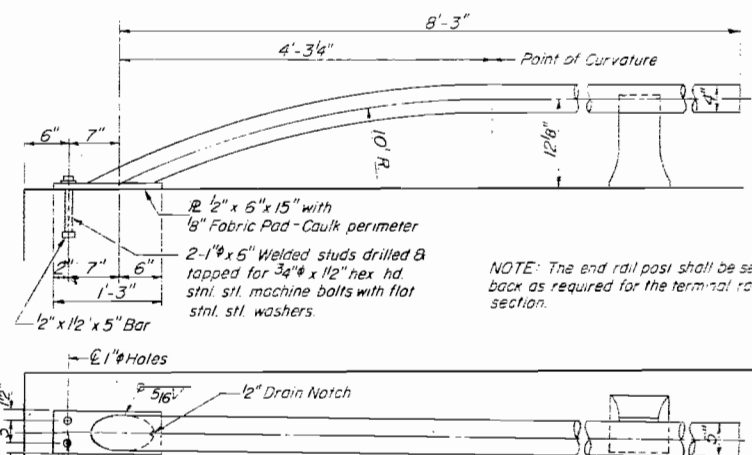
	A	B	N
N.B. Rdwy. S. Exp. Jt.	31'-8 1/8"	22'-9 1/8"	52
N.B. Rdwy. N. Exp. Jt.	32'-0 1/4"	20'-0 1/4"	48
S.B. Rdwy. S. Exp. Jt.	22'-6 1/4"	10'-6 1/4"	40
S.B. Rdwy. N. Exp. Jt.	23'-11 3/8"	16'-11 3/8"	46

BEAM LAYOUT
 FAI ROUTE 55 SEC. 57-1HB
 MC LEAN COUNTY
 STATION 398+57.00

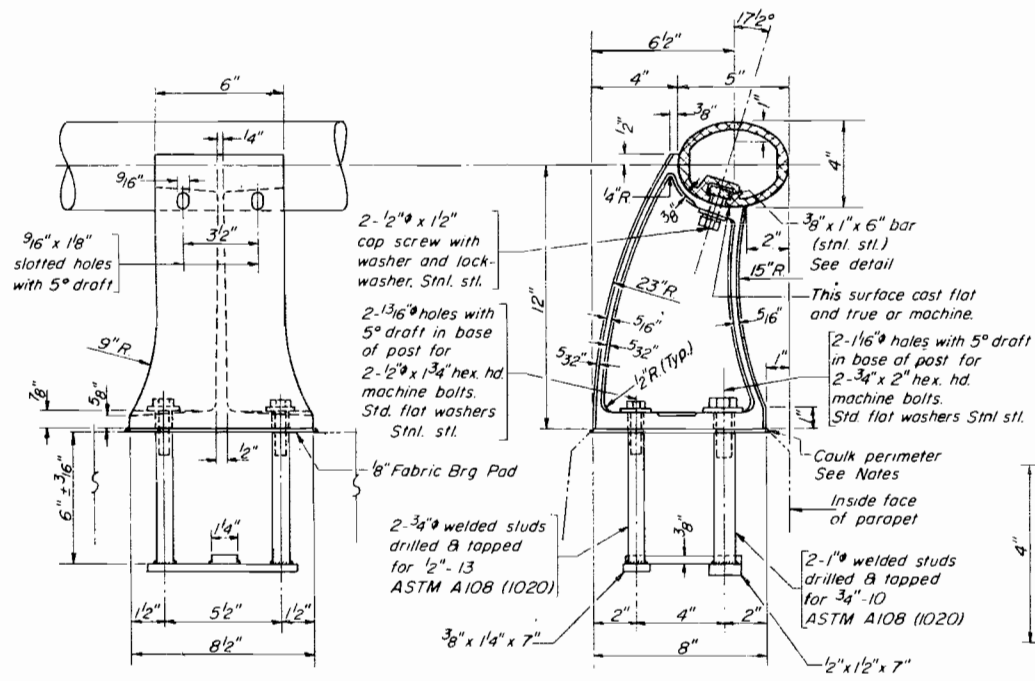
Sheet 14 of 25 Sheets



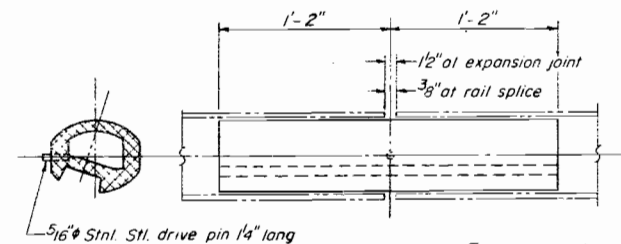
LOCATION	A	B	C	D
S.B. Br. Flared Side	4'-4 1/4"	12'-1 1/4"	13'-5 1/4"	98'-3 3/8"
N.B. Br. Flared Side	4'-3 1/4"	12'-0 3/4"	13'-4 3/4"	98'-2 3/8"
S.B. & N.B. Brs. Unflared Sides	4'-5"	12'-0 3/4"	13'-4 1/2"	98'-2 1/8"



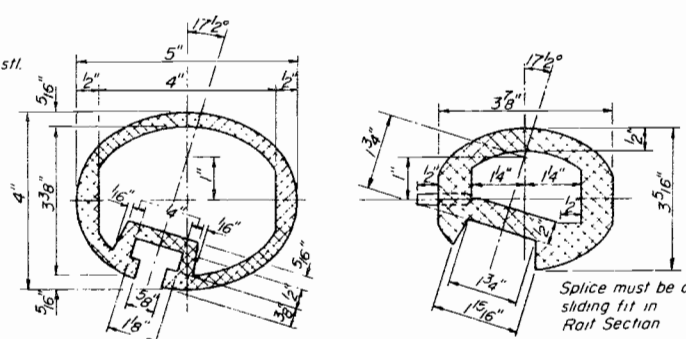
NOTE: The end rail post shall be set back as required for the terminal rail section.



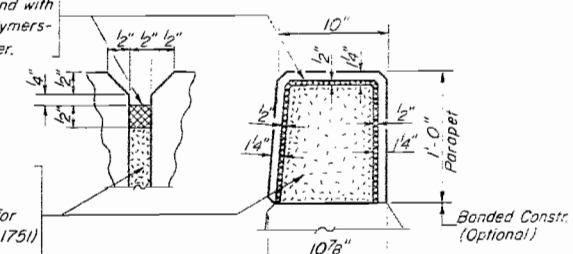
- 2 - 1/2" x 1/2" cap screw with washer and lock-washer. Stnl. stl.
- 2 - 1/16" holes with 5° draft in base of post for 2 - 1/2" x 1 3/4" hex. hd. machine bolts. Std. flat washers Stnl. stl.
- 8" Fabric Brg Pad
- 2 - 3/4" welded studs drilled & tapped for 1/2" - 13 ASTM A108 (1020)
- 3/8" x 1 1/4" x 7"
- 2 - 1/16" holes with 5° draft in base of post for 2 - 3/4" x 2" hex. hd. machine bolts. Std. flat washers Stnl. stl.
- 2 - 1" welded studs drilled & tapped for 3/4" - 10 ASTM A108 (1020)
- 1/2" x 1/2" x 7"



Two component non-staining gray sealing compound with polysulfide liquid polymers - gun grade with primer.

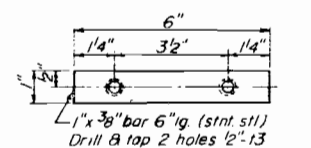
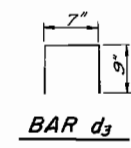


Splice must be a sliding fit in Rail Section



PARAPETS & RAILS BILL OF MATERIAL

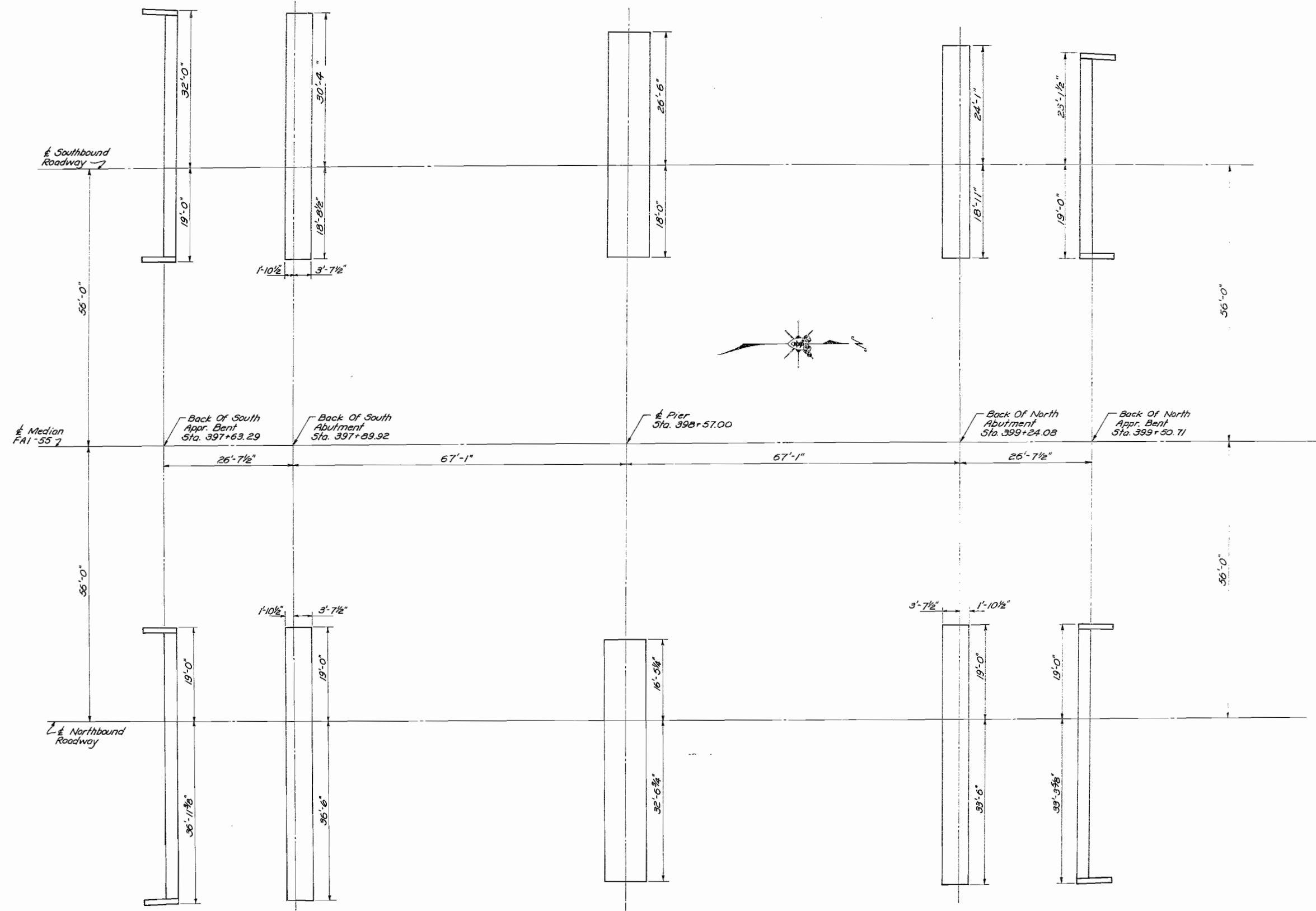
Bar	No.	Size	Length	Shape
e	64	#4	11'-9"	
e ₁	36	#4	18'-2"	
e ₂	32	#4	11'-0"	
e ₃	176	#4	2'-1"	
Reinforcement Bars			Lbs.	2160
Class X Concrete			Cu. Yds.	23.5
Aluminum Railing			Lin Ft	782



NOTES:

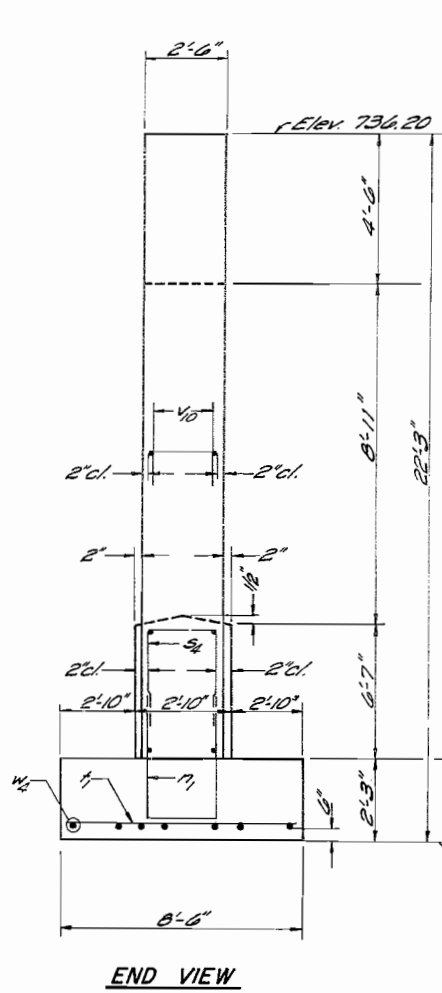
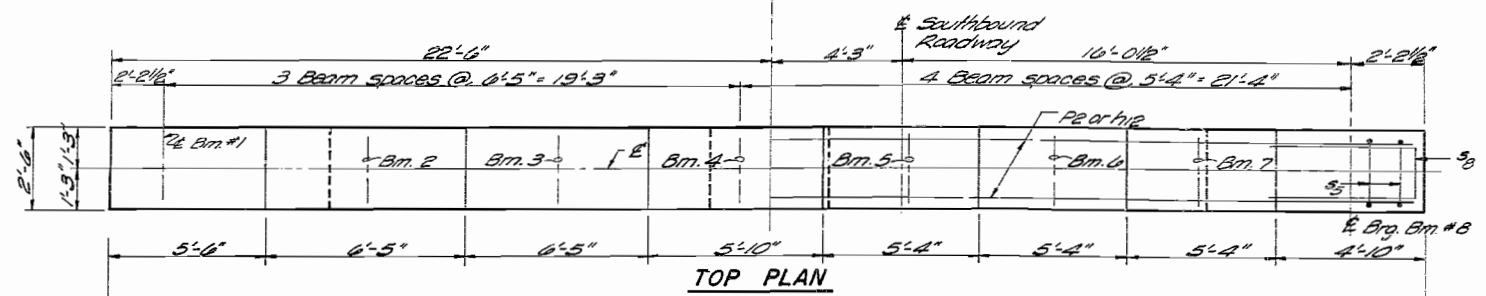
- All Aluminum Alloy Extruded Rail shall be supplied in modular lengths of 30 feet, except at the end of bridge or over open joints in bridge deck where the rail shall be attached to a minimum of 2 posts. If the rail is on a horizontal curve of 2300 foot radius or less, the modular lengths may be reduced but shall be attached to a minimum of 2 posts.
- All joints in rail shall be spliced per detail.
- Provide 1 - 8" and 2 - 1/6" Aluminum Shims for 25% of the Posts. Rail element shall be parallel to Grade - high spots shall be ground and low spots shimmed.
- Seal perimeter of base of post to parapet with two component non-staining gray sealing compound with polysulfide liquid polymers, gun grade with primer. Fabric Bearing Pad shall have same dimensions as base of post.
- Aluminum alloy rail shall conform to ASTM B221 alloy 6061-T6 or 6351-T5 with min yield 35 ksi, min tensile 38 ksi, and elongation of 10% in 2 inches.

ALUMINUM RAILING
 FAI ROUTE 55 SEC. 57-1HB
 MC LEAN COUNTY
 STATION 398+57.00

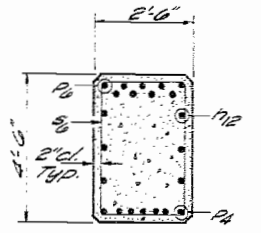
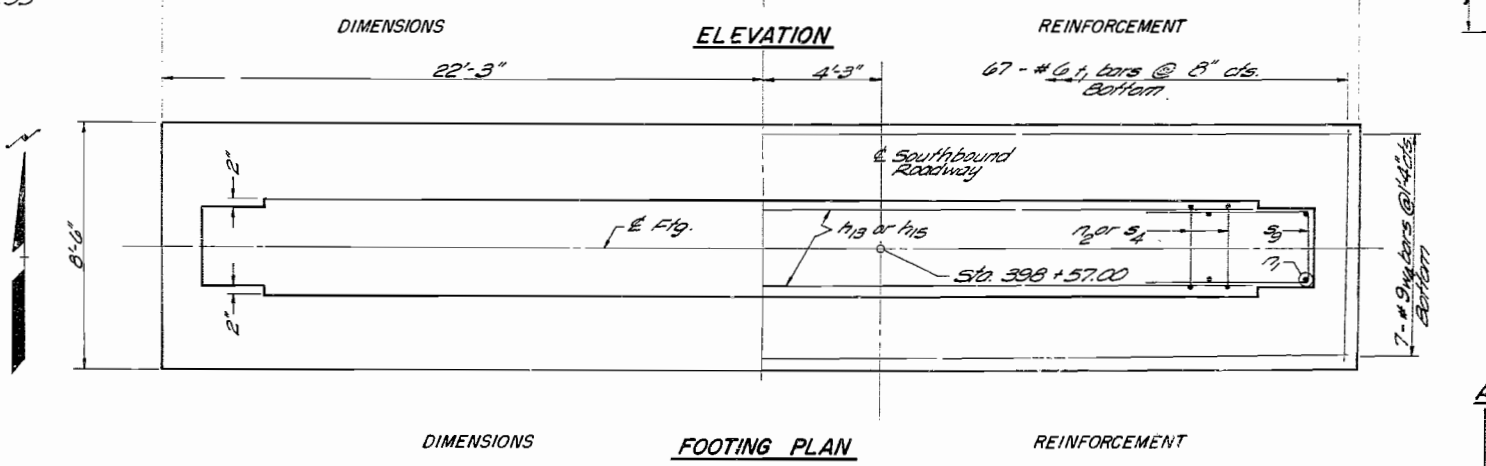
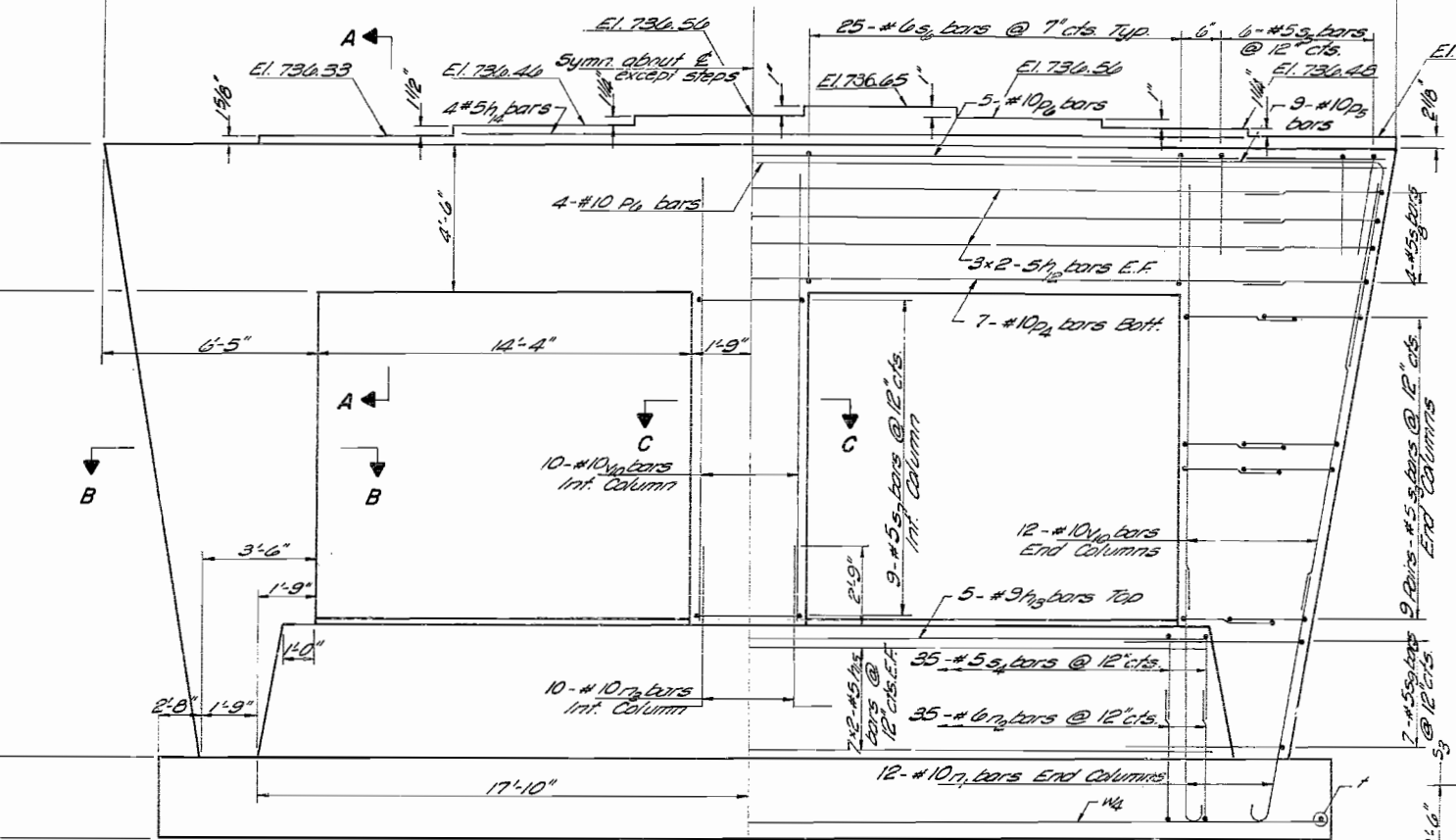


FOOTING LAYOUT
 FAI ROUTE 55 SEC. 57-IHB
 MC LEAN COUNTY
 STATION 398+57.00

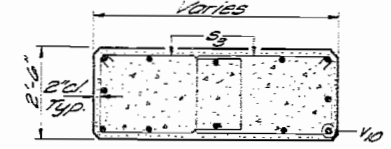
Notes:
 Space reinforcement in opp to miss anchor balls.
 All edges shall have standard 3/4" chamfers except as noted.
 Four steps monolithically with cap.



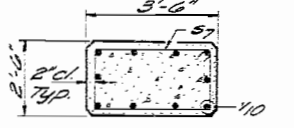
Max. Soil Pressure = 2.1 T.S.F.
 Min. Soil Pressure = 0.94 T.S.F.



SECTION A-A



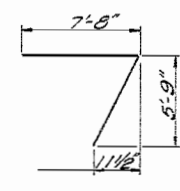
SECTION B-B



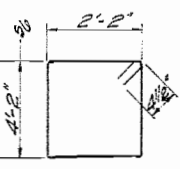
SECTION C-C



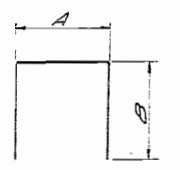
BAR n2



BAR P5



BARS s3, s6, s7



BARS n2, s4, s5, s8, s9

A & B DIMENSIONS

Bar	A	B
n2	2'-6"	6'-6"
s5	2'-2"	3'-0"
s4	2'-6"	3'-0"
s9	2'-2"	4'-6"
s8	2'-2"	3'-0"

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
n12	12	#5	21'-9"	
n13	5	#9	34'-0"	
n14	4	#5	28'-0"	
n15	28	#5	16'-5"	
n1	24	#10	12'-0"	
n2	35	#6	15'-6"	
n3	10	#10	5'-9"	
n4	7	#10	43'-6"	
n5	18	#10	13'-6"	
n6	9	#10	44'-9"	
s3	14	#5	11'-2"	
s4	36	#5	12'-1"	
s5	35	#5	8'-6"	
s6	12	#5	8'-2"	
s7	50	#6	13'-5"	
s8	9	#5	11'-5"	
s9	8	#5	8'-2"	
n7	67	#6	8'-5"	
n8	34	#10	12'-18"	
n9	12	#9	23'-6"	

Class X Concrete Cu. Yds. 89.7
 Reinforcement Bars Lbs. 13,890

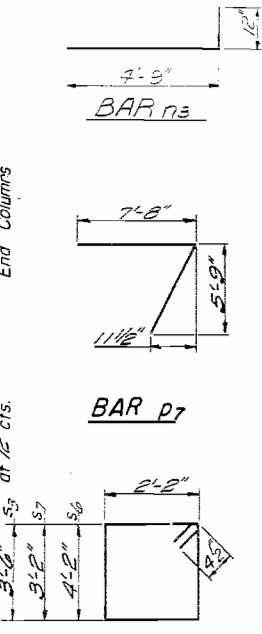
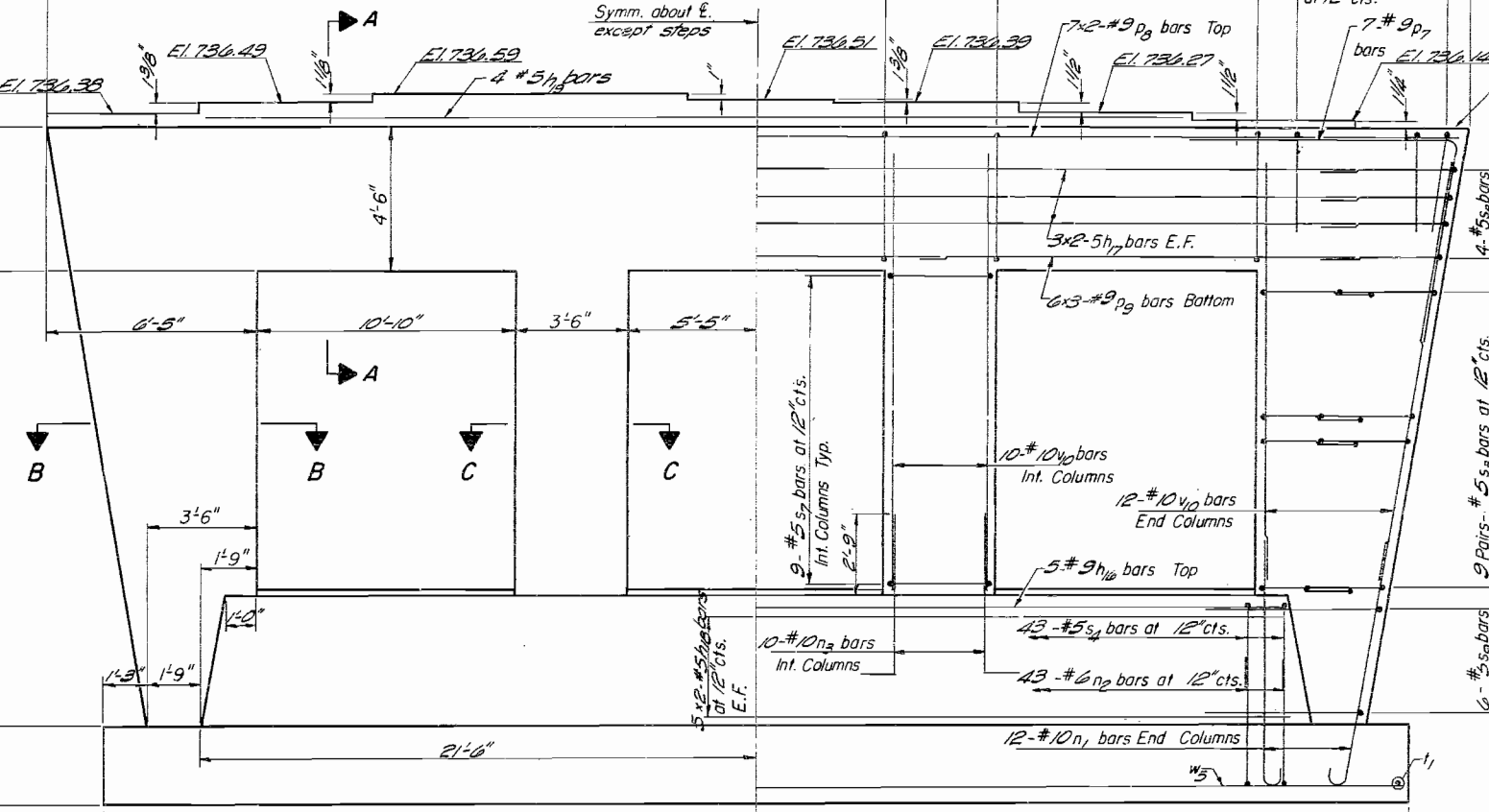
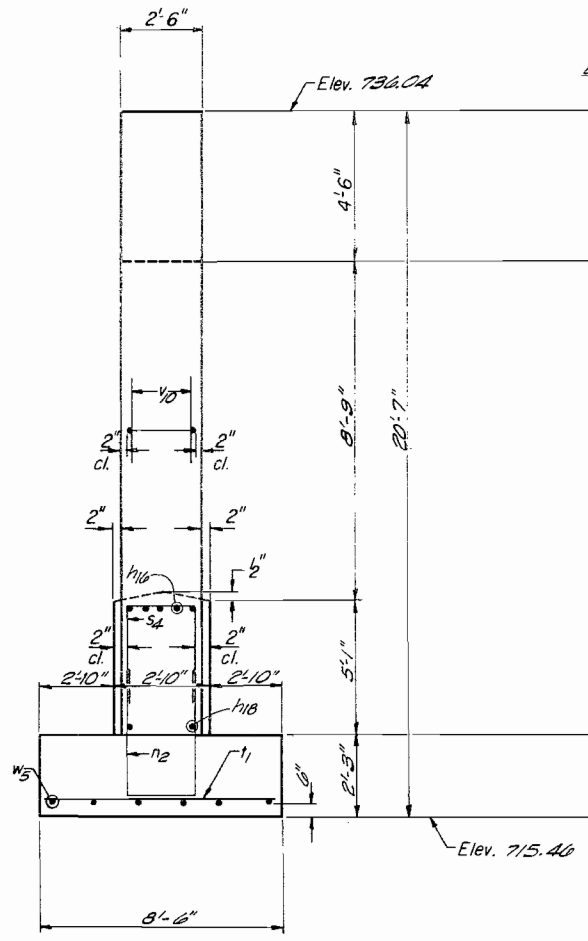
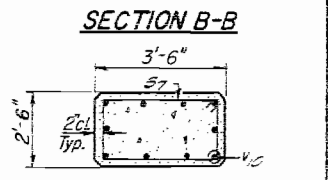
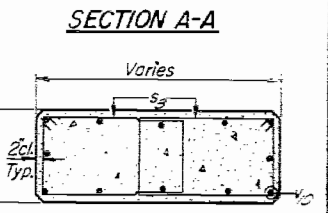
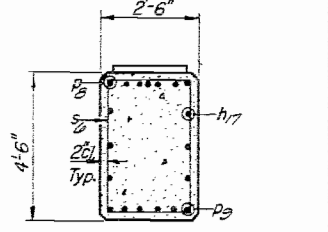
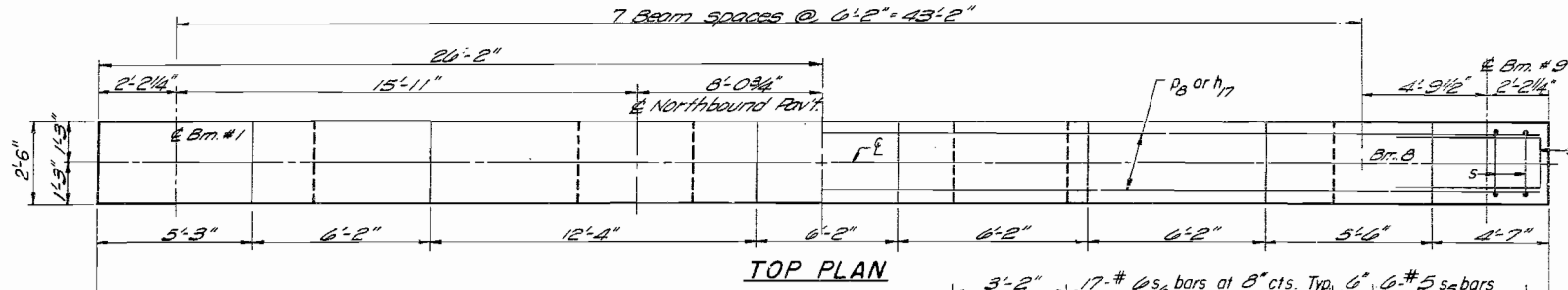
PIER
 SOUTHBOUND ROADWAY
 FAI ROUTE 55 SEC. 57-118
 MCI EAN COUNTY
 STA. 398 + 57.00

NOTES

Space reinforcement in cap to miss anchor bolts.
 All edges shall have standard $\frac{3}{4}$ " chamfers except as noted.
 Pour steps monolithically with cap.

PROJECT NO.	SEC.	COUNTY	SHEET	TOTAL SHEETS
FAI-55 57-1HB	McLean	56	38	
STA.	TO STA.			

SHEET 17 OF 25 SHEETS

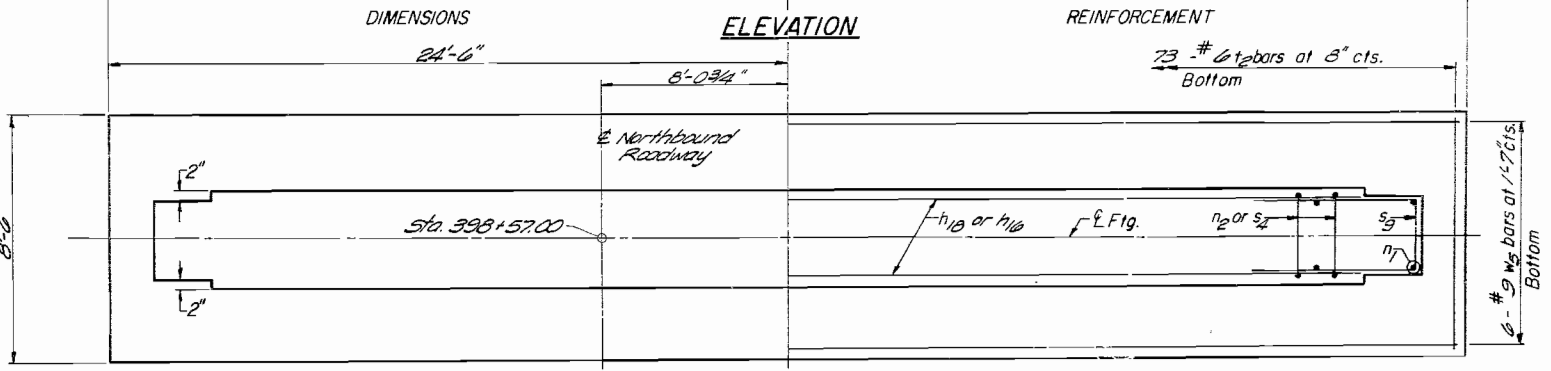


BILL OF MATERIAL

Bar	No.	Size	Length	Shape
n2	5	#9	41'-0"	
h7	12	#5	24'-0"	
n3	20	#5	22'-0"	
n4	4	#5	36'-9"	
n1	24	#10	12'-0"	
n2	43	#6	15'-6"	
n3	20	#10	5'-9"	
p7	14	#9	13'-6"	7
p8	14	#9	27'-0"	
p9	18	#9	18'-0"	
s2	43	#5	8'-6"	
s3	20	#5	8'-2"	
s4	12	#5	11'-2"	
s7	18	#5	11'-2"	
s3	51	#6	18'-5"	
s3	36	#5	12'-1"	
s8	3	#5	8'-2"	
v10	44	#10	12'-0"	
w2	12	#9	20'-0"	

Class X Concrete	Cu. Yds.	36.5
Reinforcement Bars	Lbs.	14,100

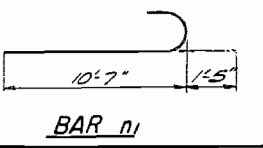
PIER
 NORTHBOUND ROADWAY
 FAI ROUTE 55 SEC. 57-1HB
 MC LEAN COUNTY
 STA. 398 + 57.00

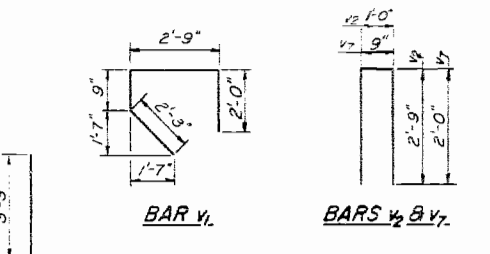
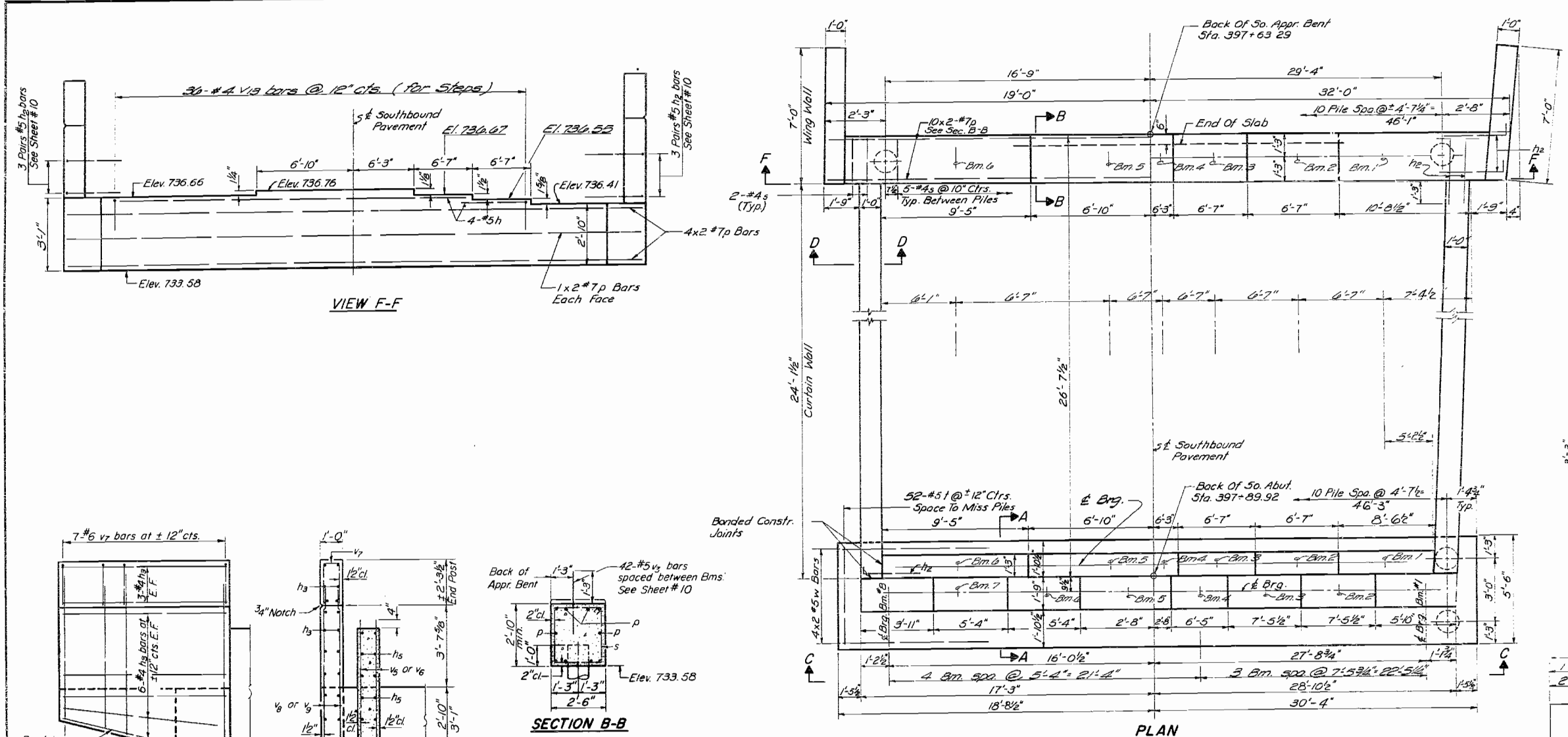


A & B DIMENSIONS

Bar	A	B
n2	2'-6"	4'-6"
s2	2'-6"	3'-0"
s5	2'-2"	3'-0"
s9	2'-2"	4'-6"
s8	2'-2"	3'-0"

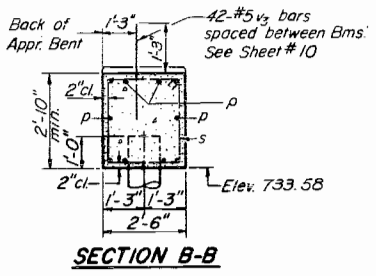
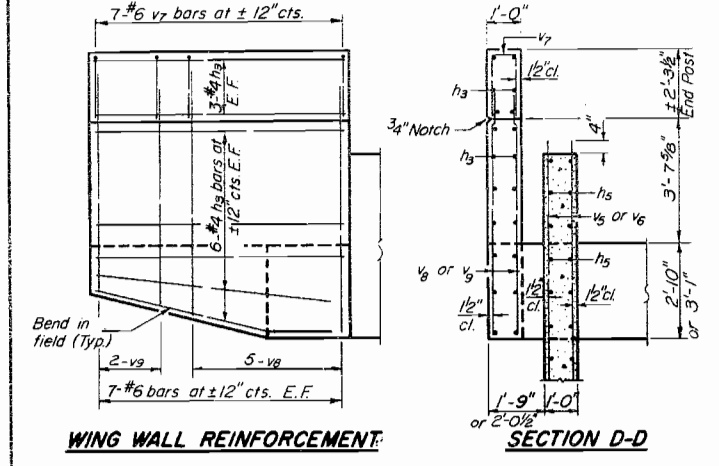
Max. Soil Pressure = 2.1 T.S.F.
 MIN. Soil Pressure = 0.98 T.S.F.





BILL OF MATERIAL

Bar	No	Size	Length	Shape
n	10	#5	29'-6"	—
h1	64	#5	24'-0"	—
h2	82	#5	3'-9"	—
h3	36	#5	6'-9"	—
h4	18	#5	21'-6"	—
h5	24	#5	24'-10"	—
h6	4	#6	24'-6"	—
V11	30	#4	4'-5"	—
V12	30	#4	4'-11"	—
V13	30	#4	5'-10"	—
n	102	#5	4'-3"	—
p	20	#7	26'-3"	—
s	54	#4	10'-1"	—
l	52	#5	5'-3"	—
v	94	#5	12'-8"	—
v1	45	#5	7'-9"	—
v2	45	#5	6'-6"	—
v3	92	#5	2'-6"	—
v4	12	#5	16'-9"	—
v5	36	#5	20'-10"	—
v6	16	#5	5'-9"	—
v7	14	#6	4'-9"	—
v8	20	#6	7'-9"	—
v9	8	#6	7'-0"	—
w	8	#5	25'-6"	—
Reinforcement Bars		Lbs	10,070	
Class X Concrete		Cu Yds	106.0	
Concrete Piles		Lin. Ft.	1,243	

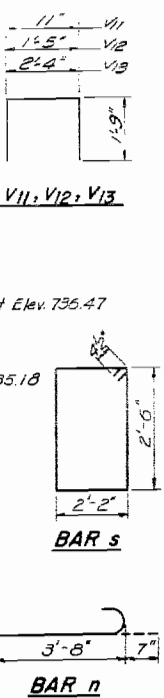
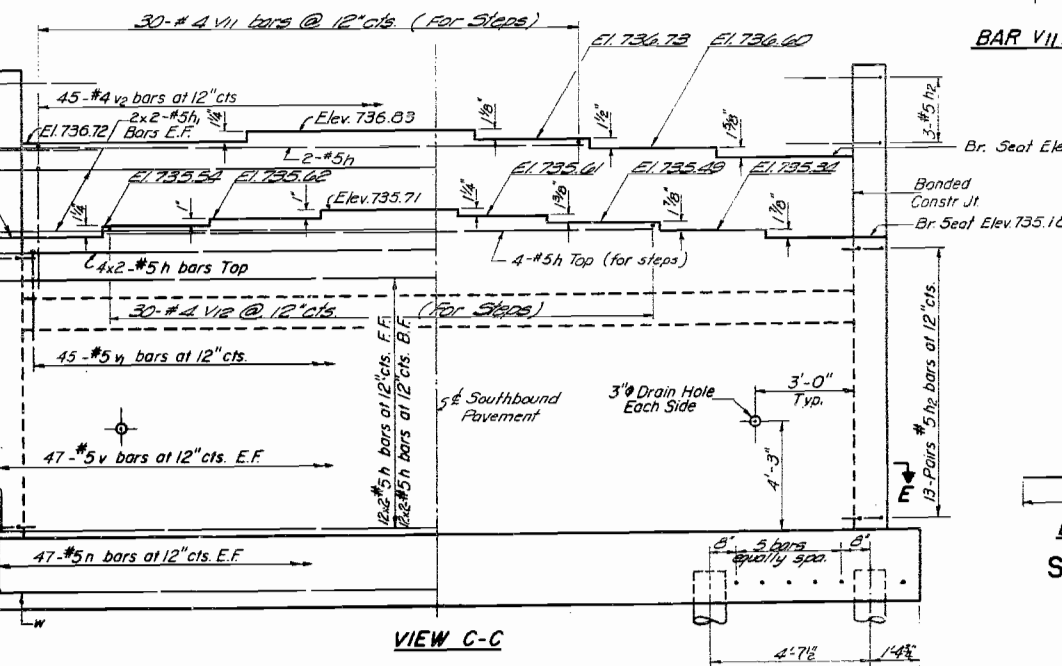
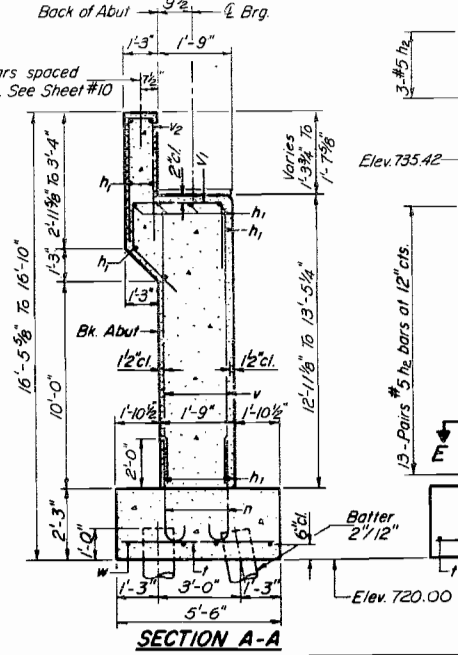


ABUT. PILE DATA

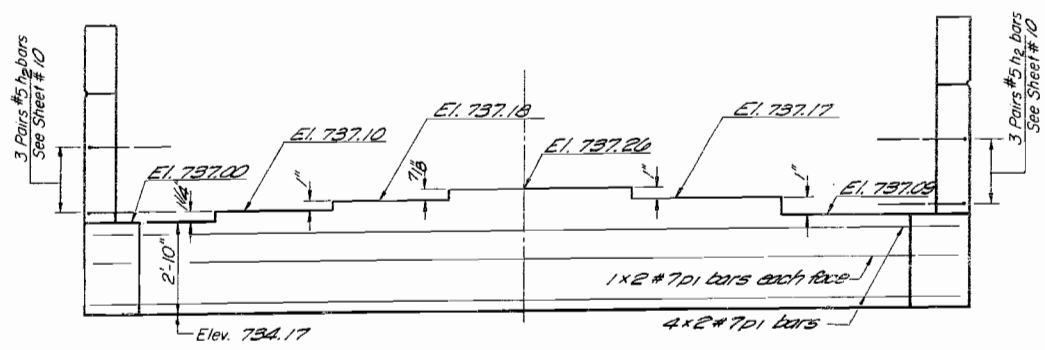
Type	- Concrete
Capacity	- 30 Tons
Est. Length	- 34 Feet
No. Req'd.	- 22

APPR. BENT PILE DATA

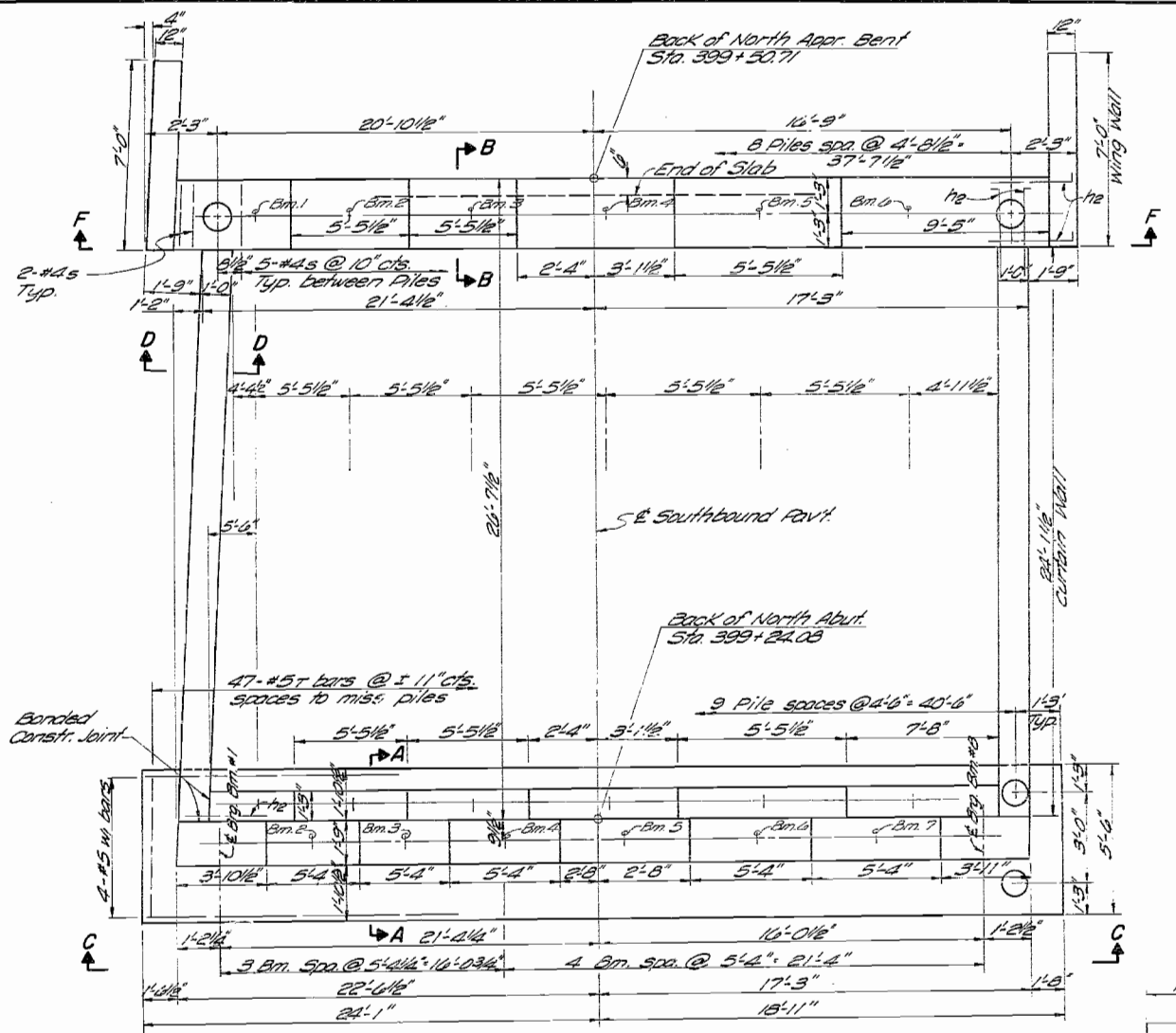
Type	- Concrete
Capacity	- 30 Tons
Est. Length	- 45 Feet
No. Req'd.	- 11



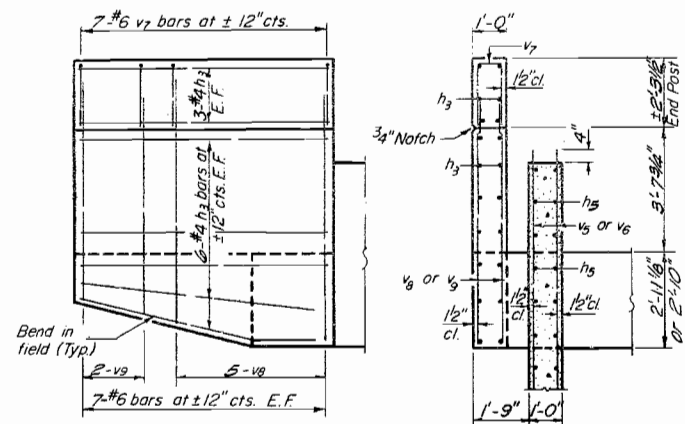
SOUTH ABUT. & SOUTH APPR. BENT
 SOUTHBOUND ROADWAY
 FAI ROUTE 55 SEC. 57-1HB
 MC LEAN COUNTY
 STATION 398+57.00



VIEW F-F

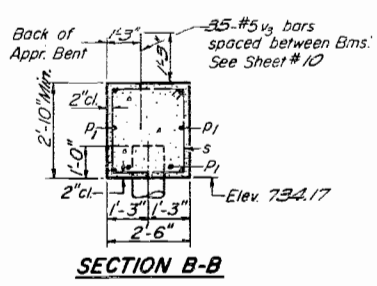


PLAN



WING WALL REINFORCEMENT

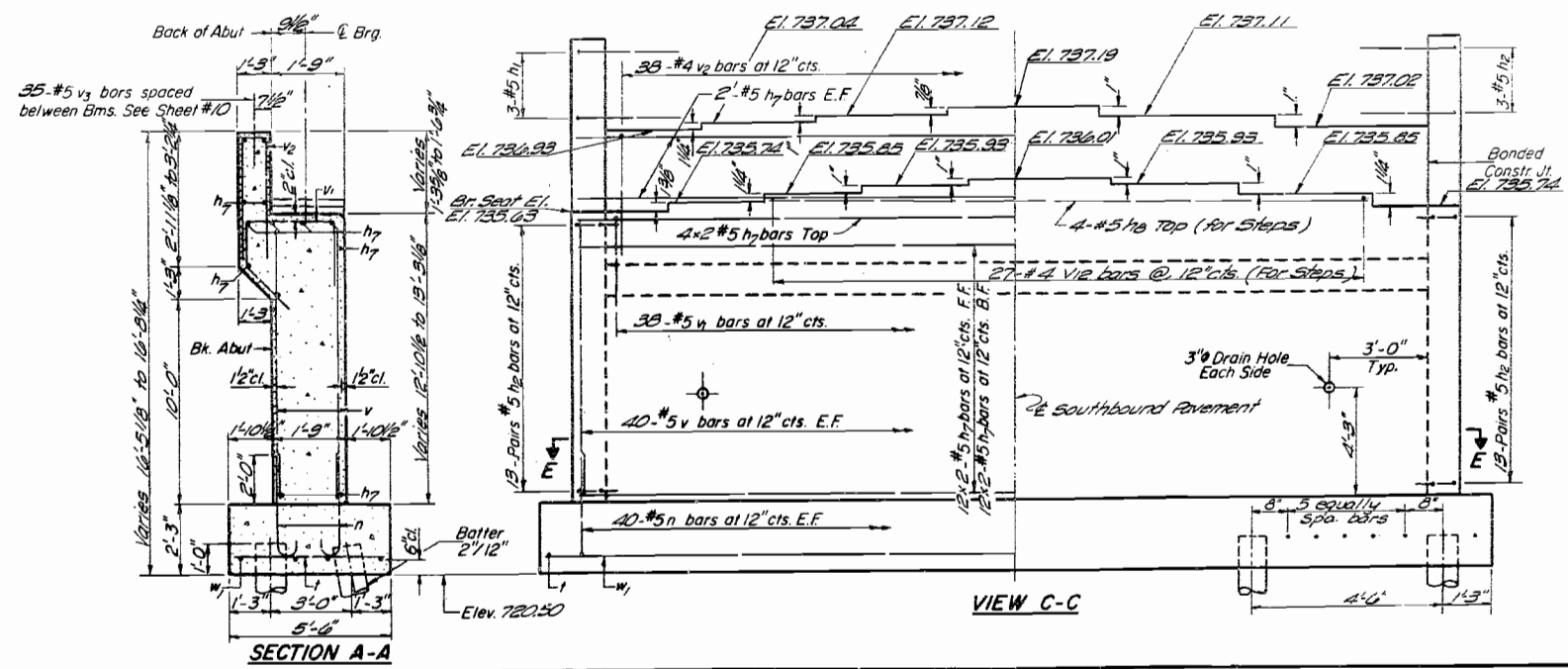
SECTION D-D



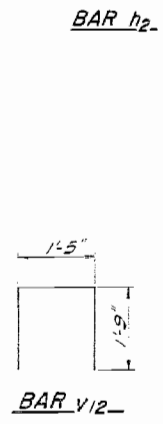
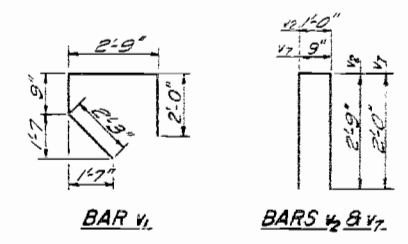
SECTION B-B

ABUT. PILE DATA
 Type ~ Concrete
 Capacity 30 Tons
 Est. Length 31
 No. Reqd 20

ABUT. BENT PILE DATA
 Type Concrete
 Capacity 30 Tons
 Est. Length 42
 No. Reqd. 8 + 1 Test Pile



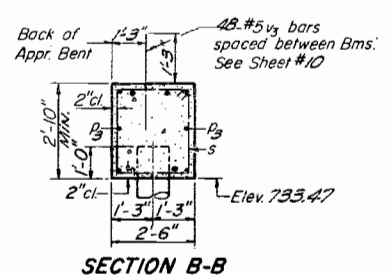
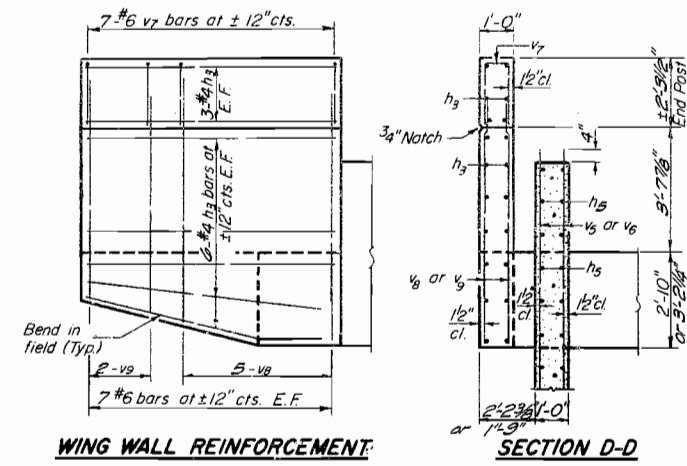
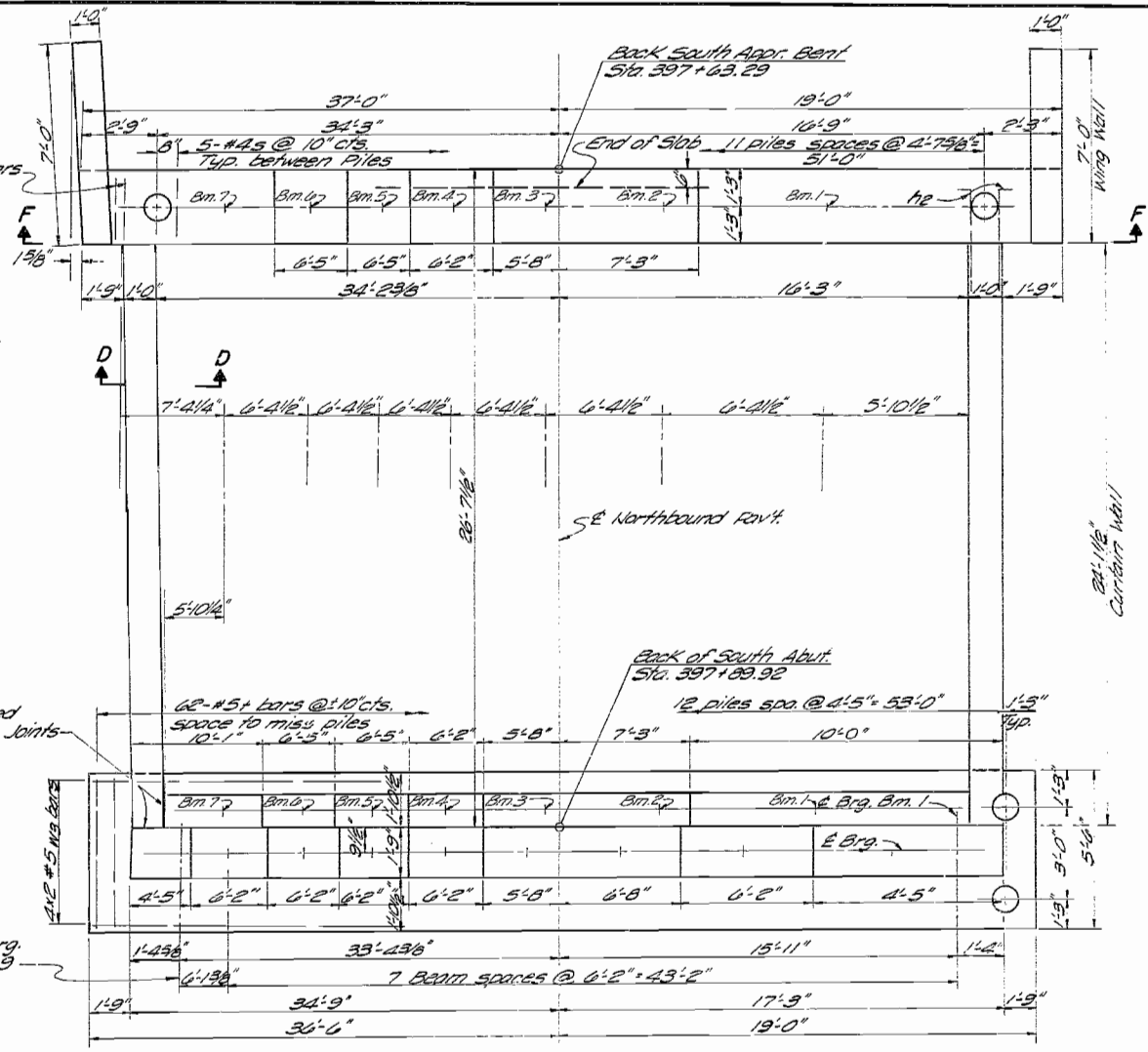
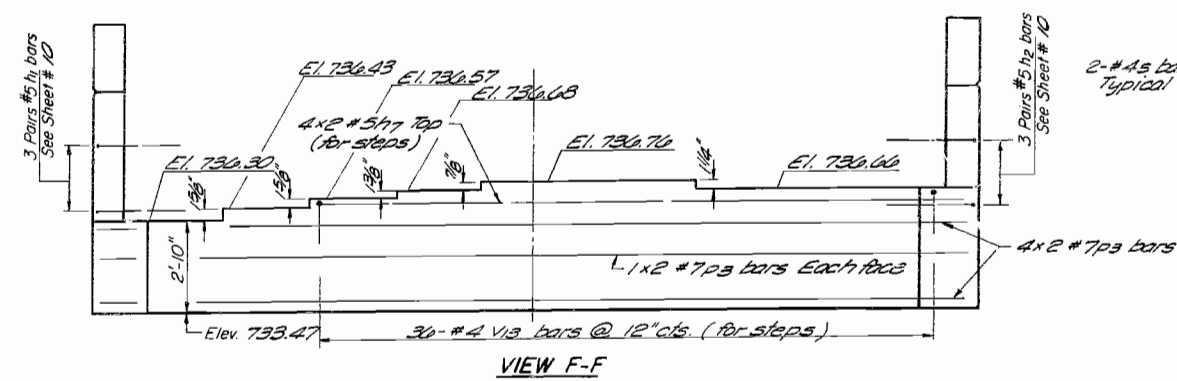
VIEW C-C



BILL OF MATERIAL

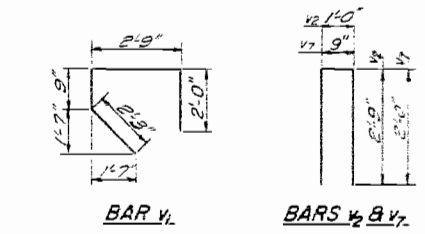
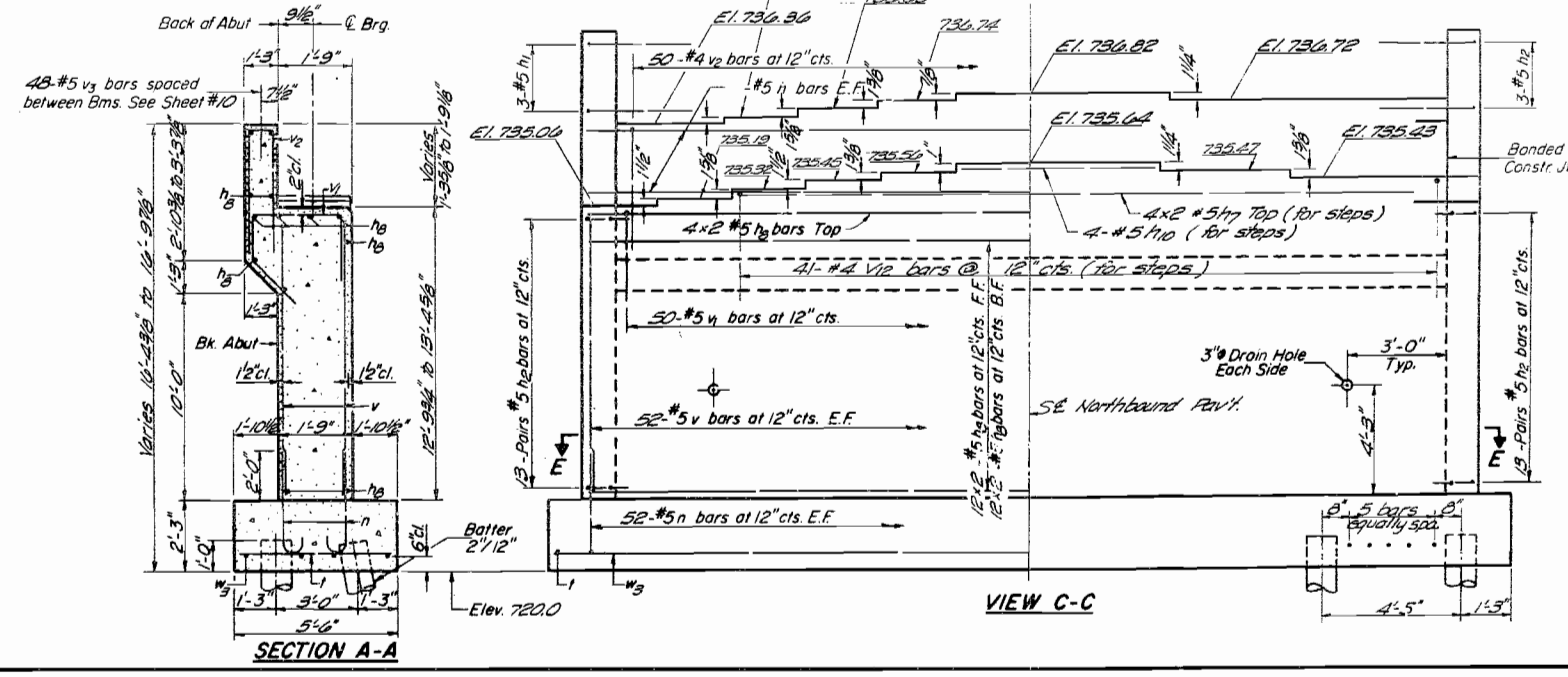
Bar	No	Size	Length	Shape
h2	82	#5	5'-9"	U
h3	30	#5	6'-9"	U
h4	18	#5	21'-6"	U
h5	24	#5	24'-10"	U
h6	4	#6	24'-6"	U
h7	64	#5	20'-6"	U
h8	4	#5	28'-6"	U
n	88	#5	4'-9"	U
p1	20	#7	22'-0"	U
s	44	#4	10'-1"	U
1	47	#5	5'-5"	U
v	80	#5	12'-8"	U
v1	38	#5	7'-9"	U
v2	38	#5	6'-0"	U
v3	78	#5	2'-6"	U
v4	18	#5	10'-9"	U
v5	38	#5	20'-10"	U
v6	10	#5	5'-9"	U
v7	14	#6	4'-9"	U
v8	20	#6	7'-9"	U
v9	8	#6	7'-0"	U
v12	27	#4	4'-1"	U
w1	8	#5	22'-0"	U
Reinforcement Bars		Lbs	8,720	
Class X Concrete		Cu Yds	22.2	
Concrete Piles		LIN FT	552	
Test Piles		Each	1	

NO. ABUT. & NO. APPR. BENT
 SOUTHBOUND ROADWAY
 FAI ROUTE 55 SEC. 57-1HB
 MCLEAN COUNTY
 STA. 398 + 57.00



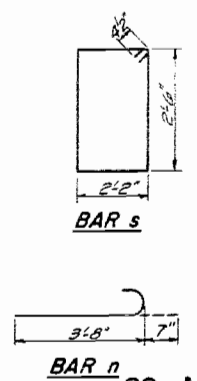
ABUT. PILE DATA
 Type Concrete
 Capacity 30 Tons
 Est. Length 34'
 No. Reg'd. 23 + 1 Test Pile

ABUT. BENT PILE DATA
 Type Concrete
 Capacity 30 Tons
 Est. Length 15'
 No. Reg'd. 12



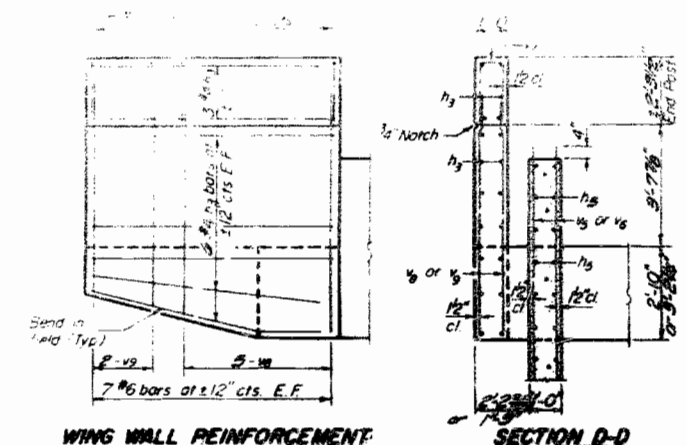
BILL OF MATERIAL

Bar	No	Size	Length	Shape
h2	4	#5	12'-0"	—
h2	32	#5	3'-9"	J
h2	30	#5	6'-9"	—
h2	18	#5	2'-6"	—
h2	24	#5	24'-0"	—
h2	4	#6	24'-0"	—
h2	10	#5	20'-8"	—
h2	64	#5	24'-6"	—
n	112	#5	4'-9"	J
Pa	20	#7	28'-9"	—
s	39	#4	10'-1"	□
1	62	#5	5'-5"	—
V12	41	#4	4'-1"	—
V	104	#5	12'-8"	—
V1	50	#5	7'-9"	□
V2	50	#5	6'-6"	□
V3	104	#5	2'-6"	—
V4	12	#5	10'-9"	—
V5	39	#5	20'-0"	—
V6	12	#5	5'-9"	—
V7	4	#6	4'-9"	□
V8	20	#6	7'-9"	—
V9	8	#6	7'-0"	—
V13	30	#4	5'-0"	—
W2	8	#5	28'-3"	—
Reinforcement Bars		Lbs.	5760	
Class X Concrete		Cu Yds.	115.2	
Concrete Piles		Lbs.	1,322	
Test Piles Concrete		Ed.	1	



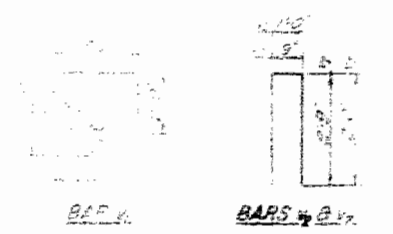
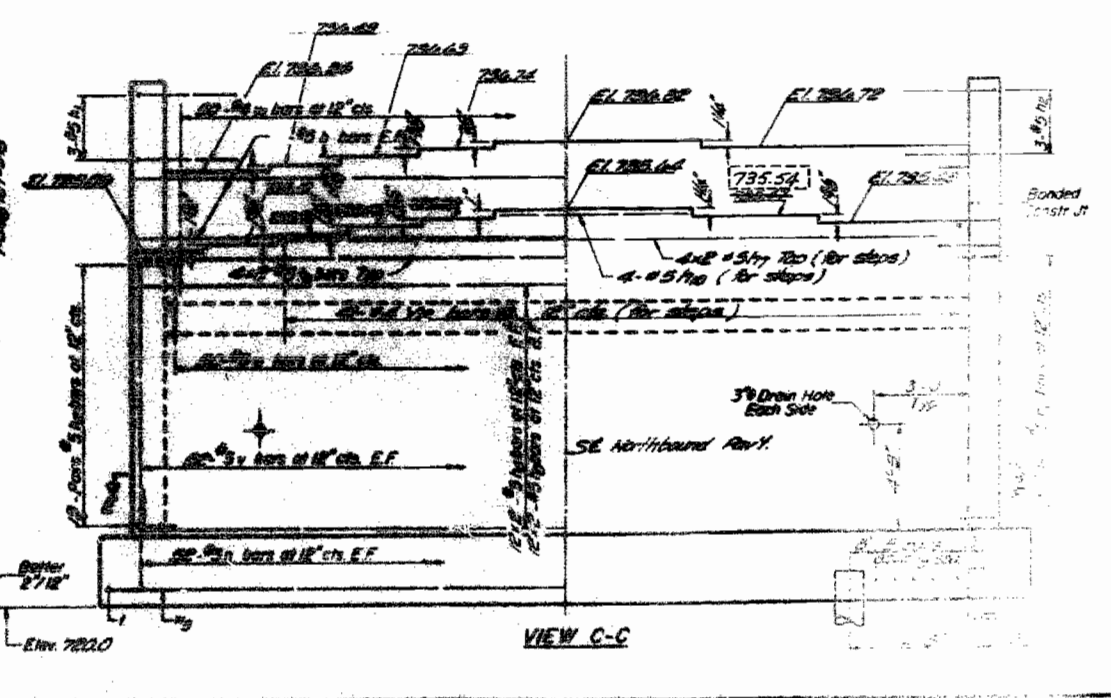
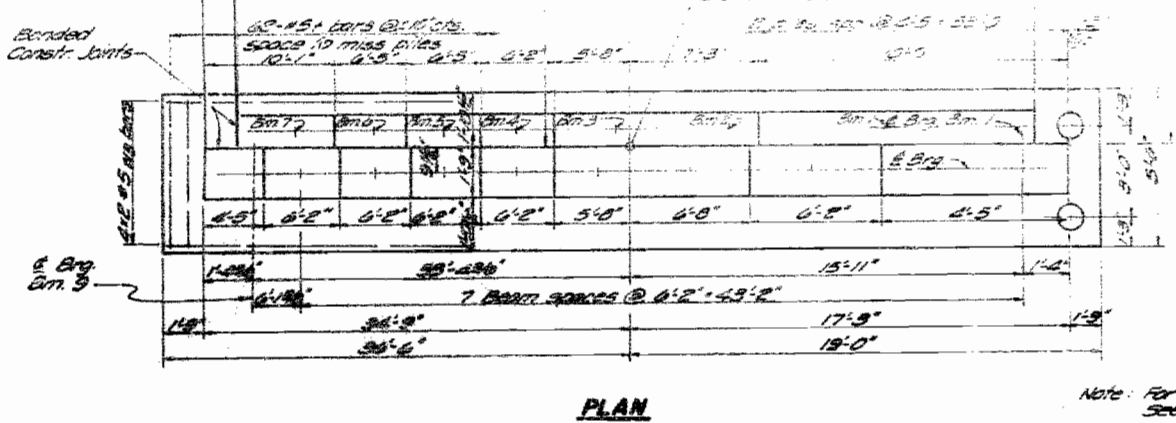
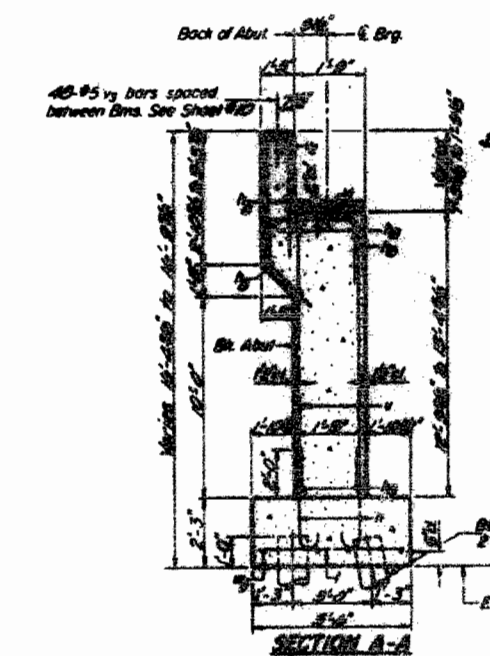
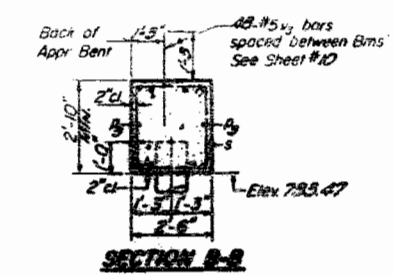
SO. ABUT. & SO. APPR. BENT
NORTHBOUND ROADWAY
 FAI ROUTE 55 SEC. 57-1HB
 MCLEAN COUNTY
 STA. 398 + 57.00

Note: For bars V11, V12 & V13 See Sheet 18 of 25



ABUT. PILE DATA
Type Concrete
Capacity 30 Tons
Est. Length 34'
No. Reqd. 29 + 1 Test Pile

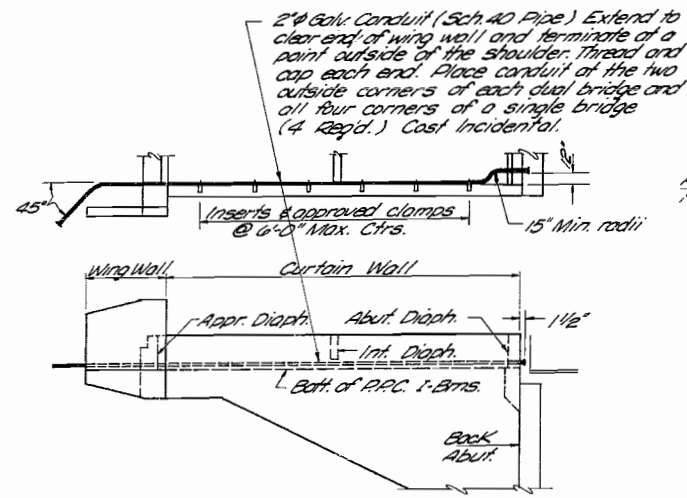
ABUT. BENT PILE DATA
Type Concrete
Capacity 30 Tons
Est. Length 45'
No. Reqd. 12



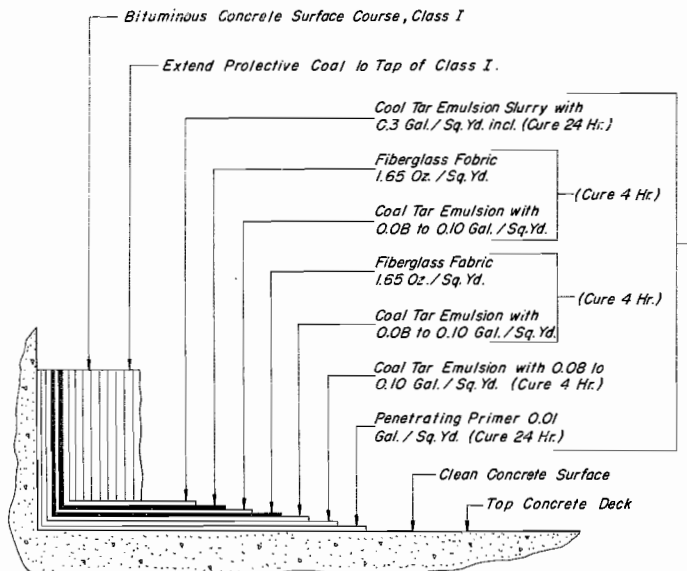
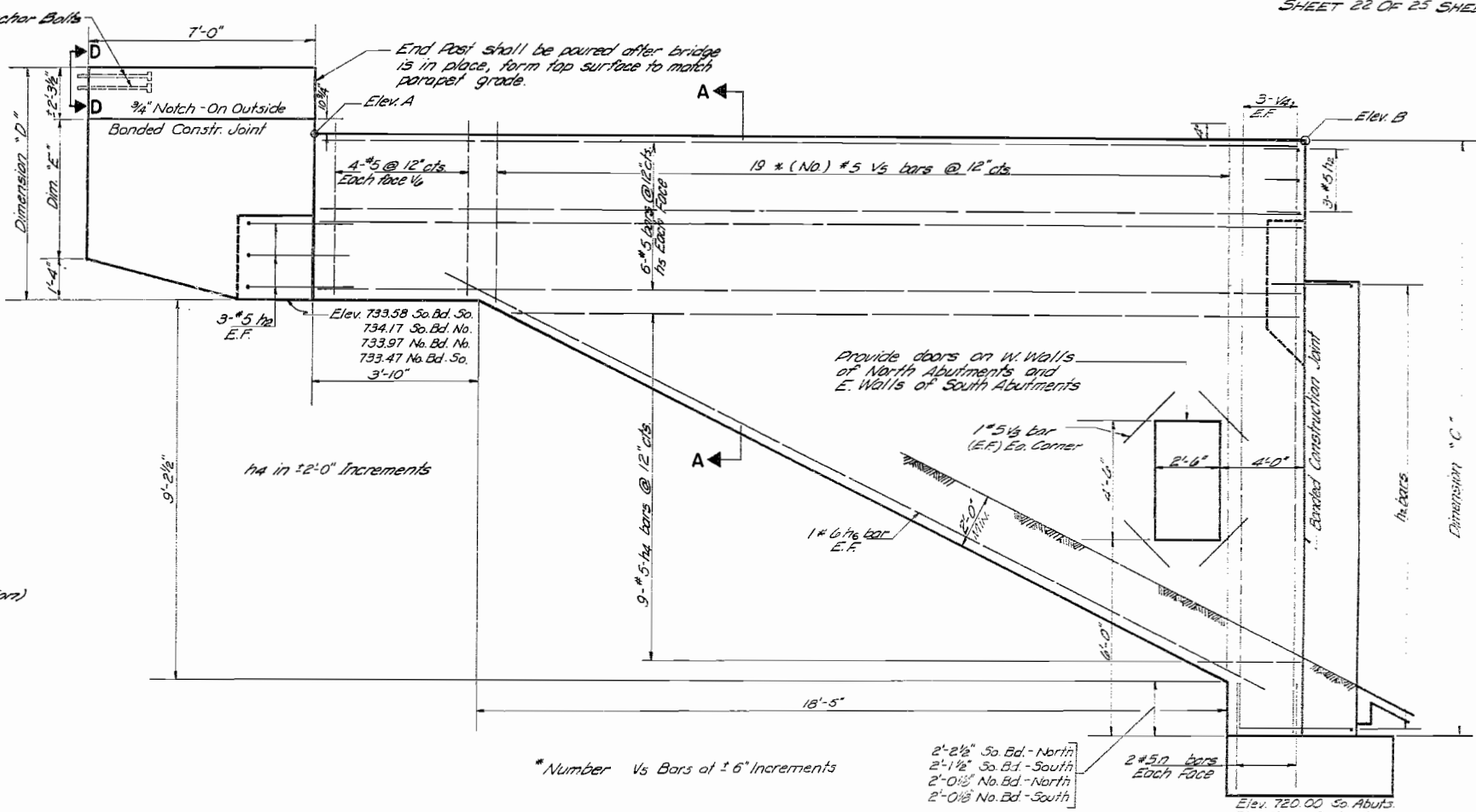
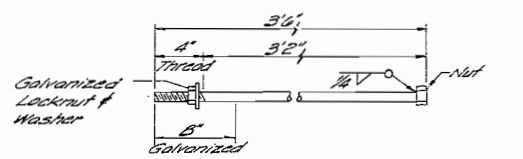
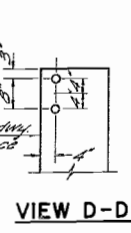
BILL OF MATERIAL

Bar No.	Size	Length	Shape
1	#5	18'-0"	
2	#5	3'-0"	
3	#5	4'-0"	
4	#5	2'-0"	
5	#5	2'-0"	
6	#5	2'-0"	
7	#5	2'-0"	
8	#5	2'-0"	
9	#5	2'-0"	
10	#5	2'-0"	
11	#5	2'-0"	
12	#5	2'-0"	
13	#5	2'-0"	
14	#5	2'-0"	
15	#5	2'-0"	
16	#5	2'-0"	
17	#5	2'-0"	
18	#5	2'-0"	
19	#5	2'-0"	
20	#5	2'-0"	
21	#5	2'-0"	
22	#5	2'-0"	
23	#5	2'-0"	
24	#5	2'-0"	
25	#5	2'-0"	
26	#5	2'-0"	
27	#5	2'-0"	
28	#5	2'-0"	
29	#5	2'-0"	
30	#5	2'-0"	
31	#5	2'-0"	
32	#5	2'-0"	
33	#5	2'-0"	
34	#5	2'-0"	
35	#5	2'-0"	
36	#5	2'-0"	
37	#5	2'-0"	
38	#5	2'-0"	
39	#5	2'-0"	
40	#5	2'-0"	
41	#5	2'-0"	
42	#5	2'-0"	
43	#5	2'-0"	
44	#5	2'-0"	
45	#5	2'-0"	
46	#5	2'-0"	
47	#5	2'-0"	
48	#5	2'-0"	
49	#5	2'-0"	
50	#5	2'-0"	
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80	#5	2'-0"	
81	#5	2'-0"	
82	#5	2'-0"	
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91	#5	2'-0"	
92	#5	2'-0"	
93	#5	2'-0"	
94	#5	2'-0"	
95	#5	2'-0"	
96	#5	2'-0"	
97	#5	2'-0"	
98	#5	2'-0"	
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100	#5	2'-0"	
101	#5	2'-0"	
102	#5	2'-0"	
103	#5	2'-0"	
104	#5	2'-0"	
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119	#5	2'-0"	
120	#5	2'-0"	
121	#5	2'-0"	
122	#5	2'-0"	
123	#5	2'-0"	
124	#5	2'-0"	
125	#5	2'-0"	
126	#5	2'-0"	
127	#5	2'-0"	
128	#5	2'-0"	
129	#5	2'-0"	
130	#5	2'-0"	
131	#5	2'-0"	
132	#5	2'-0"	
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141	#5	2'-0"	
142	#5	2'-0"	
143	#5	2'-0"	
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199	#5	2'-0"	
200	#5	2'-0"	

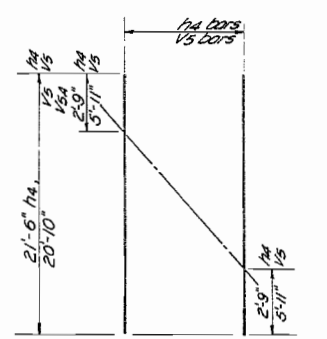
SO ABUT. & SO APPR. BENT
NORTHBOUND ROADWAY
FAI ROUTE 55 SEC. 57-1NB
MCLEAN COUNTY
STA. 398 + 57.00



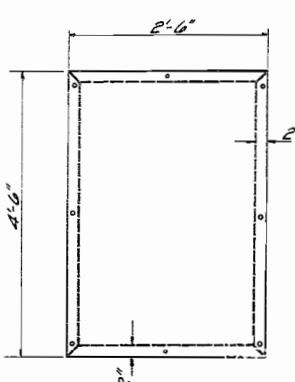
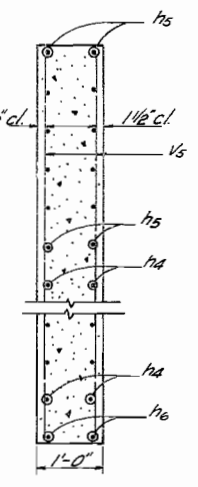
ELECTRICAL CONDUIT LOCATION



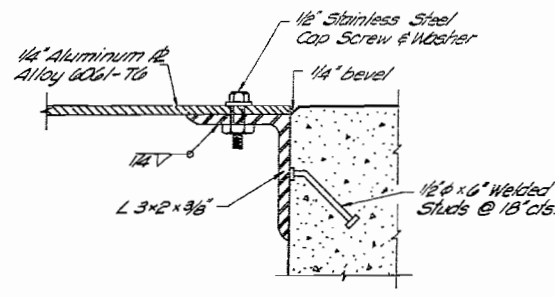
DECK SURFACING
with Cool Tar Interlayer Protective Coat (By Others)



* Order 1/4 & 1/8 bars full length cut to fit as shown and use remainder of bars in other face

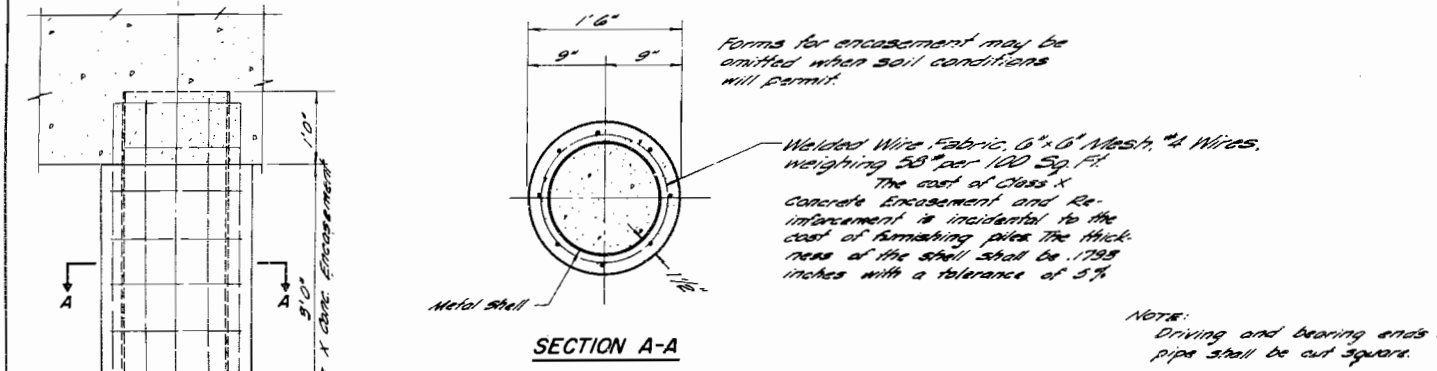
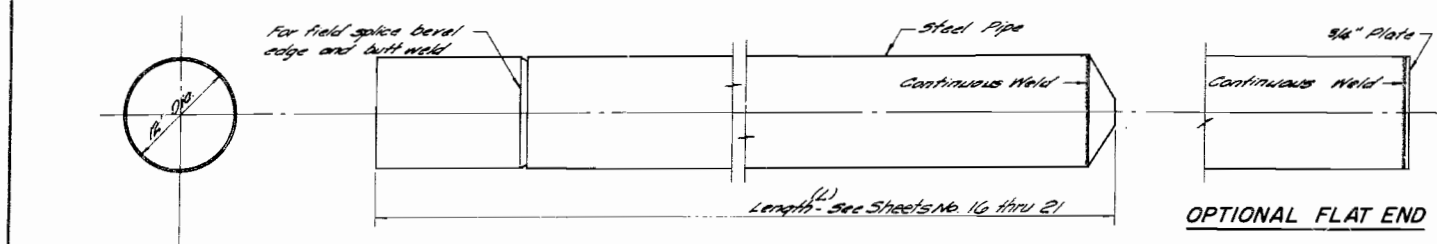


Cost of doors and frames are incidental

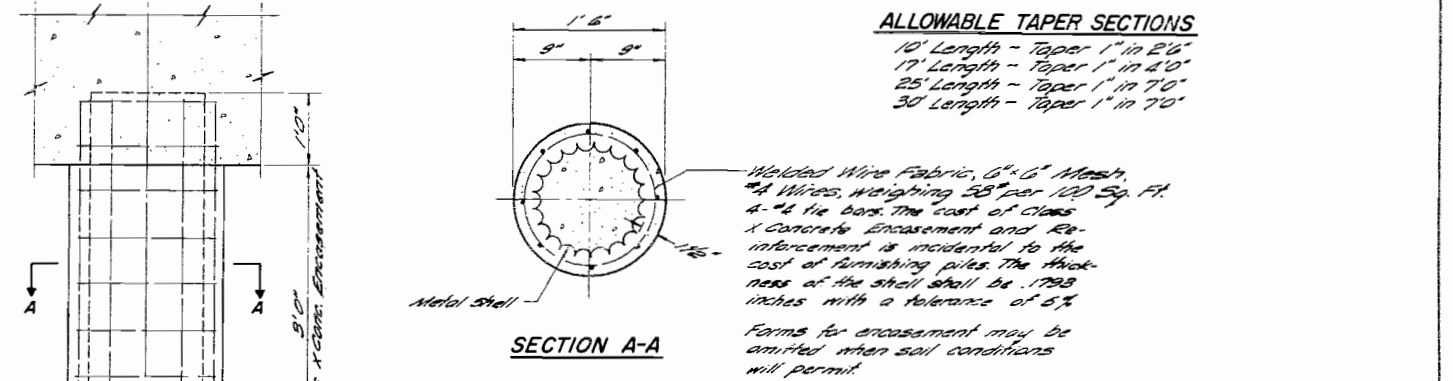
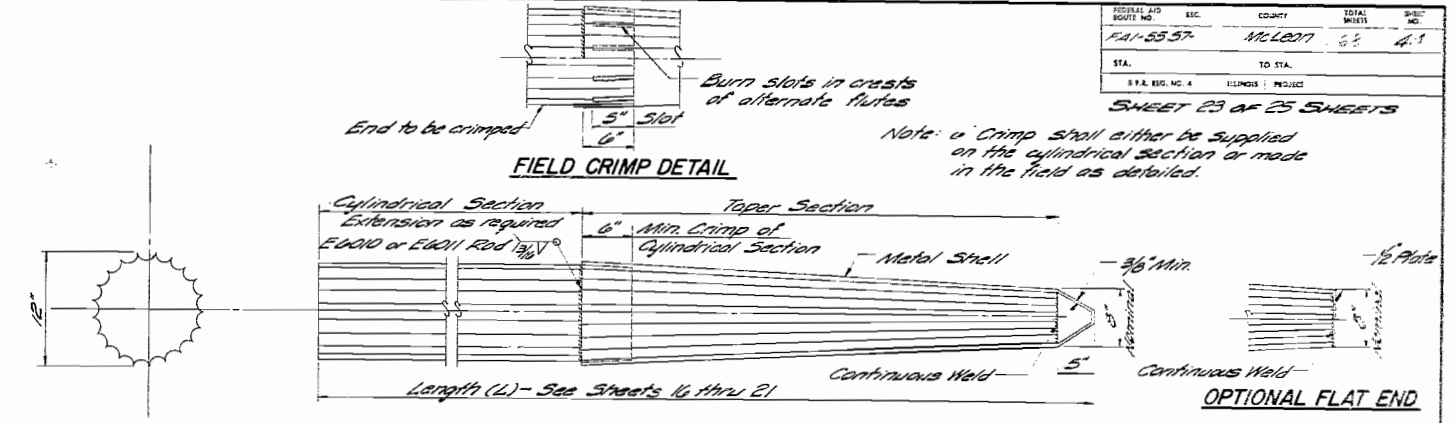


	LOCATION	DIM. "C"	DIM. "D"	DIM. "E"
South Bounding Abutment	W. Wall S. Abut.	16'-11 1/2"	8'-9 1/2"	5'-1 1/2"
	E. Wall S. Abut.	17'-2 3/4"	8'-0 1/2"	5'-4 7/8"
	W. Wall N. Abut.	16'-11 1/2"	8'-9 1/2"	5'-2 1/2"
	E. Wall N. Abut.	17'-0 3/8"	8'-10 1/2"	5'-3"
North Bounding Abutment	W. Wall S. Abut.	17'-2 3/4"	9'-1 1/2"	5'-6 1/2"
	E. Wall S. Abut.	16'-10 1/2"	8'-9 3/8"	5'-1 3/8"
	W. Wall N. Abut.	17'-0 3/8"	9'-0 1/2"	5'-5 3/8"
E. Wall N. Abut.	16'-9 1/2"	8'-9 1/2"	5'-2"	
	LOCATION	ELEV. A	ELEV. B	
South Bounding Abutment	W. Wall S. Abut.	739.15	739.23	
	E. Wall S. Abut.	739.42	739.48	
	W. Wall N. Abut.	739.70	739.69	
	E. Wall N. Abut.	739.86	739.80	
North Bounding Abutment	W. Wall S. Abut.	739.42	739.48	
	E. Wall S. Abut.	739.06	739.12	
	W. Wall N. Abut.	739.86	739.80	
E. Wall N. Abut.	739.57	739.51		

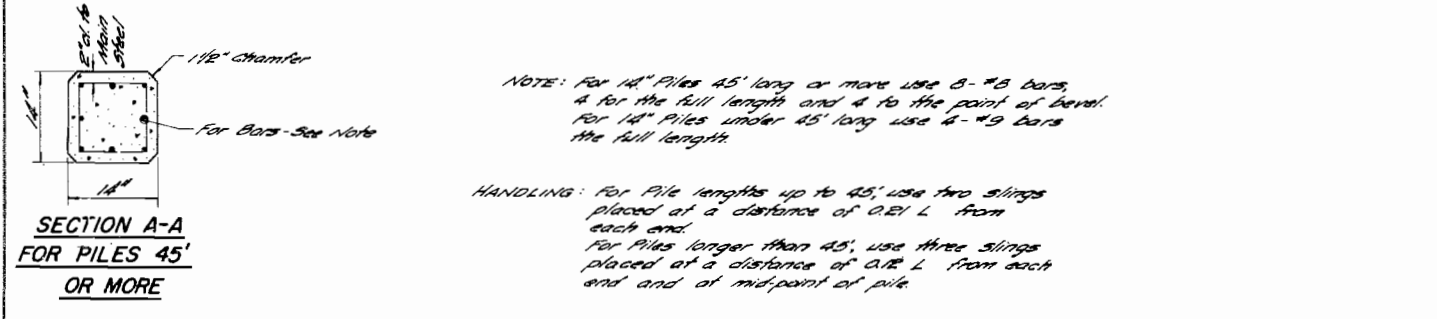
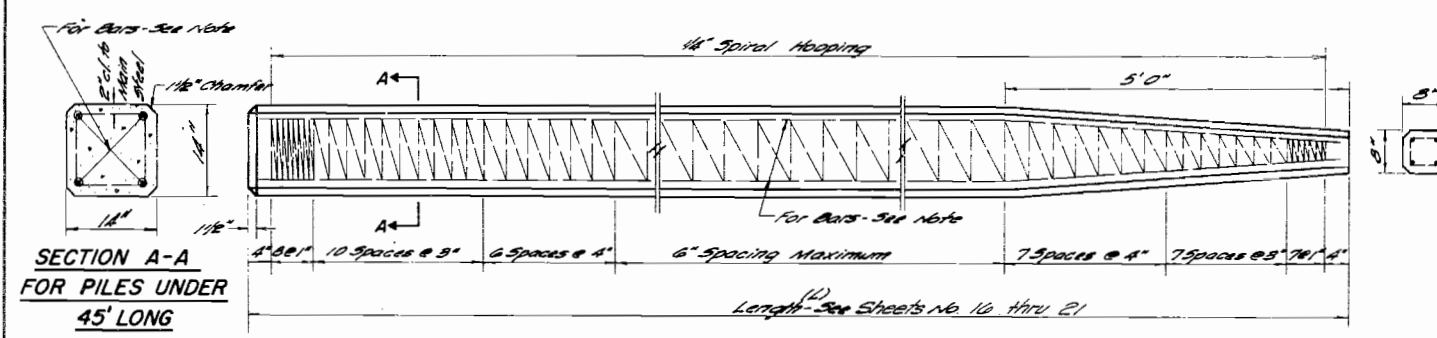
CURTAIN WALL
 FAI ROUTE 55 SEC. 57-1HB
 MC LEAN COUNTY
 STATION 398+57.00



DETAIL OF CYLINDRICAL STEEL SHELL FOR CAST IN PLACE CONCRETE PILES



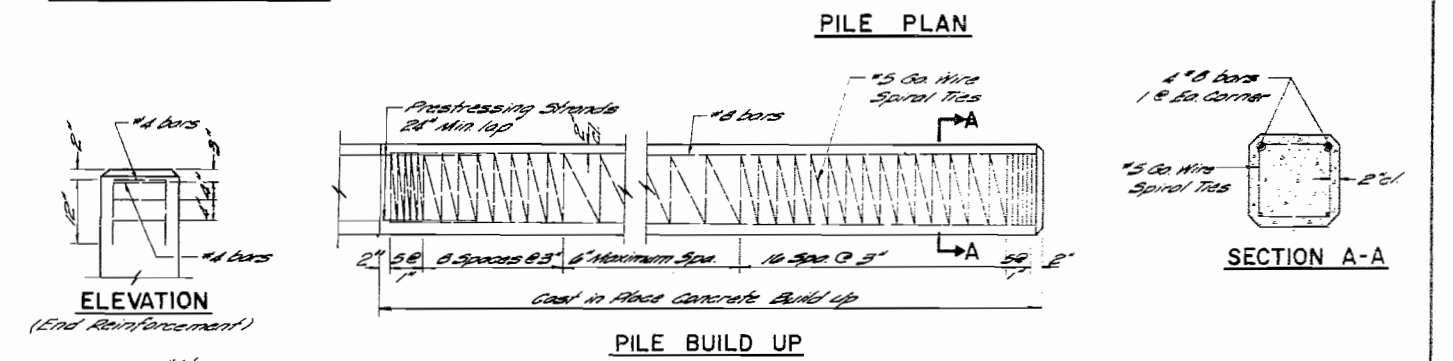
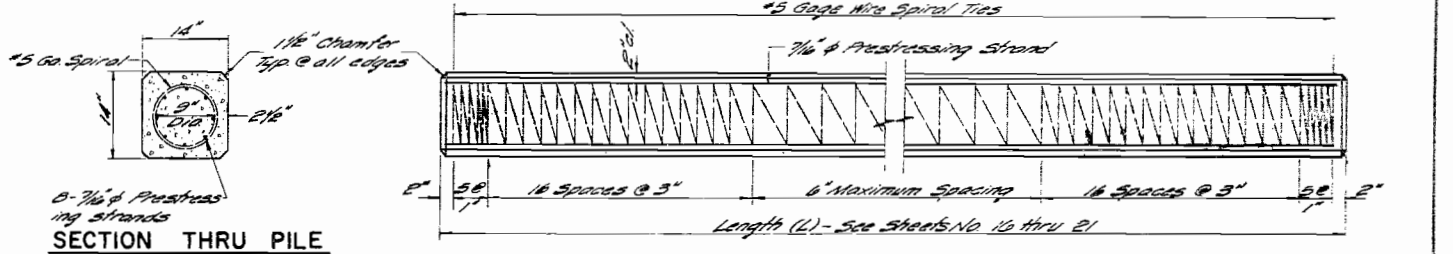
DETAIL OF TAPERED METAL SHELL FOR CAST IN PLACE CONCRETE PILES



NOTE: For 14" Piles 45' long or more use 8-#8 bars, 4 for the full length and 4 to the point of bevel. For 14" Piles under 45' long use 4-#9 bars the full length.

HANDLING: For Pile lengths up to 45', use two slings placed at a distance of 0.21 L from each end. For Piles longer than 45', use three slings placed at a distance of 0.12 L from each end and at mid-point of pile.

DETAIL OF PRECAST CONCRETE PILES



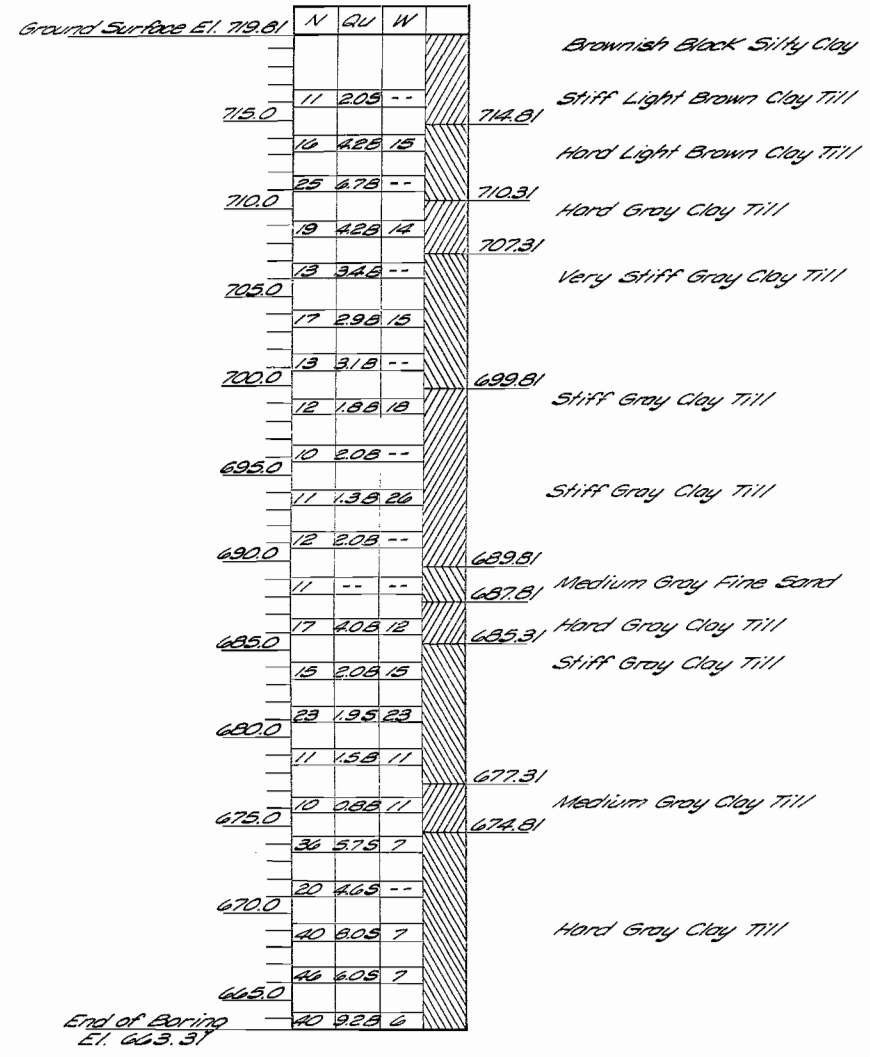
DESIGN STRESSES

- f_c' = 5000 psi
- f_s' = 4000 psi
- f_s' = 200,000 psi (20,000 ksi)
- f_s' = 100,000 psi (10,000 ksi)

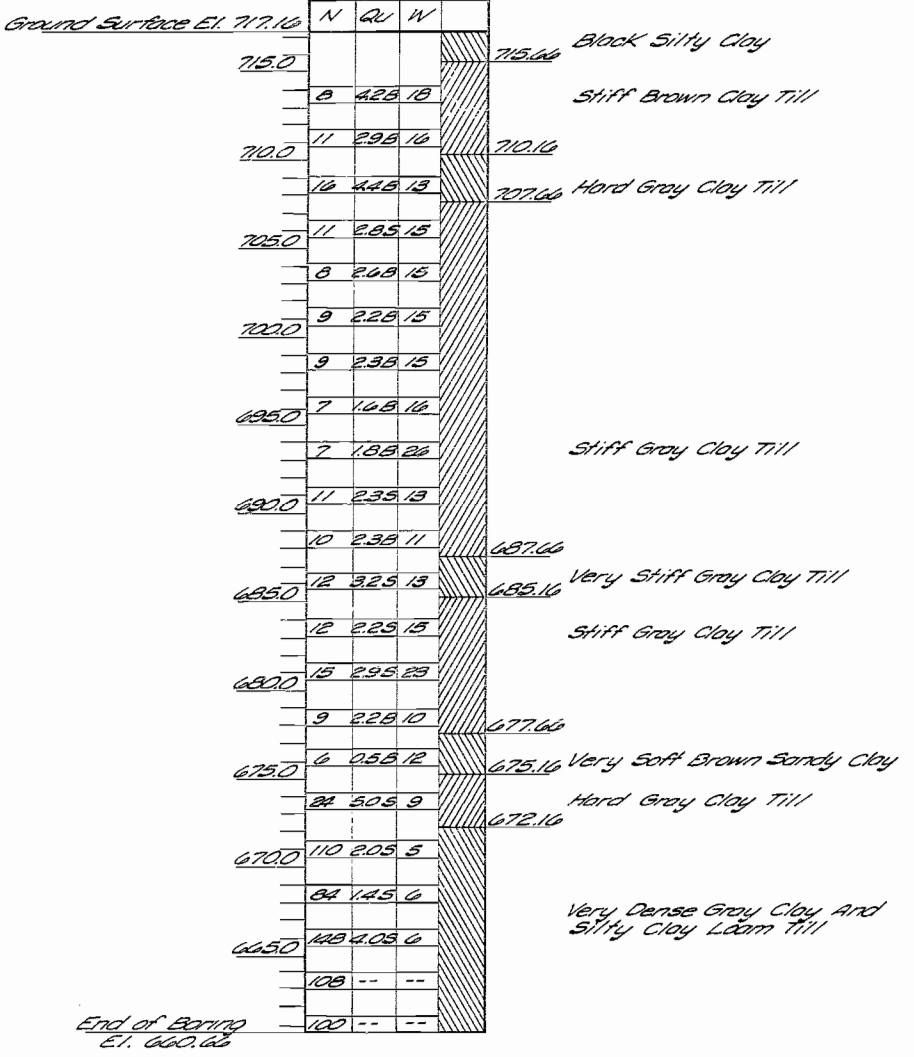
Note: Prestressing steel shall be non-galvanized extra high strength stress-relieved 7 wire strand. The nom. dia. shall be 7/16" and the minimum nominal cross-sectional area shall be 0.1155 square inch.

Handling: For pile lengths up to 45', use two slings placed at a distance of 0.21 L from each end. For piles longer than 45', use three slings placed at a distance of 0.12 L from each end and at midpoint of pile.

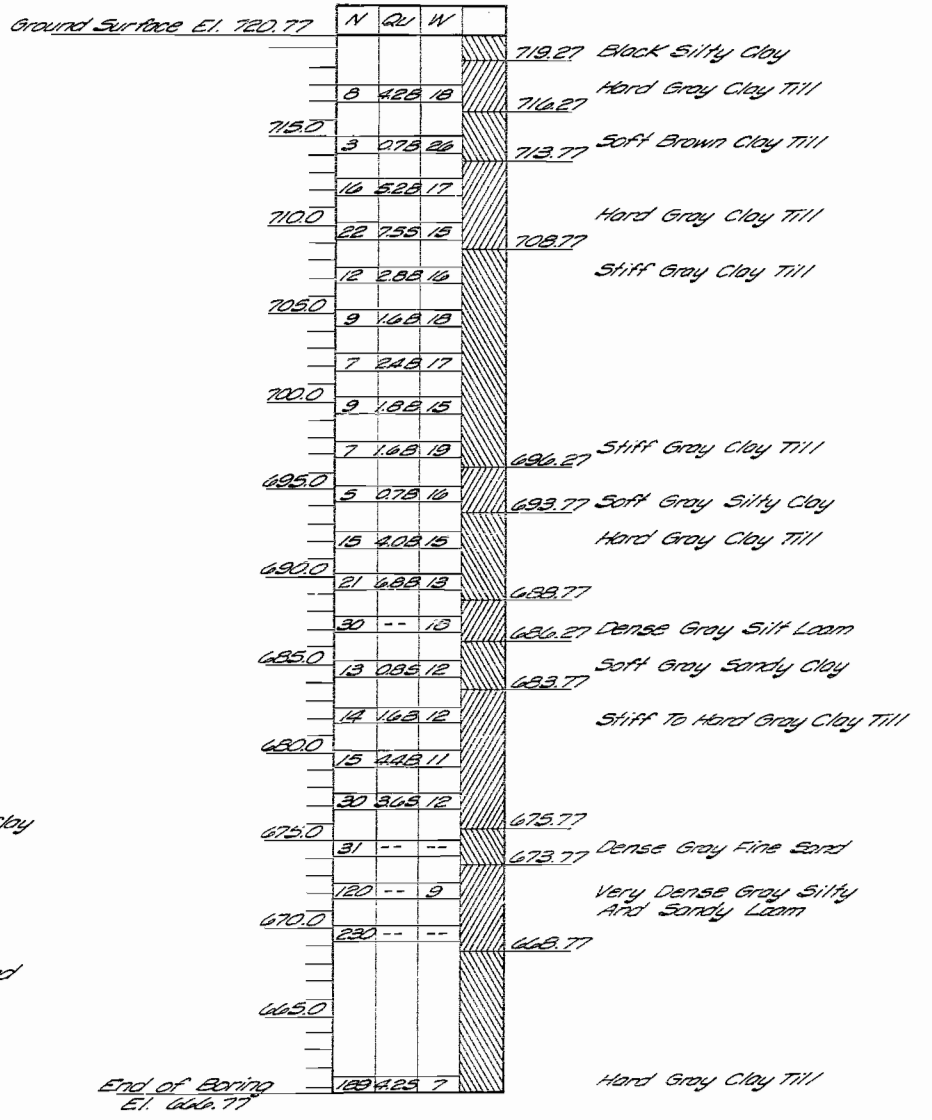
DETAIL OF PRECAST PRESTRESSED CONCRETE PILES



B-1
STA. 397 + 89
56' LT. C



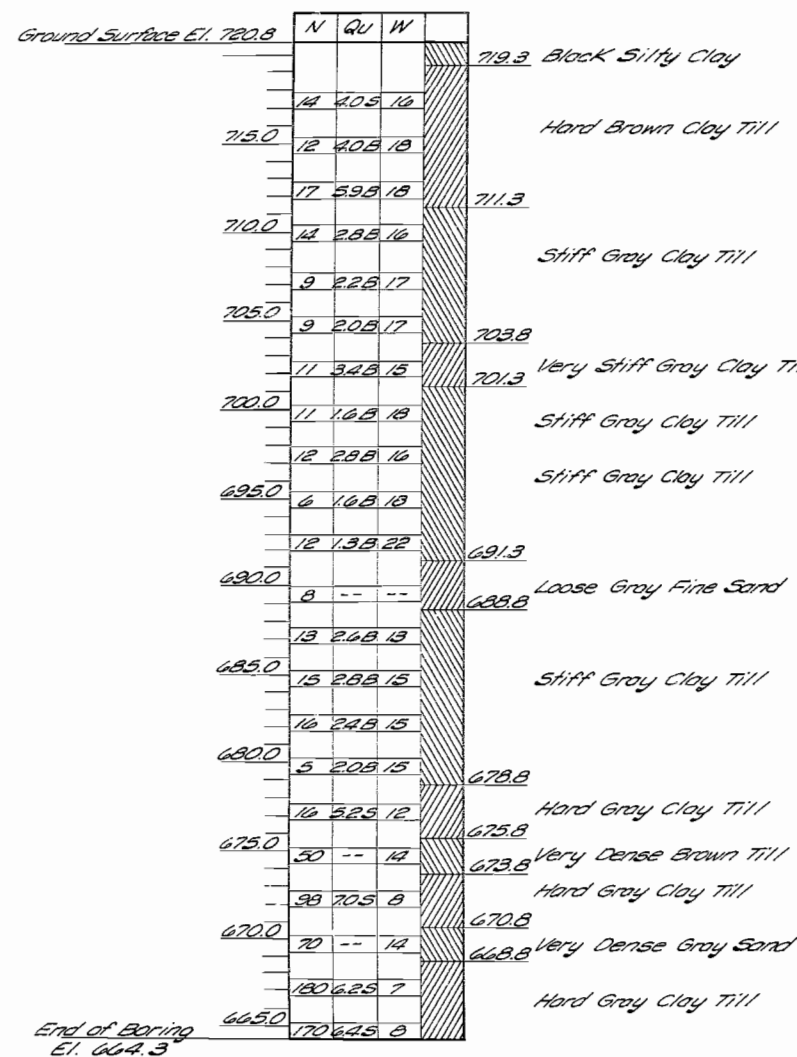
B-2
STA. 398 + 40
56' LT. C



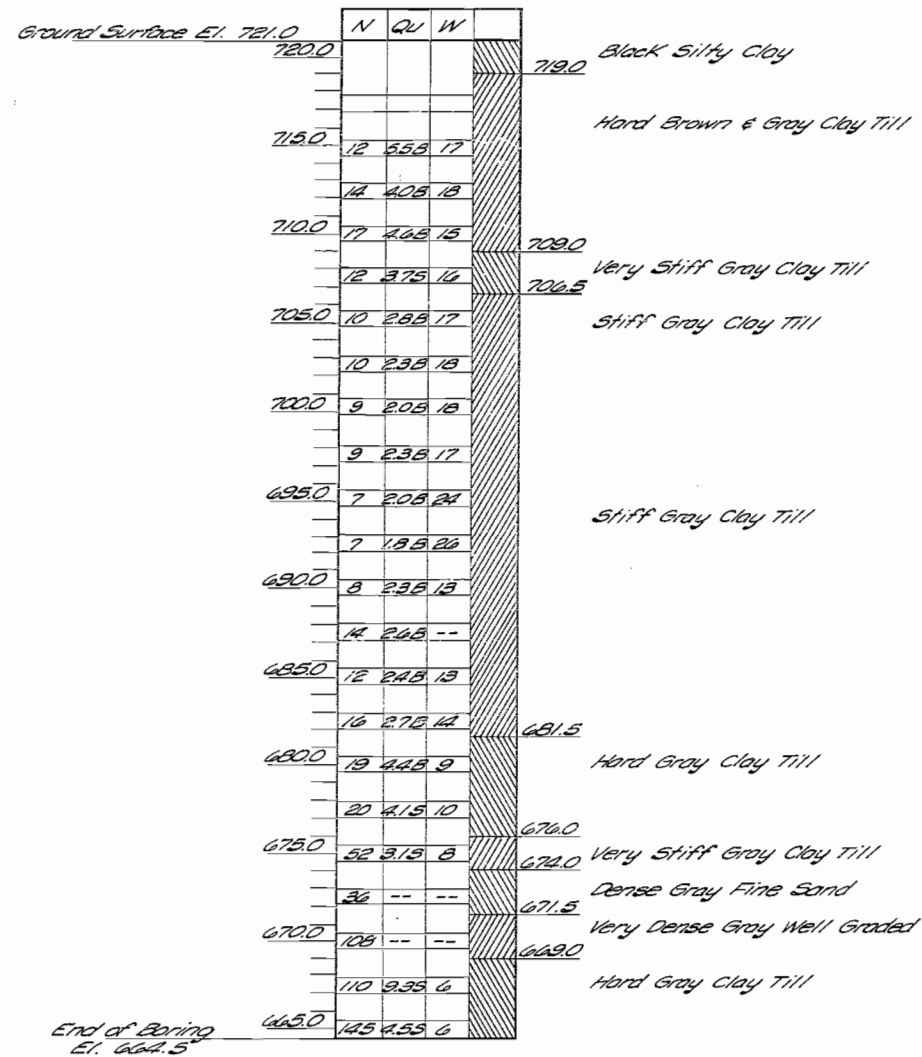
B-3
STA. 399 + 21
56' LT. C

N = Standard Penetration Test - Blows per ft. to drive 2" O.D. Split Spoon Sampler 12" with 140# hammer falling 30"
 QU = Unconfined Compressive Strength - Tons per Sq Ft.
 W = Water Content - Percentage of Oven Dry Weight - %
 Type Failure:
 B - Bulge Failure
 S - Shear Failure
 E - Estimated Value

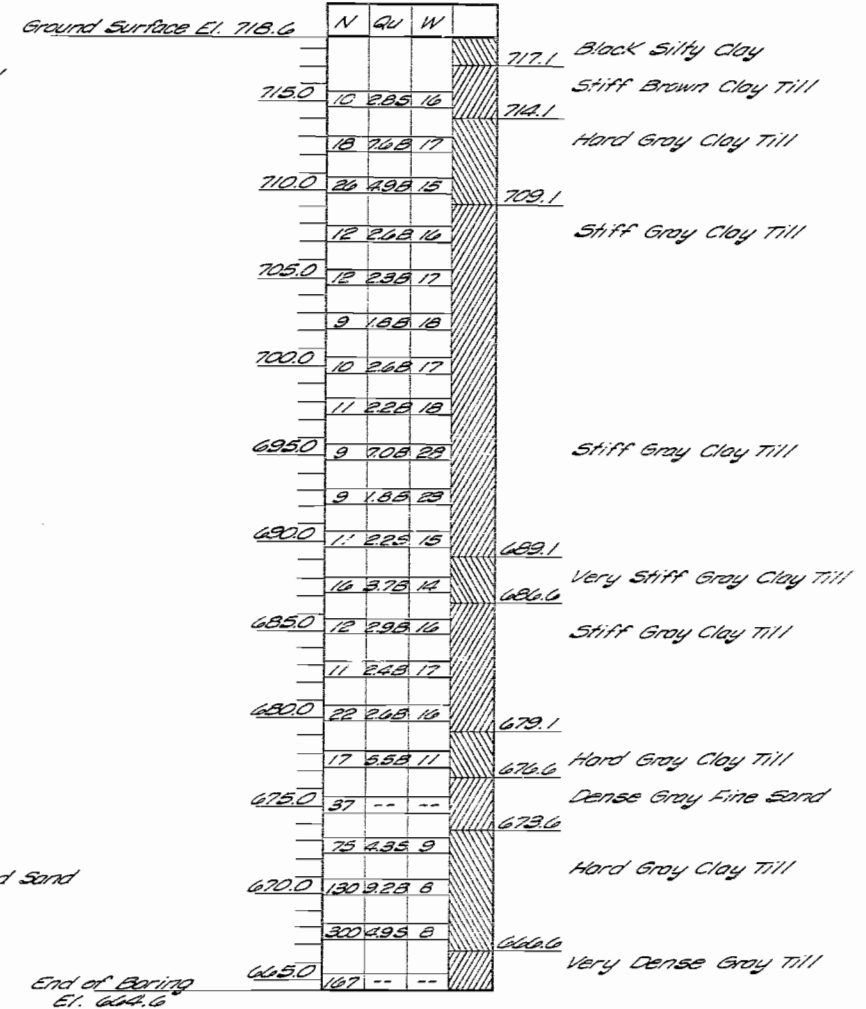
BORING DATA
 FAI ROUTE 55 SEC. 57-1HB
 McLEAN COUNTY
 STATION 398 + 55.00



B-4
STA. 397 + 89
56' RT. ☺



B-5
STA. 398 + 68
56' RT. ☺



B-6
STA. 399 + 21
56' RT. ☺

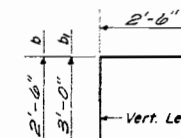
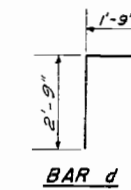
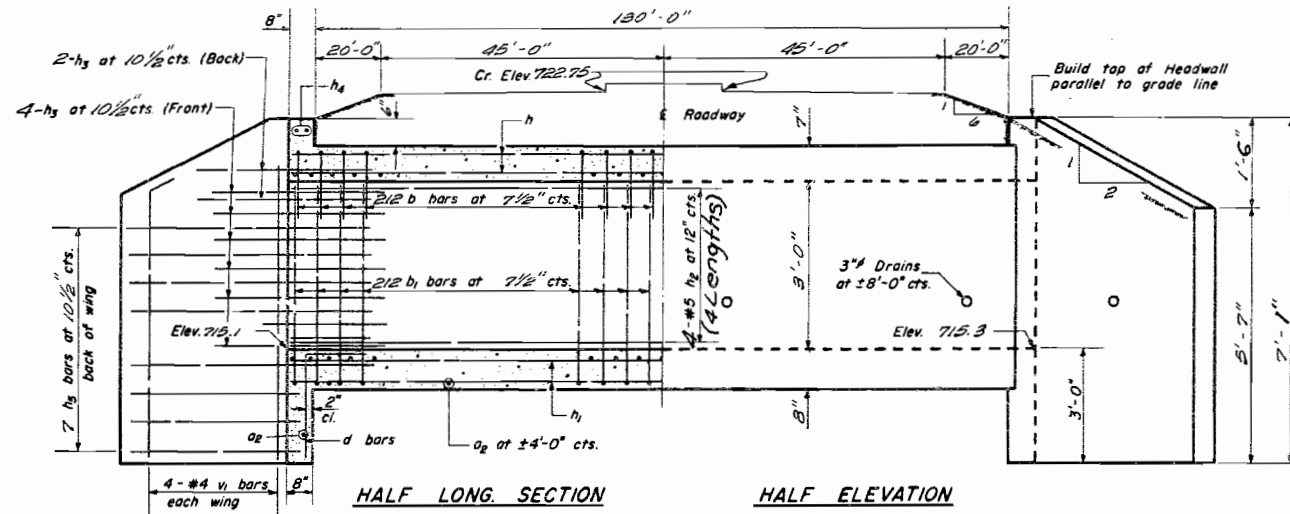
N = Standard Penetration Test - Blows per ft. to drive 2" O.D. Split Spoon Sampler 12" with 140# hammer falling 30"
 QU = Unconfined Compressive Strength - Tons per Sq. Ft.
 W = Water Content - Percentage of Oven Dry Weight - %

Type Failure:
 B = Bulge Failure
 S = Shear Failure
 E = Estimated Value

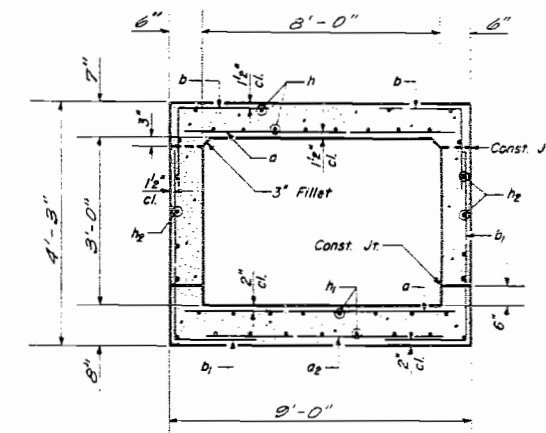
BORING DATA
 FAI ROUTE 55 SEC. 57-1HB
 McLEAN COUNTY
 STATION 398 + 55.00

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

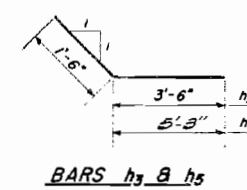
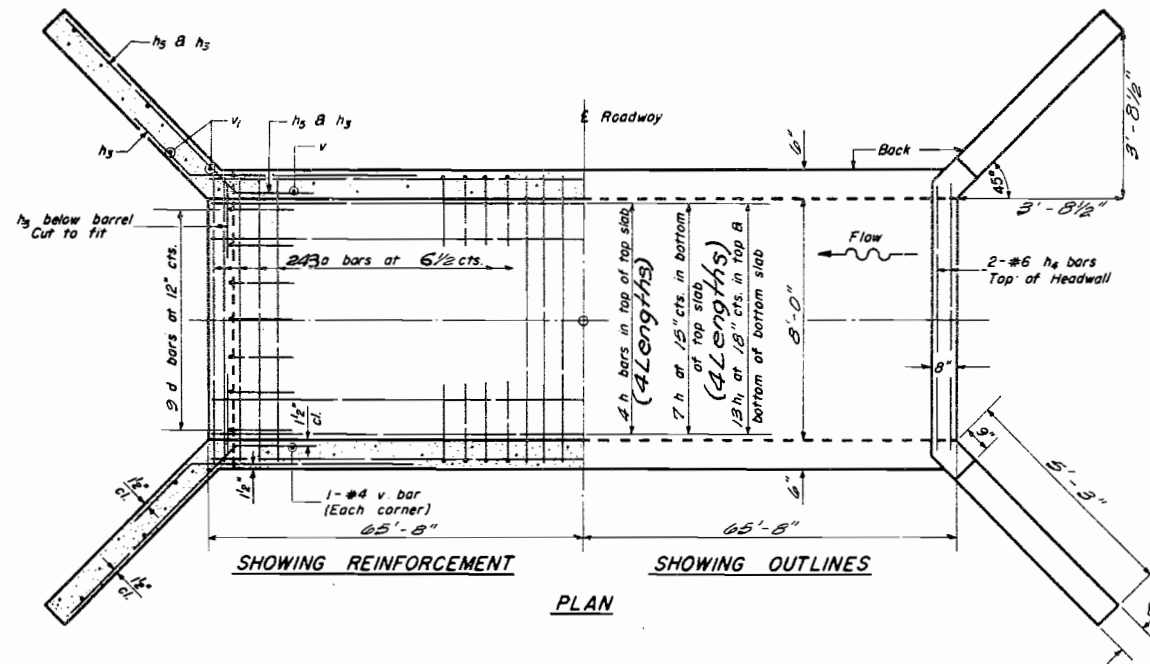
PROJECT NO.	SECTION	CONTRACT	SHEET NO.	SHEET NO.
57-118	M'LEADY	68	47	47
SHEETS				



BARS b & b1



SECTION THRU BARREL



BARS h3 & h5

BILL OF MATERIAL

Bar	No.	Size	Length
a	486	#6	8'-8"
o2	35	#4	4'-9"
b	424	#5	5'-0"
b1	424	#5	5'-6"
d	18	#4	4'-5"
h	44	#5	33'-9"
h1	52	#5	33'-9"
h2	32	#5	33'-9"
h3	24	#4	5'-0"
h4	4	#6	8'-6"
h5	28	#4	6'-9"
v	4	#4	4'-0"
v1	16	#4	6'-3"
Class X Concrete			Cu Yds. 73.6
Reinforcement Bars			Lbs. 15,980

GENERAL NOTES

Class X Concrete shall be used throughout.
At least six feet of Barrel shall be poured monolithically with wingwalls.
Exposed edges shall be beveled 1/4".
For backfilling & embankments see Std. Spec's.
Tilt hook of "a" bars, if necessary, to obtain h" minimum clearance at top of hook.
The top of the culvert, the backs of the sidewalls above the lower construction joint and the backs of the wings shall be waterproofed in accordance with Art. 503J of the Std. Spec's.
All bars shall be lapped 24 diameters unless otherwise specified.

fs = 20,000 psi
fc = 1400 psi. Barrel
fc = 1200 psi. Wings
v = 90 psi.
n = 10

LOADING HS20-44

R.C. Box Culvert
S/O. 1531+00

STANDARD 2030R

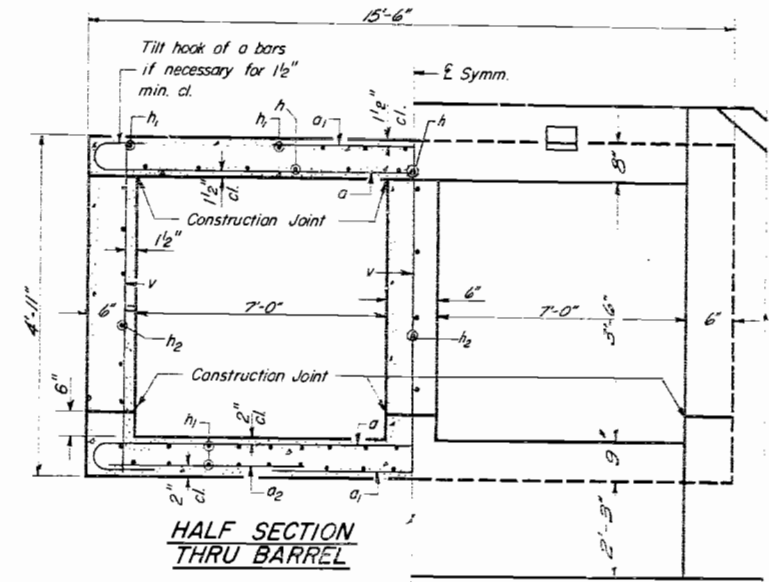
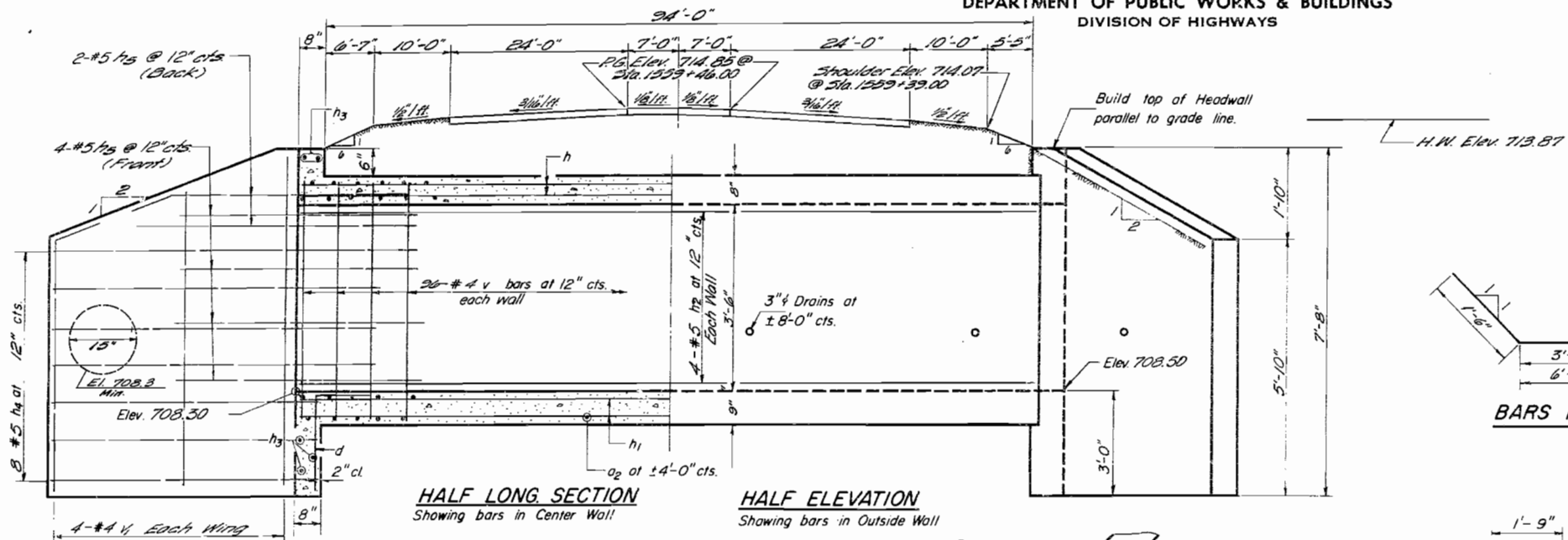
DESIGNED	T.J.P.
CHECKED	
DRAWN	
CHECKED	

EXAMINED	July 17, 1972
PASSED	
APPROVED	

Rev. Reinf. from 17,071# to 15,980# 7-17-72 L.W.

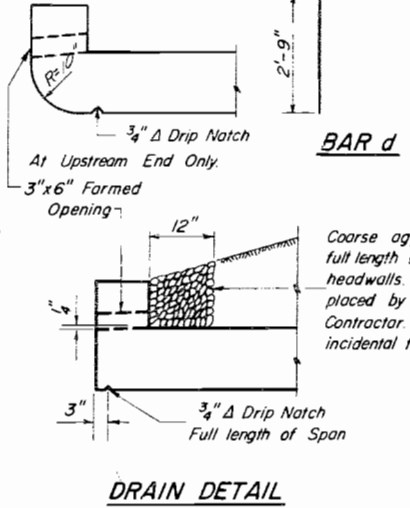
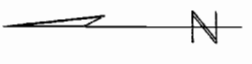
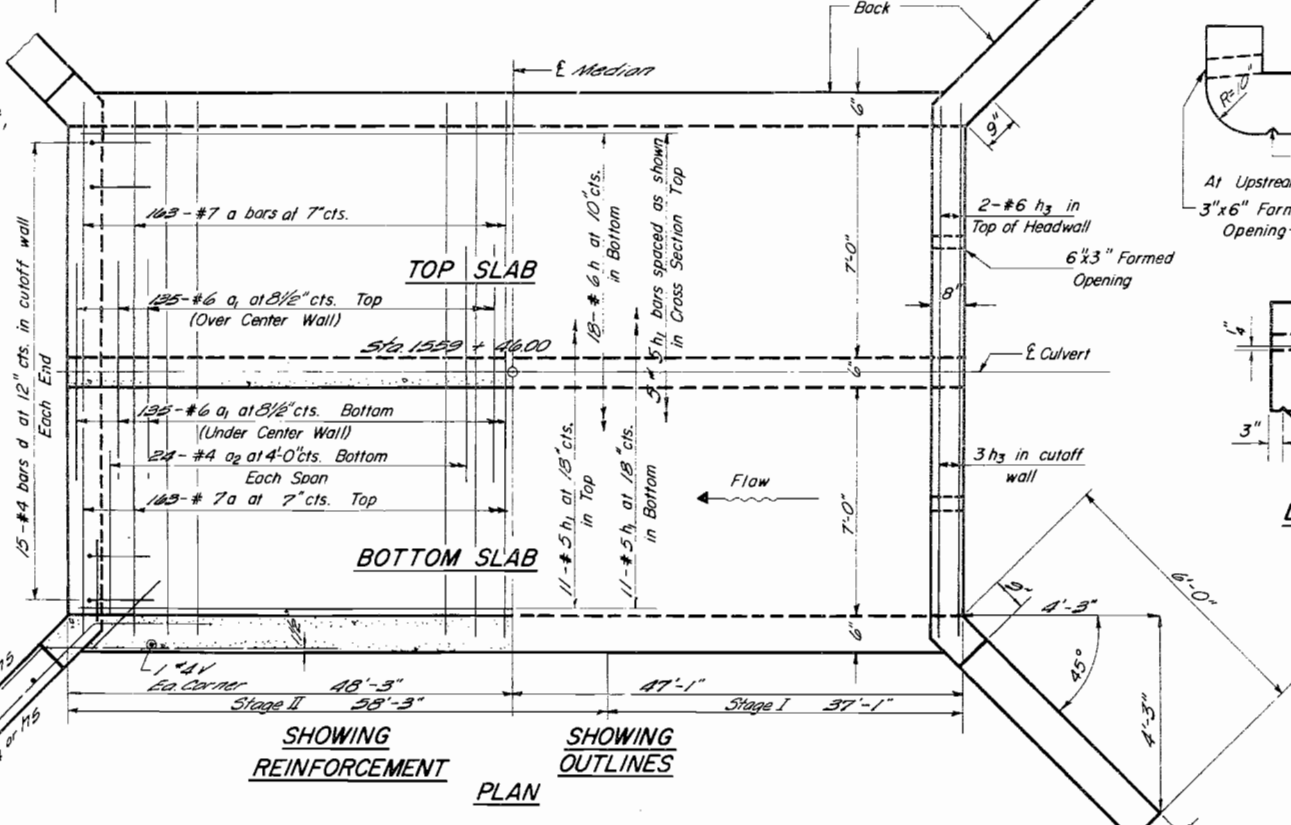
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNT	TOTAL SHEETS	SHEET NO.
55	57-11B	McLEAN	68	48
PROJ. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		SHEETS



REINFORCEMENT BACK FACE

cut h4 bars in the field to fit, and use cut portion around 15" pipe.



BAR a

ROADWAY DATA
Class: Major
DHW: 1201 (1993)
Design Speed: 50 m.p.h.

WATERWAY INFORMATION

Drainage Area: 672 Acres
Character: Level, Cultivated
Existing Opening: 31.5 Sq. Ft.
Required Opening: 49 Sq. Ft.
Proposed Opening: 49 Sq. Ft.
Q(50): 333 cfs

BILL OF MATERIAL

BAR	NO.	SIZE	LENGTH	SHAPE
a	326	#6	16'-10"	U
a1	270	#7	7'-0"	—
a2	24	#4	14'-9"	—
d	30	#4	4'-5"	U
h	90	#6	20'-6"	—
h1	135	#5	20'-3"	—
h2	60	#5	20'-3"	—
h3	24	#5	5'-0"	—
h4	10	#6	15'-0"	—
h5	32	#5	7'-6"	—
v	392	#4	4'-7"	—
v1	16	#4	7'-0"	—
Class X Concrete		Cu. Yds	102.1	
Reinforcement Bars		Lbs.	20,940	

Bar	Location	No.
h1	Top of top slab	5
h	Bottom of top slab	13
h1	Top of bottom slab	11
h1	Bottom of bottom slab	11
h2	Each outside wall	4
h2	Center wall	4

5 Lengths each required.

DESIGN STRESSES

fs = 20,000 p.s.i.
fc = 1400 p.s.i. Barrel
fc = 1200 p.s.i. Wings
vc = 90 p.s.i. Barrel
vc = 75 p.s.i. Footing
n = 10

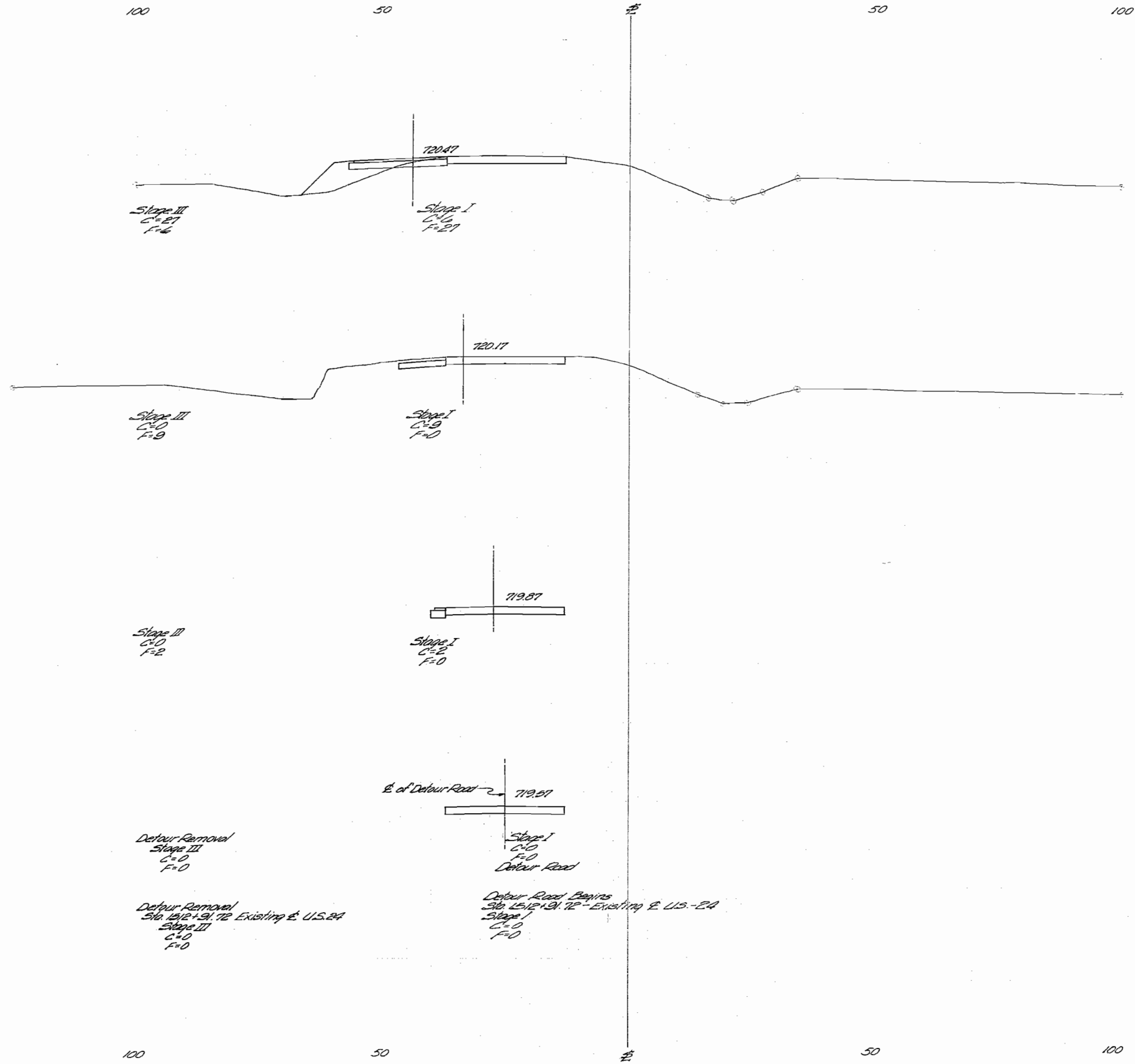
DOUBLE 7'x35' R.C. BOX CULVERT

U.S. ROUTE 24 SECTION 57-1(1)
STATION 1559+46 57-11B
MC LEAN COUNTY

DESIGNED	H.P.G.
CHECKED	T.J.P.
DRAWN	J.C.
CHECKED	T.J.P.
DB-L-0	8-1-65

APPROVED
FOR STRUCTURAL ADEQUACY ONLY
Harry P. Leaban
#1976
1976

150			
PROJECT NO.	DATE	BY	CHECKED
FAI-55 5710	McLeary	EB	AG
STA. 1515+00	TO STA. 1516+00		
SHEET NO. 4 OF 4			

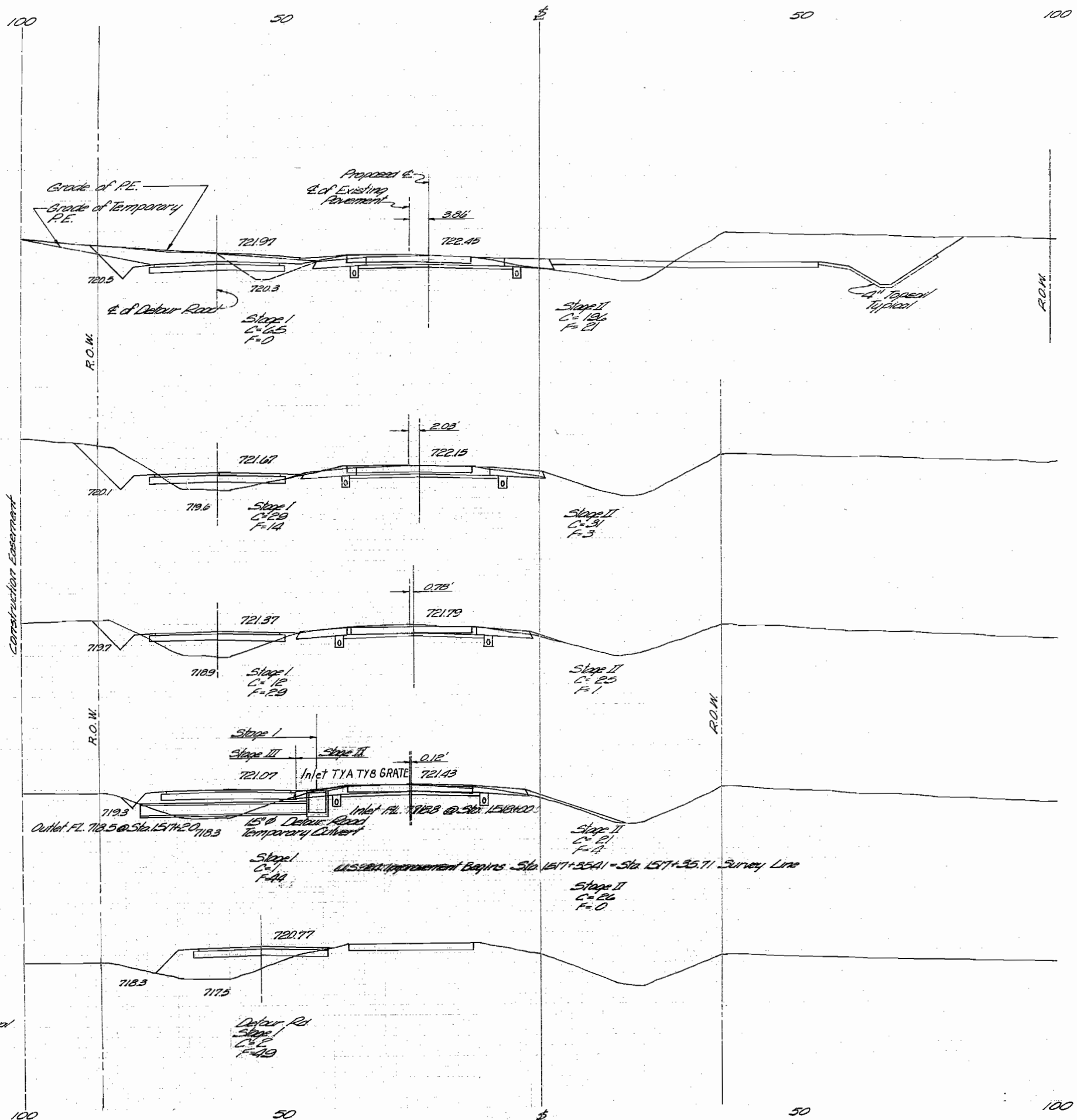


1516
+
00
715

1515
+
00
715

1514
+
00
720

1513
+
00
720



1521
 +00
 715

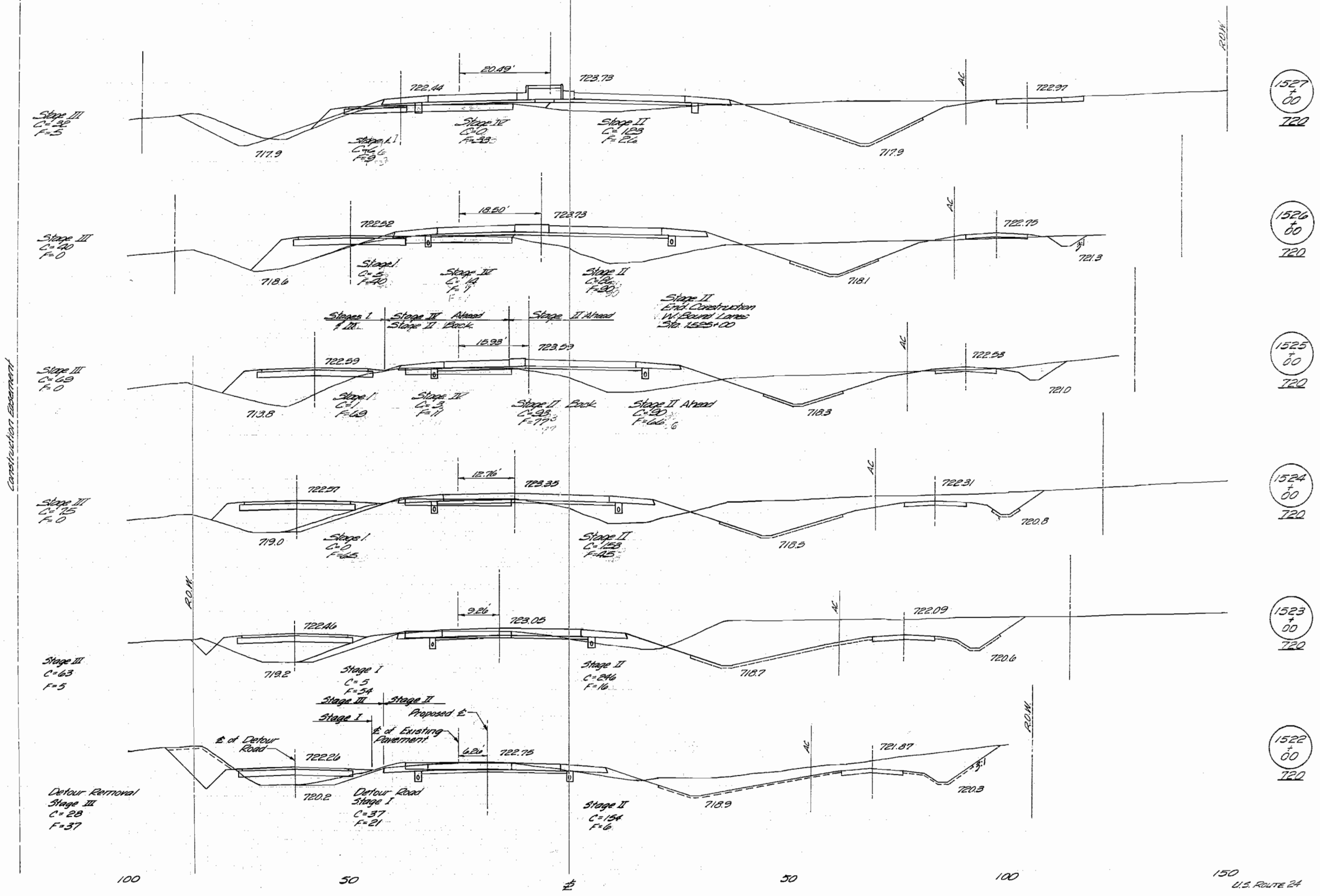
1520
 +00
 715

1519
 +00
 715

1518
 +00
 715

1517
 +00
 715

F21-25-57-10) McLean 68 51
1521+00 1527+00



1527
+00
720

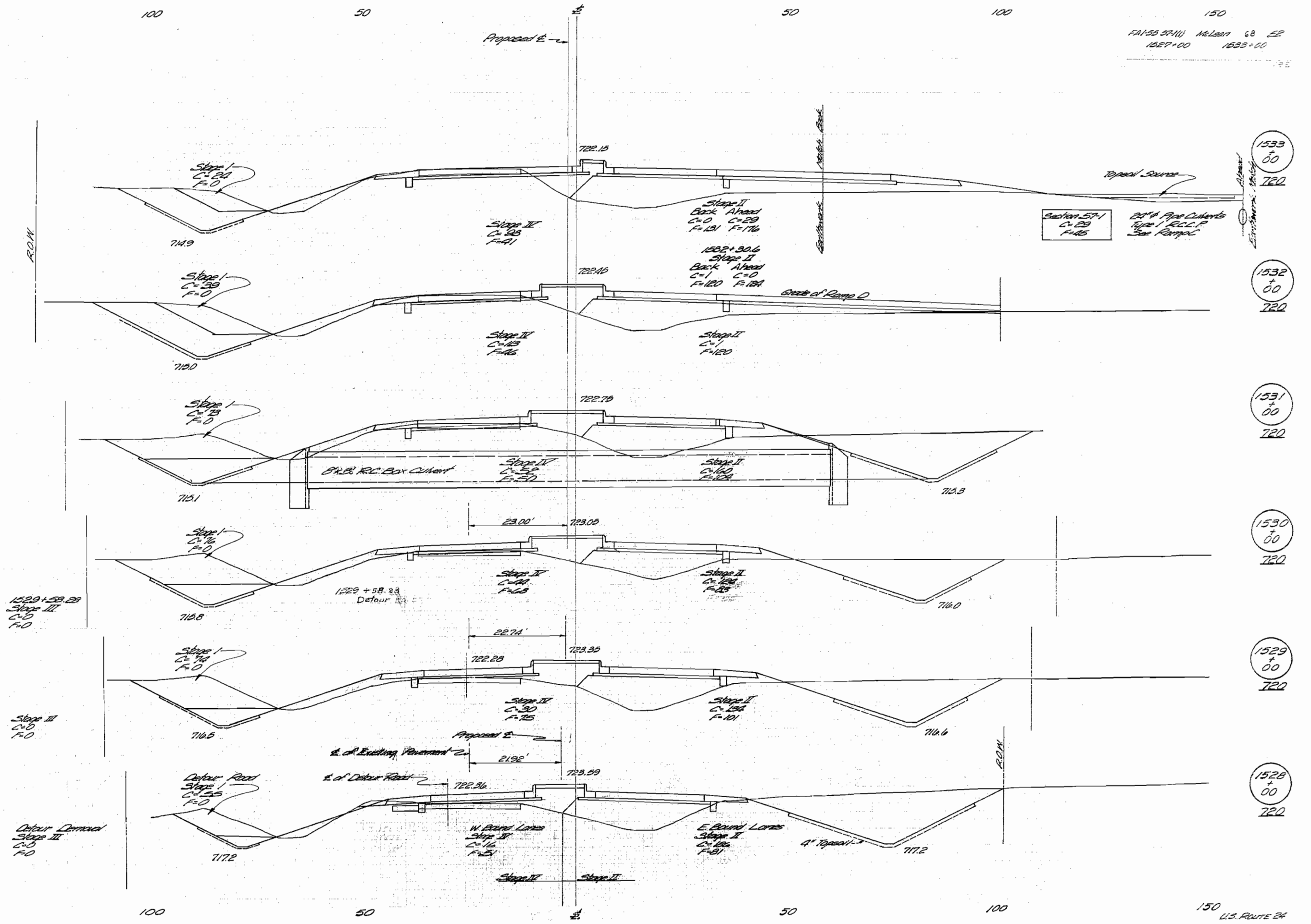
1526
+00
720

1525
+00
720

1524
+00
720

1523
+00
720

1522
+00
720



150

100

50

Proposed E

720.65
720.71
720.95
721.25
721.55
721.85

50

100

150

FAI-5557(1) McLean 88 53
1538+00 1538+00

1538
+00

715

Remove Existing 36" ϕ Pipe
& Handrails

Slope II
C=67
F=29

Slope II
C=56
F=16

Section 57-1
C=133
F=0

St. 1976 Hand.

St. 1976 Hand.

1537
+00

715

Slope II
C=49
F=50

714.4

36" Pipe Culverts Typical R.C.P.

720.95

Slope II
C=50
F=134

714.8

Section 57-1
C=156
F=0

714.5

Slope II
C=69
F=33

Slope II
C=59
F=117

714.9

Section 57-1
C=223
F=21

1537
+00

715

721.25

714.6

Slope II
C=85
F=33

Slope II
C=74
F=95

715.0

Section 57-1
C=136
F=23

1536
+00

715

721.55

714.7

Slope II
C=102
F=21

Slope II
C=84
F=72

715.1

Section 57-1
C=215
F=22

1535
+00

715

721.85

St. 1976 Hand.

714.8

Slope I Ends

Slope II
C=79
F=24

Slope II
C=75
F=11

715.2

Temp. Ditch

Section 57-1
C=221
F=16

1534
+00

715

Proposed E

150

100

50

E

50

100

150

L.I.S. ROUTE 24

150

100

50

±

50

100

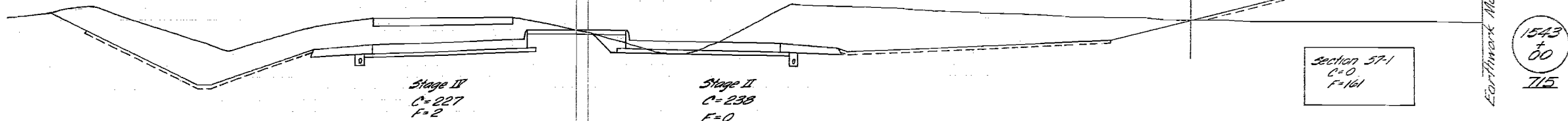
150

Proposed ±

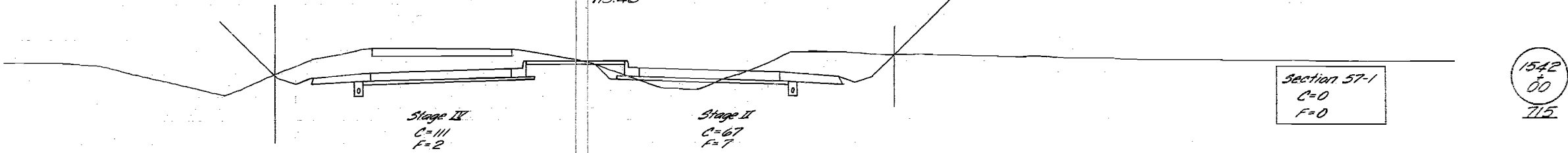
719.15

FAI-55.59(11) McLEOD 68 SL
1539+00 1543+00

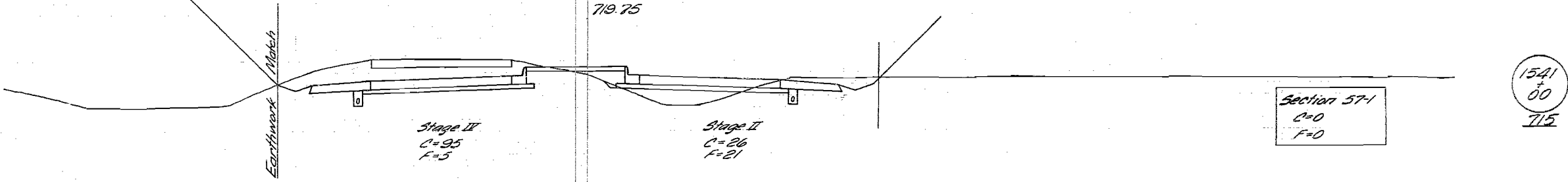
Sec 57-1 HB Sec 57-1



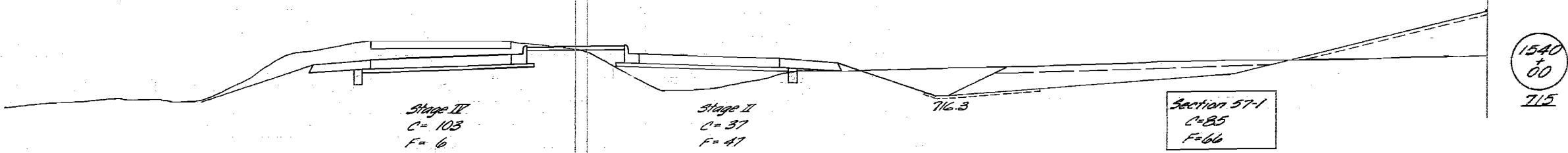
719.45



719.75

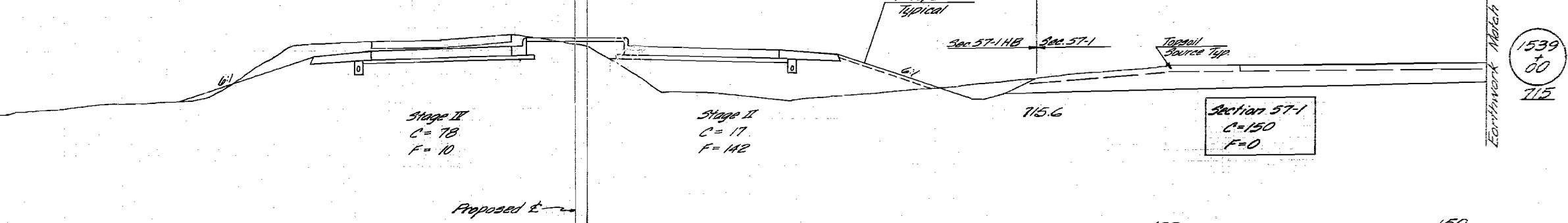


720.05



720.35

ROW



150

100

50

±

50

100

150

U.S. ROUTE 24

150

100

50

±

50

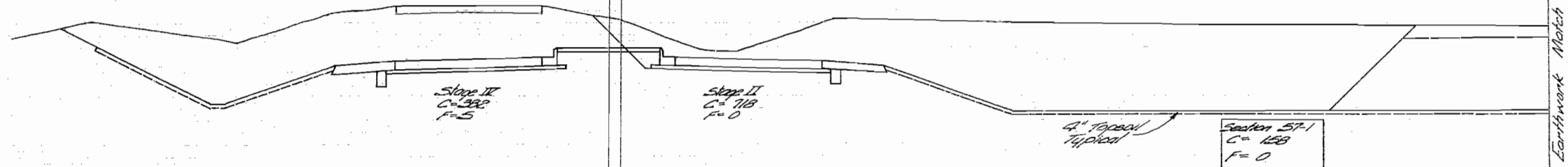
100

150

FAI-55-57-RU McLean 68 55
1543+00 1548+00

Proposed E

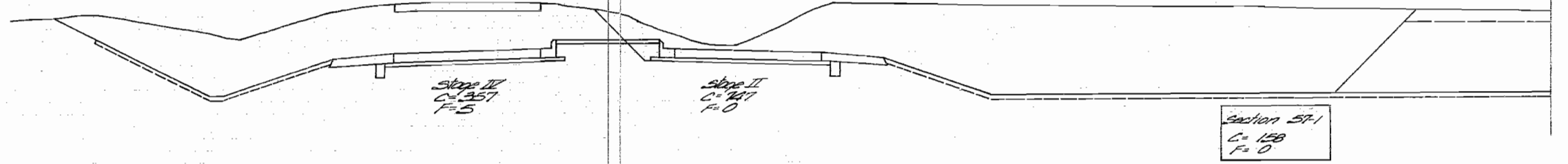
718.19



1543
+00
720

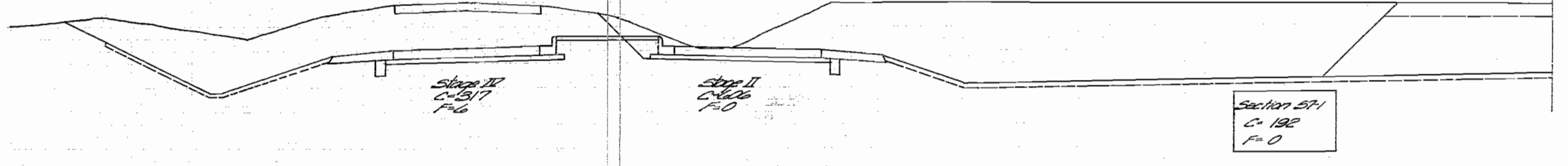
Earthwork Match

718.31



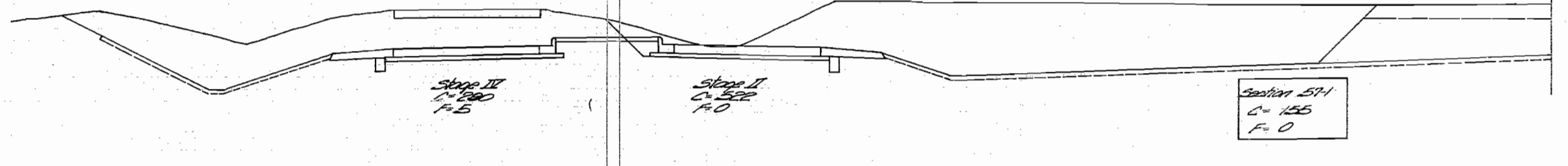
1547
+00
720

718.45



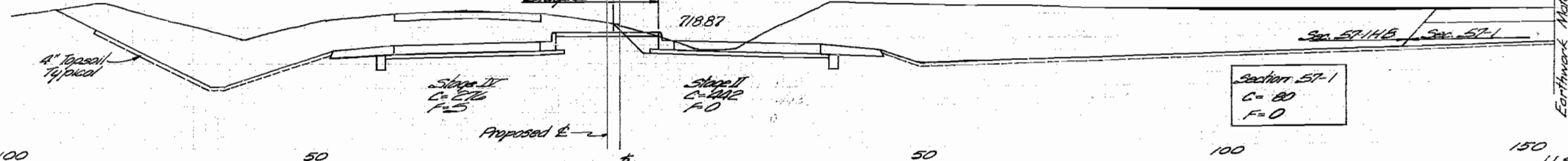
1546
+00
720

718.64



1545
+00
720

718.87



1544
+00
720

Earthwork Match

Proposed E

150

100

50

±

50

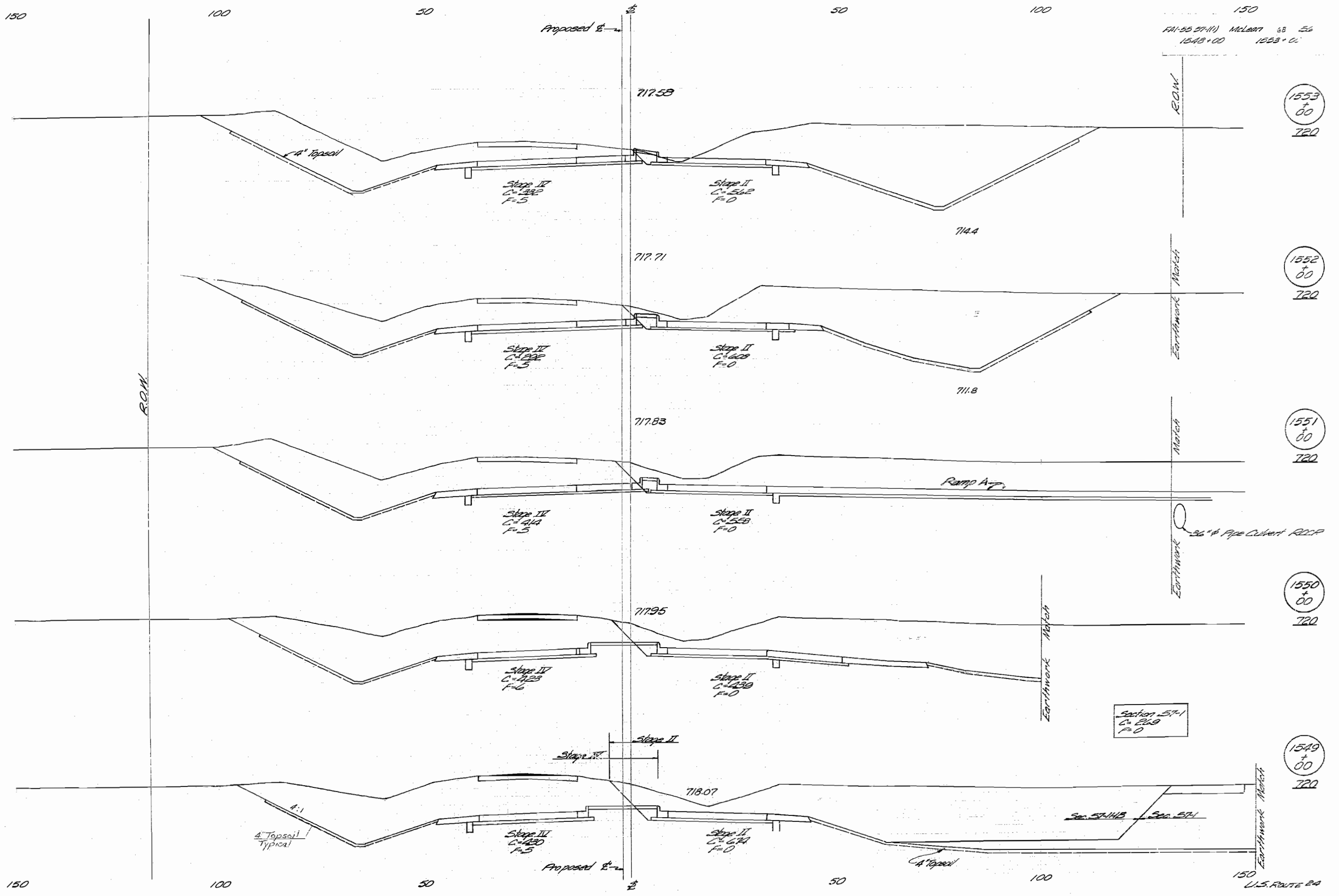
100

150

U.S. ROUTE 24

R.O.W.

FM: 95 27-11) MCL 107 38 26
1548+00 1553+00



1553
+
00
720

1552
+
00
720

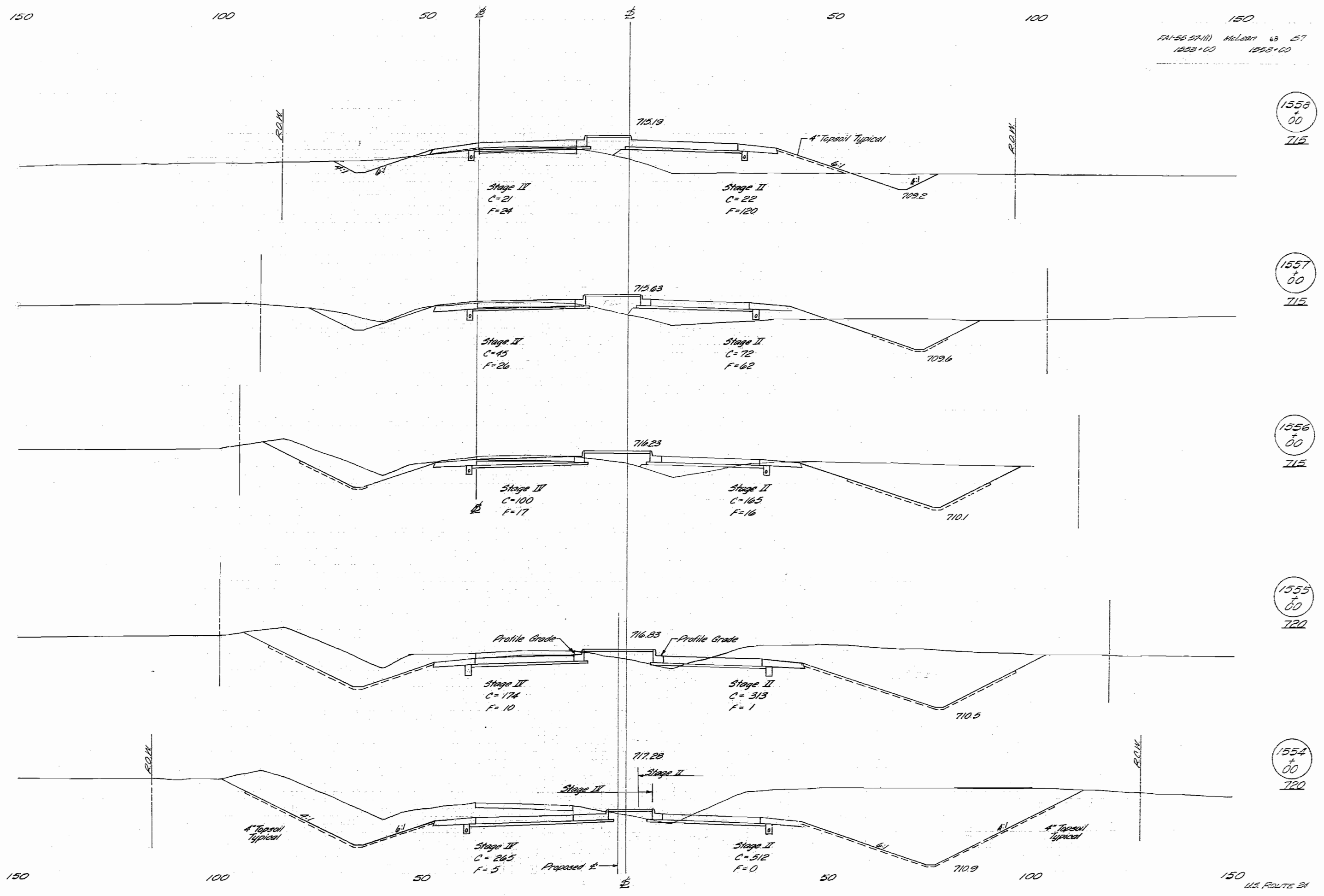
1551
+
00
720

1550
+
00
720

1549
+
00
720

U.S. ROUTE 24

FH-56-57(III) McLean 68 57
1553+00 1558+00



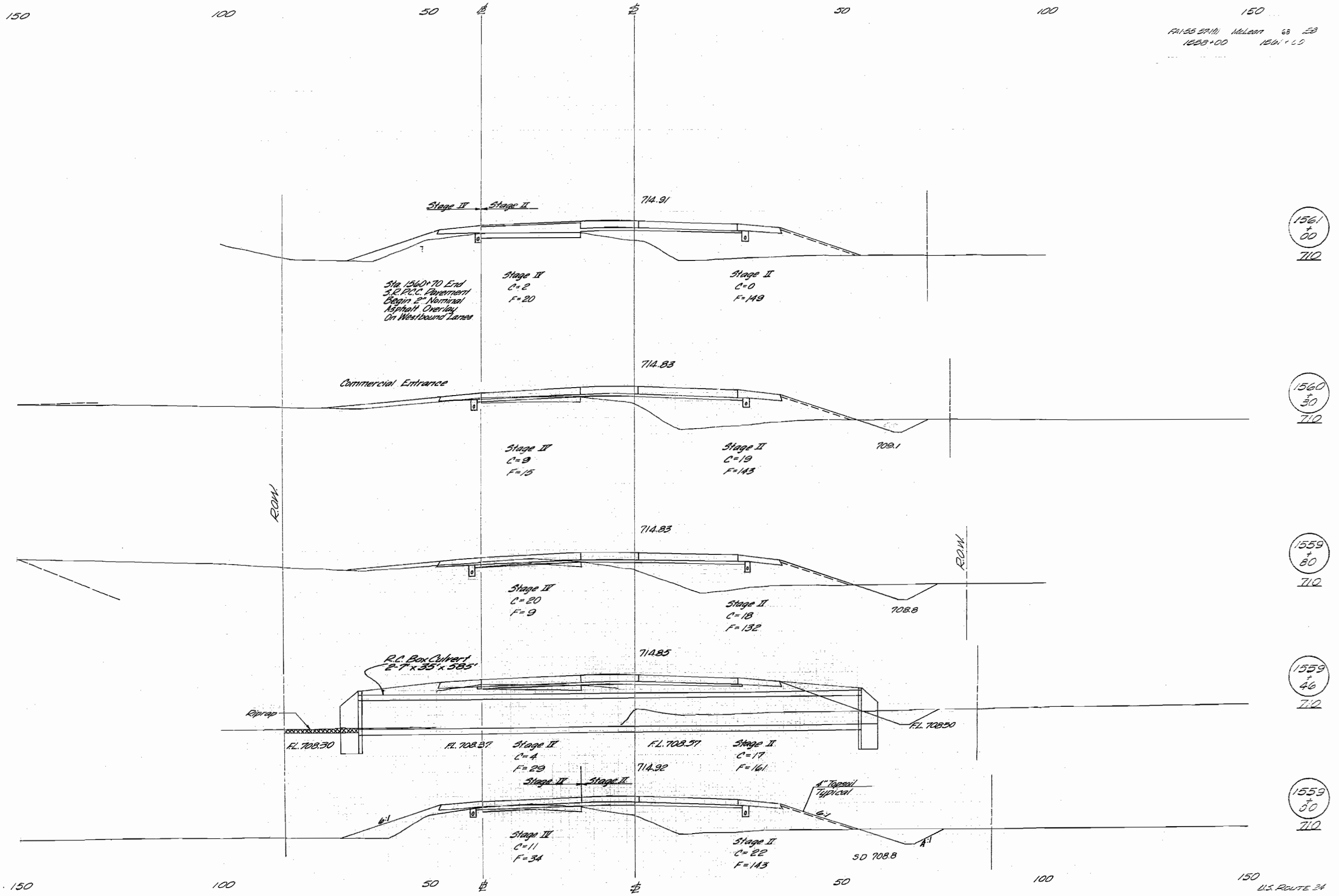
1558
+00
715

1557
+00
715

1556
+00
715

1555
+00
720

1554
+00
720



1561
+00
710

1560
+30
710

1559
+80
710

1559
+46
710

1559
+50
710

150

100

50

0

0

50

100

150

FA-5557(11) M.L.B.M. 68 50
1561+00 1563+50.00

End Construction
Westbound Lanes
Sta. 1563+61.55

715.97

W. Edge of Existing
U.S. 66 Pavement

1563
+ 52
710

715.77

1563
+ 46
710

715.70

Stage II
C=0
F=0

1563
+ 31
710

715.57

End Construction Eastbound
Lanes Sta. 1563+00

1563
+ 00
710

Stage II
C=19
F=12

Stage II
C=5
F=59

1562
+ 00
710

715.16

Stage II Stage II

4" Topsoil
Typical

Stage II
C=2
F=14

Stage II
C=0
F=144

R.O.W.

R.O.W.

R.O.W.

150

100

50

0

0

50

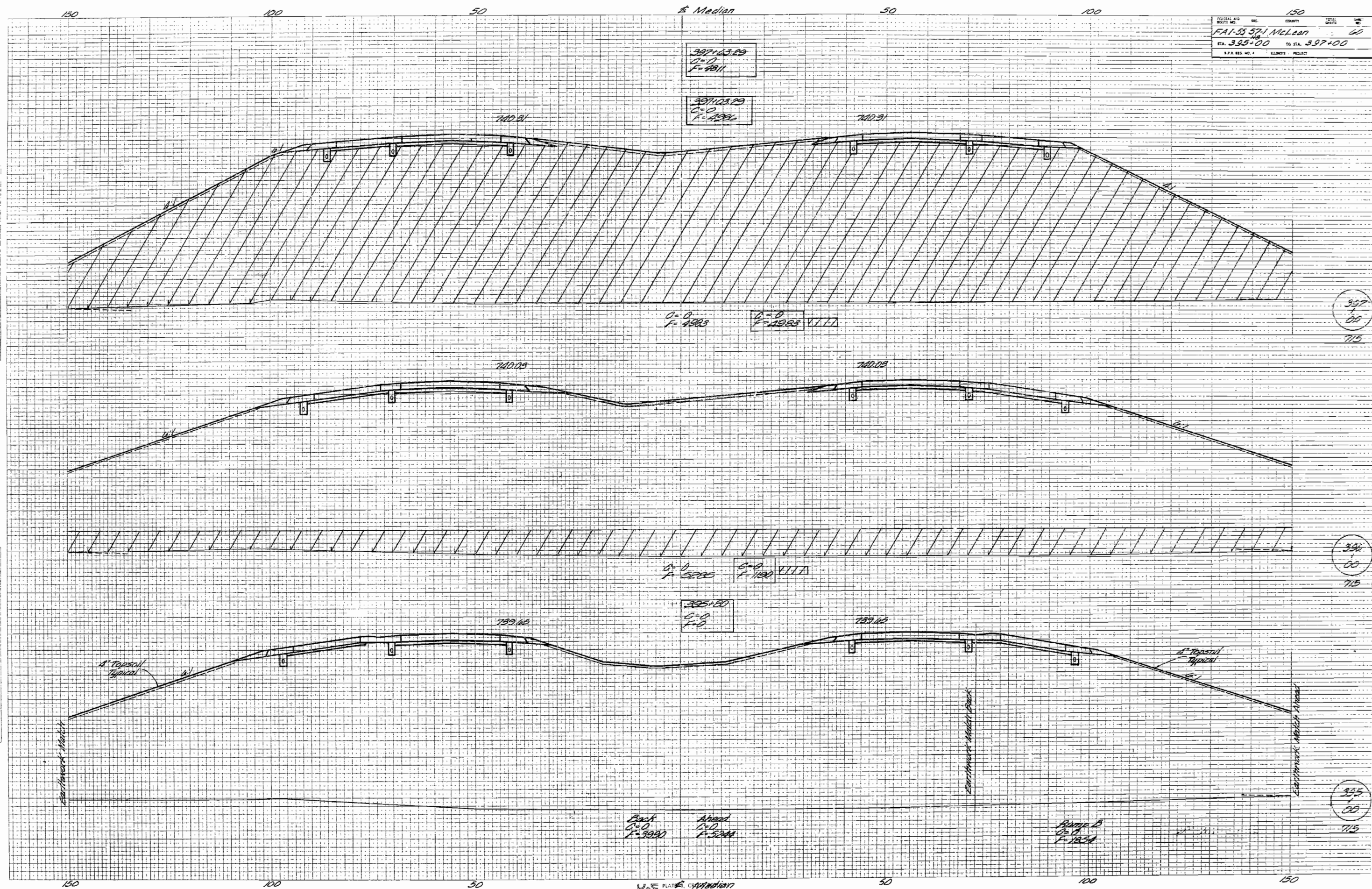
100

150

U.S. ROUTE 24

FINAL SURVEY PLOTTED BY [unclear] CHECKED BY [unclear] DATE FILED [unclear]

ORIGINAL SURVEY PLOTTED BY [unclear] CHECKED BY [unclear] DATE FILED [unclear]



397
00
715

396
00
715

395
00
715

150

100

50

E Median

50

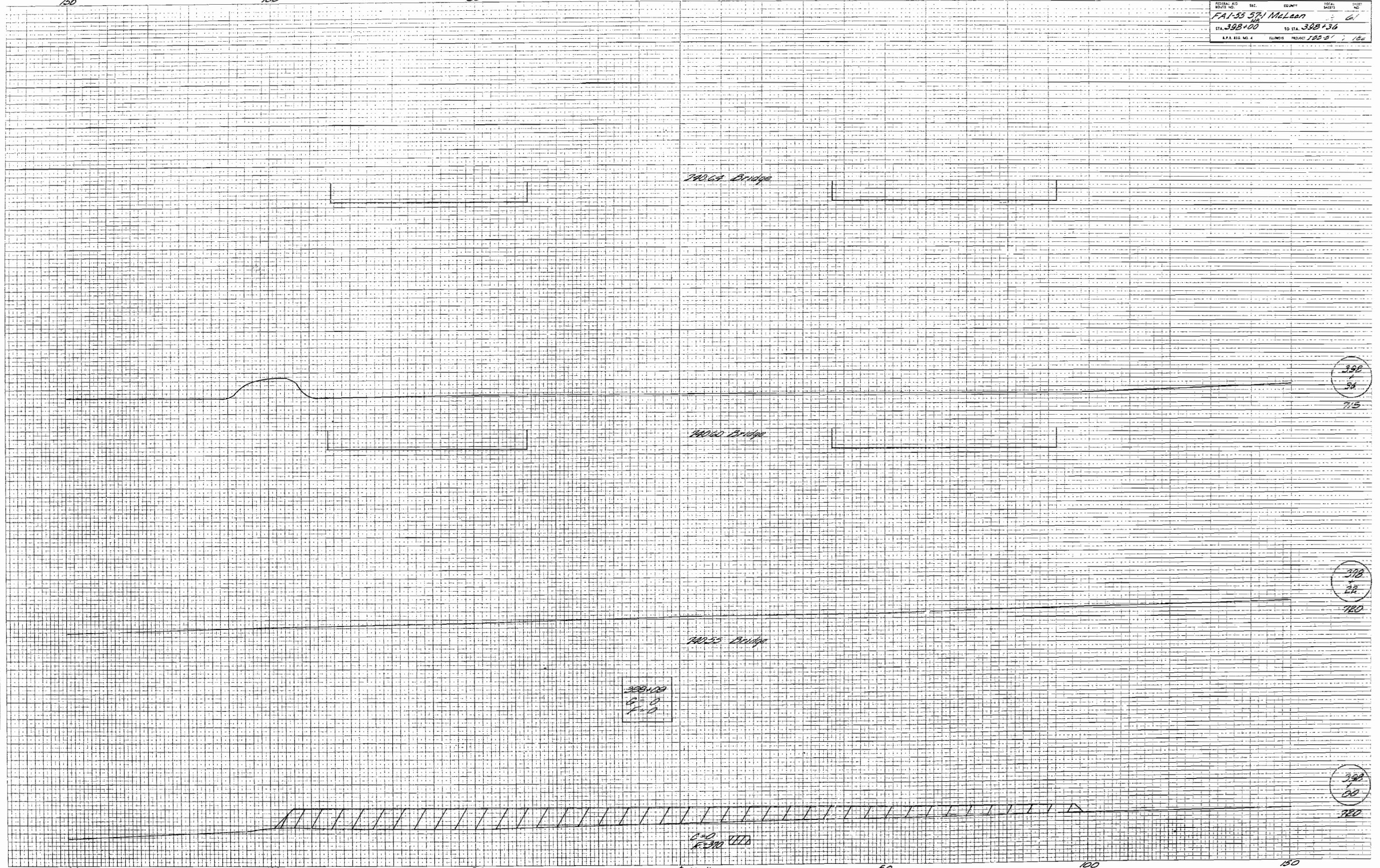
100

150

FEDERAL AID ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-55	571	McLean	61	61
STA. 398+00	TO STA. 398+36			
S.P.A. REG. NO. 4		ILLINOIS PROJECT 195-51	12a	

FINAL SURVEY
 NO. 100115
 DATE 11/17/51
 AREA AS SHOWN
 AREA 174.15
 AREA 174.15

ORIGINAL SURVEY
 NO. 100115
 DATE 11/17/51
 AREA AS SHOWN
 AREA 174.15
 AREA 174.15



398+00
 6-0
 1-0

398
 58
 715

398
 22
 780

398
 50
 780

150

100

50

± Median

50

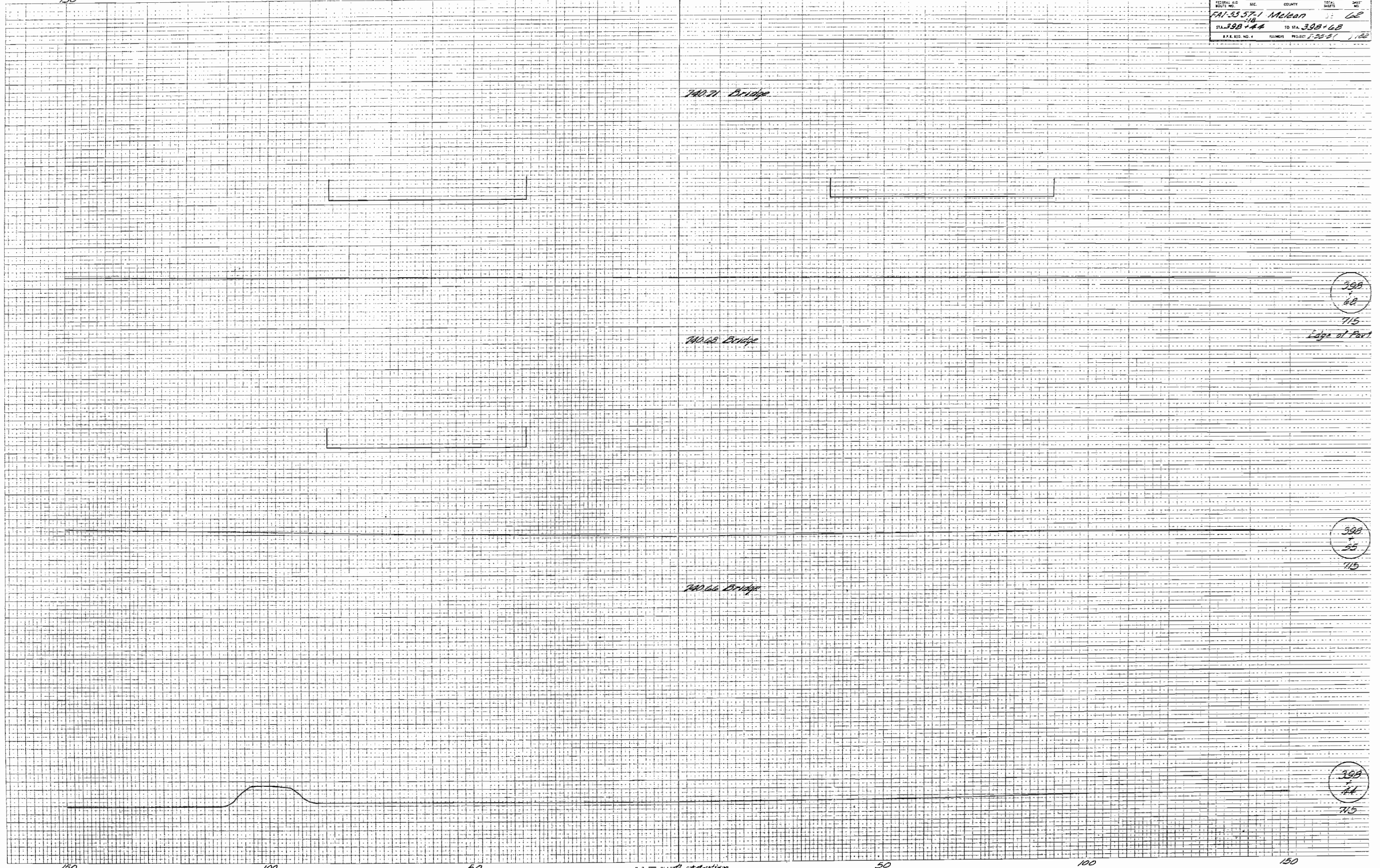
100

150

FEDERAL AID DISTRICT NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-55-57-1	18	Melean	62	62
STA. 398+44		TO STA. 398+68		
STATE PROJECT NO.	FEDERAL PROJECT NO.	DATE		
		1/22/51		

FINAL SURVEY PLATINUM NOTE BOOK NO.

ORIGINAL SURVEY PLATINUM NOTE BOOK NO.



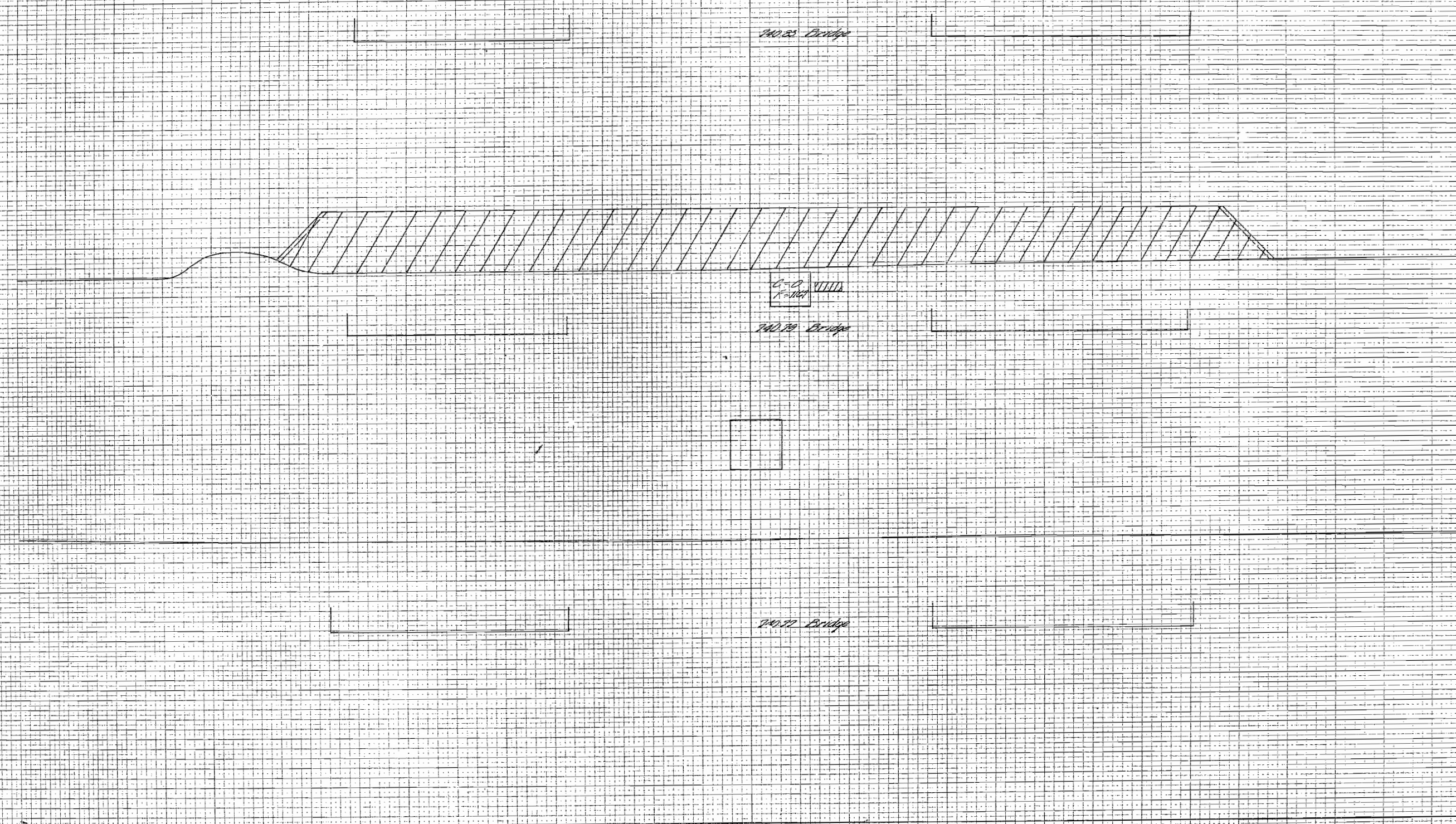
398
68
715
Edge of Part

398
44
715

398
44
715

FEDERAL AID DISTRICT NO.	SEC.	COUNTY	TOWNSHIP	SHEET NO.
FA-55	57	McLean		63
STA. 398+92	TO STA. 399+17			
S.P.A. DIST. NO. 4		ILLINOIS	PROJECT 13021	1.52

150 100 50 *E Median* 50 100 150



399
17
715

399
00
715

398
92
715

Edge of Pavt.

FINAL SURVEY
NOTE BOOK NO.

ORIGINAL SURVEY
NOTE BOOK NO.

SECTION NO. 150
 COUNTY McLean
 TO STA. 400+00
 FROM STA. 390+24
 PROJECT:

ACQUISITION
 OF
 F-4013

741.08

Topsoil Source

C=26
 F=3000
 1111

740.89 Bridge

399+50.71
 C=0
 F=3000

C=0
 F=3000
 1111

740.89 Bridge

C=0
 F=3000
 1111

400
 +
 00
 715

399
 +
 40
 715

399
 +
 24
 715

150

100

50

Base Line

50

100

150

TOTAL SQ. FEET	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FA1-55-5710	McLeon		65	65
STA. 1701+00	TO STA. 1703+00			
S.P. 1. 889 NO. 4				

FINAL SURVEY NOTE BOOK NO. _____

SUBMITTED FOR REVIEW BY _____

DATE _____

AREAS CHECKED _____

ORIGINAL SURVEY NOTE BOOK NO. _____

SUBMITTED FOR REVIEW BY _____

DATE _____

AREAS CHECKED _____

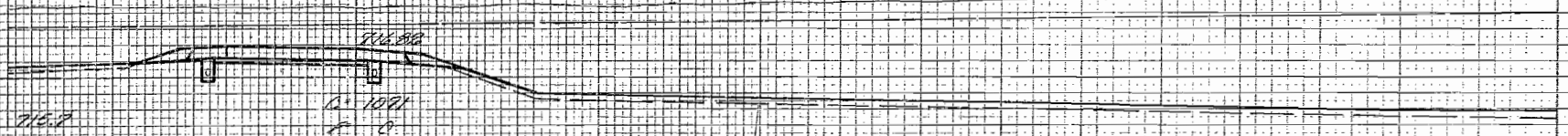
Subgrade Replacement Begins Sta 1702+00

1702+32
G=962
F=0
500.57-1

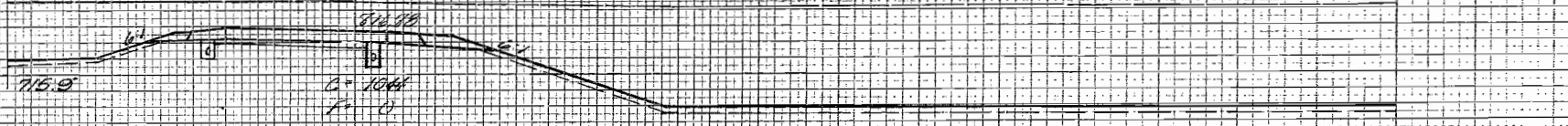
1702+17.07
G=1013
F=0

1702+52 Ramp Construction Ends
G=962
F=0

1702+17.07
G=1013
F=0



1701+53 Back Ahead
G=962 G=1159
F=0 F=0



4" Topsoil Typical

Topsoil Source (See)

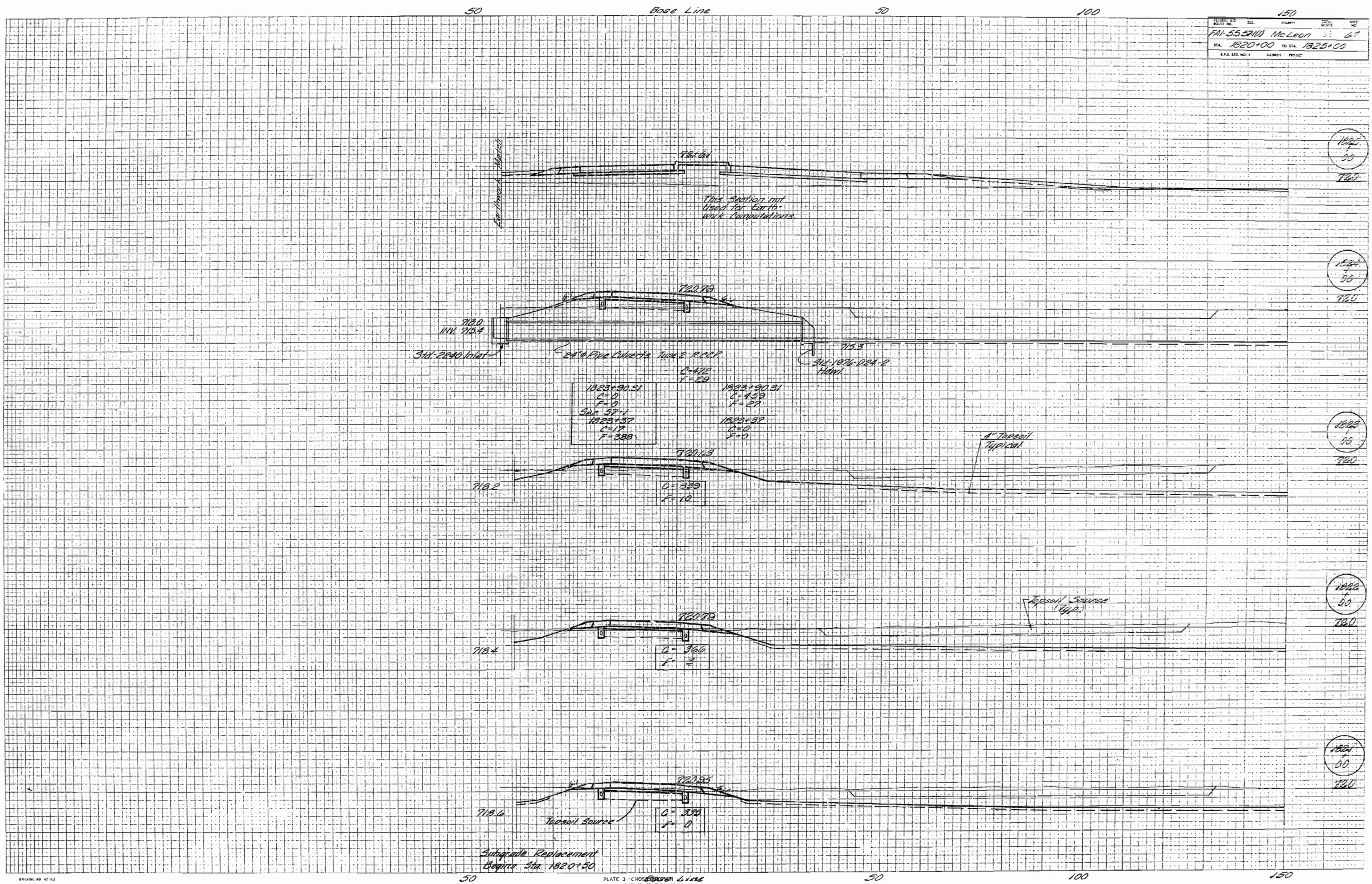
1703
65

1702
65

1701
65

FINAL SURVEY SUBMITTED BY DATE
 SURVEY PLOTTED BY DATE
 NOTE BOOK NO. NO. NO.
 AREAS CHECKED

ORIGINAL SURVEY SUBMITTED BY DATE
 SURVEY PLOTTED BY DATE
 NOTE BOOK NO. NO. NO.
 AREAS CHECKED



This Section not used for earthwork Computations

3rd-2240 Inlet
 24\" Pipe Culverts Two 2 R.C.C.P.
 3rd-1876-DEA-2 Inlet

1823+90.51
 C=0
 F=0
 Set 57+1
 C=17
 F=388

1823+90.51
 C=159
 F=22
 1823+97
 C=0
 F=0

720.63
 C=339
 F=10

720.79
 C=366
 F=3

720.95
 C=393
 F=0

Subgrade Replacement Begins Sta. 1820+50

4\" Topsoil Typical

Topsoil Source (Typ.)

1825+00
50
720

1824+00
50
720

1823+00
50
720

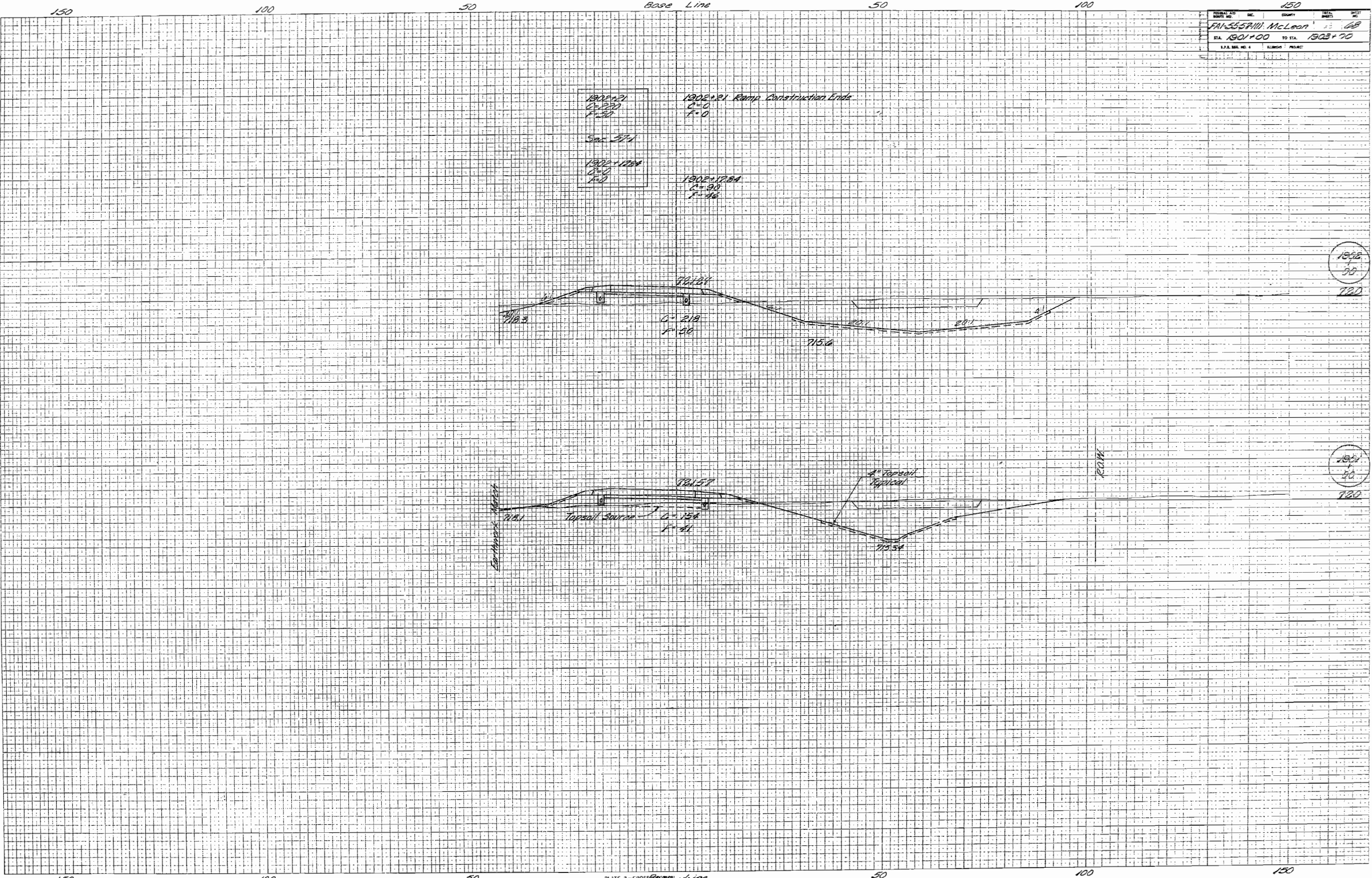
1822+00
50
720

1821+00
50
720

FEDERAL AID DISTRICT NO.	STATE	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-555P(11)	McLean		68	68
STA. 1901+00	TO STA. 1903+70			
S.P.A. MAP NO. 4	SECTION	PROJECT		

FINAL SURVEY
 SURVEYED
 PLOTTED
 NOTE BOOK
 AREAS CHECKED

ORIGINAL SURVEY
 SURVEYED
 PLOTTED
 NOTE BOOK
 AREAS CHECKED



1902
 21
 720

1902
 1784
 720

PLATE 3 - CROSS SECTION Line

Ramp D