

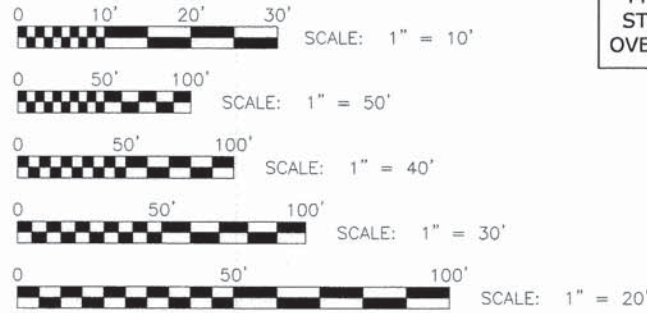
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY  
OVERHILL AVENUE  
LAWRENCE AVENUE TO FOSTER AVENUE  
ROADWAY WIDENING AND RECONSTRUCTION  
PROJECT M-4003(219)  
SECTION 13-00064-00-FP  
VILLAGE OF NORRIDGE  
COOK COUNTY  
C 91-421-13**

FOR INDEX OF SHEETS SEE SHEET NO. 2

**TRAFFIC DATA**

ADT (2013) = 2,500 VPD  
POSTED SPEED LIMIT: 20 MPH  
DESIGN SPEED LIMIT: 30 MPH  
MINOR ARTERIAL

PROJECT LOCATED IN  
THE VILLAGE OF NORRIDGE  
NORWOOD PARK TOWNSHIP



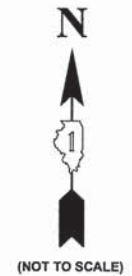
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

PROJECT ENDS  
STATION 26+76  
OVERHILL AVENUE

LOCATION MAP  
RANGE 12 EAST PRINCIPAL MERIDIAN



MAP SCALE : NONE



(NOT TO SCALE)

█ - AREA OF IMPROVEMENT  
GROSS AND NET LENGTH OF PROJECT = 2,576 FT. = 0.488 MI.

PROJECT BEGINS  
STATION 1+00  
OVERHILL AVENUE

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	1
ILLINOIS PROJECT				
SECTION				
CONTRACT NO. 61A82				



LOCATION OF SECTION  
INDICATED THUS: █

**STATE OF ILLINOIS  
ILLINOIS DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

APPROVED October 16 20 14  
*[Signature]*  
VILLAGE OF NORRIDGE, PRESIDENT

PASSED November 7 20 14  
*[Signature]*  
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID  
BASED ON LIMITED  
REVIEW November 10 20 14  
*[Signature]*  
DEPUTY DIRECTOR OF HIGHWAYS,  
REGION 1 ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS



SIGNED: Stephen B. Clark  
DATE: 10/16/14 LICENSE EXPIRES: 11-30-15



**Know what's below.  
Call before you dig.**

CONTRACT NO. 61A82

**HANCOCK ENGINEERING**  
9933 WEST ROOSEVELT ROAD  
WESTCHESTER, IL 60154  
PHONE: (708)865-0300  
FAX: (708)865-1212  
WWW.EHANCOCK.COM

Drawing file: W:\Projects\by\_Village\Norridge\6121322 - Overhill Ave Pk (1)1\_Cover.dwg Oct 02, 2014 - 3:49pm

PROGRAM AND OFFICE ENGINEER: FAWAD AQUEEL, P.E.  
SCHAUMBURG, ILLINOIS  
PHONE: (847)705-4021

CONSULTANT:  
EDWIN HANCOCK ENGINEERING COMPANY  
9933 ROOSEVELT ROAD  
WESTCHESTER, ILLINOIS 60154  
PHONE: (708)865-0300

**INDEX OF SHEETS**

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1	TITLE SHEET, LOCATION MAP
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3	GENERAL NOTES
4-5	SUMMARY OF QUANTITIES
6-7	TYPICAL SECTIONS
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19-31	UTILITIES PLAN AND PROFILE
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40	STREET LIGHT PLAN
41	STREET LIGHT WIRING DIAGRAM
42	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
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50	LIGHT POLE FOUNDATION, METAL (BE-305)
51	ALUMINUM LIGHT POLE 30'-0" (9.144m) MOUNTING HEIGHT (BE-403)
52	MISC. ELECTRICAL DETAILS, SHEET A (BE-702)
53	DISTRICT 1 - STANDARD TRAFFIC SIGNAL DESIGN DETAILS (TS-05)
54	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)
55-62	CROSS SECTIONS

**BENCHMARKS**

BENCHMARK NO.	DESCRIPTION	ELEVATION
1	NORTHWEST FLANGE BOLT ON FIRE HYDRANT AT NORTHEAST CORNER OF OVERHILL AND GUNNISON	647.65
2	NORTHWEST FLANGE BOLT ON FIRE HYDRANT AT 4922 OVERHILL	648.29
3	NORTHWEST FLANGE BOLT ON FIRE HYDRANT AT NORTHWEST CORNER OF OVERHILL AND STRONG	648.27
4	NORTHEAST FLANGE BOLT ON FIRE HYDRANT AT 5025 OVERHILL	651.96
5	NORTHEAST FLANGE BOLT ON FIRE HYDRANT AT 5177 OVERHILL	650.37
6	NORTHEAST FLANGE BOLT ON FIRE HYDRANT AT SOUTHEAST CORNER OF OVERHILL AND WINONA	649.82

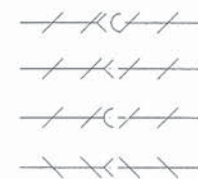
**LEGEND OF SYMBOLS**

(TO BE USED IN CONJUNCTION WITH I.D.O.T. STANDARD 000001-06)

EXISTING	PROPOSED	DESCRIPTION	EXISTING	PROPOSED	DESCRIPTION
		MAILBOX TO BE RELOCATED			
		POWER POLE			
		GAS VALVE			
		STREET LIGHT POLE			
		WATER MAIN BUFFALO BOX			
		WATER MAIN VALVE BOX			
		SPRINKLER			
		WATER MAIN VALVE VAULT			
		STORM SEWER MANHOLE			
		STORM CATCH BASIN			
		STORM INLET			
		FIRE HYDRANT			
		EXISTING STRUCTURE TO BE REMOVED			
		EXISTING STRUCTURE TO BE FILLED			
		TREE TO BE REMOVED			
		BUSH			
		TREE			
		EXISTING CURB AND GUTTER TO BE REMOVED			
		EARTH EXCAVATION			
		EXISTING CONCRETE SIDEWALK TO BE REMOVED			
		EXISTING CONCRETE DRIVEWAY TO BE REMOVED			
		HOT-MIX ASPHALT SURFACE REMOVAL- BUTT JOINT			
		EXISTING HOT-MIX ASPHALT DRIVEWAY TO BE REMOVED			
		COMBINATION SEWER PIPE TO BE REMOVED			
		STORM SEWER PIPE TO BE REMOVED			
		SANITARY SEWER PIPE TO BE ABANDONED			
		STORM SEWER PIPE TO BE ABANDONED			

**I.D.O.T. STANDARD DRAWINGS**

STANDARD NO.	TITLE OR DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424026-01	ENTRANCE/ALLEY PEDESTRIAN CROSSINGS
602601-03	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701101-04	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER OF CROSSWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS
729001-01	APPLICATIONS OF TYPES A&B METAL POSTS
780001-05	TYPICAL PAVEMENT MARKINGS



**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

**INDEX OF SHEETS, LEGEND OF SYMBOLS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	2
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

## ACCESS

THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN INGRESS AND EGRESS FOR EMERGENCY VEHICLES AT ALL TIMES DURING THE CONSTRUCTION PERIOD. DURING CONSTRUCTION, GARBAGE TRUCKS MUST BE PERMITTED ACCESS TO THE WORK ZONE IN ORDER TO PICK UP GARBAGE FOR THOSE PROPERTIES IMPACTED BY CONSTRUCTION. IF GARBAGE TRUCK ACCESS IS NOT PROVIDED, THE CONTRACTOR WILL BE REQUIRED TO MOVE TRASH CONTAINERS TO AN ACCESSIBLE LOCATION WHICH WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (SPECIAL).

## UNDERGROUND UTILITIES

THE LOCATIONS OF THE UNDERGROUND UTILITIES AS SHOWN ON THE PLANS HAVE BEEN OBTAINED BY FIELD SURVEYS AND SEARCHES OF AVAILABLE RECORDS. IT IS BELIEVED THAT THE DATA IS ESSENTIALLY CORRECT, BUT THE VILLAGE OF NORRIDGE, THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND/OR OTHER OFFICES AND AGENCIES ASSOCIATED WITH THE DEVELOPMENT OF THESE PLANS DO NOT GUARANTEE THEIR ACCURACY OR COMPLETENESS. THE CONTRACTOR WILL BE REQUIRED TO VERIFY THE LOCATION OF EACH FACILITY WITH THE UTILITY COMPANY, AND SHALL TAKE DUE CARE IN ALL PHASES OF THE CONSTRUCTION TO PROTECT ANY SUCH FACILITIES WHICH MAY BE AFFECTED BY THE WORK. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IN ACCORDANCE WITH ARTICLE LR 105.

ADJUSTMENTS REQUIRED BY UTILITY COMPANIES WILL BE PERFORMED BY THE COMPANY INVOLVED OR ITS CONTRACTOR, BUT WILL BE COORDINATED BY GENERAL CONTRACTOR.

COORDINATION OF ALL UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT A PRE-CONSTRUCTION CONFERENCE.

THE CONTRACTOR SHALL USE EXTREME CAUTION IN THE REMOVAL OF ABANDONED EXISTING GAS LINES SINCE RESIDUAL MATERIALS CONTAINED THEREIN ARE HIGHLY EXPLOSIVE, FLAMMABLE, AND TOXIC. ONCE THE MAINS ARE ABANDONED BY THE OWNER, THE CONTRACTOR SHALL ASSUME ALL RESPONSIBILITY FOR ANY DAMAGE AND/OR INJURY OCCURRING ON THE PROJECT DUE TO HIS OPERATIONS NEXT TO THE MAINS AND/OR THE METHOD OF REMOVAL OF THE ABANDONED MAINS.

## STORM SEWER

THE VERTICAL AND HORIZONTAL CLEARANCES BETWEEN WATER MAINS AND PROPOSED OR EXISTING STORM SEWERS SHALL CONFORM TO THE REQUIREMENTS OF SECTIONS 41-1.02A THROUGH 41-1.02D OF THE "STANDARD SPECIFICATIONS FOR WATER & SEWER MAIN CONSTRUCTION IN ILLINOIS".

## FRAMES AND GRATES

THE TYPE OF FRAMES AND GRATES REQUIRED FOR ALL CATCH BASINS AND MANHOLES LISTED IN THE SUMMARY OF QUANTITIES MAY BE FOUND ON THE PLANS AT THEIR RESPECTIVE LOCATIONS. WHERE LIDS ARE CALLED FOR ON THE PLANS, THEY SHALL BE IN ACCORDANCE WITH ARTICLE 604.01 OF THE STANDARD SPECIFICATIONS AND THE TERM LID IS USED IN LIEU OF GRATE.

ON ALL IMPROVEMENTS, THE FRAMES AND LIDS OF EXISTING CATCH BASINS, INLETS, MANHOLES, AND VALVE VAULTS WHICH ARE TO BE ABANDONED OR REMOVED DUE TO CONSTRUCTION OF THIS IMPROVEMENT ARE TO REMAIN THE PROPERTY OF THE VILLAGE AND BE SALVAGED. THE CONTRACTOR IS TO DELIVER FRAMES AND LIDS TO THE VILLAGE'S PUBLIC WORKS YARD AT THE INTERSECTION OF FOSTER AND CUMBERLAND.

## FORMS FOR CONCRETE SIDEWALKS, DRIVEWAYS, PAVEMENT, AND GUTTER FLAGS

A 2" X 6" BOARD WILL BE USED AS A FORM FOR ALL SIDEWALKS TO BE INSTALLED FIVE INCHES (5") IN THICKNESS. A 2" X 8" BOARD WILL BE USED AS A FORM FOR ALL DRIVEWAYS TO BE INSTALLED SEVEN INCHES (7") IN THICKNESS. A 2" X 10" BOARD WILL BE USED AS THE FORM FOR ALL DRIVEWAYS TO BE INSTALLED EIGHT INCHES (8") IN THICKNESS. A 2" X 12" BOARD WILL BE USED AS THE FORM FOR THE FACE OF THE GUTTER FLAGS TO BE INSTALLED TEN INCHES (10") IN THICKNESS. ALL FORMS MUST BE OF A MINIMUM HEIGHT OF THE PROPOSED THICKNESS OF THE RESPECTIVE CONCRETE ITEMS TO BE INSTALLED.

## OPEN EXCAVATION

THE CONTRACTOR WILL NOT BE ALLOWED TO LEAVE EXCAVATIONS OPEN OVERNIGHT. THE CONTRACTOR WILL BE RESPONSIBLE FOR COMPLETELY BACKFILLING OR PLATING OVER OF ALL EXCAVATIONS AT THE END OF EACH DAY. IF THE EXCAVATIONS ARE BACKFILLED THEY SHALL BE FILLED WITH AN AGGREGATE MEETING THE GRADATION OF CA-6. THE MATERIAL WILL BE COMPACTED SUFFICIENTLY TO PREVENT RUTTING OR SETTLEMENT OF MATERIAL UNDER TRAFFIC LOADS. IF PLATES ARE USED THEY SHALL BE OF SUFFICIENT THICKNESS TO SUPPORT VEHICULAR LOADS. ADDITIONALLY THEY SHALL EXTEND A MINIMUM OF NINE INCHES (9") BEYOND THE LIMITS OF THE EXCAVATION ON ALL SIDES. IF THE PLATES ARE TO BE LEFT OVER THE WEEKEND, THE EDGES OF THE PLATES SHALL BE CUSHIONED WITH A BITUMINOUS MIXTURE IN AREAS WHERE VEHICULAR TRAFFIC WILL CROSS THE PLATES.

THE COSTS FOR PROVIDING THE AGGREGATE, PLATES AND BITUMINOUS MIXTURE WILL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE VARIOUS CONTRACT ITEMS.

## MAINTENANCE OF SEWER FLOWS

THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS SO AS TO MAINTAIN AT ALL TIMES FLOWS THROUGH EXISTING STORM AND SANITARY SEWER SYSTEMS. HE SHALL ALSO PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT IF NECESSARY AND A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER COLLECTED IN A SAFE MANNER WITHOUT DAMAGE OF ANY KIND TO ADJACENT PROPERTIES. THE ENDS OF EXISTING DRAINAGE LINES WHICH ARE NOT TO BE INCORPORATED INTO THE PROJECT ARE TO BE SEALED AS SPECIFIED IN THE SPECIAL PROVISIONS. THE COST OF ALL THE PREVIOUSLY MENTIONED WORK SHALL BE INCLUDED IN THE RESPECTIVE SEWER PAY ITEMS. ALL ACCUMULATION OF MATERIAL IN THE STRUCTURE DUE TO CONSTRUCTION OPERATIONS AS WELL AS MATERIAL EXISTING BEFORE CONSTRUCTION, SHALL BE REMOVED BY THE CONTRACTOR AT HIS EXPENSE.

## EXISTING STRUCTURE MODIFICATIONS

ALL KNOWN EXISTING STRUCTURES IN THE PAVEMENT OR ADJACENT AREAS WHICH ARE INVOLVED IN THE CONSTRUCTION HAVE BEEN SHOWN ON THE PLANS AND NOTED AS TO BE REMOVED, FILLED, RECONSTRUCTED, OR ADJUSTED BY THE CONTRACTOR EXCEPT THOSE OF AMERITECH, COMED, AND THE NICOR GAS COMPANY, WHICH ARE TO BE ADJUSTED BY THE APPROPRIATE UTILITY FORCE. WHERE EXISTING STRUCTURES ARE TO BE REMOVED OR FILLED, OR THE EXISTING CASTING REPLACED, THE CASTINGS REMOVED FROM THE STRUCTURE ARE TO REMAIN THE PROPERTY OF THE VILLAGE AND BE SALVAGED. THE CONTRACTOR IS TO DELIVER FRAMES AND LIDS TO THE VILLAGE'S PUBLIC WORKS YARD AT THE INTERSECTION OF FOSTER AND CUMBERLAND.

## SHEETING OR SHORING

ANY SHEETING OR SHORING REQUIRED FOR THE STORM SEWER INSTALLATION OR OTHER CONSTRUCTION ELEMENTS REQUIRING RELATIVELY DEEP EXCAVATIONS, SHALL BE INCLUDED IN THE PARTICULAR PAYMENT ITEM AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR ANY SUPPLEMENTAL WORK ASSOCIATED WITH THE MAINTENANCE OF TRENCH SIDES OR OTHER EXCAVATED AREAS.

## MAINTENANCE OF EXISTING DRAINAGE STRUCTURES

ANY LOOSE MATERIAL THAT IS DEPOSITED IN THE FLOW LINE OF GUTTERS OR DRAINAGE STRUCTURE THAT INHIBITS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE FACILITIES SHALL BE CLEAN AND FREE OF ALL OBSTRUCTIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE RESPECTIVE SEWER PAY ITEMS.

## SAW CUTTING

THE CONTRACTOR SHALL SAW CUT ASPHALT PAVEMENT AND CONCRETE PAVEMENT AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER, TO SEPARATE THE EXISTING PAVEMENT TO BE REMOVED BY MEANS OF AN APPROVED CONCRETE SAW TO A DEPTH AS DIRECTED BY THE ENGINEER. SUITABLE GUIDELINES OR DEVICES SHALL BE USED TO ASSURE CUTTING A NEAT, STRAIGHT LINE AS SHOWN ON THE PLANS. CARE SHALL BE TAKEN BY THE CONTRACTOR SO AS NOT TO DAMAGE THE REMAINING PAVEMENT DIRECTLY ADJACENT TO THE PAVEMENT TO BE REMOVED. ANY DAMAGE TO THE EXISTING PAVEMENT RESULTING FROM PAVEMENT REMOVAL OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THE PRICE OF SAW CUTTING, AS NOTED ABOVE, SHALL BE INCLUDED IN THE PARTICULAR PAY ITEMS.

## PROPOSED STRUCTURES

THE CONTRACTOR SHALL NOT ORDER PROPOSED STRUCTURES UNTIL A JULIE REQUEST HAS BEEN EXECUTED AND THE ENGINEER HAS BEEN NOTIFIED BY THE CONTRACTOR WHETHER ANY CONFLICTS EXISTS.

## ITEMS TO BE SALVAGED

WHERE SPECIFIED ON THE PLANS OR AS DIRECTED BY THE ENGINEER, EXISTING SEWER FRAMES AND LIDS, VALVE BOXES, DOMESTIC WATER SERVICE BOXES, FIRE HYDRANTS AND OTHER CASTINGS ARE TO BE REMOVED BY THE CONTRACTOR. ADDITIONALLY, ALL EXISTING SIGNAGE SCHEDULED FOR REMOVAL WILL ALSO BE APPROPRIATED BY THE CONTRACTOR. THESE ITEMS WILL REMAIN THE PROPERTY OF THE VILLAGE, AND SHALL BE DELIVERED TO THE OWNER AT ITS PUBLIC WORKS FACILITY AT CORNER OF FOSTER AND CUMBERLAND.

## NOTIFICATION OF RESIDENTS

THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING WRITTEN NOTICE TO ALL RESIDENCES AND/OR PLACES OF BUSINESS IN THE WORK ZONE AT LEAST ONE (1) WORKING DAY PRIOR TO PERFORMING ANY CONSTRUCTION ACTIVITY THAT WILL IMPACT ACCESS TO THEIR PROPERTY. THE WRITTEN NOTICE SHALL BE APPROVED BY THE ENGINEER AND COORDINATED WITH THE VILLAGE PRIOR TO THE BEGINNING OF CONSTRUCTION.

## NOTE

THE CONTRACTOR SHALL NOTIFY THE IDOT TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

## TRAFFIC PROTECTION

WHEN WORK COMMENCES, THE CONTRACTOR SHALL ASSUME THE MAINTENANCE OF ALL PAVEMENT, SHOULDERS, DRAINAGE FACILITIES, TRAFFIC CONTROL SIGNS, PAVEMENT MARKINGS, AND OTHER APPURTENANCES ON ROADWAYS WITHIN THE LIMITS OF THE CONTRACT WHICH ARE TO BE USED BY THE PUBLIC DURING CONSTRUCTION AND TO RETAIN THIS MAINTENANCE RESPONSIBILITY UNTIL PROJECT COMPLETION. NEED FOR SNOW AND ICE CONTROL DURING THE CONSTRUCTION PERIOD SHALL BE ACCOMMODATED FOR BY OTHERS. ALL UNBALLASTED TYPE I & TYPE II BARRICADES SHALL HAVE TWO SANDBAGS ONE ACROSS EACH BOTTOM RAIL.

## SOILS REPORT

A SOILS REPORT HAS BEEN PREPARED CONTAINING CERTAIN INFORMATION RELATING TO GENERAL SOIL CONDITIONS TO BE ENCOUNTERED ALONG THE ROUTE OF THE WORK. THE CONTRACTOR WILL BE PERMITTED TO EXAMINE THIS INFORMATION AND DETERMINE ITS VALUE. ANY ADDITIONAL BORINGS DEEMED NECESSARY BY THE CONTRACTOR SHALL BE MADE AT HIS OWN EXPENSE. THE REPORT CAN BE REVIEWED AT THE OFFICE OF HANCOCK ENGINEERING AT 9933 ROOSEVELT ROAD, WESTCHESTER, IL 60154.

## PLUGGING EXISTING SEWERS AND DRAINS

UNLESS OTHERWISE SPECIFIED, ABANDONED SEWERS AND DRAINS, AS DESIGNATED BY THE ENGINEER, SHALL BE PLUGGED AT BOTH ENDS WITH A MINIMUM OF TWO (2) FOOT LONG NON-SHRINK/MORTAR PLUG. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE PAY ITEMS FOR REMOVING AND/OR FILLING THE VARIOUS TYPES OF STRUCTURES.

## UNDERCUT AND AGGREGATE SUBGRADE IMPROVEMENT

A QUANTITY OF AGGREGATE SUBGRADE IMPROVEMENT AND SUBGRADE REMOVAL HAVE BEEN PROVIDED FOR USE IF THE EXISTING SOILS ARE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH IMPROVED SUBGRADE WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHALL BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSUITABLE MATERIAL IS ENCOUNTERED, THE SOIL SHALL BE REMOVED AND REPLACED AS DETERMINED BY THE ENGINEER. IF UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE SPECIFIC QUANTITY OF AGGREGATE SUBGRADE IMPROVEMENT WILL REMAIN UNUSED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

## STORM SEWER STRUCTURES

PRE-CAST REINFORCED CONCRETE SECTIONS FABRICATED IN ACCORDANCE WITH ASTM C-478 WILL BE USED ON ALL STRUCTURES AND RECONSTRUCTED STRUCTURES. FINAL ADJUSTMENT SHALL BE MADE USING PRE-CAST ADJUSTING RINGS. A MAXIMUM OF 6" OF ADJUSTING RINGS WILL BE PERMITTED. THE WORK DESCRIBED WITHIN THE SPECIAL PROVISION FOR FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) SHALL BE PERFORMED WHEN INSTALLING A NEW STORM SEWER STRUCTURE. THIS ITEM WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE UNIT PRICE FOR THE VARIOUS PROPOSED STORM SEWER STRUCTURES IN THE CONTRACT.

## MANHOLE OR VALVE VAULT COVERS

THE WORD "WATER", "SANITARY", OR "STORM" SHALL BE CAST INTO THE LID OF EACH RESPECTIVE MANHOLE OR VALVE VAULT.

## BACKFILLING SEWERS UNDER ROADWAY

FOR SEWERS UNDER THE ROADWAY, BACKFILLING METHODS TWO AND THREE (JETTING) AS DESCRIBED IN ARTICLE 550.07 WILL NOT BE ALLOWED.

## CONCRETE BREAKERS

WHEN REMOVING PAVEMENT, CURB AND GUTTER, SHOULDER, AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS SUCH AS DROP HAMMERS, WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES, WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.

## HOT-MIX ASPHALT PAVING OPERATIONS

THE HOT-MIX BINDER COURSE WILL BE PLACED IN TWO (2) PASSES WITH A COLD JOINT LOCATED A MINIMUM OF SIX INCHES (6") FROM THE CROWN LINE OF THE PAVEMENT. THE HOT-MIX ASPHALT SURFACE COURSE WILL BE PLACED IN TWO (2) PASSES WITH A COLD JOINT LOCATED AT THE CROWN LINE OF THE PAVEMENT. THE CONTRACTOR WILL BE REQUIRED TO SCHEDULE HIS OPERATIONS SO THAT NO SECTIONS OF PAVEMENT ALONG THE CROWN LINE WILL HAVE A COLD JOINT OVERNIGHT.

## OVERHILL AVENUE RECONSTRUCTION VILLAGE OF NORRIDGE, ILLINOIS

## GENERAL NOTES

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-0064-00-FP	COOK	62	3
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

E.H.E. PROJECT NO. 612-13-23201

USER NAME -	DESIGNED -	SBC/CB	REVISED -
	DRAWN -	MK,DMM,ECW	REVISED -
PLOT SCALE -	CHECKED -	WOP	REVISED -
PLOT DATE -	DATE -	10/02/14	REVISED -

**SUMMARY OF QUANTITIES**

S.P.	S.I.	Code	Item	Unit	Total Quantity	Const. Type Code Reconstruct 0004 80%Federal 20%Local
		20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CUYD	600	600
	*	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	70	70
	*	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	70	70
	*	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	70	70
	*	25200100	SODDING	SQ YD	3,500	3,500
	*	25200200	SUPPLEMENTAL WATERING	UNIT	100	100
~		28000510	INLET FILTERS	EACH	31	31
~		30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	700	700
		30300112	AGGREGATE SUBGRADE IMPROVEMENT, 12"	SQ YD	8,750	8,750
		35102100	AGGREGATE BASE COURSE, TYPE B 9"	SQ YD	100	100
		40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	75	75
		40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	3,000	3,000
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	200	200
		40701821	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 7"	SQ YD	8,750	8,750
~		40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	60	60
		42001300	PROTECTIVE COAT	SQ YD	6,149	6,149
		42300100	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 5 INCH	SQ YD	1,500	1,500
		42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	28,000	28,000
~		42400800	DETECTABLE WARNINGS	SQ FT	300	300
~		44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1,500	1,500
~		44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	5,250	5,250
~		44000600	SIDEWALK REMOVAL	SQ FT	22,000	22,000
~	*	56106400	ADJUSTING WATER MAIN 8"	FOOT	20	20

S.P.	S.I.	Code	Item	Unit	Total Quantity	Const. Type Code Reconstruct 0004 80%Federal 20%Local
~	*	56300100	ADJUSTING SANITARY SEWERS, 8-INCH DIAMETER OR LESS	FOOT	300	300
~		550A2540	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 2 18"	FOOT	160	160
~		550A2550	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 2 21"	FOOT	135	135
~		550A2560	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 2 24"	FOOT	640	640
~		550B0340	STORM SEWERS, CLASS B, TYPE 2 12"	FOOT	373	373
~		550B0360	STORM SEWERS, CLASS B, TYPE 2 15"	FOOT	531	531
~		550B0400	STORM SEWERS, CLASS B, TYPE 2 21"	FOOT	55	55
~		56400400	FIRE HYDRANTS TO BE RELOCATED	EACH	1	1
~	*	56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	75	75
~	*	56500800	DOMESTIC WATER SERVICE BOXES	EACH	20	20
~		60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	10	10
~		60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	5	5
		60218300	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	6	6
		60221000	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	9	9
~		60223800	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1
~		60250200	CATCH BASINS TO BE ADJUSTED	EACH	6	6
~		60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	3	3
~		60255500	MANHOLES TO BE ADJUSTED	EACH	4	4
~		60257900	MANHOLES TO BE RECONSTRUCTED	EACH	9	9
~		60265700	VALVE VAULTS TO BE ADJUSTED	EACH	4	4
~		60266100	VALVE VAULTS TO BE RECONSTRUCTED	EACH	8	8
~		60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	10	10
~		60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	25	25

~ DENOTES SPECIAL PROVISION HAS BEEN PROVIDED  
\* DENOTES SPECIALTY ITEM

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USER NAME -	DESIGNED - SBC/CB	REVISED -
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PLOT SCALE -	CHECKED - WOP	REVISED -
PLOT DATE -	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

**SUMMARY OF QUANTITIES**

SCALE: NONE SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	4
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		


**SUMMARY OF QUANTITIES**

S.P.	S.I.	Code	Item	Unit	Total Quantity	Const. Type Code Reconstruct 0004 80%Federal 20%Local
		67100100	MOBILIZATION	L. SUM	1	1
		70106800	CHANGEABLE MESSAGE SIGN	CAL MO	6	6
		72000100	SIGN PANEL - TYPE 1	SQ FT	200	200
		72900100	METAL POST - TYPE A	FOOT	750	750
	*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	850	850
	*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	175	175
~	*	80400100	ELECTRIC SERVICE INSTALLATION	EACH	1	1
~	*	80400200	ELECRIC UTILITY SERVICE CONNECTION	LSUM	1	1
~	*	81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	120	120
~	*	81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	15	15
~	*	81603051	UNIT DUCT, 600V, 3-1C NO.6, 1/C NO.8 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE	FOOT	2,750	2,750
~	*	81702400	ELECTRICAL CABLE IN CONDUIT, 600 V(XLP-TYPE USE) 3-1/C NO. 2	FOOT	15	15
	*	82500335	LIGHTING CONTROLLER, PEDESTAL MOUNTED, 240VOLT, 100AMP	EACH	1	1
	*	83000300	LIGHT POLE, ALUMINUM, 30 FT. M.H., 8 FT. DAVIT ARM	EACH	16	16
	*	83600352	LIGHT POLE FOUNDATION, METAL, 11 1/2" BOLT CIRCLE, 8 5/8" X 6'	EACH	16	16
	*	83800505	BREAKAWAY DEVICE, COUPLING WITH ALUMINUM SKIRT	EACH	16	16
	*	84200500	REMOVAL OF LIGHT UNIT, SALVAGE	EACH	17	17
	*	84200804	REMOVAL OF POLE FOUNDATION	EACH	17	17
~	*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1	1
~	*	88600100	DETECTOR LOOP, TYPE I	FOOT	140	140
~	*	K0026840	SHRUBS TO BE RELOCATED	EACH	15	15

S.P.	S.I.	Code	Item	Unit	Total Quantity	Const. Type Code Reconstruct 0004 80%Federal 20%Local
~		X0325607	GROUND STABILIZATION GEOSYNTHETIC	SQ YD	3,728	3,728
		X0323868	DRAINAGE RESTRICTOR	EACH	13	13
~		X0327611	REMOVE AND REINSTALL BRICK PAVER	SQ FT	100	100
~		X2020410	EARTH EXCAVATION (SPECIAL)	CU YD	5,010	5,010
~		X2080250	TRENCH BACKFILL, SPECIAL	CU YD	1,350	1,350
~	*	X2110104	TOPSOIL FURNISH AND PLACE, 4" (SPECIAL)	SQ YD	3,500	3,500
~		X2130010	EXPLORATION TRENCH, SPECIAL	FOOT	500	500
~		X4023000	TEMP ACCESS (ROAD)	EACH	8	8
~		X6020399	CONNECTION TO EXISTING MANHOLE	EACH	5	5
~		X6022805	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID, SPECIAL	EACH	11	11
~		X6064200	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL)	FOOT	5,500	5,500
~		X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L. SUM	1	1
~	*	X8212160	LUMINAIRE, METAL HALIDE, HORIZONTAL MOUNT, 250 WATT (SPECIAL)	EACH	16	16
~		XX006281	STORM SEWERS, DUCTILE IRON, TYPE 1 10"	FOOT	49	49
~		XX006282	STORM SEWERS, DUCTILE IRON, TYPE 1 12"	FOOT	295	295
~	*	XX006834	ELECTRICAL CONNECTION TO EXISTING LIGHTING SYSTEM	EACH	2	2
~		Z0018700	DRAINAGE STRUCTURE TO BE REMOVED	EACH	30	30
~		Z0019600	DUST CONTROL WATERING	UNIT	100	100
~		Z0023200	FILLING DRAINAGE STRUCTURES	EACH	5	5
~		Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	52	52
~	*	Z0033028	MAINTENANCE OF LIGHTING SYSTEM	CAL MO	4	4

~ DENOTES SPECIAL PROVISION HAS BEEN PROVIDED  
\* DENOTES SPECIALTY ITEM

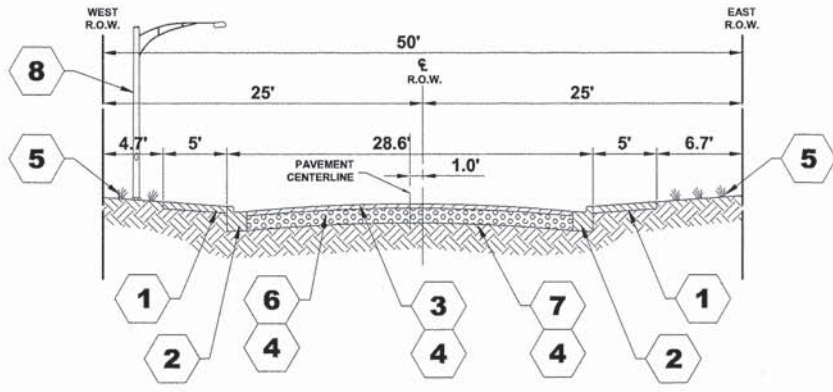
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 <p><b>HANCOCK ENGINEERING</b> Civil Engineers Municipal Consultants Established 1911</p>	USER NAME -	DESIGNED - SBC/CB	REVISED -	<b>OVERHILL AVENUE RECONSTRUCTION</b> <b>VILLAGE OF NORRIDGE, ILLINOIS</b>	<b>SUMMARY OF QUANTITIES</b>		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -		13-0064-00-FP	COOK	62	5			
	PLOT DATE -	CHECKED - WOP	REVISED -		FIELD BOOK NO. 1635/AERIALS	CONTRACT NO. 61A82					
		DATE - 10/02/14	REVISED -		FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT					
SCALE: NONE					SHEET NO. 2 OF 2 SHEETS		STA.		TO STA.		

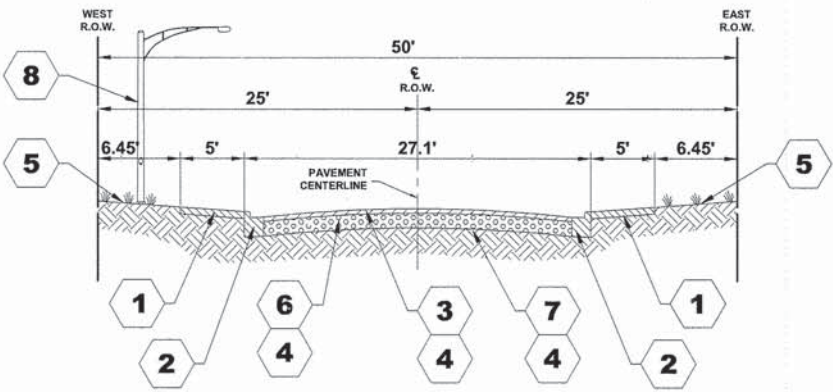
**TYPICAL CROSS SECTION LEGEND**

**EXISTING**

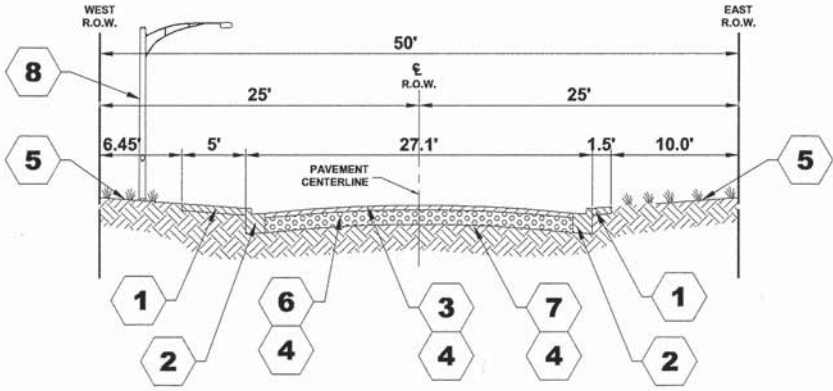
- 1 PORTLAND CEMENT CONCRETE SIDEWALK REMOVAL
- 2 COMBINATION CURB AND GUTTER REMOVAL
- 3 HOT-MIX ASPHALT BINDER & SURFACE COURSE, 3" - 5"
- 4 EARTH EXCAVATION (SPECIAL)
- 5 GRASS PARKWAY
- 6 CRUSHED STONE BASE COURSE, 7" - 10"
- 7 VARIOUS CLAY SOILS
- 8 EXISTING STREET LIGHT REMOVAL



**EXISTING TYPICAL SECTION  
OVERHILL AVENUE  
(STA. 1+00 TO STA. 14+50)**



**EXISTING TYPICAL SECTION  
OVERHILL AVENUE  
(STA. 14+50 TO STA. 24+40)**



**EXISTING TYPICAL SECTION  
OVERHILL AVENUE  
(STA. 24+40 TO STA. 26+76)**

**PAVEMENT CORE SUMMARY**

BORE	STREET	STATION	LANE	BITUMINOUS	STONE BASE
B-1	OVERHILL	1+25	NORTHBOUND LANE	4"	8"
B-2	OVERHILL	4+02	SOUTHBOUND LANE	4"	9"
B-3	OVERHILL	6+80	NORTHBOUND LANE	4"	12"
B-4	OVERHILL	9+57	SOUTHBOUND LANE	5"	7"
B-5	OVERHILL	12+34	NORTHBOUND LANE	5"	14"
B-6	OVERHILL	15+10	SOUTHBOUND LANE	5"	7"
B-7	OVERHILL	17+87	NORTHBOUND LANE	3"	10"
B-8	OVERHILL	20+65	SOUTHBOUND LANE	4"	9"
B-9	OVERHILL	23+42	NORTHBOUND LANE	6"	6"
B-10	OVERHILL	26+19	SOUTHBOUND LANE	4"	0"

**NOTES:**

THESE CORES REFLECT ONLY THE INFORMATION FOUND AT LOCATIONS LISTED. THEY DO NOT REFLECT ANY VARIATIONS WHICH MAY OCCUR BETWEEN THESE BORINGS.

\* CORES OBTAINED BY ECS ON MAY 20, 2013

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USER NAME -	DESIGNED - SBC/CB	REVISED -
	DRAWN - MK,DMM,ECW	REVISED -
PLOT SCALE -	CHECKED - WOP	REVISED -
PLOT DATE -	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

**TYPICAL SECTIONS**

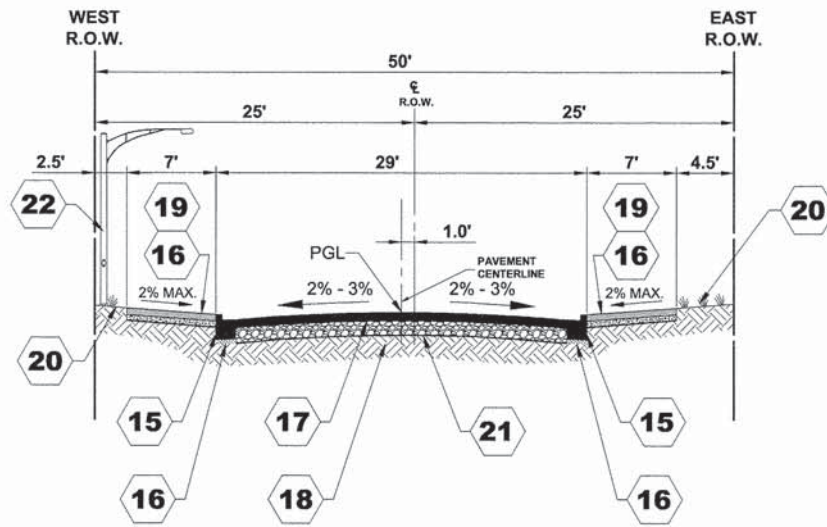
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	6
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

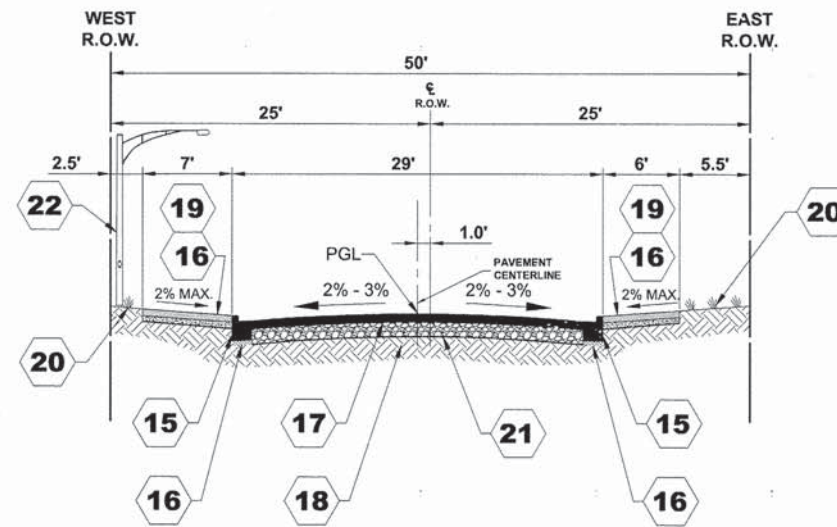
## TYPICAL CROSS SECTION LEGEND

### PROPOSED

- 15** COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL)
- 16** SUBBASE GRANULAR MATERIAL, TYPE B 2" (INCLUDED IN THE COST OF ITEM)
- 17** HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 7"  
- HOT-MIX ASPHALT SURFACE COURSE, MIX D, N50, 1 3/4"  
- HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 5 1/2" (INSTALLED IN TWO LIFTS)
- 18** PROPOSED AGGREGATE BASE COURSE, TYPE B 12"
- 19** PORTLAND CEMENT CONCRETE SIDEWALK, 5"
- 20** TOPSOIL FURNISH AND PLACE, 4" (SPECIAL) SODDING
- 21** GROUND STABILIZATION GEOSYNTHETIC
- 22** PROPOSED STREET LIGHTS (PROPOSED UNDERGROUND CONDUIT)



**PROPOSED TYPICAL HMA SECTION**  
OVERHILL AVENUE  
(STA. 1+00 TO STA. 3+08)



**PROPOSED TYPICAL HMA SECTION**  
OVERHILL AVENUE  
(STA. 3+08 TO STA. 14+50)

## HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
<b>OVERHILL FULL DEPTH PAVEMENT</b>	
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 5 1/2" (2 LIFTS)	4% @ 50 Gyr.
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5mm), 1 3/4"	4% @ 50 Gyr.
<b>INCIDENTAL HOT-MIX ASPHALT SURFACING - HMA PARKWAYS</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 3" (2 LIFTS) (IL 9.5mm)	4% @ 50 Gyr.
<b>TEMPORARY PAVEMENT</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5mm), 2"	4% @ 50 Gyr.
<b>ARMITAGE AVENUE</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5mm), 1 3/4"	4% @ 50 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

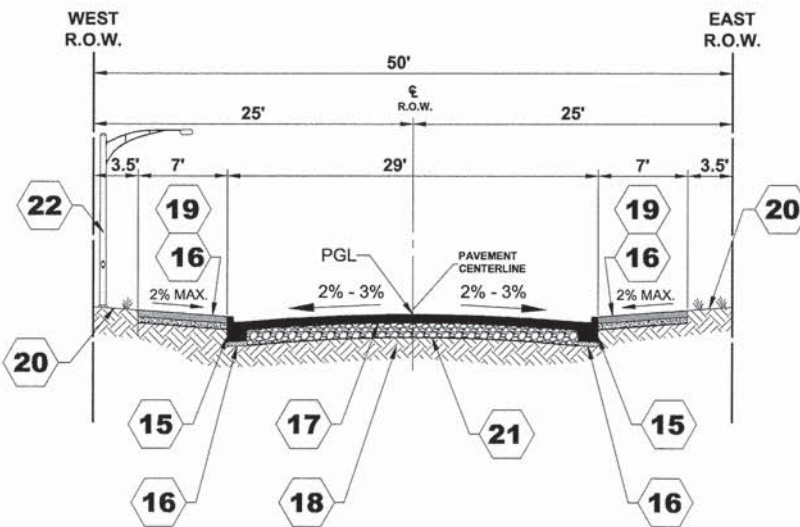
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 MUST BE PLACED WITH A MINIMUM LIFT OF 2 1/4" AND AN INITIAL LIFT OF 3".

MAXIMUM LIFT OF ANY HOT-MIX ASPHALT TO BE 3".

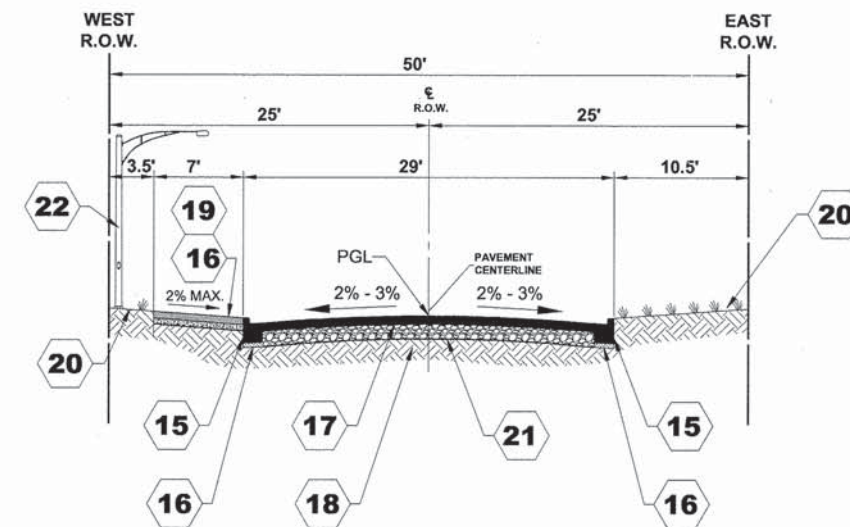
## AREAS TO BE UNDERCUT

☆ STATION 1+00 TO 3+00	12" UNDERCUT	FULL PAVEMENT WIDTH
☆ STATION 5+50 TO 8+00	12" UNDERCUT	FULL PAVEMENT WIDTH

☆ ALL AREAS WILL BE FIELD VERIFIED. NO UNDERCUTS TO BE COMPLETED WITHOUT PRIOR APPROVAL OF ENGINEER.



**PROPOSED TYPICAL HMA SECTION**  
OVERHILL AVENUE  
(STA. 14+50 TO STA. 24+40)



**PROPOSED TYPICAL HMA SECTION**  
OVERHILL AVENUE  
(STA. 24+40 TO STA. 26+76)

Drawing file: W:\Projects\_by\_Village\Norridge\61213232 - Overhill Ave Ph I\08-09\_Detour.dwg Oct 15, 2014 - 2:03pm

LEGEND OF SYMBOLS		
SYMBOL	DESCRIPTION	CODE & SIZE
		W20-3 48"x48"
		W20-2 48"x48"
		R11-2 48"x30"
		M3-1 24"x12"
		M3-3 24"x12"
		CUSTOM 36"x12" 5" BLACK LETTERS ON ORANGE BACKGROUND
		W20-3 48"x48"
		R11-4 60"x30"

LEGEND OF SYMBOLS		
SYMBOL	DESCRIPTION	CODE & SIZE
		M4-8 24"x18"
		M5-3 21"x15"
		M5-1 21"x15"
		M5-1 21"x15"
		M5-1R 21"x15"
		M5-1L 21"x15"
		M4-10 48"x18"
		M4-10 48"x18"
		R3-1 24"x24"
		R3-2 24"x24"

**NOTE:**  
CONTRACTOR TO NOTIFY IDOT HEAD OF TRAFFIC MAINTENANCE (847)705-4470, SEVENTY-TWO HOURS IN ADVANCE OF SETTING UP DETOUR ROUTE.



USER NAME -	DESIGNED - SBC/CB	REVISED -
	DRAWN - MK,DMM,ECW	REVISED -
	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

**DETOUR FOR THROUGH TRAFFIC SIGNING PLAN**

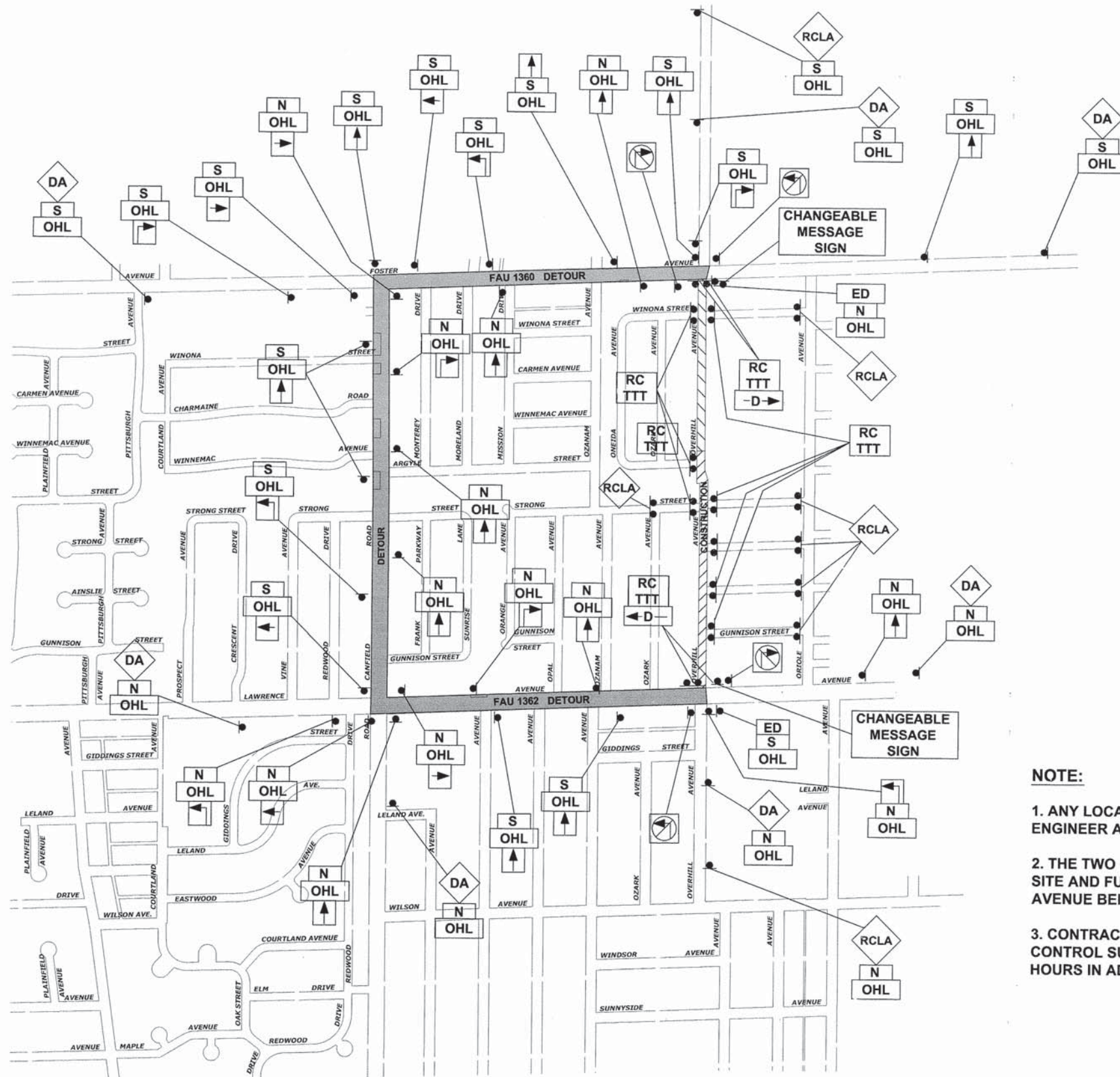
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	8
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



**PROJECT STAGING & LOCAL ACCESS**

1. THE TRENCHES FOR STORM SEWER LATERALS SHALL BE BACKFILLED TO THE TOP OF THE EXISTING PAVEMENT TO PROVIDE LOCAL ACCESS.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR BAGGING ALL REGULATORY SIGNS THAT ARE NOT APPLICABLE DURING CONSTRUCTION.
3. ACCESS MUST BE MAINTAINED FOR GARBAGE COLLECTION AND MAIL DELIVERIES FOR THE DURATION OF THIS PROJECT. IF ACCESS IS UNAVAILABLE DURING PICK-UP, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO RELOCATE REFUSE CONTAINERS TO A SUITABLE LOCATION TO BE PICKED UP. THE CONTRACTOR WILL BE RESPONSIBLE FOR RETURNING THE CONTAINERS TO THE PROPER LOCATION AFTER COLLECTION.
4. ALL UNDERGROUND WORK MUST BE COMPLETED PRIOR TO BEGINNING ROADWORK.



**NOTE:**

1. ANY LOCAL DETOURS SHALL BE APPROVED BY THE ENGINEER AT LEAST 48 HOURS PRIOR TO INSTALLATION.
2. THE TWO CHANGEABLE MESSAGE SIGNS ARE TO BE ON SITE AND FUNCTIONING 1 (ONE) WEEK PRIOR TO OVERHILL AVENUE BEING CLOSED.
3. CONTRACTOR TO NOTIFY THE IDOT ARTERIAL TRAFFIC CONTROL SUPERVISOR (847) 705-4470, SEVENTY-TWO (72) HOURS IN ADVANCE OF SETTING UP DETOUR.

Drawing file: W:\Projects\_by\_Village\Norridge\61213232 - Overhill Ave Ph II\08-05\_Detour.dwg Oct 20, 2014 - 11:04am



913 Riverside Road  
Winnetka, Illinois 60093-2700  
Phone: 708/652-0300  
Fax: 708/652-0322

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PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

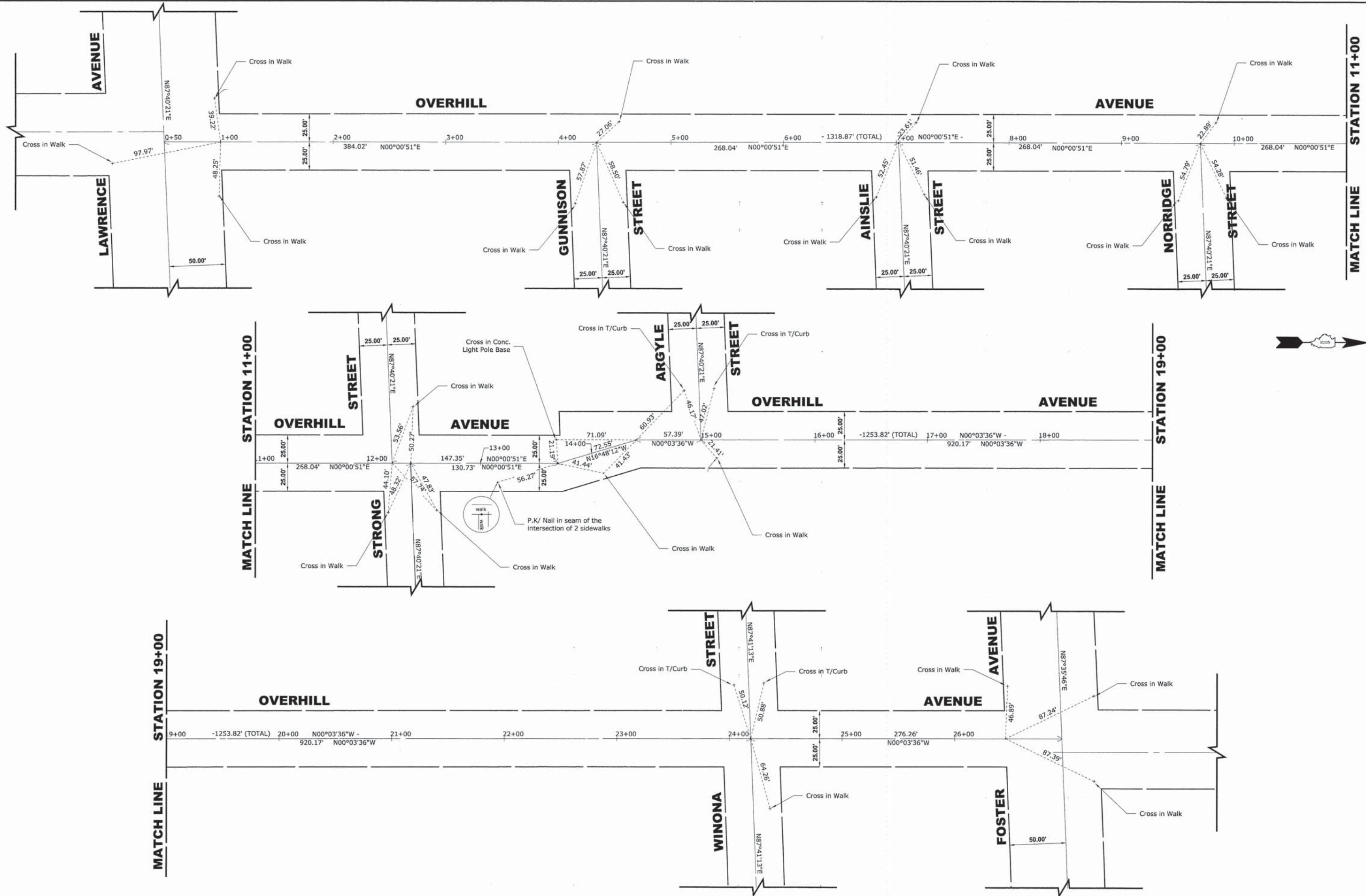
**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

**DETOUR FOR THROUGH TRAFFIC SIGNING PLAN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	9
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

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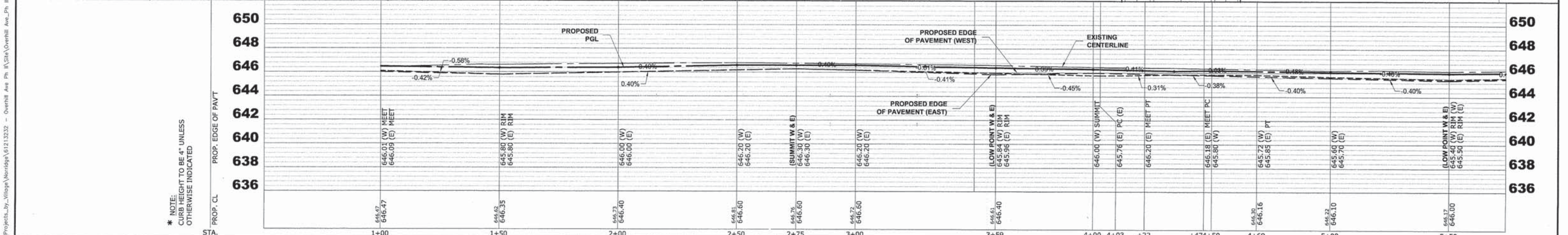
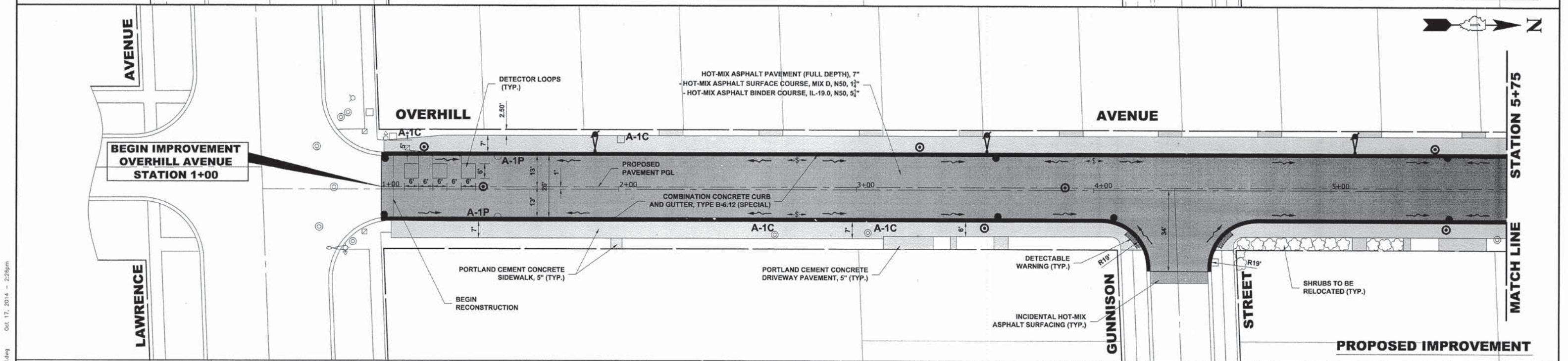
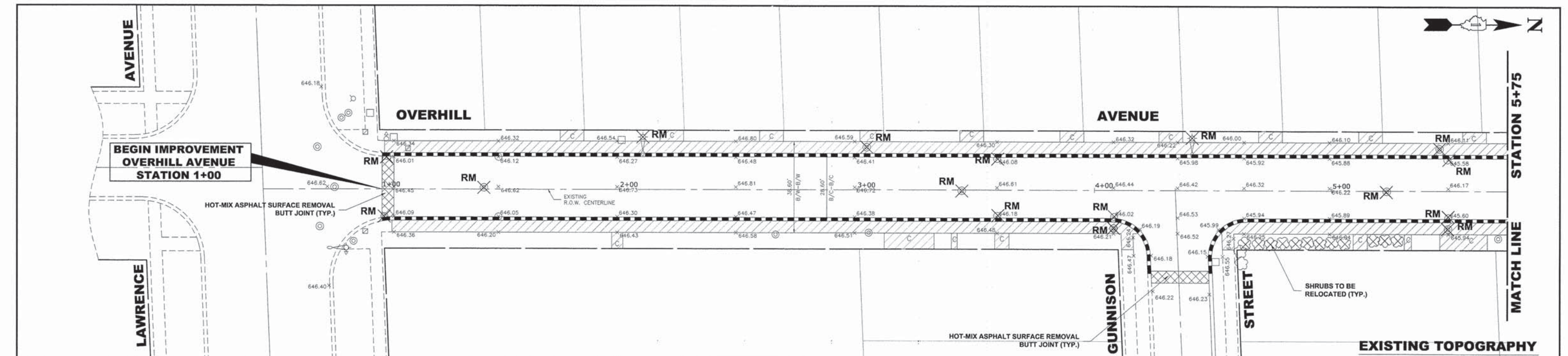
USER NAME -	DESIGNED - SBC/CB	REVISED -
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

<b>OVERHILL AVENUE ALIGNMENT AND TIES</b>	
SCALE: 1"=40'	SHEET NO. 1 OF 1 SHEETS
STA. 0+00	TO STA. 27+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	10
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

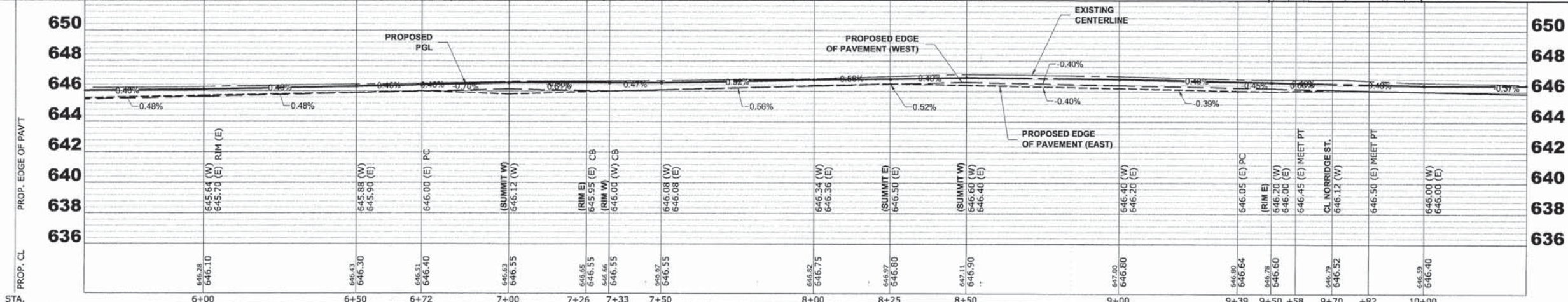
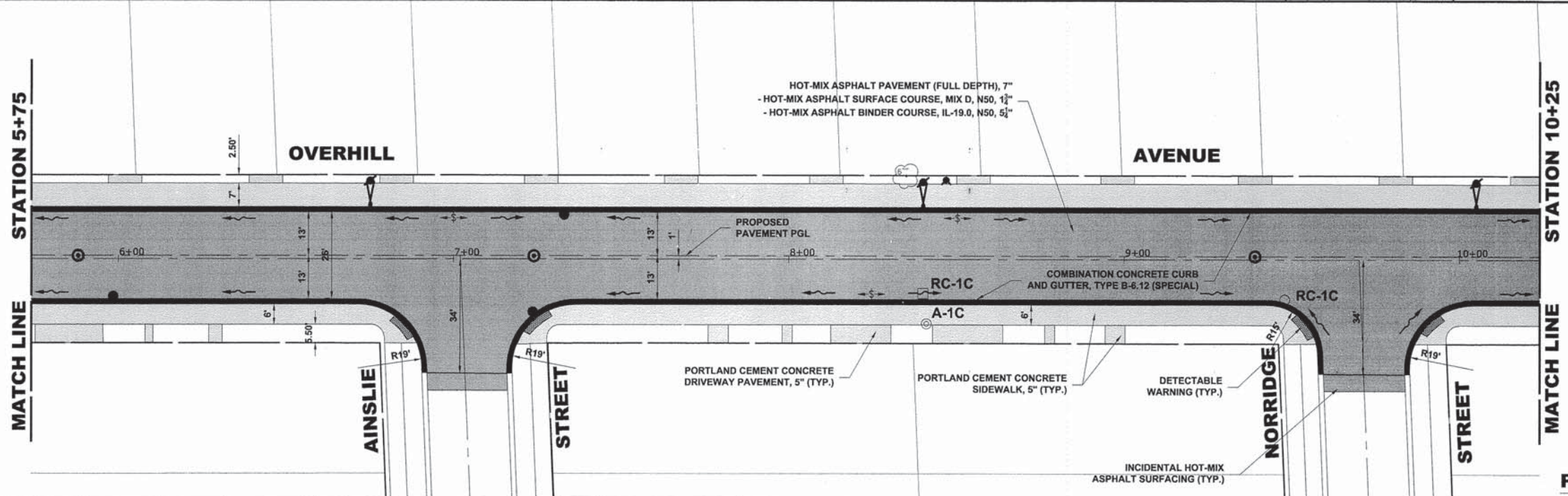
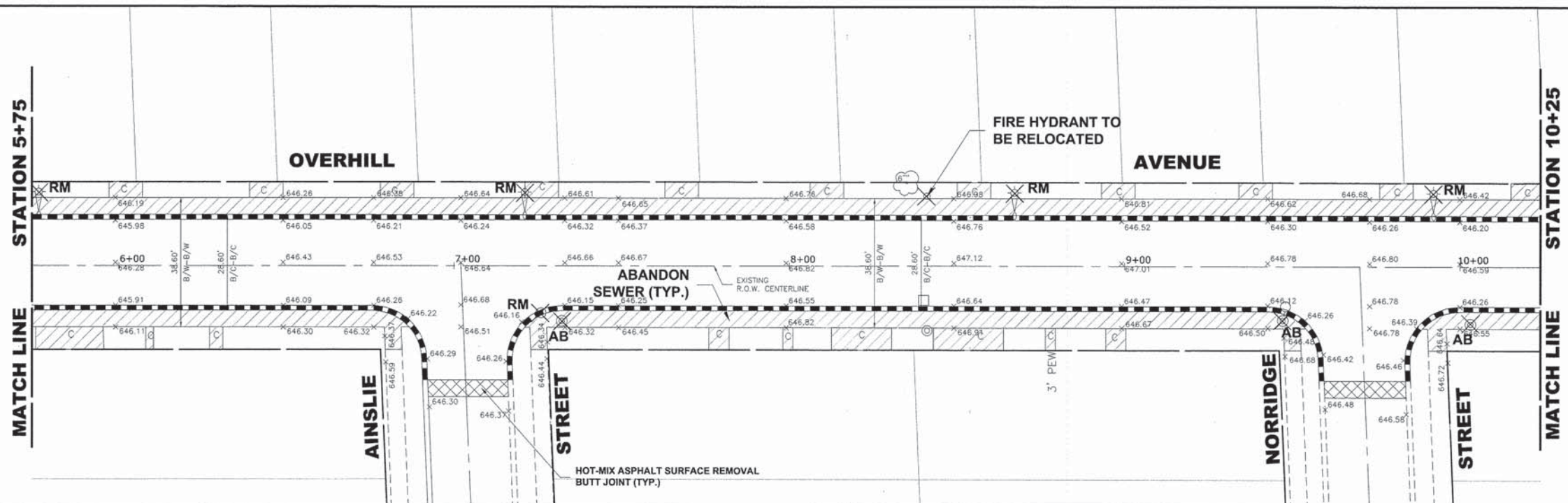
E.H.E. PROJECT NO. 612-13-23201



<b>HANCOCK ENGINEERING</b> Civil Engineers Municipal Consultants Established 1911	USER NAME - DESIGNED - SBC/CB DRAWN - MK,DMM,ECW CHECKED - WOP PLOT DATE -	REVISED - IDOT REVIEW REVISED - REVISED - REVISED -	<b>OVERHILL AVENUE RECONSTRUCTION          VILLAGE OF NORRIDGE, ILLINOIS</b>		<b>OVERHILL AVENUE          PAVING PLAN AND PROFILE</b>		F.A.U. RTE. - SECTION - 13-00064-00-FP FIELD BOOK NO. 1635/AERIALS FED. ROAD DIST. NO. 1	COUNTY - COOK CONTRACT NO. 61A82 ILLINOIS FED. AID PROJECT	TOTAL SHEETS - 62 SHEET NO. - 11
	PLOT SCALE - PLOT DATE -	DATE - 10/02/14 REVISED -	SCALE: H:1"=20', V:1"=4'		SHEET NO. 1 OF 6 SHEETS		STA. 0+00 TO STA. 5+75		E.H.E. PROJECT NO. 612-13-23201
	* NOTE: CURB HEIGHT TO BE 4" UNLESS OTHERWISE INDICATED								
	STA. 1+00 1+50 2+00 2+50 2+75 3+00 3+59 4+00 4+03 +22 +474+50 4+69 5+00 5+50								

Drawing file: W:\Projects\_by\_Village\Norridge\61213232 - Overhill Ave Ph II\Site\Overhill Ave\_Ph II.dwg  
 Oct. 17, 2014 - 2:26pm

Drawing file: W:\Projects\_by\_Village\Norridge\61213232 - Overhill Ave Ph II\Site\Overhill Ave\_Ph\_II.dwg Oct 17, 2014 - 2:27pm



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PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

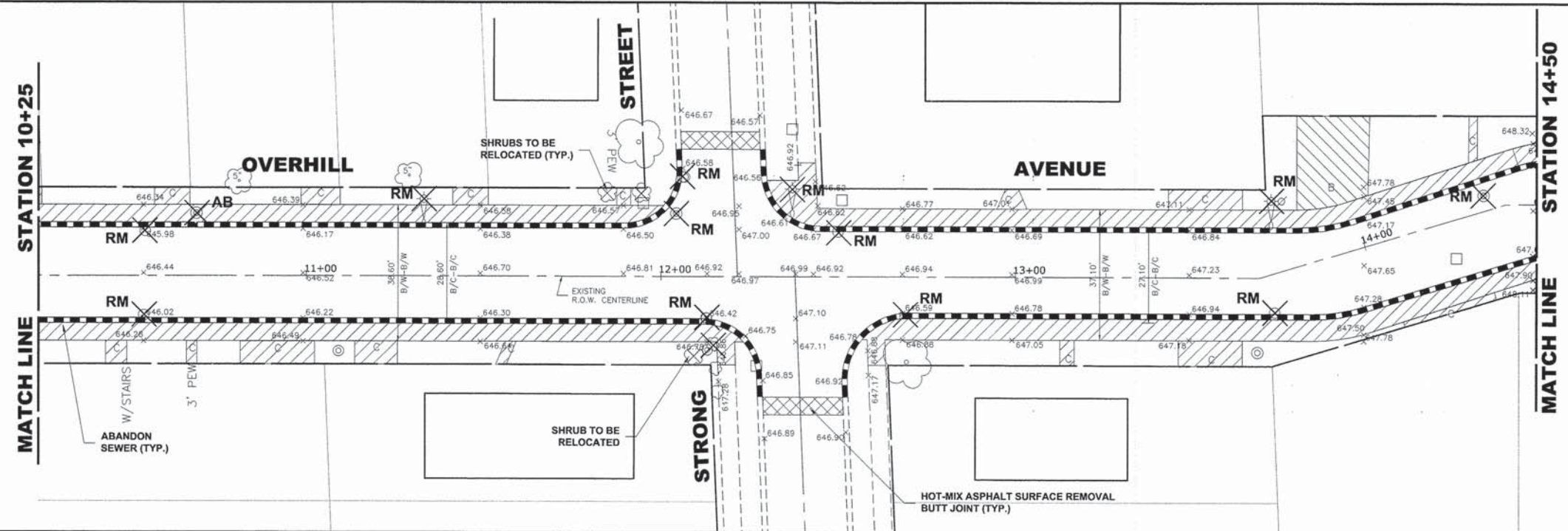
**OVERHILL AVENUE  
 PAVING PLAN AND PROFILE**

SCALE: H:1"=20',V:1"=4'    SHEET NO. 2 OF 6 SHEETS    STA. 5+75    TO STA. 10+25

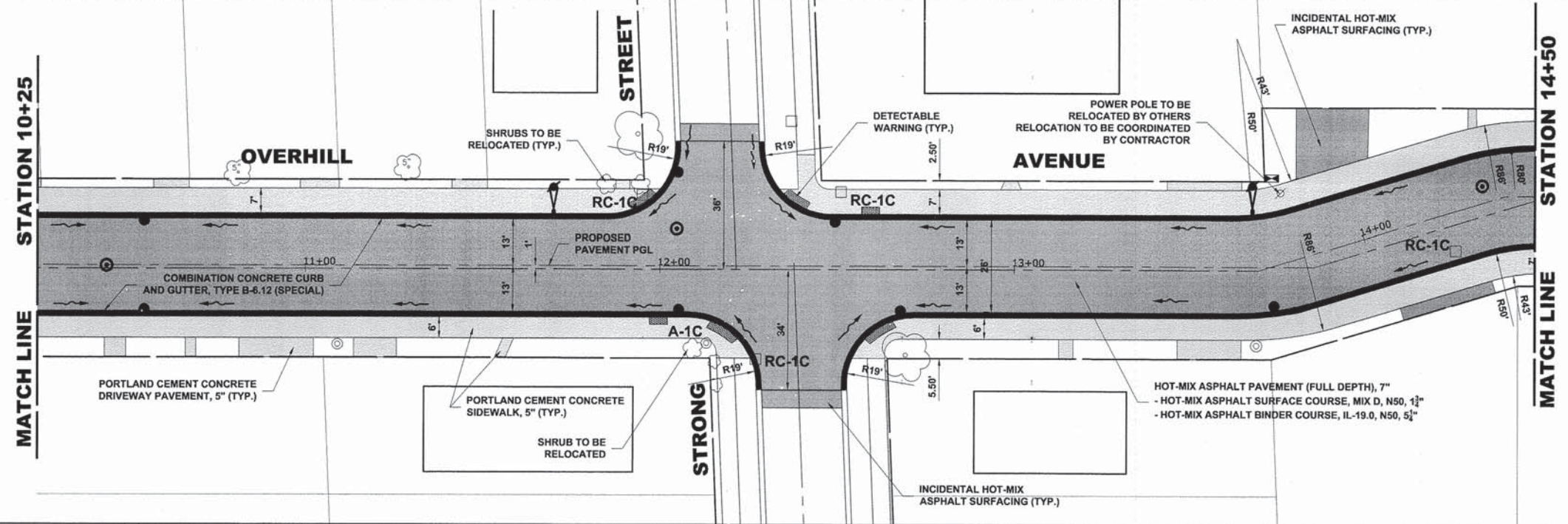
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	12
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

E.H.E. PROJECT NO. 612-13-23201

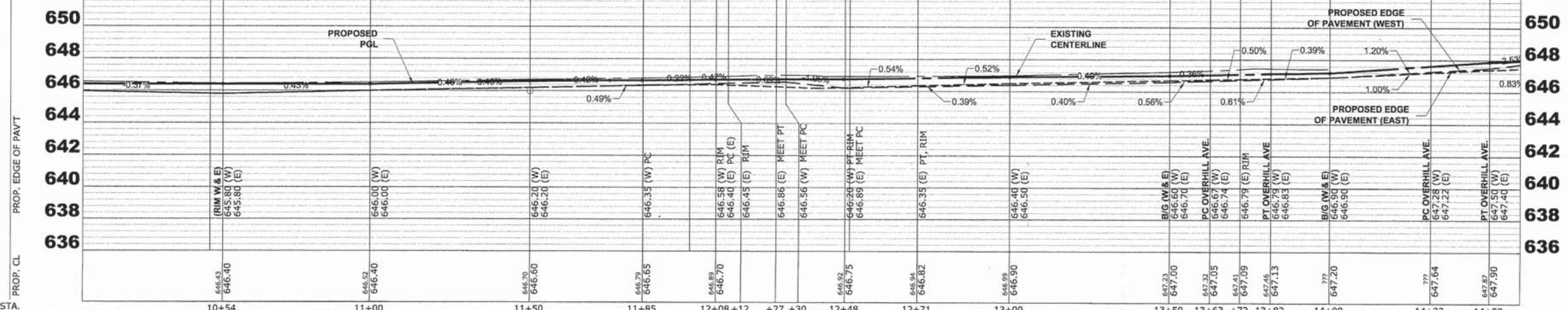
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**EXISTING TOPOGRAPHY**



**PROPOSED IMPROVEMENT**



\* NOTE:  
CURB HEIGHT TO BE 4" UNLESS OTHERWISE INDICATED

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PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

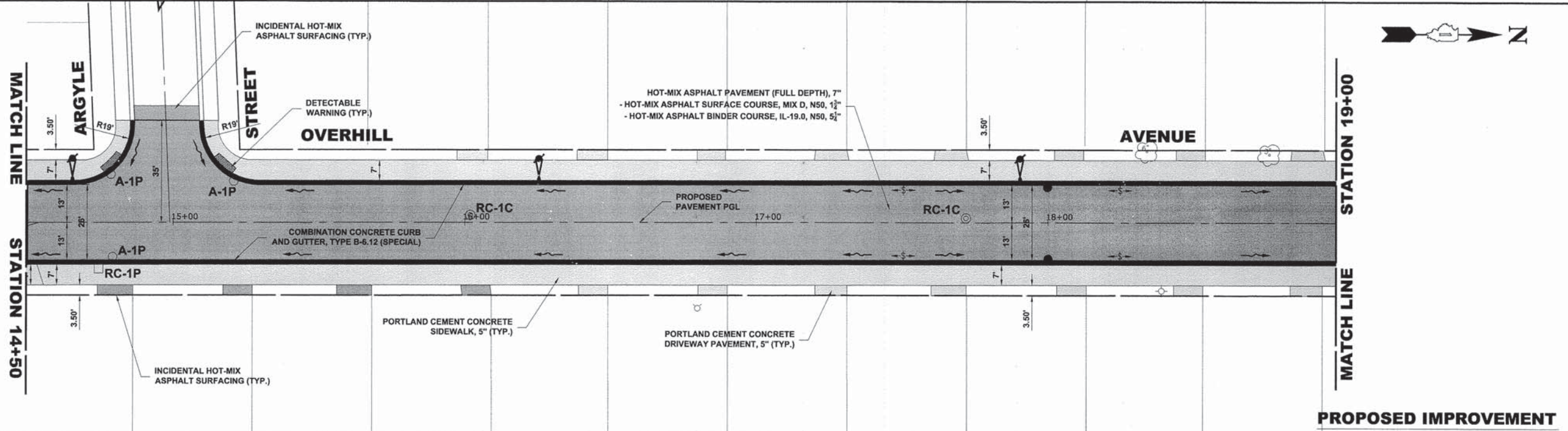
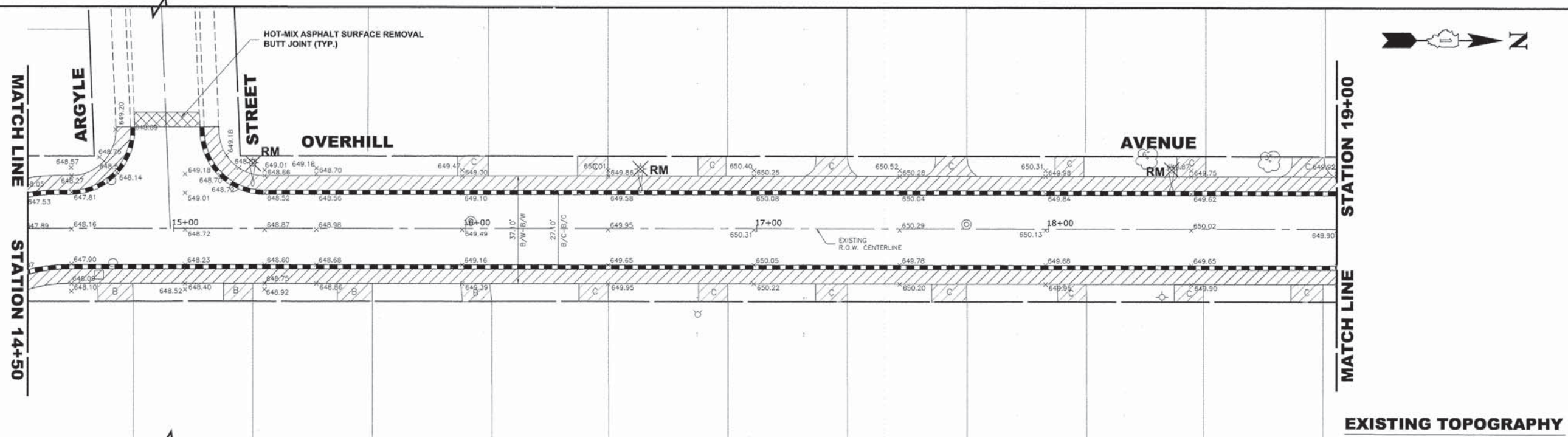
**OVERHILL AVENUE  
PAVING PLAN AND PROFILE**

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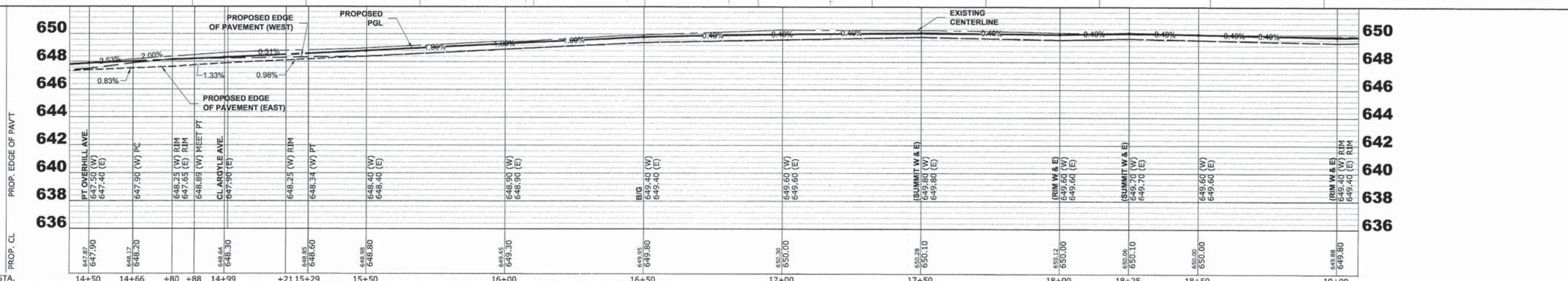
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-	13-00064-00-FP	COOK	62	13
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

E.H.E. PROJECT NO. 612-13-23201

Drawing file: W:\Projects\by\_village\Norridge\61213232 - Overhill Ave Ph II\Site\Overhill Ave\_Ph\_II.dwg Oct 17, 2014 - 2:27pm



\* NOTE:  
CURB HEIGHT TO BE 4" UNLESS OTHERWISE INDICATED



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PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

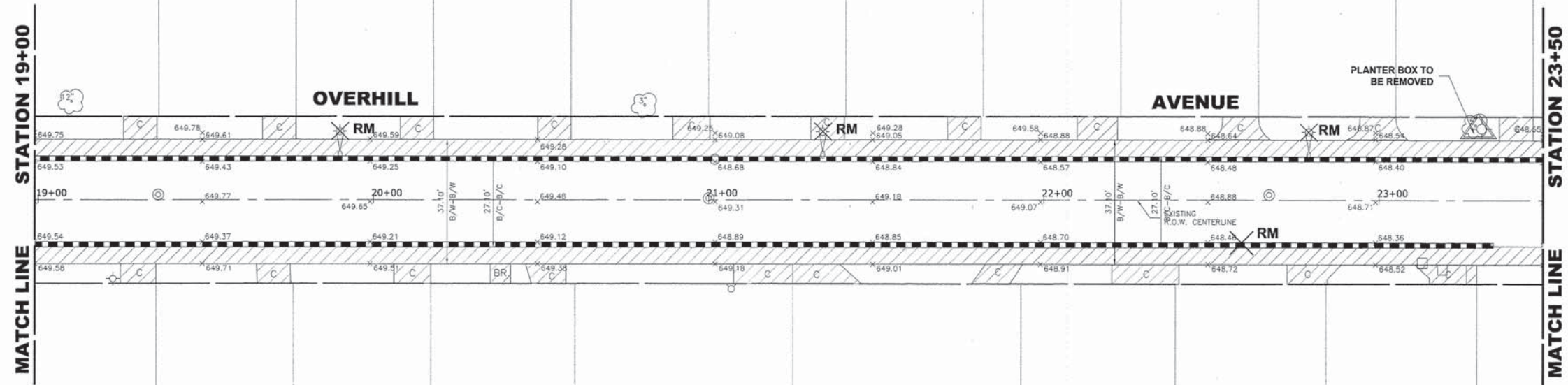
**OVERHILL AVENUE  
PAVING PLAN AND PROFILE**

SCALE: H:1"=20',V:1"=4'    SHEET NO. 4 OF 6 SHEETS    STA. 14+50 TO STA. 19+00

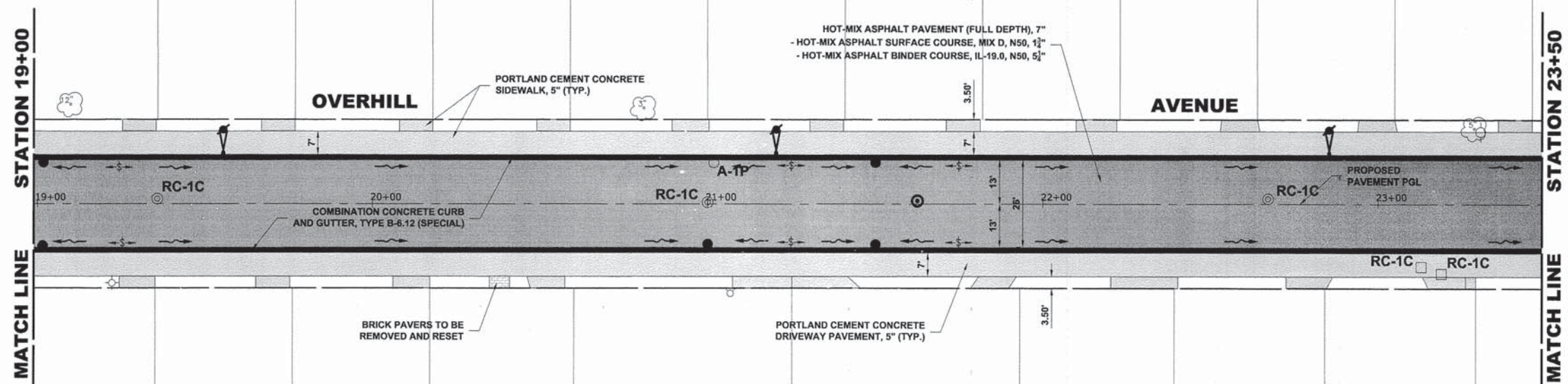
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	14
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

I.H.E. PROJECT NO. 612-13-23201

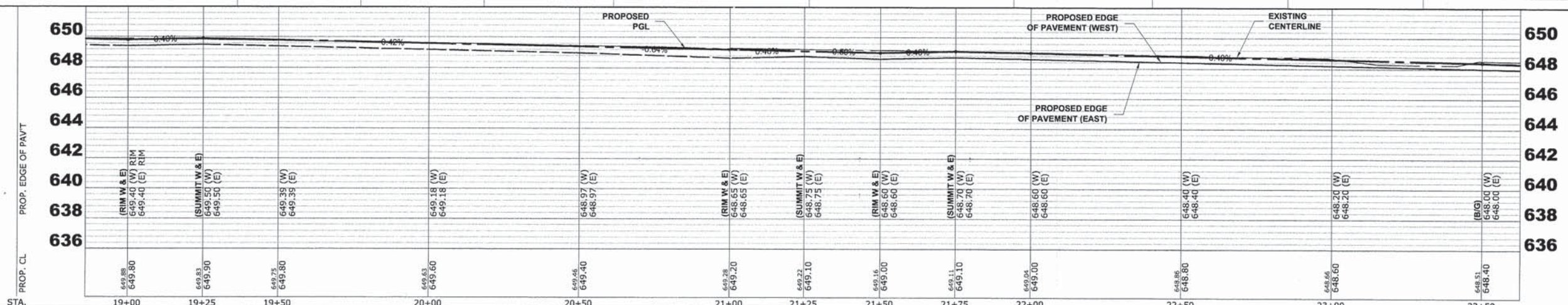
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**EXISTING TOPOGRAPHY**



**PROPOSED IMPROVEMENT**



\* NOTE:  
CURB HEIGHT TO BE 4" UNLESS OTHERWISE INDICATED

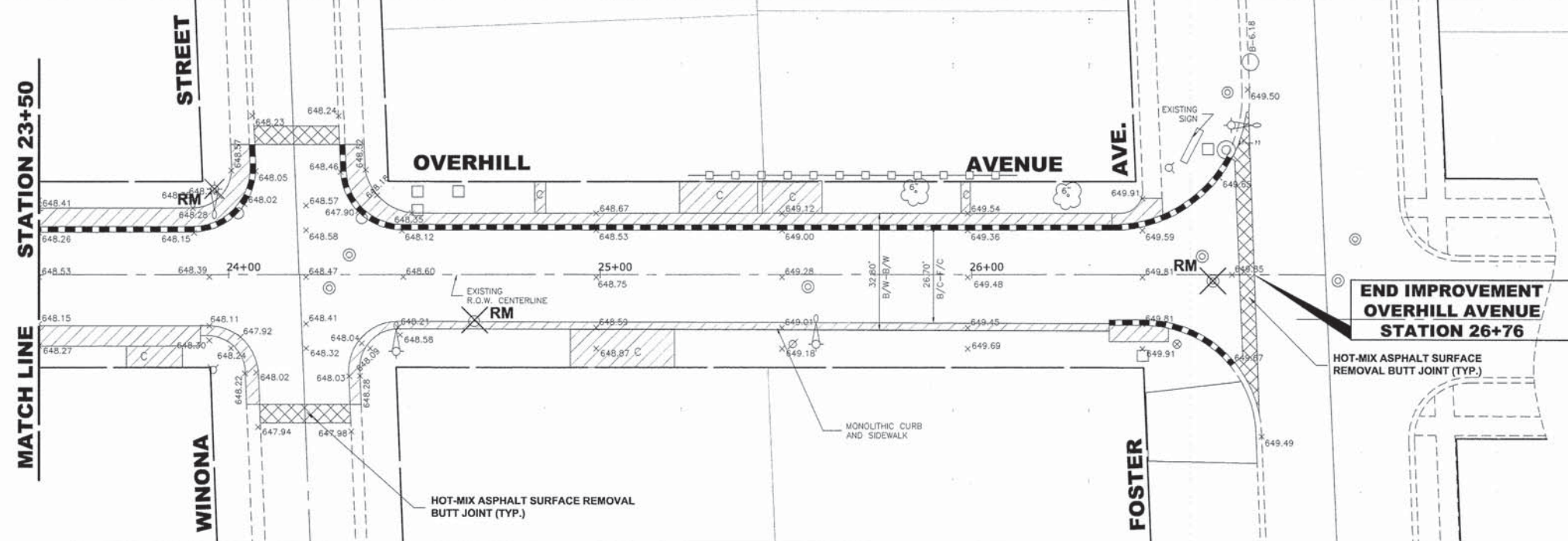
USER NAME -	DESIGNED - SBC/CB	REVISED - IDOT REVIEW
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

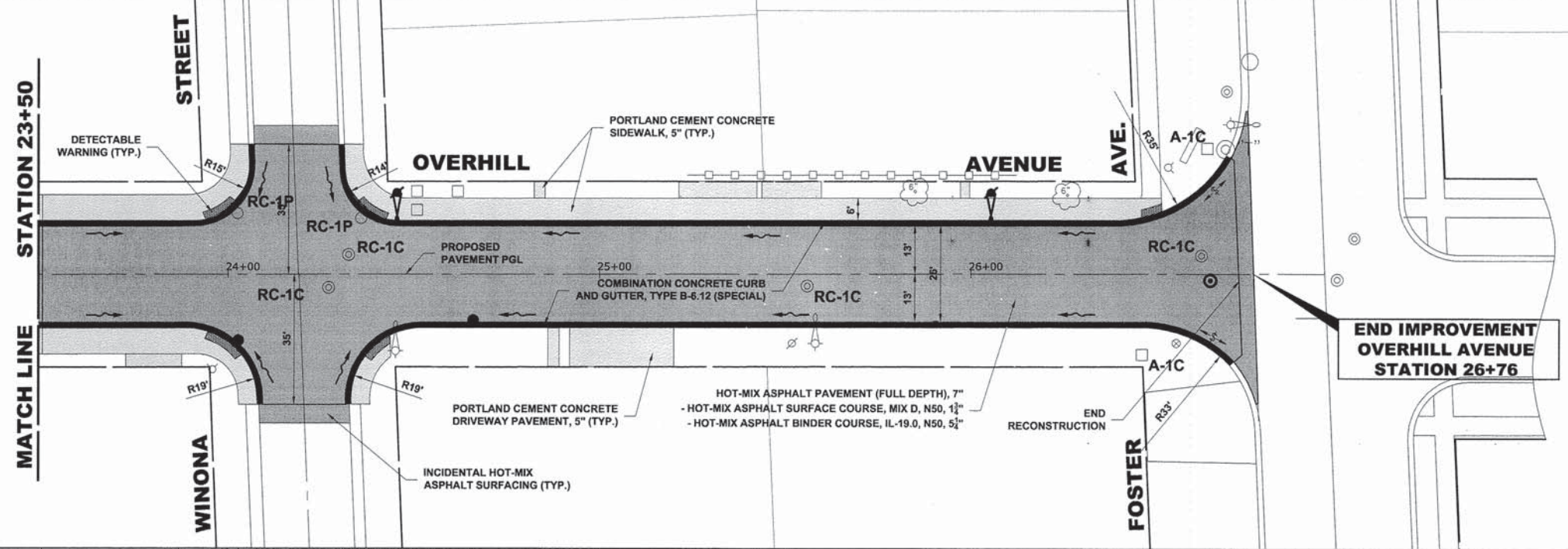
**OVERHILL AVENUE  
PAVING PLAN AND PROFILE**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	15
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

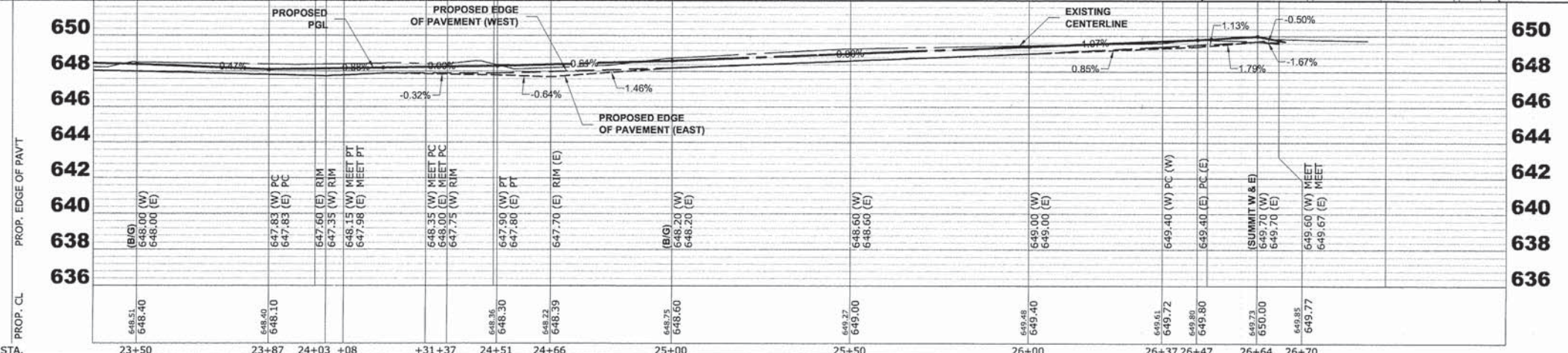
SCALE: H:1"=20', V:1"=4' SHEET NO. 5 OF 6 SHEETS STA. 19+00 TO STA. 23+50



EXISTING TOPOGRAPHY



PROPOSED IMPROVEMENT



\* NOTE:  
CURB HEIGHT TO BE 4" UNLESS OTHERWISE INDICATED

STA.	PROP. CL.	PROP. EDGE OF PAV'T
650		
648		
646		
644		
642		
640		
638		
636		

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**HANCOCK ENGINEERING**  
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USER NAME -	DESIGNED - SBC/CB	REVISED - IDOT REVIEW
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

**OVERHILL AVENUE  
PAVING PLAN AND PROFILE**  
SCALE: H:1"=20',V:1"=4'  
SHEET NO. 6 OF 6 SHEETS  
STA. 23+50 TO STA. 28+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	16
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

E.H.E. PROJECT NO. 612-13-23201



**GENERAL NOTES**

**DEPRESSED CURBS** - THE TOP OF CURBS SHALL BE DEPRESSED WHERE THE CURB AND GUTTER IS CONSTRUCTED AT HANDICAP ACCESSIBLE SIDEWALK RAMP, AT ALLEY RETURNS AND STREET INTERSECTIONS, AND FOR PRIVATE AND COMMERCIAL DRIVES AND AS DIRECTED BY THE ENGINEER

**DRAINAGE OPENINGS** - AT ALL LOCATIONS WHERE CASTINGS ARE TO BE INCORPORATED IN THE CURB AND GUTTER, A 3/4" EXPANSION JOINT SHALL BE INSTALLED IN THE CURB AND GUTTER A DISTANCE OF 5 FT. FROM EACH SIDE OF THE CASTING. 2-NO. 4 RE-BARS, 9' IN LENGTH, SHALL BE INCORPORATED IN THE CONTINUOUS PORTION OF CONCRETE CURB BEHIND THE CASTING.

**DETECTABLE WARNINGS** - DETECTABLE WARNINGS SHALL BE INSTALLED AT HANDICAP ACCESSIBLE SIDEWALK RAMP, AT ALLEY RETURNS, AND STREET INTERSECTIONS. THESE SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE APPLICABLE IDOT STANDARDS.

**SLIPFORM CONSTRUCTION** - VERTICAL FACES MAY BE BATTERED AT THE RATE OF 3/4" PER FOOT OF HEIGHT TO AID IN SLIPFORM OPERATIONS. THE PROPOSED CURB HEIGHT IS VARIABLE, BUT WILL TYPICALLY BE 4"

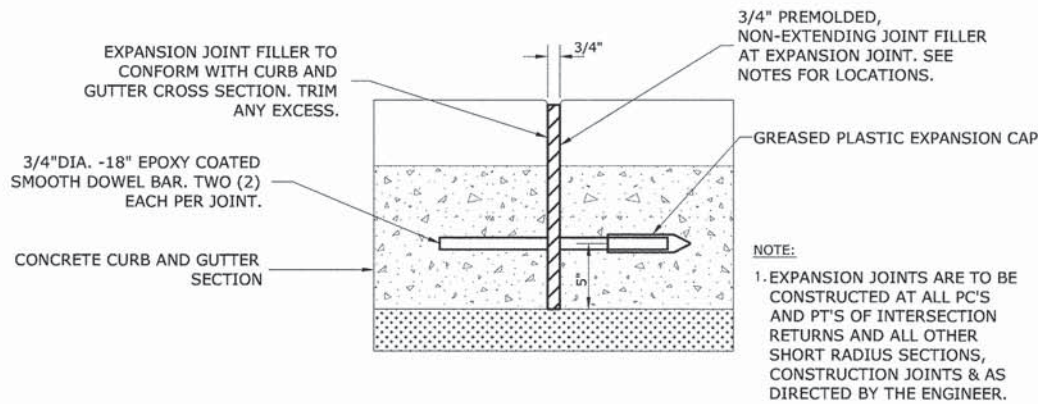
**DEPRESSED CURB HEIGHT** - THE HEIGHT OF THE DEPRESSED CURB SHALL BE 1-1/2" AT DRIVEWAYS. SEE IDOT STANDARD 424001-05 FOR HEIGHT AT SIDEWALK RAMP.

**BITUMINOUS EXPANSION JOINTS** - THREE QUARTER INCH (3/4") BITUMINOUS PREMOLDED INORGANIC FIBER EXPANSION JOINTS SHALL BE INSTALLED WHERE NEW SIDEWALK OR CURB AND GUTTER OR DRIVEWAY PAVEMENT ABUTS AN EXISTING CONCRETE WALK, DRIVE, OR CURB WHICH IS TO REMAIN IN PLACE, AND AT NOT LESS THAN NINETY FOOT (90') INTERVALS AT LOCATIONS WHERE CURB REPLACEMENT IS IN EXCESS OF NINETY FEET (90'); AT RADIUS POINTS, AT BOTH SIDES OF FRAMES AND GRATES WHICH FALL IN THE CURB; AND AS DIRECTED BY THE ENGINEER.

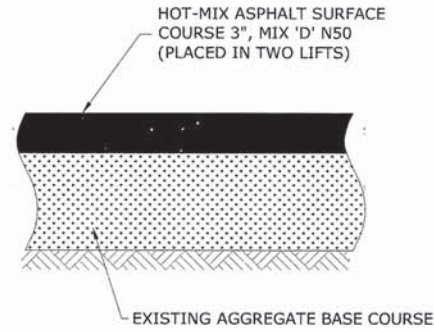
ALL EXPANSION JOINTS LOCATED IN THE CURB AND GUTTER SHALL HAVE TWO (2) THREE QUARTER INCH (3/4") DIAMETER, SMOOTH, ROUND, EPOXY COATED DOWEL BARS, EIGHTEEN INCHES (18") IN LENGTH, WITH GREASED PLASTIC END CAPS INSERTED TO ALLOW THE CURB AND GUTTER TO EXPAND AND CONTRACT LATERALLY. CONTRACTION JOINTS SHALL BE TOOLED INTO THE CURB AND GUTTER AT INTERVALS NOT TO EXCEED FIFTEEN FEET (15'). THESE CONTRACTION JOINTS SHALL BE SAW CUT TO A DEPTH OF TWO INCHES (2") WITHIN TWENTY-FOUR (24) HOURS OF CONCRETE PLACEMENT. THE COST OF THE ABOVE WORK SHALL BE INCLUDED IN THE RESPECTIVE ITEMS FOR CONCRETE INSTALLATION.

THE COSTS FOR REMOVAL OF ANY ASPHALT OVERLAY THAT EXTENDS INTO THE GUTTER PORTION OF THE CURB AND GUTTER WILL BE INCLUDED IN THE PRICE FOR COMBINATION CURB AND GUTTER REMOVAL.

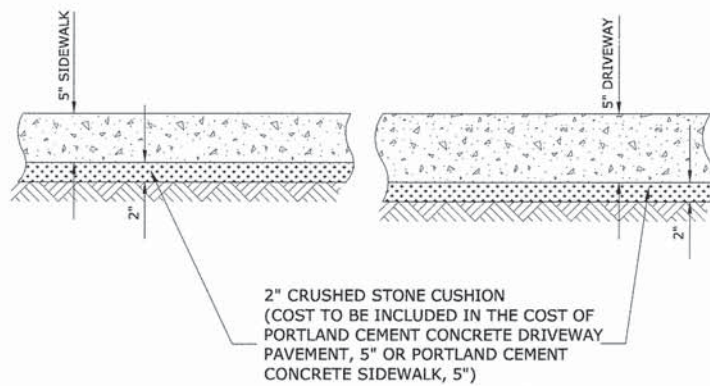
THE EDWIN HANCOCK ENGINEERING COMPANY AND THE VILLAGE PUBLIC WORKS DEPARTMENT SHALL BE NOTIFIED AT LEAST TWO (2) WORKING DAYS PRIOR TO ANY CONSTRUCTION ACTIVITY.



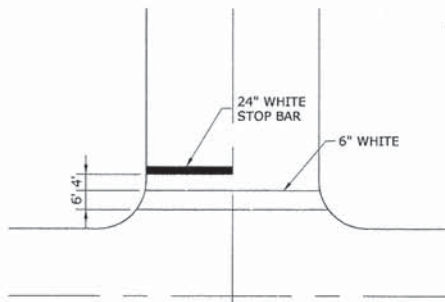
**TYPICAL CURB AND GUTTER EXPANSION JOINT**



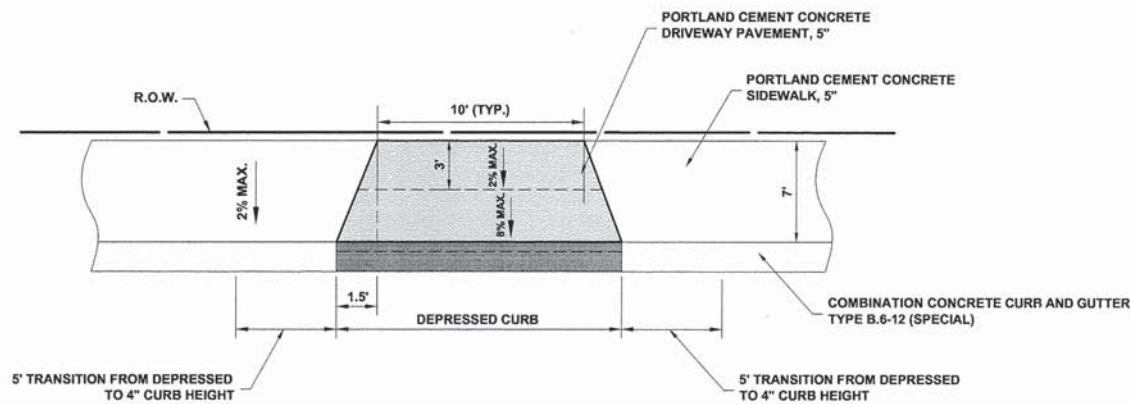
**INCIDENTAL HOT-MIX ASPHALT SURFACING (TYPICAL ASPHALT DRIVEWAY)**



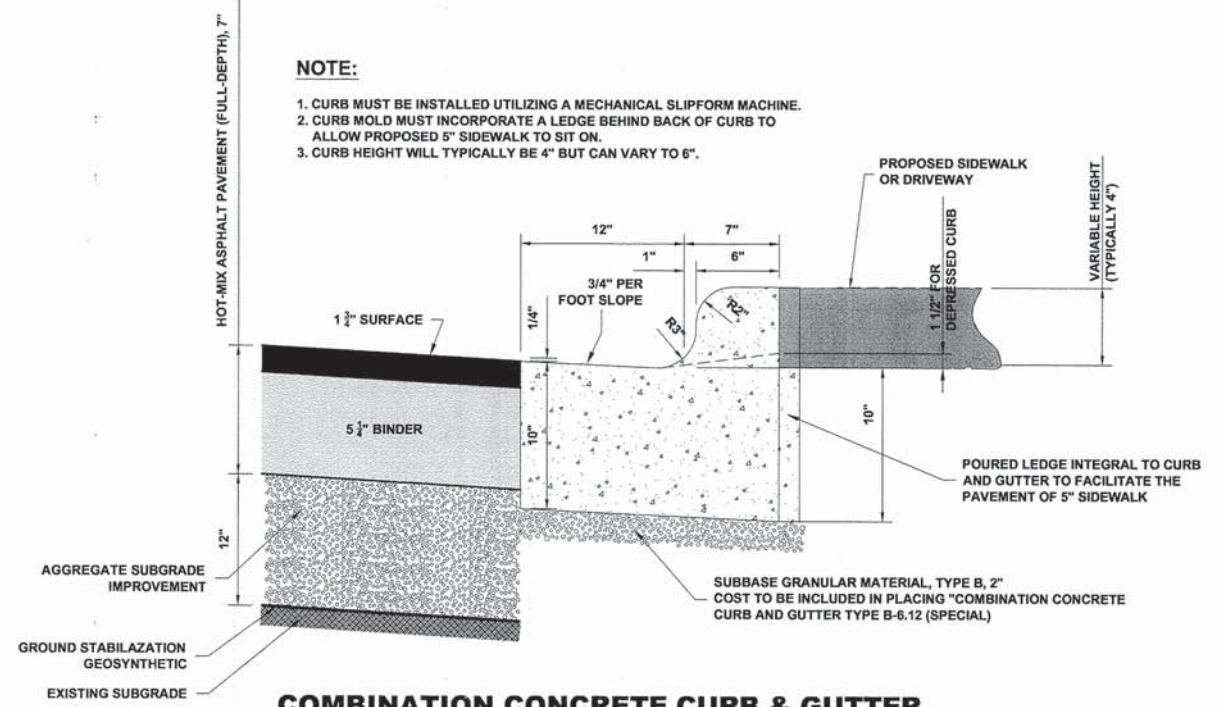
**TYPICAL P.C.C. SIDEWALK, DRIVEWAY**



**TYPICAL CROSSWALK & STOP BAR DETAIL**



**DRIVEWAY DETAIL**



**COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12 (SPECIAL)**

**NOTE:**

1. CURB MUST BE INSTALLED UTILIZING A MECHANICAL SLIPFORM MACHINE.
2. CURB MOLD MUST INCORPORATE A LEDGE BEHIND BACK OF CURB TO ALLOW PROPOSED 5" SIDEWALK TO SIT ON.
3. CURB HEIGHT WILL TYPICALLY BE 4" BUT CAN VARY TO 6".

Drawing file: W:\Projects\_2014\Projects\_Village\Norridge\61213232 - Overhill Ave Ph. II\17\_Roadway-Details.dwg Oct. 02, 2014 - 4:17pm



USER NAME -	DESIGNED - SBC/CB	REVISED -
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

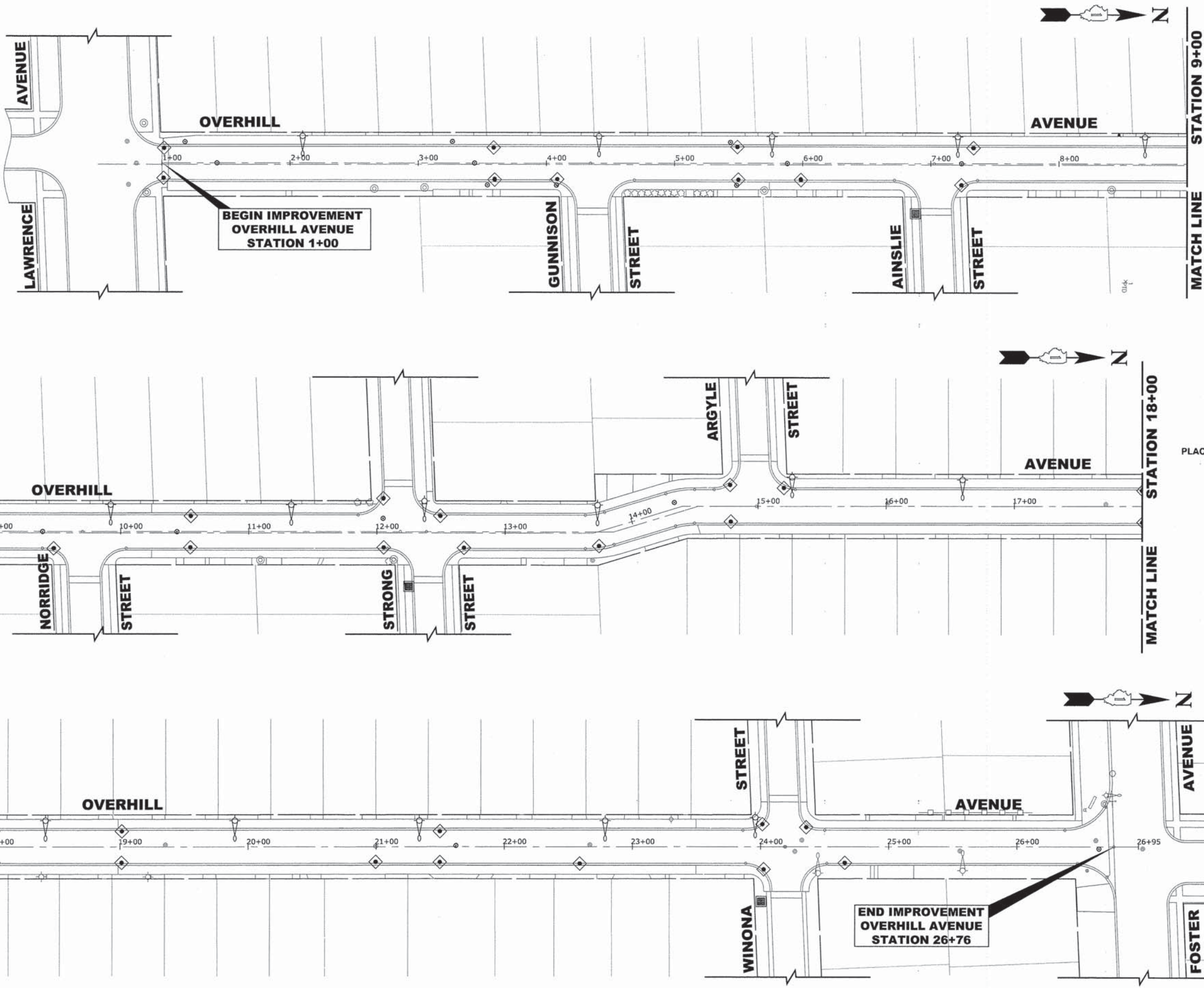
**OVERHILL AVENUE RECONSTRUCTION VILLAGE OF NORRIDGE, ILLINOIS**

**ROADWAY DETAILS**

SCALE: NONE SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	17
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

E.H.E. PROJECT NO. 612-13-23201

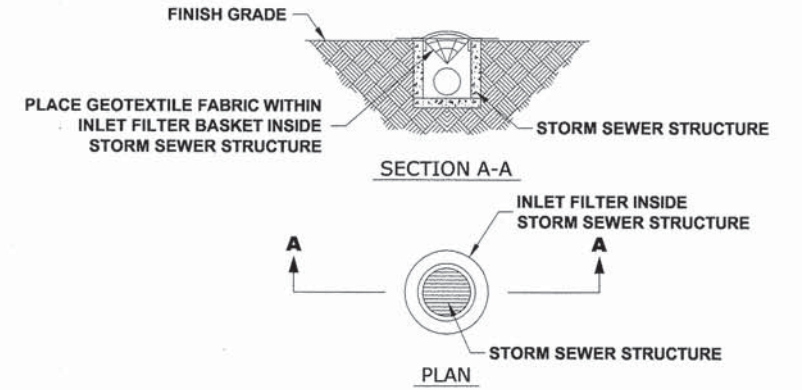


**LEGEND**

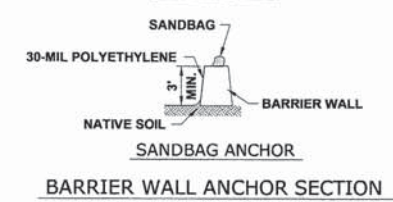
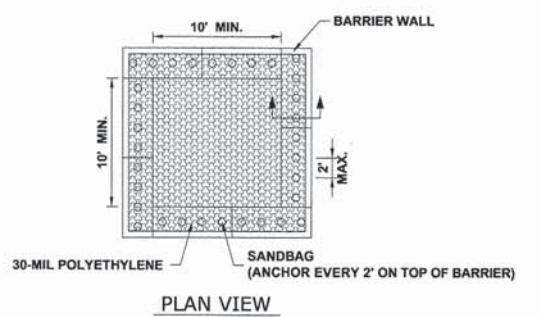
SYMBOL	DESCRIPTION
	INLET FILTER/SEDIMENT CONTROL, DRAINAGE STRUCTURE, INLET FILTER CLEANING
	CONCRETE WASHOUT

**NOTES**

- SEE IDOT STANDARD 280001-07 FOR TEMPORARY EROSION CONTROL SYSTEMS.
- THE CONTRACTOR SHALL VERIFY THAT ADJACENT PROPERTIES REMAIN PROTECTED FROM SEDIMENT DEPOSITION.
- SOIL STOCKPILES SHALL BE PROTECTED WITH PERIMETER EROSION BARRIER OR OTHER EROSION PROTECTION SPECIFIED BY THE RESIDENT ENGINEER. THE COST SHALL BE INCLUDED IN THE UNIT PRICE FOR THE INDIVIDUAL SOIL MATERIALS.
- WHEREVER CONSTRUCTION VEHICLE ACCESS ROUTES INTERSECT PAVED PUBLIC ROADS PROVISIONS SHALL BE MADE TO MINIMIZE THE TRANSPORT OF SEDIMENT BY RUNOFF OR VEHICLE TRACKING ONTO THE PAVED SURFACE. THE PROVISIONS MAY INCLUDE SPRAYING VEHICLE WHEELS WITH WATER TO CLEAR SEDIMENT BEFORE EXITING THE CONSTRUCTION SITE OR OTHER MEASURES APPROVED BY THE ENGINEER. THE COST SHALL BE INCLUDED IN THE CONTRACT.
- THE CONTRACTOR SHALL UTILIZE A SPECIFIC CONCRETE WASH OUT AREA LOCATED AWAY FROM DRAINAGE STRUCTURES, AT A FREQUENCY OF NO MORE THAN 1 X PER 1000' LOCATION TO BE DETERMINED IN THE FIELD.
- INLET FILTERS TO BE INSPECTED AND CLEANED WEEKLY AND AFTER EVERY 0.5" RAIN.



**INLET FILTER**



**NOTES**

- MAINTAINING TEMPORARY CONCRETE FACILITIES SHALL INCLUDE REMOVING AND DISPOSING OF HARDEN CONCRETE AND/OR SLURRY AND RETURNING THE FACILITIES TO A FUNCTIONAL CONDITION.
- FACILITY SHALL BE CLEANED OR RECONSTRUCTED IN A NEW AREA ONCE WASHOUT BECOMES TWO-THIRDS FULL.

**CONCRETE WASHOUT**

Drawing File: W:\Projects\by\_village\Norrige\61213232 - Overhill Ave Ph II\35-41\_Overhill\_40scale.dwg Oct 02, 2014 - 4:30pm

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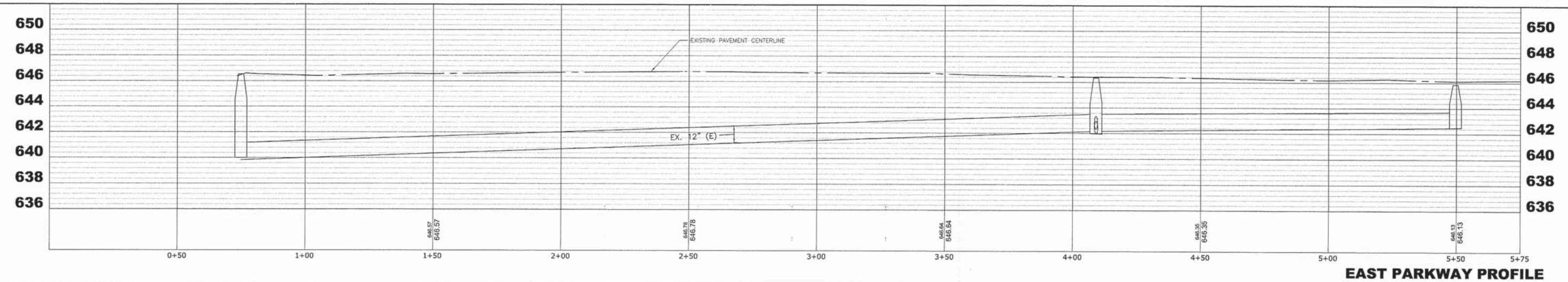
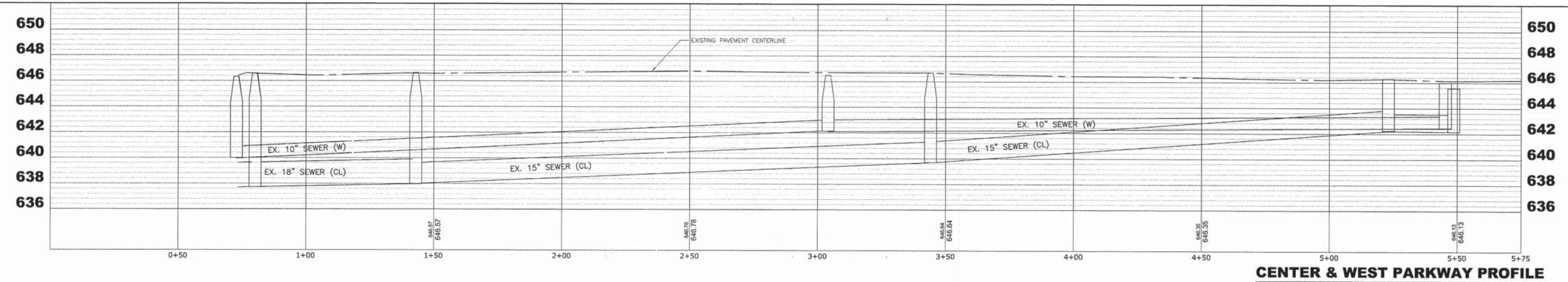
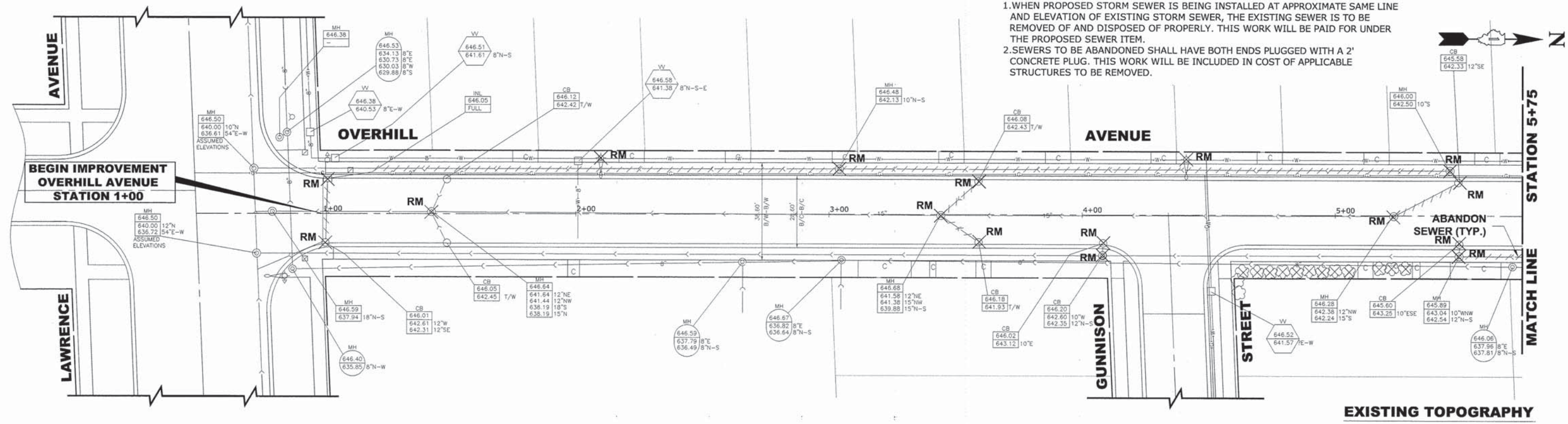
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PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

<b>EROSION CONTROL PLAN</b>	
SCALE: 1" = 40'	SHEET NO. 1 OF 1 SHEETS
STA. 1+00	TO STA. 26+88

F.A.U. RTE. -	SECTION 13-00064-00-FP	COUNTY COOK	TOTAL SHEETS 62	SHEET NO. 18
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

**NOTE:**  
 1. WHEN PROPOSED STORM SEWER IS BEING INSTALLED AT APPROXIMATE SAME LINE AND ELEVATION OF EXISTING STORM SEWER, THE EXISTING SEWER IS TO BE REMOVED OF AND DISPOSED OF PROPERLY. THIS WORK WILL BE PAID FOR UNDER THE PROPOSED SEWER ITEM.  
 2. SEWERS TO BE ABANDONED SHALL HAVE BOTH ENDS PLUGGED WITH A 2' CONCRETE PLUG. THIS WORK WILL BE INCLUDED IN COST OF APPLICABLE STRUCTURES TO BE REMOVED.



Drawing file: W:\Projects\by\_village\Norridge\61213232 - Overhill Ave Ph II\Site\Overhill Ave\_Ph II.dwg Oct 17, 2014 - 2:25pm

**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants  
 Established 1911

USER NAME -	DESIGNED - SBC/CB	REVISED - IDOT REVIEW
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

**OVERHILL AVENUE  
 EXISTING UTILITIES PLAN**

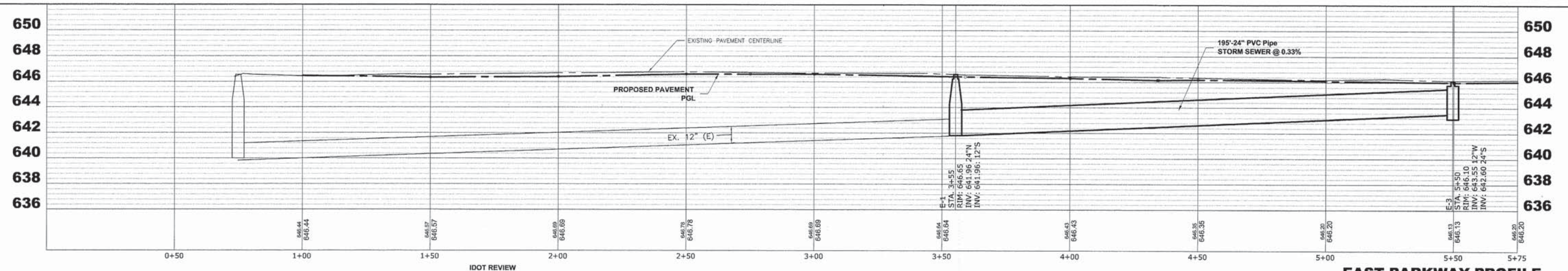
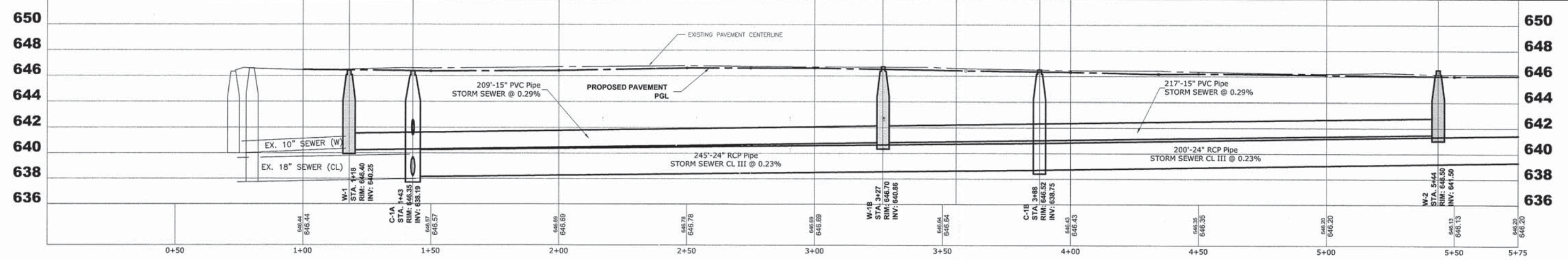
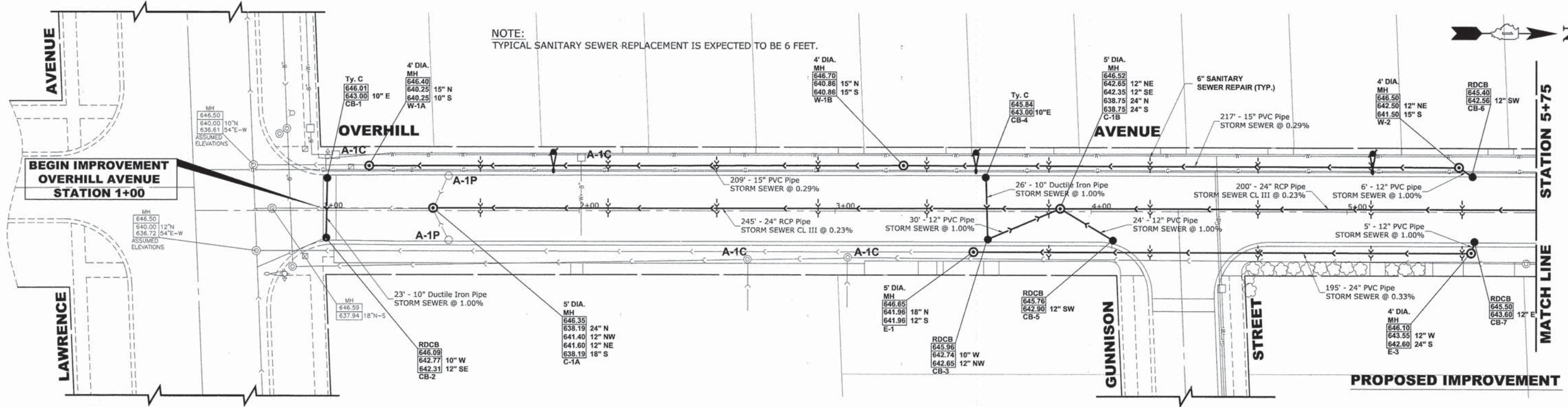
SCALE: 1"=20'      SHEET NO. 1 OF 12 SHEETS      STA. 0+00      TO STA. 5+75

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	19
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

E.H.E. PROJECT NO. 612-13-23201



NOTE:  
TYPICAL SANITARY SEWER REPLACEMENT IS EXPECTED TO BE 6 FEET.



**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants  
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PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

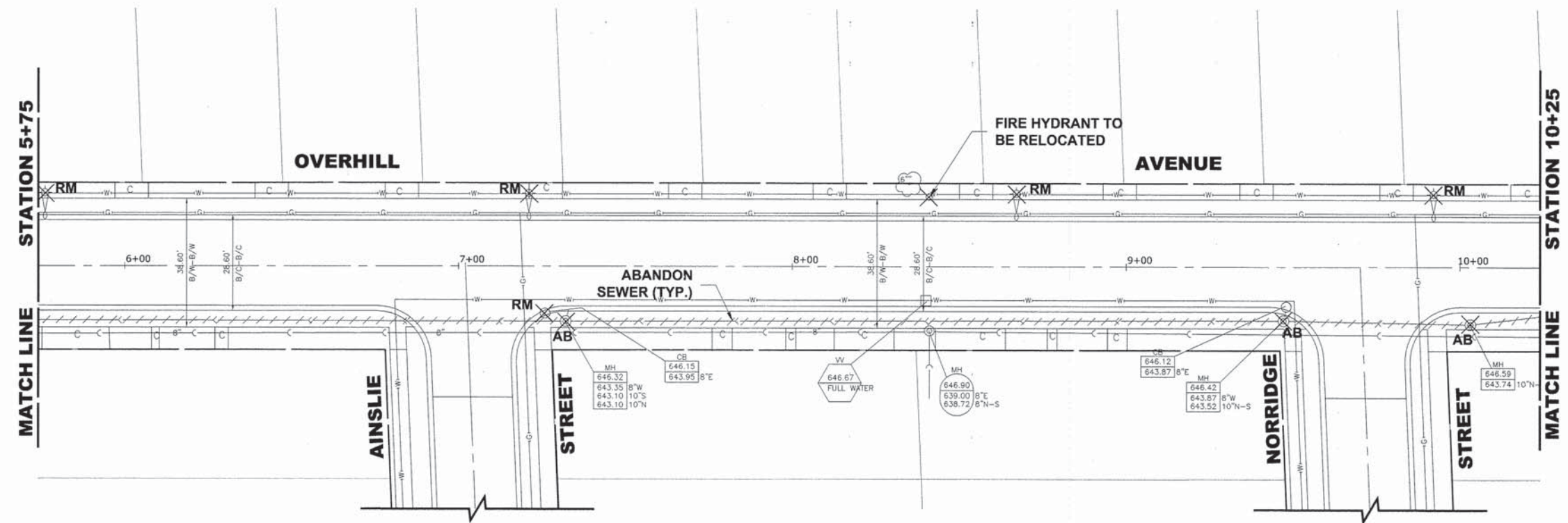
**OVERHILL AVENUE  
 PROPOSED UTILITIES PLAN**

SCALE: 1"=20'    SHEET NO. 2 OF 12 SHEETS    STA. 0+00    TO STA. 5+75

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	20
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

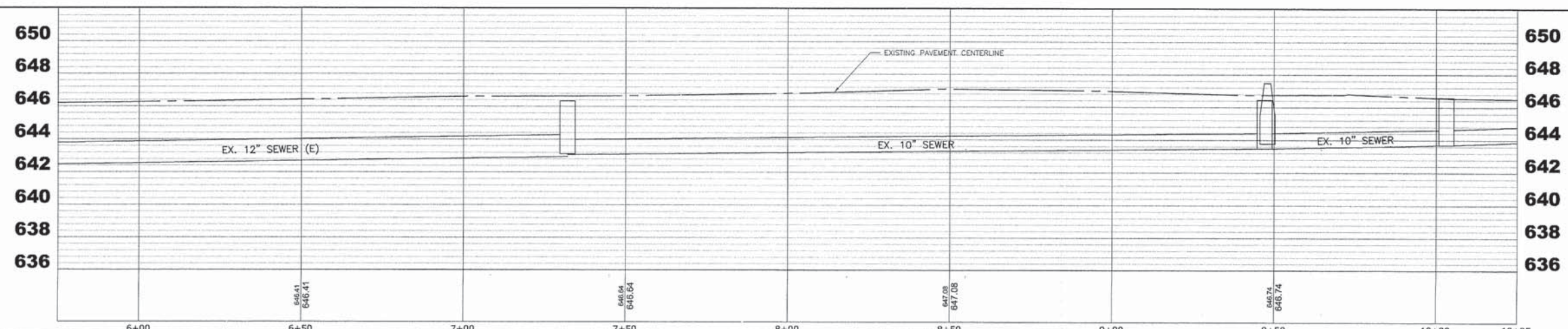
E.H.E. PROJECT NO. 612-13-23201

Drawing file: W:\Projects\by\_village\Norridge\61213232 - Overhill Ave Ph II\Site\Overhill Ave\_Ph II.dwg    Oct 17, 2014 - 2:29pm



**EXISTING TOPOGRAPHY**

**NOTE:**  
 1. WHEN PROPOSED STORM SEWER IS BEING INSTALLED AT APPROXIMATE SAME LINE AND ELEVATION OF EXISTING STORM SEWER, THE EXISTING SEWER IS TO BE REMOVED OF AND DISPOSED OF PROPERLY. THIS WORK WILL BE PAID FOR UNDER THE PROPOSED SEWER ITEM.  
 2. SEWERS TO BE ABANDONED SHALL HAVE BOTH ENDS PLUGGED WITH A 2' CONCRETE PLUG. THIS WORK WILL BE INCLUDED IN COST OF APPLICABLE STRUCTURES TO BE REMOVED.



Drawing File: W:\Projects\by\_Village\Norridge\61213232 - Overhill Ave Ph II\Site\Overhill Ave\_Ph II.dwg Oct 17, 2014 - 2:30pm

**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants  
 Established 1911

USER NAME -	DESIGNED - SBC/CB	REVISED - IDOT REVIEW
	DRAWN - MK,DMM,ECW	REVISED -
PLOT SCALE -	CHECKED - WOP	REVISED -
PLOT DATE -	DATE - 10/02/14	REVISED -

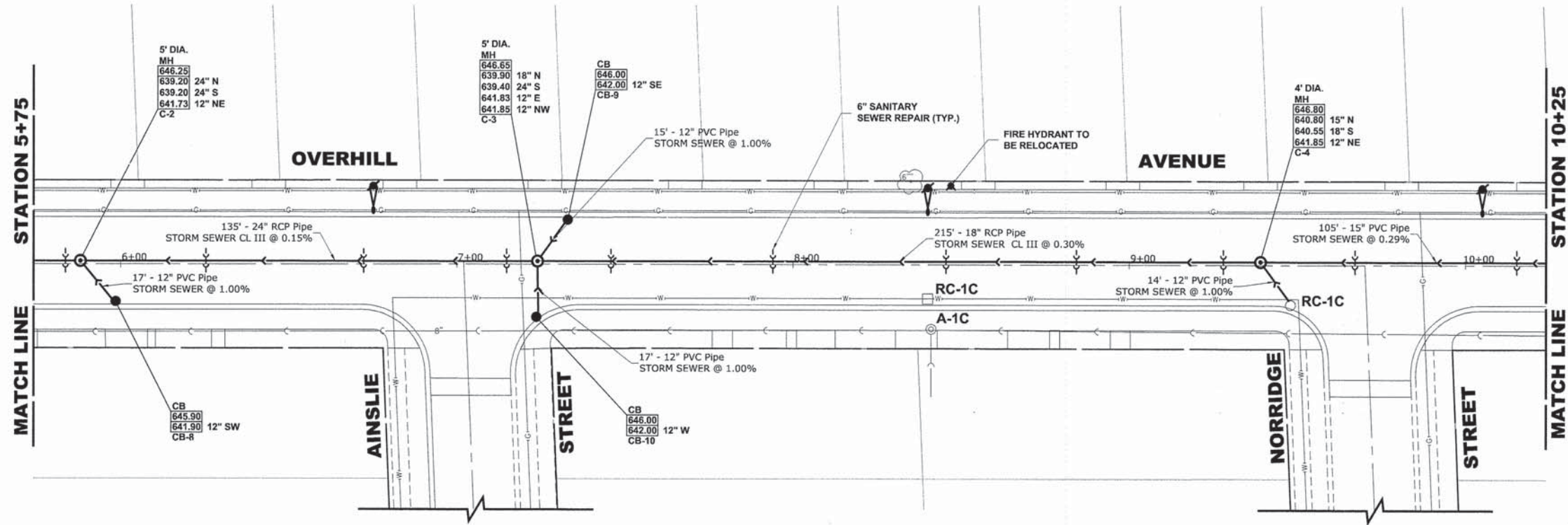
**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

**OVERHILL AVENUE  
 EXISTING UTILITIES PLAN**

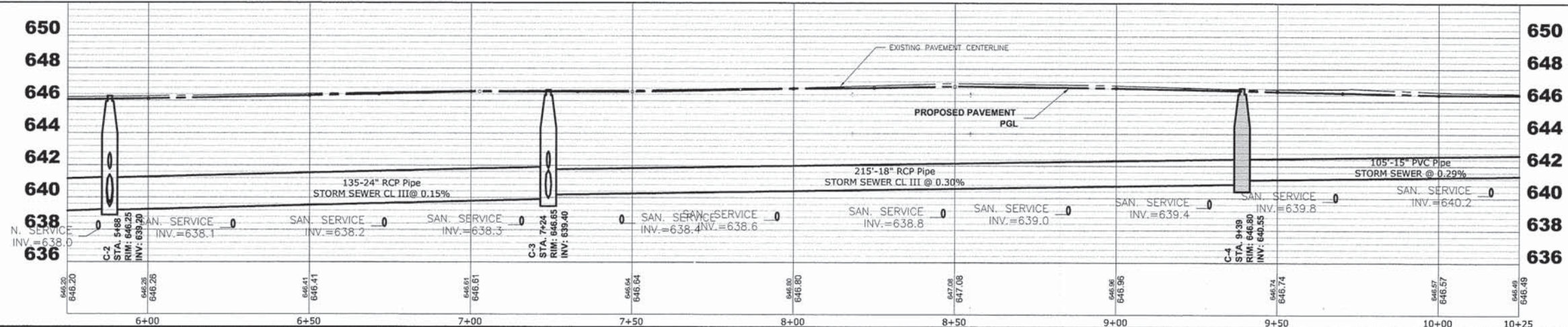
SCALE: 1"=20'      SHEET NO. 3 OF 12 SHEETS      STA. 5+75      TO STA. 10+25

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-0064-00-FP	COOK	62	21
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

NOTE:  
TYPICAL SANITARY SEWER REPLACEMENT IS EXPECTED TO BE 6 FEET.



PROPOSED IMPROVEMENT



Drawing file: W:\Projects\_by\_Village\Norridge\61213232 - Overhill Ave Ph II\Site\Overhill Ave\_Ph II.dwg Oct 17, 2014 - 2:30pm

**HANCOCK ENGINEERING**  
Civil Engineers  
Municipal Consultants  
Established 1911

9121 Riverchase Blvd  
Woodbridge, IL 60096-4790  
Phone: 708.560.0100  
Fax: 708.560.0102

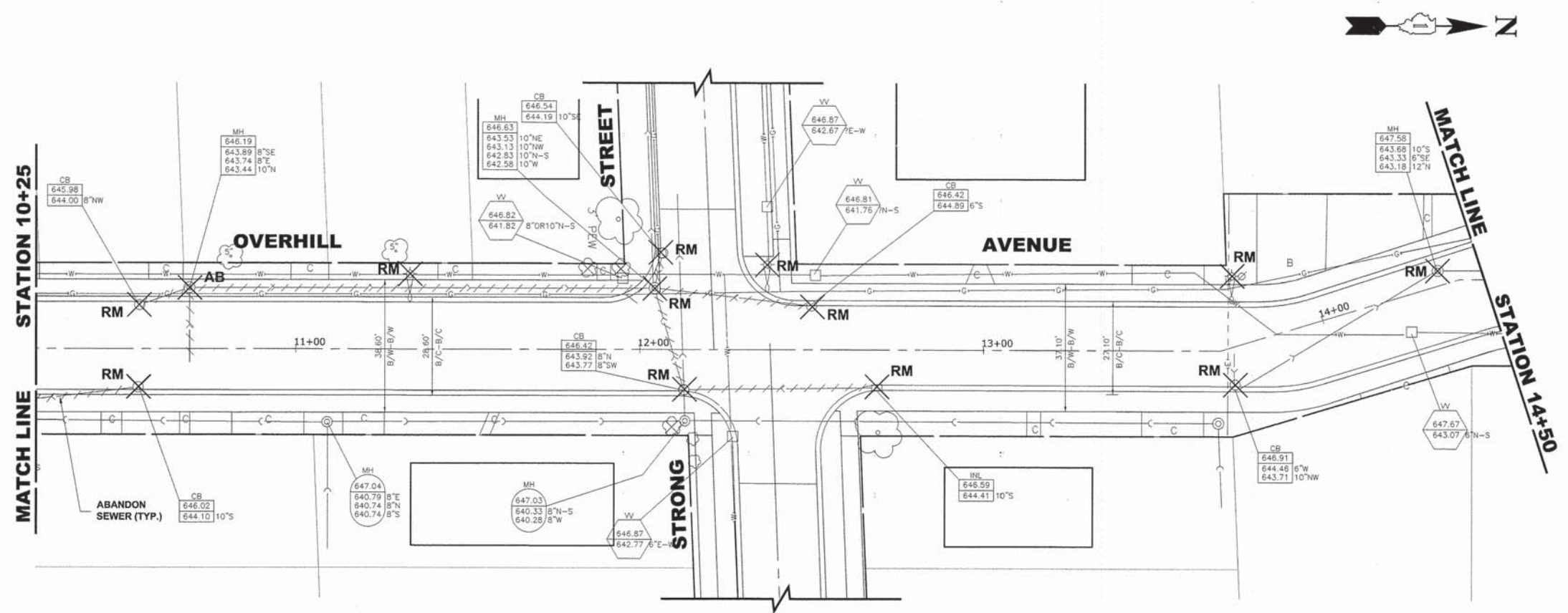
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PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

<b>OVERHILL AVENUE PROPOSED UTILITIES PLAN</b>	
SCALE: 1"=20'	SHEET NO. 2 OF 12 SHEETS
STA. 5+75	TO STA. 10+25

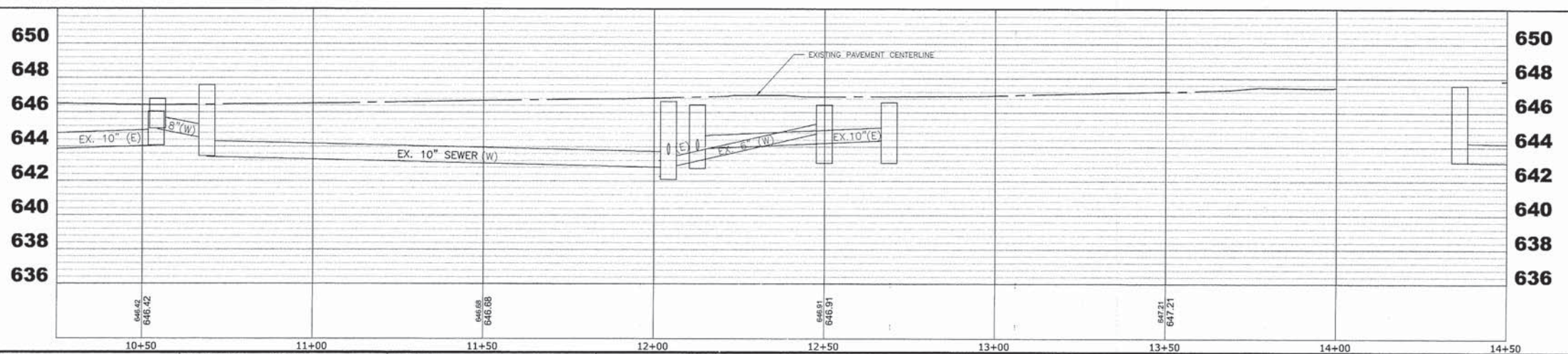
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-	13-00064-00-FP	COOK	62	22
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

Drawing file: W:\Projects\by\_village\Norridge\61213232 - Overhill Ave Ph II\Site\Overhill Ave\_Ph II.dwg Oct 17, 2014 - 2:31pm



**EXISTING TOPOGRAPHY**

**NOTE:**  
 1. WHEN PROPOSED STORM SEWER IS BEING INSTALLED AT APPROXIMATE SAME LINE AND ELEVATION OF EXISTING STORM SEWER, THE EXISTING SEWER IS TO BE REMOVED OF AND DISPOSED OF PROPERLY. THIS WORK WILL BE PAID FOR UNDER THE PROPOSED SEWER ITEM.  
 2. SEWERS TO BE ABANDONED SHALL HAVE BOTH ENDS PLUGGED WITH A 2' CONCRETE PLUG. THIS WORK WILL BE INCLUDED IN COST OF APPLICABLE STRUCTURES TO BE REMOVED.



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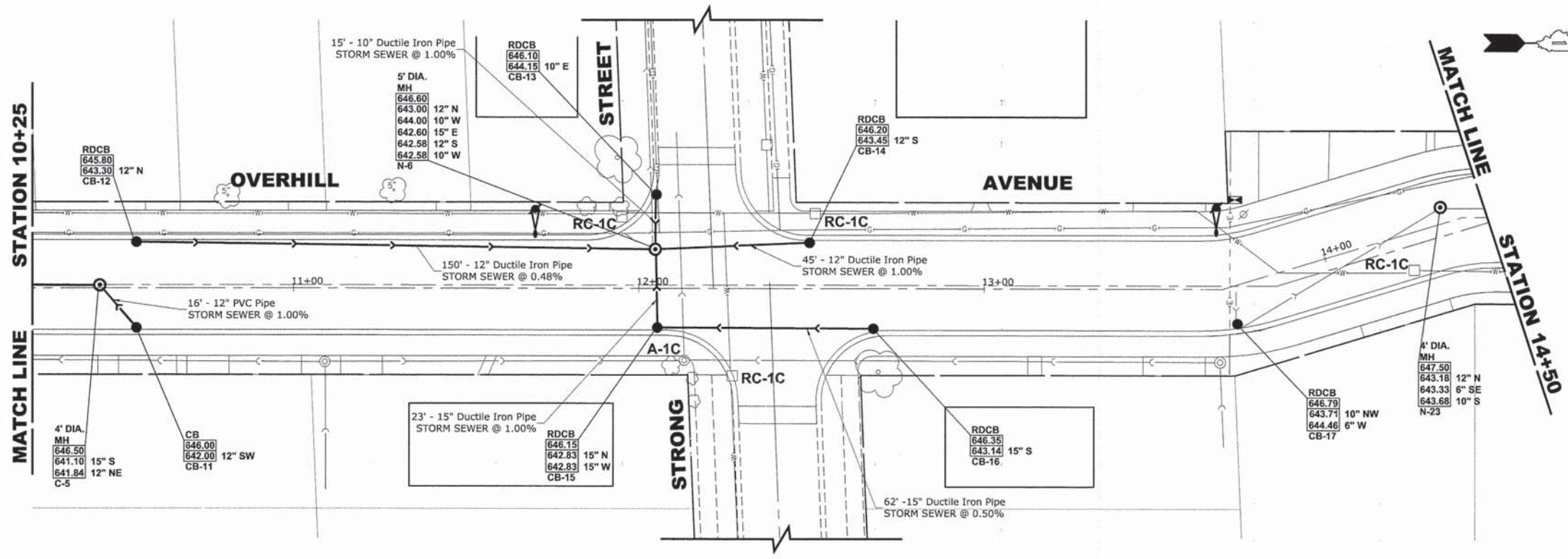
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PLOT DATE -	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

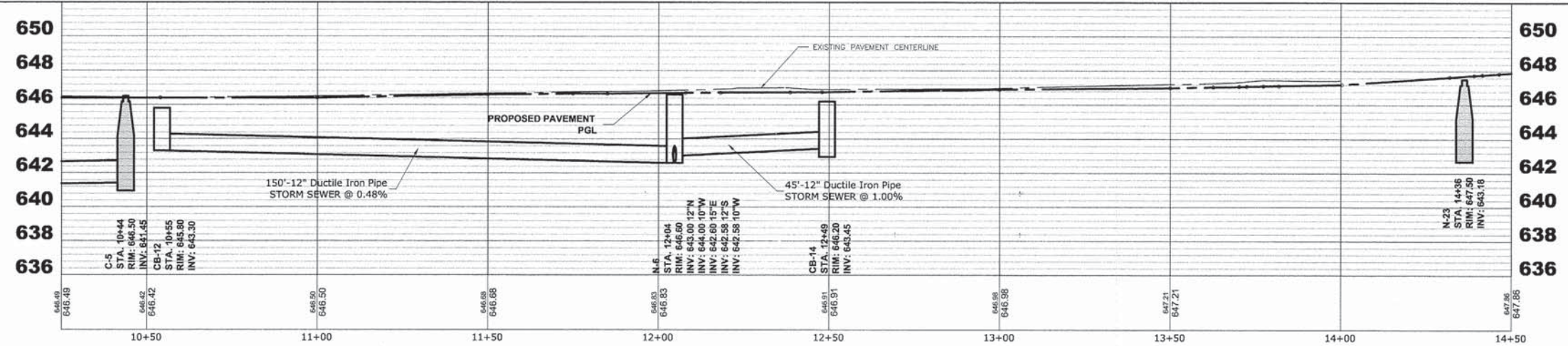
<b>OVERHILL AVENUE EXISTING UTILITIES PLAN</b>	
SCALE: 1"=20'	SHEET NO. 5 OF 12 SHEETS
STA. 10+25	TO STA. 14+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	23
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

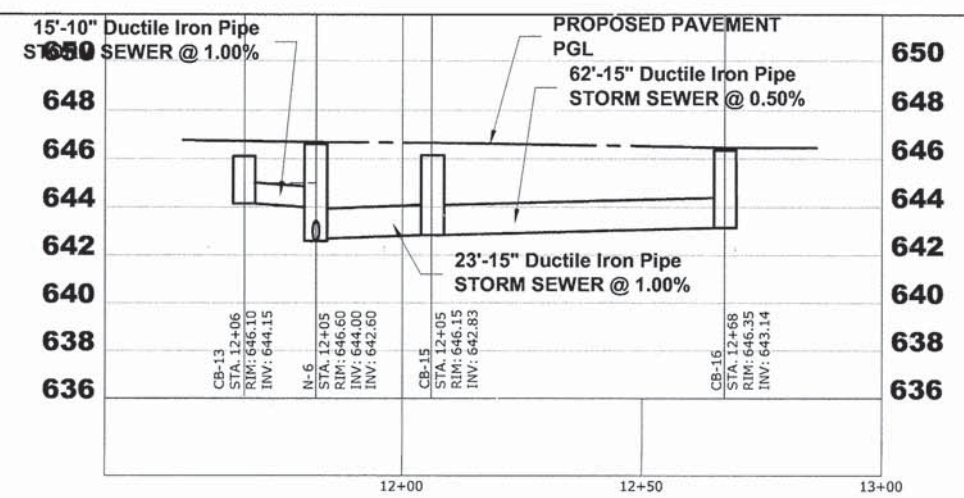
E.H.E. PROJECT NO. 612-13-23201



**PROPOSED IMPROVEMENT**



**CENTER & WEST SIDE SEWER PROFILE**



**EAST SIDE SEWER PROFILE**

Drawing file: W:\Projects\by\_Village\Norridge\61213232 - Overhill Ave Ph II\Site\Overhill Ave\_Ph\_II.dwg Oct 17, 2014 - 2:31pm

**HANCOCK ENGINEERING**  
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USER NAME -	DESIGNED - SBC/CB	REVISED - IDOT REVIEW
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

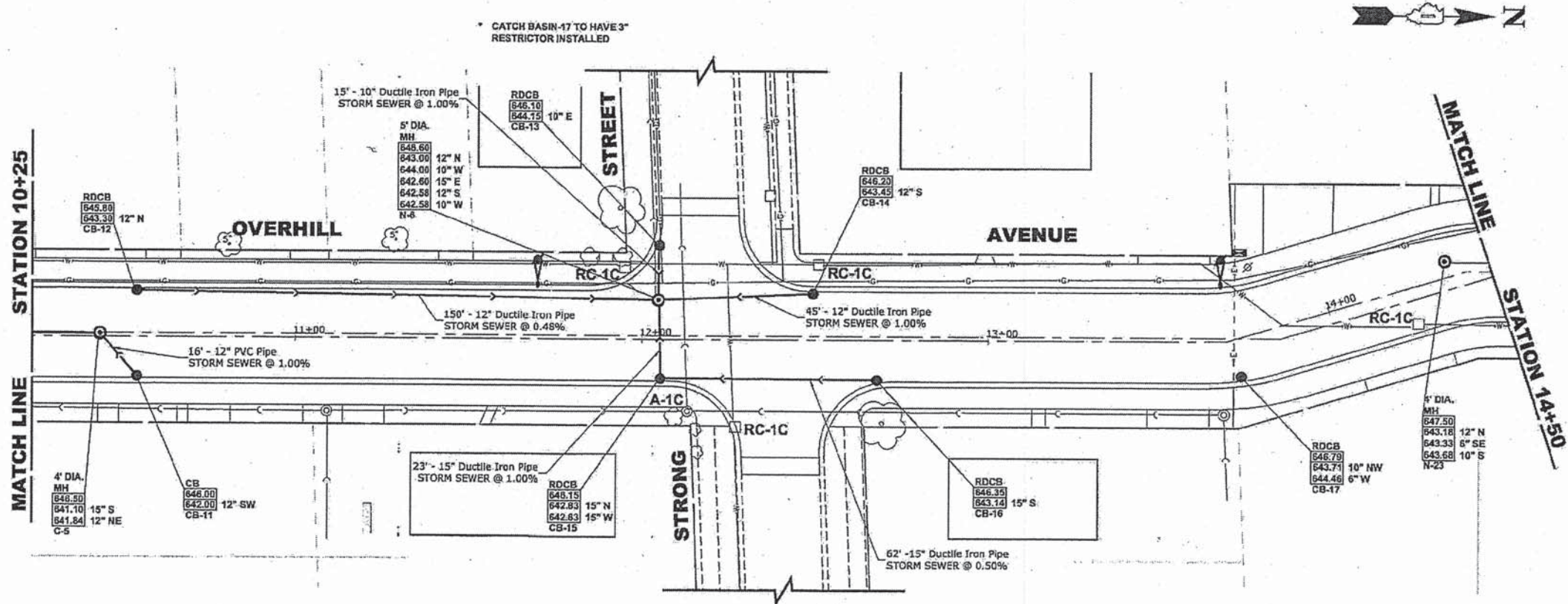
<b>OVERHILL AVENUE PROPOSED UTILITIES PLAN</b>	
SCALE: 1"=20'	SHEET NO. 6 OF 12 SHEETS
STA. 10+25	TO STA. 14+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	24
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

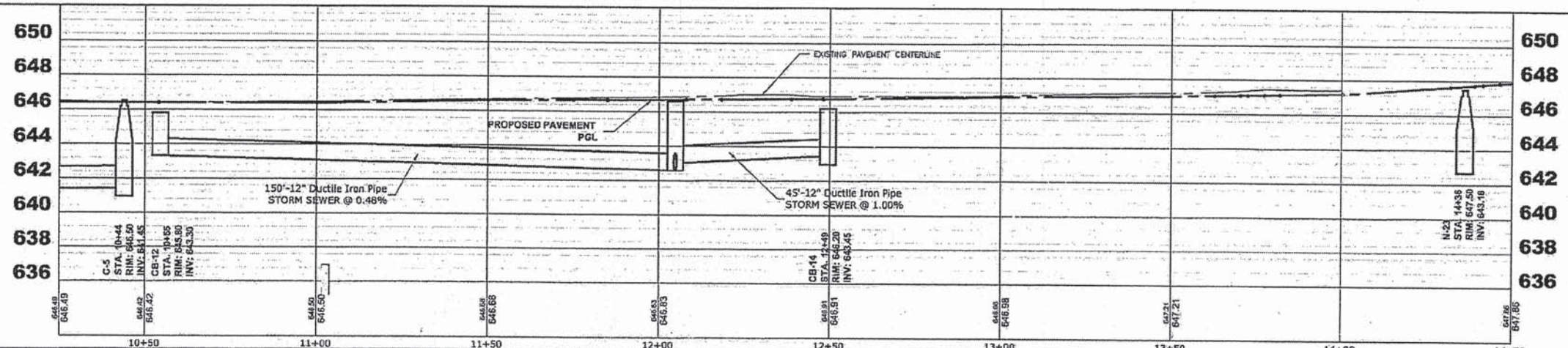
E.H.E. PROJECT NO. 612-13-23201



Drawing file: W:\Projects\_Sy\_Village\Norridge\01211512 - Overhill Ave Ph A\Site\Overhill\_Ave\_Ph\_Bldg Dec 10, 2014 - 11:29am



**PROPOSED IMPROVEMENT**



**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants

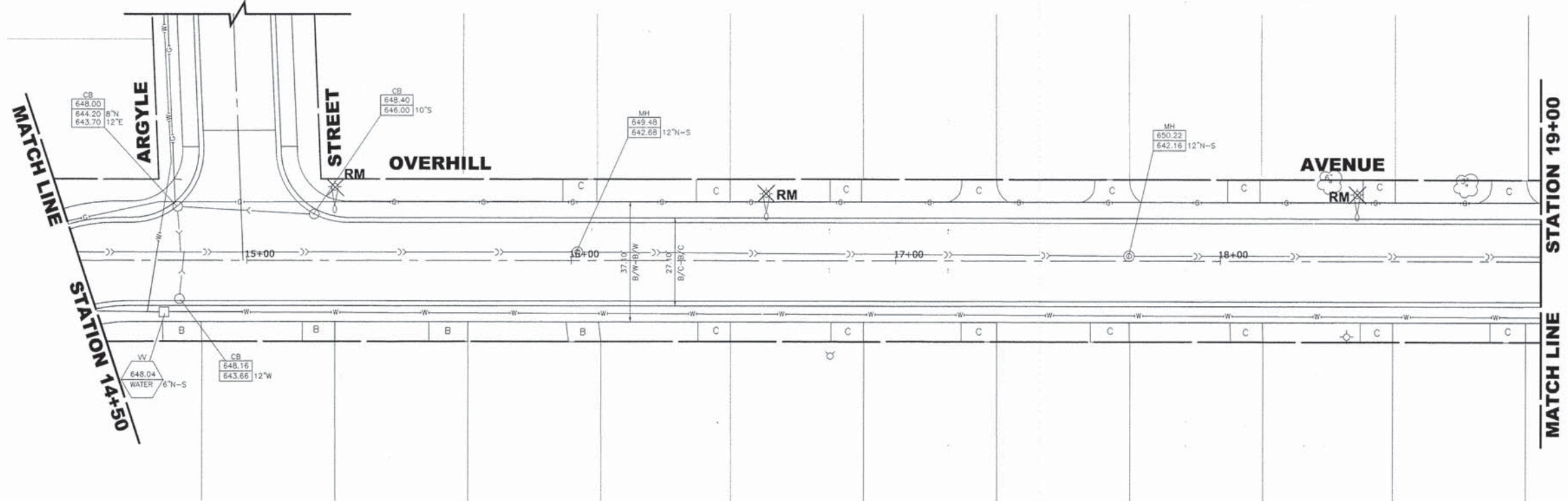
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PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
BY DATE -	CHECKED - WOP	REVISED -
	DATE - 10/29/14	DESIGNER -

**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

**OVERHILL AVENUE  
 PROPOSED UTILITIES PLAN**

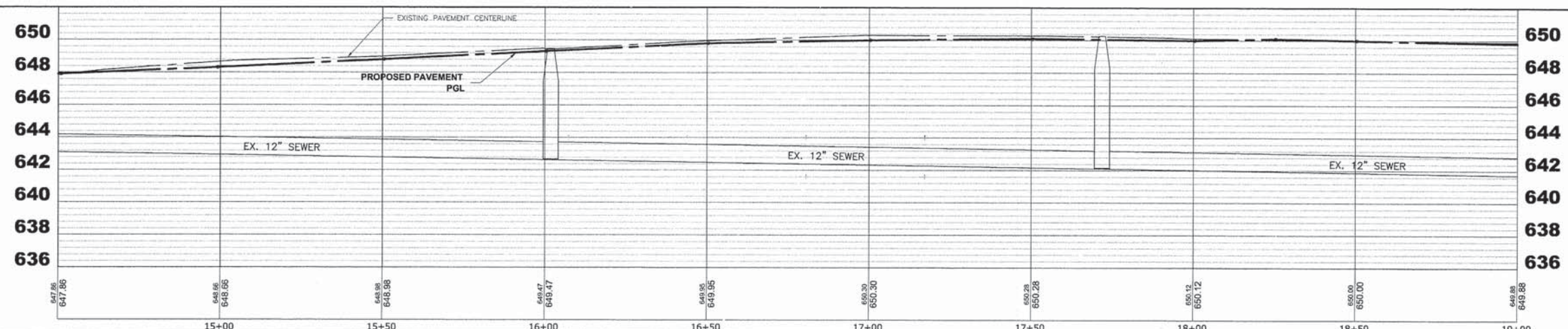
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FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		

SCALE: 1"=20' SHEET NO. 6 OF 12 SHEETS STA. 10+25 TO STA. 14+50



**EXISTING TOPOGRAPHY**

**NOTE:**  
 1. WHEN PROPOSED STORM SEWER IS BEING INSTALLED AT APPROXIMATE SAME LINE AND ELEVATION OF EXISTING STORM SEWER, THE EXISTING SEWER IS TO BE REMOVED OF AND DISPOSED OF PROPERLY. THIS WORK WILL BE PAID FOR UNDER THE PROPOSED SEWER ITEM.  
 2. SEWERS TO BE ABANDONED SHALL HAVE BOTH ENDS PLUGGED WITH A 2' CONCRETE PLUG. THIS WORK WILL BE INCLUDED IN COST OF APPLICABLE STRUCTURES TO BE REMOVED.



Drawing file: W:\Projects\_by\_Village\Norrige\61213232 - Overhill Ave Ph. I\Site\Overhill Ave\_Ph. I.dwg Oct 17, 2014 - 2:31pm

**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants  
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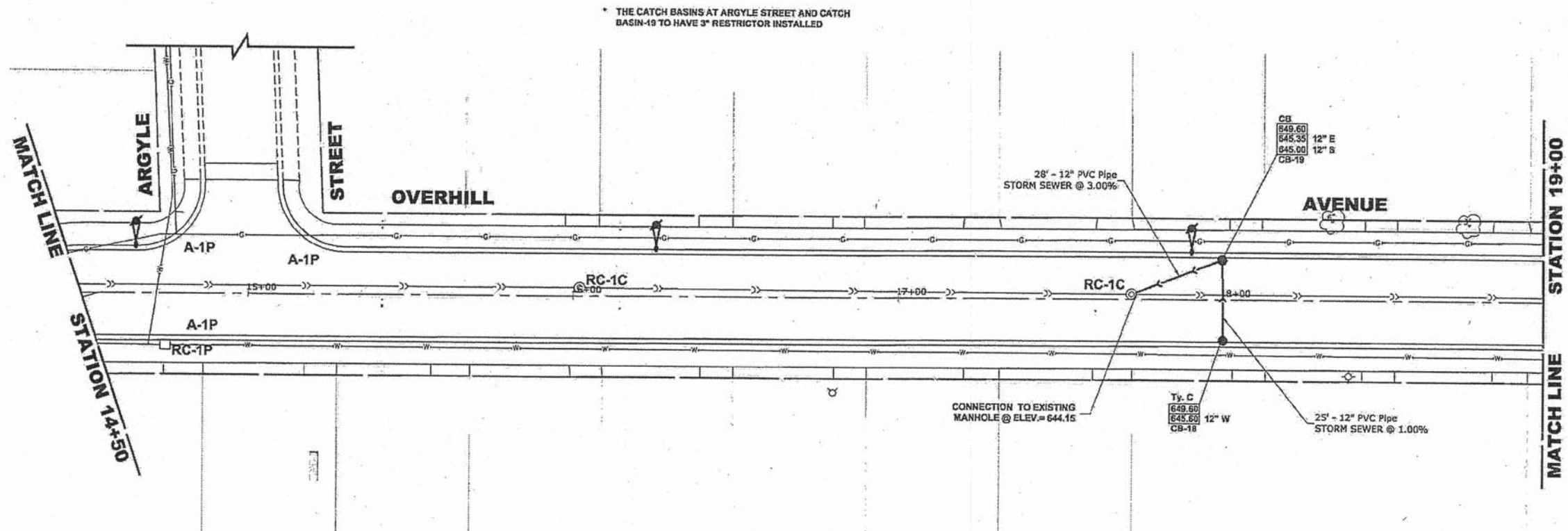
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**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

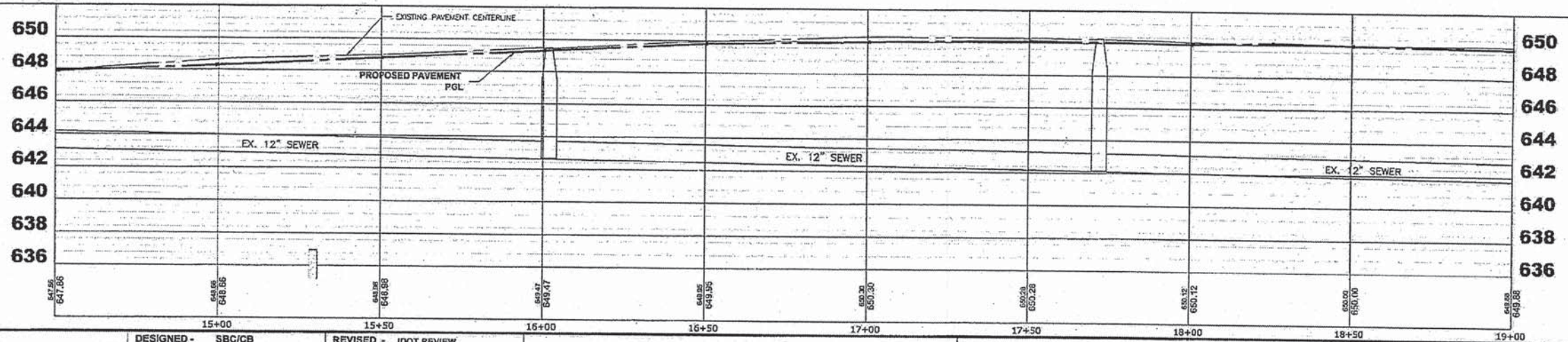
<b>OVERHILL AVENUE EXISTING UTILITIES PLAN</b>	
SCALE: 1"=20'	SHEET NO. 7 OF 12 SHEETS
STA. 14+50	TO STA. 19+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	26
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

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**PROPOSED IMPROVEMENT**



**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants

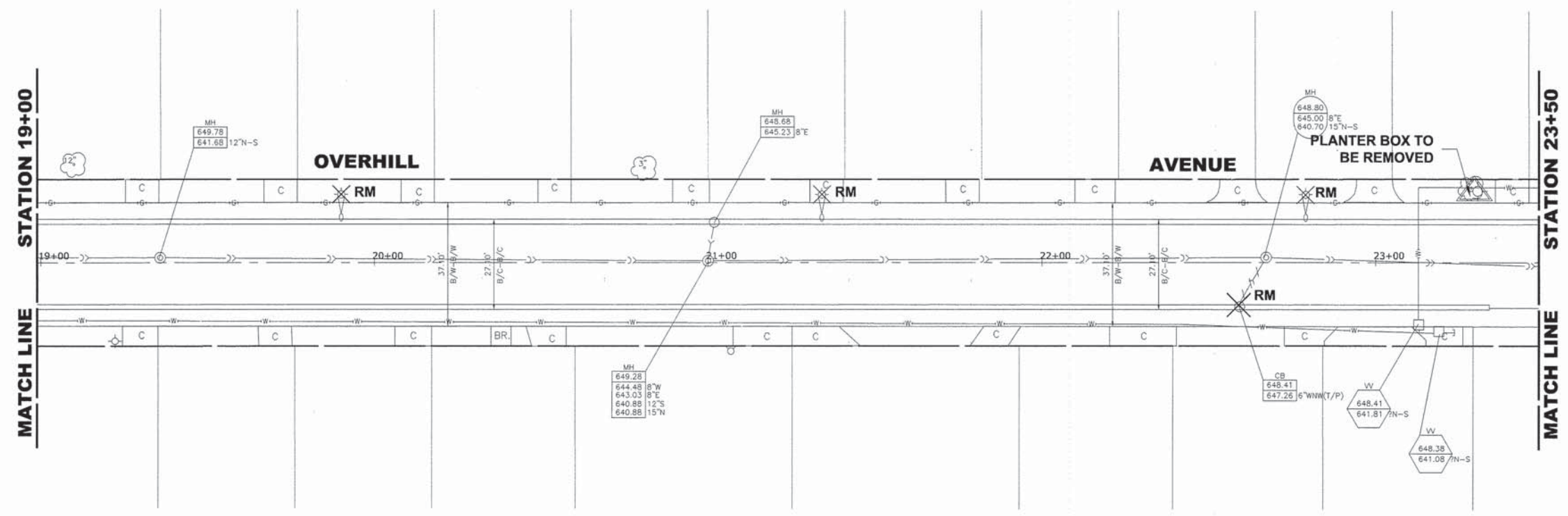
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PLOT SCALE -	CHECKED - WOP	REVISED -
	DATE 10/29/14	DESIGNER

**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

**OVERHILL AVENUE  
 PROPOSED UTILITIES PLAN**

SCALE: 1"=20' | SHEET NO. 8 OF 12 SHEETS | STA. 14+50 TO STA. 19+00

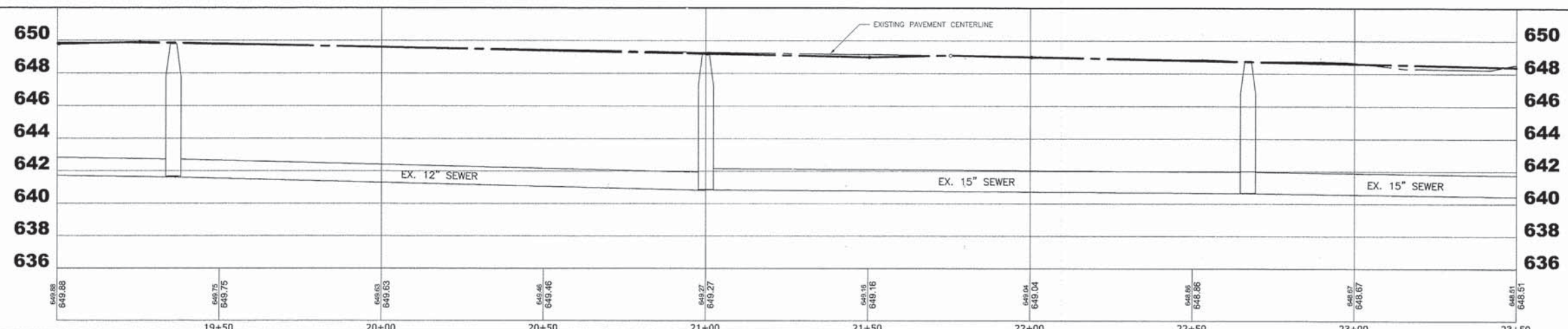
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	27
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		



**NOTE:**

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**EXISTING TOPOGRAPHY**



Drawing file: W:\Projects\_by\_Village\Norridge\61213232 - Overhill Ave Ph II\Site\Overhill Ave\_Ph II.dwg Oct 17, 2014 - 2:32pm

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USER NAME -	DESIGNED - SBC/CB	REVISED - IDOT REVIEW
	DRAWN - MK,DMM,ECW	REVISED -
PLOT SCALE -	CHECKED - WOP	REVISED -
PLOT DATE -	DATE - 10/02/14	REVISED -

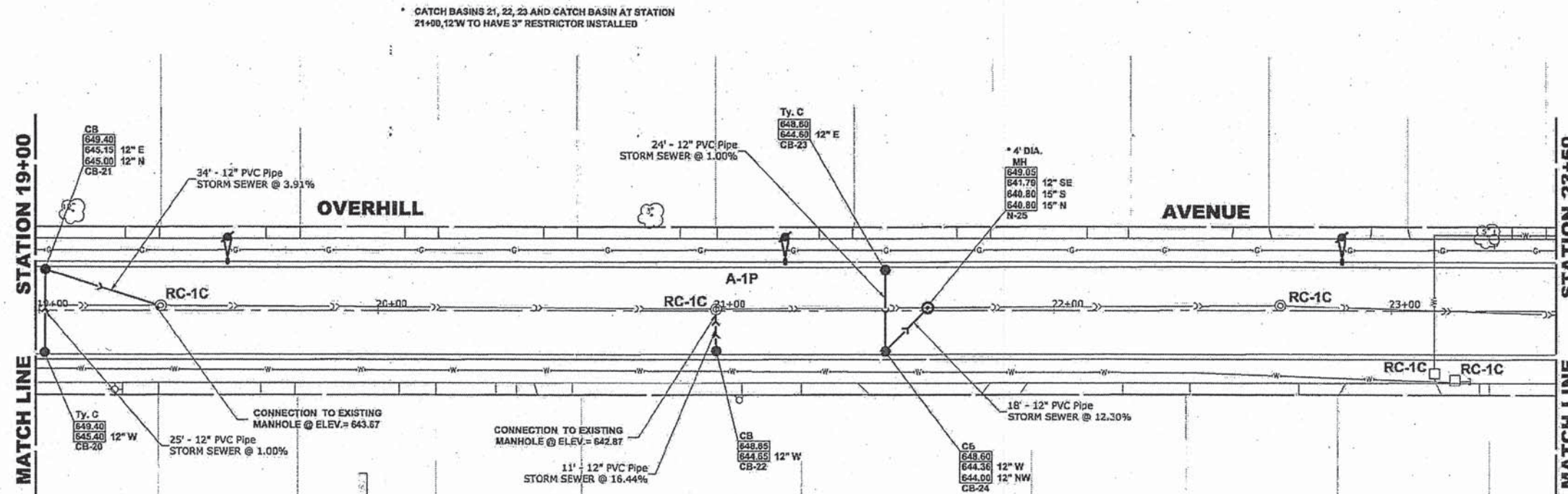
**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

**OVERHILL AVENUE  
 EXISTING UTILITIES PLAN**

SCALE: 1"=20'    SHEET NO. 9 OF 12 SHEETS    STA. 19+00 TO STA. 23+50

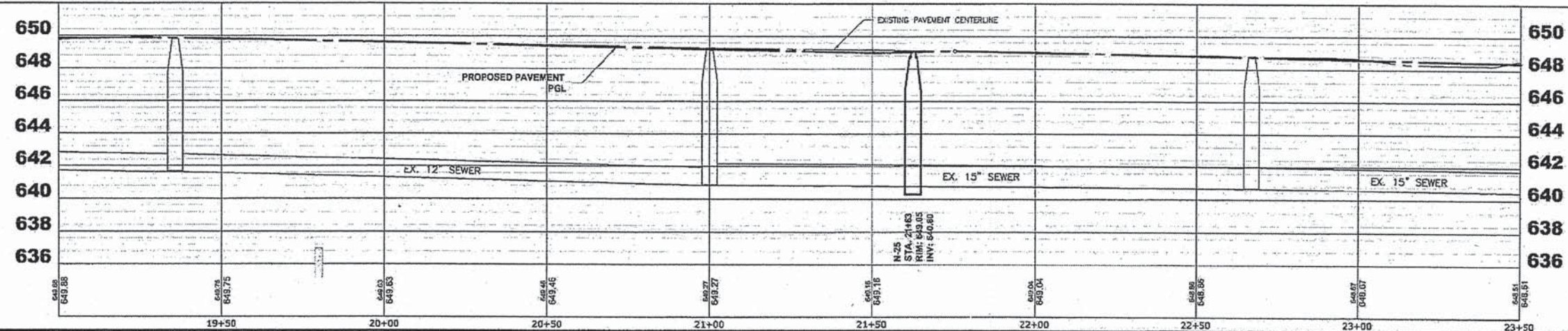
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-	13-00064-00-FP	COOK	62	28
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

Drawing File: \\projects\_by\_nillage\murray\01213332 - Overhill Ave Pk Village\Overhill Ave\_Pk.dwg Dec 10, 2014 - 11:30am



**PROPOSED IMPROVEMENT**

\* THIS MANHOLE SHALL BE CONSTRUCTED WITH A "DOG HOUSE" MANHOLE PER MWRD REQUIREMENTS AND AS SHOWN ON DETAIL SHEET 33. THIS ITEM SHALL BE PAID FOR AS "MANHOLE, TYPE A, 4' DIAMETER, TYPE I FRAME, OPEN LID"



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 Civil Engineers  
 Municipal Consultants

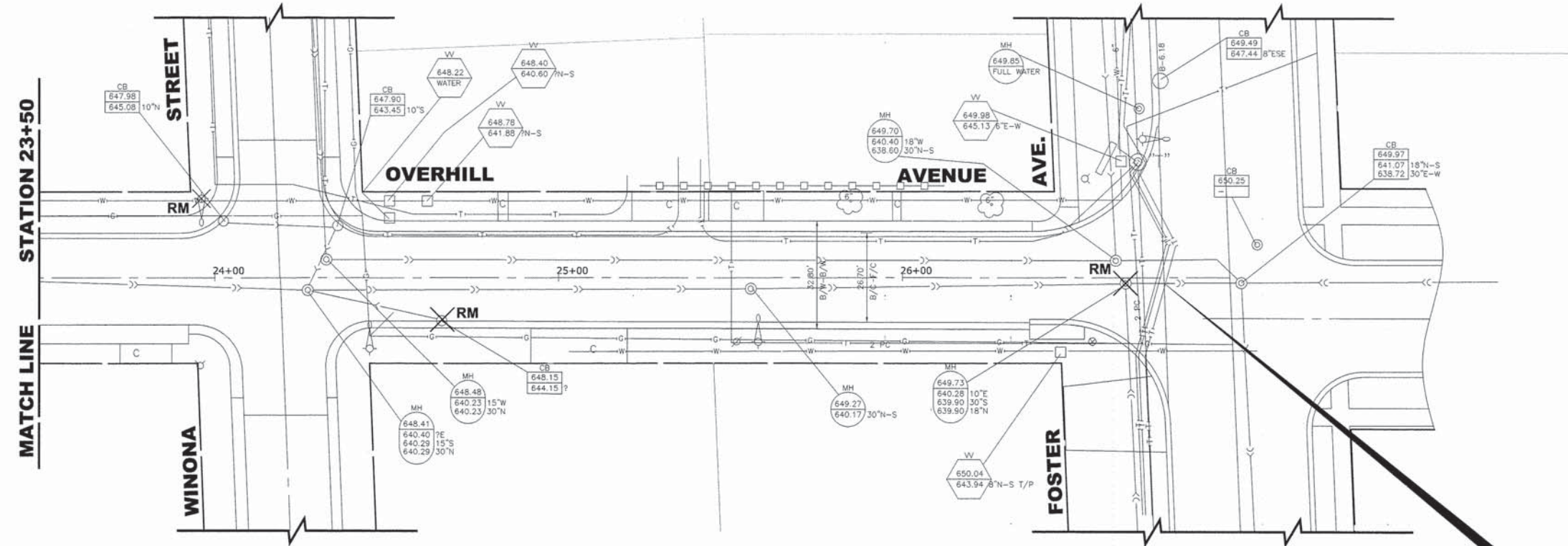
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DATE -	10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

**OVERHILL AVENUE  
 PROPOSED UTILITIES PLAN**

SCALE: 1"=20' | SHEET NO. 10 OF 12 SHEETS | STA. 19+00 TO STA. 23+50

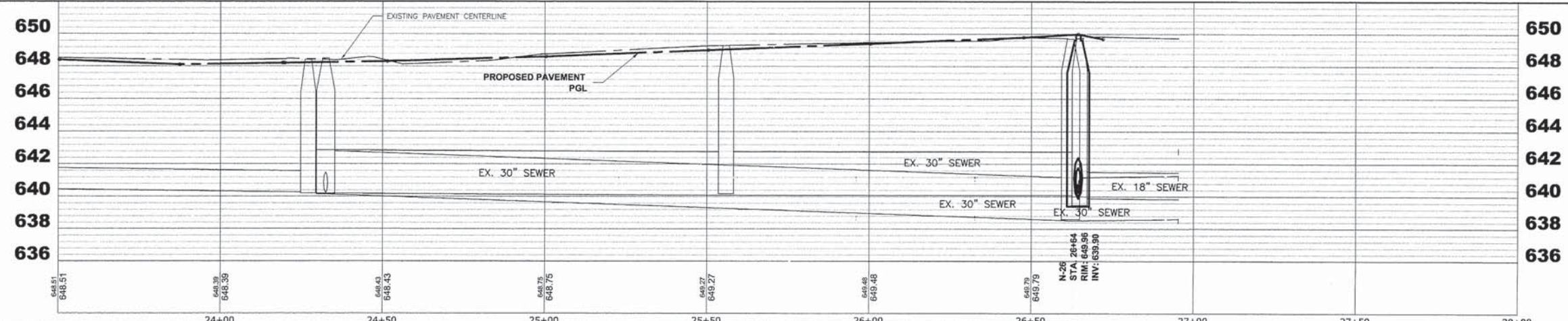
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	13-00064-00-FP	COOK	82	29
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		



**EXISTING TOPOGRAPHY**

**NOTE:**  
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**END IMPROVEMENT  
 OVERHILL AVENUE  
 STATION 26+76**



Drawing file: W:\Projects\_by\_Village\Norridge\61213232 - Overhill Ave Ph II\Site\Overhill Ave\_Ph II.dwg Oct. 17, 2014 - 2:32pm

**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants  
 Established 1911

1913 Kimbark Road  
 Wheeling, Illinois 60090-1100  
 Phone: 708.261.6000  
 Fax: 708.261.7000

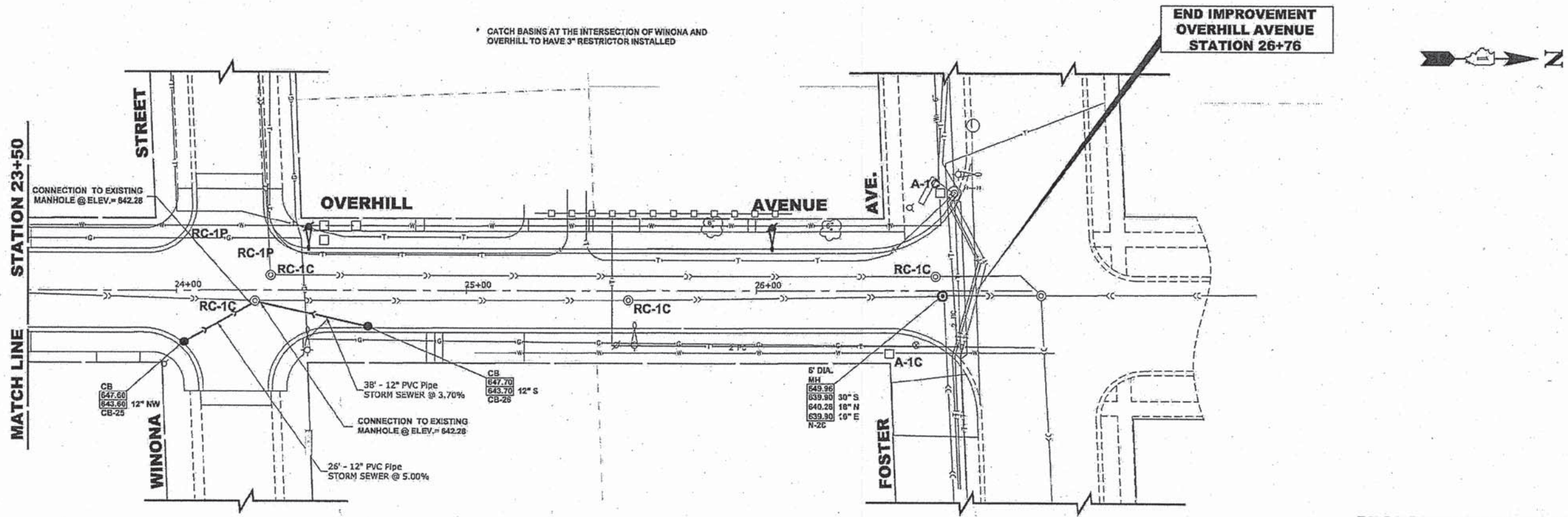
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**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

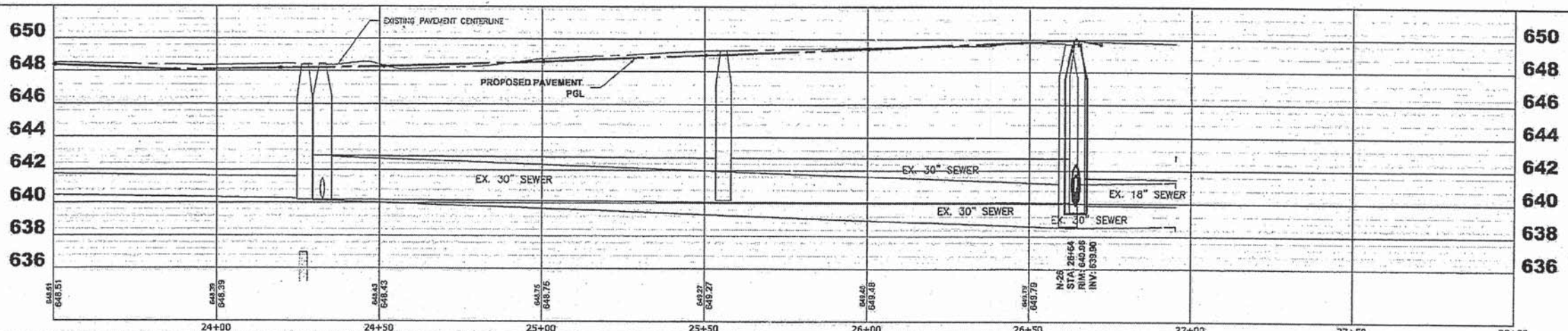
<b>OVERHILL AVENUE    EXISTING UTILITIES PLAN</b>	
SCALE: 1"=20'	SHEET NO. 11 OF 12 SHEETS
STA. 23+50	TO STA. 28+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	30
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

E.H.E. PROJECT NO. 612-13-23201



**PROPOSED IMPROVEMENT**



**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants

USER NAME -	DESIGNED - SBC/CB	REVISED - IDOT REVIEW
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
	CHECKED - WOP	REVISED -
	DATE 4/12/14	DESIGNER

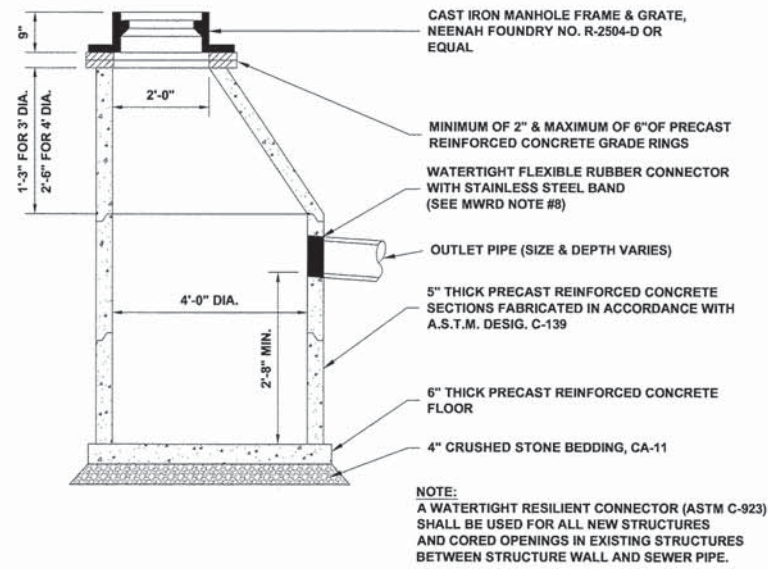
**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

**OVERHILL AVENUE  
 PROPOSED UTILITIES PLAN**

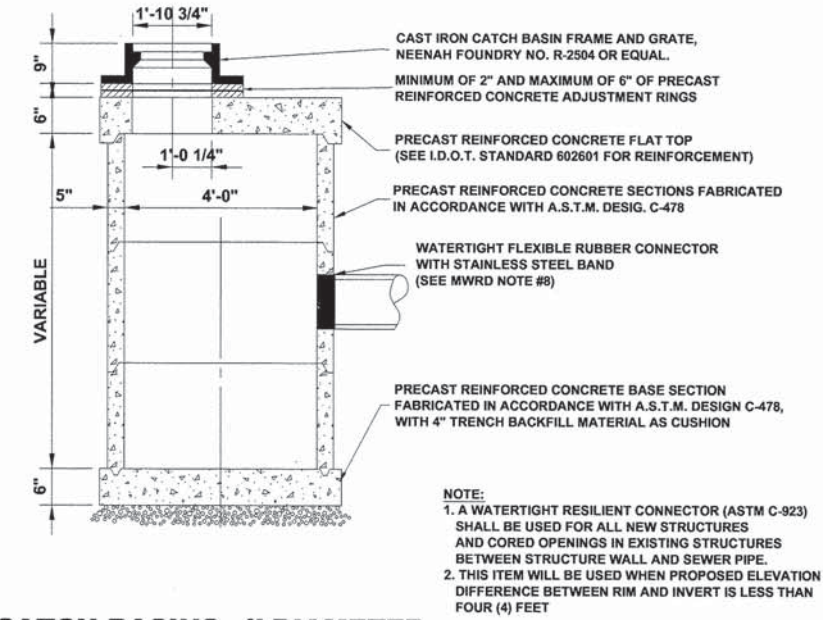
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 STA 2440 TO STA 2840

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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FIELD BOOK NO. 163/AERIALS		CONTRACT NO. 61A82		

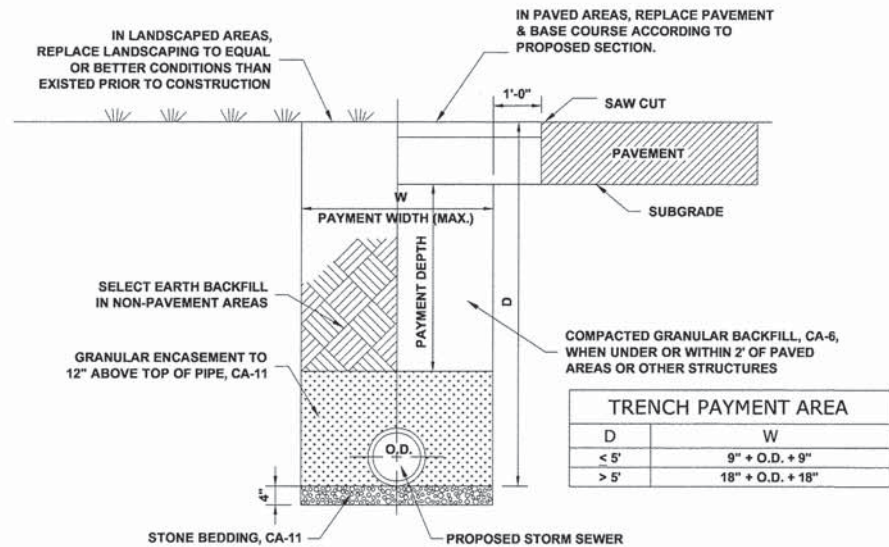
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**CATCH BASINS, TYPE A, 4' DIAMETER, TYPE 1 FRAME, OPEN LID**

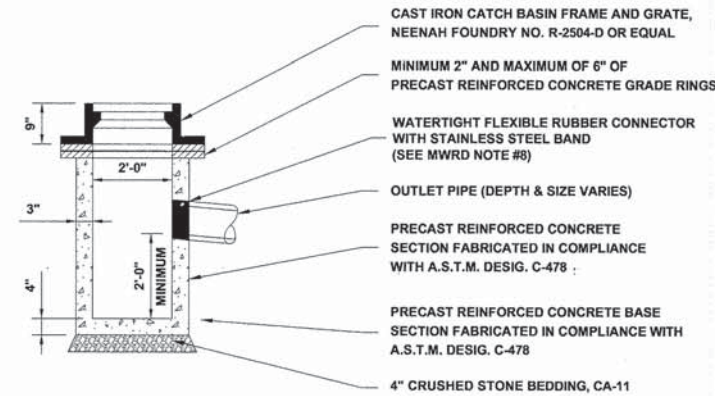


**CATCH BASINS, 4' DIAMETER, TYPE 1 FRAME, OPEN LID (SPECIAL)**

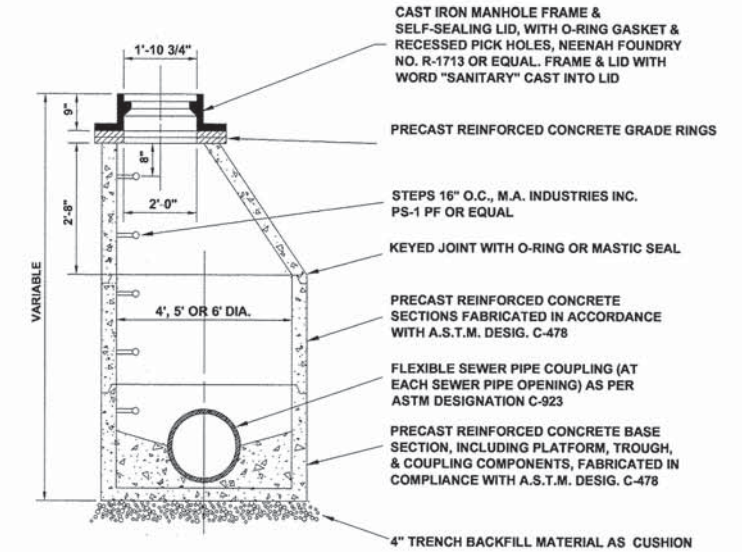


**TYPICAL SEWER TRENCH DETAIL**

TRENCH PAYMENT AREA	
D	W
< 5'	9" + O.D. + 9"
> 5'	18" + O.D. + 18"



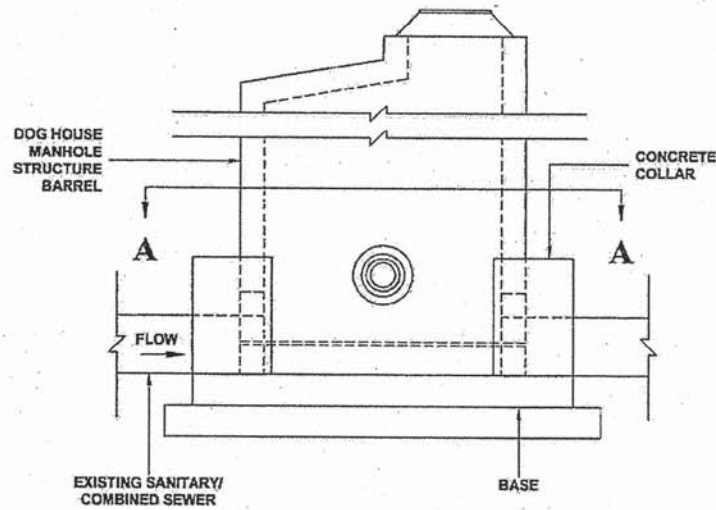
**CATCH BASIN, TYPE C**



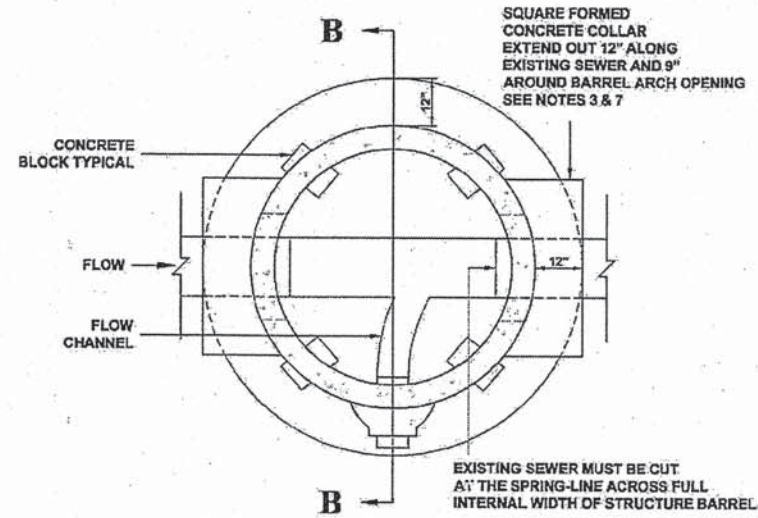
**MANHOLES, TYPE A, TYPE 1 FRAME, CLOSED LID**

Drawing file: W:\Projects\_by\_Village\Norrridge\61213232 - Overhill Ave Ph II\30-34\_Drainage-Details.dwg Oct 02, 2014 - 4:26pm

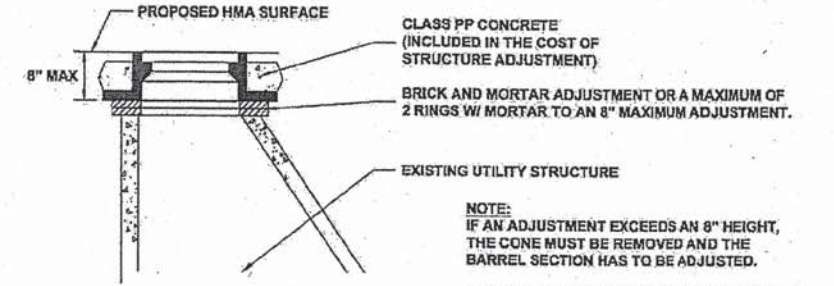




**"DOG HOUSE" MANHOLE PROFILE**

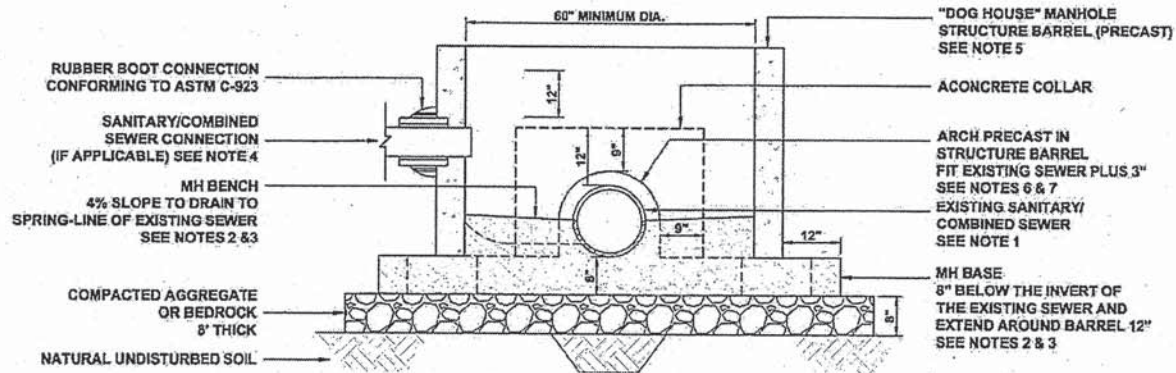


**SECTION A-A**

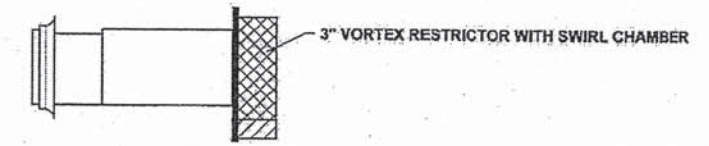


**STRUCTURE ADJUSTMENT**

- NOTES:**
- EXISTING SANITARY OR COMBINED SEWER MUST BE 15" DIAMETER OR LARGER FOR "DOG HOUSE" MANHOLE USE.
  - INTEGRAL POUR FOR BASE AND BENCH. (NO PRECAST BASE)
  - ALL POURED-IN-PLACE CONCRETE MUST BE 4000 PSI NON-SHRINK MDX.
  - EXTERNAL DROP CONNECTION MUST BE PROVIDED IF INVERT OF CONNECTING SEWER IS 24" OR MORE ABOVE THE INVERT OF OUTLET. (SEE SEPARATE MWRD STANDARD DROP DETAIL.)
  - MANHOLE DIAMETER MINIMUM 60" - INCREASES BASED ON THE EXISTING SEWER DIAMETER.
  - CONCRETE BONDING AGENT MUST BE APPLIED TO ALL INTERFACES OF PRECAST CONCRETE SURFACES WITH POURED-IN-PLACE CONCRETE.
  - A CURVED INTERNAL ARCH FORM MUST BE USED DURING COLLAR CONCRETE FILL. NO BRICK, MORTAR, OR DEBRIS IS TO BE USED IN PLACE OF CONSOLIDATED CONCRETE.
  - DEBRIS MUST NOT BE ALLOWED TO ENTER THE SEWER SYSTEM AT ANY TIME DURING CONSTRUCTION.
  - ALL DIMENSIONS NOTED ARE MINIMUM ALLOWED.
  - THE STRUCTURE MUST NOT BE BACKFILLED FOR A MINIMUM OF 24 HOURS AFTER CONSTRUCTION.



**SECTION B-B**



- PULL ON RESTRICTOR TO VERIFY THAT A TIGHT FIT IS MADE
- INSERT THE RESTRICTOR WITH THE OPENING DOWN. UPON TIGHTENING OF THE 2 BOLTS ON THE FACE OF THE RESTRICTOR, THE RUBBER O-RINGS WILL PROVIDE A WATER TIGHT SEAL.
- THE RESTRICTORS CAN BE OBTAINED FROM DWM CENTRAL DISTRICT AT 3901 S. ASHLAND AVE. THE CONTRACTOR SHOULD ARRANGE FOR PICK-UP BY CONTACTING 312-747-1177.

**CATCH BASIN TRAP AND RESTRICTOR DETAIL**

VORTEX RESTRICTORS MUST BE INSTALLED ON ALL EXISTING AND PROPOSED CATCH BASINS NORTH OF STRONG AVENUE.

**\* MANHOLE, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID**

THIS MANHOLE IS TO BE USED FOR PROPOSED MH# N-25

Drawing file: W:\Projects\by\_dwg\Norridge\131322 - Overhill Ave Ph II\31-31\_Drainage-Detail.dwg Dec 10, 2014 - 11:35am



USER NAME -	DESIGNED - SBC/CB	REVISED -
PLOT SCALE -	DRAWN - MK, DMM, ECW	REVISED -
DATE -	CHECKED - WOP	REVISED -
	DATE - 10/29/14	DESIGNER -

**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

**DRAINAGE AND UTILITIES DETAILS**

SCALE: NONE | SHEET NO. 3 OF 3 SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	13-0064-00-PP	COOK	62	33
FIELD BOOK NO. 1839/AERIALS		CONTRACT NO. 61A82		

## DRAINAGE AND UTILITIES NOTES

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, COMBINED SEWERS, TELEPHONE LINES, COMMUNICATION LINES, ELECTRIC LINES, GAS MAINS, AND WATER SERVICES ARE APPROXIMATE AND THEIR SPECIFIC LOCATIONS ARE TO BE DETERMINED IN THE FIELD AT NO COMPENSATION TO THE CONTRACTOR.

COORDINATION OF ALL UTILITY WORK INVOLVED WITHIN THE CONSTRUCTION AREAS SHALL BE SUBJECT TO DISCUSSION AND CLARIFICATION AT A PRECONSTRUCTION MEETING.

WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINAGE STRUCTURES OR SEWERS UNTIL PERMANENT CONNECTIONS TO SEWERS ARE BUILT AND IN SERVICE. THIS WORK SHALL BE PAID FOR AS TEMPORARY DRAINAGE CONNECTIONS.

IF, DURING CONSTRUCTION OPERATIONS, ANY LOOSE MATERIALS ARE DEPOSITED IN THE FLOW LINES OF GUTTERS OR DRAINAGE STRUCTURES SO THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE OBSTRUCTING MATERIALS SHALL BE REMOVED AT THE CLOSE OF EACH WORK DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES ARE TO BE FREE OF ALL DIRT, DEBRIS, AND OBSTRUCTING MATERIALS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCIDENTAL TO THE CONTRACT.

ALL COSTS INVOLVED IN CONNECTING PROPOSED STORM SEWERS AND STORM STRUCTURES TO EXISTING STORM SEWERS OR PROPOSED STORM SEWERS SHALL BE CONSIDERED INCLUDED IN THE COST OF PROPOSED ITEMS BEING CONSTRUCTED.

THE CONTRACTOR SHALL VERIFY THE TYPE OF ALL WATER MAIN HARDWARE INCLUDING VALVES, FIRE HYDRANTS, VALVE BOXES, CORPORATION STOPS, CURB STOPS, AND WATER SERVICES BOXES WITH THE UTILITY SUPERINTENDENT PRIOR TO ORDERING SUCH MATERIAL.

THE TYPE OF FRAMES AND GRATES REQUIRED FOR ALL CATCH BASINS AND MANHOLES LISTED IN THE SUMMARY OF QUANTITIES MAY BE FOUND ON THE PLANS AT THEIR RESPECTIVE LOCATIONS. WHERE LIDS ARE CALLED FOR ON THE PLANS, THEY SHALL BE IN ACCORDANCE WITH ARTICLE 604.01 OF THE STANDARD SPECIFICATIONS AND THE TERM LID IS USED IN LIEU OF GRATE.

ON ALL IMPROVEMENTS, THE FRAMES AND LIDS OF EXISTING CATCH BASINS, INLETS, MANHOLES, AND VALVE VAULTS WHICH ARE TO BE ABANDONED DUE TO CONSTRUCTION OF THIS IMPROVEMENT ARE TO REMAIN THE PROPERTY OF THE VILLAGE OF NORRIDGE AND BE SALVAGED. THE CONTRACTOR IS TO DELIVER FRAMES AND LIDS TO THE VILLAGE OF NORRIDGE PUBLIC WORKS YARD LOCATED AT CORNER OF FOSTER AND CUMBERLAND AVE.

ANY COSTS FOR SHEETING OR SHORING REQUIRED FOR THE STORM SEWER INSTALLATION OR OTHER CONSTRUCTION ELEMENTS REQUIRING RELATIVELY DEEP EXCAVATIONS SHALL BE INCLUDED IN THE PARTICULAR PAYMENT ITEM AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR ANY SUPPLEMENTAL WORK ASSOCIATED WITH THE MAINTENANCE OF TRENCH SIDES OR OTHER EXCAVATED AREAS.

UNLESS OTHERWISE SPECIFIED, ABANDONED SEWERS AND DRAINS, AS DESIGNATED BY THE ENGINEER, SHALL BE PLUGGED WITH CLASS "SI" CONCRETE OR BRICK AND SUITABLE MORTAR TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE PAY ITEMS FOR REMOVING AND/OR FILLING THE VARIOUS TYPES OF STRUCTURES.

SEWER PIPE INSTALLED ON THIS PROJECT SHALL CONFORM TO THE FOLLOWING STANDARD:

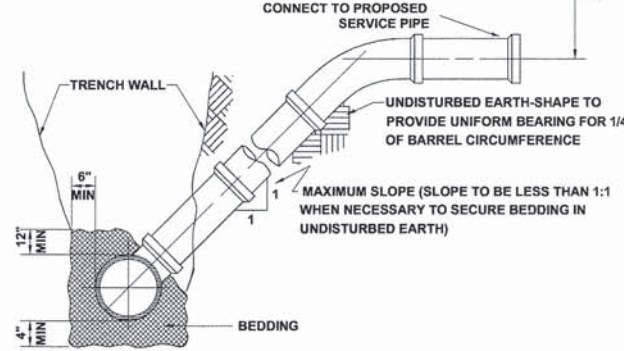
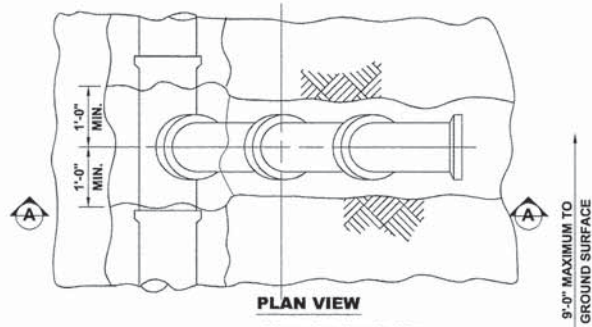
TYPE OF PIPE	MATERIAL STANDARD	JOINT STANDARD
REINFORCED CONCRETE PIPE	ASTM C-76	ASTM C-443
POLYVINYLCHLORIDE PIPE, SDR 26	ASTM D-2241	ASTM D-3139
DUCTILE IRON PIPE, CLASS 52	ASTM A-21.51	ASTM A-21.11

## M.W.R.D.G.C. GENERAL NOTES

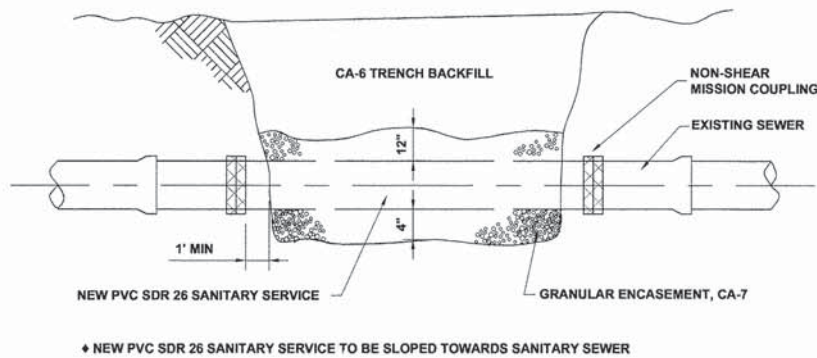
- THE MWRD LOCAL SEWER SYSTEMS SECTION FIELD OFFICE MUST BE NOTIFIED AT LEAST TWO (2) WORKING DAYS PRIOR TO THE COMMENCEMENT OF ANY WORK. THE FIELD OFFICE PHONE NUMBER IS 708-588-4055.
- ELEVATION DATUM IS USGS
- THE MWRD CONSIDERS 0.00 CHICAGO CITY DATUM (CCD) TO BE 579.48 MSL 1929 ADJUSTMENT
- ALL FLOOR DRAINS SHALL DISCHARGE TO THE SANITARY SEWER.
- ALL DOWNSPOUTS AND FOOTING DRAINS SHALL DISCHARGE TO THE STORM SEWER SYSTEM.
- ALL SANITARY SEWER CONSTRUCTION (AND STORM SEWER CONSTRUCTION IN COMBINED SEWER AREAS), REQUIRES STONE BEDDING WITH STONE 1/4 INCH TO 1 INCH IN SIZE, WITH MINIMUM BEDDING THICKNESS EQUAL 1/4 THE OUTSIDE DIAMETER OF THE SEWER PIPE, BUT NOT LESS THAN FOUR (4) INCHES NOR MORE THAN EIGHT (8) INCHES. MATERIAL SHALL BE IDOT GRADATION CA-11 OR CA-13 AND SHALL BE EXTENDED AT LEAST 12 INCHES ABOVE THE TOP OF THE PIPE WHEN PVC PIPE IS USED. DUCTILE IRON DOES REQUIRE STONE BEDDING. IF A CONCRETE CRADLE OR ENCASEMENT IS PROVIDED, BEDDING CAN BE ELIMINATED. <PROVIDE DETAIL>
- A NON-SHEAR MISSION COUPLING SHALL BE USED FOR THE CONNECTION OF SEWER PIPES OF DISSIMILAR MATERIALS.
- WHEN CONNECTING TO AN EXISTING SEWER MAIN BY OTHER THAN AN EXISTING WYE, TEE OR AN EXISTING MANHOLE, ONE OF THE FOLLOWING METHODS SHALL BE USED:
  - USING A CIRCULAR CORING MACHINE, CORE DRILL AN OPENING INTO THE EXISTING PIPE AND INSTALL A SADDLE OR PREFABRICATED TEE.
  - REMOVE AN ENTIRE SECTION OF THE PIPE BREAKING ONLY THE TOP OF ONE BELL AND REPLACE WITH A WYE OR TEE BRANCH SECTION.
  - WITH A PIPE CUTTER, NEATLY AND ACCURATELY CUT OUT DESIRED LENGTH OF PIPE FOR INSERTION OF PROPER FITTING, USING A NON-SHEAR MISSION COUPLING TO HOLD IT FIRMLY ON PLACE.
- WHENEVER A SANITARY/COMBINED SEWER CROSSES UNDER A WATER MAIN, THE MINIMUM VERTICAL DISTANCE FROM THE TOP OF THE SEWER TO THE BOTTOM OF THE WATER MAIN SHALL BE 18 INCHES. FURTHERMORE, A MINIMUM HORIZONTAL DISTANCE OF 10 FEET BETWEEN SANITARY/COMBINED SEWERS AND WATER MAINS SHALL BE MAINTAINED UNLESS: THE SEWER IS LAID IN A SEPARATE TRENCH, KEEPING A MINIMUM 18-INCH VERTICAL SEPARATION; OR THE SEWER IS LAID IN THE SAME TRENCH WITH THE WATER MAIN LOCATED ON THE OPPOSITE SIDE ON A BENCH OF UNDISTURBED EARTH, KEEPING A MINIMUM 18-INCH VERTICAL SEPARATION. IF EITHER THE VERTICAL OR HORIZONTAL DISTANCES DESCRIBED ABOVE CANNOT BE MAINTAINED, OR THE SEWER CROSSES ABOVE THE WATER MAIN, THE SEWER SHALL BE CONSTRUCTED TO WATER MAIN STANDARDS.
- ALL EXISTING SEPTIC SYSTEMS SHALL BE ABANDONED. ABANDONED TANKS SHALL BE FILLED WITH ACCEPTABLE GRANULAR MATERIAL OR REMOVED.
- ALL SANITARY MANHOLES AND STORM MANHOLES IN COMBINED SEWER AREAS SHALL HAVE A MINIMUM INSIDE DIAMETER OF 48 INCHES AND SHALL BE CAST IN PLACE CONCRETE OR PRE-CAST REINFORCED CONCRETE.
- FOOTING DRAINS. EXCEPT FOR FOUNDATION/FOOTING DRAINS PROVIDED TO PROTECT BUILDINGS AND FOR UNDERDRAINS SERVING GREEN INFRASTRUCTURE, DRAIN TILES/FIELD TILES/UNDERDRAINS/PERFORATED PIPES ARE NOT ALLOWED TO BE CONNECTED TO OR TRIBUTARY TO COMBINED SEWERS, SANITARY SEWERS, OR STORM SEWERS TRIBUTARY TO COMBINED SEWERS IN COMBINED SEWER AREAS. CONSTRUCTION OF NEW FACILITIES OF THIS TYPE IS PROHIBITED; AND ALL EXISTING DRAIN TILES AND PERFORATED PIPES, OTHER THAN THOSE SERVING GREEN INFRASTRUCTURE, ENCOUNTERED WITHIN THE PROJECT AREA SHALL BE PLUGGED OR REMOVED, AND SHALL NOT BE CONNECTED TO COMBINED SEWERS, SANITARY SEWERS, OR STORM SEWERS TRIBUTARY TO COMBINED SEWERS.

## DUCTILE IRON PIPE SPECIFICATIONS

- ALL DUCTILE IRON PIPE SEWER MAINS SHALL BE CLASS 52, CEMENT-LINED AND TAR-COATED, MEETING THE REQUIREMENTS OF SPECIFICATIONS ANSI/AWWA C151/A21.51 WITH "PUSH-ON" JOINTS MEETING THE REQUIREMENTS OF SPECIFICATIONS ANSI/AWWA C111/A21.11. WHERE SPECIFIED ON THE PLANS, OR IN THE SPECIFICATIONS, MECHANICAL JOINTS AND "LOCK-TYPE" JOINTS SHALL BE USED IN LIEU OF "PUSH-ON" JOINTS.
- ALL PIPE FITTINGS AND SPECIAL CASTINGS SHALL BE DUCTILE IRON CONFORMING TO ANSI/AWWA C153/A21.53 AND ANSI/AWWA C111/A21.11 SPECIFICATIONS AND SHALL MEET THE MINIMUM REQUIREMENTS OF CLASS 150 DUCTILE IRON PIPE. IF CERTAIN FITTINGS ARE NOT MANUFACTURED IN DUCTILE IRON, CAST IRON FITTINGS SHALL BE ACCEPTABLE. MECHANICAL JOINT TYPE FITTINGS SHALL BE USED.
- ALL PROPOSED DUCTILE IRON PIPE SEWER MAIN WILL BE ENCASED WITHIN FOUR (4) MIL THICK, HIGH-DENSITY POLYETHYLENE TUBING. ALL FITTINGS SHALL BE ENCASED IN A DOUBLE-LAYER OF POLYETHYLENE TUBING. THE POLYETHYLENE MATERIAL SHALL BE MANUFACTURED AND INSTALLED IN COMPLIANCE WITH ANSI/AWWA C105/A21.5. ALL PROPOSED SEWER SERVICES SHALL BE ENCASED IN POLYETHYLENE TUBING FOR A MINIMUM DISTANCE OF THREE FEET (3') FROM THE PROPOSED SEWER MAIN.



**TYPICAL RISER  
FOR SERVICE LATERAL**



**ADJUSTING SANITARY SEWERS, 8 INCH DIAMETER OR LESS**

USER NAME -	DESIGNED - SBC/CB	REVISED -
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PLOT DATE -	DATE - 10/02/14	REVISED -

### OVERHILL AVENUE RECONSTRUCTION VILLAGE OF NORRIDGE, ILLINOIS

### DRAINAGE AND UTILITIES DETAILS, DRAINAGE NOTES AND MWRD GENERAL NOTES

SCALE: NONE SHEET NO. 3 OF 3 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-0064-00-FP	COOK	62	34
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

JOB-SITE SIGNING CODES AND SIZES			JOB-SITE SIGNING CODES AND SIZES		
SYMBOL	SIGN	CODE & SIZE	SYMBOL	SIGN	CODE & SIZE
S	STOP	R1-1 30"x30"	AINSL	Ainslie St.	TO BE REMOVED, SAFELY STORED, AND REINSTALLED BY THE CONTRACTOR
AW	ALL WAY	R1-4 18"x6"	ARGYL	Argyle St.	TO BE REMOVED, SAFELY STORED, AND REINSTALLED BY THE CONTRACTOR
20	SPEED LIMIT 20	R2-1 24"x30"	FOSTR	Foster Ave.	TO BE REMOVED, SAFELY STORED, AND REINSTALLED BY THE CONTRACTOR
SLOW	SLOW Children	R7-1 12"x18"	GUNSN	Gunnison St.	TO BE REMOVED, SAFELY STORED, AND REINSTALLED BY THE CONTRACTOR
NPAT	NO PARKING IN THE	R7-1 12"x18"	LAWRN	Lawrence Ave.	TO BE REMOVED, SAFELY STORED, AND REINSTALLED BY THE CONTRACTOR
NP	P	R7-1 12"x18"	NORRG	Norridge St.	TO BE REMOVED, SAFELY STORED, AND REINSTALLED BY THE CONTRACTOR
SNOW	SNOW ROUTE NO PARKING AFTER 3 INCH SNOW FALL OR MORE UNTIL REMOVAL IS COMPLETED VEHICLES WILL BE TOWED	R7-1 12"x18"	OVRHL	Overhill Ave.	TO BE REMOVED, SAFELY STORED, AND REINSTALLED BY THE CONTRACTOR
NTOR PED	NO TURN ON RED WHEN PEDESTRIANS ARE PRESENT	R10-11b 24"x30"	STRNG	Strong St.	TO BE REMOVED, SAFELY STORED, AND REINSTALLED BY THE CONTRACTOR
NP	P	R7-1 12"x18"	WNONA	Winona St.	TO BE REMOVED, SAFELY STORED, AND REINSTALLED BY THE CONTRACTOR
NP	NO PARKING STREET MAINTENANCE WEDNESDAY 8AM - 1PM	R7-1 12"x18"	ONEIDA	Oneida Ave. →	TO BE REMOVED, SAFELY STORED, AND REINSTALLED BY THE CONTRACTOR
CT	CROSS TRAFFIC DOES NOT STOP	R1-4 18"x6"	WNONA	Ozark Ave. →	TO BE REMOVED, SAFELY STORED, AND REINSTALLED BY THE CONTRACTOR

**SIGNING NOTES:**

DESIGN: AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS, 1975.

CONSTRUCTION: CURRENTS STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS, AND APPLICABLE SPECIAL PROVISIONS.

LOADING: FOR 80 MPH WIND VELOCITY WITH 30% GUST FACTOR, NORMAL TO SIGN.

SOIL PRESSURE: MINIMUM ALLOWABLE SOIL PRESSURE = 1.25 TSF.

MATERIALS: POSTS SHALL CONFORM TO THE REQUIREMENTS OF ARTICLE 1006.29, STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, STATE OF ILLINOIS.

HARDWARE FOR ATTACHING SIGN PANELS TO POSTS SHALL BE CADMIUM OR ZINC COATED STEEL, STAINLESS STEEL, OR ALUMINUM, AND SHALL CONFORM TO THE FOLLOWING SPECIFICATIONS: CADMIUM OR ZINC COATED STEEL: BOLTS, NUTS, AND WASHERS: ASTM A165, TYPE NS. ZINC COATED IN ACCORDANCE WITH AASHTO M-232 OR ASTM A164, TYPE GS.

STAINLESS STEEL: BOLTS: ASTM A193, CLASS I, GRADE B8, NUTS: ASTM A194, GRADE 8 OR 8F, WASHERS: ASTM A240, TYPE 302 OR 304.

ALUMINUM: BOLTS: ASTM B211 ALLOY 6061-T6 OR 2024-T4, NUTS: ASTM B211 ALLOY 6061-T6 OR 6262-T9, WASHERS: ASTM B209 ALCLAD 2024-T4.

**NOTE:**  
PER 2009 MUTCD SECTION 2D.42, PARAGRAPH 03,  
STREET NAME SIGNS ARE TO BE MIXED CASE FONT.

**LEGEND OF SYMBOLS**

SYMBOL	DESCRIPTION
	METAL POST - TYPE A

**NOTE:**  
CONTRACTOR SHALL VERIFY MESSAGE AND COLOR  
OF PROPOSED SIGNAGE WITH ENGINEER PRIOR  
TO ORDERING.

Drawing file: W:\Projects\_by\_Village\Norridge\61213232 - Overhill Ave Ph 0135-41\_Overhill\_03scale.dwg Oct 02, 2014 - 4:13 pm

**HANCOCK ENGINEERING**  
Civil Engineers  
Municipal Consultants  
Established 1911

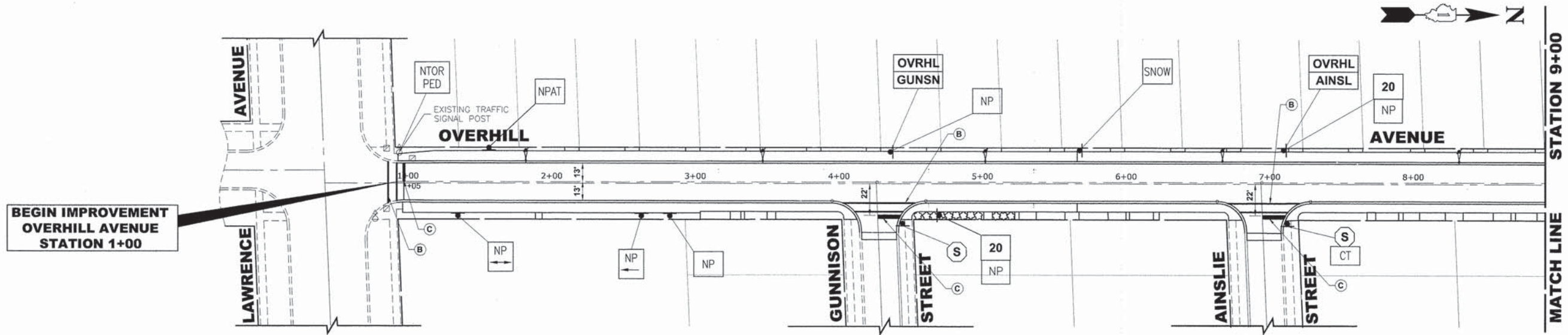
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PLOT DATE -	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

**SIGNING LEGENDS AND NOTES**

SCALE: NTS SHEET NO. 1 OF 1 SHEETS STA. - TO STA. -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	35
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



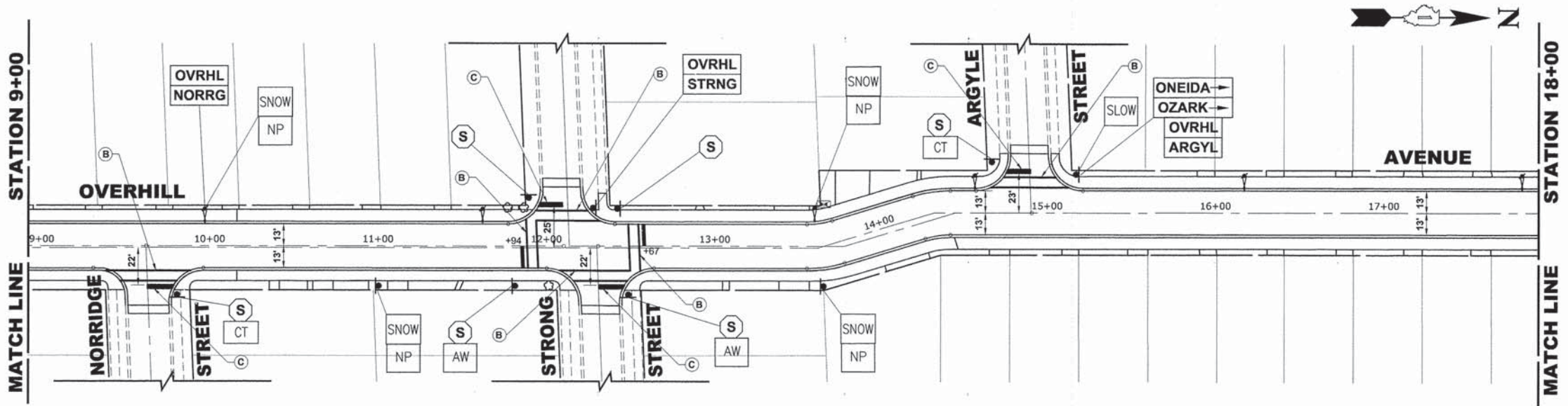
**BEGIN IMPROVEMENT  
OVERHILL AVENUE  
STATION 1+00**

**NOTE:**

1. VILLAGE MISCELLANEOUS (SPECIALTY) SIGNS ARE TO BE REMOVED BY THE THE CONTRACTOR PRIOR TO CONSTRUCTION. AS DESIGNATED BY ENGINEER, THE CONTRACTOR SHALL REINSTALL SIGNS AFTER COMPLETION OF PROJECT. REMOVAL AND NEW REPLACEMENT OF VILLAGE MISCELLANEOUS (SPECIALTY) SIGNS AND THEIR POSTS ARE TO BE DETERMINED BY THE ENGINEER. (COST IS TO BE INCLUDED IN THE "REMOVE AND RE-ERECT EXISTING SIGN " OR "SIGN PANEL - TYPE 1" AND "METAL POST - TYPE I" PAY ITEM)
2. STREET NAME SIGNS AND POSTS ARE TO BE REMOVED BY CONTRACTOR AT THE BEGINNING OF CONSTRUCTION, SAFELY STORED BY THE CONTRACTOR AND REINSTALLED BY THE CONTRACTOR AT THE COMPLETION OF THE PROJECT (COST IS TO BE INCLUDED IN THE "REMOVE AND RE-ERECT EXISTING SIGN" PAY ITEM)
3. ALL POSTS TO BE TYPE A UNLESS OTHERWISE NOTED.
4. CONTRACTOR TO VERIFY LOCATIONS OF ALL SIGNAGE.

**PAVEMENT MARKINGS LEGEND**

ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING TYPE 1 - LINE 6", CROSS WALK, WHITE	B
THERMOPLASTIC PAVEMENT MARKING TYPE 1 - LINE 24", STOP BAR, WHITE	C



**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants  
 Established 1911

3011 Skowhegan Road  
 Westbrook, ME 04094-1700  
 Phone: 708-663-0100  
 Fax: 708-663-0102

USER NAME -	DESIGNED - SBC/CB	REVISED - IDOT REVIEW
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

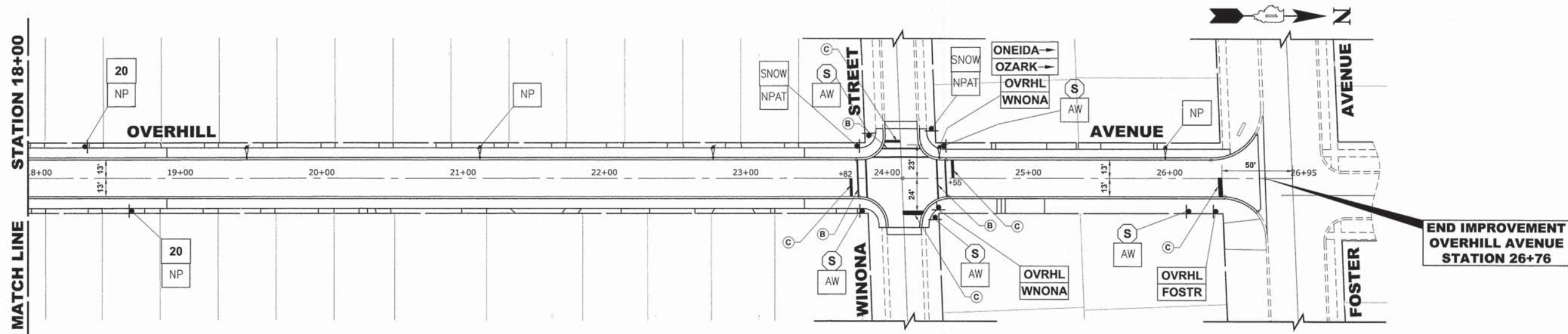
**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

**PAVEMENT MARKING  
AND SIGNING PLAN**

SCALE: 1" = 40' SHEET NO. 1 OF 2 SHEETS STA. 1+00 TO STA. 18+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	36
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

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**NOTE:**

- VILLAGE MISCELLANEOUS (SPECIALTY) SIGNS ARE TO BE REMOVED BY THE THE CONTRACTOR PRIOR TO CONSTRUCTION. AS DESIGNATED BY ENGINEER, THE CONTRACTOR SHALL REINSTALL SIGNS AFTER COMPLETION OF PROJECT. REMOVAL AND NEW REPLACEMENT OF VILLAGE MISCELLANEOUS (SPECIALTY) SIGNS AND THEIR POSTS ARE TO BE DETERMINED BY THE ENGINEER. (COST IS TO BE INCLUDED IN THE "REMOVE AND RE-ERECT EXISTING SIGN " OR "SIGN PANEL - TYPE 1" AND "METAL POST - TYPE I" PAY ITEM)
- STREET NAME SIGNS AND POSTS ARE TO BE REMOVED BY CONTRACTOR AT THE BEGINNING OF CONSTRUCTION, SAFELY STORED BY THE CONTRACTOR AND REINSTALLED BY THE CONTRACTOR AT THE COMPLETION OF THE PROJECT (COST IS TO BE INCLUDED IN THE "REMOVE AND RE-ERECT EXISTING SIGN" PAY ITEM)
- ALL POSTS TO BE TYPE A UNLESS OTHERWISE NOTED.
- CONTRACTOR TO VERIFY LOCATIONS OF ALL SIGNAGE.

**PAVEMENT MARKINGS LEGEND**

ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING TYPE 1 - LINE 6", CROSS WALK, WHITE	B
THERMOPLASTIC PAVEMENT MARKING TYPE 1 - LINE 24", STOP BAR, WHITE	C

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**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants  
 Established 1911

USER NAME -	DESIGNED - SBC/CB	REVISED - IDOT REVIEW
	DRAWN - MK,DMM,ECW	REVISED -
PLOT SCALE -	CHECKED - WOP	REVISED -
PLOT DATE -	DATE - 10/02/14	REVISED -

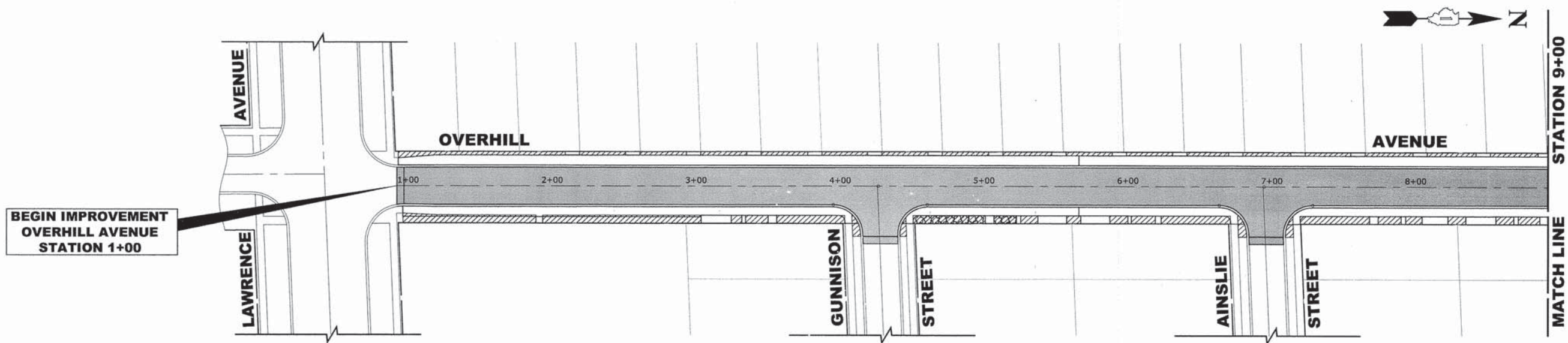
**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

**PAVEMENT MARKING  
 AND SIGNING PLAN**

SCALE: 1" = 40' SHEET NO. 2 OF 2 SHEETS STA. 18+00 TO STA. 26+88

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	37
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

E.N.E. PROJECT NO. 612-13-23201



**BEGIN IMPROVEMENT  
OVERHILL AVENUE  
STATION 1+00**

**LANDSCAPING NOTES**

**SODDING:**  
ALL DISTURBED AREAS INDICATED ON CROSS SECTION OR LANDSCAPING PLANS TO BE SODDED, INCLUDING FERTILIZER NUTRIENTS\*, AND SUPPLEMENTAL WATERING WHICH IS TO BE USED AFTER THE INITIAL WATERING (AS SPECIFIED AND AS DIRECTED BY THE ENGINEER) AT THE RATE OF 10 GALLONS PER SQUARE YARD SODDING. SODDED SLOPES WHICH ARE 2:1 OR STEEPER SHALL BE STAKED. (PROVIDE A 3" STRIP AROUND CATCH BASINS IN SEEDING AREAS).

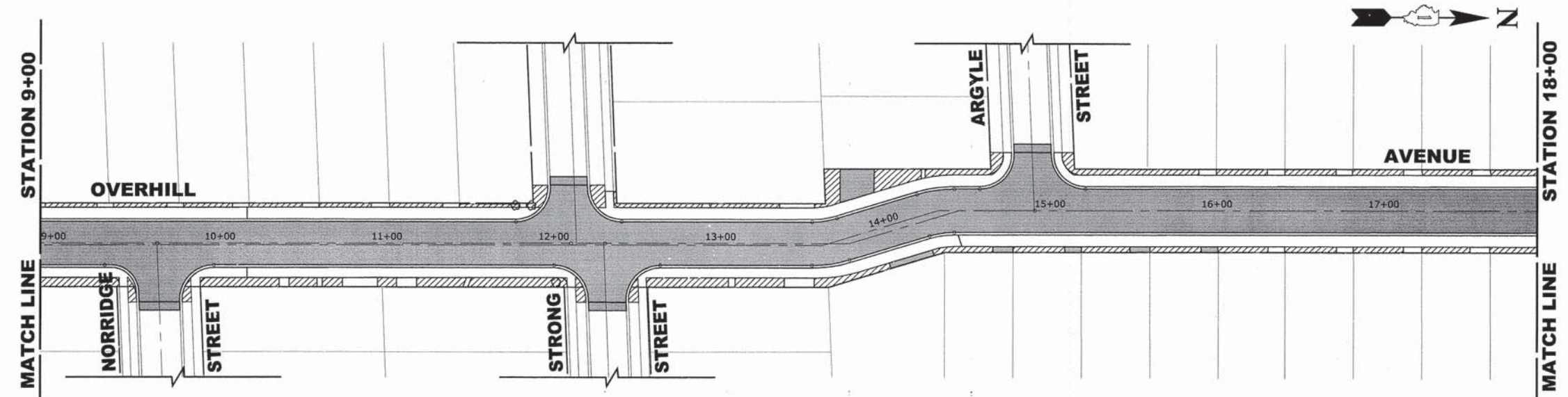
**FERTILIZER NUTRIENTS\* FOR SODDING:**  
USE A FERTILIZER WITH A 1:1:1 RATIO PER ACRE:  
60 LBS. NITROGEN FERTILIZER NUTRIENT  
60 LBS. PHOSPHORUS FERTILIZER NUTRIENT  
60 LBS. POTASSIUM FERTILIZER NUTRIENT  
**TOTAL: 180 LBS. PER ACRE**

**4" TOP SOIL LAYER:**  
SOIL SHALL BE SPREAD AS INDICATED ON THE TYPICAL CROSS SECTIONS.

A MINIMUM OF GRADING TO BE PERMITTED WITHIN AN APPROXIMATE RADIUS OF 6' FROM ALL TREES TO BE SAVED AS DETERMINED BY THE ENGINEER.

**LEGEND OF SYMBOLS**

SYMBOL	DESCRIPTION
[Empty rectangle]	PROPOSED CONCRETE AREA
[Hatched rectangle]	PROPOSED SODDED PARKWAY
[Solid grey rectangle]	PROPOSED HOT-MIX ASPHALT AREA



USER NAME -	DESIGNED - SBC/CB	REVISED - IDOT REVIEW
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

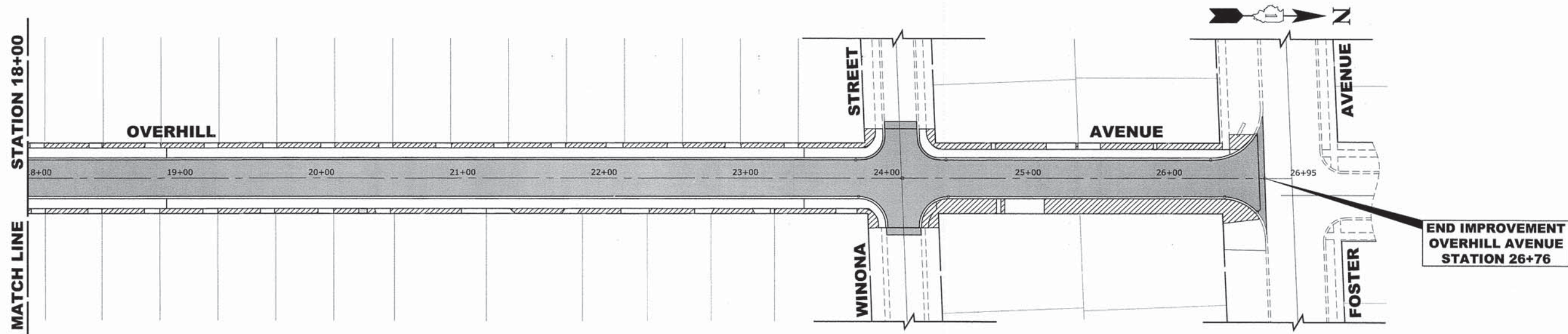
**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

**ASPHALT AND LANDSCAPING PLAN**

SCALE: 1" = 40' SHEET NO. 1 OF 2 SHEETS STA. 1+00 TO STA. 18+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	38
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

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**LANDSCAPING NOTES**

**SODDING:**

ALL DISTURBED AREAS INDICATED ON CROSS SECTION OR LANDSCAPING PLANS TO BE SODDED, INCLUDING FERTILIZER NUTRIENTS\*, AND SUPPLEMENTAL WATERING WHICH IS TO BE USED AFTER THE INITIAL WATERING (AS SPECIFIED AND AS DIRECTED BY THE ENGINEER) AT THE RATE OF 10 GALLONS PER SQUARE YARD SODDING. SODDED SLOPES WHICH ARE 2:1 OR STEEPER SHALL BE STAKED. (PROVIDE A 3" STRIP AROUND CATCH BASINS IN SEEDED AREAS).

**FERTILIZER NUTRIENTS\* FOR SODDING:**

USE A FERTILIZER WITH A 1:1:1 RATIO PER ACRE:

- 60 LBS. NITROGEN FERTILIZER NUTRIENT
- 60 LBS. PHOSPHORUS FERTILIZER NUTRIENT
- 60 LBS. POTASSIUM FERTILIZER NUTRIENT

TOTAL: 180 LBS. PER ACRE

**4" TOP SOIL LAYER:**

SOIL SHALL BE SPREAD AS INDICATED ON THE TYPICAL CROSS SECTIONS.

A MINIMUM OF GRADING TO BE PERMITTED WITHIN AN APPROXIMATE RADIUS OF 6' FROM ALL TREES TO BE SAVED AS DETERMINED BY THE ENGINEER.

**LEGEND OF SYMBOLS**

SYMBOL	DESCRIPTION
	PROPOSED CONCRETE AREA
	PROPOSED SODDED PARKWAY
	PROPOSED HOT-MIX ASPHALT AREA

Drawing file: W:\Projects\_by\_Village\Norridge\61213232 - Overhill Ave Ph II\35-41\_Overhill\_40pcole.dwg Oct 02, 2014 - 4:35pm



USER NAME -	DESIGNED - SBC/CB	REVISED - IDOT REVIEW
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PLOT SCALE -	CHECKED - WOP	REVISED -
PLOT DATE -	DATE - 10/02/14	REVISED -

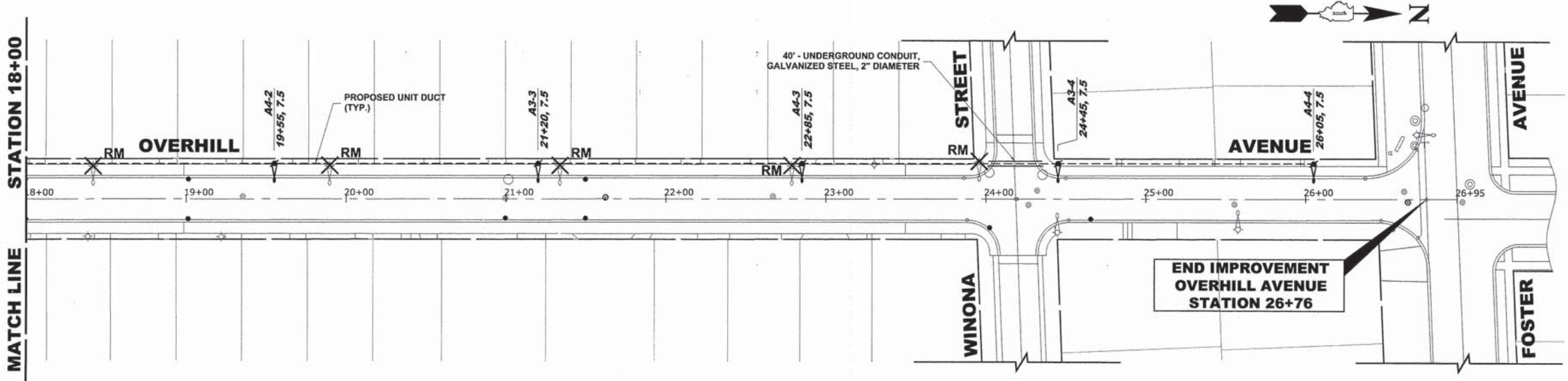
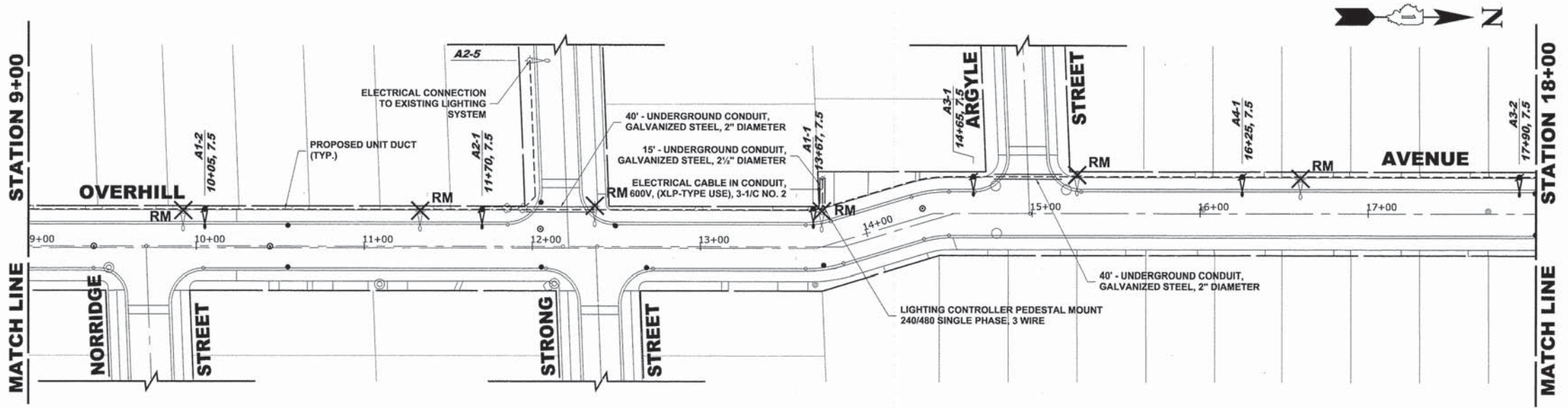
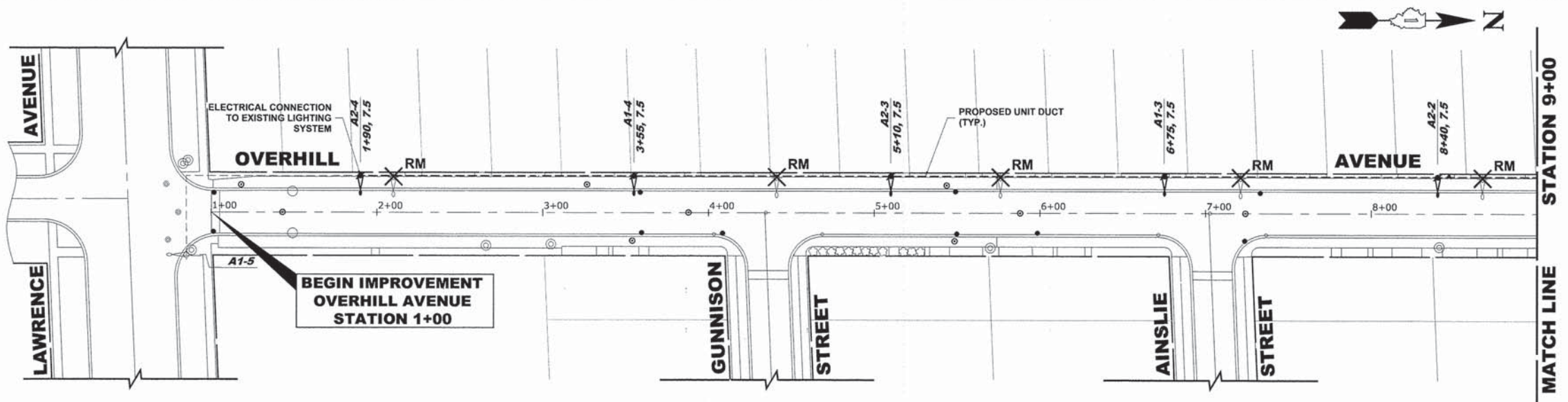
**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

**ASPHALT AND LANDSCAPING PLAN**

SCALE: 1" = 40' SHEET NO. 2 OF 2 SHEETS STA. 18+00 TO STA. 26+88

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	39
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

E.H.E. PROJECT NO. 612-13-23201



LEGEND OF SYMBOLS	
	PROPOSED CONTROL CENTER
	EXISTING LIGHT POLE AND FIXTURE
	EXISTING LIGHT POLE AND FIXTURE TO BE REMOVED
	PROPOSED 30' MOUNTING HEIGHT POLE WITH 8' MAST ARM AND 250 WATT MH LAMP
	PROPOSED UNIT DUCT WITH 3-1/C No. 6 & 1-1/C No. 8 GROUND, 600V (XLP-TYPE USE), 1 1/4\"/>
	PROPOSED GALVANIZED STEEL CONDUIT
	CONTROLLER IDENTIFICATION (TYP.)
	CIRCUIT NUMBER (TYP.)
	POLE NUMBER (TYP.)
	SET BACK FROM BACK OF CURB (TYP.)
	STATION NUMBER (TYP.)

**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants  
 Established 1911

USER NAME -	DESIGNED - SBC/CB	REVISED - IDOT REVIEW
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

**STREET LIGHTING PLAN**

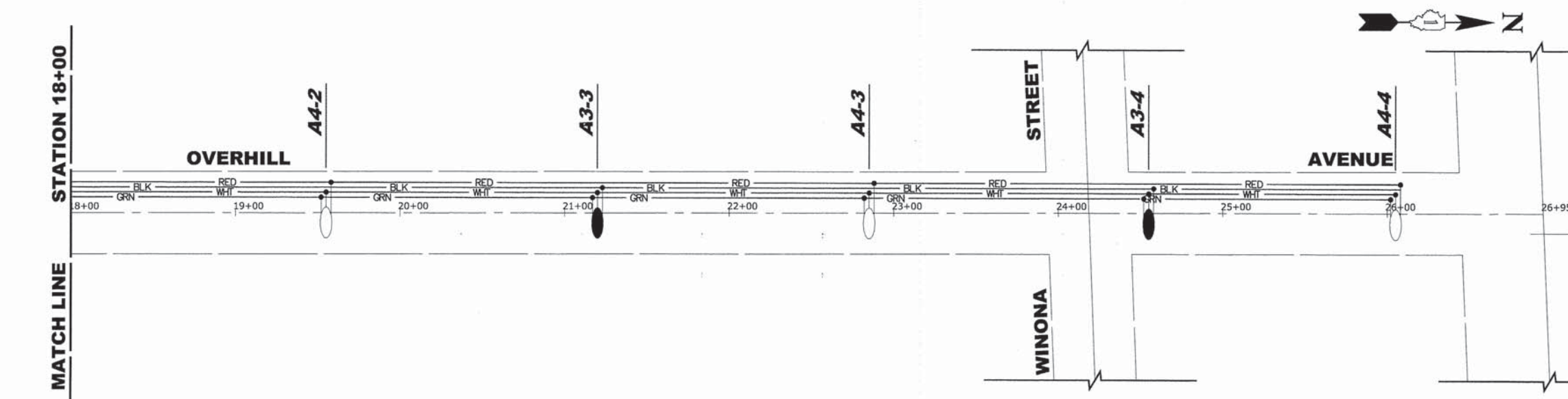
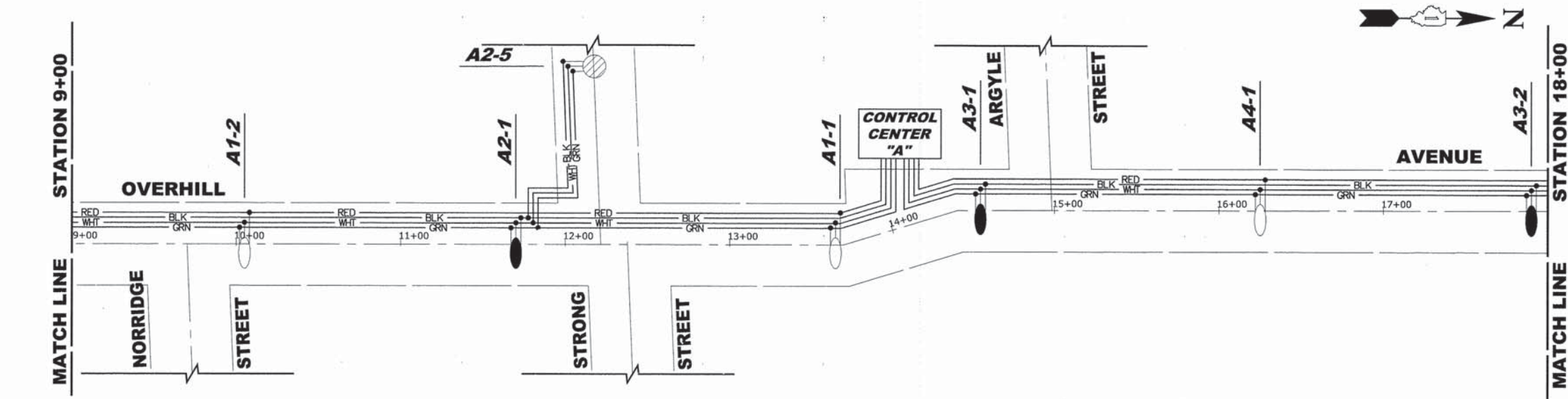
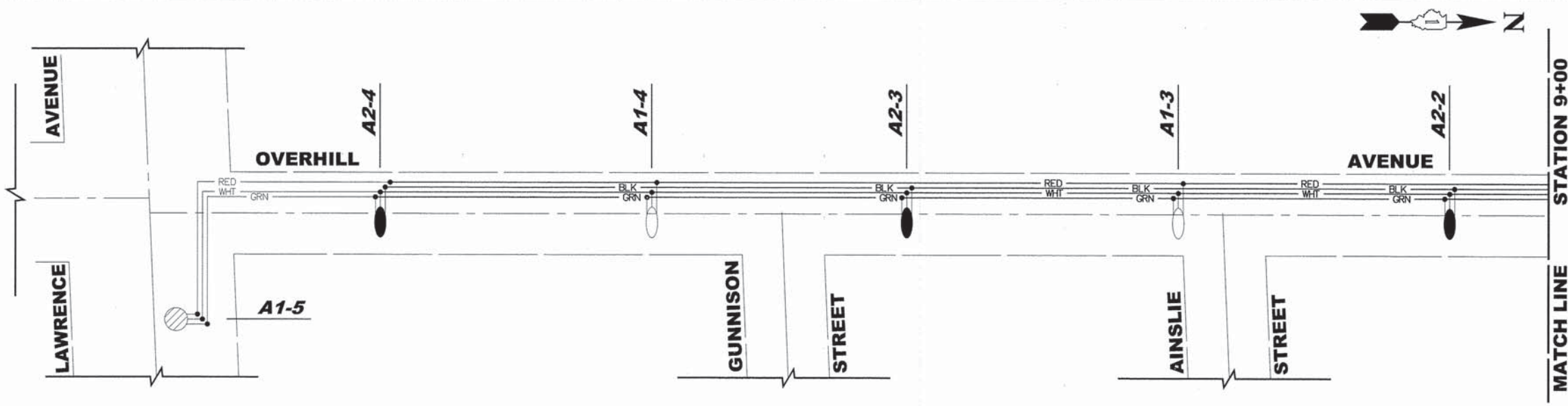
SCALE: 1" = 40' SHEET NO. 1 OF 1 SHEETS STA. 1+00 TO STA. 26+88

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FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

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Drawing file: W:\Projects\_by\_Village\Norridge\61213232 - Overhill Ave Ph II\35-41\_Overhill\_40isc06.dwg Oct 16, 2014 - 2:38pm



LEGEND OF SYMBOLS	
— BLK —	240 VOLT PHASE CABLE (BLACK)
— RED —	240 VOLT PHASE CABLE (RED)
— WHT —	240 VOLT NUETRAL CABLE (WHITE)
— GRN —	GROUND WIRE (GREEN)
●	SPLICE
○	PROPOSED 250 WATT MH ROADWAY LUMINAIRE (240 VOLT) ON BLACK WIRE
○	PROPOSED 250 WATT MH ROADWAY LUMINAIRE (240 VOLT) ON RED WIRE
⊗	EXISTING ROADWAY LUMINAIRE
<b>A1-1</b>	PROPOSED CIRCUIT/POLE NUMBER

**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants  
 Established 1911

USER NAME -	DESIGNED - SBC/CB	REVISED - IDOT REVIEW
	DRAWN - MK,DMM,ECW	REVISED -
PLOT SCALE -	CHECKED - WOP	REVISED -
PLOT DATE -	DATE - 10/02/14	REVISED -

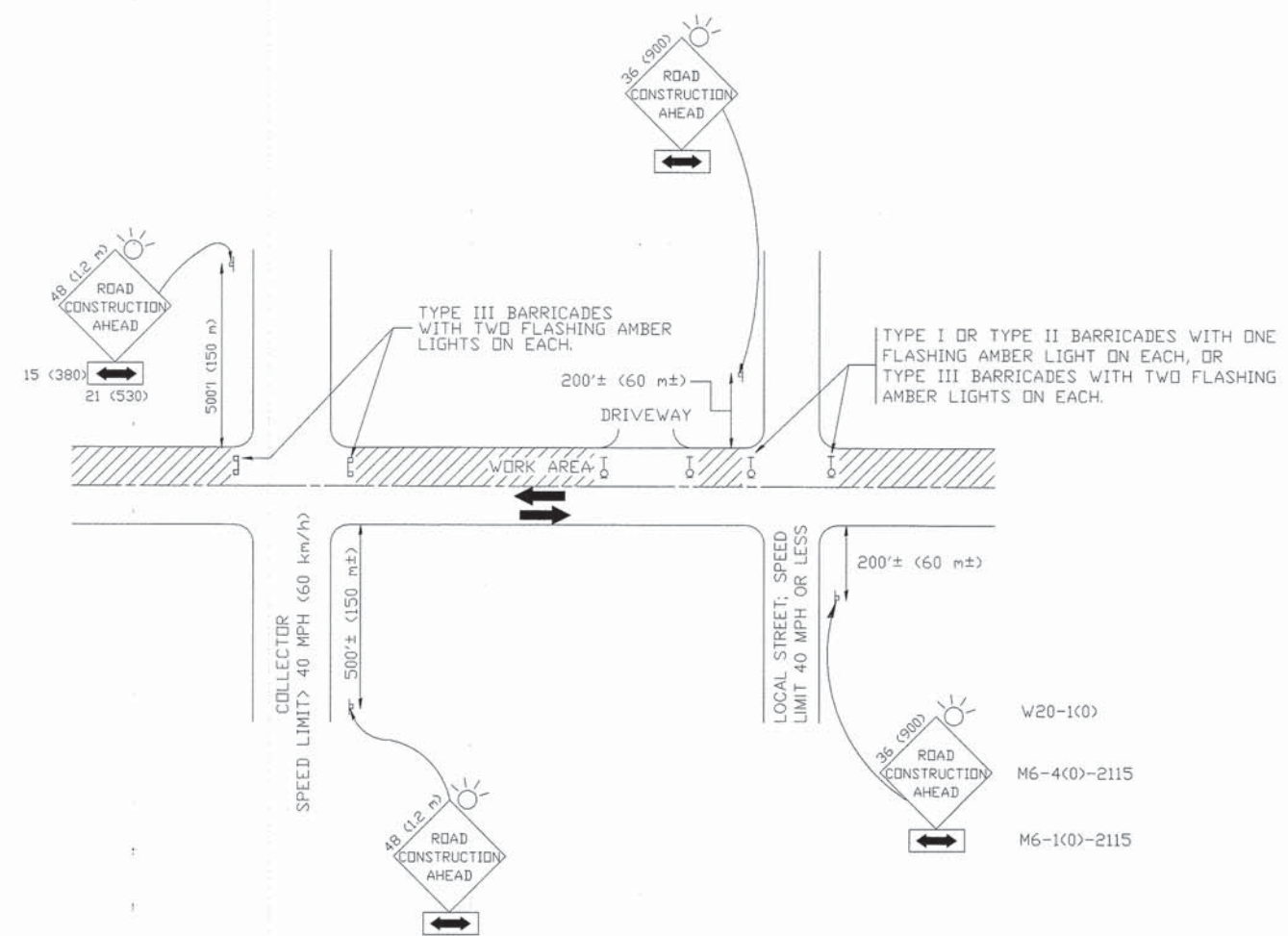
**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

**LIGHTING WIRING DIAGRAM**

SCALE: NONE      SHEET NO. 1 OF 1 SHEETS      STA. 1+00      TO STA. 26+88

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	13-00064-00-FP	COOK	62	41
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

Drawing file: W:\Projects\_by\_Village\Norrige\61213232 - Overhill Ave Ph II\42-TC-10.dwg Oct 03, 2014 - 8:45am



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

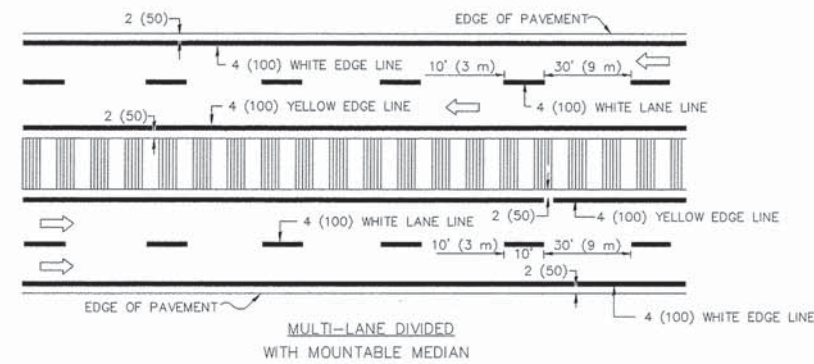
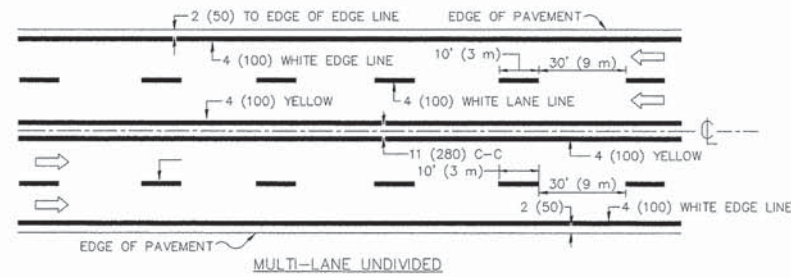
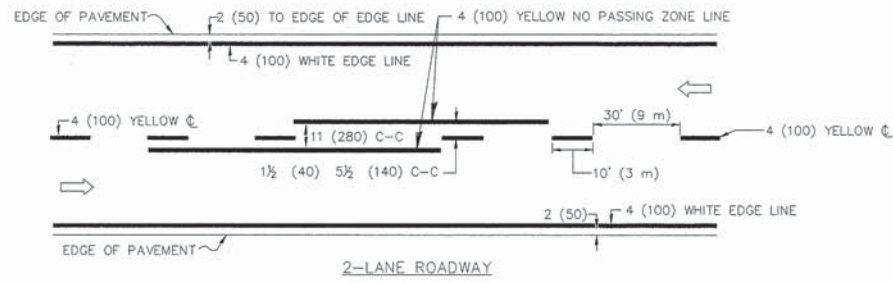
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	DRAWN -	REVISED - A. HOUSEH 03-06-96
PLOT SCALE =	CHECKED -	REVISED - A. HOUSEH 10-15-96
PLOT DATE =	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

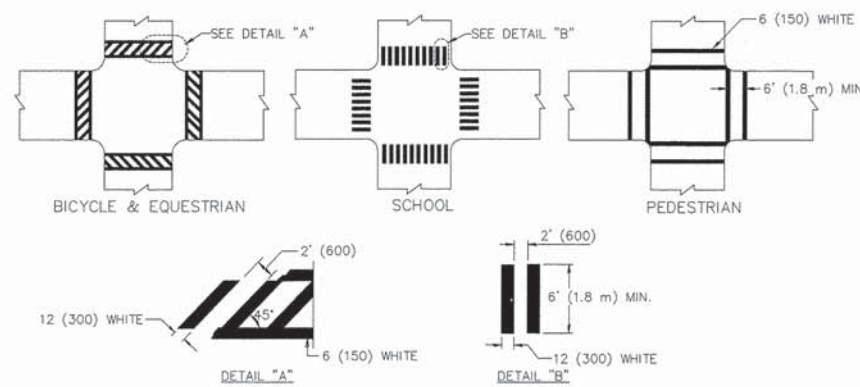
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MS RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

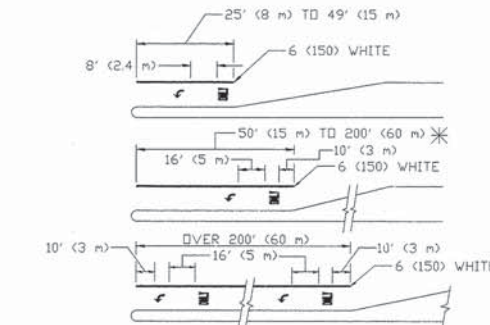
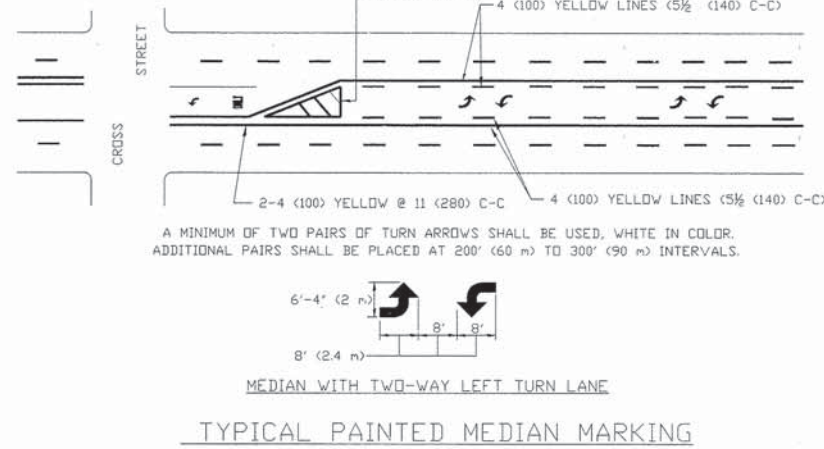
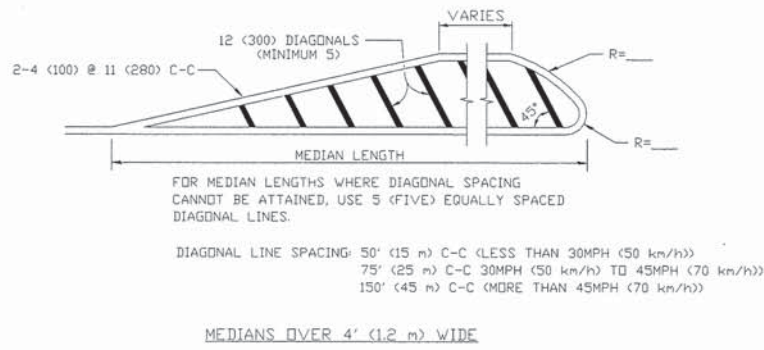
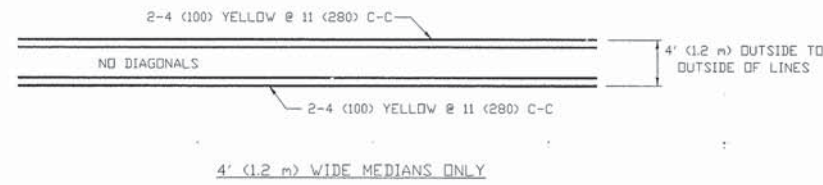


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



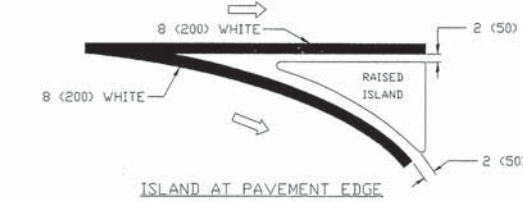
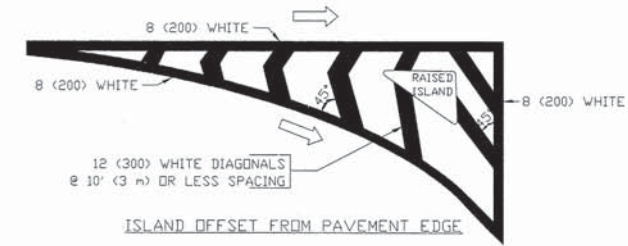
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.

\* AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW TWO WAY TRAFFIC WHITE ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

Drawing file: W:\Projects\_by\_village\NorrIDGE\61213232 - Overall Ave Ph. II\43-TC-13.dwg Oct. 03, 2014 - 8:46am

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		DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

MS RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-0004-00-FP	COOK	62	43
TC-13		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 612-13-23201

**ROUTE MARKERS**

FOR U.S. ROUTES  
M1-40-2424

FOR ILLINOIS ROUTES  
M1-50-2424

R.R. UNMARKED ROUTES  
SPECIAL 24" x 18" VARIABLE  
4" BLACK LETTERS ON WHITE  
REFLECTIVE BACKGROUND

**ARROWS SIGNS**

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-1-2115

M6-3-2115

**CARDINAL DIRECTION & DETOUR SIGNS**

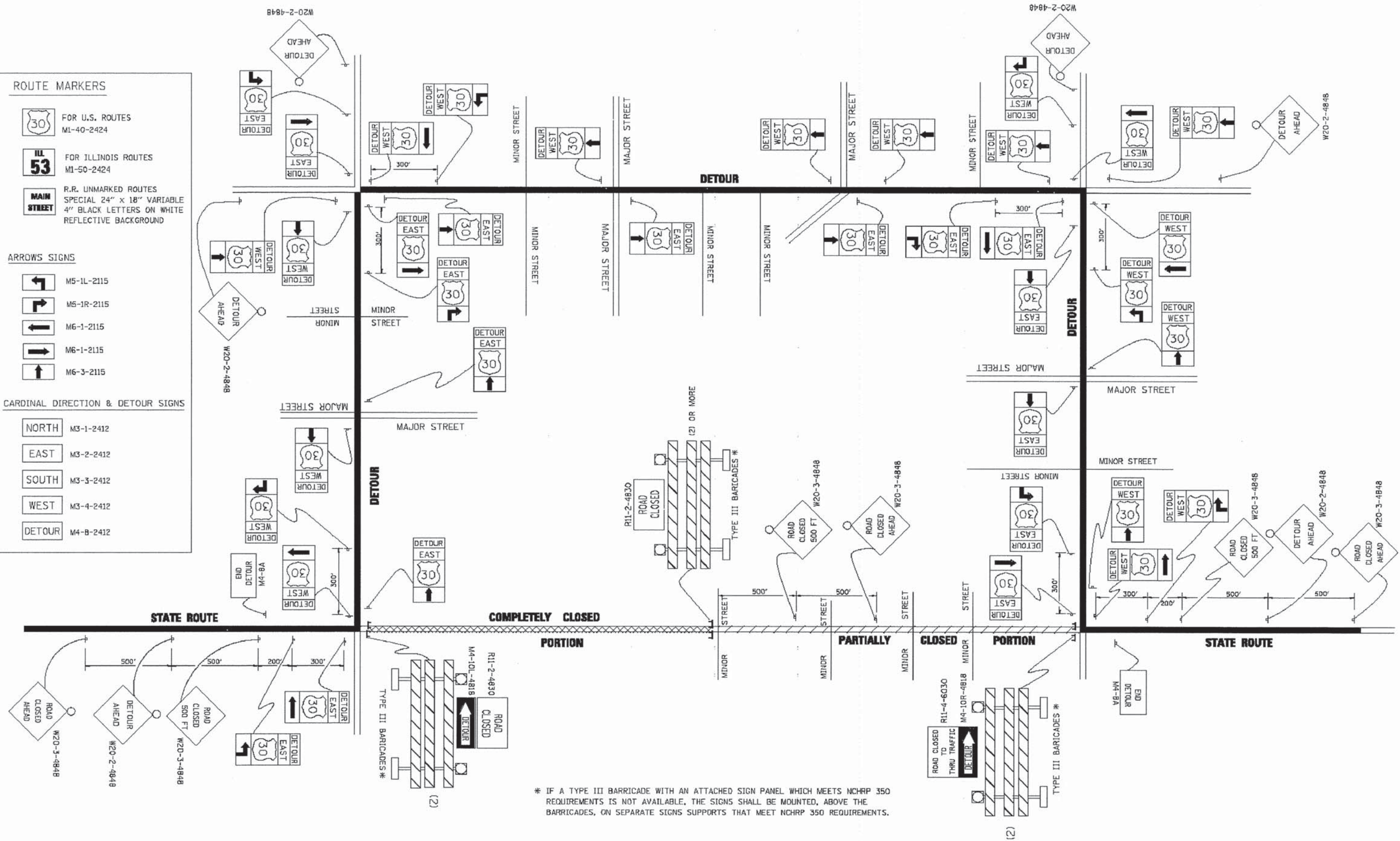
NORTH M3-1-2412

EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

DETOUR M4-B-2412



\* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

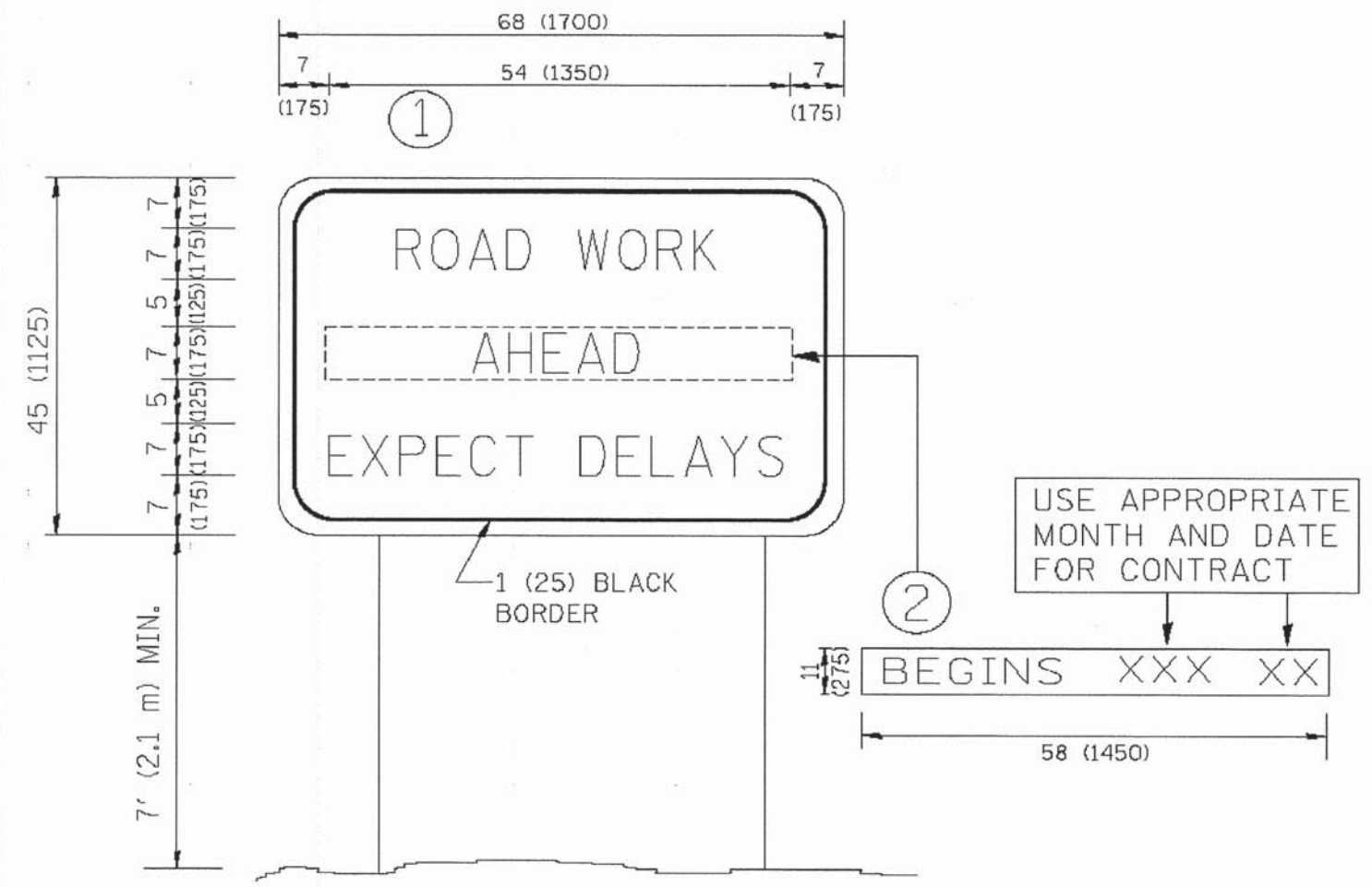
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		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETOUR SIGNING FOR CLOSING STATE HIGHWAYS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-PP	COOK	62	44
<b>TC-21</b>		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

Drawing file: W:\Projects\_by\_Village\Norridge\61213322 - Overhill Ave Ph II\45-TC-22.dgn Oct. 03, 2014 - 8:49am



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

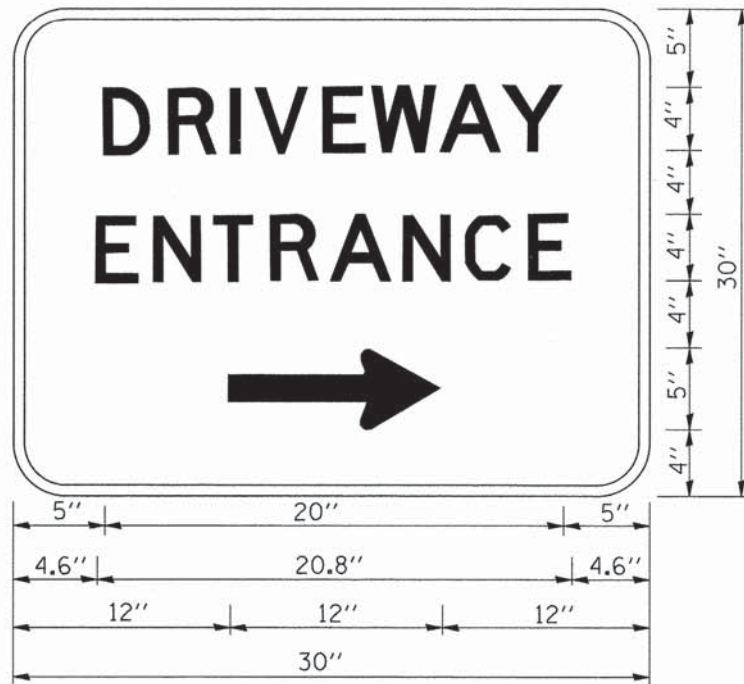
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		DRAWN -	REVISED - R. MIRS 12-11-97
		CHECKED -	REVISED - T. RAMMACHER 02-02-99
		DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
----	13-00064-00-FP	COOK	62	45
<b>TC-22</b>		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

Drawing file: W:\Projects\by\_village\Norridge\61213232 - Overhill Ave Ph II\46-TC-26.dwg Oct 03, 2014 - 8:50am

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	PLOT DATE = 1/4/2008	DATE -	REVISED -

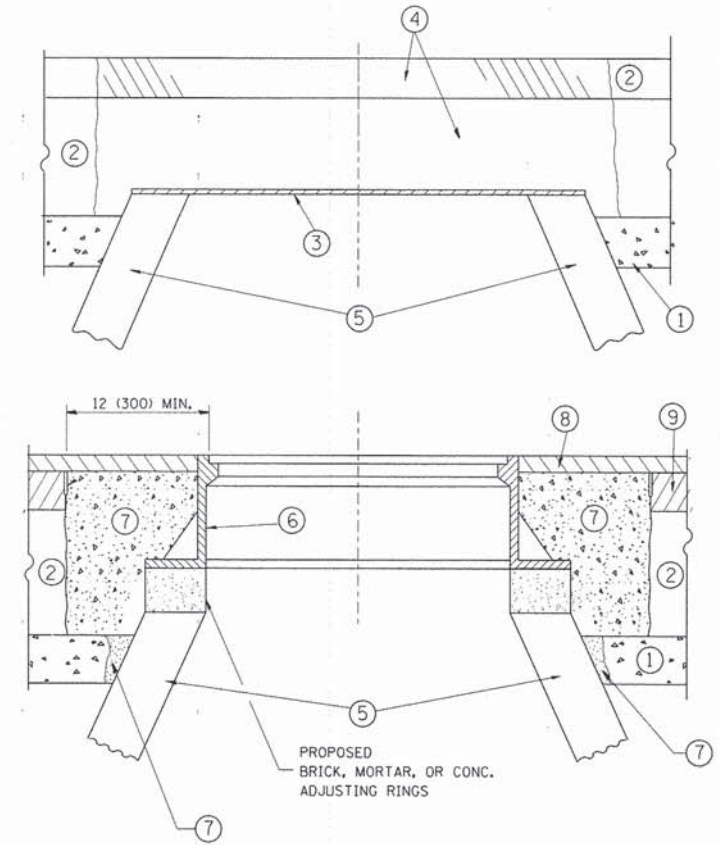
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY ENTRANCE SIGNING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	46
<b>TC-26</b>		<b>CONTRACT NO. 61A82</b>		

Drawing file: W:\Projects\by\_Village\Warridge\61212322 - Overhill Ave Ph II\47-BD\_08.dwg Oct 03, 2014 - 8:51am  
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**CONSTRUCTION PROCEDURES**

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
  - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
  - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
  - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- \* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- |  |                               |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL                 | ⑥ FRAME AND LID (SEE NOTES)   |
| ② EXISTING PAVEMENT                          | ⑦ CLASS PP-1* CONCRETE        |
| ③ 36 (900) DIAMETER METAL PLATE              | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE  |
| ⑤ EXISTING STRUCTURE                         |                               |

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

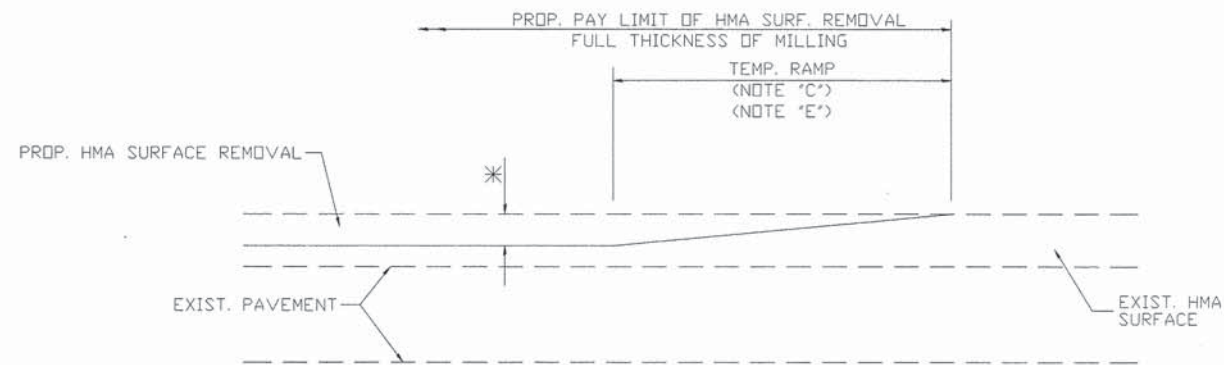
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

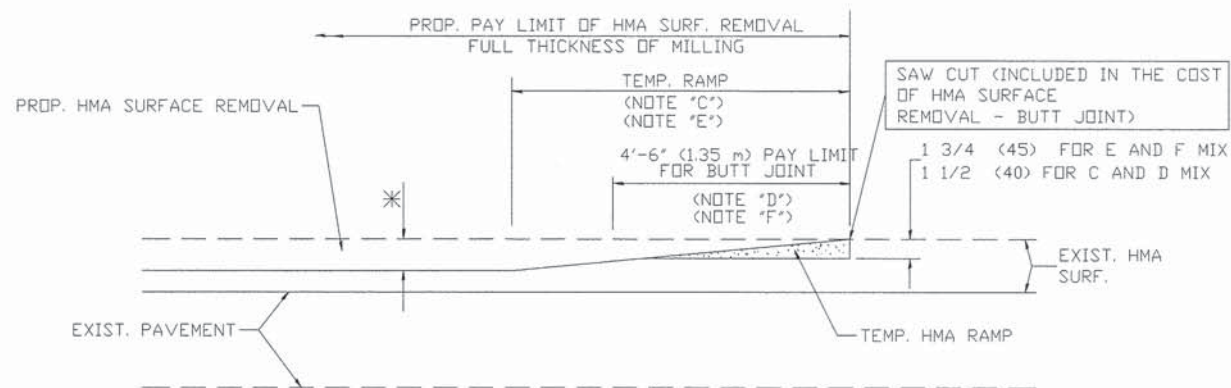
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = baerd	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>		F.A. RTE. =	SECTION =	COUNTY =	TOTAL SHEETS =	SHEET NO. =	
		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	-	13-00064-00-FP		62	47
		PLOT SCALE = 1/8" = 1'-0"	REVISED - R. BORO 03-09-11						<b>BD600-03 (BD-8)</b>			
		PLOT DATE = 12/6/2011	REVISED - R. BORO 12-06-11						FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT		



MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

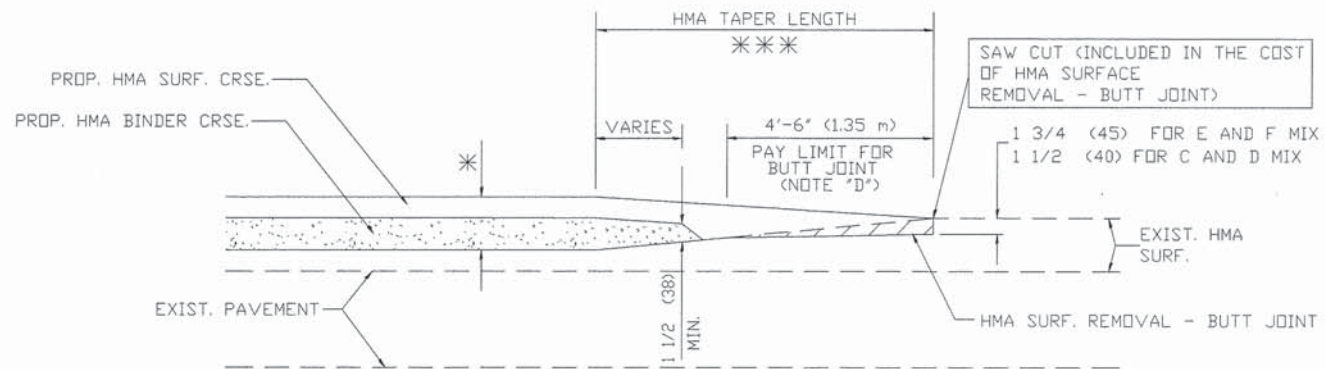
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

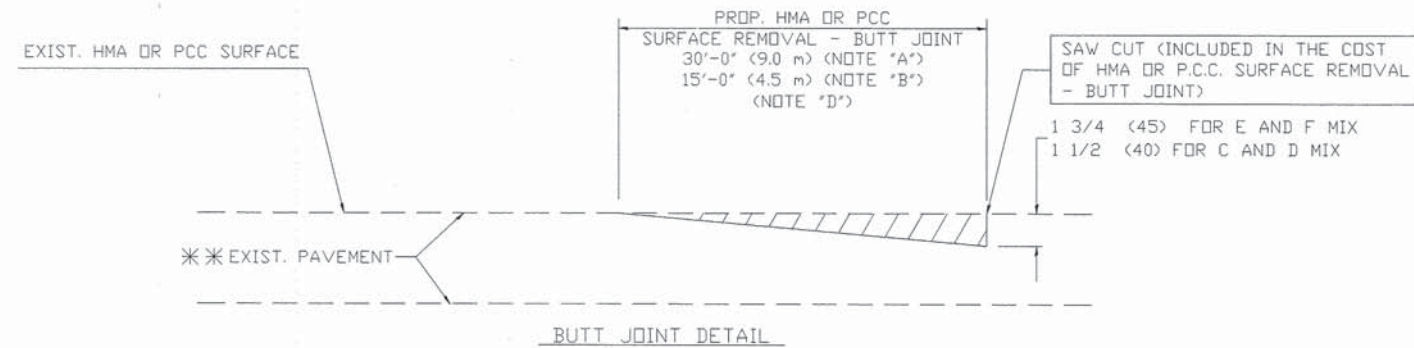
OPTION 2

TYPICAL TEMPORARY RAMP

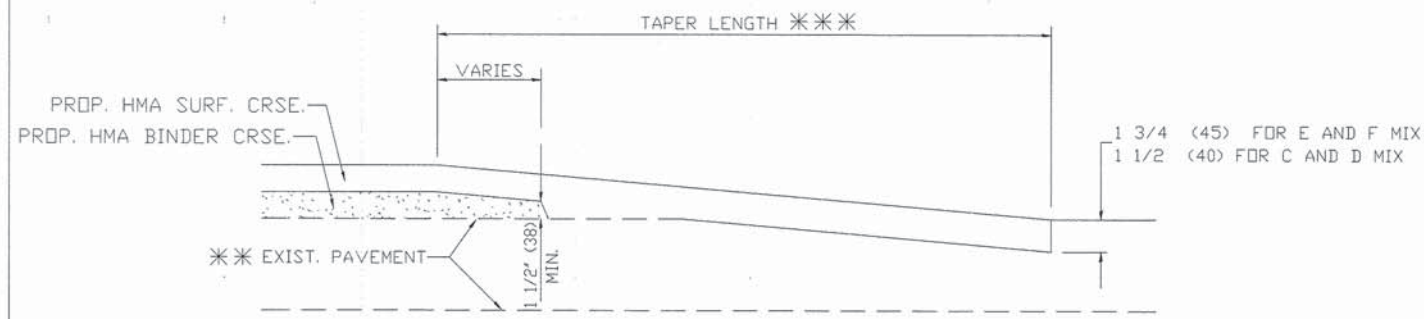


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE 'A')  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE 'B')

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

Drawing file: W:\Projects\by\_village\Norridge\61213232 - Overhill Ave Ph 1\48-BD-32.dwg Oct 03, 2014 - 8:55am

USER NAME =	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
PLOT SCALE =	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT DATE =	CHECKED -	REVISED - M. GOMEZ 04-06-01
	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
---	13-00064-00-FP	COOK	62	48
BD400-05 (BD-32)		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 612-13-007



0.125 INCH (3.175) TYPE 5052-H32 ALUMINUM ENCLOSURE. FOR INSIDE DIMENSION SEE NOTE 1.

1/2" (12.7) DIA. MINIMUM DOOR HANDLE WITH PADLOCK PROVISION SEALED WITH NEOPRENE GASKET

NEMA 3R ENCLOSURE

4" (101.6) RIGID ALUMINUM SCHEDULE 40 CONDUIT STEM ANCHORED TO 356-T6 CAST ALUMINUM PEDESTAL BASE.

HANDHOLE

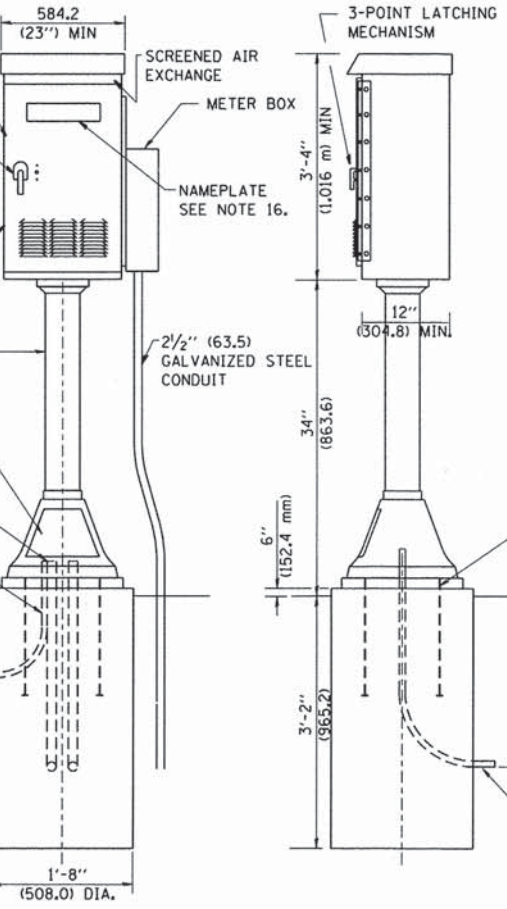
3" (76.2) RACEWAY PROJECTION

1" (25.4) STEEL CONDUIT

1'-0" (304.8)

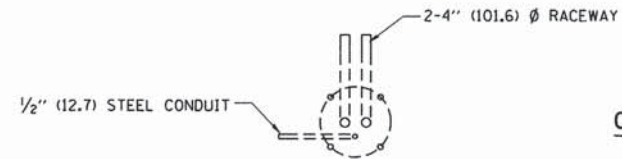
CLASS-X CONCRETE

5/8" (15.875) X 10' (3,048 m) GROUND ROD WITH EXOTHERMIC WELD GROUND WIRE SHALL MEET ASTM STANDARDS AND SHALL NOT BE LESS THAN NO. 2



FRONT

SIDE

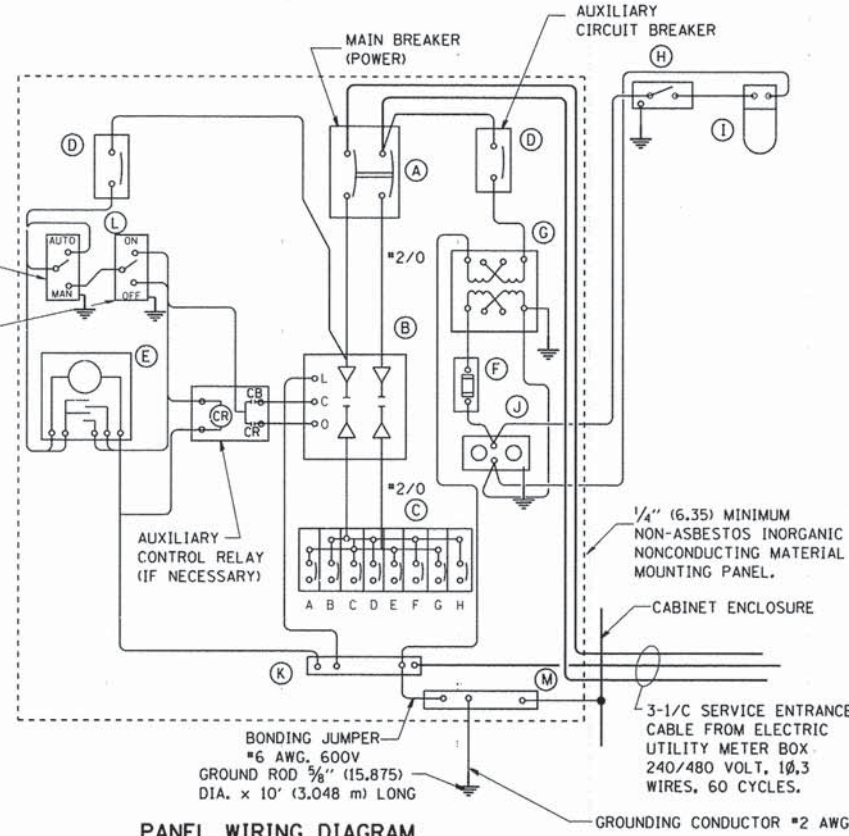


FOUNDATION PLAN

CONTROL CABINET

TOGGLE SWITCH MOMENTARY CONTACT TYPE SPDT 20 A, 240V AC AND TOGGLE SWITCH 20A, 240V, TYPE SPDT

TWO POSITION TOGGLE SWITCH



PANEL WIRING DIAGRAM

PANEL EQUIPMENT

BILL OF MATERIAL		
ITEM	QUANTITY	DESCRIPTION
A	1	MAIN CIRCUIT BREAKER, 2 POLE, 600 VOLT 100 AMP. FRAME, 100 AMP. NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA-22000 AMP. AT 480 VOLT.
B	1	REMOTE CONTROL SWITCH, ELECTRICALLY OPERATED, MECHANICALLY HELD, 2 POLE, SINGLE THROW, 100 AMP., 600 VOLTS CONTROL CIRCUIT 240 VOLT.
C	8	CIRCUIT BREAKERS, 1 POLE, 277V., 100 AMP., FRAME 50 AMP. NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA-10,000 AMP. AT 240 V.
D	2	CONTROL CIRCUIT-CIRCUIT BREAKER, 1 POLE, 240 V., 100 AMP. FRAME, 15 AMP. NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA-5000 AMP. AT 240 V.
E	1	ASTRONOMIC MICROPROCESSOR-BASED 2-CHANNEL CONTROLLER [TIME SWITCH].
F	1	20 A., 120 V. FUSE.
G	1	1.5 KVA, SINGLE PHASE, ENCAPSULATED TRANSFORMER 240 X 480 / 120 X 240 VOLT, 60 Hz.
H	1	SPST 20A SWITCH ON DOOR, TO TURN LIGHT ON WHEN DOOR IS OPEN.
I	1	INCANDESCENT LIGHTING FIXTURE ENCLOSED AND GASKETED WITH 60 WATT, 120 V. LAMP.
J	1	20 A., 120 V., DUPLEX RECEPTACLE, GFCI.
K	1	COPPER GROUND BUS 1/4" (6.35) X 1" (25.4) X 12" (304.8 mm) LONG MOUNTED ON PANEL WITH LUGS AND 4 SPARE LUGS
L	1	TOGGLE SWITCHES MOUNTED IN 4" (101.6) X 4" (101.6 mm) BOX.
M	1	COPPER GROUND BUS 1/4" (6.35) X 1" (25.4) X 12" (304.8 mm) LONG MOUNTED ON PANEL WITH LUGS AND SPARE LUGS

NOTES:

- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- UNLESS OTHERWISE INDICATED, THE CABINET SHALL BE MOUNTED ATOP A 4-INCH (101.6 mm) RIGID ALUMINUM SCHEDULE 40 CONDUIT STEM ANCHORED TO A CAST ALUMINUM PEDESTAL BASE.
- IN FRONT OF CONTROL CABINET DOOR, REMOVE VEGETATION AND 2" (50.8 mm) TOP SOIL. LEVEL THE AREA AND ON TOP, PLACE LENGTH WISE PARALLEL TO CONTROL CABINET, A CONCRETE PAD 36" (914.4 mm) X 60" (18,288 m) X 4" (101 mm) MIN. SIZE. THE COST OF LABOR AND MATERIALS ARE INCLUDED IN THE COST OF THE CONTROLLER.
- DOOR SHALL BE CONSTRUCTED FROM SAME TYPE OF MATERIAL AND THICKNESS AS CABINET.
- DOOR SHALL BE EQUIPPED WITH THREE POINT LATCHING MECHANISM WITH NYLON ROLLERS AT TOP THE BOTTOM.
- DOOR HINGE SHALL BE A HEAVY GAUGE CONTINUOUS HINGE WITH A 1/4" (6.35 mm) DIA. STAINLESS STEEL HINGE PIN.
- ALL EXTERNAL HARDWARE SHALL BE STAINLESS STEEL.
- CONTROL WIRING TO BE #12 AWG, 600V, TYPE "SIS" GRAY SWITCH BOARD WIRE, STRANDED COPPER.
- METER BOX SHALL BE MOUNTED ON THE SIDE OF CONTROL CABINET, NEAR TO THE SERVICE POLE.
- CABINETS SHALL BE PRIMED AND PAINTED AS SPECIFIED.
- THE HEADS OF CONNECTORS SCREWS SHALL BE PAINTED WHITE FOR NEUTRAL BAR CONNECTION AND GREEN FOR GROUND BAR CONNECTORS.
- ALL WIRING WITHIN THE CABINET SHALL BE COLOR CODED AS INDICATED.  
R = RED      BL = BLUE      W = WHITE  
B = BLACK      Y = YELLOW      G = GREEN
- PROVIDE SEALING GROMMETS FOR ALL OPEN WIRING EXTENDED FROM DEVICES IN BOXES OR CABINETS WITHIN THE CONTROL CABINET.
- ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.
- THE CONTROLLER SHALL BE CONSTRUCTED TO U.L. STD. 508 AND BEAR THE U.L. LABEL "ENCLOSED INDUSTRIAL CONTROL PANEL".
- 12" (304.8) X 16" (406.4 mm) STAINLESS STEEL EXTERIOR NAMEPLATE SHALL BE ENGRAVED TO "STATE OF ILLINOIS LIGHTING CONTROLS" UNLESS OTHERWISE SPECIFIED.

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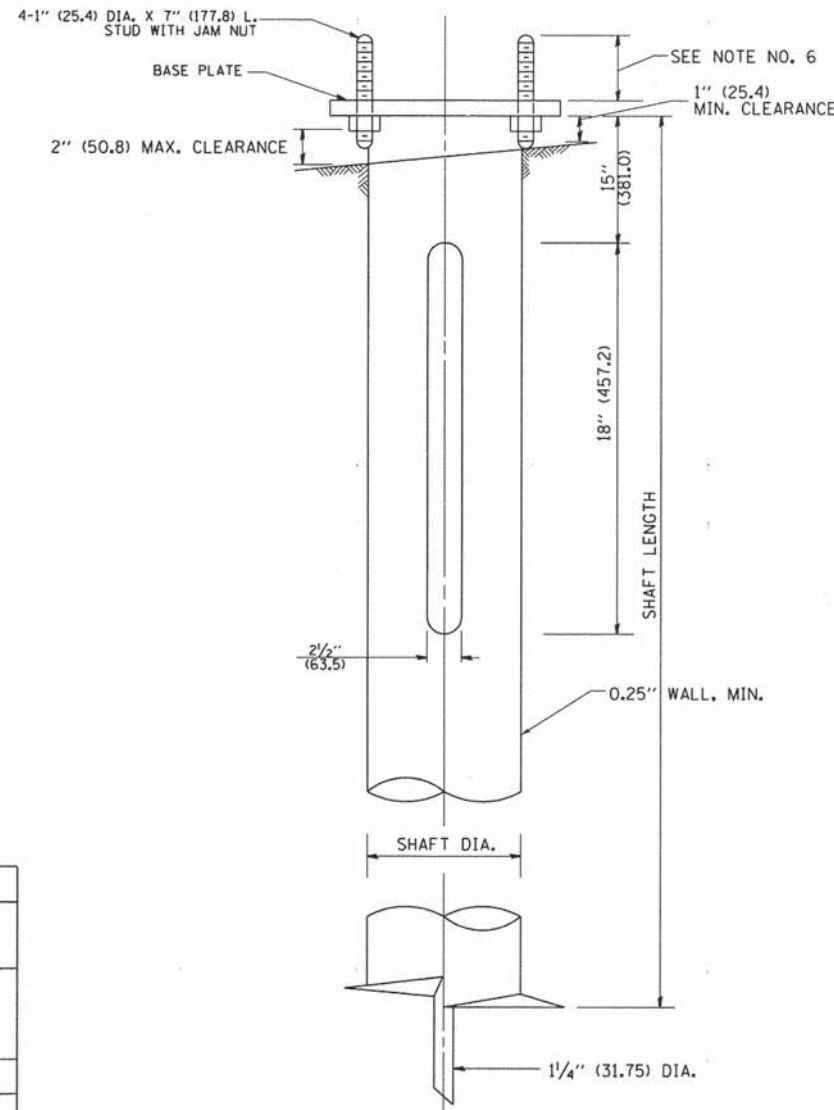
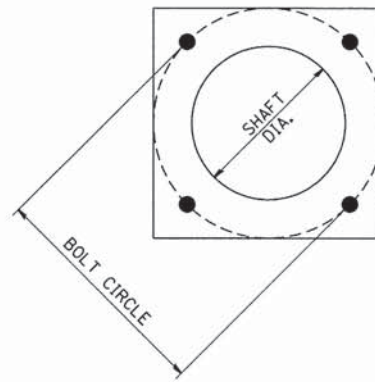
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		DRAWN -	REVISED - R. TOMSONS 08-19-04
	PLOT SCALE = 50,0000 ' / IN.	CHECKED - R. GUPTA	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

LIGHTING CONTROLLER  
PEDESTAL MOUNT

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	13-00064-00-FP		62	49
BE-210		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. ALL DIMENSION IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
2. ALL MATERIAL SHALL BE GALVINIZED ACCORDING TO AASHTO M111, UNLESS OTHERWISE SPECIFIED.
3. ALL WELDS SHALL BE CONTINUOUS AND NOT LESS THAN 1/4" (6.35 mm) FILLET WELDS. THE WELDED FOUNDATION SHALL BE CAPABLE OF WITHSTANDING 10,000 FT/LBS (13558.18 n.m) OF INSTALLATION TORQUE APPLIED ABOUT THE AXIS OF THE FOUNDATION.
4. THE HELIX FOUNDATION SHAFT SHALL BE INSTALLED VERTICAL AND THE BASE PLATE SHALL BE IN LEVEL. THE BREAKAWAY COUPLINGS AND HARDWARE SHALL NOT BE USED TO ALIGN THE POLE INSTALLATION.
5. THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE INSTALLATION OF THE LIGHT POLE.
6. THE CONTRACTOR SHALL COORDINATE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF THE BASE PLATE WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS.
7. ANY VOIDS WITHIN THE METAL FOUNDATION SHALL BE FILLED WITH FINE AGGREGATE.
8. METAL FOUNDATIONS SHALL BE INSTALLED IN UNDISTURBED SOIL. PREDRILLING A PILOT HOLE AND/OR BACKFILLING AROUND THE FOUNDATION IS NOT ALLOWED.
9. THE METAL FOUNDATION SHALL NOT BE INSTALLED TO A TORQUE WHICH EXCEEDS THE MANUFACTURER'S MAXIMUM TORQUE RATING NOR SHALL IT BE INSTALLED TO AN INSTALLATION TORQUE VALUE OF LESS THAN 3,500 FT LB (4,750 KNM). METAL FOUNDATIONS THAT ARE NOT INSTALLED TO FULL INSTALLATION DEPTH OR DO NOT ACHIEVE THE MINIMUM INSTALLATION TORQUE SHALL BE REMOVED AND REPLACED WITH A CONCRETE FOUNDATION AT NO ADDITIONAL COST.
10. THE BASEPLATE SHALL BE PERPENDICULAR TO THE SHAFT AXIS ( $\pm 1^\circ$ ) AND THE HOLE CENTERLINE SHALL BE CONCENTRIC ( $\pm 0.188$ ) TO THE SHAFT AXIS.
11. THE PILOT POINT AND SHAFT AXIS SHALL BE CONCENTRIC ( $\pm 0.125$ ) AND IN LINE ( $\pm 2^\circ$ ).
12. THE BASEPLATE SHALL BE STAMPED WITH THE MANUFACTURERS NAME AND DATE OF MANUFACTURE.

**HELIX FOUNDATION SIZE**

POLE MOUNTING HEIGHT	BOLT CIRCLE	SHAFT DIAMETER	SHAFT LENGTH	BASEPLATE
30 FT.	11 1/2"	8 5/8"	6 FT.	12"x12"x1"
31 FT.-35 FT.	11 1/2"	8 5/8"	6 FT.	12"x12"x1"
36 FT.-40FT.	15"	8 5/8"	6 FT.	15"x15"x1 1/4"
41 FT.-45 FT.	15"	8 5/8"	6 FT.	15"x15"x1 1/4"
46 FT.-50 FT.	15"	10"	8 FT.	15"x15"x1 1/4"

**METAL HELIX FOUNDATION MATERIALS**

ITEM	MATERIAL REQUIREMENT
BASEPLATE	AASHTO M 270M, GRADE 36 (M270M, GRADE 250)
SHAFT	ASTM A 252, GRADE 2 (PHOSPHOROUS 0.04% MAXIMUM, SULFUR 0.05% MAXIMUM)
HELIX SCREW	AASHTO M 183 (ASTM A 635)
PILOT POINT	AASHTO M 270 (ASTM A 575)
ANCHOR RODS/STUDS	AASHTO M 314 (ASTM F 1554)
HEXAGON NUTS	AASHTO M 291M (ASTM A 563) GRADE DH, OR AASHTO M 292 (ASTM A 194) GRADE 2H
WASHERS	AASHTO M 293 (ASTM F 436)

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PLOT DATE = 1/4/2008			

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

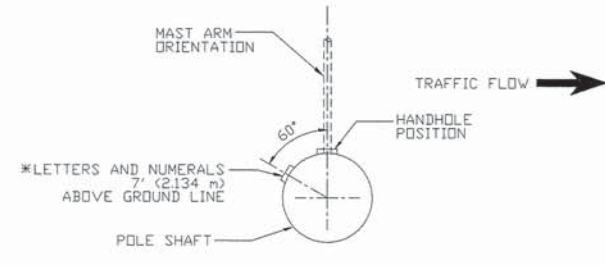
**LIGHT POLE FOUNDATION, METAL**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

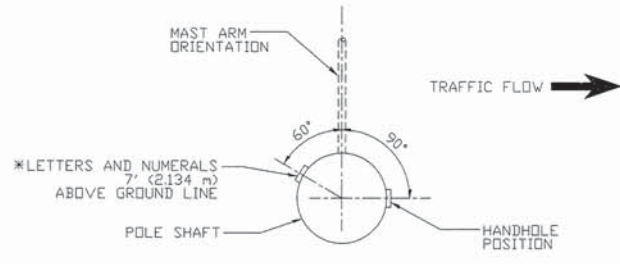
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	50
BE-305		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 612-13-23201

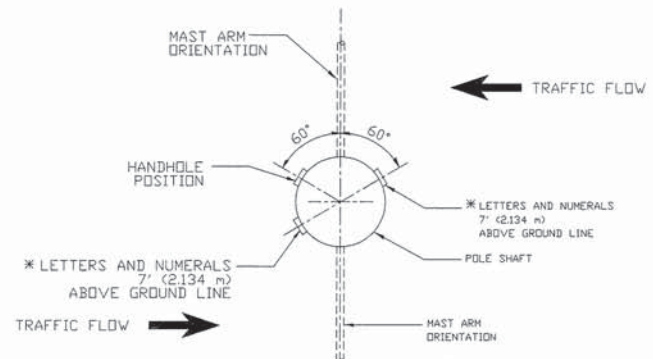
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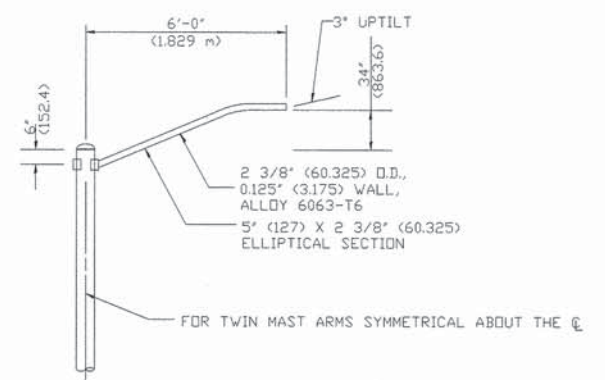
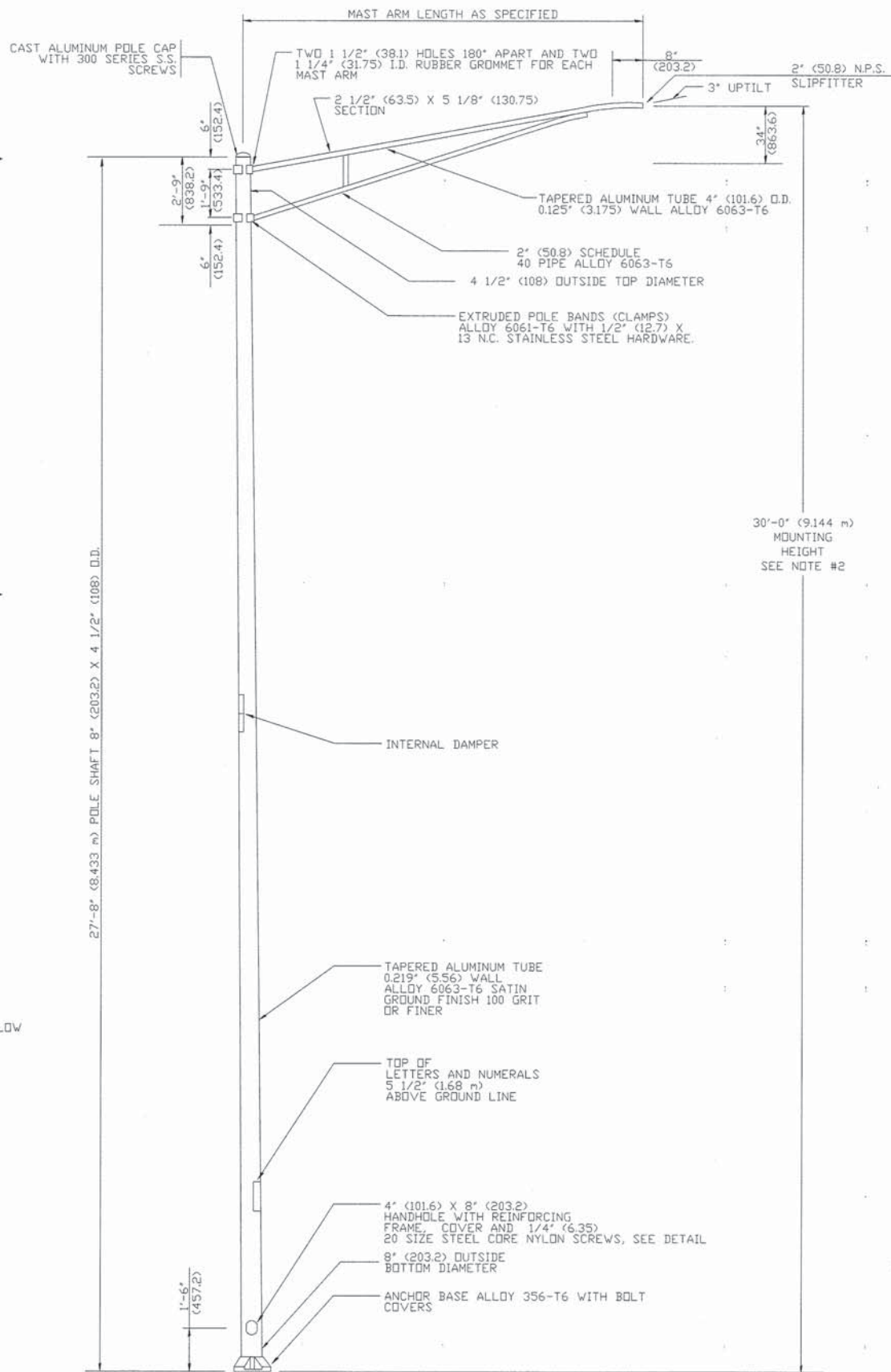
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES MOUNTED ON BRIDGE PARAPET OR BARRIER WALL



POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES

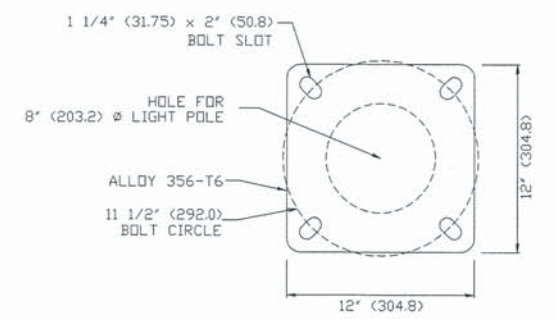


POSITION OF HANDHOLE AND POLE NUMBER FOR TWIN MAST ARM POLES

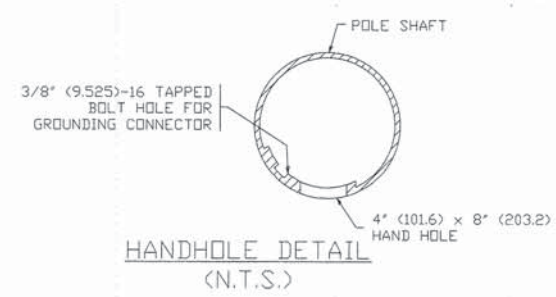


6' (1.8 m) SINGLE MEMBER MAST ARM (N.T.S.)

- NOTES:**
1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
  2. MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENDON TO THE BOTTOM OF THE ANCHOR BASE.
  3. THE LIGHT POLE WILL MEET AASHTO DESIGN CRITERIA AS SPECIFIED.
  4. THE INSTALLING CONTRACTOR WILL PROVIDE A UL LISTED GROUNDING CONNECTOR, BURNDY K2C23, T&B SP4DL OR APPROVED EQUAL.
  5. LIGHT POLES WILL NOT BE INSTALLED WITHOUT MAST ARMS AND LUMINAIRES.
  6. LIGHT POLES WILL BE SET PLUMB ON THE FOUNDATION WITHOUT THE USE OF LEVELING NUTS, WASHERS OR SHIMS.
  7. LIGHTING UNIT IDENTIFICATION NUMBERS SHALL BE INSTALLED BEFORE THE LIGHTING UNIT IS ENERGIZED.



LIGHT POLE BASE PLATE DETAIL  
11 1/2" (292.0) BOLT CIRCLE



HANDHOLE DETAIL (N.T.S.)

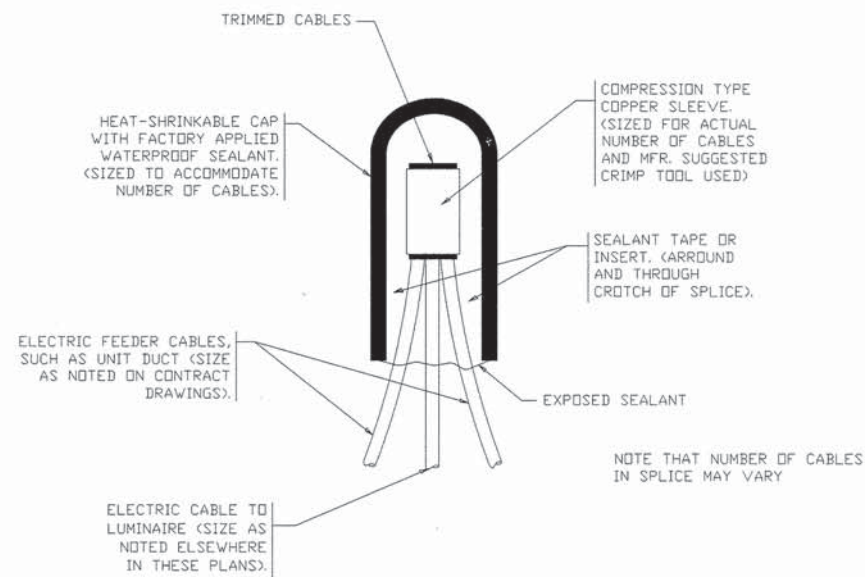
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PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -
	DATE 6/08/2011	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ALUMINUM LIGHT POLE  
30'-0" (9.144 m) MOUNTING HEIGHT

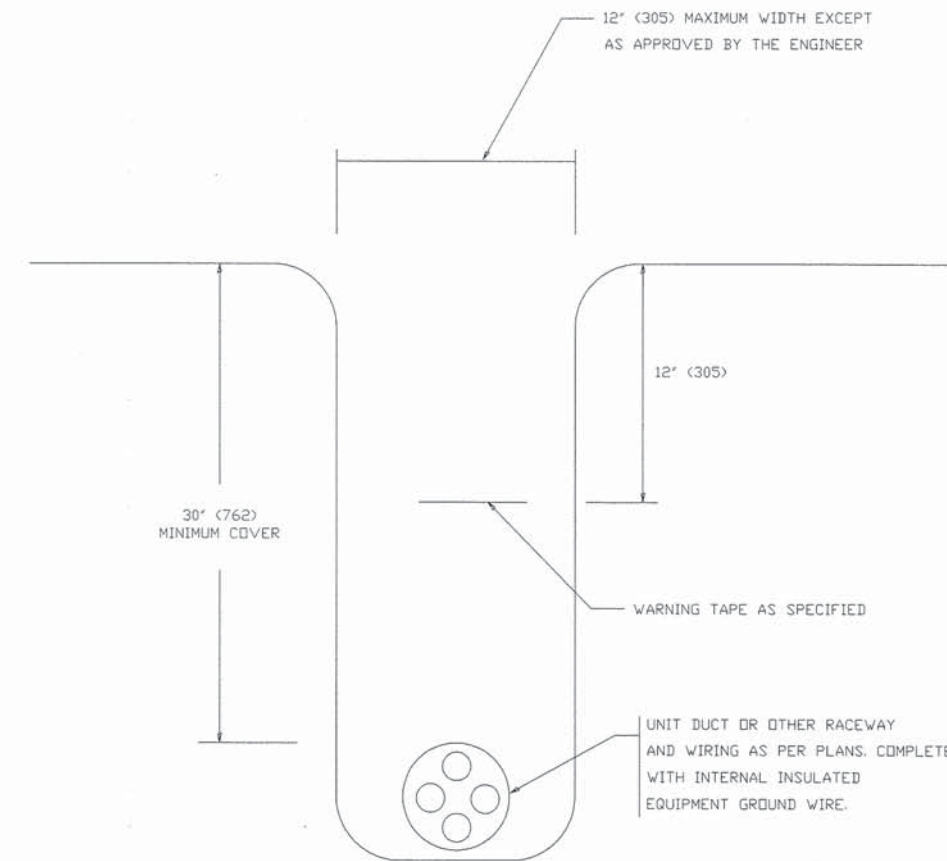
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS NO.
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BE-403		CONTRACT NO. 61A82	
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT			

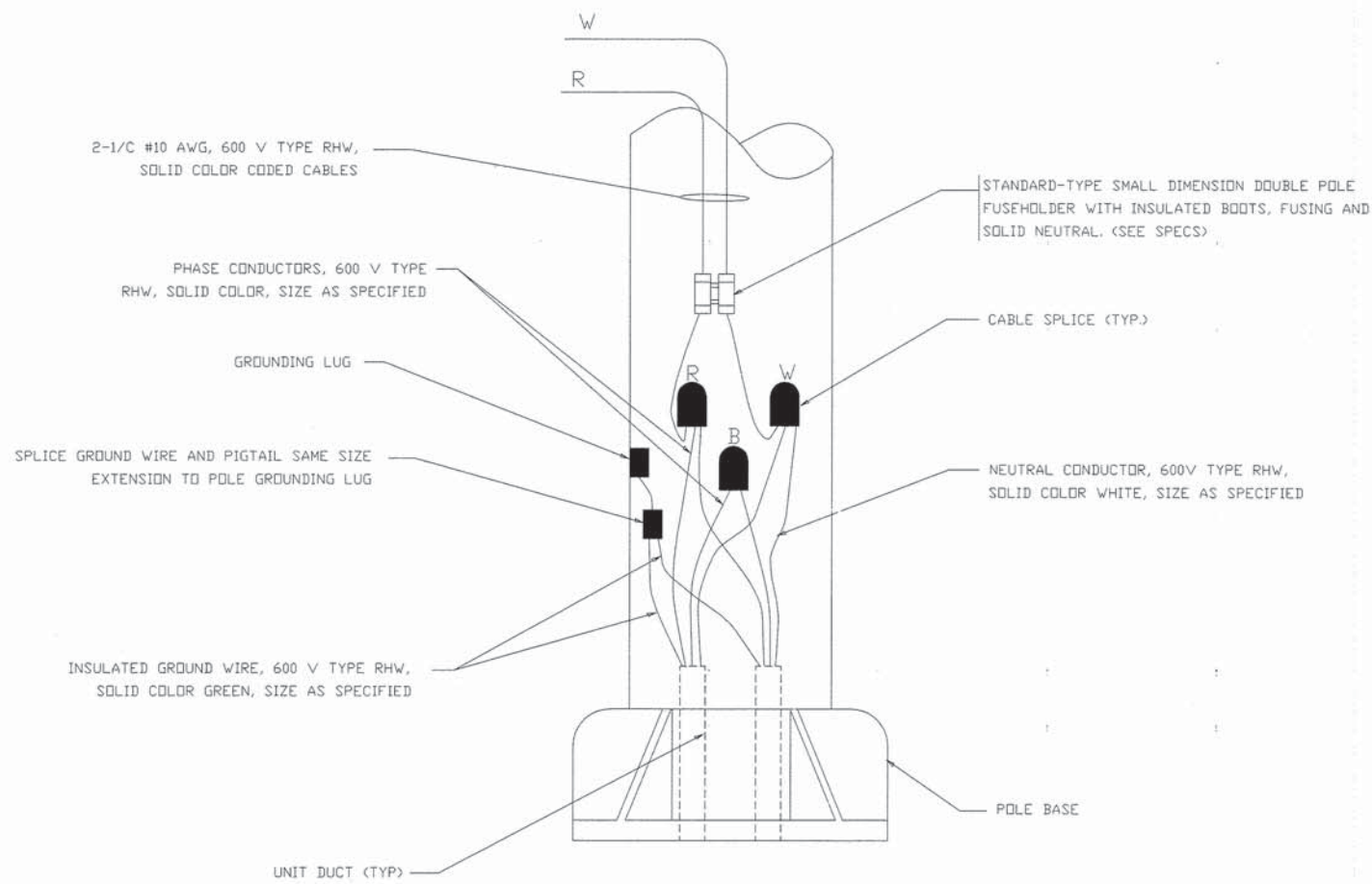


TYPICAL SPLICE DETAIL  
N.T.S.

NOTE THAT NUMBER OF CABLES  
IN SPLICE MAY VARY



TYPICAL WIRING IN TRENCH DETAIL  
N.T.S.



POLE WIRING DETAIL  
N.T.S.

Drawing file: W:\Projects\_by\_Village\Norriddg\61213232 - Overhill Ave Ph II\52-BE\_702.dwg Oct 03, 2014 - 8:57am

USER NAME =	DESIGNED -	REVISED -	08-08-03
	DRAWN -	REVISED -	
PLOT SCALE =	CHECKED -	REVISED -	
PLOT DATE =	DATE 6/08/2011	REVISED -	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

MISC. ELECTRICAL DETAILS  
SHEET A

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

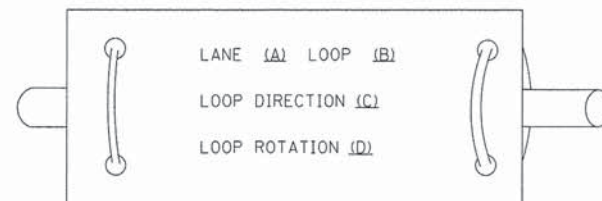
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-	13-0064-00-FP		62	52
BE-702		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 612-13-23201

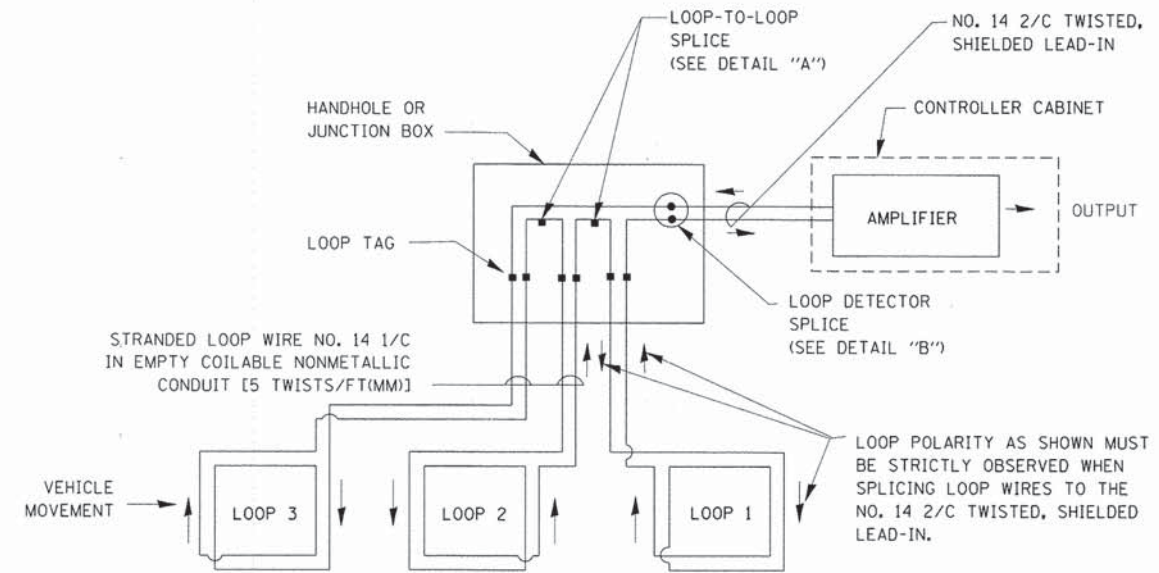
**LOOP DETECTOR NOTES**

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

**LOOP LEAD-IN CABLE TAG**

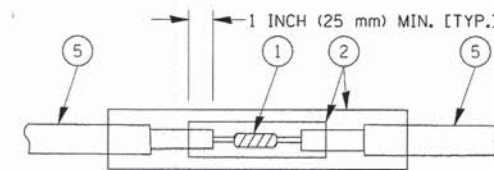


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

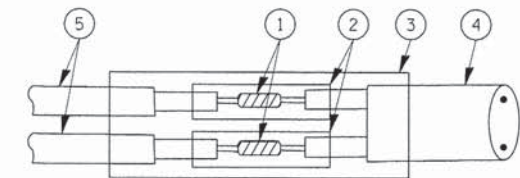


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

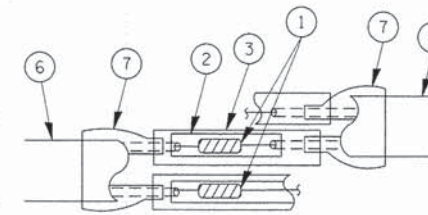


**DETAIL "A"  
LOOP-TO-LOOP SPLICE**

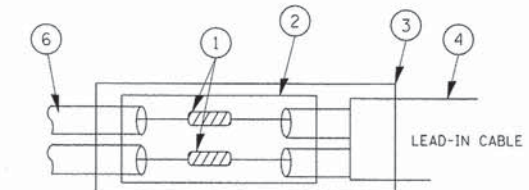


**DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE**

**TYPE I LOOP**



**DETAIL "A"  
LOOP-TO-LOOP SPLICE**



**DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE**

**LOOP DETECTOR SPLICE**

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- PREFORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

Drawing file: W:\Projects\by\_silloga\Nortridge\61213232 - Overhill Ave Ph. 11\53-TS-05.dwg Oct 03, 2014 - 8:59am

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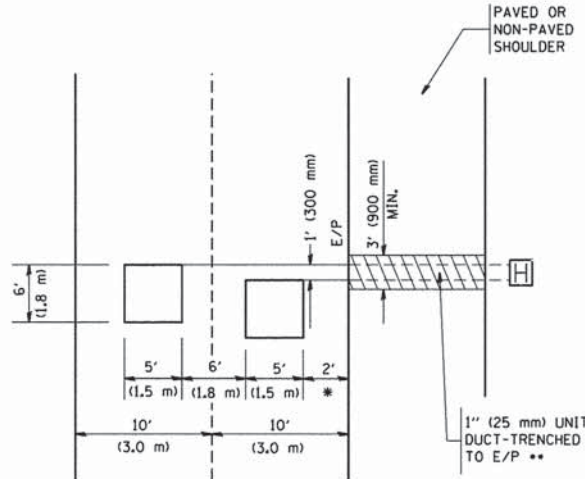
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 6 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	00-00064-00-FP		62	53
<b>TS-05</b>		CONTRACT NO. <b>61A82</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

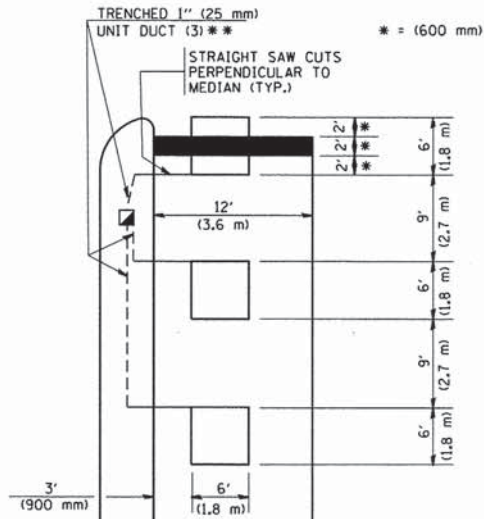


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**

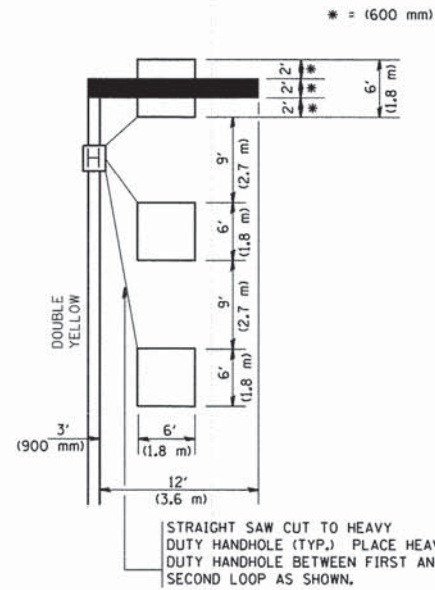
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

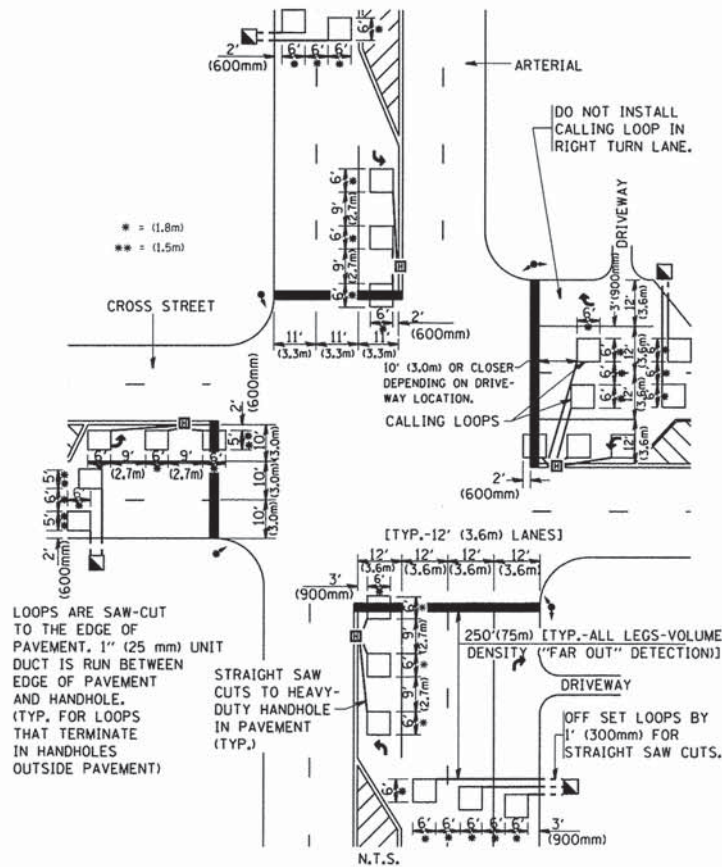
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**



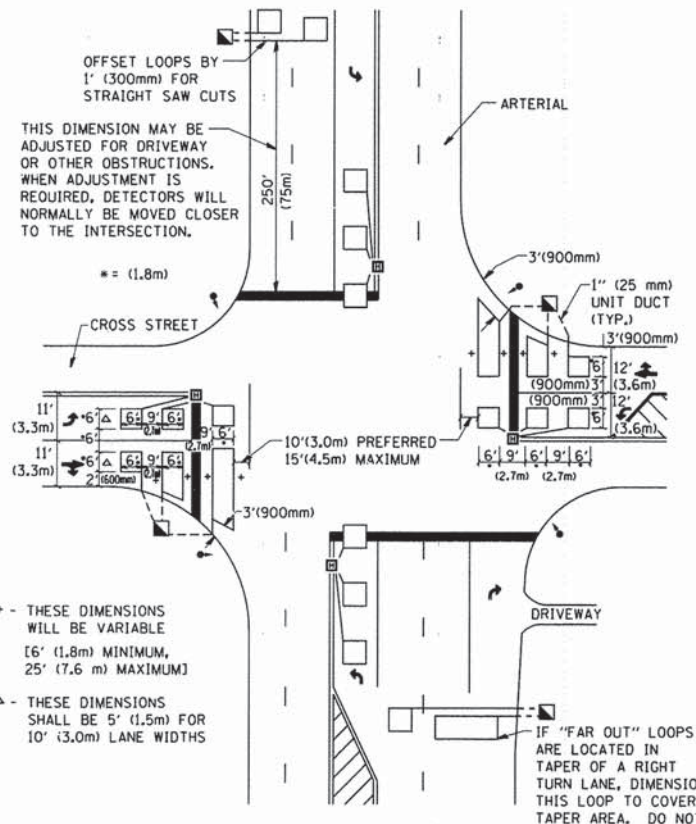
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2**  
N.T.S.

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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PLOT DATE = 1/4/2008

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DRAWN -	REVISED -
CHECKED - R.K.F.	REVISED -
DATE -	REVISED -

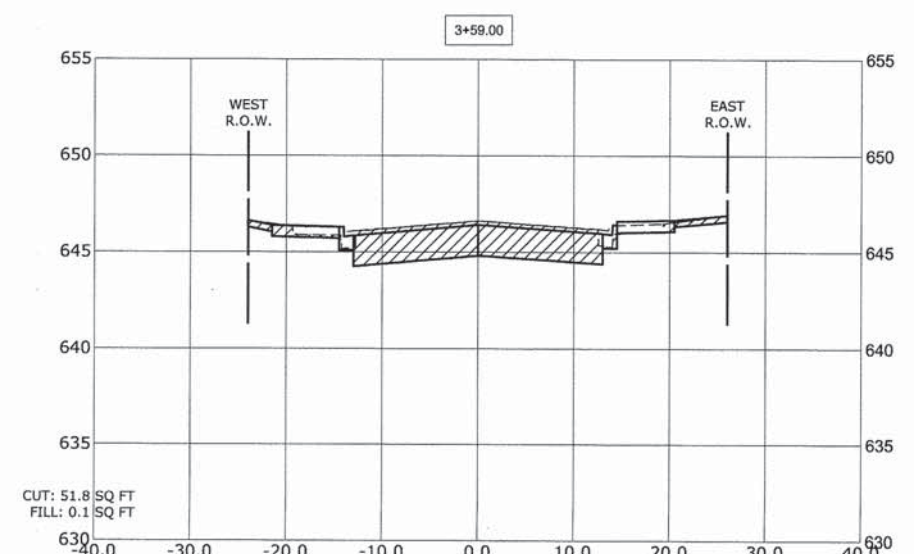
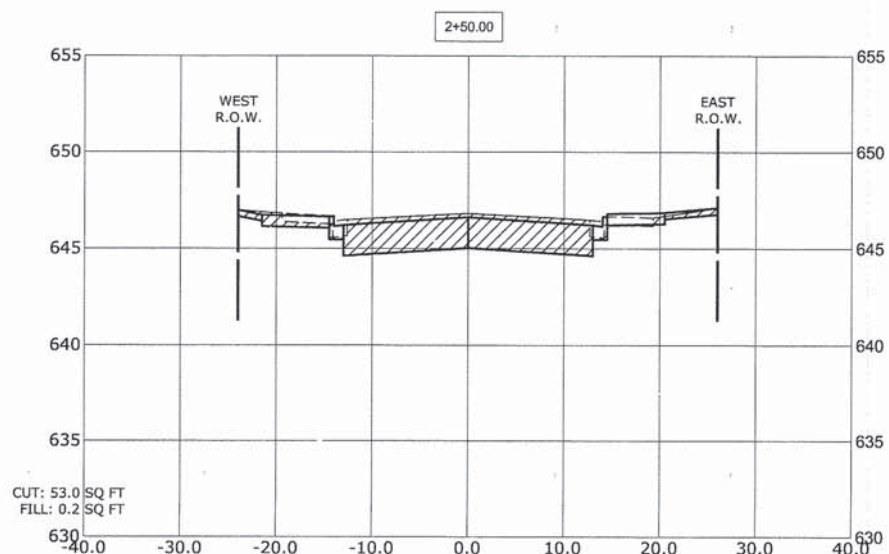
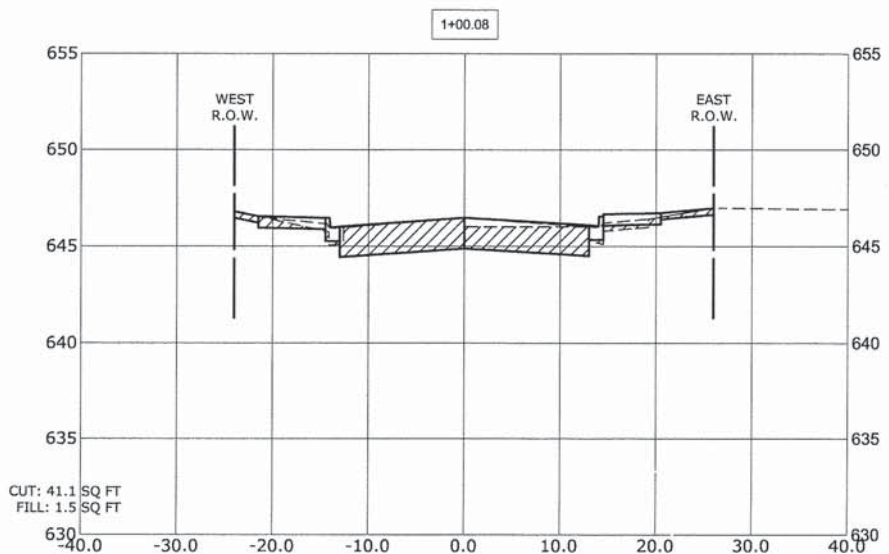
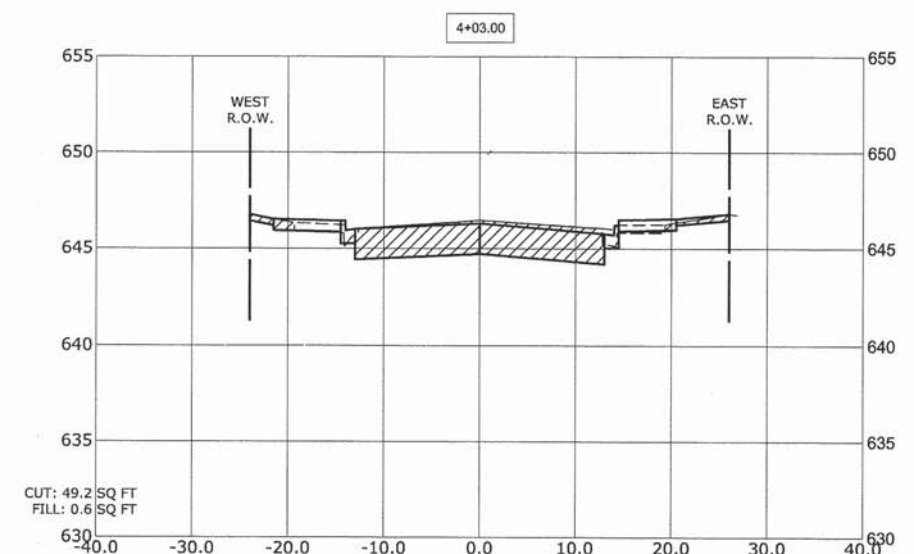
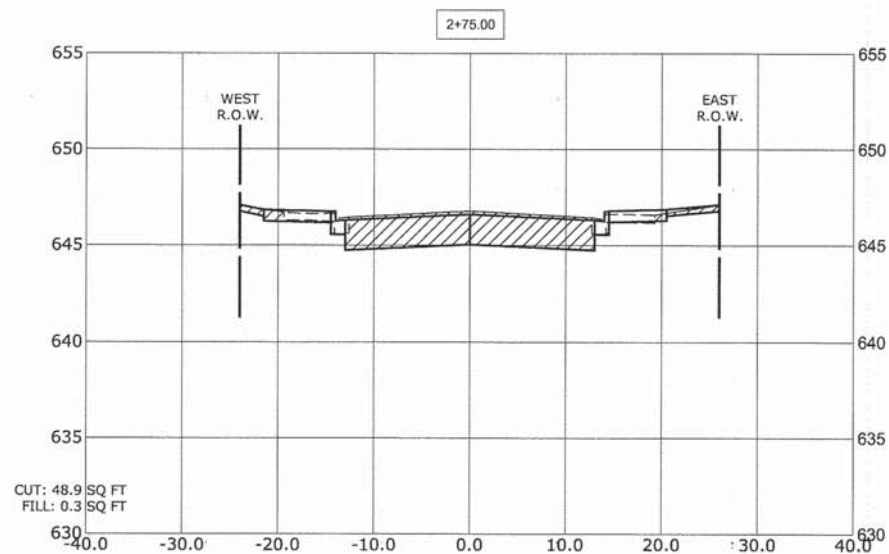
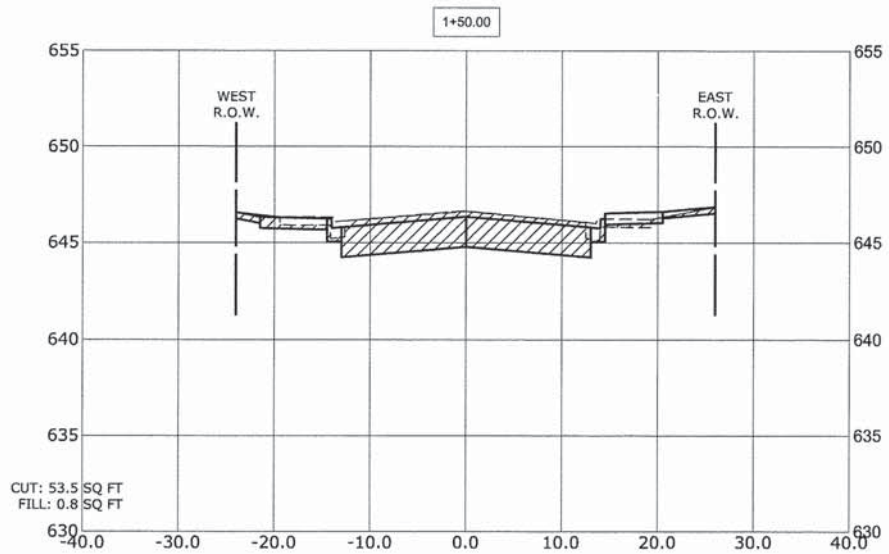
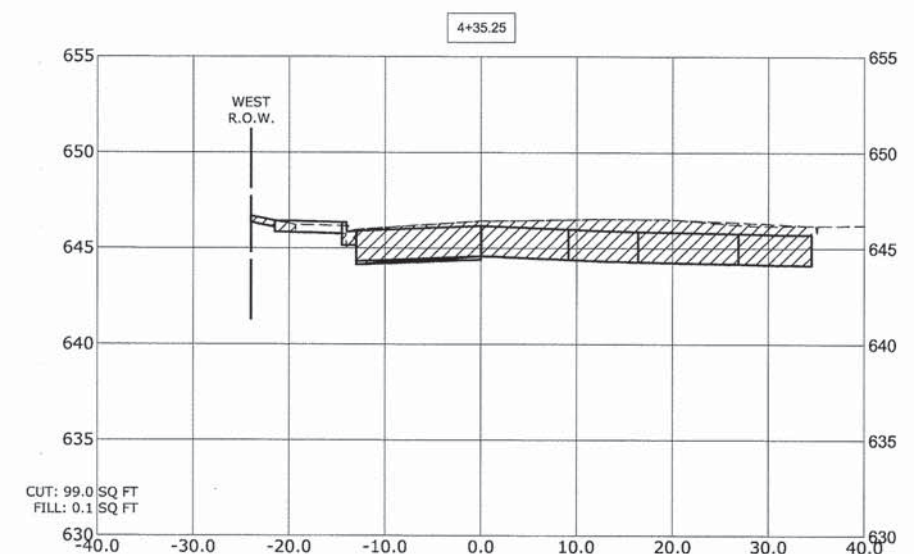
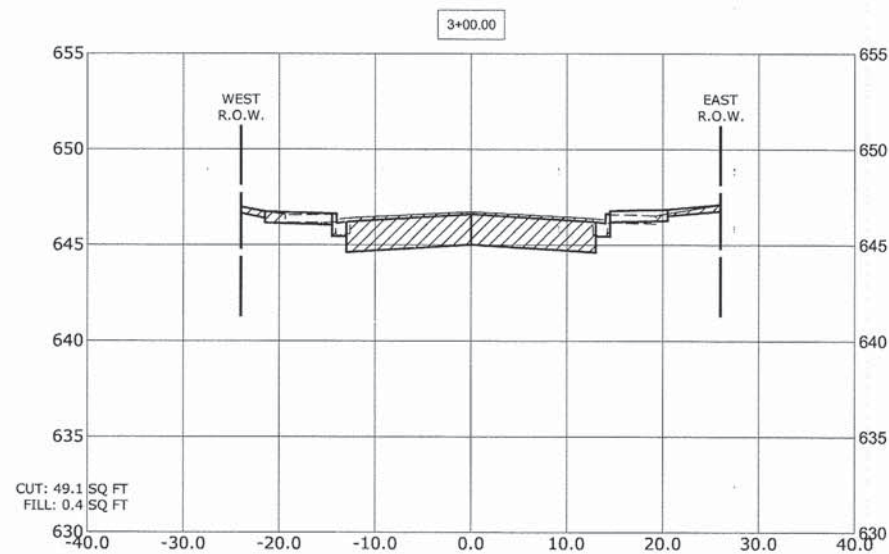
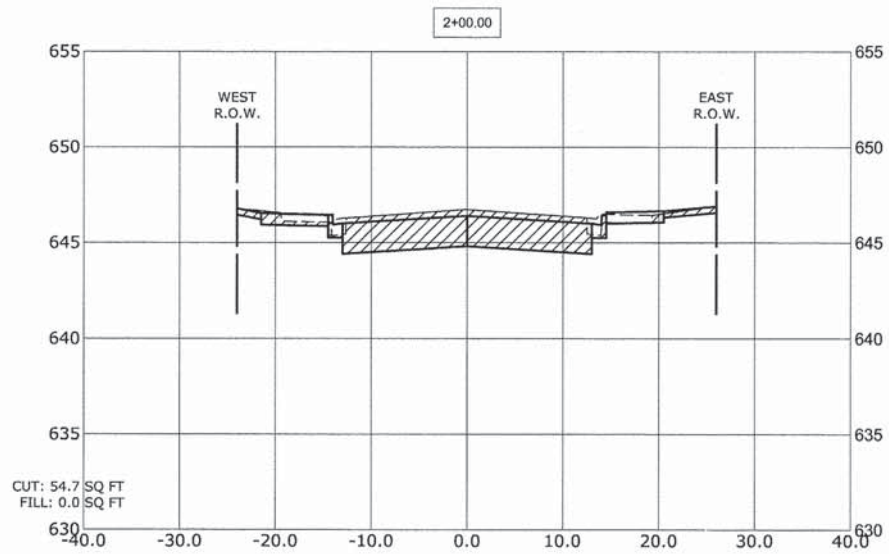
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION  
DETAILS FOR ROADWAY RESURFACING**

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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	00-00064-00-FP		62	54
TS-07			CONTRACT NO. 61A82	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

<b>CROSS SECTION LEGEND</b>	
C = CUT (SQ. FT.)	
F = FILL (SQ. FT.)	



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**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants  
 Established 1911

USER NAME -	DESIGNED - SBC/CB	REVISED -
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

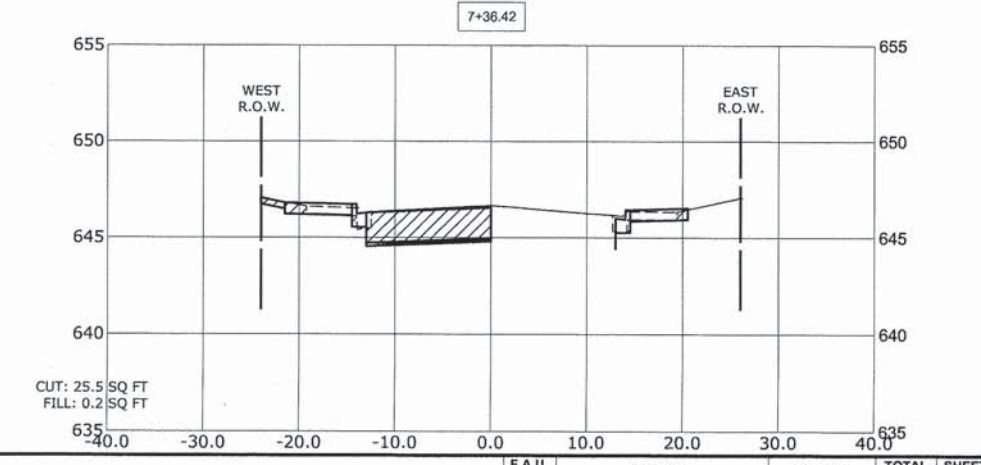
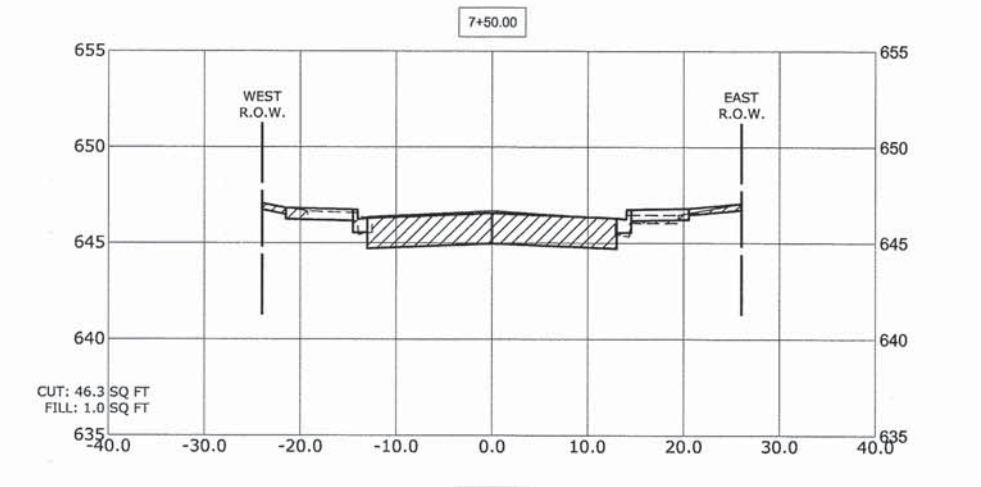
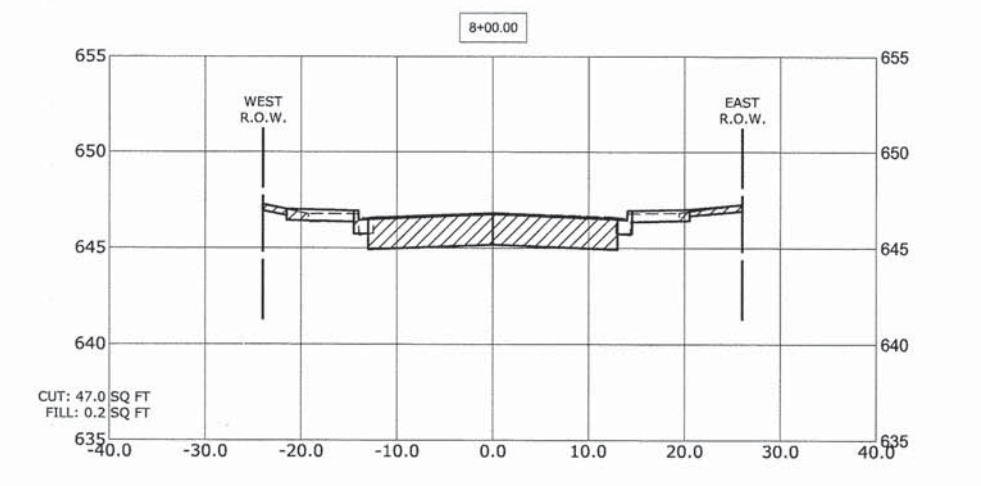
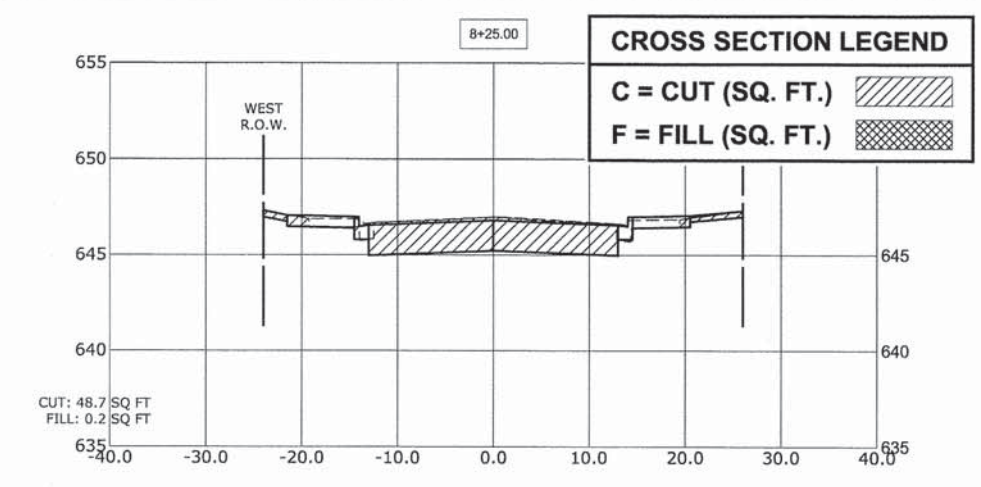
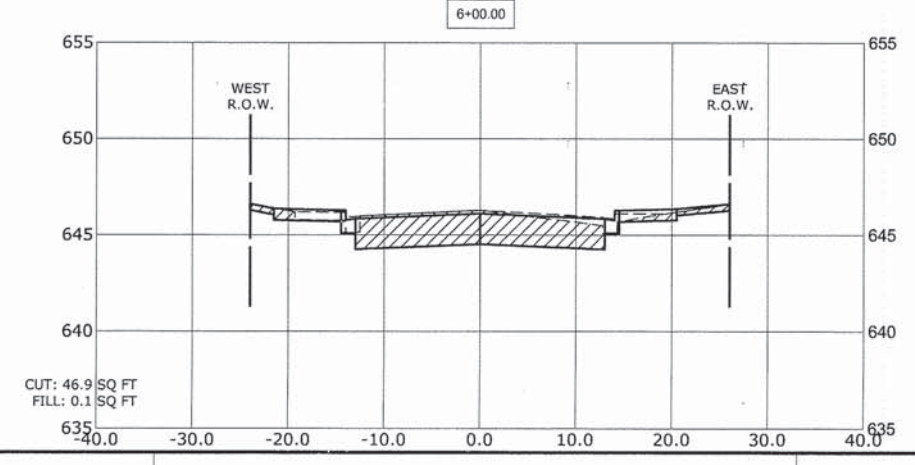
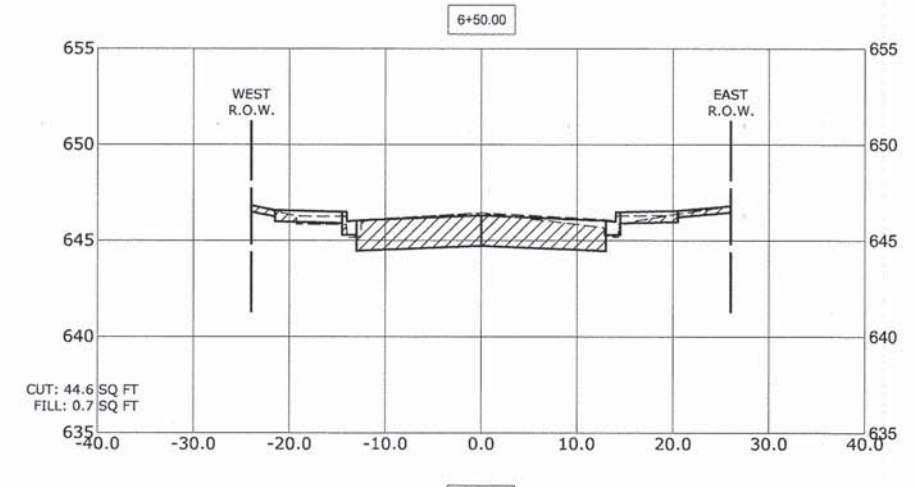
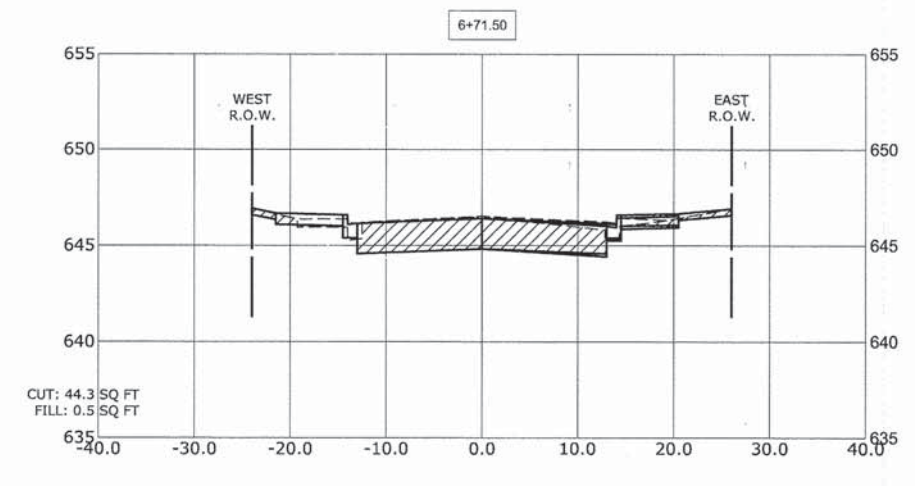
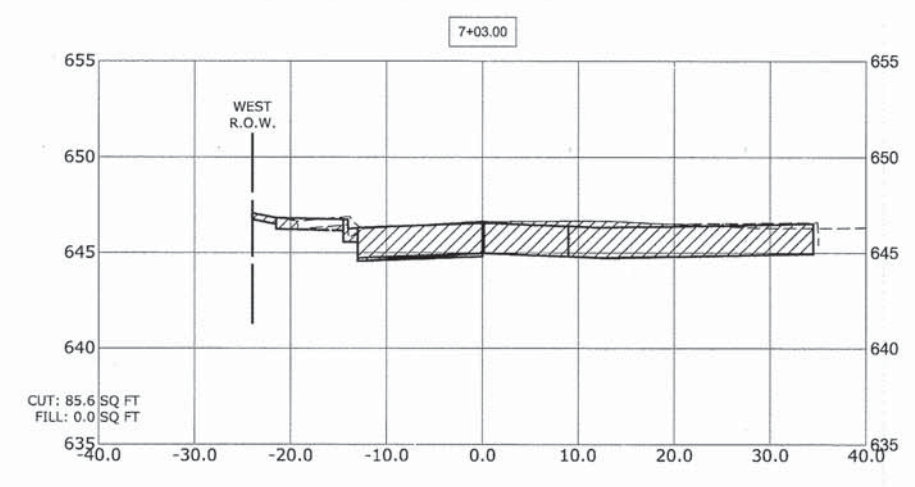
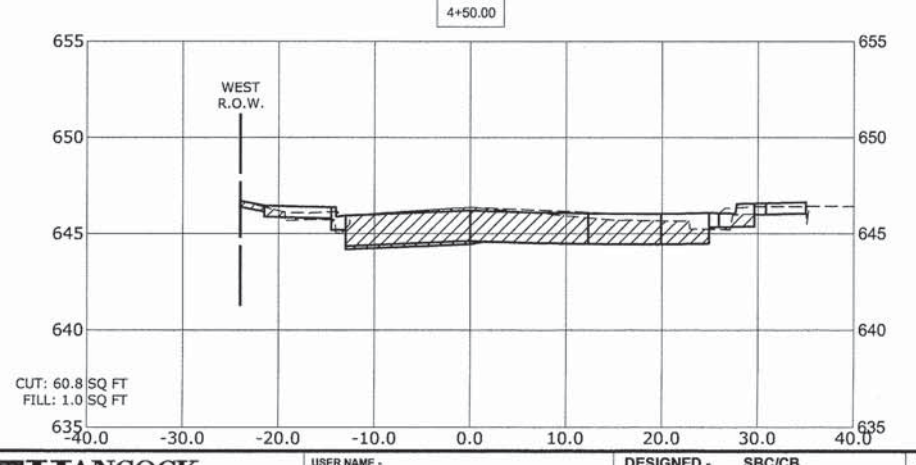
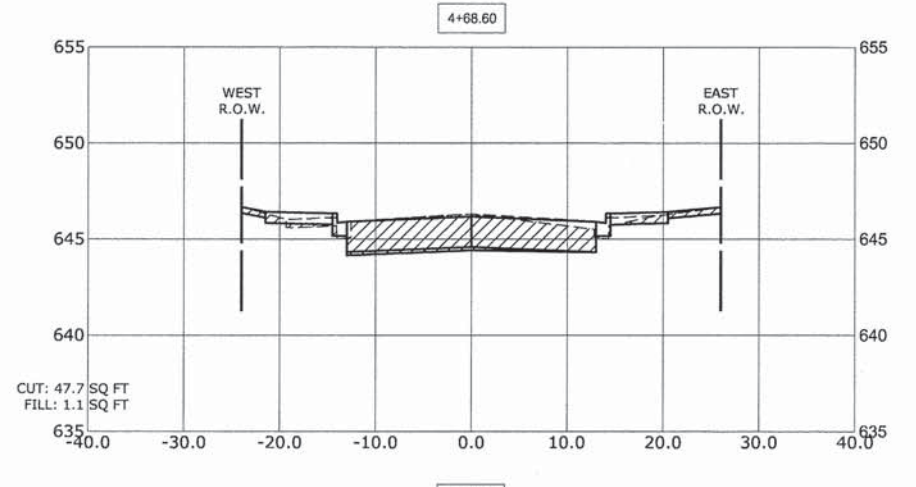
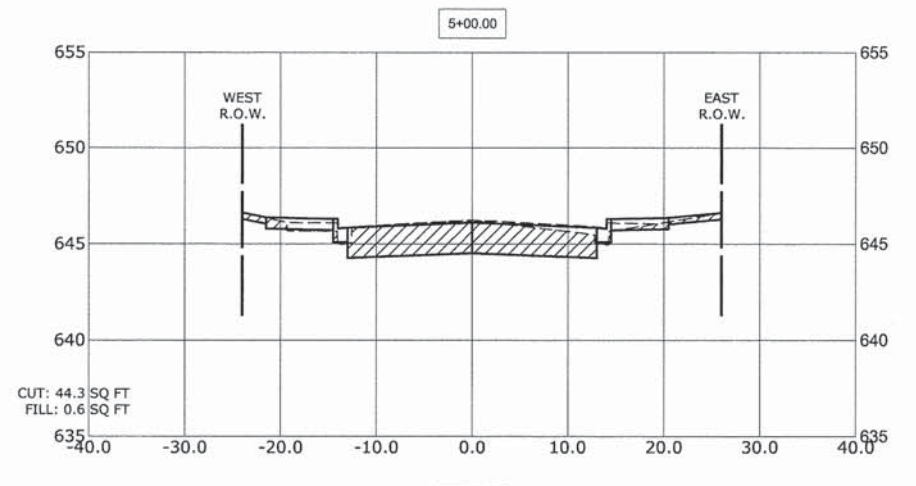
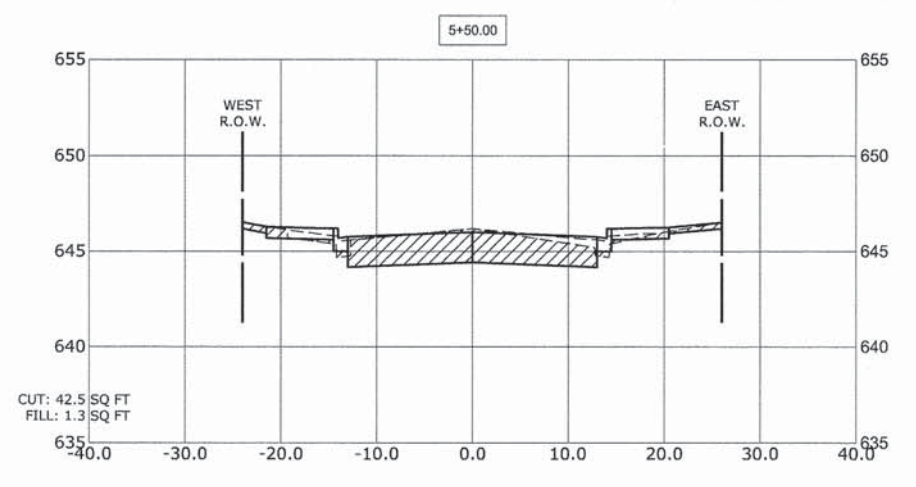
**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

**CROSS SECTIONS**

SCALE: H:1"=10', V:1"=5'    SHEET NO. 1 OF 8 SHEETS    STA. -    TO STA. -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	55
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

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**CROSS SECTION LEGEND**

C = CUT (SQ. FT.)

F = FILL (SQ. FT.)

**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants  
 Established 1911

USER NAME -	DESIGNED - SBC/CB	REVISED -
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

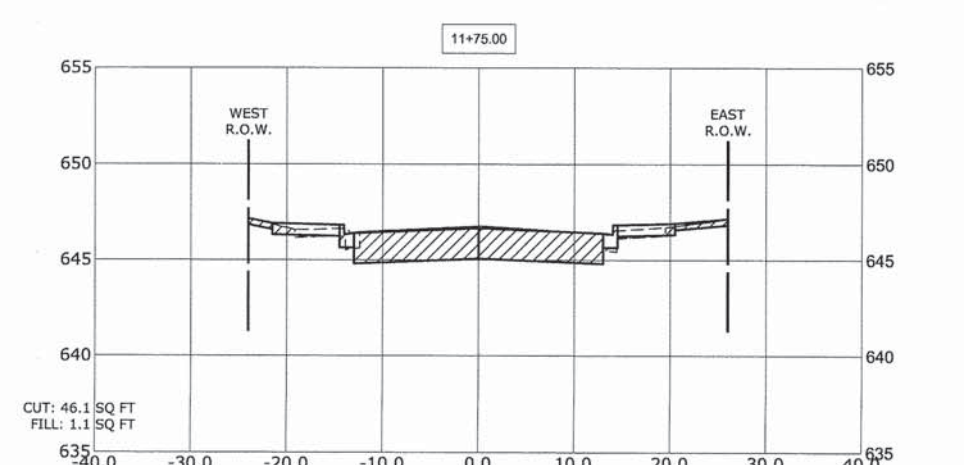
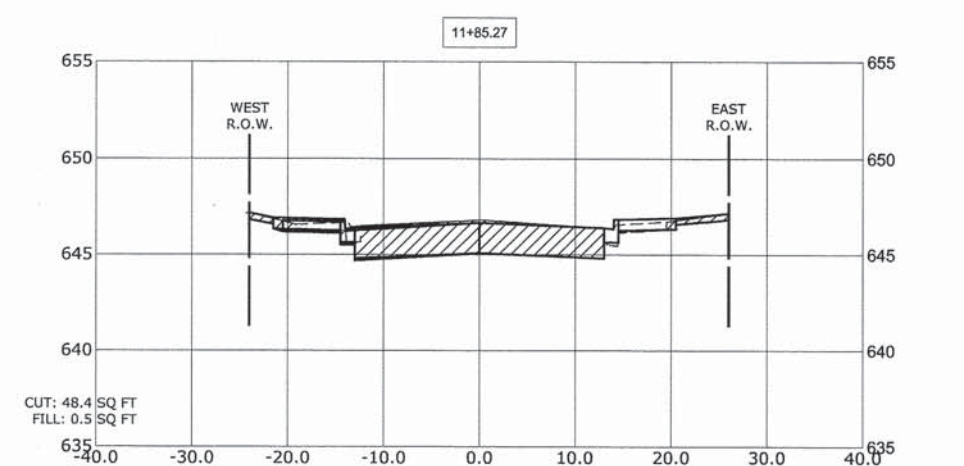
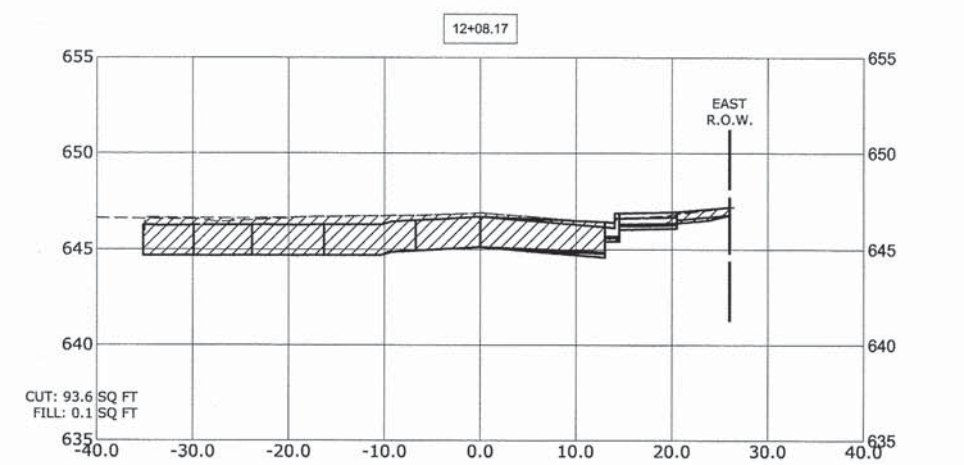
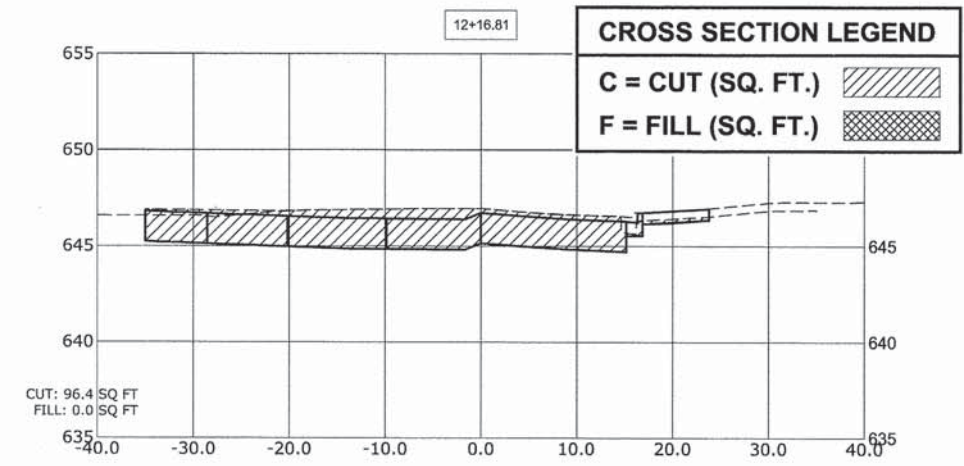
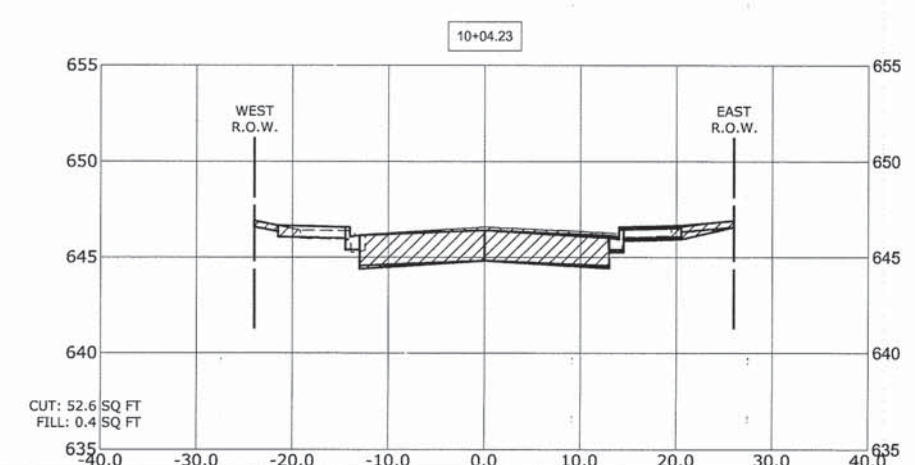
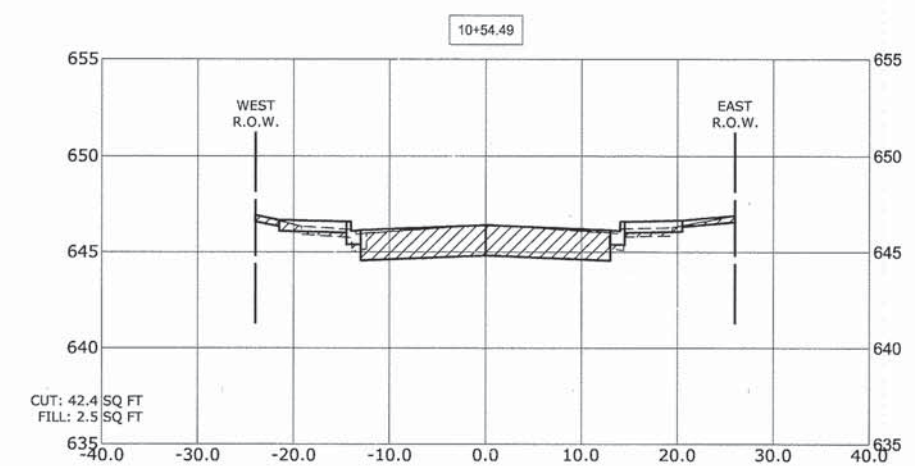
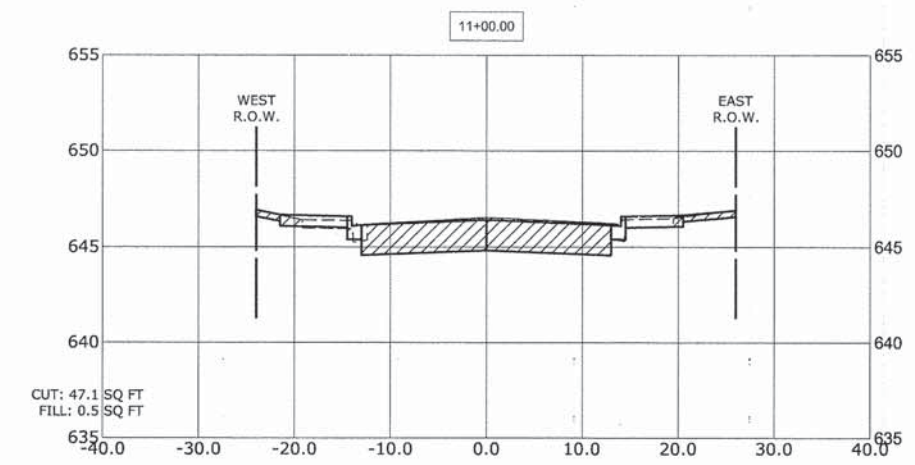
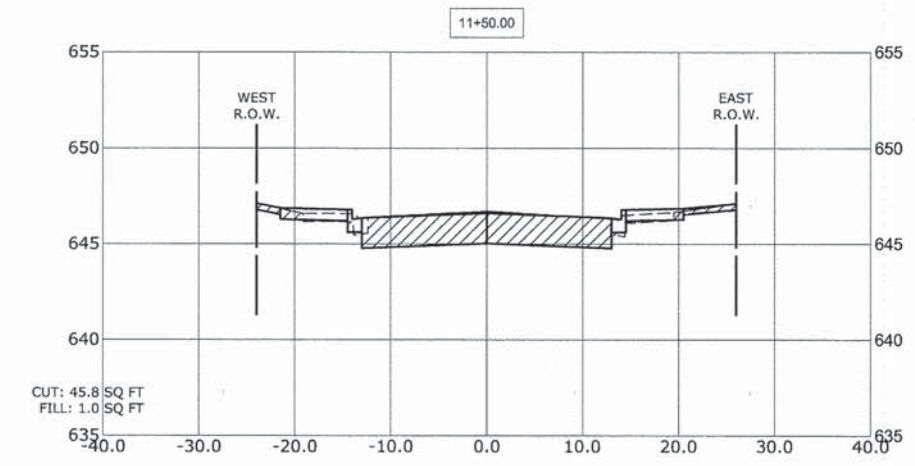
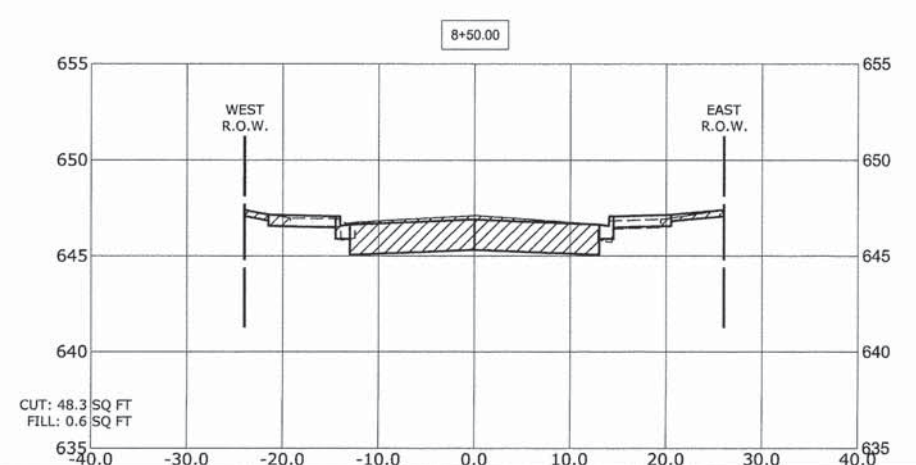
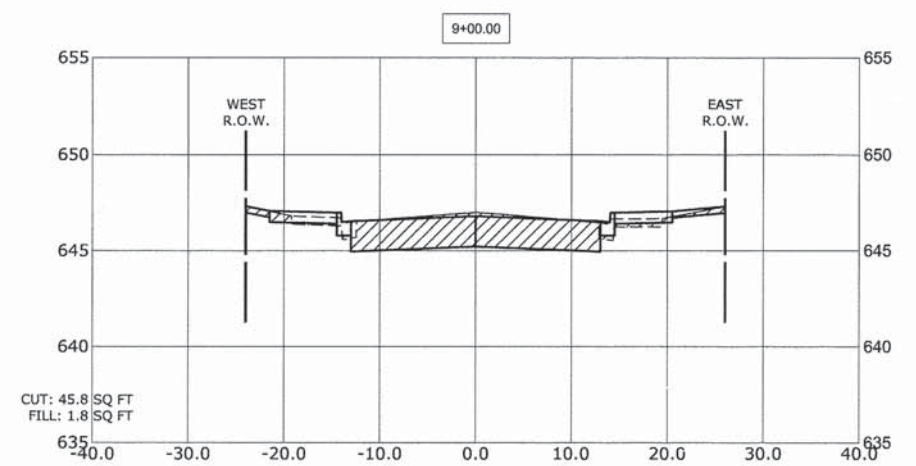
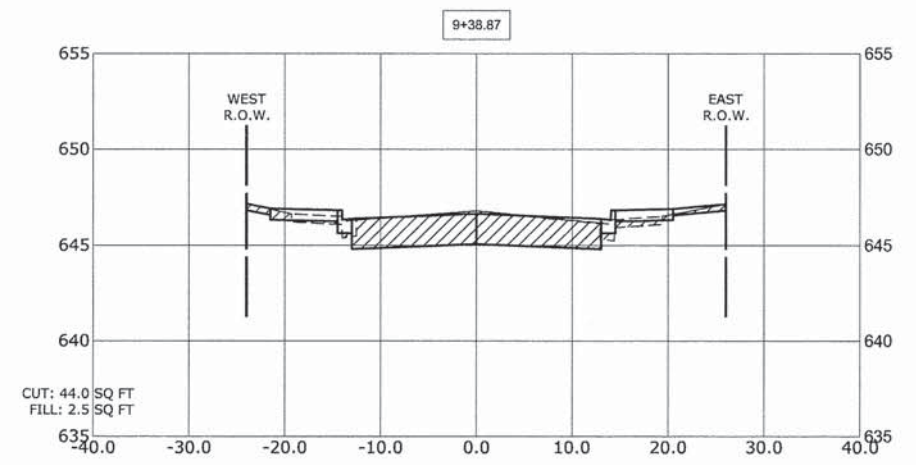
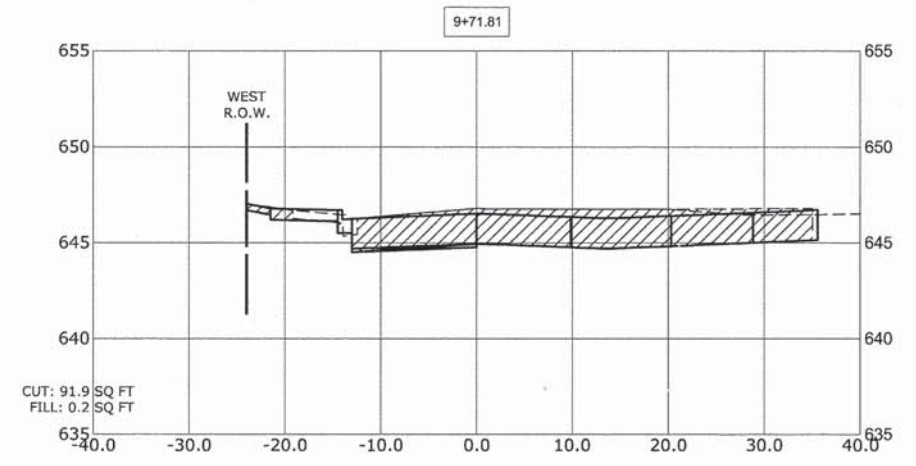
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	56
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



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**CROSS SECTION LEGEND**

C = CUT (SQ. FT.)

F = FILL (SQ. FT.)

**HANCOCK ENGINEERING**

Civil Engineers  
Municipal Consultants  
Established 1911

9113 Skokie Blvd  
Skokie, Illinois 60076-2700  
Phone: 708.962.4300  
Fax: 708.962.2252

USER NAME -	DESIGNED - SBC/CB	REVISED -
PLOT SCALE -	DRAWN - MK, DMM, ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

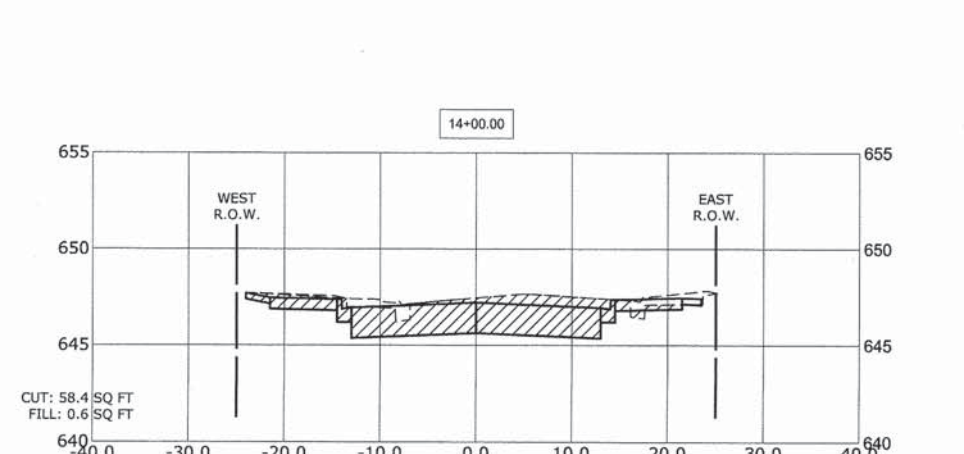
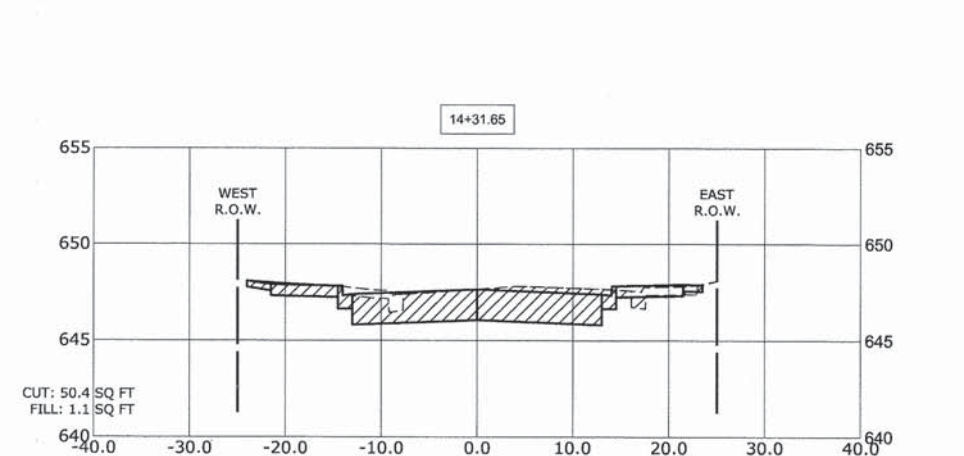
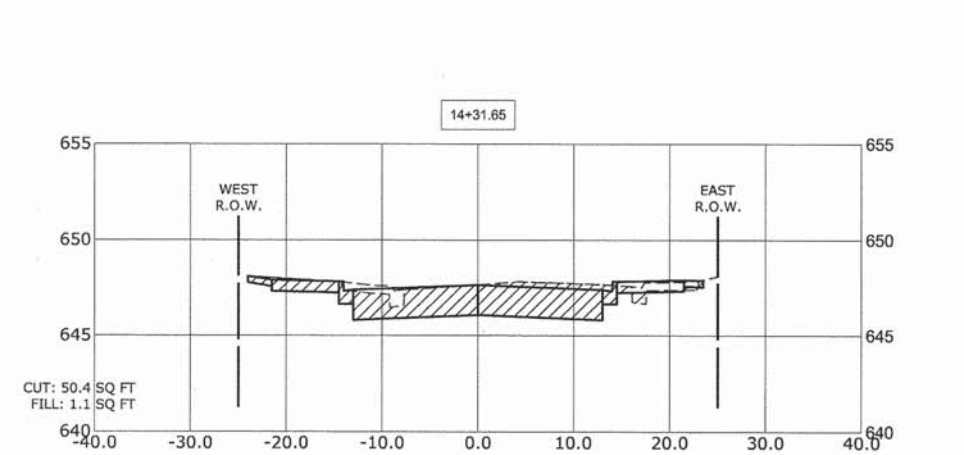
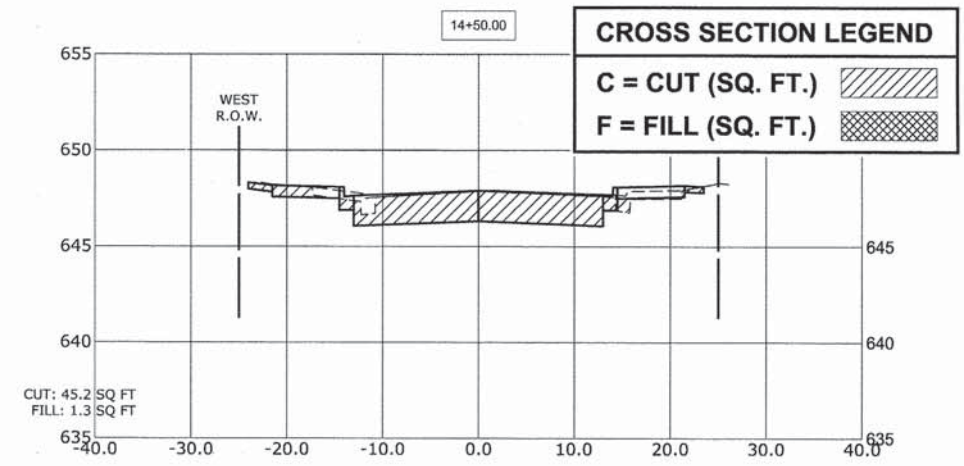
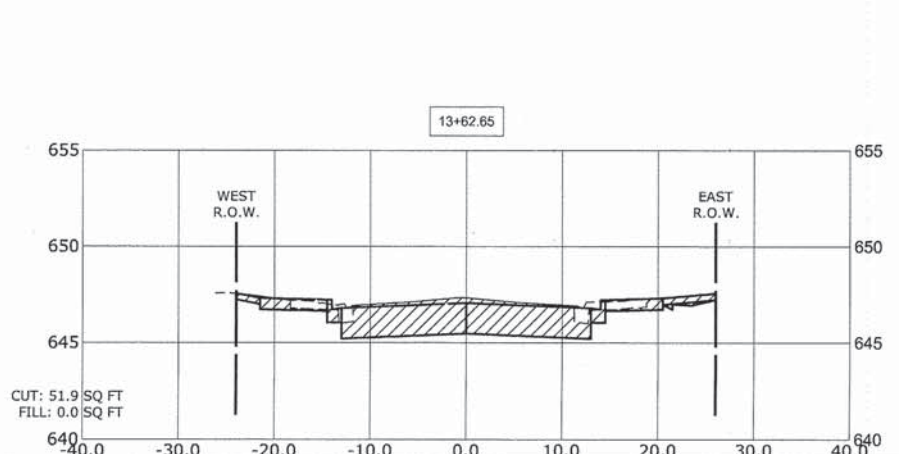
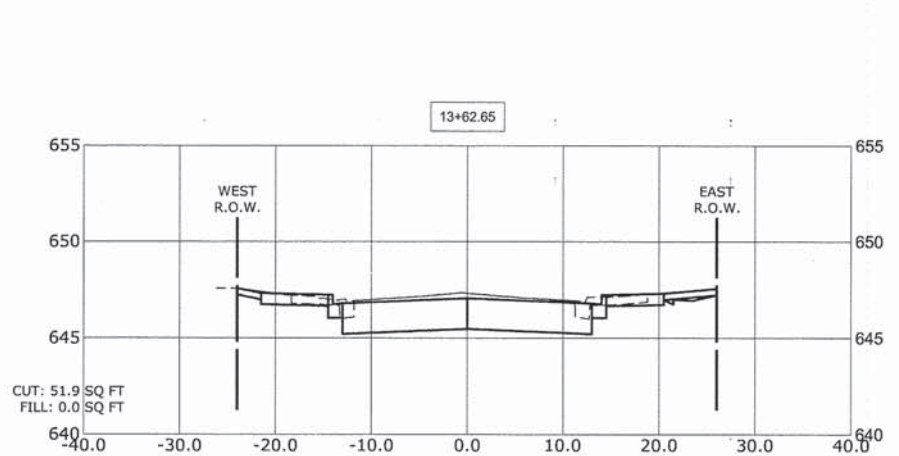
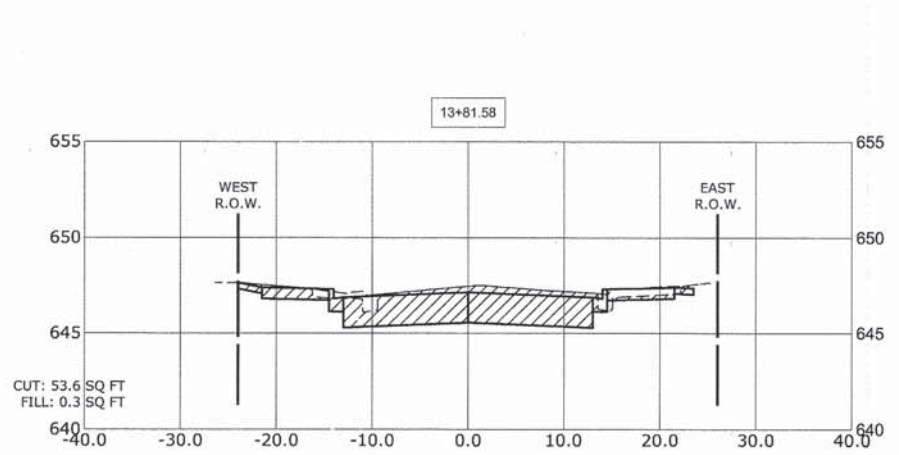
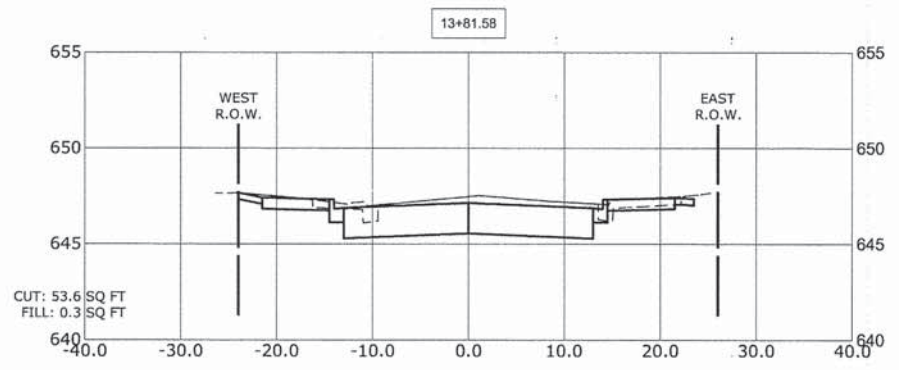
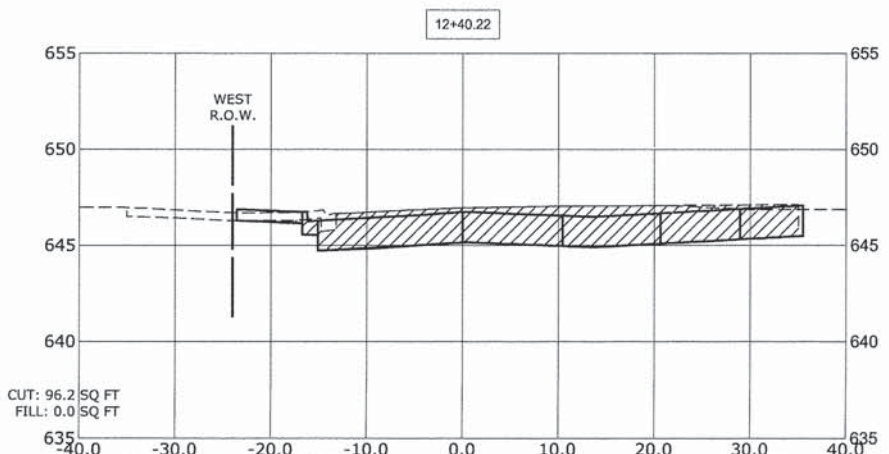
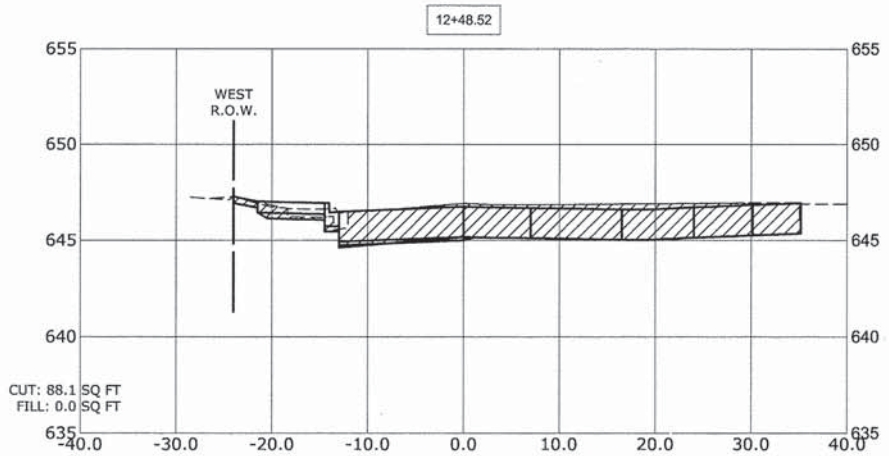
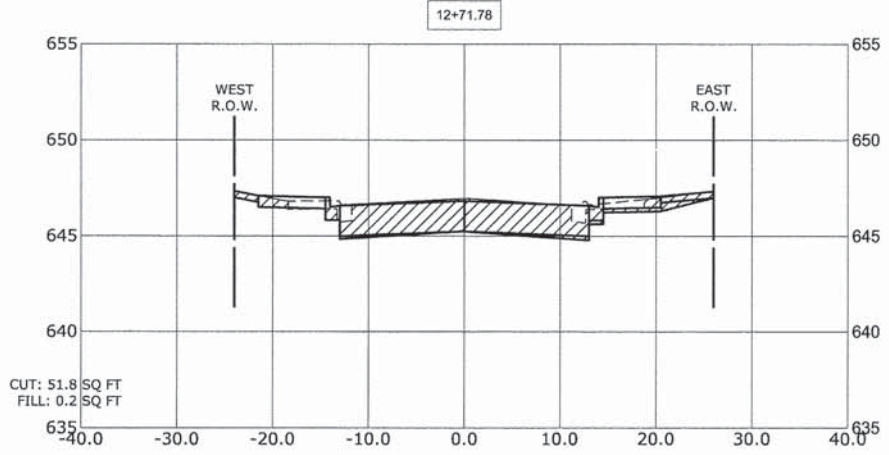
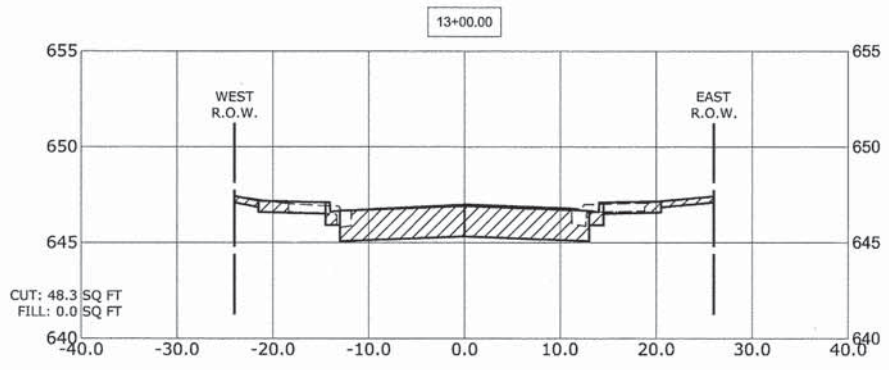
**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

**CROSS SECTIONS**

SCALE: H:1"=10', V:1"=5' SHEET NO. 3 OF 8 SHEETS STA. - TO STA. -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	57
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

Drawing file: W:\Projects\by\_Village\Norridge\61213232 - Overhill Ave Ph II\Site\Overhill Ave\_Ph\_II.dwg Oct 03, 2014 - 9:20am



CROSS SECTION LEGEND	
C = CUT (SQ. FT.)	
F = FILL (SQ. FT.)	



USER NAME -	DESIGNED - SBC/CB	REVISED -
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

**CROSS SECTIONS**

SCALE: H:1"=10',V:1"=5' SHEET NO. 4 OF 8 SHEETS STA. - TO STA. -

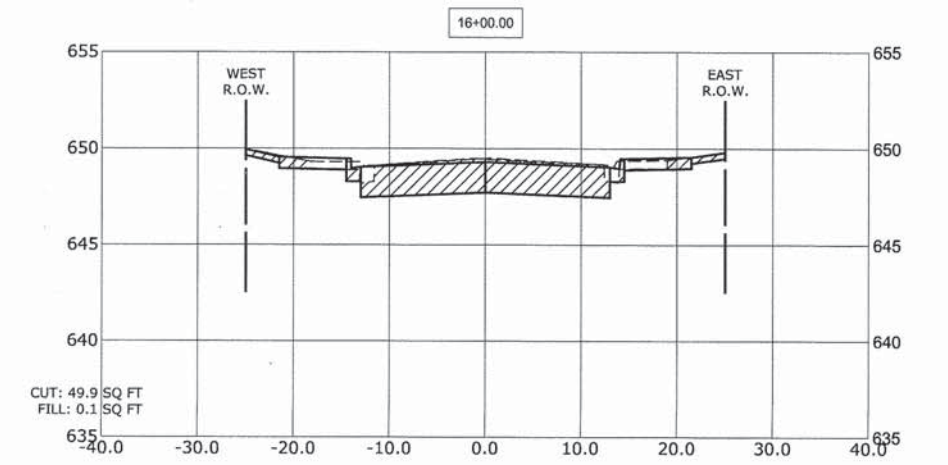
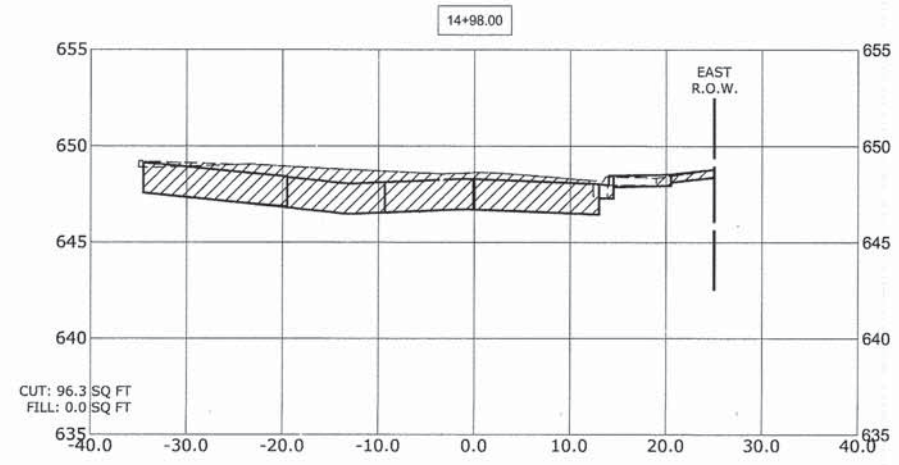
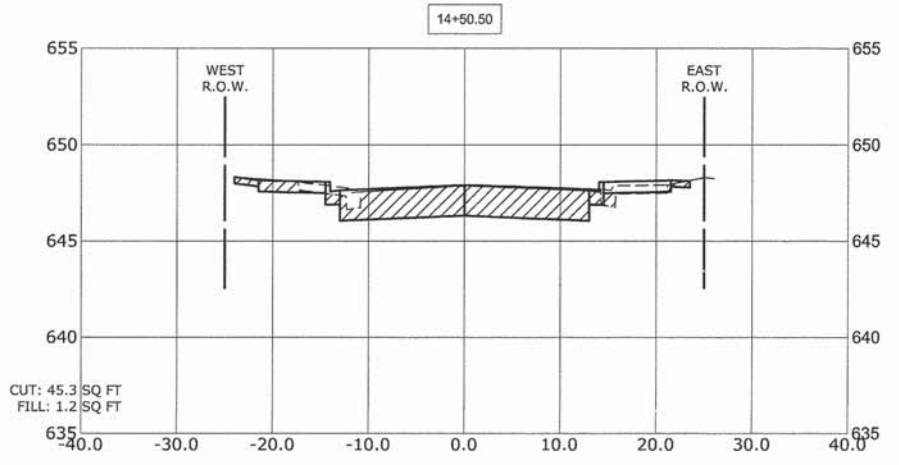
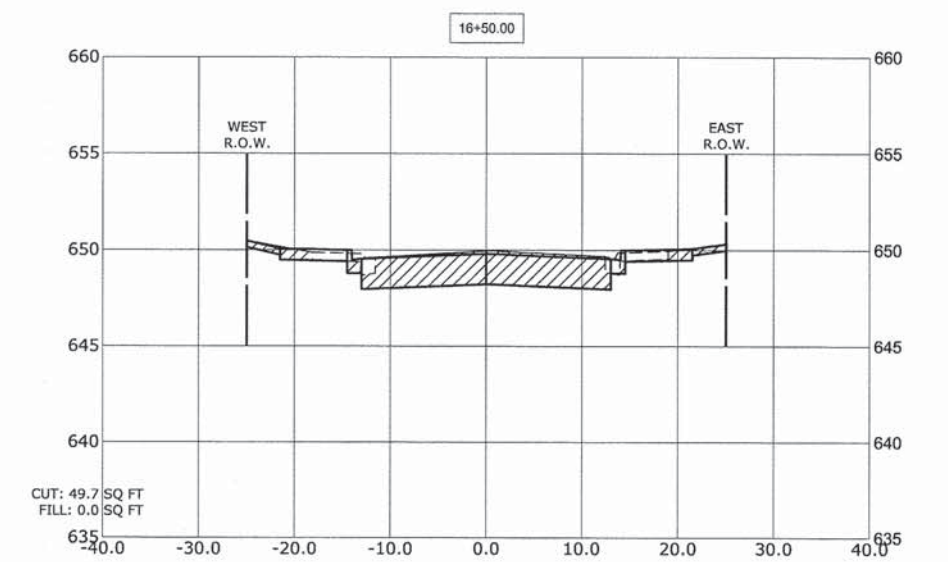
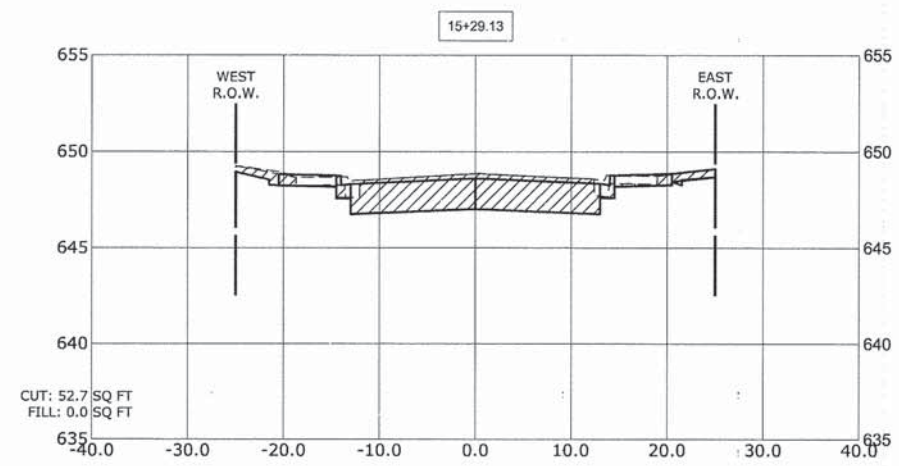
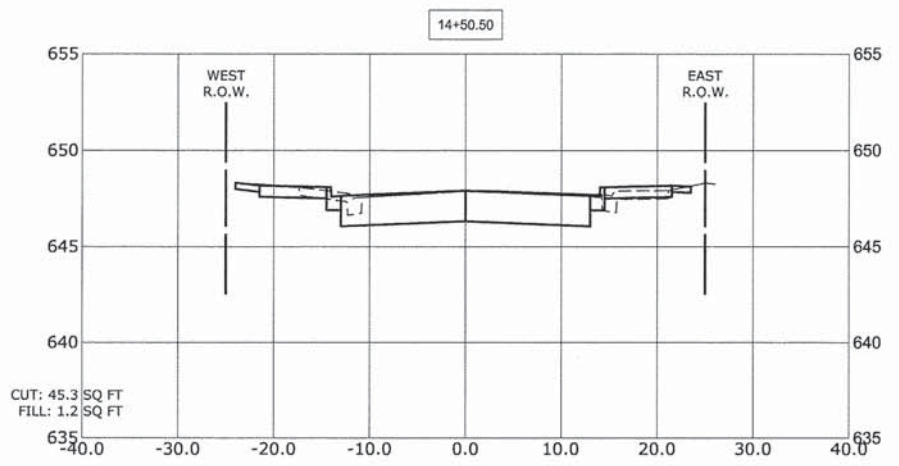
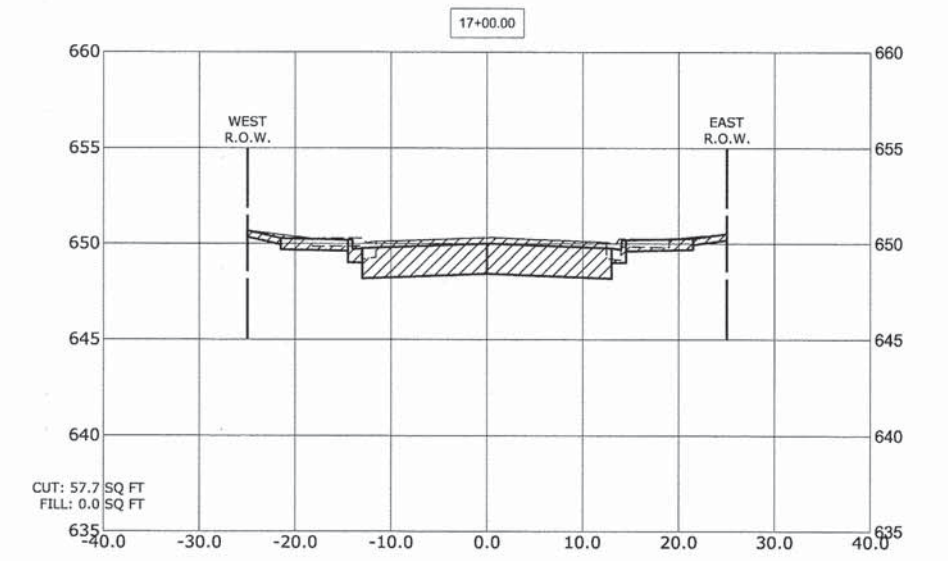
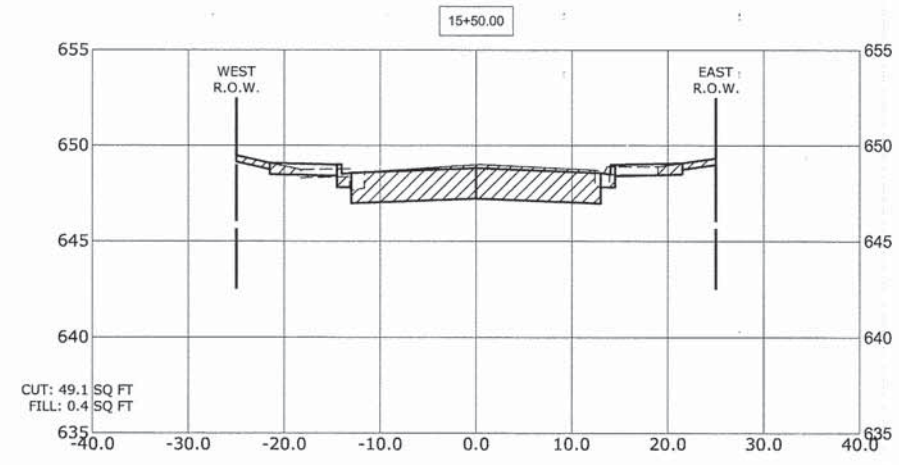
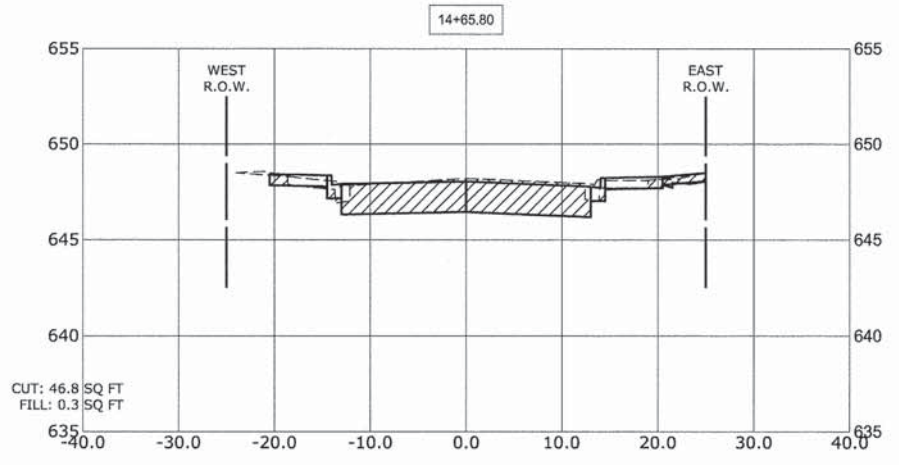
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	58
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

E.H.E. PROJECT NO. 612-13-23201

**CROSS SECTION LEGEND**

C = CUT (SQ. FT.)

F = FILL (SQ. FT.)



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**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants  
 Established 1911

USER NAME -	DESIGNED - SBC/CB	REVISED -
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

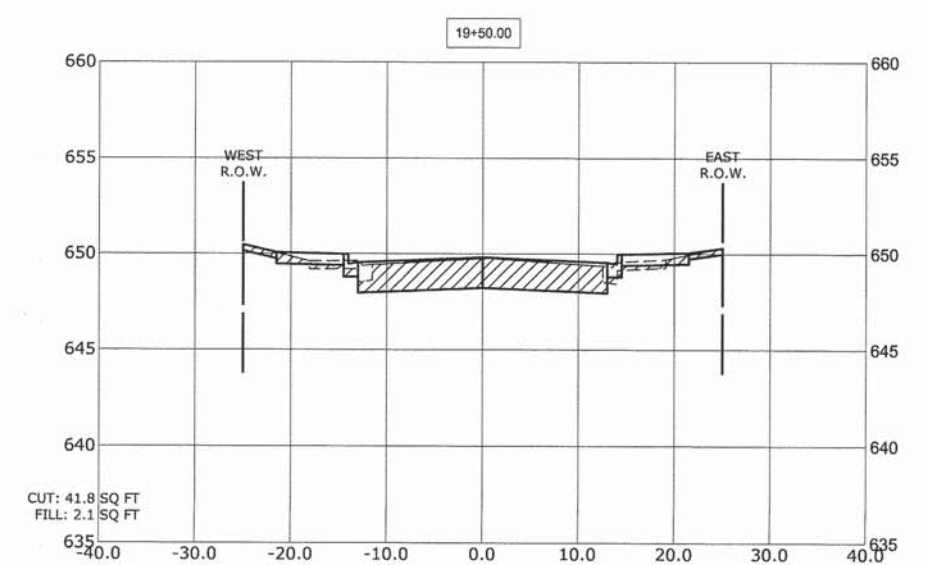
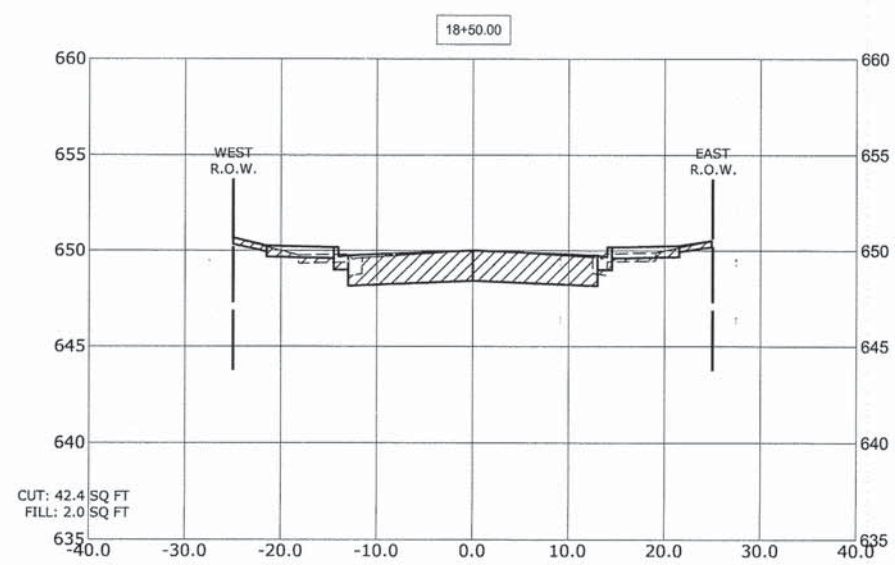
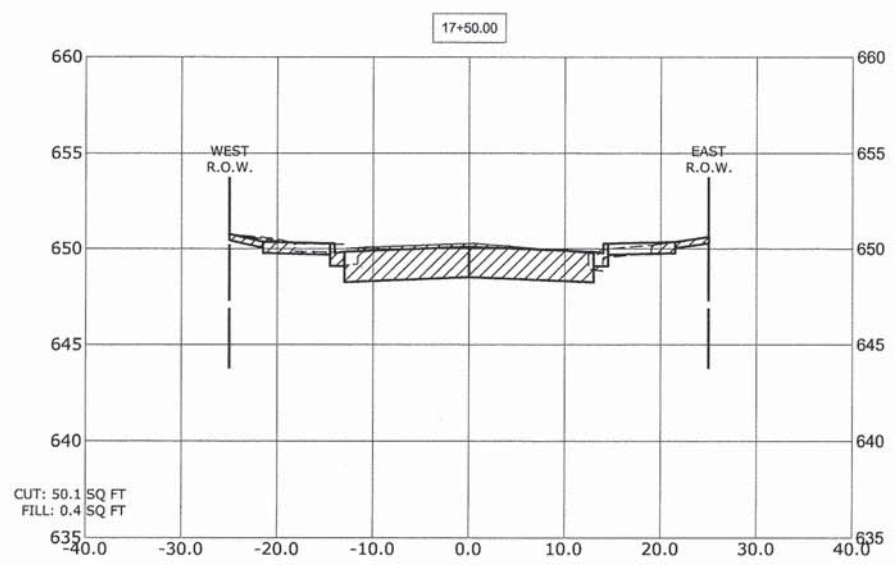
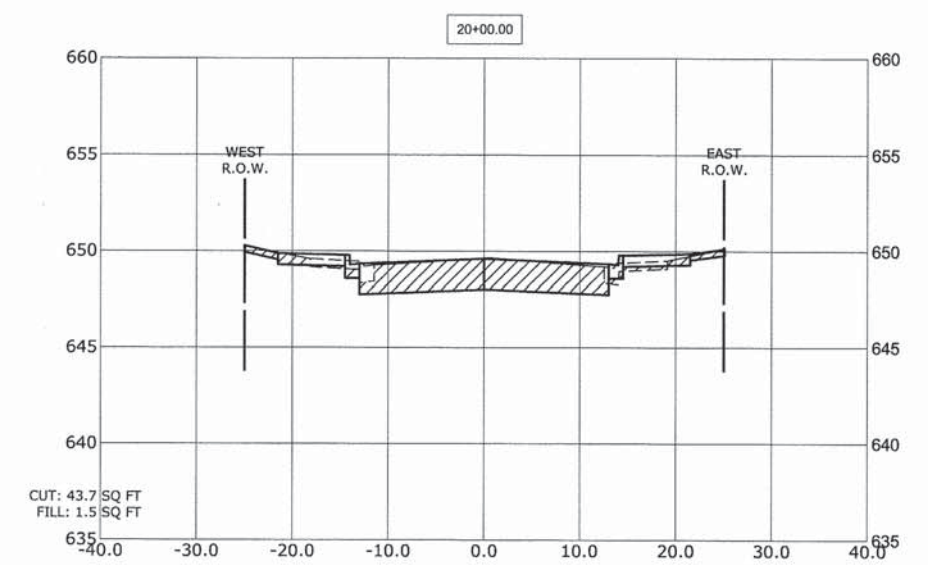
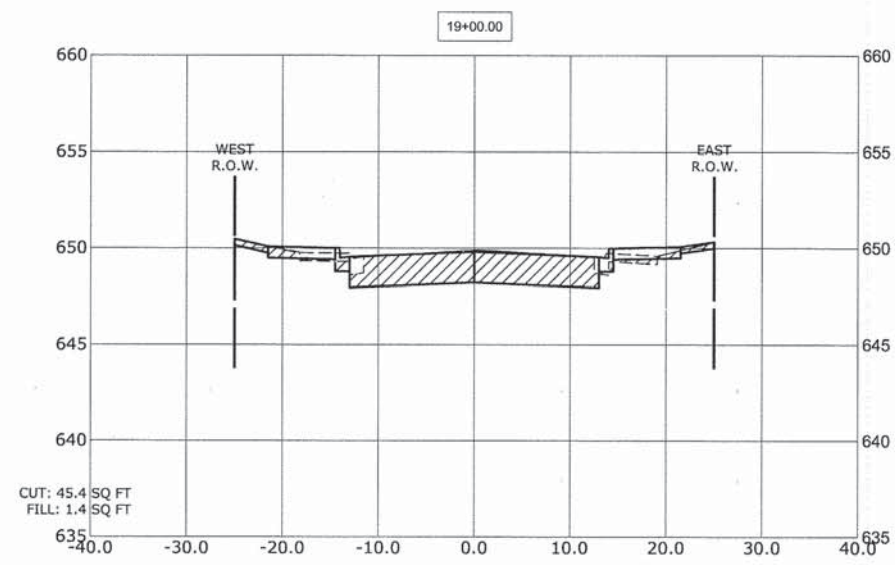
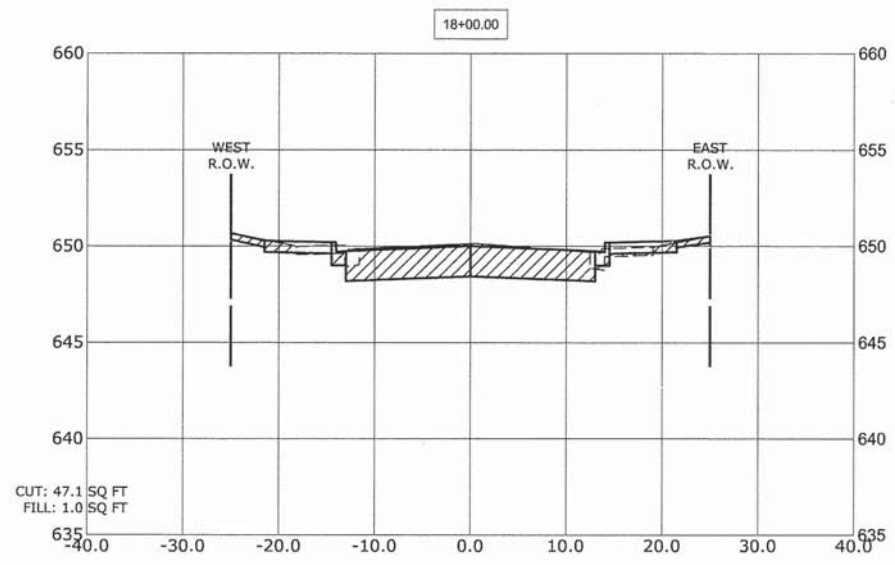
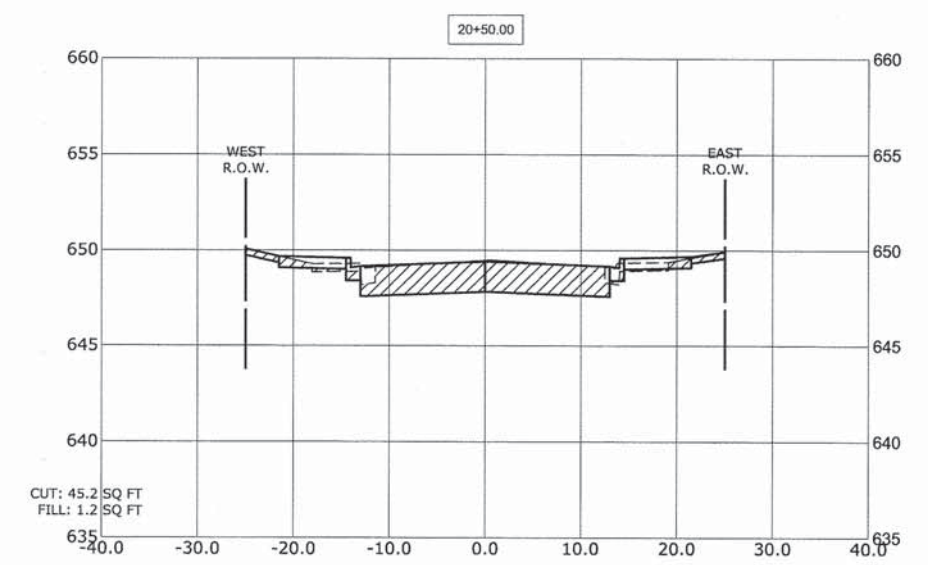
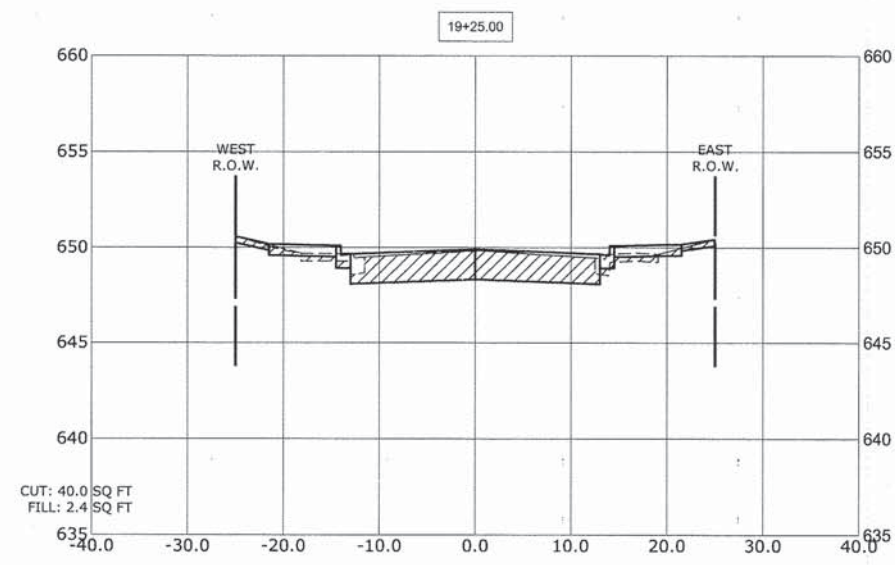
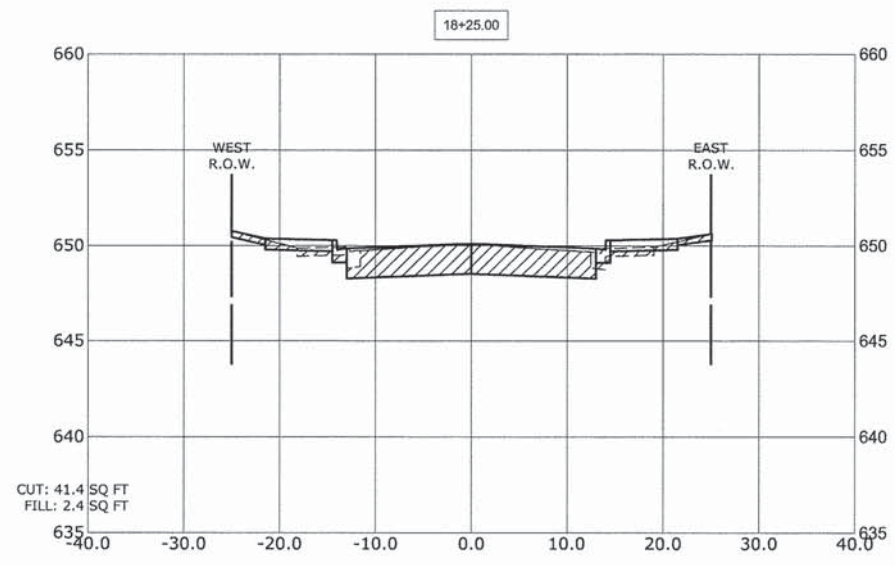
**CROSS SECTIONS**

SCALE: H:1"=10', V:1"=5' SHEET NO. 5 OF 8 SHEETS STA. - TO STA. -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	59
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

E.H.E. PROJECT NO. 612-13-23201

CROSS SECTION LEGEND	
C = CUT (SQ. FT.)	
F = FILL (SQ. FT.)	



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**HANCOCK ENGINEERING**  
 Civil Engineers  
 Municipal Consultants  
 Established 1911

USER NAME -	DESIGNED - SBC/CB	REVISED -
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

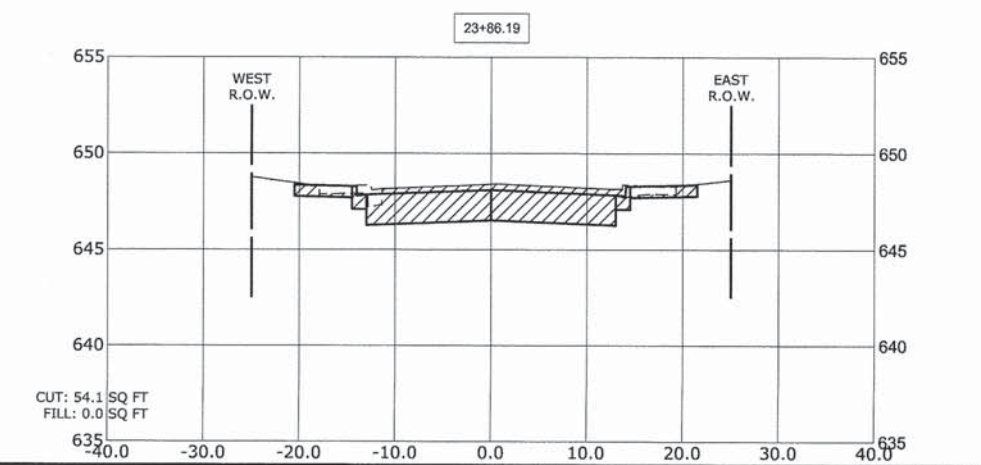
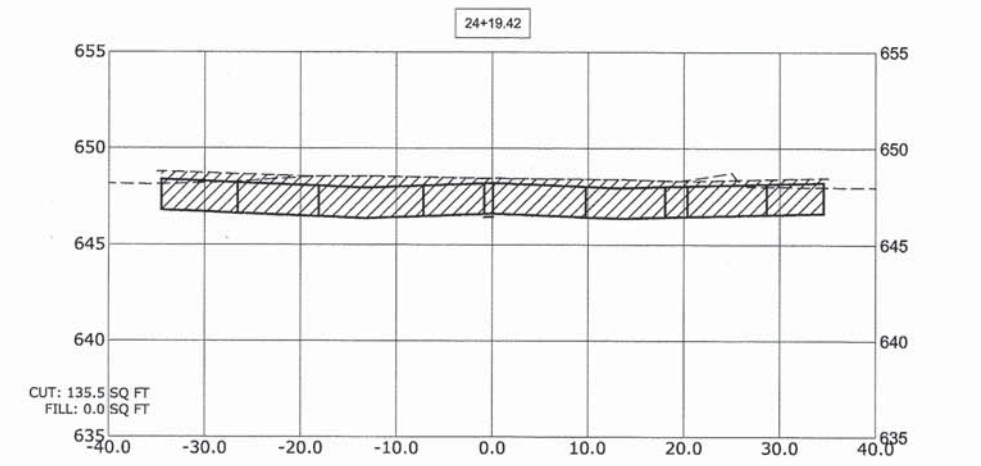
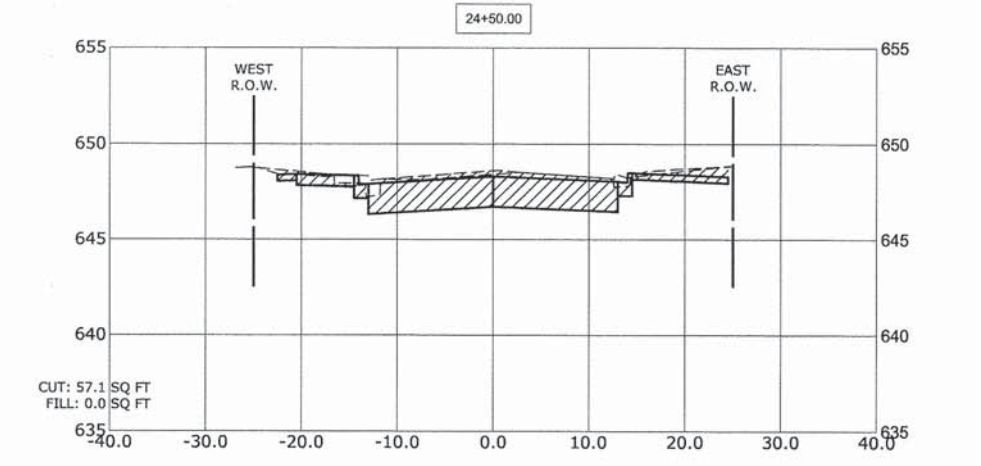
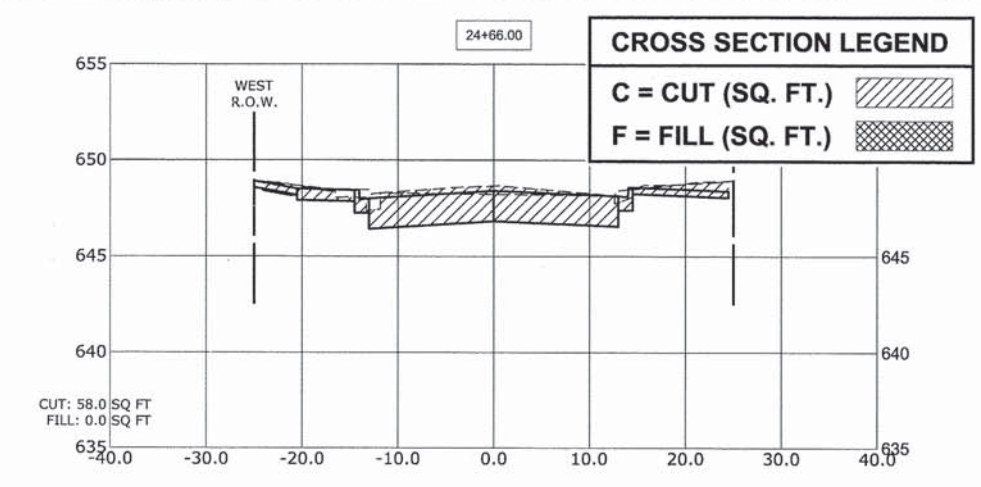
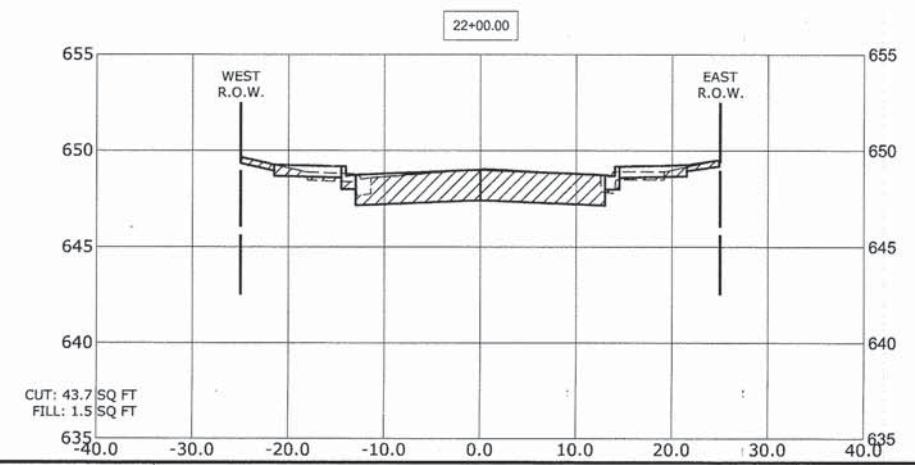
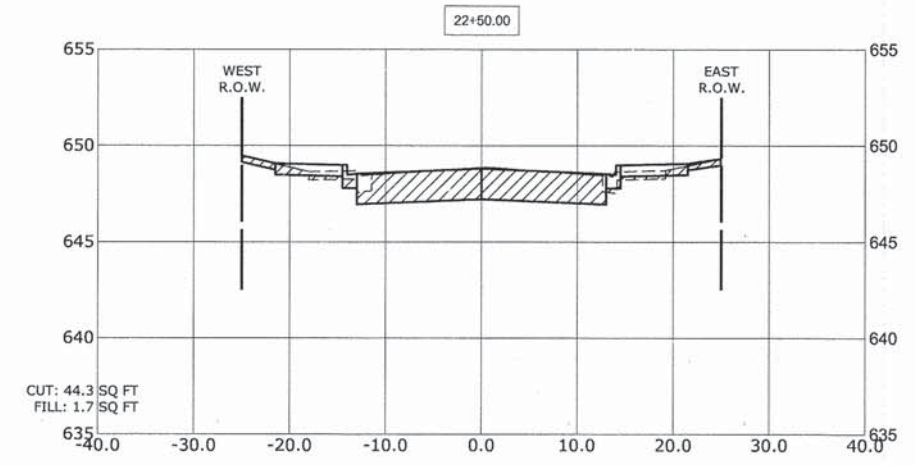
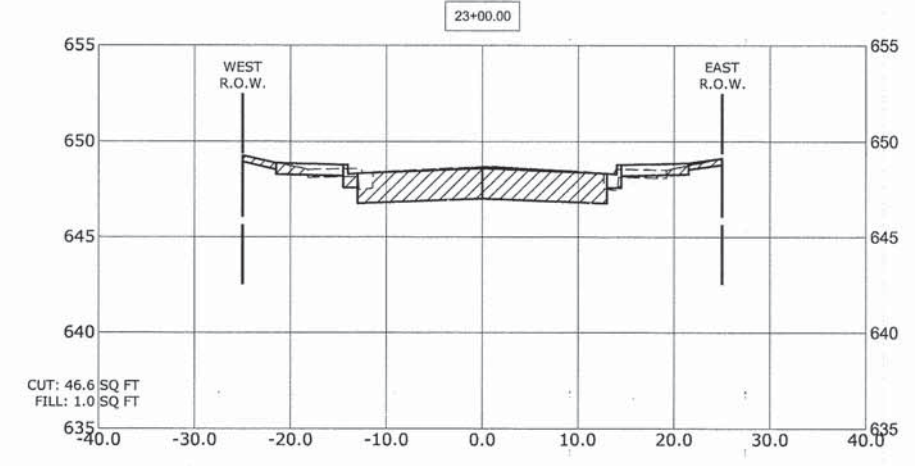
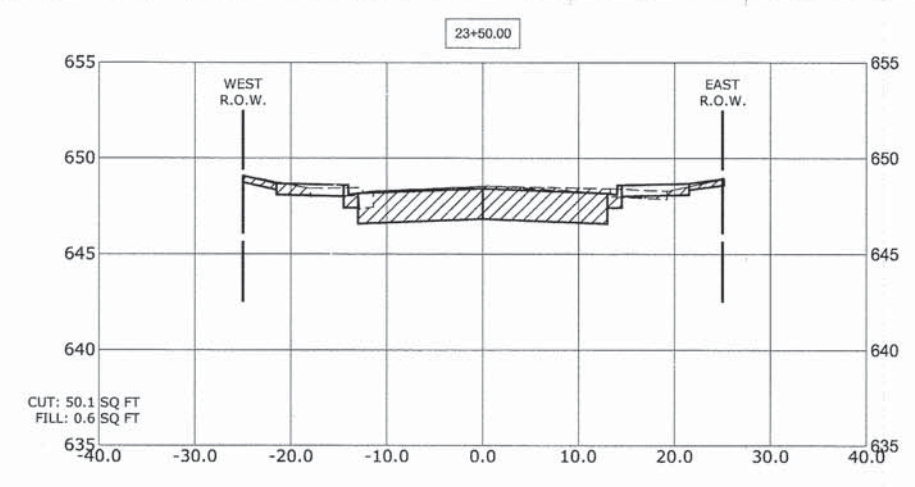
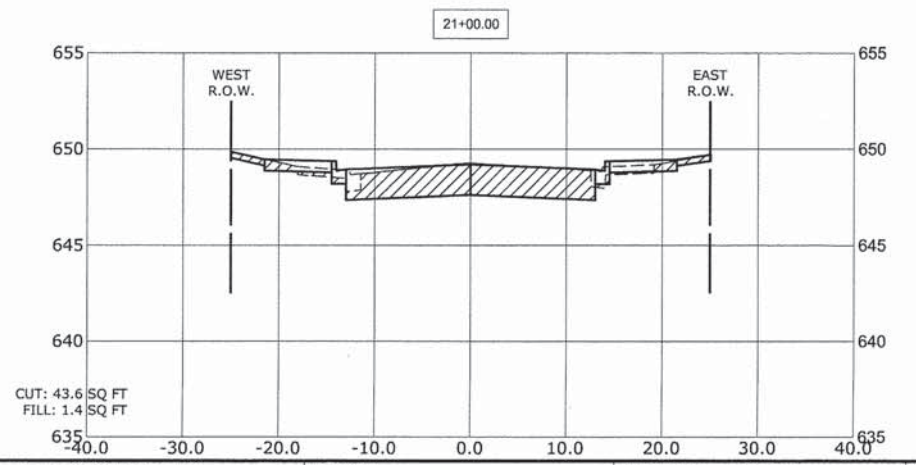
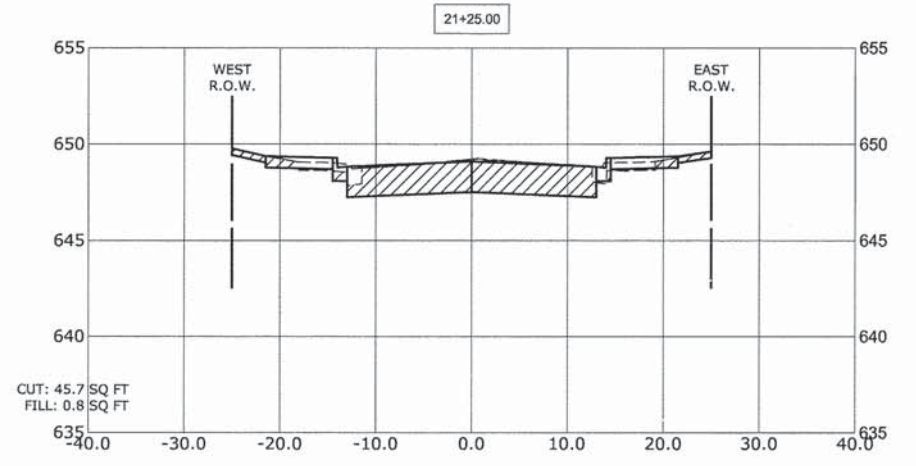
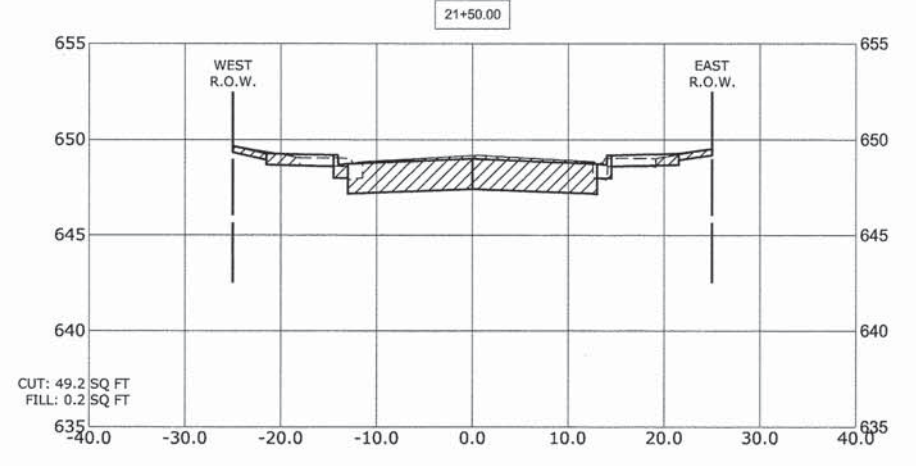
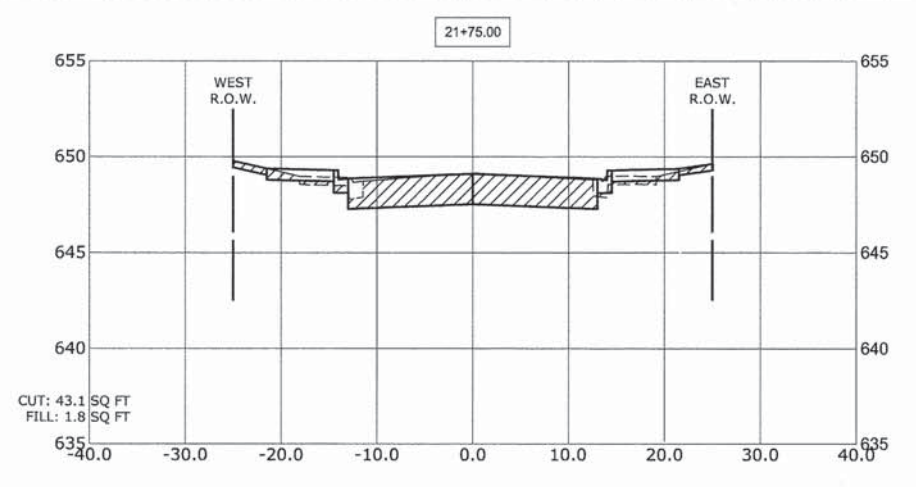
**OVERHILL AVENUE RECONSTRUCTION  
 VILLAGE OF NORRIDGE, ILLINOIS**

**CROSS SECTIONS**

SCALE: H:1"=10', V:1"=5'    SHEET NO. 6 OF 8 SHEETS    STA. -    TO STA. -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	60
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

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**CROSS SECTION LEGEND**

C = CUT (SQ. FT.)

F = FILL (SQ. FT.)



USER NAME -	DESIGNED - SBC/CB	REVISED -
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

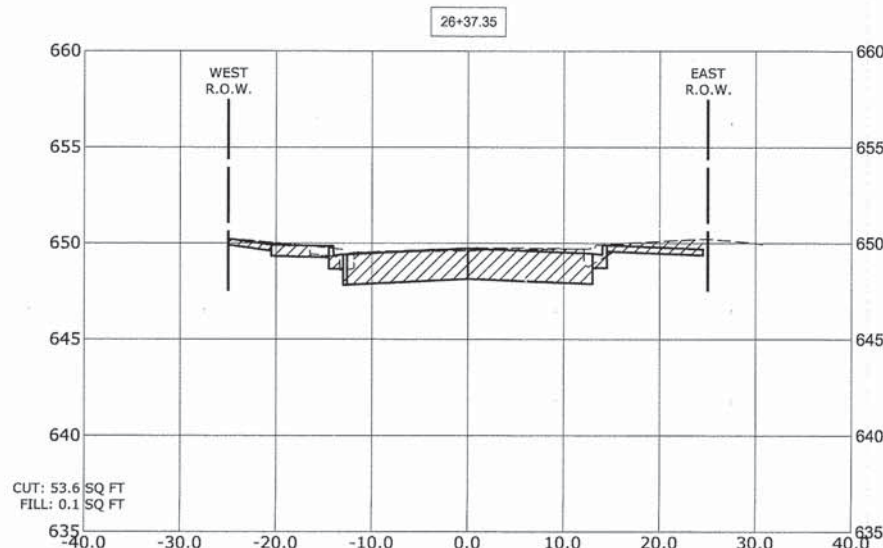
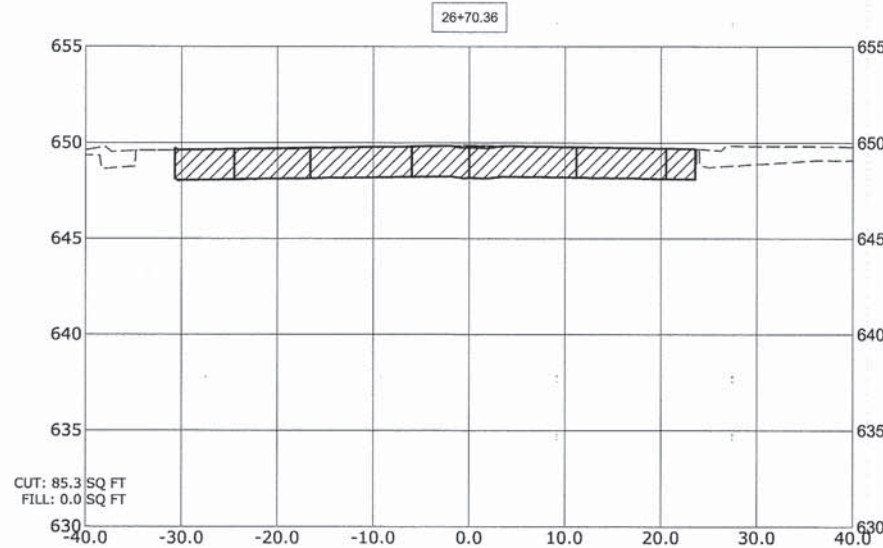
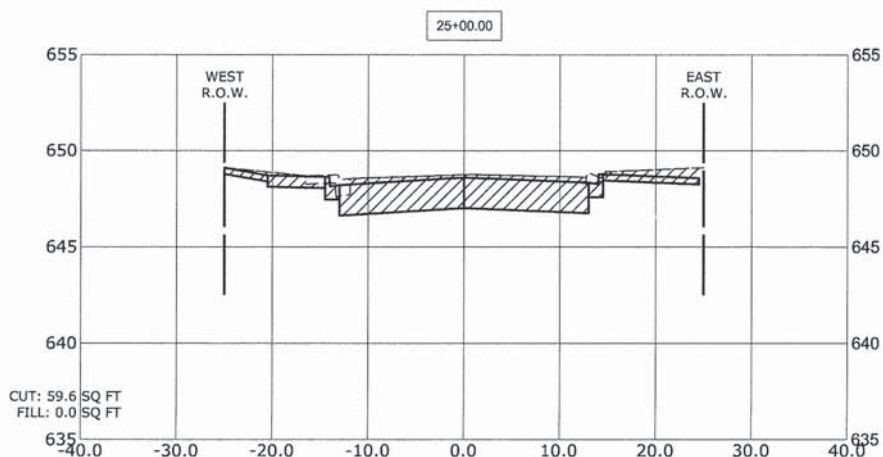
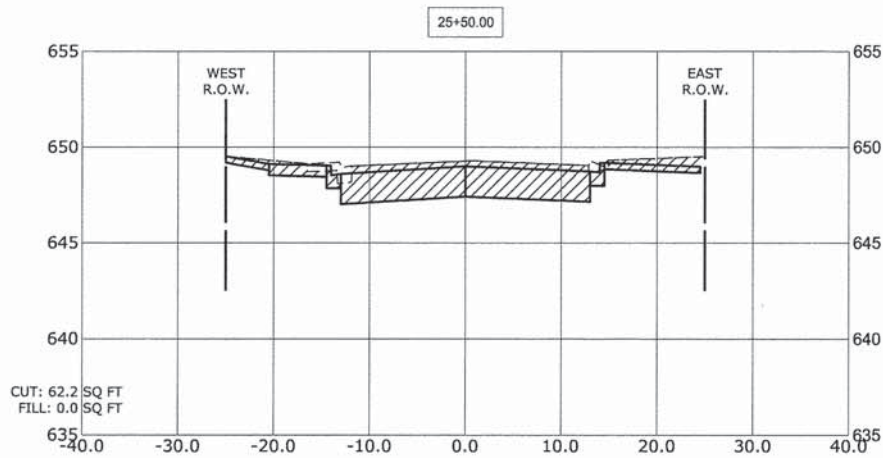
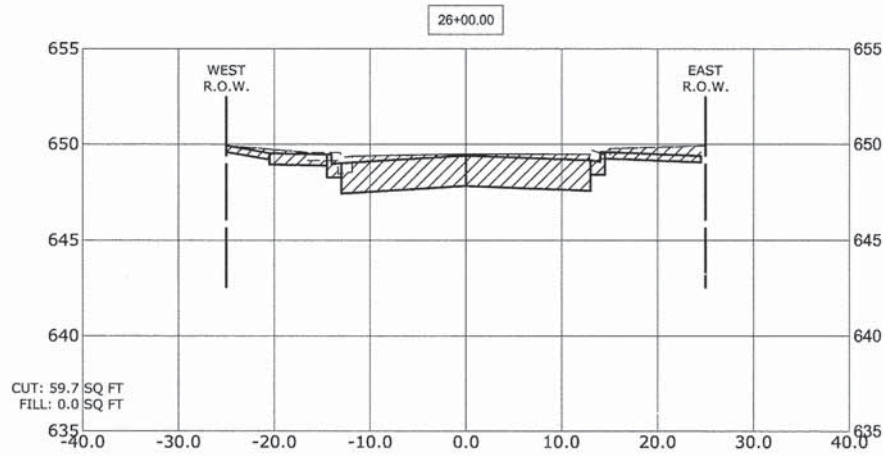
**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

**CROSS SECTIONS**

SCALE: H:1"=10',V:1"=5' SHEET NO. 7 OF 8 SHEETS STA. - TO STA. -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	61
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

CROSS SECTION LEGEND	
C = CUT (SQ. FT.)	
F = FILL (SQ. FT.)	



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**HANCOCK ENGINEERING**  
Civil Engineers  
Municipal Consultants  
Established 1911

USER NAME -	DESIGNED - SBC/CB	REVISED -
PLOT SCALE -	DRAWN - MK,DMM,ECW	REVISED -
PLOT DATE -	CHECKED - WOP	REVISED -
	DATE - 10/02/14	REVISED -

**OVERHILL AVENUE RECONSTRUCTION  
VILLAGE OF NORRIDGE, ILLINOIS**

**CROSS SECTIONS**

SCALE: H:1"=10', V:1"=5' SHEET NO. 8 OF 8 SHEETS STA. - TO STA. -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	13-00064-00-FP	COOK	62	62
FIELD BOOK NO. 1635/AERIALS		CONTRACT NO. 61A82		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		