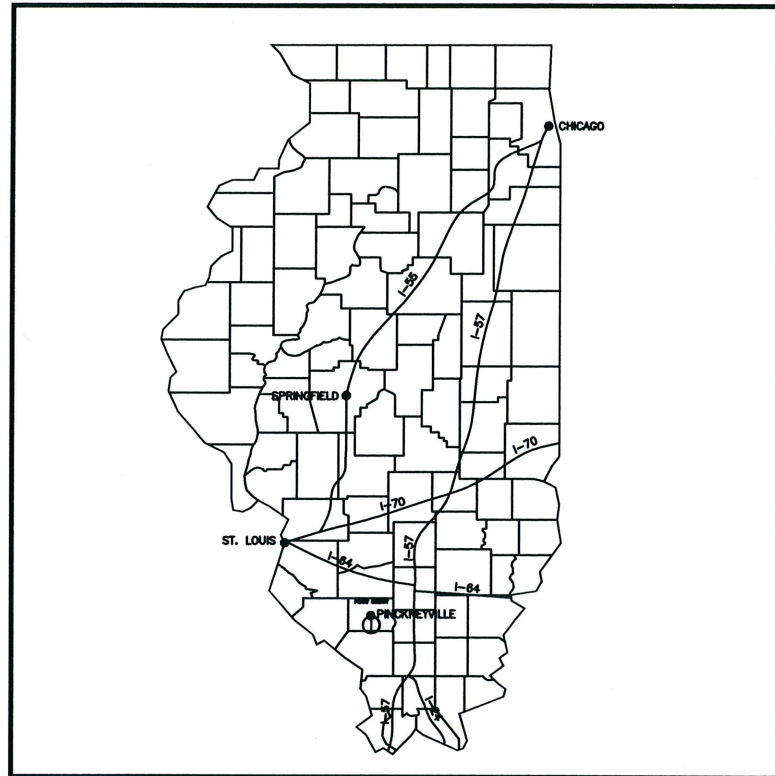


CONSTRUCTION PLANS FOR PINCKNEYVILLE-DU QUOIN AIRPORT

REHABILITATE RUNWAY, TAXIWAY AND APRON

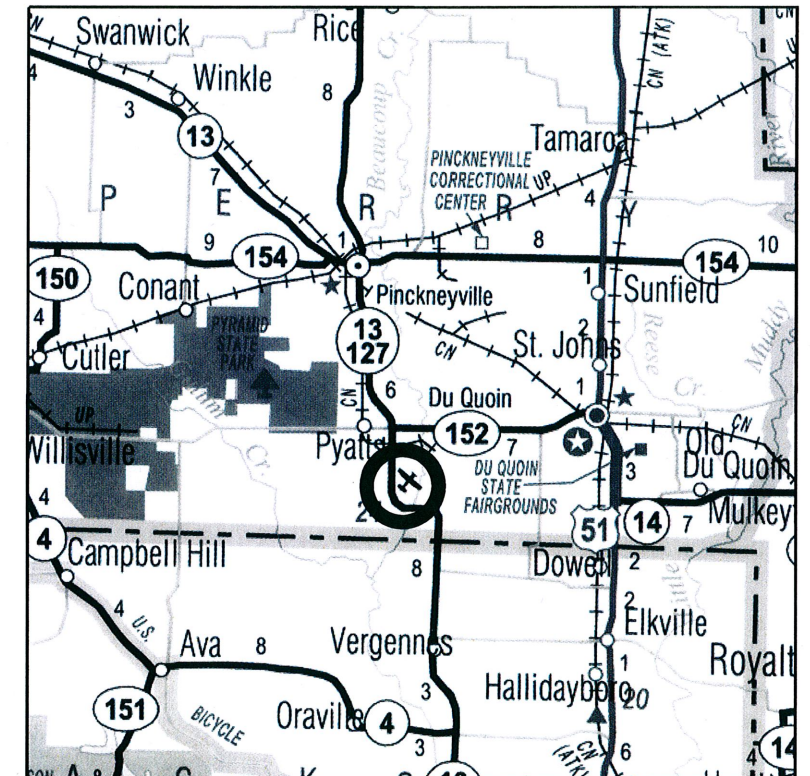
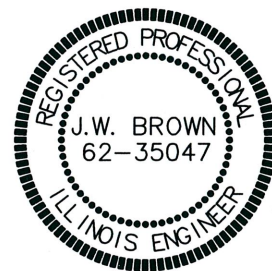


LOCATION MAP

ILLINOIS PROJECT NUMBER: PJY-4383
SBG PROJECT NUMBER: 3-17-SBGP-XX

PINCKNEYVILLE, ILLINOIS

DATE: NOVEMBER 14, 2014



VICINITY MAP

BROWN AND ROBERTS, INC.
CONSULTING ENGINEER
PRESIDENT
SUBMITTED BY: *J.W. Brown*
JIM W. BROWN, PRESIDENT
DATE SUBMITTED: 11/14/2014
LICENSE NUMBER: 062-035047
LICENSE EXPIRATION DATE: NOVEMBER 30, 2015

PLANS PREPARED BY:

BROWN AND ROBERTS, INC.
1 WESTRIDGE ROAD
HARRISBURG, IL. 62946
(618) 252-8111

PINCKNEYVILLE-DU QUOIN AIRPORT
ACTING CHAIRMAN
APPROVED BY: *Kevin Kuhnert* 11/13/14
KEVIN KUHNERT DATE
SECRETARY
ATTESTED BY: *Stephen Moss* 11/13/14
STEPHEN MOSS DATE

INDEX OF SHEETS	
SHEET NUMBER	DESCRIPTION
1	COVER SHEET
2	INDEX AND SUMMARY OF QUANTITIES
3	RUNWAY SAFETY PLAN
4	TYPICAL SECTIONS
5	RUNWAY 18-36 PLAN VIEW
6	APRON/TAXIWAY PLAN VIEW AND DETAILS
7	APRON DETAIL
8	RUNWAY/TAXIWAY INTERSECTION PLAN AND PROFILE

SUMMARY OF QUANTITIES			
ITEM NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY
AR150520	MOBILIZATION	L.S.	1
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	L.F.	8000
AR401613	BIT. SURF. CSE. - METHOD I, SUPERPAVE	TON	900
AR401650	BITUMINOUS PAVEMENT MILLING	S.Y.	9300
AR402622	POROUS FRICTION COURSE, 0.10'	S.Y.	30500
AR501507	7" PCC PAVEMENT	S.Y.	121
AR603510	BITUMINOUS TACK COAT	GAL.	10500
AR620520	PAVEMENT MARKING - WATERBORNE	S.F.	20000
AR620525	PAVEMENT MARKING - BLACK BORDER	S.F.	7500
AR801258	REMOVE & REPLACE UNSUITABLE SUBGRADE	S.Y.	800

SCOPE OF WORK

THE PROJECT SCOPE CONSISTS OF POROUS FRICTION COURSE OVERLAY OF THE RUNWAY AND TAXIWAY, RESURFACING OF THE EXISTING APRON, NEW MARKINGS AND OTHER NECESSARY AND RELATED WORK.

PROPOSED SAFETY PLAN

GENERAL – THE PINCKNEYVILLE-DU QUOIN AIRPORT CURRENTLY HAS A PAVED NORTH-SOUTH RUNWAY (4000-FT BY 60-FT).

PROJECT CONSTRUCTION SHALL BE STAGED TO MINIMIZE THE FREQUENCY OF CLOSURES OF THE EXISTING RUNWAY.

CONTRACTOR'S RESPONSIBILITIES

IDENTIFICATION – THE CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE PROPERLY MARKED WITH 3-FOOT SQUARE INTERNATIONAL ORANGE AND WHITE CHECKERED FLAGS ANYTIME THEY ARE ON AIRPORT PROPERTY.

THE CONTRACTOR AND HIS EMPLOYEES SHALL BE RESTRICTED TO THE WORK AREA.

EQUIPMENT PARKING AND STORAGE – THE CONTRACTOR'S EQUIPMENT PARKING, STORAGE, AND EMPLOYEE PARKING WILL BE AT THE LOCATION SHOWN ON THIS SHEET. ONLY CONTRACTOR VEHICLES AND EQUIPMENT REQUIRED FOR CONSTRUCTION WILL BE ALLOWED OUTSIDE THIS AREA.

BARRICADES AND TRAFFIC CONES – IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND TRAFFIC CONES AS REQUIRED AND AS DIRECTED BY THE RESIDENT ENGINEER. BARRICADES, THEIR MAINTENANCE, PLACEMENT, AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE CONTRACTOR WILL NOT BE ALLOWED ON ANY AIRPORT PAVEMENT. THE CONTRACTOR WILL BE RESPONSIBLE FOR REPAIRING ANY DAMAGE TO EXISTING PAVEMENTS CAUSED BY HIS PERSONNEL OR EQUIPMENT.

HAUL ROUTE AND EQUIPMENT PARKING

THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND EQUIPMENT PARKING AREA SHOWN ON THIS SAFETY PLAN. THE PROPOSED EQUIPMENT PARKING AREA WILL BE TWO AREAS APPROXIMATELY 70-FT BY 50-FT. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE AND PARKING AREA THROUGHOUT THE COURSE OF THE PROJECT. AT THE CONCLUSION OF THE PROJECT, ALL AREAS DISTURBED WILL BE RESTORED AS NEEDED TO ITS ORIGINAL STATE. RESTORATION OF THE HAUL ROUTE AND EQUIPMENT PARKING AREA WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

UTILITY NOTE

THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING ALL UTILITY COMPANIES AND ORGANIZATIONS THAT HAVE LINES OR CONDUITS IN THE PROPOSED WORK AREA. ALL LINES AND CONDUITS SHALL BE LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY EXCAVATION BEGINS. THE CONTRACTOR SHALL CALL JULIE (1-800-892-0123) TO ACCOMPLISH THESE REQUIREMENTS. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING ALL NON-JULIE UTILITIES LOCATED WITHIN THE PROPOSED CONSTRUCTION LIMITS. THESE UTILITIES ARE TO BE LOCATED PRIOR TO THE START OF CONSTRUCTION.

JULIE INFORMATION

COUNTY.....PERRY
 CITY.....PINCKNEYVILLE
 TOWNSHIP/RNG.....6S/2W
 SECTION NO.....30
 NEAREST MAJOR ROAD INTERSECTION...IL RTE 13/127 AND IL RTE 152
 AIRPORT ADDRESS...PINCKNEYVILLE-DU QUOIN AIRPORT
 1352 STATE ROUTE 13/127
 DU QUOIN, IL 62274

AIRPORT SECURITY

AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE PROPOSED HAUL ROUTE SHOWN ON THIS SAFETY PLAN IS THE ONLY ACCESS CONTRACTOR EQUIPMENT AND PERSONNEL WILL BE ALLOWED TO USE. THE CONTRACTOR SHALL PROVIDE BARRICADES AT THIS ACCESS AND ENSURE THE BARRICADES ARE IN PLACE AT THE END OF EACH WORKING DAY.

AIRCRAFT OPERATIONAL AREA

THE CONTRACTOR, HIS EMPLOYEES, OR ANY EQUIPMENT WILL NOT PROCEED WITH ANY WORK WITHIN THE AIRCRAFT OPERATIONAL AREA WITHOUT FIRST CLOSING THE RUNWAY.

NOTE:

THE COST OF CONSTRUCTING, PLACING, MAINTAINING, AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE RESIDENT ENGINEER. THE CROSSES WILL BE PLACED AT THE ENDS OF THE RUNWAY AND SECURED IN A MANNER APPROVED BY THE RESIDENT ENGINEER. THE PROPOSED CROSSES WILL BE PLACED WHEN THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES AT NO ADDITIONAL COST TO THE CONTRACT.

RUNWAY CLOSURE PROCEDURES:

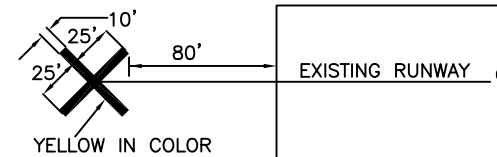
- * CONTACT THE AIRPORT MANAGER OR HIS ASSIGNED REPRESENTATIVE.
- * ISSUANCE OF NOTAM BY THE AIRPORT MANAGER OR HIS ASSIGNED REPRESENTATIVE.
- * PLACEMENT OF CROSSES (SEE DETAIL THIS SHEET).
- * PLACEMENT OF LIGHTED BARRICADES. ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS WITHIN 200-FT OF THE AFFECTED RUNWAY CENTERLINE AND WITHIN 600 FT OF THE RUNWAY END BEGIN.
- * RUNWAY LIGHTS SHALL BE DISABLED

RUNWAY RE-OPENING PROCEDURES:

- * REMOVE CROSSES.
- * REMOVE LIGHTED BARRICADES.
- * NOTIFY THE AIRPORT MANAGER OR HIS REPRESENTATIVE TO CANCEL THE NOTAM.
- * CANCELLATION OF THE NOTAM. A CLOSED RUNWAY WILL NOT BE RE-OPENED UNTIL ALL EQUIPMENT AND WORK ARE FURTHER THAN 200 FT. FROM THE AFFECTED RUNWAY CENTERLINE
- * RUNWAY LIGHTS SHALL BE REACTIVATED.

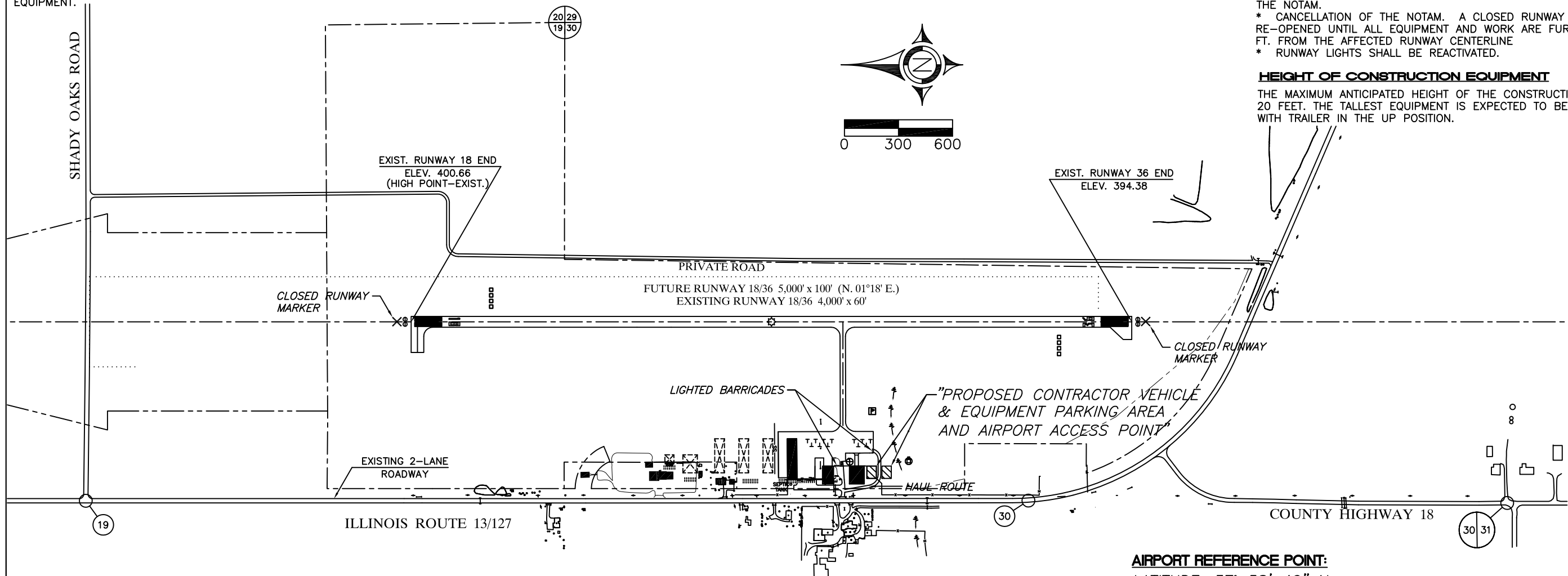
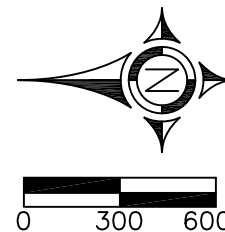
HEIGHT OF CONSTRUCTION EQUIPMENT

THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT IS 20 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A SEMI-TRUCK WITH TRAILER IN THE UP POSITION.



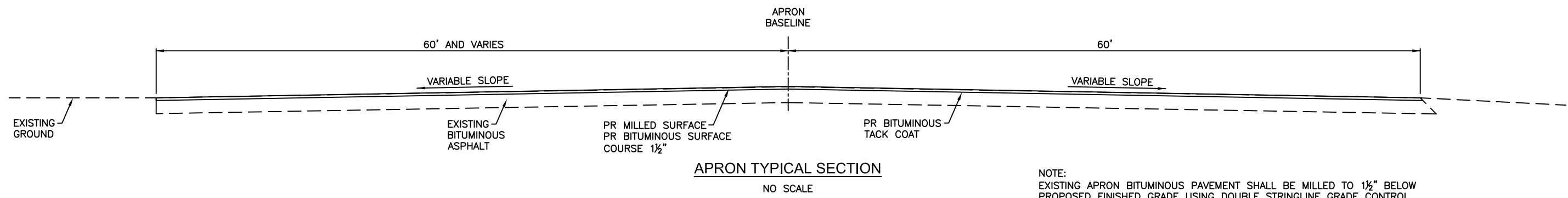
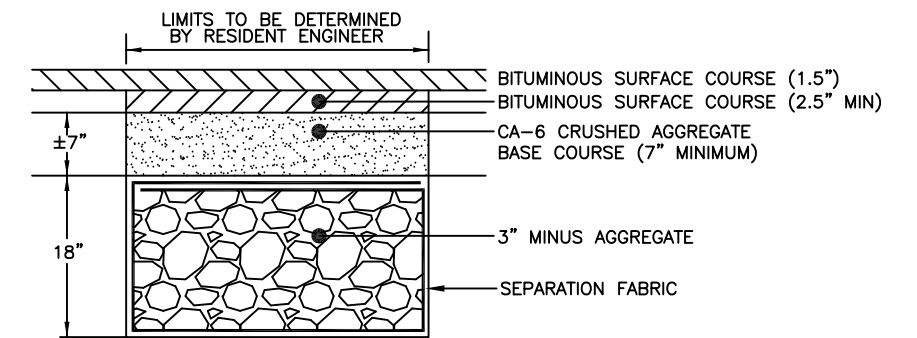
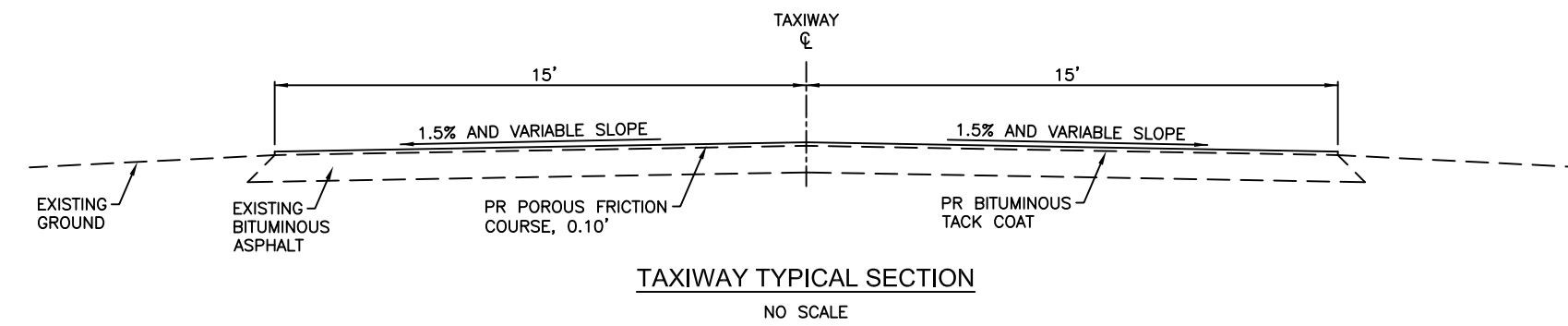
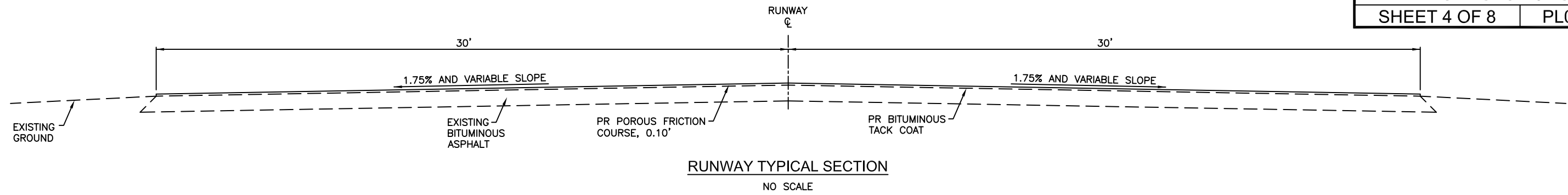
DETAIL OF CROSS FOR CLOSED RUNWAY

"NOT TO SCALE"

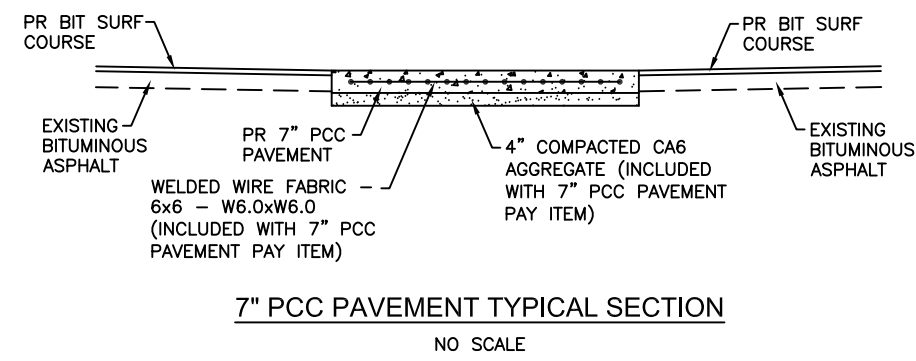


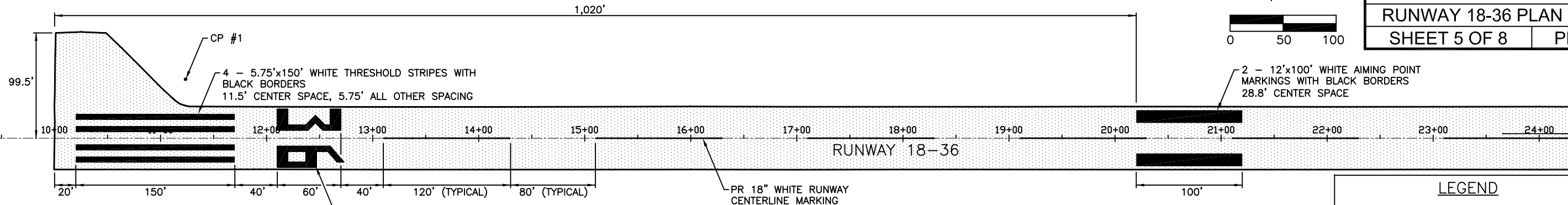
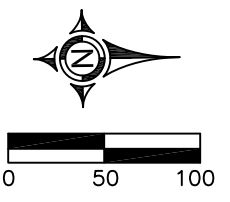
AIRPORT REFERENCE POINT:

LATITUDE: 37° 58' 40" N
 LONGITUDE: 89° 21' 38" W
 ELEVATION: 401 MSL



NOTE:
EXISTING APRON BITUMINOUS PAVEMENT SHALL BE MILLED TO 1 1/2" BELOW PROPOSED FINISHED GRADE USING DOUBLE STRINGLINE GRADE CONTROL. AREAS OF ZERO MILLING WILL REQUIRE ADDITIONAL BITUMINOUS SURFACE COURSE ABOVE THE TYPICAL 1 1/2" MINIMUM PROPOSED.

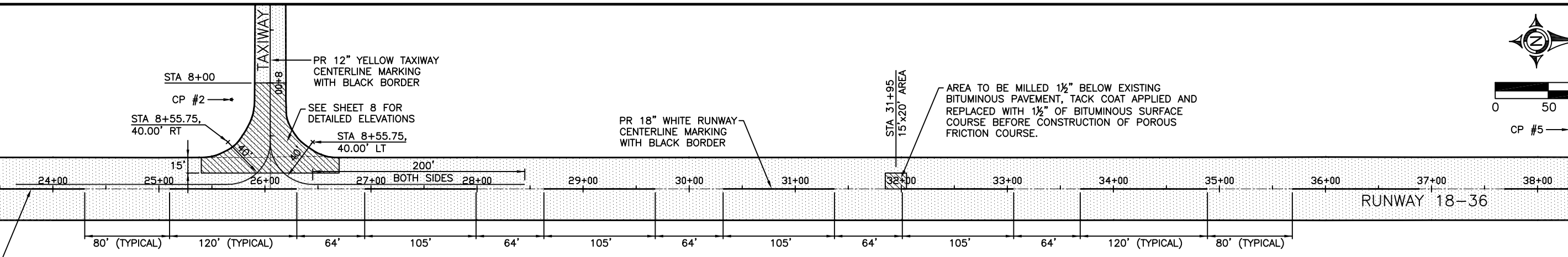




BENCHMARK AND CONTROL POINT #1
 STA 11+23.76, 55.67' LT
 N: 476543.27, E: 2528883.80
 ELEV = 391.85
 NGS MONUMENT

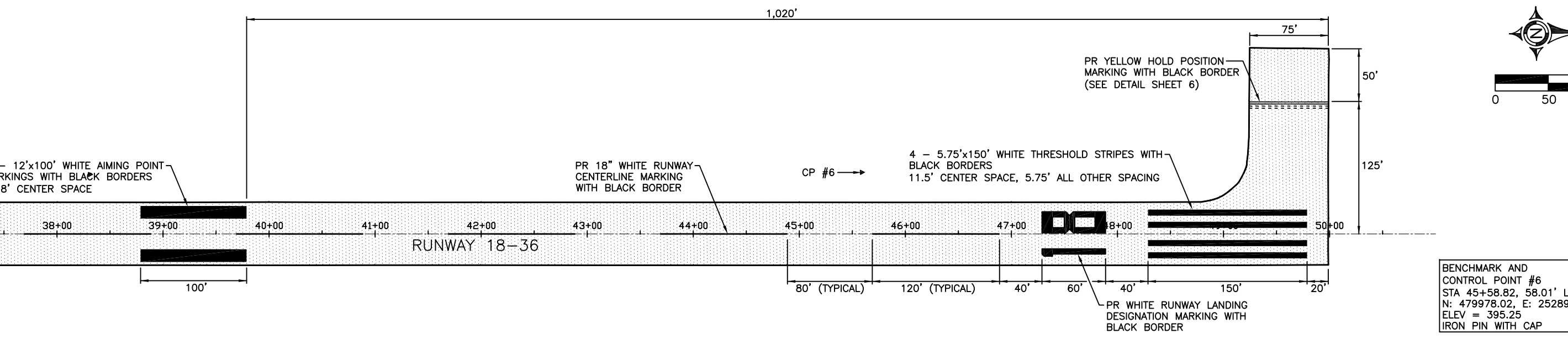
LEGEND

- PR BITUMINOUS TACK COAT AND POROUS FRICTION COURSE, 0.10'
- PR BITUMINOUS PAVEMENT MILLING, BITUMINOUS TACK COAT AND BITUMINOUS SURFACE COURSE



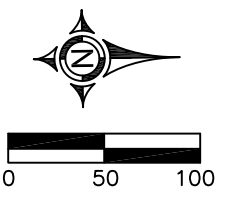
BENCHMARK AND CONTROL POINT #2
 STA 25+68.71, 84.36' LT
 N: 477988.48, E: 2528875.55
 ELEV = 391.64
 NGS MONUMENT

BENCHMARK AND CONTROL POINT #5
 STA 38+30.92, 56.00' LT
 N: 479250.17, E: 2528921.77
 ELEV = 392.30
 IRON PIN WITH CAP



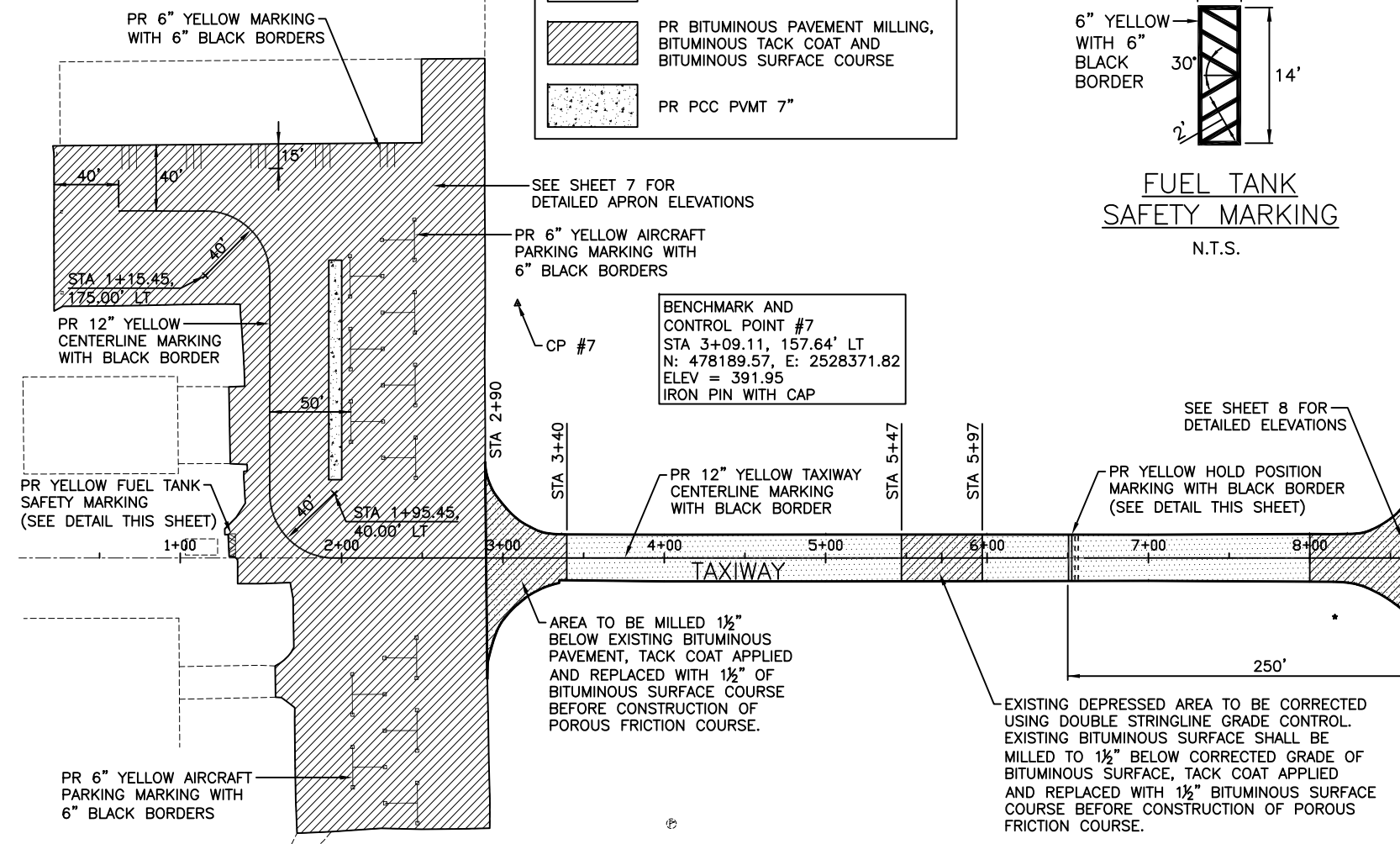
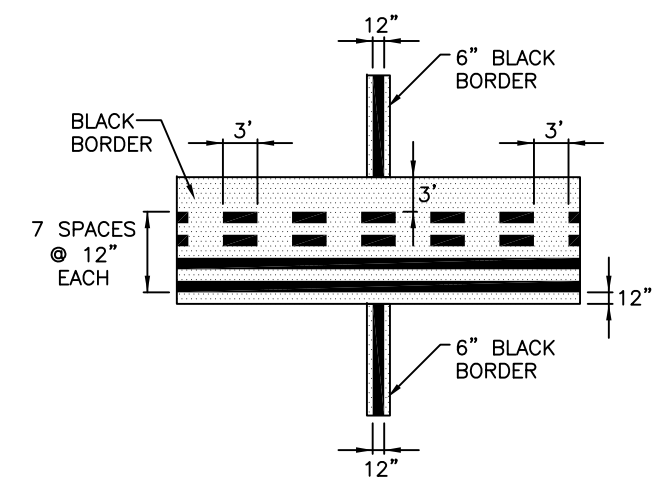
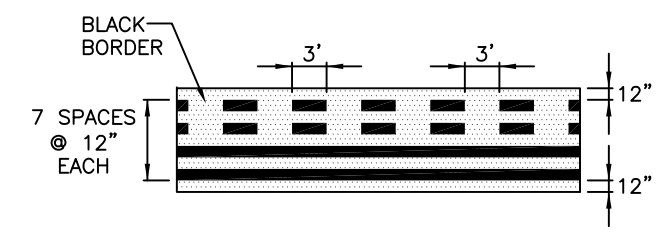
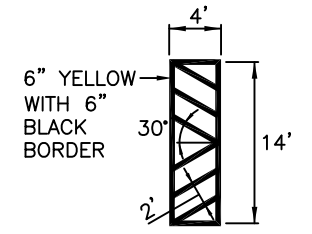
BENCHMARK AND CONTROL POINT #6
 STA 45+58.82, 58.01' LT
 N: 479978.02, E: 2528930.06
 ELEV = 395.25
 IRON PIN WITH CAP

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LEGEND

	PR BITUMINOUS TACK COAT AND POROUS FRICTION COURSE, 0.10'
	PR BITUMINOUS PAVEMENT MILLING, BITUMINOUS TACK COAT AND BITUMINOUS SURFACE COURSE
	PR PCC PVMT 7"



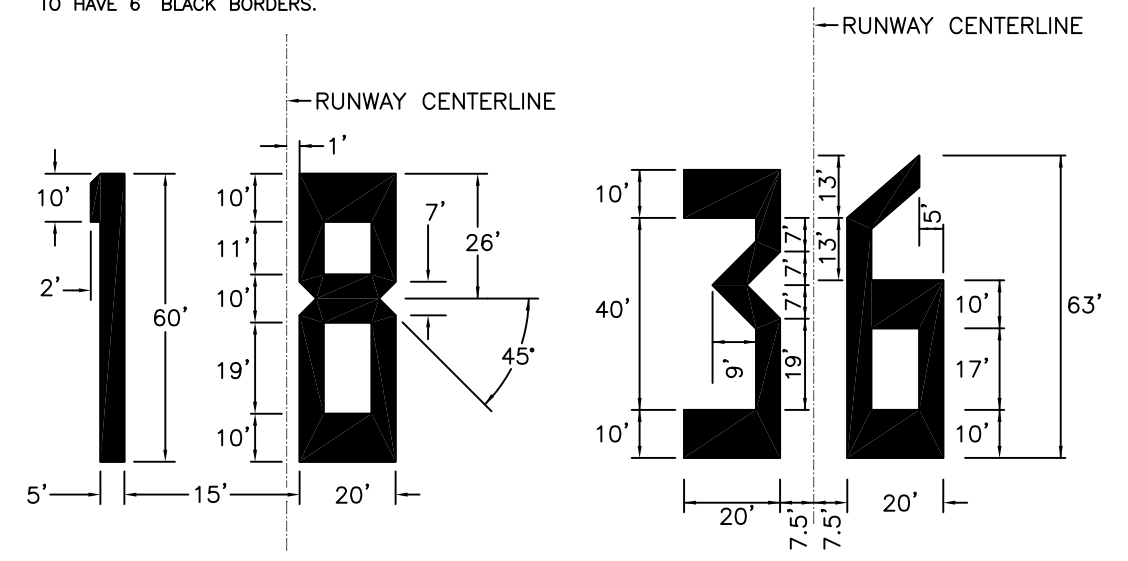
PAVEMENT MARKING - WATERBORNE SCHEDULE

DESCRIPTION	UNIT AREA (SQ FT)	NO. REQ'D	TOTAL (SQ FT)
NUMERAL 1	318	1	318
NUMERAL 8	876	1	876
NUMERAL 3	633	1	633
NUMERAL 6	712	1	712
5.75' x 150' STRIPE	863	8	6904
12' x 100' STRIPE	1200	4	4800
CENTERLINE STRIPE - 120'	180	14	2520
CENTERLINE STRIPE - 105'	157.5	4	630
TOTAL WHITE			17393
HOLD POSITION MARKING - RNWY 18	228	1	228
HOLD POSITION MARKING - TAXIWAY	96	1	96
TAXIWAY CENTERLINE	1495	1	1495
AIRCRAFT PARKING	22.5	12	270
T-HANGAR PARKING GUIDES	7.5	15	112.5
FUEL TANK SAFETY MARKING	33	1	33
TOTAL YELLOW			2235
TOTAL WATERBORNE			19628

PAVEMENT MARKING - BLACK BORDER SCHEDULE





DESCRIPTION	UNIT AREA (SQ FT)	NO. REQ'D	TOTAL (SQ FT)
NUMERAL 1	68	1	68
NUMERAL 8	133	1	133
NUMERAL 3	103	1	103
NUMERAL 6	114	1	114
5.75' x 150' STRIPE	157	8	1256
12' x 100' STRIPE	113	4	452
CENTERLINE STRIPE - 120'	123	14	1722
CENTERLINE STRIPE - 105'	108	4	432
HOLD POSITION MARKING - RNWY 18	447	1	447
HOLD POSITION MARKING - TAXIWAY	234	1	234
TAXIWAY CENTERLINE	1495	1	1495
AIRCRAFT PARKING	45	12	540
T-HANGAR PARKING GUIDES	15	15	225
FUEL TANK SAFETY MARKING	66	1	66
TOTAL BLACK BORDER			7287

NOTE:
 RUNWAY DESIGNATION NUMERALS TO HAVE 6" BLACK BORDERS.

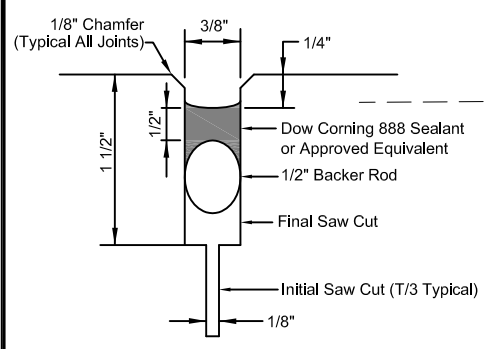
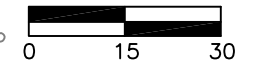


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LEGEND

-  PR BITUMINOUS TACK COAT AND POROUS FRICTION COURSE, 0.10'
-  PR BITUMINOUS PAVEMENT MILLING, BITUMINOUS TACK COAT AND BITUMINOUS SURFACE COURSE
-  PR PCC PVMT 7"
-  PROPOSED/EXISTING ELEVATIONS

NOTE:
 EXISTING BITUMINOUS PAVEMENT SHALL BE MILLED TO 1/2" BELOW PROPOSED FINISHED GRADE USING DOUBLE STRINGLINE GRADE CONTROL. AREAS OF ZERO MILLING WILL REQUIRE ADDITIONAL BITUMINOUS SURFACE COURSE ABOVE THE TYPICAL 1/2" MINIMUM PROPOSED.



JOINT DETAIL 'A'
 NO SCALE

EXISTING MAINTENANCE HANGAR

EXISTING T-HANGAR

EXISTING TURF AREA

EXISTING TERMINAL BUILDING/T-HANGAR

EXISTING TURF AREA

EXISTING TURF AREA

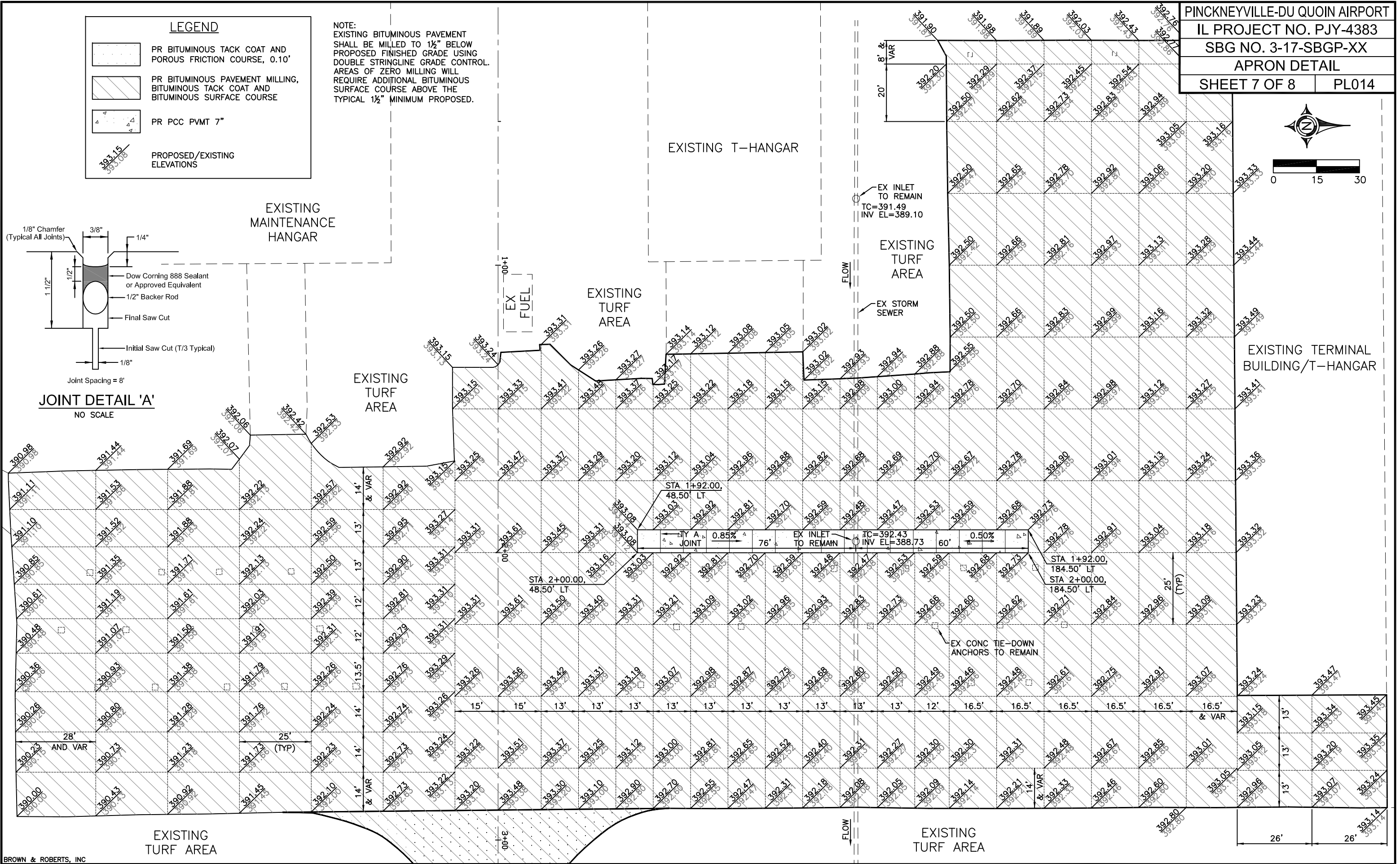
EX FUEL

EX INLET TO REMAIN
 TC=391.49
 INV EL=389.10

FLOW

EXISTING TURF AREA

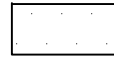
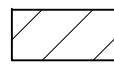
EX STORM SEWER



EXISTING TURF AREA

EXISTING TURF AREA

LEGEND

-  PR BITUMINOUS TACK COAT AND POROUS FRICTION COURSE, 0.10"
-  PR BITUMINOUS PAVEMENT MILLING, BITUMINOUS TACK COAT AND BITUMINOUS SURFACE COURSE 1 1/2"

NOTE:
 PROPOSED GRADES SHOWN ARE FOR TOP OF BITUMINOUS SURFACE COURSE BEFORE CONSTRUCTION OF POROUS FRICTION COURSE. EXISTING BITUMINOUS PAVEMENT SHALL BE MILLED TO 1/2" BELOW PROPOSED FINISHED GRADE USING DOUBLE STRINGLINE GRADE CONTROL.

