STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION **DIVISION OF HIGHWAYS**

PROPOSED HIGHWAY PLANS

FAU 1008 (CLAIRE BOULEVARD) I-294 TO 139TH STREET RESURFACING SECTION NO.: 13-00028-00-RS PROJECT NO.: M-4003(255) **VILLAGE OF ROBBINS COOK COUNTY** JOB NO.: C-91-131-14

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

J.U.L.I.E. DESIGN STAGE REQUEST DIG. No. X2881739



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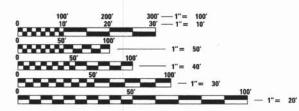
CONTACT JULIE AT 811 OR 800-892-0123

CITY-TWNSHP. = ROBBINS-BREMEN SEC. & 1/4 SEC. NO. = S2 SW & SE 1/4 T36N R13E 48 HOURS (2 working days) BEFORE YOU DIG

TRAFFIC DATA CLAIRE BOULEVARD POSTED AND DESIGN SPEED = 30 MPH 2008 ADT = 8,100

DESIGN DESIGNATION ROADWAY CLASSIFICATION: COLLECTOR

BAXTER & WOODMAN, INC. STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM LICENSE NO. 184-001121 EXPIRES 4/30/2015



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



FAU 1008 CLAIRE BOULEVARD IMPROVEMENT BEGINS STA 19+40

BREMEN TOWNSHIP GROSS LENGTH OF IMPROVEMENT = 2,923 LF OR 0.554 MILES

NET LENGTH OF IMPROVEMENT = 2,923 LF OR 0.554 MILES

WOODMAN

13-00028-00-RS COOK 17 1 TILINOIS CONTRACT NO. 61406



FAU 1008 CLAIRE BOULEVARD

IMPROVEMENT ENDS

STA 48+63

PROJECT ENGINEER
"LICENSE EXPIRES 11-30-2015" STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION **DIVISION OF HIGHWAYS**

PROFESSIONAL

ENGINEER OF

PASSED DECEMBER 10, 2013 DISTRICT | ENGINEER OF

RELEASING FOR BID LOCAL ROADS & STREETS BASED ON LIMITED

ber 17 2013 In Formoun & HIGHWAYS, REGION 1 ENGINEER

OF THE STATE OF ILLINOIS B&W PROJECT NO.: 130097.40

PRINTED BY THE AUTHORITY

CONTRACT NO. 61A06

GENERAL NOTES

- 1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE JANUARY 1, 2012 EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
- 2. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES, INCLUDING SPRINKLER SYSTEMS, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS.
- 3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES, INCLUDING SPRINKLER SYSTEMS, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY OR SPRINKLER SYSTEM THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER.
- 4. THE CONTRACTOR SHALL NOTIFY THE VILLAGE OF ROBBINS PUBLIC WORKS ADMINISTRATOR (708-254-2715) AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER.
- 5. THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING BAN" IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE VILLAGE OF ROBBINS RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER IF DEEMED NECESSARY.
- 5. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY THE ENGINEER, RESIDENTS, AND THE VILLAGE OF ROBBINS WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO CURB AND GUTTER AND/OR DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE ENGINEER TO RESIDENTS. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED.
- 7. ACCESS TO PRIVATE DRIVEWAYS SHALL BE PROVIDED AT ALL TIMES EXCEPT DURING ACTUAL CONSTRUCTION ADJACENT THERE TO. TEMPORARY RAMPS SHALL BE CONSTRUCTED AS NEEDED TO PROVIDE SUCH ACCESS, UTILIZING CRUSHED STONE OR CRUSHED GRAVEL. THIS WORK SHALL BE INCLUDED IN THE PAY ITEM COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- 8. EXISTING PAVEMENT, DRIVEWAY PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE PRICE OF THE APPROPRIATE REMOVAL PAY ITEM.
- IN AREAS WHERE THE EXISTING DRIVEWAY, SIDEWALK, OR CURB AND GUTTER IS TO BE REMOVED AND REPLACED, THE REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL REQUIRED TO ESTABLISH THE PROPOSED DRIVEWAY, SIDEWALK, OR CURB AND GUTTER SUBGRADE ELEVATION SHALL BE INCLUDED IN THE PAY ITEMS, DRIVEWAY PAVEMENT REMOVAL, SIDEWALK REMOVAL OR COMBINATION CURB AND GUTTER REMOVAL.
- 10. CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
- 11. THE FINISHED HOT-MIX ASPHALT SURFACE SHALL BE CONSTRUCTED 0.25 INCH ABOVE THE GUTTER FLAG.
- 12. THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED. ONE (1) WEIGHTED SANDBAG SHALL BE PLACED ACROSS EACH BOTTOM RAIL.
- 3. PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 6-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES DRIVEWAYS. TRANSVERSE EXPANSION JOINTS 3/4" SHALL BE PLACED EVERY 50 FEET OR AS DETERMINED BY THE ENGINEER. TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED EVERY 5-FEET. THIS WORK SHALL BE INCLUDED IN THE PAY ITEM PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH.
- 14. A 1/2-INCH THICK EXPANSION JOINT SHALL BE PROVIDED AT THE JUNCTION OF THE DRIVEWAY APRON AND CURB, AND AT THE JUNCTION OF THE DRIVEWAY APRON AND THE SIDEWALK. THIS WORK WILL BE INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT OR PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH.
- 15. DETECTABLE WARNINGS SHALL BE CONSTRUCTED WITH THE INSTALLATION OF A CAST-IN-PLACE PANEL. THE PANEL SHALL BE A POLYMER COMPOSITE AND COMPLY WITH ADA REQUIREMENTS. THE DOMES LOCATED ON THE PANEL SHALL PARALLEL THE PAVEMENT CROSS WALK WITH THE CLOSEST EDGE LOCATED AT THE BACK OF CURB. THE PANEL COLOR SHALL BE COORDINATED FOR ACCEPTANCE BY THE VILLAGE. INSTALLATION SHALL OCCUR IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATIONS.

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-07	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-01	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-01	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424021-02	DEPRESSED CORNER FOR SIDEWALKS
424026-01	ENTRANCE/ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C & D PATCHES
604001-03	FRAMES AND LIDS, TYPE 1
606001-05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	LANE CLOSURE, MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-03	TRAFFIC CONTROL DEVICES
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTOR LOOPS

BAYTERSWOODMAN	
BAXTER WOODMAN Consulting Engineers	

2013, NOIS

DESIGNED	-	JDM	REVISED -
DRAWN	-	KAR	REVISED -
CHECKED	-	MWP	REVISED -
DATE	-	10-18-13	FILE - 130097SHT-GenNotes.dgn

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

- 16. ALL CRACKS AND JOINTS SHALL BE CLEANED PRIOR TO FILLING THEM. THIS WORK SHALL BE INCLUDED IN THE ITEM "MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS."
- 17. THE PRIME COAT APPLICATION RATE SHALL BE 0.1 GAL/SY.
- 18. THE DETAIL FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT SHOWN IN THESE DOCUMENTS SHALL BE MODIFIED TO INCLUDE THE FOLLOWING. THE WORK SHALL INCLUDE SAW-CUTTING AND REMOVING THE EXISTING PAVEMENT A MINIMUM OF 6-INCHES MEASURED FROM THE EXISTING EDGE OF PAVEMENT, AND FILLING THE 6" GAP WITH CLASS SI CONCRETE TO AN ELEVATION 2-1/4" BELOW THE PROPOSED CURB AND GUTTER FLAG. IF THE CONCRETE IS POURED HIGHER THAN 2-1/4" FROM THE GUTTER FLAG FOR STREETS TO BE RESURFACED, THE CONTRACTOR WILL BE REQUIRED TO GRIND ADDITIONAL CONCRETE TO THE REQUIRED 2-1/4" DEPTH. THE AREA BEHIND THE PROPOSED CURB AND GUTTER SHALL BE RESTORED WITH NATIVE SOD IN ACCORDANCE WITH ARTICLE 1081.03 RATHER THAN SALT TOLERANT SOD.
- 19. CONCRETE DRIVEWAY PAVEMENT SHALL NOT BE REMOVED FOR CURB AND GUTTER FORMING PURPOSES UNLESS DETERMINED BY THE ENGINEER. ANY DAMAGE TO CONCRETE DRIVEWAYS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- 20. ALL CONSTRUCTION PERSONNEL WILL BE REQUIRED TO WEAR A FLUORESCENT VEST PER ARTICLE 701.12 AND 701.13 OF THE STANDARD SPECIFICATIONS AT ALL TIMES WHILE ON THE CONSTRUCTION SITE. COMPLIANCE WITH THIS REQUIREMENT SHALL BE INCLUDED IN THE CONTRACT.
- 21. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKS
 UNTIL THE FINGINEER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- 22. AGGREGATE WEDGE SHOULDER, TYPE B SHALL MEET THE REQUIREMENTS OF ARTICLE 1004.4 OF THE STANDARD SPECIFICATIONS. RECLAIMED ASPHALT PAVEMENT (RAP) SHALL NOT BE ALLOWED AS AN ACCEPTABLE MATERIAL FOR AGGREGATE WEDGE SHOULDER, TYPE B.
- 23. THE CONTRACTOR SHALL REPLACE ALL STREET SIGNS AND MAIL BOXES REMOVED DURING CONSTRUCTION AS NEAR AS POSSIBLE TO THEIR ORIGINAL LOCATION OR AS DETERMINED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE CONTRACT.
- 24. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED)
- 25. MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, DRIVEWAYS, CURB AND GUTTER, HOT-MIX ASPHALT SURFACES, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGEMENT OF THE ENGINEER, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE ENGINEER WILL HAVE THE MATERIAL REMOVED BY THE LOCAL AGENCY. THE COST OF THIS WORK SHALL BE CALCULATED IN ACCORDANCE WITH SECTION 109,04 AND SHALL BE DEDUCTED FROM THE FINAL PAYMENT.
- 26. THE CONTRACTOR SHALL LIMIT THE WORK TO ONE SIDE OF THE STREET UNLESS AS DETERMINED BY THE ENGINEER.
- 27. THE LOCATIONS OF THE CLASS D PATCHES SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
- 28. THE DAY'S PAVING OPERATION SHALL RESULT IN A SINGLE TRANSVERSE JOINT. ANY COLD LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY PAVING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE PAVING OF THE ADJACENT LANE IN THE SAME DAY.
- 29. THE CONTRACTOR SHALL NOTIFY IDOT BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS PRIOR TO THE PLACEMENT OF HOT-MIX ASPHALT OR PORTLAND CEMENT CONCRETE.
- 30. FOR STEEL BARS CERTIFICATION, PLEASE CONTACT IDOT BUREAU OF MATERIALS AT (847) 705-4337.
- 31. ALL FRAME AND LID CASTINGS LOCATED WITHIN THE PAVEMENT WHICH REQUIRE RESETTING TO FINISH GRADE SHALL BE BACKFILLED WITH CLASS SI CONCRETE AND ALLOWED TO CURE FOR 72 HOURS PRIOR TO PLACEMENT OF SURFACE COURSE. CLASS PP CONCRETE SHALL BE USED IF PLACEMENT OF SURFACE COURSE IS PLANNED IN LESS THAN 72 HOURS. HMA MATERIALS WILL NOT BE ALLOWED AS BACKFILL AROUND AN ADJUSTED CASTING. THIS WORK SHALL APPLY TO ALL CASTINGS ADJUSTED OR RECONSTRUCTED AS PART OF THIS CONTRACT, WHETHER PAID FOR SEPARATELY OR INCLUDED IN OTHER CONTRACT WORK.
- 22. IN AREAS WHERE EXISTING SIDEWALK IS REMOVED AND NOT REPLACED, RESTORATION SHALL CONSIST OF TOPSOIL AND SOD AND THIS SHALL BE INSTALLED TO MAINTAIN POSITIVE DRAINAGE. ANY VOID CREATED BY SIDEWALK REMOVAL AND RESTORATION SHALL BE FILLED WITH TOPSOIL. FILLING THE VOID WILL NOT BE PAID FOR SEPARATELY RATHER INCLUDED IN THE TOPSOIL FURNISH AND PLACE,
- 33. FRAMES AND LIDS THAT ARE REPLACED AS PART OF ADJUSTMENT SHALL BE DELIVERED TO THE VILLAGE PUBLIC WORKS FACILITY. THIS WORK SHALL BE INCLUDED IN THE STRUCTURE ADJUSTMENT PAY ITEM.
- 34. ALL OPEN LIDS SHALL BE BICYCLE SAFE, THIS REQUIREMENTS SHALL BE INCLUDED IN THE FRAME REPLACEMENT PAY ITEM.
- 35. ALL CLOSED LIDS SHALL BE STAMPED WITH THE WORDS "STORM", "SANITARY" OR "WATER" AS APPROPRIATE, THIS REQUIREMENT SHALL BE INCLUDED IN THE FRAME REPLACEMENT PAY ITEM.

INDEX OF SHEETS

SCALE: NON

SHEET	NO.	TITLE
1		COVER SHEET
2		HIGHWAY STANDARDS, INDEX OF SHEETS AND GENERAL NOTES
3 -	4	SUMMARY OF QUANTITIES
5 -	6	TYPICAL SECTIONS
7 -	9	PLAN - CLAIRE BOULEVARD
10		BD-08 DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
11		BD-22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
12		BD-24 CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
13		BD-32 BUTT JOINT AND HMA TAPER DETAILS
14		TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
15		TC-13 DISTRICT 1 TYPICAL PAVEMENT MARKINGS
16		TC-22 ARTERIAL ROAD INFORMATION SIGN
17		TS-07 DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING

	SUMMARY OF QUANTITIES		URBAN	CONSTRUCTION TYPE CODE
CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	0005 QUANTITY
20200100	EARTH EXCAVATION	CUYD	5	
20200600	EXCAVATING AND GRADING EXISTING SHOULDER	UNIT	43	43
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	4	4
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	171	171
25200100	SODDING	SQ YD	171	171
25200200	SUPPLEMENTAL WATERING	UNIT	11	11
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	. 112	112
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	6	6
40600300	AGGREGATE (PRIME COAT)	TON	53	53
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	2	2
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	599	599
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	221	221
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1,161	1,161
42001300	PROTECTIVE COAT	SQ YD	103	103
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	791	791
42400800	DETECTABLE WARNINGS	SQ FT	86	86
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	13,020	13,020
44000600	SIDEWALK REMOVAL	SQ FT	1,086	1,086
44201811	CLASS D PATCHES, TYPE I, 14 INCH	SQYD	260	260
44201815	CLASS D PATCHES, TYPE II, 14 INCH	SQ YD	521	521
44201819	CLASS D PATCHES, TYPE III, 14 INCH	SQ YD	781	781
44201821	CLASS D PATCHES, TYPE IV, 14 INCH	SQ YD	1,042	1,042
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	204	204
56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	2	2
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	5	5
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	9	g
67100100	MOBILIZATION	L SUM	1	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LSUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	- 1	1
*	INDICATES SPECIALTY ITEM			

* INDICATES SPECIALTY ITEM # INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE

REVISED REVISED JDM BAXTER WOODMAN DRAWN KAR REVISED FILE - 130097SHT-S00.dgn MWP CHECKED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE

F.A.U. SECTION RTE. 1008 13-00028-00-RS COOK 3 3 CONTRACT NO. 61A06

					CONSTRUCTION
		SUMMARY OF QUANTITIES		URBAN	TYPE CODE
	CODE NO		LINUT	TOTAL	0005
		ITEM DESCRIPTION	UNIT	QUANTITY	QUANTITY
	70300100	SHORT-TERM PAVEMENT MARKING	FOOT	3,824	3,824
	70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	319	319
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	37	37
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	12,660	12,660
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	315	315
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	730	730
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	37	37
* #	88600600	DETECTOR LOOP REPLACEMENT	FOOT	152	152
#	X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	5	5
#	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	9	9
#	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	68	68
#	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	283	283
		· !			
		i i			3
		INDICATES OFFICIAL TV ITTA	^		

^{*} INDICATES SPECIALTY ITEM

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BAXTER WOODMAN

DESIGNED - JDM REVISED DRAWN - KAR REVISED CHECKED - MWP
DATE - 10-18-13 REVISED FILE - 130097SHT-S00.dgn

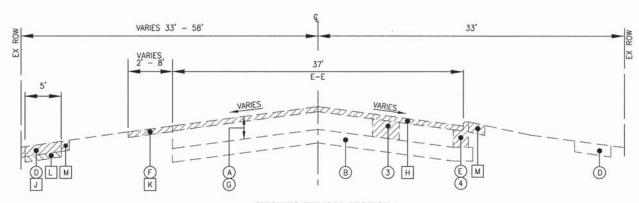
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

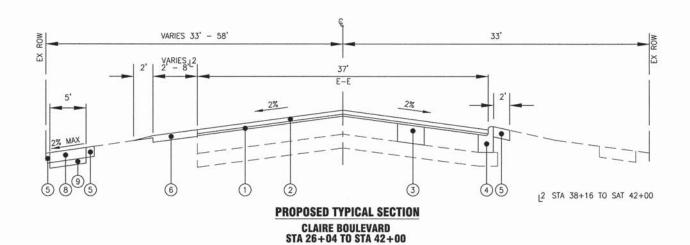
SCALE: NONE

F.A.U. RTE. 1008

[#] INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE



EXISTING TYPICAL SECTION CLAIRE BOULEVARD STA 26+04 TO STA 42+00



HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXUTRE TYPE	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5mm); 1 1/2"	4% @ 50 Gyr.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	3.5% @ 50 Gyr.
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 mm), 14" (5 LIFTS)	4% @ 70 Gvr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY THE DISTRICT ONE SPECIAL PROVISIONS.

FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

THE CONTRACTOR SHALL MILL BEFORE PATCHING

EXISTING PAVEMENT DATA

CORE NUMBER	HMA THICKNESS	PCC BASE THICKNESS
B-8	6.5	6
B-9	6.5	10
B-10	5.25	10.25
B-12	5	10
B-13	5.5	9.75
B-14	4	10
B-16	4	11.5

LEGEND

)	EXISTING	HOT-MIX	ASPHALT	SURFACE	
5					

- EXISTING AGGREGATE BASE
- RESERVED
- (E)
- F
- Н HOT-MIX ASPHALT SURFACE REMOVAL - 2"
- J SIDEWALK REMOVAL
 - EXCAVATING AND GRADING OF EXISTING SHOULDER
 - AGGREGATE BASE COURSE REMOVAL (NOT PAID FOR SEPARATELY)
- ITEMS TO BE REMOVED

BAXTER WOODMAN

DESIGNED - JDM REVISED REVISED CHECKED -FILE - 130097SHT-TypSec.dgn 10-18-13

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

COUNTY TYPICAL SECTIONS COOK 1008 13-00028-00-RS CONTRACT NO. 61A06 SCALE: NONE TO STA. STA.

PROPOSED

RESERVED

(5)

6

(7)

8

POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - ₹"

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 12"

(AS DETERMINED BY THE ENGINEER)

TOPSOIL FURNISH AND PLACE, 4"; SODDING

AGGREGATE WEDGE SHOULDER, TYPE B

AGGREGATE BASE COURSE, TYPE B, 4"

CLASS D PATCHES - 8" (AS DETERMINED BY THE ENGINEER)

PCC SIDEWALK, 5-INCH (AS DETERMINED BY THE ENGINEER)

(B)

EXISTING

0

0 EXISTING SIDEWALK

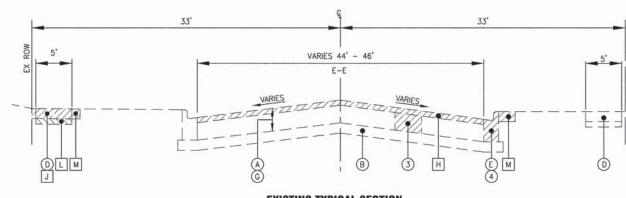
EXISTING COMBINATION CONCRETE CURB AND GUTTER

EXISTING AGGREGATE SHOULDER

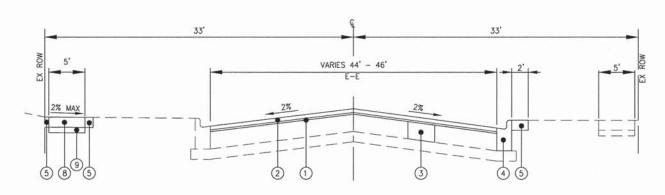
(G) EXISTING PCC BASE COURSE

(AS DETERMINED BY THE ENGINEER)

REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (TOPSOIL)
(NOT PAID FOR SEPARATELY WHEN ADJACENT TO EXISTING SIDEWALK OR CURB AND GUTTER)



EXISTING TYPICAL SECTION CLAIRE BOULEVARD STA 47+33 TO STA 48+63



PROPOSED TYPICAL SECTION CLAIRE BOULEVARD STA 47+33 TO STA 48+63

3

(5)

6

7

LEGEND

EXISTING PAVEMENT DATA

ORE NUMBER	HMA THICKNESS	PCC BASE THICKNESS
B-8	6.5	6
B-9	6.5	10
B-10	5.25	10.25
B-12	5	10
B-13	5.5	9.75
B-14	4	10
B-16	4	11.5

EXISTING

- EXISTING HOT-MIX ASPHALT SURFACE
- B EXISTING AGGREGATE BASE
- 0 RESERVED
- 0 EXISTING SIDEWALK
- E EXISTING COMBINATION CONCRETE CURB AND GUTTER
- F EXISTING AGGREGATE SHOULDER
- (G) EXISTING PCC BASE COURSE
- Н HOT-MIX ASPHALT SURFACE REMOVAL - 2"
- J
- EXCAVATING AND GRADING OF EXISTING SHOULDER (AS DETERMINED BY THE ENGINEER)
- AGGREGATE BASE COURSE REMOVAL (NOT PAID FOR SEPARATELY)
- REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (TOPSOIL)
 (NOT PAID FOR SEPARATELY WHEN ADJACENT TO EXISTING SIDEWALK OR CURB AND GUTTER)
- ITEMS TO BE REMOVED

BAXTER WOODMAN

DESIGNED - JDM REVISED DRAWN - KAR REVISED CHECKED - MWP REVISED FILE - 130097SHT-TypSec.dgn 10-18-13

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

TOTAL SHEE SHEETS NO. SECTION COUNTY TYPICAL SECTIONS соок 1008 13-00028-00-RS CONTRACT NO. 61A06 SCALE: NONE STA. TO STA.

RESERVED

POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3"

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1"

CLASS D PATCHES - 8" (AS DETERMINED BY THE ENGINEER)

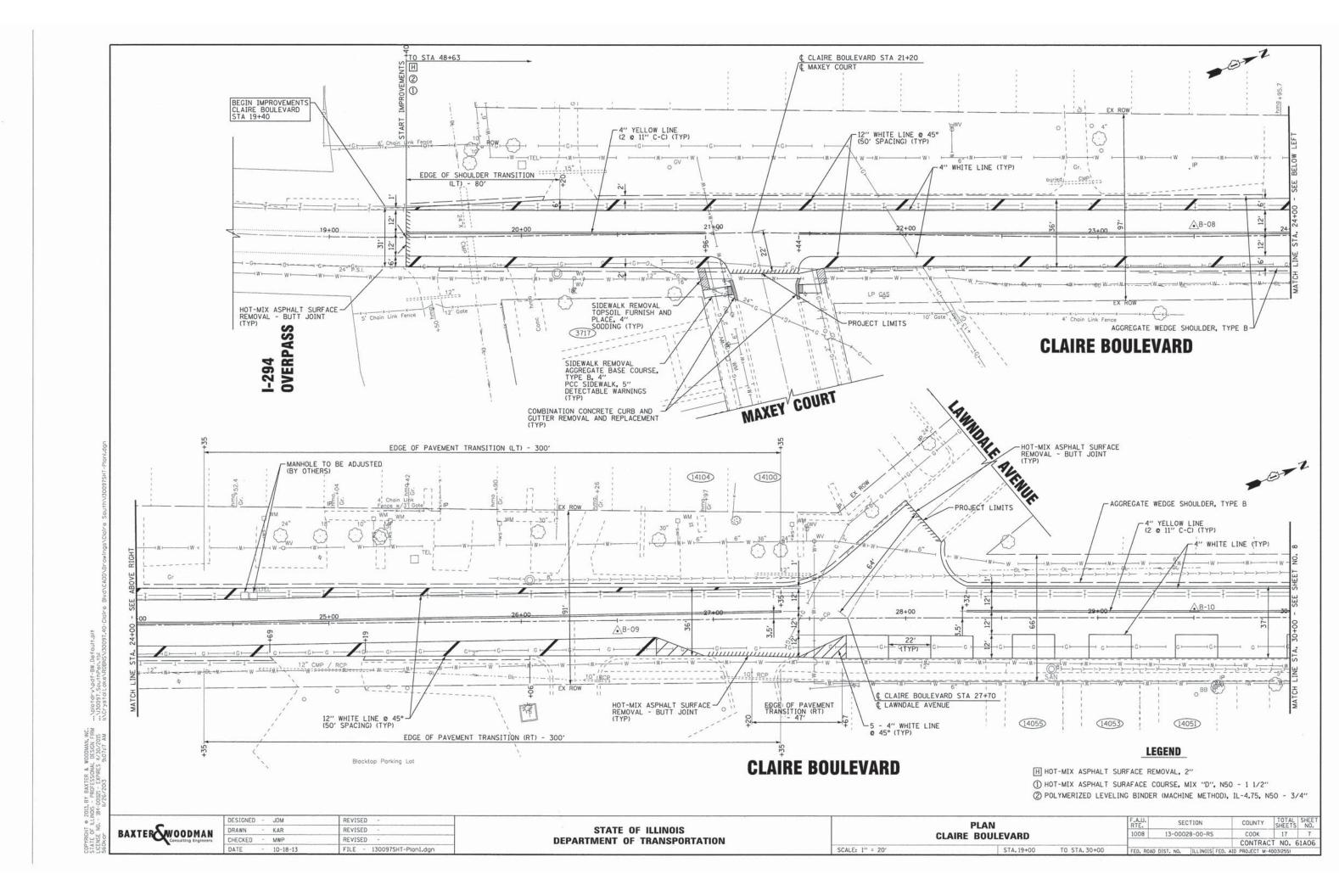
PCC SIDEWALK, 5-INCH (AS DETERMINED BY THE ENGINEER)

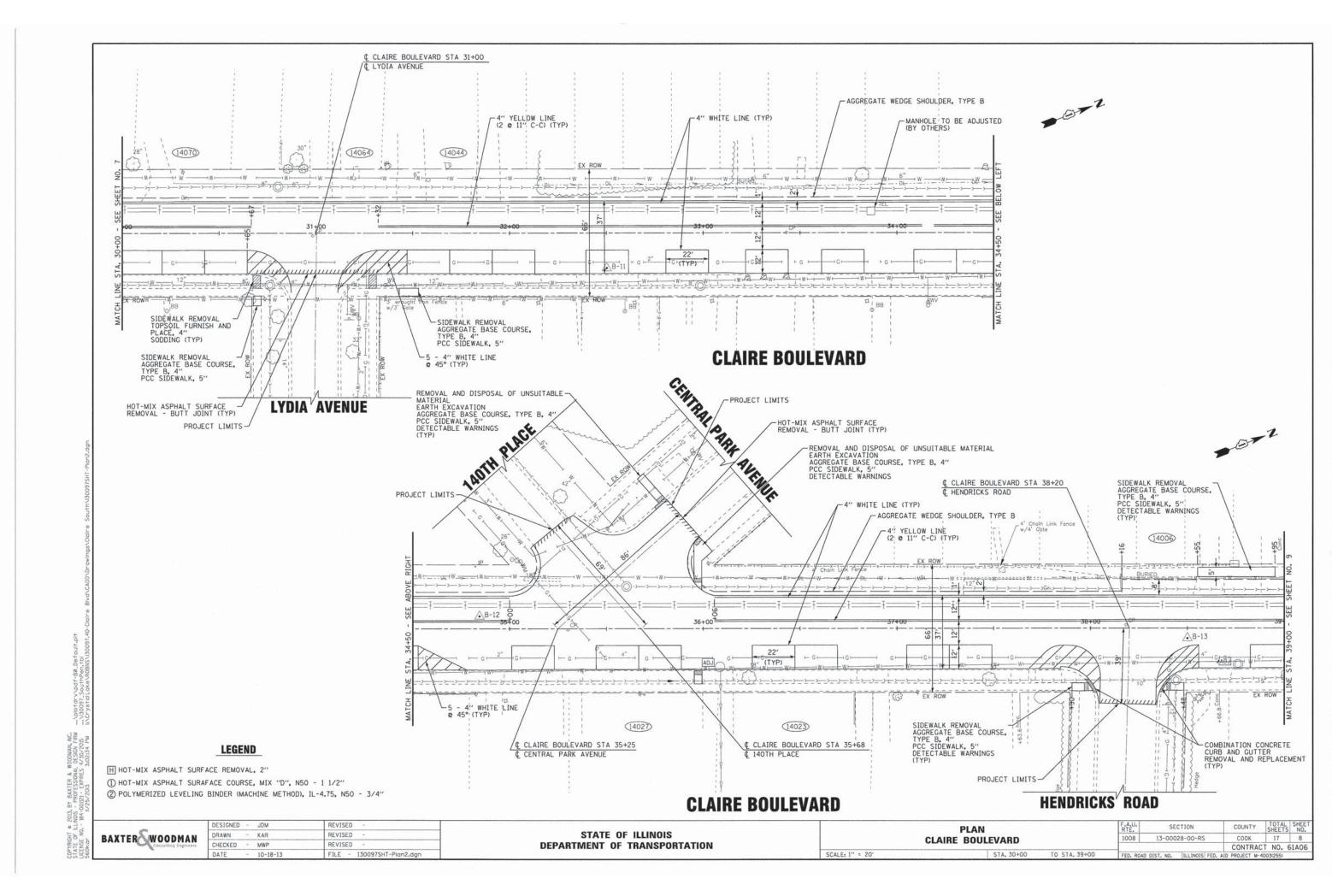
(AS DETERMINED BY THE ENGINEER)

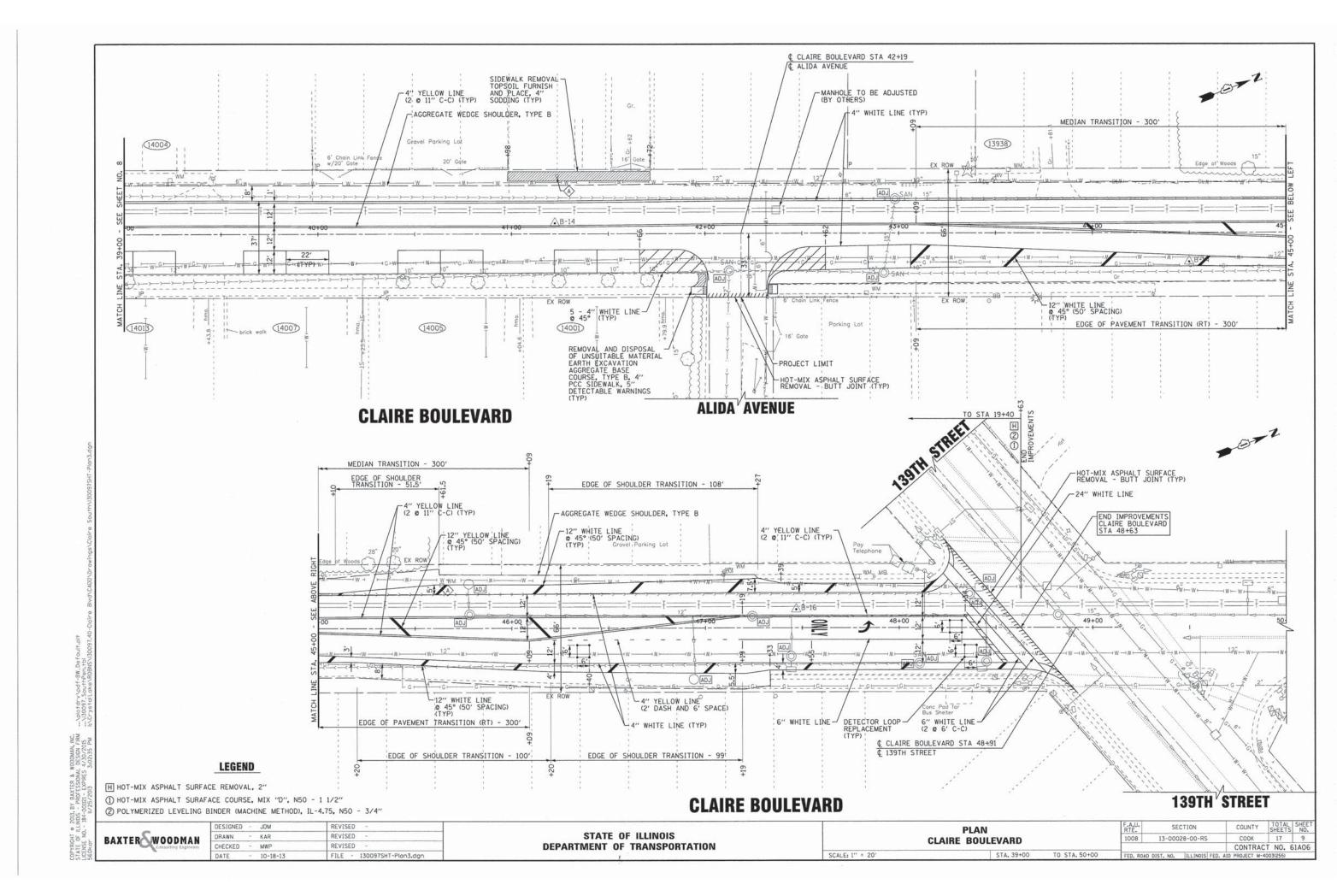
TOPSOIL FURNISH AND PLACE, 4"; SODDING

AGGREGATE WEDGE SHOULDER, TYPE B

AGGREGATE BASE COURSE, TYPE B, 4"







2

2

3

5

12 (300) MIN.

8

9

PROPOSED
BRICK, MORTAR, OR CONC.
ADJUSTING RINGS

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- *UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- 1 SUB-BASE GRANULAR MATERIAL
- 6 FRAME AND LID (SEE NOTES)
- 2 EXISTING PAVEMENT
- 7 CLASS PP-1* CONCRETE
- 3 36 (900) DIAMETER METAL PLATE
- 1000
- PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- (5) EXISTING STRUCTURE
- (8) PROPOSED HMA SURFACE COURSE
 (9) PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURRED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SDEC(AL)"

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

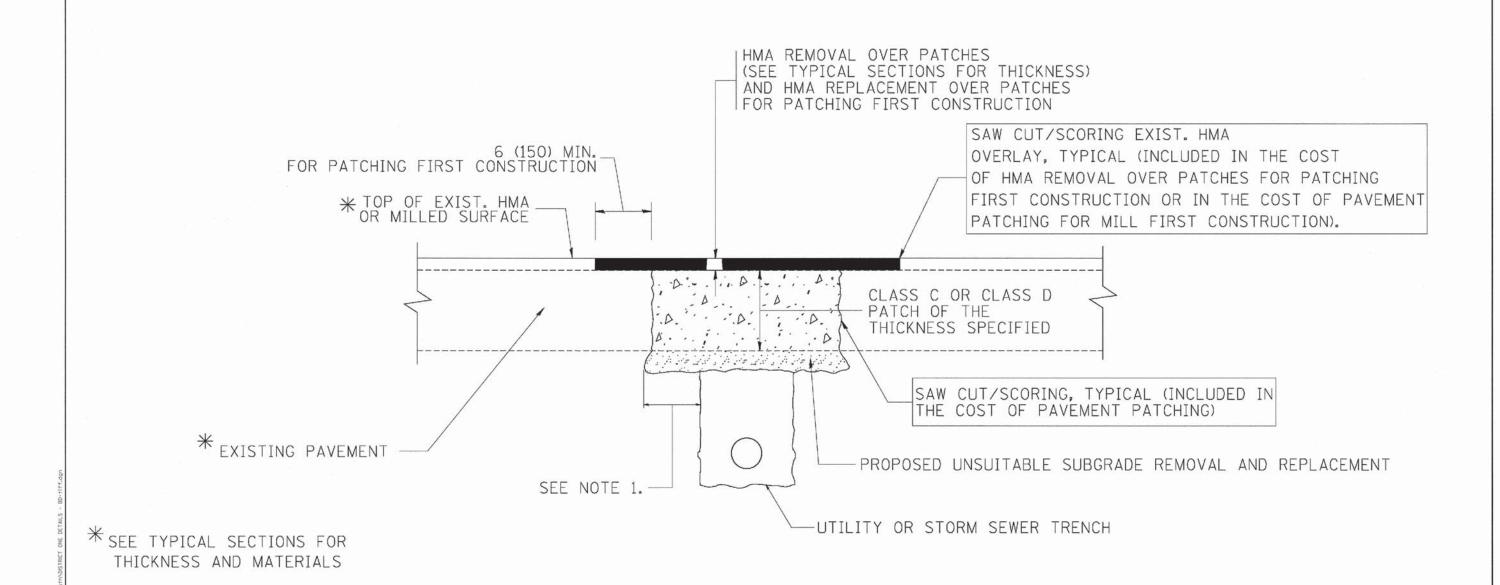
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) LINLESS OTHERWISE SHOWN

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ום במאם יתיספאים ומספס מניינים. הימוד פי היא מאני המשביעה מאורוקט אליניסור מי מסברון אנים ומיני יי פירבי טבי אני

OPYROUT & 2013, RT BAXTER & WOODMAN, INC.

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NOTES:

- 1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
- 2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

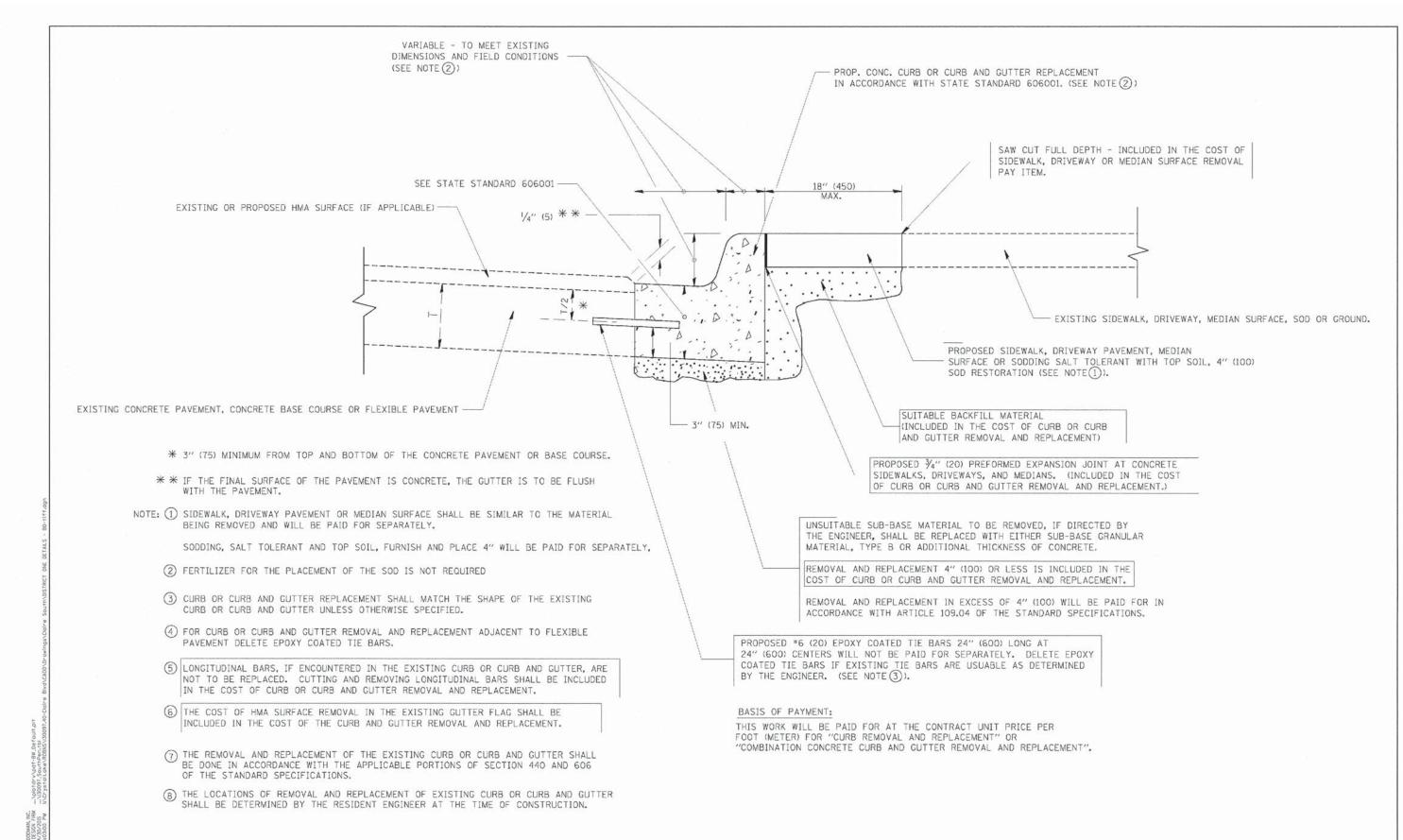
- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST 41/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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FILE	NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	NATE OF THE PARTY	PAVEMENT PATCHING FOR	F.A.U.	SECTION	COUNTY	TOTAL	SHEET
e:\pr	o Jec tə\dıətətd22x34\bd22.dgn		DRAWN -	REVISED - R. BORO 01-01-07	-01-07 STATE OF ILLINOIS		1008	13-00028-00-PS	COOK	17	11
p		PLOT SCALE = 50.000 '/ IN.	CHECKED -	REVISED - R. BORO 09-04-07	DEPARTMENT OF TRANSPORTATION	HMA SURFACED PAVEMENT		RD400_04 (RD_22)	CONTRACT	T NO. 61	A06
260		PLOT DATE = 18/27/2008 DATE	DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROA	AD DIST. NO. 1 ILLINOIS FED. A	ID PROJECT M	-4003-(25	5)

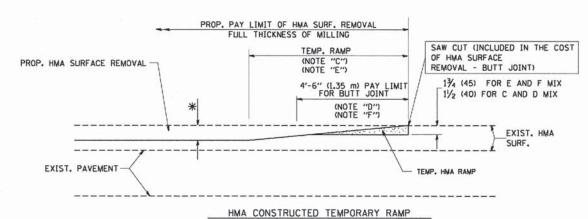


CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

INOIS 184	= IPA/ 111	1821- KAM = Hrsvakosgo	DESIGNED A. HOUSEH	REVISED	R. SHAH 10-03-96			CURB OR CURB AND GUTTER		F.A.U.	SECTION	COUNTY	TOTAL SHEET SHEETS NO.
N 12.0	o:/pw_work/pwidct/drivakosgn/d0188315/bd	2/adgn	DRAWN -	REVISED	- A. ABBAS 03-21-97	STATE OF ILLINOIS				1008	13-00028-00-RS	соок	17 12
NSE OK		JR V + HWKSC = 6381 1.1.01	CHECKED	REVISED	M. GOMEZ 01-22-01	DEPARTMENT OF TRANSPORTATION		REMOVAL AND REPLACEMENT			BD600-06 (BD-24)	CONTRACT	NO. 61A06
STA		PLCT CATE = 12/15/2009	DATE - 03-11-94	REVISED	- R. BORO 12-15-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA.	TO STA.	FED. R		AID PROJECT M-40	

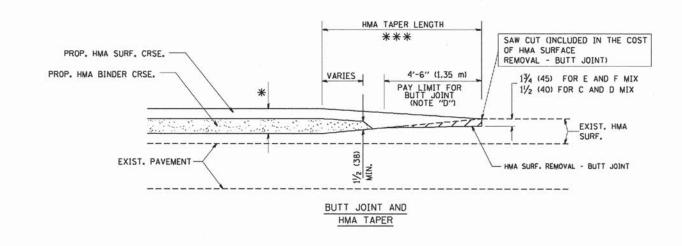
OPTION 1



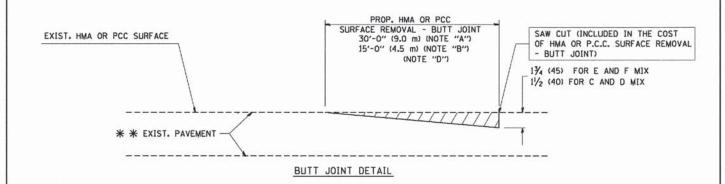
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

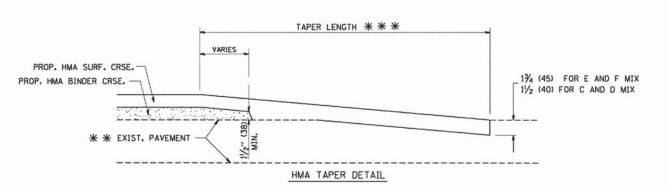
OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING





TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND			RTE.	SECTION	COUNTY	TOTAL	SHEE NO.			
		MRAA	TARER DE	ETAIL O		1008	13-00028-00-RS	COOK	17	13
HMA TAPER DETAILS				BD400-05 BD32	CONTRACT	NO. 6	1A06			
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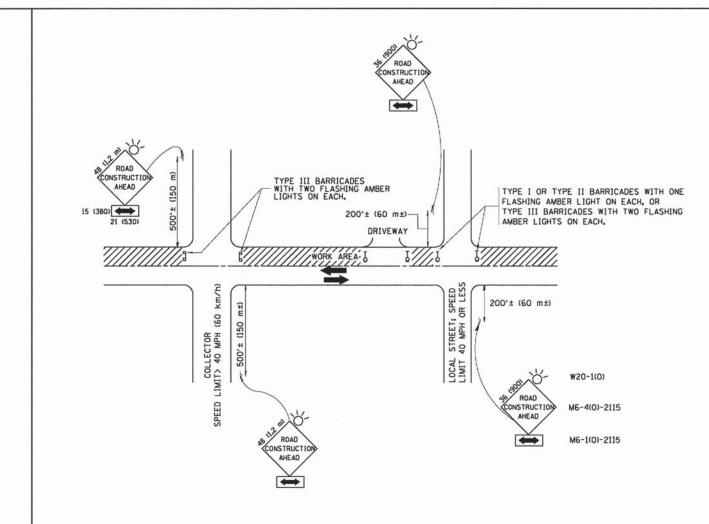
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TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- Q) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE 1. TYPE 11 OR TYPE 111 BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (MG-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

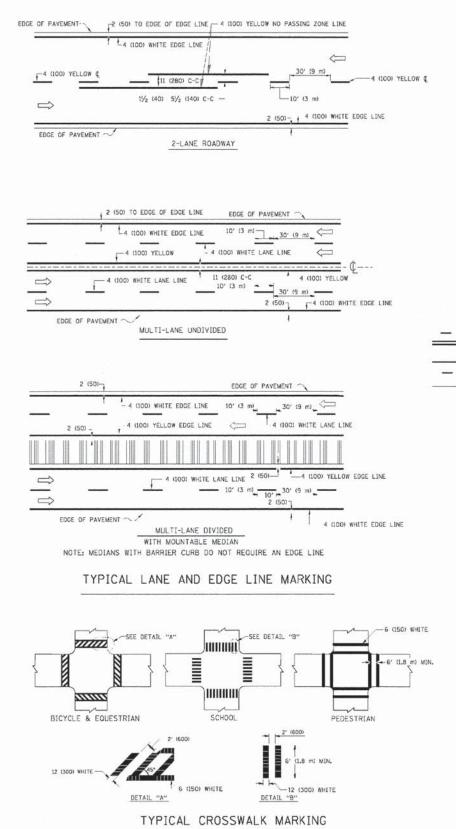
All dimensions are in millimeters (inches)

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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

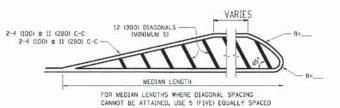
	TRAFFIC CONTROL AND PROTECTION FOR				F.A.U. RTE.	SECTION	COUNTY	TOTAL	SHEE NO.
					1008	13-00028-00-RS	COOK	17	14
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS				TC-10	CONTRACT	NO.	61A06		
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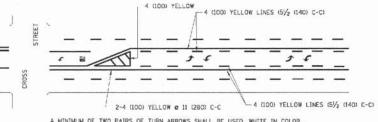
2-4 (100) YELLOW @ 11 (280) C-C-4' (1.2 m) OUTSIDE TO NO DIAGONALS OUTSIDE OF LINES - 2-4 (100) YELLOW # 11 (280) C-C

4' (1.2 m) WIDE MEDIANS ONLY



DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

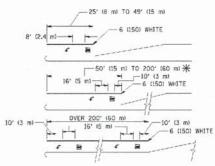


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

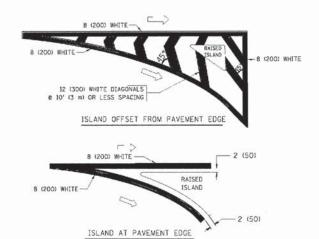
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m2) ONLY AREA = 20.8 SQ. FT. (1.9 m2)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAYEMENT	2 a 4 (100)	SOL1D	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 b 4 (100)	SOL 10 SOL 1D	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C DMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES MEXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOL10	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 9 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 51/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHETE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 8 6 (150) 12 (300) 8 45° 12 (300) 9 90°	SOLID SOLID SOLID	WHETE WHETE WHETE	NOT LESS THAN 6' (1.8 m) APART 2' (500) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOL10	wнtте	PLACE 4' IL2 m) IN ADVANCE OF AND PARALLEL TO GROSSMALK, IF PRESENT. DIMERRISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 B 4 (100) WITH 12 (300) DIAGONALS 6 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHETE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOL10	WHITE	SEE STATE STANDARD 780001 AREA OF: "%"5.6 SO. FT. (0.33 m²) EACH "%"5.40 SO. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) © 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) T0 45MPH (70 km/h) 150' (45 m) C-C (0VER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

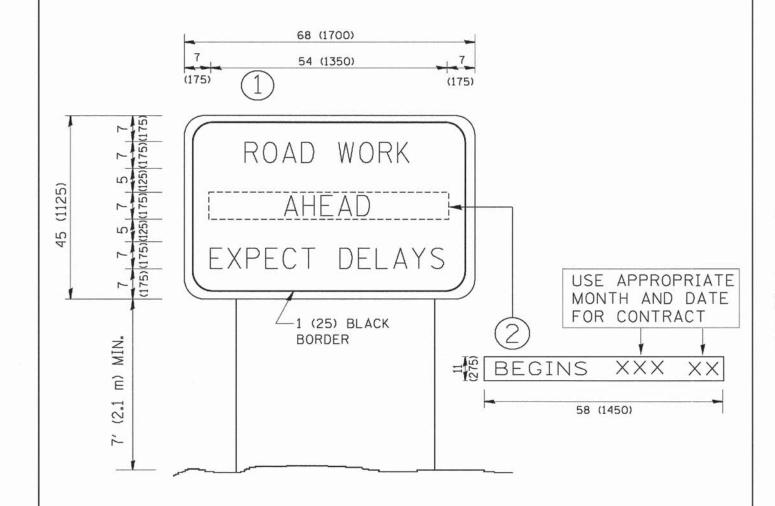
All dimensions are in inches (millimeters) unless otherwise shown.

TYPICAL TURN LANE MARKING

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	PELL 5339 - = 9262008 17 IN.	CHECKED	REVISED
	PLCT CATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE TYPICAL PAVEMENT MARKINGS				F.A.U. RTE. 1008	SECTION 13-00028-00-RS TC-13	COUNTY COOK CONTRACT	TOTAL SHEETS 17	SHEET NO. 15 61AO6
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. RDAI	D DIST. NO. 1 ILLINOIS FED. A		M-40030	



NOTES:

- 1. USE BLACK LETTERING ON ORANGE BACKGROUND.
- 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- 4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
- 7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

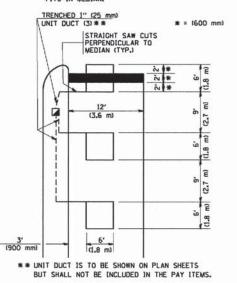
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1	FILE NAME =	USER NAME = gaglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97	**************************************	ARTERIAL ROAD	F.A.U.	SECTION	COUNTY	TOTAL SHEET SHEETS NO.
NO.	ngb.SSat/16xSS/bteterb/sW		DRAWN -	REVISED - R. MIRS 12-11-97	STATE OF ILLINOIS	0.07771/0.0771/0.0771/0.0771/0.0771/0.0771/0.0771/0.0771/0.0771/0.0771/0.0771/0.0771/0.0771/0.0771/0.0771/0.07	1008	13-00028-00-RS	соок	17 16
N P		PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 02-02-99	DEPARTMENT OF TRANSPORTATION	INFORMATION SIGN	TC-22		CONTRACT NO. 6	
560		PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROA	DIST. NO. 1 ILLINOIS FED. A		M-4003(255)

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER. PAVED OR NON-PAVED SHOULDER PAVED OR NON-PAVED SHOULDER ** = (600 mm) ** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS. ARTERIAL - VOLUME DENSITY ("FAR O

LEFT TURN LANES WITH MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

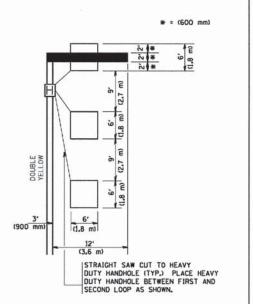
HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

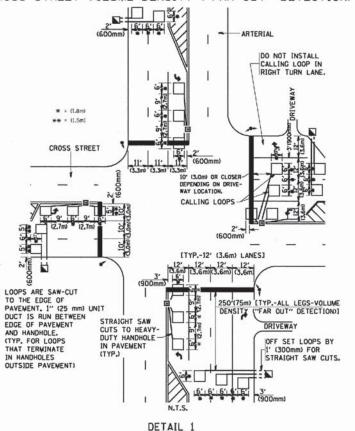
(PROTECTED / PERMITTED LEFT TURN PHASING)

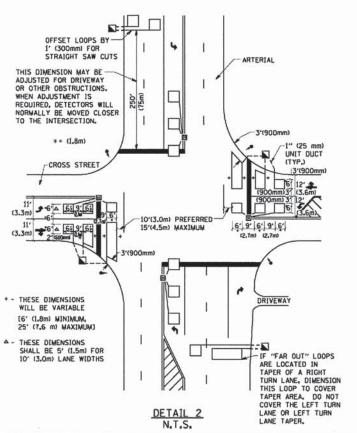


NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)





NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE.
 THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR
 (I.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE 1.D.O.T. DISTRICT 1
TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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N.T.S.							
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560kar	PLOT DATE = 1/4/2008	DATE -	REVISED -				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION				F.A.U. RTE.	SECTION	COUNTY	TOTAL	
			1008	13-00028-00-RS	соок	17	ï	
DETAILS FOR ROADWAY RESURFACING				TS-07	CONTRACT	NO. 6	1	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD	DIST. NO. 1 JLLINOIS FED.	AID PROJECT N	1-4003(25)	5

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