

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU 4013 (BUNKER HILL DRIVE)
RESURFACING FAU 3866 (SQUARE BARN ROAD) TO SARATOGA CIRCLE

SECTION NO. 13-00085-00-RS
PROJECT No. M-4003(224)
VILLAGE OF ALGONQUIN
McHENRY COUNTY
C-91-017-14

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4013	13-00085-00-RS	McHENRY	26	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 63399		

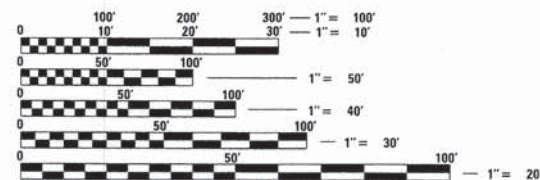
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TRAFFIC DATA

BUNKER HILL DRIVE
ADT (YEAR) = 4900 (2008)
SPEED LIMIT = 25 MPH

DESIGN DESIGNATION: COLLECTOR



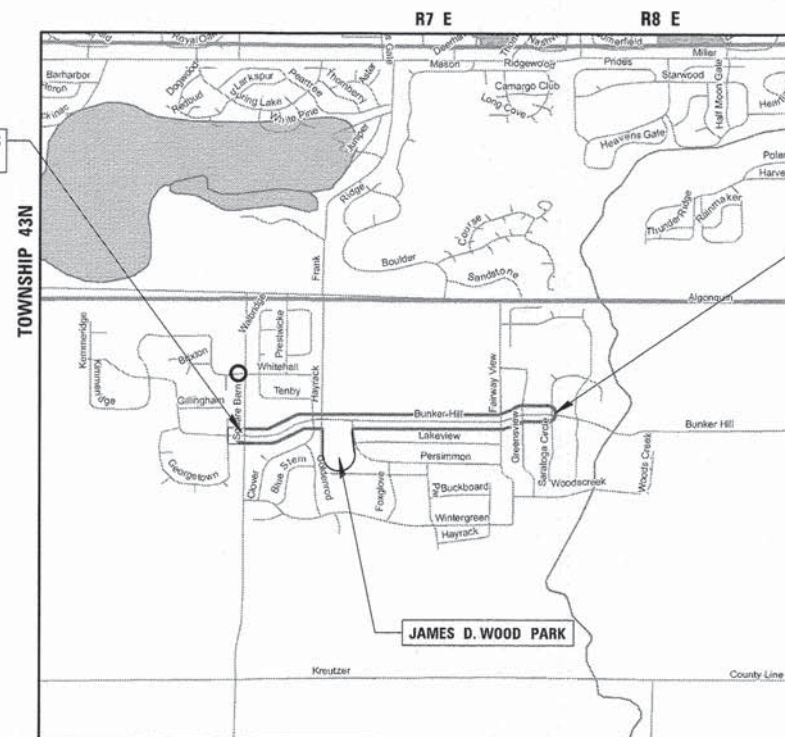
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
8-1-1 OR 1-800-892-0123

CB **CHRISTOPHER B. BURKE** ENGINEERING LTD.
9575 West Higgins Road, Suite 600
Rosemont, Illinois 60018 (847) 823-0500

PROFESSIONAL DESIGN FIRM NO. 184-001175
EXPIRATION DATE: 04/30/15

CONTRACT NO. 63399



LOCATION MAP

3rd P.M.

BUNKER HILL DRIVE
GROSS LENGTH OF PROJECT = 4,905 LINEAL FEET (0.93 MILES)
NET LENGTH OF PROJECT = 4,905 LINEAL FEET (0.93 MILES)

THE PROJECT IS LOCATED IN THE VILLAGE OF ALGONQUIN



LOCATION OF SECTION INDICATED THIS: -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 12/19/2013
[Signature]
PUBLIC WORKS DIRECTOR VILLAGE OF ALGONQUIN

PASSED DEC 26 2013
[Signature]
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW Dec 26, 2013
[Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

LEE M. FELL
062-053708
ENGINEER
12/20/13
DATE
LEE M. FELL
ILLINOIS REGISTRATION No. 062-053708
EXPIRATION DATE: 11/30/2015

GENERAL NOTES

UTILITIES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITY FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR AT HIS OWN EXPENSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE VILLAGE. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 8-1-1 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE

TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED)
THE CONTRACTOR SHALL CONTACT IDOT'S BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS BEFORE PLACING HOT MIX ASPHALT OR PORTLAND CEMENT CONCRETE.

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE VILLAGE. ITS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

WATER, STORM SEWER AND SANITARY SEWER

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF INLET FILTERS.

ALL EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR AND INCIDENTAL TO THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. CONTACT THE VILLAGE OF ALGONQUIN WATER DEPARTMENT (TEL. 847-658-2754) FOR THEM TO TURN VALVES OR OPERATE HYDRANTS. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

MISCELLANEOUS

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ADJUTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION. THE COST TO PROVIDE ACCESS SHALL BE PAID FOR AND INCLUDED IN THE ITEM AGGREGATE FOR TEMPORARY ACCESS.

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE INCLUDED TO REMOVAL ITEMS AND SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.

REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL, AGGREGATE SUBGRADE IMPROVEMENT, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL AND REPLACEMENT, DRIVEWAY REMOVAL AND REPLACEMENT AND STRUCTURES TO BE ADJUSTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD AND WILL NOT EXCEED THE PLANNED QUANTITY.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

DETECTABLE WARNINGS FOR THE HANDICAPPED SHALL BE INSTALLED AT INTERSECTING STREETS, DRIVEWAYS, AND ALLEYS AS SHOWN ON THE PLANS (SEE IDOT STD. 424001-07 INCLUDED IN THE SPECIFICATIONS).

PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT OR SURFACE COURSE, UNLESS OTHERWISE INDICATED.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH ARTICLE 107.25 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES. THIS WORK SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.

ADVANCED WARNING CHANGEABLE MESSAGE BOARDS SHALL BE POSTED AT BOTH ENDS OF THE ROADWAY 2 WEEKS PRIOR TO THE COMMENCEMENT OF ANY CONSTRUCTION WITHIN THE VILLAGE'S RIGHT-OF-WAY NOTIFYING THE MOTORING PUBLIC OF THE UPCOMING WORK (ROAD CONSTRUCTION/LANE REDUCTION BEGINNING _____, EXPECT DELAYS, SEEK ALTERNATE ROUTES, ECT.). THE LANGUAGE MUST BE PROVIDED TO THE ENGINEER FOR REVIEW/APPROVAL PRIOR TO THEIR ACTIVATION. THE MESSAGE BOARD LOCATION SHALL BE REVIEWED AND APPROVED BY THE ENGINEER PRIOR TO PLACEMENT. THIS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CALENDAR MONTH AS "CHANGEABLE MESSAGE SIGN."

FRESH OIL SIGNS SHALL BE POSTED AT BOTH ENDS OF THE ROADWAY AND ALL SIDE STREETS AS DIRECTED BY THE ENGINEER. CONSTRUCTION AHEAD SIGNS SHALL BE PLACED AT ALL SIDE STREETS AND BOTH ENDS OF THE ROADWAY WHILE CONSTRUCTION IS IN PROGRESS. THIS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION.

PROPOSED CONCRETE CURB AND GUTTER SHALL BE TRANSITIONED TO EXISTING CURB AND GUTTER OVER A LENGTH OF 5 FEET. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.

CONTRACTOR SHALL NOT PLACE SEED UNTIL THE TEMPERATURE IS 80 OR LESS AND THE FORECAST FOR THE NEXT 7 DAYS SHOWS TEMPERATURES OF 80 OR LESS. IF ALL OTHER PAY ITEMS ARE COMPLETED, THE CONTRACTOR WILL NOT BE CHARGED WORKING DAYS FOR DELAYS IN PARKWAY RESTORATION DUE TO TEMPERATURE.

NO CONSTRUCTION SHALL BEGIN UNTIL ALL PROPER TEMPORARY SIGNS AND BARRICADES HAVE BEEN INSTALLED.

AT NO TIME SHALL LESS THAN HALF OF THE STREET BE AVAILABLE FOR PARKING.

ALL ROADS MUST HAVE ONLY ONE LONGITUDINAL JOINT WHILE PAVING.

ALL HOT-MIX ASPHALT SHALL BE IN PLACE PRIOR TO THE BRICK WORK COMMENCING.

PAY ITEMS IN THE SUMMARY OF QUANTITIES HAVE BEEN ESTIMATED. IF, IN THE ENGINEER'S OPINION, THE WORK IS NOT REQUIRED, THE ITEM WILL BE DEDUCTED FROM THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ANY REPAIRS FOR DAMAGE BY THE CONTRACTOR OUTSIDE THE LIMITS OF WORK TO SIDEWALKS AND DRIVEWAY APRONS SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

VANDALISM - SPECIAL ATTENTION IS CALLED TO THE SPECIAL PROVISION FOR "INSPECTION" AS WELL AS ARTICLE 107.30 OF THE "STANDARD SPECIFICATIONS." ANY DEFACTED WORK AS DETERMINED AND DIRECTED BY THE VILLAGE SHALL BE CORRECTED OR REPLACED TO THE SATISFACTION OF THE ENGINEER BY THE CONTRACTOR AT HIS SOLE EXPENSE PRIOR TO FINAL PAYMENT. THE VILLAGE OF ALGONQUIN WILL COOPERATE WITH THE CONTRACTOR TO MINIMIZE VANDALISM, BUT THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE TO CORRECT ANY DAMAGE. THE VILLAGE WILL NOT BE RESPONSIBLE FOR THE SECURITY OF THE WORK SITE IN THIS REGARD, OTHER THAN NORMAL PATROLLING AND RESPONSE TO EMERGENCIES. THE COST OF ADDITIONAL SECURITY REQUIRED TO MEET THIS SPECIAL PROVISION SHALL BE SOLELY THE CONTRACTOR'S RESPONSIBILITY.

THE BRICK PAVER REMOVAL AND REPLACEMENT SHALL OCCUR AT ONE LOCATION AT A TIME, ONCE THE FIRST LOCATION HAS BEEN COMPLETED THE NEXT LOCATION CAN BE DISTURBED. A SEQUENCING PLAN SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW.

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-07	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
602011-02	CATCH BASIN TYPE C
602401-03	MANHOLE TYPE A
604001-03	FRAME AND LIDS TYPE 1
606001-05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF ROAD OPERATIONS
701201-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-03	TRAFFIC CONTROL DEVICES
BLR 17-4	TRAFFIC CONTROL DEVICES - DAY LABOR CONSTRUCTION
BLR 18-5	TRAFFIC CONTROL DEVICES - DAY LABOR MAINTENANCE

DISTRICT ONE DETAILS

TC-10	TRAFFIC CONTROL & PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-16	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
BD-32	BUTT JOINT AND HMA TAPER DETAILS

UTILITY COORDINATION

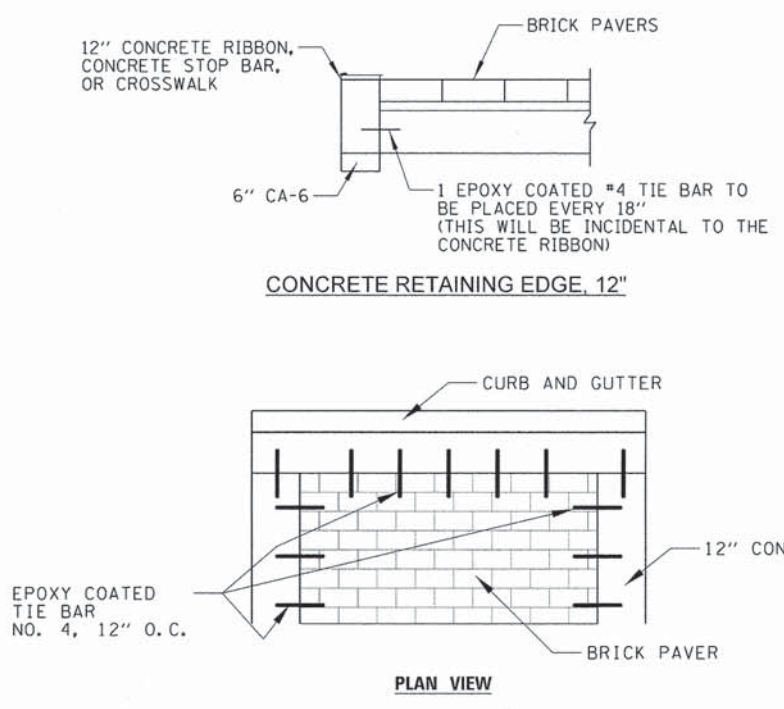
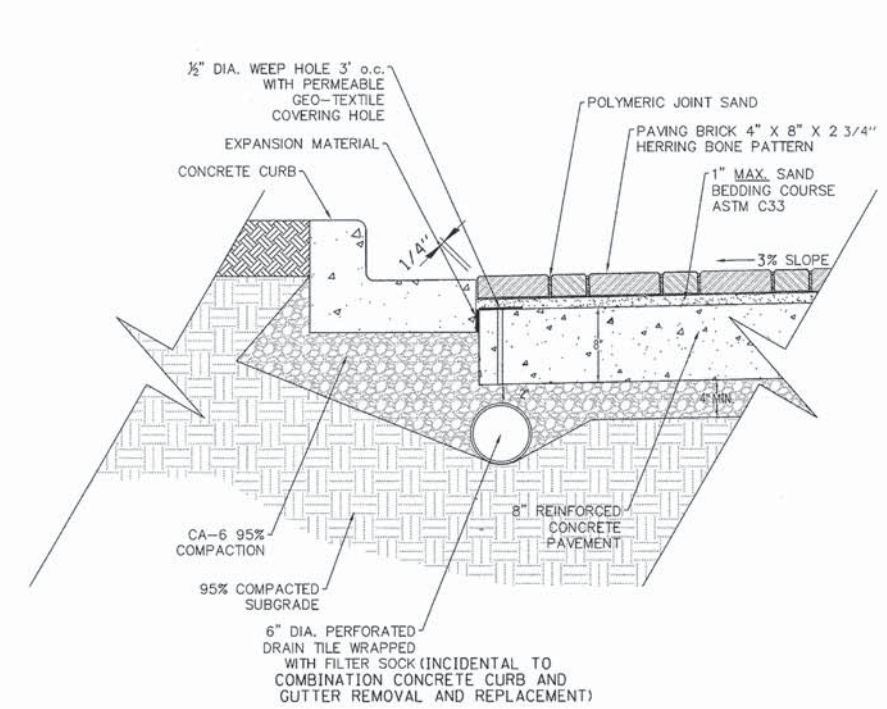
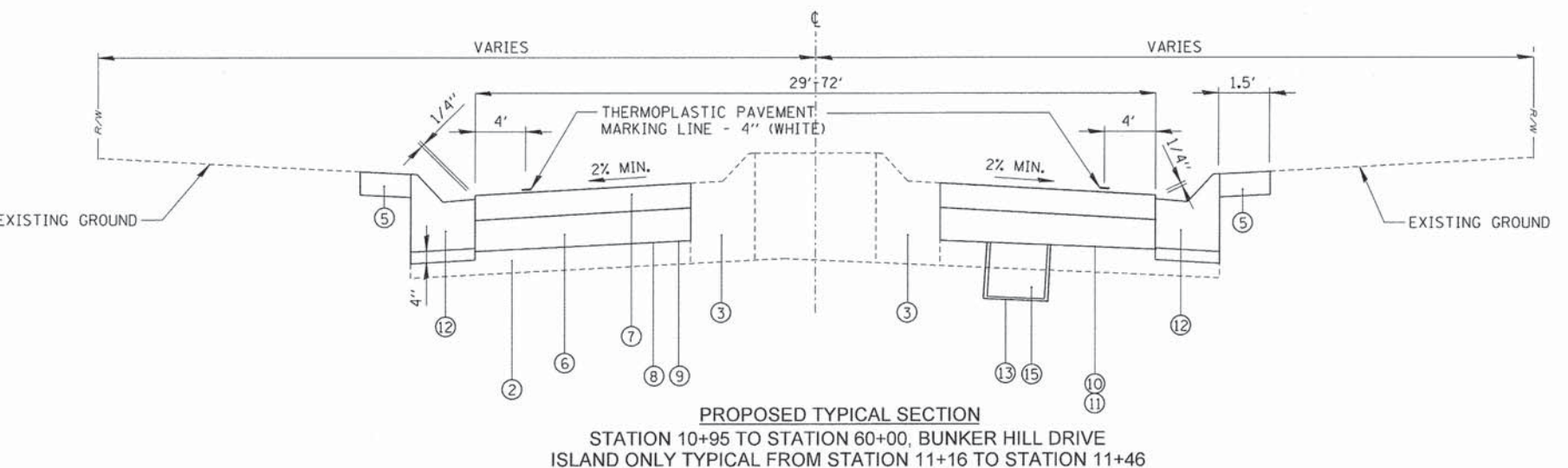
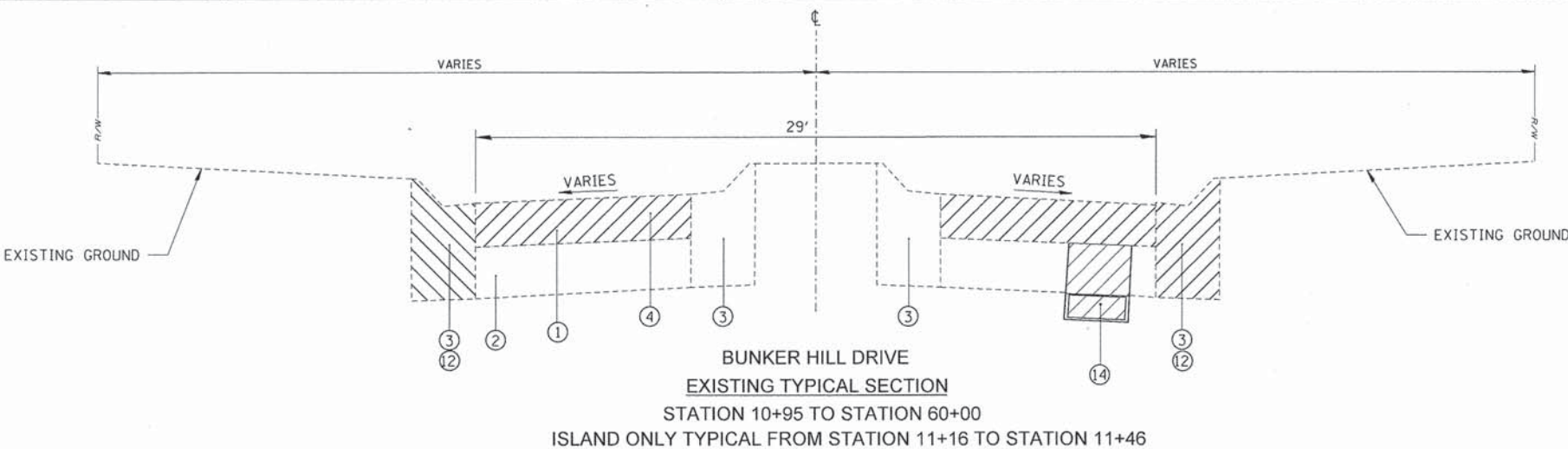
UTILITY COMPANY	AT&T (SBC)	MCHENRY COUNTY DIVISION OF TRANSPORTATION	COMCAST	COMMONWEALTH EDISON	NICOR GAS
CONSTRUCTION CONTACT PERSON	JOANNE MARSHALL	GLEN LINDSEY	MARTHA GIERAS	TOM STUTZMAN	CONSTANCE LANE
PHONE			630-600-6352	630-437-2236	630-983-8676 EXT. 2362
FAX			630-600-6390	630-437-2177	630-983-4028
ADDRESS	225 E. CHICAGO ST. ELGIN, IL 60120	16111 NELSON RD WOODSTOCK, IL 60098-9533	688 INDUSTRIAL DR. ELMHURST, IL 60126	2 LINCOLN CENTER OAKBROOK TERRACE, IL 60181	1844 FERRY RD. NAPERVILLE, IL 60563-9600
FACILITIES IN / NEAR PROJECT	UNDERGROUND FACILITIES	UNDERGROUND FACILITIES	UNDERGROUND FACILITIES	UNDERGROUND FACILITIES	UNDERGROUND FACILITIES
STATUS ON DATE OF PLAN PRINTING	NO CONFLICTS ANTICIPATED	NO CONFLICTS ANTICIPATED	NO CONFLICTS ANTICIPATED	NO CONFLICTS ANTICIPATED	NO CONFLICTS ANTICIPATED

FILE NAME =	USER NAME = edtoda	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUNKER HILL DRIVE GENERAL NOTES AND HIGHWAY STANDARDS		F-A-U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
\\NALGONDQUIN\078273\078273.00030\Civil\et_070273_00030.sht		DRAWN - EDT	REVISED -		4013	13-00085-00-RS	MCHENRY	26	2	CONTRACT NO. 63399	
PLOT SCALE = NOT TO SCALE		CHECKED - JGS	REVISED -		SCALE: NOT TO SCALE	SHEET NO. 2 OF 26 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		
PLOT DATE = 12/23/2013		DATE - 12/20/13	REVISED -								

SUMMARY OF QUANTITIES

SPECIALTY ITEM	CODE NO.	ITEM	UNIT	QUANTITY 0005	Non Participating QUANTITY	Total QUANTITY
*	20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	70	0	70
*	20101200	TREE ROOT PRUNING, SPECIAL	EACH	20	0	20
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	550	50	600
	21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	1650	150	1800
*	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	2500	800	3300
*	21101625	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	75	0	75
*	25000100	SEEDING, CLASS 1	ACRE	0.5	0.2	0.7
	28000510	INLET FILTERS	EACH	30	0	30
	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	550	50	600
	35101582	AGGREGATE BASE COURSE, TYPE B 2"	SQ YD	50	0	50
	35101800	AGGREGATE BASE COURSE, TYPE B 6"	SQ YD	150	0	150
	35102400	AGGREGATE BASE COURSE, TYPE B 12"	SQ YD	420	0	420
	35800100	PREPARATION OF BASE	SQ YD	16500	0	16500
	35800200	AGGREGATE BASE REPAIR	TON	900	0	900
	40201000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	TON	200	0	200
	40201000	TEMPORARY ACCESS (ROAD)	TON	50	0	50
	40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	1650	0	1650
	40600300	AGGREGATE (PRIME COAT)	TON	33	0	33
	40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	4100	0	4100
	40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1900	250	2150
	42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	50	0	50
	42400800	DETECTABLE WARNINGS	SQ FT	84	0	84
	44000150	HOT-MIX ASPHALT SURFACE REMOVAL, 1/4"	SQ YD	0	2000	2000
	44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	608	0	608
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	90	0	90
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	600	0	600
	44000600	SIDEWALK REMOVAL	SQ FT	230	0	230
	550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	63	0	63
*	56400400	FIRE HYDRANTS TO BE RELOCATED	EACH	2	0	2
	60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	2	0	2
	60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	2	0	2
	60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	0	1
	60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	1	0	1
	60609200	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12	FOOT	700	0	700
	67100100	MOBILIZATION	L. SUM	1	0	1
	70106900	CHANGEABLE MESSAGE SIGN	CAL MO	2	0	2
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	17100	0	17100
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	200	0	200
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	282	0	282
*	A2000090	TREE, ACER X FREEMANII ARMSTRONG (ARMSTRONG FREEMAN MAPLE), 3" CALIPER, BALLED AND BURLAPPED	EACH	70	0	70
*	K0012990	PERENNIAL PLANTS, ORNAMENTAL TYPE, GALLON POT	UNIT	2	0	2
A	Z0076600	TRAFFIC	HOURL	500	0	500
A	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	3550	0	3550
A	Z0076604	TRAFFIC TRAVEL PROGRAM GRADUATE	HOURL	500	0	500
	Z0013798	CONSTRUCTION LAYOUT	L. SUM	1	0	1
	Z0018700	DRAINAGE STRUCTURE TO BE REMOVED	EACH	1	0	1
	X0326862	STRUCTURES TO BE ADJUSTED	EACH	28	0	28
	X0540000	BRICK PAVERS	SQ FT	6761	0	6761
	X0800250	TRENCH BACKFILL, SPECIAL	CU YD	25	0	25
	X0240420	PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH, SPECIAL	SQ FT	2500	0	2500
	X0401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	16200	0	16200
	X0710216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L. SUM	1	0	1
	X0001621	BRICK PAVER REMOVAL	SQ FT	6761	0	6761
	X0003219	UNIT PAVERS	SQ FT	500	0	500
	X0006044	WOOD FENCE TO BE REMOVED AND REPLACED	FOOT	350	0	350
	X0008947	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	365	0	365
	X0008929	BRICK PAVERS (MATERIAL ONLY)	EACH	0	3000	3000

*INDICATES SPECIALTY ITEM
A 0042



HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
ITEM	AC TYPE	AIR VOIDS AT Ndes
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	PG 58-22/58-28*	3.5% @ 50 GYR
LEVELING BINDER (MACHINE METHOD), N50	PG 58-22/58-28*	3.5% @ 50 GYR
HOT-MIX ASPHALT BINDER COURSE, IL-19, N50	PG 58-22/58-28*	3.5% @ 50 GYR

• WHEN ASPHALT BINDER REPLACEMENT (ABR) EXCEEDS 15%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-28. NO MORE THAN 2% RECLAIMED ASPHALT SHINGLES SHALL BE ALLOWED IN THE ASPHALT.

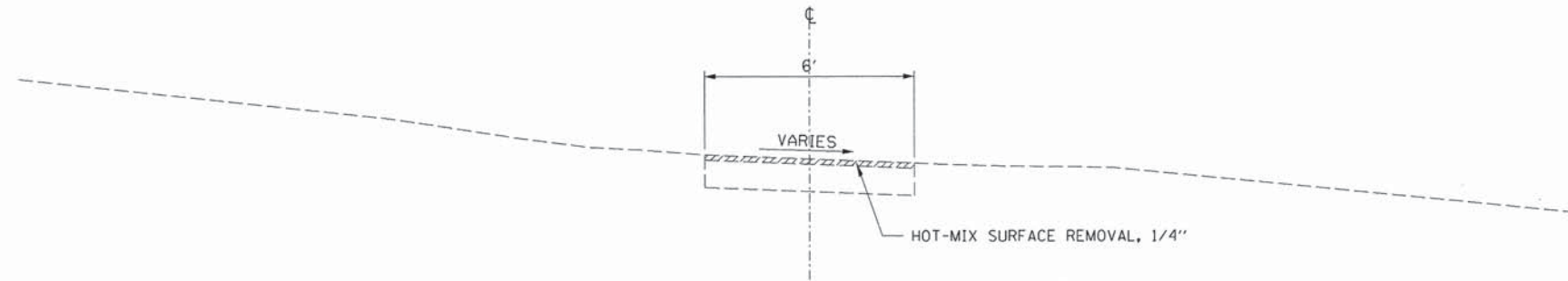
•• THIS TABLE WAS PROVIDED BY TIM MURPHY, P.E., PRESIDENT OF MURPHY PAVEMENT TECHNOLOGY, INC., WHICH HAS BEEN APPROVED BY IDOT BUREAU OF MATERIALS.

LEGEND

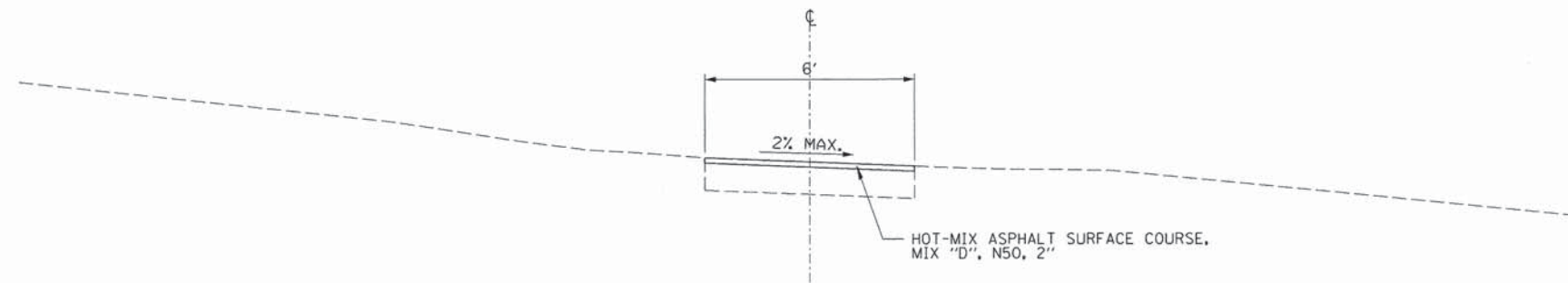
- ① EXISTING HOT-MIX ASPHALT PAVEMENT (3.0"-3.5")
- ② EXISTING AGGREGATE SUBBASE (10"-14")
- ③ EXISTING M6.12 CURB AND GUTTER
- ④ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3"-3.5") (AGGREGATE SHALL BE REMOVED WHERE NECESSARY (SEE CORES) TO ATTAIN 6.5 INCHES OF REMOVAL. THIS WILL BE INCLUDED IN THE COST OF HOT-MIX ASPHALT REMOVAL, VARIABLE DEPTH)
- ⑤ SEEDING, CLASS I, AND TOPSOIL, 4" AS NECESSARY FOR CURB AND GUTTER REMOVAL AND REPLACEMENT (INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT)
- ⑥ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50-4.5".
- ⑦ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2"
- ⑧ PROPOSED BITUMINOUS MATERIAL (PRIME COAT)
- ⑨ PROPOSED AGGREGATE (PRIME COAT)
- ⑩ PREPARATION OF BASE
- ⑪ AGGREGATE BASE REPAIR
- ⑫ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (REPLACE IN KIND WITH MOUNTABLE CURB AS DIRECTED BY THE ENGINEER). INCLUDES 4" SUBBASE GRANULAR MATERIAL TYPE B.
- ⑬ PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- ⑭ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- ⑮ POROUS GRANULAR EMBANKMENT, SUBGRADE

NOTE:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE IS 112 LBS/SY/IN. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
2. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.
3. THE HIGH SIDE OF THE ROADWAY SHALL BE PAVED FIRST.
4. AGGREGATE BASE REPAIR - ANY NEW MATERIAL NECESSARY TO BRING THE EXISTING SUBBASE TO THE GRADE MIN. DEPTH 9" CROSS SLOPE OR WIDTH SHOWN SHALL BE PAID FOR UNDER THIS ITEM.
5. ANY AGGREGATE BASE REMOVAL DUE TO PROPOSED ASPHALT SHALL BE CONSIDERED INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH)
6. AGGREGATE SUBGRADE IMPROVEMENT HAS BEEN PROVIDED AT THE LOCATIONS INDICATED FOR SOILS WHICH TEND TO BE UNSTABLE WHEN WET. ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGE WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER (BY USE OF A CONE PENETROMETER IN CONJUNCTION WITH THE IDOT SUBGRADE MANUAL OR PROOF ROLL PERFORMED BY CONTRACTOR UNDER INSPECTION OF THE RESIDENT ENGINEER). IF UNSUITABLE SOILS ARE ENCOUNTERED, THE SOILS SHALL BE REMOVED AND REPLACED WITH PGE AND GROUND FABRIC FOR GROUND STABILIZATION. NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.








EXISTING TYPICAL SECTION
WALKING PATH

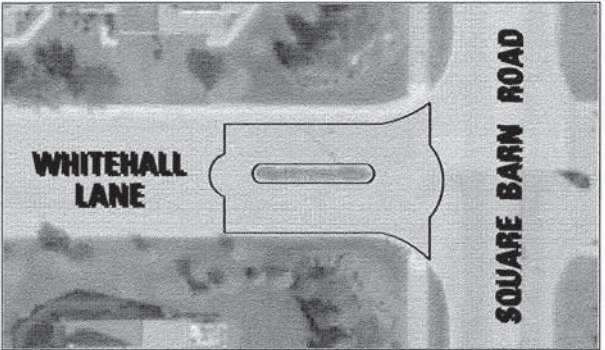


PROPOSED TYPICAL SECTION
WALKING PATH

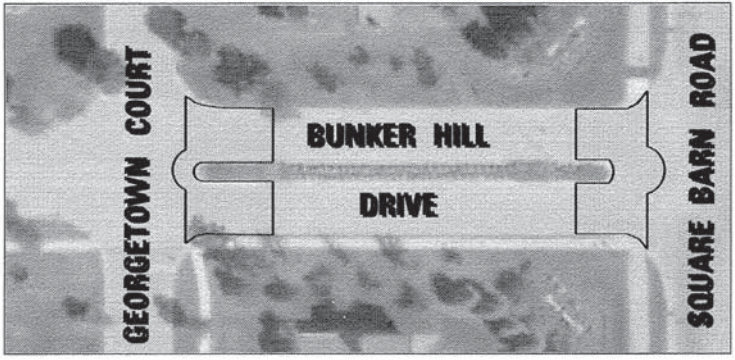
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	YP_070273.00030.sht	DRAWN - EDT	REVISED -			4013	13-00085-00-RS	McHENRY	26	5
PLOT SCALE = NOT TO SCALE	CHECKED - JGS	REVISED -			SCALE: NOT TO SCALE SHEET NO. 5 OF 26 SHEETS STA. TO STA.		CONTRACT NO. 63399			
PLOT DATE = 12/23/2013	DATE - 12/20/13	REVISED -			ILLINOIS FED. AID PROJECT					

- LEGEND**
-  HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
 -  HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 -  SIDEWALK REMOVAL
 -  HMA DRIVEWAY REMOVAL
 -  COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

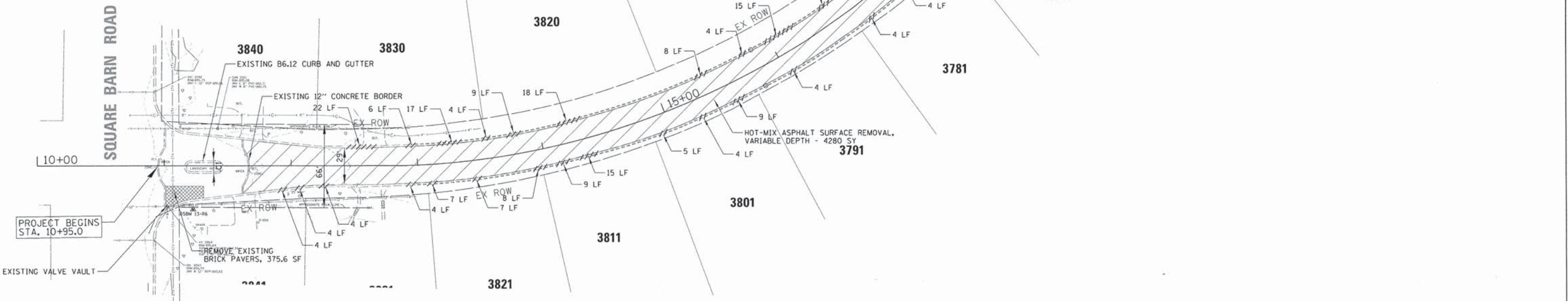
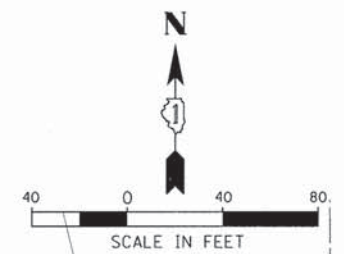
NOTE:
 BUTT JOINTS WILL BE AT SIDE STREETS AND AT LIMITS OF THE PROJECT. ALL BUTT JOINTS ARE INCLUDED IN THE COST OF SURFACE COURSE.



WHITEHALL LANE AND SQUARE BARN ROAD
 -ONLY BROKEN OR MISSING PAVERS TO BE REPLACED (PAID FOR AS CONCRETE PAVERS). THIS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.








WEST BUNKER HILL DRIVE AND SQUARE BARN ROAD
 -ONLY BROKEN OR MISSING PAVERS TO BE REPLACED (PAID FOR AS CONCRETE PAVERS). THIS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.



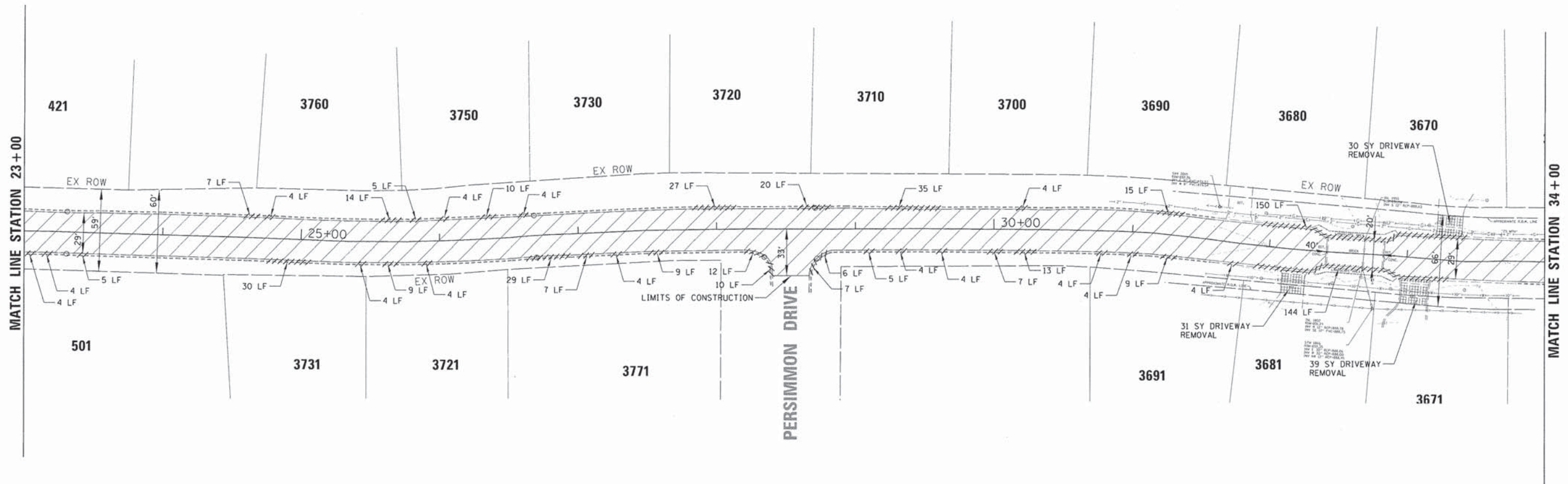
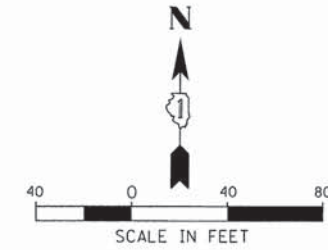
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N:\ALGONDUI\07273\07273.00030\Civil\EM_070273_1.dgn		DRAWN - EDT	REVISED -		SCALE: 48'	SHEET NO. 6 OF 26 SHEETS	STA.	TO STA.	4013	13-00085-00-RS	MCHENRY	26	6
	PLOT SCALE = 48'	CHECKED - JGS	REVISED -		CONTRACT NO. 63399								
	PLOT DATE = 12/23/2013	DATE - 12/20/13	REVISED -		ILLINOIS FED. AID PROJECT								

LEGEND

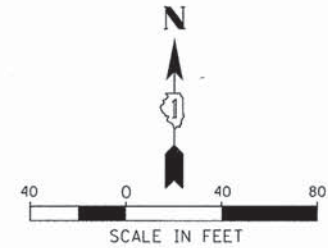
-  HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
-  HOT-MIX ASPHALT SURFACE REMOVAL, 2"
-  SIDEWALK REMOVAL
-  HMA DRIVEWAY REMOVAL
-  COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

NOTE:

BUTT JOINTS WILL BE AT SIDE STREETS AND AT LIMITS OF THE PROJECT. ALL BUTT JOINTS ARE INCLUDED IN THE COST OF SURFACE COURSE.



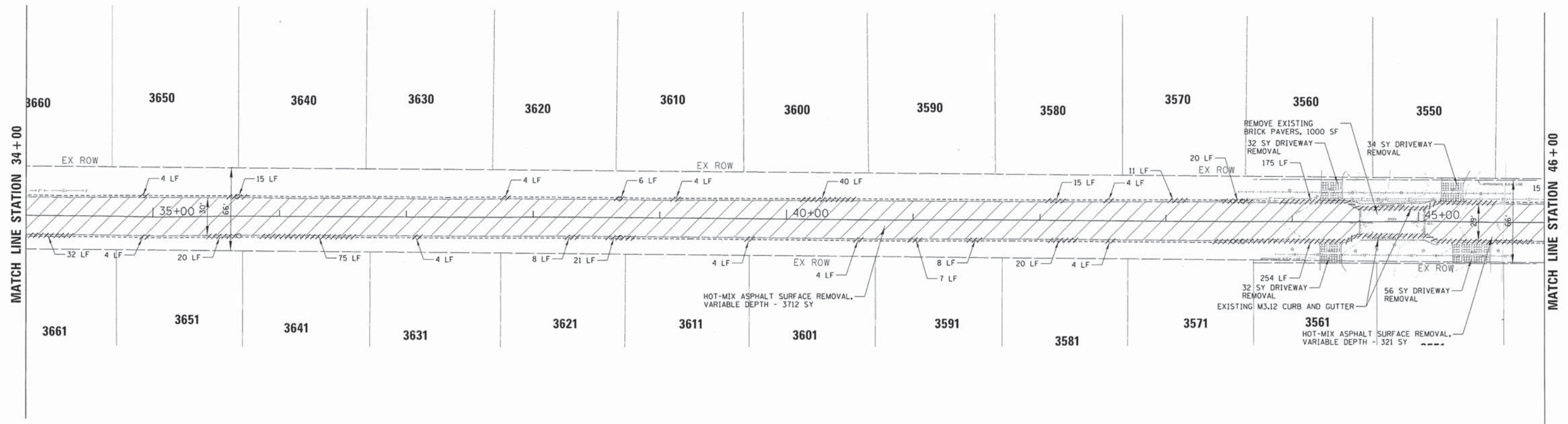
FILE NAME =	USER NAME = edtd	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUNKER HILL DRIVE EXISTING CONDITIONS AND REMOVAL PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\ALCONDUIN\070273\070273.00030\Civil\EM_070273_2.dgn		DRAWN - EDT	REVISED -			4013	13-00085-00-RS	McHENRY	26	7	
PLOT SCALE = 48"		CHECKED - JGS	REVISED -			CONTRACT NO. 63399					
PLOT DATE = 12/23/2013		DATE - 12/20/13	REVISED -			ILLINOIS FED. AID PROJECT					
				SCALE: 48"		SHEET NO. 7 OF 26 SHEETS		STA. 23+00 TO STA. 34+00			








LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- SIDEWALK REMOVAL
- HMA DRIVEWAY REMOVAL
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

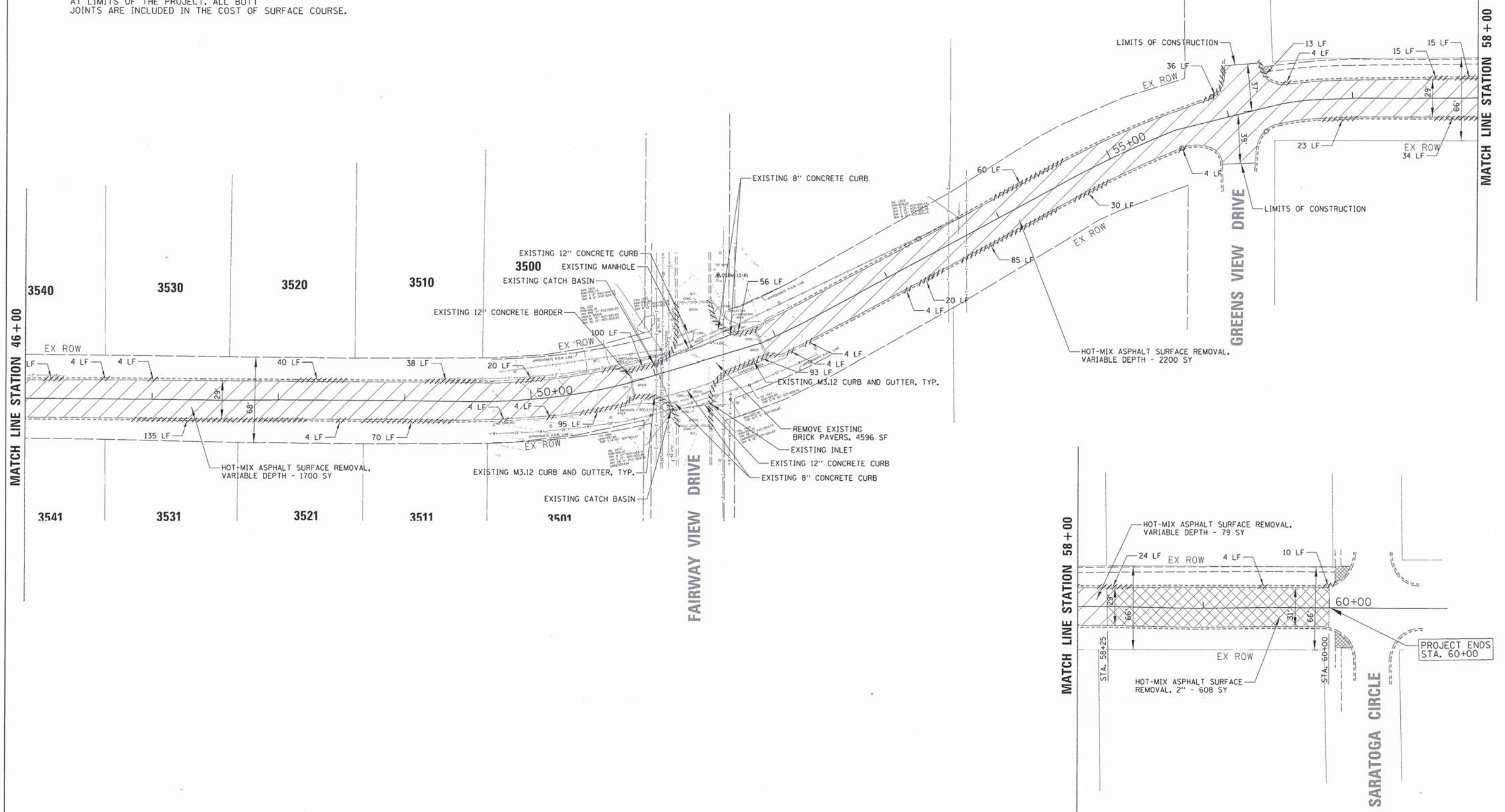
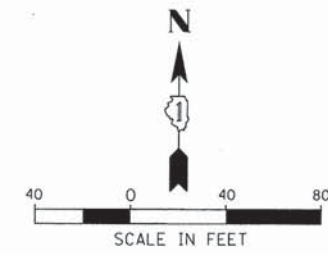
NOTE:
 BUTT JOINTS WILL BE AT SIDE STREETS AND AT LIMITS OF THE PROJECT. ALL BUTT JOINTS ARE INCLUDED IN THE COST OF SURFACE COURSE.



FILE NAME =	USER NAME = edtodo	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUNKER HILL DRIVE EXISTING CONDITIONS AND REMOVAL PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
NAVALCONQUIN\070273\070273.00030\CIVIL\EM_070273_3.dgn		DRAWN - EDT	REVISED -			4013	13-00085-00-RS	MCHEMRY	26	8	
PLOT SCALE = 40'		CHECKED - JGS	REVISED -			CONTRACT NO. 63399					
PLOT DATE = 12/23/2013		DATE - 12/20/13	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE: 40'	SHEET NO. 8 OF 26 SHEETS		STA. 34+00 TO STA. 46+00			

- LEGEND**
-  HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
 -  HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 -  SIDEWALK REMOVAL
 -  HMA DRIVEWAY REMOVAL
 -  COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

NOTE:
 BUTT JOINTS WILL BE AT SIDE STREETS AND AT LIMITS OF THE PROJECT. ALL BUTT JOINTS ARE INCLUDED IN THE COST OF SURFACE COURSE.



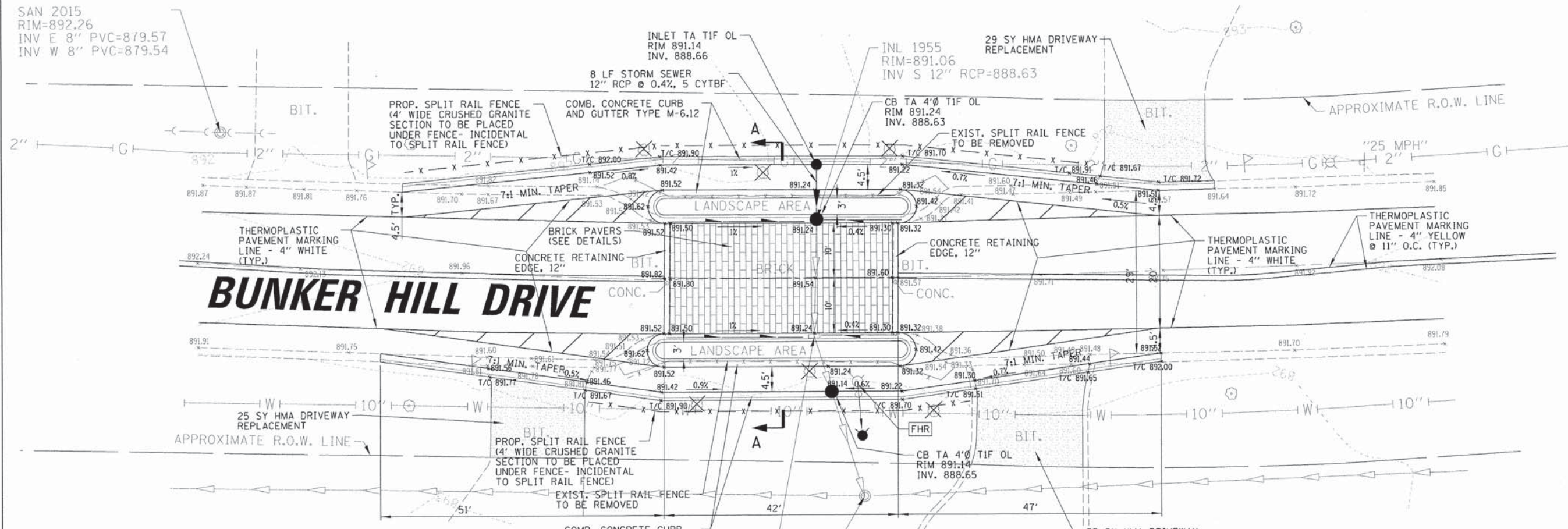
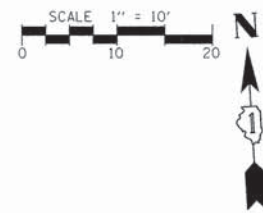
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	PLOT DATE = 12/23/2013	DATE - 12/20/13	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

BUNKER HILL DRIVE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EXISTING CONDITIONS AND REMOVAL PLAN		4013	13-00085-00-RS	McHENRY	26	9
SCALE: 4/8"		SHEET NO. 9 OF 26 SHEETS		STA. 46+00 TO STA. 60+00		CONTRACT NO. 63399

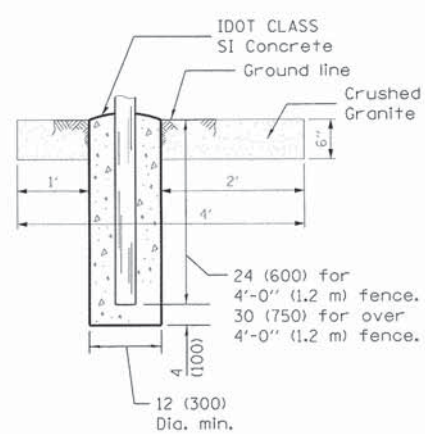
ILLINOIS FED. AID PROJECT						
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SAN 2015
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INV E 8" PVC=879.57
INV W 8" PVC=879.54

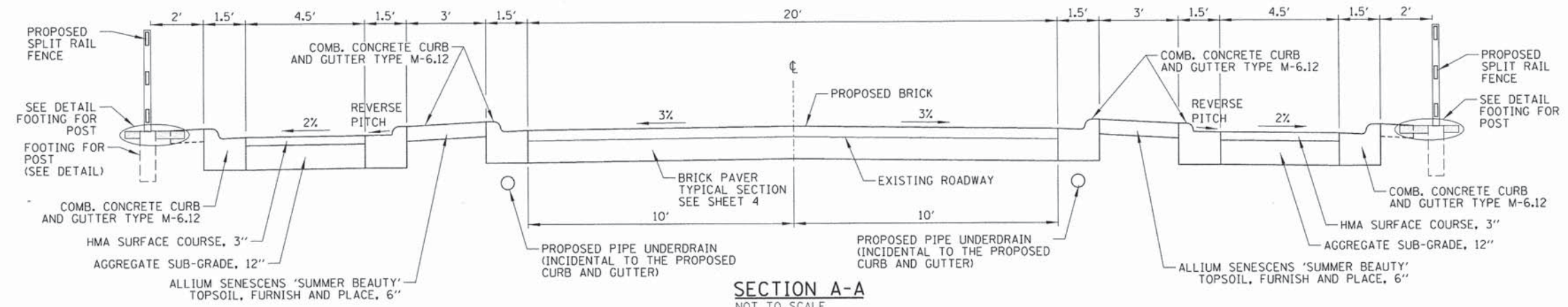


NOTE:
1. THE BRICK PAVER REMOVAL AND REPLACEMENT SHALL OCCUR AT ONE LOCATION AT A TIME. ONCE THE FIRST LOCATION HAS BEEN COMPLETED THE NEXT LOCATION CAN BE DISTURBED. A SEQUENCING PLAN SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW.
2. CURB REMOVAL AND CONCRETE CURB AND GUTTER, TYPE M-6.12 ONLY FROM STATION 31+95 TO 33+30.

- LEGEND:**
- TREE REMOVAL
 - BRICK PAVERS
 - FIRE HYDRANT TO BE RELOCATED
 - HMA DRIVEWAY REPLACEMENT



FOOTING FOR POST
NOT TO SCALE

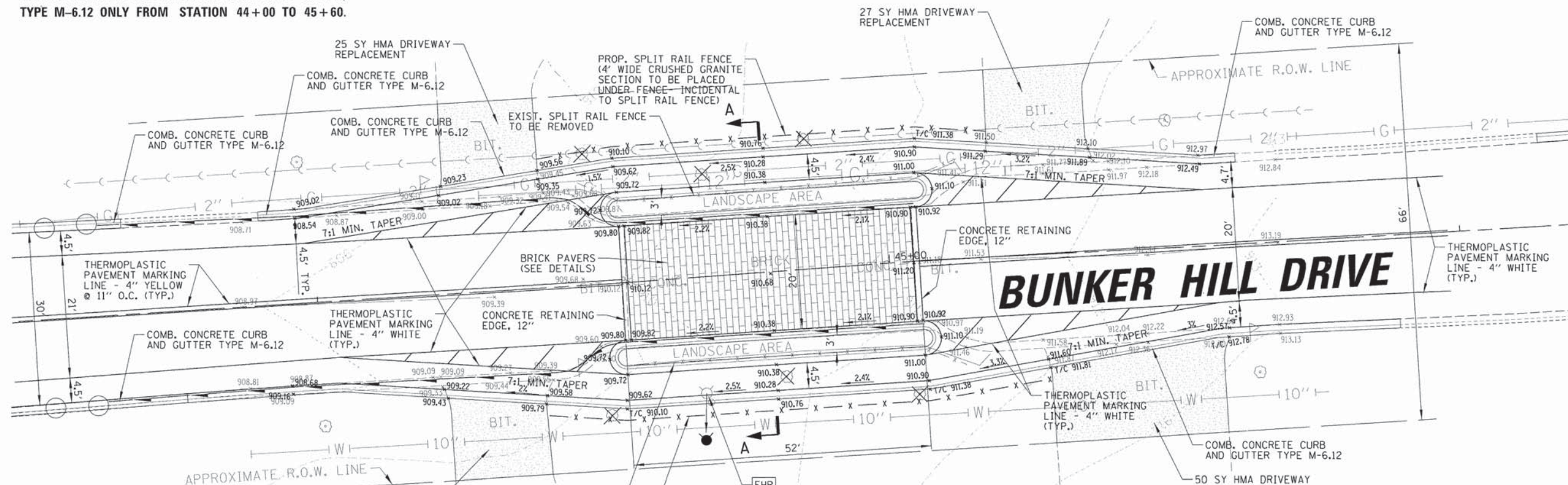
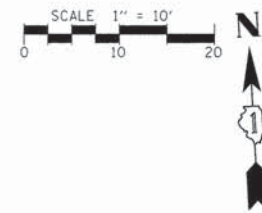


SECTION A-A
NOT TO SCALE

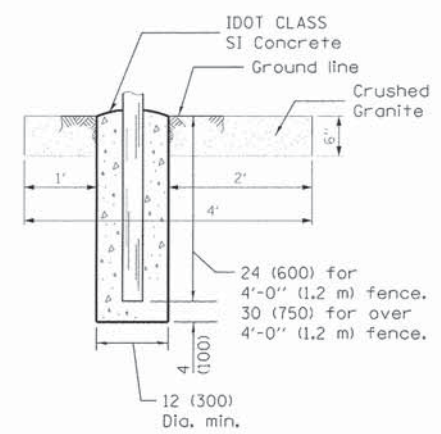
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	PLOT SCALE = 10'	CHECKED - JGS	REVISED -		SCALE: 10' = 1" TO STA.	SHEET NO. 10 OF 26 SHEETS	STA. TO STA.	CONTRACT NO. 63399		ILLINOIS FED. AID PROJECT	
PLOT DATE = 12/23/2013	DATE = 12/20/13	DATE = 12/20/13	REVISED -								

NOTE:

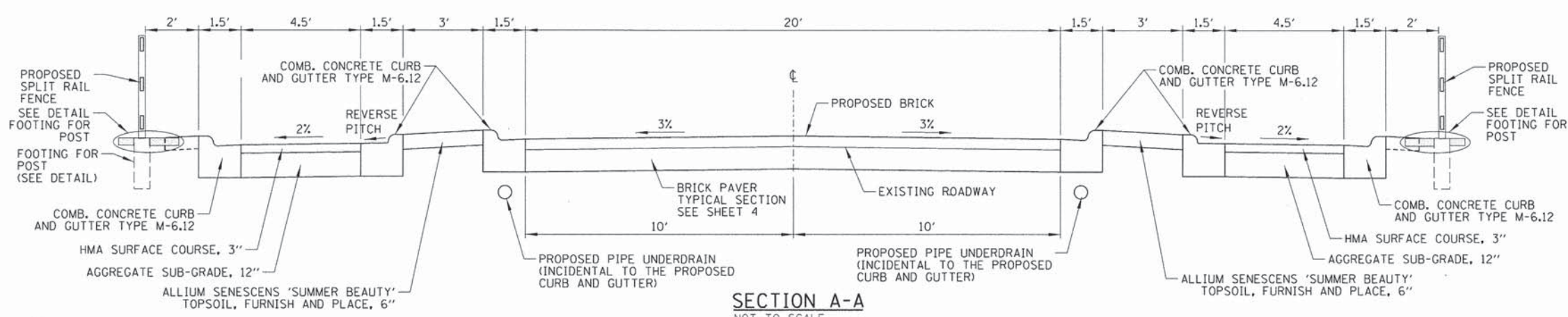
1. THE BRICK PAVER REMOVAL AND REPLACEMENT SHALL OCCUR AT ONE LOCATION AT A TIME. ONCE THE FIRST LOCATION HAS BEEN COMPLETED THE NEXT LOCATION CAN BE DISTURBED. A SEQUENCING PLAN SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW.
2. CURB REMOVAL AND CONCRETE CURB AND GUTTER, TYPE M-6.12 ONLY FROM STATION 44+00 TO 45+60.



- LEGEND:**
- TREE REMOVAL
 - BRICK PAVERS
 - FIRE HYDRANT TO BE RELOCATED
 - HMA DRIVEWAY REPLACEMENT

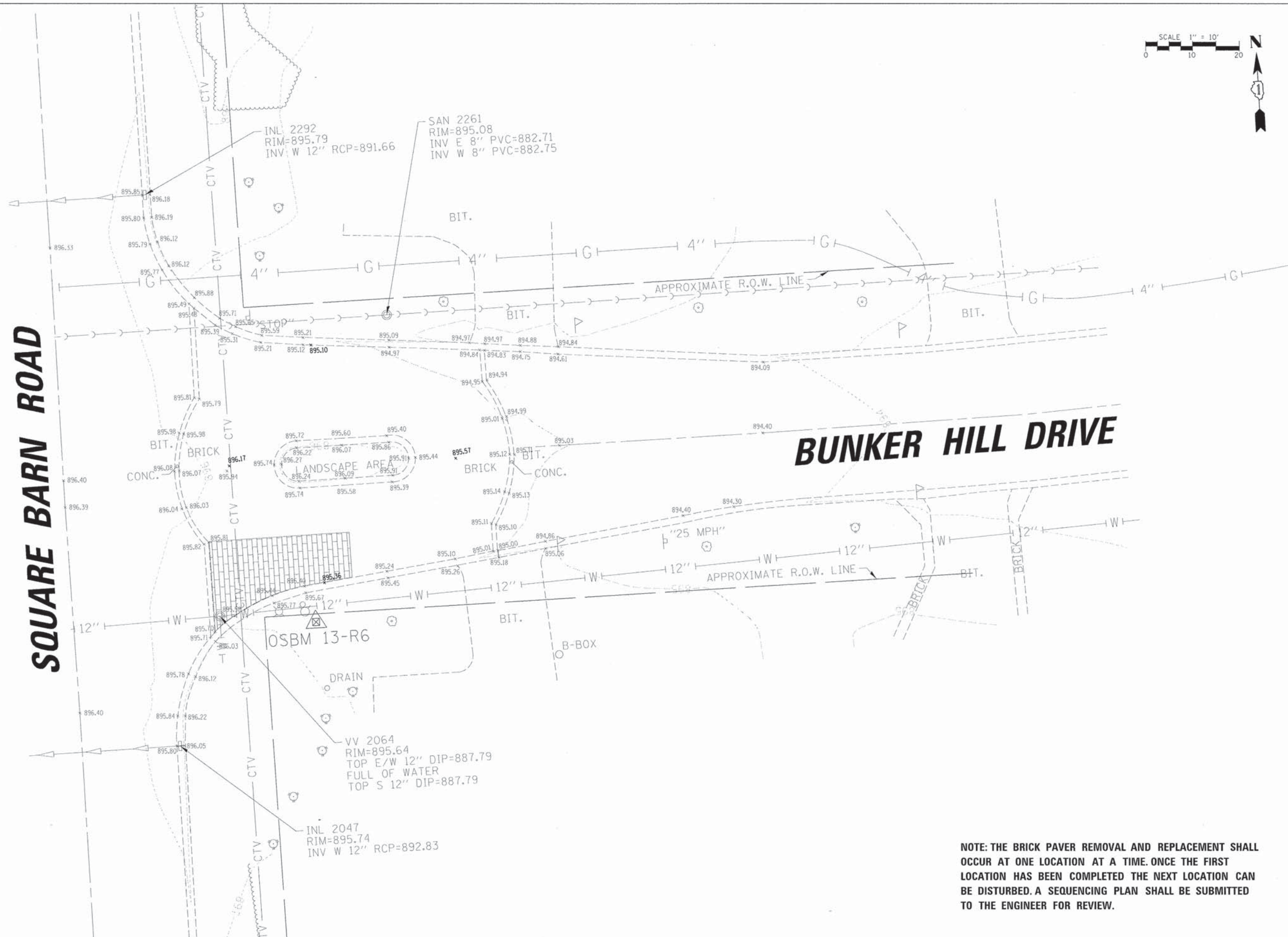
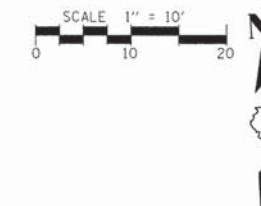


FOOTING FOR POST
NOT TO SCALE



SECTION A-A
NOT TO SCALE

FILE NAME =	USER NAME = edtda	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUNKER HILL DRIVE PROPOSED GRADING FOR CHOKERS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT SCALE = 1/8"	CHECKED - JGS	REVISED -									
	PLOT DATE = 12/23/2013	DATE - 12/20/13	REVISED -									
CONTRACT NO. 63399												
ILLINOIS FED. AID PROJECT												

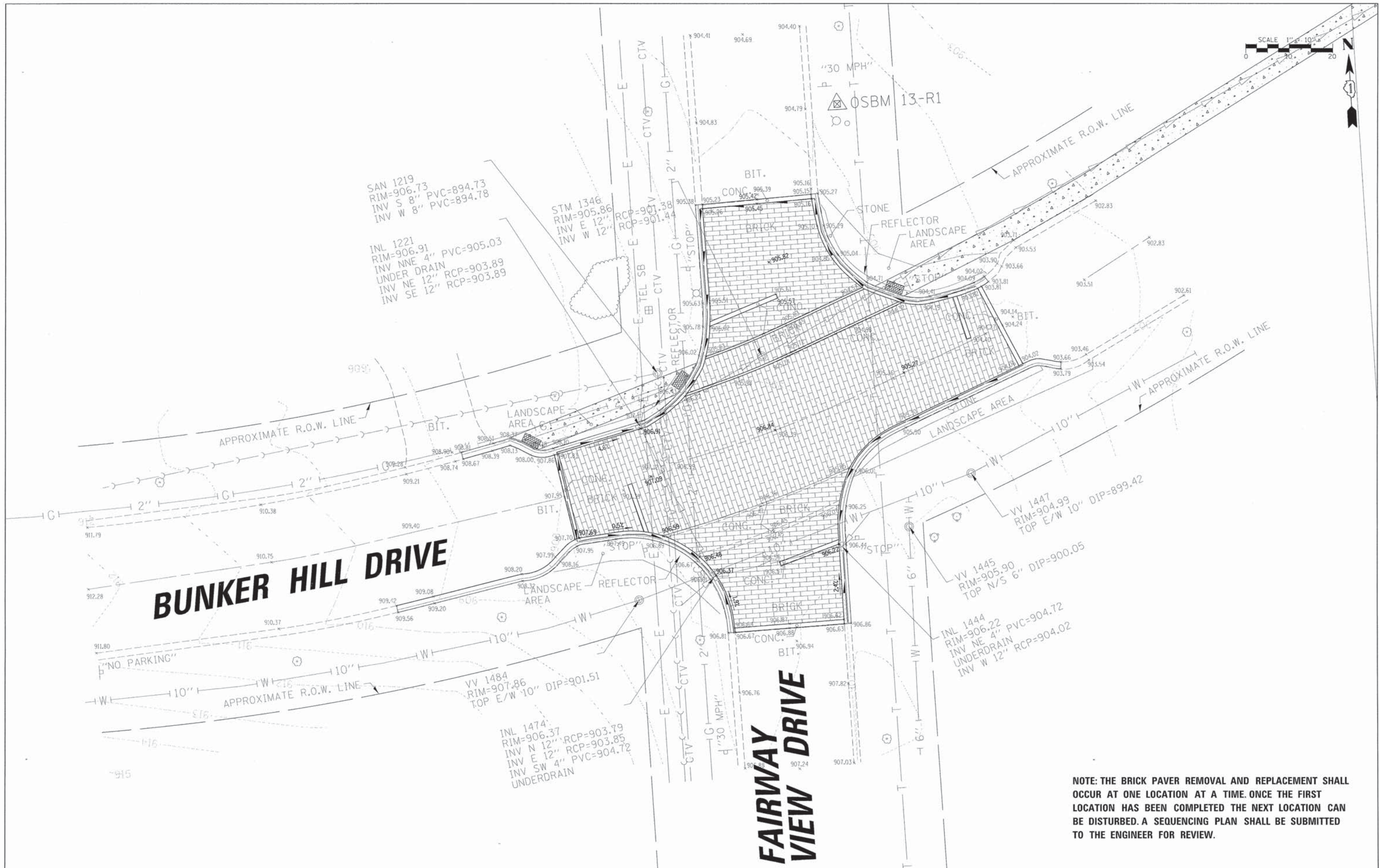
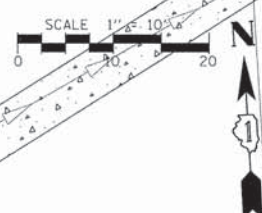


BUNKER HILL DRIVE

SQUARE BARN ROAD

NOTE: THE BRICK PAVER REMOVAL AND REPLACEMENT SHALL OCCUR AT ONE LOCATION AT A TIME. ONCE THE FIRST LOCATION HAS BEEN COMPLETED THE NEXT LOCATION CAN BE DISTURBED. A SEQUENCING PLAN SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW.

FILE NAME = N:\ALGONDUI\107273\107273_00030\CIVIL\	USER NAME = edtda LN3_07027300030A.SHT	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUNKER HILL DRIVE AND SQUARE BARN ROAD INTERSECTION		F.A.U. RTE. 4013	SECTION 13-00085-00-RS	COUNTY McHENRY	TOTAL SHEETS 26	SHEET NO. 12
	PLOT SCALE = 10'	DRAWN - EDT	REVISED -		SCALE: 1/8"	SHEET NO. 12 OF 26 SHEETS	STA. TO STA.	CONTRACT NO. 63399		ILLINOIS FED. AID PROJECT	
	PLOT DATE = 12/23/2013	CHECKED - JGS	REVISED -								
		DATE - 12/20/13	REVISED -								








BUNKER HILL DRIVE

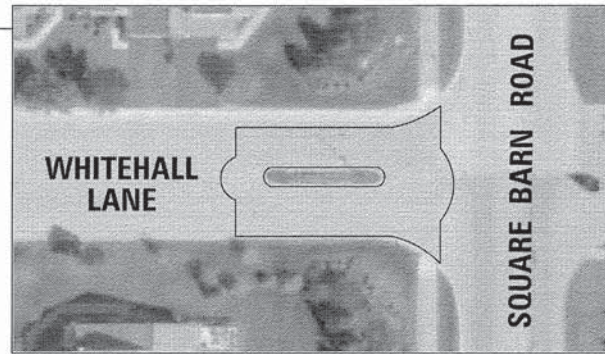
**FAIRWAY DRIVE
VIEW DRIVE**

FILE NAME =	USER NAME = edtdo	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUNKER HILL DRIVE AND FAIRWAY VIEW DRIVE INTERSECTION		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\ALCONDUIN\07273\07273.00030\Civil\	LN4_0727300030A.SHT	DRAWN - EDT	REVISED -		SCALE: 1/8"	SHEET NO. 13 OF 26 SHEETS	STA.	TO STA.	4013	13-00085-00-RS	MCHENRY	26 13
	PLOT SCALE = 1/8"	CHECKED - JGS	REVISED -									CONTRACT NO. 63399
	PLOT DATE = 12/23/2013	DATE - 12/20/13	REVISED -									ILLINOIS FED. AID PROJECT

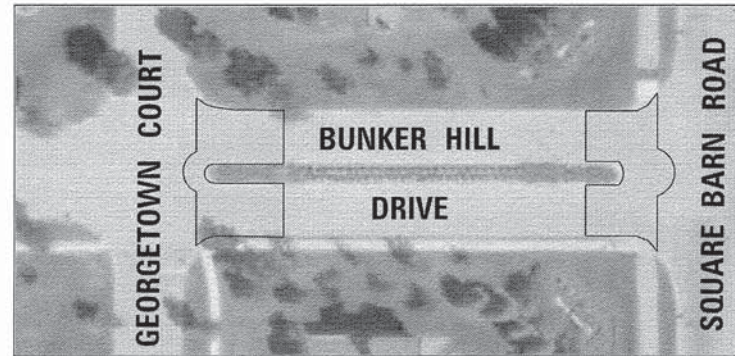
LEGEND

-  HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
-  HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 4 1/2"
-  PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK, 4"
-  COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, TYPE B-6.12
-  STRUCTURE TO BE ADJUSTED

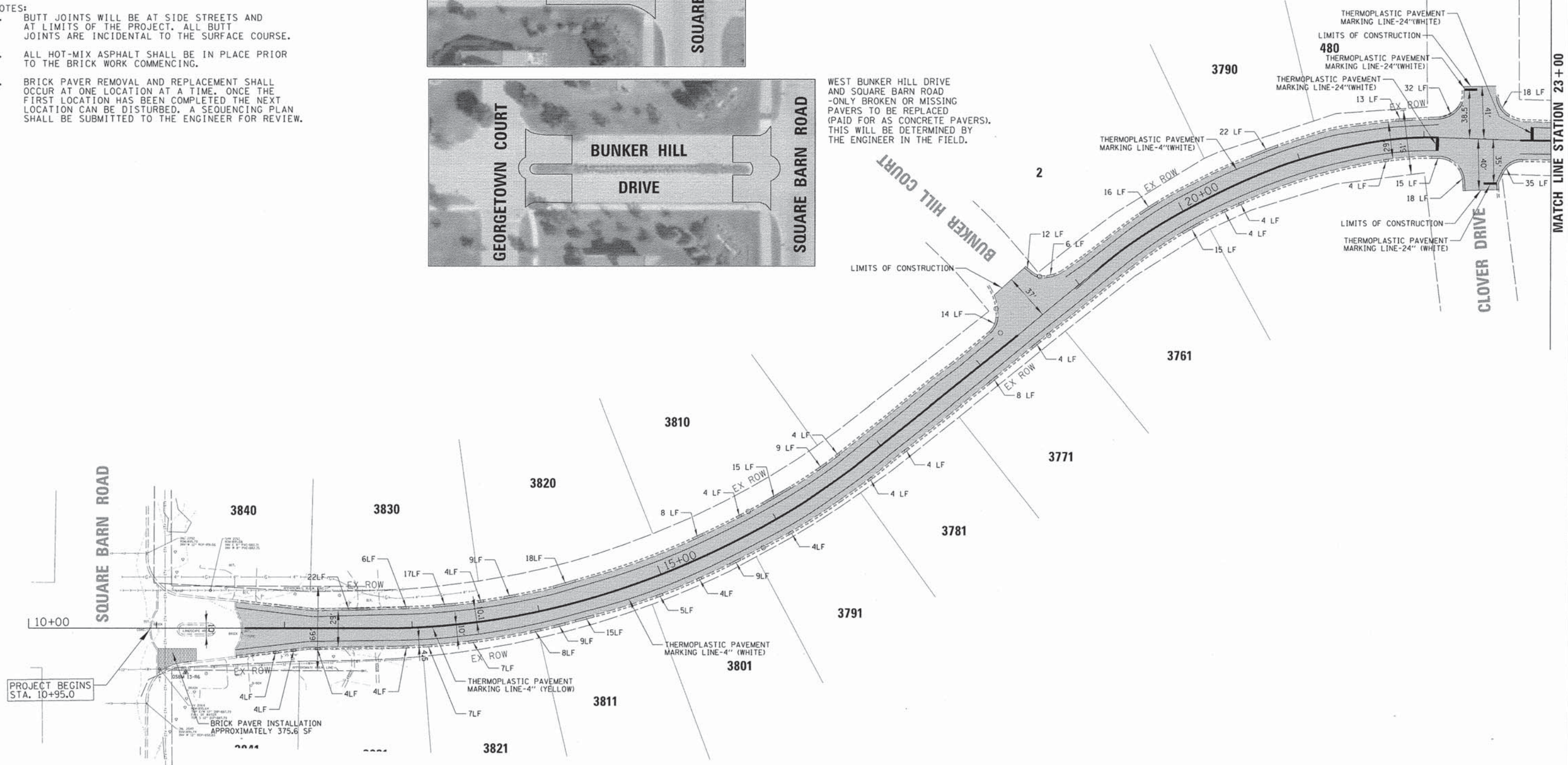
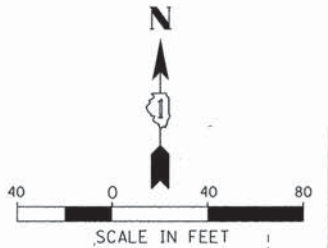
- NOTES:**
1. BUTT JOINTS WILL BE AT SIDE STREETS AND AT LIMITS OF THE PROJECT. ALL BUTT JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.
 2. ALL HOT-MIX ASPHALT SHALL BE IN PLACE PRIOR TO THE BRICK WORK COMMENCING.
 3. BRICK PAVER REMOVAL AND REPLACEMENT SHALL OCCUR AT ONE LOCATION AT A TIME. ONCE THE FIRST LOCATION HAS BEEN COMPLETED THE NEXT LOCATION CAN BE DISTURBED. A SEQUENCING PLAN SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW.



WHITEHALL LANE AND SQUARE BARN ROAD
-ONLY BROKEN OR MISSING PAVERS TO BE REPLACED (PAID FOR AS CONCRETE PAVERS). THIS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.



WEST BUNKER HILL DRIVE AND SQUARE BARN ROAD
-ONLY BROKEN OR MISSING PAVERS TO BE REPLACED (PAID FOR AS CONCRETE PAVERS). THIS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.



FILE NAME =	USER NAME = edtda	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUNKER HILL DRIVE PROPOSED PLAN	F.A.U. RTE. 4013	SECTION 13-00085-00-RS	COUNTY McHENRY	TOTAL SHEETS 26	SHEET NO. 14		
SCALE = 40'	SHEET NO. 14 OF 26 SHEETS	STA. 10+00	TO STA. 23+00			CONTRACT NO. 63399		ILLINOIS FED. AID PROJECT				
DRAWN - EDT	CHECKED - JGS	DATE - 01/02/14	REVISED -									
PL01 SCALE = 40'	PL01 DATE = 1/3/2014	BRICK PAVER INSTALLATION APPROXIMATELY 375.6 SF	REVISED -									

LEGEND

 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 4 1/2"

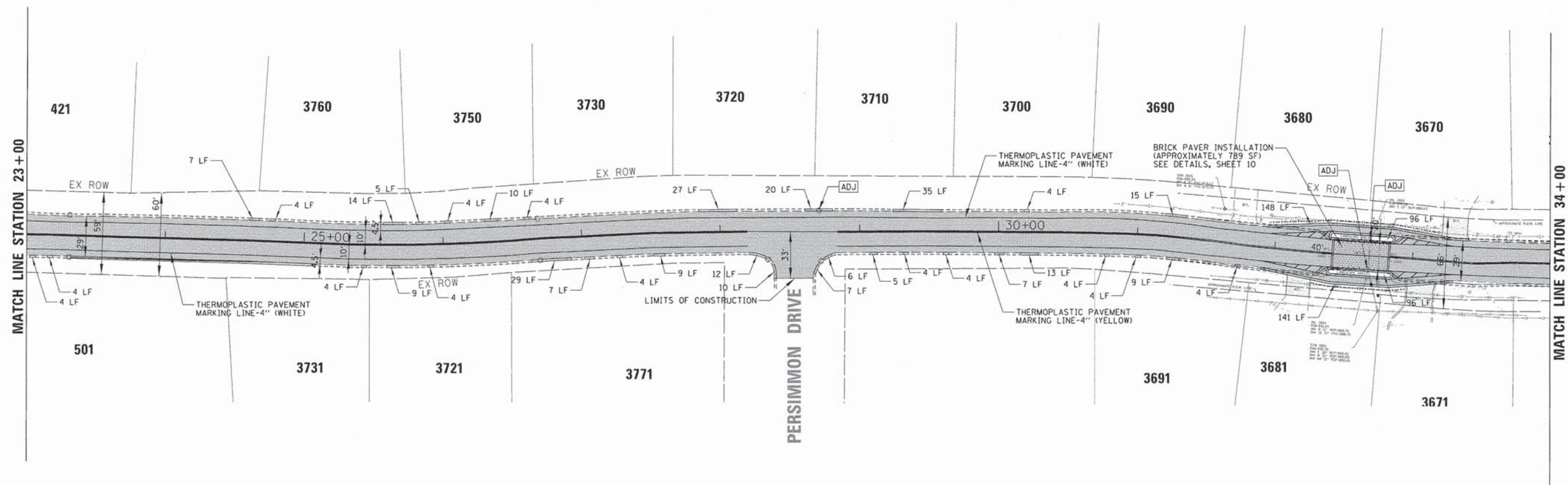
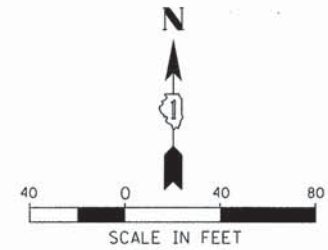
 PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK, 4"

 COMBINATION CONCRETE CURB AND GUTTER
REMOVAL AND REPLACEMENT, TYPE B-6.12

 STRUCTURE TO BE ADJUSTED

NOTES:

- BUTT JOINTS WILL BE AT SIDE STREETS AND AT LIMITS OF THE PROJECT. ALL BUTT JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.
- ALL HOT-MIX ASPHALT SHALL BE IN PLACE PRIOR TO THE BRICK WORK COMMENCING.
- BRICK PAVER REMOVAL AND REPLACEMENT SHALL OCCUR AT ONE LOCATION AT A TIME. ONCE THE FIRST LOCATION HAS BEEN COMPLETED THE NEXT LOCATION CAN BE DISTURBED. A SEQUENCING PLAN SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW.



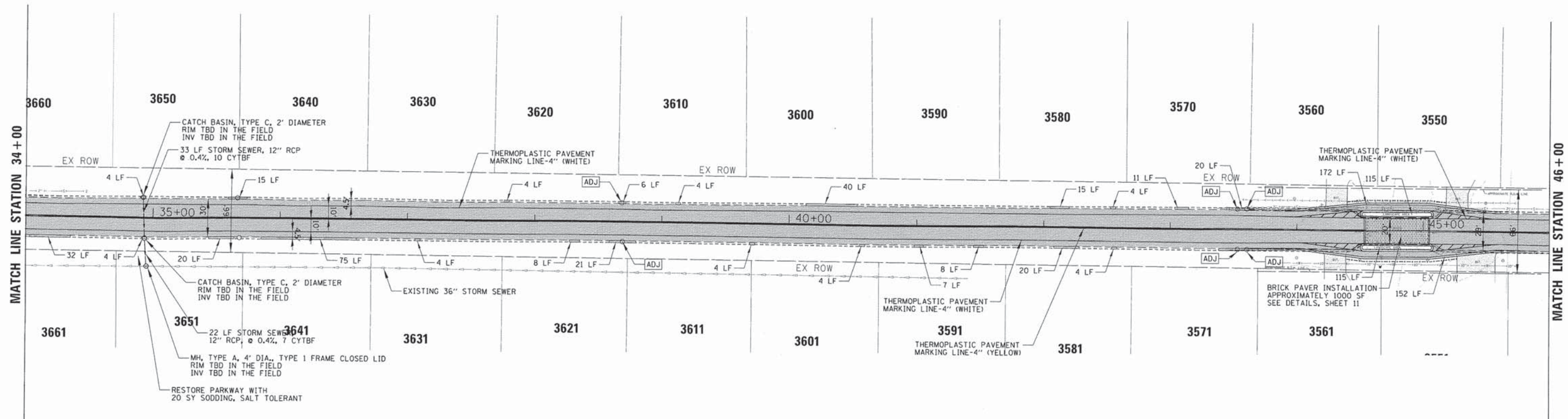
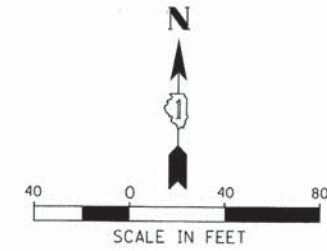
FILE NAME =	USER NAME = edtode	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUNKER HILL DRIVE PROPOSED PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
\\VAL.GONQUIN\070273\070273.0003R\Civil\1\1\1\070273.dgn	PL. DT SCALE = 40'	DRAWN - EDT	REVISED -			4013	13-00085-00-RS	McHENRY	26	15	
	PL. DT DATE = 1/3/2014	CHECKED - JGS	REVISED -			CONTRACT NO. 63399					
		DATE - 01/02/14	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE: 40'	SHEET NO. 15 OF 26 SHEETS		STA. 23+00 TO STA. 34+00			

LEGEND

- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
- HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 4 1/2"
- PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK, 4"
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, TYPE B-6.12
- ADJ STRUCTURE TO BE ADJUSTED

NOTES:

1. BUTT JOINTS WILL BE AT SIDE STREETS AND AT LIMITS OF THE PROJECT. ALL BUTT JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.
2. ALL HOT-MIX ASPHALT SHALL BE IN PLACE PRIOR TO THE BRICK WORK COMMENCING.
3. BRICK PAVER REMOVAL AND REPLACEMENT SHALL OCCUR AT ONE LOCATION AT A TIME. ONCE THE FIRST LOCATION HAS BEEN COMPLETED THE NEXT LOCATION CAN BE DISTURBED. A SEQUENCING PLAN SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW.



FILE NAME =	USER NAME = edtda	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUNKER HILL DRIVE PROPOSED PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN - EDT	REVISED -			4013	13-00085-00-RS	MCHEMRY	26	16	
		CHECKED - JGS	REVISED -			CONTRACT NO. 63399					
		DATE = 12/20/13	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE: 48'		SHEET NO. 16 OF 26 SHEETS		STA. 34+00 TO STA. 46+00		

LEGEND

 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 4 1/2"

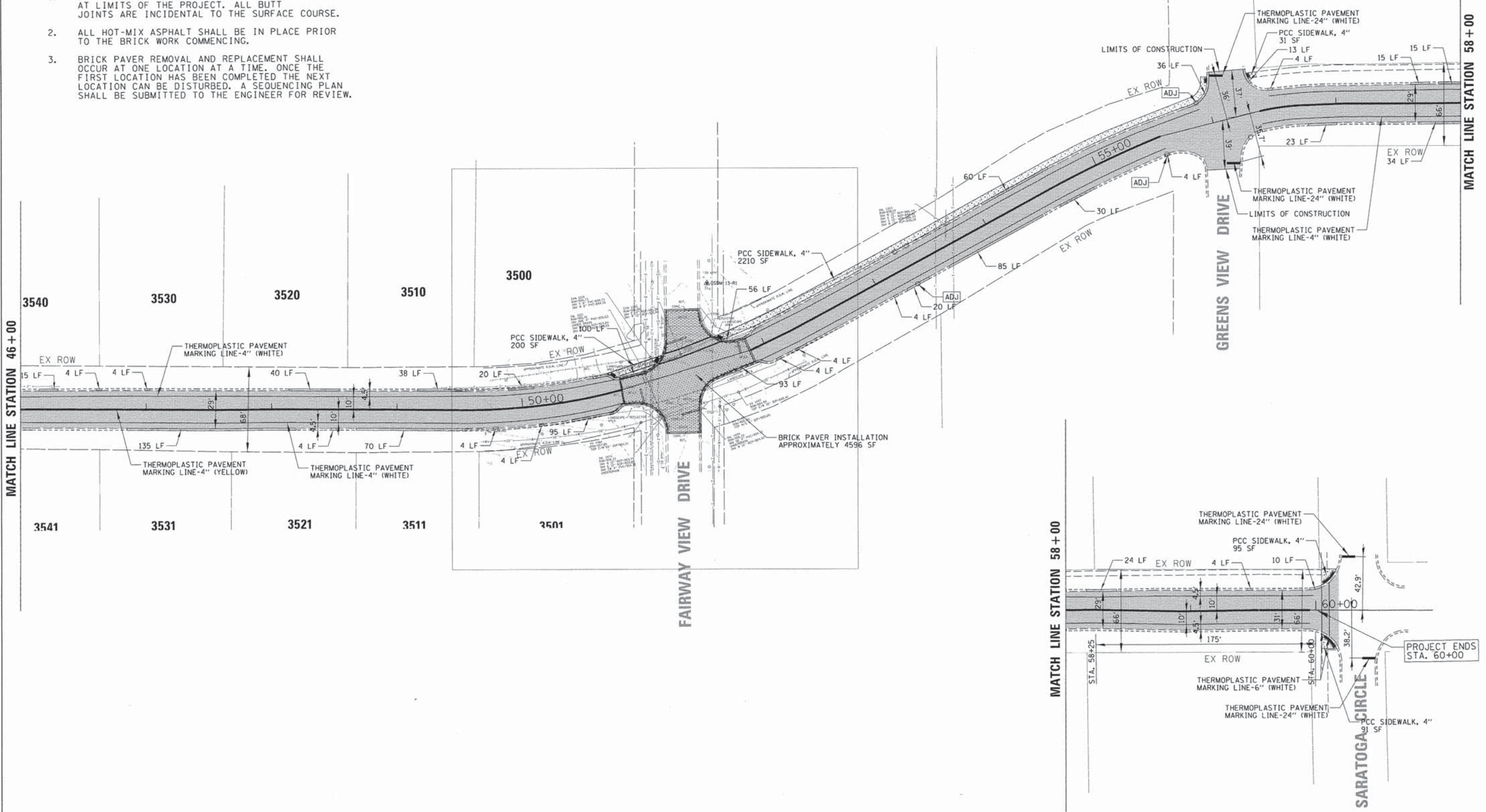
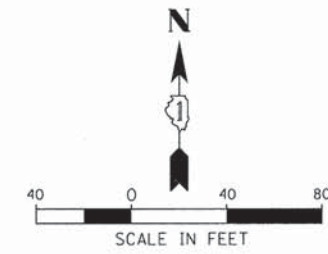
 PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK, 4"

 COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, TYPE B-6.12

 STRUCTURE TO BE ADJUSTED

NOTES:

1. BUTT JOINTS WILL BE AT SIDE STREETS AND AT LIMITS OF THE PROJECT. ALL BUTT JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.
2. ALL HOT-MIX ASPHALT SHALL BE IN PLACE PRIOR TO THE BRICK WORK COMMENCING.
3. BRICK PAVER REMOVAL AND REPLACEMENT SHALL OCCUR AT ONE LOCATION AT A TIME. ONCE THE FIRST LOCATION HAS BEEN COMPLETED THE NEXT LOCATION CAN BE DISTURBED. A SEQUENCING PLAN SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW.



FILE NAME =	USER NAME = edtda	DESIGNED - LMF	REVISED -
H:\ALGONDUI\N\078273\078273.00030\Civil\N\R_078273_4.dgn		DRAWN - EDT	REVISED -
	PLOT SCALE = 48"	CHECKED - JGS	REVISED -
	PLOT DATE = 12/23/2013	DATE - 12/20/13	REVISED -

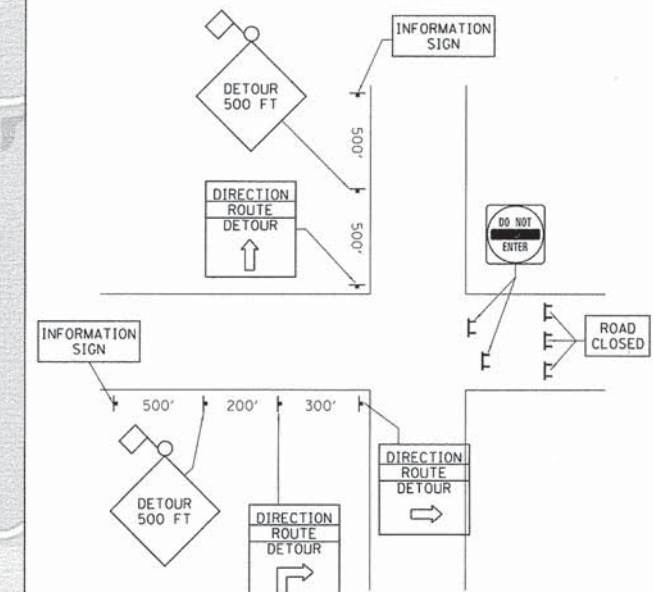
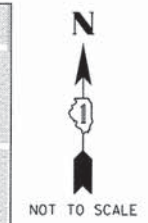
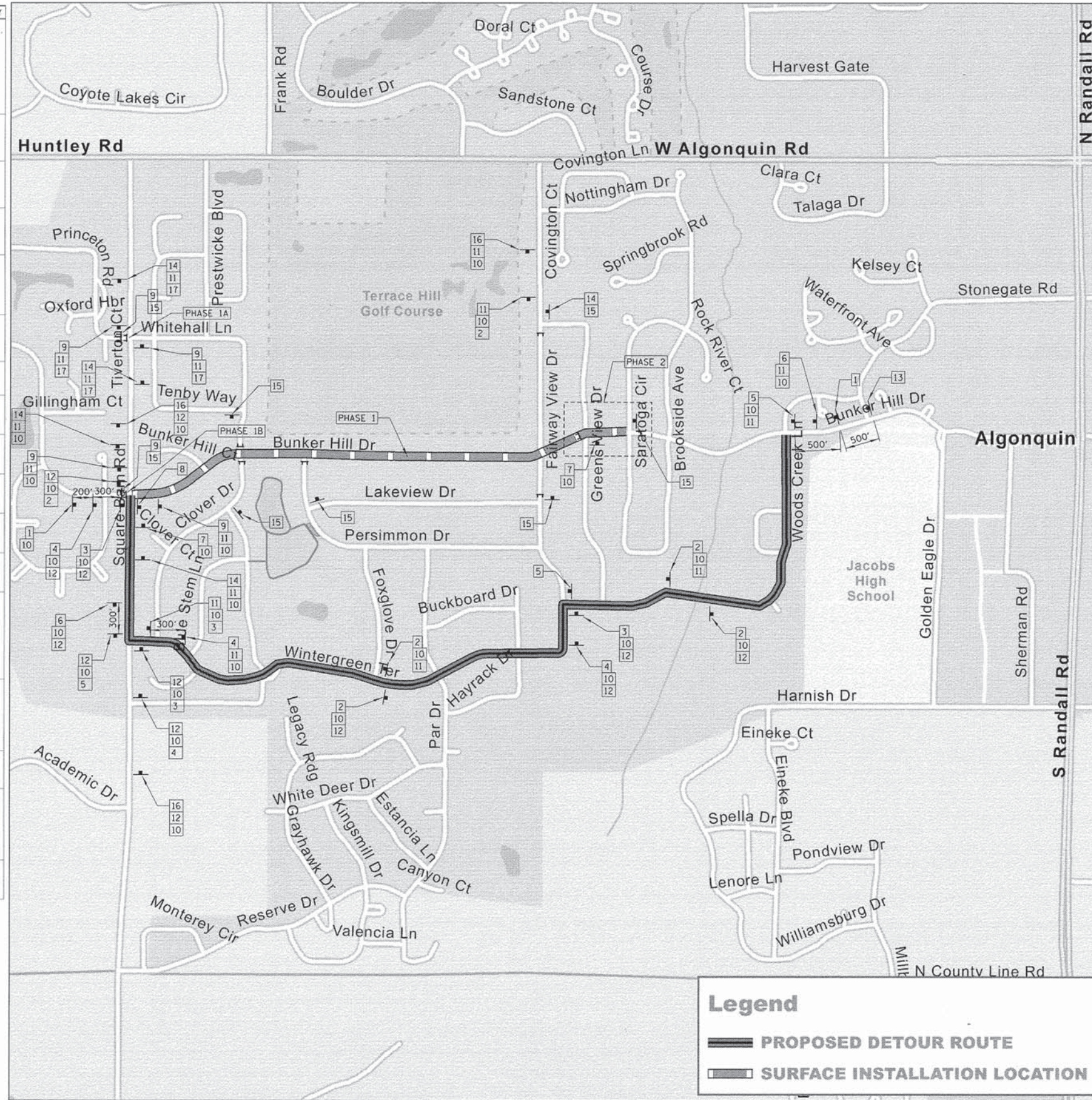
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUNKER HILL DRIVE PROPOSED PLAN	
SCALE: 48"	SHEET NO. 17 OF 26 SHEETS
STA. 46+00	TO STA. 60+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4013	13-00085-00-RS	McHENRY	26	17
CONTRACT NO. 63399			ILLINOIS FED. AID PROJECT	

SCHEDULE OF SIGNS

SIGN NO.	SIGN TYPE	QUANTITY
1	W20-2 (O) 48	2
2	M4-9 (O) 30 21	6
3	M4-9 R (O) 30 21	4
4	M4-9 R (O) 30 21	4
5	M4-9 L (O) 30 21	3
6	M4-9 L (O) 30 21	2
7	M4-9a (O) 24 18	2
8	R-5-1-3030	1
9	R-11-2-4830	6
10	SPECIAL NO. 1	28
11	M3-3 (O) 24 12	16
12	M3-1 (O) 24 12	13
13	To Thru Traffic Follow Detour Beginning (month) (day)	1
14	W20-1103 (O)-48	5
15	R11-4-6030	8
16	W20-2 (FO)	3
17	SPECIAL NO. 1	4



TYPICAL SIGN INSTALLATION

LEGEND

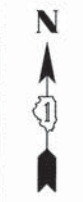
- 48" x 48" CONSTRUCTION WARNING SIGN WITH AMBER FLASHING LIGHT AND ORANGE WARNING FLAG. NUMBER DENOTES TYPE.
- TYPE III BARRICADE WITH AMBER FLASHING LIGHTS.

- NOTES:
- THIS WORK IS TO BE PAID FOR AS PART OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
 - THE DETOUR IS ONLY IN EFFECT FOR 20 WORKING DAYS FOR THE BRICK PAVER REMOVAL AND REPLACEMENT.
 - THE BRICK PAVER REMOVAL AND REPLACEMENT SHALL OCCUR AT ONE LOCATION AT A TIME, ONCE THE FIRST LOCATION HAS BEEN COMPLETED THE NEXT LOCATION CAN BE DISTURBED. A SEQUENCING PLAN SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW.
 - PHASE 1A IS TO BE COMPLETED FIRST, ONCE PHASE 1A IS COMPLETED, PHASE 1B CAN BEGIN. THE TRAFFIC CONTROL FOR PHASE 1B SHOULD NOT BE ERRECTED UNTIL PHASE 1A IS COMPLETED. PHASE 1 CAN BE COMPLETED WHILE PHASES 1A AND 1B ARE BEING CONSTRUCTED.

Legend

PROPOSED DETOUR ROUTE

SURFACE INSTALLATION LOCATION



- LEGEND**
- WALK PATH REMOVAL AND REPLACEMENT
 - HMA SURFACE REMOVAL, 1/4"
 - HMA SURFACE COURSE, MIX "D", NSO, 2"

FILE NAME :	USER NAME :	DESIGNED :	REVISED :
N:\ALCONQUIN\878273\878273.00230\Civil\878273.WALK.ACCESS.shx	@edtd	LMF	-
PLOT SCALE :	CHECKED :	DATE :	REVISED :
NOT TO SCALE	JGS	12/20/13	-
PLOT DATE :			
12/23/2013			

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUNKER HILL DRIVE
WALKING PATH AND ACCESS PLAN**

SCALE: NOT TO SCALE SHEET NO. 19 OF 26 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4013	13-00085-00-RS	MCHENRY	26	19
CONTRACT NO. 63399				
ILLINOIS FED. AID PROJECT				

CONSTRUCTION PROCEDURES

STAGE I (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12" OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36" DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1" THICK HOT-MIX MATERIAL APPROVED BY THE ENGINEER.

STAGE II (AFTER PAVEMENT MILLING)

- A) REMOVE THE HOT-MIX MATERIAL AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602 AND 603 OF THE STANDARD SPECIFICATIONS.

LOCATION OF STRUCTURES

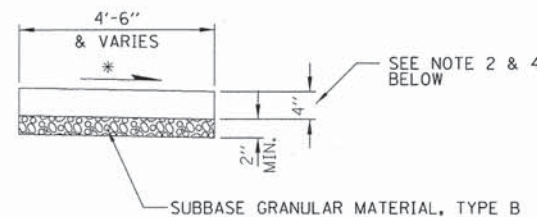
THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

STRUCTURE TO BE ADJUSTED.

NOTES

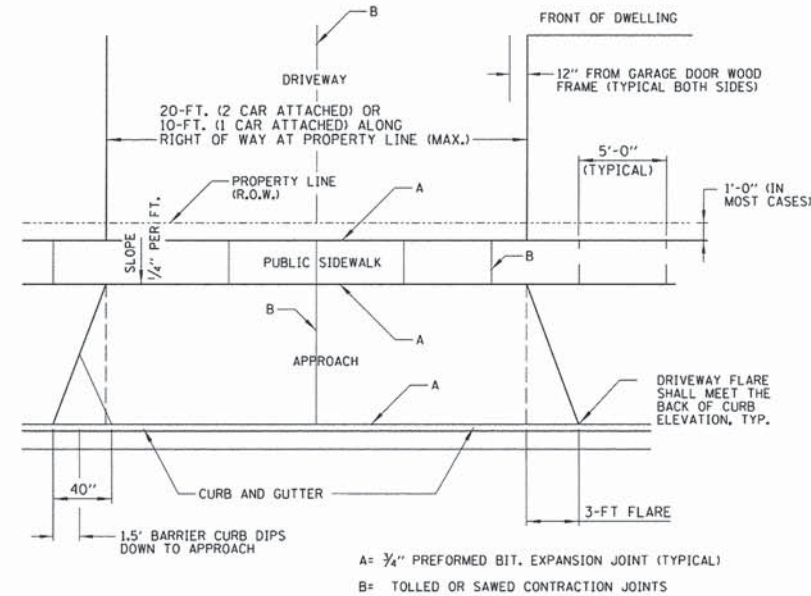
- 1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- 2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- 3. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- 4. EXTERNAL MANHOLE CHIMNEY SEAL SHALL BE PROVIDED AND SHALL CONSIST OF A RUBBER SLEEVE, COMPRESSION BAND AND EXTENSION SKIRT. RUBBER SLEEVE SHALL BE HIGH GRADE RUBBER COMPOUND CONFORMING TO ASTM C293 WITH A HARDNESS OF 45 PLUS OR MINUS 5. COMPRESSION BANDS SHALL BE 16 GAUGE TYPE 304 STAINLESS STEEL WITH A MINIMUM WIDTH OF 1 INCH. EXTENSION WEIGHT OF 12 OUNCES PER SQUARE YARD. EXTERNAL MANHOLE CHIMNEY SEAL SHALL BE MANUFACTURED BY CANUSA, INFISHIELD OR APPROVED EQUAL.



CROSS SLOPE 2% OR AS SHOWN ON CROSS SECTIONS

- 1. ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK SHALL BE INCLUDED IN THE COST OF P.C.C. SIDEWALK 4 INCH, REMOVE AND REPLACE
- 2. WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFILLED WITHIN 24 HOURS.
- 3. SEEDING AND TOPSOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF SIDEWALK REMOVAL AND REPLACEMENT.
- 4. PUBLIC SIDEWALK SHALL BE 6" AT RESIDENTIAL DRIVEWAYS AND 8" AT COMMERCIAL/INDUSTRIAL DRIVEWAYS.

P.C.C. SIDEWALK DETAIL



DRIVEWAY WITH A CURB AND GUTTER

GENERAL NOTES:

- 1. DRIVEWAY SHALL HAVE A MIN. SLOPE OF 2% AND MAX. SLOPE OF 6%.
- 2. APPROACH SHALL HAVE A MIN. SLOPE OF 2% AND MAX. OF 6%.
- 3. ALL AGGREGATE SUB-BASE SHALL BE MECHANICALLY COMPACTED. (95% PROCTOR)
- 4. PUBLIC SIDEWALK SHALL BE 6" AT RESIDENTIAL DRIVEWAYS AND 8" AT COMMERCIAL/INDUSTRIAL DRIVEWAYS. (NO WIRE MESH)
- 5. MINIMUM THICKNESS FOR APPROACH. (NO WIRE MESH). THIS WILL BE PAID FOR BY THE FOLLOWING ITEMS:
 - A. 7" THK. P.C. CONCRETE ON 2" AGGREGATE BASE COURSE TYPE B OR
 - B. 3" THK. HOT-MIX ASPHALT SURFACE, MIX "D" N50 ON 6" AGGREGATE BASE COURSE TYPE B
- 6. SEEDING AND TOPSOIL, 4" (100) RESTORATION WILL BE PAID FOR SEPARATELY AS THEIR RESPECTIVE PAY ITEMS.

FRAME AND LID: EAST JORDAN IRON WORKS OR APPROVED EQUAL. CATALOG NO. 1050-Z1 WITH 1" CONCEALED PICK HOLE AND THE WORD "STORM" AND VILLAGE OF ALGONQUIN LOGO CAST IN LID.

ALL FLAT TOP STRUCTURES SHALL HAVE A 4" CAST RING (TO BE INCLUDED IN THE COST OF "STRUCTURES TO BE ADJUSTED IF THE STRUCTURE IS EXISTING, OR IN THE COST OF THE PROPOSED STRUCTURE IF THE STRUCTURE IS NEW).

CHIMNEY SEAL TO BE CANUSA RAPID SEAL TYPE ANS OR APPROVED EQUAL TO CAPTURE 4" OF FRAME. ALL RINGS AND 4" OF BARREL SECTION. CHIMNEY SEAL TO BE USED ON ALL SANITARY MANHOLES, AND ANY CATCH BASINS OR INLETS IN THE ROADWAY.

WALLS: PRECAST REINFORCED CONCRETE RISER UNITS
MINIMUM THICKNESSES:
5" FOR 4'-0" INSIDE DIAMETER
6" FOR 5'-0" INSIDE DIAMETER

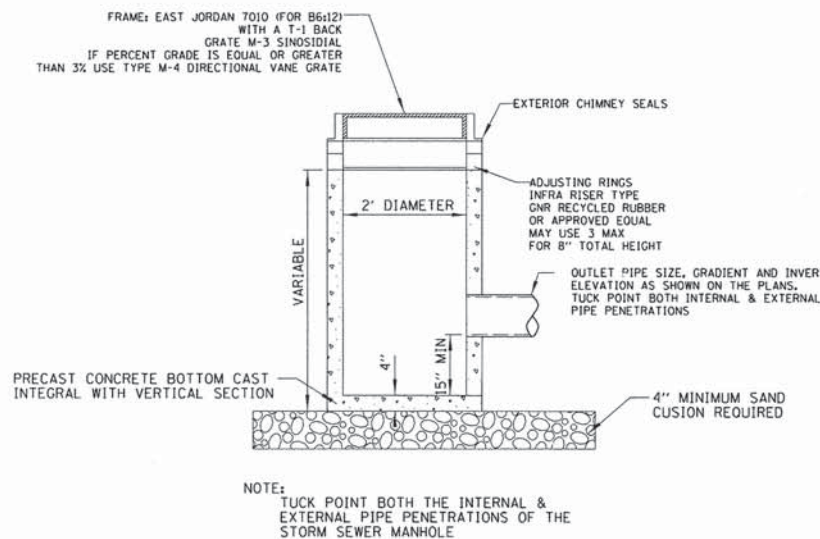
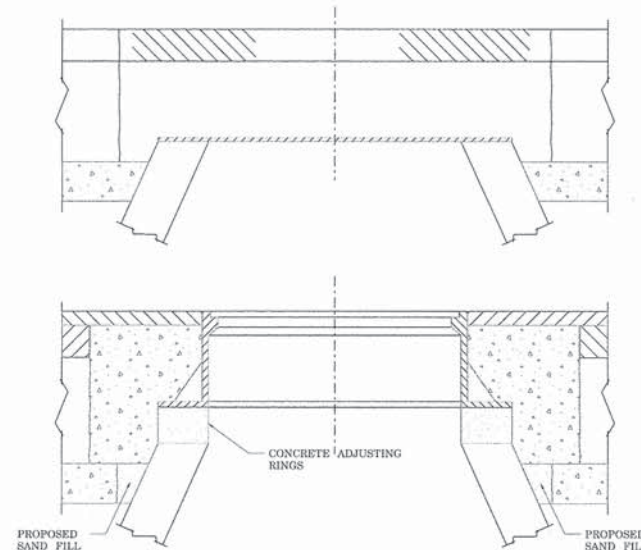
BOTTOM: PRECAST REINFORCED AND CAST INTEGRAL WITH FIRST VERTICAL SECTION.

INSIDE DIAMETERS:
4'-0" FOR 18" MAIN SEWER AND UNDER
FOR MANHOLES 20' DEEP OR LESS
5'-0" FOR 21" TO 42" MAIN SEWER
FOR MANHOLES 20' DEEP OR DEEPER
BOTH INCLUSIVE

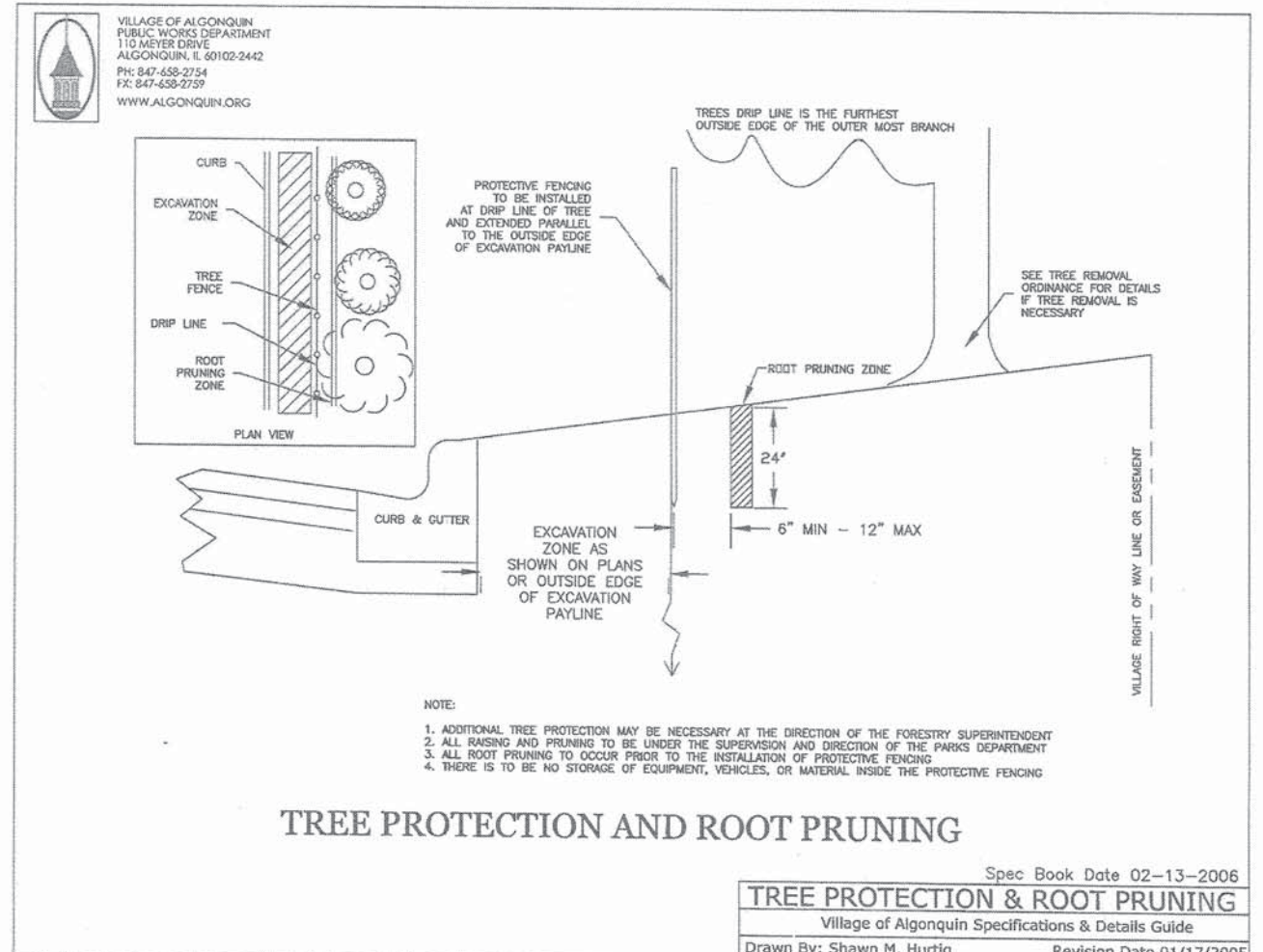
ADJUSTMENT RINGS:
TOP ADJUSTMENT RING OR RISER RING TO BE "INFRA-RISER" TYPE
GNR RECYCLED RUBBER OR APPROVED EQUAL
TOTAL OF 3 RINGS MAY BE USED
FOR A 8" MAX ADJUSTMENT

RISER JOINTS AND FRAME AND COVER:
ALL JOINTS BETWEEN PRE-CAST ELEMENTS, ADJUSTING RINGS AND MANHOLE FRAMES ON ALL UNDERGROUND STRUCTURES- ALL MANHOLES IN THE ROADWAY AND PARKWAY, SHALL BE SET IN PLACE WITH ONE OF THE FOLLOWING BUTYL RUBBER JOINT SEALANTS; CONCRETE PRODUCTS SUPPLY CO. - EZ STIK 8, HAMILTON-KENT GASKET CO. - KENT SEAL, OR EQUAL, AS APPROVED BY DISTRICT ENGINEER AND ALL JOINTS TO BE TUCKPOINTED WITH HYDRAULIC CEMENT.

**DETAILS FOR FRAMES AND LIDS
ADJUSTMENT WITH MILLING**



CATCH BASIN - TYPE C (SPECIAL)

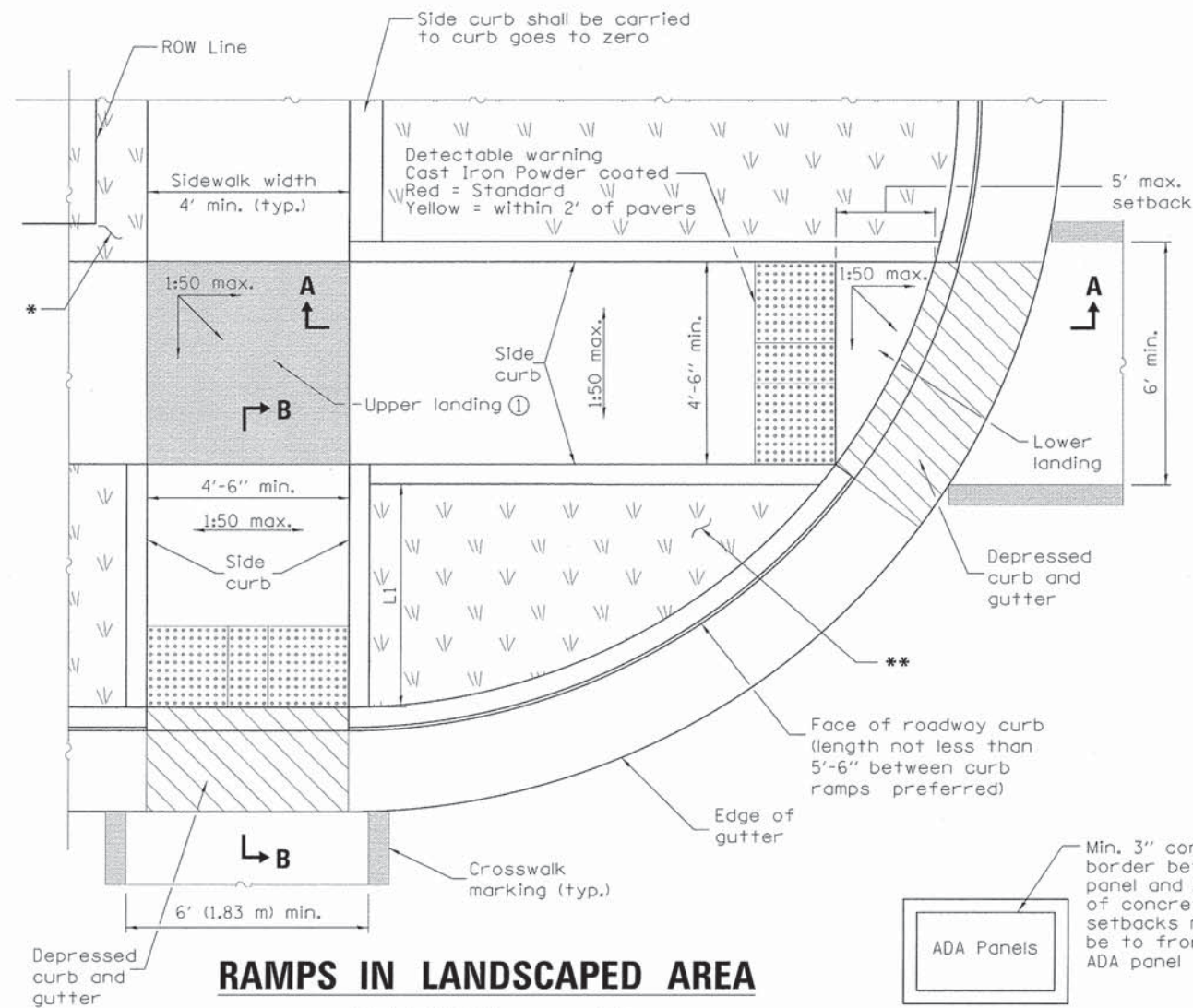


TREE PROTECTION AND ROOT PRUNING

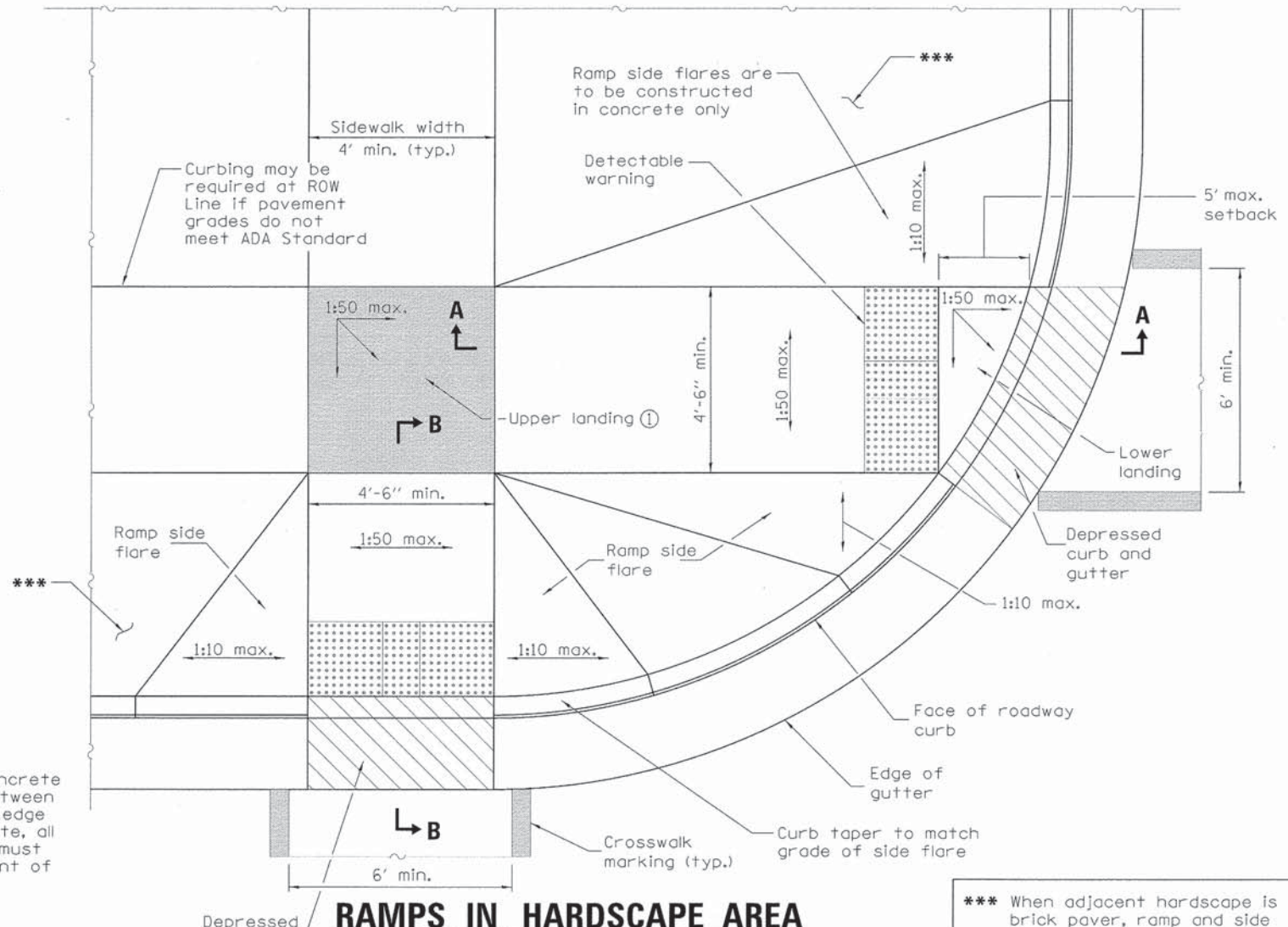
Spec Book Date 02-13-2006

TREE PROTECTION & ROOT PRUNING	
Village of Algonquin Specifications & Details Guide	
Drawn By: Shawn M. Hurtig	Revision Date 01/17/2005

FILE NAME: N:\AL GOND\IN\87273\87273.00030\Civil\st_070273_00019_01.sht	USER NAME: edLoda	DESIGNED: LMF	REVISED:	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUNKER HILL DRIVE CONSTRUCTION DETAILS	F.A.U. RTE. 4013	SECTION 13-00085-00-RS	COUNTY McHENRY	TOTAL SHEETS 26	SHEET NO. 20	CONTRACT NO. 63399
PLDT SCALE: NOT TO SCALE	PLDT DATE: 1/3/2014	DRAWN: EDT	REVISED:	SCALE: NOT TO SCALE	SHEET NO. 20 OF 26 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			

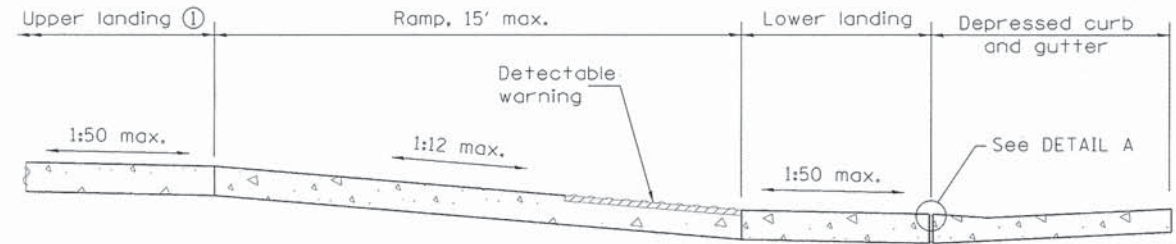


**RAMPS IN LANDSCAPED AREA
SETBACK ≤ 5'**



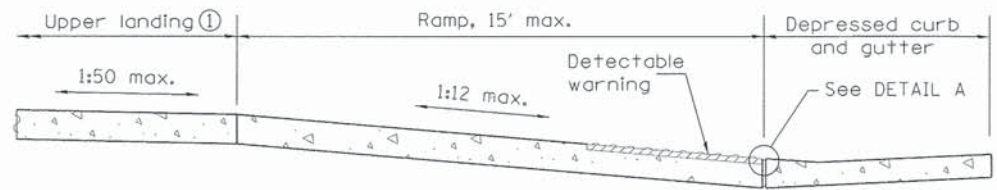
**RAMPS IN HARDSCAPE AREA
SETBACK ≤ 5'**

*** When adjacent hardscape is brick paver, ramp and side flare to be constructed in concrete.



SECTION A-A

① Upper landing not required for ramp slopes flatter than 1:20.



SECTION B-B

① Upper landing not required for ramp slopes flatter than 1:20.

GENERAL NOTES

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

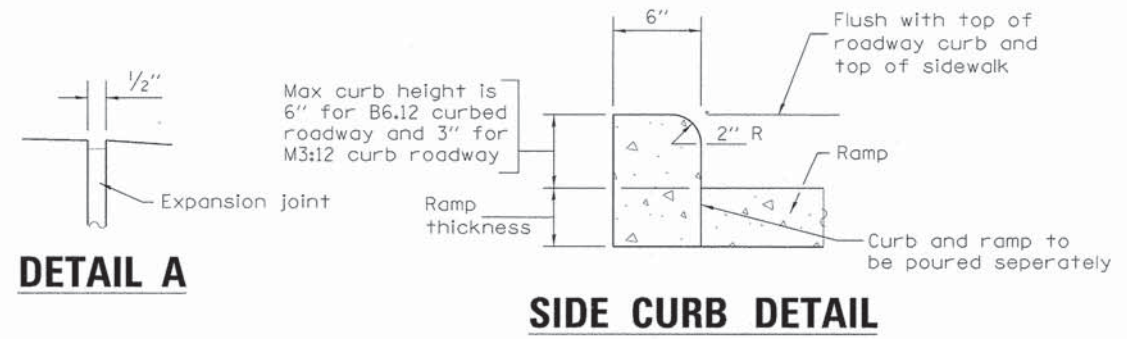
Where 1:50 maximum slope is shown, 1:64 is preferred.

All dimensions are in inches.

* If Grading exceeds 3:1 from walk to ROW then install back of walk side curb. If grade from top of back of walk curb to ROW exceeds 3:1 then a temporary easement is required for grading.
** Trapped Material must be:

L1 Length	TOPSOIL & SEED		BRICK PAVER	
	B6:12	M3:12	B6:12	M3:12
3 ft or less			x	x
3 - 6 feet		x	x	
6 - 9+ feet	x	x		

Must have mower access (3' wide uncurbed area from sidewalk to trapped material) in all cases in order for Topsoil & Seed to be installed, otherwise brick pavers are required.



DETAIL A

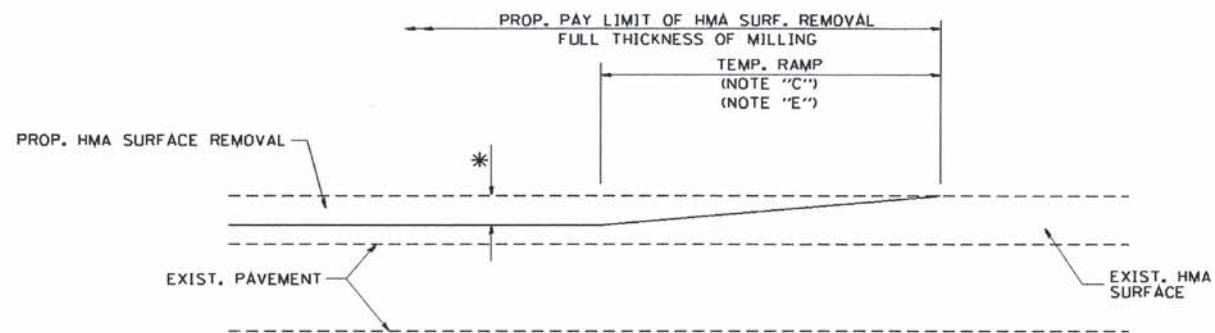
SIDE CURB DETAIL

DATE	REVISIONS
10-25-13	VILLAGE OF ALGONQUIN

PERPENDICULAR CURB RAMPS FOR SIDEWALKS

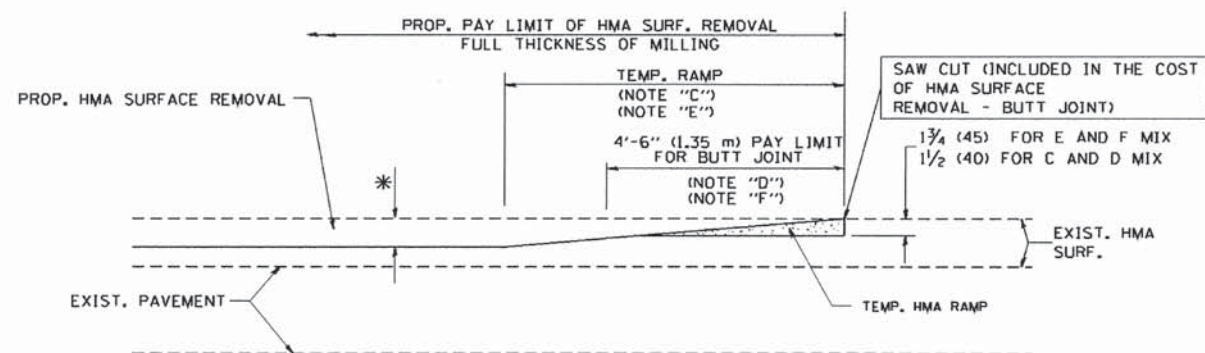
VILLAGE OF ALGONQUIN

VillageAlgon_PerpendicularCurbRamps-01.dgn



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

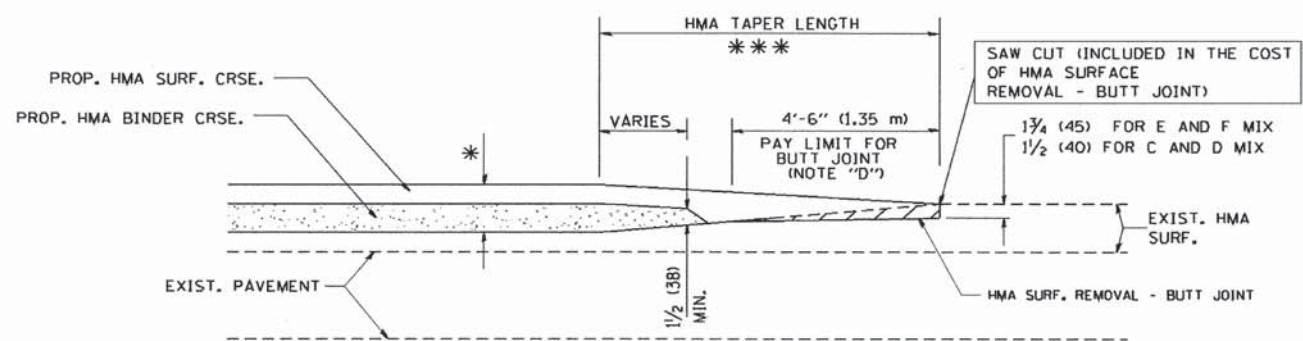
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

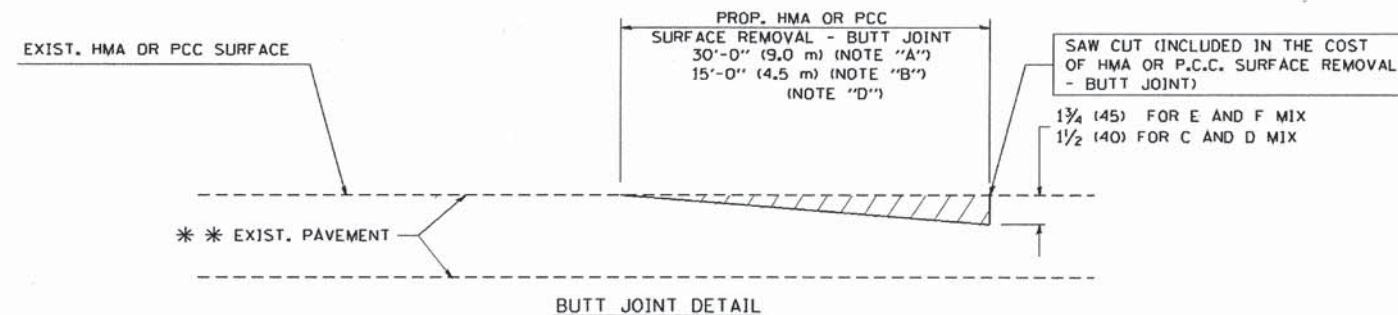
OPTION 2

TYPICAL TEMPORARY RAMP

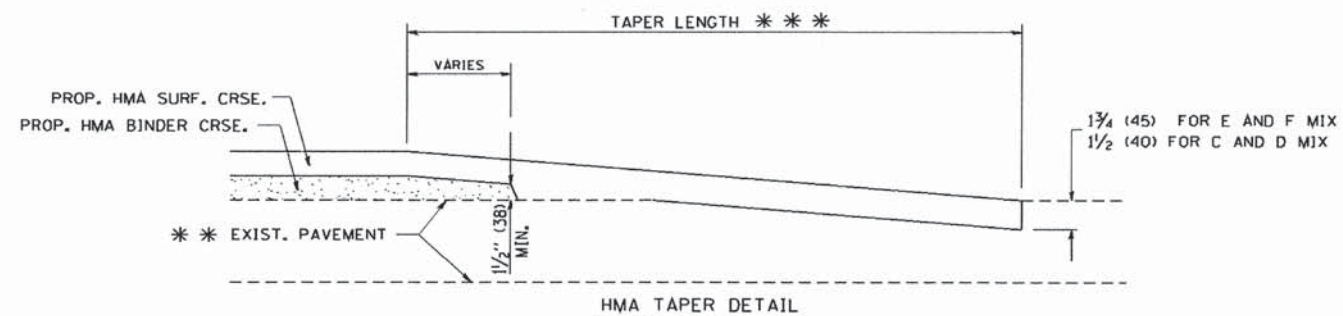


BUTT JOINT AND
HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

ALL BUTT JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\distatd\22\34\bd32.dgn	USER NAME = gaglienabt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 1/4 IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2000	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 4013	SECTION 13-00085-00-RS	COUNTY MCHENRY	TOTAL SHEETS 26	SHEET NO. 22
BD400-05 BD32		CONTRACT NO. 63399		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

SEE STATE STANDARD 606001

1/4" (5) **

18" (450) MAX.

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

1/2" *

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

3" (75) MIN.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT, DRIVEWAY REMOVAL AND REPLACEMENT, AND/OR SIDEWALK REMOVAL AND REPLACEMENT.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

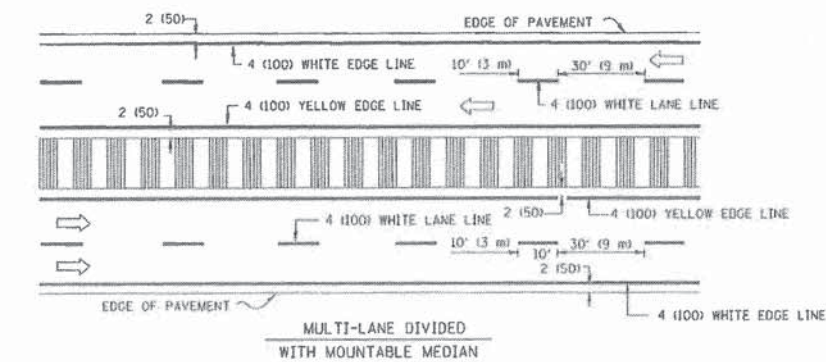
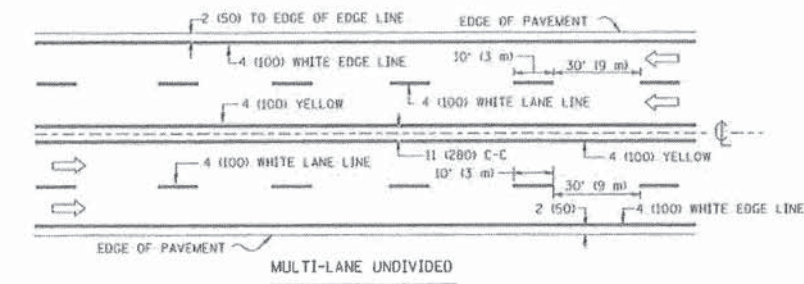
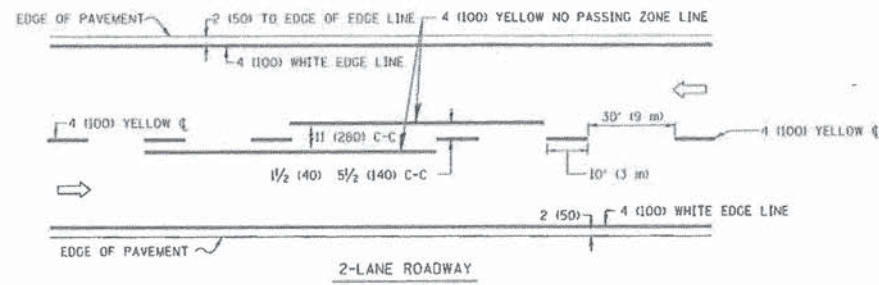
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

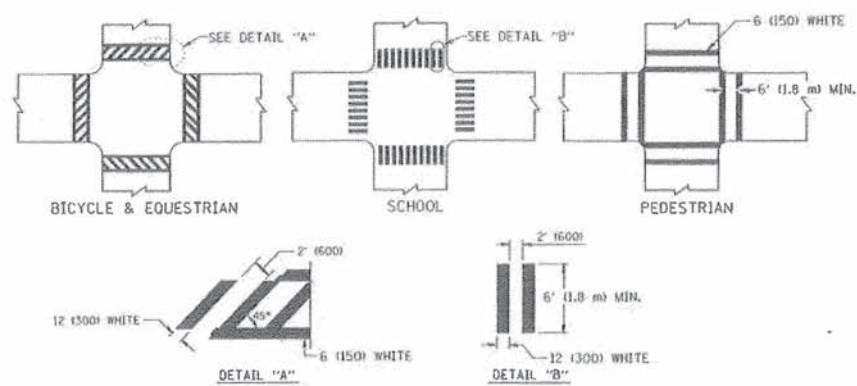
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

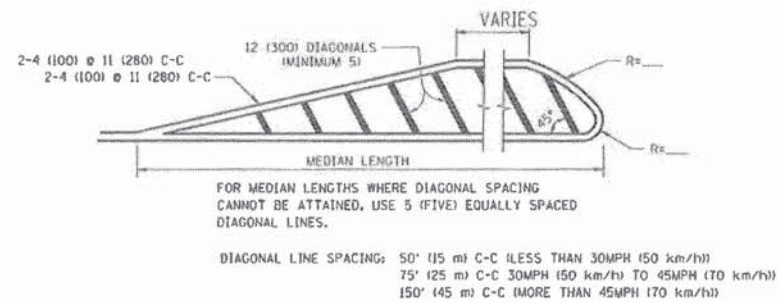
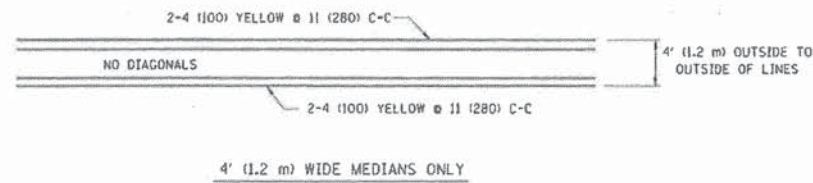
FILE NAME *	DESIGNED -	REVISED -	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\13-00085-00-RS\13-00085-00-RS.dgn	DRAWN -	REVISED -			4013	13-00085-00-RS	MCHENRY	26	23
	CHECKED -	REVISED -			CONTRACT NO. 63399			ILLINOIS FED. AID PROJECT	
	DATE -	REVISED -			SCALE: NONE	SHEET NO. 23 OF 26 SHEETS	STA.	TO STA.	



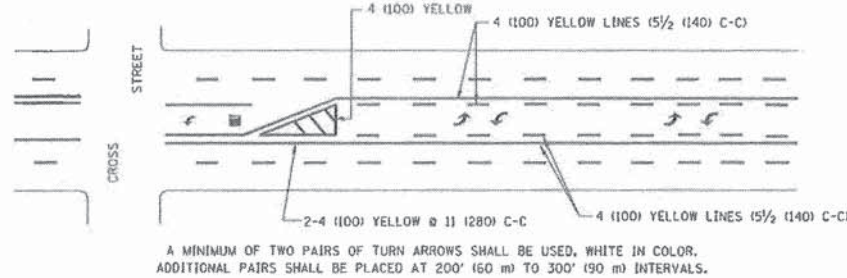
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

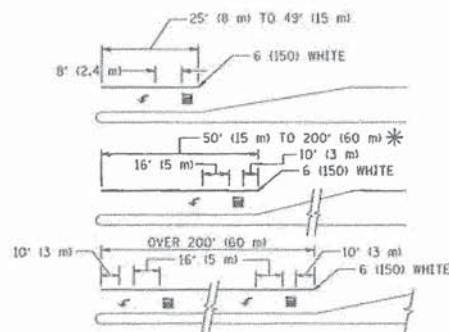


MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

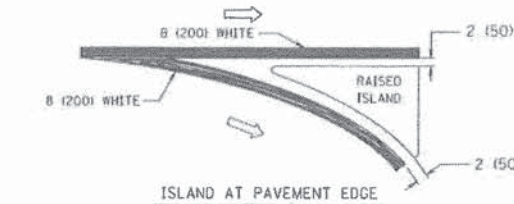
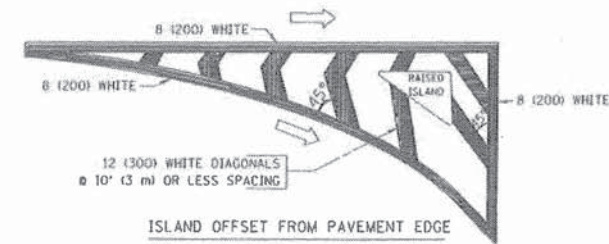


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
* AREA = 15.6 SQ. FT. (1.5 m²) □ AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES; FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW TWO WAY TRAFFIC WHITE ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

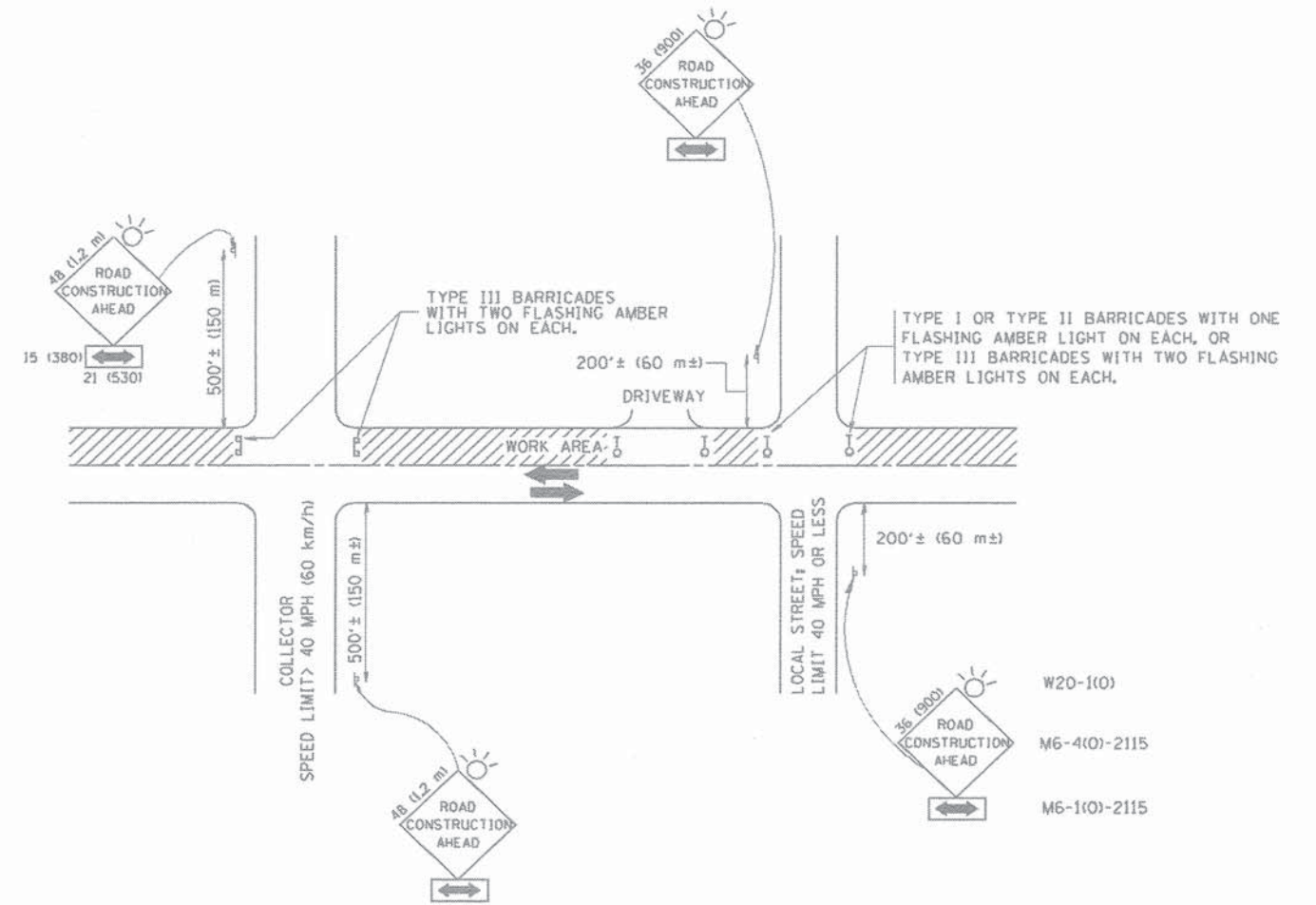
All dimensions are in Inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drivakosgn	DESIGNED = EVERS	REVISED = T. RAMMACHER 10-27-94
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	PLOT SCALE = 50.000 ' / IN.	CHECKED =	REVISED =
	PLOT DATE = 9/9/2009	DATE = 03-19-90	REVISED =

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

F.A. RTE. 4013	SECTION 13-00085-00-RS	COUNTY MCHENRY	TOTAL SHEETS 26	SHEET NO. 24
TC-13		CONTRACT NO. 63399		
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1,2 m x 1,2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (Inches) unless otherwise shown.

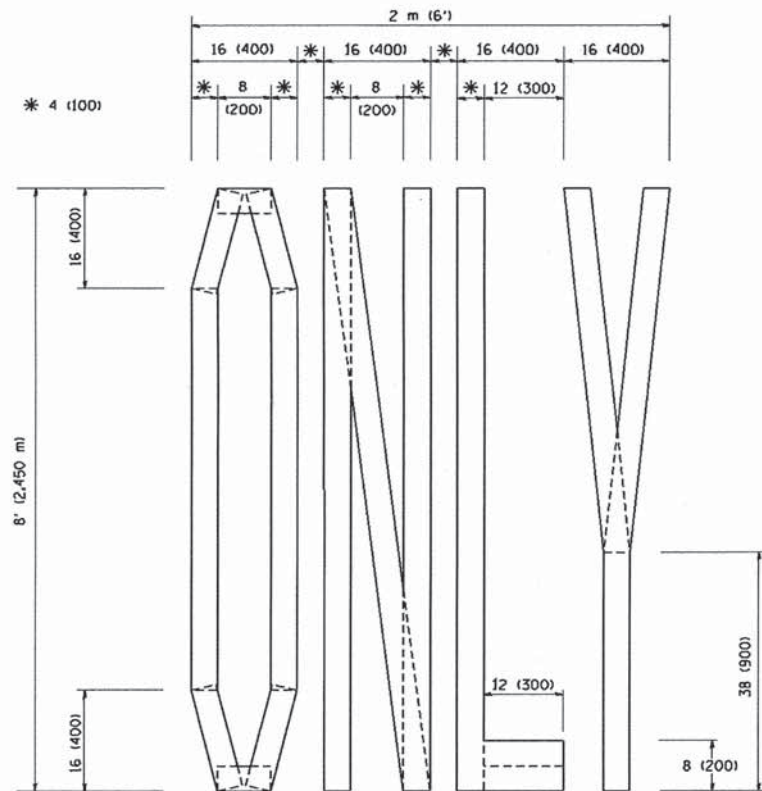
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

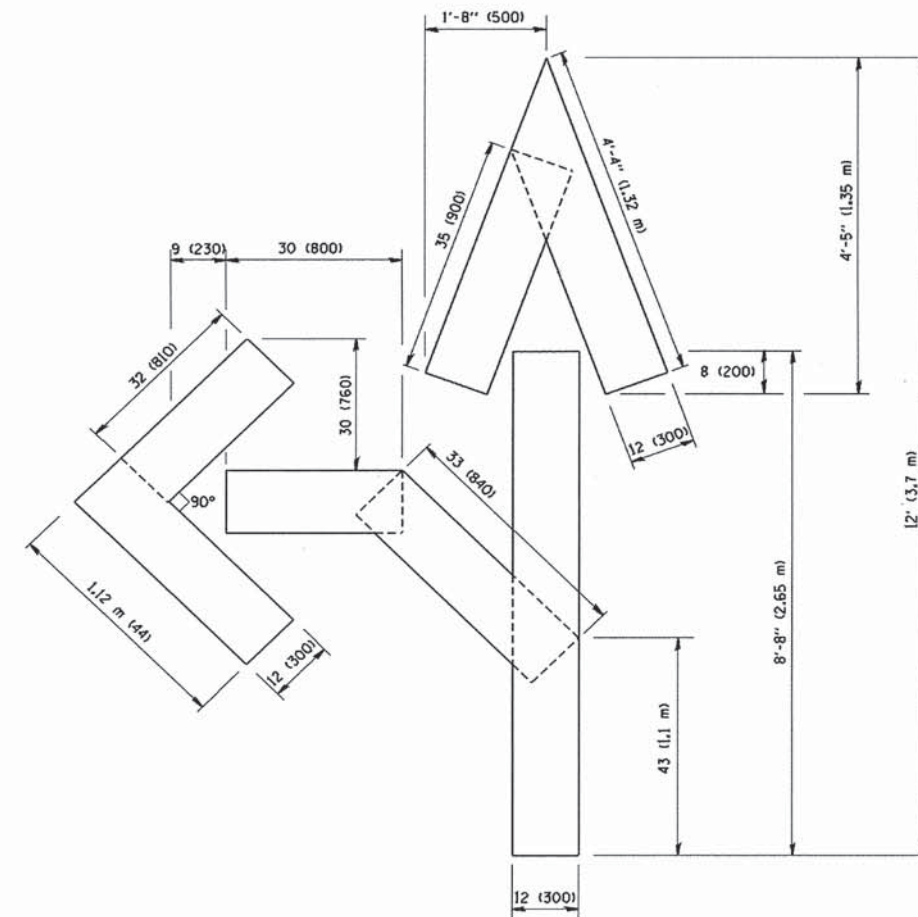
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

F.A. - RTE. 4013	SECTION 13-00085-00-RS	COUNTY MCHENRY	TOTAL SHEETS 26	SHEET NO. 25
TC-10			CONTRACT NO. 63399	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

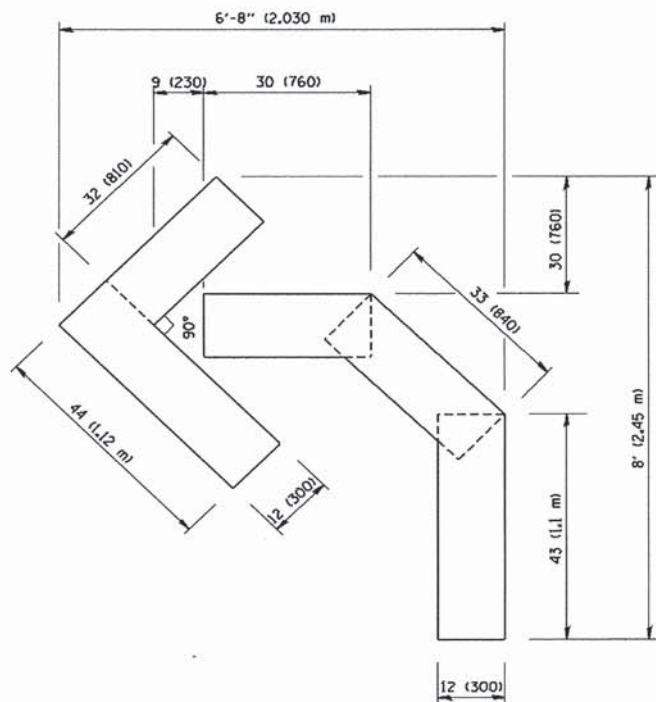
SCALE: NONE SHEET NO. 25 OF 26 SHEETS STA. TO STA.



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22\34\tcl6.dgn	USER NAME = gaglianob	DESIGNED - DRAWN -	REVISED -T. RAMMACHER 06-05-96 REVISED -T. RAMMACHER 11-04-97
PLOT SCALE = 50.0000 / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98	
PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING**

SCALE: NONE SHEET NO. 26 OF 26 SHEETS STA. TO STA.

F.A. - RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4013	13-00085-00-RS	MCHENRY	26	26
TC-16			CONTRACT NO. 63399	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				