

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

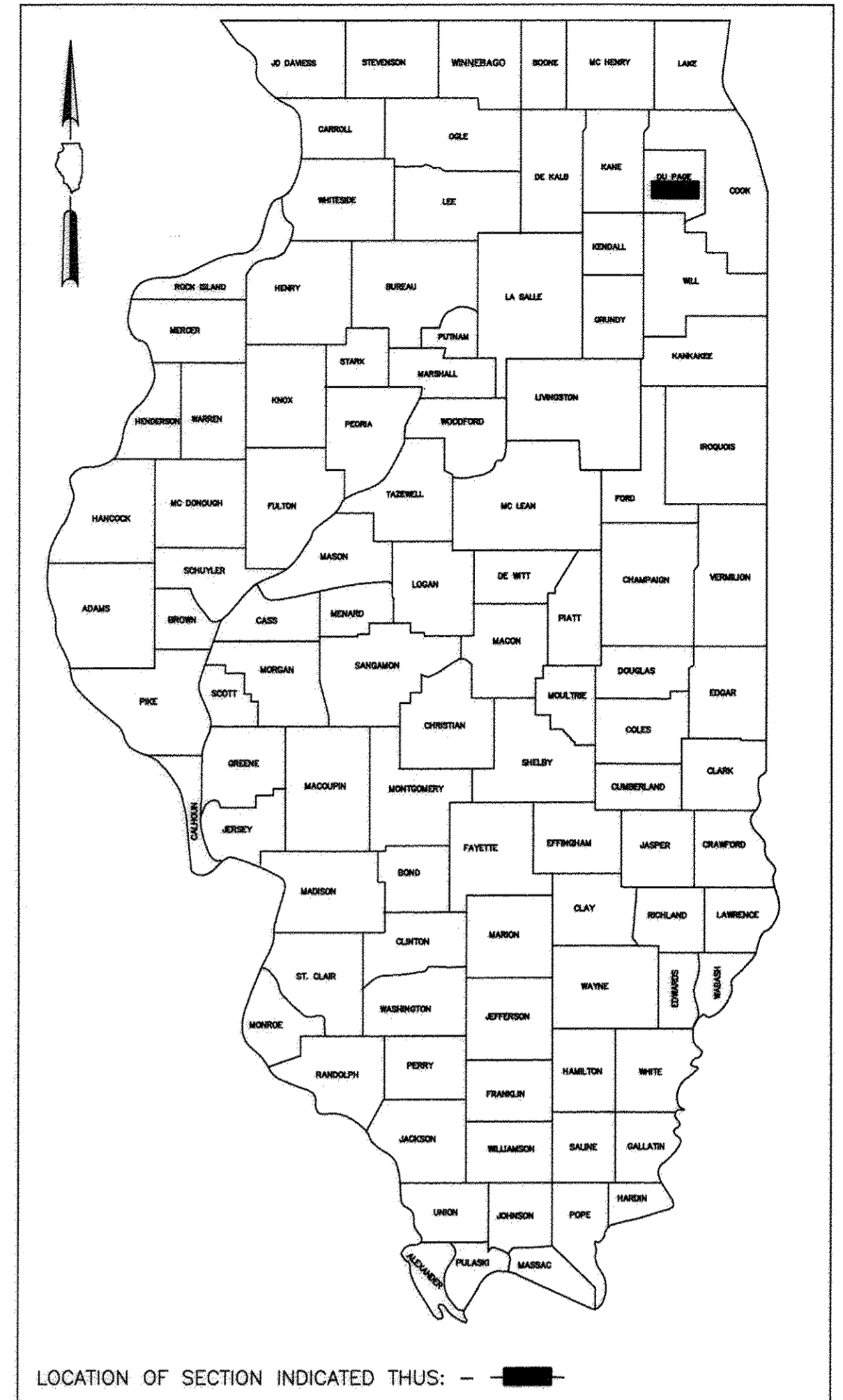
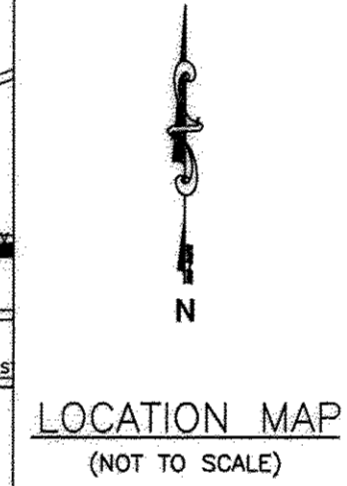
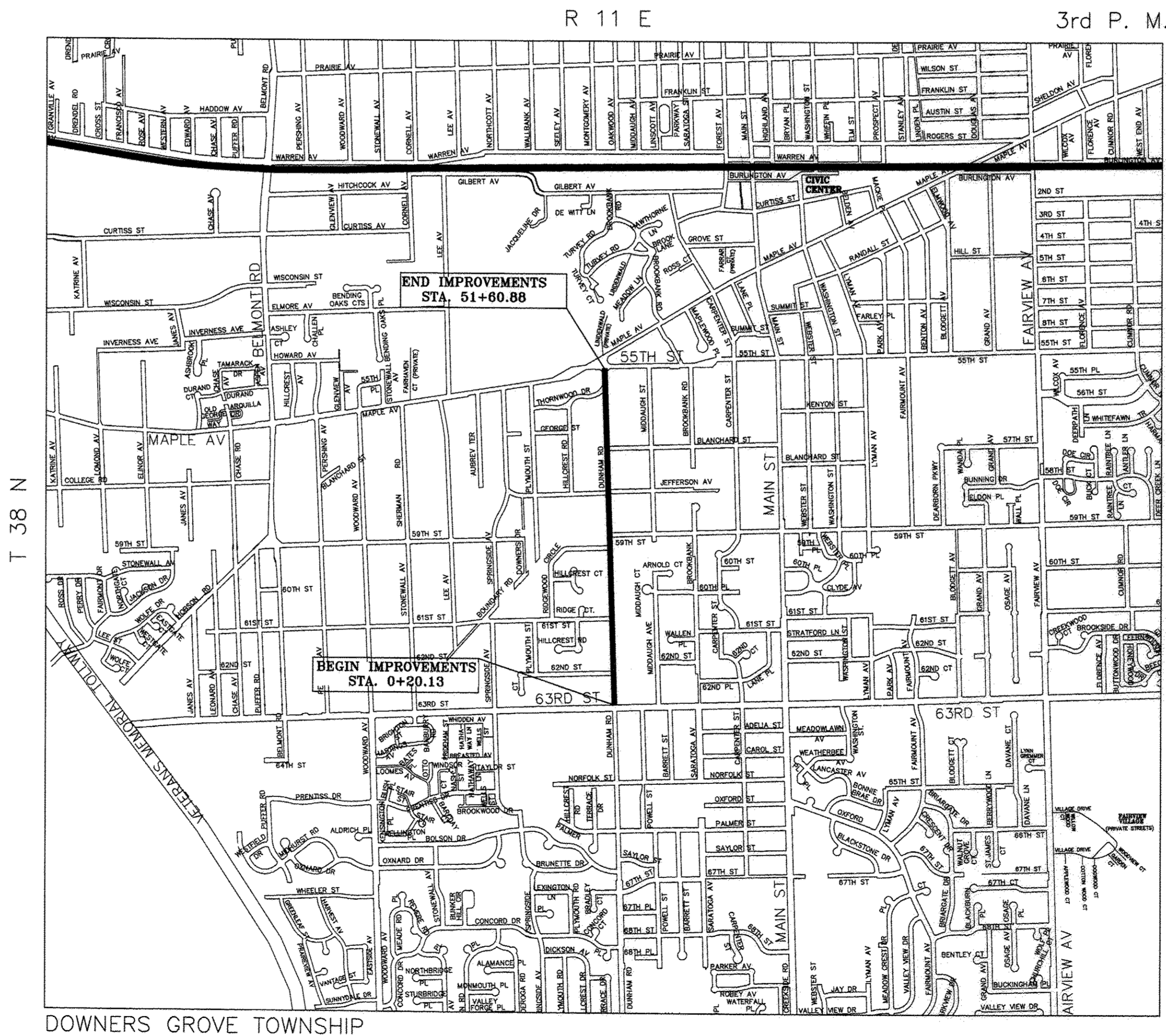
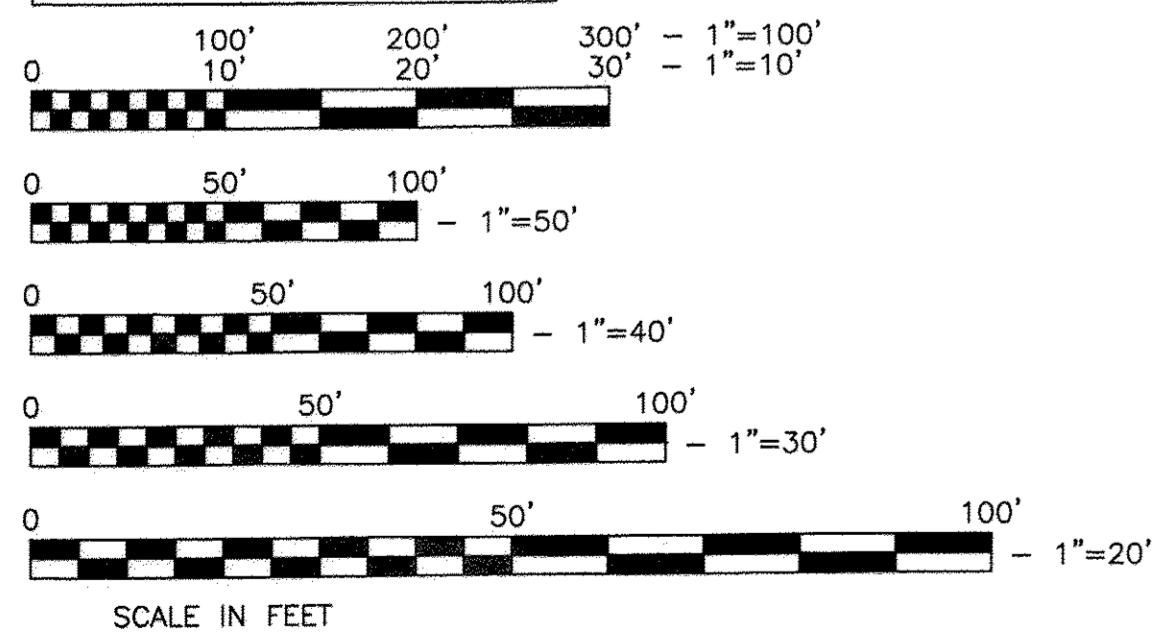
F. A. U. R.T.C.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2612	16-00110-00-RS	DU PAGE	21	1
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 61D55				

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
FAU ROUTE 2612 (DUNHAM ROAD)
FAU 1518 (63RD STREET) TO FAU 1504 (55TH STREET)
RESURFACING**
SECTION: 16-00110-00-RS
PROJECT: M-4003(848)
VILLAGE OF DOWNERS GROVE
DU PAGE COUNTY
C-91-110-17

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

DUNHAM ROAD DESIGN DESIGNATION
MAJOR COLLECTOR
DESIGN SPEED: 30 MPH
ADT: 7,800

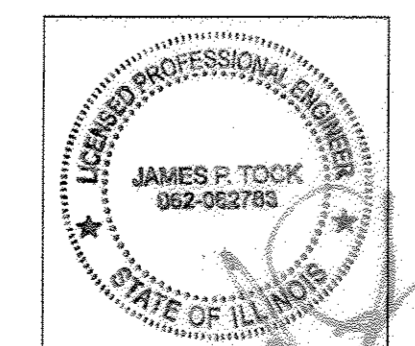


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED December 5 2016
Nancy O'Newlon
DIRECTOR OF PUBLIC WORKS, VILLAGE OF DOWNERS GROVE

PASSED December 19 2016
Chad Holt
DIVISION 4 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW December 20 2016
John Fortman
REGIONAL ENGINEER



James P. Tock, P.E.
#062-062783
My License Expires 11/30/17

GROSS LENGTH: 5,208 FT = 0.99 MILE
NET LENGTH: 5,141 FT = 0.97 MILE

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

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21	TS-07 DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

1. ALL REFERENCES TO THE 'VILLAGE' IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE VILLAGE OF DOWNERS GROVE.

2. ALL REFERENCES TO THE 'STANDARD SPECIFICATIONS' IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE 'STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION' ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) ON APRIL 1, 2016, ALONG WITH SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS AS ADOPTED JANUARY 1, 2017.

3. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE VILLAGE, DEPARTMENT, AND THE ENGINEERS DO NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING THE CONSTRUCTION OPERATION SO AS NOT TO DAMAGE THEM. IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 107.20 OF THE 'STANDARD SPECIFICATIONS' THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS.

4. THOSE EXISTING TRAFFIC SIGNS WHICH ARE SO DESIGNATED BY THE ENGINEER SHALL BE REMOVED, STORED AND SUBSEQUENTLY RELOCATED BY THE CONTRACTOR AT NO ADDITIONAL COST. ANY SIGNS WHICH ARE DAMAGED BY THE CONTRACTOR AS DETERMINED BY THE ENGINEER SHALL BE REPLACED IN KIND BY THE CONTRACTOR AND TO THE SATISFACTION OF THE ENGINEER.

5. ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE PLANS AND IDOT STANDARDS FOR TRAFFIC CONTROL AND PROTECTION.

6. SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING.

7. DEBRIS REMOVAL - MATERIALS RESULTING FROM THE VARIOUS CONSTRUCTION OPERATIONS SHALL BE REMOVED AT THE END OF EACH WORK DAY TO AN APPROVED SITE. IN THE JUDGEMENT OF THE ENGINEER, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE ENGINEER WILL REMOVE MATERIALS AND THE CONTRACTOR SHALL BE BILLED ACCORDINGLY.

8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THE WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

9. WHENEVER, DURING CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS.

10. THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS WITHOUT PRIOR AUTHORIZATION FROM THE VILLAGE WATER DEPARTMENT.

11. QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL VERIFY FINAL PATCH LOCATIONS IN THE FIELD PRIOR TO REMOVAL.

12. HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

13. THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NORMAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASE ON WHICH THEY ARE PLACED. PLAN THICKNESS SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

14. MAILBOXES WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED, TEMPORARILY RELOCATED, AND REPLACED UPON COMPLETION OF THE PROPOSED IMPROVEMENTS IN ACCORDANCE WITH ARTICLE 107.20 AND AS DIRECTED BY THE ENGINEER.

15. THE CONTRACTOR SHALL NOT PLACE SOD UNTIL THE TEMPERATURE IS 80 DEGREES OR LESS AND THE FORECAST FOR THE NEXT 7 DAYS SHOWS TEMPERATURES OF 80 DEGREES OR LESS. IF ALL OTHER PAY ITEMS ARE COMPLETED THE CONTRACTOR WILL NOT BE CHARGED WORKING DAYS FOR DELAYS IN PARKWAY RESTORATION DUE TO TEMPERATURE.

16. SPECIAL ATTENTION IS CALLED TO ARTICLE 107.30 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DEFACEMENT OF ANY CONCRETE POURS BEFORE THEY HAVE SET UP. CONCRETE SIDEWALK, DRIVEWAY, CURB, AND CURB AND GUTTER THAT HAVE BEEN DEFACED, IN THE OPINION OF THE ENGINEER, SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR AT HIS EXPENSE.

17. FOR WORK OUTSIDE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.

DUPAGE COUNTY DIVISION OF TRANSPORTATION GENERAL NOTES AND SPECIFICATIONS

1. ALL CONSTRUCTION WITHIN THE COUNTY'S RIGHT-OF-WAY SHALL BE PERFORMED ACCORDING TO IDOT'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (LATEST EDITION) AND THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" (LATEST EDITION).

2. DAILY LANE CLOSURES ARE PERMITTED BETWEEN 9:00 A.M. AND 4:00 P.M. ONLY. TRAFFIC CONTROL SHALL CONFORM TO IDOT'S HIGHWAY STANDARDS THE FHWA'S 'MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES' AND IDOT'S SUPPLEMENT TO THE MUTCD AT ALL TIMES DURING CONSTRUCTION.

3. LANE CLOSURES ARE NOT PERMITTED ON COUNTY ROADWAYS DURING SNOWFALL OR WITHIN 2 HOURS PRIOR TO PREDICTED SNOWFALL OR PRECIPITATION CONDITIONS BETWEEN NOVEMBER 15 AND APRIL 15 FOR MAINTENANCE OF THE ROADWAY PAVEMENT BY COUNTY HIGHWAY MAINTENANCE DEPARTMENT STAFF AND EQUIPMENT.

4. DISTURBED AREAS OF THE RIGHT-OF-WAY SHALL BE DRESSED WITH A MINIMUM OF 6" TOPSOIL AND CLASS 2A SALT TOLERANT SEED (WITH EROSION CONTROL BLANKET) OR SOD (SALT TOLERANT AND STAKED IN PLACE).

5. THE DUPAGE COUNTY DIVISION OF TRANSPORTATION OPERATES/MAINTAINS TRAFFIC SIGNALS AND RELATED EQUIPMENT WITHIN THE VICINITY OF THE PROJECT. CONTACT THE DIVISION OF TRANSPORTATION A MINIMUM OF 48 HOURS PRIOR TO THE START OF CONSTRUCTION WITHIN THE COUNTY'S RIGHT OF WAY AND WITHIN 300' OF ANY COUNTY MAINTAINED SIGNAL TO LOCATE SAID EQUIPMENT. TRAFFIC SIGNALS AND RELATED EQUIPMENT ARE NOT ON THE J.U.L.I.E. SYSTEM.

6. EROSION CONTROL MEASURES SHALL COMPLY WITH THE MINIMUM REQUIREMENTS OF THE DUPAGE COUNTY STORMWATER AND FLOODPLAIN ORDINANCE SPECIFICATIONS AT ALL TIMES.

7. EQUIPMENT AND MATERIALS SHALL NOT BE STORED WITHIN THE COUNTY'S RIGHT-OF-WAY AT ANY TIME WITHOUT THE EXPRESS WRITTEN PERMISSION OF THE COUNTY ENGINEER, OR HIS DULY AUTHORIZED ASSIGN.

8. PAVEMENT, CURB/GUTTER AND STORM STRUCTURES WITHIN THE COUNTY'S RIGHT-OF-WAY SHALL BE MAINTAINED FREE OF MUD/DEBRIS AT ALL TIMES AND SHALL BE CLEANED AS IS REQUIRED AND/OR AS DIRECTED BY DUPAGE COUNTY.

9. CONTACT DUPAGE COUNTY (630/407-6900) A MINIMUM OF 48 HOURS PRIOR TO THE START OF CONSTRUCTION TO ARRANGE FOR INSPECTIONS OF AND AT THE COMPLETION OF THE DESCRIBED WORK WITHIN THE COUNTY'S RIGHT-OF-WAY.

10. TRENCH BACKFILL FOR NON-PAVED AREAS SHALL BE INSTALLED WITHIN THE COUNTY'S RIGHT-OF-WAY PER DUPAGE COUNTY'S STANDARD.

11. TRENCH BACKFILL BELOW EXISTING OR PROPOSED PAVEMENT, CURB/GUTTER AND/OR SIDEWALK SHALL BE INSTALLED WITHIN THE COUNTY'S RIGHT OF WAY PER DUPAGE COUNTY'S STANDARD.

IDOT STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-09	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424016-03	MID-BLOCK CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C & D PATCHES
606001-06	CONCRETE CURB TYPE B & COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-06	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS

FILE NAME =	USER NAME -- USER	DESIGNED -- NRH	REVISED	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DUNHAM ROAD IMPROVEMENTS GENERAL NOTES AND INDEX OF STANDARDS	F.A.U. #	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
FILE NAME =		DRAWN -- NRH	REVISED			2612	16-00110-00-RS	DU PAGE	21	2	
	PLOT SCALE --	CHECKED -- JPT	REVISED			CONTRACT NO. 61D55					
	PLOT DATE -- 09/30/16	DATE -- 09/30/16	REVISED			NOT TO SCALE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS

SUMMARY OF QUANTITIES

DUNHAM ROAD				
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE 0005
20200100	EARTH EXCAVATION	CU YD	45	45
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	400	400
25200110	SODDING, SALT TOLERANT	SQ YD	400	400
25200200	SUPPLEMENTAL WATERING	UNIT	5	5
28000510	INLET FILTERS	EACH	49	49
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	16,500	16,500
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	786	786
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	225	225
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1,570	1,570
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT 6 INCH	SQ YD	35	35
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2,800	2,800
42400800	DETECTABLE WARNINGS	SQ FT	190	190
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	17,950	17,950
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	60	60
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	200	200
44000600	SIDEWALK REMOVAL	SQ FT	2,800	2,800
44201690	CLASS D PATCHES, TYPE I, 4 INCH	SQ YD	200	200
44201692	CLASS D PATCHES, TYPE II, 4 INCH	SQ YD	200	200
44201694	CLASS D PATCHES, TYPE III, 4 INCH	SQ YD	200	200
44201696	CLASS D PATCHES, TYPE IV, 4 INCH	SQ YD	200	200
48101498	AGGREGATE SHOULDERS, TYPE B 4"	SQ YD	1,400	1,400
60266600	VALVE BOXES TO BE ADJUSTED	EACH	3	3
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	20	20
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	1	1
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	1	1
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	600	600

DUNHAM ROAD				
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE 0005
67100100	MOBILIZATION	LSUM	1	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LSUM	1	1
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1
70300100	SHORT TERM PAVEMENT MARKING	FOOT	4,190	4,190
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1,615	1,615
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	220	220
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	75	75
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	9,500	9,500
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	240	240
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	430	430
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	150	150
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	500	500
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	LSUM	1	1
X2800510	INLET FILTER CLEANING	EACH	49	49
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	19	19
X6061005	CONCRETE CURB TYPE B (SPECIAL)	FOOT	50	50
XX008693	HOT MIX ASPHALT SIDEWALK	SQ YD	18	18
Z0004510	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	25	25
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	52	52

* DENOTES SPECIALTY ITEM

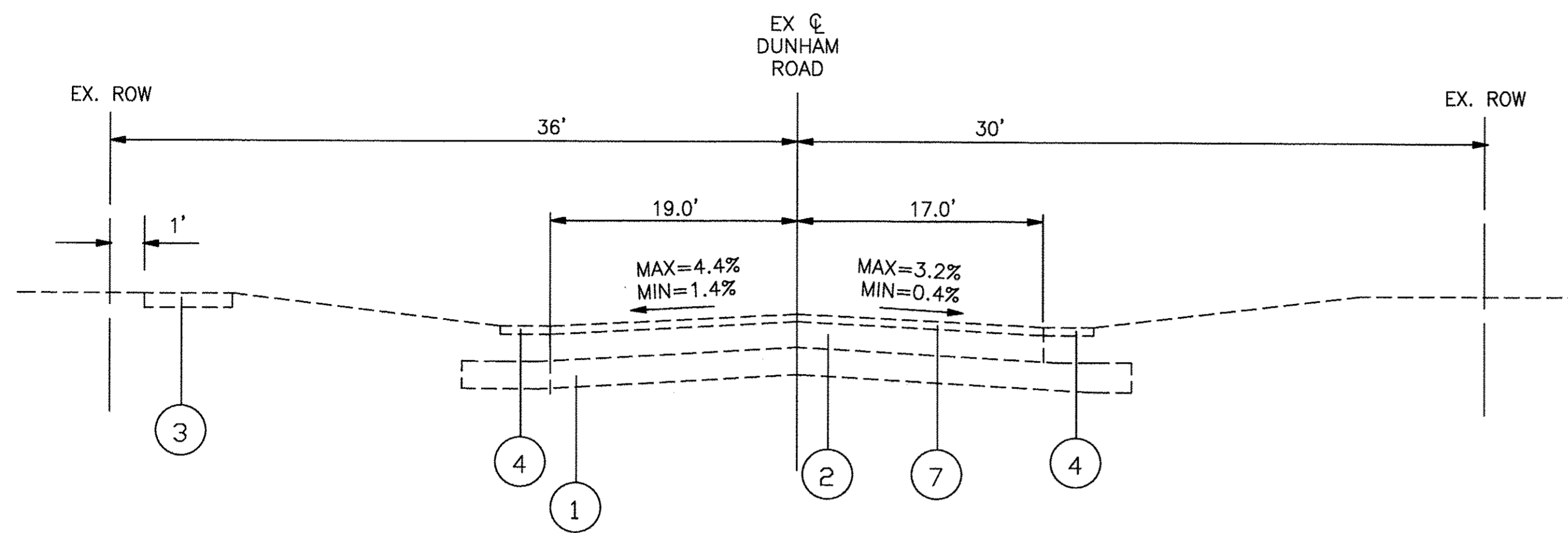
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	PLOT DATE -- 09/30/16	DATE -- 09/30/16	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNHAM ROAD IMPROVEMENTS
SUMMARY OF QUANTITIES

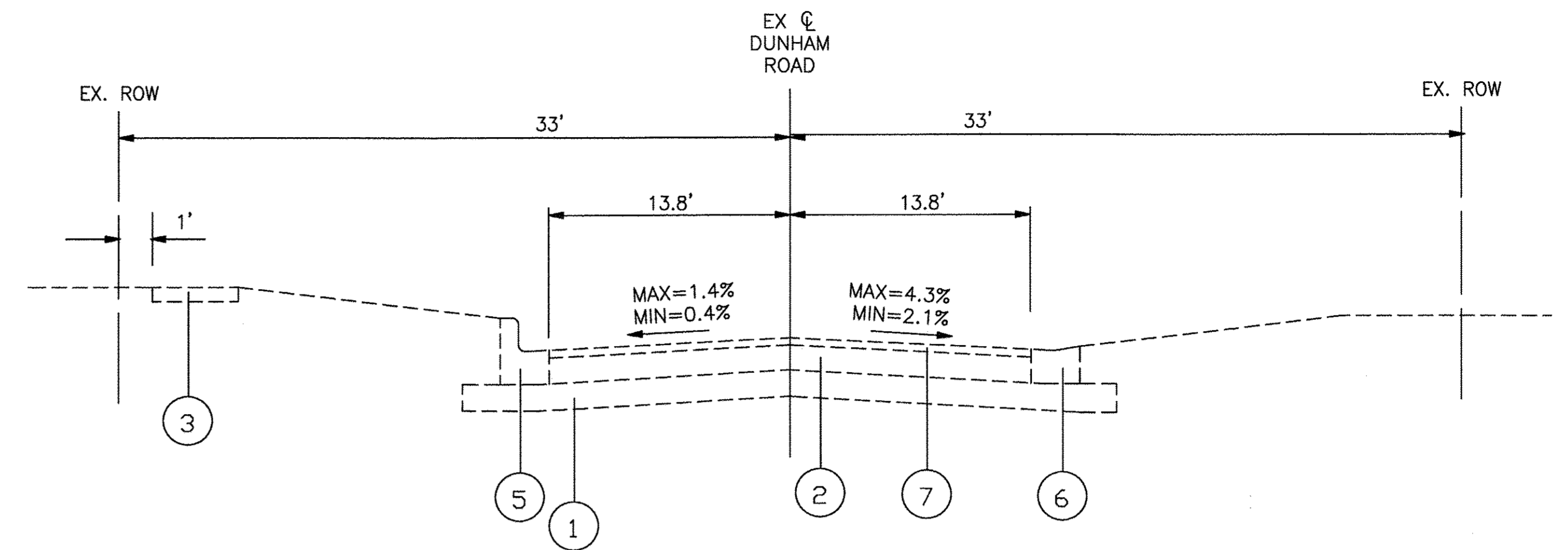
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2612	16-00110-00-RS	DU PAGE	21	3
CONTRACT NO. 61D55				
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

NOT TO SCALE SHEET NO. 1 OF 1 SHEETS STA. TO STA.



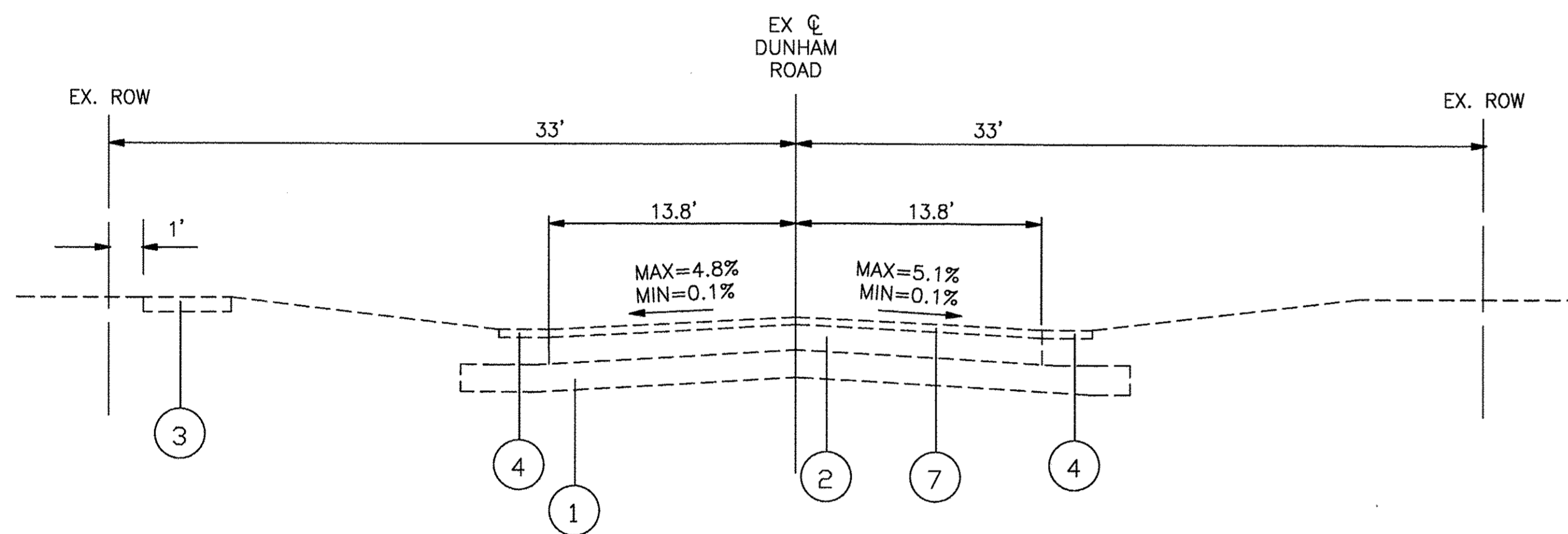
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STA. 0+20 TO STA. 1+41, DUNHAM ROAD
 STA. 1+41 TO STA. 2+39, DUNHAM ROAD TAPER SECTION



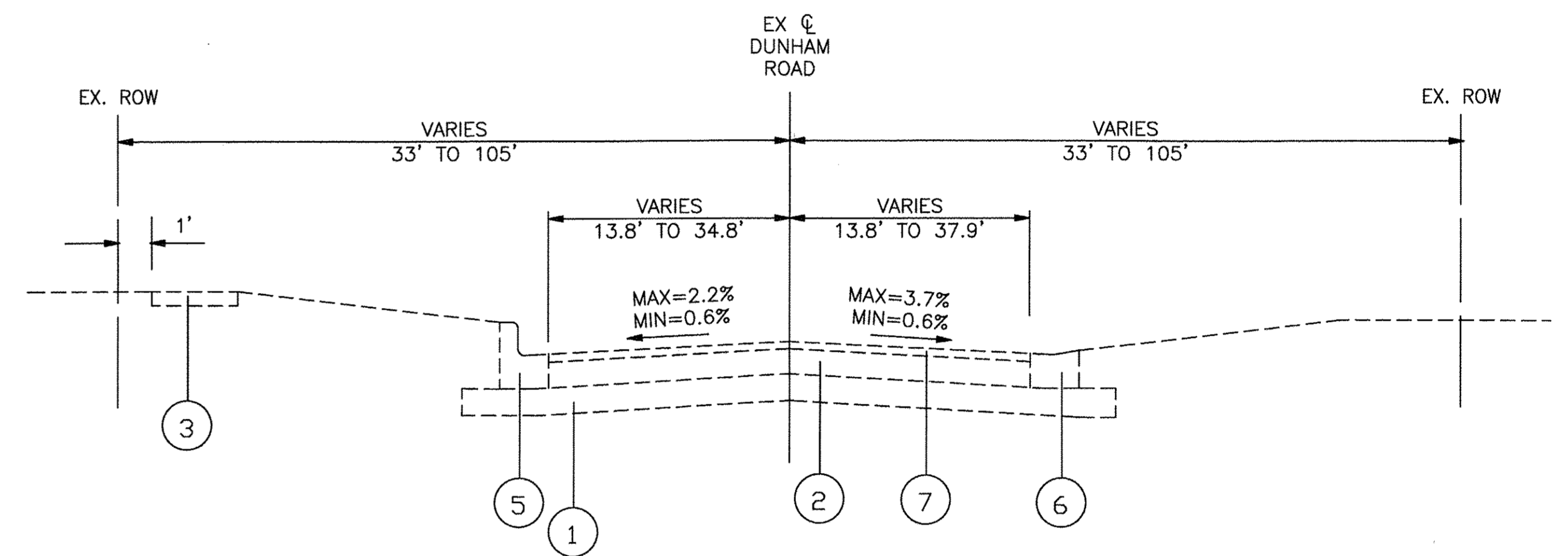
EXISTING TYPICAL SECTION

STA. 44+94 TO STA. 50+21, DUNHAM ROAD



EXISTING TYPICAL SECTION

STA. 2+39 TO STA. 44+94, DUNHAM ROAD



EXISTING TYPICAL SECTION

STA. 50+21 TO STA. 51+61, DUNHAM ROAD

LEGEND

- ① EXISTING SUBGRADE
- ② EXISTING BITUMINOUS PAVEMENT, 4" - 7", VARIES
- ③ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- ④ EXISTING AGGREGATE SHOULDER, TYPE B REMOVAL WHERE SHOWN ON PLANS
- ⑤ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B6.12 REMOVAL WHERE SHOWN ON PLANS
- ⑥ EXISTING CONCRETE CURB & GUTTER, TYPE B6.12 (DEPRESSED) REMOVAL WHERE SHOWN ON PLANS
- ⑦ PROPOSED HMA SURFACE REMOVAL, 2"

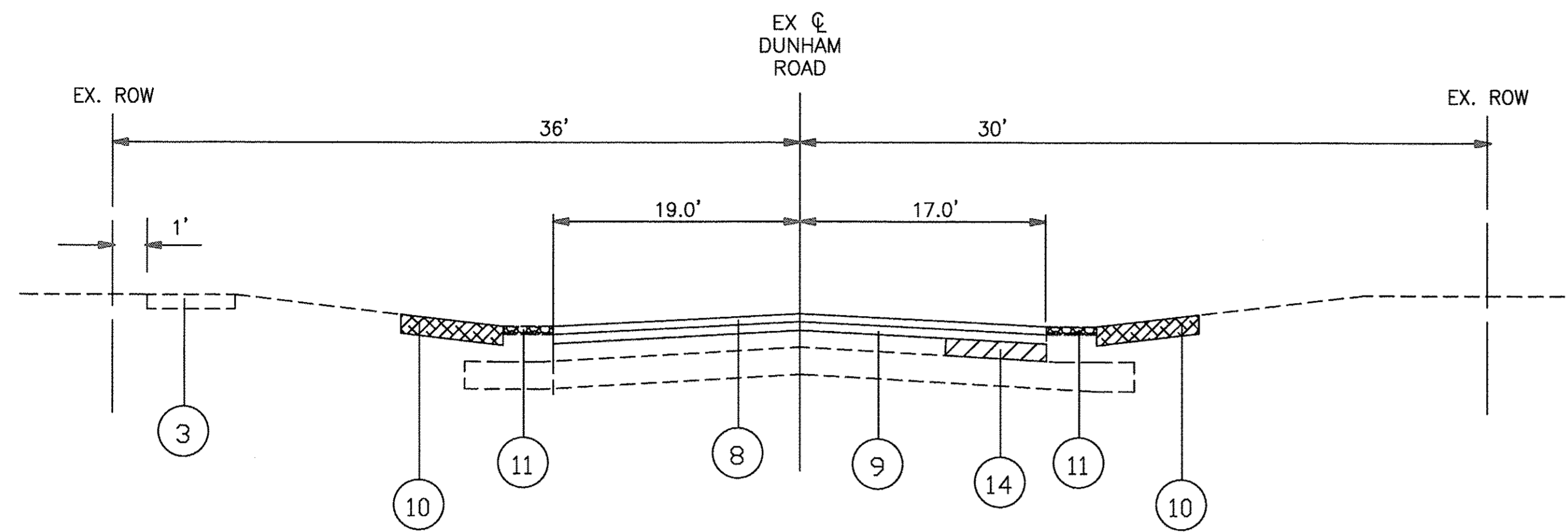
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DUNHAM ROAD IMPROVEMENTS
 EXISTING TYPICAL SECTIONS

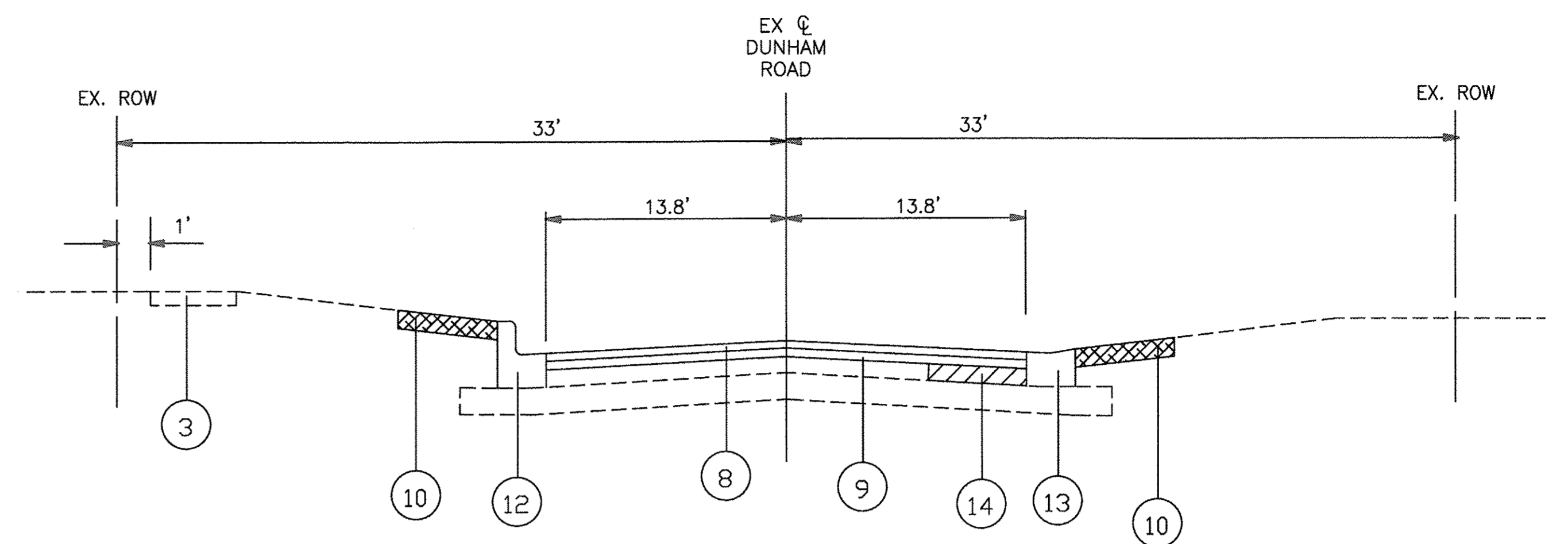
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F. A. U. RLE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2612	16-00110-00-RS	DU PAGE	21	4
CONTRACT NO. 61055				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



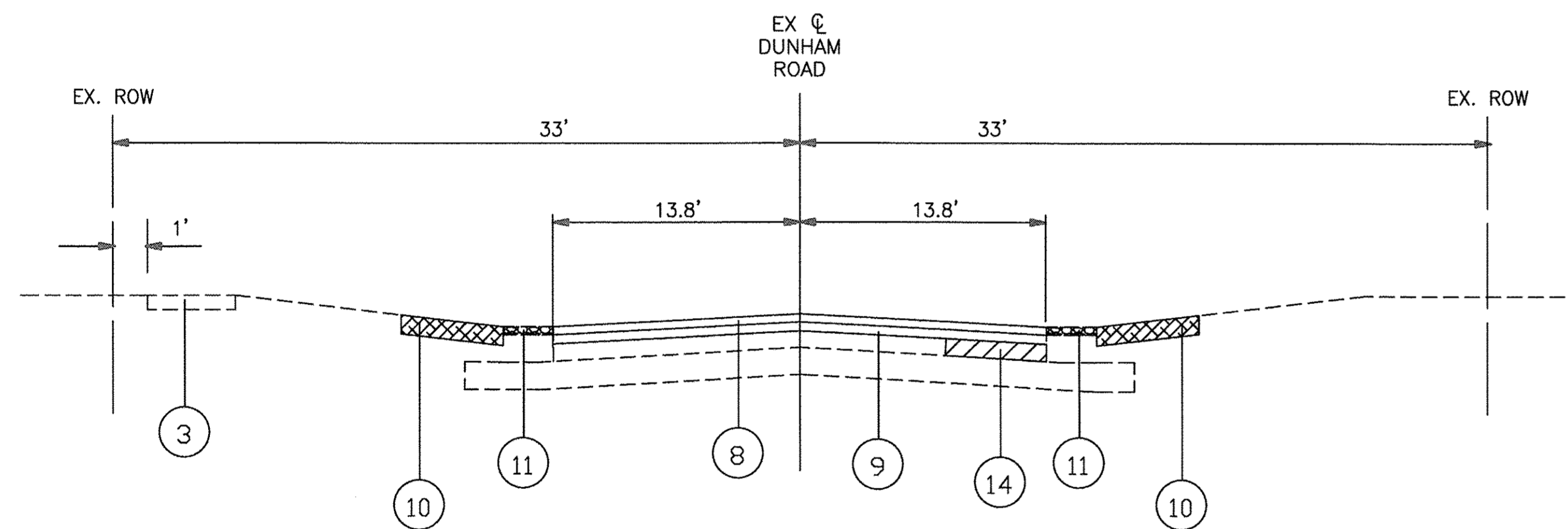
PROPOSED TYPICAL SECTION

STA. 0+20 TO STA. 1+41, DUNHAM ROAD
 STA. 1+41 TO STA. 2+39, DUNHAM ROAD TAPER SECTION



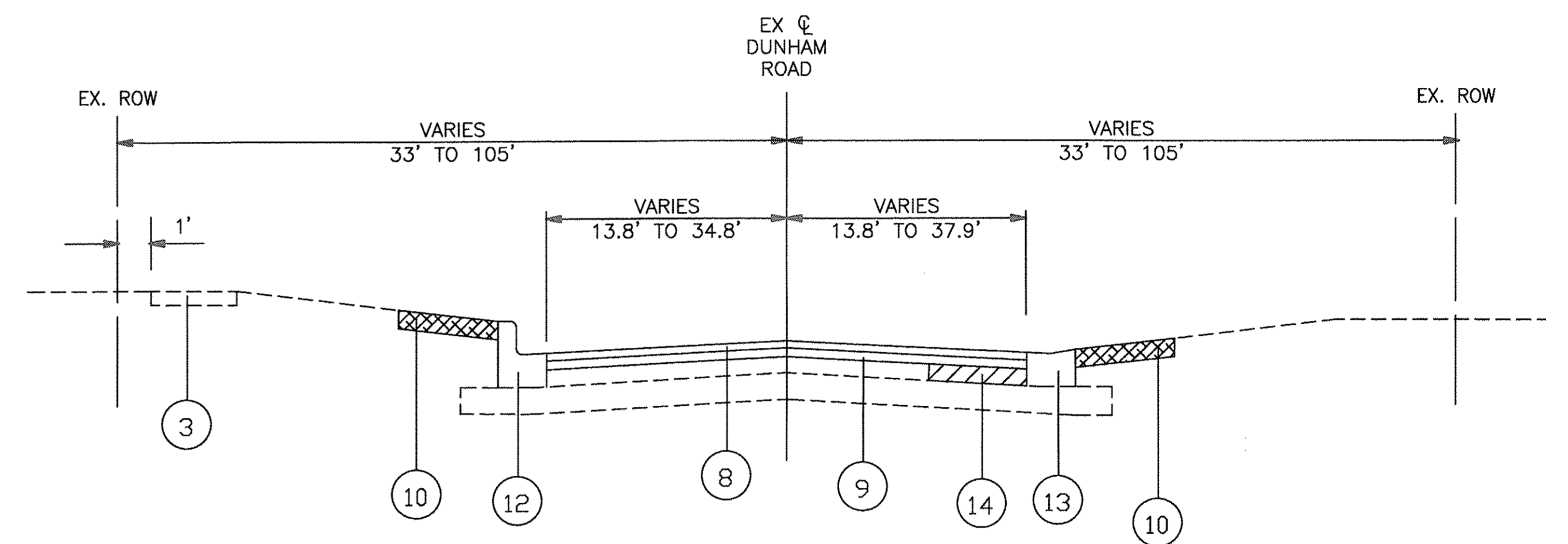
PROPOSED TYPICAL SECTION

STA. 44+94 TO STA. 50+21, DUNHAM ROAD



PROPOSED TYPICAL SECTION

STA. 2+39 TO STA. 44+94, DUNHAM ROAD



PROPOSED TYPICAL SECTION

STA. 50+21 TO STA. 51+61, DUNHAM ROAD TAPER SECTION

LEGEND

- ⑧ PROPOSED HMA SURFACE COURSE, MIX "D", N50, 1 1/2"
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑩ PROPOSED SODDING, SALT TOLERANT & TOP SOIL FURNISH AND PLACE, 4" (LOCATIONS DETERMINED BY ENGINEER)
- ⑪ PROPOSED AGGREGATE SHOULDER, TYPE B REPLACEMENT WHERE SHOWN ON PLANS
- ⑫ PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B6.12 REPLACEMENT WHERE SHOWN ON PLANS
- ⑬ PROPOSED CONCRETE CURB & GUTTER, TYPBE B6.12 (DEPRESSED) REPLACEMENT WHERE SHOWN ON PLANS
- ⑭ CLASS D PATCH (LOCATION AND DIMENSIONS DETERMINED BY ENGINEER), 4"

CONTRACTOR SHALL MILL FIRST BEFORE PATCHING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 1 1/2"	4% @ 50 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	3.5% @ 50 GYR
DRIVEWAY: HOT MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 3"	4% @ 50 GYR
SIDEWALK: HOT MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 3"	4% @ 50 GYR
CLASS D PATCHES, (HMA BINDER) IL-19mm, N70, 4" (IN 2 LIFTS)	4% @ 70 GYR

-THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.

-THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 - 22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 - 22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

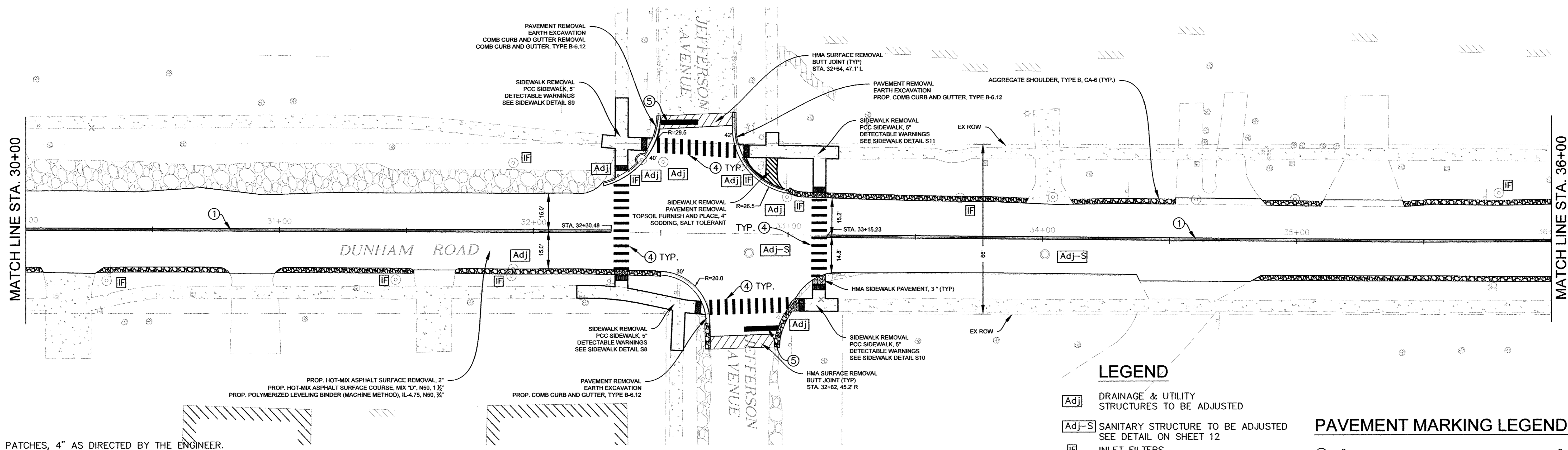
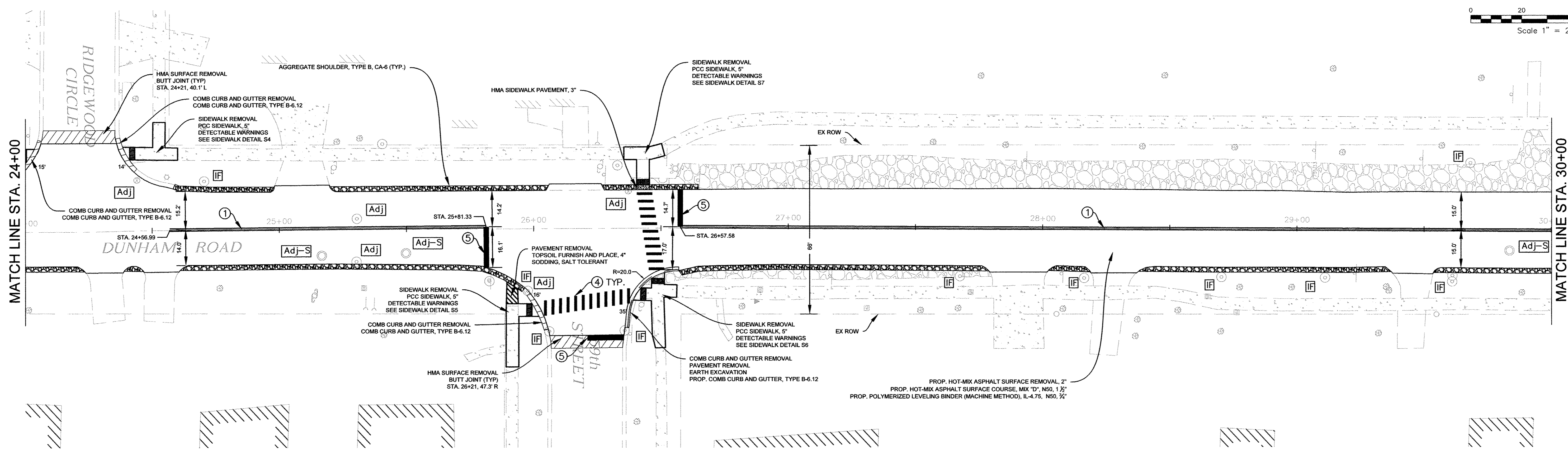
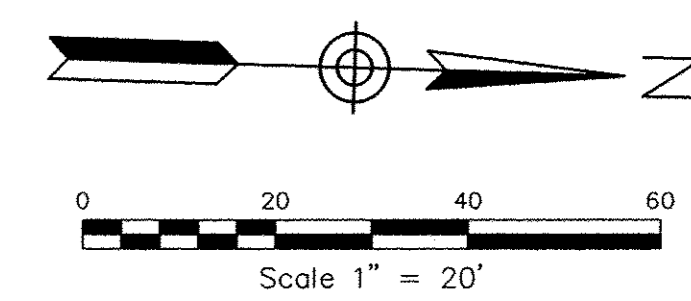
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DUNHAM ROAD IMPROVEMENTS
 PROPOSED TYPICAL SECTIONS

F. A. U. R/E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2612	16-00110-00-RS	DU PAGE	21	5
CONTRACT NO. 61055				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

NOT TO SCALE SHEET NO. 1 OF 1 SHEETS STA. TO STA.



NOTES

1. CLASS D PATCHES, 4" AS DIRECTED BY THE ENGINEER.
2. ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ADJUSTMENT ITEM.
3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.
4. ALL CURB AND GUTTER TO MATCH EXISTING EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.

LEGEND

- [Adj] DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
- [Adj-S] SANITARY STRUCTURE TO BE ADJUSTED SEE DETAIL ON SHEET 12
- [IF] INLET FILTERS
- [Hatched] HMA SIDEWALK PAVEMENT, 3"
- [Dotted] AGGREGATE SHOULDERS, TYPE B
- [Diagonal Lines] REMOVE AND REPLACE EXISTING CURB AND GUTTER
- [Double Line] PROPOSED COMB C&G B6.12
- [Dashed Line] PROPOSED COMB C&G B6.12 (DEPRESSED)

PAVEMENT MARKING LEGEND

- ① 4" DOUBLE YELLOW THERMOPLASTIC LINE @ 11" C-C - SOLID
- ② 6" WHITE THERMOPLASTIC LINE - SKIP DASH
- ③ 6" WHITE THERMOPLASTIC LINE - SOLID
- ④ 12" WHITE THERMOPLASTIC LINE - SOLID
- ⑤ 24" WHITE THERMOPLASTIC LINE - SOLID
- ⑥ LETTERS AND SYMBOLS - FULL SIZE

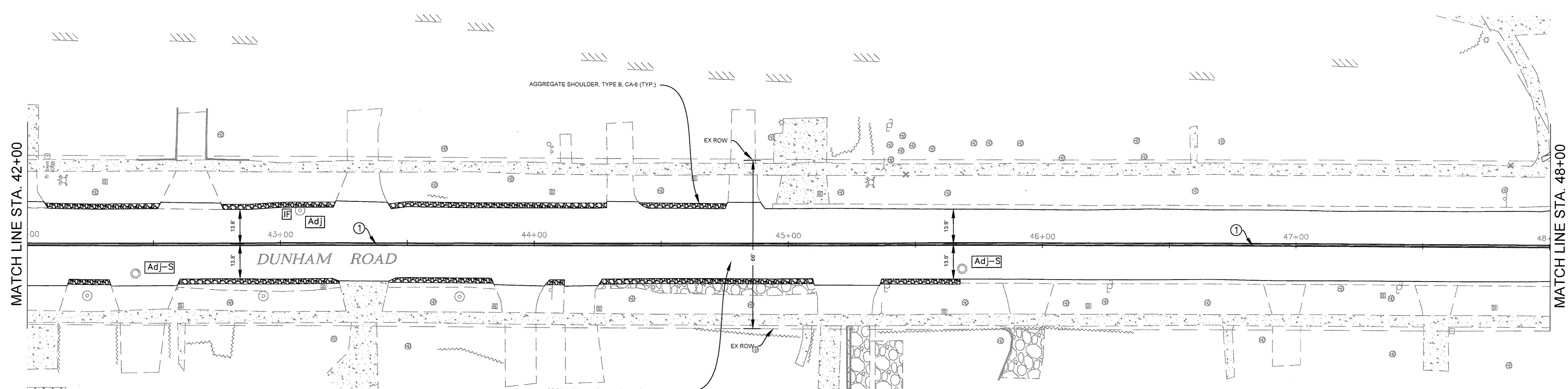
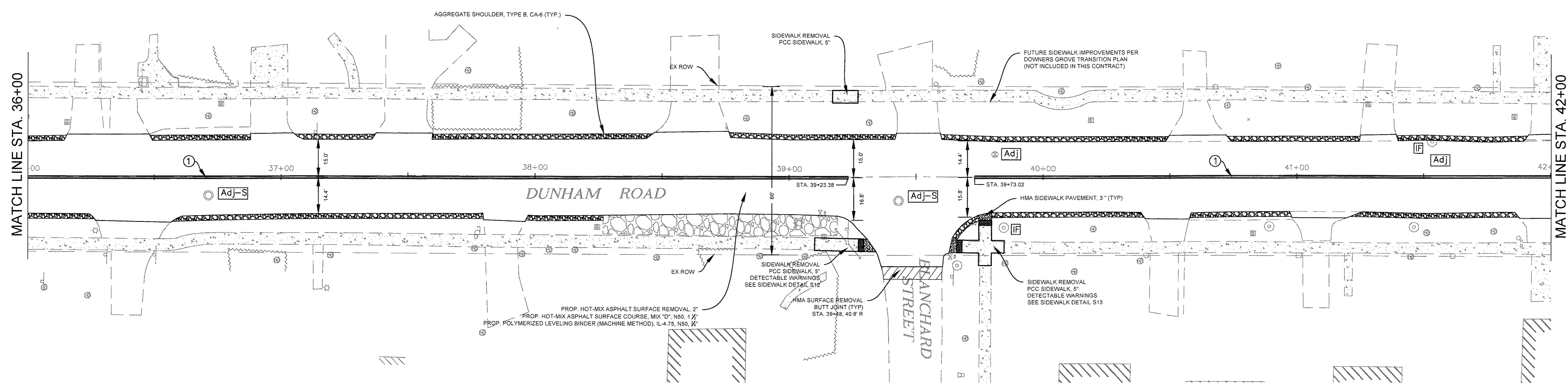
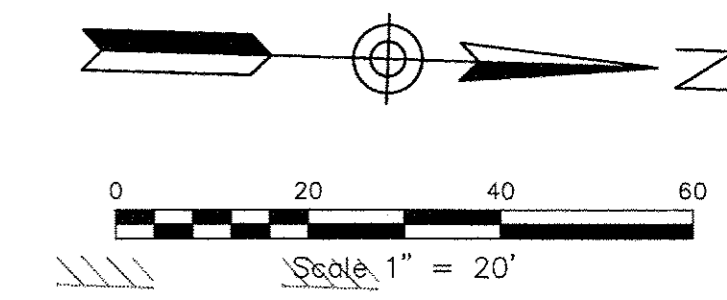
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNHAM ROAD IMPROVEMENTS
IMPROVEMENT PLAN

SCALE: 1"=20' SHEET NO. 3 OF 5 SHEETS STA. 24+00 TO STA. 36+00

F.A.I. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2612	16-00110-00-RS	DU PAGE	21	8
CONTRACT NO. 61D55				
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



NOTES

1. CLASS D PATCHES, 4' AS DIRECTED BY THE ENGINEER.
2. ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ADJUSTMENT ITEM.
3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.
4. ALL CURB AND GUTTER TO MATCH EXISTING EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.

LEGEND

- Adj DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
- Adj-S SANITARY STRUCTURE TO BE ADJUSTED SEE DETAIL ON SHEET 12
- IF INLET FILTERS
- HMA SIDEWALK PAVEMENT, 3"
- AGGREGATE SHOULDERS, TYPE B

PAVEMENT MARKING LEGEND

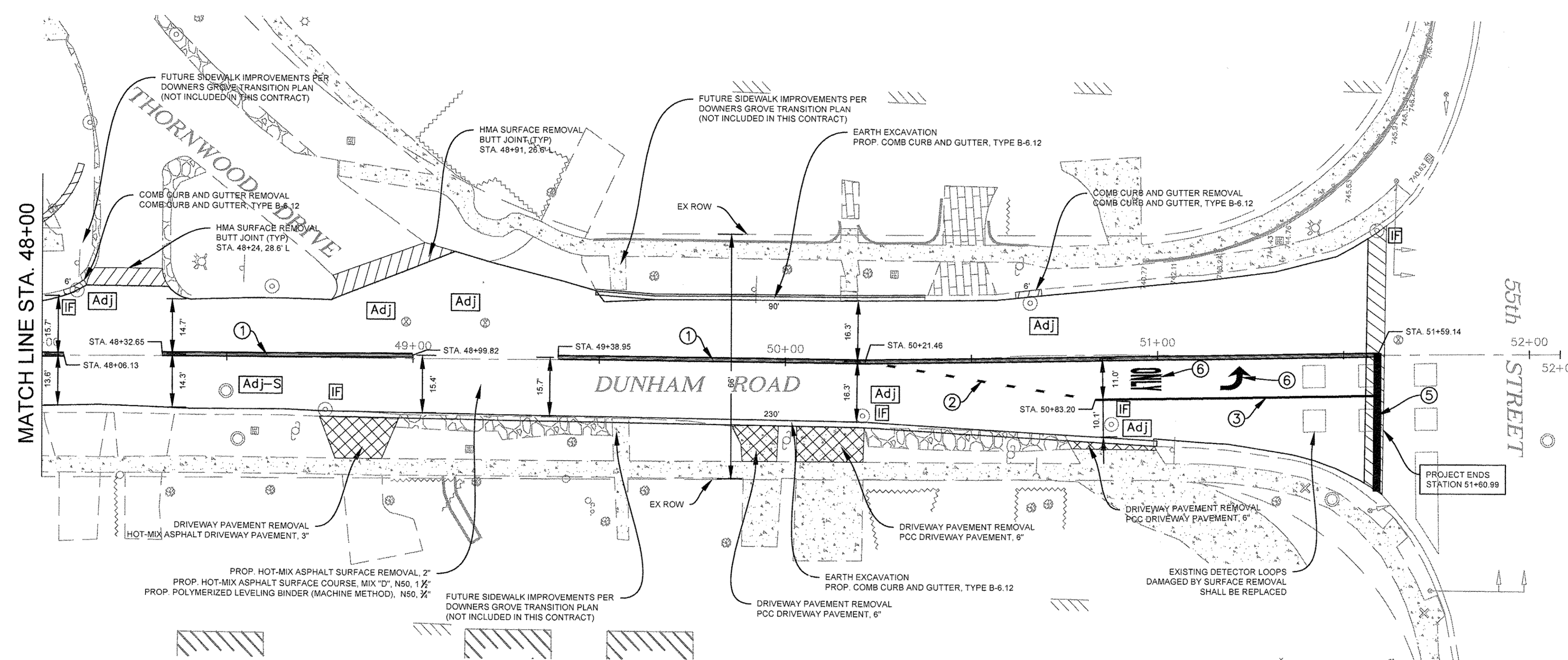
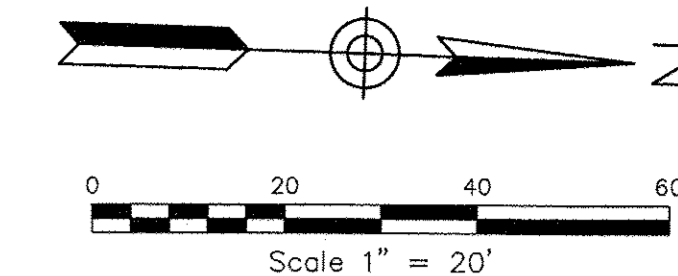
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- ② 6" WHITE THERMOPLASTIC LINE - SKIP DASH
- ③ 6" WHITE THERMOPLASTIC LINE - SOLID
- ④ 12" WHITE THERMOPLASTIC LINE - SOLID
- ⑤ 24" WHITE THERMOPLASTIC LINE - SOLID
- ⑥ LETTERS AND SYMBOLS - FULL SIZE

FILE NAME =	USER NAME - USER	DESIGNED - NRH	REVISED
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	PLOT DATE - 09/30/16	DATE - 09/30/16	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNHAM ROAD IMPROVEMENTS
IMPROVEMENT PLAN

F.A. DIST. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2612	16-00110-00-RS	DU PAGE	21	9
CONTRACT NO. 61D55				



NOTES

1. CLASS D PATCHES, 4" AS DIRECTED BY THE ENGINEER.
2. ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ADJUSTMENT ITEM.
3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.
4. ALL CURB AND GUTTER TO MATCH EXISTING EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.

LEGEND

- Adj DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
- Adj-S SANITARY STRUCTURE TO BE ADJUSTED SEE DETAIL ON SHEET 12
- IF INLET FILTERS
- [Hatched Box] REMOVE AND REPLACE EXISTING CURB AND GUTTER
- [Dashed Line] PROPOSED COMB C&G B6.12
- [Dashed Line] PROPOSED COMB C&G B6.12 (DEPRESSED)

PAVEMENT MARKING LEGEND

- ① 4" DOUBLE YELLOW THERMOPLASTIC LINE @ 11" C-C - SOLID
- ② 6" WHITE THERMOPLASTIC LINE - SKIP DASH
- ③ 6" WHITE THERMOPLASTIC LINE - SOLID
- ④ 12" WHITE THERMOPLASTIC LINE - SOLID
- ⑤ 24" WHITE THERMOPLASTIC LINE - SOLID
- ⑥ LETTERS AND SYMBOLS - FULL SIZE

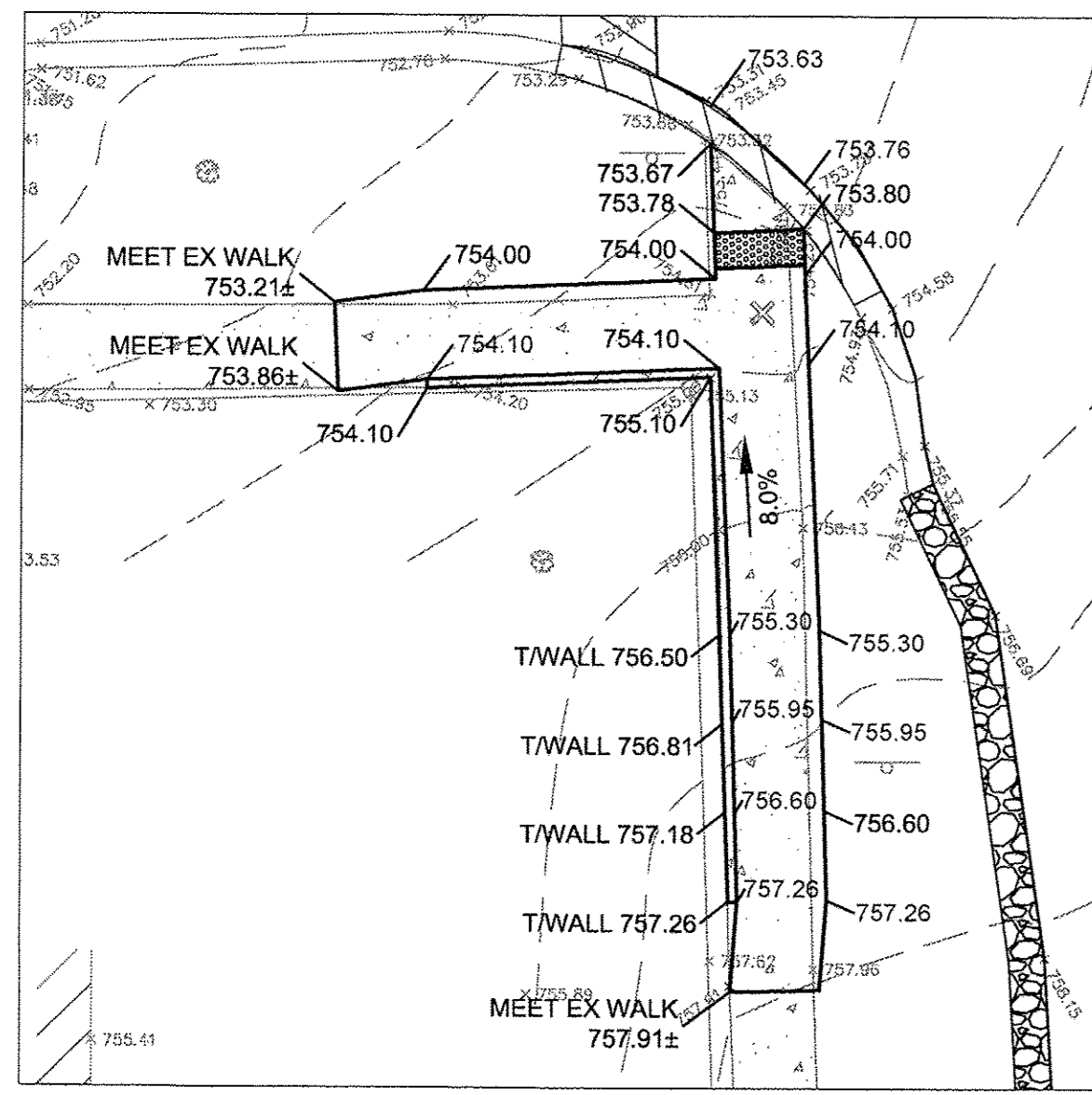
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	PLOT DATE -- 09/30/16	DATE -- 09/30/16	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

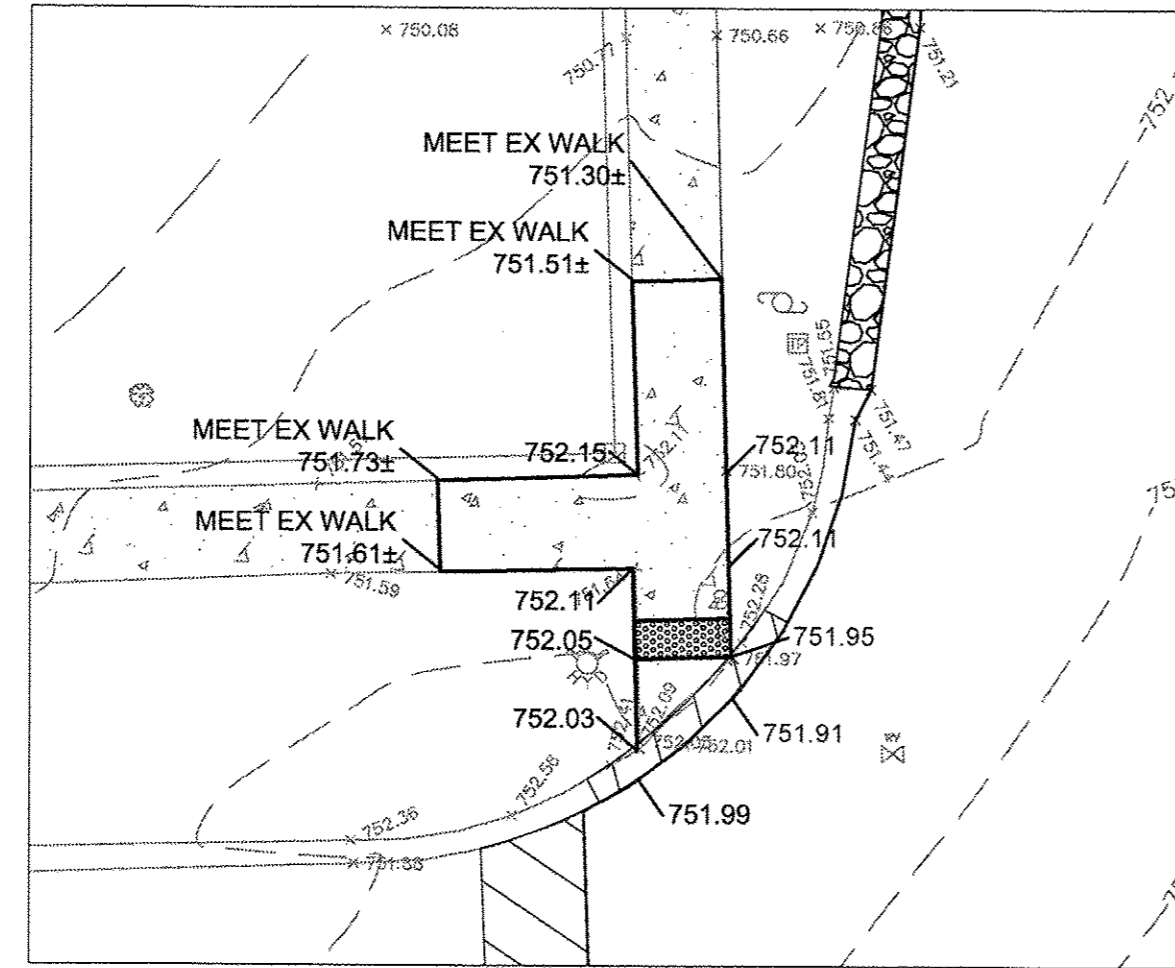
DUNHAM ROAD IMPROVEMENTS
IMPROVEMENT PLAN

SCALE: 1"=20' SHEET NO. 5 OF 5 SHEETS STA. 48+00 TO STA. 52+08

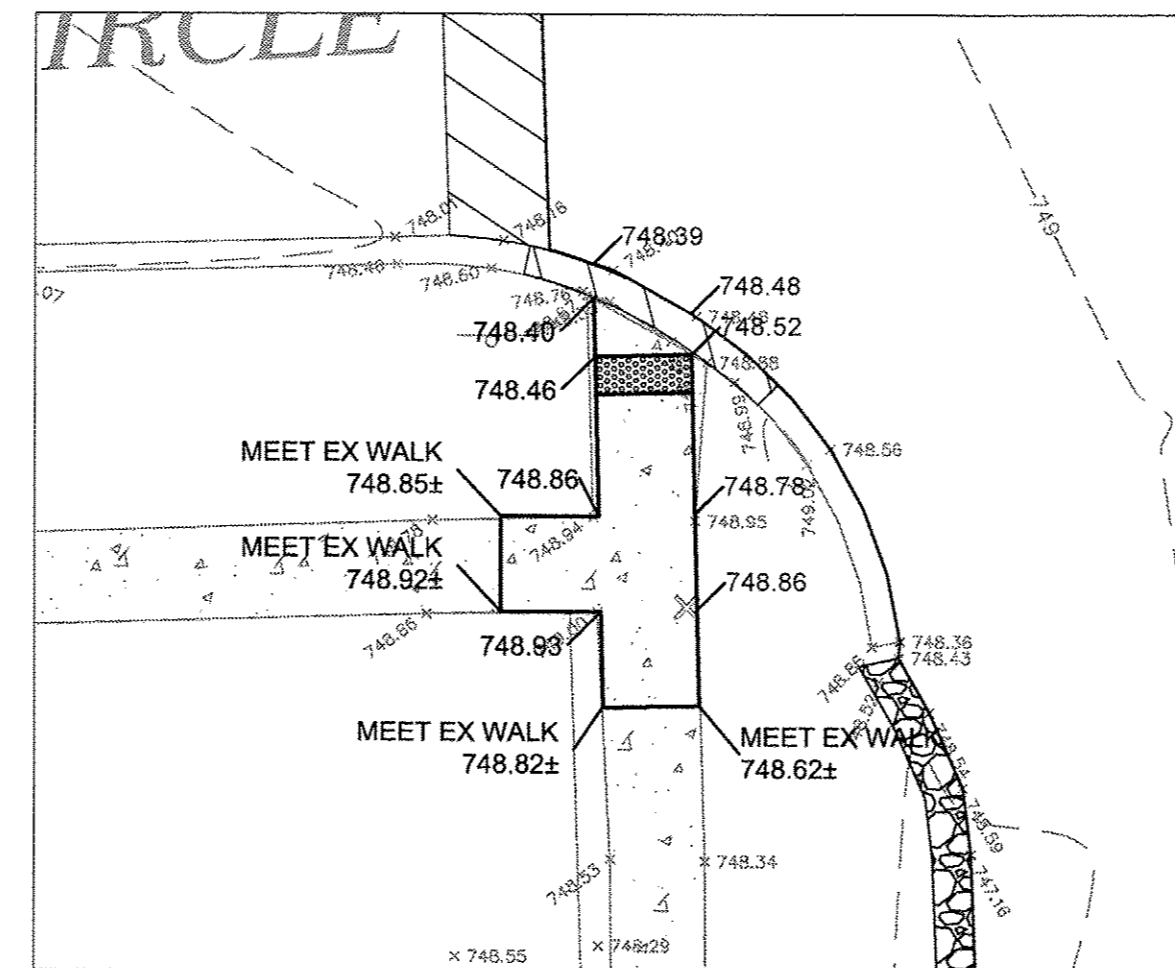
F. A. U. REC.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2612	16-00110-00-RS	DU PAGE	21	10
		CONTRACT NO. 61D55		
		FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT



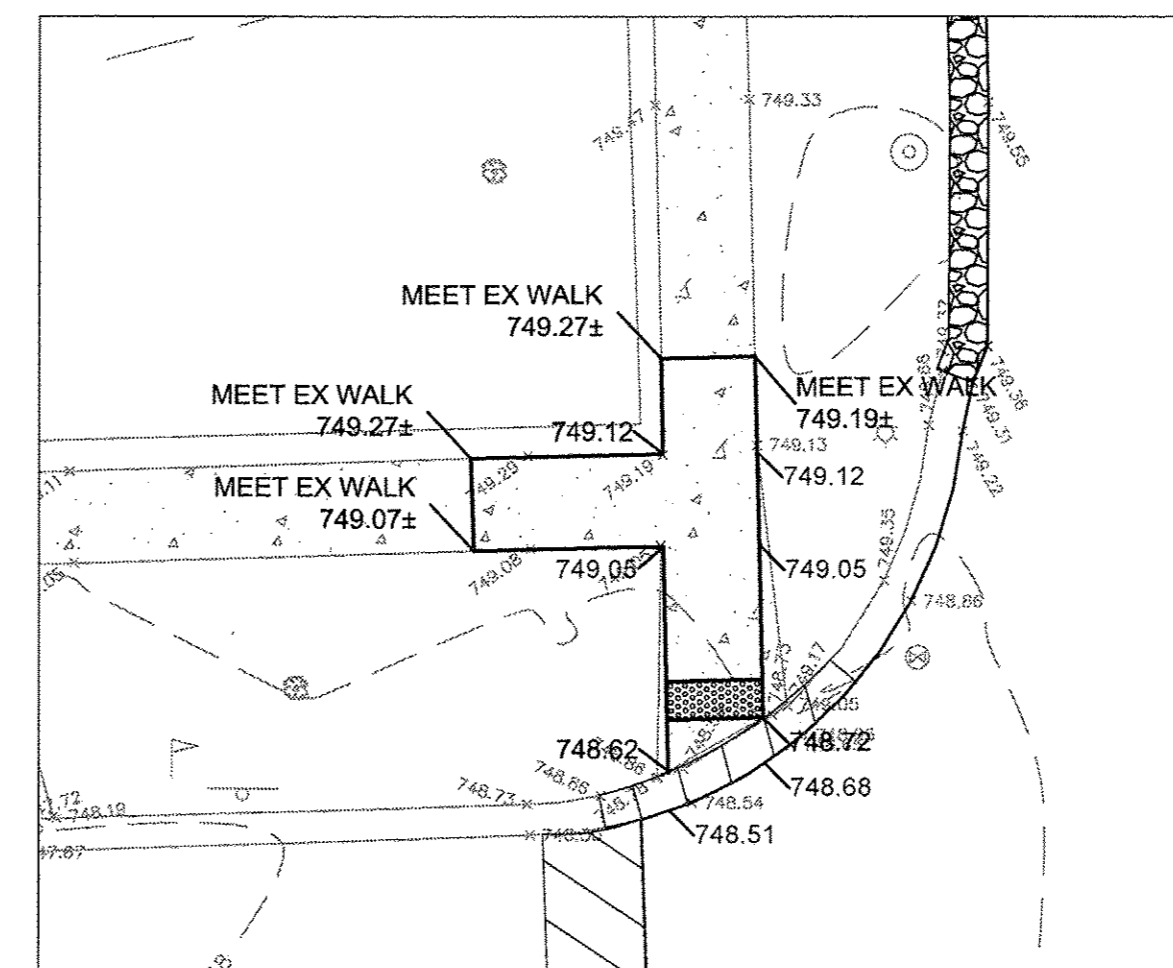
**SIDEWALK DETAIL S1
STA. 5+78, LEFT SIDE**



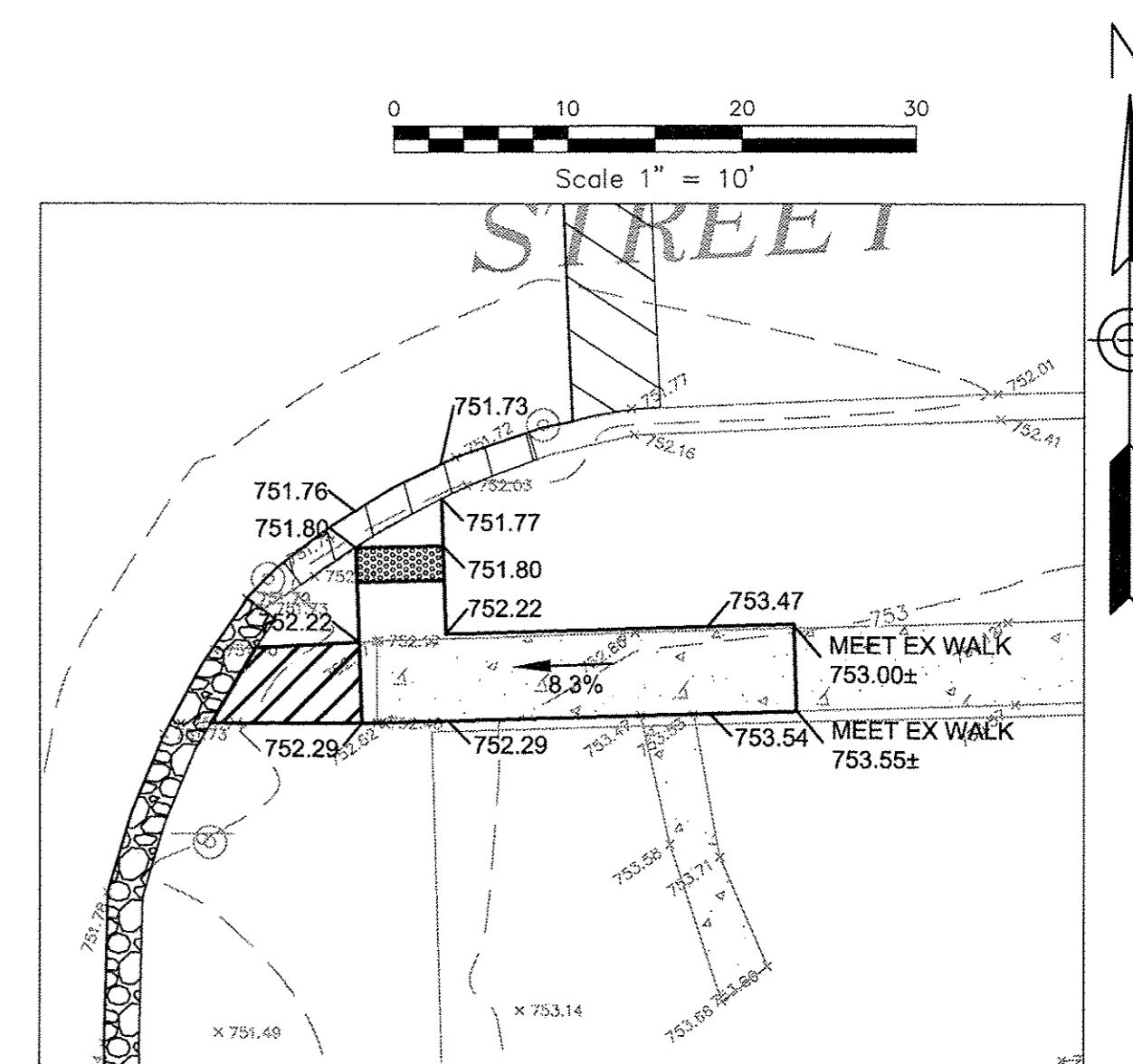
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STA. 5+30, LEFT SIDE**



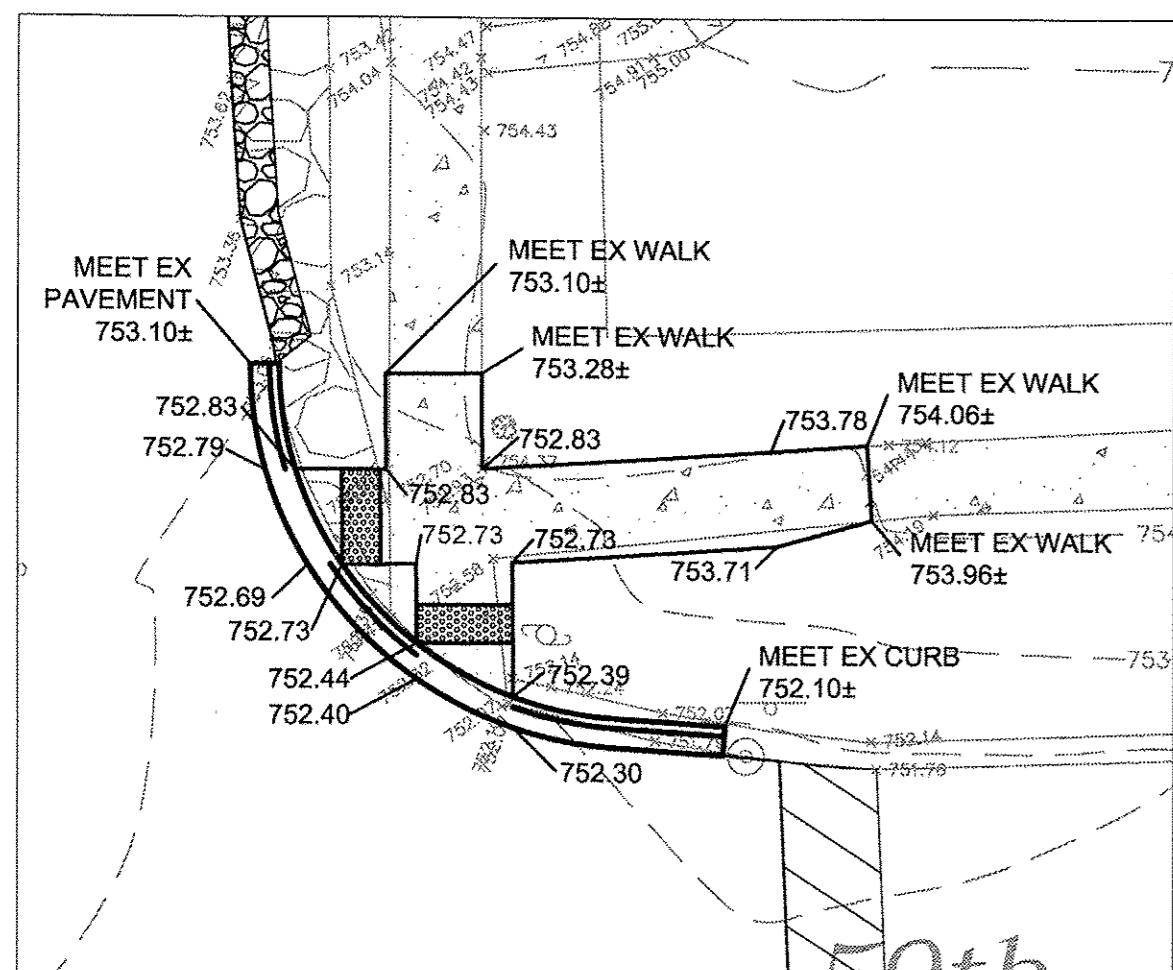
**SIDEWALK DETAIL S3
STA. 24+00, LEFT SIDE**



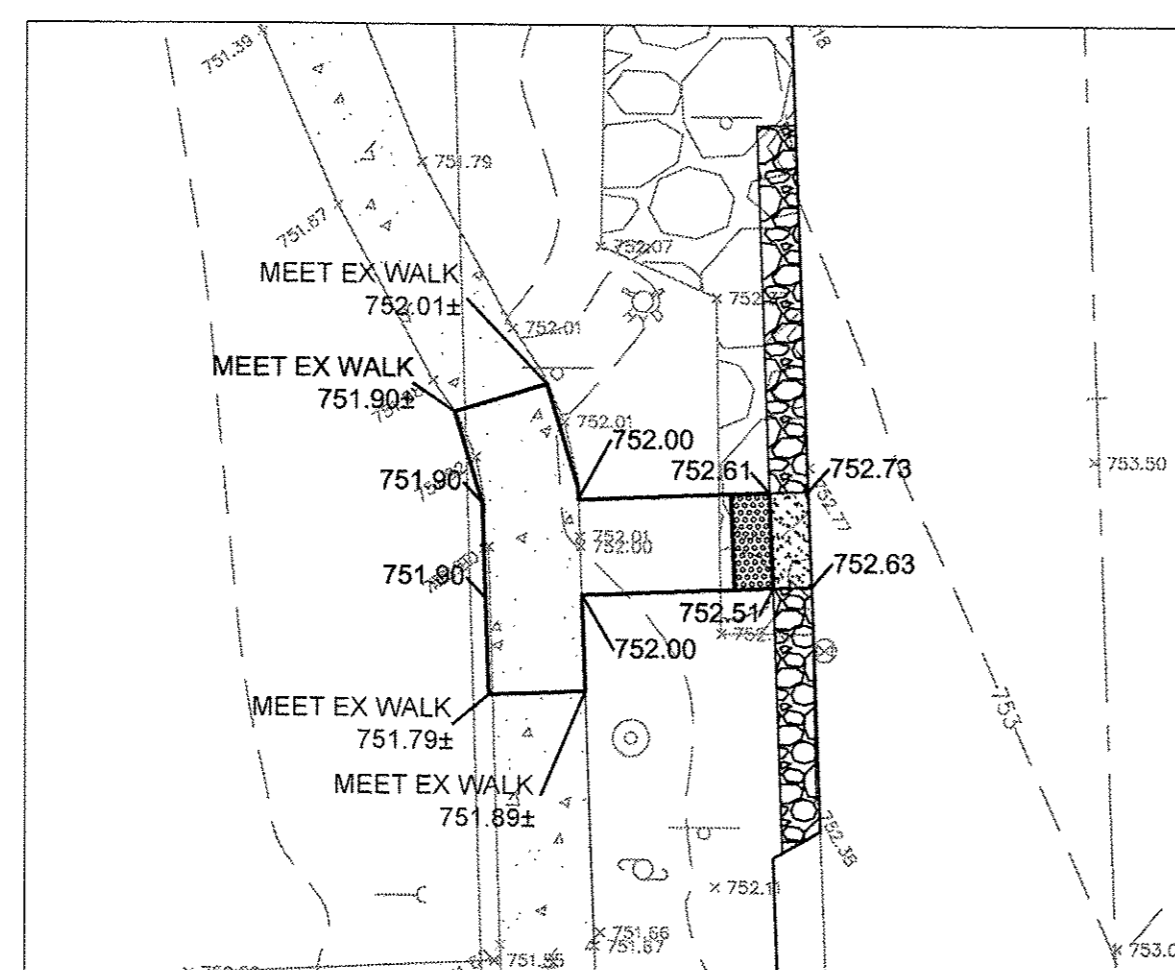
**SIDEWALK DETAIL S4
STA. 24+40, LEFT SIDE**



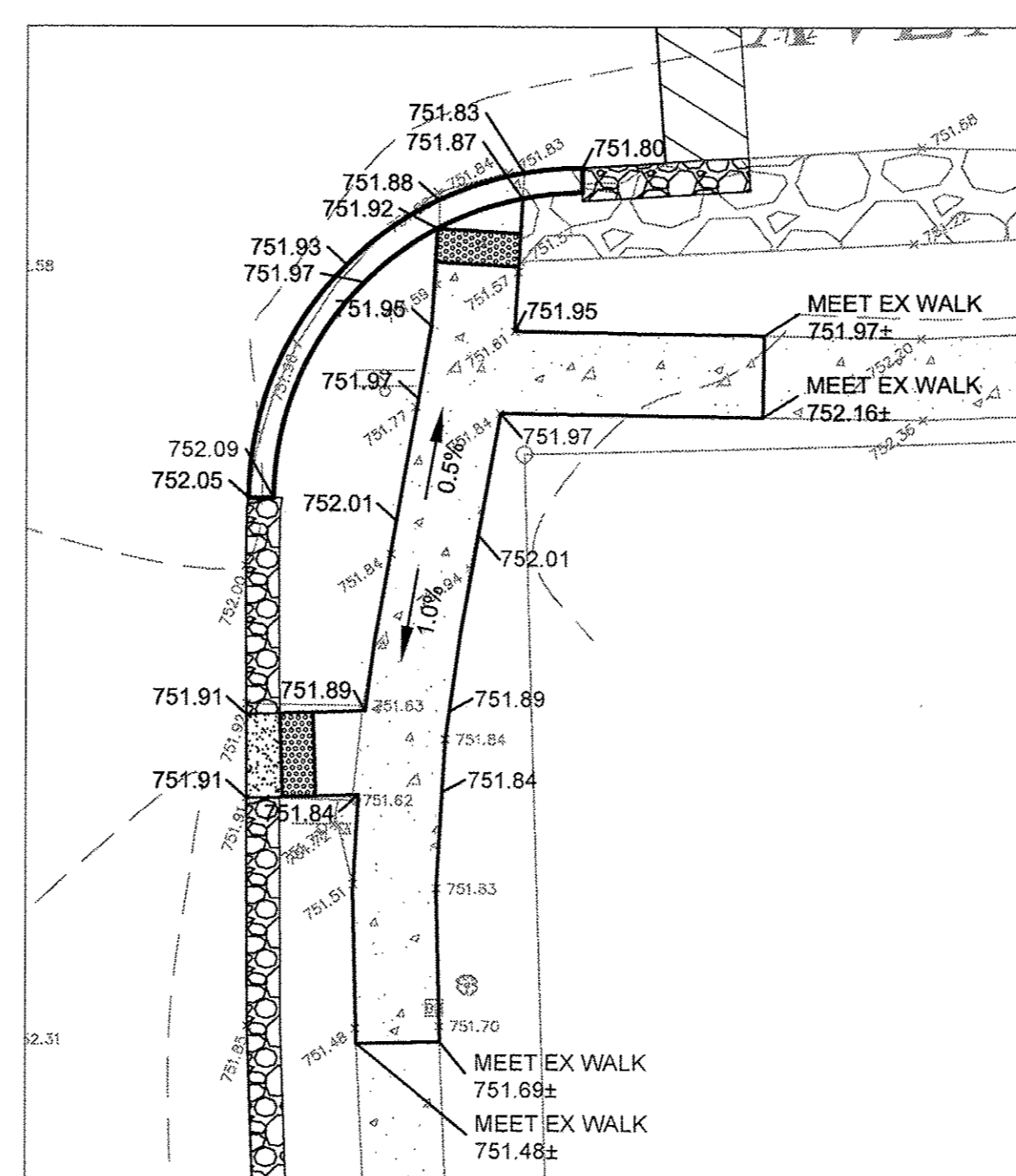
**SIDEWALK DETAIL S5
STA. 25+95, RIGHT SIDE**



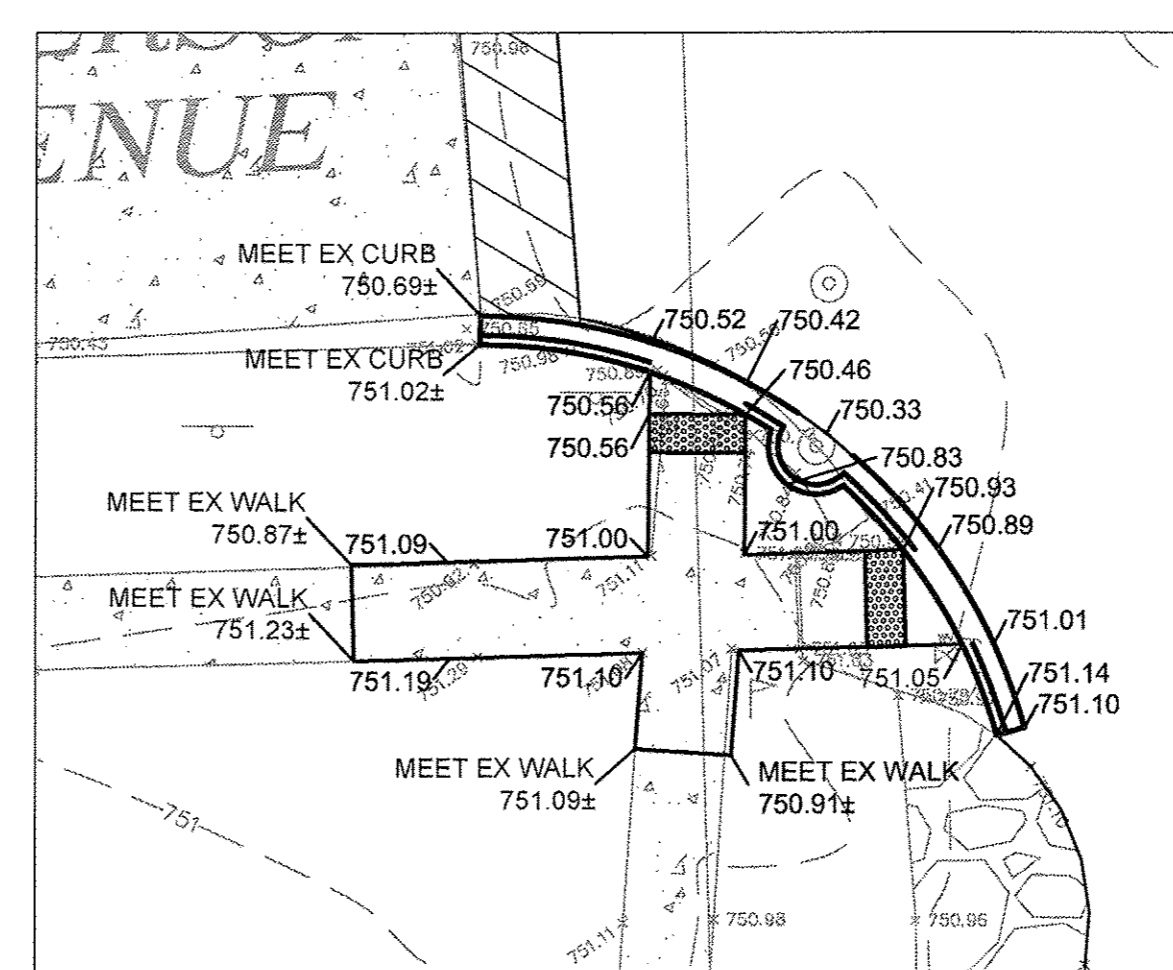
**SIDEWALK DETAIL S6
STA. 26+50, RIGHT SIDE**



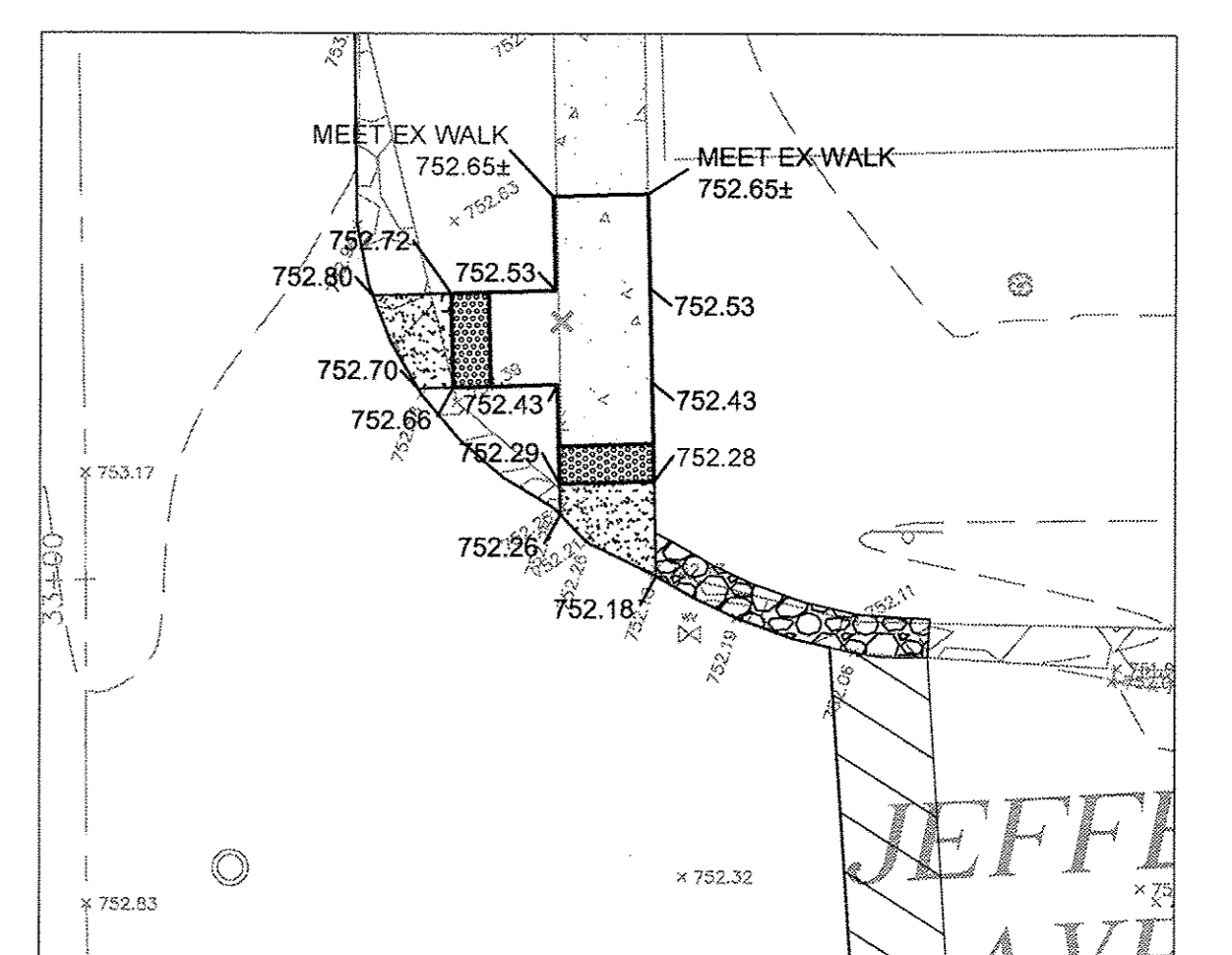
**SIDEWALK DETAIL S7
STA. 26+43, LEFT SIDE**



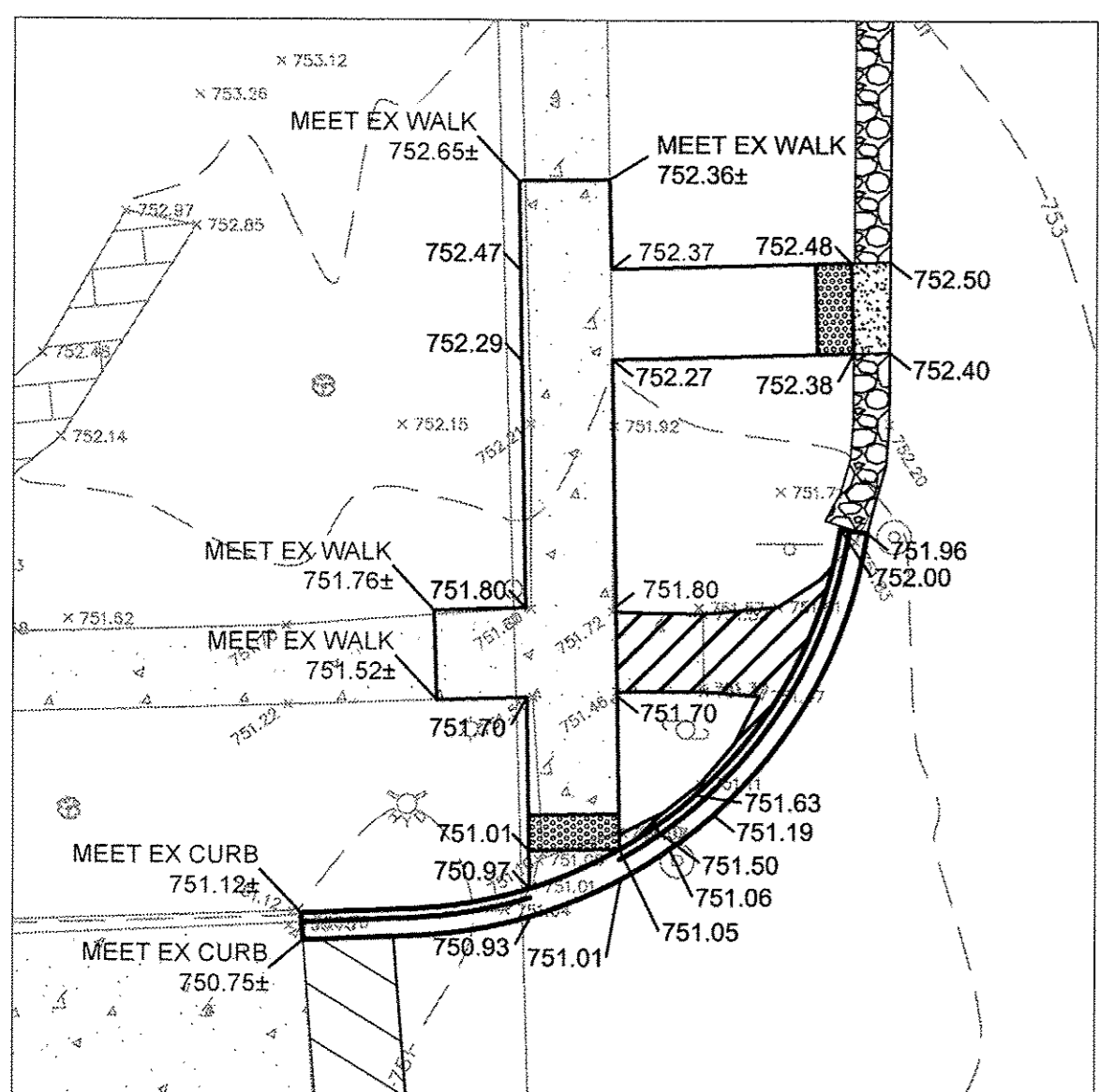
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STA. 32+60, RIGHT SIDE**



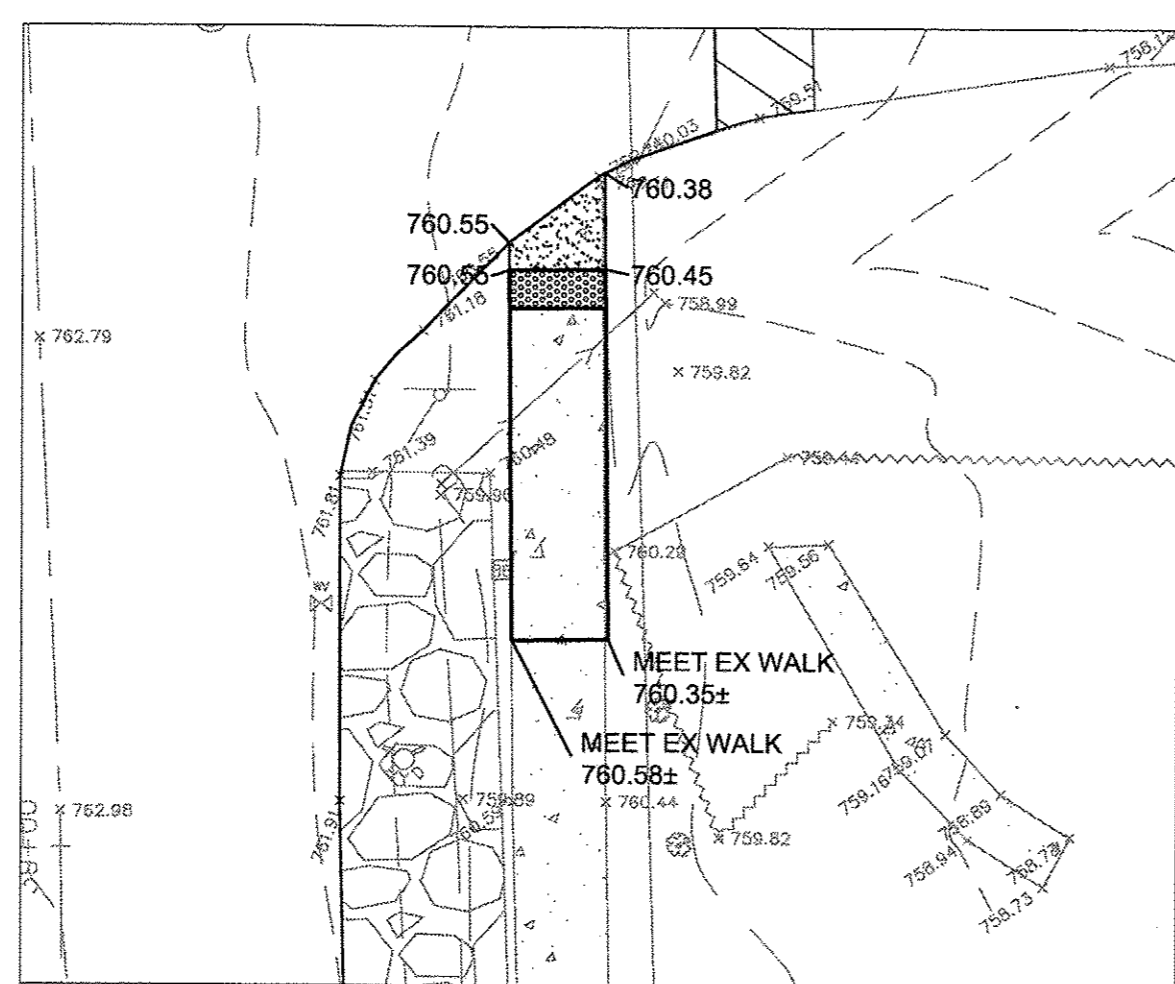
**SIDEWALK DETAIL S9
STA. 32+35, LEFT SIDE**



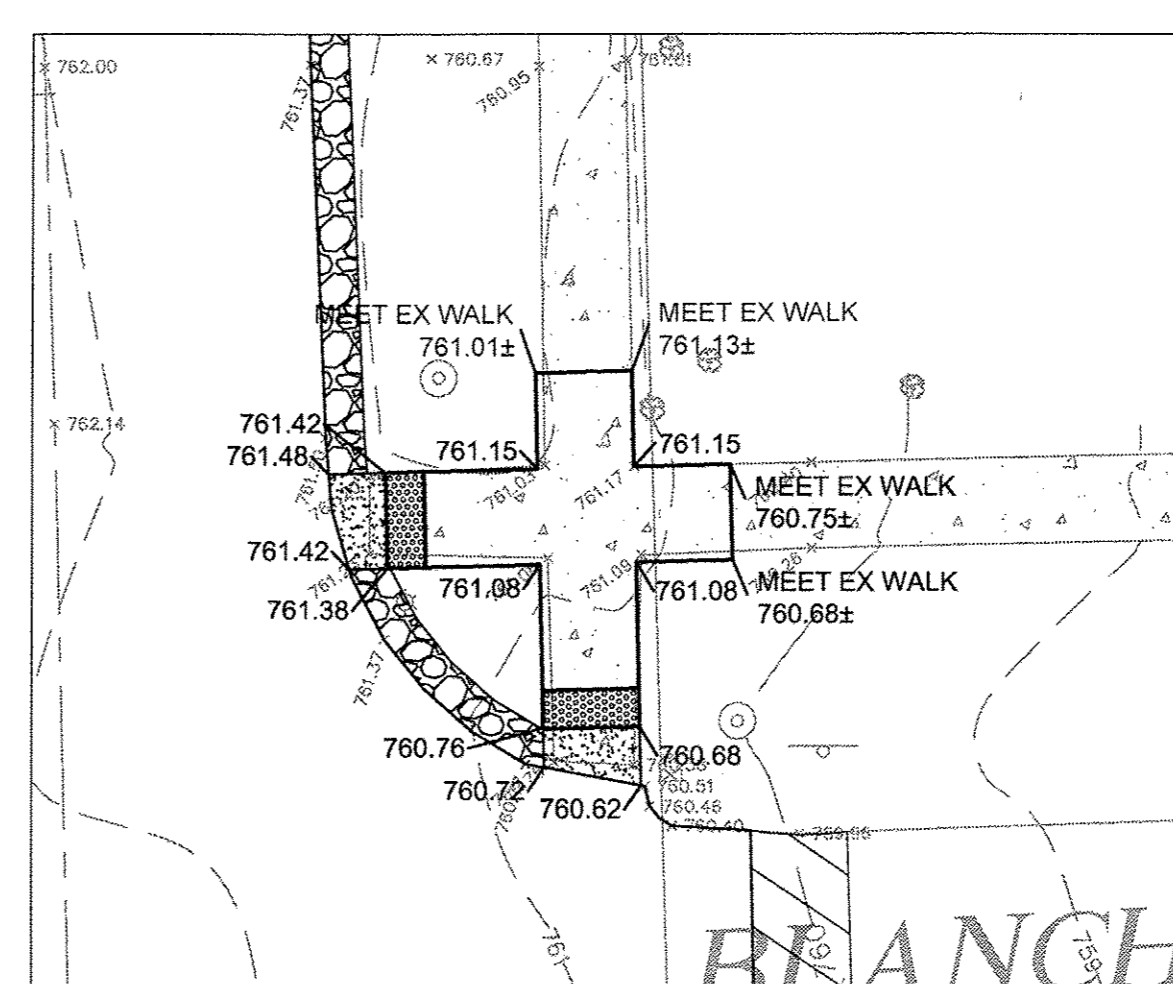
**SIDEWALK DETAIL S10
STA. 33+11, RIGHT SIDE**



**SIDEWALK DETAIL S11
STA. 33+11, LEFT SIDE**



**SIDEWALK DETAIL S12
STA. 39+26, RIGHT SIDE**



**SIDEWALK DETAIL S13
STA. 39+75, RIGHT SIDE**

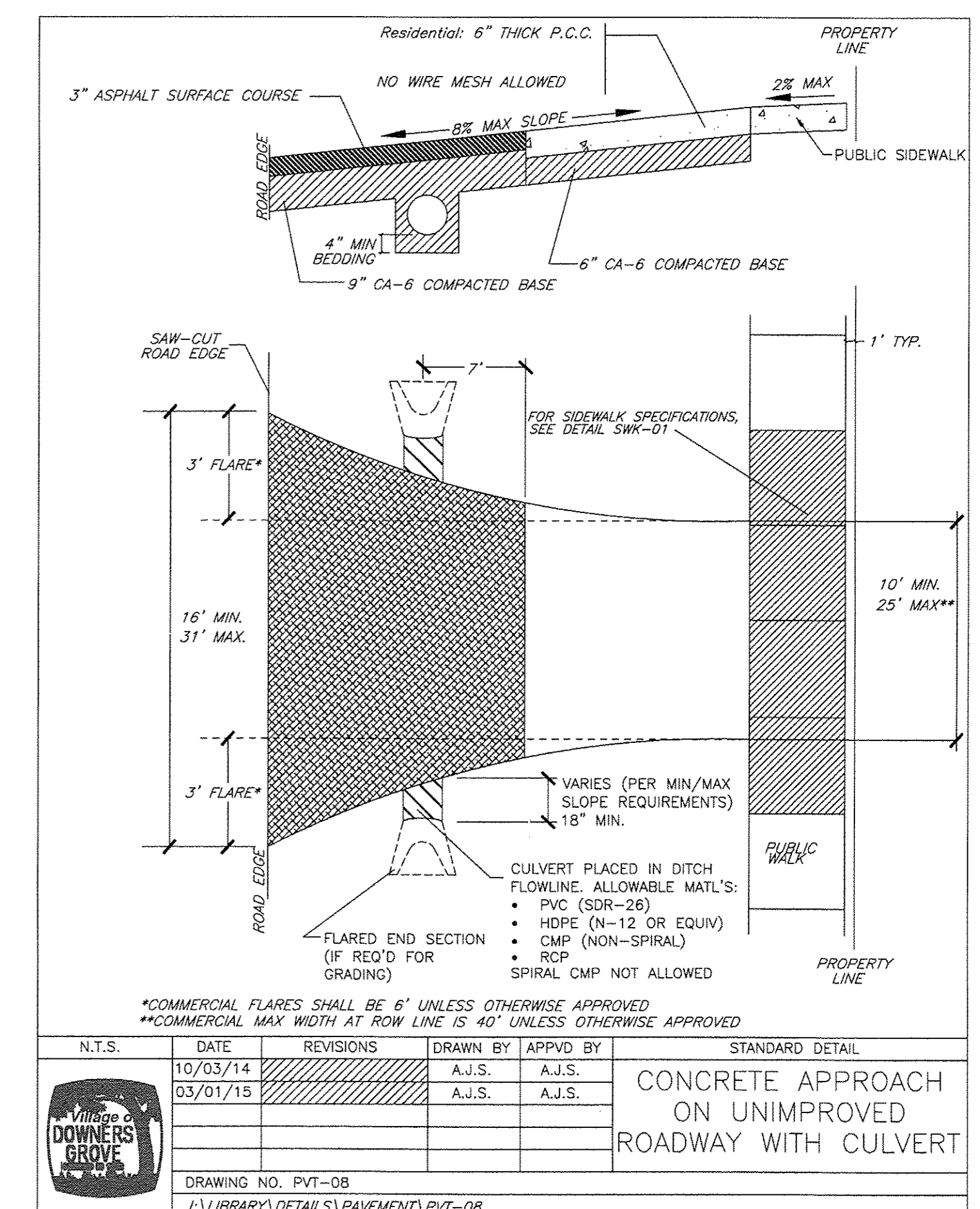
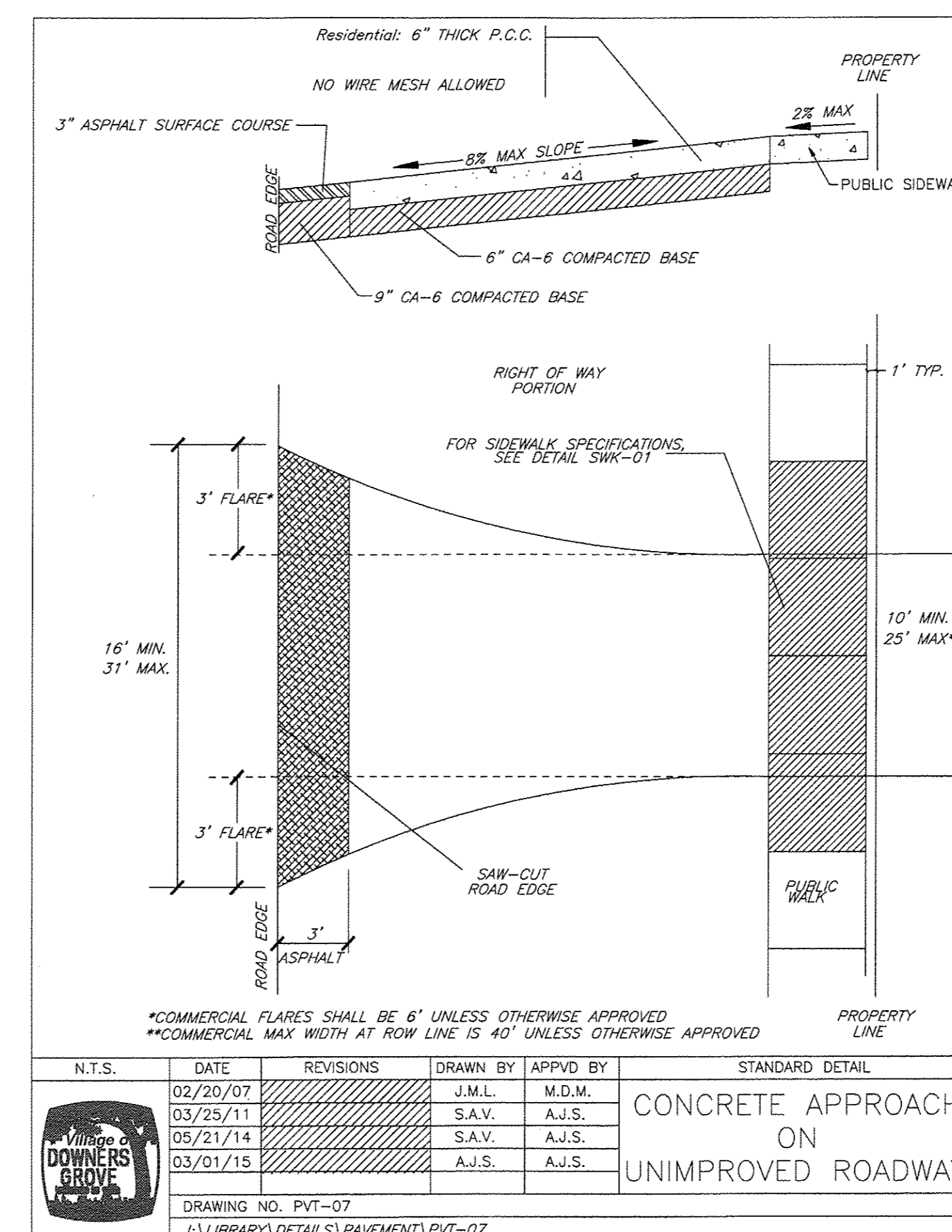
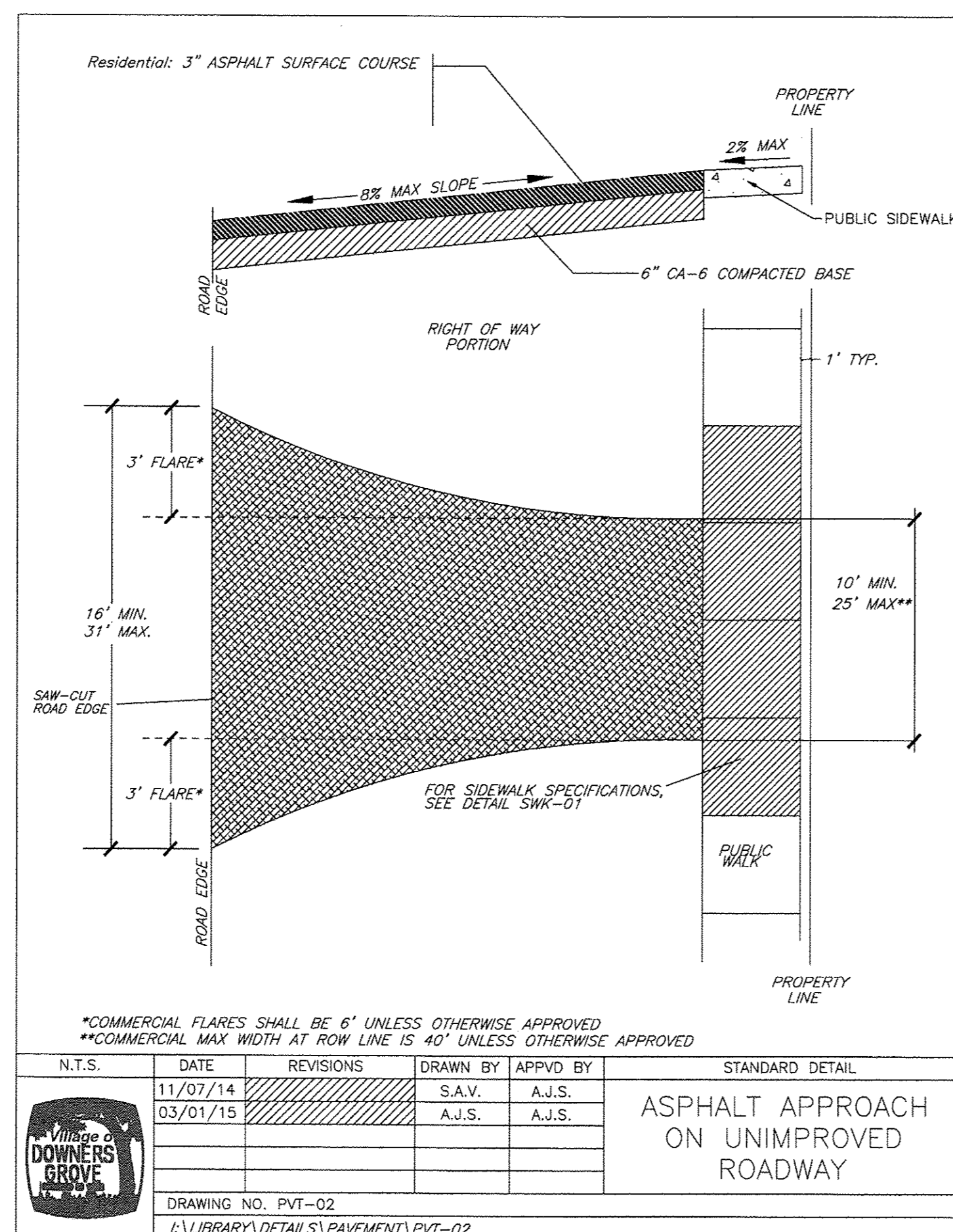
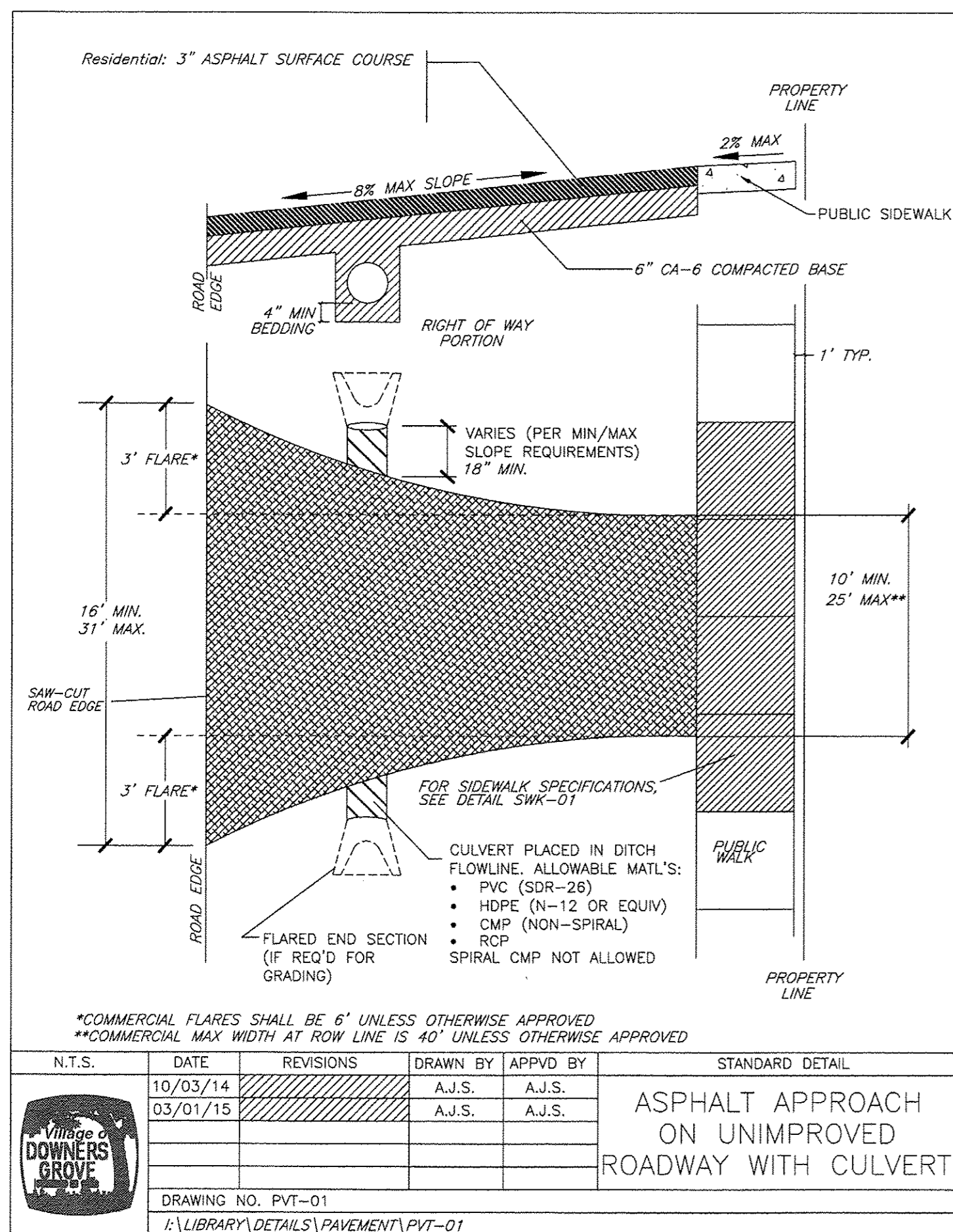
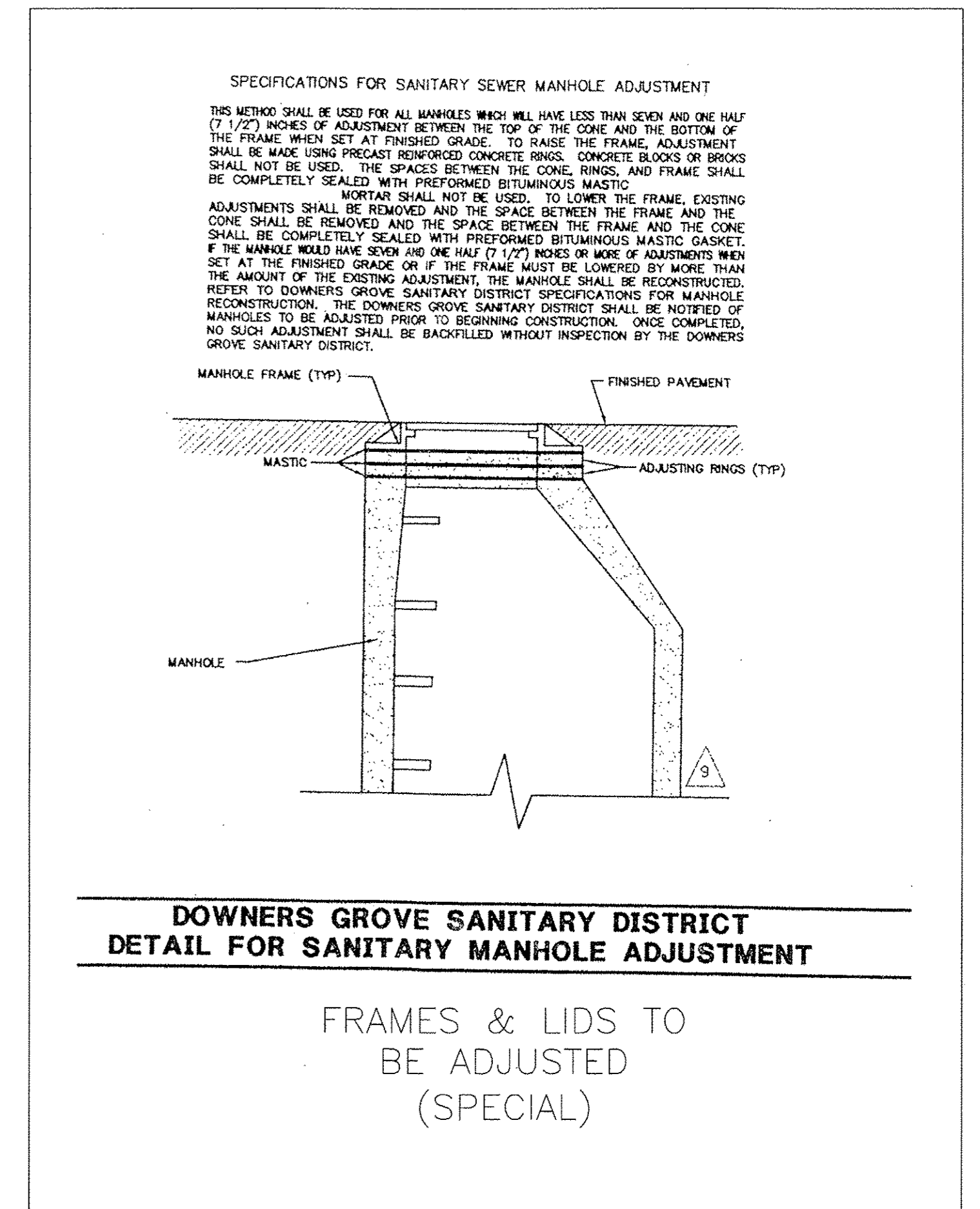
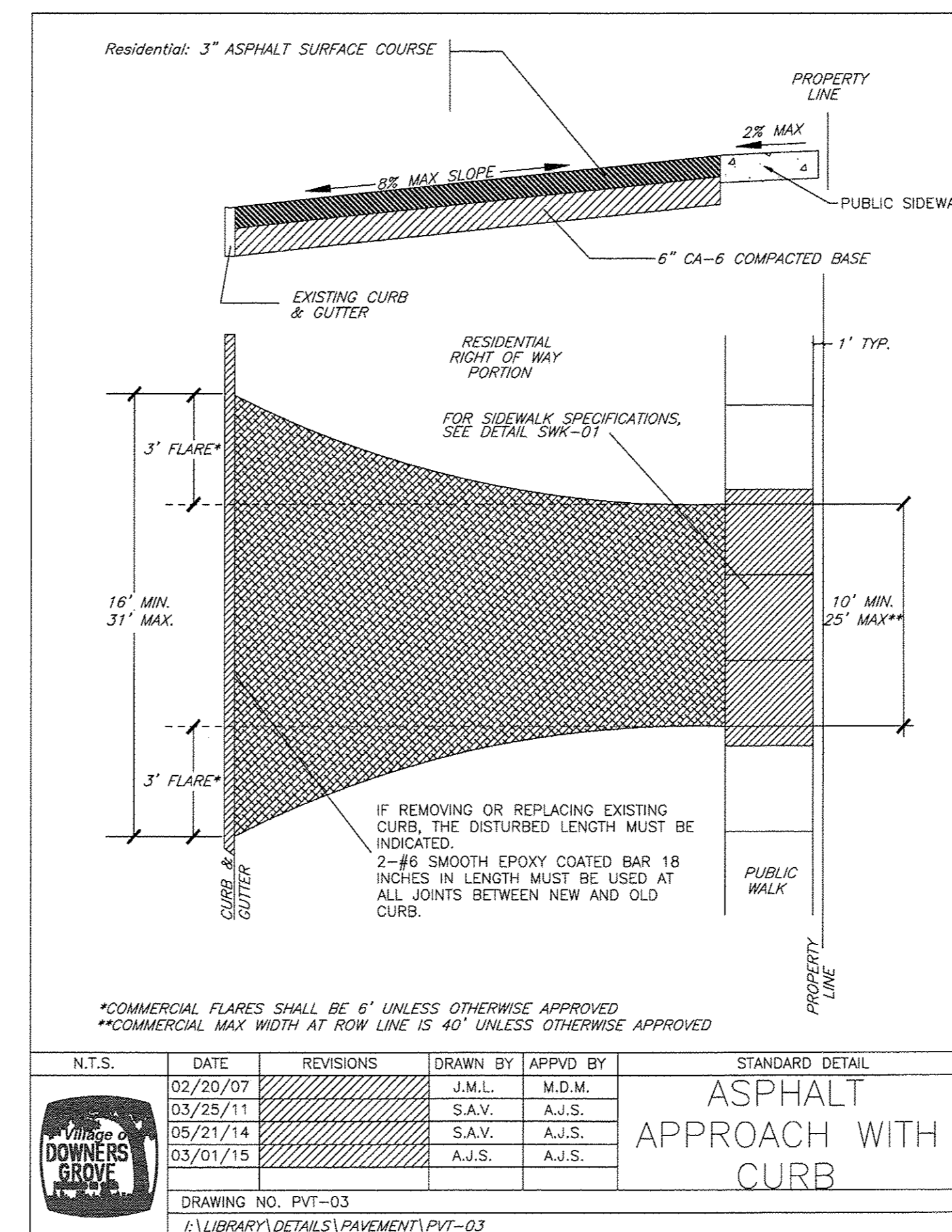
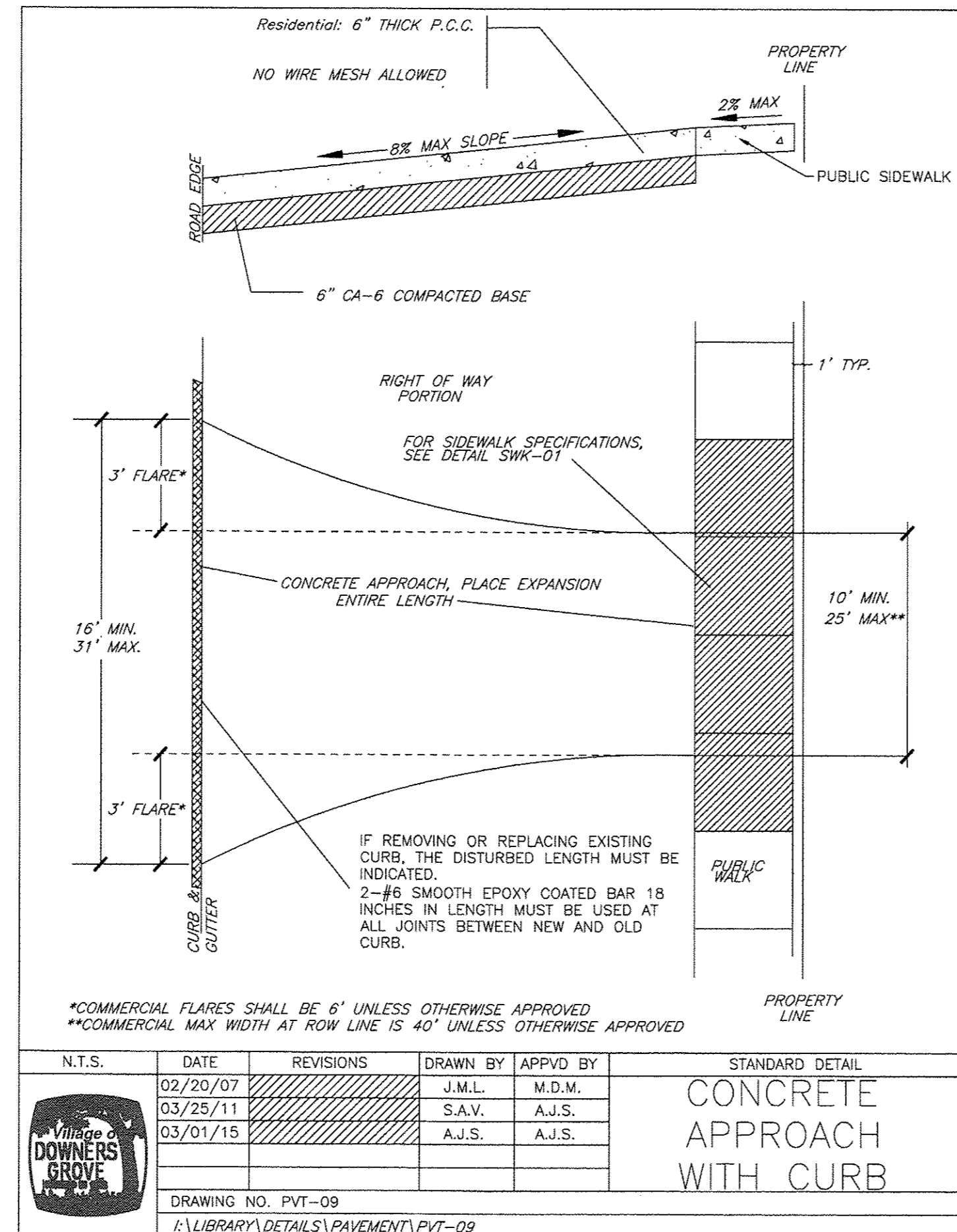
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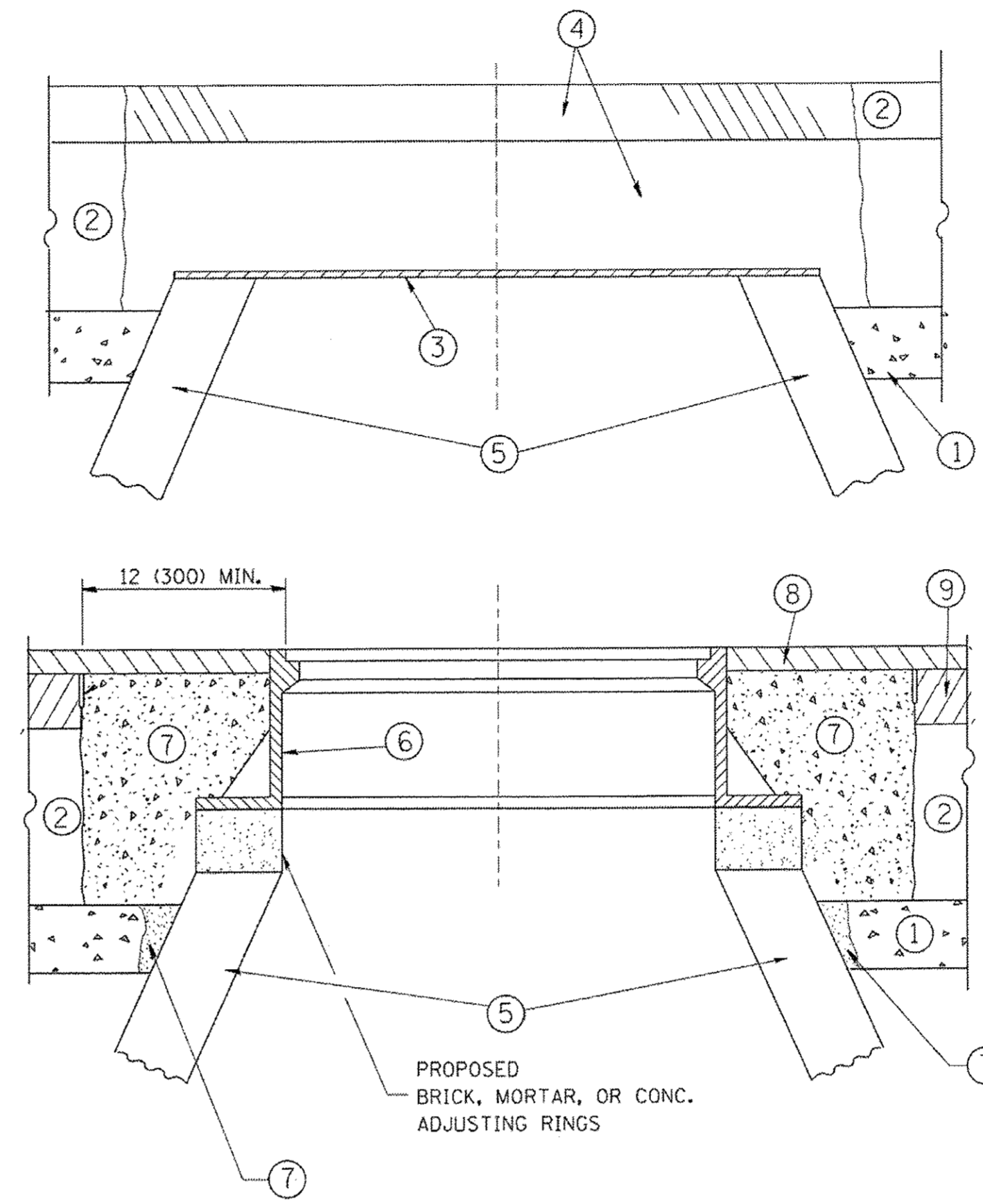
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNHAM ROAD IMPROVEMENTS
SIDEWALK DETAILS

SCALE: 1"=10' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F. A. U. I. D.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2612	16-00110-00-RS	DU PAGE	21	11
CONTRACT NO. 61D55				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		





CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

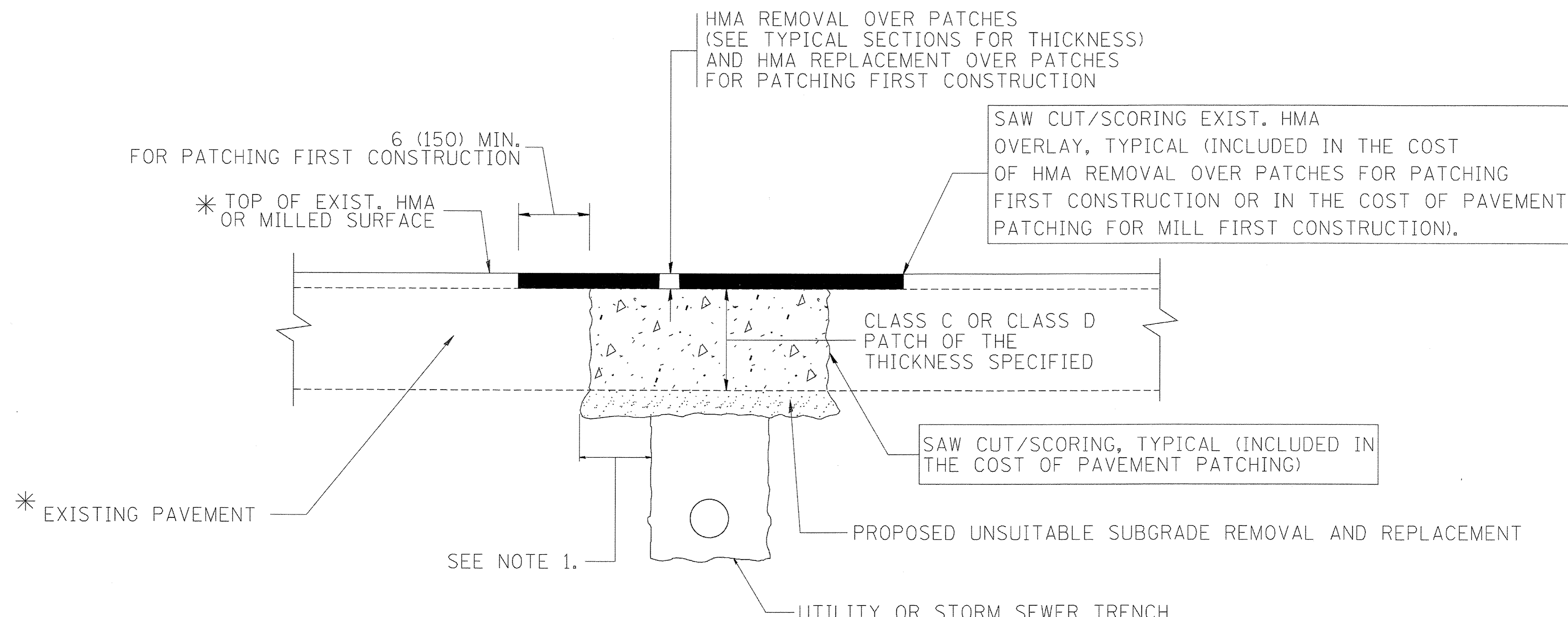
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		CHECKED -	REVISED - R. BORO 03-09-11
		DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2612	16-00110-00-RS	DU PAGE	21	13
BD600-03 (BD-8)			CONTRACT NO. 61055	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

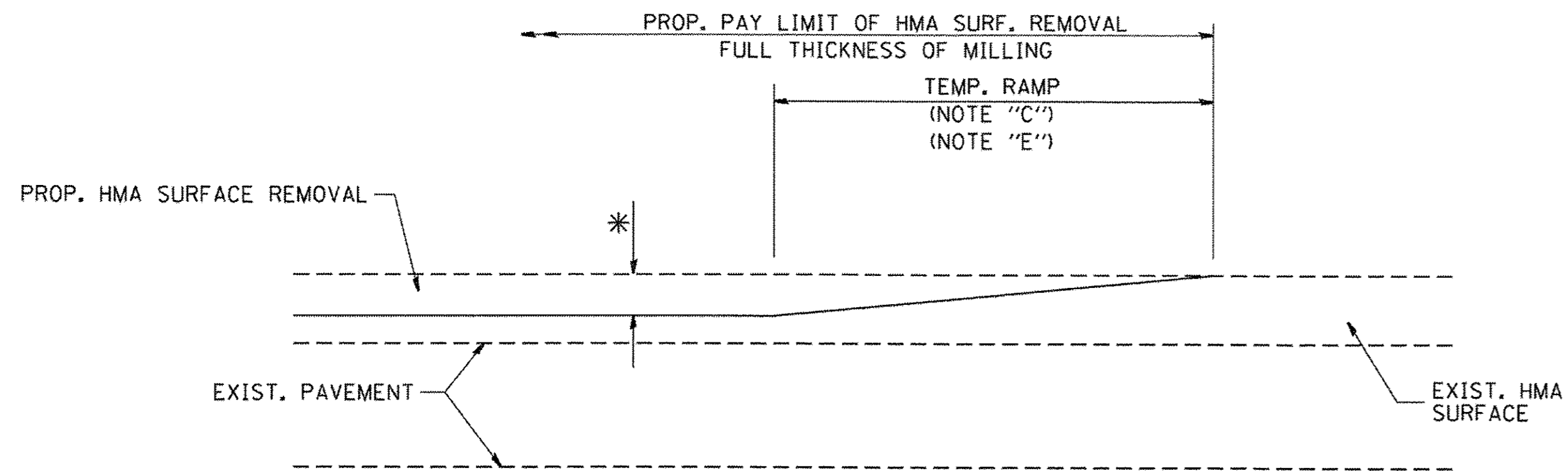
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

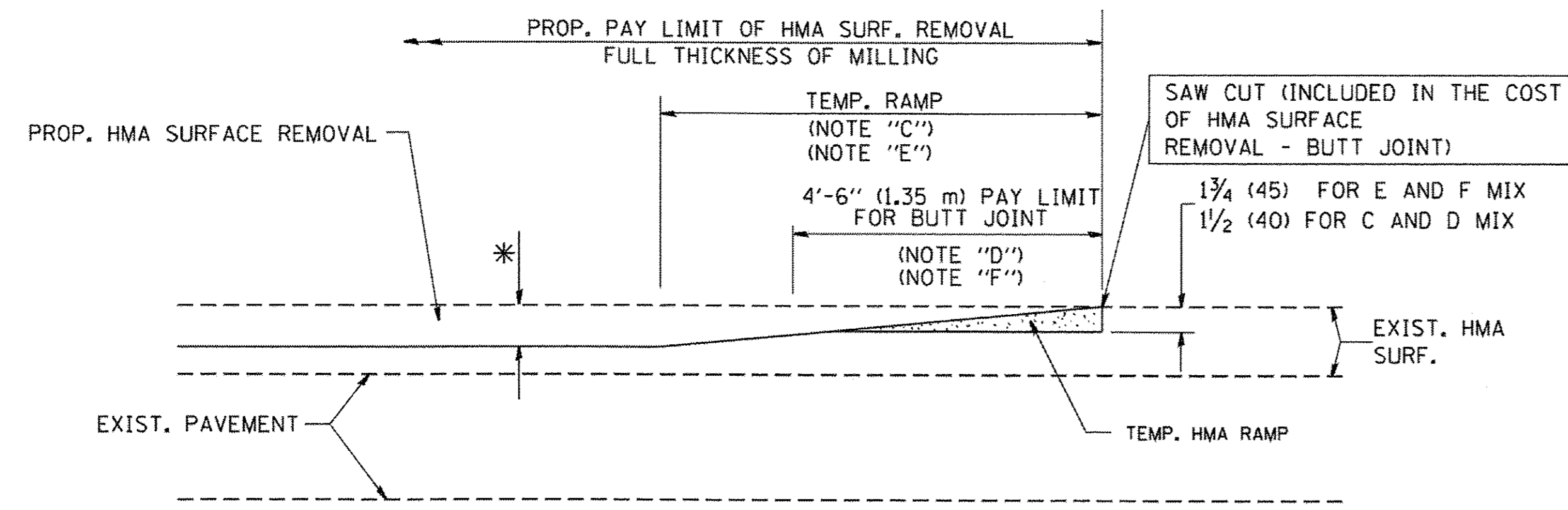
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - R. BORO 01-01-07			2612	16-00110-00-RS	DU PAGE	21	14
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08			BD400-04 (BD-22)		CONTRACT NO.		61D55
						FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

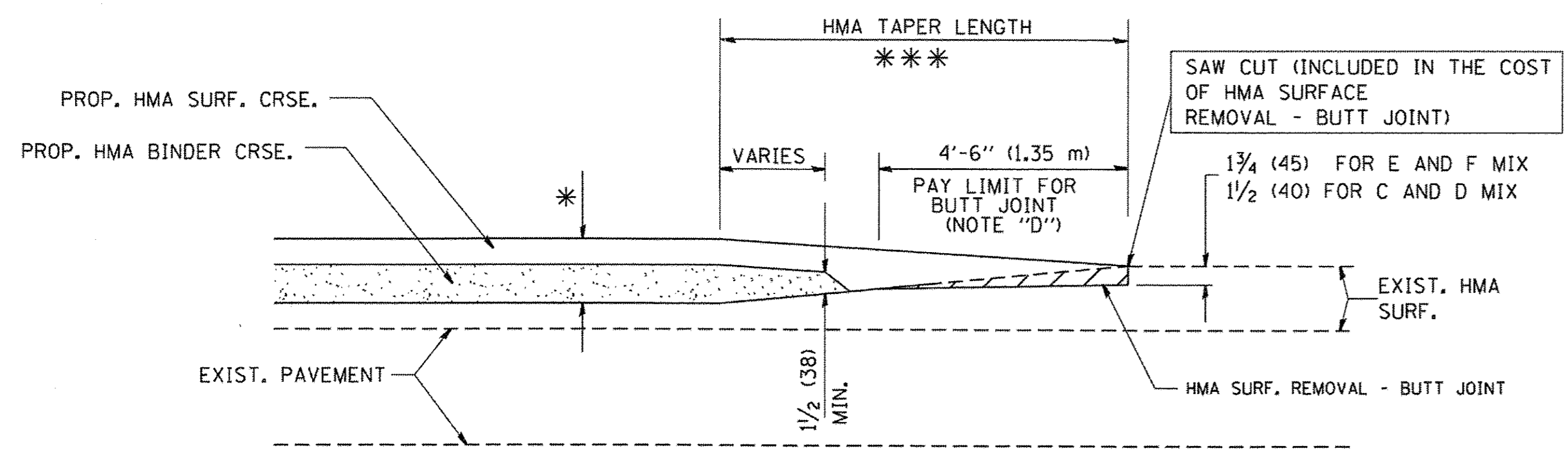
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

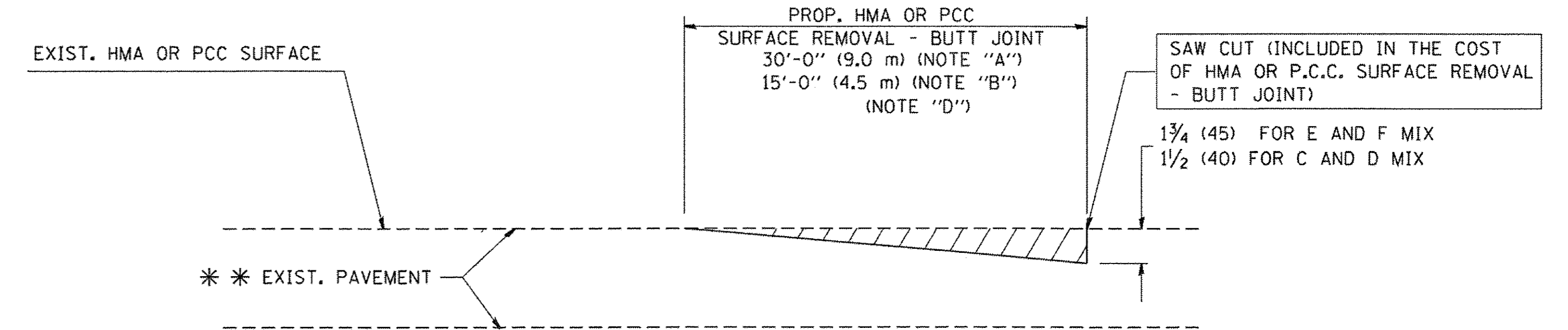
OPTION 2

TYPICAL TEMPORARY RAMP

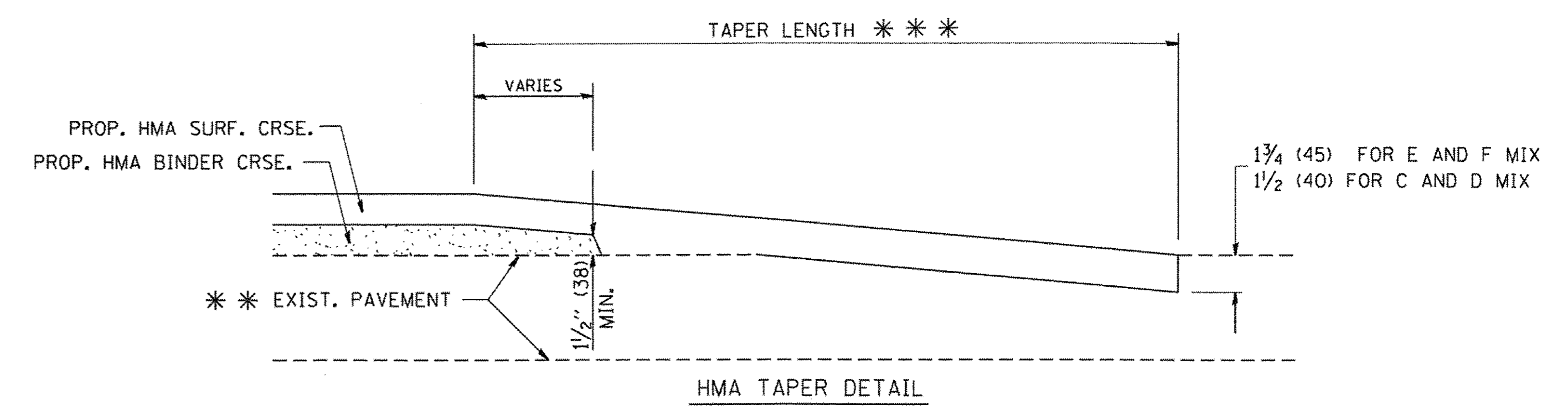


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

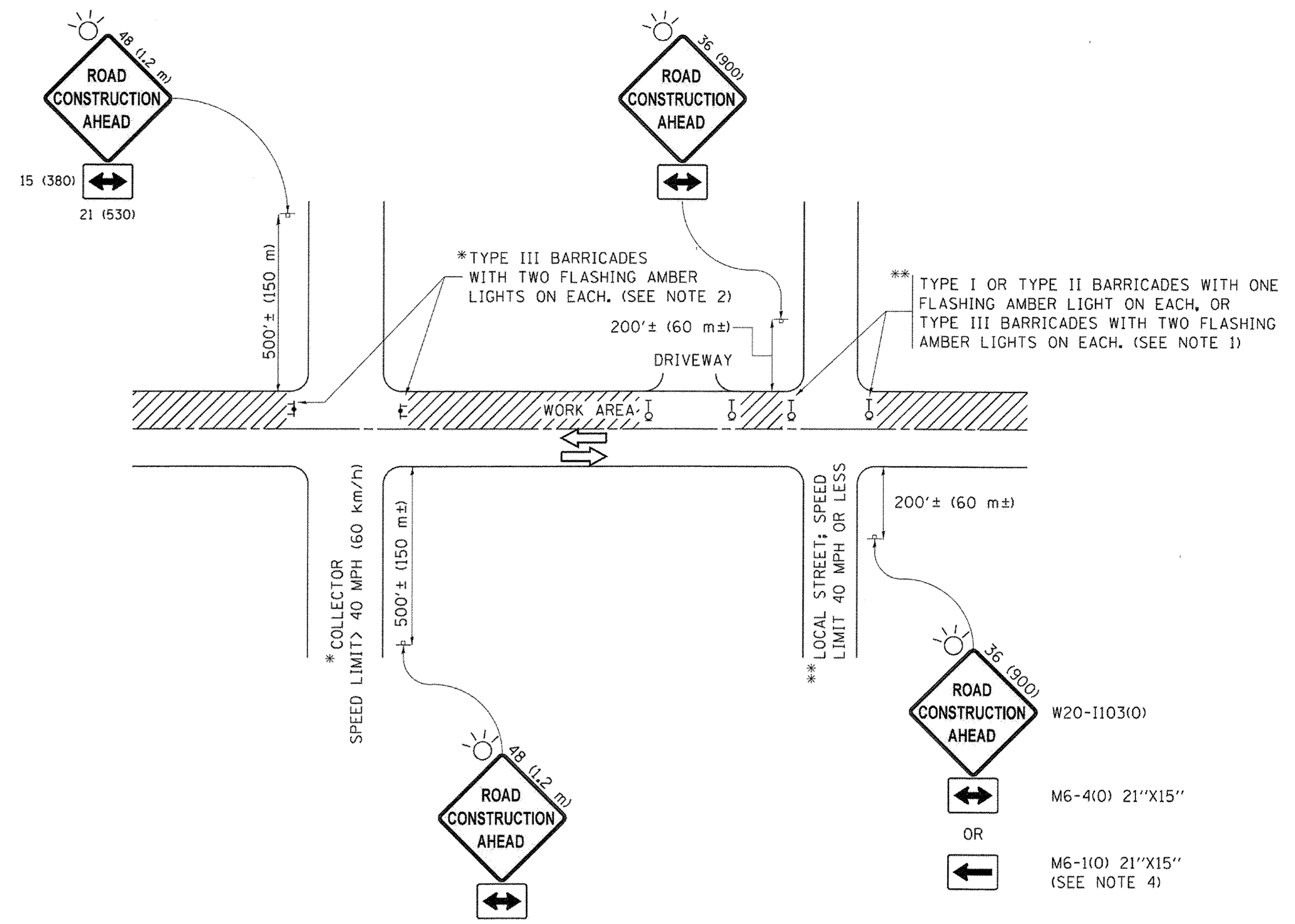
- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gaglienbt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS		F.A.U. RTE. 2812	SECTION 16-00110-00-RS	COUNTY DU PAGE	TOTAL SHEETS 21	SHEET NO. 15
	PLOT SCALE = 50,0000 ' / IN.	DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD400-05 BD32		CONTRACT NO. 61D55
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - M. GOMEZ 04-06-01								
		DATE - 06-13-90	REVISED - R. BORO 01-01-07								



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S), THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = Footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
p:\l\084EBIDINTEG\Illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\Dist	DRAWN - CADDData\CADsheets\tcl10.dgn	CHECKED -	REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 50.000 1/ in.	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 9/15/2016		REVISED - A. SCHUETZE 09-15-16

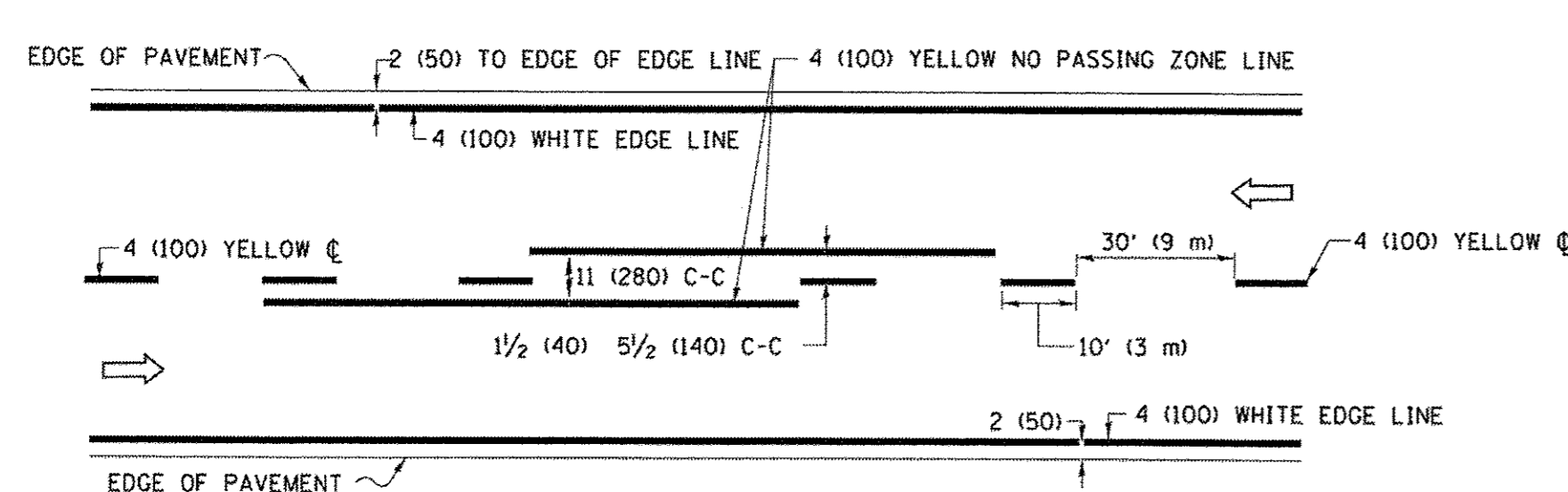
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

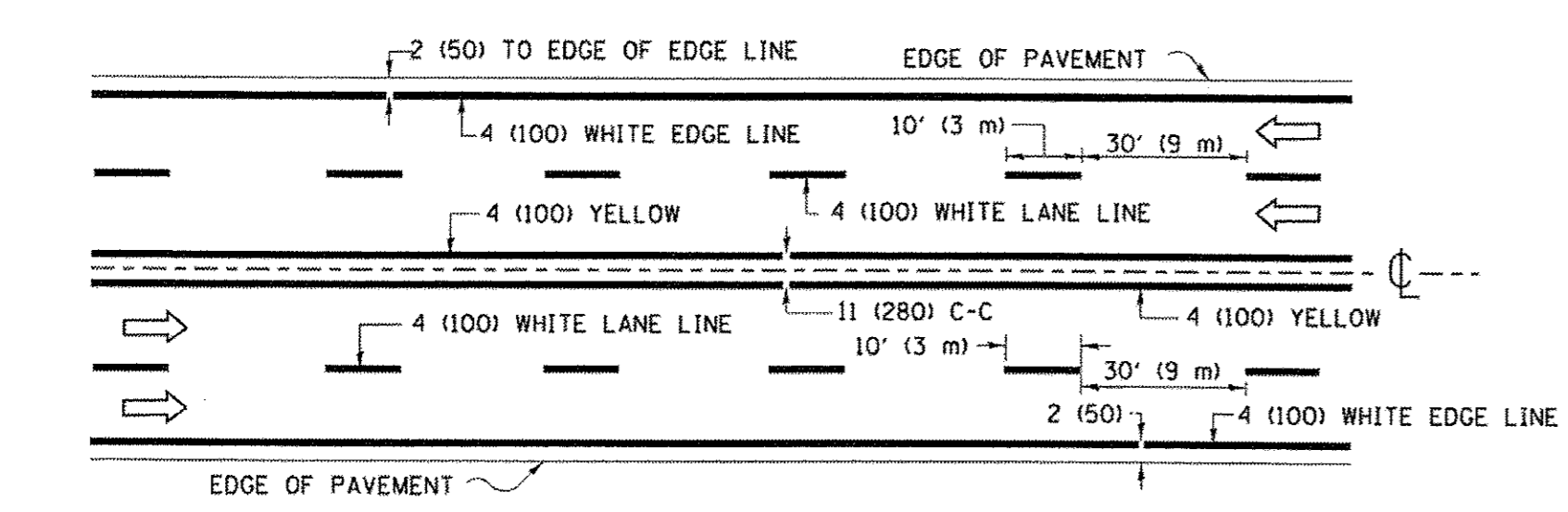
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2612	16-00110-00-RS	DU PAGE	21	16
TC-10			CONTRACT NO. 61D55	
ILLINOIS FED. AID PROJECT				

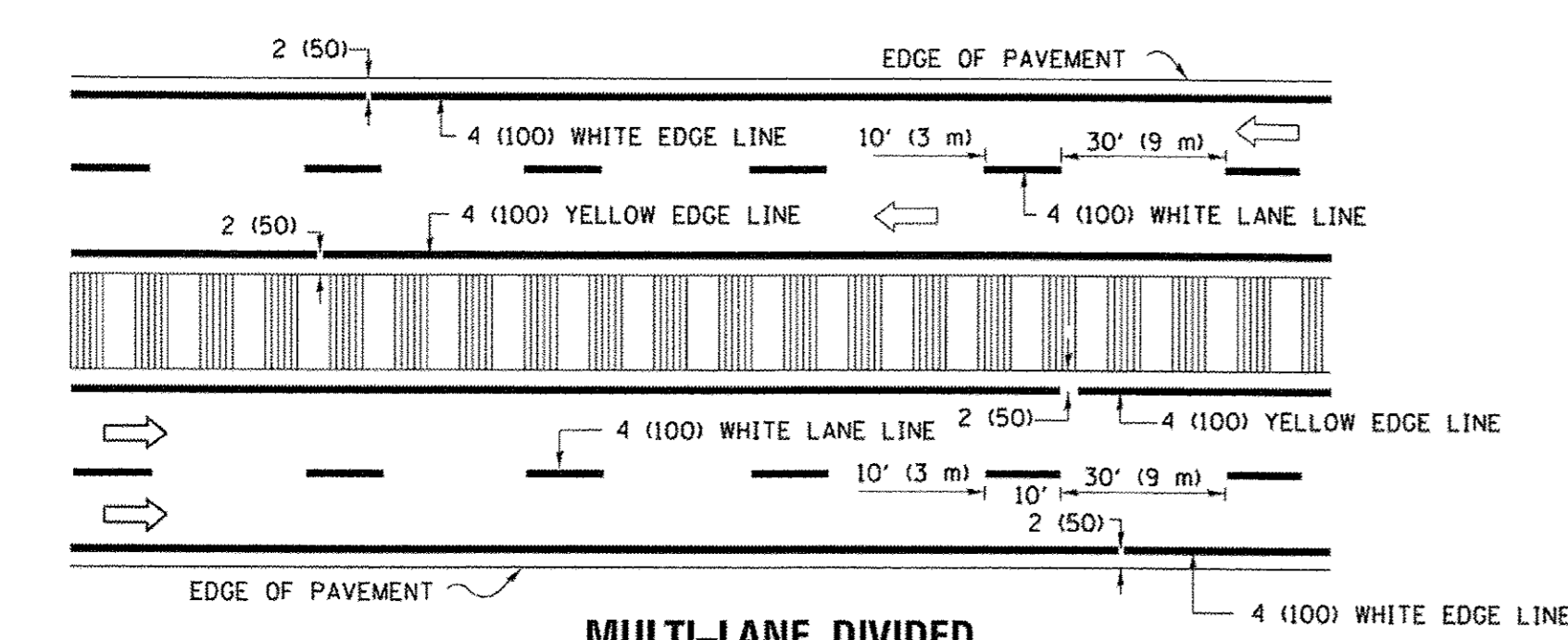
D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55



2-LANE ROADWAY

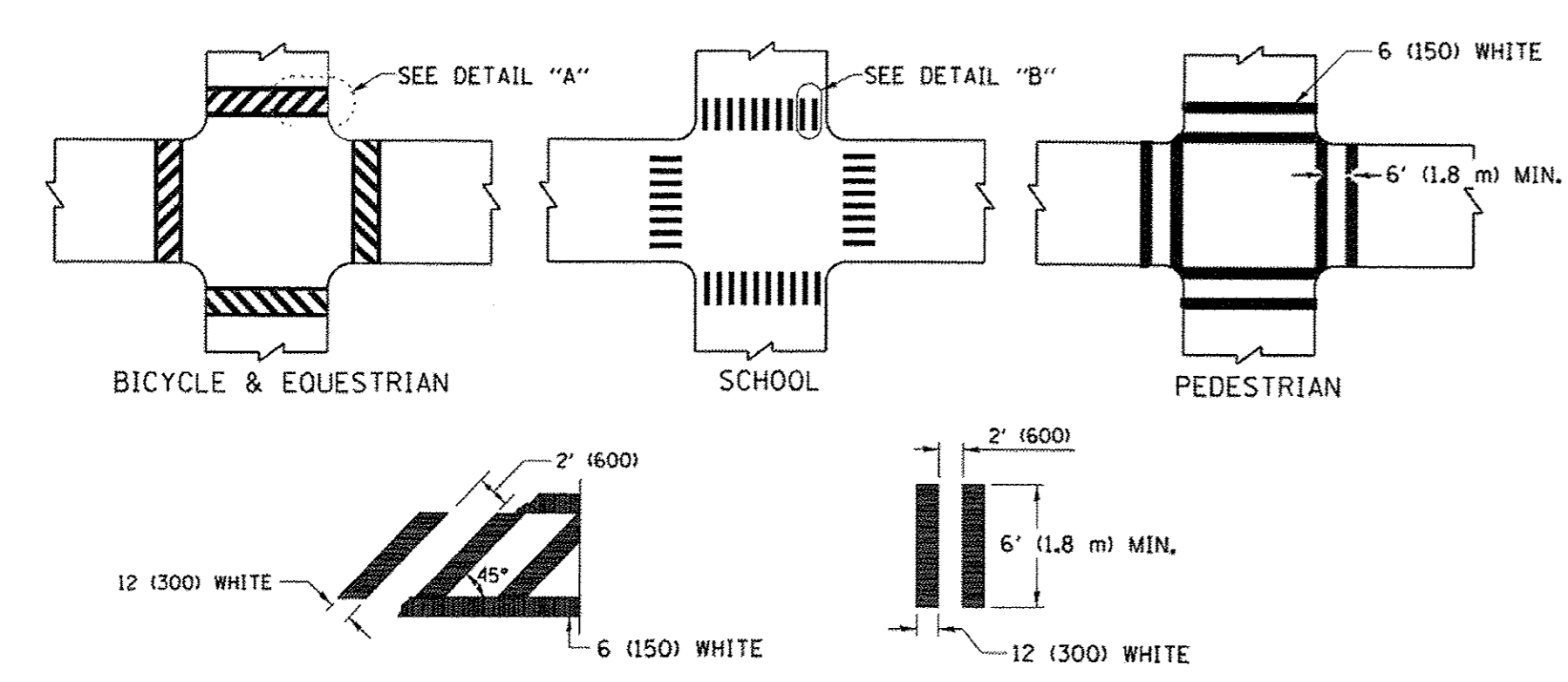


MULTI-LANE UNDIVIDED



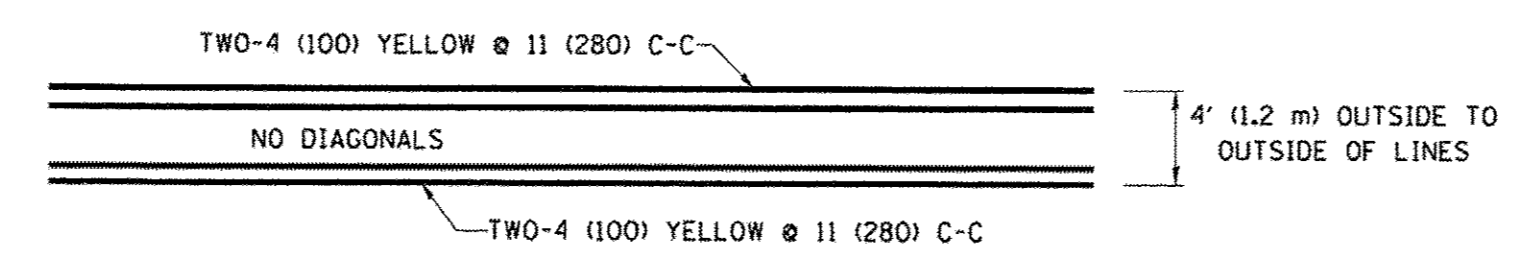
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

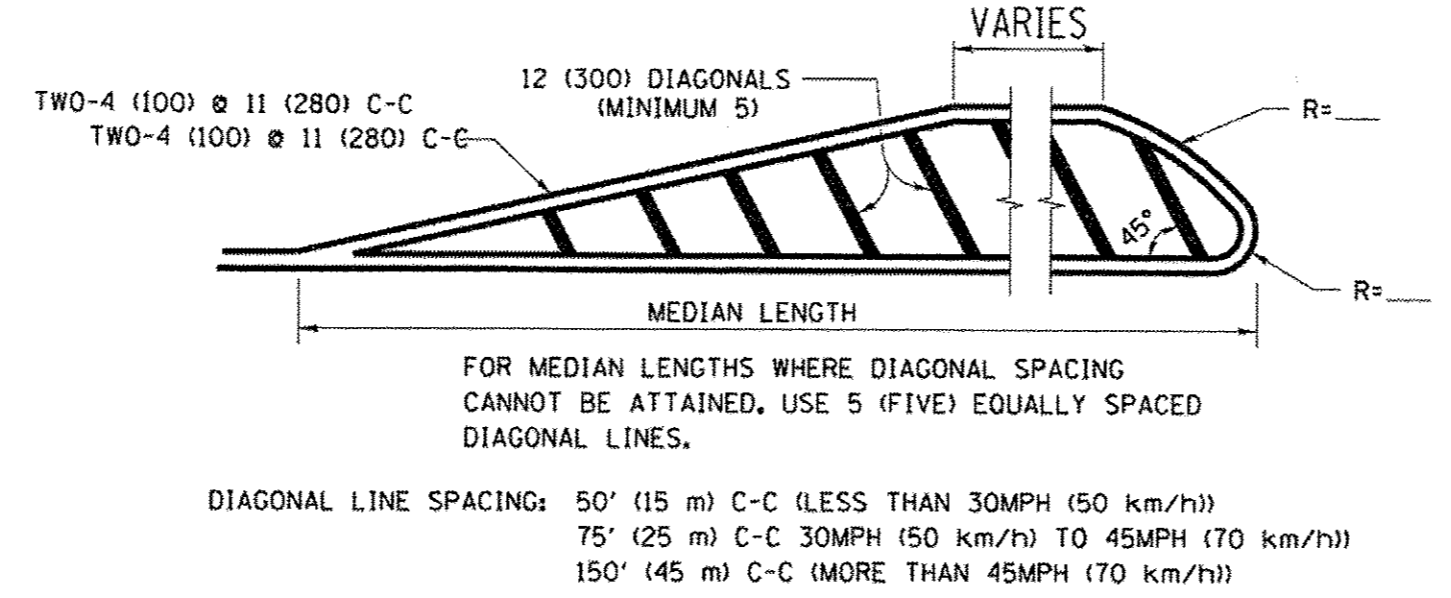


TYPICAL CROSSWALK MARKING

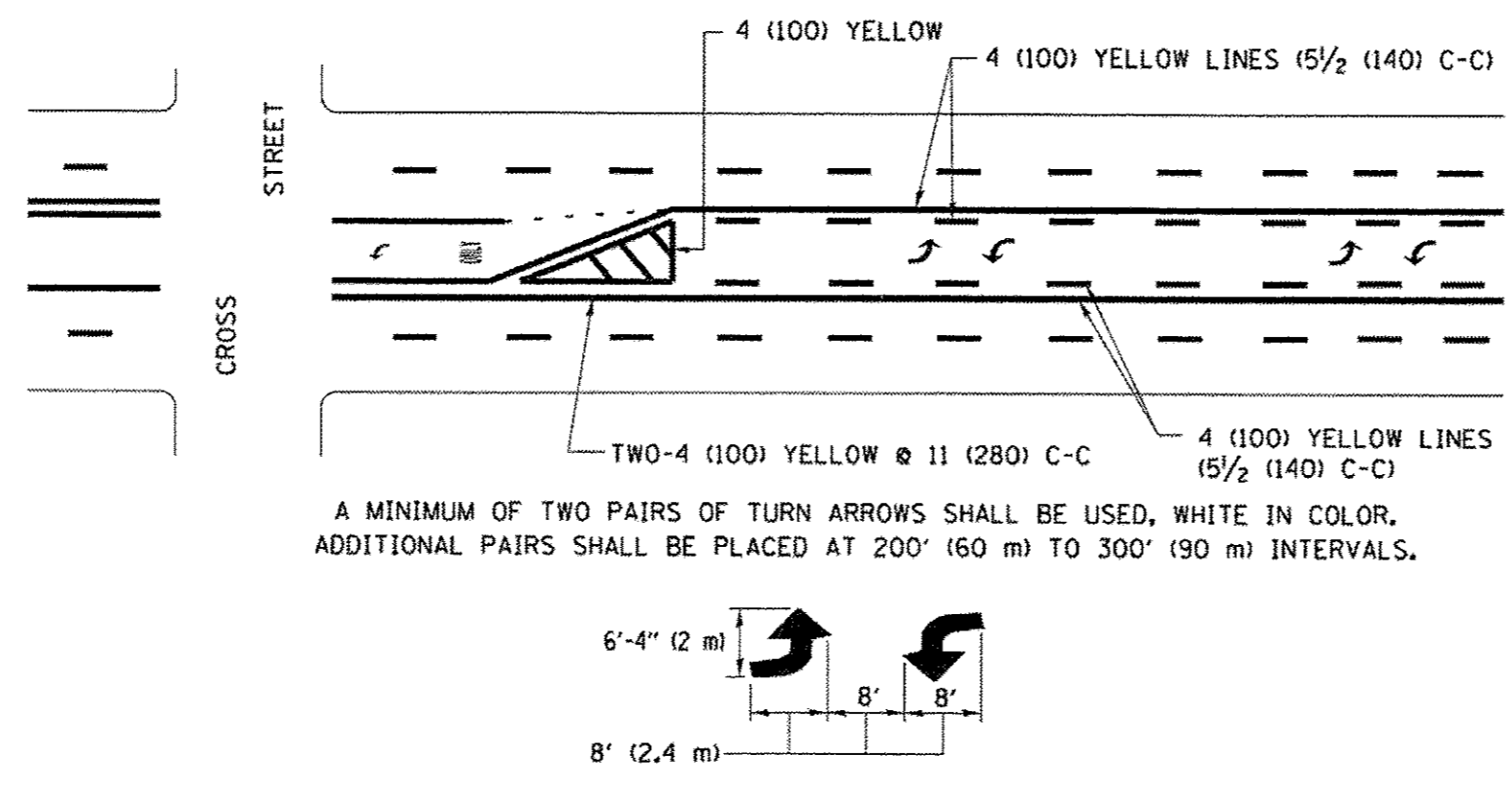
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



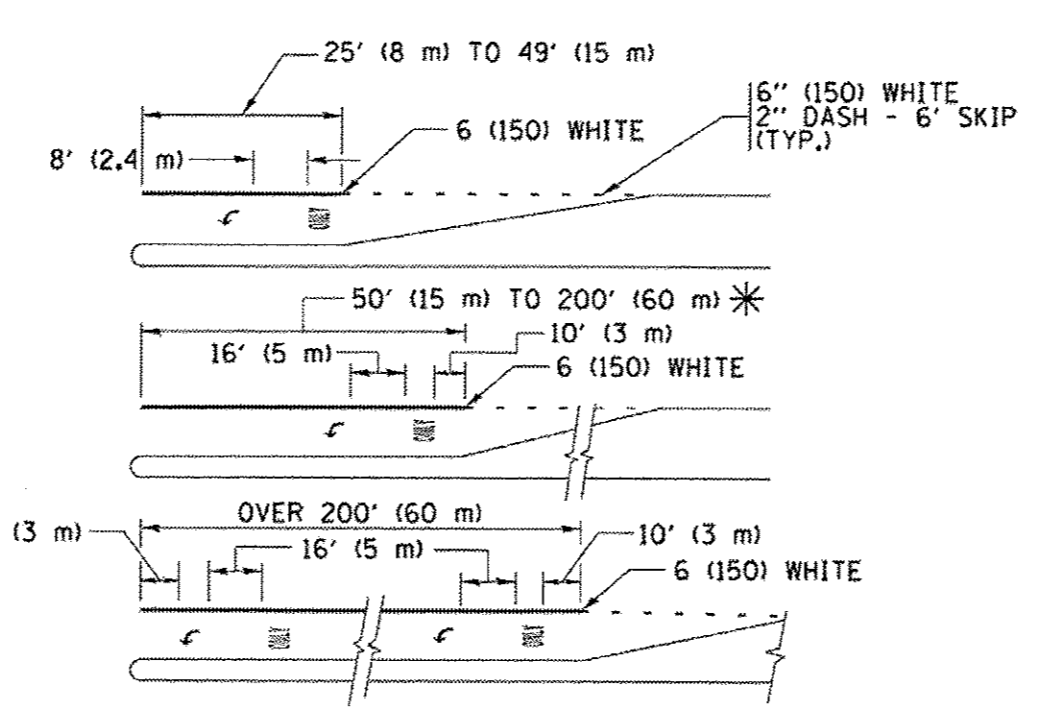
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE



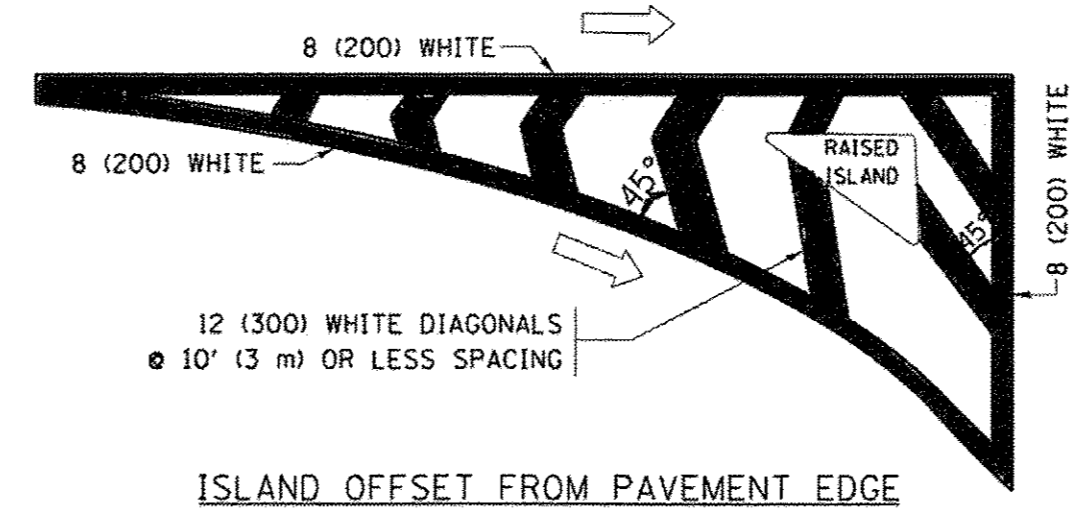
TYPICAL PAINTED MEDIAN MARKING



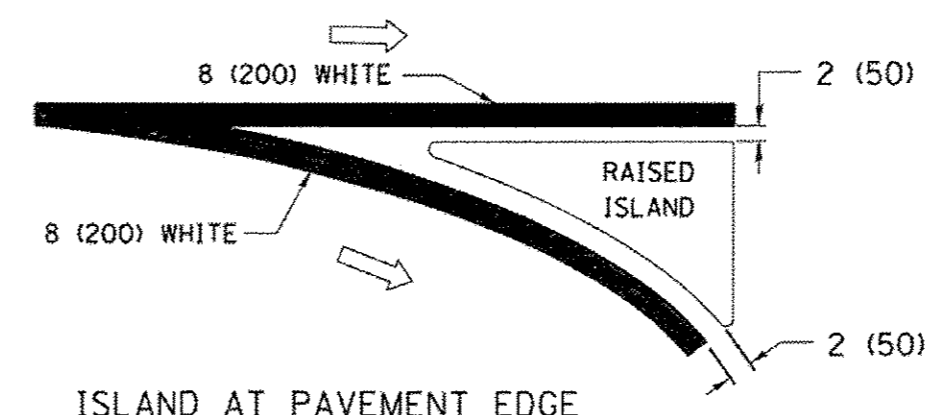
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

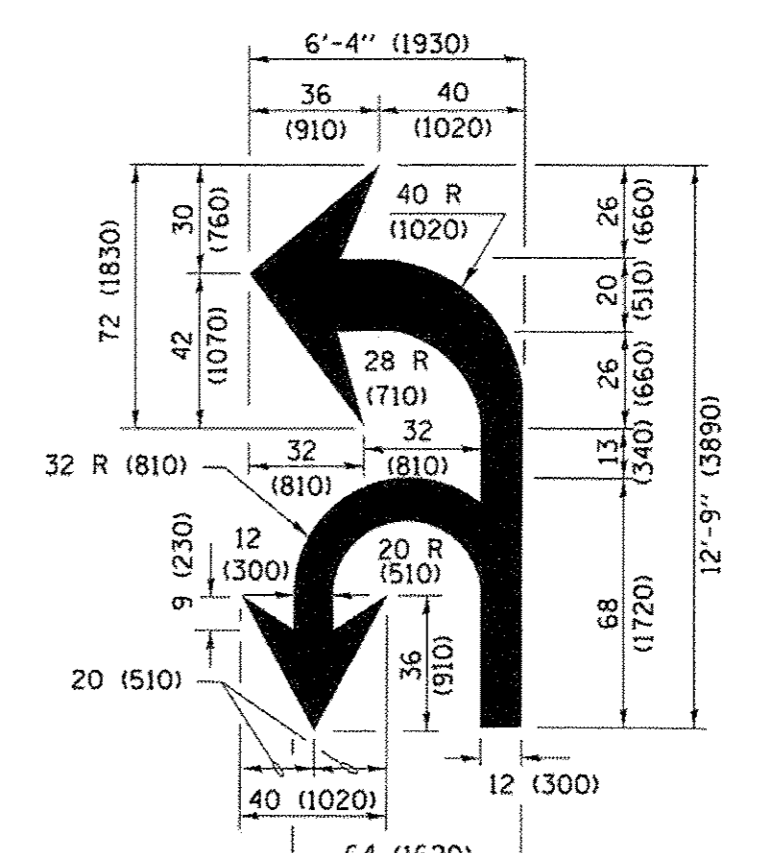
FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



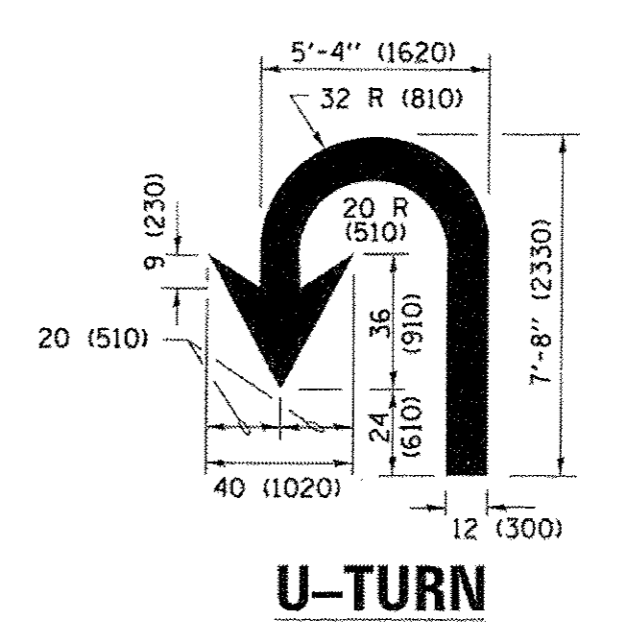
ISLAND OFFSET FROM PAVEMENT EDGE



TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

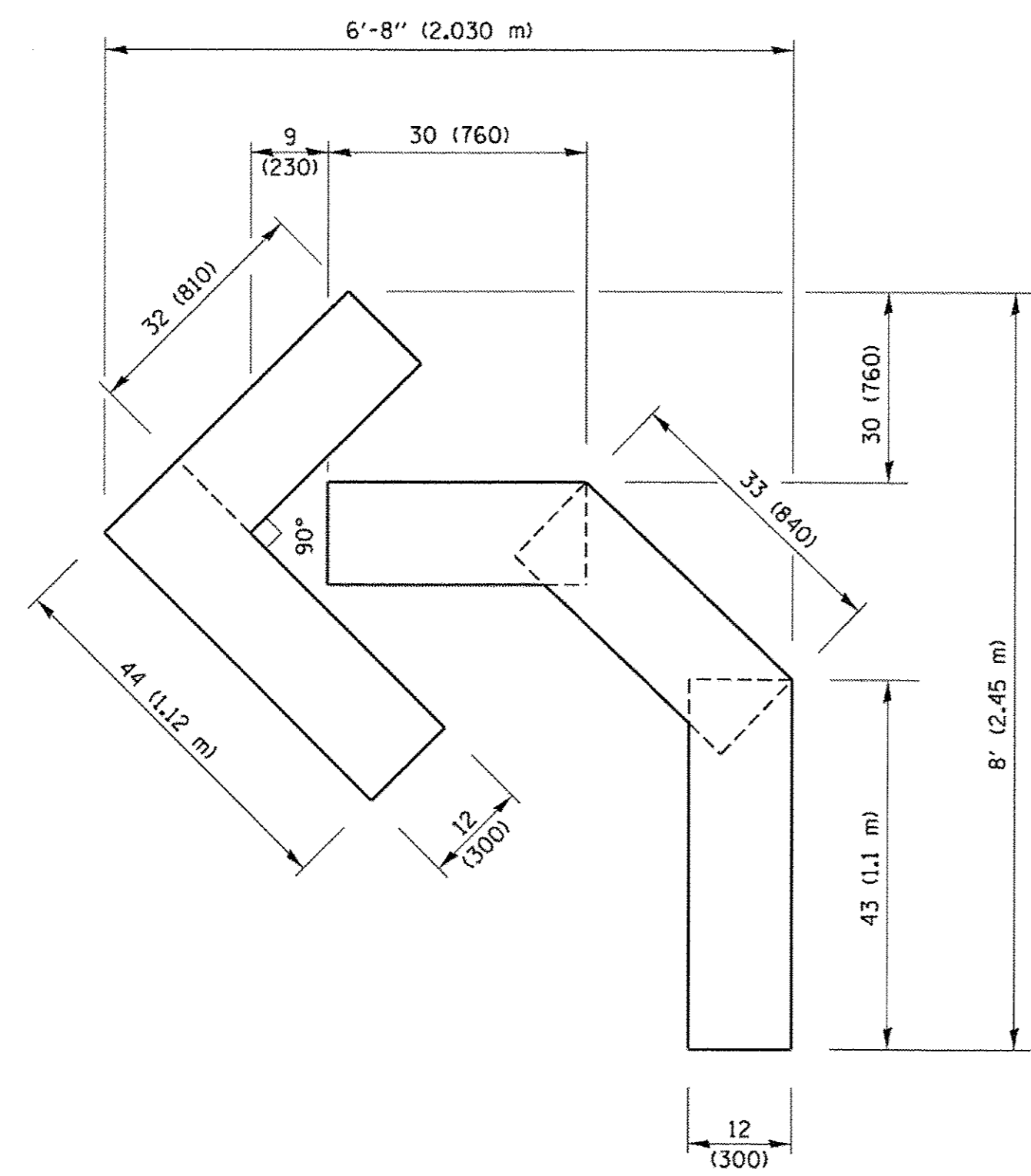
LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

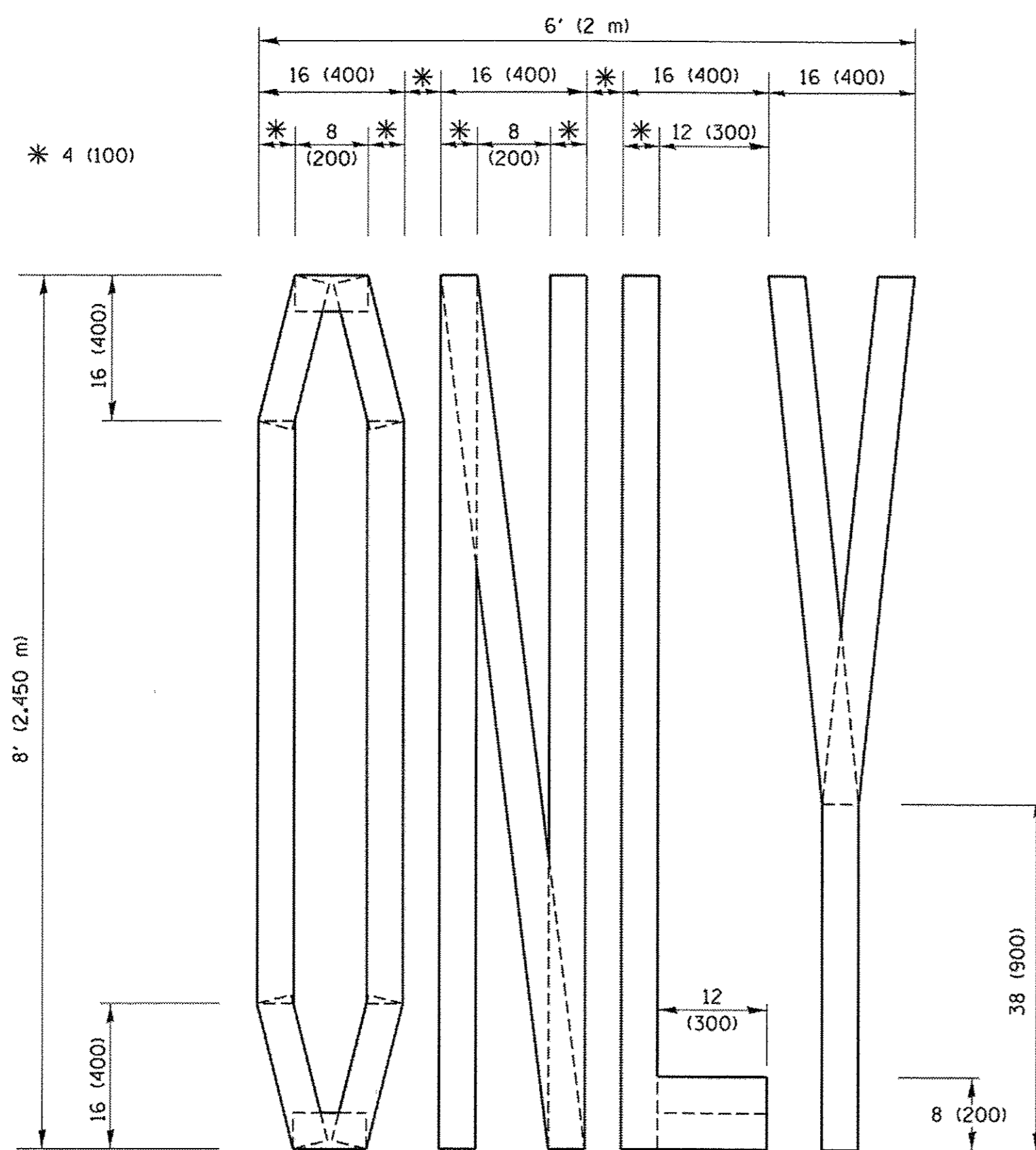
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE
FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100)	SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
(EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	5 (125) ON FREEWAYS	SKIP-DASH	WHITE	
DOTTED LINES	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID IN PAIRS	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW		WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN)	2 @ 6 (150)	SOLID	WHITE	NOT LESS THAN 6' (1.8 m) APART
A. DIAGONALS (BIKE & EQUESTRIAN)	12 (300) @ 45°	SOLID	WHITE	2' (600) APART
B. LONGITUDINAL BARS (SCHOOL)	12 (300) @ 90°	SOLID	WHITE	2' (600) APART
				SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

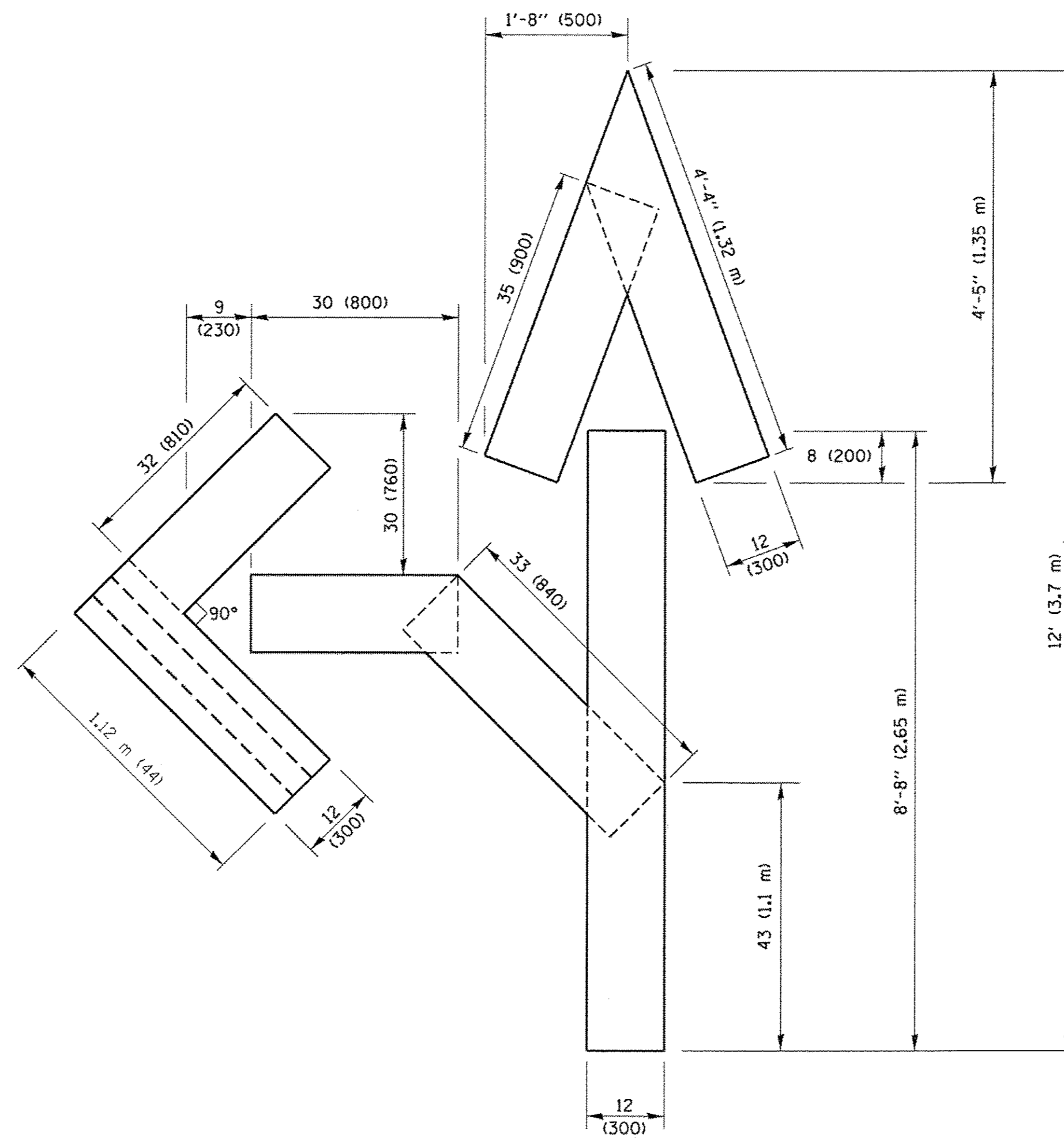
All dimensions are in inches (millimeters) unless otherwise shown.



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)



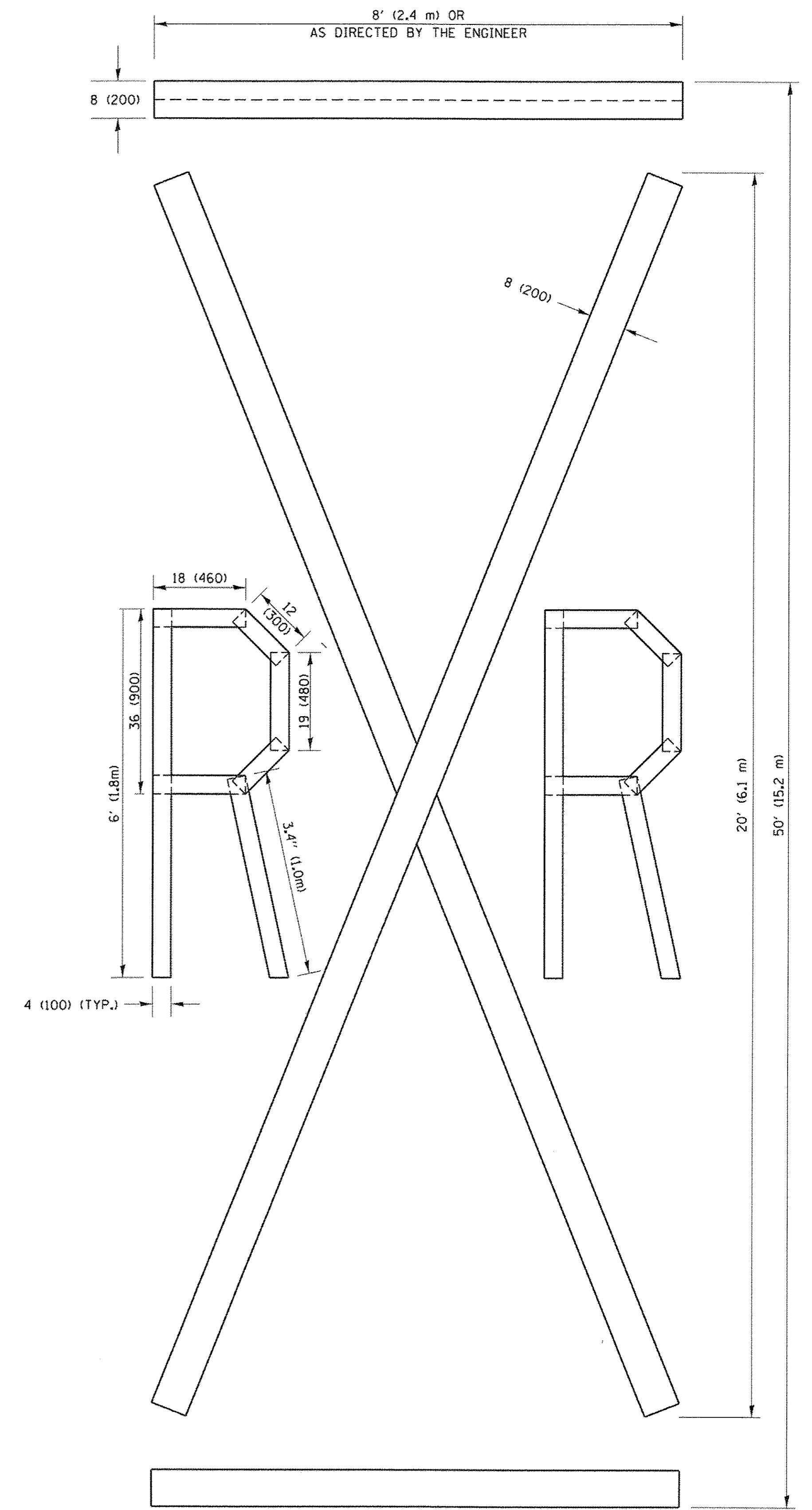
QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

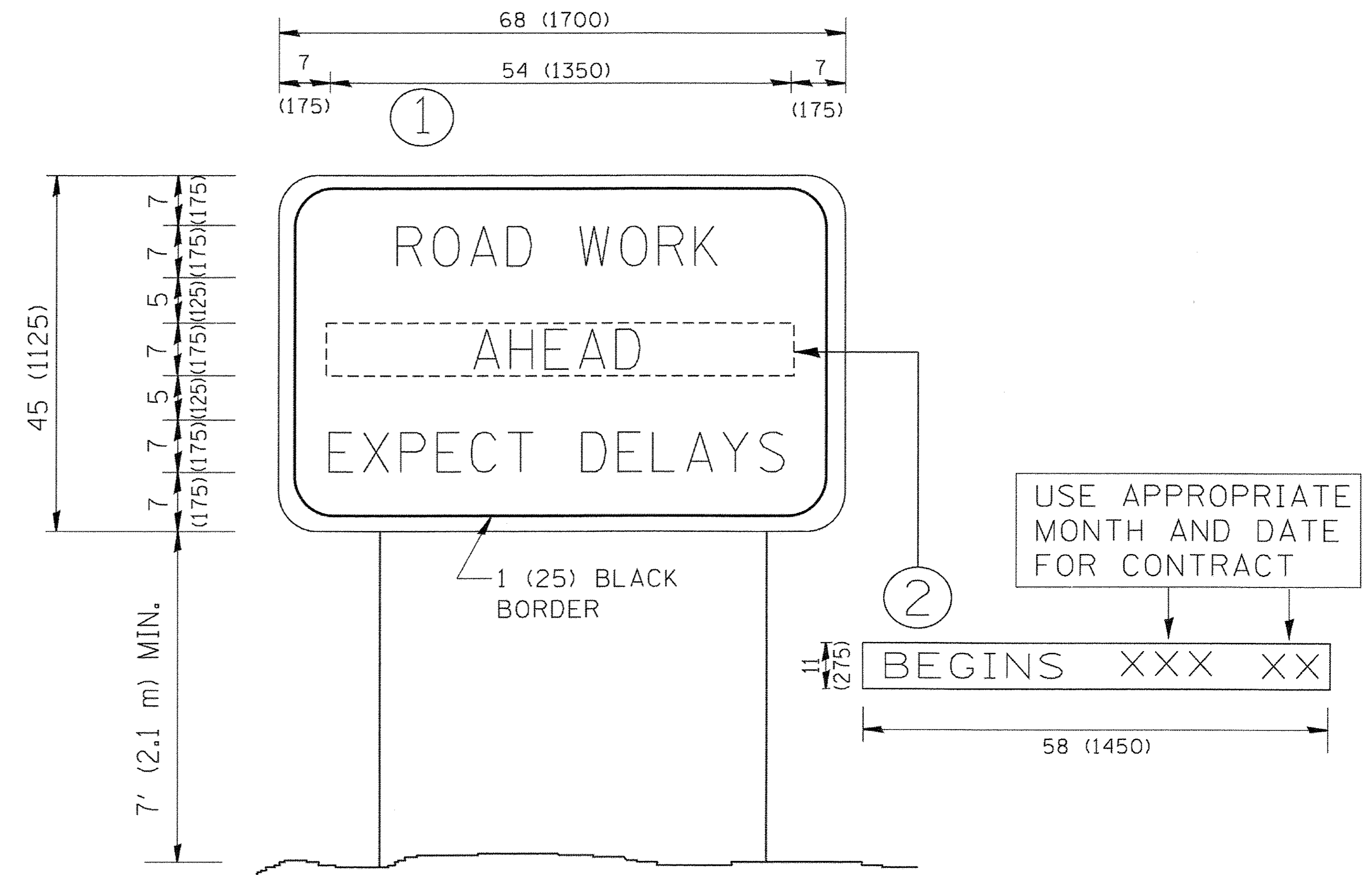
FILE NAME =	USER NAME = Footen.j	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
pw:\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\Dist 1\DRAWN\CADData\CADsheets\tcl6.dgn		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2612	16-00110-00-RS	DU PAGE	21	18
TC-16		CONTRACT NO.	61D55	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

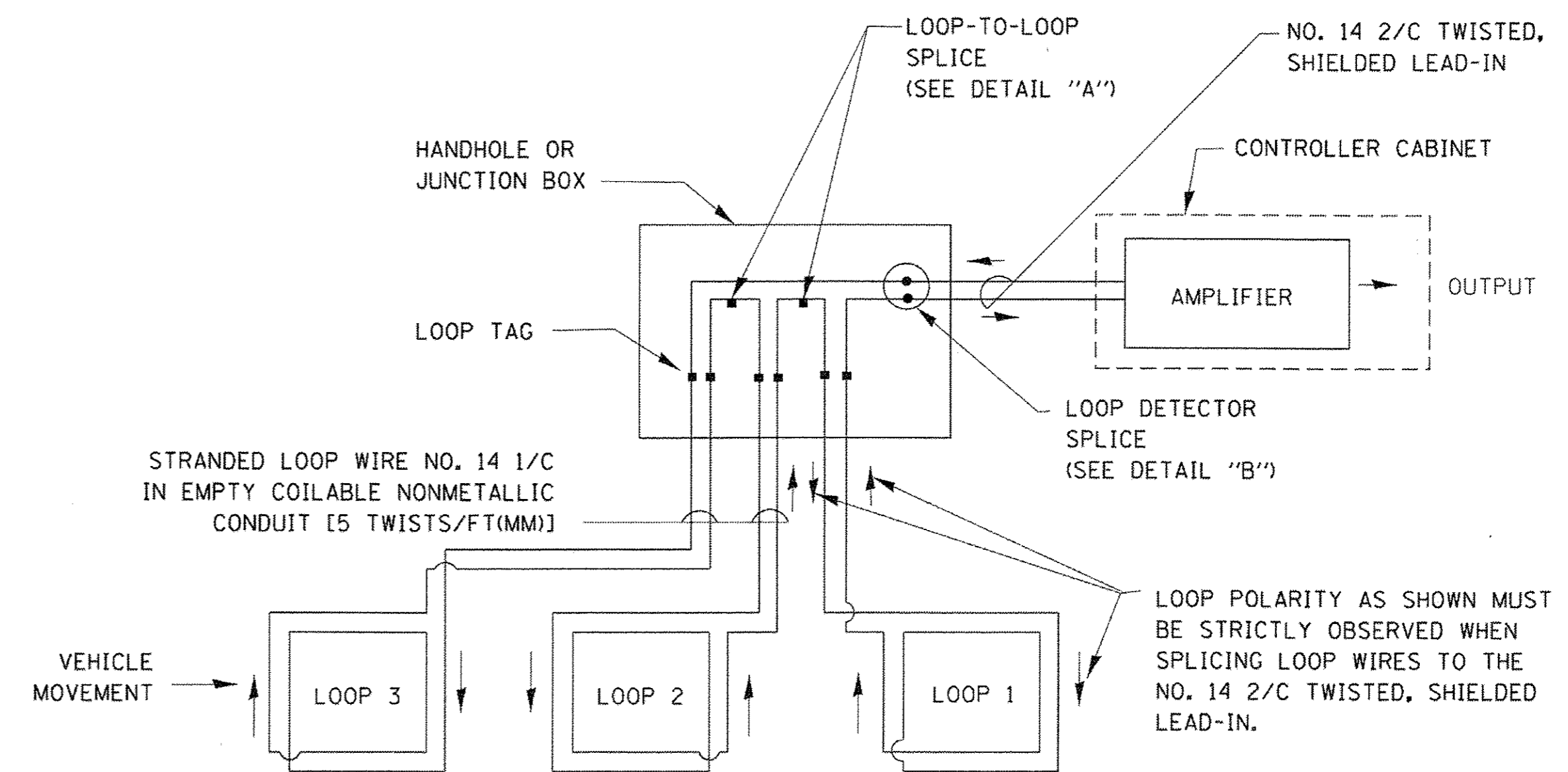
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dist\td\22x34\tc22.dgn	USER NAME = geglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - R. MIRS 12-11-97					2590	16-00111-00-RS	DU PAGE	21	19
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - T. RAMMACHER 02-02-99					TC-22		CONTRACT NO.		61D55
		DATE -	REVISED - C. JUCIUS 01-31-07					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

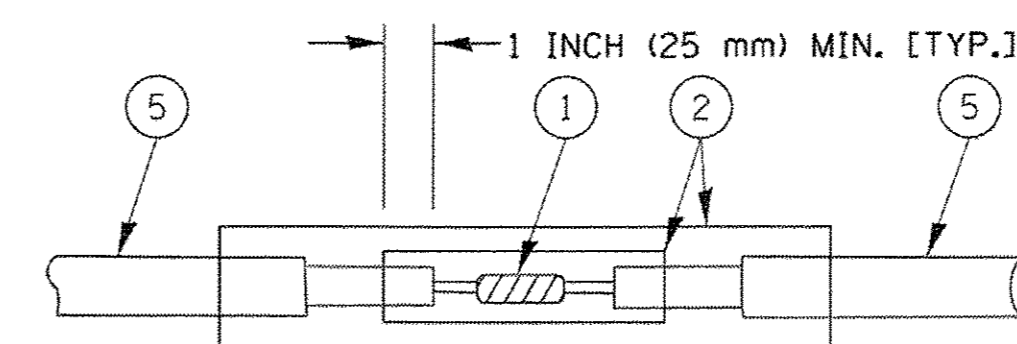
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

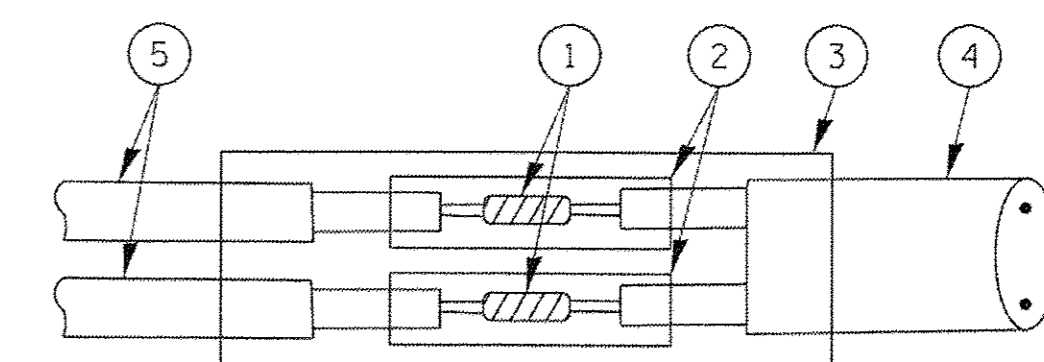


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

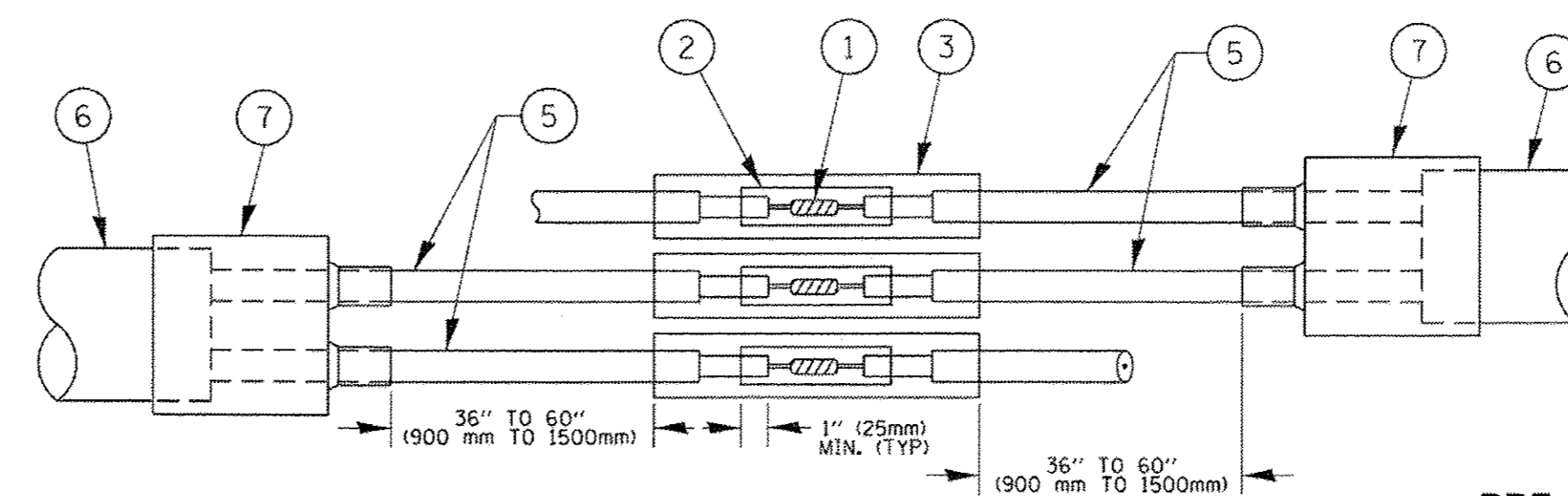


DETAIL "A"
LOOP-TO-LOOP SPLICE

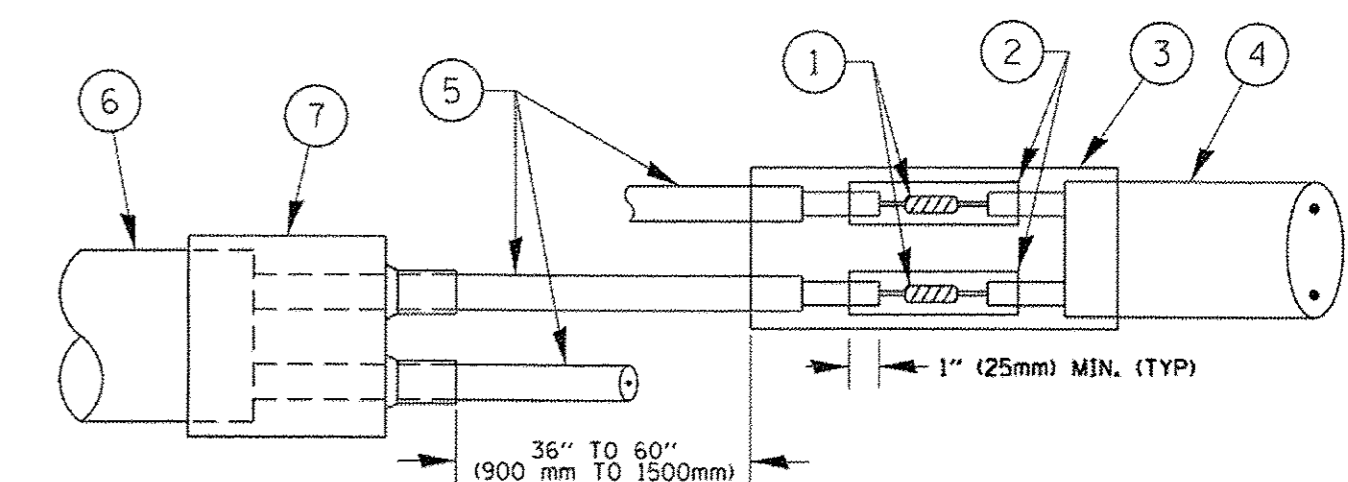


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE

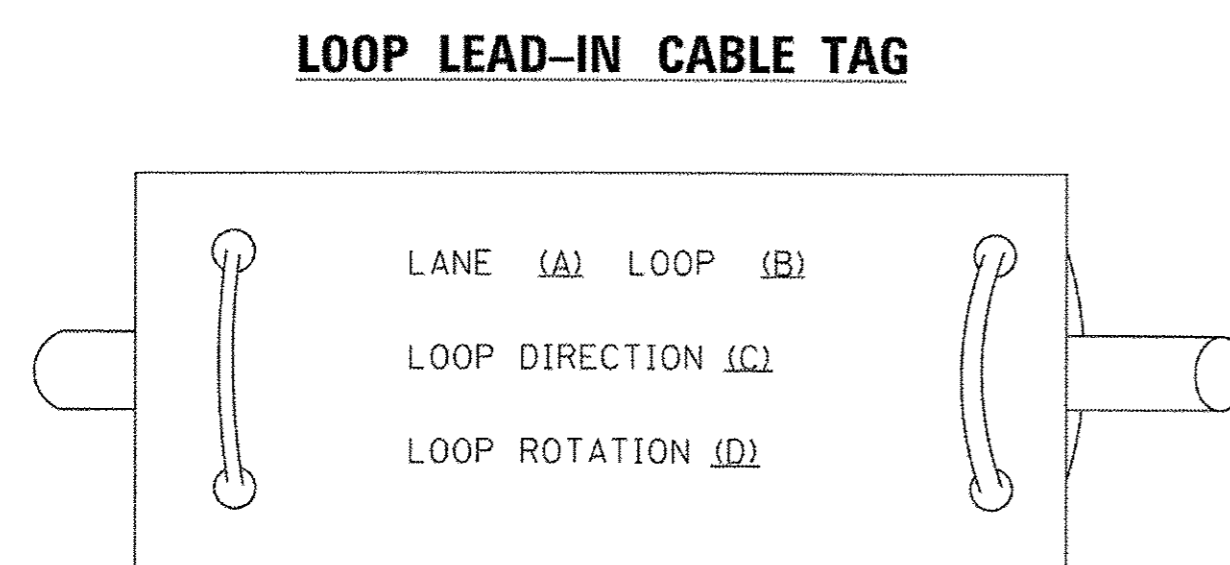


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

LOOP DETECTOR SPLICE

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- ⑥ PRE-FORMED LOOP
- ⑦ XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

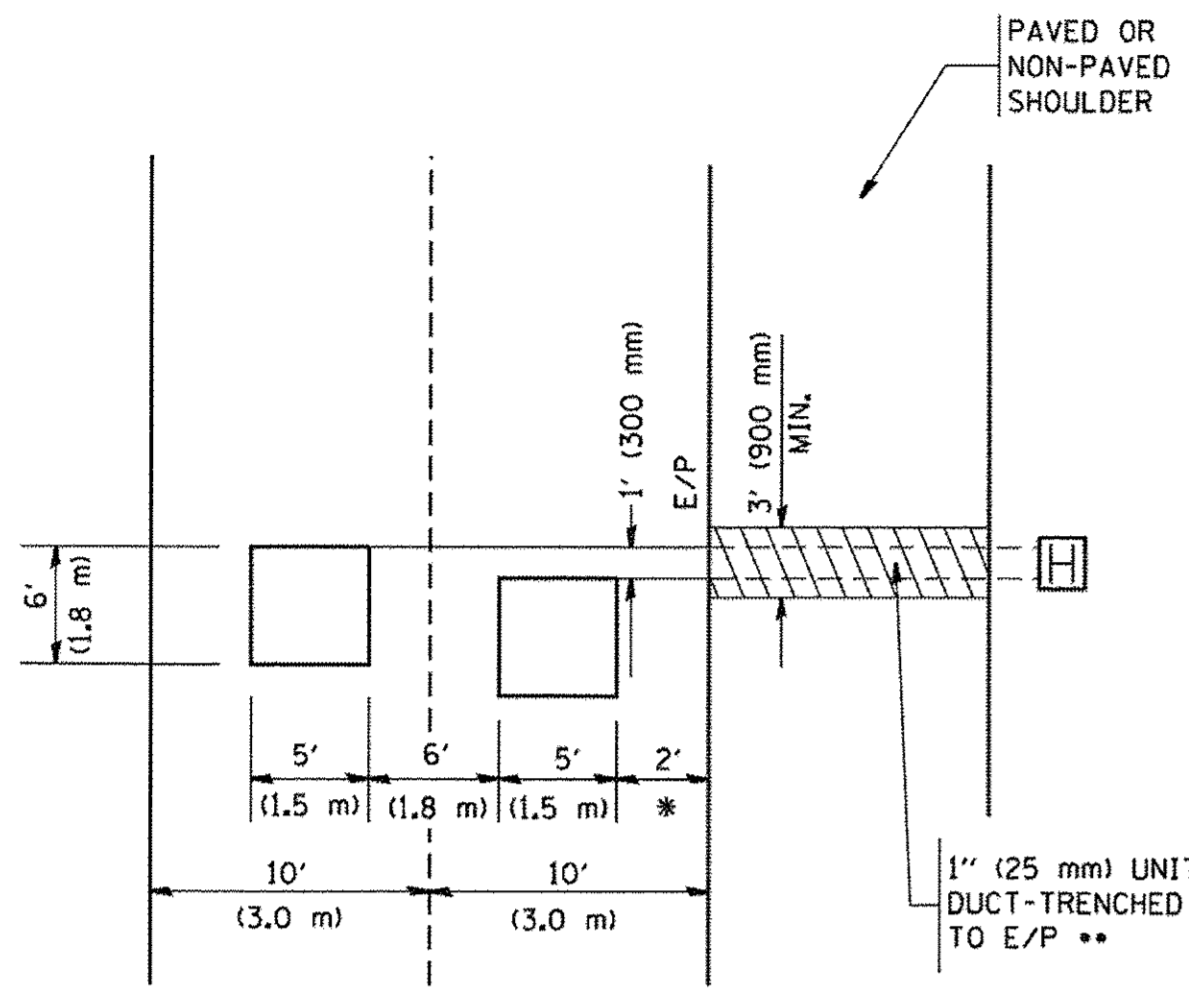


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

FILE NAME = c:\pwork\pindat\footem\0108315\ts05.dgn	USER NAME = footemj	DESIGNED - DAD	REVISED - DAG 1-1-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A.U. RTE. 2612	SECTION 16-00110-00-RS	COUNTY DU PAGE	TOTAL SHEETS 21	SHEET NO. 20
PLOT SCALE = 50.0000' / in.	CHECKED - DAD	DATE - 10-28-09	REVISED -		SCALE: NONE	SHEET NO. 2 OF 7 SHEETS	STA. TO STA.	TS-05		CONTRACT NO. 61D55		
PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



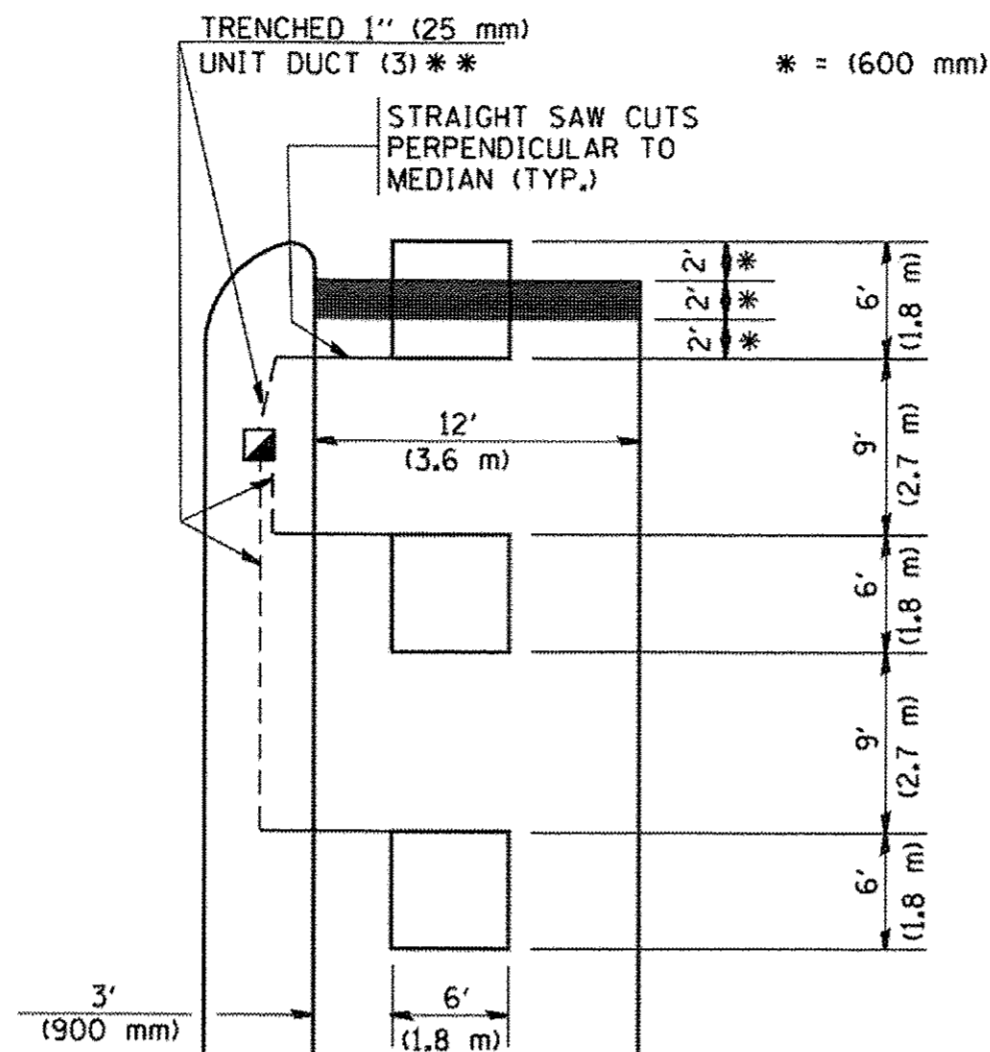
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

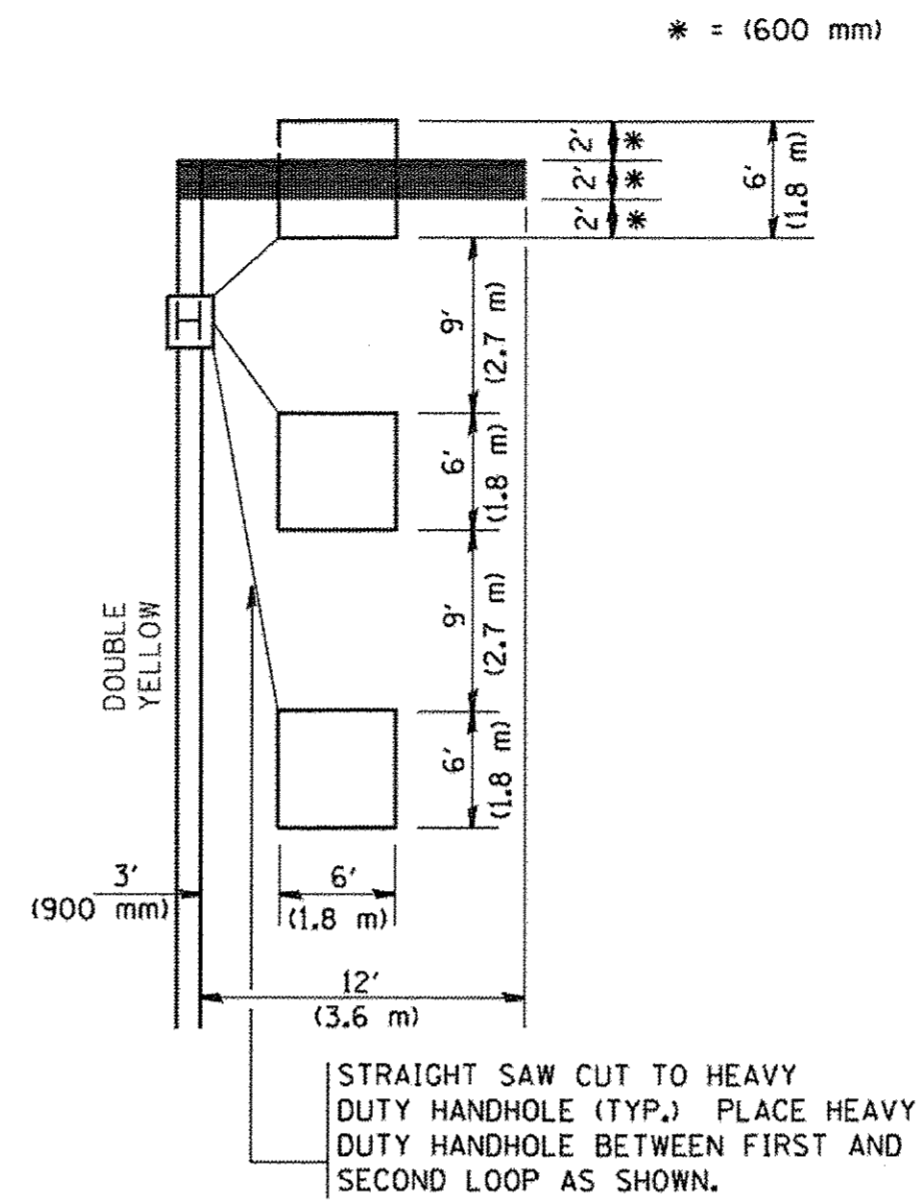


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

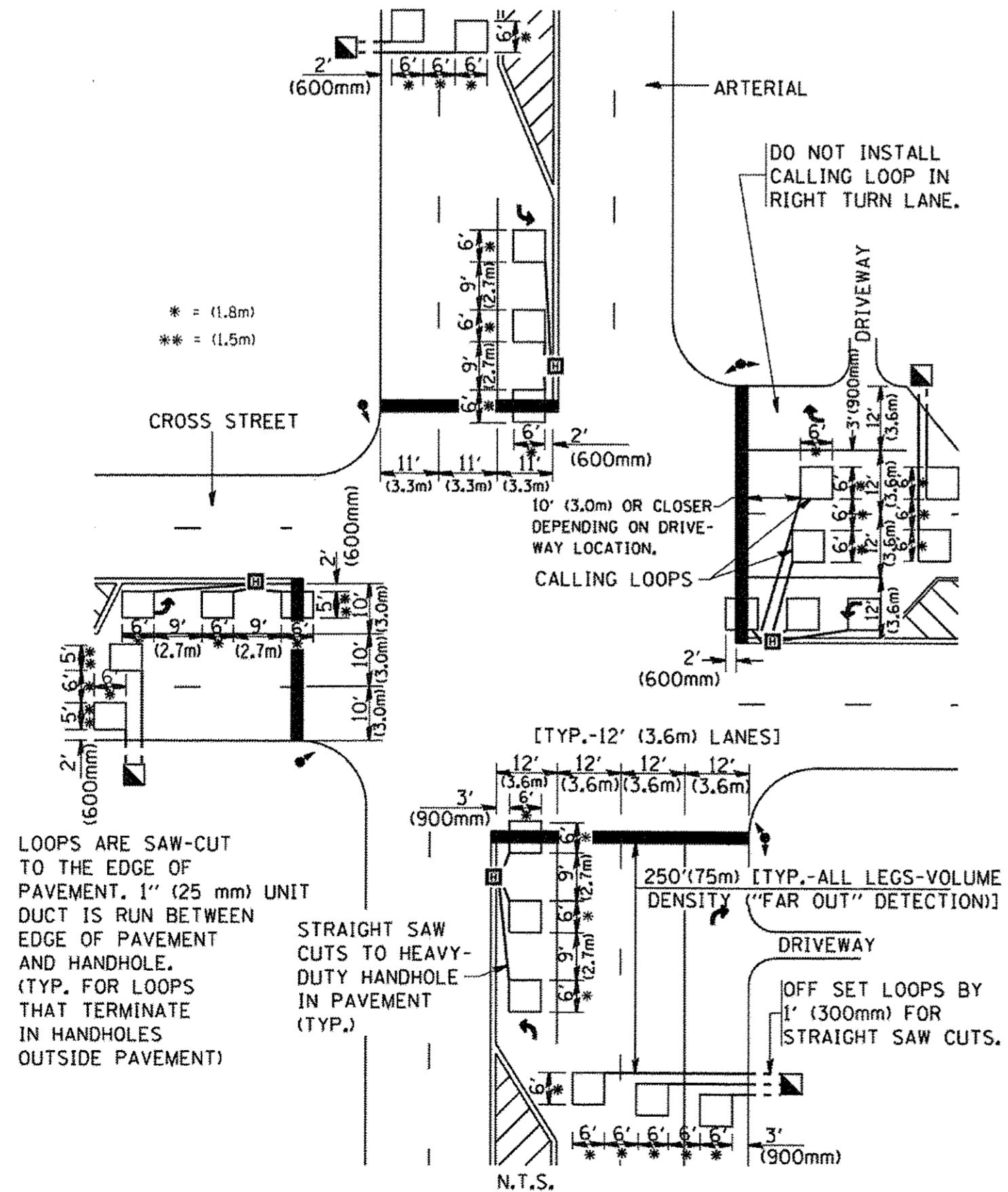
**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



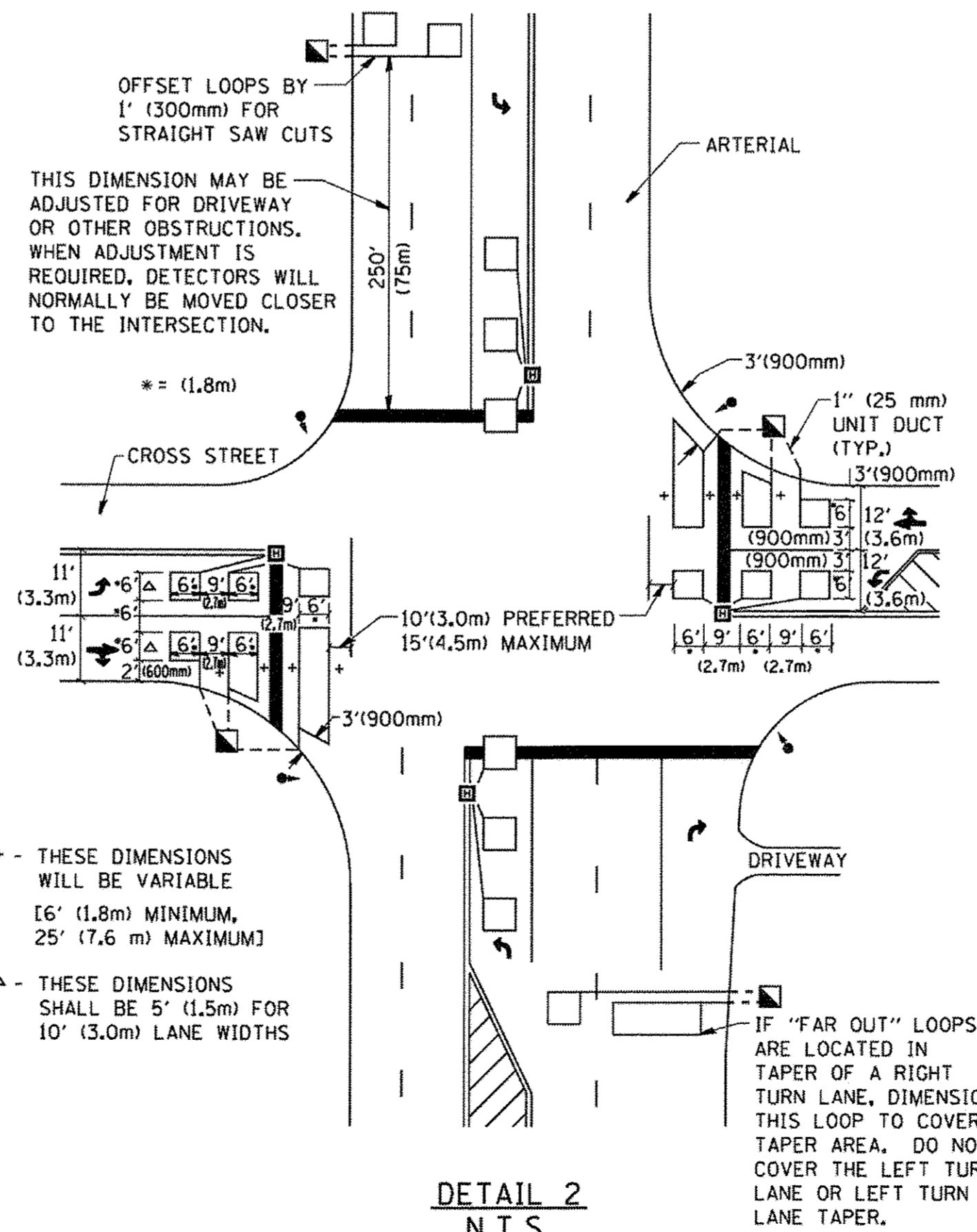
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\dist\td\22x34\td07.dgn	USER NAME = gqglanobt	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 50,0000' / IN.	CHECKED - R.K.F.	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	2612	16-00110-00-RS	DU PAGE	21	21
	PLOT DATE = 1/4/2008	DATE -	REVISED -					TS-07		CONTRACT NO.	61D55	
								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				