

FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, PE (847) 705-4021, SCHAUMBURG, IL

**FOR INDEX OF SHEETS,  
SEE SHEET NO. 2**

FOR UNDERGROUND UTILITY  
LOCATIONS CALL  
**J.U.L.I.E.**  
CALL 811  
Call Before You Dig OR TOLL FREE (800) 892-0123  
illinois1call.com

**TRAFFIC DATA**  
POSTED SPEED - 30 MPH  
DESIGN SPEED - 30 MPH  
2015 ADT = 20100  
MINOR ARTERIAL

# STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS

## PLANS FOR PROPOSED FEDERAL AID HIGHWAY

03-04-2016 LETTING ITEM 086

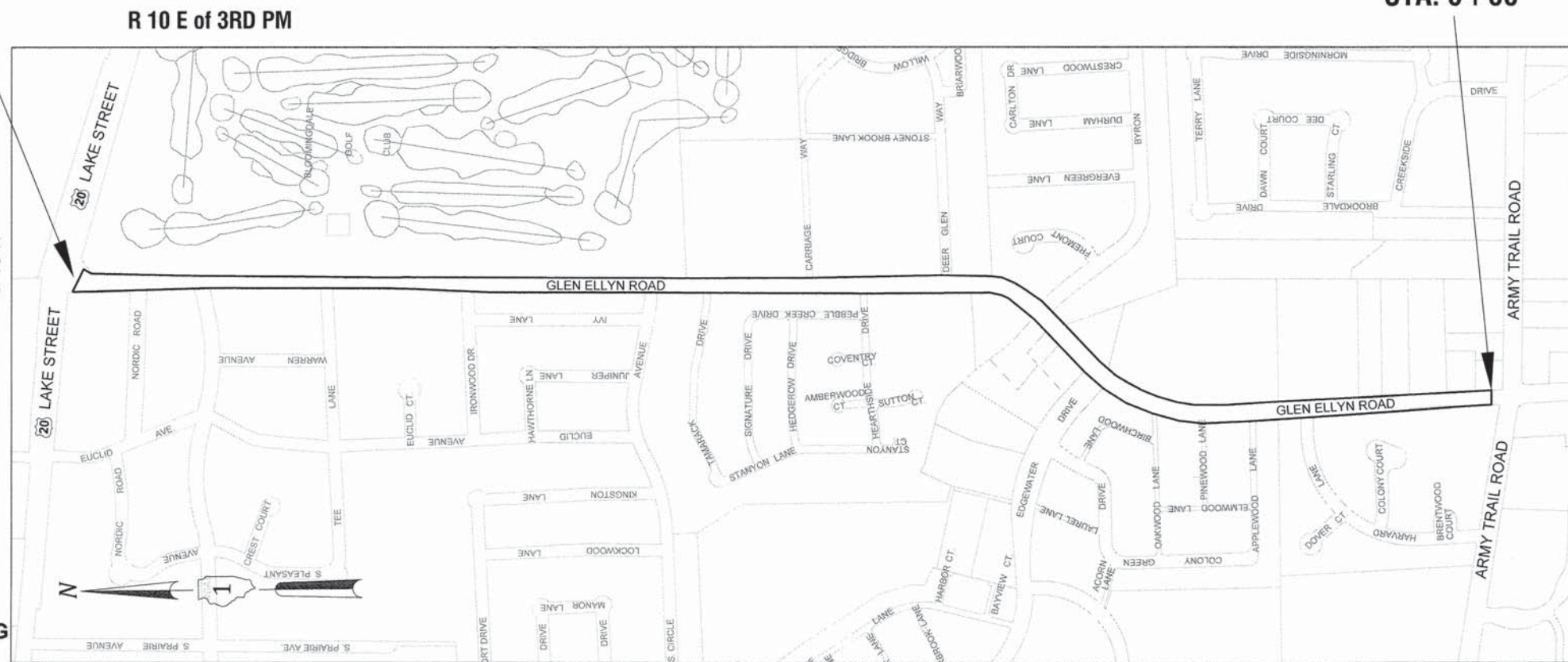
FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2581	14-00062-00-RS	DUPAGE	14	1
		ILLINOIS	CONTRACT NO. 61C23	

**FAU ROUTE 2581 (GLEN ELLYN ROAD)  
LAKE STREET (US 20) TO ARMY TRAIL ROAD  
RESURFACING  
SECTION NO. 14-00062-00-RS  
PROJECT NO. M-4003(345)  
VILLAGE OF BLOOMINGDALE  
DUPAGE COUNTY  
C-91-359-14**



**END IMPROVEMENT  
STA. 79+12**

**BEGIN IMPROVEMENT  
STA. 0+00**



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZE PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED SIZE PLANS, THE ABOVE SCALE MAY BE USED.

**GROSS AND NET LENGTH = 1.5 MILES = 7912 FT**

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

APPROVED 12-18 20 15  
*B.P. Sin*  
VILLAGE OF BLOOMINGDALE

PASSED 12-24 20 15  
*C. J. H. H. H.*  
DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID DECEMBER 30, 20 15  
BASED ON LIMITED REVIEW  
*J. K. K.*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

**PROFESSIONAL ENGINEER'S SIGN & SEAL**

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

**CONTRACT NO. 61C23**

**GENERAL NOTES**

- ALL REFERENCES TO THE "VILLAGE" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE VILLAGE OF BLOOMINGDALE.
- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION," ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION ON JANUARY 1, 2012.
- ALL CURB & GUTTER AND SIDEWALK REMOVAL AND REPLACEMENT SECTIONS ARE TO BE DETERMINED IN THE FIELD BY THE ENGINEER.
- PAVEMENT GRADES. THE ELEVATIONS SHOWN ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT, UNLESS OTHERWISE INDICATED.
- PUBLIC OR PRIVATE UTILITIES. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE VILLAGE AND ITS ENGINEER DO NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING HIS CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM. IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 105.07 OF THE "STANDARD SPECIFICATIONS" THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE "STANDARD SPECIFICATIONS".
- LOCATIONS OF DRAINAGE STRUCTURES. THE STATION/OFFSET/ELEVATION NOTED FOR ALL PROPOSED DRAINAGE STRUCTURES LOCATED IN THE CURB LINE REFER TO THE POSITION OF THE ADJACENT PROPOSED EDGE OF PAVEMENT. THE CONTRACTOR WILL BE RESPONSIBLE FOR DETERMINING THE OFFSET NECESSARY FOR EACH STRUCTURE TO SET THE FRAME IN ITS PROPER LOCATION. ALL OTHER STRUCTURE OFFSETS ARE TO THE CENTER OF STRUCTURE.
- TOP OF FRAME ELEVATIONS. PROPOSED TOP OF FRAME (T/F) ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF EACH STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATIONS OF THE AREAS IN WHICH THEY ARE LOCATED, AS PART OF THE COST OF THE STRUCTURE.
- DAMAGE TO SEWER AND WATER SERVICES. ALL SEWER AND WATER SERVICES CROSSED BY NEW STORM SEWERS SHALL BE PROPERLY LOCATED AND PROTECTED DURING CONSTRUCTION. ANY DAMAGE TO SAID SEWERS NOT CONSIDERED TO BE IN CONFLICT WITH THE PROPOSED STORM SEWER SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.
- CONCRETE BREAKERS. WHEN REMOVING CURB, CURB AND GUTTER, PAVEMENT, SIDEWALK OR ANY OTHER STRUCTURE, THE CONTRACTOR SHALL TAKE EVERY PRECAUTION NECESSARY TO ENSURE HE WILL NOT UNDERGROUND PUBLIC OR PRIVATE UTILITIES. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL CONCRETE BREAKER BE ALLOWED.
- DISPOSAL OF SURPLUS MATERIAL. THE CONTRACTOR IS PROHIBITED FROM BURNING ANY MATERIAL WITHIN OR ADJACENT TO THE IMPROVEMENT. ALL EXCESS OR WASTE MATERIAL SHALL EITHER BE HAULED AWAY FROM THE SITE OF THE IMPROVEMENT BY THE CONTRACTOR AND DEPOSITED AT LOCATIONS PROVIDED BY HIM, OR DISPOSED OF WITHIN THE RIGHT OF WAY IN A MANNER OTHER THAN BURNING, SUBJECT TO THE APPROVAL OF THE ENGINEER. NO EXTRA COMPENSATION WILL BE ALLOWED THE CONTRACTOR FOR ANY EXPENSE INCURRED BY COMPLYING WITH THIS REQUIREMENT.
- ACCESS TO ABUTTING PROPERTY. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ABUTTING PROPERTY DURING THE CONSTRUCTION OF THIS PROJECT EXCEPT FOR PERIODS OF SHORT DURATION, AS APPROVED BY THE ENGINEER.
- PROTECTIVE COAT. PROTECTIVE COAT SHALL BE APPLIED TO ALL GUTTER FLAGS, FACES AND TOPS OF CURBS, SIDEWALKS, AND P.C.C. DRIVEWAYS.
- DEPRESSED CURB. PROPOSED CURB SHALL BE DEPRESSED AT ALL SIDEWALK AND DRIVEWAY LOCATIONS AS DETERMINED BY THE ENGINEER.

**GENERAL NOTES**

- SAWING ASPHALT OR CONCRETE FOR REMOVAL ITEMS. THE WORK SHALL CONSIST OF SAWING JOINTS IN THE EXISTING ROADWAY, HMA SURFACE, CURB AND GUTTER AND SIDEWALK IN ORDER TO SEPARATE THOSE PORTIONS TO BE REMOVED FROM THOSE WHICH WILL REMAIN IN PLACE. THIS WORK SHALL BE PERFORMED AT THE AT THE LOCATIONS SPECIFIED IN THE PLANS AND/OR AS OTHERWISE DESIGNATED BY THE ENGINEER. IN AREAS OF FULL DEPTH REMOVAL, SAW CUTS SHALL ALSO BE FULL DEPTH. THE CONTRACTOR WILL BE REQUIRED TO SAW VERTICAL CUTS SO AS TO FORM CLEAN VERTICAL JOINTS. SHOULD THE CONTRACTOR DEFACE ANY EDGE, A NEW SAWED JOINT SHALL BE PROVIDED AND ANY ADDITIONAL WORK, INCLUDING REMOVAL AND REPLACEMENT WILL BE DONE AT THE CONTRACTORS EXPENSE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE THICKNESS OF EXISTING PAVEMENT AND WETHER OR NOT IT CONTAINS REINFORCEMENT. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE REMOVAL ITEMS FOR WHICH THE SAWING IS REQUIRED.
- RESPONSIBILITY FOR VANDALISM. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DEFAACEMENT OF ANY CONCRETE PLACEMENT BEFORE SAID CONCRETE HAS SET UP. ANY CONCRETE THAT HAS BEEN DEFACED, IN THE OPINION OF THE ENGINEER, SHALL BE REPAIRED OR REMOVED AND REPLACED BY THE CONTRACTOR AT THEIR EXPENSE.
- WATER FOR CONSTRUCTION & USE OF FIRE HYDRANTS. ANY USE OR ATTEMPT TO ACCESS A FIRE HYDRANT WITHIN THE VILLAGE WITHOUT THE ENGINEERS CONSENT IS STRICTLY PROHIBITED. WATER MAY BE OBTAINED USING A HYDRANT METER AND AN ACCOUNT SHALL BE ARRANGED WITH THE VILLAGE SERVICES DEPARTMENT (630-893-7073) PRIOR TO OBTAINING WATER.

**INDEX OF SHEETS**

- 1 COVER SHEET
- 2 INDEX OF SHEETS, HIGHWAY STANDARDS & GENERAL NOTES
- 3 SUMMARY OF QUANTITIES
- 4 TYPICAL SECTIONS
- 5 OVERLAY & STRIPING STA. 0+00 TO 24+50
- 6 OVERLAY & STRIPING STA. 24+50 TO 54+00
- 7 OVERLAY & STRIPING STA. 54+00 TO 79+12
- 8-14 DISTRICT ONE DETAILS

**HIGHWAY STANDARDS**

- 000001-06 STANDARD SYMBOLS, ABB. & PATTERNS
- 424001-08 CURB RAMPS FOR SIDEWALKS
- 424006-02 DIAGONAL CURB RAMPS
- 424011-02 CORNER CURB RAMPS
- 424016-02 MIDBLOCK CURB RAMPS
- 424021-03 DEPRESSED CORNER
- 442201-03 CLASS D PATCHES
- 606001-06 CONCRETE CURB AND COMBINATION CONC. CURB AND GUTTER
- 701301-04 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701311-03 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
- 701501-06 URBAN LANE CLOSURE - UNDIVIDED
- 701701-09 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701801-05 SIDEWALK CORNER OR CROSSWALK CLOSURE
- 701901-04 TRAFFIC CONTROL DEVICES
- 780001-05 TYPICAL PAVEMENT MARKINGS

**DISTRICT ONE DETAILS**

- BD-08 FRAMES AND LIDS ADJUSTMENT WITH MILLING
- BD-22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
- BD-24 CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
- BD-32 BUTT JOINT AND HMA TAPER DETAILS
- TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
- TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
- TC-22 ARTERIAL ROAD INFORMATIONAL SIGN

BY: <b>VILLAGE OF BLOOMINGDALE</b> 201 S. Bloomingdale Road Bloomingdale, IL 60188 (630) 893-7000	DATE: 9-10-15 SCALE: nts DRAWN BY: 张坤 APPROVED BY:	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>INDEX OF SHEETS, HIGHWAY STANDARDS &amp; GENERAL NOTES</b>			FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
						2581	14-00062-00-RS	DUPAGE	14	2
						ILLINOIS		CONTRACT NO. 61C23		

## SUMMARY OF QUANTITIES

Specialty Item	Code No.	Pay Item	Unit	Total Quantity	CONSTRUCTION CODE
	25200100	SODDING	SQ YD	1200	0005 1200
	25200200	SUPPLEMENTAL WATERING	UNIT	20	20
	40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	20440	20440
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	52	52
	40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1930	1930
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	772	772
	40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	2895	2895
	42001300	PROTECTIVE COAT	SQ YD	1220	1220
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	4000	4000
	42400800	DETECTABLE WARNINGS	SQ FT	200	200
	44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	33698	33698
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	3500	3500
	44000600	SIDEWALK REMOVAL	SQ FT	4000	4000
	44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	695	695
	44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	695	695
	60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	7	7
	60608562	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12	FOOT	3500	3500
	67100100	MOBILIZATION	LSUM	1	1
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LSUM	1	1
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	16250	16250
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	375	375
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	17000	17000
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2650	2650
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	475	475
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	320	320
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	15	15
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	52	52

PREPARED BY:



**VILLAGE OF BLOOMINGDALE**

201 S. Bloomingdale Road  
Bloomingdale, IL 60188  
(630) 893-7000

DATE: 9-10-15

SCALE: nts

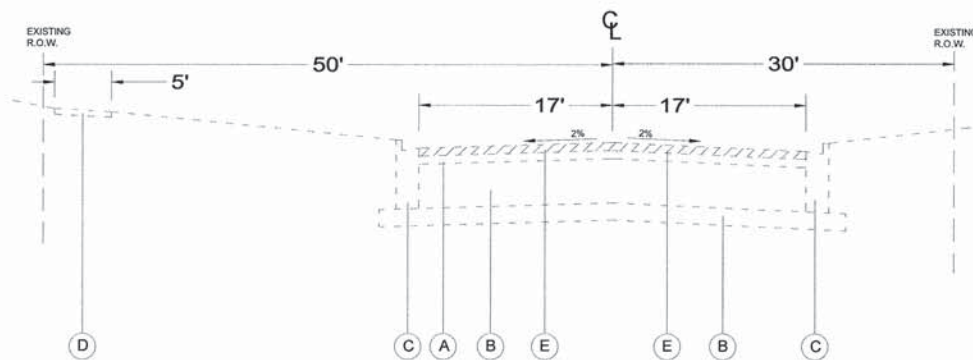
DRAWN BY: [Signature]

APPROVED BY:

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2581	14-00062-00-RS	DUPAGE	14	3
		ILLINOIS	<b>CONTRACT NO. 61C23</b>	

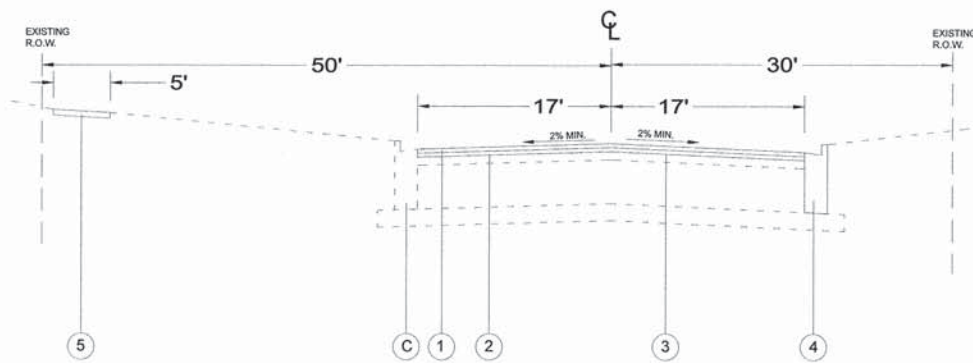


**EXISTING TYPICAL SECTION**

GLEN ELLYN ROAD  
STA. 0+00 to STA. 79+20

**LEGEND - EXISTNG**

- (A) EXISTING HMA SURFACE & BINDER, 6"
- (B) EXISTING GRANULAR SUBBASE
- (C) EXISTING CONCRETE CURB AND GUTTER
- (D) EXISTING PCC SIDEWALK
- (E) HMA SURFACE REMOVAL



**PROPOSED TYPICAL SECTION**

GLEN ELLYN ROAD  
STA. 0+00 to STA. 79+20

**LEGEND - PROPOSED**

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1.5"
- (2) POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 1"
- (3) BITUMINOUS MATERIALS (PRIME COAT)
- (4) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, (AS NEEDED - LOCATIONS DETERMINED BY THE ENGINEER)
- (5) SIDEWALK REMOVAL AND PCC SIDEWALK, 5" (AS NEEDED - LOCATIONS DETERMINED BY THE ENGINEER)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
<b>PAVEMENT RESURFACING</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL - 9.5mm), 1.5"	4% @ 70 Gyr.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"	3.5% @ 50 Gyr.
<b>PATCHING</b>	
CLASS D PATCH (HMA BINDER IL-19 mm)	4% @ 70 Gyr.

THE UNIT WEIGHT USED TO CALCULATED ALL HOT-MIX SURFACE QUANTITIES IS 112 LBS./SQ. YD./IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA, THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS, SEE SPECIAL PROVISIONS.

PREPARED BY:



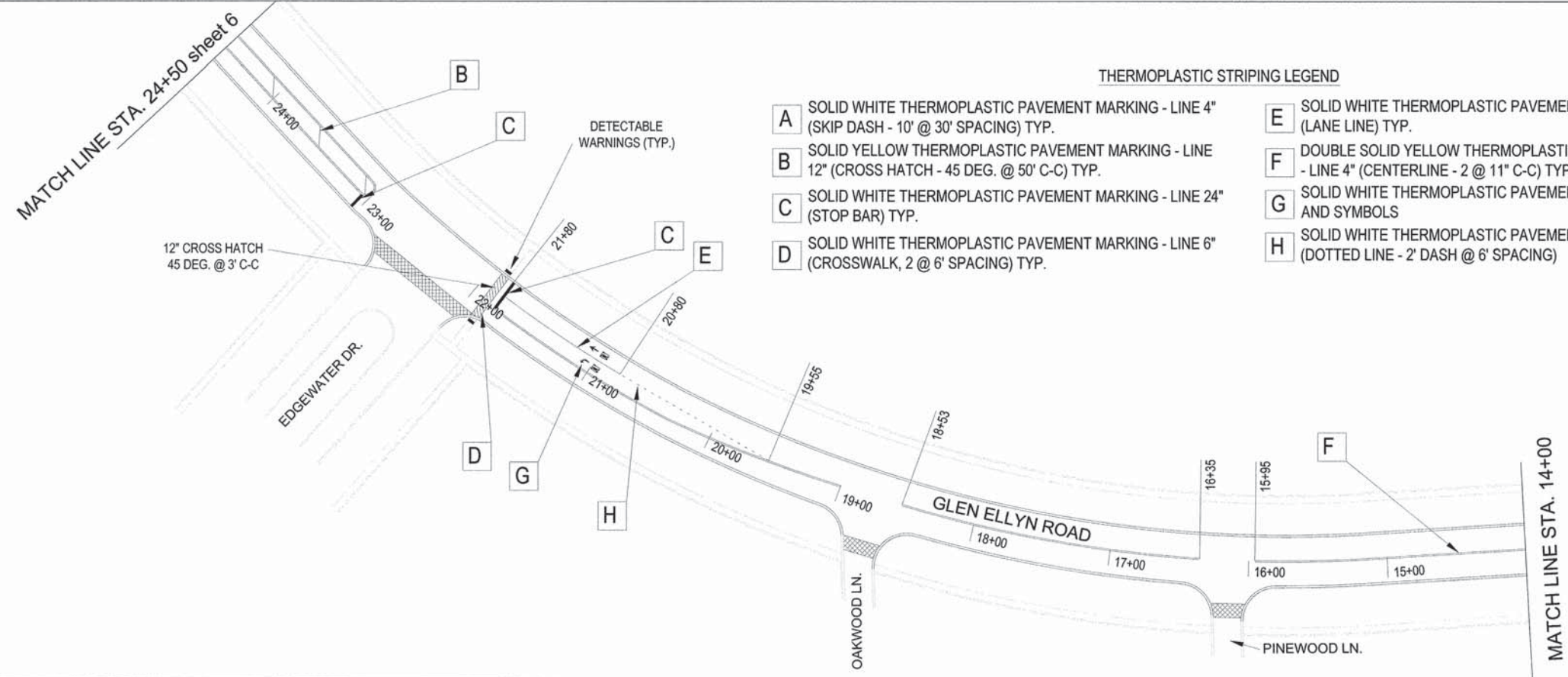
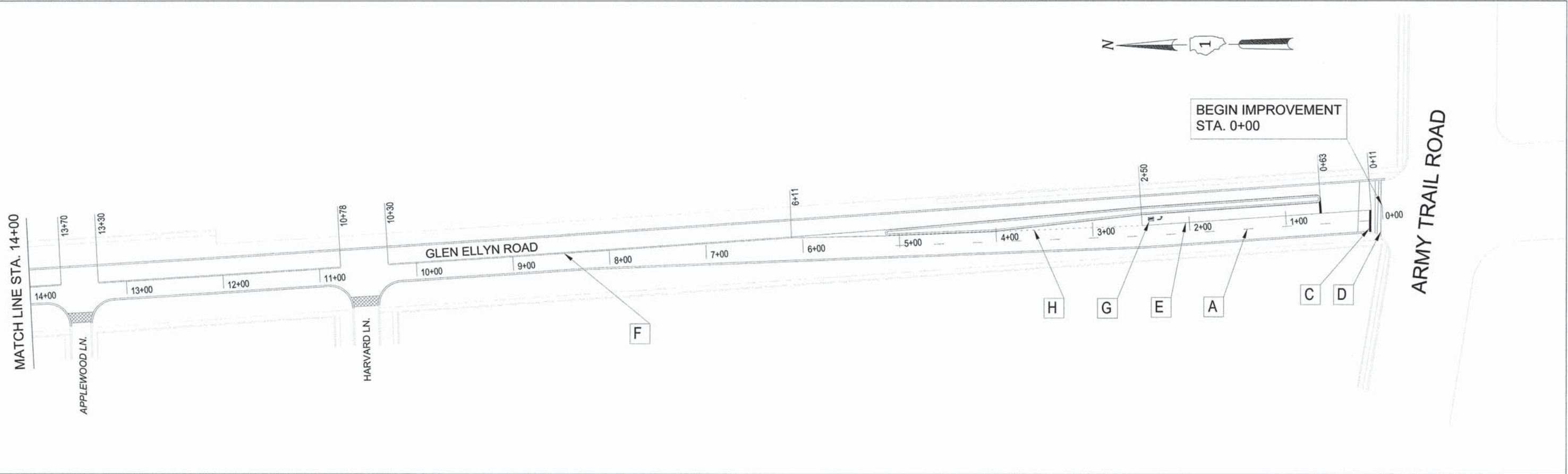
**VILLAGE OF BLOOMINGDALE**  
201 S. Bloomingdale Road  
Bloomingdale, IL 60188  
(630) 893-7000

DATE: 9-10-15  
SCALE: nts  
DRAWN BY: [Signature]  
APPROVED BY:

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2581	14-00062-00-RS	DUPAGE	14	4
		ILLINOIS	<b>CONTRACT NO. 61C23</b>	



**THERMOPLASTIC STRIPING LEGEND**

- A** SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 4" (SKIP DASH - 10' @ 30' SPACING) TYP.
- B** SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 12" (CROSS HATCH - 45 DEG. @ 50' C-C) TYP.
- C** SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 24" (STOP BAR) TYP.
- D** SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (CROSSWALK, 2 @ 6' SPACING) TYP.
- E** SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (LANE LINE) TYP.
- F** DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 4" (CENTERLINE - 2 @ 11" C-C) TYP.
- G** SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- H** SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (DOTTED LINE - 2' DASH @ 6' SPACING)

**LEGEND**  
 BUTT JOINT

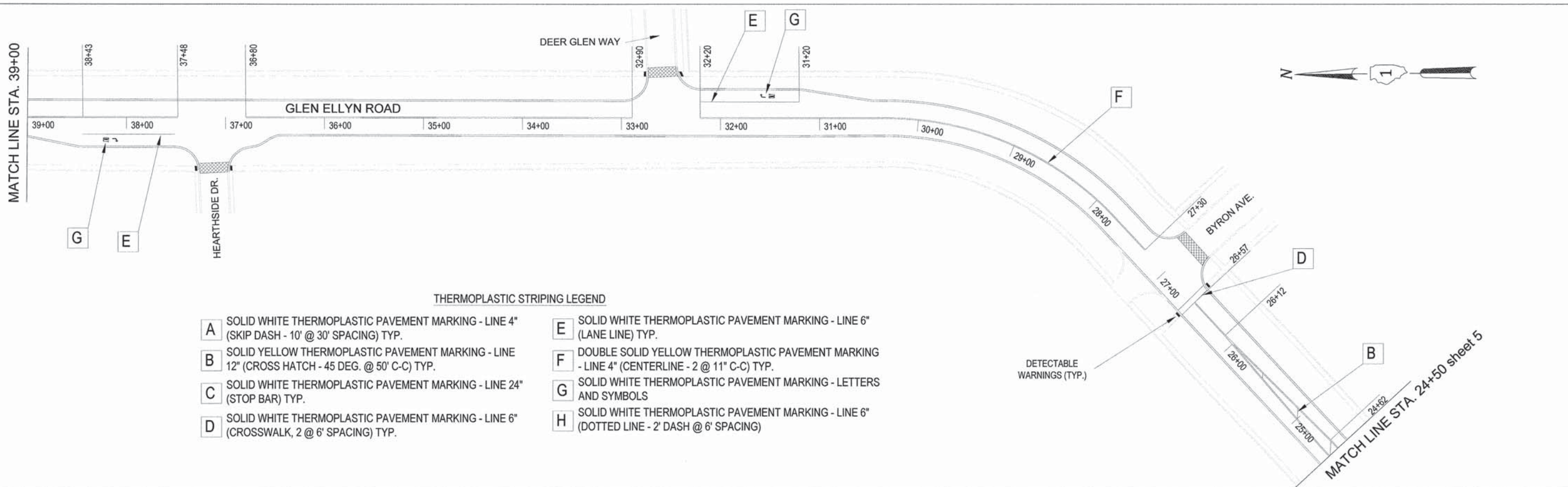
PREPARED BY:  
**VILLAGE OF BLOOMINGDALE**  
 201 S. Bloomingdale Road  
 Bloomingdale, IL 60188  
 (630) 893-7000

DATE: 9-10-15  
 SCALE: 1" = 50'  
 DRAWN BY: [Signature]  
 APPROVED BY:

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

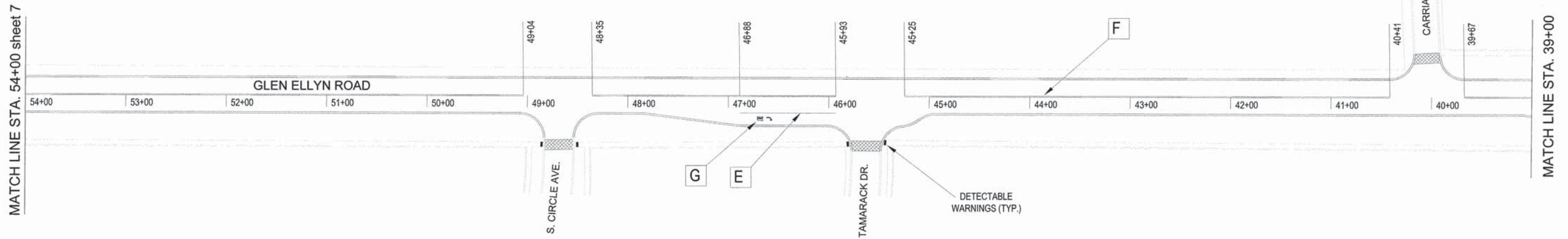
**OVERLAY / STRIPING PLAN**

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2581	14-00062-00-RS	DUPAGE	14	5
		ILLINOIS	<b>CONTRACT NO. 61C23</b>	



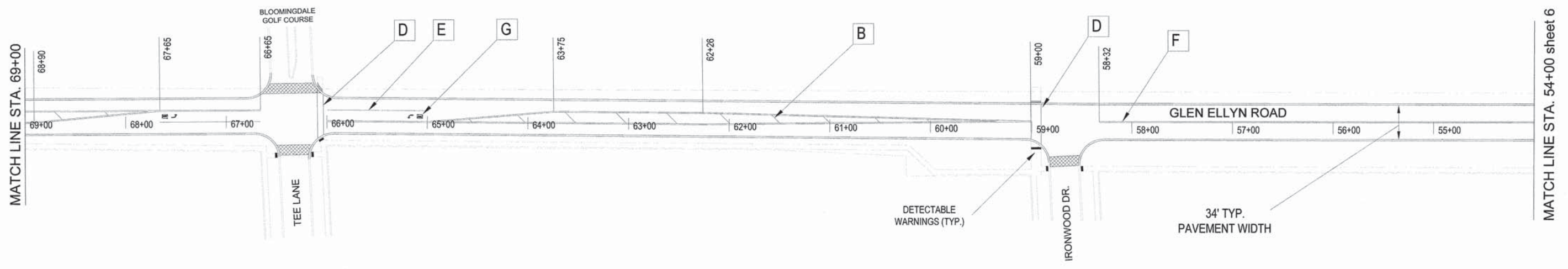
**THERMOPLASTIC STRIPING LEGEND**

- |                                                                                                        |                                                                                                       |
|--------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------|
| <b>A</b> SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 4" (SKIP DASH - 10' @ 30' SPACING) TYP.     | <b>E</b> SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (LANE LINE) TYP.                        |
| <b>B</b> SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 12" (CROSS HATCH - 45 DEG. @ 50' C-C) TYP. | <b>F</b> DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 4" (CENTERLINE - 2 @ 11" C-C) TYP. |
| <b>C</b> SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 24" (STOP BAR) TYP.                         | <b>G</b> SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS                             |
| <b>D</b> SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (CROSSWALK, 2 @ 6' SPACING) TYP.         | <b>H</b> SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (DOTTED LINE - 2' DASH @ 6' SPACING)    |



**LEGEND**  
 BUTT JOINT

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2581	14-00062-00-RS	DUPAGE	14	6
ILLINOIS		CONTRACT NO. 61C23		

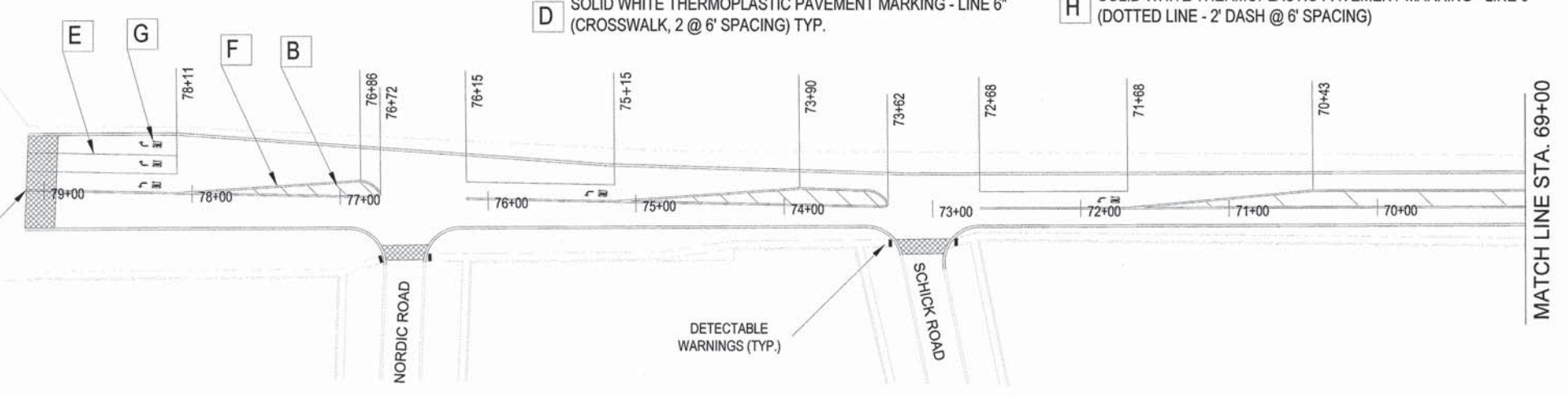


**THERMOPLASTIC STRIPING LEGEND**

- A** SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 4" (SKIP DASH - 10' @ 30' SPACING) TYP.
- B** SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 12" (CROSS HATCH - 45 DEG. @ 50' C-C) TYP.
- C** SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 24" (STOP BAR) TYP.
- D** SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (CROSSWALK, 2 @ 6' SPACING) TYP.
- E** SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (LANE LINE) TYP.
- F** DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING - LINE 4" (CENTERLINE - 2 @ 11" C-C) TYP.
- G** SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- H** SOLID WHITE THERMOPLASTIC PAVEMENT MARKING - LINE 6" (DOTTED LINE - 2' DASH @ 6' SPACING)

**LAKE STREET**

END IMPROVEMENT  
STA. 79+12



**LEGEND**  
 BUTT JOINT

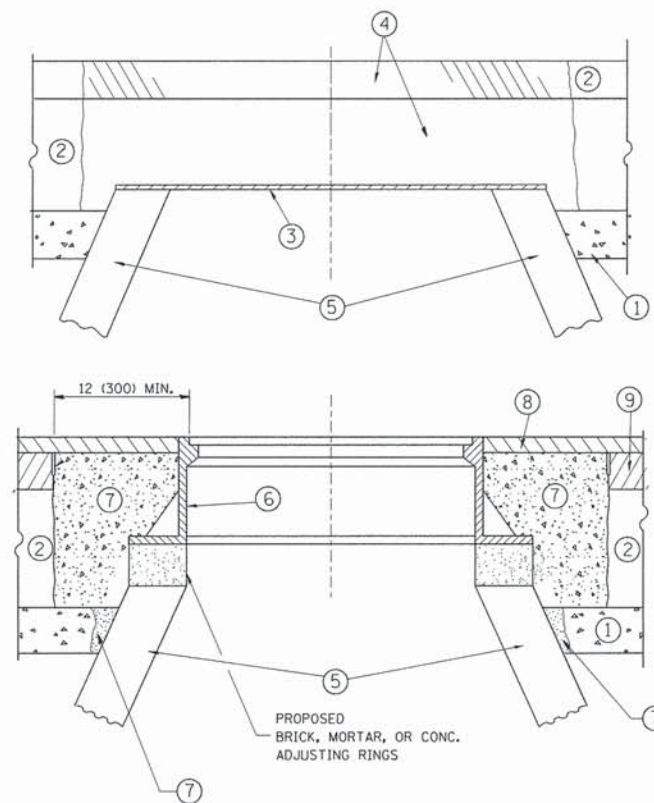
PREPARED BY:  
**VILLAGE OF BLOOMINGDALE**  
 201 S. Bloomingdale Road  
 Bloomingdale, IL 60188  
 (630) 893-7000

DATE: 9-10-15  
 SCALE: 1" = 50'  
 DRAWN BY: [Signature]  
 APPROVED BY:

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**OVERLAY / STRIPING PLAN**

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2581	14-00062-00-RS	DUPAGE	14	7
ILLINOIS		CONTRACT NO. 61C23		



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

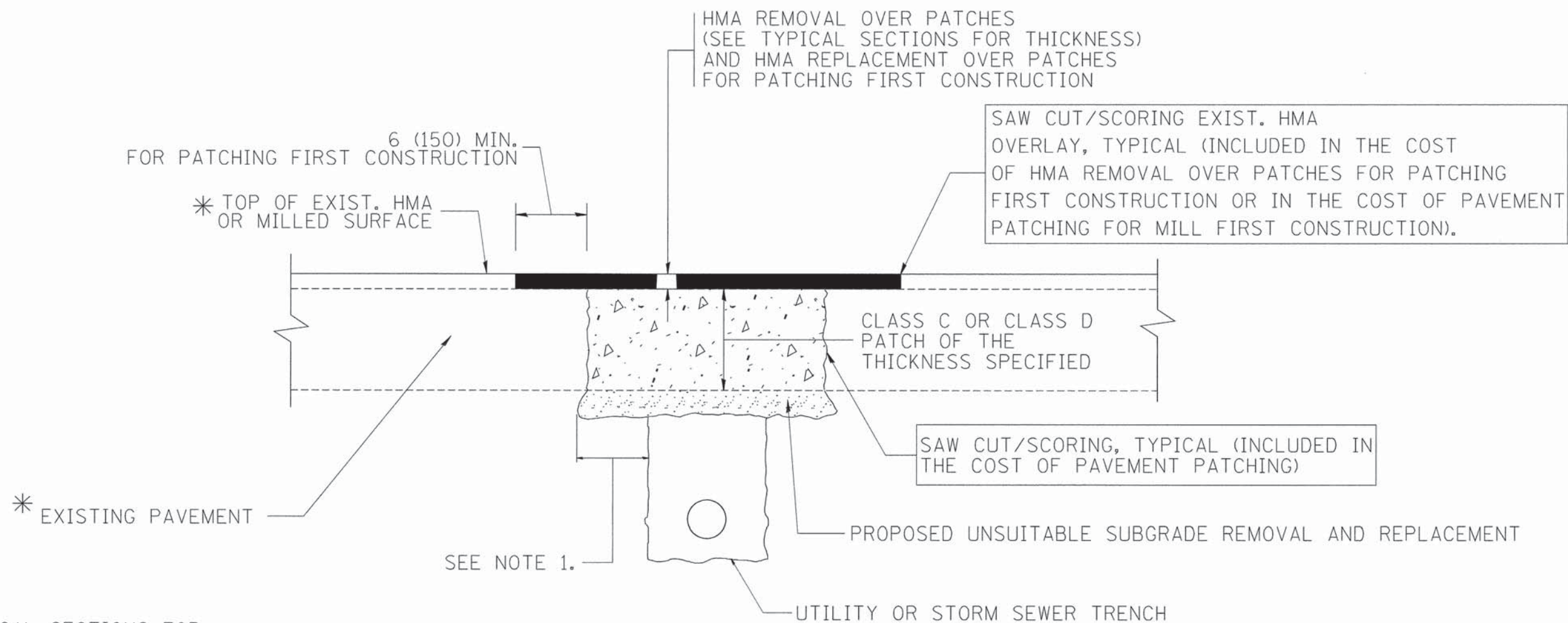
WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			F.A. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cs:\pw_work\pwsdot\bauerdl\d0108315\bd08.dgn		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	2581	14-00062-00-RS	DUPAGE	14	8
PLOT SCALE = 1/8" = 1'-0"		CHECKED -	REVISED - R. BORO 03-09-11		<b>BD600-03 (BD-8)</b>			CONTRACT NO. 61C23				
PLOT DATE = 12/6/2011		DATE - 10-25-94	REVISED - R. BORO 12-06-11		FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT							





\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bouverd	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A. RTE. 2581	SECTION 14-00062-00-RS	COUNTY DUPAGE	TOTAL SHEETS 14	SHEET NO. 9
	PLOT SCALE = 50,000 / IN.	CHECKED -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	<b>BD400-04 (BD-22)</b> CONTRACT NO. 61C23				
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - R. BORO 09-04-07		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT							
			REVISED - K. ENG 10-27-08									

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001  
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

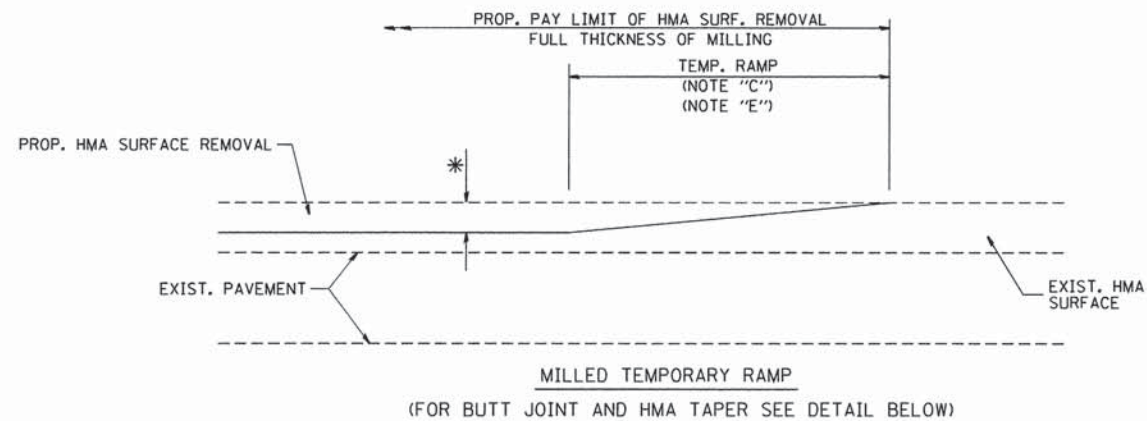
**BASIS OF PAYMENT:**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

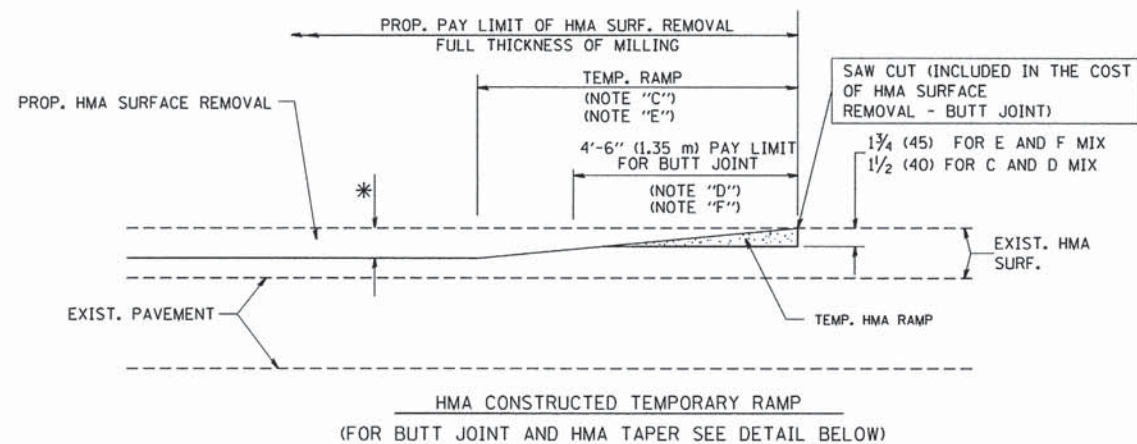
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

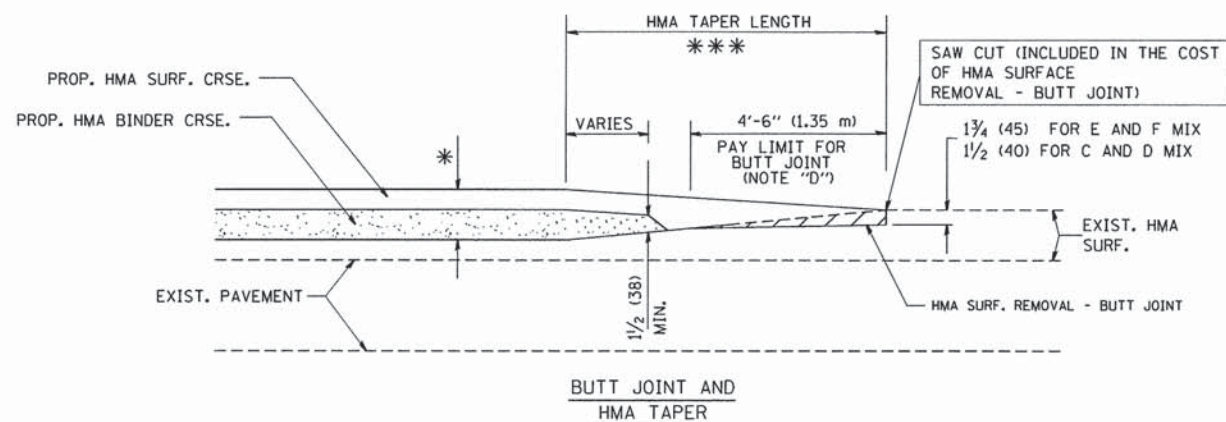
FILE NAME =	USER NAME = drivokosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>		F.A. RTE. 2581	SECTION 14-00062-00-RS	COUNTY DUPAGE	TOTAL SHEETS 14	SHEET NO. 10	
ci:\pw_work\p\dot\drivokosgn\d0108315\bd24.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	<b>BD600-06 (BD-24)</b> CONTRACT NO. 61C23				
		PLOT SCALE = 50.000 / IN.	REVISED - M. GOMEZ 01-22-01									
		DATE = 12/15/2009	REVISED - R. BORO 12-15-09									



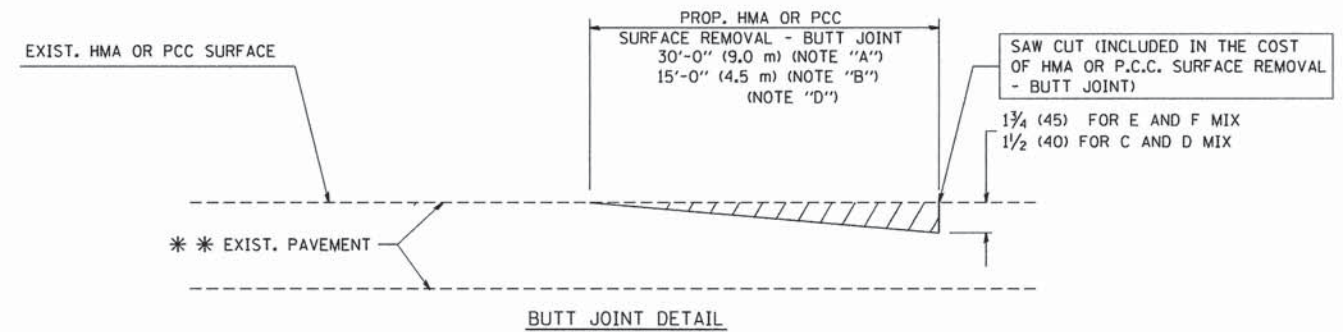
OPTION 1



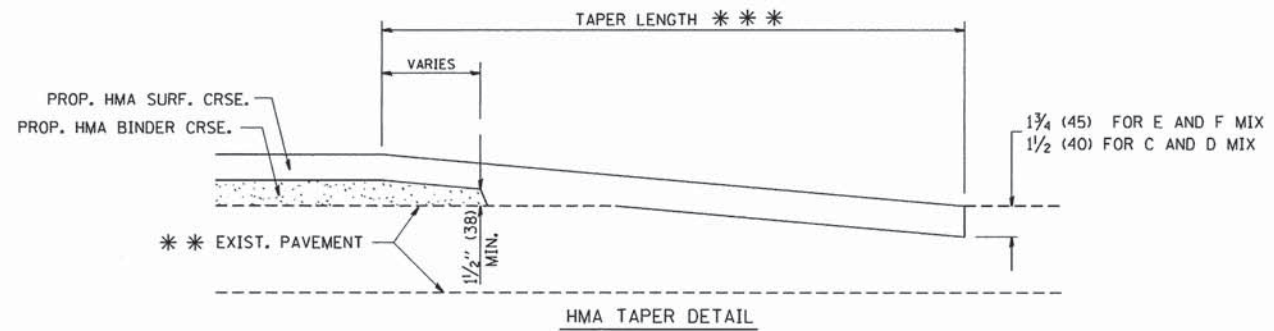
OPTION 2  
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dststd\22x34\bd32.dgn

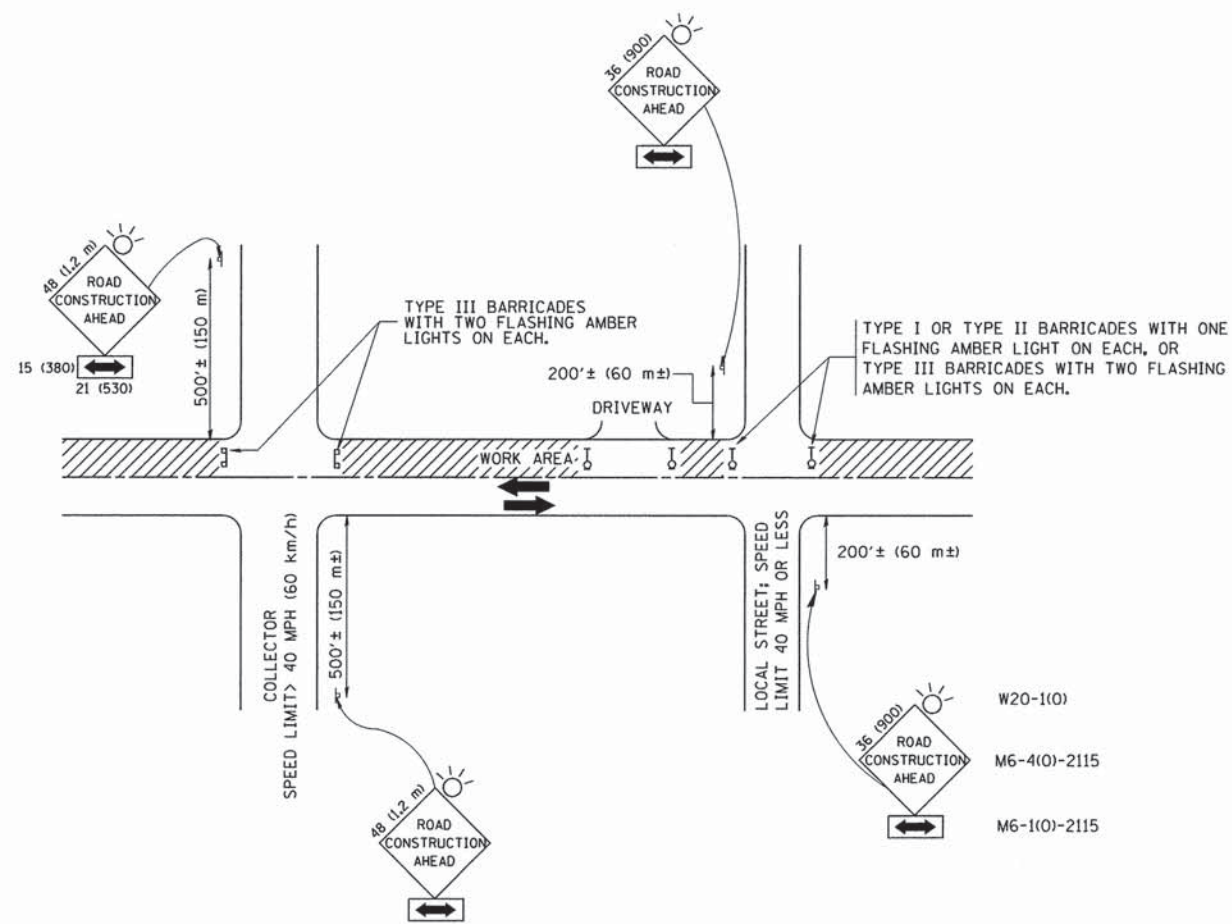
USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 2581	SECTION 14-00062-00-RS	COUNTY DUPAGE	TOTAL SHEETS 14	SHEET NO. 11
BD400-05 BD32			CONTRACT NO. 61C23	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

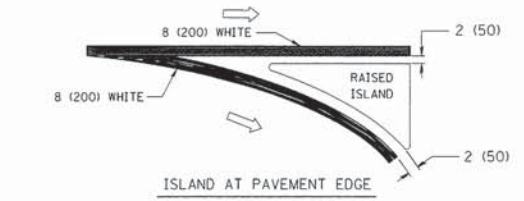
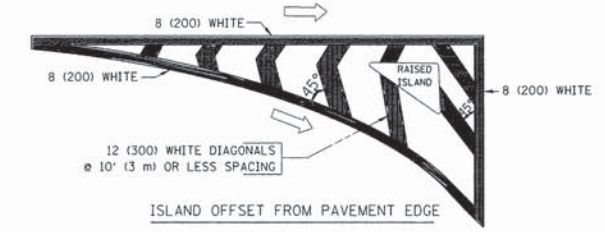
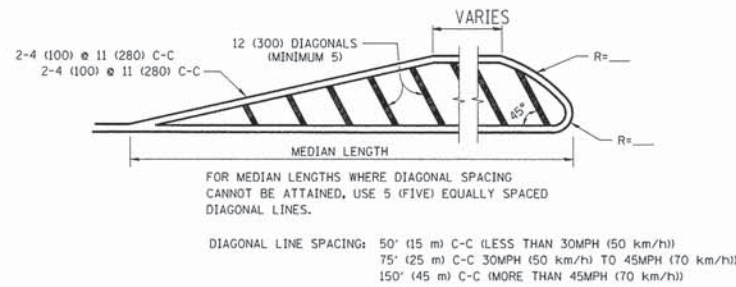
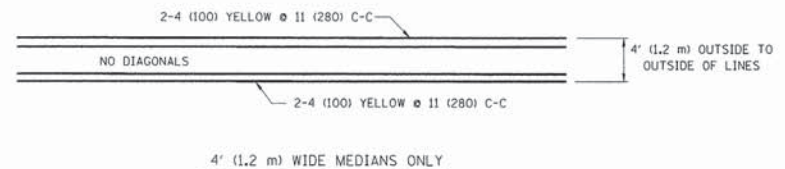
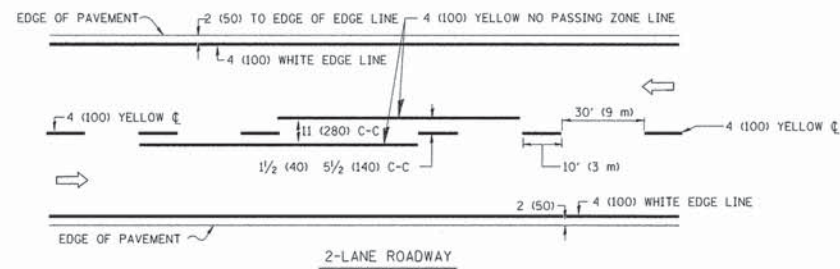
FILE NAME = W:\dststd\22x34\trcl0.dgn	USER NAME = goglionobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

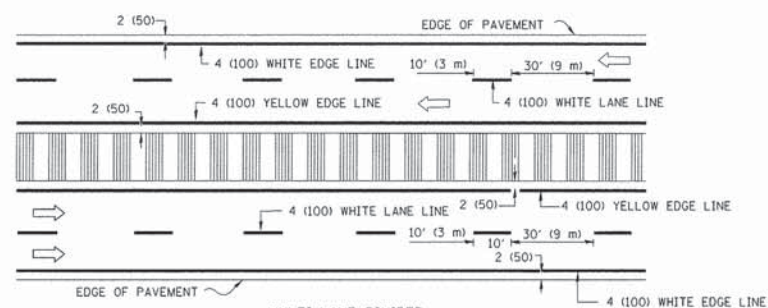
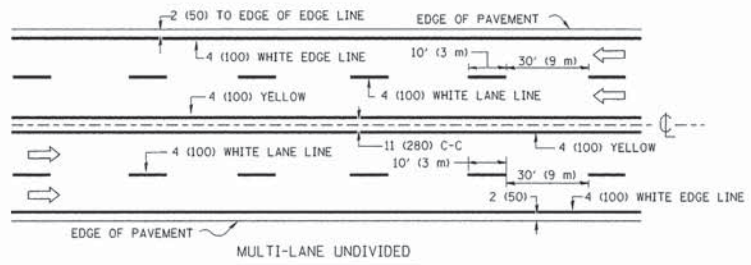
TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

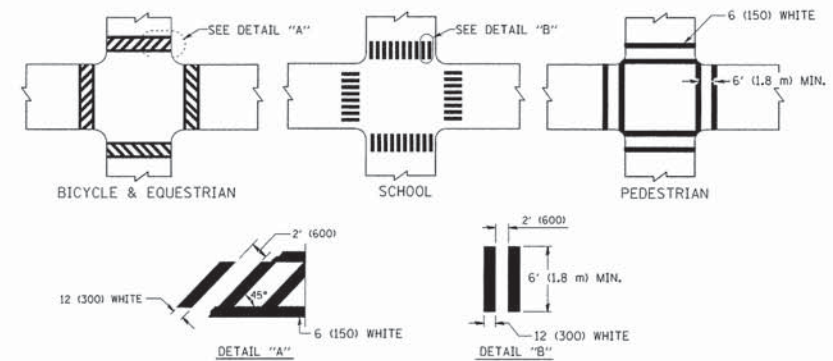
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2581	14-00062-00-RS	DUPAGE	14	12
TC-10			CONTRACT NO. 61C23	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				



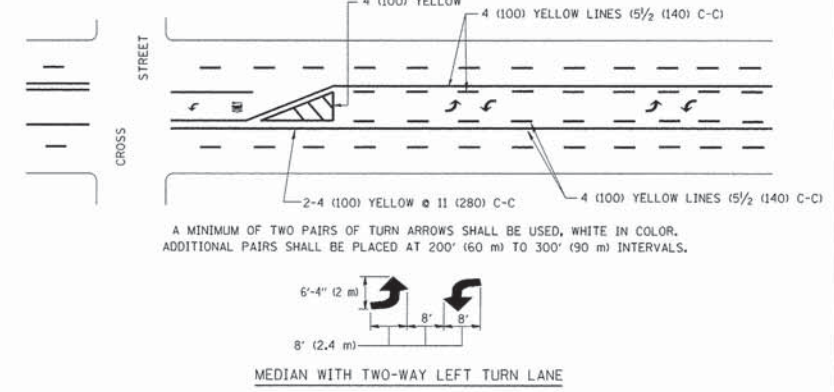
TYPICAL ISLAND MARKING



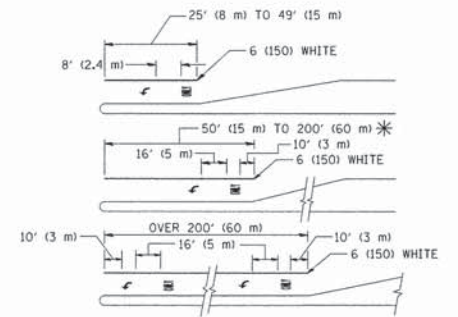
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



TYPICAL PAINTED MEDIAN MARKING



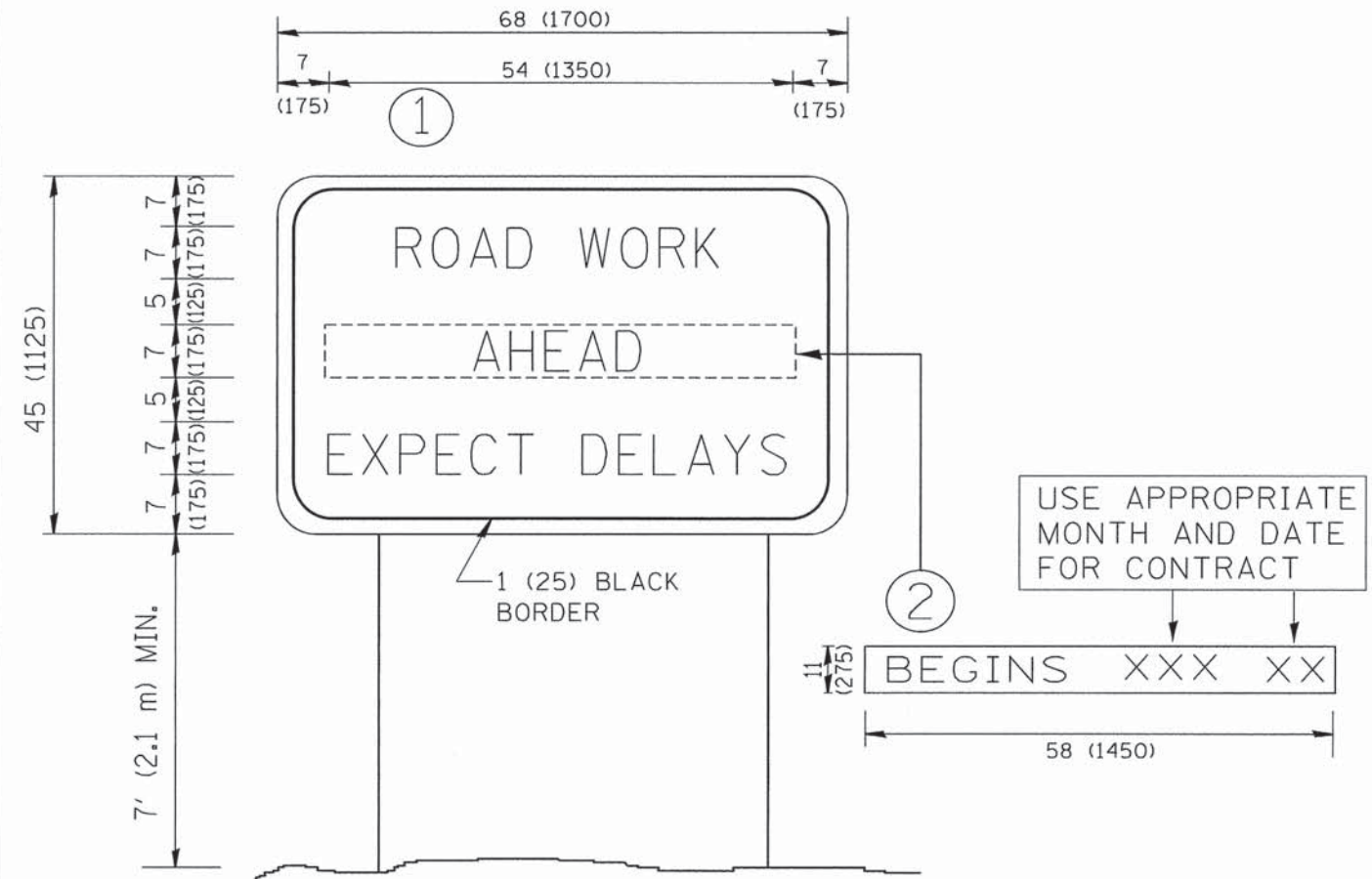
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\to22.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>		F.A. RTE. 2581	SECTION 14-00062-00-RS	COUNTY DUPAGE	TOTAL SHEETS 14	SHEET NO. 14	
	PLOT SCALE = 50,000 ' / IN.	DRAWN -	REVISED - R. MIRS 12-11-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	<b>TC-22</b>		CONTRACT NO. 61C23		
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - T. RAMMACHER 02-02-99				FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT	
		DATE -	REVISED - C. JUCIUS 01-31-07									