

FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E. 847-705-4021, SCHAUMBURG IL CONSULTING ENGINEER: ENGINEERING ENTERPRISES, INC.

FOR INDEX OF SHEETS AND HIGHWAY STANDARDS SEE SHEET NO. 2

03-04-2016 LETTING ITEM 091

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU 2548 (BRIARCLIFF ROAD) FAP 349 (U.S. ROUTE 30) TO FAU 3579 (MONTGOMERY ROAD)

RESURFACING

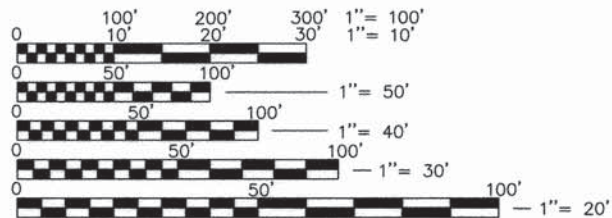
SECTION: 15-00054-00-RS PROJECT NUMBER: M-4003(536) VILLAGE OF MONTGOMERY KANE/KENDALL COUNTIES JOB NUMBER: C-91-391-15

Table with columns: F.A.U. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO. Values: 2548, 15-00054-00-RS, KANE/KENDALL, 20, 1. CONTRACT NO. 61C37

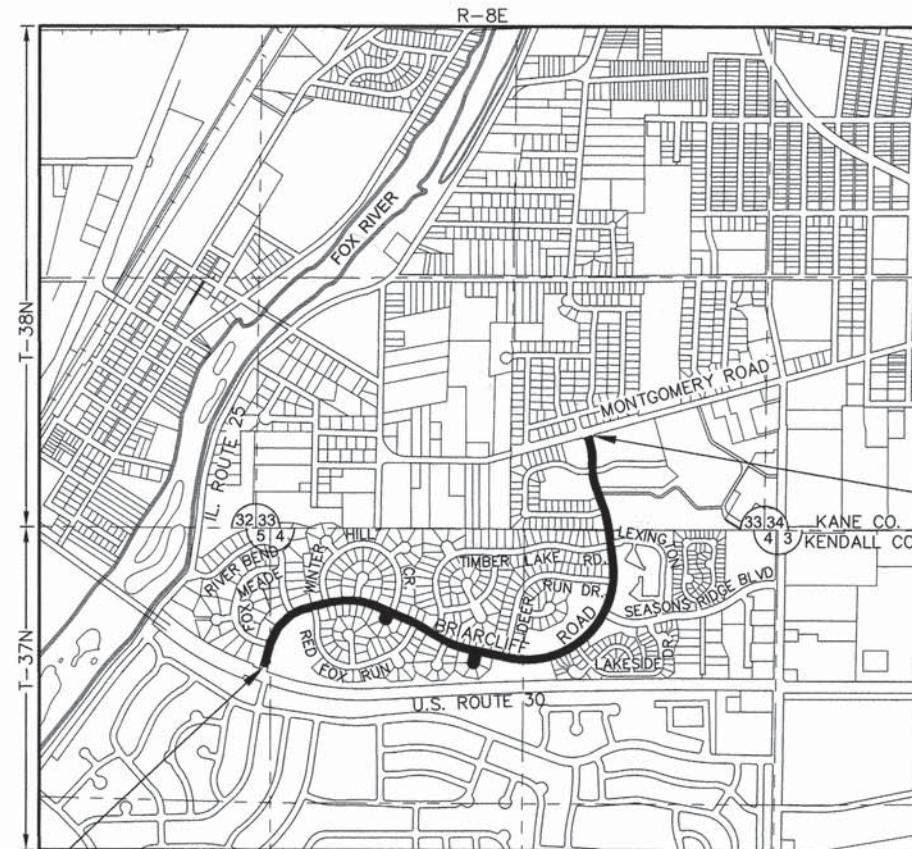


LOCATION OF SECTION INDICATED THUS: [Black bar]

DESIGN DESIGNATION BRIARCLIFF RD. MAJOR COLLECTOR DESIGN SPEED = 30 M.P.H. POSTED SPEED = 30 M.P.H. ADT (2014) = 3,250 VPD



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



PROJECT ENDS STA. 61+80.0

PROJECT BEGINS STA. 1+85.0

SE 1/4 SECTION 33, T38N, R8E, 3RD PM, AURORA TOWNSHIP N 1/2 SECTION 4, NE 1/4 SECTION 4 T37N, R8E, 3RD PM, OSWEGO TOWNSHIP LOCATION MAP SCALE: 1" = 1000' GROSS & NET LENGTH OF PROJECT = 5,995 FEET (1.135 MILE)

JULIE JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION CALL 811 logo with text 'Call Before You Dig' and 'Simply Call 811'.

Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eeiweb.com

Approval section with signatures and dates: APPROVED November 30, 2015 by Todd Hays; PASSED December 17, 2015 by Christopher Holt; RELEASING FOR BID BASED ON LIMITED REVIEW December 17, 2015 by John Furthman. Includes seal for Timothy V. Weidner, License No. 59549, expires November 30, 2017.

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

CONTRACT NO. 61C37

Plotted: November 30, 2015 @ 7:25 AM By: Jim Schmidt - Tab: 01 Cvr 22-34

Path: \\sdsprg01\w01500\w01510\dwg\TRIAL_ENG\W01510-CVR

GENERAL NOTES

SPECIFICATIONS, STANDARDS, AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED JANUARY 1, 2012 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS), THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JANUARY 1, 2015, THE LATEST EDITION OF THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, THE STANDARD SPECIFICATIONS FOR WATER & SEWER MAIN CONSTRUCTION IN ILLINOIS, SEVENTH EDITION, THE CODES AND ORDINANCES OF THE VILLAGE OF MONTGOMERY, ILLINOIS, THE DETAILS IN THE PLANS AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

NO SUBSTITUTIONS OR VARIANCES WILL BE PERMITTED TO ANY STANDARD NOTES OR ORDINANCES UNLESS APPROVED OTHERWISE IN WRITING PRIOR TO COMMENCING CONSTRUCTION ACTIVITY.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH ARTICLE 107.14 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL AT ALL TIMES PROVIDE PROTECTION FOR TRAFFIC AS CALLED FOR IN THE APPLICATION OF TRAFFIC CONTROL DEVICES, THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS AND THE PLANS

UTILITIES

THE CONTRACTOR SHALL COOPERATE WITH THE OWNER IF ANY UTILITY IMPROVEMENTS ARE REQUIRED WITHIN THE DURATION OF THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL EXISTING AND PROPOSED UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.

THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, FIELD TILES AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND NOT NECESSARILY COMPLETE; THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED. THIS WORK SHALL BE ARRANGED BY THE UTILITY COMPANY AND SHALL BE AT THE CONTRACTOR'S EXPENSE.

IF EXISTING UTILITY LINES OF ANY NATURE ARE ENCOUNTERED WHICH CONFLICT IN LOCATION WITH NEW CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT MAY BE RESOLVED.

UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR OPERATE ANY VALVES OR HYDRANTS.

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, THE OWNER'S AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS ARE FROM THE CENTERLINE AS SHOWN ON THE PLANS.

MISCELLANEOUS

SAWING OF REMOVAL ITEMS AS NOTED ON THE PLANS, SPECIFIED IN THE STANDARD SPECIFICATIONS, OR AS REQUIRED BY THE ENGINEER SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

AT ALL MAINLINE AND SIDE ROAD BUTT JOINT LOCATIONS, THE EXISTING SURFACE SHALL BE CUT TO A MINIMUM THICKNESS OF TWO (2) INCHES AS INDICATED ON THE PLANS.

THE THICKNESS OF ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASES ON WHICH THE ASPHALT MIXTURES ARE TO BE PLACED.

THE CONTRACTOR SHALL BE REQUIRED TO MAKE ARRANGEMENTS FOR THE PROPER BRACING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS, STRUCTURES, POLES, CABLES AND PIPE LINES, BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE ENGINEER AT THEIR OWN EXPENSE. ANY SHEETING AND/OR SHORING USED FOR THIS IMPROVEMENT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

THE CONTRACTOR SHALL PROTECT ALL EXISTING FACILITIES (E.G. CURB, DRIVEWAYS, PAVEMENT) THAT ARE NOT INDICATED TO BE REMOVED ON THE PLANS. ANY FACILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED AT THE CONTRACTOR'S EXPENSE.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THIS CONTRACT.

EXISTING PAVEMENT THICKNESSES SHOWN ON THE PLANS ARE APPROXIMATE, BASED ON AVAILABLE INFORMATION AT THE TIME OF DESIGN. ANY ADDITIONAL COSTS REQUIRED BY THE CONTRACTOR DUE TO THICKNESSES OTHER THAN THOSE SHOWN ON THE PLANS WILL BE INCLUDED IN THE COST OF THE CONTRACT.

WHERE NEW WORK MEETS EXISTING FEATURES TO REMAIN, THE CONTRACTOR SHALL FIELD CHECK ALL DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH CONSTRUCTION. IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES.

THE CONTRACTOR WILL BE REQUIRED TO COMPLY WITH ALL STATE REGULATIONS REGARDING AIR, WATER AND NOISE POLLUTION. THE CONTRACTOR IS PROHIBITED FROM BURNING ANY MATERIAL WITHIN OR ADJACENT TO THE IMPROVEMENT.

ALL DISTURBED AREAS WITHIN THE PROJECT THAT ARE NOT OTHERWISE SURFACED SHALL BE CLEANED, LAYERED WITH TOPSOIL, AND SODDED AS SHOWN IN THE PLANS. LIMITS SHOWN ON THE PLANS ARE THE MAXIMUM PAY WIDTHS FOR PAYMENT PURPOSES. ADDITIONAL AREAS DAMAGED BY MACHINERY, CONSTRUCTION EQUIPMENT, CONTRACTOR NEGLIGENCE OR OVER-EXCAVATION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED AT THE COST OF THE CONTRACTOR.

THE CONTRACTOR SHALL DISPOSE OF AND REMOVE FROM THE SITE EACH DAY ALL CURB AND GUTTER, PAVEMENT AND ALL OTHER EXCAVATED MATERIAL NOT FOR SALVAGE. THE COST FOR HAULING AND TRUCKING TO DISPOSAL LOCATIONS WILL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

THE ENGINEER IS NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR FOR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF THEIR WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND SPECIFICATIONS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING FRESH CONCRETE FROM DAMAGE AND VANDALISM. ANY DAMAGED OR VANDALIZED CONCRETE SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.

BACKFILL AREAS ADJACENT TO COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT SHALL BE BACKFILLED WITH CLASS SI CONCRETE AND HAVE AN HMA OVERLAY AS SHOWN IN THE SPECIAL DETAIL. THE CLASS SI CONCRETE WILL BE CONSIDERED INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.

PATCHING, SIDEWALK REMOVAL AND REPLACEMENT, AND COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT LOCATIONS SHALL BE VERIFIED BY THE ENGINEER AND MARKED OUT BY THE ENGINEER IN THE FIELD.

BRIARCLIFF ROAD SHALL BE OPEN TO TRAFFIC AT ALL TIMES. WHEN IT IS NECESSARY TO CLOSE ONE LANE OF TRAFFIC DUE TO CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN TWO-WAY TRAFFIC DURING CONSTRUCTION HOURS WITH THE USE OF TRAFFIC CONTROL DEVICES, SIGNS AND FLAGGERS AS APPLICABLE IN THE TRAFFIC CONTROL STANDARDS.

THE CONTRACTOR SHALL CONTACT THE IDOT TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT ANY RESIDENT OR BUSINESS OF ANY REMOVAL AND REPLACEMENT ACTIVITIES THAT WILL INHIBIT OR PROHIBIT ACCESS TO THEIR DRIVEWAY, IN WRITING, A MINIMUM OF 48 HOURS BUT NOT MORE THAN 72 HOURS, PRIOR TO THE COMMENCEMENT OF THESE ACTIVITIES. THE MORNING OF THE WORK, THE CONTRACTOR SHALL AGAIN NOTIFY THE OWNER VERBALLY, TO ALLOW THE OWNER TIME TO MOVE THEIR VEHICLE SO AS NOT TO PROHIBIT THE VEHICLE FROM LEAVING THE DRIVEWAY UPON REMOVAL OF ANY MATERIAL. THE NOTICE GIVEN OUT BY THE CONTRACTOR SHALL PROVIDE INFORMATION REGARDING THE ANTICIPATED DATE THAT FULL ACCESS WILL BE RESTORED. COORDINATION BETWEEN ACTIVITIES SHOULD ALLOW ALL WORK TO BE DONE IN A TIMELY MANNER SO AS TO PERMIT ACCESS TO THE ROADWAY. ANY ADDITIONAL COST OF STAGING REQUIRED TO MAINTAIN ACCESS IS CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

IN AREAS OF NEW SIDEWALK CONSTRUCTION WHERE THERE IS ASPHALT PAVEMENT, THE PAVEMENT REMOVAL WILL BE PAID FOR AS SIDEWALK REMOVAL. THE SIDEWALK SHALL BE CONSTRUCTED ON A BASE OF 4" AGGREGATE BASE COURSE, TYPE B WHICH WILL BE CONSIDERED INCLUDED IN THE COST OF THE PCC SIDEWALK.

INDEX OF SHEETS

SHEET NO.	SHEET DESCRIPTION
1.	COVER SHEET AND LOCATION MAP
2.	GENERAL NOTES, HIGHWAY STANDARDS, SUPPLEMENTAL LEGEND AND INDEX OF SHEETS
3.	SUMMARY OF QUANTITIES
4.-7.	TYPICAL SECTIONS
8.-12.	GENERAL PLAN
13.	DETAILS
DISTRICT ONE DETAILS	
14.	(TC-10) TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
15.	(TC-13) DISTRICT ONE - TYPICAL PAVEMENT MARKINGS
16.	(TC-16) PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
17.	(BD-08) DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
18.	(BD-22) PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
19.	(BD-24) CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
20.	(BD-32) BUTT JOINT AND HMA TAPER DETAILS

HIGHWAY STANDARDS

STD. NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-02	DIAGONAL CURB RAMPS FOR SIDEWALKS
424016-02	MID-BLOCK CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701001-02	OFF-ROAD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5M) AWAY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15'(4.5M) TO 24" (600MM) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE 2 L 2 W, UNDIVIDED
701502-06	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

SUPPLEMENTAL LEGEND

- SEE IDOT HIGHWAY STANDARDS FOR ADDITIONAL INFORMATION
- EXISTING CONCRETE SIDEWALK OR DRIVEWAY TO REMAIN IN PLACE
 - SIDEWALK REMOVAL AND PCC SIDEWALK, 5"
 - PCC DRIVEWAY REMOVAL AND REPLACEMENT
 - HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT
 - HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
 - HOT-MIX ASPHALT SURFACE REMOVAL, 3"
 - HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
 - CLASS D PATCHES
 - EXISTING COMBINATION CONCRETE CURB AND GUTTER
 - COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

Printed: November 30, 2015 @ 9:25 AM By: jim.schmidt - Tab: 02.india 22-34

COPYRIGHT © 2015 ENGINEERING ENTERPRISES, INC.

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
830.466.6700 / www.eetweb.com

USER NAME =	DESIGNED - CMF	REVISED -
PLOT SCALE =	DRAWN - JPS/CLN/KKP	REVISED -
PLOT DATE =	CHECKED - TVW	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES, HIGHWAY STANDARDS,
SUPPLEMENTAL LEGEND AND INDEX OF SHEETS**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2548	15-00054-00-RS	KENDALL/KANE	20	2
CONTRACT NO. 61C37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

Path: H:\EST\PROJ\MO1500\MO1500\DWG\FINAL_ENG\MO1500-CR

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 75% FEDERAL 25% LOCAL 0005	
				KANE COUNTY	KENDALL COUNTY
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	390	90	300
25200110	SODDING, SALT TOLERANT	SQ YD	390	90	300
25200200	SUPPLEMENTAL WATERING	UNIT	19	5	14
* 40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	17,643	2,790	14,853
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	10	2	8
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1,465	232	1,233
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	378	76	302
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	2,930	464	2,466
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	3,100	800	2,300
42400800	DETECTABLE WARNINGS	SQ FT	420	140	280
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SQ YD	24,695	2,690	22,005
44000600	SIDEWALK REMOVAL	SQ FT	3,100	800	2,300
44201682	CLASS D PATCHES, TYPE II, 3 INCH	SQ YD	222	91	131
44201683	CLASS D PATCHES, TYPE III, 3 INCH	SQ YD	163	82	81
44201684	CLASS D PATCHES, TYPE IV, 3 INCH	SQ YD	389	82	307
60260100	INLETS TO BE ADJUSTED	EACH	17	2	15
67100100	MOBILIZATION	LSUM	1	0.2	0.8
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LSUM	1	0.2	0.8
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	LSUM	1	0.2	0.8
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	0.5	0.5
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	0.25	0.75
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	4	2	2
70300100	SHORT TERM PAVEMENT MARKING	FOOT	1,820	370	1,450
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	3,210	1,550	1,660
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1,280	560	720
Δ 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	110	73	37
Δ 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3,210	1,550	1,660
Δ 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,040	390	650
Δ 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	84	84	0
Δ 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	200	70	130
* X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	1,442	1,442	0
* X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	1	0	1
* X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	4	0	4
* Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1,112	205	907
* XX003435	PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	69	0	69
* XX006947	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	4	0	4

Δ INDICATES SPECIALTY ITEM
* SEE SPECIAL PROVISIONS

Plotted: November 30, 2015 10:00 AM By: jim.schmidt - Tab: 03_Sum of Qunt 22-34

COPYRIGHT © 2015 ENGINEERING ENTERPRISES, INC.

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eeiweb.com

USER NAME =
DESIGNED - CMF
DRAWN - JPS/CLN/KKP
PLOT SCALE =
CHECKED - TVW
PLOT DATE =
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

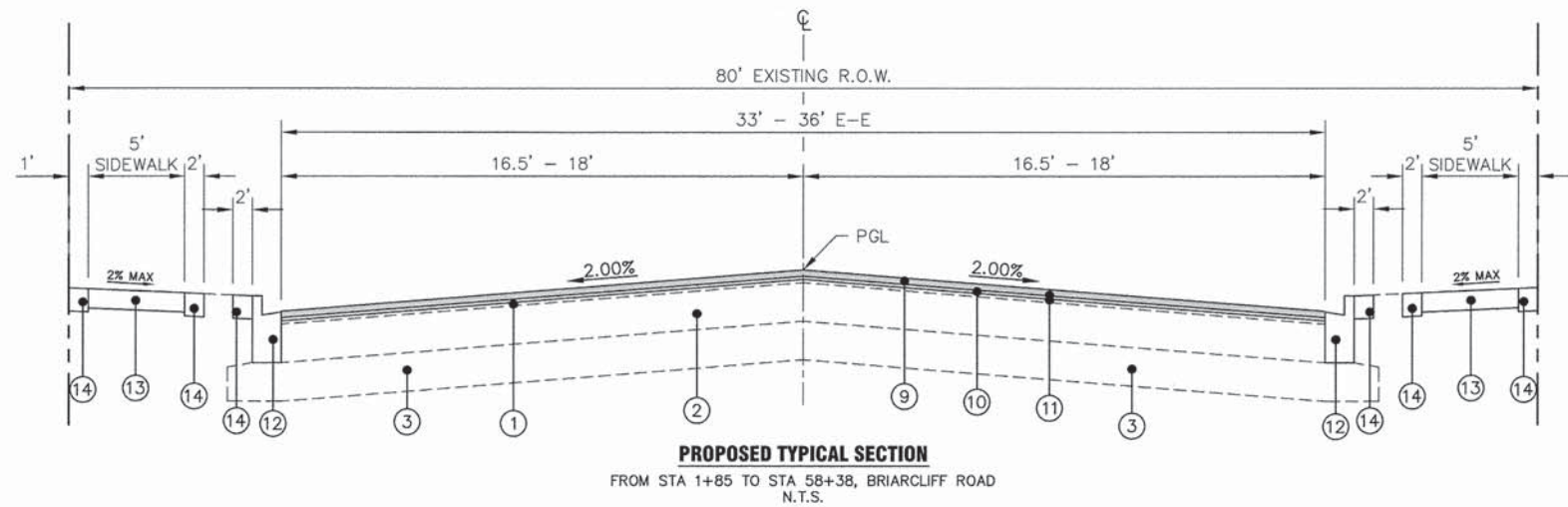
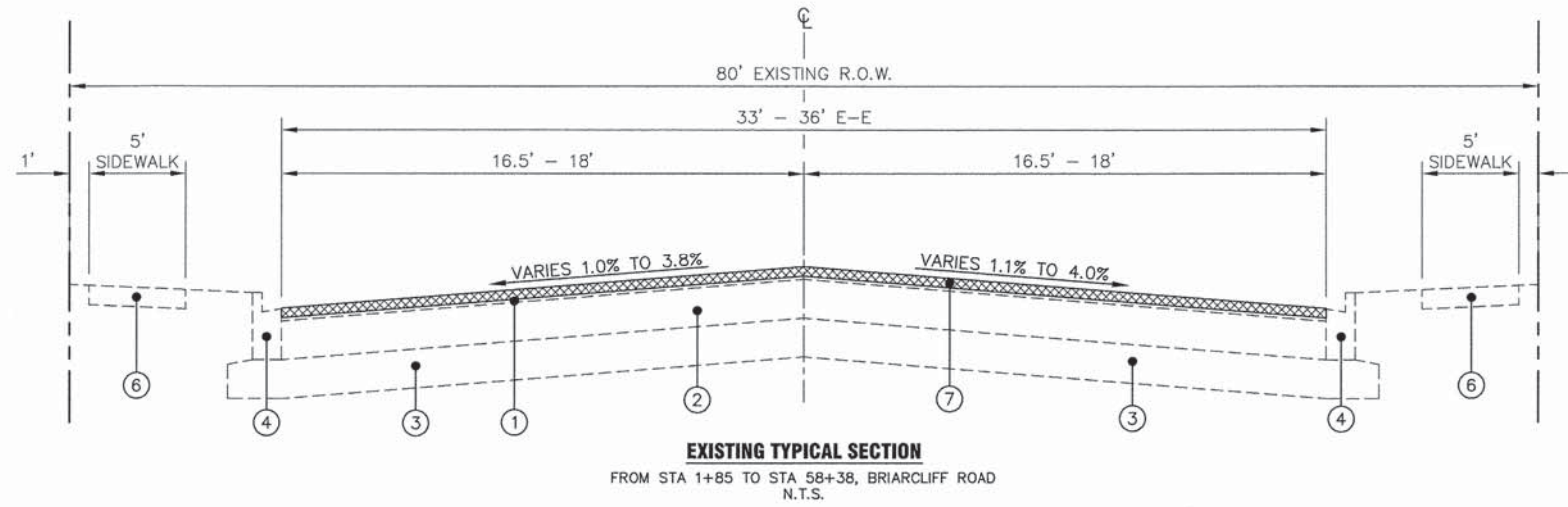
SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2548	15-00054-00-RS	KENDALL/KANE	20	3
CONTRACT NO. 61C37				

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP

Path: \\S05KPROX\01500\01510\DWG\FINAL_ENG\01510-CVR



PATCHING SHALL BE PERFORMED AFTER MILLING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

OPERATION	MIXTURE TYPE	AIR VOIDS @ N ₁₀₀
BRIARCLIFF ROAD RESURFACING	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"	3.5% @ 50 Gyr.
PATCHING	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm), 2"	4% @ 50 Gyr.
	CLASS D PATCHES, 3 INCH	
	HMA BINDER COURSE, IL-19.0, N70, 3" (IN 1 LIFT)	4% @ 70 Gyr.
DRIVEWAY RECONSTRUCTION	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	
	HMA BINDER COURSE, IL-19.0, N50, 2 1/4"	4% @ 50 Gyr.
	HMA SURFACE COURSE, MIX "D", N50 (IL-9.5 mm), 2"	4% @ 50 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/INCH.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS

LEGEND	
① EXISTING 1.25" TO 4.25" ASPHALT SURFACE COURSE	⑨ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2.0"
② EXISTING 1" TO 4.5" ASPHALT BINDER COURSE	⑩ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1.0"
③ EXISTING 9" TO 19" AGGREGATE BASE COURSE	⑪ BITUMINOUS MATERIALS (PRIME COAT)
④ EXISTING B-6.12 COMBINATION CONCRETE CURB AND GUTTER	⑫ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS)
⑤ EXISTING B-6.06 COMBINATION CONCRETE CURB AND GUTTER	⑬ SIDEWALK REMOVAL AND PORTLAND CEMENT CONCRETE SIDEWALK 5" (AT VARIOUS LOCATIONS).
⑥ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK, 4"	⑭ TOPSOIL AND SOD (LOCATIONS AS DIRECTED BY THE ENGINEER)
⑦ HOT-MIX ASPHALT SURFACE REMOVAL, 3"	
⑧ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	

Plotfile: November 30, 2015 @ 9:29 AM By: Jim Schmidt - Tab: 04 Typ Section 22-34

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - CMF	REVISED -
PLOT SCALE =	DRAWN - JPS/CLN/KKP	REVISED -
PLOT DATE =	CHECKED - TVW	REVISED -
	DATE -	REVISED -

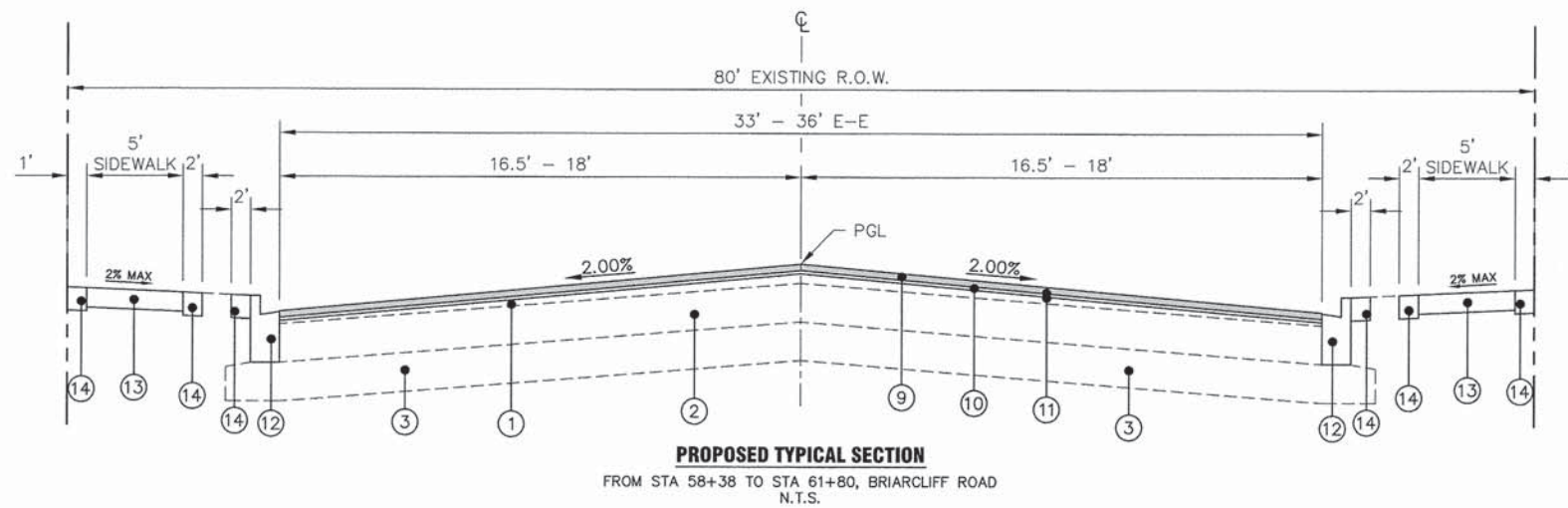
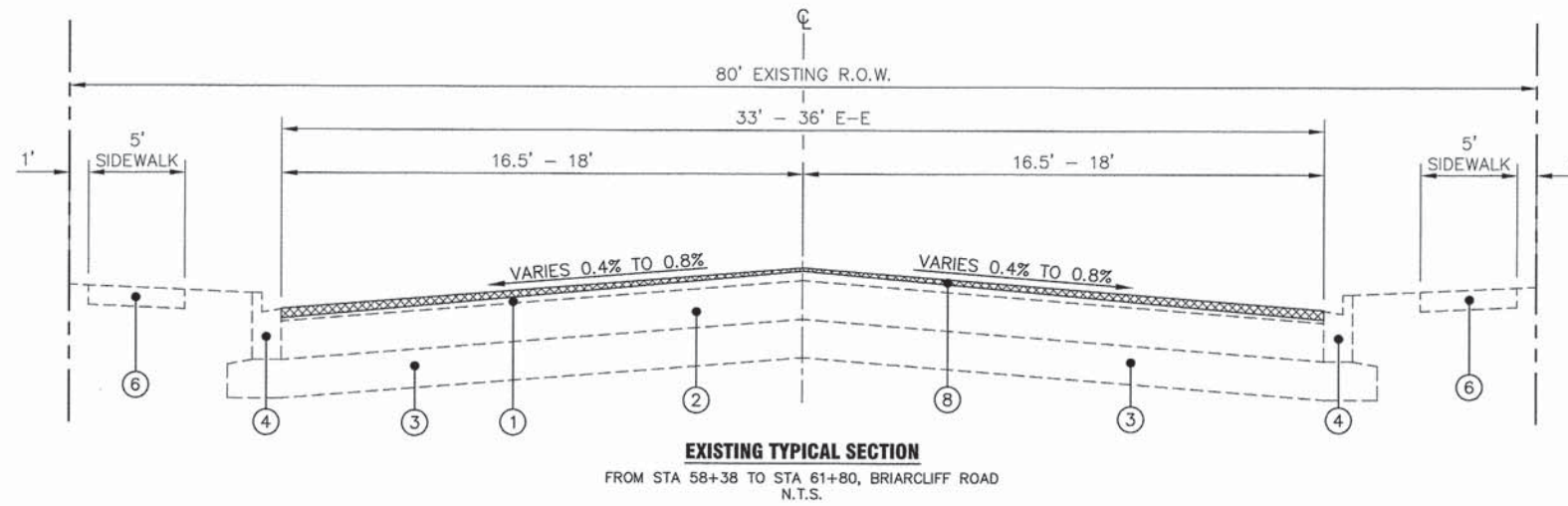
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS

SCALE: N.T.S. SHEET NO. 1 OF 4 SHEETS STA. 1+85 TO STA. 58+38

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2548	15-00054-00-RS	KENDALL/KANE	20	4
CONTRACT NO. 61C37			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP	

Path: \\SBS\PROJ\MOIS00\MOIS00\DWG\FINAL_ENG\M01010-CR



LEGEND	
<ul style="list-style-type: none"> ① EXISTING 1.25" TO 4.25" ASPHALT SURFACE COURSE ② EXISTING 1" TO 4.5" ASPHALT BINDER COURSE ③ EXISTING 9" TO 19" AGGREGATE BASE COURSE ④ EXISTING B-6.12 COMBINATION CONCRETE CURB AND GUTTER ⑤ EXISTING B-6.06 COMBINATION CONCRETE CURB AND GUTTER ⑥ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK, 4" ⑦ HOT-MIX ASPHALT SURFACE REMOVAL, 3" ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH 	<ul style="list-style-type: none"> ⑨ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2.0" ⑩ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1.0" ⑪ BITUMINOUS MATERIALS (PRIME COAT) ⑫ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS) ⑬ SIDEWALK REMOVAL AND PORTLAND CEMENT CONCRETE SIDEWALK 5" (AT VARIOUS LOCATIONS) ⑭ TOPSOIL AND SOD (LOCATIONS AS DIRECTED BY THE ENGINEER)

Plotted: November 30, 2015 @ 10:01 AM By: Jim Schmidt - Tab: 05 Typ: Section 22x34
 Copyright © 2015 ENGINEERING ENTERPRISES, INC.

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 830.466.6700 / www.eetweb.com

USER NAME =	DESIGNED - CMF	REVISED -	
	DRAWN - JPS/CLN/KKP	REVISED -	
PLOT SCALE =	CHECKED - TVW	REVISED -	
PLOT DATE =	DATE -	REVISED -	

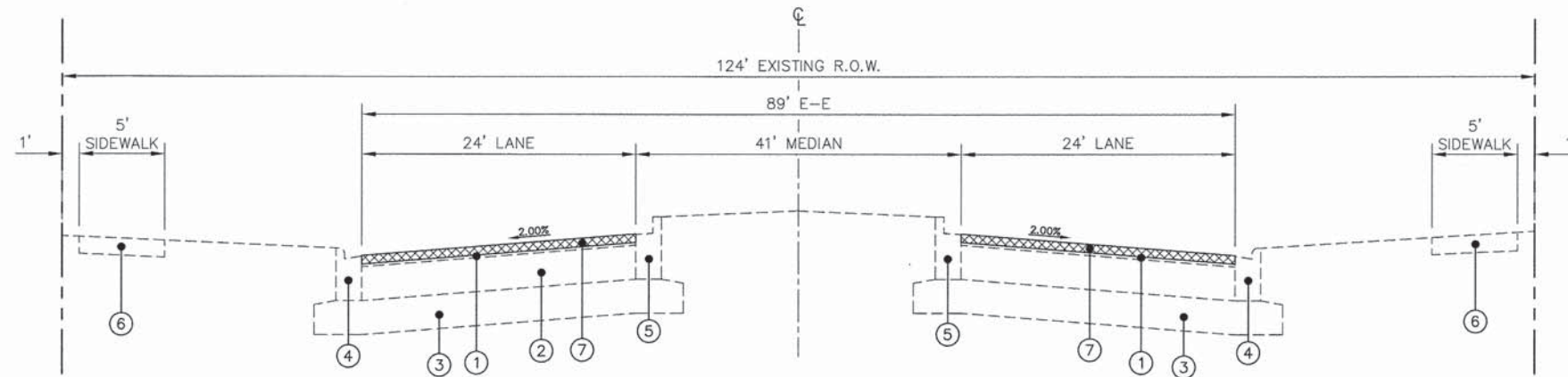
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

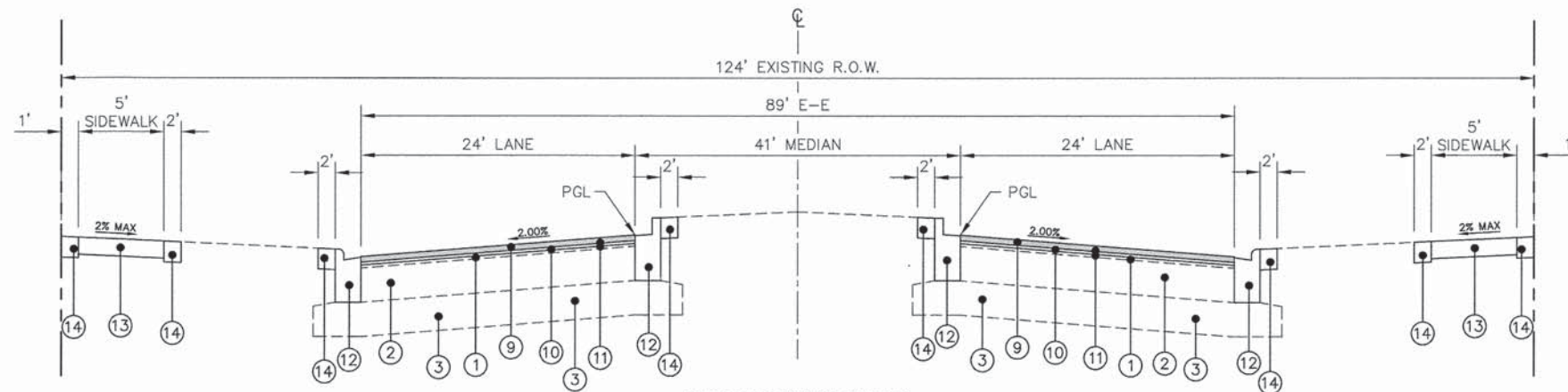
SCALE: N.T.S. SHEET NO. 2 OF 4 SHEETS STA. 58+38 TO STA. 61+97

F.A.I.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2548	15-00054-00-RS	KENDALL/KANE	20	5
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP			CONTRACT NO. 61C37	

Path: H:\SSR\PROJ\MO1500\MO1500\DWG\FINAL_ENG\MO1500-CR



EXISTING TYPICAL SECTION
FROM STA 100+00 TO STA 101+50, BRIARCLIFF ROAD
N.T.S.



PROPOSED TYPICAL SECTION
FROM STA 100+00 TO STA 101+50, BRIARCLIFF ROAD
N.T.S.

LEGEND

- | | |
|--|---|
| ① EXISTING 1.25" TO 4.25" ASPHALT SURFACE COURSE | ⑨ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2.0" |
| ② EXISTING 1" TO 4.5" ASPHALT BINDER COURSE | ⑩ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1.0" |
| ③ EXISTING 9" TO 19" AGGREGATE BASE COURSE | ⑪ BITUMINOUS MATERIALS (PRIME COAT) |
| ④ EXISTING B-6.12 COMBINATION CONCRETE CURB AND GUTTER | ⑫ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS) |
| ⑤ EXISTING B-6.06 COMBINATION CONCRETE CURB AND GUTTER | ⑬ SIDEWALK REMOVAL AND PORTLAND CEMENT CONCRETE SIDEWALK 5" (AT VARIOUS LOCATIONS) |
| ⑥ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK, 4" | ⑭ TOPSOIL AND SOD (LOCATIONS AS DIRECTED BY THE ENGINEER) |
| ⑦ HOT-MIX ASPHALT SURFACE REMOVAL, 3" | |
| ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH | |

Printed: November 30, 2015 @ 10:01 AM By: Jim Schmidt - Tab: 08 Typ Section 22x34

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - CMF	REVISED -
DRAWN - JPS/CLN/KKP	REVISED -	
CHECKED - TVW	REVISED -	
DATE -	REVISED -	
PLOT SCALE =		
PLOT DATE =		

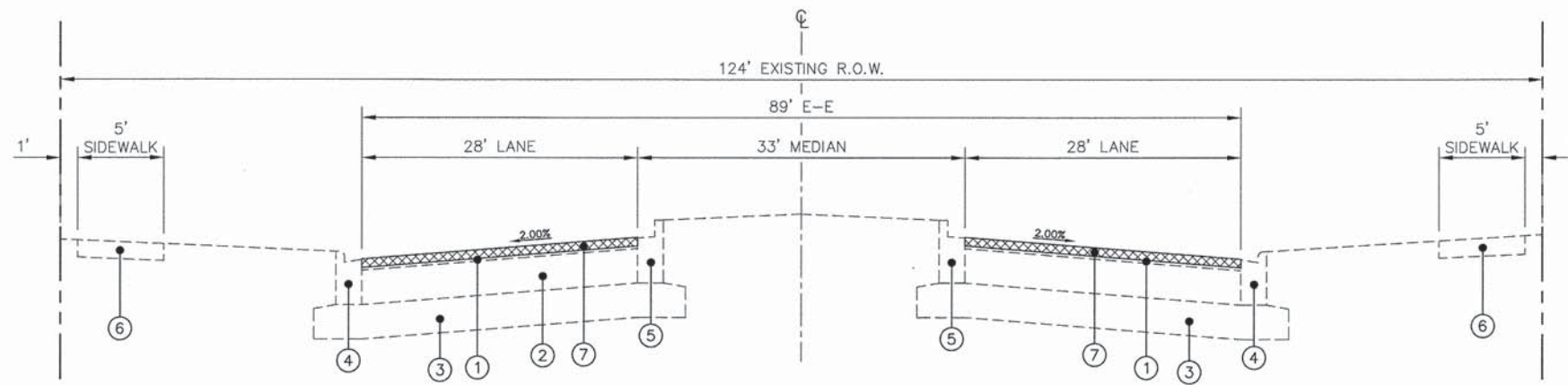
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS

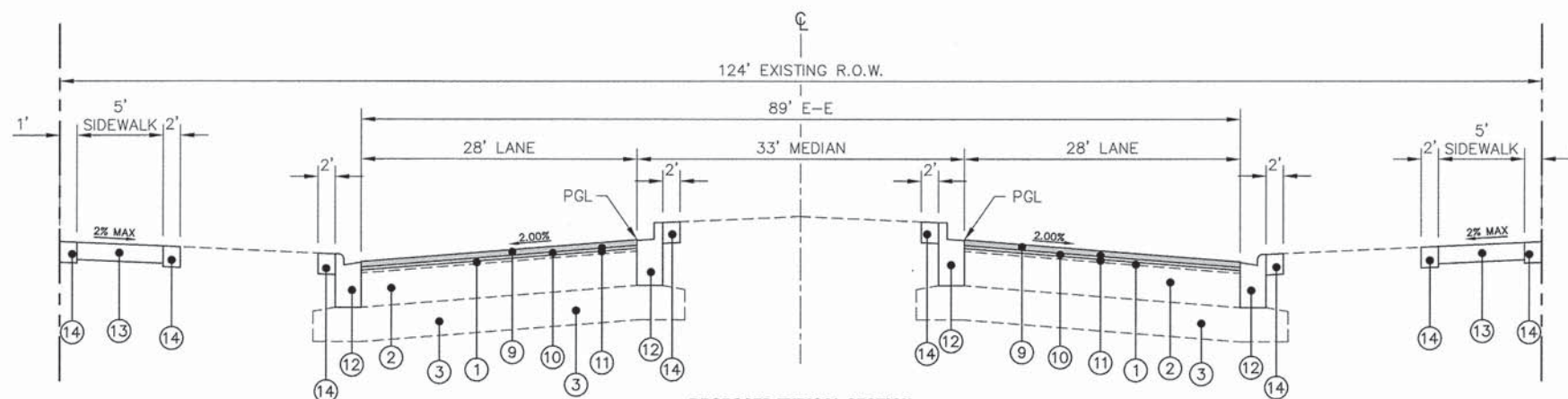
SCALE: N.T.S. SHEET NO. 3 OF 4 SHEETS STA. 100+00 TO STA. 101+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2548	15-00054-00-RS	KENDALL/KANE	20	6
CONTRACT NO. 61C37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

Path: S:\SR04\M01500\M01500\DWG\FINAL_ENG\M0150-CR



EXISTING TYPICAL SECTION
FROM STA 200+00 TO STA 202+00, BRIARCLIFF ROAD
N.T.S.



PROPOSED TYPICAL SECTION
FROM STA 200+00 TO STA 202+00, BRIARCLIFF ROAD
N.T.S.

LEGEND	
① EXISTING 1.25" TO 4.25" ASPHALT SURFACE COURSE	⑨ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2.0"
② EXISTING 1" TO 4.5" ASPHALT BINDER COURSE	⑩ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1.0"
③ EXISTING 9" TO 19" AGGREGATE BASE COURSE	⑪ BITUMINOUS MATERIALS (PRIME COAT)
④ EXISTING B-6.12 COMBINATION CONCRETE CURB AND GUTTER	⑫ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS)
⑤ EXISTING B-6.06 COMBINATION CONCRETE CURB AND GUTTER	⑬ SIDEWALK REMOVAL AND PORTLAND CEMENT CONCRETE SIDEWALK 5" (AT VARIOUS LOCATIONS)
⑥ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK, 4"	⑭ TOPSOIL AND SOD (LOCATIONS AS DIRECTED BY THE ENGINEER)
⑦ HOT-MIX ASPHALT SURFACE REMOVAL, 3"	
⑧ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	

Plotted: November 30, 2015 @ 10:02 AM By: Jim Schmidt - Tab: 07 Typ Section 22x34
 COPYRIGHT © 2015 ENGINEERING ENTERPRISES, INC.

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

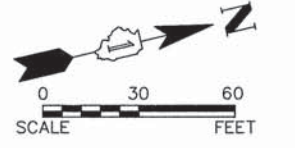
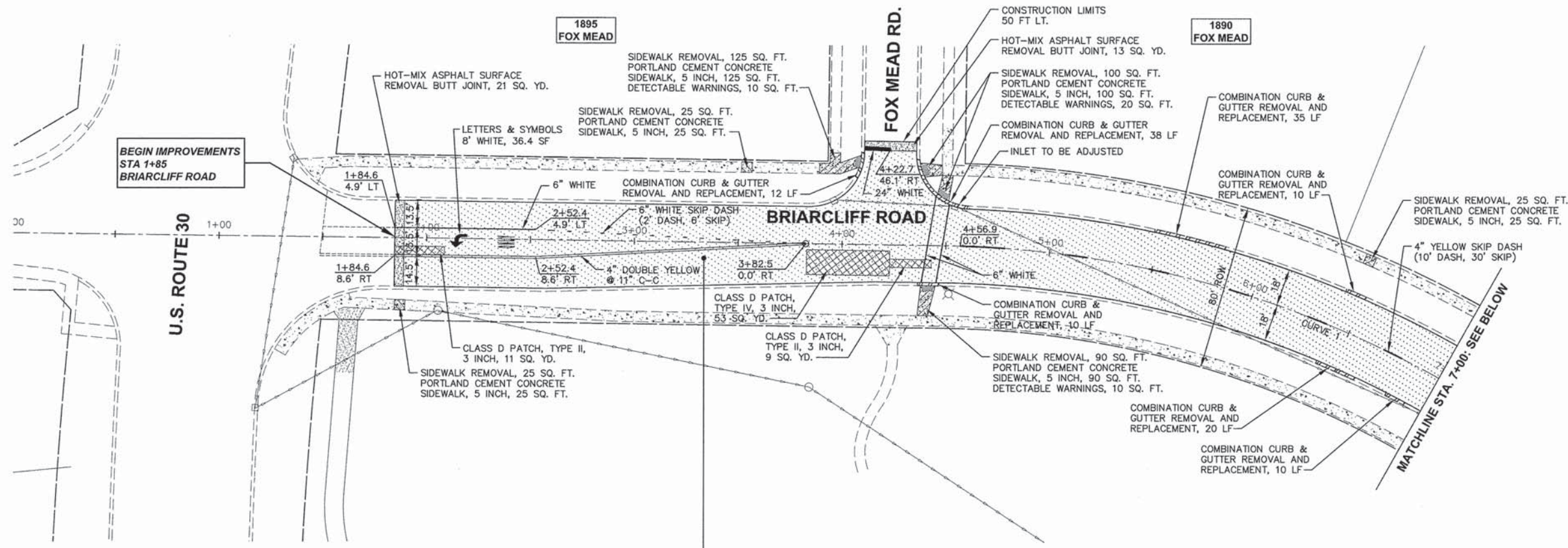
USER NAME =	DESIGNED - CMF	REVISED -
PLOT SCALE =	DRAWN - JPS/CLN/KKP	REVISED -
PLOT DATE =	CHECKED - TVW	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS	
SCALE: N.T.S.	SHEET NO. 4 OF 4 SHEETS
STA. 200+00	TO STA. 202+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2548	15-00054-00-RS	KENDALL/KANE	20	7
CONTRACT NO. 61C37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

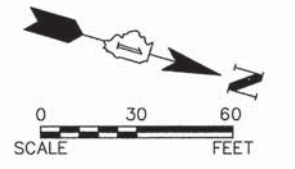
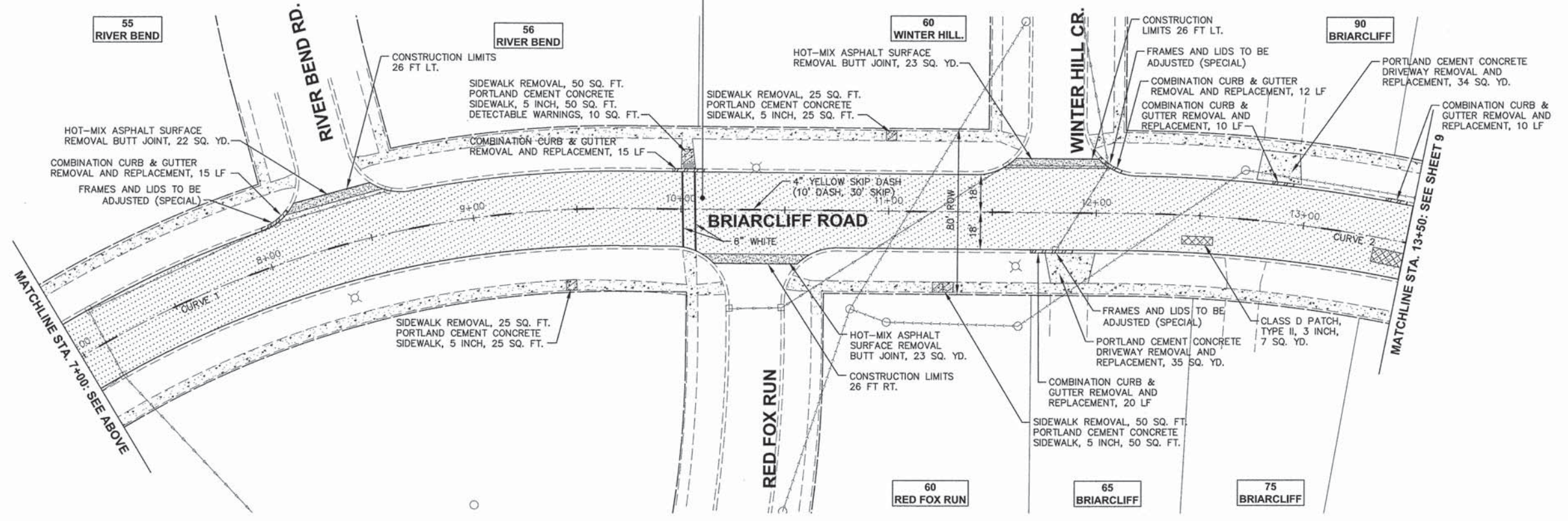
Path: H:\GIS\PROJ\MO1501\DWG\FINAL_ENG\MO1501-CR



CURVE 1
 P.I. STA. = 7+38.59
 $\Delta = 60^{\circ}09'41''$
 $D = 11^{\circ}27'33''$
 $R = 500.00'$
 $T = 289.62'$
 $L = 525.01'$
 $E = 77.82'$
 P.C. STA. = 4+48.97
 P.T. STA. = 9+73.98

CURVE 2
 P.I. STA. = 14+53.94
 $\Delta = 37^{\circ}28'46''$
 $D = 8^{\circ}05'55''$
 $R = 707.47'$
 $T = 240.01'$
 $L = 562.78'$
 $E = 39.60'$
 P.C. STA. = 12+13.93
 P.T. STA. = 16+76.71

HOT-MIX ASPHALT SURFACE REMOVAL, 3", 5015 SQ YD
 BITUMINOUS MATERIALS (PRIME COAT), 3385 POUNDS
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1", 281 TONS
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2", 562 TONS



Plotted: November 30, 2015 @ 10:09 AM By: Jim Schmidt - Tab: 08 Plan: 01 22-34
 Copyright © 2015 ENGINEERING ENTERPRISES, INC.

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 830.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - CMF	REVISED -
PLOT SCALE =	DRAWN - JPS/CLN/KKP	REVISED -
PLOT DATE =	CHECKED - TVW	REVISED -
	DATE -	REVISED -

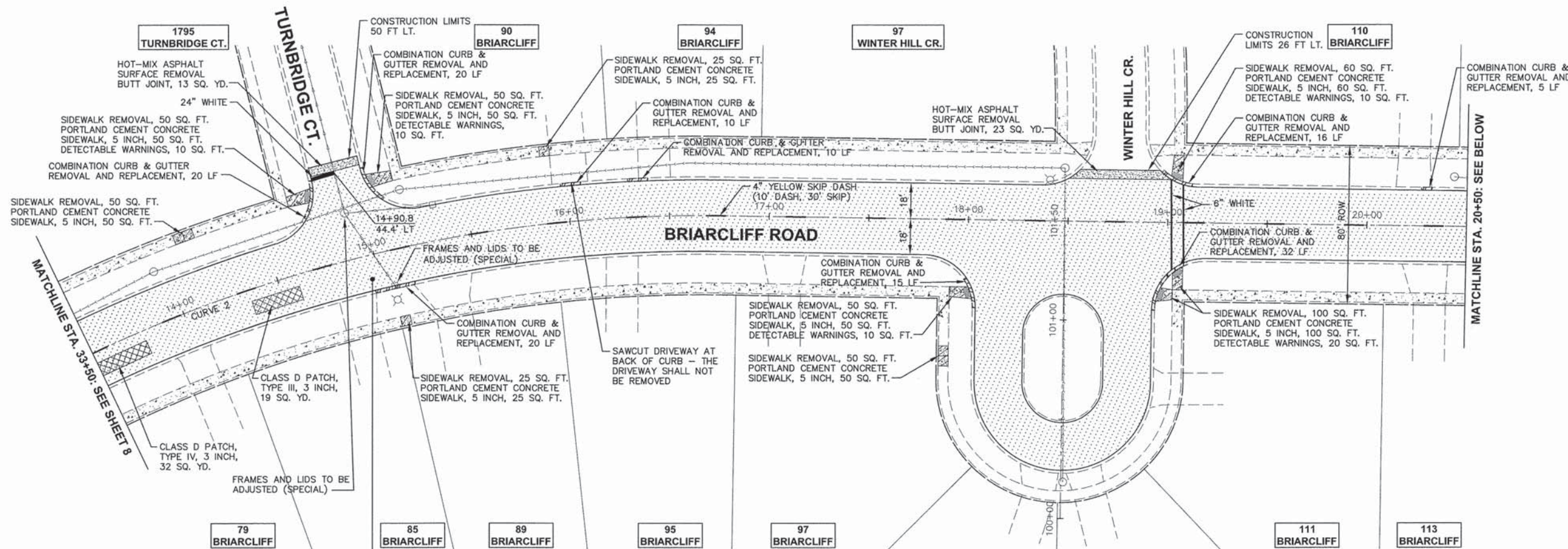
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN

SCALE: 1"=30' SHEET NO. 1 OF 5 SHEETS STA. 1+85 TO STA. 13+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2548	15-00054-00-RS	KENDALL/KANE	20	8
CONTRACT NO. 61C37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

Path: \\S:\PROJ\MO1500\MO1500\JWG_FINAL_ENG\MO1500-PLAN

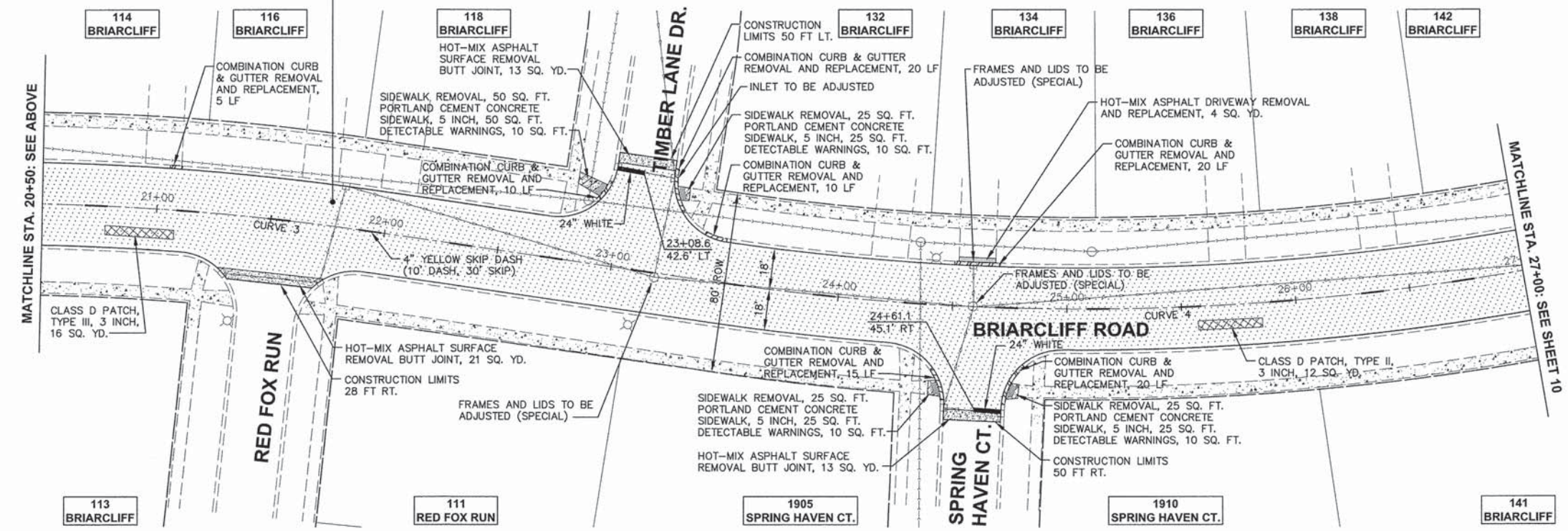


CURVE 2
P.I. STA. = 14+53.94
 $\Delta = 37^{\circ}28'46''$
 $D = 8^{\circ}05'55''$
 $R = 707.47'$
 $T = 240.01'$
 $L = 562.78'$
 $E = 39.60'$
P.C. STA. = 12+13.93
P.T. STA. = 16+76.71

CURVE 3
P.I. STA. = 21+16.28
 $\Delta = 7^{\circ}29'10''$
 $D = 4^{\circ}53'27''$
 $R = 1171.49'$
 $T = 76.64'$
 $L = 153.07'$
 $E = 2.50'$
P.C. STA. = 20+39.64
P.T. STA. = 21+92.70

CURVE 4
P.I. STA. = 25+26.13
 $\Delta = 18^{\circ}25'02''$
 $D = 5^{\circ}12'04''$
 $R = 1101.61'$
 $T = 178.59'$
 $L = 354.10'$
 $E = 14.38'$
P.C. STA. = 23+47.54
P.T. STA. = 27+01.64

- HOT-MIX ASPHALT SURFACE REMOVAL, 3", 6615 SQ YD
- BITUMINOUS MATERIALS (PRIME COAT), 4465 POUNDS
- POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1", 371 TONS
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2", 742 TONS



Plotted: November 30, 2015 @ 10:11 AM By: Jim Schmidt - Tab: 09 Plan 02 22-34
 Copyright © 2015 ENGINEERING ENTERPRISES, INC.

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

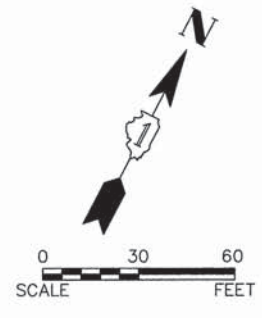
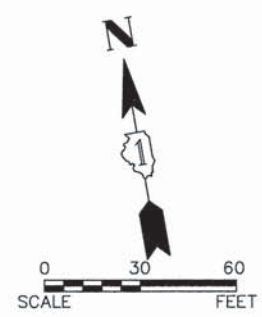
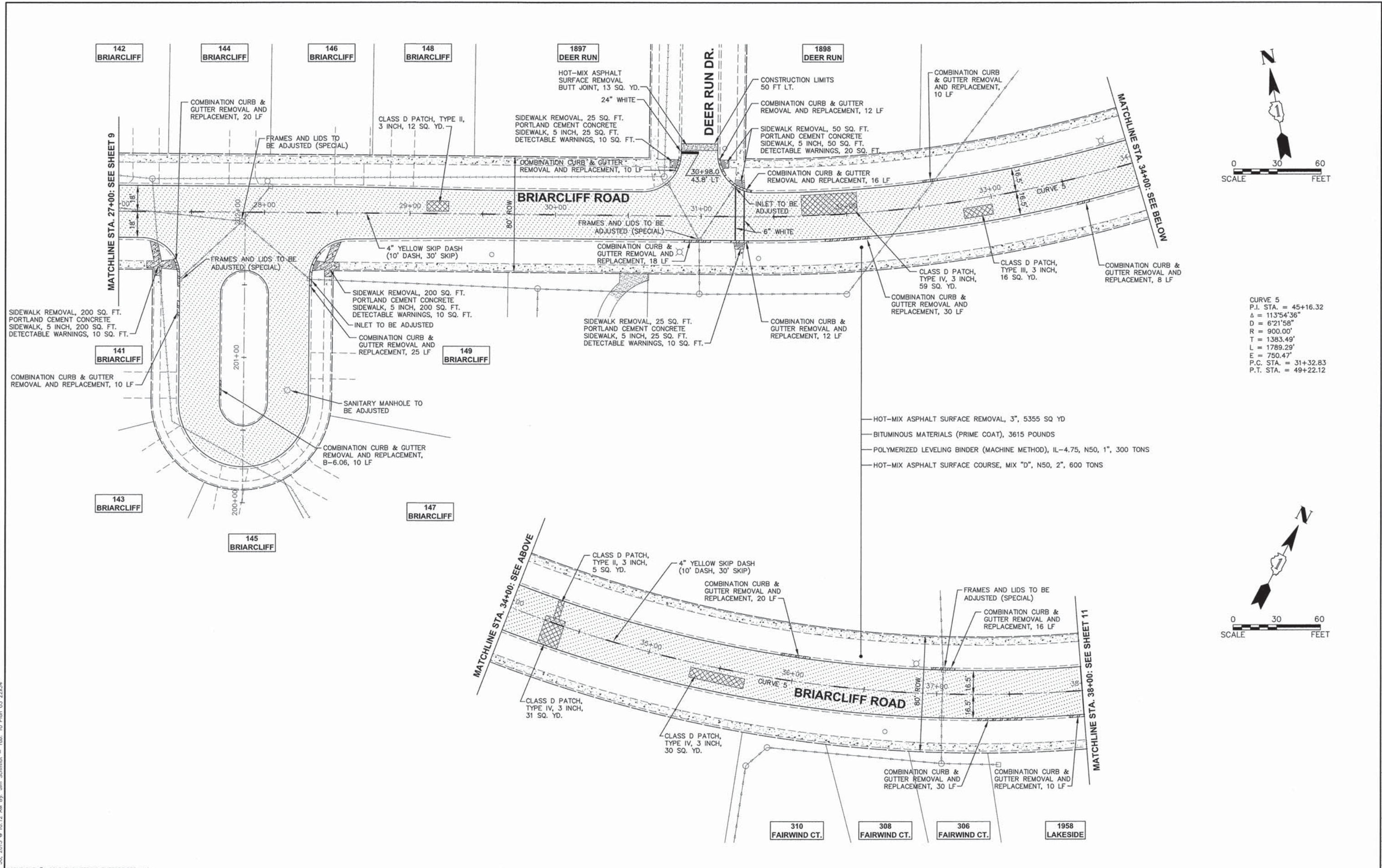
USER NAME =	DESIGNED - CMF	REVISED -
PLOT SCALE =	DRAWN - JPS/CLN/KKP	REVISED -
PLOT DATE =	CHECKED - TVW	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN

SCALE: 1"=30' SHEET NO. 2 OF 5 SHEETS STA. 13+50 TO STA. 27+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2548	15-00054-00-RS	KENDALL/KANE	20	9
CONTRACT NO. 61C37			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP	



Plotted: November 30, 2015 @ 10:12 AM By: Jim Schmidt - Tab: 10 Plan 03 22x34
 COPYRIGHT © 2015 ENGINEERING ENTERPRISES, INC.

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

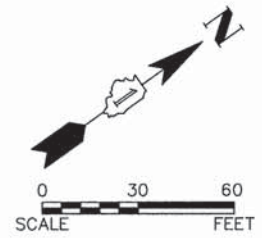
USER NAME =	DESIGNED - CMF	REVISED -
DRAWN - JPS/CLN/KKP	REVISED -	
CHECKED - TVW	REVISED -	
DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

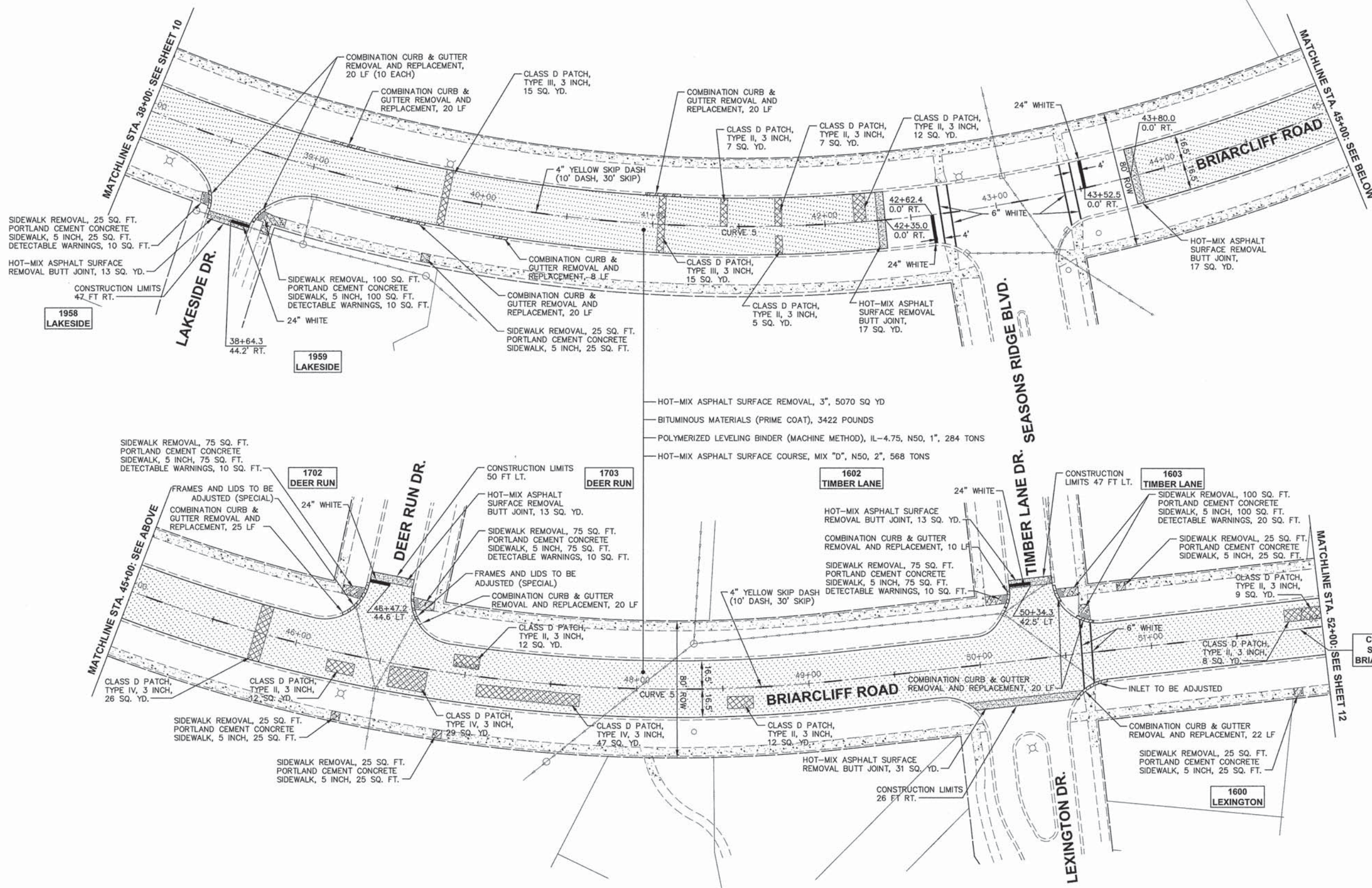
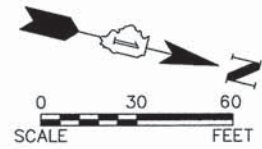
GENERAL PLAN
 SCALE: 1"=30'
 SHEET NO. 3 OF 5 SHEETS
 STA. 27+00 TO STA. 38+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2548	15-00054-00-RS	KENDALL/KANE	20	10
CONTRACT NO. 61C37			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP	

P:\14\15054\15054-00\15054-00-03.dwg FINAL ENG\15054-00-03.dwg



CURVE 5
 P.I. STA. = 45+16.32
 $\Delta = 113^{\circ}54'36''$
 $D = 6^{\circ}21'58''$
 $R = 900.00'$
 $T = 1383.49'$
 $L = 1789.29'$
 $E = 750.47'$
 P.C. STA. = 31+32.83
 P.T. STA. = 49+22.12



HOT-MIX ASPHALT SURFACE REMOVAL, 3", 5070 SQ YD
 BITUMINOUS MATERIALS (PRIME COAT), 3422 POUNDS
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1", 284 TONS
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2", 568 TONS

COUNTY LINE
 STA 51+86.48
 BRIARCLIFF ROAD

Plotted: November 30, 2015 @ 10:39 AM By: Jim Schmidt - Tab: 11 Plan 04 22x34

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - CMF	REVISED -
PLOT SCALE =	DRAWN - JPS/CLN/KKP	REVISED -
PLOT DATE =	CHECKED - TVW	REVISED -
	DATE -	REVISED -

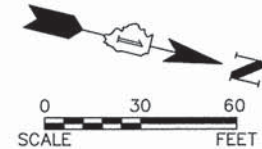
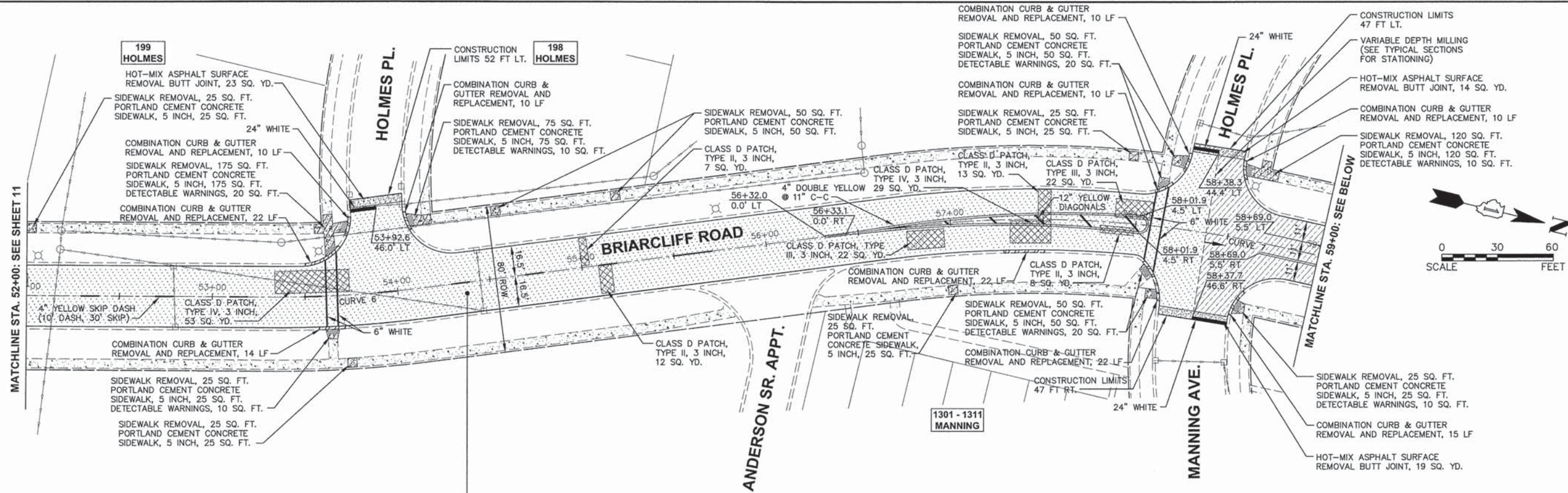
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

GENERAL PLAN

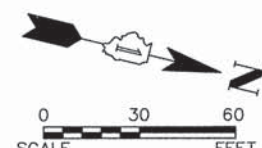
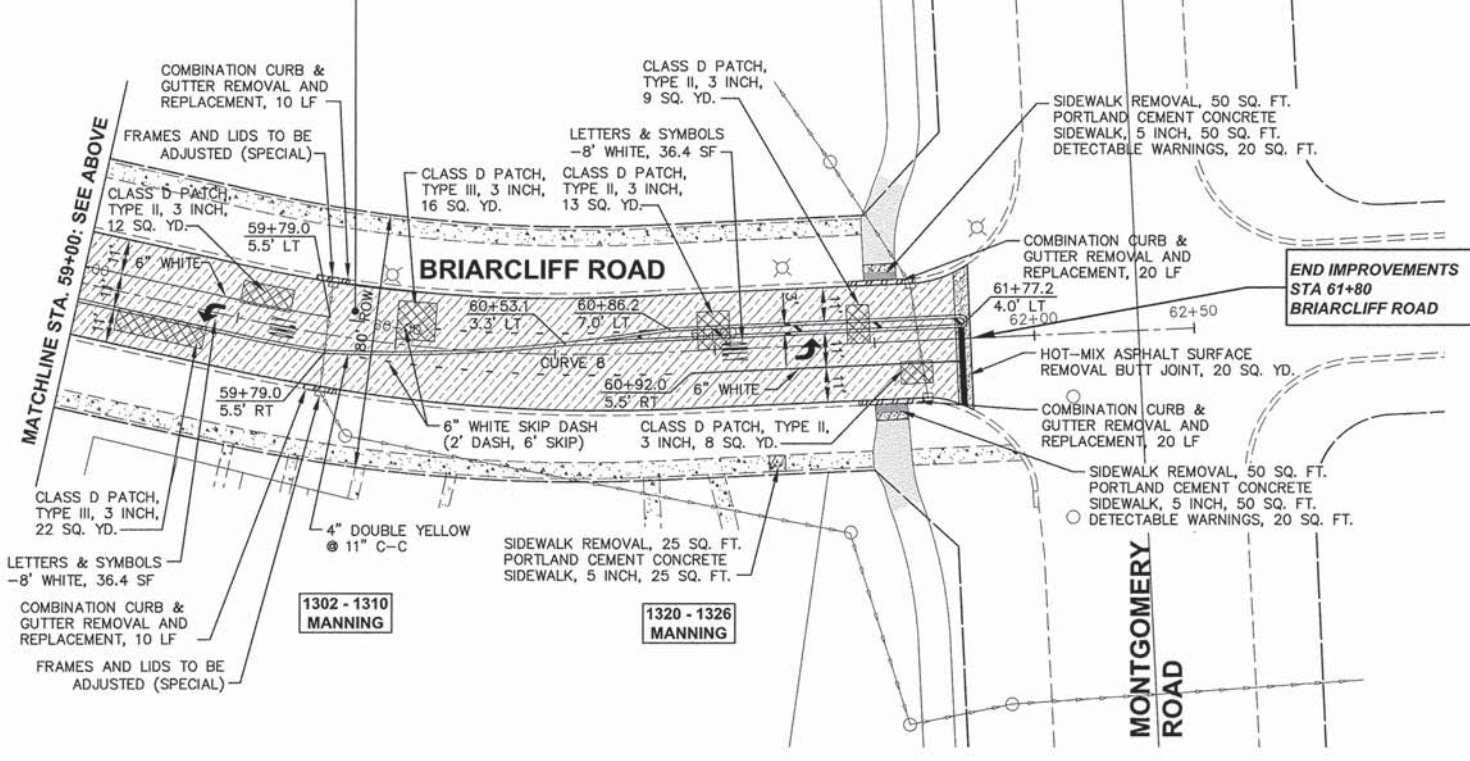
SCALE: 1"=30' SHEET NO. 4 OF 5 SHEETS STA. 38+00 TO STA. 52+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2548	15-00054-00-RS	KENDALL/KANE	20	11
CONTRACT NO. 61C37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

Path: V:\SRP\1501500\1501501\DWG\FINAL_ENG\1501501D-PLAN



—HOT-MIX ASPHALT SURFACE REMOVAL, 3", 2640 SQ YD
 —HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH, 1442 SQ YD
 —BITUMINOUS MATERIALS (PRIME COAT), 2756 POUNDS
 —POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1", 229 TONS
 —HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2", 458 TONS



CURVE 6
 P.I. STA. = 53+72.97
 $\Delta = 7'45'40''$
 $D = 9'56'17''$
 $R = 576.53'$
 $T = 39.11'$
 $L = 78.09'$
 $E = 1.32'$
 P.C. STA. = 53+33.86
 P.T. STA. = 54+11.96

CURVE 7
 P.I. STA. = 57+72.78
 $\Delta = 19'59'46''$
 $D = 9'32'57''$
 $R = 600.00'$
 $T = 105.78'$
 $L = 209.40'$
 $E = 9.25'$
 P.C. STA. = 56+67.01
 P.T. STA. = 58+76.41

CURVE 8
 P.I. STA. = 60+13.93
 $\Delta = 15'19'59''$
 $D = 11'27'33''$
 $R = 500.00'$
 $T = 67.30'$
 $L = 133.81'$
 $E = 4.51'$
 P.C. STA. = 59+46.63
 P.T. STA. = 60+80.43

Plotted: November 30, 2015 @ 10:40 AM By: Jim Schmidt - Tab: 12 Plan 05 22-34

COPYRIGHT © 2015 ENGINEERING ENTERPRISES, INC.
Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

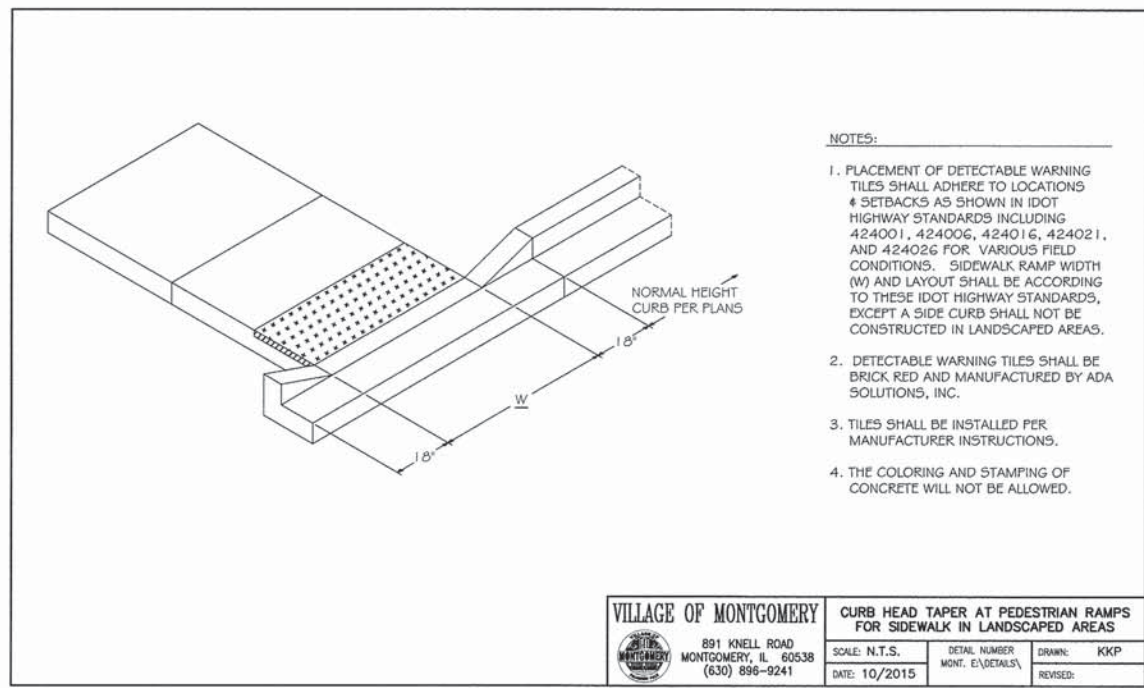
USER NAME =	DESIGNED - CMF	REVISED -
DRAWN - JPS/CLN/KKP	REVISED -	
CHECKED - TVW	REVISED -	
DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION


GENERAL PLAN
 SCALE: 1"=30'
 SHEET NO. 5 OF 5 SHEETS
 STA. 52+00 TO STA. 62+00

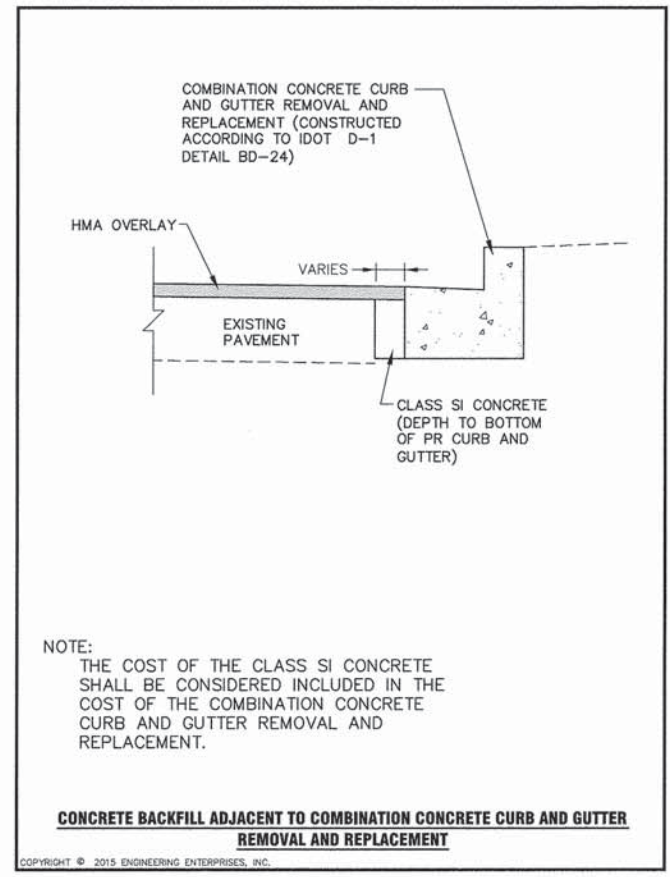
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2548	15-00054-00-RS	KENDALL/KANE	20	12
CONTRACT NO. 61C37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

Path: \\SBSRPROD\MOT500\MOT500\DWG_FINAL_ENG\MOT500-PLAN



- NOTES:**
1. PLACEMENT OF DETECTABLE WARNING TILES SHALL ADHERE TO LOCATIONS & SETBACKS AS SHOWN IN IDOT HIGHWAY STANDARDS INCLUDING 424001, 424006, 424016, 424021, AND 424026 FOR VARIOUS FIELD CONDITIONS. SIDEWALK RAMP WIDTH (W) AND LAYOUT SHALL BE ACCORDING TO THESE IDOT HIGHWAY STANDARDS, EXCEPT A SIDE CURB SHALL NOT BE CONSTRUCTED IN LANDSCAPED AREAS.
 2. DETECTABLE WARNING TILES SHALL BE BRICK RED AND MANUFACTURED BY ADA SOLUTIONS, INC.
 3. TILES SHALL BE INSTALLED PER MANUFACTURER INSTRUCTIONS.
 4. THE COLORING AND STAMPING OF CONCRETE WILL NOT BE ALLOWED.

VILLAGE OF MONTGOMERY  891 KNEEL ROAD MONTGOMERY, IL 60538 (630) 896-9241	CURB HEAD TAPER AT PEDESTRIAN RAMPS FOR SIDEWALK IN LANDSCAPED AREAS		
	SCALE: N.T.S. DATE: 10/2015	DETAIL NUMBER: MONT. E\DETAILS\	DRAWN: KKP REVISED:



NOTE:
 THE COST OF THE CLASS SI CONCRETE SHALL BE CONSIDERED INCLUDED IN THE COST OF THE COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.

CONCRETE BACKFILL ADJACENT TO COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

COPYRIGHT © 2015 ENGINEERING ENTERPRISES, INC.

Plotted: November 30, 2015 @ 10:41 AM By: Jim Schmidt - Tab: 13 Details 22x34


Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

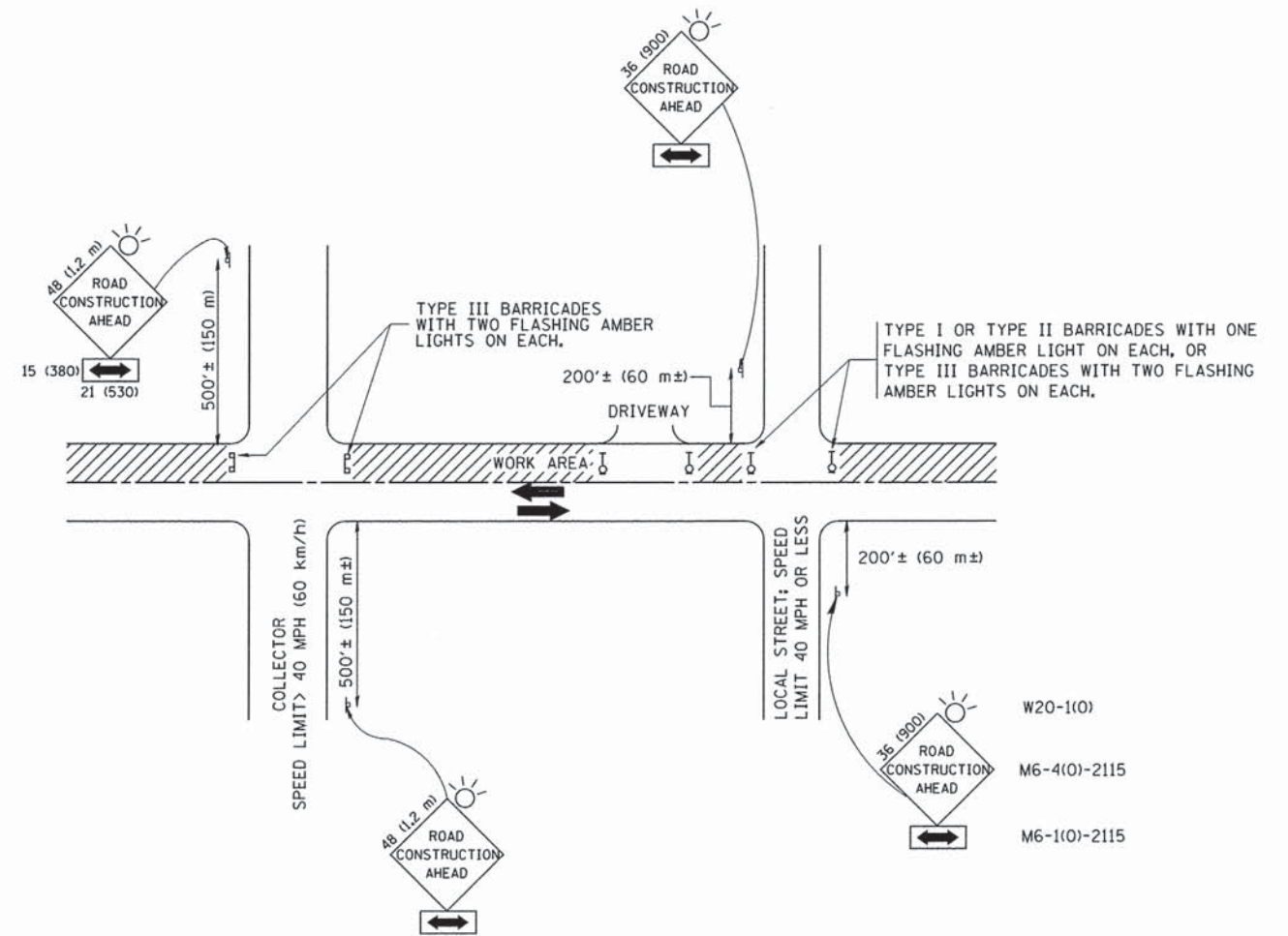
USER NAME =	DESIGNED - CMF	REVISED -
	DRAWN - JPS/CLN/KKP	REVISED -
PLOT SCALE =	CHECKED - TVW	REVISED -
PLOT DATE =	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS	
SCALE: N.T.S.	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2548	15-00054-00-RS	KENDALL/KANE	20	13
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 61C37	
FED. AID PROJECT - STP				

Path: C:\SHPROJ\150500\M01500\DWG_FINAL\ENG\M0150-CR



W20-1(0)
M6-4(0)-2115
M6-1(0)-2115

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

- a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

- a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

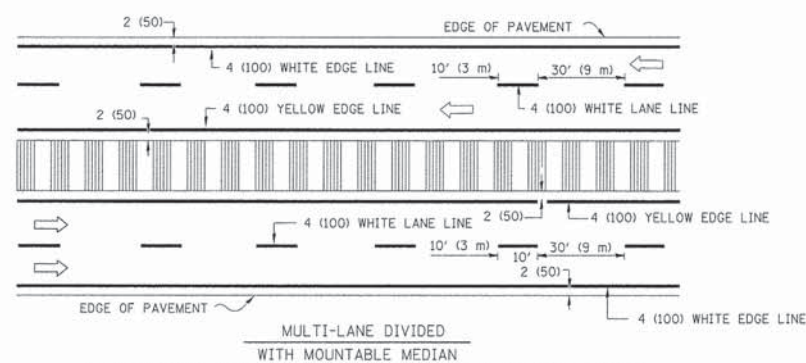
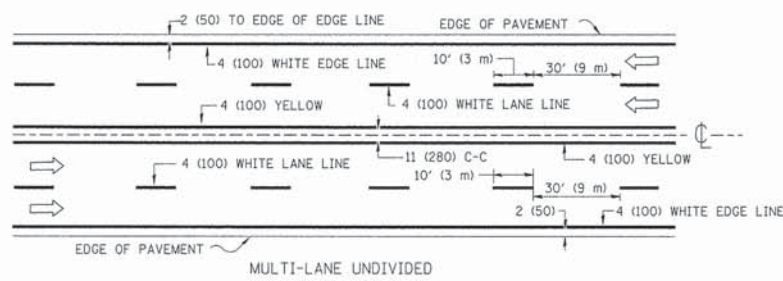
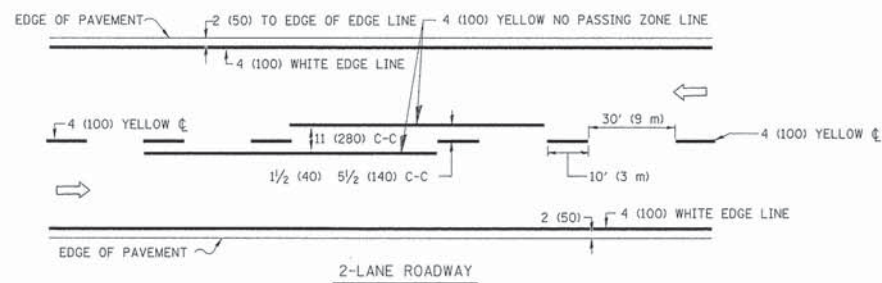
FILE NAME = W:\diststd\22x34\to10.dgn	USER NAME = geglinoht	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 "/ IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2000	DATE - 06-89	REVISED - T. RAMMACH 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

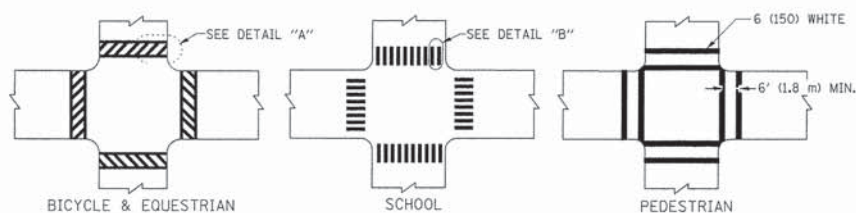
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2548	15-00054-00-RS	KENDALL/KANE	20	14
TC-10			CONTRACT NO. 61C37	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

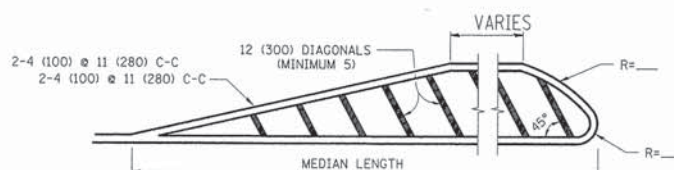
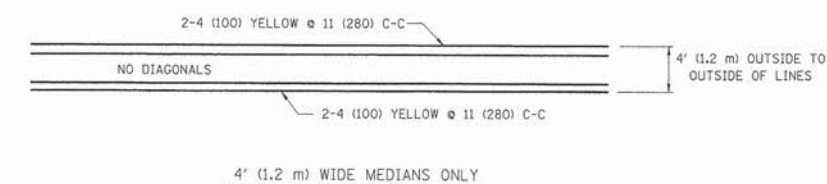


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

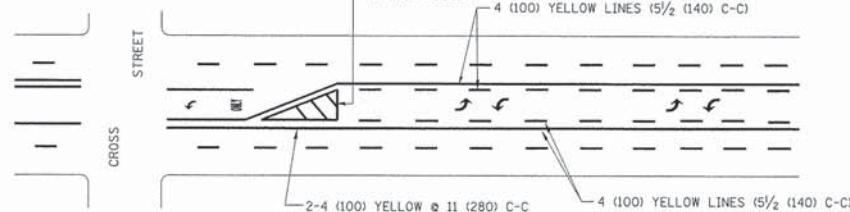


TYPICAL CROSSWALK MARKING



FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

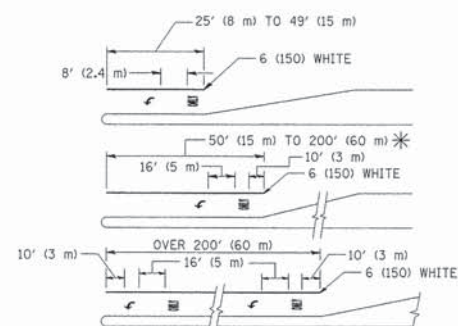


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

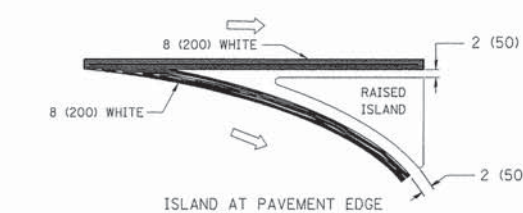
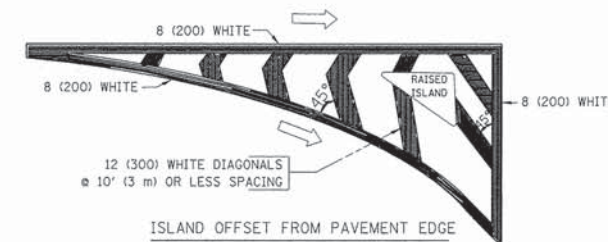
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100)	SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
ON FREEWAYS	5 (125)	SKIP-DASH	WHITE	
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN)	2 @ 6 (150)	SOLID	WHITE	NOT LESS THAN 6' (1.8 m) APART
A. DIAGONALS (BIKE & EQUESTRIAN)	12 (300) @ 45°	SOLID	WHITE	2' (600) APART
B. LONGITUDINAL BARS (SCHOOL)	12 (300) @ 90°	SOLID	WHITE	2' (600) APART
				SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

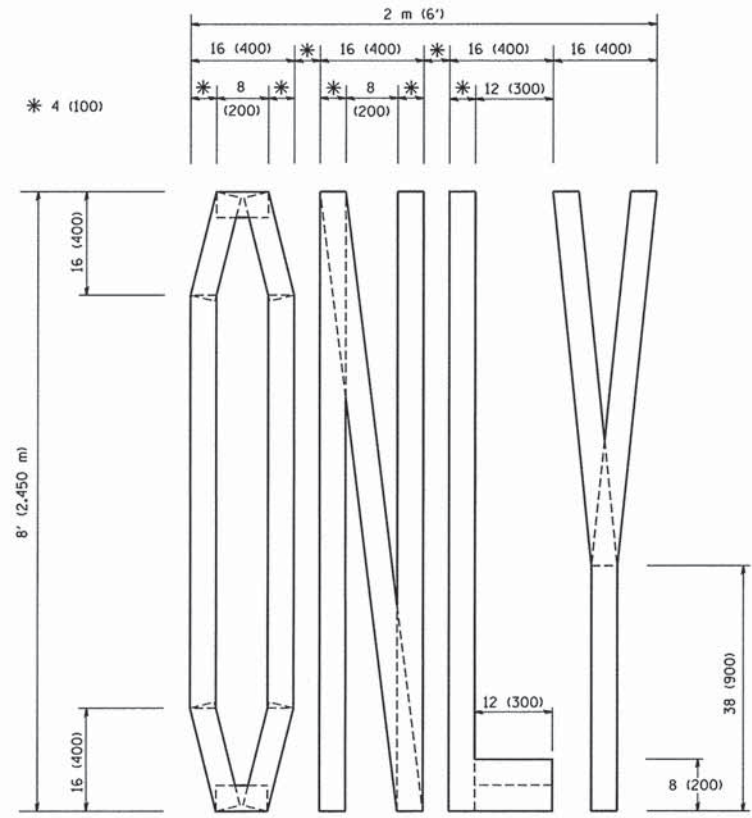
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

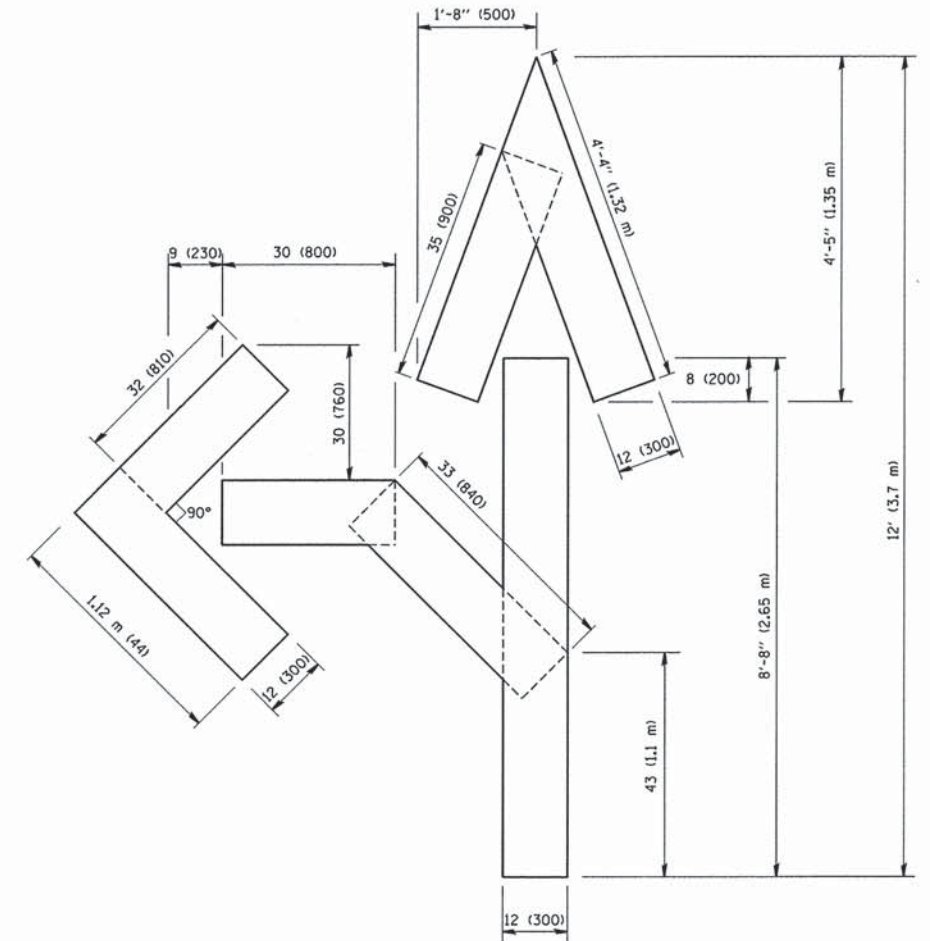
FILE NAME =	USER NAME = drvakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
c:\pwork\pwork\drvakosgn\d0108315\to	3.dgn	DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50.000 "/ IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

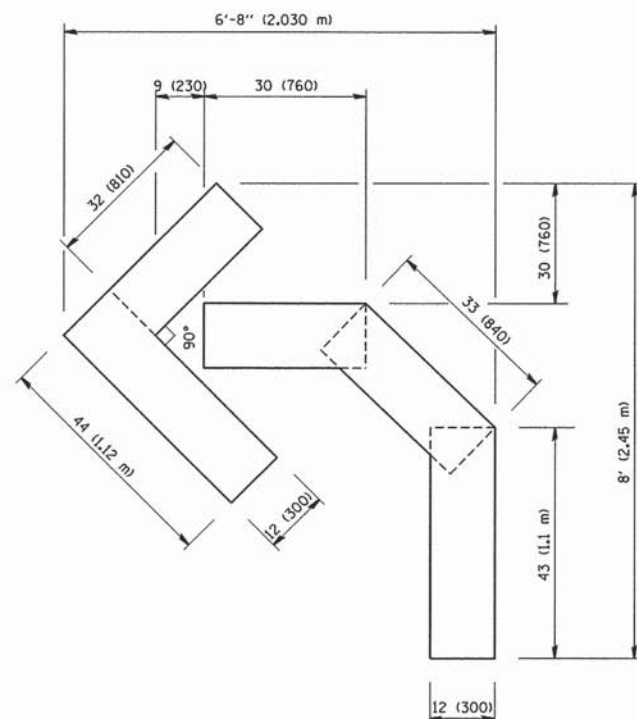
DISTRICT ONE		F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		2548	15-00054-00-RS	KENDALL/KANE	20	15
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.	CONTRACT NO. 61C37	
		FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

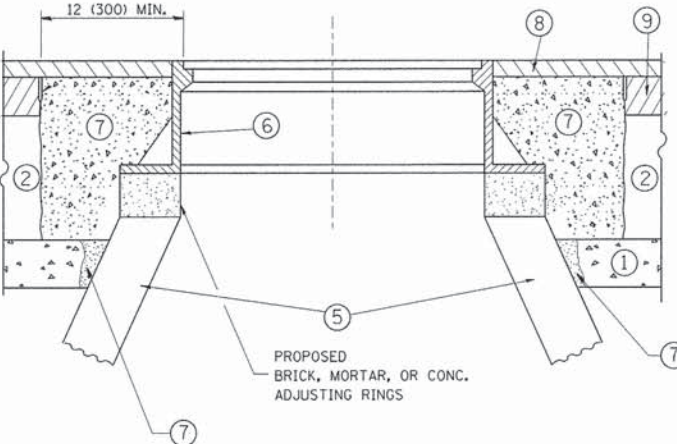
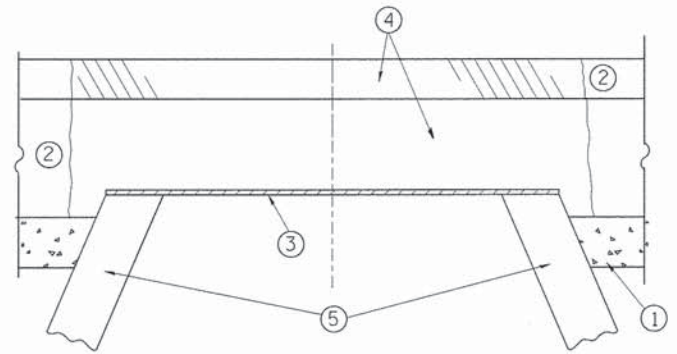
FILE NAME = W:\diststd\22x34\tc16.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2548	SECTION 15-00054-00-RS	COUNTY KENDALLUKANE	TOTAL SHEETS 20	SHEET NO. 16
TC-16			CONTRACT NO. 61C37	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

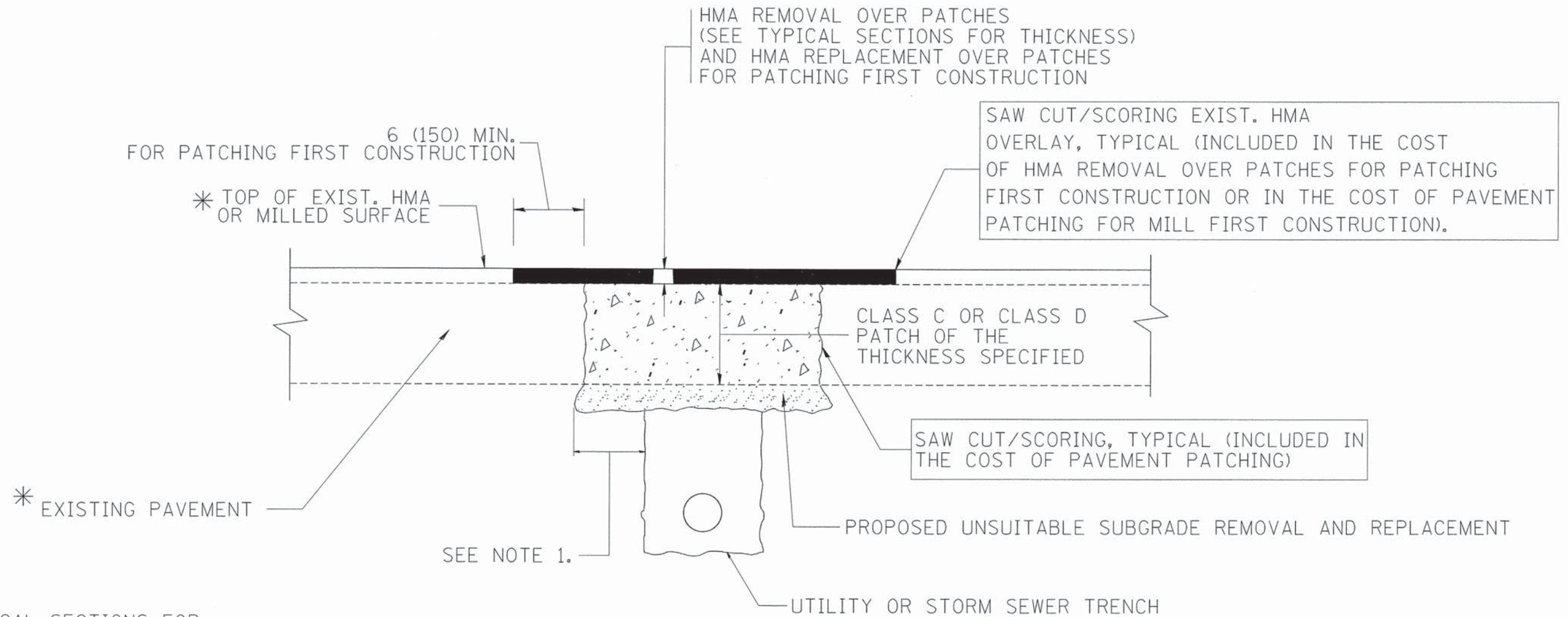
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
es:\pw_work\pwsdot\bauerdl\140109315\bd08.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 1/8" = 1'-0"	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE. 2548	SECTION 15-00054-00-RS	COUNTY KENDALL/KANE	TOTAL SHEETS 20	SHEET NO. 17
BD600-03 (BD-8)		CONTRACT NO. 61C37		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

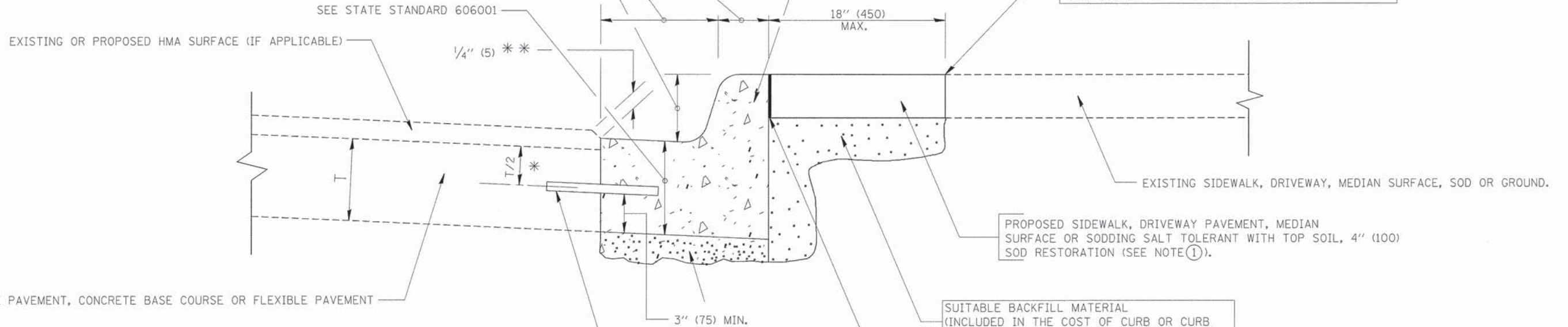
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bawerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A.U. RTE. 2548	SECTION 15-00054-00-RS	COUNTY KENDALL/KANE	TOTAL SHEETS 20	SHEET NO. 18
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08						BD400-04 (BD-22) CONTRACT NO. 61C37	

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

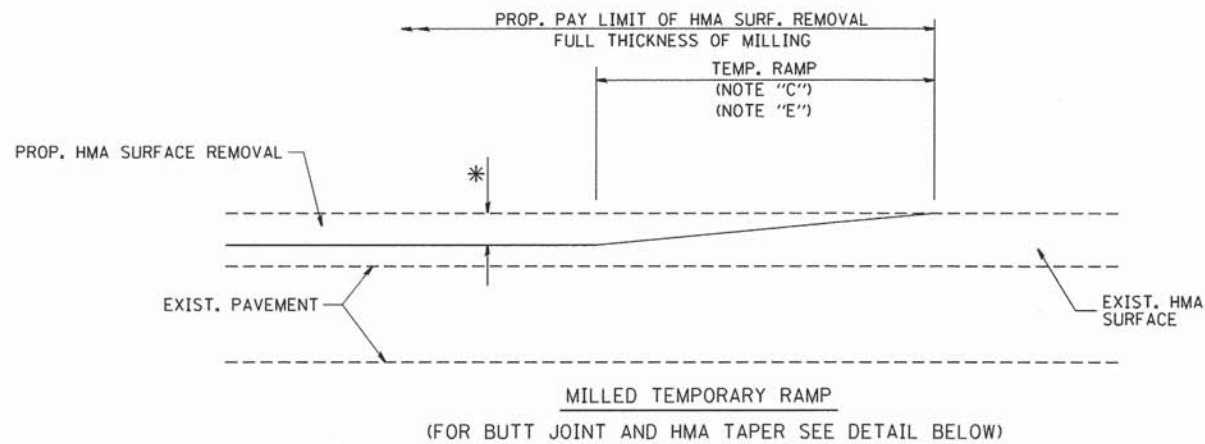
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

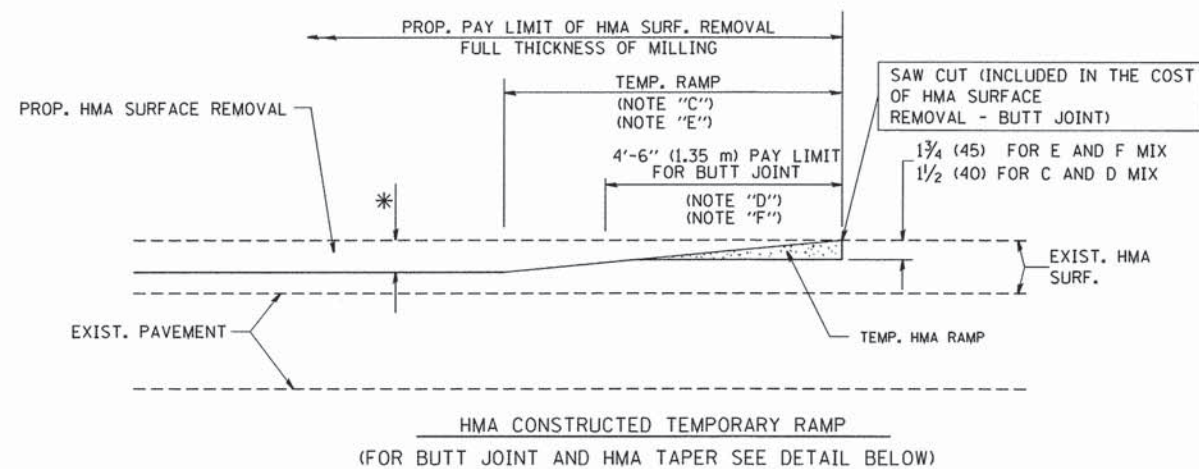
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

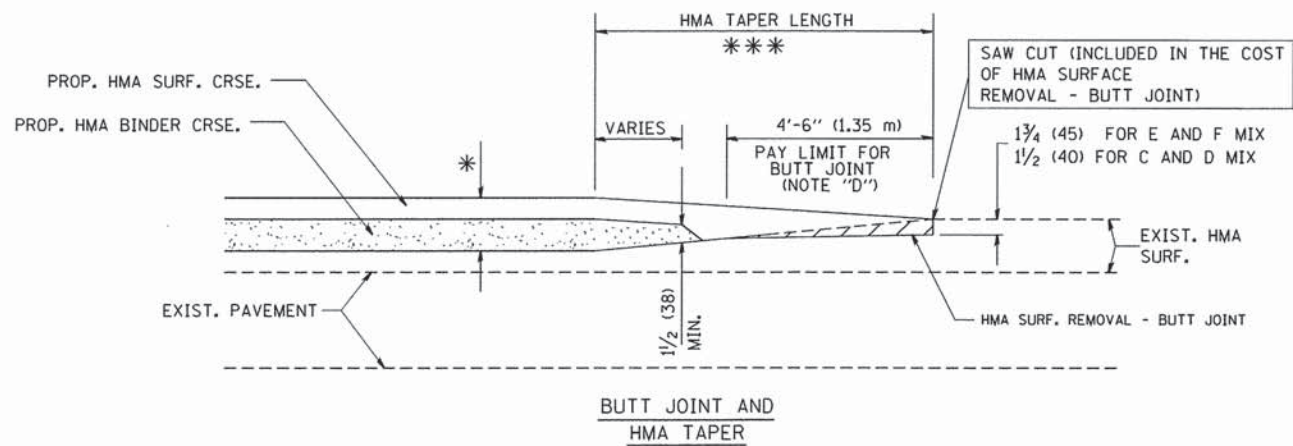
FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cc:\pwork\psidot\drivakosgn\d0100315\bd24.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01			2548	15-00054-00-RS	KENDALL/KANE	20	19
PLOT SCALE = 50,000 ' / IN.	CHECKED -	REVISED - R. BORO 12-15-09				BD600-06 (BD-24)		CONTRACT NO. 61C37		
PLOT DATE = 12/15/2009	DATE - 03-11-94					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



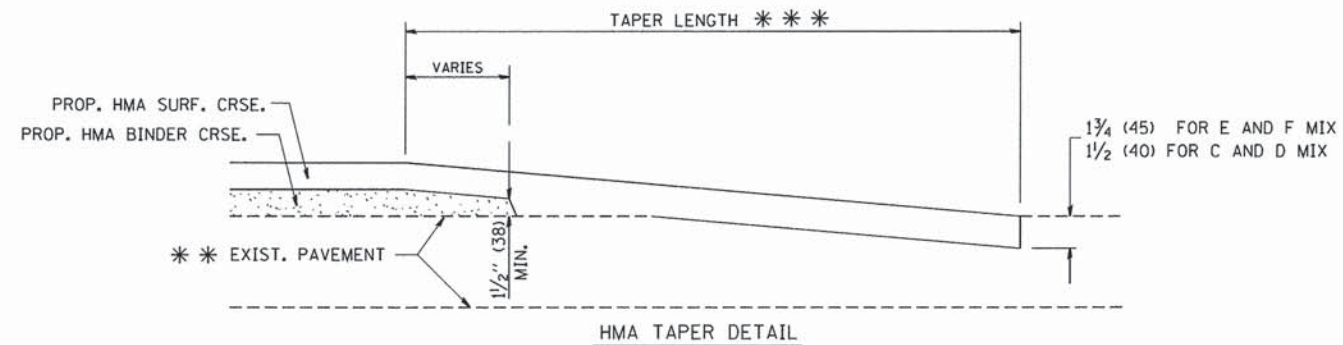
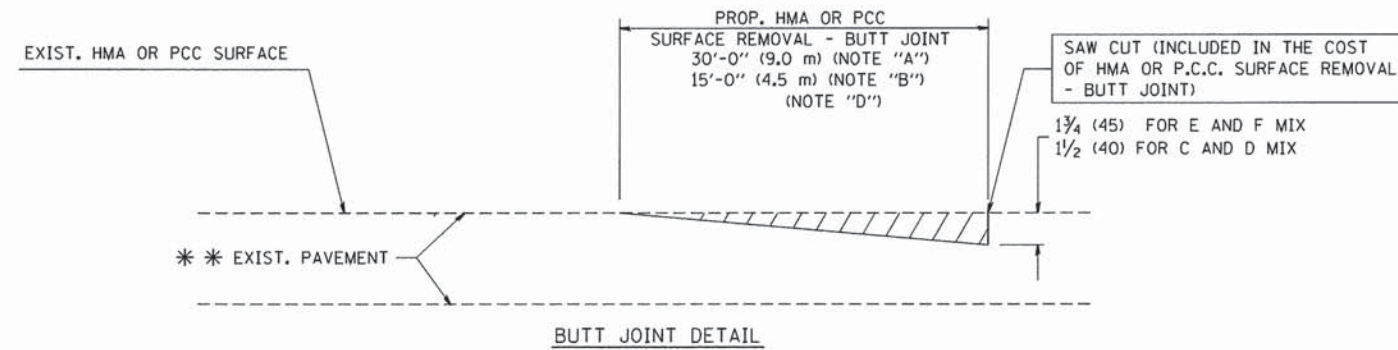
OPTION 1



OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = geglienobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50,0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE. 2548	SECTION 15-00054-00-RS	COUNTY KENDALLKANE	TOTAL SHEETS 20	SHEET NO. 20
BD400-05 BD32		CONTRACT NO. 61C37		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				