

03-04-2016 LETTING ITEM 116

| F.A.U. RTE.  | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------|----------------|--------|--------------|-----------|
| 362          | 14-F3000-12-BT | DUPAGE | 69           | 1         |
| CONTRACT NO. |                |        | 61C31        |           |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED  
FEDERAL AID PROJECT**

**FAU ROUTE 362 (COUNTY FARM ROAD) BRIDGE AND TRAIL IMPROVEMENTS**

**OVER COUNTY FARM ROAD**

**SECTION 14-F3000-12-BT**

**PROJECT TE-M-4003(972)**

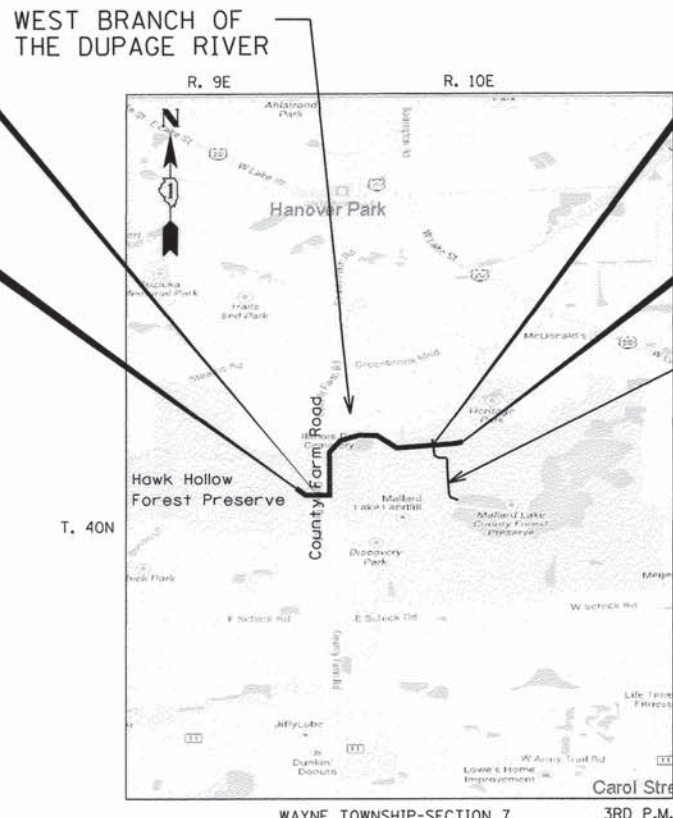
**FOREST PRESERVE DISTRICT OF DUPAGE COUNTY**

**DUPAGE COUNTY**

**JOB NO. C-91-219-14**

**FOR INDEX OF SHEETS, SEE SHEET NO. 2**

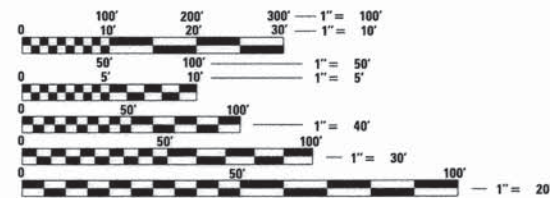
DESIGN DESIGNATION = OFF SYSTEM BIKE PATH  
COUNTY FARM ROAD TRAFFIC = 27,300 ADT 2012  
COUNTY FARM ROAD SPEED LIMIT = 40 MPH  
COUNTY FARM ROAD FUNCTIONAL CLASSIFICATION = PRINCIPAL ARTERIAL



**LOCATION MAP**

NOT TO SCALE

PROJECT LENGTH (GROSS /NET) = 5,167.75 (0.98 MILES)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

**CONTRACT NO. 61C31**



12-17-2015  
MATTHEW D. SANTEFORD, P.E., S.E.  
NO. 081-007244  
EXP. DATE 11/30/2016  
(SHEETS 26 - 43)



12-17-2015  
DAVID W. BLOCK, P.E.  
NO. 062-050966  
EXP. DATE 11/30/17  
(SHEETS 1 - 25 AND 44 - 69)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED ON: DECEMBER 17, 2015  
*[Signature]*  
FOREST PRESERVE DISTRICT OF DUPAGE COUNTY

PASSED 12-24-15  
*[Signature]*  
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID  
BASED ON LIMITED  
REVIEW DECEMBER 30, 2015, 2016  
*[Signature]*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

1475 EAST WOODFIELD ROAD, SUITE 600  
SCHAMBURG, ILLINOIS 60173  
(847) 605-6800  
FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E., PTOE, (847) 705-4021, SCHAMBURG, IL





**GENERAL NOTES**

1. ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2012.
2. ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
3. PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. THE CONTRACTOR MUST VERIFY THE ENGINEER'S LINE AND GRADE STAKES. IF THERE ARE DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, HE MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANY WORK, OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
4. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
5. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1, THE VILLAGE OF HANOVER PARK AT 630-823-5600, AND THE FOREST PRESERVE DISTRICT OF DUPAGE COUNTY AT 630-871-6404 FIVE DAYS PRIOR TO EXCAVATION FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED). THE CONTRACTOR SHALL ALSO CONTACT THE VILLAGE OF HANOVER PARK PUBLIC WORKS DEPARTMENT WATER DIVISION AT 630-823-5700 48 HOURS PRIOR TO ALL WATER MAIN SHUTOFFS.
6. SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.
7. OFFSET LOCATIONS GIVEN IN THE PLANS ARE FROM THE TRAIL CENTERLINE.
8. WHEN REMOVING PAVEMENT, CURB AND GUTTER, SHOULDER, AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS WHICH MIGHT DAMAGE THE UNDERGROUND PUBLIC OR PRIVATE UTILITIES WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.

**UTILITIES NOTES**

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
2. THE LOCATION OF EXISTING UNDERGROUND DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ALL UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER IN ACCORDANCE WITH ARTICLES 105.07 AND 107.20.
4. ALL UTILITY COMPANIES SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
5. THE CONTRACTOR SHALL USE NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
6. THE CONTRACTOR SHALL ENSURE THAT ALL WATER SYSTEM VALVES, VALVE VAULTS, FIRE HYDRANTS AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.
7. ALL LOOSE MATERIAL FROM CONSTRUCTION ACTIVITIES DEPOSITED IN THE FLOWLINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICE OF THE STABILIZED CONSTRUCTION ENTRANCE.

**INDEX OF SHEETS**

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| 1     | TITLE SHEET  |
| 2     | INDEX OF SHEETS, GENERAL NOTES AND STATE STANDARDS |
| 3     | SUMMARY OF QUANTITIES                              |
| 4     | TYPICAL SECTIONS                                   |
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| 15-16 | TRAIL PROFILES                                     |
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| 44-69 | CROSS SECTIONS                                     |

**STATE STANDARDS**

| STD. NO.  | DESCRIPTION   |
|-----------|---|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS                                  |
| 280001-07 | TEMPORARY EROSION CONTROL SYSTEMS   |
| 515001-03 | NAME PLATE FOR BRIDGES  |
| 542301-03 | PRECAST REINFORCED CONCRETE FLARED END SECTION                                |
| 601101-01 | CONCRETE HEADWALL FOR PIPE DRAIN  |
| 606001-06 | CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER                 |
| 701101-04 | OFF-ROAD OPERATIONS, MULTILANE 15" (4.5 m) TO 24" (600 MM) FROM PAVEMENT EDGE |
| 701427-03 | LANE CLOSURE, MULTILANE INTERMITTENT OR MOVING OPER., FOR SPEEDS <= 40 MPH    |
| 701606-10 | URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN                |
| 701901-04 | TRAFFIC CONTROL DEVICES   |
| 720001-01 | SIGN PANEL MOUNTING DETAILS   |
| 720006-04 | SIGN PANEL ERECTION DETAILS   |
| 720011-01 | METAL POSTS FOR SIGNS, MARKERS & DELINEATORS                                  |
| 729001-01 | APPLICATIONS OF TYPES A & B METAL POSTS (FOR SIGNS & MARKERS)                 |

|   |                       |                |                   |   |  |                         |              |                     |                           |           |
|---|-----------------------|----------------|-------------------|---|--|-------------------------|--------------|---------------------|---------------------------|-----------|
| FILE NAME = G:\CH\3\0895\Road\Sheets\38095-SHT-NOTE | USER NAME = bshae1ger | DESIGNED = MLB | REVISED =         | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS<br/>INDEX OF SHEETS, GENERAL NOTES AND STATE STANDARDS</b> | F.A.U. RTE.             | SECTION      | COUNTY              | TOTAL SHEETS              | SHEET NO. |
| PLOT SCALE =  | CHECKED = DWB         | REVISED =      | 362               |   |  | 14-F3000-12-BT          | DUPAGE       | 69                  | 2                         |           |
| PLOT DATE = 12/16/2015                              | DATE = 12/7/2015      | REVISED =      | CONTRACT NO 61C31 |   |  |                         |              |                     |                           |           |
|   |                       |                | SCALE: NTS        |   |  | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT |           |











PROP. CURVE PRBIKE-1  
 PI STA. = 99+21.03  
 $\Delta = 31^\circ 09' 12''$  (RT)  
 $D = 19^\circ 13' 24''$   
 $R = 298.05'$   
 $T = 83.09'$   
 $L = 162.06'$   
 $E = 11.36'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 98+37.94  
 P.T. STA = 100+00.00

PROP. CURVE PRBIKE-4  
 PI STA. = 112+16.02  
 $\Delta = 3^\circ 50' 18''$  (RT)  
 $D = 11^\circ 27' 33''$   
 $R = 500.00'$   
 $T = 16.75'$   
 $L = 33.49'$   
 $E = 0.28'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 111+99.27  
 P.T. STA = 112+32.76

PROP. CURVE PRBIKE-7  
 PI STA. = 118+61.35  
 $\Delta = 2^\circ 36' 36''$  (LT)  
 $D = 8^\circ 11' 06''$   
 $R = 700.00'$   
 $T = 15.95'$   
 $L = 31.89'$   
 $E = 0.18'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 118+45.41  
 P.T. STA = 118+77.29

PROP. CURVE PRBIKE-10  
 PI STA. = 122+84.70  
 $\Delta = 60^\circ 50' 36''$  (RT)  
 $D = 57^\circ 17' 45''$   
 $R = 100.00'$   
 $T = 58.72'$   
 $L = 106.19'$   
 $E = 15.97'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 122+25.97  
 P.T. STA = 123+32.17

PROP. CURVE PRBIKE-2  
 PI STA. = 101+87.32  
 $\Delta = 42^\circ 41' 18''$  (LT)  
 $D = 32^\circ 44' 26''$   
 $R = 175.00'$   
 $T = 68.39'$   
 $L = 130.38'$   
 $E = 12.89'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 101+18.93  
 P.T. STA = 102+49.32

PROP. CURVE PRBIKE-5  
 PI STA. = 115+28.05  
 $\Delta = 3^\circ 02' 40''$  (LT)  
 $D = 9^\circ 32' 57''$   
 $R = 600.00'$   
 $T = 15.94'$   
 $L = 31.88'$   
 $E = 0.21'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 115+12.11  
 P.T. STA = 115+43.99

PROP. CURVE PRBIKE-8  
 PI STA. = 120+12.11  
 $\Delta = 3^\circ 25' 46''$  (RT)  
 $D = 11^\circ 27' 33''$   
 $R = 500.00'$   
 $T = 14.97'$   
 $L = 29.93'$   
 $E = 0.22'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 119+97.14  
 P.T. STA = 120+27.07

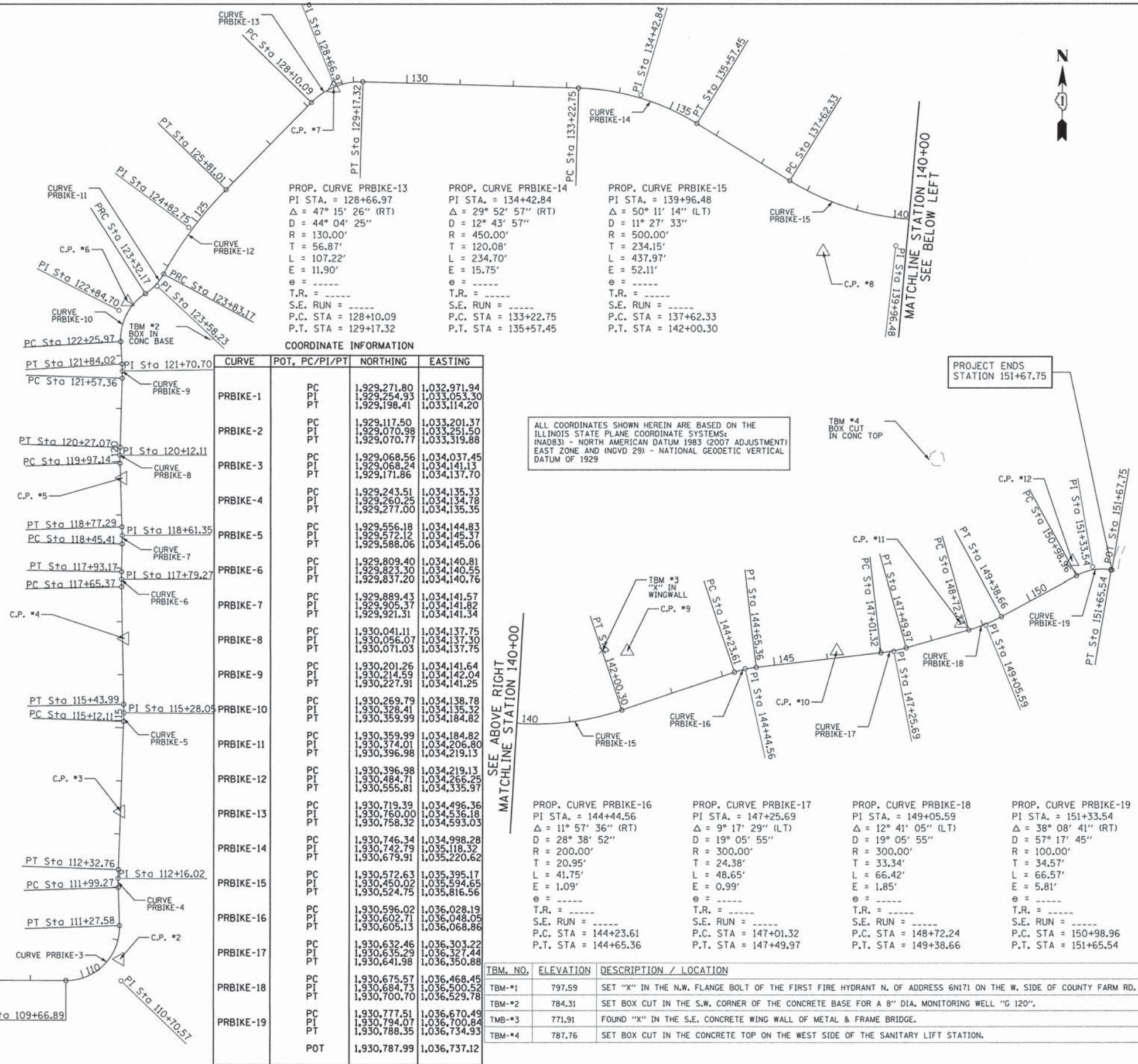
PROP. CURVE PRBIKE-11  
 PI STA. = 123+58.23  
 $\Delta = 29^\circ 13' 15''$  (LT)  
 $D = 57^\circ 17' 45''$   
 $R = 100.00'$   
 $T = 26.07'$   
 $L = 51.00'$   
 $E = 3.34'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 123+32.17  
 P.T. STA = 123+83.17

PROP. CURVE PRBIKE-3  
 PI STA. = 110+70.57  
 $\Delta = 92^\circ 04' 12''$  (LT)  
 $D = 57^\circ 17' 45''$   
 $R = 100.00'$   
 $T = 103.68'$   
 $L = 160.69'$   
 $E = 44.05'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 109+66.89  
 P.T. STA = 111+27.58

PROP. CURVE PRBIKE-6  
 PI STA. = 117+79.27  
 $\Delta = 1^\circ 59' 29''$  (RT)  
 $D = 7^\circ 09' 43''$   
 $R = 800.00'$   
 $T = 13.90'$   
 $L = 27.80'$   
 $E = 0.12'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 117+65.37  
 P.T. STA = 117+93.17

PROP. CURVE PRBIKE-9  
 PI STA. = 121+70.70  
 $\Delta = 5^\circ 05' 28''$  (LT)  
 $D = 19^\circ 05' 55''$   
 $R = 300.00'$   
 $T = 13.34'$   
 $L = 26.66'$   
 $E = 0.30'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 121+57.36  
 P.T. STA = 121+84.02

PROP. CURVE PRBIKE-12  
 PI STA. = 124+82.75  
 $\Delta = 16^\circ 11' 36''$  (RT)  
 $D = 8^\circ 11' 06''$   
 $R = 700.00'$   
 $T = 99.58'$   
 $L = 197.84'$   
 $E = 7.05'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 123+83.17  
 P.T. STA = 125+81.01



PROP. CURVE PRBIKE-13  
 PI STA. = 128+66.97  
 $\Delta = 47^\circ 15' 26''$  (RT)  
 $D = 44^\circ 04' 25''$   
 $R = 130.00'$   
 $T = 56.87'$   
 $L = 107.22'$   
 $E = 11.90'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 128+10.09  
 P.T. STA = 129+17.32

PROP. CURVE PRBIKE-14  
 PI STA. = 134+42.84  
 $\Delta = 29^\circ 52' 57''$  (RT)  
 $D = 12^\circ 43' 57''$   
 $R = 450.00'$   
 $T = 120.08'$   
 $L = 234.70'$   
 $E = 15.75'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 133+22.75  
 P.T. STA = 135+57.45

PROP. CURVE PRBIKE-15  
 PI STA. = 139+96.48  
 $\Delta = 50^\circ 11' 14''$  (LT)  
 $D = 11^\circ 27' 33''$   
 $R = 500.00'$   
 $T = 234.15'$   
 $L = 437.97'$   
 $E = 52.11'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 137+62.33  
 P.T. STA = 142+00.30

| CURVE     | POT. | PC/PI/PT     | NORTHING     | EASTING      |
|-----------|------|--------------|--------------|--------------|
| PRBIKE-1  | PC   | 1,929,271.80 | 1,032,971.94 | 1,033,053.30 |
| PRBIKE-2  | PC   | 1,929,117.50 | 1,033,201.37 | 1,033,251.50 |
| PRBIKE-3  | PC   | 1,929,068.56 | 1,034,037.45 | 1,034,141.13 |
| PRBIKE-4  | PC   | 1,929,243.51 | 1,034,135.33 | 1,034,135.35 |
| PRBIKE-5  | PC   | 1,929,556.18 | 1,034,144.83 | 1,034,145.37 |
| PRBIKE-6  | PC   | 1,929,809.40 | 1,034,140.81 | 1,034,140.55 |
| PRBIKE-7  | PC   | 1,929,889.43 | 1,034,141.57 | 1,034,141.82 |
| PRBIKE-8  | PC   | 1,930,041.11 | 1,034,137.75 | 1,034,137.75 |
| PRBIKE-9  | PC   | 1,930,201.26 | 1,034,141.64 | 1,034,142.04 |
| PRBIKE-10 | PC   | 1,930,269.79 | 1,034,138.78 | 1,034,135.32 |
| PRBIKE-11 | PC   | 1,930,359.99 | 1,034,184.82 | 1,034,206.80 |
| PRBIKE-12 | PC   | 1,930,396.98 | 1,034,219.13 | 1,034,335.97 |
| PRBIKE-13 | PC   | 1,930,719.39 | 1,034,496.36 | 1,034,593.03 |
| PRBIKE-14 | PC   | 1,930,746.34 | 1,034,998.28 | 1,035,118.32 |
| PRBIKE-15 | PC   | 1,930,572.63 | 1,035,395.17 | 1,035,594.65 |
| PRBIKE-16 | PC   | 1,930,596.02 | 1,036,028.19 | 1,036,048.05 |
| PRBIKE-17 | PC   | 1,930,632.46 | 1,036,303.22 | 1,036,327.44 |
| PRBIKE-18 | PC   | 1,930,675.57 | 1,036,468.45 | 1,036,500.52 |
| PRBIKE-19 | PC   | 1,930,777.51 | 1,036,670.49 | 1,036,700.84 |
| POT       |      | 1,930,787.99 | 1,036,737.12 |              |

ALL COORDINATES SHOWN HEREIN ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEMS (NAD83) - NORTH AMERICAN DATUM 1983 (2007 ADJUSTMENT) EAST ZONE AND (NGVD 29) - NATIONAL GEODETIC VERTICAL DATUM OF 1929

SEE ABOVE RIGHT MATCHLINE STATION 140+00

PROP. CURVE PRBIKE-16  
 PI STA. = 144+44.56  
 $\Delta = 11^\circ 57' 36''$  (RT)  
 $D = 28^\circ 38' 52''$   
 $R = 200.00'$   
 $T = 20.95'$   
 $L = 41.75'$   
 $E = 1.09'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 144+23.61  
 P.T. STA = 144+65.36

PROP. CURVE PRBIKE-17  
 PI STA. = 147+25.69  
 $\Delta = 9^\circ 17' 29''$  (LT)  
 $D = 19^\circ 05' 55''$   
 $R = 300.00'$   
 $T = 24.38'$   
 $L = 48.65'$   
 $E = 0.99'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 147+01.32  
 P.T. STA = 147+49.97

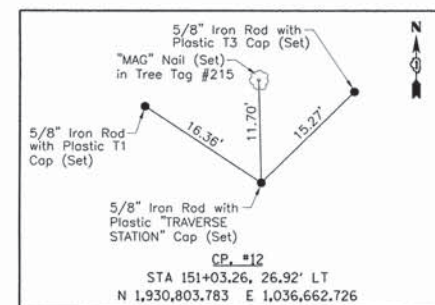
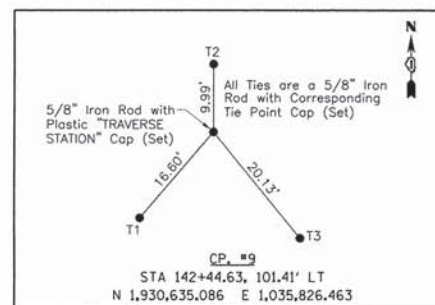
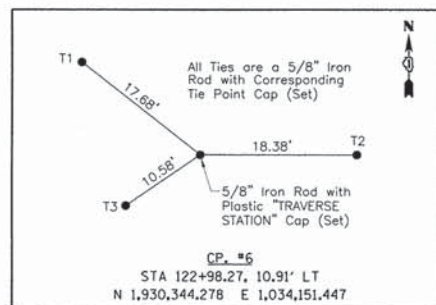
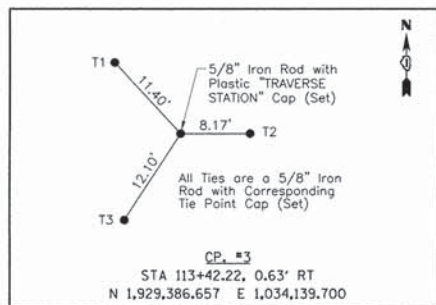
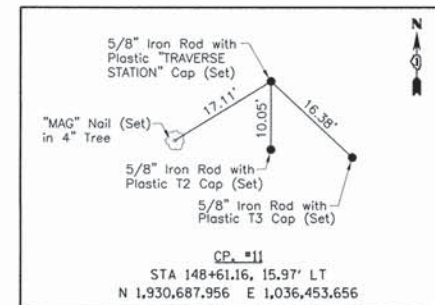
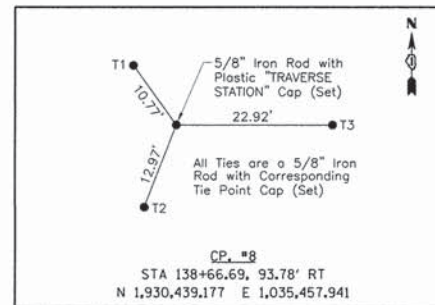
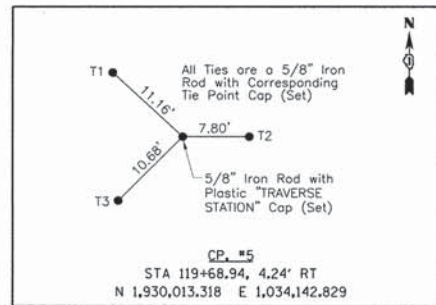
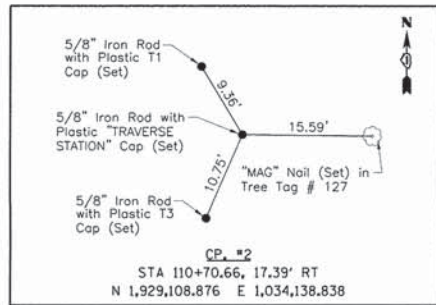
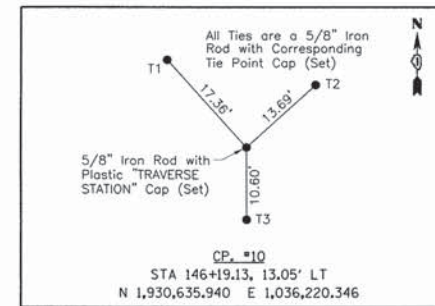
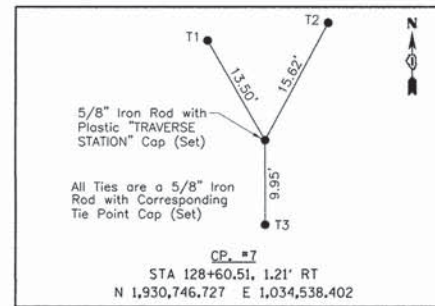
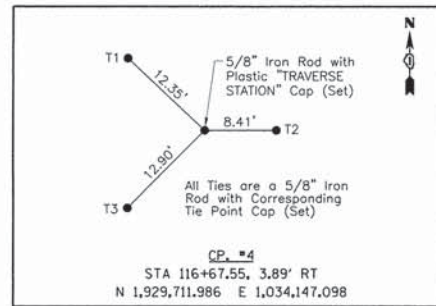
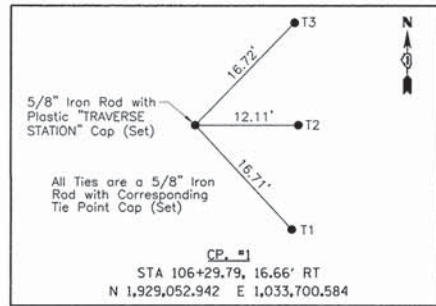
PROP. CURVE PRBIKE-18  
 PI STA. = 149+05.59  
 $\Delta = 12^\circ 41' 05''$  (LT)  
 $D = 19^\circ 05' 55''$   
 $R = 300.00'$   
 $T = 33.34'$   
 $L = 66.42'$   
 $E = 1.85'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 148+72.24  
 P.T. STA = 149+38.66

PROP. CURVE PRBIKE-19  
 PI STA. = 151+33.54  
 $\Delta = 38^\circ 08' 41''$  (RT)  
 $D = 57^\circ 17' 45''$   
 $R = 100.00'$   
 $T = 34.57'$   
 $L = 66.57'$   
 $E = 5.81'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 P.C. STA = 150+98.96  
 P.T. STA = 151+65.54

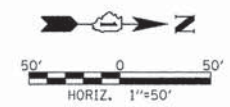
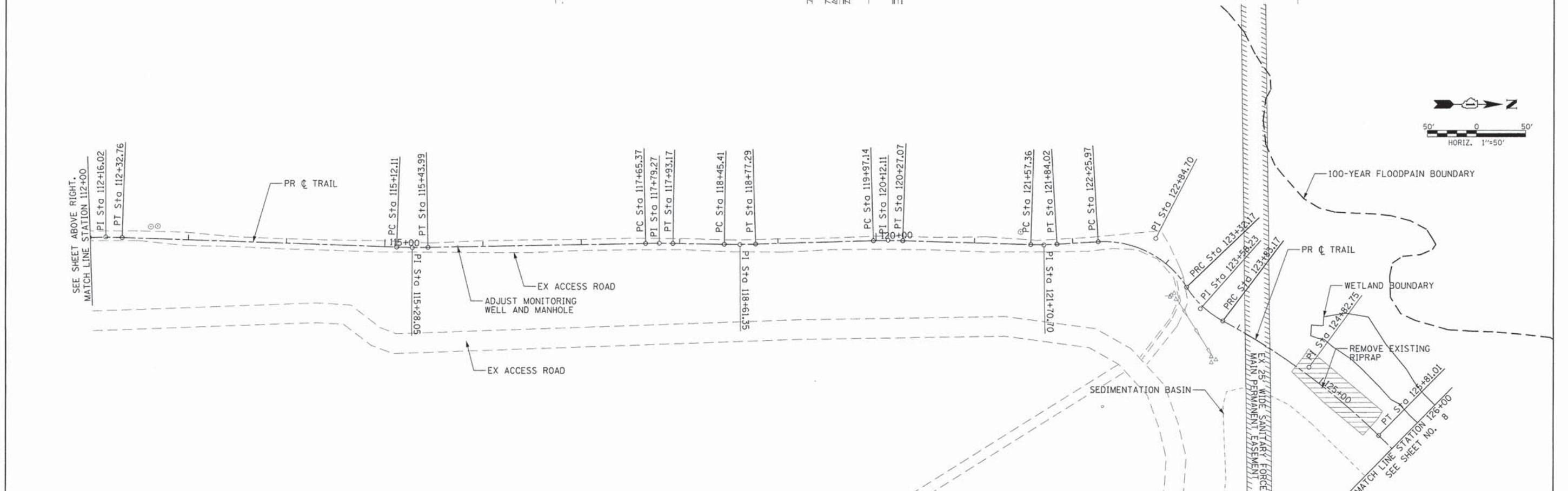
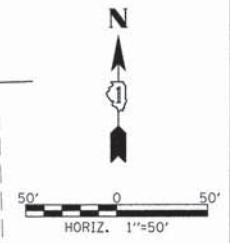
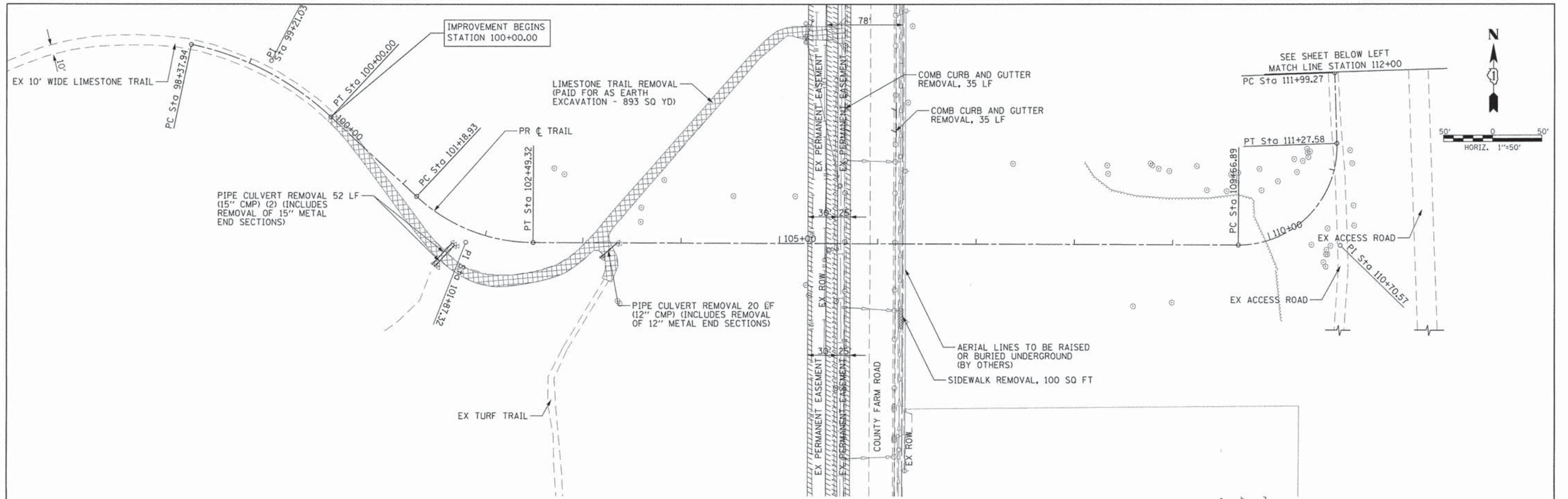
| TBM NO. | ELEVATION | DESCRIPTION / LOCATION  |
|---------|-----------|---|
| TBM-#1  | 797.59    | SET "X" IN THE N.W. FLANGE BOLT OF THE FIRST FIRE HYDRANT N. OF ADDRESS 6N17I ON THE W. SIDE OF COUNTY FARM RD. |
| TBM-#2  | 784.31    | SET BOX CUT IN THE S.W. CORNER OF THE CONCRETE BASE FOR A 8" DIA. MONITORING WELL "G 120".                      |
| TBM-#3  | 771.91    | FOUND "X" IN THE S.E. CONCRETE WING WALL OF METAL & FRAME BRIDGE.   |
| TBM-#4  | 787.76    | SET BOX CUT IN THE CONCRETE TOP ON THE WEST SIDE OF THE SANITARY LIFT STATION.                                  |



| C.P. NO. | NORTHING      | EASTING       | ELEVATION | DESCRIPTION  |
|----------|---------------|---------------|-----------|--|
| C.P. #1  | 1,929,052.942 | 1,033,700.584 | 797.89    | SET 5/8" IRON ROD WITH PLASTIC "TRAVERSE STATION" CAP E. SIDE OF COUNTY FARM ROAD. |
| C.P. #2  | 1,929,108.876 | 1,034,138.838 | 812.33    | SET 5/8" IRON ROD WITH PLASTIC "TRAVERSE STATION" CAP.                             |
| C.P. #3  | 1,929,386.657 | 1,034,139.700 | 810.93    | SET 5/8" IRON ROD WITH PLASTIC "TRAVERSE STATION" CAP.                             |
| C.P. #4  | 1,929,711.986 | 1,034,147.098 | 796.94    | SET 5/8" IRON ROD WITH PLASTIC "TRAVERSE STATION" CAP.                             |
| C.P. #5  | 1,930,013.318 | 1,034,142.829 | 796.58    | SET 5/8" IRON ROD WITH PLASTIC "TRAVERSE STATION" CAP.                             |
| C.P. #6  | 1,930,344.278 | 1,034,151.447 | 778.55    | SET 5/8" IRON ROD WITH PLASTIC "TRAVERSE STATION" CAP.                             |
| C.P. #7  | 1,930,746.727 | 1,034,538.402 | 779.94    | SET 5/8" IRON ROD WITH PLASTIC "TRAVERSE STATION" CAP.                             |
| C.P. #8  | 1,930,439.177 | 1,035,457.941 | 782.54    | SET 5/8" IRON ROD WITH PLASTIC "TRAVERSE STATION" CAP.                             |
| C.P. #9  | 1,930,635.086 | 1,035,826.463 | 768.06    | SET 5/8" IRON ROD WITH PLASTIC "TRAVERSE STATION" CAP.                             |
| C.P. #10 | 1,930,635.940 | 1,036,220.346 | 774.24    | SET 5/8" IRON ROD WITH PLASTIC "TRAVERSE STATION" CAP.                             |
| C.P. #11 | 1,930,687.956 | 1,036,453.656 | 788.29    | SET 5/8" IRON ROD WITH PLASTIC "TRAVERSE STATION" CAP.                             |
| C.P. #12 | 1,930,803.783 | 1,036,662.726 | 791.77    | SET 5/8" IRON ROD WITH PLASTIC "TRAVERSE STATION" CAP.                             |

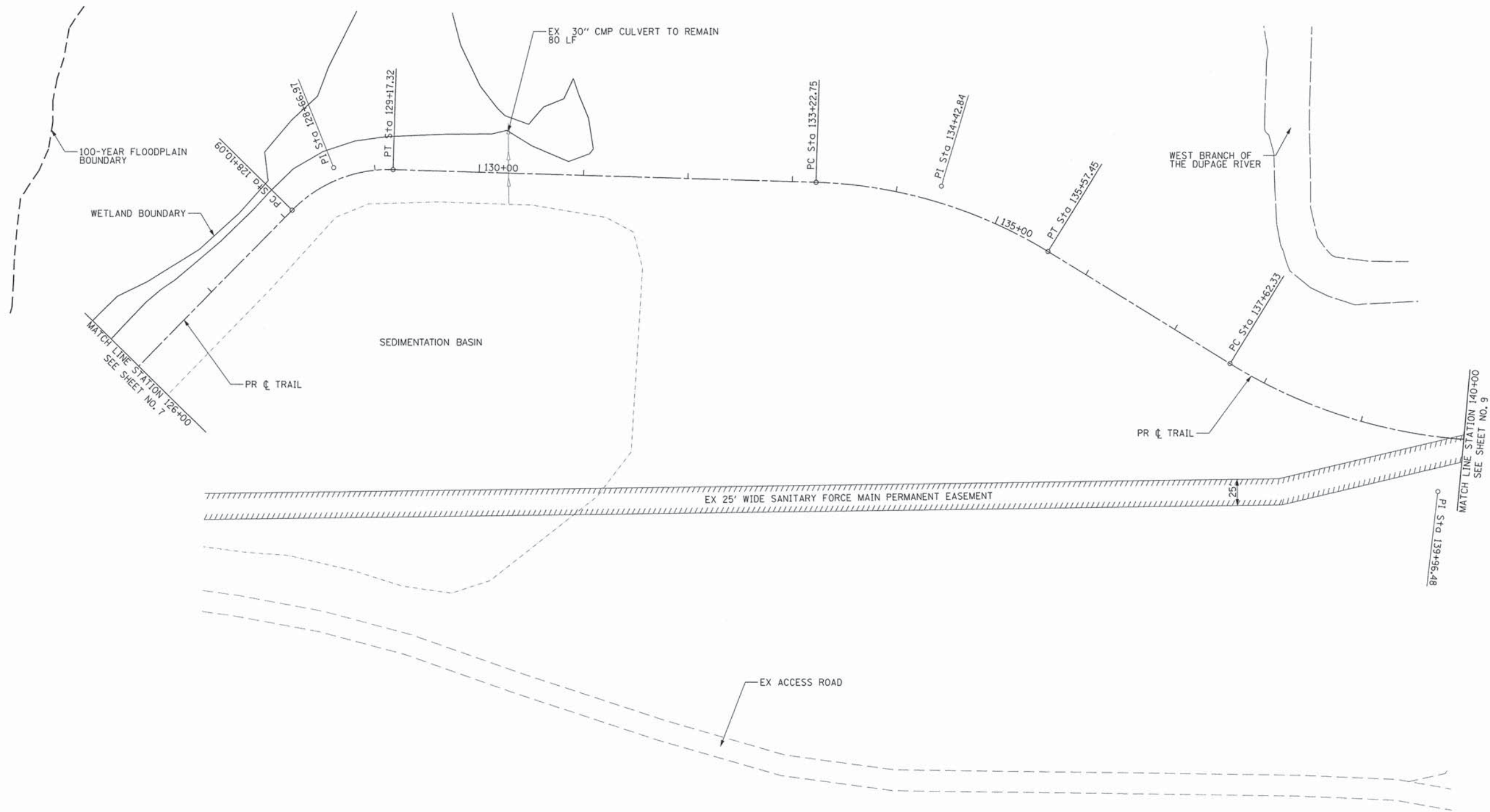






|  |                        |                  |           |   |  |   |                         |                            |              |           |  |
|--|------------------------|------------------|-----------|---|--|---|-------------------------|----------------------------|--------------|-----------|--|
| FILE NAME =                                | USER NAME = bshae1zger | DESIGNED - MLB   | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS<br/>REMOVAL PLAN</b> | F.A.U. RTE.                                     | SECTION                 | COUNTY                     | TOTAL SHEETS | SHEET NO. |  |
| DR\CH13\0855\Road\Sheets\30855-SHT-REM.dgn |                        | DRAWN - MLB      | REVISED - |   |  | 362   | 14-F3000-12-BT          | DUPAGE                     | 69           | 7         |  |
| PLOT SCALE =                               |                        | CHECKED - DWB    | REVISED - |   |  | CONTRACT NO 61C31                               |                         |                            |              |           |  |
| PLOT DATE = 12/16/2015                     |                        | DATE - 12/7/2015 | REVISED - |   |  | FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                         |                            |              |           |  |
|  |                        |                  |           |   |  | SCALE: 1"=50'                                   | SHEET NO. 1 OF 3 SHEETS | STA. 100+00 TO STA. 126+00 |              |           |  |





|  |                          |                  |           |
|--|--------------------------|------------------|-----------|
| FILE NAME *                                  | USER NAME = bshaeffinger | DESIGNED - MLB   | REVISED - |
| G:\CH13\0055\Road\Sheets\30055-SHT-REM\2.dgn |                          | DRAWN - MLB      | REVISED - |
| PLOT SCALE =                                 |                          | CHECKED - DWB    | REVISED - |
| PLOT DATE = 12/16/2015                       |                          | DATE - 12/7/2015 | REVISED - |

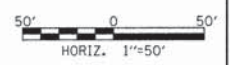
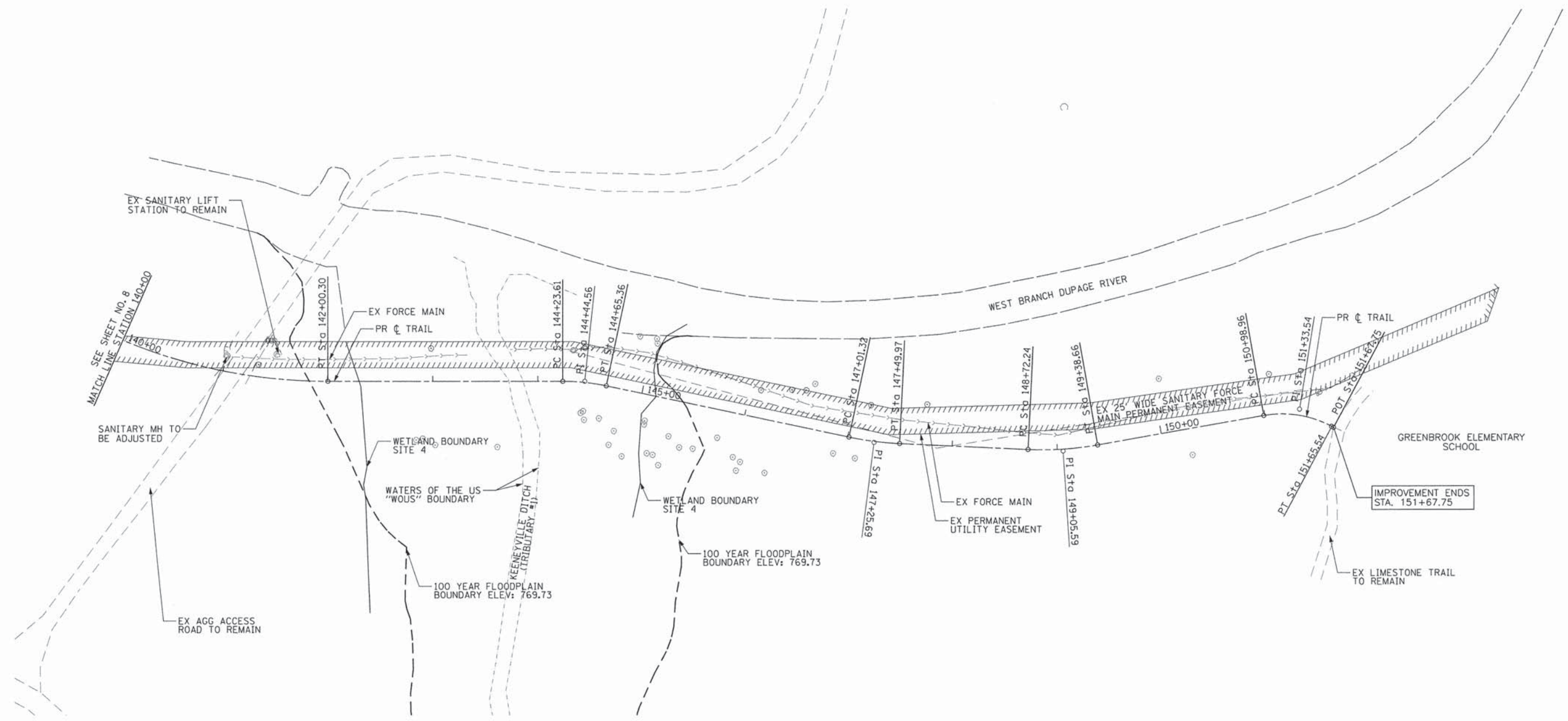
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS  
REMOVAL PLAN**

SCALE: 1"=50'    SHEET NO. 2 OF 3 SHEETS    STA. 126+00 TO STA. 140+00

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A.U. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 362   | 14-F3000-12-BT | DUPAGE | 69           | 8         |
| CONTRACT NO 61C31                               |                |        |              |           |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                |        |              |           |





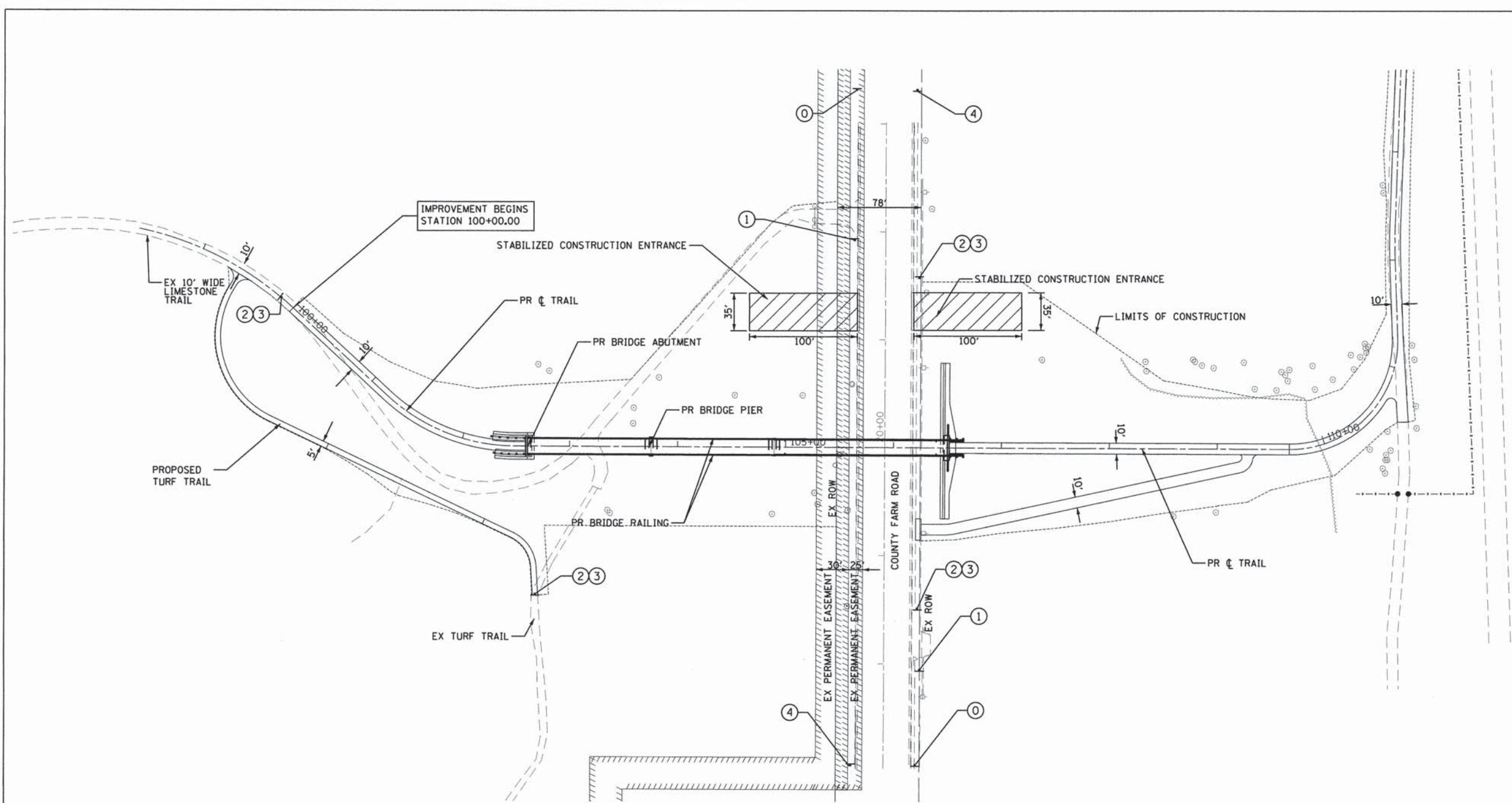
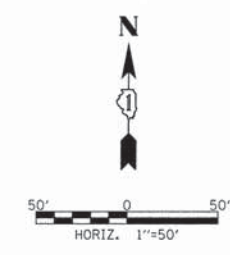
|  |                            |                   |           |
|--|----------------------------|-------------------|-----------|
| FILE NAME =<br>G:\CH13\0855\Road\Sheets\30855-SHT-REM3.dgn | USER NAME =<br>bshaeffiger | DESIGNED -<br>MLB | REVISED - |
| PLOT SCALE =   | DATE =<br>12/7/2015        | DRAWN -<br>MLB    | REVISED - |
| PLOT DATE =<br>12/16/2015                                  | CHECKED -<br>DWB           | REVISOR -         | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|  |                         |                               |  |
|--|-------------------------|-------------------------------|--|
| <b>COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS<br/>REMOVAL PLAN</b> |                         |                               |  |
| SCALE: 1"=50'  | SHEET NO. 3 OF 3 SHEETS | STA. 140+00 TO STA. 151+67.49 |  |

|   |                           |                  |                    |                |
|---|---------------------------|------------------|--------------------|----------------|
| F.A.U. RTE.<br>362                              | SECTION<br>14-F3000-12-BT | COUNTY<br>DuPAGE | TOTAL SHEETS<br>69 | SHEET NO.<br>9 |
| CONTRACT NO 61C31                               |                           |                  |                    |                |
| FED. ROAD DIST. NO.   ILLINOIS FED. AID PROJECT |                           |                  |                    |                |

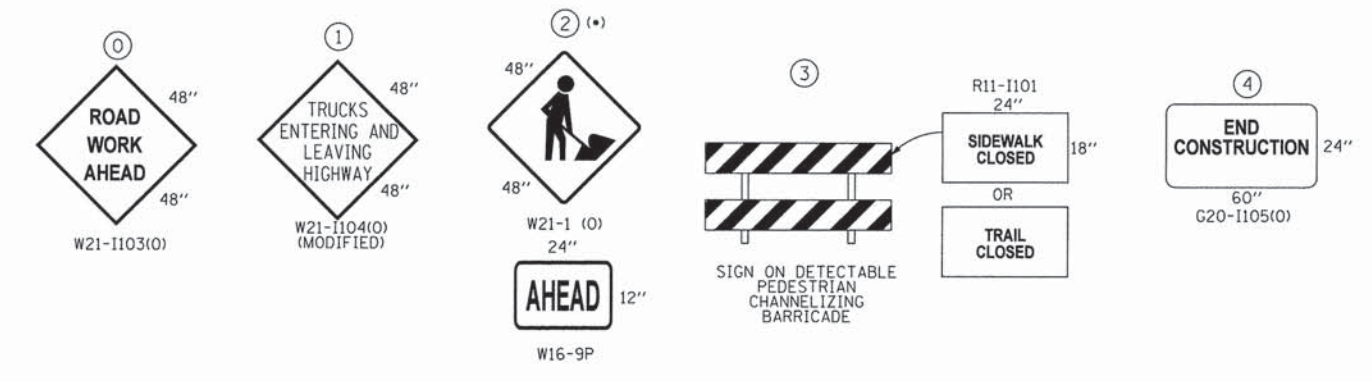




- CONSTRUCTION STAGING:**
1. CONSTRUCT PROPOSED TURF TRAIL TO CONNECT EXISTING LIMESTONE TRAIL TO EXISTING TURF TRAIL PRIOR TO REMOVING EXISTING LIMESTONE TRAIL EAST OF STATION 100+00.
  2. CONSTRUCT OVERPASS APPROACHES ON WEST AND EAST SIDE OF COUNTY FARM RD.
  3. INSTALL PREFABRICATED BRIDGE SECTIONS. THESE SHALL BE INSTALLED AT NIGHT BETWEEN THE HOURS OF 9 PM AND 5 AM. OPEN NEW OVERPASS.
  4. LANDSCAPE RESTORATION WORK.

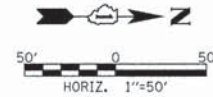
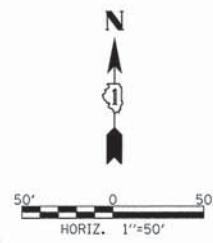
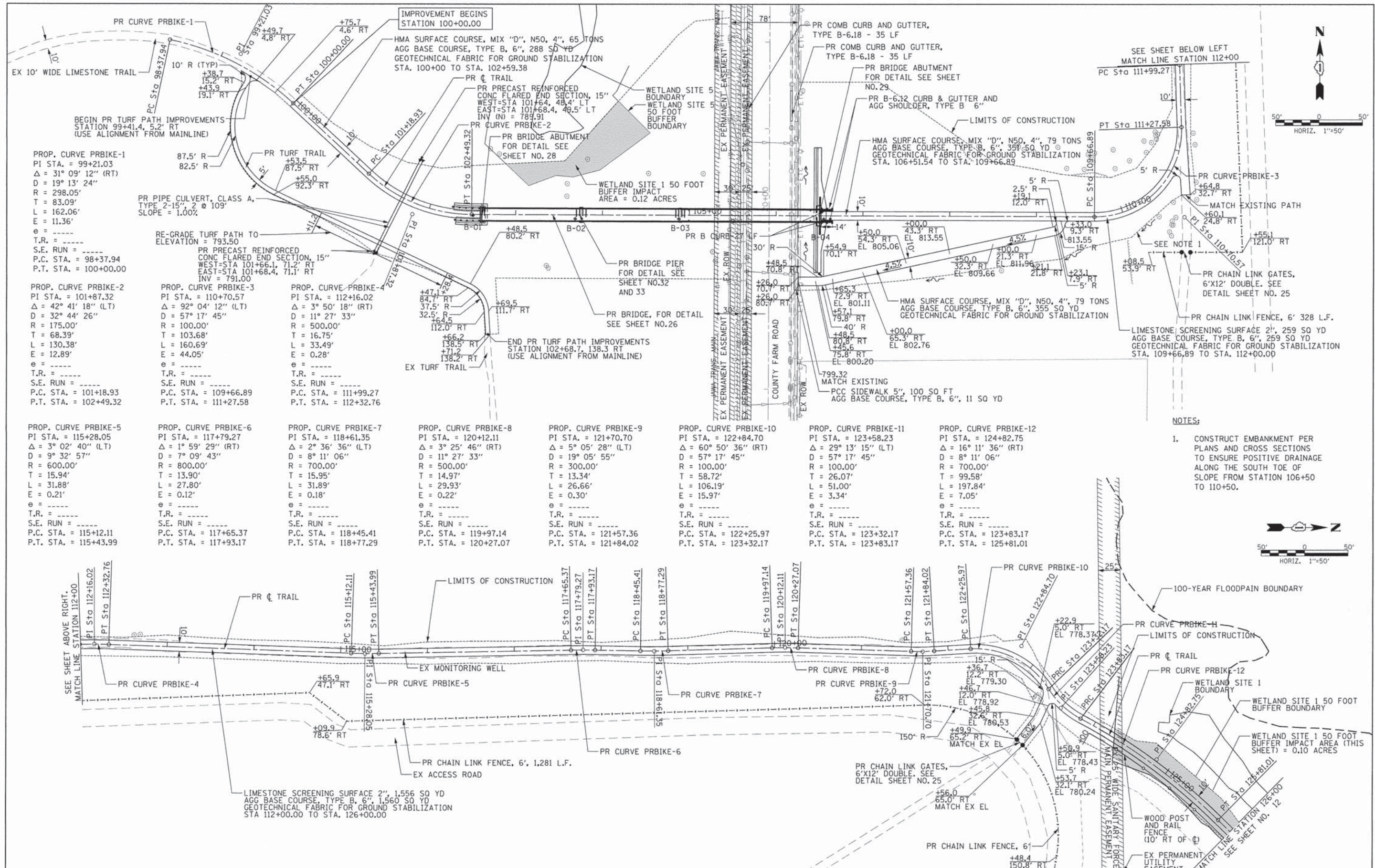
**NOTES:**  
ALL TEMPORARY SIGNING SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION, (SPECIAL)

(\*) SIGN NEEDS TO BE REMOVED WHEN WORKERS NOT PRESENT FOR MORE THAN ONE HOUR.



|  |                      |                  |           |   |   |  |                |        |              |           |  |
|--|----------------------|------------------|-----------|---|---|--|----------------|--------|--------------|-----------|--|
| FILE NAME =                                | USER NAME = bshae11g | DESIGNED - MLB   | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS<br/>CONSTRUCTION STAGING PLAN AND NOTES</b> | F.A.U. RTE.  | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |  |
| G:\CHI3\0855\Road\Sheets\30055-SHT-MOT.dgn |                      | DRAWN - MLB      | REVISED - |   |   | 362  | 14-F3000-12-BT | DUPAGE | 69           | 10        |  |
| PLOT SCALE =                               |                      | CHECKED - DWB    | REVISED - |   |   | CONTRACT NO 61C31  |                |        |              |           |  |
| PLOT DATE = 12/16/2015                     |                      | DATE - 12/7/2015 | REVISED - |   |   | ILLINOIS FED. AID PROJECT  |                |        |              |           |  |
|  |                      |                  |           |   |   | SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. 100+00 TO STA. 126+00 |                |        |              |           |  |





PROP. CURVE PRBIKE-1  
 PI STA. = 99+21.03  
 $\Delta = 31^\circ 09' 12''$  (RT)  
 $D = 19^\circ 13' 24''$   
 $R = 298.05'$   
 $T = 83.09'$   
 $L = 162.06'$   
 $E = 11.36'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 98+37.94$   
 $P.T. \text{ STA.} = 100+00.00$

PROP. CURVE PRBIKE-2  
 PI STA. = 101+87.32  
 $\Delta = 42^\circ 41' 18''$  (LT)  
 $D = 32^\circ 44' 26''$   
 $R = 175.00'$   
 $T = 68.39'$   
 $L = 130.38'$   
 $E = 12.89'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 101+18.93$   
 $P.T. \text{ STA.} = 102+49.32$

PROP. CURVE PRBIKE-3  
 PI STA. = 110+70.57  
 $\Delta = 92^\circ 04' 12''$  (LT)  
 $D = 57^\circ 17' 45''$   
 $R = 100.00'$   
 $T = 103.68'$   
 $L = 160.69'$   
 $E = 44.05'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 109+66.89$   
 $P.T. \text{ STA.} = 111+27.58$

PROP. CURVE PRBIKE-4  
 PI STA. = 112+16.02  
 $\Delta = 3^\circ 50' 18''$  (RT)  
 $D = 11^\circ 27' 33''$   
 $R = 500.00'$   
 $T = 16.75'$   
 $L = 33.49'$   
 $E = 0.28'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 111+99.27$   
 $P.T. \text{ STA.} = 112+32.76$

PROP. CURVE PRBIKE-5  
 PI STA. = 115+28.05  
 $\Delta = 3^\circ 02' 40''$  (LT)  
 $D = 9^\circ 32' 57''$   
 $R = 600.00'$   
 $T = 15.94'$   
 $L = 31.88'$   
 $E = 0.21'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 115+12.11$   
 $P.T. \text{ STA.} = 115+43.99$

PROP. CURVE PRBIKE-6  
 PI STA. = 117+79.27  
 $\Delta = 1^\circ 59' 29''$  (RT)  
 $D = 7^\circ 09' 43''$   
 $R = 800.00'$   
 $T = 13.90'$   
 $L = 27.80'$   
 $E = 0.12'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 117+65.37$   
 $P.T. \text{ STA.} = 117+93.17$

PROP. CURVE PRBIKE-7  
 PI STA. = 118+61.35  
 $\Delta = 2^\circ 36' 36''$  (LT)  
 $D = 8^\circ 11' 06''$   
 $R = 700.00'$   
 $T = 15.95'$   
 $L = 31.89'$   
 $E = 0.12'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 118+45.41$   
 $P.T. \text{ STA.} = 118+77.29$

PROP. CURVE PRBIKE-8  
 PI STA. = 120+12.11  
 $\Delta = 3^\circ 25' 46''$  (RT)  
 $D = 11^\circ 27' 33''$   
 $R = 500.00'$   
 $T = 14.97'$   
 $L = 29.93'$   
 $E = 0.22'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 119+97.14$   
 $P.T. \text{ STA.} = 120+27.07$

PROP. CURVE PRBIKE-9  
 PI STA. = 121+70.70  
 $\Delta = 5^\circ 05' 28''$  (LT)  
 $D = 19^\circ 05' 55''$   
 $R = 300.00'$   
 $T = 13.34'$   
 $L = 26.66'$   
 $E = 0.30'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 121+57.36$   
 $P.T. \text{ STA.} = 121+84.02$

PROP. CURVE PRBIKE-10  
 PI STA. = 122+84.70  
 $\Delta = 60^\circ 50' 36''$  (RT)  
 $D = 57^\circ 17' 45''$   
 $R = 100.00'$   
 $T = 58.72'$   
 $L = 106.19'$   
 $E = 15.97'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 122+25.97$   
 $P.T. \text{ STA.} = 123+32.17$

PROP. CURVE PRBIKE-11  
 PI STA. = 123+58.23  
 $\Delta = 29^\circ 13' 15''$  (LT)  
 $D = 57^\circ 17' 45''$   
 $R = 100.00'$   
 $T = 26.07'$   
 $L = 51.00'$   
 $E = 3.34'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 123+32.17$   
 $P.T. \text{ STA.} = 123+83.17$

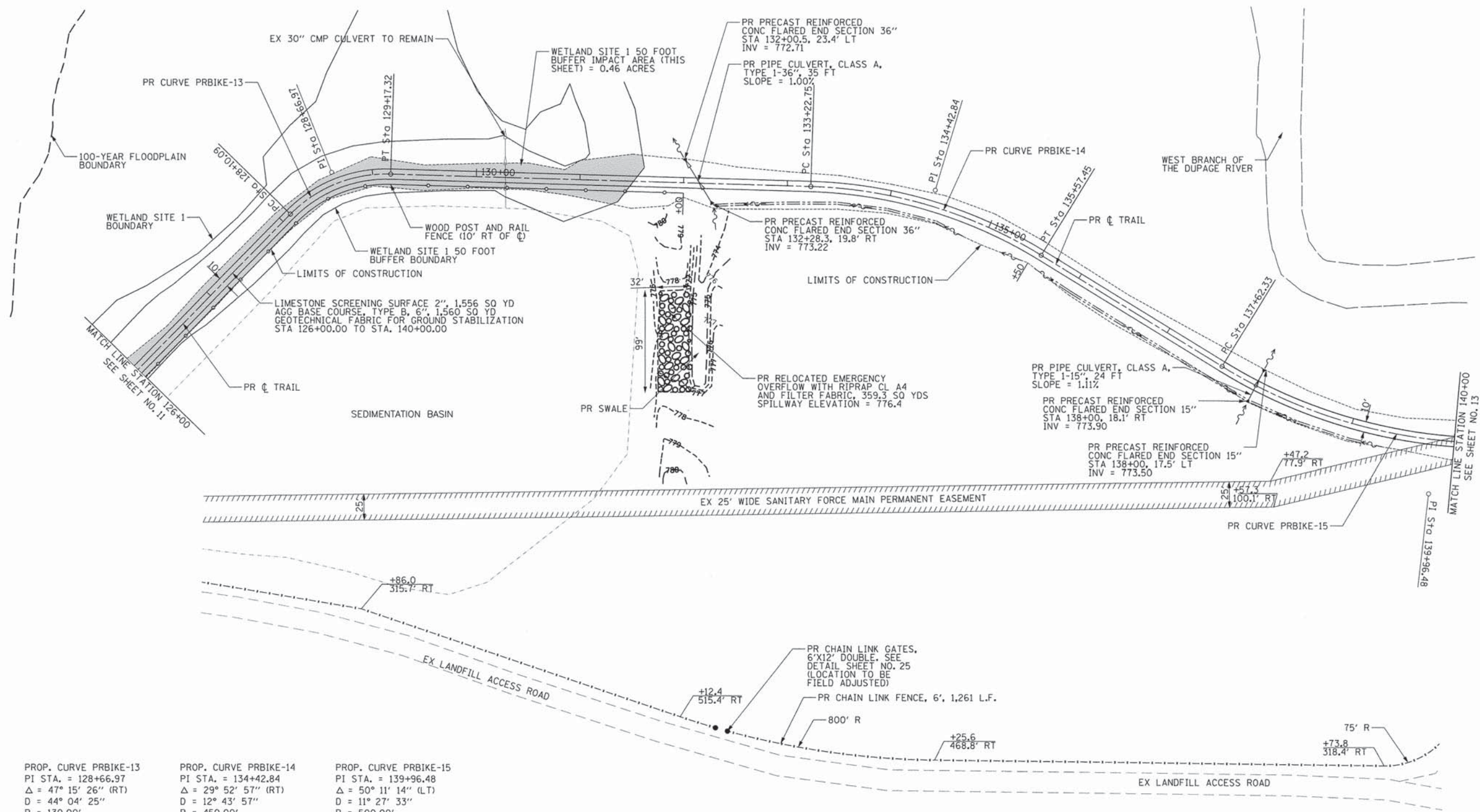
PROP. CURVE PRBIKE-12  
 PI STA. = 124+82.75  
 $\Delta = 16^\circ 11' 36''$  (RT)  
 $D = 8^\circ 11' 06''$   
 $R = 700.00'$   
 $T = 99.58'$   
 $L = 197.84'$   
 $E = 7.05'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 123+83.17$   
 $P.T. \text{ STA.} = 125+81.01$

**NOTES:**

1. CONSTRUCT EMBANKMENT PER PLANS AND CROSS SECTIONS TO ENSURE POSITIVE DRAINAGE ALONG THE SOUTH TOE OF SLOPE FROM STATION 106+50 TO 110+50.

|  |                        |                  |           |   |  |                |                   |        |                 |
|--|------------------------|------------------|-----------|---|--|----------------|-------------------|--------|-----------------|
| FILE NAME = G:\CH3\0895\Road\Sheets\38855-SHT-PLAN.dgn | USER NAME = bshae1iger | DESIGNED - MLB   | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS<br/>TRAIL AND UTILITY PLAN</b> |                | SECTION           | COUNTY | TOTAL SHEET NO. |
| PLOT SCALE =   | PLOT DATE = 12/16/2015 | DRAWN - MLB      | REVISED - |   | 362  | 14-F3000-12-BT | DUPAGE            | 69     | 11              |
|  |                        | CHECKED - DWB    | REVISED - |   | SCALE: 1"=50' SHEET NO. 01 OF 3 SHEETS STA. 100+00 TO STA. 126+00                |                | CONTRACT NO 61C31 |        |                 |
|  |                        | DATE - 12/7/2015 | REVISED - |   | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT                                    |                |                   |        |                 |





| PROP. CURVE PRBIKE-13             | PROP. CURVE PRBIKE-14             | PROP. CURVE PRBIKE-15             |
|-----------------------------------|-----------------------------------|-----------------------------------|
| PI STA. = 128+66.97               | PI STA. = 134+42.84               | PI STA. = 139+96.48               |
| $\Delta = 47^\circ 15' 26''$ (RT) | $\Delta = 29^\circ 52' 57''$ (RT) | $\Delta = 50^\circ 11' 14''$ (LT) |
| D = 44° 04' 25"                   | D = 12° 43' 57"                   | D = 11° 27' 33"                   |
| R = 130.00'                       | R = 450.00'                       | R = 500.00'                       |
| T = 56.87'                        | T = 120.08'                       | T = 234.15'                       |
| L = 107.22'                       | L = 234.70'                       | L = 437.97'                       |
| E = 11.90'                        | E = 15.75'                        | E = 52.11'                        |
| e = -----                         | e = -----                         | e = -----                         |
| T.R. = -----                      | T.R. = -----                      | T.R. = -----                      |
| S.E. RUN = -----                  | S.E. RUN = -----                  | S.E. RUN = -----                  |
| P.C. STA. = 128+10.09             | P.C. STA. = 133+22.75             | P.C. STA. = 137+62.33             |
| P.T. STA. = 129+17.32             | P.T. STA. = 135+57.45             | P.T. STA. = 142+00.30             |

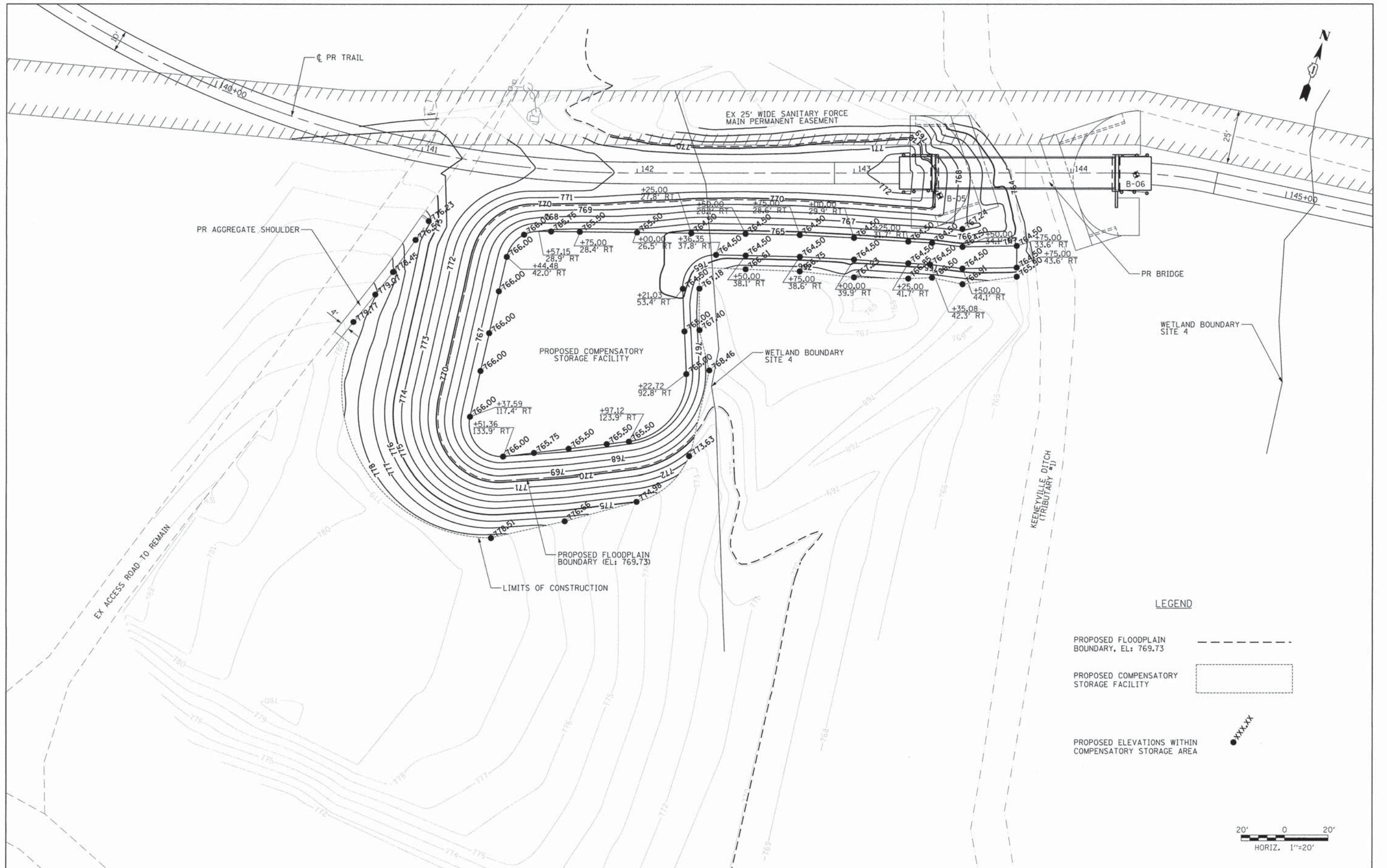


|  |                         |                  |           |   |  |   |                          |                            |                   |                |  |  |
|--|-------------------------|------------------|-----------|---|--|---|--------------------------|----------------------------|-------------------|----------------|--|--|
| FILE NAME = G:\CH13\0855\Road\Sheets\30855-SHT-PLA02.dgn | USER NAME = bshoefliger | DESIGNED - MLB   | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS<br/>TRAIL AND UTILITY PLAN</b> | F.A.U. RTE. = 362                               | SECTION = 14-F3000-12-BT | COUNTY = DUPAGE            | TOTAL SHEETS = 69 | SHEET NO. = 12 |  |  |
| PLOT SCALE =   | PLOT DATE = 12/16/2015  | DRAWN - MLB      | REVISED - |   |  | SCALE: 1"=50'                                   | SHEET NO. 2 OF 3 SHEETS  | STA. 126+00 TO STA. 140+00 | CONTRACT NO 61C31 |                |  |  |
|  |                         | CHECKED - DWB    | REVISED - |   |  | FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                          |                            |                   |                |  |  |
|  |                         | DATE - 12/7/2015 | REVISED - |   |  |   |                          |                            |                   |                |  |  |
|  |                         |                  |           |   |  |   |                          |                            |                   |                |  |  |









**LEGEND**

- PROPOSED FLOODPLAIN BOUNDARY, EL: 769.73
- PROPOSED COMPENSATORY STORAGE FACILITY
- PROPOSED ELEVATIONS WITHIN COMPENSATORY STORAGE AREA



|   |                        |
|---|------------------------|
| FILE NAME =                             | USER NAME = bshae1iger |
| G:\CHI\3\0895\Road\Sheets\30895-SHT-COM | STORAGE.dgn            |
| PLOT SCALE =                            |                        |
| PLOT DATE = 12/16/2015                  |                        |

|                  |           |
|------------------|-----------|
| DESIGNED - MLB   | REVISED - |
| DRAWN - MLB      | REVISED - |
| CHECKED - DWB    | REVISED - |
| DATE - 12/7/2015 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

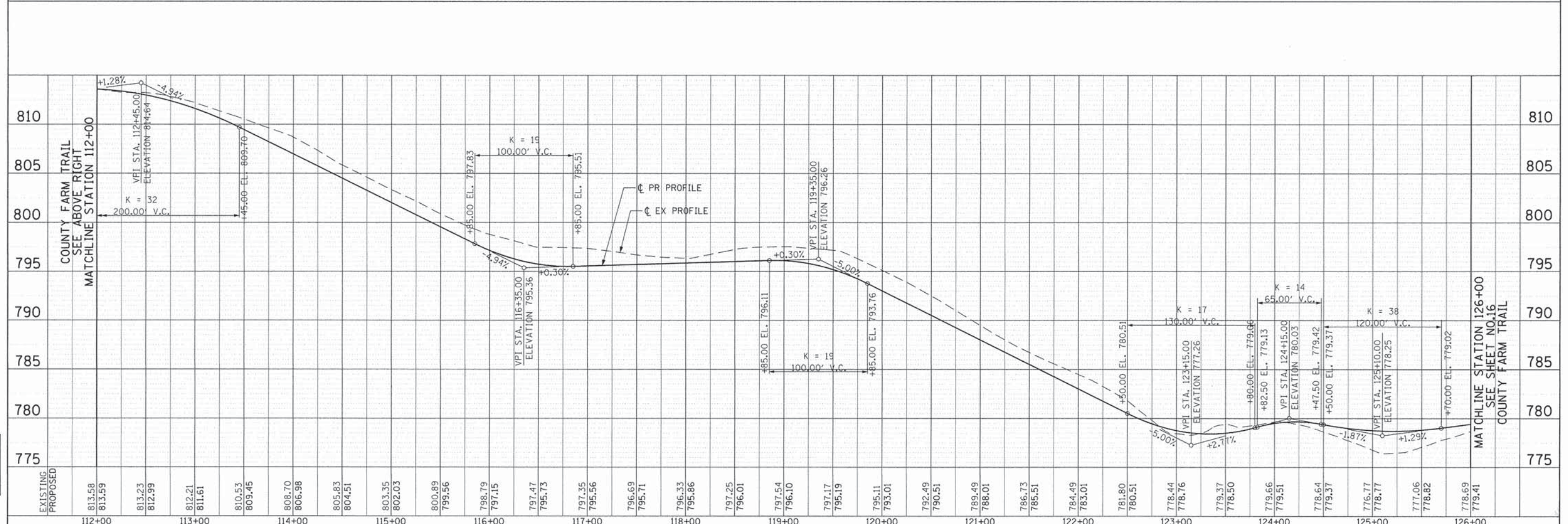
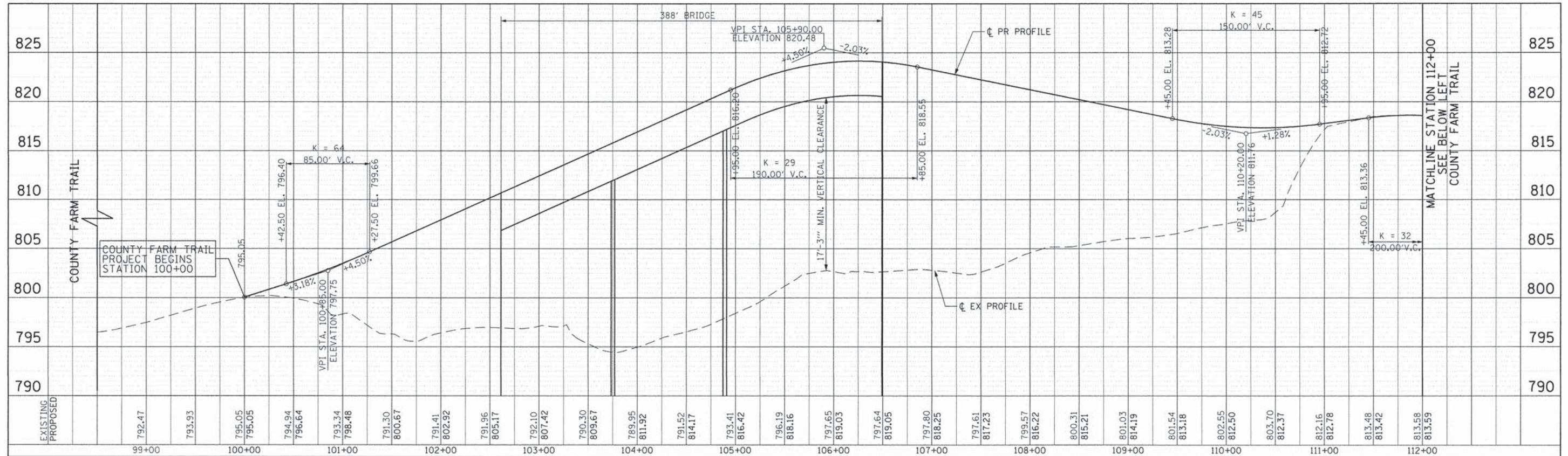
**COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS  
COMPENSATORY STORAGE FACILITY**

|  |                |        |              |           |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE.  | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 362  | 14-F3000-12-BT | DUPAGE | 69           | 14        |
| CONTRACT NO 61C31  |                |        |              |           |
| SCALE: 1"=20' SHEET NO. 14 OF 69 SHEETS STA. 140+00 TO STA. 146+00 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |



|      |          |    |      |
|------|----------|----|------|
| PLAN | DESIGNED | BY | DATE |
|      | PLOTTED  |    |      |
|      | CHECKED  |    |      |
|      | ALIGNED  |    |      |
|      | FILED    |    |      |
|      | NO.      |    |      |

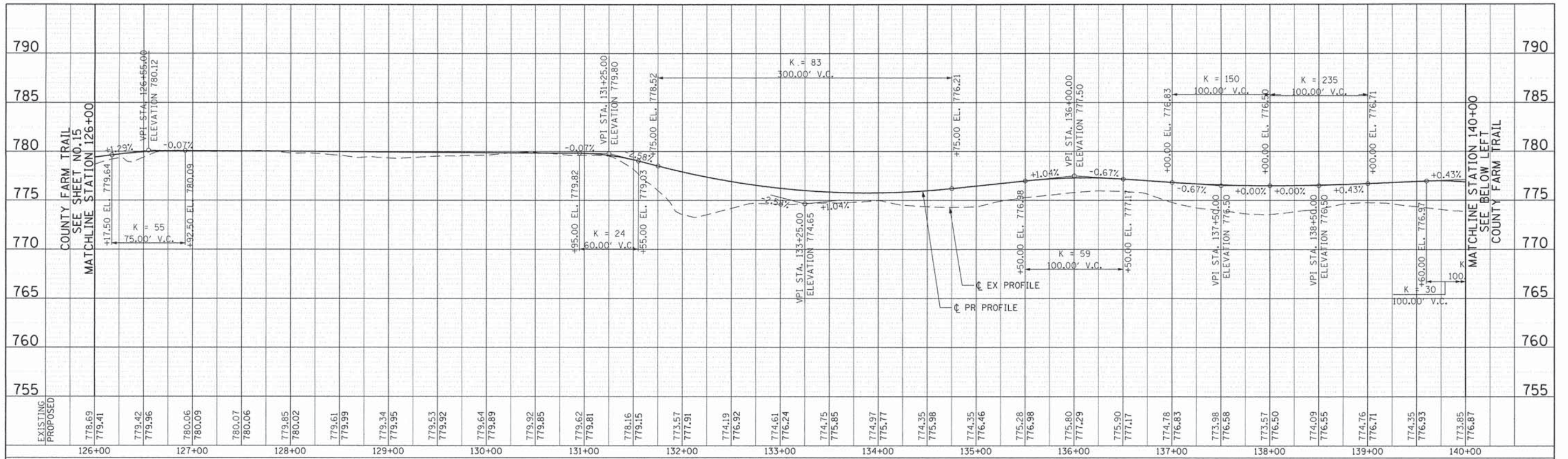
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|---------|----------|----|------|
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|         | PLOTTED  |    |      |
|         | CHECKED  |    |      |
|         | ALIGNED  |    |      |
|         | FILED    |    |      |
|         | NO.      |    |      |



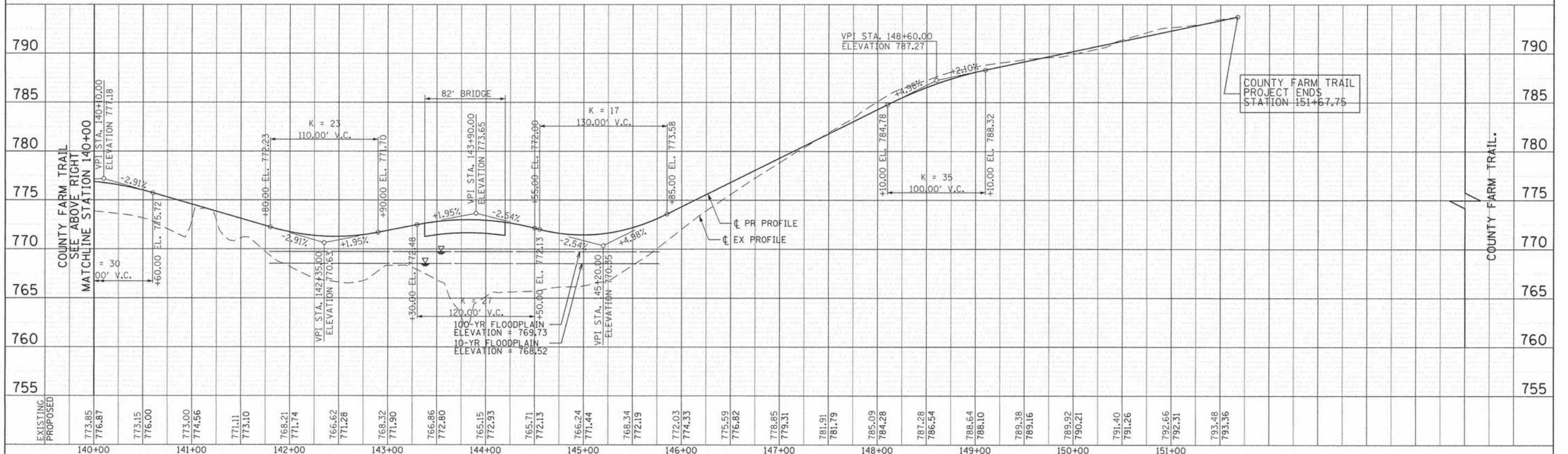
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| FILE NAME = G:\CH13\0055\Road\Sheets\30055-SHT-PROF11 | USER NAME = Mibeering  | DESIGNED - MLB   | REVISED - | <b>STATE OF ILLINOIS</b><br><b>DEPARTMENT OF TRANSPORTATION</b> | <b>COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS</b><br><b>TRAIL PROFILE</b> | F.A. RTE. 362   | SECTION 14-F3000-12-BT | COUNTY DUPAGE             | TOTAL SHEETS 69    | SHEET NO. 15 |  |  |
| PLOT SCALE = 50.0000' / 1\"/>                         |                        |                  |           |   |   |                 |                        |                           |                    |              |  |  |
| MODELNAME#  | PLOT DATE = 12/17/2015 | DATE = 12/7/2015 | REVISED - |   |   | SCALE: H: 1\"/> | SHEET 1 OF 2 SHEETS    | STA. 99+00 TO STA. 126+00 | CONTRACT NO. 61C31 |              |  |  |
| ILLINOIS FED. AID PROJECT                             |                        |                  |           |   |   |                 |                        |                           |                    |              |  |  |



|      |           |    |      |
|------|-----------|----|------|
| PLAN | SUBMITTED | BY | DATE |
| NO.  | PLOTTED   |    |      |
| NO.  | CHECKED   |    |      |
| NO.  | FILE NAME |    |      |



|         |           |    |      |
|---------|-----------|----|------|
| PROFILE | FORWARDED | BY | DATE |
| NO.     | PLOTTED   |    |      |
| NO.     | CHECKED   |    |      |
| NO.     | FILE NAME |    |      |



|  |                        |                  |           |   |   |                           |                        |               |                   |                    |  |  |
|--|------------------------|------------------|-----------|---|---|---------------------------|------------------------|---------------|-------------------|--------------------|--|--|
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| PLOT SCALE = 50.0000' / in.                                    | PLOT DATE = 12/17/2015 | DRAWN - MLB      | REVISED - |   |   | SCALE: H: 1"=50'          | SHEET 2 OF 2 SHEETS    | STA. 126+00   | TO STA. 151+67.49 | CONTRACT NO. 61C31 |  |  |
| #MODELNAME#  |                        | CHECKED - DWB    | REVISED - |   |   | ILLINOIS FED. AID PROJECT |                        |               |                   |                    |  |  |
|  |                        | DATE - 12/7/2015 | REVISED - |   |   |                           |                        |               |                   |                    |  |  |



**EROSION AND SEDIMENTATION CONTROL NOTES**

ALL CONSTRUCTION ACTIVITIES AND WATER QUALITY STANDARDS SHALL BE IN ACCORDANCE WITH THE IEPA NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES), IEPA SECTION 401 WATER QUALITY CONDITIONS, SOIL AND WATER CONSERVATION DISTRICT (SWCD), SECTION 280 AND 282 OF THE STANDARD SPECIFICATIONS, IDOT CONSTRUCTION MEMORANDUM NO. 06-60, AND STORMWATER POLLUTION PREVENTION PLAN (SWPPP). SEE IEPA SECTION 401 WATER QUALITY CONDITIONS AND SWPPP IN THE SPECIAL PROVISIONS.

UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE CONSTRUCTED ACCORDING TO MINIMUM STANDARDS AND SPECIFICATIONS IN THE ILLINOIS URBAN MANUAL.

A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.

THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE KANE - DUPAGE SOIL AND WATER CONSERVATION DISTRICT

SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.

THE KANE - DUPAGE SWCD MUST BE NOTIFIED ONE WEEK PRIOR TO THE PRECONSTRUCTION MEETING, ONE WEEK PRIOR TO THE COMMENCEMENT OF LAND DISTURBING ACTIVITIES, AND ONE WEEK PRIOR TO FINAL INSPECTION.

CONSTRUCTION LIMITS SHALL BE MINIMIZED TO KEEP SOIL DISTURBANCE TO A MINIMUM LEAVING AS MUCH EXISTING VEGETATION IN PLACE AS POSSIBLE. DIVERT "CLEAR" WATER FLOWING THROUGH THE CONSTRUCTION SITE AWAY FROM DISTURBED AREAS. INTERCEPT AND CONTAIN SEDIMENT CLOSE TO ITS SOURCE. ALL PROJECT-RELATED SEDIMENT SHALL BE RETAINED ON THE PROJECT SITE. NO RUNOFF FLOW FROM DISTURBED AREAS SHALL LEAVE THE SITE WITHOUT BEING TREATED.

ALL DISTURBED AREAS, EXISTING EROSION CONTROL MEASURES, VEHICLE ACCESS SITES AND ALL OTHER AREAS SUBJECT TO EROSION SHALL BE INSPECTED AT LEAST ONCE EVERY SEVEN DAYS AND WITHIN 24 HOURS OF THE END OF EACH ONE-HALF INCH OR GREATER RAINFALL OR EQUIVALENT SNOWFALL.

DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE.

THE CONTRACTOR SHALL COMPLETE PERMANENT EROSION CONTROL MEASURES AS SOON AS PRACTICAL AFTER THE COMPLETION OF GRADING. TEMPORARY MEASURES SHALL BE INSTALLED AND MAINTAINED UNTIL PERMANENT MEASURES ARE ESTABLISHED.

A QUANTITY OF TEMPORARY EROSION CONTROL SEEDING HAS BEEN PROVIDED TO REDUCE EROSION OF ALL EXPOSED EARTH SLOPES THAT ARE NOT READY FOR PERMANENT STABILIZATION, AREAS THAT REMAIN DENUDED FOR MORE THAN 14 DAYS, AREAS THAT WILL BE RE-DISTURBED, AND STOCKPILES. THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 280 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL APPLY TEMPORARY EROSION CONTROL SEEDING TO ALL ERODIBLE BARE EARTH AREAS WITHIN THE CONTRACT LIMITS EACH WEEK REGARDLESS OF WEATHER CONDITIONS OR PROGRESS OF THE WORK, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. ERODIBLE EMBANKMENT AND EXCAVATION AREAS WHERE WORK IS IN PROGRESS SHALL BE INCLUDED ON THE AREAS TO BE SEEDED.

ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.

SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF THE DEVELOPMENT SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SEEDING IS PERFORMED.

PROPERTIES AND CHANNELS ADJOINING THE DEVELOPMENT SITE SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION.

A STABILIZED MAT OF AGGREGATE UNDERLAIN WITH FILTER CLOTH (OR OTHER APPROPRIATE MEASURES) SHALL BE LOCATED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION-SITE OF A MAJOR DEVELOPMENT TO OR FROM A PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA. ANY SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA. THE WORK DESCRIBED IN THIS PARAGRAPH SHALL BE PAID FOR AS STABILIZED CONSTRUCTION ENTRANCE.

DUST SHALL BE CONTROLLED IN ACCORDANCE WITH ARTICLE 107.36 OF THE STANDARD SPECIFICATIONS.

IF A STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN THREE DAYS, THEN EROSION AND SEDIMENT CONTROL SHALL BE PROVIDED. STOCKPILES TO REMAIN IN PLACE FOR 30 DAYS OR MORE SHALL RECEIVE TEMPORARY SEEDING.

SOIL STOCKPILES SHALL NOT BE LOCATED IN A FLOOD-PRONE AREA OR A DESIGNATED BUFFER PROTECTING WATERS OF THE UNITED STATES, OR ISOLATED WATERS OF DUPAGE COUNTY.

ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED IN A WORKING CONDITION.

ALL EROSION CONTROL PRODUCTS FURNISHED SHALL BE SPECIFICALLY RECOMMENDED BY THE MANUFACTURER FOR THE USE SPECIFIED IN THE EROSION CONTROL PLAN. PRIOR TO THE APPROVAL AND USE OF THE PRODUCT, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A NOTARIZED CERTIFICATION BY THE PRODUCER STATING THE INTENDED USE OF THE PRODUCT, AND THAT THE PHYSICAL PROPERTIES REQUIRED FOR THIS APPLICATION ARE MET OR EXCEEDED. THE CONTRACTOR SHALL PROVIDE MANUFACTURER INSTALLATION PROCEDURES TO FACILITATE THE ENGINEER IN CONSTRUCTION INSPECTIONS.

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY WITHIN THE PROJECT LIMITS AT LOCATIONS AS DESIGNATED BY THE ENGINEER. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OF OTHER POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

THE CONTRACTOR SHALL PROVIDE WATER-TIGHT TANKS OR BARRELS, OR CONSTRUCT A SUMP SEALED WITH PLASTIC SHEETS TO BE USED TO DISPOSE OF CHEMICAL POLLUTANTS, SUCH AS DRAINED LUBRICATING OR TRANSMISSION FLUIDS, GREASE, SOAPS, CONCRETE MIXER WASH WATER, ASPHALT, ETC., PRODUCED AS A BY-PRODUCT OF THE CONSTRUCTION ACTIVITIES. AT THE COMPLETION OF THE CONSTRUCTION WORK, SUMPS SHALL BE REMOVED AND THE AREA RESTORED TO ITS ORIGINAL CONDITION AS SPECIFIED IN SECTION 8 OF THE ILLINOIS URBAN MANUAL. SUMP REMOVAL SHALL BE CONDUCTED WITHOUT CAUSING POLLUTION.

SANITARY FACILITIES SUCH AS CHEMICAL TOILETS OR SEPTIC TANKS SHALL NOT BE LOCATED ADJACENT TO LIVE STREAMS, WELLS, OR SPRINGS. THEY SHALL BE LOCATED AT A DISTANCE SUFFICIENT TO PREVENT CONTAMINATION OF ANY WATER SOURCE. AT THE COMPLETION OF CONSTRUCTION ACTIVITIES, FACILITIES SHALL BE DISPOSED OF WITHOUT CAUSING POLLUTION AS SPECIFIED IN SECTION 8 OF THE ILLINOIS URBAN MANUAL.

ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (30) DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.

THE CONTRACTOR'S OPERATIONS SHALL NOT DISTURB THE WEST BRANCH OF DUPAGE RIVER AND THE KEENEVILLE DITCH BANKS.

THE CONTRACTOR SHALL PROVIDE ADEQUATE RECEPTACLES FOR THE DEPOSITION OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE DEVELOPMENT PROCESS. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL DEBRIS UPON OR INTO ANY DEVELOPMENT SITE, CHANNEL, WATERS OF THE U.S., OR ISOLATED WATERS OF DUPAGE COUNTY. THE CONTRACTOR SHALL MAINTAIN THE DEVELOPMENT SITE FREE OF CONSTRUCTION MATERIAL DEBRIS.

ALL HEAVY EQUIPMENT SHALL BE POWER-WASHED TO REMOVE ANY INVASIVE SEED PRESENT ON EQUIPMENT PRIOR TO ENTERING THE CONSTRUCTION SITE.

THE EXCAVATED MATERIAL BEING TRANSPORTED OFF SITE, SPECIFICALLY FROM THE WATERWAYS, SHALL BE PREVENTED FROM LEAKING OR SPILLING ONTO THE ROADWAYS BY DRYING THE MATERIAL BEFORE TRANSPORT, PROVIDING A LINER FOR THE TRUCKS, OR OTHER APPROVED METHOD.

**EROSION AND SEDIMENTATION CONTROL SEQUENCES**

I. THE FOLLOWING EROSION CONTROL MEASURES ARE TO BE INSTALLED PRIOR TO CLEARING

SILT FENCES  
ERECT ALL SILT FENCES AS SHOWN ON THE EROSION CONTROL PLANS TO CONTROL SEDIMENT FROM THE RUNOFF LEAVING THE DISTURBED AREAS.

TREE PROTECTION  
INSTALL TREE PROTECTION TO THE TREES SHOWN ON THE EROSION AND SEDIMENT CONTROL PLAN OR AS DIRECTED BY THE ENGINEER.

II. THE FOLLOWING EROSION CONTROL MEASURE IS TO TAKE PLACE PRIOR TO GRADING

TEMPORARY DITCH CHECK  
INSTALL TEMPORARY AND PERMANENT DITCH CHECKS IN EXISTING DITCHES TO CONTROL RUNOFF VELOCITY AS SHOWN ON THE EROSION CONTROL PLAN OR AS DIRECTED BY ENGINEER.

III. WITHIN 7 DAYS OF THE COMPLETION OF CLEARING OR GRADING OR WITHIN 14 DAYS OF LAST DISTURBANCE, THE FOLLOWING MEASURES SHALL BE ENFORCED.

TEMPORARY STABILIZATION  
PROVIDE TEMPORARY STABILIZATION OVER AREAS THAT CANNOT BE STABILIZED WITH PERMANENT VEGETATIVE MEASURES FOR 14 DAYS OR MORE AND CONSEQUENTLY REQUIRE TEMPORARY SEEDING AND TEMPORARY EROSION CONTROL BLANKET. THESE AREAS SHALL BE TREATED WITH PERMANENT VEGETATIVE COVER AT SOME FUTURE DATE.

PERMANENT EROSION CONTROL  
PROVIDE PERMANENT VEGETATION AND INSTALL ALL THE PERMANENT EROSION CONTROL MEASURES AS SHOWN ON THE PLANS OR DIRECTED BY ENGINEER, BEFORE REMOVAL OF THE TEMPORARY EROSION CONTROL MEASURES.

IV. THE FOLLOWING EROSION CONTROL MEASURE SHALL BE PERFORMED AFTER PROPOSED PRIOR TO TOPSOIL STRIPPING AND GRADING

INLET FILTER PROTECTION  
INLET PROTECTION SHALL BE PROVIDED TO ALL PROPOSED INLETS OR CATCH BASINS LOCATED IN ALL AREAS AS SHOWN ON EROSION CONTROL PLANS OR AS DIRECTED BY THE ENGINEER.

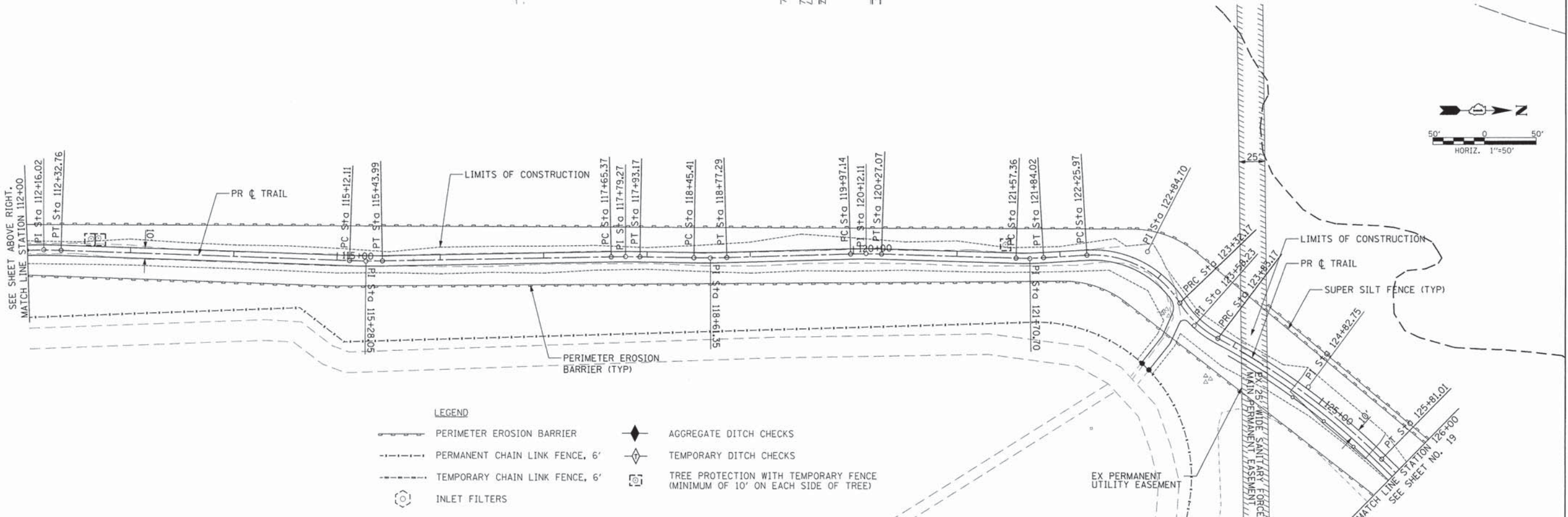
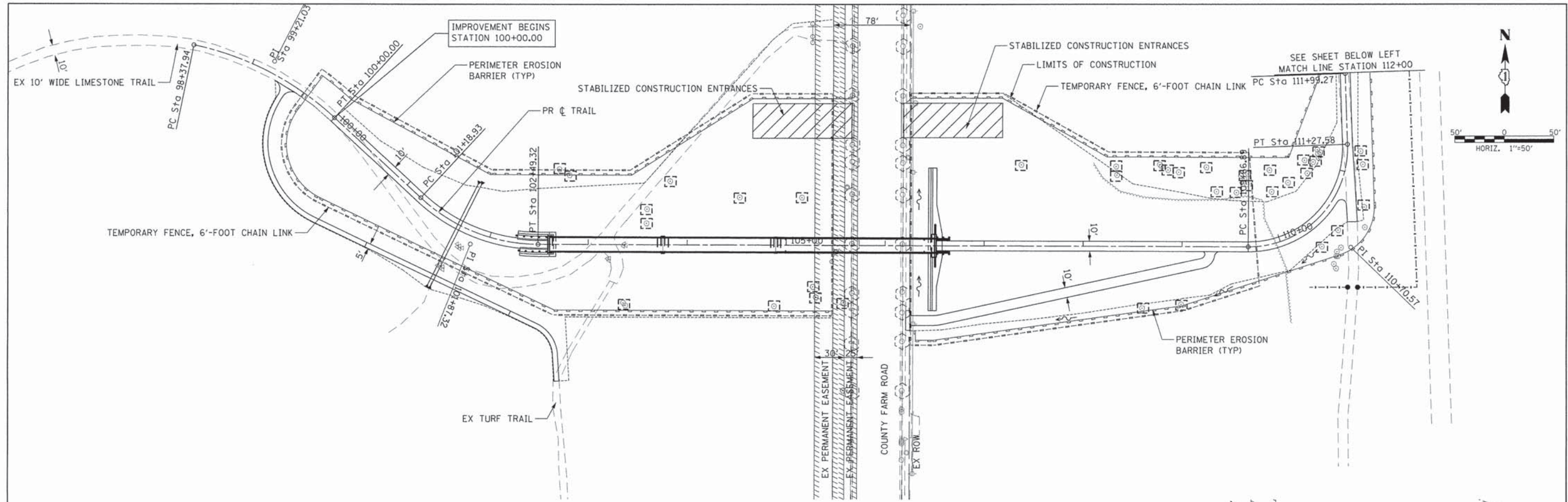
V. THE FOLLOWING MEASURES SHALL BE PROVIDED DURING THE CONTRACT ON AN AS NEEDED BASIS.

DUST CONTROL WATERING  
DUST CONTROL WATERING SHALL BE APPLIED TO CONTROL THE DUST RESULTING FROM THE CONSTRUCTION OPERATION.

STREET CLEANING AND SWEEPING  
STREET CLEANING AND SWEEPING SHALL BE PERFORMED ON EACH WORKDAY, AS REQUIRED AND DIRECTED AND APPROVED BY THE ENGINEER. COST FOR THIS WORK WILL BE INCLUDED IN THE COST OF EARTH EXCAVATION.

|  |                        |                |                   |   |  |                |         |        |              |           |
|--|------------------------|----------------|-------------------|---|--|----------------|---------|--------|--------------|-----------|
| FILE NAME =  | USER NAME = bshaef1@er | DESIGNED - MLB | REVISED -         | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS<br/>EROSION AND SEDIMENT CONTROL NOTES</b> | F.A.U. RTE.    | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| G:\CH13\0055\Road\Sheets\30055-SHT-EROSION NOTES.dgn | DRAWN - MLB            | REVISED -      | 362               |   |  | 14-F3000-12-BT | DUPAGE  | 69     | 17           |           |
| PLOT SCALE =   | CHECKED - DWB          | REVISED -      | CONTRACT NO 61C31 |   |  |                |         |        |              |           |
| PLOT DATE = 12/16/2015                               | DATE - 12/7/2015       | REVISED -      | SCALE: NONE       |   |  | SHEET NO.      | OF      | SHEETS | STA.         | TO STA.   |

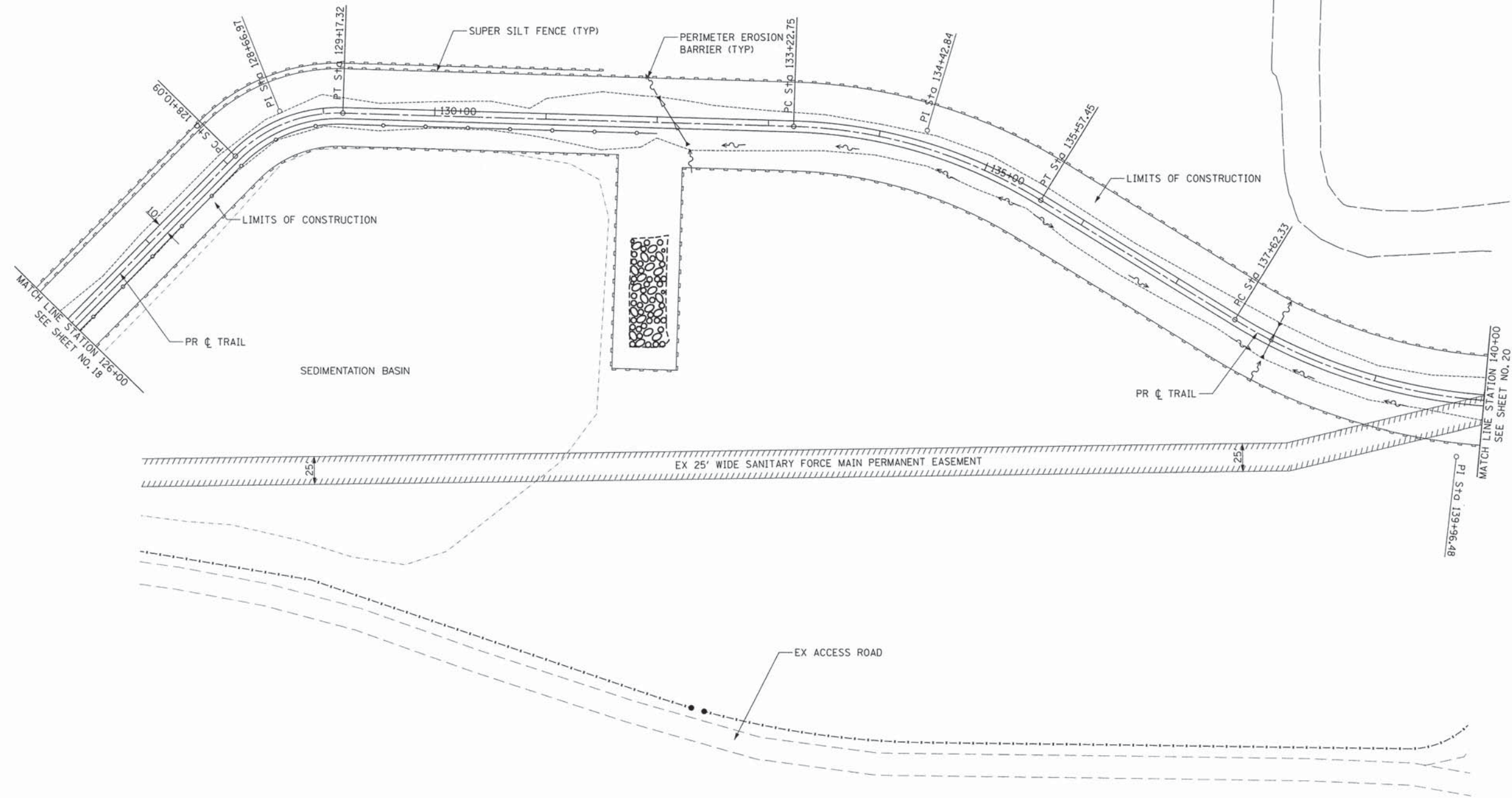




- LEGEND**
- PERIMETER EROSION BARRIER
  - PERMANENT CHAIN LINK FENCE, 6'
  - TEMPORARY CHAIN LINK FENCE, 6'
  - INLET FILTERS
  - AGGREGATE DITCH CHECKS
  - TEMPORARY DITCH CHECKS
  - TREE PROTECTION WITH TEMPORARY FENCE (MINIMUM OF 10' ON EACH SIDE OF TREE)

|  |                         |                |           |   |   |                   |                          |                            |                           |                   |  |  |
|--|-------------------------|----------------|-----------|---|---|-------------------|--------------------------|----------------------------|---------------------------|-------------------|--|--|
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| PLOT SCALE =   | DATE = 12/7/2015        | DRAWN - MLB    | REVISED - |   |   | SCALE: 1"=50'     | SHEET NO. 1 OF 3 SHEETS  | STA. 100+00 TO STA. 126+00 | ILLINOIS FED. AID PROJECT | CONTRACT NO 61C31 |  |  |
| PLOT DATE = 12/16/2015                                     | DATE = 12/7/2015        | CHECKED - DWB  | REVISED - |   |   |                   |                          |                            |                           |                   |  |  |
|  |                         |                |           |   |   |                   |                          |                            |                           |                   |  |  |





|   |                          |                |           |
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| PLOT SCALE =                            | CHECKED - DWB            | REVISED -      | REVISED - |
| PLOT DATE = 12/16/2015                  | DATE - 12/7/2015         | REVISED -      | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

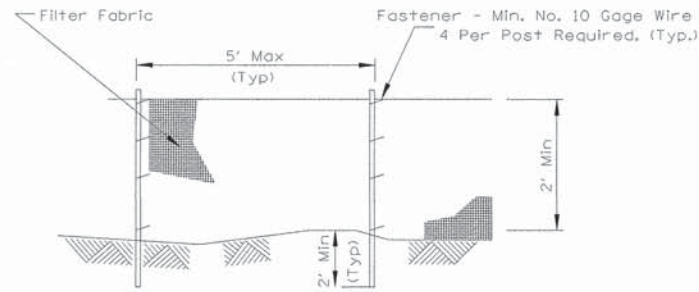
**COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS  
EROSION AND SEDIMENT CONTROL PLAN**

SCALE: 1"=50' SHEET NO. 2 OF 3 SHEETS STA. 126+00 TO STA. 140+00

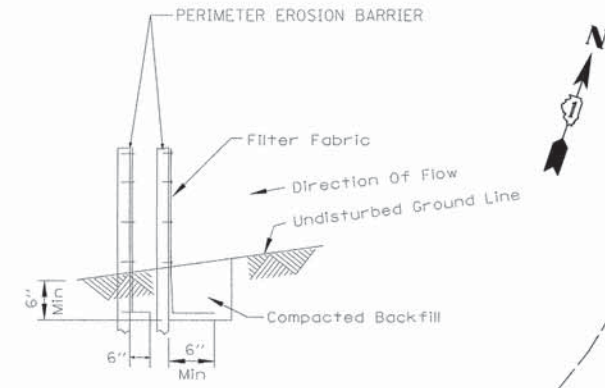
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| F.A.U. RTE.         | SECTION        | COUNTY                    | TOTAL SHEETS      | SHEET NO. |
| 362                 | 14-F3000-12-BT |                           | 69                | 19        |
| FED. ROAD DIST. NO. |                | ILLINOIS FED. AID PROJECT | CONTRACT NO 61C31 |           |



**SUPER SILT FENCE**

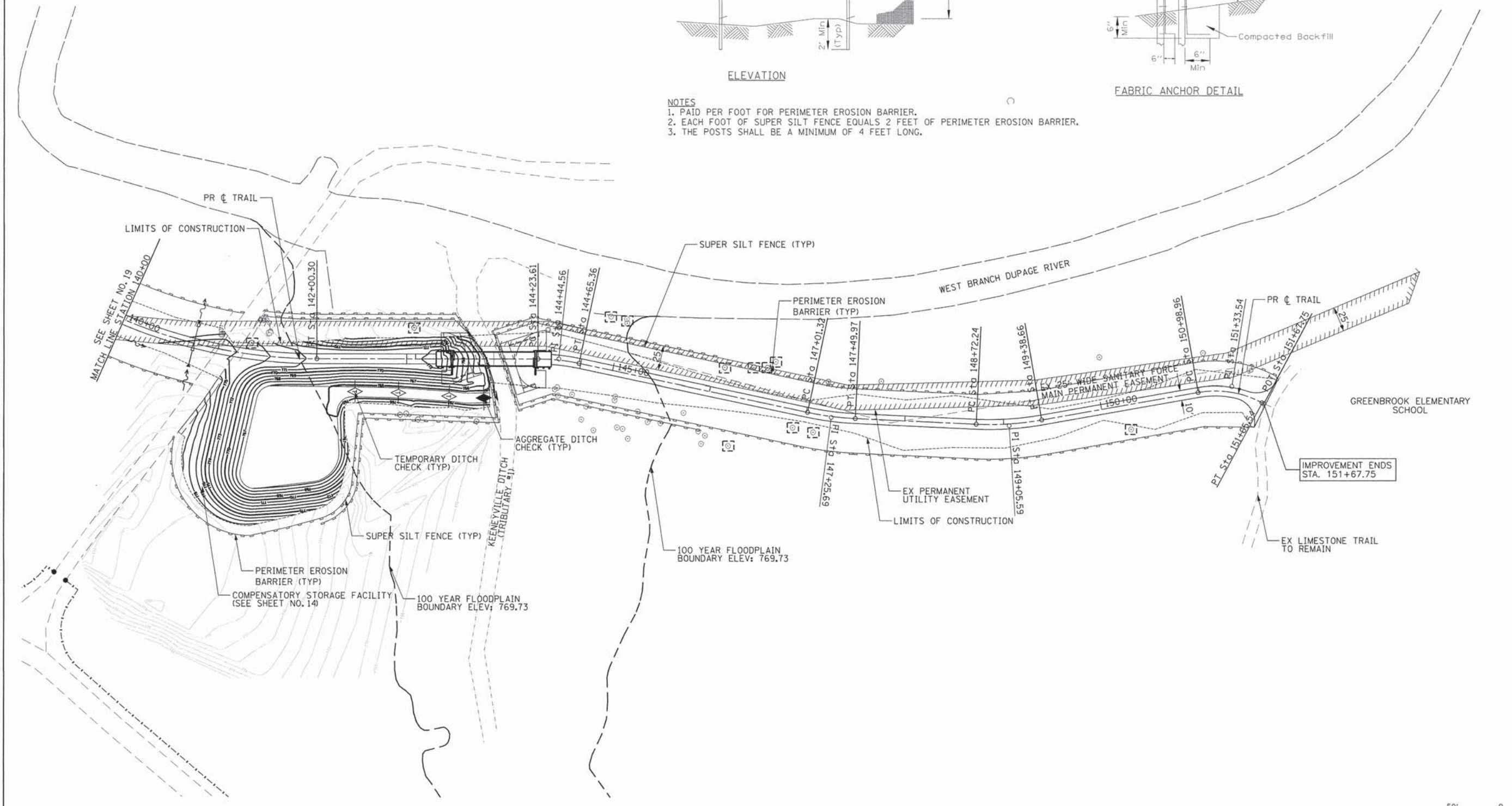


ELEVATION



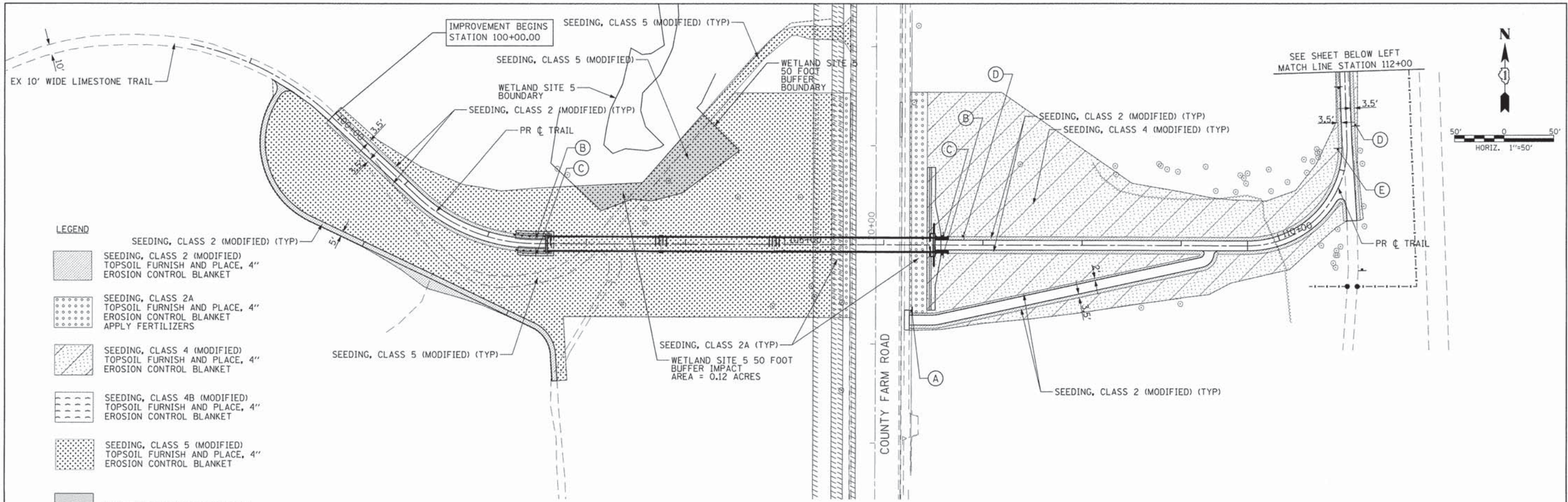
FABRIC ANCHOR DETAIL

- NOTES**
1. PAID PER FOOT FOR PERIMETER EROSION BARRIER.
  2. EACH FOOT OF SUPER SILT FENCE EQUALS 2 FEET OF PERIMETER EROSION BARRIER.
  3. THE POSTS SHALL BE A MINIMUM OF 4 FEET LONG.



|  |                        |                |           |   |   |   |                          |                 |                   |                |  |
|--|------------------------|----------------|-----------|---|---|---|--------------------------|-----------------|-------------------|----------------|--|
| FILE NAME = G:\CH13\0855\Road\Sheets\30855-SHT-EROSION.dgn | USER NAME = bshae1lger | DESIGNED - MLB | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS<br/>EROSION AND SEDIMENT CONTROL PLAN</b> | F.A.U. RTE. = 362   | SECTION = 14-F3000-12-BT | COUNTY = DUPAGE | TOTAL SHEETS = 69 | SHEET NO. = 20 |  |
| PLOT SCALE =   | CHECKED - DWB          | REVISER -      | REVISER - |   |   | CONTRACT NO 61C31   |                          |                 |                   |                |  |
| PLOT DATE = 12/16/2015                                     | DATE = 12/7/2015       | REVISER -      | REVISER - |   |   | SCALE: 1"=50' SHEET NO. 3 OF 3 SHEETS STA. 140+00 TO STA. 151+67.49 |                          |                 |                   |                |  |
|  |                        |                |           |   |   | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT                       |                          |                 |                   |                |  |





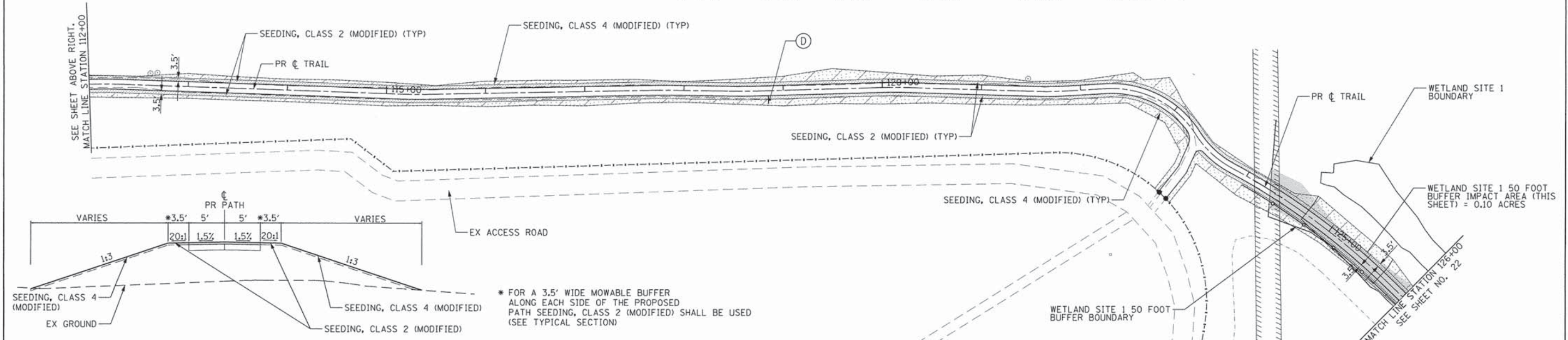
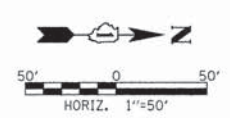
**LEGEND**

- SEEDING, CLASS 2 (MODIFIED) (TYP)  
TOPSOIL FURNISH AND PLACE, 4"  
EROSION CONTROL BLANKET
- SEEDING, CLASS 2A  
TOPSOIL FURNISH AND PLACE, 4"  
EROSION CONTROL BLANKET  
APPLY FERTILIZERS
- SEEDING, CLASS 4 (MODIFIED)  
TOPSOIL FURNISH AND PLACE, 4"  
EROSION CONTROL BLANKET
- SEEDING, CLASS 4B (MODIFIED)  
TOPSOIL FURNISH AND PLACE, 4"  
EROSION CONTROL BLANKET
- SEEDING, CLASS 5 (MODIFIED)  
TOPSOIL FURNISH AND PLACE, 4"  
EROSION CONTROL BLANKET
- WETLAND BUFFER IMPACT AREA

NOTE: USE HEAVY DUTY EROSION CONTROL BLANKET FOR SLOPES STEEPER THAN 1:3.

**SIGNING LEGEND**

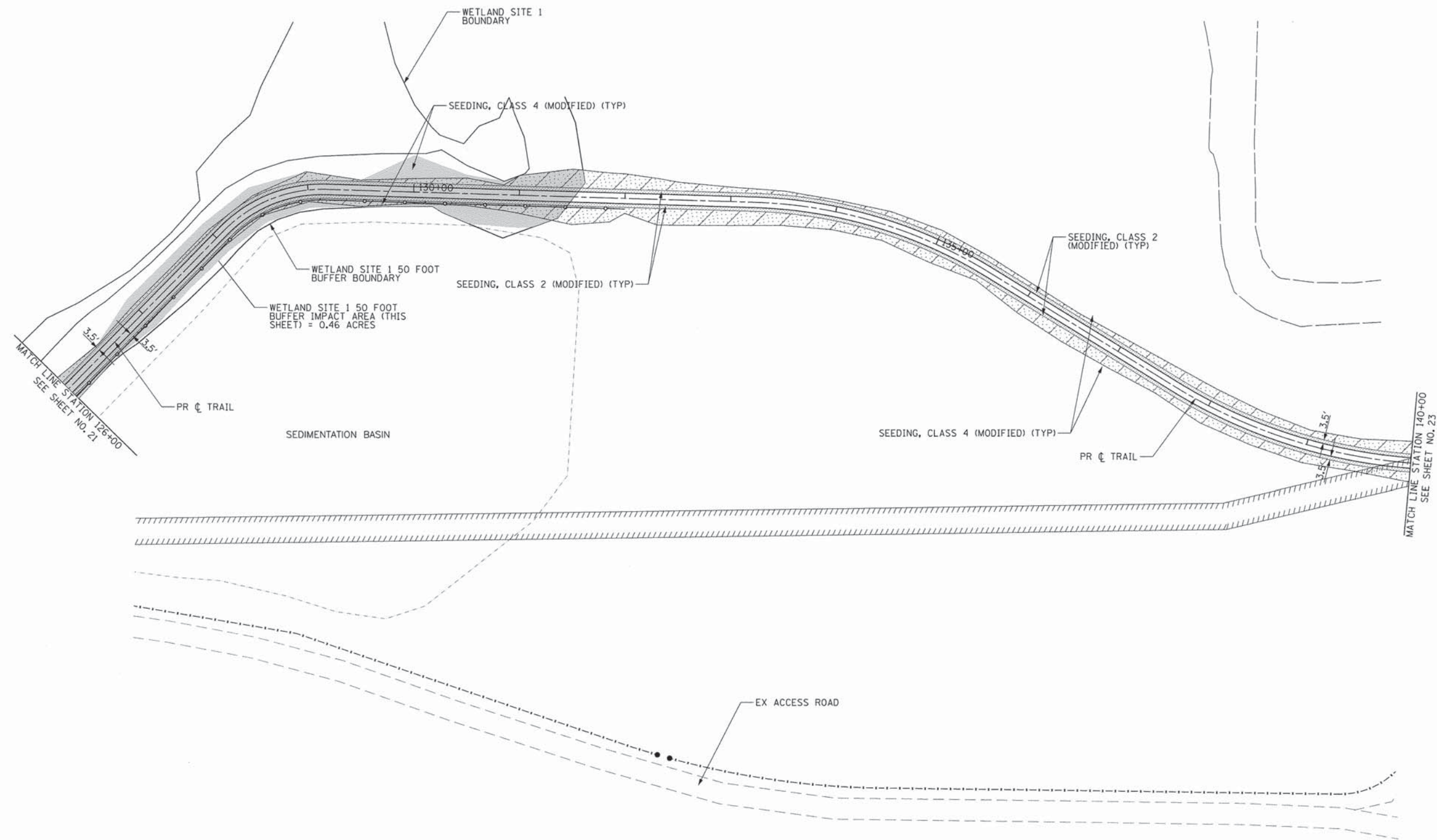
- A  
R1-1  
18" X 18"
- B  
OM3-L  
6" X 18"
- C  
OM3-R  
6" X 18"
- D  
W7-5  
18" X 18"
- E  
W1-2  
18" X 18"
- F  
R1-2  
18" X 18" X 18"



|   |                        |                |             |   |  |                         |                            |                        |                           |                   |              |  |
|---|------------------------|----------------|-------------|---|--|-------------------------|----------------------------|------------------------|---------------------------|-------------------|--------------|--|
| FILE NAME = G:\CH3\0095\Road\Sheets\30095-SHT-LSS.dgn | USER NAME = bahae1iger | DESIGNED - MLB | REVISED -   | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS<br/>TRAIL SIGNING &amp; LANDSCAPING PLAN</b> |                         | F.A.U. RTE. 362            | SECTION 14-F3000-12-BT | COUNTY DUPAGE             | TOTAL SHEETS 69   | SHEET NO. 21 |  |
| PLOT SCALE =  | PLOT DATE = 12/16/2015 | DRAWN - MLB    | REVISED -   |   | SCALE: 1"=50'  | SHEET NO. 1 OF 3 SHEETS | STA. 100+00 TO STA. 126+00 | FED. ROAD DIST. NO.    | ILLINOIS FED. AID PROJECT | CONTRACT NO 61C31 |              |  |
| CHECKED - DWB   | DATE - 12/7/2015       | REVISOR -      | REVISIONS - |   |  |                         |                            |                        |                           |                   |              |  |
|   |                        |                |             |   |  |                         |                            |                        |                           |                   |              |  |
|   |                        |                |             |   |  |                         |                            |                        |                           |                   |              |  |



FOR LANDSCAPING NOTES AND  
LEGEND SEE SHEET NO. 21



|  |                        |                  |           |
|--|------------------------|------------------|-----------|
| FILE NAME =                                | USER NAME = bshae1zger | DESIGNED - MLB   | REVISED - |
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| PLOT SCALE =                               |                        | CHECKED - DWB    | REVISED - |
| PLOT DATE = 12/16/2015                     |                        | DATE - 12/7/2015 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

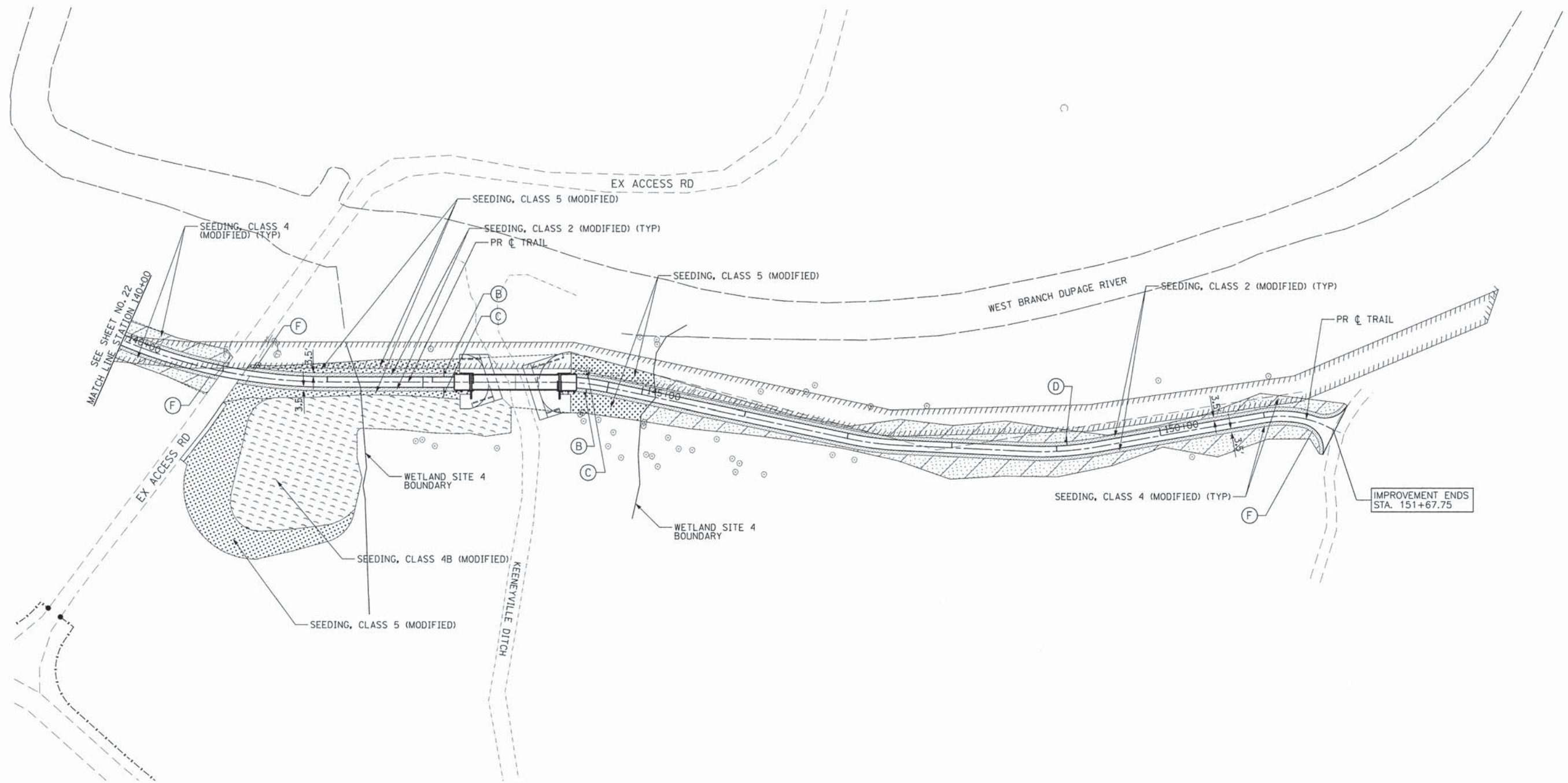
**COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS  
TRAIL SIGNING & LANDSCAPING PLAN**

SCALE: 1"=50'    SHEET NO. 2 OF 3 SHEETS    STA. 126+00 TO STA. 140+00

|  |                |        |              |           |
|--|----------------|--------|--------------|-----------|
| F.A.U. RTE.                                      | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 362  | 14-F3000-12-BT | DUPAGE | 69           | 22        |
| CONTRACT NO 61C31                                |                |        |              |           |
| FED. ROAD DIST. NO.    ILLINOIS FED. AID PROJECT |                |        |              |           |



FOR LANDSCAPING NOTES AND  
LEGEND SEE SHEET NO. 21



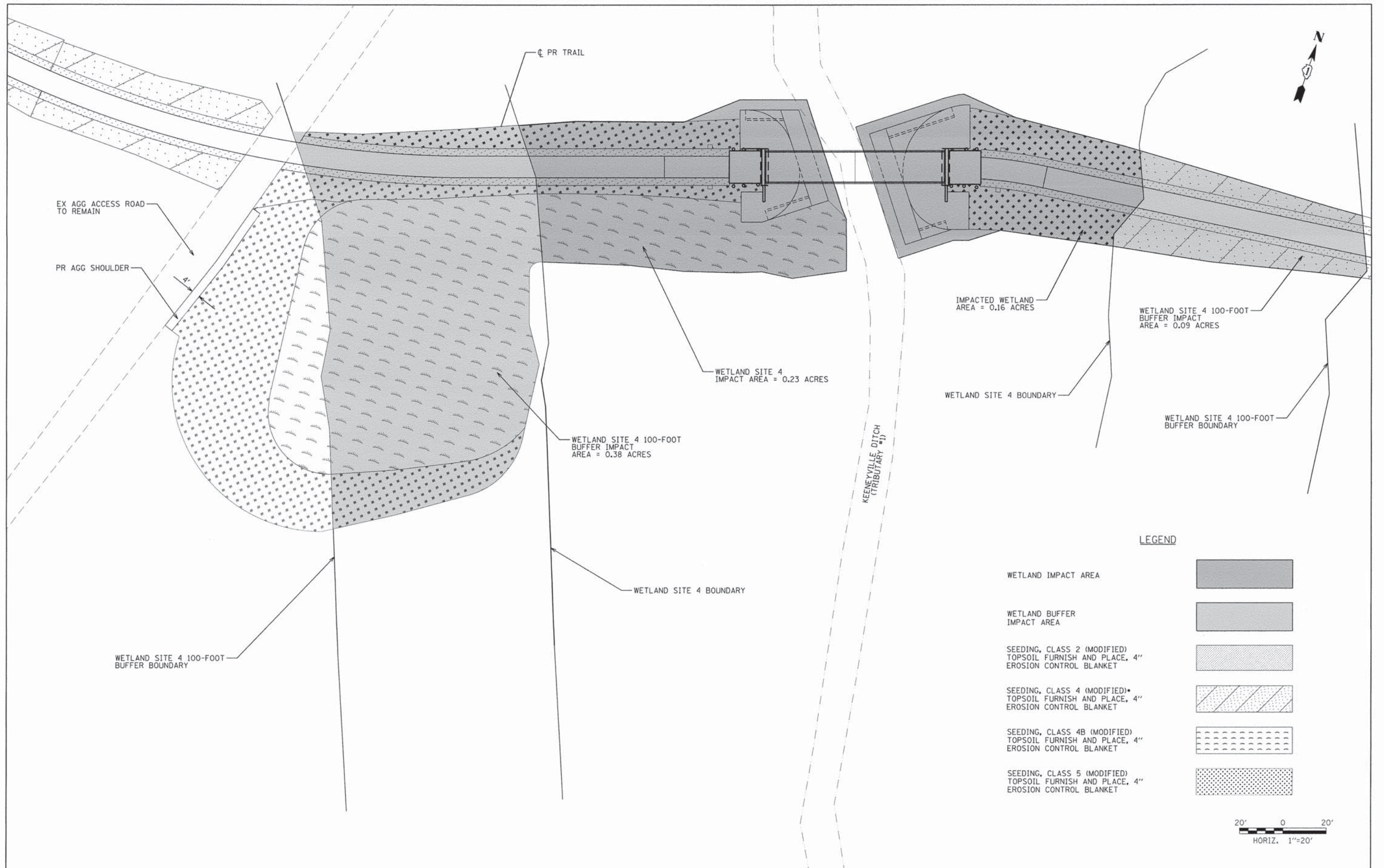
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| PLOT SCALE =   |                         | DRAWN - MLB      | REVISED - |
| PLOT DATE = 12/16/2015                                     |                         | CHECKED - DWB    | REVISED - |
|  |                         | DATE - 12/7/2015 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|  |                         |                               |   |
|--|-------------------------|-------------------------------|---|
| <b>COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS<br/>TRAIL SIGNING &amp; LANDSCAPING PLAN</b> |                         |                               |   |
| SCALE: 1"=50'  | SHEET NO. 3 OF 3 SHEETS | STA. 140+00 TO STA. 151+67.49 | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |

|                    |                           |                  |                    |                 |
|--------------------|---------------------------|------------------|--------------------|-----------------|
| F.A.U. RTE.<br>362 | SECTION<br>14-F3000-12-BT | COUNTY<br>DuPAGE | TOTAL SHEETS<br>69 | SHEET NO.<br>23 |
| CONTRACT NO 61C31  |                           |                  |                    |                 |





|   |                         |                  |           |
|---|-------------------------|------------------|-----------|
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| PLOT SCALE =  |                         | CHECKED - DWB    | REVISED - |
| PLOT DATE = 12/16/2015                                  |                         | DATE - 12/7/2015 | REVISED - |

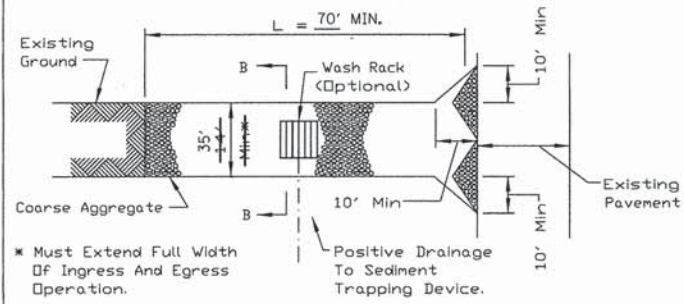
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS  
COMPENSATORY STORAGE PLANTING PLAN**

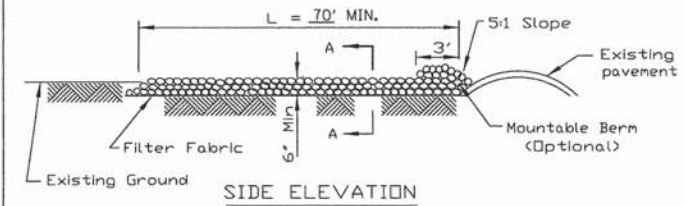
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|--|----------------|--------|--------------|-----------|
| F.A.U. RTE.  | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 362  | 14-F3000-12-BT | DUPAGE | 69           | 24        |
| CONTRACT NO 61C31  |                |        |              |           |
| SCALE: 1"=20' SHEET NO. 24 OF 69 SHEETS STA. 140+00 TO STA. 146+00 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |



STABILIZED CONSTRUCTION ENTRANCE PLAN



PLAN VIEW



SIDE ELEVATION

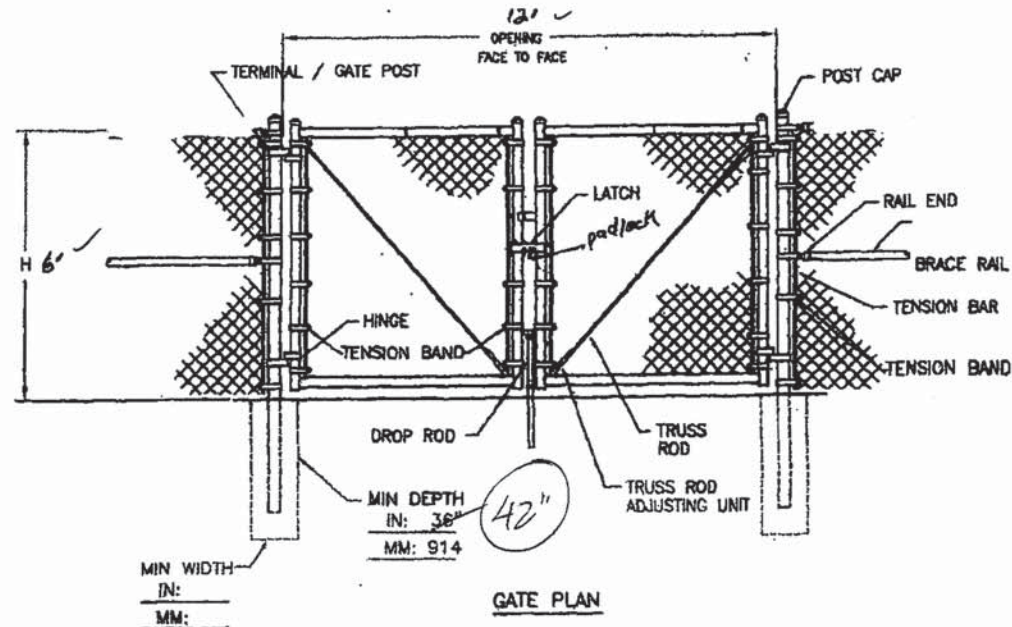
NOTES:

1. Filter fabric shall meet the requirements of material specification 592 GEOTEXTILE, Table I or 2, Class I, II or IV and shall be placed over the cleared area prior to the placing of rock.
2. Rock or reclaimed concrete shall meet one of the following IDDT coarse aggregate gradation, CA-1, CA-2, CA-3 or CA-4 and be placed according to construction specification 25 ROCKFILL using placement Method 1 and Class III compaction.
3. Any drainage facilities required because of washing shall be constructed according to manufacturers specifications.
4. If wash racks are used they shall be installed according to the manufacturer's specifications.

|           |         |       |
|-----------|---------|-------|
| REFERENCE | Project | _____ |
| Designed  | Date    | _____ |
| Checked   | Date    | _____ |
| Approved  | Date    | _____ |



|                   |         |
|-------------------|---------|
| STANDARD DWG. NO. | IL-630  |
| SHEET 1 OF 2      |         |
| DATE              | 8-18-94 |

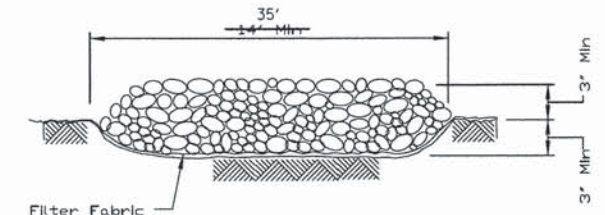


GATE PLAN

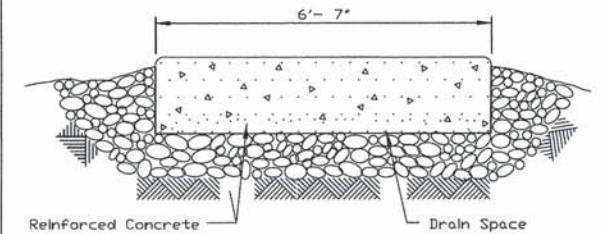
NOTES:

1. METRIC DIMENSIONS ARE NOMINAL EQUIVALENTS TO U.S. DIMENSIONS.
2. SPECIFICATIONS SHOWN CAN BE CHANGED BY THE MANUFACTURER ONLY.
3. FOOTING WIDTH TO BE 4 X POST WIDTH
4. BLACK PVC COATED FINISH

STABILIZED CONSTRUCTION ENTRANCE PLAN



SECTION A-A



SECTION B-B

|           |         |       |
|-----------|---------|-------|
| REFERENCE | Project | _____ |
| Designed  | Date    | _____ |
| Checked   | Date    | _____ |
| Approved  | Date    | _____ |



|                   |         |
|-------------------|---------|
| STANDARD DWG. NO. | IL-630  |
| SHEET 2 OF 2      |         |
| DATE              | 8-18-94 |



Benchmark: Cross cut in northwest flange bolt of fire hydrant on west side of County Farm Road, Elev. 797.59

Existing Structure: None.



*Matthew D. Santeford* 12-17-2015  
 MATTHEW D. SANTEFORD, P.E., S.E.  
 NO. 081-007244  
 EXP. DATE 11/30/2016

**CURVE DATA**

**PRBIKE-2**  
 P.I. Sta. = 101+87.32  
 $\Delta = 42^\circ 41' 18''$  (LT)  
 $D = 32^\circ 44' 26''$   
 $R = 175.00'$   
 $T = 68.39'$   
 $L = 130.38'$   
 $E = 12.89'$   
 P.C. Sta. = 101+18.93  
 P.T. Sta. = 102+49.32

"I certify that to the best of my knowledge, information and belief, this bridge design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with the requirements of the current 'AASHTO LRFD Bridge Design Specifications'."

**LOADING**

Pedestrian Live Load = 90 psf  
 Vehicle Live Load = H10

**DESIGN SPECIFICATIONS**

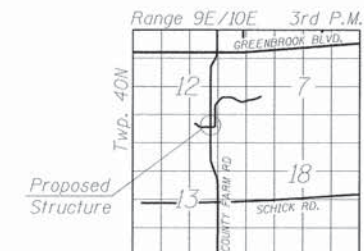
2014 AASHTO LRFD Bridge Design Specifications, 7th Edition  
 2009 AASHTO LRFD Guide Specifications for the Design of Pedestrian Bridges  
 Geosynthetic Reinforced Soil Integrated Bridge System Interim Implementation Guide, FHWA-HRT-11-026, June 2012

**DESIGN STRESSES**

**FIELD UNITS**  
 $f'_c = 3,500$  psi  
 $f'_c = 4,500$  psi (Precast Modular Blocks)  
 $f_y = 60,000$  psi (Reinforcement)  
 $f_y = 50,000$  psi (M270 Grade 50W)

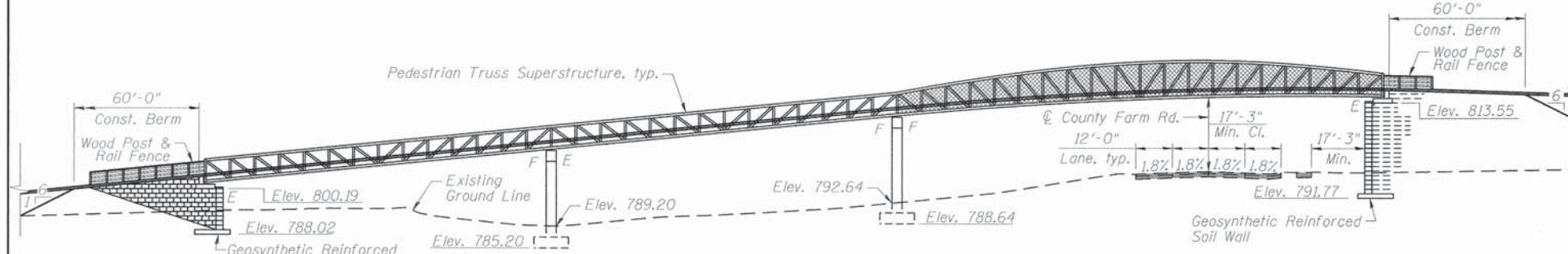
**SEISMIC DATA**

Seismic Performance Zone (SPZ) = 1  
 Design Spectral Acceleration at 1.0 sec. ( $S_{D1}$ ) = 0.061g  
 Design Spectral Acceleration at 0.2 sec. ( $S_{D5}$ ) = 0.115g  
 Soil Site Class = C

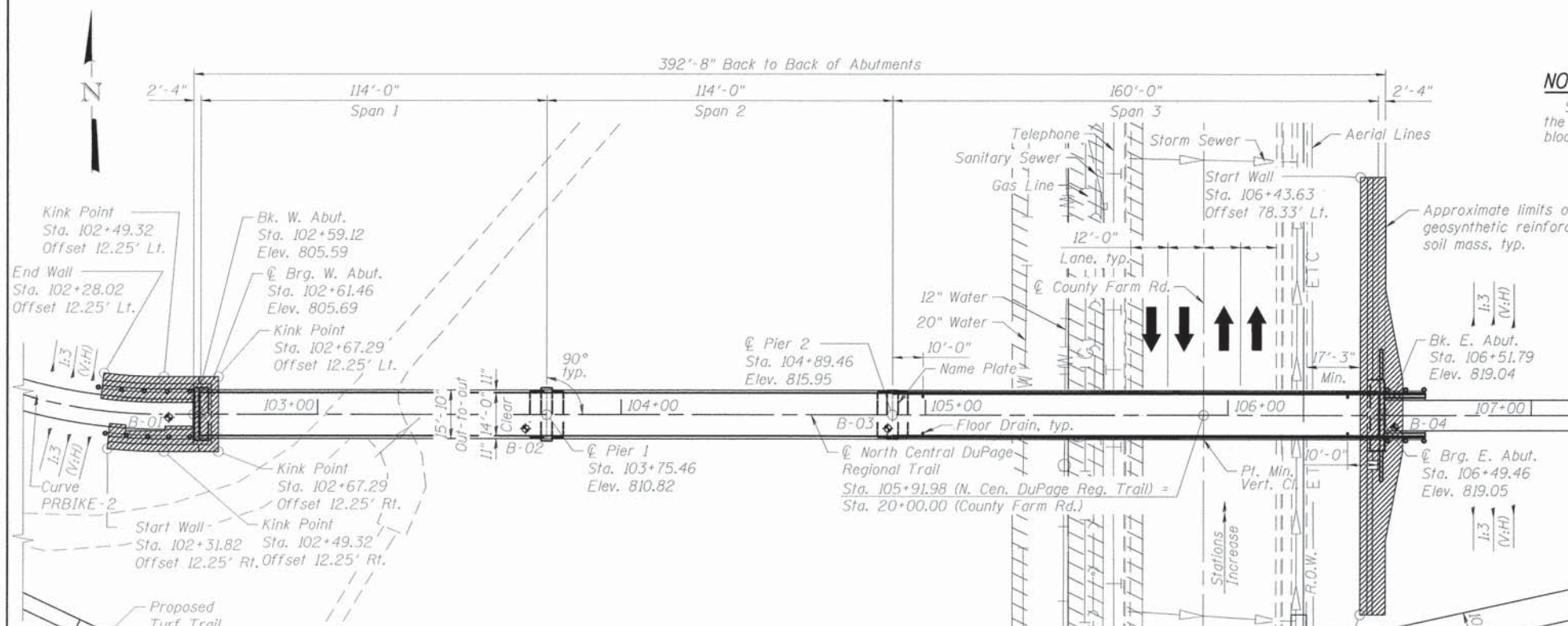


**LOCATION SKETCH**

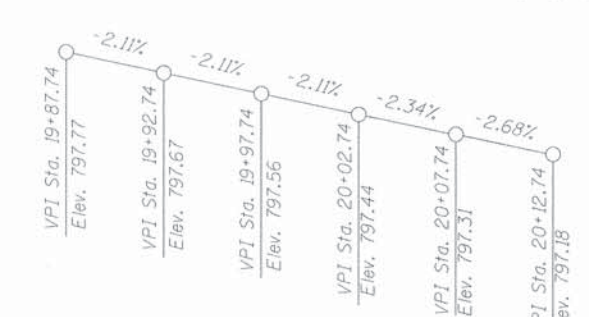
**GENERAL PLAN AND ELEVATION**  
**NORTH CENTRAL DUPAGE**  
**REGIONAL TRAIL OVER**  
**COUNTY FARM ROAD**  
**KEY ROUTE 0362**  
**SECTION 14-F3000-12-BT**  
**DUPAGE COUNTY**  
**STATION 105+91.98**  
**STRUCTURE NUMBER 022-3186**



**ELEVATION**

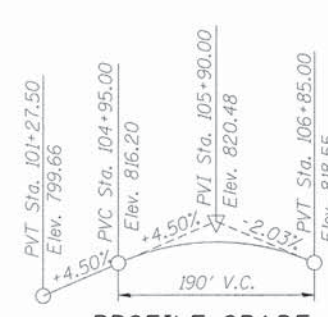


**PLAN**



**PROFILE GRADE**

Along  $\text{\textcircled{C}}$  County Farm Road



**PROFILE GRADE**

Along  $\text{\textcircled{C}}$  North Central DuPage Regional Trail

**NOTE:**

Stations and offsets are along the front face of precast concrete blocks.

4:57:13 PM  
 xxxxx-01-GPE.dgn

|                          |                        |              |         |   |  |                           |                |        |              |           |  |
|--------------------------|------------------------|--------------|---------|---|--|---------------------------|----------------|--------|--------------|-----------|--|
|                          | USER NAME = bshoeffler | DESIGNED JNP | REVISED | <b>STATE OF ILLINOIS</b><br><b>DEPARTMENT OF TRANSPORTATION</b> | <b>GENERAL PLAN AND ELEVATION</b><br><b>STRUCTURE NO. 022-3186</b> | F.A.P.                    | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |  |
|                          |                        | CHECKED JRM  | REVISED |   |  | 0362                      | 14-F3000-12-BT | DUPAGE | 69           | 26        |  |
|                          |                        | DRAWN JNP    | REVISED |   |  | CONTRACT NO 61C31         |                |        |              |           |  |
|                          |                        | CHECKED JRM  | REVISED |   |  | ILLINOIS FED. AID PROJECT |                |        |              |           |  |
| SHEET NO. 1 OF 10 SHEETS |                        |              |         |   |  |                           |                |        |              |           |  |



**GENERAL NOTES**

1. Fasteners shall be ASTM A325 Type 1, mechanically galvanized bolts in painted areas and ASTM A325 Type 3 in unpainted areas. Bolt size shall be determined by the Contractor.
2. All structural steel shall be AASHTO M270 Grade 50W (except expansion joints which shall be AASHTO M270 Grade 36)
3. No field welding is permitted except as specified in the contract documents.
4. Reinforcement bars designated (E) shall be epoxy coated.
5. Concrete sealer shall be applied to the designated areas of the exposed surfaces of the pier caps and abutments up to the back of backwall location.
6. All structural steel and exposed surfaces of bearings within a distance of 10 ft. each way from the deck joints shall be painted as specified in Section 506 of the Standard Specifications.
7. The embankment configuration shown shall be the minimum that must be placed and compacted prior to construction of the abutments.
8. The Organic Zinc Rich Primer / Epoxy / Urethane Paint System shall be used for painting the new structural steel except where otherwise noted. The entire system shall be shop applied, with the exception of masked off connection surfaces, field installed fasteners and damaged areas shall be touched up in the field. The color of the final finish coat shall be Reddish Brown, Munsell No. 2.5 YR 3/4.
9. The contractor shall submit for approval a detailed structure drawing prior to the layout of the structure foundations. See Check Sheet #10 in the IDOT Supplemental Specifications and Recurring Special Provisions.

**TRUSS MANUFACTURER**

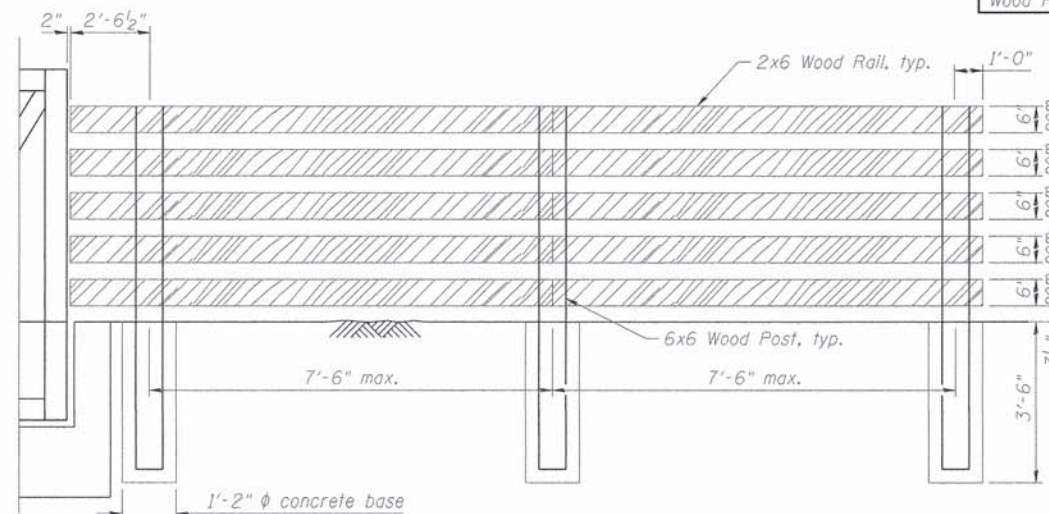
The substructure is designed per AASHTO LRFD and based on the assumed truss loads (including deck) shown below.  
 Total factored superstructure dead load at West Abutment = 110,375 pounds.  
 Total factored superstructure dead load at East Abutment = 168,313 pounds.  
 Total factored superstructure dead load at Pier 1 = 220,750 pounds.  
 Total factored superstructure dead load at Pier 2 = 278,688 pounds.  
 Truss manufacturer shall camber the truss as necessary to provide allowance for dead load deflection.  
 Bridge bearing seat elevations are subject to revision based on the approved pedestrian truss superstructure shop drawings. Contractor shall verify all dimensions and elevations with final shop drawings.  
 Truss manufacturer shall provide the reinforced concrete deck design. Concrete deck to utilize stay-in-place galvanized forms. Reinforcement shall be epoxy coated. Contractor shall place the concrete deck after truss is set. Cost included with Pedestrian Truss Superstructure.

**INDEX OF SHEETS**

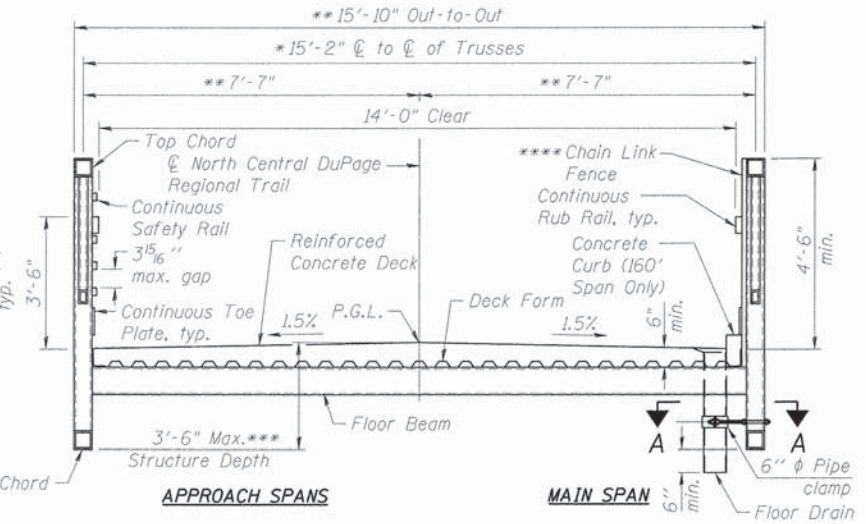
- 1 General Plan and Elevation
- 2 General Data
- 3 West Abutment
- 4 East Abutment
- 5 West Abutment GRS Wall
- 6 East Abutment GRS Wall
- 7 Pier I Details
- 8 Pier II Details
- 9-10 Boring Logs

**TOTAL BILL OF MATERIAL**

| Item                             | Unit    | Super | Sub    | Total  |
|----------------------------------|---------|-------|--------|--------|
| Structure Excavation             | Cu. Yd. |       | 460    | 460    |
| Floor Drains                     | Each    | 4     |        | 4      |
| Concrete Structures              | Cu. Yd. |       | 108.8  | 108.8  |
| Concrete Superstructure          | Cu. Yd. | 3.8   |        | 3.8    |
| Form Liner Textured Surface      | Sq. Ft. | 1,072 |        | 1,072  |
| Protective Coat                  | Sq. Yd. | 15    |        | 15     |
| Reinforcement Bars, Epoxy Coated | Pounds  |       | 16,900 | 16,900 |
| Name Plates                      | Each    | 1     |        | 1      |
| Concrete Sealer                  | Sq. Ft. |       | 1,522  | 1,522  |
| Concrete Gutter, Type B          | Foot    | 180   |        | 180    |
| Pedestrian Truss Superstructure  | Sq. Ft. | 5,491 |        | 5,491  |
| Geosynthetic Reinforced Soil     | Sq. Ft. |       | 2,759  | 2,759  |
| Reinforced Soil Foundation       | Foot    |       | 244    | 244    |
| Precast Modular Blocks           | Sq. Ft. |       | 2,759  | 2,759  |
| Wood Post And Rail Fence         | Foot    | 224   |        | 224    |

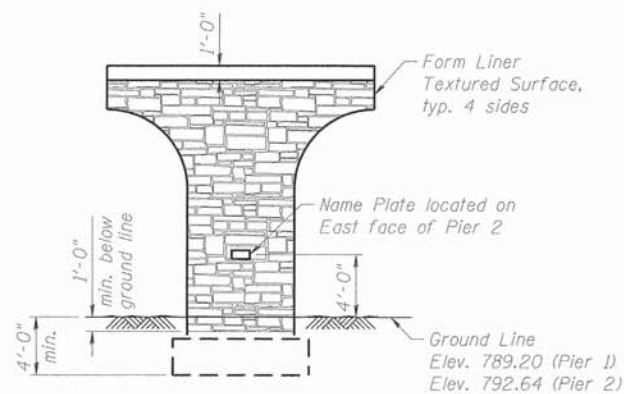


**WOOD POST AND RAIL FENCE DETAIL**



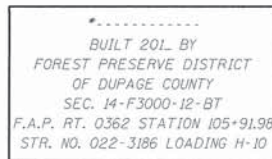
**CROSS SECTION THRU TRUSS  
14' FT. SPAN & 160' FT SPAN**

- \*\* Subject to refinement per Truss Manufacturer
- \*\*\* Structure depth does not include height of bearing. Truss supplier shall provide joint covers over all open joints between span units and at abutments (typ. 4 locations).
- \*\*\*\* Fence to be Black Vinyl Coated per Section 1006.27(a)(1) of the Standard Specifications. Cost of Chain Link Fence included with Pedestrian Truss Superstructure.



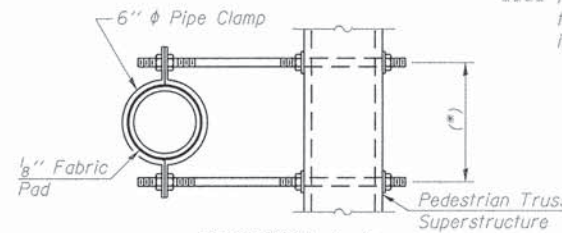
**PIER SKETCH**

Note: Form Liner Textured Surface shall meet the requirements of Custom Rock pattern #2014 Bearpath Coursed Stone, Fitzgerald pattern #17067 Kentucky Drystack or Milestones pattern #MS-1019 Small Ashlar or approved equal (See Special Provision).



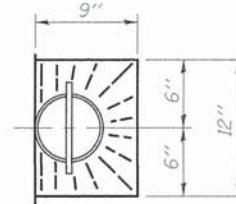
**NAME PLATE**

\* Name of bridge to be furnished by the Forest Preserve District of DuPage County.

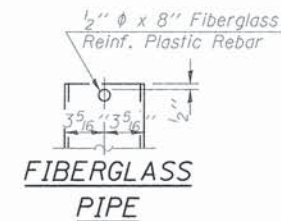


**SECTION A-A**

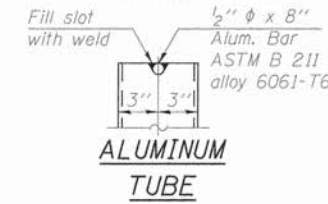
(\*Connection to be provided by truss supplier)



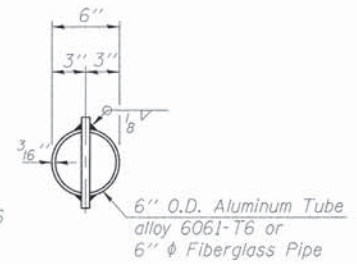
**TOP PLAN**



**FIBERGLASS PIPE**



**ALUMINUM TUBE**



**TOP PLAN (Showing Aluminum Tube)**

**FLOOR DRAIN DETAILS**

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GENERAL DATA  
STRUCTURE NO. 022-3186**

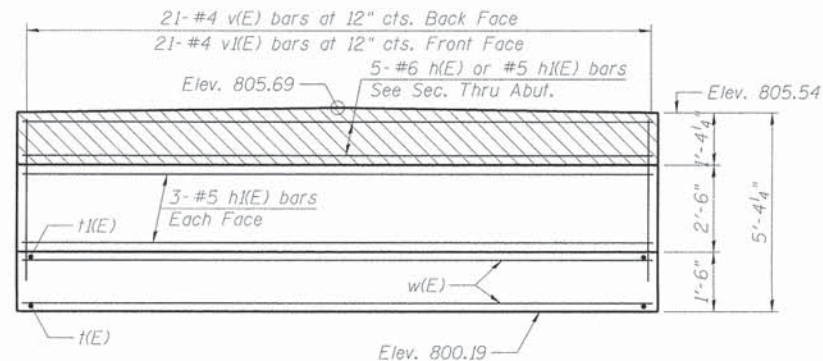
SHEET NO. 2 OF 10 SHEETS

| F.A.P. | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------|----------------|--------|--------------|-----------|
| 0362   | 14-F3000-12-BT | DUPAGE | 69           | 27        |

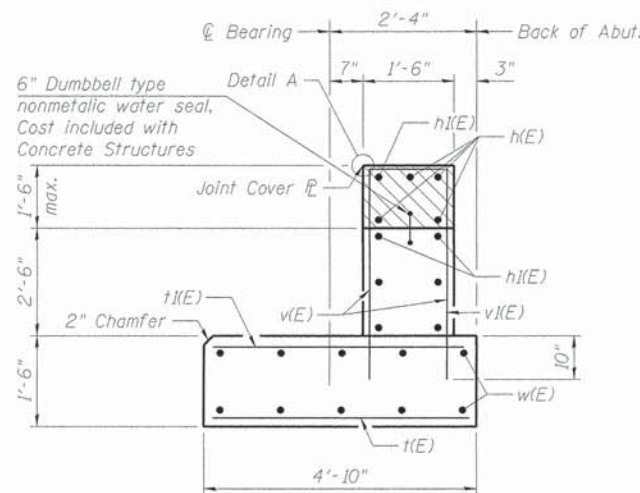
CONTRACT NO 61C31  
[ILLINOIS] FED. AID PROJECT

| USER NAME    | DESIGNED | CHECKED | DRAWN | DATE       |
|--------------|----------|---------|-------|------------|
| bshoeffliger | JNP      | JRM     | JNP   | 12/16/2015 |

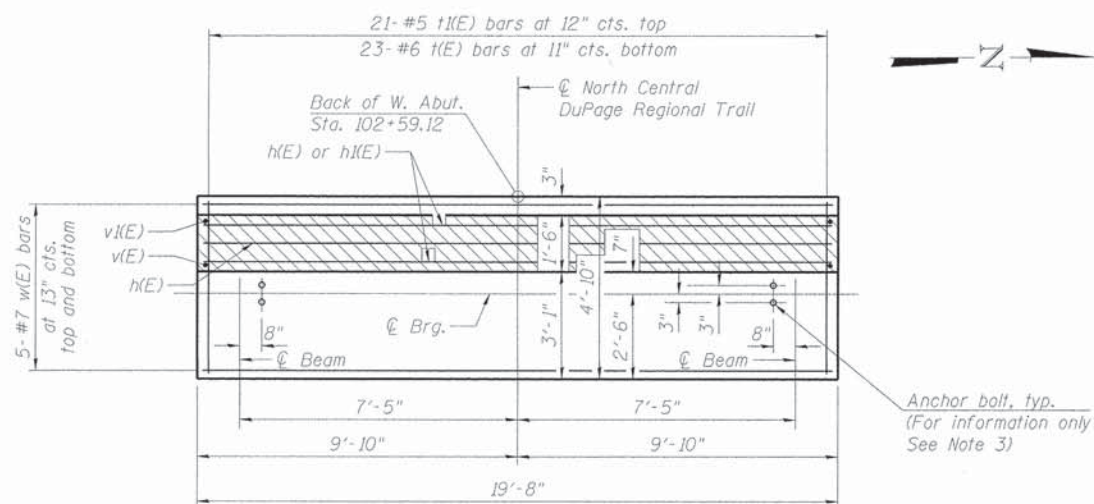




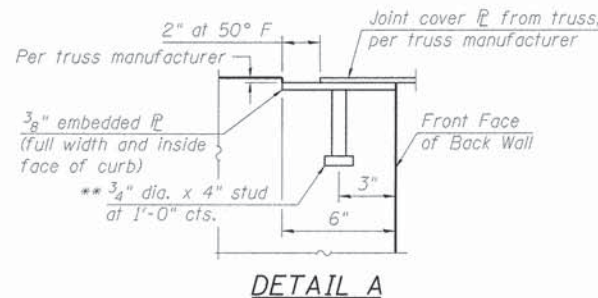
**ELEVATION**



**SECTION THRU ABUTMENT**



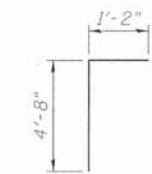
**PLAN**



**DETAIL A**

**BILL OF MATERIAL**

| Bar                              | No. | Size | Length  | Shape |
|----------------------------------|-----|------|---------|-------|
| h(E)                             | 4   | #6   | 19'-4"  | —     |
| h(E)                             | 7   | #5   | 19'-4"  | —     |
| t(E)                             | 23  | #6   | 4'-6"   | —     |
| t(E)                             | 21  | #5   | 4'-6"   | —     |
| v(E)                             | 21  | #4   | 5'-10"  | L     |
| v(E)                             | 21  | #4   | 4'-8"   | —     |
| w(E)                             | 10  | #7   | 19'-4"  | —     |
| Concrete Structures              |     |      | Cu. Yd. | 8.0   |
| Concrete Superstructure          |     |      | Cu. Yd. | 1.6   |
| Protective Coat                  |     |      | Sq. Yd. | 7     |
| Reinforcement Bars, Epoxy Coated |     |      | Pound   | 1,060 |
| Concrete Sealer                  |     |      | Sq. Ft. | 110   |



**BAR v(E)**

**MINIMUM BAR LAPS**

| Bar | Lap    |
|-----|--------|
| #5  | 3'-3"  |
| #6  | 3'-10" |
| #7  | 5'-2"  |

**Notes:**  
 Hatched area to be poured after superstructure false work has been removed. Quantity of concrete included with Concrete Superstructure. Concrete clear cover shall be 2" minimum unless noted otherwise. Truss fabricator shall determine size and location of anchor bolts. Cost included with Pedestrian Truss Superstructure. All edges shall have standard 3/4" chamfers except as noted. Space reinforcement in cap to miss anchor bolts. Concrete Sealer shall be applied to the beam seat and front face of backwall and stem. Truss fabricator shall provide a 1" nominal shim pack for each bearing consisting of a 1/2", 1/4" and 2-1/8" shim plates matching the dimensions of the masonry plate provided, including holes for anchor bolts. Cost included in the cost of Pedestrian Truss Superstructure.

4:57:15 PM xxxxxx-03-West-Abutment.dgn



|                                  |              |         |
|----------------------------------|--------------|---------|
| USER NAME = bshaeffler           | DESIGNED JNP | REVISED |
| FLAT SCALE = 0:1,0000 '1" / 1in. | CHECKED JRM  | REVISED |
| FLAT DATE = 12/16/2015           | DRAWN JNP    | REVISED |
|                                  | CHECKED JRM  | REVISED |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

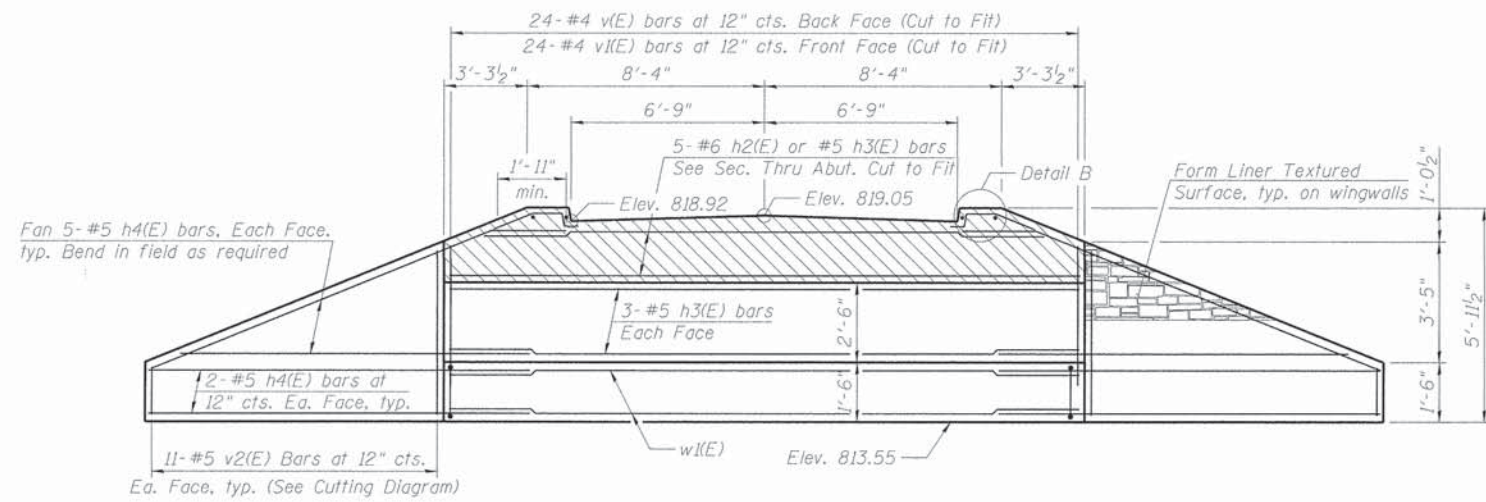
**WEST ABUTMENT  
STRUCTURE NO. 022-3186**

SHEET NO. 3 OF 10 SHEETS

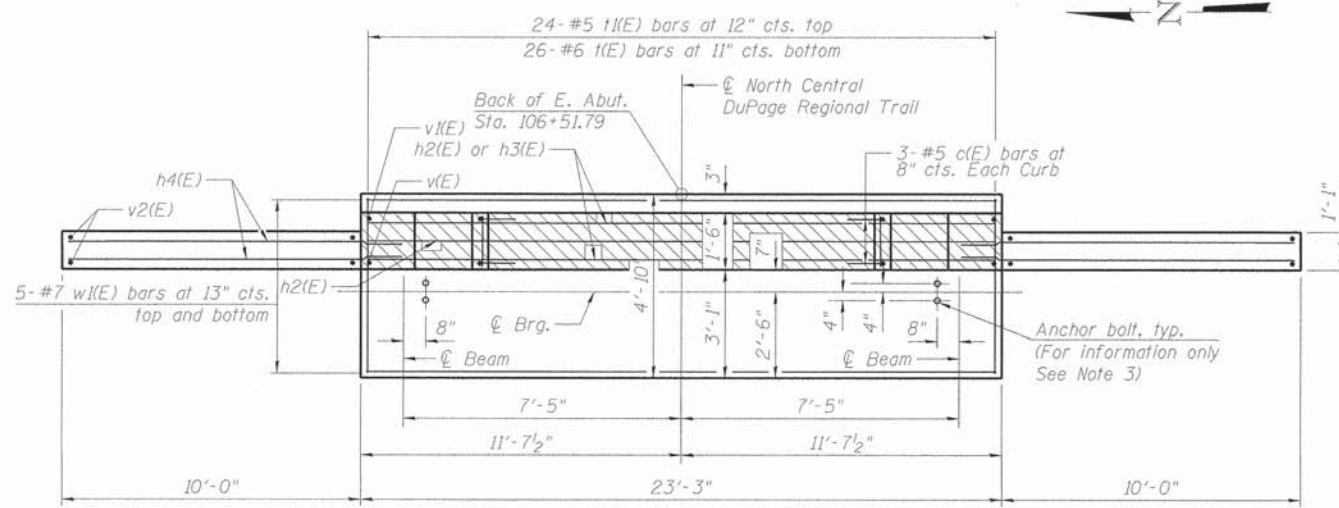
| F.A.P.            | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------------|----------------|--------|--------------|-----------|
| 0362              | 14-F3000-12-BT | DUPAGE | 69           | 28        |
| CONTRACT NO 61C31 |                |        |              |           |

ILLINOIS FED. AID PROJECT

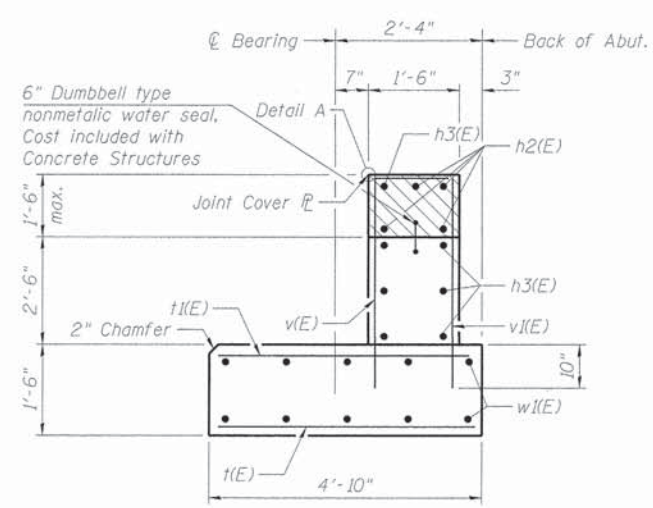




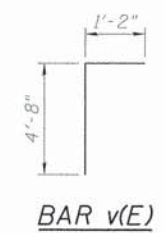
**ELEVATION**



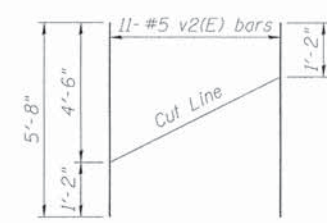
**PLAN**



**SECTION THRU ABUTMENT**

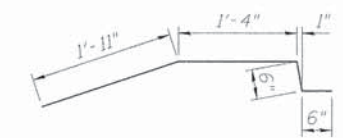


**BAR v(E)**

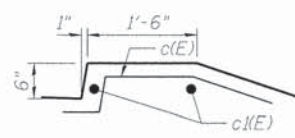


**BAR v2(E)**

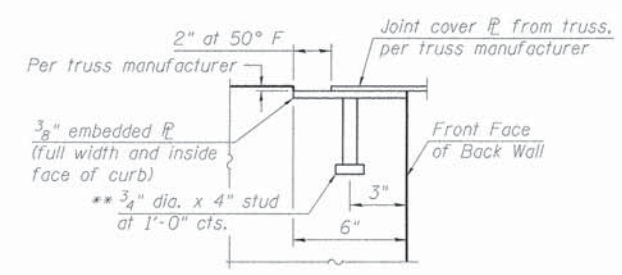
Order v2(E) full length. Cut as shown and use remainder of bars in opposite face.



**BAR c(E)**



**DETAIL B**



**DETAIL A**

**Notes:**  
 Hatched area to be poured after superstructure false work has been removed. Quantity of concrete included with Concrete Superstructure.  
 Concrete clear cover shall be 2" minimum unless noted otherwise.  
 Truss fabricator shall determine size and location of anchor bolts. Cost included with Pedestrian Truss Superstructure.  
 All edges shall have standard 3/4" chamfers except as noted.  
 Space reinforcement in cap to miss anchor bolts.  
 Concrete Sealer shall be applied to the beam seat and front face of backwall and stem.  
 Truss fabricator shall provide a 1" nominal shim pack for each bearing consisting of a 1/2", 1/4" and 2-1/8" shim plates matching the dimensions of the masonry plate provided, including holes for anchor bolts. Cost included in the cost of Pedestrian Truss Superstructure.  
 Form Liner Textured Surface shall be applied to the exposed front face of the wingwalls.

**BILL OF MATERIAL**

| Bar                              | No. | Size | Length  | Shape |
|----------------------------------|-----|------|---------|-------|
| c(E)                             | 6   | #5   | 4'-3"   |       |
| v(E)                             | 4   | #5   | 1'-2"   |       |
| h2(E)                            | 4   | #6   | 22'-11" |       |
| h3(E)                            | 7   | #5   | 22'-11" |       |
| h4(E)                            | 28  | #5   | 14'-6"  |       |
| t(E)                             | 26  | #6   | 4'-6"   |       |
| t1(E)                            | 24  | #5   | 4'-6"   |       |
| v2(E)                            | 24  | #4   | 5'-10"  |       |
| v(E)                             | 24  | #4   | 4'-8"   |       |
| v2(E)                            | 22  | #5   | 5'-8"   |       |
| w(E)                             | 10  | #7   | 22'-11" |       |
| Concrete Structures              |     |      | Cu. Yd. | 12.0  |
| Concrete Superstructure          |     |      | Cu. Yd. | 2.3   |
| Form Liner Textured Surface      |     |      | Sq. Ft. | 63    |
| Protective Coat                  |     |      | Sq. Yd. | 8     |
| Reinforcement Bars, Epoxy Coated |     |      | Pound   | 1,790 |
| Concrete Sealer                  |     |      | Sq. Ft. | 194   |

**MINIMUM BAR LAPS**

| Bar | Lap    |
|-----|--------|
| #5  | 3'-3"  |
| #6  | 3'-10" |
| #7  | 5'-2"  |

4:57:15 PM XXXXX-01-East-Abutment.dgn



|                                |              |         |
|--------------------------------|--------------|---------|
| USER NAME = bshaeffger         | DESIGNED JNP | REVISED |
| PLOT SCALE = 0:1,0000 1" = 10' | CHECKED JRM  | REVISED |
| PLOT DATE = 12/16/2015         | DRAWN JNP    | REVISED |
|                                | CHECKED JRM  | REVISED |

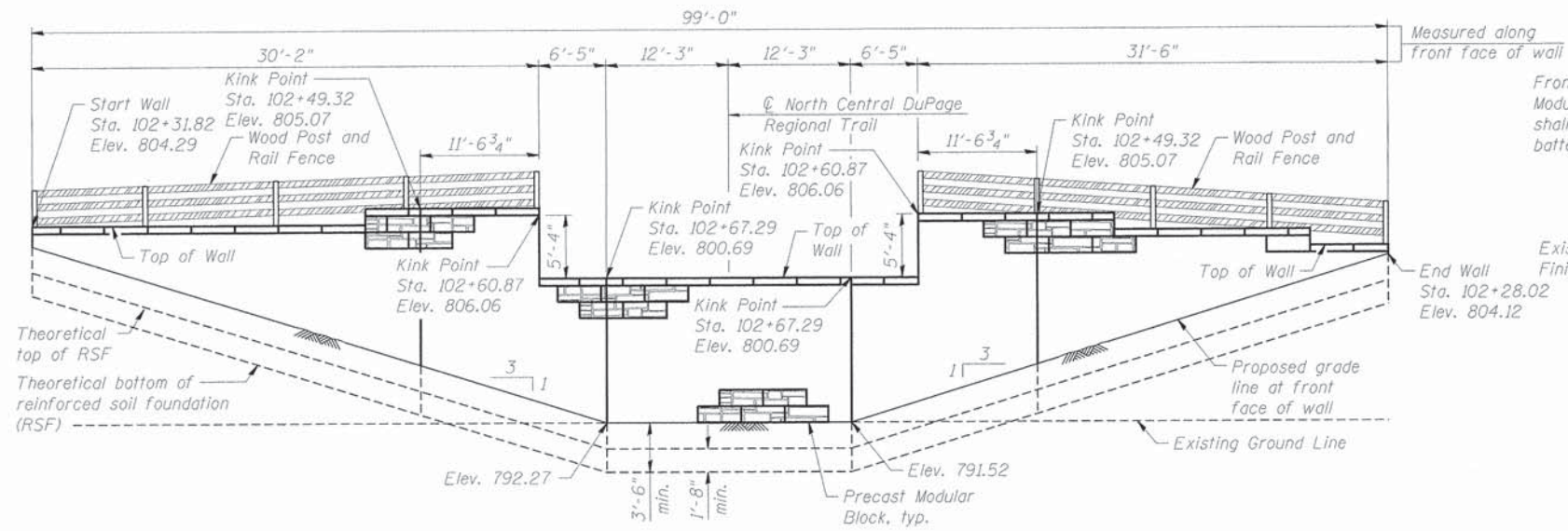
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EAST ABUTMENT  
STRUCTURE NO. 022-3186**  
SHEET NO. 4 OF 10 SHEETS

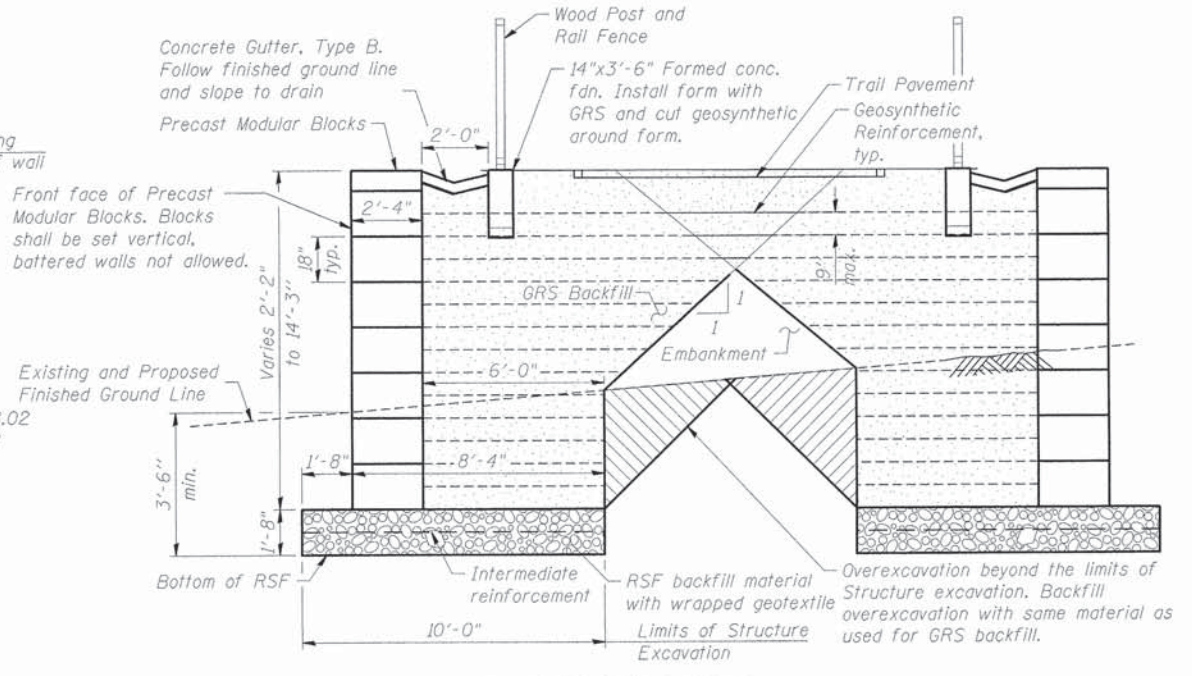
| F.A.P.            | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------------|----------------|--------|--------------|-----------|
| 0362              | 14-F3000-12-BT | DUPAGE | 69           | 29        |
| CONTRACT NO 61C31 |                |        |              |           |

ILLINOIS FED. AID PROJECT





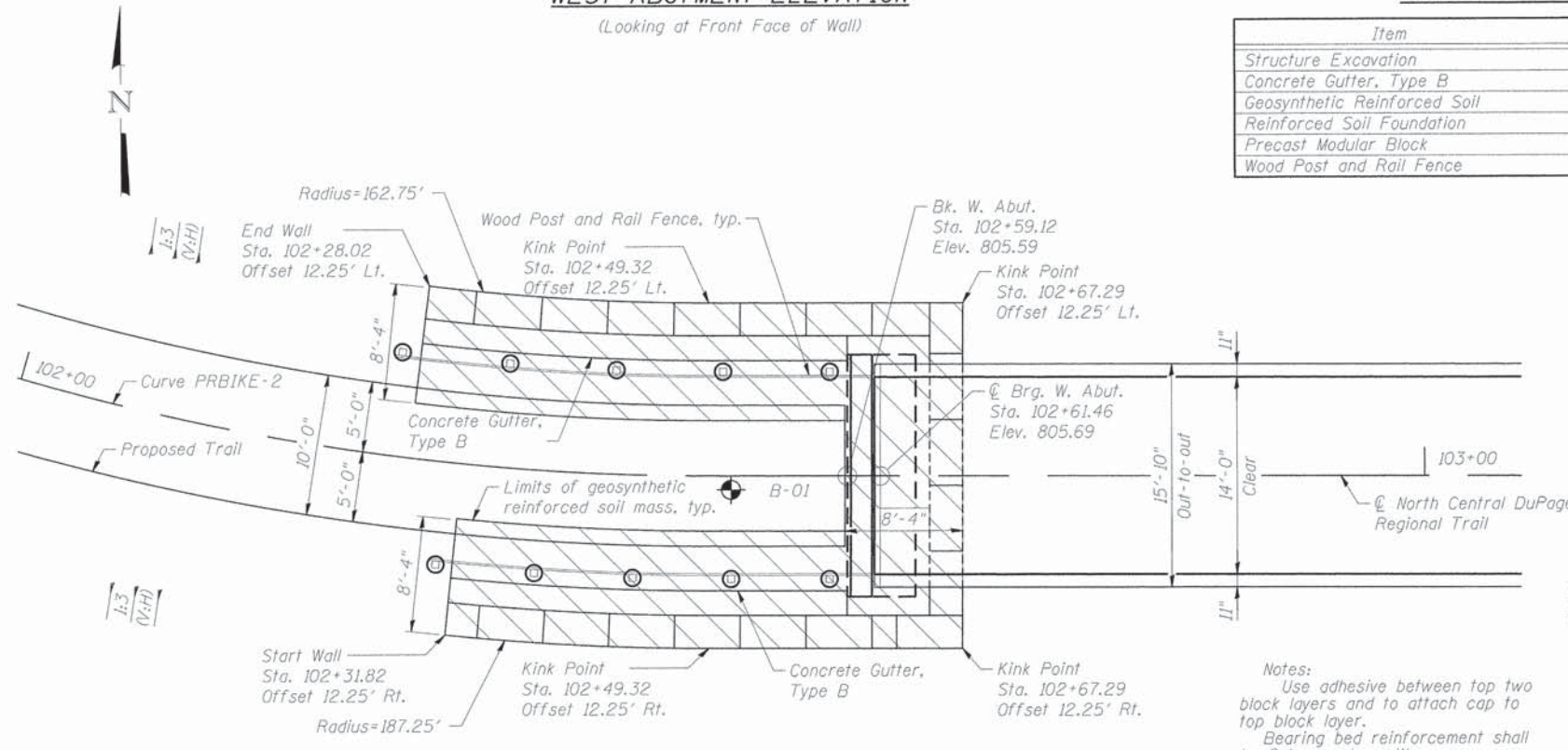
**WEST ABUTMENT ELEVATION**  
(Looking at Front Face of Wall)



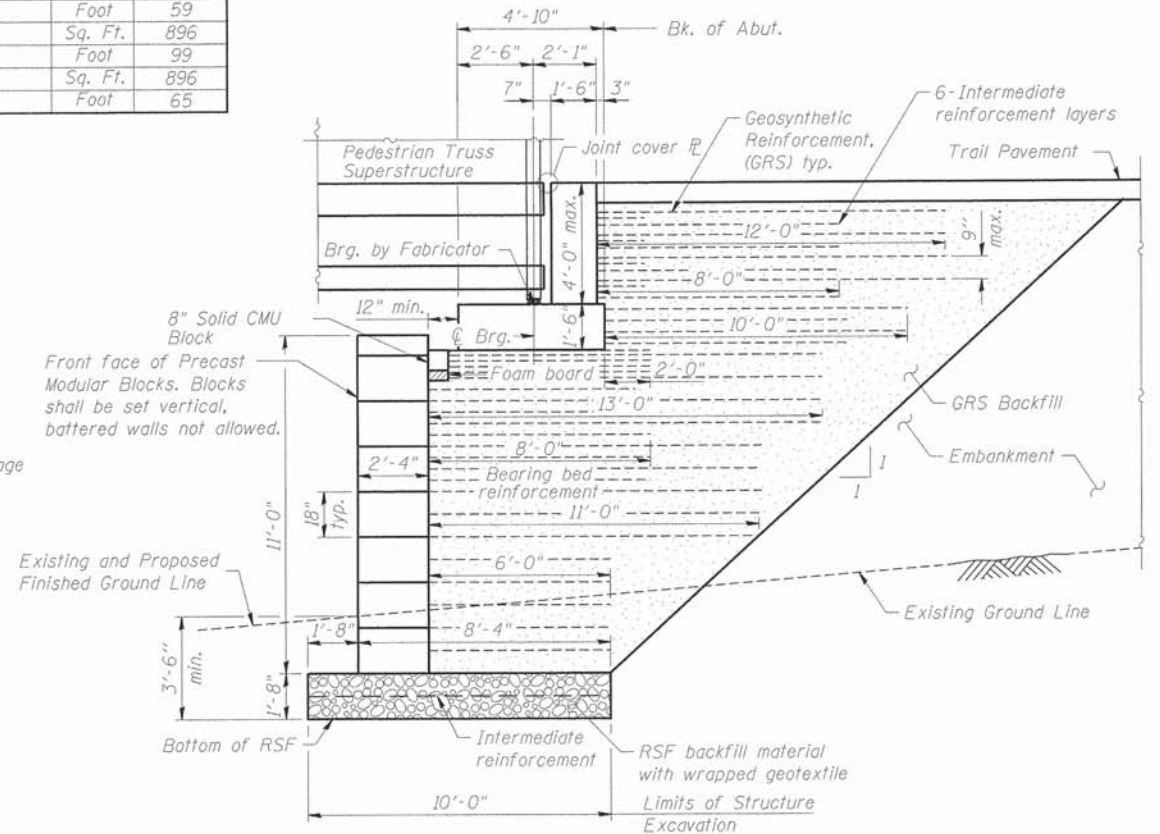
**SECTION THRU GRS RETAINING WALL**

**BILL OF MATERIAL**

| Item                         | Unit    | Quantity |
|------------------------------|---------|----------|
| Structure Excavation         | Cu. Yd. | 107      |
| Concrete Gutter, Type B      | Foot    | 59       |
| Geosynthetic Reinforced Soil | Sq. Ft. | 896      |
| Reinforced Soil Foundation   | Foot    | 99       |
| Precast Modular Block        | Sq. Ft. | 896      |
| Wood Post and Rail Fence     | Foot    | 65       |



**PLAN**



**SECTION THRU GRS ABUTMENT**

Notes:  
Use adhesive between top two block layers and to attach cap to top block layer.  
Bearing bed reinforcement shall be 8 layers deep. Wrap reinforcement behind CMU block and foam board.  
Stations and offsets are along the front face of precast modular blocks.  
See Sheet 3 of 10 for Joint Cover Plate detail.

457116 PM 3/11/16 West Abutment GRS Wall.dgn



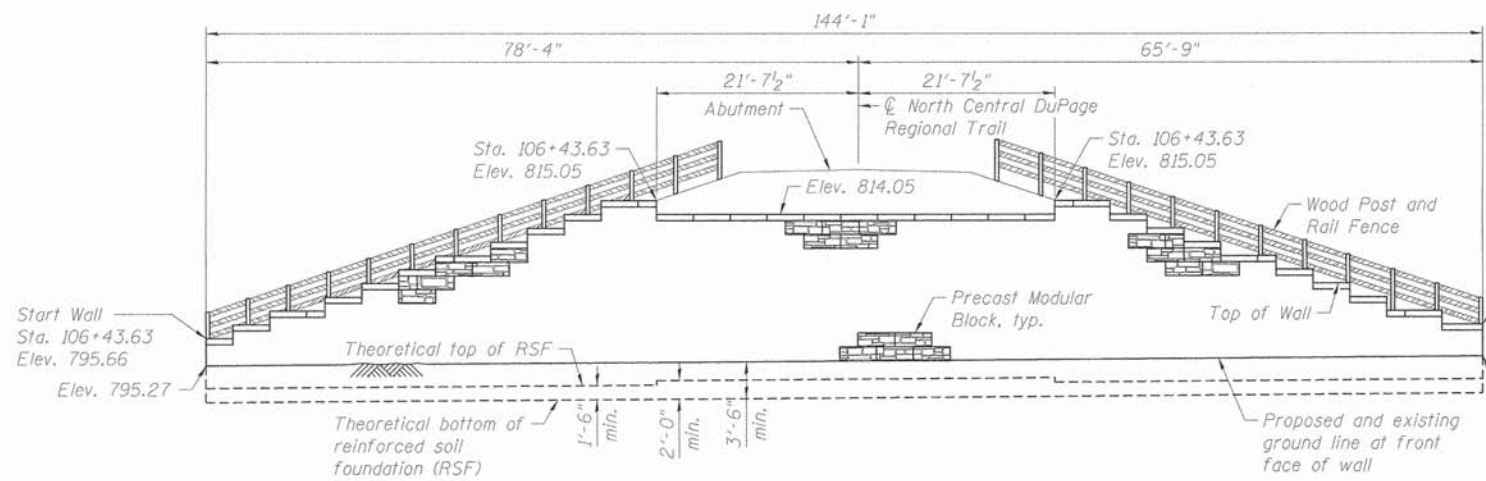
|                                  |              |         |
|----------------------------------|--------------|---------|
| USER NAME = bshoeflger           | DESIGNED JNP | REVISED |
| PLOT SCALE = 6x0.0000 'in' / in. | CHECKED JRM  | REVISED |
| PLOT DATE = 12/16/2015           | DRAWN JNP    | REVISED |
|                                  | CHECKED JRM  | REVISED |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WEST ABUTMENT GRS WALL**  
**STRUCTURE NO. 022-3186**  
SHEET NO. 5 OF 10 SHEETS

|                           |                |        |              |           |
|---------------------------|----------------|--------|--------------|-----------|
| F.A.P.                    | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 0362                      | 14-F3000-12-BT | DUPAGE | 69           | 30        |
| CONTRACT NO 61C31         |                |        |              |           |
| ILLINOIS FED. AID PROJECT |                |        |              |           |



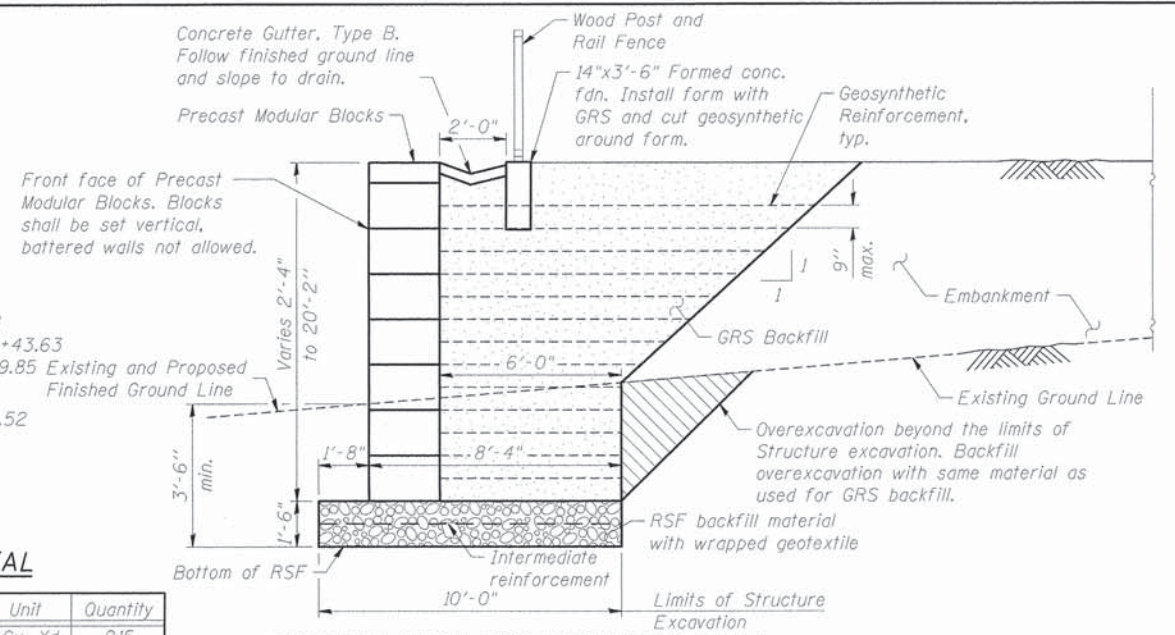


**EAST ABUTMENT ELEVATION**  
(Looking at Front Face of Wall)

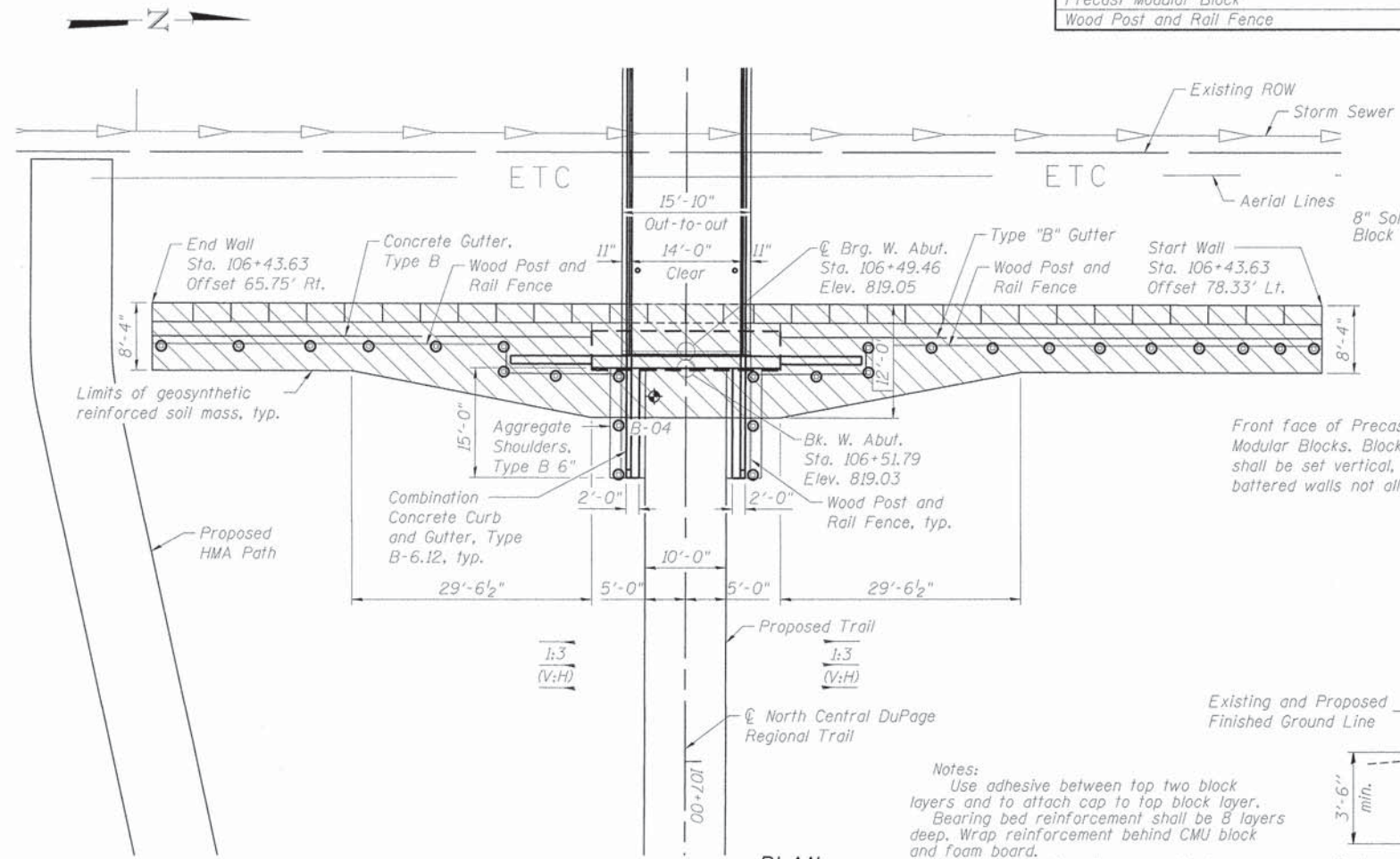
**NOTE:**  
Stations and offsets are along the front face of precast modular blocks.

**BILL OF MATERIAL**

| Item                         | Unit    | Quantity |
|------------------------------|---------|----------|
| Structure Excavation         | Cu. Yd. | 215      |
| Concrete Gutter, Type B      | Foot    | 121      |
| Geosynthetic Reinforced Soil | Sq. Ft. | 1,863    |
| Reinforced Soil Foundation   | Foot    | 145      |
| Precast Modular Block        | Sq. Ft. | 1,863    |
| Wood Post and Rail Fence     | Foot    | 159      |

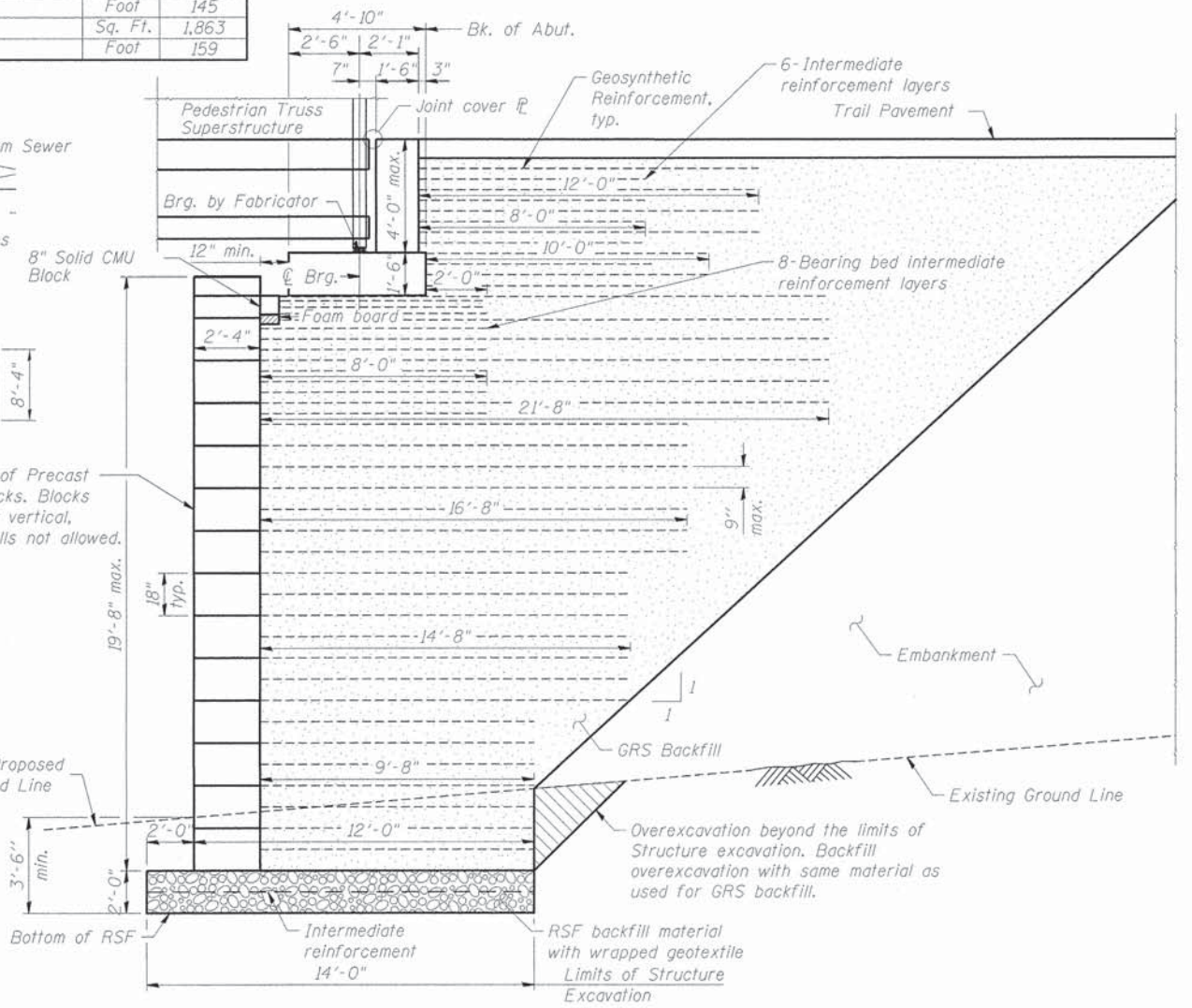


**SECTION THRU GRS RETAINING WALL**



**PLAN**

**Notes:**  
Use adhesive between top two block layers and to attach cap to top block layer. Bearing bed reinforcement shall be 8 layers deep. Wrap reinforcement behind CMU block and foam board.  
See Roadway plans for payment of Aggregate Shoulders, Type B 6" and Combination Concrete Curb and Gutter, Type B-6.12.  
See Sheet 4 of 10 for Joint Cover Plate detail.



**SECTION THRU GRS ABUTMENT**

4:57:17 PM XXXXX-06-East Abutment GRS Wall.dgn



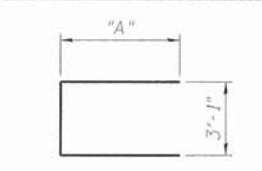
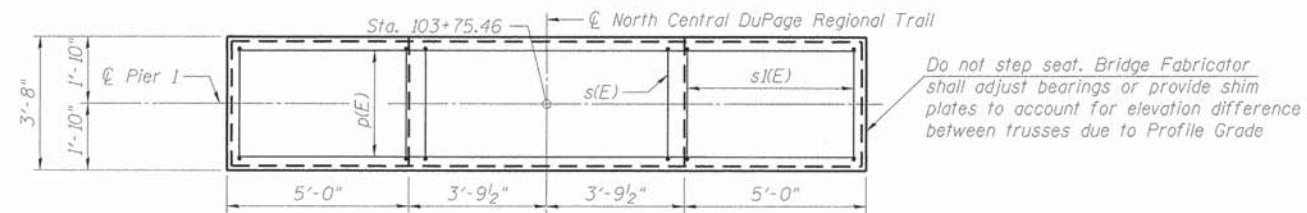
|                         |              |         |
|-------------------------|--------------|---------|
| USER NAME = bshoefliger | DESIGNED JNP | REVISED |
| FLDT SCALE = 10:0 "/>   |              |         |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**EAST ABUTMENT GRS WALL**  
**STRUCTURE NO. 022-3186**  
SHEET NO. 6 OF 10 SHEETS

|                           |                |        |              |           |
|---------------------------|----------------|--------|--------------|-----------|
| F.A.P.                    | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 0362                      | 14-F3000-12-BT | DUPAGE | 69           | 31        |
| CONTRACT NO 61C31         |                |        |              |           |
| ILLINOIS FED. AID PROJECT |                |        |              |           |



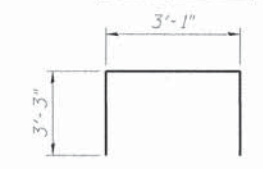
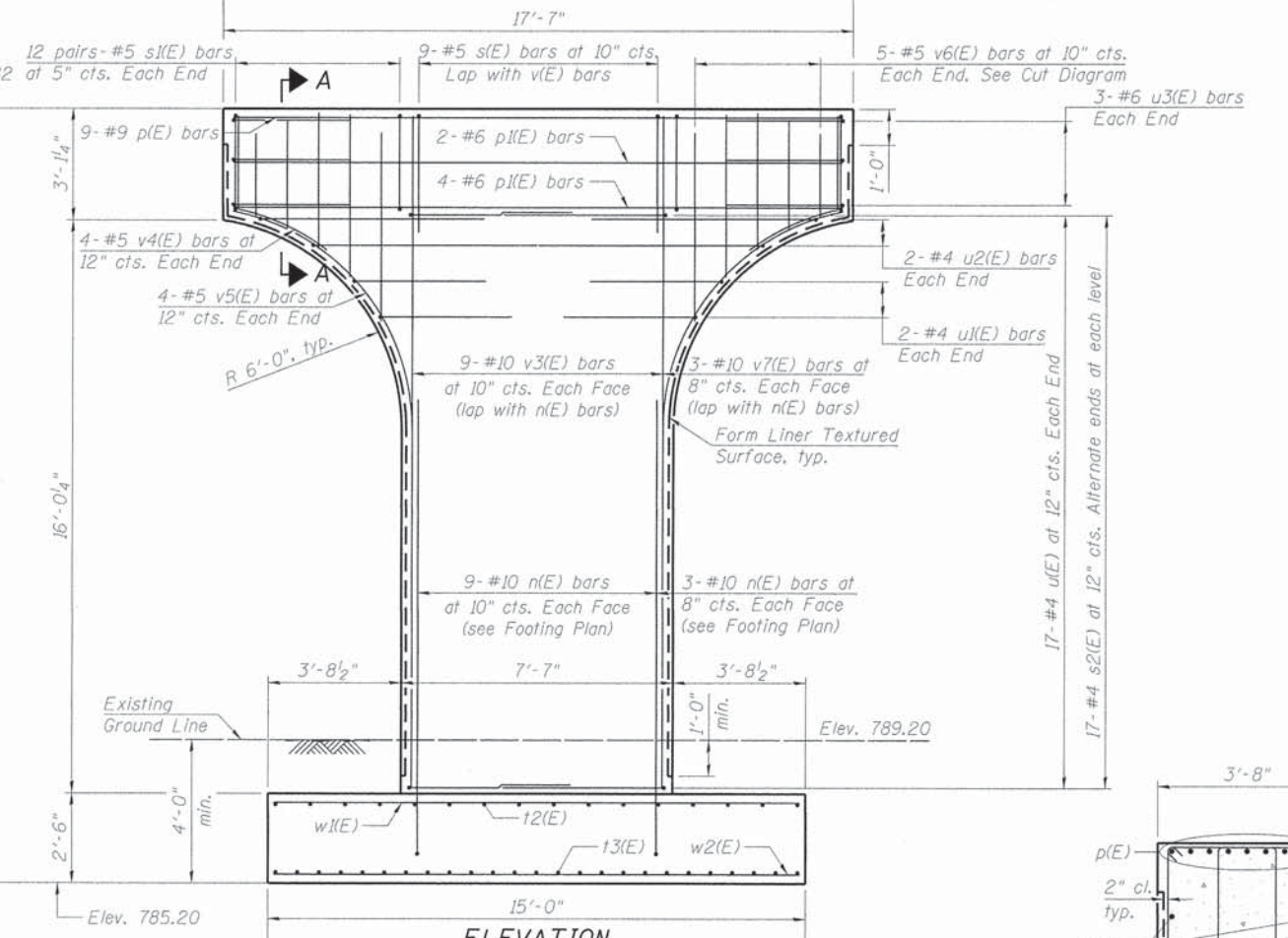
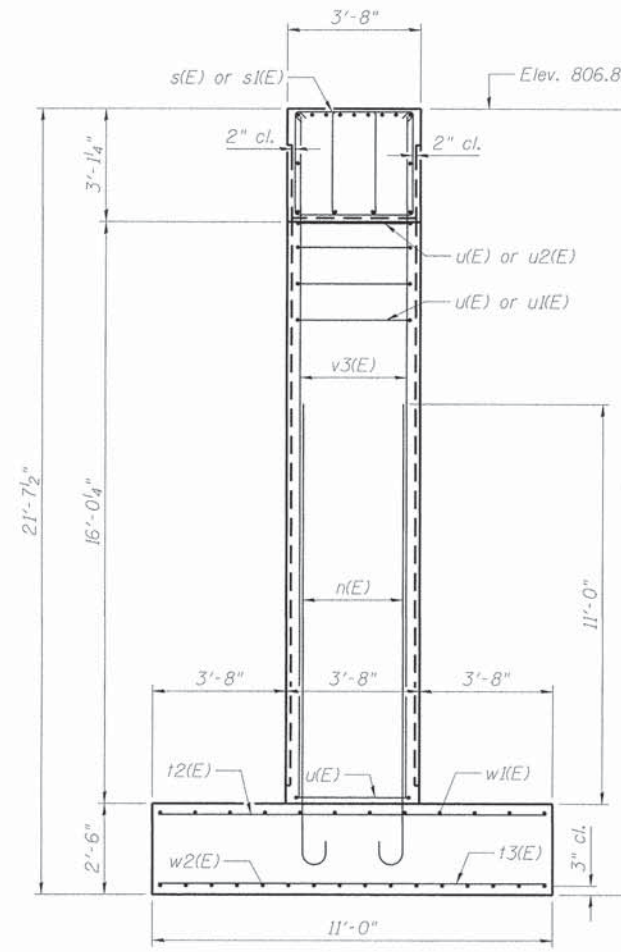


**BARS u(E), u1(E), u2(E), & u3(E)**

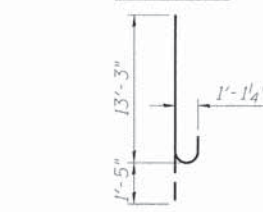
| Bar   | "A"    |
|-------|--------|
| u(E)  | 5'-4"  |
| u1(E) | 3'-8"  |
| u2(E) | 6'-3"  |
| u3(E) | 3'-10" |

**BILL OF MATERIAL**

| Bar                              | No. | Size    | Length | Shape |
|----------------------------------|-----|---------|--------|-------|
| n(E)                             | 24  | #10     | 14'-8" |       |
| p(E)                             | 9   | #9      | 17'-0" |       |
| p1(E)                            | 6   | #6      | 17'-0" |       |
| s(E)                             | 9   | #5      | 9'-7"  |       |
| s1(E)                            | 48  | #5      | 10'-7" |       |
| s2(E)                            | 17  | #4      | 3'-10" |       |
| 12(E)                            | 16  | #5      | 10'-8" |       |
| 13(E)                            | 25  | #6      | 10'-8" |       |
| u(E)                             | 34  | #4      | 13'-9" |       |
| u1(E)                            | 4   | #4      | 10'-5" |       |
| u2(E)                            | 4   | #4      | 15'-7" |       |
| u3(E)                            | 6   | #6      | 10'-9" |       |
| v3(E)                            | 24  | #10     | 18'-9" |       |
| v4(E)                            | 8   | #5      | 5'-11" |       |
| v5(E)                            | 8   | #5      | 11'-9" |       |
| v6(E)                            | 10  | #5      | 7'-7"  |       |
| w1(E)                            | 12  | #5      | 14'-8" |       |
| w2(E)                            | 16  | #6      | 14'-8" |       |
| Structure Excavation             |     | Cu. Yd. | 65     |       |
| Concrete Structures              |     | Cu. Yd. | 41.3   |       |
| Form Liner Textured Surface      |     | Sq. Ft. | 487    |       |
| Reinforcement Bars, Epoxy Coated |     | Pound   | 6,610  |       |
| Concrete Sealer                  |     | Sq. Ft. | 578    |       |



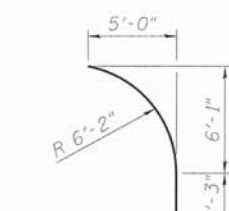
**BAR s(E)**



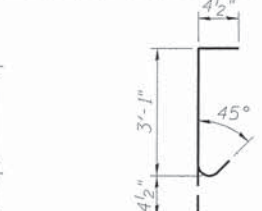
**BAR n(E)**



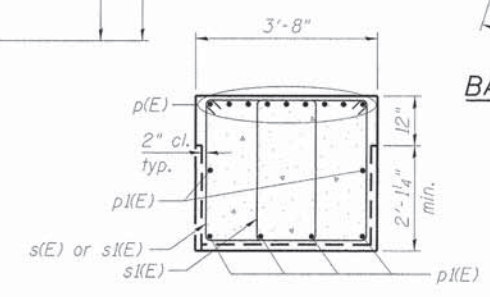
**BAR v4(E)**



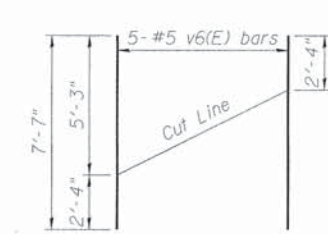
**BAR v5(E)**



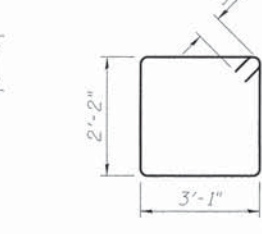
**BAR s2(E)**



**SECTION A-A**



**BAR v6(E)**



**BAR s1(E)**

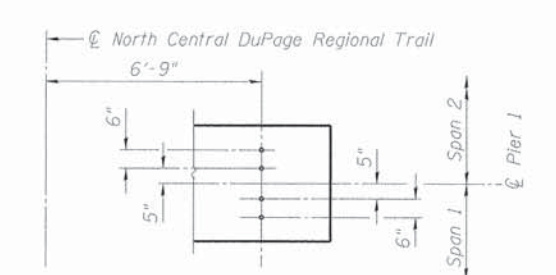
Order v6(E) full length. Cut as shown and use remainder of bars in opposite face.

**Notes:**

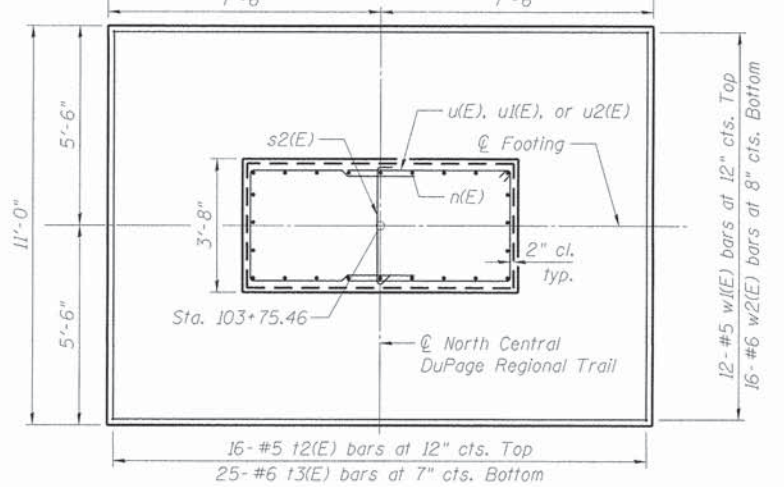
Concrete clear cover shall be 2" minimum unless noted otherwise. Truss fabricator shall determine size and location of anchor bolts. Cost included with Pedestrian Truss Superstructure. All edges shall have standard 3/4" chamfers except as noted. Space reinforcement in cap to miss anchor bolts. Concrete Sealer shall be applied to all exposed surfaces of the pier. Truss fabricator shall provide a 1" nominal shim pack for each bearing consisting of a 1/2", 1/4" and 2-1/8" shim plates matching the dimensions of the masonry plate provided, including holes for anchor bolts. Cost included in the cost of Pedestrian Truss Superstructure.

**MINIMUM BAR LAPS**

| Bar | Lap     |
|-----|---------|
| #4  | 2'-7"   |
| #5  | 3'-3"   |
| #10 | 10'-10" |



**ANCHOR BOLT LAYOUT**  
(For information only. See Note 2)



**FOOTING PLAN**

4:57:19 PM xxxxxx-06-Pier 1 Details.dgn



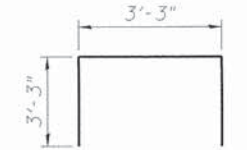
Sta. 104+89.46 — North Central DuPage Regional Trail

Do not step seat. Bridge Fabricator shall adjust bearings or provide shim plates to account for elevation difference between trusses due to Profile Grade

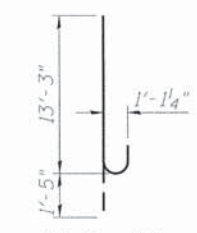


**BARS u4(E), u5(E), u6(E), & u7(E)**

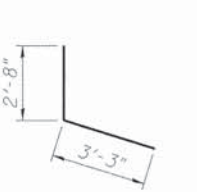
| Bar   | "A"    |
|-------|--------|
| u4(E) | 5'-4"  |
| u5(E) | 3'-8"  |
| u6(E) | 6'-3"  |
| u7(E) | 3'-10" |



**BAR s3(E)**



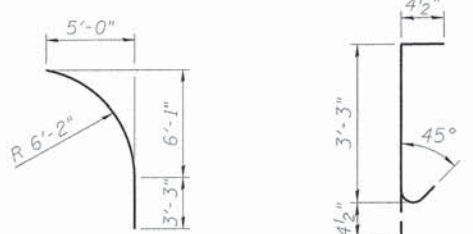
**BAR n(E)**



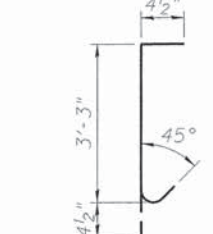
**BAR v4(E)**

**BILL OF MATERIAL**

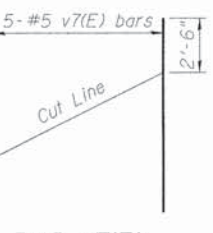
| Bar                              | No. | Size    | Length  | Shape |
|----------------------------------|-----|---------|---------|-------|
| n(E)                             | 24  | #10     | 14'-8"  | U     |
| p2(E)                            | 12  | #9      | 17'-2"  | —     |
| p3(E)                            | 6   | #6      | 17'-2"  | —     |
| s3(E)                            | 9   | #5      | 9'-9"   | □     |
| s4(E)                            | 40  | #5      | 10'-11" | □     |
| s5(E)                            | 18  | #4      | 4'-0"   | U     |
| t2(E)                            | 19  | #5      | 10'-8"  | —     |
| t3(E)                            | 31  | #6      | 10'-8"  | —     |
| u4(E)                            | 36  | #4      | 13'-11" | —     |
| u5(E)                            | 4   | #4      | 10'-7"  | —     |
| u6(E)                            | 4   | #4      | 15'-9"  | —     |
| u7(E)                            | 6   | #6      | 10'-11" | —     |
| v4(E)                            | 8   | #5      | 5'-11"  | —     |
| v5(E)                            | 8   | #5      | 11'-9"  | —     |
| v6(E)                            | 24  | #10     | 20'-6"  | —     |
| v7(E)                            | 10  | #5      | 7'-9"   | —     |
| w3(E)                            | 12  | #5      | 17'-8"  | —     |
| w4(E)                            | 26  | #6      | 17'-8"  | —     |
| Structure Excavation             |     | Cu. Yd. | 73      |       |
| Concrete Structures              |     | Cu. Yd. | 47.5    |       |
| Form Liner Textured Surface      |     | Sq. Ft. | 522     |       |
| Reinforcement Bars, Epoxy Coated |     | Pound   | 7,440   |       |
| Concrete Sealer                  |     | Sq. Ft. | 640     |       |



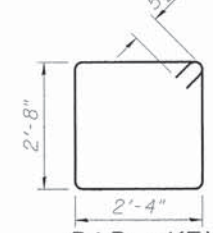
**BAR v5(E)**



**BAR s5(E)**



**BAR v7(E)**



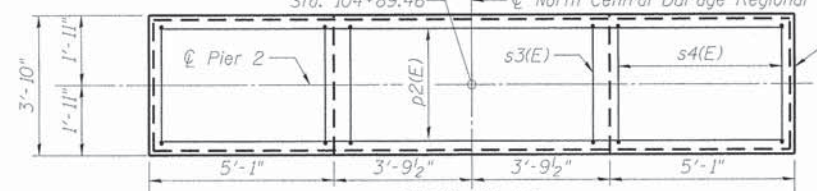
**BAR s4(E)**

Order v7(E) full length. Cut as shown and use remainder of bars in opposite face.

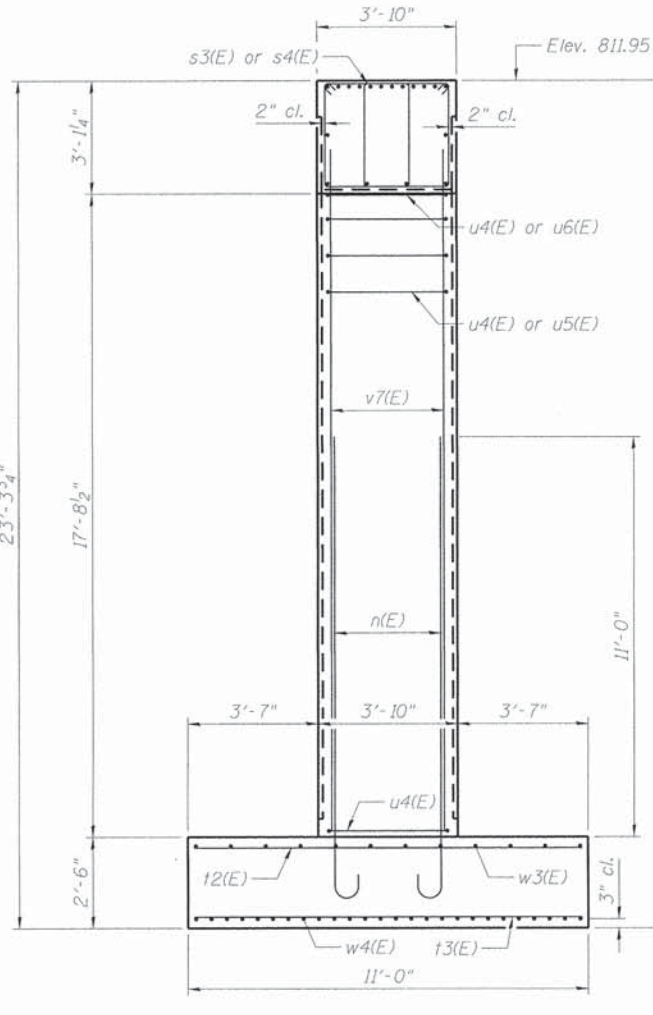
**Notes:**  
 Concrete clear cover shall be 2" minimum unless noted otherwise.  
 Truss fabricator shall determine size and location of anchor bolts. Cost included with Pedestrian Truss Superstructure.  
 All edges shall have standard 3/4" chamfers except as noted.  
 Space reinforcement in cap to miss anchor bolts.  
 Concrete Sealer shall be applied to all exposed surfaces of the pier.  
 Truss fabricator shall provide a 1" nominal shim pack for each bearing consisting of a 1/2", 1/4" and 2-1/8" shim plates matching the dimensions of the masonry plate provided, including holes for anchor bolts. Cost included in the cost of Pedestrian Truss Superstructure.

**MINIMUM BAR LAPS**

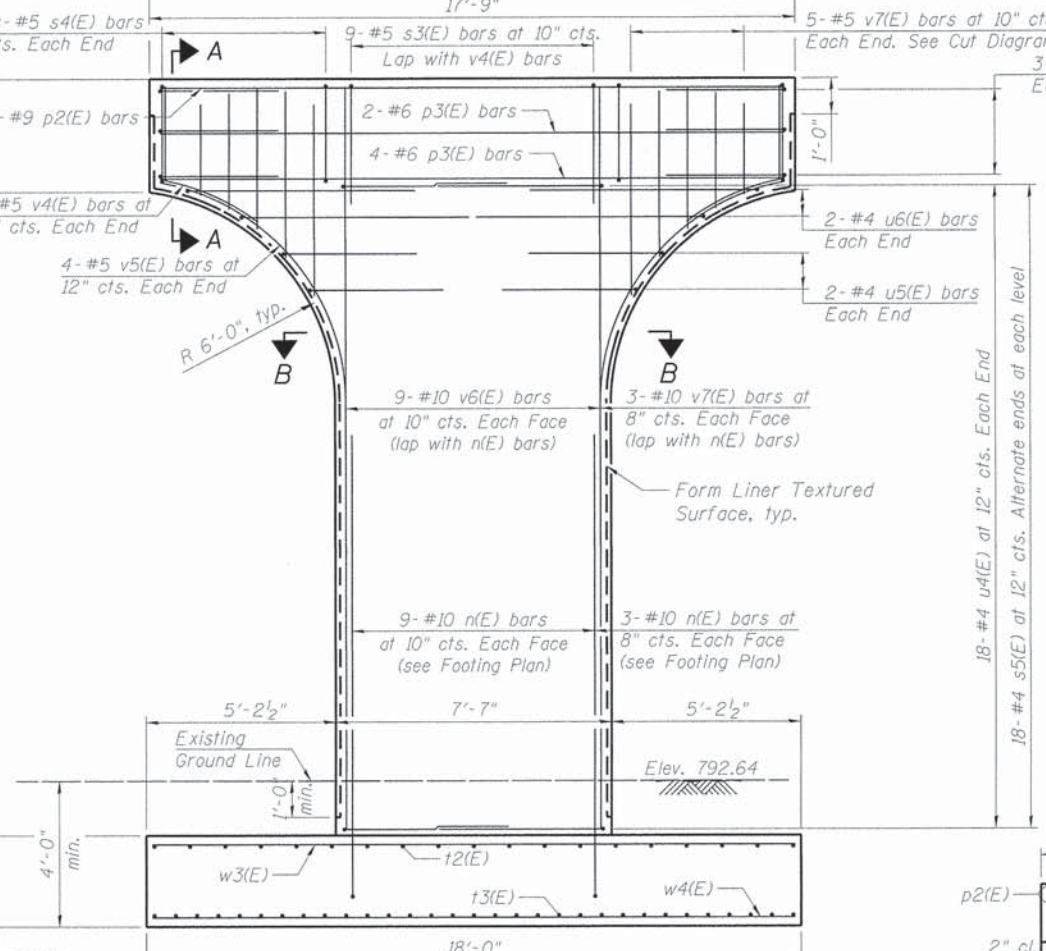
| Bar | Lap     |
|-----|---------|
| #4  | 2'-7"   |
| #5  | 3'-3"   |
| #10 | 10'-10" |



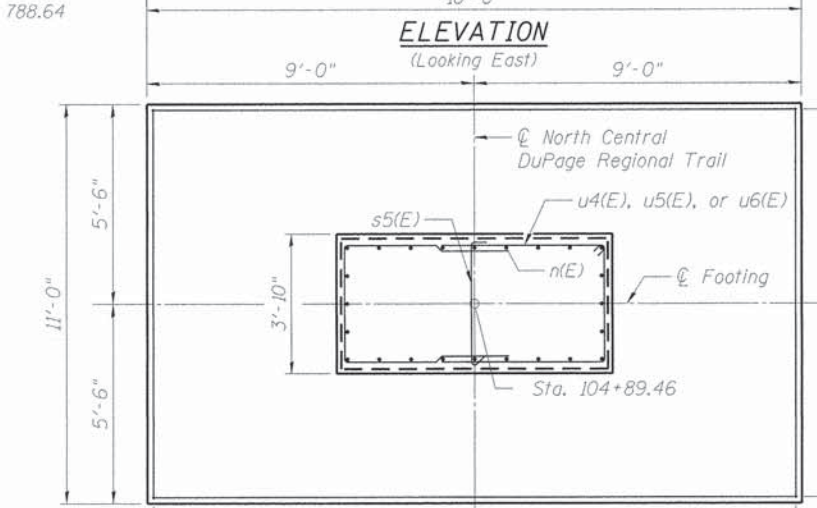
**TOP PLAN**



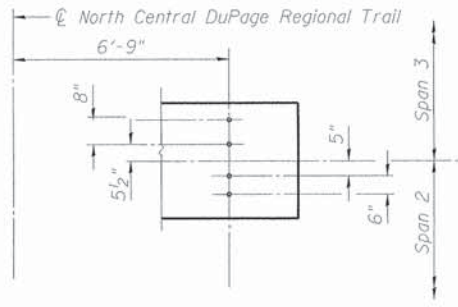
**END VIEW**



**ELEVATION (Looking East)**



**FOOTING PLAN**



**ANCHOR BOLT LAYOUT**  
(For information only. See Note 2)

4/5/19 PM xxxxx-05-Pier 11 Details.dgn



|   |              |          |
|---|--------------|----------|
| USER NAME = bshoeflger                                      | DESIGNED JNP | REVISION |
|   | CHECKED JRM  | REVISION |
| PLOT SCALE = 2x6 "/td> <td>DRAWN JNP</td> <td>REVISION</td> | DRAWN JNP    | REVISION |
| PLOT DATE = 12/16/2015                                      | CHECKED JRM  | REVISION |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PIER 2 DETAILS  
STRUCTURE NO. 022-3186**  
SHEET NO. 8 OF 10 SHEETS

| F.A.P.                    | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------|-----------|
| 0362                      | 14-F3000-12-BT | DUPAGE | 69           | 33        |
| CONTRACT NO 61C31         |                |        |              |           |
| ILLINOIS FED. AID PROJECT |                |        |              |           |



| Profile Elevation (ft) | SOIL AND ROCK DESCRIPTION   | Depth (ft) | Sample No. | SPT Values (blows/6 in) | Qu (tsf) | Moisture Content (%) | Profile Elevation (ft) | SOIL AND ROCK DESCRIPTION | Depth (ft) | Sample No. | SPT Values (blows/6 in) | Qu (tsf) | Moisture Content (%) |
|------------------------|---|------------|------------|-------------------------|----------|----------------------|------------------------|---------------------------|------------|------------|-------------------------|----------|----------------------|
| 791.2                  | 12-inch thick, black SILTY CLAY<br>--TOPSOIL--<br>Medium stiff to hard, brown to gray SILTY CLAY to SILTY CLAY LOAM, trace to little gravel<br>--L <sub>c</sub> (%)=37, P <sub>L</sub> (%)=17--<br>--%Gravel=1.0--<br>--%Sand=5.4--<br>--%Silt=55.7--<br>--%Clay=38.0-- |            | 1          | 4 5 6                   | 4.02 B   | 20                   |                        |                           |            | 11         | 4 5 6                   | 2.95 B   | 22                   |
|                        |   |            | 2          | 6 7 9                   | 4.18 B   | 19                   |                        |                           |            | 12         | 11 9 11                 | 0.50 P   | 19                   |
|                        |   |            | 3          | 6 7 10                  | 4.02 B   | 23                   |                        |                           |            |            |                         |          |                      |
|                        |   | 10         | 4          | 5 6 8                   | 3.20 B   | 23                   |                        |                           |            | 13         | 5 7 10                  | 1.00 P   | 23                   |
|                        |   |            | 5          | 4 4 6                   | 2.38 B   | 22                   |                        |                           |            |            |                         |          |                      |
|                        |   | 15         | 6          | 4 3 6                   | 2.21 B   | 21                   |                        |                           |            | 14         | 8 10 13                 | 5.00 B   | 17                   |
|                        |   |            | 7          | 4 4 5                   | 1.97 B   | 22                   |                        |                           |            |            |                         |          |                      |
|                        |   | 20         | 8          | 3 4 5                   | 2.30 B   | 24                   |                        |                           |            | 15         | 6 8 9                   | 2.79 B   | 19                   |
|                        |   |            | 9          | 4 6 8                   | 2.21 B   | 23                   |                        |                           |            |            |                         |          |                      |
|                        |   | 25         | 10         | 5 8 7                   | 1.00 P   | 12                   |                        |                           |            | 16         | 5 10 8                  | 2.30 B   | 22                   |

**GENERAL NOTES**  
 Begin Drilling 03-27-2014 Complete Drilling 03-27-2014  
 Drilling Contractor WTS Drill Rig Diedrich D-50 ATV  
 Driller K&K Logger A. Tomaras Checked by B. Wilson  
 Drilling Method 2.25" SSA to 10', mud rotary thereafter; boring backfilled upon completion

**WATER LEVEL DATA**  
 While Drilling Dry  
 At Completion of Drilling Dry  
 Time After Drilling NA  
 Depth to Water NA  
 The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

Boring terminated at 50.00 ft

| Profile Elevation (ft) | SOIL AND ROCK DESCRIPTION   | Depth (ft) | Sample No. | SPT Values (blows/6 in) | Qu (tsf) | Moisture Content (%) | Profile Elevation (ft) | SOIL AND ROCK DESCRIPTION | Depth (ft) | Sample No. | SPT Values (blows/6 in) | Qu (tsf) | Moisture Content (%) |
|------------------------|---|------------|------------|-------------------------|----------|----------------------|------------------------|---------------------------|------------|------------|-------------------------|----------|----------------------|
| 788.3                  | 12-inch thick, black SILTY CLAY<br>--TOPSOIL--<br>Stiff to hard, brown to gray SILTY CLAY, trace gravel |            | 1          | 3 3 3                   | 1.72 B   | 20                   |                        |                           |            | 11         | 9 10 13                 | 3.53 B   | 14                   |
|                        |   |            | 2          | 3 3 5                   | 3.28 B   | 24                   |                        |                           |            | 12         | 5 6 8                   | 2.71 B   | 21                   |
|                        |   |            | 3          | 9 13 17                 | 6.72 B   | 17                   |                        |                           |            |            |                         |          |                      |
|                        |   | 10         | 4          | 7 9 9                   | 4.10 B   | 19                   |                        |                           |            | 13         | 5 5 10                  | 1.80 B   | 21                   |
|                        |   |            | 5          | 5 6 7                   | 3.28 B   | 22                   |                        |                           |            |            |                         |          |                      |
|                        |   | 15         | 6          | 5 5 7                   | 3.12 B   | 22                   |                        |                           |            | 14         | 5 7 11                  | 3.94 B   | 19                   |
|                        |   |            | 7          | 6 6 8                   | 2.62 B   | 21                   |                        |                           |            |            |                         |          |                      |
|                        |   | 20         | 8          | 7 6 9                   | 2.21 B   | 23                   |                        |                           |            | 15         | 9 9 14                  | 5.25 B   | 18                   |
|                        |   |            | 9          | 3 7 9                   | 3.53 B   | 22                   |                        |                           |            |            |                         |          |                      |
|                        |   | 25         | 10         | 7 18 13                 | 2.00 P   | 27                   |                        |                           |            | 16         | 3 6 10                  | NP       | 18                   |

**GENERAL NOTES**  
 Begin Drilling 03-27-2014 Complete Drilling 03-27-2014  
 Drilling Contractor WTS Drill Rig Diedrich D-50 ATV  
 Driller K&K Logger A. Tomaras Checked by B. Wilson  
 Drilling Method 2.25" SSA to 10', mud rotary thereafter; boring backfilled upon completion

**WATER LEVEL DATA**  
 While Drilling Dry  
 At Completion of Drilling Dry  
 Time After Drilling NA  
 Depth to Water NA  
 The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

Boring terminated at 50.00 ft

4:57:21 PM xxxxxx-10-Soil Borings 1.dgn

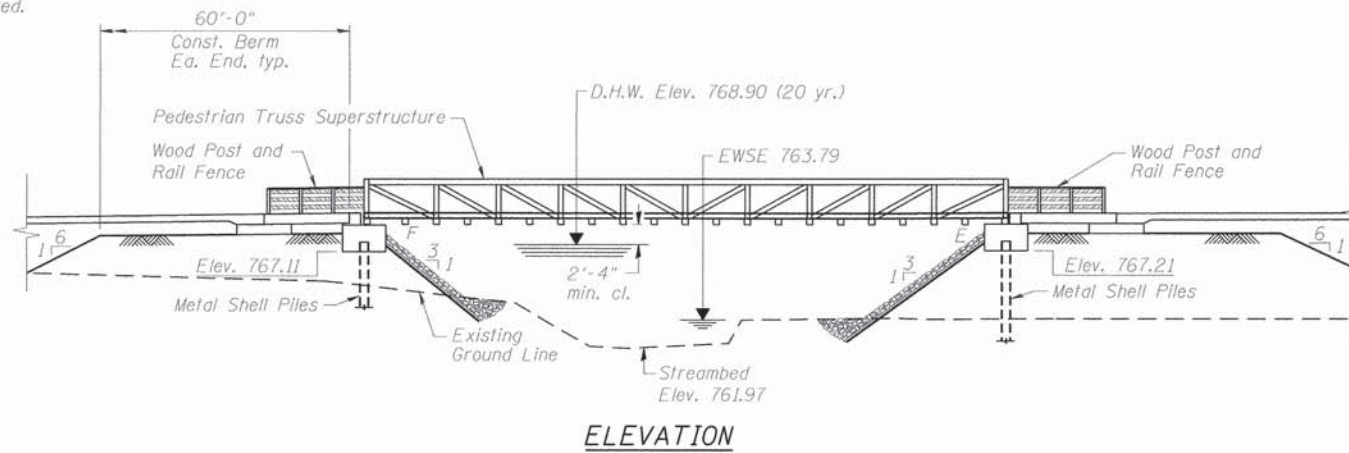






Bench Mark: Found Cross Cut in Southeast Concrete Wing Wall of Steel Bridge. Elev. 771.91.

Existing Structure: None. No traffic control required.



**CURVE DATA**

**PRBIKE-16**

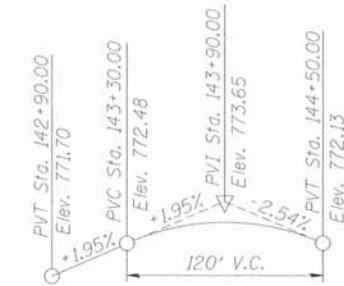
P.I. Sta. = 144+44.56  
 $\Delta = 11^\circ 57' 36''$  (RT)  
 $D = 28^\circ 38' 52''$   
 $R = 200.00'$   
 $T = 20.95'$   
 $L = 41.75'$   
 $E = 1.09'$   
 $S.E. = \text{-----}$   
 P.C. Sta. = 144+23.61  
 P.T. Sta. = 144+65.36



*Matthew D. Santeford* 12-17-2015

MATTHEW D. SANTEFORD, P.E., S.E.  
 NO. 081-007244  
 EXP. DATE 11/30/2016

"I certify that to the best of my knowledge, information and belief, this bridge design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with the requirements of the current 'AASHTO LRFD Bridge Design Specifications'."



**PROFILE GRADE**

Along  $\phi$  North Central DuPage Regional Trail

**LOADING**

Pedestrian Live Load = 90 psf  
 Vehicle Live Load = H10

**DESIGN SPECIFICATIONS**

2014 AASHTO LRFD Bridge Design Specifications, 7th Edition  
 2009 AASHTO LRFD Guide Specifications for the Design of Pedestrian Bridges

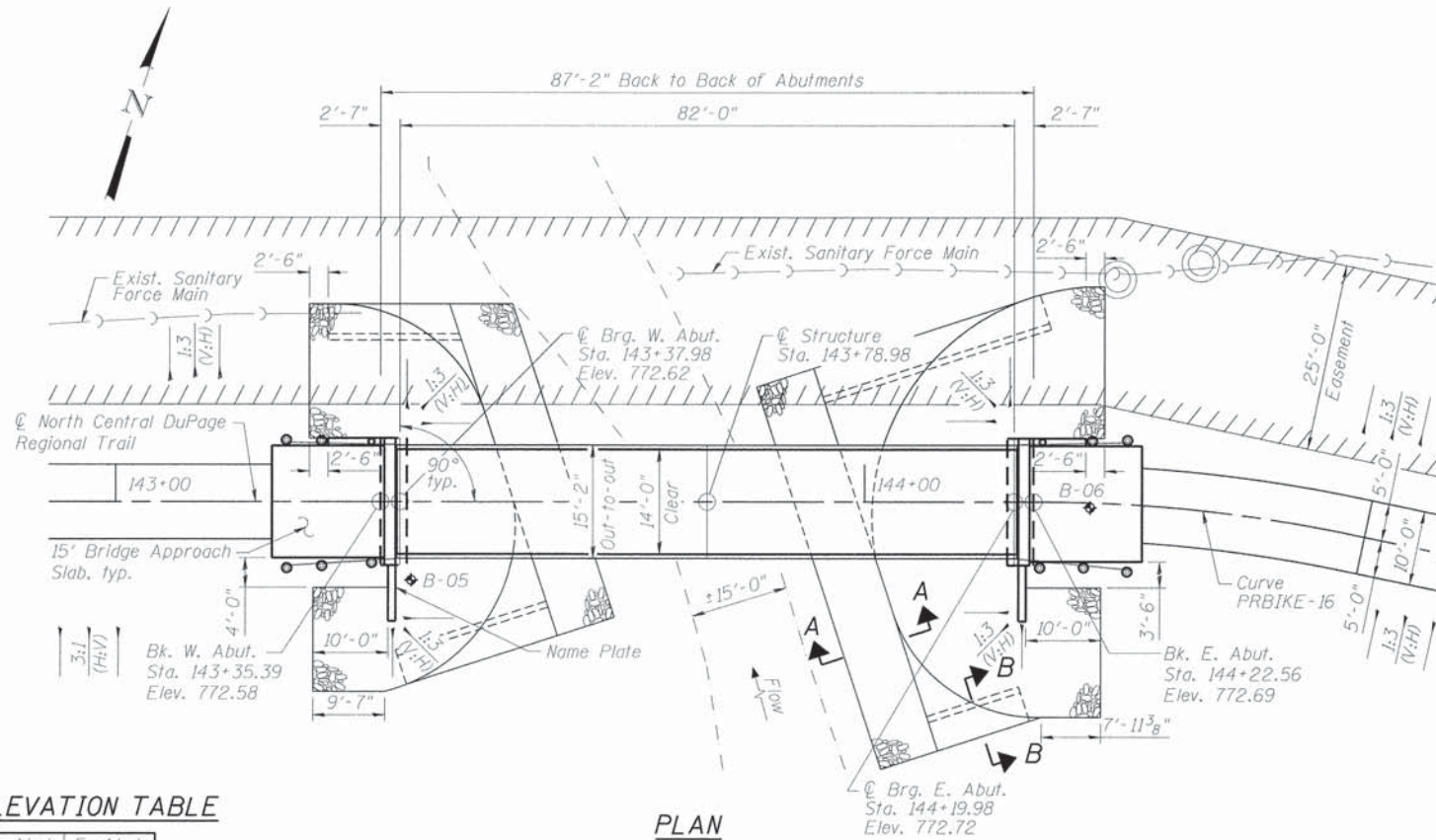
**DESIGN STRESSES**

**FIELD UNITS**

$f'_c = 3,500$  psi  
 $f_y = 60,000$  psi (Reinforcement)  
 $f_y = 50,000$  psi (M270 Grade 50W)

**SEISMIC DATA**

Seismic Performance Zone (SPZ) = 1  
 Design Spectral Acceleration at 1.0 sec. ( $S_{D1}$ ) = 0.086 g  
 Design Spectral Acceleration at 0.2 sec. ( $S_{D5}$ ) = 0.153 g  
 Soil Site Class = D



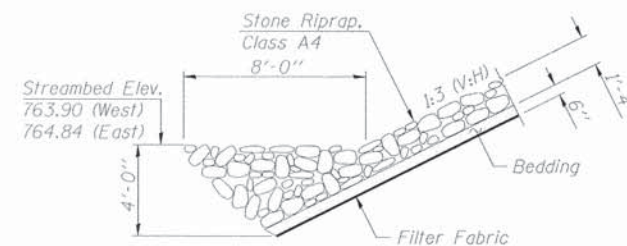
**DESIGN SCOUR ELEVATION TABLE**

|                              |                 |                 |
|------------------------------|-----------------|-----------------|
| Design Scour Elevation (ft.) | W. Abut. 767.11 | E. Abut. 767.21 |
|------------------------------|-----------------|-----------------|

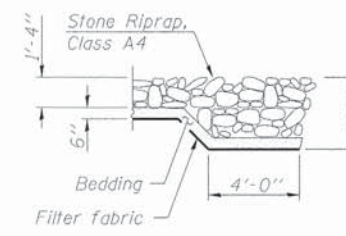
**WATERWAY INFORMATION**

| Flood       | Freq. Yr. | Q C.F.S. | Opening Sq. Ft. |             | Head - Ft. |          | Headwater El. |
|-------------|-----------|----------|-----------------|-------------|------------|----------|---------------|
|             |           |          | Proposed        | Nat. H.W.E. | Proposed   | Proposed |               |
| Design      | 10        | 161      | 238             | 768.52      | 0.00       | 768.52   | 768.90        |
| Base        | 100       | 326      | 324             | 769.73      | 0.00       | 769.73   | 769.73        |
| Overtopping | -         | -        | -               | -           | -          | -        | -             |
| Max. Calc.  | 500       | 491      | 382             | 770.50      | 0.00       | 770.50   | 770.50        |

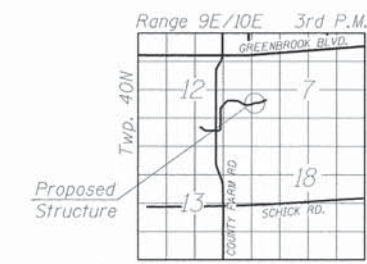
10 year velocity through proposed bridge = 0.67 fps



**SECTION A-A**



**SECTION B-B**



**LOCATION SKETCH**

**GENERAL PLAN AND ELEVATION**

**NORTH CENTRAL DUPAGE REGIONAL TRAIL OVER KEENEYVILLE TRIBUTARY SECTION 14-F3000-12-BT DUPAGE COUNTY STATION 143+78.98 STRUCTURE NUMBER 022-9008**



|                                     |              |         |
|-------------------------------------|--------------|---------|
| USER NAME = bshaefflger             | DESIGNED JNP | REVISED |
| PLLOT SCALE = 12:0.0003 ft/in / in. | CHECKED JRM  | REVISED |
| PLLOT DATE = 12/16/2015             | DRAWN JNP    | REVISED |
|                                     | CHECKED JRM  | REVISED |

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN AND ELEVATION STRUCTURE NO. 022-9008**

SHEET NO. 1 OF 8 SHEETS

|                           |                        |               |                   |              |
|---------------------------|------------------------|---------------|-------------------|--------------|
| F.A.P. 0362               | SECTION 14-F3000-12-BT | COUNTY DUPAGE | TOTAL SHEETS 69   | SHEET NO. 36 |
|                           |                        |               | CONTRACT NO 61C31 |              |
| ILLINOIS FED. AID PROJECT |                        |               |                   |              |



**GENERAL NOTES**

1. Fasteners shall be ASTM A325 Type 1, mechanically galvanized bolts in painted areas and ASTM A325 Type 3 in unpainted areas. Bolt size shall be determined by the Contractor.
2. All structural steel shall be AASHTO M270 Grade 50W (except expansion joints which shall be AASHTO M270 Grade 36)
3. No field welding is permitted except as specified in the contract documents.
4. Reinforcement bars designated (E) shall be epoxy coated.
5. Concrete sealer shall be applied to the designated areas of the exposed surfaces of the abutments up to the back of backwall location.
6. All structural steel and exposed surfaces of bearings within a distance of 10 ft. each way from the deck joints shall be painted as specified in Section 506 of the Standard Specifications.
7. Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.
8. The embankment configuration shown shall be the minimum that must be placed and compacted prior to construction of the abutments.
9. The Organic Zinc Rich Primer / Epoxy / Urethane Paint System shall be used for painting the new structural steel except where otherwise noted. The entire system shall be shop applied, with the exception of masked off connection surfaces, field installed fasteners and damaged areas shall be touched up in the field. The color of the final finish coat shall be Reddish Brown, Munsell No. 2.5 YR 3/4.
10. The contractor shall submit for approval a detailed structure drawing prior to the layout of the structure foundations. See Check Sheet #10 in the IDOT Supplemental Specifications and Recurring Special Provisions.

**TRUSS MANUFACTURER**

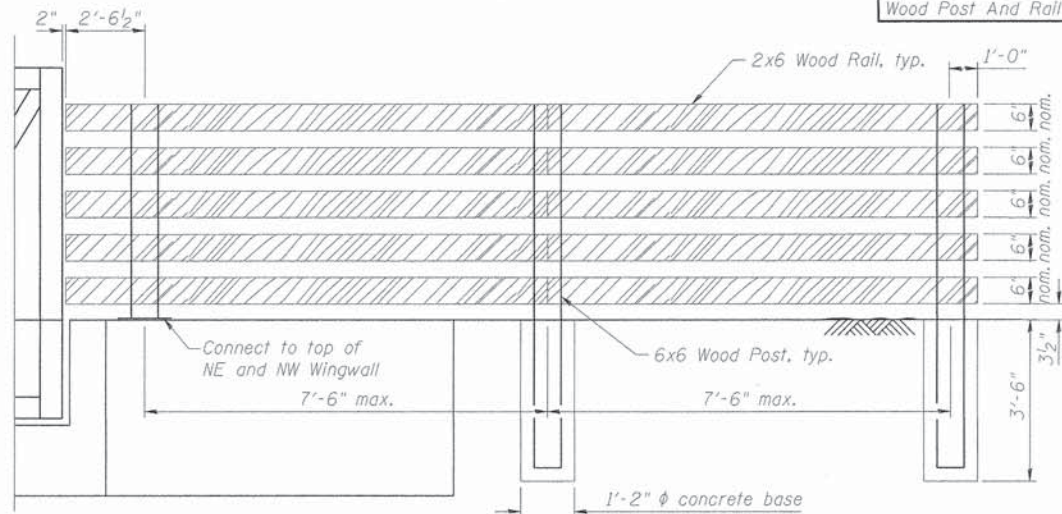
The substructure is designed per AASHTO LRFD and based on the assumed truss loads (including deck) shown below.  
 Total factored superstructure dead load of West Abutment = 75,000 pounds.  
 Total factored superstructure dead load at East Abutment = 75,000 pounds.  
 Truss manufacturer shall camber the truss as necessary to provide allowance for dead load deflection.  
 Bridge bearing seat elevations are subject to revision based on the approved pedestrian truss superstructure shop drawings. Contractor shall verify all dimensions and elevations with final shop drawings.  
 Truss manufacturer shall provide the reinforced concrete deck design. Concrete deck to utilize stay-in-place galvanized forms. Reinforcement shall be epoxy coated. Contractor shall place the concrete deck after truss is set. Cost included with Pedestrian Truss Superstructure.

**INDEX OF SHEETS**

- 1 General Plan and Elevation
- 2 General Data
- 3 Bridge Approach Slab Details
- 4 West Abutment Details
- 5 East Abutment Details
- 6 Metal Shell Pile Details
- 7 Bar Splicer Assembly and Mechanical Splicer Details
- 8 Borings Logs

**TOTAL BILL OF MATERIAL**

| Item                                      | Unit    | Super | Sub   | Total |
|---|---------|-------|-------|-------|
| Stone Riprap, Class A4                    | Sq. Yd. |       | 382   | 382   |
| Filter Fabric                             | Sq. Yd. |       | 359   | 359   |
| Concrete Structures                       | Cu. Yd. |       | 25.1  | 25.1  |
| Concrete Superstructure                   | Cu. Yd. | 20.7  |       | 20.7  |
| Protective Coat                           | Sq. Yd. | 65    |       | 65    |
| Reinforcement Bars, Epoxy Coated          | Pound   | 3,830 | 2,560 | 6,390 |
| Bar Splicers                              | Each    | 34    |       | 34    |
| Furnishing Metal Shell Piles 12" X 0.250" | Foot    |       | 170   | 170   |
| Driving Piles                             | Foot    |       | 170   | 170   |
| Test Pile Metal Shells                    | Each    |       | 2     | 2     |
| Name Plates                               | Each    | 1     |       | 1     |
| Concrete Sealer                           | Sq. Ft. |       | 120   | 120   |
| Geocomposite Wall Drain                   | Sq. Yd. |       | 22    | 22    |
| Pedestrian Truss Superstructure           | Sq. Ft. | 1,221 |       | 1,221 |
| Granular Backfill For Structures          | Cu. Yd. |       | 21    | 21    |
| Pipe Underdrains For Structures 4"        | Foot    |       | 72    | 72    |
| Wood Post And Rail Fence                  | Foot    | 60    |       | 60    |



**WOOD POST AND RAIL FENCE DETAIL**

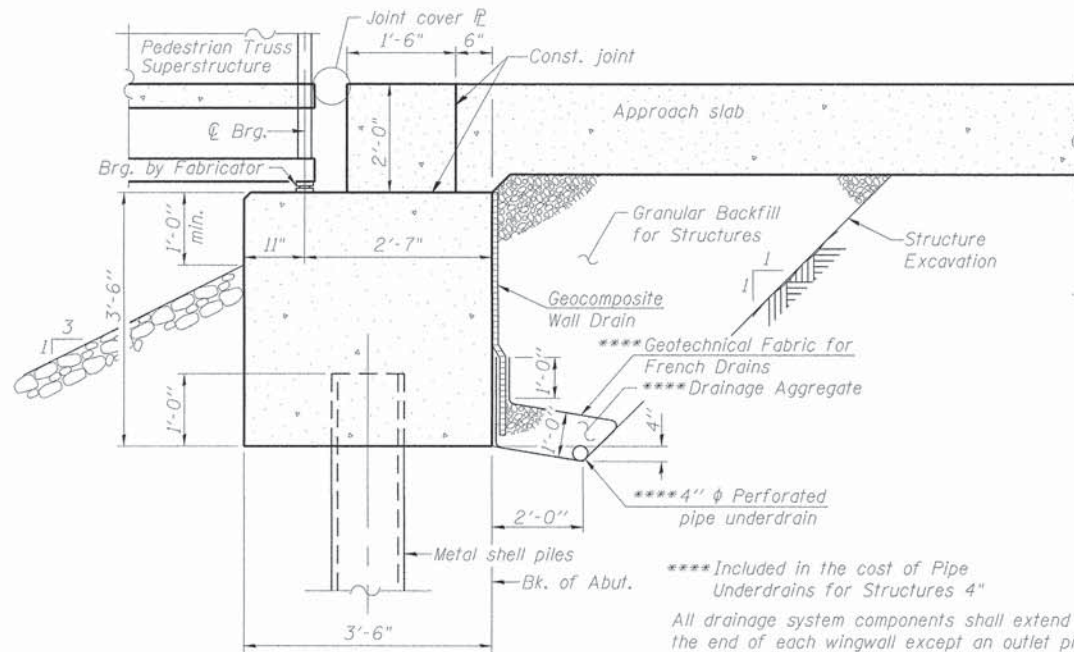
Notes:  
 Wood shall be southern pine or douglas fir. Wood shall be pressure treated with CCA or ACA. Two coats of clear sealing product shall be applied to the completed fence. See Special Provisions.

BUILT 201. BY  
 FOREST PRESERVE DISTRICT  
 OF DUPAGE COUNTY  
 SEC. 14-F3000-12-BT  
 F.A.P. RT. 0362 STATION 143+78.98  
 STR. NO. 022-9008 LOADING H-10

**NAME PLATE**

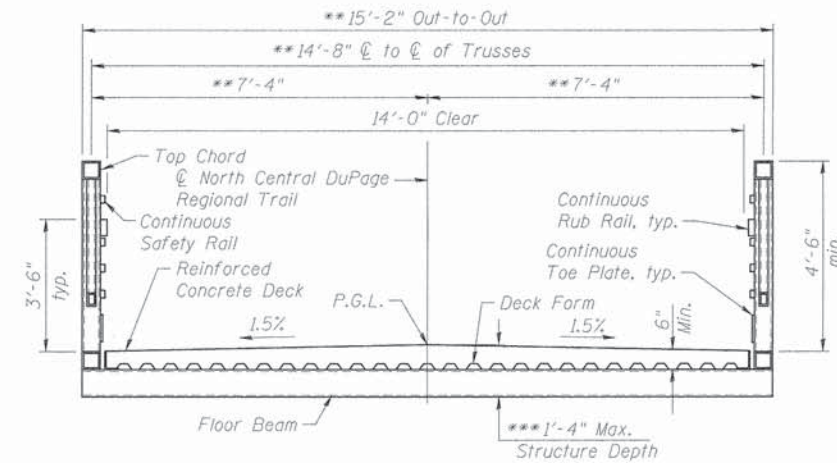
See Std. 515001

\* Name of bridge to be furnished by the Forest Preserve District of DuPage County.



**SECTION THRU PILE SUPPORTED ABUTMENT**

\*\*\*\*Included in the cost of Pipe Underdrains for Structures 4".  
 All drainage system components shall extend to 2'-0" from the end of each wingwall except an outlet pipe shall extend until intersecting with the side slopes. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 60110). Cost of Concrete Headwalls for Structures included with Pipe Underdrains for Structures 4".



**CROSS SECTION THRU TRUSS 82 FT. SPAN**

Notes:  
 \*\* Subject to refinement per Truss Manufacturer.  
 \*\*\* Structure depth does not include height of bearing.  
 Truss supplier shall provide joint covers over all open joints at abutments (typ. 2 locations).

4:57:42 PM xxx-xx-02-General Data.dgn



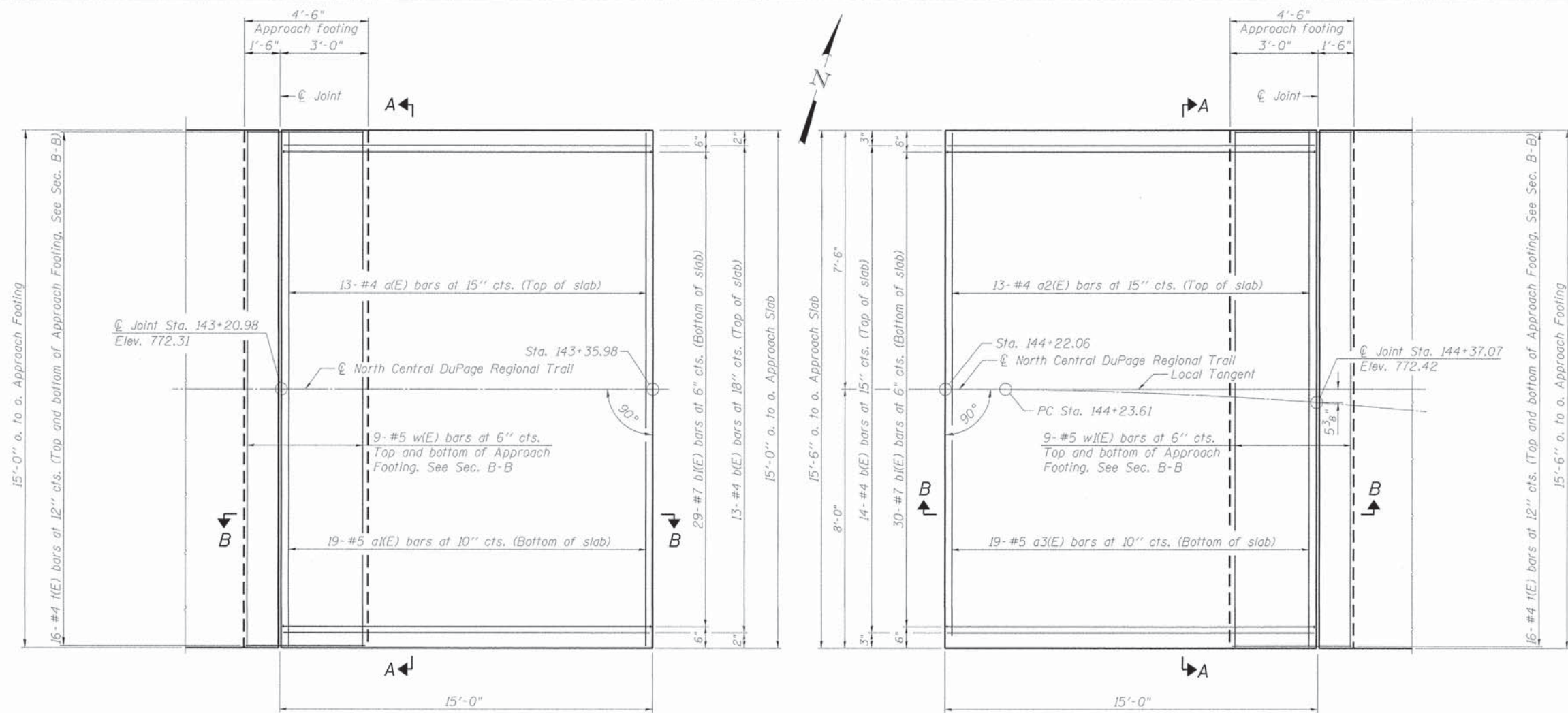
|                                   |              |           |
|-----------------------------------|--------------|-----------|
| USER NAME = bshoeffler            | DESIGNED JNP | REVISIONS |
| PLLOT SCALE = 24:0.0000 's' / in. | CHECKED JRM  | REVISIONS |
| PLLOT DATE = 12/16/2015           | DRAWN JNP    | REVISIONS |
|                                   | CHECKED JRM  | REVISIONS |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**GENERAL DATA  
 STRUCTURE NO. 022-9008  
 SHEET NO. 2 OF 8 SHEETS**

|                   |                |        |                           |           |
|-------------------|----------------|--------|---------------------------|-----------|
| F.A.P.            | SECTION        | COUNTY | TOTAL SHEETS              | SHEET NO. |
| 0362              | 14-F3000-12-BT | DUPAGE | 69                        | 37        |
| CONTRACT NO 61C31 |                |        | ILLINOIS FED. AID PROJECT |           |





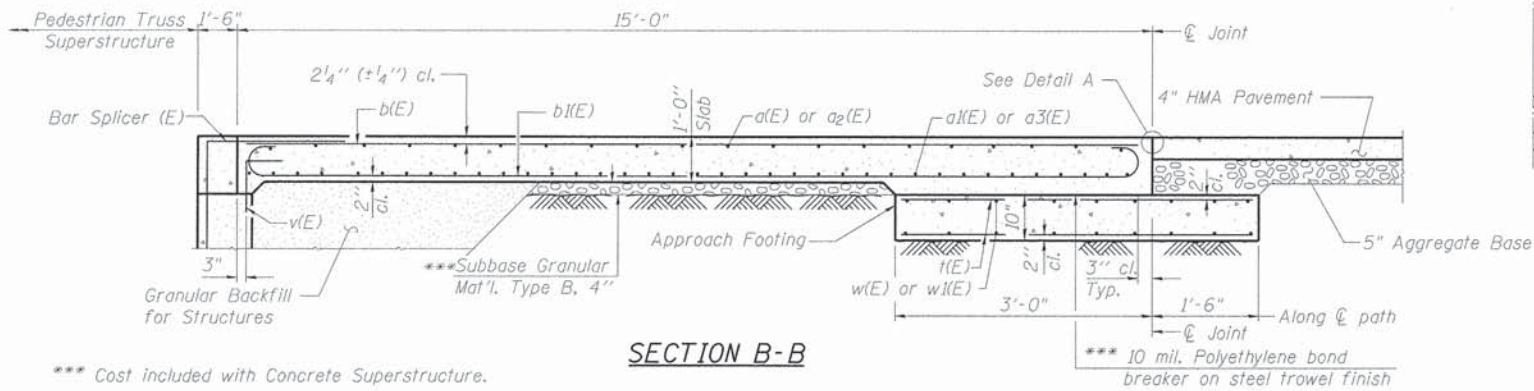
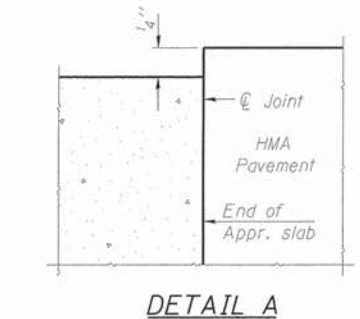
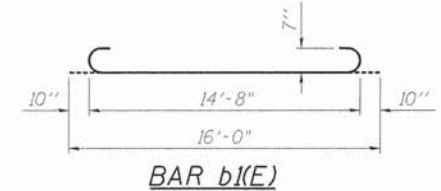
WEST APPROACH - PLAN

EAST APPROACH - PLAN

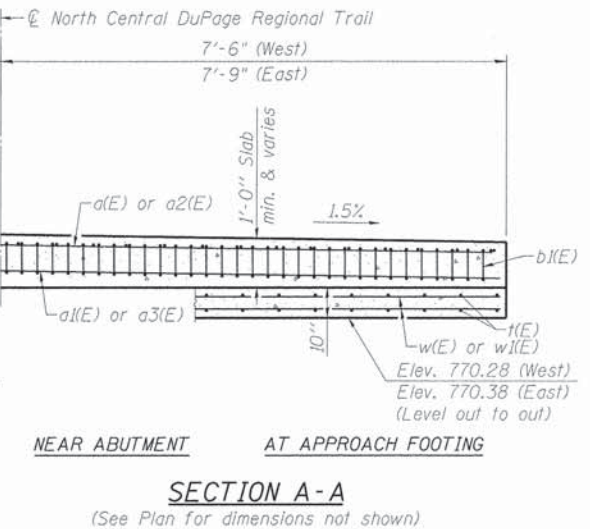
Notes:  
 Approach slab concrete shall be paid for as Concrete Superstructure.  
 Approach footing concrete shall be paid for as Concrete Structures.  
 Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.  
 The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.  
 Cost of excavation for approach footing included with Concrete Structures.  
 For v(E) bar details, see sheet 4 and 5 of 8.  
 For Granular Backfill for Structures and drainage treatment details, see sheet 2 of 8.

**TWO APPROACHES  
 BILL OF MATERIAL**

| Bar                              | No. | Size | Length  | Shape |
|----------------------------------|-----|------|---------|-------|
| a(E)                             | 13  | #4   | 14'-8"  | —     |
| a1(E)                            | 19  | #5   | 14'-8"  | —     |
| a2(E)                            | 13  | #4   | 15'-2"  | —     |
| a3(E)                            | 19  | #5   | 15'-2"  | —     |
| b(E)                             | 27  | #4   | 14'-8"  | —     |
| b1(E)                            | 59  | #7   | 16'-4"  | U     |
| i(E)                             | 66  | #4   | 4'-2"   | —     |
| w(E)                             | 18  | #5   | 14'-8"  | —     |
| w1(E)                            | 18  | #5   | 15'-2"  | —     |
| Concrete Structures              |     |      | Cu. Yd. | 4.3   |
| Concrete Superstructure          |     |      | Cu. Yd. | 16.9  |
| Protective Coat                  |     |      | Sq. Yd. | 51    |
| Reinforcement Bars, Epoxy Coated |     |      | Pound   | 3,830 |



SECTION B-B



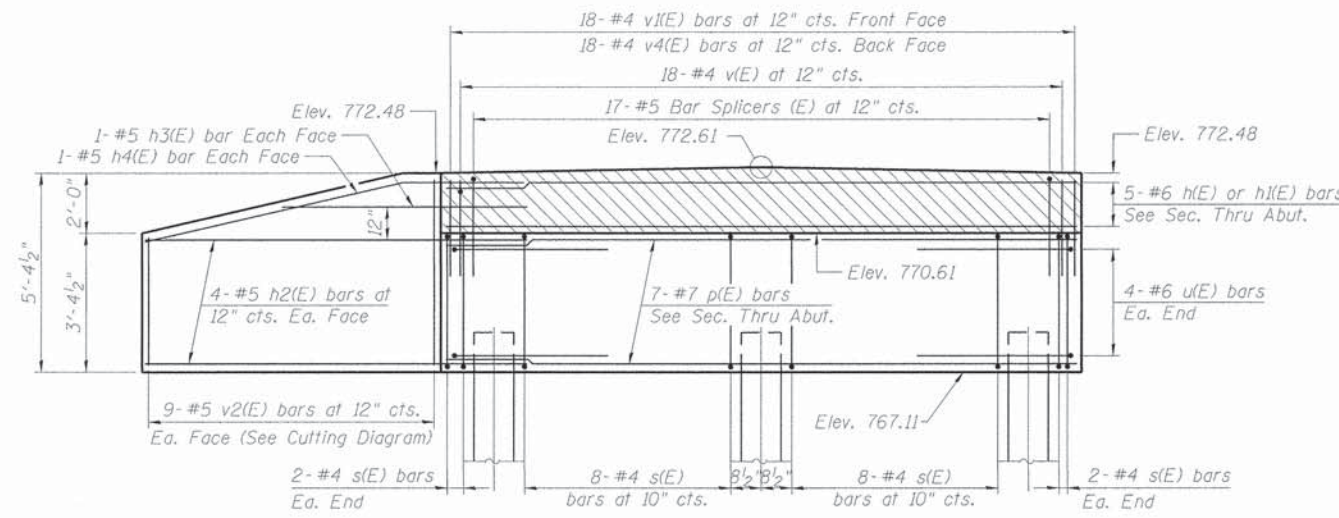
SECTION A-A

\*\*\* Cost included with Concrete Superstructure.

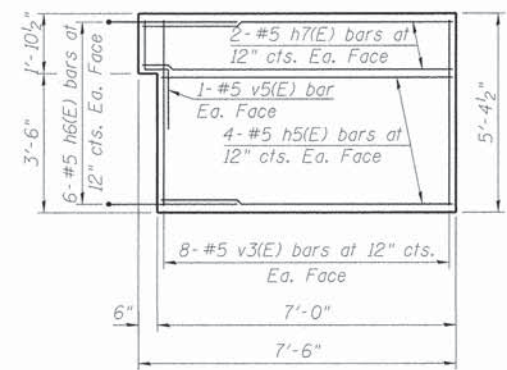
\*\*\* 10 mil. Polyethylene bond breaker on steel trowel finish

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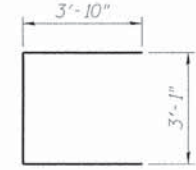




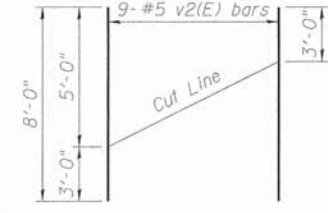
**ELEVATION**  
(Looking West)



**ELEVATION- NORTHWEST WINGWALL**



**BAR u(E)**



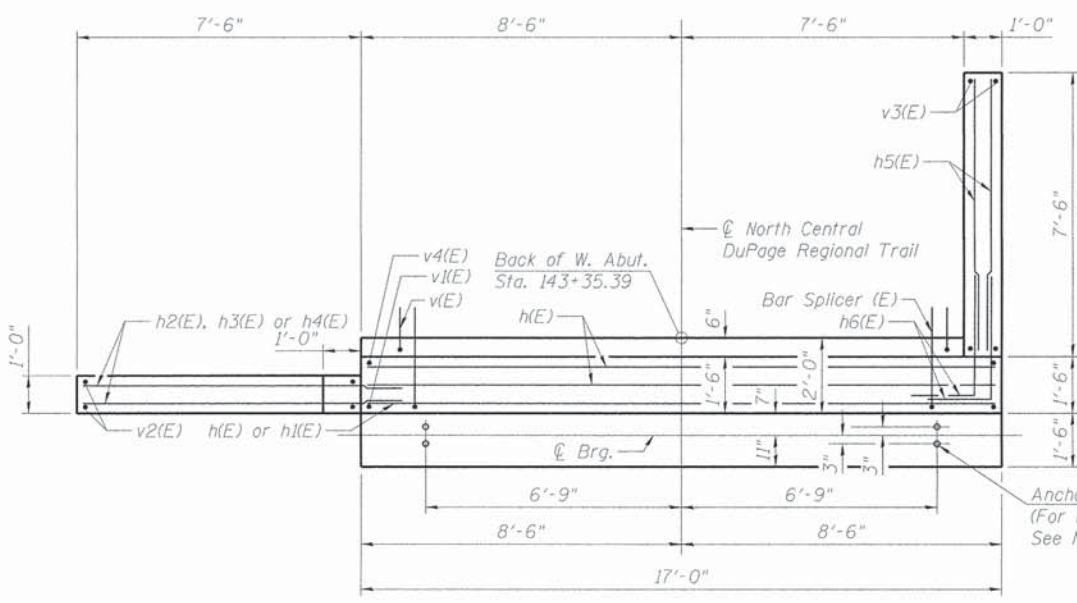
**BAR v2(E)**

Order v2(E) full length. Cut as shown and use remainder of bars in opposite face.

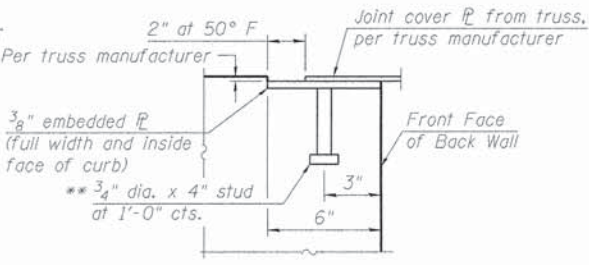
**BILL OF MATERIAL**

| Bar   | No. | Size | Length  | Shape |
|-------|-----|------|---------|-------|
| h(E)  | 4   | #6   | 16'-8"  | —     |
| h1(E) | 1   | #5   | 16'-8"  | —     |
| h2(E) | 8   | #5   | 10'-9"  | —     |
| h3(E) | 2   | #5   | 7'-6"   | —     |
| h4(E) | 2   | #5   | 10'-11" | —     |
| h5(E) | 8   | #5   | 6'-8"   | —     |
| h6(E) | 12  | #5   | 5'-7"   | J     |
| h7(E) | 4   | #5   | 7'-2"   | —     |
| p(E)  | 7   | #7   | 16'-8"  | —     |
| s(E)  | 20  | #4   | 13'-1"  | □     |
| u(E)  | 8   | #6   | 10'-9"  | C     |
| v(E)  | 18  | #5   | 4'-1"   | └     |
| v1(E) | 18  | #4   | 2'-11"  | —     |
| v2(E) | 9   | #5   | 8'-0"   | —     |
| v3(E) | 16  | #5   | 5'-0"   | —     |
| v4(E) | 18  | #4   | 4'-3"   | —     |
| v5(E) | 2   | #5   | 3'-11"  | —     |

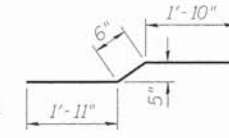
|   |         |       |
|---|---------|-------|
| Concrete Structures                       | Cu. Yd. | 10.4  |
| Concrete Superstructure                   | Cu. Yd. | 1.9   |
| Protective Coat                           | Sq. Yd. | 7     |
| Reinforcement Bars, Epoxy Coated          | Pound   | 1,280 |
| Bar Splicers                              | Each    | 17    |
| Furnishing Metal Shell Piles 12" x 0.250" | Foot    | 66    |
| Driving Piles                             | Foot    | 66    |
| Test Pile Metal Shells                    | Each    | 1     |
| Concrete Sealer                           | Sq. Ft. | 60    |



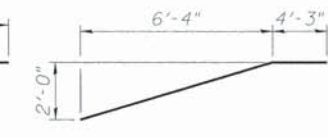
**TOP VIEW**



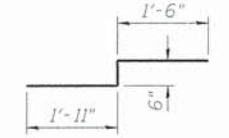
**DETAIL A**



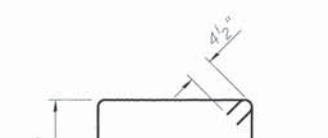
**BAR v4(E)**



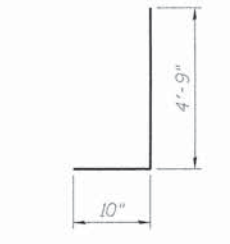
**BAR h4(E)**



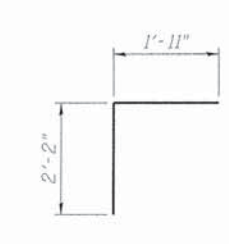
**BAR v5(E)**



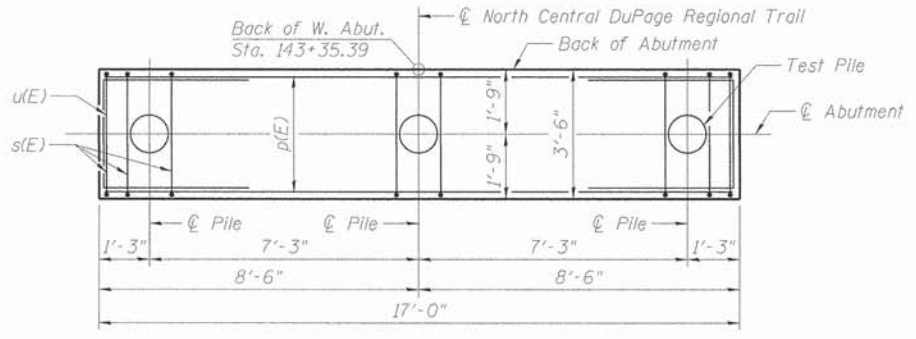
**BAR s(E)**



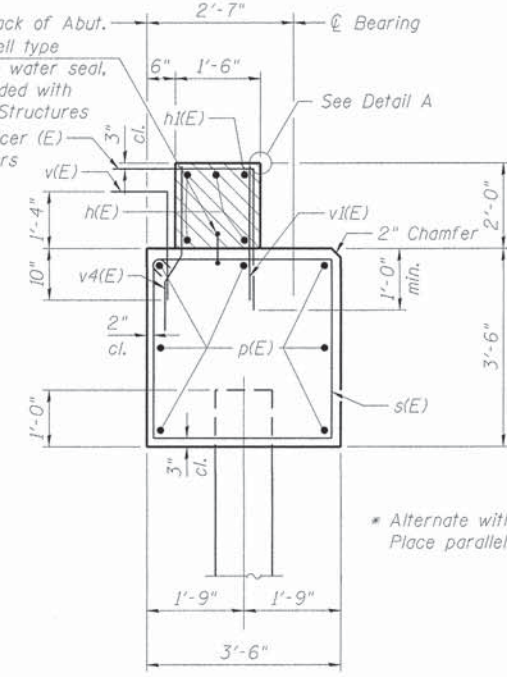
**BAR h6(E)**



**BAR v(E)**



**PLAN-PILE CAP**



**SECTION THRU ABUTMENT**

**MINIMUM BAR LAPS**

| Bar | Lap    |
|-----|--------|
| #5  | 3'-3"  |
| #6  | 3'-10" |

**PILE DATA**

Type: Metal Shell-12 in. dia. x 0.25 in. walls  
 Nominal Required Bearing: 225 Kips  
 Factored Resistance Available: 123 Kips  
 Est. Length: 33 ft.  
 No. Production Piles: 2  
 No. Test Piles: 1

**LEGEND**

○ Denotes Vertical Metal Shell Pile

Notes:  
 Concrete clear cover shall be 2" minimum unless noted otherwise.  
 Truss fabricator shall determine size and location of anchor bolts. Cost included with Pedestrian Truss Superstructure.  
 All edges shall have standard 3/4" chamfers except as noted.  
 Space reinforcement in cap to miss anchor bolts.  
 For details of piles, see sheet 6 of 8.  
 Concrete Sealer shall be applied to the beam seat and front face of backwall and cap.  
 Truss fabricator shall provide a 1" nominal shim pack for each bearing consisting of a 1/2", 1/4" and 2-1/8" shim plates matching the dimensions of the masonry plate provided, including holes for anchor bolts. Cost included in the cost of Pedestrian Truss Superstructure.  
 Hatched area to be poured after superstructure false work has been removed. Quantity of concrete included with Concrete Superstructure.  
 \*\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.  
 See General Note #10 for construction layout requirements prior to driving piles.

4:57:44 PM xxxxxx-04-West AbutmentDetails.dgn

|  |                                  |              |         |   |   |                         |                |        |              |           |  |
|--|----------------------------------|--------------|---------|---|---|-------------------------|----------------|--------|--------------|-----------|--|
|  | USER NAME = bshoeffler           | DESIGNED JNP | REVISED | <b>STATE OF ILLINOIS</b><br><b>DEPARTMENT OF TRANSPORTATION</b> | <b>WEST ABUTMENT DETAILS</b><br><b>STRUCTURE NO. 022-9008</b> | F.A.P.                  | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |  |
|  | PLDT SCALE = 24:0.0000 '1" / in. | CHECKED JRM  | REVISED |   |   | 0362                    | 14-F3000-12-BT | DUPAGE | 69           | 39        |  |
|  | PLDT DATE = 12/16/2015           | DRAWN JNP    | REVISED |   |   | SHEET NO. 4 OF 8 SHEETS |                |        |              |           |  |
|  |                                  | CHECKED JRM  | REVISED |   |   | CONTRACT NO 61C31       |                |        |              |           |  |



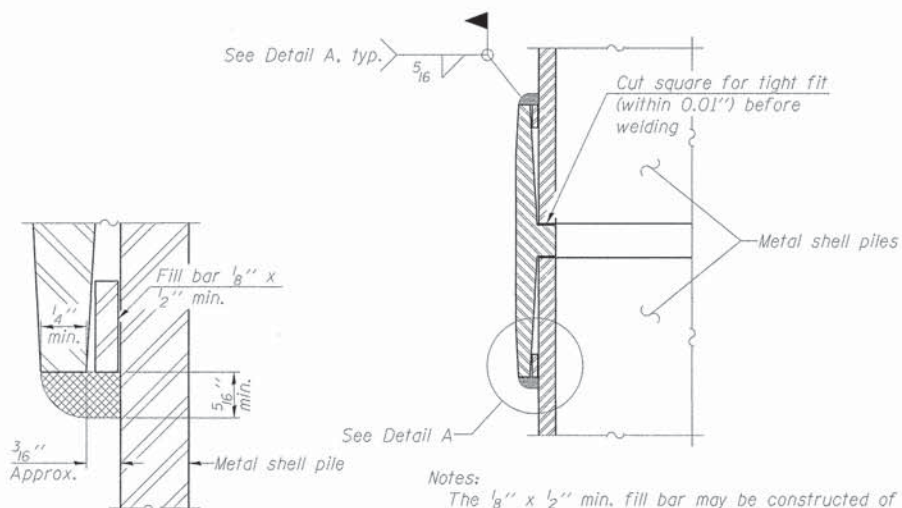






**METAL SHELL PILE TABLE**

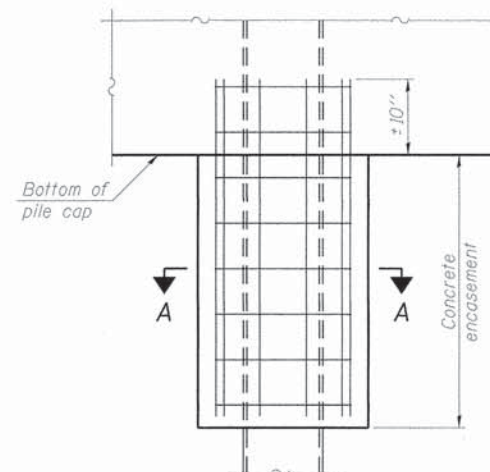
| Designation and outside diameter | Wall thickness t | Weight per foot (Lbs./ft.) | Inside volume (yd. <sup>3</sup> /ft.) |
|----------------------------------|------------------|----------------------------|---------------------------------------|
| PP12                             | 0.179"           | 22.60                      | 0.0274                                |
| PP12                             | 0.250"           | 31.37                      | 0.0267                                |
| PP14                             | 0.250"           | 36.71                      | 0.0368                                |
| PP14                             | 0.312"           | 45.61                      | 0.0361                                |



Notes:  
 The  $\frac{1}{8}$ " x  $\frac{1}{2}$ " min. fill bar may be constructed of 2 bars with a  $\frac{1}{8}$ " max. gap between them.  
 Pile segments shall be driven to solid contact with splicer before welding.

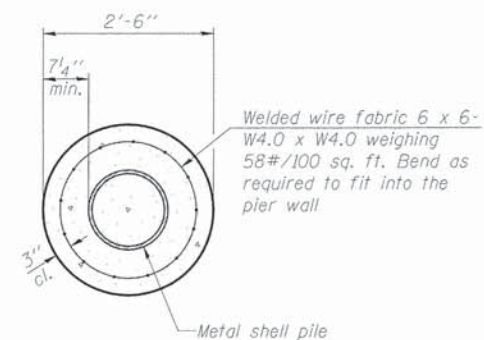
**DETAIL A**

**WELDED COMMERCIAL SPLICE**



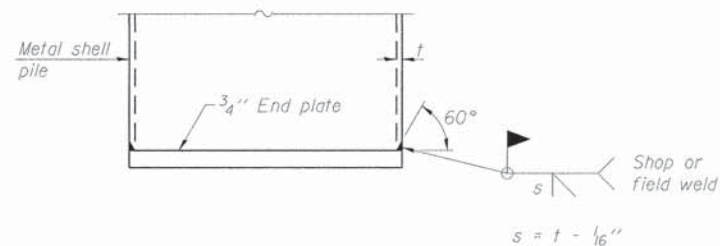
**ELEVATION**

**CONCRETE ENCASEMENT AT PIERS**

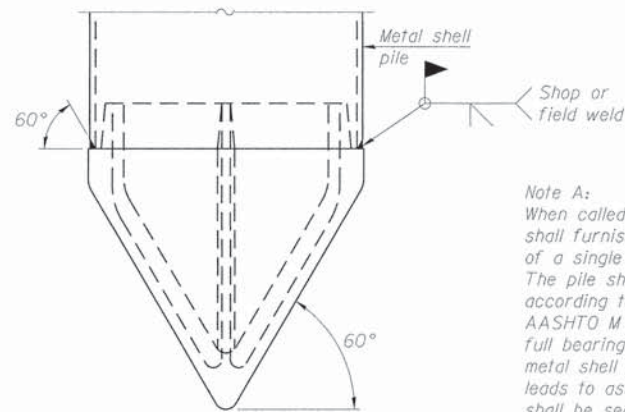


**SECTION A-A**

Note:  
 Forms for encasement may be omitted when soil conditions permit.



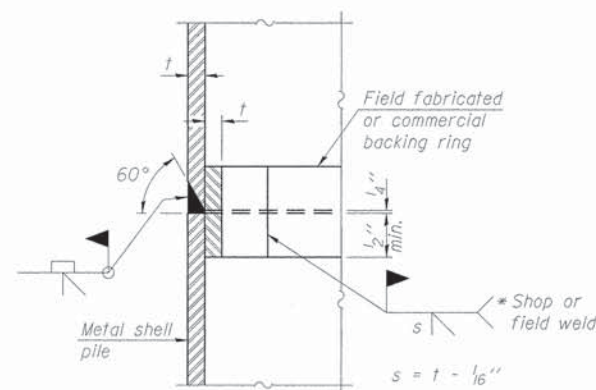
**END PLATE ATTACHMENT**



Note A:  
 When called for on the plans, the Contractor shall furnish metal shell pile shoes consisting of a single piece conical pile point as shown. The pile shoes shall be cast in one piece steel according to either ASTM A 148 Grade 90-60 or AASHTO M 103 Grade 65-35 and shall provide full bearing over the full circumference of the metal shell pile. The pile shoe shall have tapered leads to assure proper alignment and fitting and shall be secured to the pile with a circumferential weld.

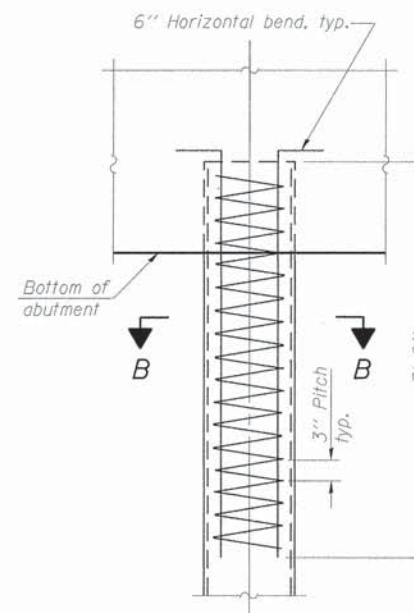
**METAL SHELL PILE SHOE ATTACHMENT**

(See Note A)



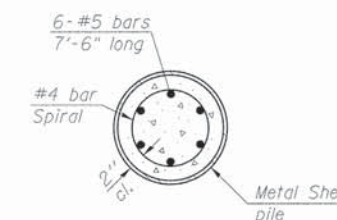
**COMPLETE PENETRATION WELD SPLICE**

\* Field fabricated backing ring may be made from pile shell by removing segment to allow reducing circumference and vertically rejoin with partial joint penetration weld.



**ELEVATION**

**METAL SHELL REINFORCEMENT AT ABUTMENTS**



**SECTION B-B**

Note:  
 The metal shell piles shall be according to ASTM A 252 Grade 3.

F-MS

1-27-12



|                                  |              |           |
|----------------------------------|--------------|-----------|
| USER NAME = bshoeffliger         | DESIGNED JNP | REVISIONS |
| PLDT SCALE = 24:0.0000 's' / in. | CHECKED JRM  | REVISIONS |
| PLDT DATE = 12/16/2015           | DRAWN JNP    | REVISIONS |
|                                  | CHECKED JRM  | REVISIONS |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

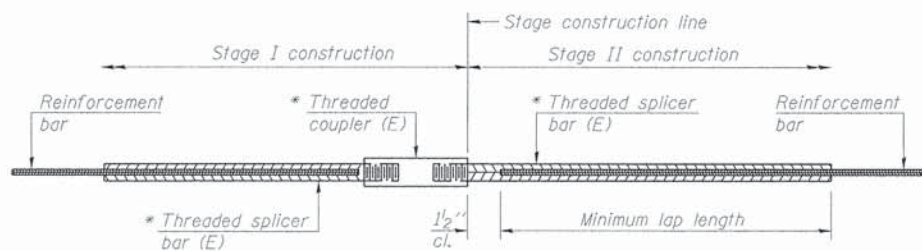
METAL SHELL PILE DETAILS  
 STRUCTURE NO. 022-9008

SHEET NO. 6 OF 8 SHEETS

|                           |                |        |              |           |
|---------------------------|----------------|--------|--------------|-----------|
| F.A.P.                    | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 0362                      | 14-F3000-12-BT | DUPAGE | 69           | 41        |
| CONTRACT NO 61C31         |                |        |              |           |
| ILLINOIS FED. AID PROJECT |                |        |              |           |

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**STANDARD BAR SPLICER ASSEMBLY**

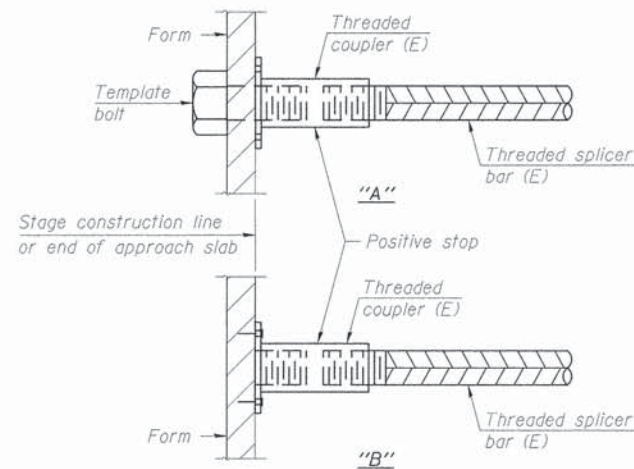
| Bar size to be spliced | Minimum Lap Lengths |         |         |         |         |         |
|------------------------|---------------------|---------|---------|---------|---------|---------|
|                        | Table 1             | Table 2 | Table 3 | Table 4 | Table 5 | Table 6 |
| 3, 4                   | 1'-5"               | 1'-11"  | 2'-1"   | 2'-4"   | 2'-7"   | 2'-11"  |
| 5                      | 1'-9"               | 2'-5"   | 2'-7"   | 2'-11"  | 3'-3"   | 3'-8"   |
| 6                      | 2'-1"               | 2'-11"  | 3'-1"   | 3'-6"   | 3'-10"  | 4'-5"   |
| 7                      | 2'-9"               | 3'-10"  | 4'-2"   | 4'-8"   | 5'-2"   | 5'-10"  |
| 8                      | 3'-8"               | 5'-1"   | 5'-5"   | 6'-2"   | 6'-9"   | 7'-8"   |
| 9                      | 4'-7"               | 6'-5"   | 6'-10"  | 7'-9"   | 8'-7"   | 9'-8"   |

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Class C
- Table 6: Epoxy bar, Top bar top, Class C

Threaded splicer bar length = min. lap length + 1/2" + thread length

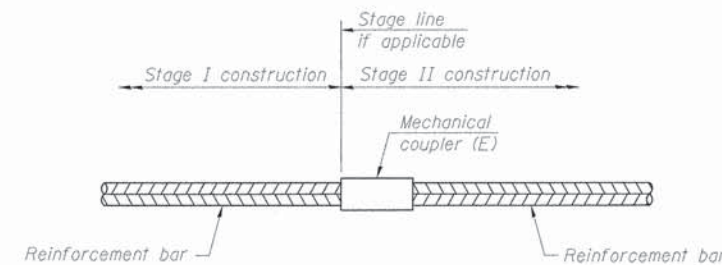
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

| Location | Bar size | No. assemblies required | Table for minimum lap length |
|----------|----------|-------------------------|------------------------------|
|          |          |                         |                              |
|          |          |                         |                              |
|          |          |                         |                              |
|          |          |                         |                              |



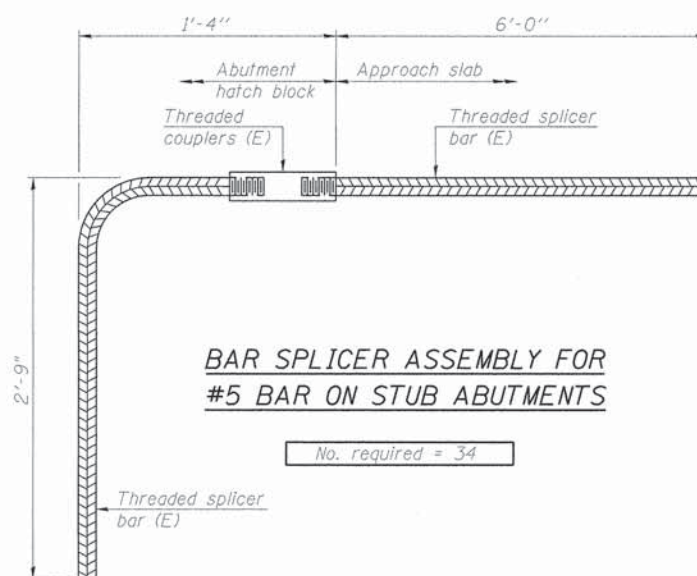
**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

| Location | Bar size | No. assemblies required |
|----------|----------|-------------------------|
|          |          |                         |
|          |          |                         |
|          |          |                         |
|          |          |                         |



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

No. required = 34

**NOTES**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.  
 All reinforcement shall be lapped and tied to the splicer bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.  
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

8-31-12



|                                   |              |         |
|-----------------------------------|--------------|---------|
| USER NAME = bshaeffiger           | DESIGNED JNP | REVISED |
| PLOT SCALE = 12x0.0001 : 1" / 12" | CHECKED JRM  | REVISED |
| PLOT DATE = 12/16/2015            | DRAWN JNP    | REVISED |
|                                   | CHECKED JRM  | REVISED |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
STRUCTURE NO. 022-9008

SHEET NO. 7 OF 8 SHEETS

|                   |                |        |                           |           |
|-------------------|----------------|--------|---------------------------|-----------|
| F.A.P.            | SECTION        | COUNTY | TOTAL SHEETS              | SHEET NO. |
| 0362              | 14-F3000-12-BT | DUPAGE | 69                        | 42        |
| CONTRACT NO 61C31 |                |        | ILLINOIS FED. AID PROJECT |           |

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| Profile Elevation (ft) | SOIL AND ROCK DESCRIPTION  | Depth (ft) | Sample No. | SPT Values (blows/6 in) | Cu (tsf) | Moisture Content (%) | Profile Elevation (ft) | SOIL AND ROCK DESCRIPTION                                | Depth (ft) | Sample No. | SPT Values (blows/6 in) | Cu (tsf) | Moisture Content (%) |
|------------------------|--|------------|------------|-------------------------|----------|----------------------|------------------------|--|------------|------------|-------------------------|----------|----------------------|
| 784.5                  | 12-inch thick, black SILTY CLAY --TOPSOIL--<br>Medium stiff, black SILTY CLAY LOAM, trace gravel | 1          | 2          | 2                       | 0.66     | 26                   | 740.0                  | Soft to hard, gray CLAY to SILTY CLAY LOAM, trace gravel | 11         | 2          | 2                       | 1.72     | 23                   |
|                        |  | 2          | 1          | 2                       | 1.07     | 25                   |                        |  | 12         | 3          | 7                       | 1.00     | 15                   |
| 759.7                  | Loose to medium dense, gray, medium SAND, little gravel and silt lenses<br>--WET--               | 3          | 6          | 4                       | NP       | 19                   |                        |  | 13         | 5          | 3                       | 1.07     | 16                   |
| 755.0                  | Loose, gray SILT with sand lenses  | 4          | 4          | 3                       | NP       | 20                   |                        |  | 14         | 4          | 3                       | 0.41     | 14                   |
|                        |  | 5          | 1          | 4                       | NP       | 21                   |                        |  | 15         | 5          | 8                       | 0.98     | 12                   |
|                        |  | 6          | 3          | 4                       | NP       | 20                   |                        |  | 16         | 10         | 14                      | 4.59     | 17                   |
| 750.0                  | Medium dense to dense, gray, GRAVELLY SAND   | 7          | 7          | 11                      | NP       | 10                   |                        |  | 17         | 8          | 10                      |          |                      |
|                        |  | 8          | 8          | 14                      | NP       | 14                   |                        |  | 18         | 11         | 20                      |          |                      |
|                        |  | 9          | 13         | 12                      | NP       | 14                   |                        |  | 19         | 12         | 13                      |          |                      |
|                        |  | 10         | 10         | 21                      | NP       | 13                   |                        |  | 20         | 14         | 22                      |          |                      |
|                        |  | 11         | 13         | 12                      | NP       | 14                   |                        |  | 21         | 15         | 13                      |          |                      |
|                        |  | 12         | 14         | 25                      | NP       | 14                   |                        |  | 22         | 16         | 10                      |          |                      |
|                        |  | 13         | 13         | 12                      | NP       | 14                   |                        |  | 23         | 17         | 14                      |          |                      |
|                        |  | 14         | 10         | 21                      | NP       | 13                   |                        |  | 24         | 18         | 14                      |          |                      |
|                        |  | 15         | 10         | 22                      | NP       | 13                   |                        |  | 25         | 19         | 14                      |          |                      |

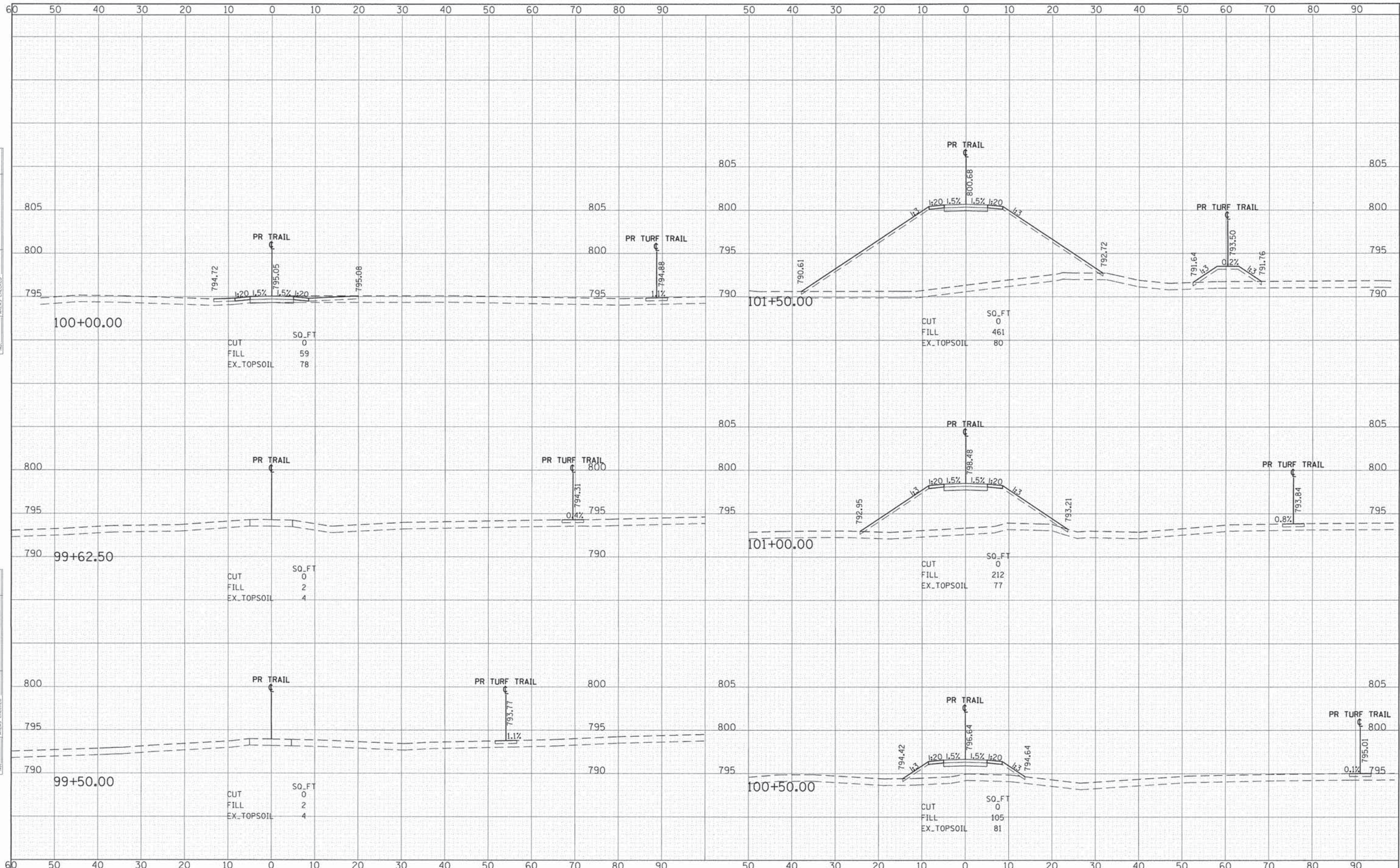
|                      |  |   |                   |
|----------------------|--|---|-------------------|
| <b>GENERAL NOTES</b> |  | <b>WATER LEVEL DATA</b>   |                   |
| Begin Drilling       | 03-31-2014   | Complete Drilling   | 04-01-2014        |
| Drilling Contractor  | WTS  | Drill Rig   | Diedrich D-50 ATV |
| Driller              | K&K  | Logger  | A. Tomaras        |
| Checked by           | B. Wilson  | Time After Drilling   | NA                |
| Drilling Method      | 3.25" HSA to 45', mud rotary thereafter; boring backfilled upon completion | Depth to Water  | NA                |
|                      |  | The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual. |                   |
| While Drilling       | 5.75 ft  | At Completion of Drilling   | 6.00 ft           |

| Profile Elevation (ft) | SOIL AND ROCK DESCRIPTION  | Depth (ft) | Sample No. | SPT Values (blows/6 in) | Cu (tsf) | Moisture Content (%) | Profile Elevation (ft) | SOIL AND ROCK DESCRIPTION                         | Depth (ft) | Sample No. | SPT Values (blows/6 in) | Cu (tsf) | Moisture Content (%) |
|------------------------|--|------------|------------|-------------------------|----------|----------------------|------------------------|---|------------|------------|-------------------------|----------|----------------------|
| 761.6                  | 12-inch thick, black SILTY LOAM --TOPSOIL--<br>Medium stiff to stiff, brown and gray CLAY to SILTY CLAY LOAM                               | 1          | 3          | 3                       | 1.39     | 29                   | 728.6                  | Stiff, gray SILTY CLAY LOAM, trace to some gravel | 11         | 3          | 3                       | 1.75     | 8                    |
|                        |  | 2          | 2          | 2                       | 0.74     | 18                   |                        |   | 12         | 8          | 6                       | NP       | 12                   |
| 757.1                  | Loose, gray SANDY LOAM, trace gravel   | 3          | 3          | 3                       | NP       | 17                   |                        |   | 13         | 6          | 8                       | 1.75     | 8                    |
| 755.8                  | Loose to medium dense, gray SILTY LOAM with sand seams<br>--WET--<br>--%Gravel=1.9--<br>--%Sand=26.6--<br>--%Silt=57.5--<br>--%Clay=14.0-- | 4          | 2          | 2                       | NP       | 16                   |                        |   | 14         | 6          | 9                       | 1.15     | 13                   |
|                        |  | 5          | 4          | 4                       | NP       | 21                   |                        |   | 15         | 9          | 4                       | NP       | 11                   |
| 749.6                  | Very stiff, gray SILTY CLAY, trace gravel  | 6          | 4          | 5                       | 2.13     | 23                   |                        |   | 16         | 11         | 10                      | NP       | 8                    |
| 747.1                  | Medium dense, gray GRAVELLY SAND   | 7          | 6          | 6                       | NP       | 9                    |                        |   | 17         | 9          | 7                       | NP       | 6                    |
|                        |  | 8          | 15         | 11                      | NP       | 8                    |                        |   | 18         | 12         | 14                      | NP       | 9                    |
|                        |  | 9          | 13         | 9                       | NP       | 6                    |                        |   | 19         | 11         | 27                      | 6.31     | 9                    |
|                        |  | 10         | 12         | 14                      | NP       | 9                    |                        |   | 20         | 16         | 11                      |          |                      |
|                        |  | 11         | 14         | 10                      | NP       | 9                    |                        |   | 21         | 17         | 27                      |          |                      |

|                      |  |   |                   |
|----------------------|--|---|-------------------|
| <b>GENERAL NOTES</b> |  | <b>WATER LEVEL DATA</b>   |                   |
| Begin Drilling       | 03-31-2014   | Complete Drilling   | 03-31-2014        |
| Drilling Contractor  | WTS  | Drill Rig   | Diedrich D-25 ATV |
| Driller              | N&J  | Logger  | F. Bozga          |
| Checked by           | B. Wilson  | Time After Drilling   | NA                |
| Drilling Method      | 2.25" HSA to 10', mud rotary thereafter; boring backfilled upon completion | Depth to Water  | NA                |
|                      |  | The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual. |                   |
| While Drilling       | 7.00 ft  | At Completion of Drilling   | 6.00 ft           |

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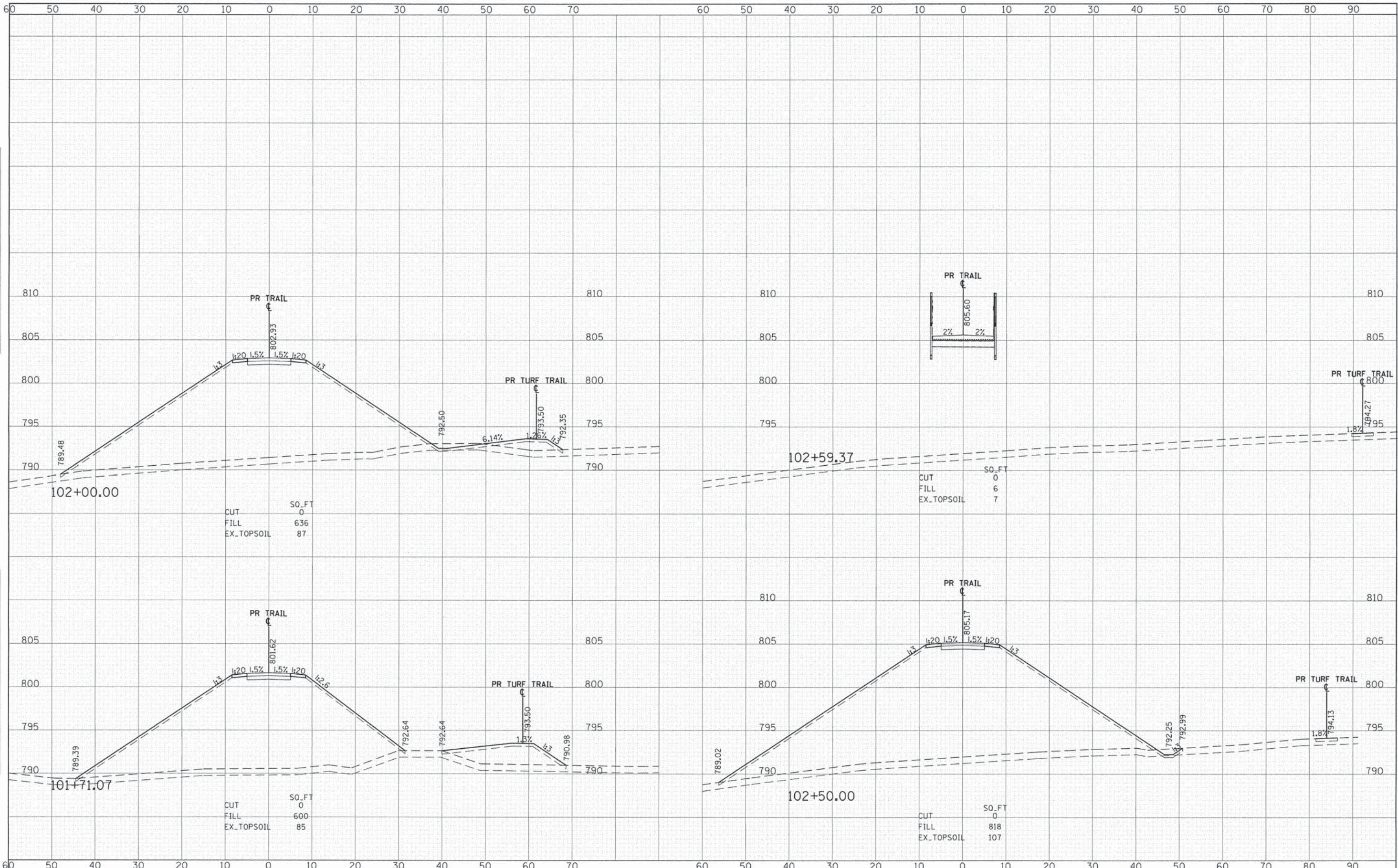
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| BY        |  |
| DESIGNED  |  |
| DRAWN     |  |
| CHECKED   |  |
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| FILE NAME |  |
| USER NAME |  |
| DESIGNED  |  |
| DRAWN     |  |
| CHECKED   |  |
| DATE      |  |

|           |  |
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| DATE      |  |
| BY        |  |
| DESIGNED  |  |
| DRAWN     |  |
| CHECKED   |  |
| DATE      |  |
| FILE NAME |  |
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| DESIGNED  |  |
| DRAWN     |  |
| CHECKED   |  |
| DATE      |  |

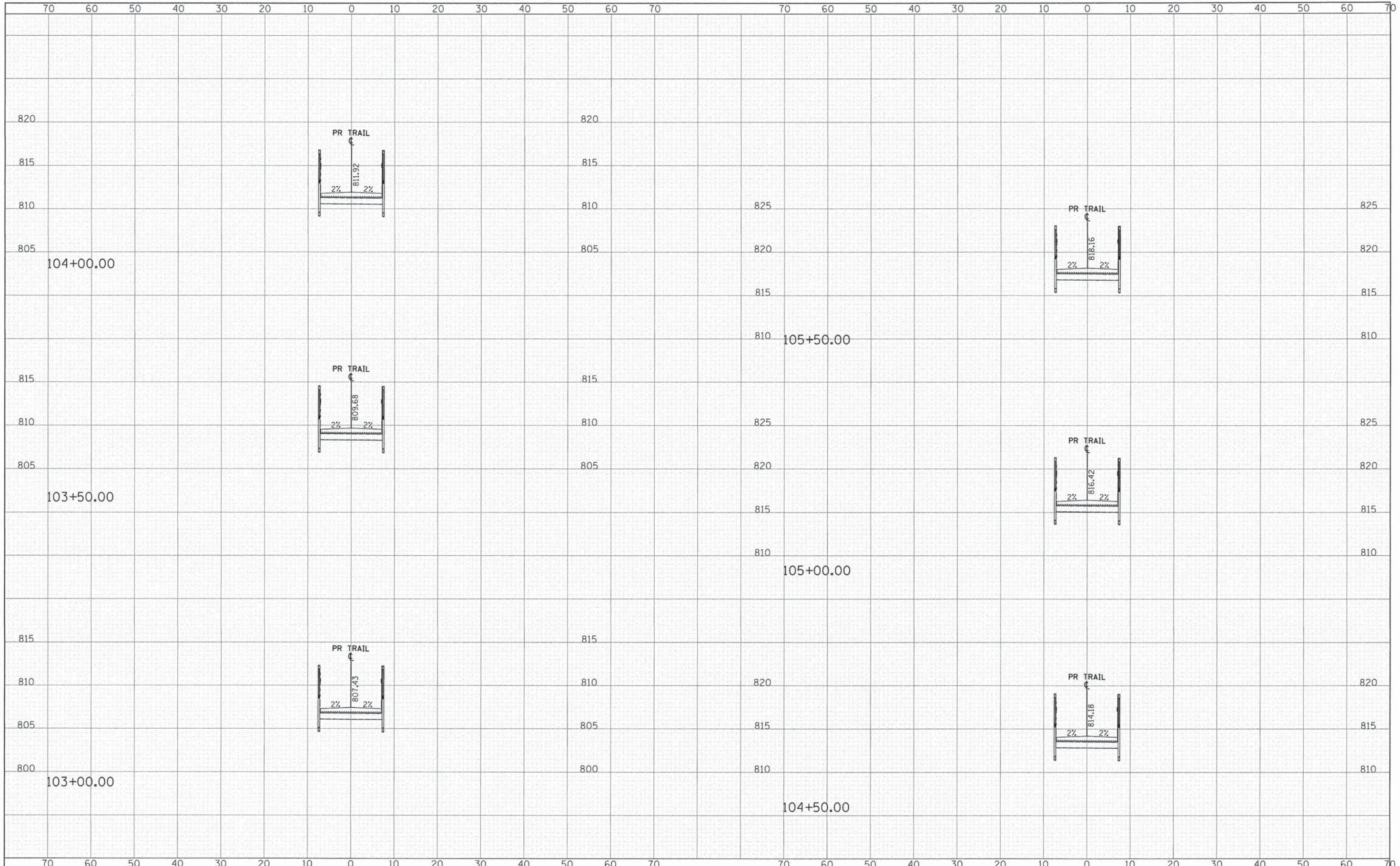


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|--------------|----------|------|
| FINAL SURVEY | DESIGNED | DATE |
| NOTE BOOK    | PLOTTED  |      |
| AREAS        | TEMPLATE |      |
| CHECKED      | AREAS    |      |

|                 |          |      |
|-----------------|----------|------|
| ORIGINAL SURVEY | DESIGNED | DATE |
| NOTE BOOK       | PLOTTED  |      |
| AREAS           | TEMPLATE |      |
| CHECKED         | AREAS    |      |



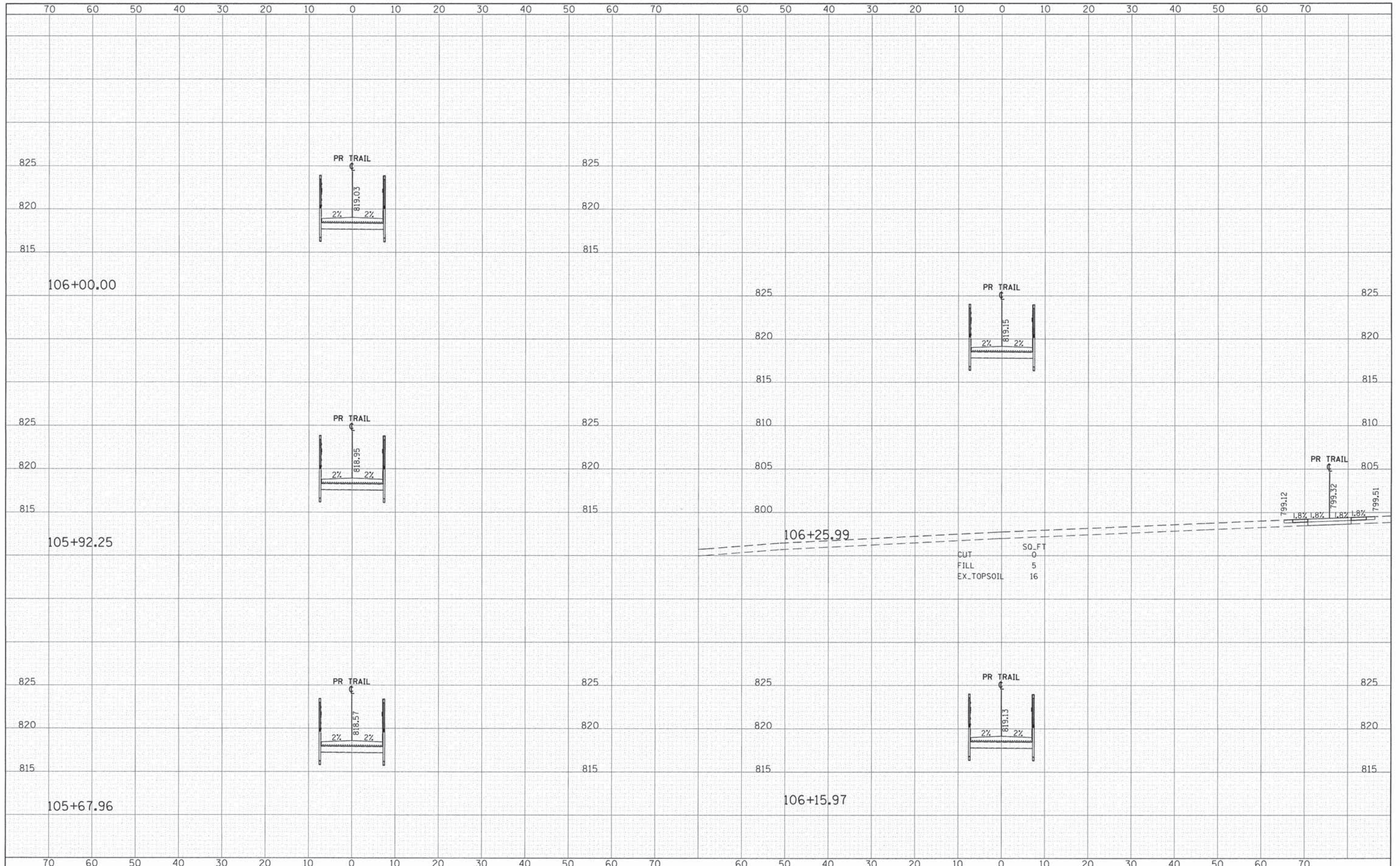




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|------------------|---------------|------|
| FINAL SURVEY NO. | SURVEYED      | DATE |
| NOTE BOOK NO.    | PLOTTED       |      |
|                  | TEMPLATE      |      |
|                  | AREAS CHECKED |      |

|                     |               |      |
|---------------------|---------------|------|
| ORIGINAL SURVEY NO. | SURVEYED      | DATE |
| NOTE BOOK NO.       | PLOTTED       |      |
|                     | TEMPLATE      |      |
|                     | AREAS CHECKED |      |





|              |           |
|--------------|-----------|
| DATE         |           |
| BY           |           |
| FINAL SURVEY | SUBMITTED |
| NOTE BOOK    | PLOTTED   |
| NO.          | TEMPLATE  |
|              | AREAS     |
|              | CHECKED   |

|                 |           |
|-----------------|-----------|
| DATE            |           |
| BY              |           |
| ORIGINAL SURVEY | SUBMITTED |
| NOTE BOOK       | PLOTTED   |
| NO.             | TEMPLATE  |
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|                 | CHECKED   |

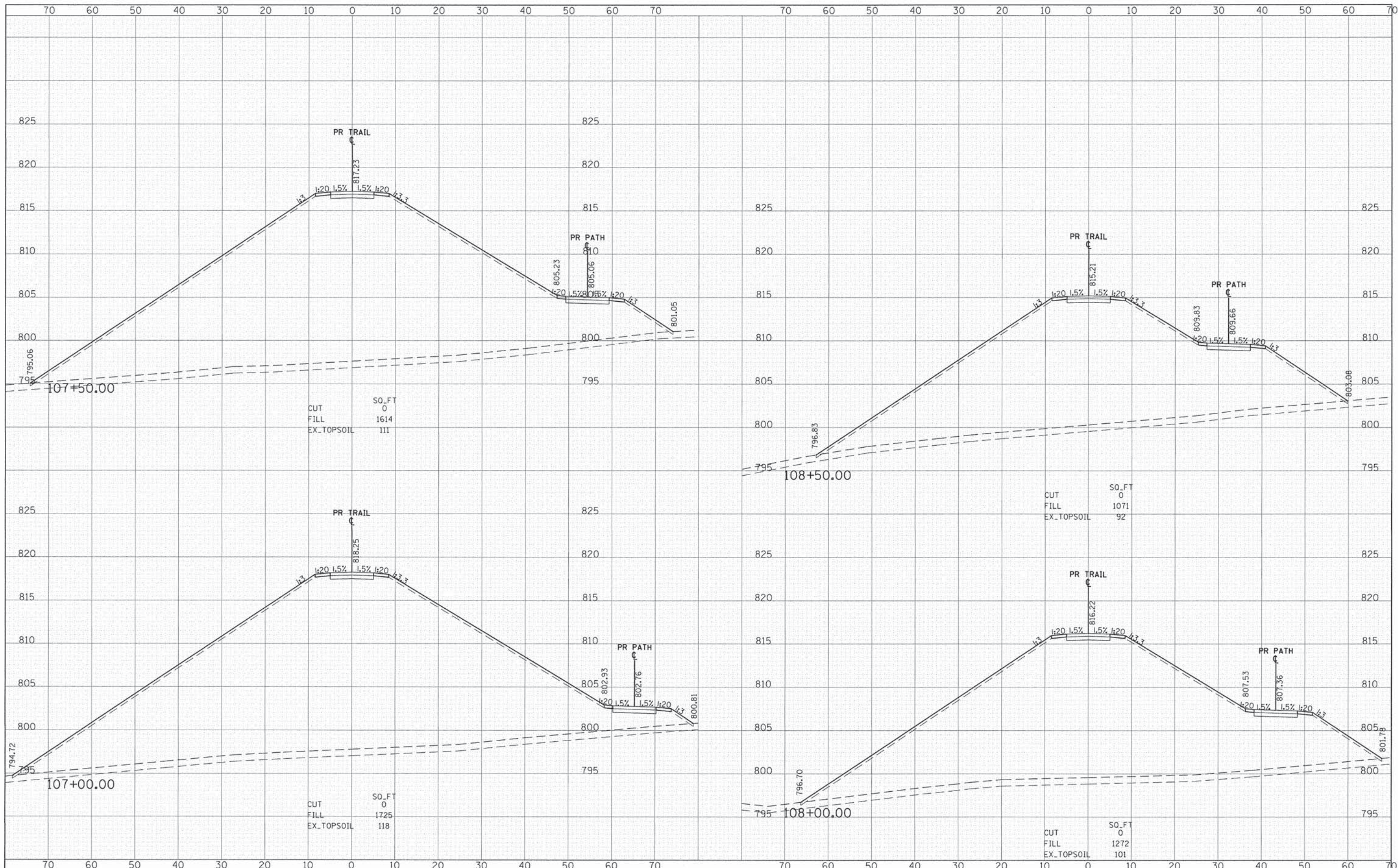




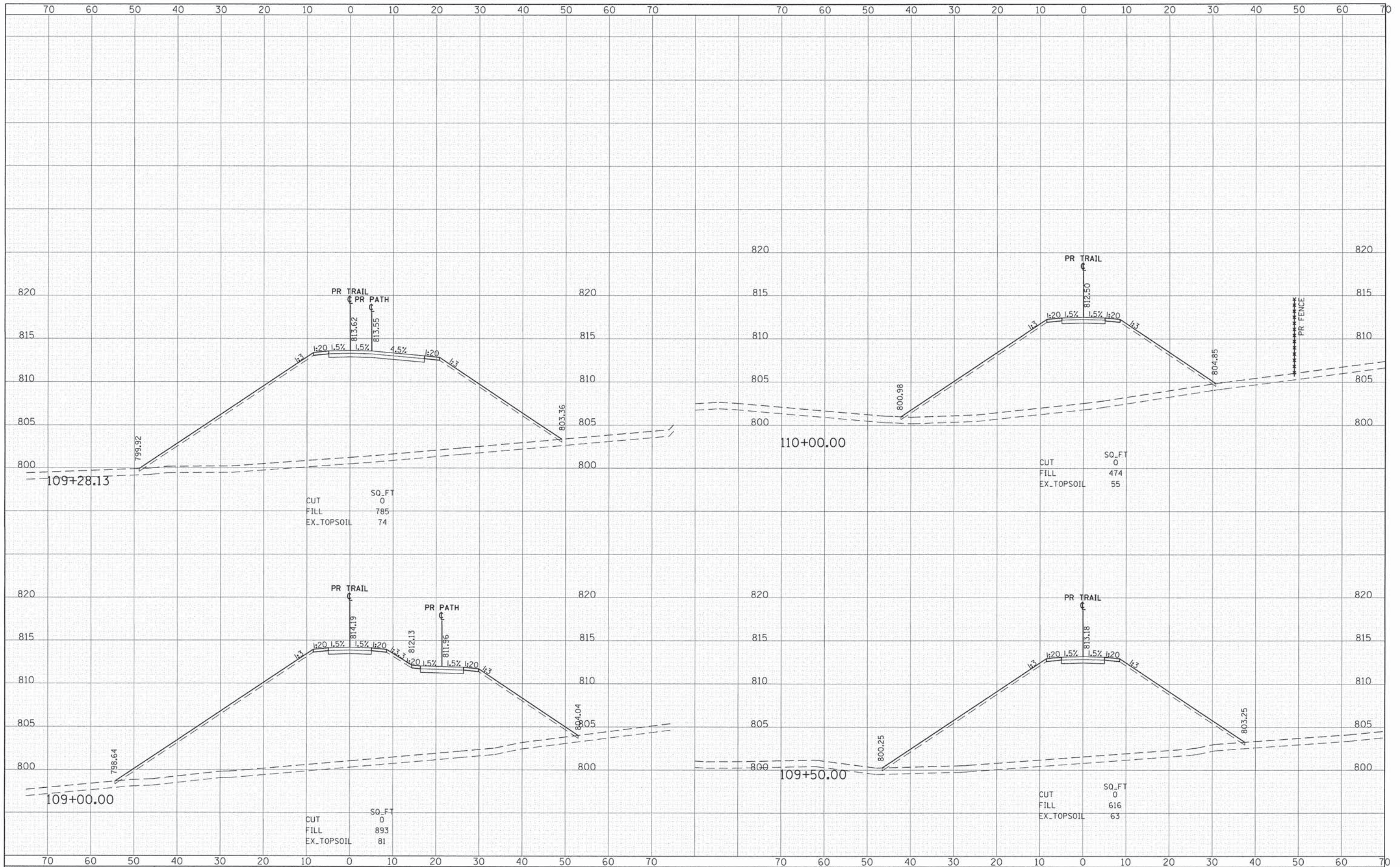


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| FINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK    | PLOTTED  |      |
| NO.          | TEMPLATE |      |
|              | AREAS    |      |
|              | CHECKED  |      |

|                 |          |      |
|-----------------|----------|------|
| ORIGINAL SURVEY | SURVEYED | DATE |
| NOTE BOOK       | PLOTTED  |      |
| NO.             | TEMPLATE |      |
|                 | AREAS    |      |
|                 | CHECKED  |      |







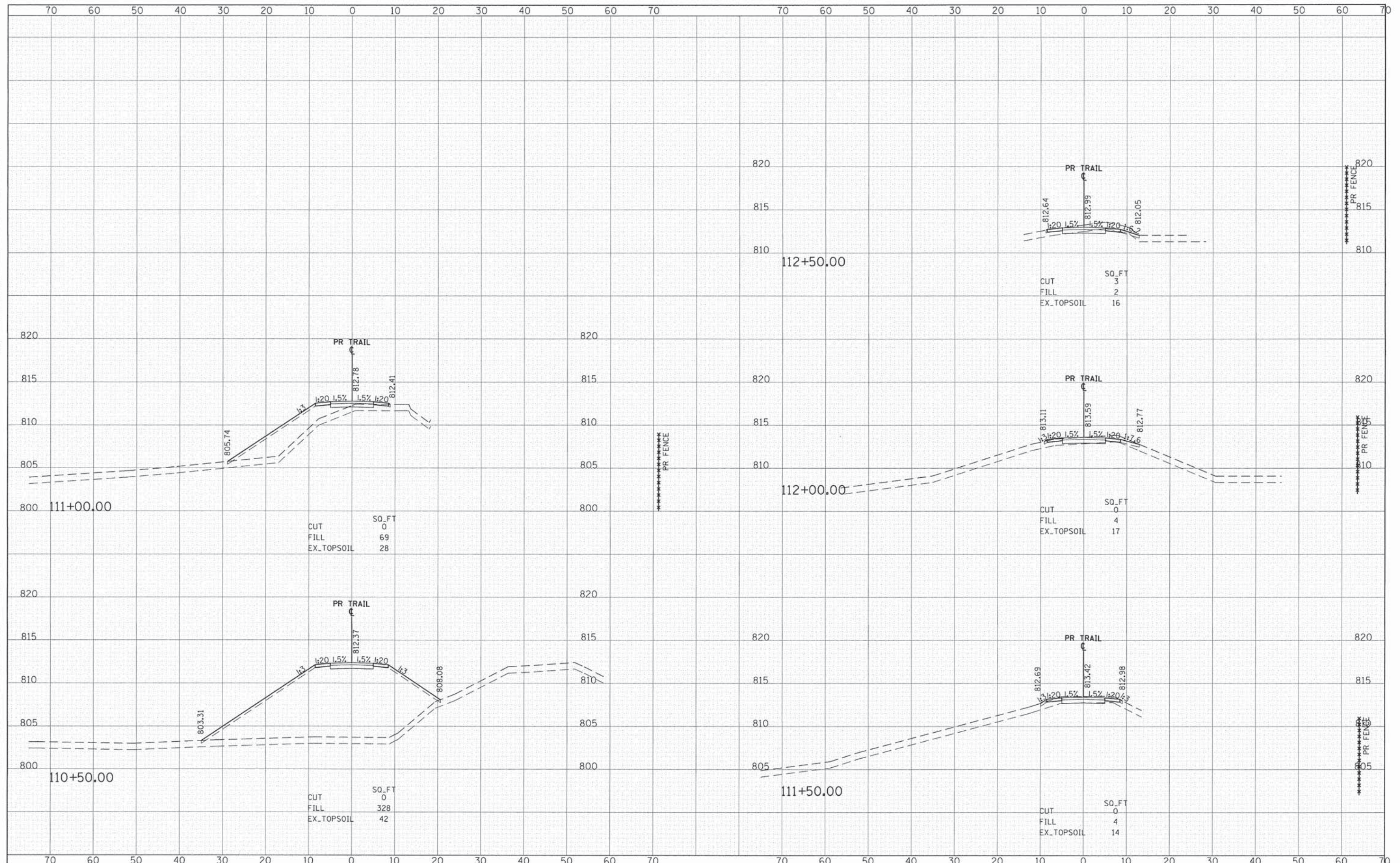
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| NOTE BOOK    | PLOTTED       |
| NO.          | TEMPLATE      |
|              | AREAS CHECKED |

|                 |               |
|-----------------|---------------|
| DATE            |               |
| BY              |               |
| ORIGINAL SURVEY | SURVEYED      |
| NOTE BOOK       | PLOTTED       |
| NO.             | TEMPLATE      |
|                 | AREAS CHECKED |



|              |               |      |
|--------------|---------------|------|
| FINAL SURVEY | SURVEYED      | DATE |
| NOTE BOOK    | PLOTTED       |      |
| NO.          | TEMPLATE      |      |
|              | AREAS CHECKED |      |

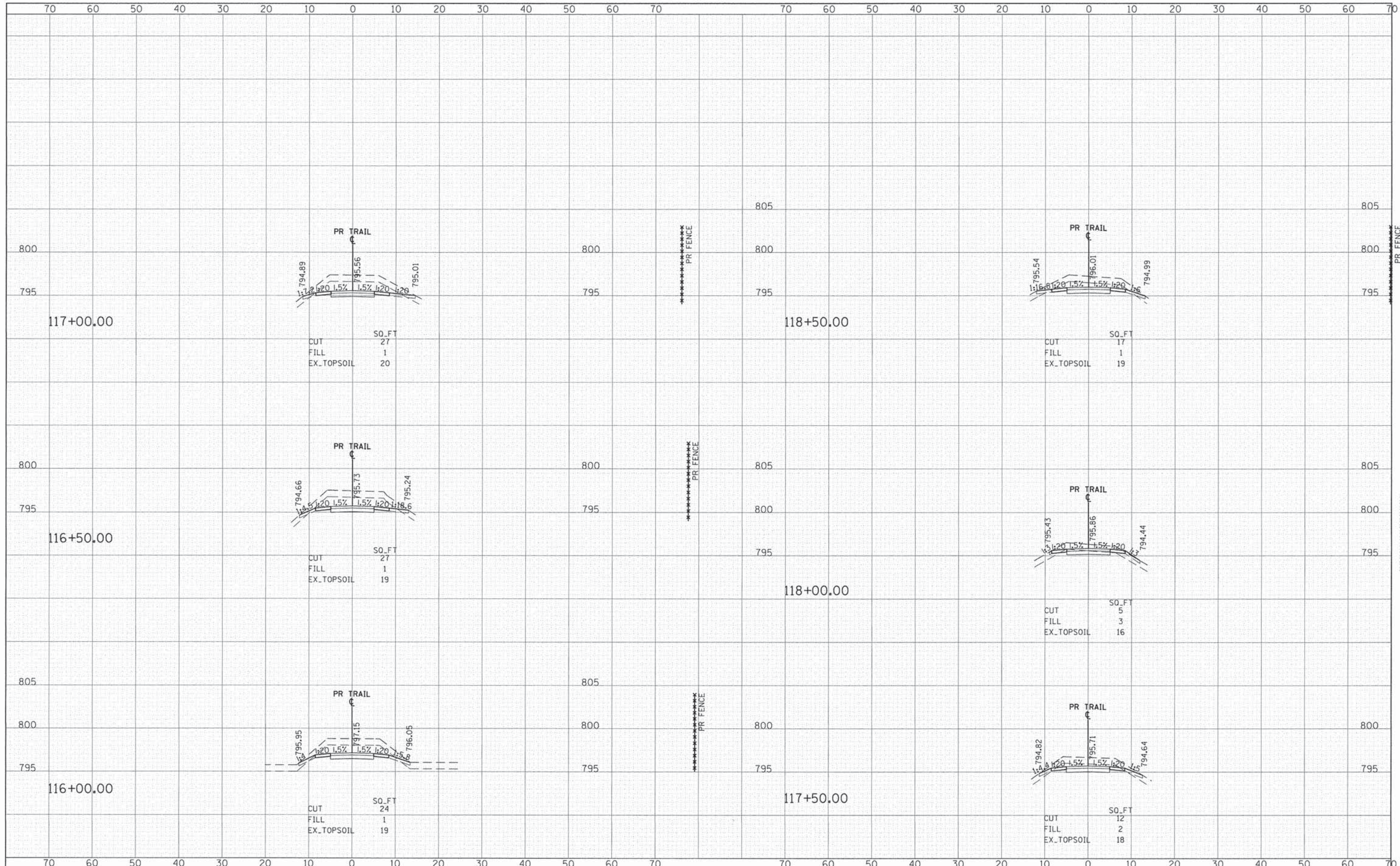
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|-----------------|---------------|------|
| ORIGINAL SURVEY | SURVEYED      | DATE |
| NOTE BOOK       | PLOTTED       |      |
| NO.             | TEMPLATE      |      |
|                 | AREAS CHECKED |      |







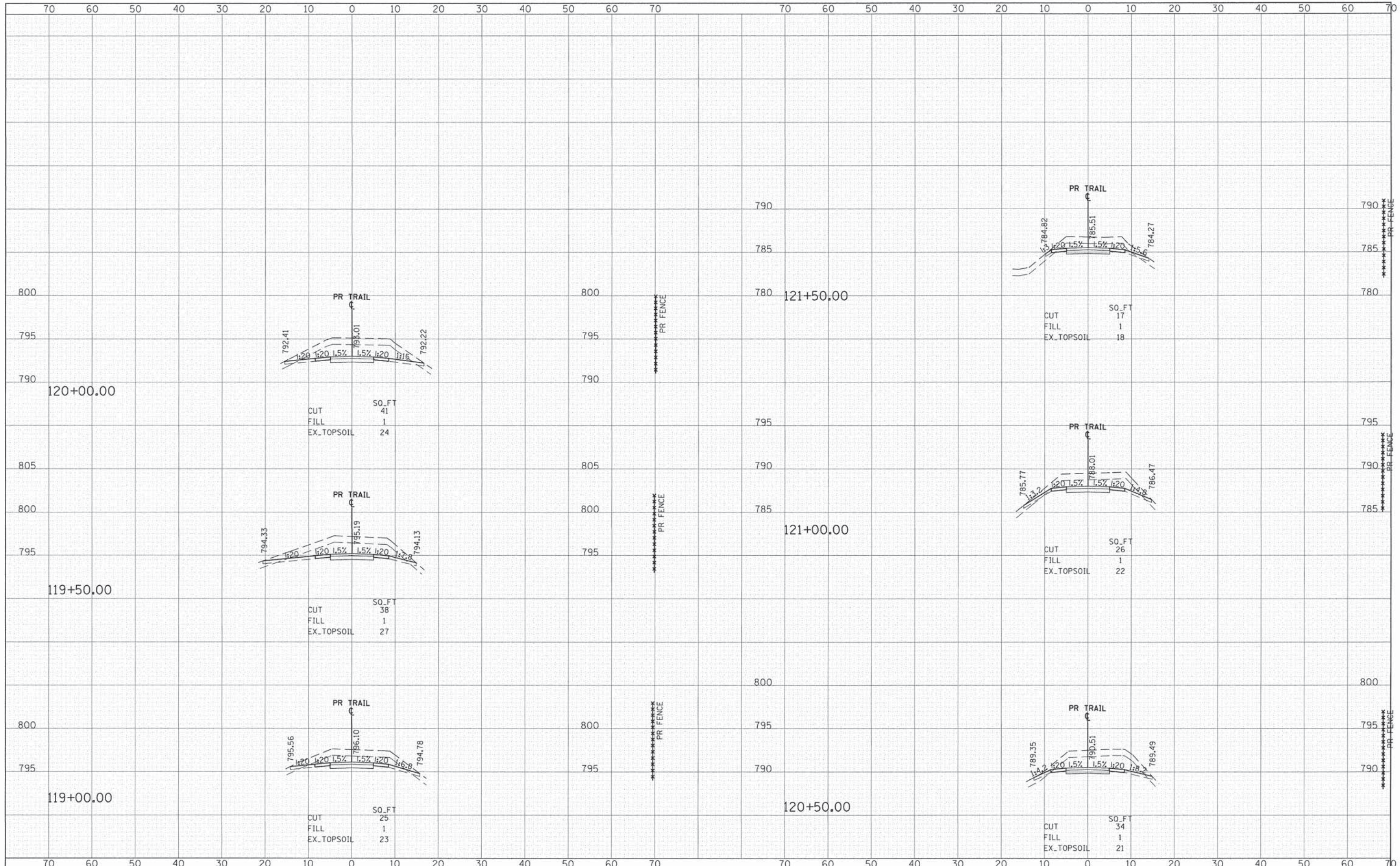




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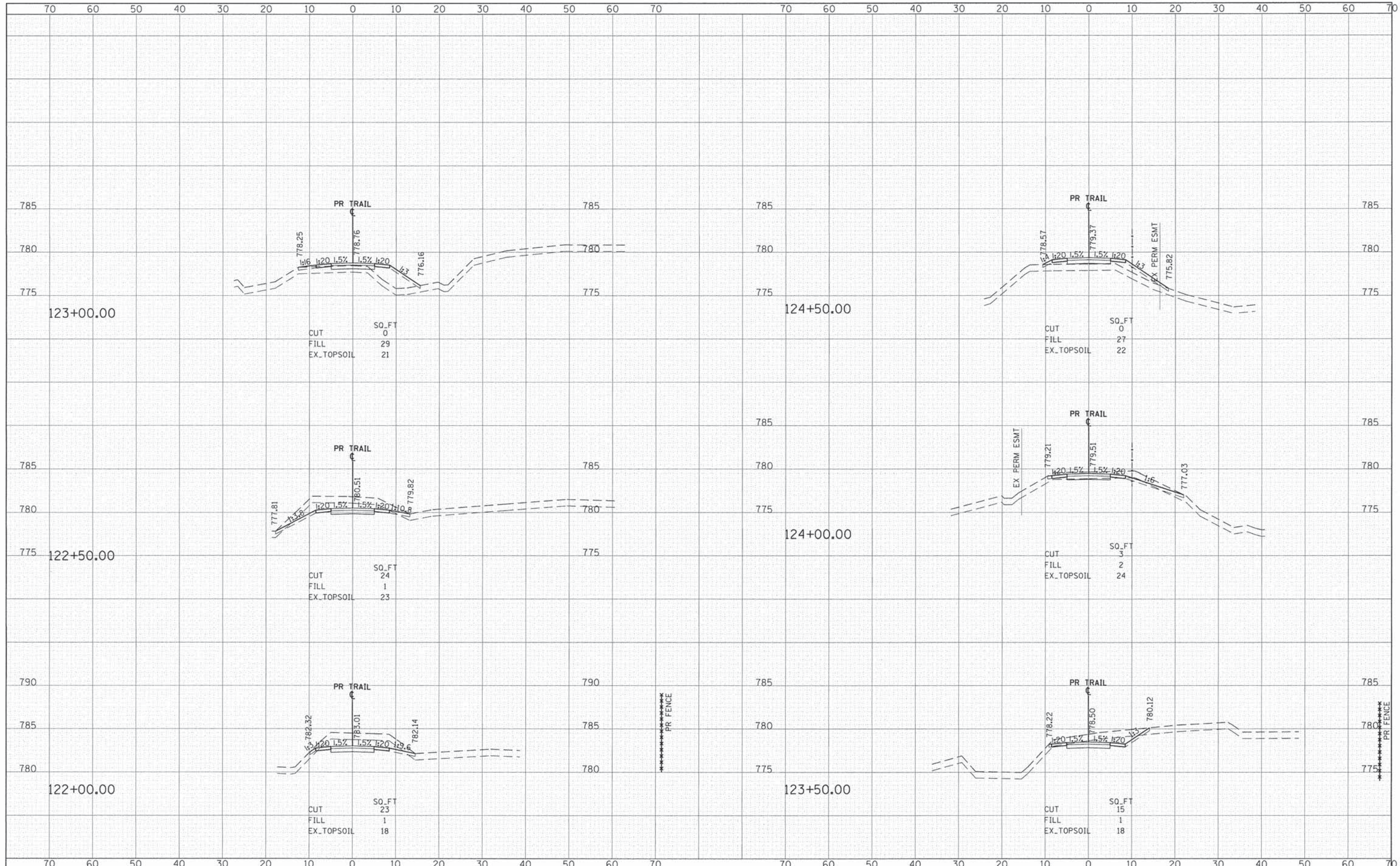




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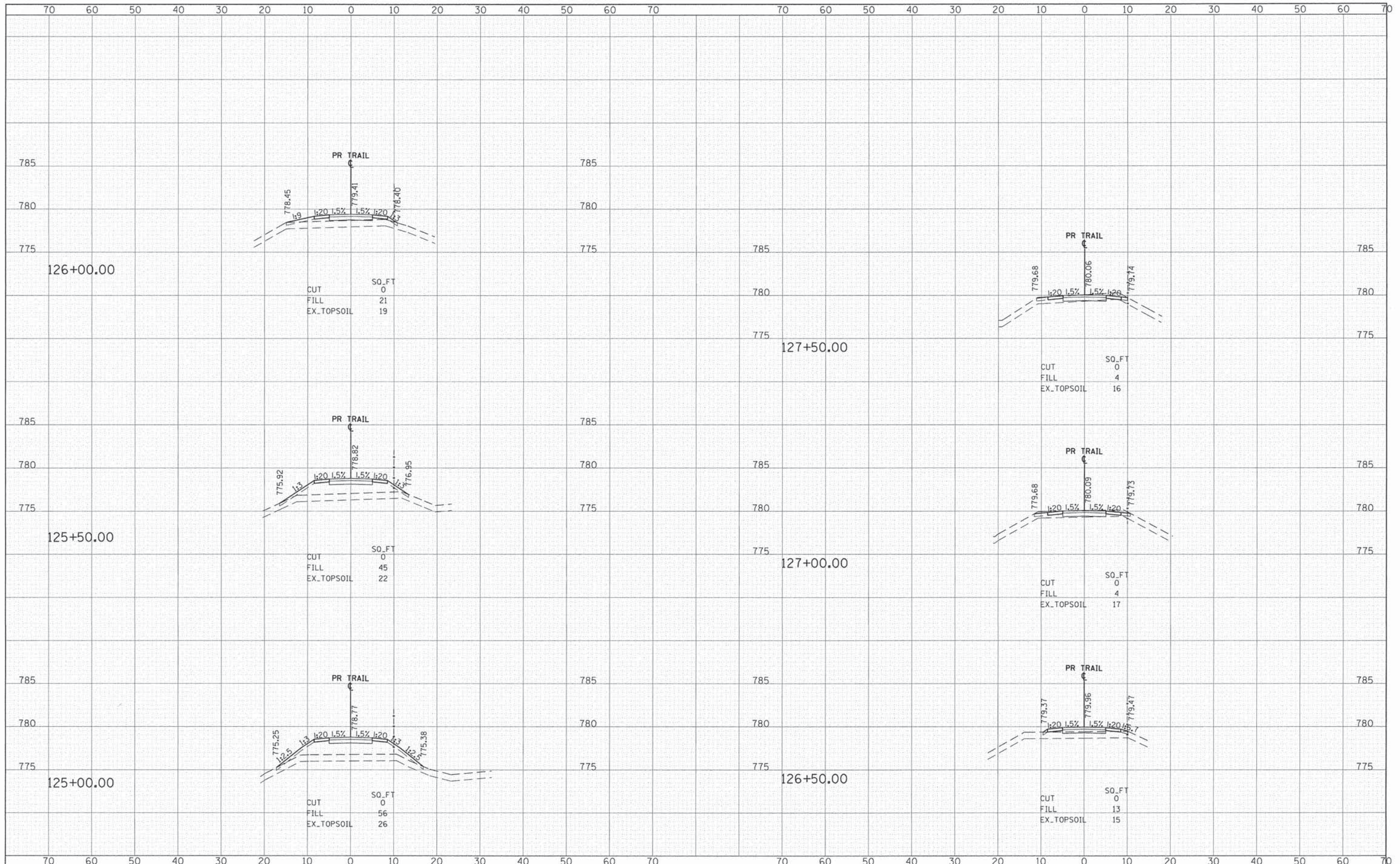
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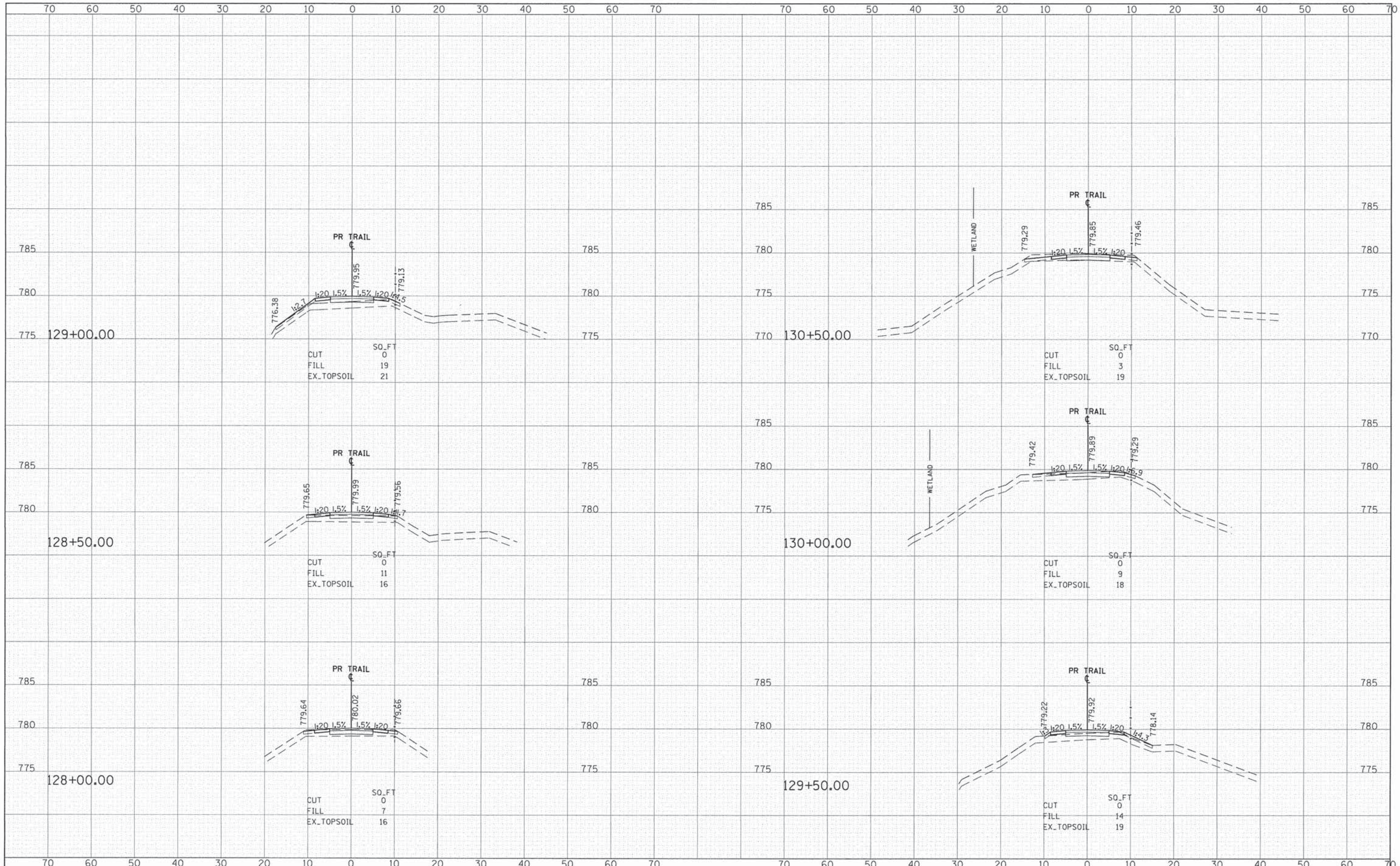


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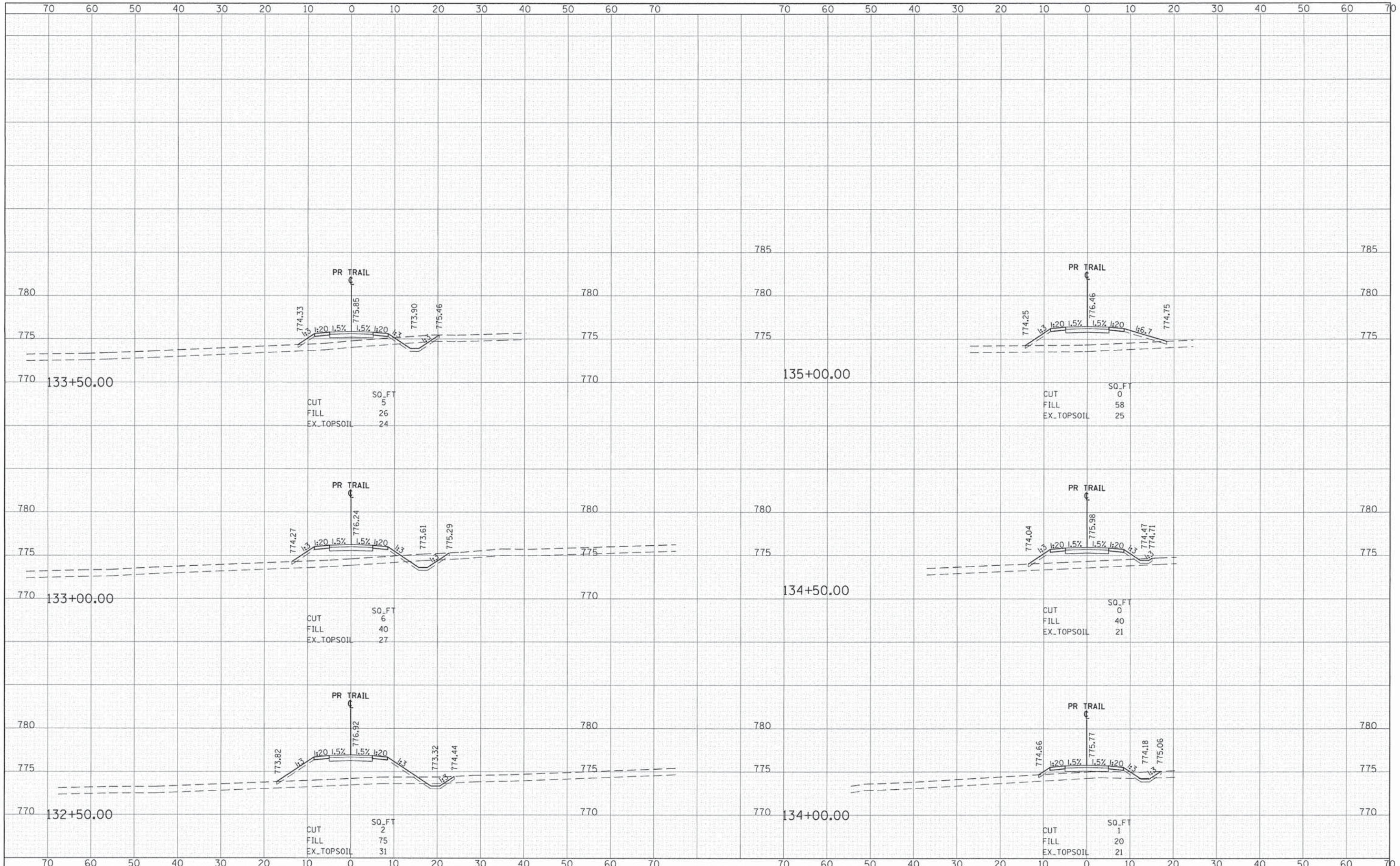
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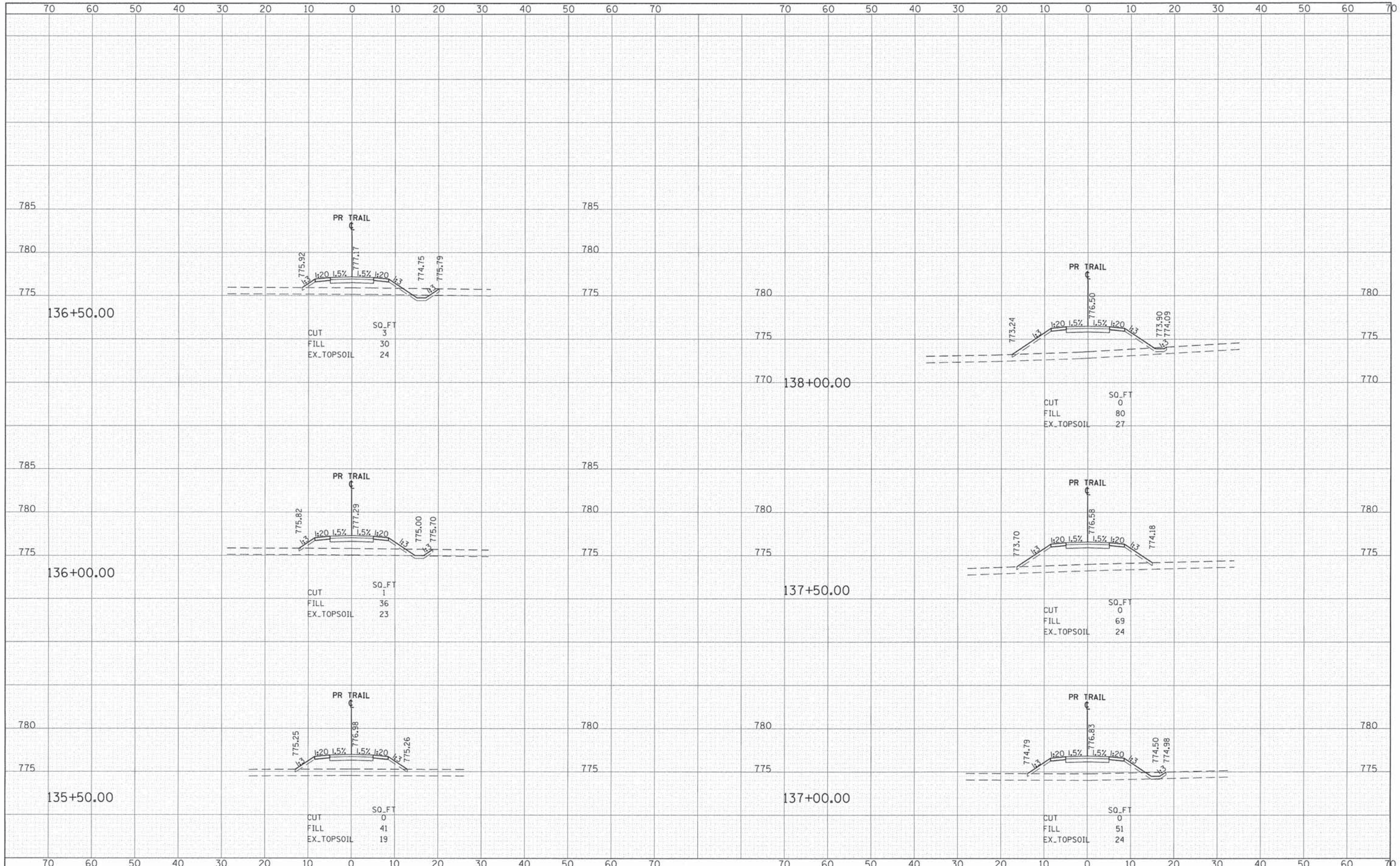
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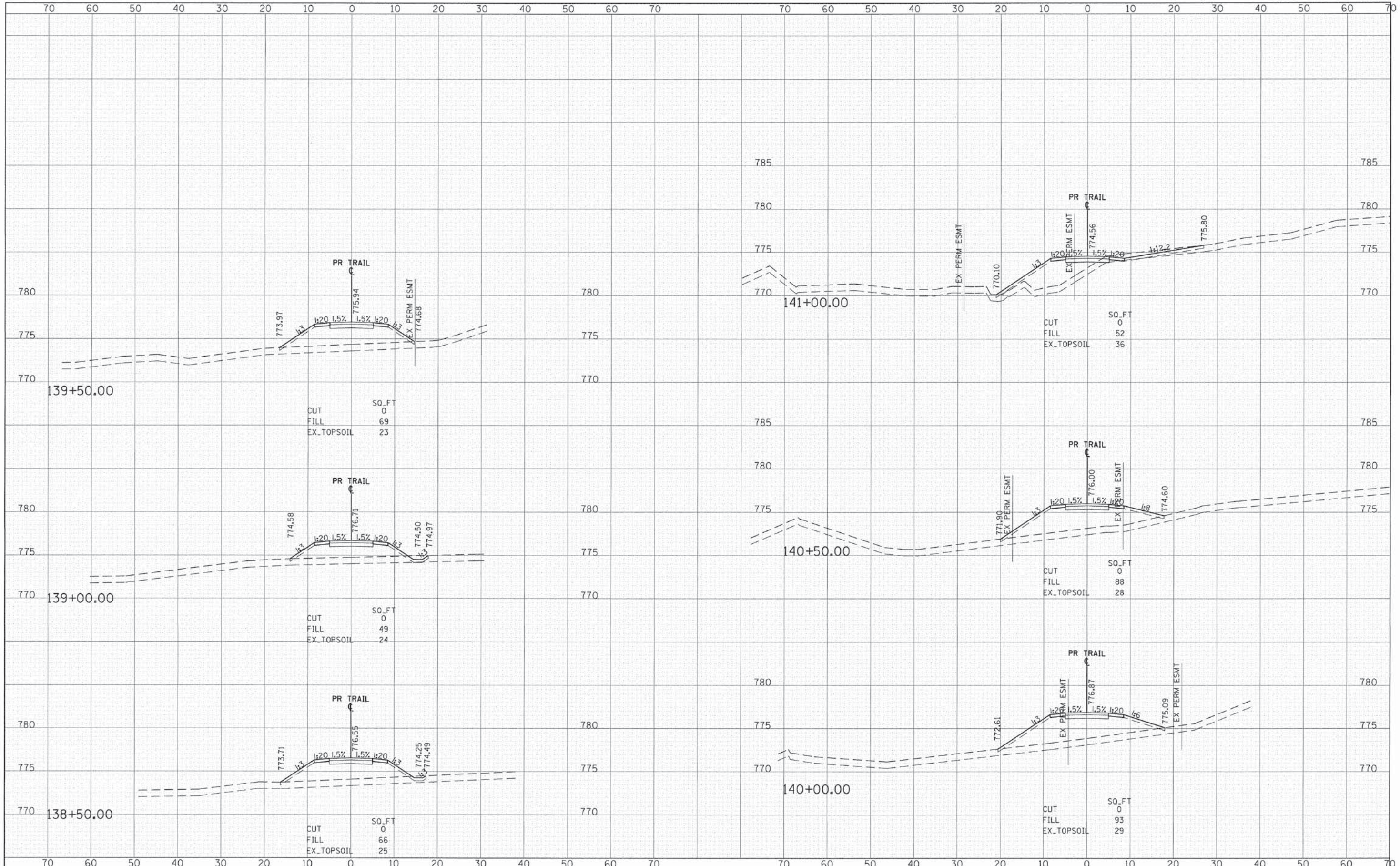


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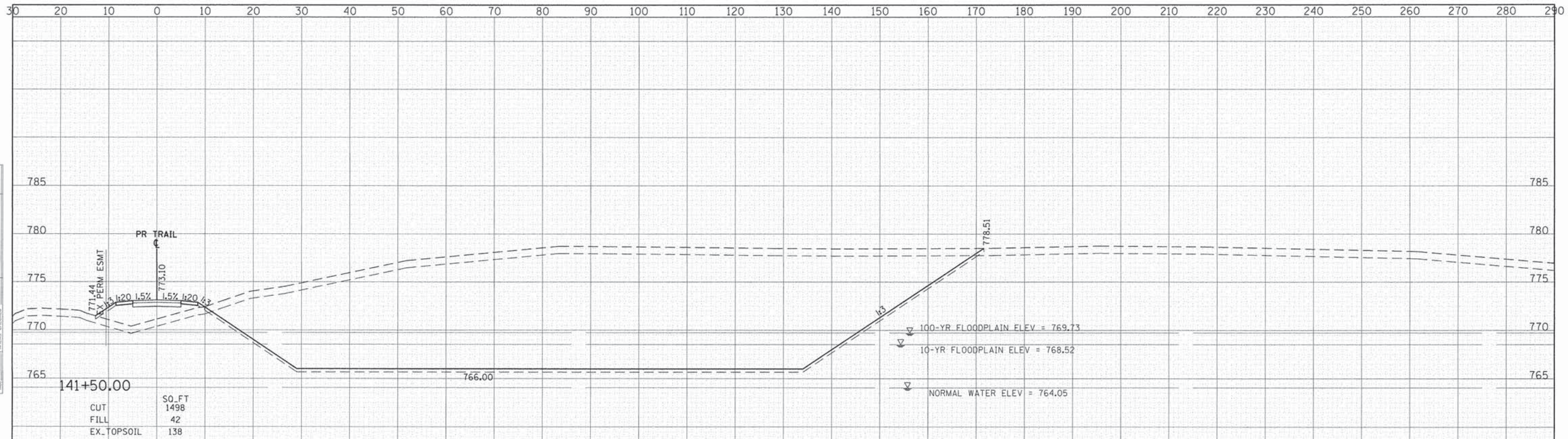




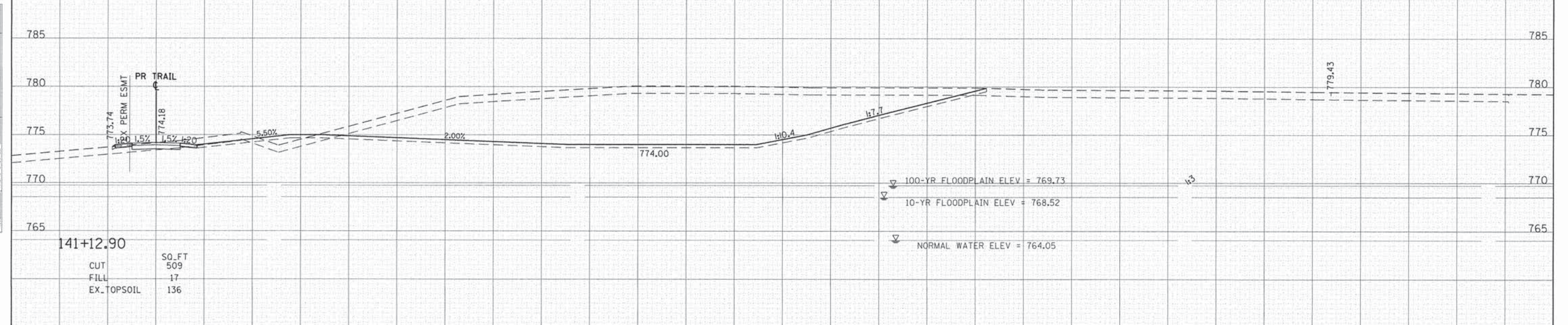
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|             |       |
|-------------|-------|
| 141+50.00   | 50 FT |
| CUT         | 1498  |
| FILL        | 42    |
| EX. TOPSOIL | 138   |



|             |       |
|-------------|-------|
| 141+12.90   | 50 FT |
| CUT         | 509   |
| FILL        | 17    |
| EX. TOPSOIL | 136   |









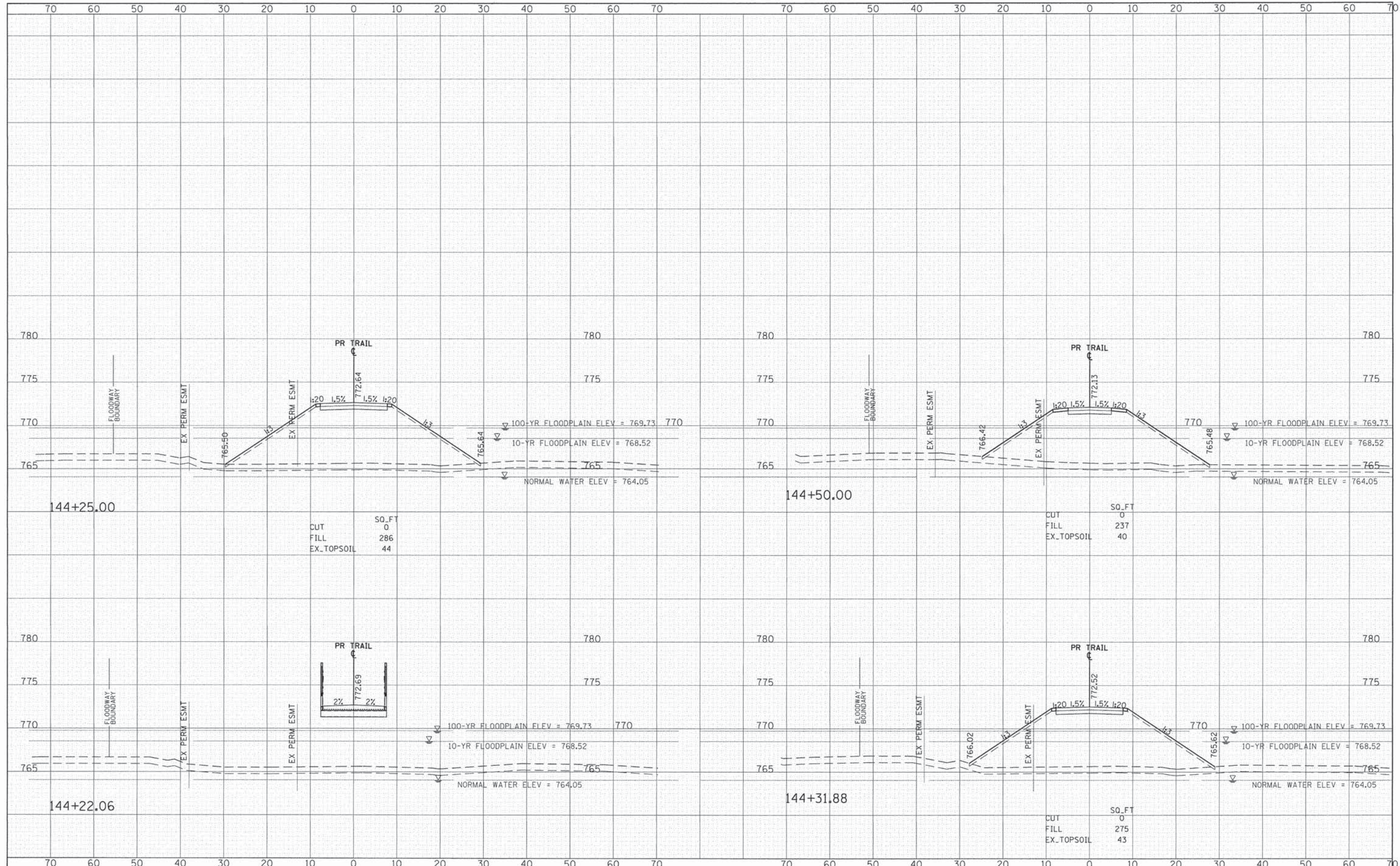






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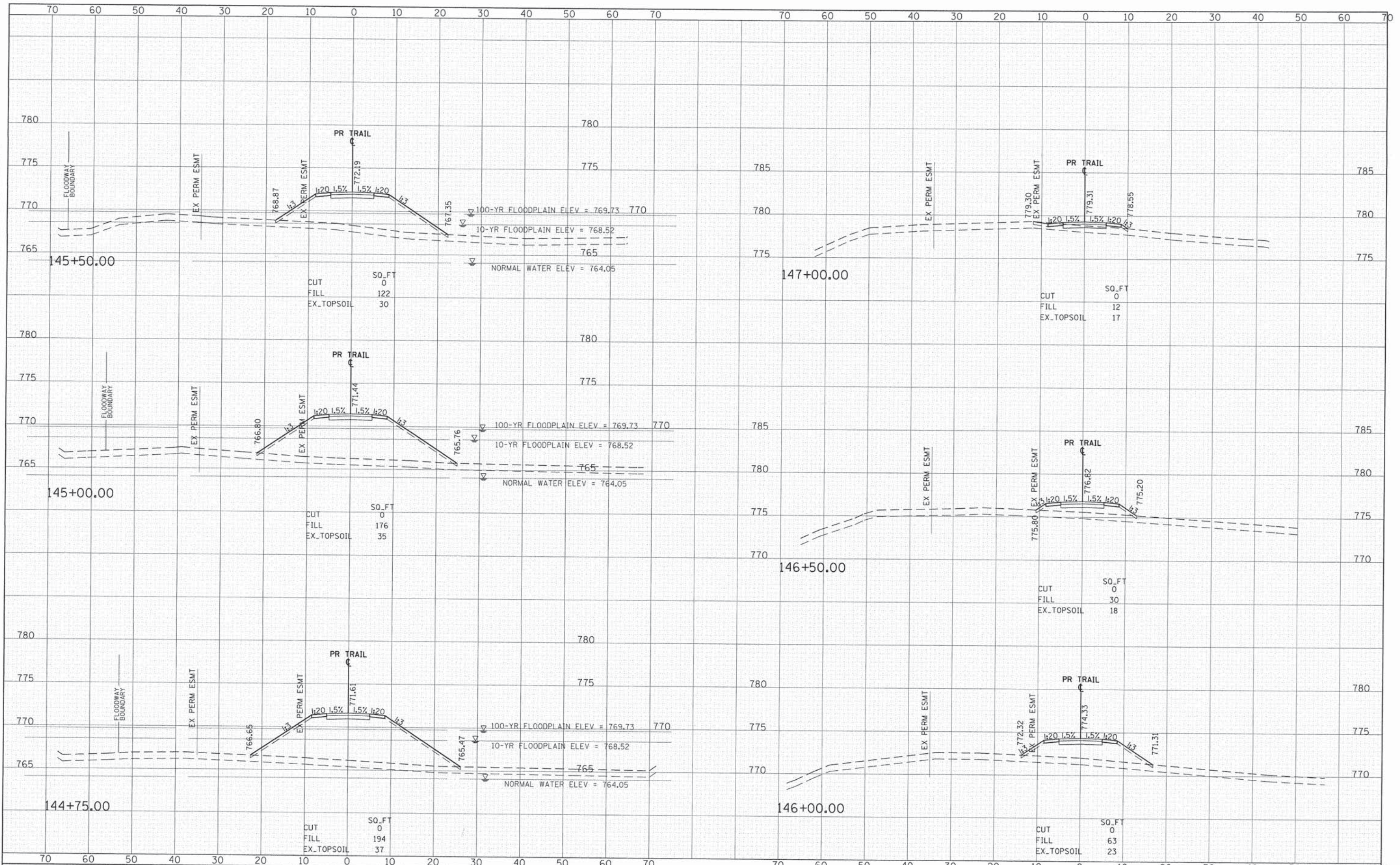


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|--|-------------------------|-------------------|-----------|---|--|---------------------------|-----------------------|----------------------------------|--------------|-----------|
| FILE NAME =  | USER NAME = bshaaf11ger | DESIGNED - MLB    | REVISED - | <b>STATE OF ILLINOIS</b><br><b>DEPARTMENT OF TRANSPORTATION</b> | <b>COUNTY FARM ROAD BRIDGE AND TRIAL IMPROVEMENTS</b><br><b>CROSS SECTIONS</b> | F.A.U. RTE.               | SECTION               | COUNTY                           | TOTAL SHEETS | SHEET NO. |
| G:\CH13\0855\Road\Sheets\38895-SHT-COUNTYFARM\KSEC.dgn |                         | DRAWN - MLB       | REVISED - |   |  | 14-F3000-12-BT            | DUPAGE                | 69                               | 66           |           |
| PLOT SCALE = 1/8"=1'-0"                                |                         | CHECKED - DWB     | REVISED - |   |  | CONTRACT NO. 61C31        |                       |                                  |              |           |
| PLOT DATE = 12/16/2015                                 |                         | DATE - 12/17/2015 | REVISED - |   |  | ILLINOIS FED. AID PROJECT |                       |                                  |              |           |
|  |                         |                   |           |   |  | SCALE: 1"=20'             | SHEET 66 OF 69 SHEETS | STA. 144+22.06 TO STA. 144+50.00 |              |           |



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| FILE NAME =   | USER NAME = bshaef11gr | DESIGNED - MLB   | REVISED - |
| G:\CHI3\0055\Road\Sheets\30855-SHT-COUNTY FARM\KSEC.dgn |                        | DRAWN - MLB      | REVISED - |
|   |                        | CHECKED - DWB    | REVISED - |
|   |                        | DATE = 12/7/2015 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**COUNTY FARM ROAD BRIDGE AND TRIAL IMPROVEMENTS  
CROSS SECTIONS**

SCALE: 1"=20'    SHEET 67 OF 69 SHEETS    STA. 144+75.00 TO STA. 147+00.00

|             |                |        |                           |           |
|-------------|----------------|--------|---------------------------|-----------|
| F.A.U. RTE. | SECTION        | COUNTY | TOTAL SHEETS              | SHEET NO. |
|             | 14-F3000-12-BT | DuPAGE | 69                        | 67        |
|             |                |        | CONTRACT NO.              | 61C31     |
|             |                |        | ILLINOIS FED. AID PROJECT |           |

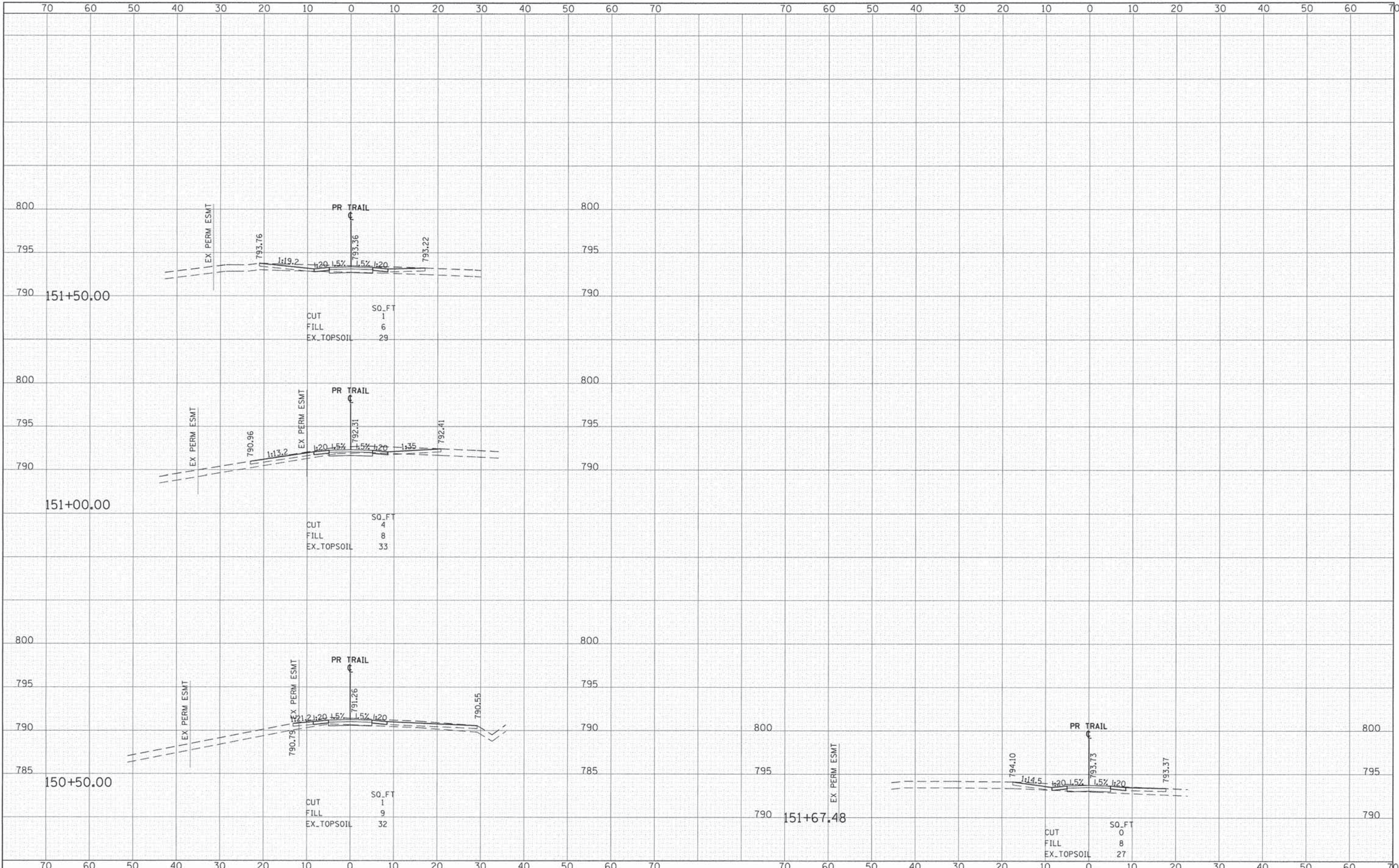






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| FILE NAME =  | USER NAME = bshae11ger | DESIGNED - | MLB       | REVISED - |  | <b>STATE OF ILLINOIS</b><br><b>DEPARTMENT OF TRANSPORTATION</b> | <b>COUNTY FARM ROAD BRIDGE AND TRAIL IMPROVEMENTS</b><br><b>CROSS SECTIONS</b> |                       |                                  |                | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
| Gu:\CH13\0855\Road\Sheets\38895-SHT-COUNTY FARM\KSEC.dgn |                        | DRAWN -    | MLB       | REVISED - |  |   | SCALE: 1"=20'  | SHEET 69 OF 69 SHEETS | STA. 150+50.00 TO STA. 151+67.48 | 14-F3000-12-BT | DuPAGE      | 69      | 69     |                           |           |
| PLOT SCALE = 18,000' / 1"                                |                        | CHECKED -  | DWB       | REVISED - |  |   |  |                       |                                  |                |             |         |        | CONTRACT NO. 61C31        |           |
| PLOT DATE = 12/16/2015                                   |                        | DATE -     | 12/7/2015 | REVISED - |  |   |  |                       |                                  |                |             |         |        | ILLINOIS FED. AID PROJECT |           |