

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROPOSED HIGHWAY PLANS

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FAU ROUTE 0292 CATON FARM ROAD
AT FAU 0326 ESSINGTON ROAD
SECTION 11-00446-00-CH
PROJECT M-4003(254)
INTERSECTION IMPROVEMENT
TRAFFIC SIGNAL INSTALLATION AND LIGHTING
CITY OF JOLIET
WILL COUNTY
C-91-127-14

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	1
		ILLINOIS	CONTRACT NO. 61C29	



LOCATION OF SECTION INDICATED THUS: - [rectangle] -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

APPROVED December 3 2015
James R. Tinspa
 ACTING DIRECTOR OF ENGINEERING AND COMMUNITY DEVELOPMENT

PASSED DECEMBER 16, 2015 20
Christopher Heit
 DISTRICT ONE ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID
 BASED ON LIMITED
 REVIEW December 17 2015
John Furtman
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

**PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS**

FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E., PTOE (847) 705-4021
 CONSULTANT ENGINEER: JONATHAN R. VANA, P.E. CIVILTECH ENGINEERING, INC.

PROJECT LENGTH:

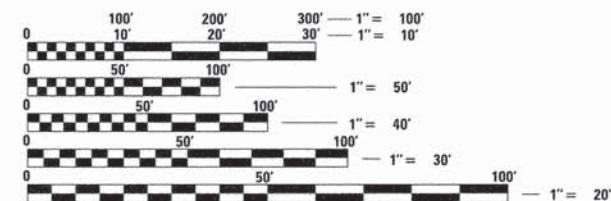
CATON FARM ROAD - 1127.0 FT. (0.213 MILE) (NET AND GROSS)
 ESSINGTON ROAD - 682.0 FT. (0.129 MILE) (NET AND GROSS)

DESIGN DESIGNATION:

CATON FARM ROAD - 19,600 (2015) - STRATEGIC REGIONAL ARTERIAL
 ESSINGTON ROAD - 23,300 (2015) - MINOR ARTERIAL

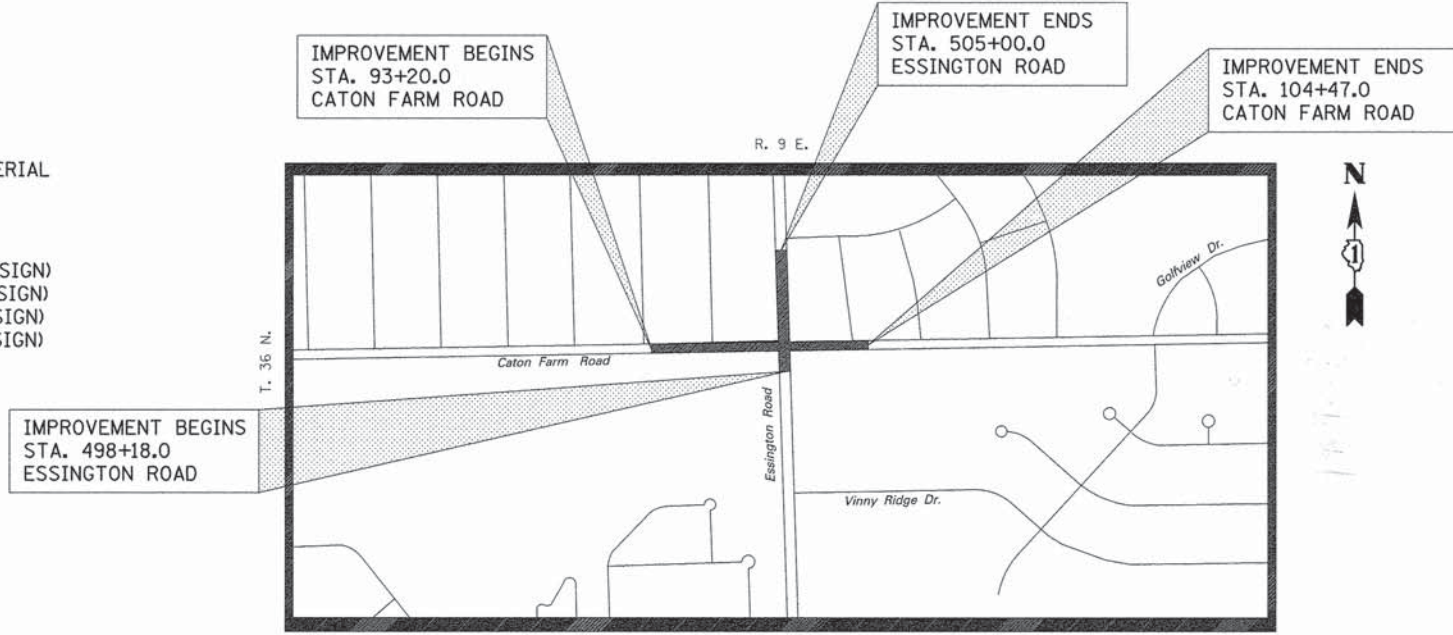
POSTED AND DESIGN SPEEDS:

CATON FARM ROAD WEST LEG - 30 MPH (POSTED) - 40 MPH (DESIGN)
 CATON FARM ROAD EAST LEG - 30 MPH (POSTED) - 35 MPH (DESIGN)
 ESSINGTON ROAD NORTH LEG - 30 MPH (POSTED) - 40 MPH (DESIGN)
 ESSINGTON ROAD SOUTH LEG - 40 MPH (POSTED) - 45 MPH (DESIGN)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811



LOCATION MAP
 NOT TO SCALE



SIGNED *JRV*
 DATE 12-4-2015 EXPIRES 11-30-2017
 FOR DRAWINGS 1 - 51, 82 - 110



SIGNED *JJE*
 DATE 12/4/15 EXPIRES 11-30-2017
 FOR DRAWINGS 52 - 74



SIGNED *DM*
 DATE 12/4/15 EXPIRES 11-30-2017
 FOR DRAWINGS 75 - 81

CONTRACT NO. 61C29



450 E. Devon Ave, Suite 300 - Itasca, Illinois 60143
 Tel: 630.773.3900 - Fax: 630.773.3975
 www.civiltechinc.com

INDEX OF DRAWINGS

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SPECIFICATIONS, STANDARDS, AND SPECIAL PROVISIONS

1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ("STANDARD SPECIFICATIONS"), ADOPTED JANUARY 1, 2012; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2015; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" 2014 SEVENTH EDITION, THE DETAILS IN THE PLANS, AND THE SPECIAL PROVISIONS AND IDOT STANDARD DRAWINGS INCLUDED IN THE CONTRACT DOCUMENTS.

2. ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED AS THE RESIDENT ENGINEER.

3. NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET AND APPROPRIATE PERMITS HAVE BEEN OBTAINED FROM THE CITY OF JOLIET, WILL COUNTY SOIL CONSERVATION DISTRICT, AND THE WILL COUNTY DIVISION OF TRANSPORTATION.

4. ALL UTILITY COMPANIES, SCHOOL DISTRICTS, AND LOCAL POLICE AND FIRE DEPARTMENTS SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 72 HOURS PRIOR TO THE START OF CONSTRUCTION.

5. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

STAKING

1. ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.

2. THE STATION/OFFSET/ELEVATIONS NOTED FOR ALL DRAINAGE STRUCTURES LOCATED IN THE CURB LINE REFER TO THE POSITION OF THE ADJACENT PROPOSED EDGE OF PAVEMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE OFFSET NECESSARY FOR EACH STRUCTURE TO SET THE FRAME AND GRATE IN THE PROPER LOCATION. ALL OTHER STRUCTURES ARE DIMENSIONED TO THE CENTER OF STRUCTURE.

3. PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT, UNLESS OTHERWISE INDICATED.

4. THE CONSTRUCTION BASELINE HAS BEEN ESTABLISHED FOR STAKING PURPOSES ONLY AND IS NOT INTENDED TO BE A CENTERLINE OF RIGHT-OF-WAY.

PAVING AND CURB & GUTTER

1. THE CONTRACTOR SHALL SAW CUT PAVEMENT, CURB & GUTTER, MEDIAN, SIDEWALK AND DRIVEWAYS AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING MATERIAL TO BE REMOVED BY MEANS OF AN APPROVED CONCRETE SAW TO A DEPTH AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

THE CONTRACTOR SHALL BE REQUIRED TO SAW VERTICAL CUTS SO AS TO FORM CLEAN VERTICAL JOINTS. SHOULD THE CONTRACTOR DEFACE ANY EDGE, A NEW SAWED JOINT SHALL BE PROVIDED AND ANY ADDITIONAL WORK, INCLUDING REMOVAL AND REPLACEMENT, SHALL BE DONE AT THE CONTRACTOR'S EXPENSE.

2. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE THICKNESS OF THE EXISTING PAVEMENT AND WHETHER OR NOT IT CONTAINS REINFORCEMENT.

3. BINDER COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY CURED AND BACKFILLED TO THE SATISFACTION OF THE ENGINEER.

4. HOT-MIX ASPHALT SURFACE COURSE SHALL NOT BE PLACED UNTIL ALL EARTH EXCAVATION, TOPSOIL PLACEMENT, AND HOT-MIX ASPHALT BINDER COURSE HAVE BEEN COMPLETED TO THE SATISFACTION OF THE ENGINEER.

5. THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE BINDER OR BASE UPON WHICH THE HOT-MIX ASPHALT MATERIALS ARE PLACED.

6. THE ENGINEER SHALL APPROVE ALL DRIVEWAYS TO BE REMOVED AND THE CONFIGURATION OF ANY DRIVEWAY TO BE RECONSTRUCTED.

7. ALL DRIVEWAYS SHALL BE PORTLAND CEMENT CONCRETE UNLESS OTHERWISE SPECIFIED.

TREE REMOVAL, CLEARING AND HEDGE REMOVAL

1. THE CONTRACTOR SHALL NOTE THE PRESERVATION OF EXISTING TREES IS OF UTMOST IMPORTANCE TO THE CITY OF JOLIET. THIS WORK SHALL BE IN ACCORDANCE WITH SECTION 201 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. TEMPORARY FENCE SHALL BE ERECTED ALONG THE DRIP LINE OF EXISTING TREES TO REMAIN WITHIN THE LIMITS OF CONSTRUCTION OR AS DIRECTED BY THE ENGINEER. AFTER TREES ARE SAFELY, FENCED NOTHING IS TO BE STORED, DRIVEN, OR DISTURBED INSIDE THE FENCE. TEMPORARY FENCE SHALL BE REMOVED ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED.

2. ALL CLEARING AND REMOVAL OF TREES UNDER 6" IN DIAMETER SHALL BE INCLUDED IN THE COST OF "EARTH EXCAVATION".

3. ALL CLEARING AND THE REMOVAL AND/OR RELOCATION OF BUSHES, AS DIRECTED BY THE ENGINEER, SHALL BE INCLUDED IN THE COST OF "EARTH EXCAVATION".

4. ALL LIMBS, BRANCHES AND OTHER DEBRIS RESULTING FROM THIS WORK SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN EXPENSE OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY.

UTILITIES

1. THE CONTRACTOR SHALL COOPERATE WITH THE CITY IN ANY UNDERGROUND UTILITY CONSTRUCTION WHICH THE CITY MAY WANT TO PLACE DURING THE CONTRACTOR'S OPERATIONS.

2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES. THE LOCATION OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING HIS CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 107.31 OF THE "STANDARD SPECIFICATIONS." THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS.

3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ABOVE AND BELOW GROUND UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE CITY. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL NOTIFY ALL UTILITY OWNERS OF THIS CONSTRUCTION SCHEDULE AND SHALL COORDINATE CONSTRUCTION OPERATIONS WITH THE UTILITY OWNERS SO THAT RELOCATION OF UTILITY LINES AND STRUCTURES MAY PROCEED IN AN ORDERLY MANNER. NOTIFICATION SHALL BE IN WRITING, WITH COPIES TRANSMITTED TO THE ENGINEER.

4. COORDINATION OF ANY UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.

5. THE CONTRACTOR SHALL RECEIVE NO ADDITIONAL COMPENSATION FOR CONSTRUCTION STAGING NECESSARY TO ACCOMMODATE UTILITY RELOCATION OR ADJUSTMENT AND/OR FOR DELAYS CAUSED BY UTILITY RELOCATION OR ADJUSTMENT.

6. BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS, WATER, SEWER AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)

7. ALL AUXILIARY VALVES, FRAMES, GRATES, LIDS AND BOXES REMOVED FROM EXISTING WATER SERVICE OR SEWER STRUCTURES WHICH ARE TO BE ABANDONED OR ADJUSTED WITH A NEW OR DIFFERENT FRAME AND LID SHALL BECOME THE PROPERTY OF THE CONTRACTOR.

8. THE CONTRACTOR SHALL FURNISH ALL LABOR, EQUIPMENT AND MATERIAL NECESSARY FOR DEWATERING TRENCH EXCAVATIONS AS WELL AS SHORING TRENCH WALLS DURING UTILITY OPERATIONS. COMPLIANCE WITH THE ABOVE WILL BE INCLUDED IN THE COST OF THE UTILITY INSTALLATIONS.

9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING LOCAL AGENCIES OR OWNERS MAINTAINING SANITARY SEWERS AND WATER MAINS TO VERIFY THE MATERIALS AND METHODS ALLOWED FOR THE ADJUSTMENT OR RELOCATION OF THEIR FACILITIES, IF NECESSARY.

ROADWAY EXCAVATION

1. AGGREGATE SUBGRADE IMPROVEMENT HAS BEEN PROVIDED TO REPLACE SOILS WHICH TEND TO BE UNSTABLE WHEN WET. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. IF UNSUITABLE SOILS ARE ENCOUNTERED THE SOILS SHALL BE REMOVED AND REPLACED WITH AGGREGATE SUBGRADE IMPROVEMENT. THE REMOVAL AND REPLACEMENT AREA SHALL EXTEND TO 12 INCHES BEYOND THE CURB AND GUTTER AND COME UP AT A 1:1 SLOPE TO EXISTING GROUND SURFACE. THESE LIMITS MAY BE ALTERED BY THE ENGINEER IF FIELD CONDITIONS SO WARRANT. REMOVAL OF THESE UNSUITABLE SOILS SHALL BE PAID FOR AS "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL."

2. ADJACENT STREETS MUST BE KEPT CLEAR OF DEBRIS. THE STREETS SHOULD BE INSPECTED DAILY AND CLEANED WHEN NECESSARY. THE COST TO CLEAN THE STREETS SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

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PLLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
GENERAL NOTES, INDEX OF DRAWINGS, AND STANDARD DRAWINGS

SCALE: N.T.S. SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	2
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				

STORM & SANITARY SEWER

- THE COST OF MAKING SEWER CONNECTIONS TO EXISTING OR PROPOSED SEWER OR DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE COST OF THE SEWER OR STRUCTURE BEING CONSTRUCTED.
- UNLESS OTHERWISE NOTED ON THE PLANS, THE EXISTING DRAINAGE FACILITIES SHALL REMAIN IN USE DURING THE PERIOD OF CONSTRUCTION. LOCATIONS OF EXISTING DRAINAGE STRUCTURES AND SEWERS AS SHOWN ON THE PLANS ARE APPROXIMATE. PRIOR TO COMMENCING WORK THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL DETERMINE THE EXACT LOCATIONS OF EXISTING STRUCTURES WHICH ARE WITHIN THE PROPOSED CONSTRUCTION LIMITS.

DURING CONSTRUCTION, IF THE CONTRACTOR ENCOUNTERS OR OTHERWISE BECOMES AWARE OF ANY SEWER, UNDERDRAINS OR FIELD DRAINS WITHIN THE RIGHT-OF-WAY OTHER THAN THOSE SHOWN ON THE PLANS, HE SHALL SO INFORM THE ENGINEER, WHO SHALL DIRECT THE WORK NECESSARY TO MAINTAIN OR REPLACE THE FACILITIES IN SERVICE AND TO PROTECT THEM FROM DAMAGE DURING CONSTRUCTION IF MAINTAINED.

EXISTING FACILITIES TO BE MAINTAINED THAT ARE DAMAGED BECAUSE OF THE NON-COMPLIANCE WITH THIS PROVISION SHALL BE REPLACED AT THE CONTRACTOR'S OWN EXPENSE. SHOULD THE ENGINEER HAVE DIRECTED THE REPLACEMENT OF A FACILITY, THE NECESSARY WORK AND PAYMENT SHALL BE IN ACCORDANCE WITH SECTIONS 550 AND 601, AND ARTICLE 104.02 OF THE STANDARD SPECIFICATIONS.
- WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE THE SAME. HE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET. HE SHALL BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWER ARE BUILT AND IN SERVICE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT.
- ALL ABANDONED PIPE AND STRUCTURE INVERTS SHALL BE PLUGGED WITH BRICK AND MORTAR TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE STORM SEWER ITEMS BEING REMOVED.
- TOP OF FRAME ("RIM") ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF EACH STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATIONS OF THE AREAS IN WHICH THEY ARE LOCATED, AS PART OF THE STRUCTURE COST.
- DRAINAGE STRUCTURE FLAT-TOPS AND CONES SHALL BE TURNED SO THAT THE FRAMES ARE CLOSEST TO THE CENTERLINE OF THE ROAD WHERE POSSIBLE. ALL FLAT-TOPS AND CONES ARE ASSUMED TO BE ECCENTRIC.
- ALL SEWER AND WATER SERVICES CROSSED BY NEW STORM SEWERS SHALL BE PROPERLY LOCATED AND PROTECTED DURING CONSTRUCTION. ANY DAMAGE TO SAID SERVICES NOT CONSIDERED TO BE IN CONFLICT WITH THE PROPOSED STORM SEWER SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.
- WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT.
- ANY EXISTING OR PROPOSED SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO COST TO THE CITY.
- ALL EXISTING STORM SEWER CASTING BEING REMOVED SHALL REMAIN PROPERTY OF THE WCDOT. THE CONTRACTOR SHALL CONTACT ERIC WESEL AT THE WCDOT (815-727-8476) TO ARRANGE DELIVERY OF THE REMOVED CASTINGS TO THE DEPARTMENT. THE COST OF REMOVING AND DELIVERING THE CASTINGS SHOULD BE INCLUDED IN THE COST OF THE DRAINAGE STRUCTURE BEING REMOVED.

SIGNING, STRIPING & LANDSCAPING

- THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR SHALL BE RESTORED BY THE CONTRACTOR AT HIS EXPENSE, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ALL EXISTING TRAFFIC SIGNS WHICH INTERFERE WITH THE CONTRACTOR'S WORK SHALL BE REMOVED AND RELOCATED ON A SIGN SUPPORT APPROVED BY, AND AT A LOCATION DETERMINED BY, THE ENGINEER. THESE SIGNS SHALL BE MAINTAINED STRAIGHT AND CLEAN UNTIL THE PERMANENT SIGNS ARE REINSTALLED. AFTER THE PERMANENT SIGNS HAVE BEEN INSTALLED, UNUSED SIGNS AND POSTS SHALL BE RETURNED TO THE CITY OF JOLIET'S PUBLIC WORKS FACILITY.

THE COST OF RELOCATING THE EXISTING SIGNS AND THE TEMPORARY SIGN SUPPORTS AND THEIR SUBSEQUENT REMOVAL SHALL BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
- PAVEMENT MARKING TAPE, TYPE III, IF REQUIRED AND AT THE DIRECTION OF THE ENGINEER, SHALL BE APPLIED TO THE FINAL PAVEMENT SURFACE PRIOR TO APPLICATION OF THE PERMANENT PAVEMENT MARKING, IN ACCORDANCE WITH SECTIONS 703 AND 1095 OF THE STANDARD SPECIFICATIONS.

MATERIALS QC /QA POLICY

- ALL HOT-MIX ASPHALT AND P.C. CONCRETE MATERIALS USED ON THIS PROJECT SHALL BE TESTED AND INSPECTED IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S QC/QA REQUIREMENTS.
- THE CONTRACTOR SHALL PROVIDE QC TESTING TO THE ILLINOIS DEPARTMENT OF TRANSPORTATION BUREAU OF MATERIALS ORDER BOARD (PHONE: (847) 705-4337 OR FAX: (847) 705-4529) BY 4:00 P.M., 24 HOURS IN ADVANCE OF CONSTRUCTION FOR INSPECTION OF ALL HOT-MIX ASPHALT AND CONCRETE MATERIALS USED ON THIS PROJECT.
- THE CONTRACTOR IS TO SUBMIT A QC PLAN FOR HMA AND CONCRETE MATERIALS TO THE QA MANAGER FOR APPROVAL PRIOR TO CONSTRUCTION OPERATIONS COMMENCING. THE QA MANAGER WILL APPROVE THIS PLAN AND COPY THE DISTRICT MATERIALS OFFICE ON THE APPROVAL LETTER.
- QC AND QA REPORTS FOR CONCRETE WILL BE SENT TO THE DISTRICT BUREAU OF MATERIALS OFFICE AFTER REVIEW AND APPROVAL BY THE QA MANAGER.
- QC REPORT FOR HOT-MIX ASPHALT MIXTURES WILL BE TRANSMITTED DIRECTLY BY THE CONTRACTOR DAILY DURING PRODUCTION. THE DISTRICT WILL REVIEW AND RETAIN THE QA PLANT REPORTS. THE QA FIELD REPORTS CAN BE SUBMITTED BY THE QA MANAGER TO THE DISTRICT VIA THE DISTRICT LOCAL ROADS OFFICE.
- THE COSTS TO COMPLY WITH THESE REQUIREMENTS SHALL BE INCLUDED IN THE COST OF THE VARIOUS HOT-MIX ASPHALT AND P.C. CONCRETE ITEMS.

MISCELLANEOUS

- THE CONTRACTOR SHALL ADHERE TO IDOT STANDARD DRAWING DRAWING NO. 701801-05 WHEN CLOSING ANY SIDEWALK TO PERMIT CONSTRUCTION OF THE IMPROVEMENTS.
- UNLESS OTHERWISE NOTED IN THE PLANS OR AUTHORIZED BY THE ENGINEER, ALL EXISTING ACCESS POINTS SHALL BE MAINTAINED AT ALL TIMES BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOT CROSS COMPLETED BINDER COURSE, OR EXISTING PAVEMENT NOT SCHEDULED TO BE REMOVED, WITH CONSTRUCTION EQUIPMENT WHICH MAY DAMAGE THE PAVEMENT.
- THE WILL COUNTY DIVISION OF TRANSPORTATION MUST BE NOTIFIED A MINIMUM OF TWO (2) WORKINGS DAYS IN ADVANCE OF ANY CONSTRUCTION WITHIN THE COUNTY RIGHT-OF-WAY.
- A WILL COUNTY UTILITY PERMIT IS REQUIRED FOR ALL UTILITY INSTALLATIONS OR RELOCATIONS (WATER MAIN, SANITARY SEWER, STREETLIGHTS, ETC.) WITHIN THE COUNTY RIGHT-OF-WAY.

DISTRICT ONE STANDARD DETAILS

STANDARD NO.	DESCRIPTION
BD-07	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER
BD-08	FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-32	BUTT JOINTS AND HMA TAPER
BD-36	FIRE HYDRANT TO BE MOVED
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-11	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-16	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
TC-22	ARTERIAL ROAD INFORMATION SIGNING
TC-26	DRIVEWAY ENTRANCE SIGN
TS-02	MAST ARM MOUNTED STREET NAME SIGNS
TS-05	STANDARD TRAFFIC SIGNAL DESIGN DETAILS

STANDARD NO.

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424021-03	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
602001-02	CATCH BASIN, TYPE A
602011-02	CATCH BASIN, TYPE C
602301-04	INLET, TYPE A
602401-03	MANHOLE, TYPE A
602601-03	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-04	FRAME AND LIDS, TYPE 1
604091-03	FRAME AND GRATE, TYPE 24
606001-06	CONCRETE CURB TYPE B & COMBINATION CONCRETE CURB & GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
606306-03	CORRUGATED PC CONCRETE MEDIANS
701101-04	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24' FROM EDGE OF PAVEMENT
701427-04	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS < 40 MPH
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
728001-01	TELESCOPING STEEL SIGN SUPPORT
780001-05	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
877011-05	STEEL COMB. MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'

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PLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
GENERAL NOTES, INDEX OF DRAWINGS, AND STANDARD DRAWINGS

SCALE: N.T.S. SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	3
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				

SPECIAL PROVISION	SPECIALTY ITEM	CODED PAY ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	0004	0021	0021	0021	0031
						ROADWAY (RECONSTRUCTION NO CAPACITY ADDED)	SAFETY (SIGNALS)	SAFETY (SIDEWALK)	SAFETY (LIGHTING)	LANDSCAPING AND OTHER SCENIC BEAUTIFICATION
X		20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	156	156				
X		20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	18	18				
X	X	20101000	TEMPORARY FENCE	FOOT	960	960				
X	X	20101200	TREE ROOT PRUNING	EACH	14	14				
X	X	20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	7	7				
X	X	20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	7	7				
		20200100	EARTH EXCAVATION	CU YD	1,650	1,650				
		20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	140	140				
X		20400800	FURNISHED EXCAVATION	CU YD	605	605				
X		20800150	TRENCH BACKFILL	CU YD	115	115				
		21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	440	440				
X		21101625	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	3,166					3,166
X		25000400	NITROGEN FERTILIZER NUTRIENT	POUND	39					39
X		25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	39					39
X		25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	39					39
X		25200110	SODDING, SALT TOLERANT	SQ YD	3,166					3,166
X		25200200	SUPPLEMENTAL WATERING	UNIT	47					47
		28000400	PERIMETER EROSION BARRIER	FOOT	332	332				
		28000500	INLET AND PIPE PROTECTION	EACH	2	2				
		28000510	INLET FILTERS	EACH	27	27				
X		30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	140	140				
X		30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	2,609	2,609				
		31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	1,954	986		968		
		35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	1,113	1,113				
		40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	17,508	17,508				
		40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	535	535				
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	161	161				
		40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	1,630	1,630				
		40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	18	18				
		40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1,091	1,091				
		42001300	PROTECTIVE COAT	SQ YD	3,074	2,148		926		
		42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	234	234				
		42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	805	805				
		42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	8,400			8,400		
		42400800	DETECTABLE WARNINGS	SQ FT	445			445		
		44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	223	223				
		44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	11,774	11,774				
		44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1,057	1,057				
		44000300	CURB REMOVAL	FOOT	326	326				
		44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	3,534	3,534				
		44000600	SIDEWALK REMOVAL	SQ FT	7,643	7,643				

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
 SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET 1 OF 5 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	4
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

SPECIAL PROVISION SPECIALTY ITEM	CODED PAY ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	0004	0021	0021	0021	0031
					ROADWAY (RECONSTRUCTION NO CAPACITY ADDED)	SAFETY (SIGNALS)	SAFETY (SIDEWALK)	SAFETY (LIGHTING)	LANDSCAPING AND OTHER SCENIC BEAUTIFICATION
	44003100	MEDIAN REMOVAL	SO FT	1,774	1,774				
	44201761	CLASS D PATCHES, TYPE I, 10 INCH	SO YD	14	14				
	44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	56	56				
	44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	45	45				
	44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	87	87				
	550A0040	STORM SEWERS, CLASS A, TYPE 1 10"	FOOT	10	10				
	550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	184	184				
	550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	15	15				
	550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	6	6				
	550A0400	STORM SEWERS, CLASS A, TYPE 2 21"	FOOT	24	24				
	550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	18	18				
	55100400	STORM SEWER REMOVAL 10"	FOOT	8	8				
	55100500	STORM SEWER REMOVAL 12"	FOOT	94	94				
X	56400100	FIRE HYDRANTS TO BE MOVED	EACH	2	2				
	60107700	PIPE UNDERDRAINS 6"	FOOT	5	5				
	60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	12	12				
	60208240	CATCH BASINS, TYPE C, TYPE 24 FRAME AND GRATE	EACH	8	8				
	60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	4	4				
	60236800	INLETS, TYPE A, TYPE 11 FRAME AND GRATE	EACH	1	1				
	60237470	INLETS, TYPE A, TYPE 24 FRAME AND GRATE	EACH	5	5				
	60258100	MANHOLES TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1	1				
	60260100	INLETS TO BE ADJUSTED	EACH	1	1				
	60262700	INLETS TO BE RECONSTRUCTED	EACH	1	1				
X	60266600	VALVE BOXES TO BE ADJUSTED	EACH	2	2				
	60500040	REMOVING MANHOLES	EACH	3	3				
	60500050	REMOVING CATCH BASINS	EACH	13	13				
	60500060	REMOVING INLETS	EACH	1	1				
	60500090	REMOVING INLETS TO MAINTAIN FLOW	EACH	1	1				
	60600605	CONCRETE CURB, TYPE B	FOOT	495	495				
	60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	768	768				
	60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	2,874	2,874				
	60619600	CONCRETE MEDIAN, TYPE SB-6.12	SO FT	891	891				
	60624600	CORRUGATED MEDIAN	SO FT	70	70				
	67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	9	9				
	67100100	MOBILIZATION	LSUM	1	1				
	70106800	CHANGEABLE MESSAGE SIGN	CAL MO	28	28				
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	1,584	1,584				
	70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	473	473				
	70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	20,341	20,341				
	70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	3,557	3,557				
	70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	605	605				

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET 2 OF 5 SHEETS STA. TO STA.

F.A.U. RTE. 292	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 5
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

SPECIAL PROVISION	SPECIALTY ITEM	CODED PAY	ITEM	UNIT	TOTAL QUANTITY	0004	0021	0021	0021	0031
		ITEM NO.				ROADWAY (RECONSTRUCTION NO CAPACITY ADDED)	SAFETY (SIGNALS)	SAFETY (SIDEWALK)	SAFETY (LIGHTING)	LANDSCAPING AND OTHER SCENIC BEAUTIFICATION
		70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	443	443				
		70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	3,915	3,915				
		72000100	SIGN PANEL - TYPE 1	SQ FT	151	151				
		72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	6	6				
		72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	1	1				
		72400310	REMOVE SIGN PANEL - TYPE 1	SQ FT	4	4				
		72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	11	11				
		72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	EACH	2	2				
		72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	187	187				
X		78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	437	437				
X		78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	7,738	7,738				
X		78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,779	1,779				
X		78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	2,035	2,035				
X		78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	187	187				
X		78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	229	229				
X		78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	95	95				
X		80500020	SERVICE INSTALLATION - POLE MOUNTED	EACH	1		1			
X		81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	1,841		1,841			
X		81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	458		73		385	
X		81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	51		51			
X		81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	508		508			
X		81028740	UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 1 1/2" DIA.	FOOT	1,701				1,701	
X	X	81400100	HANDHOLE	EACH	7		7			
X	X	81400200	HEAVY DUTY HANDHOLE	EACH	4		4			
X	X	81400300	DOUBLE HANDHOLE	EACH	2		2			
X	X	81702130	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6	FOOT	1,803				1,803	
X	X	81702300	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 2-1/C NO. 4	FOOT	1,803				1,803	
X		82102400	LUMINARE, SODIUM VAPOR, HORIZONTAL MOUNT, 400 WATT	EACH	4				4	
X		83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	50				50	
X		84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	5				5	
X		85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2		2			
X		86400100	TRANSCEIVER - FIBER OPTIC	EACH	1		1			
X	X	87300925	ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1C	FOOT	934		934			
X	X	87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1,360		1,360			
X	X	87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1,741		1,741			
X	X	87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1,253		1,253			
X	X	87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	2,628		2,628			
X	X	87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	3,991		3,991			
X	X	87301805	ELECTRIC CABLE IN CONDUIT, SERVICE NO. 6 2C	FOOT	222		222			
X	X	87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	963		963			
X	X	87502440	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 10 FT.	EACH	1		1			

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET 3 OF 5 SHEETS STA. TO STA.

F.A.U. RTE. 292	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 6
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

SPECIAL PROVISION	SPECIALTY ITEM	CODED PAY	ITEM	UNIT	TOTAL QUANTITY	0004	0021	0021	0021	0031
		ITEM NO.				ROADWAY (RECONSTRUCTION NO CAPACITY ADDED)	SAFETY (SIGNALS)	SAFETY (SIDEWALK)	SAFETY (LIGHTING)	LANDSCAPING AND OTHER SCENIC BEAUTIFICATION
X	X	87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 16 FT.	EACH	4		4			
X	X	87702940	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 42 FT.	EACH	2		2			
X	X	87702960	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 46 FT.	EACH	1		1			
X	X	87702990	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 54 FT.	EACH	1		1			
X	X	87800100	CONCRETE FOUNDATION, TYPE A	FOOT	20		20			
X	X	87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4		4			
X	X	87800415	CONCRETE FOUNDATION, TYPE E, 36" DIA	FOOT	54		54			
	X	87900200	DRILL EXISTING HANDHOLE	EACH	1		1			
X	X	88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION MAST ARM MOUNTED	EACH	6		6			
X	X	88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION BRACKET MOUNTED	EACH	6		6			
X	X	88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION MAST ARM MOUNTED	EACH	6		6			
X	X	88102717	PEDESTRIAN HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8		8			
X	X	88200410	TRAFFIC SIGNAL BACKPLATE LOUVERED, FORMED PLASTIC	EACH	12		12			
	X	88500100	INDUCTIVE LOOP DETECTOR	EACH	14		14			
	X	88600100	DETECTOR LOOP, TYPE I	FOOT	875		875			
X	X	88700200	LIGHT DETECTOR	EACH	2		2			
X	X	88700300	LIGHT DETECTOR AMPLIFIER	EACH	1		1			
X	X	88800100	PEDESTRIAN PUSH BUTTON	EACH	8		8			
X	X	89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1		1			
X	X	89501510	RELOCATE EXISTING FLASHING BEACON	EACH	1		1			
	X	89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	1,973		256		1,717	
	X	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1		1			
X	X	89502380	REMOVE EXISTING HANDHOLE	EACH	9		9			
X	X	89502382	REMOVE EXISTING DOUBLE HANDHOLE	EACH	2		2			
X	X	89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	5		5			
	X	A2001020	TREE, ACER RUBRUM (RED MAPLE), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	6					6
	X	A2004820	TREE, GLEDITSIA TRIACANTHOS INERMIS SKYLINE (SKYLINE THORNLESS COMMON	EACH	5					5
X	X	X0324085	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3C	FOOT	322		322			
	X	X0327301	RELOCATE EXISTING MAILBOX	EACH	3	3				
X	X	X0327425	RELOCATE WIRELESS INTERCONNECT (COMPLETE)	LSUM	1		1			
X	X	X0327698	LED INTERNALLY ILLUMINATED STREET NAME SIGN	EACH	4		4			
	X	X2130010	EXPLORATION TRENCH, SPECIAL	FOOT	525	525				
	X	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	7	7				
	X	X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	7	7				
	X	X4023000	TEMPORARY ACCESS (ROAD)	EACH	2	2				
	X	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	17	17				
	X	X6060052	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (SPECIAL)	FOOT	118	118				
	X	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	LSUM	1	1				
X	X	X7030030	WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH	FOOT	7,563	7,563				
X	X	X8360215	LIGHT POLE FOUNDATION, 24" DIAMETER, OFFSET	FOOT	30				30	

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET 4 OF 5 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	7
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

SPECIAL PROVISION	SPECIALTY ITEM	CODED PAY	ITEM	UNIT	TOTAL QUANTITY	0004	0021	0021	0021	0031
		ITEM NO.				ROADWAY (RECONSTRUCTION NO CAPACITY ADDED)	SAFETY (SIGNALS)	SAFETY (SIDEWALK)	SAFETY (LIGHTING)	LANDSCAPING AND OTHER SCENIC BEAUTIFICATION
X	X	X8570226	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1		1			
X	X	X8620200	UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1		1			
X	X	X8710024	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	957		957			
X		Z0013798	CONSTRUCTION LAYOUT	LSUM	1	1				
X		Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	190	190				
X	X	Z0033028	MAINTENANCE OF LIGHTING SYSTEM	CAL MO	5			7		
X	X	Z0033046	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2	EACH	1		1			
X		Z0056608	STORM SEWER (WATER MAIN REQUIREMENTS) 12 INCH	FOOT	169	169				
X	X	Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1		1			
X	o	Z0076600	TRAINEES	HOUR	500	500				
X	o	Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500	500				
X	X	XX006826	REMOVE AND RELOCATE LAWN SPRINKLER SYSTEM	FOOT	200					200
X	X	XX008608	CABLE, SPECIAL	FOOT	800		800			

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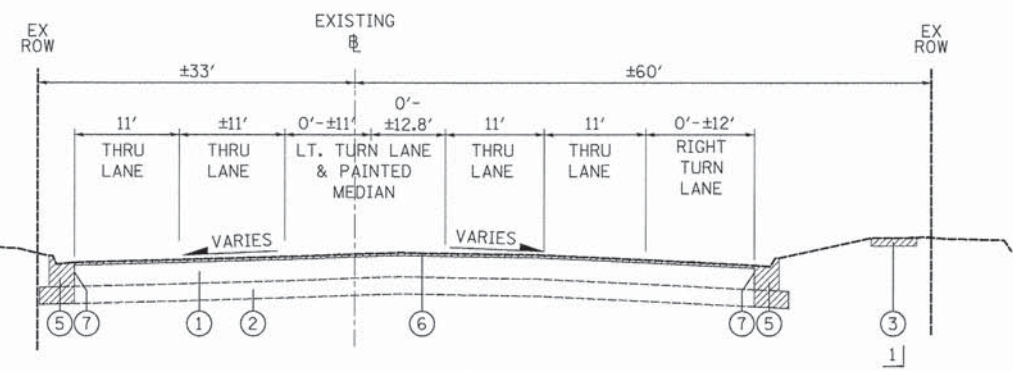
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DEPARTMENT OF TRANSPORTATION

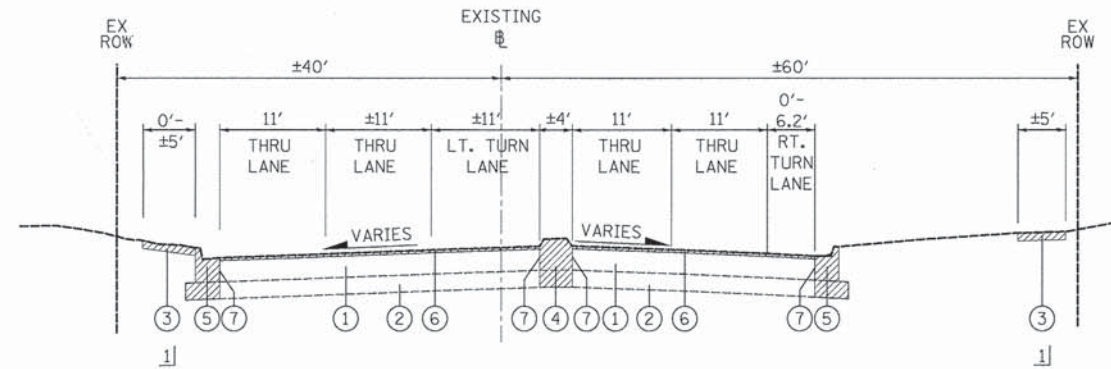
CATON FARM ROAD AND ESSINGTON ROAD
SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET 5 OF 5 SHEETS STA. TO STA.

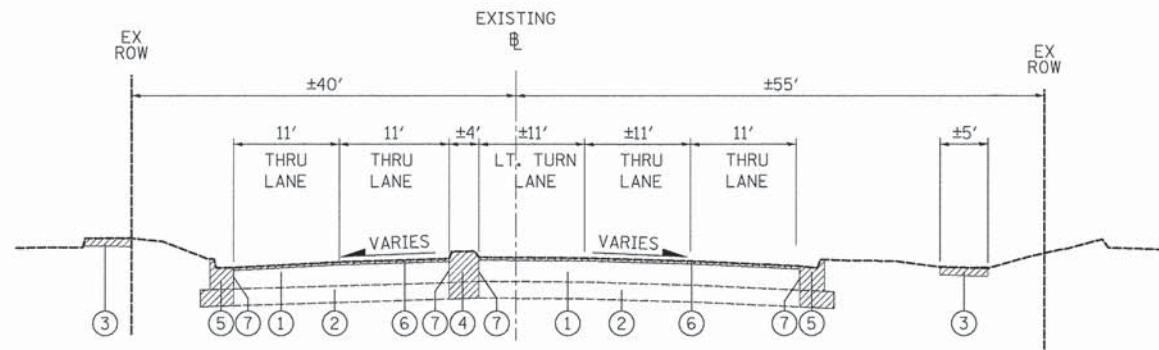
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	8
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	



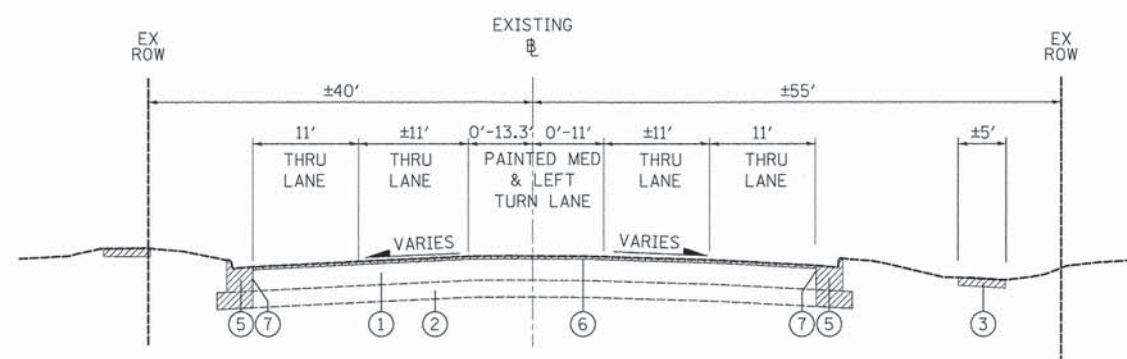
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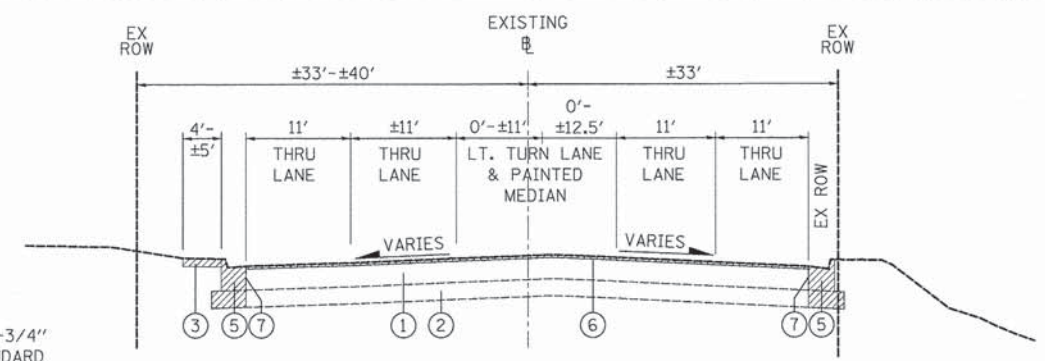
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STA. 100+39.8 TO STA. 101+82.2



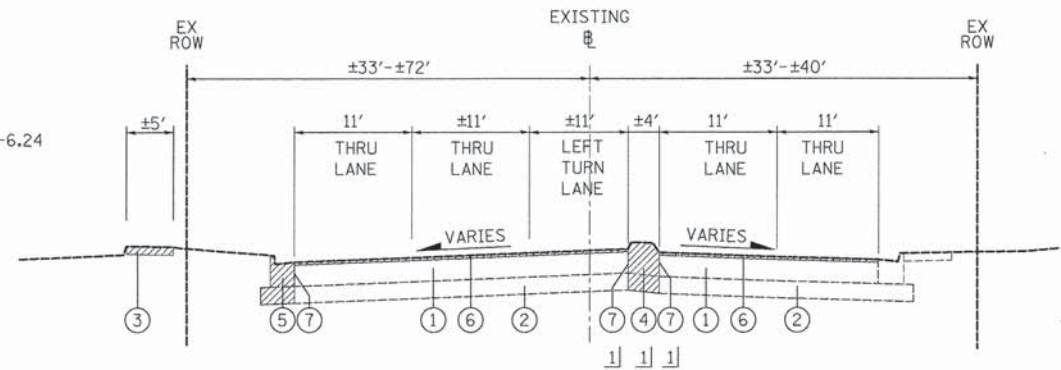
CATON FARM ROAD – EXISTING TYPICAL SECTION
STA. 98+17.9 TO STA. 99+60.3



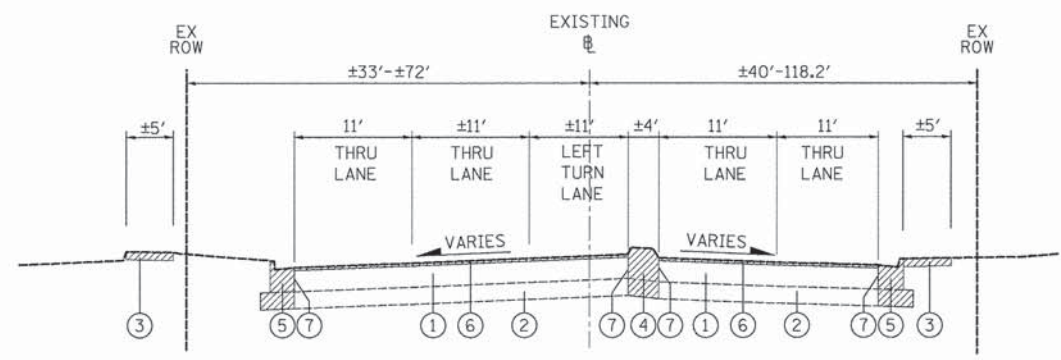
CATON FARM ROAD – EXISTING TYPICAL SECTION
STA. 93+07.0 TO STA. 98+17.9
STA. 99+60.3 TO STA. 100+39.8



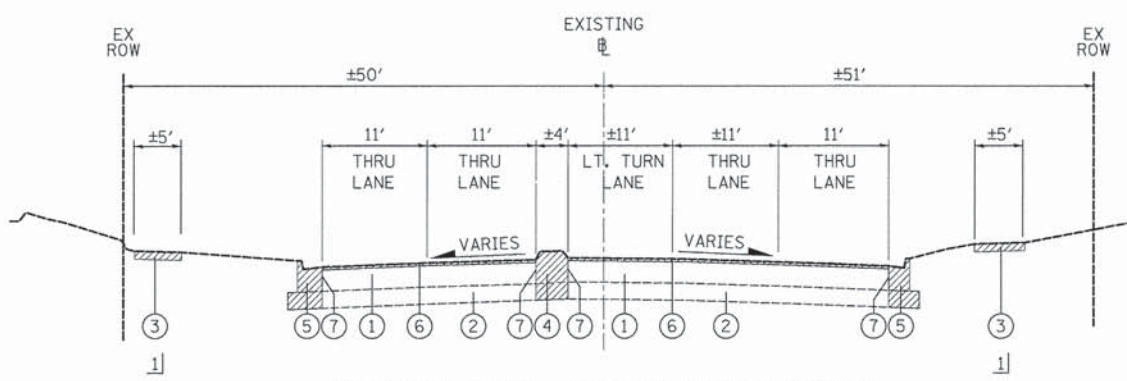
ESSINGTON ROAD – EXISTING TYPICAL SECTION
STA. 503+08.8 TO STA. 505+00.0



ESSINGTON ROAD – EXISTING TYPICAL SECTION
STA. 501+00.0 TO STA. 503+08.8



ESSINGTON ROAD – EXISTING TYPICAL SECTION
STA. 500+00.0 TO STA. 501+00.0



ESSINGTON ROAD – EXISTING TYPICAL SECTION
STA. 498+18.0 TO STA. 500+00.0

LEGEND

- ① EXISTING HOT-MIX ASPHALT PAVEMENT, DEPTH VARIES FROM 12" TO 15-3/4" (SEE ROADWAY GEOTECHNICAL REPORT PREPARED BY MIDLAND STANDARD ENGINEERING & TESTING, INC. PREPARED APRIL 2013)
- ② EXISTING GRANULAR SUB-BASE, DEPTH VARIES FROM 5" TO 11" (SEE ROADWAY GEOTECHNICAL REPORT PREPARED BY MIDLAND STANDARD ENGINEERING & TESTING, INC. PREPARED APRIL 2013)
- ③ EXISTING P.C.C. SIDEWALK
- ④ EXISTING P.C.C. MEDIAN
- ⑤ EXISTING P.C.C. COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- ⑥ HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
- ⑦ FULL-DEPTH SAW CUT (INCLUDED IN THE COST OF THE ITEM BEING REMOVED)

NOTE:

- 1] SEE REMOVAL PLANS FOR STATION LIMITS OF REMOVAL



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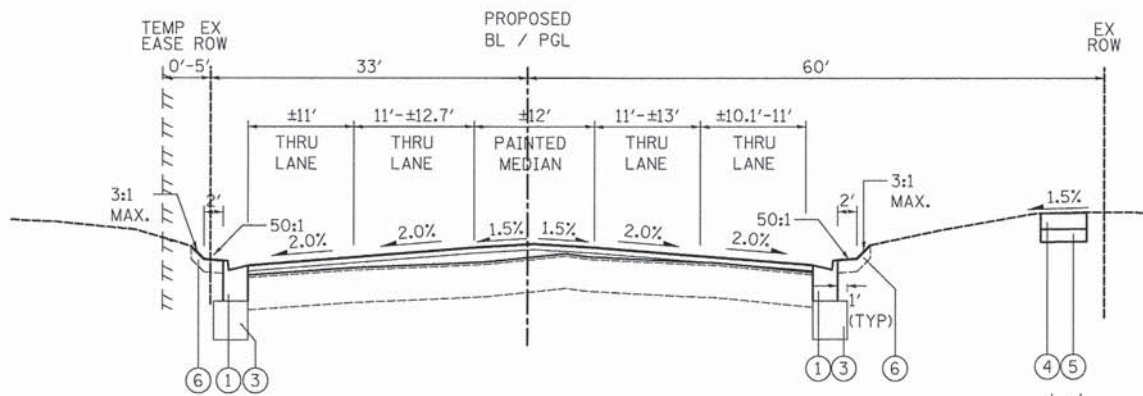
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

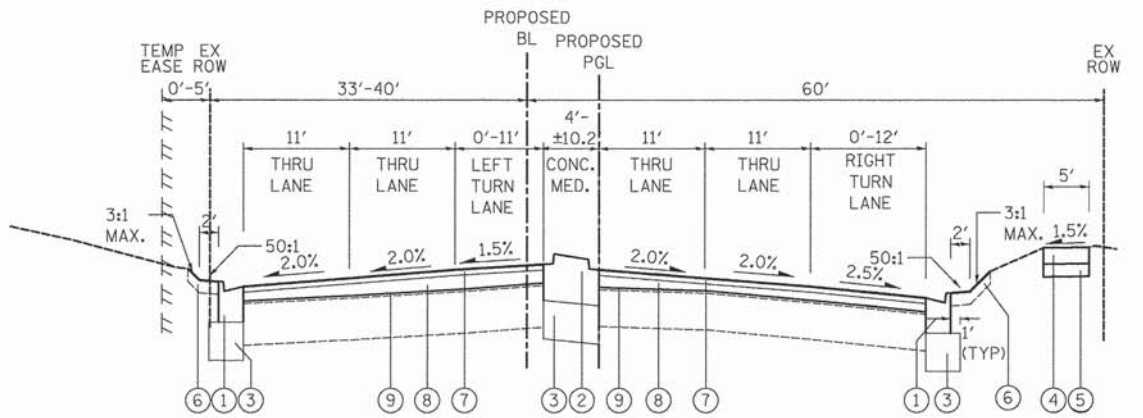
**CATON FARM ROAD AND ESSINGTON ROAD
EXISTING TYPICAL SECTIONS**

SCALE: N.T.S. SHEET 1 OF 2 SHEETS STA. 93+07.00 TO STA. 505+00.00

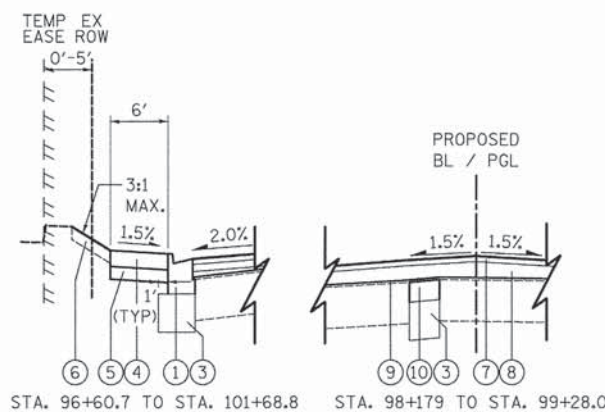
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292	11-00446-00-CH	WILL	110	9
				CONTRACT NO. 61C29
ILLINOIS FED. AID PROJECT M-4003 (254)				



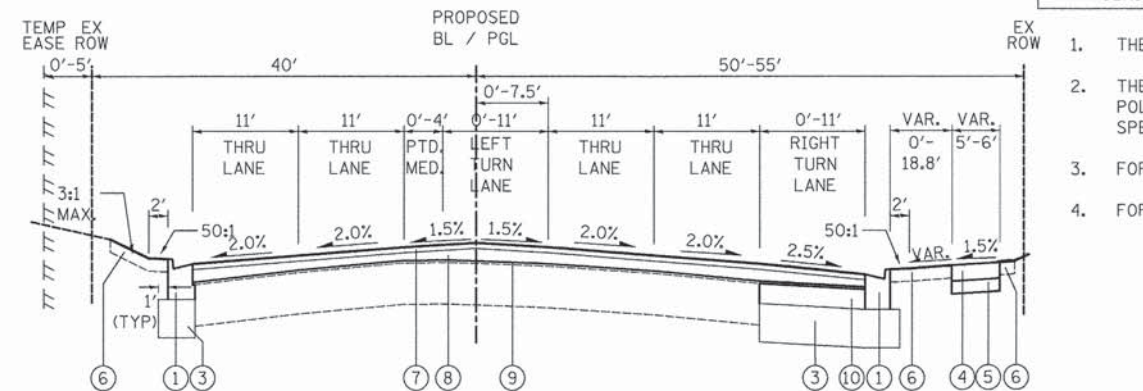
CATON FARM ROAD - PROPOSED TYPICAL SECTION
STA. 103+40.5 TO STA. 104+47.0



CATON FARM ROAD - PROPOSED TYPICAL SECTION
STA. 100+70.0 TO STA. 103+40.5



STA. 96+60.7 TO STA. 101+68.8 STA. 98+179 TO STA. 99+28.0



CATON FARM ROAD - PROPOSED TYPICAL SECTION
STA. 93+07.0 TO STA. 100+70.0

LEGEND

- ① PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (GUTTER SLOPE 6% UNLESS OTHERWISE NOTED, GUTTER FLAG THICKNESS = 10")
- ② PROPOSED CONCRETE MEDIAN, TYPE SB-6.12 (GUTTER SLOPE 2% PITCHED OUT UNLESS OTHERWISE NOTED)
- ③ PROPOSED AGGREGATE SUBGRADE IMPROVEMENT 12"
- ④ PROPOSED P.C.C. SIDEWALK 5 INCH
- ⑤ PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4"
- ⑥ PROPOSED TOPSOIL FURNISH AND PLACE, 6" AND SODDING, SALT TOLERANT
- ⑦ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1-1/2"
- ⑧ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - VARIABLE DEPTH (2-1/4" MIN.)
- ⑨ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4"
- ⑩ HOT-MIX ASPHALT BASE COURSE, 6"
- ⑪ AGGREGATE SUBGRADE IMPROVEMENT
- ⑫ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION

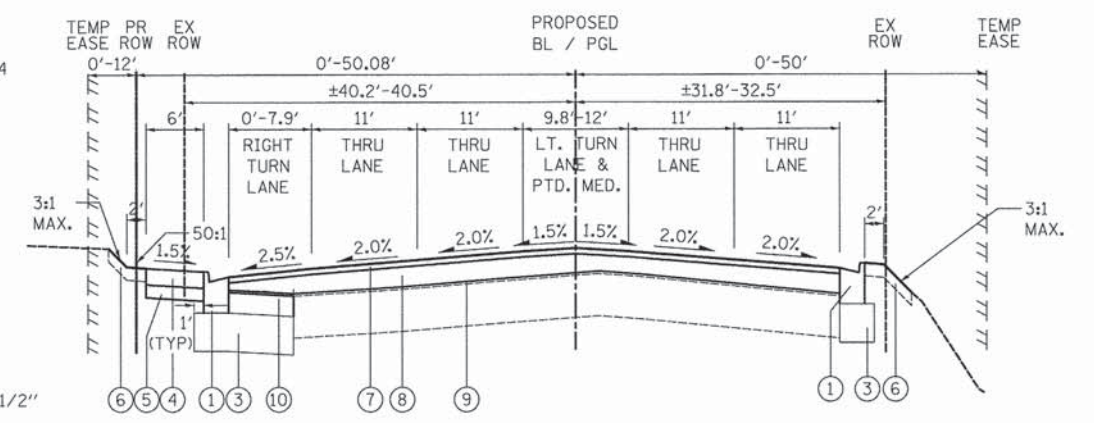
NOTE:

- 1] SEE PLAN AND PROFILE SHEETS FOR STATION LIMITS

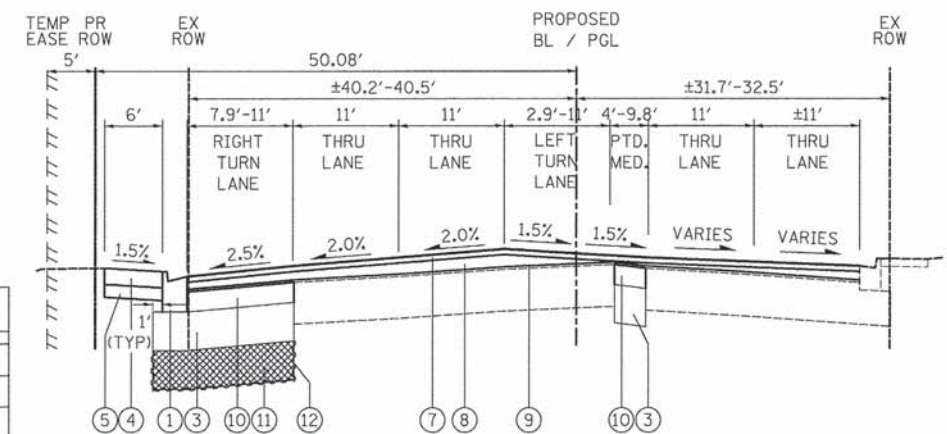
HOT-MIX ASPHALT MIXTURE REQUIREMENTS

PAY ITEM	PERCENT AIR VOIDS @ Ndes.
WIDEN AND OVERLAY	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1-1/2" (1 LIFT)	4% @ 70 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - VARIABLE DEPTH (2-1/4" MIN.)	4% @ 70 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4" (1 LIFT)	3.5% @ 50 GYR.
HOT-MIX ASPHALT BASE COURSE, 6" (2 LIFTS)	4% @ 50 GYR.
OVERLAY	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1-1/2" (1 LIFT)	4% @ 70 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - VARIABLE DEPTH (2-1/4" MIN.)	4% @ 70 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4" (1 LIFT)	3.5% @ 50 GYR.
EASTLINE DRIVE	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1-1/2" (1 LIFT)	4% @ 70 GYR.
CLASS D PATCH, 10"	
CLASS D PATCH (HMA BINDER IL-19 MM): 10" (3 LIFTS)	4% @ 70 GYR.

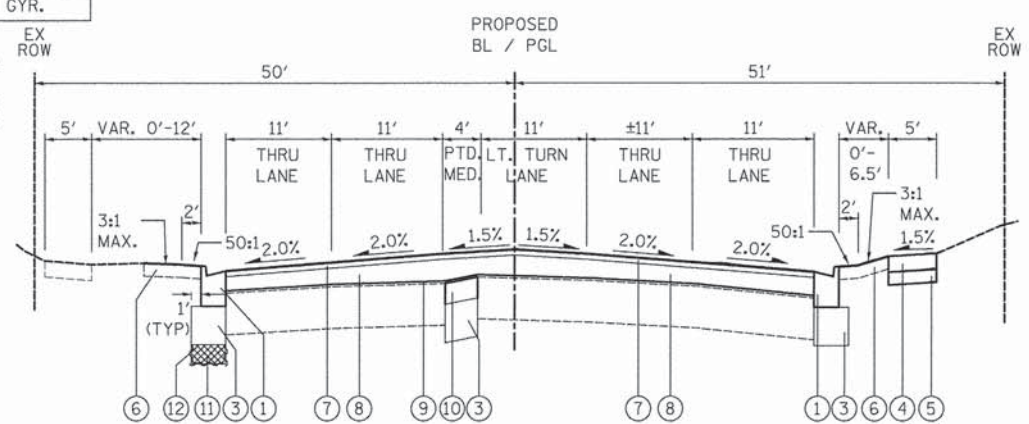
- 1. THE UNIT WEIGHT TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LB/SY-IN.
- 2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
- 3. FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS.
- 4. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.



ESSINGTON ROAD - PROPOSED TYPICAL SECTION
STA. 503+08.8 TO STA. 505+00.0



ESSINGTON ROAD - PROPOSED TYPICAL SECTION
STA. 501+00.0 TO STA. 503+08.8



ESSINGTON ROAD - PROPOSED TYPICAL SECTION
STA. 498+18.0 TO STA. 499+31.0

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	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD	
PROPOSED TYPICAL SECTIONS	
SCALE: N.T.S.	SHEET 2 OF 2 SHEETS
STA. 93+07.0	TO STA. 505+00.0

F.A.U. RTE. 292	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 10
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

EXISTING AND PROPOSED CATON FARM ROAD BASELINE

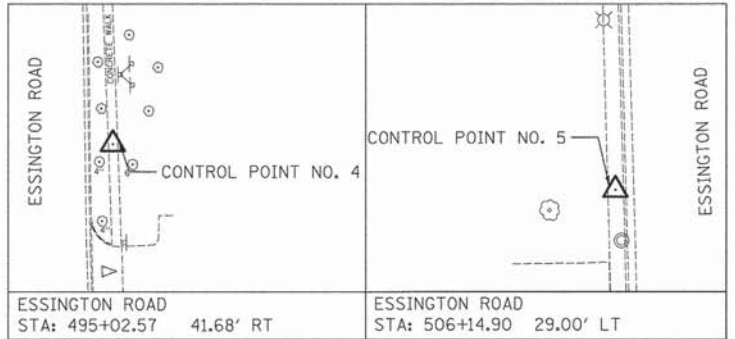
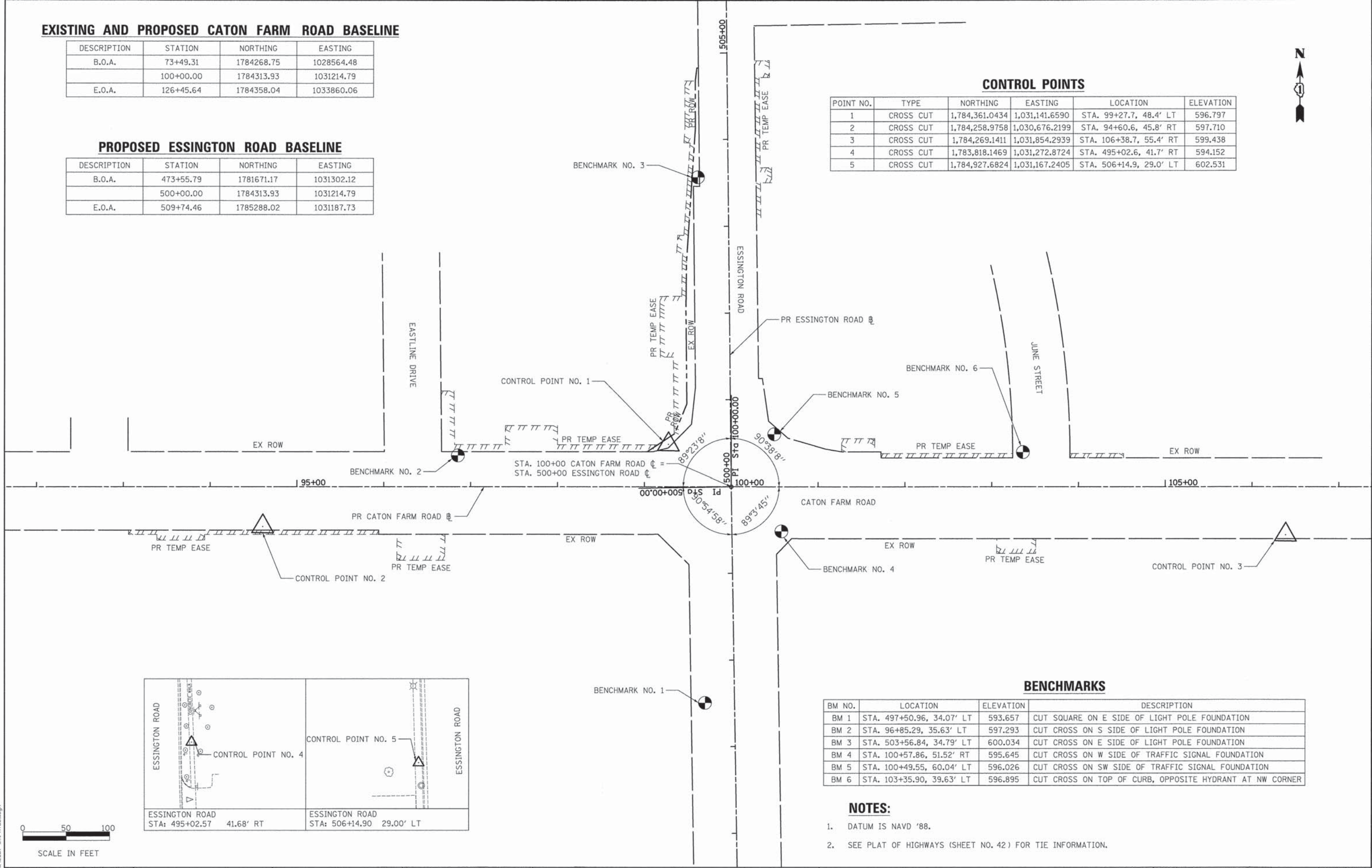
DESCRIPTION	STATION	NORTHING	EASTING
B.O.A.	73+49.31	1784268.75	1028564.48
	100+00.00	1784313.93	1031214.79
E.O.A.	126+45.64	1784358.04	1033860.06

PROPOSED ESSINGTON ROAD BASELINE

DESCRIPTION	STATION	NORTHING	EASTING
B.O.A.	473+55.79	1781671.17	1031302.12
	500+00.00	1784313.93	1031214.79
E.O.A.	509+74.46	1785288.02	1031187.73

CONTROL POINTS

POINT NO.	TYPE	NORTHING	EASTING	LOCATION	ELEVATION
1	CROSS CUT	1,784,361.0434	1,031,141.6590	STA. 99+27.7, 48.4' LT	596.797
2	CROSS CUT	1,784,258.9758	1,030,676.2199	STA. 94+60.6, 45.8' RT	597.710
3	CROSS CUT	1,784,269.1411	1,031,854.2939	STA. 106+38.7, 55.4' RT	599.438
4	CROSS CUT	1,783,818.1469	1,031,272.8724	STA. 495+02.6, 41.7' RT	594.152
5	CROSS CUT	1,784,927.6824	1,031,167.2405	STA. 506+14.9, 29.0' LT	602.531



BENCHMARKS

BM NO.	LOCATION	ELEVATION	DESCRIPTION
BM 1	STA. 497+50.96, 34.07' LT	593.657	CUT SQUARE ON E SIDE OF LIGHT POLE FOUNDATION
BM 2	STA. 96+85.29, 35.63' LT	597.293	CUT CROSS ON S SIDE OF LIGHT POLE FOUNDATION
BM 3	STA. 503+56.84, 34.79' LT	600.034	CUT CROSS ON E SIDE OF LIGHT POLE FOUNDATION
BM 4	STA. 100+57.86, 51.52' RT	595.645	CUT CROSS ON W SIDE OF TRAFFIC SIGNAL FOUNDATION
BM 5	STA. 100+49.55, 60.04' LT	596.026	CUT CROSS ON SW SIDE OF TRAFFIC SIGNAL FOUNDATION
BM 6	STA. 103+35.90, 39.63' LT	596.895	CUT CROSS ON TOP OF CURB, OPPOSITE HYDRANT AT NW CORNER

NOTES:

- DATUM IS NAVD '88.
- SEE PLAT OF HIGHWAYS (SHEET NO. 42) FOR TIE INFORMATION.



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

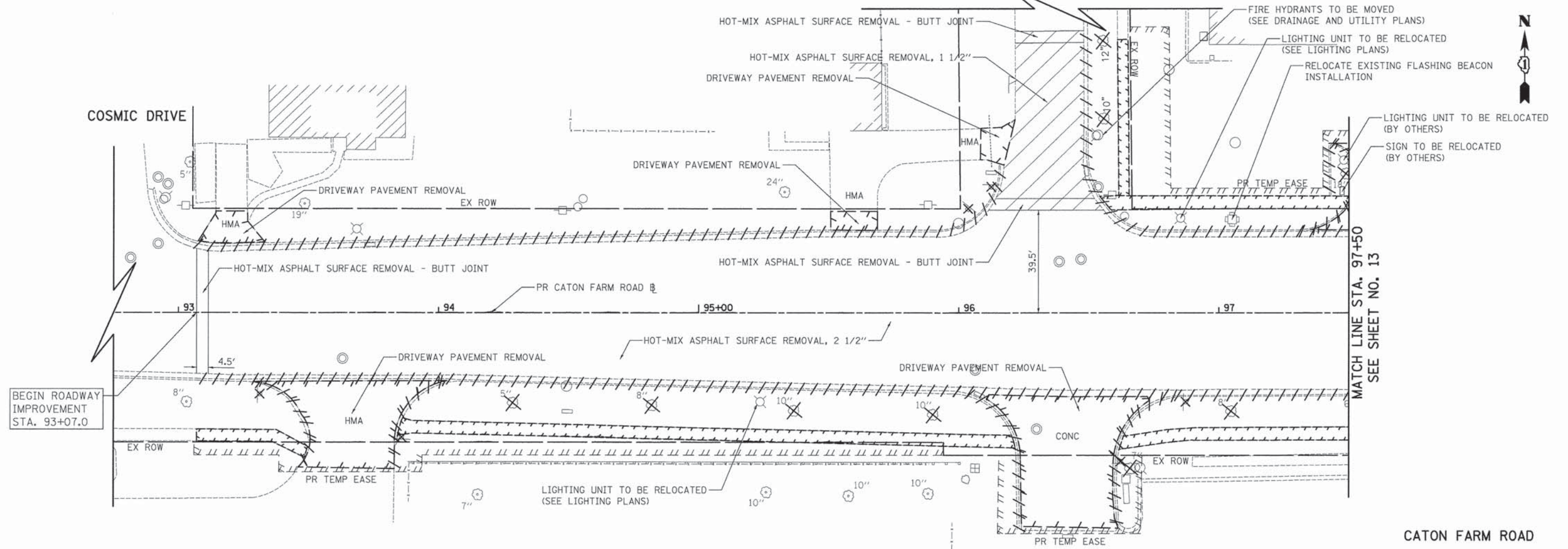
**CATON FARM ROAD AND ESSINGTON ROAD
ALIGNMENT AND BENCHMARKS**

SCALE: 1" = 50' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	11
				CONTRACT NO. 61C29
ILLINOIS FED. AID PROJECT M-4003 (254)				

COSMIC DRIVE

EASTLINE DRIVE



MATCH LINE STA. 97+50
SEE SHEET NO. 13

CATON FARM ROAD

JUNE STREET

NOTES:

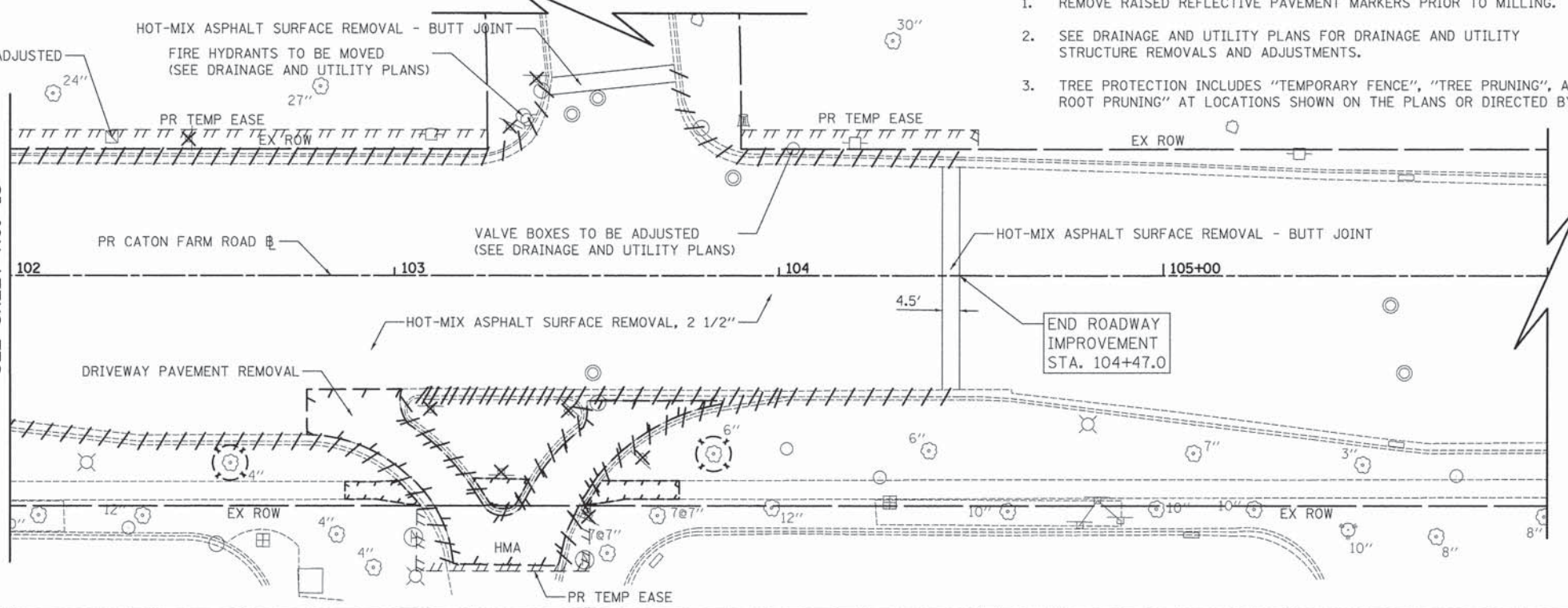
1. REMOVE RAISED REFLECTIVE PAVEMENT MARKERS PRIOR TO MILLING.
2. SEE DRAINAGE AND UTILITY PLANS FOR DRAINAGE AND UTILITY STRUCTURE REMOVALS AND ADJUSTMENTS.
3. TREE PROTECTION INCLUDES "TEMPORARY FENCE", "TREE PRUNING", AND "TREE ROOT PRUNING" AT LOCATIONS SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER.



LEGEND:

- PAVEMENT REMOVAL
- DRIVEWAY PAVEMENT REMOVAL
- MEDIAN REMOVAL (AS NOTED)
- SIDEWALK REMOVAL
- CURB AND GUTTER REMOVAL
- TREE REMOVAL (SIZE AS NOTED)
- SIGN PANEL REMOVAL OR RELOCATION (SEE SIGNING AND STRIPING PLAN)
- TREE PROTECTION (SEE NOTE #3)

MATCH LINE STA. 102+00
SEE SHEET NO. 13



END ROADWAY
IMPROVEMENT
STA. 104+47.0

CATON FARM ROAD

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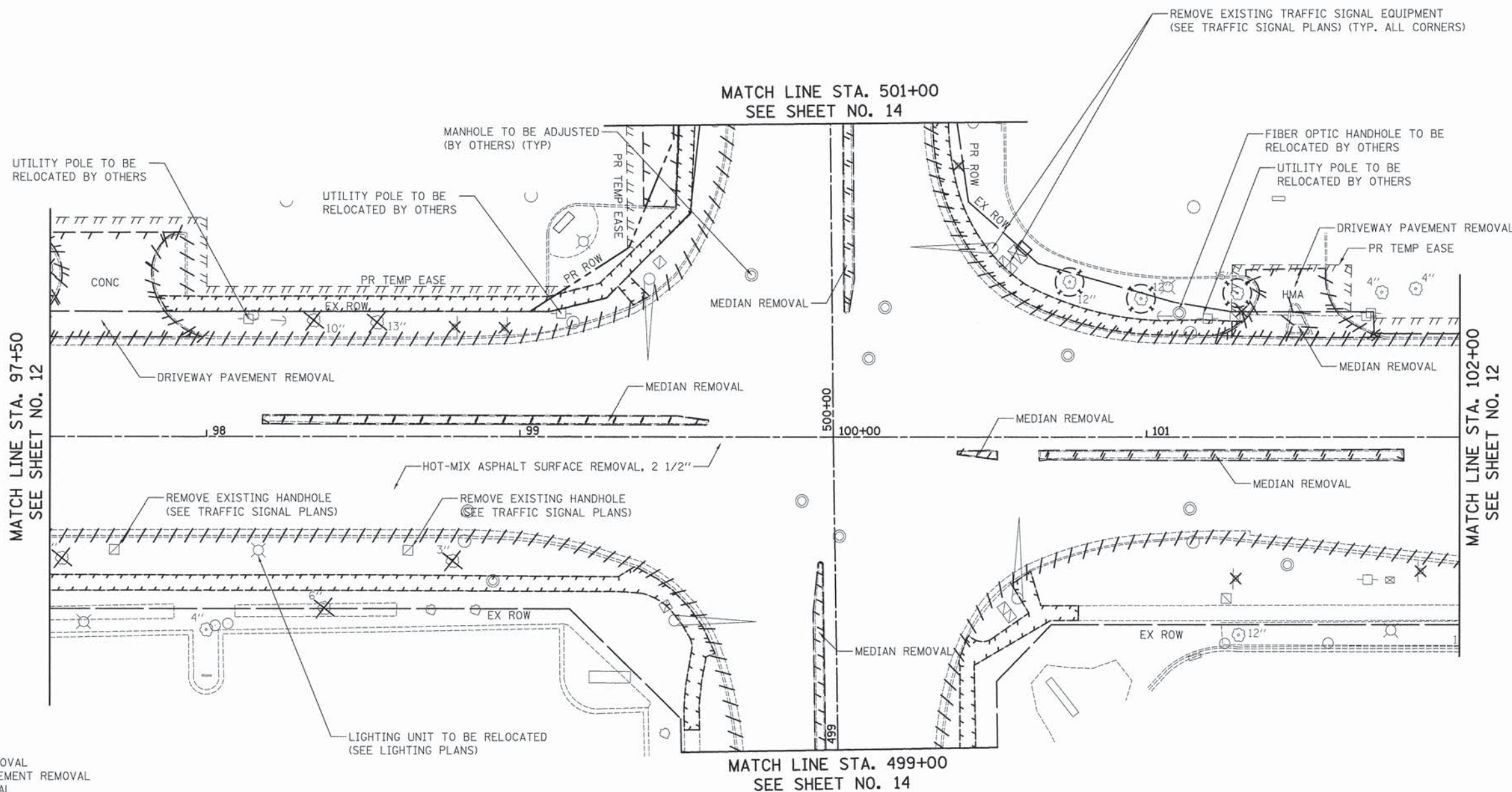
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	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**CATON FARM ROAD
REMOVAL PLAN**

SCALE: 1" = 20' SHEET 1 OF 3 SHEETS STA. 93+07.0 TO STA. 104+47.0

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	12
				CONTRACT NO. 61C29
ILLINOIS FED. AID PROJECT M-4003 (254)				



LEGEND:

- PAVEMENT REMOVAL
- DRIVEWAY PAVEMENT REMOVAL
- MEDIAN REMOVAL (AS NOTED)
- SIDEWALK REMOVAL
- CURB AND GUTTER REMOVAL
- TREE REMOVAL (SIZE AS NOTED)
- SIGN PANEL REMOVAL OR RELOCATION (SEE SIGNING AND STRIPING PLAN)
- TREE PROTECTION (SEE NOTE #3)

NOTES:

1. REMOVE RAISED REFLECTIVE PAVEMENT MARKERS PRIOR TO MILLING.
2. SEE DRAINAGE AND UTILITY PLANS FOR DRAINAGE AND UTILITY STRUCTURE REMOVALS AND ADJUSTMENTS.
3. TREE PROTECTION INCLUDES "TEMPORARY FENCE", "TREE PRUNING", AND "TREE ROOT PRUNING" AT LOCATIONS SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER.

CATON FARM ROAD

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




**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

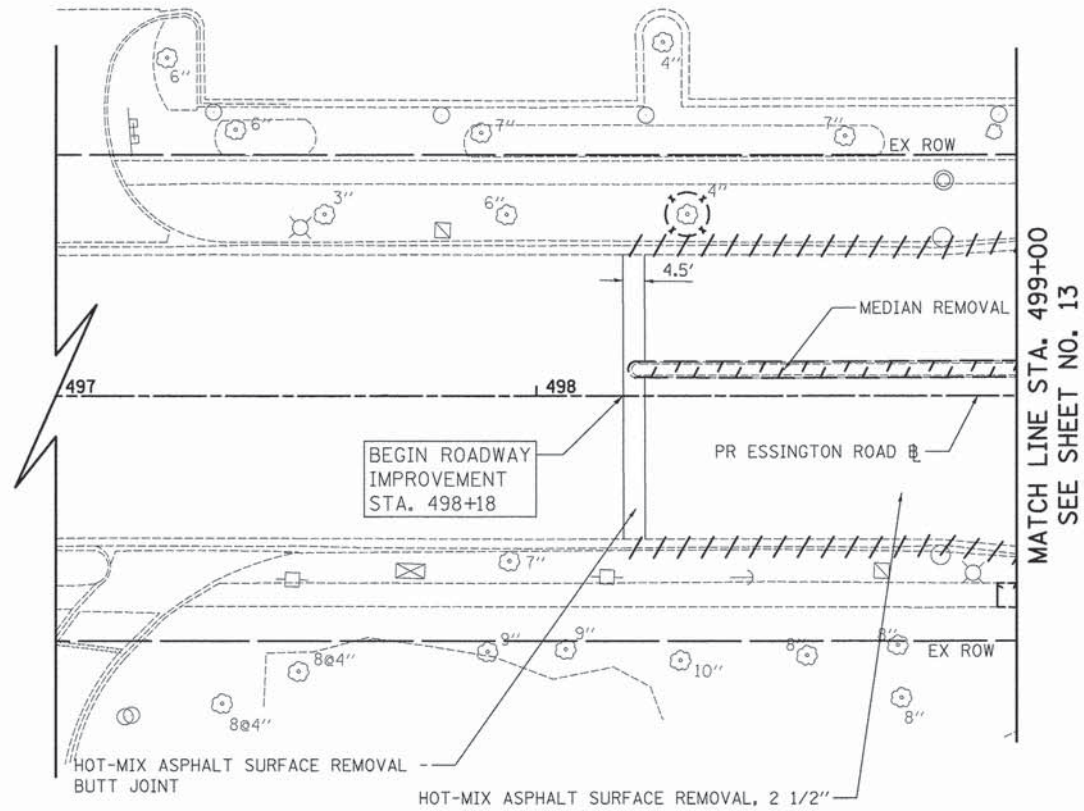
**CATON FARM ROAD
REMOVAL PLAN**

SCALE: 1" = 20' SHEET 2 OF 3 SHEETS STA. 97+50.0 TO STA. 102+00.0

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	13
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				

LEGEND:

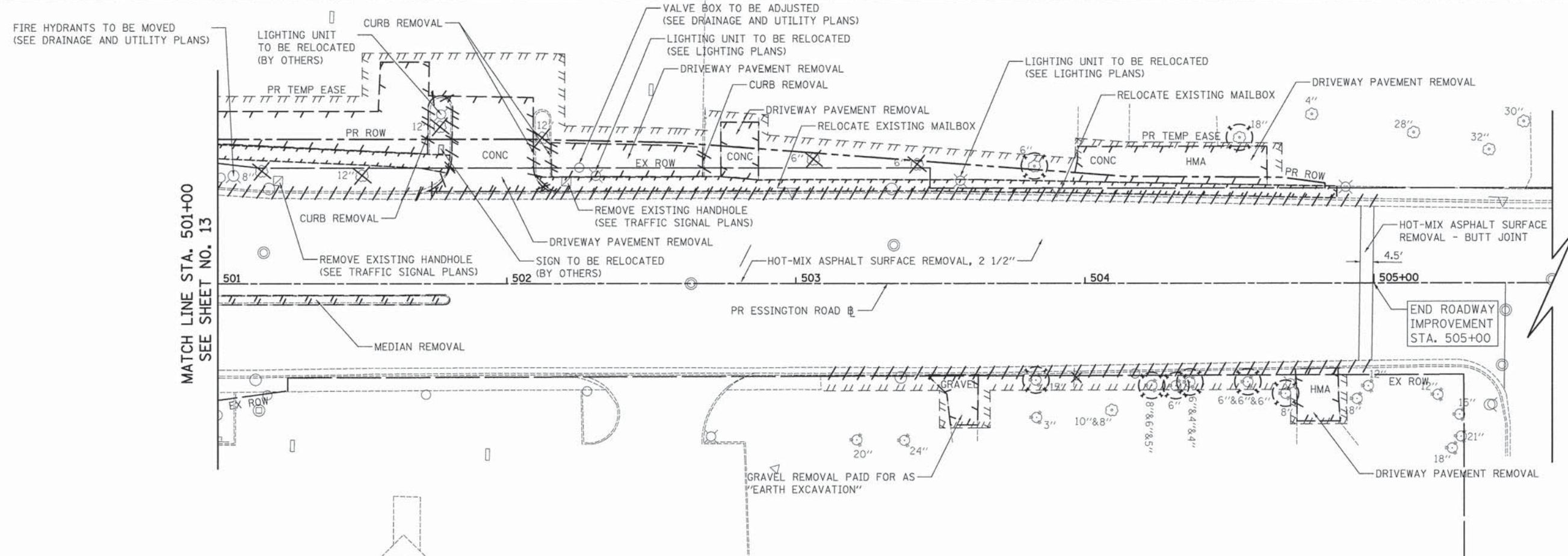
-  PAVEMENT REMOVAL
DRIVEWAY PAVEMENT REMOVAL
MEDIAN REMOVAL
(AS NOTED)
-  SIDEWALK REMOVAL
-  CURB AND GUTTER REMOVAL
-  TREE REMOVAL (SIZE AS NOTED)
-  SIGN PANEL REMOVAL OR RELOCATION
(SEE SIGNING AND STRIPING PLAN)
-  TREE PROTECTION (SEE NOTE #3)



NOTES:

1. REMOVE RAISED REFLECTIVE PAVEMENT MARKERS PRIOR TO MILLING.
2. SEE DRAINAGE AND UTILITY PLANS FOR DRAINAGE AND UTILITY STRUCTURE REMOVALS AND ADJUSTMENTS.
3. TREE PROTECTION INCLUDES "TEMPORARY FENCE", "TREE PRUNING", AND "TREE ROOT PRUNING" AT LOCATIONS SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER.

ESSINGTON ROAD



ESSINGTON ROAD

PLOT DRIVER = ...\\p1et\csm\fig\ven\p1et\p1et
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PLOT DATE = 12/3/2015	DATE - 12/07/15	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ESSINGTON ROAD
REMOVAL PLAN**

SCALE: 1" = 20' SHEET 3 OF 3 SHEETS STA. 498+18.0 TO STA. 504+83.0

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	11-00446-00-CH	WILL	110	14
				CONTRACT NO. 61C29
ILLINOIS FED. AID PROJECT M-4003 (254)				

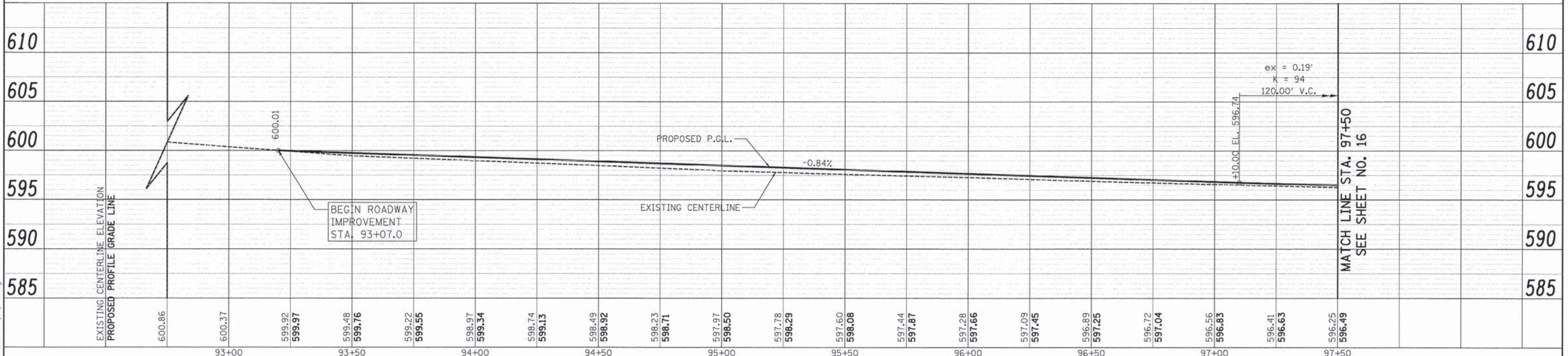
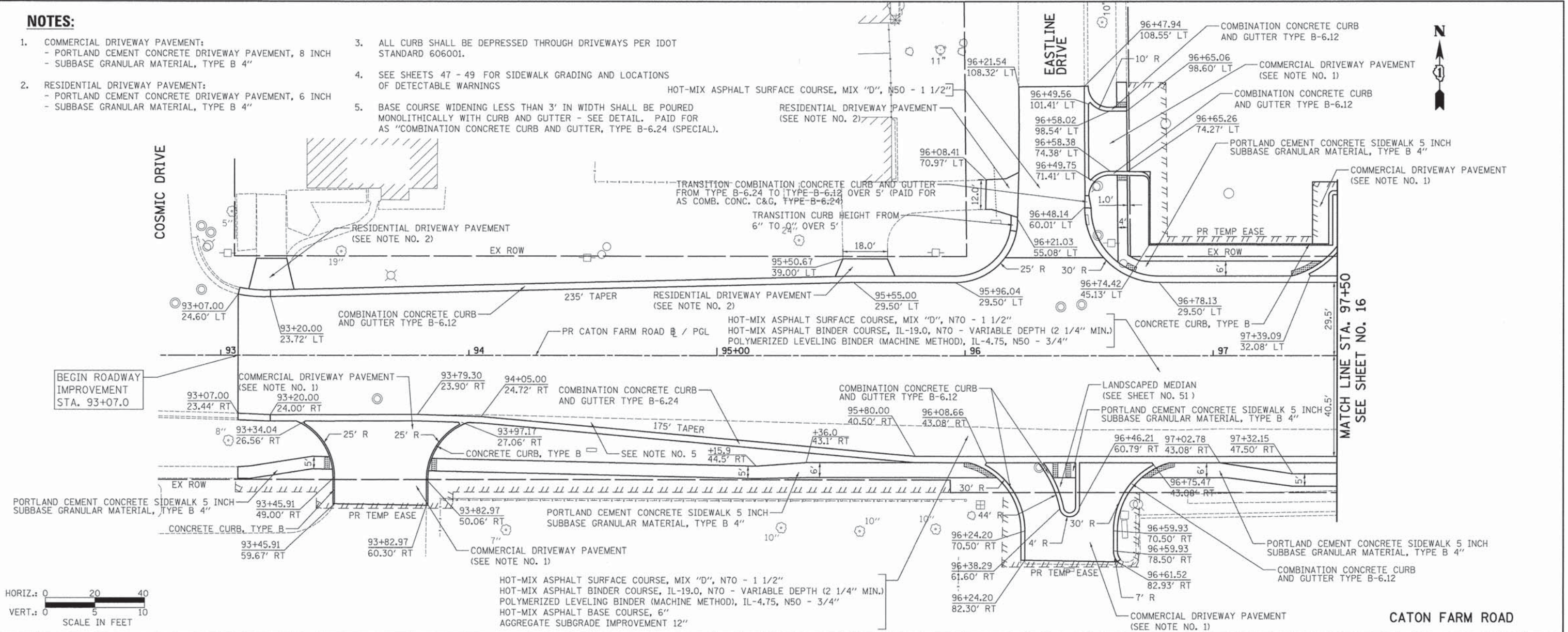
NOTES:

- COMMERCIAL DRIVEWAY PAVEMENT:
 - PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH
 - SUBBASE GRANULAR MATERIAL, TYPE B 4"
- RESIDENTIAL DRIVEWAY PAVEMENT:
 - PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH
 - SUBBASE GRANULAR MATERIAL, TYPE B 4"
- ALL CURB SHALL BE DEPRESSED THROUGH DRIVEWAYS PER IDOT STANDARD 606001.
- SEE SHEETS 47 - 49 FOR SIDEWALK GRADING AND LOCATIONS OF DETECTABLE WARNINGS
- BASE COURSE WIDENING LESS THAN 3' IN WIDTH SHALL BE POURED MONOLITHICALLY WITH CURB AND GUTTER - SEE DETAIL. PAID FOR AS "COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (SPECIAL).

DATE	
BY	
PLAN	
NO.	
DATE	
BY	
PROFILE	
NO.	

DATE	
BY	
PROFILE	
NO.	

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 PEN TABLE = ...
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	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD ROADWAY PLAN AND PROFILE			
SCALE: 1" = 20'	SHEET 1 OF 5 SHEETS	STA. 93+07.0 TO STA. 97+50.0	

F.A.U. RTE. 292	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 15
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

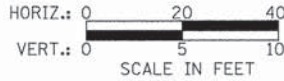
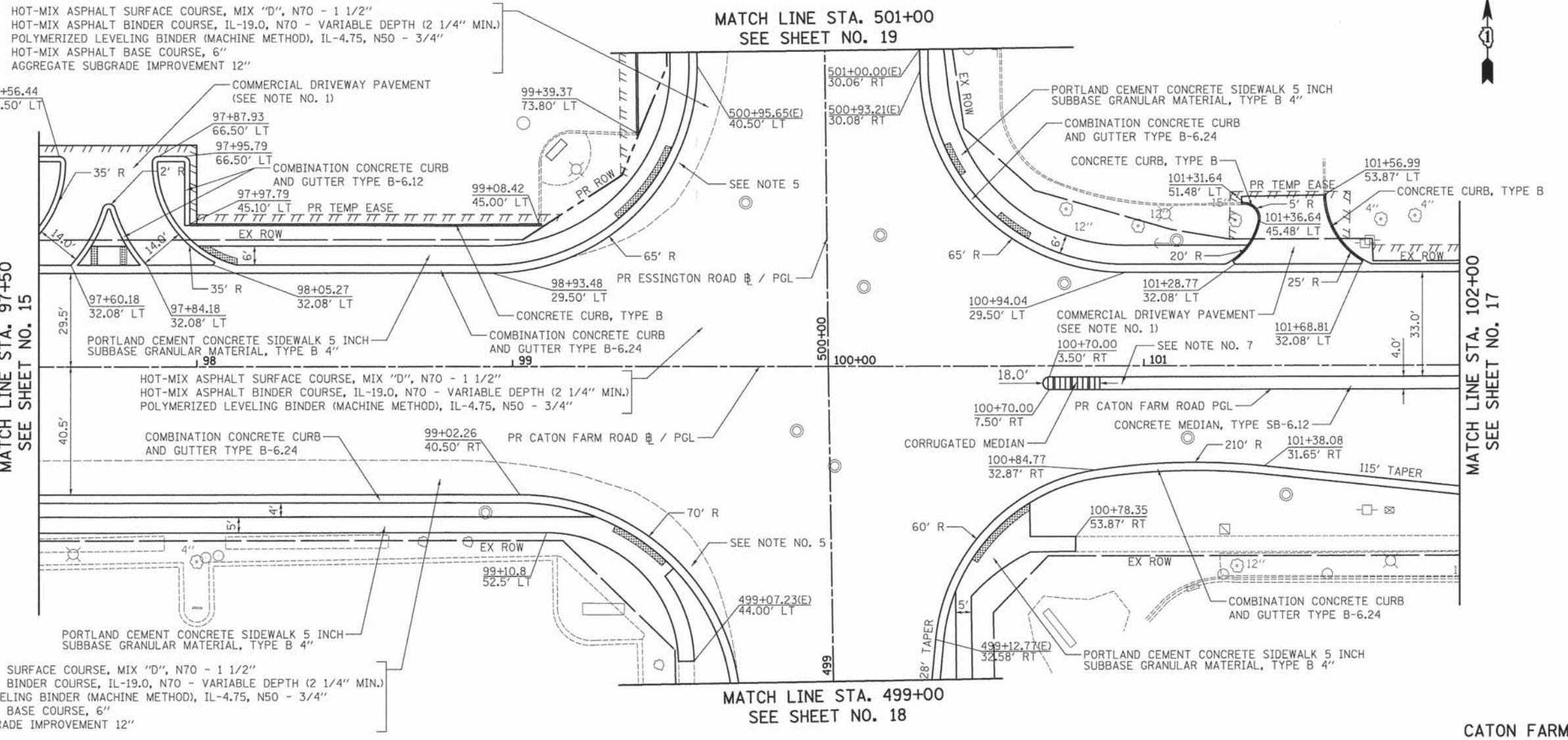
NOTES:

- COMMERCIAL DRIVEWAY PAVEMENT:
 - PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH
 - SUBBASE GRANULAR MATERIAL, TYPE B 4"
- RESIDENTIAL DRIVEWAY PAVEMENT:
 - PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH
 - SUBBASE GRANULAR MATERIAL, TYPE B 4"
- ALL CURB SHALL BE DEPRESSED THROUGH DRIVEWAYS PER IDOT STANDARD 606001.
- SEE SHEETS 47 - 49 FOR SIDEWALK GRADING AND LOCATIONS OF DETECTABLE WARNINGS
- ALL CALL-OFFS NOTATED WITH (E) ARE MEASURED FROM THE ESSINGTON ROAD PROPOSED ALIGNMENT.
- BASE COURSE WIDENING LESS THAN 3' IN WIDTH SHALL BE POURED MONOLITHICALLY WITH CURB AND GUTTER - SEE DETAIL. PAID FOR AS "COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (SPECIAL).
- RAMPED MEDIAN TRANSITION PER IDOT STANDARD 606301. PAID FOR AS "CONCRETE MEDIAN, TYPE SB-6.12"

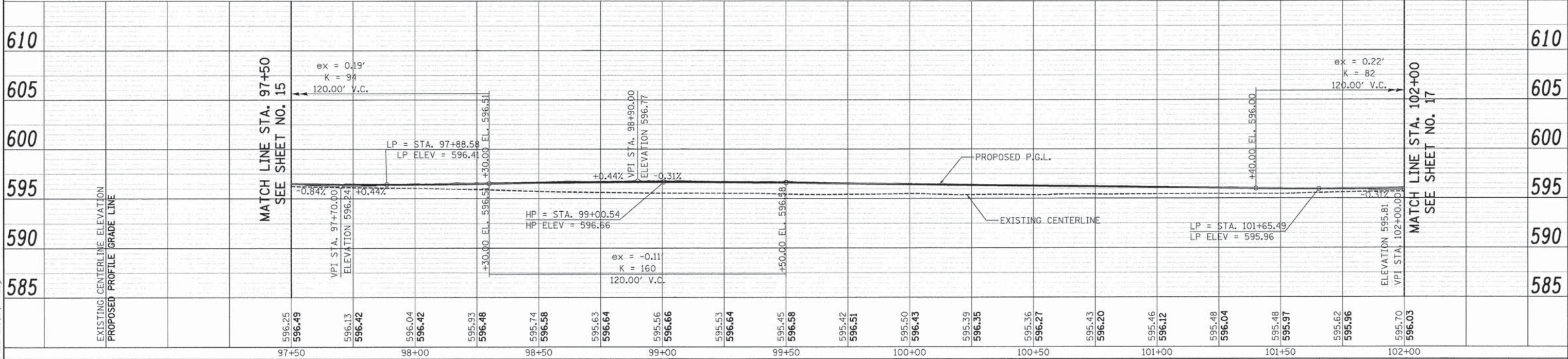
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PROFILE	DATE	BY

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HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - VARIABLE DEPTH (2 1/4" MIN.)
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4"
 HOT-MIX ASPHALT BASE COURSE, 6"
 AGGREGATE SUBGRADE IMPROVEMENT 12"



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PLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CATON FARM ROAD ROADWAY PLAN AND PROFILE			
SCALE: 1" = 20'	SHEET 2 OF 5 SHEETS	STA. 97+50.0 TO STA. 102+00.0	

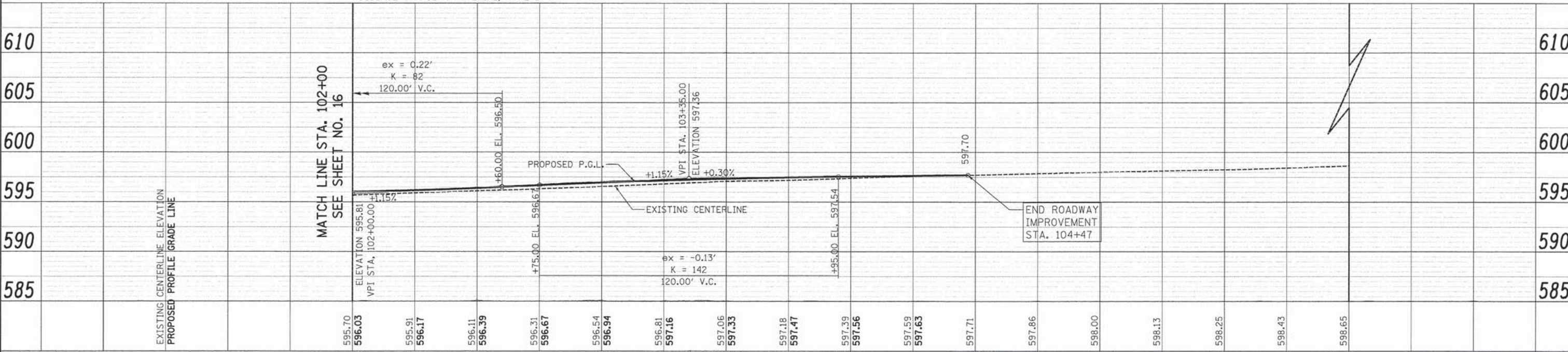
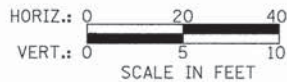
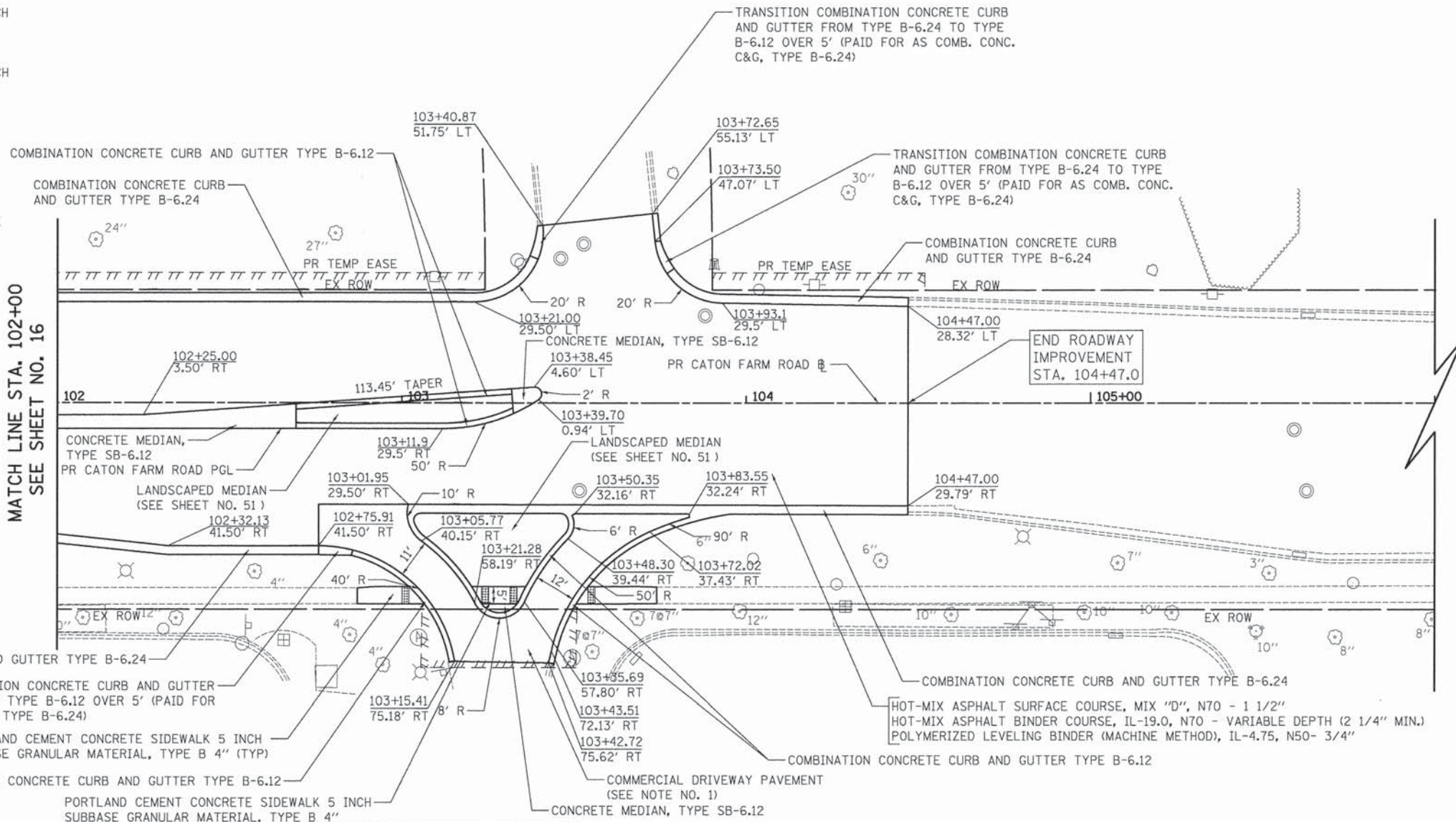
F.A.U. RTE. 292	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 16
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

NOTES:

- COMMERCIAL DRIVEWAY PAVEMENT:
 - PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH
 - SUBBASE GRANULAR MATERIAL, TYPE B 4"
- RESIDENTIAL DRIVEWAY PAVEMENT:
 - PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH
 - SUBBASE GRANULAR MATERIAL, TYPE B 4"
- ALL CURB SHALL BE DEPRESSED THROUGH DRIVEWAYS PER IDOT STANDARD 606001.
- SEE SHEETS 47 - 49 FOR SIDEWALK GRADING AND LOCATIONS OF DETECTABLE WARNINGS
- BASE COURSE WIDENING LESS THAN 3' IN WIDTH SHALL BE POURED MONOLITHICALLY WITH CURB AND GUTTER - SEE DETAIL. PAID FOR AS "COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (SPECIAL).

DATE	BY
DATE	BY
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PEN TABLE = ...
FILE NAME = ...



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

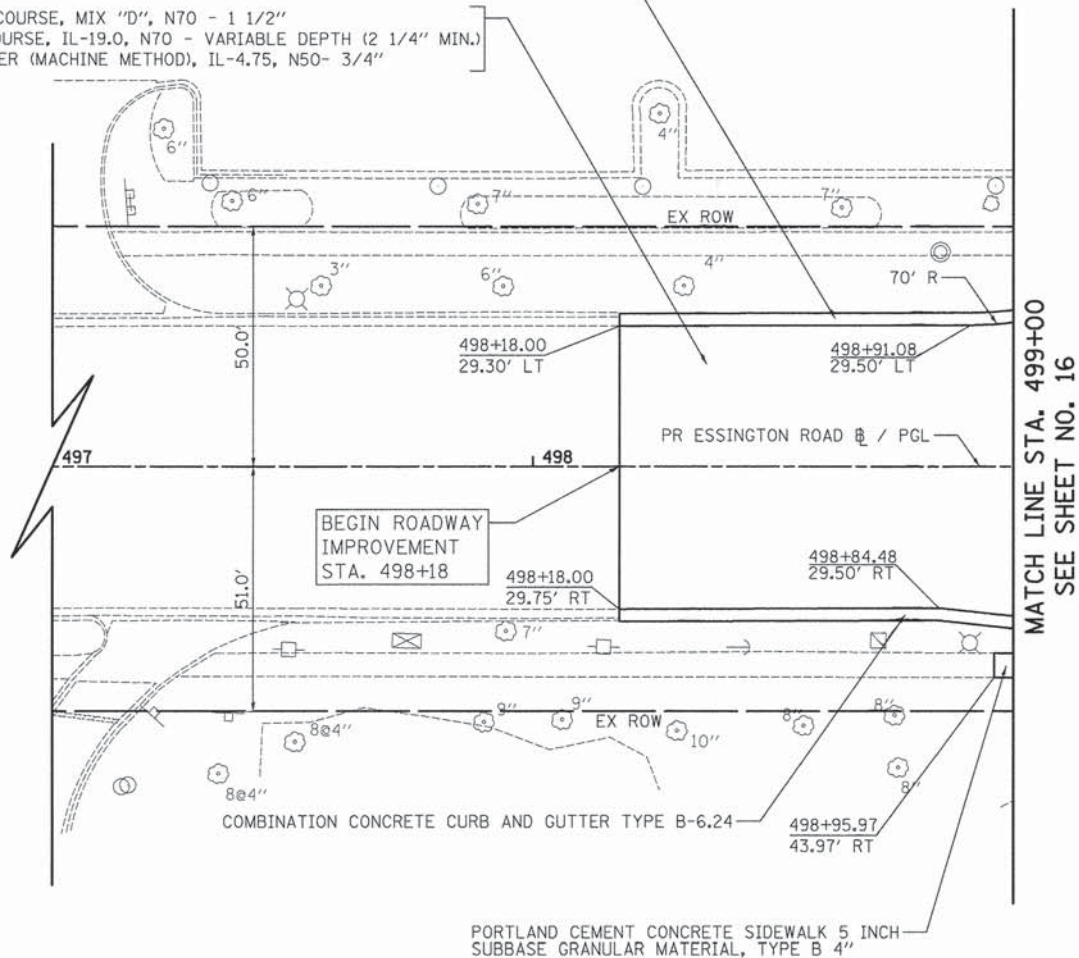
CATON FARM ROAD
ROADWAY PLAN AND PROFILE

SCALE: 1" = 20' SHEET 3 OF 5 SHEETS STA. 102+00.0 TO STA. 104+47.0

F.A.U. R.T.E. 292	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 17
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.24

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 - VARIABLE DEPTH (2 1/4" MIN.)
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50- 3/4"



MATCH LINE STA. 499+00
SEE SHEET NO. 16

NOTES:

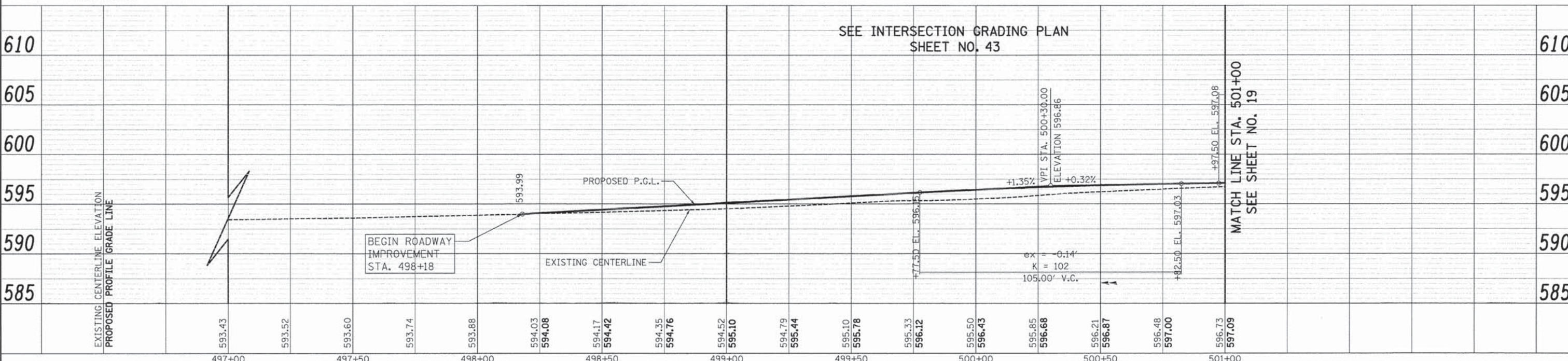
- COMMERCIAL DRIVEWAY PAVEMENT:
- PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH
- SUBBASE GRANULAR MATERIAL, TYPE B 4"
- RESIDENTIAL DRIVEWAY PAVEMENT:
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- SUBBASE GRANULAR MATERIAL, TYPE B 4"
- ALL CURB SHALL BE DEPRESSED THROUGH DRIVEWAYS PER IDOT STANDARD 606001.
- SEE SHEETS 47 - 49 FOR SIDEWALK GRADING AND LOCATIONS OF DETECTABLE WARNINGS
- BASE COURSE WIDENING LESS THAN 3' IN WIDTH SHALL BE POURED MONOLITHICALLY WITH CURB AND GUTTER - SEE DETAIL. PAID FOR AS "COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (SPECIAL).

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	RT. OF WAY CHECKED		
	CADD FILE NAME		
	NO.		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	GRADED		
	STRUCTURE NOTATIONS CHECKED		
	NO.		



ESSINGTON ROAD



SEE INTERSECTION GRADING PLAN
SHEET NO. 43

MATCH LINE STA. 501+00
SEE SHEET NO. 19

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PEN TABLE = ...
FILE NAME = ...



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PLLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ESSINGTON ROAD
ROADWAY PLAN AND PROFILE

SCALE: 1" = 20' SHEET 4 OF 5 SHEETS STA. 498+18.0 TO STA. 501+00.0

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	11-00446-00-CH	WILL	110	18
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

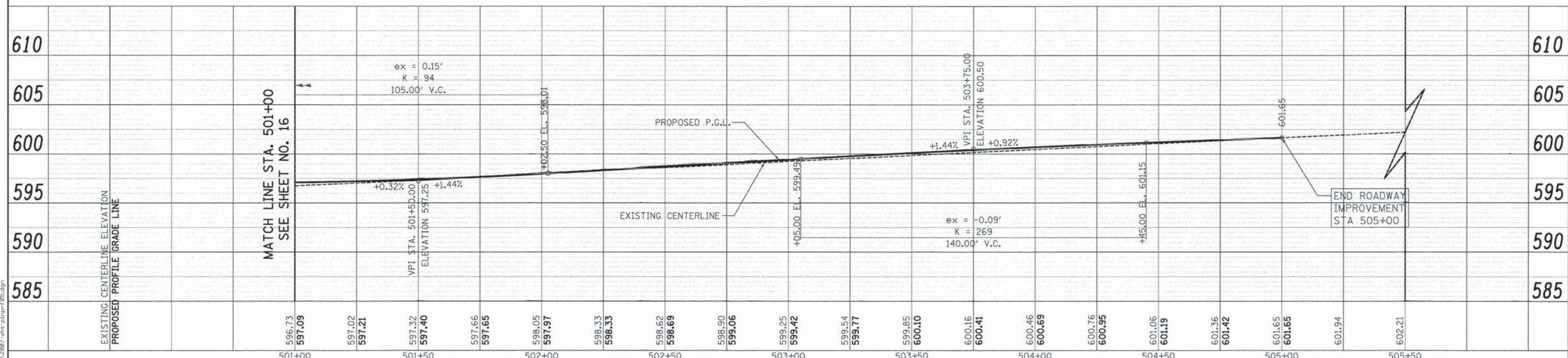
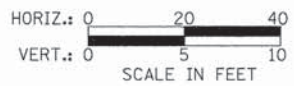
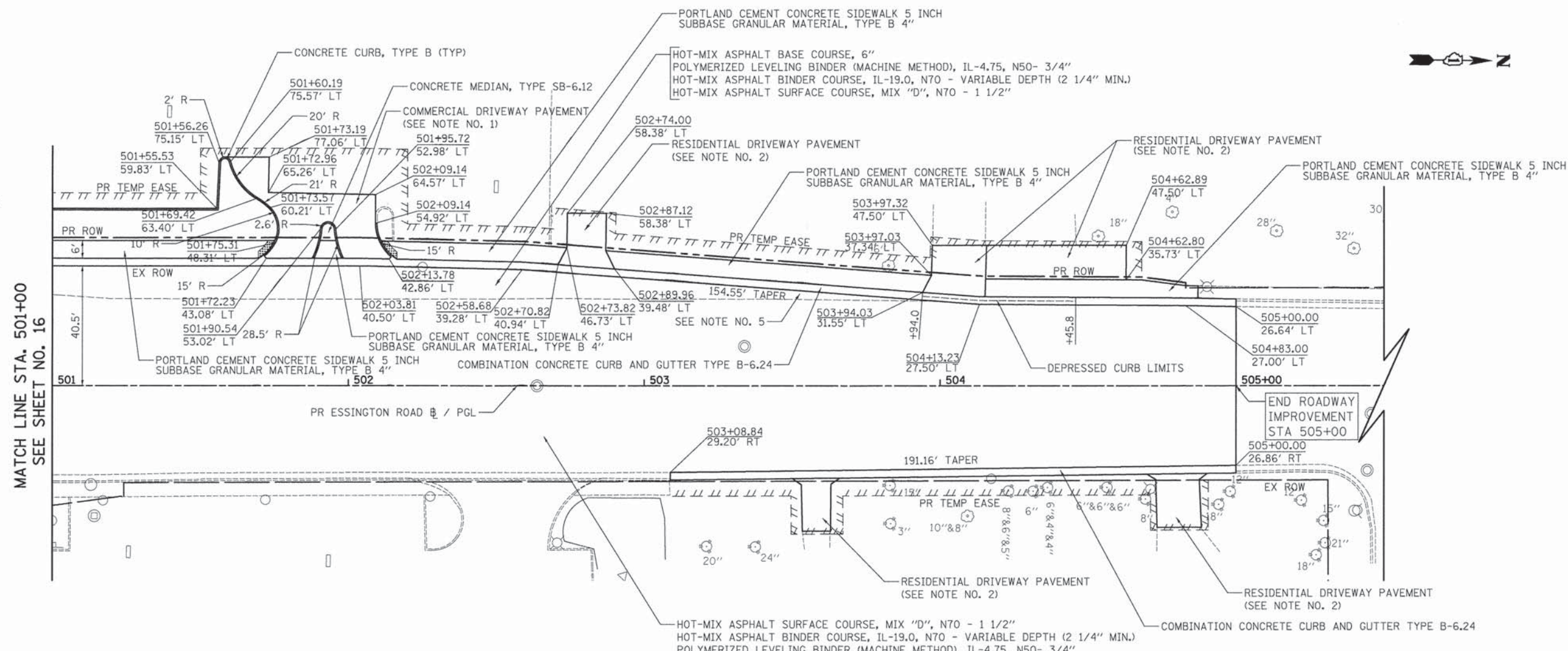
NOTES:

- COMMERCIAL DRIVEWAY PAVEMENT:
 - PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH
 - SUBBASE GRANULAR MATERIAL, TYPE B 4"
- RESIDENTIAL DRIVEWAY PAVEMENT:
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 - SUBBASE GRANULAR MATERIAL, TYPE B 4"
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- SEE SHEETS 47 - 49 FOR SIDEWALK GRADING AND LOCATIONS OF DETECTABLE WARNINGS
- BASE COURSE WIDENING LESS THAN 3' IN WIDTH SHALL BE POURED MONOLITHICALLY WITH CURB AND GUTTER - SEE DETAIL. PAID FOR AS "COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (SPECIAL).

PLAN	SURVEYED	DATE
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	GRADES	
	BY	
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	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES	
	BY	
	NO.	
	FILE NAME	

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USER NAME = tgb	DESIGNED - JSM	REVISED -
	DRAWN - EDS	REVISED -
	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ESSINGTON ROAD
ROADWAY PLAN AND PROFILE
SCALE: 1" = 20'
SHEET 5 OF 5 SHEETS
STA. 501+00.0 TO STA. 504+83.0

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	11-00446-00-CH	WILL	110	19
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

PRE-STAGE 1 (NOT ILLUSTRATED)

1. INSTALL AND ACTIVATE TEMPORARY TRAFFIC SIGNALS AT THE INTERSECTION OF CATON FARM ROAD AND ESSINGTON ROAD. (NOTE: THE EXISTING SIGNALS SHALL BE TURNED OFF AT THE SAME TIME THE TEMPORARY SIGNALS ARE ACTIVATED.)
2. REMOVE EXISTING SIGNAL POLES AND EQUIPMENT.
3. INSTALL STREET LIGHTING FOUNDATIONS AND RUN UNIT DUCT. RELOCATE EXISTING STREET LIGHTING POLES SHOWN FOR RELOCATION. LIGHTING MUST BE OPERATIONAL AT THE CONCLUSION OF EACH WORKING DAY AND THRU THE DURATION OF THE PROJECT.
4. REMOVE EXISTING MEDIANS AND CONSTRUCT AGGREGATE SUBGRADE AND BASE COURSE FOR PROPOSED MEDIAN USING DAILY LANE CLOSURES IN ACCORDANCE WITH IDOT STANDARD 701701.
5. REMOVE EXISTING RAISED REFLECTIVE PAVEMENT MARKERS.
6. CONSTRUCT ALL STORM SEWER AND DRAINAGE STRUCTURES WITHIN THE EXISTING PAVEMENT USING DAILY LANE CLOSURES.

STAGE 1

1. PLACE ALL CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS, AND BARRICADES. ADJUST TEMPORARY TRAFFIC SIGNALS AND SHIFT TRAFFIC AS INDICATED.
2. CLOSE RIGHT-IN RIGHT-OUT DRIVEWAY BY BANK ON ESSINGTON ROAD.
3. REMOVE EXISTING CURB AND GUTTER, SIDEWALK, AND DRIVEWAYS AS SHOWN.
4. CONSTRUCT STORM SEWERS AND DRAINAGE STRUCTURES IN CURBLINE AS SHOWN.
5. CONSTRUCT PROPOSED AGGREGATE SUBGRADE.
6. CONSTRUCT ALL CURB AND GUTTER, SIDEWALK, AND DRIVEWAYS OUTSIDE THE EXISTING EDGES OF PAVEMENT. DAILY LANE CLOSURES, WHEN REQUIRED, SHALL BE IN ACCORDANCE WITH IDOT STANDARD 701606 OR 701701.
7. CONSTRUCT HMA BASE COURSE AT PAVEMENT WIDENING LOCATIONS.
8. MILL THE EXISTING SURFACE ON ESSINGTON ROAD AND CATON FARM ROAD.
9. PLACE LEVELING BINDER AND BINDER WITHIN LIMITS OF STAGE 1 WIDENING AND EXISTING PAVEMENT INTO STAGE #2 WORKZONE, MAINTAINING TRAFFIC PER STANDARD 701606.

STAGE 2

1. PLACE ALL CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS, AND BARRICADES. ADJUST TEMPORARY TRAFFIC SIGNALS AND SHIFT TRAFFIC AS INDICATED.
2. REMOVE EXISTING CURB AND GUTTER, SIDEWALK, AND DRIVEWAYS AS SHOWN.
3. CONSTRUCT STORM SEWERS AND DRAINAGE STRUCTURES IN CURBLINE AS SHOWN.
4. CONSTRUCT PROPOSED AGGREGATE SUBGRADE.
5. CONSTRUCT ALL CURB AND GUTTER, SIDEWALK, AND DRIVEWAYS OUTSIDE THE EXISTING EDGES OF PAVEMENT. DAILY LANE CLOSURES, WHEN REQUIRED, SHALL BE IN ACCORDANCE WITH IDOT STANDARD 701606 OR 701701.
6. CONSTRUCT ALL CURB AND GUTTER, SIDEWALK, AND DRIVEWAYS OUTSIDE THE EXISTING EDGES OF PAVEMENT. DAILY LANE CLOSURES, WHEN REQUIRED, SHALL BE IN ACCORDANCE WITH IDOT STANDARD 701606 OR 701701.
7. CONSTRUCT HMA BASE COURSE AT PAVEMENT WIDENING LOCATIONS.
8. PLACE REMAINING LEVELING BINDER AND BINDER WITHIN LIMITS OF STAGE 2 WIDENING, MAINTAINING TRAFFIC PER STANDARD 701606.

STAGE 3

1. PLACE ALL CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS, AND BARRICADES. ADJUST TEMPORARY TRAFFIC SIGNALS AND SHIFT TRAFFIC AS INDICATED.
2. REMOVE PAVEMENT FOR MEDIAN AND CONSTRUCT AGGREGATE SUBGRADE AND THE MEDIAN ON THE EAST LEG OF THE INTERSECTION. DAILY LANE CLOSURES, WHEN REQUIRED, SHALL BE IN ACCORDANCE WITH IDOT STANDARD 701606 OR 701701.

STAGE 4 (NOT ILLUSTRATED)

1. COMPLETE ALL LANDSCAPING.
2. PLACE SURFACE COURSE TO FINISHED GRADE, MAINTAINING TRAFFIC PER STANDARD 701606.
3. PLACE PERMANENT PAVEMENT MARKINGS, RAISED REFLECTIVE PAVEMENT MARKERS AND SIGNS.
4. REMOVE CONSTRUCTION SIGNS AND OPEN ALL LANES TO TRAFFIC.

MAINTENANCE OF TRAFFIC GENERAL NOTES

1. TRAFFIC CONTROL DEPICTED IN THESE PLANS AND THE APPLICABLE I.D.O.T. DETAILS AND STANDARDS ARE THE MINIMUM REQUIREMENTS. OTHER WORK OR SIGNING MAY BE REQUIRED BY THE ENGINEER. TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, DIVISION 700; APPLICABLE GUIDELINES IN THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS; AND APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL, UNLESS HEREIN REVISED.
2. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND TRAFFIC CONTROL DEVICES SHALL FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
3. ALL CONSTRUCTION SIGNS SHALL HAVE FLUORESCENT ORANGE BACKGROUNDS.
4. ALL SIGNS SHALL BE MOUNTED ON METAL POSTS, 7 FEET ABOVE THE EXISTING GROUND AND DRIVEN A MINIMUM OF 3 FEET INTO THE GROUND. A J.U.L.I.E. LOCATE SHALL BE PERFORMED PRIOR TO THE INSTALLATION OF THE POSTS.
5. BARRICADES WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS WILL BE REQUIRED ADJACENT TO PAVEMENT EDGES WHERE WIDENING, CURB AND GUTTER OR OVERLAYING WORK IS BEING DONE, AS SPECIFIED IN SECTION 701 OF THE STANDARD SPECIFICATIONS. SPACING SHALL BE AS SHOWN ON THE CONSTRUCTION STAGING PLANS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOPS OF THE BARRICADES ARE IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF STANDARD 701901.
6. ALL BARRICADES AT LANE DIVERSIONS WITHIN TAPER SECTIONS SHALL HAVE DIRECTION INDICATOR PANELS.
7. TYPE II BARRICADES OR DRUMS EQUIPPED WITH ONE-WAY FLASHING LIGHTS WILL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER STRUCTURES, AND AT ANY OTHER LOCATIONS DESIGNATED BY THE ENGINEER OR LAW ENFORCEMENT AGENCIES. BARRICADES SHALL BE PLACED AT 50' CENTERS ALONG TANGENTS, 25' ALONG TAPERS AND 10' AROUND RADII.
8. DRUMS SHALL HAVE ALTERNATING REFLECTORIZED TYPE AA OR TYPE AP FLUORESCENT ORANGE AND REFLECTORIZED WHITE HORIZONTAL, CIRCUMFERENTIAL STRIPES.
9. DRUMS AND BARRICADES SHALL MEET THE REQUIREMENTS OF THE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) REPORT 350 AND THE SUPPLEMENTAL SPECIAL PROVISION "WORK ZONE TRAFFIC CONTROL DEVICES".
10. TYPE III BARRICADES ARE TO BE PLACED IN ACCORDANCE WITH STANDARD 701901 UNLESS AUTHORIZED BY THE ENGINEER TO USE AN ALTERNATE ARRANGEMENT.
11. THE CONTRACTOR SHALL INFORM THE ENGINEER OF ANY STAGE CHANGE AT LEAST TWO WEEKS IN ADVANCE OF THE CHANGE.
12. EXISTING TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE REMOVED OR RELOCATED BY THE CONTRACTOR AFTER THE TRAFFIC CONTROL REQUIREMENTS ARE MET OR AS AUTHORIZED BY THE ENGINEER; ANY SIGNS OR DEVICES LEFT IN PLACE ARE TO BE PROTECTED FROM DAMAGE AND MAINTAINED. ANY DAMAGE CAUSED BY HIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.
13. THE FIRST WARNING SIGNS IN EACH DIRECTION OF TRAVEL SHALL BE EQUIPPED WITH MONO-DIRECTIONAL AMBER FLASHING LIGHTS DURING HOURS OF DARKNESS. FLAGS ARE OPTIONAL.
14. EXCEPT FOR APPROVED CLOSURES AS DEPICTED ON THE MAINTENANCE OF TRAFFIC PLANS, ALL ROADS SHALL BE KEPT OPEN TO TRAFFIC DURING THE ENTIRE CONSTRUCTION PERIOD. THE CONTRACTOR MAY CLOSE ONE LANE OF TRAFFIC (BECAUSE OF CONSTRUCTION) ONLY BETWEEN THE HOURS OF 9:00 AM AND 3:00 PM.
15. "FRESH OIL" SIGNS (W21-2-484B) WITH DATE SIGNS SHALL BE ERECTED 48 HOURS PRIOR TO PRIMING. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
16. "WORKERS" SIGNS SHALL ONLY BE ERECTED WHEN WORKERS ARE PRESENT. SIGN MUST BE COVERED OR REMOVED WHEN NO WORKERS ARE PRESENT.
17. THE CONTRACTOR SHALL ERECT TEMPORARY STREET NAME SIGNS ON METAL POSTS THROUGHOUT CONSTRUCTION TO THE SATISFACTION OF THE ENGINEER. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
18. TEMPORARY PAVEMENT MARKING TAPE SHALL BE USED ON ALL SURFACES OUTSIDE OF THE PROJECT LIMITS AND ON THE FINAL PAVEMENT SURFACE. THIS WORK SHALL BE PAID FOR AS "WET REFLECTIVE TEMPORARY TAPE TYPE III" OF THE SIZE SPECIFIED.
19. ARROW BOARDS WILL BE REQUIRED WHEN IMPLEMENTING ALL LANE CLOSURES, AND SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
20. THE COST OF SUPPLYING, ERECTING, AND MAINTAINING BARRICADES, DRUMS, WARNING LIGHTS, AND SIGNS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)". QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS, TEMPORARY PAVEMENT MARKINGS, AND WORK ZONE PAVEMENT MARKING REMOVAL ARE NOT INCLUDED IN THE ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)" AND SHALL BE MEASURED SEPARATELY FOR PAYMENT.
21. THE "ROAD CLOSED" (R11-2) SIGNS SHALL BE MOUNTED ON THE TYPE III BARRICADES. ALL TYPE III BARRICADES SHALL HAVE 2 AMBER TYPE A-LOW INTENSITY FLASHING LIGHTS SPACED NEAR THE CENTERLINES OF THE SUPPORTS.
22. A QUANTITY FOR "CHANGEABLE MESSAGE SIGN" HAS BEEN INCLUDED FOR USE WHEN DIRECTED BY THE ENGINEER.
23. ACCESS TO PROPERTIES SHALL BE MAINTAINED AT ALL TIMES BY STAGE CONSTRUCTING THE IMPROVEMENTS IN FRONT OF ENTRANCES. A QUANTITY FOR "TEMPORARY INFORMATION SIGNING" HAS BEEN INCLUDED FOR USE WHEN DIRECTED BY THE ENGINEER TO PROVIDE GUIDANCE SIGNS WHEN A DRIVEWAY MUST BE CLOSED TEMPORARILY FOR CONSTRUCTION OF THE DRIVEWAY APRON. PROPERTIES WITH MULTIPLE ENTRANCES SHALL ONLY HAVE ONE ENTRANCE CLOSED AT A TIME.
24. A QUANTITY OF ONE "AGGREGATE FOR TEMPORARY ACCESS (ROAD)" HAS BEEN INCLUDED FOR USE IN PROVIDING PEDESTRIAN ACCESS ACROSS THE WIDENING AT THE INTERSECTION OF CATON FARM ROAD AND ESSINGTON ROAD.
25. ALL INTERSECTING STREETS SHALL BE KEPT OPEN TO TRAFFIC, AS DIRECTED BY THE ENGINEER.
26. THE "NEW LANES OPEN STOP HERE" SIGNS SHALL BE PLACED AT ALL DRIVEWAYS AND SIDEROADS WHEN DIRECTED BY THE ENGINEER. THE SIGNS SHALL REMOVED WHEN DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
27. ALL TRAFFIC CONTROL REQUIRED BETWEEN THE CONTRACT COMPLETION DATE AND THE END OF GUARANTEED WORKING DAYS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)".

ACCESS MAINTENANCE NOTES

MAINTAINING ACCESS TO DRIVEWAYS IS OF THE UTMOST IMPORTANCE TO THE CITY. ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUB-STAGING THE CONSTRUCTION OF DRIVEWAYS. THE CONTRACTOR SHALL FOLLOW THESE PROCEDURES TO ENSURE PROPER DRIVEWAY ACCESS WHEN DIRECTED BY THE ENGINEER:

1. THE CONTRACTOR SHALL WORK WITH ADJACENT BUSINESS OWNERS TO DETERMINE DRIVEWAY RECONSTRUCTION SCHEDULING. ALL DRIVEWAY CLOSURES SHALL BE APPROVED BY THE ENGINEER.
2. TEMPORARY DRIVES SHALL BE CONSTRUCTED USING 100% RECYCLED ASPHALT PAVEMENT. THE WIDTH OF THE DRIVE SHALL BE DETERMINED BY THE ENGINEER. THE CONTRACTOR SHALL NOTE THAT THE TEMPORARY DRIVEWAY WIDTH MAY EXCEED THE WIDTH OF THE EXISTING DRIVEWAY.

THE COST OF PLACING, MAINTAINING AND REMOVING TEMPORARY DRIVES SHALL BE INCLUDED IN THE COST OF "TEMPORARY ACCESS (COMMERCIAL ENTRANCE)".
3. A 4" PVC DRAIN SHALL BE PLACED UNDER TEMPORARY DRIVES TO PROVIDE POSITIVE DRAINAGE WHEN THE ROADWAY IS EXCAVATED. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF "DRIVEWAY PAVEMENT REMOVAL".
4. ALL BARRICADES REQUIRED TO SUB-STAGE DRIVEWAY CONSTRUCTION AND MAINTAIN ACCESS TO DRIVEWAYS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".

SIDEWALK MAINTENANCE NOTES

1. THE SIDEWALK ON ONE SIDE OF THE STREET MUST REMAIN OPEN AND ACCESSIBLE AT ALL TIMES. SIGNING DIRECTING PEDESTRIANS TO THE OPEN SIDEWALK SHALL BE IN ACCORDANCE WITH IDOT HIGHWAY STANDARD 701801-05. THE REQUIRED SIGNING SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
2. TEMPORARY AGGREGATE REQUIRED TO MAINTAIN PEDESTRIAN ACCESS ACROSS THE WORKZONE SHALL BE INCLUDED IN THE COST OF "TEMPORARY ACCESS (ROAD)".

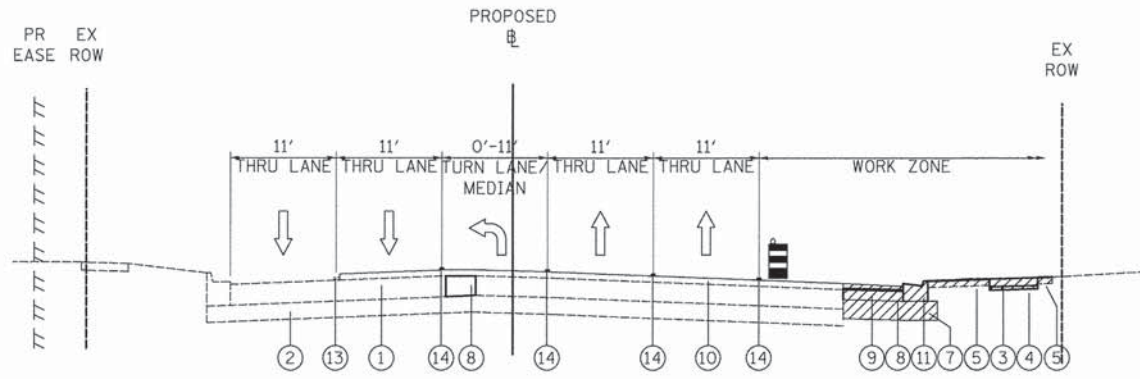
DROP-OFF REQUIREMENTS

WHEN WORKING ADJACENT TO THE ROAD, DAILY LANE CLOSURES IN ACCORDANCE WITH IDOT STANDARD 701701 SHALL BE ALLOWED BETWEEN THE HOURS OF 9:00AM AND 3:00 PM. DROP-OFFS ADJACENT TO THE TRAVEL LANES SHALL BE KEPT TO A MINIMUM. DROP-OFFS GREATER THAN 18" WILL NOT BE ALLOWED WHEN TRAFFIC IS PRESENT IN THE ADJACENT LANE. THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE EXCAVATION REQUIRED FOR THE CONSTRUCTION OF THE WIDENING DURING THE HOURS THAT THE ADJACENT LANE IS CLOSED, AS NOTED ABOVE. PRIOR TO RE-OPENING THE LANE TO TRAFFIC THE CONTRACTOR SHALL PLACE SUFFICIENT MATERIAL TO REDUCE THE DROP-OFF TO LESS THAN 18". THE CONTRACTOR SHALL BE RESPONSIBLE TO DETERMINE THE AMOUNT OF WORK THAT CAN BE COMPLETED WITHIN THE TIME LIMIT OF THE DAILY LANE CLOSURE. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED TO COMPLY WITH THIS REQUIREMENT.



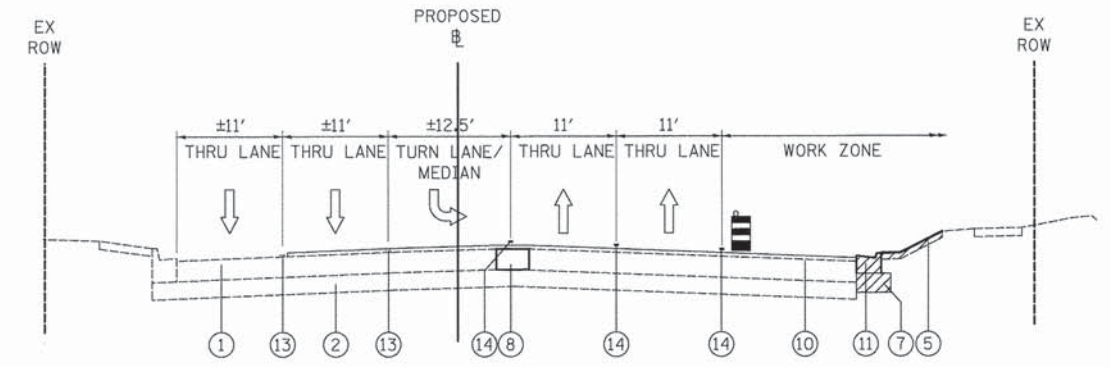
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													ILLINOIS FED. AID PROJECT M-4003 (254)	

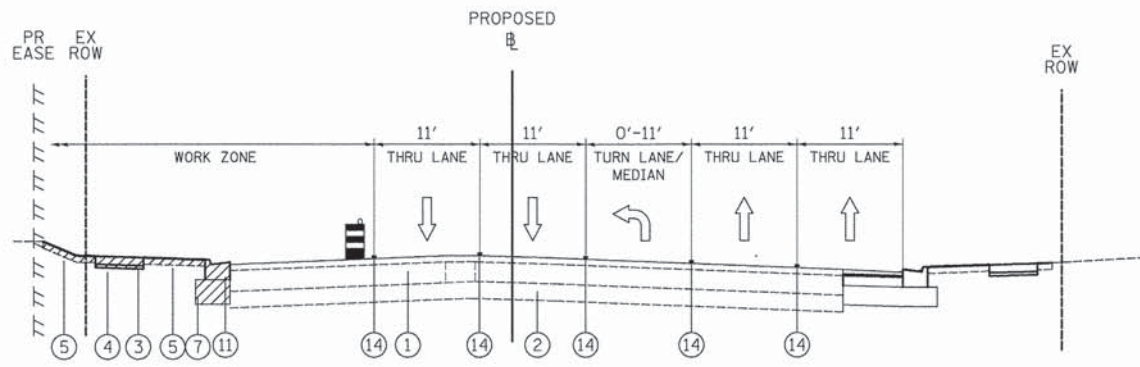


CATON FARM ROAD - STAGE 1 TYPICAL SECTION
WEST LEG LOOKING EAST

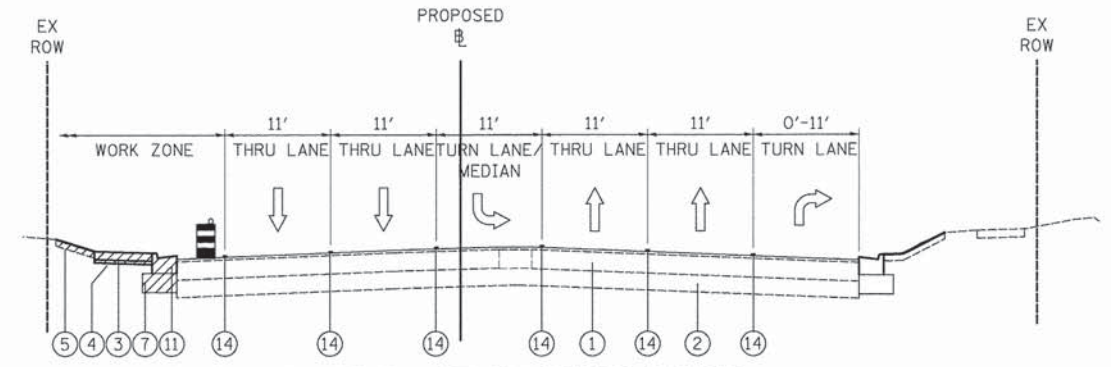
- LEGEND**
- ① EXISTING HMA PAVEMENT
 - ② EXISTING AGGREGATE SUBGRADE
 - ③ PROPOSED PCC SIDEWALK
 - ④ PROPOSED SUBBASE GRANULAR MATERIAL
 - ⑤ PROPOSED TOPSOIL
 - ⑥ NOT USED
 - ⑦ PROPOSED AGGREGATE SUBGRADE
 - ⑧ PROPOSED HMA BASE COURSE
 - ⑨ PROPOSED LEVELING BINDER
 - ⑩ PROPOSED HMA BINDER COURSE
 - ⑪ PROPOSED COMBINATION CONCRETE CURB AND GUTTER
 - ⑫ PROPOSED CONCRETE MEDIAN
 - ⑬ EXISTING PAVEMENT MARKINGS (SOLID, DOTTED, OR SKIP-DASH)
 - ⑭ TEMPORARY PAINT PAVEMENT MARKINGS (SOLID, DOTTED, OR SKIP-DASH)
- CONSTRUCTION ZONE
 - TRAFFIC FLOW
 - TYPE II BARRICADES OR DRUMS W/ MONO DIRECTIONAL STEADY BURN LIGHT



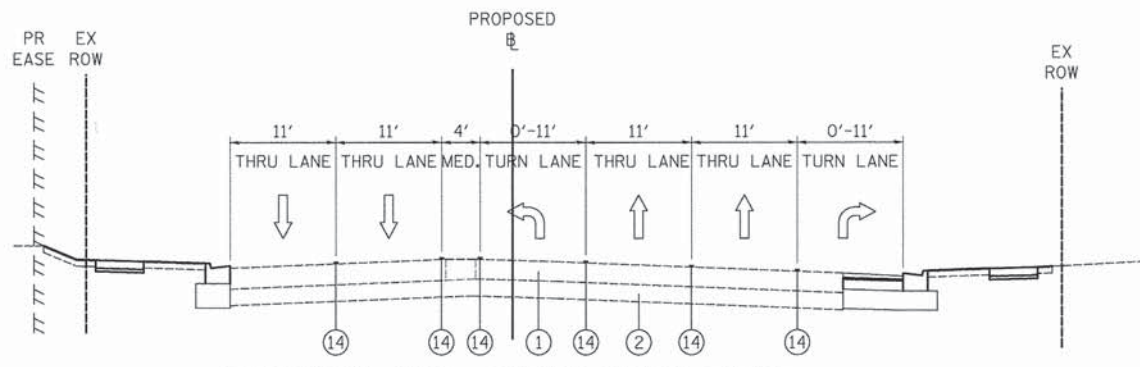
CATON FARM ROAD - STAGE 1 TYPICAL SECTION
EAST LEG LOOKING EAST



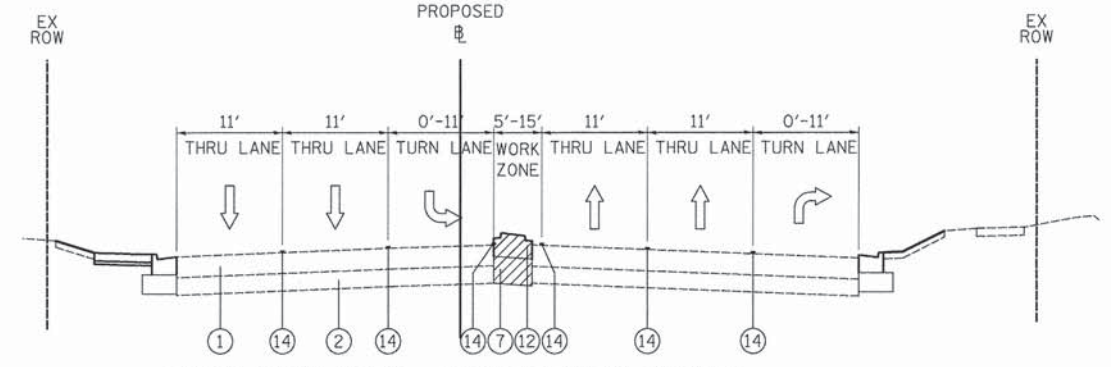
CATON FARM ROAD - STAGE 2 TYPICAL SECTION
WEST LEG LOOKING EAST



CATON FARM ROAD - STAGE 2 TYPICAL SECTION
EAST LEG LOOKING EAST



CATON FARM ROAD - STAGE 3 TYPICAL SECTION
WEST LEG LOOKING EAST



CATON FARM ROAD - STAGE 3 TYPICAL SECTION
EAST LEG LOOKING EAST

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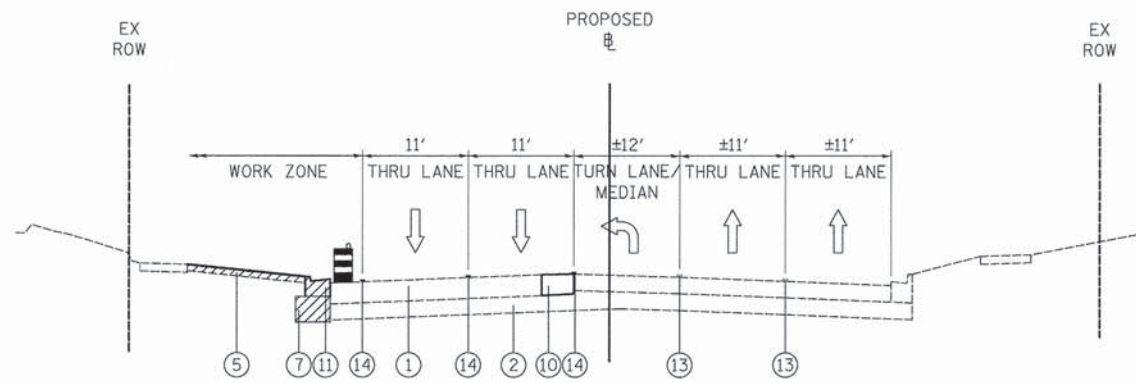


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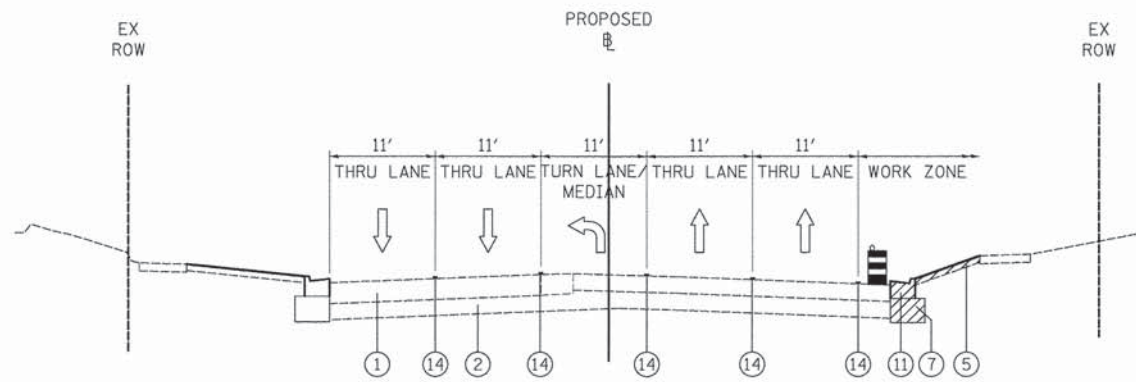
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CATON FARM ROAD MOT TYPICAL SECTIONS	
SCALE: N.T.S.	SHEET 1 OF 1 SHEETS STA. TO STA.

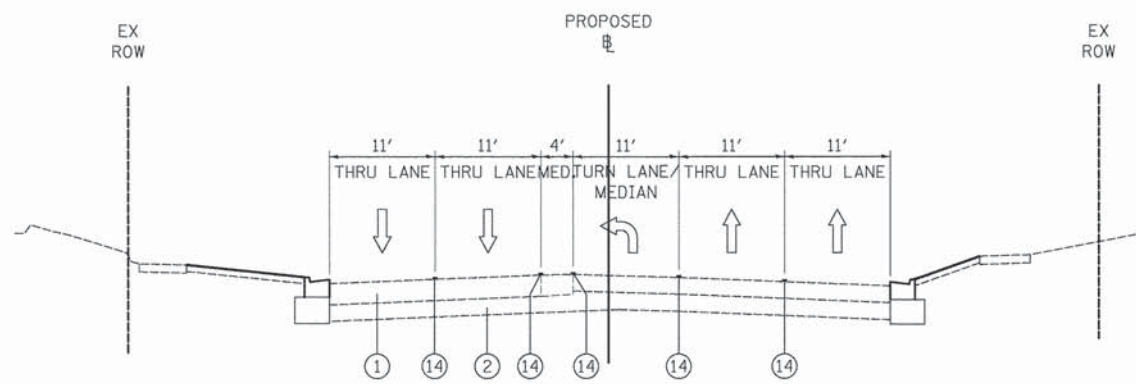
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	21
				CONTRACT NO. 61C29
ILLINOIS FED. AID PROJECT M-4003 (254)				



ESSINGTON ROAD - STAGE 1 TYPICAL SECTION
SOUTH LEG LOOKING NORTH



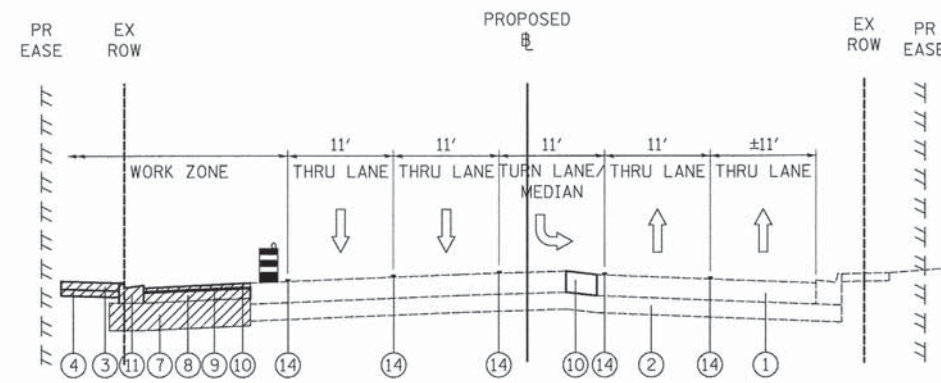
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SOUTH LEG LOOKING NORTH



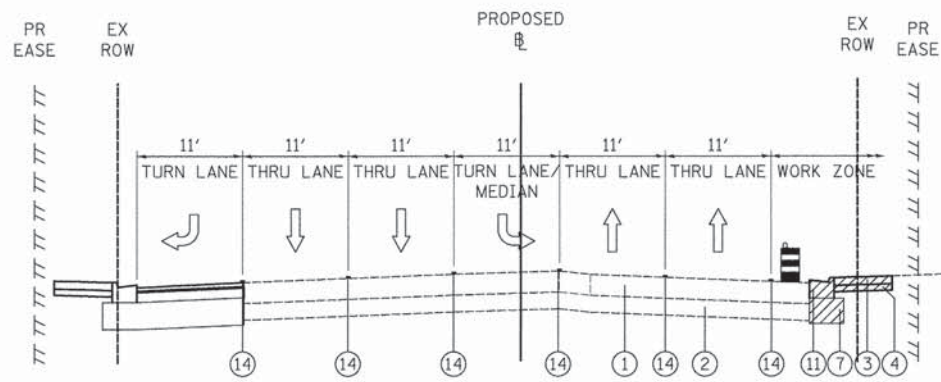
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SOUTH LEG LOOKING NORTH

LEGEND

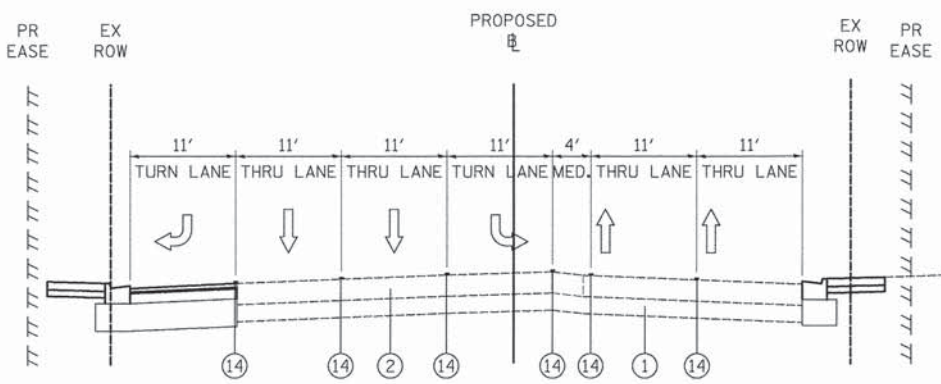
- ① EXISTING HMA PAVEMENT
 - ② EXISTING AGGREGATE SUBGRADE
 - ③ PROPOSED PCC SIDEWALK
 - ④ PROPOSED SUBBASE GRANULAR MATERIAL
 - ⑤ PROPOSED TOPSOIL
 - ⑥ PROPOSED TEMPORARY PAVEMENT
 - ⑦ PROPOSED AGGREGATE SUBGRADE
 - ⑧ PROPOSED HMA BASE COURSE
 - ⑨ PROPOSED LEVELING BINDER
 - ⑩ PROPOSED HMA BINDER COURSE
 - ⑪ PROPOSED COMBINATION CONCRETE CURB AND GUTTER
 - ⑫ PROPOSED CONCRETE MEDIAN
 - ⑬ EXISTING PAVEMENT MARKINGS (SOLID, DOTTED, OR SKIP-DASH)
 - ⑭ TEMPORARY PAINT PAVEMENT MARKINGS (SOLID, DOTTED, OR SKIP-DASH)
- ▨ CONSTRUCTION ZONE
- ↑ TRAFFIC FLOW
- ▬ TYPE II BARRICADES OR DRUMS W/ MONO DIRECTIONAL STEADY BURN LIGHT



ESSINGTON ROAD - STAGE 1 TYPICAL SECTION
NORTH LEG LOOKING NORTH



ESSINGTON ROAD - STAGE 2 TYPICAL SECTION
NORTH LEG LOOKING NORTH



ESSINGTON ROAD - STAGE 3 TYPICAL SECTION
NORTH LEG LOOKING NORTH

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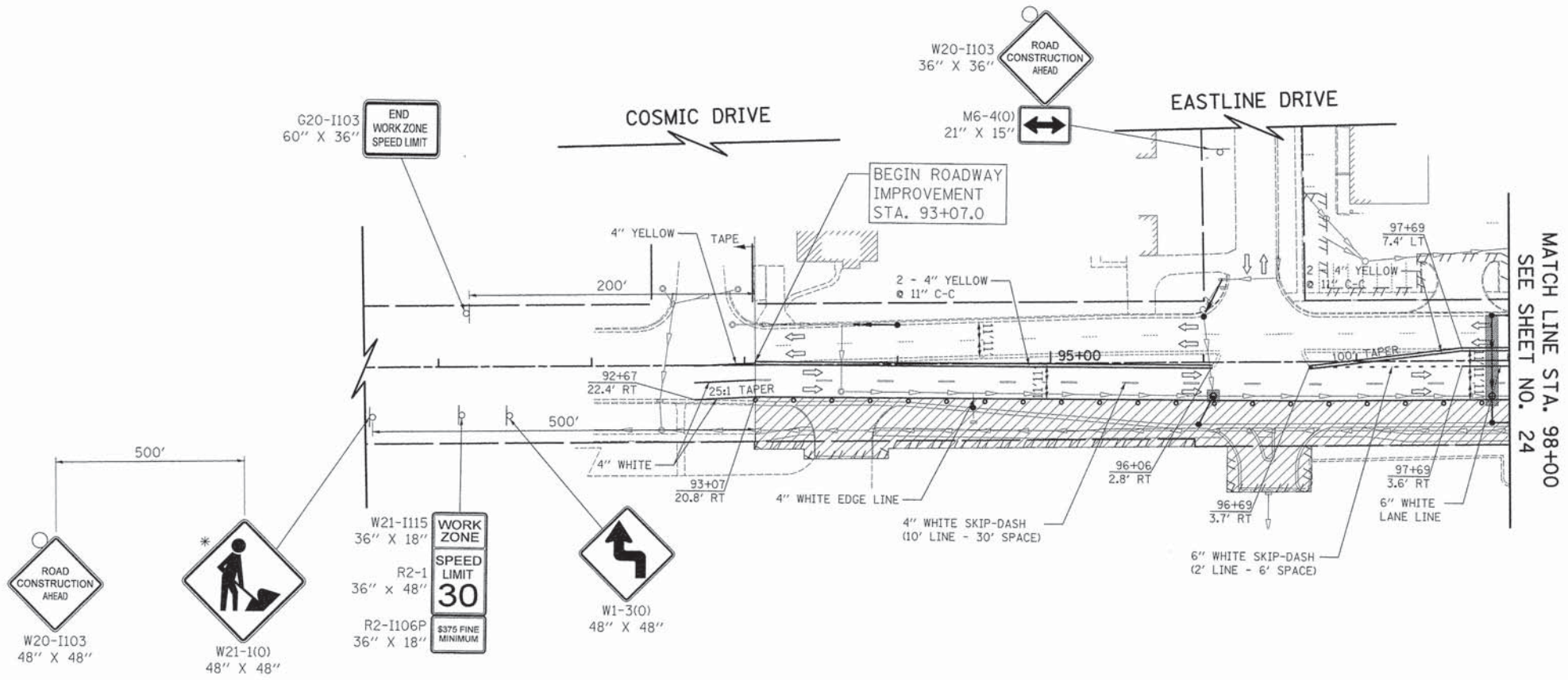
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	DATE - 12/07/15	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ESSINGTON ROAD
MOT TYPICAL SECTIONS**

SCALE: N.T.S. SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 292	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 22
				CONTRACT NO. 61C29
ILLINOIS FED. AID PROJECT M-4003 (254)				



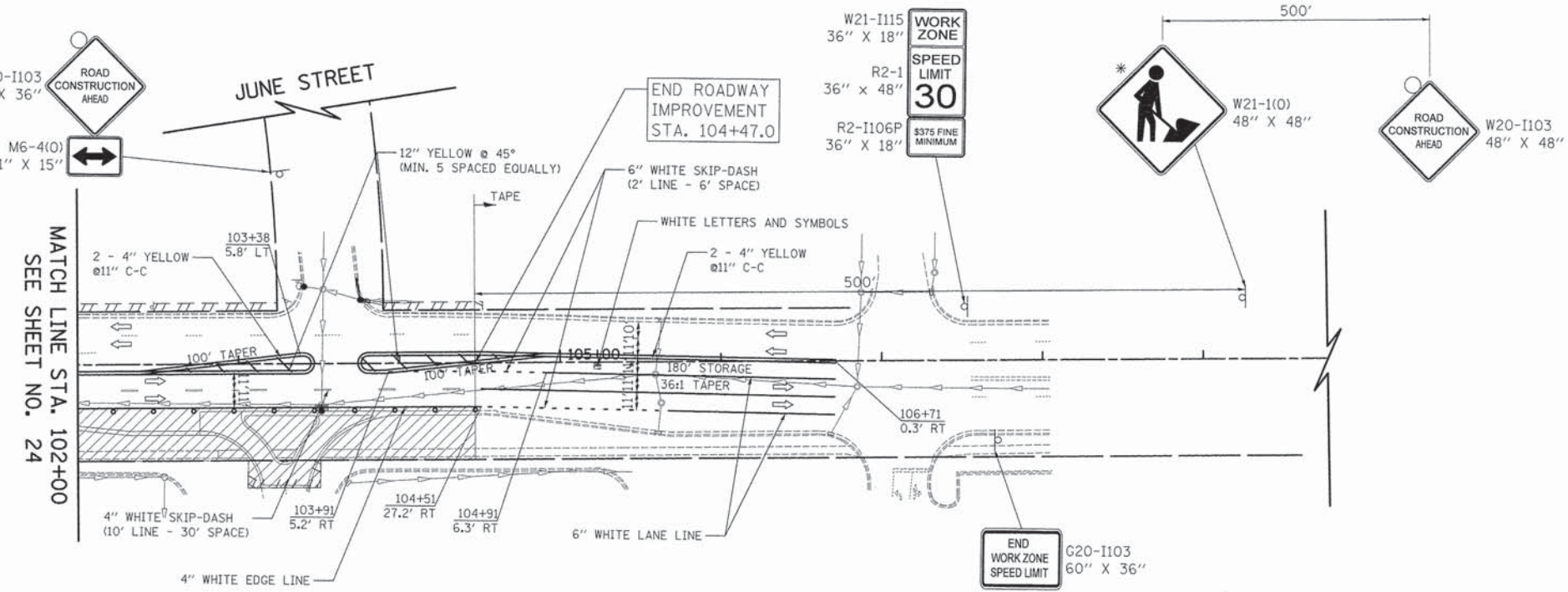
LEGEND

- TYPE II BARRICADES OR DRUMS WITH MONO DIRECTIONAL STEADY BURN LIGHT
- ⇌ DIRECTION OF TRAFFIC
- ⊥ TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ▨ WORK ZONE
- CLASS D PATCH, 10 INCH, OF THE TYPE SPECIFIED

MATCH LINE STA. 98+00
SEE SHEET NO. 24

CATON FARM ROAD

- NOTES**
1. ALL PAVEMENT MARKINGS OUTSIDE OF THE LIMITS OF PAVEMENT RECONSTRUCTION SHALL BE WET REFLECTIVE TEMPORARY TAPE.
 2. DRIVEWAY ENTRANCE SIGNS (TC-26) SHALL BE PLACED AS DIRECTED BY THE ENGINEER.
- * TO BE REMOVED WHEN WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR.



MATCH LINE STA. 102+00
SEE SHEET NO. 24

END WORK ZONE
SPEED LIMIT
G20-I103
60" X 36"

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PLOT DATE = 12/18/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CATON FARM ROAD AND ESSINGTON ROAD
MOT - STAGE 1**

SCALE: 1" = 50' SHEET 1 OF 2 SHEETS STA. TO STA.

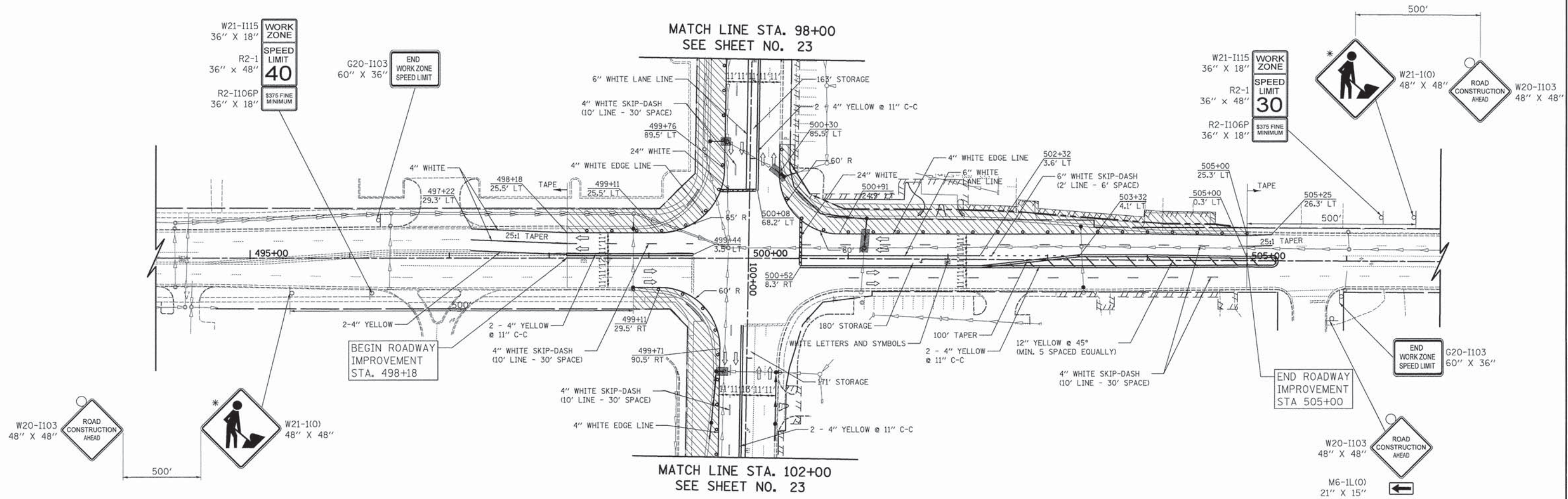
F.A.U. RTE.		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292		11-00446-00-CH	WILL	110	23
CONTRACT NO. 61C29					
ILLINOIS FED. AID PROJECT M-4003 (254)					

CATON FARM ROAD



NOTES

1. ALL PAVEMENT MARKINGS OUTSIDE OF THE LIMITS OF PAVEMENT RECONSTRUCTION SHALL BE WET REFLECTIVE TEMPORARY TAPE.
 2. DRIVEWAY ENTRANCE SIGNS (TC-26) SHALL BE PLACED AS DIRECTED BY THE ENGINEER.
- * TO BE REMOVED WHEN WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR.



LEGEND

- TYPE II BARRICADES OR DRUMS WITH MONO DIRECTIONAL STEADY BURN LIGHT
- ⇨ DIRECTION OF TRAFFIC
- ⊥ TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ▨ WORK ZONE
- CLASS D PATCH, 10 INCH, OF THE TYPE SPECIFIED

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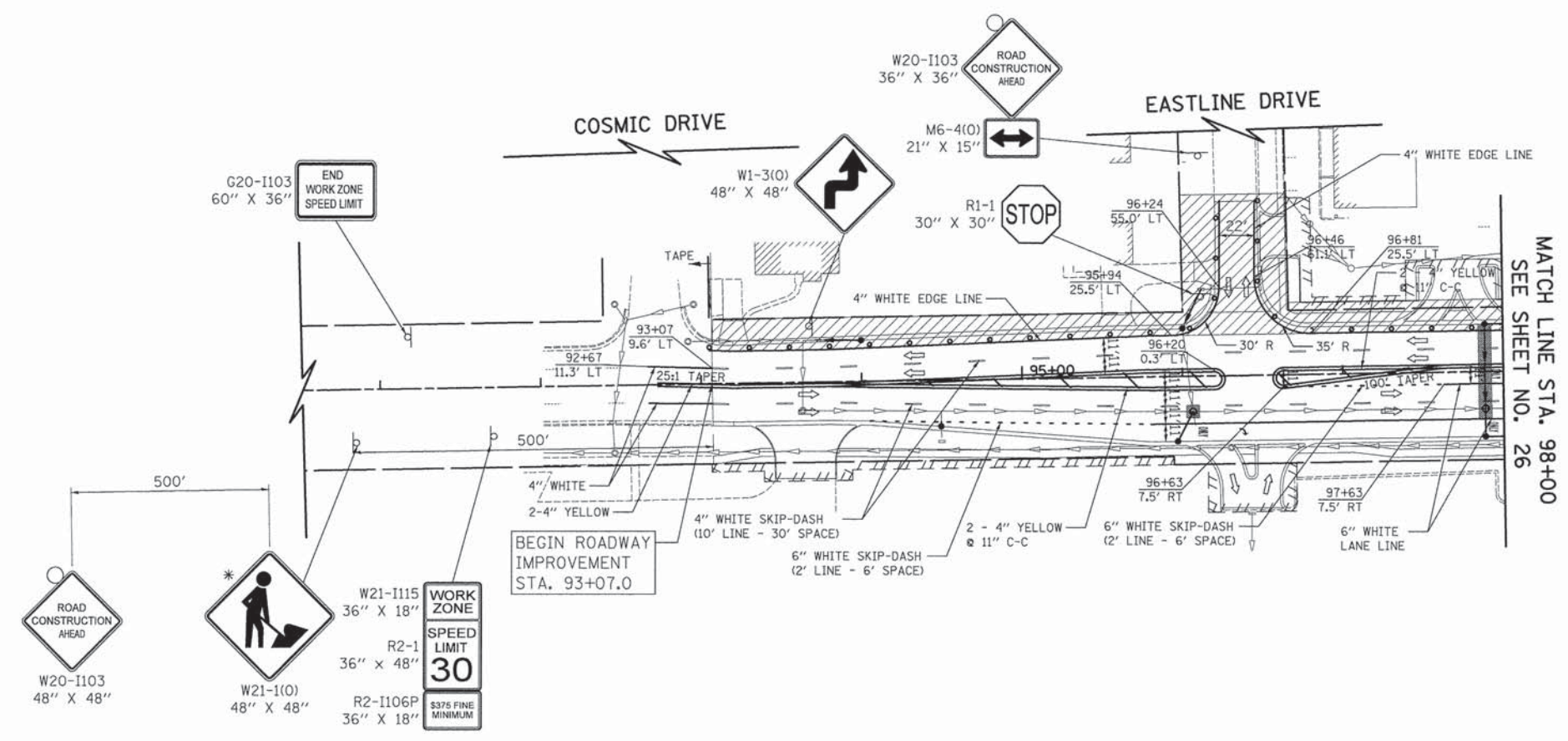
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PLOT DATE = 12/3/2015	DATE - 12/07/15	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CATON FARM ROAD AND ESSINGTON ROAD
MOT - STAGE 1**

SCALE: 1" = 50' SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	24
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				



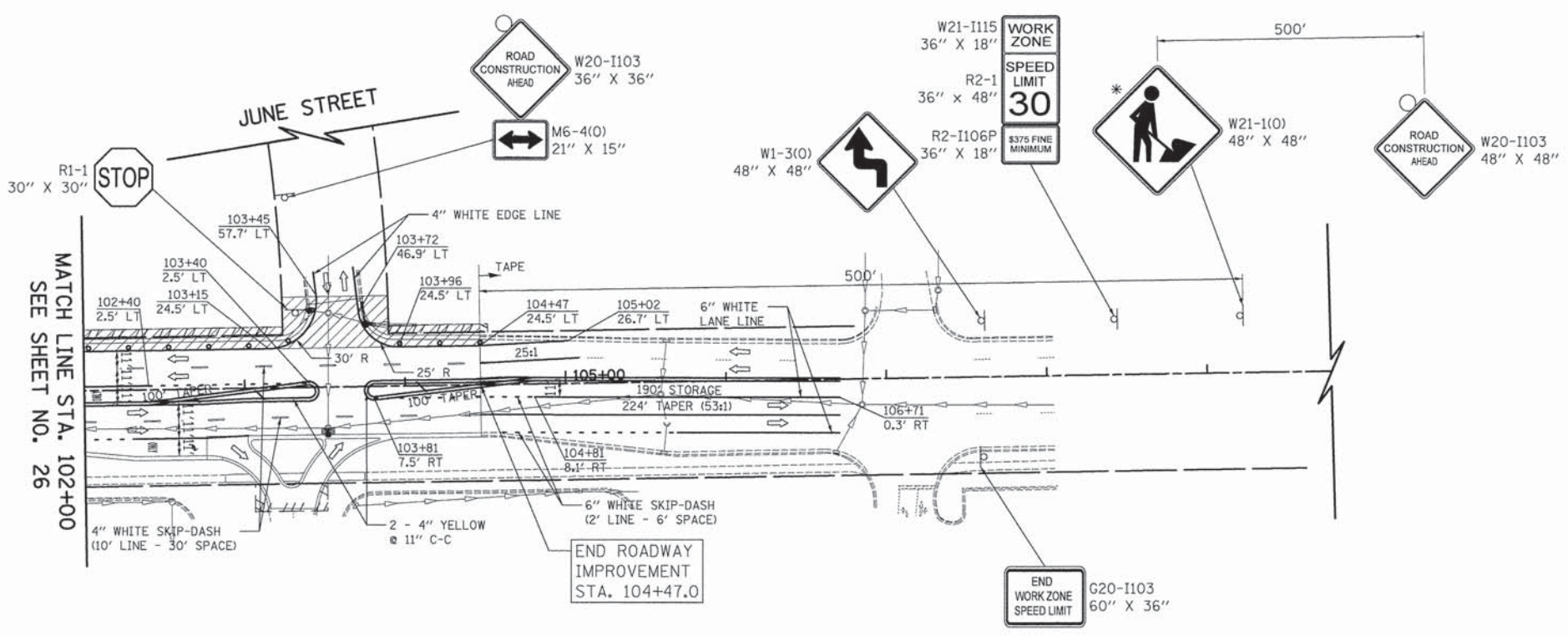
LEGEND

- TYPE II BARRICADES OR DRUMS WITH MONO DIRECTIONAL STEADY BURN LIGHT
- ⇨ DIRECTION OF TRAFFIC
- ⊥ TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ▨ WORK ZONE
- CLASS D PATCH, 10 INCH, OF THE TYPE SPECIFIED

CATON FARM ROAD

NOTES

1. ALL PAVEMENT MARKINGS OUTSIDE OF THE LIMITS OF PAVEMENT RECONSTRUCTION SHALL BE WET REFLECTIVE TEMPORARY TAPE.
2. DRIVEWAY ENTRANCE SIGNS (TC-26) SHALL BE PLACED AS DIRECTED BY THE ENGINEER.
- * TO BE REMOVED WHEN WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR.



CATON FARM ROAD

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PLOT SCALE = 50.0000' / in.	DRAWN - TGB	REVISED -
PLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

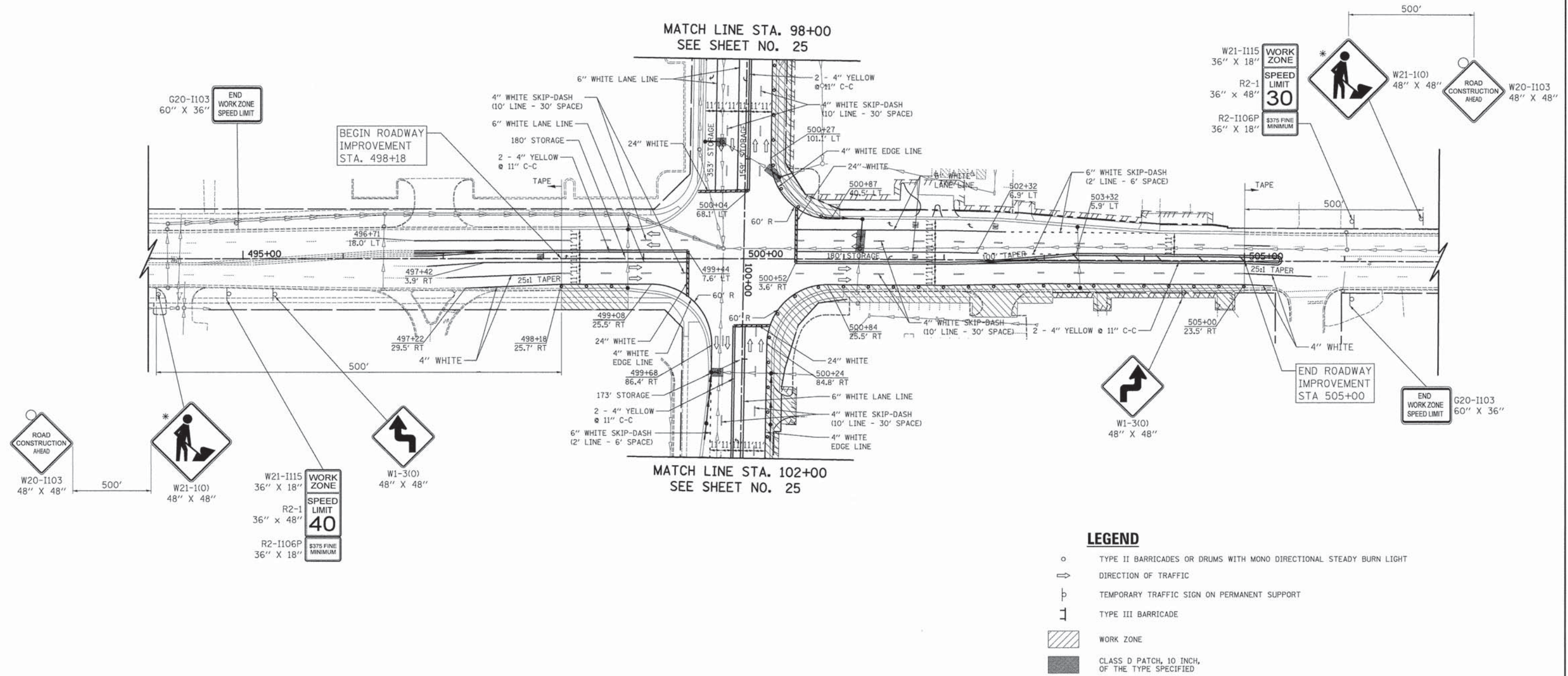
CATON FARM ROAD AND ESSINGTON ROAD MOT - STAGE 2			
SCALE: 1" = 50'	SHEET 1	OF 2 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	25
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				



NOTES

1. ALL PAVEMENT MARKINGS OUTSIDE OF THE LIMITS OF PAVEMENT RECONSTRUCTION SHALL BE WET REFLECTIVE TEMPORARY TAPE.
 2. DRIVEWAY ENTRANCE SIGNS (TC-26) SHALL BE PLACED AS DIRECTED BY THE ENGINEER.
- * TO BE REMOVED WHEN WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR.



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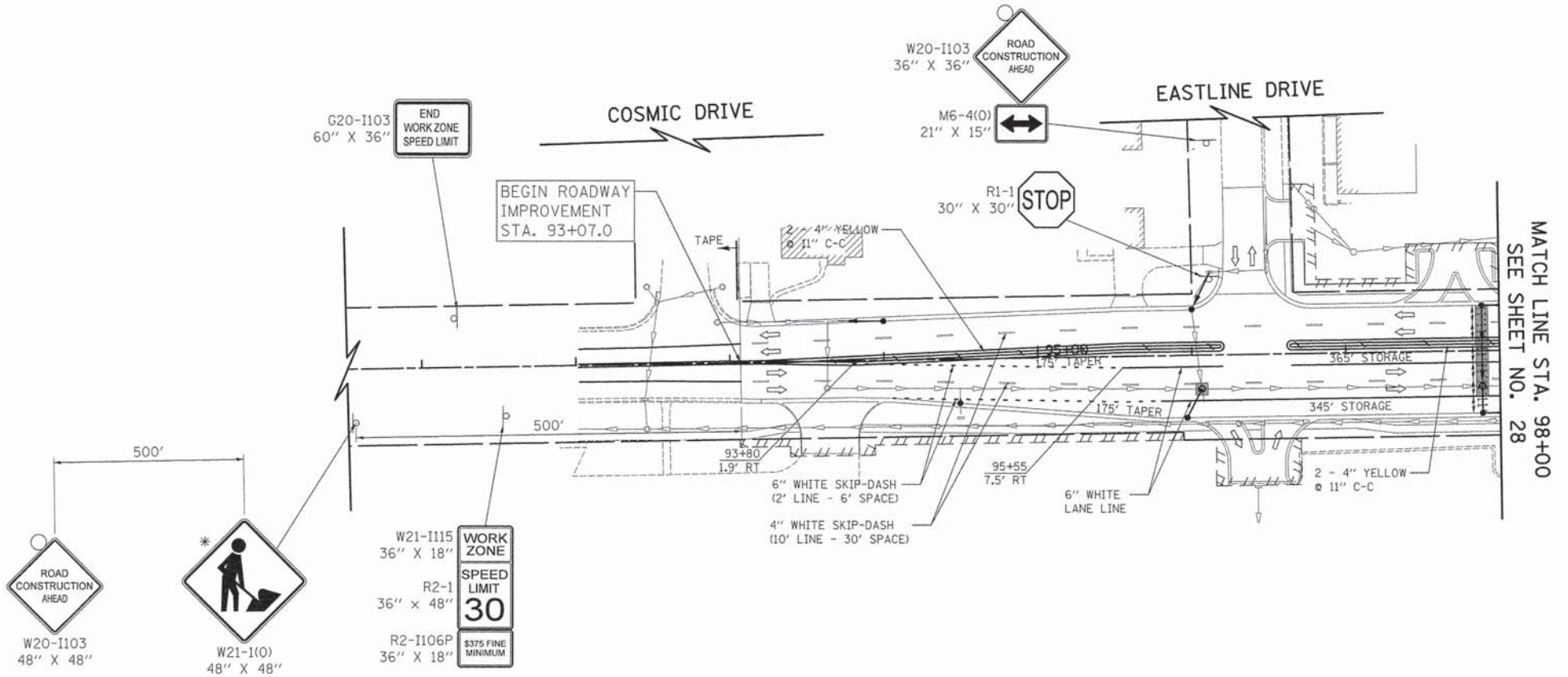


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	DRAWN - TGB	REVISED -
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PLOT DATE = 12/3/2015	DATE - 12/07/15	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CATON FARM ROAD AND ESSINGTON ROAD MOT - STAGE 2	
SCALE: 1" = 50'	SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	26
CONTRACT NO. 61C29				ILLINOIS FED. AID PROJECT M-4003 (254)



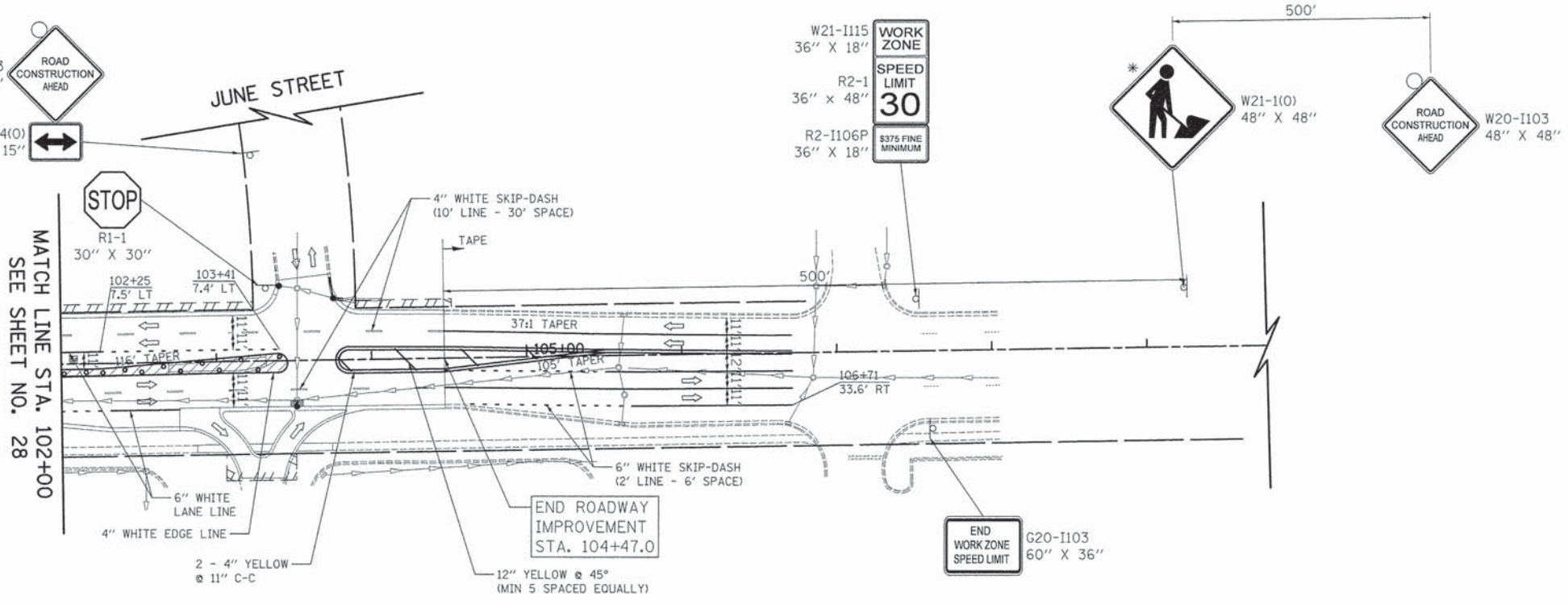
LEGEND

- TYPE II BARRICADES OR DRUMS WITH MONO DIRECTIONAL STEADY BURN LIGHT
- ⇨ DIRECTION OF TRAFFIC
- ⊥ TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ▨ WORK ZONE
- CLASS D PATCH, 10 INCH, OF THE TYPE SPECIFIED

CATON FARM ROAD

NOTES

1. ALL PAVEMENT MARKINGS OUTSIDE OF THE LIMITS OF PAVEMENT RECONSTRUCTION SHALL BE WET REFLECTIVE TEMPORARY TAPE.
 2. DRIVEWAY ENTRANCE SIGNS (TC-26) SHALL BE PLACED AS DIRECTED BY THE ENGINEER.
- * TO BE REMOVED WHEN WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR.



CATON FARM ROAD

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PLOT SCALE = 50,0000' / 1"	DRAWN - TGB	REVISED -
PLOT DATE = 12/18/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
MOT - STAGE 3

SCALE: 1" = 50' SHEET 1 OF 2 SHEETS STA. TO STA.

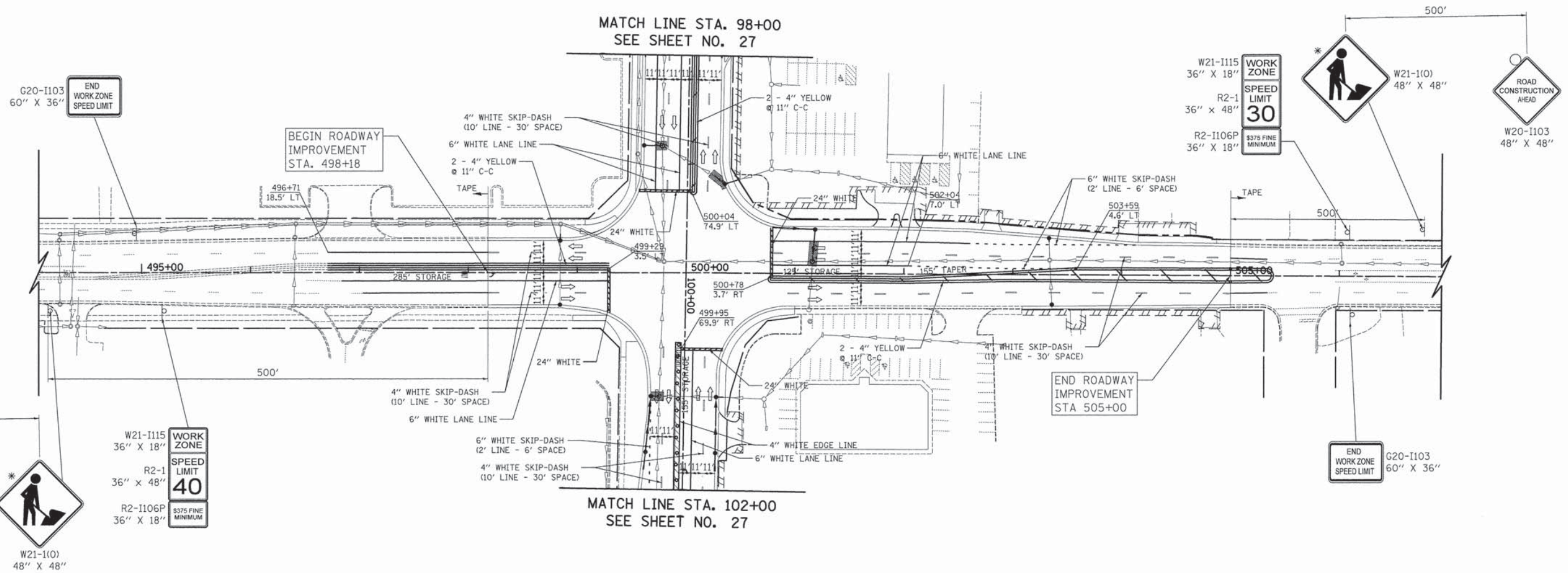
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	27
CONTRACT NO. 61C29			[ILLINOIS] FED. AID PROJECT M-4003 (254)	



NOTES

1. ALL PAVEMENT MARKINGS OUTSIDE OF THE LIMITS OF PAVEMENT RECONSTRUCTION SHALL BE WET REFLECTIVE TEMPORARY TAPE.
 2. DRIVEWAY ENTRANCE SIGNS (TC-26) SHALL BE PLACED AS DIRECTED BY THE ENGINEER.
- * TO BE REMOVED WHEN WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR.

MATCH LINE STA. 98+00
SEE SHEET NO. 27



MATCH LINE STA. 102+00
SEE SHEET NO. 27

LEGEND

- TYPE II BARRICADES OR DRUMS WITH MONO DIRECTIONAL STEADY BURN LIGHT
- ⇨ DIRECTION OF TRAFFIC
- ⊥ TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- ⌊ TYPE III BARRICADE
- ▨ WORK ZONE
- CLASS D PATCH, 10 INCH, OF THE TYPE SPECIFIED

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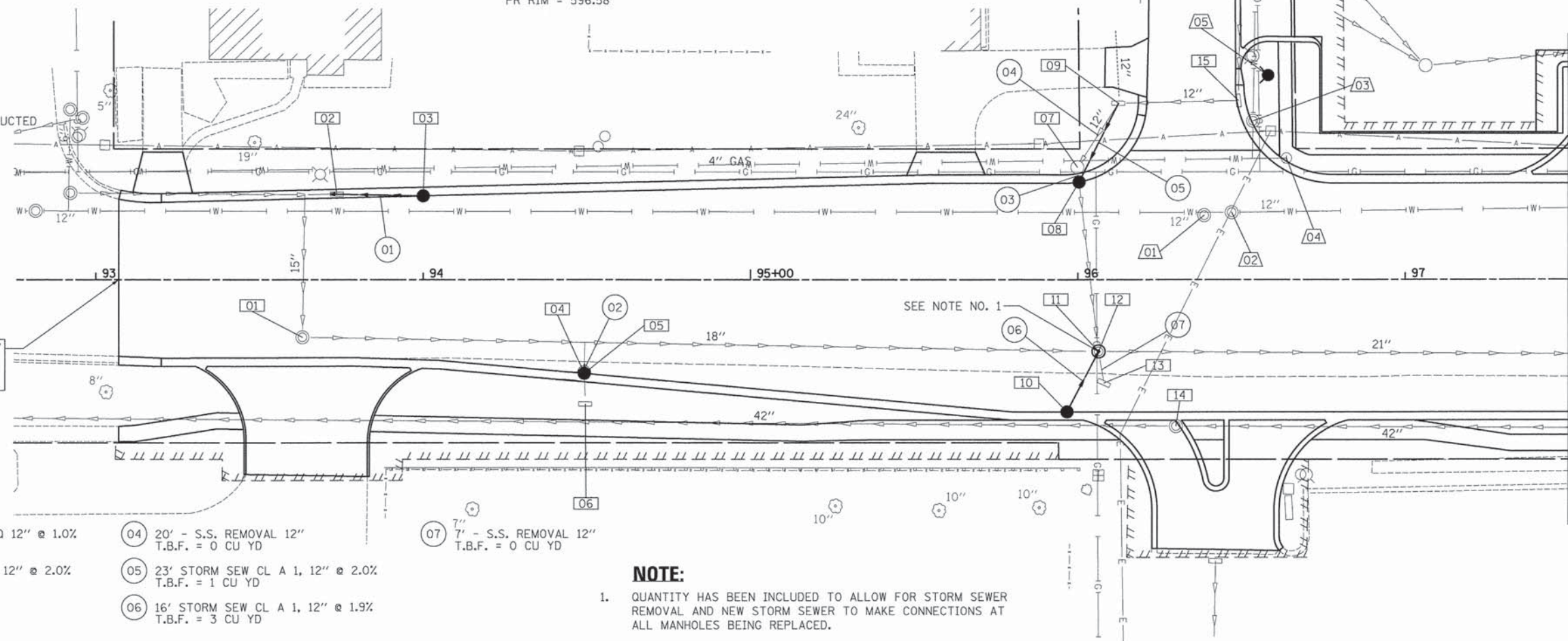
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
MOT - STAGE 3

SCALE: 1" = 50' SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	28
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

- [01]** STA 93+63.10, 17.6' RT
FR & LIDS ADJUST SPL
EX RIM = 598.89
PR RIM = 599.31
- [02]** STA 93+73.65, 26.1' LT
REMOVING INLETS TO
MAINTAIN FLOW
INV = 595.88 (EX 12" W)
- [03]** STA 94+00.0, 25.6' LT
CB TC T24F&G
RIM = 598.84
INV = 596.15(W)
- [04]** STA 94+49.2, 28.3' RT
REMOVING CATCH BASINS
- [05]** STA 94+49.2, 28.7' RT
CB TA 4 DIA T24F&G
RIM = 598.35
INV = 593.84 (S)
INV = 593.35 (N)
- [06]** STA 94+49.6, 38.1' RT
INLETS TO BE RECONSTRUCTED
EX RIM = 596.50
PR RIM = 597.59
- [07]** STA 95+99.8, 34.4' LT
REMOVING CATCH BASINS
- [08]** STA 96+00.0, 34.4' LT
CB TA 4 DIA T24F&G
RIM = 597.10
INV = 594.24 (NE)
INV = 593.86 (S)
- [09]** STA 96+12.4, 54.0' LT
INLET TO REMAIN
RIM = 596.59 (EX)
INV = 594.70 (EX 12" E)
INV = 594.70 (SW)
- [10]** STA 95+96.7, 40.5' RT
CB TC T24F&G
RIM = 596.86
INV = 592.70 (NE)
- [11]** STA 96+06.3, 22.0' RT
REMOVING MANHOLES
- [12]** STA 96+06.3, 22.0' RT
MAN TA 5 DIA TIF CL
RIM = 597.21
INV = 592.38 (EX 12" N)
INV = 592.40 (SW)
INV = 592.20 (EX 18" W)
INV = 592.20 (EX 21" E)
- [13]** STA 96+07.9, 31.7' RT
REMOVING INLETS
- [14]** STA 96+29.9, 44.9' RT
FR & LIDS ADJUST SPL
EX RIM = 596.31
PR RIM = 596.58
- [15]** STA 96+07.9, 31.7' RT
INLETS TO BE ADJUSTED
EX RIM = 597.04
PR RIM = 597.13



EASTLINE DRIVE



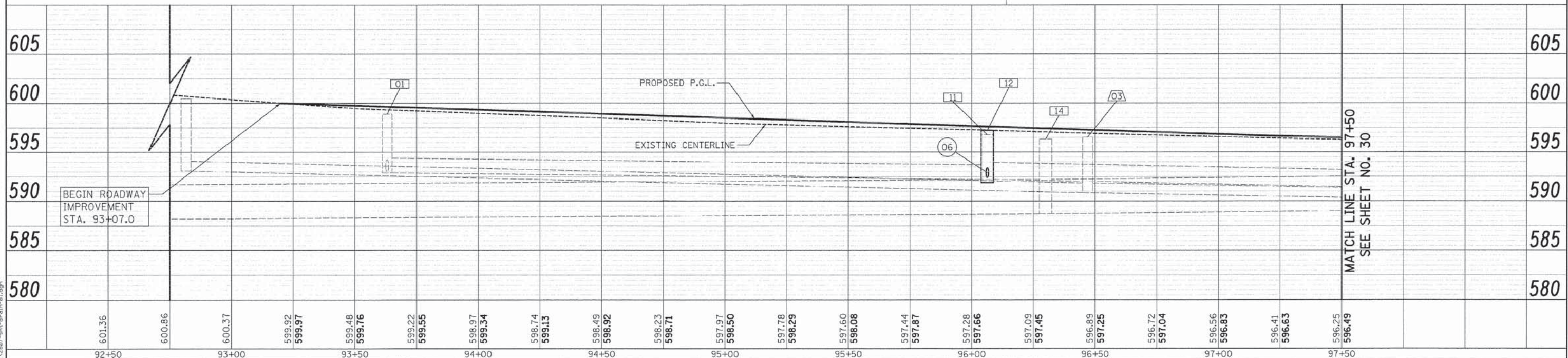
MATCH LINE STA. 97+50
SEE SHEET NO. 30

- [01]** STA. 96+38.6, 19.7' LT
FR & LIDS ADJUST SPL
EX RIM = 596.68
PR RIM = 596.98
- [02]** STA. 96+47.0, 20.6' LT
FR & LIDS ADJUST SPL
EX RIM = 596.53
PR RIM = 596.90
- [03]** STA. 96+53.5, 48.5' LT
FR & LIDS ADJUST SPL
EX RIM = 597.14
PR RIM = 597.27
- [04]** STA. 96+63.7, 37.1' LT
VALVE BOX ADJ
EX RIM = 597.16
PR RIM = 596.64
- [05]** STA. 96+58.1, 62.6' LT
FIRE HYDNTS TO BE MVD

- [01]** 27' STORM SEW WM REQ 12" @ 1.0%
T.B.F. = 4 CU YD
- [02]** 3' STORM SEW CL A 1, 12" @ 2.0%
T.B.F. = 1 CU YD
- [03]** 2' - S.S. REMOVAL 12"
T.B.F. = 1 CU YD
- [04]** 20' - S.S. REMOVAL 12"
T.B.F. = 0 CU YD
- [05]** 23' STORM SEW CL A 1, 12" @ 2.0%
T.B.F. = 1 CU YD
- [06]** 16' STORM SEW CL A 1, 12" @ 1.9%
T.B.F. = 3 CU YD
- [07]** 7' - S.S. REMOVAL 12"
T.B.F. = 0 CU YD

NOTE:
1. QUANTITY HAS BEEN INCLUDED TO ALLOW FOR STORM SEWER REMOVAL AND NEW STORM SEWER TO MAKE CONNECTIONS AT ALL MANHOLES BEING REPLACED.

CATON FARM ROAD



MATCH LINE STA. 97+50
SEE SHEET NO. 30

	USER NAME = tgb DESIGNED = JSM DRAWN = EDS CHECKED = JRV DATE = 12/07/15	REVISED = REVISED = REVISED = REVISED =	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CATON FARM ROAD DRAINAGE AND UTILITIES	SCALE: 1" = 20' SHEET 1 OF 5 SHEETS STA. 93+07.0 TO STA. 97+50.0	<table border="1" style="font-size: small;"> <tr> <td>F.A.U. RTE.</td> <td>SECTION</td> <td>COUNTY</td> <td>TOTAL SHEETS</td> <td>SHEET NO.</td> </tr> <tr> <td>292</td> <td>11-00446-00-CH</td> <td>WILL</td> <td>110</td> <td>29</td> </tr> <tr> <td colspan="5" style="text-align: center;">CONTRACT NO. 61C29</td> </tr> <tr> <td colspan="5" style="text-align: center;">ILLINOIS FED. AID PROJECT M-4003 (254)</td> </tr> </table>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	292	11-00446-00-CH	WILL	110	29	CONTRACT NO. 61C29					ILLINOIS FED. AID PROJECT M-4003 (254)				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.																						
292	11-00446-00-CH	WILL	110	29																						
CONTRACT NO. 61C29																										
ILLINOIS FED. AID PROJECT M-4003 (254)																										

PLAN

SURVEYED	DATE
PLOTTED	BY
CHECKED	
NOTE BOOK NO.	
FILE NAME	

PROFILE

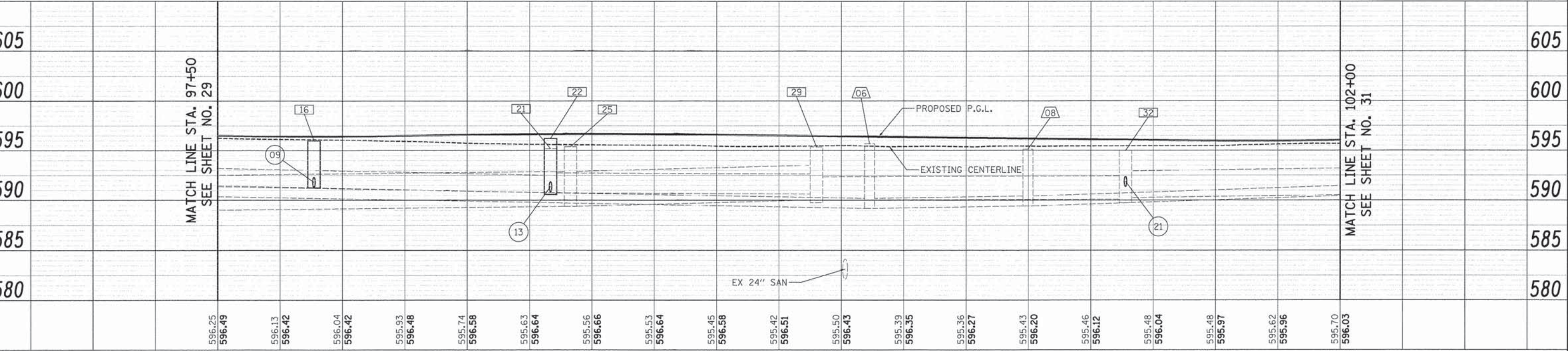
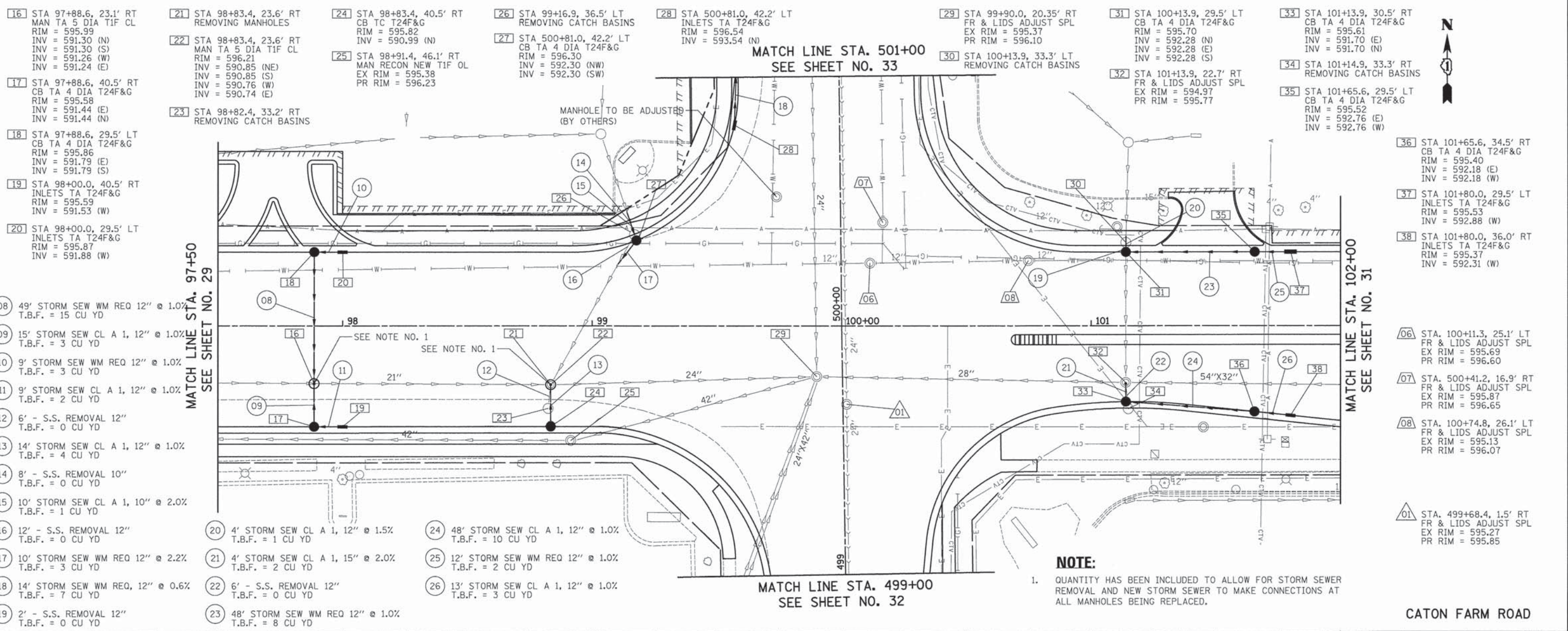
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PLOTTED	BY
CHECKED	
NOTE BOOK NO.	
FILE NAME	

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 SURVEYED _____
 PLOTTED _____
 CHECKED _____
 RT. OF WAY CHECKED _____
 NOTE BOOK NO. _____
 CAD FILE NAME _____

DATE: _____ BY: _____
 SURVEYED _____
 PLOTTED _____
 CHECKED _____
 BLK. NOTED _____
 STRUCTURE NOTATION CHRD _____
 NOTE BOOK NO. _____

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STATION	ELEVATION	REMARKS
97+50	596.25	
	596.49	
	596.13	
	596.42	
98+00	596.04	
	596.42	
	595.93	
	596.48	
98+50	595.74	
	596.58	
	595.63	
	596.64	
99+00	595.56	
	596.66	
	595.53	
	596.64	
99+50	595.45	
	596.58	
	595.42	
	596.51	
100+00	595.50	
	596.43	
	595.39	
	596.35	
100+50	595.36	
	596.27	
	595.43	
	596.20	
101+00	595.46	
	596.12	
	595.48	
	596.04	
101+50	595.48	
	595.97	
	595.62	
	595.96	
102+00	595.70	
	596.03	



USER NAME = tgb	DESIGNED - JSM	REVISED -
PLT SCALE = 28.0000''/1'	DRAWN - EDS	REVISED -
PLT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SCALE: 1" = 20'	SHEET 2 OF 5 SHEETS	STA. 97+50.0 TO STA. 102+00.0
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F.A.U. RTE. 292	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 30
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	



- 36 STA 103+38.2, 48.4' LT
REMOVING CATCH BASINS
- 37 STA 103+40.9, 48.0' LT
CB TC T24F&G
RIM = 596.87
INV = 594.45 (E)
- 38 STA 103+52.8, 46.3' LT
FR & LIDS ADJUST SPL
EX RIM = 596.98
PR RIM = 596.94
- 39 STA 103+51.6, 25.5' RT
FR & LIDS ADJUST SPL
EX RIM = 596.84
PR RIM = 596.98
INV = 592.79 (S)
INV = 591.84 (EX 21"X15" N)
INV = 591.14 (EX 42" E)
INV = 591.04 (EX 54"X32" W)

- 40 STA 103+51.6, 29.6' RT
CB TC T24F&G
RIM = 596.90
INV = 592.81 (N)
- 41 STA 103+52.9, 33.3' RT
REMOVING CATCH BASINS
- 42 STA 103+75.8, 39.6' LT
CB TA 4 DIA T24F&G
RIM = 596.85
INV = 593.79 (N)
INV = 593.79 (E)
INV = 593.62 (NW)
- 43 STA 103+79.8, 38.5' LT
REMOVING CATCH BASINS

- 44 STA 103+73.1, 51.1' LT
INLET TA T11F&G
RIM = 596.88
INV = 593.88 (S)

- 09 STA. 103+46.1, 42.1' LT
FR & LIDS ADJUST SPL
EX RIM = 596.76
PR RIM = 596.85
- 10 STA. 103+88.1, 26.1' LT
FR & LIDS ADJUST SPL
EX RIM = 596.90
PR RIM = 597.16

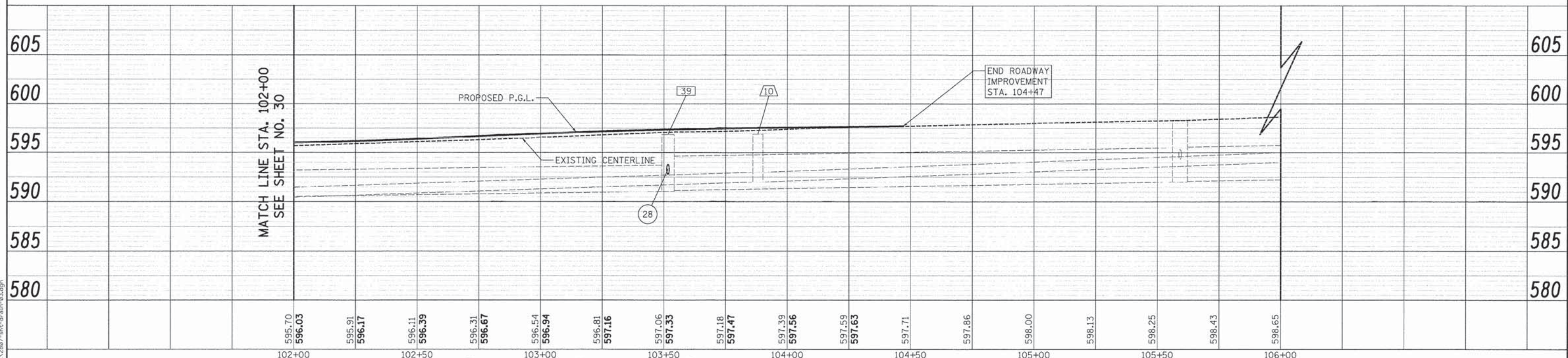
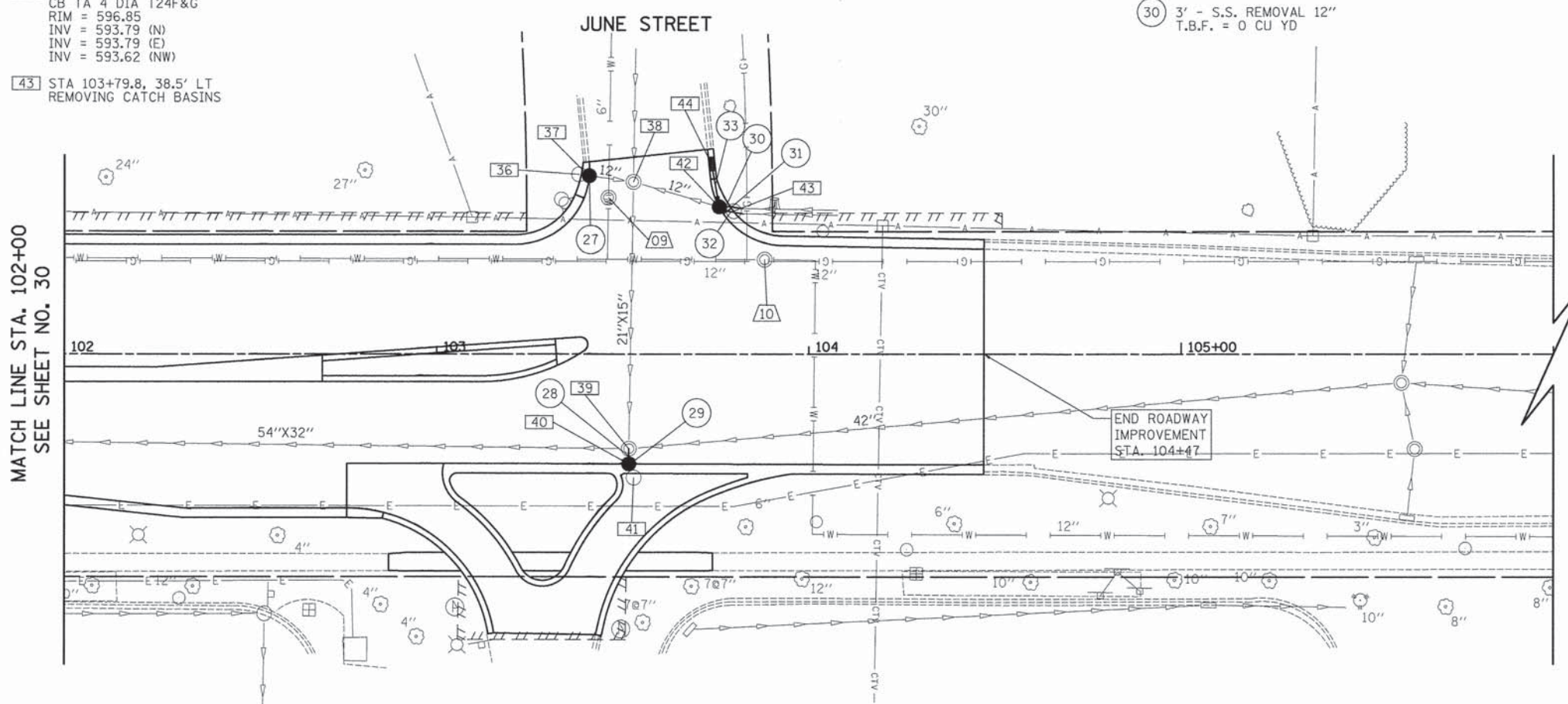
- 27 3' - S.S. REMOVAL 12"
T.B.F. = 1 CU YD
- 28 1' STORM SEW CL A 1, 12" @ 2.0%
T.B.F. = 1 CU YD
- 29 4' - S.S. REMOVAL 12"
T.B.F. = 0 CU YD
- 30 3' - S.S. REMOVAL 12"
T.B.F. = 0 CU YD

- 31 5' STORM SEW CL A 1, 12" @ 2.0%
T.B.F. = 1 CU YD
- 32 5' PIPE UNDERDRAINS 6"
CONNECT TO EXISTING DRAIN
- 33 9' STORM SEW CL A 1, 12" @ 1.0%
T.B.F. = 2 CU YD

PLAN	SURVEYED	DATE
	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	

PROFILE	SURVEYED	DATE
	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	

PLOT DRIVER = ...
PEN TABLE = ...
FILE NAME = ...



USER NAME = tgb	DESIGNED - JSM	REVISED -
PLOT SCALE = 20.0000' / 1"	DRAWN - EDS	REVISED -
PLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

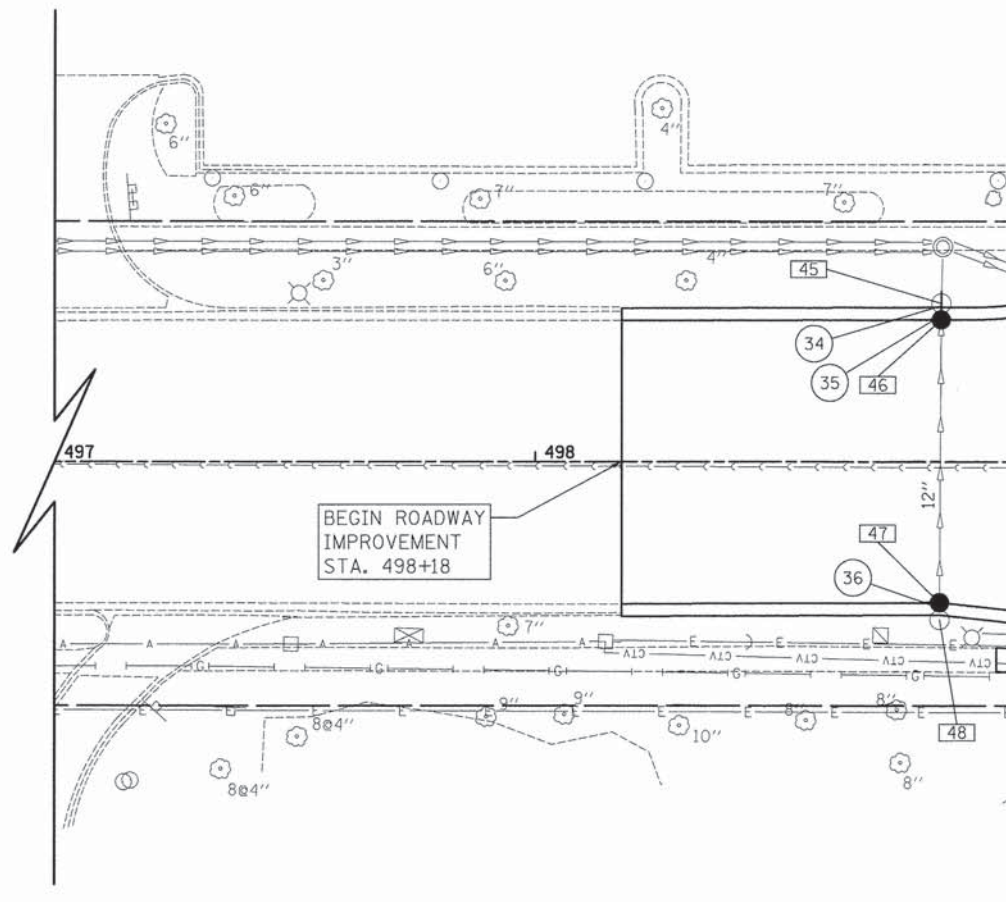
CATON FARM ROAD DRAINAGE AND UTILITIES			
SCALE: 1" = 20'	SHEET 3	OF 5 SHEETS	STA. 102+00.0 TO STA. 104+47.0

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	31
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				

PLAN	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF WAY CHECKED	
	CADD FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF WAY CHECKED	
	CADD FILE NAME	

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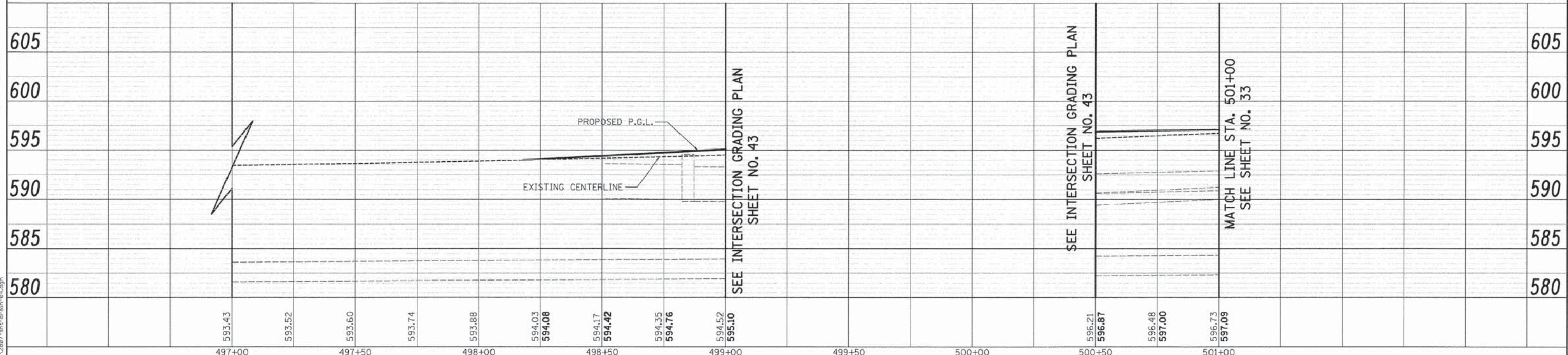


MATCH LINE STA. 499+00
SEE SHEET NO. 30

- 45 STA 498+84.5, 32.9' LT
REMOVING CATCH BASINS
- 46 STA 498+84.5, 29.5' LT
CB TA 4 DIA T24F&G
RIM = 594.48
INV = 589.66 (E)
INV = 589.80 (W)
- 47 STA 498+84.2, 29.5' RT
CB TC T24F&G
RIM = 594.36
INV = 591.01 (W)
- 48 STA 498+84.2, 33.1' RT
REMOVING CATCH BASINS

- 34 4' STORM SEW CL A 1, 12" @ 1.0%
T.B.F. = 2 CU YD
- 35 3' - S.S. REMOVAL 12"
T.B.F. = 0 CU YD
- 36 2' - S.S. REMOVAL 12"
T.B.F. = 1 CU YD

ESSINGTON ROAD



SEE INTERSECTION GRADING PLAN
SHEET NO. 43

SEE INTERSECTION GRADING PLAN
SHEET NO. 43

MATCH LINE STA. 501+00
SEE SHEET NO. 33



USER NAME =	tgb
DESIGNED -	JSM
REVISOR -	
DRAWN -	EDS
REVISOR -	
CHECKED -	JRV
REVISOR -	
DATE -	12/07/15

DESIGNED -	JSM
REVISOR -	
DRAWN -	EDS
REVISOR -	
CHECKED -	JRV
REVISOR -	
DATE -	12/07/15

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ESSINGTON ROAD			
DRAINAGE AND UTILITIES			
SCALE: 1" = 20'	SHEET 4	OF 5 SHEETS	STA. 498+18.0 TO STA. 501+00.0

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	11-00446-00-CH	WILL	110	32
ILLINOIS FED. AID PROJECT M-4003 (254)			CONTRACT NO. 61C29	

- 49 STA 501+15.9, 10.9' LT
REMOVING MANHOLES
- 50 STA 501+15.9, 10.9' LT
MAN TA 5 DIA TIF CL
RIM = 597.23
INV = 592.63 (EX 12" E)
INV = 593.44 (W)
INV = 591.04 (EX 24" N)
INV = 590.99 (EX 24" S)
- 51 STA 501+17.7, 32.8' LT
REMOVING CATCH BASINS
- 52 STA 501+18.4, 40.5' LT
CB TC T24F&G
RIM = 596.63
EX RIM = 591.76 (E)
INV = 593.63 (S)
- 53 STA 503+33.1, 33.6' LT
CB TA 4 DIA T24F&G
RIM = 599.29
INV = 596.40 (E)

- 54 STA 503+33.1, 32.8' LT
REMOVING CATCH BASINS
- 55 STA 503+33.9, 13.3' LT
FR & LIDS ADJUST SPL
EX RIM = 599.36
PR RIM = 599.70

- 56 STA 503+36.0, 27.5' RT
CB TC T24F&G
RIM = 599.38
INV = 596.42 (W)
- 57 STA 503+36.3, 32.7' RT
REMOVING CATCH BASINS

- 37 26' STORM SEW CL A 1, 12" @ 1.0%
T.B.F. = 9 CU YD
- 38 19' - S.S. REMOVAL 12"
T.B.F. = 0 CU YD
- 39 2' STORM SEW CL A 1, 12" @ 2.5%
T.B.F. = 1 CU YD
- 40 5' - S.S. REMOVAL 12"
T.B.F. = 1 CU YD

NOTE:

1. QUANTITY HAS BEEN INCLUDED TO ALLOW FOR STORM SEWER REMOVAL AND NEW STORM SEWER TO MAKE CONNECTIONS AT ALL MANHOLES BEING REPLACED.

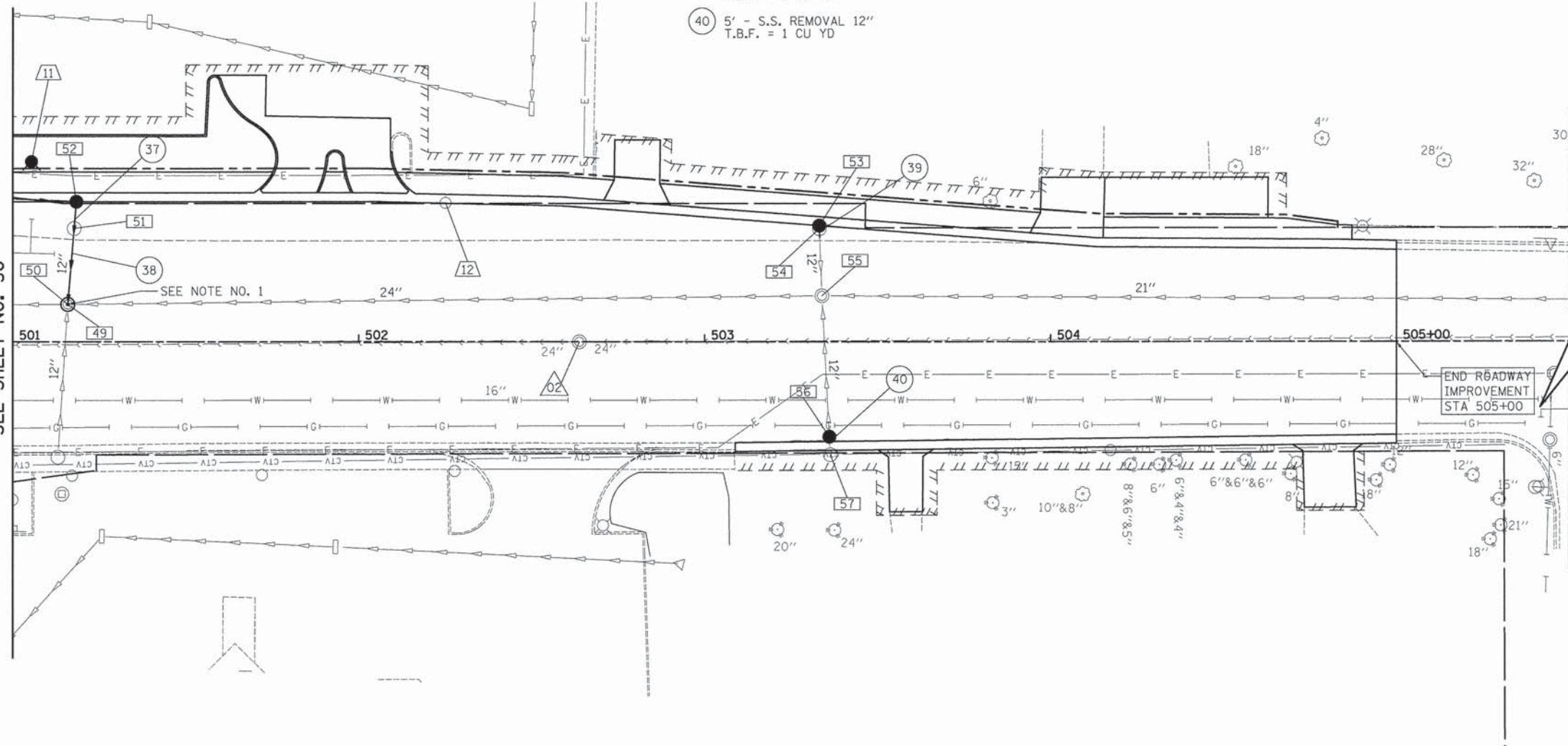


DATE	
BY	
PLAN	
SURVEYED	
PLOTTED	
NOTE BOOK	
NO.	
FILE NAME	

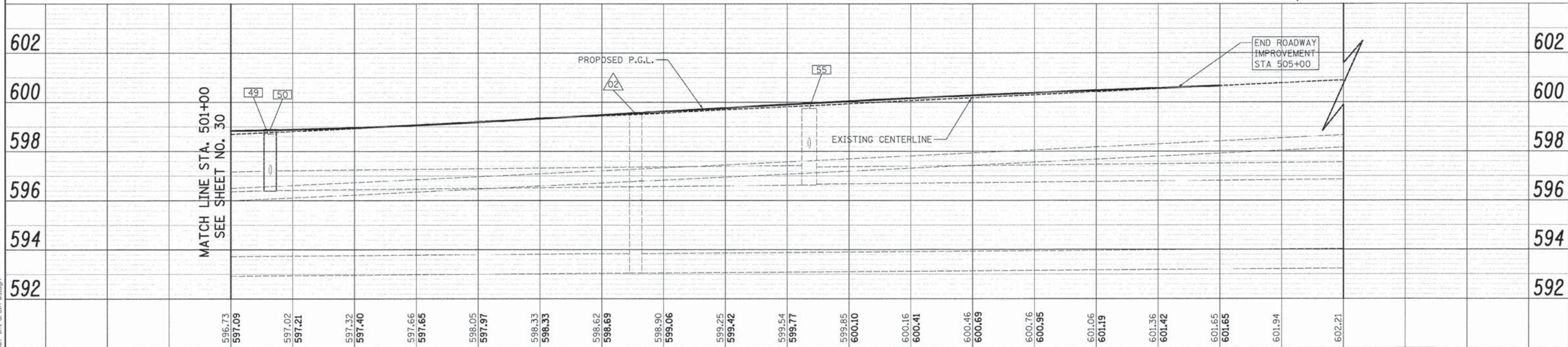
DATE	
BY	
PROFILE	
SURVEYED	
PLOTTED	
NOTE BOOK	
NO.	
FILE NAME	

- 02 STA. 502+63.8, 0.0' RT
FR & LIDS ADJUST SPL
EX RIM = 598.81
PR RIM = 598.89
- 11 STA. 502+05.4, 52.1' LT
FIRE HYDNTS TO BE MVD
- 12 STA. 502+25.1, 40.1' LT
VALVE BOX ADJ
EX RIM = 598.08
PR RIM = 597.72

MATCH LINE STA. 501+00
SEE SHEET NO. 30



ESSINGTON ROAD



PLOT DRIVER = ...
PEN TABLE = ...
FILE NAME = ...



USER NAME = tgb	DESIGNED - JSM	REVISED -
PLOT SCALE = 20,0000' / in.	DRAWN - EDS	REVISED -
PLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ESSINGTON ROAD
DRAINAGE AND UTILITIES

SCALE: 1" = 20' SHEET 5 OF 5 SHEETS STA. 501+00.0 TO STA. 504+83.0

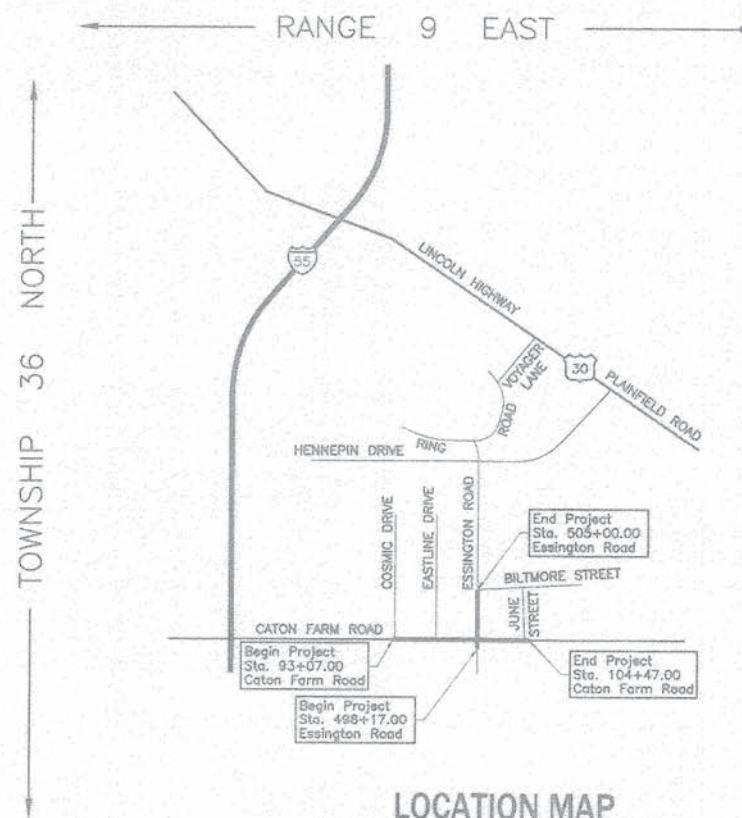
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	11-00446-00-CH	WILL	110	33
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

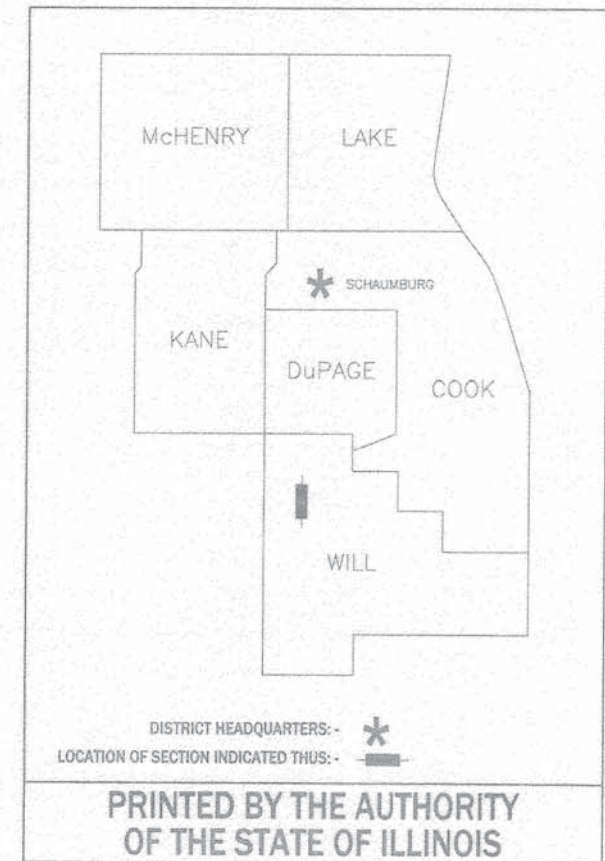
PLAT OF HIGHWAYS

ROUTE: F.A.U. 0292 (CATON FARM ROAD) & F.A.U. 0326 (ESSINGTON ROAD)
SECTION: 11-00446-00-CH
COUNTY: WILL
LIMITS: CATON FARM ROAD (FROM COSMIC DR. TO JUNE ST.)
ESSINGTON ROAD (FROM BILTMORE ST. TO CATON FARM RD.)
JOB NO.: R-55-001-97

PARCEL NUMBER	OWNER	SHEET NUMBER	PROPERTY ACQUIRED BY
0001TE	THE BOARD OF EDUCATION OF PLAINFIELD COMMUNITY CONSOLIDATED SCHOOL DISTRICT 202	2	
0002TE	BAC SO, LLC, AN ILLINOIS LIMITED LIABILITY COMPANY	2	
0003 0003TE-A 0003TE-B	CRYSTAL SQUARE, LLC, AN ILLINOIS LIMITED LIABILITY COMPANY	3	
0004TE-B	FAMILY VIDEO MOVIE CLUB, INC.	5	
0005TE	BARRY RIGONI AND SANDRA G. RIGONI FORMERLY KNOWN AS SANDRA G. BERGER, A SINGLE PERSON, IN JOINT TENANCY	5	
0006TE	STANDARD BANK AND TRUST COMPANY, AN ILLINOIS BANKING CORPORATION	4	
0007TE	INLAND WEST RIVER CROSSINGS, L.L.C., A DELAWARE LIMITED LIABILITY COMPANY	6	
0008TE	CHARLES J. WILSON	6	
0009 0009TE	LID INVESTMENTS, LLC	7	
0010TE	RICHARD JAMES SHEGA, A BACHELOR	8	
0011 0011TE	ROY S. PROROK	7	
0012TE	KAYJAY LLC 2418 ESSINGTON	8	
0013 0013TE	DOROTHY S. SWITHIN	7	



PROJECT LENGTH = 1,140.00 LIN. FT. = 0.216 MILE, CATON FARM ROAD
= 683.00 LIN. FT. = 0.129 MILE, ESSINGTON ROAD
TOTAL LENGTH = 1,823.00 LIN. FT. = 0.345 MILE



IDOT USE ONLY

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PLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
PLAT OF HIGHWAYS

SCALE: SHEET 1 OF 9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	34
				CONTRACT NO. 61C29
ILLINOIS FED. AID PROJECT				

PART OF THE NE 1/4 OF SEC 35, TWP. 36 N., R. 9 E.
OF THE 3RD. P.M., IN WILL COUNTY, ILLINOIS.

PARCEL NUMBER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER
0001TE	19.573	N/A	N/A	19.573	0.041	DRIVEWAY CONSTRUCTION	05-03-35-202-041 09-03-35-209-084 06-01-36-209-086
0002TE	1.422	N/A	N/A	1.422	0.038	DRIVEWAY CONSTRUCTION	05-03-35-202-044

EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
0001	R73-32188	OCTOBER 23, 1973
0002	R2006-143197	AUGUST 25, 2006
903424 MAY 11, 1960		

UNIT ONE OF
CRYSTAL LAWN ADDITION
RECORDED MAY 11, 1960
AS DOCUMENT NO. 903424

SW CORNER SECTION 25-36-9
FOUND 1/2" IRON ROD
RECORDED MARCH 15, 2012
AS DOC. NO. R2012-028659
N. 1,784,313.933
E. 1,031,214.788
STA. 100+00.00
ESSINGTON ROAD = STA. 500+00.00

LEGEND

SECTION CORNER
QUARTER SECTION CORNER

SECTION LINE
QUARTER SECTION LINE
QUARTER SECTION LINE
FLATTED LOT LINE
PROPERTY (DEED) LINE

API
APPARENT PROPERTY LINE
EXISTING CENTER LINE
PROPOSED CENTER LINE
EXISTING RIGHT OF WAY LINE
PROPOSED RIGHT OF WAY LINE
EXISTING EASEMENT
PROPOSED EASEMENT
EXISTING ACCESS CONTROL LINE
PROPOSED ACCESS CONTROL LINE
MEASURED DIMENSION
COMPUTED DIMENSION
RECORD DIMENSION

EXISTING BUILDING

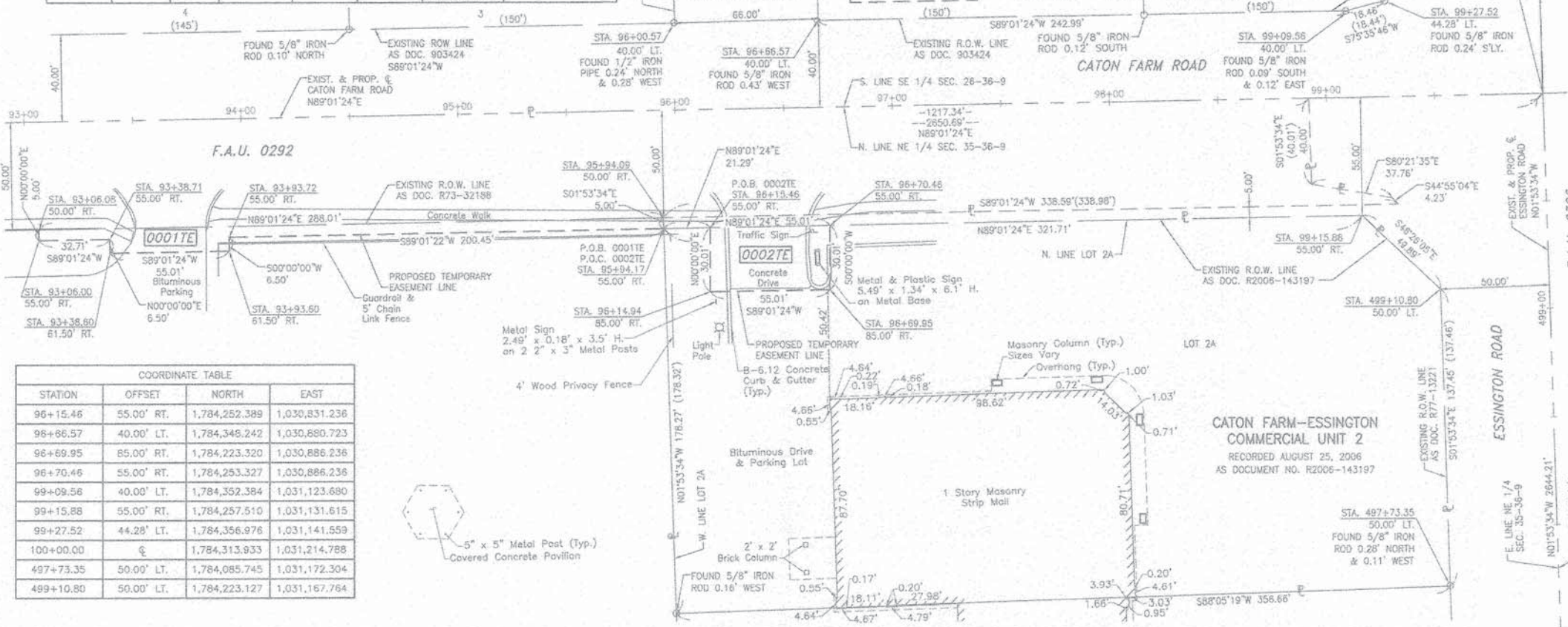
Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

○ IRON PIPE OR ROD FOUND
+ CUT CROSS FOUND OR SET
■ STAKING OF PROPOSED RIGHT OF WAY, SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
□ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }
COUNTY OF LAKE }
THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 35, TOWNSHIP 36N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS. DATED AT LAKE VILLA, ILLINOIS THIS 19th DAY OF December 2015 A.D.

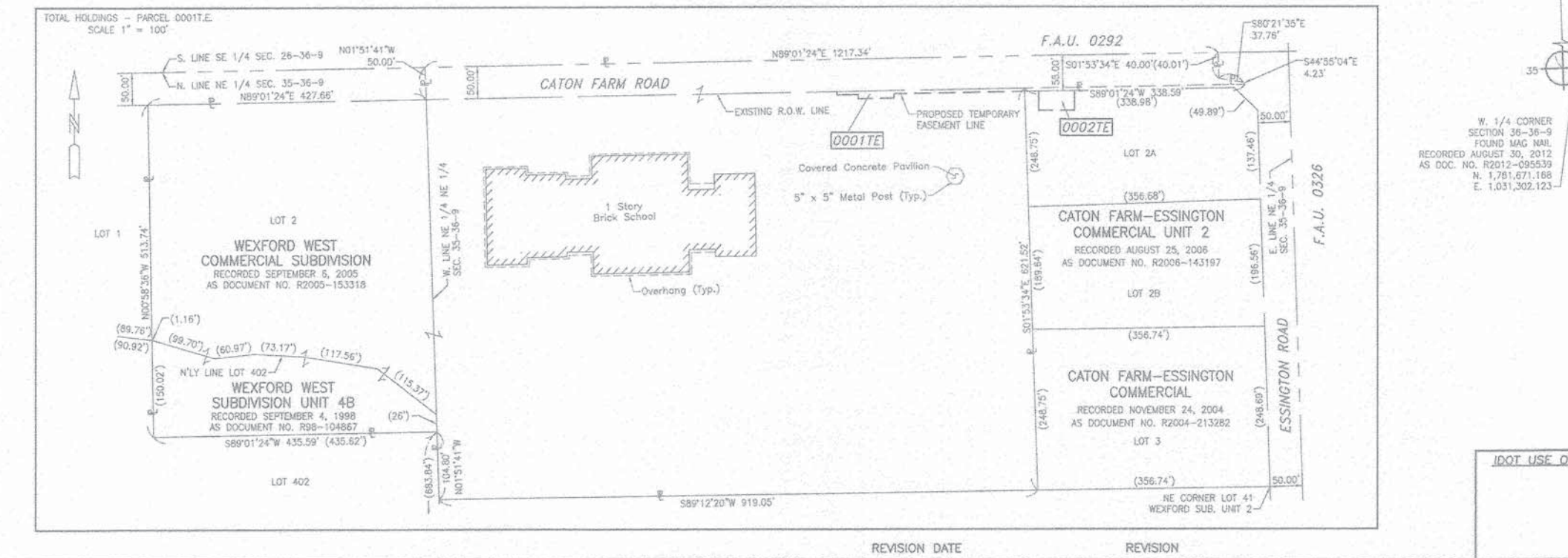
CHRISTIAN H. JORGENSEN
PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRATION DATE: NOVEMBER 30, 2016
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
All dimensions are measured unless otherwise specified.
Areas shown on this plat are ground.
All measured and computed distances are grid not ground.
To obtain ground distances, divide grid distances shown by the combined factor of 0.999954.

SCALE: 1"=30'



COORDINATE TABLE

STATION	OFFSET	NORTH	EAST
96+15.46	55.00' RT.	1,784,252.389	1,030,831.236
96+66.57	40.00' LT.	1,784,348.242	1,030,880.723
96+69.95	85.00' RT.	1,784,223.320	1,030,886.236
96+70.46	55.00' RT.	1,784,253.327	1,030,886.236
99+09.56	40.00' LT.	1,784,352.384	1,031,123.680
99+15.88	55.00' RT.	1,784,257.510	1,031,131.615
99+27.52	44.28' LT.	1,784,356.976	1,031,141.559
100+00.00	€	1,784,313.933	1,031,214.788
497+73.35	50.00' LT.	1,784,085.745	1,031,172.304
499+10.80	50.00' LT.	1,784,223.127	1,031,167.764



COORDINATE TABLE

STATION	OFFSET	NORTH	EAST
93+06.00	55.00' RT.	1,784,247.112	1,030,521.821
93+06.08	50.00' RT.	1,784,252.115	1,030,521.821
93+38.60	81.50' RT.	1,784,241.169	1,030,554.528
93+38.71	55.00' RT.	1,784,247.670	1,030,554.528
93+93.60	61.50' RT.	1,784,242.106	1,030,609.528
93+93.72	55.00' RT.	1,784,248.607	1,030,609.528
95+94.09	50.00' RT.	1,784,257.024	1,030,809.785
95+94.17	55.00' RT.	1,784,252.026	1,030,809.951
96+00.57	40.00' LT.	1,784,347.117	1,030,814.732
96+14.94	85.00' RT.	1,784,222.382	1,030,831.236

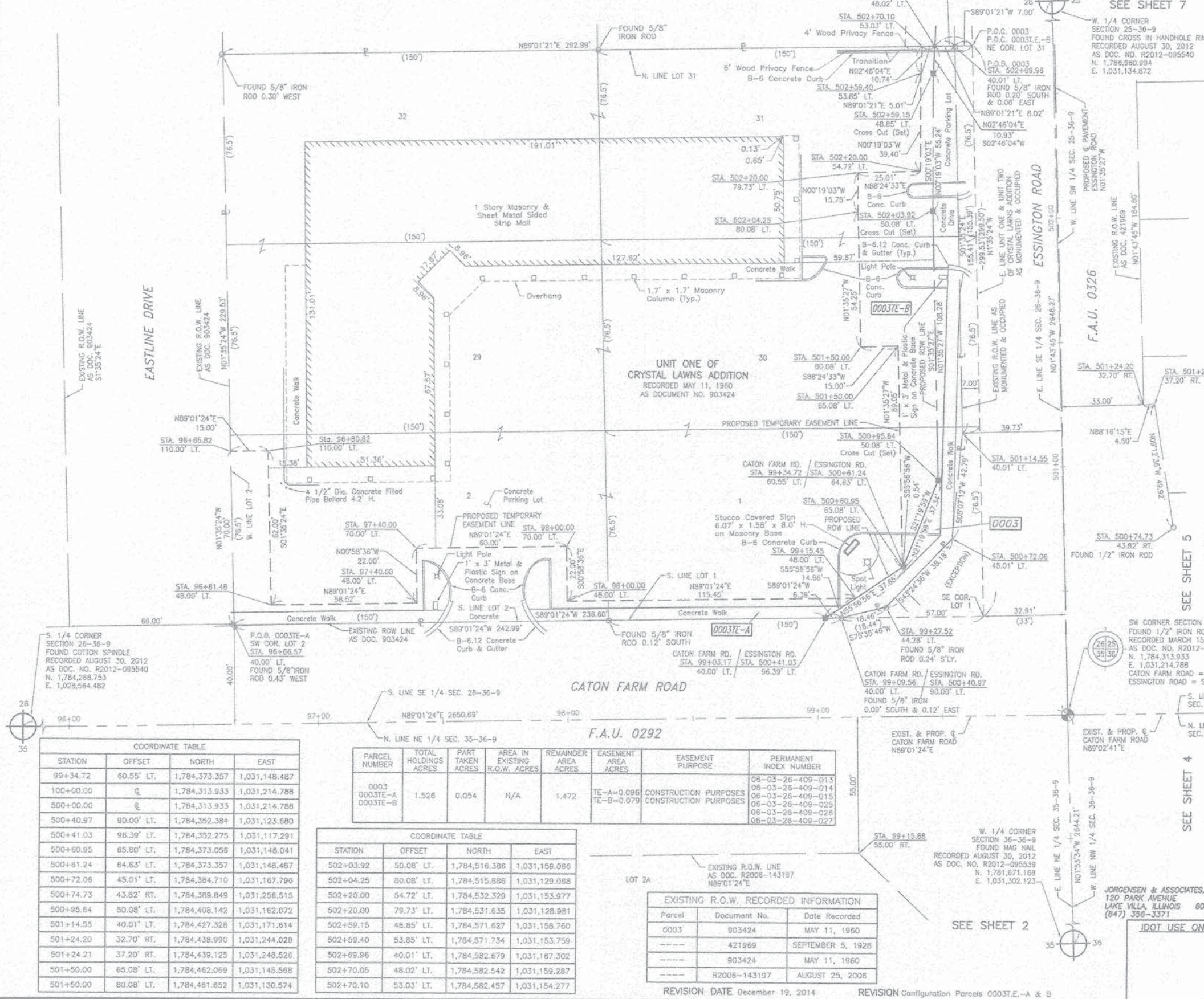
JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60046
(847) 356-3371

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
F.A.U. 0292 (CATON FARM ROAD) & F.A.U. 0326 (ESSINGTON ROAD)
LIMITS: CATON FARM ROAD (FROM COSMIC DR. TO JUNE ST.)
ESSINGTON ROAD (FROM BILTMORE ST. TO CATON FARM RD.)
COUNTY: WILL
SECTION: 11-00446-00-CH
JOB NO.: R-55-001-97
STATION 93+00 TO STATION 100+00.00
STATION 497+00 TO STATION 501+00
SCALE: 1"=30'
SHEET 2 OF 9

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

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PART OF THE SE 1/4 OF SEC. 26, TWP. 36 N., R. 9 E. OF THE 3RD. P.M., IN WILL COUNTY, ILLINOIS.



LEGEND

SECTION CORNER
QUARTER SECTION CORNER

SECTION LINE
QUARTER SECTION LINE
PLATTED LOT LINE
PROPERTY (DEED) LINE

APPARENT PROPERTY LINE
EXISTING CENTER LINE
PROPOSED CENTER LINE
EXISTING RIGHT OF WAY LINE
PROPOSED RIGHT OF WAY LINE
EXISTING EASEMENT
PROPOSED EASEMENT
EXISTING ACCESS CONTROL LINE
PROPOSED ACCESS CONTROL LINE
MEASURED DIMENSION
COMPUTED DIMENSION
RECORD DIMENSION

EXISTING BUILDING

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

○ IRON PIPE OR ROD FOUND
+ CUT CROSS FOUND OR SET
■ STAKING OF PROPOSED RIGHT OF WAY, SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
● PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }
COUNTY OF LAKE } SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 26, TOWNSHIP 36N, RANGE 9E, OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 22nd DAY OF December 2014.

CHRISTIAN H. JORGENSEN
2787 PROFESSIONAL LAND SURVEYOR
STATE OF ILLINOIS
LAKE VILLA, ILLINOIS

SCALE: 1"=20'

STATION	OFFSET	NORTH	EAST
99+34.72	60.55' LT.	1,784,373.357	1,031,148.487
100+00.00	□	1,784,313.933	1,031,214.788
500+00.00	□	1,784,313.933	1,031,214.788
500+40.97	90.00' LT.	1,784,352.384	1,031,123.680
500+41.03	96.39' LT.	1,784,352.275	1,031,117.291
500+60.95	65.80' LT.	1,784,373.056	1,031,148.041
500+61.24	64.63' LT.	1,784,373.357	1,031,148.487
500+72.06	45.01' LT.	1,784,384.710	1,031,167.796
500+74.73	43.62' RT.	1,784,389.849	1,031,256.515
500+95.64	50.08' LT.	1,784,408.142	1,031,162.072
501+14.55	40.01' LT.	1,784,427.328	1,031,171.614
501+24.20	32.70' RT.	1,784,438.990	1,031,244.028
501+24.21	37.20' RT.	1,784,438.125	1,031,248.526
501+50.00	65.08' LT.	1,784,462.069	1,031,145.568
501+50.00	80.08' LT.	1,784,461.652	1,031,130.574

STATION	OFFSET	NORTH	EAST
502+03.92	50.08' LT.	1,784,516.386	1,031,159.066
502+04.25	80.08' LT.	1,784,515.886	1,031,129.068
502+20.00	54.72' LT.	1,784,532.329	1,031,153.977
502+20.00	79.73' LT.	1,784,531.635	1,031,128.981
502+59.15	48.85' LT.	1,784,571.627	1,031,158.760
502+59.40	53.85' LT.	1,784,571.734	1,031,153.759
502+69.96	40.01' LT.	1,784,582.679	1,031,167.302
502+70.05	48.02' LT.	1,784,582.542	1,031,159.287
502+70.10	53.03' LT.	1,784,582.457	1,031,154.277

Parcel	Document No.	Date Recorded
0003	903424	MAY 11, 1960
----	421969	SEPTEMBER 5, 1928
----	903424	MAY 11, 1960
----	R2008-143197	AUGUST 25, 2008

STATION	OFFSET	NORTH	EAST
96+65.82	110.00' LT.	1,784,418.219	1,030,878.780
96+66.57	40.00' LT.	1,784,348.242	1,030,880.723
96+80.82	110.00' LT.	1,784,418.475	1,030,893.779
96+81.48	48.00' LT.	1,784,356.495	1,030,895.500
97+40.00	48.00' LT.	1,784,357.493	1,030,954.008
97+40.00	70.00' LT.	1,784,379.489	1,030,953.633
98+00.00	48.00' LT.	1,784,358.515	1,031,013.999
98+00.00	70.00' LT.	1,784,380.512	1,031,013.624
99+03.17	40.00' LT.	1,784,352.275	1,031,117.291
99+09.56	40.00' LT.	1,784,352.384	1,031,123.680
99+15.45	48.00' LT.	1,784,360.483	1,031,129.437
99+15.85	55.00' RT.	1,784,257.510	1,031,131.615
99+27.52	44.28' LT.	1,784,356.976	1,031,141.559

STATION	OFFSET	NORTH	EAST
99+34.72	60.55' LT.	1,784,373.357	1,031,148.487
100+00.00	□	1,784,313.933	1,031,214.788
500+00.00	□	1,784,313.933	1,031,214.788
500+40.97	90.00' LT.	1,784,352.384	1,031,123.680
500+41.03	96.39' LT.	1,784,352.275	1,031,117.291
500+60.95	65.80' LT.	1,784,373.056	1,031,148.041
500+61.24	64.63' LT.	1,784,373.357	1,031,148.487
500+72.06	45.01' LT.	1,784,384.710	1,031,167.796
500+74.73	43.62' RT.	1,784,389.849	1,031,256.515
500+95.64	50.08' LT.	1,784,408.142	1,031,162.072
501+14.55	40.01' LT.	1,784,427.328	1,031,171.614
501+24.20	32.70' RT.	1,784,438.990	1,031,244.028
501+24.21	37.20' RT.	1,784,438.125	1,031,248.526
501+50.00	65.08' LT.	1,784,462.069	1,031,145.568
501+50.00	80.08' LT.	1,784,461.652	1,031,130.574

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
F.A.U. 0292 (CATON FARM ROAD) & F.A.U. 0326 (ESSINGTON ROAD)
LIMITS: CATON FARM ROAD (FROM COSMIC DR. TO JUNE ST.)
ESSINGTON ROAD (FROM BILTMORE ST. TO CATON FARM RD.)
COUNTY: WILL
JOB NO.: R-55-001-97
SECTION: 11-00446-00-CH
STATION 96+00 TO STATION 100+00.00
STATION 500+00.00 TO STATION 503+00
SCALE: 1"=20'
SHEET 3 OF 9

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

PLT DRIVER = ...
PEN TABLE = ...
FILE NAME = ...



USER NAME = tgb
DESIGNED - TGB
DRAWN - TGB
CHECKED - JRV
DATE - 12/07/15

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
PLAT OF HIGHWAYS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	36

CONTRACT NO. 61C29
ILLINOIS FED. AID PROJECT

SCALE: SHEET 3 OF 9 SHEETS STA. TO STA.

PART OF THE NW 1/4 OF SEC 36, TWP. 36 N., R. 9 E. OF THE 3RD. P.M., IN WILL COUNTY, ILLINOIS.

EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
0006	R98-080953	JULY 15, 1998
---	421969	SEPTEMBER 5, 1928

LEGEND

SECTION CORNER
QUARTER SECTION CORNER

SECTION LINE
QUARTER SECTION LINE
QUARTER, QUARTER SECTION LINE
PLATTED LOT LINE
PROPERTY (DEED) LINE

APL
APPARENT PROPERTY LINE
EXISTING CENTER LINE
PROPOSED CENTER LINE
EXISTING RIGHT OF WAY LINE
PROPOSED RIGHT OF WAY LINE
EXISTING EASEMENT
PROPOSED EASEMENT
EXISTING ACCESS CONTROL LINE
PROPOSED ACCESS CONTROL LINE
MEASURED DIMENSION
COMPUTED DIMENSION
RECORD DIMENSION

EXISTING BUILDING

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

○ IRON PIPE OR ROD FOUND ⊕ "MAG" NAIL SET
+ CUT CROSS FOUND OR SET ● 5/8" REBAR SET

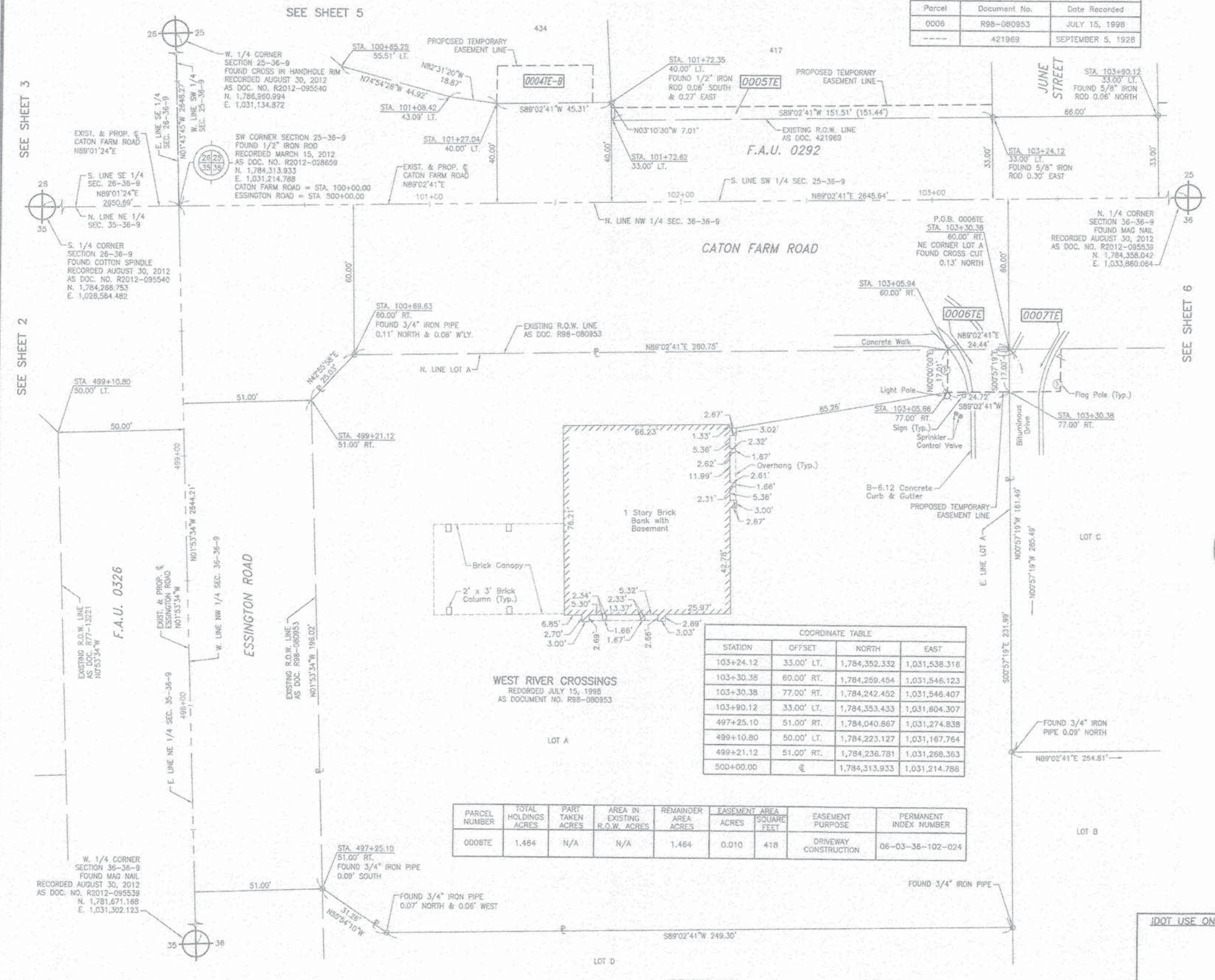
■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

⊕ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)

□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }
COUNTY OF LAKE } SS



CHRISTIAN H. JORGENSEN
2787 PROFESSIONAL LAND SURVEYOR
STATE OF ILLINOIS
LAKE VILLA, ILLINOIS

CHRISTIAN H. JORGENSEN
PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRATION DATE: NOVEMBER 30, 2016
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
All dimensions are measured unless otherwise specified.
Areas shown on this plat are ground.
All measured and computed distances are grid not ground.
To obtain ground distances, divide grid distances shown by the combined factor of 0.999954.

JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60045
(647) 356-3371

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
F.A.U. 0292 (CATON FARM ROAD) & F.A.U. 0326 (ESSINGTON ROAD)
LIMITS: CATON FARM ROAD (FROM COSMIC DR. TO JUNE ST.)
ESSINGTON ROAD (FROM BILTMORE ST. TO CATON FARM RD.)
COUNTY: WILL
SECTION: 11-00446-00-CH
STATION 100+00.00 TO STATION 104+00
STATION 497+00 TO STATION 500+00.00
SCALE: 1"=20'
JOB NO.: R-55-001-97
SHEET 4 OF 9

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

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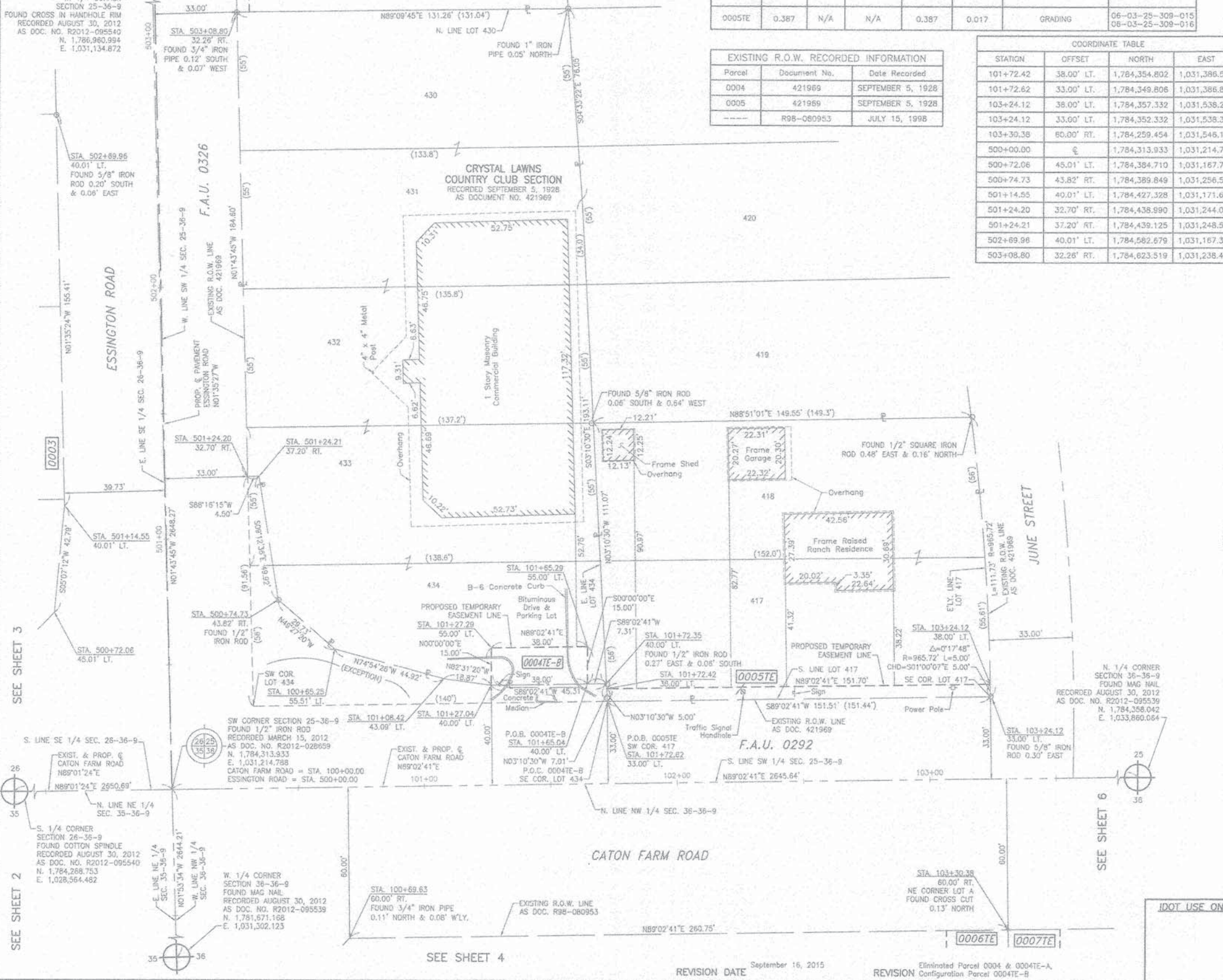
USER NAME = tgb	DESIGNED - TGB	REVISED -
PLOT SCALE = 1:2000' / in.	DRAWN - TGB	REVISED -
PLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD PLAT OF HIGHWAYS		F.A.U. RTE. 292	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 37
SCALE:	SHEET 4 OF 9 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT			
						CONTRACT NO. 61C29

PART OF THE SW 1/4 OF SEC. 25, TWP. 36 N., R. 9 E. OF THE 3RD. P.M., IN WILL COUNTY, ILLINOIS.

SEE SHEET 7
W. 1/4 CORNER SECTION 25-36-9
FOUND CROSS IN HANDHOLE RIM
RECORDED AUGUST 30, 2012
AS DOC. NO. R2012-095540
N. 1,786,960.994
E. 1,031,134.872



PARCEL NUMBER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER
0004TE-B	0.801	N/A	N/A	0.801	TE-B=0.013	DRIVEWAY CONSTRUCTION	06-03-25-309-024
0005TE	0.387	N/A	N/A	0.387	0.017	GRADING	06-03-25-309-015 06-03-25-309-016

Parcel	Document No.	Date Recorded
0004	421969	SEPTEMBER 5, 1928
0005	421969	SEPTEMBER 5, 1928
---	R98-080953	JULY 15, 1998

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
101+72.42	38.00' LT.	1,784,354.802	1,031,386.555
101+72.62	33.00' LT.	1,784,349.806	1,031,386.832
103+24.12	38.00' LT.	1,784,357.332	1,031,538.229
103+24.12	33.00' LT.	1,784,352.332	1,031,538.316
103+30.38	80.00' RT.	1,784,259.454	1,031,546.123
500+00.00	€	1,784,313.933	1,031,214.788
500+72.06	45.01' LT.	1,784,384.710	1,031,167.796
500+74.73	43.82' RT.	1,784,389.849	1,031,256.515
501+14.55	40.01' LT.	1,784,427.528	1,031,171.614
501+24.20	32.70' RT.	1,784,438.990	1,031,244.028
501+24.21	37.20' RT.	1,784,439.125	1,031,248.526
502+69.96	40.01' LT.	1,784,582.679	1,031,167.302
503+08.80	32.26' RT.	1,784,623.519	1,031,238.457

LEGEND

SECTION CORNER
QUARTER SECTION CORNER

SECTION LINE
QUARTER SECTION LINE
PLATTED LOT LINE
PROPERTY (DEED) LINE

APPARENT PROPERTY LINE
EXISTING CENTER LINE
PROPOSED CENTER LINE
EXISTING RIGHT OF WAY LINE
PROPOSED RIGHT OF WAY LINE
EXISTING EASEMENT
PROPOSED EASEMENT
EXISTING ACCESS CONTROL LINE
PROPOSED ACCESS CONTROL LINE
MEASURED DIMENSION
COMPUTED DIMENSION
RECORD DIMENSION

EXISTING BUILDING

IRON PIPE OR ROD FOUND
CUT CROSS FOUND OR SET
STAKING OF PROPOSED RIGHT OF WAY, SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
PERMANENT SURVEY MARKER, I.O.D.T STD 2135 (TO BE SET BY OTHERS)
RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }
COUNTY OF LAKE }
SS

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

0 20' 40'
SCALE: 1"=20'

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 25, TOWNSHIP 36N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 19th DAY OF December 20, 2015 A.D.

Christian H. Jorgensen PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRATION DATE: NOVEMBER 30, 2016
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
All dimensions are measured unless otherwise specified.
Areas shown on this plat are ground.
All measured and computed distances are grid not ground.
To obtain ground distances, divide grid distances shown by the combined factor of 0.999954.



COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
100+00.00	€	1,784,313.933	1,031,214.788
100+65.25	55.51' LT.	1,784,370.523	1,031,279.109
100+69.63	60.00' RT.	1,784,255.106	1,031,285.412
101+08.42	43.09' LT.	1,784,358.827	1,031,322.478
101+27.04	40.00' LT.	1,784,356.046	1,031,341.142
101+27.29	55.00' LT.	1,784,371.048	1,031,341.142
101+85.04	40.00' LT.	1,784,356.679	1,031,379.142
101+85.29	55.00' LT.	1,784,371.681	1,031,379.142
101+72.35	40.00' LT.	1,784,356.801	1,031,386.444

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
F.A.U. 0292 (CATON FARM ROAD) & F.A.U. 0326 (ESSINGTON ROAD)
LIMITS: CATON FARM ROAD (FROM COSMIC DR. TO JUNE ST.)
ESSINGTON ROAD (FROM BILTMORE ST. TO CATON FARM RD.)

SECTION: 11-00446-00-CH
STATION 100+00.00 TO STATION 104+00
STATION 500+00.00 TO STATION 504+00
SCALE: 1"=20' SHEET 5 OF 9

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

PLOT DRIVER = ...
PEN TABLE = ...
FILE NAME = ...



USER NAME = tgb	DESIGNED - TGB	REVISED -
PLAT SCALE = 1:2000	DRAWN - TGB	REVISED -
PLAT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD	
PLAT OF HIGHWAYS	
SCALE:	SHEET 5 OF 9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	38
				CONTRACT NO. 61C29
ILLINOIS FED. AID PROJECT				

PART OF THE SW 1/4 OF SEC. 25 AND PART OF THE NW 1/4 OF SEC 36, TWP. 36 N., R. 9 E. OF THE 3RD. P.M., IN WILL COUNTY, ILLINOIS.

PARCEL NUMBER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA SQUARE FEET	EASEMENT PURPOSE	PERMANENT INDEX NUMBER
0007TE	4.345	N/A	N/A	4.345	0.008	DRIVEWAY CONSTRUCTION	06-03-38-102-028 06-03-38-102-027
0008TE	0.648	N/A	N/A	0.648	0.007	GRADING	06-03-25-310-022

LEGEND

SECTION CORNER 16 15
QUARTER SECTION CORNER

SECTION LINE
QUARTER SECTION LINE
QUARTER SECTION LINE
PLATTED LOT LINE
PROPERTY (DEED) LINE

APPARENT PROPERTY LINE
EXISTING CENTER LINE
PROPOSED CENTER LINE
EXISTING RIGHT OF WAY LINE
PROPOSED RIGHT OF WAY LINE
EXISTING EASEMENT
PROPOSED EASEMENT
EXISTING ACCESS CONTROL LINE
PROPOSED ACCESS CONTROL LINE
MEASURED DIMENSION
COMPUTED DIMENSION
RECORD DIMENSION

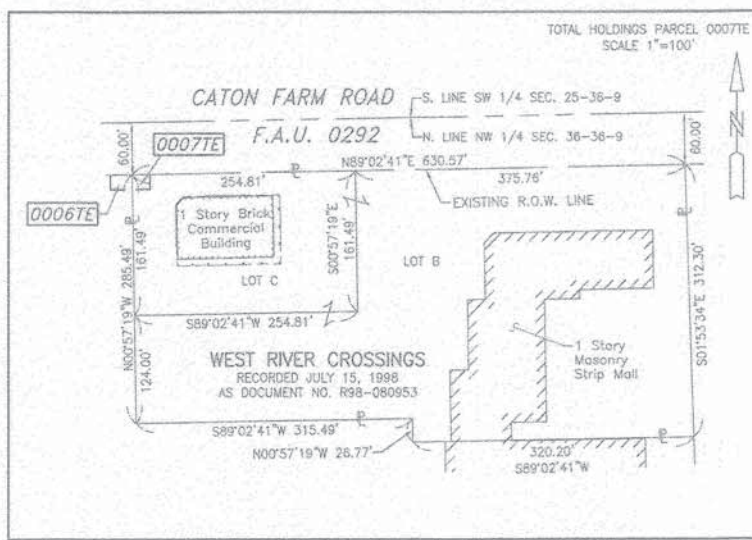
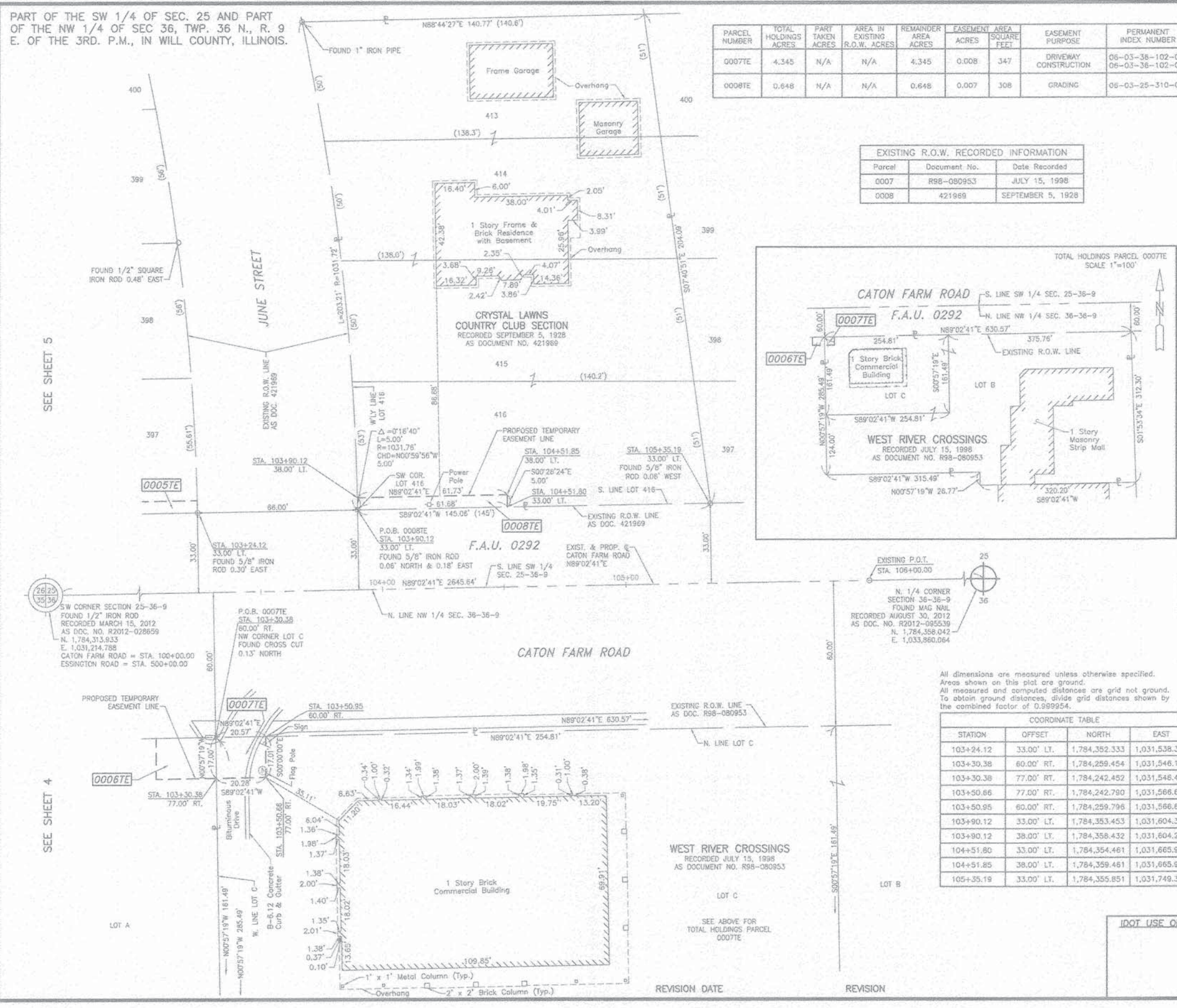
EXISTING BUILDING

Scale: 1"=20'

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

EXISTING R.O.W. RECORDED INFORMATION

Parcel	Document No.	Date Recorded
0007	R98-080953	JULY 15, 1998
0008	421969	SEPTEMBER 5, 1928



- IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊕ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }
COUNTY OF LAKE }
SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 25, TOWNSHIP 36N., RANGE 9E. AND SECTION 36, TOWNSHIP 36N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 22nd DAY OF December 2016 A.D.



Christian H. Jorgensen PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRATION DATE: NOVEMBER 30, 2016
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

All dimensions are measured unless otherwise specified.
Areas shown on this plat are ground.
All measured and computed distances are grid not ground.
To obtain ground distances, divide grid distances shown by the combined factor of 0.999954.

COORDINATE TABLE

STATION	OFFSET	NORTH	EAST
103+24.12	33.00' LT.	1,784,352.333	1,031,538.316
103+30.38	60.00' RT.	1,784,259.454	1,031,546.123
103+30.38	77.00' RT.	1,784,242.452	1,031,548.407
103+50.66	77.00' RT.	1,784,242.790	1,031,566.687
103+50.95	60.00' RT.	1,784,259.796	1,031,566.687
103+90.12	33.00' LT.	1,784,353.453	1,031,604.307
103+90.12	38.00' LT.	1,784,358.432	1,031,604.220
104+51.80	33.00' LT.	1,784,354.461	1,031,665.976
104+51.85	38.00' LT.	1,784,359.461	1,031,665.938
105+35.19	33.00' LT.	1,784,355.851	1,031,749.350

JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60046
(847) 356-3371

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
F.A.U. 0292 (CATON FARM ROAD) & F.A.U. 0326 (ESSINGTON ROAD)
LIMITS: CATON FARM ROAD (FROM COSMIC DR. TO JUNE ST.)
ESSINGTON ROAD (FROM BILTMORE ST. TO CATON FARM RD.)
COUNTY: WILL
SECTION: 11-00446-00-CH
JOB NO.: R-55-001-97
TO STATION 103+00
TO STATION 108+00.00
SCALE: 1"=20'
SHEET 6 OF 9

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

USER NAME = tgb	DESIGNED - TGB	REVISED -
PLOT SCALE = 1:8000' / 1"	DRAWN - TGB	REVISED -
PLOT DATE = 12/31/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
PLAT OF HIGHWAYS
SCALE: SHEET 6 OF 9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	39

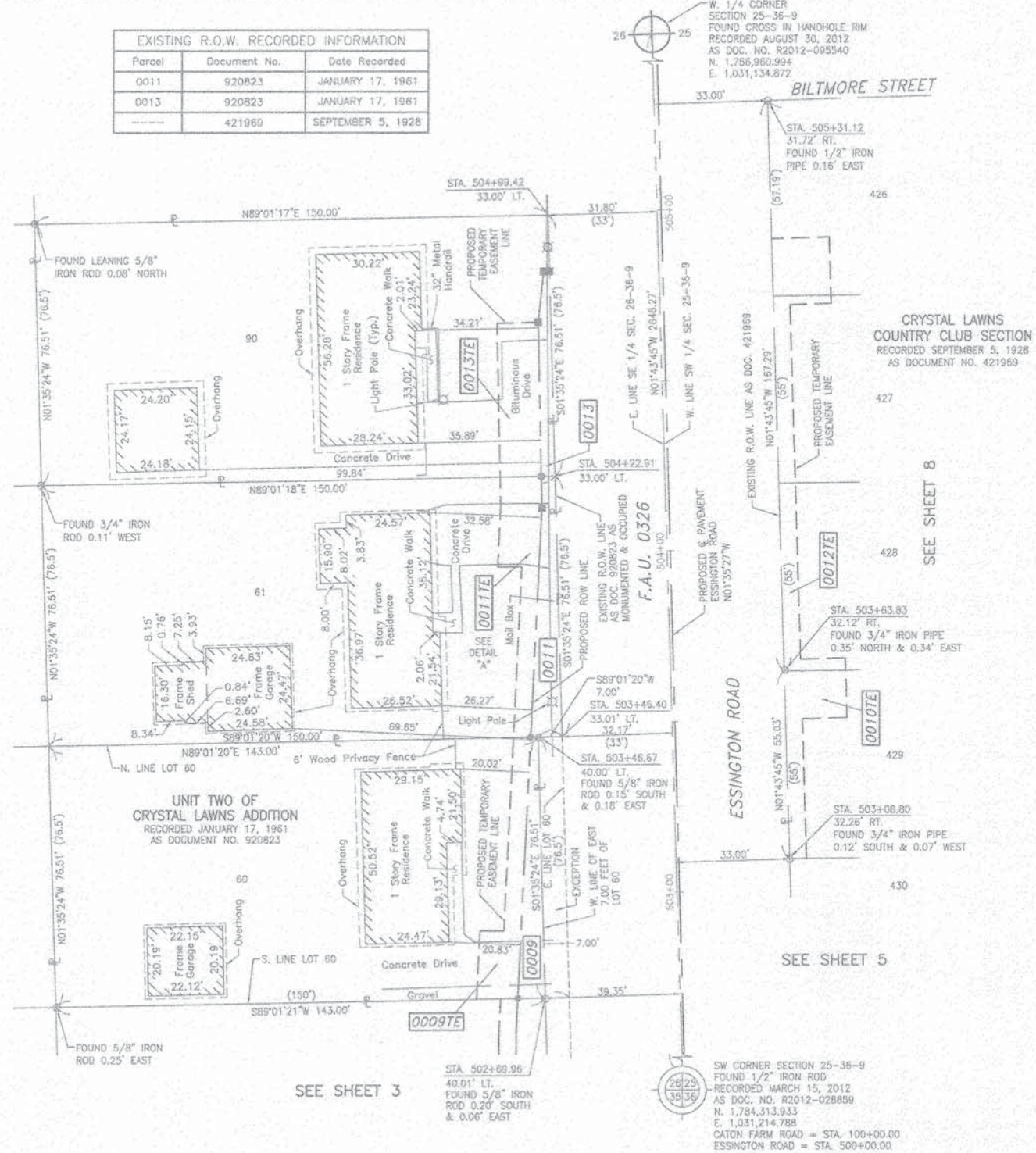
CONTRACT NO. 61C29
ILLINOIS FED. AID PROJECT

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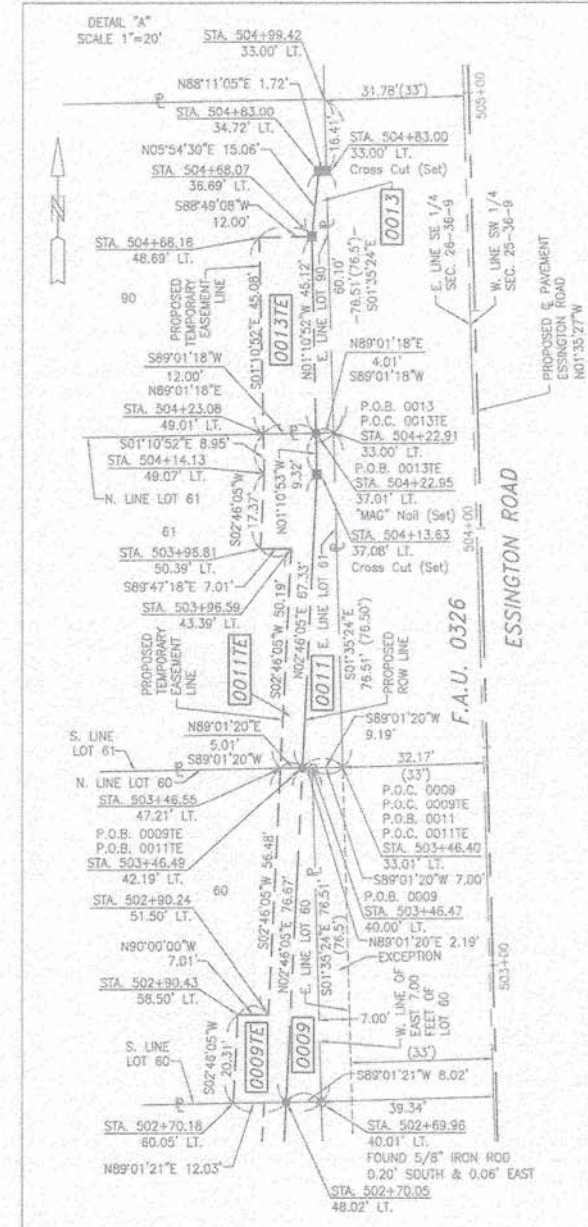
PART OF THE SE 1/4 OF SEC. 26, TWP. 36 N., R. 9 E. OF THE 3RD. P.M., IN WILL COUNTY, ILLINOIS.

EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
0011	920823	JANUARY 17, 1961
0013	920823	JANUARY 17, 1961
---	421969	SEPTEMBER 5, 1928



PARCEL NUMBER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER
0009 0009TE	0.251	0.009	390	N/A	0.242	CONSTRUCTION PURPOSES	06-03-26-409-024
0011 0011TE	0.263	0.011	N/A	N/A	0.252	CONSTRUCTION PURPOSES	06-03-26-409-023
0013 0013TE	0.263	0.005	214	N/A	0.258	DRIVEWAY CONSTRUCTION	06-03-26-409-022

SW CORNER SECTION 25-36-9
FOUND 1/2" IRON ROD
RECORDED MARCH 15, 2012
AS DOC. NO. R2012-028859
N. 1,784,313.933
E. 1,031,214.788
CATON FARM ROAD = STA. 100+00.00
ESSINGTON ROAD = STA. 500+00.00



STATION	OFFSET	NORTH	EAST
503+46.49	42.19' LT.	1,784,659.123	1,031,162.890
503+46.55	47.21' LT.	1,784,659.037	1,031,157.980
503+63.83	32.12' RT.	1,784,678.516	1,031,236.796
503+96.59	43.39' LT.	1,784,709.164	1,031,160.403
503+96.81	50.39' LT.	1,784,709.190	1,031,153.398
504+13.63	37.08' LT.	1,784,726.374	1,031,166.241
504+14.13	49.07' LT.	1,784,726.540	1,031,154.235
504+22.91	33.00' LT.	1,784,735.760	1,031,170.958
504+22.95	37.01' LT.	1,784,735.692	1,031,166.049
504+23.08	49.01' LT.	1,784,735.487	1,031,154.050
504+68.07	36.69' LT.	1,784,780.803	1,031,165.119
504+68.16	48.69' LT.	1,784,780.556	1,031,153.121
504+83.00	34.72' LT.	1,784,795.779	1,031,166.668
504+83.00	33.00' LT.	1,784,795.834	1,031,168.388
504+99.42	33.00' LT.	1,784,812.241	1,031,167.933
505+31.12	31.72' RT.	1,784,845.729	1,031,231.748

LEGEND

- SECTION LINE
- QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- EXISTING CENTER LINE
- PROPOSED CENTER LINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- EXISTING EASEMENT
- PROPOSED EASEMENT
- EXISTING ACCESS CONTROL LINE
- PROPOSED ACCESS CONTROL LINE
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORD DIMENSION
- EXISTING BUILDING

BEARINGS AND COORDINATES ARE REFERENCED TO THE ILLINOIS COORDINATE SYSTEM NAD 83(2007) EAST ZONE.

0 IRON PIPE OR ROD FOUND
+ CUT CROSS FOUND OR SET
■ STAKING OF PROPOSED RIGHT OF WAY, SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, BURIED 5/8 INCH METAL ROD 30 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
● PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
□ RIGHT OF WAY STAKING PROPOSED TO BE SET.
STATE OF ILLINOIS }
COUNTY OF LAKE }
THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 26, TOWNSHIP 36N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS, DATED AT LAKE VILLA, ILLINOIS THIS 19th DAY OF December 2015.

CHRISTIAN H. JORGENSEN
2787 PROFESSIONAL LAND SURVEYOR
STATE OF ILLINOIS
LAKE VILLA, ILLINOIS

CHRISTIAN H. JORGENSEN, PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRATION DATE: NOVEMBER 30, 2016

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
All dimensions are measured unless otherwise specified.
Areas shown on this plat are ground.
All measured and computed distances are grid not ground.
To obtain ground distances, divide grid distances shown by the combined factor of 0.999954.

STATION	OFFSET	NORTH	EAST
500+00.00	0	1,784,313.933	1,031,214.788
502+69.96	40.01' LT.	1,784,582.679	1,031,167.302
502+70.05	48.02' LT.	1,784,582.542	1,031,159.267
502+70.18	80.05' LT.	1,784,582.337	1,031,147.263
502+90.24	51.50' LT.	1,784,602.626	1,031,155.252
502+90.43	58.50' LT.	1,784,602.626	1,031,148.244
503+08.80	32.26' RT.	1,784,623.509	1,031,238.457
503+46.40	33.01' LT.	1,784,659.279	1,031,172.178
503+46.47	40.00' LT.	1,784,659.160	1,031,165.179



COORDINATE TABLE

STATION	OFFSET	NORTH	EAST
500+00.00	0	1,784,313.933	1,031,214.788
502+69.96	40.01' LT.	1,784,582.679	1,031,167.302
502+70.05	48.02' LT.	1,784,582.542	1,031,159.267
502+70.18	80.05' LT.	1,784,582.337	1,031,147.263
502+90.24	51.50' LT.	1,784,602.626	1,031,155.252
502+90.43	58.50' LT.	1,784,602.626	1,031,148.244
503+08.80	32.26' RT.	1,784,623.509	1,031,238.457
503+46.40	33.01' LT.	1,784,659.279	1,031,172.178
503+46.47	40.00' LT.	1,784,659.160	1,031,165.179

JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60046
(847) 356-3371

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
F.A.U. 0292 (CATON FARM ROAD) & F.A.U. 0326 (ESSINGTON ROAD)
LIMITS: CATON FARM ROAD (FROM COSMIC DR. TO JUNE ST.)
ESSINGTON ROAD (FROM BILTMORE ST. TO CATON FARM RD.)
COUNTY: WILL
JOB NO.: R-55-001-97
SECTION: 11-00446-00-CH
STATION 502+00 TO STATION 506+00
SCALE: 1"=20'
SHEET 7 OF 9

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

PLOT DRIVER = ...
PEN TABLE = ...
FILE NAME = ...



USER NAME = tgb
DESIGNED - TGB
DRAWN - TGB
CHECKED - JRV
DATE = 12/3/2015

REVISION DATE
REVISION

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

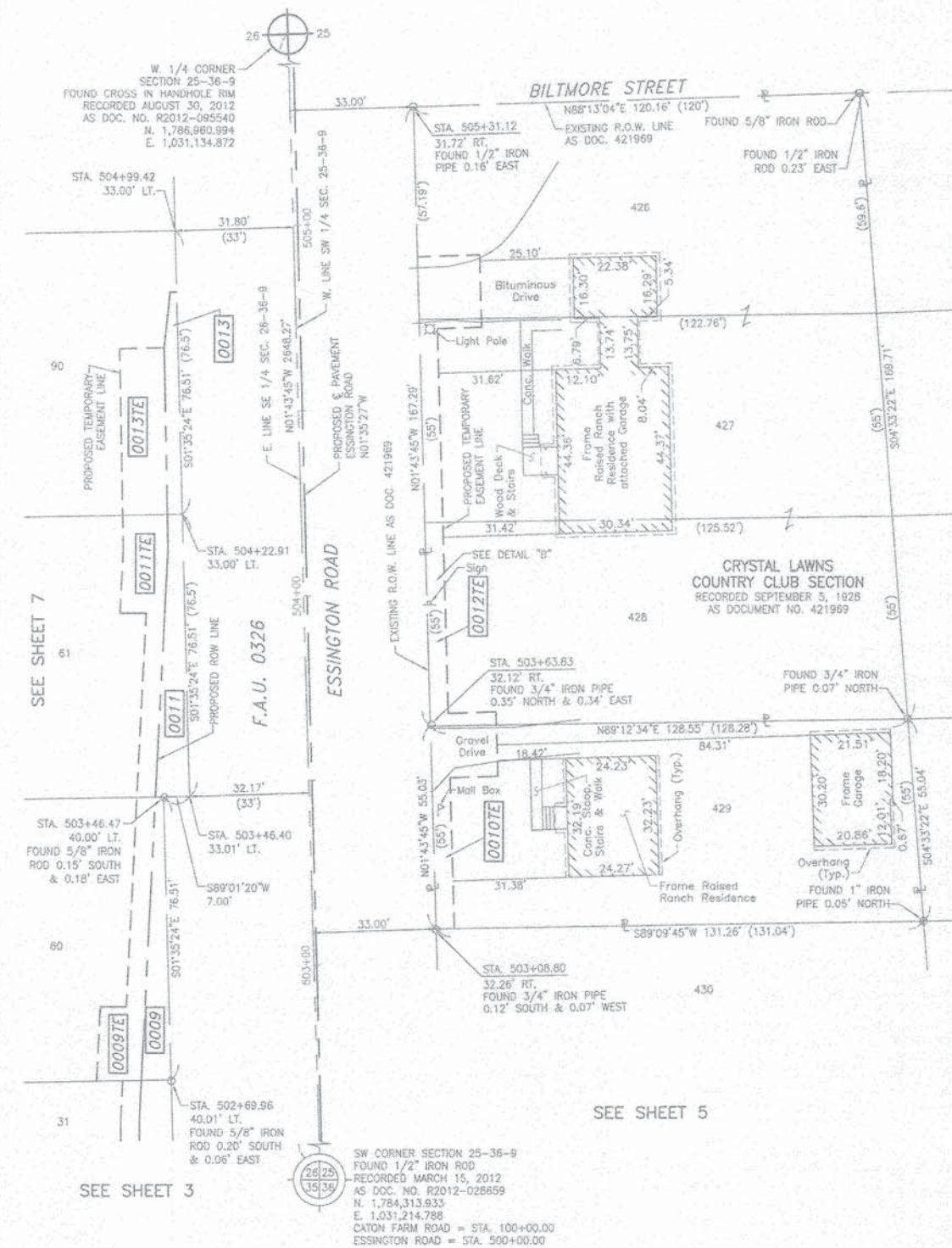
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
PLAT OF HIGHWAYS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	40

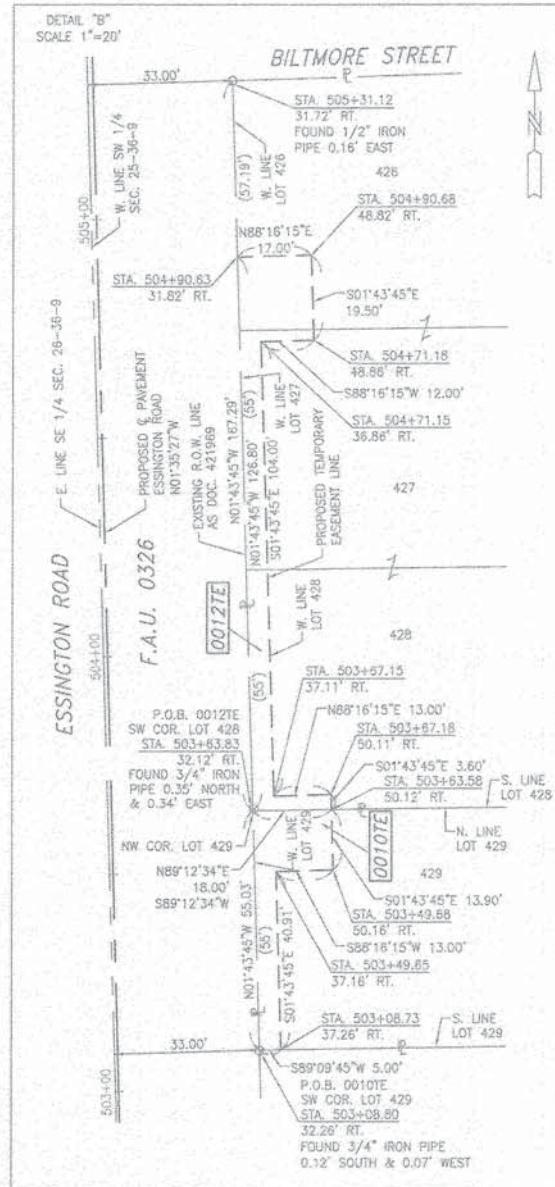
CONTRACT NO. 61C29
ILLINOIS FED. AID PROJECT

PART OF THE SW 1/4 OF SEC 25, TWP. 36 N., R. 9 E. OF THE 3RD. P.M., IN WILL COUNTY, ILLINOIS.



PARCEL NUMBER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT AREA SQUARE FEET	EASEMENT PURPOSE	PERMANENT INDEX NUMBER
0010TE	0.164	N/A	N/A	0.164	0.010	487	CONSTRUCTION PURPOSES	06-03-25-309-002
0012TE	0.481	N/A	N/A	0.481	0.021	N/A	CONSTRUCTION PURPOSES	06-03-25-309-001

Parcel	Document No.	Date Recorded
0010TE	421969	SEPTEMBER 5, 1928
0012TE	421969	SEPTEMBER 5, 1928



STATION	OFFSET	NORTH	EAST
503+63.83	32.12' RT.	1,784,678.516	1,031,236.796
503+67.15	37.11' RT.	1,784,681.970	1,031,241.894
503+67.18	50.11' RT.	1,784,682.362	1,031,254.688
504+22.91	33.00' LT.	1,784,735.780	1,031,170.056
504+71.15	36.86' RT.	1,784,785.922	1,031,238.556
504+71.18	48.86' RT.	1,784,786.284	1,031,250.550
504+90.63	31.82' RT.	1,784,805.263	1,031,232.970
504+90.88	48.82' RT.	1,784,805.776	1,031,249.962
504+99.42	33.00' LT.	1,784,812.241	1,031,167.933
505+31.12	31.72' RT.	1,784,845.728	1,031,231.748

LEGEND

SECTION CORNER: 910, 1815
QUARTER SECTION CORNER: 16, 15

SECTION LINE
QUARTER SECTION LINE
QUARTER, QUARTER SECTION LINE
PLATTED LOT LINE
PROPERTY (DEED) LINE

APPARENT PROPERTY LINE
EXISTING CENTER LINE
PROPOSED CENTER LINE
EXISTING RIGHT OF WAY LINE
PROPOSED RIGHT OF WAY LINE
EXISTING EASEMENT
PROPOSED EASEMENT
EXISTING ACCESS CONTROL LINE
PROPOSED ACCESS CONTROL LINE
MEASURED DIMENSION
COMPUTED DIMENSION
RECORD DIMENSION

EXISTING BUILDING

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

IRON PIPE OR ROD FOUND
CUT CROSS FOUND OR SET
STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
PERMANENT SURVEY MARKER, I.D.O.T. STD 2135 (TO BE SET BY OTHERS)
RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }
COUNTY OF LAKE } SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 25, TOWNSHIP 36N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS. DATED AT LAKE VILLA, ILLINOIS THIS 22nd DAY OF December 2014 A.D.

Christian H. Jorgensen, PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRES DATE: NOVEMBER 30, 2016

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

All dimensions are measured unless otherwise specified.
Areas shown on this plot are ground.
To obtain ground distances, divide grid distances shown by the combined factor of 0.999954.

Christian H. Jorgensen, PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRES DATE: NOVEMBER 30, 2016

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

All dimensions are measured unless otherwise specified.
Areas shown on this plot are ground.
To obtain ground distances, divide grid distances shown by the combined factor of 0.999954.

STATION	OFFSET	NORTH	EAST
500+00.00	@	1,784,313.935	1,031,214.788
502+69.96	40.01' LT.	1,784,582.679	1,031,187.302
503+08.73	37.26' RT.	1,784,623.582	1,031,243.457
503+08.80	32.26' RT.	1,784,623.509	1,031,238.457
503+46.40	33.01' LT.	1,784,659.279	1,031,172.178
503+46.47	40.00' LT.	1,784,659.160	1,031,165.179
503+49.65	37.16' RT.	1,784,664.478	1,031,242.222
503+49.68	50.16' RT.	1,784,664.870	1,031,255.216
503+63.58	50.12' RT.	1,784,678.765	1,031,254.797

JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60046
(847) 356-3371

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
F.A.U. 0292 (CATON FARM ROAD) & F.A.U. 0326 (ESSINGTON ROAD)
LIMITS: CATON FARM ROAD (FROM COSMIC DR. TO JUNE ST.)
ESSINGTON ROAD (FROM BILTMORE ST. TO CATON FARM RD.)
COUNTY: WILL
JOB NO.: R-55-001-97
SECTION: 11-00446-00-CH TO STATION 506+00
STATION 503+00 SHEET 8 OF 9
SCALE: 1"=20'

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

PLOT DRIVER = ...
PEN TABLE = ...
FILE NAME = ...



USER NAME	DESIGNED	REVISION
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	TGB	-
	JRV	-
	-	-

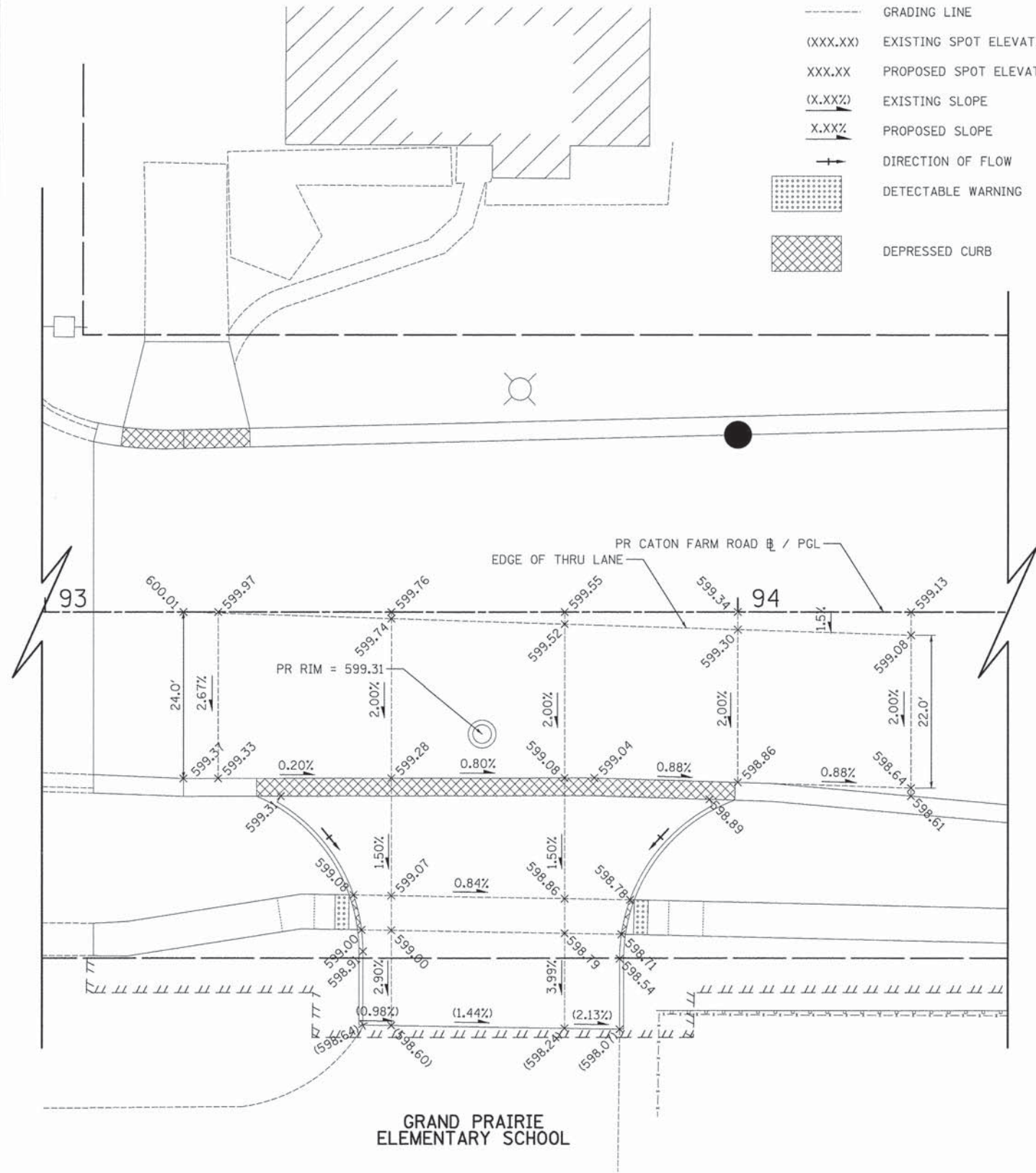
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
	8	9				

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	41
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT				

LEGEND

- GRADING LINE
- (XXX.XX) EXISTING SPOT ELEVATION
- XXX.XX PROPOSED SPOT ELEVATION
- (X.XX%) EXISTING SLOPE
- X.XX% PROPOSED SLOPE
- DIRECTION OF FLOW
- [Dotted Pattern] DETECTABLE WARNING
- [Cross-hatched Pattern] DEPRESSED CURB

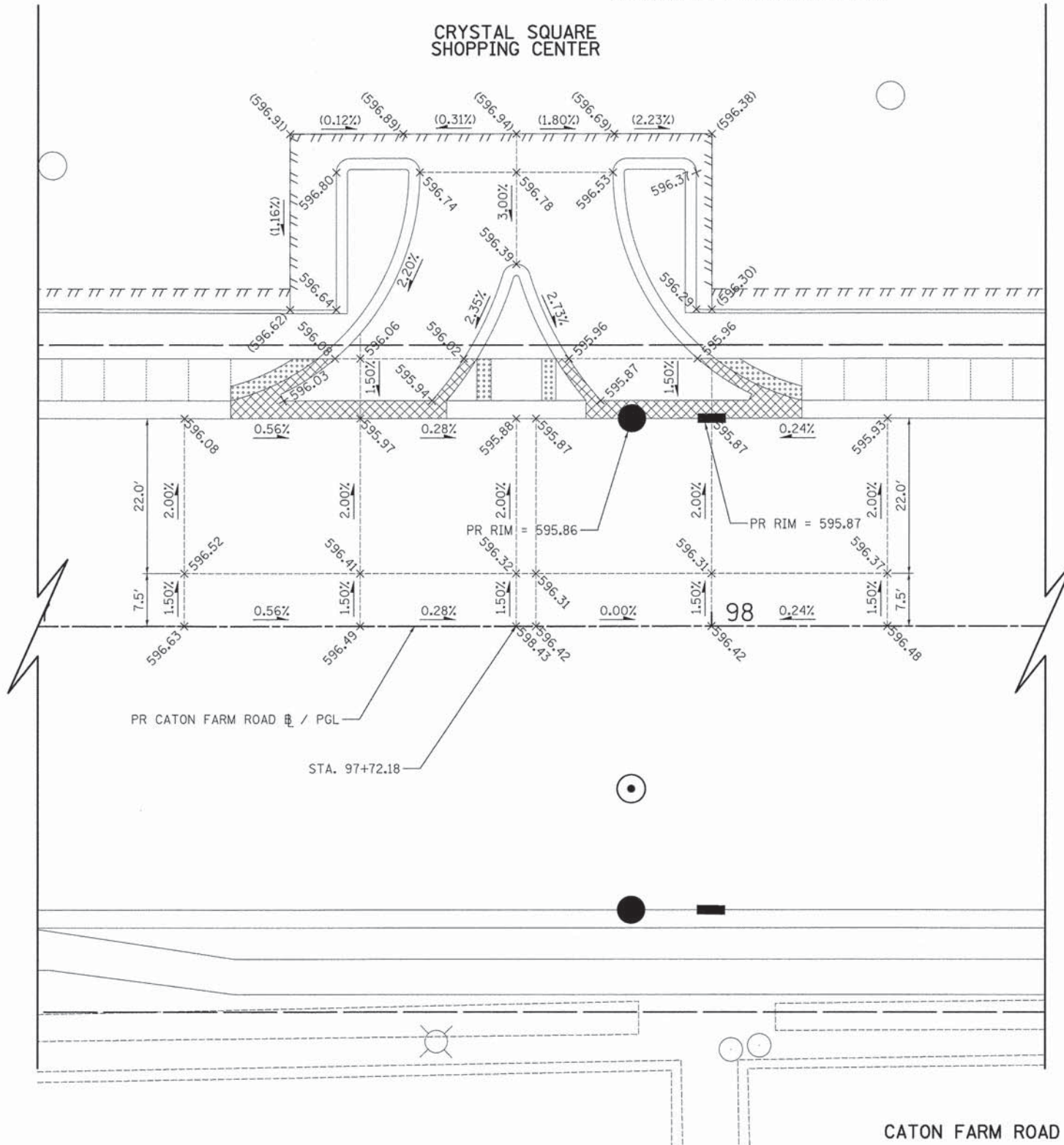


NOTES

1. ELEVATIONS ARE SHOWN AT 25' STATION INTERVALS UNLESS OTHERWISE NOTED.

CATON FARM ROAD

GRAND PRAIRIE
ELEMENTARY SCHOOL



CATON FARM ROAD

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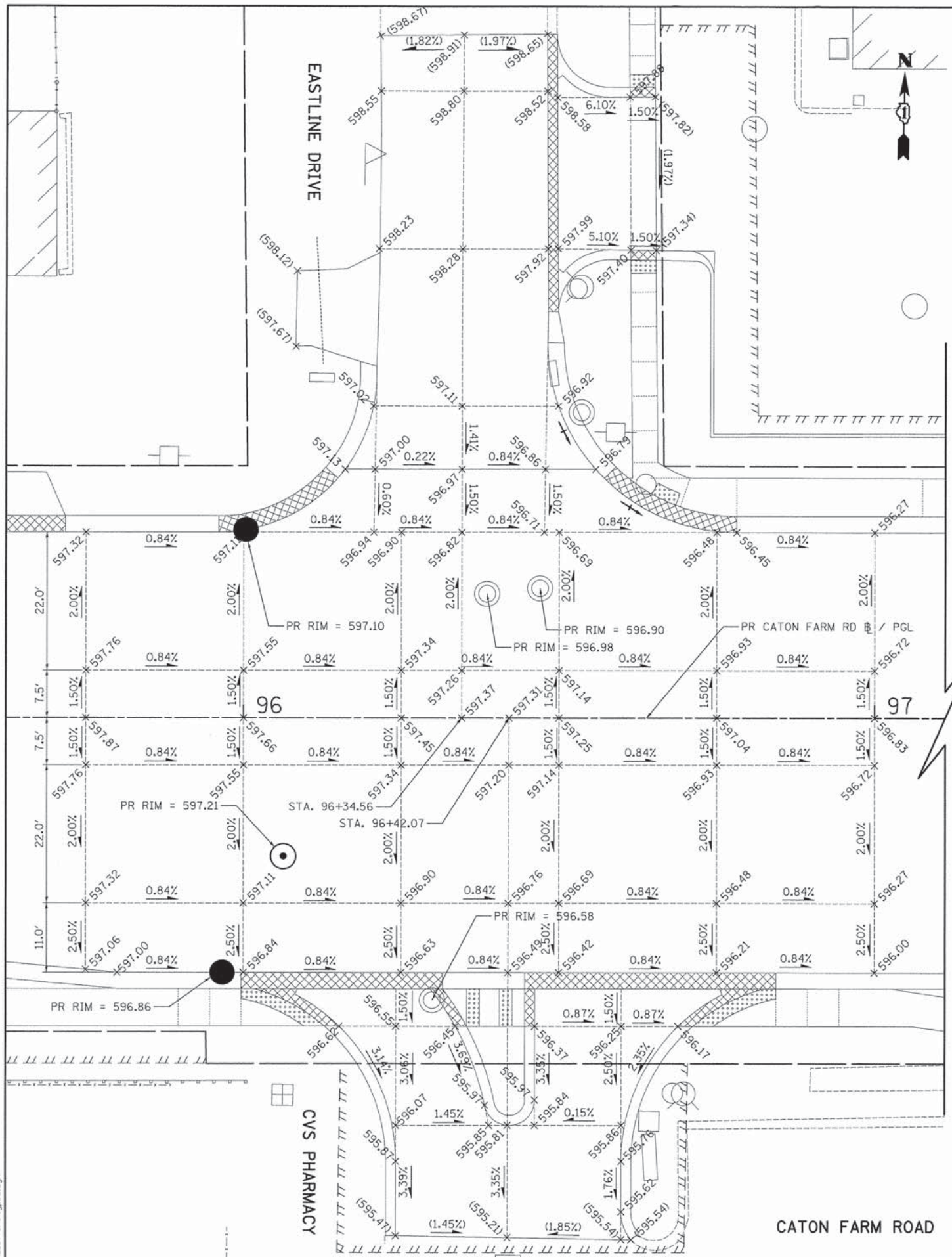


USER NAME = tgb	DESIGNED - EDS	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN - EDS	REVISED -
PLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

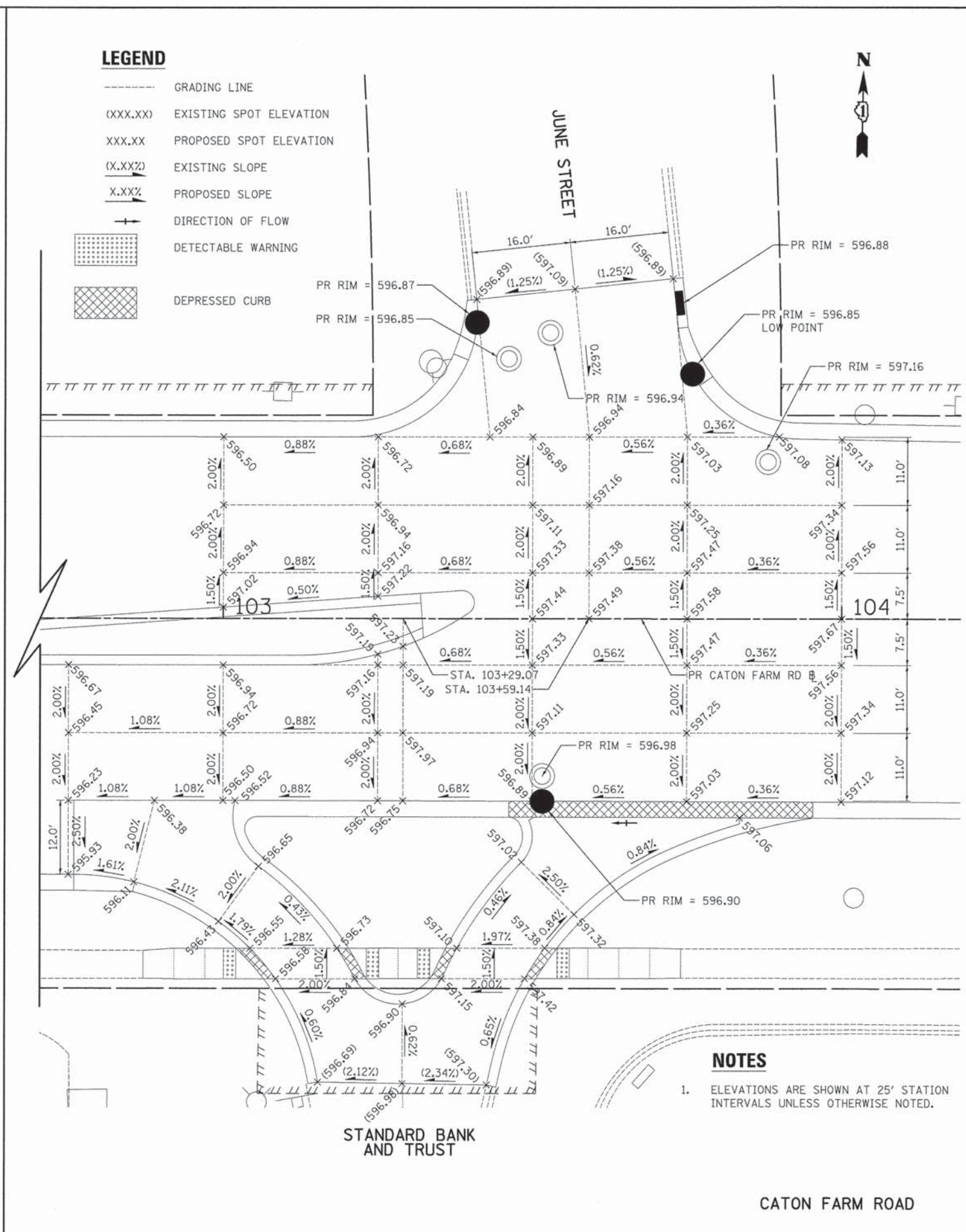
CATON FARM ROAD INTERSECTION GRADING PLAN		
SCALE: 1" = 10'	SHEET 1 OF 4 SHEETS	STA. TO STA.

F.A.U. RTE. 292	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 43
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				



LEGEND

- GRADING LINE
- (XXX.XX) EXISTING SPOT ELEVATION
- XXX.XX PROPOSED SPOT ELEVATION
- (X.XX%) EXISTING SLOPE
- X.XX% PROPOSED SLOPE
- DIRECTION OF FLOW
- [Pattern] DETECTABLE WARNING
- [Pattern] DEPRESSED CURB



- NOTES**
- ELEVATIONS ARE SHOWN AT 25' STATION INTERVALS UNLESS OTHERWISE NOTED.

PLOT DRIVER = ...
 PEN TABLE = ...
 FILE NAME = ...



USER NAME = tgb	DESIGNED - EDS	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN - EDS	REVISED -
PLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

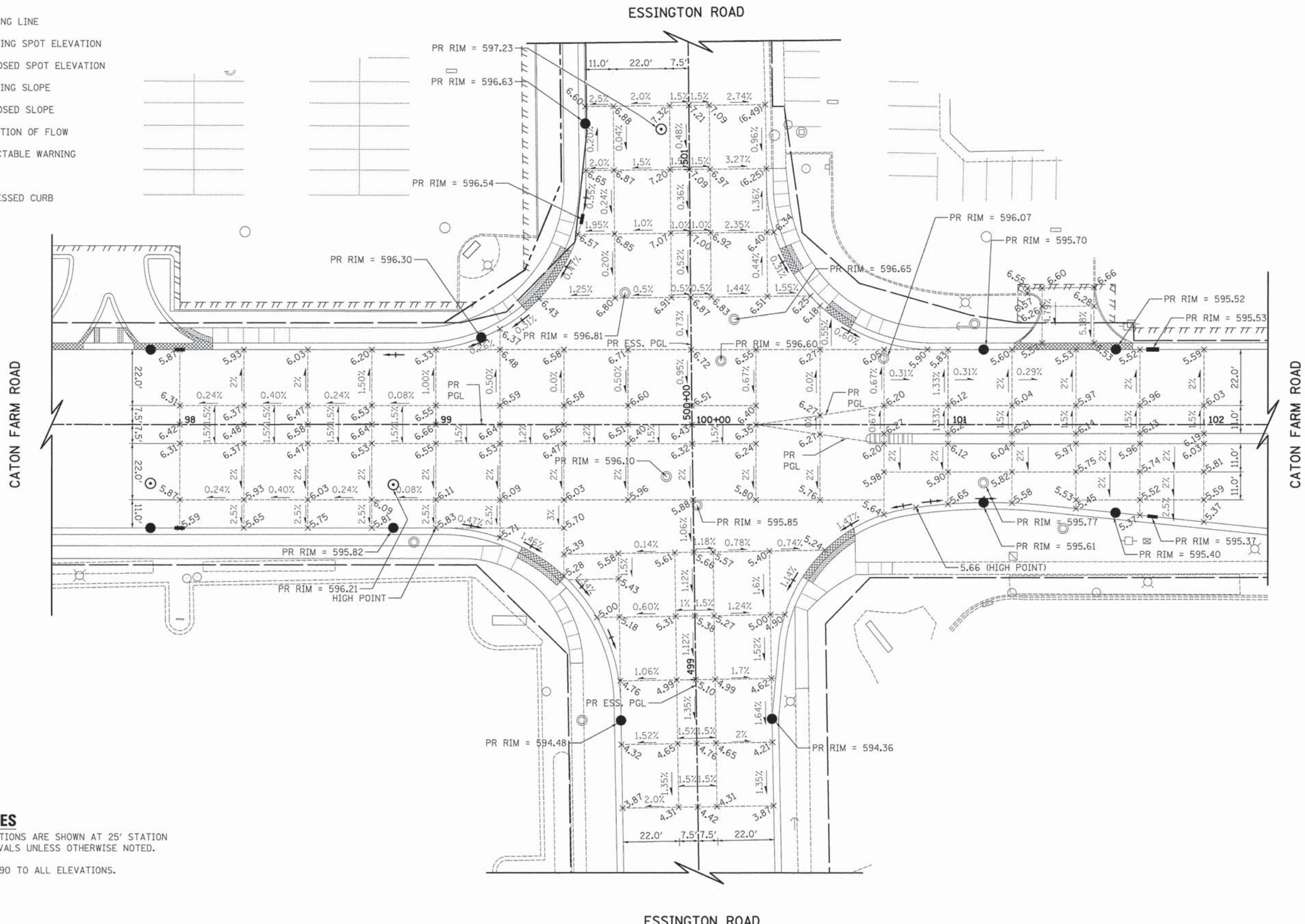
CATON FARM ROAD
INTERSECTION GRADING PLAN

SCALE: 1" = 10' SHEET 2 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	44
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

LEGEND

- GRADING LINE
- (XXX.XX) EXISTING SPOT ELEVATION
- XXX.XX PROPOSED SPOT ELEVATION
- (X.XX%) EXISTING SLOPE
- X.XX% PROPOSED SLOPE
- DIRECTION OF FLOW
- [Stippled Box] DETECTABLE WARNING
- [Cross-hatched Box] DEPRESSED CURB



NOTES

1. ELEVATIONS ARE SHOWN AT 25' STATION INTERVALS UNLESS OTHERWISE NOTED.
2. ADD 590 TO ALL ELEVATIONS.

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USER NAME = tgb	DESIGNED - EDS	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - EDS	REVISED -
PLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CATON FARM ROAD
INTERSECTION GRADING PLAN**

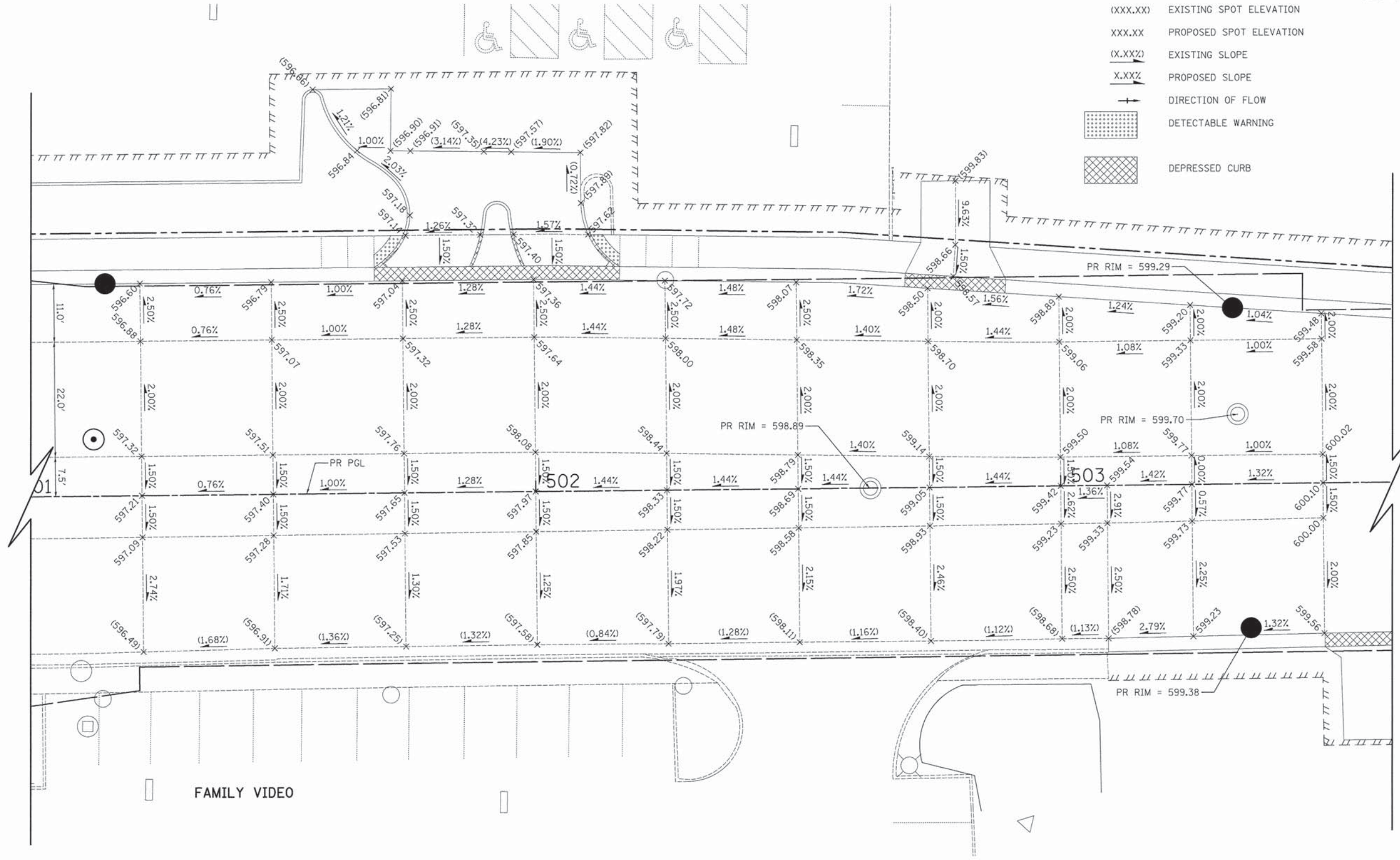
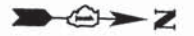
SCALE: 1" = 20' SHEET 3 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	45
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				

CRYSTAL SQUARE SHOPPING CENTER

LEGEND

- GRADING LINE
- (XXX.XX) EXISTING SPOT ELEVATION
- XXX.XX PROPOSED SPOT ELEVATION
- (X.XX%) EXISTING SLOPE
- X.XX% PROPOSED SLOPE
- DIRECTION OF FLOW
- [Grid Pattern] DETECTABLE WARNING
- [Cross-hatch Pattern] DEPRESSED CURB



NOTES

1. ELEVATIONS ARE SHOWN AT 25' STATION INTERVALS UNLESS OTHERWISE NOTED.

FAMILY VIDEO

ESSINGTON DRIVE

PLOT DRIVER = ...\\p101\civ\p101\p101.dwg
 PEN TABLE = ...\\p101\civ\p101\p101.tbl
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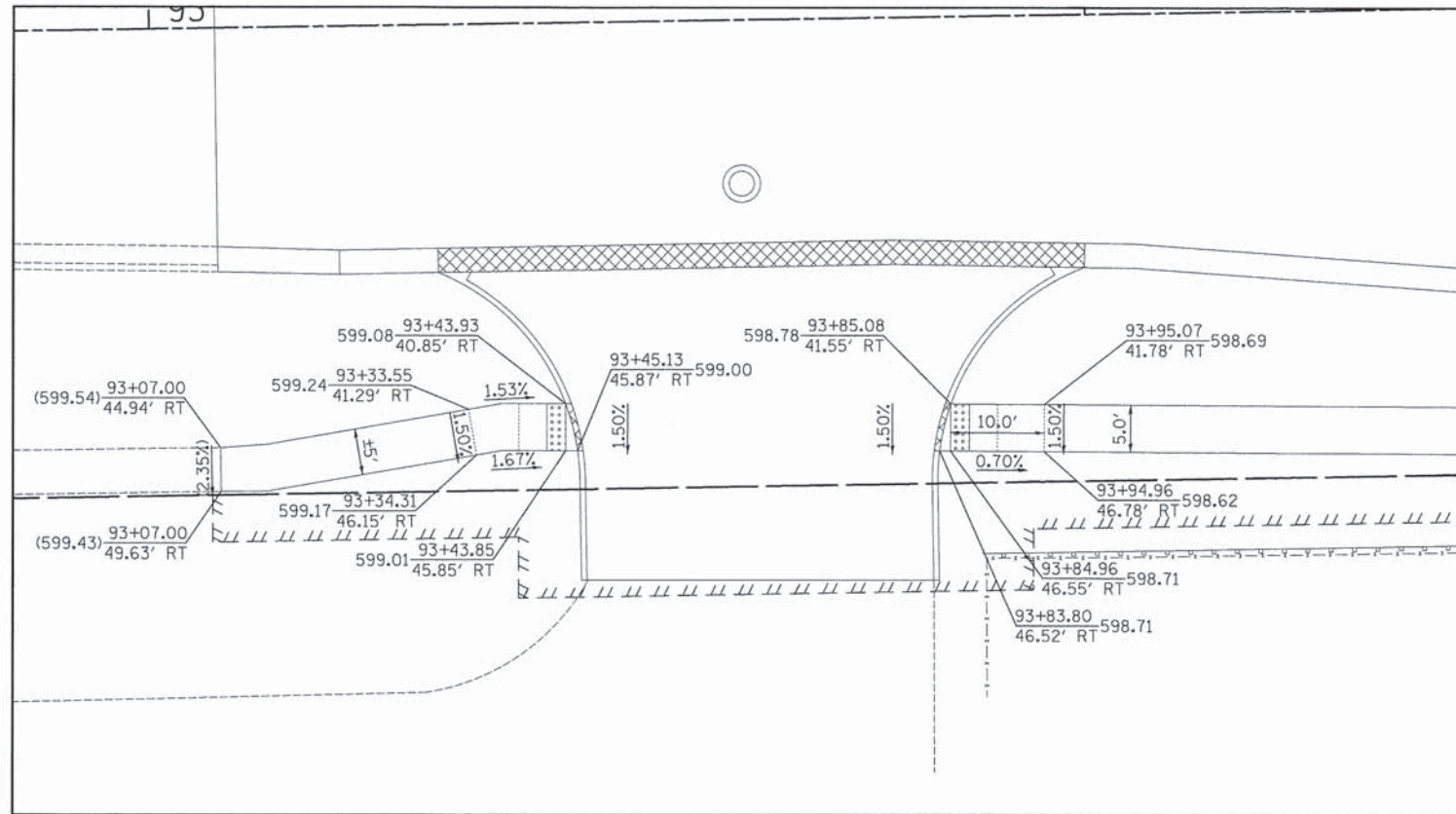


USER NAME = tgb	DESIGNED - EDS	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN - EDS	REVISED -
PLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

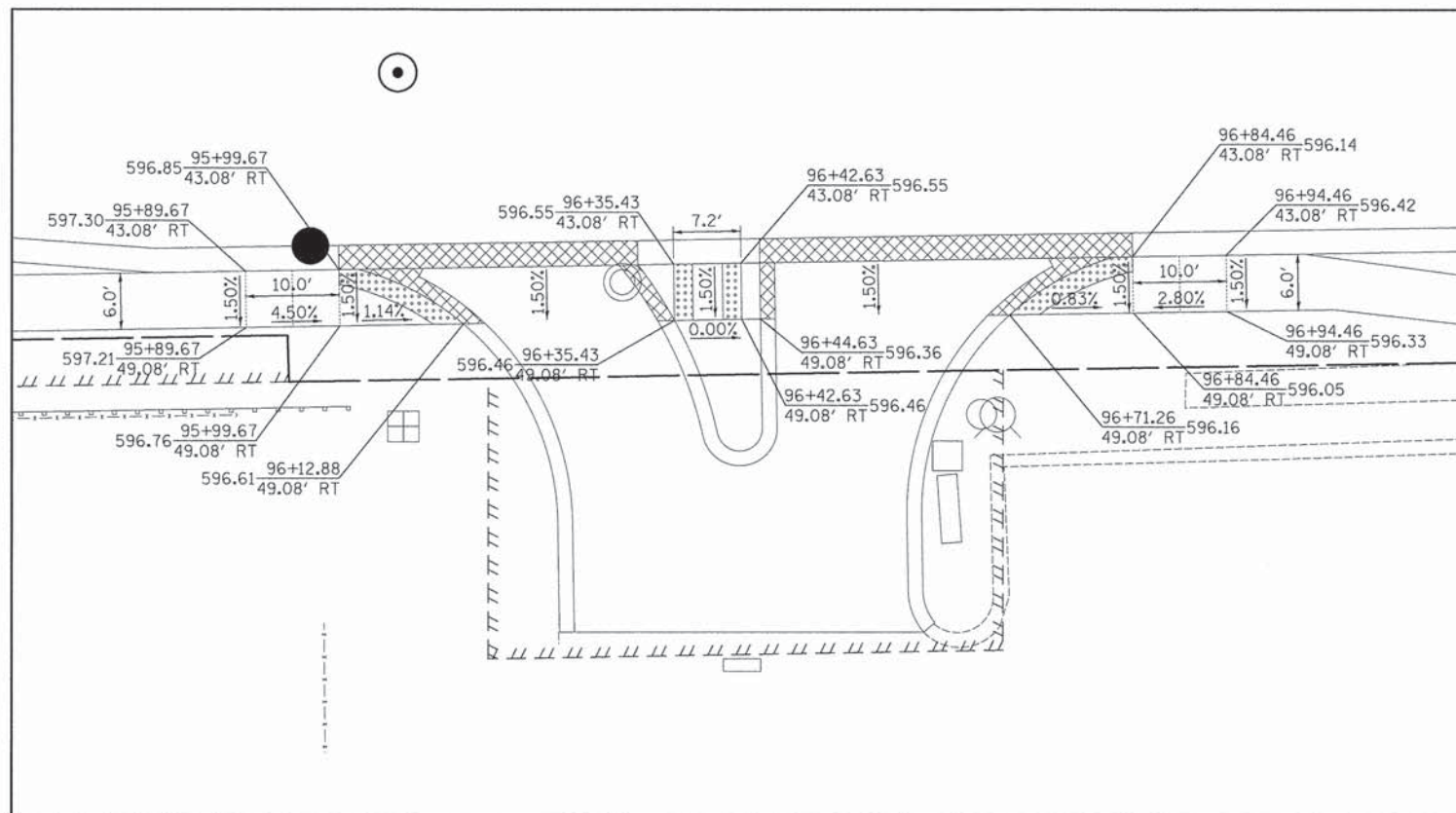
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD
INTERSECTION GRADING PLAN
SCALE: 1" = 10' SHEET 4 OF 4 SHEETS STA. TO STA.

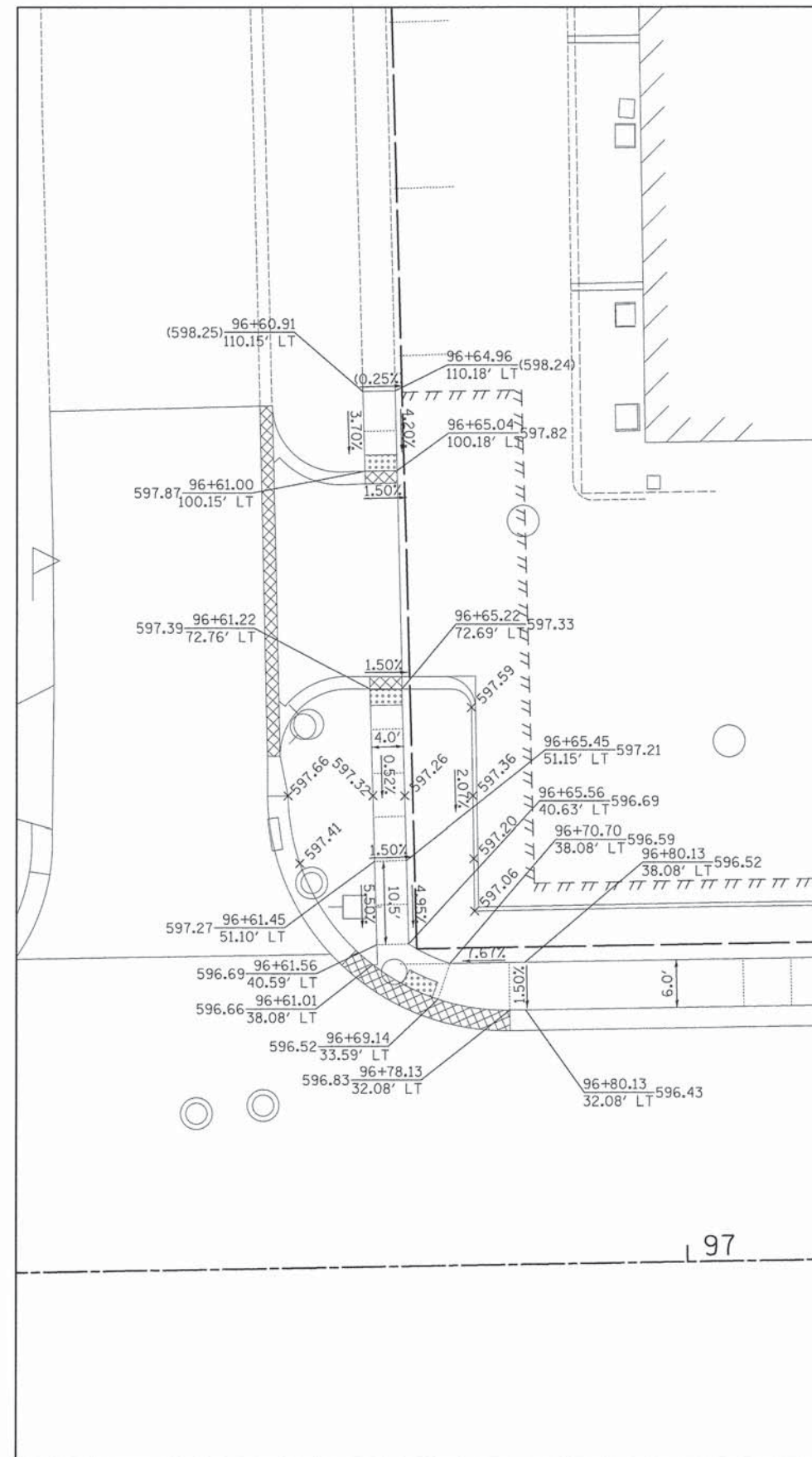
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	46
				CONTRACT NO. 61C29
				ILLINOIS FED. AID PROJECT M-4003 (254)



**DRIVEWAY CROSSING
AT GRAND PRAIRIE ELEMENTARY**



**DRIVEWAY CROSSING
AT CVS PHARMACY**



**NE CATON FARM ROAD AND EASTLINE DRIVE
AND WESTERN DRIVEWAY CROSSING AT CRYSTAL SQUARE SHOPPING CENTER**

LEGEND

- DETECTABLE WARNING
- DEPRESSED CURB
- SIDEWALK JOINT

STATION: XX+XX.XX
 OFFSET: XX.XX' XX
 PROPOSED SIDEWALK ELEVATION: XXX.XX
 LT OR RT

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 PEN TABLE = ...\\pen\tbl\pen.tbl
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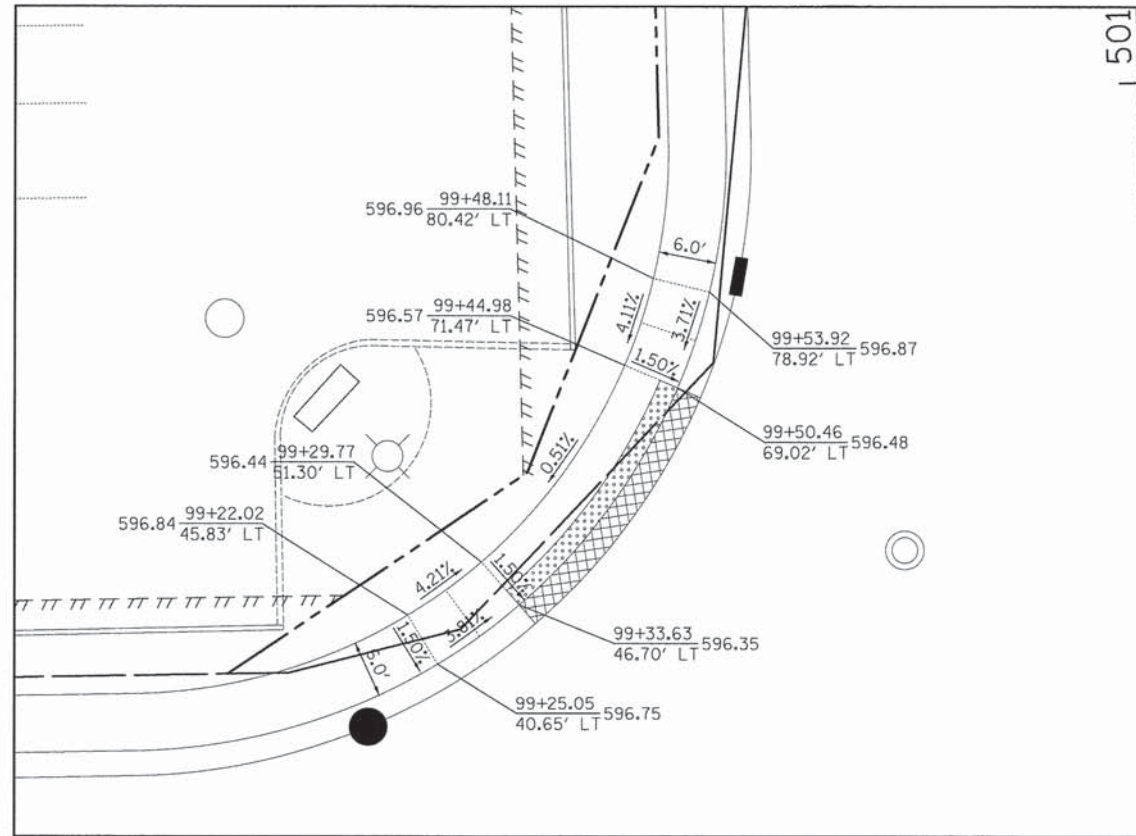


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PLOT SCALE = 10.0000' / 1"	DRAWN - EDS	REVISED -
PLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

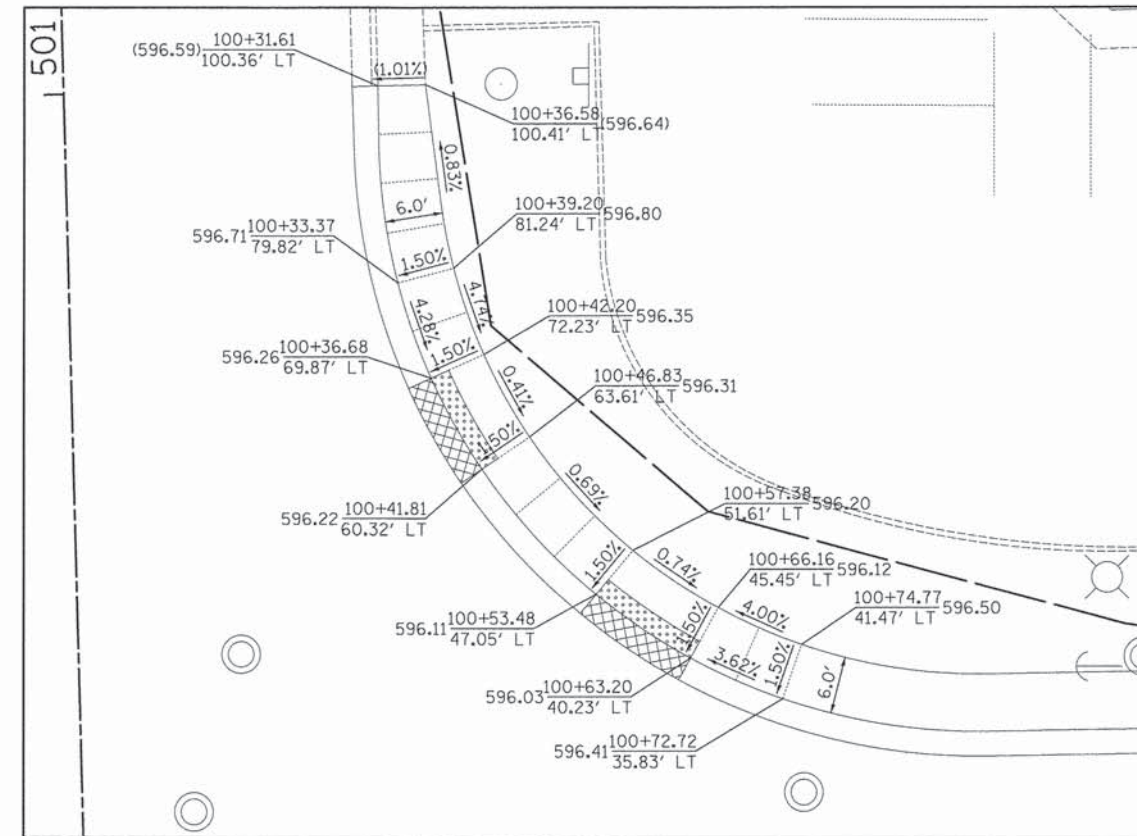
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CATON FARM ROAD CURB RAMP GRADING PLAN	
SCALE: 1" = 10'	SHEET 1 OF 3 SHEETS STA. TO STA.

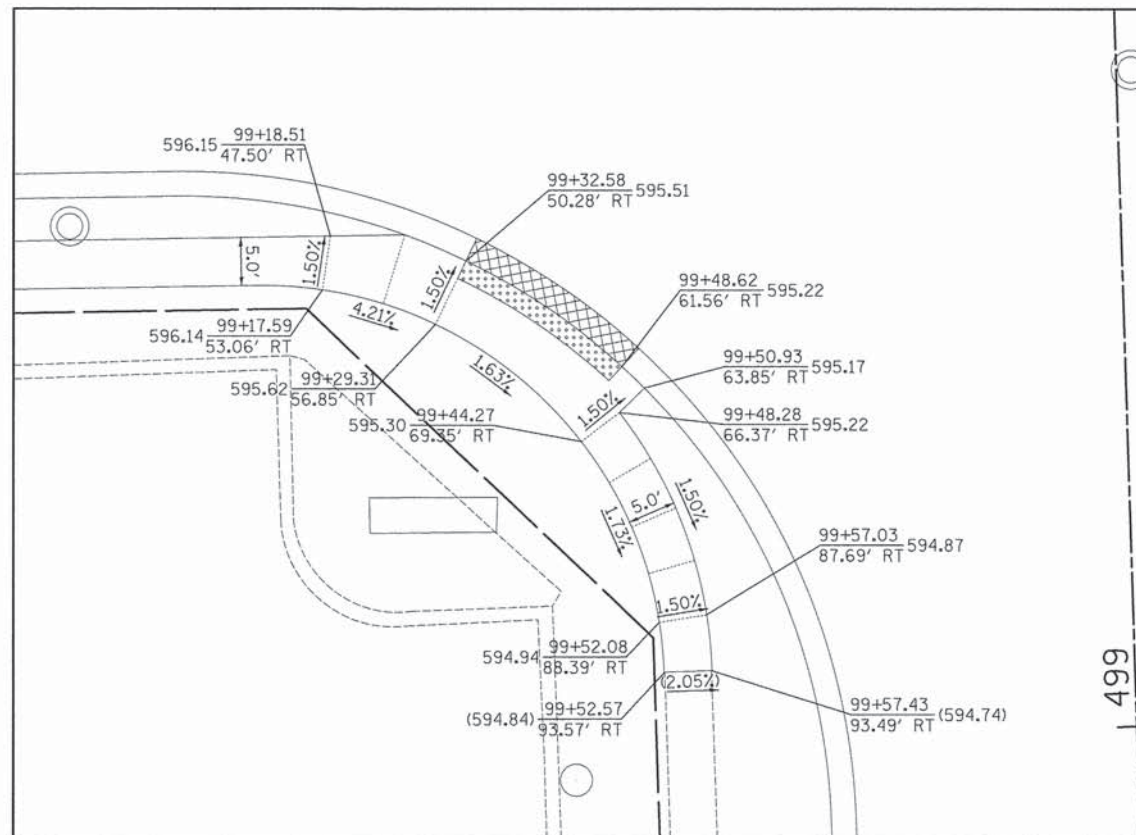
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	47
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				



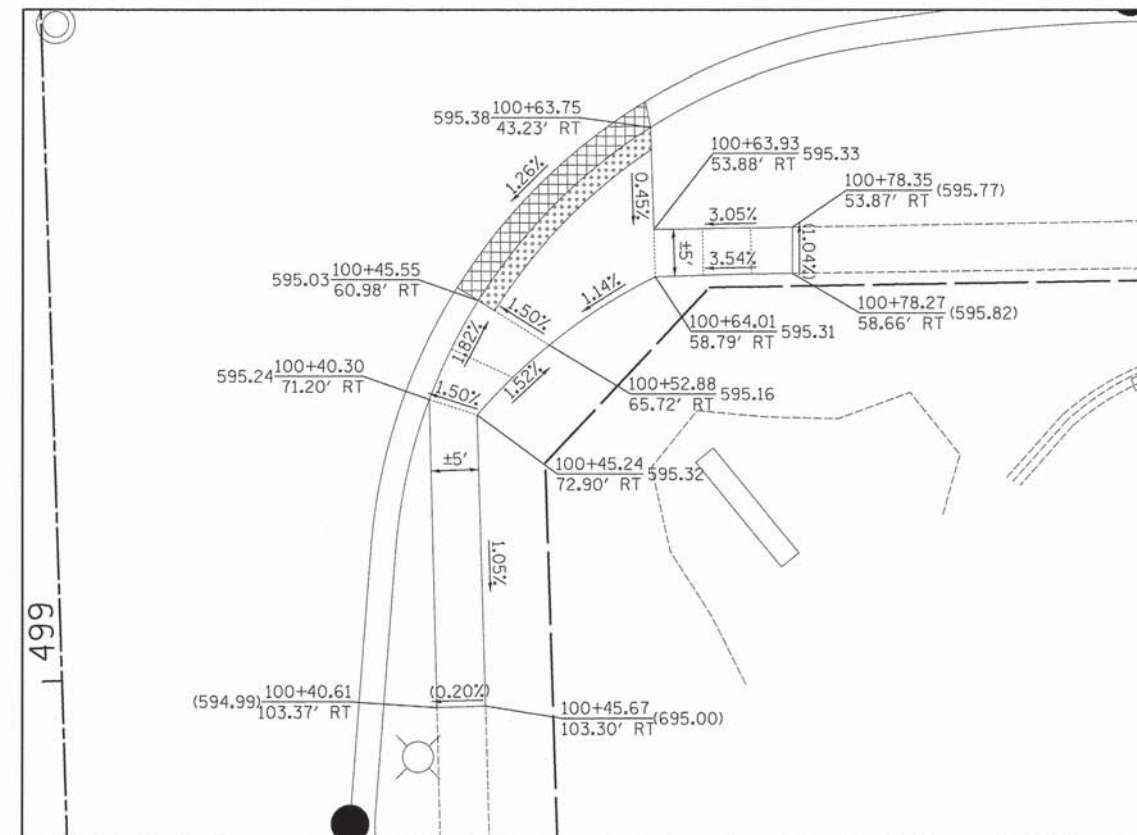
**NW CATON FARM ROAD
& ESSINGTON ROAD**



**NE CATON FARM ROAD
& ESSINGTON ROAD**






**SW CATON FARM ROAD
& ESSINGTON ROAD**



**SE CATON FARM ROAD
& ESSINGTON ROAD**

LEGEND

-  DETECTABLE WARNING
-  DEPRESSED CURB
-  SIDEWALK JOINT
- STATION: $\frac{XX+XX.XX}{XX.XX'}$ XX
- PROPOSED SIDEWALK ELEVATION: XXX.XX
- OFFSET: LT OR RT



PLOT DRIVER = ...\\p015\conf\gen\...
 PEN TABLE = ...\\p015\conf\gen\...
 FILE NAME = ...\\2887-sat-cur-bramp02.dgn



USER NAME = tgb	DESIGNED - TGB	REVISED -
DRAWN - TGB	REVISED -	
CHECKED - JRV	REVISED -	
DATE - 12/07/15	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**




**CATON FARM ROAD
CURB RAMP GRADING PLAN**

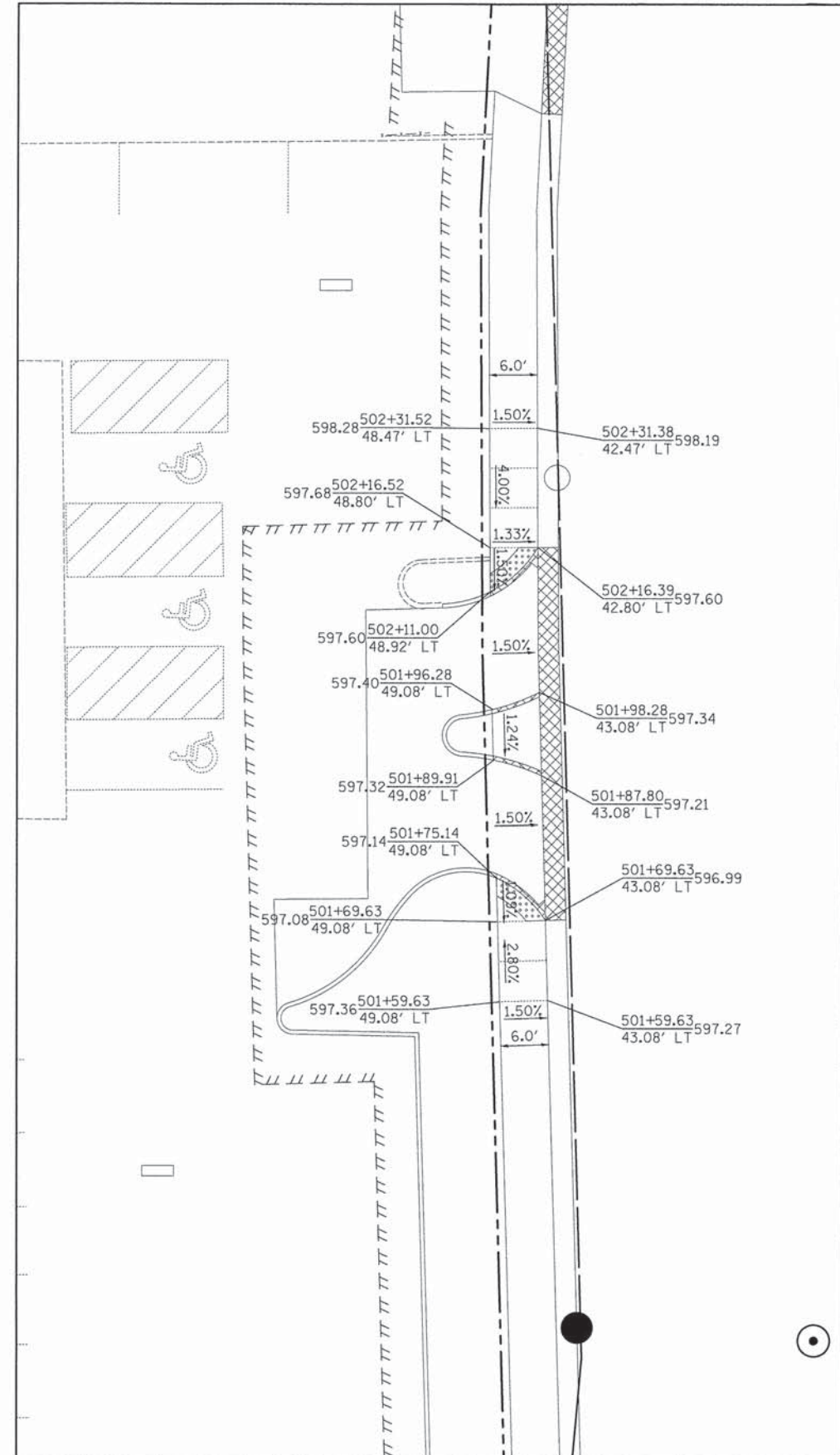
SCALE: 1" = 10' SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	48
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				

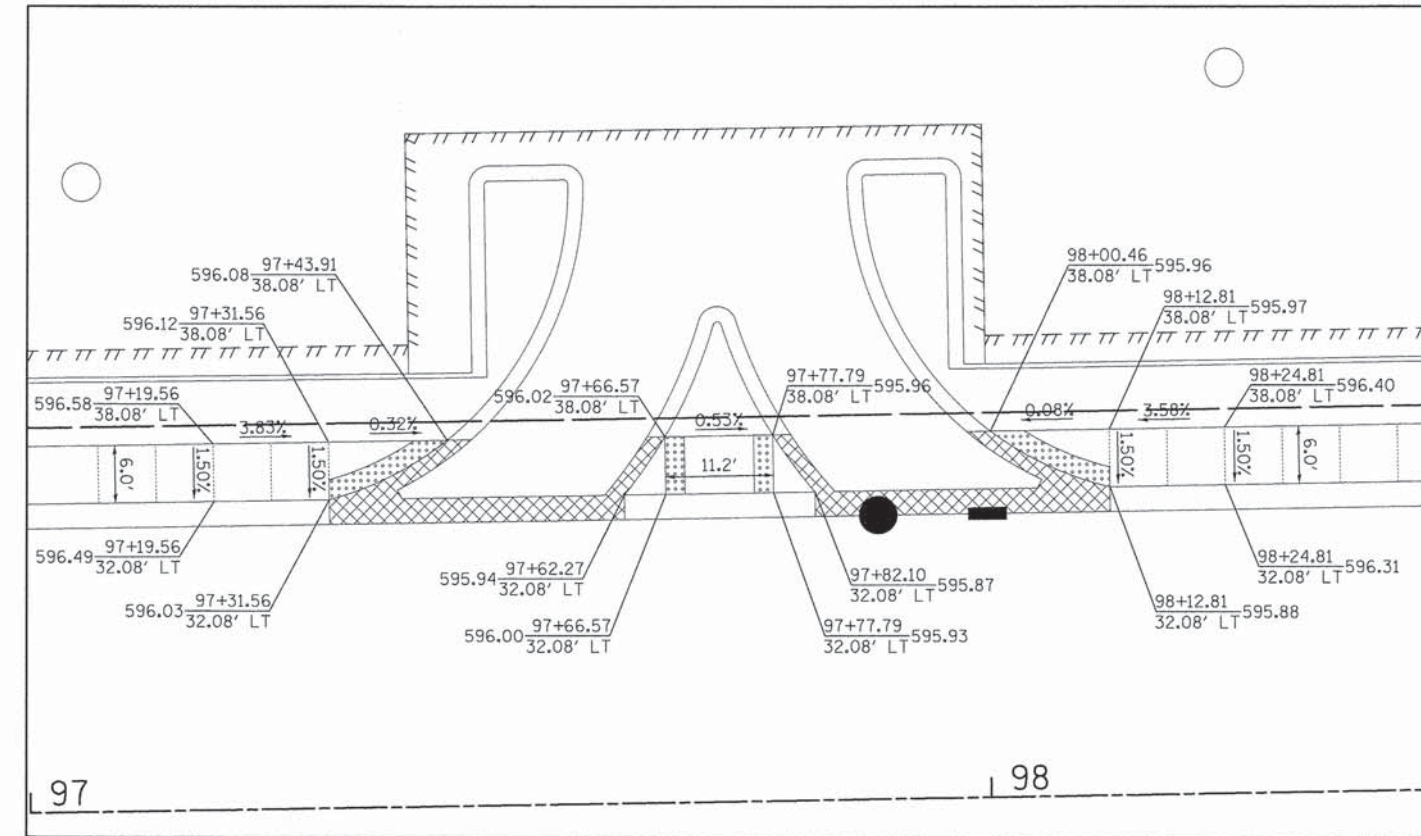


LEGEND

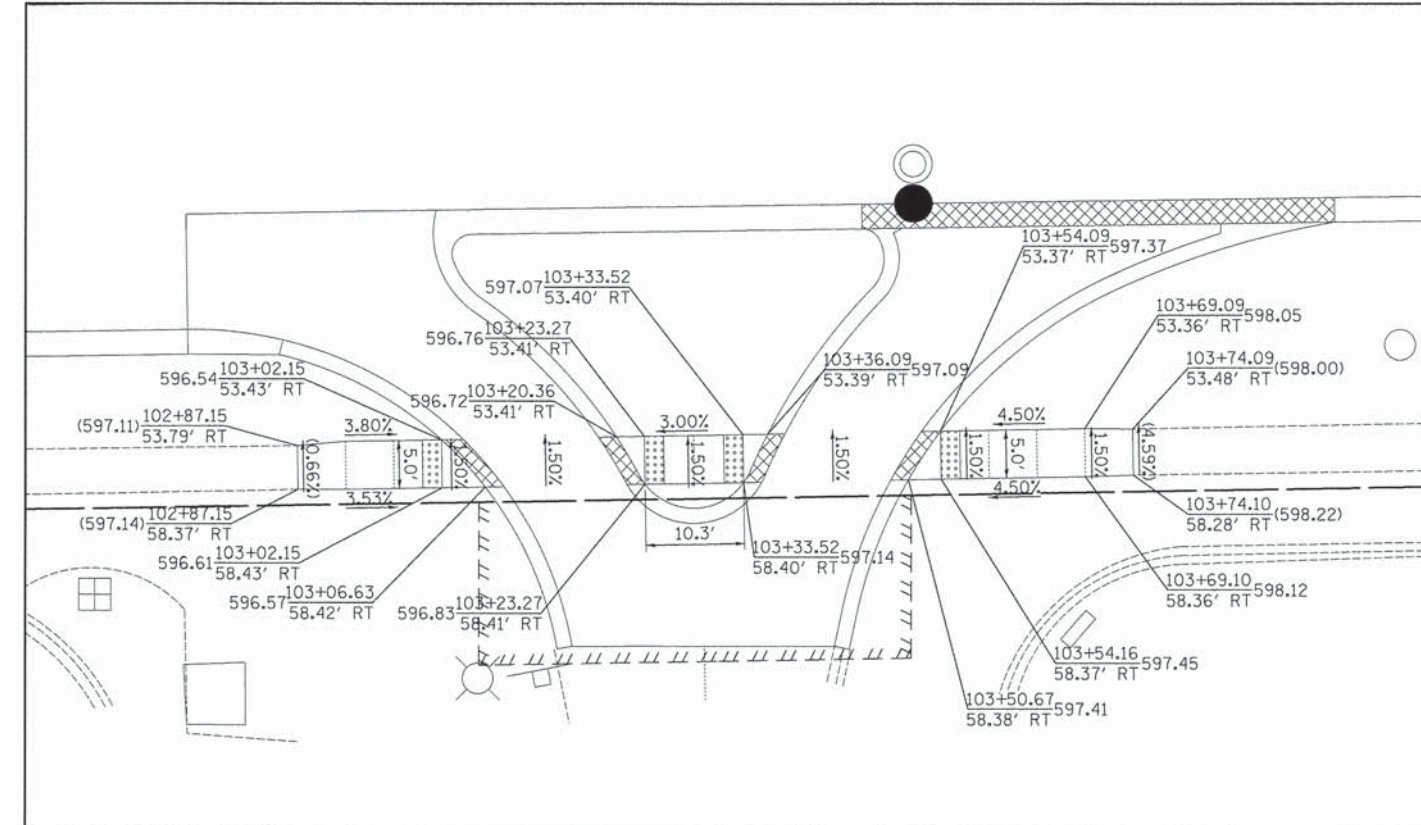
-  DETECTABLE WARNING
-  DEPRESSED CURB
-  SIDEWALK JOINT
- STATION: $XX+XX.XX$
- PROPOSED SIDEWALK ELEVATION: $XXX.XX$
- OFFSET: $XX.XX'$
- LT OR RT



**EASTERN DRIVEWAY CROSSING
AT CRYSTAL SQUARE SHOPPING CENTER**



**SOUTHERN DRIVEWAY CROSSING
AT CRYSTAL SQUARE SHOPPING CENTER**



**DRIVEWAY CROSSING
AT STANDARD BANK AND TRUST**

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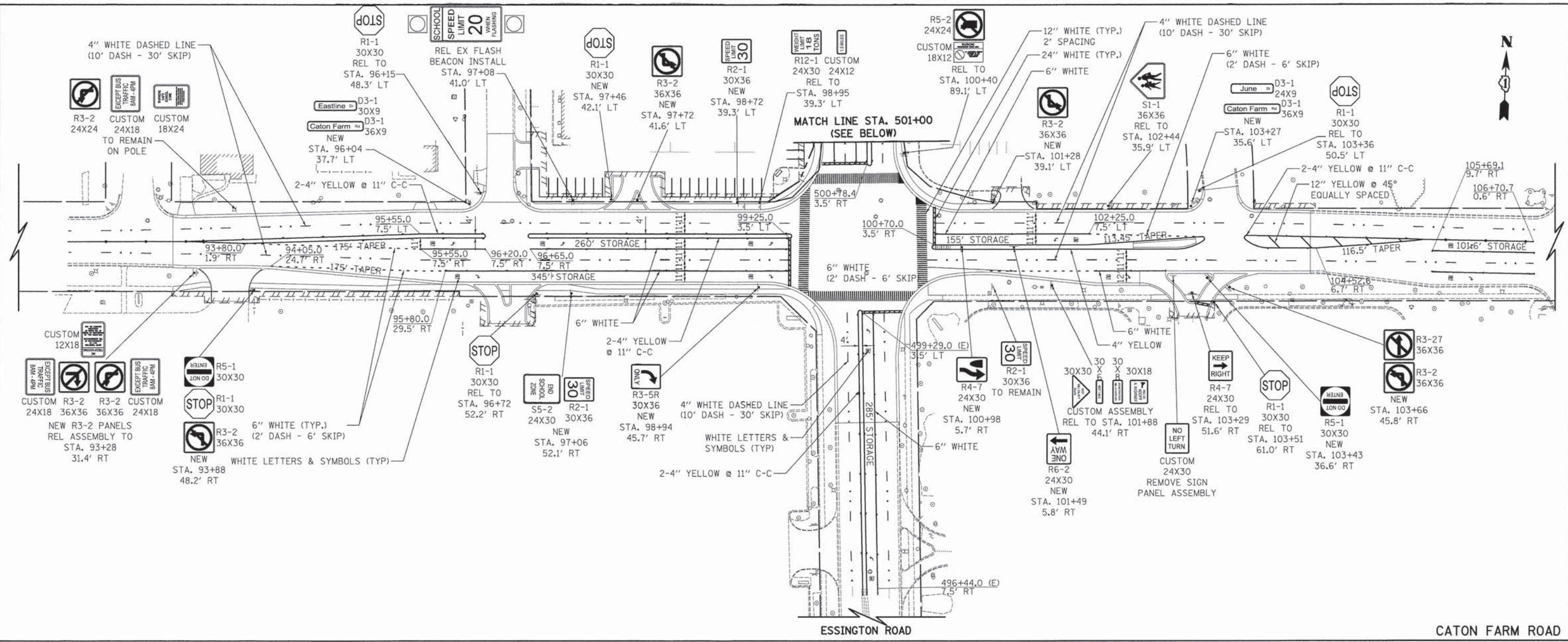
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PLOT DATE = 12/3/2015	DATE - 12/07/15	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

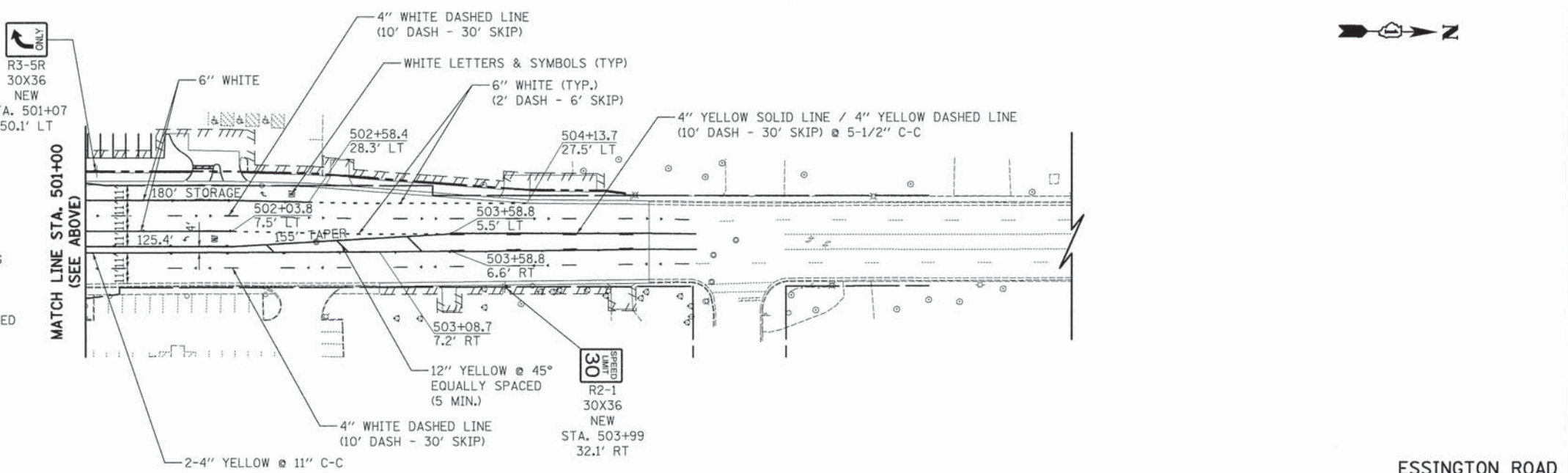
**CATON FARM ROAD
CURB RAMP GRADING PLAN**

SCALE: 1" = 10' SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.U. RTE. 292	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 49
CONTRACT NO. 61C29				ILLINOIS FED. AID PROJECT M-4003 (254)

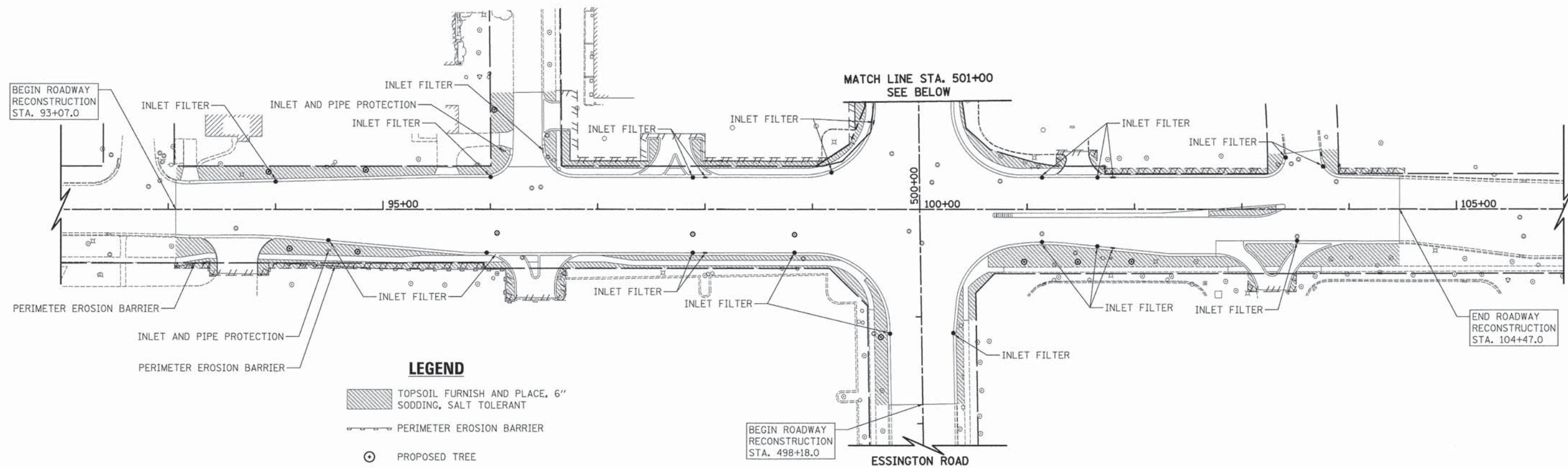


- NOTES:**
- ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
 - DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.
 - SEE I.D.O.T. STANDARD DETAIL TC-13 (SHEET NO. 25) FOR TURN LANE MARKING DETAILS.
 - ALL EXISTING SIGN PANEL ASSEMBLIES, INCLUDING SUPPORTS, SHALL BE REMOVED AND SAFELY STORED BY THE CONTRACTOR. ALL SIGN PANEL ASSEMBLIES SHALL BE RELOCATED PER PLAN OR AS DIRECTED BY ENGINEER OR SHALL BE RETURNED TO THE VILLAGE IF SHOWN FOR REMOVAL. THE COST OF REMOVING, STORING, AND RE-ERECTING THE SIGN PANEL ASSEMBLY AND SUPPORTS SHALL BE PAID FOR AS "RELOCATE SIGN PANEL ASSEMBLY - TYPE A OR TYPE B." THE COST OF REMOVING AND RETURNING SIGN OR SIGN PANEL ASSEMBLY TO THE VILLAGE SHALL BE PAID FOR AS "REMOVE SIGN PANEL - TYPE 1" OR "REMOVE SIGN PANEL ASSEMBLY - TYPE A OR TYPE B."
 - ALL RAISED REFLECTIVE PAVEMENT MARKERS USED WITH SKIP DASHES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS, AND OFFSET 4 INCHES FROM THE CENTERLINE OF THE DASH TO MATCH THE EXISTING MARKERS ON THE COUNTY HIGHWAY.



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	USER NAME = tgb	DESIGNED - TGB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CATON FARM ROAD AND ESSINGTON ROAD SIGNING AND PAVEMENT MARKING PLAN	F.A.U. RTE. = 292	SECTION = 11-00446-00-CH	COUNTY = WILL	TOTAL SHEETS = 110	SHEET NO. = 50	
	PLOT SCALE = 50,0000' = 1"	DRAWN - TGB	REVISED -			SCALE: 1" = 50'	SHEET 1 OF 1 SHEETS	STA. TO STA.	CONTRACT NO. 61C29		ILLINOIS FED. AID PROJECT M-4003 (254)
	PLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -			DATE = 12/07/15			DATE = 12/07/15		



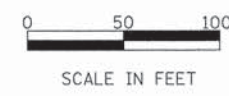
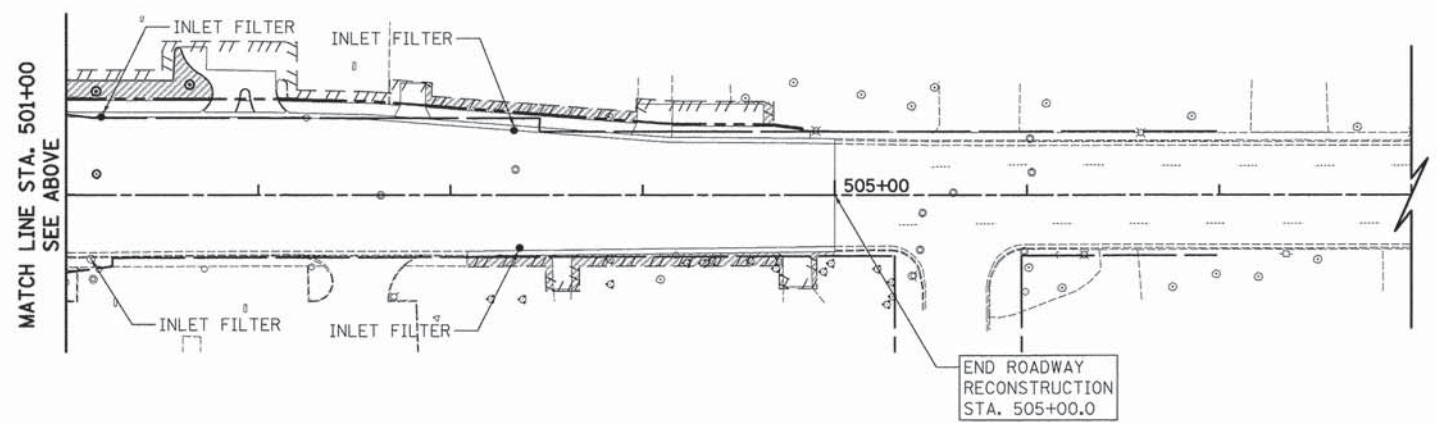
LEGEND

- TOPSOIL FURNISH AND PLACE, 6" SODDING, SALT TOLERANT
- PERIMETER EROSION BARRIER
- PROPOSED TREE

CATON FARM ROAD

NOTE:

1. ALL EROSION CONTROL MEASURES ARE TO BE IN PLACE BEFORE ANY WORK SHALL BEGIN.
2. PLACE INLET FILTER ON FIRST OPEN STRUCTURE TO THE SOUTH OF PROJECT LIMITS ON ESSINGTON ROAD.
3. CONTRACTOR SHALL PROVIDE SPADE EDGES FOR ALL SODDING AREA ABUTTING EXISTING TREES BY MAINTAINING A MINIMUM 5' DIAMETER MULCH BED AROUND EACH EXISTING TREE.
4. THE EXACT LOCATION OF PROPOSED TREES SHALL BE DETERMINED BY THE ENGINEER.



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PLOT SCALE = 50.0000' / 1"	DRAWN - TGB	REVISED -
PLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CATON FARM ROAD AND ESSINGTON ROAD
EROSION CONTROL AND LANDSCAPING PLAN**

SCALE: 1" = 50' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	51
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				

ESSINGTON ROAD

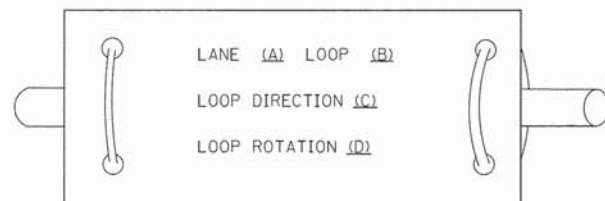
TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED																	
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE																				
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE																				
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA																				
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED																				
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F																				
UNINTERRUPTABLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F																				
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				UNDERGROUND CONDUIT, GALVANIZED STEEL (UC)				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F																				
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F																				
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F																				
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE																				
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED																				
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM				STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED																				
SIGNAL POST				REMOVE ITEM				ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED																				
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM				STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED																				
GUY WIRE				ABANDON ITEM				SIGNAL POST AND FOUNDATION TO BE REMOVED																				
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				INTERSECTION & SAMPLING (SYSTEM) DETECTOR																				
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				SAMPLING (SYSTEM) DETECTOR																				
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				QUEUE DETECTOR																				
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD				PREFORMED QUEUE DETECTOR																				
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				"RB" INDICATES REFLECTIVE BACKPLATE				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR																				
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				PREFORMED SAMPLING (SYSTEM) DETECTOR																				
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED				<h2 style="margin: 0;">RAILROAD SYMBOLS</h2> <table style="width: 100%; border: none;"> <thead> <tr> <th style="width: 50%;"></th> <th style="width: 25%;">EXISTING</th> <th style="width: 25%;">PROPOSED</th> </tr> </thead> <tbody> <tr> <td>RAILROAD CONTROL CABINET</td> <td></td> <td></td> </tr> <tr> <td>RAILROAD CANTILEVER MAST ARM</td> <td></td> <td></td> </tr> <tr> <td>FLASHING SIGNAL</td> <td></td> <td></td> </tr> <tr> <td>CROSSING GATE</td> <td></td> <td></td> </tr> <tr> <td>CROSSBUCK</td> <td></td> <td></td> </tr> </tbody> </table>				EXISTING	PROPOSED	RAILROAD CONTROL CABINET			RAILROAD CANTILEVER MAST ARM			FLASHING SIGNAL			CROSSING GATE			CROSSBUCK		
	EXISTING	PROPOSED																										
RAILROAD CONTROL CABINET																												
RAILROAD CANTILEVER MAST ARM																												
FLASHING SIGNAL																												
CROSSING GATE																												
CROSSBUCK																												
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID																								
ILLUMINATED SIGN "NO LEFT TURN"				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER																								
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO INTERCONNECT																								
DETECTOR LOOP, TYPE I				RADIO REPEATER																								
PREFORMED DETECTOR LOOP				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED																								
MICROWAVE VEHICLE SENSOR				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)																								
VIDEO DETECTION CAMERA																												
VIDEO DETECTION ZONE																												
PAN, TILT, ZOOM CAMERA																												
WIRELESS DETECTOR SENSOR																												
WIRELESS ACCESS POINT																												

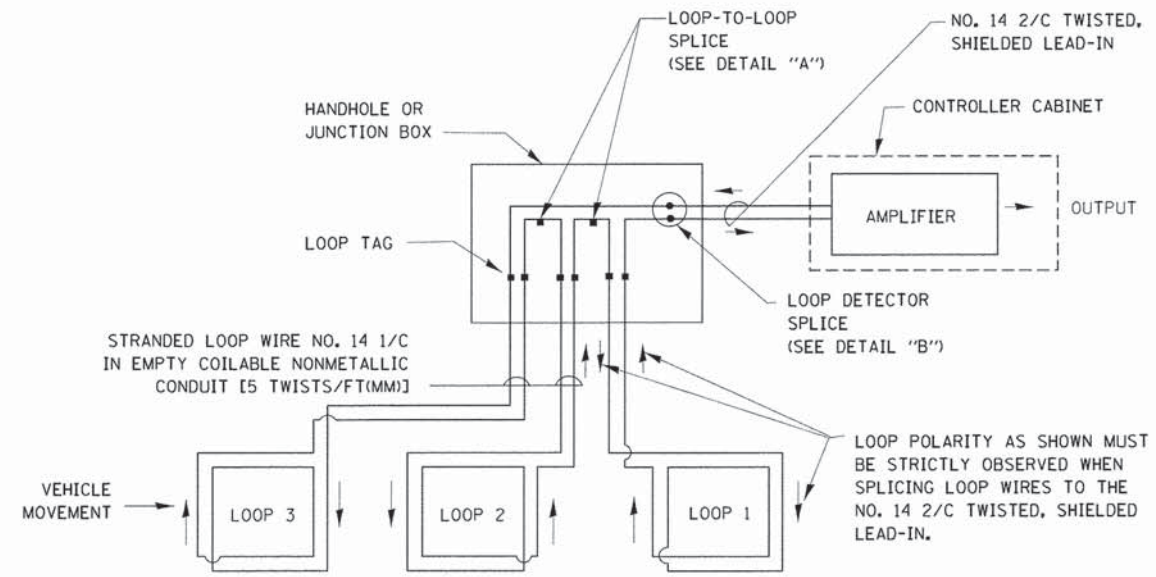
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

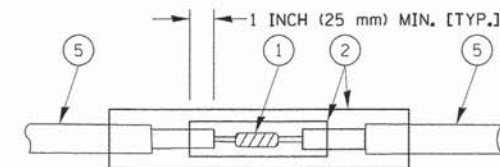


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

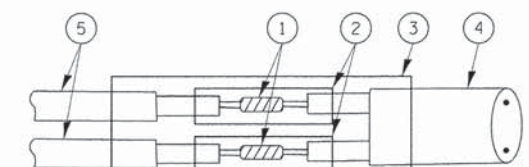


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

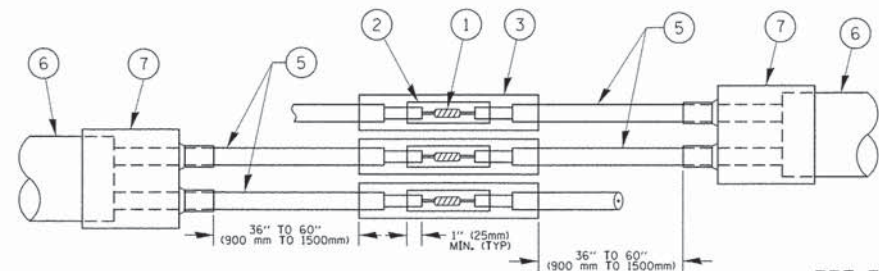


DETAIL "A"
LOOP-TO-LOOP SPLICE

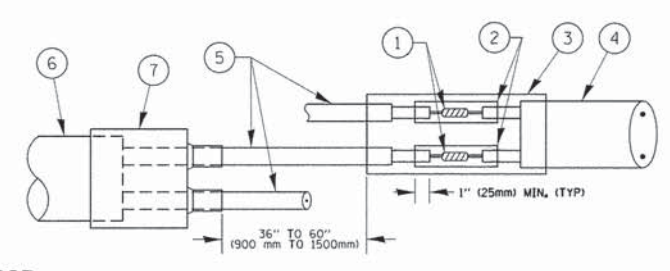


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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et\pw\work\p\dot\Footemj\d0188315\ts05.dgn		DRAWN - BCK	REVISED -
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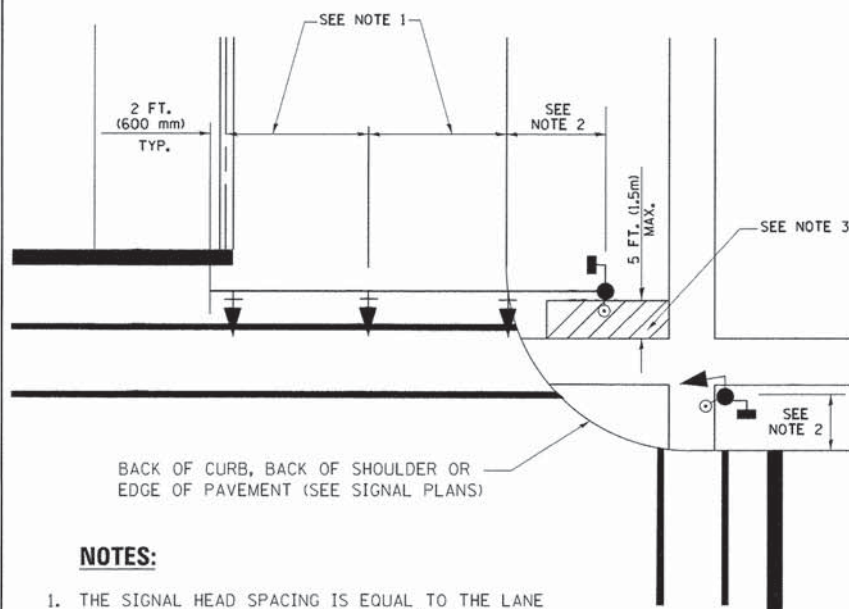
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET NO. 2 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	53
TS-05		CONTRACT NO. 61C29		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-4003 (254)				

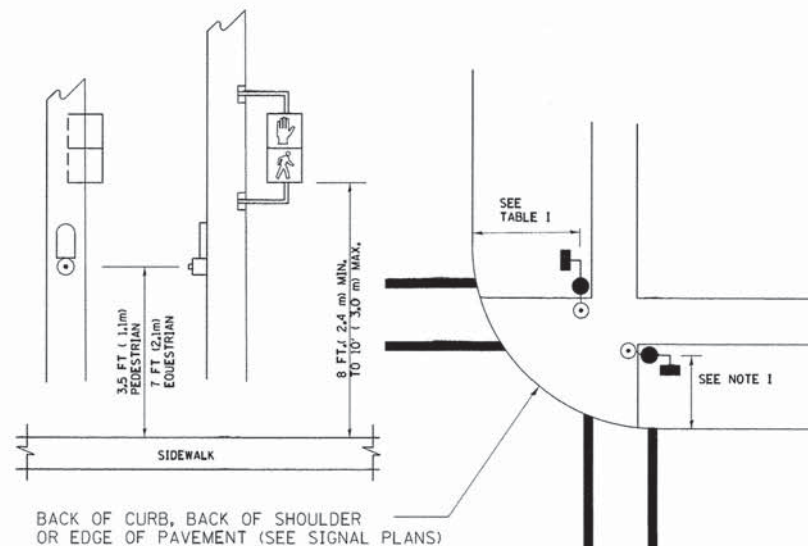
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.**



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

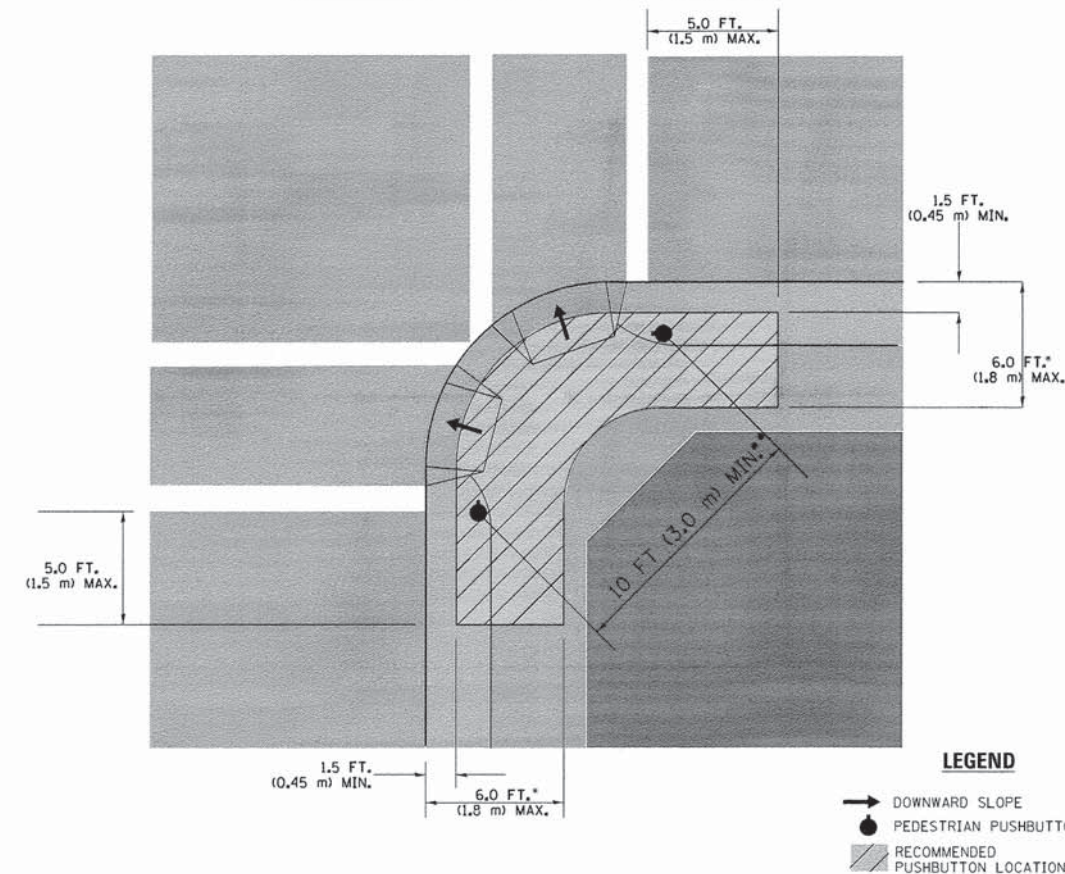
**PEDESTRIAN SIGNAL POST
AND
PEDESTRIAN PUSH BUTTON POST**



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

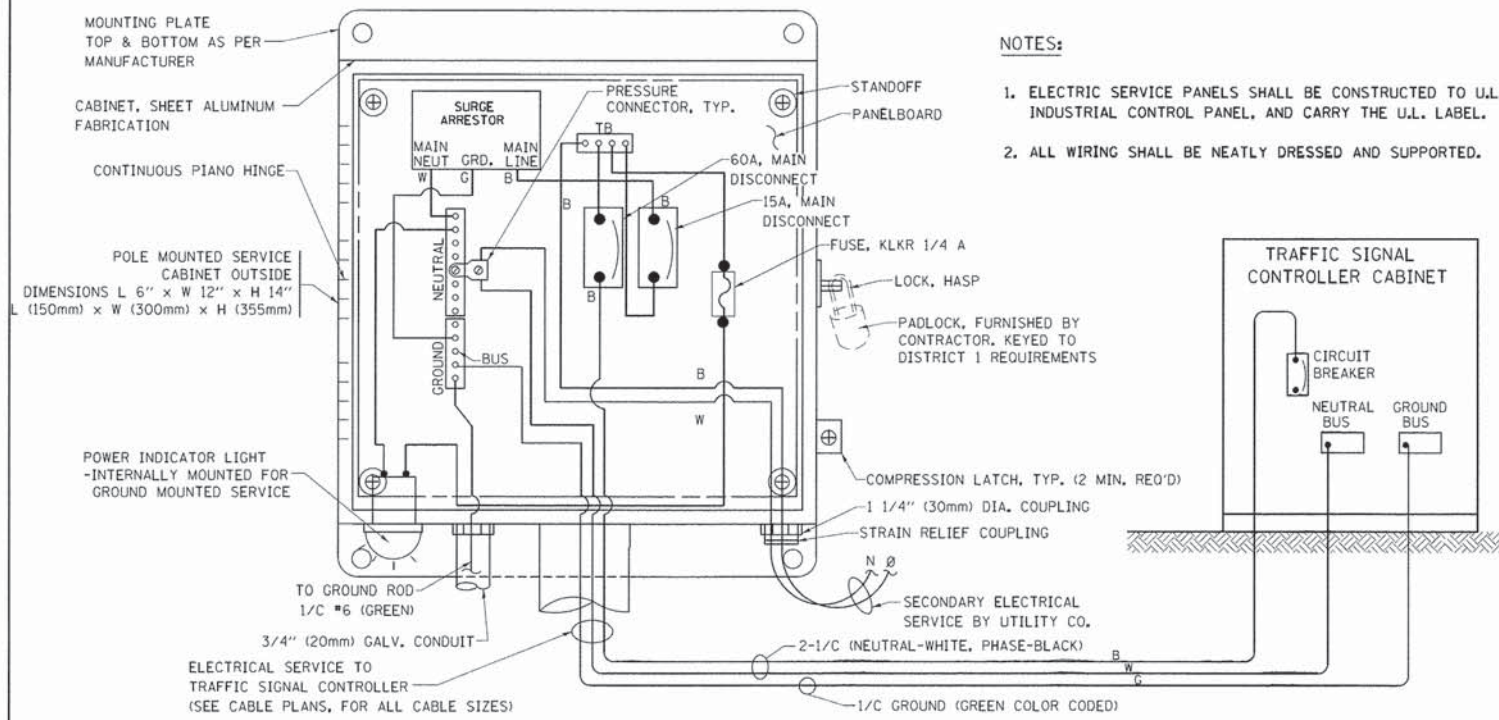
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

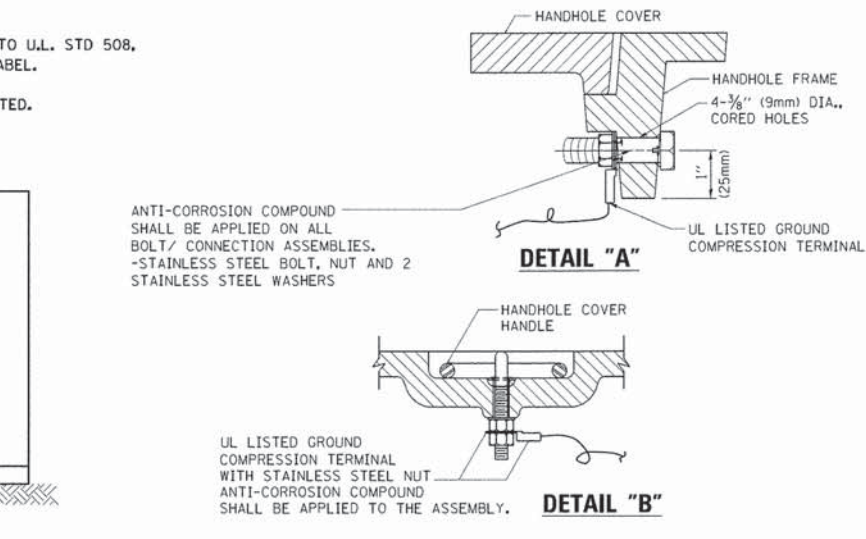
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

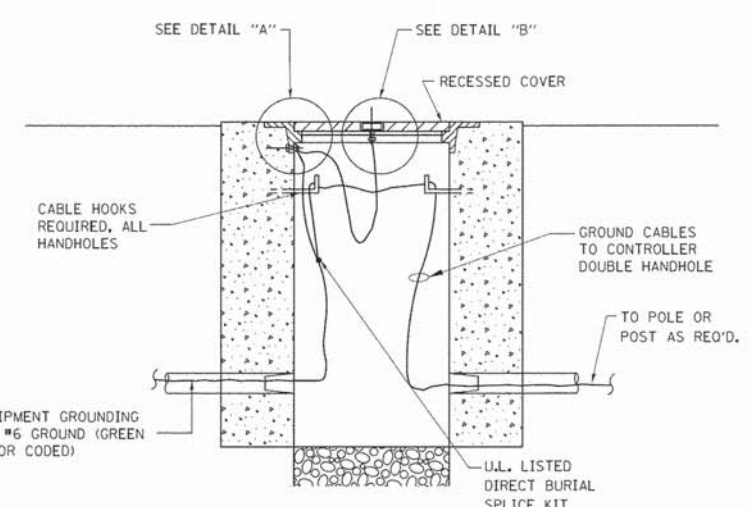


**ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)**

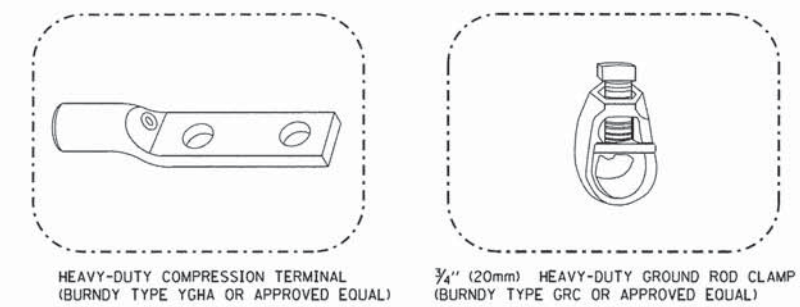


**NOTES:
GROUNDING SYSTEM**

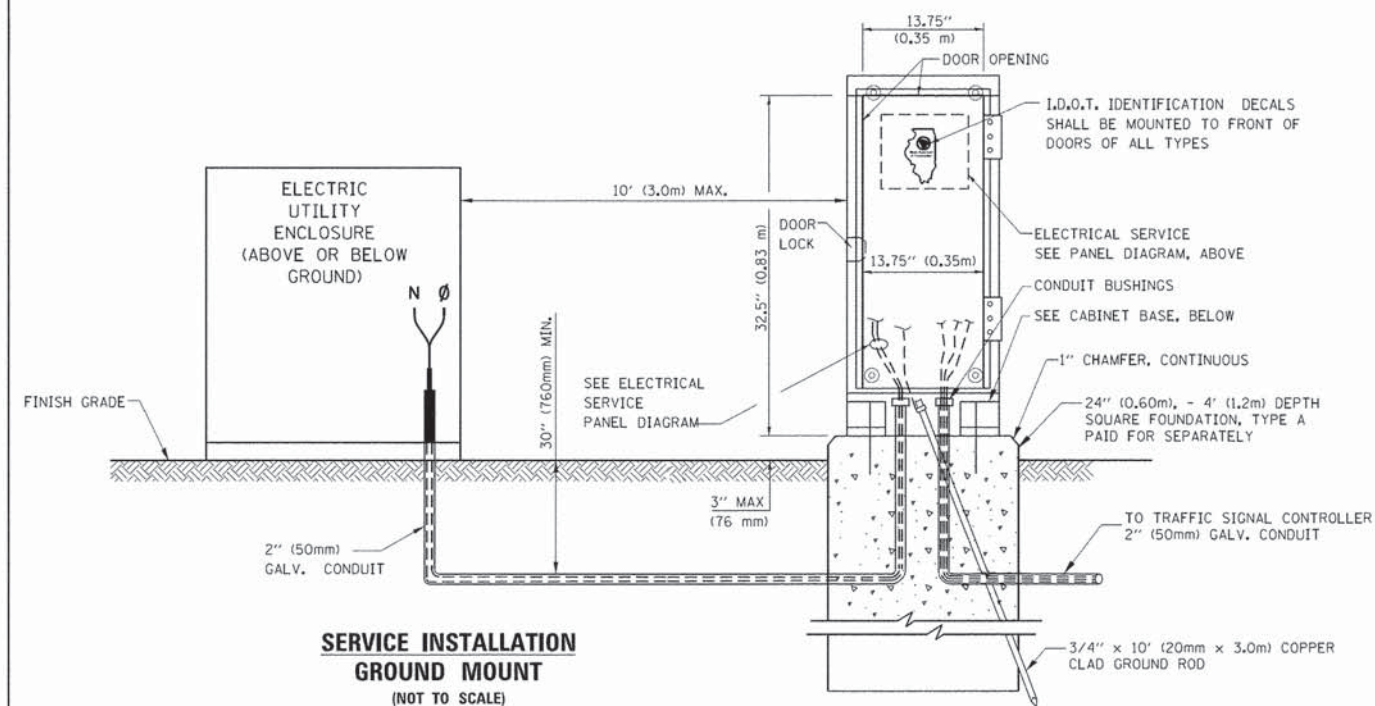
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.), GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



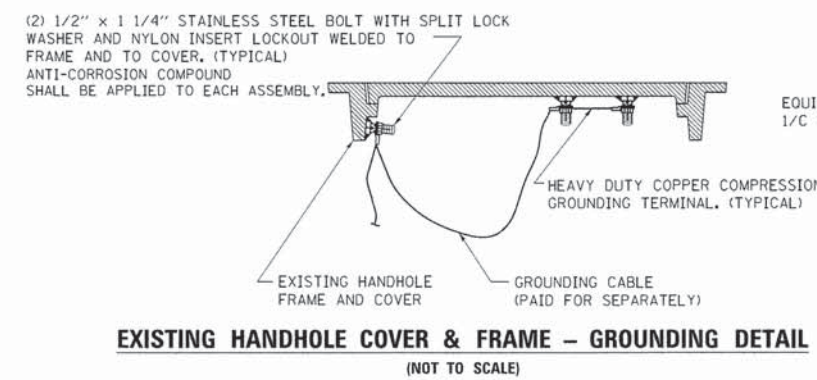
**HANDHOLE COVER & FRAME – GROUNDING DETAIL
(NOT TO SCALE)**



- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

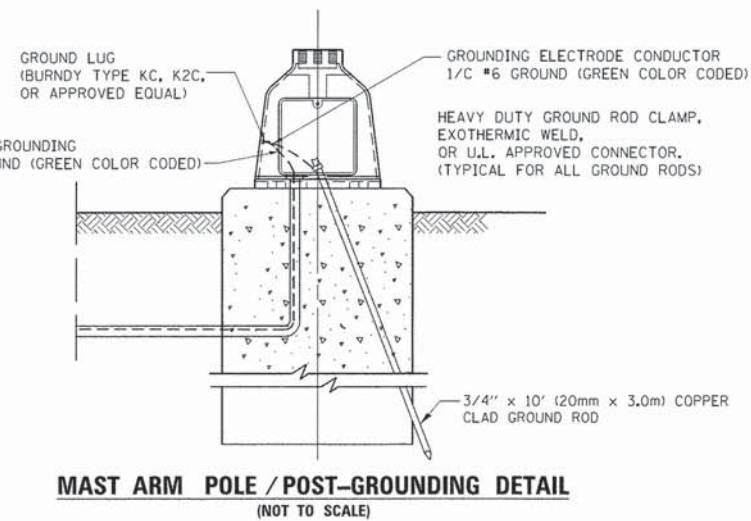
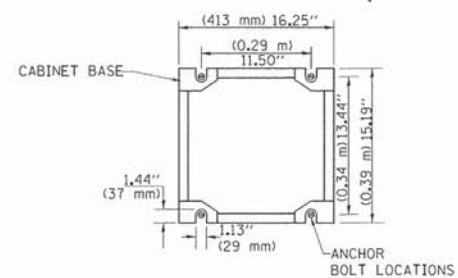


**SERVICE INSTALLATION GROUND MOUNT
(NOT TO SCALE)**



**EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL
(NOT TO SCALE)**

**CABINET – BASE BOLT PATTERN
(NOT TO SCALE)**



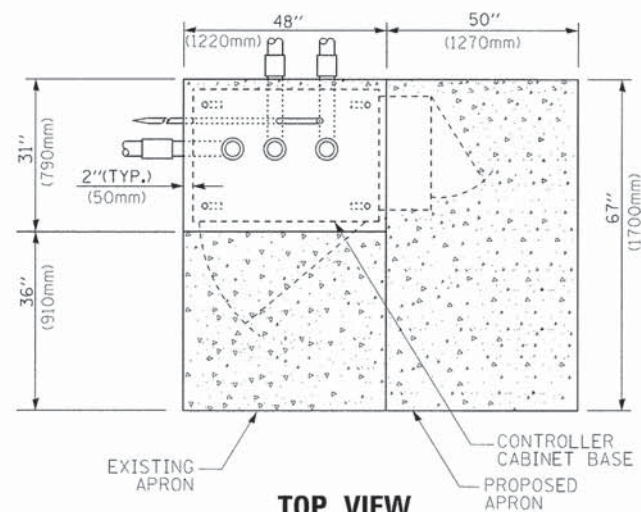
**MAST ARM POLE / POST-GROUNDING DETAIL
(NOT TO SCALE)**

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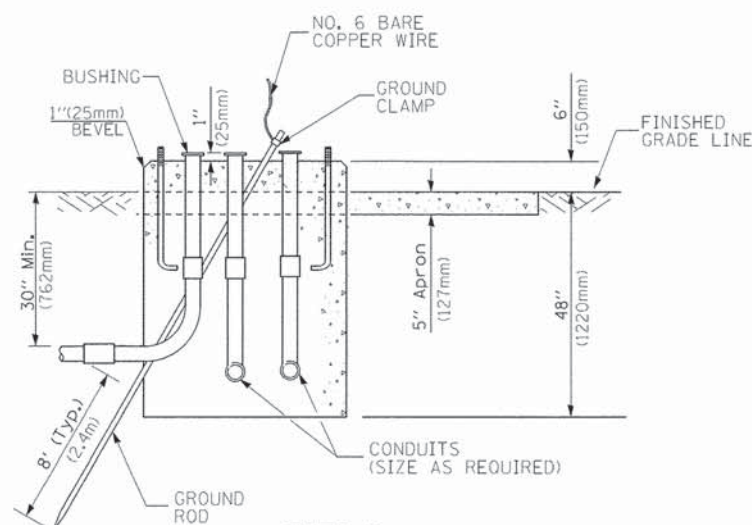
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
STANDARD TRAFFIC SIGNAL DESIGN DETAILS			
SCALE: NONE	SHEET NO. 4 OF 7 SHEETS	STA.	TO STA.

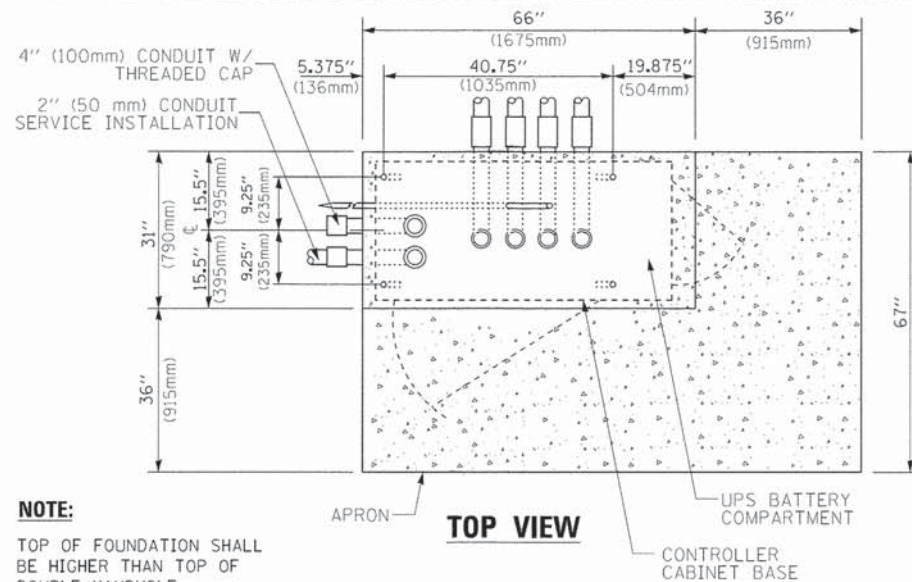
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292	11-00446-00-CH	WILL	110	55
TS-05			CONTRACT NO. 61C29	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (254)				



TOP VIEW



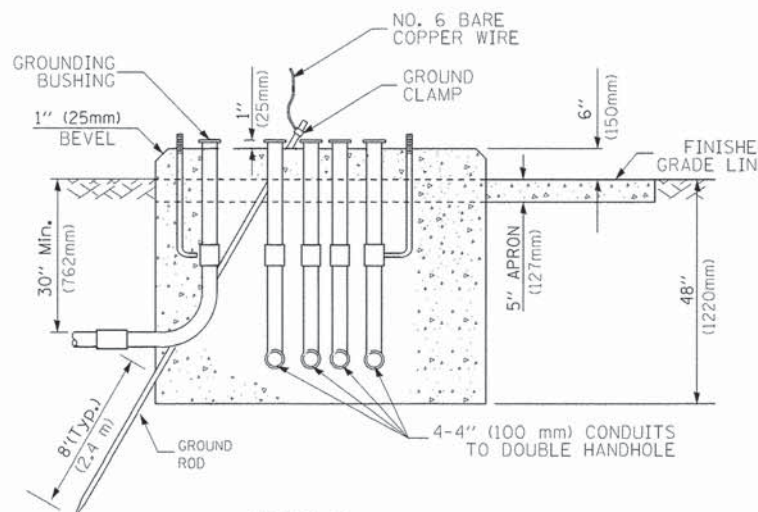
**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**



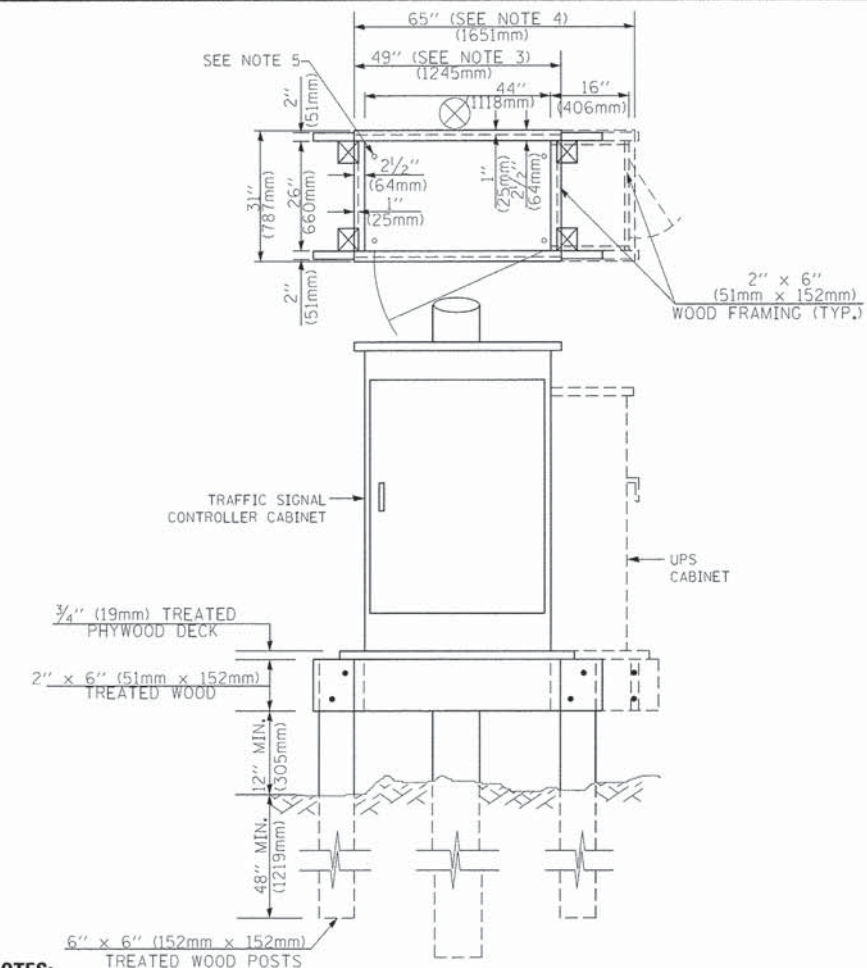
TOP VIEW

NOTE:

TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS**



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

MAST ARM LENGTH	① FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and less than 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and up to 56' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average unconfined compressive strength (Qu) > 1.0 tsf (100 kPa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001..

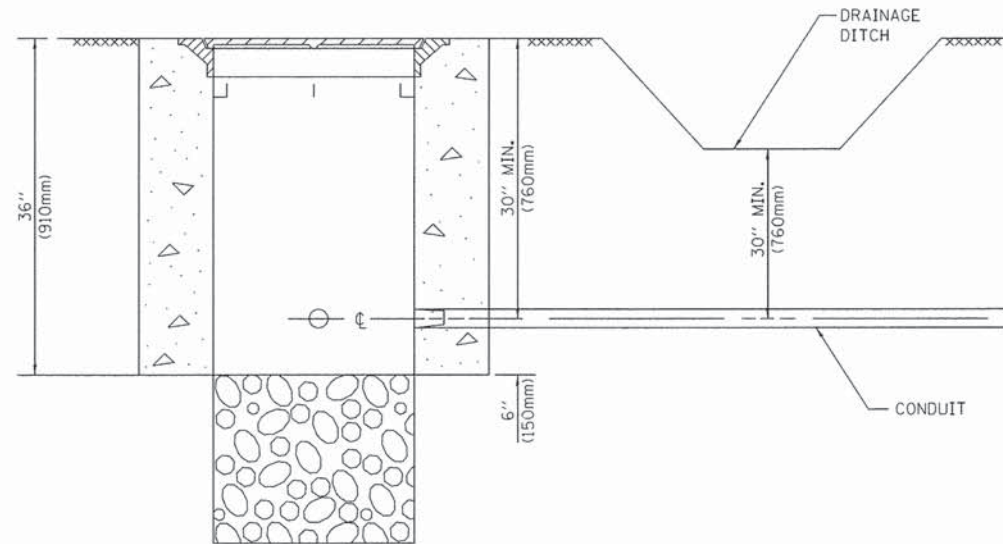
DEPTH OF MAST ARM FOUNDATIONS, TYPE E

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	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
STANDARD TRAFFIC SIGNAL DESIGN DETAILS			
SCALE: NONE	SHEET NO. 5 OF 7 SHEETS	STA.	TO STA.

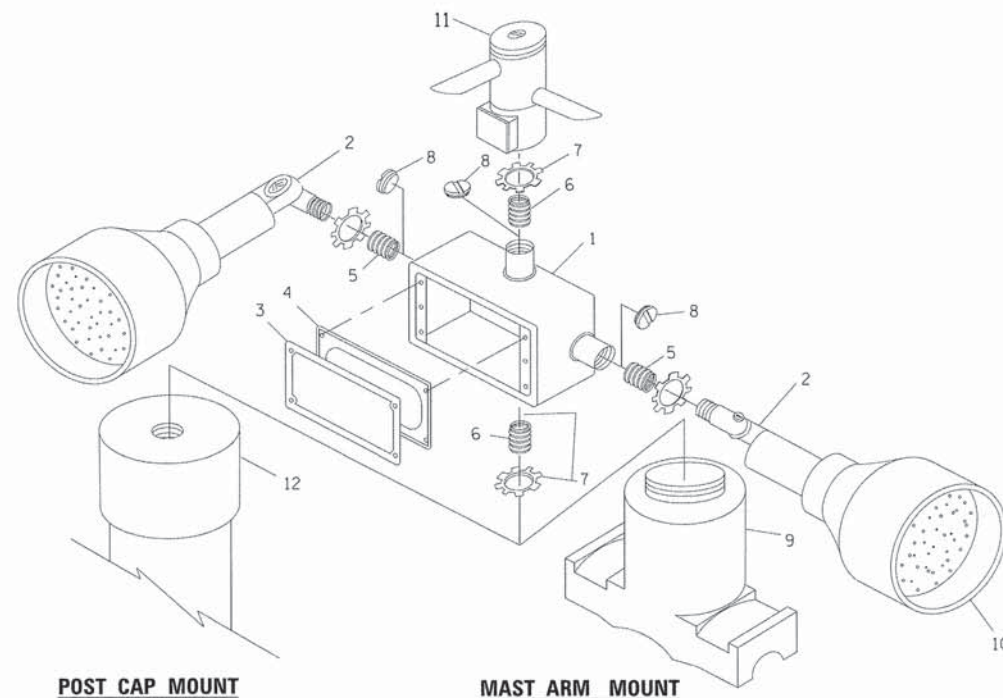
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292	11-00446-00-CH	WILL	110	56
TS-05		CONTRACT NO. 61C29		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-4003 (254)				



NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

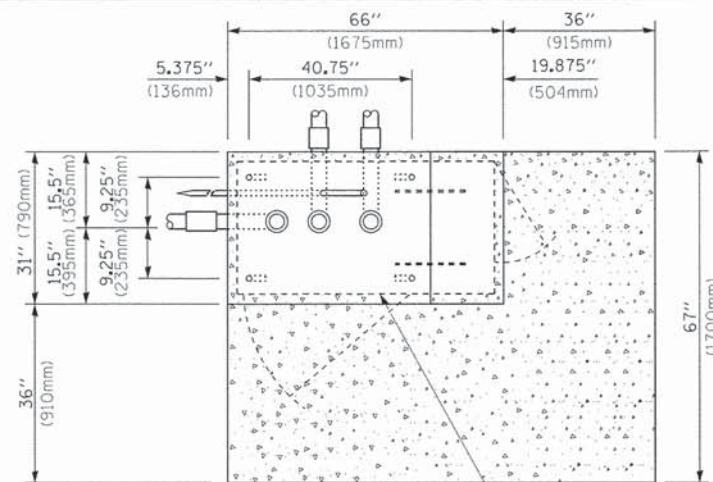
HANDHOLE WITH MINIMUM CONDUIT DEPTH
(NOT TO SCALE)



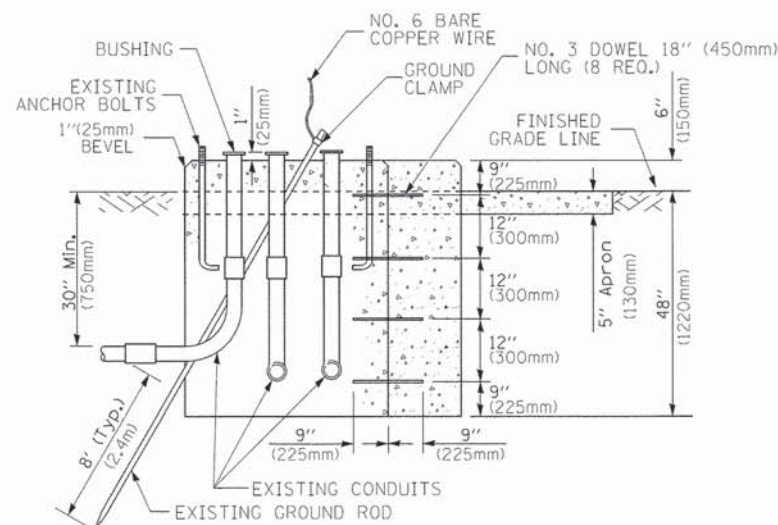
POST CAP MOUNT

MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



TOP VIEW
(NOT TO SCALE)

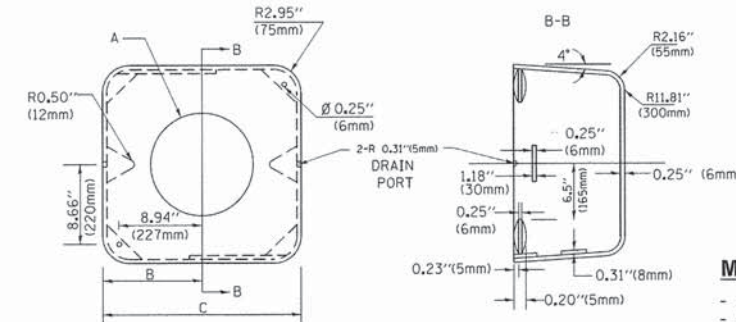


MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION
(NOT TO SCALE)

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0,000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



MATERIAL:
- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5" (241mm)	19" (483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75" (273mm)	21.5" (546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0" (330mm)	26" (660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5" (470mm)	37" (940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

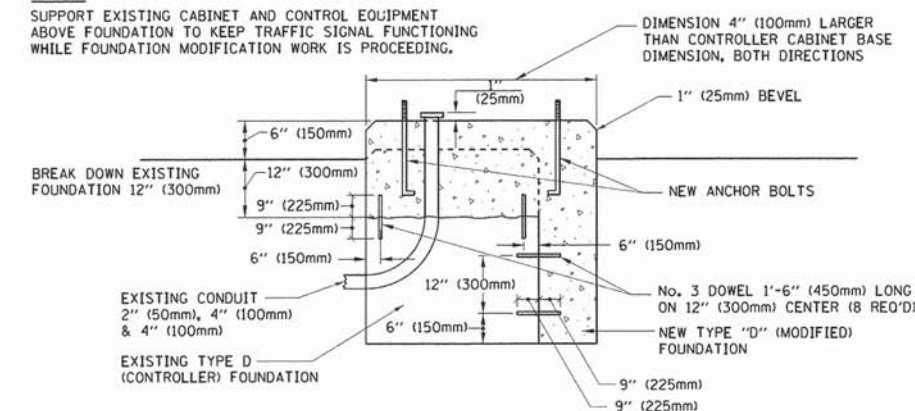
SHROUD

NOTES:

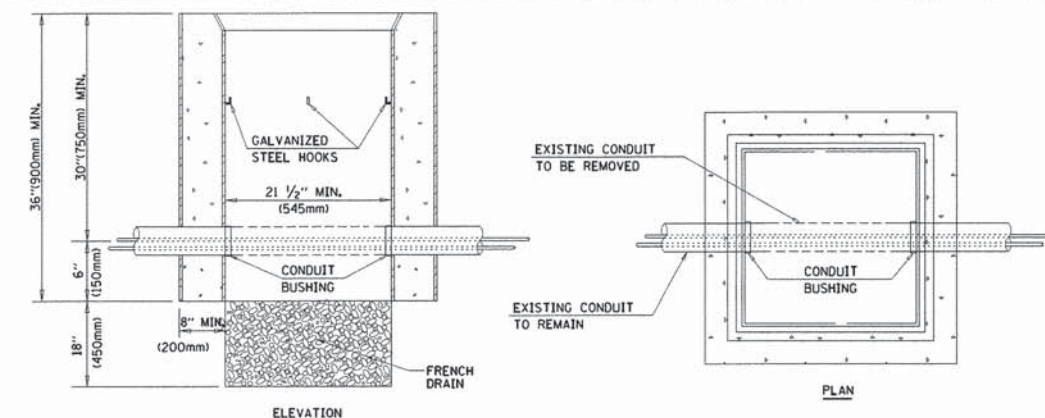
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

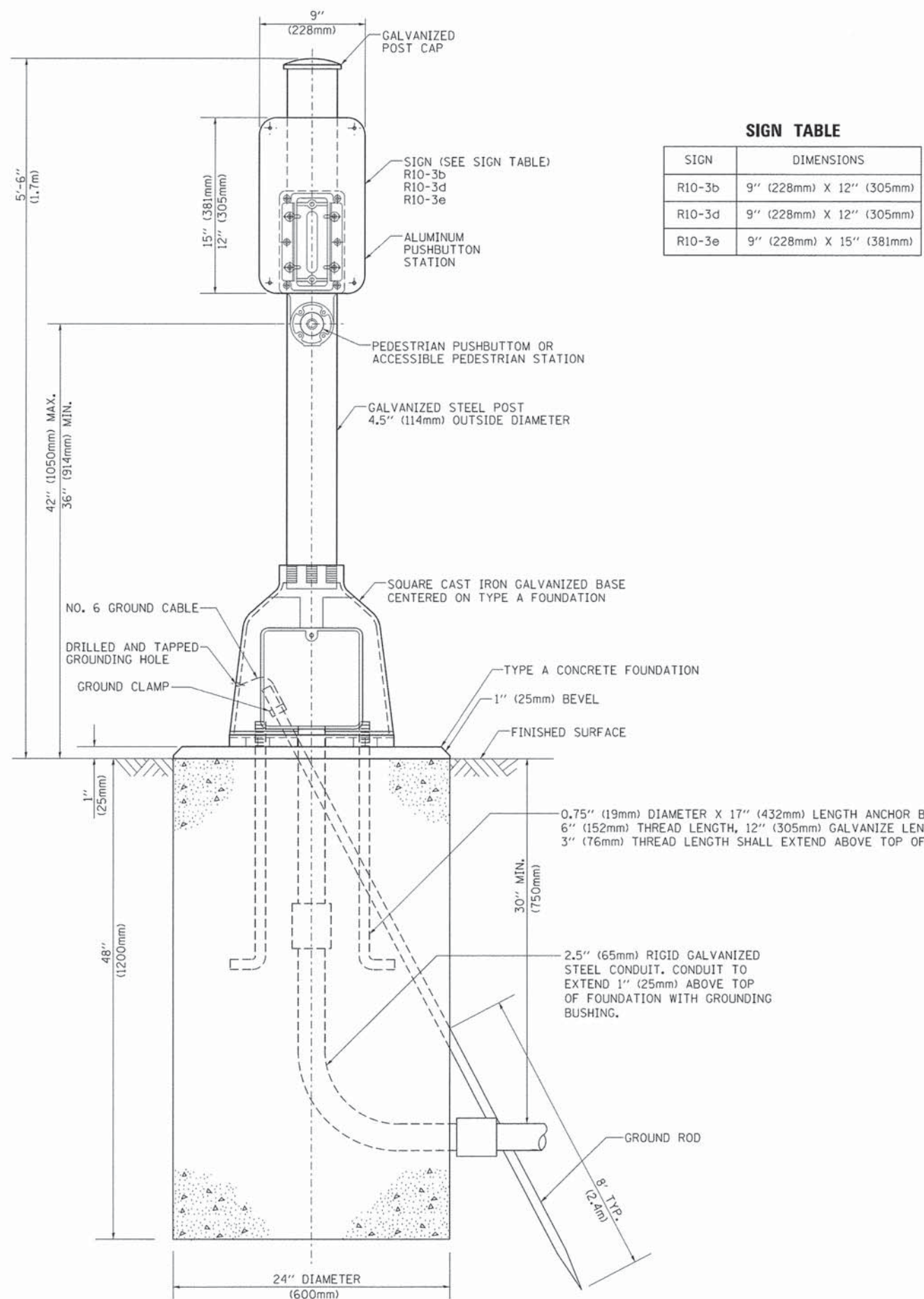
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

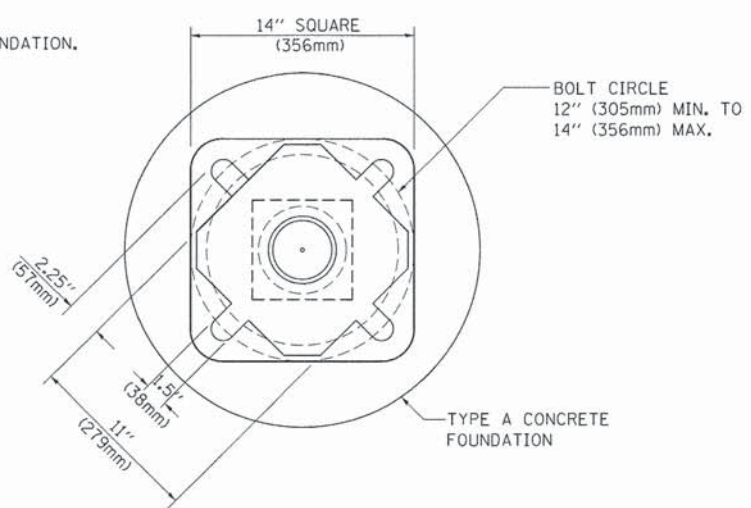
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	57
TS-05		CONTRACT NO. 61C29		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-4003 (254)				



SIGN TABLE

SIGN	DIMENSIONS
R10-3b	9" (228mm) X 12" (305mm)
R10-3d	9" (228mm) X 12" (305mm)
R10-3e	9" (228mm) X 15" (381mm)



BOLT PATTERN

PEDESTRIAN PUSH BUTTON POST, TYPE A

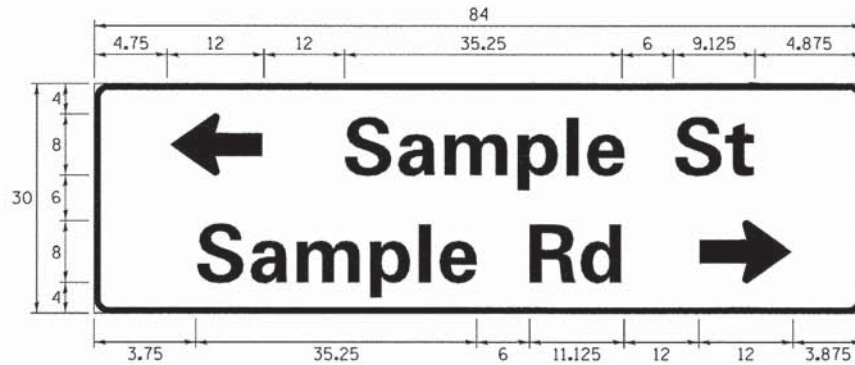
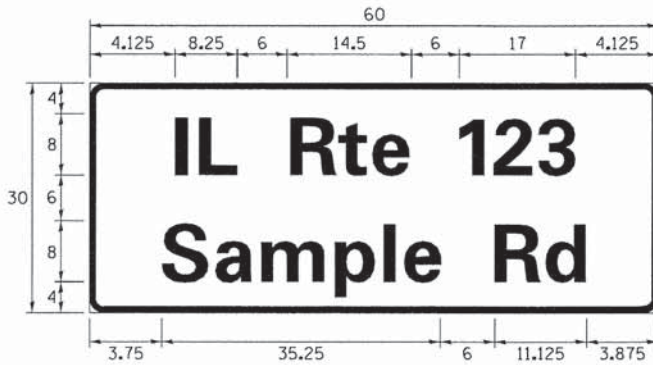
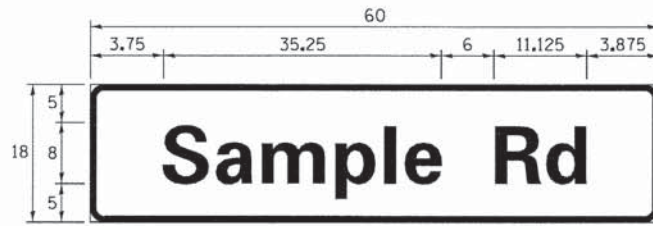
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
STANDARD TRAFFIC SIGNAL DESIGN DETAILS			
SCALE: NONE	SHEET NO. 7 OF 7 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	58
TS-05			CONTRACT NO. 61C29	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (254)				

SIGN PANEL – TYPE 1 OR TYPE 2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D OR C	-	1 OR 2	ZZ	-

COMMON STREET NAME ABBREVIATIONS AND WIDTHS

NAME	ABBREVIATION	WIDTH (INCH)	
		SERIES "C"	SERIES "D"
AVENUE	Ave	15.000	18.250
BOULEVARD	Blvd	17.125	20.000
CIRCLE	Cir	11.125	13.000
COURT	Ct	8.250	9.625
DRIVE	Dr	8.625	10.125
HIGHWAY	Hwy	18.375	22.000
ILLINOIS	IL	7.000	8.250
LANE	Ln	9.125	10.750
PARKWAY	Pkwy	23.375	27.375
PLACE	Pl	7.125	7.750
ROAD	Rd	9.625	11.125
ROUTE	Rte	12.625	14.500
STREET	St	8.000	9.125
TERRACE	Ter	12.625	14.625
TRAIL	Tr	7.750	9.125
UNITED STATES	US	10.375	12.250

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL CONSIST OF A WHITE LEGEND AND BORDER (TYPE ZZ SHEETING) ON A GREEN BACKGROUND (TYPE ZZ SHEETING)
- THE SIGN LENGTH SHALL BE IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'-0". ALL BORDERS SHALL BE 3/4" WIDE. CORNER RADIUS SHALL BE 1-7/8". THE SPACING BETWEEN THE WORDS SHOULD BE 6", IF POSSIBLE, BUT MAY BE REDUCED TO 5" WHEN SPACING IS CRITICAL. A MINIMUM OF 2-1/2" SHALL BE INCLUDED BETWEEN THE WORD AND THE RIGHT AND LEFT EDGES OF THE SIGN.
- A PREFERRED METHOD FOR THE SIGN DESIGN IS TO USE SERIES "D" LETTER ON A ONE-LINE SIGN 18" IN HEIGHT AND A MAXIMUM OF 8'-0" IN WIDTH. IF SERIES "D" DOES NOT FIT ON A 8'-0" SIGN, THEN SERIES "C" SHOULD BE TRIED. IF SERIES "C" DOES NOT FIT ON A 8'-0" SIGN, A 30" HIGH TWO-LINE SIGN CAN BE USED. THE CROSSROAD DESIGNATION AS TO STREET, AVENUE, ETC. SHOULD BE SPELLED OUT ON THE SECOND LINE, IF THERE IS SPACE AVAILABLE.
- LED ILLUMINATED STREET NAME SIGNS CAN BE USED IN PLACE OF REGULAR SIGN PANELS BUT ANY SPECIAL WORDING AND SYMBOLOGY MUST BE APPROVED BY THE DEPARTMENT. GENERAL DESIGN REQUIREMENT AS LISTED ABOVE (COLOR, FONT, SIZE, ETC.) MUST BE FOLLOWED.
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS.

LOCAL SUPPLIERS:

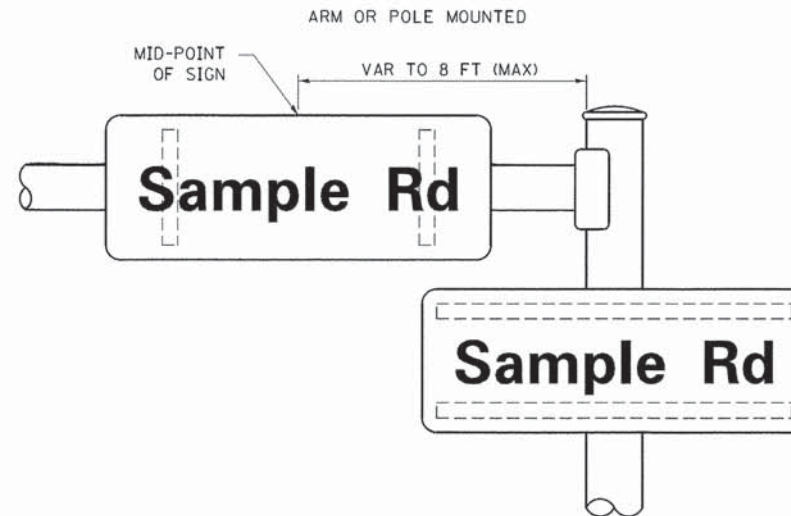
- J.O. HERBERT COMPANY, INC
MIDLOTHIAN, VA
- WESTERN REMAC, INC.
WOODRIDGE, IL

PARTS LISTING:

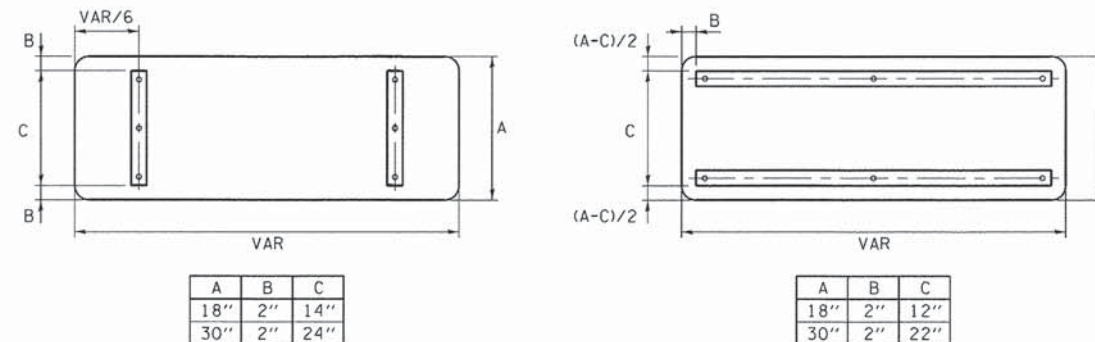
- SIGN CHANNEL PART *HPN053 (MED. CHANNEL)
- SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3
- BRACKETS SELF TAPPING WITH NEOPRENE WASHER
- CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING PART *HPN034 (UNIVERSAL)

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

MOUNTING LOCATION



SUPPORTING CHANNELS



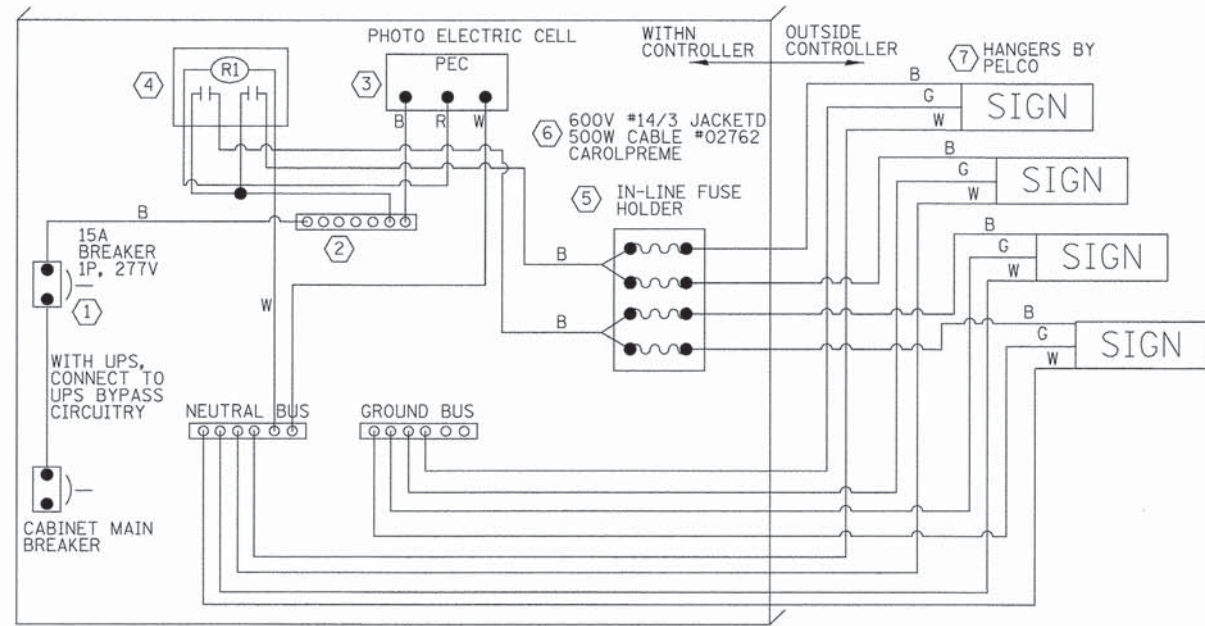
STANDARD ALPHABETS SPACING CHART

(8") UPPER CASE AND (6") LOWER CASE

FHWA SERIES "C"				FHWA SERIES "D"			
CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)	CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)
A	0.240	5.122	0.240	A	0.240	6.804	0.240
B	0.880	4.482	0.480	B	0.960	5.446	0.400
C	0.720	4.482	0.720	C	0.800	5.446	0.800
D	0.880	4.482	0.720	D	0.960	5.446	0.800
E	0.880	4.082	0.480	E	0.960	4.962	0.400
F	0.880	4.082	0.240	F	0.960	4.962	0.240
G	0.720	4.482	0.720	G	0.800	5.446	0.800
H	0.880	4.482	0.880	H	0.960	5.446	0.960
I	0.880	1.120	0.880	I	0.960	1.280	0.960
J	0.240	4.082	0.880	J	0.240	5.122	0.960
K	0.880	4.482	0.480	K	0.960	5.604	0.400
L	0.880	4.082	0.240	L	0.960	4.962	0.240
M	0.880	5.284	0.880	M	0.960	6.244	0.960
N	0.880	4.482	0.880	N	0.960	5.446	0.960
O	0.720	4.722	0.720	O	0.800	5.684	0.800
P	0.880	4.482	0.720	P	0.960	5.446	0.240
Q	0.720	4.722	0.720	Q	0.800	5.684	0.800
R	0.880	4.482	0.480	R	0.960	5.446	0.400
S	0.480	4.482	0.480	S	0.400	5.446	0.400
T	0.240	4.082	0.240	T	0.240	4.962	0.240
U	0.880	4.482	0.880	U	0.960	5.446	0.960
V	0.240	4.962	0.240	V	0.240	6.084	0.240
W	0.240	6.084	0.240	W	0.240	7.124	0.240
X	0.240	4.722	0.240	X	0.400	5.446	0.400
Y	0.240	5.122	0.240	Y	0.240	6.884	0.240
Z	0.480	4.482	0.480	Z	0.400	5.446	0.400
a	0.320	3.842	0.640	a	0.400	4.562	0.720
b	0.720	4.082	0.480	b	0.800	4.802	0.480
c	0.480	4.002	0.240	c	0.480	4.722	0.240
d	0.480	4.082	0.720	d	0.480	4.802	0.800
e	0.480	4.082	0.320	e	0.480	4.722	0.320
f	0.320	2.480	0.160	f	0.320	2.882	0.160
g	0.480	4.082	0.720	g	0.480	4.802	0.800
h	0.720	4.082	0.640	h	0.800	4.722	0.720
i	0.720	1.120	0.720	i	0.800	1.280	0.800
j	0.000	2.320	0.720	j	0.000	2.642	0.800
k	0.720	4.322	0.160	k	0.800	5.122	0.160
l	0.720	1.120	0.720	l	0.800	1.280	0.800
m	0.720	6.724	0.640	m	0.800	7.926	0.720
n	0.720	4.082	0.640	n	0.800	4.722	0.720
o	0.480	4.082	0.480	o	0.480	4.882	0.480
p	0.720	4.082	0.480	p	0.800	4.802	0.480
q	0.480	4.082	0.720	q	0.480	4.802	0.800
r	0.720	2.642	0.160	r	0.800	3.042	0.160
s	0.320	3.362	0.240	s	0.320	3.762	0.240
t	0.080	2.882	0.080	t	0.080	3.202	0.080
u	0.640	4.082	0.720	u	0.720	4.722	0.800
v	0.160	4.722	0.160	v	0.160	5.684	0.160
w	0.160	7.524	0.160	w	0.160	9.046	0.160
x	0.000	5.202	0.000	x	0.000	6.244	0.000
y	0.160	4.962	0.160	y	0.160	6.004	0.160
z	0.240	3.362	0.240	z	0.240	4.002	0.240
1	0.720	1.680	0.880	1	0.800	2.000	0.960
2	0.480	4.482	0.480	2	0.800	5.446	0.800
3	0.480	4.482	0.480	3	1.440	5.446	0.800
4	0.240	4.962	0.720	4	0.160	6.004	0.960
5	0.480	4.482	0.480	5	0.800	5.446	0.800
6	0.720	4.482	0.720	6	0.800	5.446	0.800
7	0.240	4.482	0.720	7	0.560	5.446	0.560
8	0.480	4.482	0.480	8	0.800	5.446	0.800
9	0.480	4.482	0.480	9	0.800	5.446	0.800
0	0.720	4.722	0.720	0	0.800	5.684	0.800
-	0.240	2.802	0.240	-	0.240	2.802	0.240

DATE	
BY	
SURVEYED	
PLOTTED	
NOTE BOOK	
NO.	

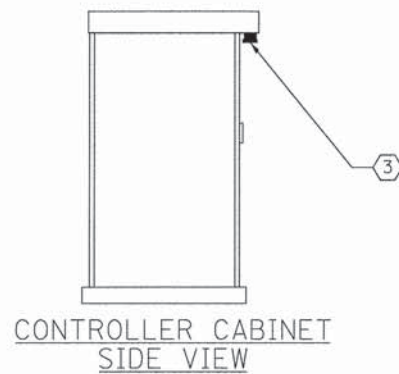
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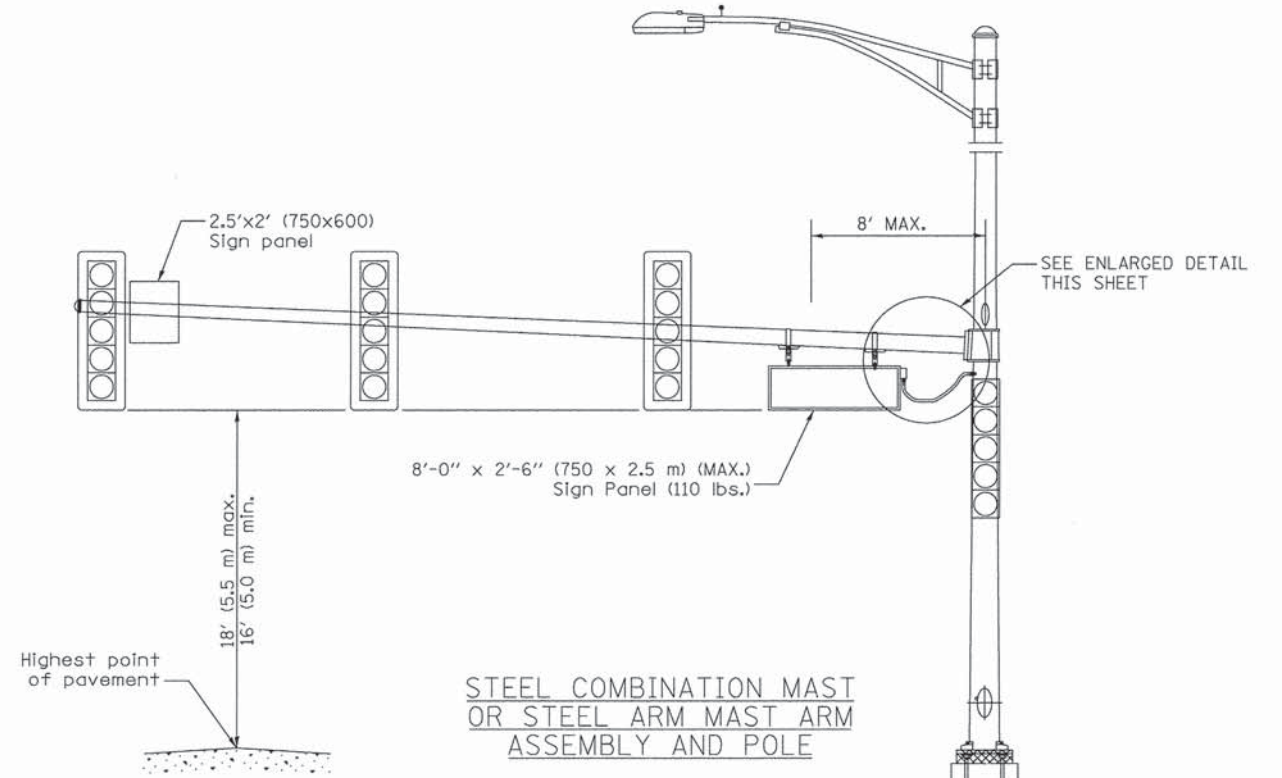
L.E.D. SIGN WIRING DETAIL

DESCRIPTION	MANUFACTURER	MODEL	NOTES
① CIRCUIT BREAKER		15 AMPERE	Molded Case, Thermal Mag. min. R.I. of 14K R.M.S. symmetrical Ampere at 277V.
② TERMINAL BLOCK	MARATHON	1502 DJSV	
③ PHOTO ELECTRIC CONTROL	FISHER PIERCE	B124-1.5-07762	
④ CONTROL RELAY	SQUARE D	8501X020V02	BOLT ON W/SCREW TERMINAL
⑤ INLINE FUSE HOLDER WITH 5 AMP FUSE	BUSSMAN	S-8000 BK/S-8-3-4-R	
⑥ ELECTIC CABLE, No 14, 3/C (BLACK, WHITE, GREEN)	CAROLPRENE/S00W	02762	
⑦ SIGN MOUNTING HARDWARE	PELCO	SE-5015	S.S. HARDWARE

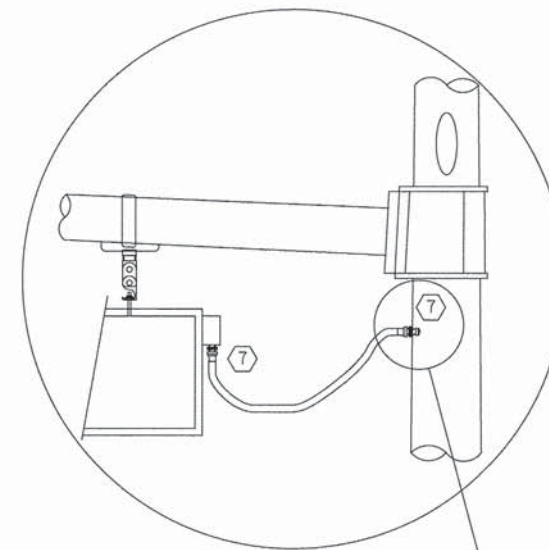
BILL OF MATERIALS



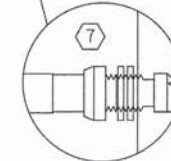
CONTROLLER CABINET SIDE VIEW



STEEL COMBINATION MAST OR STEEL ARM MAST ARM ASSEMBLY AND POLE



L.E.D. SIGN ENLARGED CABLE CONNECTOR DETAIL



L.E.D. SIGN ENLARGED CABLE CONNECTOR DETAIL



LED ILLUMINATED SIGN PANEL
8'-0" x 2'-6" (2.5 m x 750 mm) (MAX)
FHWA SERIES C OR D FONT

NOTES:

- SIGNS SHALL BE DUAL SIDED. FRONT AND BACK OF SIGN WILL BE THE SAME.
- CERTAIN ADDITIONAL INFORMATION MAY BE ALLOWED ON THE SIGN. VERIFY WITH ENGINEER.
- SIGNS SHALL NOT BE ENERGIZED WHEN TRAFFIC SIGNALS ARE POWERED BY THE UPS. THE SIGNS SHALL BE CONNECTED TO THE UPS BYPASS CIRCUITRY.
- ALL WIRING WITHIN THE CABLE SHALL BE COLOR CODED AS INDICATED:
R = RED BL = BLUE W = WHITE
B = BLACK Y = YELLOW G = GREEN
- ALL 120 VOLT SYSTEM AND ALL CONTROL WIRING SHALL BE #12 AWG STRANDED UNLESS OTHERWISE INDICATED.
- ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

TEMPORARY TRAFFIC SIGNAL NOTES

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12 INCHES (300 MM). TRAFFIC SIGNAL SECTIONS SHALL BE LED WITH EXPANDABLE VIEW, UNLESS OTHERWISE APPROVED BY THE ENGINEER. PEDESTRIAN SIGNAL HEADS SHALL BE LIGHT EMITTING DIODE (LED) PEDESTRIAN COUNTDOWN SIGNAL HEADS EXCEPT WHEN A TEMPORARY TRAFFIC SIGNAL IS INSTALLED AT AN INTERSECTION INTERCONNECTED WITH A RAILROAD GRADE CROSSING. WHEN A TEMPORARY TRAFFIC SIGNAL IS INSTALLED AT AN INTERSECTION INTERCONNECTED WITH A RAILROAD GRADE CROSSING, LIGHT EMITTING DIODE (LED) PEDESTRIAN SIGNAL HEADS SHALL BE FURNISHED. THE TEMPORARY TRAFFIC SIGNAL HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH EXTRA CABLE LENGTH TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT AT NO ADDITIONAL COST TO THE CONTRACT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. ALL TEMPORARY TRAFFIC SIGNAL INSTALLATIONS SHALL HAVE UNINTERRUPTIBLE POWER SUPPLY (UPS). THE UPS CABINET SHALL BE MOUNTED TO THE TEMPORARY TRAFFIC SIGNAL CABINET AND MEET THE REQUIREMENTS OF UNINTERRUPTABLE POWER SUPPLY IN DIVISIONS 800 AND 1000 OF THESE SPECIFICATIONS.
8. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
9. DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. PEDESTRIAN PUSH BUTTONS SHALL BE PROVIDED FOR ALL PEDESTRIAN SIGNAL HEADS/PHASES AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER. DETECTION SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN TEMPORARY TRAFFIC SIGNAL INSTALLATION PAY ITEM.
10. WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

TRAFFIC SIGNAL GENERAL NOTES

THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", JANUARY 1, 2012; MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, LATEST EDITION; PROJECT SPECIFICATIONS; ALL APPLICABLE REQUIREMENTS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND THE CITY OF JOLIET; ALL APPLICABLE REQUIREMENTS OF THE ORDINANCES OF AUTHORITIES HAVING JURISDICTION; AND ALL ADDENDA THERETO SHALL GOVERN THIS WORK.

THE STANDARD SPECIFICATIONS, PROJECT SPECIFICATIONS, CONSTRUCTION PLANS, AND SUBSEQUENT DETAILS ARE ALL TO BE CONSIDERED AS PART OF THE CONTRACT. INCIDENTAL ITEMS OR ACCESSORIES NECESSARY TO COMPLETE THIS WORK MAY NOT BE SPECIFICALLY NOTED BUT ARE TO BE CONSIDERED A PART OF THE CONTRACT.

WHENEVER, DURING CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF GUTTERS, DRAINAGE STRUCTURES, DITCHES, ETC. SUCH THAT THE NATURAL FLOW LINE OF WATER IS OBSTRUCTED, THE LOOSE MATERIAL WILL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES AND FLOW LINES SHALL BE FREE FROM DIRT AND DEBRIS. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR'S FAILURE TO PROVIDE THE ABOVE WILL PRECLUDE ANY POSSIBLE ADDED COMPENSATION REQUESTED DUE TO DELAYS OF UNSTABLE MATERIALS CREATED AS A RESULT THEREOF.

THE CONTRACTOR SHALL SOLELY BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ADEQUATE SIGNS, TRAFFIC CONTROL DEVICES, AND WARNING DEVICES TO INFORM AND PROTECT THE PUBLIC DURING ALL PHASES OF CONSTRUCTION.

THE CONTRACTOR IS RESPONSIBLE FOR RETURNING ALL AREAS AFFECTED BY EQUIPMENT OR LABORERS TO EXISTING CONDITIONS. THE CONTRACTOR IS ALSO RESPONSIBLE FOR PROTECTING ALL NEW WORK UNTIL COMPLETION OF THIS CONTRACT.

EXISTING UTILITIES: WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES, SUCH INFORMATION IS BASED ON RECORD INFORMATION PROVIDED BY THE INDIVIDUAL UTILITY OWNERS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES. THE CONTRACTOR SHALL ALSO CONTACT J.U.L.I.E. TO OBTAIN LOCATES OF THE RESPECTIVE UTILITY COMPANIES' UNDERGROUND FACILITIES.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.

THE CONTRACTOR SHALL CONTACT THE CITY OF JOLIET PUBLIC WORKS DEPARTMENT AT (815) 724-4200 72 HOURS IN ADVANCE OF BEGINNING WORK.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.

THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811.

IF THIS CONTRACT REQUIRES THE SERVICES OF AN ELECTRICAL CONTRACTOR, THE CONTRACTOR SHALL BE RESPONSIBLE AT HIS/HER OWN EXPENSE FOR LOCATING EXISTING IDOT ELECTRICAL FACILITIES PRIOR TO PERFORMING ANY WORK. IF THIS CONTRACT DOES NOT REQUIRE THE SERVICES OF AN ELECTRICAL CONTRACTOR, THE CONTRACTOR MAY REQUEST ONE FREE LOCATE FOR EXISTING IDOT ELECTRICAL FACILITIES FROM THE DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR PRIOR TO THE START OF ANY WORK. ADDITIONAL REQUESTS MAY BE AT THE EXPENSE OF THE CONTRACTOR. THE LOCATION OF UNDERGROUND TRAFFIC FACILITIES DOES NOT RELIEVE THE CONTRACTOR OF THEIR RESPONSIBILITY TO REPAIR ANY FACILITIES DAMAGED DURING CONSTRUCTION AT THEIR EXPENSE.

THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.

RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDER, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

ALL CONDUIT SHALL BE PLACED IN TRENCH. ALL ROADWAY SURFACES SUCH AS PAVEMENT, SIDEWALK, ETC. SHALL BE REPLACED IN KIND. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PAY ITEM "UNDERGROUND CONDUIT, GALVANIZED STEEL" OF THE SIZE SPECIFIED. NO ADDITIONAL COMPENSTATION SHALL BE PROVIDED FOR TRENCH AND BACKFILL OR FOR RESTORATION.

THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC SIGNAL GENERAL NOTES

NOT TO SCALE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	61
				CONTRACT NO. 61C29

ILLINOIS FED. AID PROJECT M-4003 (254)



MATCH LINE STA. 501+50
SEE SHEET NO. 63

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 1 EACH SERVICE INSTALLATION
- 4 EACH COMBINATION STEEL MAST ARM ASSEMBLY AND POLE
- 4 EACH SIGNAL HEAD, 1-FACE, 5-SECTION
- 4 EACH SIGNAL HEAD, 1-FACE, 3-SECTION
- 4 EACH SIGNAL HEAD, 2-FACE, 1-3 SECTION, 1-5 SECTION
- 4 EACH PEDESTRIAN SIGNAL HEAD, 2-FACE
- 8 EACH PEDESTRIAN PUSH BUTTON
- 8 EACH TRAFFIC SIGNAL BACKPLATE
- 4 EACH LUMINAIRE

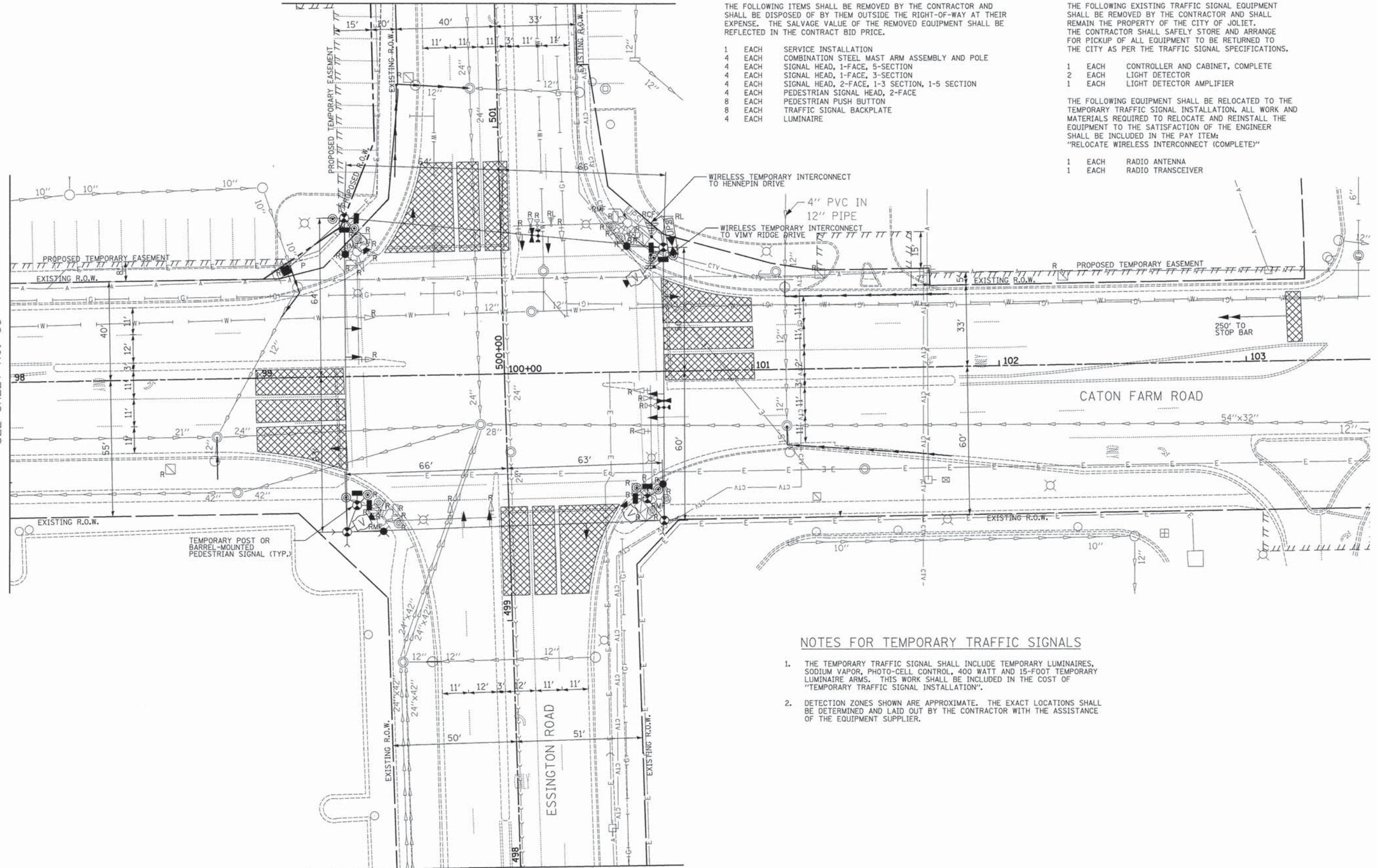
THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE CITY OF JOLIET. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICKUP OF ALL EQUIPMENT TO BE RETURNED TO THE CITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- 1 EACH CONTROLLER AND CABINET, COMPLETE
- 2 EACH LIGHT DETECTOR
- 1 EACH LIGHT DETECTOR AMPLIFIER

THE FOLLOWING EQUIPMENT SHALL BE RELOCATED TO THE TEMPORARY TRAFFIC SIGNAL INSTALLATION. ALL WORK AND MATERIALS REQUIRED TO RELOCATE AND REINSTALL THE EQUIPMENT TO THE SATISFACTION OF THE ENGINEER SHALL BE INCLUDED IN THE PAY ITEM:
"RELOCATE WIRELESS INTERCONNECT (COMPLETE)"

- 1 EACH RADIO ANTENNA
- 1 EACH RADIO TRANSCEIVER

MATCH LINE STA. 98+00
SEE SHEET NO. 63



NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. THE TEMPORARY TRAFFIC SIGNAL SHALL INCLUDE TEMPORARY LUMINAIRES, SODIUM VAPOR, PHOTO-CELL CONTROL, 400 WATT AND 15-FOOT TEMPORARY LUMINAIRE ARMS. THIS WORK SHALL BE INCLUDED IN THE COST OF "TEMPORARY TRAFFIC SIGNAL INSTALLATION".
2. DETECTION ZONES SHOWN ARE APPROXIMATE. THE EXACT LOCATIONS SHALL BE DETERMINED AND LAID OUT BY THE CONTRACTOR WITH THE ASSISTANCE OF THE EQUIPMENT SUPPLIER.

MATCH LINE STA. 498+00
SEE SHEET NO. 63

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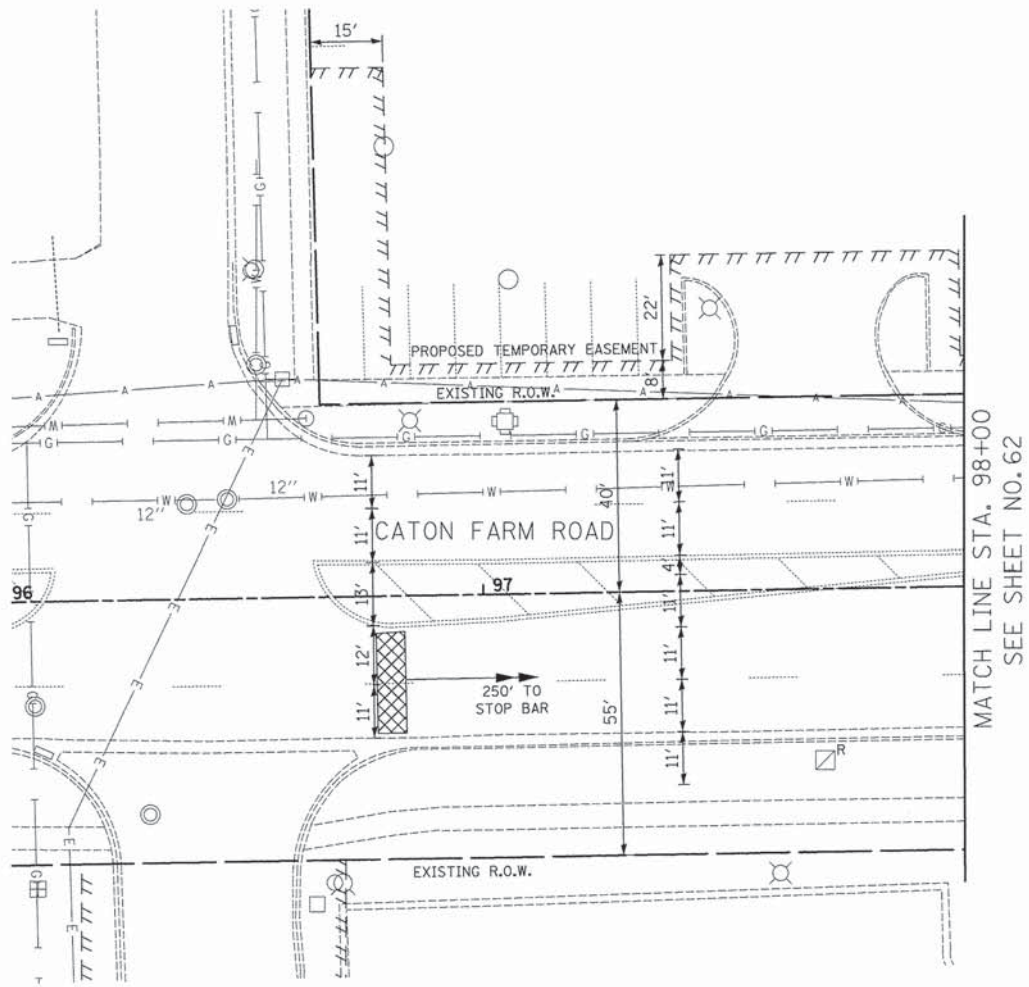
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

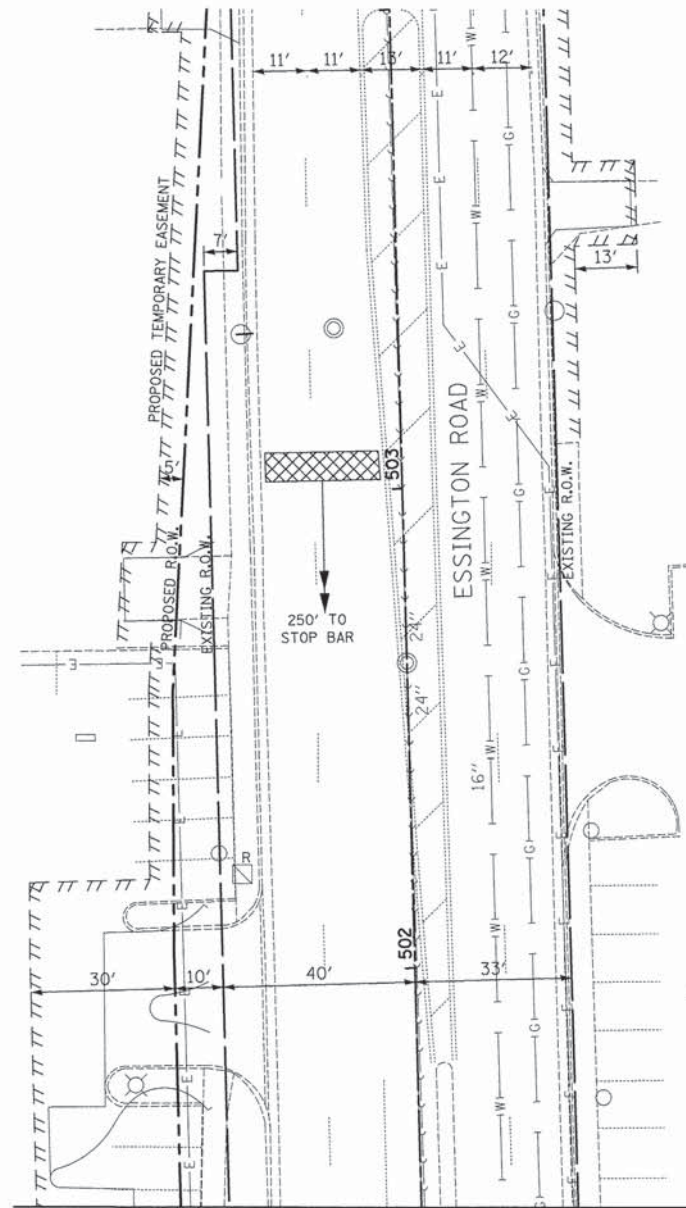
CATON FARM ROAD AND ESSINGTON ROAD
TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVAL PLAN

SCALE: 1" = 20' SHEET 1 OF 2 SHEETS STA. TO STA.

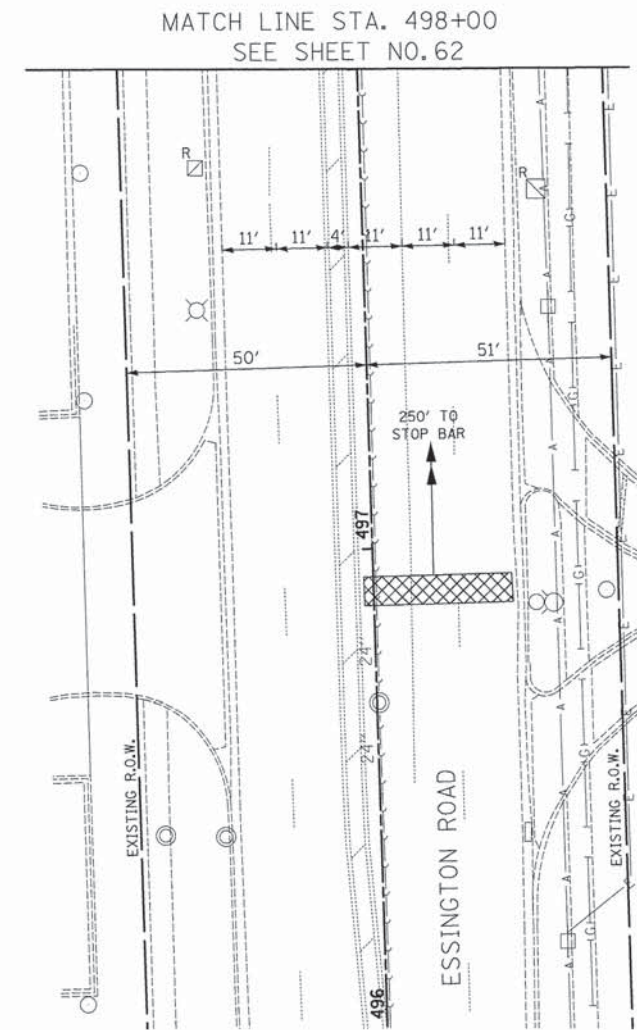
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292	11-00446-00-CH	WILL	110	62
				CONTRACT NO. 61C29
ILLINOIS FED. AID PROJECT M-4003 (254)				



MATCH LINE STA. 98+00
SEE SHEET NO. 62



MATCH LINE STA. 501+50
SEE SHEET NO. 62



MATCH LINE STA. 498+00
SEE SHEET NO. 62

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	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

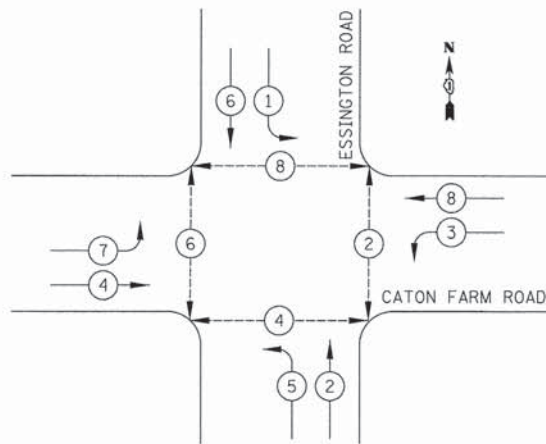
CATON FARM ROAD AND ESSINGTON ROAD
TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVAL PLAN

SCALE: 1" = 20' SHEET 2 OF 2 SHEETS STA. TO STA.

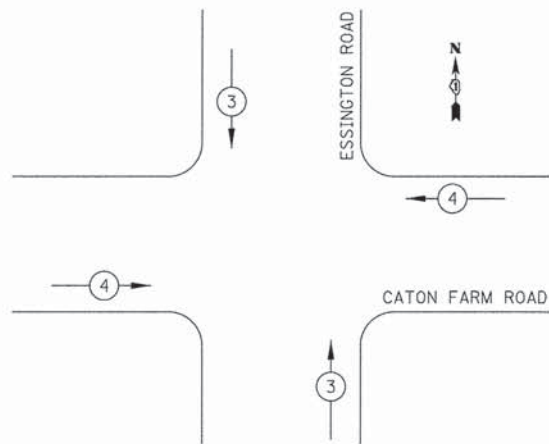
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	63
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				



TEMPORARY CONTROLLER SEQUENCE



TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE



LEGEND

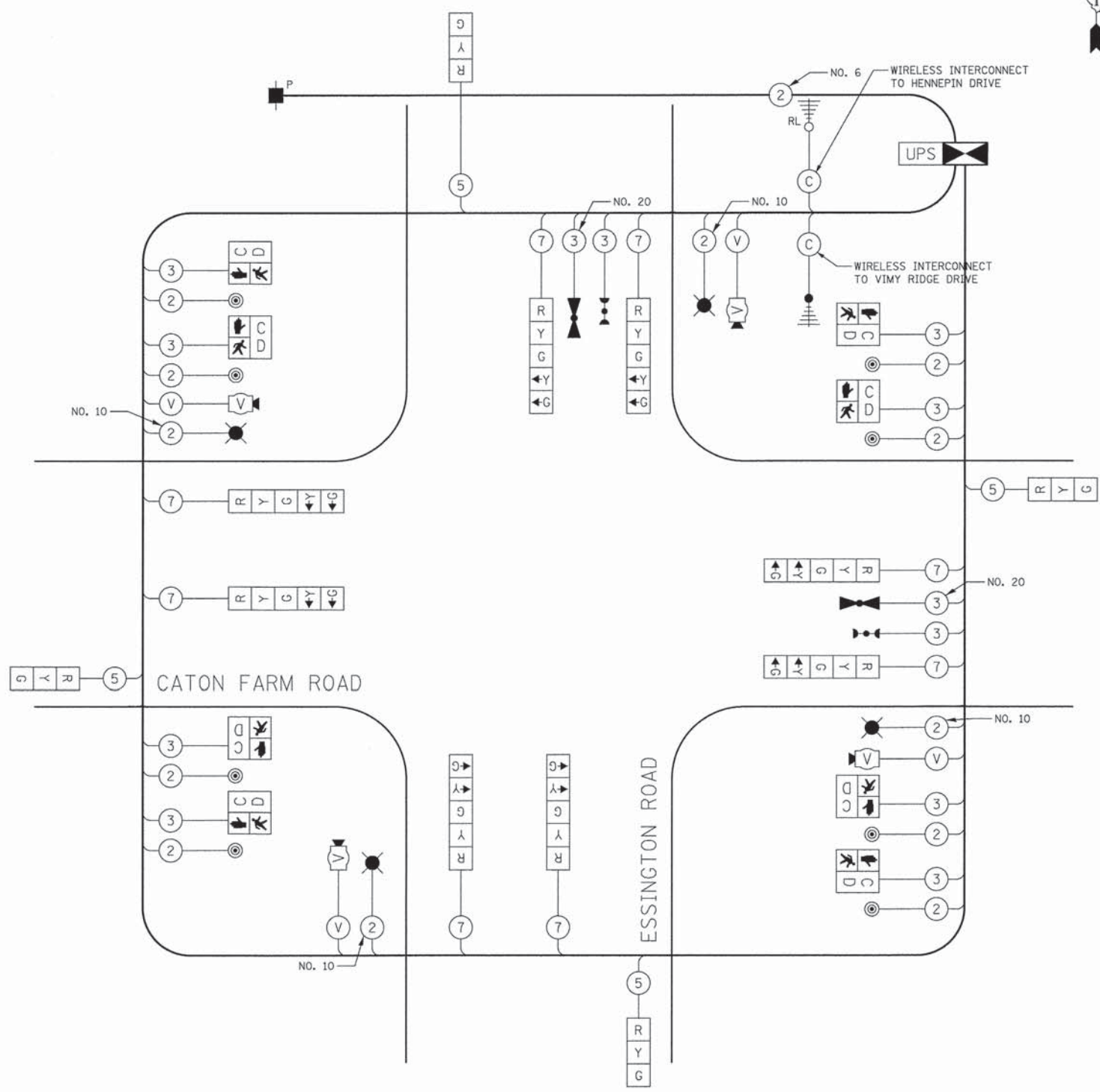
- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- OVERLAP
- PEDESTRIAN PHASE
- NUMBER REFERS TO ASSOCIATED PHASE

TEMPORARY PHASE DESIGNATION DIAGRAM

TEMPORARY EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT		

TEMPORARY CABLE PLAN NOTES

- THE TEMPORARY LUMINAIRES SHALL NOT BE CONNECTED TO THE UNINTERRUPTIBLE POWER SUPPLY.



TEMPORARY CABLE PLAN
NOT TO SCALE

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE	
TYPE	NO. LAMPS	WATTAGE		% OPERATION		
SIGNAL (RED)	12	INCAND.	17	0.50	102	
	(YELLOW)	12		25	0.25	75
	(GREEN)	12		15	0.25	45
ARROW	8		12	0.10	10	
PED. SIGNAL	8		25	1.00	200	
CONTROLLER	1		100	1.00	100	
VIDEO SYSTEM	1		150	1.00	150	
LUMINAIRE	4	400		0.50	800	
TOTAL =					1482	

ENERGY COSTS TO: CITY OF JOLIET
150 W. JEFFERSON ST.
JOLIET, IL 60432

ENERGY SUPPLY: CONTACT: DONNA GILE
PHONE: (815) 724-5923
COMPANY: COM ED

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	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
TEMPORARY CABLE PLAN, PHASE DESIGNATION DIAGRAM, AND
TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE

F.A.U. RTE. 292	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 64
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

NOT TO SCALE SHEET 1 OF 1 SHEETS STA. TO STA.

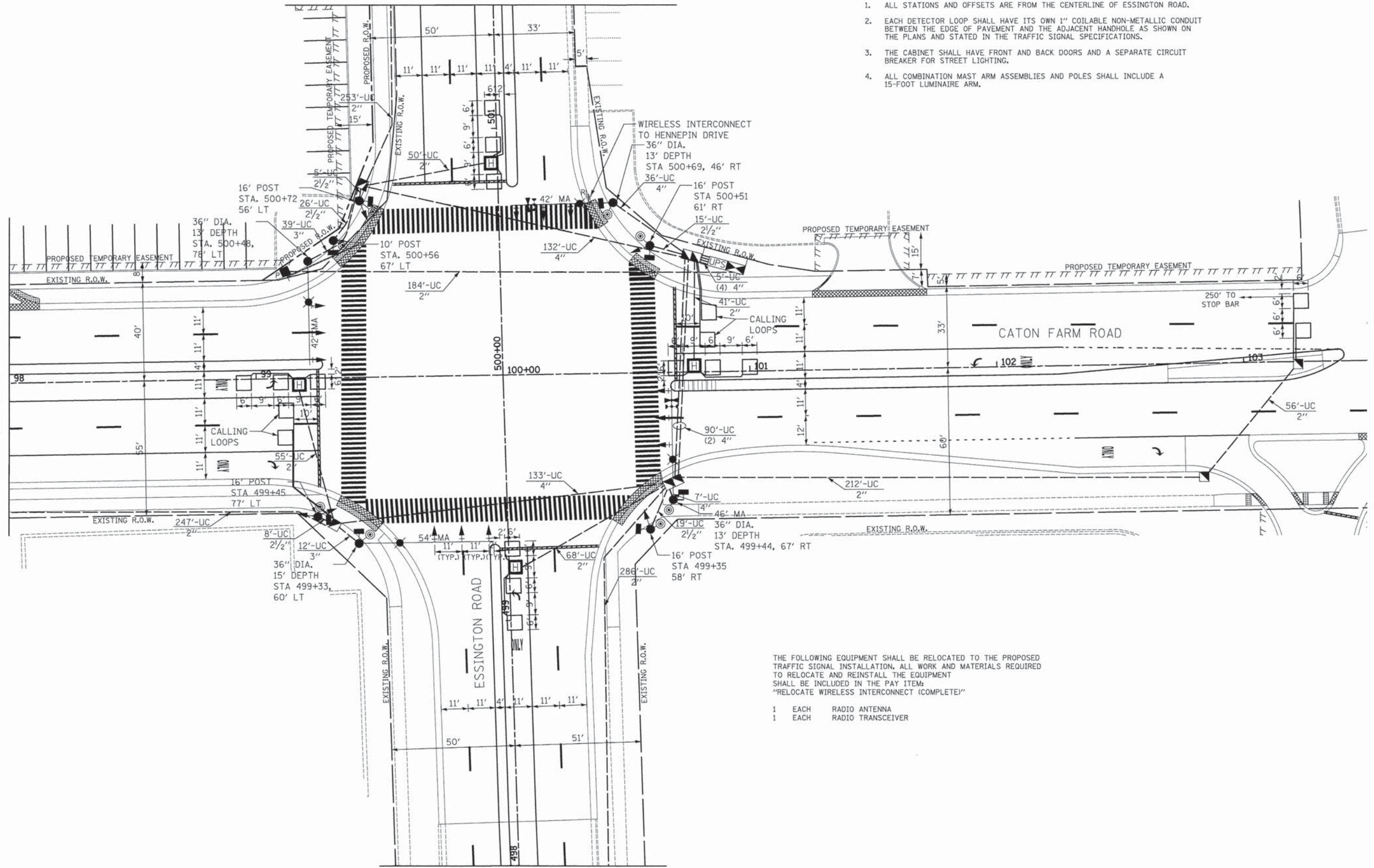


MATCH LINE STA. 501+50
SEE SHEET NO. 66

NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL STATIONS AND OFFSETS ARE FROM THE CENTERLINE OF ESSINGTON ROAD.
2. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
3. THE CABINET SHALL HAVE FRONT AND BACK DOORS AND A SEPARATE CIRCUIT BREAKER FOR STREET LIGHTING.
4. ALL COMBINATION MAST ARM ASSEMBLIES AND POLES SHALL INCLUDE A 15-FOOT LUMINAIRE ARM.

MATCH LINE STA. 98+00
SEE SHEET NO. 66



THE FOLLOWING EQUIPMENT SHALL BE RELOCATED TO THE PROPOSED TRAFFIC SIGNAL INSTALLATION. ALL WORK AND MATERIALS REQUIRED TO RELOCATE AND REINSTALL THE EQUIPMENT SHALL BE INCLUDED IN THE PAY ITEM:
"RELOCATE WIRELESS INTERCONNECT (COMPLETE)"

- 1 EACH RADIO ANTENNA
- 1 EACH RADIO TRANSCEIVER

MATCH LINE STA. 498+00
SEE SHEET NO. 66

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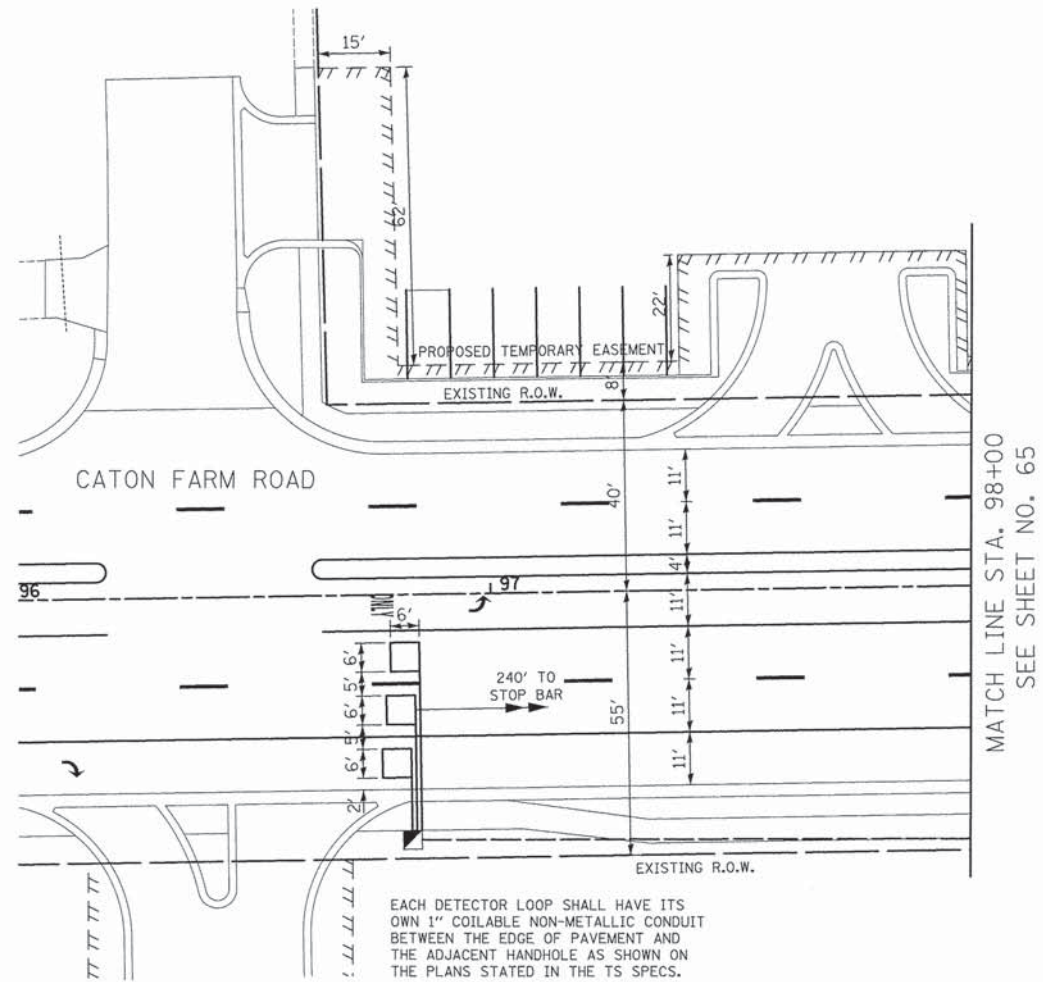
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	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

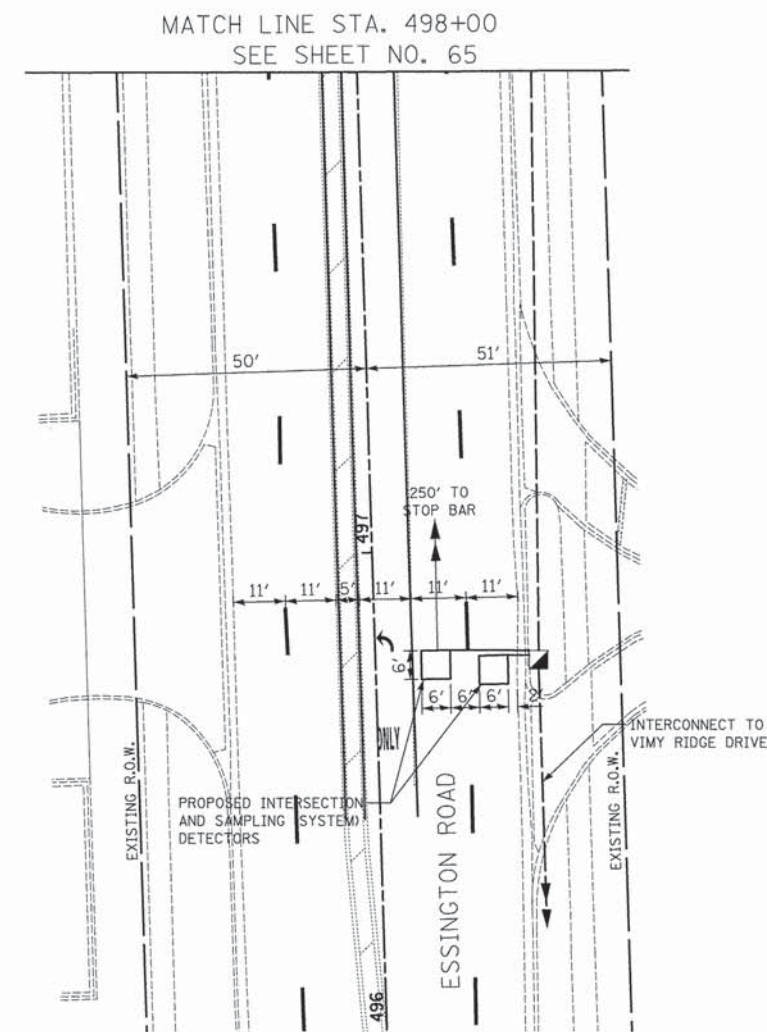
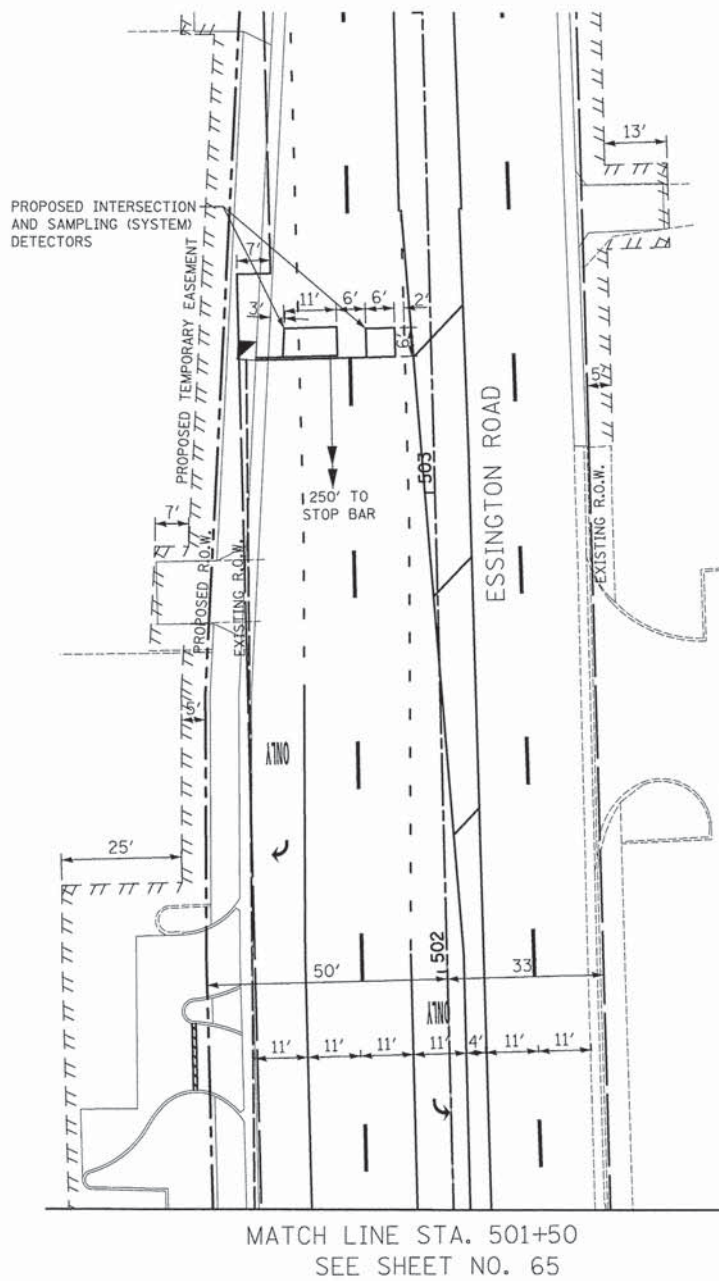
CATON FARM ROAD AND ESSINGTON ROAD
TRAFFIC SIGNAL MODERNIZATION PLAN

SCALE: 1" = 20' SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	65
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				



MATCH LINE STA. 98+00
SEE SHEET NO. 65



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	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

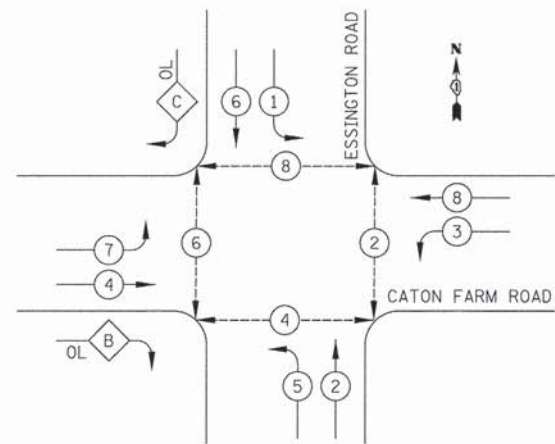
CATON FARM ROAD AND ESSINGTON ROAD
TRAFFIC SIGNAL MODERNIZATION PLAN

SCALE: 1" = 20' SHEET 2 OF 2 SHEETS STA. TO STA.

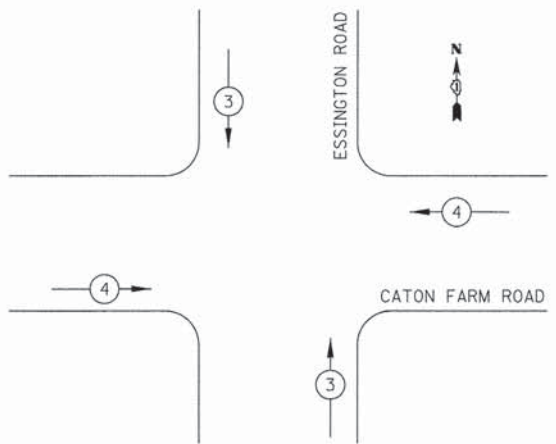
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	66
				CONTRACT NO. 61C29
ILLINOIS FED. AID PROJECT M-4003 (254)				



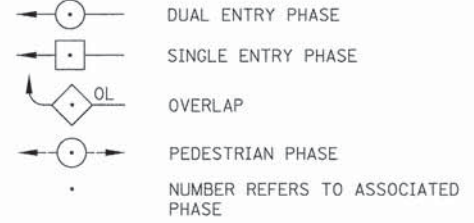
PROPOSED CONTROLLER SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



LEGEND



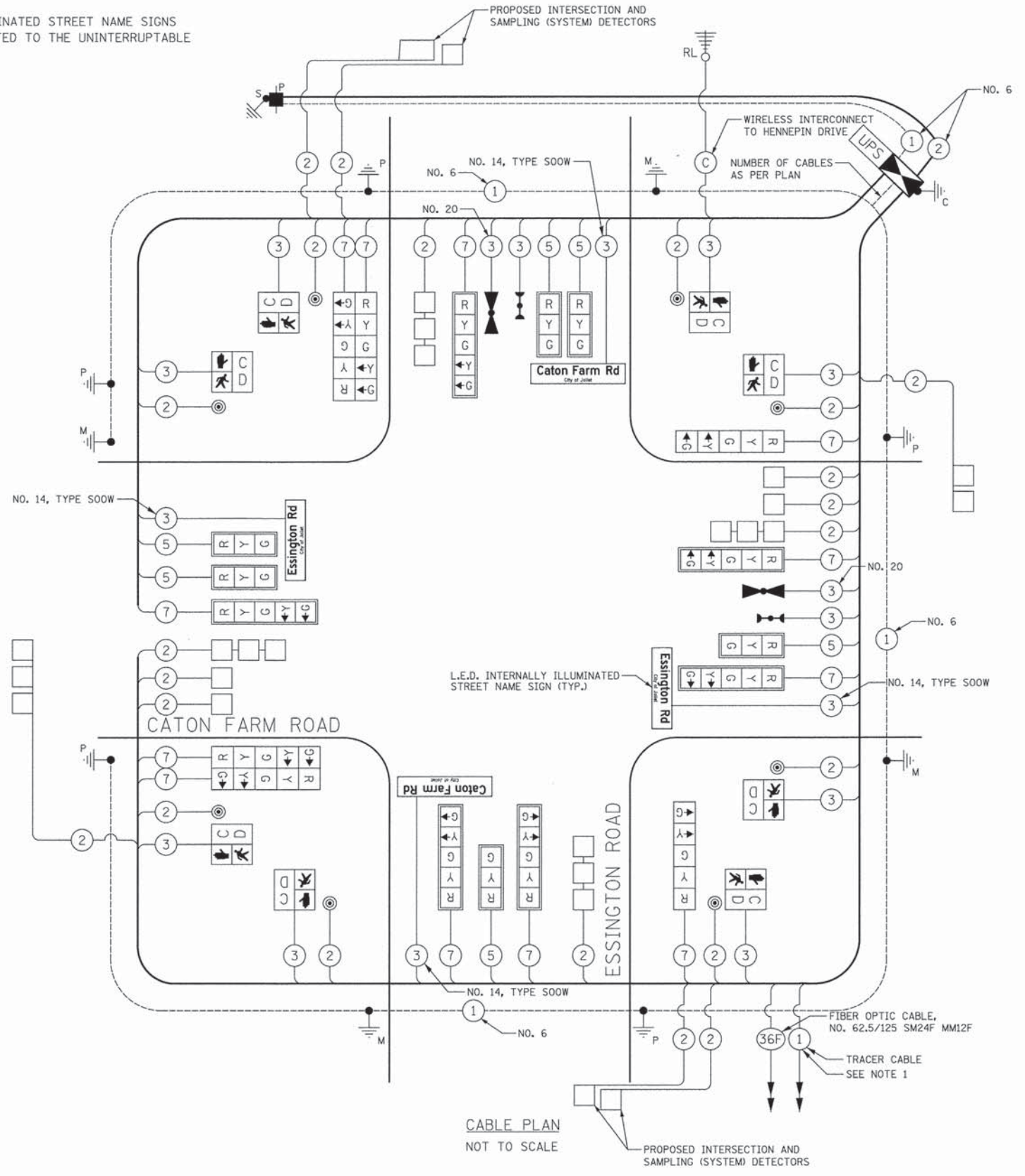
PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	↑↓	→←

PHASE DESIGNATION DIAGRAM

RIGHT TURN OVERLAP PHASE DESIGNATION			
OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE	
B	= 4	+	5
C	= 6	+	7

TRAFFIC SIGNAL NOTES

1. THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.
2. LED INTERNALLY ILLUMINATED STREET NAME SIGNS SHALL NOT BE CONNECTED TO THE UNINTERRUPTABLE POWER SUPPLY.



I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE		% OPERATION	
SIGNAL (RED)	18	INCAND.	17	0.50	153
(YELLOW)	18		25	0.25	113
(GREEN)	18		15	0.25	68
ARROW	24		12	0.10	29
PED. SIGNAL	8		25	1.00	200
CONTROLLER	1		100	1.00	100
LED INT ILLUMINATED STREET SIGN	4		64	0.50	128
FLASHER				0.50	
TOTAL =					791

ENERGY COSTS TO: CITY OF JOLIET
150 W. JEFFERSON ST.
JOLIET, IL 60432

ENERGY SUPPLY: CONTACT: DONNA GILE
PHONE: (815) 724-5923
COMPANY: COM ED

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
CABLE PLAN AND SEQUENCE OF OPERATIONS

NOT TO SCALE SHEET 1 OF 1 SHEETS STA. TO STA.

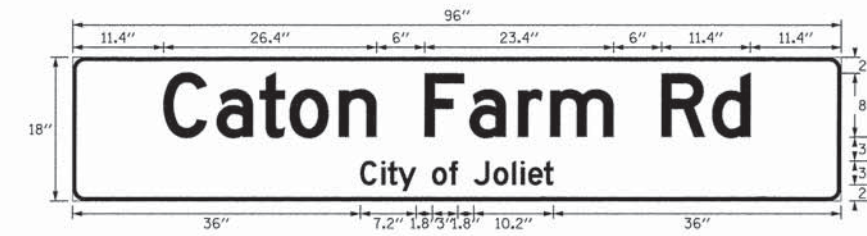
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CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	



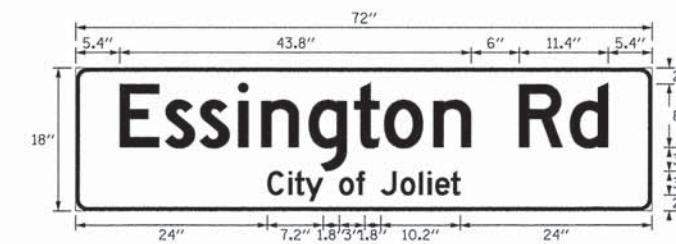
SCHEDULE OF QUANTITIES

PAY ITEM	UNIT	QNTY.
SERVICE INSTALLATION - POLE MOUNTED	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	1452
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	73
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	51
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	508
HANDHOLE	EACH	7
HEAVY-DUTY HANDHOLE	EACH	4
DOUBLE HANDHOLE	EACH	2
TRANSCIVER - FIBER OPTIC	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1360
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	EACH	1741
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1253
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	2628
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	3991
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	222
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	963
TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	1
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	4
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 42 FT.	EACH	2
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 46 FT.	EACH	1
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 54 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	20
CONCRETE FOUNDATION, TYPE C	FOOT	4
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	54
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	6
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	6
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	6
PEDESTRIAN HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8
TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	12
INDUCTIVE LOOP DETECTOR	EACH	14
DETECTOR LOOP, TYPE I	FOOT	875
LIGHT DETECTOR	EACH	2
LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	8
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	9
REMOVE EXISTING DOUBLE HANDHOLE	EACH	2
REMOVE EXISTING CONCRETE FOUNDATION	EACH	5
EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3C	FOOT	322
LED INTERNALLY ILLUMINATED STREET NAME SIGN	EACH	4
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1
CABLE, SPECIAL	FOOT	800
UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1

* THE CABINET SHALL HAVE FRONT AND BACK DOORS AND A SEPERATE CIRCUIT BREAKER FOR STREET LIGHTING.



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D	12.00	LED SNS	N/A	2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D	9.00	LED SNS	N/A	2

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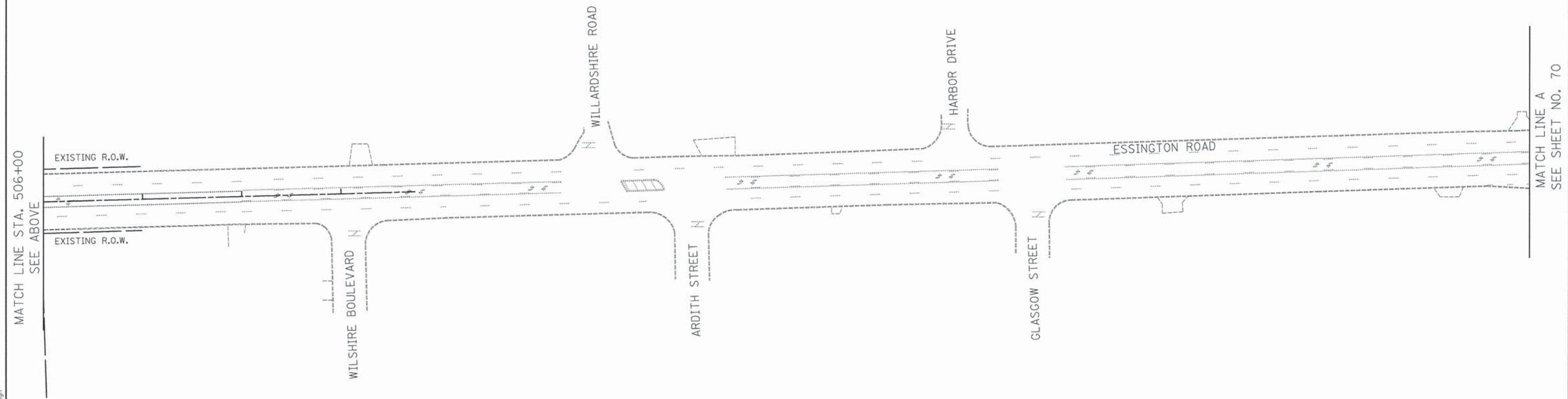
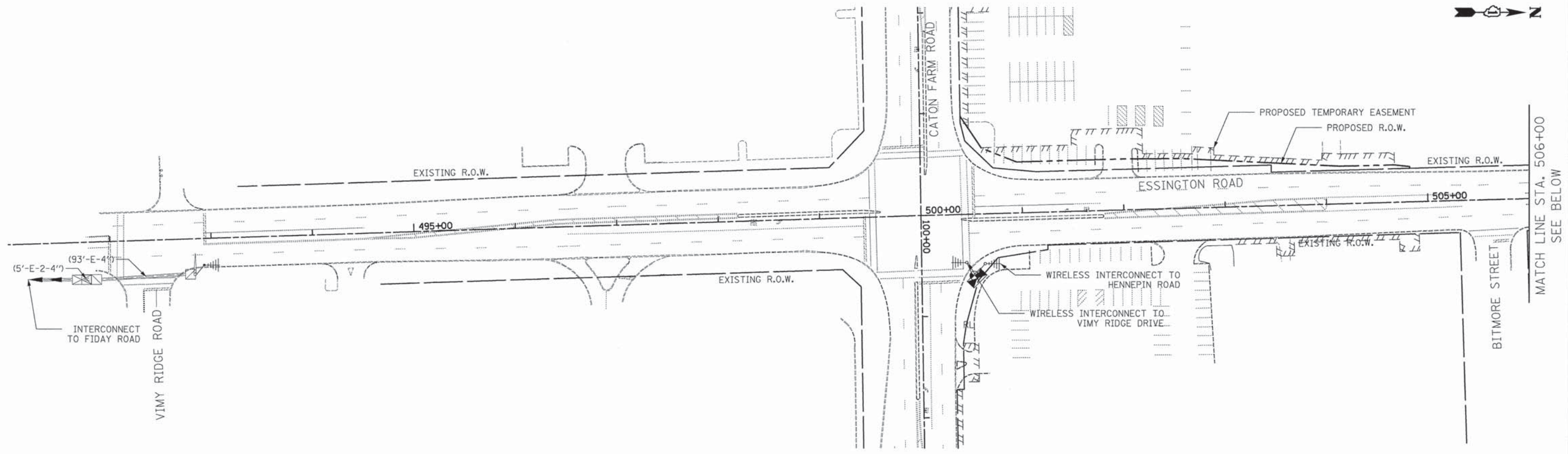
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
QUANTITIES AND STREET NAME SIGNS

NOT TO SCALE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	68
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				



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MATCH LINE STA. 506+00
 SEE ABOVE

MATCH LINE A
 SEE SHEET NO. 70



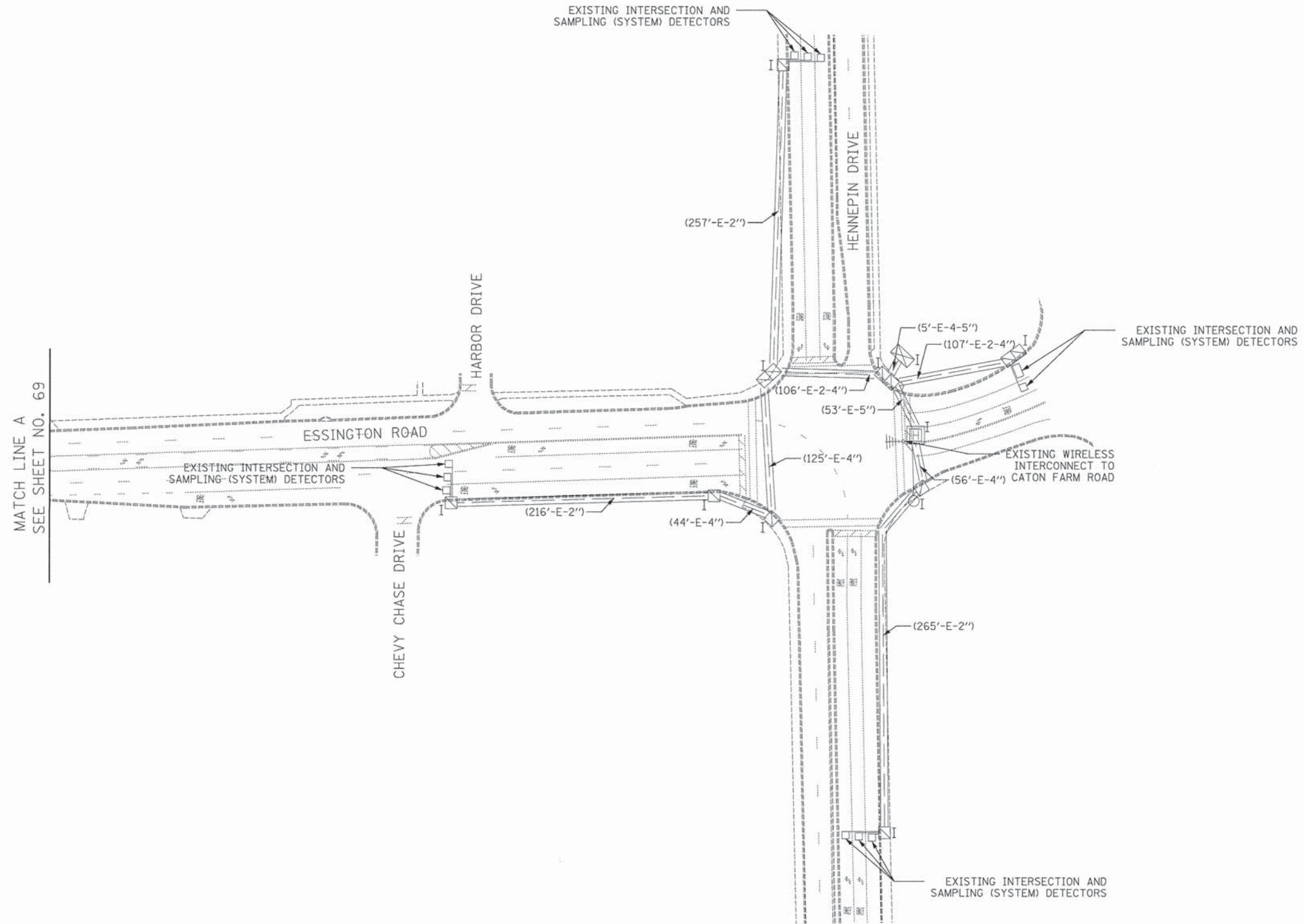
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
TEMPORARY INTERCONNECT PLAN

SCALE: 1" = 50' SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	69
				CONTRACT NO. 61C29
ILLINOIS FED. AID PROJECT M-4003 (254)				



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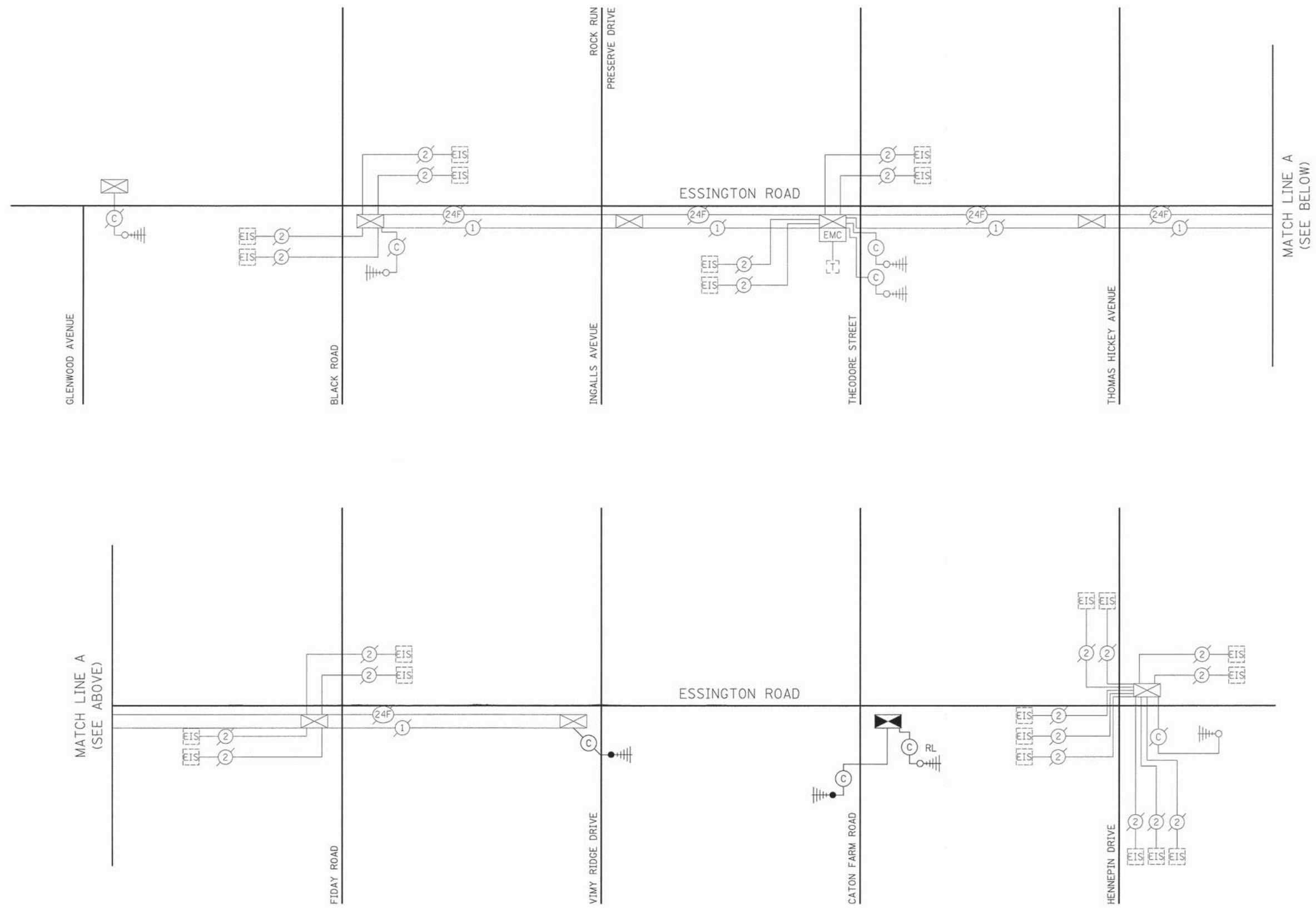
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
TEMPORARY INTERCONNECT PLAN

SCALE: 1" = 50' SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	70
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				



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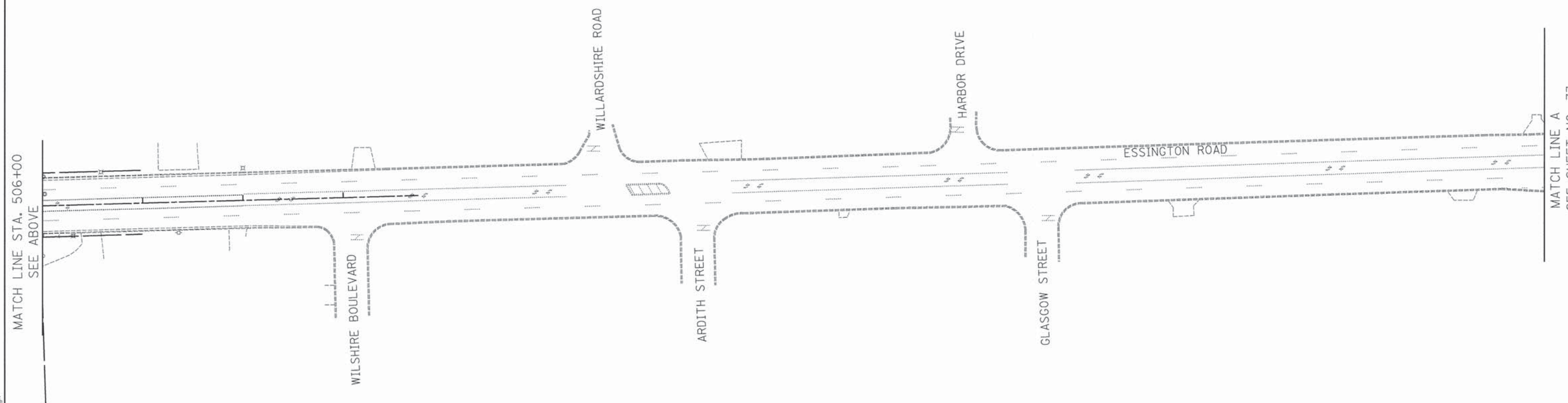
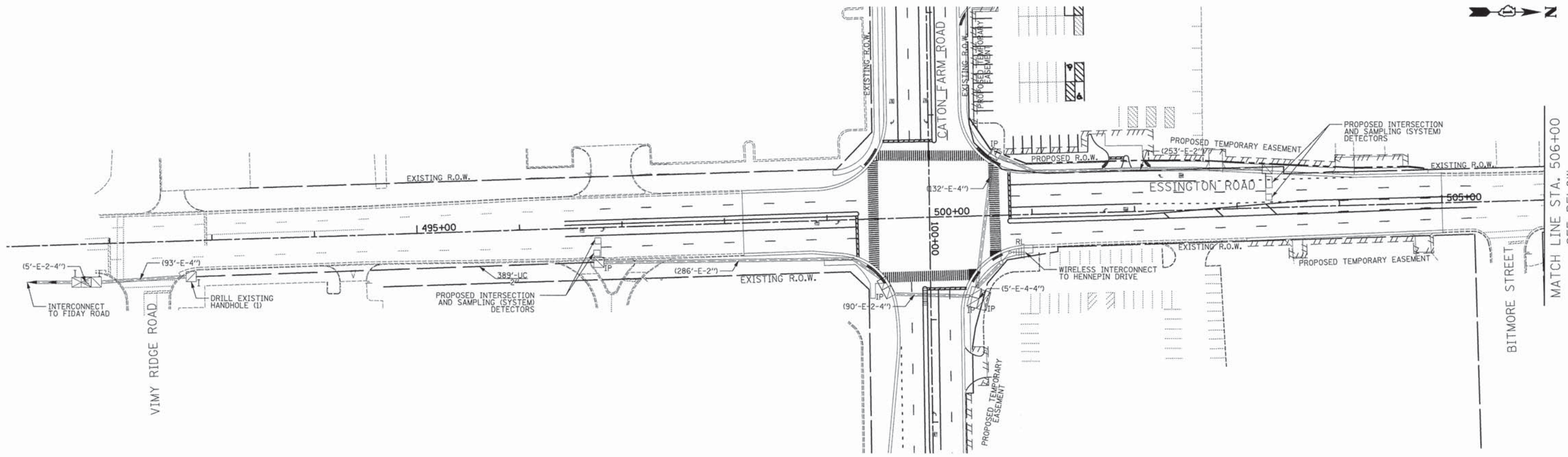
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PLOT DATE = 12/4/2015	DATE - 12/07/15	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TEMPORARY INTERCONNECT SCHEMATIC
 ESSINGTON ROAD**

NOT TO SCALE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	71
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				



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MATCH LINE STA. 506+00
 SEE ABOVE

MATCH LINE A
 SEE SHEET NO. 73

MATCH LINE STA. 506+00
 SEE BELOW



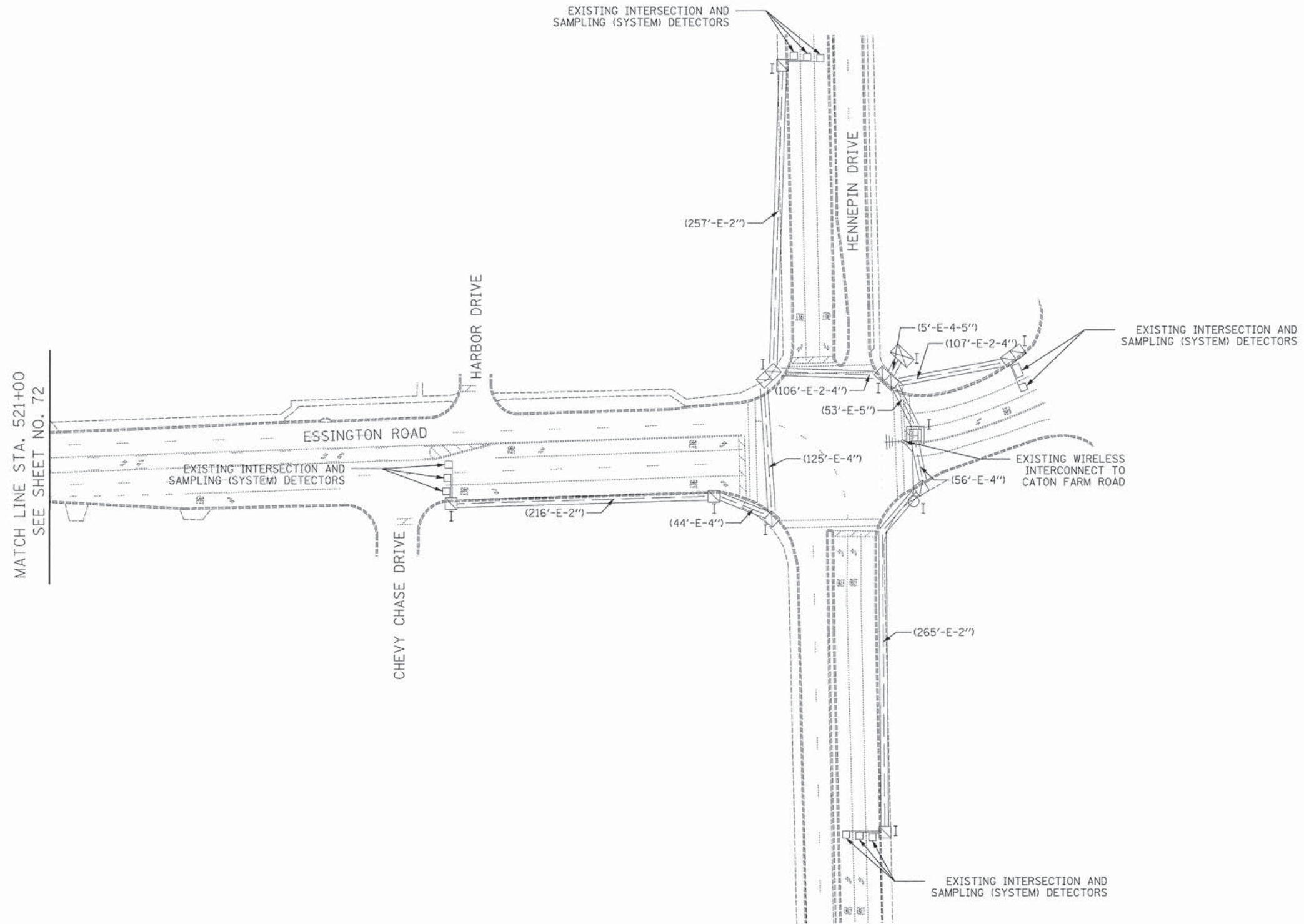
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PLOT DATE = 12/4/2015	DATE - 12/07/15	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CATON FARM ROAD AND ESSINGTON ROAD
 PROPOSED INTERCONNECT PLAN**

SCALE: 1" = 50' SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	72
				CONTRACT NO. 61C29
ILLINOIS FED. AID PROJECT M-4003 (254)				



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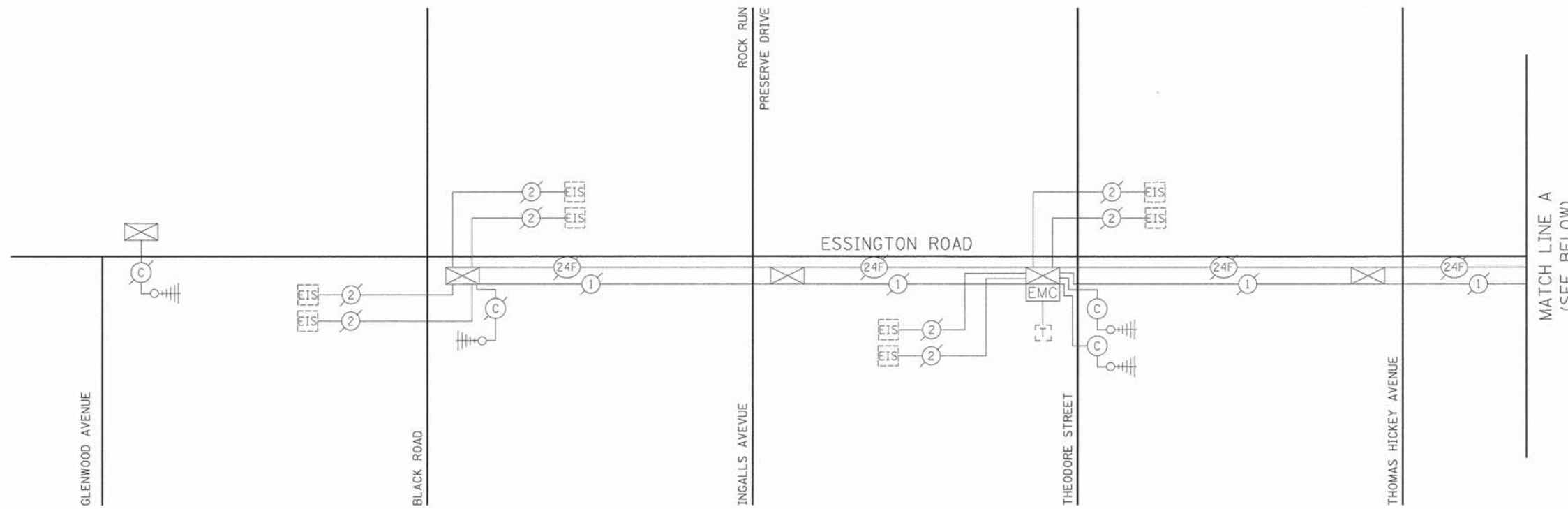
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	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
PROPOSED INTERCONNECT PLAN

SCALE: 1" = 50' SHEET 2 OF 2 SHEETS STA. TO STA.

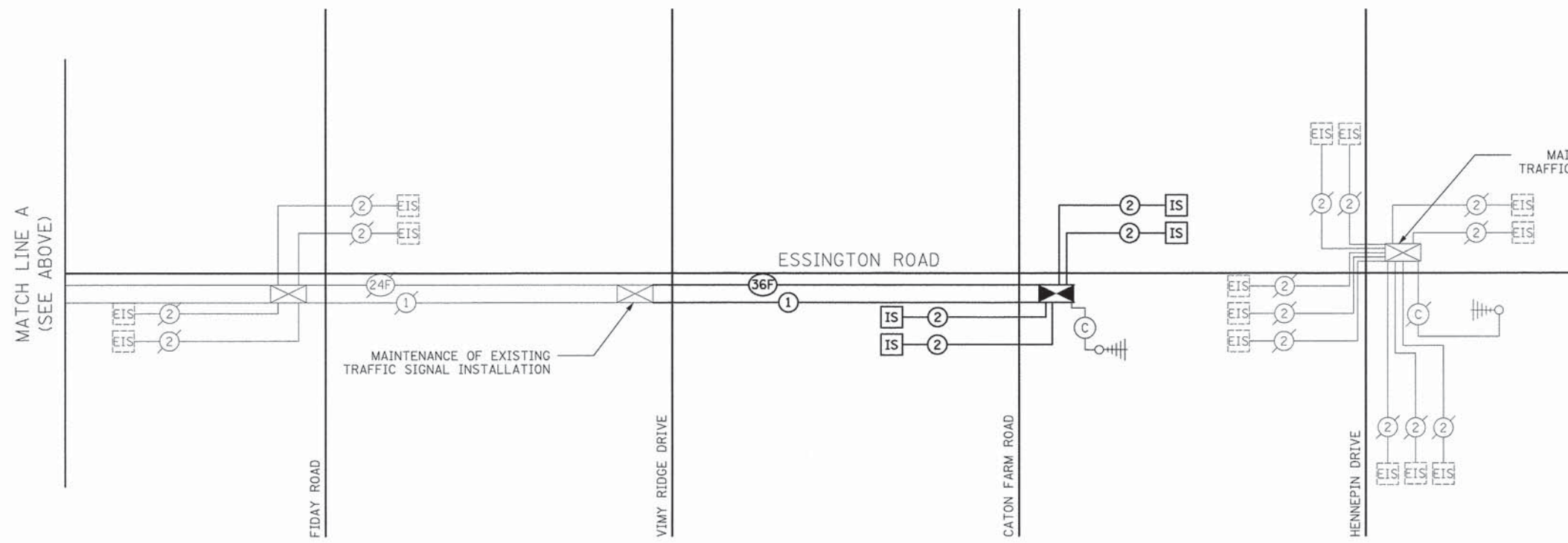
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	73
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				



MATCH LINE A
(SEE BELOW)

INTERCONNECT SCHEDULE OF QUANTITIES

PAY ITEM	UNIT	QNTY.
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	389
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	934
DRILL EXISTING HANDHOLE	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	256
RELOCATE WIRELESS INTERCONNECT (COMPLETE)	L SUM	1
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	957
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2	EACH	1



MAINTENANCE OF EXISTING
TRAFFIC SIGNAL INSTALLATION

MAINTENANCE OF EXISTING
TRAFFIC SIGNAL INSTALLATION

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PLOT DATE = 12/4/2015	CHECKED - BRD	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INTERCONNECT SCHEMATIC
ESSINGTON ROAD

NOT TO SCALE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	74
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	








LIGHTING GENERAL NOTES:

1. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
2. PAY ITEMS IN THE SUMMARY OF QUANTITIES HAVE BEEN ESTIMATED. IF, IN THE ENGINEER'S OPINION, ANY WORK IS NOT REQUIRED, THAT ITEM WILL BE DEDUCTED FROM THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
3. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MARK THE PROPOSED LOCATIONS OF ALL LIGHT POLES FOR EXAMINATION AND CONFIRMATION WITH THE ENGINEER PRIOR TO INSTALLATION OF LIGHTING FOUNDATIONS.
4. RELOCATED LIGHTING UNIT SHALL BE INSTALLED IMMEDIATELY ON THE PROPOSED LIGHTING FOUNDATION AND ALL ELECTRICAL CABLES SHALL BE CONNECTED AND BECOME OPERATIONAL THAT SAME EVENING WITHOUT INTERRUPTION.
5. THE QUANTITY FOR LIGHT POLE FOUNDATION, 24" DIAMETER, OFFSET IS ESTIMATED. IF IT IS DETERMINED IN THE FIELD THAT A LIGHT POLE FOUNDATION, 24" DIAMETER CANNOT BE PLACED, THE PAY ITEM FOR LIGHT POLE FOUNDATION, 24" DIAMETER, OFFSET SHALL BE USED WITH THE APPROVAL OF THE ENGINEER.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ESTABLISHMENT OF FINISHED GRADE. THE ENGINEER MAY ASSIST THE CONTRACTOR, AS APPLICABLE, BUT THE RESPONSIBILITY FOR COORDINATING THE FINISHED GRADE ELEVATION WITH THE INSTALLATION OF CONDUITS, UNIT DUCTS AND THE TOP OF THE FOUNDATION HEIGHTS REMAIN WITH THE CONTRACTOR.
7. ANY DAMAGE TO PAVEMENT, SIDEWALK, CURB, OR ANY OTHER PORTION OF THE ROADWAY NOT SPECIFICALLY TO BE REMOVED AND REPLACED SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST AND REPLACEMENT SHALL MEET THE APPROVAL OF THE ENGINEER.
8. WHEN SPLICING TO EXISTING POLE, ANY AND ALL WORK REQUIRED TO RUN THE PROPOSED UNIT DUCT INTO EXISTING FOUNDATION SLEEVE AND SPLICING IN EXISTING POLE SHALL BE COVERED AND INCLUDED IN THE PAY ITEM FOR THE UNIT DUCT.
9. COORDINATE WITH TRAFFIC SIGNAL PLANS FOR EXACT LOCATION OF COMBINATION POLES. COMBINATION POLES ARE TRAFFIC SIGNAL PAY ITEMS, LUMINAIRES ARE IN PROPOSED LIGHTING PAY ITEMS.
10. EQUIPMENT GROUND CONDUCTORS SHALL BE SPLICED AND BONDED AT EACH LIGHT POLE OR OTHER PIECE OF EQUIPMENT.
11. CONDUIT AND UNIT DUCT MUST BE POSITIONED IN THE FIELD TO AVOID CONFLICT WITH TREES, BUSHES, DRAINS, AND OTHER UTILITIES.
12. ALL DISTURBED AREAS WHERE RESTORATION IS NOT SPECIFICALLY SHOWN ON THE CONTRACT PLANS MUST BE RESTORED BY THE CONTRACTOR AT NO ADDITIONAL COST AND MEET THE APPROVAL OF THE ENGINEER
13. OFFSET CALL-OFFS ARE FROM THE CENTER OF POLES TO EDGE OF PAVEMENT (E.O.P.).

LIGHTING SCHEDULE OF QUANTITIES

CODED PAY ITEM NUMBER	ITEMS	UNIT	TOTAL QUANTITY
81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	385
81028740	UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 1 1/2" DIA.	FOOT	1701
81702130	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6	FOOT	1803
81702300	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 2-1/C NO. 4	FOOT	1803
82102400	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 400 WATT	EACH	4
83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	50
84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	5
89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	1717
X8360215	LIGHT POLE FOUNDATION, 24" DIAMETER, OFFSET	FOOT	30
Z0033028	MAINTENANCE OF LIGHTING SYSTEM	CAL MO	7

LEGEND

-  EXISTING LIGHTING UNIT
-  EXISTING UNIT DUCT
-  RELOCATED EXISTING LIGHTING UNIT
-  PROPOSED COMBINATION TRAFFIC SIGNAL AND LIGHTING
-  PROPOSED UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 1 1/2" DIA.;
PROPOSED ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 2-1/C NO. 4;
PROPOSED ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6
-  PROPOSED CONDUIT (SIZE AS SPECIFIED)
-  REMOVE ELECTRIC CABLE FROM CONDUIT

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	DRAWN - SJC	REVISED -
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PLOT DATE = 12/3/2015	DATE - 12/07/15	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CATON FARM ROAD AND ESSINGTON ROAD
LIGHTING NOTES, LEGEND, AND SCHEDULE OF QUANTITIES,**

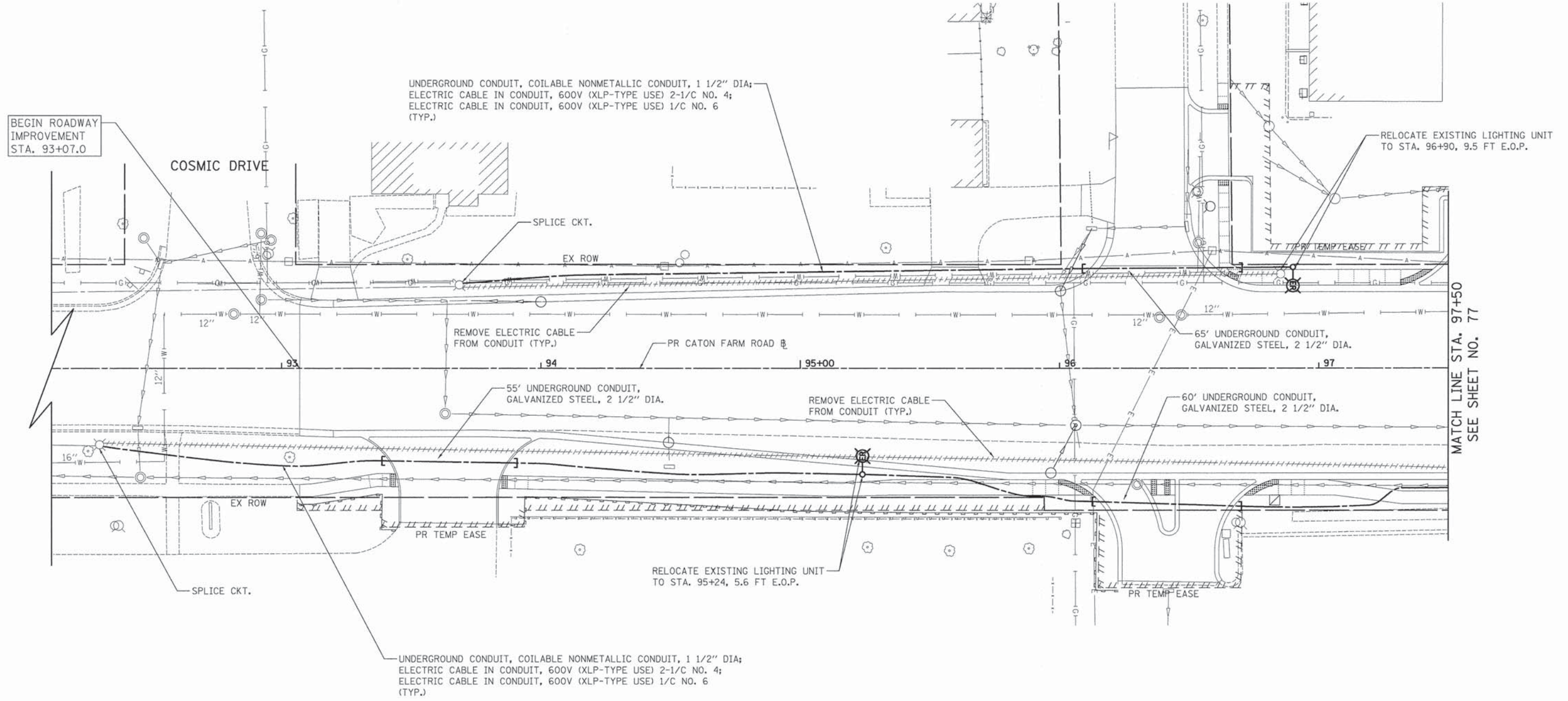
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	75
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

NOTE:
 LIGHTING UNITS ON CANTON FARM ROAD WEST OF ESSINGTON ROAD
 ARE POWERED FROM EXISTING LIGHTING CONTROLLER "B" ON THE
 NORTH SIDE OF CANTON FARM ROAD WEST OF WESTLINE DRIVE.



EASTLINE DRIVE



MATCH LINE STA. 97+50
 SEE SHEET NO. 77

CANTON FARM ROAD

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	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CANTON FARM ROAD
 LIGHTING PLAN

SCALE: 1" = 20' SHEET 1 OF 3 SHEETS STA. TO STA.

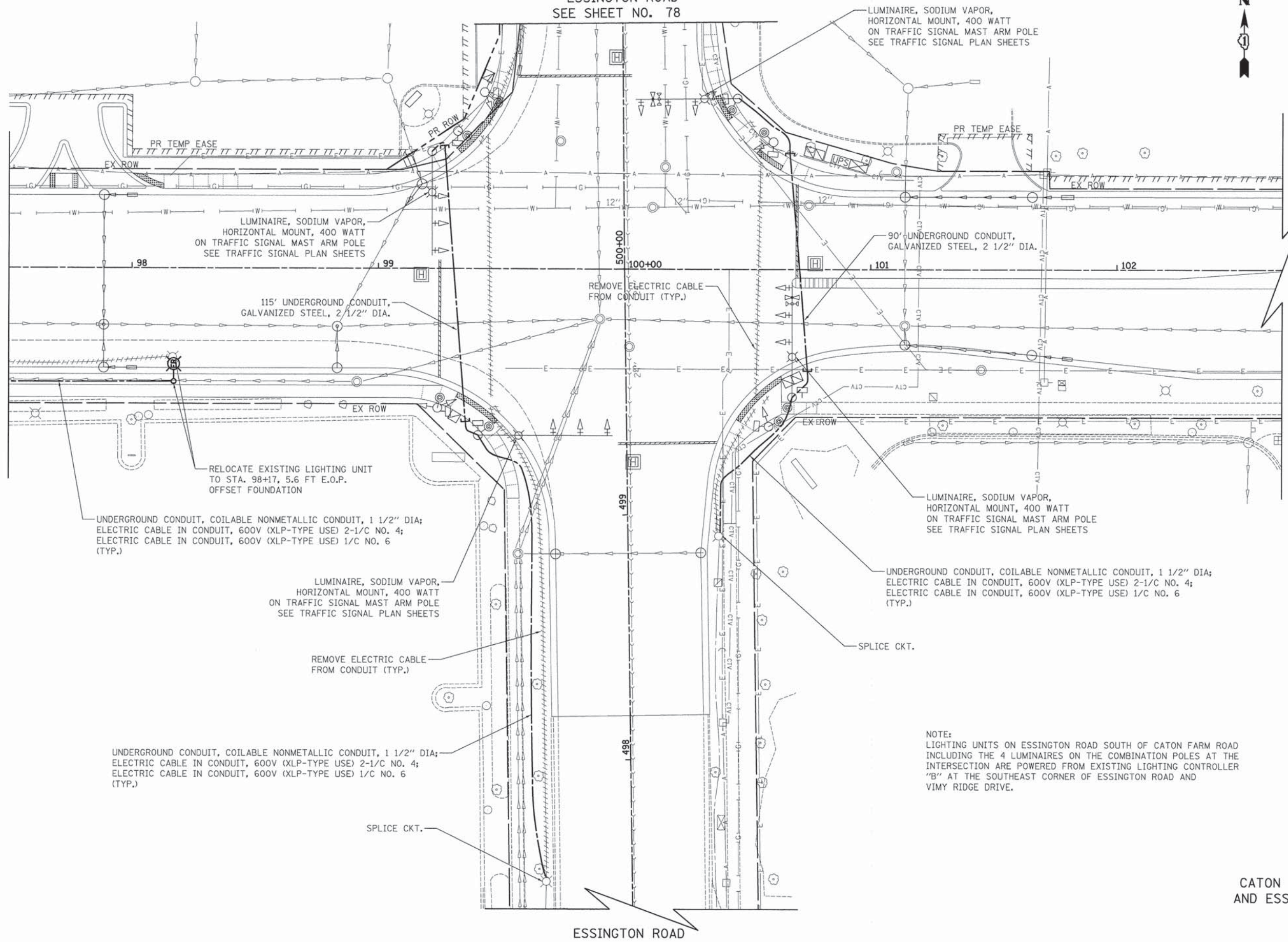
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	76
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

MATCH LINE STA. 501+00
 ESSINGTON ROAD
 SEE SHEET NO. 78



MATCH LINE STA. 97+50
 CATON FARM ROAD
 SEE SHEET NO. 76

CATON FARM ROAD



UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 1 1/2" DIA;
 ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 2-1/C NO. 4;
 ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6 (TYP.)

UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 1 1/2" DIA;
 ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 2-1/C NO. 4;
 ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6 (TYP.)

UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 1 1/2" DIA;
 ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 2-1/C NO. 4;
 ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6 (TYP.)

NOTE:
 LIGHTING UNITS ON ESSINGTON ROAD SOUTH OF CATON FARM ROAD INCLUDING THE 4 LUMINAIRES ON THE COMBINATION POLES AT THE INTERSECTION ARE POWERED FROM EXISTING LIGHTING CONTROLLER "B" AT THE SOUTHEAST CORNER OF ESSINGTON ROAD AND VIMY RIDGE DRIVE.

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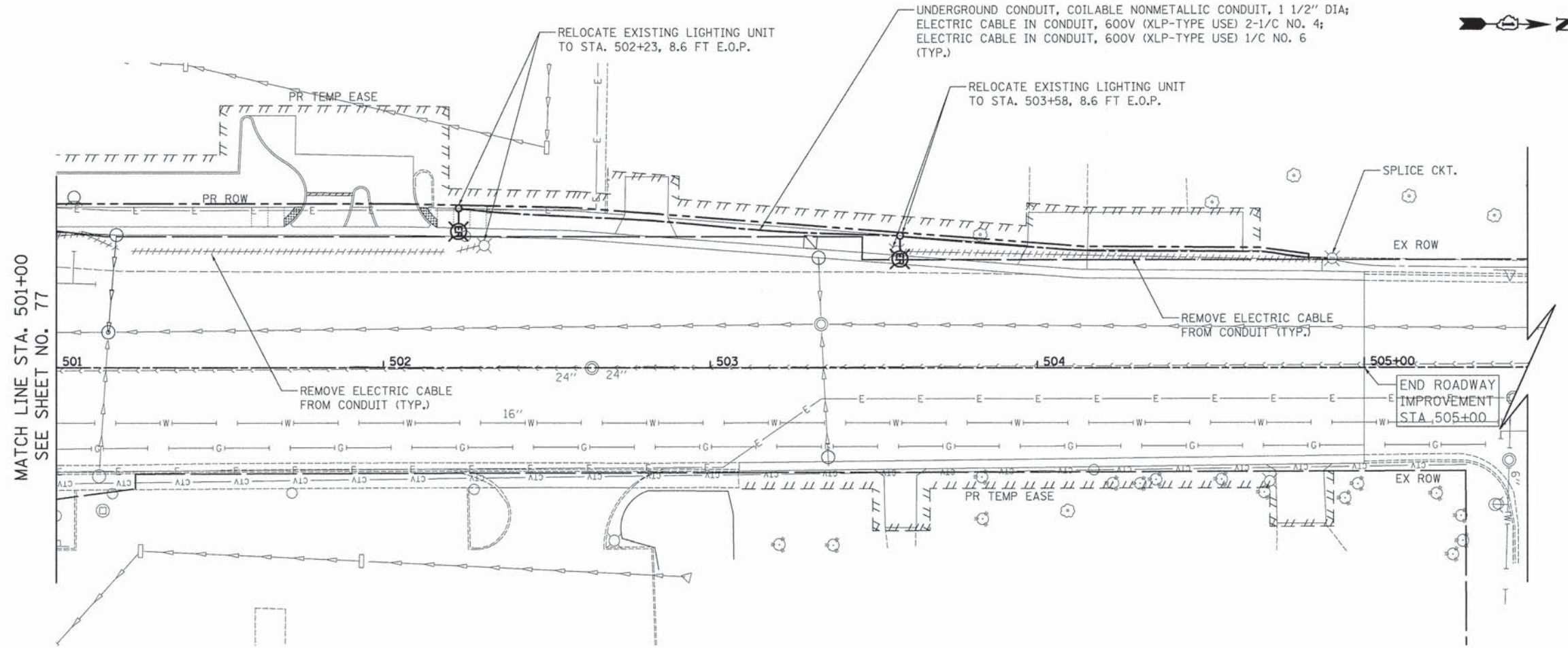
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
 LIGHTING PLAN

SCALE: 1" = 20' SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	77
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

NOTE:
 LIGHTING UNITS ON ESSINGTON ROAD NORTH OF CATON FARM ROAD
 ARE POWERED FROM THE EXISTING LIGHTING CONTROLLER "A"
 ON THE WEST SIDE OF ESSINGTON ROAD NORTH OF ARDITH STREET.



ESSINGTON ROAD

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DATE - 12/07/15	REVISOR -	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

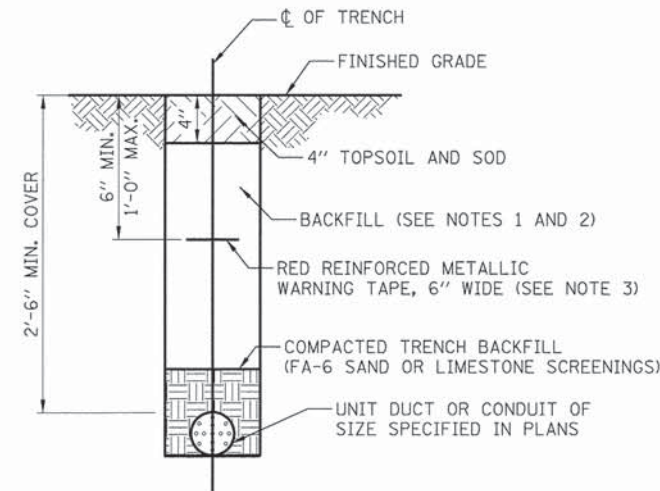
ESSINGTON ROAD
 LIGHTING PLAN

SCALE: 1" = 20' SHEET 3 OF 3 SHEETS STA. TO STA.

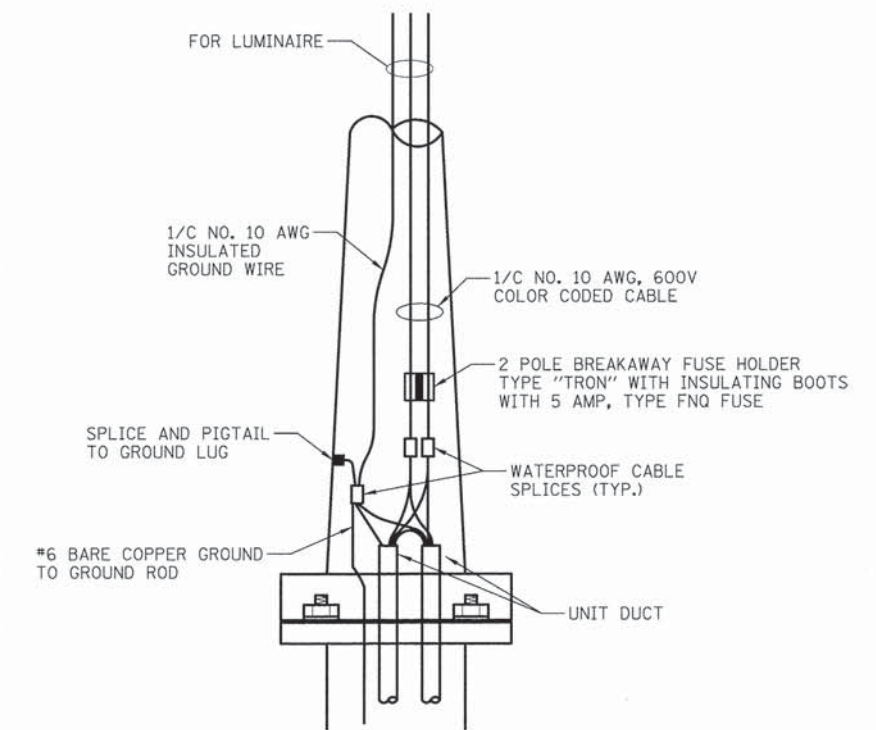
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292	11-00446-00-CH	WILL	110	78
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				

NOTES:

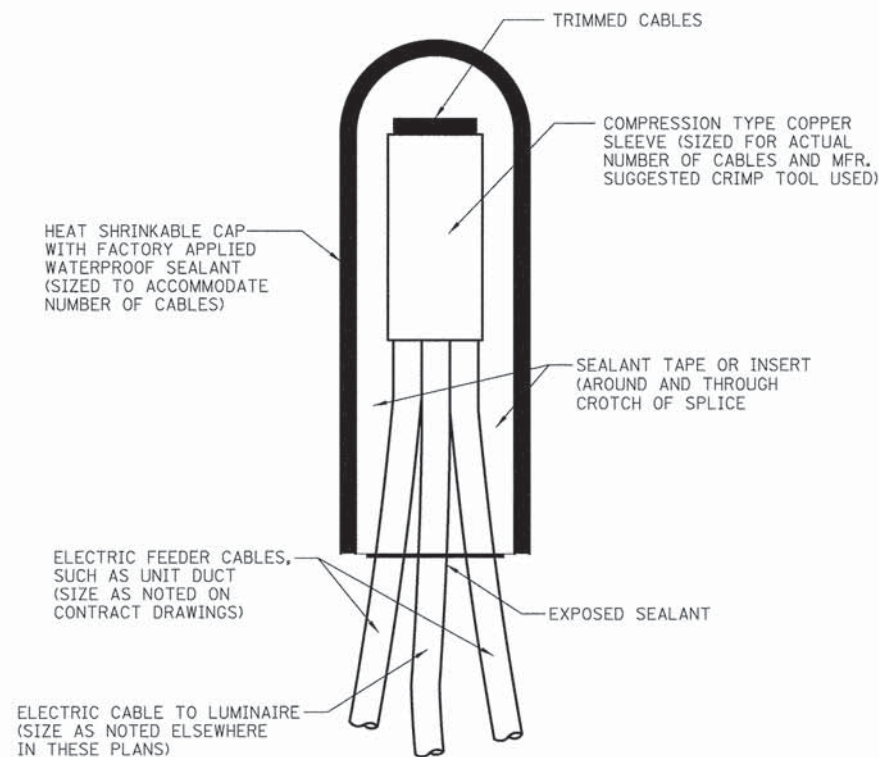
1. IN GRASS COVERED AREAS, THE BACKFILL MAY BE COMPACTED EARTH.
2. TRENCHES WITHIN 2' OF PROPOSED OR EXISTING STREETS, DRIVEWAYS, OR SIDEWALKS WILL BE BACKFILLED WITH COMPACTED FA-6 SAND OR LIMESTONE SCREENINGS.
3. WARNING TAPE WILL BE RED WITH BLACK LETTERING TO READ "CAUTION - ELECTRIC LINE BURIED BELOW".
4. ALL GRASS COVERED AREAS DISTURBED DURING CONSTRUCTION WILL BE RESTORED WITH 4" OF TOPSOIL AND SOD.



TYPICAL TRENCH CROSS SECTION

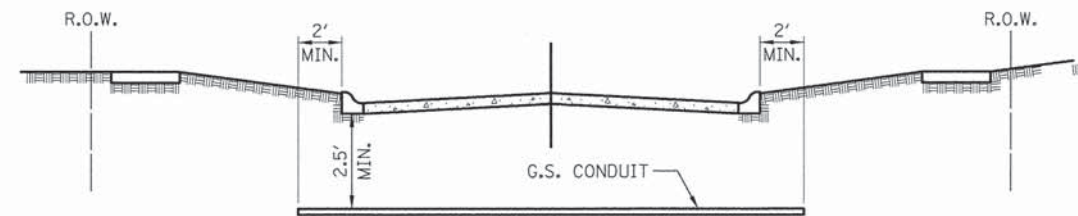


ROADWAY POLE HANDHOLE WIRING DIAGRAM



NOTE: NUMBER OF CABLES IN SPLICE MAY VARY

**SPLICING ELECTRIC CABLES
BASIC MATERIALS AND METHODS**



- ① CONDUIT SHALL BE HEAVY WALL RIGID G.S. CONDUIT.
- ② CONDUIT SHALL EXTEND A MINIMUM OF 2 FT. BEYOND BACK OF CURB.
- ③ CONDUIT SHALL BE A MINIMUM OF 2.5 FT. BELOW BOTTOM OF CURB.

ELECTRICAL CONDUIT UNDER PAVEMENT

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

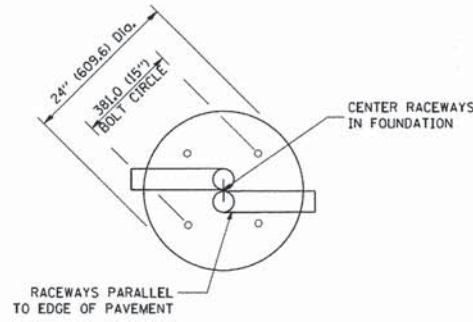
CATON FARM ROAD AND ESSINGTON ROAD
LIGHTING DETAILS

SCALE: N.T.S. SHEET 1 OF 2 SHEETS STA. TO STA.

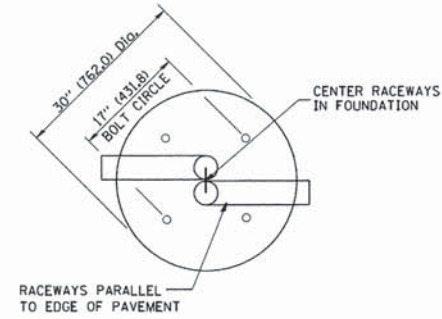
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	79
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				

LIGHT POLE FOUNDATION DEPTH TABLE
40 FT. (12.192 m) TO 47.5 FT. (14.478 m) MOUNTING HEIGHT

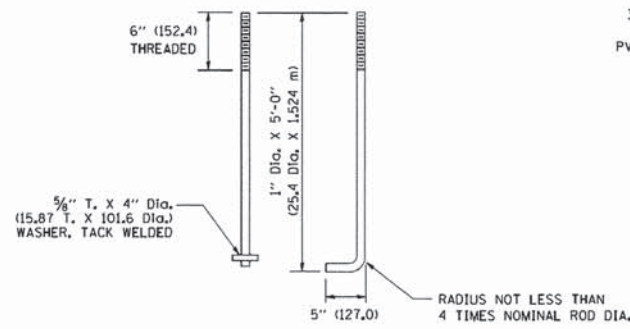
SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION	
	SINGLE ARM POLE	TWIN ARM POLE
SOFT CLAY O _u = 0.375 TON/SQ. FT.	13'-0" (3.96 m)	15'-0" (4.57 m)
MEDIUM CLAY O _u = 0.75 TON/SQ. FT.	9'-6" (2.93 m)	10'-9" (3.23 m)
STIFF CLAY O _u = 1.50 TON/SQ. FT.	7'-0" (2.13 m)	8'-0" (2.44 m)
LOOSE SAND φ = 34°	9'-0" (2.74 m)	10'-0" (3.05 m)
MEDIUM SAND φ = 37.5°	8'-3" (2.52 m)	9'-0" (2.74 m)
DENSE SAND φ = 40°	7'-9" (2.36 m)	9'-0" (2.74 m)



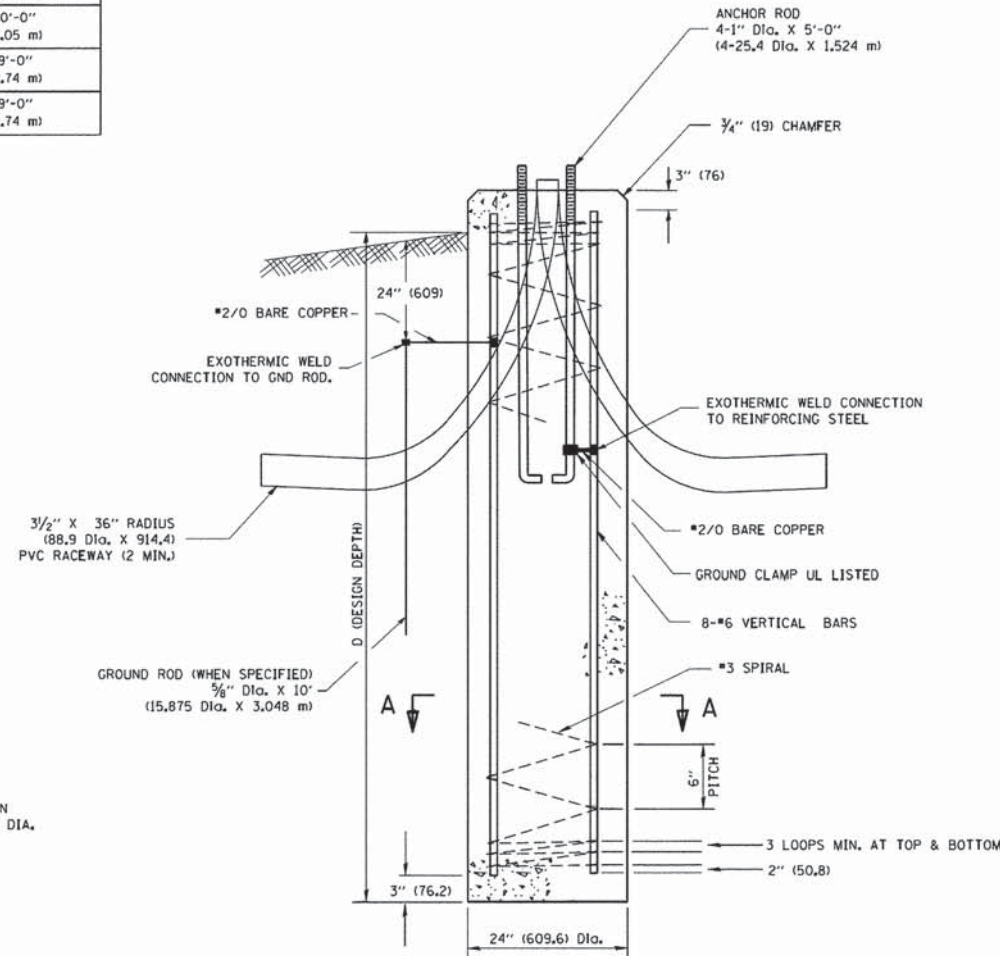
TOP VIEW



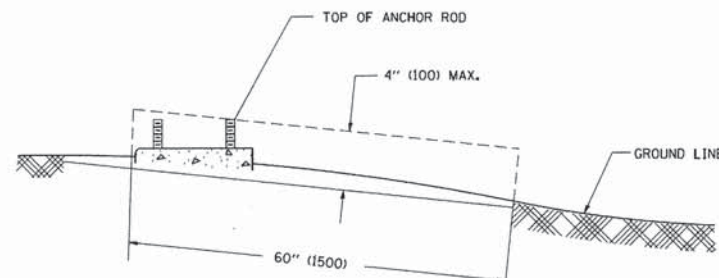
TOP VIEW



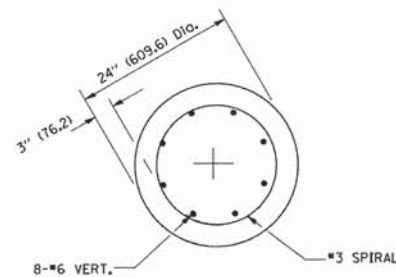
ANCHOR ROD DETAIL



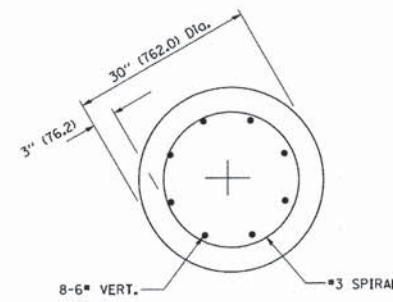
FOUNDATION DETAIL



FOUNDATION EXTENSION DETAIL



SECTION A-A



SECTION A-A

NOTES

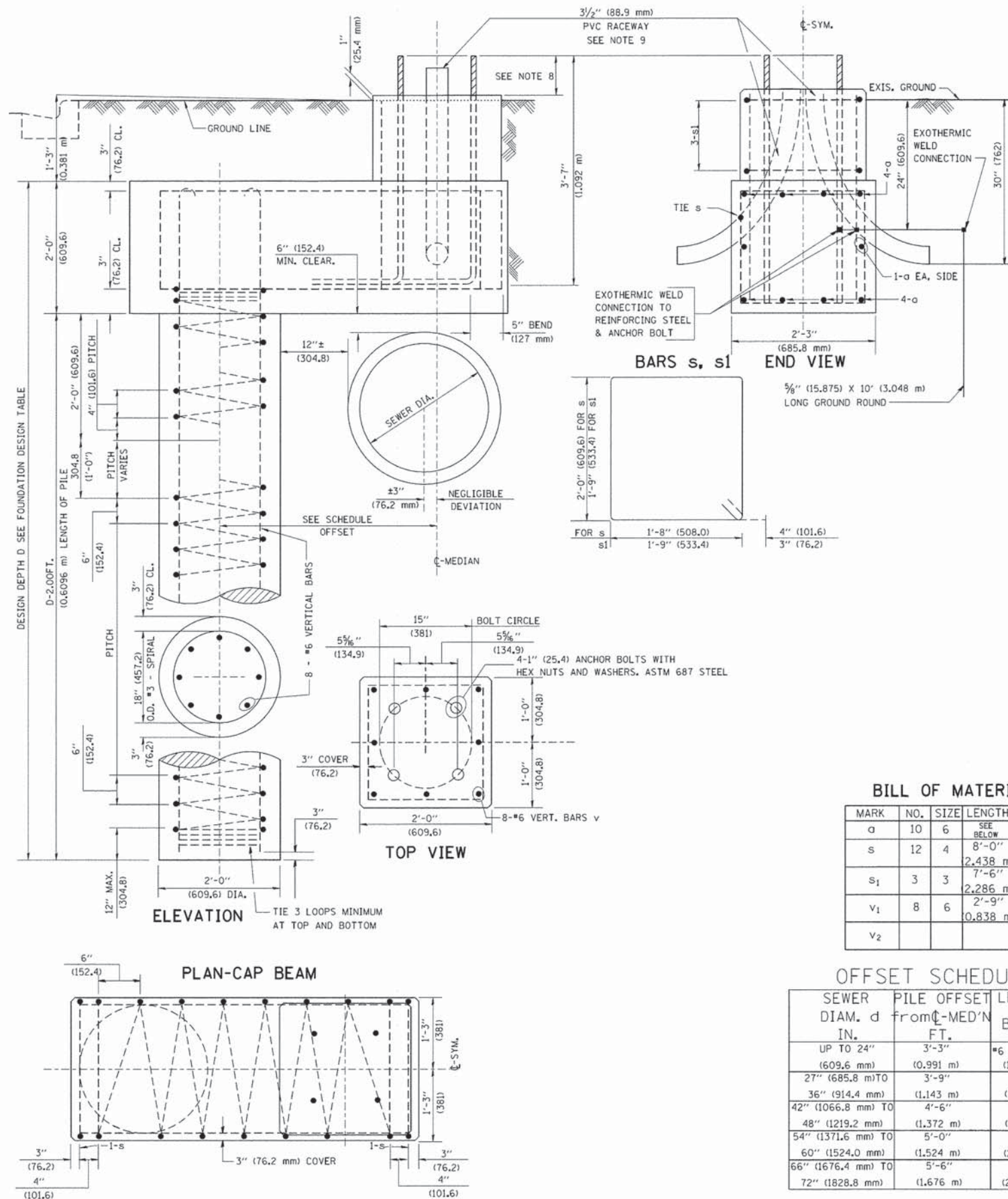
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 100MM (4 IN.) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3/4-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS SI. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UMG MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.

FOUNDATION DESIGN TABLE

TYPE OF SOIL	DESIGN DEPTH OF FOUNDATION		REINFORCEMENT IN FOUNDATION			
	SINGLE ARM D	TWIN ARM D	SINGLE ARM		TWIN ARM	
			VERT BARS	SPIRAL	VERT BARS	SPIRAL
SOFT CLAY	13'-0" (3.962 m)	15'-0" (4.572 m)	8-#6X12'-6" (3.810 m)	#3X122' (37.186 m)	8-#6X14'-3" (4.343 m)	#3X141' (42.977 m)
MEDIUM CLAY	9'-6" (2.896 m)	10'-9" (3.277 m)	8-#6X9'-0" (2.743 m)	#3X90' (27.432 m)	8-#6X10'-0" (3.048 m)	#3X100' (30.480 m)
STIFF CLAY	7'-0" (2.134 m)	8'-0" (2.438 m)	8-#6X6'-6" (1.981 m)	#3X66' (20.112 m)	8-#6X7'-6" (2.286 m)	#3X76' (23.165 m)
LOOSE SAND	9'-0" (2.743 m)	10'-0" (3.048 m)	8-#6X8'-6" (2.591 m)	#3X85' (25.908 m)	8-#6X9'-6" (2.896 m)	#3X94' (28.651 m)
MEDIUM SAND	8'-3" (2.515 m)	9'-0" (2.743 m)	8-#6X8'-0" (2.438 m)	#3X78' (23.774 m)	8-#6X8'-6" (2.591 m)	#3X85' (25.908 m)
DENSE SAND	7'-9" (2.362 m)	9'-0" (2.743 m)	8-#6X7'-6" (2.286 m)	#3X73' (22.250 m)	8-#6X8'-6" (2.591 m)	#3X85' (25.908 m)
ROCK OR SOLIDIFIED SLAG	5'-0" (1.524 m)	5'-0" (1.524 m)	NONE	NONE	NONE	NONE

NOTES

- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ENGINEER SHALL DETERMINE THE CLASS OF SOIL DURING EXCAVATION AND SELECT THE DESIGN DEPTH OF FOUNDATION FROM THE DESIGN TABLE.
- EXCAVATION OF THE POLE FOUNDATION SHALL BE MADE WITH AN AUGER, 24" (609.6 mm) OR 30" (762.0 mm) IN DIAMETER.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR BOLTS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED IN THE FORM.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- THE CONTRACTOR SHALL COORDINATE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF FOUNDATION WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS. IF LIGHT POLE IS MOUNTED WITHOUT BREAKAWAY DEVICE, ANCHOR BOLTS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE TOP OF THE FOUNDATION. THE CONTRACTOR SHALL CONFIRM ANCHOR BOLT EXTENTION WITH ENGINEER.
- RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.
- THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE LIGHT IS ERECTED.



BILL OF MATERIAL

MARK	NO.	SIZE	LENGTH	SHAPE
a	10	6	SEE BELOW	—
s	12	4	8'-0" (2,438 m)	□
s1	3	3	7'-6" (2,286 m)	□
v1	8	6	2'-9" (0,838 m)	—
v2				

OFFSET SCHEDULE

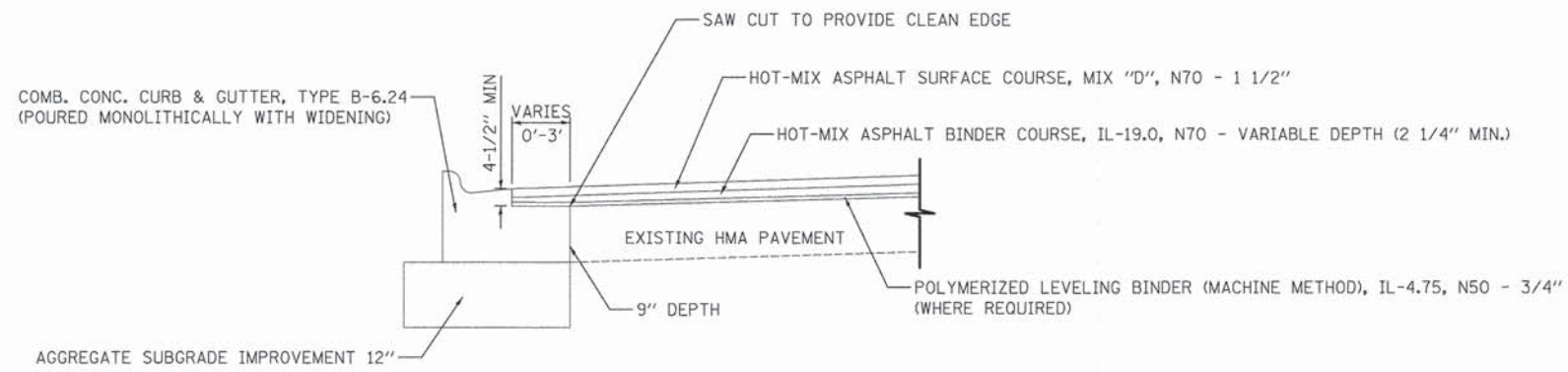
SEWER DIAM. d IN.	PILE OFFSET from C-MED'N FT.	LENGTH of BAR a FT.
UP TO 24" (609.6 mm)	3'-3" (0.991 m)	#6 x 5'-3" (1,600 m)
27" (685.8 mm) TO	3'-9" (1,143 m)	5'-9" (1,753 m)
36" (914.4 mm)	(1,143 m)	6'-6" (1,981 m)
42" (1066.8 mm) TO	4'-6" (1,372 m)	7'-0" (2,134 m)
48" (1219.2 mm)	(1,372 m)	7'-6" (2,286 m)
54" (1371.6 mm) TO	5'-0" (1,524 m)	8'-0" (2,438 m)
60" (1524.0 mm)	(1,524 m)	8'-6" (2,591 m)
66" (1676.4 mm) TO	5'-6" (1,676 m)	9'-0" (2,743 m)
72" (1828.8 mm)	(1,676 m)	(2,896 m)

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		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LIGHT POLE FOUNDATION OFFSET 40" (12.192 m) TO 47 1/2" (14.478 m) M.H. 15" (381 mm) BOLT CIRCLE			
SCALE:	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE. 292	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 81
BE-310		CONTRACT NO. 61C29		
ILLINOIS FED. AID PROJECT M-4003 (254)				



COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (MODIFIED)
AT LOCATIONS WHERE WIDENING IS 3' OR LESS

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FILE NAME = ...\\19-Detail\2807-ah-1-detail.dgn



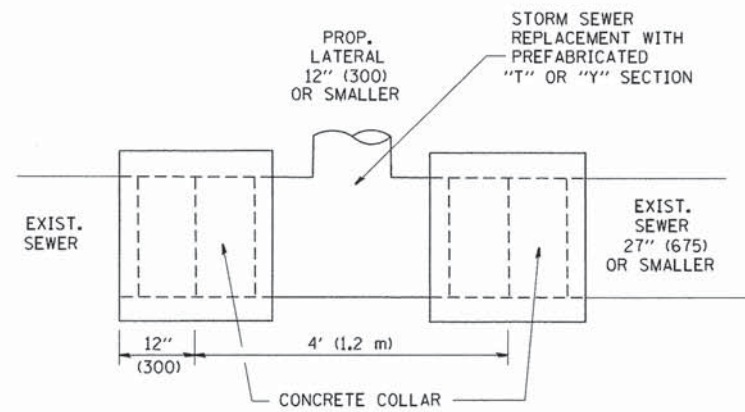
USER NAME = tgb	DESIGNED - EDS	REVISED -
PLOT SCALE = 1.0000' / in.	DRAWN - EDS	REVISED -
PLOT DATE = 12/3/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD AND ESSINGTON ROAD
CONSTRUCTION DETAILS

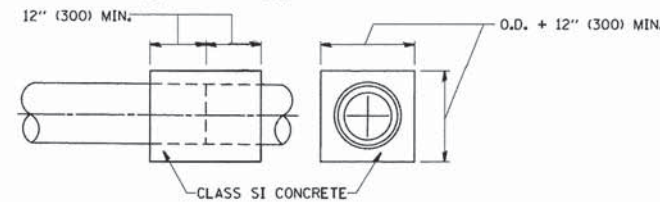
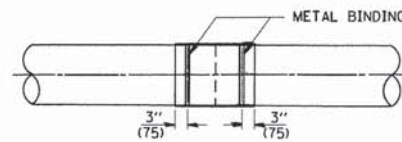
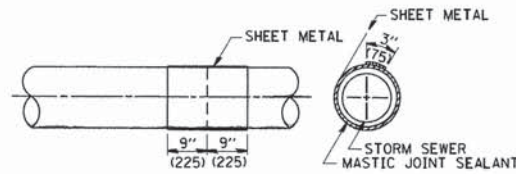
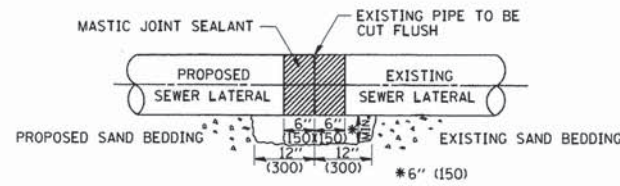
SCALE: N.T.S. SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	82
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

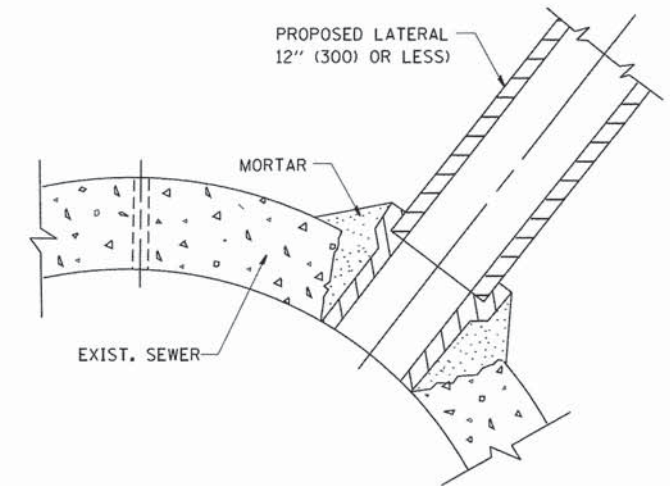


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6' (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

1. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

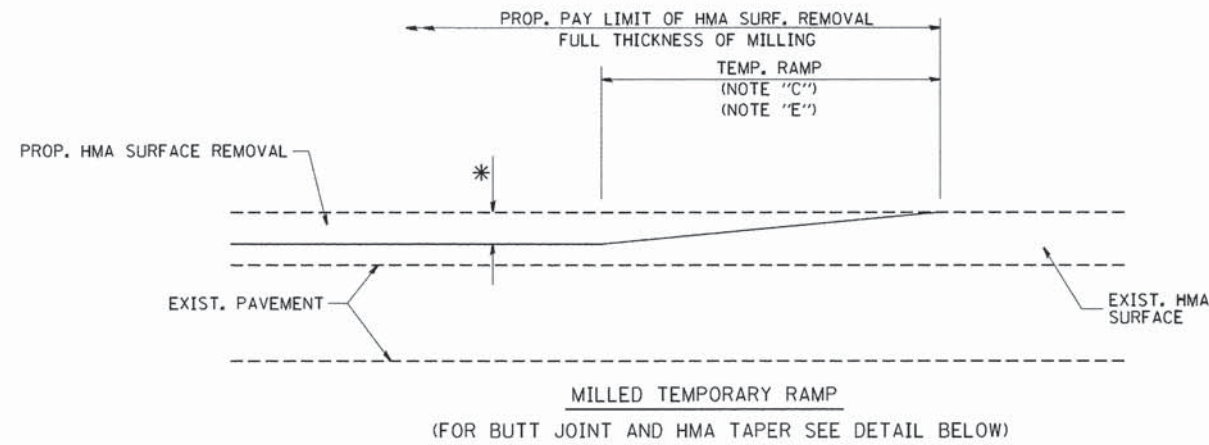


USER NAME = tgb	DESIGNED - M. DE YONG	REVISED - M. DE YONG 05-08-92
PLOT SCALE = 50,000' / IN.	DRAWN -	REVISED - R. SHAH 09-09-94
PLOT DATE = 12/3/2015	CHECKED -	REVISED - R. SHAH 10-25-94
	DATE - 07-25-90	REVISED - R. SHAH 06-12-96

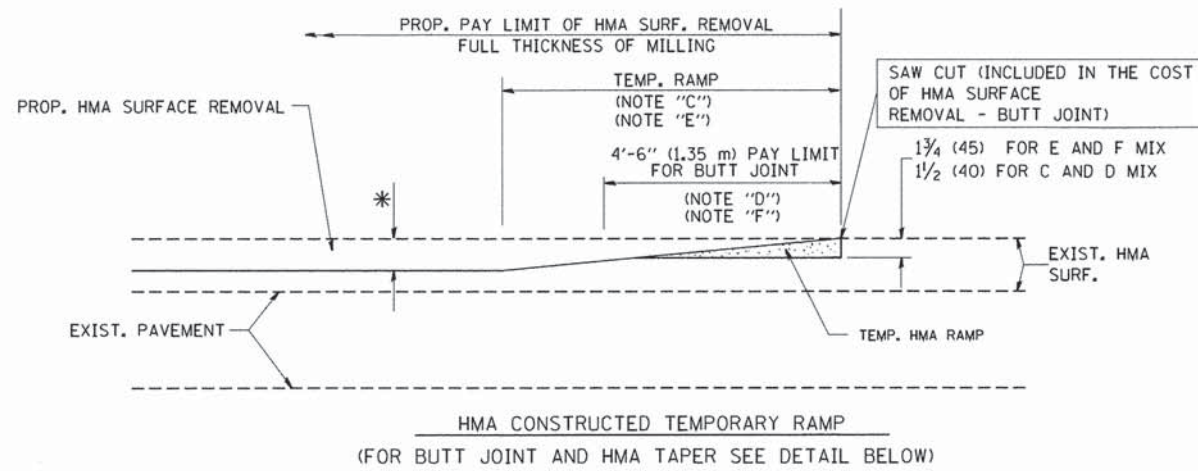
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	83
BD500-01 (BD-7)			CONTRACT NO. 61C29	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-4003 (254)				

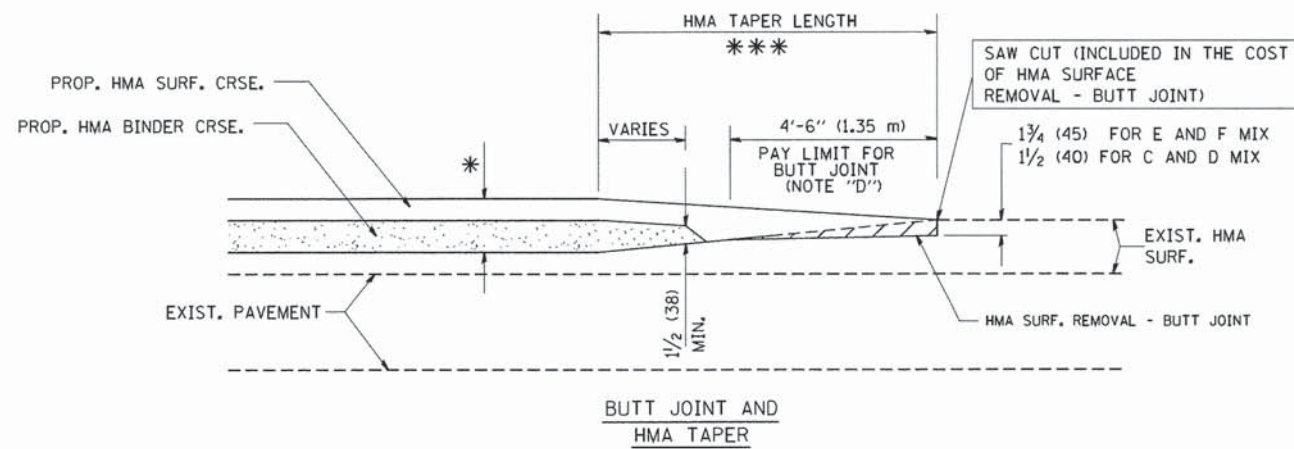


OPTION 1

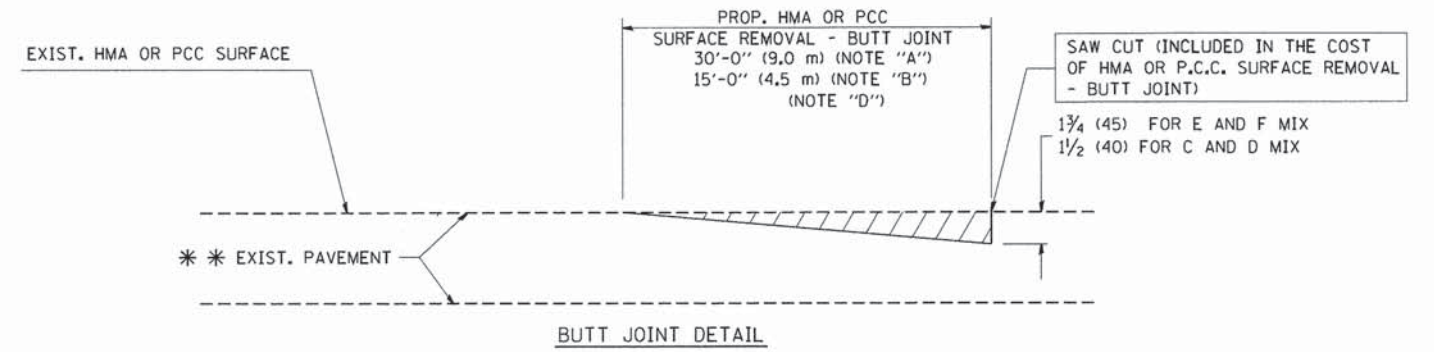


OPTION 2

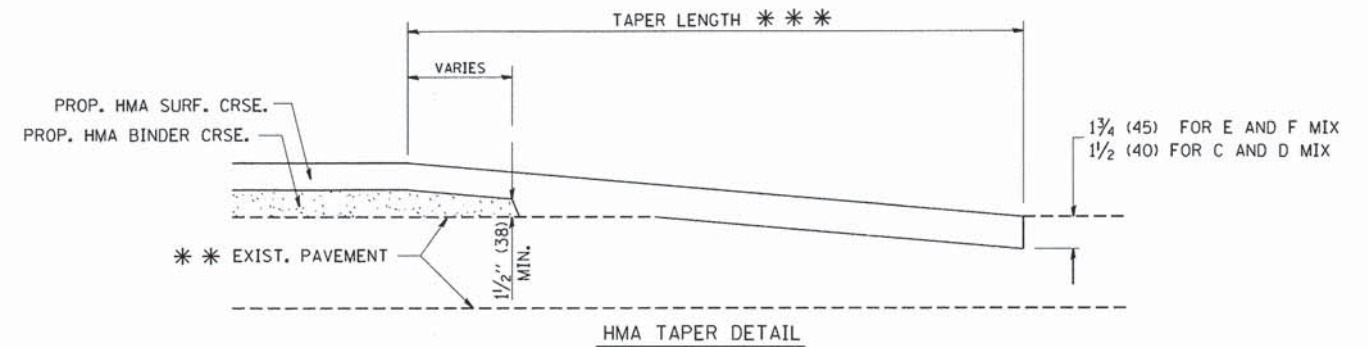
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

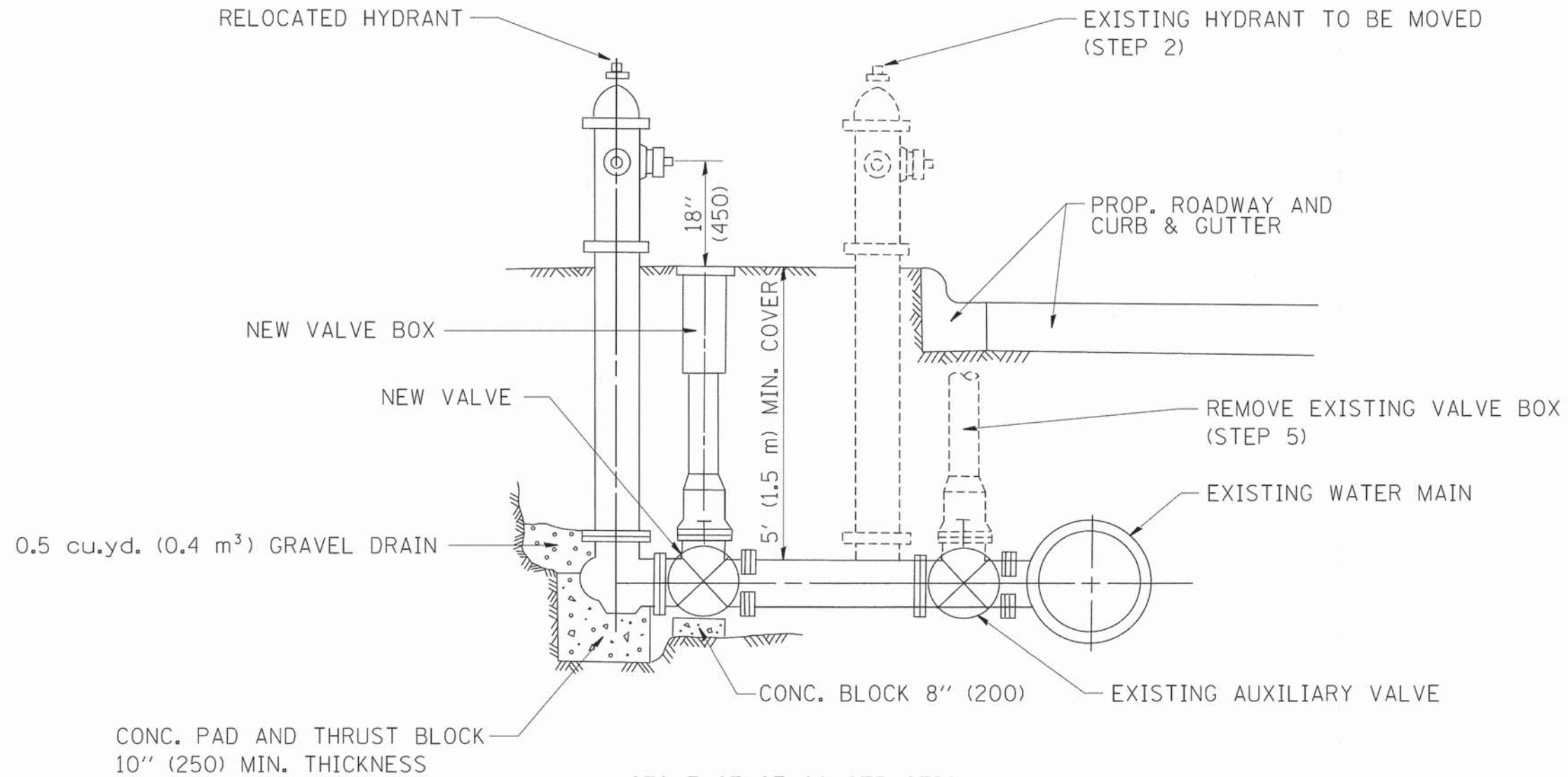


USER NAME = tgb	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 50,0000 "/>		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	85
BD400-05 BD32		CONTRACT NO. 61C29		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (254)				




SEQUENCE OF CONSTRUCTION:

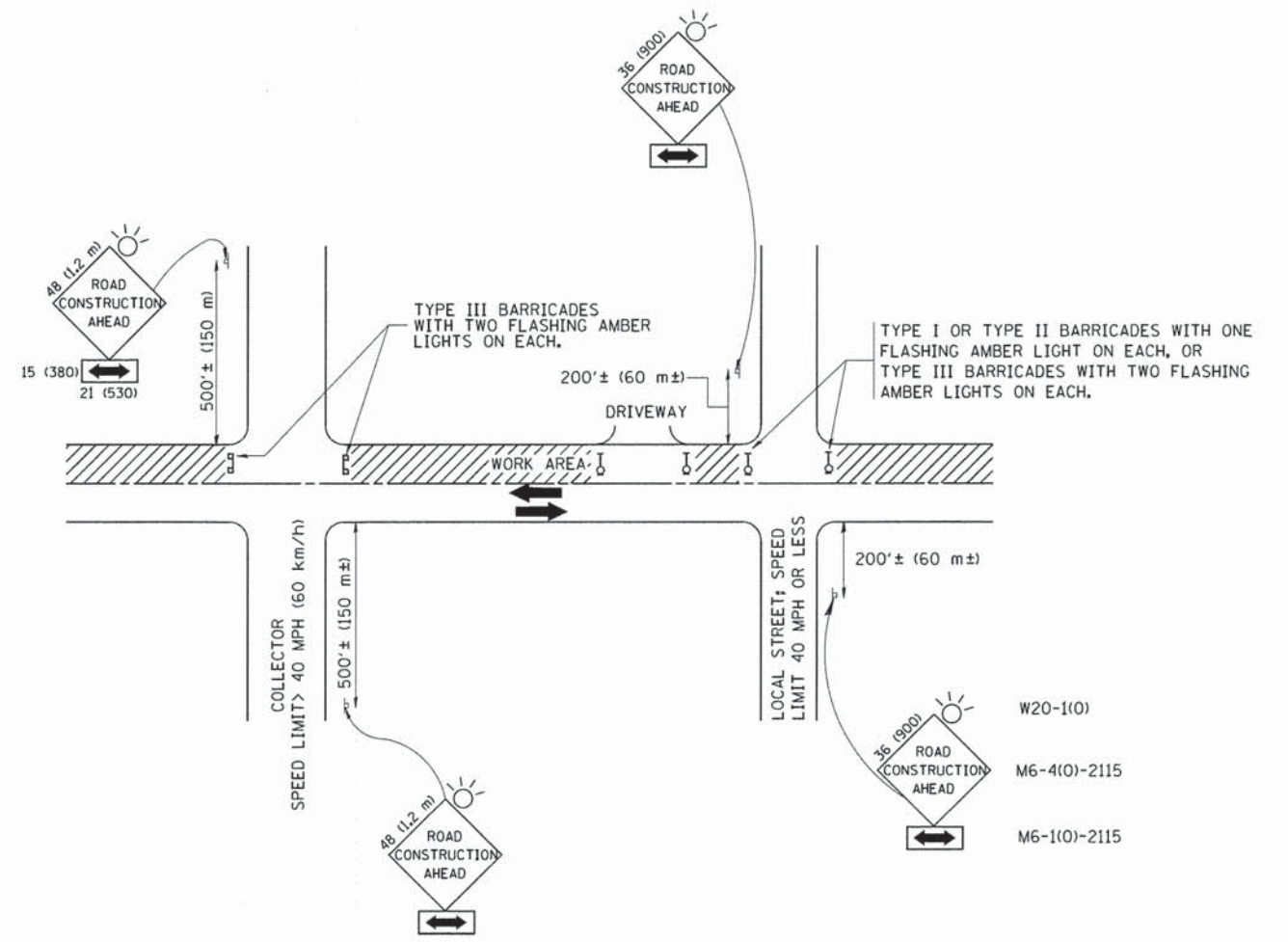
1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

	USER NAME = tgb	DESIGNED -	REVISED - R. SHAH 09-09-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FIRE HYDRANT TO BE MOVED			F.A. U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50,0000' / IN.	DRAWN -	REVISED - R. SHAH 10-25-94		292	11-00446-00-CH	WILL	110	86			
PLOT DATE = 12/3/2015	CHECKED -	REVISED -	REVISED -	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD-36		CONTRACT NO. 61C29		
	DATE -	REVISED -	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (254)								



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1,2 m x 1,2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

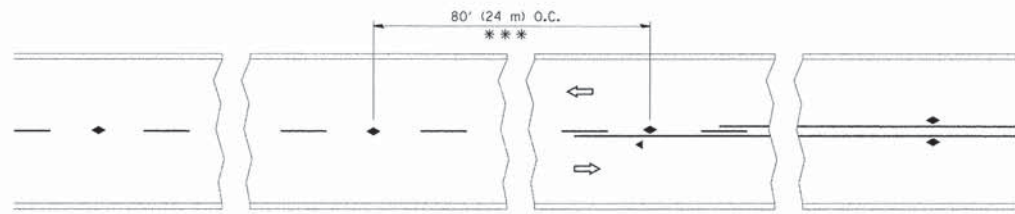
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 12/3/2015	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

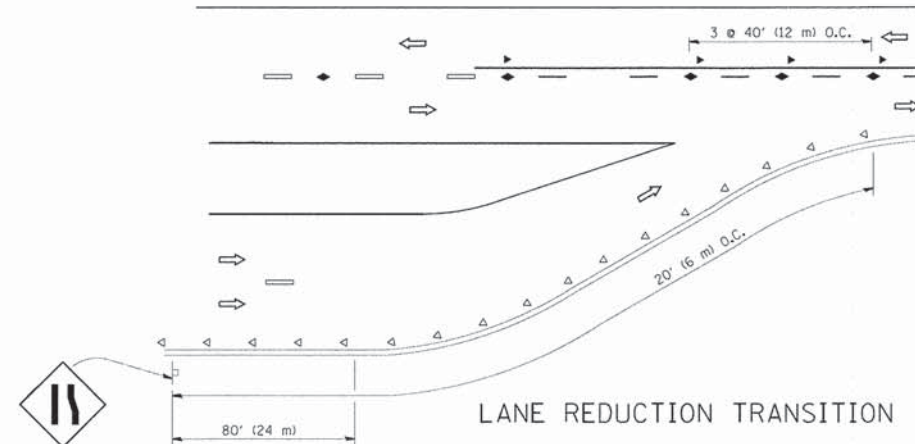
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	87
TC-10			CONTRACT NO. 61C29	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (254)				

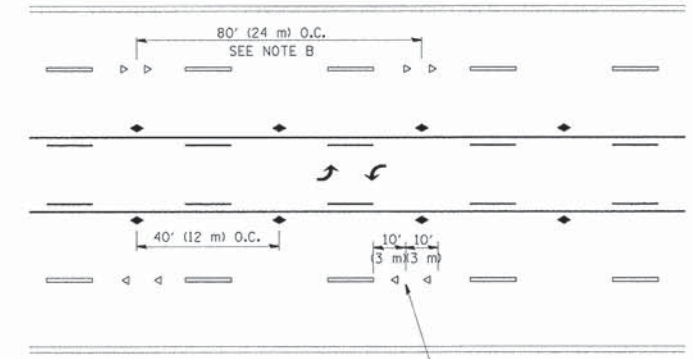


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

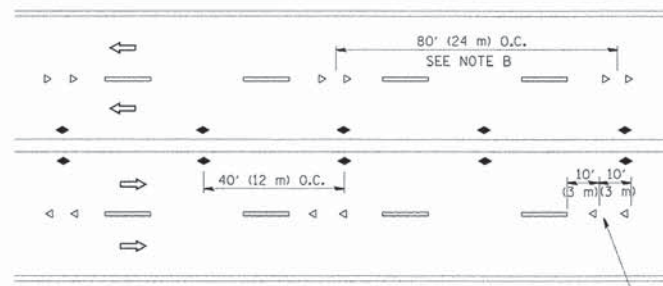
TWO-LANE/TWO-WAY



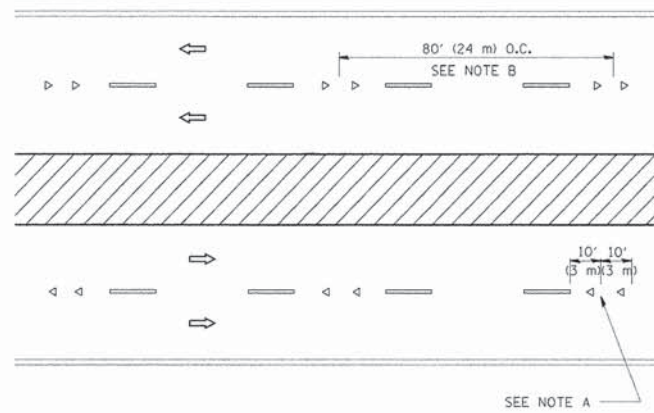
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

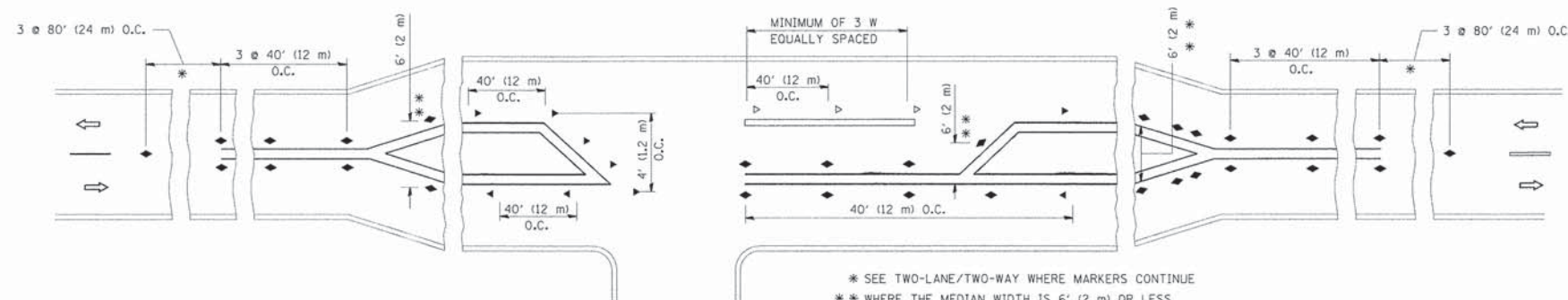
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

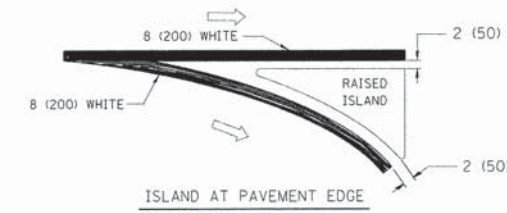
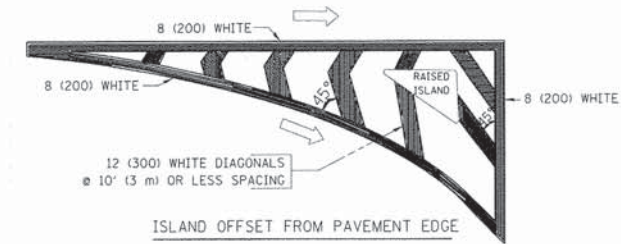
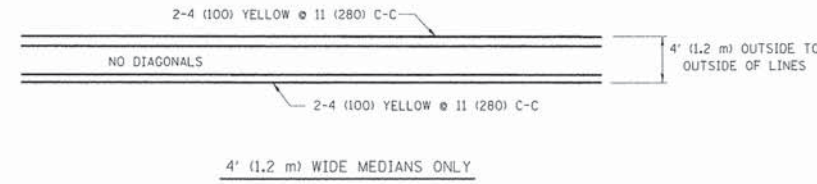
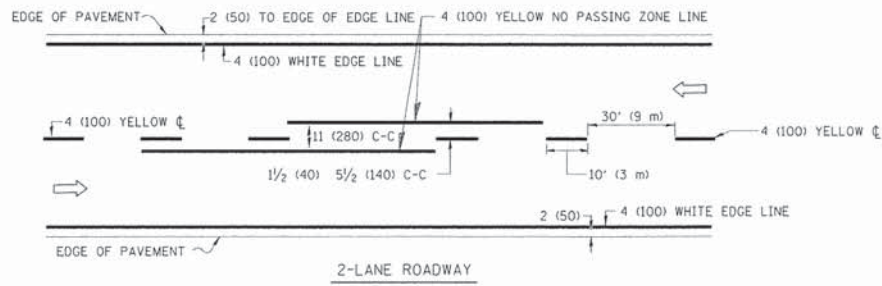


* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

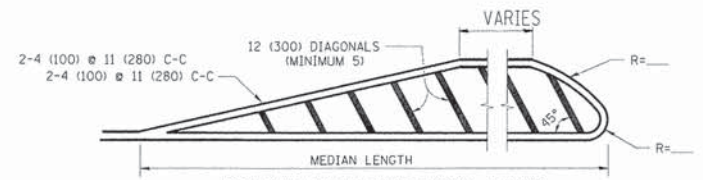
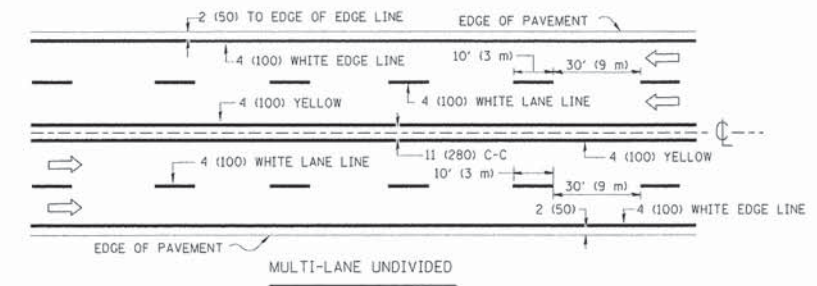
LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

	USER NAME = tgb	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 5/8" = 1' / IN.	DRAWN -	REVISED - T. RAMMACHER 03-12-99		292	11-00446-00-CH	WILL	110	88		
	PLOT DATE = 12/3/2015	CHECKED -	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.		TC-11		CONTRACT NO. 61C29		
	DATE -	REVISED - C. JUCIUS 09-09-09			FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-4003 (254)						

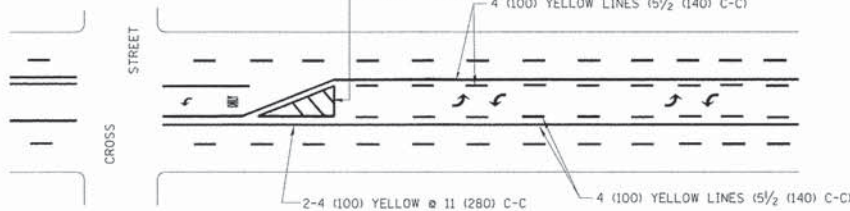


TYPICAL ISLAND MARKING

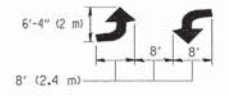


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

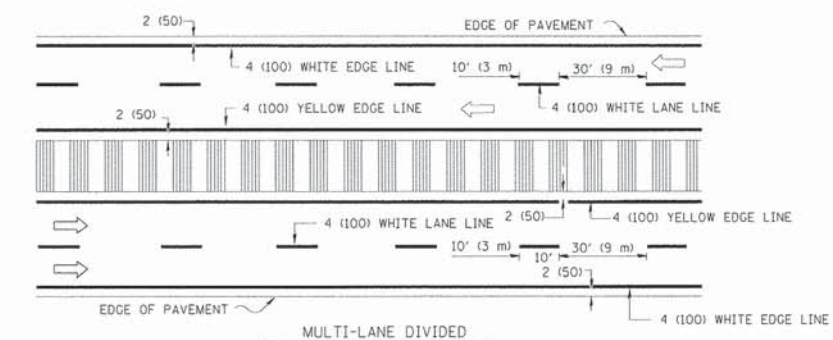


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



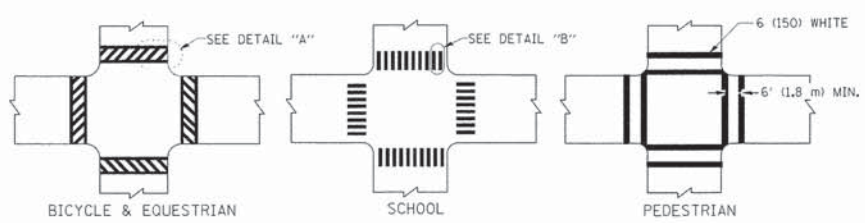
MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

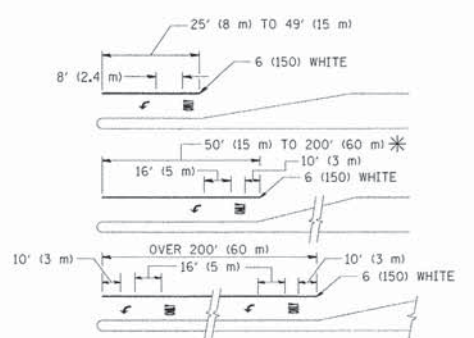


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

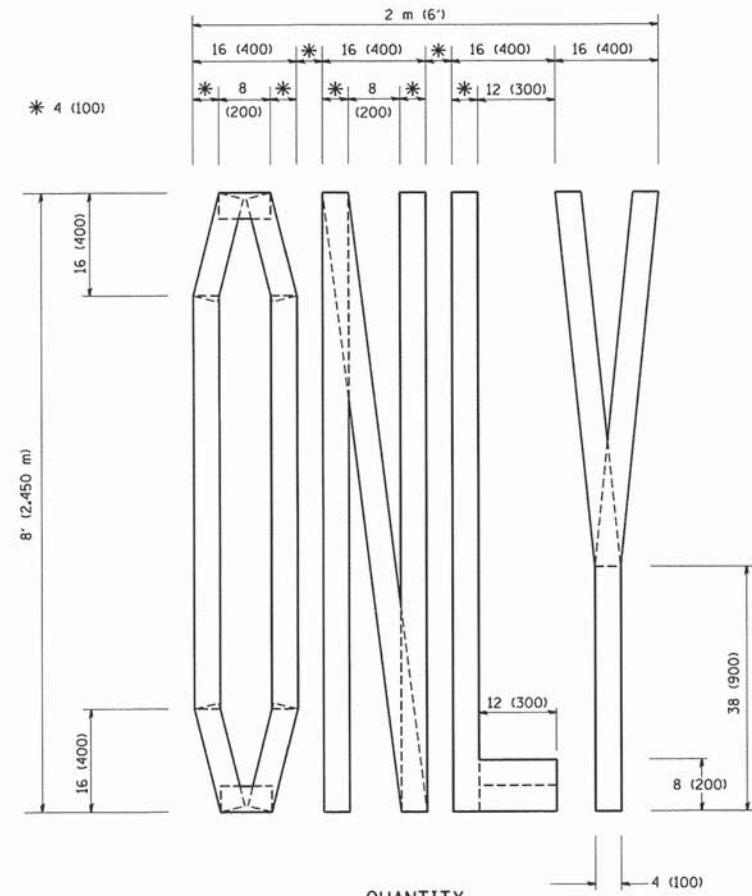


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3.dgn	DRAWN -	REVISED - C. JUCIUS 09-09-09
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PLOT DATE = 12/3/2015	DATE - 03-19-90	REVISED -

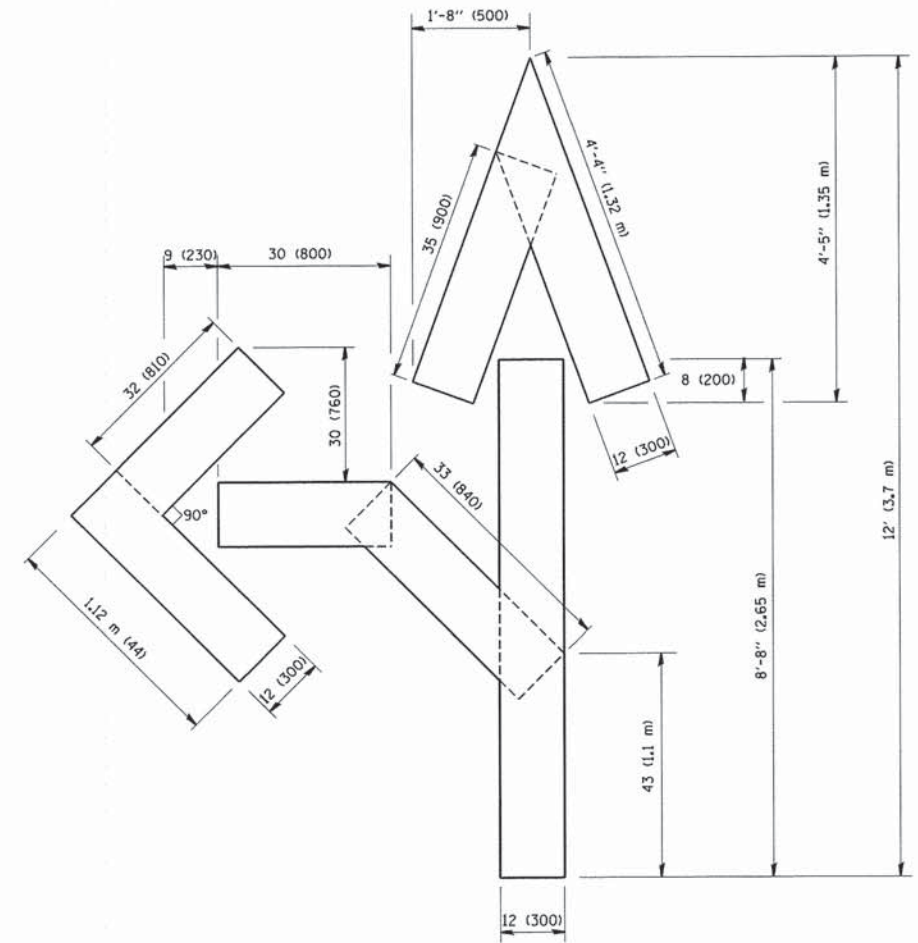
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	

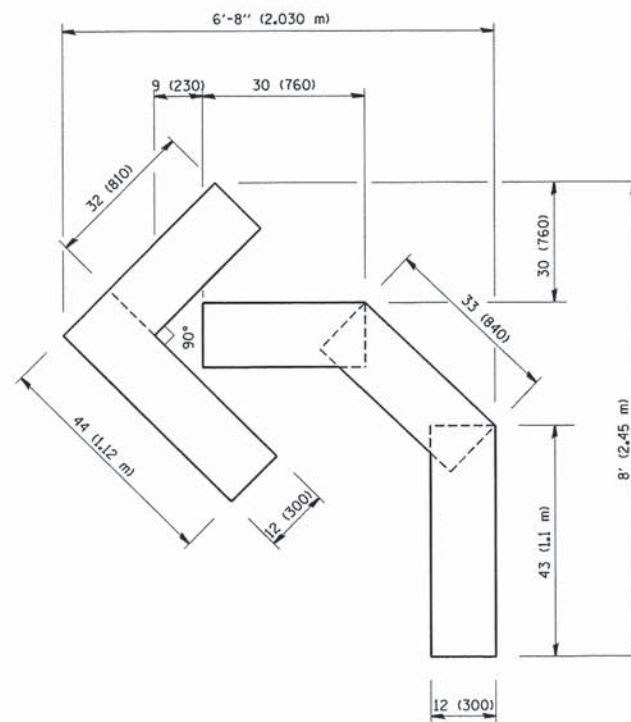
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	89
TC-13		CONTRACT NO. 61C29		
FED. ROAD DIST. NO. 1 ILLINOIS/FED. AID PROJECT M-4003 (254)				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.



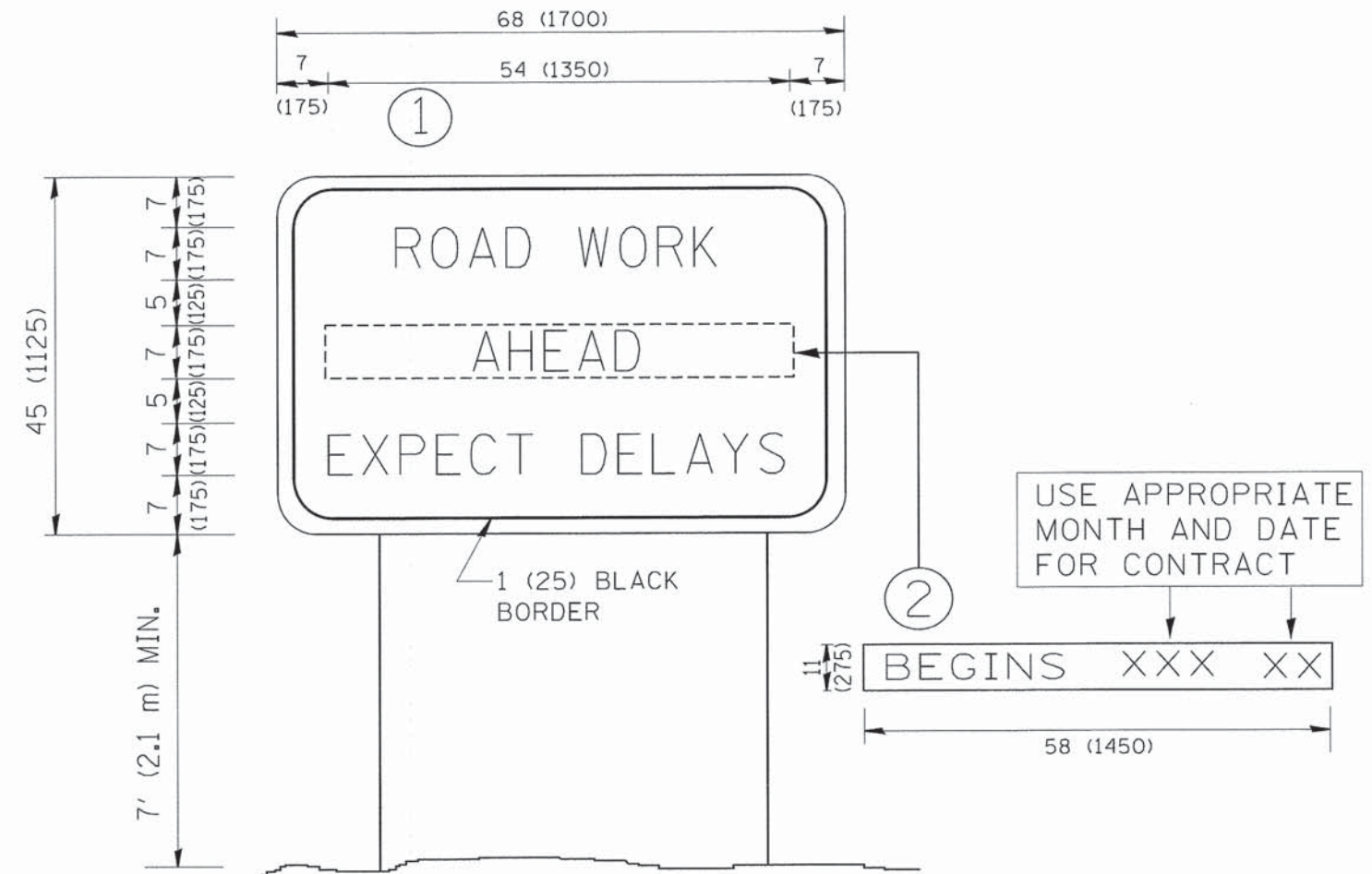
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PLOT DATE = 12/3/2015	CHECKED -	REVISED -T, RAMMACHER 03-02-98
	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	90
TC-16			CONTRACT NO. 61C29	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-4003 (254)				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



USER NAME = tgb	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 50,000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 12/3/2015	DATE -	REVISED - C. JUICIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
292	11-00446-00-CH	WILL	110	91
TC-22		CONTRACT NO. 61C29		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (254)				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".



USER NAME = tgb	DESIGNED -	REVISED - C. JUCIUS 02-15-07
tc16.dgn	DRAWN -	REVISED -
PLOT SCALE = 50,000 / 1"	CHECKED -	REVISED -
PLOT DATE = 12/3/2015	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

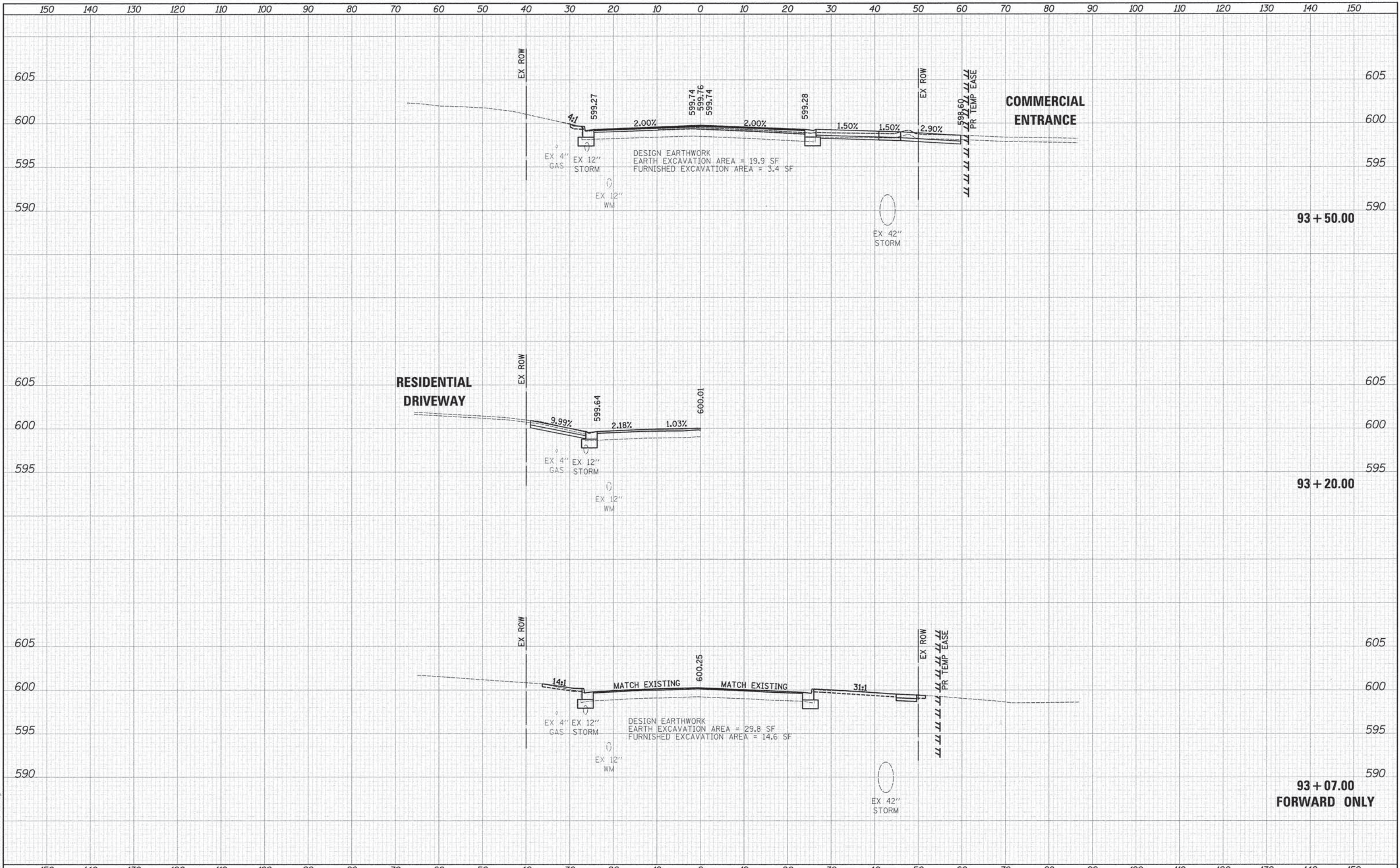
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

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292	11-00446-00-CH	WILL	110	92
TC-26			CONTRACT NO. 61C29	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-4003 (254)				

DATE	
BY	
FINAL SURVEY	
SURVEYED	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	
NO.	

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 FILE NAME = ...



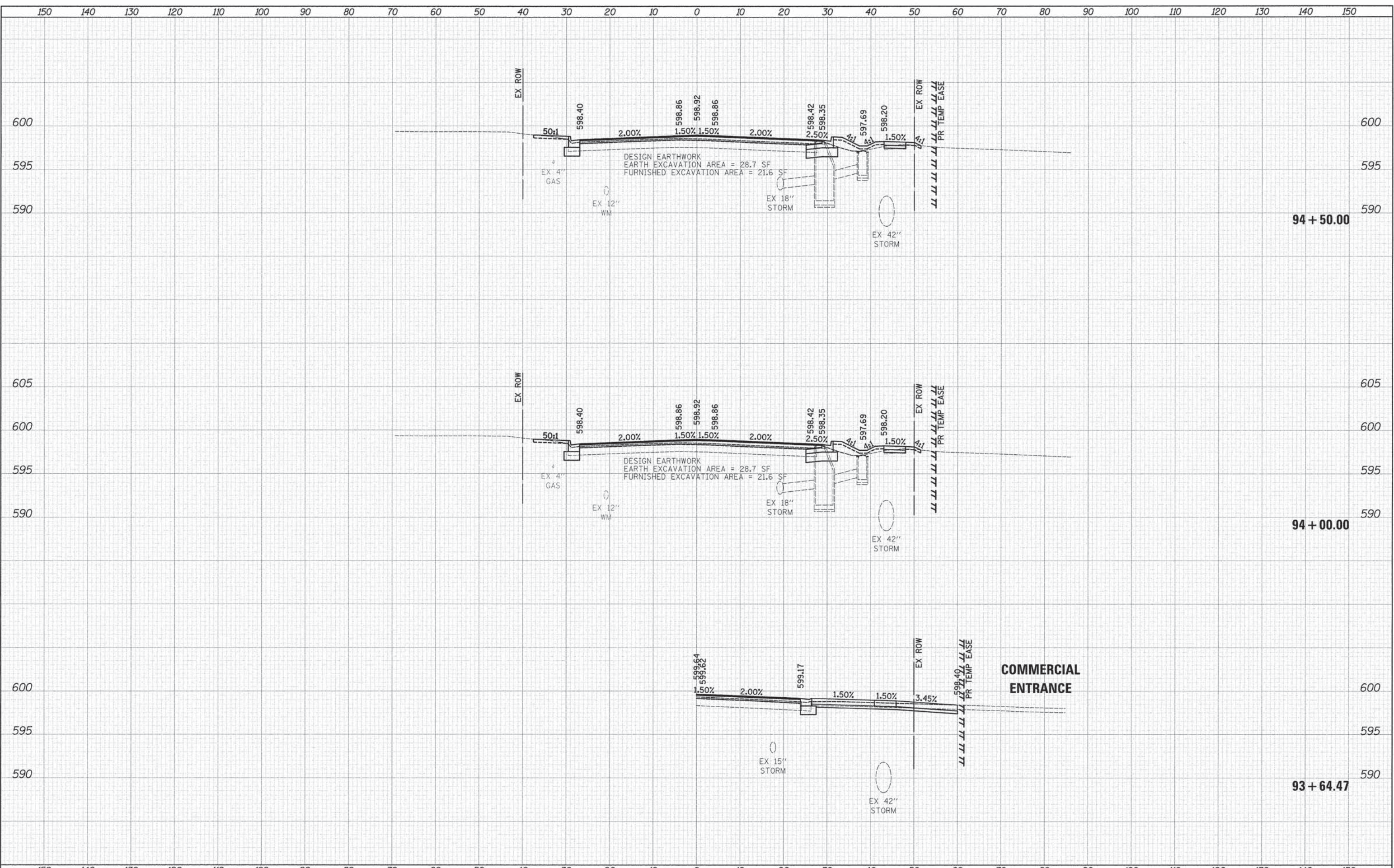
150	140	130	120	110	100	90	80	70	60	50	40	30	20	10	0	10	20	30	40	50	60	70	80	90	100	110	120	130	140	150
USER NAME = tgb DESIGNED - EDS DRAWN - EDS CHECKED - JRV PLOT DATE = 12/7/2015												DESIGNED - EDS REVISIONS - REVISIONS - REVISIONS - REVISIONS -				STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				CATON FARM ROAD CROSS SECTIONS				F.A.U. RTE. 1007 SECTION 11-00446-00-CH COUNTY WILL TOTAL SHEETS 110 SHEET NO. 93 CONTRACT NO. 61C29 ILLINOIS FED. AID PROJECT M-4003 (254)						
SCALE: 1"=10'H/5'V SHEET 1 OF 11 SHEETS STA. 93+07.00 TO STA. 93+50.00																														



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PLOT DRIVER = ...
 PEN TABLE = ...
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USER NAME =	tgb
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DRAWN -	EDS
CHECKED -	JRV
DATE -	12/07/15

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REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CATON FARM ROAD
CROSS SECTIONS**

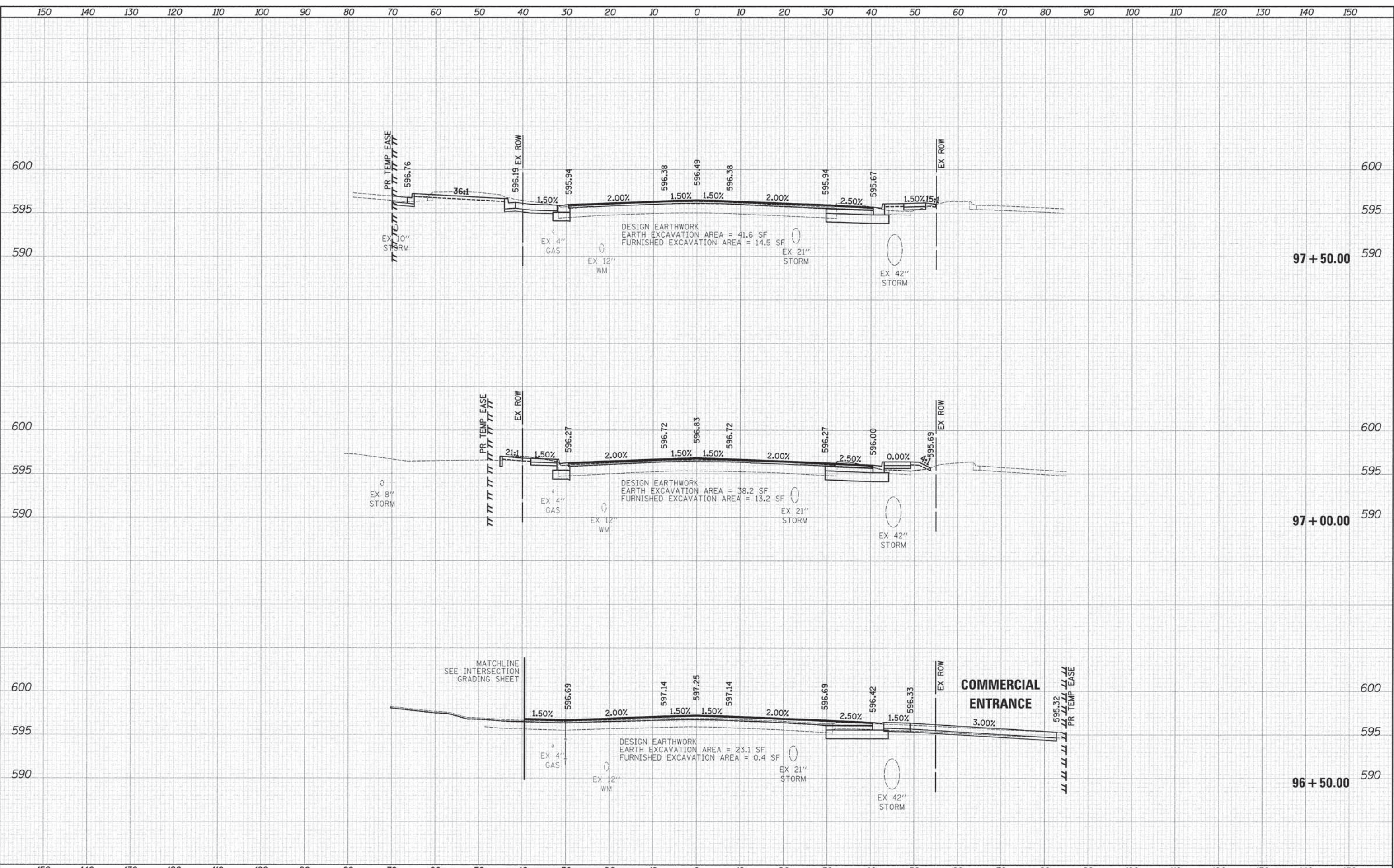
SCALE: 1"=10'H/5'V SHEET 2 OF 11 SHEETS STA. 93+64.47 TO STA. 94+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1007	11-00446-00-CH	WILL	110	94
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

DATE	
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PLOT DATE = 12/7/2015	CHECKED - JRV	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CATON FARM ROAD
CROSS SECTIONS**

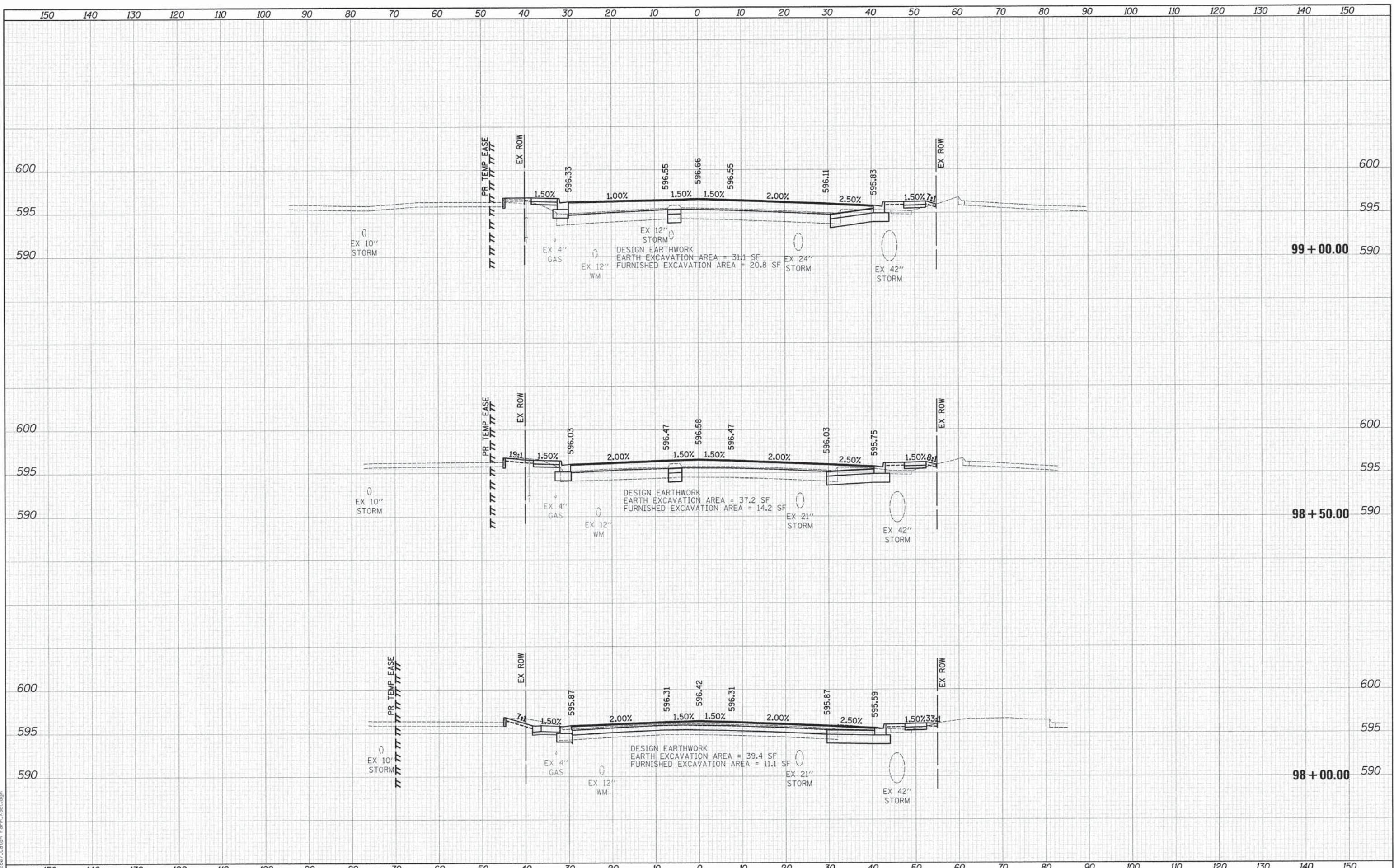
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F.A.U. RTE. 1007	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 97
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

DATE	
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SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	

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SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	

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USER NAME = tgb
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 PLOT DATE = 12/7/2015

DESIGNED - EDS
 DRAWN - EDS
 CHECKED - JRV
 DATE - 12/07/15

REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CATON FARM ROAD
 CROSS SECTIONS**

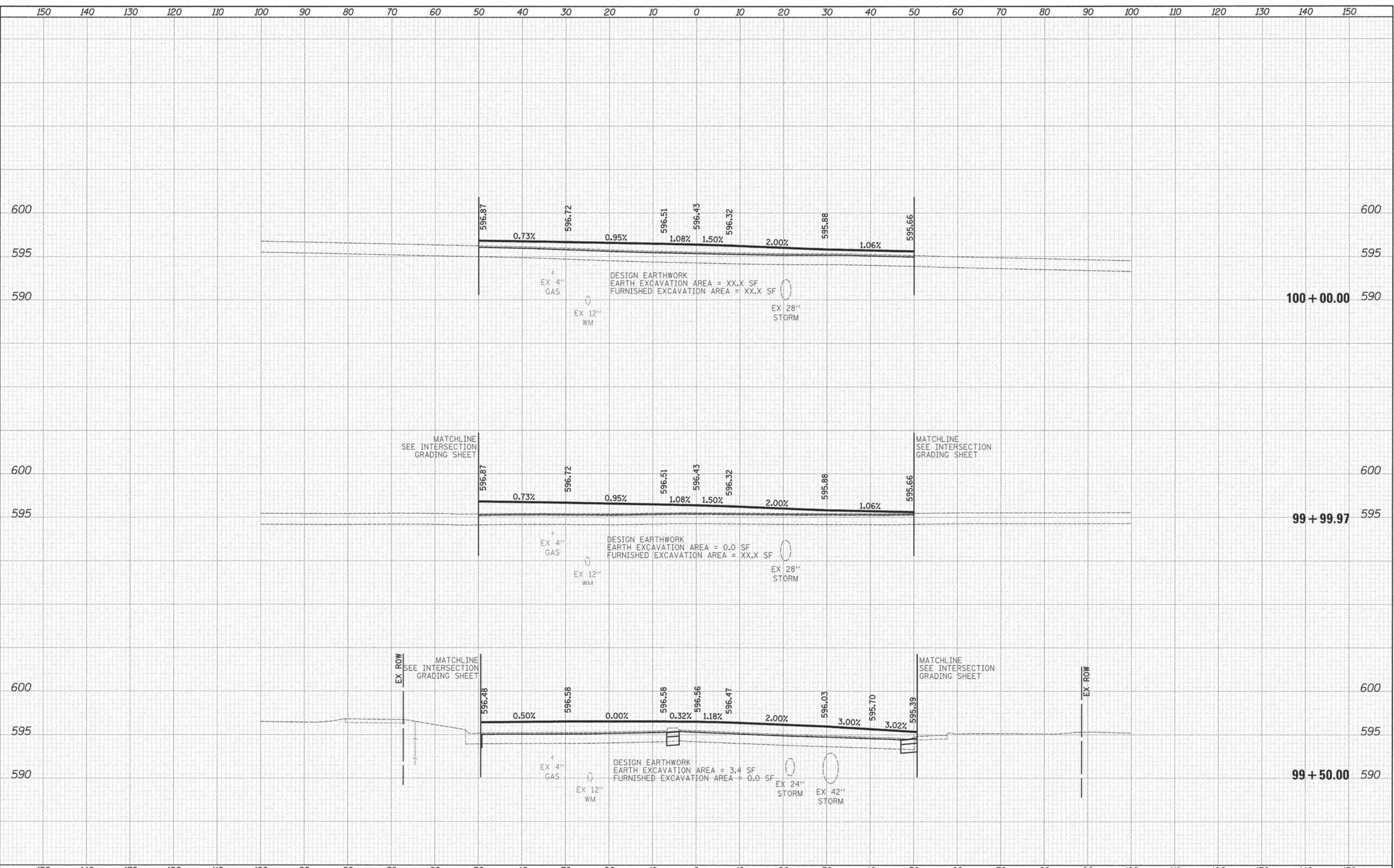
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1007	11-00446-00-CH	WILL	110	98
CONTRACT NO. 61C29				
ILLINOIS FED. AID PROJECT M-4003 (254)				

DATE	
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FINAL SURVEY	
NOTED BOOK	
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DATE	

DATE	
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ORIGINAL SURVEY	
NOTED BOOK	
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DESIGNED	
DRAWN	
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DATE	

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	DRAWN - EDS	REVISED -
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PLOT DATE = 12/7/2015	DATE - 12/07/15	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CATON FARM ROAD
CROSS SECTIONS**

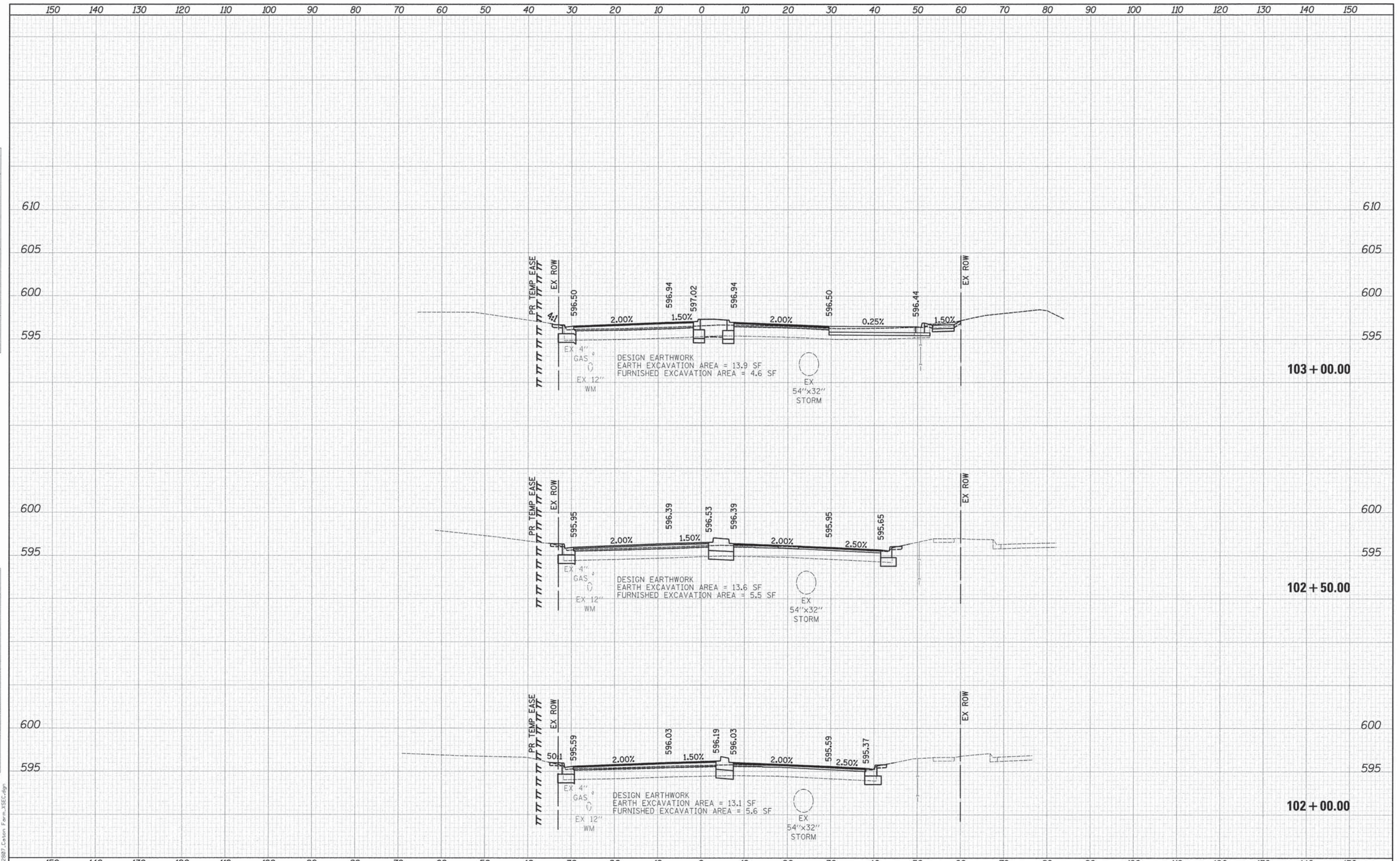
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F.A.U. RTE. 1007	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 99
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

BY	DATE

BY	DATE

ORIGINAL SURVEY SURVEYED PLOTTED TEMPLATE AREAS CHECKED
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PLOT DATE = 12/7/2015	CHECKED - JRV	REVISED -
	DATE - 12/07/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CATON FARM ROAD
CROSS SECTIONS

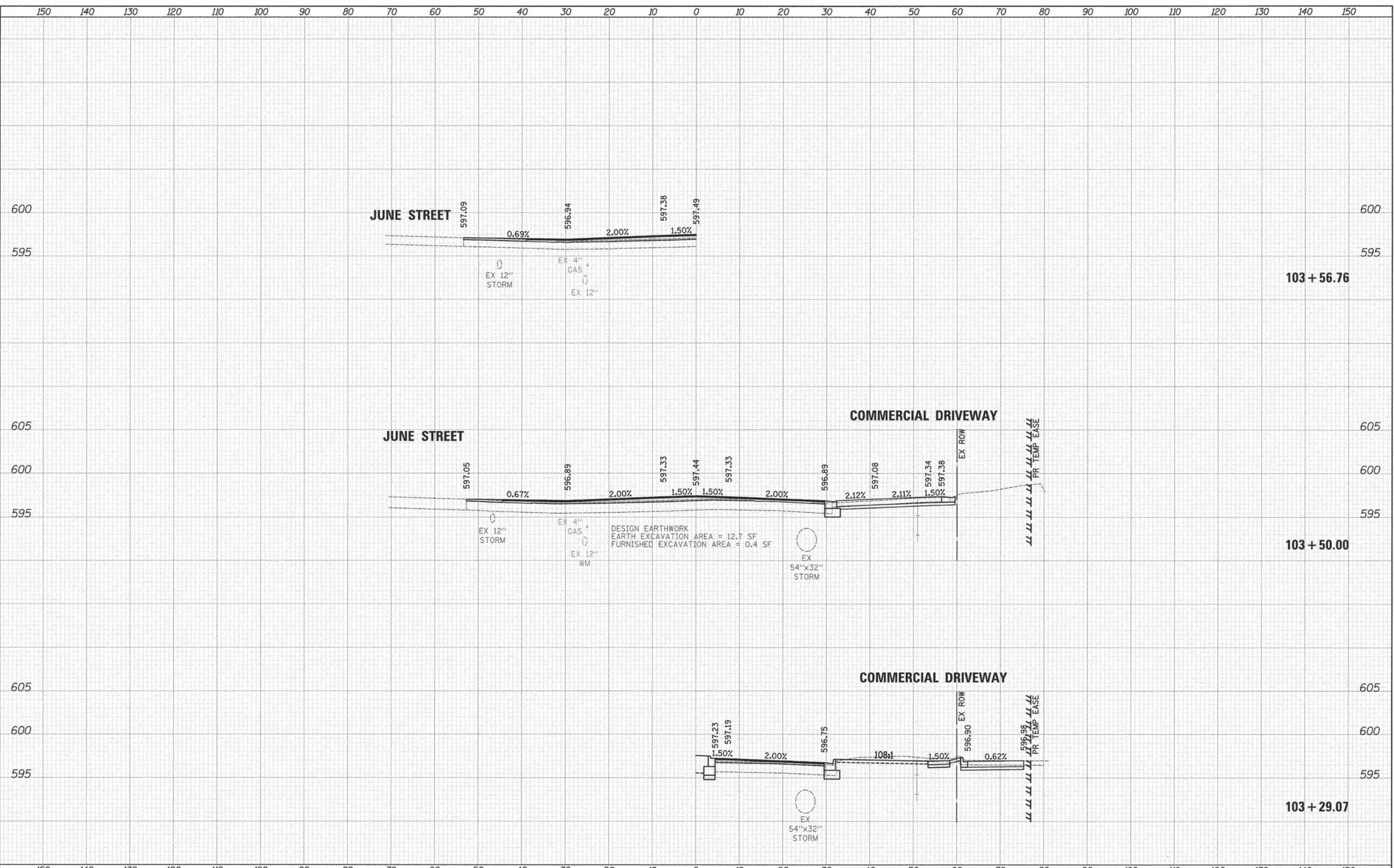
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1007	11-00446-00-CH	WILL	110	101
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

DATE	
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FINAL SURVEY	
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PLOTTED	
TEMPLATE	
AREAS	
AREAS CHECKED	
NOTE BOOK	
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DATE	
BY	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
AREAS CHECKED	
NOTE BOOK	
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	DRAWN - EDS	REVISED -
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PLOT DATE = 12/7/2015	DATE - 12/07/15	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CATONS FARM ROAD
CROSS SECTIONS**

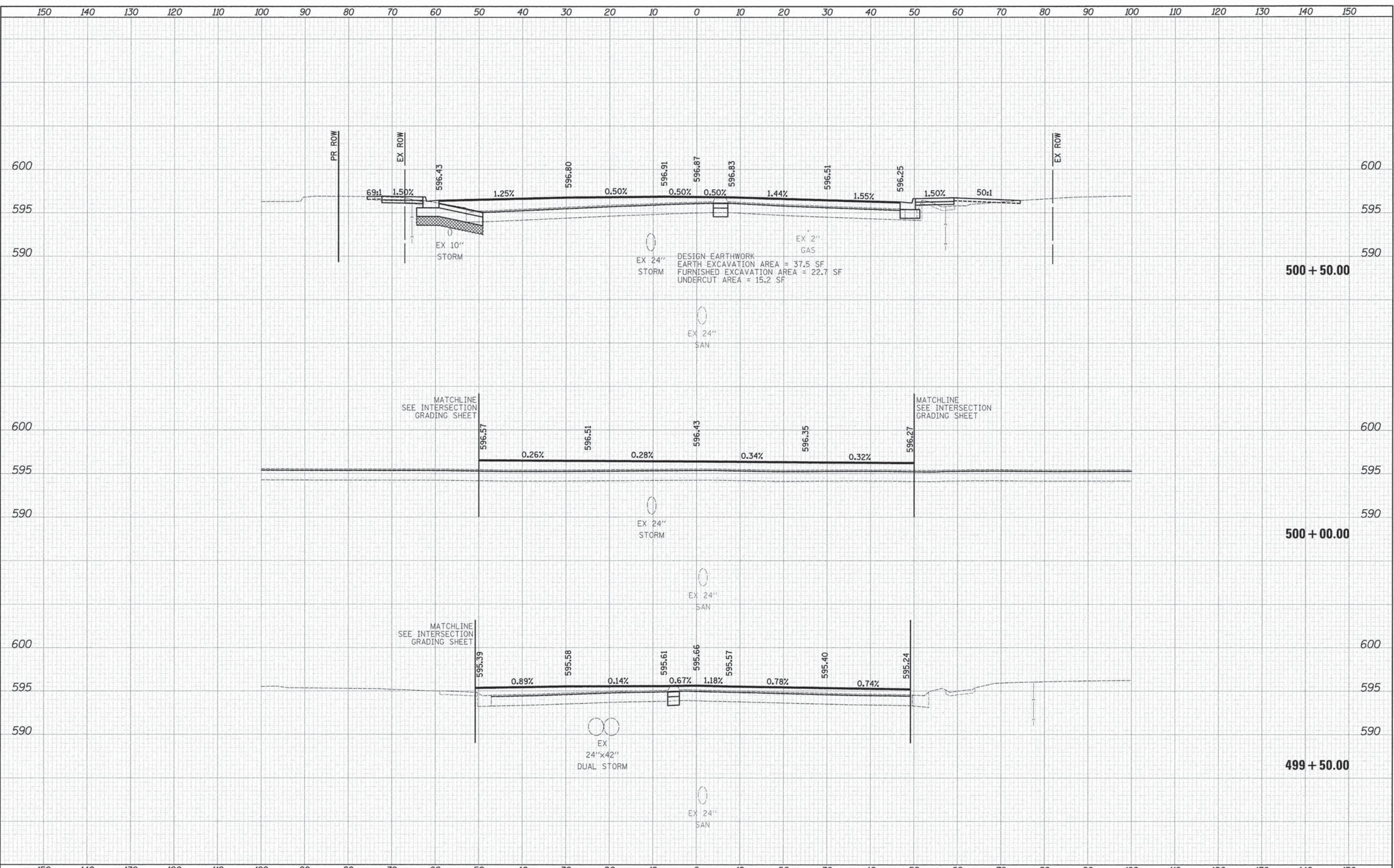
SCALE: 1"=10'H/5'V SHEET 10 OF 11 SHEETS STA. 103+29.07 TO STA. 103+56.76

F.A.U. RTE. 1007	SECTION 11-00446-00-CH	COUNTY WILL	TOTAL SHEETS 110	SHEET NO. 102
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

DATE	
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REVIEWED	
PLOTTED	
TEMPLATE	
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CHECKED	
FINAL SURVEY	
NOTE BOOK	
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DATE	
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TEMPLATE	
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ORIGINAL SURVEY	
NOTE BOOK	
NO.	

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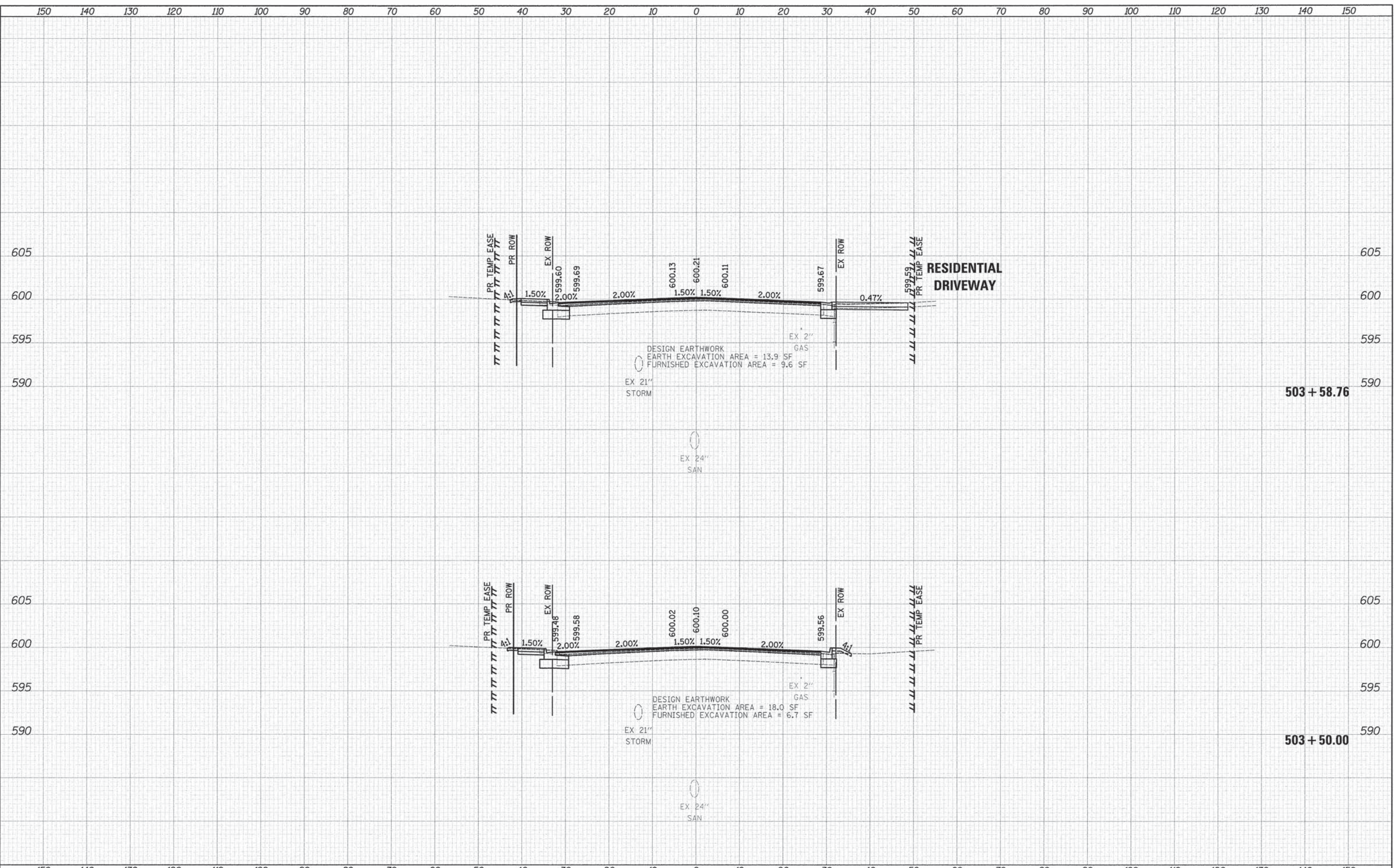


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	PLOT SCALE = 10.0000' / in.	CHECKED - JRV	REVISED -			SCALE: 1"=10'H/5'V	SHEET OF SHEETS	STA. 499+50.00 TO STA. 500+50.00	CONTRACT NO. 61C29	
	PLOT DATE = 12/7/2015	DATE - 12/07/15	REVISED -							

DATE	
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FINAL SURVEY	
SPRINTED	
PLOTTED	
TEMPLATE	
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NOTE BOOK	
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DATE	
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ORIGINAL SURVEY	
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NOTE BOOK	
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 PLOT SCALE = 10.0000' / 1" in.
 PLOT DATE = 12/7/2015

DESIGNED - EDS
 DRAWN - EDS
 CHECKED - JRV
 DATE - 12/07/15

REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
 ESSINGTON ROAD**

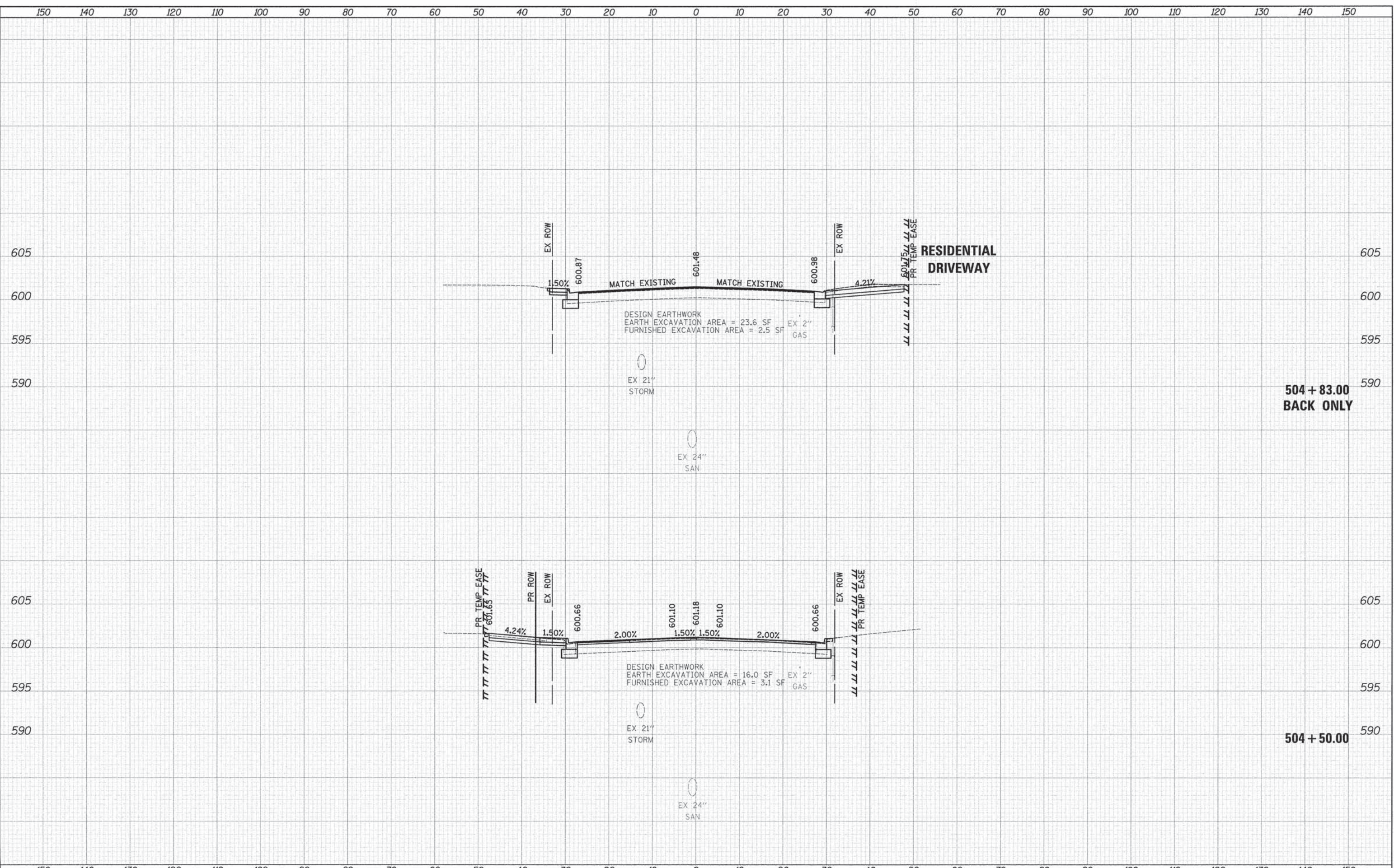
SCALE: 1"=10'H/5'V SHEET OF SHEETS STA. 503+50.00 TO STA. 503+58.76

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1007	11-00446-00-CH	WILL	110	108
CONTRACT NO. 61C29			ILLINOIS FED. AID PROJECT M-4003 (254)	

DATE	
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FINAL SURVEY	
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PLOTTED	
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NOTE BOOK	
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DATE	
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ORIGINAL SURVEY	
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TEMPLATE	
AREAS	
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NOTE BOOK	
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USER NAME = tgb
 PLOT SCALE = 10,0000 / / in.
 PLOT DATE = 12/7/2015

DESIGNED - EDS
 DRAWN - EDS
 CHECKED - JRV
 DATE - 12/07/15

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
 ESSINGTON ROAD**

SCALE: 1"=10'H/5'V SHEET OF SHEETS STA. 504+50.00 TO STA. 504+83.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1007	11-00446-00-CH	WILL	110	110
			CONTRACT NO. 61C29	
			ILLINOIS FED. AID PROJECT M-4003 (254)	