

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGE OF CARY

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS
**PLANS FOR PROPOSED
 FEDERAL AID HIGHWAY**
**F.A.P. 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY) AND
 F.A.U. ROUTE 3877 EAST MAIN STREET**
RIGHT-TURN LANE, TRAFFIC SIGNALS AND ARRA RESURFACING IMPROVEMENTS

SECTION NO.: 08-00054-00-CH
 PROJECT NO.: M-ARA-9003(313)
 JOB NO.: C-91-619-09

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
08-00054-00-CH		McHENRY	56	1
STA.		TO STA.		
FED. ROAD DIST. NO. 1 C-91-619-09	ILLINOIS	FED AID PROJECT M-ARA-9003(313)		
F.A.P. 0305 U.S. ROUTE 14 AND F.A.U. 3877 EAST MAIN STREET CONTRACT NO: 63404				



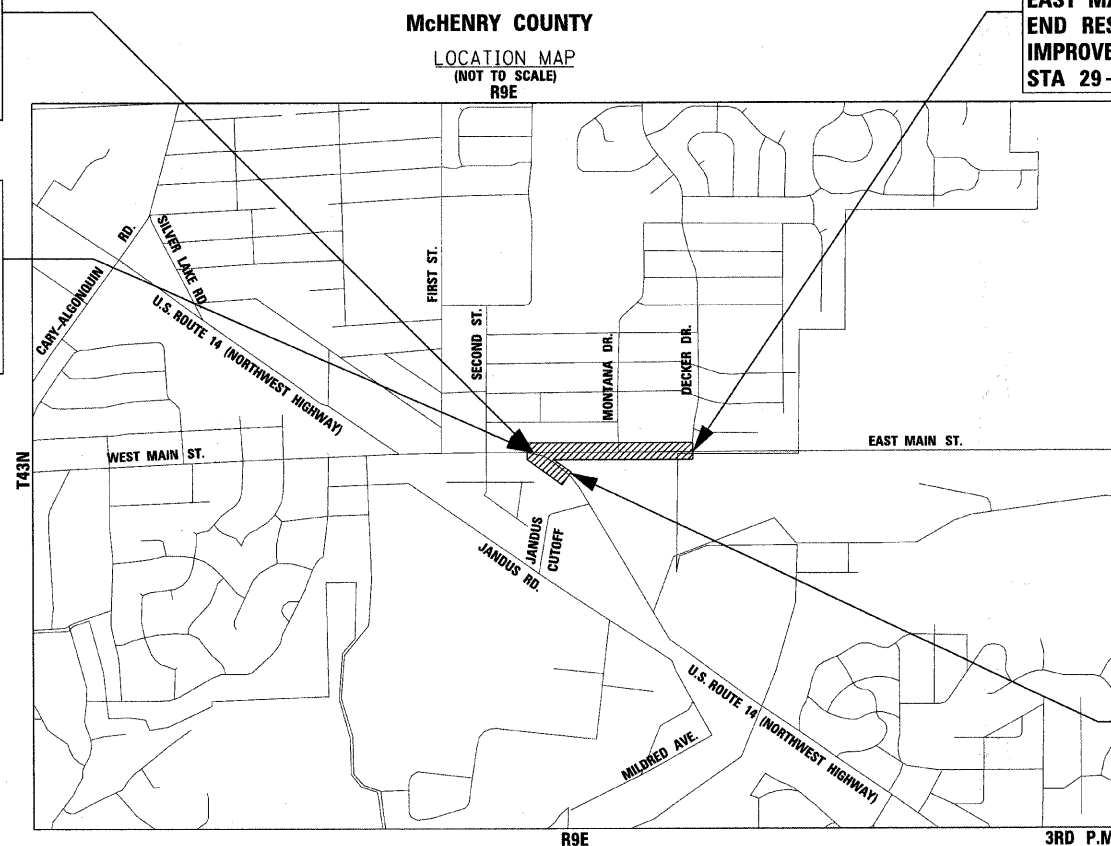
LOCATION OF SECTION INDICATED THIS: [Black Box]

PROJECT NO.: M-ARA-9003(313)
 F.A.U. ROUTE 3877
 EAST MAIN STREET
 BEGIN RESURFACING
 IMPROVEMENTS
 STA 10 + 29

PROJECT NO.: M-ARA-9003(313)
 F.A.P. ROUTE 0305
 U.S. ROUTE 14
 (NORTHWEST HIGHWAY)
 BEGIN RIGHT-TURN LANE
 IMPROVEMENTS
 STA 182 + 00

PROJECT NO.: M-ARA-9003(313)
 F.A.U. ROUTE 3877
 EAST MAIN STREET
 END RESURFACING
 IMPROVEMENTS
 STA 29 + 00

PROJECT NO.: M-ARA-9003(313)
 F.A.P. ROUTE 0305
 U.S. ROUTE 14
 (NORTHWEST HIGHWAY)
 END RIGHT-TURN LANE
 IMPROVEMENTS
 STA 185 + 30



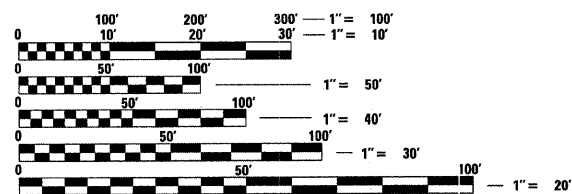
ALGONQUIN TOWNSHIP
 GROSS LENGTH OF IMPROVEMENT = 2,201 LF OR 0.417 MILES
 NET LENGTH OF IMPROVEMENT = 2,201 LF OR 0.417 MILES

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 LICENSE NO. - 184-001121 - EXPIRES 4/30/2011

TRAFFIC DATA

U.S. ROUTE 14 (NORTHWEST HIGHWAY)
 POSTED & DESIGN SPEED = 35 MPH
 2009 ADT = 36,600 VPD
 OTHER PRINCIPAL ARTERIAL

EAST MAIN STREET
 POSTED & DESIGN SPEED = 30 MPH
 2009 ADT = 6,300 VPD
 COLLECTOR



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
 ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
 CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
 ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. DESIGN STAGE REQUEST
 DIG. No. X0840787



CONTACT JULIE AT 811 OR 800-892-0123
 WITH THE FOLLOWING:
 COUNTY = McHENRY
 CITY-TOWNSHIP = CARY-ALGONQUIN
 SEC. & 1/4 SEC. NO. = SEC 18 NW & NE 1/4, T43N, R9E
 48 HOURS (2 working days) BEFORE YOU DIG



PROJECT MANAGER
 "LICENSE EXPIRES 11-30-11"

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

APPROVED 11-17-09
[Signature]
 VILLAGE OF CARY, DIRECTOR OF PUBLIC WORKS

PASSED DECEMBER 5 2009
[Signature]
 DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
 BASED ON LIMITED
 REVIEW DECEMBER 4 2009
[Signature]
 DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

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CONTRACT NO: 63404

Burlington, Wisconsin	Chicago, Illinois	Crystal Lake, Illinois	DeKalb, Illinois	Grayslake, Illinois	Itasca, Illinois	Madison, Wisconsin	Mokena, Illinois	Plainfield, Illinois
262.763.7834	312.578.0050	815.459.1260	815.787.3111	847.223.5088	630.773.1870	608.347.1542	708.478.2090	815.609.7425
(OFFICE WHICH PREPARED PLANS)								

B&W PROJECT NO.: 080298/090385 DATE: 11-17-09

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 ASSOCIATE FIELD ENGINEER: KEVIN STALLWORTH, P.E. 7847-705-4169
 DRAWING NO. 080298/090385-01
 DATE: 11/17/2009
 TIME: 2:53:25 PM
 PROJECT: F.A.P. 0305 U.S. ROUTE 14 AND F.A.U. 3877 EAST MAIN STREET
 COUNTY: McHENRY
 CITY-TOWNSHIP: CARY-ALGONQUIN
 SEC. & 1/4 SEC. NO.: SEC 18 NW & NE 1/4, T43N, R9E
 DRAWN BY: J. L. I. E.

BENCHMARKS (NAVD 88)

BM #3	RR SPIKE, FIRST POWER POLE SE OF ENTRANCE TO CVS PHARMACY ON ROUTE 14 EL 806.46
BM #4	N BOLT ON FIRE HYDRANT AT NE CORNER OF ISLAND FOR CVS PHARMACY EL 818.74
BM #6	NE BOLT ON FIRE HYDRANT N SIDE OF ROUTE 14, ADDRESS 156, FIRST FH E OF ROUTE 14 AND MAIN ST EL 821.93
BM #8	NE BOLT ON FIRE HYDRANT AT N SIDE ROUTE 14 ADDRESS 130, SECOND FH WEST OF ROUTE 14 & E MAIN ST EL 823.24
BM #9	NE BOLT ON FIRE HYDRANT ON S SIDE E MAIN ST ACROSS FROM ADDRESS 236 EL 818.28
BM #10	NE BOLT ON FIRE HYDRANT ON S SIDE OF E MAIN ST ACROSS FROM ADDRESS 256 EL 825.27
BM #12	RR SPIKE IN N SIDE POWER POLE S SIDE OF E MAIN ST ADDRESS 340 EL 824.24
BM #13	NE BOLT ON FIRE HYDRANT AT NW CORNER E MAIN ST AND DECKER DR EL 820.87

HIGHWAY STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-05	TEMPORARY EROSION CONTROL SYSTEMS
406201-01	MAILBOX TURNOUT
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHLD. STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
602001-01	CATCH BASIN TYPE A
602011-01	CATCH BASIN TYPE C
602301-02	INLET TYPE A
602306-02	INLET TYPE B
602401-02	MANHOLE TYPE A
602601-02	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-03	FRAME AND LIDS TYPE 1
604051-03	FRAME AND GRATE TYPE 11
604091-02	FRAME AND GRATE TYPE 24
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701602-04	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
720016-02	MAST ARM MOUNTED STREET NAME SIGNS
728001-01	TELESCOPING STEEL SIGN SUPPORT
780001-02	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
805001-01	ELECTRICAL SERVICE INSTALLATION DETAILS
814001-02	HANDHOLES
814006-02	DOUBLE HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
862001-01	UNINTERRUPTABLE POWER SUPPLY (UPS)
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
877001-04	STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
878001-08	CONCRETE FOUNDATION DETAILS
880001-01	SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS
B.L.R. 24-2	MAILBOX TURNOUT FOR LOCAL ROADS

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LEGEND

	BUTT JOINT OR DRIVEWAY BUTT JOINT		REMOVING MANHOLES TO MAINTAIN FLOW		RIM RIM ELEVATION
	FRAME AND LIDS TO BE ADJUSTED (SPECIAL)		REMOVING INLETS TO MAINTAIN FLOW		BC BACK OF CURB
	FRAME AND LIDS TO BE ADJUSTED (SPECIAL)		CATCH BASIN TO BE REMOVED		BW BACK OF WALK
	FRAME AND LIDS TO BE ADJUSTED (SPECIAL)		EXISTING ELEVATION		DRV DRIVEWAY
	DOMESTIC WATER SERVICE BOX TO BE ADJUSTED (SPECIAL)		PROPOSED ELEVATION		EX EXISTING
	MANHOLE TO BE ADJUSTED		VALVE VAULT TO BE ADJUSTED		
	INLET TO BE ADJUSTED		CATCH BASIN TO BE RECONSTRUCTED		
	CATCH BASIN TO BE ADJUSTED				

VILLAGE OF CARY - PHONE NUMBERS

PUBLIC WORKS	(847) 639-0003
POLICE DEPT.	(847) 639-2341
FIRE DEPT.	(847) 639-2121

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 LICENSE NO. 184-00031 - EXPIRES 4/30/2011
 263mcc



DESIGNED - JJF	REVISED - MCDPD REVIEW 11/3/09
DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-GEN_NOTES.shp

**VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS**

**INDEX TO SHEETS, HIGHWAY STANDARDS,
BENCHMARKS AND LEGEND**

SCALE: NONE

STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	08-00054-00-CH	McHENRY	56	2
	C-91-619-09			CONTRACT NO. 63404
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(313)				

SUMMARY OF QUANTITIES

CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE		
				ROADWAY 1000-1A	TRAFFIC SIGNALS Y031-1F	NON-PARTICIPATING
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	101	101	-	-
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	5,320	5,320	-	-
42400300	PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH	SQ FT	970	970	-	-
42400800	DETECTABLE WARNINGS	SQ FT	40	40	-	-
44000100	PAVEMENT REMOVAL	SQ YD	220	220	-	-
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	6,580	6,580	-	-
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	90	90	-	-
44000300	CURB REMOVAL	FOOT	152	152	-	-
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	635	635	-	-
44000600	SIDEWALK REMOVAL	SQ FT	4,230	4,230	-	-
44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	79	79	-	-
44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	118	118	-	-
44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	196	196	-	-
44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	392	392	-	-
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	70	70	-	-
51205200	TEMPORARY SHEET PILING	SQ FT	400	400	-	-
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	561	561	-	-
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	7	7	-	-
550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	16	16	-	-
55100500	STORM SEWER REMOVAL 12"	FOOT	30	30	-	-
55100700	STORM SEWER REMOVAL 15"	FOOT	11	11	-	-
55100900	STORM SEWER REMOVAL 18"	FOOT	5	5	-	-
60107600	PIPE UNDERDRAINS 4"	FOOT	75	75	-	-
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	1	-	-
60200205	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1	-	-
60207005	CATCH BASINS, TYPE C, TYPE 1 FRAME, CLOSED LID	EACH	1	1	-	-
60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	9	9	-	-
60236800	INLETS, TYPE A, TYPE 11 FRAME AND GRATE	EACH	3	3	-	-
60237470	INLETS, TYPE A, TYPE 24 FRAME AND GRATE	EACH	2	2	-	-
60240210	INLETS, TYPE B, TYPE 1 FRAME, OPEN LID	EACH	1	1	-	-

* INDICATES SPECIALTY ITEM

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 LICENSE NO. 084-000298-500-PT1 EXPIRES 4/30/2011
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DESIGNED - JJF	REVISED - MCDPD REVIEW 11/3/09
DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-500.sh+

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

SUMMARY OF QUANTITIES

SCALE: NONE STA. TO STA.

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
 F.A.U. ROUTE 3877 EAST MAIN STREET

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	08-00054-00-CH	McHENRY	56	5
C-91-619-09			CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(313)				

SUMMARY OF QUANTITIES

CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE		
				ROADWAY I000-1A	TRAFFIC SIGNALS Y031-1F	NON-PARTICIPATING
60250200	CATCH BASINS TO BE ADJUSTED	EACH	1	1	-	-
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	2	2	-	-
60255500	MANHOLES TO BE ADJUSTED	EACH	1	1	-	-
60260100	INLETS TO BE ADJUSTED	EACH	2	2	-	-
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	1	1	-	-
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	8	8	-	-
60404950	FRAMES AND GRATES, TYPE 24	EACH	1	1	-	-
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	3	3	-	-
60500050	REMOVING CATCH BASINS	EACH	1	1	-	-
60500080	REMOVING CATCH BASINS TO MAINTAIN FLOW	EACH	1	1	-	-
60500090	REMOVING INLETS TO MAINTAIN FLOW	EACH	2	2	-	-
60604200	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL)	FOOT	905	905	-	-
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	475	475	-	-
67100100	MOBILIZATION	L SUM	1	1	-	-
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	-	-
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1	-	-
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	-	-
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	-	-
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	7,000	7,000	-	-
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1,750	1,750	-	-
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	182	182	-	-
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	5,422	5,422	-	-
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	706	706	-	-
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	226	226	-	-
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	34	34	-	-
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	4	4	-	-
* 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	4	4	-	-
* 81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	215	-	215	-
* 81000700	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	90	-	90	-
* 81001000	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	20	-	20	-

* INDICATES SPECIALTY ITEM

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DESIGNED - JJF	REVISED - MCDPD REVIEW 11/3/09
DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-S00.shp

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

SUMMARY OF QUANTITIES

SCALE: NONE STA. TO STA.

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY) F.A.J. ROUTE 3877 EAST MAIN STREET	
F.A. RTE.	SECTION
C-91-619-09	08-00054-00-CH
COUNTY	TOTAL SHEETS
MCHENRY	56
CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(313)	

SUMMARY OF QUANTITIES

CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE		
				ROADWAY 1000-1A	TRAFFIC SIGNALS Y031-1F	NON-PARTICIPATING
* 81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	41	-	41	-
* 81018900	CONDUIT PUSHED, 4" DIA, GALVANIZED STEEL	FOOT	222	-	222	-
* 81400100	HANDHOLE	EACH	1	-	1	-
* 81400300	DOUBLE HANDHOLE	EACH	1	-	1	-
* 81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	324	-	324	-
* 85700205	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1	-	1	-
* 86400100	TRANSCEIVER - FIBER OPTIC	EACH	1	-	1	-
* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 2C	FOOT	190	-	190	-
* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 3C	FOOT	555	-	555	-
* 87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 5C	FOOT	1,630	-	1,630	-
* 87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 7C	FOOT	545	-	545	-
* 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1,060	-	1,060	-
* 87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	50	-	50	-
* 87500600	TRAFFIC SIGNAL POST, 10 FT.	EACH	1	-	1	-
* 87501000	TRAFFIC SIGNAL POST, 14 FT.	EACH	3	-	3	-
* 87501100	TRAFFIC SIGNAL POST, 15 FT.	EACH	1	-	1	-
* 87700160	STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.	EACH	1	-	1	-
* 87700200	STEEL MAST ARM ASSEMBLY AND POLE, 32 FT.	EACH	2	-	2	-
* 87800115	CONCRETE FOUNDATION, TYPE A	EACH	5	-	5	-
* 87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4	-	4	-
* 87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	45	-	45	-
* 87900200	DRILL EXISTING HANDHOLE	EACH	4	-	4	-
* 88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	5	-	5	-
* 88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	3	-	3	-
* 88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	2	-	2	-
* 88030230	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-4 SECTION, BRACKET MOUNTED	EACH	1	-	1	-
* 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2	-	2	-
* 88200210	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	7	-	7	-
* 88500100	INDUCTIVE LOOP DETECTOR	EACH	5	-	5	-
* 88600100	DETECTOR LOOP, TYPE I	FOOT	350	-	350	-

* INDICATES SPECIALTY ITEM

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 DRAWN BY: CJC DATE: 09/11/09
 LICENSE NO.: 084-0001 EXPIRES: 4/30/2011
 263900C 12/2/2009 10:38:22 AM \\s01c1r1\c\p\1082298-500.sht



DESIGNED - JUF	REVISED - MCDPD REVIEW 11/3/09
DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-500.sht

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

SUMMARY OF QUANTITIES

SCALE: STA. TO STA.

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY) F.A.J. ROUTE 3877 EAST MAIN STREET				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	08-00054-00-CH	McHENRY	56	7
C-91-619-09		CONTRACT NO. 63404		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003313				

HOT-MIX ASPHALT MIXTURE REQUIREMENTS NOTES

1. CONTRACTOR SHALL MILL BEFORE PATCHING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
PAVEMENT WIDENING	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90; (IL 9.5mm); 2"	4% @ 90 GYR
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90; 2 1/2"	4% @ 90 GYR
HMA BASE COURSE (HMA BINDER IL-19mm); 10"	4% @ 90 GYR
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5mm); 1 1/2"	4% @ 50 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 1"	4% @ 50 GYR
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5mm); 1 1/2"	4% @ 50 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 1"	4% @ 50 GYR
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 mm); TYPE I - IV - 4 INCH OR TYPE I - IV - 6 INCH	4% @ 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ. YD./IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

EXISTING LEGEND

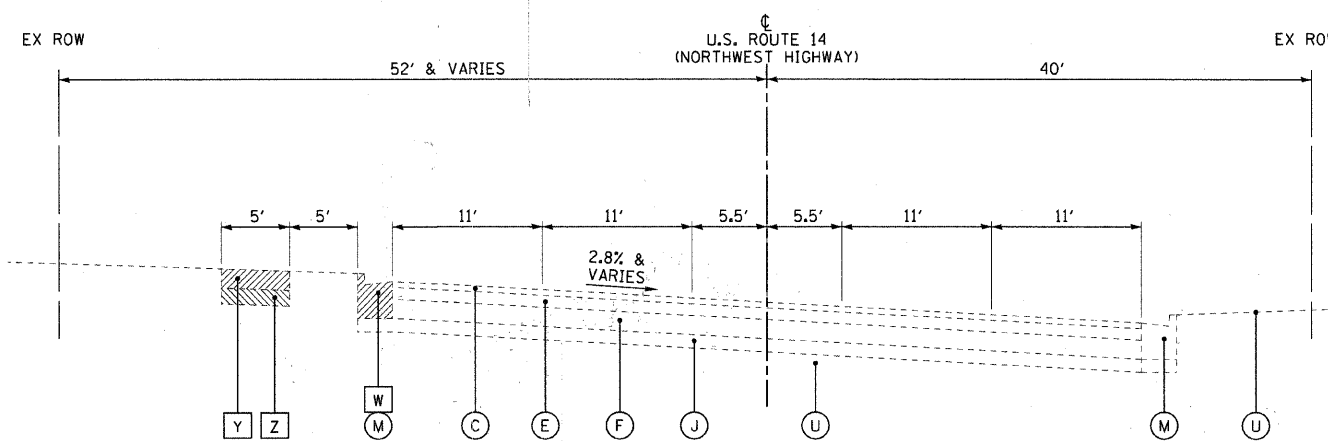
- (A) HOT-MIX ASPHALT SURFACE COURSE, 1"
- (B) HOT-MIX ASPHALT SURFACE COURSE, 1 1/4"
- (C) HOT-MIX ASPHALT SURFACE COURSE, 1 1/2"
- (D) HOT-MIX ASPHALT SURFACE COURSE, 2 1/4"
- (E) HOT-MIX ASPHALT BINDER COURSE, 3"
- (F) HOT-MIX ASPHALT BASE COURSE, 10"
- (G) BITUMINOUS STABILIZED GRANULAR MATERIAL, 2 1/2"
- (H) BITUMINOUS STABILIZED GRANULAR MATERIAL, 3 1/2"
- (I) AREA REFLECTIVE CRACK CONTROL TREATMENT (PETROMAT)
- (J) SUB-BASE GRANULAR MATERIAL, TYPE A, 4"
- (K) UNCRUSHED GRAVEL AND FINES, 7 1/2" - 9 1/2"
- (L) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

EXISTING LEGEND (CON'D.)

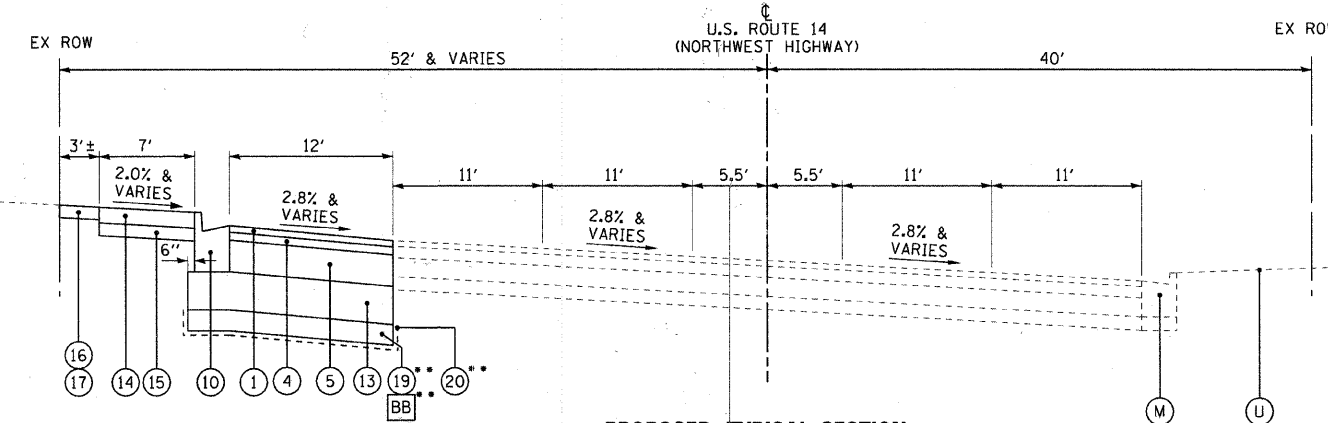
- (M) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (N) HOT-MIX ASPHALT SHOULDER (FULL DEPTH)
- (P) AGGREGATE SHOULDER, 4"
- (R) SIDEWALK
- (S) AGGREGATE BASE COURSE
- (T) EXISTING SUB-GRADE
- (U) GROUND SURFACE
- (V) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (W) COMBINATION CURB AND GUTTER REMOVAL
- (X) PAVEMENT REMOVAL
- (Y) SIDEWALK REMOVAL
- (Z) AGGREGATE BASE COURSE REMOVAL (INCLUDED IN THE EARTH EXCAVATION PAY ITEM)
- (AA) AGGREGATE SHOULDER REMOVAL, 2 1/2" (INCLUDED IN EARTH EXCAVATION PAY ITEM)
- (BB) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS **
- (Hatched) ITEM TO BE REMOVED

STRUCTURAL DESIGN DATA

STREET	STRUCTURAL DESIGN TRAFFIC			STREET CLASS	TRAFFIC FACTOR	SSR	TEMP	STRAIN	AC	E AC	REQ'D HMA THICKNESS	MECHANISTIC PAVEMENT DESIGN
	PV	SU	MU									
U.S. ROUTE 14	38,206	1,617	607	I	4.60	POOR	76°F	65	PG64-22	655	12.5 IN	2" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 2 1/2" POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 10" HOT-MIX ASPHALT BASE COURSE 12" AGGREGATE SUBGRADE



**EXISTING TYPICAL SECTION
U.S. ROUTE 14 (NORTHWEST HIGHWAY)
STA. 182+00 TO STA. 185+30
NOT TO SCALE**



**PROPOSED TYPICAL SECTION
U.S. ROUTE 14 (NORTHWEST HIGHWAY)
STA. 182+00 TO STA. 185+30
NOT TO SCALE**

PROPOSED LEGEND

- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 2"
- (2) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
- (4) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 - 2 1/2"
- (5) HOT-MIX ASPHALT BASE COURSE - 10"
- (6) CLASS D PATCHES, TYPE I - IV, 4 INCH
- (7) CLASS D PATCHES, TYPE I - IV, 6 INCH
- (8) RESERVED
- (9) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL)
- (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (11) HOT-MIX ASPHALT SHOULDERS, 2 1/2"
- (12) AGGREGATE WEDGE SHOULDER, TYPE B - 2 1/2"
- (13) AGGREGATE SUBGRADE 12"
- (14) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (15) AGGREGATE BASE COURSE, TYPE B 4"
- (16) TOPSOIL FURNISH AND PLACE, 4"
- (17) SODDING, SALT TOLERANT
- (18) RESERVED
- (19) POROUS GRANULAR EMBANKMENT, SUBGRADE** (AS DETERMINED BY THE ENGINEER) - 6" MIN
- (20) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- (21) CENTERLINE SWALE

LEGEND NOTES

- 1. SIDEWALK REMOVAL AND REPLACEMENT TO BE DETERMINED BY THE ENGINEER, UNLESS OTHERWISE NOTED.
- 2. CURB AND GUTTER REMOVAL AND REPLACEMENT TO BE DETERMINED BY THE ENGINEER, UNLESS OTHERWISE SHOWN.

** POROUS GRANULAR EMBANKMENT, SUBGRADE (PGES) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
F.A.U. ROUTE 3877 EAST MAIN STREET

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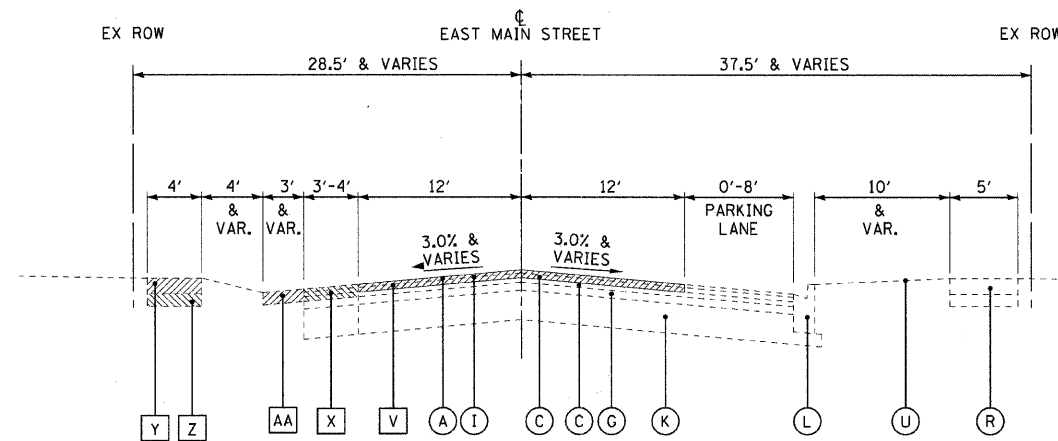


DESIGNED - JJF	REVISED - MCDPD REVIEW 11/3/09
DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-TYP_SEC.shp

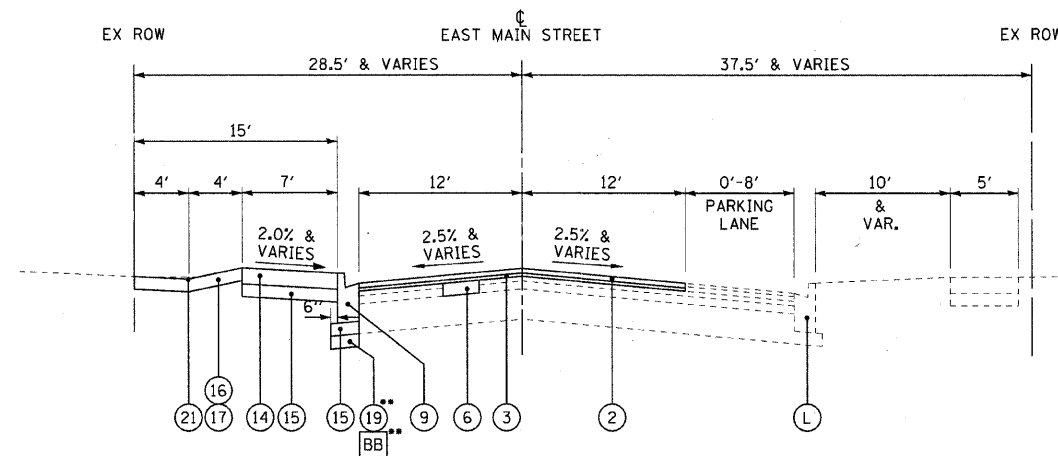
**VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS**

HOT-MIX ASPHALT MIXTURE REQUIREMENTS STRUCTURAL DESIGN DATA AND TYPICAL SECTIONS	
SCALE: NONE	STA. 182+00 TO STA. 185+30

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	08-00054-00-CH	MCHENRY	56	9
C-91-619-09			CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(313)				



EXISTING TYPICAL SECTION
EAST MAIN STREET
 STA. 10+29 TO STA. 18+30
 NOT TO SCALE



PROPOSED TYPICAL SECTION
EAST MAIN STREET
 STA. 10+29 TO STA. 18+30
 NOT TO SCALE

EXISTING LEGEND

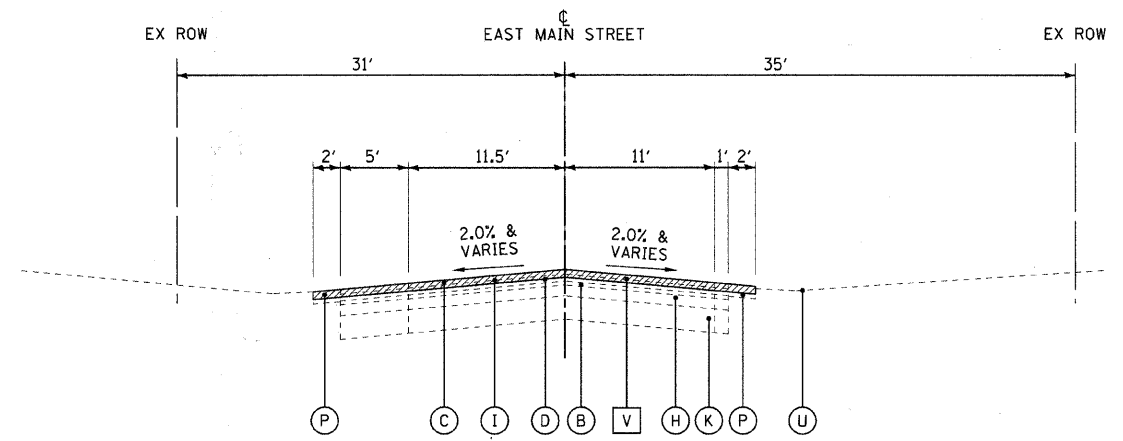
- (A) HOT-MIX ASPHALT SURFACE COURSE, 1"
- (B) HOT-MIX ASPHALT SURFACE COURSE, 1 1/4"
- (C) HOT-MIX ASPHALT SURFACE COURSE, 1 1/2"
- (D) HOT-MIX ASPHALT SURFACE COURSE, 2 1/4"
- (E) HOT-MIX ASPHALT BINDER COURSE, 3"
- (F) HOT-MIX ASPHALT BASE COURSE, 10"
- (G) BITUMINOUS STABILIZED GRANULAR MATERIAL, 2 1/2"
- (H) BITUMINOUS STABILIZED GRANULAR MATERIAL, 3 1/2"
- (I) AREA REFLECTIVE CRACK CONTROL TREATMENT (PETROMAT)
- (J) SUB-BASE GRANULAR MATERIAL, TYPE A, 4"
- (K) UNCRUSHED GRAVEL AND FINES, 7 1/2" - 9 1/2"
- (L) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

EXISTING LEGEND (CON'D.)

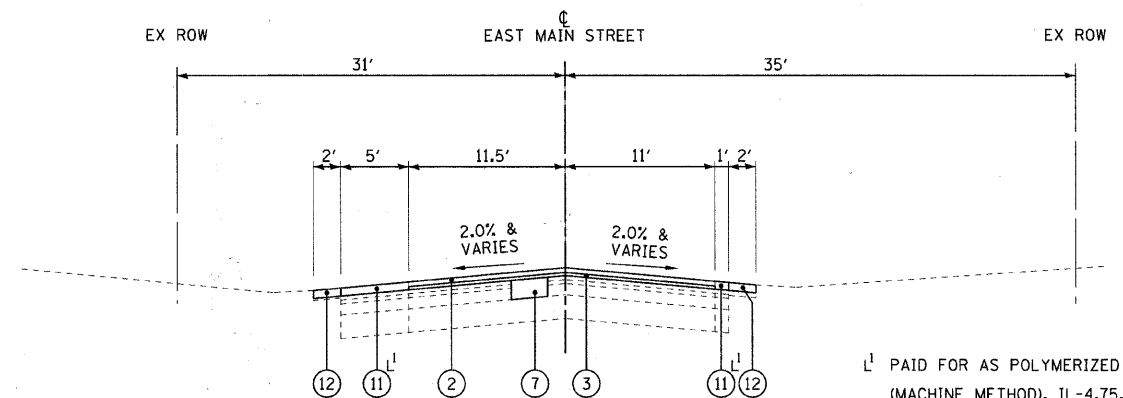
- (M) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (N) HOT-MIX ASPHALT SHOULDER (FULL DEPTH)
- (P) AGGREGATE SHOULDER, 4"
- (R) SIDEWALK
- (S) AGGREGATE BASE COURSE
- (T) EXISTING SUB-GRADE
- (U) GROUND SURFACE
- (V) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (W) COMBINATION CURB AND GUTTER REMOVAL
- (X) PAVEMENT REMOVAL
- (Y) SIDEWALK REMOVAL
- (Z) AGGREGATE BASE COURSE REMOVAL (INCLUDED IN THE EARTH EXCAVATION PAY ITEM)
- (AA) AGGREGATE SHOULDER REMOVAL, 2 1/2" (INCLUDED IN EARTH EXCAVATION PAY ITEM)
- (BB) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS ** ITEM TO BE REMOVED

PROPOSED LEGEND

- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 - 2"
- (2) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
- (4) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 - 2 1/2"
- (5) HOT-MIX ASPHALT BASE COURSE - 10"
- (6) CLASS D PATCHES, TYPE I - IV, 4 INCH
- (7) CLASS D PATCHES, TYPE I - IV, 6 INCH
- (8) RESERVED
- (9) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL)
- (10) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (11) HOT-MIX ASPHALT SHOULDERS, 2 1/2"
- (12) AGGREGATE WEDGE SHOULDER, TYPE B - 2 1/2"
- (13) AGGREGATE SUBGRADE 12"
- (14) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (15) AGGREGATE BASE COURSE, TYPE B 4"
- (16) TOPSOIL FURNISH AND PLACE, 4"
- (17) SODDING, SALT TOLERANT
- (18) RESERVED
- (19) POROUS GRANULAR EMBANKMENT, SUBGRADE** (AS DETERMINED BY THE ENGINEER) - 6" MIN
- (20) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- (21) CENTERLINE SWALE



EXISTING TYPICAL SECTION
EAST MAIN STREET
 STA. 18+30 TO STA. 29+00
 NOT TO SCALE



PROPOSED TYPICAL SECTION
EAST MAIN STREET
 STA. 18+30 TO STA. 29+00
 NOT TO SCALE

1 PAID FOR AS POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1" AND HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"

LEGEND NOTES

1. SIDEWALK REMOVAL AND REPLACEMENT TO BE DETERMINED BY THE ENGINEER, UNLESS OTHERWISE NOTED.
2. CURB AND GUTTER REMOVAL AND REPLACEMENT TO BE DETERMINED BY THE ENGINEER, UNLESS OTHERWISE SHOWN.

** POROUS GRANULAR EMBANKMENT, SUBGRADE (PGES) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSUITABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
 F.A.U. ROUTE 3677 EAST MAIN STREET

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 LICENSE NO. 08-00054-00-CH EXPIRES 4/30/2011
 PROJECT NO. 08-00054-00-CH DRAWING NO. 26-9000
 DATE 09/11/09



DESIGNED - JJF	REVISED - MCDPD REVIEW 11/3/09
DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-TYP_SEC.shp

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

SCALE: NONE

STA. 10+29 TO STA. 29+00

TYPICAL SECTIONS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
08-00054-00-CH		McHENRY	56	10
C-91-619-09			CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-ARA-9003(313)				

**HOT-MIX ASPHALT SURFACE REMOVAL
- BUTT JOINT**

LOCATION	SQ YD
U.S. ROUTE 14 (STA 10+29)	67
COMMERCIAL DRIVEWAY (STA 13+20.5, RT)	45
PARKING AREA (STA 14+50, 12' RT)	225
COMMERCIAL DRIVEWAY (STA 15+62, RT)	71
PARKING AREA (STA 16+50, 12' RT)	97
MONTANA DRIVE (18+30, LT)	29
DRIVEWAY, 304 (19+13, LT)	19
DRIVEWAY, 340 (22+05.5, LT)	19
DRIVEWAY, 356 (24+63, LT)	17
DECKER DRIVE (26+58, RT)	38
DRIVEWAY, 1515 (26+92.5, RT)	33
DRIVEWAY, 1505 (28+04, RT)	20
EAST MAIN STREET (STA 29+00)	30
TOTAL	710

DRIVEWAYS

STATION	RESID. OR COMM.	DRIVEWAY PAVEMENT AREA (SQ YD)	PCC DW. PVT. 6" (SQ YD)	AGGREGATE BASE COURSE TYPE B - 4" (SQ YD)	DRIVEWAY PAVEMENT REMOVAL (SQ YD)
EAST MAIN STREET					
12+54 LT	R	13	13	13	17
13+15 LT	R	12	12	12	-
13+78 LT	R	11	11	11	2
14+34 LT	R	18	18	18	22
14+94 LT	R	11	11	11	14
15+55 LT	R	11	11	11	11
16+12 LT	R	12	12	12	12
16+82 LT	R	13	13	13	12
TOTALS		101	101	101	90

EARTH EXCAVATION

LOCATION	UNDERCUT AND PGE REPLACEMENT (CY)	UNSUITABLE EXCAVATION (TOPSOIL) (CY)	REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL (CY)	EARTH EXCAVATION (CY)	UTILITY EXCAVATION (CY)	EXCESS STRUCTURE EXCAVATION (CY)	TOTAL SUITABLE EXCAVATION (CY)	EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE) (CY)	EMBANKMENT (CY)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CY)
STA TO STA										
US ROUTE 14										
STA 182+78 TO STA 183+00	6.0	6.0	12.0	22.5	1.0	0	23.5	20.0	2.2	17.8
STA 183+00 TO STA 184+00	30.0	27.7	57.7	91.5	3.0	0	94.5	80.4	19.9	60.5
STA 184+00 TO STA 185+00	30.0	27.6	57.6	122.3	22.0	0	144.3	122.7	15.0	107.7
STA 185+00 TO STA 185+30	6.0	4.1	10.1	24.6	0.0	0	24.6	21.0	0.0	21.0
SUBTOTAL	72	66	138	261	26	0	287	245	38	208
EAST MAIN STREET										
STA 10+29 TO STA 10+42	12.0	6.7	18.7	36.3	0.0	0.0	36.3	30.9	0.5	30
STA 10+42 TO STA 11+00	3.0	24.8	27.8	137.3	0.0	0.0	137.3	116.7	2.4	114
STA 11+00 TO STA 12+00	0.0	18.3	18.3	106.8	0.0	0.0	106.8	90.8	4.8	86
STA 12+00 TO STA 13+00	0.0	5.2	5.2	16.4	0.0	0.0	16.4	14.0	4.2	10
STA 13+00 TO STA 14+00	0.0	3.4	3.4	13.3	39.0	0.0	52.3	44.5	4.5	40
STA 14+00 TO STA 15+00	0.0	2.4	2.4	13.6	8.0	0.0	21.6	18.4	5.5	13
STA 15+00 TO STA 16+00	0.0	2.4	2.4	13.6	10.0	0.0	23.6	20.1	5.5	15
STA 16+00 TO STA 17+00	0.0	7.4	7.4	11.3	16.0	0.0	27.3	23.3	8.1	15
STA 17+00 TO STA 18+00	0.0	36.7	36.7	14.6	8.0	0.0	22.6	19.2	26.6	-7
STA 18+00 TO STA 18+30	0.0	9.4	9.4	6.9	0.0	0.0	6.9	5.9	12.9	-7
SUBTOTAL	15	117	132	370	81	0	451	384	75	309
TOTAL	87	183	270	631	107	0	738	629	113	517

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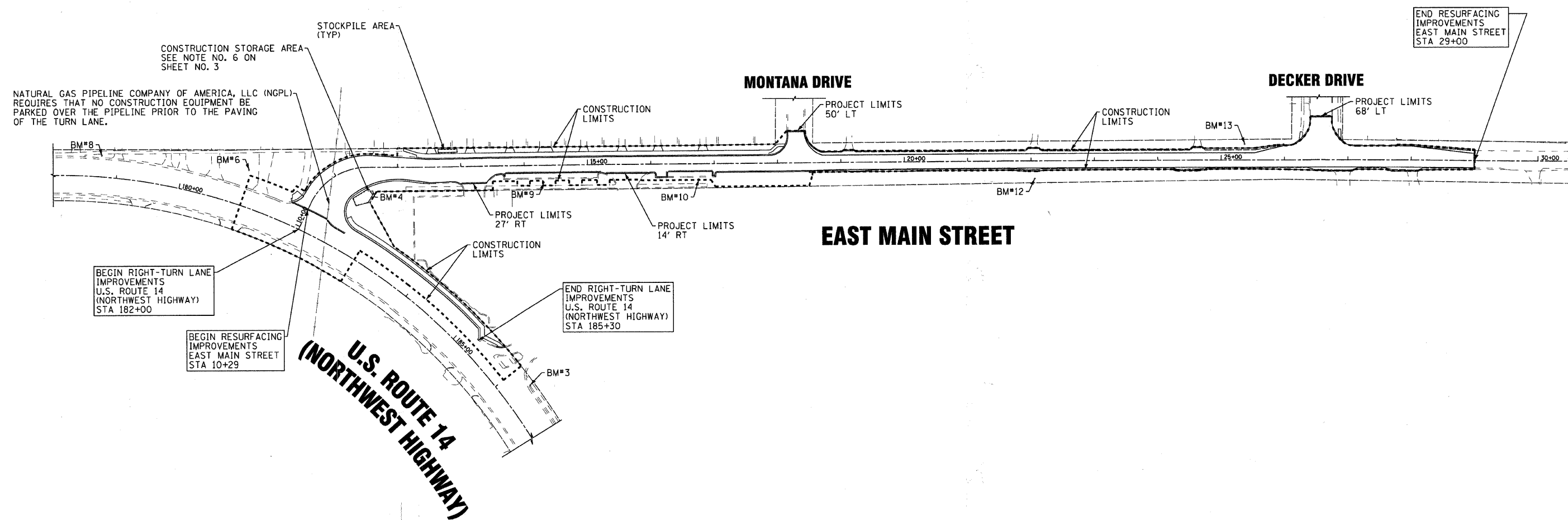
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DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-SCHEDULES.sht

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

SCALE: NONE		STA. TO STA.	

SCHEDULE OF QUANTITIES

F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY) F.A.U. ROUTE 3877 EAST MAIN STREET			
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS
	08-00054-00-CH	MCHENRY	56 11
C-91-619-09		CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-ARA-9003131			



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 STATE OF ILLINOIS - PROFESSIONAL ENGINEERING
 LICENSE NO. 08-00054-00-CH
 DATE: 02/12/2009
 PROJECT: VILLAGE CARY, ILLINOIS
 SHEET: 080298-LAYOUT.SHT



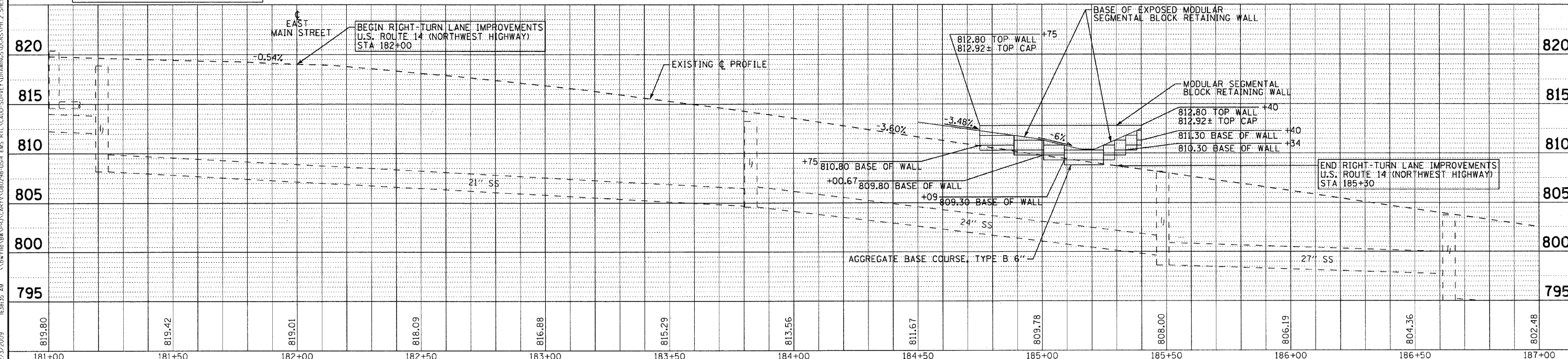
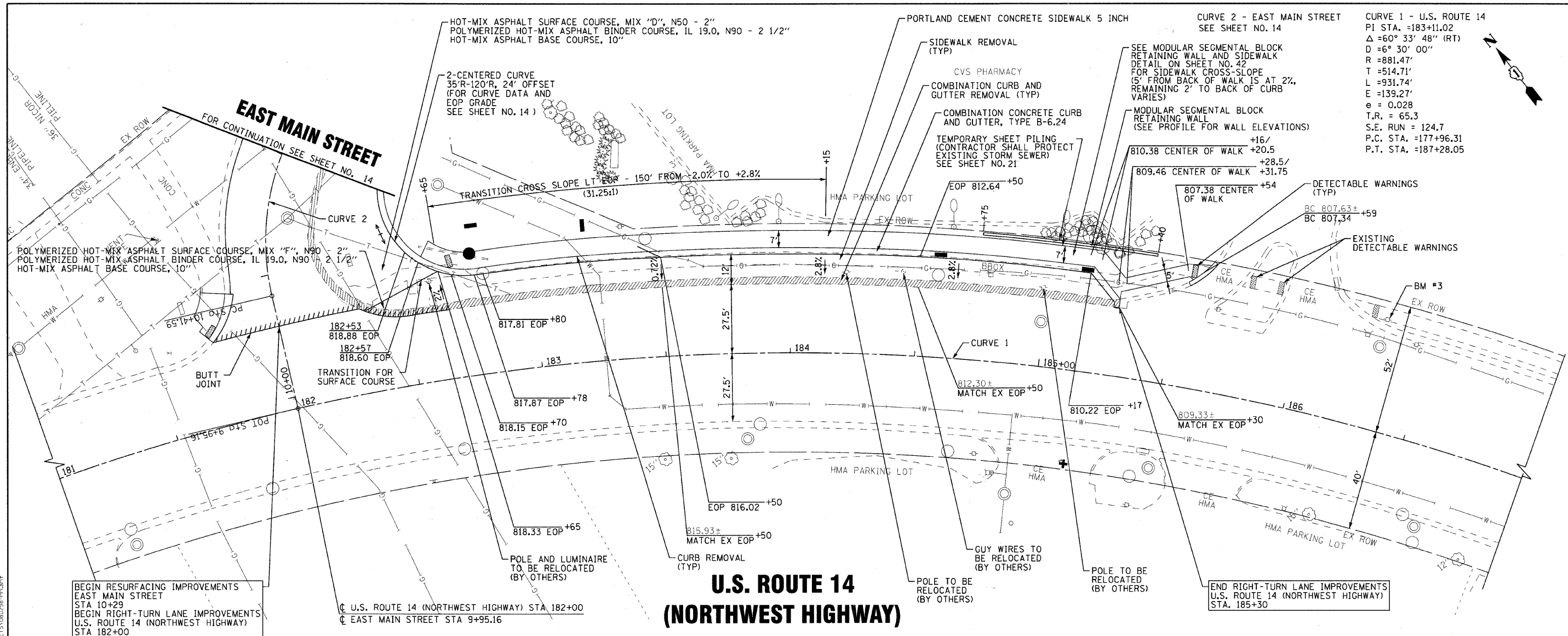
DESIGNED - JJF	REVISED - MCDP REVIEW 11/3/09
DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-LAYOUT.sht

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

OVERALL LAYOUT	
SCALE: 1" = 80'	STA. 182+00 TO STA. 185+30
	10+29 29+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	08-00054-00-CH	McHENRY	56	12
C-91-619-09			CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003313				

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
 F.A.U. ROUTE 3877 EAST MAIN STREET



819.80	819.42	819.01	818.09	816.88	815.29	813.56	811.67	809.78	808.00	806.19	804.36	802.48
181+00	181+50	182+00	182+50	183+00	183+50	184+00	184+50	185+00	185+50	186+00	186+50	187+00

DESIGNED - MWP

DRAWN - CJC

CHECKED - RWL

DATE - 09/11/09

REVISED - MCDP REVIEW 11/3/09

REVISED - IDOT REVIEW 11/17/09

REVISED -

FILE - 080298-PP1.sh

VILLAGE CARY, ILLINOIS

U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP

AND EAST MAIN STREET ARRA IMPROVEMENTS

PLAN AND PROFILE - U.S. ROUTE 14

(NORTHWEST HIGHWAY) STA 181+00 TO STA 187+00

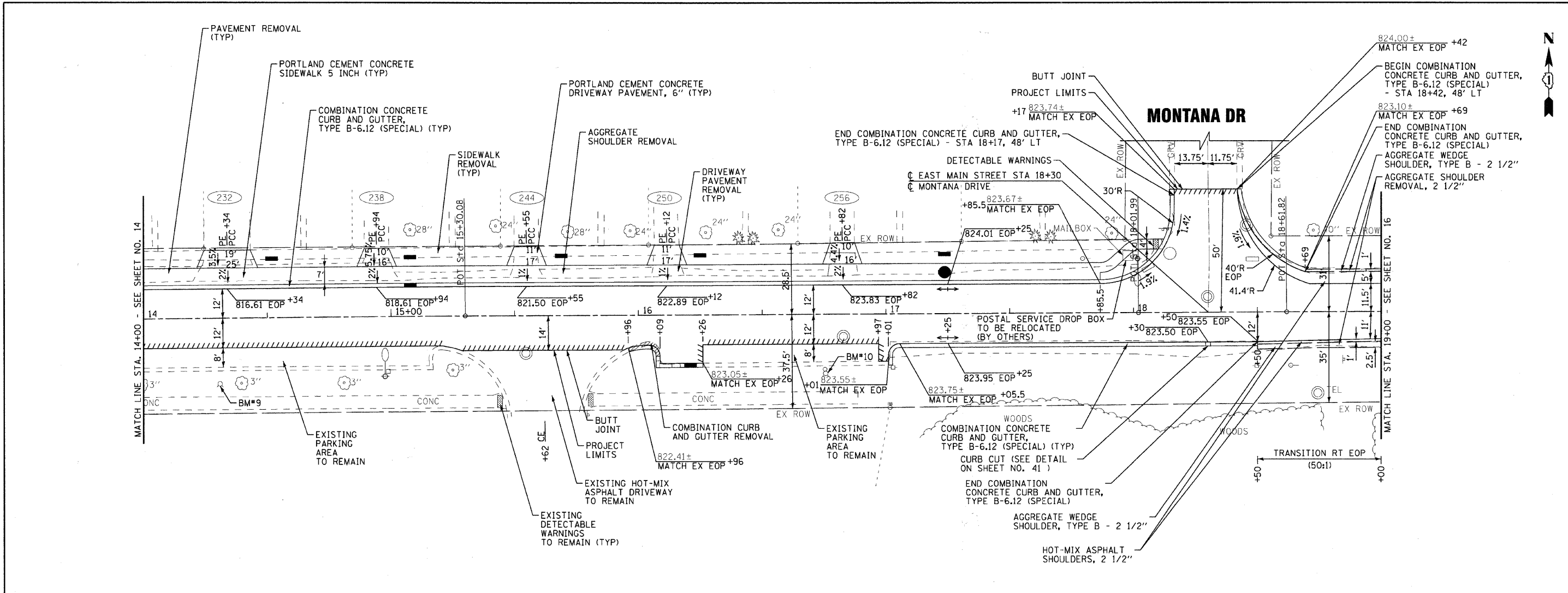
SCALE: H: 1"=20' V: 1"=5'

STA. 181+00 TO STA. 187+00

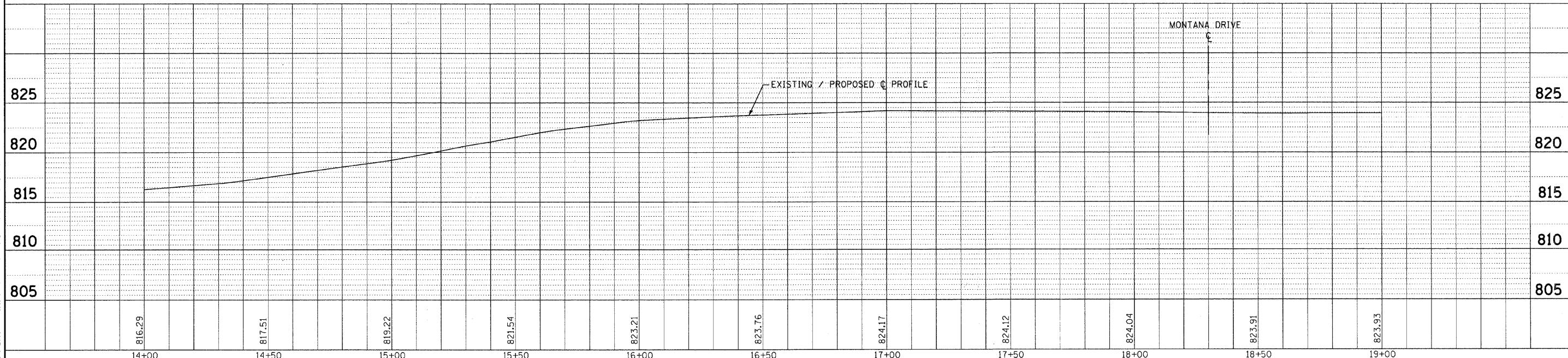
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
C-91-619-09	08-00054-00-CH	MCHENRY	56	13
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT W-ARA-9003313			CONTRACT NO. 63404	
F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)				
F.A.U. ROUTE 3877 EAST MAIN STREET				

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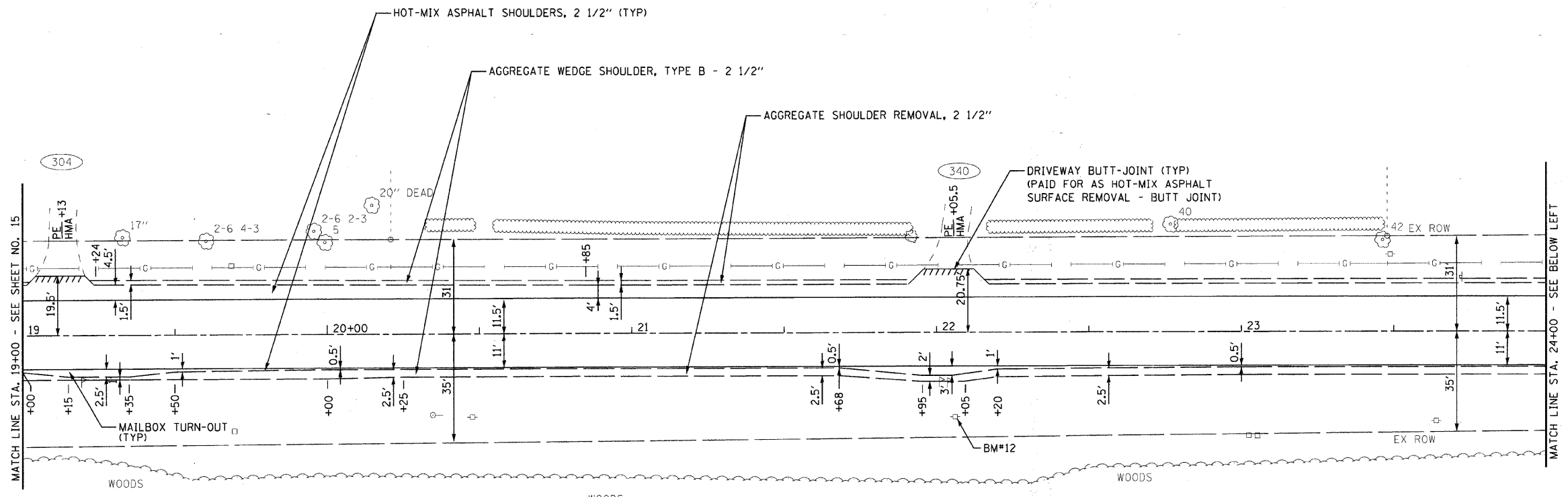


EAST MAIN ST

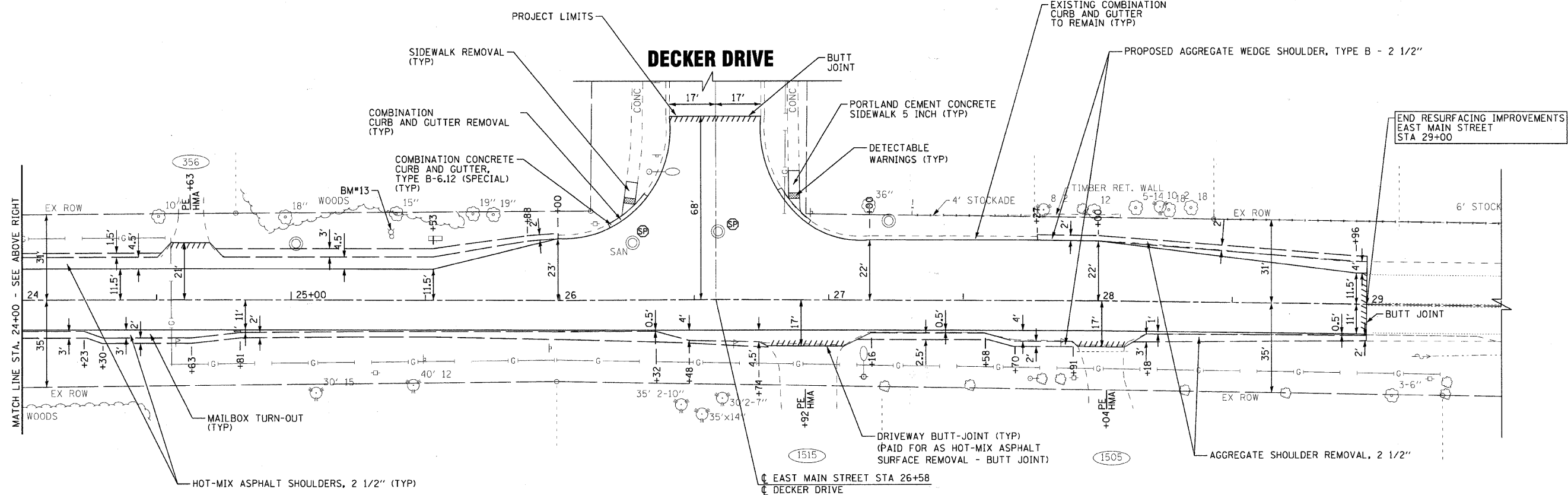


	DESIGNED - MWP DRAWN - CJC CHECKED - RWL DATE - 09/11/09	REVISED - MCDPD REVIEW 11/3/09 REVISED - IDOT REVIEW 11/17/09 REVISED FILE - 080298-PP3.shp	VILLAGE CARY, ILLINOIS U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP AND EAST MAIN STREET ARRA IMPROVEMENTS	PLAN AND PROFILE EAST MAIN STREET	F.A. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.	C-91-619-09 08-00054-00-CH MCHENRY 56 15	CONTRACT NO. 63404 FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT W-ARA-9003(313) F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY) F.A.U. ROUTE 3877 EAST MAIN STREET
--	---	--	--	--	---	--	--

SCALE: H: 1"=20' V: 1"=5'
 STA. 14+00 TO STA. 19+00



EAST MAIN STREET



EAST MAIN STREET

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 2/27/2005
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DESIGNED - JJF	REVISED - MCDPD REVIEW 11/3/09
DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED
DATE - 09/11/09	FILE - 080298-PP4.sht

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

PLAN EAST MAIN STREET

SCALE: 1"=20'
 STA. 19+00 TO STA. 29+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
C-91-619-09	08-00054-00-CH	MCHENRY	56	16
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-90031313			CONTRACT NO. 63404	

SOIL EROSION AND SEDIMENT CONTROL NOTES

1. SEDIMENTATION AND EROSION CONTROL STANDARDS

THE FOLLOWING STANDARDS MUST BE SATISFIED:

- A. ALL AREAS LOCATED DOWNSTREAM FROM DISTURBED AREAS OF A DEVELOPMENT SITE SHALL BE PROTECTED FROM POTENTIAL INCREASE OF EROSION AND SEDIMENTATION RESULTING FROM UPSTREAM ACTIVITIES.
- B. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED AND FUNCTIONAL PRIOR TO OR CONCURRENTLY WITH THE START OF DISTURBANCE.
- C. PERMANENT STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICAL IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED; BUT IN NO CASE SHALL THESE MEASURES BE INSTALLED MORE THAN 14 DAYS AFTER THE CONSTRUCTION IN THE AREA TEMPORARILY OR PERMANENTLY CEASES.

2. SEDIMENTATION AND EROSION CONTROL METHODS

THE FOLLOWING SEDIMENTATION AND EROSION CONTROL METHODS MUST BE INSTALLED AND MAINTAINED:

- A. ALL STORM SEWER STRUCTURES THAT RECEIVE RUNOFF DURING CONSTRUCTION SHALL INCLUDE INLET PROTECTION TO PREVENT DEBRIS AND EXCESSIVE SEDIMENT FROM ENTERING THE STORM SEWER PIPING SYSTEM. THESE PROTECTIVE MEASURES SHALL BE PROPERLY INSTALLED, MAINTAINED, AND REMOVED IN THEIR ENTIRETY AFTER THE AREA TRIBUTARY TO THE STORM STRUCTURE IS STABILIZED.
- B. DISCHARGES FROM DEWATERING OPERATIONS SHALL ENTER OR BE ROUTED TO A SEDIMENT AND EROSION CONTROL SYSTEM OR DEVICE.
- C. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT SHALL BE PROPERLY STABILIZED OR DISPOSED.
- D. A STABILIZED CONSTRUCTION ENTRANCE SHALL BE LOCATED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING THE CONSTRUCTION SITE. THE STABILIZED CONSTRUCTION ENTRANCE SHALL BE PERIODICALLY MAINTAINED TO REMOVE MUD AND DEBRIS. THE STABILIZED CONSTRUCTION ENTRANCE SHALL CONSIST OF AN APPROPRIATE GEOTEXTILE FABRIC COVERED WITH AT LEAST SIX INCHES OF CLEAN STONE THAT IS AT LEAST TWO (2) INCHES IN DIAMETER.
- E. TEMPORARY EARTHEN EMBANKMENTS, GRADE TRANSITIONS AND BERMS SHALL BE CONSTRUCTED WITH SIDE SLOPES NO STEEPER THAN 3H:1V. SIDE SLOPES BELOW NORMAL WATER LEVEL SHALL BE NO STEEPER THAN 2H:1V. MORE GRADUAL SLOPES MAY BE REQUIRED FOR SOILS THAT EASILY ERODE.

STRAW BALES SHALL NOT BE USED AS TEMPORARY DITCH CHECKS OR PERIMETER EROSION BARRIER. THE USE OF OTHER CONTROL METHODS MUST FIRST BE APPROVED BY THE VILLAGE OF CARY.

3. MAINTENANCE

ALL TEMPORARY MEASURES AND PERMANENT EROSION AND SEDIMENT CONTROL MUST BE MAINTAINED IN AN EFFECTIVE WORKING CONDITION AS IDENTIFIED BY REQUIRED INSPECTIONS. THIS INCLUDES, BUT IS NOT LIMITED TO, THE FOLLOWING:

- A. REPAIR, REPLACE OR MAINTAIN EROSION AND SEDIMENT CONTROL STRUCTURES AFTER A SINGULAR OR CUMULATIVE RAINFALL EVENT(S) OF 0.5 INCH OR MORE OVER A TWENTY-FOUR-HOUR PERIOD.
- B. MAKE ADJUSTMENTS TO THE SEDIMENTATION AND EROSION CONTROL PLAN AND METHODS, AS NEEDED, TO ACCOMPLISH THE INTENDED PURPOSE.
- C. ALL ADJACENT ROADWAYS MUST BE KEPT CLEAR OF DEBRIS, INSPECTED DAILY, AND CLEANED WHEN NECESSARY OR AS DETERMINED BY THE VILLAGE OF CARY OR THE ENGINEER.

4. INSPECTIONS

- A. THESE PLANS FOR UPGRADING, STRIPPING, EXCAVATING, AND FILLING WORK, BEAR THE STAMP OF APPROVAL OF THE ENFORCEMENT OFFICER. THESE PLANS SHALL BE MAINTAINED AT THE SITE DURING THE PROGRESS OF THE WORK. THE DISTRICT AND/OR ENGINEER SHALL MAKE INSPECTIONS AND MAINTAIN ON-SITE RECORDS OF SUCH INSPECTIONS AT THE INTERVALS SPECIFIED BELOW:
 - I. UPON COMPLETION OF INSTALLATION OF SEDIMENT AND RUNOFF CONTROL MEASURES (INCLUDING PERIMETER CONTROLS AND DIVERSIONS), PRIOR TO PROCEEDING WITH ANY OTHER EARTH DISTURBANCE OR GRADING;
 - II. AFTER ROUGH GRADING;
 - III. AFTER FINAL GRADING; AND
 - IV. WEEKLY AND AFTER EACH RAINFALL EVENT OF 0.5 INCH OR MORE OVER A TWENTY-FOUR-HOUR PERIOD.
- B. ANY NECESSARY REPAIRS TO SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MADE AND REPORTED IN THE ON-SITE INSPECTION RECORDS. COPIES OF THE INSPECTION RECORDS SHALL BE SUBMITTED TO THE ENFORCEMENT OFFICER IN A MONTHLY INSPECTION REPORT.

5. NOTIFICATIONS

TO FACILITATE ENFORCEMENT OFFICER COMPLIANCE INSPECTIONS THE DISTRICT AND/OR ENGINEER SHALL NOTIFY THE ENFORCEMENT OFFICER WITHIN TWO (2) WORKING DAYS OF THE COMPLETION OF THE CONSTRUCTION STAGES SPECIFIED BELOW:

- A. UPON COMPLETION OF INSTALLATION OF SEDIMENT AND RUNOFF CONTROL (CONTROLS AND DIVERSIONS), PRIOR TO PROCEEDING WITH ANY OTHER EARTH DISTURBANCE OR GRADING;
- B. AFTER ROUGH GRADING;
- C. AFTER FINAL STABILIZATION AND LANDSCAPING, PRIOR TO REMOVAL OF SEDIMENT CONTROLS.

IF STRIPPING, CLEARING, GRADING AND/OR LANDSCAPING ARE TO BE DONE IN PHASES OR AREAS, THE DISTRICT AND/OR ENGINEER SHALL GIVE NOTICE AT THE COMPLETION OF EACH OF THE ABOVE WORK STAGES IN EACH PHASE OR AREA.

6. SPECIAL PRECAUTIONS

- A. IF AT ANY STAGE OF THE GRADING ON THE SITE, THE ENFORCEMENT OFFICER DETERMINES BY INSPECTION THAT THE NATURE OF THE SITE IS SUCH THAT FURTHER WORK AUTHORIZED BY AN EXISTING PERMIT IS LIKELY TO IMPERIL ANY PROPERTY, PUBLIC WAY, STREAM, LAKE, WETLAND, OR DRAINAGE STRUCTURE, THE ENFORCEMENT OFFICER MAY REQUIRE, AS A CONDITION OF ALLOWING THE WORK TO BE DONE, THAT SUCH REASONABLE SPECIAL PRECAUTIONS BE TAKEN AS ARE CONSIDERED ADVISABLE TO AVOID THE LIKELIHOOD OF SUCH PERIL. "SPECIAL PRECAUTIONS" MAY INCLUDE, BUT SHALL NOT BE LIMITED TO, A MORE LEVEL EXPOSED SLOPE, CONSTRUCTION OF ADDITIONAL DRAINAGE FACILITIES, BERMS, TERRACING, COMPACTION, OR CRIBBING, INSTALLATION OF PLANT MATERIALS FOR EROSION CONTROL, AND RECOMMENDATIONS OF A REGISTERED SOILS ENGINEER AND/OR ENGINEERING GEOLOGIST WHICH MAY OUTLINE REQUIREMENTS FOR FURTHER WORK.
- B. WHERE IT APPEARS THAT STORM DAMAGE MAY OCCUR DUE TO INCOMPLETE GRADING AT SITE, WORK MAY BE STOPPED AND THE CONTRACTOR REQUIRED TO INSTALL TEMPORARY STRUCTURES OR TAKE SUCH OTHER MEASURES AS MAY BE REQUIRED TO PROTECT ADJOINING PROPERTY OR THE PUBLIC SAFETY. WHERE UNUSUAL SITE CONDITIONS PREVAIL, THE ENFORCEMENT OFFICER MAY SPECIFY THE START AND END DATES FOR GRADING OPERATIONS OR MAY REQUIRE THAT THE OPERATIONS BE CONDUCTED IN SPECIFIC STAGES SO AS TO ENSURE COMPLETION OF PROTECTIVE MEASURES OR DEVICES PRIOR TO THE ADVENT OF SEASONAL RAINS.

7. MCHENRY COUNTY SOIL EROSION AND SEDIMENT CONTROL

- A. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF THE DEVELOPMENT SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SEEDING IS PERFORMED.
- B. SOIL AND MATERIAL STOCKPILES SHALL NOT BE LOCATED IN A FLOOD-PRONE AREA OR A DESIGNATED BUFFER PROTECTING WATERS OF THE UNITED STATES OR ISOLATED WATERS OF MCHENRY COUNTY.
- C. THE CONTRACTOR SHALL PROVIDE ADEQUATE RECEPTACLES FOR THE DISPOSITION OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE DEVELOPMENT PROCESS. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL DEBRIS UPON OR INTO ANY DEVELOPMENT SITE, CHANNEL, EATERS OF THE U.S. OR ISOLATED WATERS OF MCHENRY COUNTY. THE CONTRACTOR SHALL MAINTAIN THE DEVELOPMENT SITE FREE OF CONSTRUCTION MATERIAL DEBRIS.

SCHEDULE OF EROSION CONTROL QUANTITIES

TEMPORARY DITCH CHECKS	FOOT	14
PERIMETER EROSION BARRIER (INCLUDES MAINTENANCE)	FOOT	318
INLET FILTERS (INCLUDES MAINTENANCE)	EACH	30
TREE TRUNK PROTECTION	EACH	5
TREE ROOT PRUNING	EACH	1
TEMPORARY EROSION CONTROL SEEDING	POUND	74
MULCH, METHOD 1	ACRE	0.74

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 2630 S. ...
 11/17/09



DESIGNED - JJF	REVISED - MCDPD REVIEW 11/3/09
DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-ER.NOTES.sht

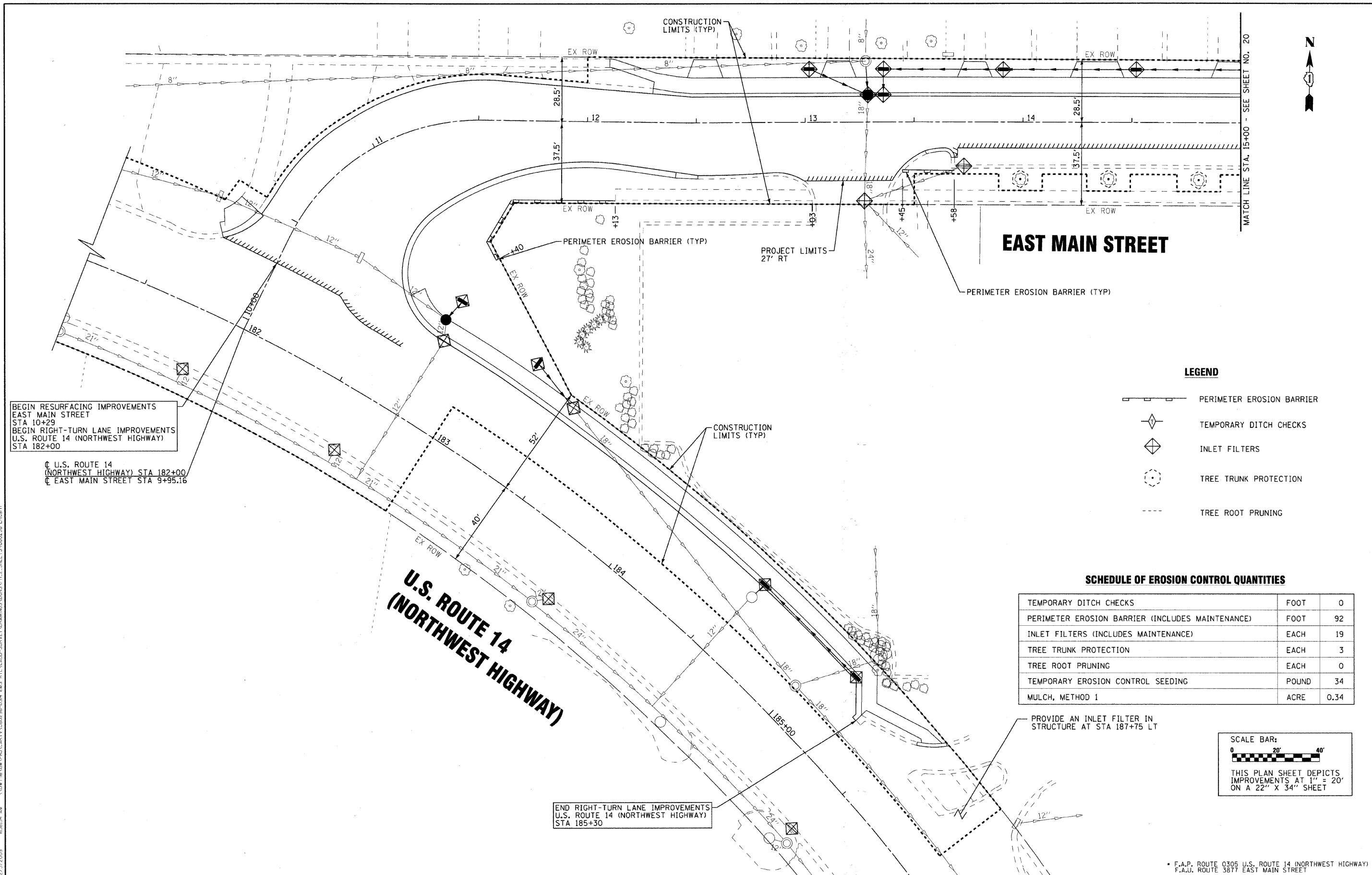
VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

EROSION AND SEDIMENT CONTROL NOTES

SCALE: NONE STA. TO STA.

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
 F.A.J. ROUTE 3877 EAST MAIN STREET

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	08-00054-00-CH	MCHENRY	56	18
C-91-619-09			CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(313)				



BEGIN RESURFACING IMPROVEMENTS
EAST MAIN STREET
STA 10+29
BEGIN RIGHT-TURN LANE IMPROVEMENTS
U.S. ROUTE 14 (NORTHWEST HIGHWAY)
STA 182+00

U.S. ROUTE 14
(NORTHWEST HIGHWAY) STA 182+00
EAST MAIN STREET STA 9+95.16

END RIGHT-TURN LANE IMPROVEMENTS
U.S. ROUTE 14 (NORTHWEST HIGHWAY)
STA 185+30

LEGEND

- PERIMETER EROSION BARRIER
- TEMPORARY DITCH CHECKS
- INLET FILTERS
- TREE TRUNK PROTECTION
- TREE ROOT PRUNING

SCHEDULE OF EROSION CONTROL QUANTITIES

TEMPORARY DITCH CHECKS	FOOT	0
PERIMETER EROSION BARRIER (INCLUDES MAINTENANCE)	FOOT	92
INLET FILTERS (INCLUDES MAINTENANCE)	EACH	19
TREE TRUNK PROTECTION	EACH	3
TREE ROOT PRUNING	EACH	0
TEMPORARY EROSION CONTROL SEEDING	POUND	34
MULCH, METHOD 1	ACRE	0.34

PROVIDE AN INLET FILTER IN
STRUCTURE AT STA 187+75 LT

SCALE BAR:
0 20' 40'

THIS PLAN SHEET DEPICTS
IMPROVEMENTS AT 1" = 20'
ON A 22" X 34" SHEET

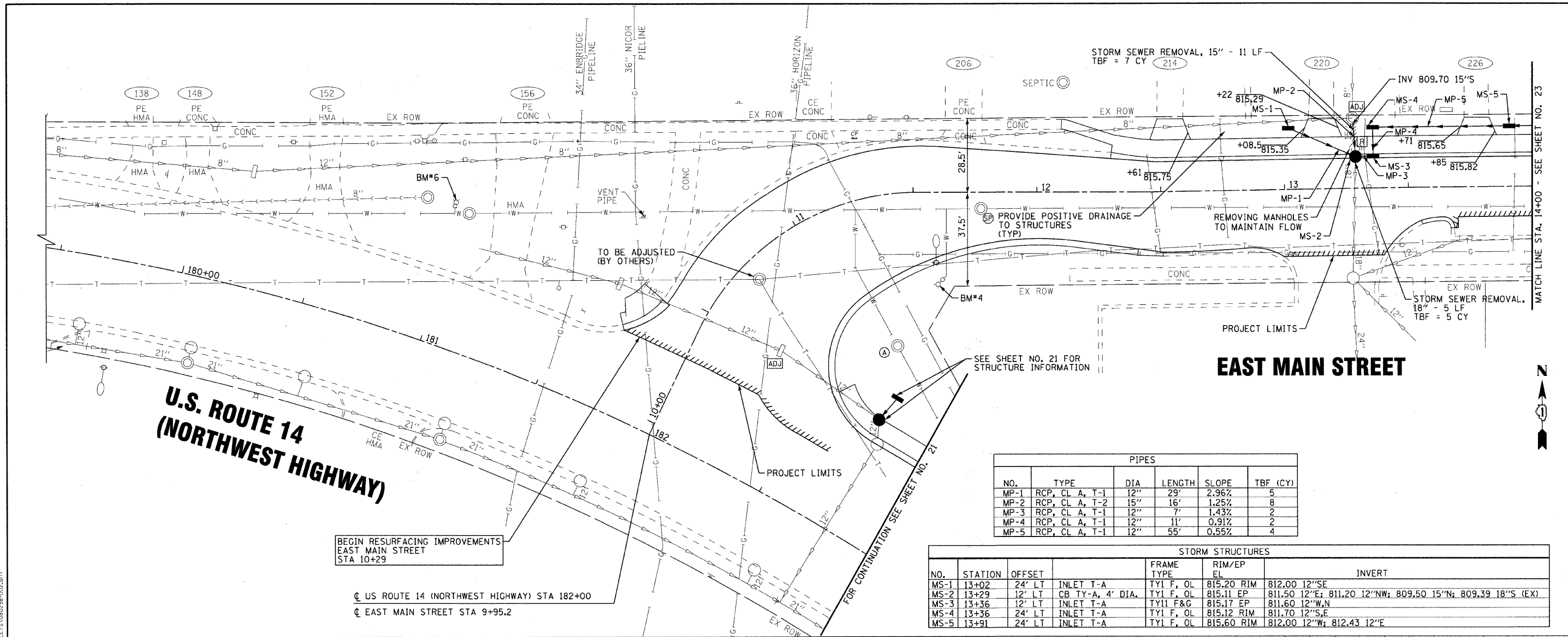
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DESIGNED - JJF	REVISED - MCDPD REVIEW 11/3/09
DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-ER1.sht

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

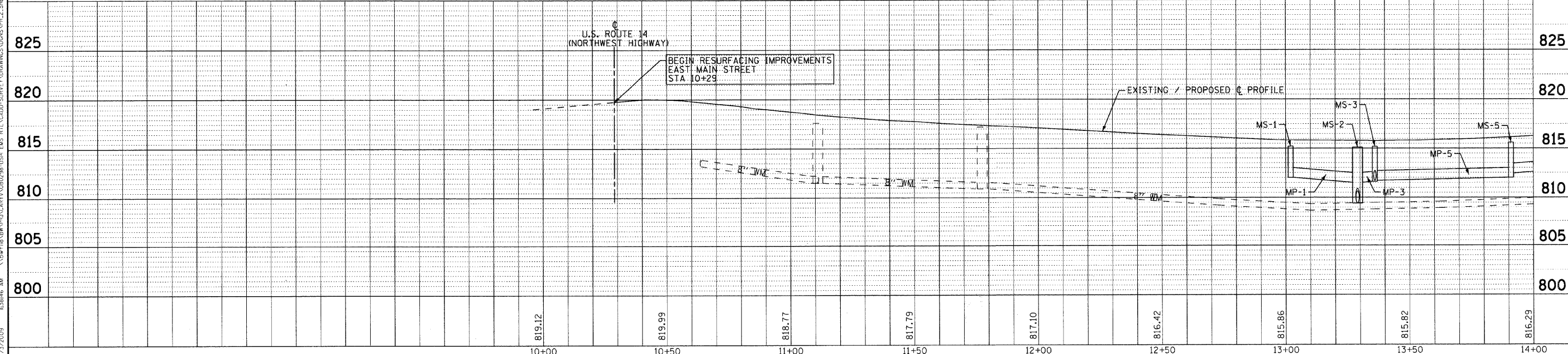
EROSION AND SEDIMENT CONTROL PLAN
U.S. ROUTE 14 AND EAST MAIN STREET
 SCALE: 1" = 20'
 STA. 181+00 TO STA. 187+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	08-00054-00-CH	McHENRY	56	19
C-91-619-09			CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-ARA-9003(313)				



PIPES					
NO.	TYPE	DIA	LENGTH	SLOPE	TBF (CY)
MP-1	RCP, CL A, T-1	12"	29'	2.96%	5
MP-2	RCP, CL A, T-2	15"	16'	1.25%	8
MP-3	RCP, CL A, T-1	12"	7'	1.43%	2
MP-4	RCP, CL A, T-1	12"	11'	0.91%	2
MP-5	RCP, CL A, T-1	12"	55'	0.55%	4

STORM STRUCTURES						
NO.	STATION	OFFSET	FRAME TYPE	RIM/EP EL	INVERT	
MS-1	13+02	24' LT	INLET T-A	TY1 F, OL 815.20 RIM	812.00 12"SE	
MS-2	13+29	12' LT	CB TY-A, 4' DIA.	TY1 F, OL 815.11 EP	811.50 12"E; 811.20 12"NW; 809.50 15"N; 809.39 18"S (EX)	
MS-3	13+36	12' LT	INLET T-A	TY11 F&G 815.17 EP	811.60 12"W,N	
MS-4	13+36	24' LT	INLET T-A	TY1 F, OL 815.12 RIM	811.70 12"S,E	
MS-5	13+91	24' LT	INLET T-A	TY1 F, OL 815.60 RIM	812.00 12"W; 812.43 12"E	



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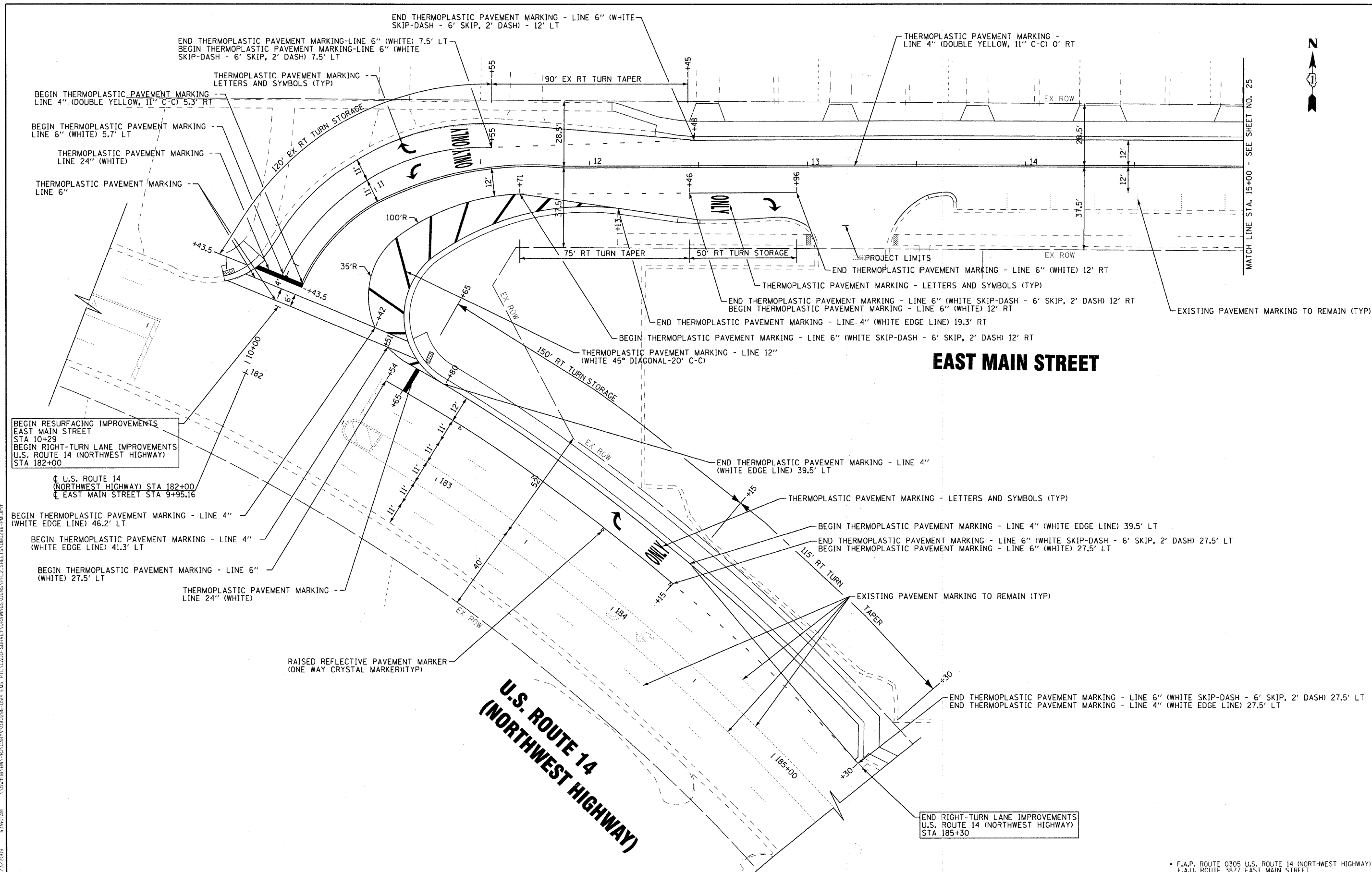


DESIGNED - MWP	REVISED - MCDPD REVIEW 11/3/09
DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-DU2.sh

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

DRAINAGE AND UTILITIES EAST MAIN STREET	
SCALE: H: 1"=20' V: 1"=5'	STA. 9+96 TO STA. 14+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
C-91-619-09	08-00054-00-CH	MCHENRY	56	22
CONTRACT NO. 63404			FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT W-ARA-9003(3)	
F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY) F.A.U. ROUTE 3877 EAST MAIN STREET				



BEGIN RESURFACING IMPROVEMENTS
EAST MAIN STREET
STA 10+29
BEGIN RIGHT-TURN LANE IMPROVEMENTS
U.S. ROUTE 14 (NORTHWEST HIGHWAY)
STA 182+00

U.S. ROUTE 14
(NORTHWEST HIGHWAY) STA 182+00
EAST MAIN STREET STA 9+95.16

BEGIN THERMOPLASTIC PAVEMENT MARKING - LINE 4"
(WHITE EDGE LINE) 46.2' LT
BEGIN THERMOPLASTIC PAVEMENT MARKING - LINE 4"
(WHITE EDGE LINE) 41.3' LT
BEGIN THERMOPLASTIC PAVEMENT MARKING - LINE 6"
(WHITE) 27.5' LT
THERMOPLASTIC PAVEMENT MARKING -
LINE 24" (WHITE)

RAISED REFLECTIVE PAVEMENT MARKER
(ONE WAY CRYSTAL MARKER)(TYP)

END RIGHT-TURN LANE IMPROVEMENTS
U.S. ROUTE 14 (NORTHWEST HIGHWAY)
STA 185+30

EAST MAIN STREET

U.S. ROUTE 14 (NORTHWEST HIGHWAY)



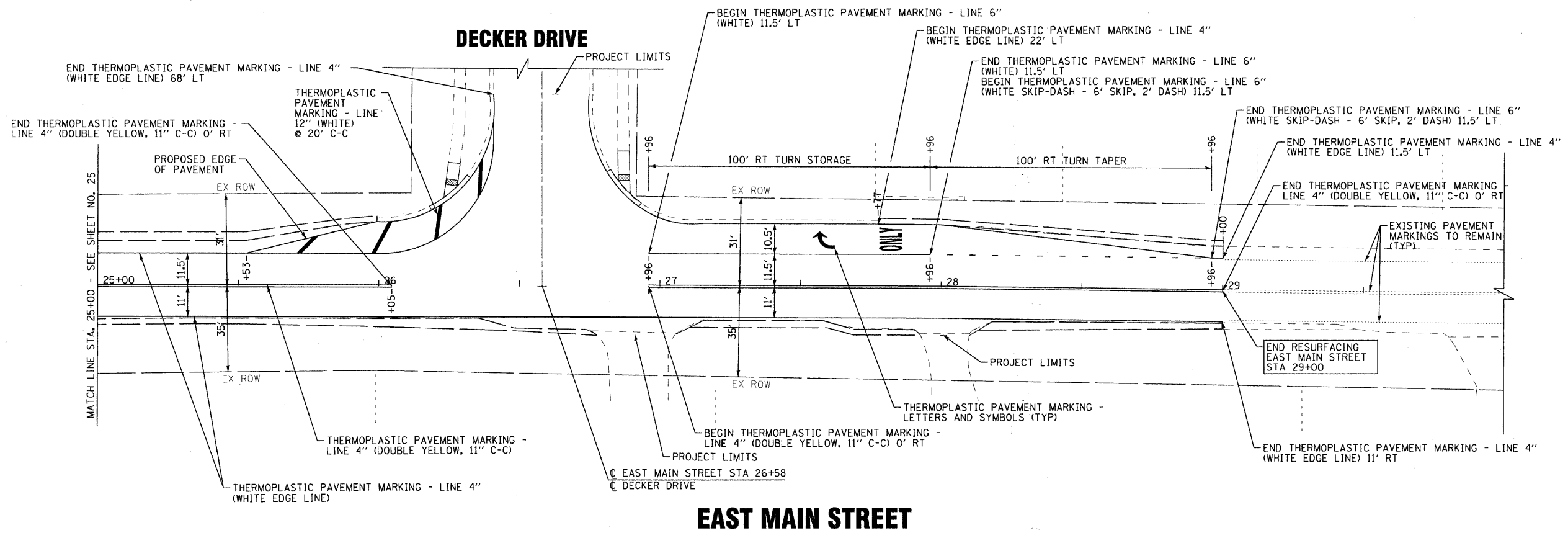
DESIGNED - JUF	REVISED - MCDPD REVIEW 11/3/09
DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-PM1.shp

VILLAGE CARY, ILLINOIS U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP AND EAST MAIN STREET ARRA IMPROVEMENTS

PAVEMENT MARKING PLAN	
U.S. ROUTE 14 AND EAST MAIN STREET	
SCALE: 1" = 20'	STA. 10+00 TO STA. 15+00

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY) F.A.J. ROUTE 3877 EAST MAIN STREET				
F.A.P. No.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
C-91-619-09	08-0054-00-CH	McHENRY	56	24
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-90031313			CONTRACT NO. 63404	

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DESIGNED - JUF	REVISED - MCDPD REVIEW 11/3/09
DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-PM3.shp

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

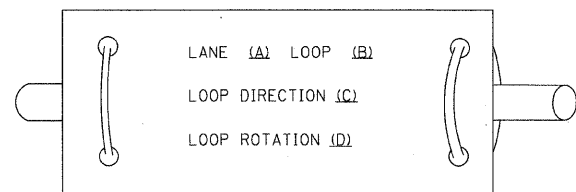
PAVEMENT MARKING PLAN	
EAST MAIN STREET	
SCALE: 1" = 20'	STA. 25+00 TO STA. 29+00

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY) F.A.U. ROUTE 3877 EAST MAIN STREET				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
08-00054-00-CH		McHENRY	56	26
C-91-619-09			CONTRACT NO. 63404	
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(313)</small>				

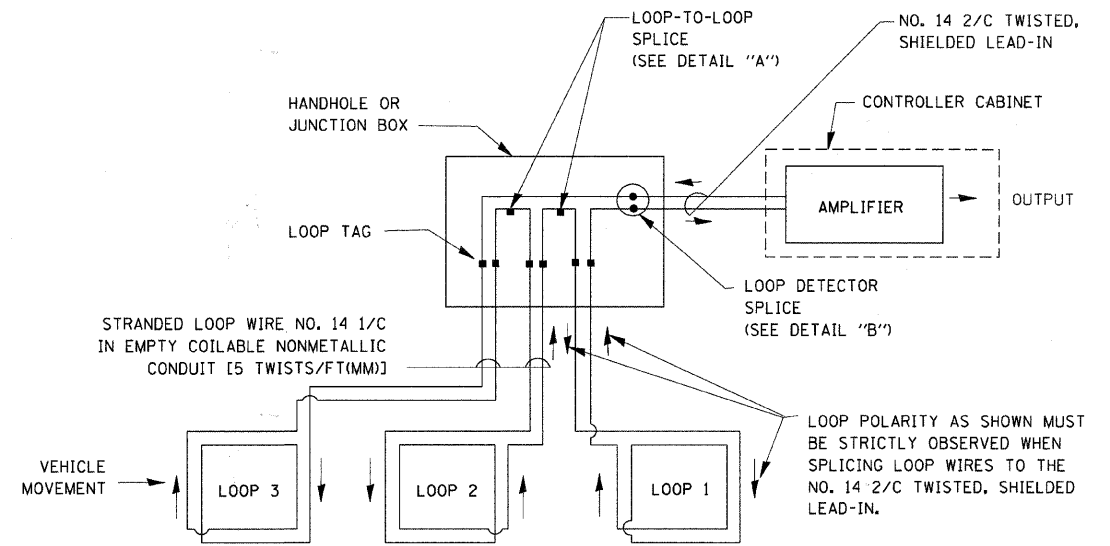
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

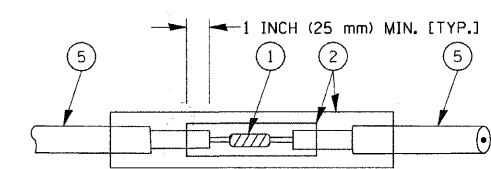


- A. LANE #1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

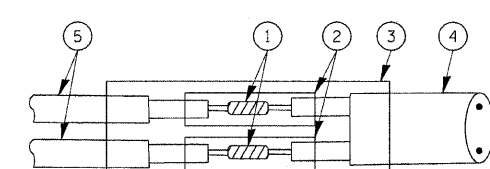


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

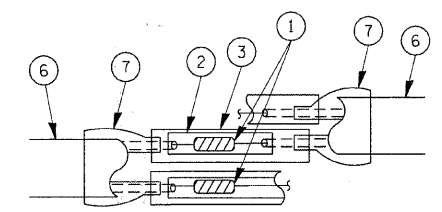


**DETAIL "A"
LOOP-TO-LOOP SPLICE**

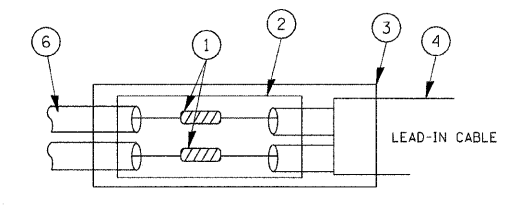


**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

TYPE I LOOP



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

LOOP DETECTOR SPLICE

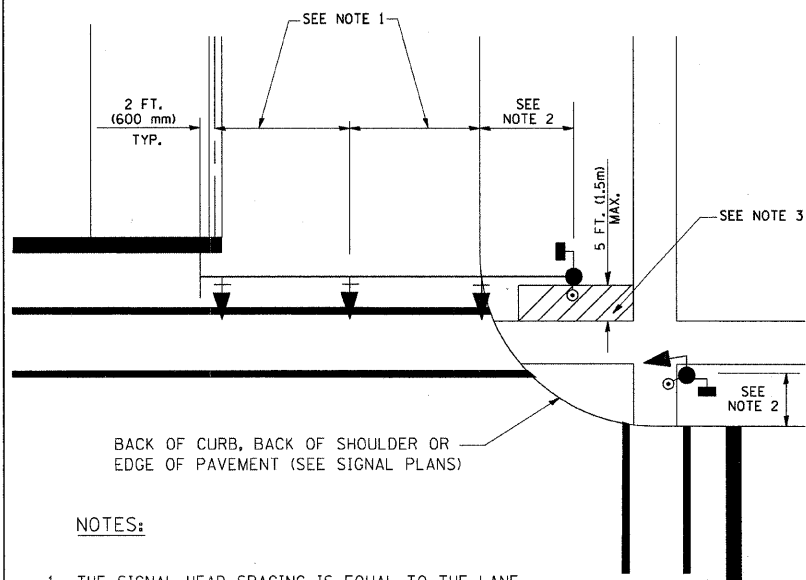
- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

*F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
 F.A.P. ROUTE 3877 EAST MAIN STREET
 PROJECT NO.: M-ARA-9003(313)
 JOB NO.: C-91-619-09

FILE NAME =	USER NAME = beuerd	DESIGNED - DAD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw_work\PW1007\BAUERDL\0106315\ts05.dgn		DRAWN - BCK	REVISED -			* 08-00054-00-CH	MCHENRY	56	27	
PLOT SCALE = 50:0000 / / IN.		CHECKED - DAD	REVISED -			TS-05		CONTRACT NO. 63404		
PLOT DATE = 11/4/2009		DATE - 10-28-09	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

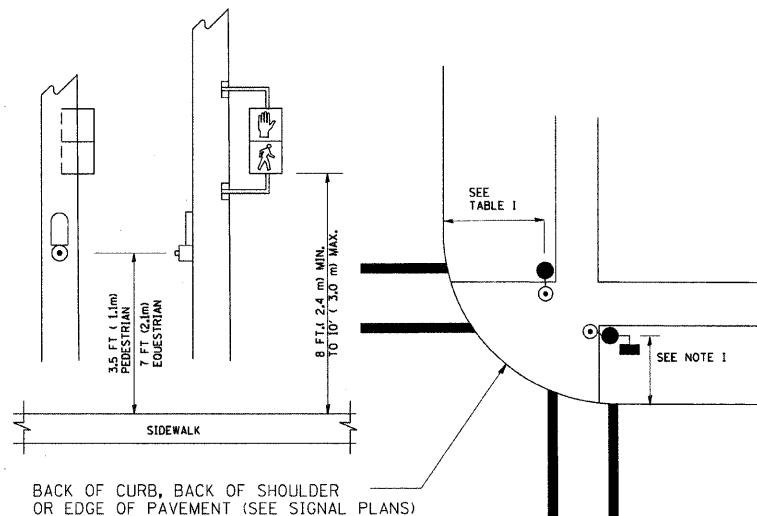
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

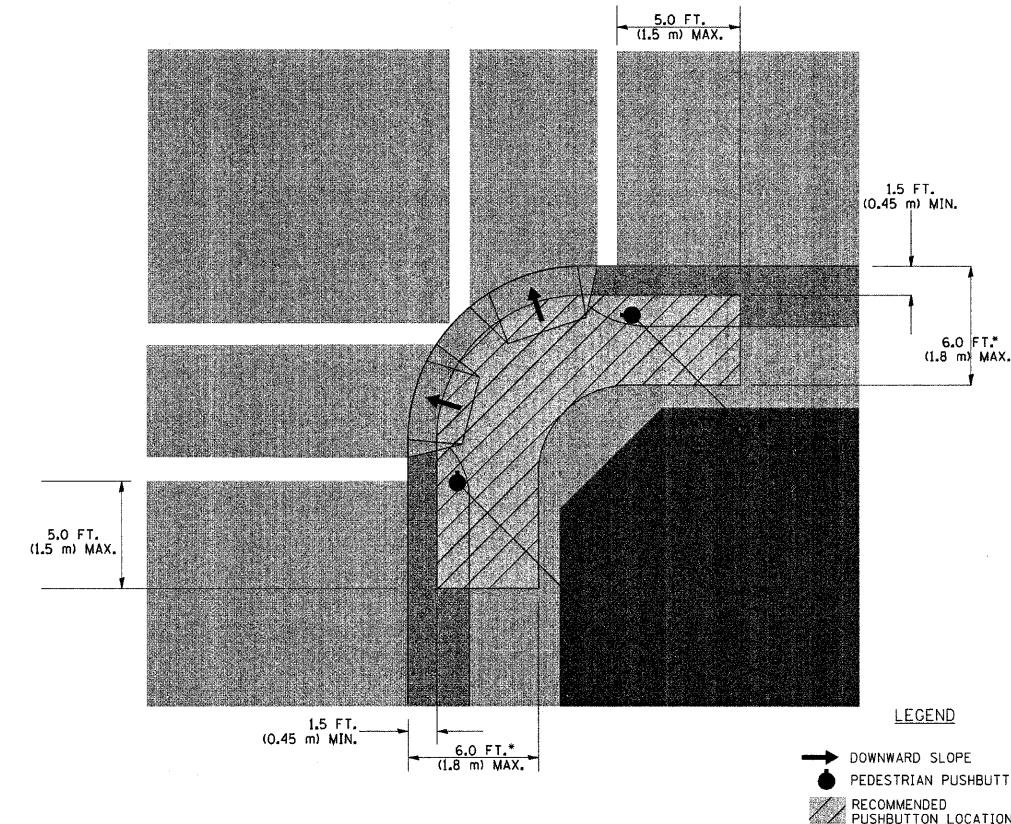
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

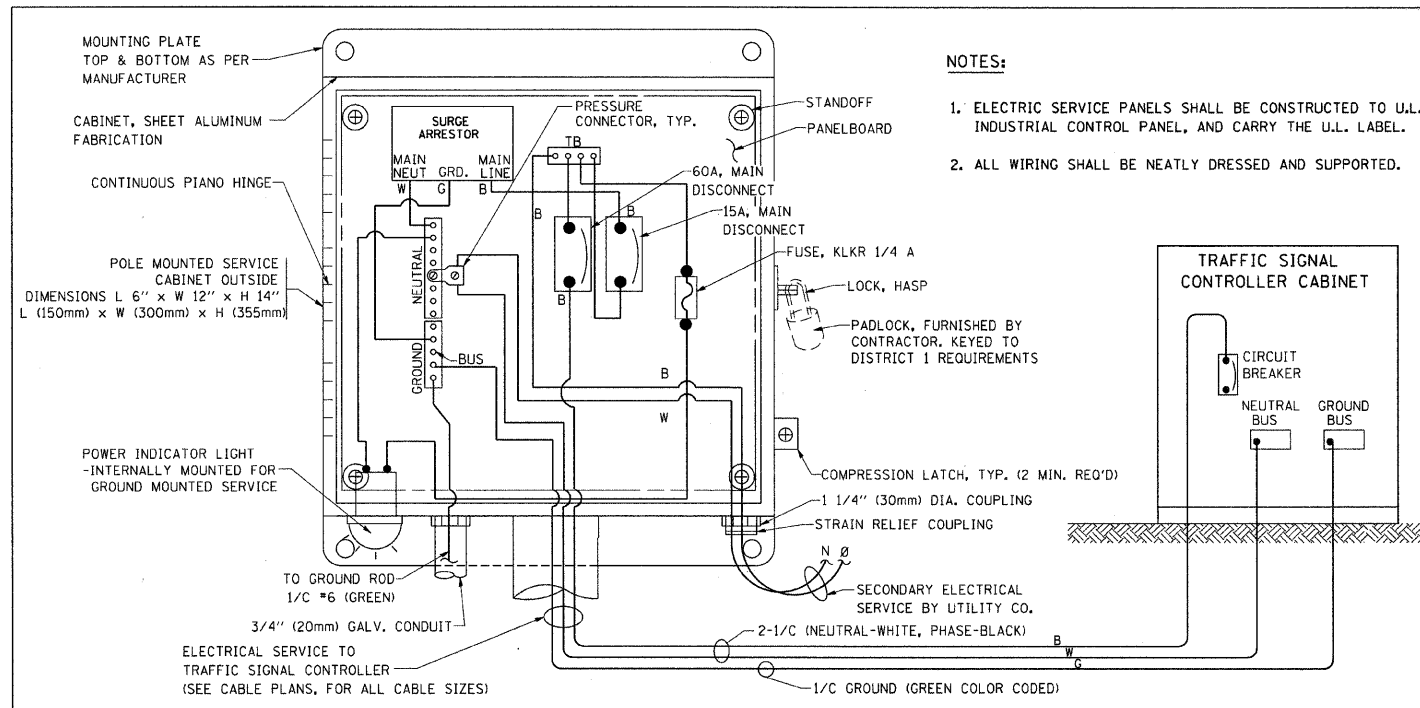
TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

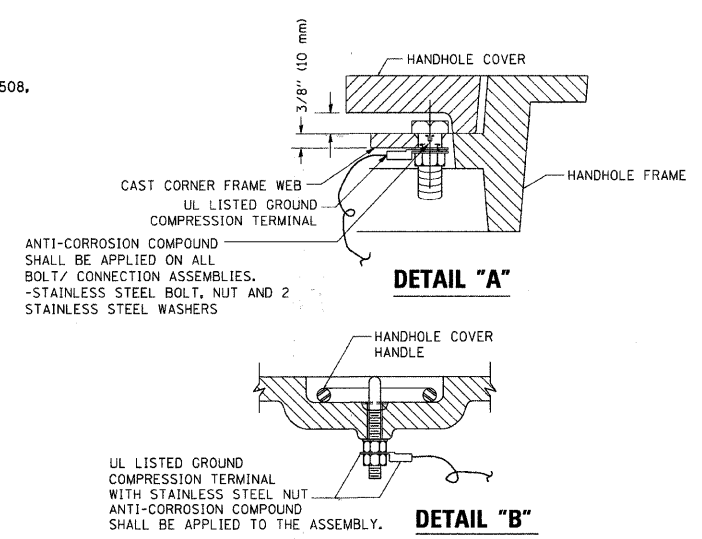
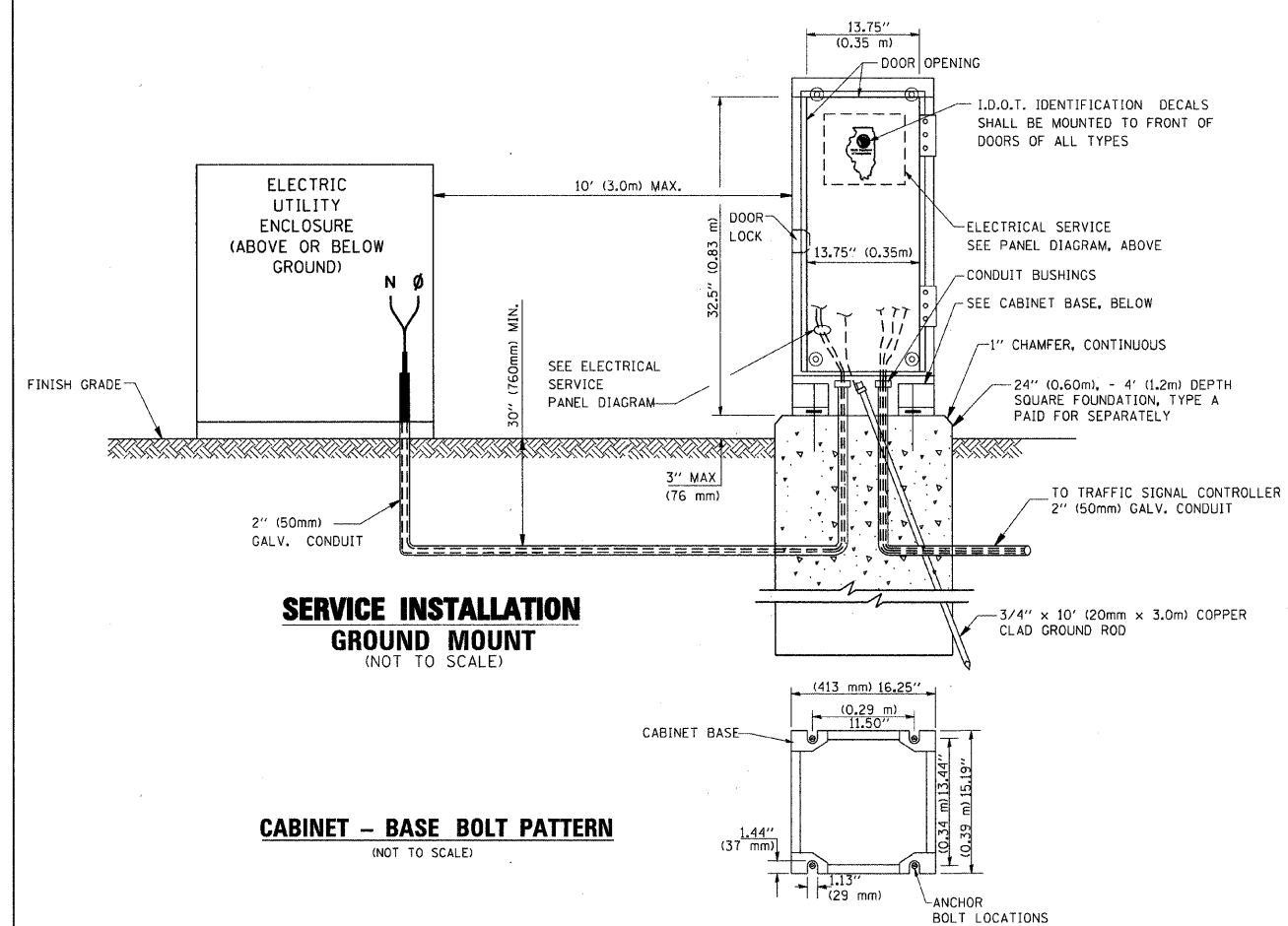
NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

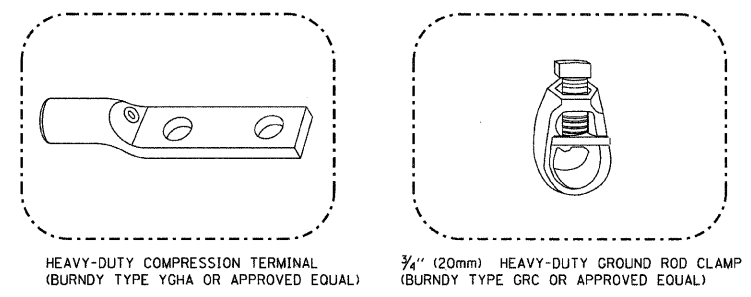
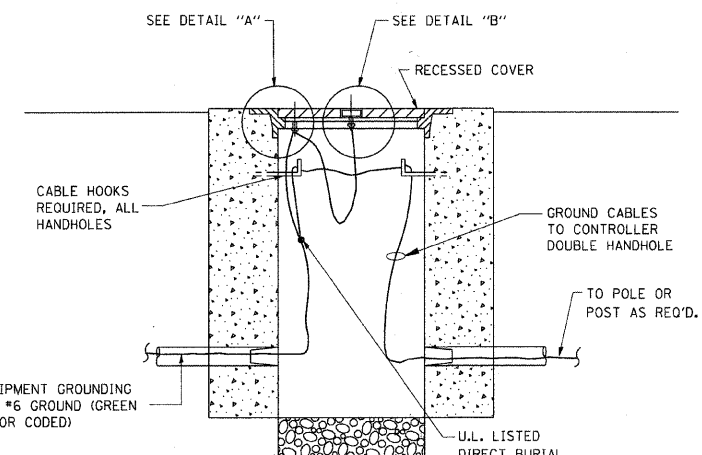
* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
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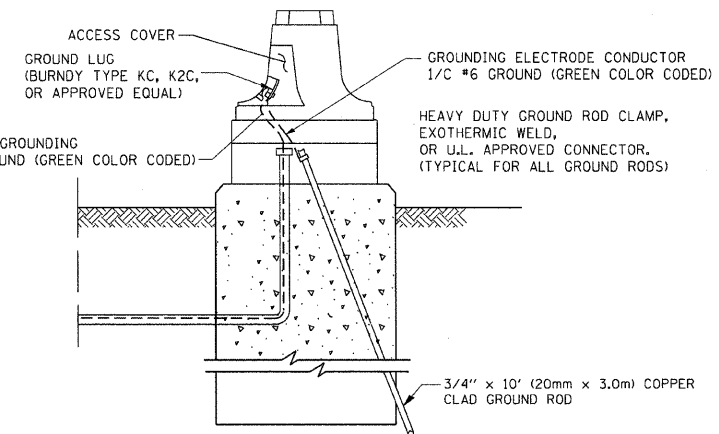
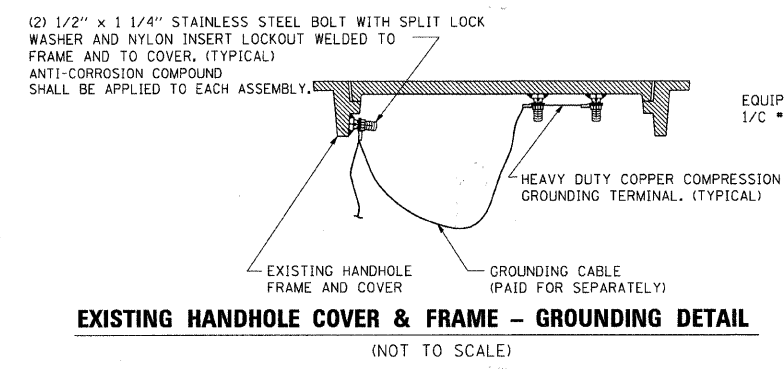
ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



- NOTES:**
- GROUNDING SYSTEM**
- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD, ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
 - THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
 - ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
 - THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

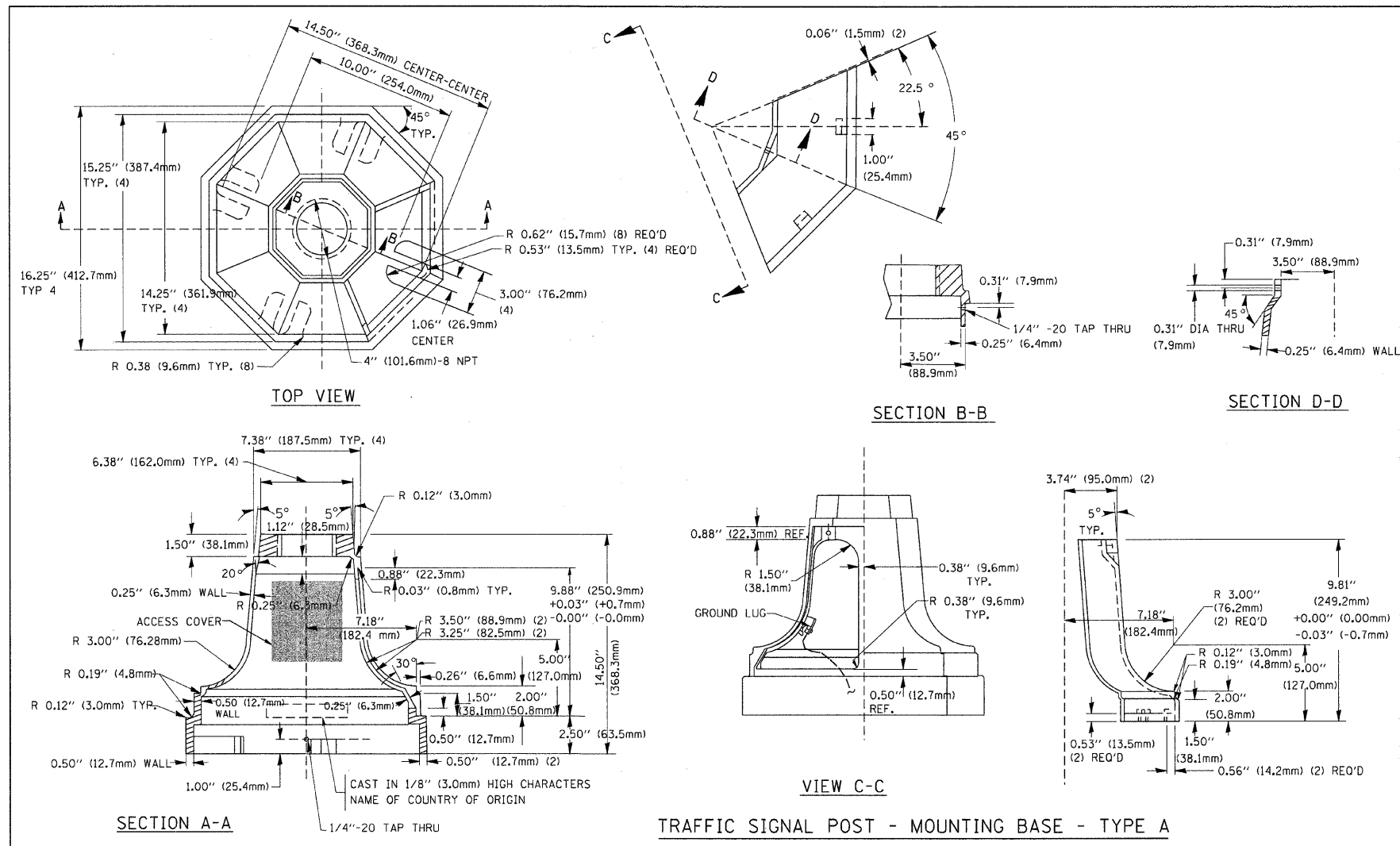


- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES. 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES. 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

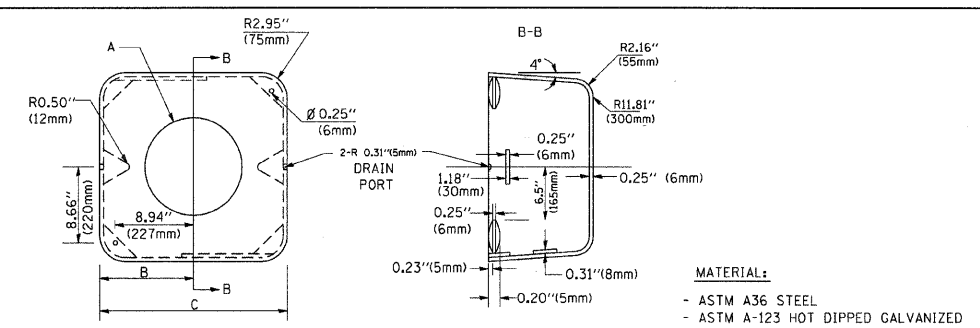


* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
 F.A.U. ROUTE 3877 EAST MAIN STREET
 PROJECT No.: M-ARA-9003(313)
 JOB No.: C-91-619-09

FILE NAME =	USER NAME = bauerd	DESIGNED - DAD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\pwidot\BAUEROL\d0108315\ts09.dgn		DRAWN - BCK	REVISED -		SCALE: NONE	SHEET NO. 3	OF 6 SHEETS	STA.	TO STA.	08-00054-00-CH	McHENRY	58	29
		CHECKED - DAD	REVISED -							TS-05			
		DATE - 10-28-09	REVISED -										
												CONTRACT NO. 63404	
												FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



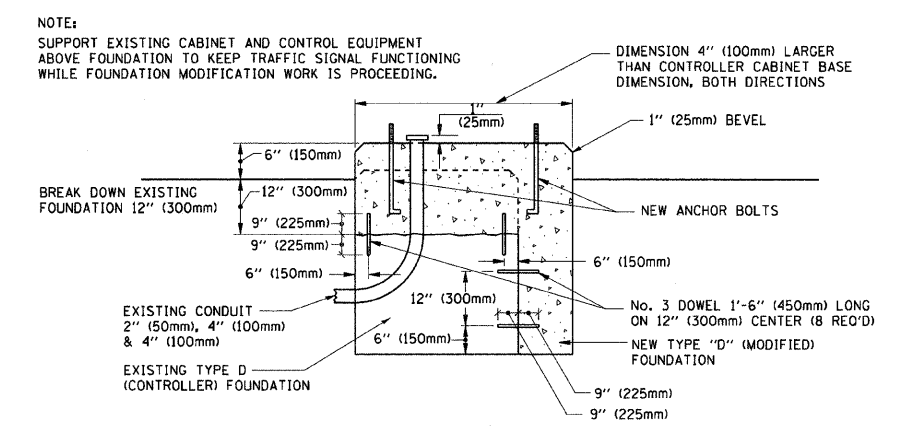
TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



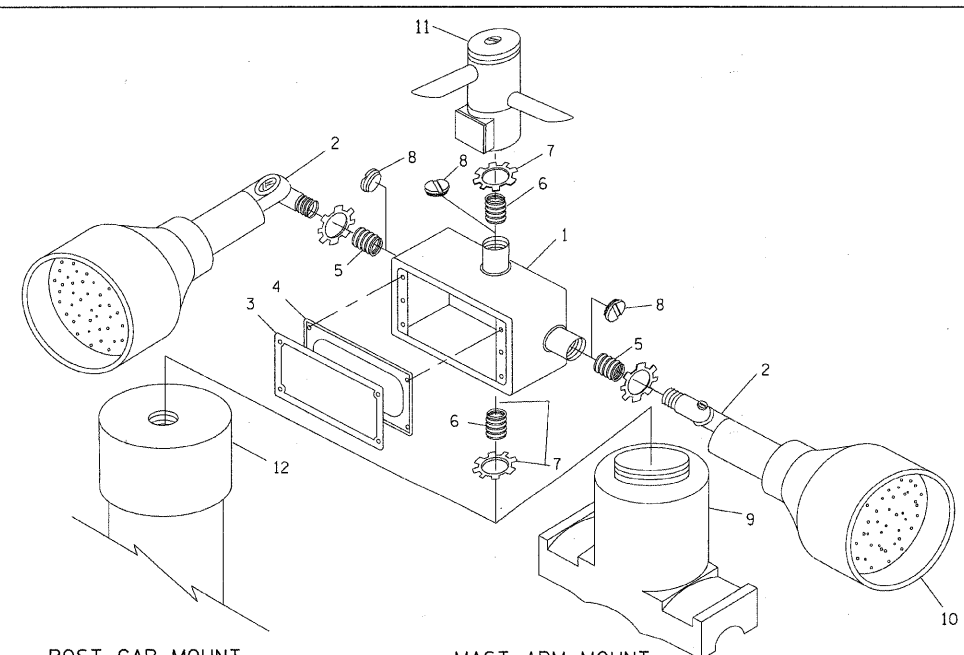
	A	B	C	HEIGHT	WEIGHT
	VARIES	9.5\"(241mm)	19\"(483mm)	7\"(178mm) - 12\"(300mm)	53 lbs (24kg)
	VARIES	10.75\"(273mm)	21.5\"(546mm)	7\"(178mm) - 12\"(300mm)	68 lbs (31 kg)
	VARIES	13.0\"(330mm)	26\"(660mm)	7\"(178mm) - 12\"(300mm)	81 lbs (37 kg)
	VARIES	18.5\"(470mm)	37\"(940mm)	7\"(178mm) - 12\"(300mm)	126 lbs (57 kg)

SHROUD

- NOTES:
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
 2. THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
 3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.



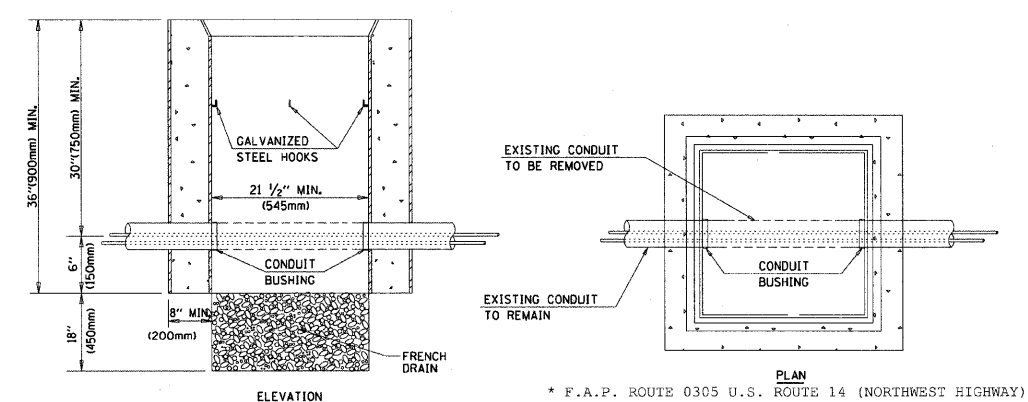
MODIFY EXISTING TYPE "D" FOUNDATION



POST CAP MOUNT
MAST ARM MOUNT
EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

ITEM NO.	IDENTIFICATION
1	OUTLET BOX - GALV. 21 CU.IN. (0.00344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\"(19 mm) CLOSE NIPPLE
7	3/4\"(19 mm) LOCKNUT
8	3/4\"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

- NOTES:
1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
 2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
 3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



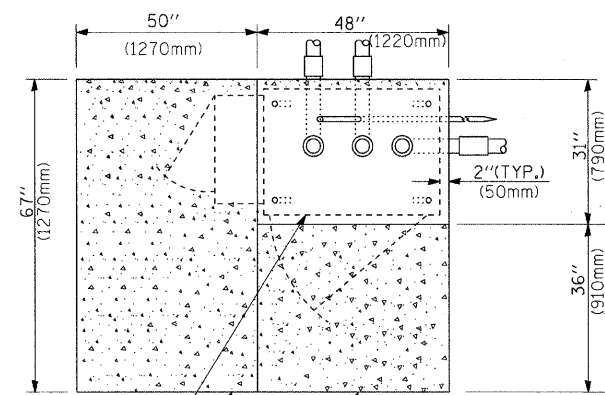
- NOTES:
1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
 2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

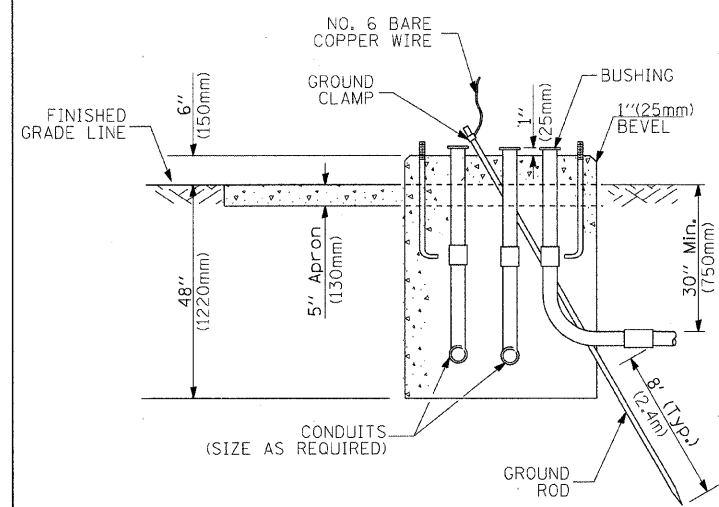
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PLOT DATE = 11/4/2009		DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

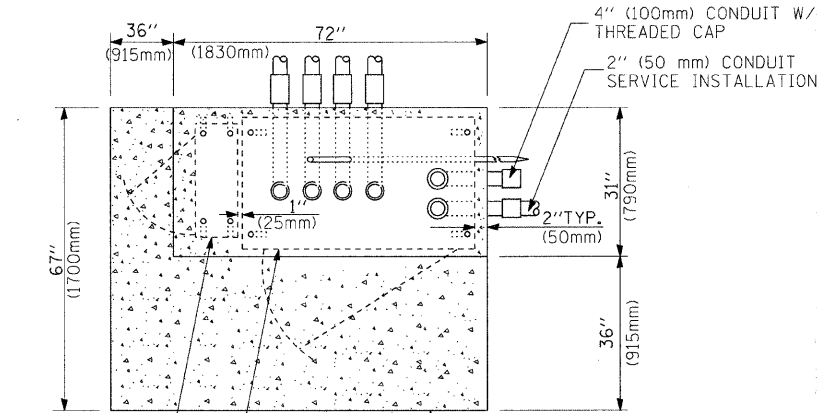
DISTRICT ONE		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STANDARD TRAFFIC SIGNAL DESIGN DETAILS		*	08-00054-00-CH	McHENRY	56	30
SCALE: NONE		TS-05		CONTRACT NO. 63404		
SHEET NO. 4 OF 6 SHEETS		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		JOB NO.: C-91-619-09		



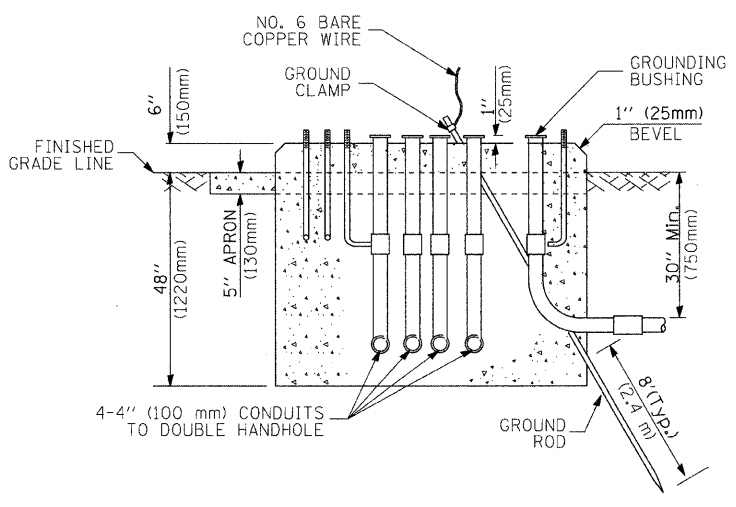
CONTROLLER CABINET BASE
EXISTING APRON
PROPOSED APRON
TOP VIEW



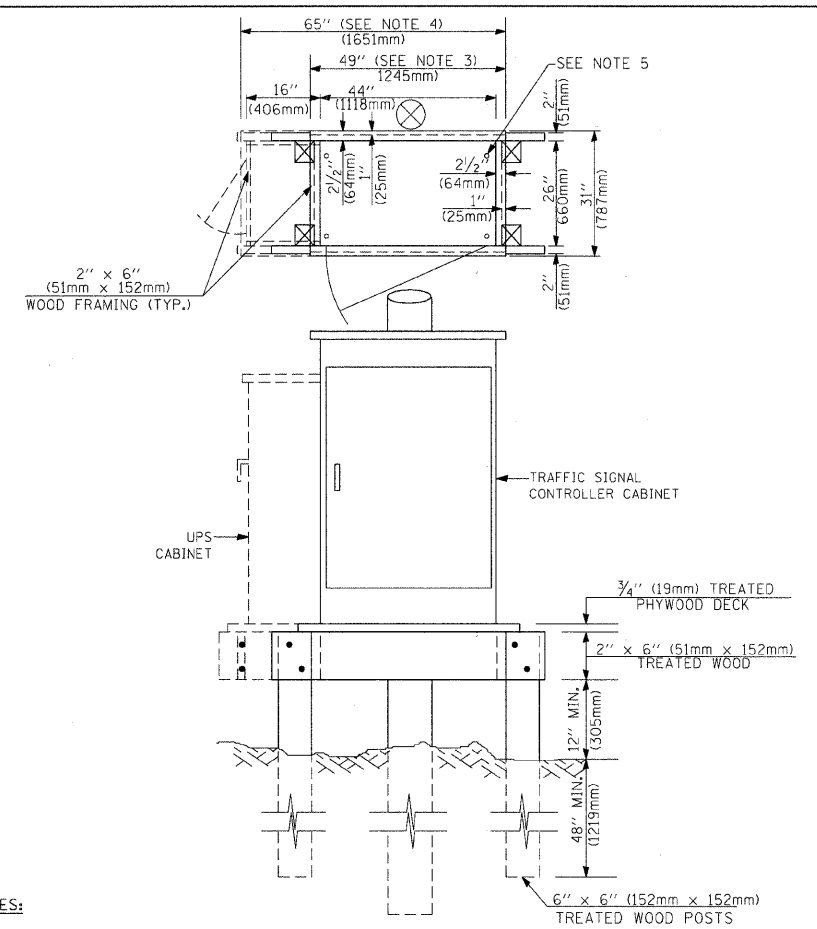
TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



UPS CABINET BASE
CONTROLLER CABINET BASE
APRON
TOP VIEW



TYPE C
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



- NOTES:**
- BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 - BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 - PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
 - PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
 - DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
 - FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and less than 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

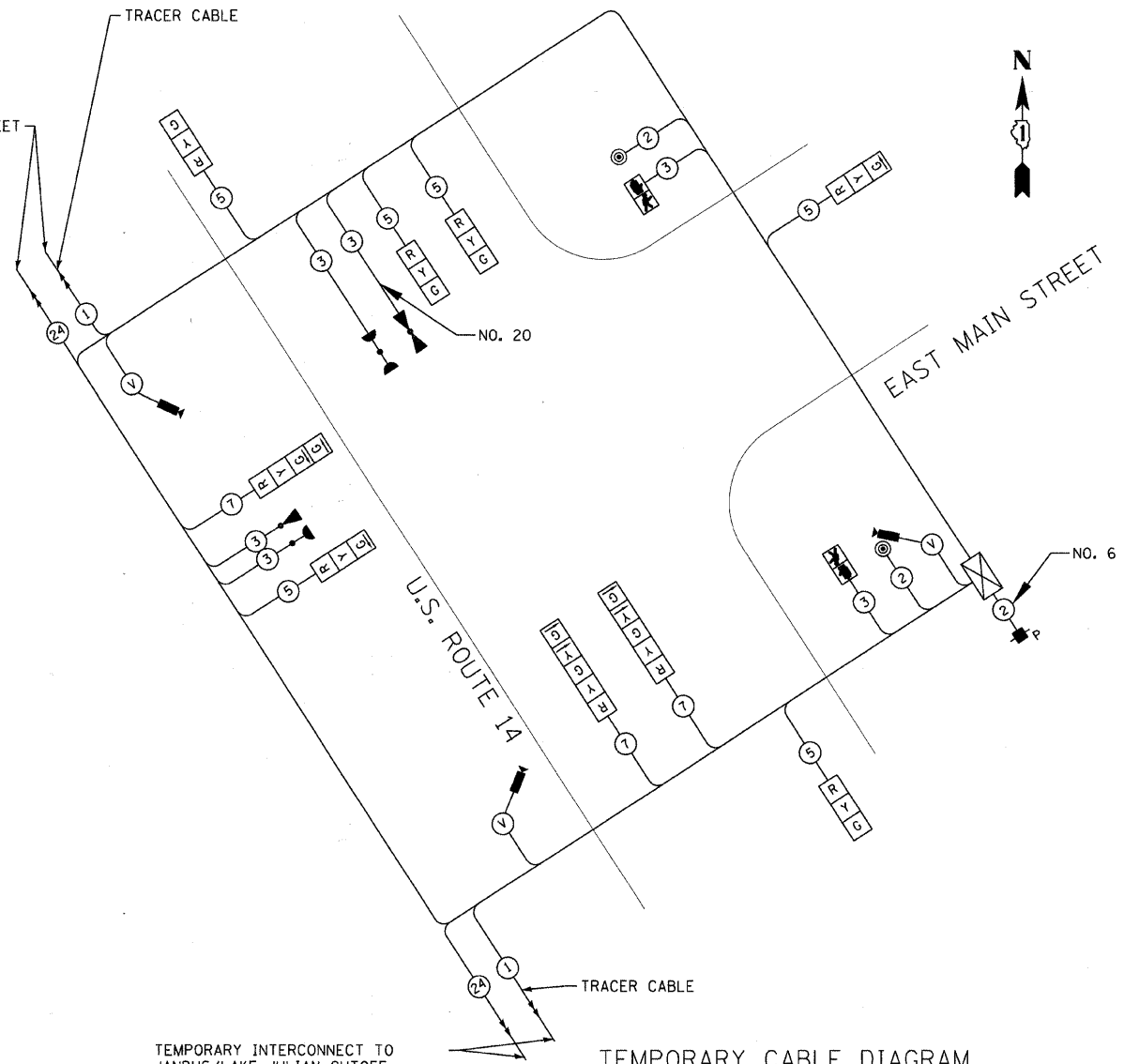
- NOTES:**
- These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Q_u) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
 - Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
 - Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
 - For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE			
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA			
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED			
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
UNINTERRUPTIBLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SMI2F			
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)				FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)			
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE			
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH			CT	CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)			CNC	STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM		S	S	ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM		I	IP	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED			
SIGNAL POST				REMOVE ITEM	R			SIGNAL POST AND FOUNDATION TO BE REMOVED			
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM	RL			INTERSECTION & SAMPLING (SYSTEM) DETECTOR			
GUY WIRE				ABANDON ITEM	A			SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				EXISTING INTERSECTION LOOP DETECTOR			
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				EXISTING PREFORMED INTERSECTION LOOP DETECTOR			
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED				PREFORMED SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID				RAILROAD SYMBOLS			
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER				EXISTING			
ILLUMINATED SIGN "NO LEFT TURN"				RADIO INTERCONNECT				RAILROAD CANTILEVER MAST ARM			
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO REPEATER				FLASHING SIGNAL			
DETECTOR LOOP, TYPE I				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED				CROSSING GATE			
PREFORMED DETECTOR LOOP				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)				CROSSBUCK			
MICROWAVE VEHICLE SENSOR											
VIDEO DETECTION CAMERA											
VIDEO DETECTION ZONE											
PAN, TILT, ZOOM CAMERA											
WIRELESS DETECTOR SENSOR											
WIRELESS ACCESS POINT											

TEMPORARY INTERCONNECT TO FIRST STREET
INSTALL CABLE FROM TEMPORARY
CONTROLLER TO EXISTING HANDHOLE AT
SOUTHEAST CORNER OF INTERSECTION



TEMPORARY INTERCONNECT TO
JANDUS/LAKE JULIAN CUTOFF
INSTALL CABLE FROM TEMPORARY
CONTROLLER TO EXISTING HANDHOLE
SOUTH OF INTERSECTION

TEMPORARY CABLE DIAGRAM
(NOT TO SCALE)

THE SIGNAL CONTROL EQUIPMENT
FOR THIS PROJECT SHALL BE "ECONOLITE"
TO MATCH THE EXISTING ADJACENT SYSTEM.

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	%OPERATION	
SIGNAL (RED)	9	17		0.50	76.5
(YELLOW)	9	25		0.25	56.25
(GREEN)	6	15		0.25	22.5
ARROW	8	12		0.10	9.6
PED. SIGNAL	2	25		1.00	50
CONTROLLER	1	100		1.00	100
ILLUM. SIGN	--	25		0.05	--
VIDEO SYSTEM	1	150		1.00	150
FLASHER				0.50	--
ENERGY COSTS TO:				TOTAL =	464.85

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196-1096
ENERGY SUPPLY CONTACT: MIKE LENOX
PHONE: 815-490-2869
COMPANY: COM ED



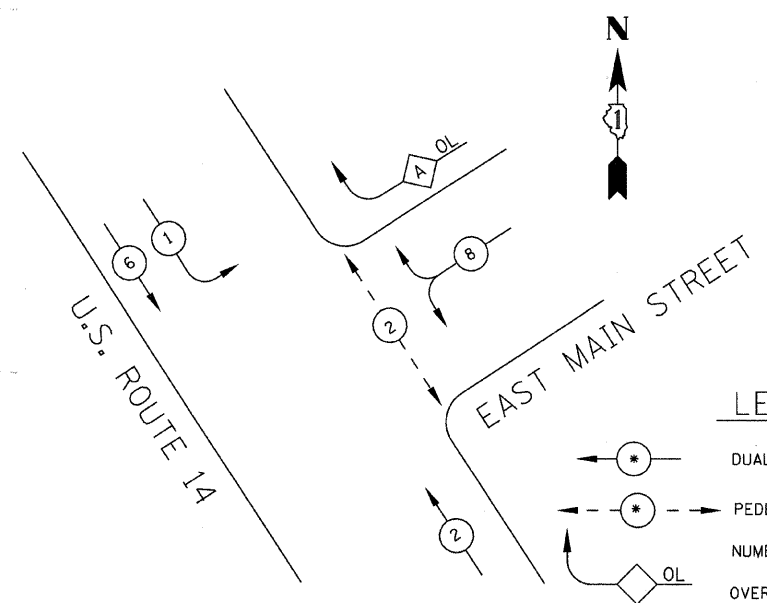
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DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-TS_TEMP_CABLE.shx

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

TEMPORARY TRAFFIC SIGNAL CABLE PLAN
AND PHASE DESIGNATION DIAGRAM

TEMPORARY CONTROLLER SEQUENCE

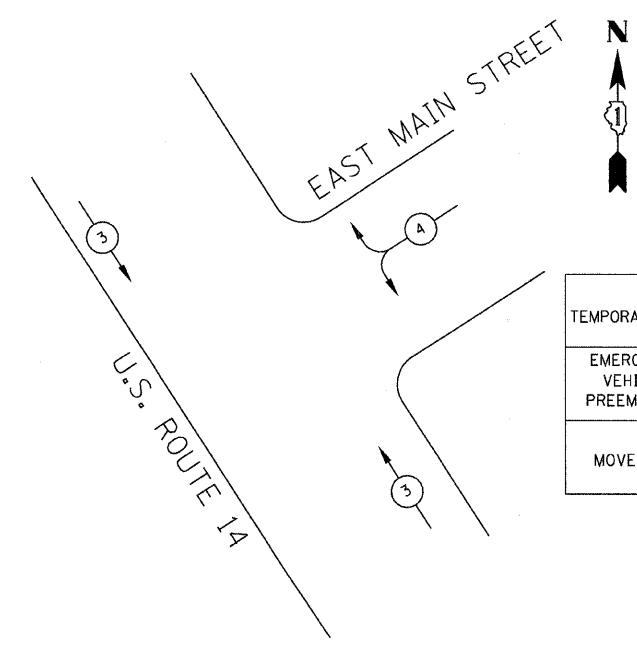
OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
A	= 8	+ 1



LEGEND

- ⊛ DUAL ENTRY PHASE
- ⊛ PEDESTRIAN PHASE
- OL NUMBER REFERS TO ASSOCIATED PHASE OVERLAP

TEMPORARY PHASE DESIGNATION DIAGRAM



PROPOSED TEMPORARY EMERGENCY VEHICLE PREEMPTION		
EMERGENCY VEHICLE PREEMPTION	3	4
MOVEMENT	↑ ↓	←

TEMPORARY EMERGENCY VEHICLE
PREEMPTION SEQUENCE

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
F.A.U. ROUTE 3877 EAST MAIN STREET

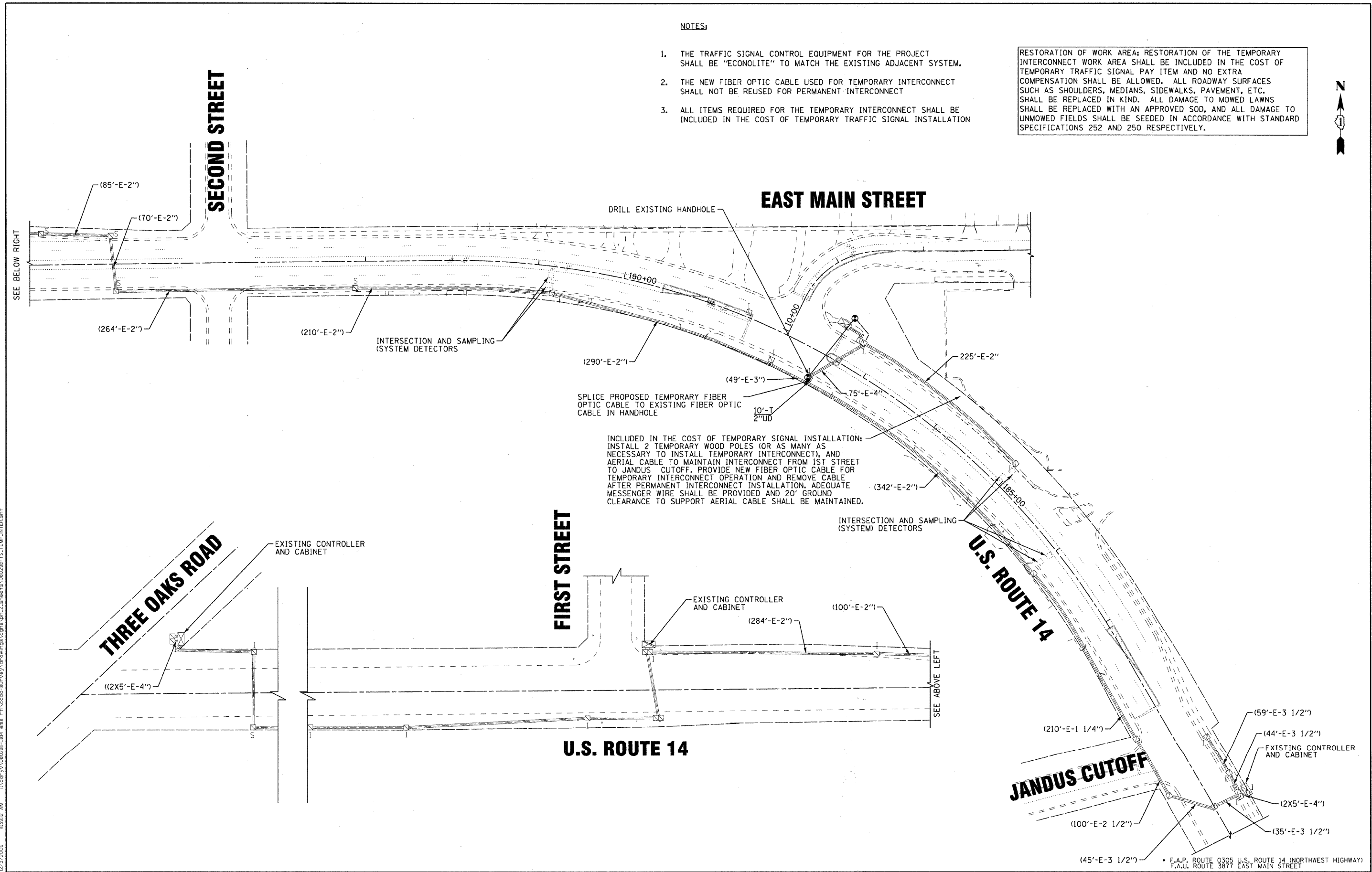
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	08-00054-00-CH	McHENRY	56	34
C-91-619-09			CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-90031313				

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NOTES:

1. THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THE PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.
2. THE NEW FIBER OPTIC CABLE USED FOR TEMPORARY INTERCONNECT SHALL NOT BE REUSED FOR PERMANENT INTERCONNECT
3. ALL ITEMS REQUIRED FOR THE TEMPORARY INTERCONNECT SHALL BE INCLUDED IN THE COST OF TEMPORARY TRAFFIC SIGNAL INSTALLATION

RESTORATION OF WORK AREA: RESTORATION OF THE TEMPORARY INTERCONNECT WORK AREA SHALL BE INCLUDED IN THE COST OF TEMPORARY TRAFFIC SIGNAL PAY ITEM AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



INCLUDED IN THE COST OF TEMPORARY SIGNAL INSTALLATION: INSTALL 2 TEMPORARY WOOD POLES (OR AS MANY AS NECESSARY TO INSTALL TEMPORARY INTERCONNECT), AND AERIAL CABLE TO MAINTAIN INTERCONNECT FROM 1ST STREET TO JANDUS CUTOFF. PROVIDE NEW FIBER OPTIC CABLE FOR TEMPORARY INTERCONNECT OPERATION AND REMOVE CABLE AFTER PERMANENT INTERCONNECT INSTALLATION. ADEQUATE MESSENGER WIRE SHALL BE PROVIDED AND 20' GROUND CLEARANCE TO SUPPORT AERIAL CABLE SHALL BE MAINTAINED.

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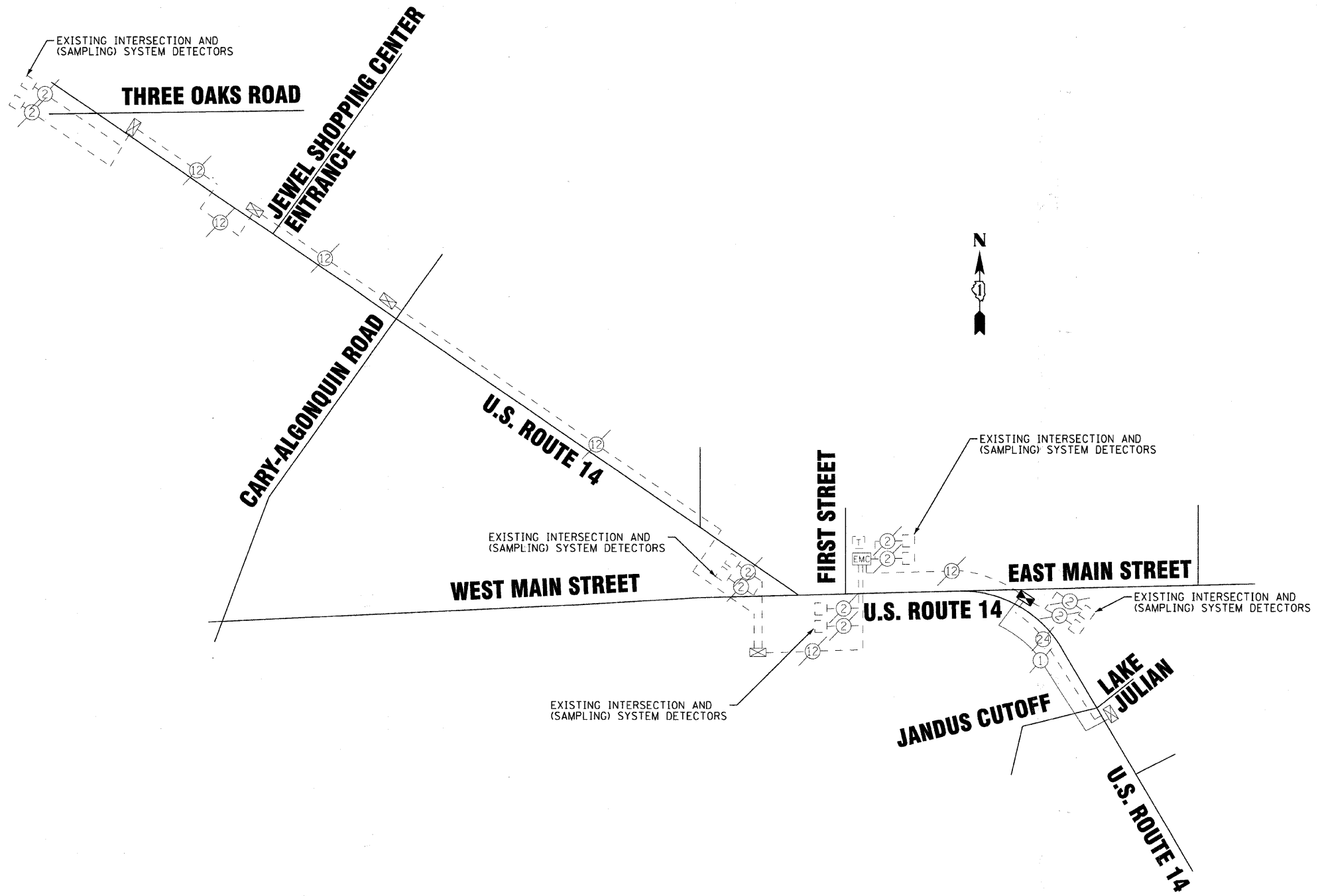


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DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-TS_TEMP_INTER.sht

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

TEMPORARY TRAFFIC SIGNAL INTERCONNECT PLAN
 SCALE: 1" = 50' STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
C-91-619-09	08-00054-00-CH	McHENRY	56	35
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(313)			CONTRACT NO. 63404	



- NOTES:**
1. THE NEW FIBER OPTIC CABLE USED FOR TEMPORARY INTERCONNECT SHALL NOT BE REUSED FOR PERMANENT INTERCONNECT.
 2. ALL ITEMS REQUIRED FOR THE TEMPORARY INTERCONNECT INSTALLATION AND REMOVAL SHALL BE INCLUDED IN THE COST OF THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.
 3. THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THE PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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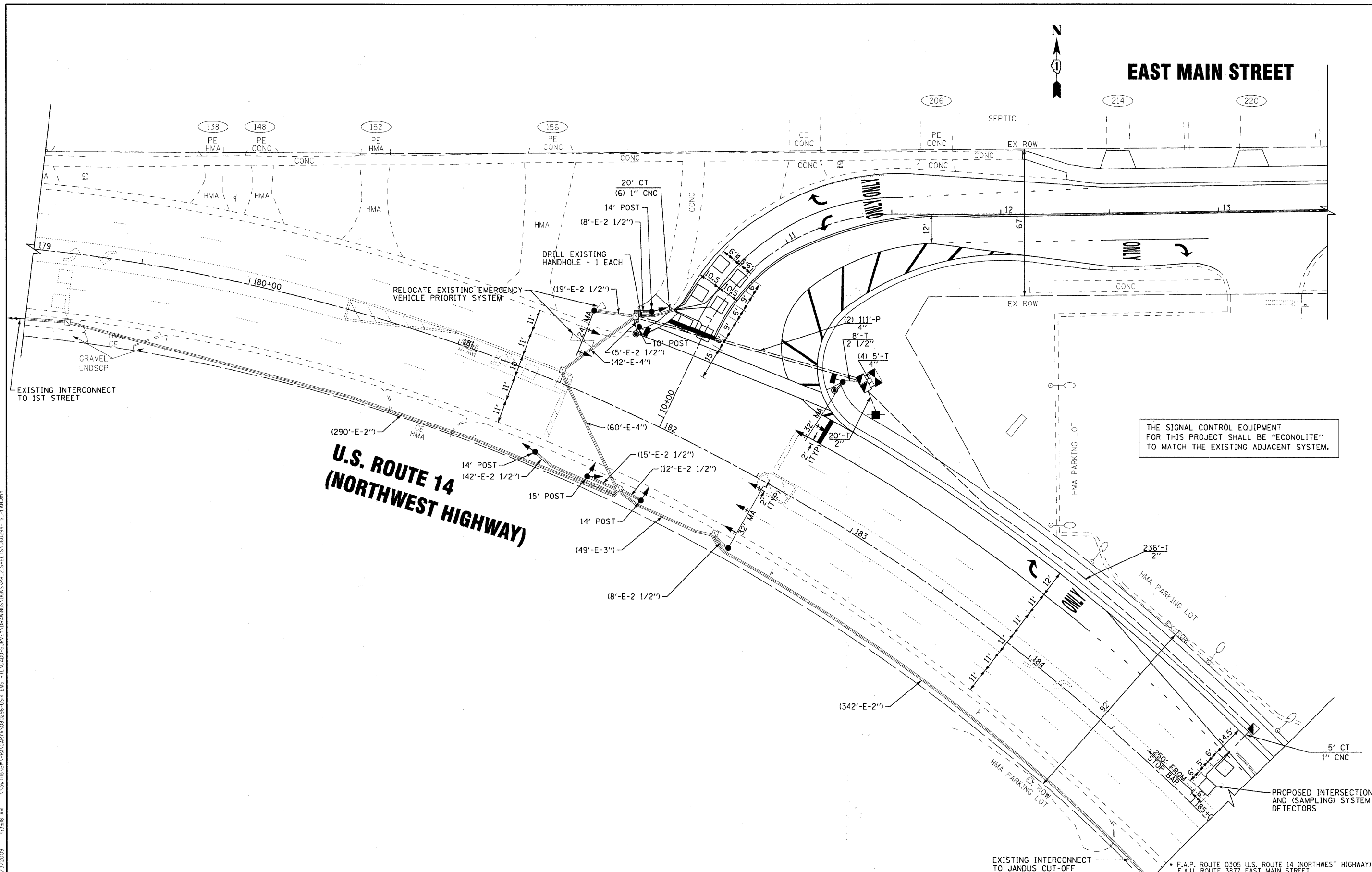
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DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-TS_TEMP_SCHEM.shx

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

TEMPORARY TRAFFIC SIGNAL INTERCONNECT SCHEMATIC

SCALE: 1" = 50' STA. TO STA.

F.A. RTE.		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
C-91-619-09		08-00054-00-CH	McHENRY	56	36
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-90031313				CONTRACT NO. 63404	



EAST MAIN STREET

**U.S. ROUTE 14
(NORTHWEST HIGHWAY)**

THE SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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**VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS**

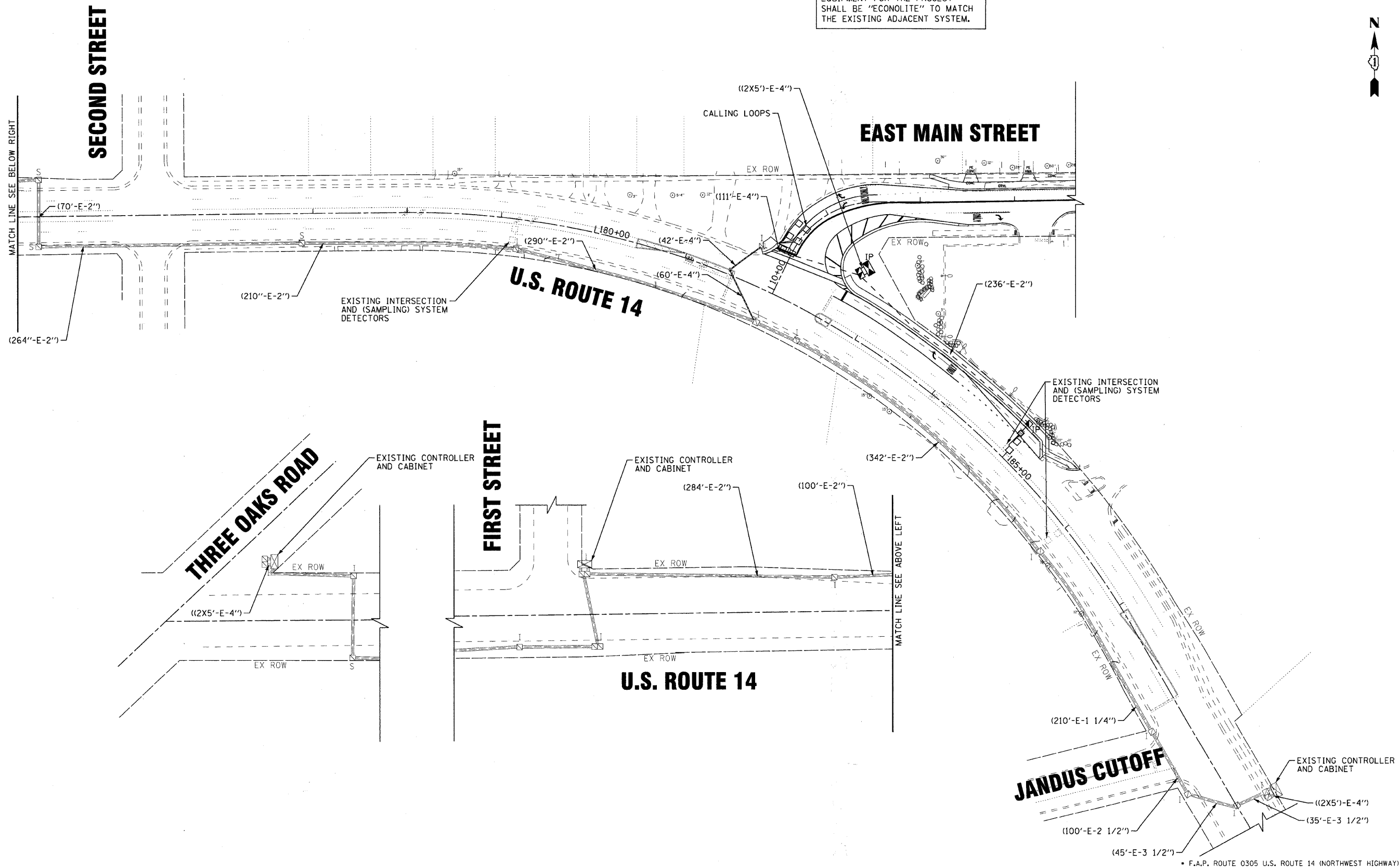
TRAFFIC SIGNAL MODERNIZATION PLAN

SCALE: 1" = 20'

STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS NO.	SHEET NO.
C-91-619-09	08-00054-00-CH	McHENRY	56	37
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT W-ARA-9003(313)			CONTRACT NO. 63404	

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THE PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.



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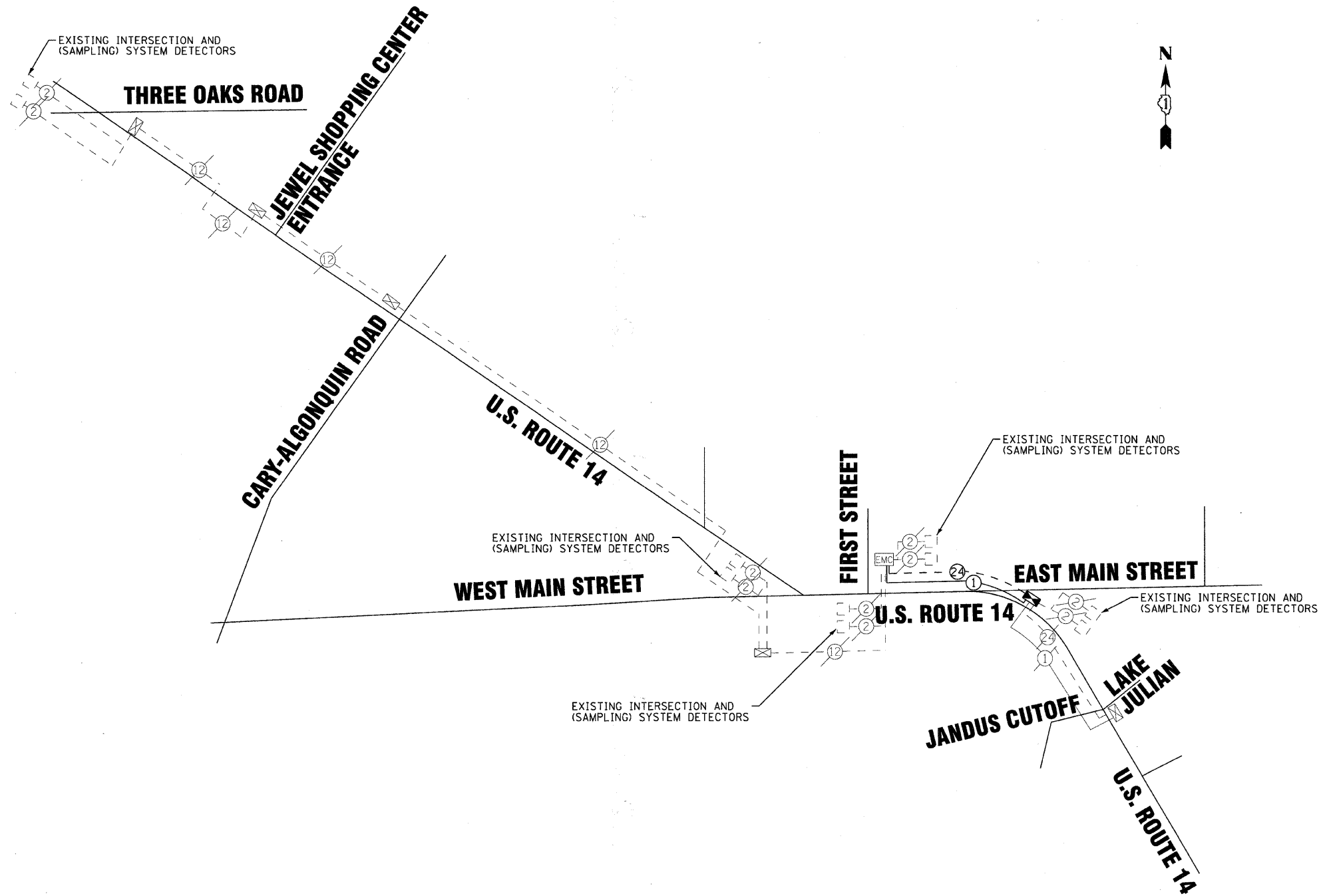


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DATE - 09/11/09	FILE - 080298-TS_INTER.shx

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

TRAFFIC SIGNAL INTERCONNECT PLAN	
SCALE: 1" = 50'	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	08-00054-00-CH	McHENRY	56	39
C-91-619-09			CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(313)				



NOTES:

1. REMOVE EXISTING INTERCONNECT CABLES IN CONDUIT FROM FIRST STREET CONTROLLER TO JANDUS CUTOFF CONTROLLER AND INSTALL NEW FIBER OPTIC CABLE IN CONDUIT, NO.62.5/125 MM 12F, SM12F AND ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1C.
2. THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

INTERCONNECT SCHEDULE OF QUANTITIES

PAY ITEM	UNIT	QUANTITY
TRANSCEIVER - FIBER OPTIC	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	2558
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	2618
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125 MULTIMODE 12 FIBERS, SINGLE MODE 12 FIBERS	FOOT	2618

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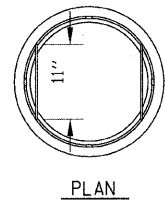
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VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

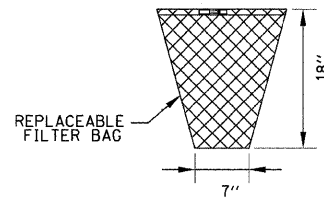
TRAFFIC SIGNAL INTERCONNECT SCHEMATIC AND
SCHEDULE OF QUANTITIES

SCALE: 1" = 50' STA. TO STA.

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.J. ROUTE 3877 EAST MAIN STREET			08-00054-00-CH	McHENRY	56	40
C-91-619-09				CONTRACT NO. 63404		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-9003(313)						



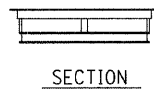
PLAN



REPLACEABLE FILTER BAG

GENERAL NOTES:

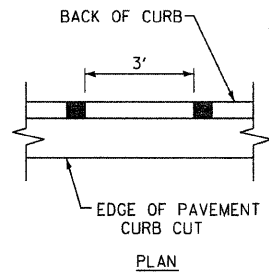
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 REPLACEABLE BAG: CONSTRUCTED FROM 4 OZ./SQ. YD. NON-WOVEN POLYPROPYLENE GEOTEXTILE REINFORCED WITH POLYESTER MESH. CONNECTED TO BASE RING WITH STAINLESS STEEL STRAP & LOCK.



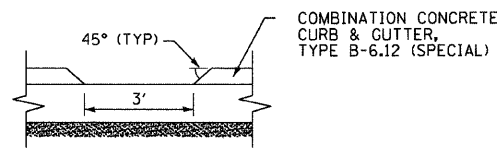
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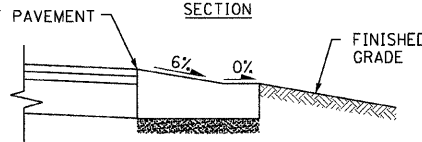
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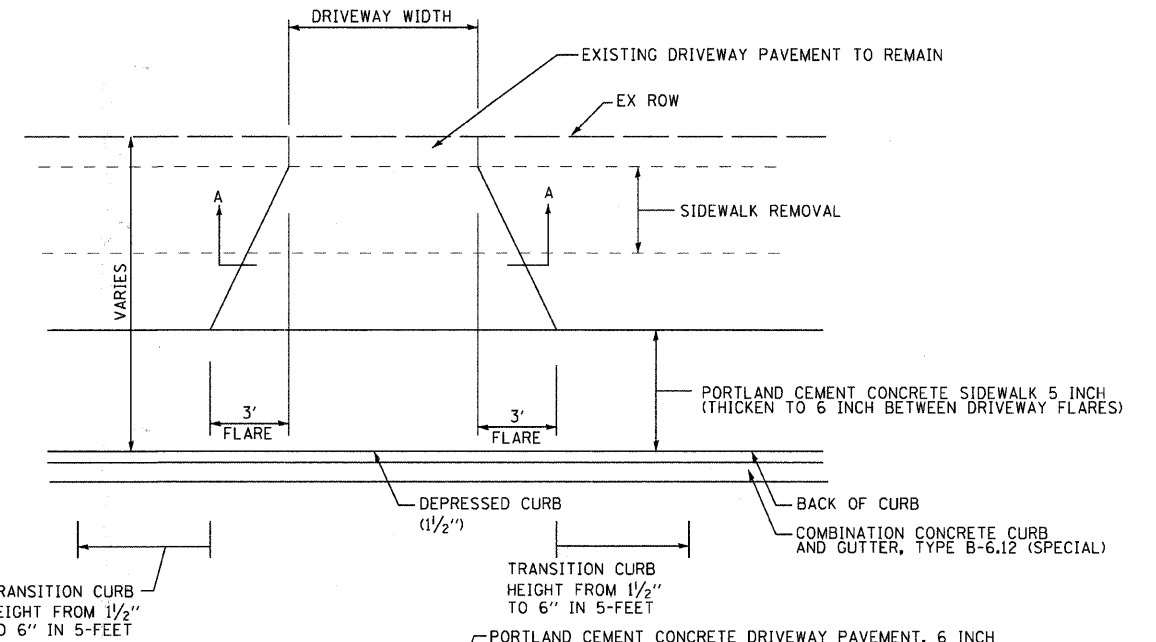


SECTION

NOTE: CURB CUTS INCLUDED IN COST OF COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL)

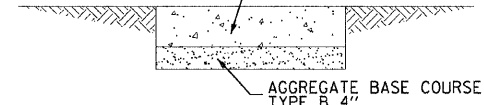
CURB CUT DETAIL

NOT TO SCALE

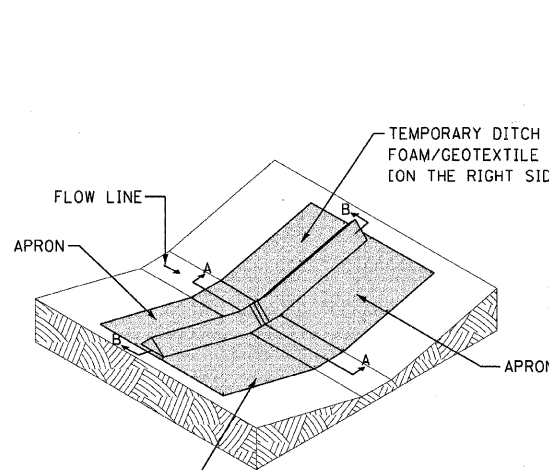


DRIVEWAY DETAIL

NO SCALE

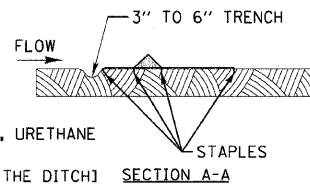


SECTION A-A

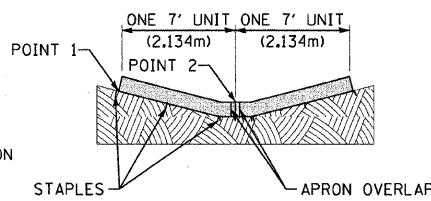


SILT DIKE UNIT
ISOMETRIC

TEMPORARY DITCH CHECK, URETHANE FOAM/GEOTEXTILE [ON THE LEFT SIDE OF THE DITCH]



SECTION A-A



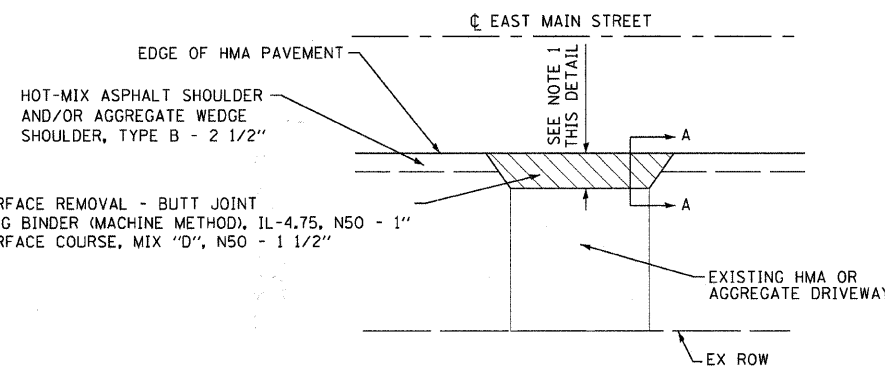
SECTION B-B

STAPLES SHALL BE PLACED WHERE THE UNITS OVERLAP AND IN THE CENTER OF THE 7' UNIT AS SHOWN ON THE DIAGRAM.

POINT 1 MUST BE HIGHER THAN POINT 2 TO INSURE THAT WATER FLOWS OVER THE DIKE AND NOT AROUND THE ENDS.

TEMPORARY DITCH CHECK

NO SCALE



HMA DRIVEWAY DETAIL

NO SCALE

NOTES: 1. 5' MIN WHEN MATCHING EXISTING HMA DRIVEWAY
 3' MIN WHEN MATCHING EXISTING AGGREGATE DRIVEWAY

NOTES:
 1. THE INSTALLATION SHOWN ABOVE WILL BE MEASURED FOR PAYMENT IN FEET PERPENDICULAR TO THE FLOW LINE.

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 263mcc



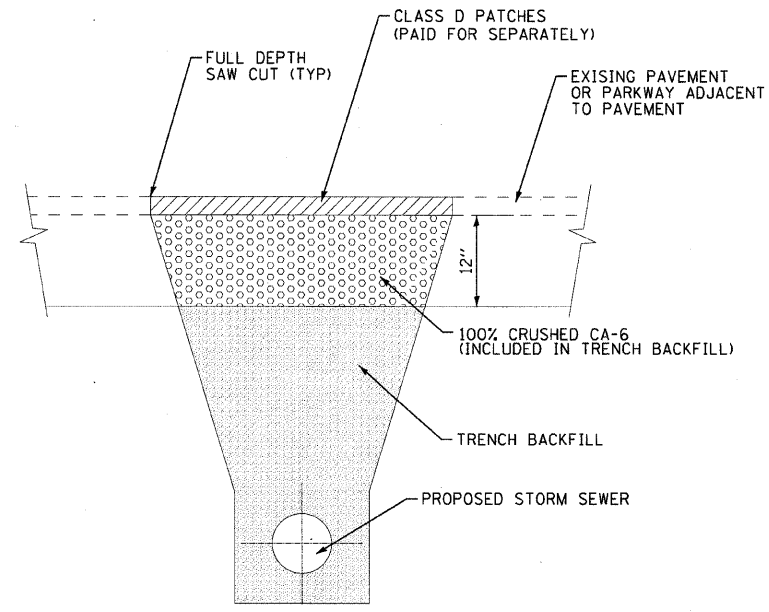
DESIGNED - JJF	REVISED - MCDPD REVIEW 11/3/09
DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-MISC-DETAILS.sht

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

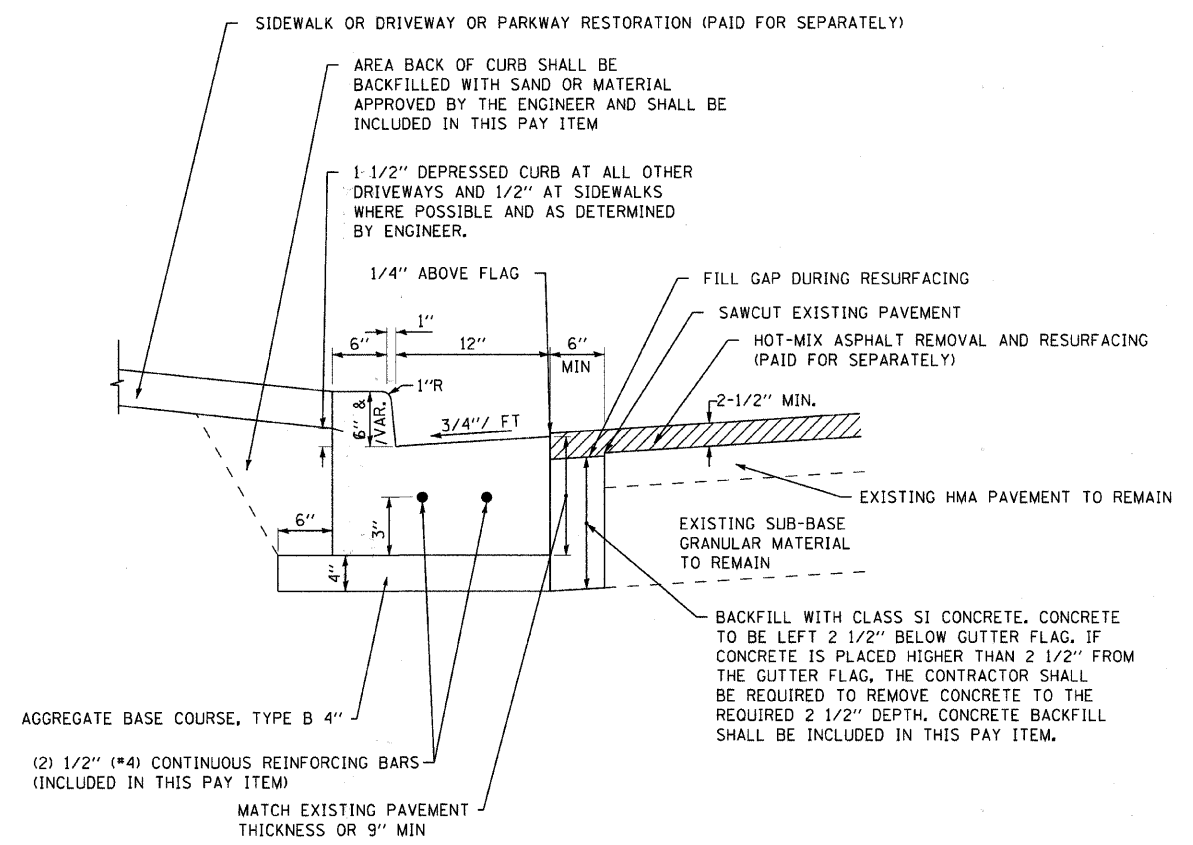
MISCELLANEOUS DETAILS

SCALE: STA. TO STA.

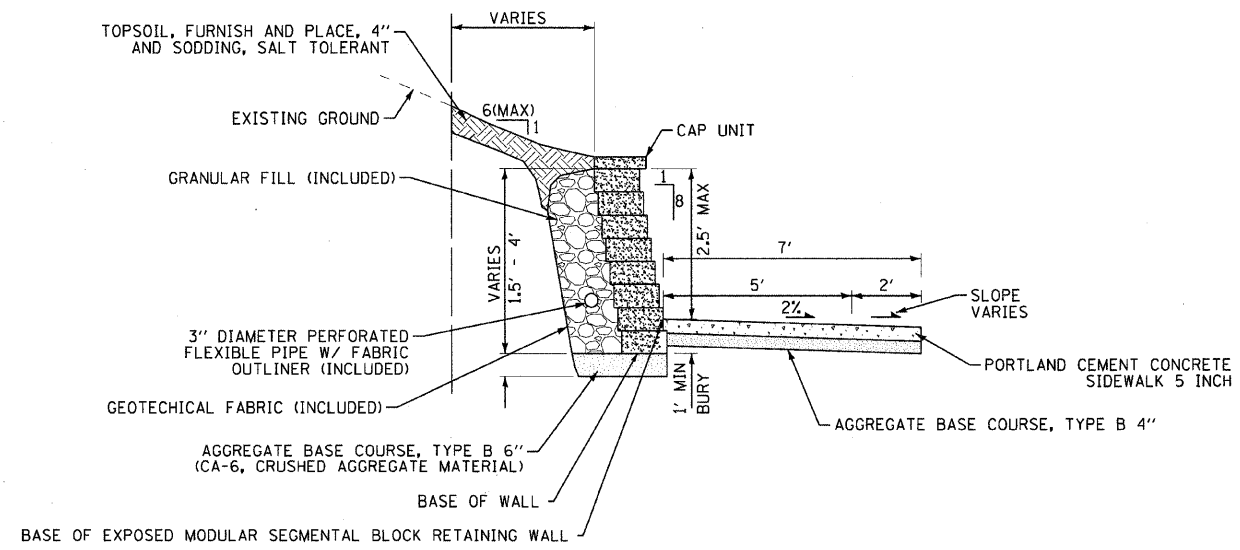
F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)		TOTAL SHEETS	
F.A.U. ROUTE 3877 EAST MAIN STREET		56	41
F.A. RTE.	SECTION	COUNTY	SHEET NO.
•	08-00054-00-CH	McHENRY	56
C-91-619-09		CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-90031313			



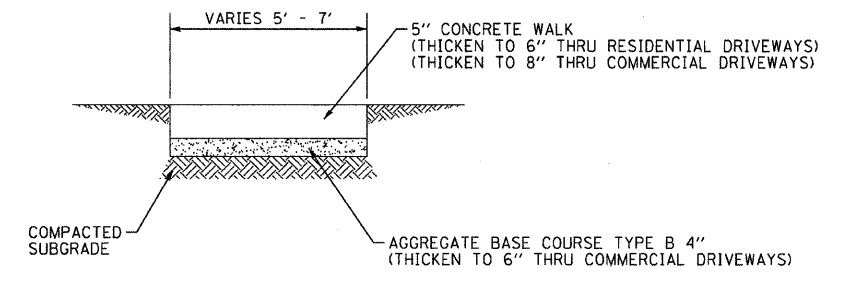
TRENCH BACKFILL
NO SCALE



COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL)
(NOT TO SCALE)



MODULAR SEGMENTAL BLOCK RETAINING WALL AND SIDEWALK DETAIL
NO SCALE



NOTE:
PROVIDE FIBER 3/4" EXPANSION JOINTS WHERE
NEW SIDEWALK MEETS EXISTING AND @ 50' O.C. MAX. AND
PROVIDE CONTROL JOINTS @ 5' O.C.

**PORTLAND CEMENT CONCRETE
SIDEWALK 5 INCH**
(NOT TO SCALE)

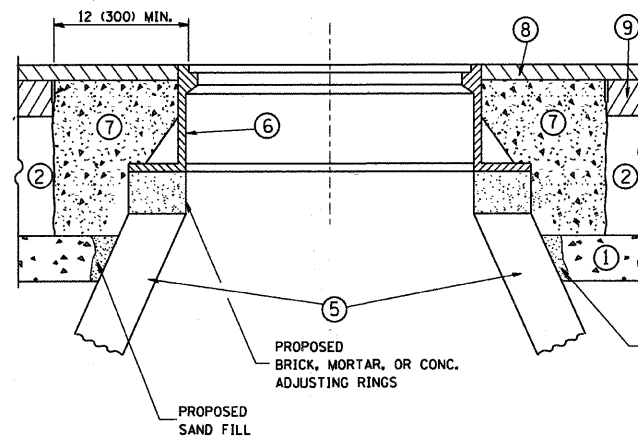
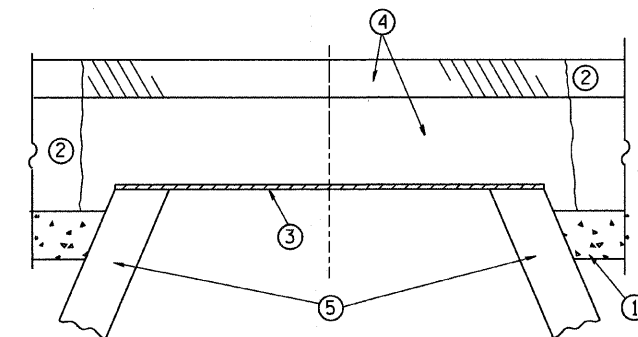
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 PROJECT: VILLAGE CARY, ILLINOIS
 SHEET: 08-00054-00-CH
 DATE: 09/11/09



DESIGNED - JJF	REVISED - MCDPD REVIEW 11/3/09
DRAWN - CJC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	FILE - 080298-MISC.DETAILS.sht

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

SCALE:		STA.		TO STA.	
F.A. RTE.		SECTION		COUNTY	TOTAL SHEETS NO.
C-91-619-09		08-00054-00-CH		McHENRY	56 42
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT M-ARA-9003(313)		CONTRACT NO. 63404	



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS S1 CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

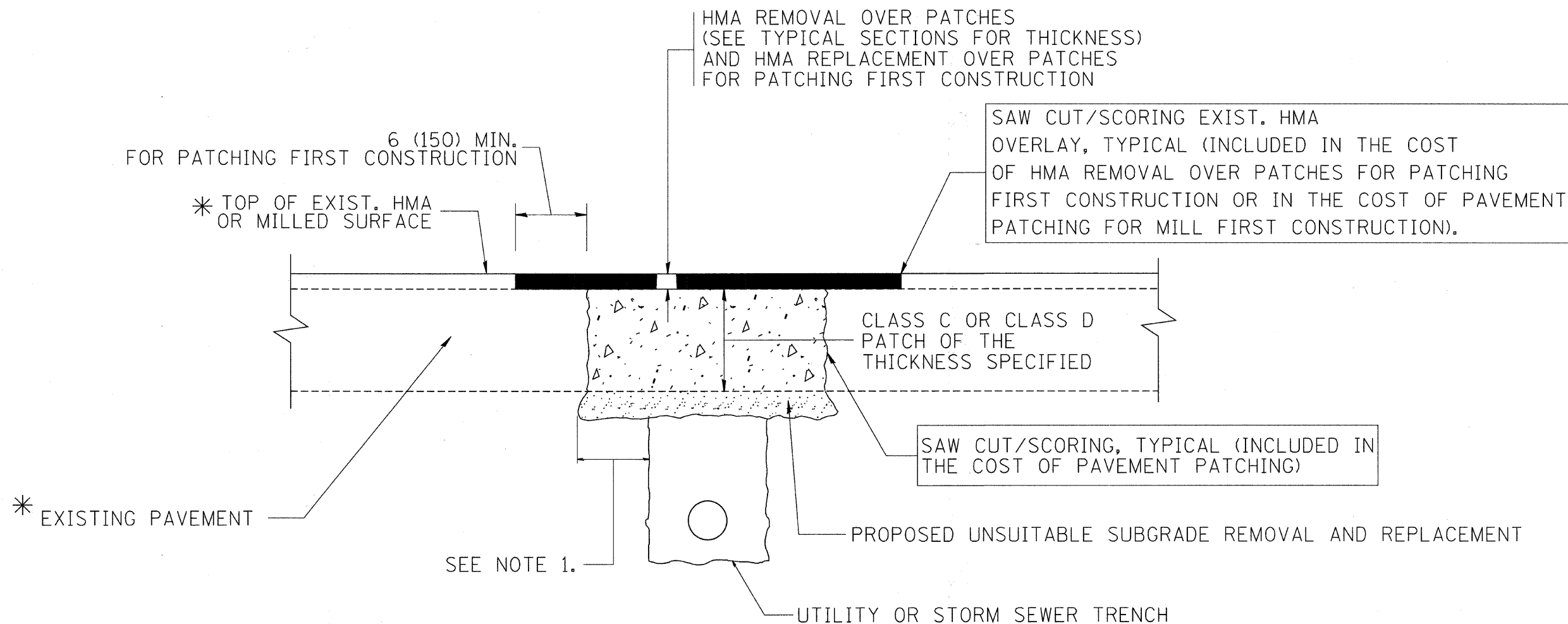
WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
 F.A.U. ROUTE 3877 EAST MAIN STREET
 PROJECT No: M-ARA-9003(313)
 JOB No: C-91-619-09

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\distatd\22x34\bd08.dgn	USER NAME = goglionobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING				F.A. RTE. *	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1	HEETS	STA.	TO STA.	08-00054-00-CH	MCHENRY	56	43
		CHECKED -	REVISED - R. WIEDEMAN 05-14-04						BD600-03 (BD-8)		CONTRACT NO. 63404		
		DATE - 10-25-94	REVISED - R. BORO 01-01-07						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

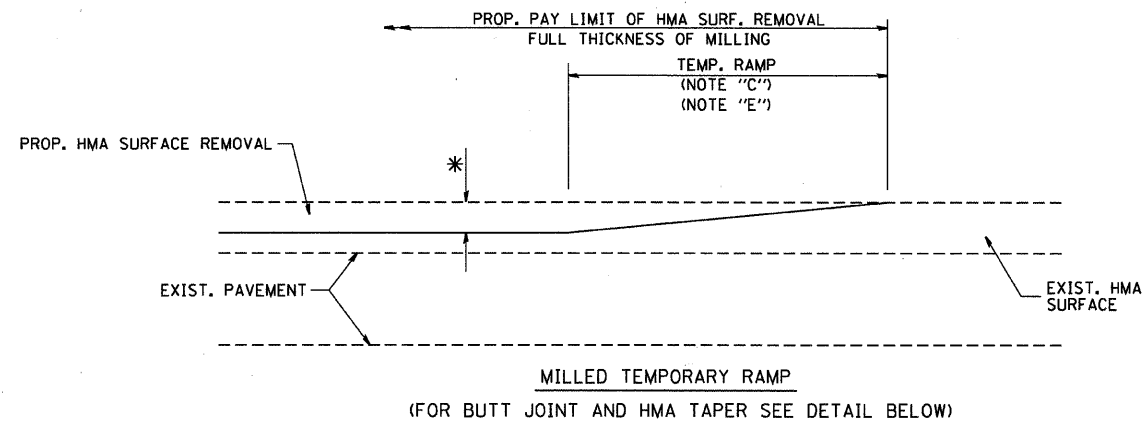
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

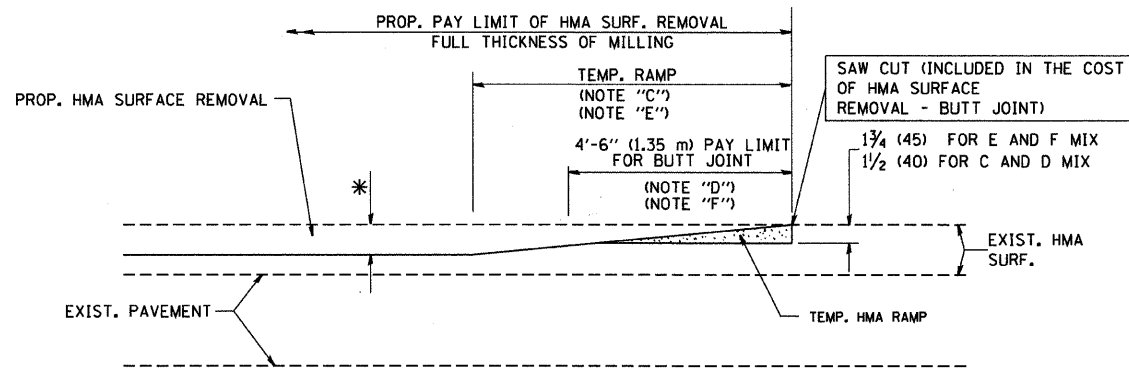
* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
 F.A.U. ROUTE 3877 EAST MAIN STREET
 PROJECT No: M-ARA-9003(313)
 JOB No: C-91-619-09
 ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bouardl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A. RTE. *	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	08-00054-00-CH	McHENRY	56	44
		CHECKED -	REVISED - R. BORO 09-04-07							BD400-04 (BD-22)			
		DATE - 10-25-94	REVISED - K. ENG 10-27-08							CONTRACT NO. 63404			
								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

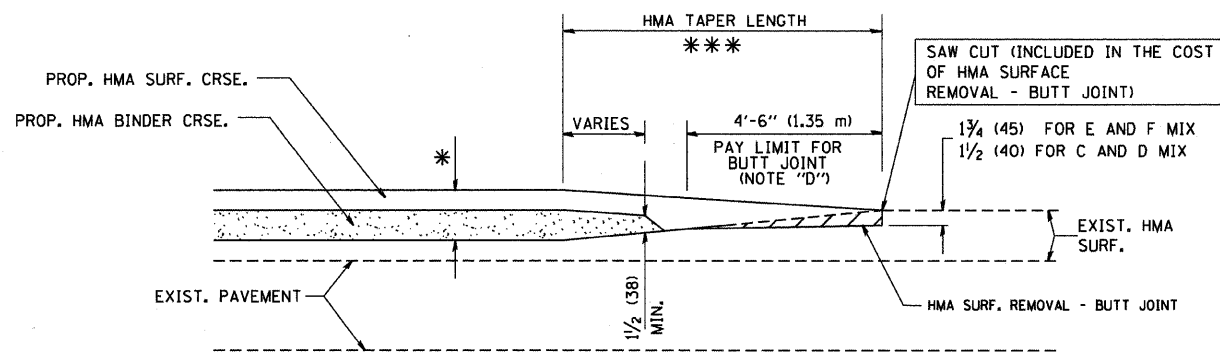
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

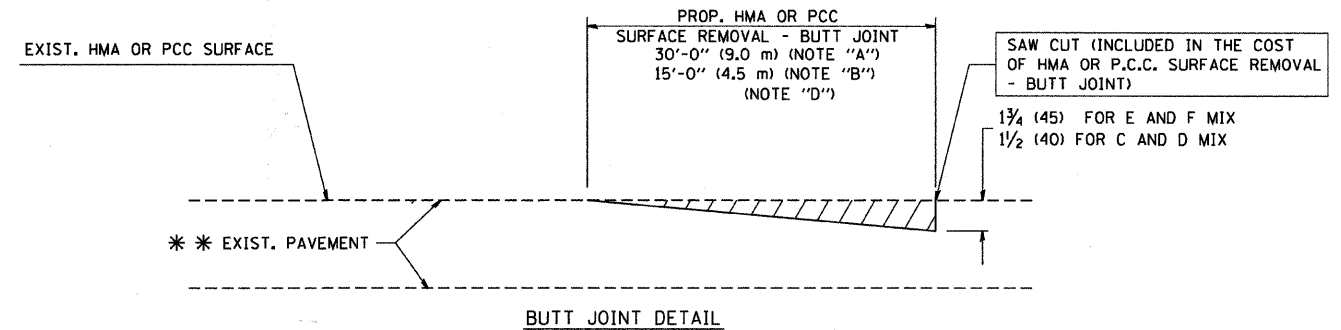
OPTION 2

TYPICAL TEMPORARY RAMP

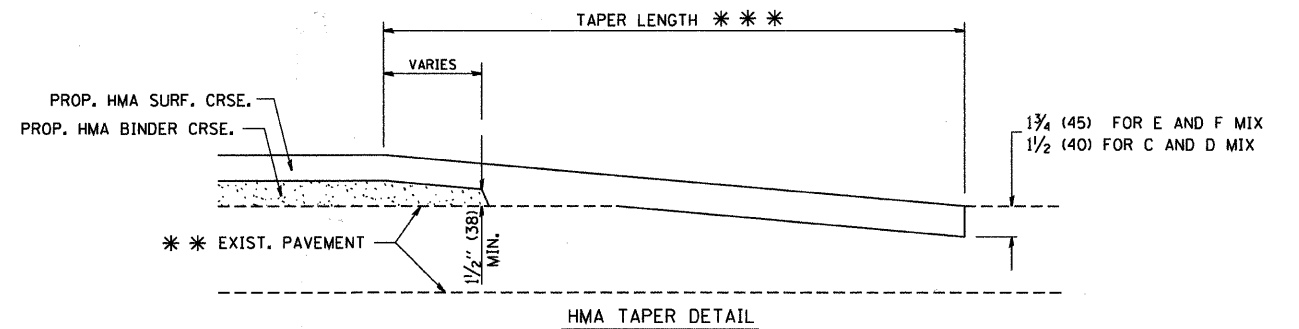


BUTT JOINT AND
HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
F.A.U. ROUTE 3877 EAST MAIN STREET
PROJECT No: M-ARA-9003(313)
JOB No: C-91-619-09

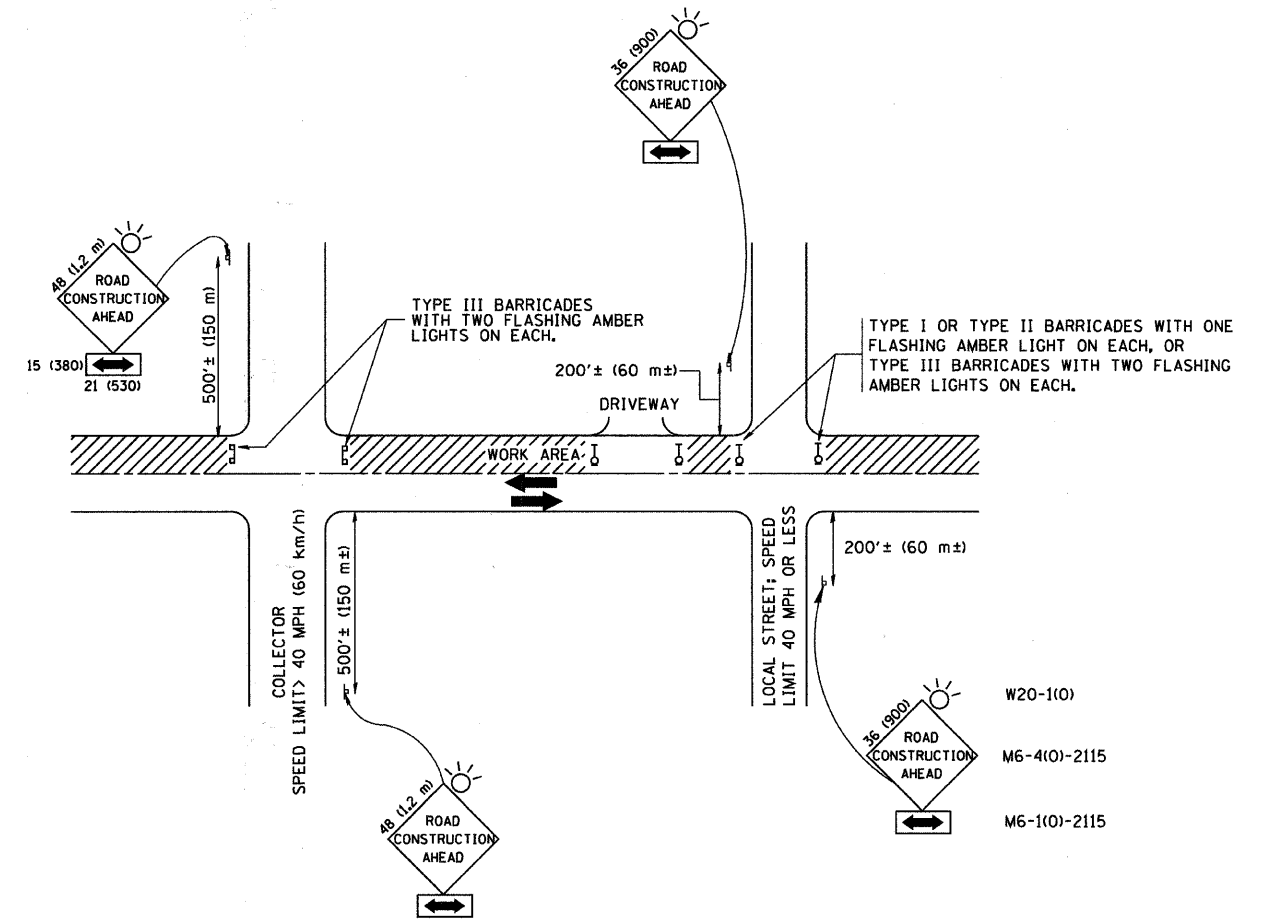
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - A. ABBAS 03-21-97
		CHECKED -	REVISED - M. GOMEZ 04-06-01
		DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	08-00054-00-CH	McHENRY	56	45
BD400-05 BD32			CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAYS:
 - USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
 F.A.U. ROUTE 3877 EAST MAIN STREET
 PROJECT No: M-ARA-9003(313)
 JOB No: C-91-619-09

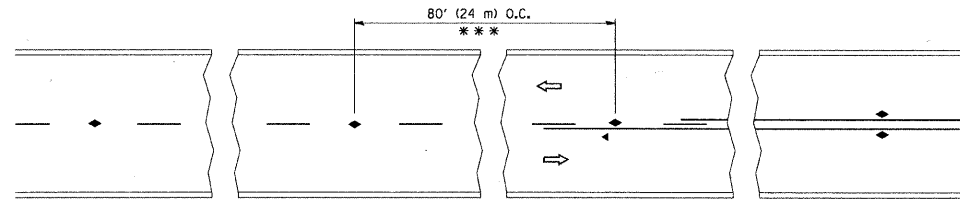
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\distatd\22x34\tcl0.dgn	USER NAME = goglionobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 58.000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

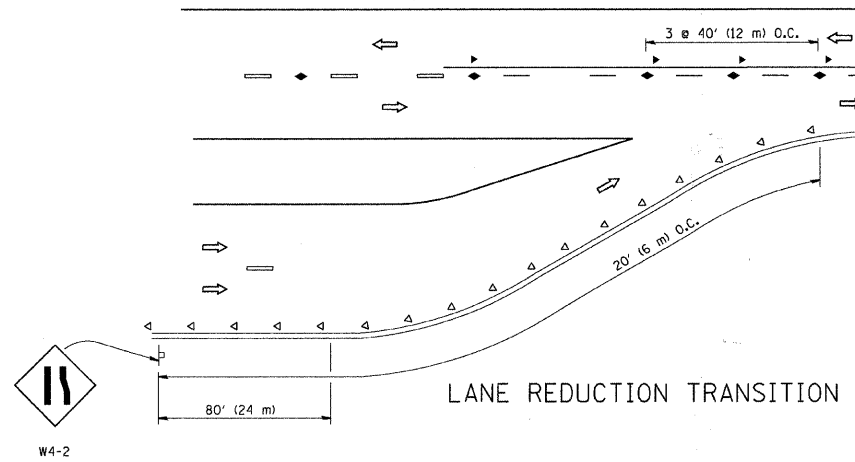
**TRAFFIC CONTROL AND PROTECTION FOR
 SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**
 SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	08-00054-00-CH	McHENRY	56	46
TC-10			CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

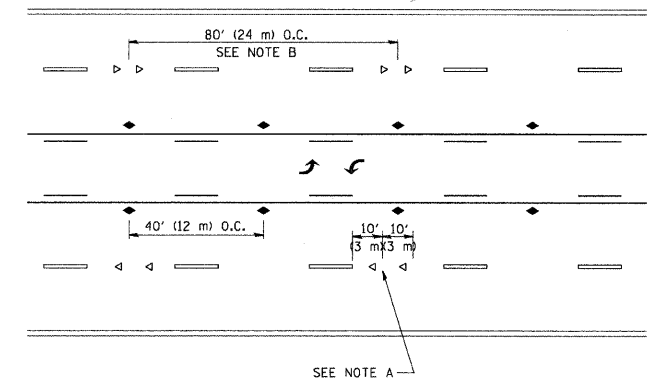


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

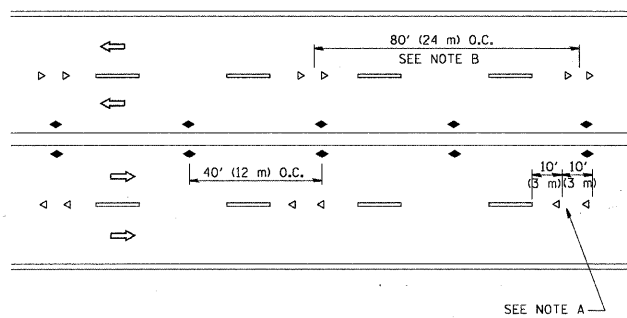
TWO-LANE/TWO-WAY



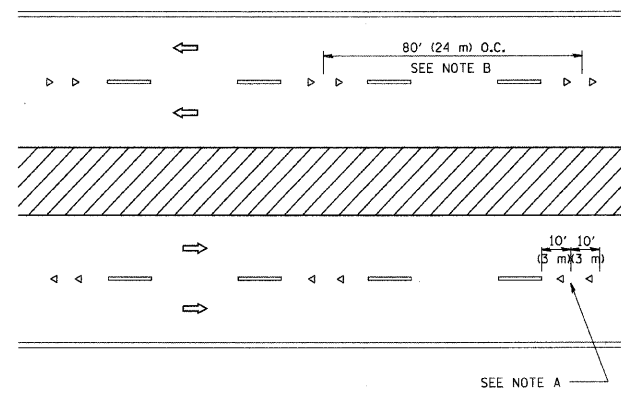
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

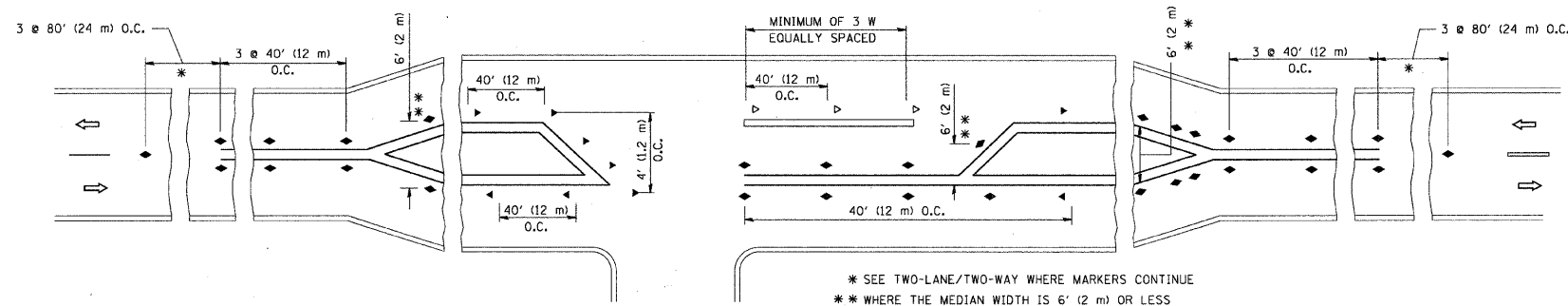
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
 F.A.U. ROUTE 3877 EAST MAIN STREET
 PROJECT No: M-ARA-9003 (313)
 JOB No: C-91-619-09

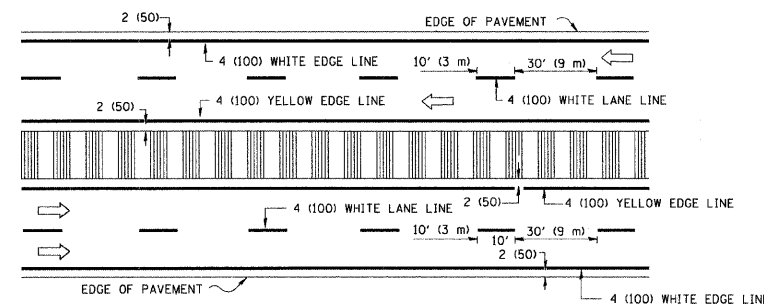
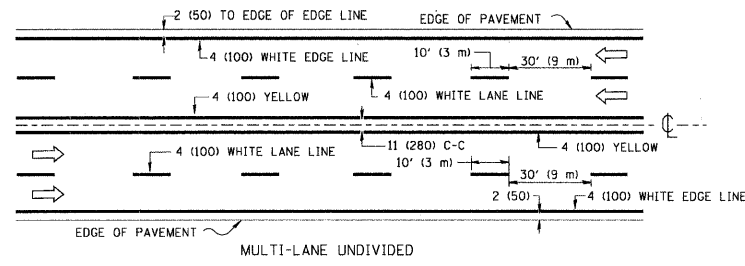
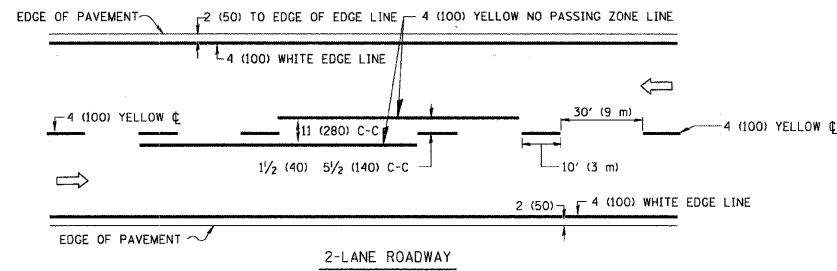
All dimensions are in inches (millimeters) unless otherwise shown.

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		CHECKED -	REVISED - T. RAMMACHER 01-06-00
		DATE -	REVISED - C. JUICIUS 09-09-09

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

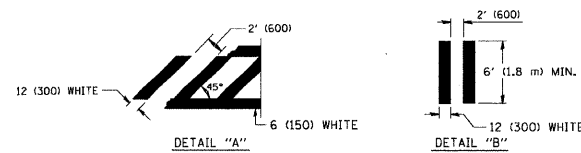
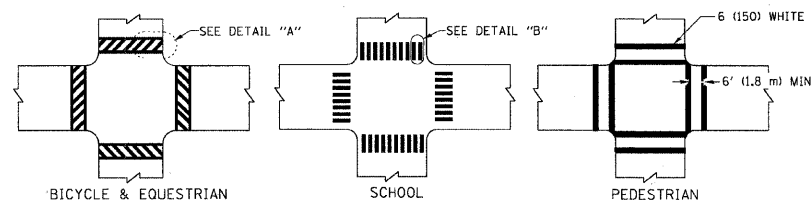
TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	08-00054-00-CH	McHENRY	56	47
TC-11			CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

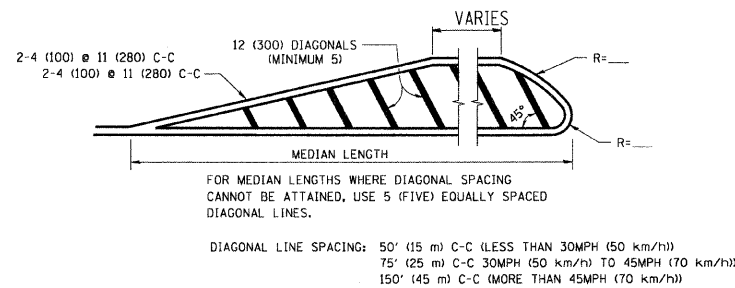
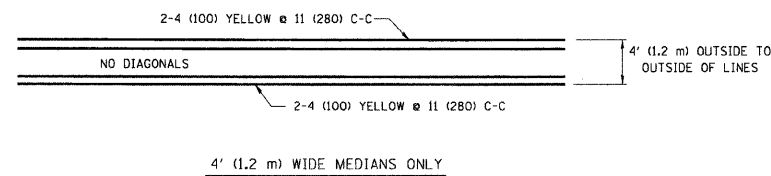


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

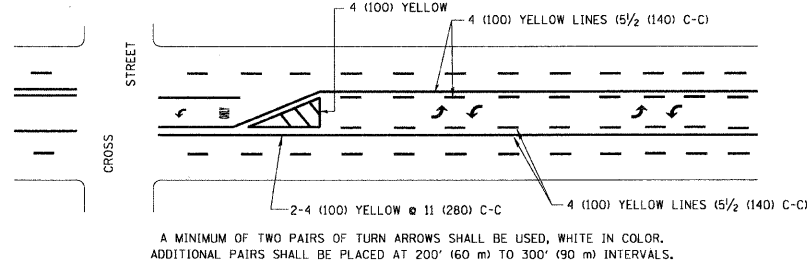
TYPICAL LANE AND EDGE LINE MARKING



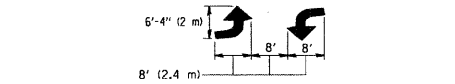
TYPICAL CROSSWALK MARKING



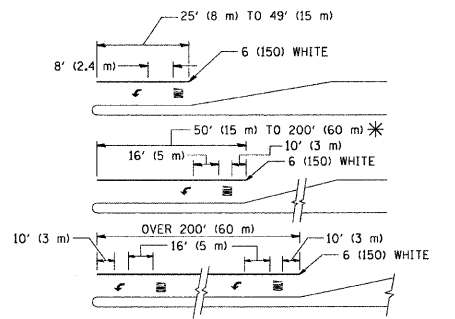
MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING



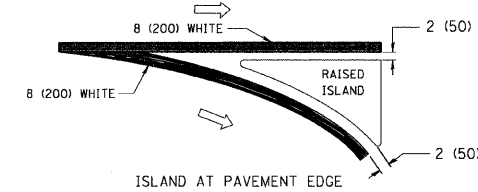
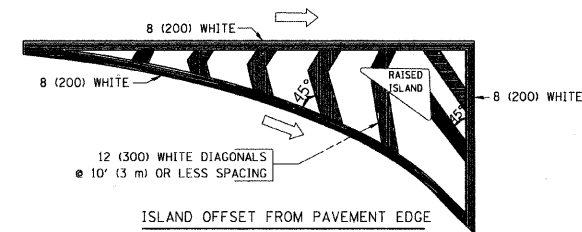
TYPICAL TURN LANE MARKING



* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "RR"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

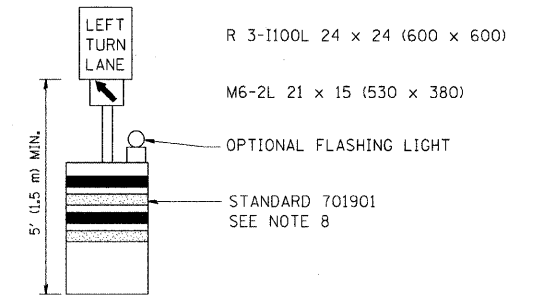
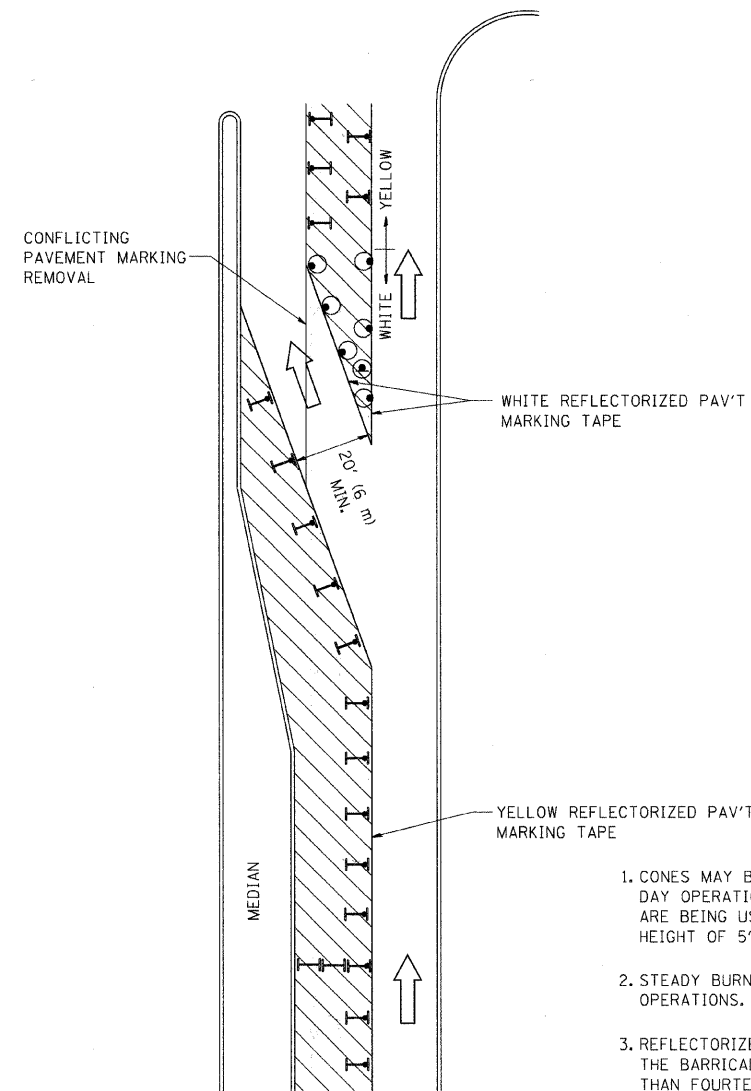
* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
F.A.U. ROUTE 3877 EAST MAIN STREET
PROJECT No: M-ARA-9003 (313)
JOB No: C-91-619-09

FILE NAME =	USER NAME = drivakasn	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
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		CHECKED -	REVISED -
		DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. - RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	08-00054-00-CH	MCHEMRY	56	48
TC-13		CONTRACT NO. 63404		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				









GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
 F.A.U. ROUTE 3877 EAST MAIN STREET
 PROJECT No: M-ARA-9003(313)
 JOB No: C-91-619-09

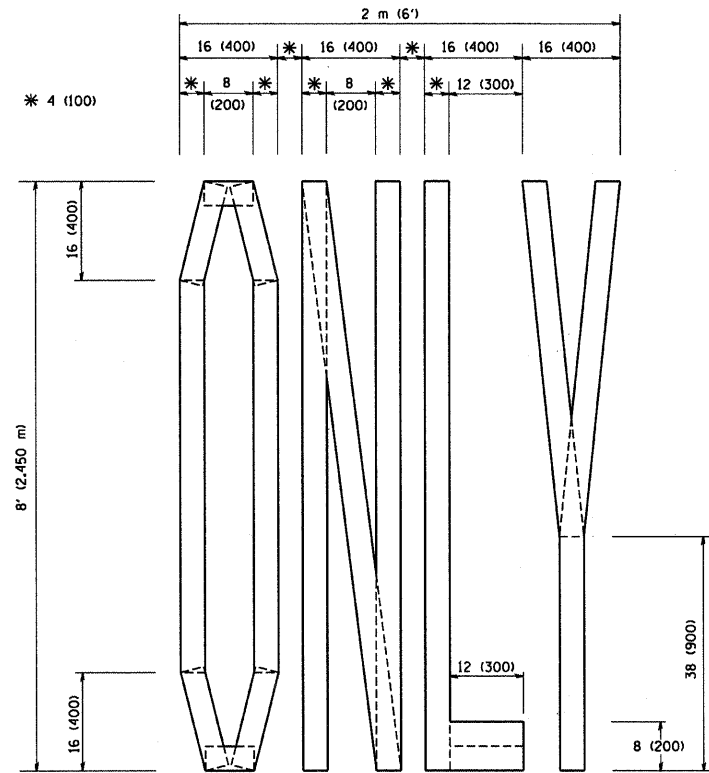
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		REVISED - A. HOUSEH 10-12-96	REVISED -
		REVISED -T. RAMMACHER 01-06-00	REVISED -
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	PLOT DATE = 9/14/2009		

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

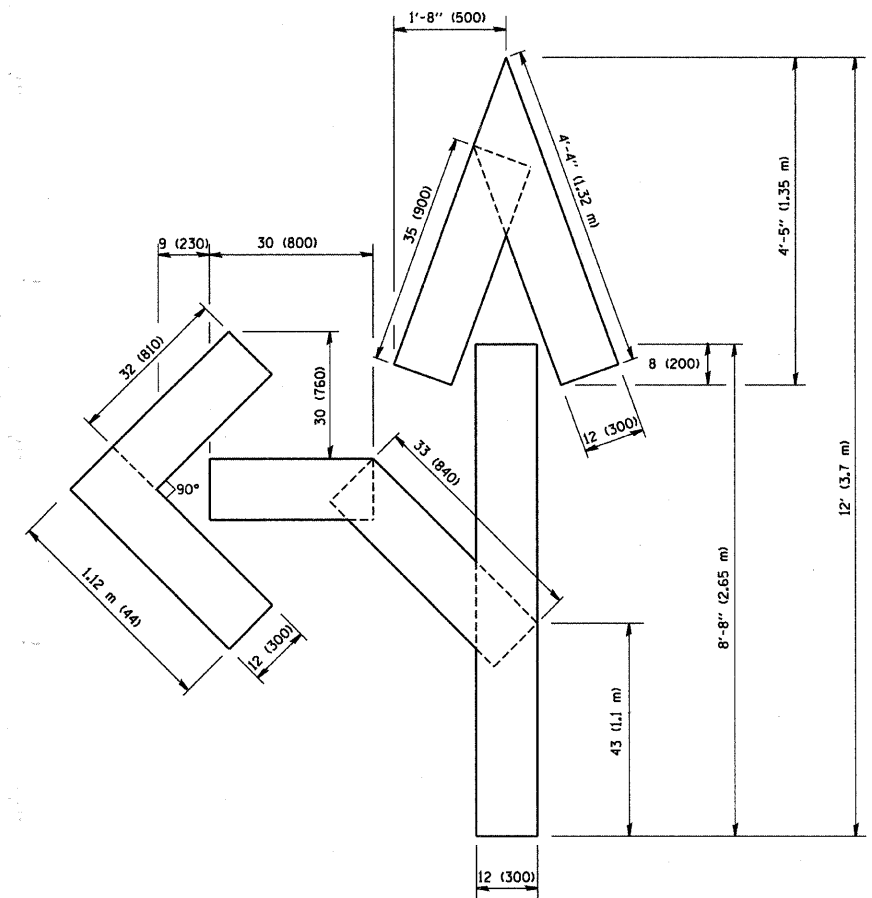
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
 (TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

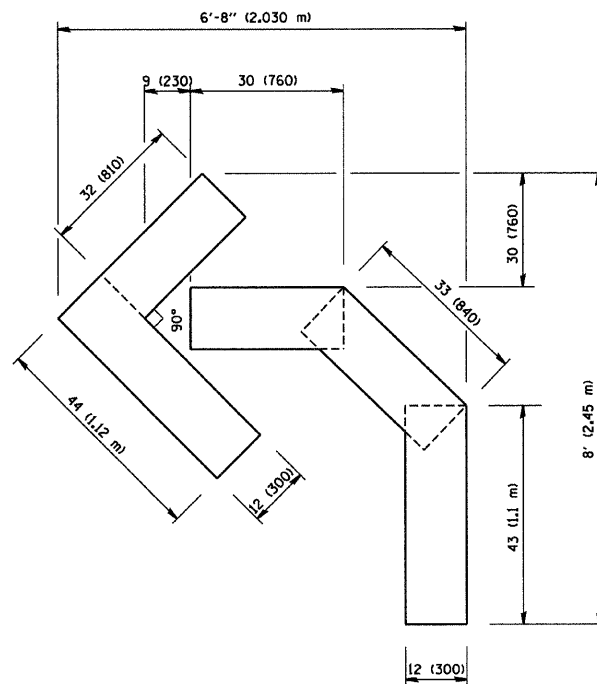
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	08-00054-00-CH	McHENRY	56	49
TC-14			CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)

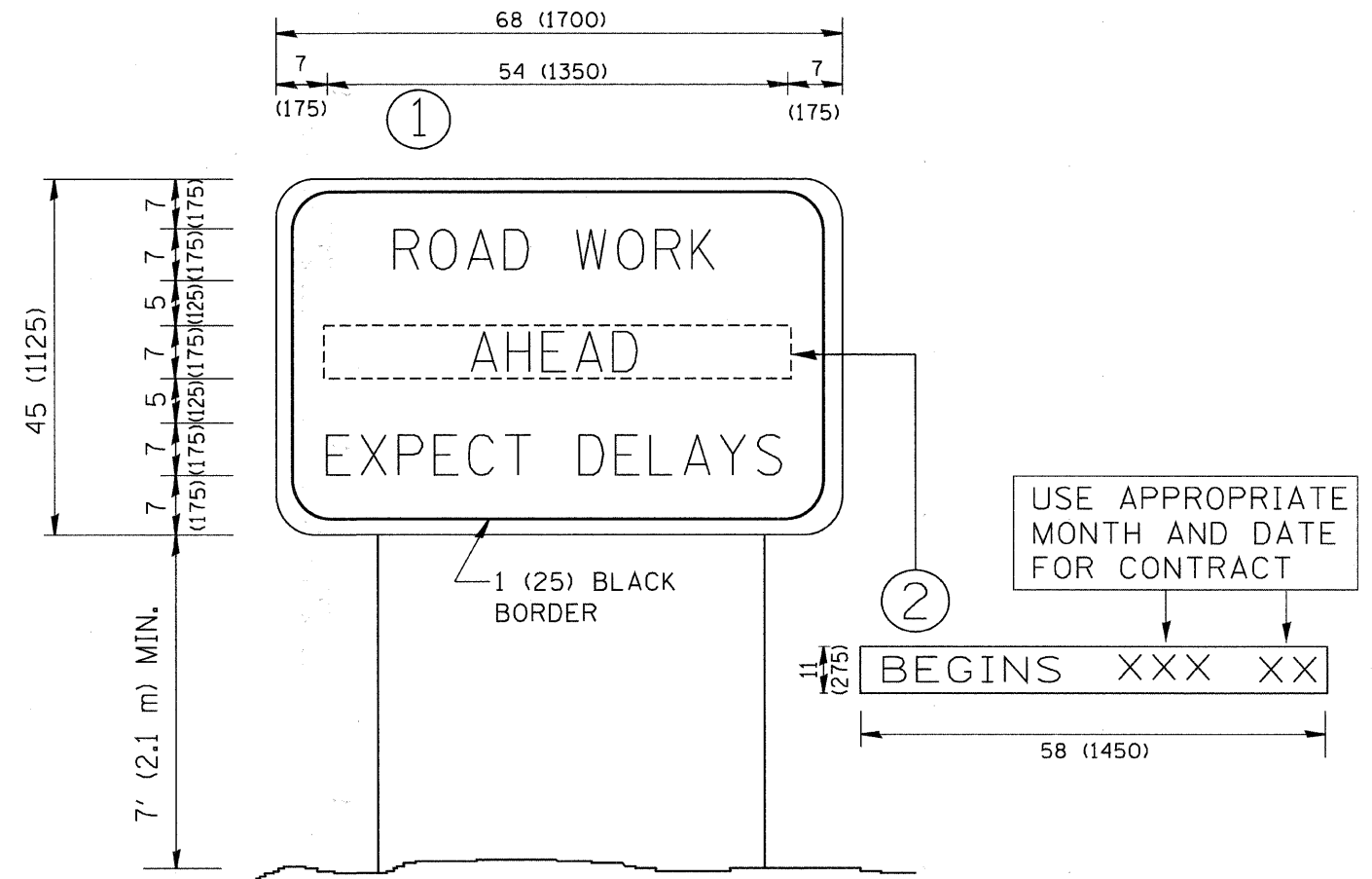


QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
 F.A.U. ROUTE 3877 EAST MAIN STREET
 PROJECT No: M-ARA-9003(313)
 JOB No: C-91-619-09

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\data\22x34\16.dgn	USER NAME = gaglianobt	DESIGNED - DRAWN -	REVISED -T. RAMMACHER 06-05-96 REVISED -T. RAMMACHER 11-04-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING				F.A. - RTE. *	SECTION 08-00054-00-CH	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 50
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -T. RAMMACHER 03-02-98		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-16		CONTRACT NO. 63404		
PLOT DATE = 1/4/2008		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
 F.A.U. ROUTE 3877 EAST MAIN STREET
 PROJECT No: M-ARA-9003(313)
 JOB No: C-91-619-09

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
 UNLESS OTHERWISE SHOWN.

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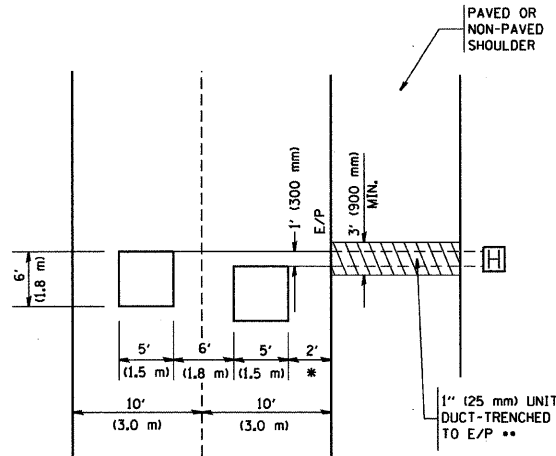
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ARTERIAL ROAD INFORMATION SIGN			
SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA. TO STA.

F.A. RTE. *	SECTION 08-00054-00-CH	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 51
TC-22			CONTRACT NO. 63404	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

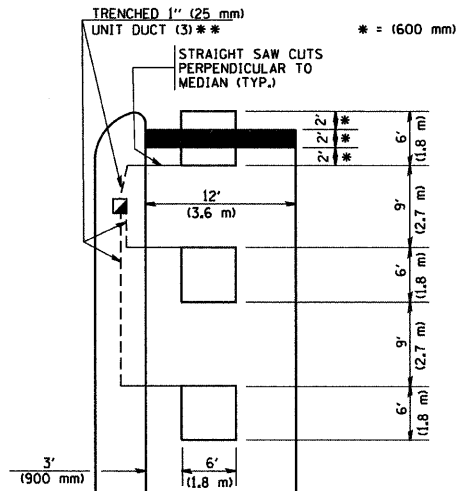


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

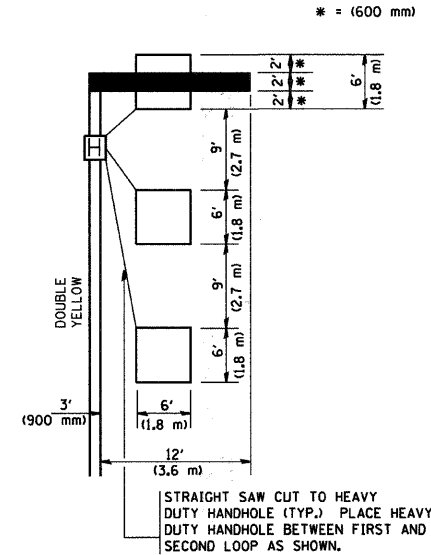
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

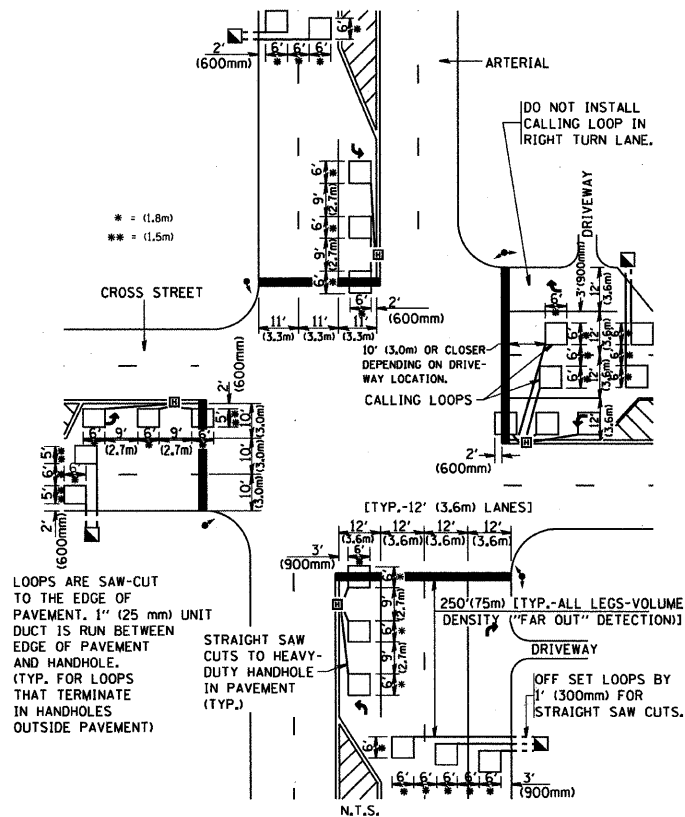
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



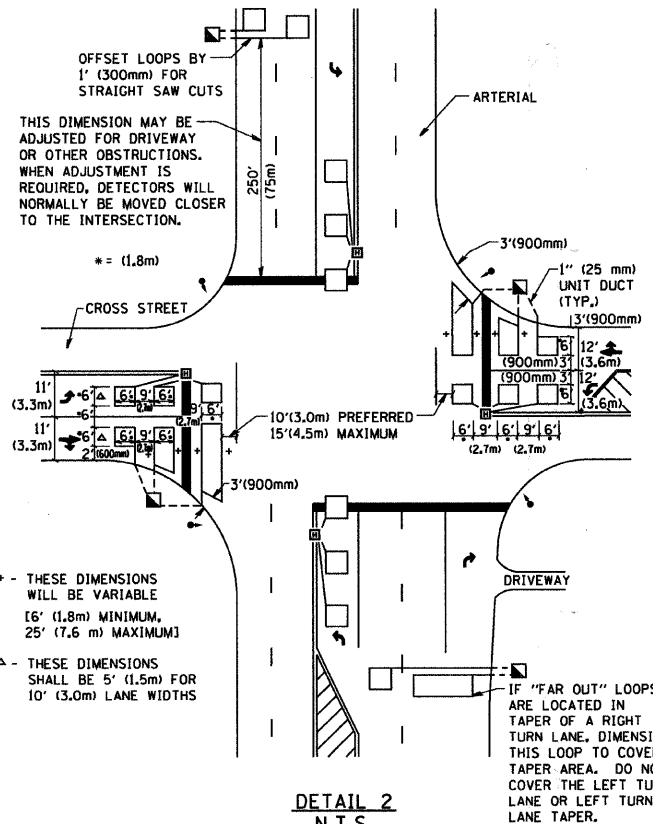
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

* F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)
F.A.U. ROUTE 3877 EAST MAIN STREET
PROJECT No: M-ARR-9003(313)
JOB No: C-91-619-09

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		DRAWN -	REVISED -
		CHECKED - R.K.F.	REVISED -
		DATE -	REVISED -

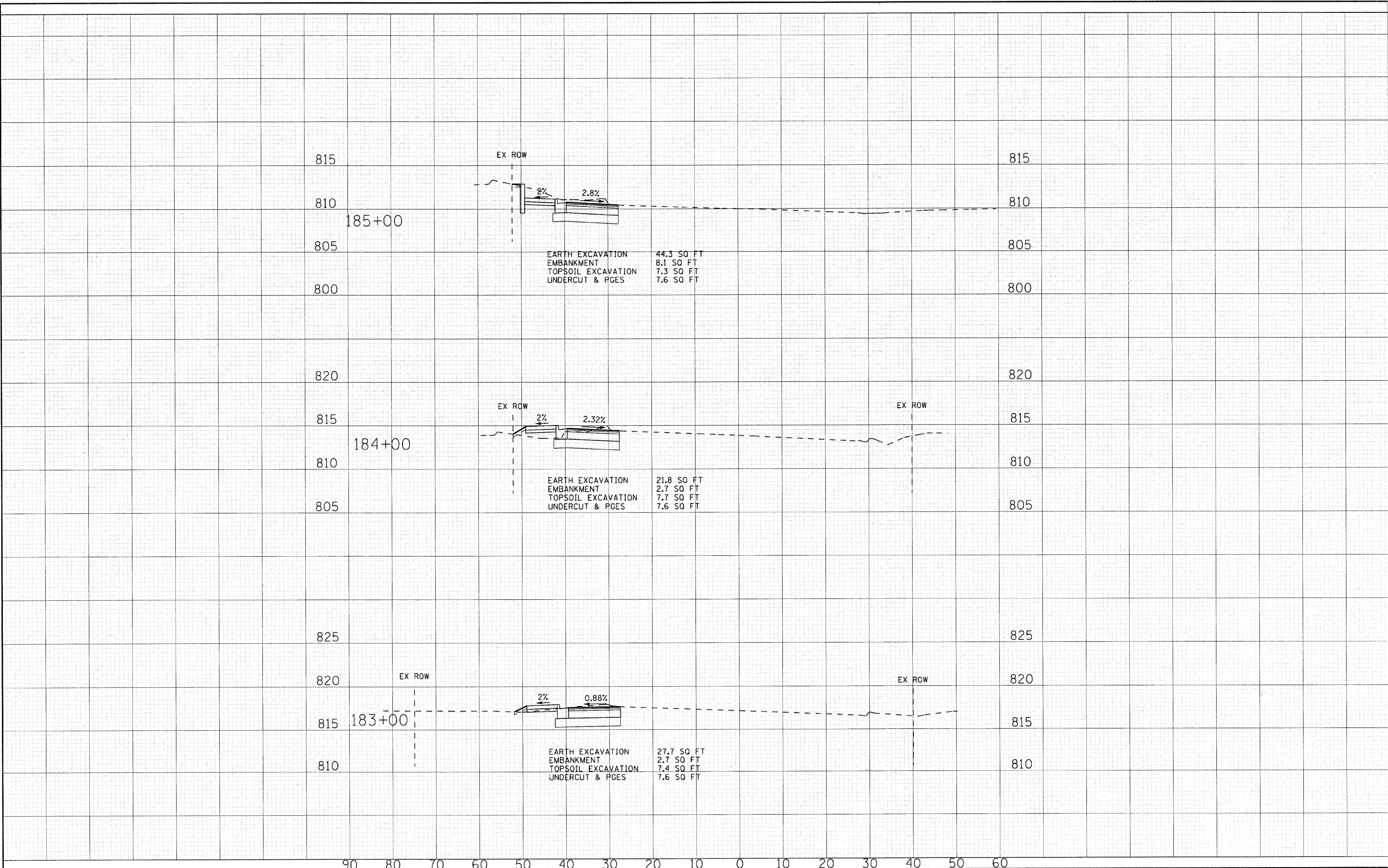
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	08-00054-00-CH	MCHENRY	56	52
TS-07				CONTRACT NO. 63404
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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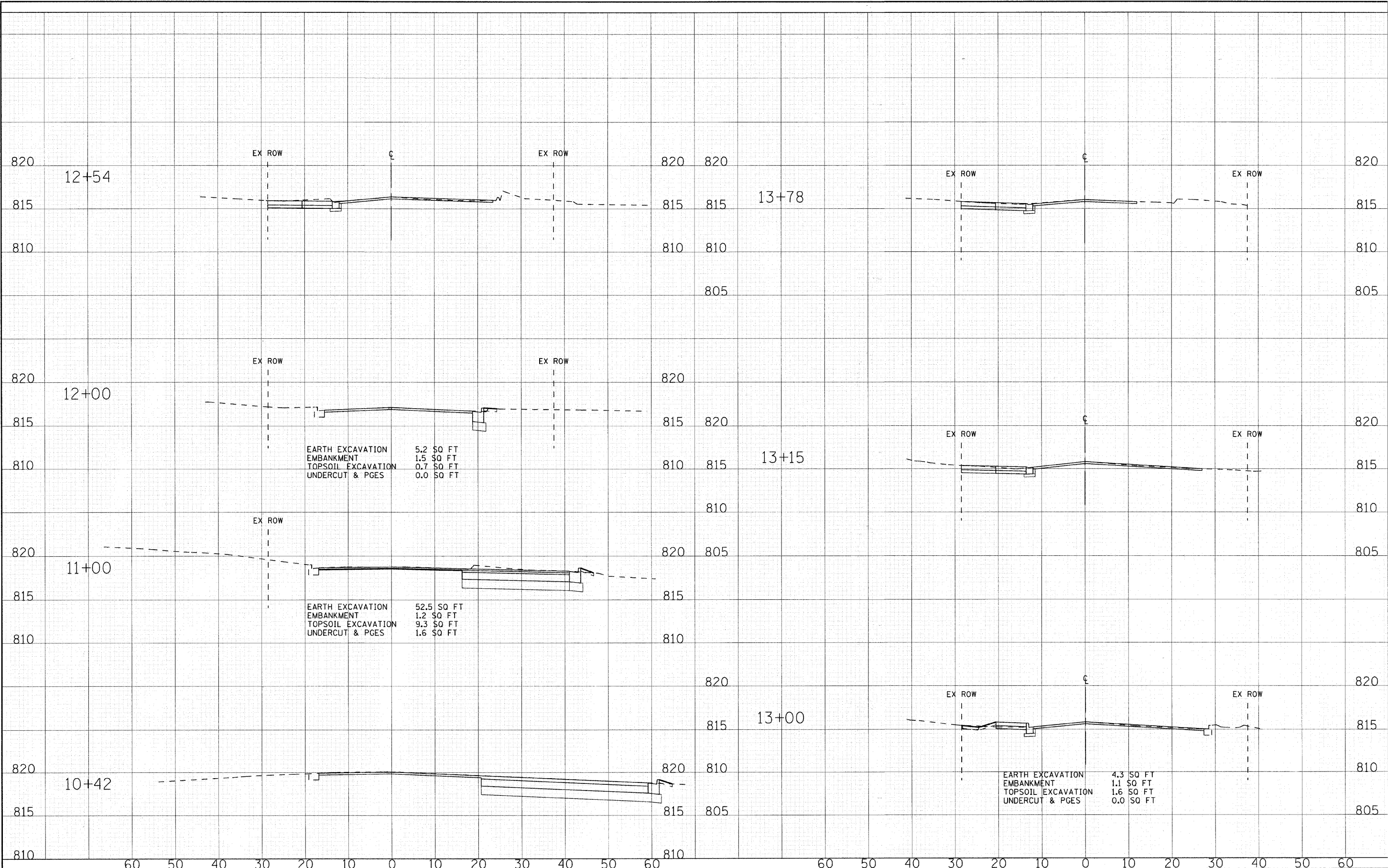
DESIGNED - MWP	REVISED - MCDPD REVIEW 11/3/09
DRAWN - MAC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	REVISED -

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

CROSS SECTIONS
U.S. ROUTE 14
 SCALE: H: 1"=10' V: 1"=5'
 STA. 183+00 TO STA. 185+00

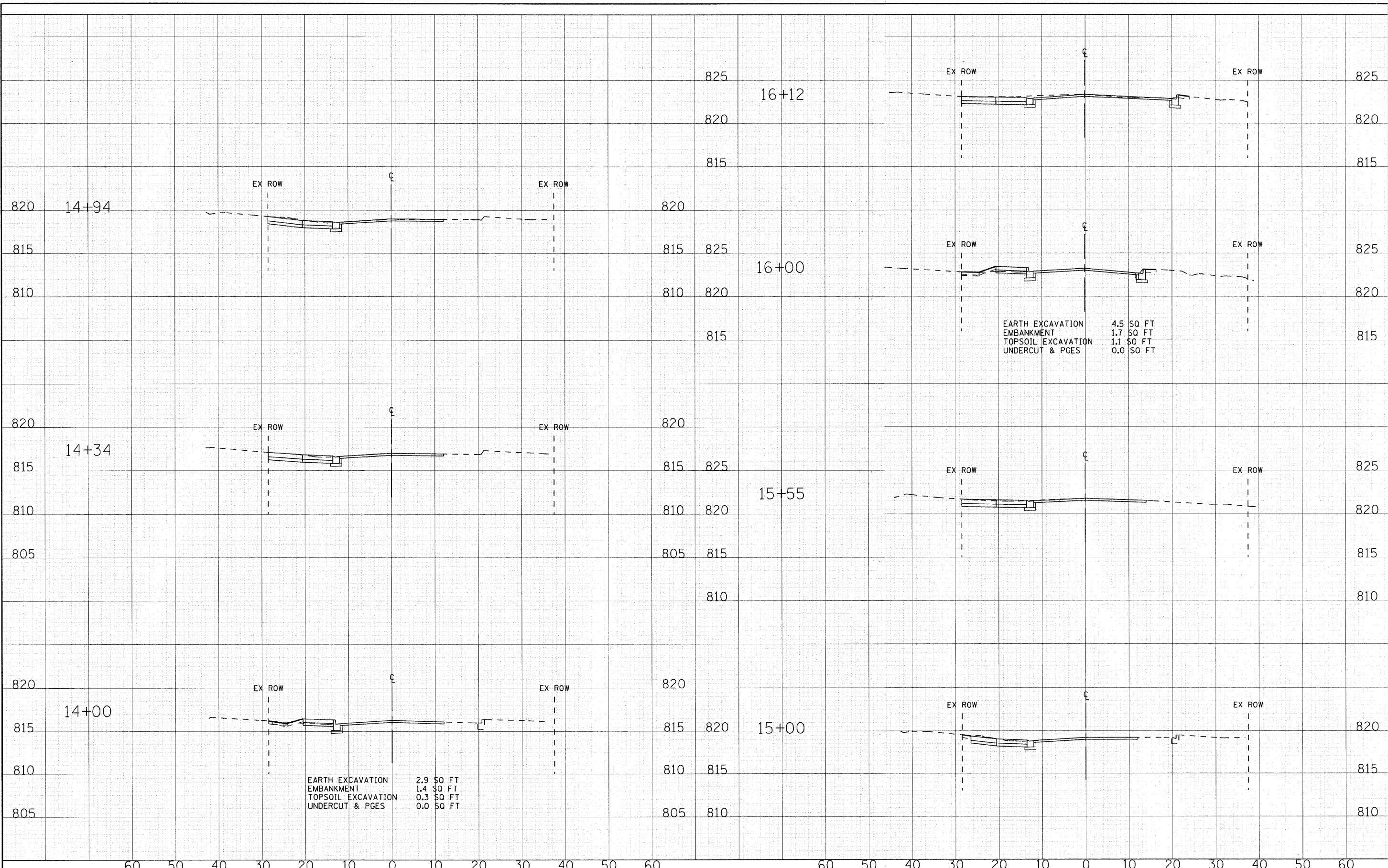
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	08-00054-00-CH	McHENRY	56	53
C-91-619-09		CONTRACT NO. 63404		
FED. ROAD DIST. NO. 1 (ILLINOIS)		FED. AID PROJECT M-ARA-90031313		
• F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY) F.A.U. ROUTE 3877 EAST MAIN STREET				

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 LICENSE NO. - BR-002071 - EXPIRES 4/30/2011
 PROJECT: VILLAGE CARY, ILLINOIS
 SHEET: 56 OF 54



	DESIGNED - MWP	REVISED - MCDPD REVIEW 11/3/09	VILLAGE CARY, ILLINOIS U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP AND EAST MAIN STREET ARRA IMPROVEMENTS	CROSS SECTIONS EAST MAIN STREET		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN - MAC	REVISED - IDOT REVIEW 11/17/09		STA. 12+54 TO STA. 13+78	08-00054-00-CH	McHENRY	56	54		
CHECKED - RWL	REVISED -		SCALE: H: 1"=10' V: 1"=5'		C-91-619-09		CONTRACT NO. 63404			
DATE - 09/11/09	REVISED -				FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT M-ARRA-9003313J			
					F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY)		F.A.U. ROUTE 3877 EAST MAIN STREET			

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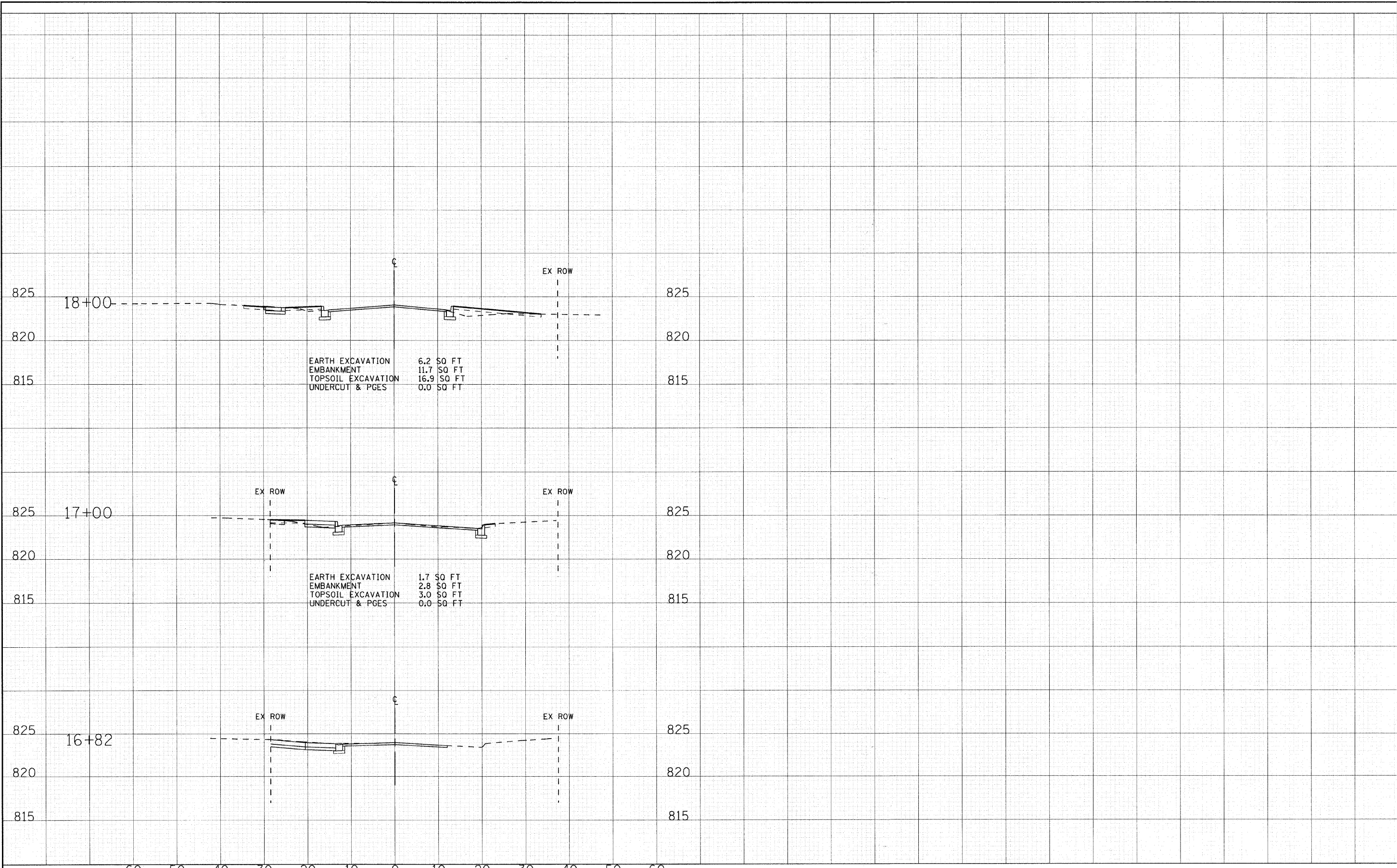
DESIGNED - MWP	REVISED - MCDPD REVIEW 11/3/09
DRAWN - MAC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	REVISED -

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

CROSS SECTIONS
EAST MAIN STREET
 SCALE: H: 1"=10' V: 1"=5'
 STA. 14+00 TO STA. 16+12

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	08-00054-00-CH	McHENRY	56	55
	C-91-619-09			CONTRACT NO. 63404
	FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT M-ARA-90031313
				F.A.U. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY) F.A.U. ROUTE 3877 EAST MAIN STREET

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 EXPIRES 07/30/2011
 08/22/2009



DESIGNED - MWP	REVISED - MCDPD REVIEW 11/3/09
DRAWN - MAC	REVISED - IDOT REVIEW 11/17/09
CHECKED - RWL	REVISED -
DATE - 09/11/09	REVISED -

VILLAGE CARY, ILLINOIS
U.S. ROUTE 14 AT EAST MAIN STREET RIGHT-TURN LANE STP
AND EAST MAIN STREET ARRA IMPROVEMENTS

CROSS SECTIONS
EAST MAIN STREET
 SCALE: H: 1"=10' V: 1"=5'
 STA. 16+82 TO STA. 18+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	08-00054-00-CH	McHENRY	56	56
C-91-619-09		CONTRACT NO. 63404		
FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT M-ARA-9003313J		
• F.A.P. ROUTE 0305 U.S. ROUTE 14 (NORTHWEST HIGHWAY) F.A.U. ROUTE 3877 EAST MAIN STREET				