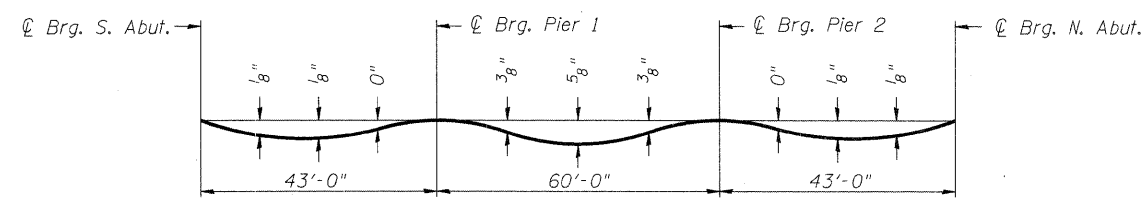


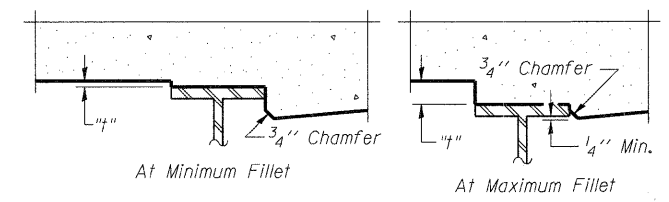
PLAN



DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)

Note:
The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown below.



To determine "h": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown below, minus slab thickness, equals the fillet heights "h" above top flange of beams.

FILLET HEIGHTS

TOP OF SLAB ELEVATIONS
THOMPSON ROAD
OVER NIPPERSINK CREEK
SECTION NO. 06-00005-00-BR
McHENRY COUNTY
STATION 206+30.48
STRUCTURE NO. 056-6006

DATE: 11/18/09

DESIGNED	MGH/JPG
CHECKED	KMA
DRAWN	WJH
CHECKED	RGD

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SHEET NO. S-9	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	4081	06-00005-00-BR	McHENRY	45	20
S-25 SHEETS			CONTRACT NO. 63409		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

FILE NAME: 060561.dwg PLOT DATE: 11/18/09 PLOT DRIVER: plot.plt PEN TABLE: standard-trans.tbl