

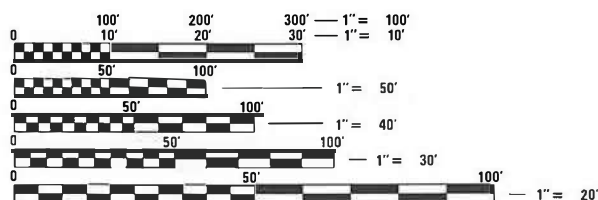
HIGHWAY STANDARDS

424001-11	631031-17	701901-08
424006-05	701311-03	780001-05
424011-04	701427-05	781001-04
424031-02	701606-10	782006-01
606001-07	701701-10	BLR-21-9
630001-12	701801-06	BLR-22-7

CADD STANDARDS

406101-D4	630101-D4
423101-D4	780001-D4
440001-D4	

ADT = 10,400 (2017)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811

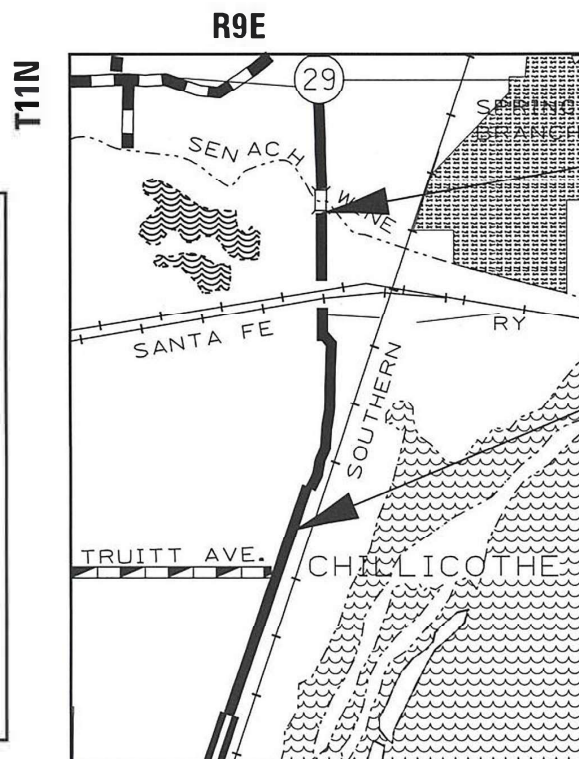
PROJECT ENGINEER: RICH DOTSON (309)671-3455
PROJECT MANAGER: MICHAEL HUDELSON (309)671-3466
CONTRACT NO. 68F12
CATALOG NO. 035834-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 318 (IL 29)
SECTION 8RS-8;(6,5)RS-3
PROJECT NHPP-2XAD(322)
PAVEMENT PRESERVATION POLICY
PEORIA COUNTY

C-94-002-20



STATION EQUATIONS

1. STA 140 + 16.09 BK =
STA 1551 + 99.79 AH
2. STA 1567 + 99.52 BK =
STA 568 + 05.41 AH
3. STA 572 + 05.60 BK =
STA 36 + 19.18 AH
4. STA 37 + 45.31 BK =
STA 34 + 93.05 AH
5. STA 38 + 84.07 BK =
STA 31 + 02.03 AH
6. STA 38 + 67.89 BK =
STA 23 + 36.17 AH

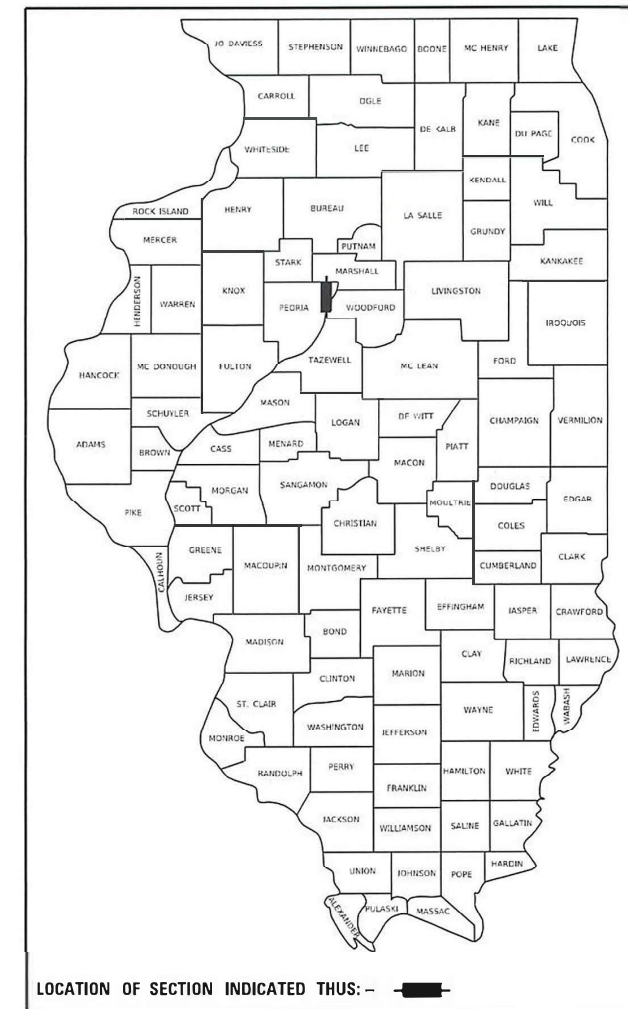
BEGIN IMPROVEMENT
(S. EDGE SN 072-0014)
STA 125 + 08

END IMPROVEMENT
(N. OF TRUITT AVE.)
STA 23 + 85

GROSS LENGTH = 4975 FT. = 0.94 MILE
NET LENGTH = 4975 FT. = 0.94 MILE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;(6,5)RS-3	PEORIA	48	1
		ILLINOIS	CONTRACT NO. 68F12	

D-94-001-20



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED Dec 11 20 20
Kensel A Barnett RSP
REGIONAL ENGINEER

January 29, 2021
[Signature]
ENGINEER OF DESIGN AND ENVIRONMENT

January 29, 2021
[Signature]
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

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- 39-48 DISTRICT 4 STANDARDS

AVAILABILITY OF ELECTRONIC FILES

MicroStation and GEOPAK files of this project will be made available to the Contractor after contract award. If there is a conflict between the electronic files and the printed contract plans and documents, the printed contract plans and documents shall take precedence over the electronic files. The Contractor shall accept all risk associated with using the electronic files and shall hold the Department harmless for any errors or omissions in the electronic files and the data contained therein. Errors or delays resulting from the use of the electronic files by the Contractor shall not result in an extension of time for any interim or final completion date or shall not be considered cause for additional compensation. The Contractor shall not use, share, or distribute these electronic files except for the purpose of constructing this contract. Any claims by third parties due to use or errors shall be the responsibility of the Contractor. The Contractor shall include this disclaimer with the transfer of these electronic files to any other parties and shall include appropriate language binding them to similar responsibilities.

CONSECUTIVE SIDE STREET (ROAD) CLOSURE – PROHIBITED

1. No two consecutive side streets (roads) may be closed at the same time during construction. The Contractor must alternate streets (roads).
2. Adjacent sideroads will not be closed simultaneously. BLR Standard 21 shall be used for all local road closures without any entrances within the closed area. BLR Standard 22 can be used where it is necessary to allow local traffic access.

EARTH EXCAVATION – INCIDENTAL TO CURB, GUTTER, & DRIVEWAY

Earth excavation and backfill for proposed curb and gutters and driveway pavements shall be included in the unit cost of the various items.

CROSSING EXISTING STRUCTURES WITH EQUIPMENT

SN 072-0014 may be crossed with the empty MTD.
Any structures not listed above shall be verified by the resident prior to beginning work.

TAPER REMOVAL @ FRAME & GRATES ADJUSTED BY OTHERS

At locations where frames and grates have previously been adjusted by others and they are surrounded by hot-mix asphalt tapers, the Contractor for this contract shall remove and dispose of the hot-mix asphalt taper material prior to the placement of the hot-mix asphalt surface course. This work will not be paid for separately, but will be considered as included in the cost of the HOT-MIX ASPHALT SURFACE COURSE pay item.

TRANSITION PAYMENT METHOD – NEW/OLD CONSTRUCTION

Ten feet (10 ft.) (3 m) transitions shall be used to match proposed items of work to existing items in the field unless otherwise shown. The transition shall be paid for at the contract unit price for the proposed item of work specified.

ADDITIONAL SUPPLEMENTAL TRAFFIC CONTROL

The Department reserves the right at any time to add additional Traffic Control Systems or devices within the active contract limits, by means of an additional contract. All terms of Article 105.08 of the Standard Specifications shall be followed by each Contractor.

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USER NAME = hudelsonme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN -	REVISED -	318				8RS-8;(6,5)RS-3	PEORIA	48	2	
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.		CONTRACT NO. 68F12				
PLOT DATE = 12/11/2020	DATE -	REVISED -		ILLINOIS FED. AID PROJECT						

ENVIRONMENTAL REVIEWS

Prior to the use of any proposed borrow areas, use areas (temporary access roads, detours, run-arounds, etc.) and/or waste areas, the Contractor shall file the required environmental resource request surveys according to Section 107.22 of the Standard Specifications. These surveys are required in order for the Department to conduct cultural and biological resource surveys for the proposed site.

The required environmental resource documentation shall include the following:

- * BDE Form 2289 (Borrow Site Review)
- * BDE Form 2290 (Waste/Use Area Review)
- * A location map showing the size limits and location of the use area
- * Color photographs depicting the use area
- * Borrow Area Entry Agreement form – D4 PI0101

Prior to any waste materials being removed from the construction site the required environmental resource surveys shall be obtained and filed by the Contractor. Excess waste products removed from the construction site shall be disposed of as required in Section 202.03 of the Standard Specifications.

Any protruding metal bars shall be removed prior to the disposal of broken concrete at approved disposal sites.

Please note that a minimum of four weeks shall be allowed for the District to obtain the required waste site environmental clearances and six weeks for the required borrow site environmental clearances.

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USER NAME = hudelsonme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN -	REVISED -	318				8RS-8;(6,5)RS-3	PEORIA	48	3	
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.		CONTRACT NO. 68F12				
PLOT DATE = 12/11/2020	DATE -	REVISED -		ILLINOIS FED. AID PROJECT						

PAVEMENT STATIONING NUMBERS & PLACEMENT

The Contractor shall provide labor and materials required to imprint pavement station numbers in the finished surface of the pavement and/or overlay. The numbers shall be approximately 3/4 inch (20 mm) wide, 5 inches (125 mm) high and 5/8 inch (15 mm) deep.

The pavement station numbers shall be installed as specified herein:

Interval – 200 feet (English stationing) or 100 meters (metric stationing)

Bottom of Numbers – 6 inches (150 mm) from the inside edge of the pavement marking

Location:

- 2, 3, & 5 Lane Pavements – right edge of pavement in direction of increasing stations
- Multi-Lane Divided Roadways – outside edge of pavement in both directions
- Ramps – along baseline edge of pavement

Position – stations shall be placed so they can be read from the adjacent shoulder

Format – English (Metric) pavement stations shall use this format "XXX (XX + X00)", where X represents the pavement station

This work will not be paid for separately, but will be included in the cost of the associated pavement and/or overlay pay items.

COMMITMENTS

Commitments are not to be altered without the written approval of all parties to which the commitment was made.
NO COMMITMENTS WERE MADE.

POLYMERIZED BITUMINOUS MATERIALS (TACK COAT) RATES

Surface Type	Residual Rate
Milled (HMA or PCC)	0.08 lb /sq ft
Existing Pavement	0.08 lb /sq ft
Fog Coat (between lifts)	0.08 lb /sq ft

NO PASSING ZONE VERIFICATION

The resident shall contact Operations to verify the location of no passing zones prior to placement of centerline striping.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

Mixture Use(s):	Polymer Surface (1.5" and 1.75")	Polymer Binder (1.5" and 2")	Shoulders Surface Lift	Shoulders Bottom Lift	Incidental Surface			
AC/PG:	SBS or SBR 76-28	SBS or SBR 76-28	PG 64-22	PG 64-22	PG 64-22			
Design Air Voids:	4.0% @ N=70	4.0% @ N=70	4.0% @ N=50	4.0% @ N=50	4.0% @ N=50			
Mixture Composition: (Mixture Gradation)	IL 9.5	IL 9.5	IL 9.5FG	IL 9.5	IL 9.5			
Friction Aggregate:	Mix E	N.A.	Mix C	N.A.	Mix C			
Quality Management Program:	QCP	QCP	QCQA	QCQA	QCQA			

Note: 1) Individual lift thickness of each mix type will be no less than 3 times nominal maximum aggregate size and no more than 6 times nominal maximum aggregate size, unless otherwise approved by the Engineer.
 2) For design purposes, mixture weight for all mixes is determined to be 112.0 lb/s.y./in., unless otherwise noted.
 3) Sublot sizes for PFP and QCP mixes will be 500 tons, unless otherwise agreed to by the Engineer and the paving contractor.

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES
 AND COMMITMENTS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;(6,5)RS-3	PEORIA	48	4
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				



Status of Utilities

Name of Utility Company _____

City of Chillicothe

Route FAP 318 (IL 29)
 Section 8RS-6;6RS-3
 County Peoria
 Contract No. 68F12
 Catalog No. 035834-00D

Route	Offset	Location	Type of Utility	Type of Conflict	Disposition
IL 29	Rt 35'	Sta 567+10	Watermain	ADA Ramps	Caution
IL 29	Rt 80'	Sta 572+80	Watermain	ADA Ramps	Caution
IL 29	Lt 90'	35+40	Watermain	ADA Ramps	Caution
IL 29	Rt 100'	39+30	Watermain	ADA Ramps	Caution
IL 29	Lt 100'	40+00	Watermain	ADA Ramps	Caution
Note: Offset distance and station locations are approximate					
NOTE: Please check all your facilities within the construction limits of this project.					

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PLOT DATE = 12/11/2020	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STATUS OF UTILITIES			
SCALE:	SHEET	OF	SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;(6,5)RS-3	PEORIA	48	5
CONTRACT NO.				68F12

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	URBAN	
				CONST. CODE	80% FED 20% STATE
35300720	PORTLAND CEMENT CONCRETE BASE COURSE 13"	SQ YD	29	0006	29
40600982	HOT - MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	766	PEORIA CO.	766
40600990	TEMPORARY RAMP	SQ YD	189		189
40603208	POLYMERIZED HOT - MIX ASPHALT BINDER COURSE, IL-9.5, N70	TON	1898		1898
40604172	POLYMERIZED HOT - MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70	TON	1790		1790
40800050	INCIDENTAL HOT - MIX ASPHALT SURFACING	TON	75		75
42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	52		52
42400100	PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH	SQ FT	1351.5		1351.5
* 42400800	DETECTABLE WARNINGS	SQ FT	126.5		126.5
44000100	PAVEMENT REMOVAL	SQ YD	29		29
44000162	HOT - MIX ASPHALT SURFACE REMOVAL, 3 1/4"	SQ YD	12304		12304
44000164	HOT - MIX ASPHALT SURFACE REMOVAL, 3 3/4"	SQ YD	10159		10159
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	255		255
44000600	SIDEWALK REMOVAL	SQ FT	862		862

*= SPECIALTY ITEM

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PLOT DATE = 12/11/2020	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	BRS-8+(6,5)RS-3	PEORIA	48	6
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	URBAN	
				CONST. CODE	0006
				PEORIA CO.	
				80% FED	20% STATE
48101200	AGGREGATE SHOULDERS, TYPE B	TON	45	45	
48203012	HOT - MIX ASPHALT SHOULDERS, 3 3/4"	SQ YD	1483	1483	
60260100	INLETS TO BE ADJUSTED	EACH	2	2	
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	235	235	
* 63000001	STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS	FOOT	100	100	
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	2	2	
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	2	2	
63200310	GUARDRAIL REMOVAL	FOOT	332	332	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	5	5	
67100100	MOBILIZATION	L SUM	1	1	
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1	
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CA DA	90	90	

*= SPECIALTY ITEM

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PLOT SCALE = 75.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/11/2020	CHECKED -	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	BRS-8+(6,5)RS-3	PEORIA	48	7
				CONTRACT NO. 68F12

ILLINOIS FED. AID PROJECT

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	URBAN	
				CONST. CODE	PEORIA CO.
				0006	80% FED 20% STATE
70107025	CHANGEABLE MESSAGE SIGN	CA DA	22		22
70300100	SHORT - TERM PAVEMENT MARKING	FOOT	1013		1013
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	338		338
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	2		2
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	19695		19695
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	256		256
* 78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	7		7
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	256		256
X0300004	BITUMINOUS MATERIALS (TACK COAT), SPECIAL	POUND	32946		32946
X7010216	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1		1
* X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	19695		19695
Z0001002	GUARDRAIL AGGREGATE EROSION CONTROL	TON	63		63
Z0013798	CONSTRUCTION LAYOUT	L SUM	1		1

*= SPECIALTY ITEM

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PLOT SCALE = 75.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/11/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	BRS-8+(6,5)RS-3	PEORIA	48	8
CONTRACT NO. 68F12			ILLINOIS FED. AID PROJECT	

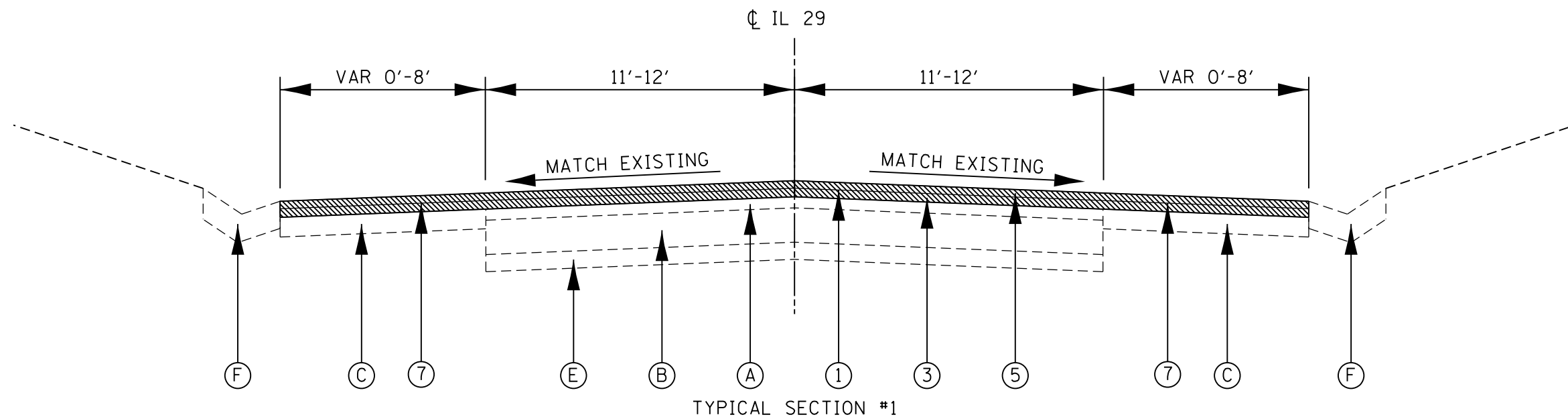
LEGEND

EXISTING ITEMS

- Ⓐ HMA OVERLAY
- Ⓑ PCC PAVEMENT
- Ⓒ HMA SHOULDERS
- Ⓓ PCC BASE COURSE
- Ⓔ STABILIZED SUB BASE
- Ⓕ PCC GUTTER
- Ⓖ PCC CURB AND GUTTER
- Ⓗ CORRUGATED PCC MEDIAN

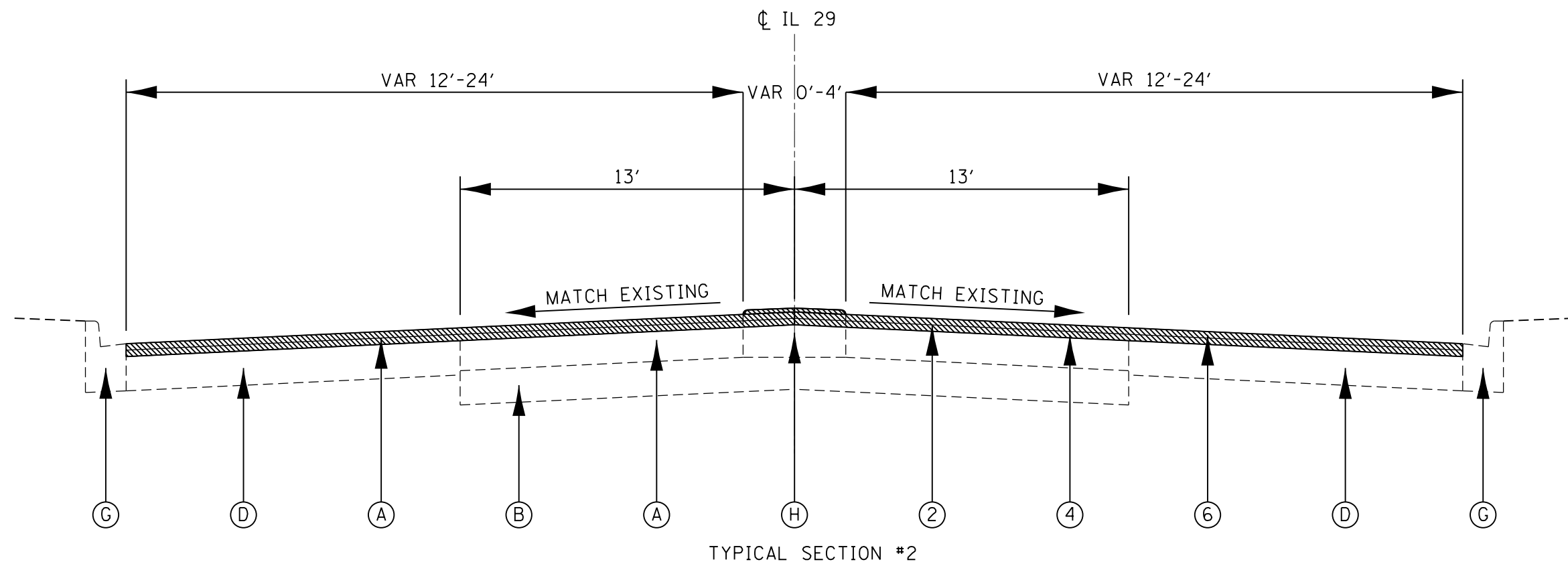
PROPOSED ITEMS

- ① HMA SURFACE REMOVAL, 3.25"
- ② HMA SURFACE REMOVAL, 3.75"
- ③ POLY HMA BINDER CSE
IL-9.5, N70 1.5"
- ④ POLY HMA BINDER CSE
IL-9.5, N70 2"
- ⑤ POLY HMA SURF CSE
IL-9.5, MIX "E", N70 1.5"
- ⑥ POLY HMA SURF CSE
IL-9.5, MIX "E", N70 1.75"
- ⑦ HMA SHLDR 3.75"



TYPICAL SECTION #1

STA. 124+75 TO STA. 140+16
 STA. EQUATION: STA. 140+16.09(BK.) = STA. 1551+99.79(AH)
 STA. 1551+99.79 TO 1564+02



TYPICAL SECTION #2

STA. 1564+02 TO STA. 1567+99.52
 STA. EQUATION: STA. 1567+99.52(BK.) = STA. 568+05.41(AH.)
 STA. 568+05.41 TO STA. 572+05.60
 STA. EQUATION: STA. 572+05.60(BK.) = STA. 36+19.18(AH.)
 STA. 36+19.18 TO STA. 37+45.31
 STA. EQUATION: STA. 37+45.31(BK.) = STA. 34+93.05(AH.)

STA. 34+93.05 TO STA. 38+84.07
 STA. EQUATION: 38+84.07(BK.) = STA. 31+02.03(AH.)
 STA. 31+02.03 TO STA. 38+67.89
 STA. EQUATION: STA. 38+67.89(BK.) = STA. 23+36.17(AH.)
 STA. 23+36.17 TO STA. 24+86

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PROPOSED TYPICAL SECTIONS

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8:(6,5)RS-3	PEORIA	48	10
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

ENGINEER'S FIELD OFFICE, TYPE A	
LOCATION	CAL MO
JOBSITE	5
TOTAL	5

MOBILIZATION	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

TRAFFIC CONTROL AND PROTECTION, STD 701606	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

TRAFFIC CONTROL AND PROTECTION, STD 701701	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

TRAFFIC CONTROL AND PROTECTION, STD 701801	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

CHANGEABLE MESSAGE SIGN	
LOCATION	CA DA
2 SIGNS @ 11 DAYS EACH	22
TOTAL	22

TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

CONSTRUCTION LAYOUT	
LOCATION	L SUM
JOBSITE	1
TOTAL	1

DRAINAGE STRUCTURE TO BE CLEANED	
LOCATION	EACH
STA 31+24, OFFSET LT 72 FT	1
TOTAL	1

INLETS TO BE ADJUSTED	
LOCATION	EACH
NW MOFFITT ST.	1
SE MOFFITT ST.	1
TOTAL	2

RESURFACING TABLE																			
LOCATION	WIDTH	LENGTH	HMA AREA	HMA SURF REM	TEMP RAMP	HMA SURF REM	HMA SURF REM	BIT MATLS (TK CT)	POLY HMA		POLY HMA		INCIDENT HMA SURF	MATERIAL TRANSFER DEVICE	HMA SHLDR	AGG SHLDR			
									BNDR CSE		SURF CSE								
									IL-9.5 N70	IL-9.5 MIX E N70	1.5"	1.75"							
			FT	FT	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	POUND	TON	TON	TON	TON	TON	TON	TON		
ROADWAY																			
124+75	TO	124+85	24	10.00	26.7	26.7	17.8	0.0	0.0	38.4	0.0	3.0	0.0	2.7	0	5.7	0	0	
124+85	TO	140+16.09	24	1531.09	4082.9	0	0	0	4082.9	5,879.4	0	457.3	0	400.2	0	857.5	0	0	
140+16.09 BK = 1551+99.79 AH																			
1551+99.79	TO	1564+02	24	1202.21	3205.9	0	0	0	3205.9	4,616.5	0	359.1	0	314.2	0	673.3	0	0	
1564+02	TO	1567+08	VAR	306.00	1028.3	0	0	1028.3	0	1,480.8	86.4	0	86.4	0.0	0	172.8	0	0	
1567+08	TO	1567+99.52	52	91.52	528.8	0	0	528.8	0	761.5	44.5	0	44.5	0.0	0	89.0	0	0	
1567+99.52 BK = 568+05.41 AH																			
568+05.41	TO	572+05.60	52	400.19	2312.2	0	0	2312.2	0	3,329.6	194.3	0	194.3	0.0	0	388.6	0	0	
572+05.60 BK = 36+19.18 AH																			
36+19.18	TO	37+45.31	52	126.13	728.8	0	0	728.8	0	1,049.5	61.3	0	61.3	0.0	0	122.6	0	0	
37+45.31 BK = 34+93.05 AH																			
34+93.05	TO	38+84.07	52	391.02	2259.2	0	0	2259.2	0	3,253.2	189.8	0	189.8	0.0	0	379.6	0	0	
38+84.07 BK = 31+02.03																			
31+02.03	TO	38+67.89	52	765.86	4425	0	0	4425	0	6,372.0	371.7	0	371.7	0.0	0	743.4	11	0	
38+67.89 BK = 23+36.17																			
23+36.17	TO	24+76	52	139.83	807.9	0	0	807.9	0	1,163.4	67.9	0	67.9	0.0	0	135.8	0	0	
24+76	TO	24+86	52	10.00	57.8	57.8	38.6	0	0	83.2	4.9	0	4.9	0.0	0	9.8	0	0	
MILL WIDE PAVE WIDE (3.75")			0.5	3049.30	169.4	0	0	0	169.4	243.9	0.0	19.0	0.0	16.7	0	35.7	0	0	
MILL WIDE PAVE WIDE (3")			1	1924.55	213.8	0	0	0	213.8	307.9	18.0	0	18.0	0.0	0	36.0	0	0	
MILL WIDE PAVE WIDE (SHLDRS)			0.5	3187.60	177.1	0	0	0	177.1	255.0	0.0	19.9	0.0	17.4	0	37.3	0	0	
LT SHOULDERS																			
124+75	TO	124+85	8	10.00	8.9	8.9	6	0	8.9	12.8	0	0	0	0	0	0.0	1.9	0	
124+85	TO	138+35	8	1350.00	1200	0	0	0	1200	1,728.0	0	0	0	0	0	0.0	252.0	0	
138+35	TO	140+16.09	VAR	181.09	87	0	0	0	87	125.3	0	0	0	0	0	0.0	18.3	0	
1551+99.79	TO	1552+18	VAR	18.21	35.7	0	0	0	35.7	51.4	0	0	0	0	0	0.0	7.5	0	
1564+09	TO	1565+18	0/4*	109.00	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0	8.3	
RT SHOULDERS																			
124+75	TO	124+85	8	10.00	8.9	8.9	6	0	8.9	12.8	0	0	0	0	0	0.0	8.9	0	
124+85	TO	126+16	8	131.00	116.4	0	0	0	116.4	167.6	0	0	0	0	0	0.0	116.4	0	
126+16	TO	128+76	3/5*	260.00	86.7	0	0	0	86.7	124.8	0	0	0	0	0	0.0	86.7	24.7	
128+76	TO	138+71	8	995.00	884.4	0	0	0	884.4	1,273.5	0	0	0	0	0	0.0	884.4	0	
138+71	TO	140+16.09	VAR	145.09	83.7	0	0	0	83.7	120.5	0	0	0	0	0	0.0	83.7	0	
1551+99.79	TO	1552+87	VAR	87.21	11.6	0	0	0	11.6	16.7	0	0	0	0	0	0.0	11.6	0	
1564+80	TO	1565+72	0/4*	92.00	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0	7	
SIDE ROADS																			
GAIL ST.			VAR	30	111.9	111.9	15.6	0	0	80.6	0	0	0	0	12.6	0.0	0	5	
WILMOT ST.			VAR	38	234.4	234.4	36.4	0	0	168.8	0	0	0	0	26.3	0.0	0	0	
MOFFIT ST. (WEST)			VAR	25	108.6	108.6	22.3	0	0	78.2	0	0	0	0	12.2	0.0	0	0	
MOFFIT ST. (EAST)**			0	0	0	0	0	0	0	0.0	0	0	0	0	0.0	0.0	0	0	
MCDOWELL ST.			VAR	20	71.8	71.8	17.8	0	0	51.7	0	0	0	0	8.1	0.0	0	0	
ALLEY			12	10	13.3	13.3	8.9	0	0	9.6	0	0	0	0	1.5	0.0	0	0	
HILDA CT.			VAR	35	123.6	123.6	19.3	0	0	89.0	0	0	0	0	13.9	0.0	0	0	
SUBTOTALS											1,038.8	858.3	1,038.8	751.2					
GRAND TOTALS						765.9	188.7	12,304.0	10,158.6	32,945.6	1,897.1	1,790.0	74.6	3,687.1	1,482.4	45.0			
GRAND TOTALS (ROUNDED)						766	189	12304	10159	32946	1898	1790	75	3688	1483	45			

* HMA / AGG ** MOFFIT ST. EAST IS CONCRETE - NO HMA WORK TO BE DONE

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PLOT DATE = 12/18/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;(6,5)RS-3	PEORIA	48	11
CONTRACT NO. 68F12				
ILLINOIS		FED. AID PROJECT		

ADA SIDEWALK CONSTRUCTION									
LOCATION		PORTLAND CEMENT CONCRETE SIDEWALK 4"	DETECTABLE WARNINGS	COMBINATION CURB AND GUTTER REMOVAL	SIDEWALK REMOVAL	COMBINATION CONC. CURB AND GUTTER B-6.24	PCC BASE COURSE 13"	PCC DRIVEWAY PAV'T 7 IN	PAVEMENT REMOVAL
		SQ FT	SQ FT	FOOT	SQ FT	FOOT	SQ YD	SQ YD	SQ YD
WILMOT ST.	NW CORNER	134.5	10	9	48	9	1	0	1
WILMOT ST.	SW CORNER	141.1	10	42	41	31	3.4	0	3.4
MOFFITT ST.	NW CORNER	134.6	10	25	28	25	2.8	0	2.8
MOFFITT ST.	SW CORNER	73.7	10	17.6	67	17.6	2	0	2
MOFFITT ST.	NE CORNER	33.2	10	6	0	6	0.7	0	0.7
MOFFITT ST.	SE CORNER	200.6	10	43.2	168	43.2	4.8	0	4.8
MCDOWELL ST.	NE CORNER	81.5	13.1	20	72	20	2.2	0	2.2
MCDOWELL ST.	SE CORNER	64.5	13.1	20	72	20	2.2	0	2.2
ALLEY	35+99 RT	321.7	20	40	246	31	6.3	51.9	6.3
HILDA CT.	NE CORNER	83	10	16	59	16	1.8	0	1.8
HILDA CT.	SE CORNER	83	10	16	61	16	1.8	0	1.8
TOTALS		1351.4	126.2	254.8	862	234.8	29	51.9	29
ROUNDED TOTALS		1351.5	126.5	255	862	235	29	52	29

GUARDRAIL TABLE										
LOCATION				GUARDRAIL REMOVAL	SPBGR TYPE A 6 FOOT POSTS	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT EACH	TRAFFIC BARRIER TERMINAL TYPE 6 EACH	TERMINAL MARKER DIRECT APPLIED EACH	GUARDRAIL REFLECTORS TYPE A EACH	GUARDRAIL AGGREGATE EROSION CONTROL TON
124+95	TO	126+70	LT	204	87.5	1	1	1	5	39.7
124+55	TO	125+55	RT	128	12.5	1	1	1	2	22.7
TOTALS				332	100	2	2	2	7	63

PAVEMENT MARKINGS										
LOCATION		MODIFIED URETHANE PVT MRKNG				GROOVING FOR RECESSED PVT MRKNG	SHORT TERM PVT MRKNG	SHORT TERM PVT MRKNG	RAISED REFL PVT	RAISED REFLECT PVT MRKR
		YELLOW		WHITE						
		SKIP 4"	SOLID 4"	SKIP 4"	SOLID 4"					
		FOOT	FOOT	FOOT	FOOT					
N End of project to 135+18 2 edge lines, 1 skip - dash		253	0	0	2020	2273	205	68.3	13	13
135+18 to 1567+00 2 edge lines, 1 double yellow line		0	3268	0	3396	6664	187	62.3	164	164
1567+00 to S end of job 2 - edge lines, 2 skip-dash, 2 - double yellow lines		0	6276	766	3308	10350	620.2	206.7	79	79
Side Roads 2 - edge lines		0	0	0	408	408	0	0	0	0
SUBTOTALS		253	9544	766	9132		1012.2	337.3		
GRAND TOTALS					19695	19695	1013	338	256	256

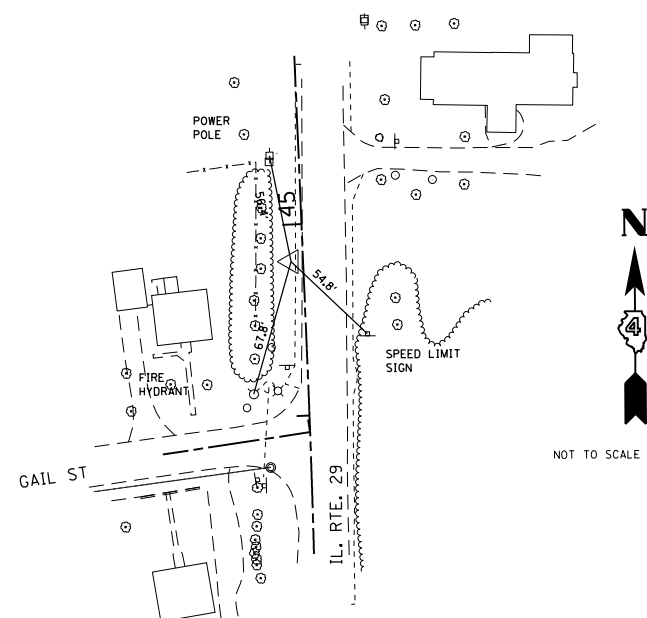
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PLOT DATE = 12/11/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

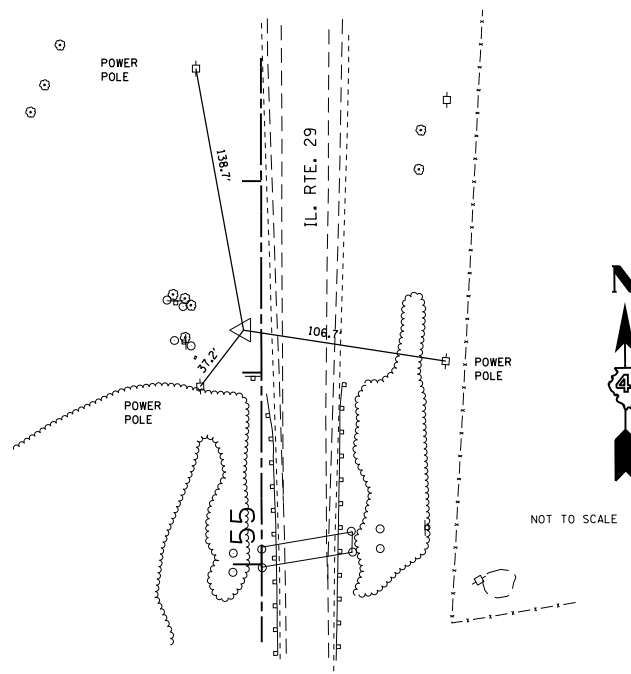
SCHEDULE OF QUANTITIES			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;(6,5)RS-3	PEORIA	48	12
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				



CONTROL POINT #203

REBAR
STATION 44+80.64
N. 1,553,199.0697
E. 2,484,689.3200

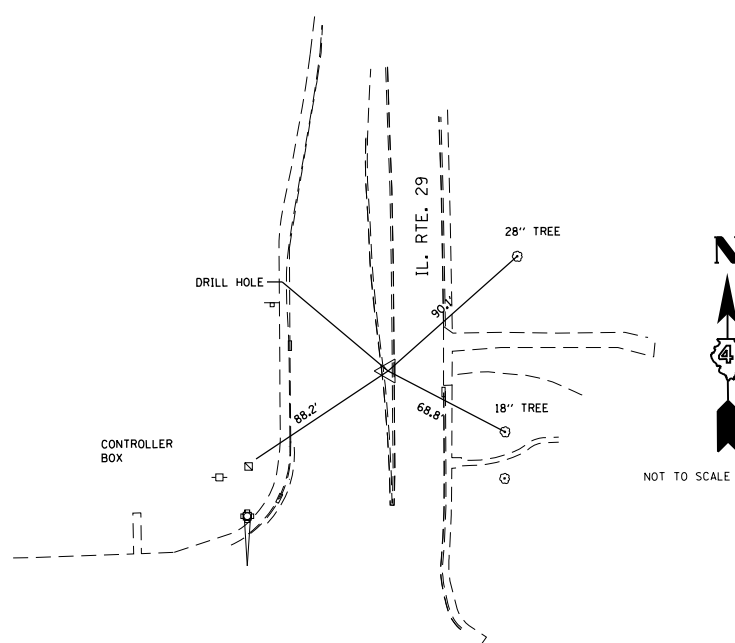


CONTROL POINT #109

REBAR
STATION 56+22.21
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E. 2,484,670.2594

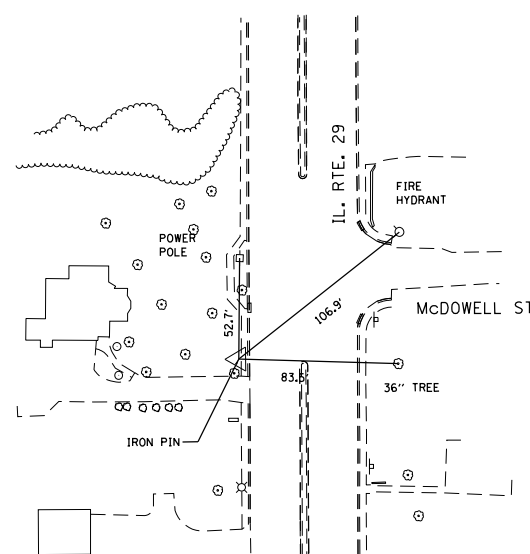
- BM CB-1 SET CHISELED "X" ON NORTH BOLT OF FIRE HYDRANT LOCATED AT S.E. INTERSECTION OF IL. RTE. 29 AND HILDA CT. 38' RT. STA. 23+10 ELEV. 503.44
- BM CB-2 FOUND CHISELED "X" ON NORTH BOLT OF FIRE HYDRANT LOCATED EAST SIDE IL. RTE. 29, APPROX. 100' SOUTH OF CHILICOTHE TOWNSHIP BUILDING. 40' RT. STA. 27+81 ELEV. 504.57
- BM CB-3 SET CHISELED "X" ON NORTH BOLT OF FIRE HYDRANT LOCATED AT S.E. INTERSECTION OF IL. RTE. 29 AND MOFFITT STREET. 48' RT. STA. 34+66 ELEV. 506.24
- BM CB-4 FOUND CHISELED "X" ON NORTH BOLT OF FIRE HYDRANT LOCATED AT N.W. INTERSECTION OF IL. RTE. 29 AND GAIL STREET. 28' LT. STA. 44+13 ELEV. 507.87
- BM CB-5 SET CHISELED SQUARE ON SOUTH SIDE OF CONCRETE BASE TO MANHOLE LID LOCATED ON WEST SIDE OF IL. RTE. 29 STRAIGHT ACROSS FROM ARC TERMINALS ENTRANCE. 31' LT. STA. 62+44 ELEV. 485.37

COORDINATES PRESENTED IN THESE PLANS ARE
GROUND COORDINATES (NAD83(1997))
GRID COORDINATES / 0.999969 = GROUND COORDINATES
VERTICAL DATUM NAVD 88



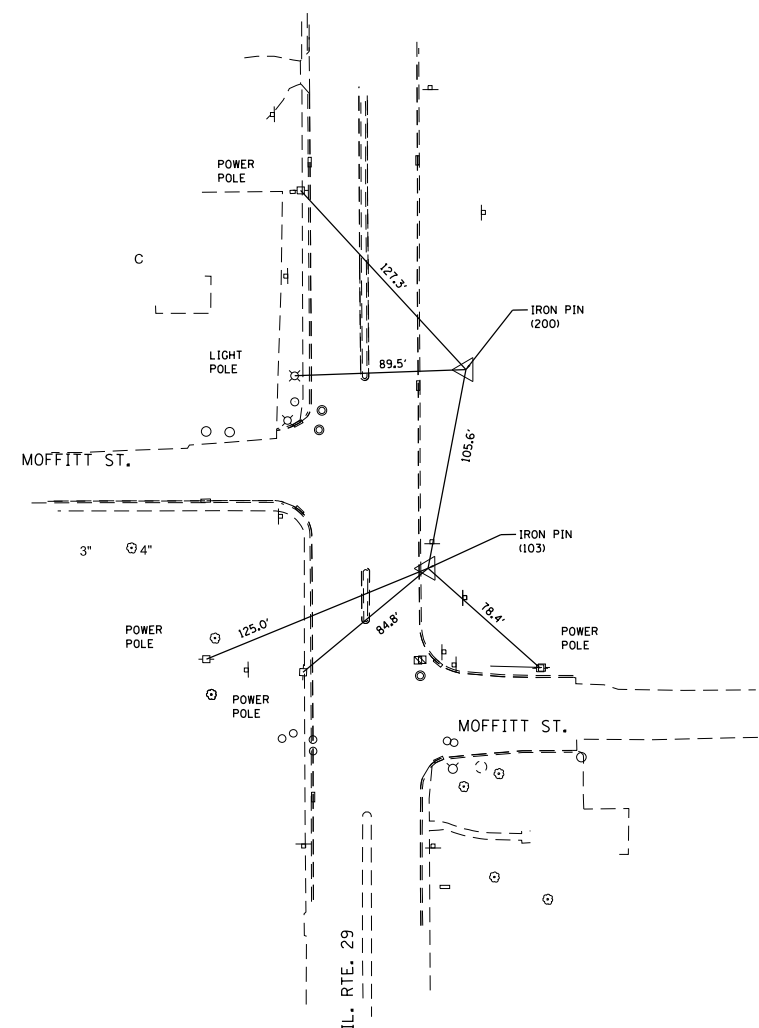
CONTROL POINT #102

DRILLED HOLE IN MEDIAN
STATION 20+73.59
N. 1,550,792.4700
E. 2,484,717.9269



CONTROL POINT #202

REBAR
STATION 30+56.96
N. 1,551,775.6406
E. 2,484,675.3336



CONTROL POINT #103 & 200

REBAR (103)
STATION 35+70.15
N. 1,552,289.1495
E. 2,484,739.4275

REBAR (200)
STATION 36+73.75
N. 1,552,392.8437
E. 2,484,759.2462

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TIES AND BENCHMARKS

NOT TO SCALE

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PLOT DATE = 12/11/2020	DATE -	REVISED -

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;(6,5)RS-3	PEORIA	48	13
CONTRACT NO. 68F12				
		ILLINOIS	FED. AID PROJECT	

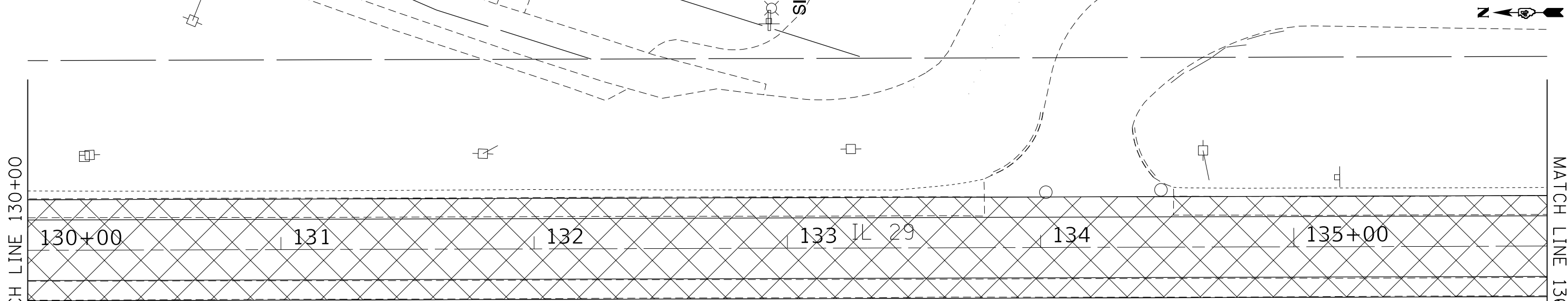
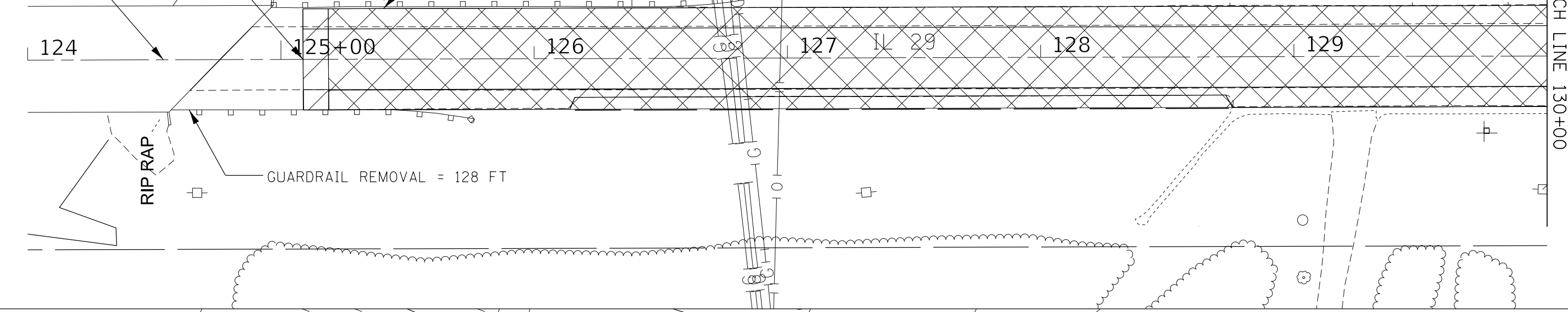


HMA SURF REM BEGINS
125+08

PROJECT BEGINS
124+55

 HMA SURF REM 3.75"

 HMA SURF REM BUTT JT



 HMA SURF REM 3.75"

RET WALL

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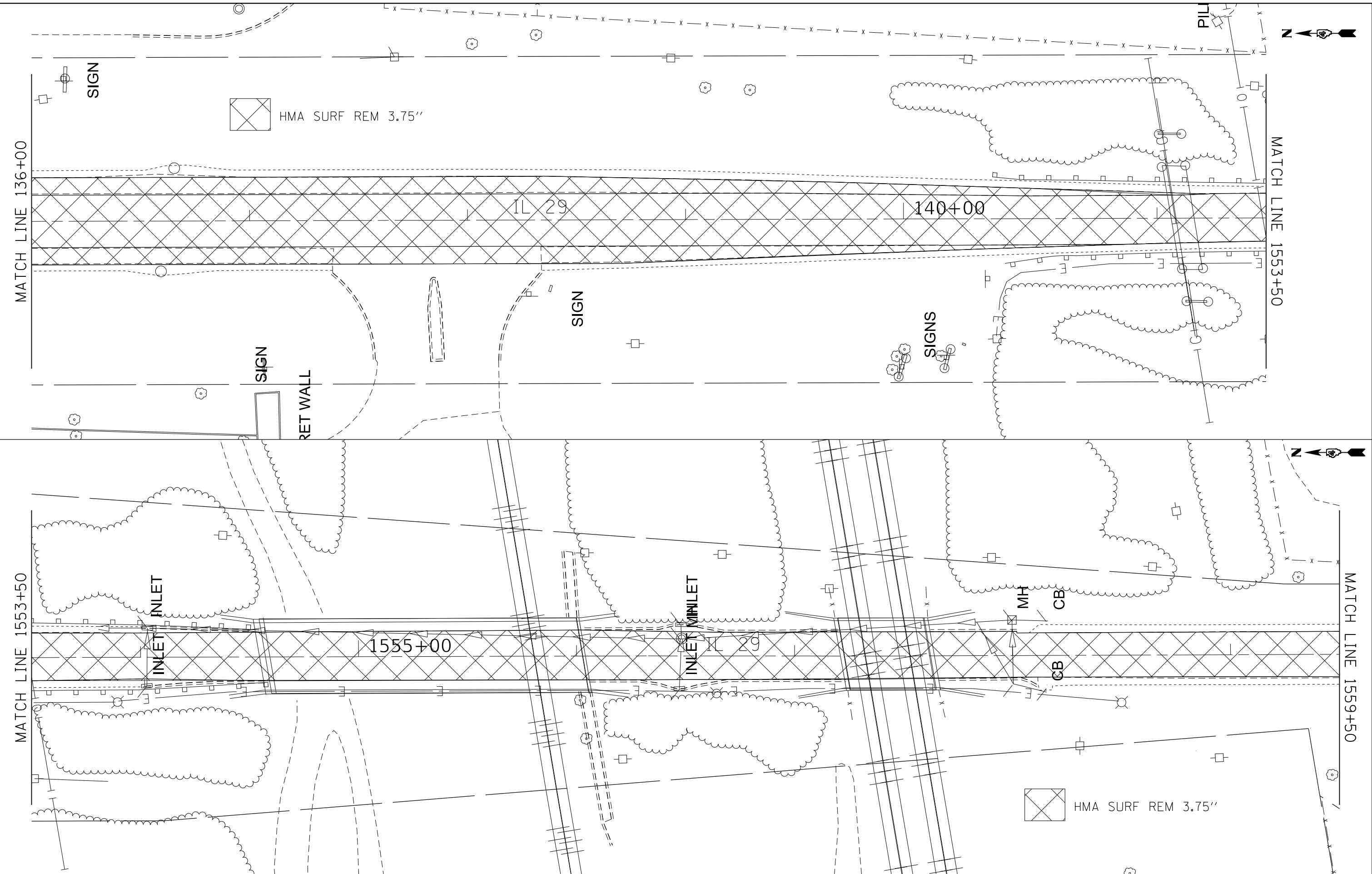
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PRELIMINARY REMOVAL PLAN				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-6;6RS-3	PEORIA	48	14
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

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PLOT DATE = 12/11/2020	DATE -	REVISED -

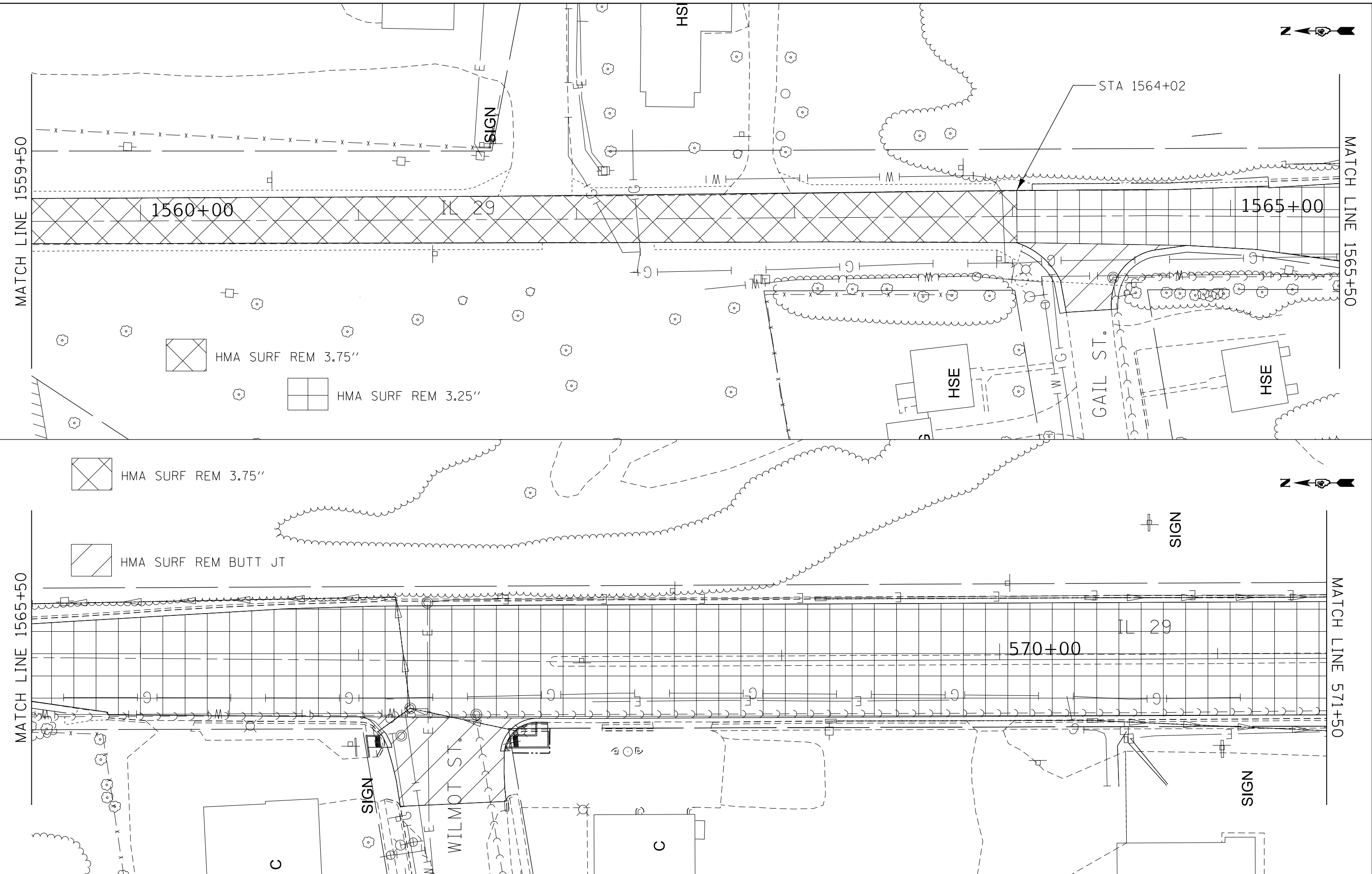
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PRELIMINARY
 REMOVAL PLAN**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-6:6RS-3	PEORIA	48	15
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

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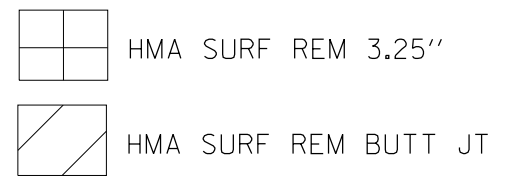
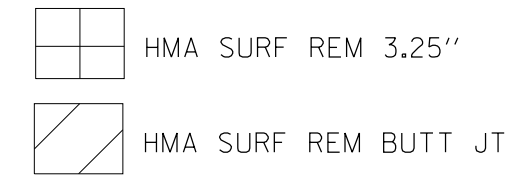
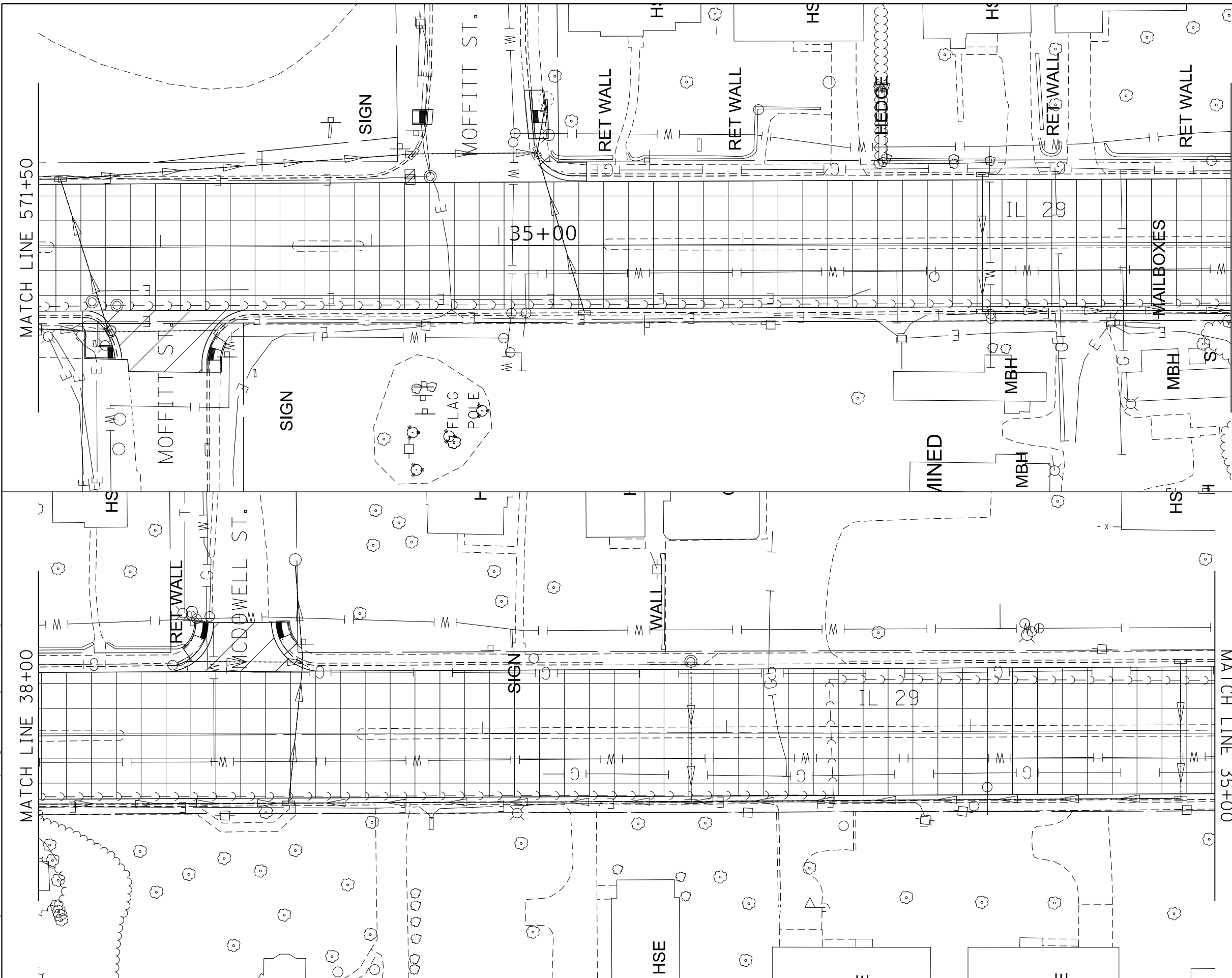
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PRELIMINARY
REMOVAL PLAN

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-6:6RS-3	PEORIA	48	16
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

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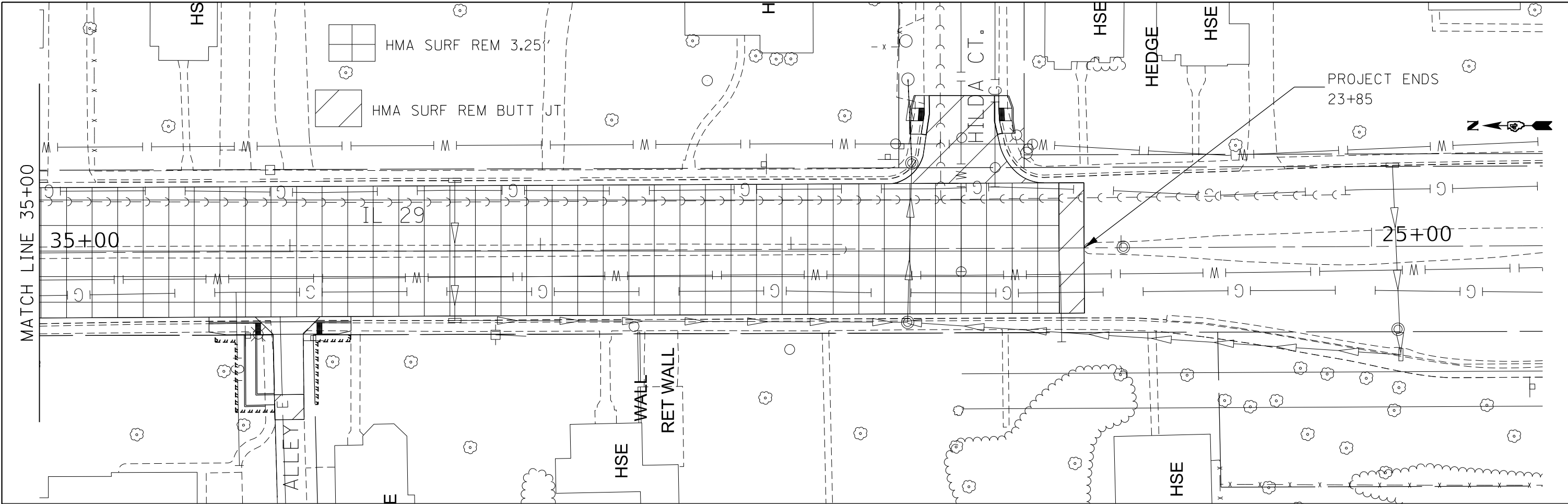


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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PRELIMINARY REMOVAL PLAN				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-6;6RS-3	PEORIA	48	17
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				



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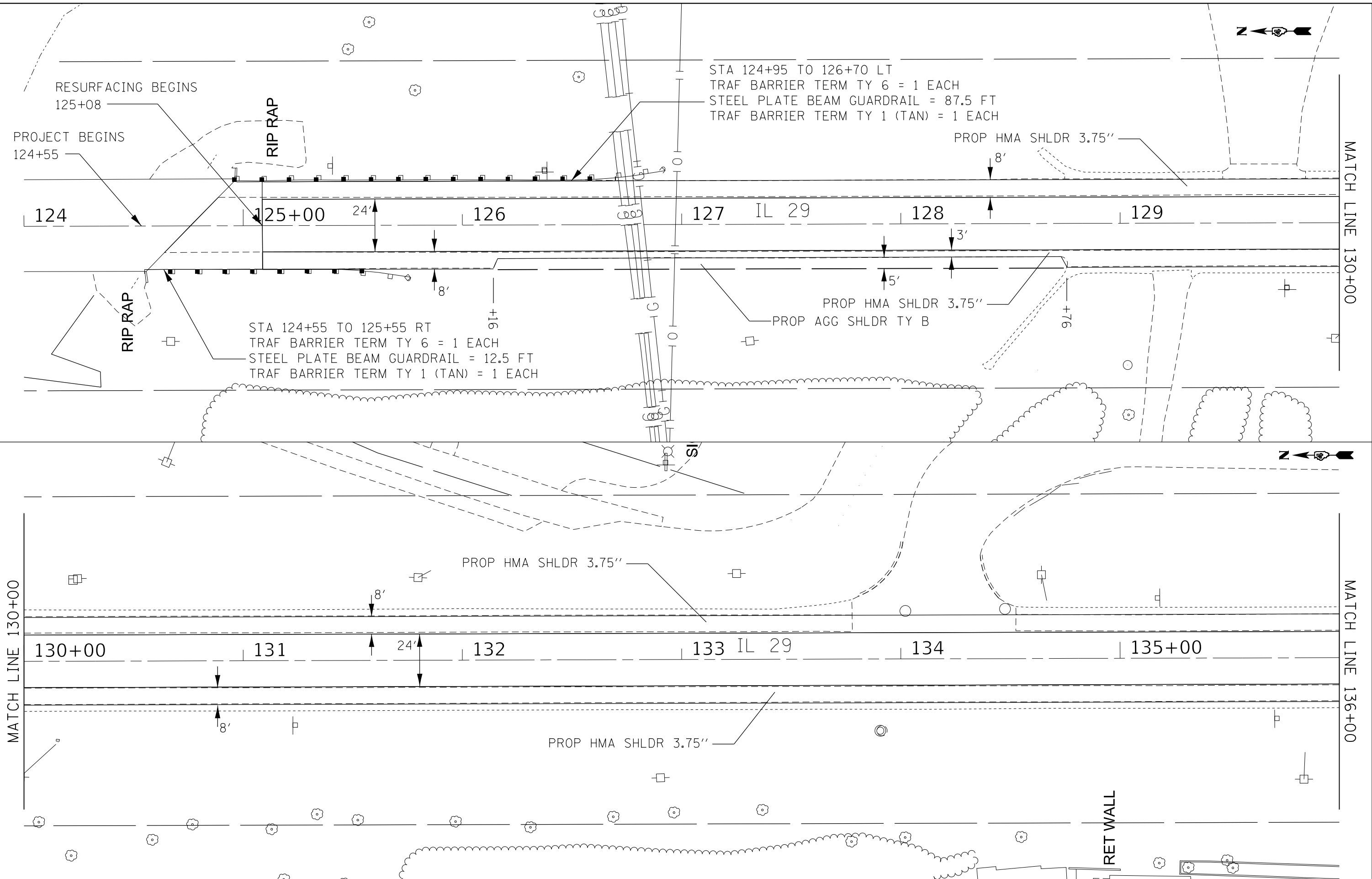
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PRELIMINARY REMOVAL PLAN				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-6;6RS-3	PEORIA	48	18
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

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STA 124+95 TO 126+70 LT
 TRAF BARRIER TERM TY 6 = 1 EACH
 STEEL PLATE BEAM GUARDRAIL = 87.5 FT
 TRAF BARRIER TERM TY 1 (TAN) = 1 EACH

STA 124+55 TO 125+55 RT
 TRAF BARRIER TERM TY 6 = 1 EACH
 STEEL PLATE BEAM GUARDRAIL = 12.5 FT
 TRAF BARRIER TERM TY 1 (TAN) = 1 EACH

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

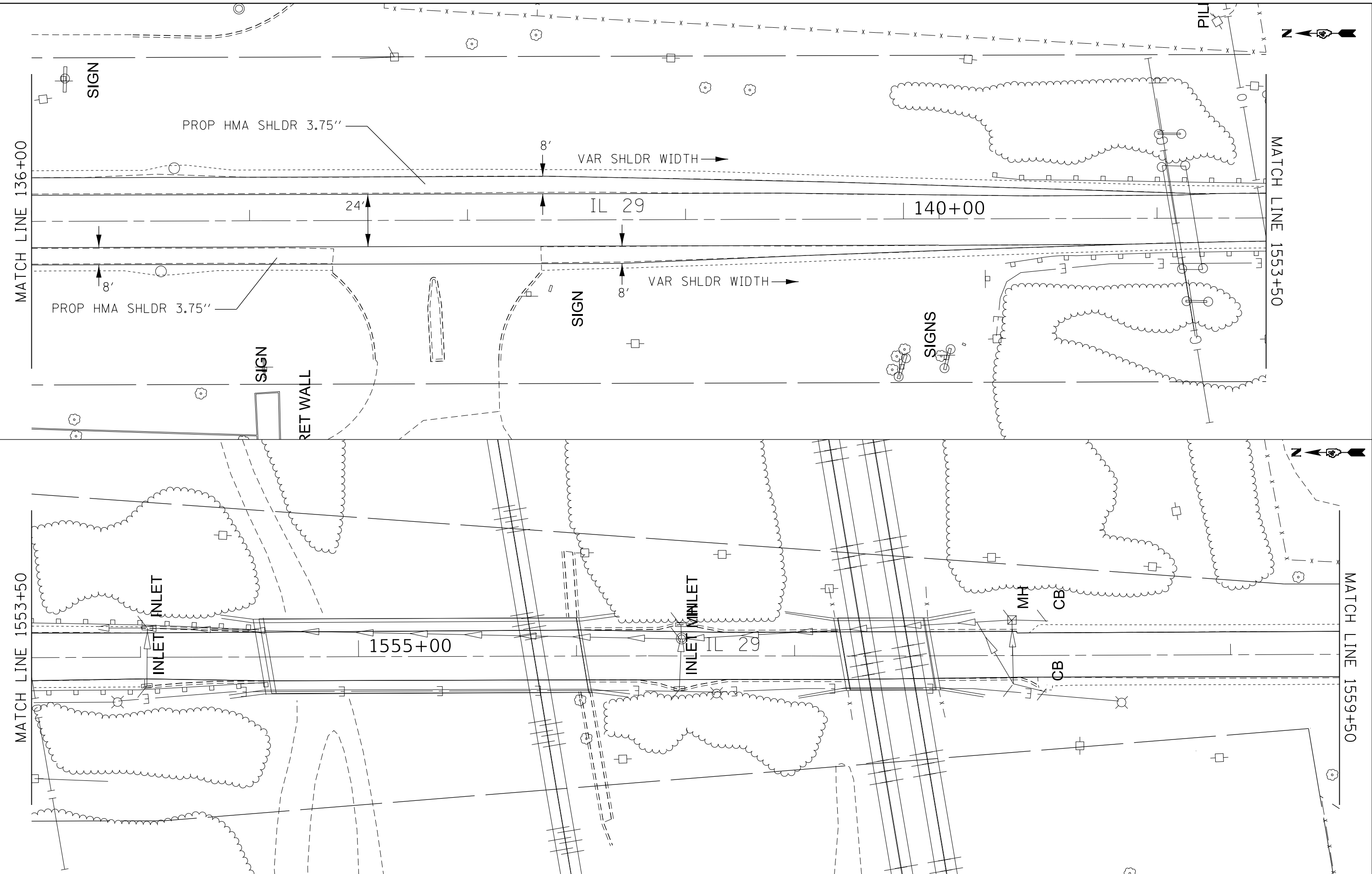
PRELIMINARY
 PROPOSED PLAN

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	DATE -	REVISED -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-6:6RS-3	PEORIA	48	19
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.

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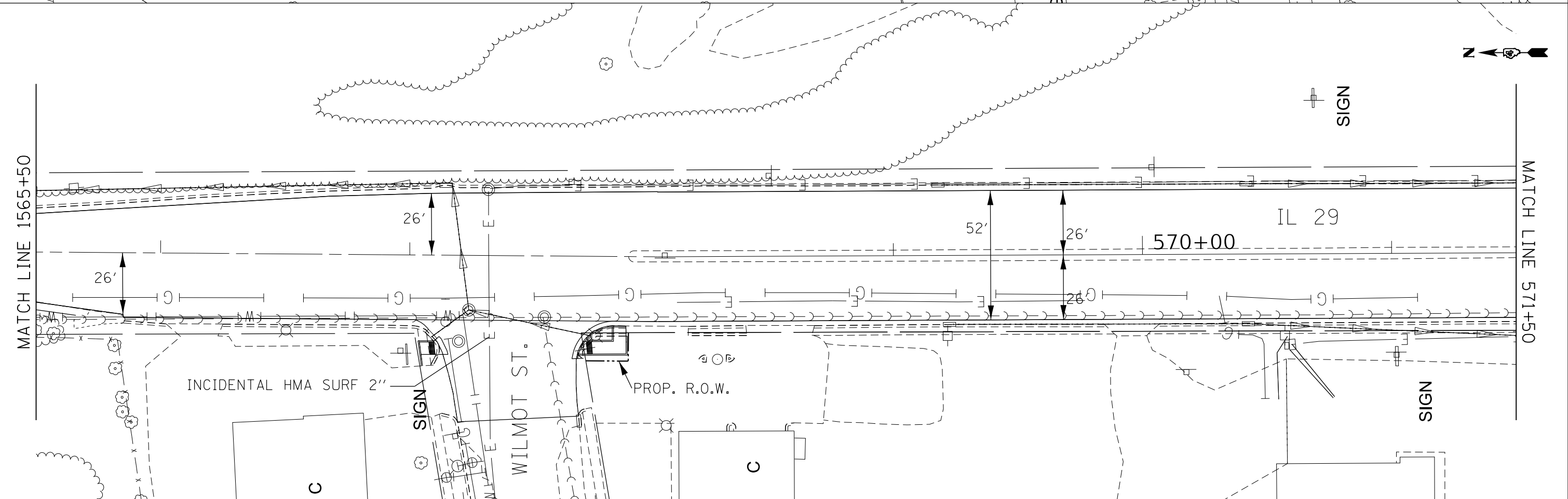
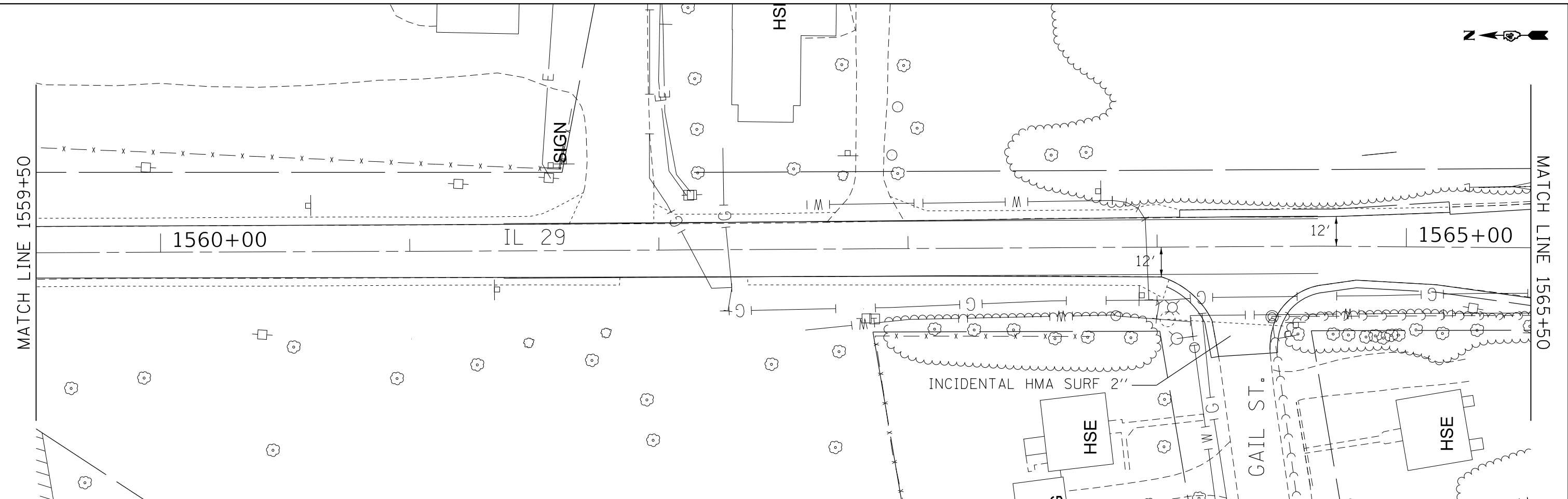
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PRELIMINARY
 PROPOSED PLAN**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-6:6RS-3	PEORIA	48	20
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

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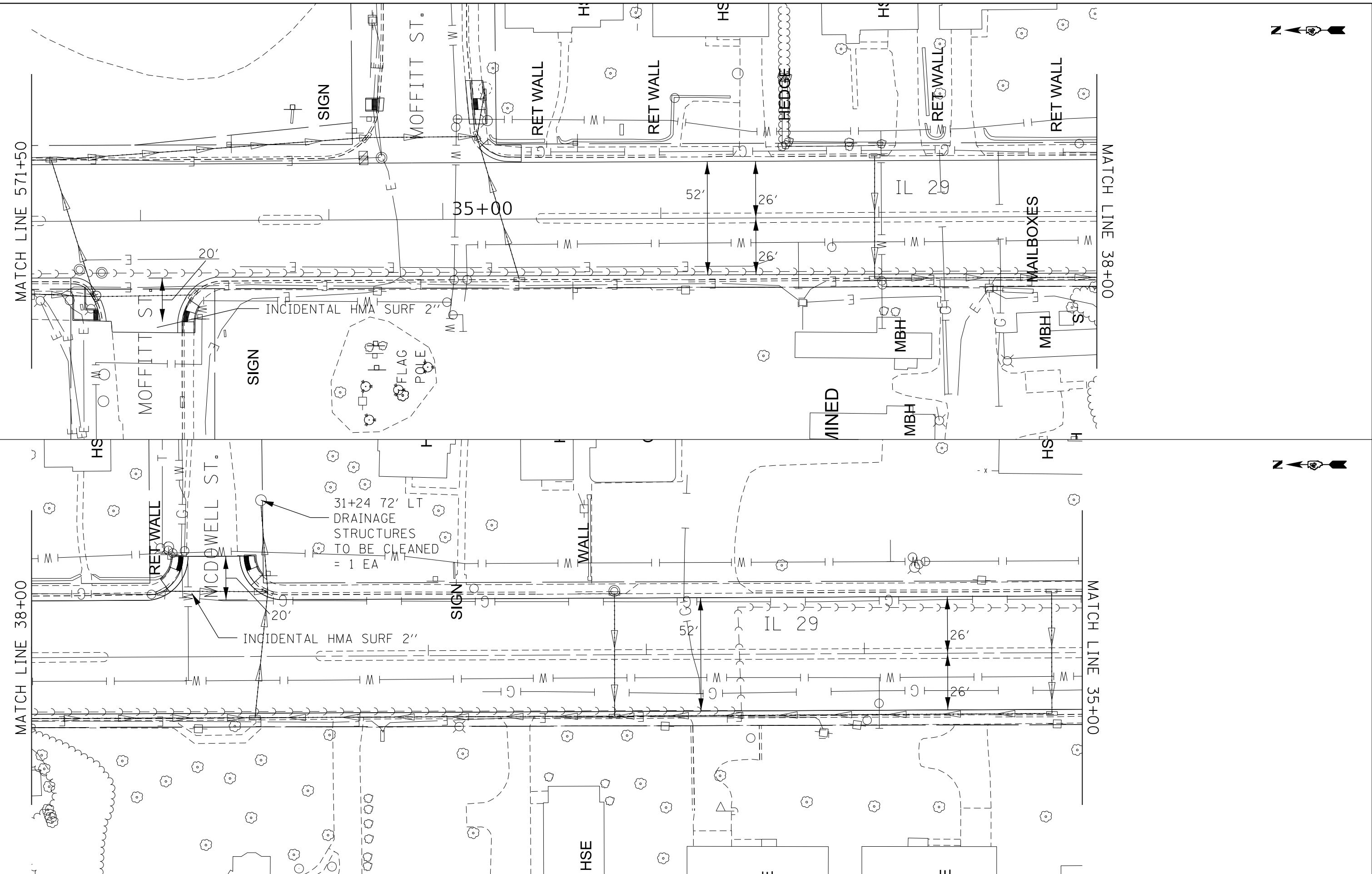
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PRELIMINARY
 PROPOSED PLAN**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-6:6RS-3	PEORIA	48	21
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

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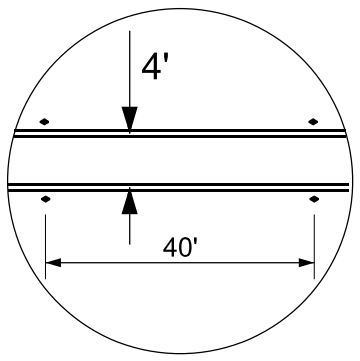
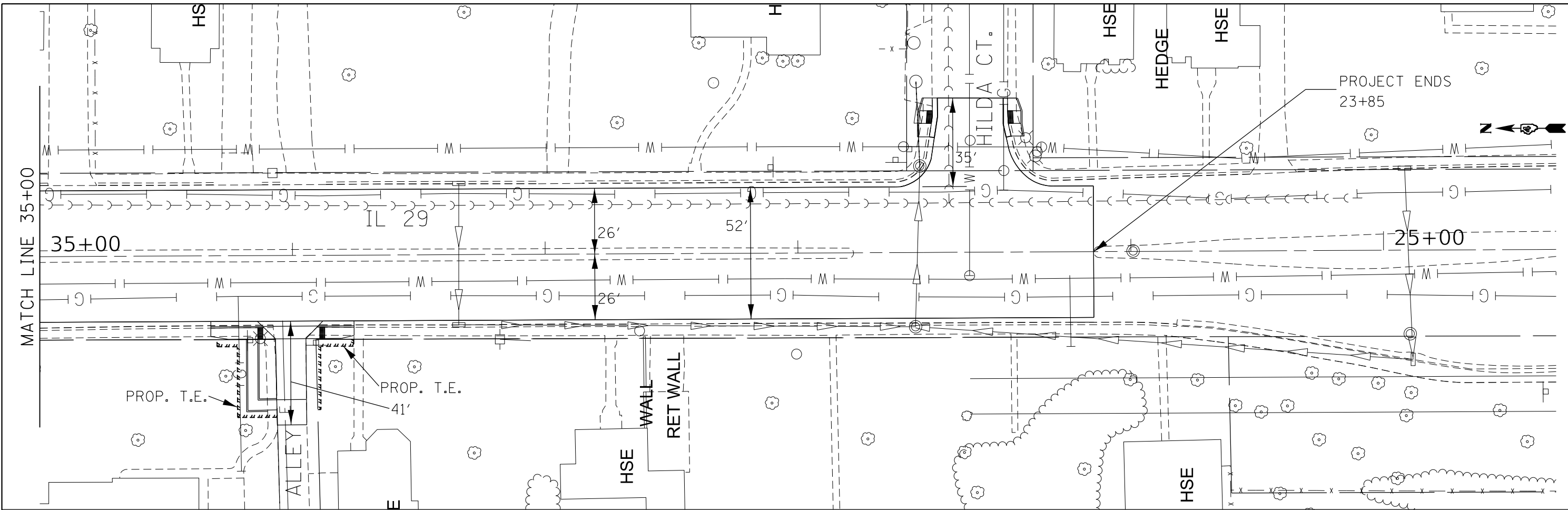
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PLOT DATE = 12/11/2020	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PRELIMINARY
 PROPOSED PLAN**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-6:6RS-3	PEORIA	48	22
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				



CENTERLINE PAVEMENT MARKING DETAIL
 1567+00 TO S. END OF JOB
 NOT TO SCALE

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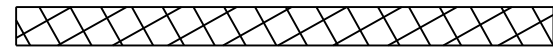
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PRELIMINARY PROPOSED PLAN				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-6;6RS-3	PEORIA	48	23
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

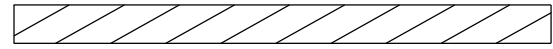
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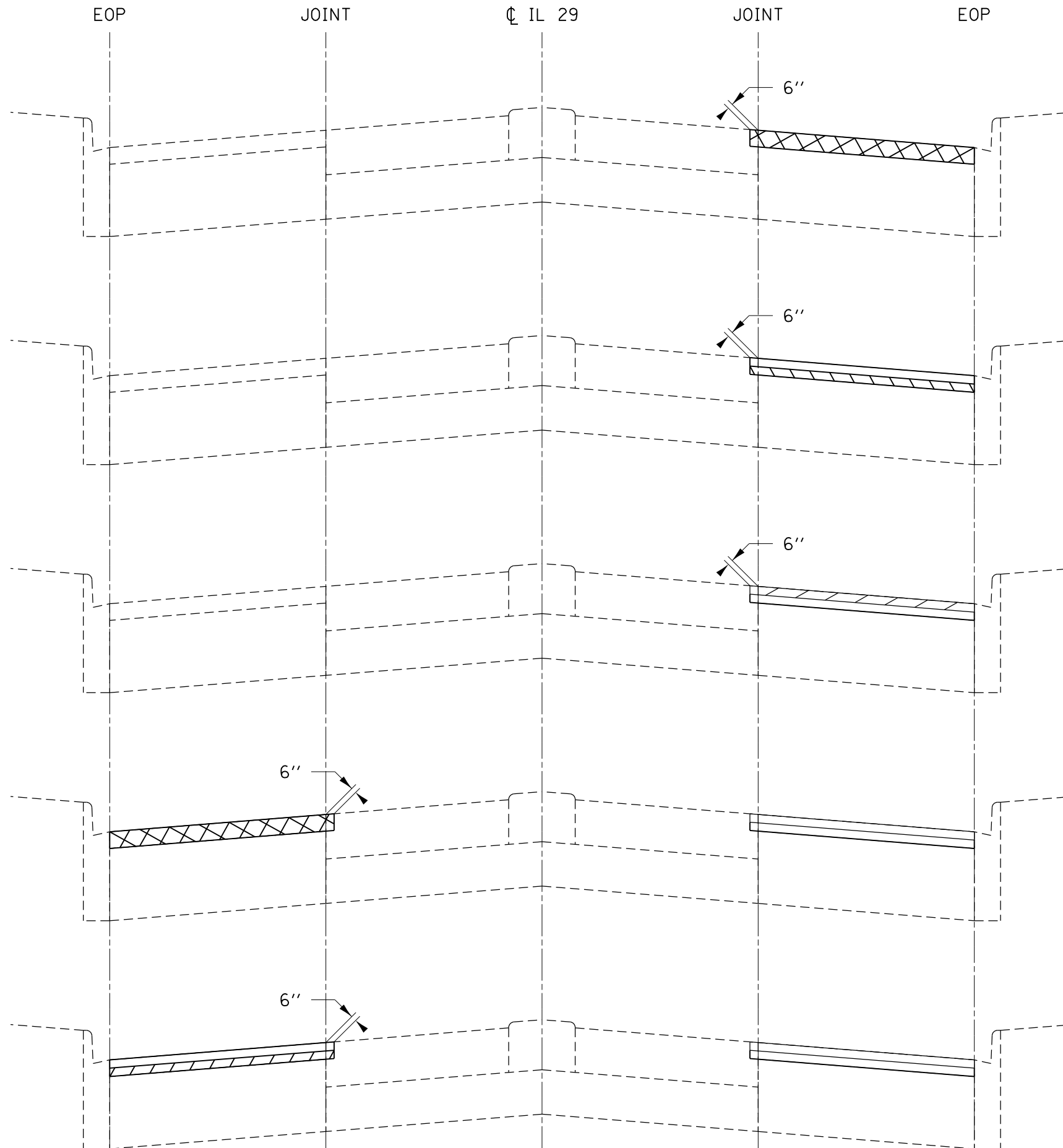
HMA SURFACE REMOVAL, 3.25"



POLY HMA BINDER COURSE
IL-9.5, N70, 1.5"



POLY HMA SURFACE COURSE
IL-9.5, MIX "E", N70, 1.5"



STAGE 1-A
HMA SURFACE REMOVAL ON THE NB LANES FROM THE EOP TO THE REFLECTIVE JOINT 13' FROM THE CENTERLINE, PLUS 6" PAST THE REFLECTIVE JOINT.

STAGE 1-B
HMA BINDER COURSE ON THE NB LANES FROM THE EOP TO THE REFLECTIVE JOINT 13' FROM THE CENTERLINE, PLUS 6" PAST THE REFLECTIVE JOINT.

STAGE 1-C
HMA SURFACE COURSE ON THE NB LANES FROM THE EOP TO THE REFLECTIVE JOINT 13' FROM THE CENTERLINE, PLUS 6" PAST THE REFLECTIVE JOINT.

STAGE 2-A
HMA SURFACE REMOVAL ON THE SB LANES FROM THE EOP TO THE REFLECTIVE JOINT 13' FROM THE CENTERLINE, PLUS 6" PAST THE REFLECTIVE JOINT.

STAGE 2-B
HMA BINDER COURSE ON THE SB LANES FROM THE EOP TO THE REFLECTIVE JOINT 13' FROM THE CENTERLINE, PLUS 6" PAST THE REFLECTIVE JOINT.

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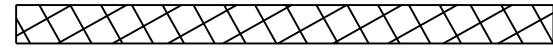
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION SEQUENCE

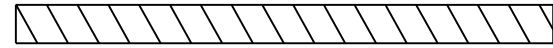
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;6.5)RS-3	PEORIA	48	24
				CONTRACT NO. 68F12
ILLINOIS FED. AID PROJECT				

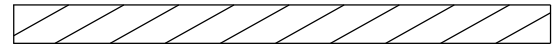
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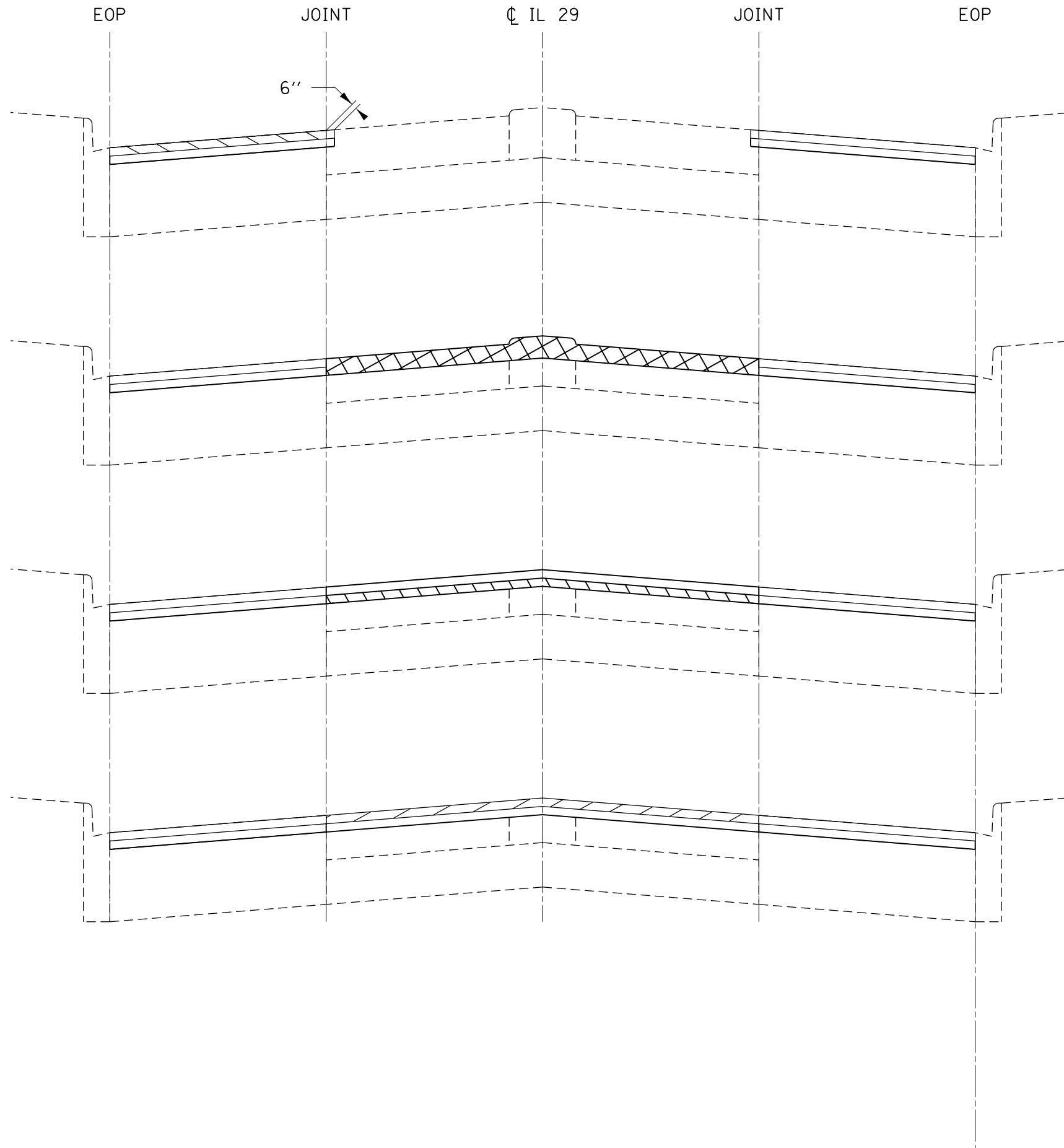
HMA SURFACE REMOVAL, 3.25"



POLY HMA BINDER COURSE
IL-9.5, N70, 1.5"



POLY HMA SURFACE COURSE
IL-9.5, MIX "E", N70, 1.5"



STAGE 2-C
HMA SURFACE COURSE ON THE SB
LANES FROM THE EOP TO THE
REFLECTIVE JOINT 13' FROM THE
CENTERLINE, PLUS 6" PAST THE
REFLECTIVE JOINT.

STAGE 3-A
HMA SURFACE REMOVAL ON THE
CENTER LANES, 13' ON EITHER SIDE
OF THE CENTERLINE, INCLUDING THE
RAISED MEDIAN.

STAGE 3-B
HMA BINDER COURSE ON THE
CENTER LANES, 13' ON EITHER SIDE
OF THE CENTERLINE, INCLUDING THE
RAISED MEDIAN.

STAGE 3-C
HMA SURFACE COURSE ON THE
CENTER LANES, 13' ON EITHER SIDE
OF THE CENTERLINE, INCLUDING THE
RAISED MEDIAN.

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PLOT DATE = 12/11/2020	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION SEQUENCE

SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;6.5)RS-3	PEORIA	48	25
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

IL 29 CLOSURE AND DETOUR



IL 29	FOLLOW
THRU	DETOUR
TRAFFIC	AHEAD

PLACE PORTABLE CHANGEABLE MESSAGE SIGN (PCMS) 1 MILE SOUTH OF IL 6 ON NB IL 29.

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 DATE: 12/11/2020

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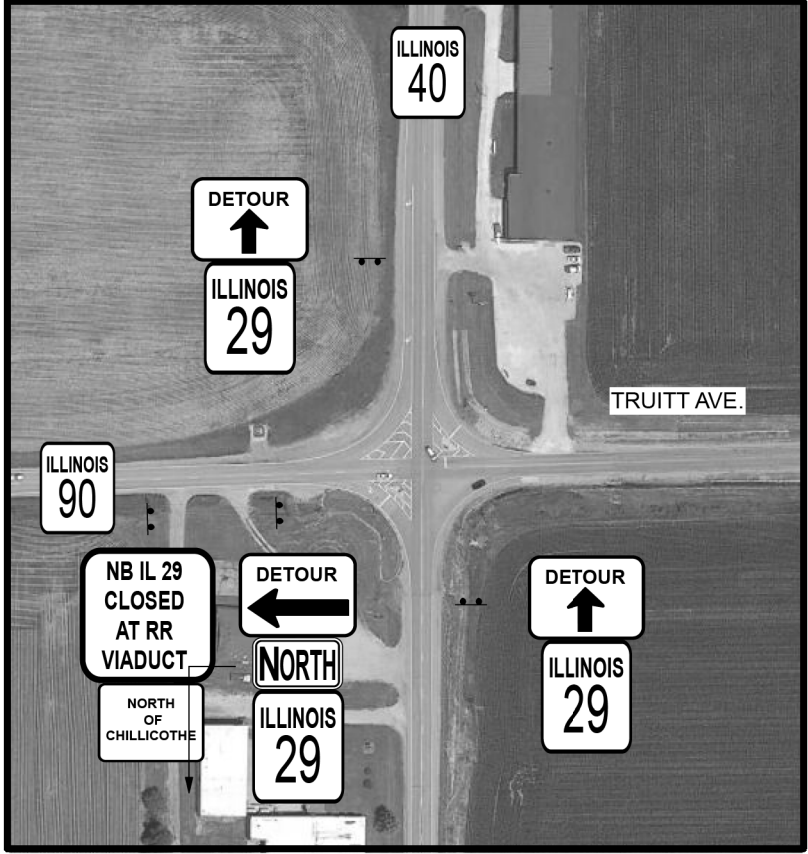
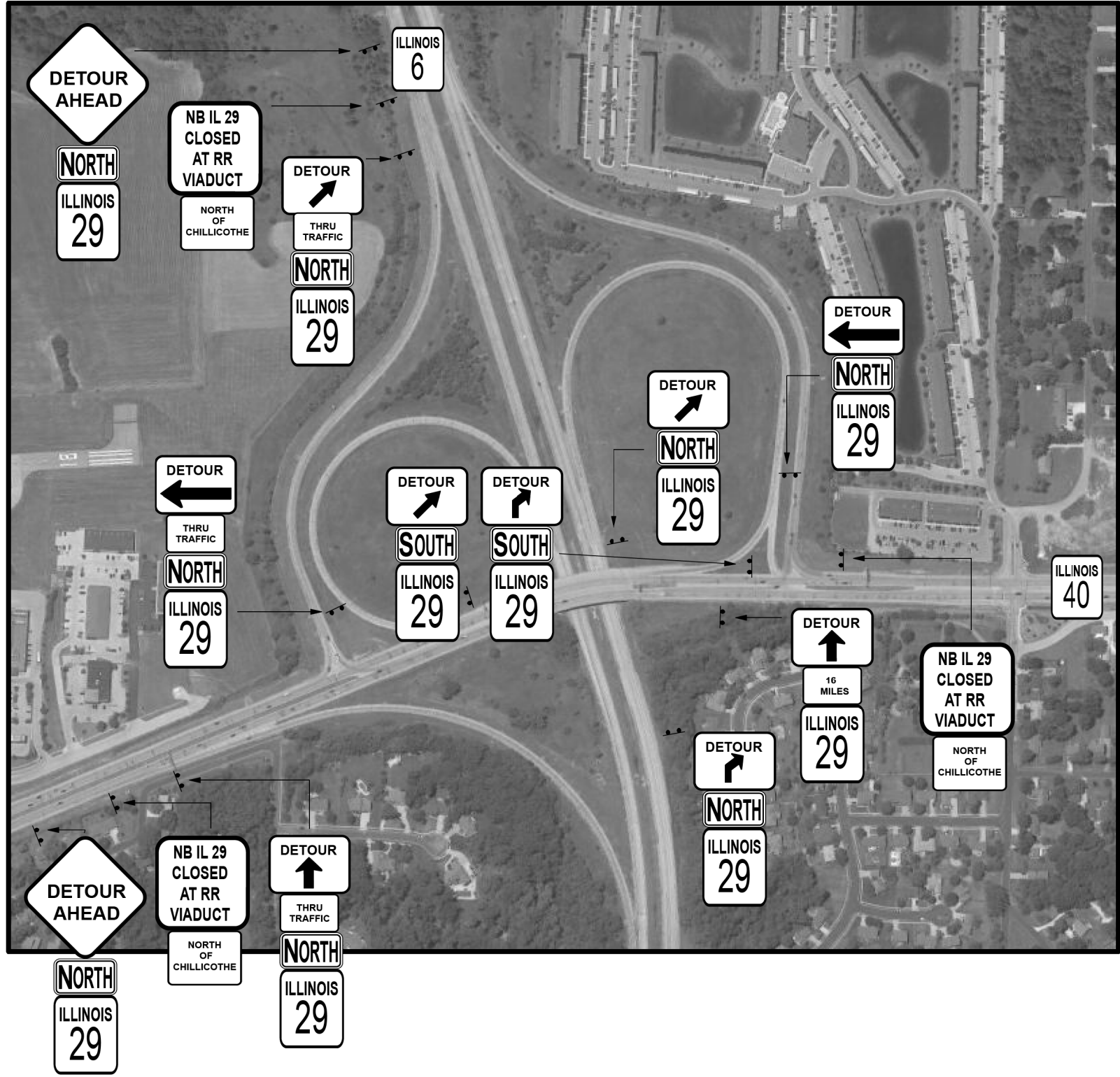
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETOUR PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;(6,5)RS-3	PEORIA	48	26
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

IL 29 CLOSURE AND DETOUR



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETOUR PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

IL 29 CLOSURE AND DETOUR

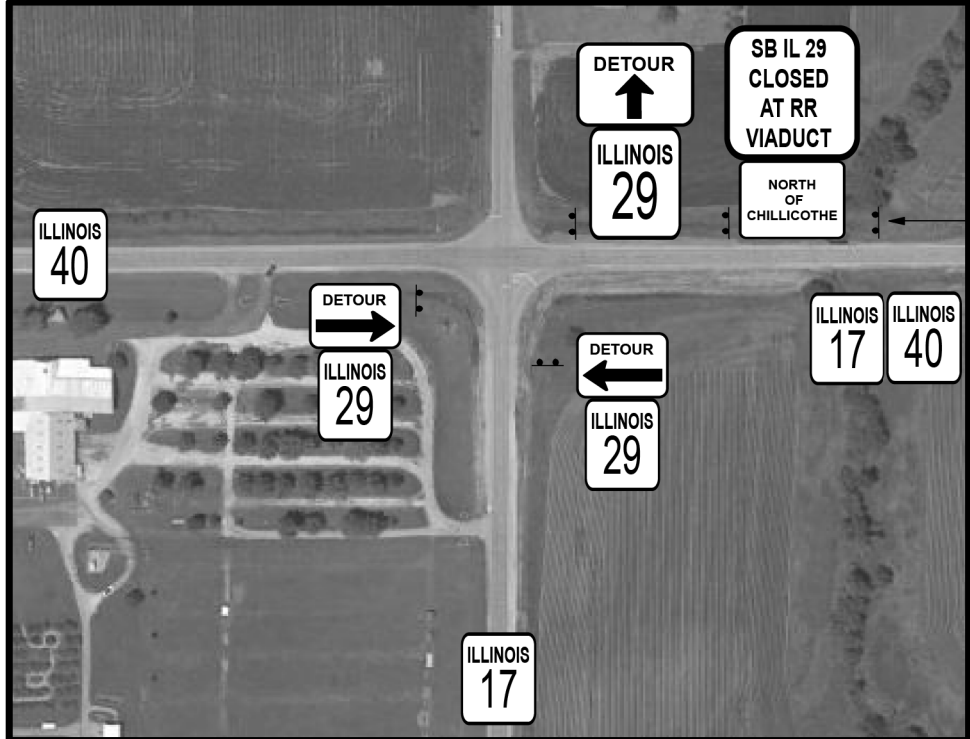
BLR 21-9 SHALL BE USED ON EACH APPROACH TO THE CLOSURE.

TWO (2) PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE PLACED SEVEN (7) CALENDAR DAYS IN ADVANCE OF THE CLOSURE. THE SAME TWO MESSAGE BOARDS SHALL BE USED FOR THE CLOSURE DURATION.

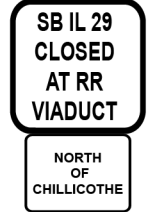
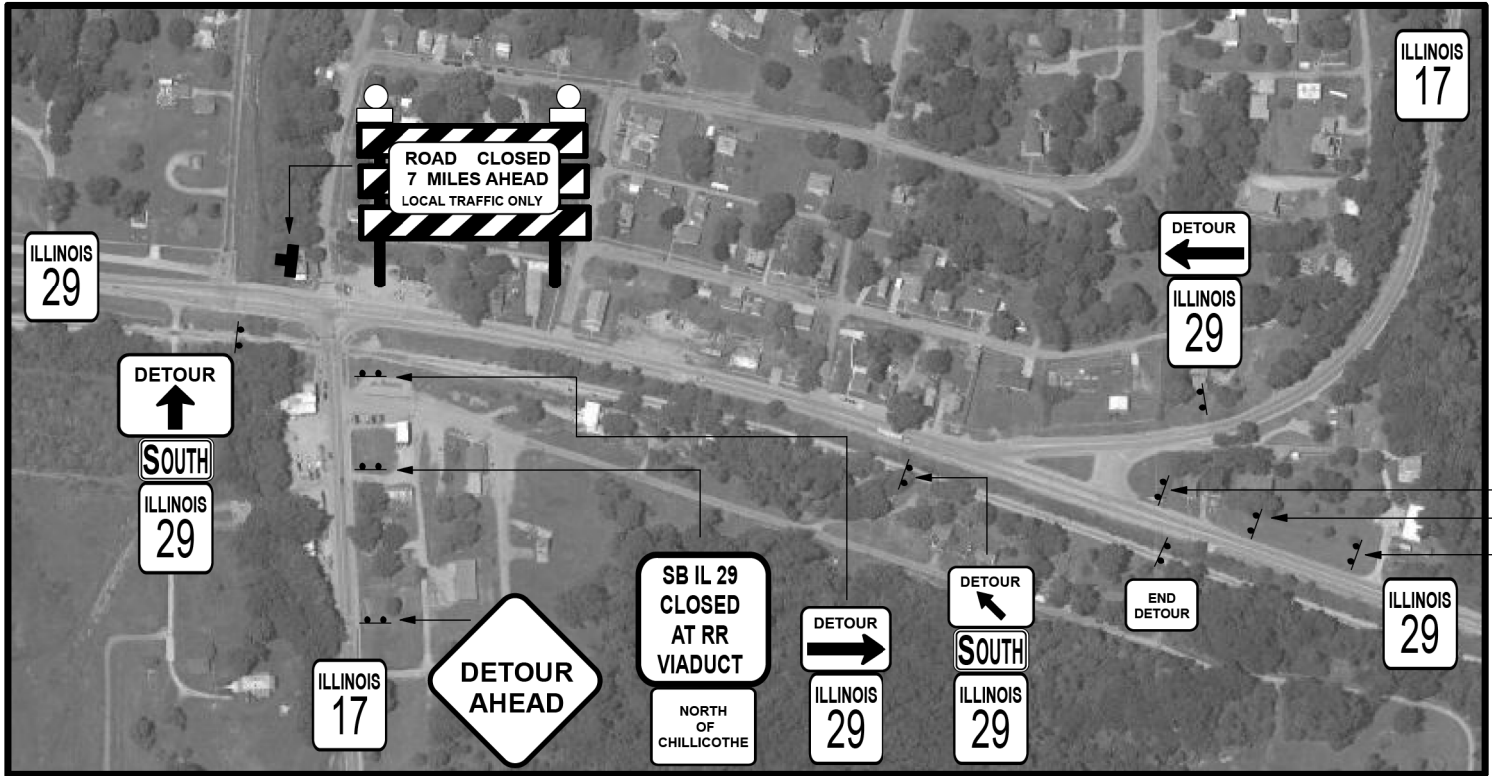
THE LOCATIONS WILL BE DISCUSSED AT THE PRECONSTRUCTION MEETING.

ALL SIGNS MAY BE ON TEMPORARY STANDS.

THE CONTRACTOR SHALL NOTIFY EMS/FIRE/POLICE/USPS OF THE PENDING CLOSURE DATE ONE WEEK IN ADVANCE OF THE CLOSURE.



PLACE ON YANKEE LANE



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETOUR PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;(6,5)RS-3	PEORIA	48	28
CONTRACT NO. 68F12			ILLINOIS FED. AID PROJECT	

NOTES FOR ADA RAMP CONSTRUCTION (REMOVAL)

1. CURB AND GUTTER REMOVAL SHALL REQUIRE FULL DEPTH SAWCUTS. THE SAWCUTS SHALL BE MADE 1 FT BEYOND THE EDGE OF THE CURB AND GUTTER TO ALLOW FOR PLACEMENT OF FORMS.
2. PAVEMENT REMOVAL SHALL REQUIRE FULL DEPTH SAWCUTS. WHERE PAVEMENT REMOVAL IS REQUIRED FOR INSTALLATION OF CURB AND/OR GUTTER THE SAWCUTS SHALL BE MADE 1 FT BEYOND THE EDGE OF THE NEW CURB AND/OR GUTTER TO ALLOW FOR PLACEMENT OF FORMS.
3. SAWCUTTING THE SIDEWALK IS INCLUDED IN THE COST OF SIDEWALK REMOVAL.
4. RADIAL DIMENSIONS SHOWN ARE MEASURED ALONG FACE OF CURB FOR CURB & GUTTER REMOVAL.
5. SAWCUTTING THE CURB AND GUTTER IS INCLUDED IN THE COST OF CURB AND GUTTER REMOVAL.
6. SAWCUTTING FOR PAVEMENT REMOVAL IS INCLUDED IN THE COST OF PAVEMENT REMOVAL.

NOTES FOR ADA RAMP CONSTRUCTION (PROPOSED)

1. SIDEWALK CROSS SLOPE = 1.5% (2.0% MAX.)
2. UPPER LANDINGS AND LOWER LANDINGS SHALL BE 1.5% MAXIMUM IN ANY DIRECTION; INCLUDING DIAGONALLY
3. MAXIMUM RAMP LONGITUDINAL SLOPE = 8.3% (1:12)
4. MAXIMUM SLOPE AROUND RADIUS = 2%
5. FLARES ARE 1.5' UNLESS OTHERWISE NOTED.
6. CONTRACTOR TO UTILIZE RADIAL DETECTABLE WARNING PANELS AROUND RADIUS. CONTRACTOR IS RESPONSIBLE FOR MEASURING THE RADIUS PRIOR TO ORDERING PANELS.
7. CONTRACTOR SHALL VERIFY REMOVAL LIMITS AND FIELD ELEVATIONS TO ENSURE THAT ADA STANDARDS ARE MET.
8. PCC BASE COURSE, 13" TO BE USED FOR 1 FT WIDE STRIP BEYOND THE EDGE OF THE NEW CURB AND GUTTER (SEE SCHEDULE FOR QUANTITY AND LOCATION).
9. ANY EXCAVATION NECESSARY FOR THE PROPOSED SIDEWALK IS INCLUDED IN THE COST OF PCC SIDEWALK, 4".
10. ALL DISTURBED AREAS ALONG SIDEWALKS SHALL BE GRADED AND SEEDED.

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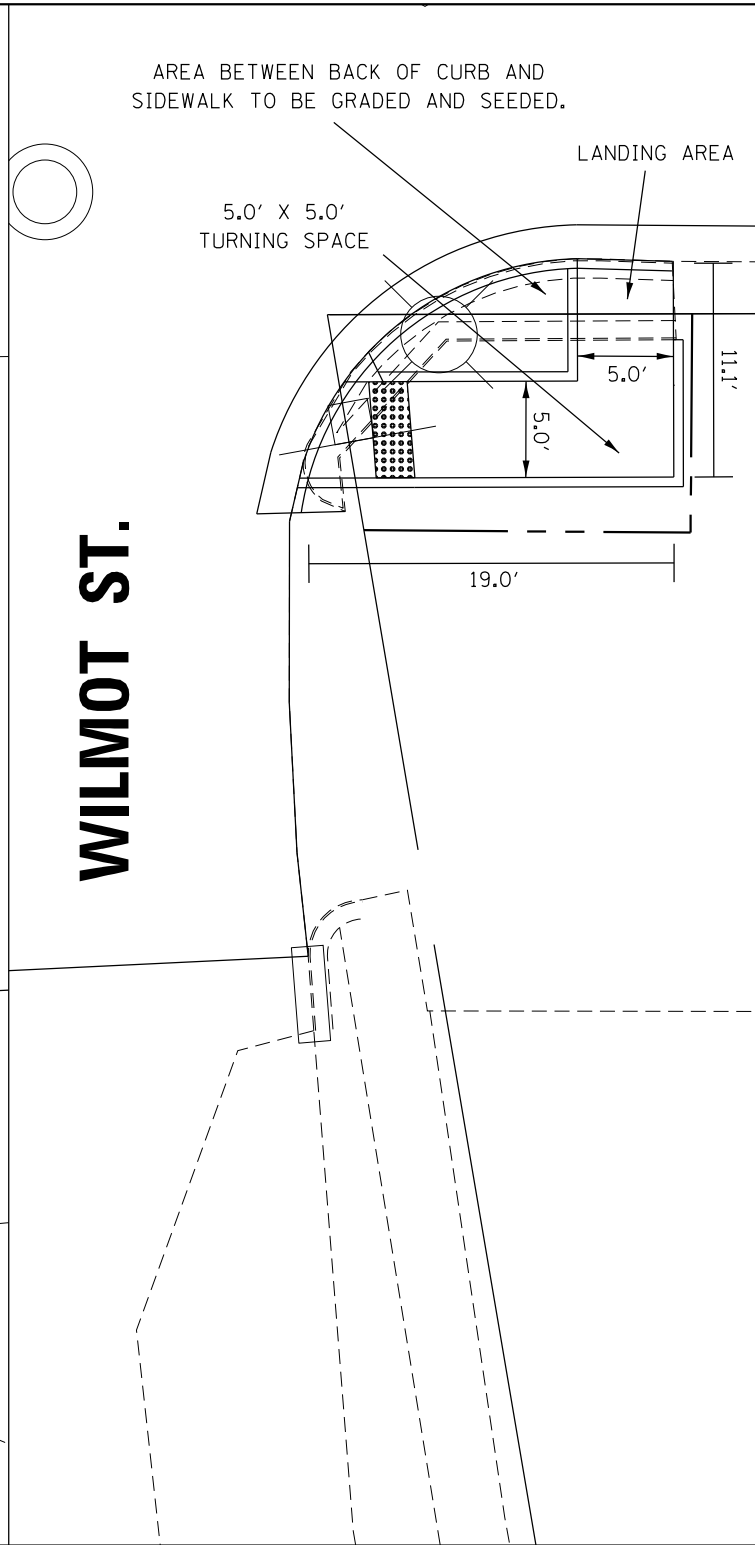
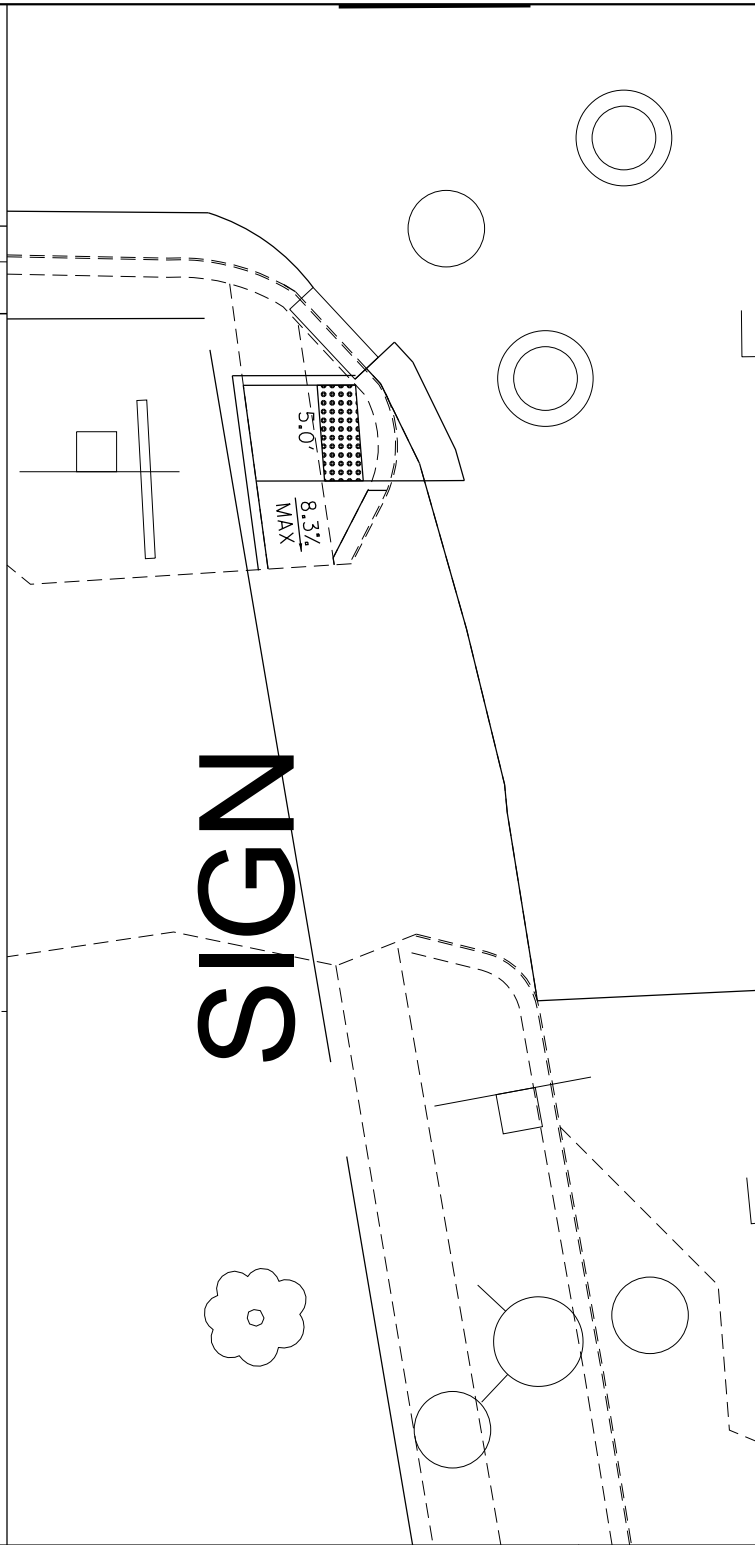
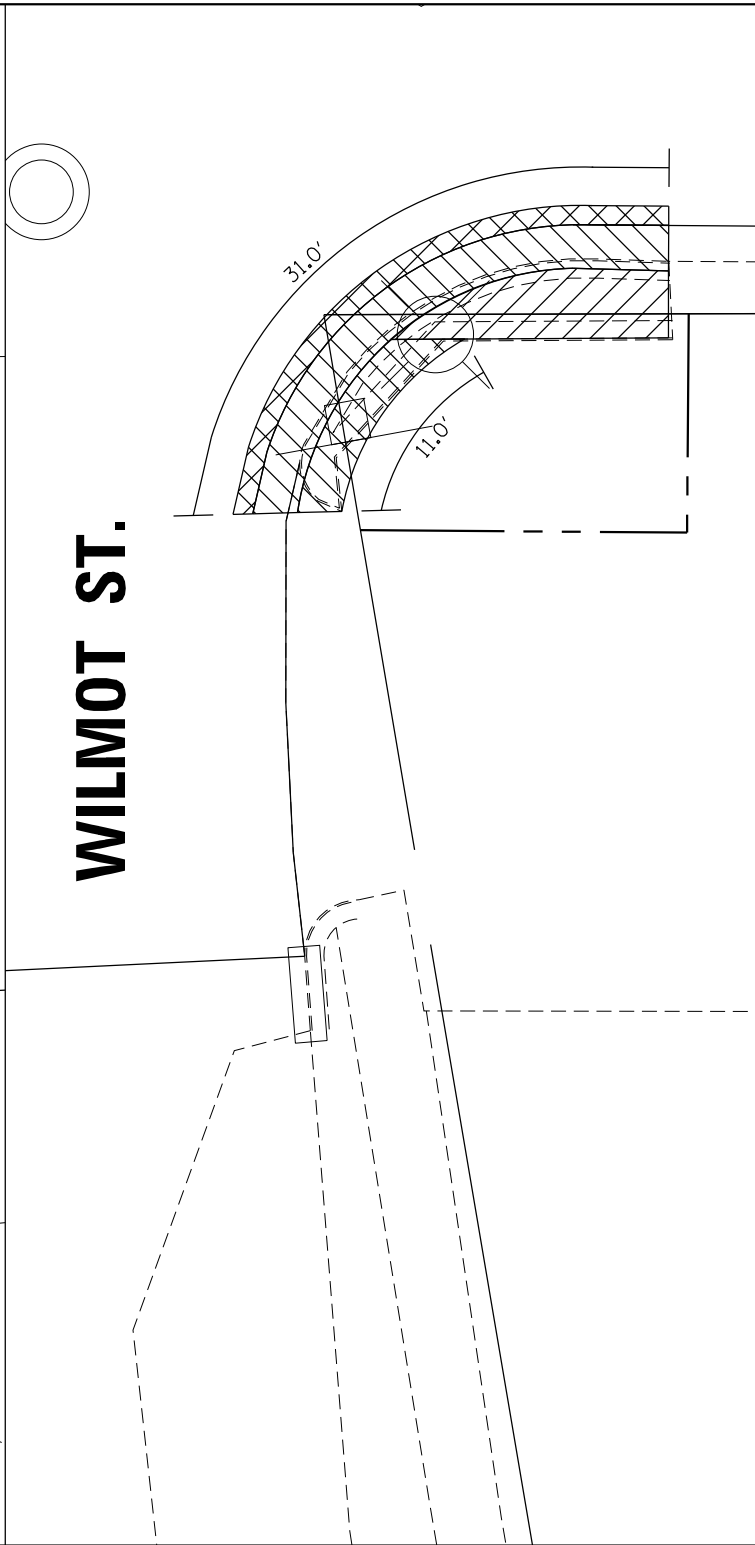
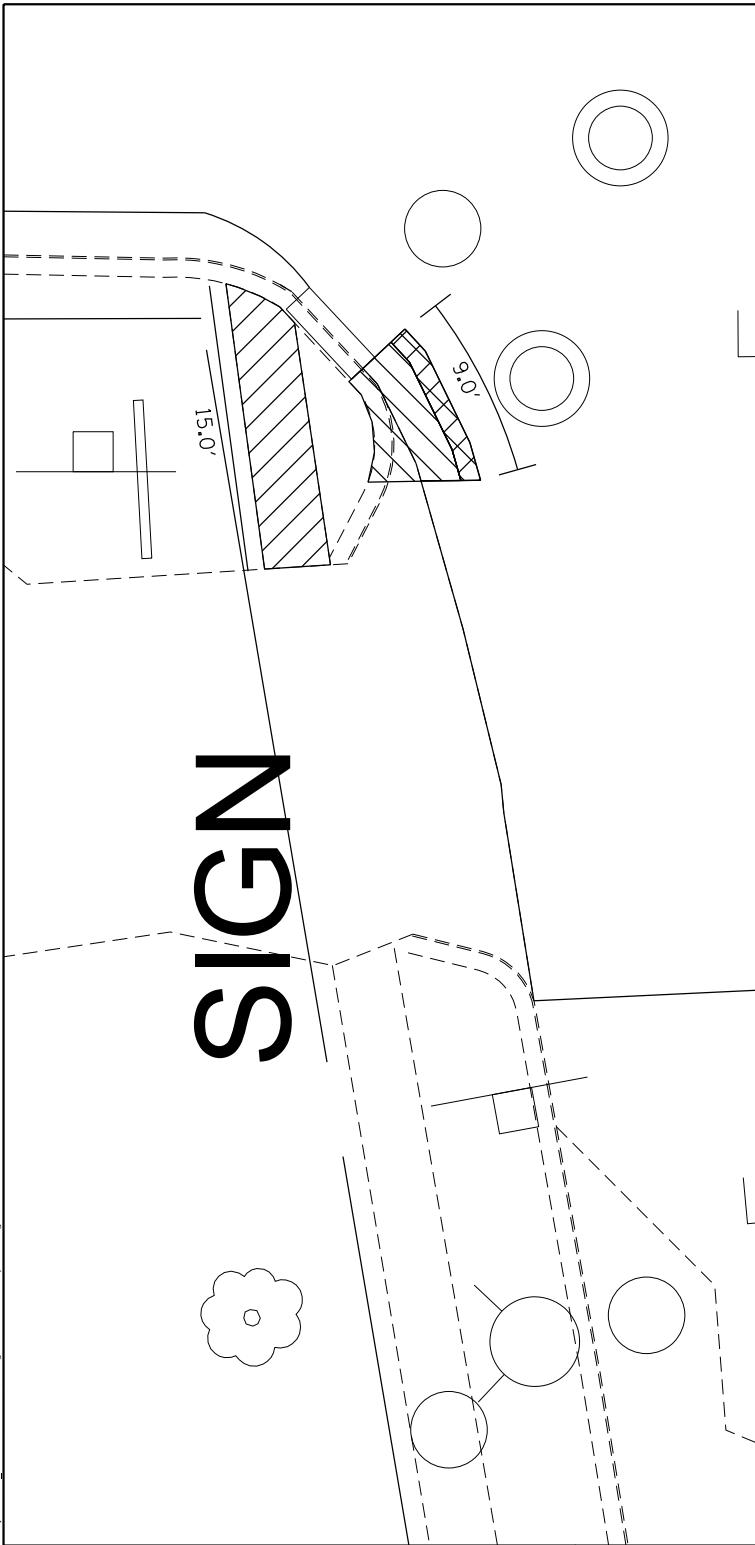
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ADA RAMP CONSTRUCTION NOTES

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8:(6,5)RS-3	PEORIA	48	29
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				



REMOVAL LEGEND

- CURB & GUTTER REMOVAL
- SIDEWALK /SIDE CURB REMOVAL
- PAVEMENT REMOVAL
- HMA SURF REM (BUTT JT)

PROPOSED LEGEND

- DETECTABLE WARNING

LANDING PR. = 1.5% MAX SLOPE EACH DIRECTION



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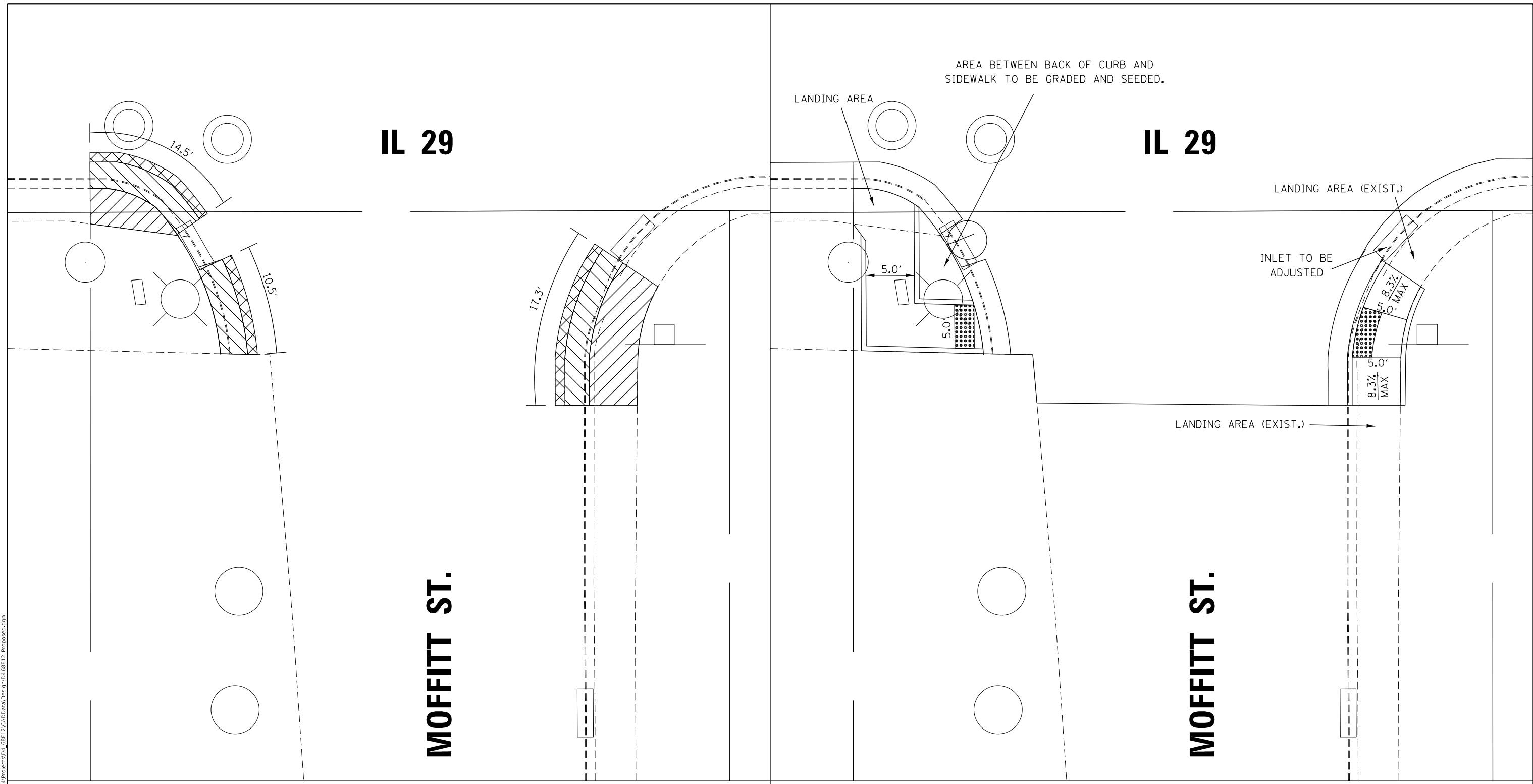
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP CONSTRUCTION
WILMOT ST.

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;(6,5)RS-3	PEORIA	30	48
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

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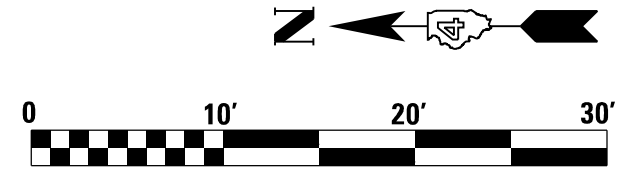


REMOVAL LEGEND

- CURB & GUTTER REMOVAL**
- SIDEWALK /SIDE CURB REMOVAL**
- PAVEMENT REMOVAL**
- HMA SURF REM (BUTT JT)**

PROPOSED LEGEND

- DETECTABLE WARNING**



LANDING PR. = 1.5% MAX SLOPE EACH DIRECTION

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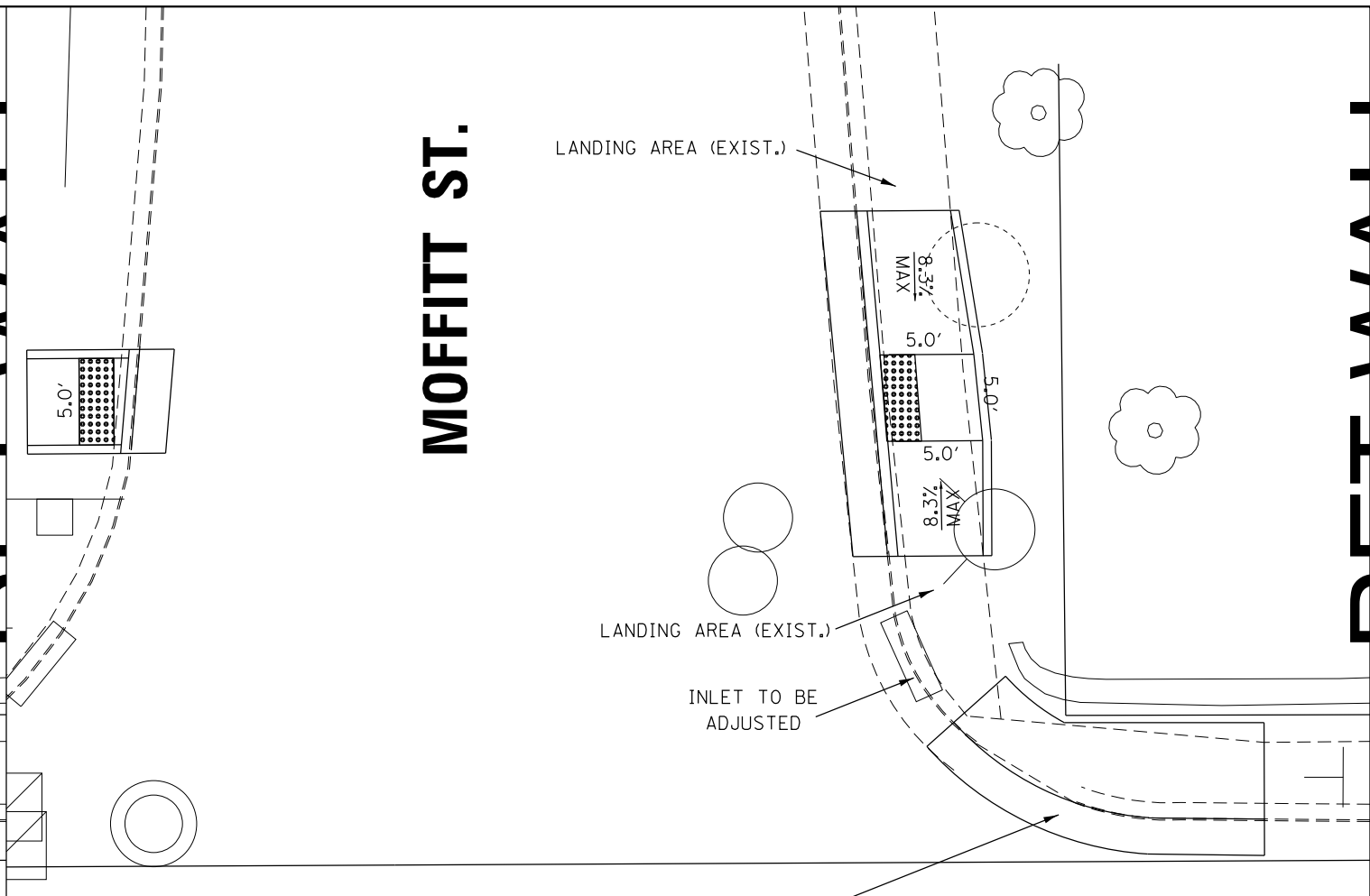
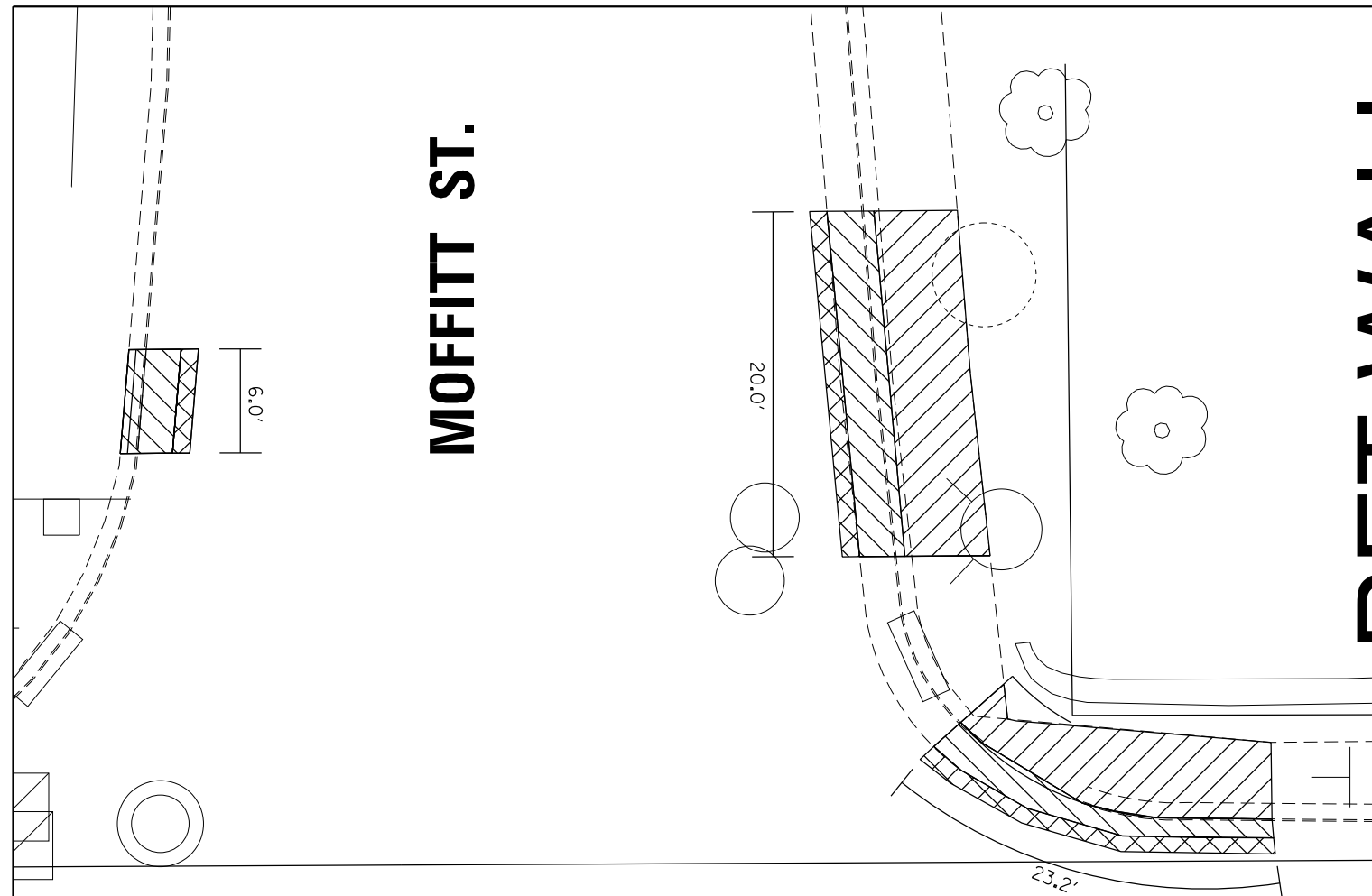
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP CONSTRUCTION
W MOFFITT ST.**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;(6,5)RS-3	PEORIA	31	48
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

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IL 29

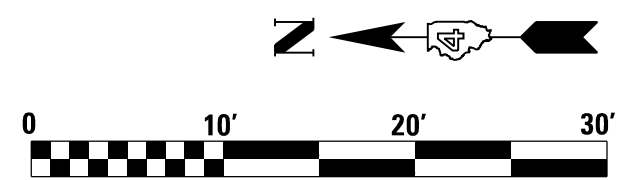
IL 29

REMOVAL LEGEND

- CURB & GUTTER REMOVAL**
- SIDEWALK /SIDE CURB REMOVAL**
- PAVEMENT REMOVAL**
- HMA SURF REM (BUTT JT)**

PROPOSED LEGEND

- DETECTABLE WARNING**



LANDING PR. = 1.5% MAX SLOPE EACH DIRECTION

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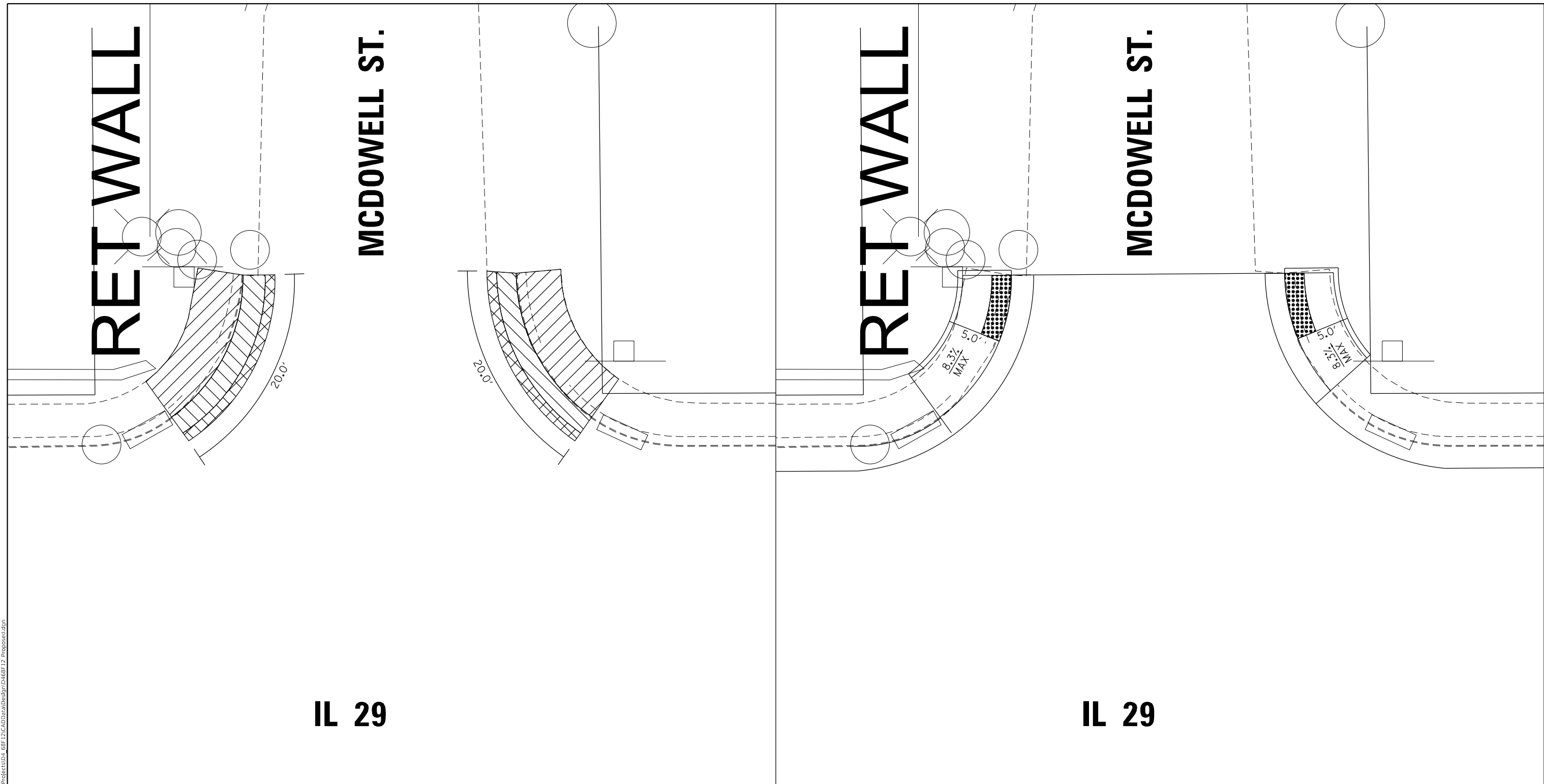
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP CONSTRUCTION
E MOFFITT ST.**





SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;(6,5)RS-3	PEORIA	32	48
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

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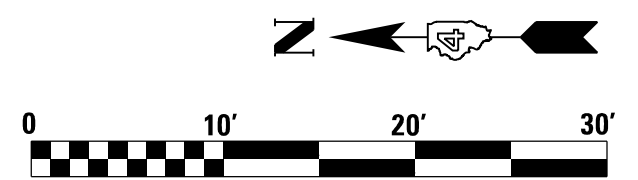
REMOVAL LEGEND

-  CURB & GUTTER REMOVAL
-  SIDEWALK /SIDE CURB REMOVAL
-  PAVEMENT REMOVAL
-  HMA SURF REM (BUTT JT)

PROPOSED LEGEND

-  DETECTABLE WARNING

LANDING PR. = 1.5% MAX SLOPE EACH DIRECTION



USER NAME = hudsonme	DESIGNED -	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

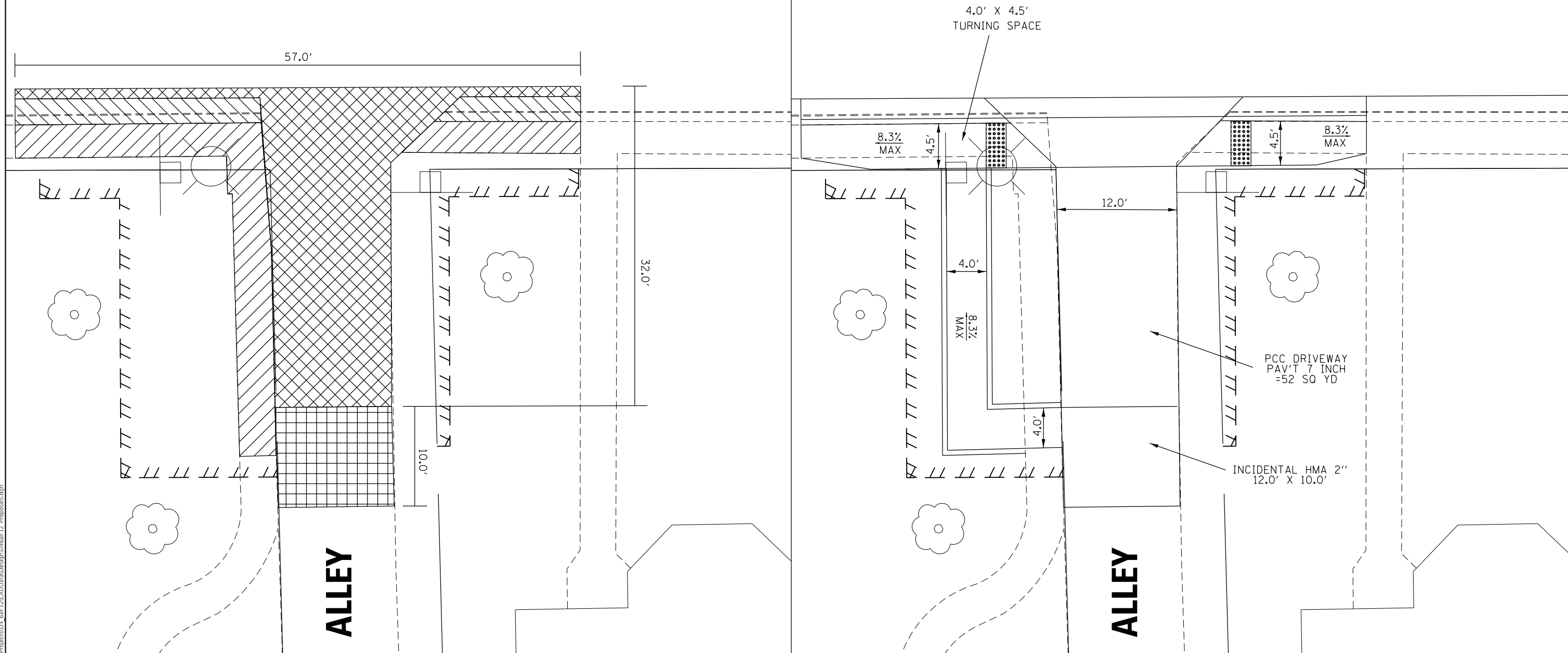
ADA RAMP CONSTRUCTION
MCDOWELL ST.

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;(6,5)RS-3	PEORIA	33	48
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

IL 29

IL 29

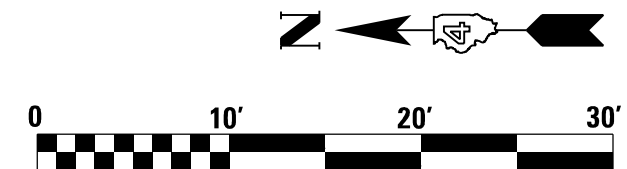


REMOVAL LEGEND

- CURB & GUTTER REMOVAL
- PAVEMENT REMOVAL
- SIDEWALK /SIDE CURB REMOVAL
- HMA SURF REM (BUTT JT)

PROPOSED LEGEND

- DETECTABLE WARNING



LANDING PR. = 1.5% MAX SLOPE EACH DIRECTION

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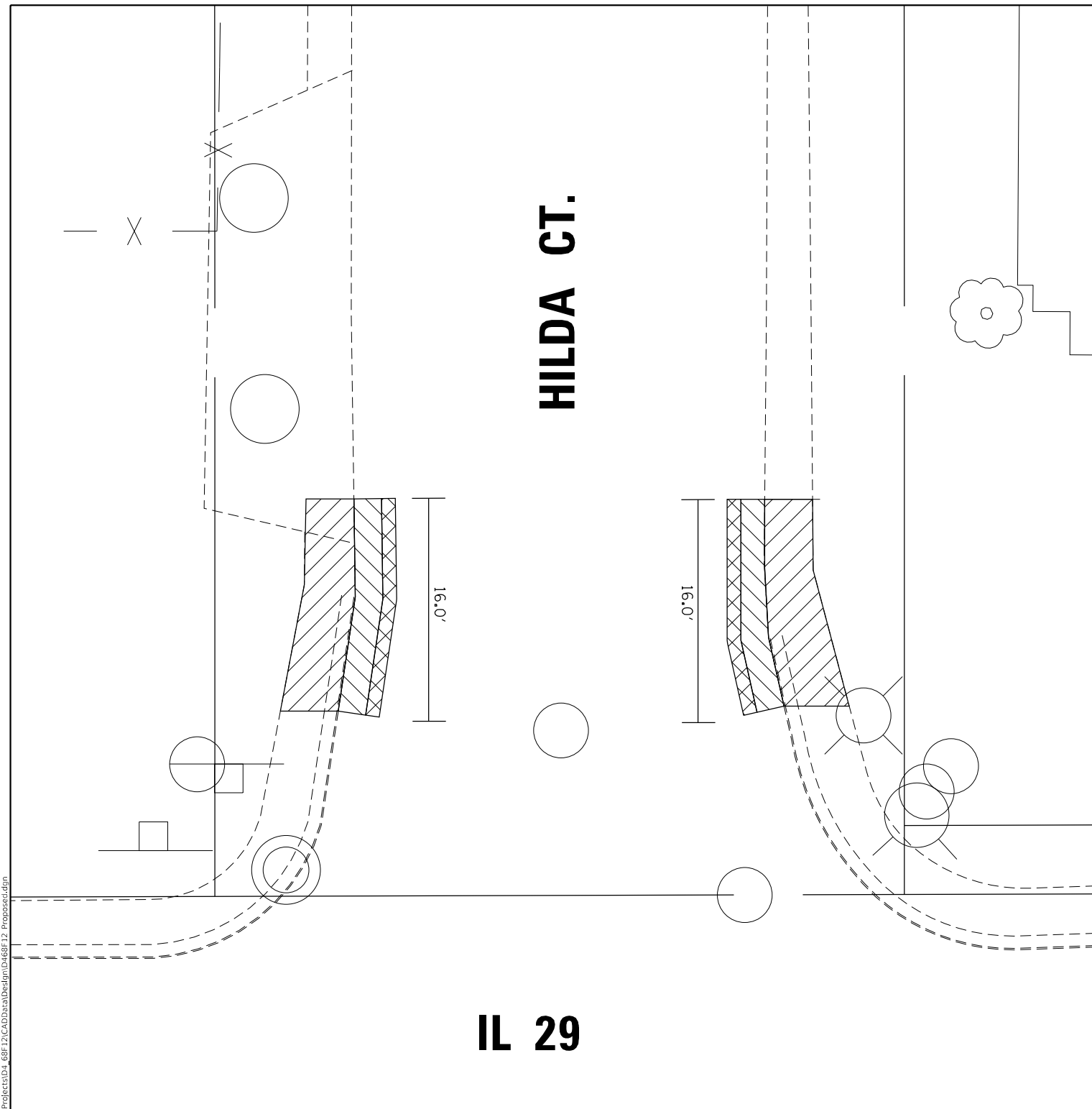
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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP CONSTRUCTION	
ALLEY 35+99 RT	
SCALE:	SHEET OF SHEETS STA. TO STA.

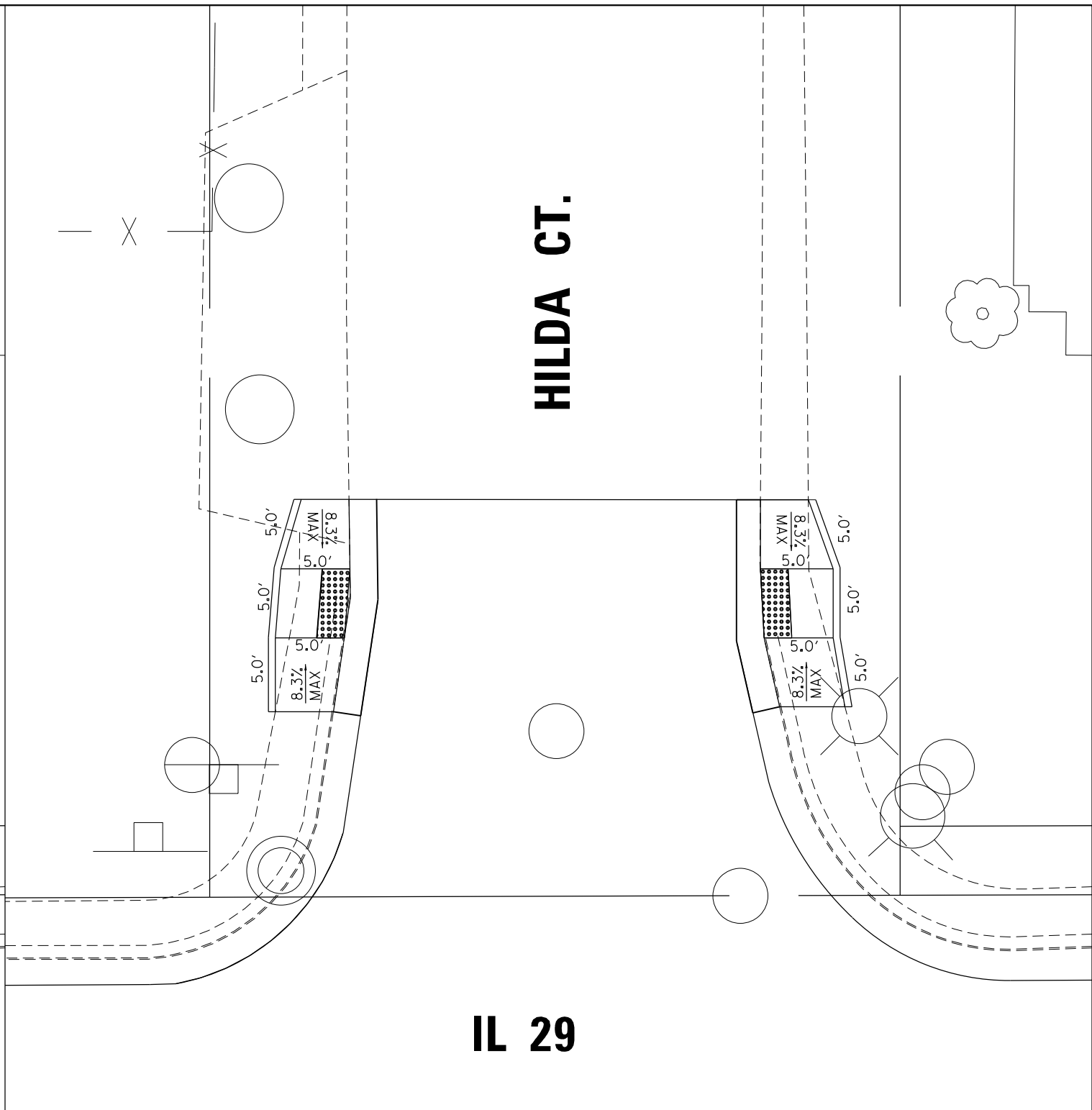
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;(6,5)RS-3	PEORIA	34	48
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				

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HILDA CT.

IL 29



HILDA CT.

IL 29

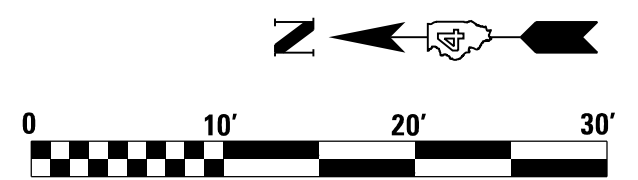
REMOVAL LEGEND

- CURB & GUTTER REMOVAL**
- SIDEWALK /SIDE CURB REMOVAL**
- PAVEMENT REMOVAL**
- HMA SURF REM (BUTT JT)**

PROPOSED LEGEND

- DETECTABLE WARNING**

LANDING PR. = 1.5% MAX SLOPE EACH DIRECTION

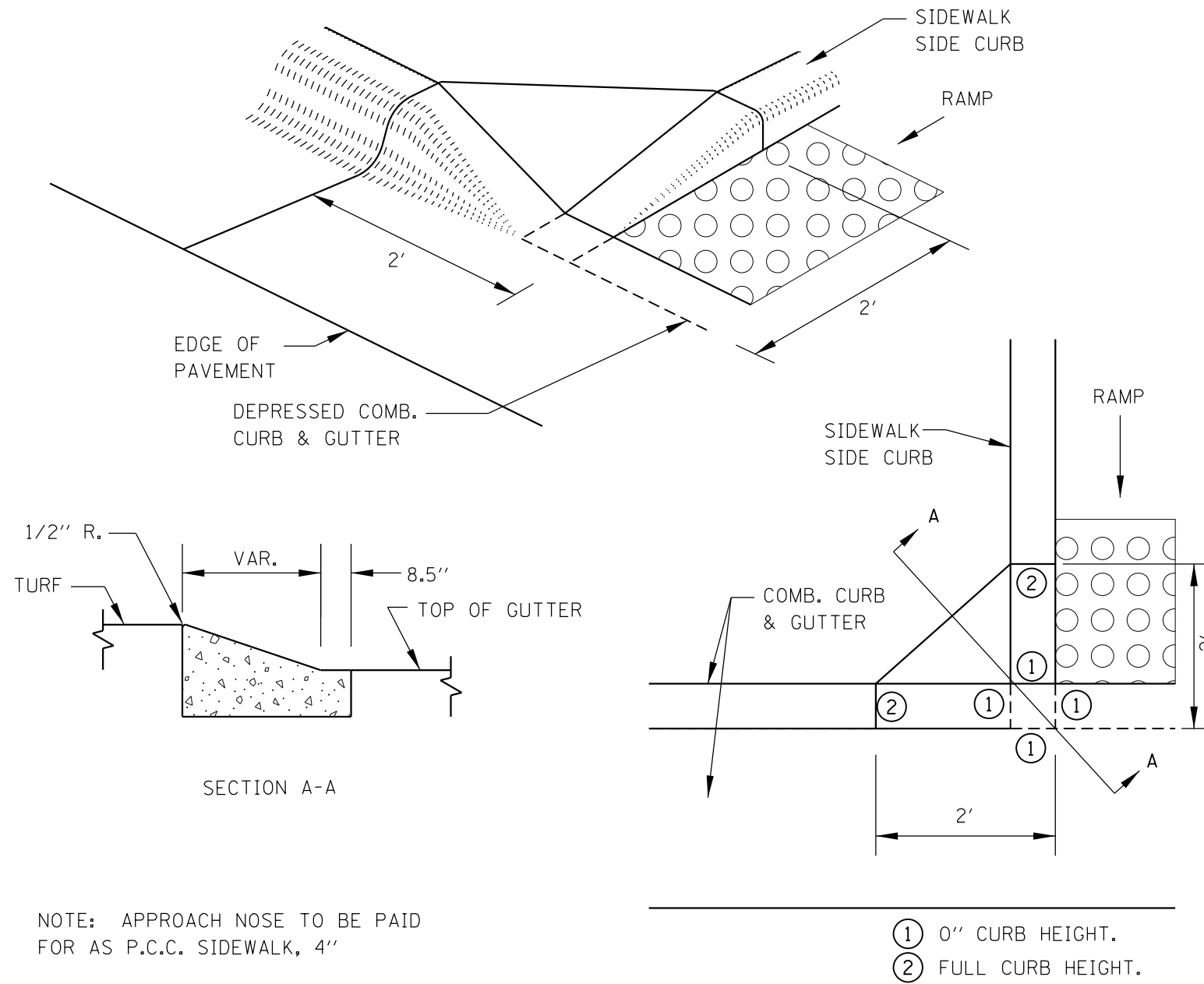


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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ADA RAMP CONSTRUCTION HILDA CT.			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;(6,5)RS-3	PEORIA	35	48
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				



APPROACH NOSE DETAIL

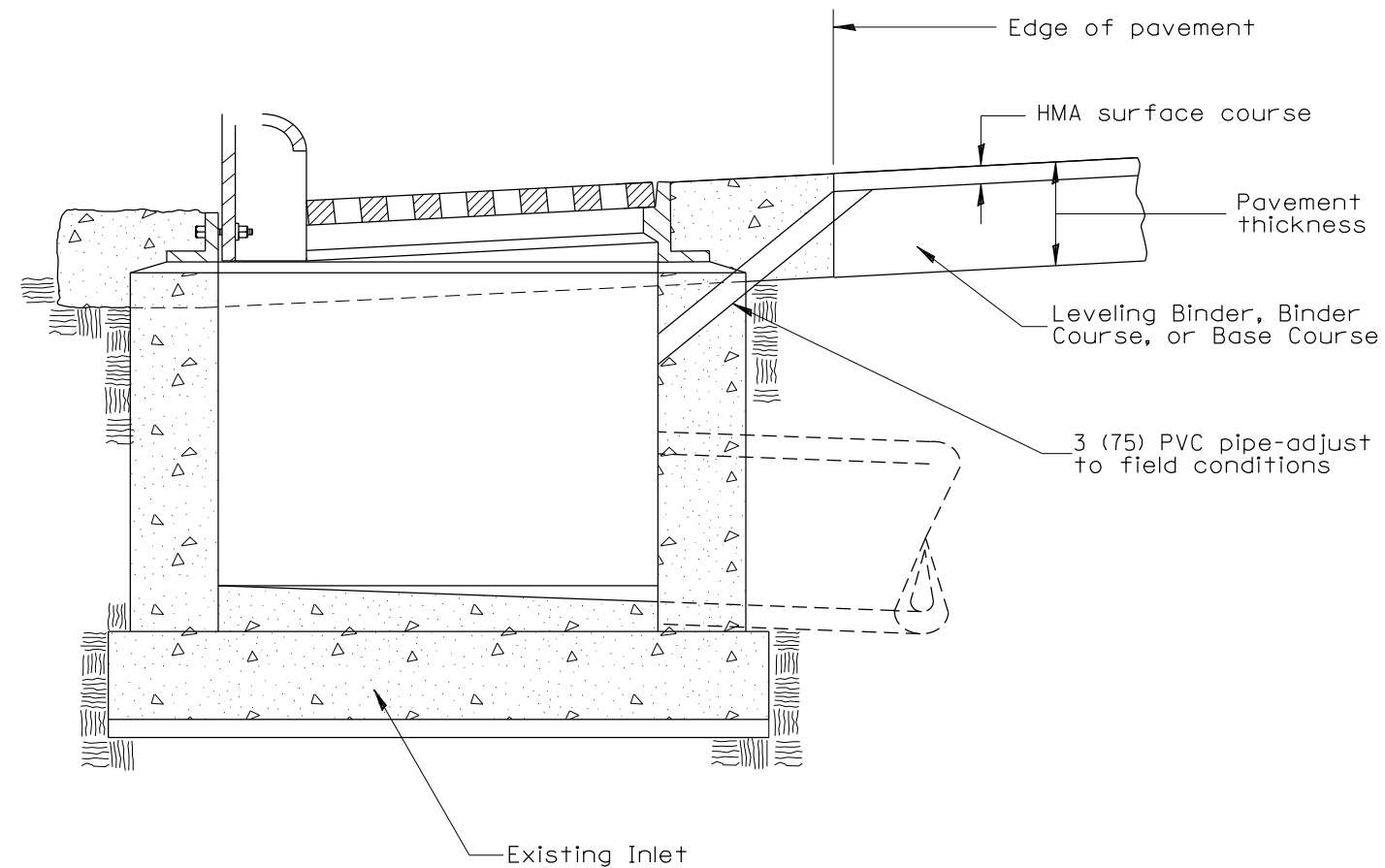
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP CONSTRUCTION
APPROACH NOSE DETAIL**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8:(6,5)RS-3	PEORIA	48	36
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68F12	



GENERAL NOTES:

1. Temporary Drainage - This work consists of furnishing and installing a 3 (75) PVC pipe in the existing inlets to facilitate drainage off the pavement prior to the placement of HMA surface course.
2. Adequate pavement drainage shall be maintained during all phases of construction on milled and leveling binder surfaces.
3. The PVC pipe is to be plugged at both ends with non-shrink hydraulic cement prior to surface course placement.
4. This detail applies to the referenced locations. However, the Engineer reserves the right to move or add locations at his or her discretion. Additional locations treated in excess of the quantity shown will be paid for in accordance with Article 109.04.

LOCATIONS:

- | | |
|---------------------------|----------------------------|
| 1. RT 1553+03 | 16. SE CORNER MOFFITT ST. |
| 2. LT 153+03 | 17. RT 35+35 |
| 3. RT 1556+47 | 18. RT 37+00 |
| 4. LT 1556+47 | 19. LT 37+00 |
| 5. LT 1567+15 | 20. NE CORNER MCDOWELL ST. |
| 6. NW CORNER WILMOT ST. | 21. SE CORNER MCDOWELL ST. |
| 7. SW CORNER WILMOT ST. | 22. RT 31+20 |
| 8. RT 569+18 | 23. RT 32+85 |
| 9. LT 569+18 | 24. LT 32+85 |
| 10. RT 570+40 | 25. RT 34+85 |
| 11. LT 570+40 | 26. LT 34+85 |
| 12. LT 571+60 | 27. RT 36+66 |
| 13. NW CORNER MOFFITT ST. | 28. LT 36+66 |
| 14. SW CORNER MOFFITT ST. | 29. NE CORNER HILDA CT. |
| 15. NE CORNER MOFFITT ST. | 30. SE CORNER HILDA CT. |

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PLOT DATE = 12/11/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TEMPORARY INLET DRAINAGE TREATMENT

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8:(6,5)RS-3	PEORIA	48	37
CONTRACT NO. 68F12				
		ILLINOIS FED. AID PROJECT		

SIDEWALK - 35 + 86

510

505

500

495

PROP T.E.

30 20 10 0 10 20 30 40 50 60 70 80 90 100

PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
	ALIGNMENT CHECKED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CURVED	
	C/CAD FILE NAME	

CL ALLEY - 35 + 99

510

505

500

495

BUTT JOINT

30 20 10 0 10 20 30 40 50 60 70 80 90 100

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CURVED	

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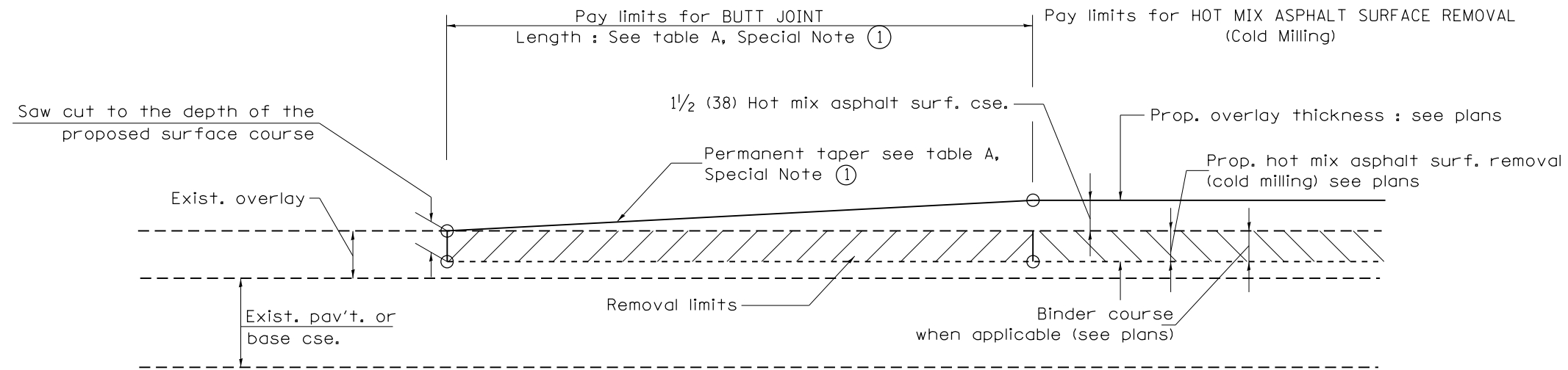
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PLOT DATE = 12/11/2020	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS - ALLEY
AND SIDEWALK

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	8RS-8;(6,5)RS-3	PEORIA	48	38
CONTRACT NO. 68F12				
ILLINOIS FED. AID PROJECT				



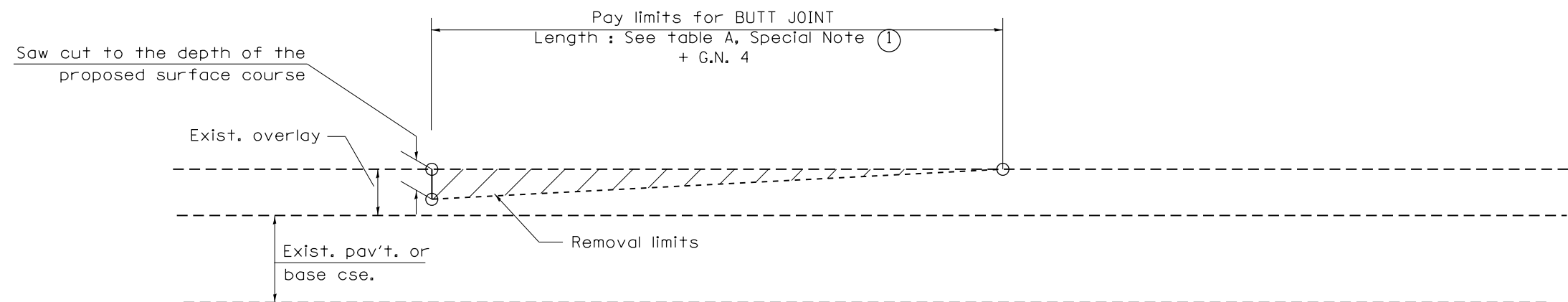
CASE 1 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

**TABLE A
TAPER RATES**

SPECIAL NOTE NUMBER	ELEMENT	MAINLINE INTERSTATES & 4-LANE EXPRESSWAYS	ALL OTHERS
①	BUTT JOINT TAPER RATE	1:480	1:240
②	TEMPORARY RAMP TAPER RATE	1:80	1:40

GENERAL NOTES

1. The work shall be done in accordance with Article 406.08 and the Special Provision for Butt Joints.
2. The pavement surface to be removed may be either bituminous or P.C. concrete. The work shall be performed in accordance with Article 440.04 and the Special Provisions for Butt Joints.
3. The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.05.
4. The length of butt joint is based on the taper rate times change in cold milling depth within the butt joint pay limits, unless otherwise indicated.
5. Temporary ramps are paid for separately and not included in the cost of the butt joints.



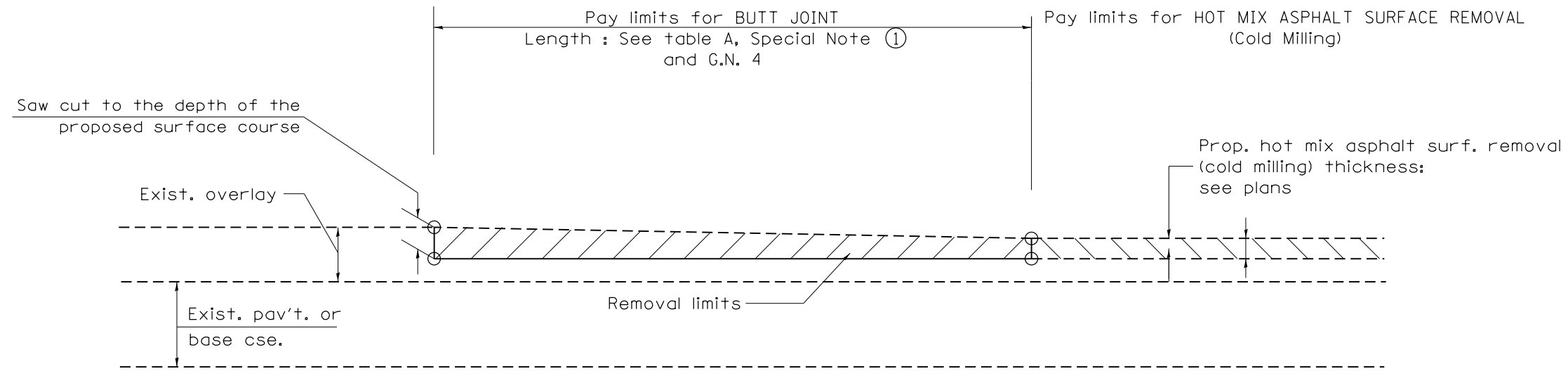
CASE 2 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

All dimensions are in inches (millimeters) unless otherwise noted.

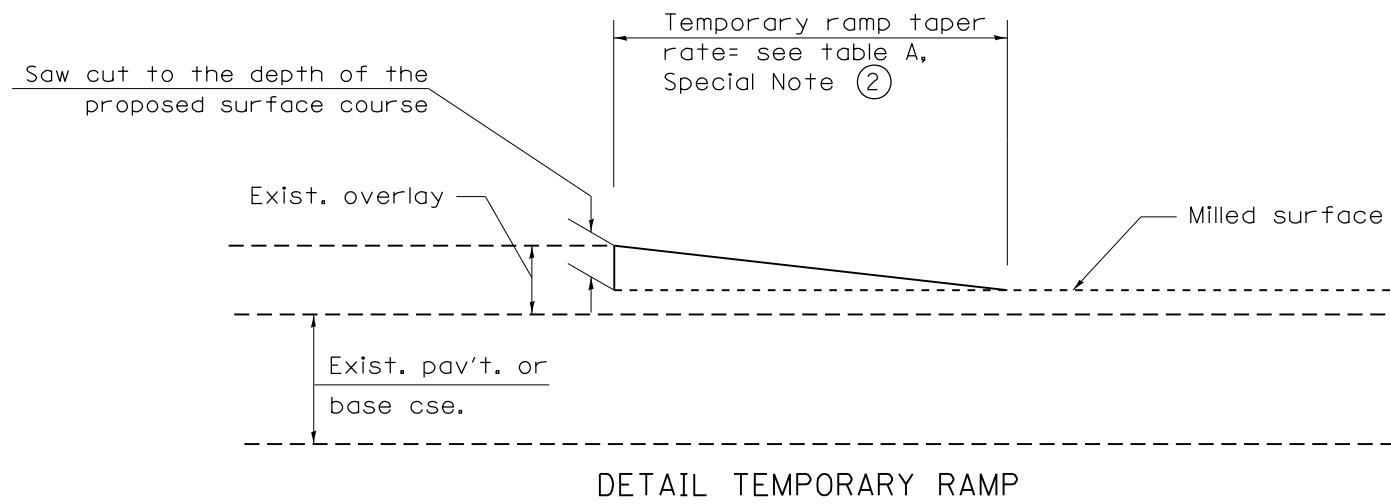
01-01-97	RENUM. C-23.01, NEW REVISION BOX	T.P.	08-21-13	MAJOR MODIFICATIONS	R.D.	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINTS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
04-01-97	CORRECTION TO DEPTH	J.A.	04-12-16	MINOR CORRECTIONS	R.D.			318	BRS-8;(6,5)RS-3	PEORIA	48	39
09-15-05	REVISED DESIGNER NOTE	M.M.A.	02-14-17	ADDED NOTE 5	R.D.			CONTRACT NO. 68F12				
10-16-06	REVISED TO 2007 SPEC.	M.A.	07-16-19	Wording and Spelling corrections	R.D.			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

NOT TO SCALE

SHT. 1 OF 3
CADD STD. 406101-D4



CASE 3 : HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER



All dimensions are in inches (millimeters) unless otherwise noted.

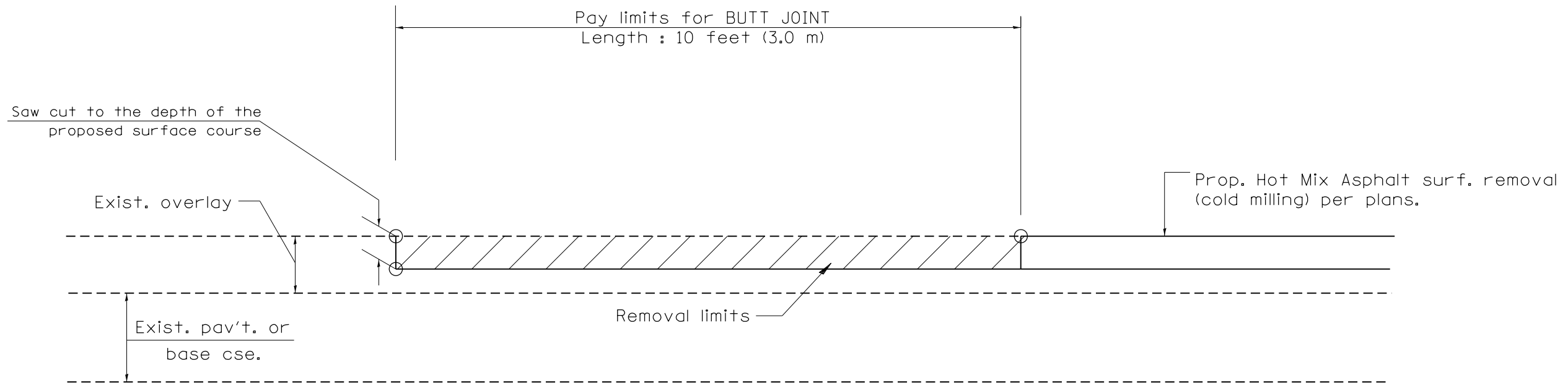
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINTS

NOT TO SCALE

SHT. 2 OF 3
CADD STD. 406101-D4

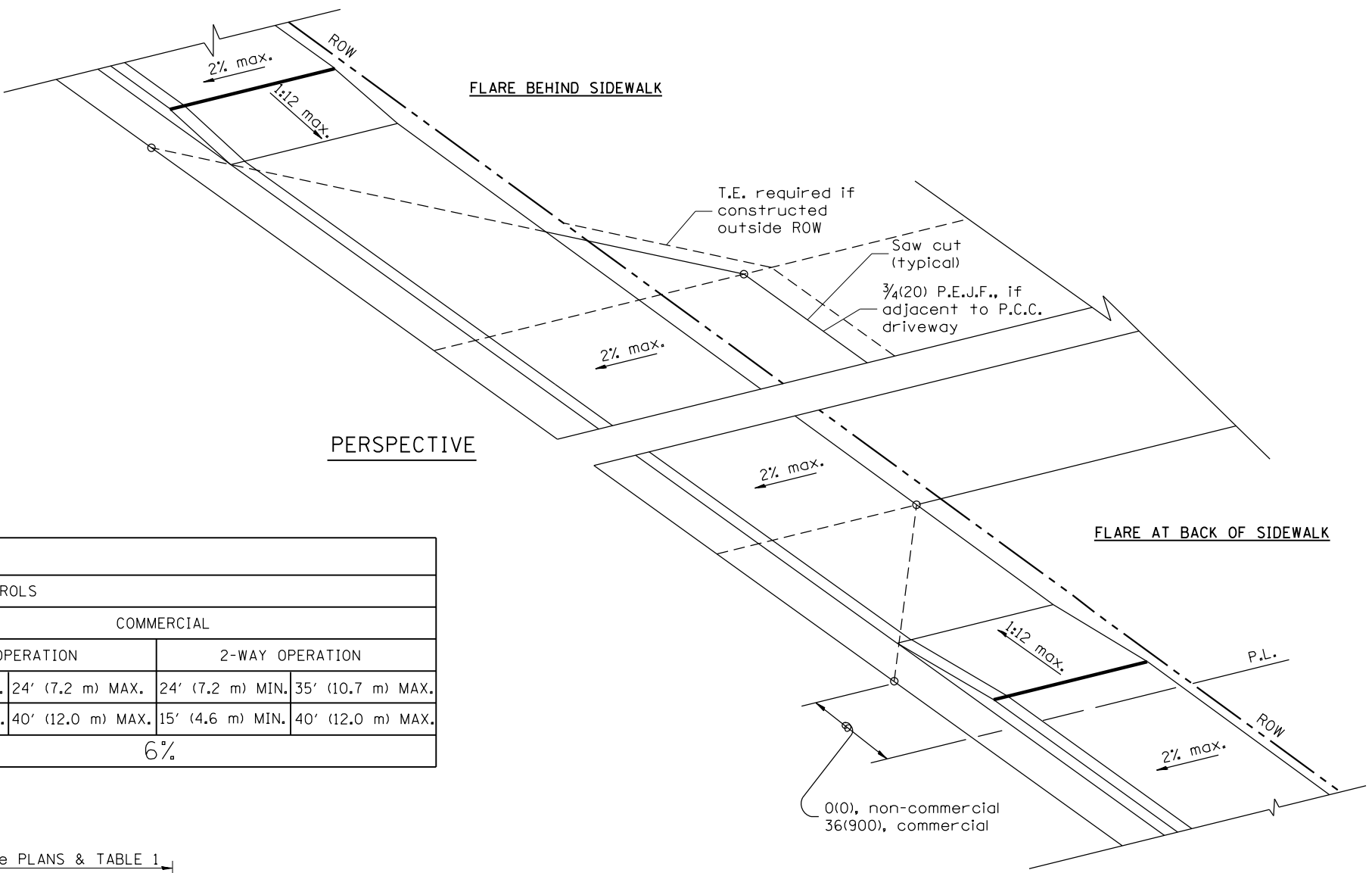
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	BRS-8;(6,5)RS-3	PEORIA	48	40
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68F12	



CASE 4 : SINGLE LIFT OVERLAY WITH EQUIVALENT DEPTH
HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER

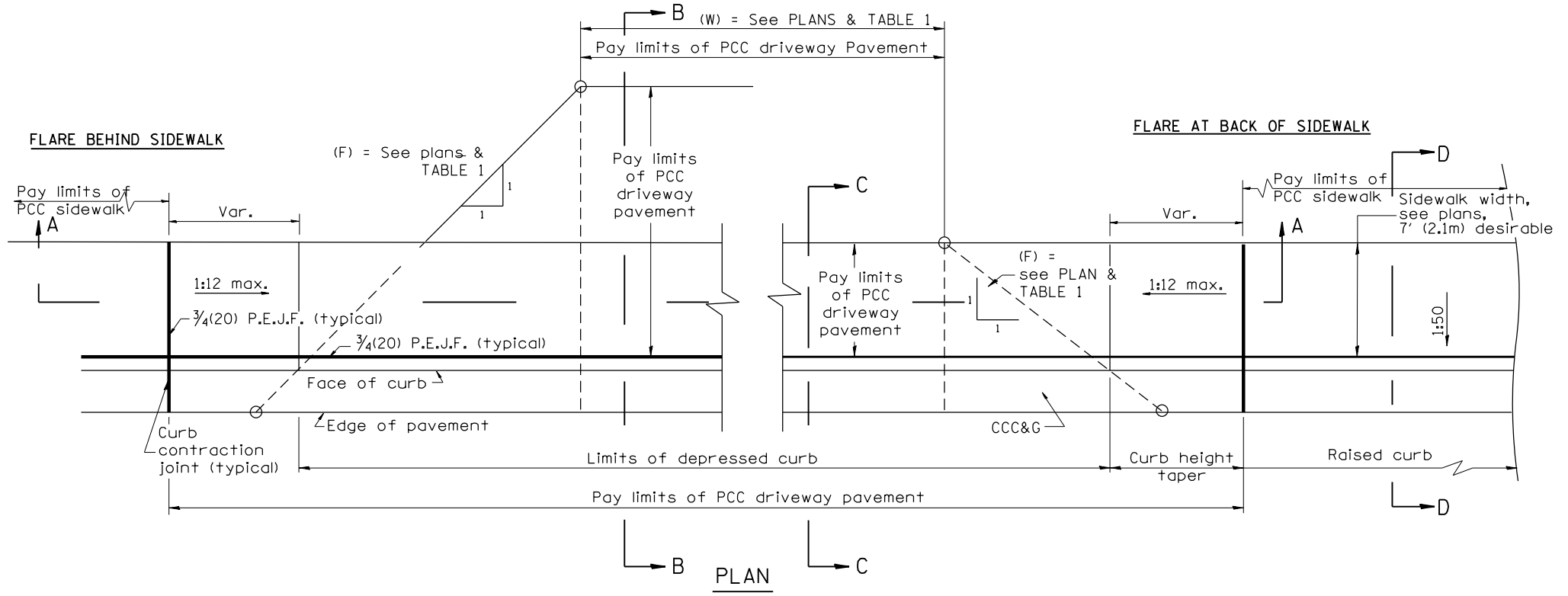
All dimensions are in inches (millimeters) unless otherwise noted.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION					BUTT JOINTS						
			NOT TO SCALE			SHT. 3 OF 3 CADD STD. 406101-D4					
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.							
318	BRS-8;(6,5)RS-3	PEORIA	48	41	CONTRACT NO. 68F12						
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT									



PERSPECTIVE

TABLE 1 URBAN ENTRANCE DESIGN CONTROLS						
ELEMENT	NON-COMMERCIAL		COMMERCIAL			
			1-WAY OPERATION		2-WAY OPERATION	
WIDTH (W)	12' (3.6 m) MIN.	24' (7.2 m) MAX.	14' (4.3 m) MIN.	24' (7.2 m) MAX.	24' (7.2 m) MIN.	35' (10.7 m) MAX.
RADIUS EQUIVALENT 1:1 FLARE (F)	5' (1.5 m) MIN.	25' (7.6 m) MAX.	15' (4.6 m) MIN.	40' (12.0 m) MAX.	15' (4.6 m) MIN.	40' (12.0 m) MAX.
MAX. GRADE (G)	8%		6%			

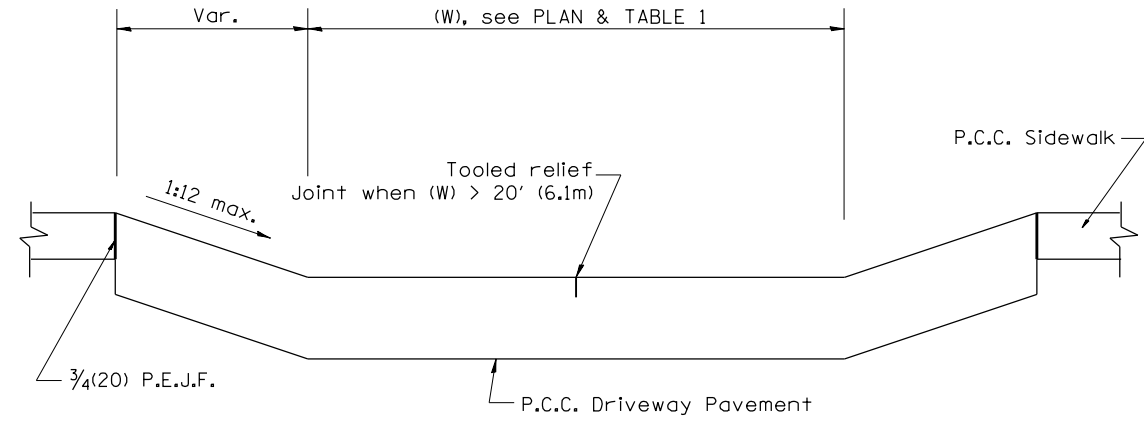


PLAN

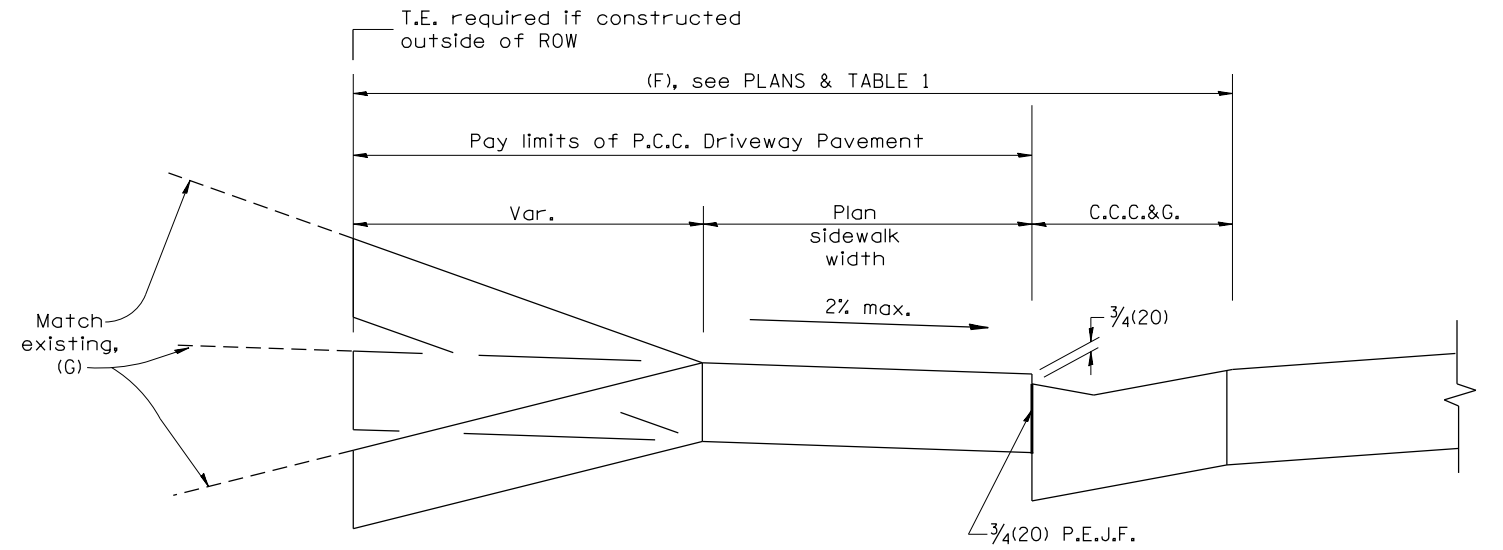
GENERAL NOTES

1. The sidewalk area located within the pay limits of P.C.C. Driveway Pavement will be of the same thickness and construction as the P.C.C. Driveway Pavement.
2. Combination Concrete Curb & Gutter shall be depressed in accordance with Standard 606001.
3. Exceptions to the radius flare/property line relationship are as shown in the plans for common entrances, with jointly executed access permits.
4. Refer to State Standard 424026 for detectable warnings at busy commercial entrances.

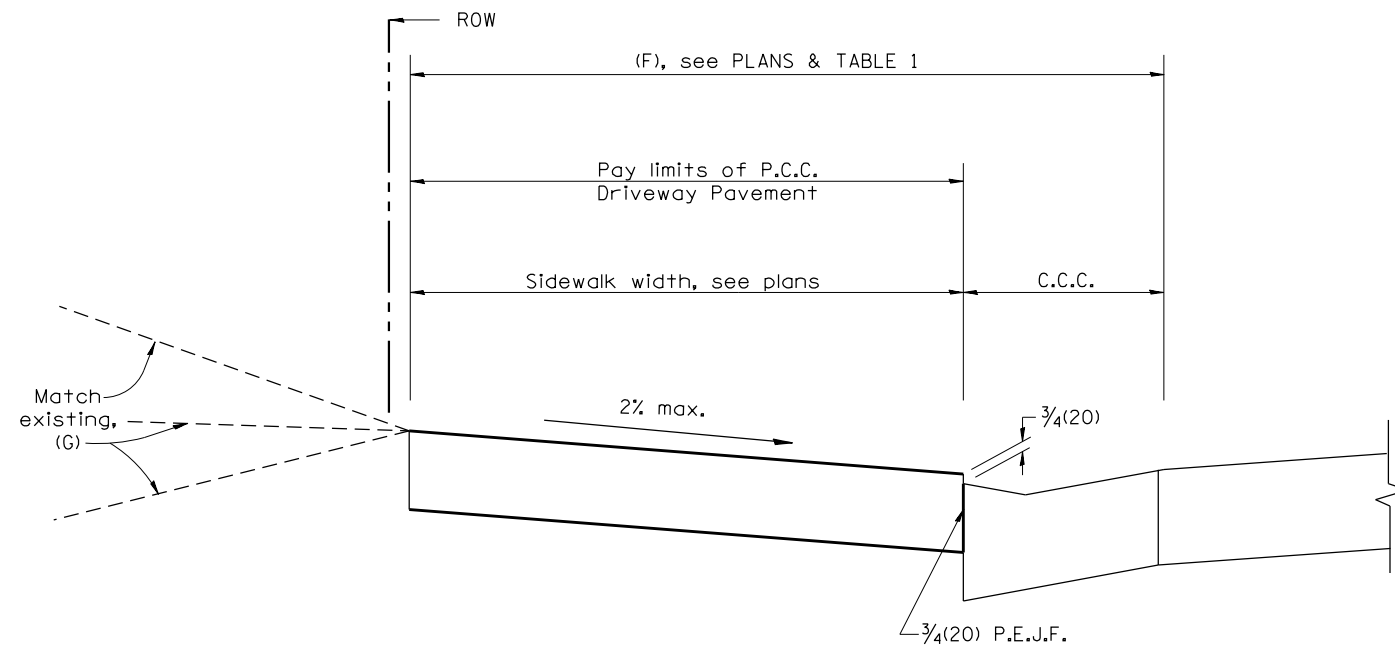
All dimensions are in inches (millimeters) unless otherwise noted.



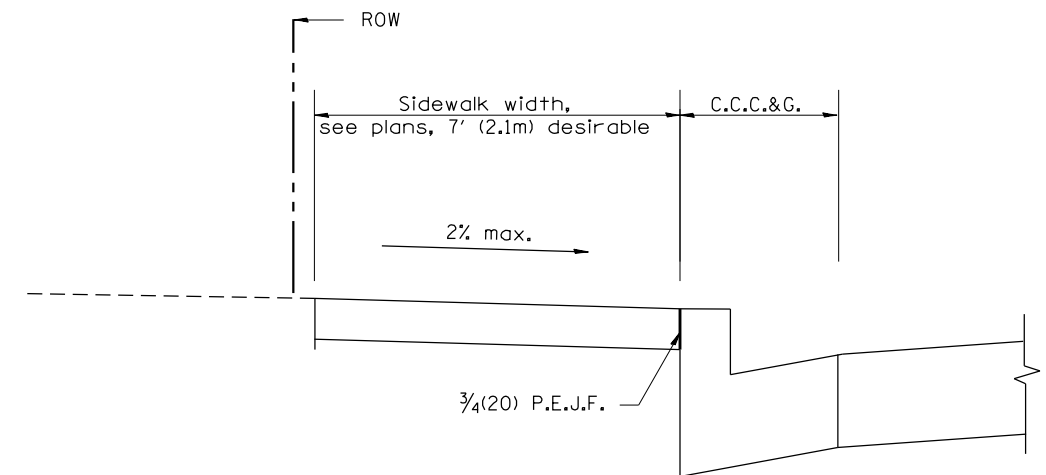
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

All dimensions are in inches (millimeters) unless otherwise noted.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

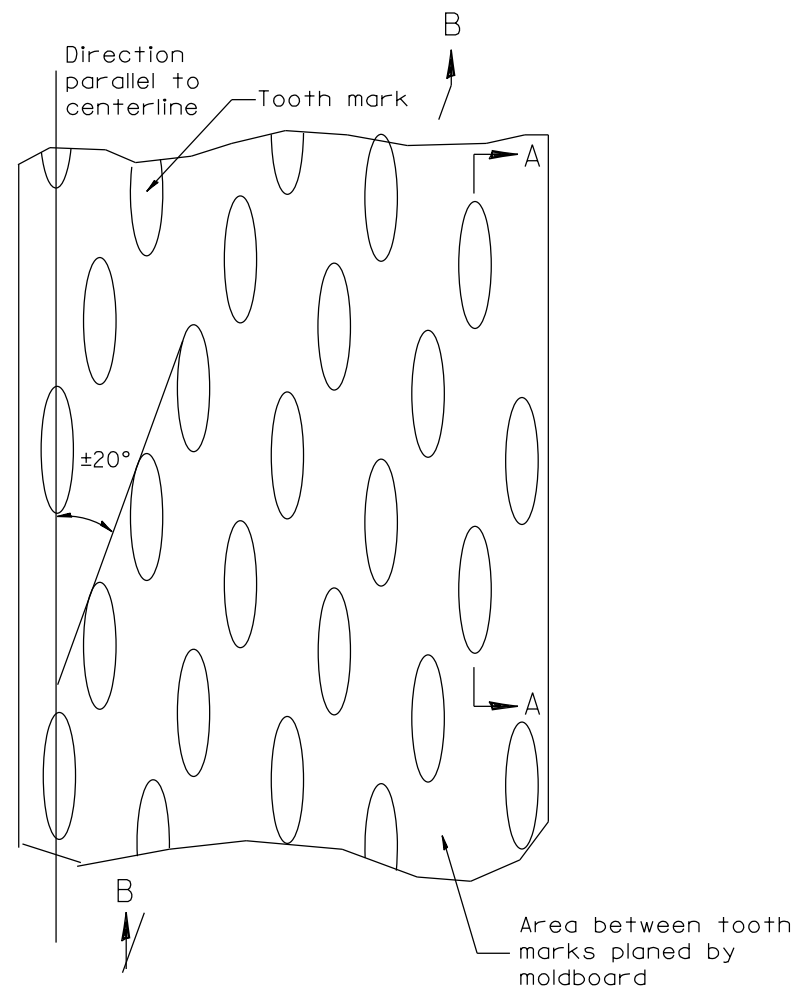
NOT TO SCALE

URBAN ENTRANCES ACCESSIBLE TO THE DISABLED,
FOR CURBLINE SIDEWALKS

SHT. 2 OF 2
CADD STD. 423101-D4

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	BRS-8;(6,5)RS-3	PEORIA	48	43
CONTRACT NO. 68F12				

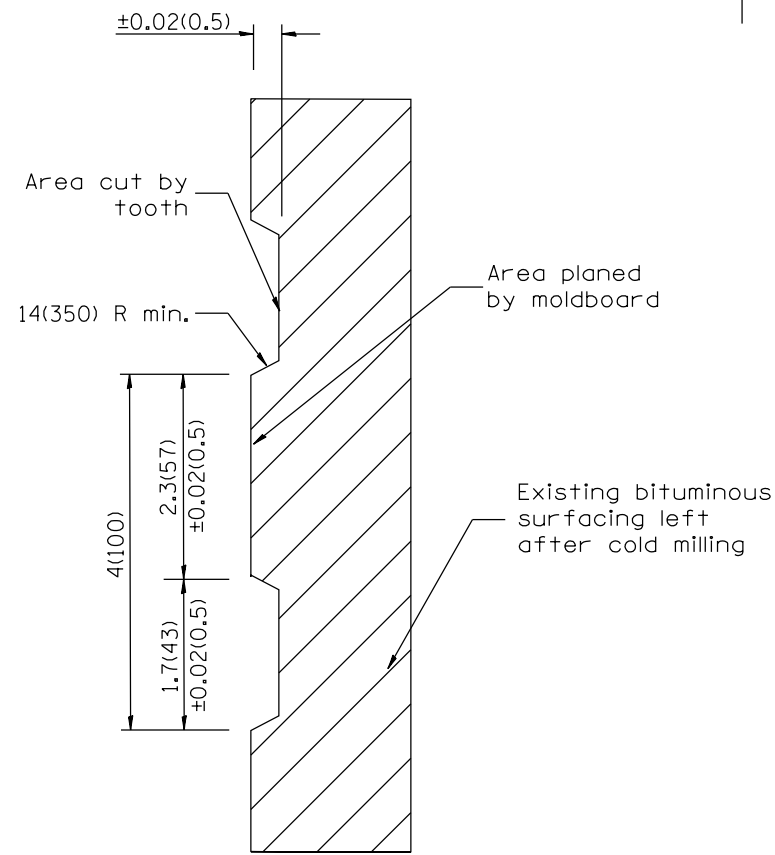
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



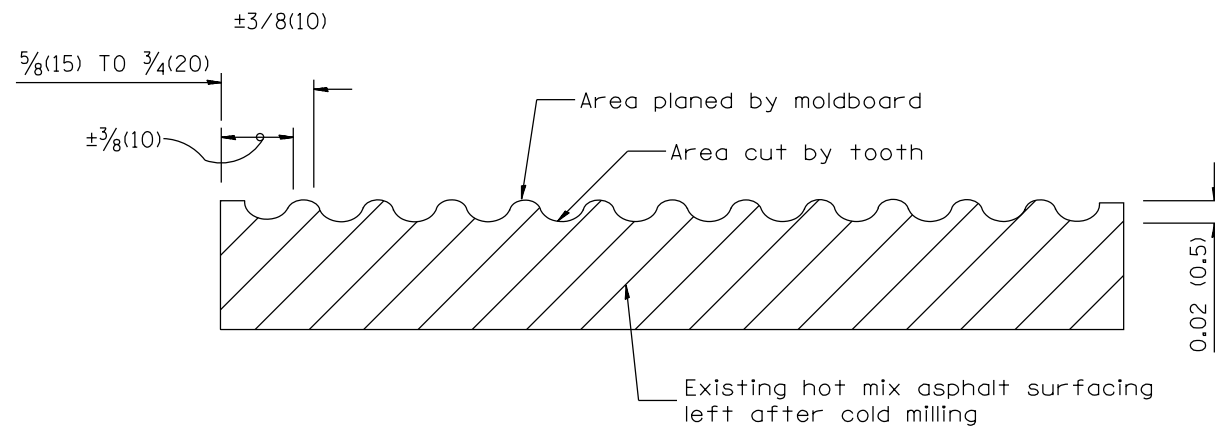
PLAN

General notes:

1. Coldmilling shall consist of two processes: Cutting with carbide teeth mounted on a rotating drum, and planing with a moldboard mounted immediately behind the cutting drum.
2. Other similar patterns will be acceptable if they consist of a smooth, flat, planed surface interspersed with a pattern of discontinuous longitudinal striations.



SECTION A-A



SECTION B-B PROJECTED PERPENDICULAR TO CENTERLINE

All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-104.01, NEW REVISION BOX	T.P.
04-20-98	REMOVED MILLING DETAIL FROM STANDARD	J.A.
09-08-98	CORRECT NOTE LEADER PLACEMENT	R.W.
10-16-06	REVISED TO 2007 SPEC.	M.A.

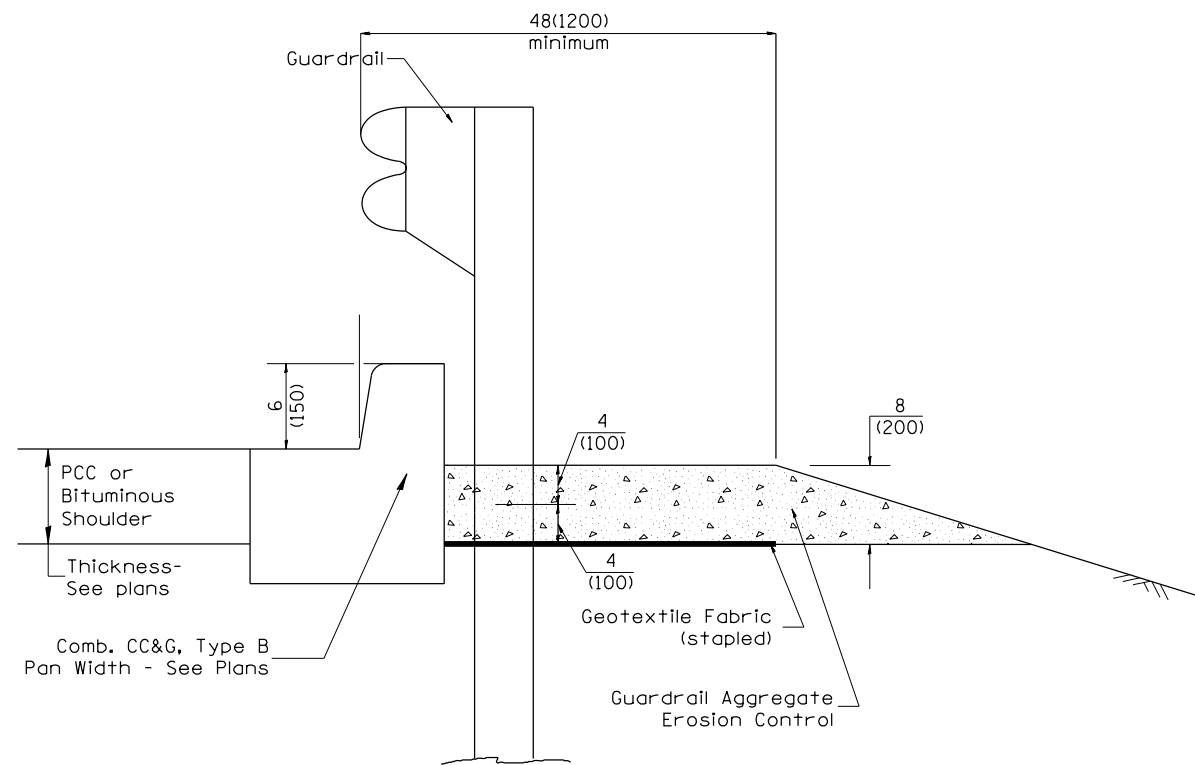
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

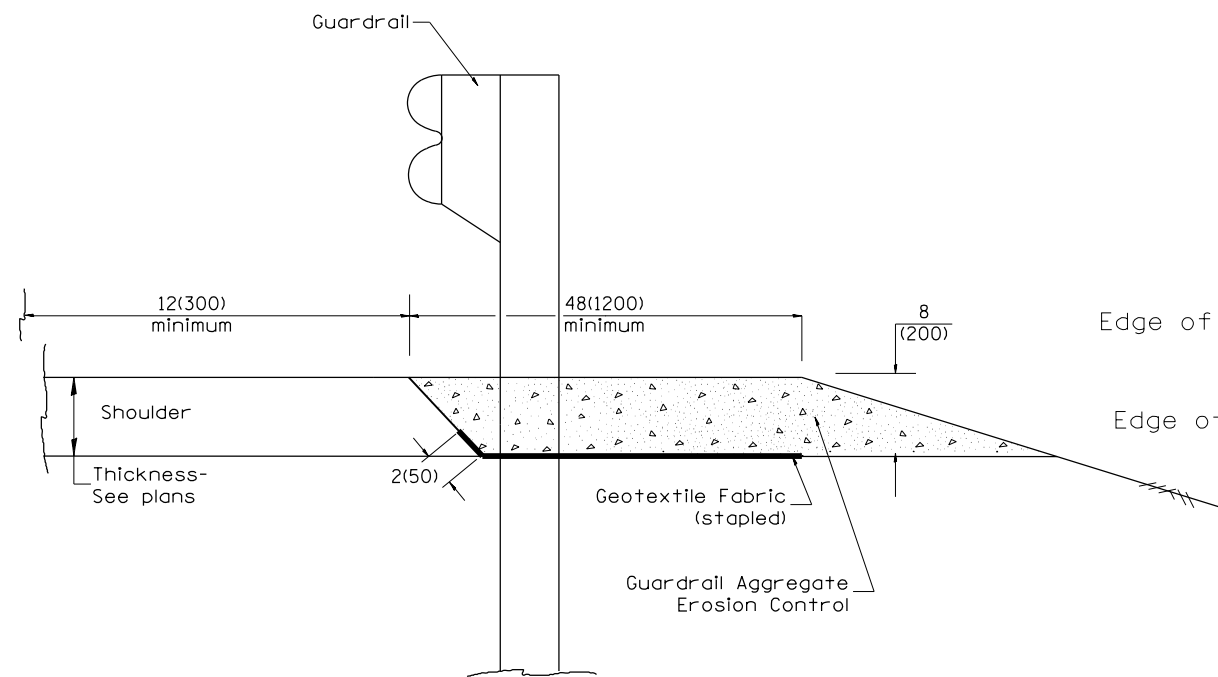
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CADD STD. 440001-D4

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	BRS-8;(6,5)RS-3	PEORIA	48	44
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68F12	



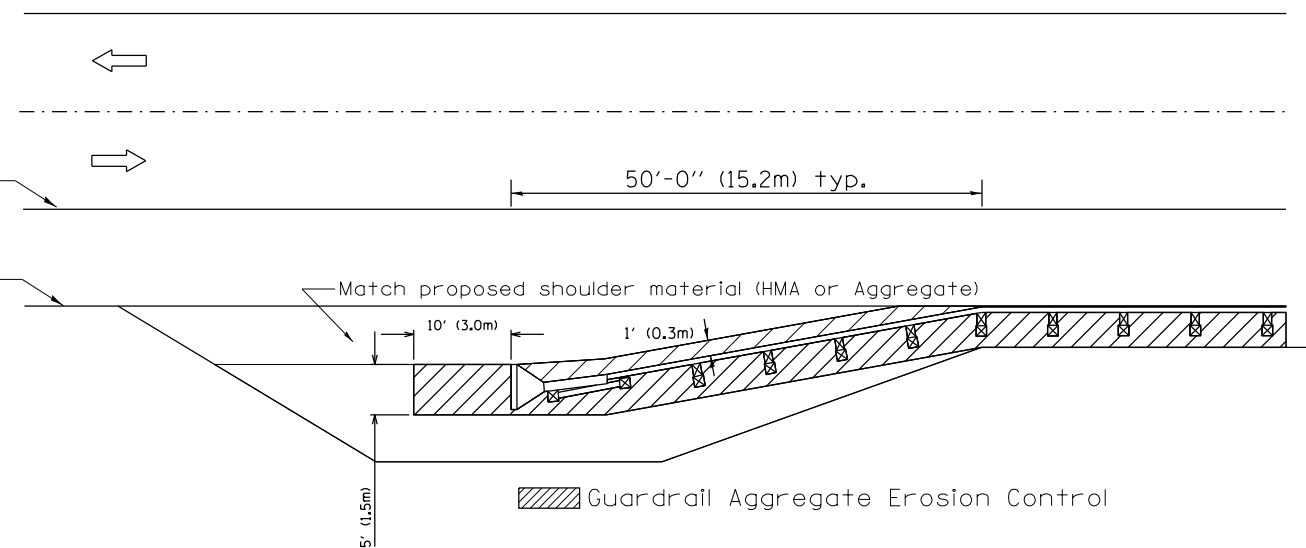
TYPICAL SECTION WITH COMBINATION CONCRETE CURB & GUTTER



TYPICAL SECTION WITHOUT EROSION CONTROL CURB

GENERAL NOTES: GUARDRAIL AGGREGATE EROSION CONTROL

1. This work shall consist of grading as needed, furnishing and installing geotextile fabric and staples, and furnishing, placing and shaping crushed aggregate around and behind Steel Plate Beam Guardrail posts in accordance with Plan Details.
2. Before placing the aggregate and the Geotextile Fabric, weeds and grass shall be removed from the area to be covered.
3. After the area has been prepared, and in a dry condition, the Geotextile fabric shall be placed with a 12(300) minimum overlap. A knife cut for guardrail post installation is necessary.
4. The aggregate shall be deposited, compacted and shaped by either mechanical or hand methods, in a manner reasonably true to line and grade.
5. The Contractor shall have the option of placing the guardrail before or after the Geotextile Fabric and Aggregate are in place. If the guardrail is placed after the Geotextile Fabric and Aggregate, then any voids must be filled and the aggregate returned to line and grade.
6. Materials shall meet the following requirements:
 - A. The crushed aggregate shall be CA1 gradation in accordance with Article 1004.01(c) of the Standard Specifications.
 - B. The Geotextile Fabric shall be nonwoven fabric in accordance with Article 1080.02 of the Standard Specifications.



All dimensions are in inches (millimeters) unless otherwise noted.

03-07-11	ADDED DETAIL SHOWING PLAN VIEW	R.D.	5-30-18	CHANGE B CURB TO CC&G	R.D.
08-10-12	REVISED CURB "B" AND AGGREGATE	R.D.	07-16-19	SPELLING CORRECTIONS	R.D.
07-15-15	ADDRESSED SHOULDER INLET CURB	R.D.			
01-26-17	REVISED	R.D.			

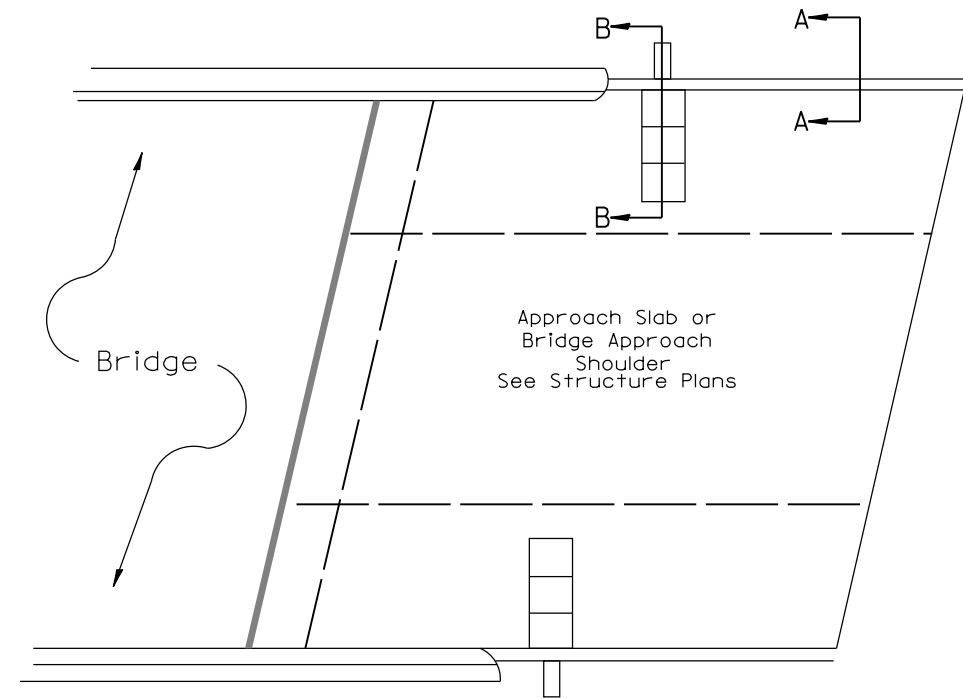
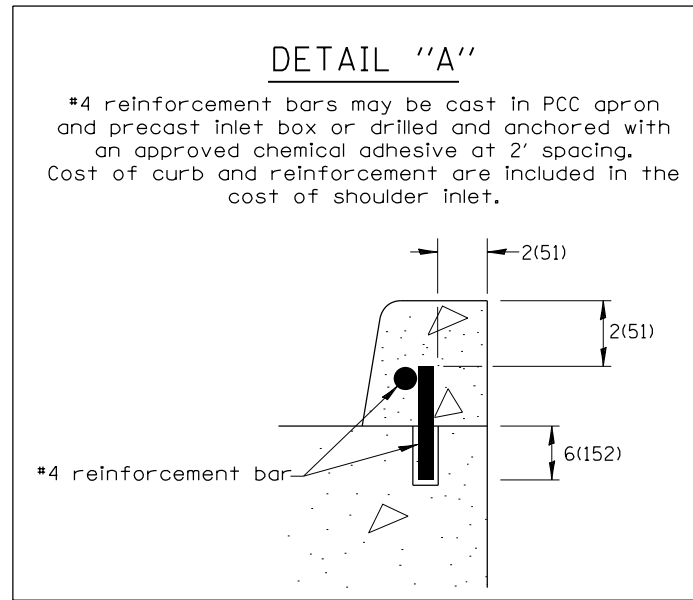
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GUARDRAIL EROSION CONTROL TREATMENTS

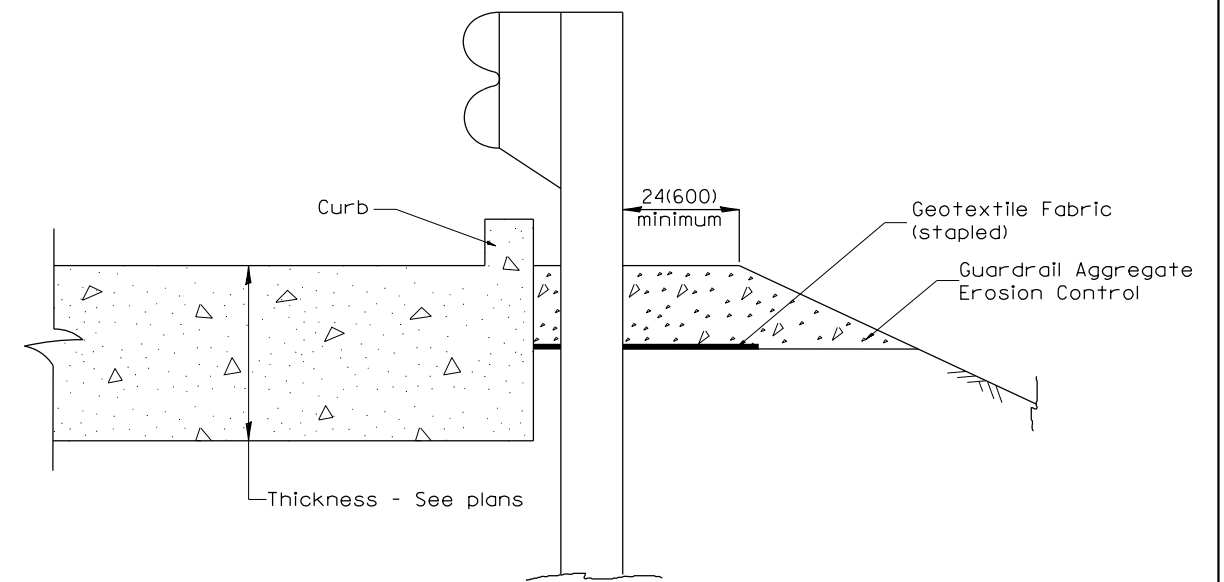
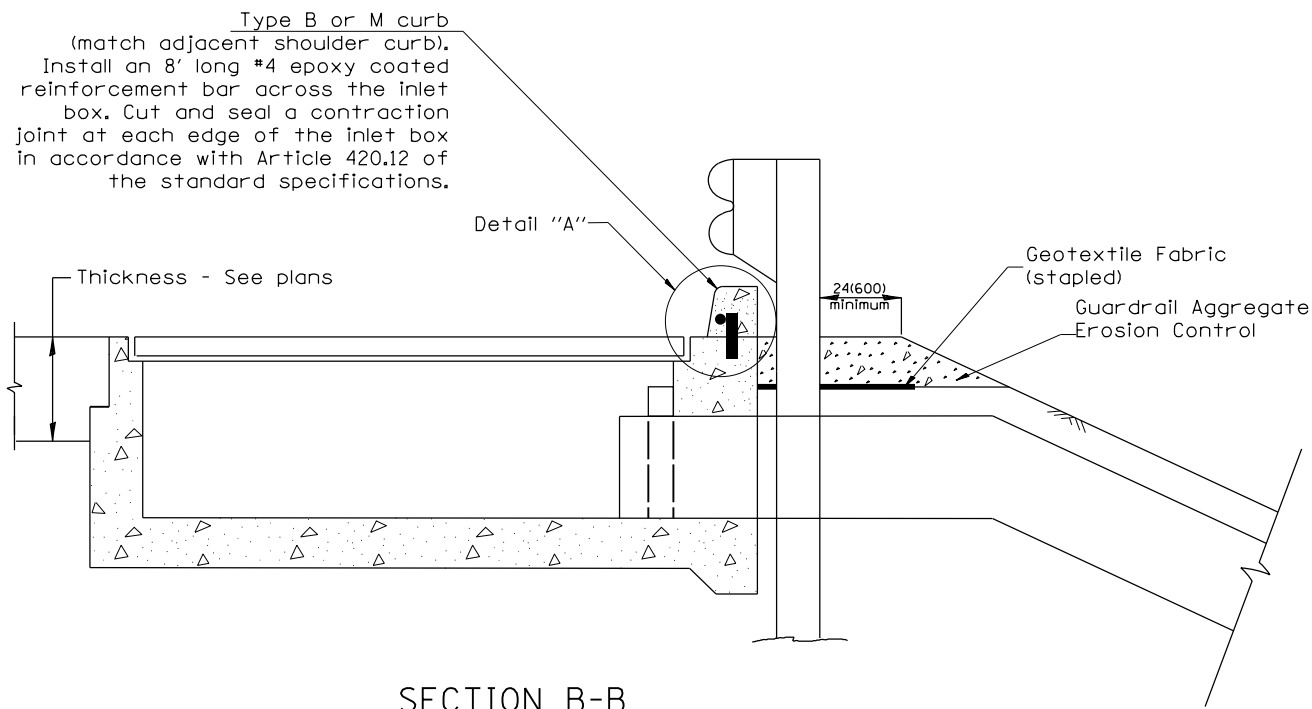
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SHT. 1 OF 2
CADD STD. 630101-D4

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	BRS-8;(6,5)RS-3	PEORIA	48	45
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68F12	



PLAN VIEW
APPROACH SLAB OR SHOULDER PLACEMENT

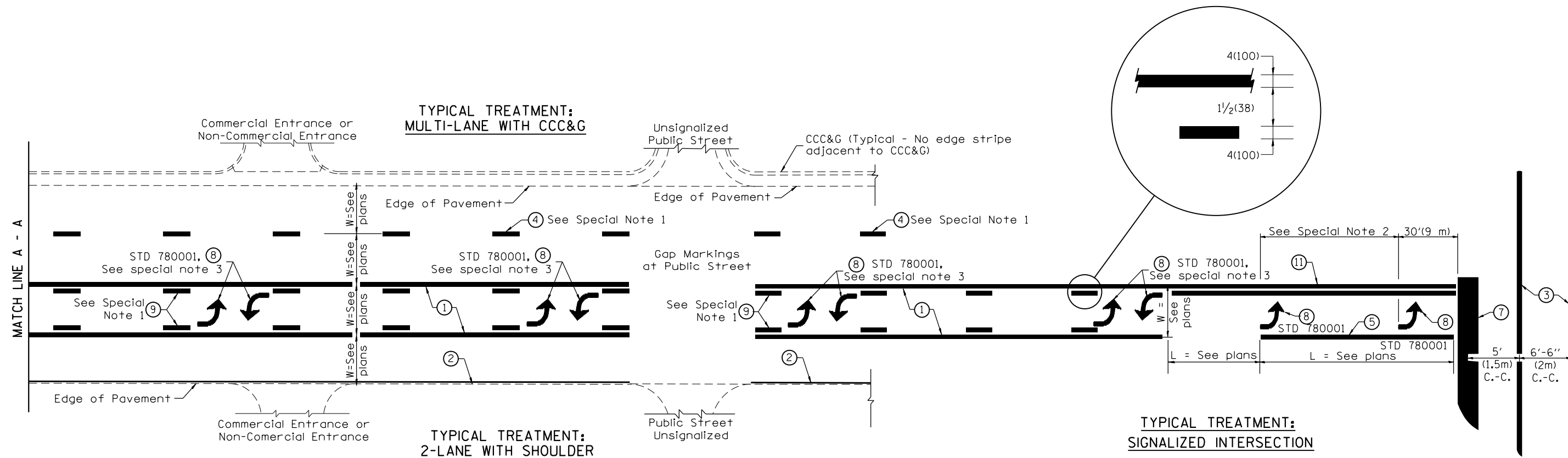


All dimensions are in inches (millimeters) unless otherwise noted.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				GUARDRAIL EROSION CONTROL TREATMENTS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
								318	BRS-8;(6,5)RS-3	PEORIA	48	46
								CONTRACT NO. 68F12				
								FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

NOT TO SCALE

SHT. 2 OF 2
CADD STD. 630101-D4



FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION

TYPICAL PAVEMENT MARKING LEGEND

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A) ⑩
- ⑪ 4(100) Double Solid (Yellow) ⑪

SPECIAL NOTES

1. Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
2. The following shall apply to arrows located in one-way left turn lanes:
 - A. A minimum of two (2) arrows is required.
 - B. The maximum spacing between arrows is 80' (24 m).
 - C. Arrows shall be evenly spaced if three (3) or more are required.
3. The following shall apply to arrow pairs located in two-way left turn lanes:
 - A. A minimum of two (2) arrow pairs is required.
 - B. The maximum spacing between arrow pairs is 200' (61 m).
 - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
 - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

GENERAL NOTES

1. Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
2. See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
3. Refer to Article 780.13 for letter, number and symbol areas (sq. ft.)
4. Areas are grooved 1" beyond each edge for the following symbols:
Through Arrow= 14.8 sq. ft.
Large Left or Right Arrow= 21.9 sq. ft.
2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.
Wrong Way Arrow= 29.5 sq. ft.
Railroad Crossing Symbol= 69.8 sq. ft.
(For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

01-01-97	RENUM. F-8.03, NEW REVISION BOX	T.P.	10-16-06	REVISED TO 2007 SPEC.	
02-07-97	ADD BI DIRECTIONAL DIMENSION	J.A.	2/29/16	ADDED GROOVING AREAS	R.D.
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.	07-16-19	SPELLING CORRECTIONS	R.D.
08-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.			

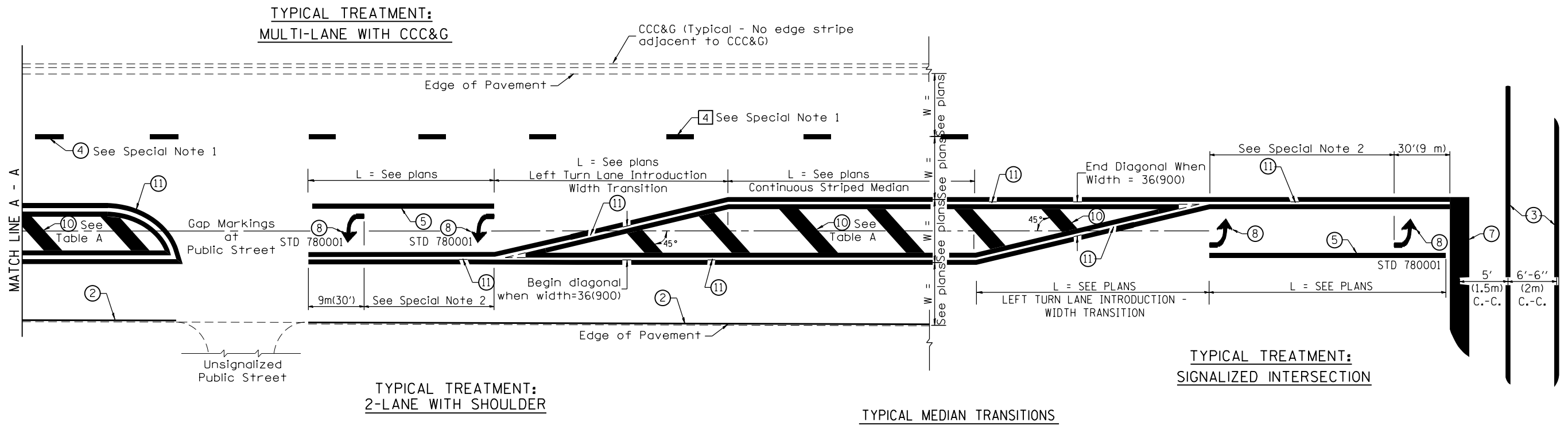
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

NOT TO SCALE

TYPICAL PAVEMENT MARKINGS

SHT. 1 OF 2
CADD STD. 780001-D4

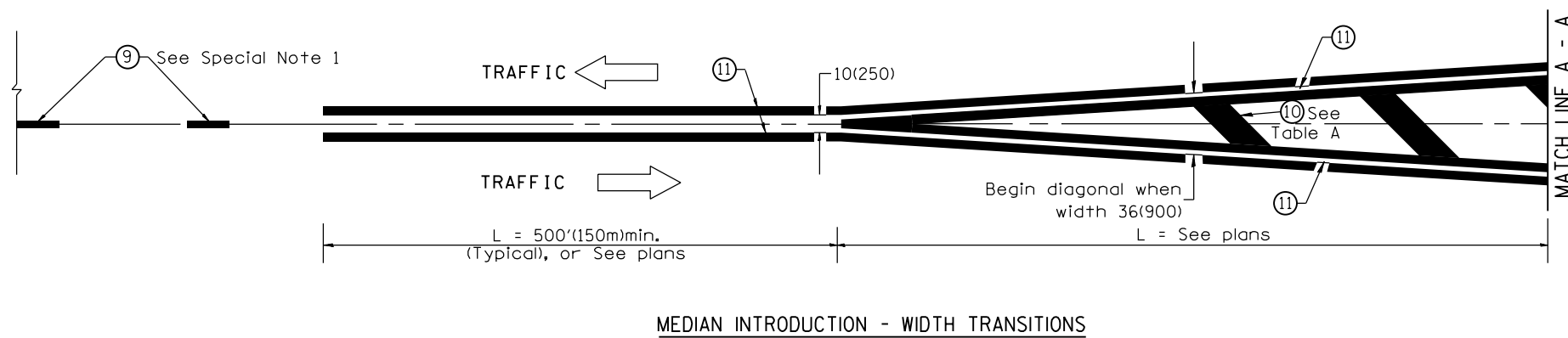
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	BRS-8;6,5RS-3	PEORIA	48	47
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68F12	



FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE

TABLE A
RECOMMENDED SPACING BETWEEN DIAGONAL LINES

SPEED LIMIT RANGE	INTERSECTION CHANNELIZATION (Includes Width Transitions for Median and Left Turn Lane Introductions)	
	CONTINUOUS	
Less Than 30 mph (50 km/h)	50' (15m)	15' (5m)
30 - 45 mph (50 - 70 km/h)	75' (23m)	20' (6m)
Over 45 mph (70 km/h)	150' (46m)	30' (9m)



All dimensions are in inches (millimeters) unless otherwise noted.