

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020 117 RS & SW	COOK	51	1
		ILLINOIS	CONTRACT NO. 62L95	

**PROPOSED
HIGHWAY PLANS**

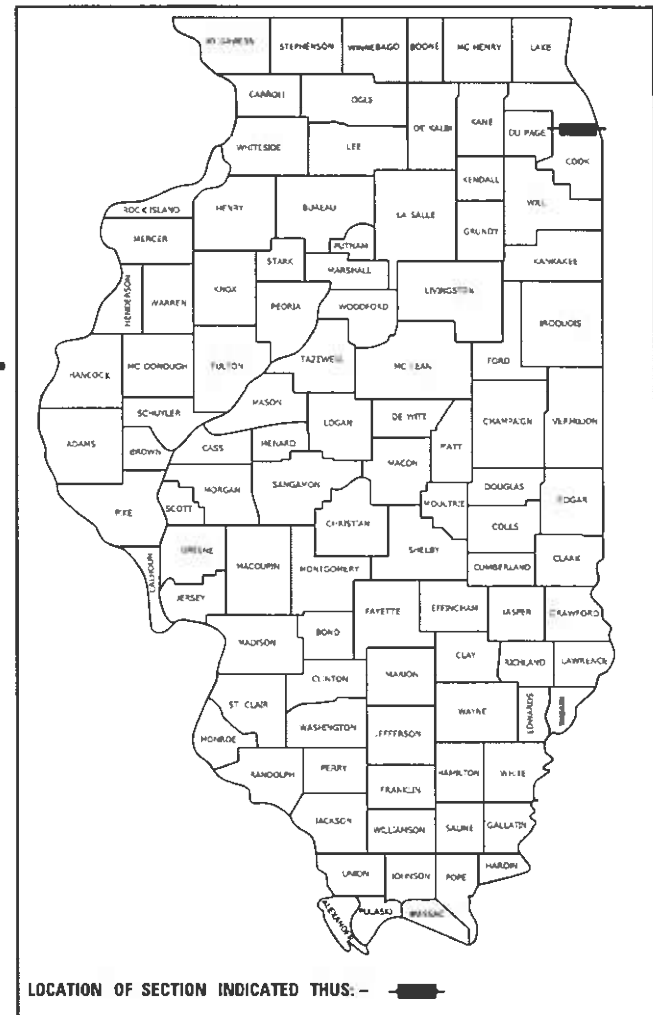
FAU ROUTE 1453 (CERMAK RD.)
FROM KEDZIE AVE. TO DAMEN AVE AND ASHLAND AVE. TO HALSTED ST.
SECTION 2020-117-RS & SW PROJECT STP- RJ5C (541)

SMART OVERLAY/ADA IMPROVEMENTS
COOK COUNTY
C-91-314-20

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

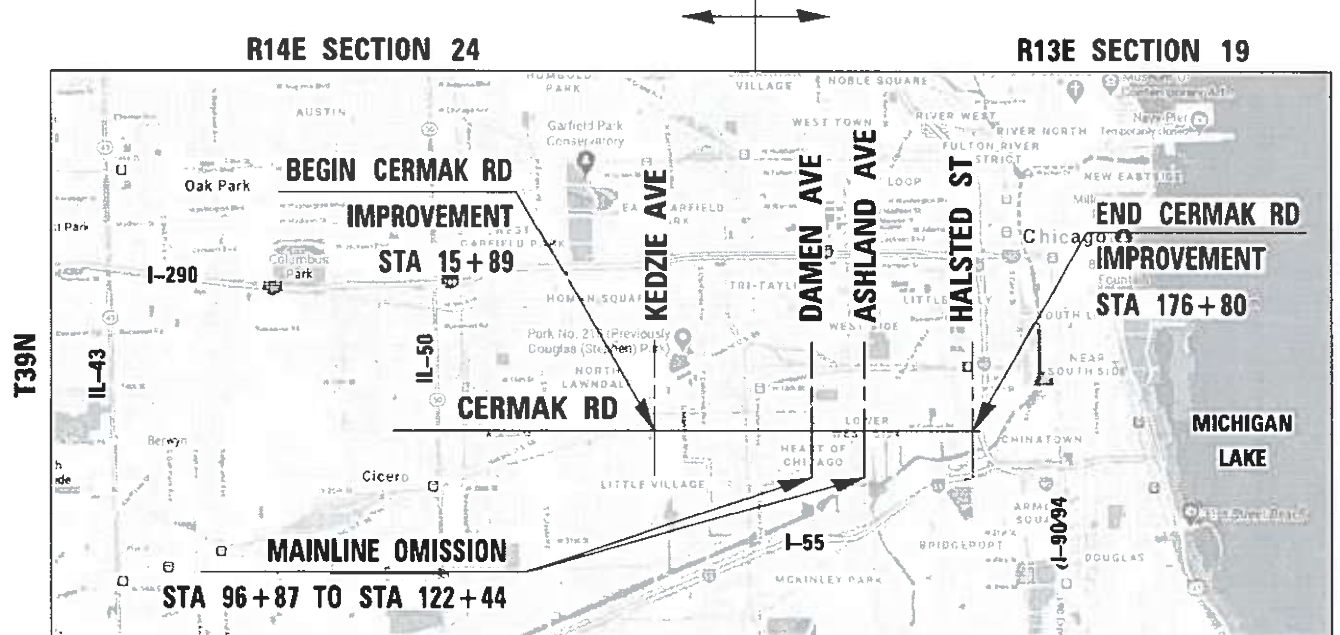
FUNCTIONAL CLASSIFICATION:
MINOR ARTERIAL
POSTED SPEED LIMIT = 30 MPH
KEDZIE TO DAMEN ASHLAND TO HALSTED
2018 ADT=14,800 2018 ADT = 14,800

D-91-516-20

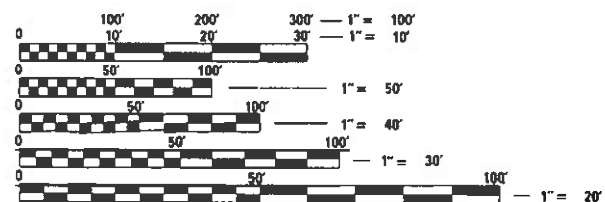


LOCATION OF SECTION INDICATED THUS: - [Symbol]

PROJECT LOCATED IN THE
CITY OF CHICAGO



WEST TOWNSHP
LOCATION MAP
NOT TO SCALE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N.
CHICAGO UTILITY ALERT NETWORK
1-312-744-700

PROJECT ENGINEER: DAN WILGREEN (847-705-4240)
PROJECT MANAGER: FAWAD AQUEEL (847-705-4247)

KEDZIE TO HALSTED GROSS LENGTH = 3.05 MILES = (16,091 FT.)
KEDZIE TO HALSTED NET LENGTH = 2.56 MILES = (13,534 FT.)

PROFESSIONAL ENGINEER'S SIGN & SEAL
FOR SHEETS: ALL SHEETS
Joseph A. Regis
JOSEPH A. REGIS, P.E.
EXPIRES 11-30-2021



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
SUBMITTED December 9, 2020
Anthony J. Quigley, Regional Engineer
January 29, 2021
Joseph A. Regis, Engineer of Design and Environment
January 29, 2021
Director of Highways Project Implementation

CONTRACT NO. 62L95

AMES Engineering, Inc.
CONSULTING ENGINEERS
6330 Belmont Road, Suite 4B
Downers Grove, IL 60515

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OF THE STATE OF ILLINOIS

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HIGHWAY STANDARDS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PERPENDICULAR CURB AND RAMPS FOR SIDEWALKS
424021-06	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHLD STRIPS/SHLDS WITH RESUFACING OR WIDENING AND RESURFACING PROJECTS
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701101-05	OFF ROAD OPERATIONS, MULTILANE, 15' TO 24 FROM PAVEMENT EDGE
701201-05	LANE CLOSURE, 2L, 2W DAY ONLY FOR SPEEDS > 45 MPH
701206-05	LANE CLOSURE, 2L, 2W NIGHT ONLY FOR SPEEDS > 45MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS DAY ONLY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "C.U.A.N." AT 312-744-7000 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).
2. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, CTA AND CITY OF CHICAGO,
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
6. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
7. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1.5 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (45 KM/H) OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (45 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H) OR A NOTCHED LONGITUDINAL WEDGE IS USED.
8. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
9. THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.
10. THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD ENGINEER VIA EMAIL AT EMAD.ALHUSSEINI@ILLINOIS.GOV, A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKING.
11. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE ALL PAVEMENT MARKING FOR THE PURPOSE OF RE-INSTALLING PAVEMENT MARKING.
12. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
13. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
14. LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
15. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
16. FRAMES AND LIDS ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
17. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN.HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
18. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
19. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE REMOVAL OF PAVEMENT MARKING TAPE, TYPE III SHALL BE PAID FOR AS SHORT TERM PAVEMENT MARKING REMOVAL.
20. SIDEWALK RAMPS MODIFICATIONS WITHIN THE LIMITS OF THE PROJECT SHALL CONFORM TO THE APPLICABLE HIGHWAY STANDARDS INCLUDED IN THE PLANS.
21. THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT
22. ALL CITY WATER VALVE VAULTS AND BOXES MUST BE RAMPED AND NOT RECEIVE SPECIAL ADJUSTMENTS DURING PAVING OPERATIONS.
23. EXPOSED RAILROAD TRACKS IN PAVEMENT SHALL BE REMOVED AND THEN PATCHED. AN ALLOWANCE OF 50 LF HAS BEEN ADDED TO THE CONTRACT QUANTITIES.

MODEL: 440BELNAME
FILE NAME: D:\1620\ch-engineer.dgn

AMES Engineering, Inc.
CONSULTING ENGINEERS
6330 Belmont Road, Suite 4B
Downers Grove, IL 60515

USER NAME = arosalva	DESIGNED - JAR	REVISED -
	DRAWN - MD	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED - AS	REVISED -
PLOT DATE = 12/10/2020	DATE - 12/10/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-R5 & SW	COOK	51	2
			CONTRACT NO. 62L95	
		ILLINOIS	FED. AID PROJECT	

REV-SEP

SUMMARY OF QUANTITIES						
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
				0005 80% FED 20% STATE	0005 100% STATE	
20200100	EARTH EXCAVATION	CU YD	31	31		
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	23	23		
25200110	SODDING, SALT TOLERANT	SQ YD	23	23		
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	36,188	36,188		
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	121	121		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	2,135	2,135		
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	26	26		

SUMMARY OF QUANTITIES						
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
				0005 80% FED 20% STATE	0005 100% STATE	
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	6,796	6,796		
42001300	PROTECTIVE COAT	SQ YD	603	603		
42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SQ FT	4,306	4,306		
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	79,731	79,731		
44000600	SIDEWALK REMOVAL	SQ FT	4,306	4,306		
44002208	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 2"	SQ YD	234	234		
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	25	25		

* SPECIALTY ITEM

MODEL: 440BELNAMES
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
SUMMARY OF QUANTITIES
SCALE: NTS SHEET 1 OF 5 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	3
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62L95	

SUMMARY OF QUANTITIES						
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
				0005 80% FED 20% STATE	0005 100% STATE	
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	232	232		
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	160	160		
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	182	182		
60255500	MANHOLES TO BE ADJUSTED	EACH	11	11		
60300305	FRAME AND LIDS TO BE ADJUSTED	EACH	29	29		
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	43	43		
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	5	5		

SUMMARY OF QUANTITIES						
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
				0005 80% FED 20% STATE	0005 100% STATE	
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1		
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1		
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	15	15		
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12		
67100100	MOBILIZATION	L SUM	1	1		
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1		
70100455	TRAFFIC CONTROL AND PROTECTION, STANDARD 701206	L SUM	1	1		

* SPECIALTY ITEM

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
SUMMARY OF QUANTITIES

SCALE: NTS SHEET 2 OF 5 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	4
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62L95	

SUMMARY OF QUANTITIES						
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
				0005 80% FED 20% STATE	0005 100% STATE	
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1		
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1		
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1		
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1		
70300100	SHORT TERM PAVEMENT MARKING	FOOT	20,835	20,835		
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	6,945	6,945		
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	1,405	1,405		

SUMMARY OF QUANTITIES						
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
				0005 80% FED 20% STATE	0005 100% STATE	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	75,139	75,139		
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2,985	2,985		
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1,669	1,669		
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	14,391	14,391		
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	10,418	10,418		
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1,405	1,405		
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	70,970	70,970		

* SPECIALTY ITEM

MODEL: 1400BELNAMES
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		DATE - 12/10/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)			
SUMMARY OF QUANTITIES			
SCALE: NTS	SHEET 3 OF 5 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	5
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62L95	

SUMMARY OF QUANTITIES						
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
				0005 80% FED 20% STATE	0005 100% STATE	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2,985	2,985		
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1,430	1,430		
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	14,391	14,391		
* 78006100	PREFORMED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	210	210		
* 78009004	MODIFIED URETHANE PAVMENT MARKING - LINE 4"	FOOT	4,169	4,169		
* 78009012	MODIFIED URETHANE PAVMENT MARKING - LINE 12"	FOOT	239	239		
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1,266	1,266		

SUMMARY OF QUANTITIES						
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
				0005 80% FED 20% STATE	0005 100% STATE	
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	296	296		
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	500	500		
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1		
X0327611	REMOVE AND REINSTALL BRICK PAVER	SQ FT	260	260		
X4240800	DETECTABLE WARNINGS (SPECIAL)	SQ FT	533	533		
X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	39	39		
X5537700	STORM SEWERS TO BE CLEANED 10"	FOOT	20		20	

* SPECIALTY ITEM

MODEL: 4100BELNAMES
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
SUMMARY OF QUANTITIES**

SCALE: NTS SHEET 4 OF 5 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	6
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62L95	

SUMMARY OF QUANTITIES						
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
				0005 80% FED 20% STATE	0005 100% STATE	
X6350010	DELINEATOR FOR MEDIAN BARRIER SEPARATOR	EACH	6	6		
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	70,271	70,271		
Z0004562	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	710	710		
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	11		11	
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	110	110		
Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	38,829	38,829		
Z0038119	PORTLAND CEMENT CONCRETE SURFACE REMOVAL 1 1/2"	SQ YD	100	100		


SUMMARY OF QUANTITIES						
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
				0005 80% FED 20% STATE	0005 100% STATE	
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1		
Z0048900	RAILROAD TRACK REMOVAL	FOOT	100	100		
∅ Z0076600	TRAINEES	HOURS	500	500		
∅ Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOURS	500	500		

* SPECIALTY ITEM

∅ 0042

REV-SEP

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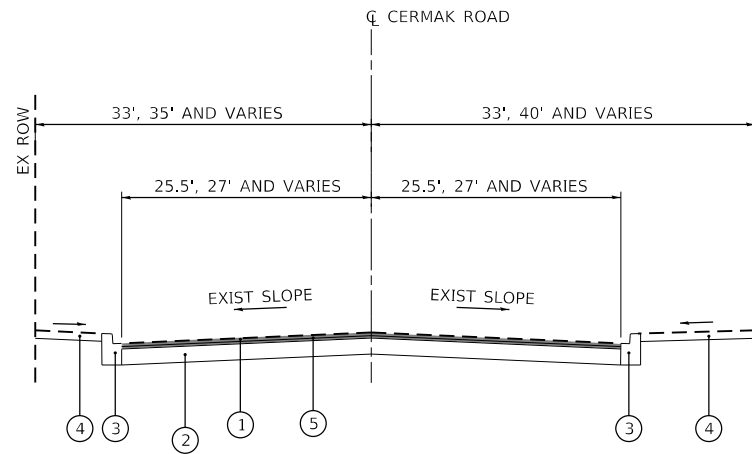
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)

SUMMARY OF QUANTITIES

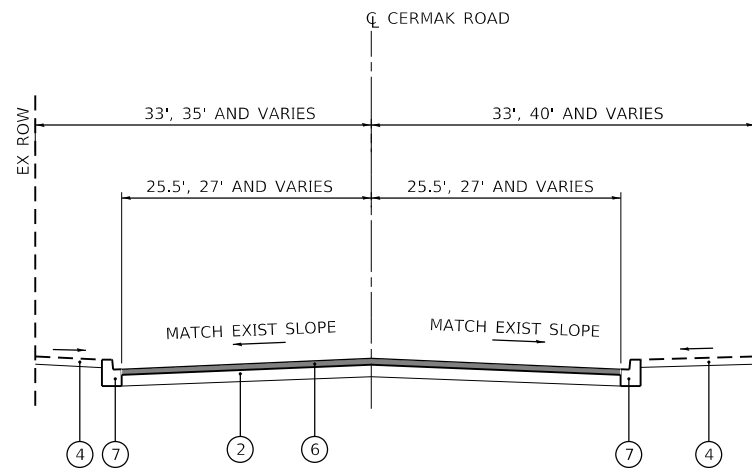
SCALE: NTS SHEET 5 OF 5 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	7
CONTRACT NO. 62L95			ILLINOIS FED. AID PROJECT	



EXISTING TYPICAL SECTION

KEDZIE AVENUE TO DAMEN AVENUE
 STA 15+89 (PROJECT START) TO
 STA 96+87 (BEGIN OMISSION)



PROPOSED TYPICAL SECTION

KEDZIE AVENUE TO DAMEN AVENUE
 STA 15+89 (PROJECT START) TO
 STA 96+87 (BEGIN OMISSION)

LEGEND:

- ① EXISTING HOT-MIX ASPHALT SURFACE COURSE, 2"±
- ② EXISTING PCC PAVEMENT 10"±
- ③ EXISTING COMBINATION CURB AND GUTTER
- ④ EXISTING CONCRETE SIDEWALK
- ⑤ PROPOSED HOT-MIX SURFACE REMOVAL - 1.5"
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" IL-9.5, N70, 1.5"
- ⑦ PROPOSED COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATIONS TO BE DETERMINED BY RESIDENT ENGINEER)
- ⑧ EXISTING STREET CAR TRACKS STRUCTURE
- ⑨ EXISTING PAVERS

HOT-MIX ASPHALT MIXTURE REQUIREMENTS			
MIXTURE USES	MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX D N70, 1.5"	4% @ 70 GYR	QCP
PATCHING	CLASS D PATCHES (HMA BINDER IL-19.0 mm)	4% @ 70 GYR	QC/QA
	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES (HMA BINDER IL-19mm)	4% @ 70 GYR	QC/QA

QMP DESIGNATED QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP)

NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR NON-POLYMERIZED HMA SHALL BE "PG 64-22" UNLESS MODIFIED BY SPECIAL PROVISIONS.
3. FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.
4. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATIONS THAT APPLIES TO THE HMA MIXTURE.

NOTES:

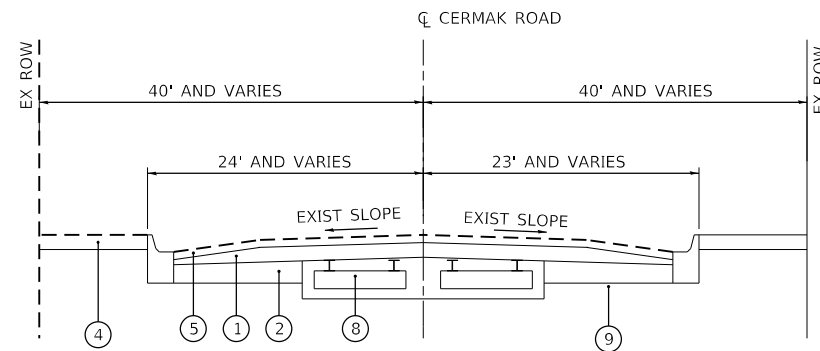
- FOR THE EXISTING HMA SURFACE, THE CONTRACTOR SHALL DO PAVEMENT PATCHING FIRST, THEN MILLING THE ROADWAY PER BD-22 DETAIL.
- HOT-MIX ASPHALT REPLACEMENT OVER PATCHES
- HOT-MIX ASPHALT REMOVAL OVER PATCHES (DEPTH EQUAL TO DEPTH OF EXISTING OVERLAY)

TYPICAL SECTION NOTES:

THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER MILLED SURFACE

EX ROW

MODEL NAME: D:\PROJECTS\2020\2020-117-RS & SW\TYPICAL SECTION.dgn

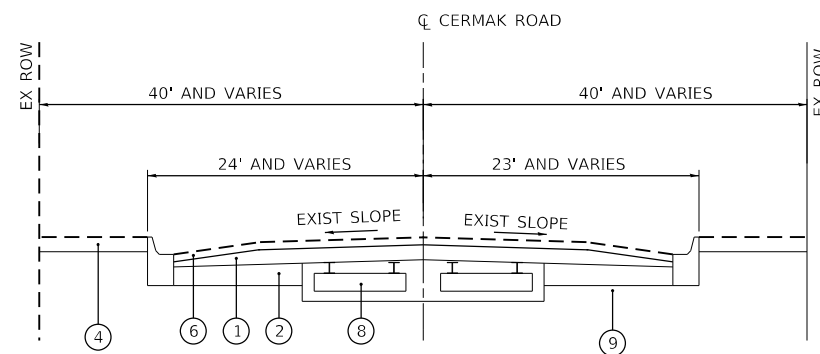


EXISTING TYPICAL SECTION

ASHLAND AVENUE TO HALSTED STREET
 STA 122+44 (END OMISSION) TO
 STA 176+80 (PROJECT END)

LEGEND:

- ① EXISTING HOT-MIX ASPHALT SURFACE COURSE, 2"±
- ② EXISTING PCC PAVEMENT 10"±
- ③ EXISTING COMBINATION CURB AND GUTTER
- ④ EXISTING CONCRETE SIDEWALK
- ⑤ PROPOSED HOT-MIX SURFACE REMOVAL - 1.5"
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" IL-9.5, N70, 1.5"
- ⑦ PROPOSED COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATIONS TO BE DETERMINED BY RESIDENT ENGINEER)
- ⑧ EXISTING STREET CAR TRACKS STRUCTURE
- ⑨ EXISTING PAVERS



PROPOSED TYPICAL SECTION

ASHLAND AVENUE TO HALSTED STREET
 STA 122+44 (END OMISSION) TO
 STA 176+80 (PROJECT END)

NOTES:

FOR THE EXISTING HMA SURFACE, THE CONTRACTOR SHALL DO PAVEMENT PATCHING FIRST, THEN MILLING THE ROADWAY PER BD-22 DETAIL.

TYPICAL SECTION NOTES:

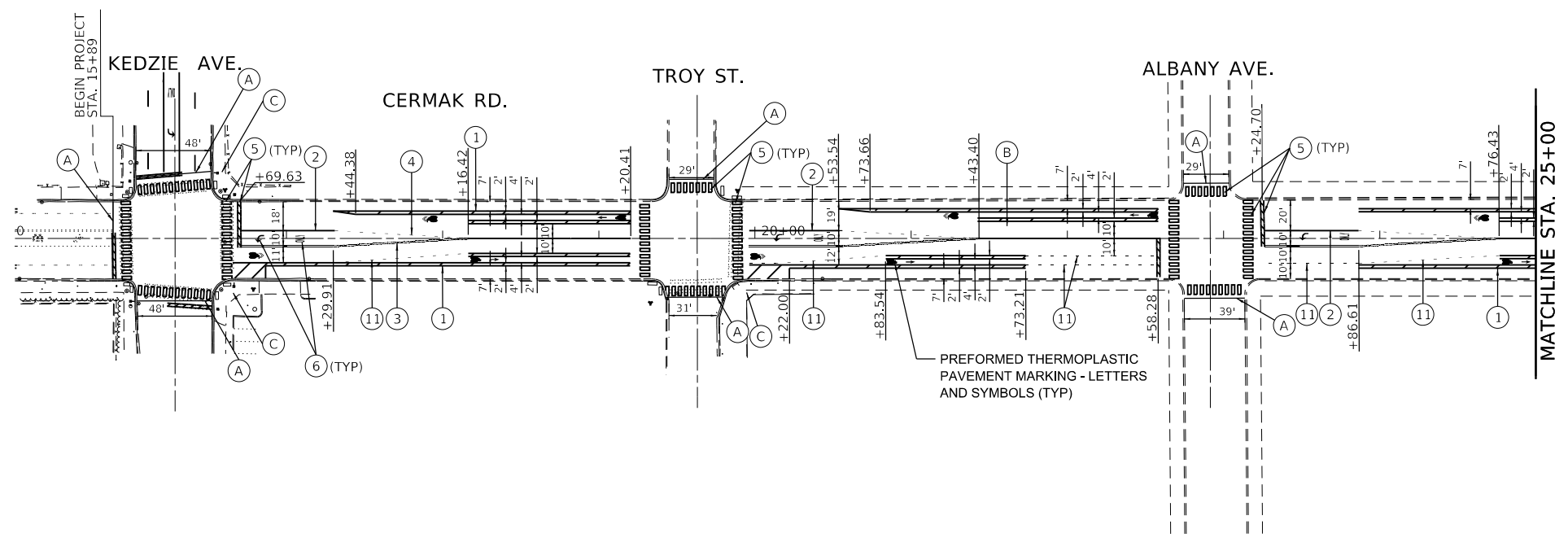
THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER MILLED SURFACE

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PLOT DATE = 12/10/2020	DATE - 12/10/2020	REVISED -

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		1453	2020-117-RS & SW	COOK	51	9
		CONTRACT NO. 62L95				
SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	

ILLINOIS	FED. AID PROJECT
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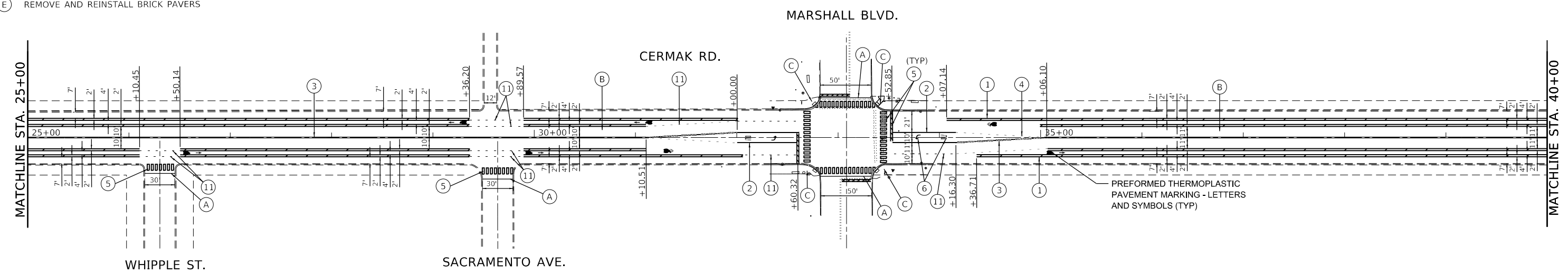


PAVEMENT MARKING LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" SOLID WHITE
- ② THERMOPLASTIC PAVEMENT MARKING - LINE 6" SOLID WHITE
- ③ THERMOPLASTIC PAVEMENT MARKING - LINE 4" DOUBLE SOLID YELLOW
- ④ THERMOPLASTIC PAVEMENT MARKING - LINE 6" 6' SKIP-2' DASH WHITE
- ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 24" WHITE
- ⑥ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (TYP)
- ⑦ THERMOPLASTIC PAVEMENT MARKING - LINE 4" SOLID WHITE 45° DIAGONAL 20' O.C.
- ⑧ THERMOPLASTIC PAVEMENT MARKING - LINE 12" SOLID YELLOW 45° DIAGONAL 20' O.C.
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- ⑩ THERMOPLASTIC PAVEMENT MARKING - LINE 6" SOLID WHITE 8' O.C.
- ⑪ THERMOPLASTIC PAVEMENT MARKING - LINE 4" 6' SKIP -2' DASH

ROADWAY LEGEND

- (A) HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT
- (B) HOT-MIX ASPHALT SURFACE REMOVAL - 1 1/2" HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D" N70
- (C) SEE DETAIL SIDEWALK PLAN
- (D) DETECTABLE WARNING REPLACEMENT
- (E) REMOVE AND REINSTALL BRICK PAVERS



MODEL: 1400BELNAMES
 FILE: 1400BELNAMES.DWG
 DATE: 12/10/2020

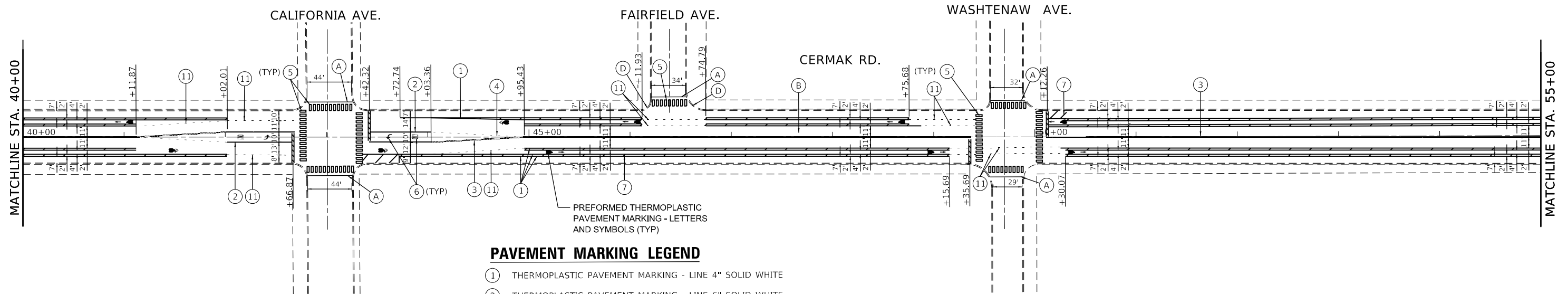
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PLOT SCALE = \$SCALE\$	DRAWN - MD	REVISED -
PLOT DATE = 12/10/2020	CHECKED - AS	REVISED -
	DATE - 12/10/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
ROADWAY AND PAVEMENT MARKING PLANS
 SCALE: 1"=50' SHEET 1 OF 5 SHEETS STA. 15+89 TO STA. 40+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	10
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				

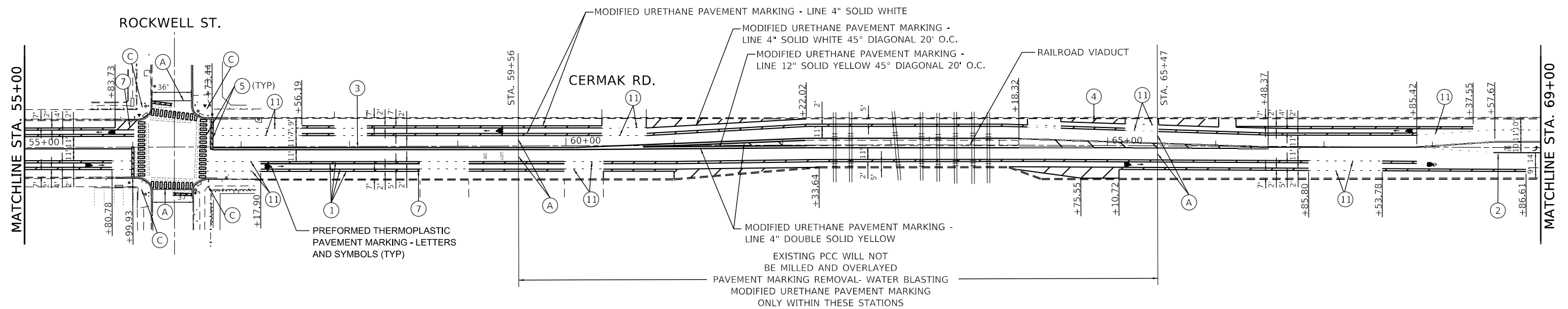


PAVEMENT MARKING LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" SOLID WHITE
- ② THERMOPLASTIC PAVEMENT MARKING - LINE 6" SOLID WHITE
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ROADWAY LEGEND

- (A) HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT
- (B) HOT-MIX ASPHALT SURFACE REMOVAL - 1 1/2" HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D" N70
- (C) SEE DETAIL SIDEWALK PLAN
- (D) DETECTABLE WARNING REPLACEMENT
- (E) REMOVE AND REINSTALL BRICK PAVERS



**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
ROADWAY AND PAVEMENT MARKING PLANS**

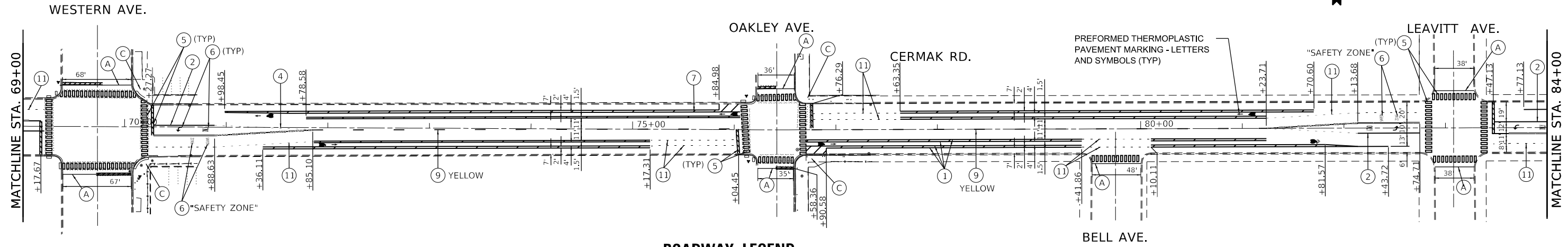
SCALE: 1"=50' SHEET 2 OF 5 SHEETS STA. 40+00 TO STA. 69+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	11
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				

MODEL: 1400BELNAMES
FILE: 1400BELN.DWG
DATE: 12/10/2020

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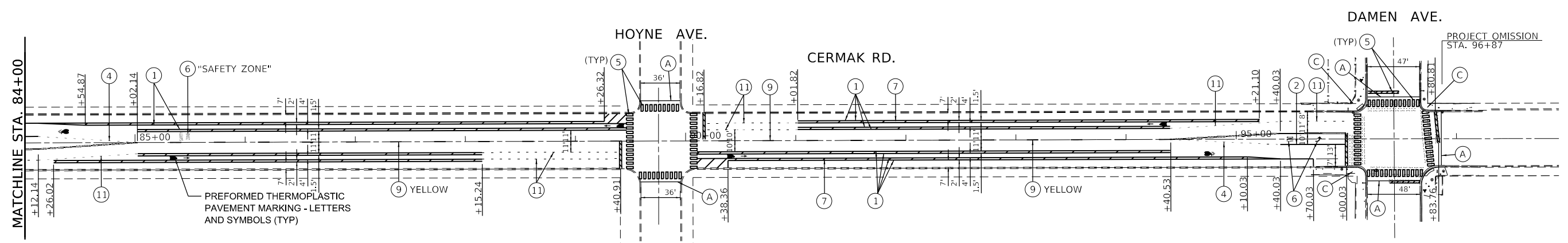


PAVEMENT MARKING LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" SOLID WHITE
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ROADWAY LEGEND

- (A) HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT
- (B) HOT-MIX ASPHALT SURFACE REMOVAL - 1 1/2" HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D" N70
- (C) SEE DETAIL SIDEWALK PLAN
- (D) DETECTABLE WARNING REPLACEMENT
- (E) REMOVE AND REINSTALL BRICK PAVERS



MODEL: 1400BELNAMES
 FILE: 1400BELNAMES.DWG
 DATE: 12/10/2020 10:50:00 AM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
ROADWAY AND PAVEMENT MARKING PLANS

SCALE: 1"=50' SHEET 3 OF 5 SHEETS STA. 69+00 TO STA. 96+87

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	12
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				

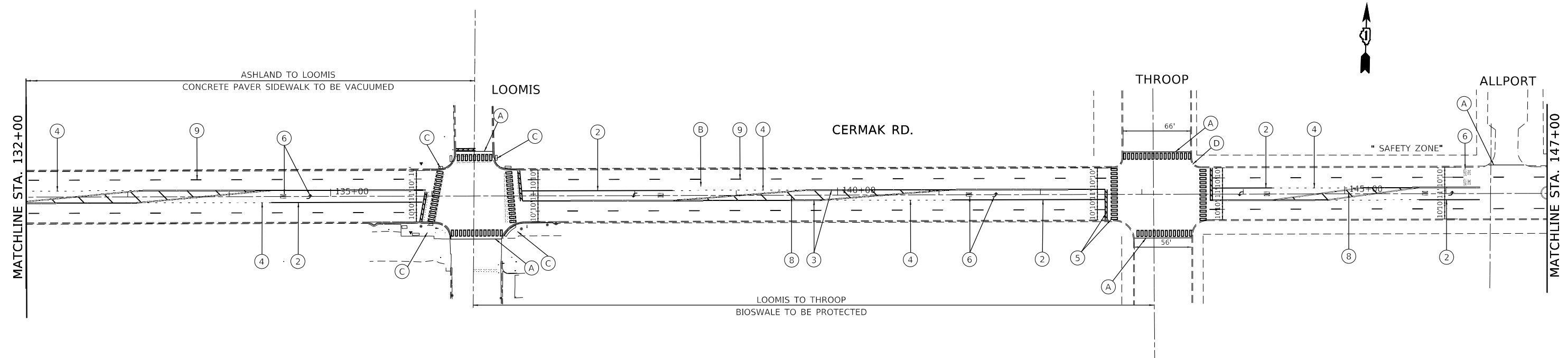
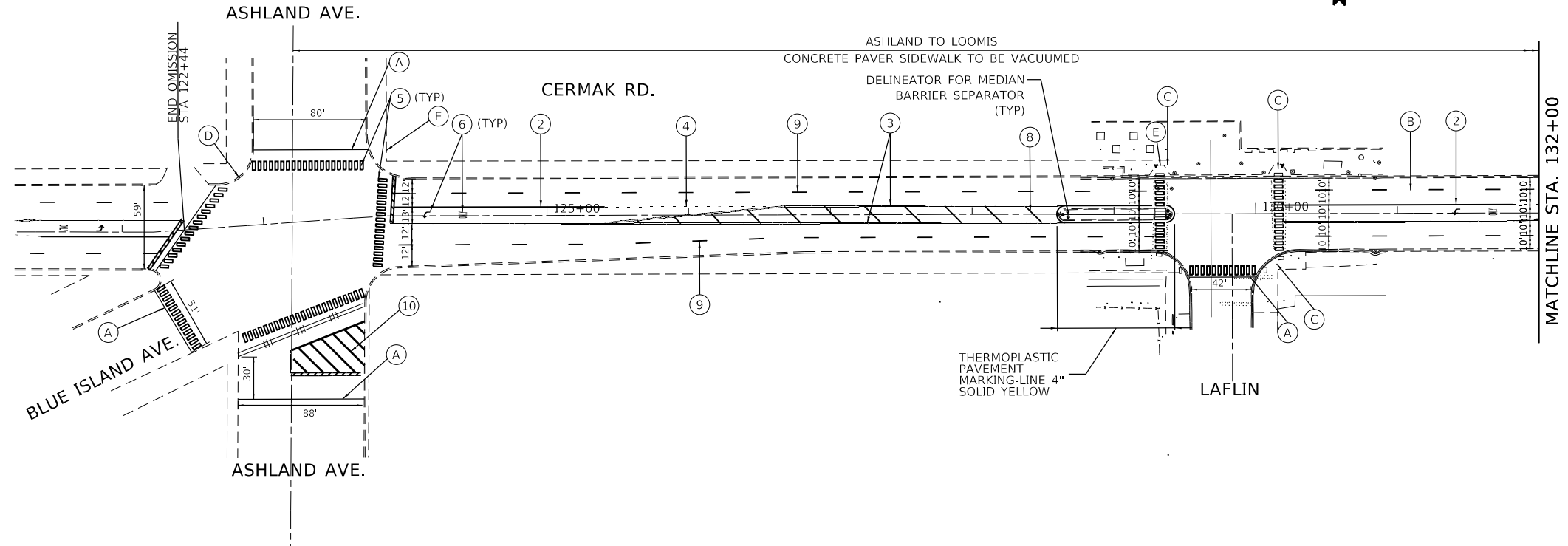


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ROADWAY LEGEND

- (A) HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT
- (B) HOT-MIX ASPHALT SURFACE REMOVAL - 1 1/2" HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D" N70
- (C) SEE DETAIL SIDEWALK PLAN
- (D) DETECTABLE WARNING REPLACEMENT
- (E) REMOVE AND REINSTALL BRICK PAVERS



MODEL: 1400BELNAMES
FILE: 1400BELNAMES.DWG
DATE: 12/10/2020

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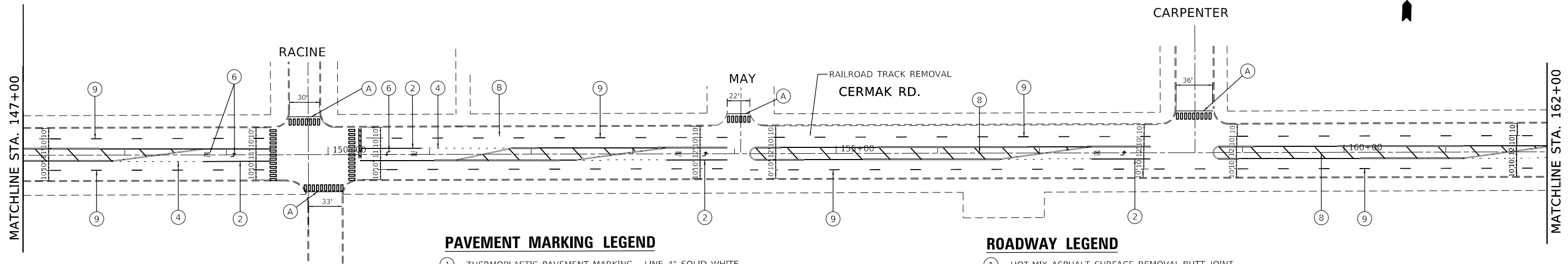
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
ROADWAY AND PAVEMENT MARKING PLANS**

SCALE: 1"=50' SHEET 4 OF 5 SHEETS STA. 122+44 TO STA. 147+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	13
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				

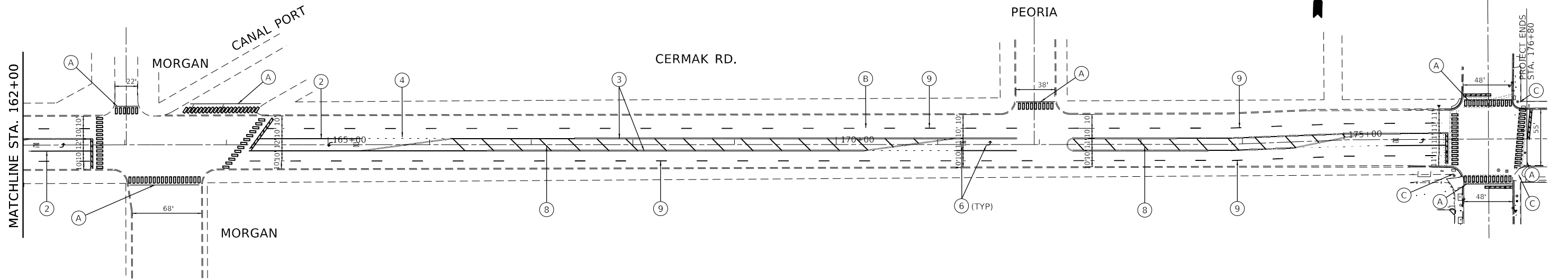


PAVEMENT MARKING LEGEND

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ROADWAY LEGEND

- (A) HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT
- (B) HOT-MIX ASPHALT SURFACE REMOVAL - 1 1/2" HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D" N70
- (C) SEE DETAIL SIDEWALK PLAN
- (D) DETECTABLE WARNING REPLACEMENT
- (E) REMOVE AND REINSTALL BRICK PAVERS



MODEL: 1409BELNAMES
 FILE: 1409BELN.DWG
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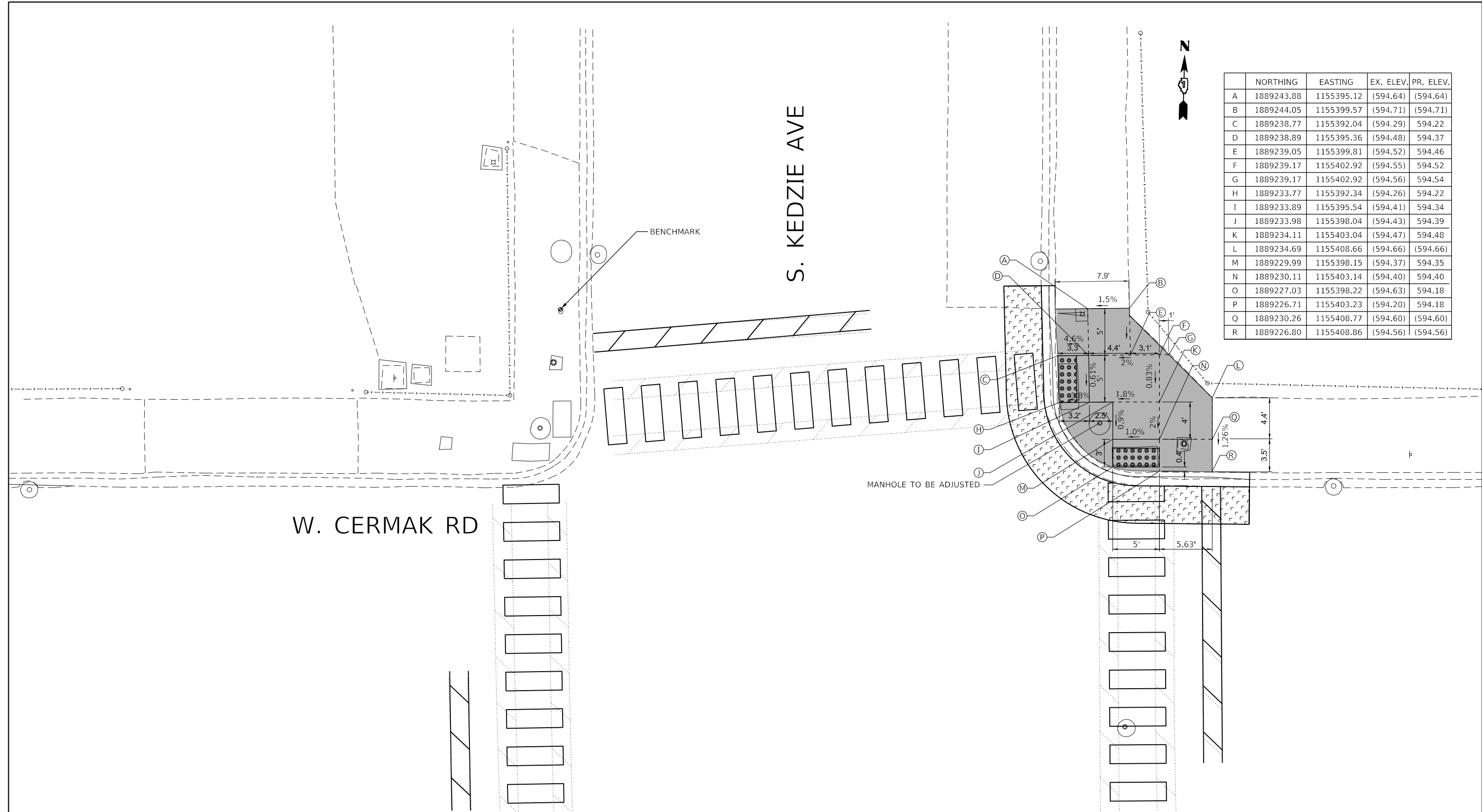
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
ROADWAY AND PAVEMENT MARKING PLANS
 SCALE: 1"=50' SHEET 5 OF 5 SHEETS STA. 147+00 TO STA. 176+80

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	14
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				



	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889243.88	1155395.12	(594.64)	(594.64)
B	1889244.05	1155399.57	(594.71)	(594.71)
C	1889238.77	1155392.04	(594.29)	594.22
D	1889238.89	1155395.36	(594.48)	594.37
E	1889239.05	1155399.81	(594.52)	594.46
F	1889239.17	1155402.92	(594.55)	594.52
G	1889239.17	1155402.92	(594.56)	594.54
H	1889233.77	1155392.34	(594.26)	594.22
I	1889233.89	1155395.54	(594.41)	594.34
J	1889233.98	1155398.04	(594.43)	594.39
K	1889234.11	1155403.04	(594.47)	594.48
L	1889234.69	1155408.66	(594.66)	(594.66)
M	1889229.99	1155398.15	(594.37)	594.35
N	1889230.11	1155403.14	(594.40)	594.40
O	1889227.03	1155398.22	(594.63)	594.18
P	1889226.71	1155403.23	(594.20)	594.18
Q	1889230.26	1155408.77	(594.60)	(594.60)
R	1889226.80	1155408.86	(594.56)	(594.56)

REFERENCE BENCHMARK TBM #120 ELEV 595.89
 BENCHMARK: TOP OF THE NORTH FLANGE BOLT OF FIRE HYDRANT
 LOCATION: NORTHWEST CORNER OF W. CERMAK ROAD & S. KEDZIE AVENUE.

LEGEND

XX.XX'	EXISTING LENGTH		PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH		CLASS D PATCH		TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
()	EXISTING ELEVATION/SLOPE		PROPOSED SIDE CURB		DETECTABLE WARNINGS		EXISTING PAVEMENT MARKING
SC	TOP OF PROPOSED SIDE CURB		COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT		SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT		PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

MODEL: STATE PLANS
 FILE NAME: D:\1020\1020\1020\1020.dwg

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PLOT SCALE = \$SCALE\$	CHECKED - AS	REVISED -
PLOT DATE = 12/10/2020	DATE - 12/10/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
SIDEWALK DETAIL PLAN

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	15
CONTRACT NO. 62L95				
SCALE: 1"=5'		SHEET OF SHEETS		STA. TO STA.

ILLINOIS FED. AID PROJECT

W. CERMAK RD

S. KEDZIE AVE

MANHOLE TO BE ADJUSTED

BENCHMARK

	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889174.54	1155409.93	(594.61)	(594.60)
B	1889170.05	1155410.06	(594.68)	(594.63)
C	1889165.72	1155410.17	(594.66)	(594.66)
D	1889165.61	1155405.19	(594.56)	594.58
E	1889169.93	1155405.06	(594.52)	594.50
F	1889172.99	1155404.97	(594.44)	594.39
G	1889174.42	1155404.93	(594.39)	594.36
H	1889172.85	1155399.97	(594.40)	594.38
I	1889169.79	1155400.06	(594.51)	594.50
J	1889167.55	1155400.13	(594.51)	594.52
K	1889154.17	1155394.59	(594.60)	(594.60)
L	1889167.41	1155395.26	(594.46)	594.42
M	1889162.38	1155394.40	(594.44)	594.41
N	1889162.41	1155395.41	(594.45)	594.43
O	1889154.40	1155402.64	(594.65)	(594.65)
P	1889162.56	1155400.28	(594.52)	594.58
Q	1889162.70	1155405.27	(594.59)	594.64
R	1889158.39	1155394.52	(594.67)	594.49
S	1889158.62	1155402.52	(594.62)	594.65

REFERENCE BENCHMARK TBM #121 ELEV 595.36

BENCHMARK: TOP OF SQUARE CUT ON CURB

LOCATION: SOUTHEAST CORNER OF W. CERMAK ROAD & S. KEDZIE AVENUE.

LEGEND

- XX.XX' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- SC TOP OF PROPOSED SIDE CURB

- PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- PROPOSED SIDE CURB
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

- CLASS D PATCH
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- EXISTING PAVEMENT MARKING
- PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
SIDEWALK DETAIL PLAN

SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	16
CONTRACT NO. 62L95				

ILLINOIS FED. AID PROJECT

MODEL: STW06L95.MXD
FILE NAME: D:\117020\117-RS-SW\62L95.dwg

AMES Engineering, Inc.
CONSULTING ENGINEERS
6330 Belmont Road, Suite 4B
Downers Grove, IL 60515

USER NAME	DESIGNED	REVISIONS
= arosalva	JAR	-
	MD	-
	AS	-
		-

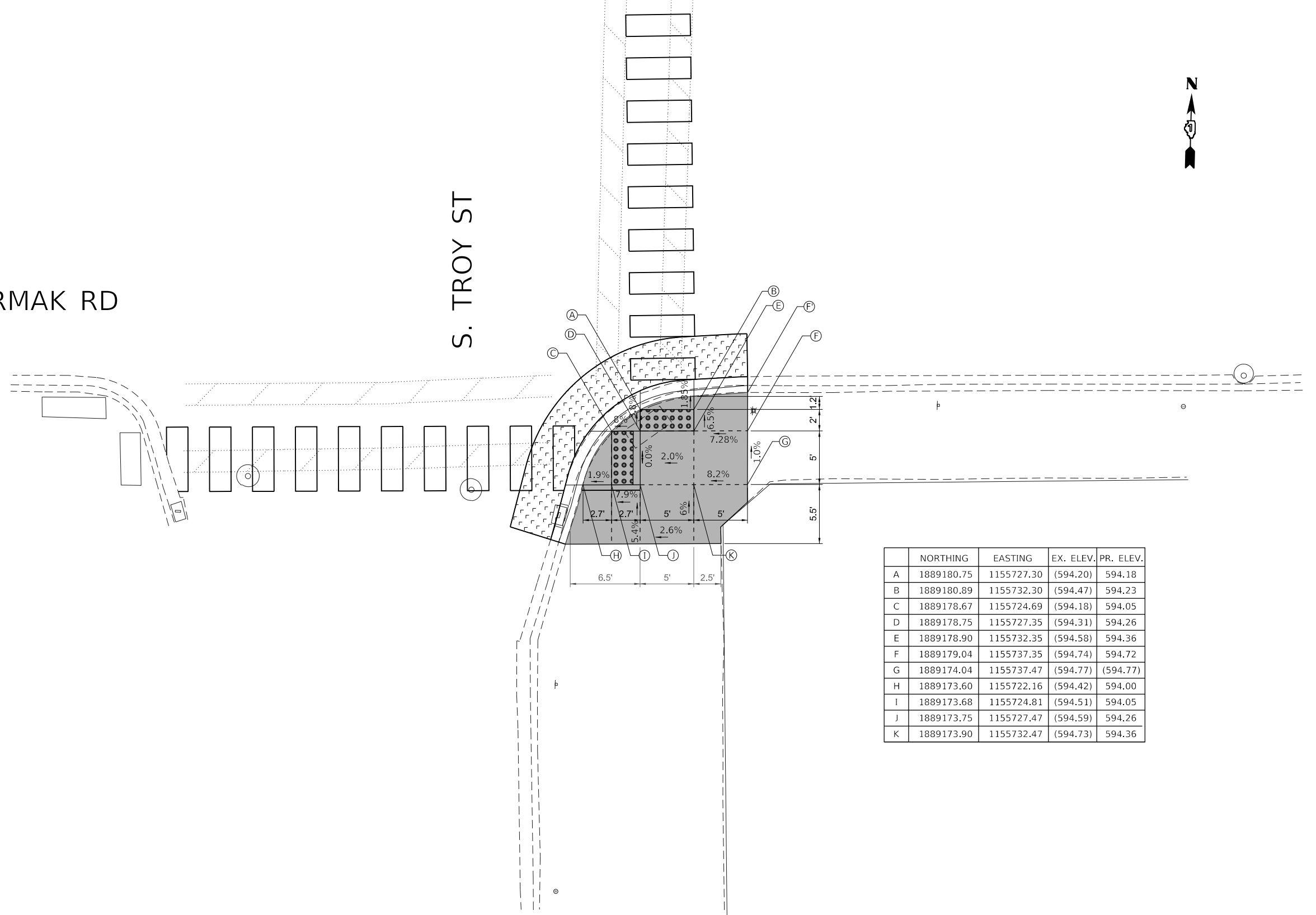
DESIGNED	DATE	REVISIONS
JAR	12/10/2020	-
MD		-
AS		-
		-

PLOT SCALE = \$SCALE\$
PLOT DATE = 12/10/2020

W. CERMAK RD

S. TROY ST

BENCHMARK



	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889180.75	1155727.30	(594.20)	594.18
B	1889180.89	1155732.30	(594.47)	594.23
C	1889178.67	1155724.69	(594.18)	594.05
D	1889178.75	1155727.35	(594.31)	594.26
E	1889178.90	1155732.35	(594.58)	594.36
F	1889179.04	1155737.35	(594.74)	594.72
G	1889174.04	1155737.47	(594.77)	(594.77)
H	1889173.60	1155722.16	(594.42)	594.00
I	1889173.68	1155724.81	(594.51)	594.05
J	1889173.75	1155727.47	(594.59)	594.26
K	1889173.90	1155732.47	(594.73)	594.36

REFERENCE BENCHMARK TBM #118 ELEV 595.41

BENCHMARK: TOP OF THE NORTH FLANGE BOLT OF FIRE HYDRANT

LOCATION: SOUTHWEST CORNER OF W. CERMAK ROAD & S. TROY STREET.

LEGEND

XX.XX'

EXISTING LENGTH

()

EXISTING ELEVATION/SLOPE

SC

TOP OF PROPOSED SIDE CURB



PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH



PROPOSED SIDE CURB



COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT



TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT



EXISTING PAVEMENT MARKING



PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

MODEL: 1408BELNAMES
FILE NAME: D:\140808\1408-ADA.dwg

AMES Engineering, Inc.
CONSULTING ENGINEERS
6330 Belmont Road, Suite 4B
Downers Grove, IL 60515

USER NAME = arosalva	DESIGNED - JAR	REVISED -
	DRAWN - MD	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED - AS	REVISED -
PLOT DATE = 12/10/2020	DATE - 12/10/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
SIDEWALK DETAIL PLAN**

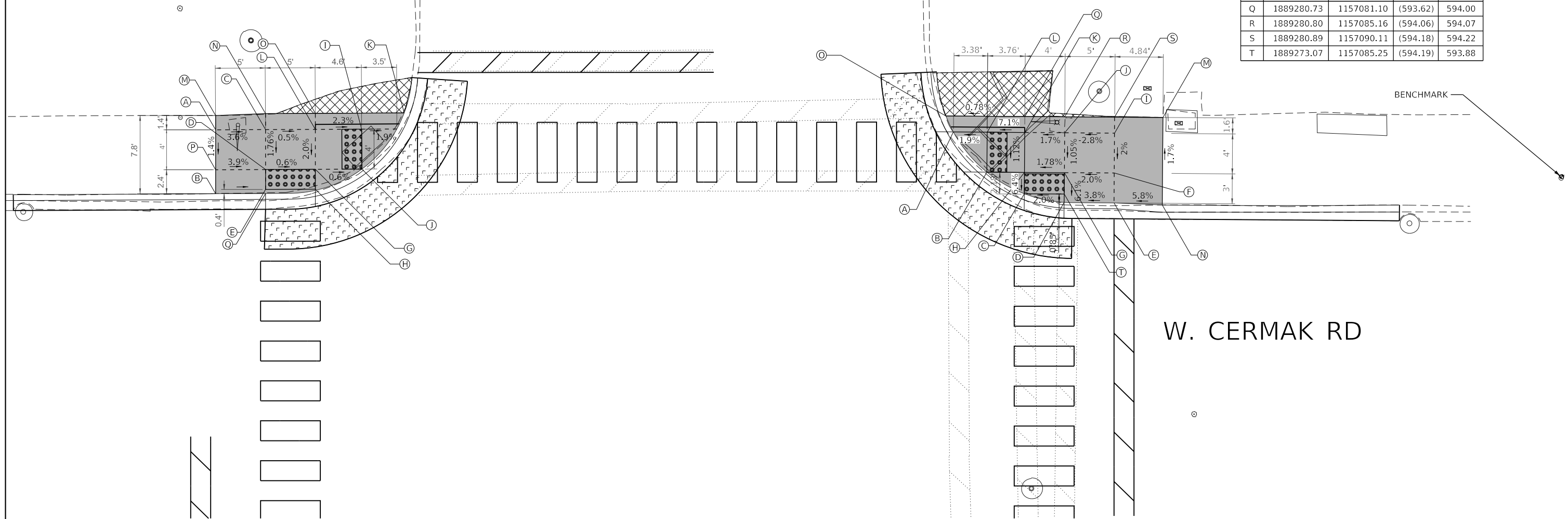
SCALE: 1"=5'

SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	17
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				

	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889277.31	1157000.08	(594.14)	(594.11)
B	1889270.93	1157000.24	(594.02)	(594.02)
C	1889277.47	1157005.08	(593.96)	593.93
D	1889273.47	1157005.18	(593.97)	593.86
E	1889271.47	1157005.23	(593.98)	593.74
F	1889277.62	1157010.07	(593.83)	593.91
G	1889273.62	1157010.18	(593.87)	593.83
H	1889271.62	1157010.23	(593.82)	593.78
I	1889277.76	1157014.71	(593.83)	593.80
J	1889273.77	1157014.83	(593.77)	593.80
K	1889279.49	1157018.93	(593.79)	593.93
L	1889277.87	1157018.23	(593.78)	593.73
M	1889278.75	1157000.04	(594.13)	(594.13)
N	1889279.07	1157005.03	(593.95)	593.96
O	1889279.22	1157010.03	(593.83)	593.94
P	1889273.32	1157000.18	(594.05)	(594.05)
Q	1889271.07	1157005.24	(593.74)	593.73

	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889278.99	1157073.99	(593.67)	593.65
B	1889275.06	1157077.45	(593.66)	593.67
C	1889272.99	1157081.25	(593.95)	593.80
D	1889272.22	1157085.27	(594.23)	593.86
E	1889272.29	1157090.27	(594.28)	594.05
F	1889275.29	1157090.21	(594.24)	594.11
G	1889275.20	1157085.21	(594.12)	594.01
H	1889275.13	1157081.21	(593.88)	593.94
I	1889279.29	1157090.14	(594.19)	594.19
J	1889279.20	1157085.14	(594.04)	594.05
K	1889279.13	1157081.14	(593.61)	593.98
L	1889279.06	1157077.38	(593.67)	593.72
M	1889280.98	1157094.95	(594.18)	(594.18)
N	1889272.21	1157095.11	(594.33)	(594.33)
O	1889280.58	1157073.20	(593.69)	593.94
Q	1889280.73	1157081.10	(593.62)	594.00
R	1889280.80	1157085.16	(594.06)	594.07
S	1889280.89	1157090.11	(594.18)	594.22
T	1889273.07	1157085.25	(594.19)	593.88



REFERENCE BENCHMARK TBM #116 ELEV 595.41
 BENCHMARK: TOP OF THE NORTH FLANGE BOLT OF FIRE HYDRANT
 LOCATION: NORTHEAST CORNER OF W. CERMAK ROAD & S. MARSHALL BLVD.

LEGEND

XX.XX'

EXISTING LENGTH

()

EXISTING ELEVATION/SLOPE

SC

TOP OF PROPOSED SIDE CURB



PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH



PROPOSED SIDE CURB



COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT



TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT



EXISTING PAVEMENT MARKING



PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

MODEL: STW06LNAME
FILE NAME: D:\102020\stwr\stwr.dwg

AMES Engineering, Inc.
 CONSULTING ENGINEERS
 6330 Belmont Road, Suite 4B
 Downers Grove, IL 60515

USER NAME = arosalva	DESIGNED - JAR	REVISED -
PLOT SCALE = \$SCALE\$	DRAWN - MD	REVISED -
PLOT DATE = 12/10/2020	CHECKED - AS	REVISED -
	DATE - 12/10/2020	REVISED -

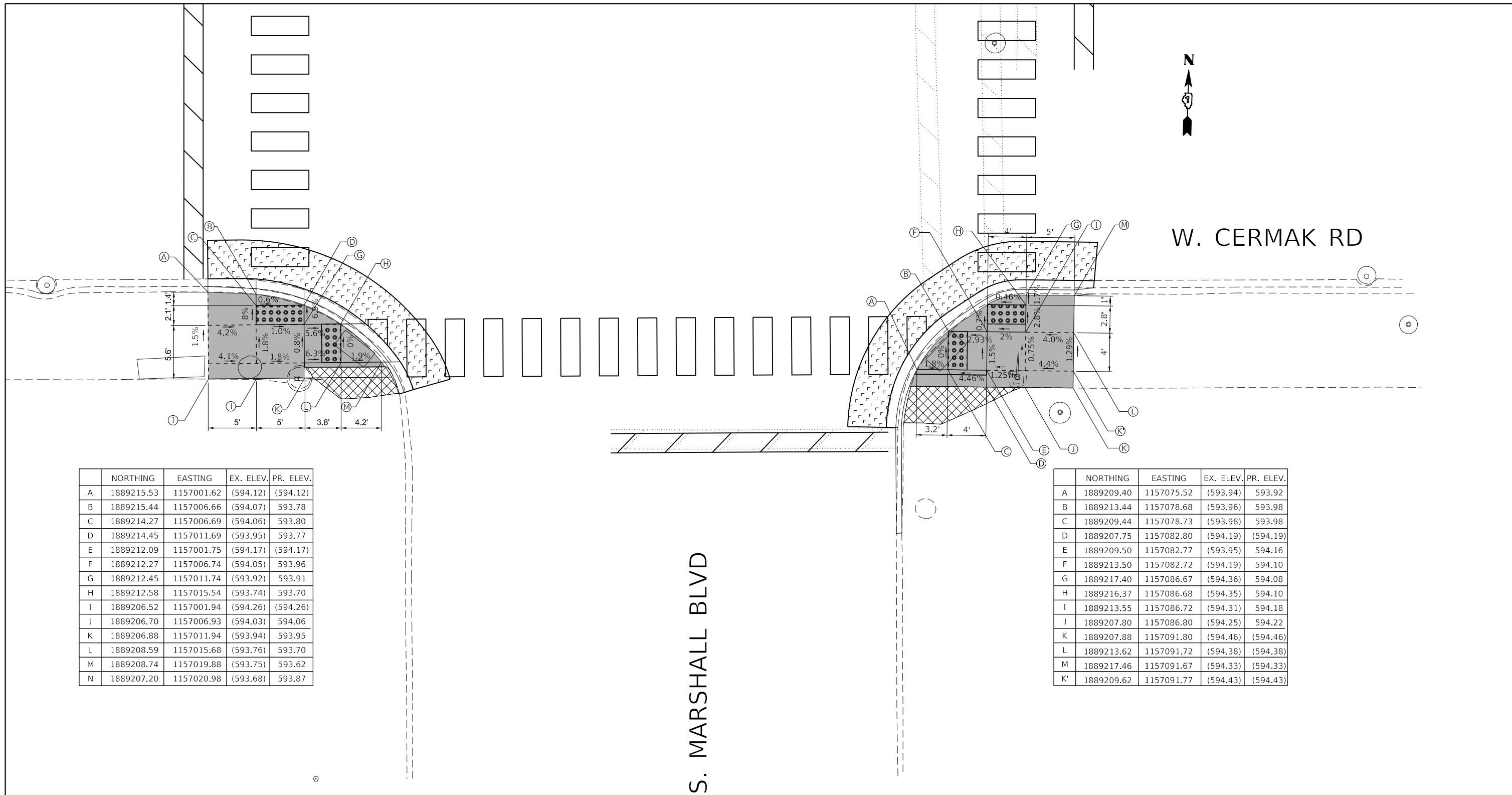
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
 SIDEWALK DETAIL PLAN**

SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	18
CONTRACT NO. 62L95				

ILLINOIS FED. AID PROJECT



	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889215.53	1157001.62	(594.12)	(594.12)
B	1889215.44	1157006.66	(594.07)	593.78
C	1889214.27	1157006.69	(594.06)	593.80
D	1889214.45	1157011.69	(593.95)	593.77
E	1889212.09	1157001.75	(594.17)	(594.17)
F	1889212.27	1157006.74	(594.05)	593.96
G	1889212.45	1157011.74	(593.92)	593.91
H	1889212.58	1157015.54	(593.74)	593.70
I	1889206.52	1157001.94	(594.26)	(594.26)
J	1889206.70	1157006.93	(594.03)	594.06
K	1889206.88	1157011.94	(593.94)	593.95
L	1889208.59	1157015.68	(593.76)	593.70
M	1889208.74	1157019.88	(593.75)	593.62
N	1889207.20	1157020.98	(593.68)	593.87

	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889209.40	1157075.52	(593.94)	593.92
B	1889213.44	1157078.68	(593.96)	593.98
C	1889209.44	1157078.73	(593.98)	593.98
D	1889207.75	1157082.80	(594.19)	(594.19)
E	1889209.50	1157082.77	(593.95)	594.16
F	1889213.50	1157082.72	(594.19)	594.10
G	1889217.40	1157086.67	(594.36)	594.08
H	1889216.37	1157086.68	(594.35)	594.10
I	1889213.55	1157086.72	(594.31)	594.18
J	1889207.80	1157086.80	(594.25)	594.22
K	1889207.88	1157091.80	(594.46)	(594.46)
L	1889213.62	1157091.72	(594.38)	(594.38)
M	1889217.46	1157091.67	(594.33)	(594.33)
K'	1889209.62	1157091.77	(594.43)	(594.43)

S. MARSHALL BLVD

W. CERMAK RD



REFERENCE BENCHMARK TBM #115 ELEV 595.55
 BENCHMARK: TOP OF THE SOUTH FLANGE BOLT OF FIRE HYDRANT
 LOCATION: SOUTHWEST CORNER OF W. CERMAK ROAD & S. MARSHALL BLVD.

LEGEND

XX.XX' EXISTING LENGTH
 () EXISTING ELEVATION/SLOPE
 SC TOP OF PROPOSED SIDE CURB

PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
 PROPOSED SIDE CURB
 COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

CLASS D PATCH
 DETECTABLE WARNINGS
 SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT

TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
 EXISTING PAVEMENT MARKING
 PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

MODEL: STW06L95M05
 FILE NAME: D:\1157020\1157020-RS-ADA.dwg
 DATE: 12/10/2020

AMES Engineering, Inc.
 CONSULTING ENGINEERS
 6330 Belmont Road, Suite 4B
 Downers Grove, IL 60515

USER NAME = arosalva	DESIGNED - JAR	REVISED -
PLOT SCALE = \$SCALE\$	DRAWN - MD	REVISED -
PLOT DATE = 12/10/2020	CHECKED - AS	REVISED -
	DATE - 12/10/2020	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

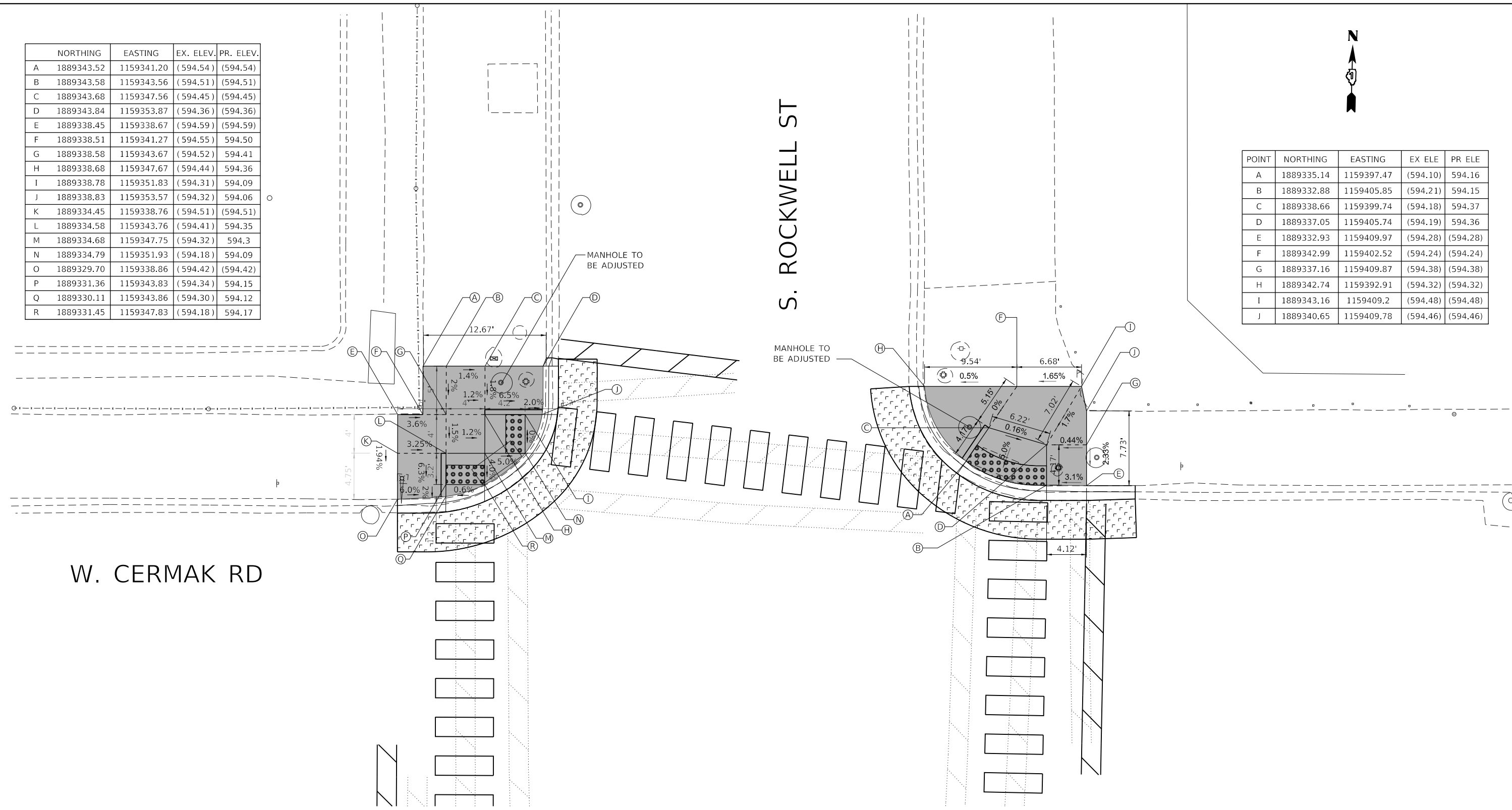
W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
 SIDEWALK DETAIL PLAN

SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	19
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				

	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889343.52	1159341.20	(594.54)	(594.54)
B	1889343.58	1159343.56	(594.51)	(594.51)
C	1889343.68	1159347.56	(594.45)	(594.45)
D	1889343.84	1159353.87	(594.36)	(594.36)
E	1889338.45	1159338.67	(594.59)	(594.59)
F	1889338.51	1159341.27	(594.55)	594.50
G	1889338.58	1159343.67	(594.52)	594.41
H	1889338.68	1159347.67	(594.44)	594.36
I	1889338.78	1159351.83	(594.31)	594.09
J	1889338.83	1159353.57	(594.32)	594.06
K	1889334.45	1159338.76	(594.51)	(594.51)
L	1889334.58	1159343.76	(594.41)	594.35
M	1889334.68	1159347.75	(594.32)	594.3
N	1889334.79	1159351.93	(594.18)	594.09
O	1889329.70	1159338.86	(594.42)	(594.42)
P	1889331.36	1159343.83	(594.34)	594.15
Q	1889330.11	1159343.86	(594.30)	594.12
R	1889331.45	1159347.83	(594.18)	594.17

POINT	NORTHING	EASTING	EX ELE	PR ELE
A	1889335.14	1159397.47	(594.10)	594.16
B	1889332.88	1159405.85	(594.21)	594.15
C	1889338.66	1159399.74	(594.18)	594.37
D	1889337.05	1159405.74	(594.19)	594.36
E	1889332.93	1159409.97	(594.28)	(594.28)
F	1889342.99	1159402.52	(594.24)	(594.24)
G	1889337.16	1159409.87	(594.38)	(594.38)
H	1889342.74	1159392.91	(594.32)	(594.32)
I	1889343.16	1159409.2	(594.48)	(594.48)
J	1889340.65	1159409.78	(594.46)	(594.46)



REFERENCE BENCHMARK TBM #114 ELEV 595.30
 BENCHMARK: TOP OF THE NORTH FLANGE BOLT OF FIRE HYDRANT
 LOCATION: NORTHWEST CORNER OF W. CERMAK ROAD & S. ROCKWELL STREET.

LEGEND

- XX.XX' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- SC TOP OF PROPOSED SIDE CURB
- PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- PROPOSED SIDE CURB
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- CLASS D PATCH
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- EXISTING PAVEMENT MARKING
- PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

MODEL: STATE OF ILLINOIS
 FILE NAME: D:\1159334\1159334-ADA.dwg

AMES Engineering, Inc.
 CONSULTING ENGINEERS
 6330 Belmont Road, Suite 4B
 Downers Grove, IL 60515

USER NAME = arosalva	DESIGNED - JAR	REVISED -
PLOT SCALE = \$SCALE\$	DRAWN - MD	REVISED -
PLOT DATE = 12/10/2020	CHECKED - AS	REVISED -
	DATE - 12/10/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

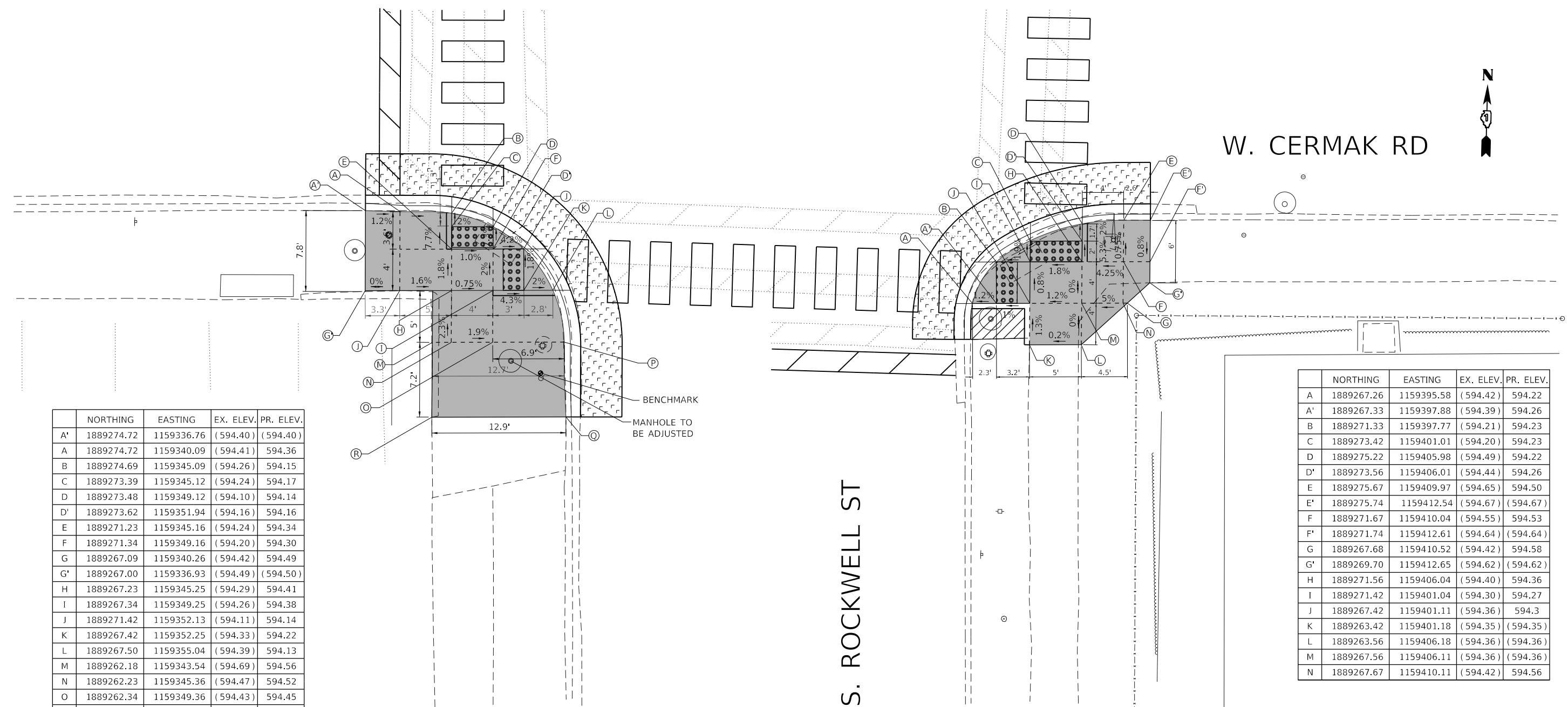
W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
SIDEWALK DETAIL PLAN
 SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	20
CONTRACT NO. 62L95				
		ILLINOIS	FED. AID PROJECT	



W. CERMAK RD

S. ROCKWELL ST



	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A'	1889274.72	1159336.76	(594.40)	(594.40)
A	1889274.72	1159340.09	(594.41)	594.36
B	1889274.69	1159345.09	(594.26)	594.15
C	1889273.39	1159345.12	(594.24)	594.17
D	1889273.48	1159349.12	(594.10)	594.14
D'	1889273.62	1159351.94	(594.16)	594.16
E	1889271.23	1159345.16	(594.24)	594.34
F	1889271.34	1159349.16	(594.20)	594.30
G	1889267.09	1159340.26	(594.42)	594.49
G'	1889267.00	1159336.93	(594.49)	(594.50)
H	1889267.23	1159345.25	(594.29)	594.41
I	1889267.34	1159349.25	(594.26)	594.38
J	1889271.42	1159352.13	(594.11)	594.14
K	1889267.42	1159352.25	(594.33)	594.22
L	1889267.50	1159355.04	(594.39)	594.13
M	1889262.18	1159343.54	(594.69)	594.56
N	1889262.23	1159345.36	(594.47)	594.52
O	1889262.34	1159349.36	(594.43)	594.45
P	1889262.53	1159356.28	(594.43)	594.31
Q	1889255.31	1159356.61	(594.36)	(594.36)
R	1889254.95	1159343.66	(594.71)	(594.71)

	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889267.26	1159395.58	(594.42)	594.22
A'	1889267.33	1159397.88	(594.39)	594.26
B	1889271.33	1159397.77	(594.21)	594.23
C	1889273.42	1159401.01	(594.20)	594.23
D	1889275.22	1159405.98	(594.49)	594.22
D'	1889273.56	1159406.01	(594.44)	594.26
E	1889275.67	1159409.97	(594.65)	594.50
E'	1889275.74	1159412.54	(594.67)	(594.67)
F	1889271.67	1159410.04	(594.55)	594.53
F'	1889271.74	1159412.61	(594.64)	(594.64)
G	1889267.68	1159410.52	(594.42)	594.58
G'	1889269.70	1159412.65	(594.62)	(594.62)
H	1889271.56	1159406.04	(594.40)	594.36
I	1889271.42	1159401.04	(594.30)	594.27
J	1889267.42	1159401.11	(594.36)	594.3
K	1889263.42	1159401.18	(594.35)	(594.35)
L	1889263.56	1159406.18	(594.36)	(594.36)
M	1889267.56	1159406.11	(594.36)	(594.36)
N	1889267.67	1159410.11	(594.42)	594.56

REFERENCE BENCHMARK TBM #113 ELEV 595.43
 BENCHMARK: TOP OF THE NORTH FLANGE BOLT OF FIRE HYDRANT
 LOCATION: SOUTHWEST CORNER OF W. CERMAK ROAD & S. ROCKWELL STREET.

LEGEND

- XX.XX' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- SC TOP OF PROPOSED SIDE CURB

- PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- PROPOSED SIDE CURB
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

- CLASS D PATCH
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT

- TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- EXISTING PAVEMENT MARKING
- PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

MODEL: 140BELNAMES
FILE NAME: D:\14050201-ADA.dwg

AMES Engineering, Inc.
 CONSULTING ENGINEERS
 6330 Belmont Road, Suite 4B
 Downers Grove, IL 60515

USER NAME = arosalva
 PLOT SCALE = \$SCALE\$
 PLOT DATE = 12/10/2020

DESIGNED - JAR
 DRAWN - MD
 CHECKED - AS
 DATE - 12/10/2020

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
SIDEWALK DETAIL PLAN
 SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

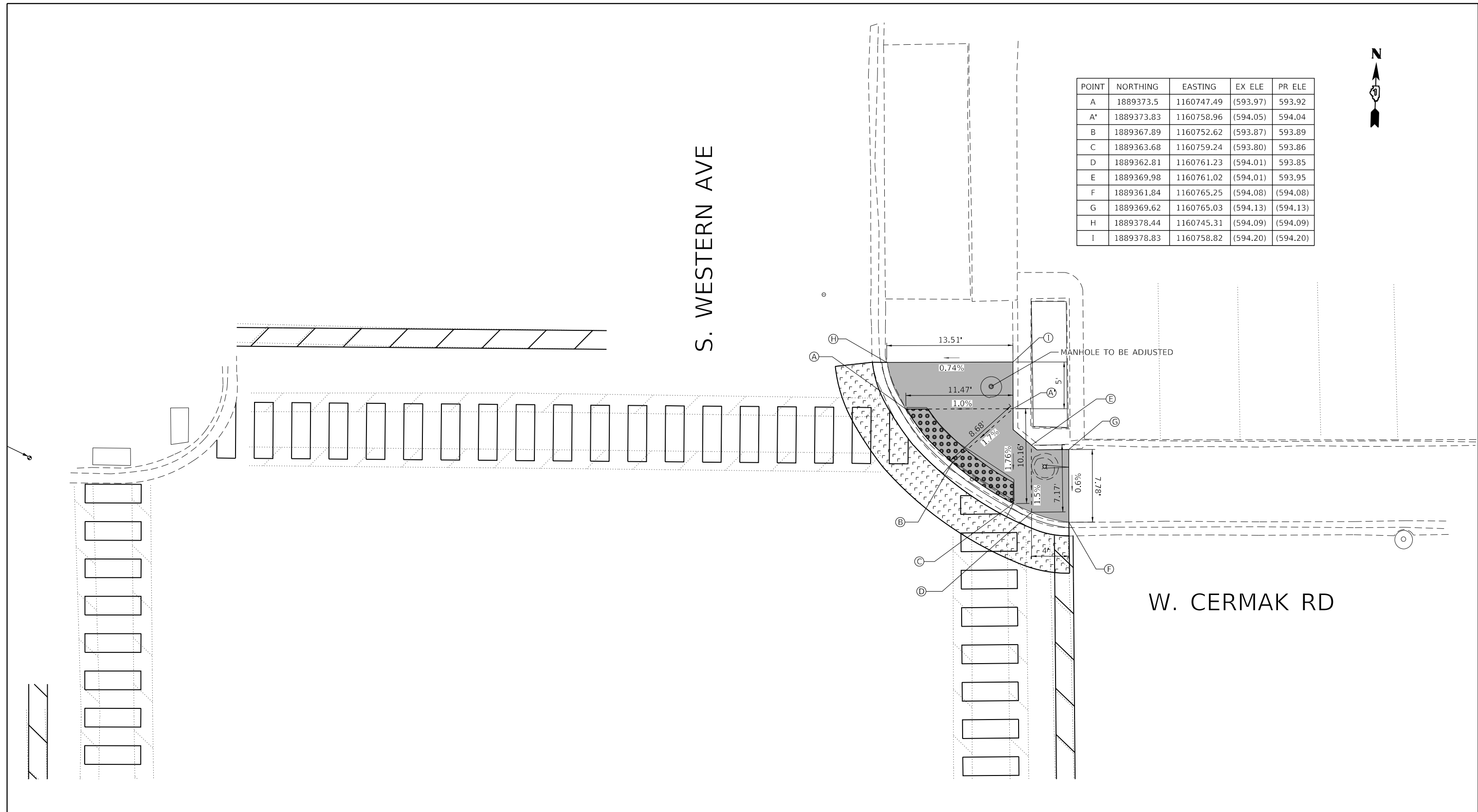
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	21
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				



POINT	NORTHING	EASTING	EX ELE	PR ELE
A	1889373.5	1160747.49	(593.97)	593.92
A'	1889373.83	1160758.96	(594.05)	594.04
B	1889367.89	1160752.62	(593.87)	593.89
C	1889363.68	1160759.24	(593.80)	593.86
D	1889362.81	1160761.23	(594.01)	593.85
E	1889369.98	1160761.02	(594.01)	593.95
F	1889361.84	1160765.25	(594.08)	(594.08)
G	1889369.62	1160765.03	(594.13)	(594.13)
H	1889378.44	1160745.31	(594.09)	(594.09)
I	1889378.83	1160758.82	(594.20)	(594.20)

S. WESTERN AVE

W. CERMAK RD



REFERENCE BENCHMARK TBM #112 ELEV 595.10
 BENCHMARK: TOP OF THE NORTH FLANGE BOLT OF FIRE HYDRANT
 LOCATION: NORTHWEST CORNER OF W. CERMAK ROAD & S. WESTERN AVENUE.

LEGEND

XX.XX' EXISTING LENGTH
 () EXISTING ELEVATION/SLOPE
 SC TOP OF PROPOSED SIDE CURB

PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
 PROPOSED SIDE CURB
 COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

CLASS D PATCH
 DETECTABLE WARNINGS
 SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT

TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
 EXISTING PAVEMENT MARKING
 PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

MODEL: 1400BELNAMES
 FILE NAME: D:\1400Bel\1400Bel.dwg

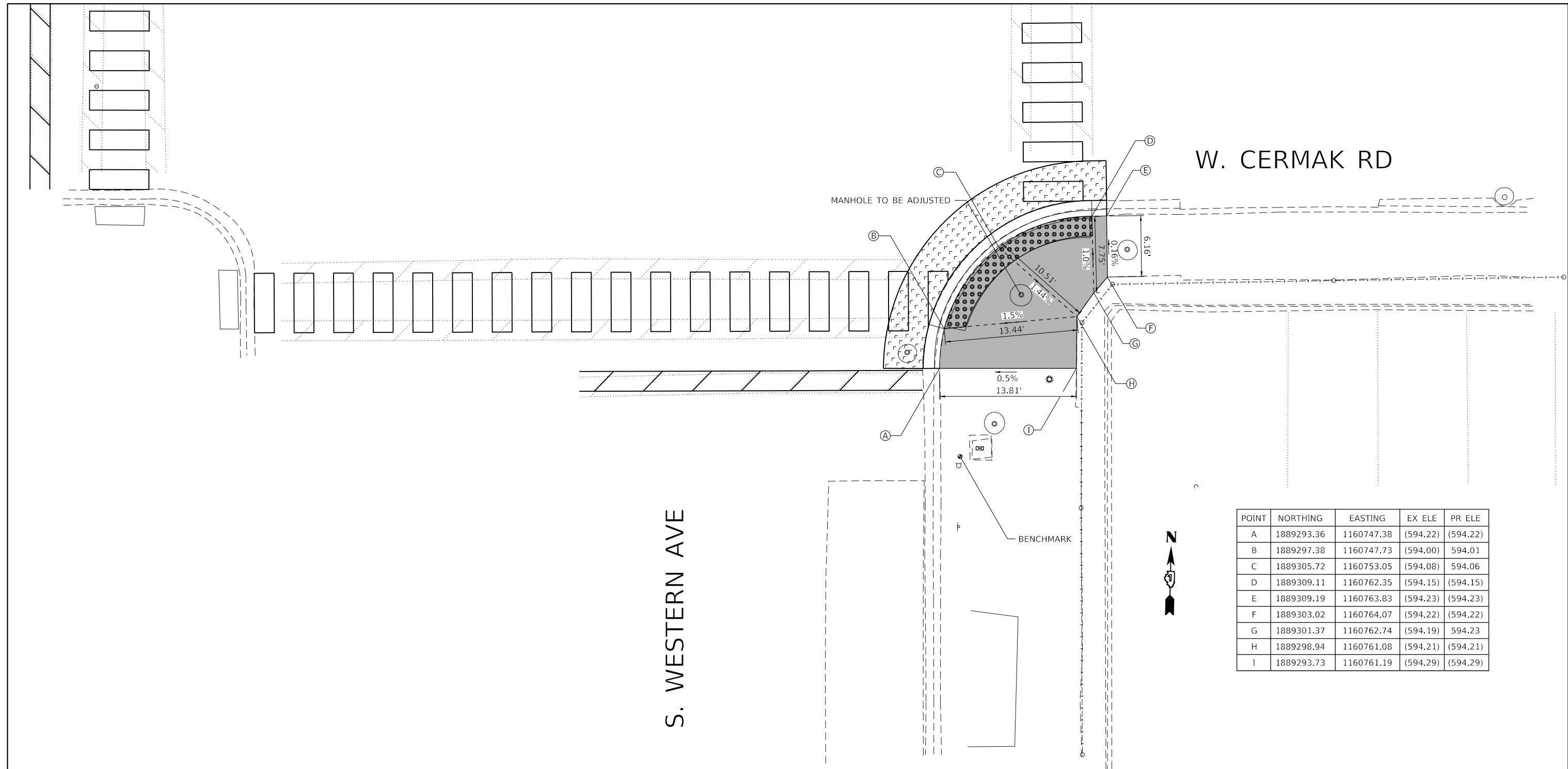
AMES Engineering, Inc.
 CONSULTING ENGINEERS
 6330 Belmont Road, Suite 4B
 Downers Grove, IL 60515

USER NAME = arosalva	DESIGNED - JAR	REVISED -
PLOT SCALE = \$SCALE\$	DRAWN - MD	REVISED -
PLOT DATE = 12/10/2020	CHECKED - AS	REVISED -
	DATE - 12/10/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
SIDEWALK DETAIL PLAN
 SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	22
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				



W. CERMAK RD

S. WESTERN AVE

POINT	NORTHING	EASTING	EX ELE	PR ELE
A	1889293.36	1160747.38	(594.22)	(594.22)
B	1889297.38	1160747.73	(594.00)	594.01
C	1889305.72	1160753.05	(594.08)	594.06
D	1889309.11	1160762.35	(594.15)	(594.15)
E	1889309.19	1160763.83	(594.23)	(594.23)
F	1889303.02	1160764.07	(594.22)	(594.22)
G	1889301.37	1160762.74	(594.19)	594.23
H	1889298.94	1160761.08	(594.21)	(594.21)
I	1889293.73	1160761.19	(594.29)	(594.29)

REFERENCE BENCHMARK TBM #111 ELEV 595.18
 BENCHMARK: TOP OF THE NORTH FLANGE BOLT OF FIRE HYDRANT
 LOCATION: SOUTHEAST CORNER OF W. CERMAK ROAD & S. WESTERN AVENUE.

LEGEND

XX.XX' EXISTING LENGTH
 () EXISTING ELEVATION/SLOPE
 SC TOP OF PROPOSED SIDE CURB

PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
 PROPOSED SIDE CURB
 COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

CLASS D PATCH
 DETECTABLE WARNINGS
 SIDEWALK REMOVAL REPLACE WITH TOPSOIL, FURNISH AND PLACE, 4" SODDING, SALT TOLERANT

TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
 EXISTING PAVEMENT MARKING
 PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

MODEL: 1400BELNAMES
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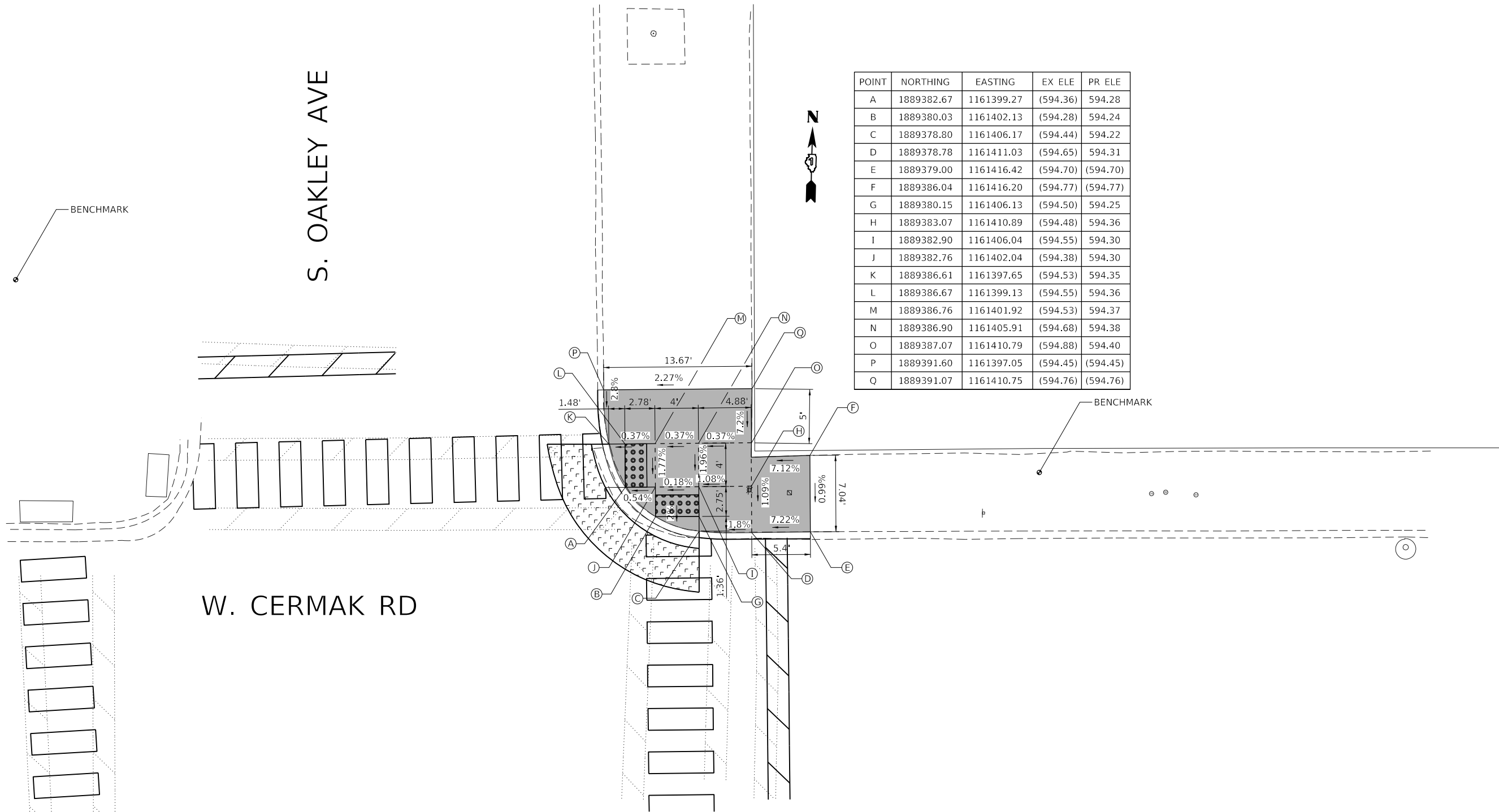
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
SIDEWALK DETAIL PLAN
 SCALE: 1"=5'
 SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	23
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				



POINT	NORTHING	EASTING	EX ELE	PR ELE
A	1889382.67	1161399.27	(594.36)	594.28
B	1889380.03	1161402.13	(594.28)	594.24
C	1889378.80	1161406.17	(594.44)	594.22
D	1889378.78	1161411.03	(594.65)	594.31
E	1889379.00	1161416.42	(594.70)	(594.70)
F	1889386.04	1161416.20	(594.77)	(594.77)
G	1889380.15	1161406.13	(594.50)	594.25
H	1889383.07	1161410.89	(594.48)	594.36
I	1889382.90	1161406.04	(594.55)	594.30
J	1889382.76	1161402.04	(594.38)	594.30
K	1889386.61	1161397.65	(594.53)	594.35
L	1889386.67	1161399.13	(594.55)	594.36
M	1889386.76	1161401.92	(594.53)	594.37
N	1889386.90	1161405.91	(594.68)	594.38
O	1889387.07	1161410.79	(594.88)	594.40
P	1889391.60	1161397.05	(594.45)	(594.45)
Q	1889391.07	1161410.75	(594.76)	(594.76)

REFERENCE BENCHMARK TBM #110 ELEV 594.72
 BENCHMARK: TOP OF CROSS CUT FOUND IN SIDEWALK
 LOCATION: NORTHEAST CORNER OF W. CERMAK ROAD & S. OAKLEY AVENUE.

LEGEND

- XX.XX' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- SC TOP OF PROPOSED SIDE CURB
- PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- PROPOSED SIDE CURB
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- CLASS D PATCH
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- EXISTING PAVEMENT MARKING
- PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

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 Downers Grove, IL 60515

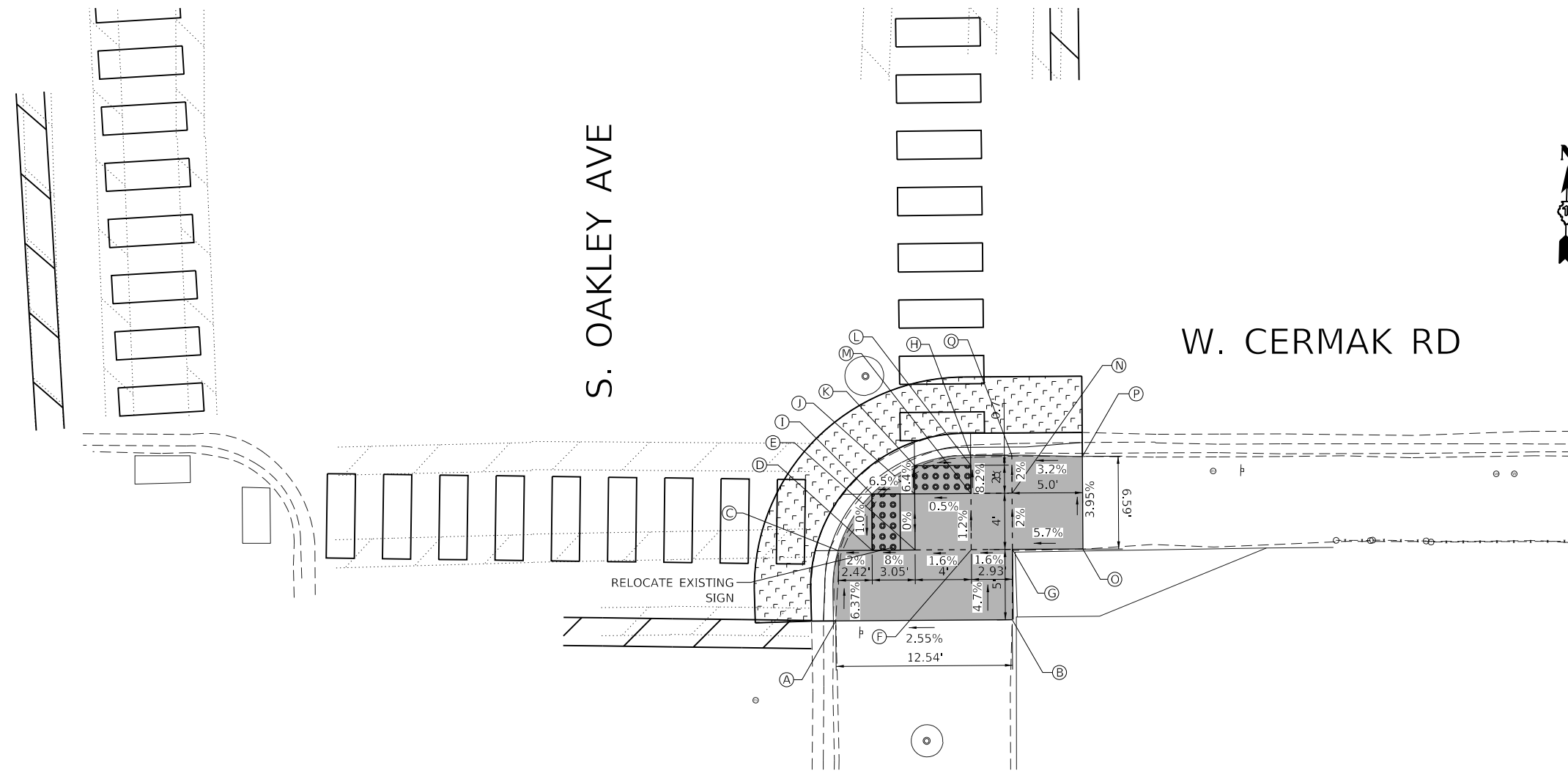
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PLOT SCALE = \$SCALE\$	DRAWN - MD	REVISED -
PLOT DATE = 12/10/2020	CHECKED - AS	REVISED -
	DATE - 12/10/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
SIDEWALK DETAIL PLAN
 SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	24
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				

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POINT	NORTHING	EASTING	EX ELE	PR ELE
A	1889315.43	1161398.88	(594.35)	(594.35)
B	1889315.83	1161411.42	(594.67)	(594.67)
C	1889320.44	1161398.92	(594.35)	594.03
D	1889320.51	1161401.33	(594.44)	594.08
E	1889320.61	1161404.38	(594.53)	594.32
F	1889320.74	1161408.37	(594.63)	594.39
G	1889320.83	1161411.3	(594.70)	594.43
H	1889327.43	1161408.16	(594.51)	594.16
I	1889324.51	1161401.21	(594.23)	594.12
J	1889324.61	1161404.25	(594.34)	594.32
K	1889326.61	1161404.19	(594.29)	594.19
L	1889326.73	1161408.18	(594.50)	594.18
M	1889324.73	1161408.25	(594.52)	594.34
N	1889324.83	1161411.18	(594.63)	594.35
O	1889320.98	1161416.31	(594.72)	(594.72)
P	1889327.57	1161416.1	(594.46)	(594.46)
Q	1889327.53	1161411.09	(594.48)	594.30

REFERENCE BENCHMARK TBM #110 ELEV 594.72
 BENCHMARK: TOP OF CROSS CUT FOUND IN SIDEWALK
 LOCATION: NORTHEAST CORNER OF W. CERMAK ROAD & S. OAKLEY AVENUE.

LEGEND

XX.XX'

EXISTING LENGTH

()

EXISTING ELEVATION/SLOPE

SC

TOP OF PROPOSED SIDE CURB



PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH



PROPOSED SIDE CURB



COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT



TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT



EXISTING PAVEMENT MARKING



PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
 SIDEWALK DETAIL PLAN

SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	25
CONTRACT NO. 62L95				

ILLINOIS FED. AID PROJECT

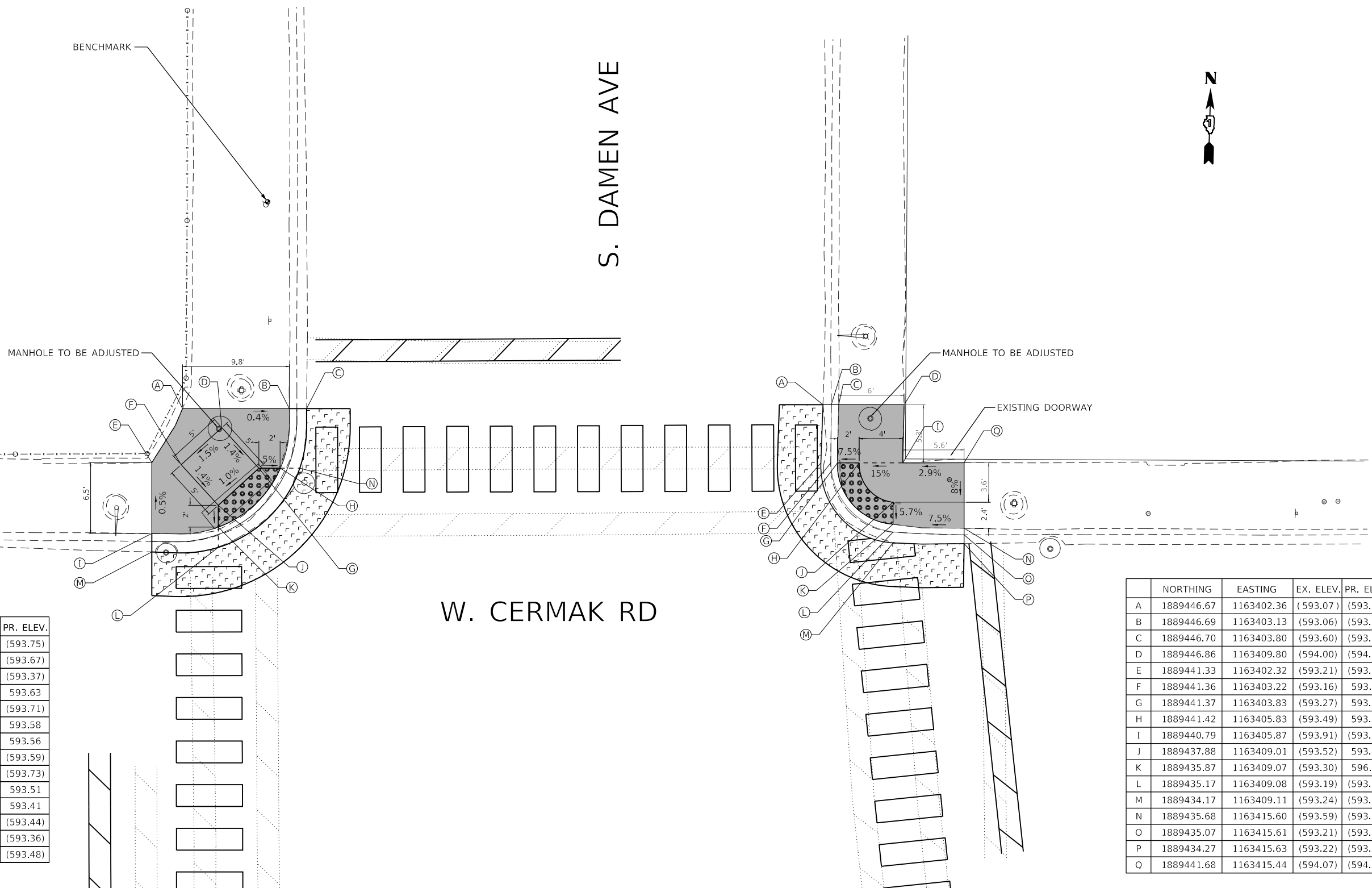
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 6330 Belmont Road, Suite 4B
 Downers Grove, IL 60515

USER NAME	DESIGNED	REVISIONS
= arosalva	JAR	-
	MD	-
	AS	-
	DATE	12/10/2020

S. DAMEN AVE

W. CERMAK RD



	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889444.81	1163343.63	(593.75)	(593.75)
B	1889445.06	1163353.43	(593.67)	(593.67)
C	1889445.10	1163355.01	(593.37)	(593.37)
D	1889443.15	1163347.28	(593.70)	593.63
E	1889439.79	1163340.95	(593.71)	(593.71)
F	1889439.67	1163343.65	(593.67)	593.58
G	1889439.53	1163350.78	(593.59)	593.56
H	1889439.58	1163352.76	(593.59)	(593.59)
I	1889433.29	1163341.11	(593.73)	(593.73)
J	1889436.03	1163347.14	(593.59)	593.51
K	1889434.03	1163347.19	(593.41)	593.41
L	1889432.13	1163347.11	(593.44)	(593.44)
M	1889431.59	1163341.16	(593.36)	(593.36)
N	1889439.62	1163354.52	(593.48)	(593.48)

	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889446.67	1163402.36	(593.07)	(593.07)
B	1889446.69	1163403.13	(593.06)	(593.06)
C	1889446.70	1163403.80	(593.60)	(593.60)
D	1889446.86	1163409.80	(594.00)	(594.00)
E	1889441.33	1163402.32	(593.21)	(593.21)
F	1889441.36	1163403.22	(593.16)	593.11
G	1889441.37	1163403.83	(593.27)	593.15
H	1889441.42	1163405.83	(593.49)	593.30
I	1889440.79	1163405.87	(593.91)	(593.91)
J	1889437.88	1163409.01	(593.52)	593.45
K	1889435.87	1163409.07	(593.30)	596.21
L	1889435.17	1163409.08	(593.19)	(593.19)
M	1889434.17	1163409.11	(593.24)	(593.24)
N	1889435.68	1163415.60	(593.59)	(593.59)
O	1889435.07	1163415.61	(593.21)	(593.21)
P	1889434.27	1163415.63	(593.22)	(593.22)
Q	1889441.68	1163415.44	(594.07)	(594.07)

REFERENCE BENCHMARK TBM #108 ELEV 594.72
 BENCHMARK: TOP OF THE NORTH FLANGE BOLT OF FIRE HYDRANT
 LOCATION: NORTHWEST CORNER OF W. CERMAK ROAD & S. DAMEN AVENUE.

LEGEND

- XX.XX' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- SC TOP OF PROPOSED SIDE CURB
- PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- PROPOSED SIDE CURB
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- CLASS D PATCH
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- EXISTING PAVEMENT MARKING
- PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

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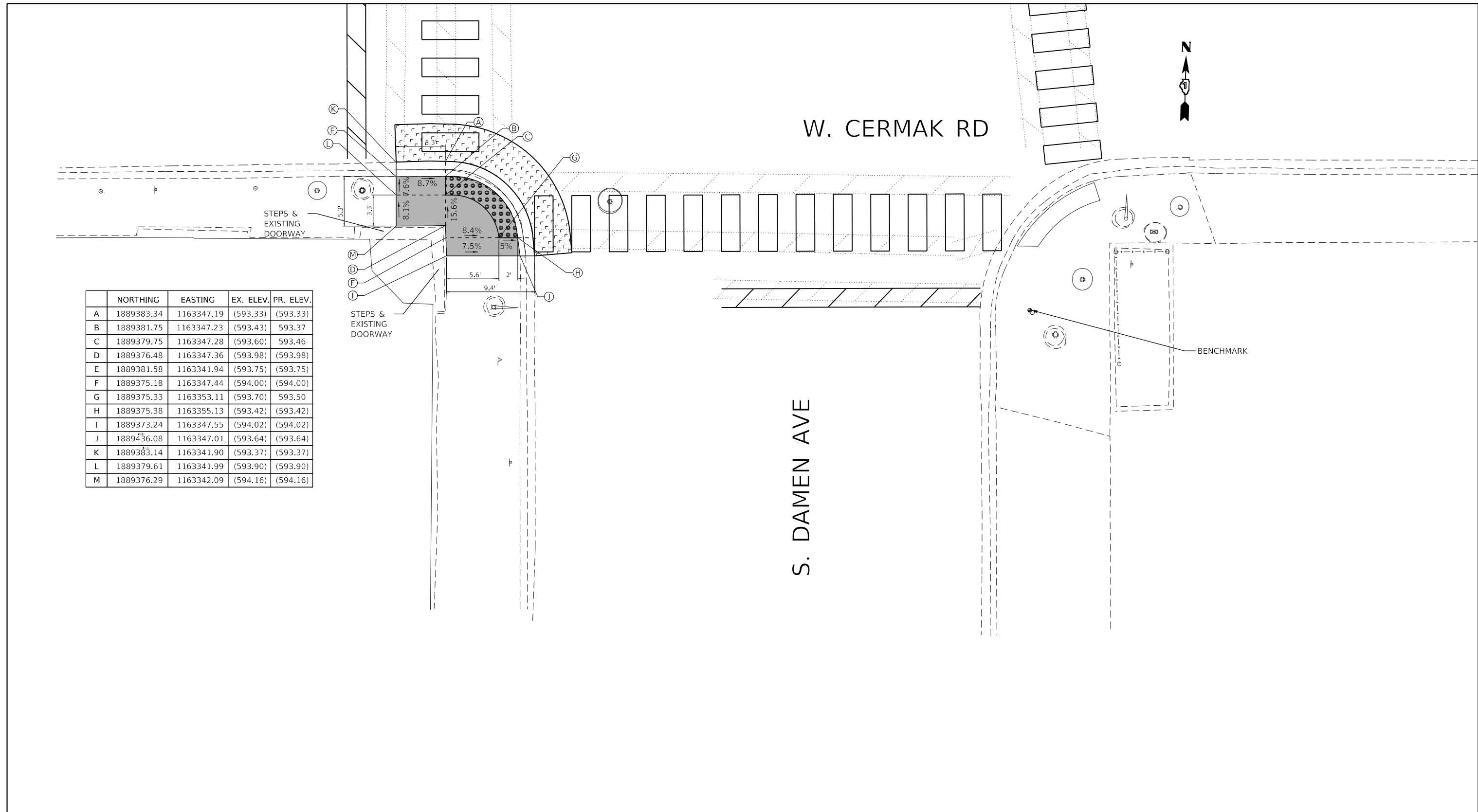
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
SIDEWALK DETAIL PLAN
 SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	26
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				



REFERENCE BENCHMARK TBM #107 ELEV 594.62

BENCHMARK: TOP OF THE NORTH FLANGE
BOLT OF FIRE HYDRANT

LOCATION: SOUTHEAST CORNER OF W. CERMAK ROAD
& S. DAMEN AVENUE.

LEGEND

EXISTING LENGTH	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	CLASS D PATCH	TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
EXISTING ELEVATION/SLOPE	PROPOSED SIDE CURB	DETECTABLE WARNINGS	EXISTING PAVEMENT MARKING
TOP OF PROPOSED SIDE CURB	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT	PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

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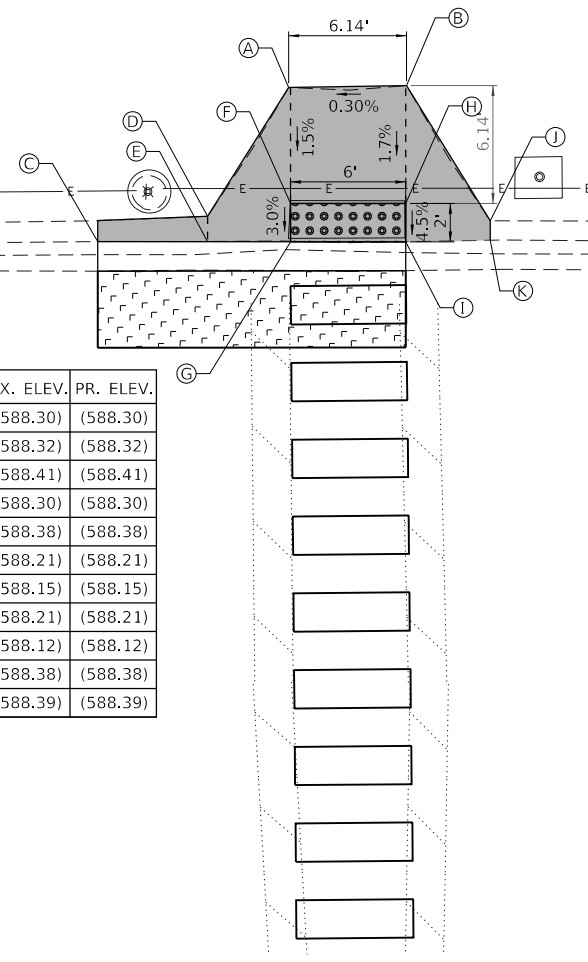
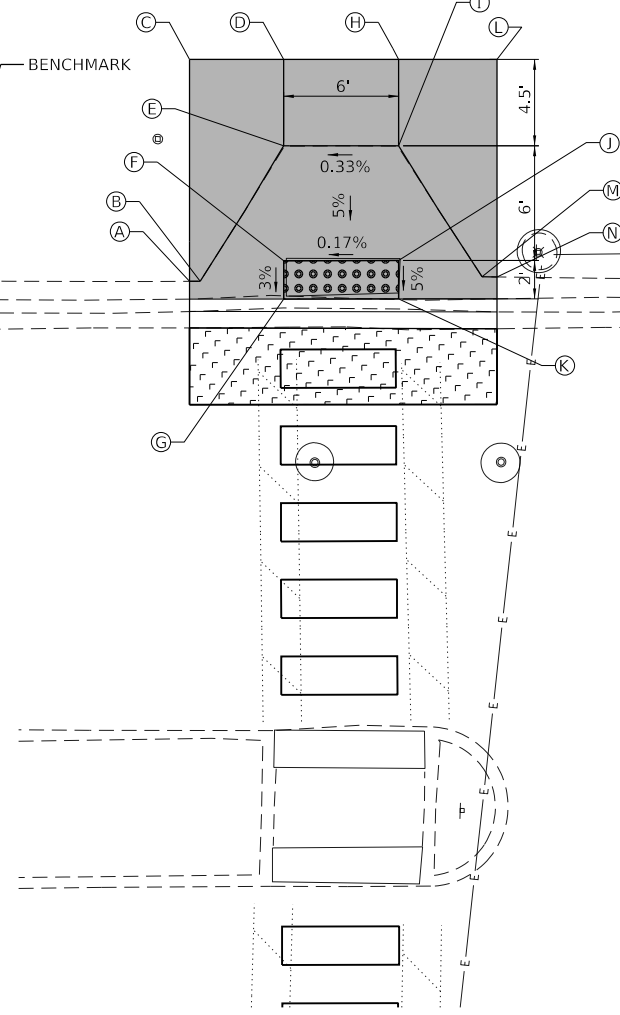
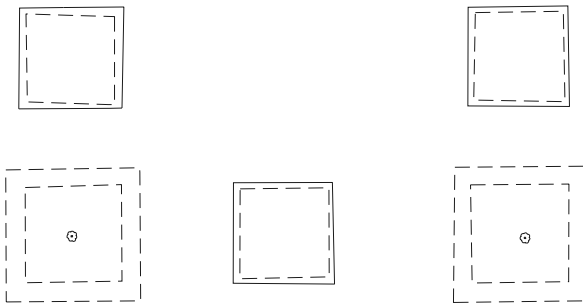
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PLOT DATE = 12/10/2020	CHECKED - AS	REVISED -
	DATE - 12/10/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
SIDEWALK DETAIL PLAN

SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	27
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				



W. CERMAK RD

S. LAFLIN ST



	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889535.14	1166658.90	(588.63)	(588.63)
B	1889535.16	1166659.45	(588.63)	(588.63)
C	1889546.71	1166658.61	(588.89)	(588.89)
D	1889546.83	1166663.50	(588.90)	(588.90)
E	1889542.32	1166663.61	(588.88)	588.73
F	1889536.35	1166663.77	(588.46)	588.43
G	1889534.35	1166663.82	(588.33)	(588.33)
H	1889546.98	1166669.49	(588.94)	(588.94)
I	1889542.49	1166669.61	(588.91)	(588.74)
J	1889536.51	1166669.76	(588.47)	(588.44)
K	1889534.51	1166669.81	(588.34)	(588.34)
L	1889547.12	1166674.62	(588.90)	(588.90)
M	1889535.77	1166674.13	(588.62)	(588.62)
N	1889534.51	1166669.81	(588.62)	(588.62)

	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889544.74	1166747.15	(588.30)	(588.30)
B	1889545.00	1166753.28	(588.32)	(588.32)
C	1889536.46	1166737.41	(588.41)	(588.41)
D	1889537.93	1166743.10	(588.30)	(588.30)
E	1889536.60	1166743.13	(588.38)	(588.38)
F	1889538.71	1166747.40	(588.21)	(588.21)
G	1889536.71	1166747.45	(588.15)	(588.15)
H	1889538.86	1166753.40	(588.21)	(588.21)
I	1889536.86	1166753.45	(588.12)	(588.12)
J	1889538.07	1166757.82	(588.38)	(588.38)
K	1889537.06	1166757.85	(588.39)	(588.39)

REFERENCE BENCHMARK TBM #105 ELEV 589.39
 BENCHMARK: TOP OF THE NORTH FLANGE BOLT OF FIRE HYDRANT
 LOCATION: NORTH SIDE OF W. CERMAK ROAD & WEST OF S. LAFLIN STREET.

LEGEND

XX.XX' EXISTING LENGTH
 () EXISTING ELEVATION/SLOPE
 SC TOP OF PROPOSED SIDE CURB

PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
 PROPOSED SIDE CURB
 COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

CLASS D PATCH
 DETECTABLE WARNINGS
 SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT

TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
 EXISTING PAVEMENT MARKING
 PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

MODEL: 1400BELNAMES
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 Downers Grove, IL 60515

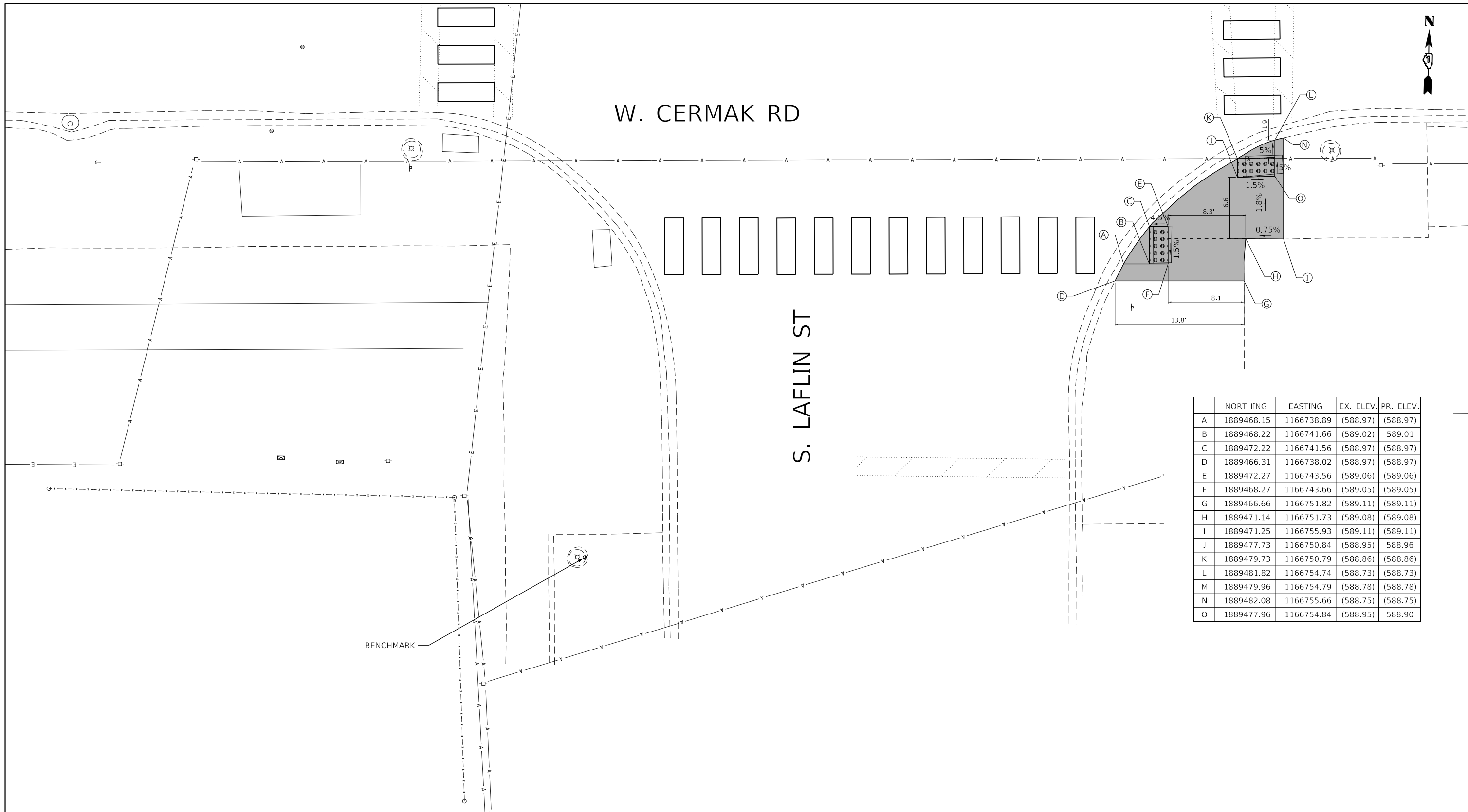
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
 SIDEWALK DETAIL PLAN**

SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	28
CONTRACT NO. 62L95				
ILLINOIS		FED. AID PROJECT		



	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889468.15	1166738.89	(588.97)	(588.97)
B	1889468.22	1166741.66	(589.02)	589.01
C	1889472.22	1166741.56	(588.97)	(588.97)
D	1889466.31	1166738.02	(588.97)	(588.97)
E	1889472.27	1166743.56	(589.06)	(589.06)
F	1889468.27	1166743.66	(589.05)	(589.05)
G	1889466.66	1166751.82	(589.11)	(589.11)
H	1889471.14	1166751.73	(589.08)	(589.08)
I	1889471.25	1166755.93	(589.11)	(589.11)
J	1889477.73	1166750.84	(588.95)	588.96
K	1889479.73	1166750.79	(588.86)	(588.86)
L	1889481.82	1166754.74	(588.73)	(588.73)
M	1889479.96	1166754.79	(588.78)	(588.78)
N	1889482.08	1166755.66	(588.75)	(588.75)
O	1889477.96	1166754.84	(588.95)	588.90

REFERENCE BENCHMARK TBM #106 ELEV 589.94
 BENCHMARK: TOP OF SQUARE CUT ON CONCRETE BASE OF LIGHT POLE
 LOCATION: SOUTHWEST CORNER OF W. CERMAK ROAD & S. LAFLIN STREET.

LEGEND

- XX.XX' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- SC TOP OF PROPOSED SIDE CURB
- PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- PROPOSED SIDE CURB
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- CLASS D PATCH
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- EXISTING PAVEMENT MARKING
- PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

MODEL: 1400BELNAMES
FILE NAME: D:\12020\117-RS\ADA.dwg

AMES Engineering, Inc.
 CONSULTING ENGINEERS
 6330 Belmont Road, Suite 4B
 Downers Grove, IL 60515

USER NAME = arosalva	DESIGNED - JAR	REVISIONS
PLOT SCALE = \$SCALE\$	DRAWN - MD	REVISIONS
PLOT DATE = 12/10/2020	CHECKED - AS	REVISIONS
	DATE - 12/10/2020	REVISIONS

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

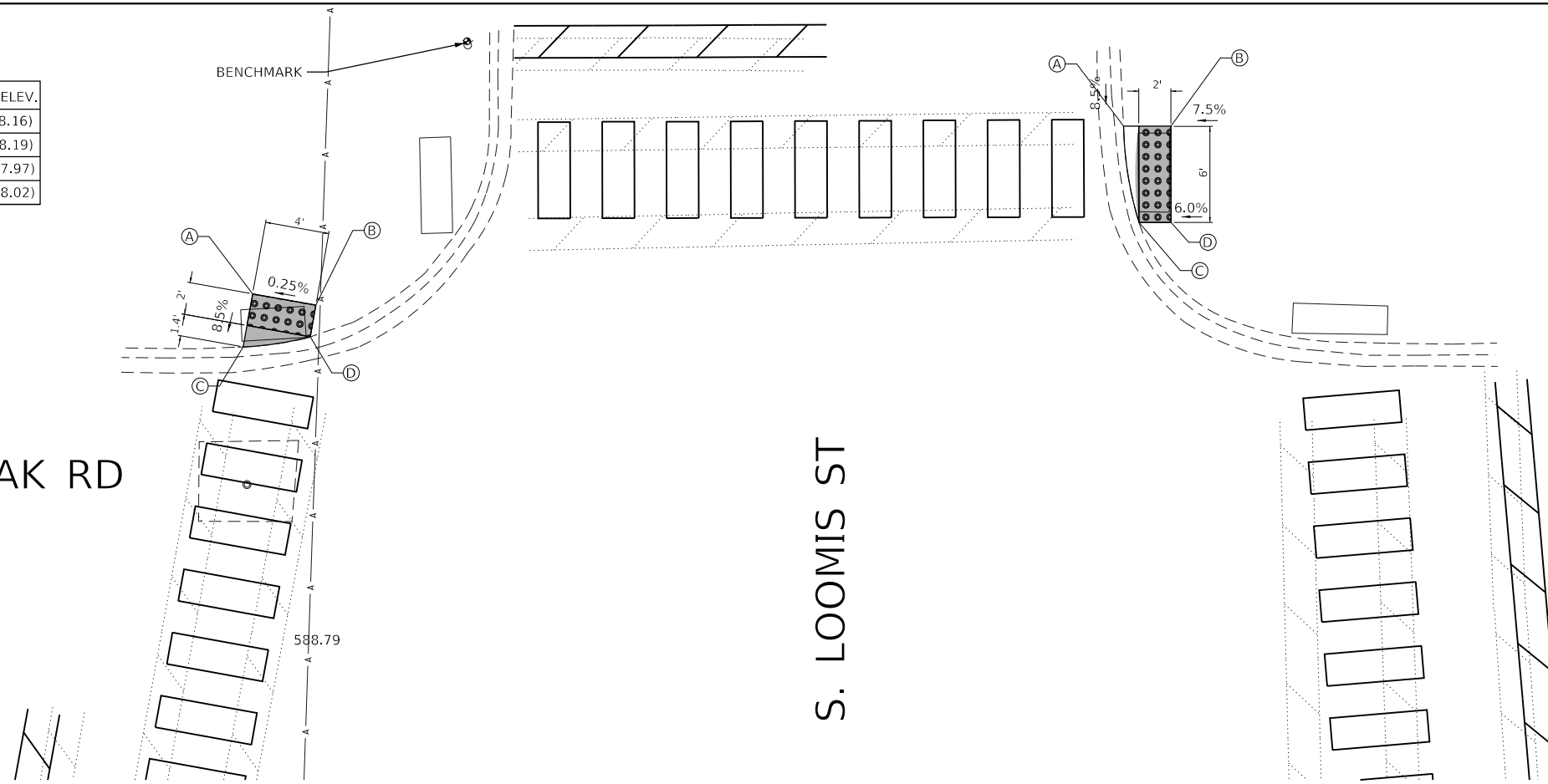
**W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
 SIDEWALK DETAIL PLAN**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	29
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				

SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889556.37	1167340.71	(588.16)	(588.16)
B	1889556.48	1167344.71	(588.19)	(588.19)
C	1889553.82	1167340.77	(587.97)	(587.97)
D	1889554.48	1167344.76	(588.02)	(588.02)

	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889569.02	1167395.29	(588.08)	(588.08)
B	1889487.12	1889487.12	(588.28)	(588.28)
C	1889487.13	1889487.13	(588.09)	(588.09)
D	1889489.78	1889489.78	(588.20)	(588.20)



W. CERMAK RD

S. LOOMIS ST

REFERENCE BENCHMARK TBM #104 ELEV 589.76

BENCHMARK: TOP OF THE NORTH FLANGE
BOLT OF FIRE HYDRANT

LOCATION: NORTHWEST CORNER OF W. CERMAK
ROAD & S. LOOMIS STREET.

LEGEND

- XX.XX' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- SC TOP OF PROPOSED SIDE CURB

- PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- PROPOSED SIDE CURB
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

- CLASS D PATCH
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT

- TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- EXISTING PAVEMENT MARKING
- PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

MODEL: 1400BELNAMES
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AMES Engineering, Inc.
CONSULTING ENGINEERS
6330 Belmont Road, Suite 4B
Downers Grove, IL 60515

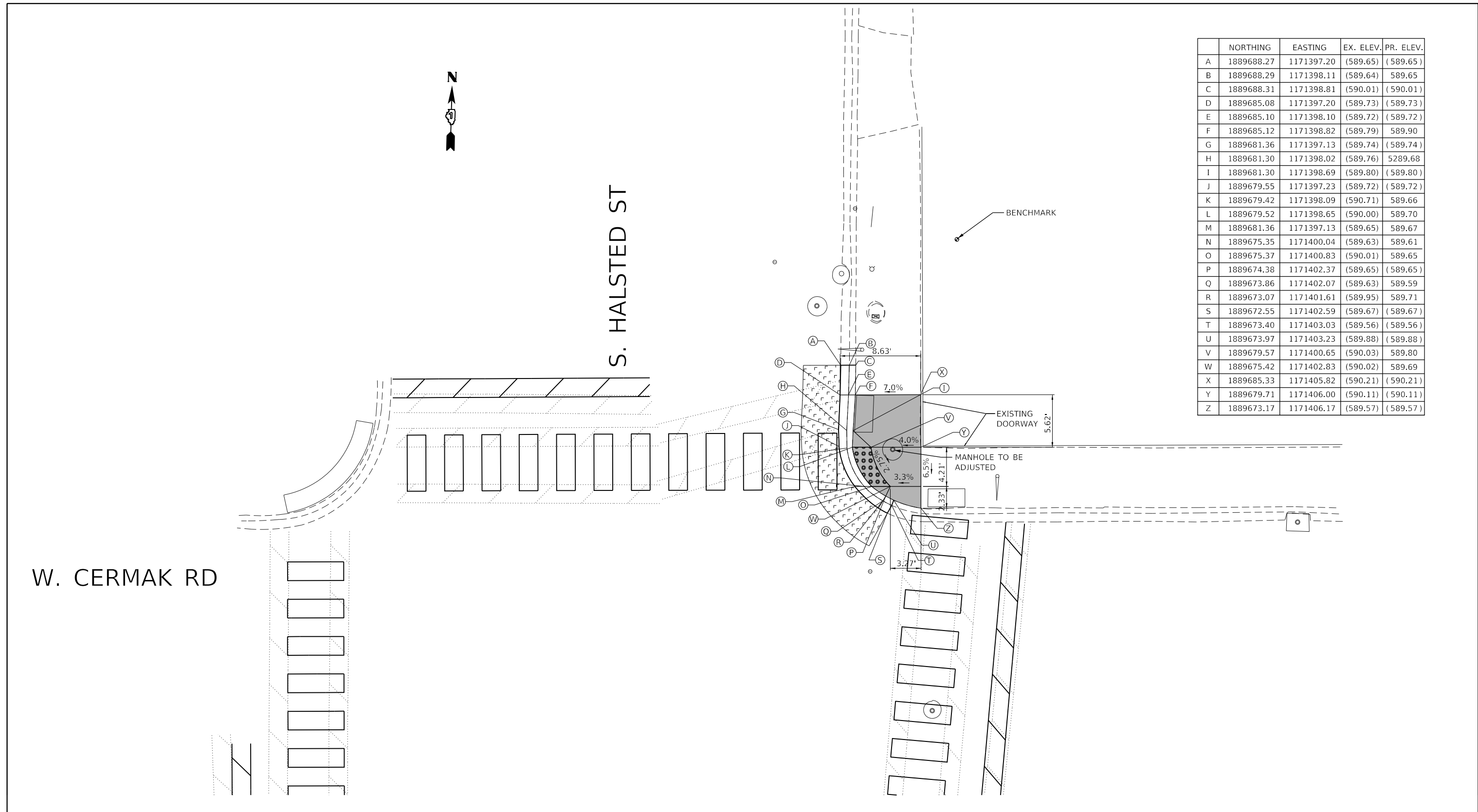
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PLOT DATE = 12/10/2020	DATE - 12/10/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
SIDEWALK DETAIL PLAN**

SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	30
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				



	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889688.27	1171397.20	(589.65)	(589.65)
B	1889688.29	1171398.11	(589.64)	589.65
C	1889688.31	1171398.81	(590.01)	(590.01)
D	1889685.08	1171397.20	(589.73)	(589.73)
E	1889685.10	1171398.10	(589.72)	(589.72)
F	1889685.12	1171398.82	(589.79)	589.90
G	1889681.36	1171397.13	(589.74)	(589.74)
H	1889681.30	1171398.02	(589.76)	5289.68
I	1889681.30	1171398.69	(589.80)	(589.80)
J	1889679.55	1171397.23	(589.72)	(589.72)
K	1889679.42	1171398.09	(590.71)	589.66
L	1889679.52	1171398.65	(590.00)	589.70
M	1889681.36	1171397.13	(589.65)	589.67
N	1889675.35	1171400.04	(589.63)	589.61
O	1889675.37	1171400.83	(590.01)	589.65
P	1889674.38	1171402.37	(589.65)	(589.65)
Q	1889673.86	1171402.07	(589.63)	589.59
R	1889673.07	1171401.61	(589.95)	589.71
S	1889672.55	1171402.59	(589.67)	(589.67)
T	1889673.40	1171403.03	(589.56)	(589.56)
U	1889673.97	1171403.23	(589.88)	(589.88)
V	1889679.57	1171400.65	(590.03)	589.80
W	1889675.42	1171402.83	(590.02)	589.69
X	1889685.33	1171405.82	(590.21)	(590.21)
Y	1889679.71	1171406.00	(590.11)	(590.11)
Z	1889673.17	1171406.17	(589.57)	(589.57)

REFERENCE BENCHMARK TBM #101 ELEV 591.01
 BENCHMARK: TOP OF THE NORTH FLANGE BOLT OF FIRE HYDRANT
 LOCATION: NORTHEAST CORNER OF W. CERMAK ROAD & S. HALSTED STREET.

LEGEND

XX.XX'	EXISTING LENGTH		PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH		CLASS D PATCH		TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
()	EXISTING ELEVATION/SLOPE		PROPOSED SIDE CURB		DETECTABLE WARNINGS		EXISTING PAVEMENT MARKING
SC	TOP OF PROPOSED SIDE CURB		COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT		SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT		PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

MODEL: \\MODEL\AMES
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 DATE: 12/10/2020

AMES Engineering, Inc.
 CONSULTING ENGINEERS
 6330 Belmont Road, Suite 4B
 Downers Grove, IL 60515

USER NAME = arosalva	DESIGNED - JAR	REVISIONS
PLOT SCALE = \$SCALE\$	DRAWN - MD	REVISIONS
PLOT DATE = 12/10/2020	CHECKED - AS	REVISIONS
	DATE - 12/10/2020	REVISIONS

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

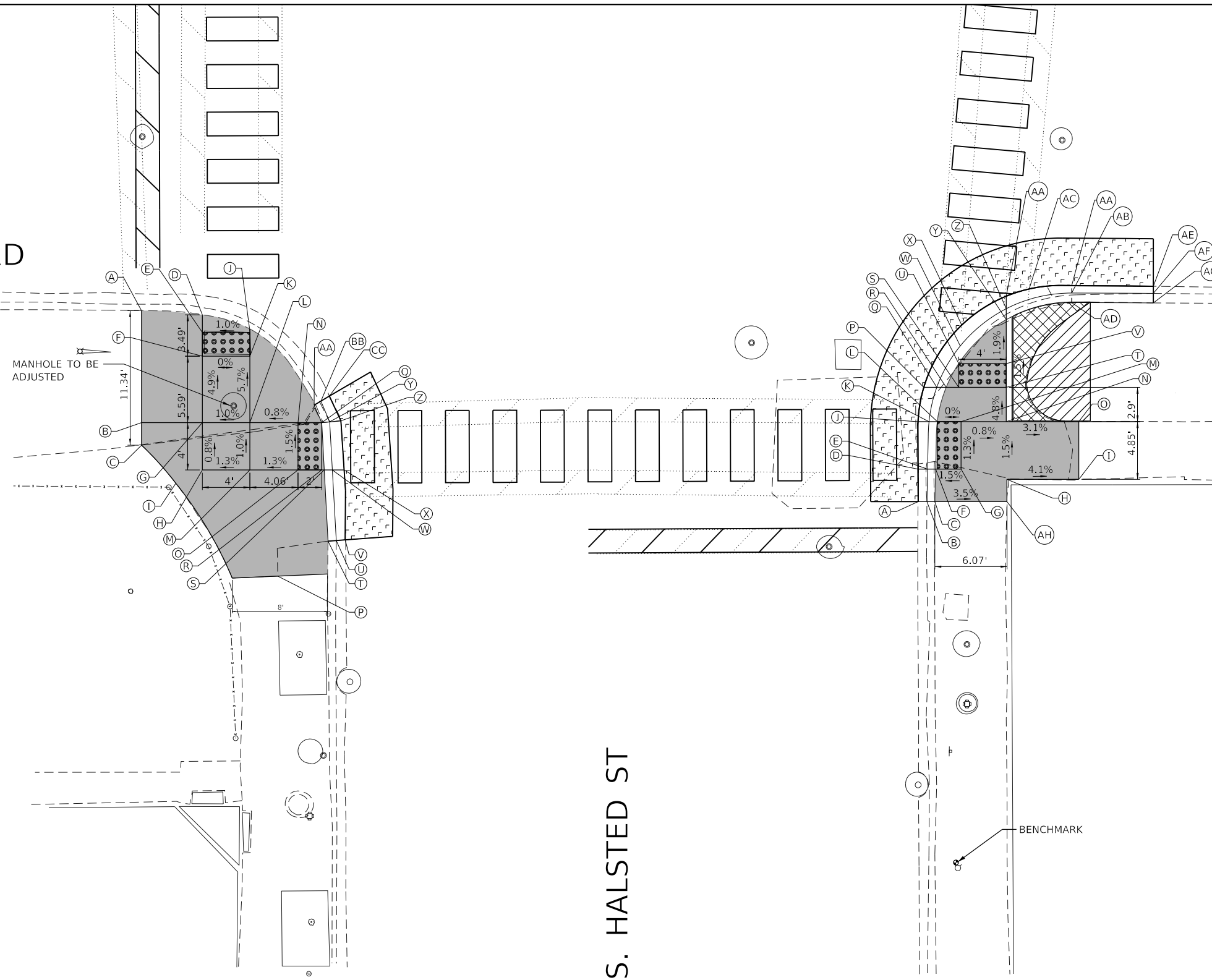
**W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
 SIDEWALK DETAIL PLAN**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	32
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				

SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.



W. CERMAK RD



S. HALSTED ST

	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889612.85	1171334.47	(590.03)	(590.03)
B	1889603.42	1171334.71	(590.06)	(590.06)
C	1889601.51	1171334.76	(590.08)	(590.08)
D	1889612.63	1171339.60	(589.71)	(589.71)
E	1889611.14	1171339.64	(589.73)	589.76
F	1889609.14	1171339.69	(589.95)	589.86
G	1889603.55	1171339.83	(590.14)	(590.14)
H	1889599.55	1171339.94	(590.17)	(590.17)
I	1889598.38	1171337.73	(590.17)	(590.17)
J	1889611.54	1171343.63	(589.83)	(589.83)
K	1889609.24	1171343.69	(589.95)	589.86
L	1889603.65	1171343.83	(590.18)	(590.18)
M	1889599.65	1171343.93	(590.15)	590.22
N	1889603.75	1171347.89	(590.28)	590.21
O	1889599.76	1171347.99	(590.15)	590.27
P	1889590.77	1171346.51	(590.08)	(590.08)
Q	1889603.81	1171349.89	(590.17)	590.16
R	1889599.81	1171349.99	(590.23)	(590.23)
S	1889599.81	1171350.20	(590.23)	(590.23)
T	1889593.83	1171350.68	(590.11)	(590.11)
U	1889593.90	1171351.37	(590.04)	(590.04)
V	1889593.97	1171352.11	(590.07)	(590.07)
W	1889599.83	1171350.89	(590.19)	(590.19)
X	1889599.85	1171351.88	(590.25)	(590.25)
Y	1889603.88	1171350.55	(590.19)	(590.12)
Z	1889603.99	1171351.49	(590.18)	(590.18)
AA	1889605.26	1171349.21	(590.23)	(590.23)
BB	1889605.61	1171349.77	(590.13)	(590.09)
CC	1889606.05	1171350.48	(590.10)	(590.10)

	NORTHING	EASTING	EX. ELEV.	PR. ELEV.
A	1889598.42	1171400.29	(589.90)	(589.90)
B	1889598.44	1171401.02	(589.91)	(589.91)
C	1889598.46	1171401.69	(590.14)	(590.14)
D	1889601.16	1171400.21	(590.01)	(589.97)
E	1889601.18	1171401.01	(589.94)	589.91
F	1889601.19	1171401.69	(590.01)	589.95
G	1889601.25	1171403.83	(589.98)	(589.98)
H	1889600.49	1171407.71	(589.96)	(589.96)
I	1889600.65	1171413.77	(589.70)	(589.70)
J	1889605.15	1171400.10	(589.91)	589.95
K	1889605.18	1171401.03	(589.91)	589.89
L	1889605.20	1171401.73	(589.93)	(589.93)
M	1889605.25	1171403.73	(589.93)	(589.93)
N	1889605.34	1171407.53	(589.90)	(589.90)
O	1889605.50	1171413.64	(589.70)	(589.70)
P	1889608.07	1171400.52	(589.89)	(589.89)
Q	1889608.09	1171401.61	(589.83)	(589.83)
R	1889608.11	1171402.40	(589.87)	(589.87)
S	1889608.14	1171403.46	(589.87)	(589.87)
T	1889608.24	1171407.45	(589.70)	589.78
U	1889610.14	1171403.40	(589.82)	589.84
V	1889610.24	1171407.40	(589.74)	589.75
W	1889611.63	1171403.50	(589.78)	589.79
X	1889613.39	1171403.58	(589.89)	589.85
Y	1889615.92	1171407.26	(589.72)	589.69
Z	1889614.87	1171407.29	(589.60)	589.65
AA	1889613.94	1171407.31	(589.38)	(589.38)
AB	1889616.95	1171412.76	(589.32)	(589.32)
AC	1889616.33	1171412.78	(589.54)	589.71
AD	1889615.52	1171412.80	(589.66)	(589.66)
AE	1889617.07	1171419.63	(589.35)	(589.35)
AF	1889616.48	1171419.64	(589.29)	(589.29)
AG	1889615.66	1171419.66	(589.65)	(589.65)
AH	1889598.61	1171407.76	(589.91)	(589.91)

REFERENCE BENCHMARK TBM #102 ELEV 591.94
 BENCHMARK: TOP OF THE NORTH FLANGE BOLT OF FIRE HYDRANT
 LOCATION: SOUTHEAST CORNER OF W. CERMAK ROAD & S. HALSTED STREET.

LEGEND

- XX.XX' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- SC TOP OF PROPOSED SIDE CURB
- PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- PROPOSED SIDE CURB
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- CLASS D PATCH
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT
- EXISTING PAVEMENT MARKING
- PROPOSED PAVEMENT MARKING (SEE ROADWAY AND PAVEMENT MARKING PLANS)

MODEL: STW08BELNAMES FILE NAME: D:\10250201-000A.dwg

AMES Engineering, Inc.
 CONSULTING ENGINEERS
 6330 Belmont Road, Suite 4B
 Downers Grove, IL 60515

USER NAME = arosalva	DESIGNED - JAR	REVISED -
PLOT SCALE = \$SCALE\$	DRAWN - MD	REVISED -
PLOT DATE = 12/10/2020	CHECKED - AS	REVISED -
	DATE - 12/10/2020	REVISED -

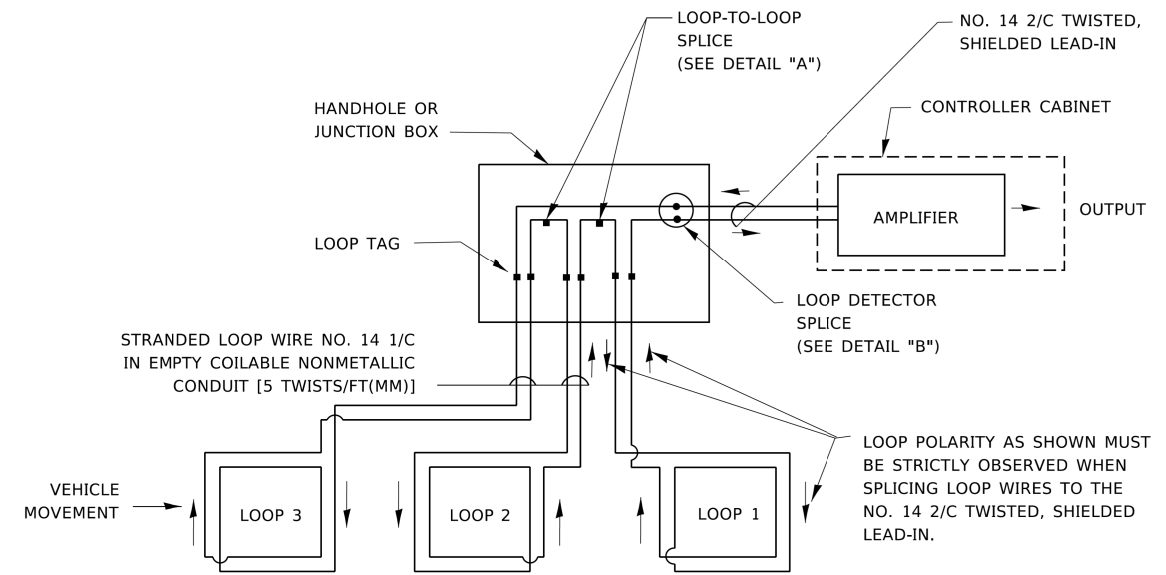
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

W. CERMAK RD (KEDZIE AVE TO DAMEN AVE & ASHLAND AVE TO HALSTED ST)
SIDEWALK DETAIL PLAN
 SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	33
CONTRACT NO. 62L95				
ILLINOIS FED. AID PROJECT				

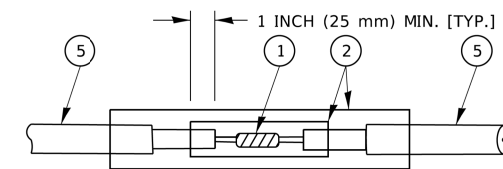
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

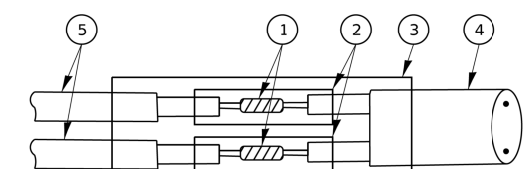


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



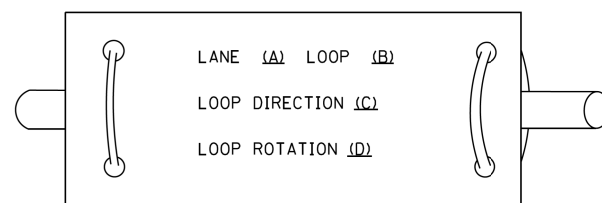
DETAIL "A"
LOOP-TO-LOOP SPLICE



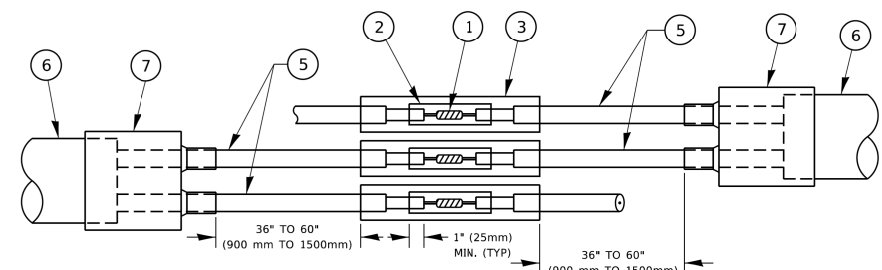
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP

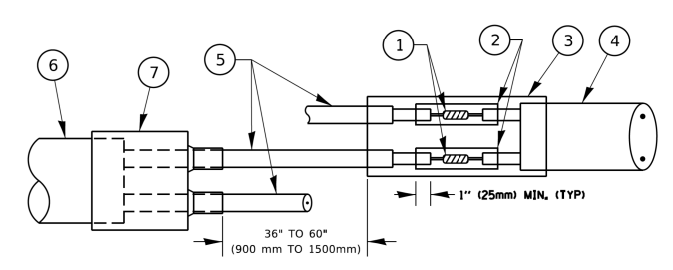
LOOP LEAD-IN CABLE TAG



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PREFORMED LOOP

LOOP DETECTOR SPLICE

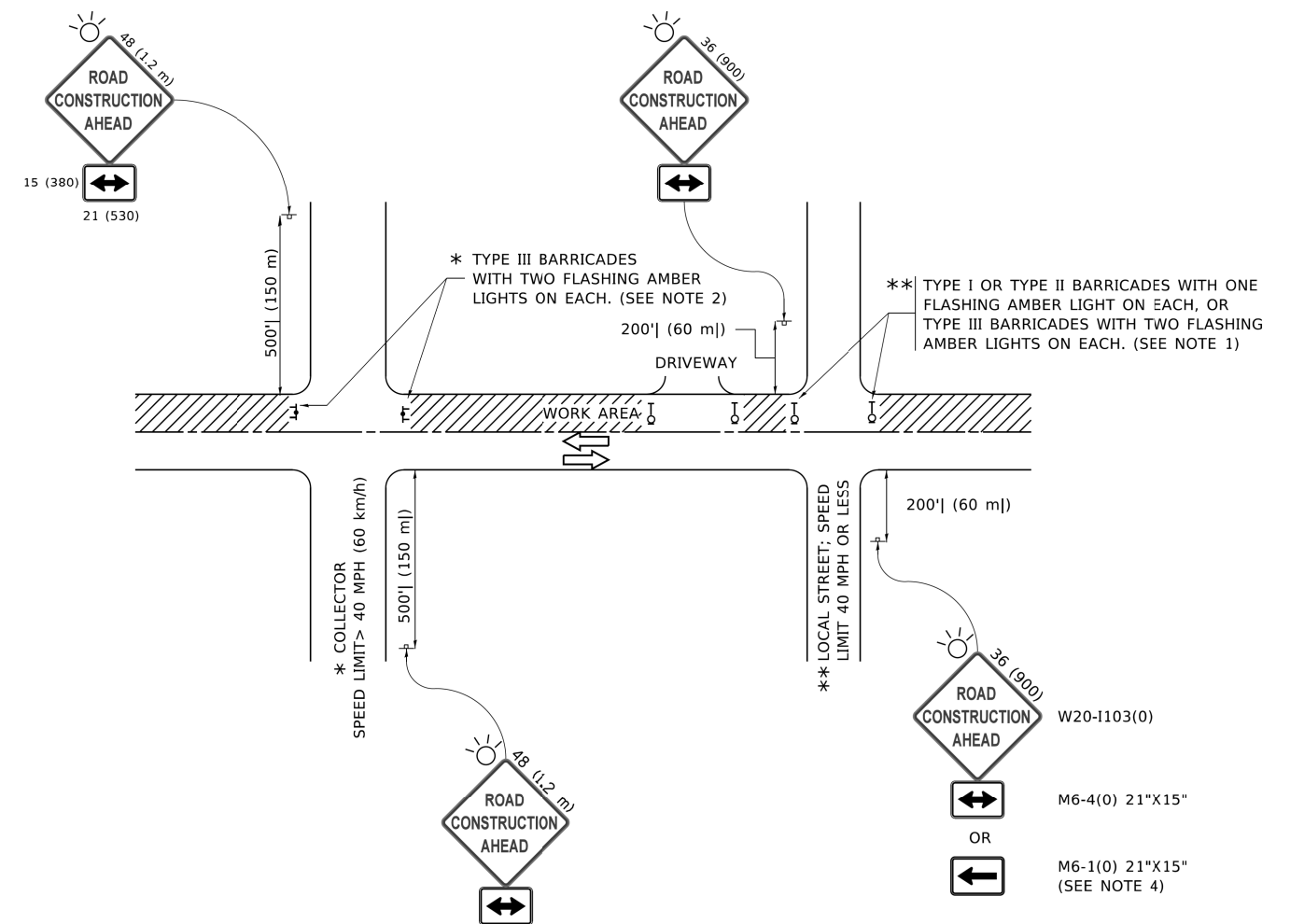
- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PREFORMED LOOP
- ⑥ XL POLYOLEFIN 2 CONDUCTOR
- ⑦ BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	34
TS-05			CONTRACT NO. 62L95	
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
FILE: \\mtrc-pw\ill0848EB\DOT\EG\Illinois.gov\PI\DOT\Documents\DOT - Offices\District 1\Projects\Sub\22\23\CAD\Drawn\CAD\Sheets\10.dgn

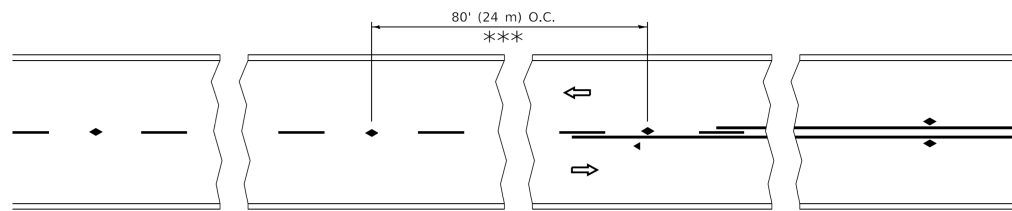
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PLOT DATE = 3/4/2019	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

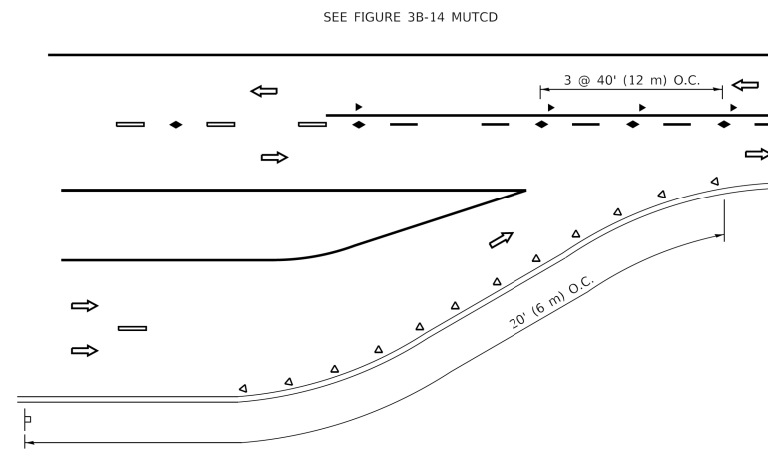
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-R5 & SW	COOK	51	35
TC-10			CONTRACT NO. 62L95	
ILLINOIS FED. AID PROJECT				

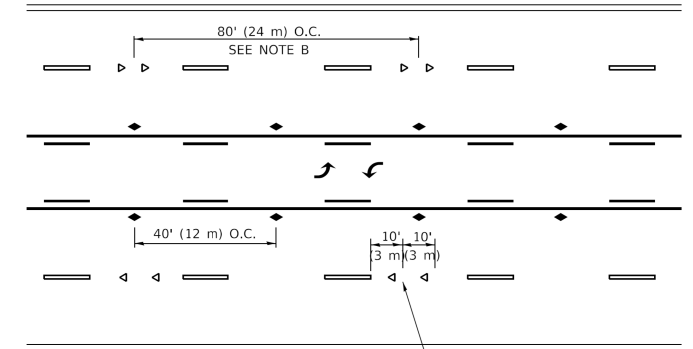


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

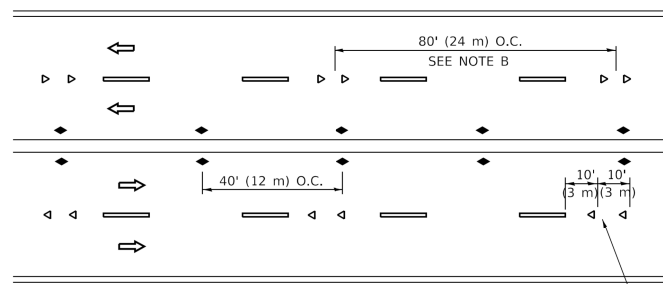
TWO-LANE/TWO-WAY



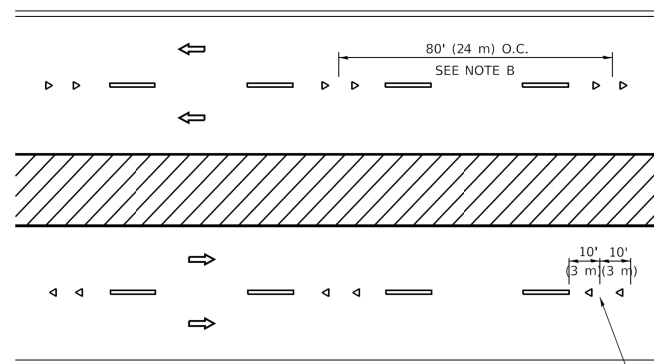
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

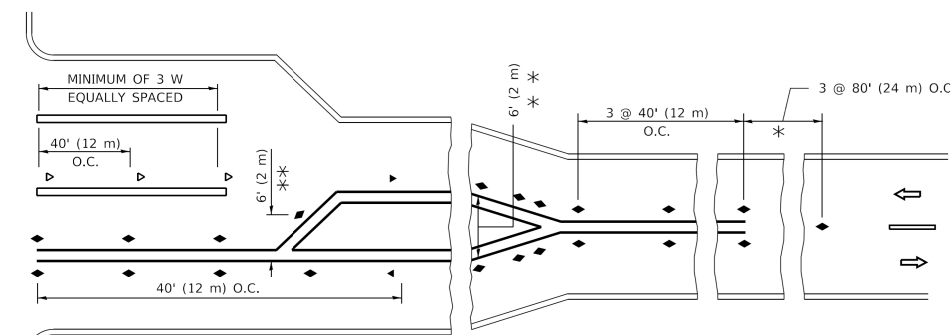
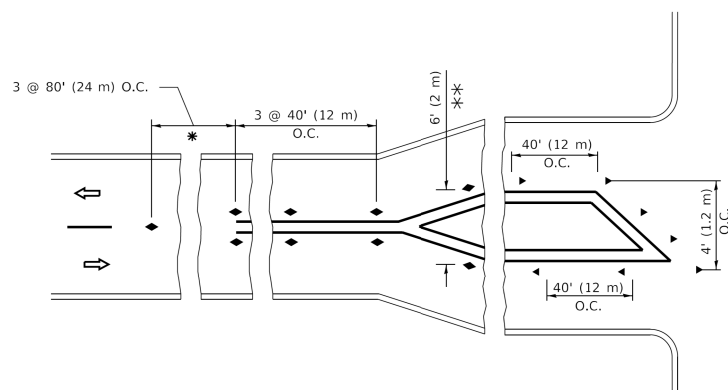
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



TURN LANES

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

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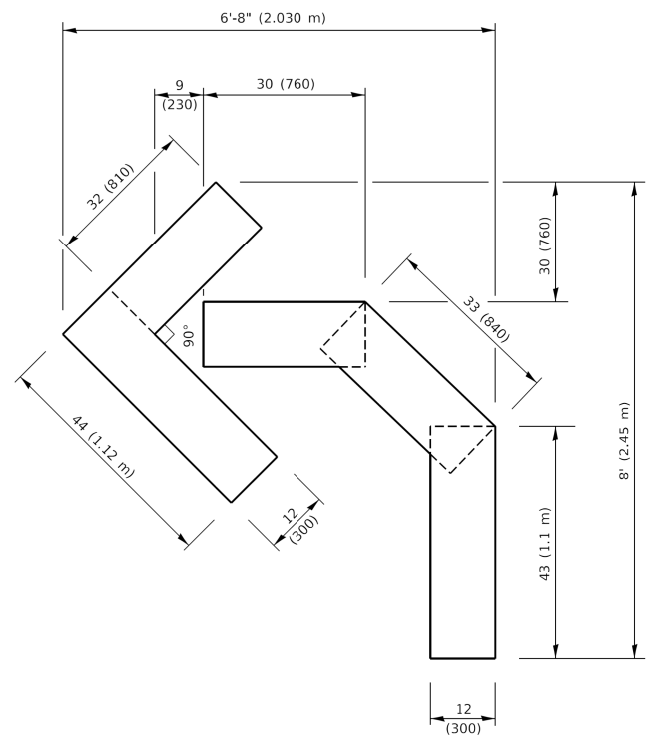
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	DRAWN -	REVISED - T. RAMMACHER 01-06-00
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PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 07-01-13

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

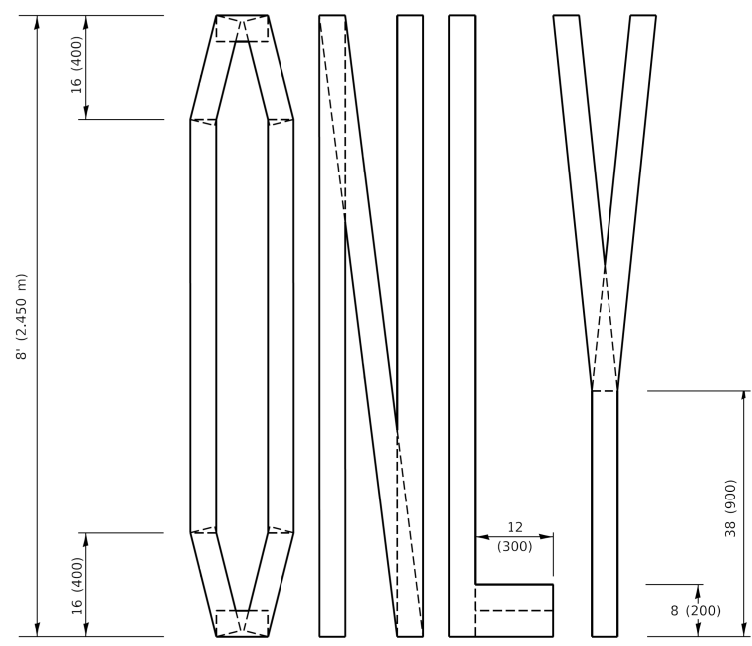
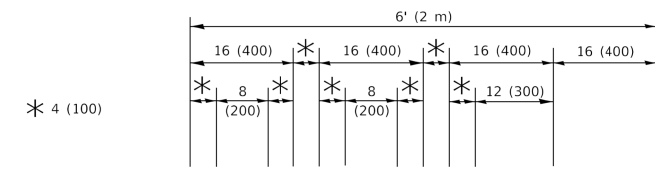
**TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

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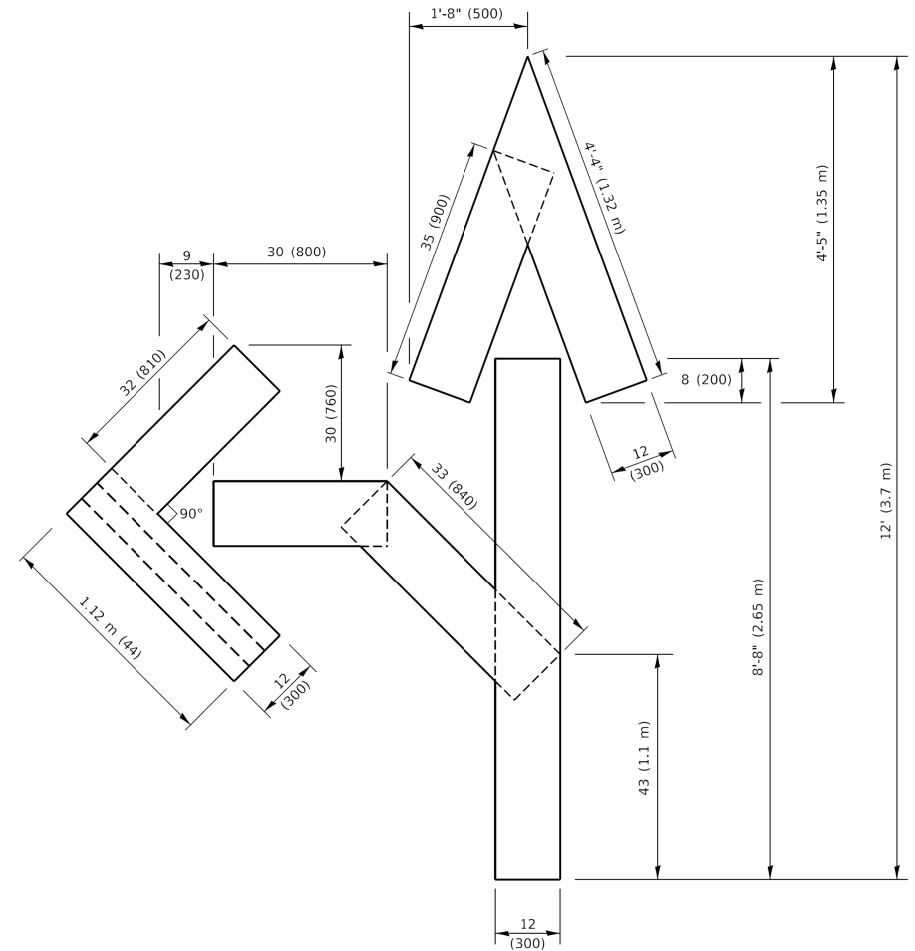
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	36
TC-11		CONTRACT NO. 62L95		
ILLINOIS		FED. AID PROJECT		



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

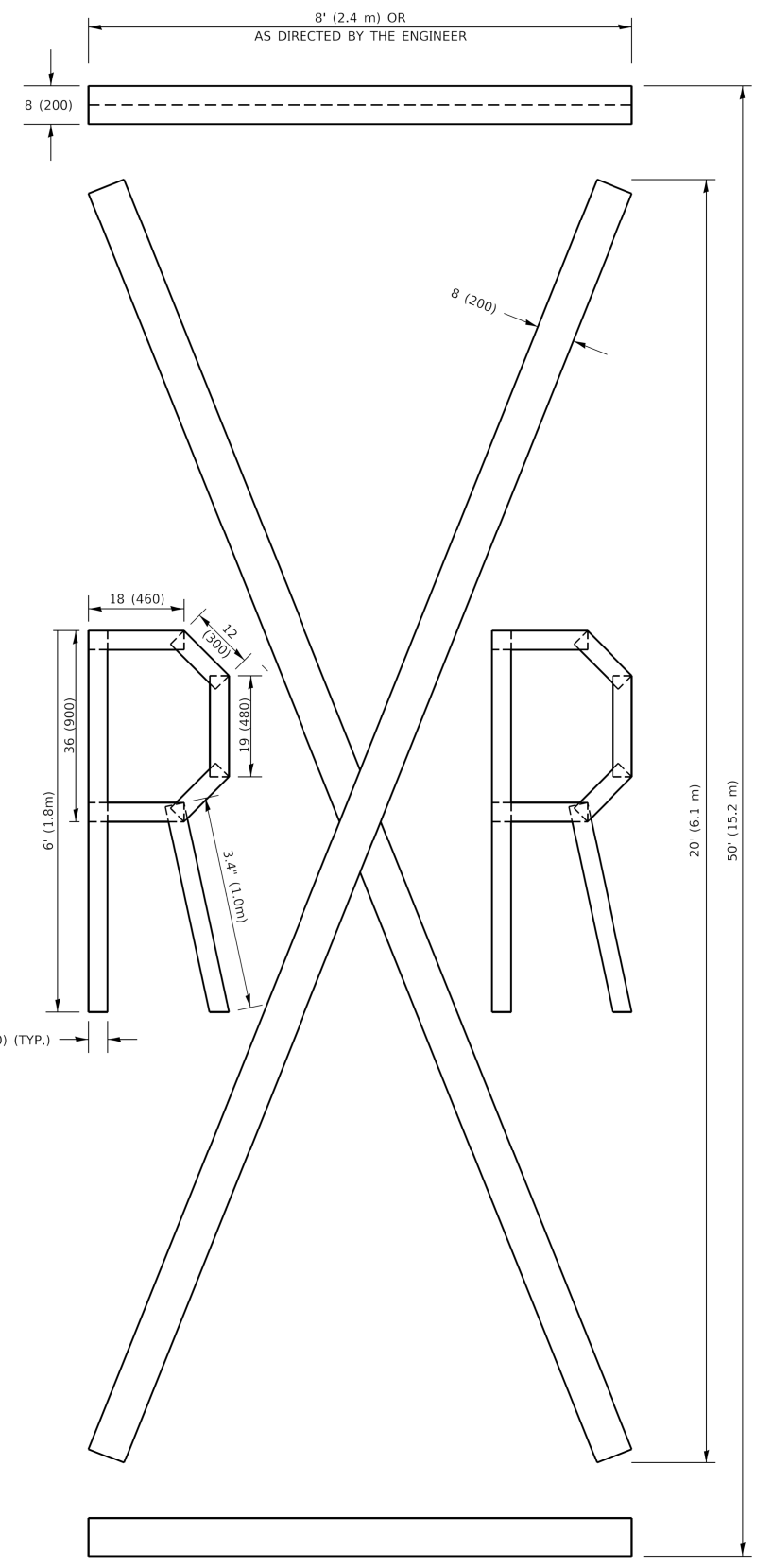


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

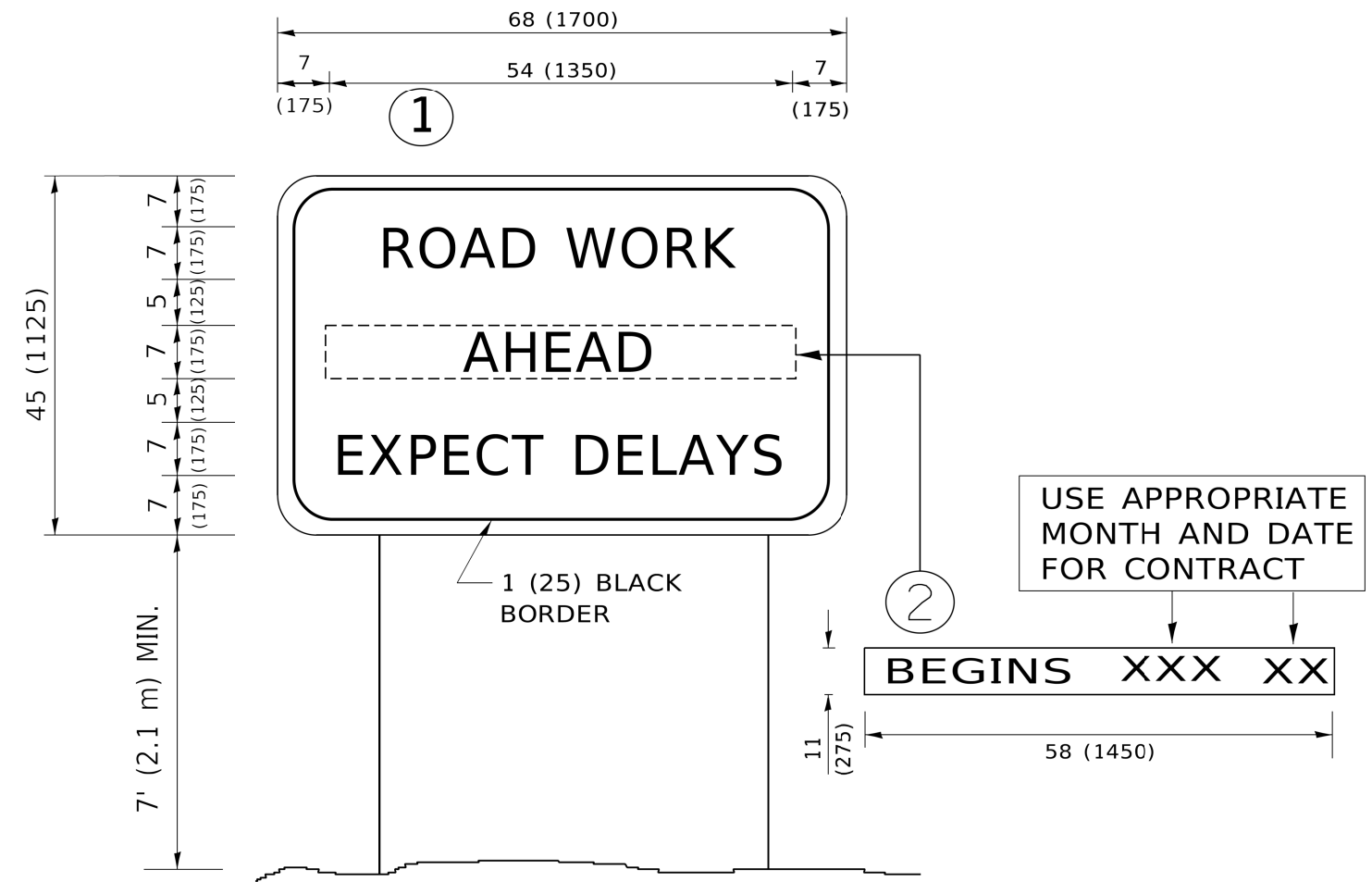
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PLOT DATE = 3/4/2019	CHECKED -	REVISED - E. GOMEZ 08-28-00
	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

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 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
 SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	37
TC-16			CONTRACT NO. 62L95	
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

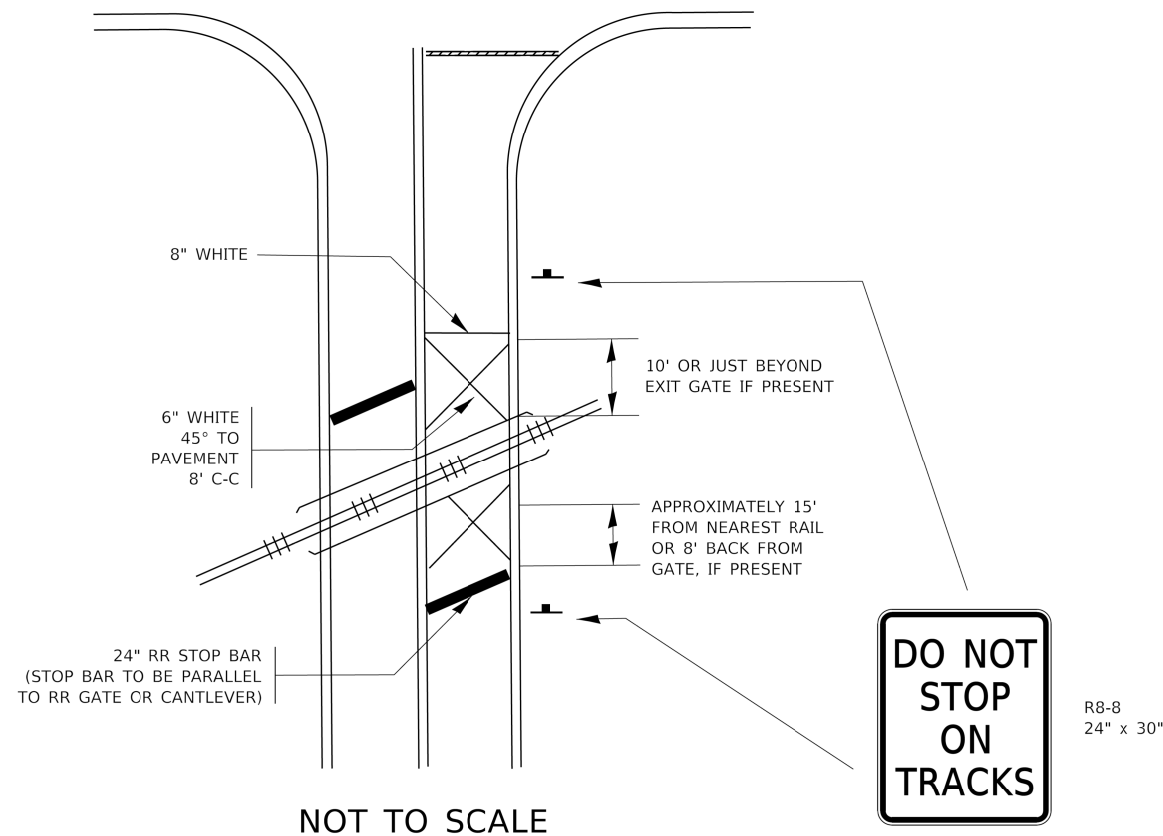
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	38
TC-22			CONTRACT NO. 62L95	
ILLINOIS FED. AID PROJECT				

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS

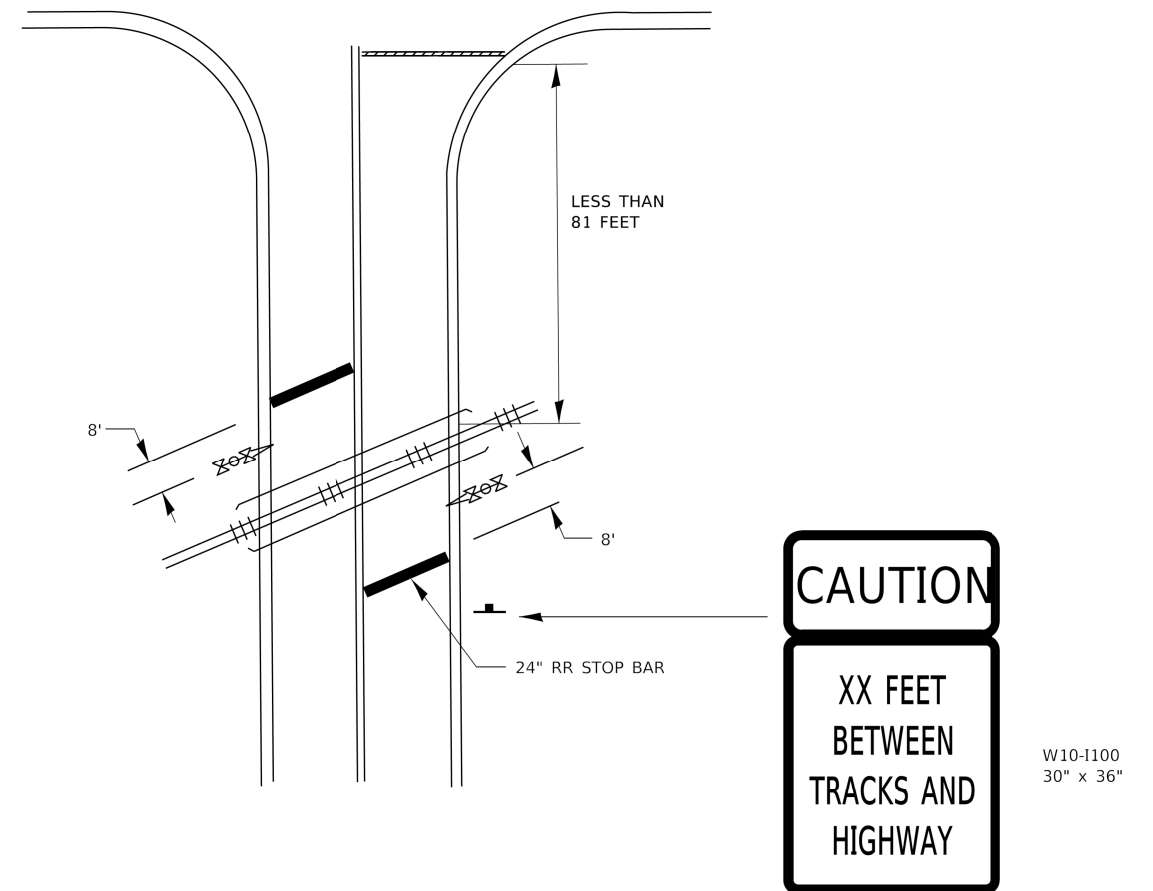
WITH SIGNALIZED INTERSECTION



NOTE:

- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION. (SEE DETAIL FOR PRE-SIGNALS).

WITH NON-SIGNALIZED INTERSECTION 81' OR LESS TO CLOSEST RAIL



NOTE:

- DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET FROM THE RAIL CLOSEST TO THE INTERSECTION OR FROM THE CLOSEST POINT ALONG THE EXIT GATE IF PRESENT OVER THE ROADWAY WHEN IN THE LOWERED POSITION TO THE STOP BAR OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET. WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
- THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6 FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKING EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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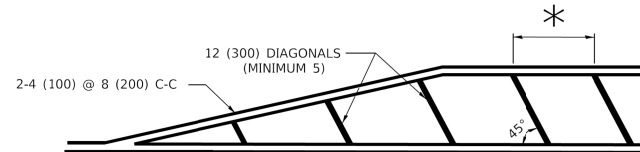
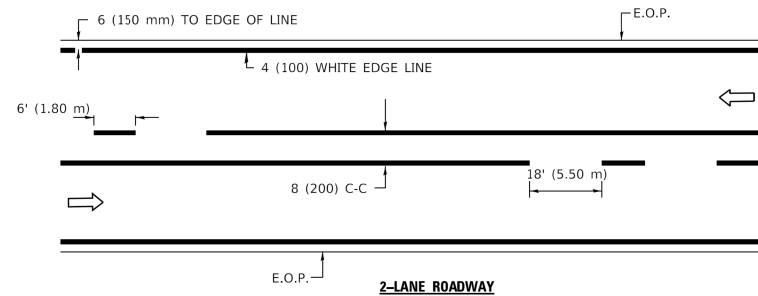
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

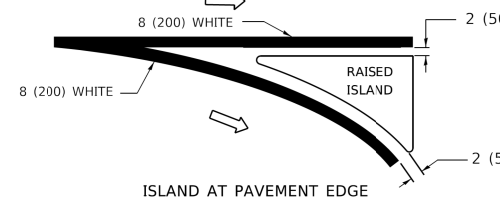
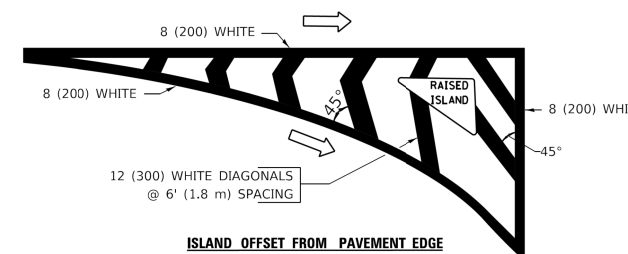
TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING
TREATMENT FOR RAILROAD CROSSINGS

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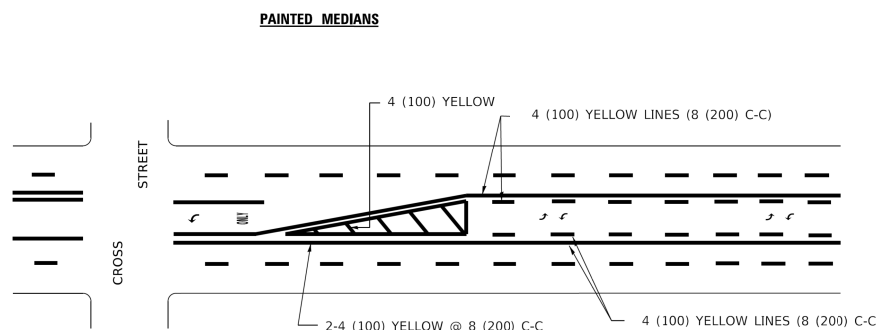
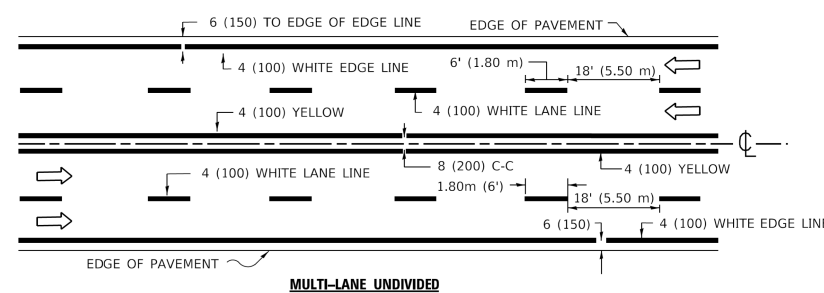
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TC-23			CONTRACT NO. 62L95	
ILLINOIS FED. AID PROJECT				



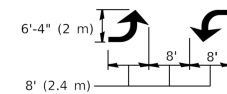
* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 * DIAGONAL LINE SPACING: 20' (6.1 m) C-C



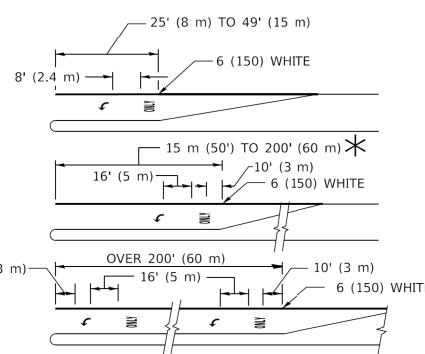
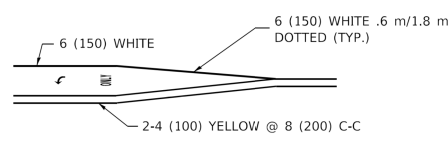
TYPICAL ISLAND MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

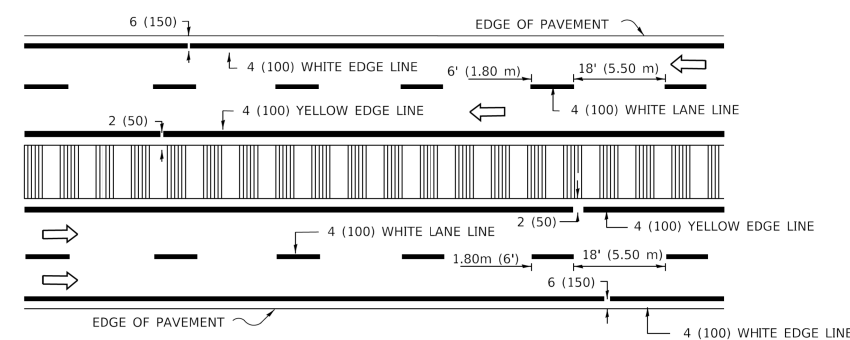
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK. IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

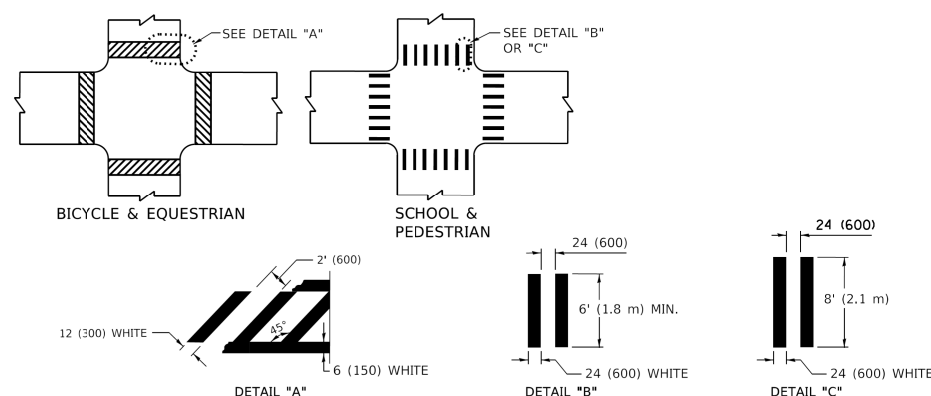
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

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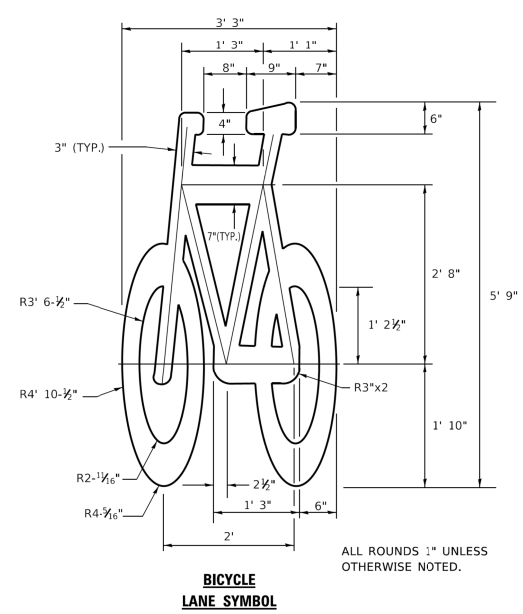
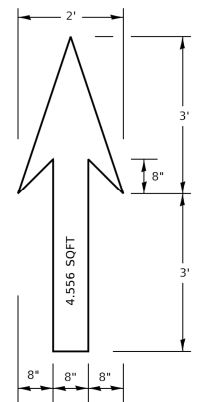
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PLOT DATE = 3/4/2019	CHECKED -	REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
 TYPICAL PAVEMENT MARKINGS

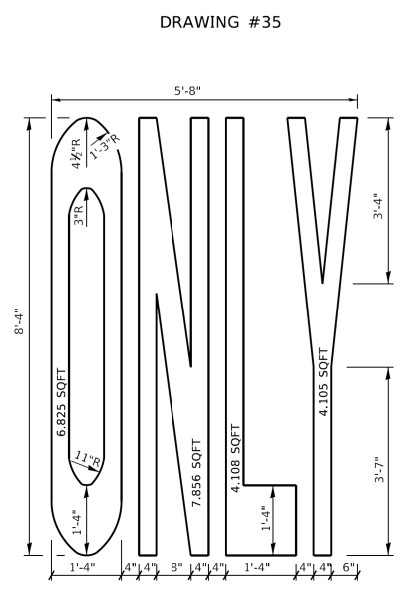
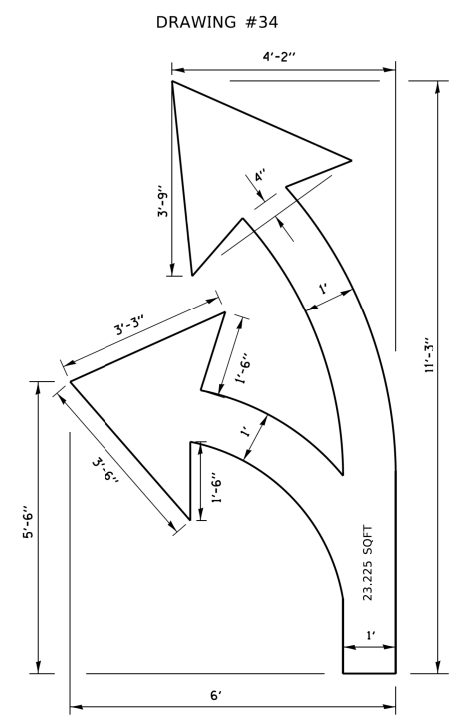
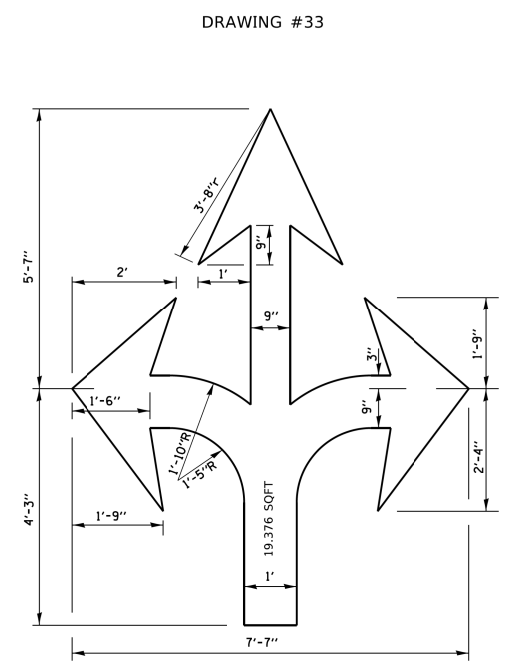
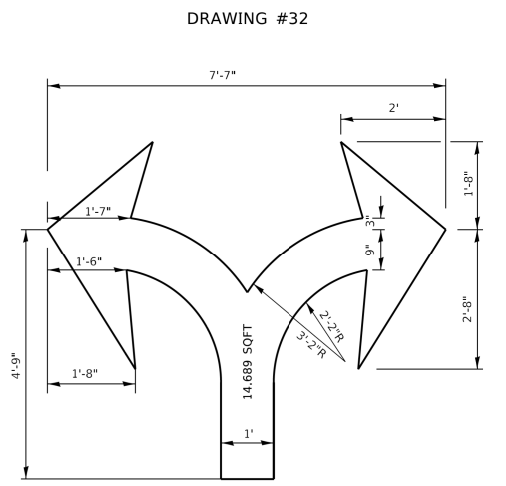
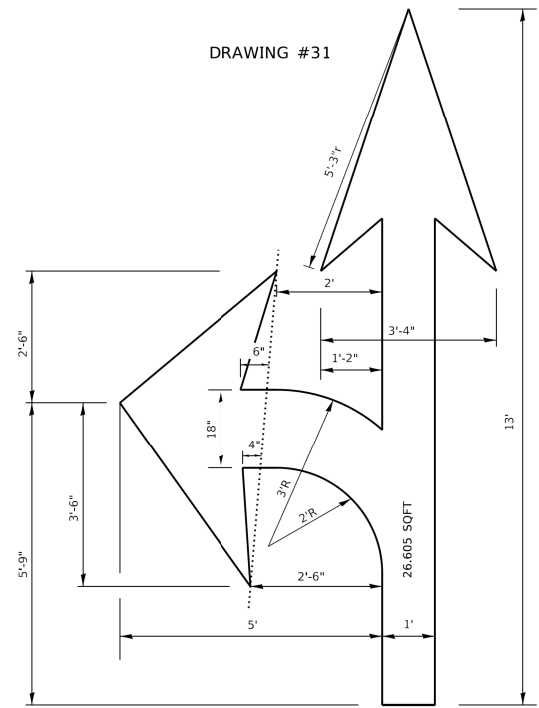
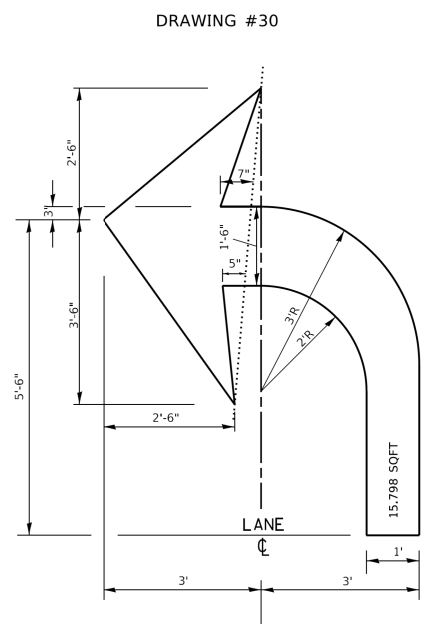
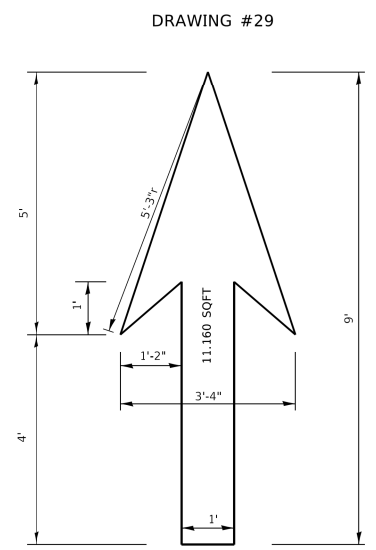
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-R5 & SW	COOK	51	41
TC-24		CONTRACT NO. 62L95		
ILLINOIS FED. AID PROJECT				



- NOTE:
- FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
 - THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28



NOTE:
ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

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PLOT DATE = 3/4/2019	CHECKED -	REVISED -
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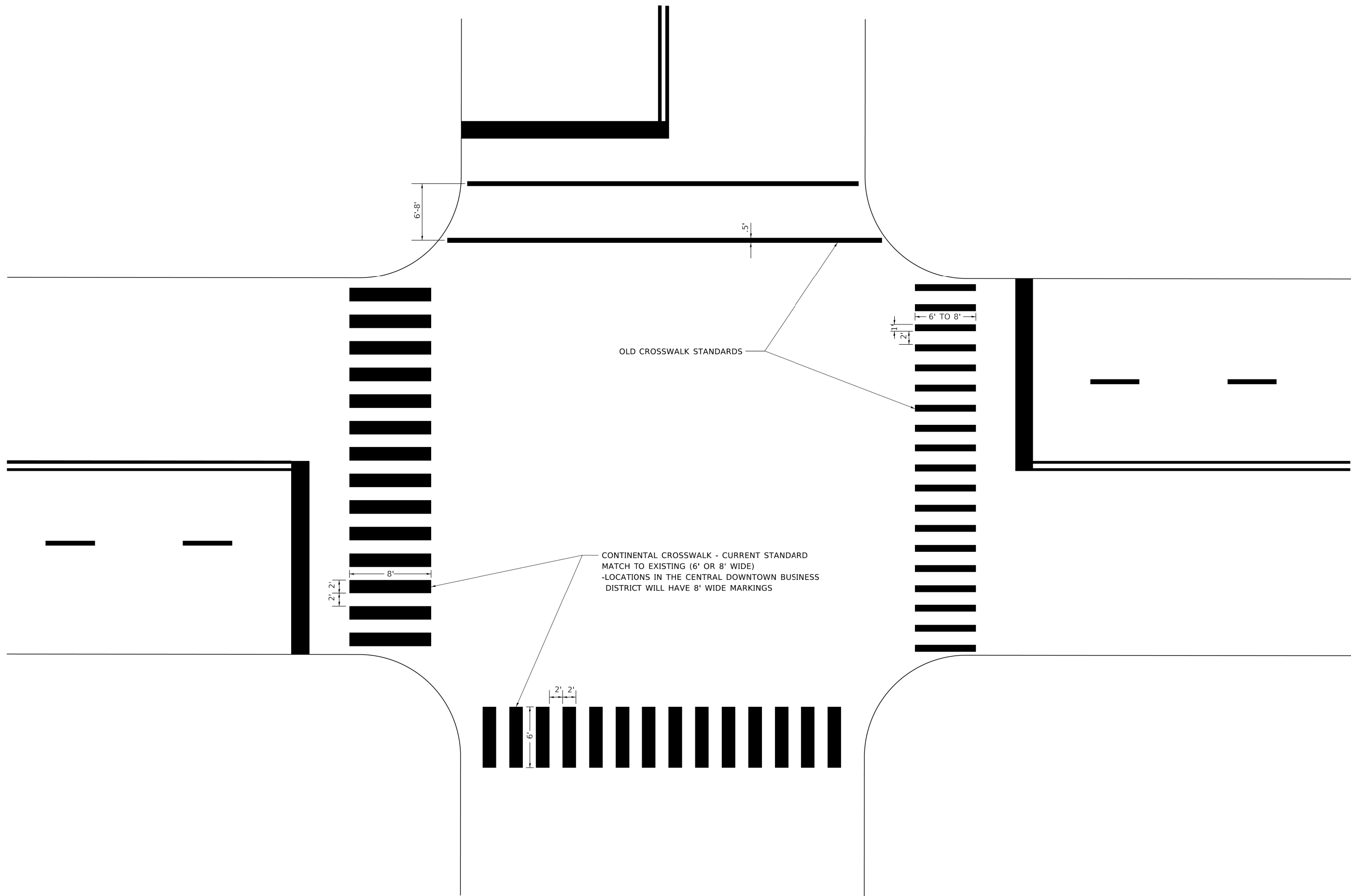
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	42
TC-24		CONTRACT NO. 62L95		
ILLINOIS FED. AID PROJECT				

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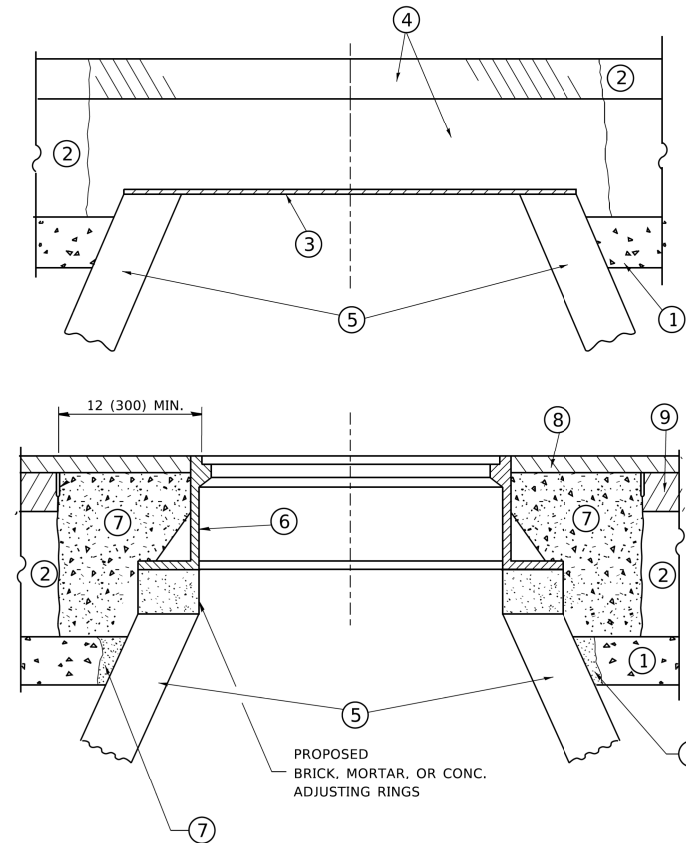
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO
 TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-R5 & SW	COOK	51	43
TC-24			CONTRACT NO. 62L95	
ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 * CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1 *CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

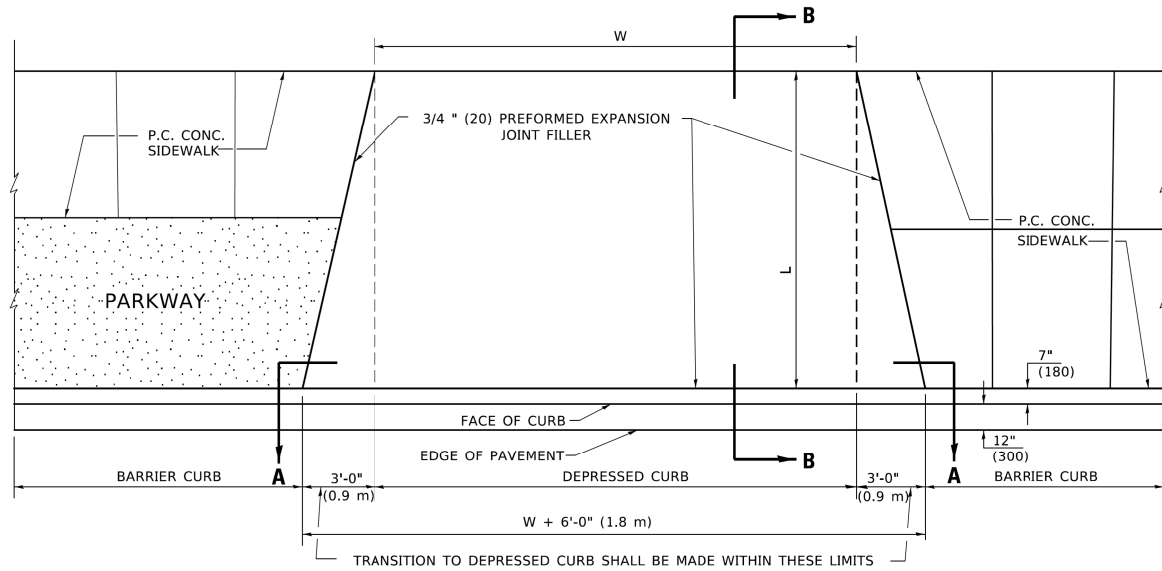
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-R5 & SW	COOK	51	44
BD600-03 (BD-8)		CONTRACT NO. 62L95		
ILLINOIS FED. AID PROJECT				

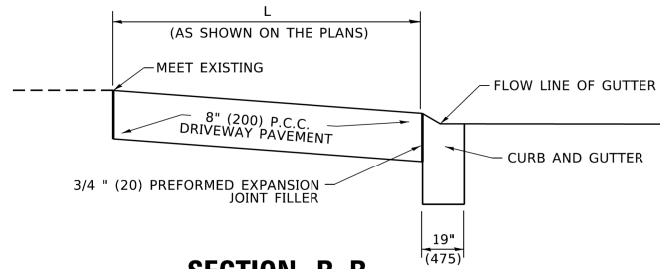
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	DRAWN -	REVISED - R. BORO 01-01-07
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PLOT DATE = 3/27/2019	DATE - 10-25-94	REVISED - R. BORO 12-06-11



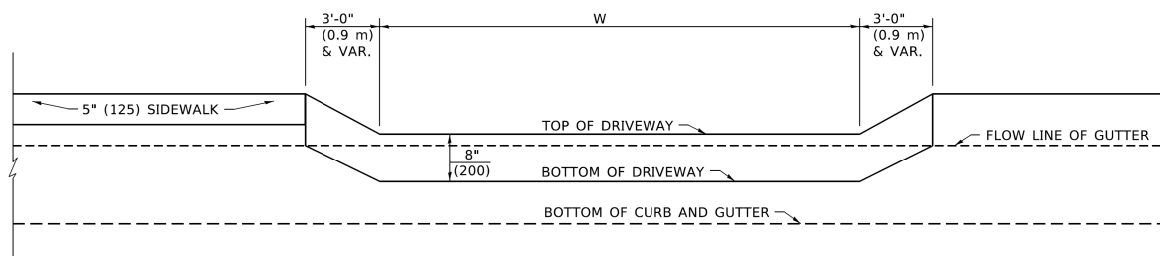
PLAN VIEW

NOTES:

1. EXPANSION JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE DETAILS FOR P.C.C. SIDEWALK.
2. THE CURB BETWEEN ADJACENT DRIVEWAYS SHALL BE FULL HEIGHT FOR A DISTANCE OF AT LEAST FOUR FEET (1.2 METERS).
3. P.C. CONCRETE DRIVEWAYS SHALL BE CONSTRUCTED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
4. 3/4" (20) PREFORMED EXPANSION JOINTS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO P.C.C. DRIVEWAY PAVEMENT 8" (200).
5. COMBINATION CONC. CURB AND GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE TRANSITION CURB AND GUTTER.

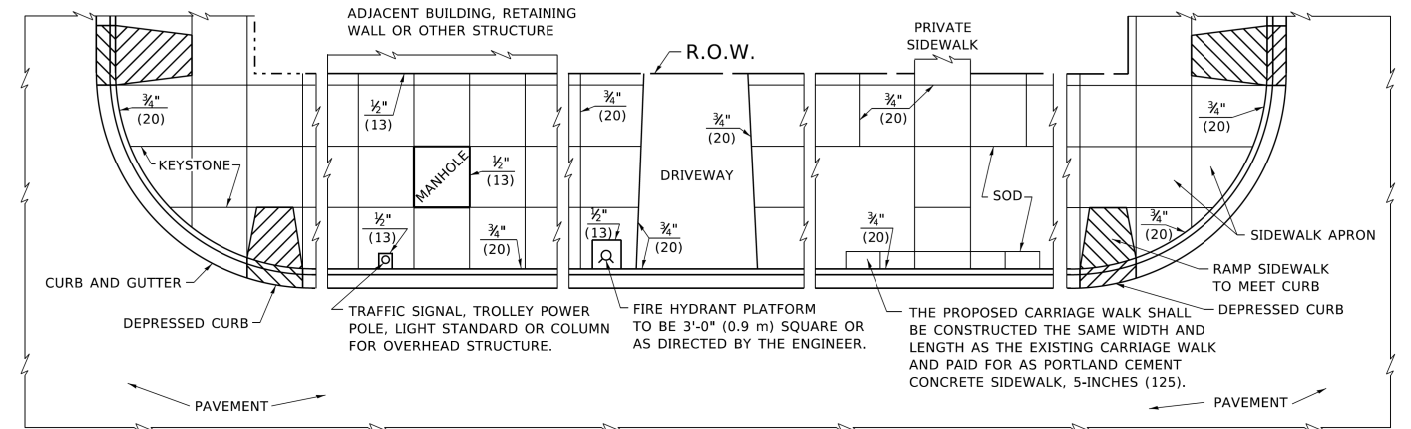


SECTION B-B



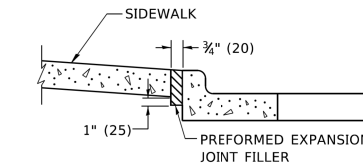
SECTION A-A

P.C.C. DRIVEWAY PAVEMENT DETAIL



NOTES:

1. ONE-HALF INCH THICK EXPANSION JOINTS SHALL BE PLACED BETWEEN THE SIDEWALK AND ALL STRUCTURES SUCH AS LIGHT STANDARDS, TRAFFIC LIGHT STANDARDS, MANHOLES, WHICH EXTEND THROUGH THE SIDEWALK.
2. 3/4" (20) THICK EXPANSION JOINTS SHALL BE PLACED AT INTERVALS OF NOT MORE THAN 100 FEET (30 METERS) IN THE SIDEWALK. WHERE THE SIDEWALK IS CONSTRUCTED ADJACENT TO PAVEMENT OR CURB HAVING EXPANSION JOINTS, THE EXPANSION JOINTS IN THE SIDEWALK SHALL BE PLACED OPPOSITE THE EXISTING EXPANSION JOINTS AS NEARLY AS PRACTICABLE. EXPANSION JOINTS SHALL ALSO BE PLACED WHERE THE SIDEWALK ABUTS EXISTING SIDEWALKS, BETWEEN DRIVEWAY PAVEMENT AND SIDEWALK, AND BETWEEN SIDEWALK AND CURBS WHERE THE SIDEWALK ABUTS THE CURB.

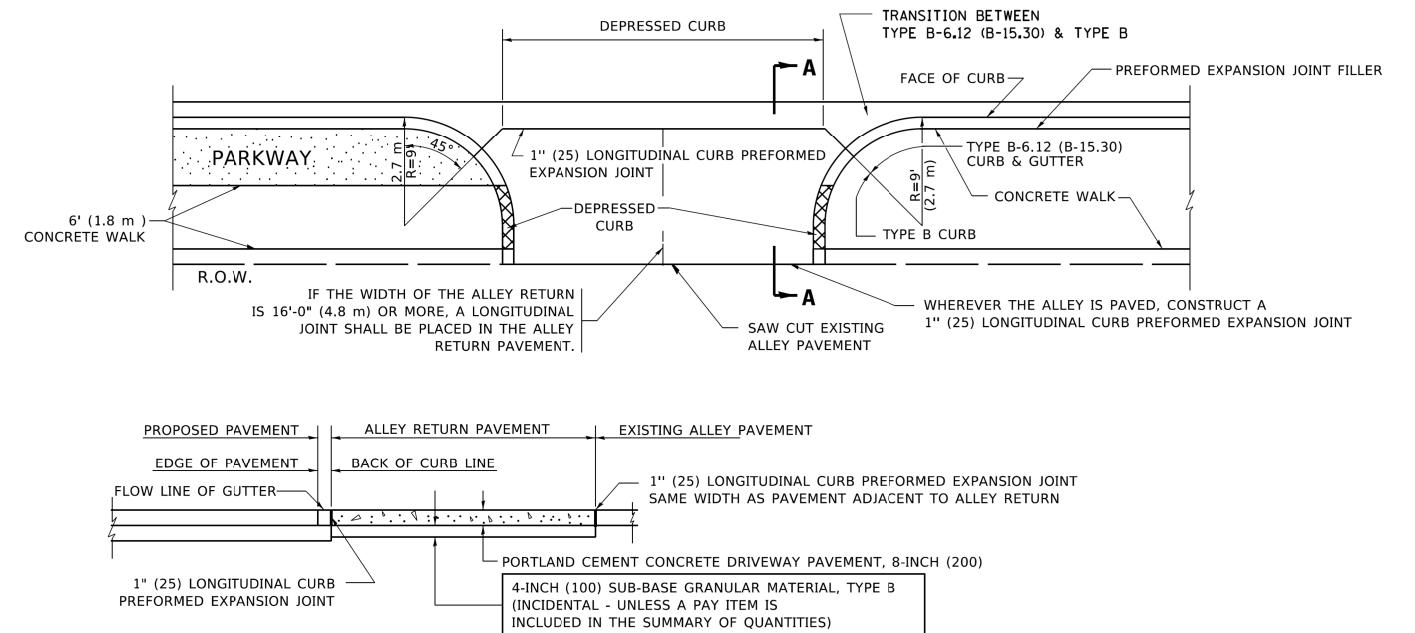


SLOPE FOR SIDEWALK
1" (25) IN 3'-0" (0.9 m) IN CHICAGO

PORTLAND CEMENT CONCRETE SIDEWALK DETAILS

NOTES:

NO EXTRA COMPENSATION SHALL BE ALLOWED FOR THE GUTTER FLARE



SECTION A-A

ALLEY RETURN DETAIL

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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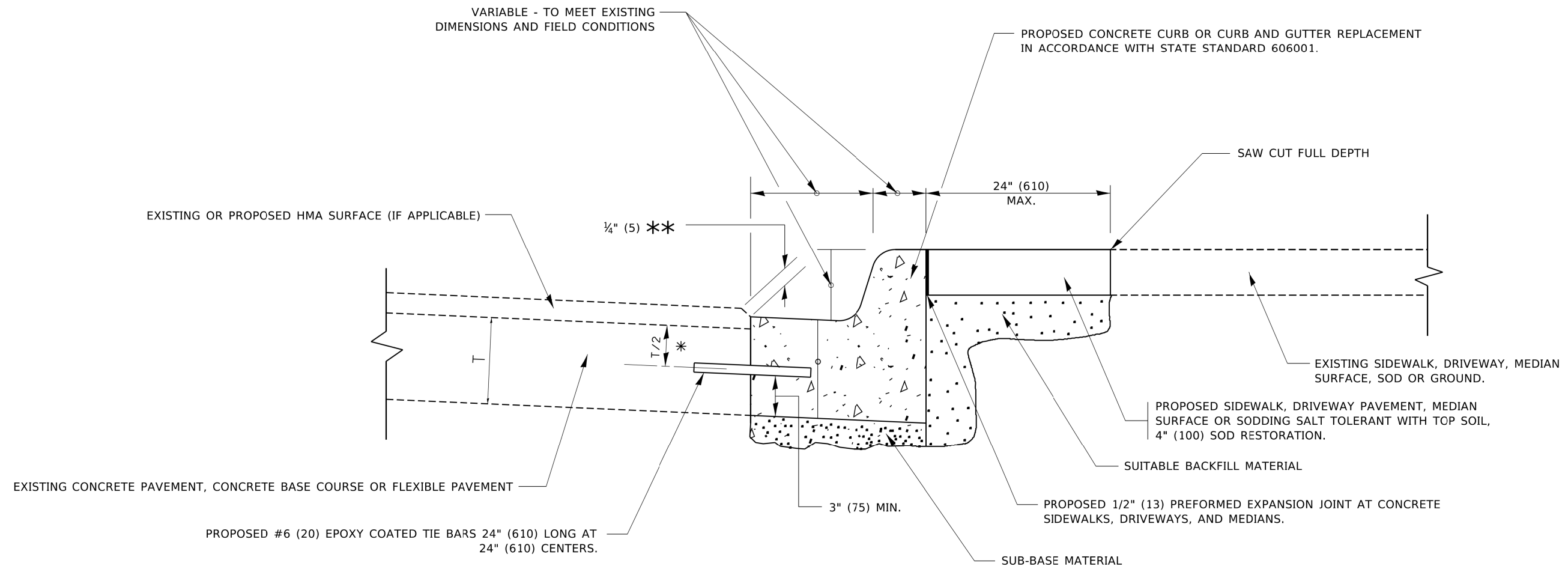
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PLOT DATE = 3/27/2019	DATE - 06-13-90	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO
DETAILS FOR P.C. CONCRETE DRIVEWAY, ALLEY RETURN AND SIDEWALK**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	45
BD400-03 (BD-17)		CONTRACT NO. 62L95		
ILLINOIS / FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

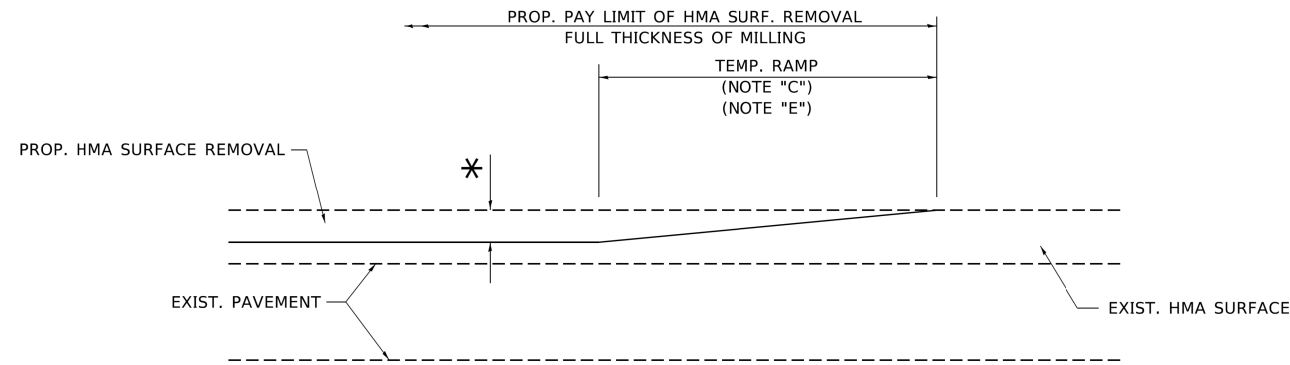
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USER NAME = footemj	DESIGNED - A. HOUSEH	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 50.0000' / 1"	CHECKED -	REVISED - R. BORO 12-15-09
PLOT DATE = 7/11/2019	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

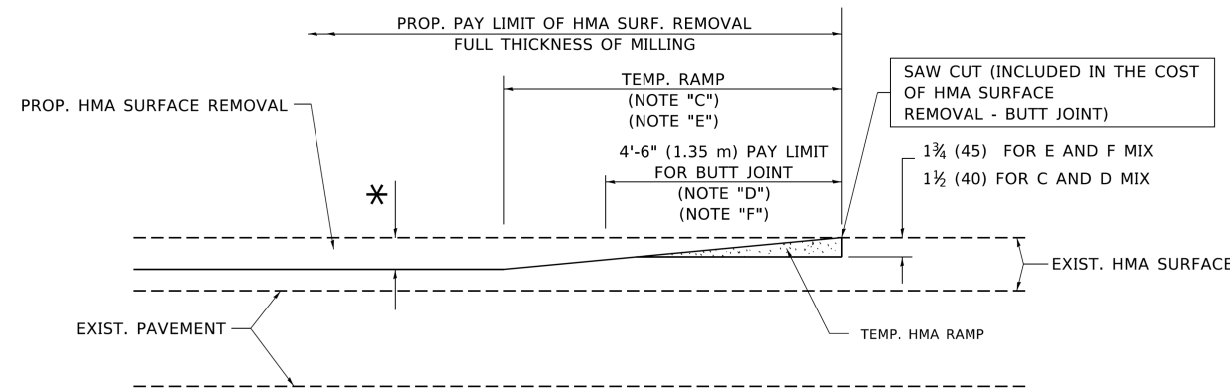
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	47
BD600-06 (BD-24)			CONTRACT NO. 62L95	
ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

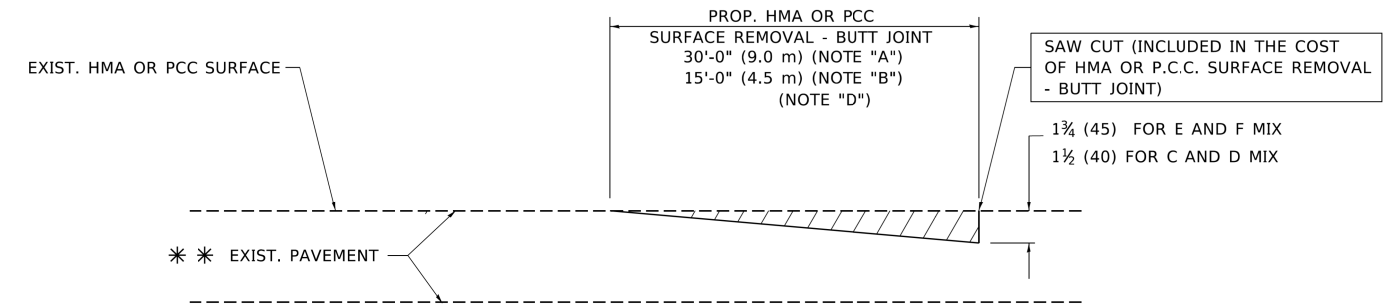


HMA CONSTRUCTED TEMPORARY RAMP

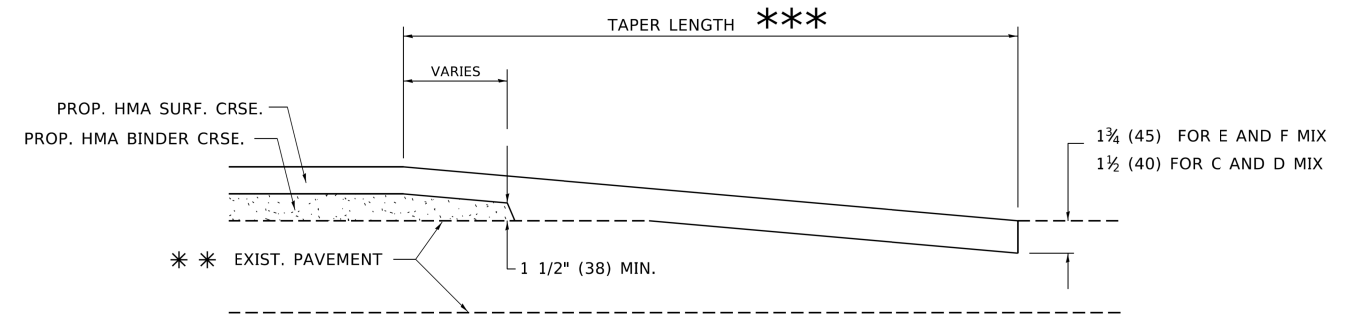
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

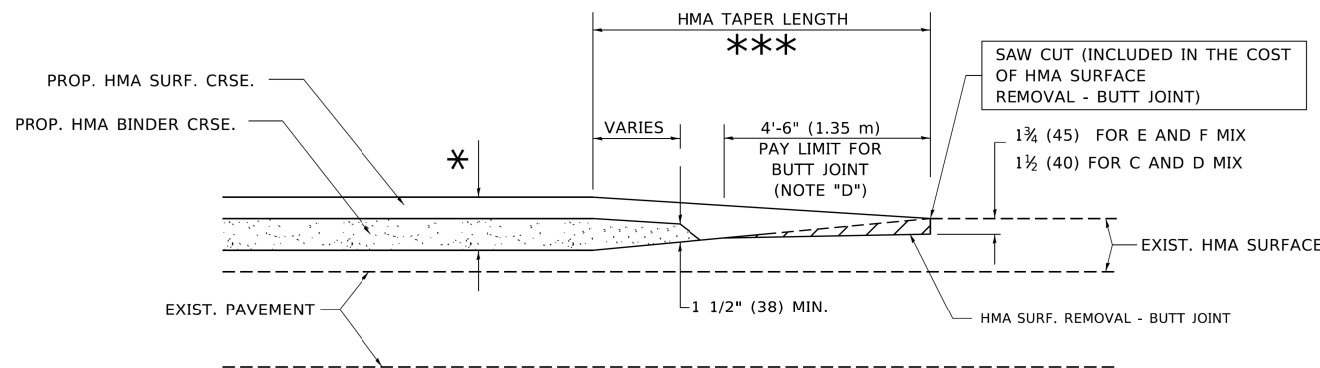
NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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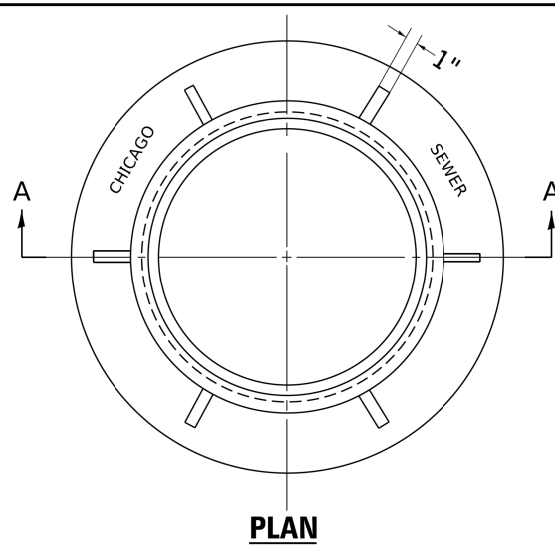
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

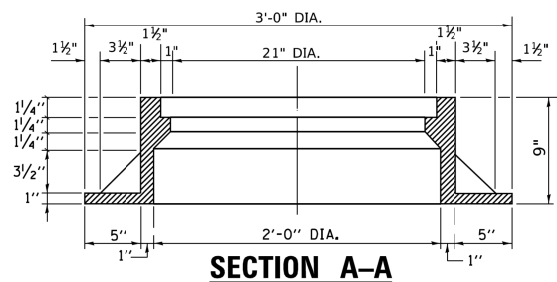
**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-RS & SW	COOK	51	48
BD400-05 BD32		CONTRACT NO. 62L95		
ILLINOIS FED. AID PROJECT				



PLAN

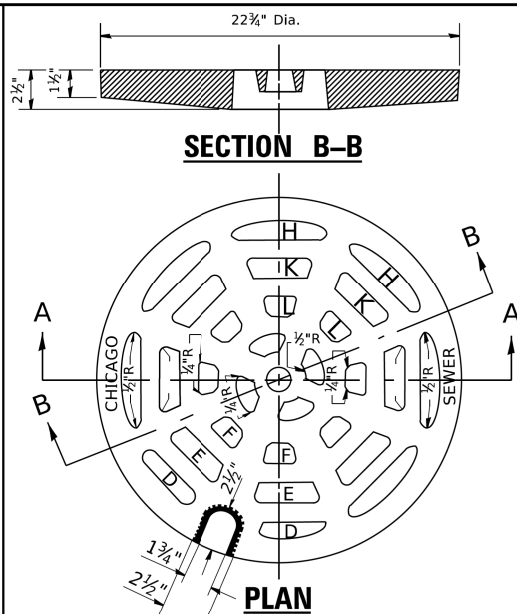


SECTION A-A

NOTE:
METAL PLATES MUST BE FURNISHED FOR PERFORATED LIDS ON MANHOLES

CHICAGO STANDARD MANHOLE FRAME

SCALE: 1 1/2"=1'-0"
MATERIAL: CAST IRON

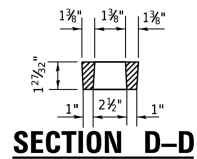


PLAN

SECTION A-A

PERFORATED LID FOR CATCH BASINS & MANHOLES

SCALE: 2"=1'-0"
MATERIAL: CAST IRON



SECTION D-D

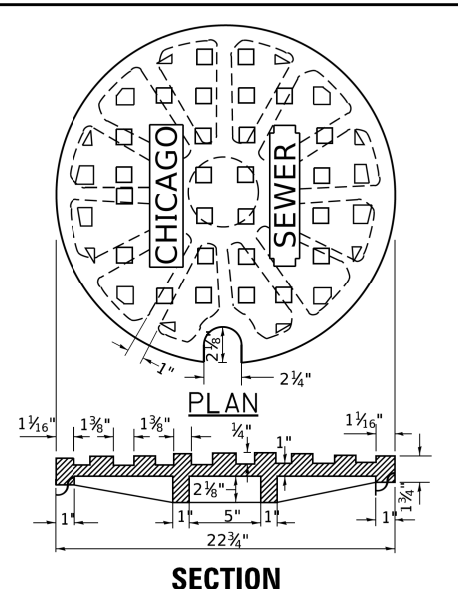
SECTION E-E

SECTION F-F

SECTION H-H

SECTION K-K

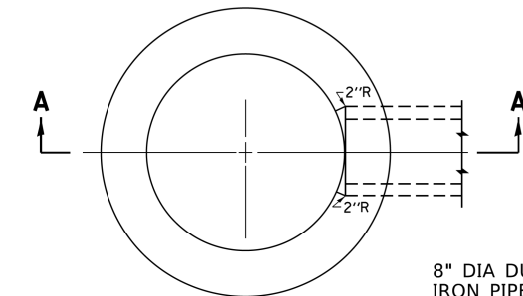
SECTION L-L



SECTION

SOLID LID FOR MANHOLES

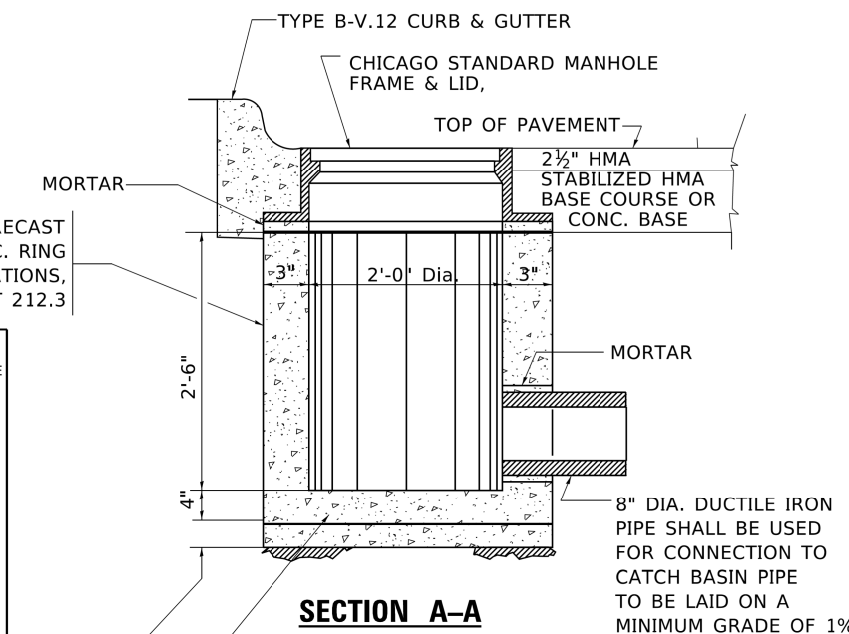
SCALE: NONE
MATERIAL: CAST IRON



PLAN

(FRAME & LID NOT SHOWN)

8" DIA DUCTILE IRON PIPE, PART OF ITEM 7G



SECTION A-A

REINFC. CONC. BASE CAST AS INTEGRAL PART OF 24" DIA. PRECAST CONC. RING
6" MINIMUM GRANULAR EMBEDMENT UNDER ALL INLETS. FURNISHING AND INSTALLING GRANULAR EMBEDMENT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 12

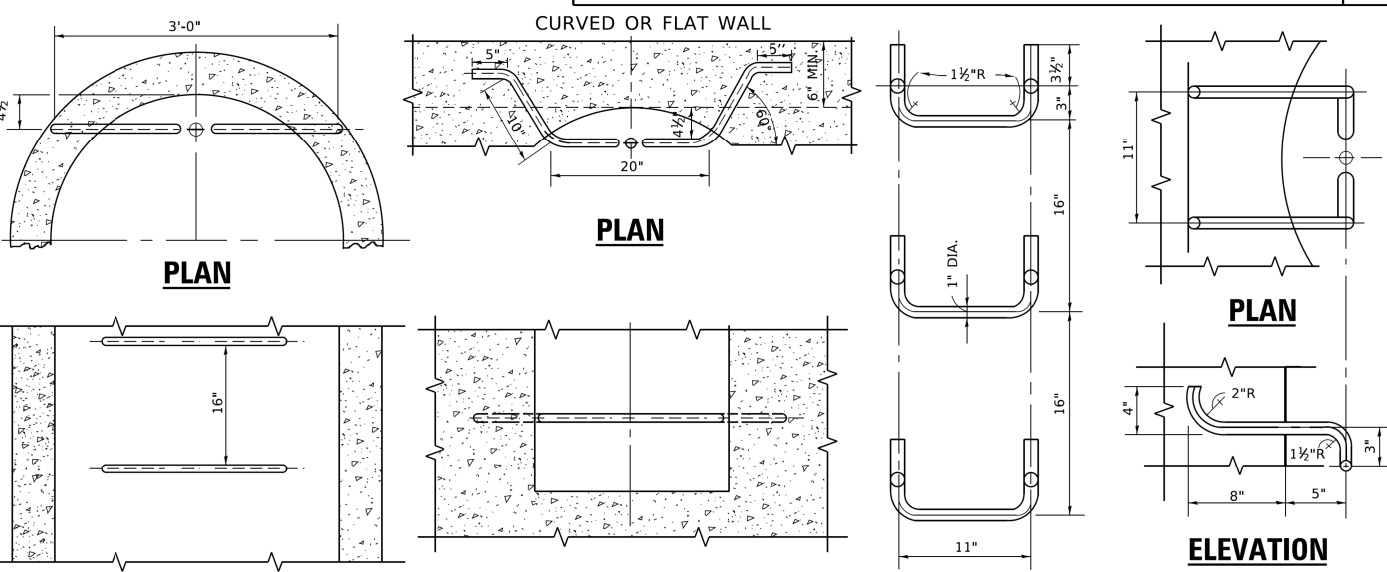
STANDARD INLETS

SCALE 1"=1'-0"
ITEM 12

THIS INLET DETAIL IS SOMETIMES REFERRED TO AS "CHICAGO STANDARD INLET, TYPE A"

NOTE:
INLETS SHALL NOT BE CONSTRUCTED UNLESS IT IS IMPOSSIBLE TO CONSTRUCT A CATCH BASIN. THE CONTRACTOR SHALL HAVE THE DEPARTMENT OF SEWERS APPROVAL BEFORE CONSTRUCTING INLETS.

CITY OF CHICAGO
DEPARTMENT OF SEWERS
ENGINEERING DIVISION



ELEVATION TYPE X

SCALE: 1"=1'-0"

ELEVATION TYPE Y

SCALE: 1"=1'-0"

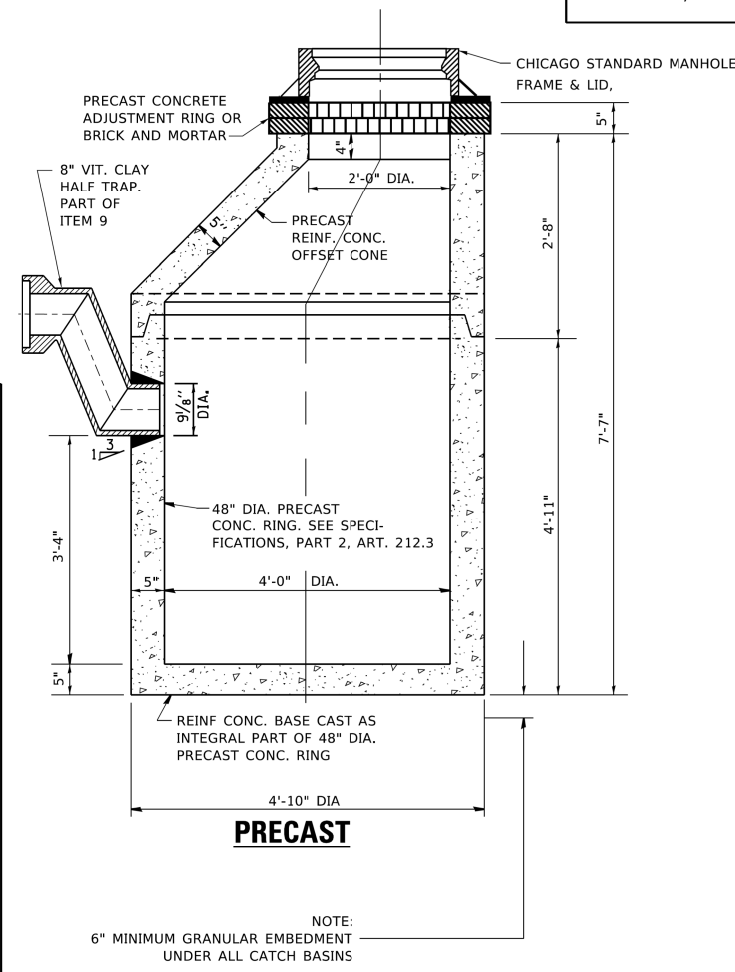
SPACING

HANDHOLD-TYPE Z RUNG

Scale: 1 1/2"=1'-0"

STANDARD LADDER RUNGS

ALL LADDER RUNGS SHALL BE ALUMINUM OR GALVANIZED WROUGHT IRON AS SPECIFIED IN THE SPECIFICATIONS, PART 2, ARTICLE 214.2. RUNGS SHALL BE 1" DIAMETER OR OF A SHAPE HAVING AN EQUIVALENT CROSS-SECTIONAL AREA.



PRECAST

NOTE:
6" MINIMUM GRANULAR EMBEDMENT UNDER ALL CATCH BASINS

STANDARD CATCH BASINS

SCALE: 3/4"=1'-0"
ITEM 9

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
CATCH BASIN, INLET AND MANHOLE DETAILS

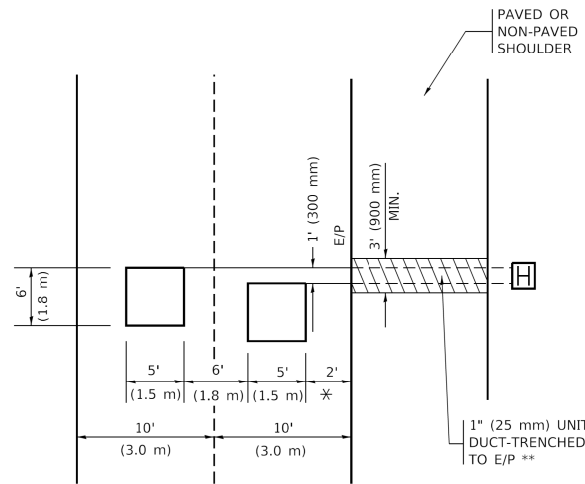
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-R5 & SW	COOK	51	49
BD600-13 (BD47)		CONTRACT NO. 62L95		
ILLINOIS FED. AID PROJECT				

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LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



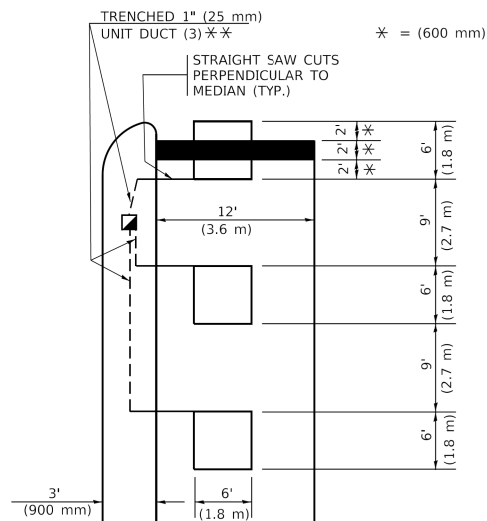
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS

VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



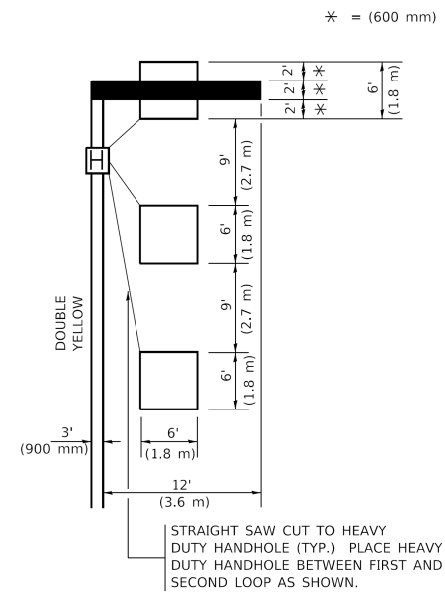
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS

VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

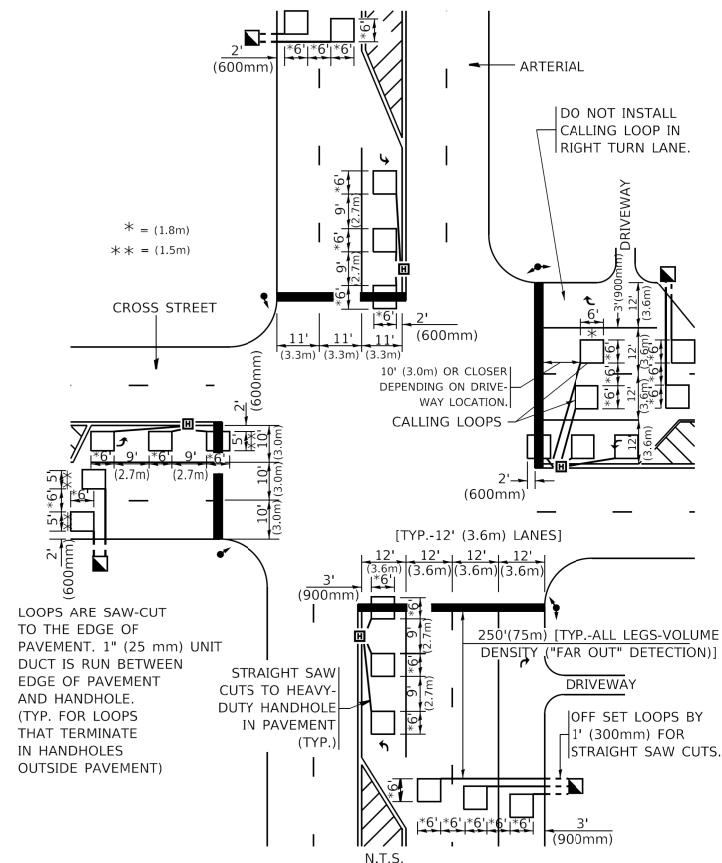
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



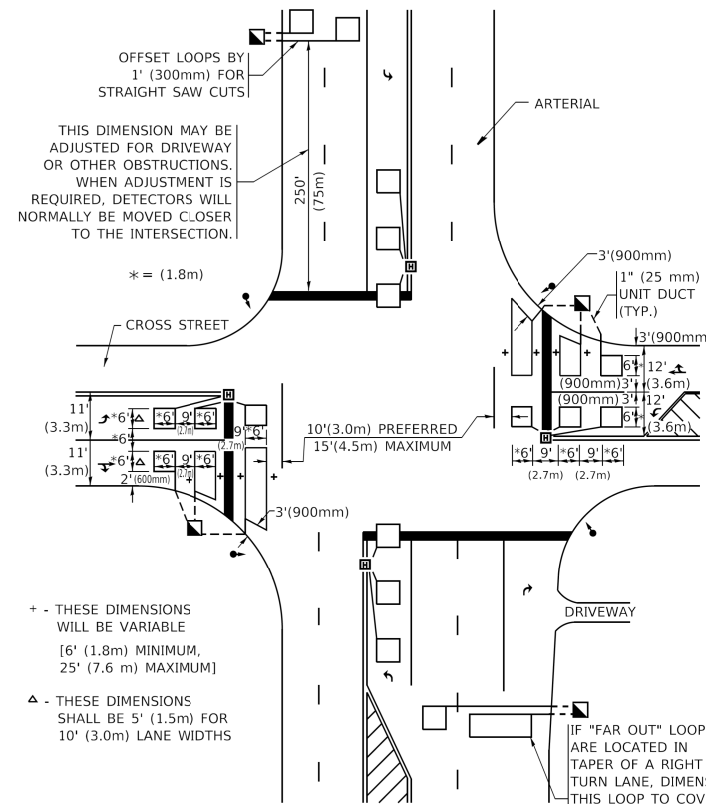
* = (1.8m)
** = (1.5m)

LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

* = (1.8m)

+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

△ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2
N.T.S.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2020-117-R5 & SW	COOK	51	51
TS-07		CONTRACT NO. 62L95		
ILLINOIS FED. AID PROJECT				