

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-AS&SW	ILLINOIS	50	1
FED. ROAD DIST. NO. 1				CONTRACT NO. 62M18

D-91-540-20



LOCATION OF SECTION INDICATED THIS: -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED: December 9, 2020

Andy J. Quarty (RAB)
REGIONAL ENGINEER

January 29, 2021

Scott A. Elk
ENGINEER OF DESIGN AND ENVIRONMENT

January 29, 2021

James J. Quinn
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION 13

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE CITY OF DES PLAINES,
CITY OF PARK RIDGE, VILLAGE OF NILES, AND
UNINCORPORATED MAINE TOWNSHIP

PROPOSED
HIGHWAY PLANS

FAU ROUTE 1319: BALLARD ROAD
RAND ROAD TO GREENWOOD AVE

SECTION: 2020-140-RS&SW
PROJECT: STP-6NJ1(425)

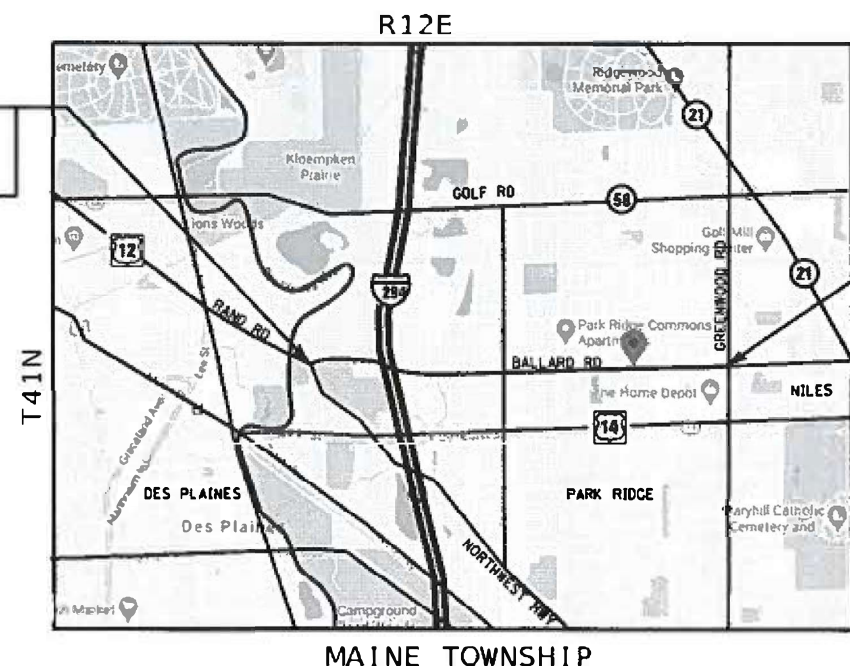
SMART OVERLAY, ADA IMPROVEMENTS

COOK COUNTY
C-91-338-20

BALLARD ROAD

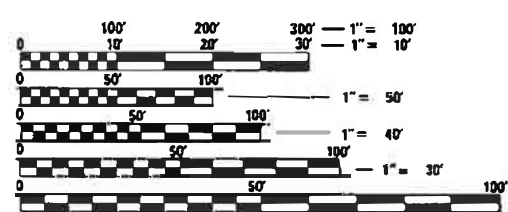
2018 ADT = 14,000
POSTED SPEED = 35-40 MPH

PROJECT BEGINS
STATION 10+75



PROJECT ENDS
STATION 110+21

OMISSION
STA. 48+49 TO STA. 65+17



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

LOCATION MAP
NOT TO SCALE

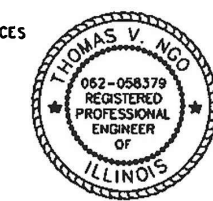
GROSS LENGTH OF PROJECT = 9,945 FT. = 1.88 MILES
NET LENGTH OF PROJECT = 8,277 FT. = 1.57 MILES

PROJECT ENGINEER: VESLIN VELICHKOW, P.E. (847) 705-4432
PROJECT MANAGER: FAWAD AQUEEL, P.E. (847) 705-4247

CONTRACT NO. 62M18

MILLENNIA PROFESSIONAL SERVICES
THOMAS V. MCG
IL. LIC. NO. 062-058379
EXPIRES 11/30/2021

SIGNED: *Thomas V. McG*
DATE: 12/01/2020
SIGNATURE AND SEAL APPLIES
TO DRWG NO 1 TO



PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

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LIST OF HIGHWAY STANDARDS

000001-08 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
 280001-07 TEMPORARY EROSION CONTROL SYSTEMS
 442201-03 CLASS C AND D PATCHES
 606001-07 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
 701011-04 OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
 701301-04 LANE CLOSURE, 2L, 2W SHORT TIME OPERATIONS
 701311-03 LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
 701427-04 LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS < OR = 40 MPH
 701501-06 URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
 701606-10 URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
 701701-10 URBAN LANE CLOSURE, MULTILANE INTERSECTION
 701801-06 SIDEWALK, CORNER OR CROSSWALK CLOSURE
 701901-08 TRAFFIC CONTROL DEVICES

COMMITMENTS

NO COMMITMENTS FOR THIS PROJECT

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- THE CONTRACTOR SHALL TAKE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS, BY THE CONTRACTOR, SHALL BE REPAIRED AT THE CONTRACTORS EXPENSE.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND VILLAGES OF DES PLAINES, GLENVIEW, NILES, AND PARK RIDGE.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCES, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
- IN ADDITION TO FIELD AND AERIAL SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO A CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS, OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS, SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- THE CONTRACTOR SHALL PROVIDE ADEQUATE DRAINAGE AND EROSION CONTROL PROTECTION DURING ALL STAGES OF CONSTRUCTION.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, AREA TRAFFIC FIELD ENGINEER, AT WALTER.CZARNY@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- POLLUTION CONTROL: THE CONTRACTOR SHALL BE REQUIRED TO COMPLY WITH THE STATE REGULATIONS REGARDING AIR, WATER, AND NOISE POLLUTION. THIS WORK IS NOT TO BE PAID SEPARATELY AND SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
- THE COST OF SAW CUTTING SHALL BE INCLUDED IN THE UNIT PRICES FOR THE VARIOUS REMOVAL PAY ITEMS.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN ON THE PLANS.
- PAVEMENT MARKING TAPE, TYPE IV SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE IV AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.
- THE CONTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT 1 TYPICAL PAVEMENT MARKINGS DETAILS (TC-13 & TC-14).
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- PROPOSED SIDEWALK RAMPS SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
- THE ENGINEER SHALL REPORT CLEARANCES UNDER BRIDGE AFTER RESURFACING

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 USER NAME = Millennium Professional Services



USER NAME = jcovarrubias	DESIGNED - TN	REVISED -
	DRAWN - JC	REVISED -
PLOT SCALE = 2.0000' / 1" =	CHECKED - TN	REVISED -
PLOT DATE = 12/11/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BALLARD RD. (RAND RD. TO GREENWOOD AVE.)
INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES**

SCALE: N/A	SHEET NO. 1 OF 1 SHEETS	STA. N/A	TO STA. N/A
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	2
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				CONTRACT NO. 62M18

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PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	CONST. TYPE CODE	
				URBAN 80% FED 20% STATE	URBAN 100% STATE
				0005	0005
20200100	EARTH EXCAVATION	CU YD	77	77	
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	844	844	
25200110	SODDING, SALT TOLERANT	SQ YD	844	844	
25200200	SUPPLEMENTAL WATERING	UNIT	4	4	
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	15890	15890	
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	53	53	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	412	412	
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	93	93	
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	2967	2967	
42001300	PROTECTIVE COAT	SQ YD	1192	1192	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	5732	5732	
42400800	DETECTABLE WARNINGS	SQ FT	532	532	
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	34899	34899	
44000600	SIDEWALK REMOVAL	SQ FT	4898	4898	
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SQ YD	1103	1103	
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	209	209	
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	523	523	

- SPECIALTY ITEM
- △ NON-PARTICIPATING (100% STATE)

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BALLARD RD. (RAND RD. TO GREENWOOD AVE.)
SCHEDULE OF QUANTITIES**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	3
CONTRACT NO. 62M18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: N/A SHEET NO. 1 OF 4 SHEETS STA. TO STA.

PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	CONST. TYPE CODE	
				URBAN 80% FED 20% STATE	URBAN 100% STATE
				0005	0005
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	314	314	
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	466	466	
60255500	MANHOLES TO BE ADJUSTED	EACH	1	1	
60266600	VALVE BOXES TO BE ADJUSTED	EACH	2	2	
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	25	25	
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	77	77	
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	11	11	
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1	
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1	
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	15	15	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12	
67100100	MOBILIZATION	L SUM	1	1	
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1	
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1	
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1	
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1	

* SPECIALTY ITEM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BALLARD RD. (RAND RD. TO GREENWOOD AVE.)
SCHEDULE OF QUANTITIES

SCALE: N/A SHEET NO. 2 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	4
CONTRACT NO. 62M18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	CONST. TYPE CODE	
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				0005	0005
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	4454	4454	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1485	1485	
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	536	536	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	24743	24743	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	3000	3000	
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1345	1345	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	752	752	
• 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	536	536	
• 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	24743	24743	
• 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3000	3000	
• 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1346	1346	
• 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	752	752	
• 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	630	630	
• 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	386	386	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	386	386	

- SPECIALTY ITEM
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BALLARD RD. (RAND RD. TO GREENWOOD AVE.)
SCHEDULE OF QUANTITIES**

SCALE: N/A SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	5
CONTRACT NO. 62M18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	CONST. TYPE CODE	
				URBAN 80% FED 20% STATE	URBAN 100% STATE
				0005	0005
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	1134	1134	
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2	2	
88600600	DETECTOR LOOP REPLACEMENT	FOOT	1649	1649	
* 89502376	REBUILD EXISTING HANDHOLE	EACH	2	2	
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1	
X4400501	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	70	70	
* X2600011	REMOVE AND RELOCATE SIGN PANEL	EACH	1	1	
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	25	25	
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	13133	13133	
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1978	1978	
△ Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	22		22
Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	2	2	
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	182	182	
Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	10673	10673	
∅ Z0076600	TRAINEES	HOURS	500	500	
∅ Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOURS	500	500	

∅ 0042

• SPECIALTY ITEM

△ NON-PARTICIPATING (100% STATE)

REV-SEP



USER NAME = jcovarrubias
 PLOT SCALE = 2.0000 / 1" =
 PLOT DATE = 12/11/2020

DESIGNED - TN
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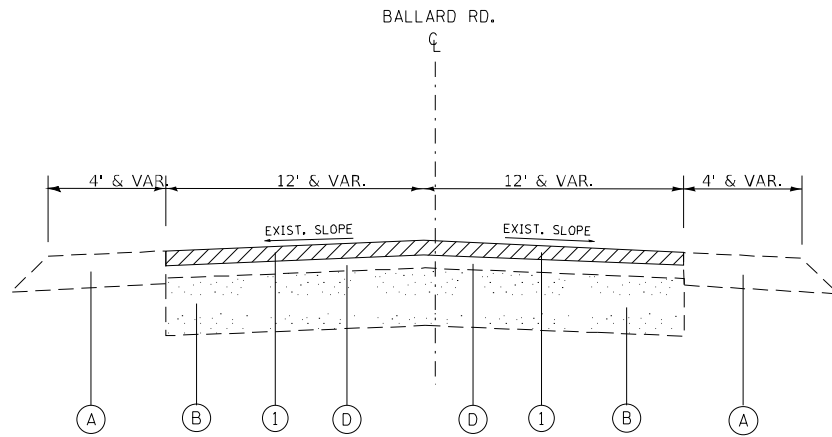
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BALLARD RD. (RAND RD. TO GREENWOOD AVE.)
 SCHEDULE OF QUANTITIES

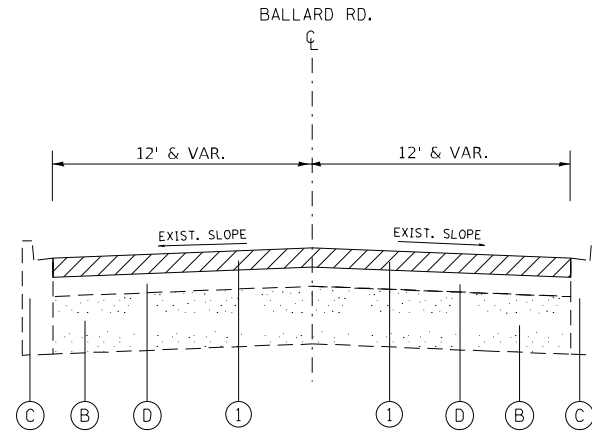
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F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	6
CONTRACT NO. 62M18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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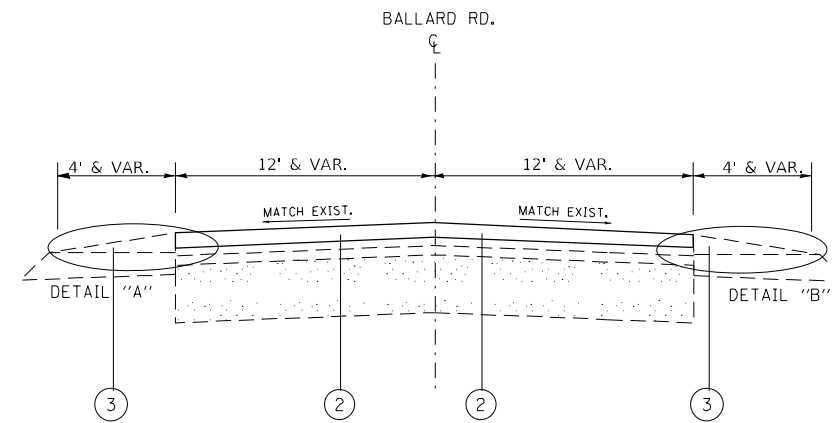
EXISTING TYPICAL SECTION
 STA. 10+75 TO STA. 66+50 **
 STA. 74+38 TO STA. 99+45



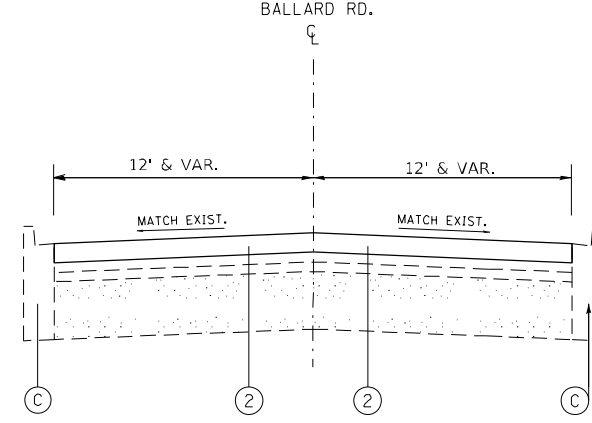
EXISTING TYPICAL SECTION
 STA. 66+50 TO STA. 74+38
 STA. 99+45 TO STA. 110+21

EXISTING LEGEND

- (A) EXISTING AGGREGATE SHOULDER, TYPE B
- (B) EXISTING PCC BASE COURSE, |9"
- (C) EXISTING COMB. CURB & GUTTER, TYPE B-6.12
- (D) EXISTING REMAINING HMA AFTER MILLING |1"



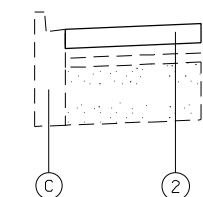
PROPOSED TYPICAL SECTION
 STA. 10+75 TO STA. 66+50 **
 STA. 74+38 TO STA. 99+45



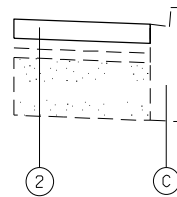
PROPOSED TYPICAL SECTION
 STA. 66+50 TO STA. 74+38
 STA. 99+45 TO STA. 110+21

PROPOSED LEGEND

- (1) PROPOSED HMA SURFACE REMOVAL 1½"
- (2) PROPOSED HMA SURFACE COURSE, IL-9.5, MIX "D", N70, 1½"
- (3) PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B



DETAIL "A"
 STA. 37+12 TO STA. 43+45



DETAIL "B"
 STA. 93+00 TO STA. 99+45

**** PROJECT OMMISION**
 STA. 48+49 TO STA. 65+17

MIXTURE NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE IS 112 LBS/SQYD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
3. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATIONS THAT APPLIES TO THE HMA MIXTURE.

LONGITUDINAL JOINT SEALANT NOTE:

THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE MILLED SURFACE

HOT MIX ASPHALT MIXTURE REQUIREMENTS			
MIXTURE USES	MIXTURE TYPE	AIR VOIDS	QMP TYPE
SMART OVERLAY	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70, 1½"	4% @ 70 GYR.	QCP
PATCHING	CLASS D PATCHES (HMA IL-19 mm)	4% @ 70 GYR.	QC/QA
PATCHING	HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)			

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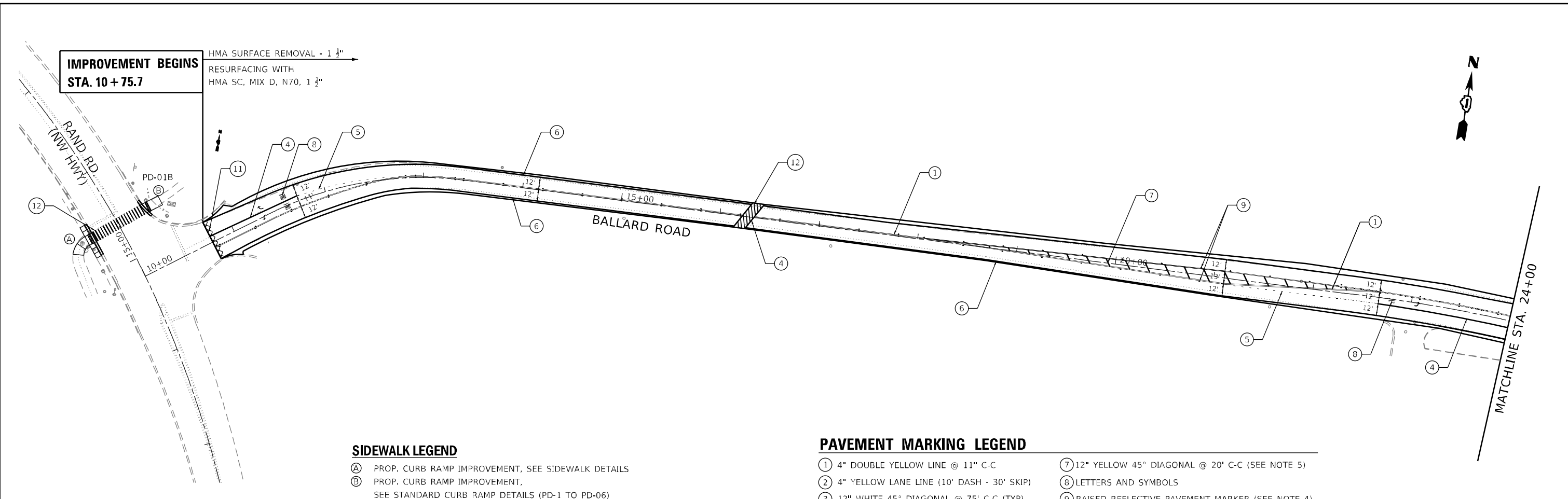
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BALLARD RD. (RAND RD. TO GREENWOOD AVE.)
TYPICAL SECTIONS

SCALE: N/A	SHEET NO. 1 OF 1 SHEETS	STA. N/A TO STA. N/A	F.A.P. RTE. 1319	SECTION 2020-140-RS&SW	COUNTY COOK	TOTAL SHEETS 49	SHEET NO. 7
						CONTRACT NO. 62M18	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

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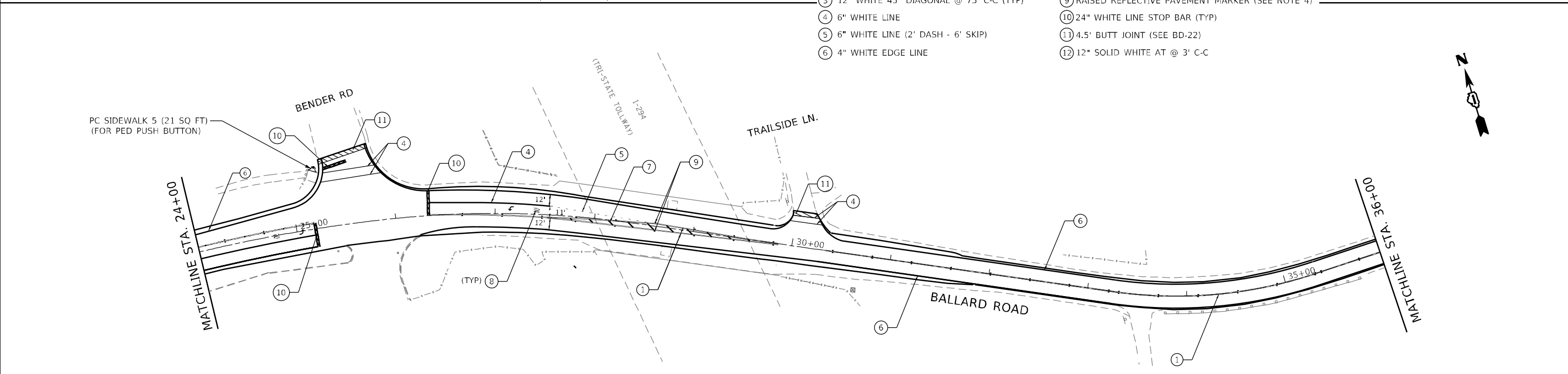


SIDEWALK LEGEND

- (A) PROP. CURB RAMP IMPROVEMENT, SEE SIDEWALK DETAILS
- (B) PROP. CURB RAMP IMPROVEMENT, SEE STANDARD CURB RAMP DETAILS (PD-1 TO PD-06)

PAVEMENT MARKING LEGEND

- ① 4" DOUBLE YELLOW LINE @ 11" C-C
- ② 4" YELLOW LANE LINE (10' DASH - 30' SKIP)
- ③ 12" WHITE 45° DIAGONAL @ 75' C-C (TYP)
- ④ 6" WHITE LINE
- ⑤ 6" WHITE LINE (2' DASH - 6' SKIP)
- ⑥ 4" WHITE EDGE LINE
- ⑦ 12" YELLOW 45° DIAGONAL @ 20' C-C (SEE NOTE 5)
- ⑧ LETTERS AND SYMBOLS
- ⑨ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 4)
- ⑩ 24" WHITE LINE STOP BAR (TYP)
- ⑪ 4.5' BUTT JOINT (SEE BD-22)
- ⑫ 12" SOLID WHITE AT @ 3' C-C



PLAN NOTES:

1. ALL DRAINAGE STRUCTURES WITHIN PROJECT LIMITS SHALL BE CLEANED.
2. THERMOPLASTIC PAVEMENT MARKINGS ARE FOR HMA PAVEMENT.
3. ALL PAVEMENT MARKINGS INSTALLED ACCORDING TO IDOT'S D1 DETAIL (TC-13).
4. ALL SED REFLECTIVE PAVEMENT MARKERS INSTALLED ACCORDING TO IDOT'S D1 DETAIL (TC-11).
5. FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

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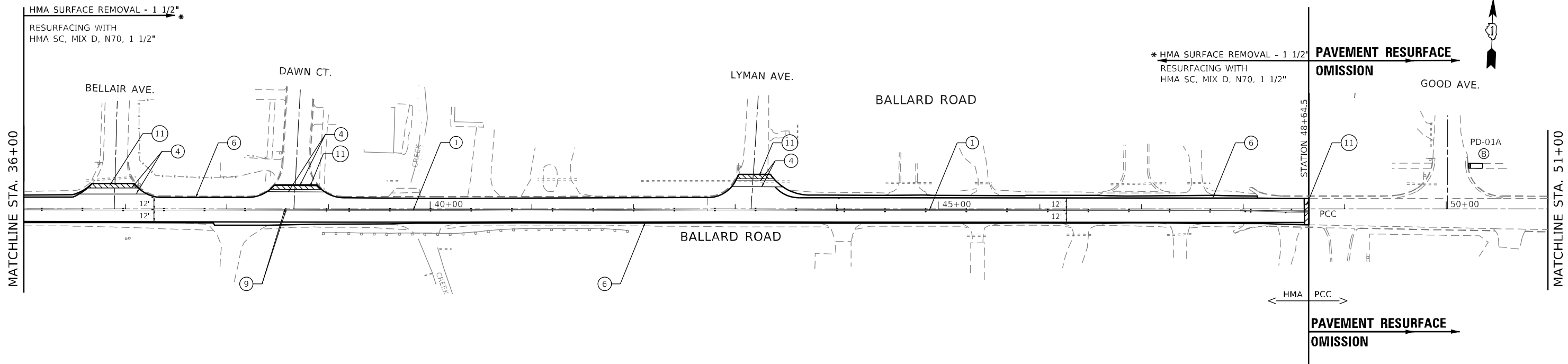
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BALLARD RD. (RAND RD. TO GREENWOOD AVE.)
ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET NO. 1 OF 4 SHEETS STA. -XX+XXX TO STA. -36+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	8
CONTRACT NO. 62M18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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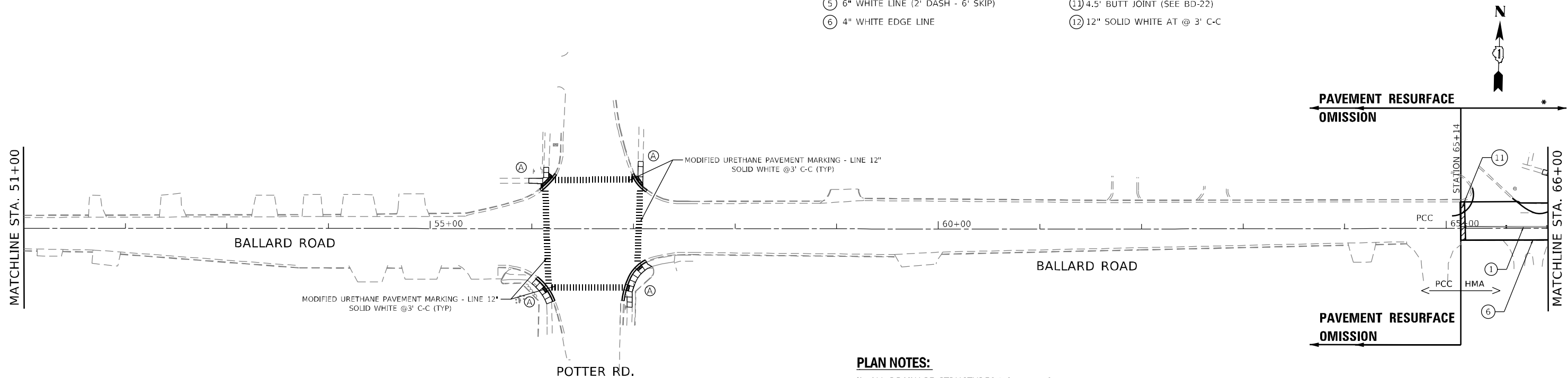


SIDEWALK LEGEND

- (A) PROP. CURB RAMP IMPROVEMENT, SEE SIDEWALK DETAILS
- (B) PROP. CURB RAMP IMPROVEMENT, SEE STANDARD CURB RAMP DETAILS (PD-1 TO PD-06)

PAVEMENT MARKING LEGEND

- ① 4" DOUBLE YELLOW LINE @ 11" C-C
- ② 4" YELLOW LANE LINE (10' DASH - 30' SKIP)
- ③ 12" WHITE 45° DIAGONAL @ 75' C-C (TYP)
- ④ 6" WHITE LINE
- ⑤ 6" WHITE LINE (2' DASH - 6' SKIP)
- ⑥ 4" WHITE EDGE LINE
- ⑦ 12" YELLOW 45° DIAGONAL @ 20' C-C (SEE NOTE 5)
- ⑧ LETTERS AND SYMBOLS
- ⑨ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 4)
- ⑩ 24" WHITE LINE STOP BAR (TYP)
- ⑪ 4.5" BUTT JOINT (SEE BD-22)
- ⑫ 12" SOLID WHITE AT @ 3' C-C



PLAN NOTES:

1. ALL DRAINAGE STRUCTURES WITHIN PROJECT LIMITS SHALL BE CLEANED.
2. THERMOPLASTIC PAVEMENT MARKINGS ARE FOR HMA PAVEMENT.
3. ALL PAVEMENT MARKINGS INSTALLED ACCORDING TO IDOT'S D1 DETAIL (TC-13).
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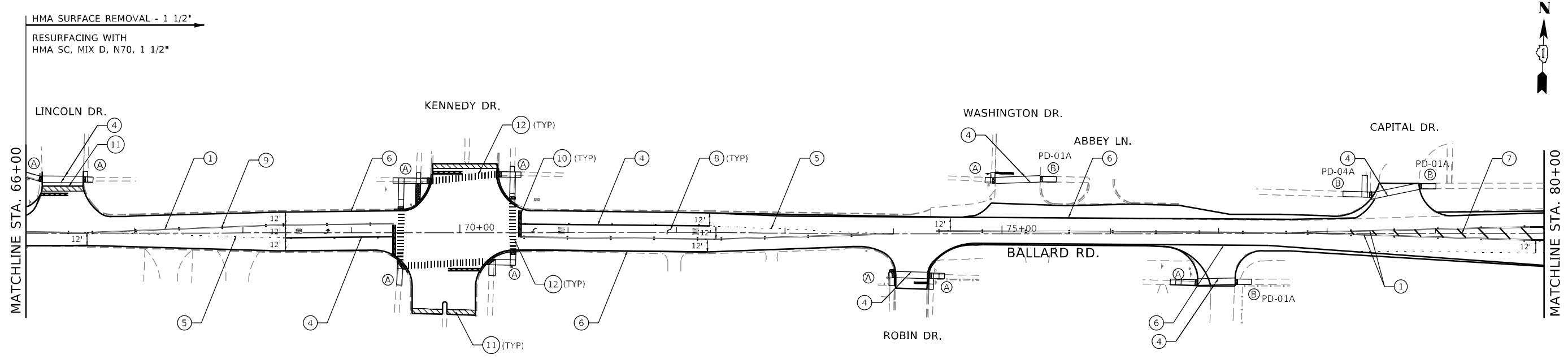
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BALLARD RD. (RAND RD. TO GREENWOOD AVE.)
ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET NO. 2 OF 4 SHEETS STA. 36+00 TO STA. 66+00

F.A.U. RTE. 1319	SECTION 2020-140-RS&SW	COUNTY COOK	TOTAL SHEETS 50	SHEET NO. 9
CONTRACT NO. 62M18				
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

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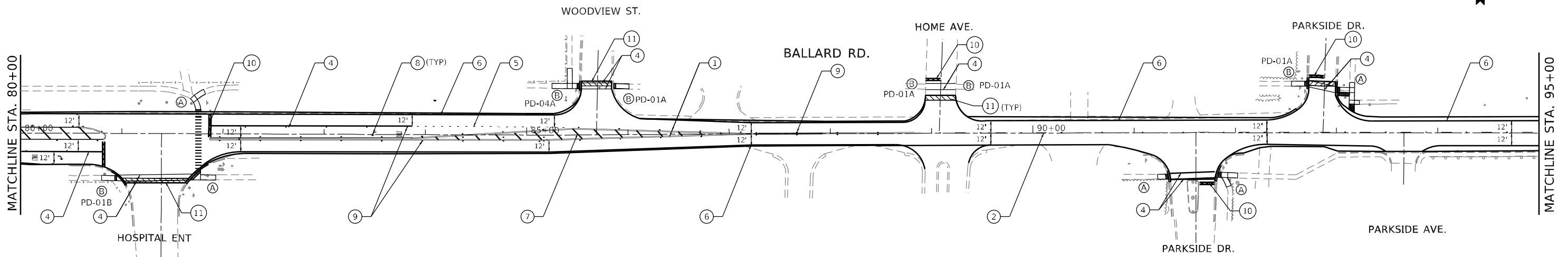


SIDEWALK LEGEND

- (A) PROP. CURB RAMP IMPROVEMENT, SEE SIDEWALK DETAILS
- (B) PROP. CURB RAMP IMPROVEMENT, SEE STANDARD CURB RAMP DETAILS (PD-1 TO PD-06)

PAVEMENT MARKING LEGEND

- (1) 4" DOUBLE YELLOW LINE @ 11" C-C
- (2) 4" YELLOW LANE LINE (10' DASH - 30' SKIP)
- (3) 12" WHITE 45° DIAGONAL @ 75' C-C (TYP)
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- (6) 4" WHITE EDGE LINE
- (7) 12" YELLOW 45° DIAGONAL @ 20' C-C (SEE NOTE 5)
- (8) LETTERS AND SYMBOLS
- (9) RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 4)
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PLAN NOTES:

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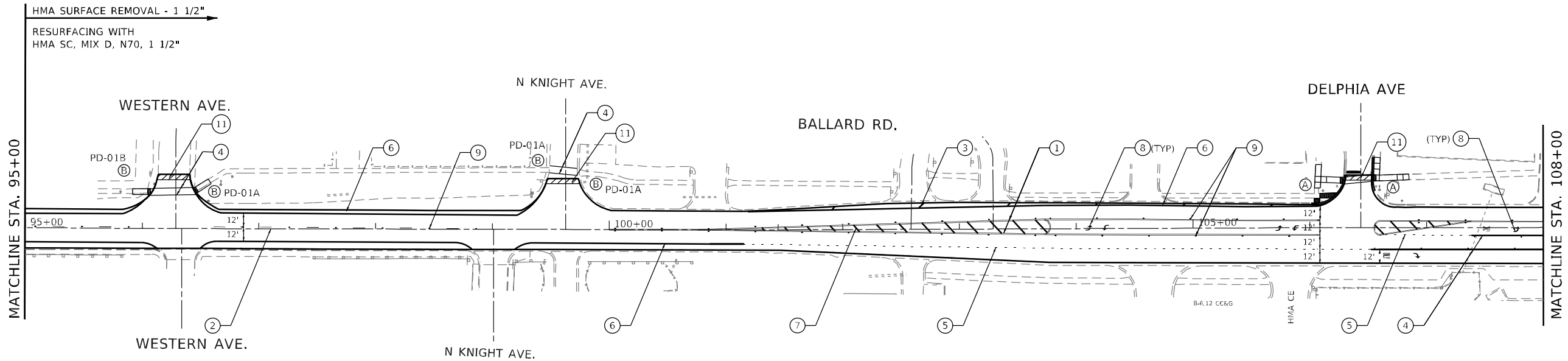
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BALLARD RD. (RAND RD. TO GREENWOOD AVE.)
ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET NO. 3 OF 4 SHEETS STA. 66+00 TO STA. 95+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	10
CONTRACT NO. 62M18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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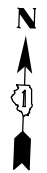
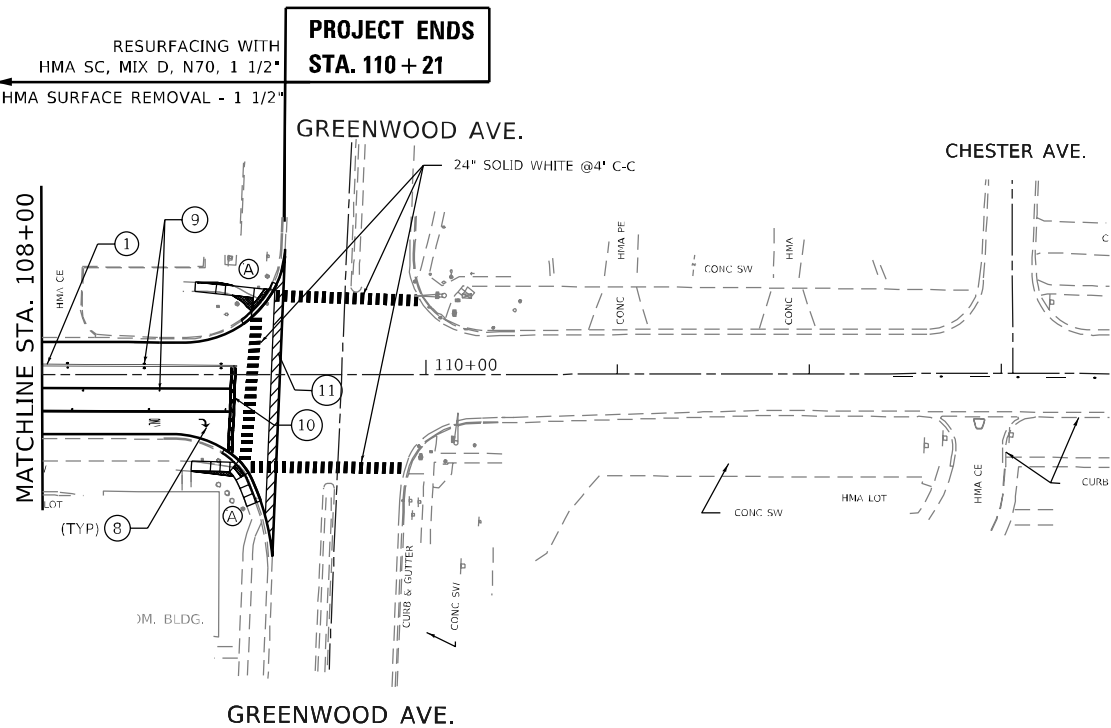


SIDEWALK LEGEND

- (A) PROP. CURB RAMP IMPROVEMENT, SEE SIDEWALK DETAILS
- (B) PROP. CURB RAMP IMPROVEMENT, SEE STANDARD CURB RAMP DETAILS (PD-1 TO PD-06)

PAVEMENT MARKING LEGEND

- (1) 4" DOUBLE YELLOW LINE @ 11" C-C
- (2) 4" YELLOW LANE LINE (10' DASH - 30' SKIP)
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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BALLARD RD. (RAND RD. TO GREENWOOD AVE.)
MAINTENANCE OF TRAFFIC**

SCALE: 1"=50' SHEET NO. 4 OF 4 SHEETS STA. 95+00 TO STA. 114+60

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	11
CONTRACT NO. 62M18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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SCHEDULE OF QUANTITIES

LOCATION				20200100	21101615	25200110	25200200	42001300	42400200	42400800	44000600	60300305	85000200	89502376	X0327998	X2600011	X4400221	Z0004562
SHEET NO.	BALLARD RD AT	CORNER	ADA DETAIL	EARTH EXCAVATION	TOPSOIL F & P 4	SODDING SALT TOLERANT	SUPPLE WATERING	PROTECTIVE COAT	PC CONC SIDEWALK 5	DETECTABLE WARNINGS	SIDEWALK REM	FR & LIDS ADJUST	MAIN EX TR SIG INSTAL	REBUILD EX HANDHOLE	BOLLARD TO BE RELOCAT	REM & REL SIGN PANEL	COMB CC&G REM & REP <= 10	COMB CC&G REM & REP
				CU YD	SQ YD	SQ YD	UNIT	SQ YD	SQ FT	SQ FT	SQ FT	EACH	EACH	EACH	EACH	EACH	FOOT	FOOT
	BENDER RD	NW		1	3	3	0.02	3	21									
ADA-01	RAND RD	NW		4	10	10	0.08	39	347	20	355	1	1	1	1			35
ADA-01	RAND RD	NE	PD-01B	2	4	4	0.03	12	105	20	105							20
	GOOD AVE	NE	PD-01A	1	7	7	0.06	10	90	10	90						10	
ADA-02	POTTER RD	NW		2	8	8	0.06	18	157	20	176							22
ADA-02	POTTER RD	NE		2	8	8	0.06	13	113	20	113							22
ADA-03	POTTER RD	SW		2	4	4	0.03	18	161	11	161		1	1				29
ADA-03	POTTER RD	SE		3	8	8	0.06	33	290	23	290							46
ADA-04	LINCOLN DR	NW		2	7	7	0.06	12	104	10	78							
ADA-04	LINCOLN DR	NE		1	3	3	0.02	8	65	10	53							
ADA-05	KENNEDY DR	NW		4	26	26	0.21	35	309	20	214							54
ADA-05	KENNEDY DR	NE		3	14	14	0.11	26	234	20	192							41
ADA-06	DEE RD	SW		2	8	8	0.06	12	108	19	88							23
ADA-06	DEE RD	SE		3	18	18	0.14	25	220	25	64							45
ADA-07	ROBIN DR	SW		1	4	4	0.03	7	62	10	59							15
ADA-07	ROBIN DR	SE		2	7	7	0.06	13	110	10	96							27
ADA-08	WASHINGTON DR	NW		1	3	3	0.02	8	65	10	51					1		
	WASHINGTON DR	NE	PD-01A	1	7	7	0.06	10	90	10	90							
ADA-09	UNNAMED ROAD	SW		2	9	9	0.07	16	138	10	96							
ADA-09	UNNAMED ROAD	SE	PD-01A	1	7	7	0.06	10	90	10	90							
	CAPITAL DR	NW	PD-04A	3	17	17	0.14	25	217	10	217							
	CAPITAL DR	NE	PD-01A	1	7	7	0.06	10	90	10	90							
ADA-10	HOSPITAL ENTRANCE	SW	PD-01B	2	8	8	0.06	12	105	10	73							20
ADA-10	HOSPITAL ENTRANCE	SE		2	10	10	0.08	15	132	26	132							29
ADA-10	HOSPITAL ENTRANCE	NE		2	8	8	0.06	15	132	10	132							
ADA-11	WOODVIEW LN	NW	PD-04A	3	17	17	0.14	25	217	10	54						10	
ADA-11	WOODVIEW LN	NE	PD-01A	1	7	7	0.06	10	90	10	90							
	HOME AVE	NW	PD-01A	1	7	7	0.06	10	90	10	90							
	HOME AVE	NE	PD-01A	1	7	7	0.06	10	90	10	90							
ADA-12	PARKSIDE DR	SW		1	4	4	0.03	6	52	10	52						10	
ADA-12	PARKSIDE DR	SE		2	9	9	0.07	17	147	10	147							16
ADA-13	PARKSIDE AVE	NW	PD-01A	1	7	7	0.06	10	90	10	61							11
ADA-13	PARKSIDE AVE	NE		2	17	17	0.14	21	187	10	223							14
ADA-14	WESTERN AVE	NW	PD-01B	2	8	8	0.06	12	105	10	52							
ADA-14	WESTERN AVE	NE	PD-01A	1	7	7	0.06	10	90	10	47							
	N. KNIGHT AVE	NW	PD-01A	1	7	7	0.06	10	90	10	90							
	N. KNIGHT AVE	NE	PD-01A	1	7	7	0.06	10	90	10	90							
ADA-15	DELPHIA ST	NW		2	23	23	0.18	22	197	10	170							42
ADA-15	DELPHIA ST	NE		3	13	13	0.10	25	225	10	225	1						29
ADA-16	GREENWOOD RD	NW		2	16	16	0.13	23	200	24	178							26
ADA-16	GREENWOOD RD	SW		3	13	13	0.10	25	217	14	209	1						33
TOTAL				77	384	384	4	651	5732	532	4973	3	2	2	1	1	30	599

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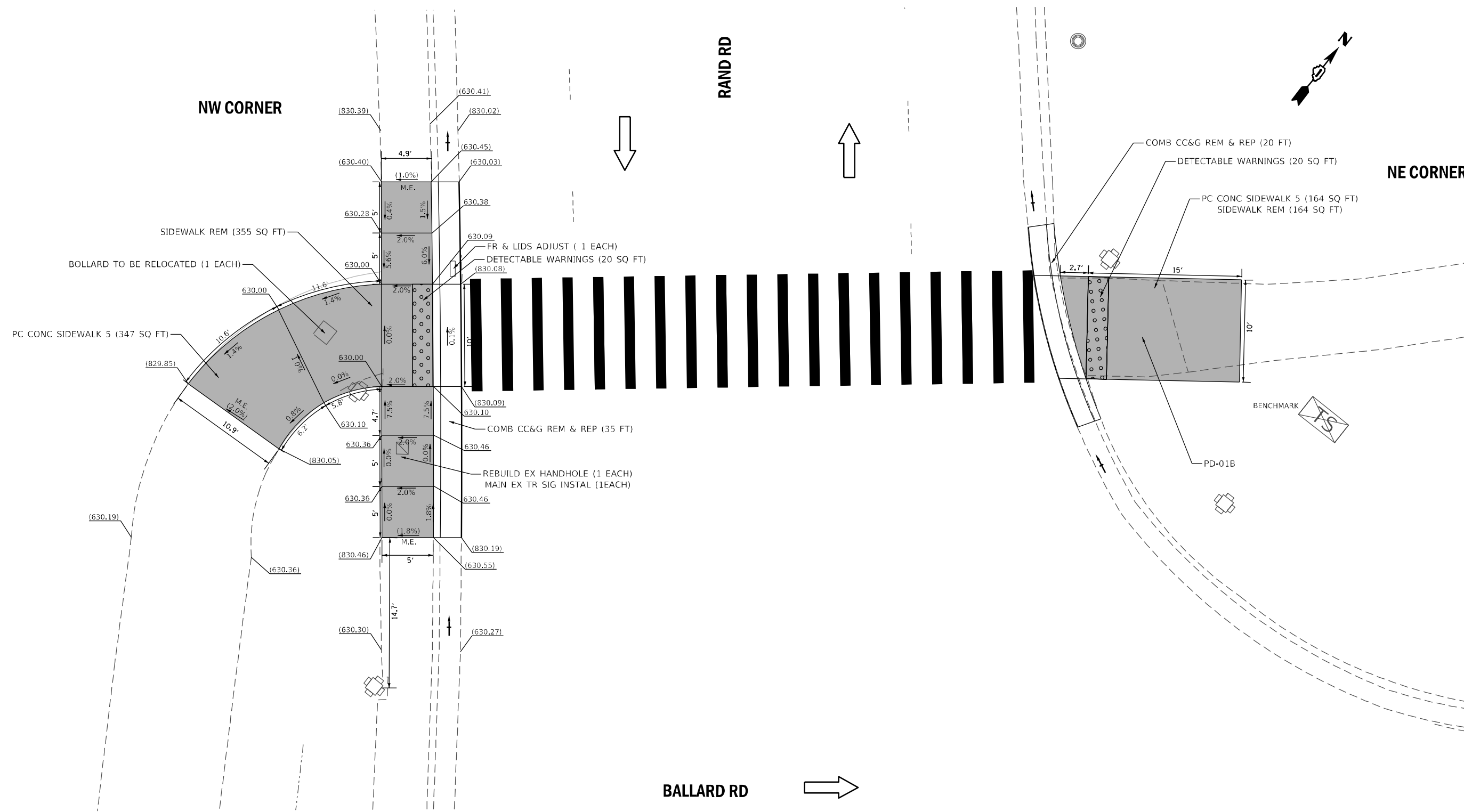


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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BALLARD RD. (RAND RD. TO GREENWOOD AVE.)
SIDEWALK RAMP SCHEDULE OF QUANTITIES**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	12
CONTRACT NO. 62M18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



ADA-01

REFERENCE BENCHMARK ELEV:
 BENCHMARK :
 LOCATION :

LEGEND	
xx.xx'	EXISTING LENGTH
—	PROPOSED SIDE CURB
()	EXISTING ELEVATION/SLOPE
[Solid Grey]	PROPOSED SIDEWALK
[Dotted]	DETECTABLE WARNINGS
[Cross-hatched]	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV: 633.606
 BENCHMARK : SQUARE ON SW CORNER OF TRAFFIC SIGNAL CONTROLLER BASE
 LOCATION : AT NE CORNER OF BALLARD RD AND RAND RD

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

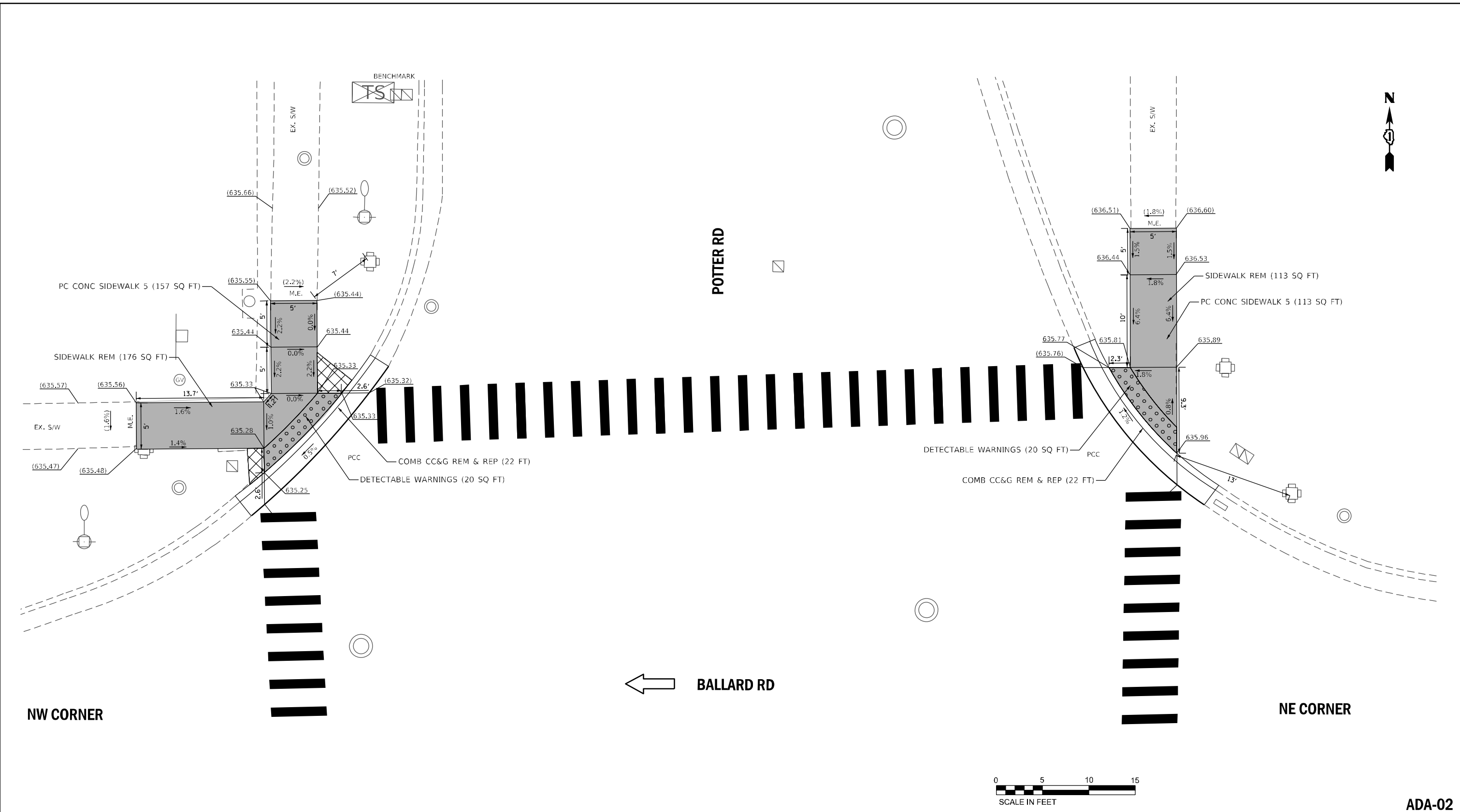
SIDEWALK DETAIL PLAN
 BALLARD RD AT RAND RD

SCALE:	SHEET NO. OF SHEETS	STA. TO STA.
--------	---------------------	--------------

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-R5&SW	COOK	50	13
CONTRACT NO. 62M18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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 USER NAME = Millennia Professional Services



NW CORNER

← BALLARD RD

POTTER RD

NE CORNER



ADA-02

REFERENCE BENCHMARK ELEV: 636.111
 BENCHMARK : SQUARE ON SE CORNER OF TRAFFIC SIGNAL CONTROLLER BASE
 LOCATION : AT NW CORNER OF BALLARD RD AND POTTER RD

LEGEND	
xx.xx'	EXISTING LENGTH
=====	PROPOSED SIDE CURB
()	EXISTING ELEVATION/SLOPE
[Solid Grey Box]	PROPOSED SIDEWALK
[Dotted Box]	DETECTABLE WARNINGS
[Cross-hatched Box]	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV:
 BENCHMARK :
 LOCATION :



USER NAME = jcovarrubias	DESIGNED - TN	REVISED -
PLOT SCALE = 10.0000' / 1" / in.	DRAWN - JP	REVISED -
PLOT DATE = 12/11/2020	CHECKED - TN	REVISED -
	DATE - 12/11/2020	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN			
BALLARD RD AT POTTER RD			
SCALE:	SHEET NO. OF SHEETS	STA.	TO STA.

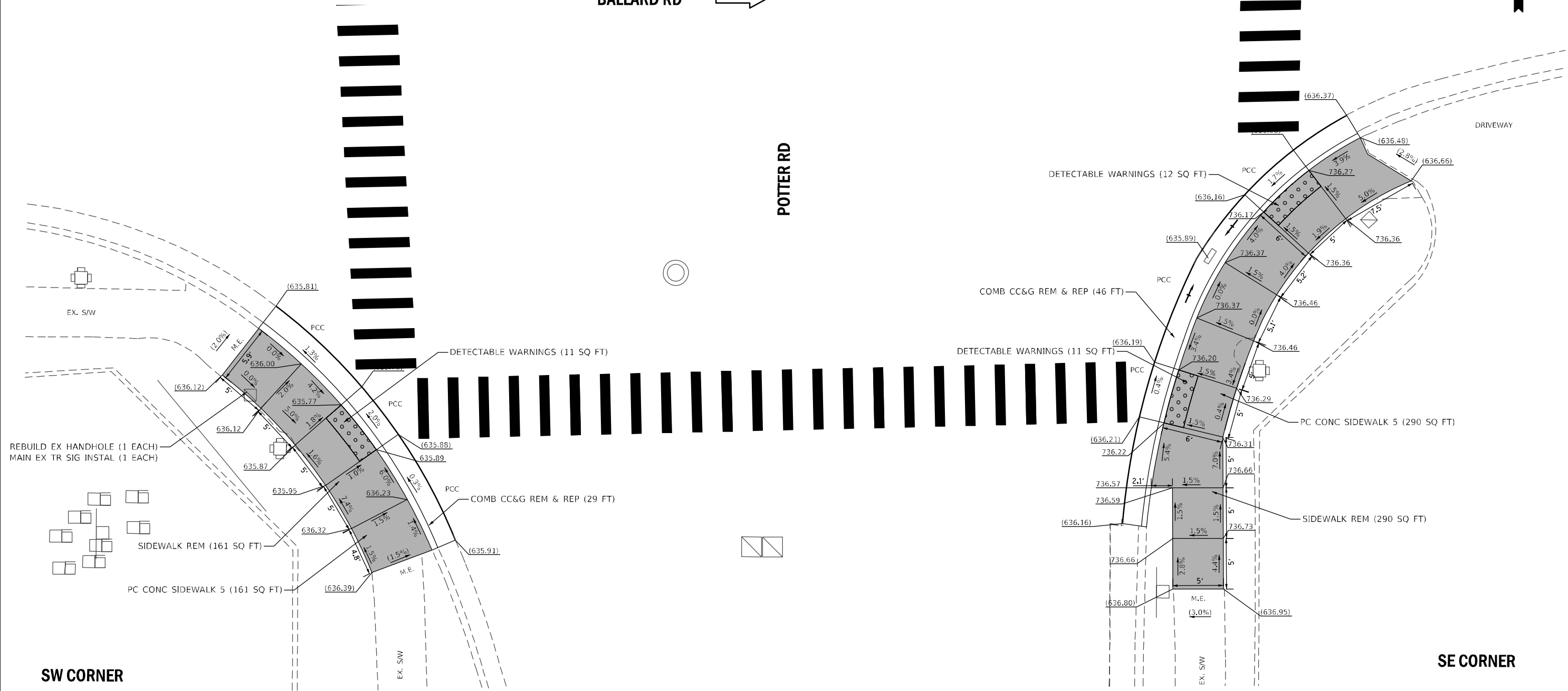
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-R5&5W	COOK	50	14
CONTRACT NO. 62M18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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BALLARD RD →

POTTER RD



EX. S/W

REBUILD EX HANDHOLE (1 EACH)
MAIN EX TR SIG INSTAL (1 EACH)

SIDEWALK REM (161 SQ FT)

PC CONC SIDEWALK 5 (161 SQ FT)

SW CORNER

EX. S/W

DETECTABLE WARNINGS (11 SQ FT)

COMB CC&G REM & REP (29 FT)

COMB CC&G REM & REP (46 FT)

DETECTABLE WARNINGS (11 SQ FT)

PC CONC SIDEWALK 5 (290 SQ FT)

SIDEWALK REM (290 SQ FT)

SE CORNER



ADA-03

REFERENCE BENCHMARK ELEV: 636.111

BENCHMARK : SQUARE ON SE CORNER OF TRAFFIC SIGNAL CONTROLLER BASE

LOCATION : AT NW CORNER OF BALLARD RD AND POTTER RD

LEGEND

xx.xx'

EXISTING LENGTH

=====

PROPOSED SIDE CURB

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV:

BENCHMARK :

LOCATION :



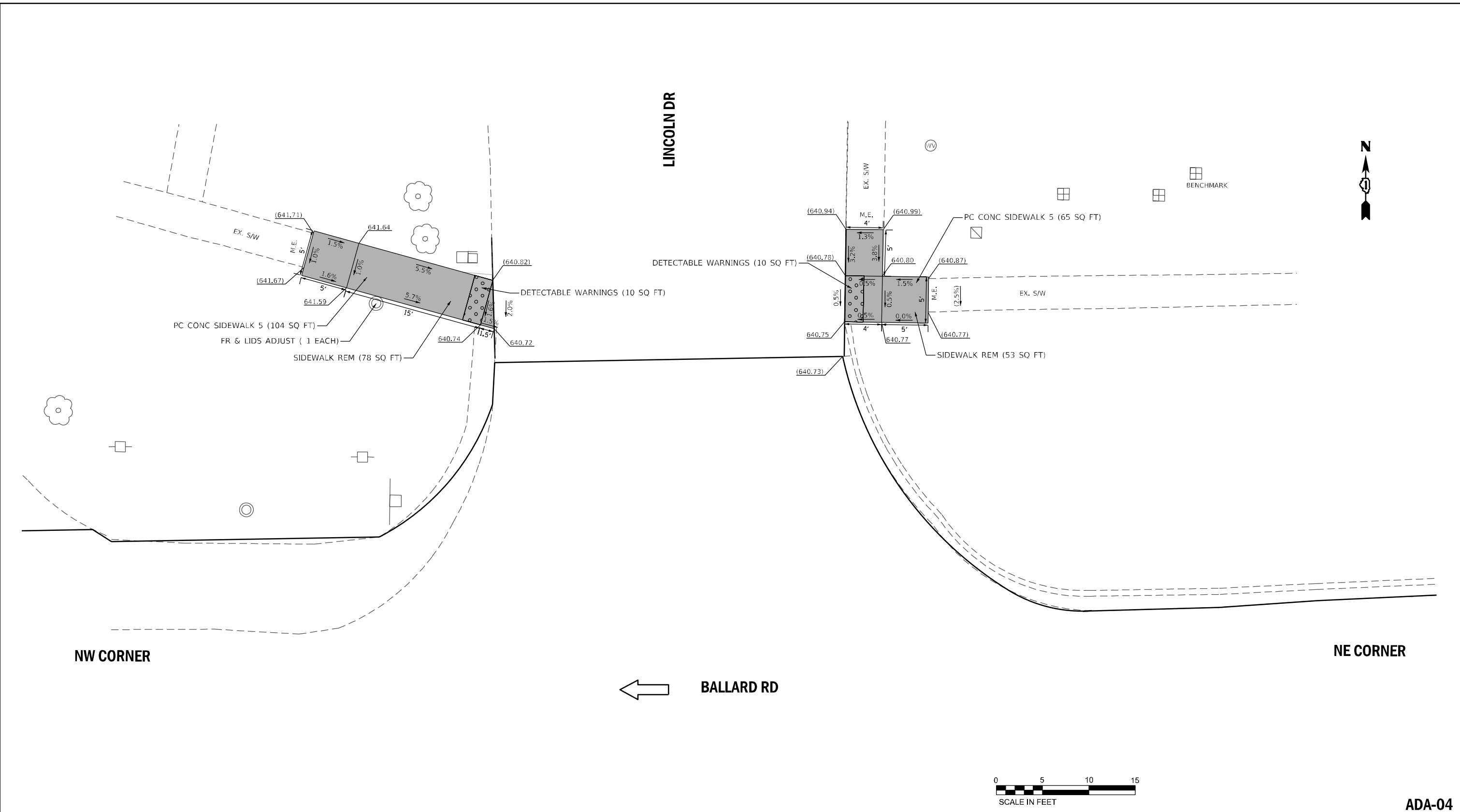
USER NAME = jcovarrubias	DESIGNED - TN	REVISED -
	DRAWN - JP	REVISED -
PLOT SCALE = 10,000' / 1"	CHECKED - TN	REVISED -
PLOT DATE = 12/11/2020	DATE - 12/11/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN BALLARD RD AT POTTER RD

SCALE:	SHEET NO. OF SHEETS	STA. TO STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	15
CONTRACT NO. 62M18				



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 PLOT SCALE = 10.0000' / 1" / in.
 USER NAME = Millennium Professional Services

NW CORNER

NE CORNER

← BALLARD RD

LINCOLN DR



ADA-04

REFERENCE BENCHMARK ELEV:
 BENCHMARK :
 LOCATION :

LEGEND

xx.xx'	EXISTING LENGTH	[Solid Grey Box]	PROPOSED SIDEWALK
[Double Line]	PROPOSED SIDE CURB	[Dotted Box]	DETECTABLE WARNINGS
()	EXISTING ELEVATION/SLOPE	[Cross-hatched Box]	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV: 640.956
 BENCHMARK : SQUARE AT SE CORNER OF AT&T ELECTRICAL BOX (NE BOX)
 LOCATION : AT NE CORNER OF BALLARD RD AND LINCOLN DR



USER NAME = jcovarrubias	DESIGNED - TN	REVISED -
	DRAWN - JP	REVISED -
PLOT SCALE = 10.0000' / 1" / in.	CHECKED - TN	REVISED -
PLOT DATE = 12/11/2020	DATE - 12/11/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

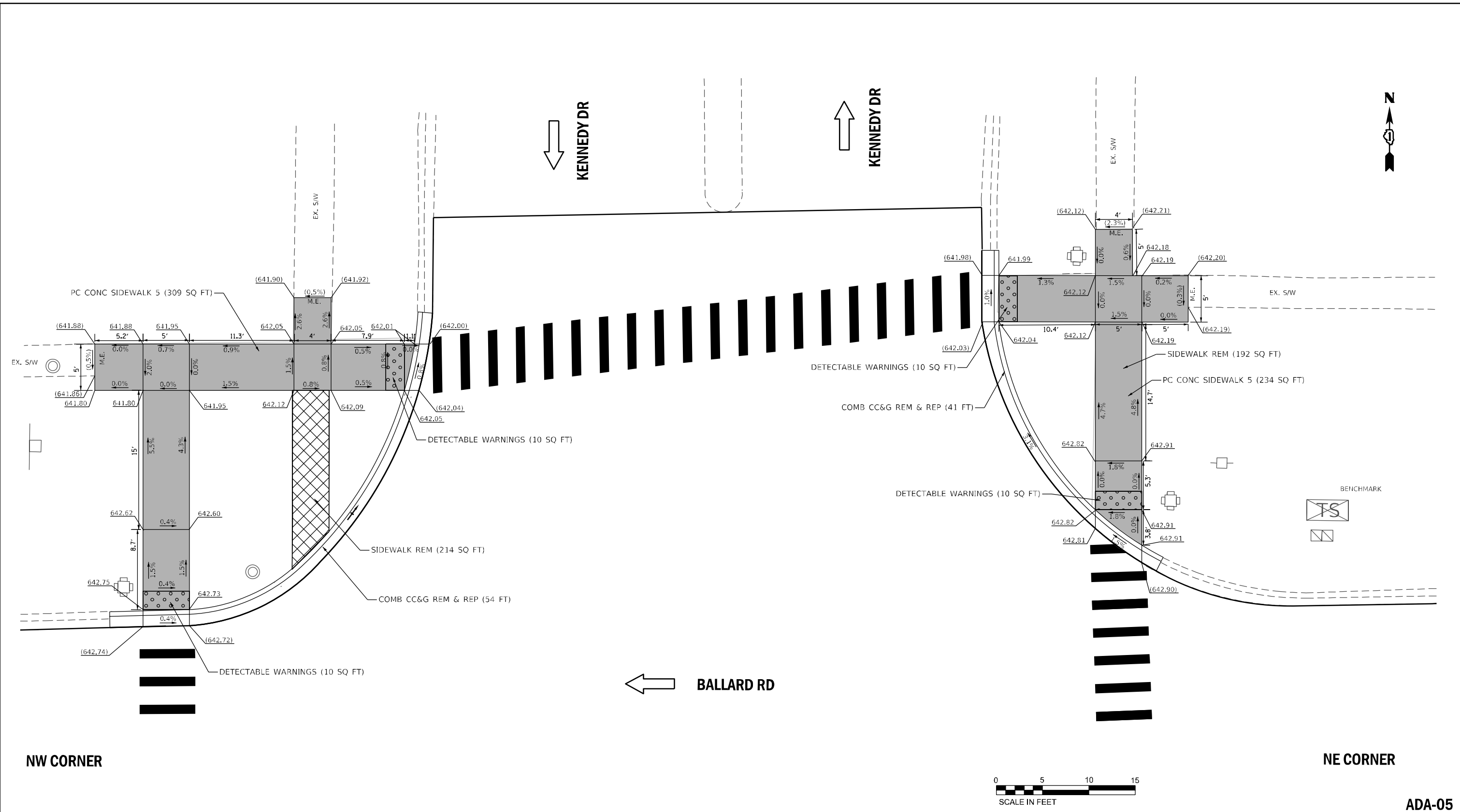
**SIDEWALK DETAIL PLAN
 BALLARD RD AT LINCOLN DR**

SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-R5&SW	COOK	50	16
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62M18	

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 PLOT SCALE = 10.0000' / 1" / in.
 USER NAME = Millennium Professional Services



NW CORNER

NE CORNER

ADA-05

REFERENCE BENCHMARK ELEV:
 BENCHMARK :
 LOCATION :

LEGEND

xx.xx'	EXISTING LENGTH	[Solid Grey Box]	PROPOSED SIDEWALK
[Double Line]	PROPOSED SIDE CURB	[Dotted Box]	DETECTABLE WARNINGS
()	EXISTING ELEVATION/SLOPE	[Cross-hatched Box]	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV: 644.133
 BENCHMARK : SQUARE AT NE CORNER OF TRAFFIC CONTROLLER BASE
 LOCATION : AT NE CORNER OF BALLARD RD AND KENNEDY DR



USER NAME = jcovarrubias	DESIGNED - TN	REVISED -
PLOT SCALE = 10.0000' / 1" / in.	DRAWN - JP	REVISED -
PLOT DATE = 12/11/2020	CHECKED - TN	REVISED -
	DATE - 12/11/2020	REVISED -

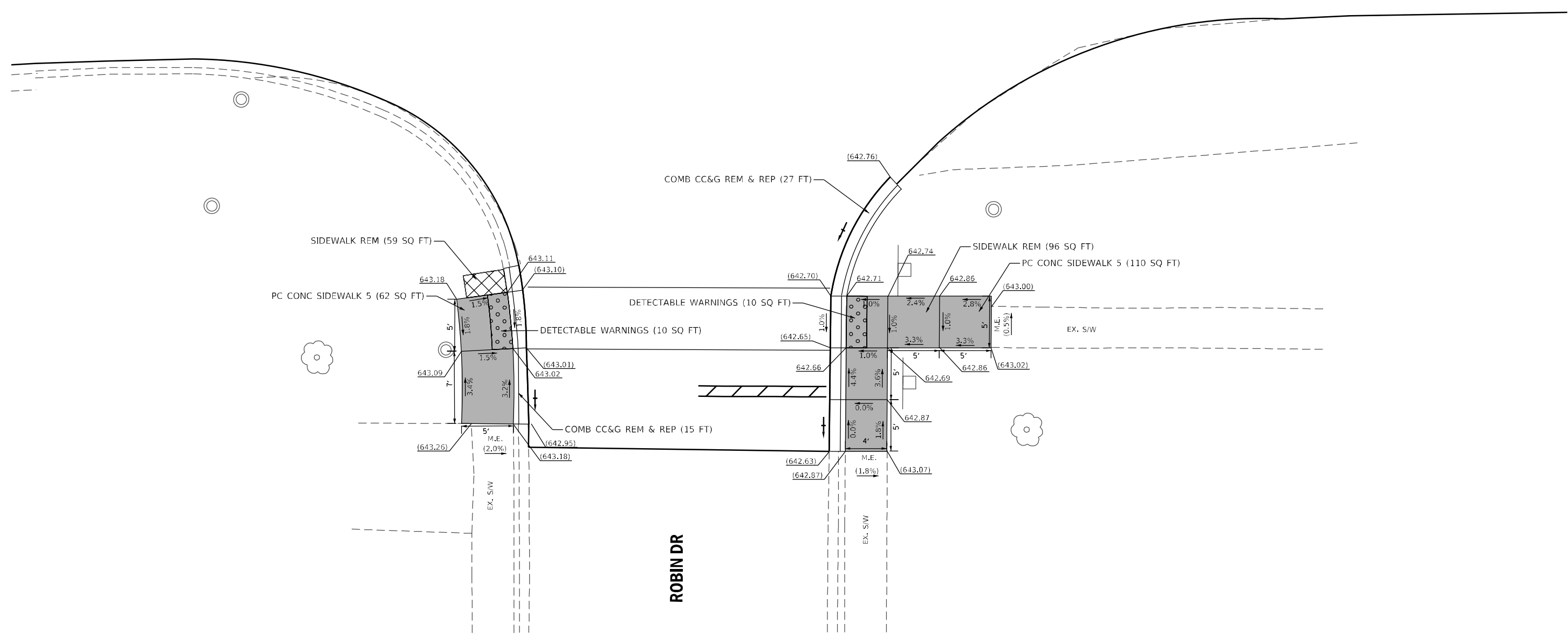
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
 BALLARD RD AT KENNEDY DR

SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	F.A.U. RTE. 1319	SECTION 2020-140-RS&SW	COUNTY COOK	TOTAL SHEETS 50	SHEET NO. 17
			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
			CONTRACT NO. 62M18				

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BALLARD RD →



SW CORNER

SE CORNER



ADA-07

REFERENCE BENCHMARK ELEV: 643.047
 BENCHMARK : SPIKE IN SOUTH FACE OF POWER POLE
 LOCATION : AT NORTH SIDE OF BALLARD RD AND ROBIN DR (SEE ADA-08)

LEGEND

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV:
 BENCHMARK :
 LOCATION :

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
 BALLARD RD AT ROBIN DR

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-R5&SW	COOK	50	19
CONTRACT NO. 62M18				



USER NAME = jcovarrubias	DESIGNED - TN	REVISED -
PLOT SCALE = 10.0000' / 1"	DRAWN - JP	REVISED -
PLOT DATE = 12/11/2020	CHECKED - TN	REVISED -
	DATE - 12/11/2020	REVISED -

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 PLOT SCALE = 10.0000' / 1"
 USER NAME = Millennia Professional Services

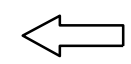
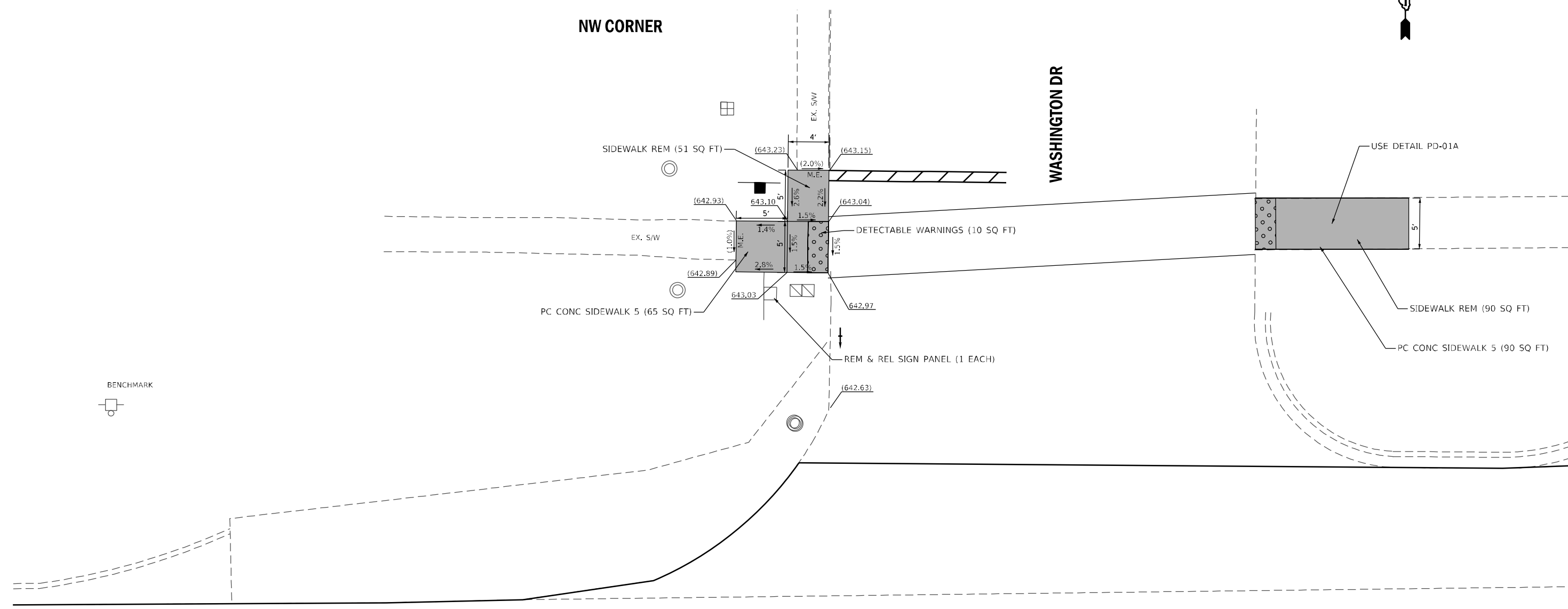
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NW CORNER

WASHINGTON DR

BALLARD RD



ADA-08

REFERENCE BENCHMARK ELEV: 643.047

BENCHMARK : SPIKE IN SOUTH FACE OF POWER POLE

LOCATION : AT NORTH SIDE OF BALLARD RD AND ROBIN DR

LEGEND

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV:

BENCHMARK :

LOCATION :



USER NAME = jcovarrubias	DESIGNED - TN	REVISED -
PLOT SCALE = 10.0000' / 1"	DRAWN - JP	REVISED -
PLOT DATE = 12/11/2020	CHECKED - TN	REVISED -
	DATE - 12/11/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
BALLARD RD AT WASHINGTON DR

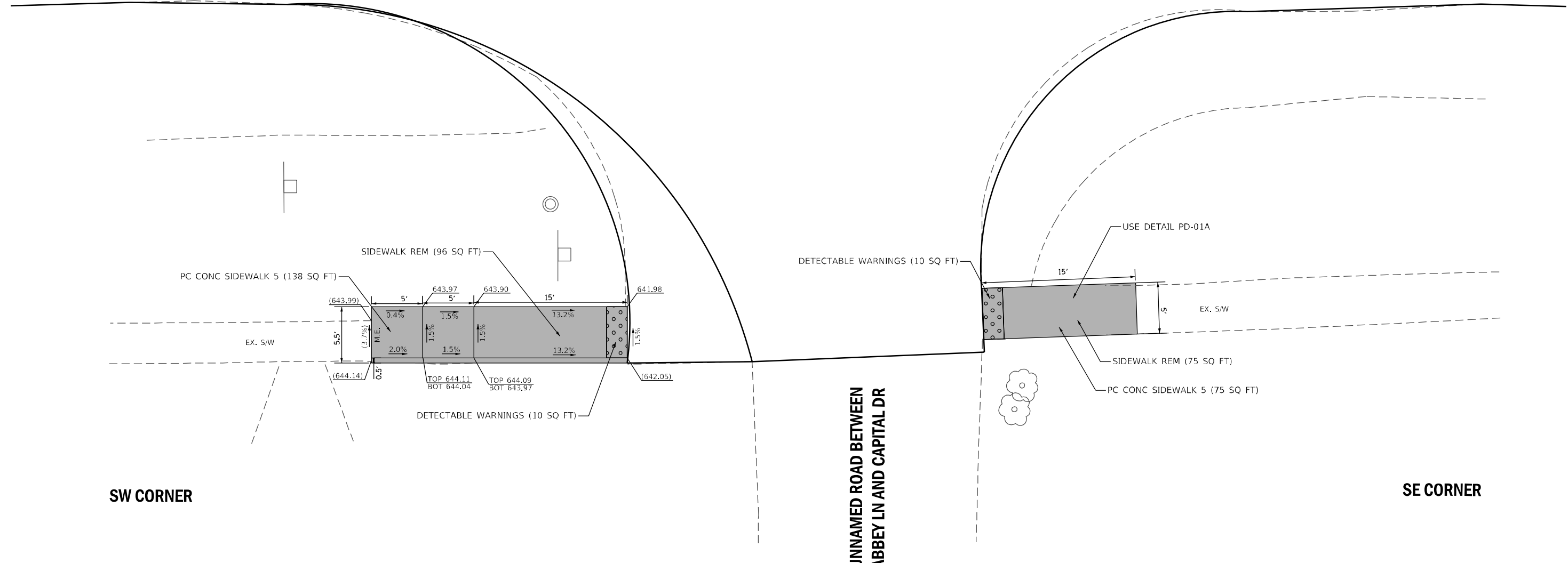
SCALE:	SHEET NO. OF SHEETS	STA. TO STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-R5&SW	COOK	50	20
CONTRACT NO. 62M18				

FILE NAME = F:\2019\ME19003.Ver-Var-PHI\W028-Ballard Road\01-CADD\02-Shts\20-D162M18.sht.ADA-08.dgn
PLOT SCALE = 10.0000' / 1"
USER NAME = Millennia Professional Services



BALLARD RD →



SW CORNER

SE CORNER

UNNAMED ROAD BETWEEN
ABBAY LN AND CAPITAL DR



ADA-09

REFERENCE BENCHMARK ELEV:
BENCHMARK :
LOCATION :

LEGEND	
xx.xx'	EXISTING LENGTH
=====	PROPOSED SIDE CURB
()	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV:
BENCHMARK :
LOCATION :



USER NAME = jcovarrubias	DESIGNED - TN	REVISED -
PLOT SCALE = 10.0000' / 1"	DRAWN - JP	REVISED -
PLOT DATE = 12/11/2020	CHECKED - TN	REVISED -
	DATE - 12/11/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

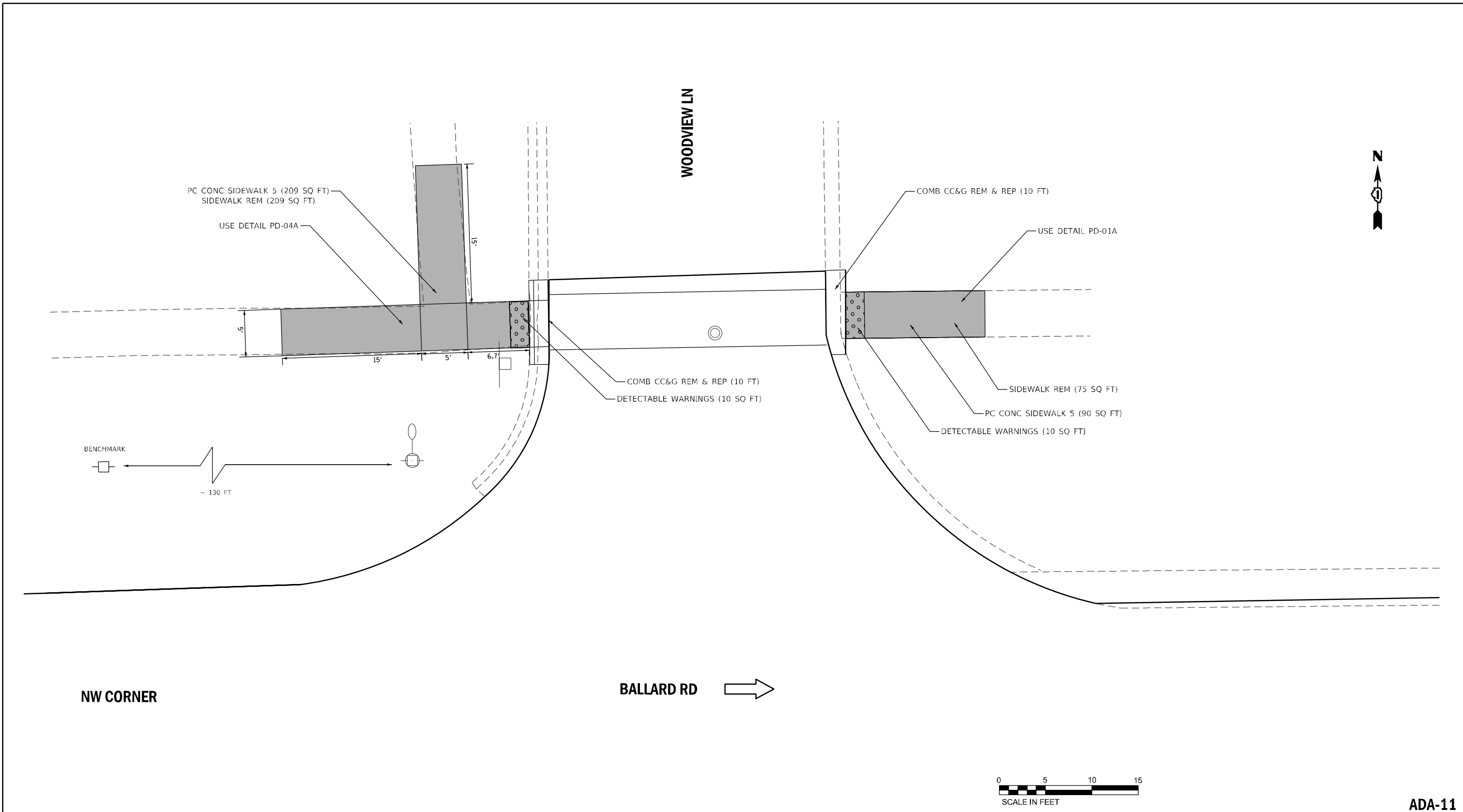
SIDEWALK DETAIL PLAN			
BALLARD RD AT UNNAMED ROAD			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-R5&SW	COOK	50	21
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 62M18				

FILE NAME = P:\2019\ME19003_Ver-Var-PHI\W028_Ballard Road\01-CADD\02-Shts\21-D162M18.sht_ADA-09.dgn
 PLOT SCALE = 10.0000' / 1"
 USER NAME = Millennia Professional Services

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 PLOT SCALE = 10.0000' / 1" / in.
 USER NAME = Millennium Professional Services



NW CORNER

BALLARD RD →

WOODVIEW LN



ADA-11

REFERENCE BENCHMARK ELEV: 642.382
 BENCHMARK : R/R SPIKE IN POWER POLE
 LOCATION : 2ND POWER POLE WEST OF WOODVIEW LN
 (NORTH SIDE OF BALLARD RD)

LEGEND	
xx.xx'	EXISTING LENGTH
=====	PROPOSED SIDE CURB
()	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV:
 BENCHMARK :
 LOCATION :



USER NAME = jcovarrubias	DESIGNED - TN	REVISED -
	DRAWN - JP	REVISED -
PLOT SCALE = 10.0000' / 1" / in.	CHECKED - TN	REVISED -
PLOT DATE = 12/11/2020	DATE - 12/11/2020	REVISED -

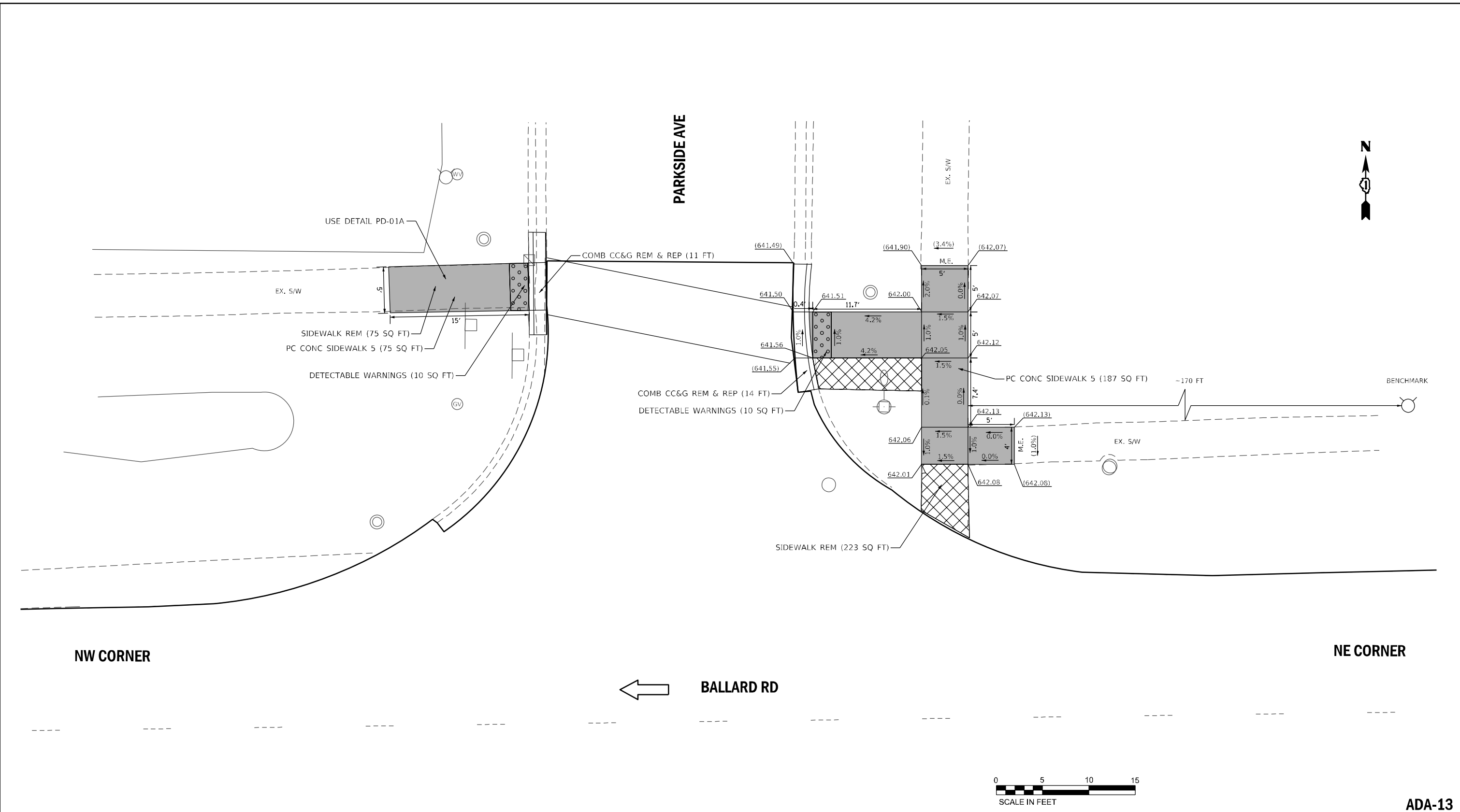
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN			
BALLARD RD AT WOODVIEW LANE			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-R5&SW	COOK	50	23
CONTRACT NO. 62M18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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 PLOT SCALE = 10.0000' / 1" / in.
 USER NAME = Millennia Professional Services



NW CORNER

NE CORNER

← BALLARD RD



ADA-13

REFERENCE BENCHMARK ELEV:
BENCHMARK :
LOCATION :

LEGEND

xx.xx'	EXISTING LENGTH		PROPOSED SIDEWALK
	PROPOSED SIDE CURB		DETECTABLE WARNINGS
()	EXISTING ELEVATION/SLOPE		SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV: 642.906
BENCHMARK : X-CUT ON SW BOLT ON FIRE HYDRANT
LOCATION : EAST OF PARKSIDE AVE (IN FRONT OF SABEL CENTER)



USER NAME = jcovarrubias	DESIGNED - TN	REVISED -
PLOT SCALE = 10.0000' / 1" / in.	DRAWN - JP	REVISED -
PLOT DATE = 12/11/2020	CHECKED - TN	REVISED -
	DATE - 12/11/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN			
BALLARD RD AT PARKSIDE AVE			
SCALE:	SHEET NO. OF	SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-R5&SW	COOK	50	25
CONTRACT NO. 62M18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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WESTERN AVE

← BALLARD RD

NW CORNER

NE CORNER

BENCHMARK

~93 FT

USE DETAIL PD-01B

DETECTABLE WARNINGS (10 SQ FT)

USE DETAIL PD-01A

SIDEWALK REM (58 SQ FT)

PC CONC SIDEWALK 5 (75 SQ FT)

SIDEWALK REM (64 SQ FT)

PC CONC SIDEWALK 5 (84 SQ FT)

DETECTABLE WARNINGS (10 SQ FT)



ADA-14

REFERENCE BENCHMARK ELEV: 642.906
 BENCHMARK : X-CUT ON SW BOLT ON FIRE HYDRANT
 LOCATION : WEST OF WESTERN AVE (IN FRONT OF SABEL CENTER)

LEGEND	
xx.xx'	EXISTING LENGTH
=====	PROPOSED SIDE CURB
()	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV:
 BENCHMARK :
 LOCATION :



USER NAME = jcovarrubias	DESIGNED - TN	REVISED -
PLOT SCALE = 10.0000' / 1"	DRAWN - JP	REVISED -
PLOT DATE = 12/11/2020	CHECKED - TN	REVISED -
	DATE - 12/11/2020	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN			
BALLARD RD AT WESTERN AVE			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-R5&SW	COOK	50	26
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62M18	

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 USER NAME = Millennia Professional Services

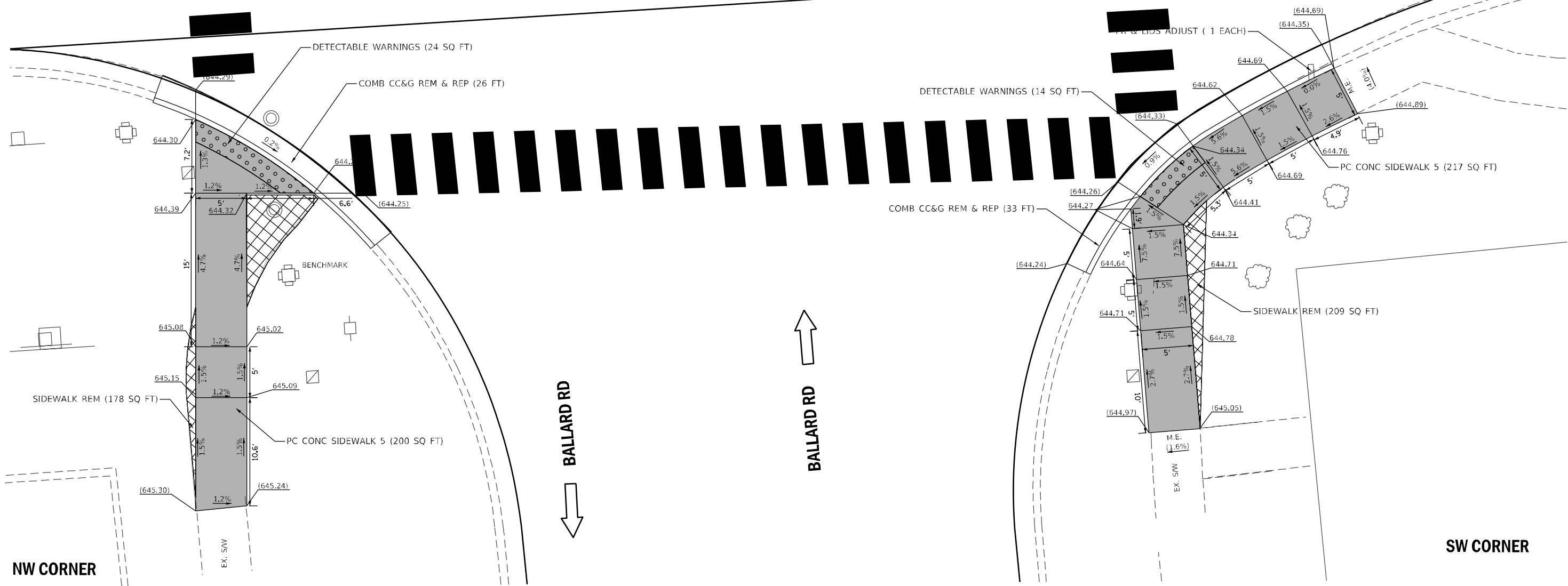
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GREENWOOD RD →

↑ BALLARD RD
↓

↑ BALLARD RD



NW CORNER

SW CORNER



ADA-16

REFERENCE BENCHMARK ELEV: 645.09
 BENCHMARK : SQUARE ON SOUTH SIDE OF MAST ARM CONCRETE BASE
 LOCATION : AT NW CORNER OF BALLARD RD AND GREENWOOD AVE

LEGEND	
xx.xx'	EXISTING LENGTH
=====	PROPOSED SIDE CURB
()	EXISTING ELEVATION/SLOPE
■	PROPOSED SIDEWALK
●●●●	DETECTABLE WARNINGS
⊗	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV:
 BENCHMARK :
 LOCATION :

FILE NAME = P:\2019\ME19003_Ver_Var_PHI\W028_Ballard Road\01-CADD\02-Shts\28-D162M18.sht, ADA-16.dgn
 PLOT SCALE = 10.0000' / 1" = 10000.0000
 USER NAME = Millennium Professional Services



USER NAME = jcovarrubias	DESIGNED - TN	REVISED -
PLOT SCALE = 10.0000' / 1"	DRAWN - JP	REVISED -
PLOT DATE = 12/11/2020	CHECKED - TN	REVISED -
	DATE - 12/11/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN			
BALLARD RD AT GREENWOOD RD			
SCALE:	SHEET NO. OF SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-R5&SW	COOK	50	28
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 62M18				

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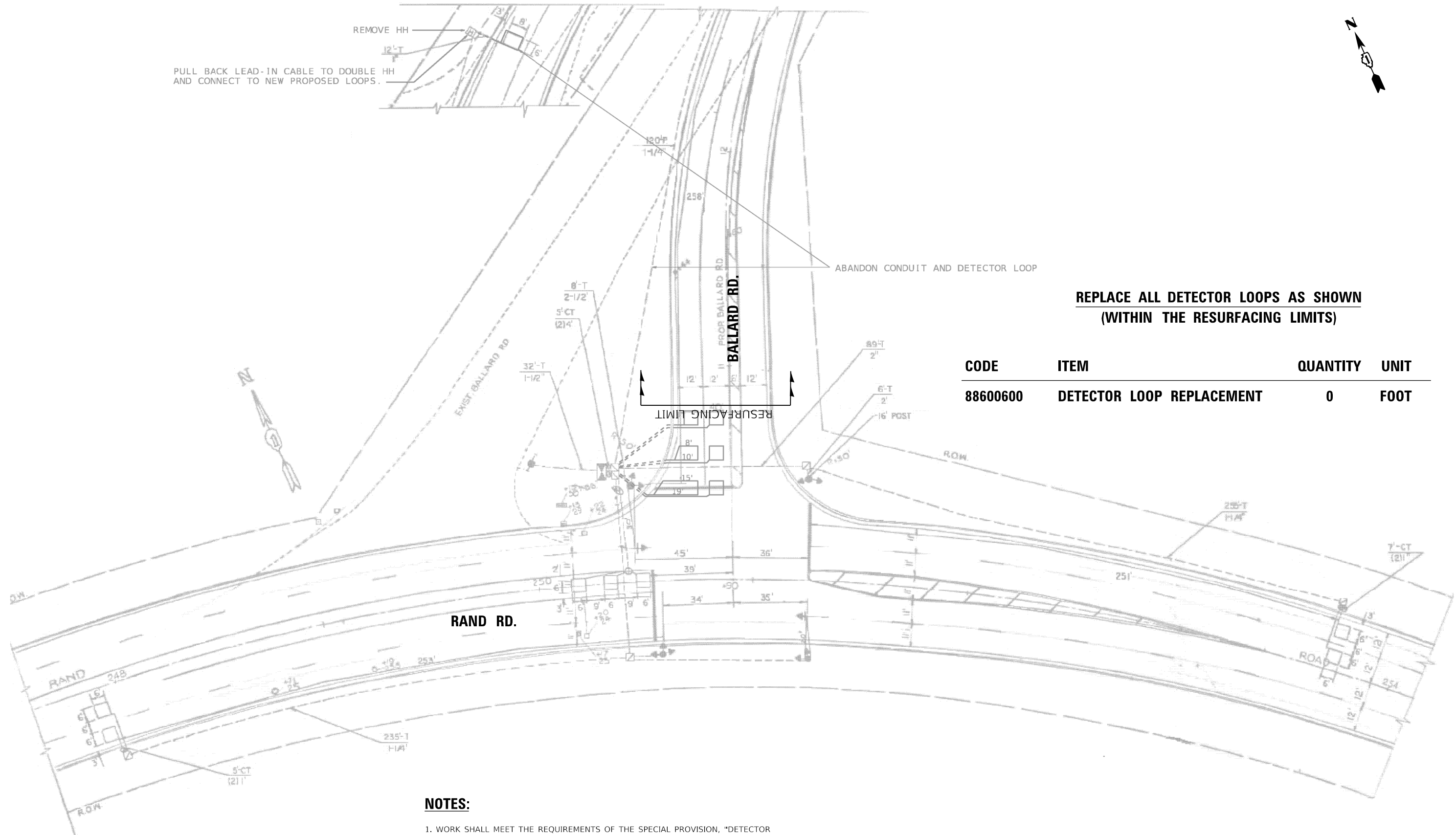
PULL BACK LEAD-IN CABLE TO DOUBLE HH AND CONNECT TO NEW PROPOSED LOOPS.

REMOVE HH

ABANDON CONDUIT AND DETECTOR LOOP

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	0	FOOT



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

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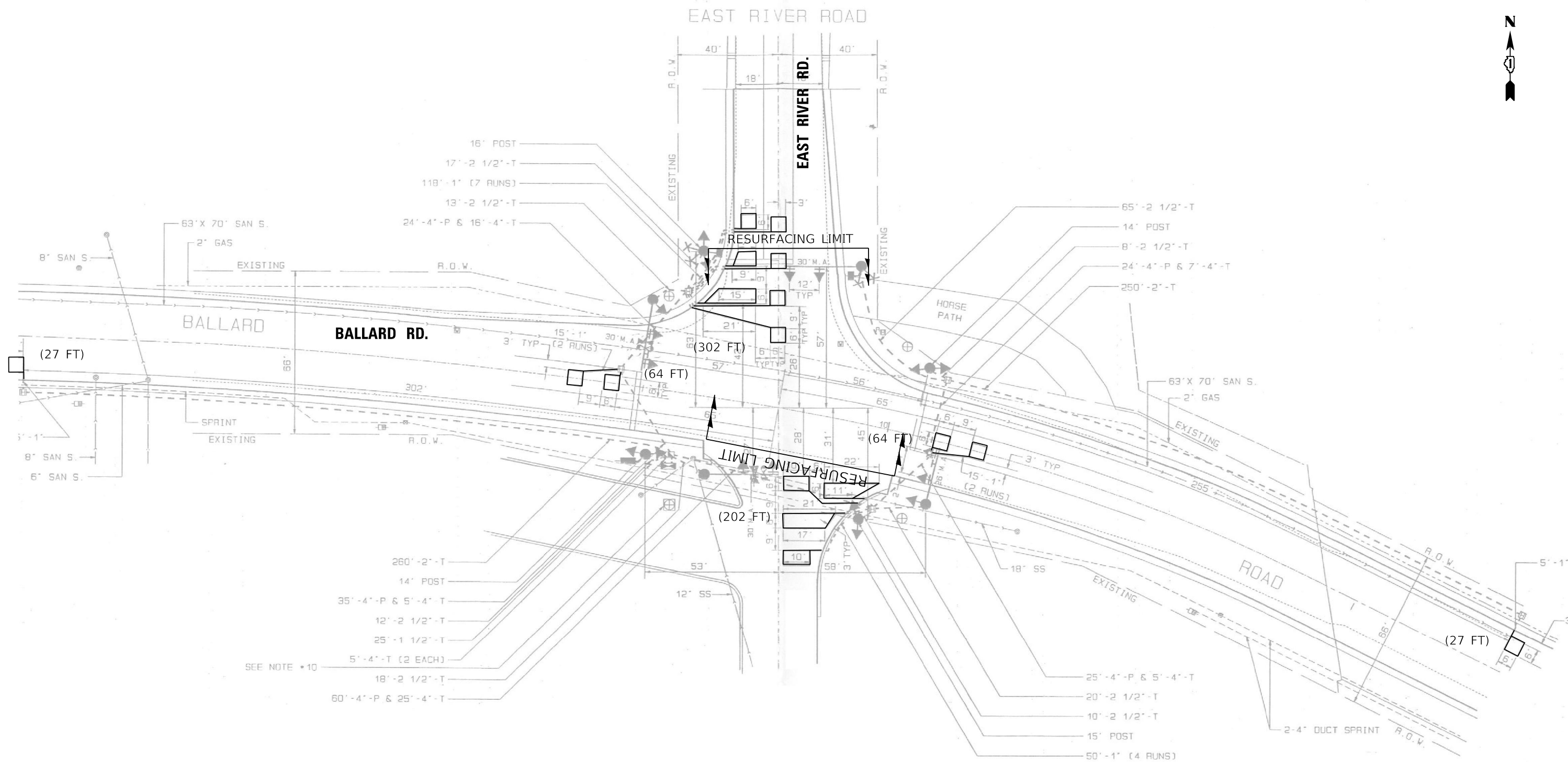
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	DRAWN - CJ	REVISED -
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PLOT DATE = 1/14/2021	DATE - 10/28/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
BALLARD RD. AT RAND RD.**

SCALE: 1"=20' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-R5&SW	COOK		
CONTRACT NO. 62M18			ILLINOIS FED. AID PROJECT	



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	686	FOOT

TS: 20385

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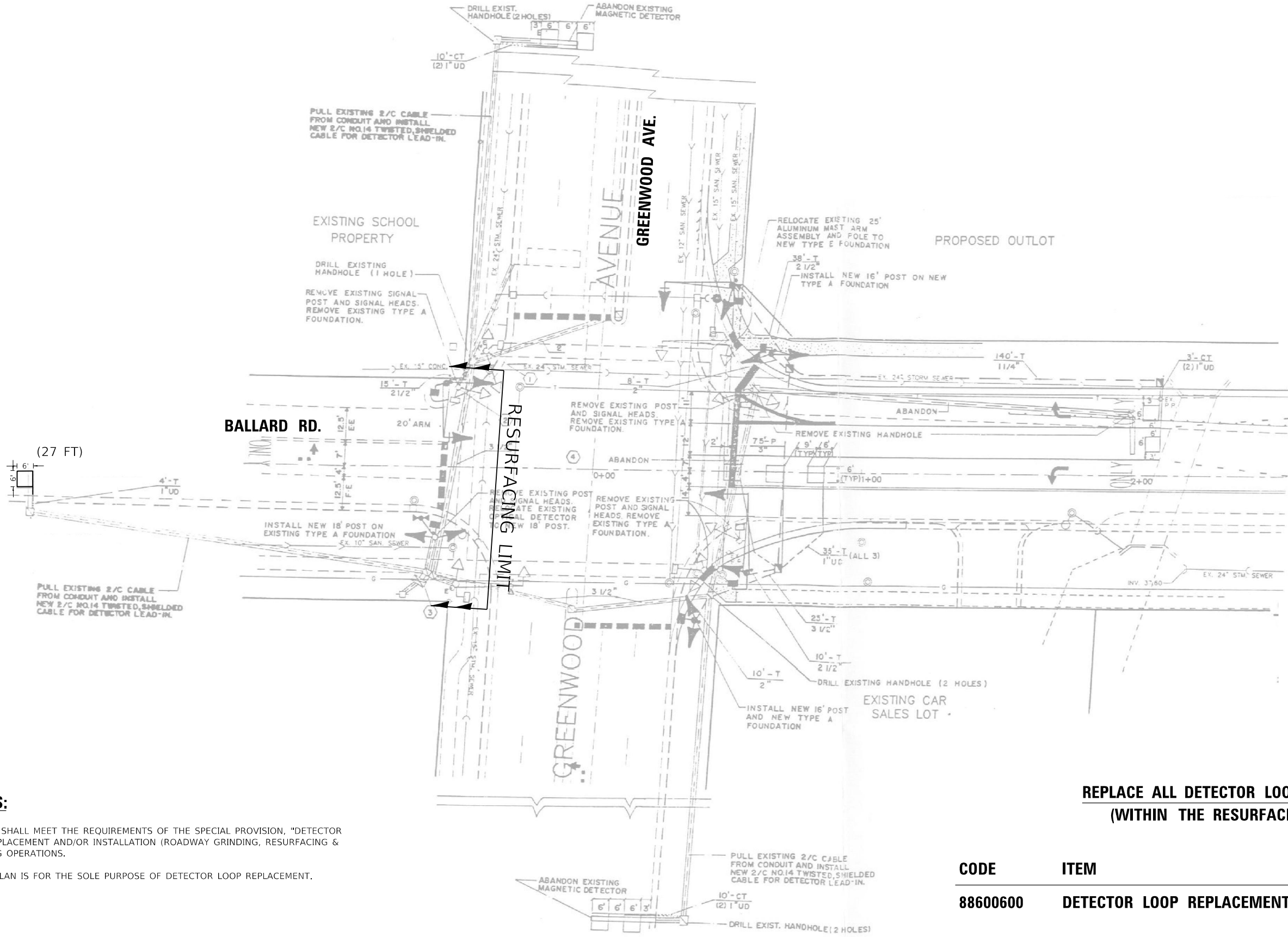
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	DRAWN - CJ	REVISED -
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PLOT DATE = 10/27/2020	DATE - 10/28/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
BALLARD RD. AT EAST RIVER RD.**

SCALE: 1"=20' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-R5&SW	COOK	50	30
CONTRACT NO. 62M18				
ILLINOIS FED. AID PROJECT				



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	27	FOOT

TS: 4150

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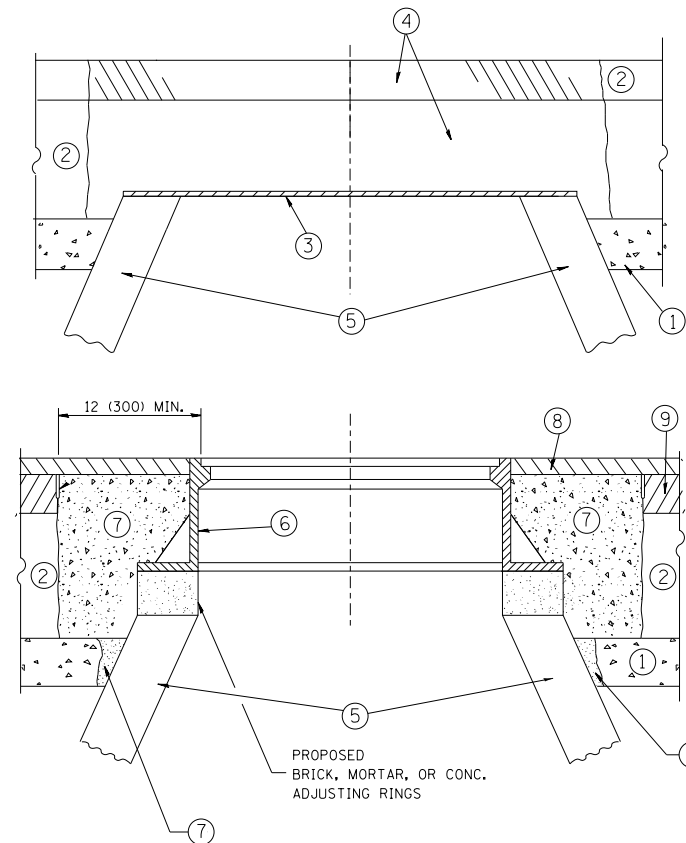
USER NAME = jiangc	DESIGNED - CJ	REVISED -
	DRAWN - CJ	REVISED -
PLOT SCALE = 40.0000 ' / in.	CHECKED - LP	REVISED -
PLOT DATE = 10/27/2020	DATE - 10/28/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
BALLARD RD. AT GREENWOOD AVE.**

SCALE: 1"=20' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-R5&SW	COOK	50	33
CONTRACT NO. 62M18				
ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

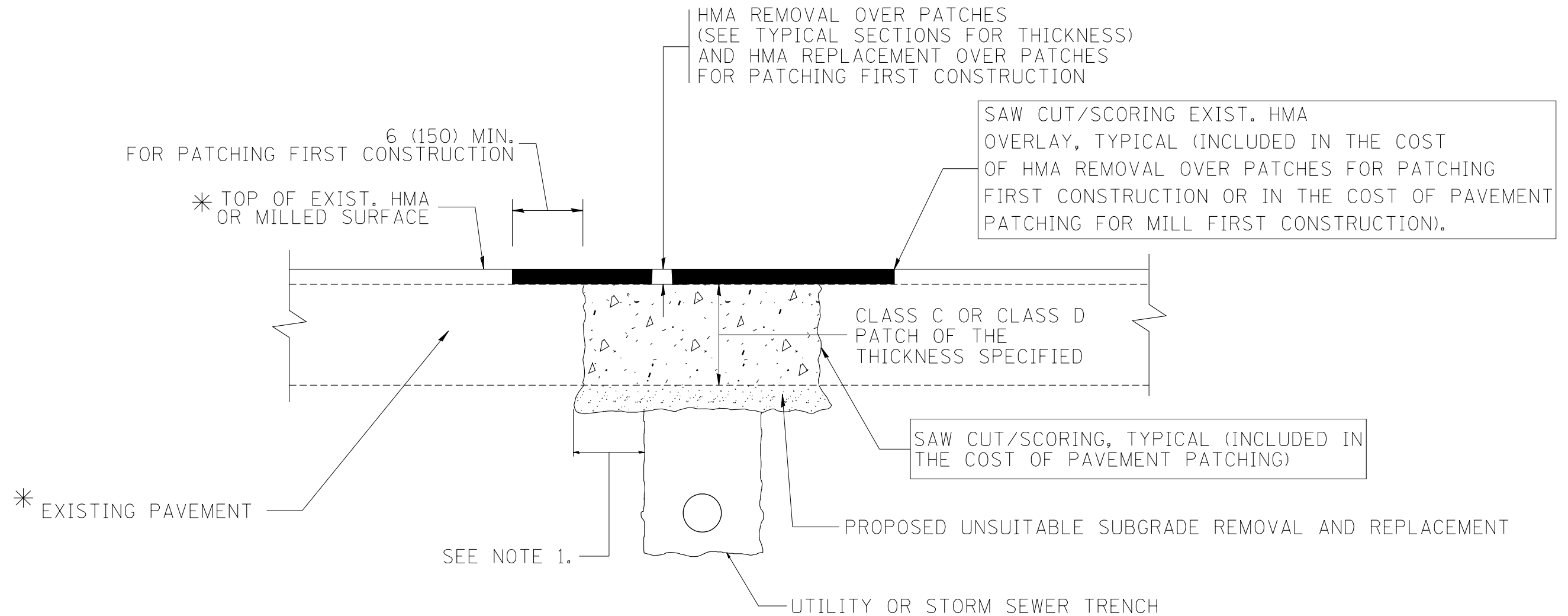
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
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	PLOT SCALE = 1/968.5000 "/ m	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	34
BD600-03 (BD-8)		CONTRACT NO. 62M18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

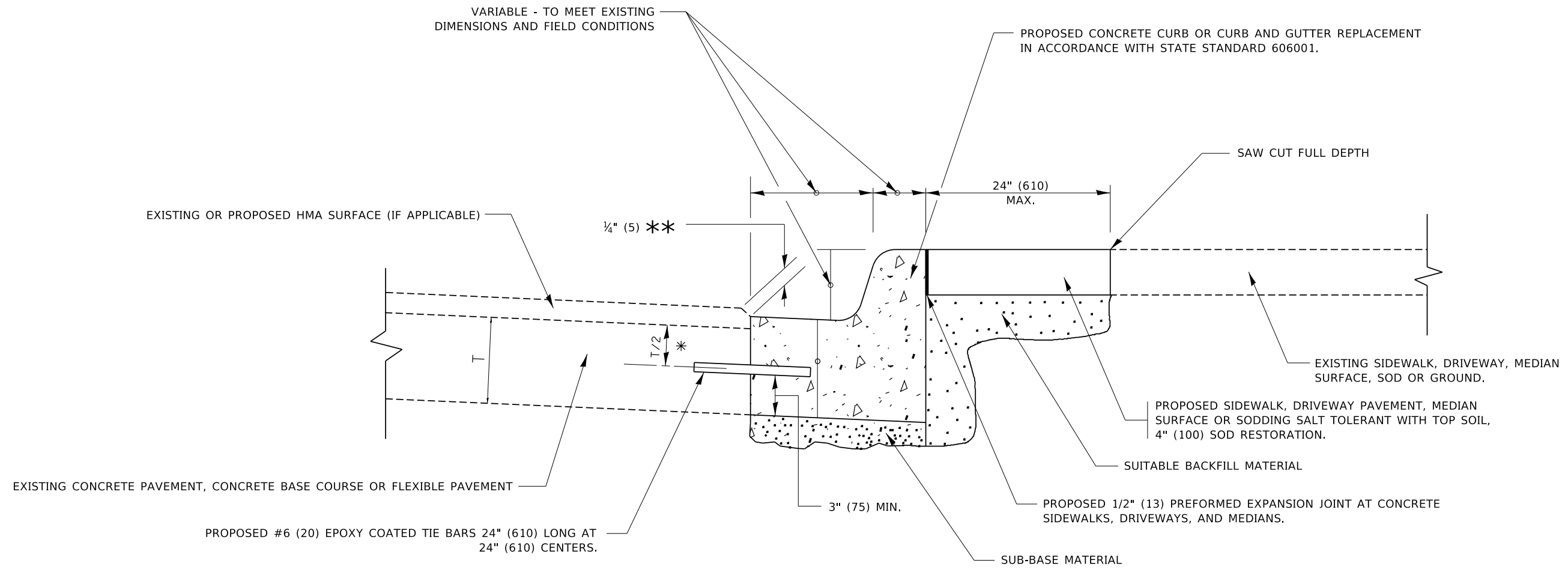
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		PLOT SCALE = 50.000' / IN.	REVISED - R. BORO 09-04-07
		PLOT DATE = 10/27/2008	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2020-140-RS&SW	COOK	50	35
BD400-04 (BD-22)			CONTRACT NO. 62M18	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

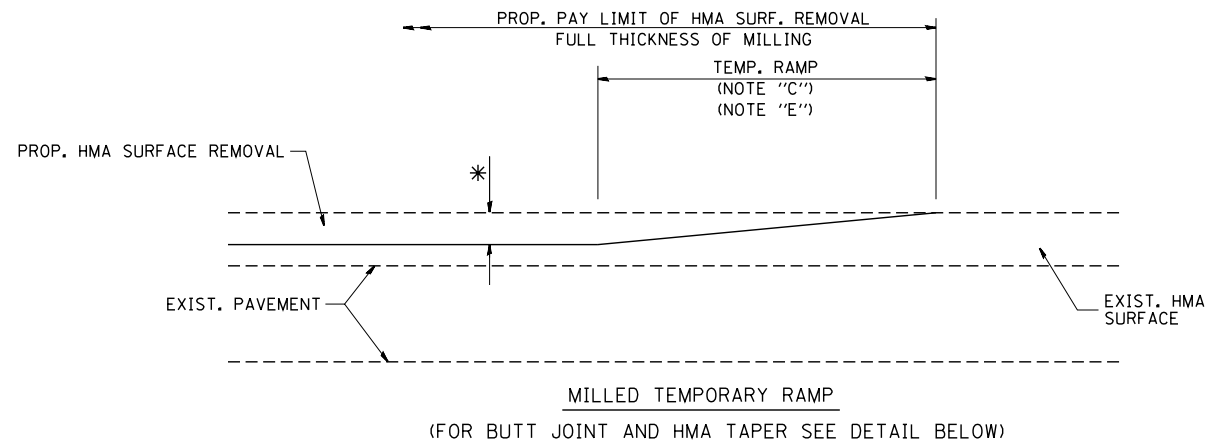
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	DRAWN -	REVISED - M. GOMEZ 01-22-01
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PLOT DATE = 7/11/2019	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

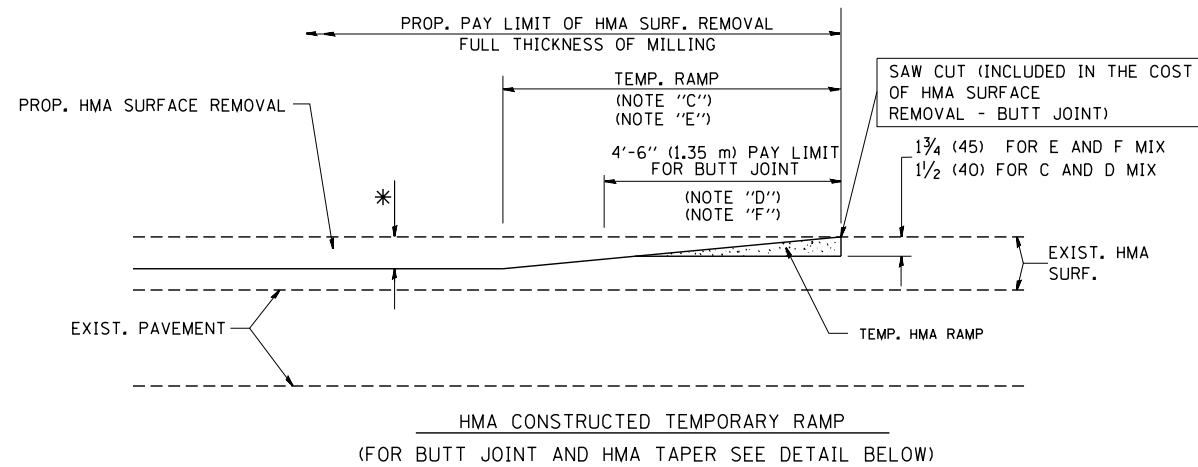
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.

F.A.O. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD600-06 (BD-24)			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				

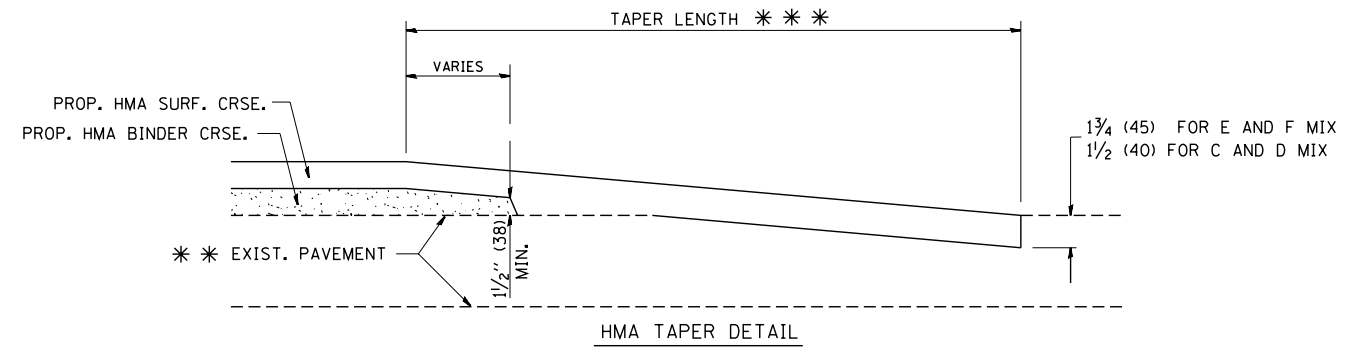
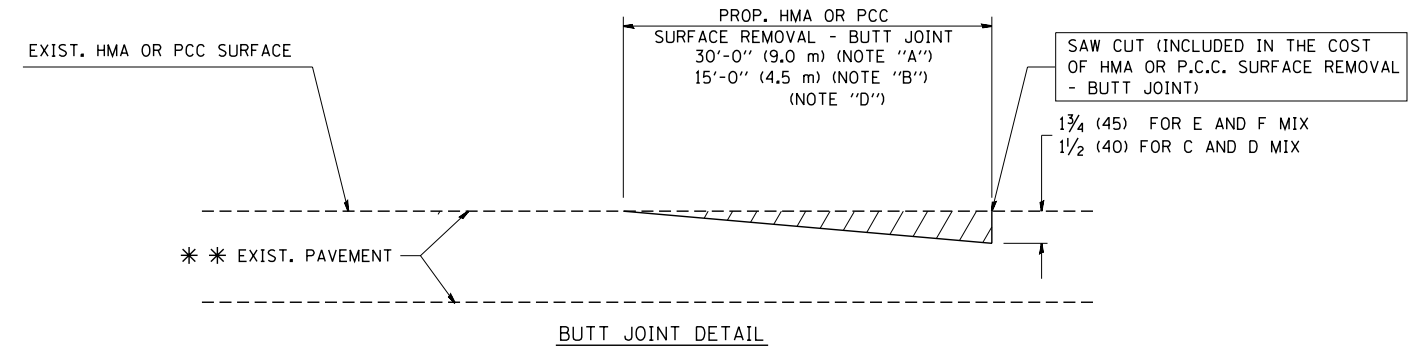


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

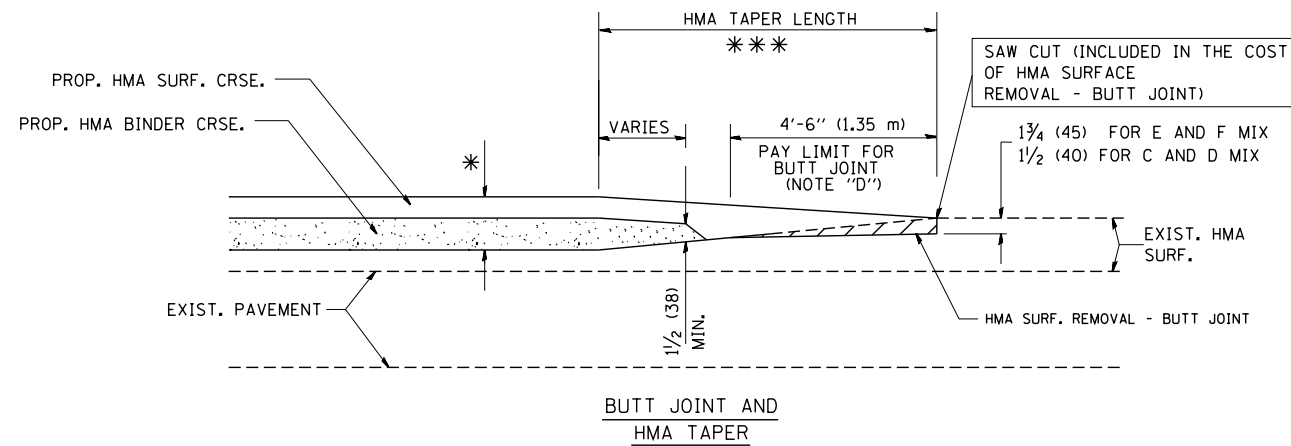
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

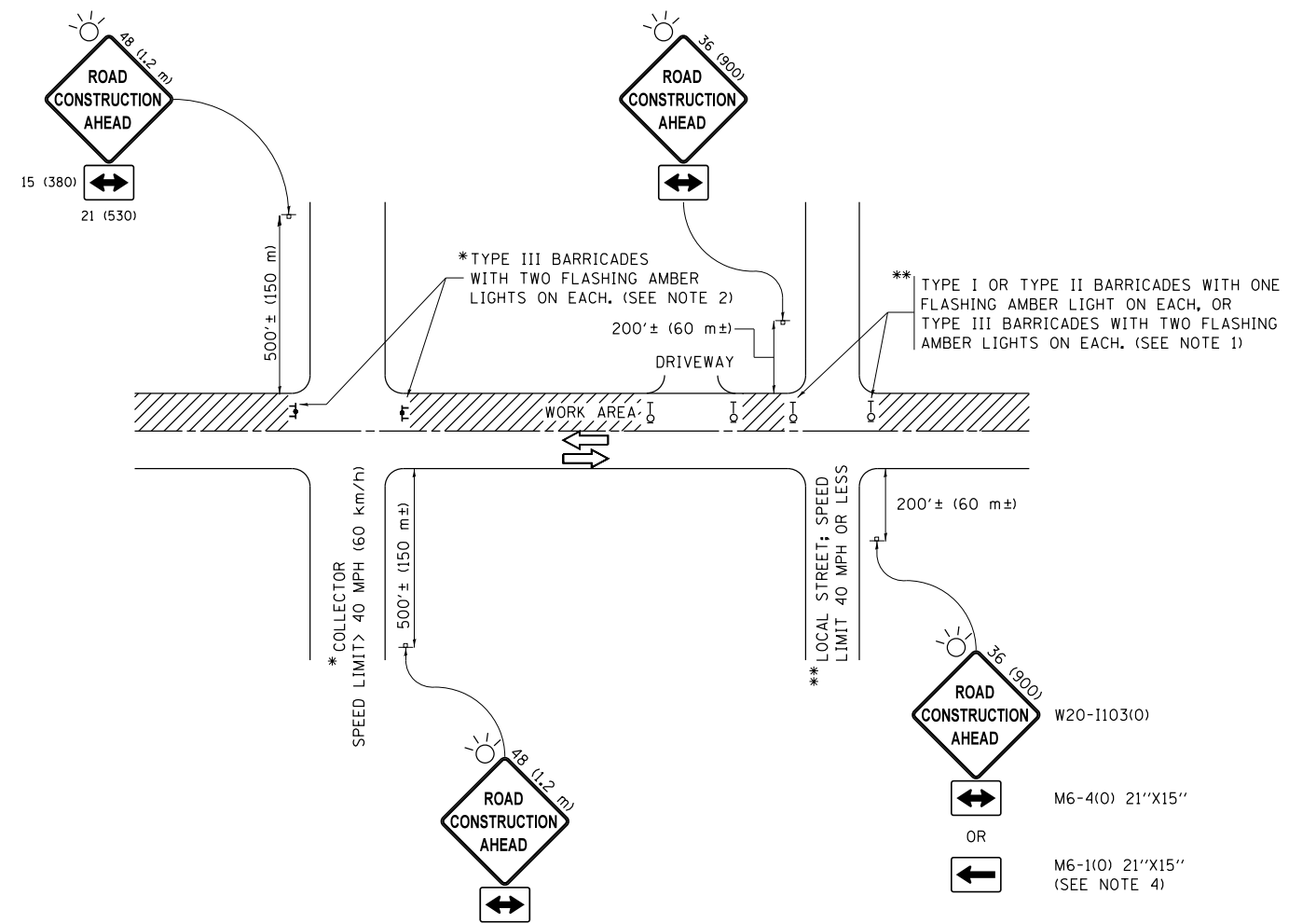
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	37
BD400-05 BD32		CONTRACT NO. 62M18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

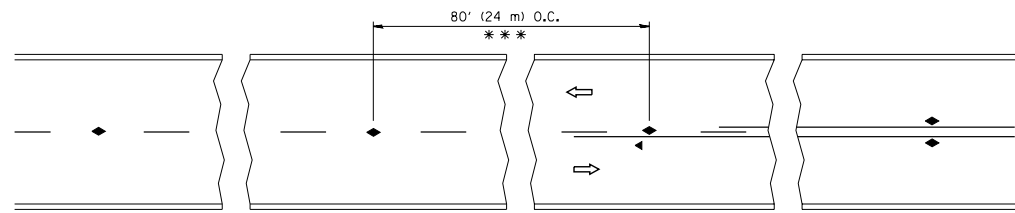
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	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

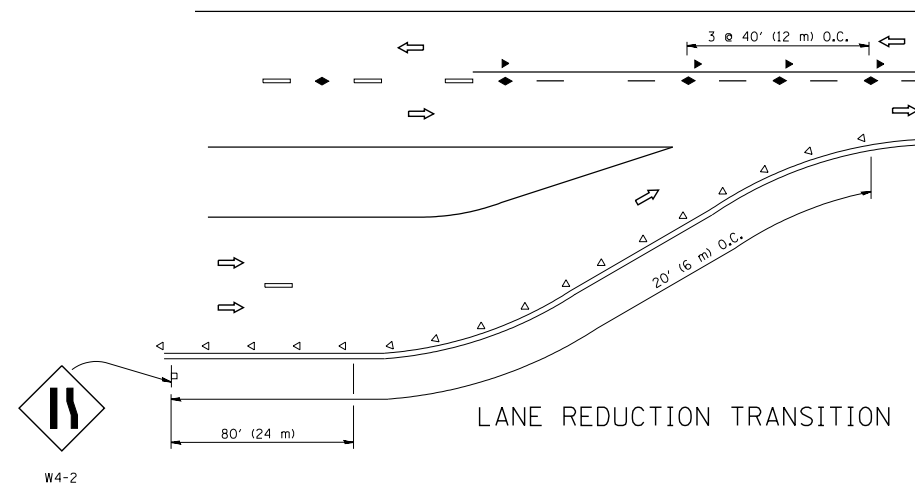
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 62M18	
ILLINOIS FED. AID PROJECT				

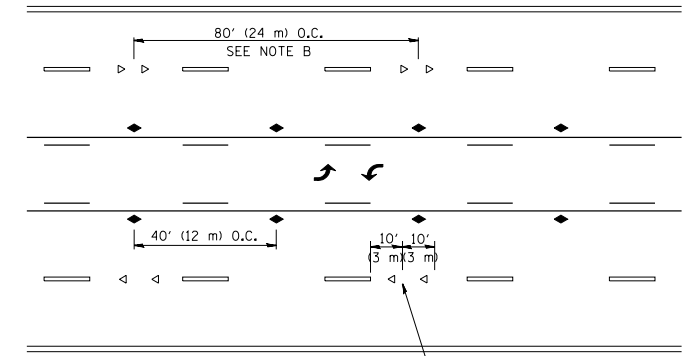


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

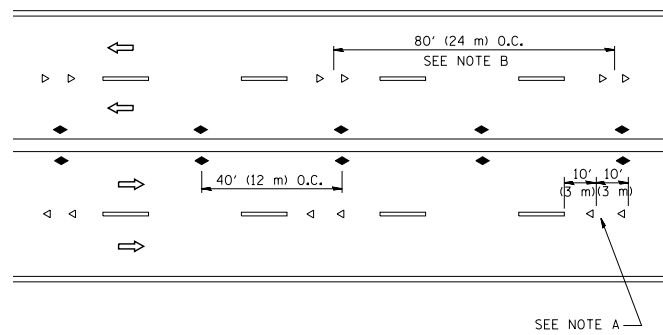
TWO-LANE/TWO-WAY



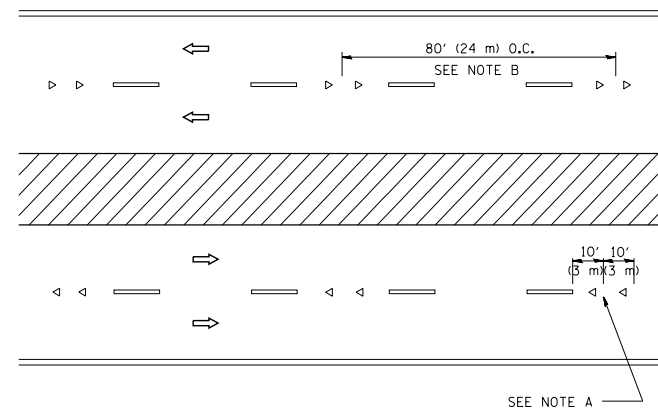
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

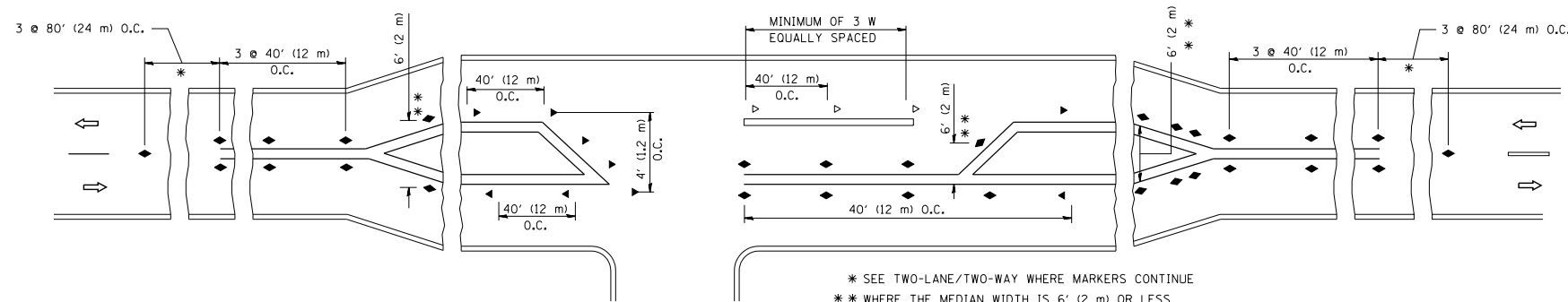
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

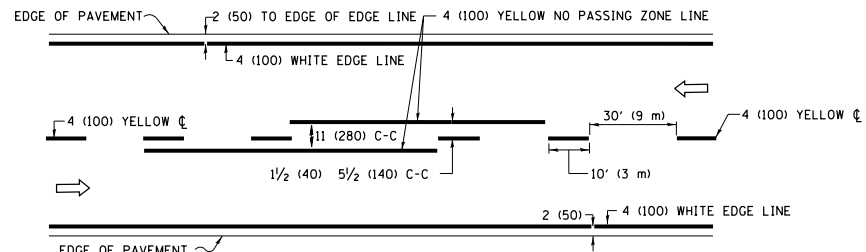
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		PLOT DATE = 3/2/2011	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

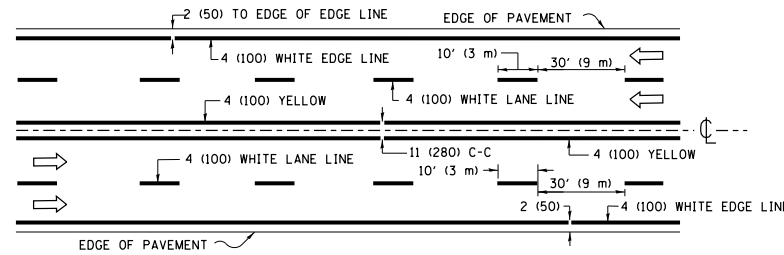
TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

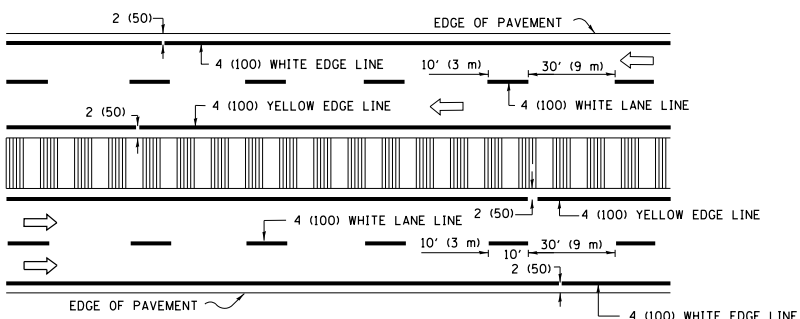
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	39
TC-11		CONTRACT NO. 62M18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

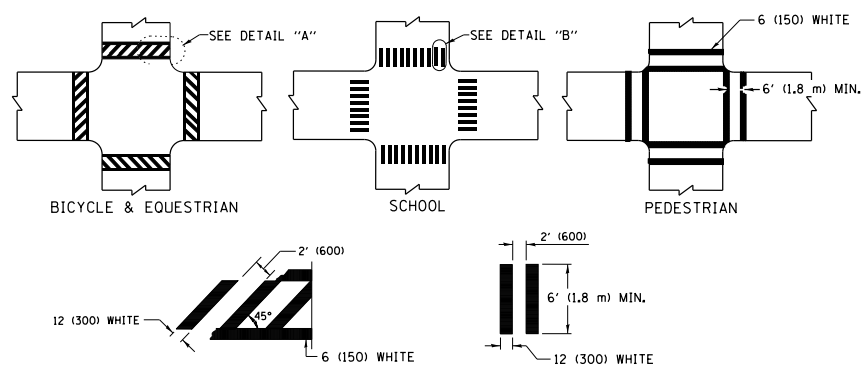


MULTI-LANE UNDIVIDED



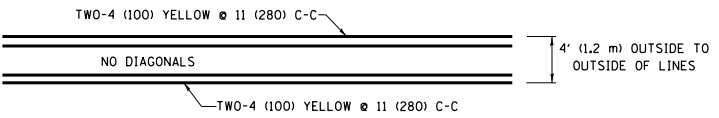
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

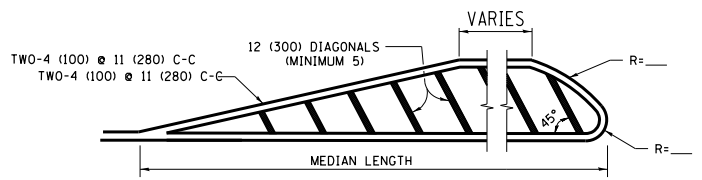


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

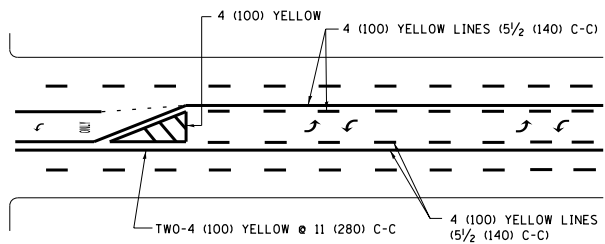


4' (1.2 m) WIDE MEDIANS ONLY

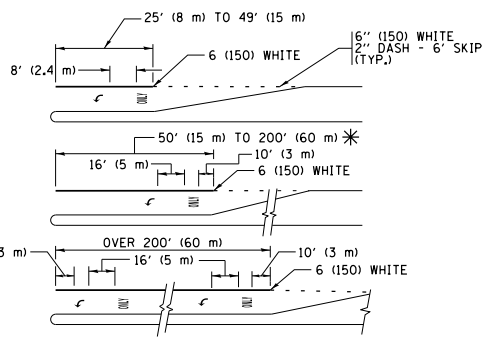


MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

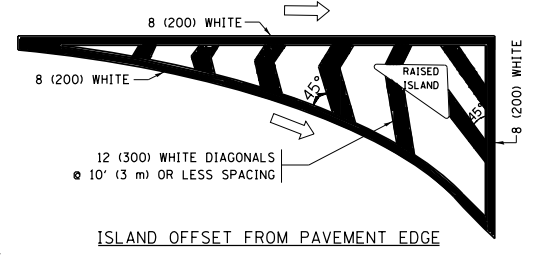


**MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING**

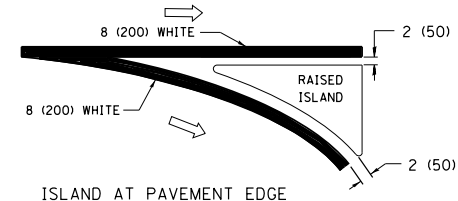


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

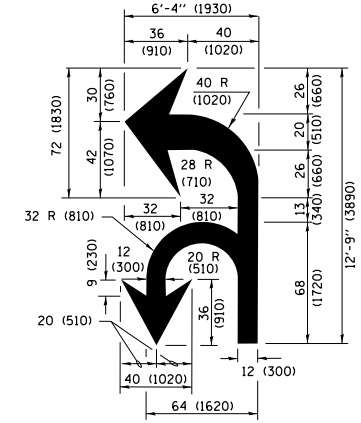
**TYPICAL LEFT (OR RIGHT) TURN LANE
TYPICAL TURN LANE MARKING**



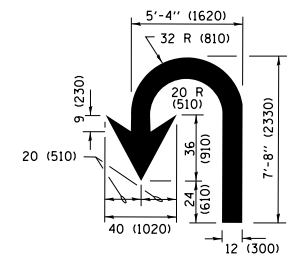
ISLAND OFFSET FROM PAVEMENT EDGE



**ISLAND AT PAVEMENT EDGE
TYPICAL ISLAND MARKING**



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE, FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = footemj	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw:\IL\084EBID\INTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\Dist 1\DRM\CADD\cadd\CAD\sheet\13.dgn		CHECKED -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 50.000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 4/13/2016		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	40
TC-13			CONTRACT NO. 62M18	
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

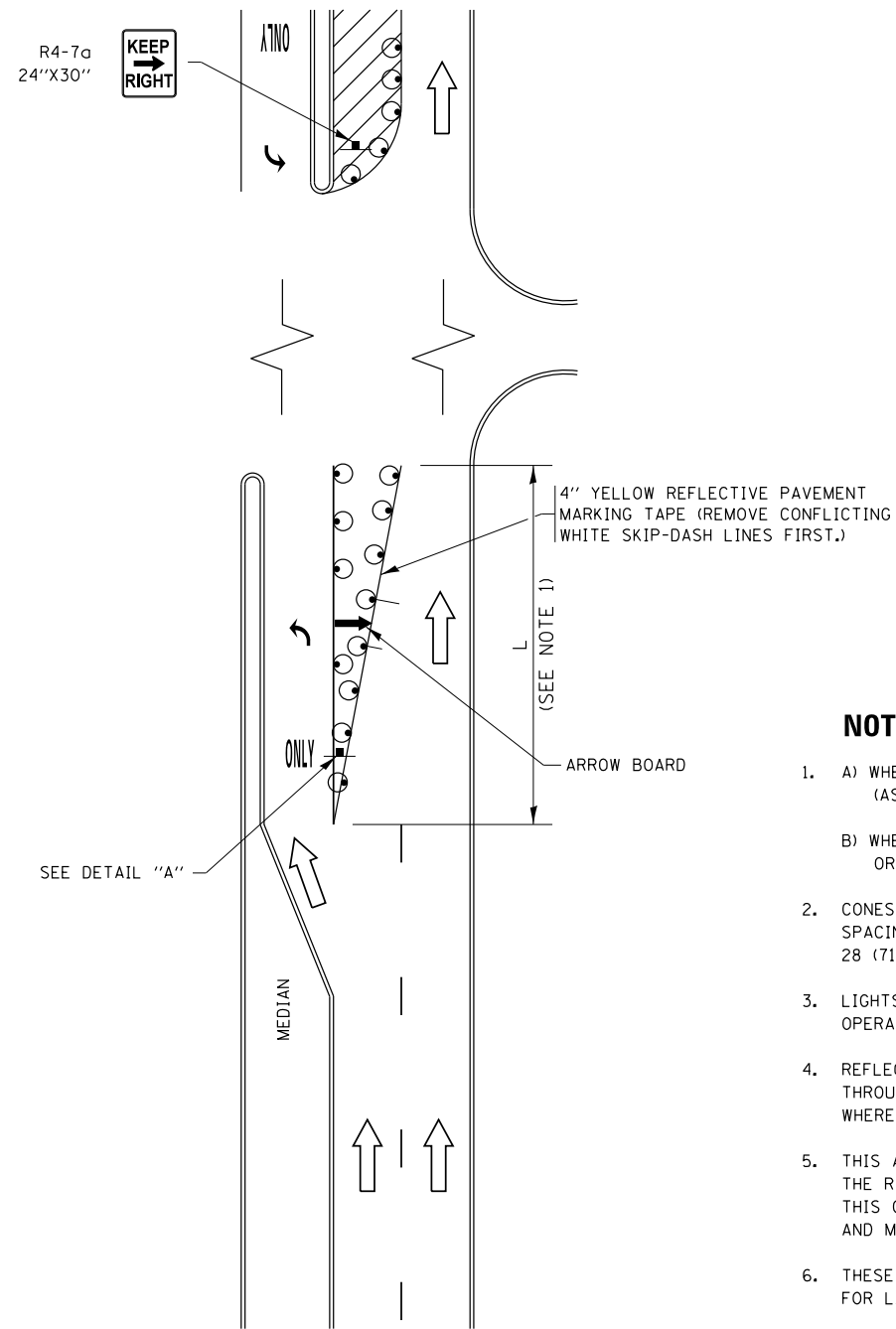


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

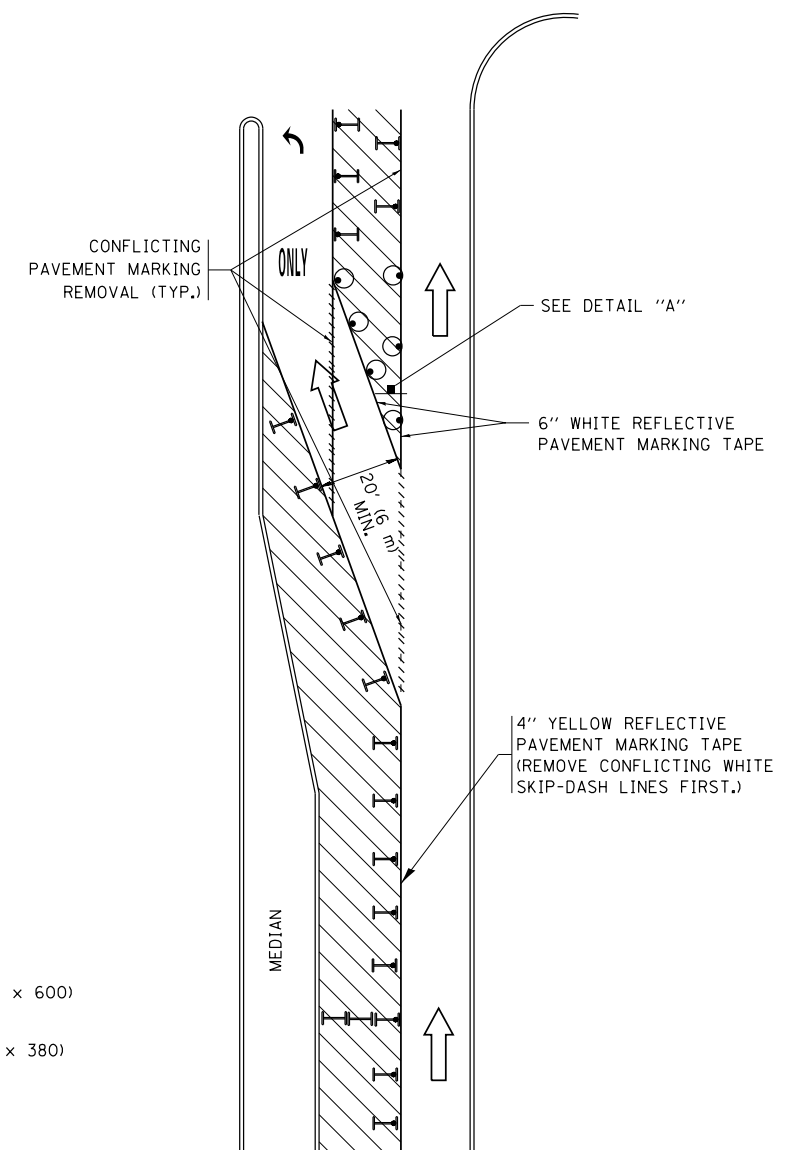


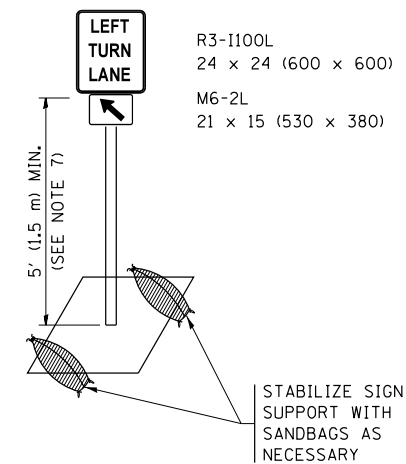
FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

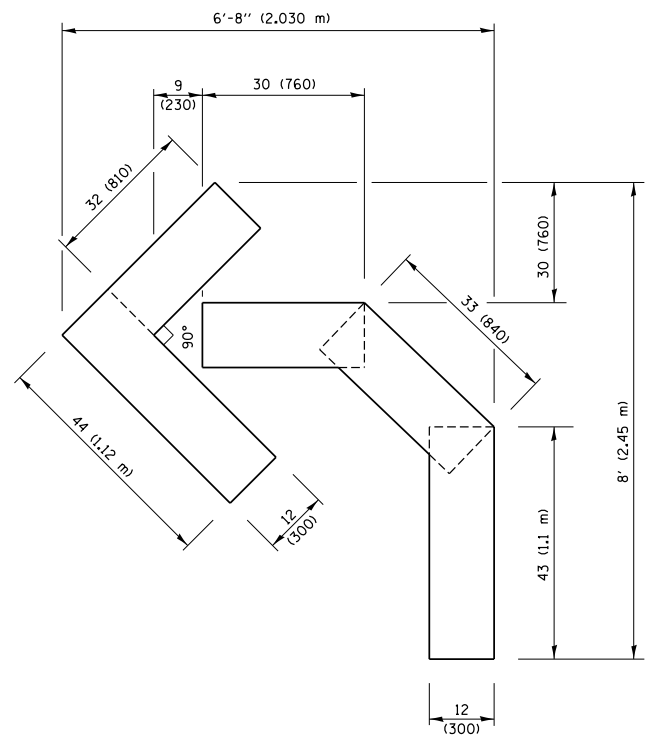
1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



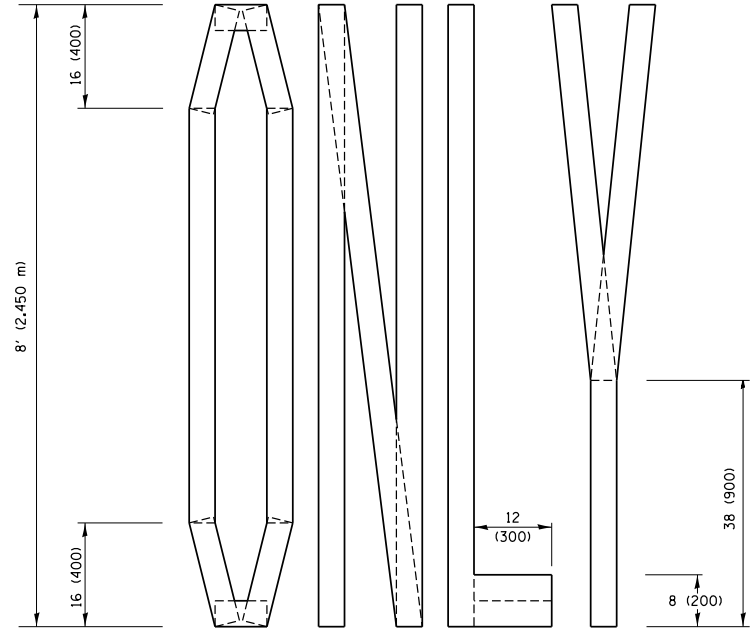
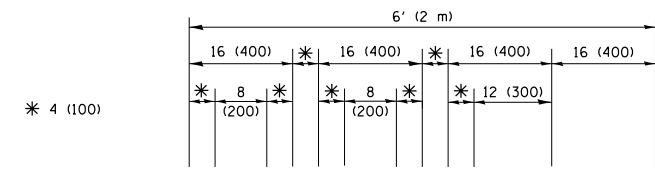
DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

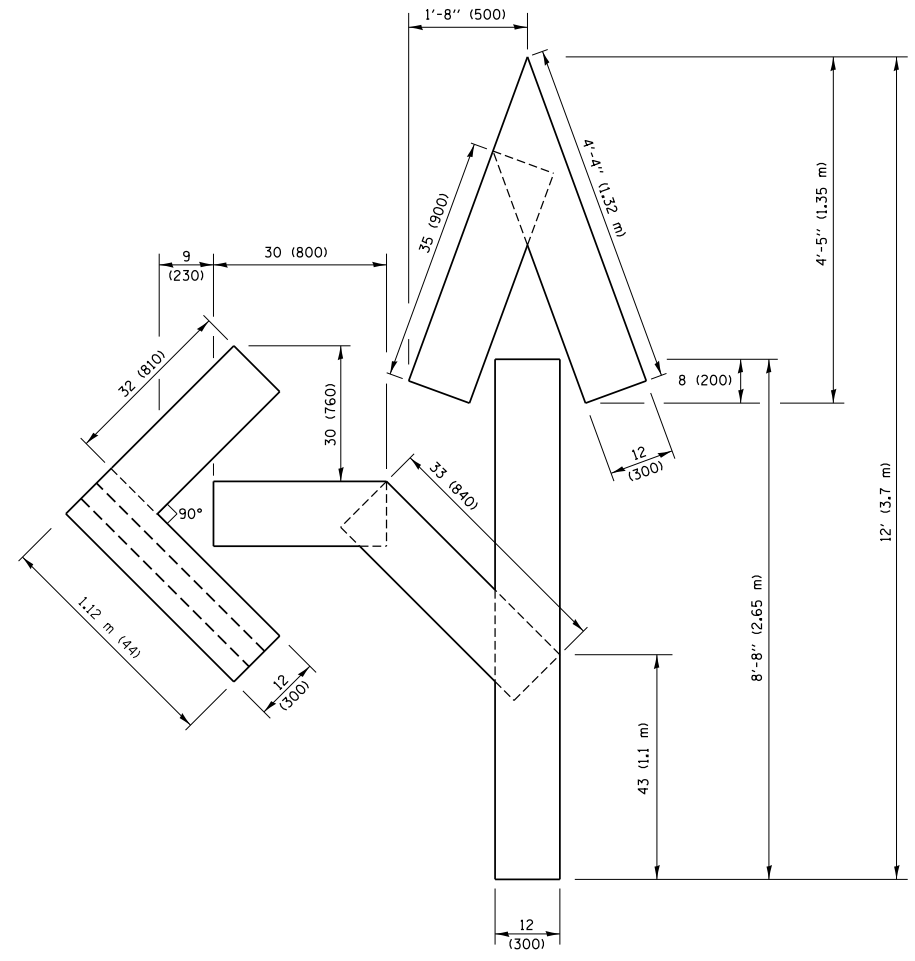
FILE NAME =	USER NAME = footemj	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	PLOT SCALE = 50.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			1319	2020-140-RS&SW	COOK	50	41
	PLOT DATE = 9/15/2016	REVISED - T. RAMMACHER 01-06-00	REVISED -			TC-14		CONTRACT NO. 62M18		ILLINOIS FED. AID PROJECT



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

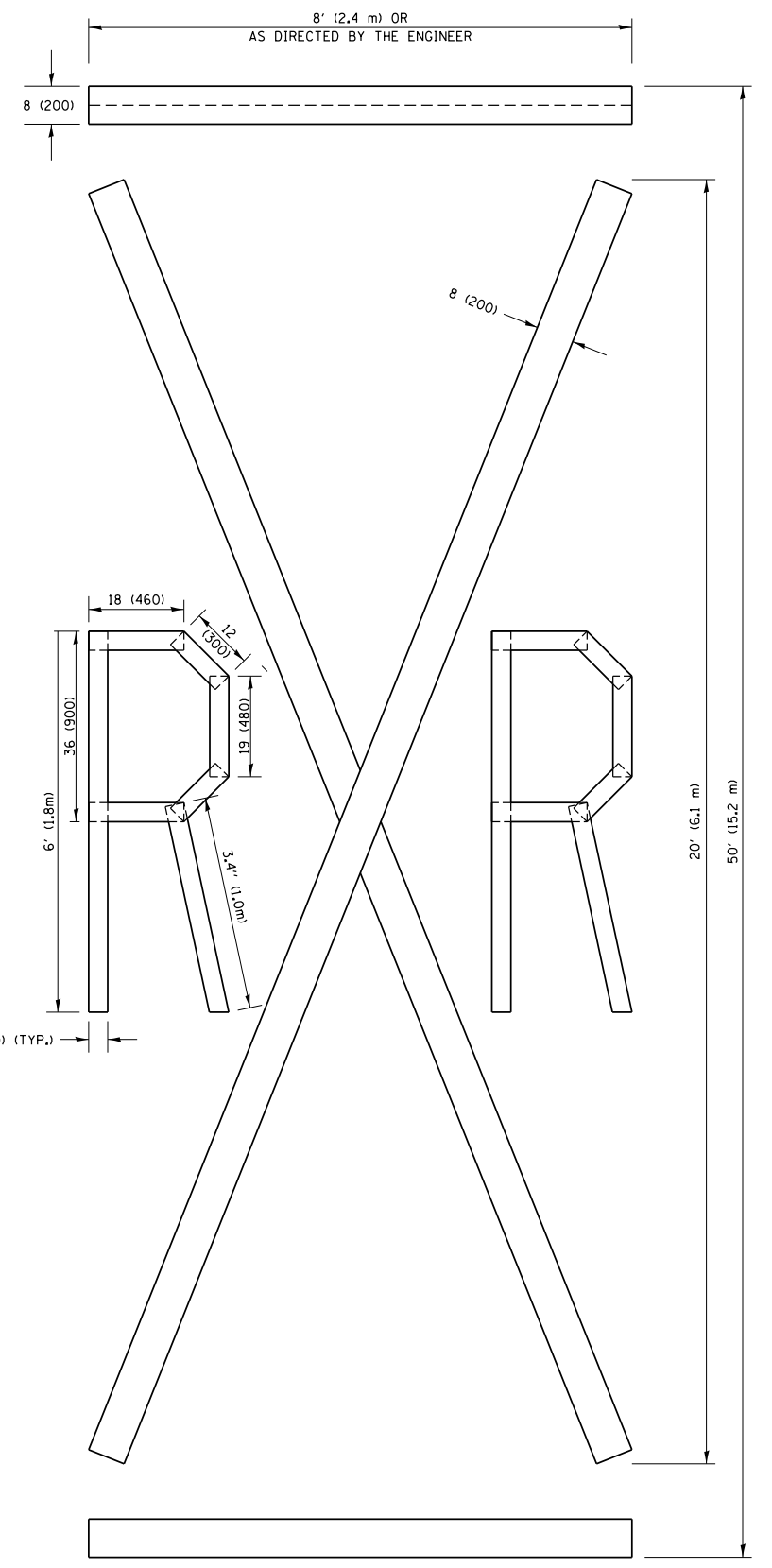


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

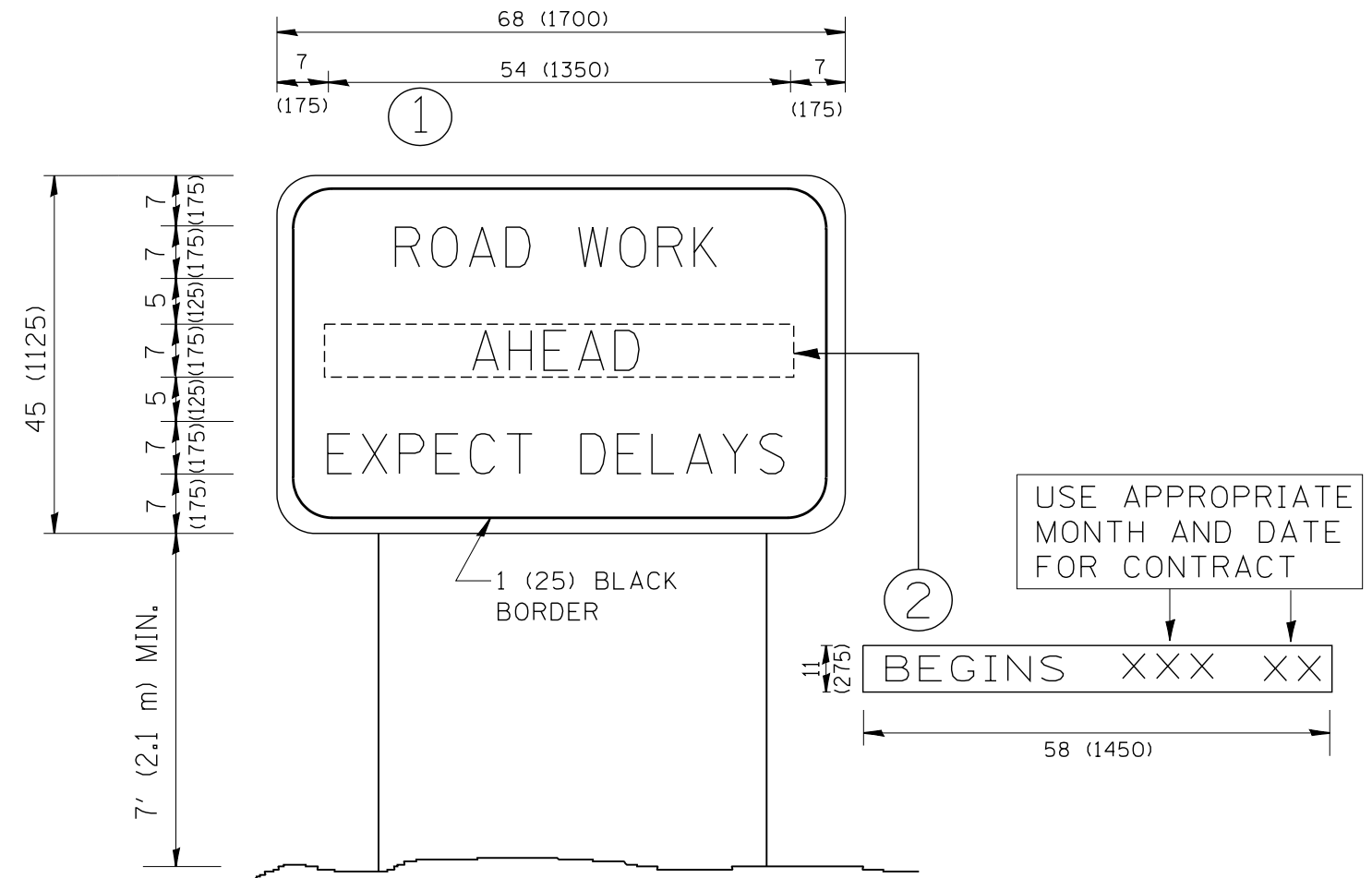
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = footemj	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
pw:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\Dist 1\084EBIDINTEG\CADD\CAD\Sheets\tc16.dgn		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	42
TC-16		CONTRACT NO. 62M18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

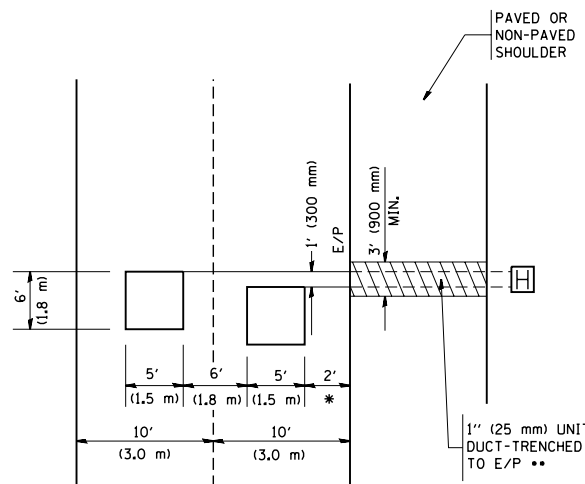
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gaglionobt	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
		DRAWN -	REVISED - R. MIRS 12-11-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	1319	2020-140-RS&SW	COOK	50	43
		PLOT SCALE = 50.000' / IN.	CHECKED -		REVISED - T. RAMMACHER 02-02-99			TC-22		CONTRACT NO. 62M18			
		PLOT DATE = 1/4/2008	DATE -		REVISED - C. JUCIUS 01-31-07	<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>							

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



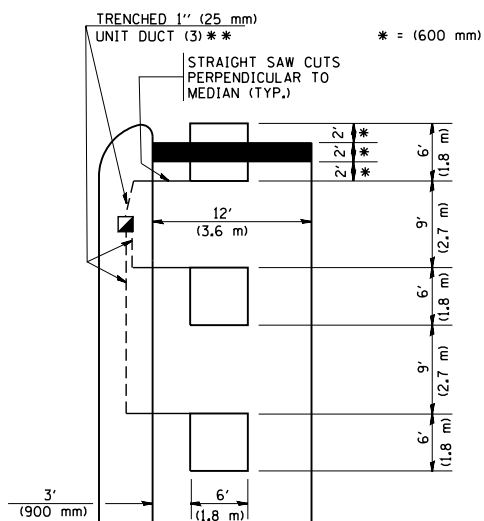
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

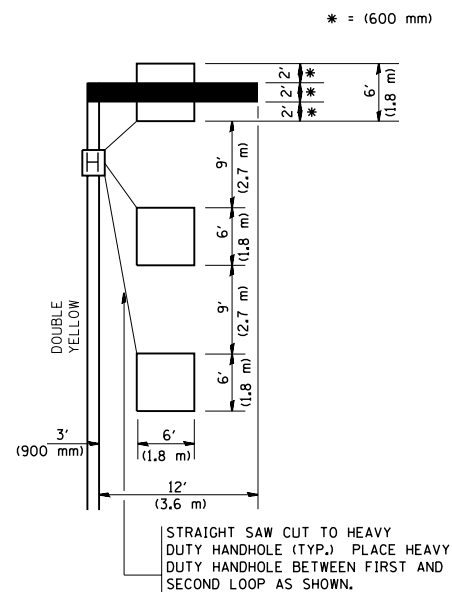


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

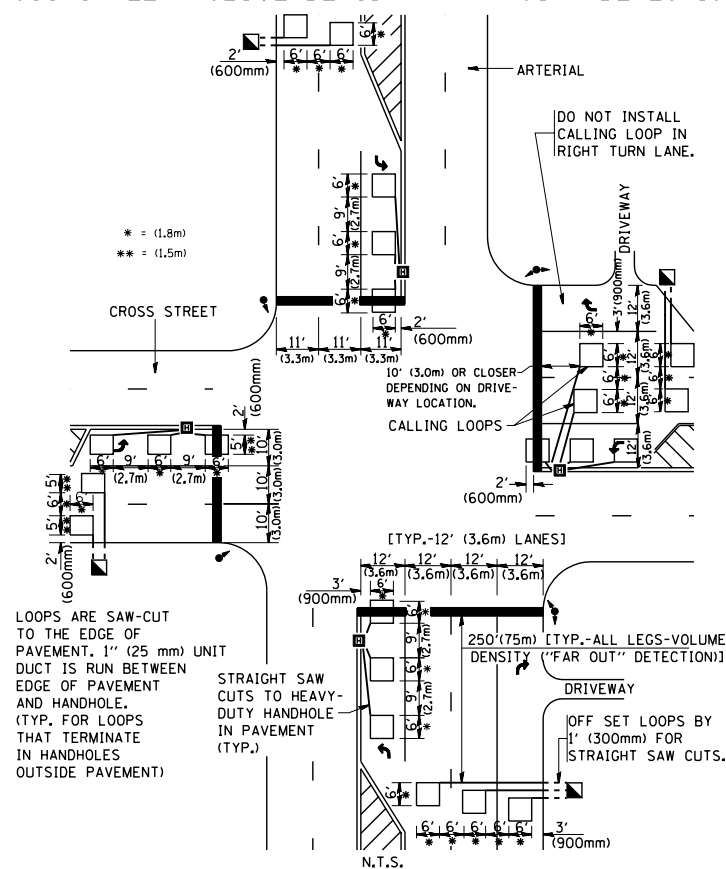
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



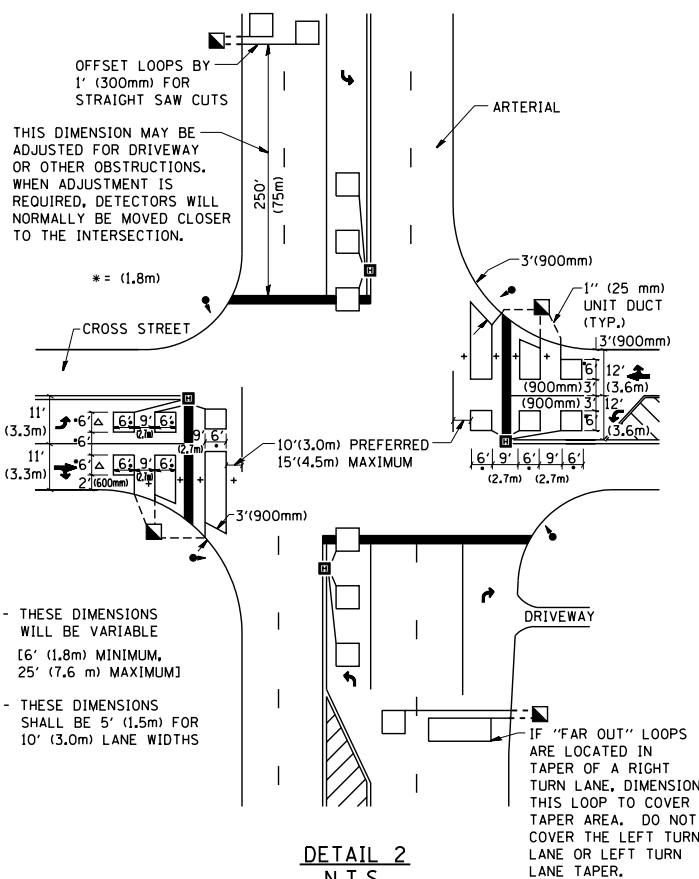
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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USER NAME = gaglanobt
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 1/4/2008

DESIGNED -
DRAWN -
CHECKED - R.K.F.
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

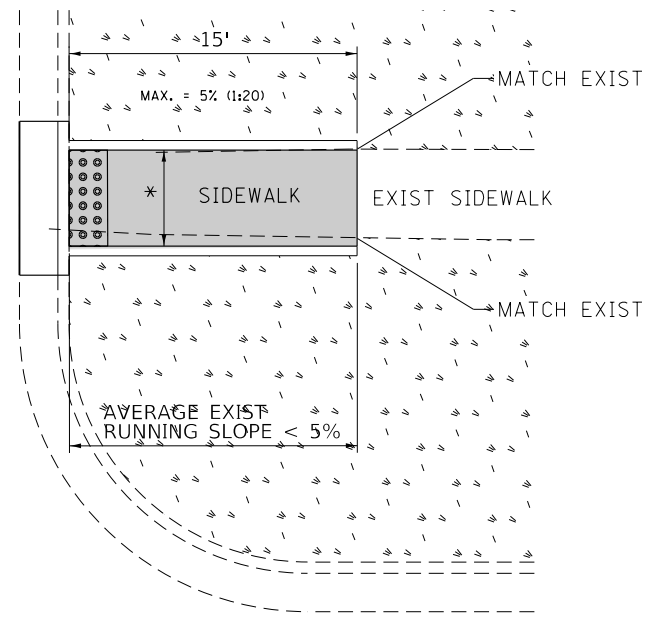
DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

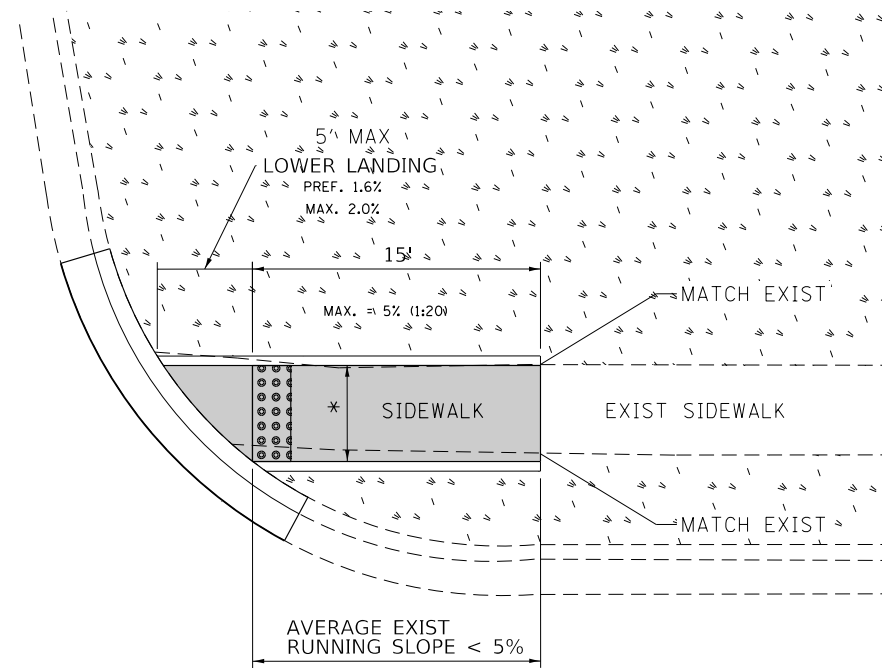
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	44
TS-07		CONTRACT NO. 62M18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

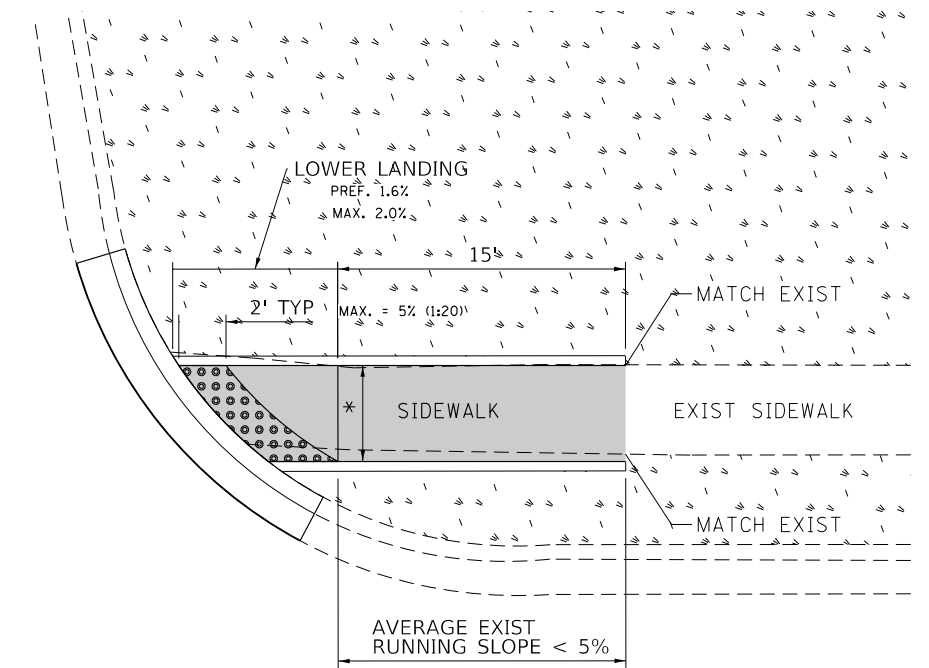
PD-01A



PD-01B



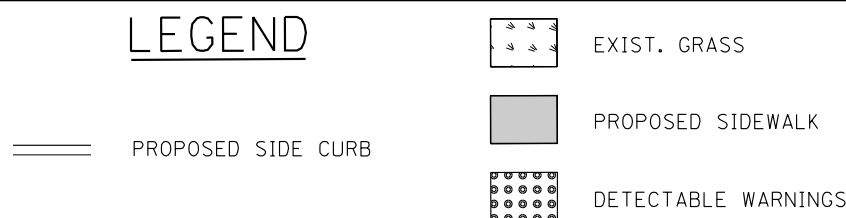
PD-01C



DESIGNER NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND



CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

FILE NAME =	USER NAME = ldezma	DESIGNED -	REVISED -
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	PLOT DATE = 11/12/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

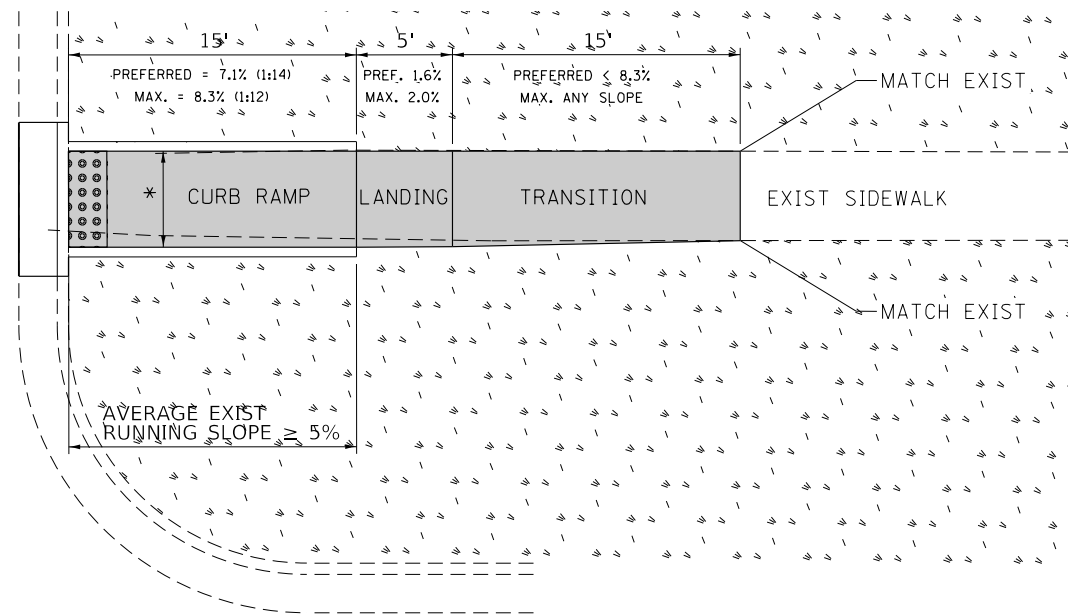
PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
(PD-01)

SCALE: SHEET OF SHEETS STA. TO STA.

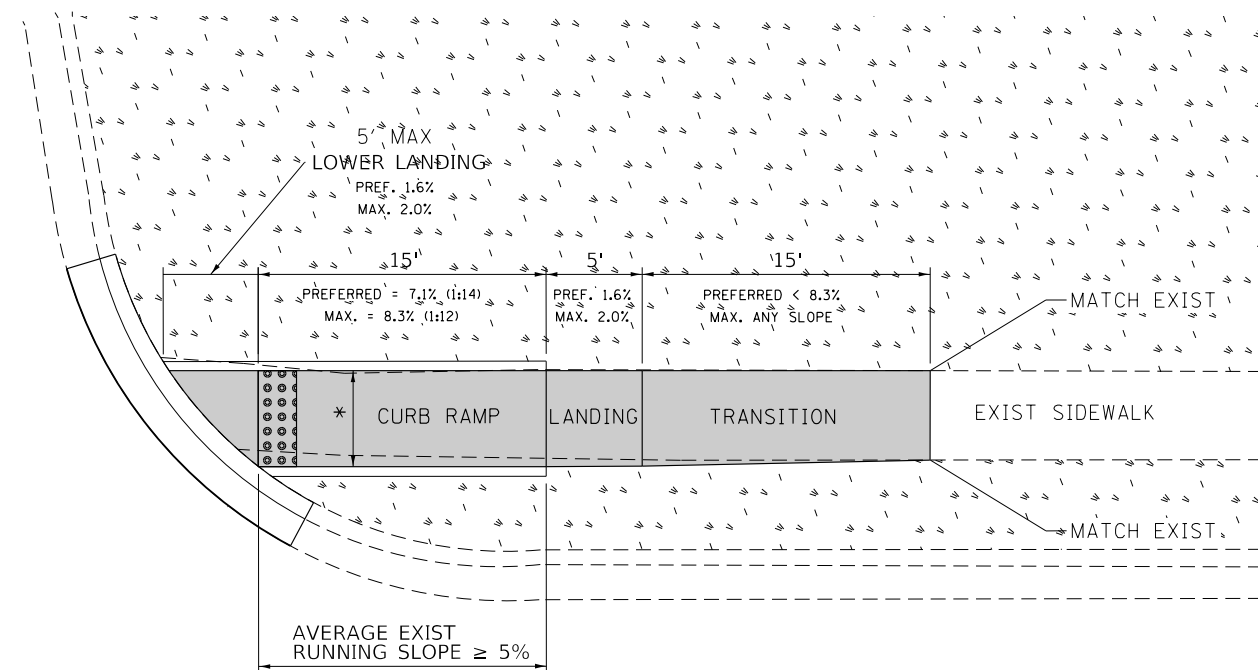
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1319	2020-140-RS&SW	COOK	50	45
PD-01		CONTRACT NO.		
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR GREATER RUN. SLOPE

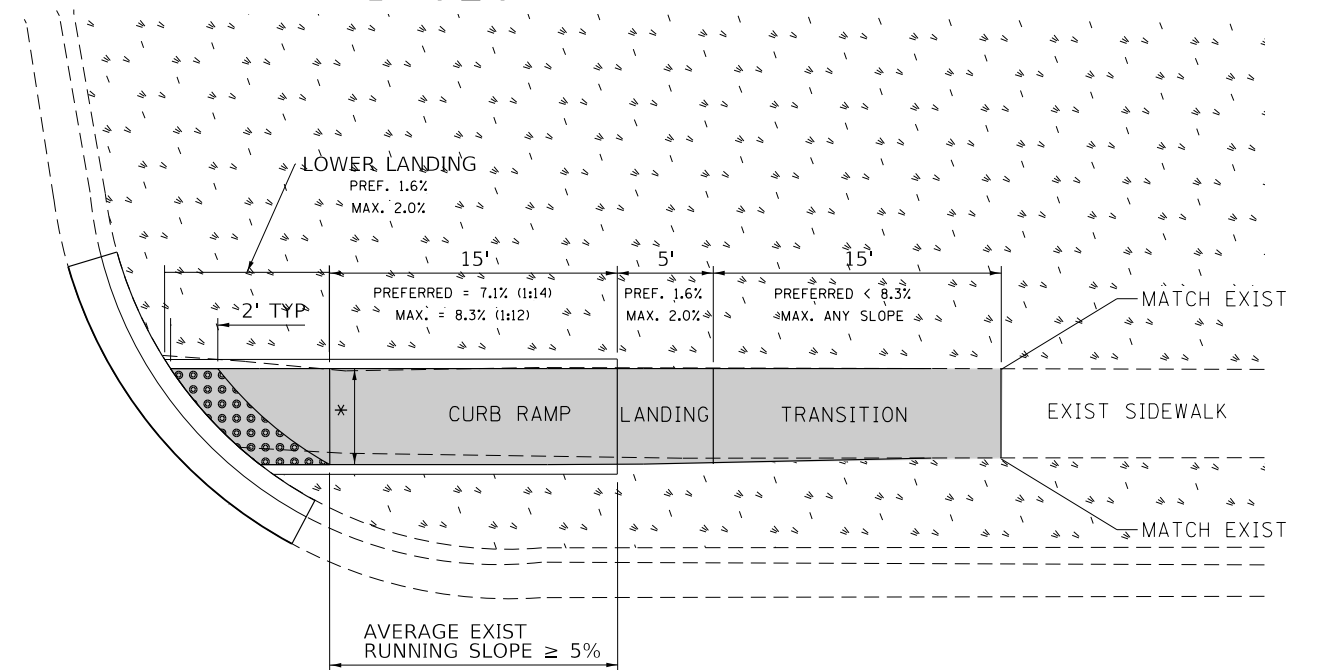
PD-02A



PD-02B



PD-02C



DESIGNER NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

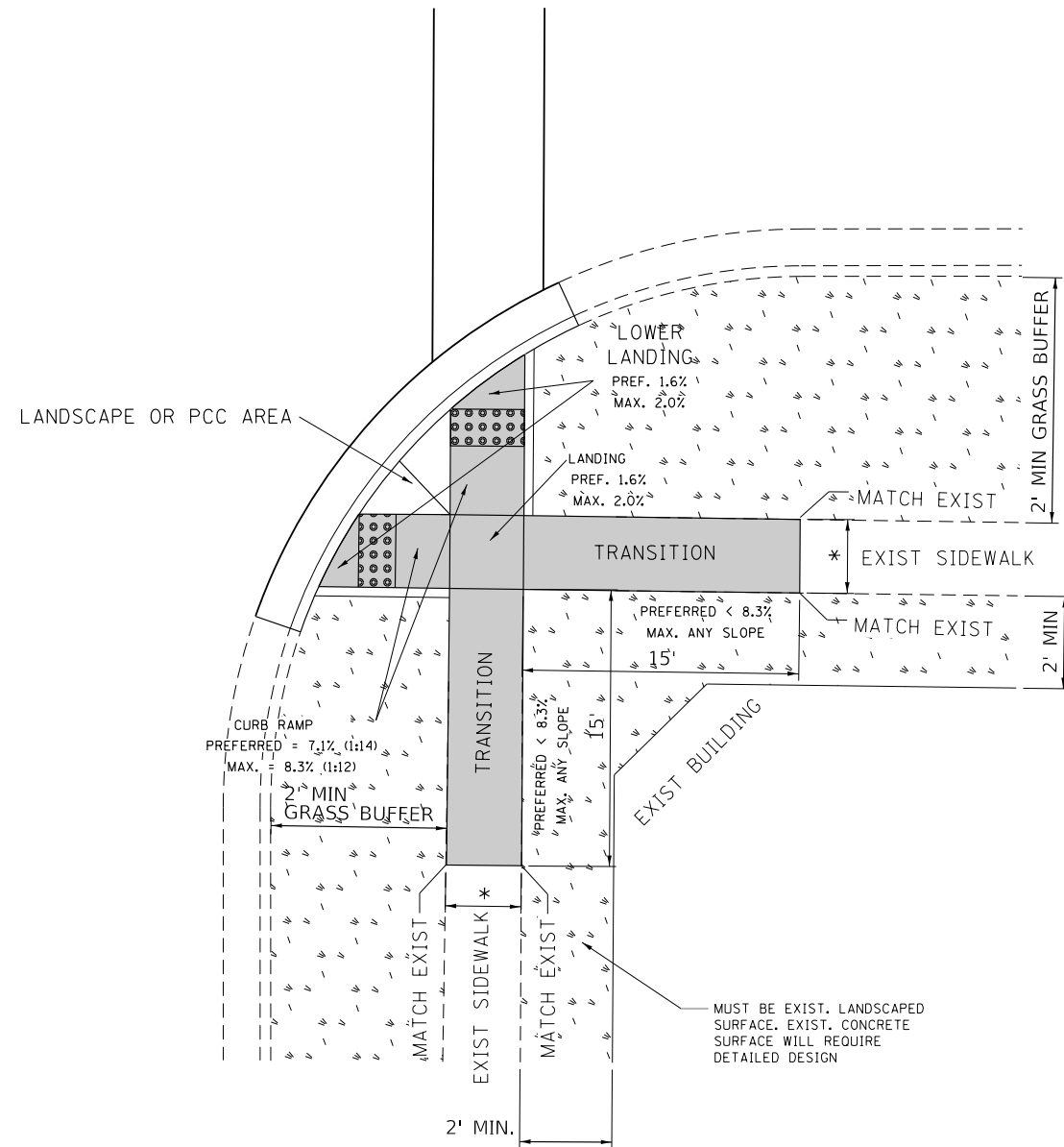
**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
(PD-02)**

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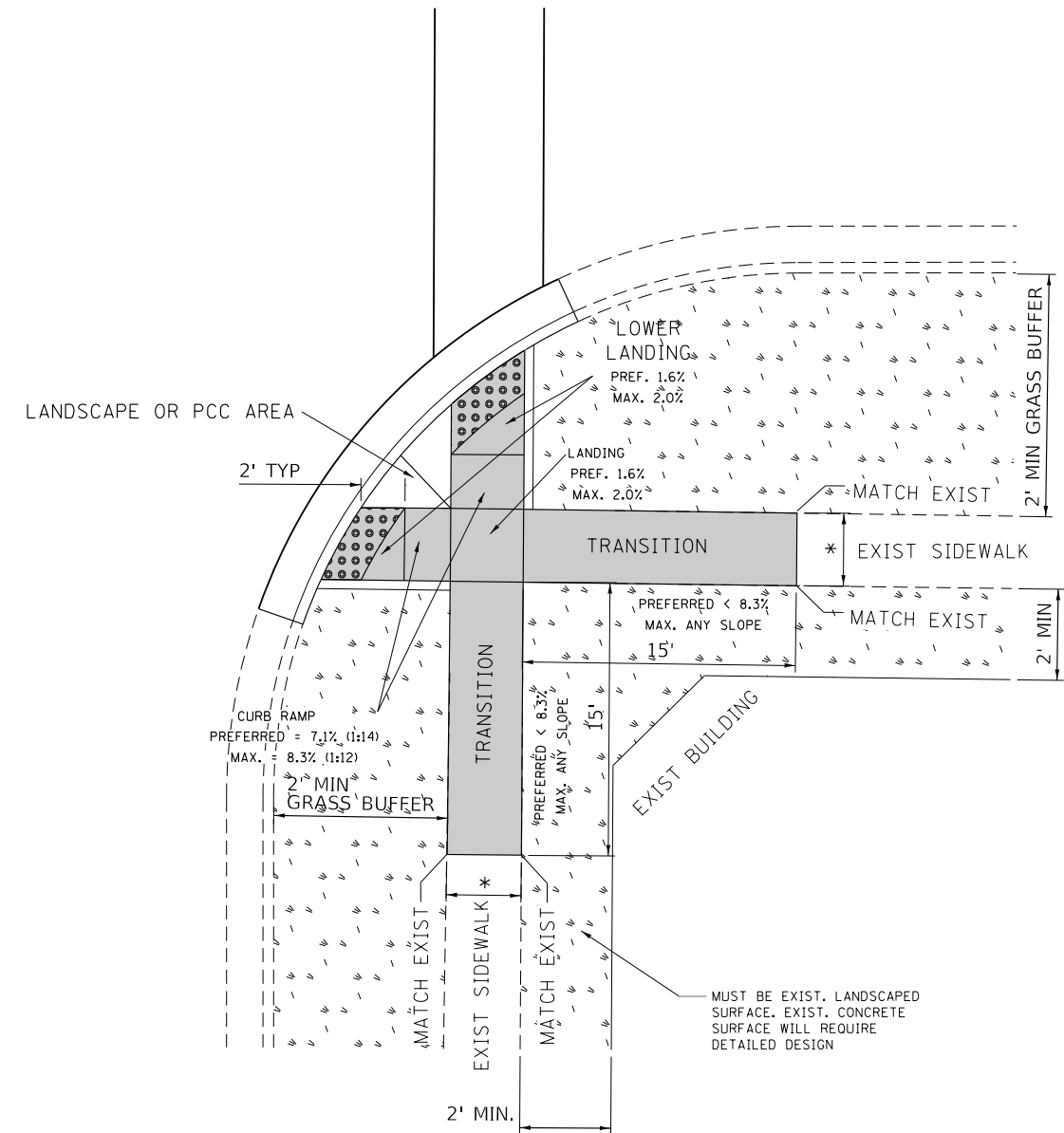
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1319	2020-140-RS&SW	COOK	50	46
PD-02			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS

PD-03A



PD-03B



- DESIGNER NOTES:**
- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
 - 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
 - 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
 - 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

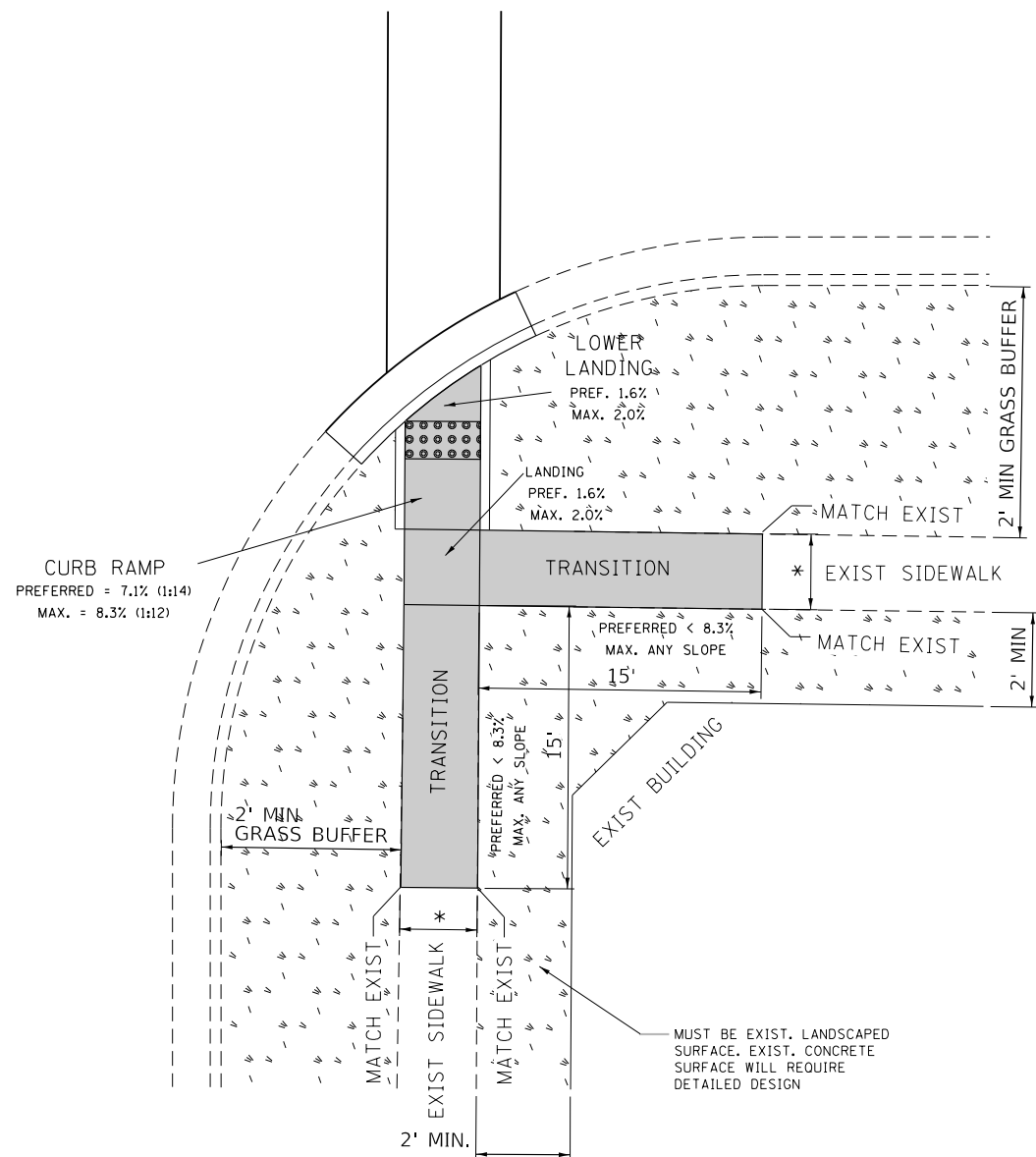
	PROPOSED SIDE CURB		EXIST. GRASS
	PROPOSED SIDEWALK		EXIST. GRASS
	PROPOSED SIDEWALK		EXIST. GRASS
	PROPOSED SIDEWALK		EXIST. GRASS

- CONSTRUCTION NOTES:**
- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

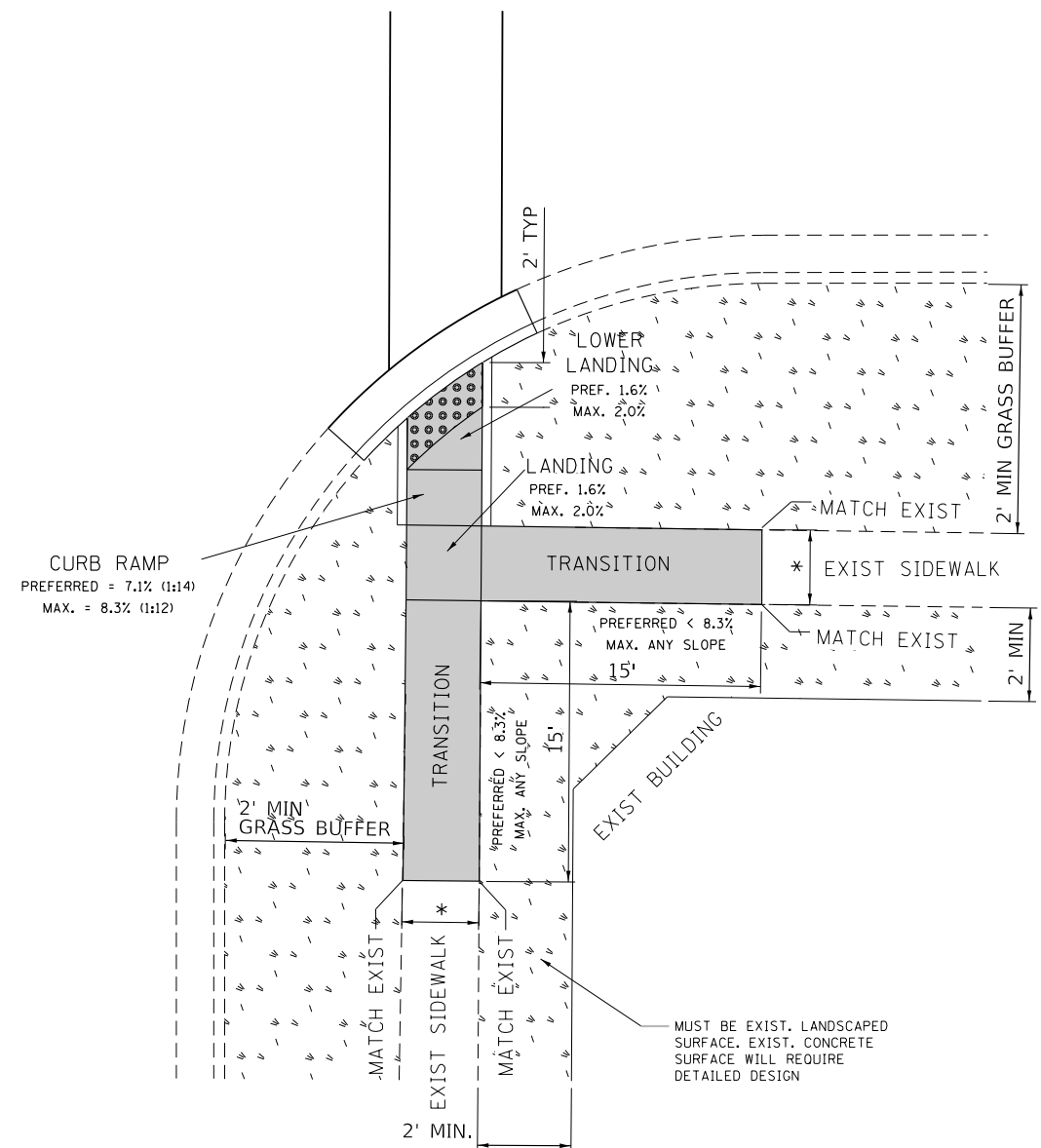
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S:\WP\PLANPREP\SQUAD_1\Des_RL\Typical ADA details\Typical-ADA-sht-pln.dgn		DRAWN - RL 11/12/2019	REVISED -	DEPARTMENT OF TRANSPORTATION	(PD-03)	1319	2020-140-RS&SW	COOK	50	47
Default	PLOT SCALE = 10.0000' / 1"	CHECKED -	REVISED -				PD-03			
	PLOT DATE = 11/12/2019	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
										ILLINOIS FED. AID PROJECT

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ TURNING SPACE

PD-04A



PD-04B



- DESIGNER NOTES:**
- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
 - 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
 - 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
 - 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

	PROPOSED SIDE CURB		EXIST. GRASS
	PROPOSED SIDEWALK		DETECTABLE WARNINGS

- CONSTRUCTION NOTES:**
- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

FILE NAME =	USER NAME = ledezmar	DESIGNED -	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

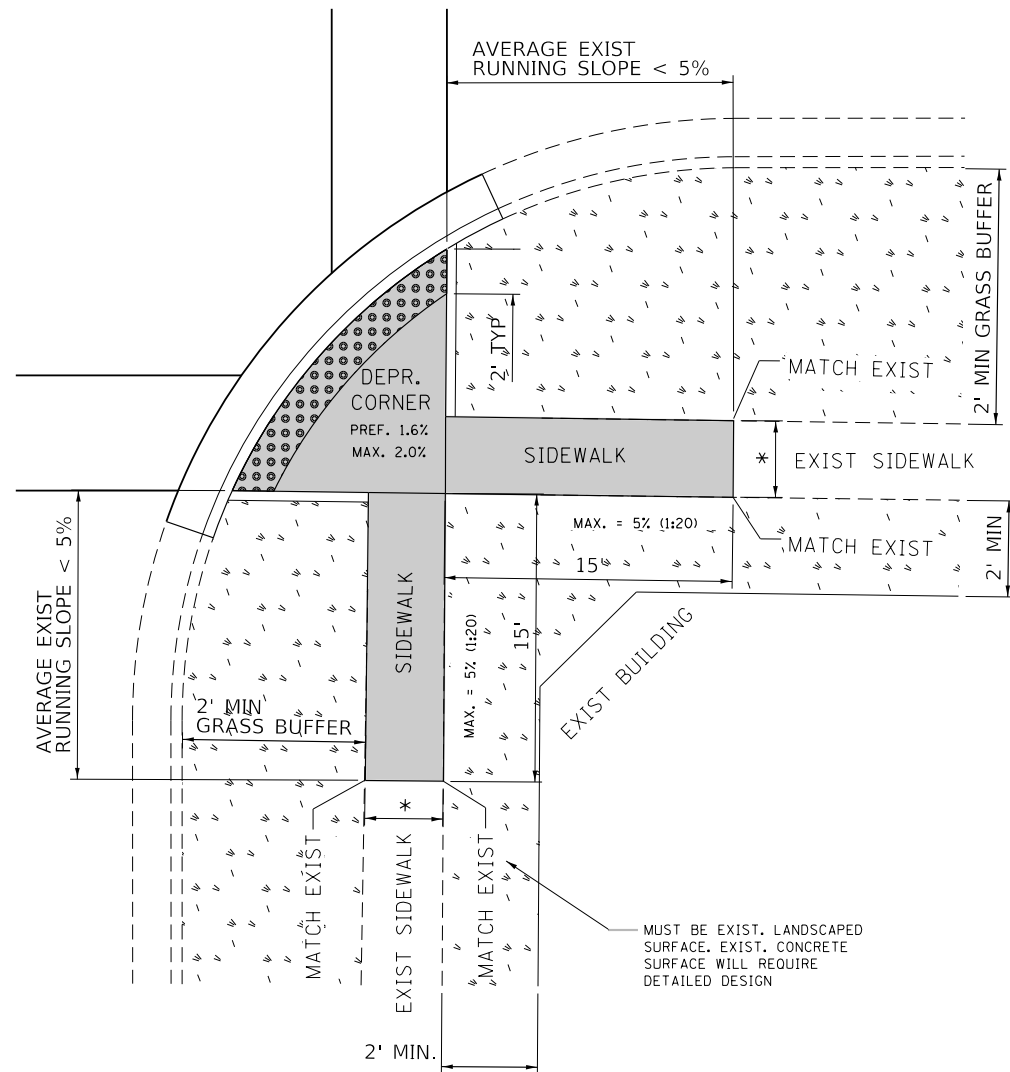
**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
W/TURNING SPACE (PD-04)**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	48
PD-04			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				

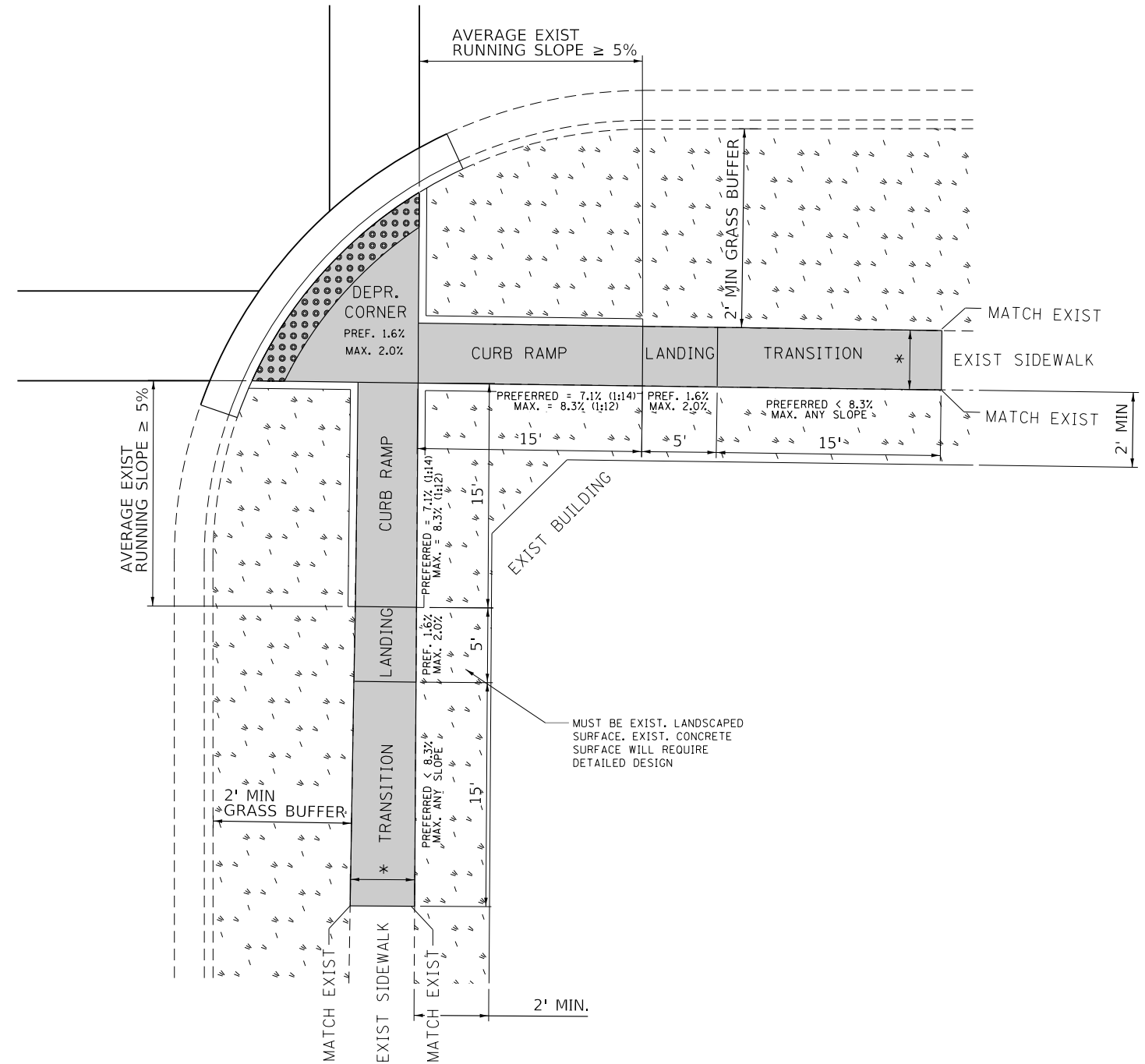
SCALE: SHEET OF SHEETS STA. TO STA.

ADA DETAIL FOR DEPRESSED CORNER CURB RAMPS

PD-05A



PD-05B



- DESIGNER NOTES:**
- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
 - 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
 - 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
 - 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

- CONSTRUCTION NOTES:**
- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

FILE NAME =	USER NAME = ledezmar	DESIGNED -	REVISED -
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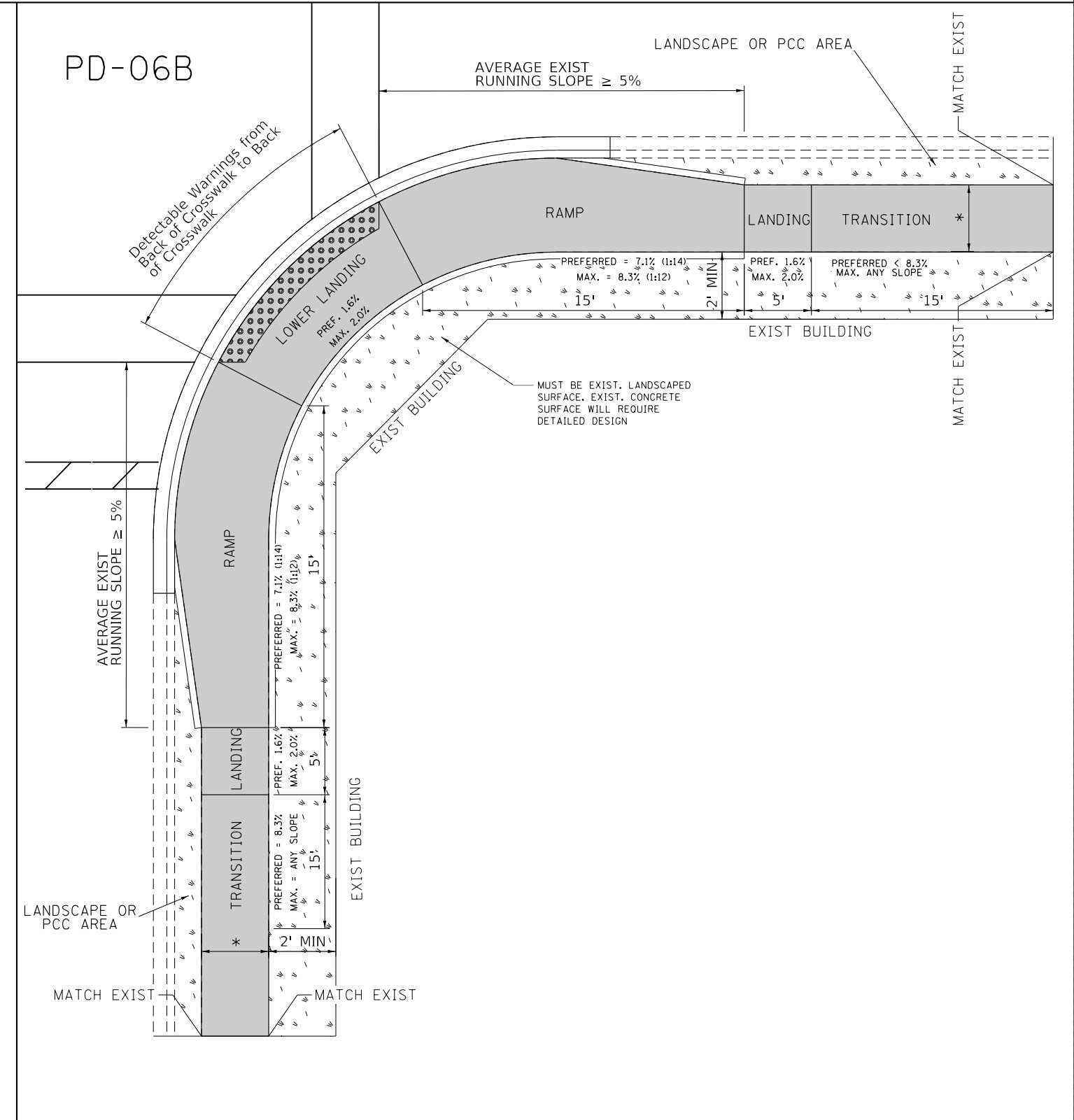
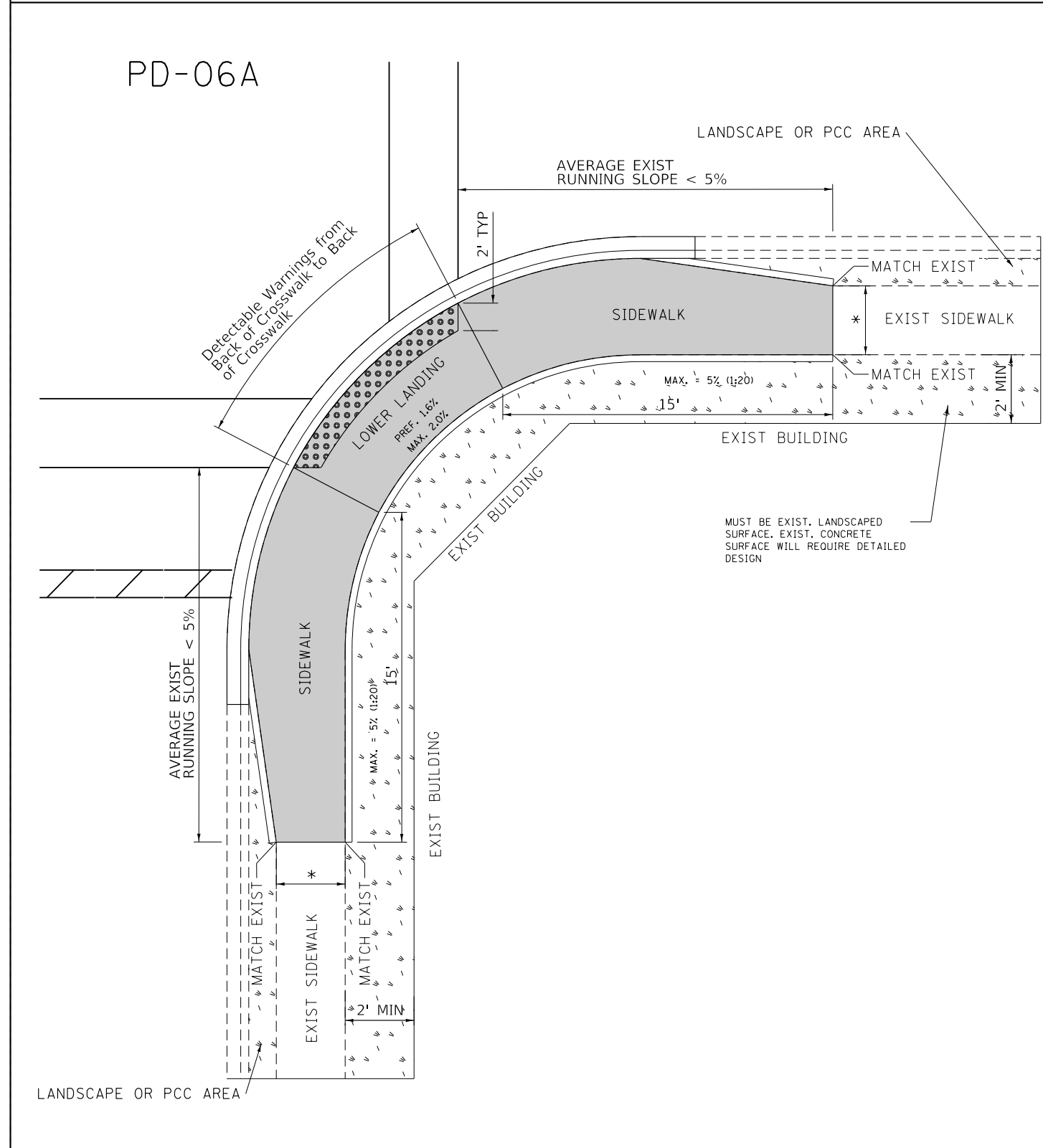
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR DEPRESSED CORNER CURB RAMPS
(PD-05)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	49
PD-05			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR PARALLEL CURB RAMPS ADJACENT TO LANDSCAPING



- DESIGNER NOTES:**
- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
 - 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
 - 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
 - 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

	PROPOSED SIDE CURB		EXIST. GRASS
	PROPOSED SIDEWALK		DETECTABLE WARNINGS

- CONSTRUCTION NOTES:**
- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

FILE NAME =	USER NAME = ledemorm	DESIGNED -	REVISED -
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Default	PLOT SCALE = 10.0000' / 1"	CHECKED -	REVISED -
	PLOT DATE = 11/12/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR PARALLEL CURB RAMPS
(PD-06)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1319	2020-140-RS&SW	COOK	50	50
PD-06		CONTRACT NO.		
ILLINOIS FED. AID PROJECT				