

03-05-2021 LETTING ITEM 097

# STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

## PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU 1710 (123RD STREET)  
76TH AVENUE TO HARLEM AVENUE  
RECONSTRUCTION

SECTION 15-00053-00-PV

PROJECT OM80(991)

CITY OF PALOS HEIGHTS

COOK COUNTY

JOB NO. C-91-423-15

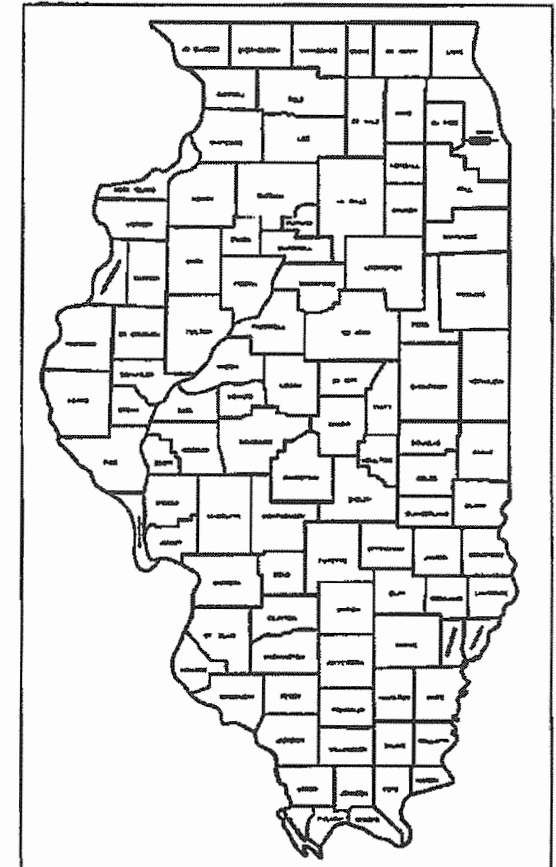
FAU RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	1

P-91-423-15  
D-91-423-15

FOR INDEX OF SHEETS  
SEE SHEET NO. 2  
FOR LIST OF HIGHWAY STANDARDS  
SEE SHEET NO. 2

FAU 1710 (123RD STREET)  
FUNCTIONAL CLASSIFICATION: MINOR COLLECTOR

950 ADT (2016)  
0.7% TRUCKS  
POSTED SPEED LIMIT = 20 MPH



LOCATION OF SECTION INDICATED THUS: —

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

APPROVED 12-22 2020  
*[Signature]*  
SPECIAL CITY OF PALOS HEIGHTS

PASSED 12-30 2020  
*[Signature]*  
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

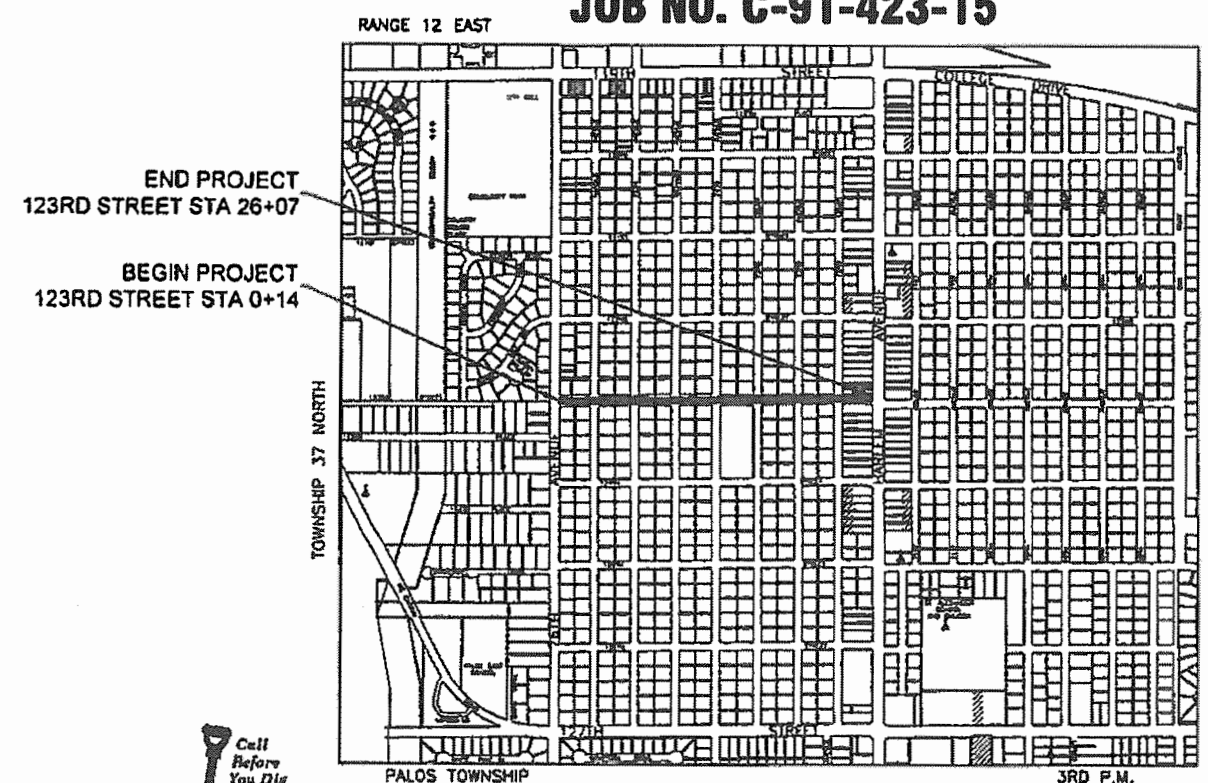
RELEASING FOR BID  
BASED ON LIMITED REVIEW  
December 30, 2020  
*[Signature]*  
REGIONAL CHIEF

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS



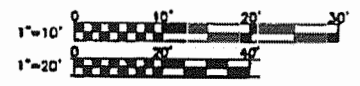
SIGNED 12/22/2020  
EXPIRES 11/30/2021

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E., SCHAUMBURG, IL  
CONSULTANT: MORRIS ENGINEERING, INC. (630) 271-0770



LOCATION MAP

MAP SCALE: N.T.S.  
— DENOTES PROJECT LOCATION  
GROSS AND NET LENGTH OF PROJECT = 2,593 FEET (0.49 MILE)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

SCALES { PLAN 1 INCH = 20 FEET  
CROSS-SECTIONS HORIZ. 1 INCH = 10 FEET  
CROSS-SECTIONS VERT. 1 INCH = 5 FEET



CONTRACT NO. 61G81

**GENERAL NOTES**

**INDEX OF SHEETS**

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- 5) TYPICAL SECTIONS
- 6) SCHEDULE OF QUANTITIES
- 7-8) ALIGNMENT, TIES AND BENCHMARKS
- 9-10) REMOVAL PLAN
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- 30-37) DETAILS
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**LIST OF ILLINOIS DOT HIGHWAY STANDARDS**

000001-08	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
601001-05	PIPE UNDERDRAINS
602001-02	CATCH BASIN TYPE A
602301-04	INLET TYPE A
604036-03	GRATE TYPE 8
701001-02	OFF-ROAD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5M) AWAY
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS – DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-06	SIDEWALK, CORNER, OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

**DISTRICT ONE DETAILS**

BD-01	DRIVEWAY DETAILS – DISTANCE BETWEEN ROW AND FACE, FACE OF CURB & EDGE OF SHOULDER 15' (4.5 M)
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-21	DETOUR SIGNING FOR CLOSING STATE HIGHWAYS
TC-22	ARTERIAL ROAD INFORMATION SIGN
TC-26	DRIVEWAY ENTRANCE SIGNING

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING SAFE AND HEALTHFUL WORKING CONDITIONS IN ACCORDANCE WITH SECTION 107 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION THROUGHOUT THE CONSTRUCTION OF THE PROPOSED IMPROVEMENTS.
2. SOIL EROSION AND SEDIMENTATION CONTROL PRACTICES AND DEVICES SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE URBAN COMMITTEE OF THE ASSOCIATION OF ILLINOIS SOIL AND WATER CONSERVATION DISTRICTS' PROCEDURES AND STANDARDS FOR URBAN SOIL EROSION AND SEDIMENTATION CONTROL IN ILLINOIS AND ALL REVISIONS THERETO AND IN ACCORDANCE WITH THE DETAILS ON THE PLANS.
3. THE CONTRACTOR SHALL BE AWARE OF POTENTIAL CONFLICTS WITH EXISTING UTILITIES AS INDICATED ON THE PLANS. THE CONTRACTOR SHALL EXCAVATE AROUND UTILITIES TO DETERMINE ELEVATIONS BEFORE BEGINNING CONSTRUCTION.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING EACH OF THE UTILITY COMPANIES BEFORE ANY WORK COMMENCES. ALL UTILITIES SHALL BE STAKED PRIOR TO CONSTRUCTION.
5. THE OWNER WILL FURNISH THE CONTRACTOR WITH LINES, GRADES AND ELEVATIONS NECESSARY TO THE PROPER EXECUTION AND CONTROL OF THE WORK ONCE.
6. THE CONTRACTOR SHALL INFORM THE ENGINEER, CITY OF PALOS HEIGHTS AT LEAST FORTY-EIGHT (48) HOURS PRIOR TO COMMENCEMENT OF CONSTRUCTION.  
  
TELEPHONE NUMBERS: ENGINEER - (630) 271-0770  
PALOS HEIGHTS - (708) 361-1806
7. WHERE SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER, EXISTING DRAINAGE STRUCTURES AND SYSTEMS SHALL BE CLEANED OF DEBRIS AND PATCHED AS NECESSARY TO ASSURE INTEGRITY OF THE STRUCTURE. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR STRUCTURES AND CONTRACT UNIT PRICE PER LINEAL FOOT FOR SYSTEMS WHICH SHALL BE PAYMENT IN FULL FOR CLEANING, PATCHING, REMOVAL AND DISPOSAL OF DEBRIS AND DIRT. DRAINAGE STRUCTURES AND SYSTEMS CONSTRUCTED AS PART OF THIS PROJECT SHALL BE MAINTAINED BY THE CONTRACTOR AT HIS EXPENSE UNTIL FINAL ACCEPTANCE BY THE OWNER OR COUNTY.
8. THE CONTRACTOR SHALL KEEP PUBLIC STREET PAVEMENTS CLEAN OF DIRT AND DEBRIS AND, WHEN NECESSARY, SHALL ON A DAILY BASIS CLEAN THE PAVEMENT OF SUCH DIRT AND DEBRIS RESULTING FROM CONSTRUCTION OPERATIONS.
9. THE CONTRACTOR SHALL PROVIDE PIPE BEDDING IN ACCORDANCE WITH THE DETAIL ON THE PLANS. THE COST OF THE BEDDING SHALL BE INCLUDED IN THE UNIT PRICE PER LINEAL FOOT OF THE VARIOUS SIZES OF STORM SEWER.
10. THE CONTRACTOR SHALL PLACE TOPSOIL AT A 4" MINIMUM DEPTH AND SEED OR SOD ALL AREAS DESIGNATED BY THE ENGINEER.
11. THE CONTRACTOR SHALL EXAMINE THE DRAINAGE PATTERNS SHOWN ON THE PLANS AND MAKE CERTAIN THAT ALL OVERFLOW POINT ELEVATIONS AND CROSS SECTIONS ARE CONSTRUCTED STRICTLY IN ACCORDANCE WITH THOSE SHOWN ON THE PLANS.
12. ALL CONSTRUCTION SHALL CONFORM WITH THE PERMIT PLANS AND REVISIONS THERETO APPROVED BY COOK COUNTY.
13. THE CONTRACTOR SHALL CONTACT J.U.L.I.E. (1-800-892-0123) PRIOR TO ANY WORK IN THE RIGHT OF WAY OR EASEMENTS TO LOCATE UTILITIES, AND CONTACT THE OWNER'S REPRESENTATIVE SHOULD PUBLIC UTILITIES APPEAR TO BE IN CONFLICT WITH THE PROPOSED IMPROVEMENTS.
14. ALL WORK HEREIN PROPOSED SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE STATE OF ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, COOK COUNTY PUBLIC WORKS DEPARTMENT, THE MUNICIPALITY, AND ALL PERTINENT LAWS, DIRECTIVES, ORDINANCES AND THE LIKE SHALL BE CONSIDERED TO BE A PART OF THESE SPECIFICATIONS.
15. THE APPROXIMATE LOCATION OF KNOWN PUBLIC UTILITIES ARE SHOWN ON THE PLANS. HOWEVER, THE ENGINEER DOES NOT GUARANTEE ITS ACCURACY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING EACH OF THE UTILITY COMPANIES BEFORE ANY WORK COMMENCES.
16. ALL PROPOSED ELEVATIONS SHOWN ARE RELATIVE TO U.S.G.S. DATUM NAVD88.

17. THE CONTRACTOR SHALL TAKE CARE TO PROTECT ALL SIGNS ALONG THE ROUTE OF CONSTRUCTION. SIGNS SHALL BE REMOVED IF THEY ARE IN CONFLICT WITH PROPOSED WORK, AND APPROVED BY ENGINEER. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY EXISTING DAMAGE TO A SIGN PRIOR TO REMOVAL. THE CONTRACTOR SHALL REPLACE ALL SIGNS AND POSTS DAMAGED AFTER REMOVAL.
18. DIRT ON PAVEMENT - WHERE A CONTRACTOR'S EQUIPMENT IS OPERATED UPON AN EXISTING PAVEMENT USED BY TRAFFIC THE CONTRACTOR SHALL CLEAN THE PAVEMENT OF ALL DIRT AND DEBRIS AT THE END OF EACH DAY'S OPERATIONS AND AT OTHER TIMES AS DIRECTED BY THE OWNER, THE ENGINEER, OR THE GOVERNING MUNICIPALITY.
19. GEOTECHNICAL FABRIC FOR GROUND STABILIZATION AND/OR AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAVE BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ABOVE ITEM WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED, AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
20. ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTOR'S VEHICLES AND/OR EQUIPMENTS IS TO BE REMOVED AND REPLACED AS DIRECT BY THE ENGINEER AT CONTRACTOR EXPENSE.
21. PIPE UNDERDRAINS SHALL BE INSTALLED ACCORDING TO SECTION 601 OF THE SSRBC AND STANDARD 601001-05. TOP OF PIPE UNDERDRAINS SHALL BE PLACED MINIMUM 6" BELOW THE AGGREGATE SUBGRADE IMPROVEMENT LAYER. THE COST OF MAKING PIPE UNDERDRAINS CONNECTIONS TO DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE COST OF THE PIPE UNDERDRAINS.
22. BACKFILLING STORM SEWER CONSTRUCTED UNDER THE ROADWAY SPECIFIED UNDER ART. 550.07(b, c) OF THE SSRBC WILL NOT BE ALLOWED.

**COMMITMENTS**

NO COMMITMENTS

PLOT DATE: Jan 26, 2021  
FILENAME: H:\20-PA\3005 - 123rd Street Reconstruction - Phase I\Plans-Eng\Rev-A\20-PA-3005-TITLE-NOTES-DETAILS-2021-01-25.dwg

USER NAME =	DESIGNED - AS	REVISED -
	DRAWN - DJB	REVISED -
PLOT SCALE =	CHECKED - AS	REVISED -
PLOT DATE = Jan 26, 2021	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, STANDARDS LIST,  
GENERAL NOTES, & COMMITMENTS**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	2
			CONTRACT NO. 61G81	
ILLINOIS FED. AID PROJECT				

SCALE: — SHEET NO. 1 OF 2 SHEETS STA. TO STA.

**WATERMAIN NOTES**

1. ALL SANITARY SEWER AND WATERMAIN WORK SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE APPLICABLE SECTIONS OF THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS, LATEST EDITION. ENGLISH UNITS SHALL BE USED.
2. THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES PRIOR TO CONSTRUCTION TO VERIFY IN THE FIELD THE LOCATIONS OF ALL EXISTING AND UNDERGROUND UTILITIES ADJACENT TO THE PROJECT, AND BE RESPONSIBLE FOR PROTECTION OF SAME. THE CONTRACTOR SHALL ALSO NOTIFY THE OWNER AND MORRIS ENGINEERING, INC. AT LEAST 48 HOURS PRIOR TO START OF CONSTRUCTION.
3. THE CONTRACTOR SHALL BEWARE OF POTENTIAL CONFLICTS WITH EXISTING UTILITIES AS INDICATED ON THE PLANS. THE CONTRACTOR SHALL EXCAVATE AROUND UTILITIES TO DETERMINE LOCATIONS AND ELEVATIONS OF ALL POTENTIALLY CONFLICTING UTILITIES BEFORE BEGINNING CONSTRUCTION.
4. ALL EXISTING FIELD DRAINAGE TILES ENCOUNTERED OR DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO THEIR ORIGINAL CONDITION, PROPERLY REROUTED AND/OR CONNECTED TO THE STORM SYSTEM. ALL LOCATIONS OF ENCOUNTERED FIELD DRAINAGE TILE SHALL BE PROPERLY INDICATED ON THE RECORD SET OF DRAWINGS.
5. THE CONTRACTOR RESPONSIBLE FOR SANITARY AND WATERMAIN IMPROVEMENTS (UNDERGROUND STRUCTURES AND CONDUITS) SHALL DISPOSE OF ALL SURPLUS EXCAVATED MATERIAL FROM TRENCHES OR STRUCTURE EXCAVATIONS AND SHALL DEPOSIT SAID SURPLUS MATERIALS ON THE SITE IN ACCORDANCE WITH THE GRADING PLAN OR AS DIRECTED BY THE ENGINEER.
6. ALL EXISTING UTILITIES OR IMPROVEMENTS, INCLUDING WALKS, CURBS, PAVEMENT AND PARKWAYS DAMAGED OR REMOVED DURING CONSTRUCTION SHALL BE PROMPTLY RESTORED TO THEIR RESPECTIVE ORIGINAL CONDITION.
7. ANY EXISTING UTILITY STRUCTURES REQUIRING ADJUSTMENT SHALL BE ADJUSTED (UP TO 8" TOTAL ADJUSTMENT) OR RECONSTRUCTED BY THE CONTRACTOR.
8. SANITARY SEWERS AND WATERMAIN SHALL HAVE A MINIMUM OF 5.5 FEET OF COVER AND SHALL BE RUN IN STRAIGHT ALIGNMENT BETWEEN STRUCTURES UNLESS SPECIFICALLY SHOWN OTHERWISE ON THE PLANS.
9. ALL WATERMAIN SHALL BE CEMENT LINED DUCTILE IRON PIPE, CLASS 52 CONFORMING TO AWWA C-150 WITH PUSH-ON OR MECHANICAL JOINTS.
10. VALVE VAULT FRAMES AND LIDS SHALL BE NEENAH R1772-C OR APPROVED EQUAL WITH LIDS IMPRINTED "WATER".
11. POLYETHYLENE FILM WRAP FOR WATERMAIN IN ACCORDANCE WITH AWWA C-105-82 SHALL BE PLACED OVER ALL WATER MAIN SUCH THAT THE WATER MAIN SHALL NOT BE IN DIRECT CONTACT WITH THE TRENCH SOIL. THIS ITEM WILL BE INCIDENTAL TO THE WATERMAIN.
12. ALL WATER MAINS SHALL BE PRESSURE TESTED AND DISINFECTED IN ACCORDANCE WITH STANDARDS AND PROCEDURES MEETING THE APPROVAL OF THE CITY OF PALOS HEIGHTS. THE COST OF TESTING AND DISINFECTION SHALL BE INCLUDED IN THE COST OF WATERMAIN INSTALLATION.
13. WATERMAIN DISINFECTIONS SHALL BE PERFORMED BY FLUSHING, THEN CHLORINATING AT A RATE OF 50 PPM TO INSURE A RESIDUAL OF 25 PPM AFTER 24 HOURS. THEN AFTER A SECOND FLUSHING A MINIMUM OF TWO WATER SAMPLES ON TWO CONSECUTIVE DAYS 24 HOURS APART SHALL BE TAKEN TO AN APPROVED TESTING SERVICE.

14. THE CONTRACTOR SHALL PROVIDE A SAMPLING TAP IF REQUIRED AT NO EXTRA COST.
15. FIRE HYDRANTS SHALL BE OF THE TYPE SHOWN ON THE WATER DETAIL SHEET OF THE PLANS.
16. ALL WATER SERVICE LINES SHALL BE CLASS "K" COPPER OR DUCTILE IRON PIPE (CLASS 52) AWWA-C600 WITH "PUSH ON" JOINTS, CEMENT LINED IN ACCORDANCE WITH ASA A21.04.
17. CONNECTIONS TO EXISTING SANITARY SEWER AND WATER SYSTEMS SHALL BE PAID AT THE CONTRACT UNIT PRICE.
18. GRANULAR CRADLE CONFORMING TO THE DETAIL SHOWN ON THE DETAIL SHEET OF THE PLANS SHALL BE PLACED IN ANY UTILITY TRENCH OVER WHICH ANOTHER UTILITY PASSES.
19. COMPACTED GRANULAR TRENCH BACKFILL SHALL BE USED IN ALL LOCATIONS WHERE THE PROPOSED UNDERGROUND UTILITY IS TO BE CONSTRUCTED UNDER OR WITHIN TWO (2) FEET OF PERMANENT TYPE PAVEMENTS, DRIVEWAYS, OR SIDEWALKS OR IN LOCATIONS AS DIRECTED BY THE ENGINEER. TRENCH BACKFILL SHALL BE EXTENDED TWO (2) FEET ON EACH SIDE OF THE PERMANENT TYPE SURFACE.
20. AT THE COMPLETION OF THIS PROJECT ONE SET OF RECORD PLANS IS TO BE SUBMITTED TO THE ENGINEER SHOWING THE LOCATION OF ALL OF THE SERVICES, PIPES, STRUCTURES, GRADING AND UTILITIES.
21. ALL SERVICES SHALL BE MARKED AT THE PROPERTY LINE USING A 2x2 PAINTED BLUE FOR WATER AND GREEN FOR SANITARY.

**PROTECTION OF WATER MAIN & WATER MAIN SERVICE LINES**

WATER MAINS AND WATER SERVICE LINES SHALL BE PROTECTED FROM SANITARY SEWERS, STORM SEWERS, COMBINED SEWERS, HOUSE SEWER SERVICE CONNECTIONS AND DRAINS AS FOLLOWS:

- a) WATER MAINS:
    - 1) HORIZONTAL SEPARATION:
      - A) WATER MAINS SHALL BE LAID AT LEAST TEN FEET HORIZONTALLY FROM ANY EXISTING OR PROPOSED DRAIN, STORM SEWER, SANITARY SEWER, COMBINED SEWER OR SEWER SERVICE CONNECTION.
- WATER MAINS MAY BE LAID CLOSER THAN TEN FEET TO A SEWER LINE WHEN:
- B)
    - i) LOCAL CONDITIONS PREVENT A LATERAL SEPARATION OF TEN FEET;
    - ii) THE WATER MAIN INVERT IS AT LEAST 18 INCHES ABOVE THE CROWN OF THE SEWER; AND
    - iii) THE WATER MAIN IS EITHER IN A SEPARATE TRENCH OR IN THE SAME TRENCH ON AN UNDISTURBED EARTH SHELF LOCATED TO ONE SIDE OF THE SEWER.
  - C) BOTH THE WATER MAIN AND DRAIN OR SEWER SHALL BE CONSTRUCTED OF SLIP-ON OR MECHANICAL JOINT CAST OR DUCTILE IRON PIPE, ASBESTOS-CEMENT PRESSURE PIPE, PRESTRESSED CONCRETE PIPE, OR PVC PIPE MEETING THE REQUIREMENTS OF SECTION 41 OF THE STANDARD SPECIFICATION FOR WATER & SEWER MAIN CONSTRUCTION IN ILLINOIS LATEST EDITION, WHEN IT IS IMPOSSIBLE TO MEET (A) OR (B) ABOVE. THE DRAIN OR SEWER SHALL BE PRESSURE TESTED TO THE MAXIMUM EXPECTED SURCHARGE HEAD BEFORE BACKFILLING.

- 2) VERTICAL SEPARATION:
  - A) A WATER MAIN SHALL BE LAID SO THAT ITS INVERT IS 18 INCHES ABOVE THE CROWN OF THE DRAIN OR SEWER WHENEVER WATER MAINS CROSS STORM SEWERS, SANITARY SEWERS OR SEWER SERVICE CONNECTIONS. THE VERTICAL SEPARATION SHALL BE MAINTAINED FOR THAT PORTION OF THE WATER MAIN LOCATED WITHIN TEN FEET HORIZONTALLY OF ANY SEWER OR DRAIN CROSSED. A LENGTH OF WATER MAIN PIPE SHALL BE CENTERED OVER THE SEWER TO BE CROSSED WITH JOINTS EQUIDISTANT FROM THE SEWER OR DRAIN.
  - B) BOTH THE WATER MAIN AND SEWER SHALL BE CONSTRUCTED OF SLIP-ON OR MECHANICAL JOINT CAST OR DUCTILE IRON PIPE, ASBESTOS-CEMENT PRESSURE PIPE, REINFORCED CONCRETE PRESSURE PIPE (AWWA C303), OR PVC PIPE MEETING REQUIREMENTS OF SECTION 41 OF THE STANDARD SPECIFICATION FOR WATER & SEWER MAIN CONSTRUCTION IN ILLINOIS LATEST EDITION, WHEN:
    - i) IT IS IMPOSSIBLE TO OBTAIN THE PROPER VERTICAL SEPARATION AS DESCRIBED IN (1) ABOVE; OR
    - ii) THE WATER MAIN PASSED UNDER A SEWER OR DRAIN.
  - C) A VERTICAL SEPARATION OF 18 INCHES BETWEEN THE INVERT OF THE SEWER OR DRAIN AND THE CROWN OF THE WATER MAIN SHALL BE MAINTAINED WHERE A WATER MAIN CROSSES UNDER A SEWER. SUPPORT THE SEWER OR DRAIN LINES TO PREVENT SETTLING AND BREAKING THE WATER MAIN.
  - D) CONSTRUCTION SHALL EXTEND ON EACH SIDE OF THE CROSSING UNTIL THE NORMAL DISTANCE FROM THE WATER MAIN TO THE SEWER OR DRAIN LINE IS AT LEAST TEN FEET.

- b) WATER SERVICE LINES:
  - 1) THE HORIZONTAL AND VERTICAL SEPARATION BETWEEN WATER SERVICES LINES AND ALL STORM SEWERS, SANITARY SEWERS, COMBINED SEWERS OR ANY DRAIN OR SEWER SERVICE CONNECTION SHALL BE THE SAME AS WATER MAIN SEPARATION DESCRIBED IN (1) ABOVE.
  - 2) WATER PIPE DESCRIBED IN (1) ABOVE SHALL BE USED FOR SEWER SERVICE LINES WHEN MINIMUM HORIZONTAL AND VERTICAL SEPARATION CANNOT BE MAINTAINED.
- c) WATER MAINS SHALL BE SEPARATED FROM SEPTIC TANKS, DISPOSAL FIELD AND SEEPAGE BEDS BY A MINIMUM OF 25 FEET.
- d) WATER MAINS AND WATER SERVICE LINES SHALL BE PROTECTED AGAINST ENTRANCE OF HYDROCARBONS THROUGH DIFFUSION THROUGH ANY MATERIAL USED IN CONSTRUCTION OF THE LINE.

MEMBERS OF J.U.L.I.E. KNOWN TO BE WITHIN THE LIMITS OF THE IMPROVEMENTS ARE:

- AT&T
- COMED
- COMCAST
- NICOR
- CITY OF PALOS HEIGHTS

PLOT DATE: Jan 26, 2021  
FILENAME: H:\20-PA\3005 - 123rd Street Reconstruction - Phase II\Plans-Eng\Rev-A\20-PH-3005-TITLE-NOTES-DETAILS-2021-01-25.dwg

USER NAME =	DESIGNED - AS	REVISED -
	DRAWN - DJB	REVISED -
PLOT SCALE =	CHECKED - AS	REVISED -
PLOT DATE = Jan 26, 2021	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STANDARDS LIST AND GENERAL NOTES**

SCALE: — SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	3
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	



SUMMARY OF QUANTITIES				ROADWAY RECONSTRUCTION CODE 0004	TRAINEES CODE 0042
80% FEDERAL / 20% LOCAL					
STU					
CODE NO	ITEM	UNIT	TOTAL	SUBTOTAL	SUBTOTAL
20101100	TREE TRUNK PROTECTION	EACH	43	43	
Δ 20101200	TREE ROOT PRUNING	EACH	13	13	
20200100	EARTH EXCAVATION	CU YD	3925	3925	
* 20800150	TRENCH BACKFILL	CU YD	414	414	
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	1947	1947	
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	12137	12137	
21400100	GRADING AND SHAPING DITCHES	FOOT	3880	3880	
Δ 25200110	SODDING, SALT TOLERANT	SQ YD	12137	12137	
25200200	SUPPLEMENTAL WATERING	UNIT	10	10	
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	379	379	
28000305	TEMPORARY DITCH CHECKS	FOOT	230	230	
28000400	PERIMETER EROSION BARRIER	FOOT	3493	3493	
28000510	INLET FILTERS	EACH	31	31	
28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	12137	12137	
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	651	651	
* 30300112	AGGREGATE SUBGRADE IMPROVEMENTS 12"	SQ YD	7780	7780	
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	305	305	
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	500	500	
40300200	BITUMINOUS MATERIALS (PRIME COAT)	TON	0.13	0.13	
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	3114	3114	
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	2696	2696	
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	1041	1041	
42300100	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 5 INCH	SQ YD	298	298	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2735	2735	
42400800	DETECTABLE WARNINGS	SQ FT	40	40	
* 44000100	PAVEMENT REMOVAL	SQ YD	7918	7918	
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1500	1500	
* 48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	975	975	
50105220	PIPE CULVERT REMOVAL	FOOT	962	962	
54213657	PRECAST REINFORCED CONCRETE FLARED-END SECTION 12"	EACH	16	16	
* 550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	732	732	
* 550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	1003	1003	

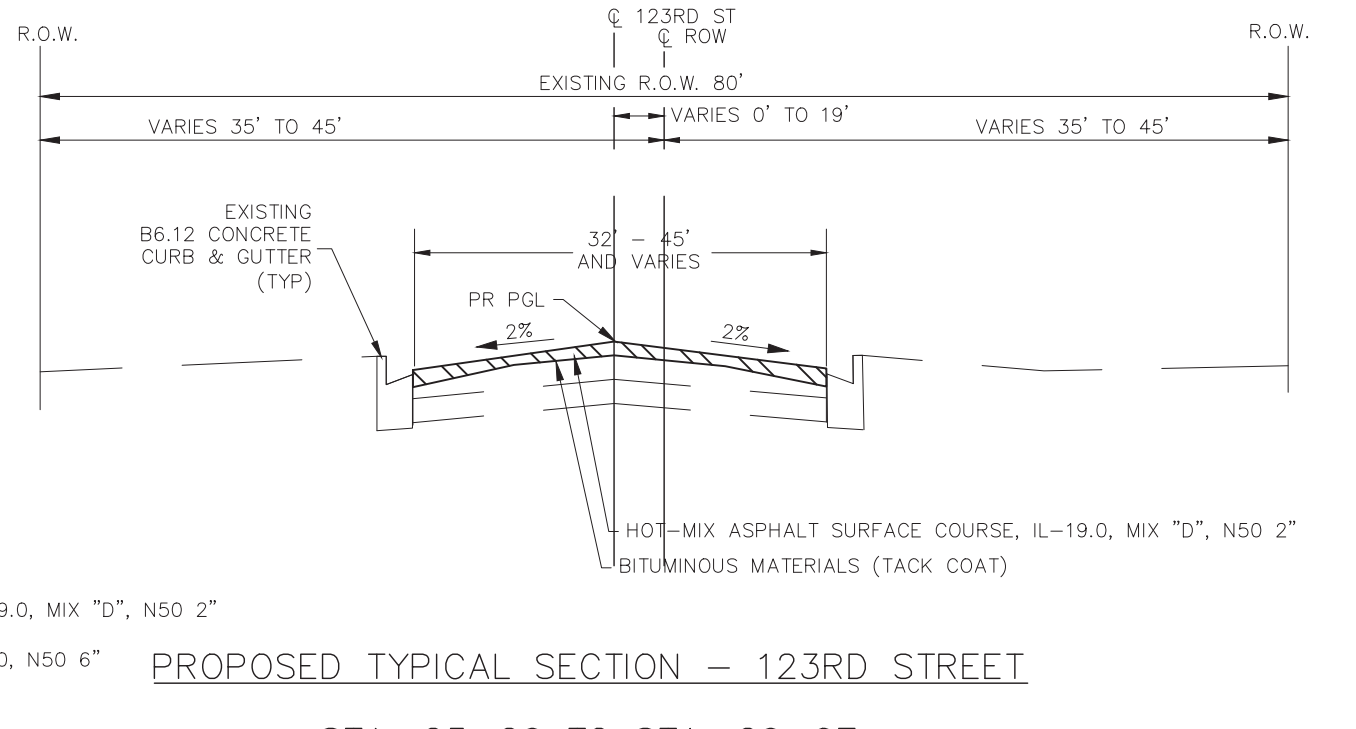
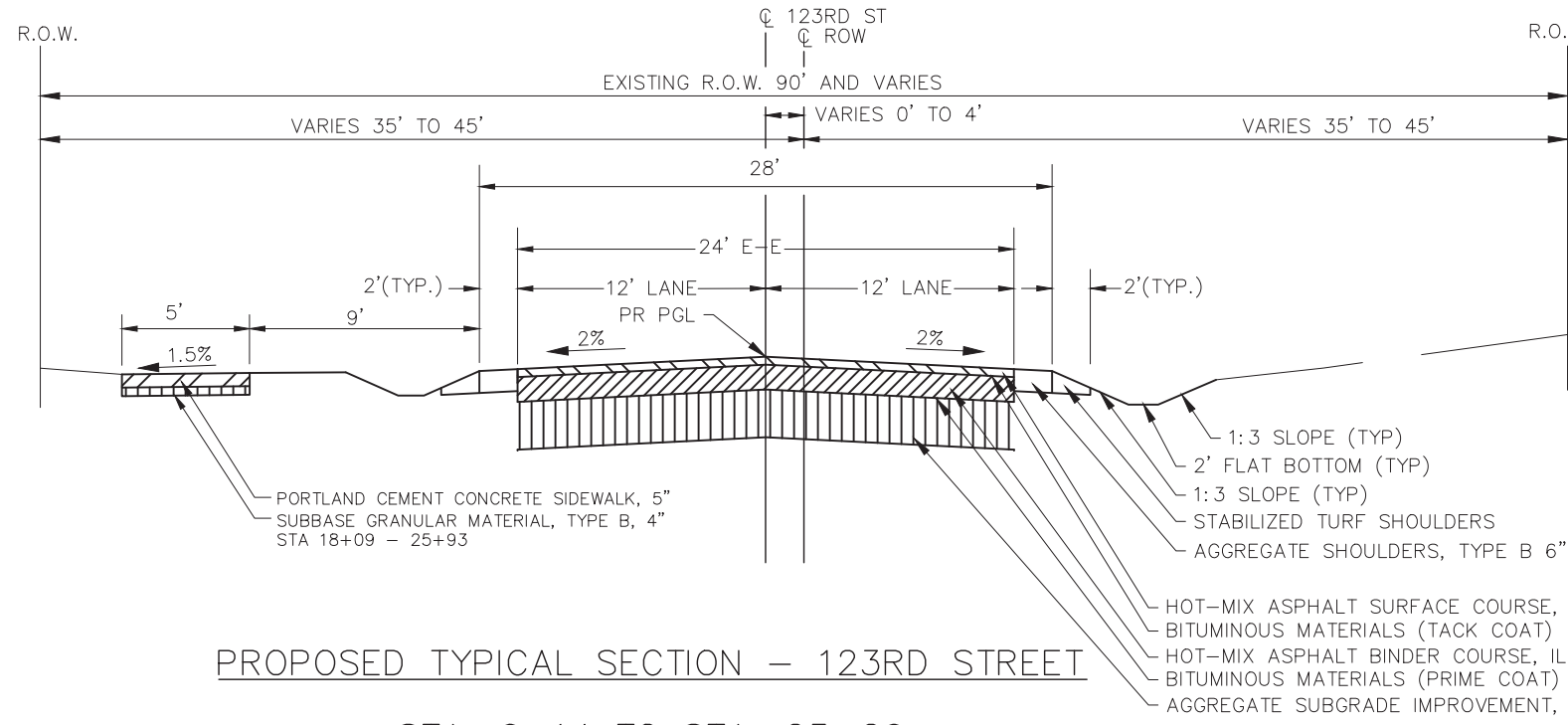
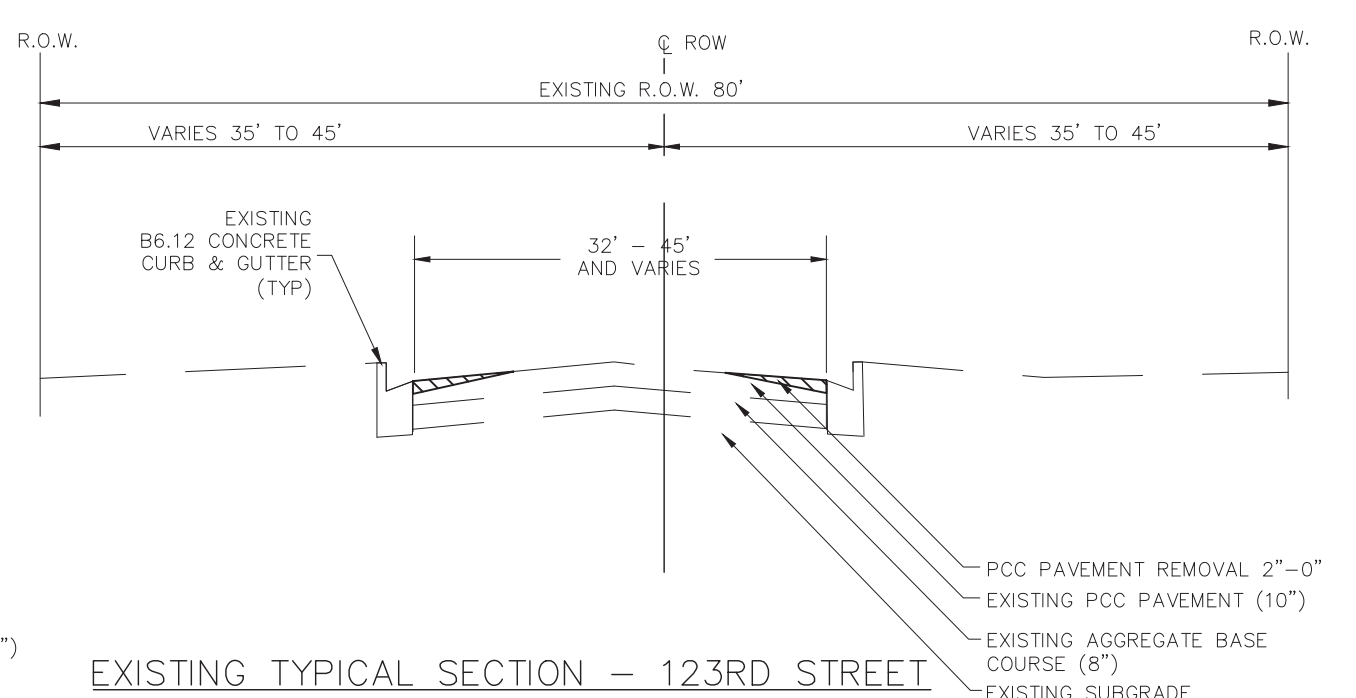
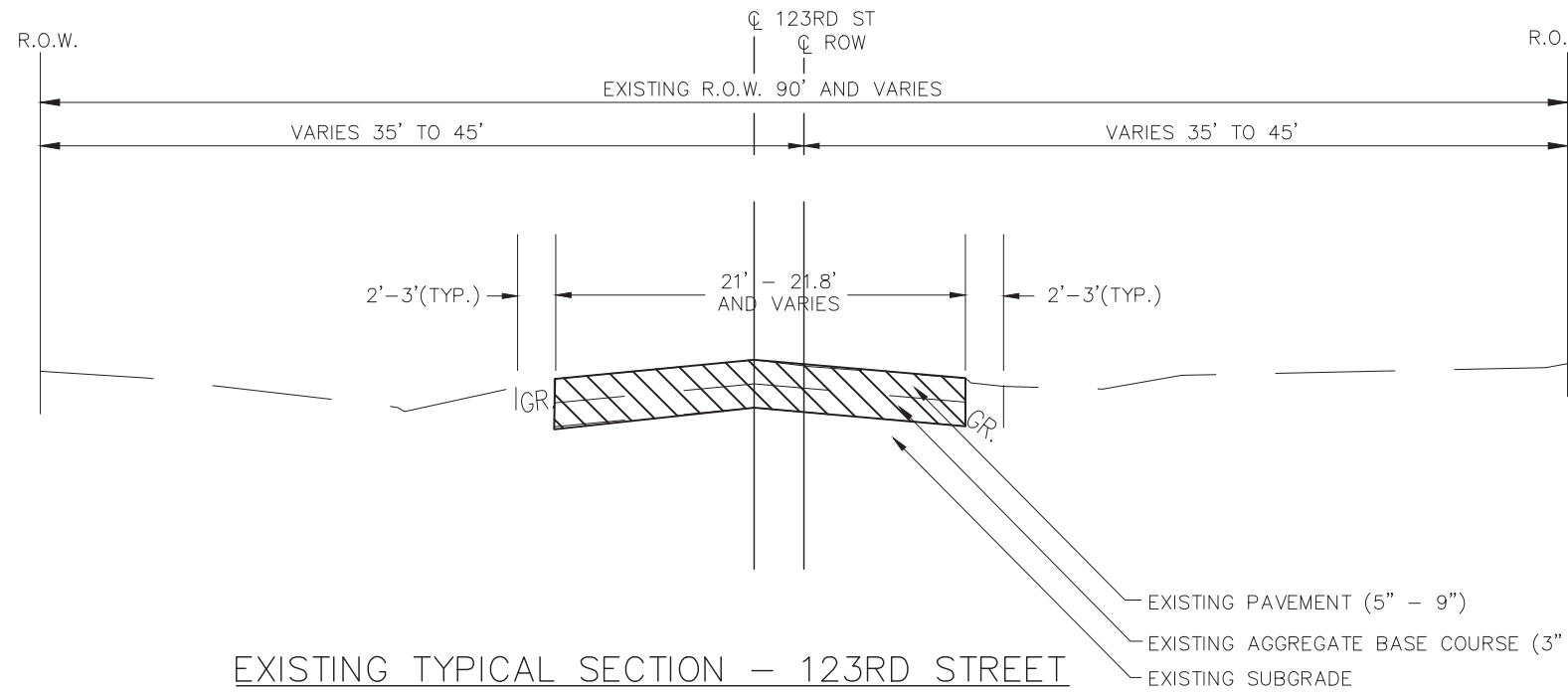
SUMMARY OF QUANTITIES				ROADWAY RECONSTRUCTION CODE 0004	TRAINEES CODE 0042
80% FEDERAL / 20% LOCAL					
STU					
CODE NO	ITEM	UNIT	TOTAL	SUBTOTAL	SUBTOTAL
Δ * 56103000	DUCTILE IRON WATER MAIN 6"	FOOT	149	149	
Δ * 56103300	DUCTILE IRON WATER MAIN 12"	FOOT	2359	2359	
60108204	PIPE UNDERDRAINS, TYPE 2, 4"	FOOT	2761	2761	
60200805	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	14	14	
60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	10	10	
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	7	7	
60500040	REMOVING MANHOLES	EACH	13	13	
67100100	MOBILIZATION	L SUM	1	1	
* 70102621	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	EACH	1	1	
Δ 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	144	144	
Δ * K0036120	MULCH PLACEMENT 4"	SQ YD	115	115	
Δ * X0323449	REMOVE EXISTING WATER VALVE	EACH	7	7	
Δ * X5610746	WATERMAIN LINE STOP, 6"	EACH	15	15	
* X7010216	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1	1	
* Z0004510	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	1185	1185	
* Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT			
* Z0042200	PORTLAND CEMENT CONCRETE PAVEMENT SURFACE REMOVAL	SQ YD	382	382	
Δ * Z0056900	SANITARY SEWER 8"	FOOT	80	80	
Δ * Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1	1	
* Z0076600	TRAINEES	hour	500		500
* Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	hour	500		500
Δ * XX000052	REMOVE EXISTING WATERMAIN	FOOT	200	200	
* XX003424	CONNECTION TO EXISTING STRUCTURE	EACH	1	1	
* XX007091	STABILIZED TURF SHOULDER	SQ YD	975	975	
Δ * XX009443	GATE VALVE 6" WITH VAULT, 5' DIAMETER	EACH	7	7	

\* SPECIAL PROVISION  
Δ SPECIALTY ITEMS

PLOT DATE: Jan 05, 2021  
 FILENAME: H:\20-PRJ\31655 - 123rd Street Reconstruction - Phase II\Plans-Eng\Rev-A\20-PRJ-31655-TITLE-NOTES-DETAILS-2021-01-04.dwg

USER NAME *	DESIGNED - AS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE *	DRAWN - DJB	REVISED -				1710	15-00053-00-PV	COOK	41	4	
PLOT DATE = Jan 05, 2021	CHECKED - AS	REVISED -				SCALE: - SHEET NO. 1 OF 1 SHEETS STA. TO STA.		CONTRACT NO. 61G81		ILLINOIS FED. AID PROJECT	
	DATE -	REVISED -									





**LEGEND**

- PROPOSED HOT MIX ASPHALT SURFACE COURSE
- PROPOSED HOT MIX ASPHALT BINDER COURSE
- AGGREGATE SUBGRADE IMPROVEMENT
- PORTLAND CEMENT CONCRETE SIDEWALK
- PORTLAND CEMENT CONCRETE PAVEMENT SURFACE REMOVAL
- REMOVAL

STA. 0+14 TO STA. 23+26

STA. 23+26 TO STA. 26+07

HOT-MIX MIXTURE REQUIREMENTS			
	MIXTURE TYPE	VOIDS	LIFT THICKNESS
HMA DRIVEWAY PAVEMENT 3"	HMA DRIVEWAY SURFACE, MIX "D", N50, (IL-9.5mm) 3"	4% @ 50 Gyr	
PAVEMENT RESURFACING	HMA SURFACE COURSE, MIX "D", N50, (IL-9.5mm)	4% @ 50 Gyr	2"
	HMA BINDER COURSE, IL-19.0, N50	4% @ 50 Gyr	6"

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.  
 FOR RECYCLED MATERIALS, SEE SPECIAL PROVISIONS  
 THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY SPECIAL PROVISIONS.

PLOT DATE: Jan 26, 2021  
 FILENAME: H:\20-PA\3005 - 123rd Street Reconstruction - Phase I\Plans-Eng\Rev-4\20-PH-3005-TITLE-NOTES-DETAILS-2021-01-25.dwg

USER NAME =	DESIGNED - AS	REVISED -
	DRAWN - DJB	REVISED -
PLOT SCALE =	CHECKED - AS	REVISED -
PLOT DATE = Jan 26, 2021	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL CROSS SECTIONS**  
 SCALE: - SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	5
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	

SCHEDULE OF QUANTITIES				123RD ST STA 0+14 TO STA 5+72	123RD ST STA 5+72 TO STA 11+44	123RD ST STA 11+44 TO STA 17+17	123RD ST STA 17+17 TO STA 22+75	123RD ST STA 22+75 TO STA 26+07
CODE NO	ITEM	UNIT	TOTAL					
20101100	TREE TRUNK PROTECTION	EACH	43	11	15	10	7	
20101200	TREE ROOT PRUNING	EACH	13	4	8	1		
20200100	EARTH EXCAVATION	CU YD	3925	798	760	1129	1052	186
* 20800150	TRENCH BACKFILL	CU YD	414	160	50	75	68	61
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	1947	424	489	488	410	136
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	12137	3100	3050	3150	2552	285
21400100	GRADING AND SHAPING DITCHES	FOOT	3880	890	1015	945	980	50
25200110	SODDING, SALT TOLERANT	SQ YD	12137	3100	3050	3150	2552	285
25200200	SUPPLEMENTAL WATERING	UNIT	10					
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	379	97	95	98	80	9
28000305	TEMPORARY DITCH CHECKS	FOOT	230	60	60	60	50	
28000400	PERIMETER EROSION BARRIER	FOOT	3493	366	1025	1000	960	142
28000510	INLET FILTERS	EACH	31	2	11	9	6	3
28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	12137	3100	3050	3150	2552	285
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	651	142	163	163	137	46
* 30300112	AGGREGATE SUBGRADE IMPROVEMENTS 12"	SQ YD	7780	1694	1954	1951	1637	544
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	305				252	53
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	500	100	100	100	100	100
40300200	BITUMINOUS MATERIALS (PRIME COAT)	TON	0.1	0.03	0.03	0.03	0.03	0.01
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	3114	678	782	781	655	218
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	2696	587	677	676	567	189
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	1041	196	226	226	189	204
42300100	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 5 INCH	SQ YD	298	134	164			
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2735	0	0	0	2260	475
42400800	DETECTABLE WARNINGS	SQ FT	40	0	0	0	20	20
* 44000100	PAVEMENT REMOVAL	SQ YD	7918	1607	1796	1952	1634	929
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1500	566	391	336	207	
* 48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	975	217	253	248	234	23
50105220	PIPE CULVERT REMOVAL	FOOT	962	219	62	175	448	58
54213657	PRECAST REINFORCED CONCRETE FLARED-END SECTION 12"	EACH	16	12	3			1
* 550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	732	378	119	135	90	10
* 550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	1003	0	0	492	453	58
* 56103000	DUCTILE IRON WATER MAIN 6"	FOOT	149	16	44	44	22	23
* 56103300	DUCTILE IRON WATER MAIN 12"	FOOT	2359	541	572	573	572	101
60108204	PIPE UNDERDRAINS, TYPE 2, 4"	FOOT	2761	571	855	780	520	35
60200805	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	14	2	1	6	4	1
60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	10	2	3	3	2	
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	7		6			1

SCHEDULE OF QUANTITIES				123RD ST STA 0+14 TO STA 5+72	123RD ST STA 5+72 TO STA 11+44	123RD ST STA 11+44 TO STA 17+17	123RD ST STA 17+17 TO STA 22+75	123RD ST STA 22+75 TO STA 26+07
CODE NO	ITEM	UNIT	TOTAL					
60500040	REMOVING MANHOLES	EACH	13	3	3	3	3	1
78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	144	0	0	0	48	96
* K0036120	MULCH PLACEMENT 4"	SQ YD	115	28	39	28	20	0
* X0323449	REMOVE EXISTING WATER VALVE	EACH	7	1	2	2	1	1
* X5610746	WATERMAIN LINE STOP, 6"	EACH	15	2	4	4	2	3
* X7010216	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1					
* Z0004514	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	1185	420	229	332	204	
* Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	50					
* Z0042200	PORTLAND CEMENT CONCRETE PAVEMENT SURFACE REMOVAL	SQ YD	382					382
* Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOURL						
* XX000052	REMOVE EXISTING WATERMAIN	FOOT	200	40	40	40	40	40
* XX003424	CONNECTION TO EXISTING STRUCTURE	EACH	1					1
* XX007091	STABILIZED TURF SHOULDER	SQ YD	975	217	253	248	234	23
* XX009443	GATE VALVE 6" WITH VAULT, 5' DIAMETER	EACH	7	1	2	2	1	1

\* SPECIALTY ITEM

TREE TRUNK PROTECTION (EA)	
STATION	DISTANCE FROM CENTER LINE
0+27	21' R
0+50	33' L
1+54	23' R
1+72	31' R
1+95	27' R
2+54	12' R
2+68	27' L
2+99	43' L
3+55	50' L
3+87	44' L
4+09	45' L
5+60	44' L
6+09	44' L
6+85	40' R
6+90	25' R
6+92	25' R
7+35	28' R
7+42	39' L
7+75	29' R
9+65	29' R
10+11	39' L
10+15	39' L
10+17	38' L
10+24	38' R
11+12	36' R
11+22	36' R
12+05	25' L
12+25	43' L
12+77	37' L
13+01	33' R
13+75	33' L
13+84	43' R
13+98	34' L
14+10	45' R
TOTAL	43

TREE TRUNK PROTECTION (EA)	
STATION	DISTANCE FROM CENTER LINE
15+19	32' L
15+84	33' L
18+12	23' L
20+27	45' R
20+37	40' R
20+60	33' L
21+51	28' L
22+05	31' L
22+07	35' L
TOTAL	43

TREE ROOT PRUNING (EA)	
STATION	DISTANCE FROM CENTER LINE
0+27	21' R
1+54	23' R
1+72	31' R
1+95	27' R
6+85	40' R
6+90	25' R
6+92	25' R
7+35	28' R
7+75	29' R
9+65	29' R
10+24	38' R
11+12	36' R
11+22	36' R
13+01	33' R
TOTAL	13

PLOT DATE: Jan 28, 2021  
 FILENAME: H:\20-PA\3005 - 123rd Street Reconstruction - Phase II\Plans-Eng\Rev-A\20-PA-3005-TITLE-NOTES-DETAILS-2021-01-25.dwg

USER NAME =	DESIGNED - AS	REVISED -
PLOT SCALE =	DRAWN - DJB	REVISED -
PLOT DATE = Jan 28, 2021	CHECKED - AS	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

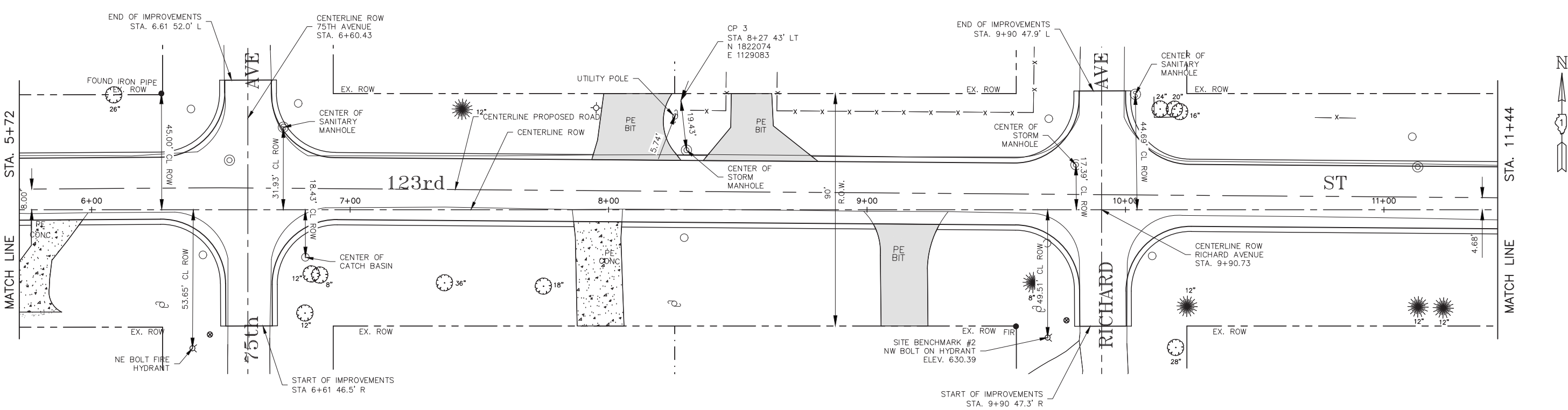
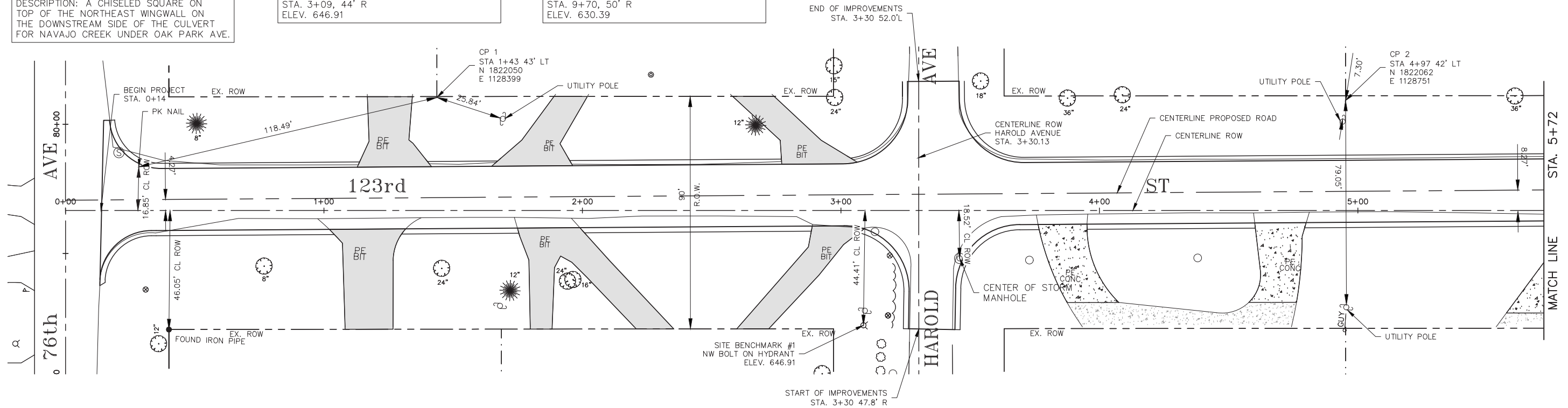
SCALE: -		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	6
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	

SOURCE BENCHMARK:  
CITY OF PALOS HEIGHTS BENCHMARK  
STATION NAME: RM-616-4  
ELEVATION: 609.408 NGVD 29  
DESCRIPTION: A CHISELED SQUARE ON  
TOP OF THE NORTHEAST WINGWALL ON  
THE DOWNSTREAM SIDE OF THE CULVERT  
FOR NAVAJO CREEK UNDER OAK PARK AVE.

SITE BENCHMARK #1  
NW BOLT ON FIRE HYDRANT ON SW CORNER  
OF 123RD STREET & HAROLD AVENUE  
N 1821968, E 1128568  
STA. 3+09, 44' R  
ELEV. 646.91

SITE BENCHMARK #2  
NW BOLT ON FIRE HYDRANT ON SW CORNER  
OF 123RD STREET & RICHARD AVENUE  
N 1821987, E 1129229  
STA. 9+70, 50' R  
ELEV. 630.39



PLOT DATE: Jan 26, 2021  
FILENAME: H:\20-PAV\3005 - 123rd Street Reconstruction - Phase II\Plans-Eng\Rev-4\20-PH-3005-SITE-2021-01-04.dwg

USER NAME =	DESIGNED - AS	REVISED -
PLOT SCALE =	DRAWN - DJB	REVISED -
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

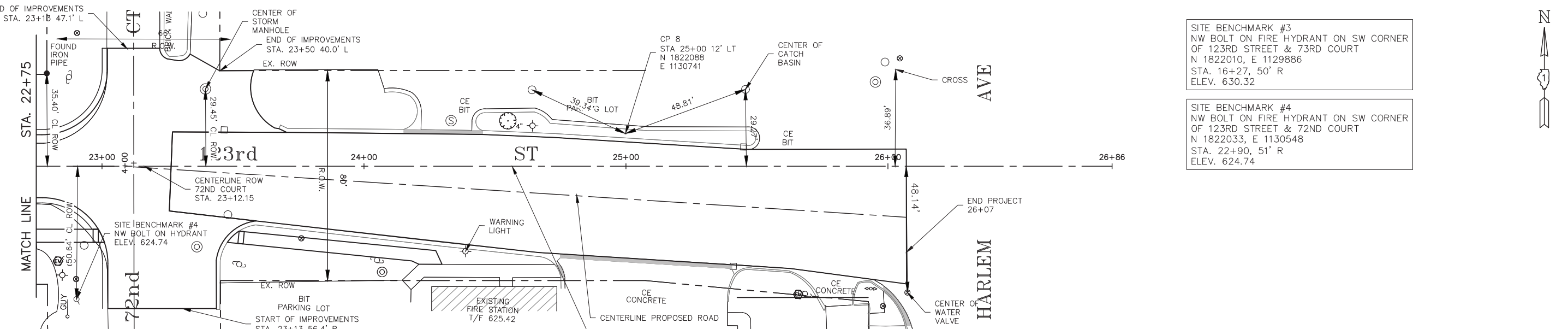
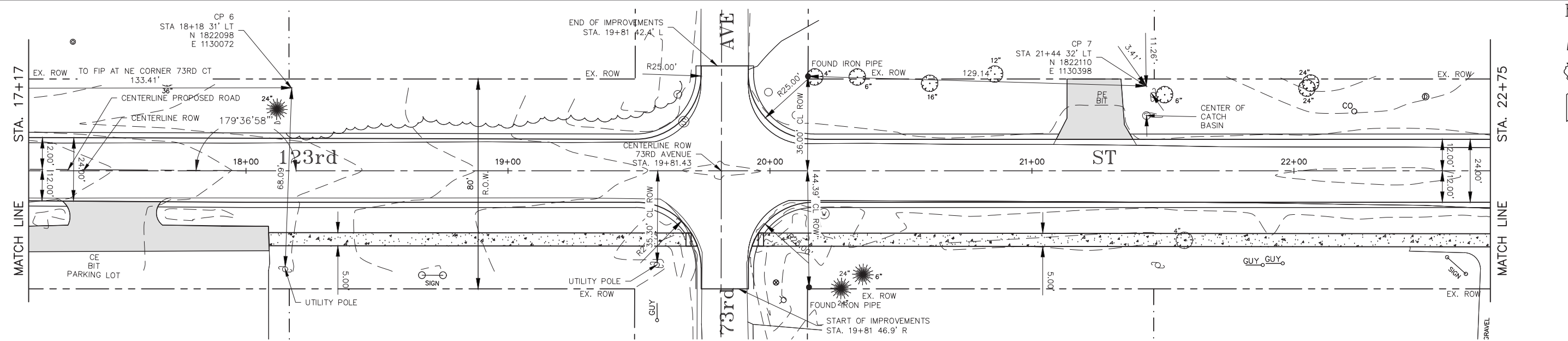
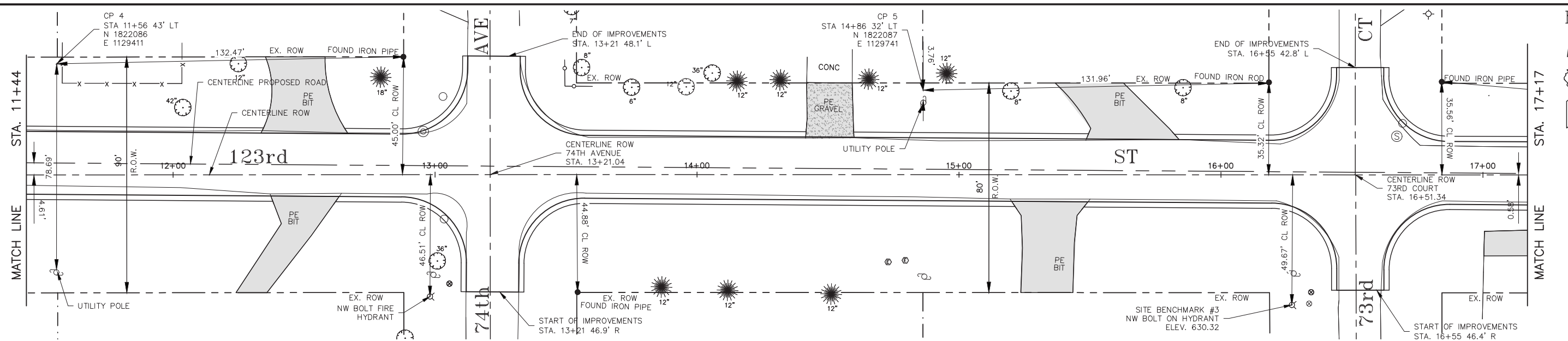
**ALIGNMENT, TIES AND BENCHMARKS**

SCALE: 1"=20'    SHEET NO. 1 OF 2 SHEETS    STA. 00+00 TO STA. 11+44

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	7
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	



PLOT DATE: Jan 26, 2021  
 FILENAME: H:\20-PAV\3005 - 123rd Street Reconstruction - Phase I\Plans-Eng\Rev-A\20-PH-3005-SITE-2021-01-04.dwg



SITE BENCHMARK #3  
 NW BOLT ON FIRE HYDRANT ON SW CORNER  
 OF 123RD STREET & 73RD COURT  
 N 1822010, E 1129886  
 STA. 16+27, 50' R  
 ELEV. 630.32

SITE BENCHMARK #4  
 NW BOLT ON FIRE HYDRANT ON SW CORNER  
 OF 123RD STREET & 72ND COURT  
 N 1822033, E 1130548  
 STA. 22+90, 51' R  
 ELEV. 624.74

USER NAME =	DESIGNED - AS	REVISED -
PLOT SCALE =	DRAWN - DJB	REVISED -
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ALIGNMENT, TIES AND BENCHMARKS**

SCALE: 1"=20'    SHEET NO. 2 OF 2 SHEETS    STA. 11+44 TO STA. 26+86

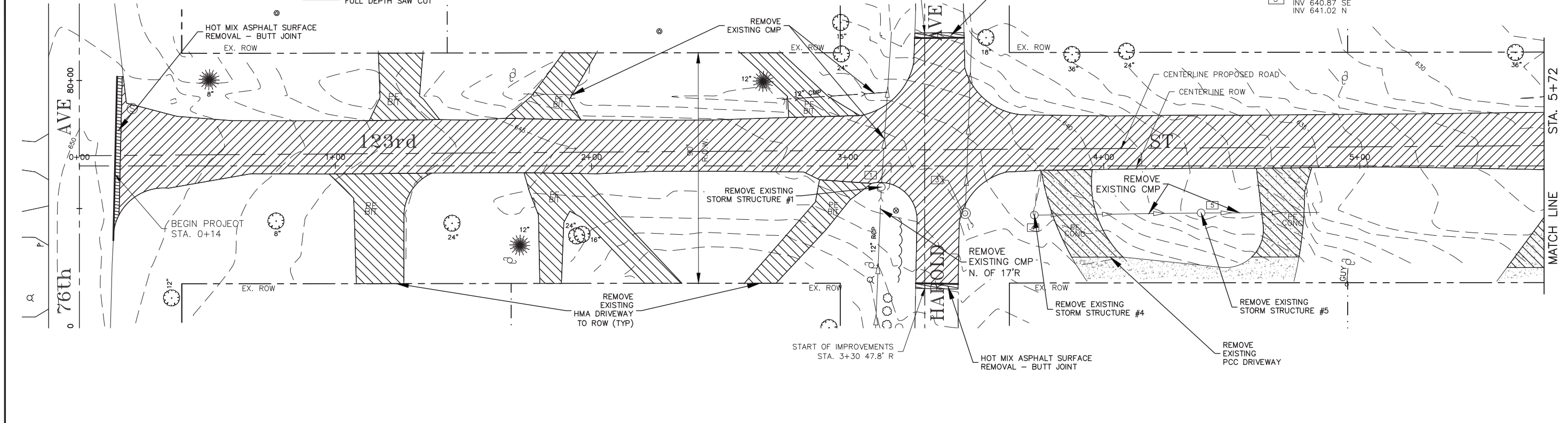
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	8
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	

**DEMOLITION LEGEND**

	FULL DEPTH HMA PAVEMENT REMOVAL
	DRIVEWAY PAVEMENT REMOVAL
	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
	PORTLAND CEMENT CONCRETE PAVEMENT SURFACE REMOVAL
	FULL DEPTH SAW CUT

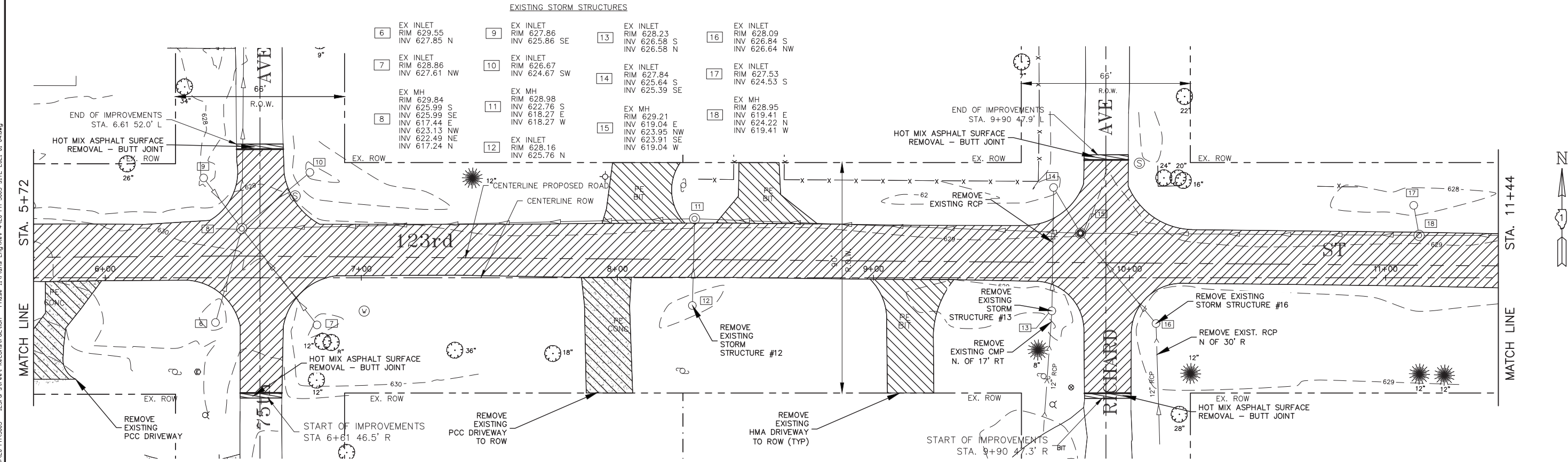
**EXISTING STORM STRUCTURES**

1 EX INLET RIM 642.78 INV 640.78 N	4 EX INLET RIM 640.97 INV 639.37 E
2 EX INLET RIM 646.14	5 EX INLET RIM 637.31 INV 635.31 W INV 635.31 E
3 EX MH RIM 642.97 INV 640.87 SE INV 641.02 N	



**EXISTING STORM STRUCTURES**

6 EX INLET RIM 629.55 INV 627.85 N	9 EX INLET RIM 627.86 INV 625.86 SE	13 EX INLET RIM 628.23 INV 626.58 S INV 626.58 N	16 EX INLET RIM 628.09 INV 626.84 S INV 626.64 NW
7 EX INLET RIM 628.86 INV 627.61 NW	10 EX INLET RIM 626.67 INV 624.67 SW	14 EX INLET RIM 627.84 INV 625.64 S INV 625.39 SE	17 EX INLET RIM 627.53 INV 624.53 S
8 EX MH RIM 629.84 INV 625.99 S INV 625.99 SE INV 617.44 E INV 623.13 NW INV 622.49 NE INV 617.24 N	11 EX MH RIM 628.98 INV 622.76 S INV 618.27 E INV 618.27 W	15 EX MH RIM 629.21 INV 619.04 E INV 623.95 NW INV 623.91 SE INV 619.04 W	18 EX MH RIM 628.95 INV 619.41 E INV 624.22 N INV 619.41 W
12 EX INLET RIM 628.16 INV 625.76 N			



PLOT DATE: Jan 26, 2021  
 FILENAME: H:\20-PAV\3005 - 123rd Street Reconstruction - Phase II\Plans-Eng\Rev-A\20-PH-3005-SITE-2021-01-04.dwg

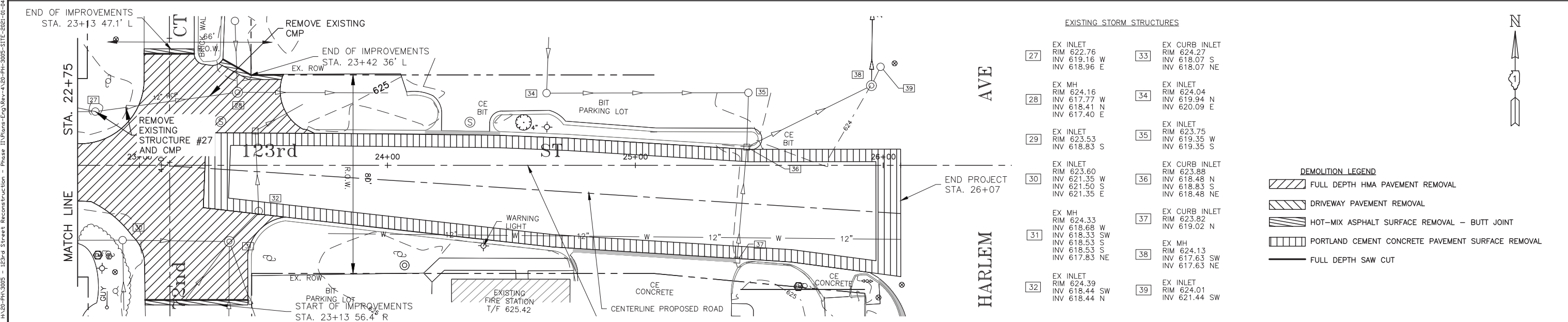
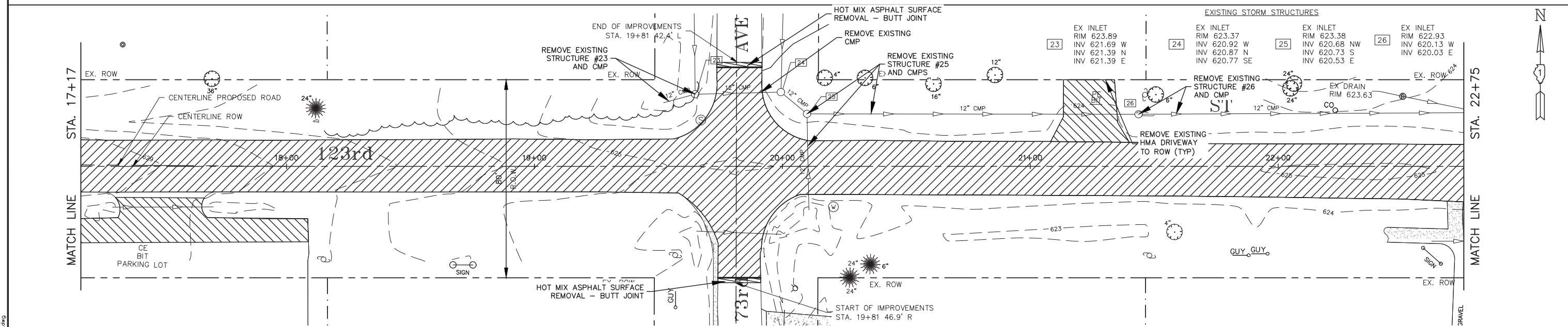
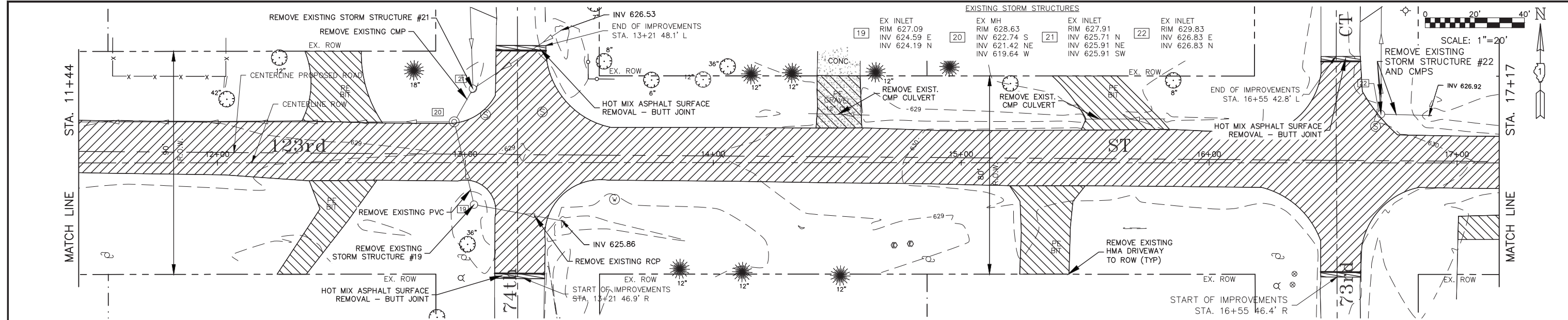
USER NAME =	DESIGNED - AS	REVISED -
PLOT SCALE =	DRAWN - DJB	REVISED -
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN**

SCALE: 1"=20' SHEET NO. 1 OF 2 SHEETS STA. 0+00 TO STA. 11+44

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	9
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	



USER NAME =	DESIGNED - AS	REVISED -
PLOT SCALE =	DRAWN - DJB	REVISED -
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN**

SCALE: 1"=20' SHEET NO. 2 OF 2 SHEETS STA. 11+44 TO STA. 26+86

	FULL DEPTH HMA PAVEMENT REMOVAL
	DRIVEWAY PAVEMENT REMOVAL
	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
	PORTLAND CEMENT CONCRETE PAVEMENT SURFACE REMOVAL
	FULL DEPTH SAW CUT

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	10
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	

PLOT DATE: Jan 26, 2021  
 FILENAME: H:\20-PAV\3005 - 123rd Street Reconstruction - Phase I\Plans-Eng\Rev-A\20-PH-3005-SITE-2021-01-04.dwg

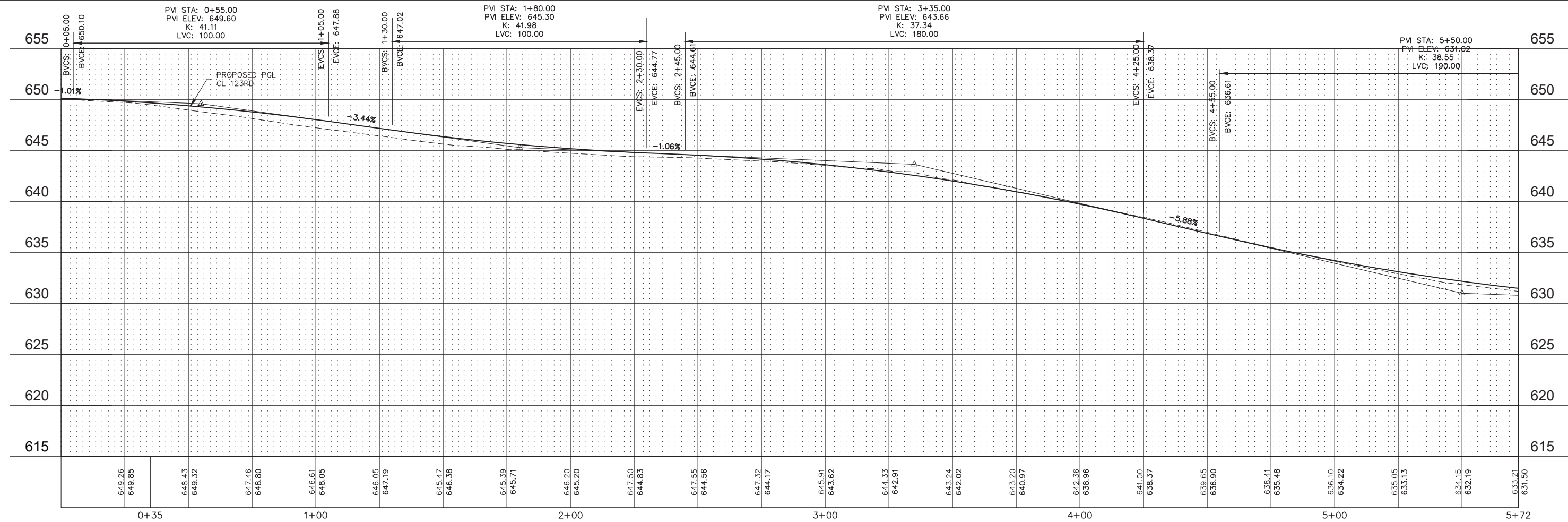
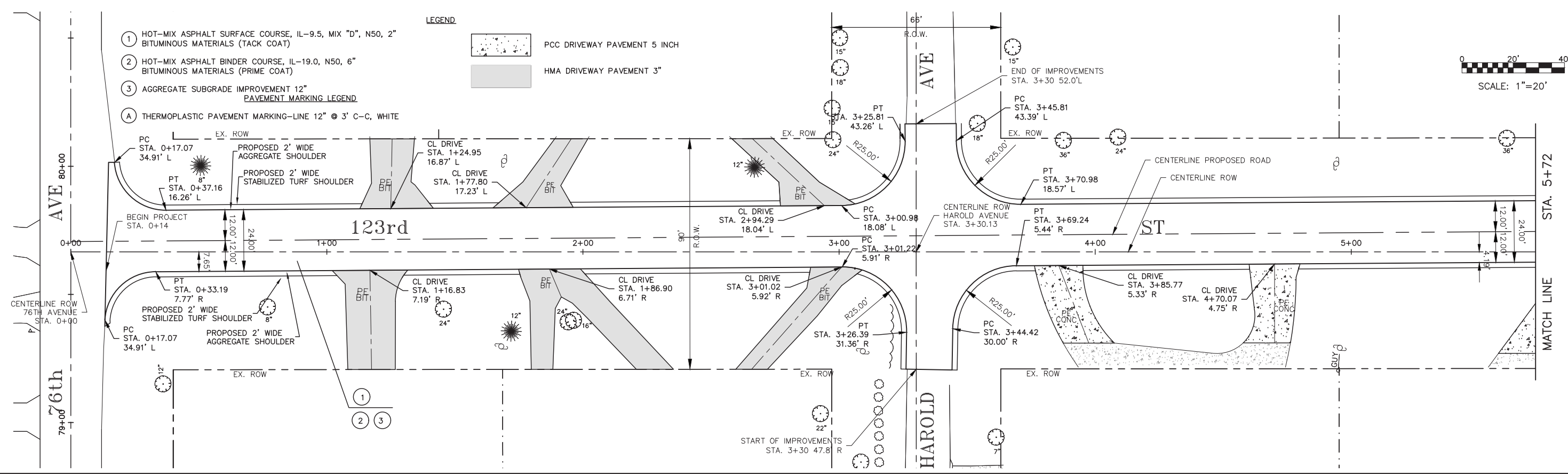
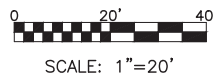


- ① HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2" BITUMINOUS MATERIALS (TACK COAT)
- ② HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 6" BITUMINOUS MATERIALS (PRIME COAT)
- ③ AGGREGATE SUBGRADE IMPROVEMENT 12" PAVEMENT MARKING LEGEND
- (A) THERMOPLASTIC PAVEMENT MARKING—LINE 12" @ 3' C-C, WHITE

**LEGEND**

PCC DRIVEWAY PAVEMENT 5 INCH

HMA DRIVEWAY PAVEMENT 3"



PLOT DATE: Jan 26, 2021  
 FILENAME: H:\20-PAV\3005 - 123rd Street Reconstruction - Phase I\Plans-Eng\Rev-A\20-PH-3005-SITE-2021-01.dwg

USER NAME =	DESIGNED - AS	REVISED -
PLOT SCALE =	DRAWN - DJB	REVISED -
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -
	DATE -	REVISED -

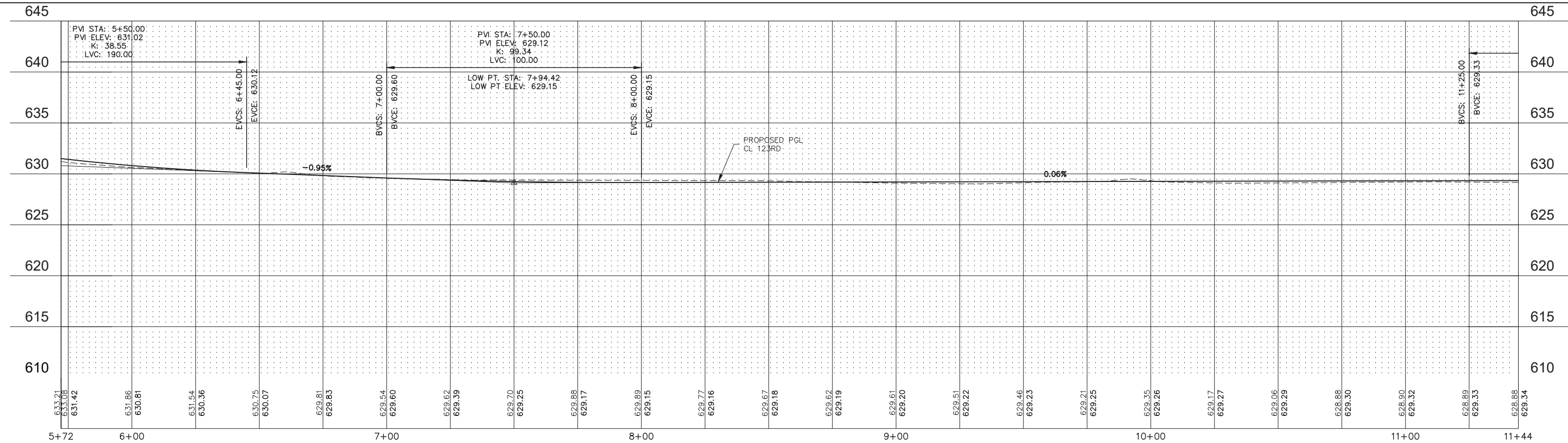
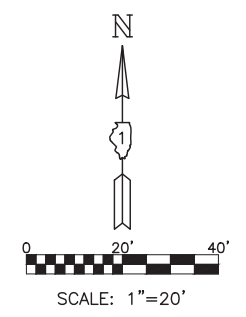
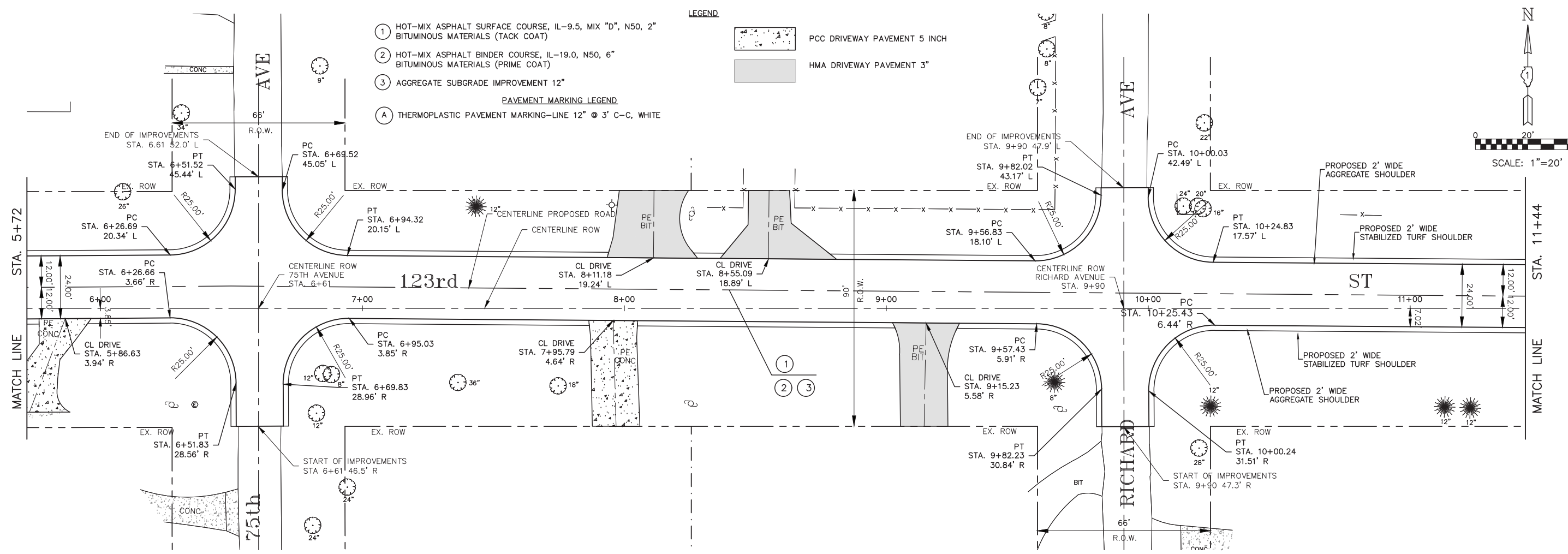
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**123RD STREET PLAN AND PROFILE,  
PAVEMENT MARKING & SIGNAGE**

SCALE: 1"=20'    SHEET NO. 1 OF 5 SHEETS    STA. 0+00 TO STA. 5+72

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	11
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	

PLOT DATE: Jan 26, 2021  
 FILENAME: H:\20-PAV\1005 - 123rd Street Reconstruction - Phase I\Plans-Eng\Rev-A\20-PH-3005-SITE-2021-01.dwg



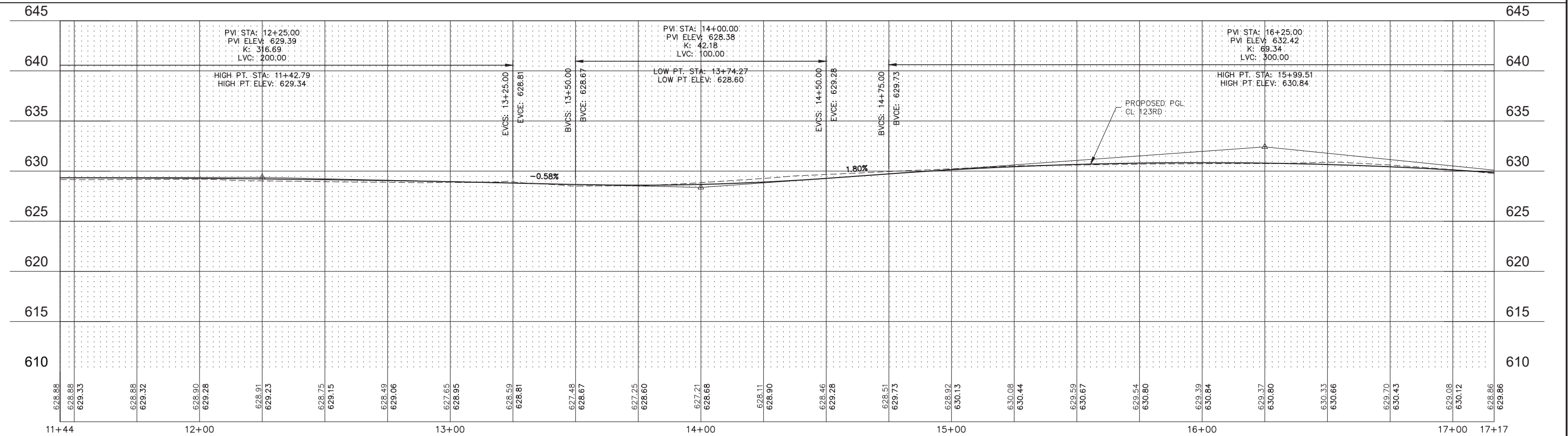
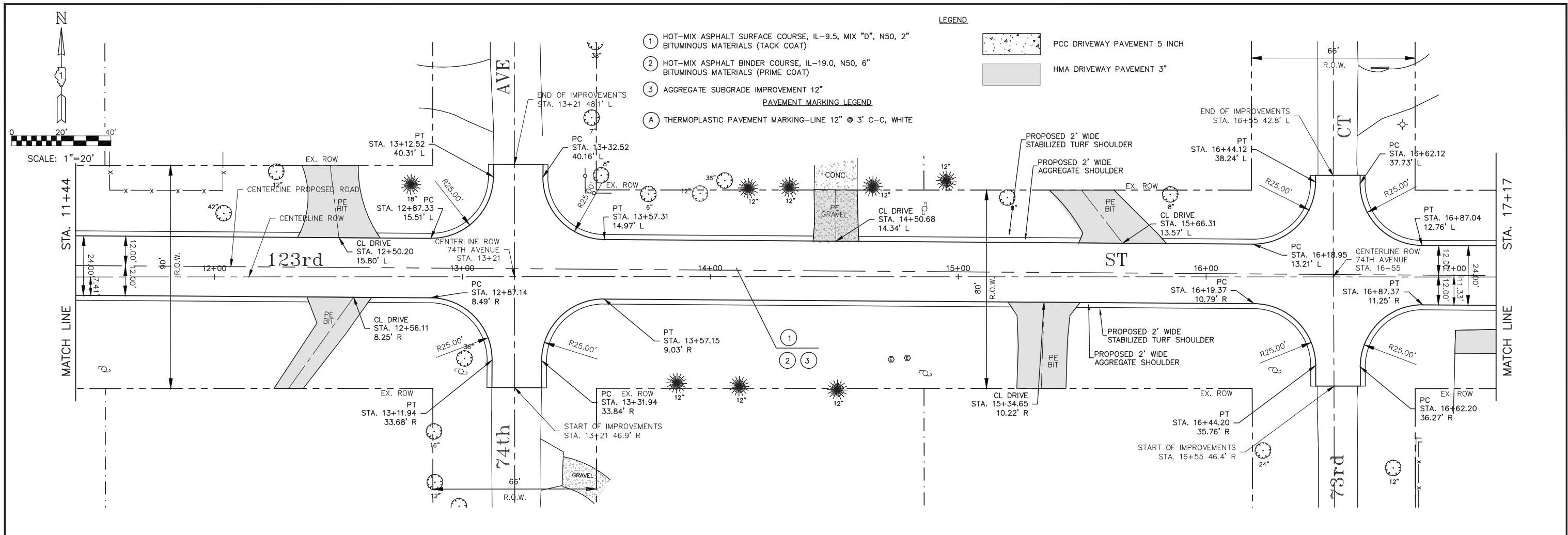
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PLOT SCALE =	DRAWN - DJB	REVISED -
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**123RD STREET PLAN AND PROFILE,**  
**PAVEMENT MARKING AND SIGNAGE**

SCALE: 1"=20'    SHEET NO. 2 OF 5 SHEETS    STA. 5+72 TO STA. 11+44

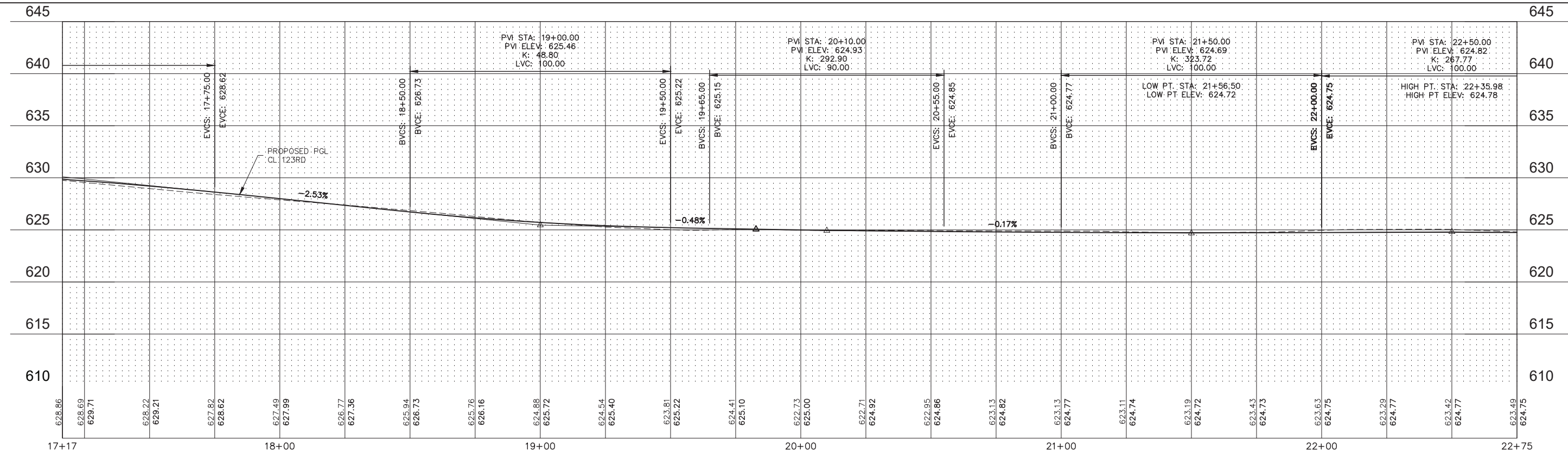
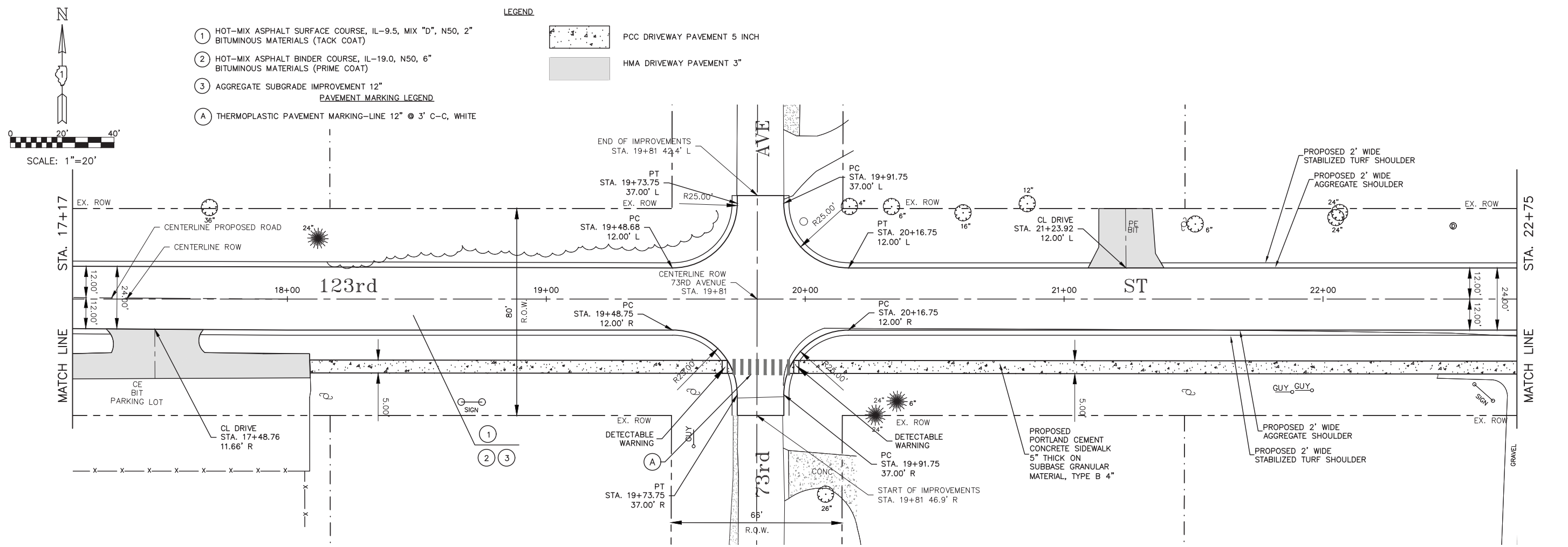
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	12
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	



PLOT DATE: Jan 26, 2021  
 FILENAME: H:\20-PAV\005 - 123rd Streets Reconstruction - Phase I\Plans-Eng\Rev-A\20-PH-3005-SITE-2021-01.dwg

USER NAME =	DESIGNED - AS	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>123RD STREET PLAN AND PROFILE, PAVEMENT MARKING AND SIGNAGE</b>		F.A.U. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE =	DRAWN - DJB	REVISED -		SCALE: 1"=20'	SHEET NO. 3 OF 5 SHEETS	1710	15-00053-00-PV	COOK	46	13
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -		STA. 11+44 TO STA. 17+17	CONTRACT NO. 61G81		ILLINOIS FED. AID PROJECT			
	DATE -	REVISED -								







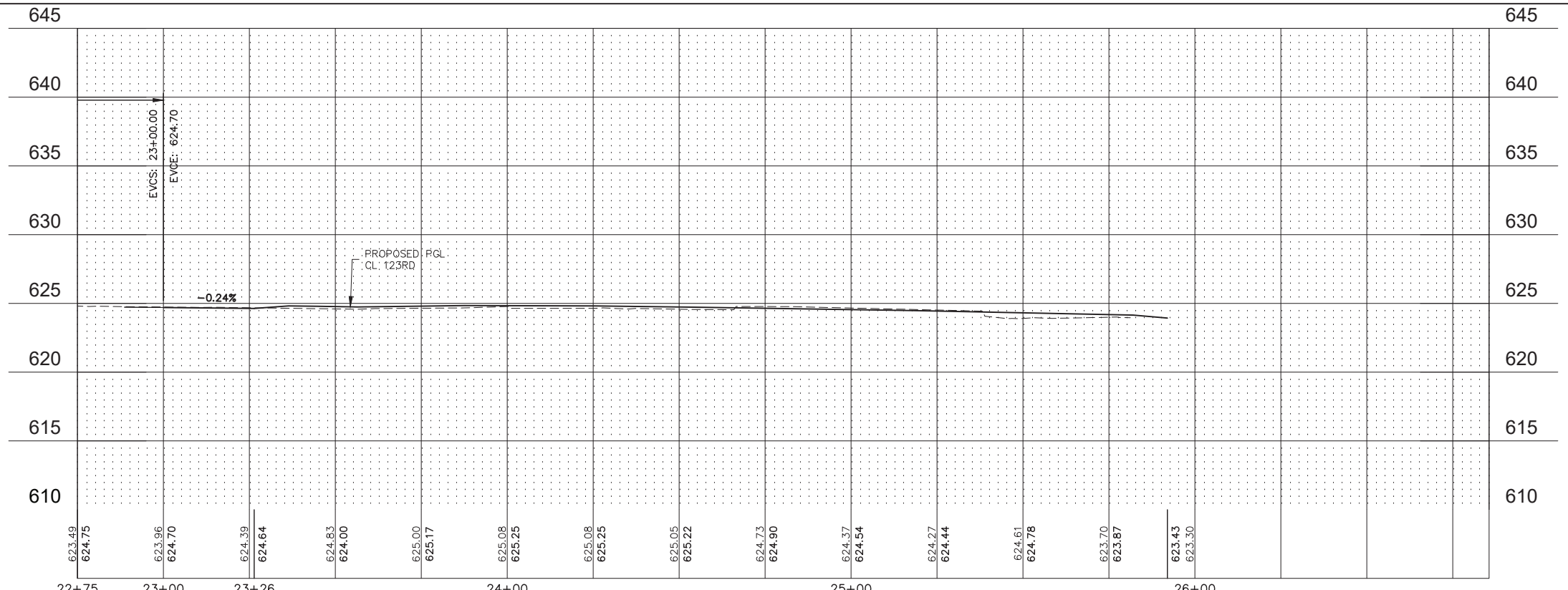
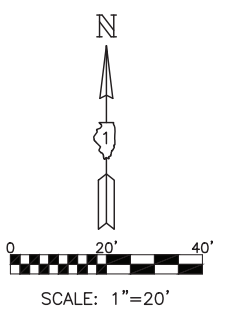
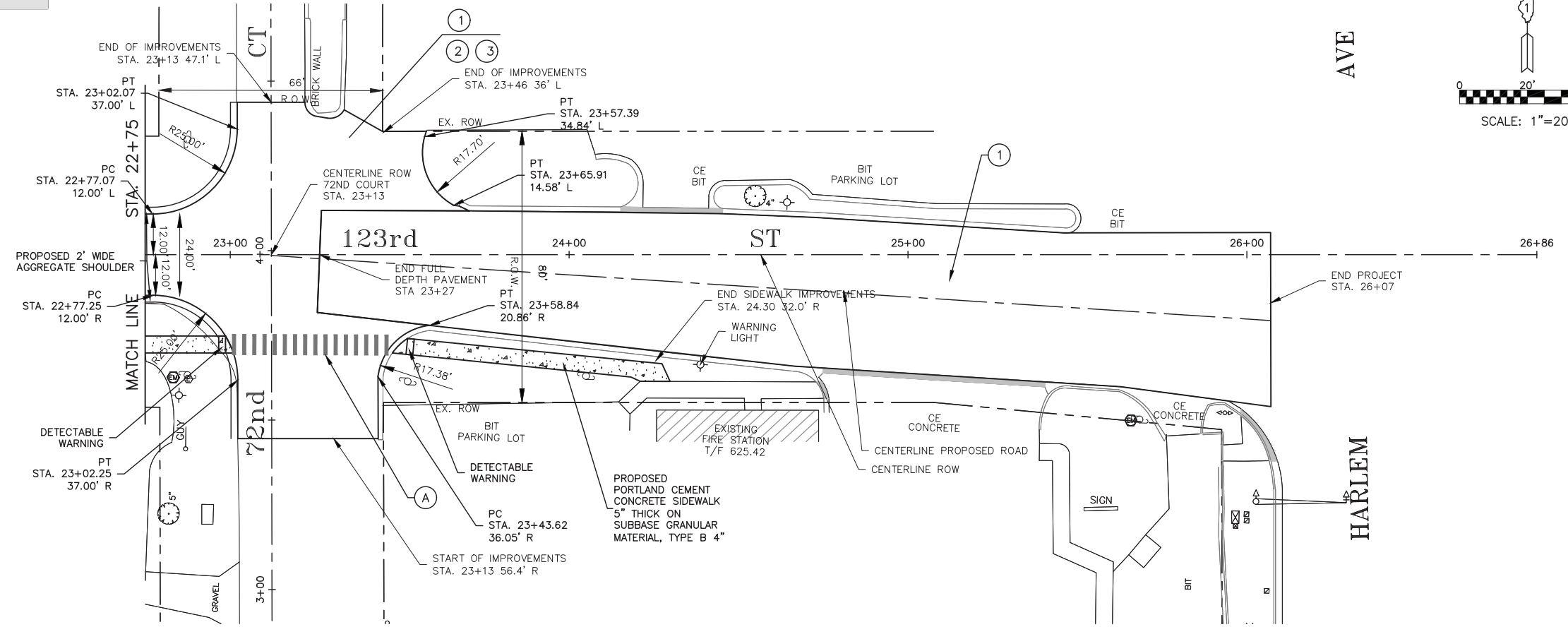
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USER NAME =	DESIGNED - AS	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>123RD STREET PLAN AND PROFILE, PAVEMENT MARKING AND SIGNAGE</b>		F.A.U. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE =	DRAWN - DJB	REVISED -		SCALE: 1"=20'	SHEET NO. 4 OF 5 SHEETS	1710	15-00053-00-PV	COOK	46	14
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -		STA. 17+17 TO STA. 22+75	CONTRACT NO. 61G81		ILLINOIS FED. AID PROJECT			
	DATE -	REVISED -								

- ① HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2" BITUMINOUS MATERIALS (TACK COAT)
  - ② HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 6" BITUMINOUS MATERIALS (PRIME COAT)
  - ③ AGGREGATE SUBGRADE IMPROVEMENT 12"
- PAVEMENT MARKING LEGEND
- (A) THERMOPLASTIC PAVEMENT MARKING—LINE 12" @ 3' C-C, WHITE

LEGEND

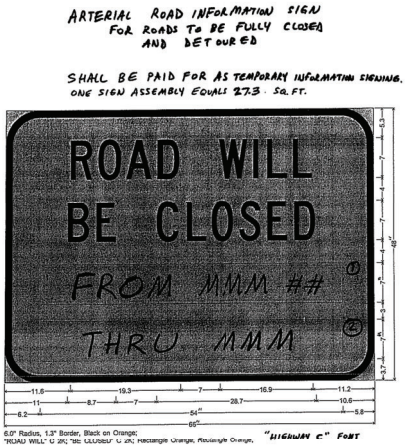
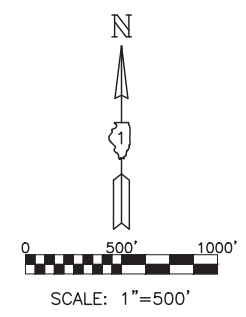
-  PCC DRIVEWAY PAVEMENT 5 INCH
-  HMA DRIVEWAY PAVEMENT 3"



PLOT DATE: Jan 26, 2021  
 FILENAME: H:\20-PAV\3005 - 123rd Streets Reconstruction - Phase I\Plans-Eng\Rev-A\20-PH-3005-SITE-2021-01.dwg

USER NAME =	DESIGNED - AS	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>123RD STREET PLAN AND PROFILE, PAVEMENT MARKING AND SIGNAGE</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE =	DRAWN - DJB	REVISED -			1710	15-00053-00-PV	COOK	46	15
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -			CONTRACT NO. 61G81		ILLINOIS FED. AID PROJECT		

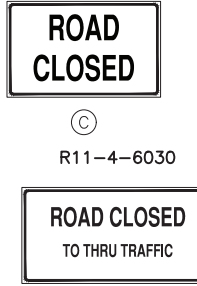
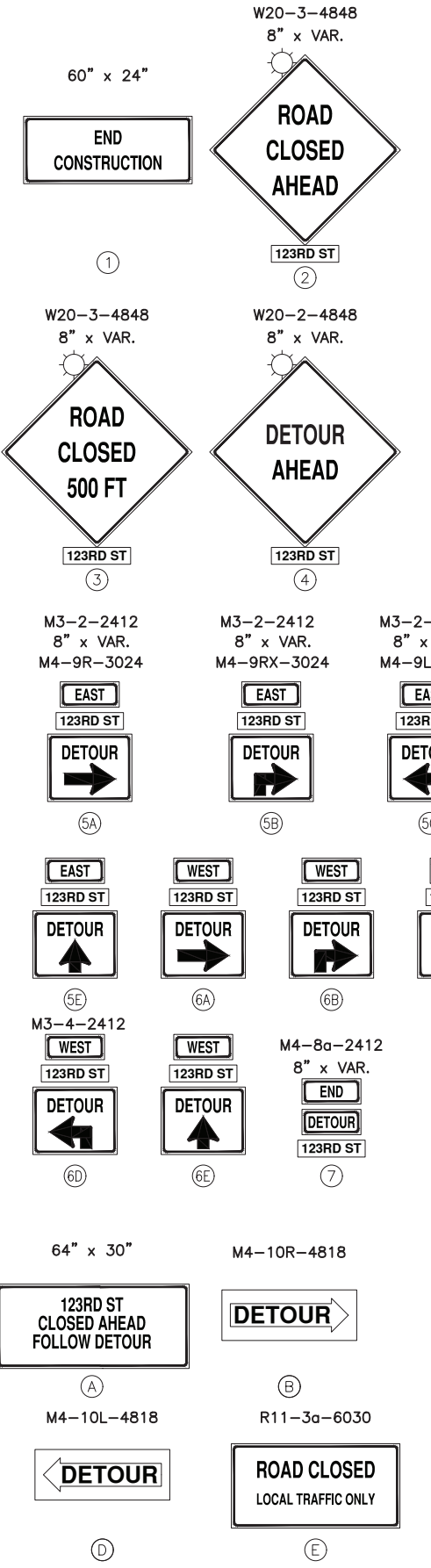
SCALE: 1"=20'    SHEET NO. 5 OF 5 SHEETS    STA. 22+75 TO STA. 26+00



- ① OVERLAY PANEL ① TO CONTAIN STARTING DATE OF FULL CLOSURE AND DETOUR IMPLEMENTATION. (I.E. "FROM MAR 2")
  - ② OVERLAY PANEL ② TO CONTAIN ENDING MONTH OF FULL CLOSURE & DETOUR. (I.E. "THRU JULY"). OMIT THE DATE ON PANEL ② MONTH ONLY.
- ERECT SIGN ASSEMBLY (POST-MOUNTED) WITH PANEL ① AND ② IN PLACE ON ROAD TO BE CLOSED IN EACH DIRECTION NEAR POINT OF CLOSURE OR WITHIN SECTION TO BE FULLY CLOSED. REMOVE SIGN FROM TO START DATE OF FULL CLOSURE. REMOVE ASSEMBLY AFTER CLOSURE.



- NOTES**
1. THIS ITEM SHALL BE PERFORMED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, SECTION 700; APPLICABLE GUIDELINES IN THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS; AND THE APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL, UNLESS HEREIN REVISED.
  2. THE TYPE III BARRICADES ARE TO BE PLACED IN ACCORDANCE WITH THE APPLICABLE TRAFFIC CONTROL STANDARDS UNLESS THE ENGINEER DIRECTS THAT ALTERNATE ARRANGEMENT BE USED.
  3. NO CONSTRUCTION SHALL BEGIN UNTIL ALL TRAFFIC CONTROL IS IN PLACE AND APPROVED BY THE ENGINEER.
  4. WHEN ANY CONSTRUCTION OPERATIONS ARE PERFORMED, ALL SIGNS, BARRICADES, CONES, WARNING LIGHTS, ETC. SHALL BE INSTALLED IN ACCORDANCE WITH THE STANDARDS. ARROW BOARDS SHALL BE REQUIRED FOR ALL LANE CLOSURES. WHENEVER TRAFFIC IS RESTRICTED TO ONE LANE OF ROADWAY, STANDARD 701501-06 SHALL BE FOLLOWED.
  5. RELOCATION OR REMOVAL OF EXISTING TRAFFIC CONTROL SIGNS AND DEVICES IN CONFLICT WITH CONSTRUCTION SHALL BE DONE WITH COST INCIDENTAL TO EARTH EXCAVATIONS. ANY SIGNS OR DEVICES LEFT IN PLACE ARE TO BE PROTECTED FROM DAMAGE BY THE CONTRACTOR, AND ANY DAMAGE CAUSED BY CONSTRUCTION WILL BE PAID FOR BY THE CONTRACTOR.
  6. ANY DROP-OFF GREATER THAN THREE INCHES BUT LESS THAN SIX INCHES, WITHIN EIGHT FEET OF THE PAVEMENT EDGE, SHALL BE PROTECTED BY TYPE I OR TYPE II BARRICADES EQUIPPED WITH MON-DIRECTIONAL STEADY-BURN LIGHTS AT 100' CENTER TO CENTER SPACING. IF THE DROP-OFF WITHIN EIGHT FEET OF THE PAVEMENT EXCEEDS SIX INCHES, THE BARRICADES MENTIONED ABOVE SHALL BE PLACED AT 50' CENTER SPACING. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOP OF THE BARRICADE IS IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF STANDARD 701501-08.
  7. TYPE I OR TYPE II BARRICADES WITH TWO WAY FLASHING LIGHTS SHALL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER STRUCTURES, TRANSVERSE PAVEMENT JOINTS, MATERIALS, EQUIPMENT WITHIN THE RIGHT-OF-WAY (NUMBER AND SPACING DEPENDS ON THE CONDITIONS), AND AT LOCATIONS DESIGNATED BY THE ENGINEER OR LOCAL LAW ENFORCEMENT AGENCIES.
  8. THE COST OF SUPPLYING, ERECTING AND MAINTAINING BARRICADES, WARNING LIGHTS, ARROW BOARDS, AND SIGNS WILL BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL).
  9. THE FOLLOWING TRAFFIC CONTROL STANDARDS ARE NOTED AS MINIMUM GUIDES IN IMPLEMENTING THE TRAFFIC CONTROL FOR THIS PROJECT:
    - 701301-04 LANE CLOSURE 2L 2W SHORT TIME OPERATIONS
    - 701501-06 URBAN LANE CLOSURE 2L 2W UNDIVIDED
    - 701801-06 SIDEWALK, CORNER OR CROSSWALK CLOSURE
    - 701901-08 TRAFFIC CONTROL & PROTECTION
    - 720005-04 SIGN MOUNTING DETAILS
    - 728001-01 TELESCOPING STEEL SIGN SUPPORT
    - TC-21 DETOUR SIGNAGE
  10. TRAFFIC CONTROL DEPICTED IS THE MINIMUM REQUIREMENT. OTHER SIGNS OR BARRICADES MAY BE REQUIRED BY THE ENGINEER TO COVER UNFORESEEN SITUATIONS OR PROJECT SEQUENCING. ADHERENCE TO THIS TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR FROM LIABILITY OR RESPONSIBILITY IN PROVIDING COMPLETE TRAFFIC SAFETY FOR MOTORISTS AND CONSTRUCTION PERSONNEL.
  11. STREET NAME SIGN TO BE SPECIAL SIGN WITH 6" BLACK UPPERCASE LETTER ON ORANGE REFLECTIVE BACKGROUND.



USER NAME =	DESIGNED - AS	REVISED -
PLOT SCALE =	DRAWN - DJB	REVISED -
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

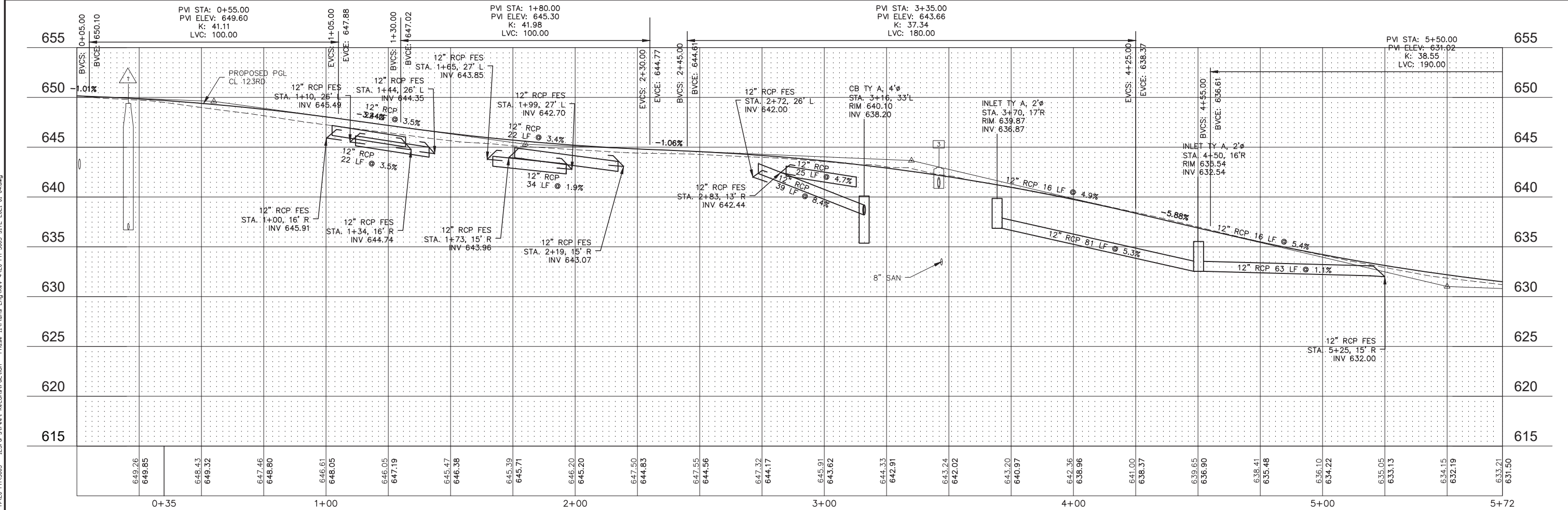
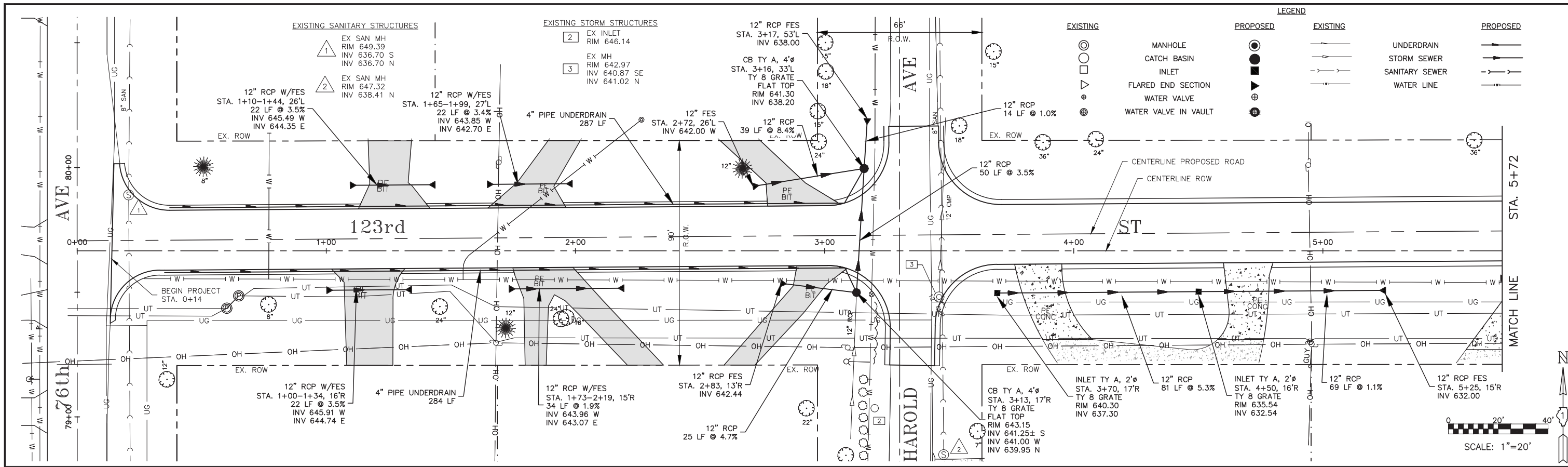
**DETOUR PLAN**

SCALE: 1"=500' SHEET NO. 1 OF 1 SHEETS STA. - TO STA. -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	16
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	

PLOT DATE: Jan 26, 2021  
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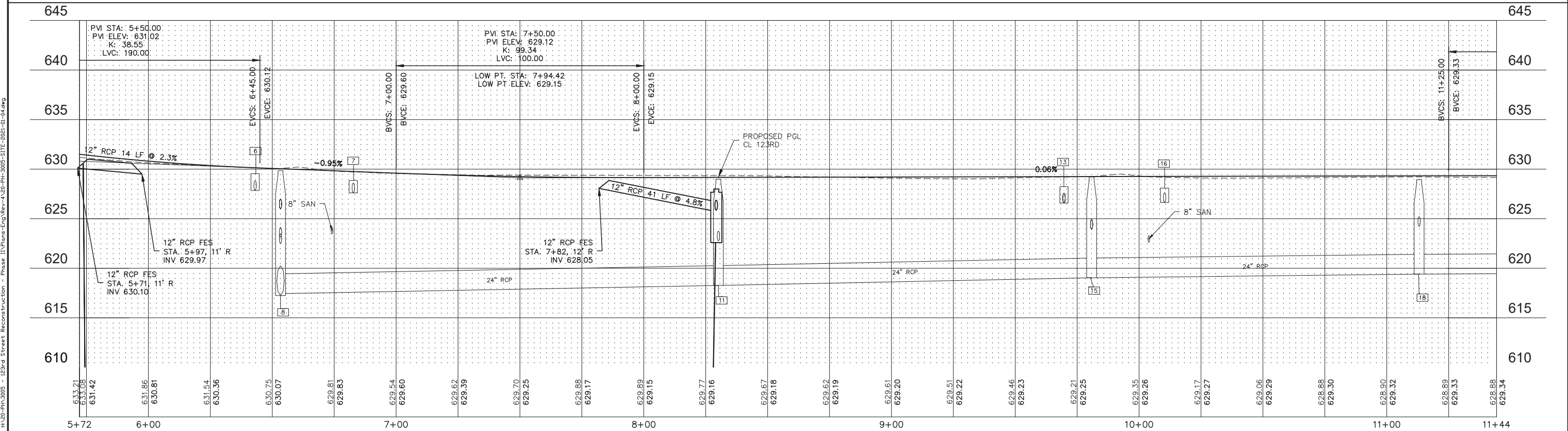
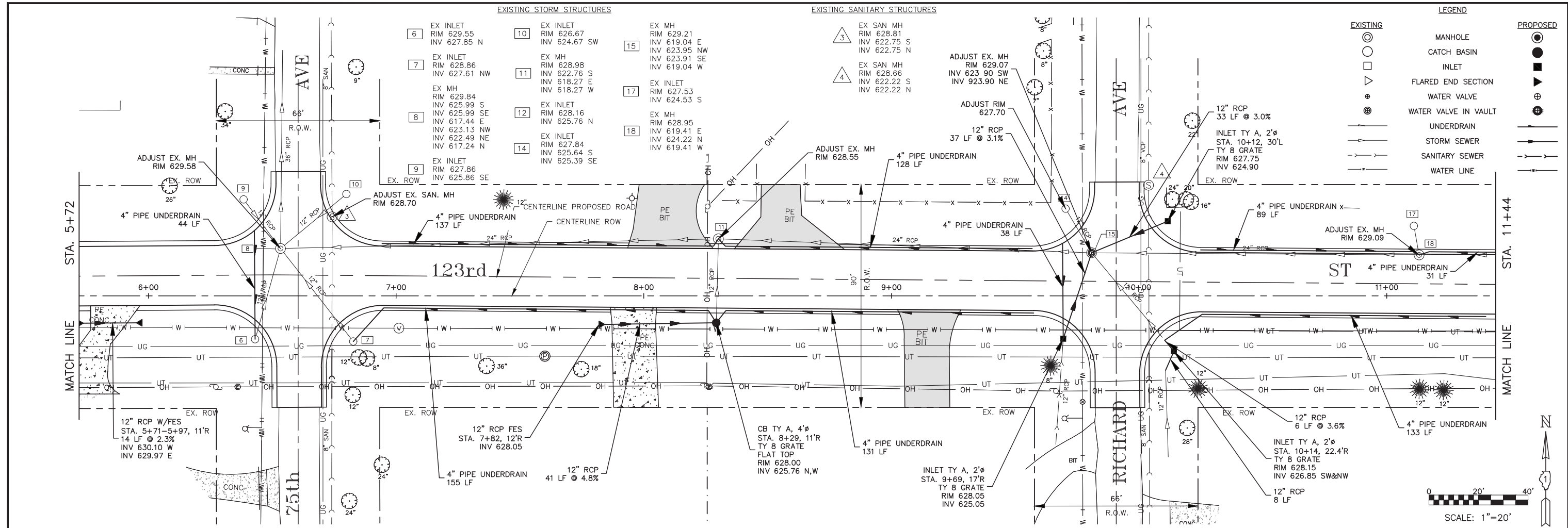
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 DRAWN - DJB  
 CHECKED - AS  
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REVISED -  
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

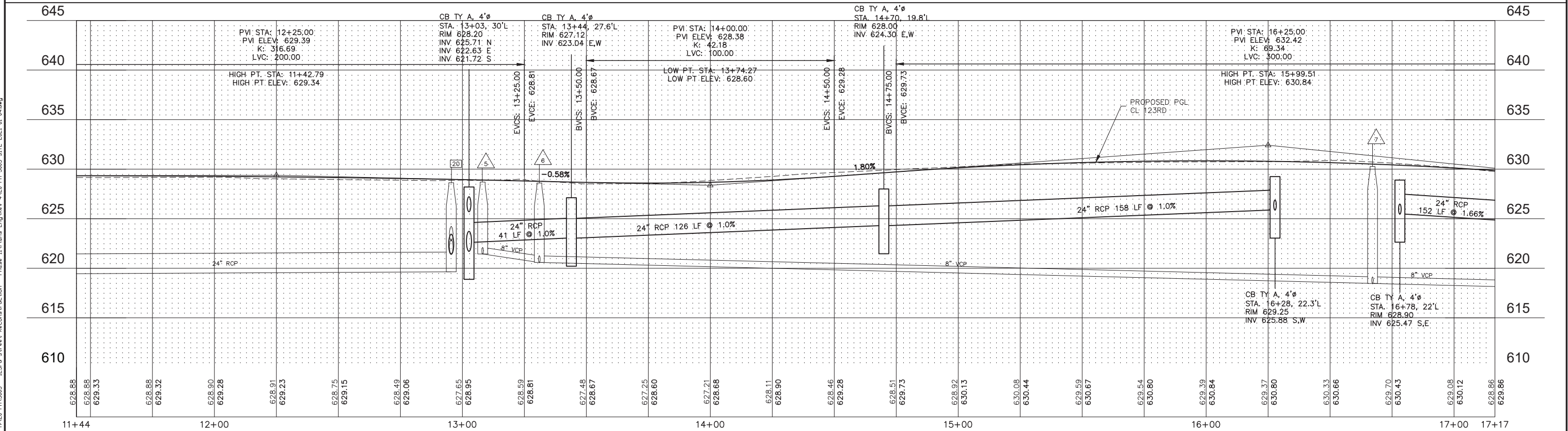
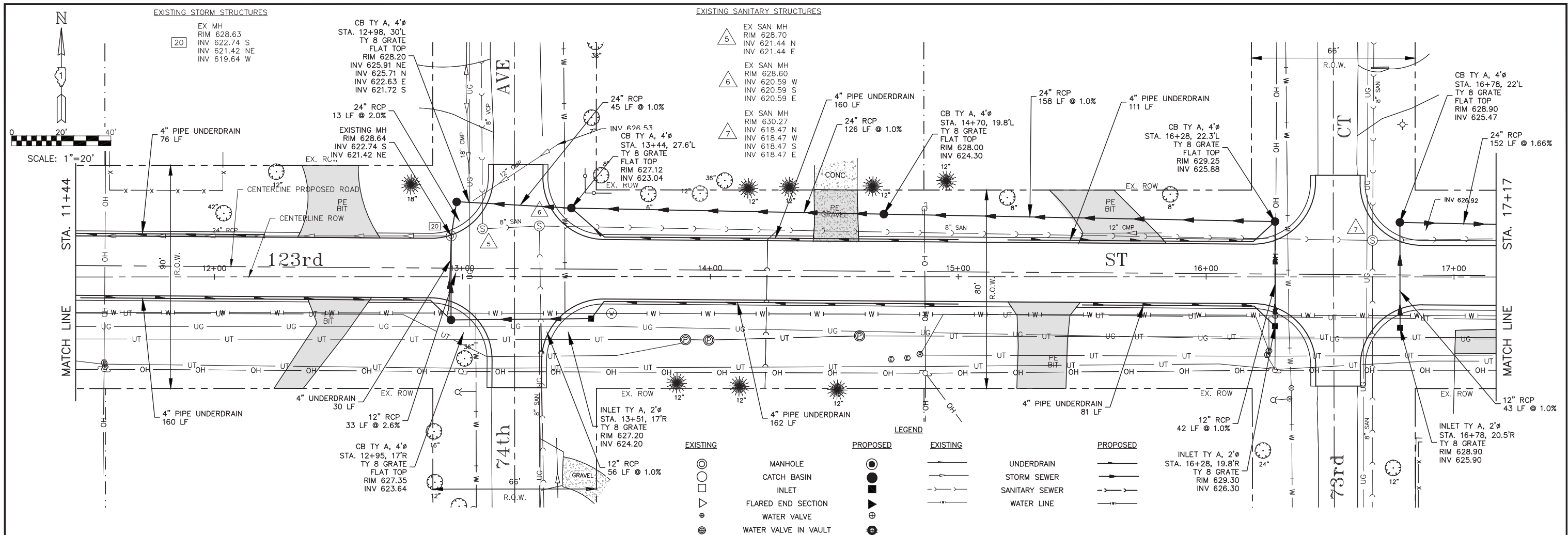
**DRAINAGE AND UTILITIES**  
 SCALE: 1"=20' SHEET NO. 1 OF 5 SHEETS STA. 0+00 TO STA. 5+72

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	17
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	



USER NAME = PLOT SCALE = PLOT DATE = Jan 26, 2021	DESIGNED - AS DRAWN - DJB CHECKED - AS DATE -	REVISED - REVISED - REVISED - REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>DRAINAGE AND UTILITIES</b> SCALE: 1"=20' SHEET NO. 2 OF 5 SHEETS STA. 5+72 TO STA. 11+44	F.A.U. SECTION COUNTY TOTAL SHEETS SHEET NO. 1710 15-00053-00-PV COOK 46 18 CONTRACT NO. 61G81 ILLINOIS FED. AID PROJECT
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PLOT DATE: Jan 26, 2021  
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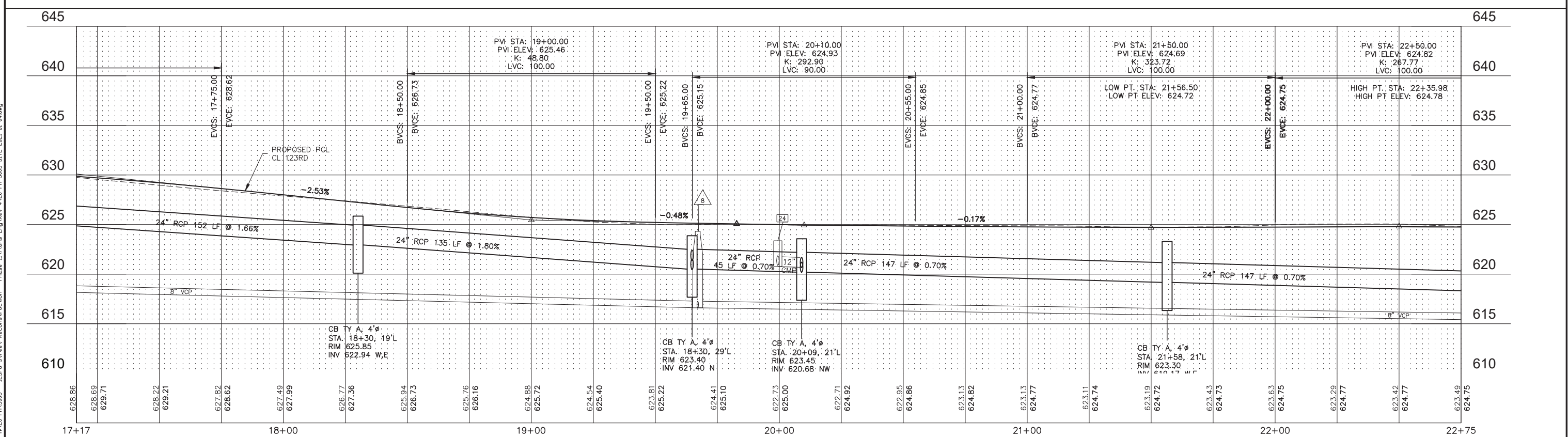
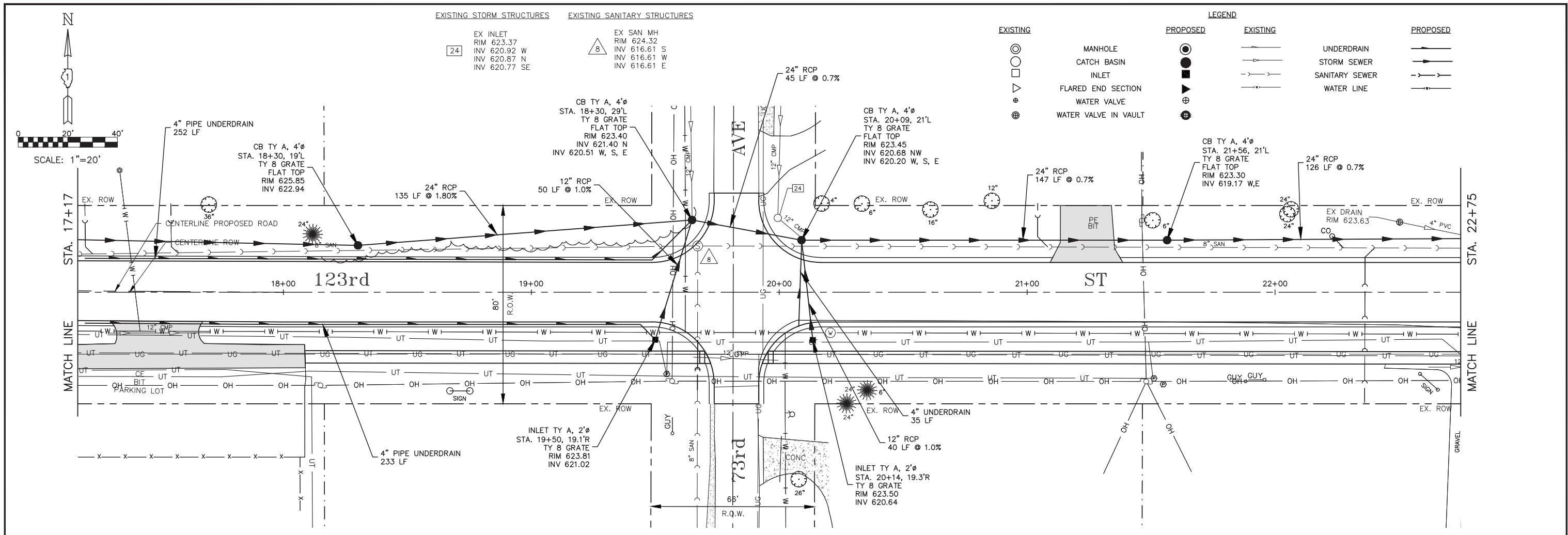
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PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DRAINAGE AND UTILITIES**  
 SCALE: 1"=20'    SHEET NO. 3 OF 5 SHEETS    STA. 11+44 TO STA. 17+17

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	19
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	





PLOT DATE: Jan 26, 2021  
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 PLOT DATE = Jan 26, 2021

DESIGNED - AS  
 DRAWN - DJB  
 CHECKED - AS  
 DATE -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DRAINAGE AND UTILITIES**  
 SCALE: 1"=20'    SHEET NO. 4 OF 5 SHEETS    STA. 17+17 TO STA. 22+75

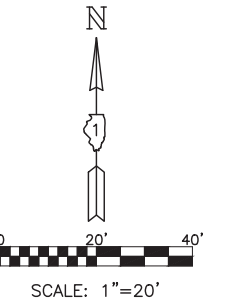
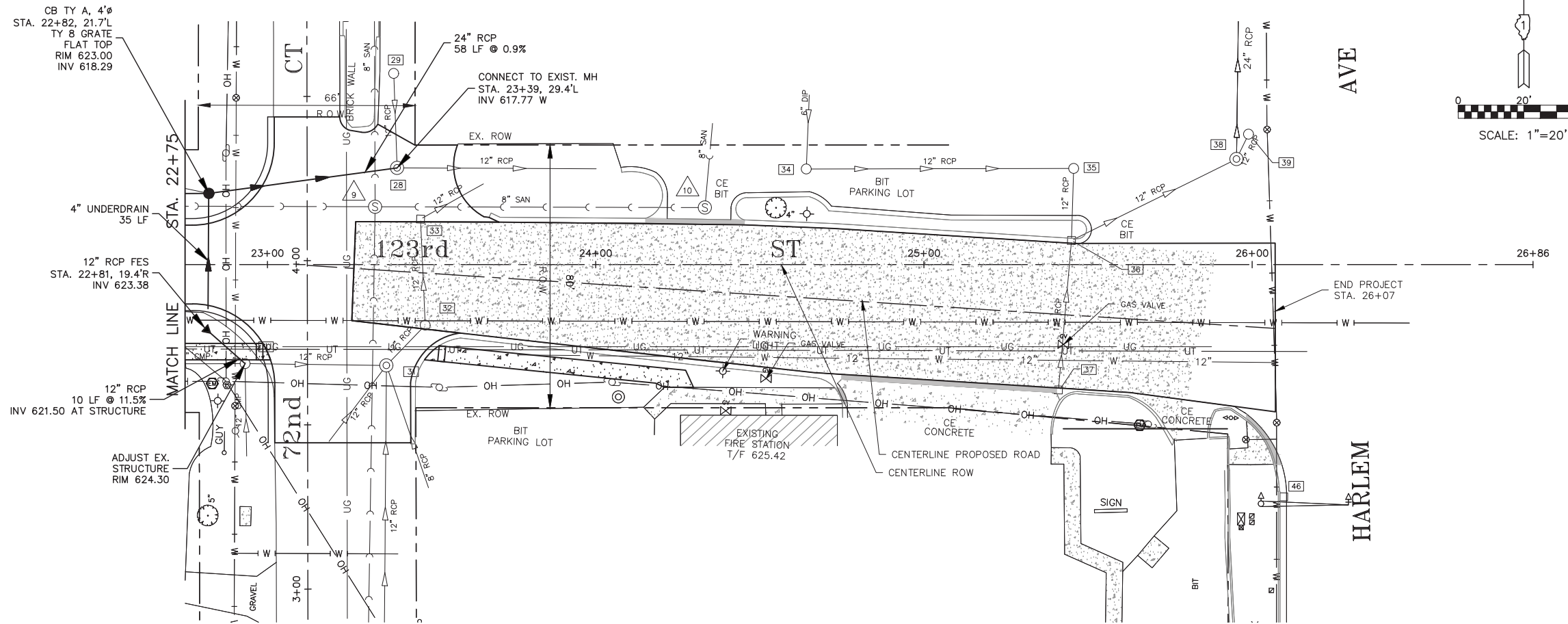
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1710	15-00053-00-PV	COOK	46	20
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	

EXISTING STORM STRUCTURES

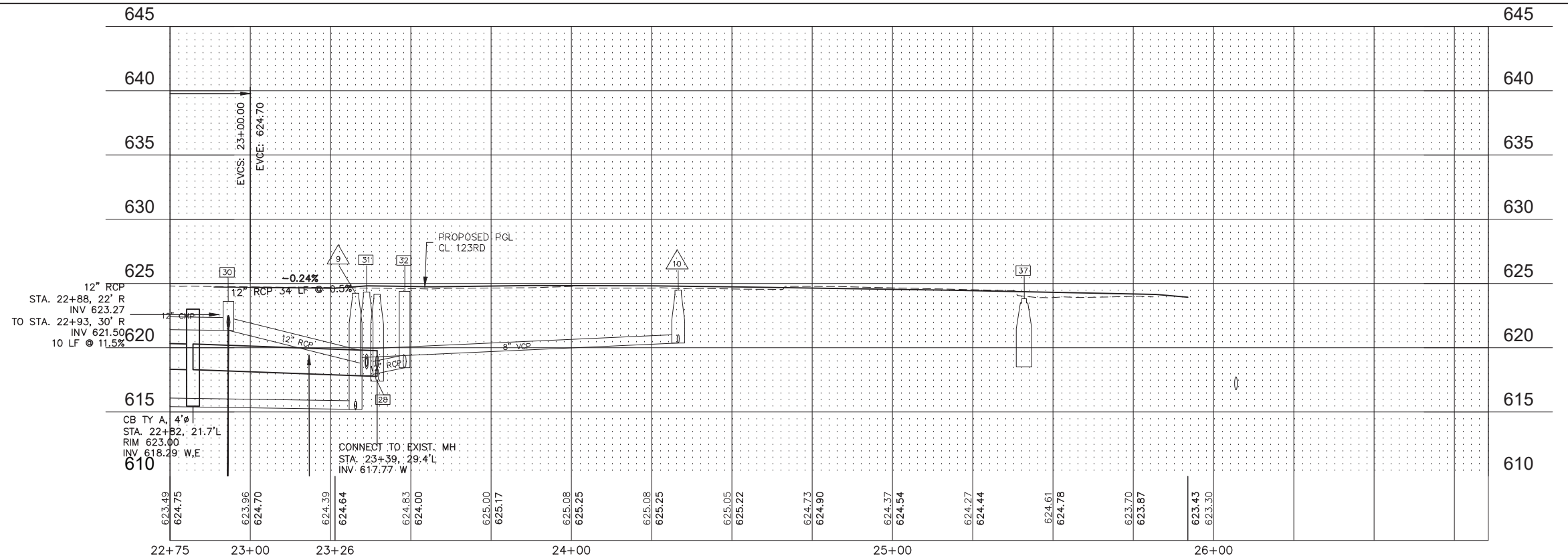
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28	EX MH RIM 624.16 INV 617.77 W INV 618.41 N INV 617.40 E	37	EX CURB INLET RIM 623.82 INV 619.02 N
29	EX INLET RIM 623.53 INV 618.83 S	38	EX MH RIM 624.13 INV 617.63 SW INV 617.63 NE
30	EX INLET RIM 623.60 INV 621.35 W INV 621.50 S INV 621.35 E	39	EX INLET RIM 624.01 INV 621.44 SW
31	EX MH RIM 624.33 INV 618.68 W INV 618.33 SW INV 618.53 S INV 618.53 SE INV 617.83 NE		
32	EX INLET RIM 624.39 INV 618.44 SW INV 618.44 N		
33	EX CURB INLET RIM 624.27 INV 618.07 S INV 618.07 NE		
34	EX INLET RIM 624.04 INV 619.94 N INV 620.09 E		
35	EX INLET RIM 623.75 INV 619.35 W INV 619.35 S		

EXISTING SANITARY STRUCTURES

9	EX SAN MH RIM 624.24 INV 615.20 W INV 615.20 S INV 619.22 E INV 615.20 N
10	EX SAN MH RIM 624.48 INV 620.37 N INV 620.37 W



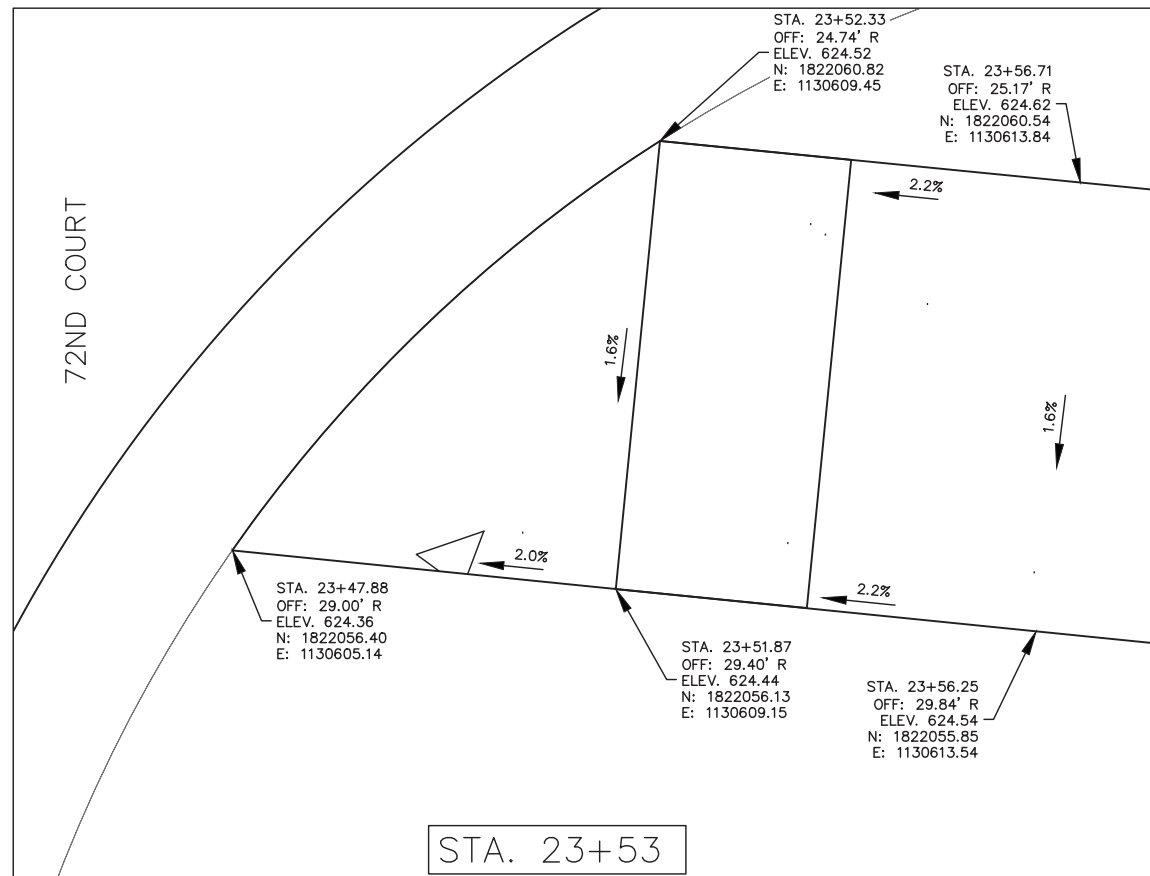
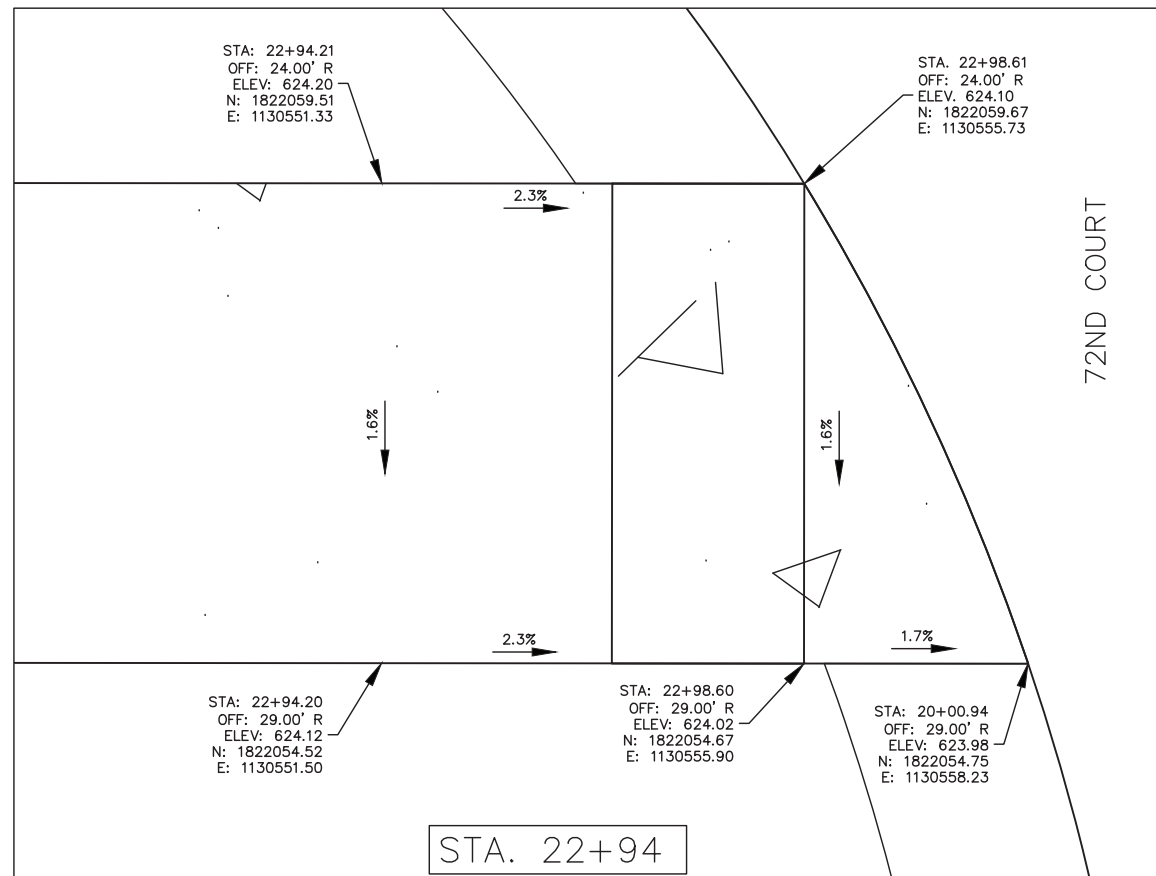
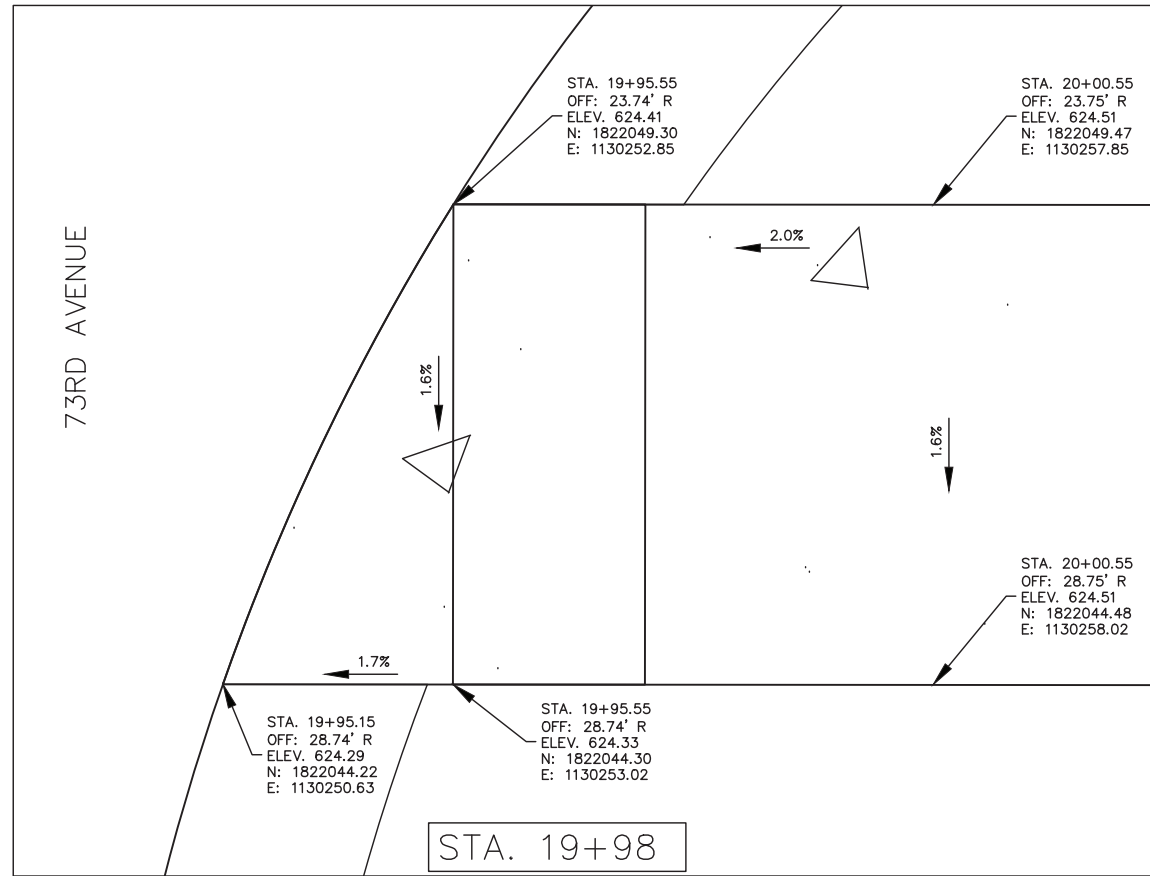
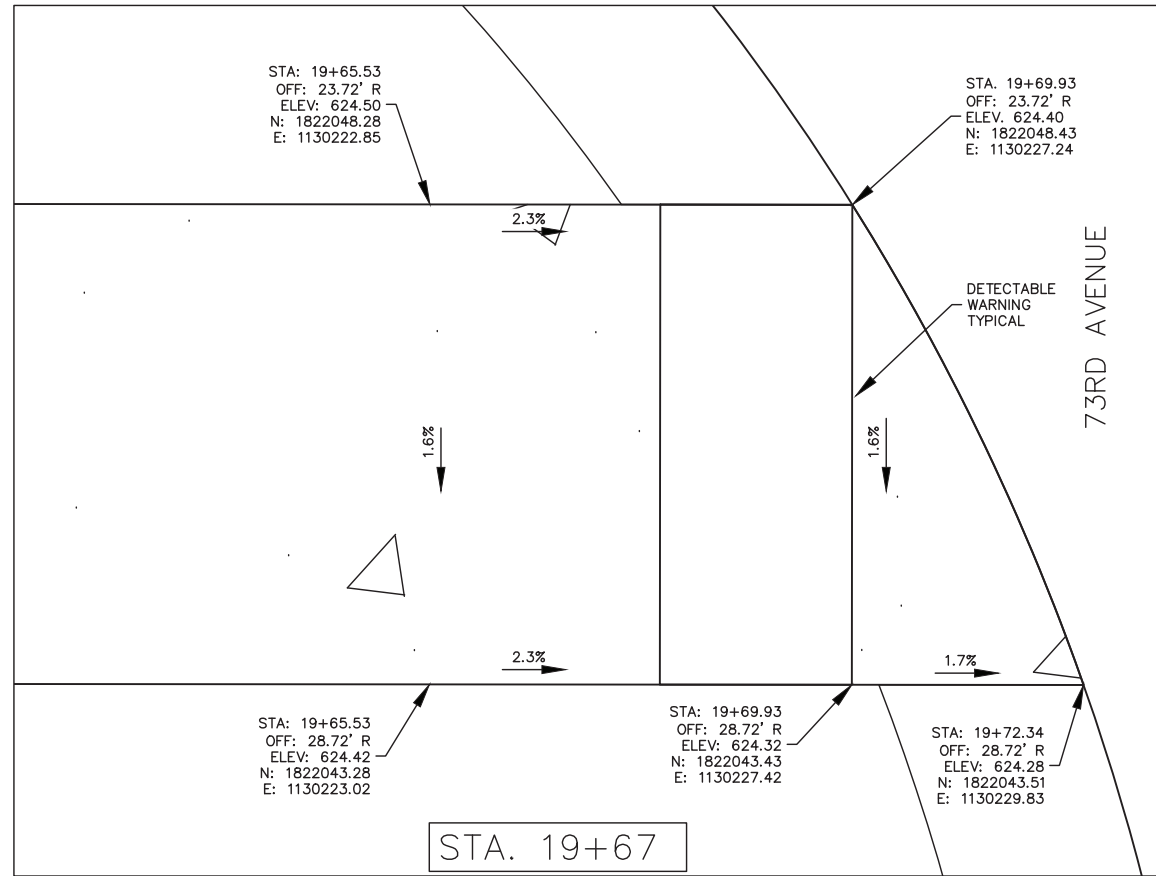
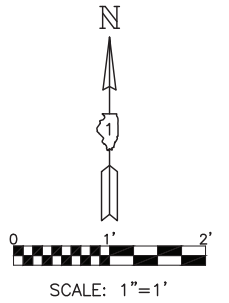
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EXISTING	PROPOSED



PLOT DATE: Jan 26, 2021  
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PLOT SCALE =	DRAWN - DJB	REVISED -			1710	15-00053-00-PV	COOK	46	21
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -			CONTRACT NO. 61G81		ILLINOIS FED. AID PROJECT		

SCALE: 1"=20' SHEET NO. 5 OF SHEETS STA. 22+75 TO STA. 26+00



PLOT DATE: Jan 26, 2021  
FILENAME: H:\20-PA\3005 - 123rd Streets Reconstruction - Phase I\Plans-Eng\Rev-A\20-PH-3005-SITE-2021-01-04.dwg

USER NAME =	DESIGNED - AS	REVISED -
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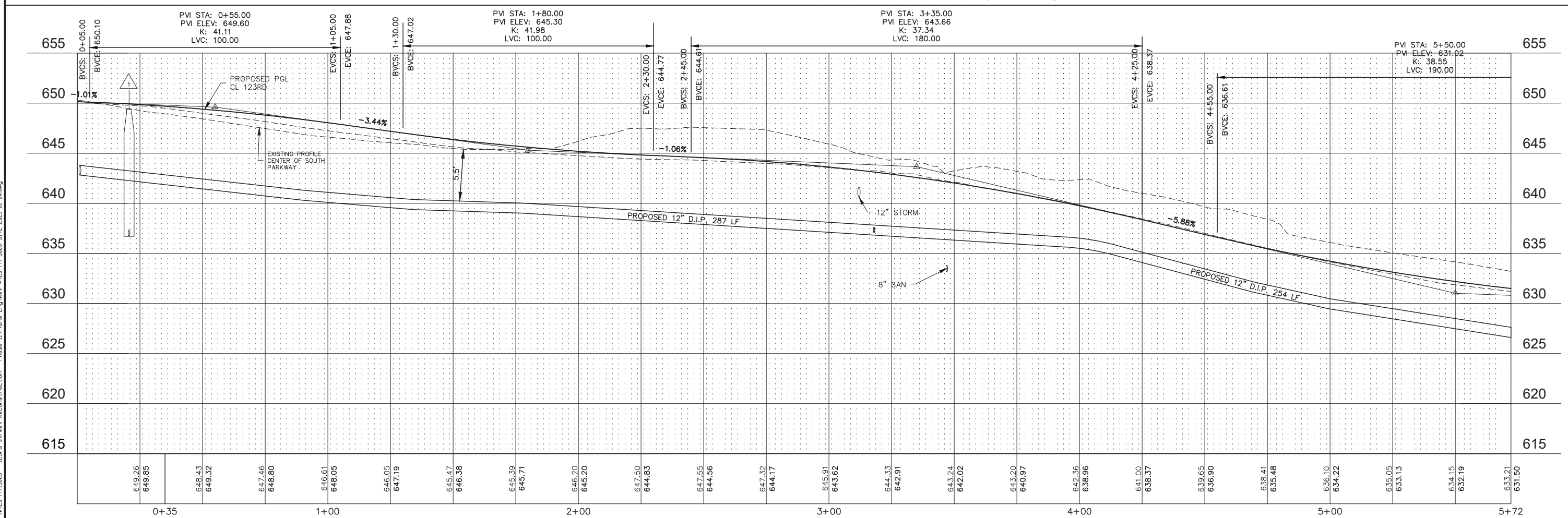
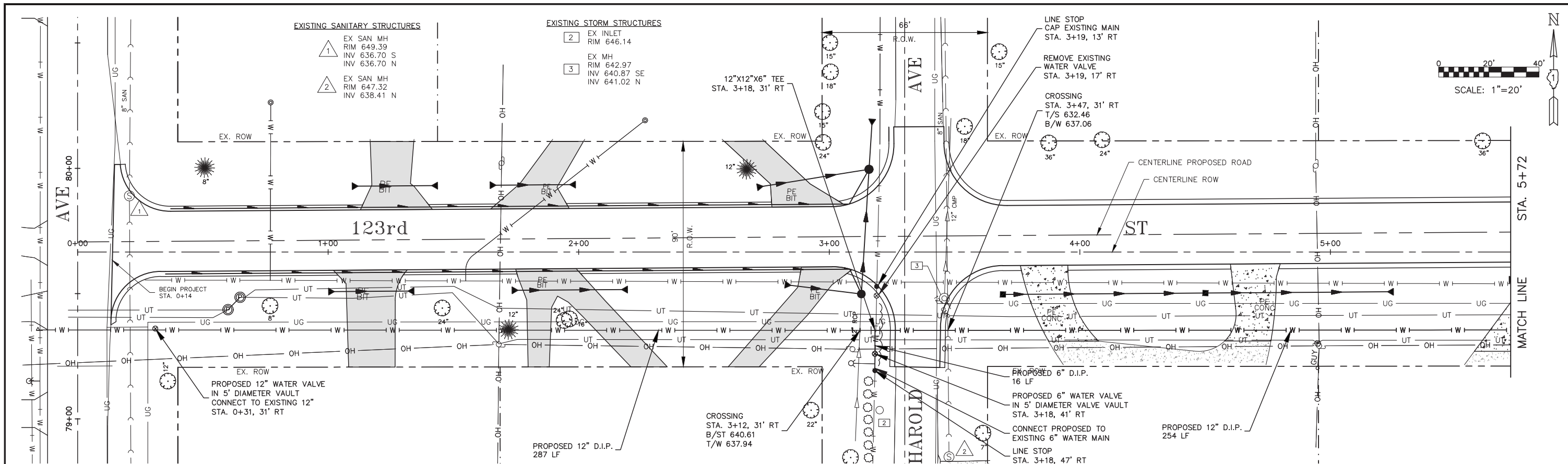
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS**

SCALE: 1"=1'    SHEET NO. 1 OF 1 SHEETS    STA.    TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	22
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	





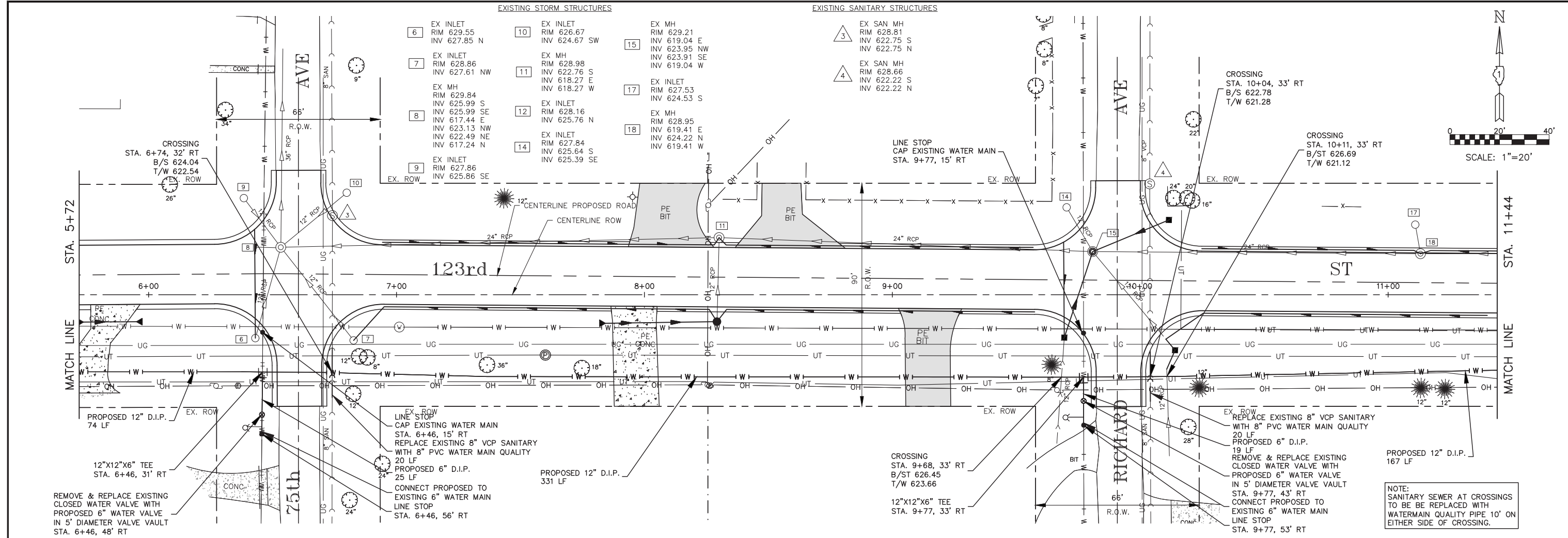
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USER NAME =	DESIGNED - AS	REVISED -
PLOT SCALE =	DRAWN - DJB	REVISED -
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

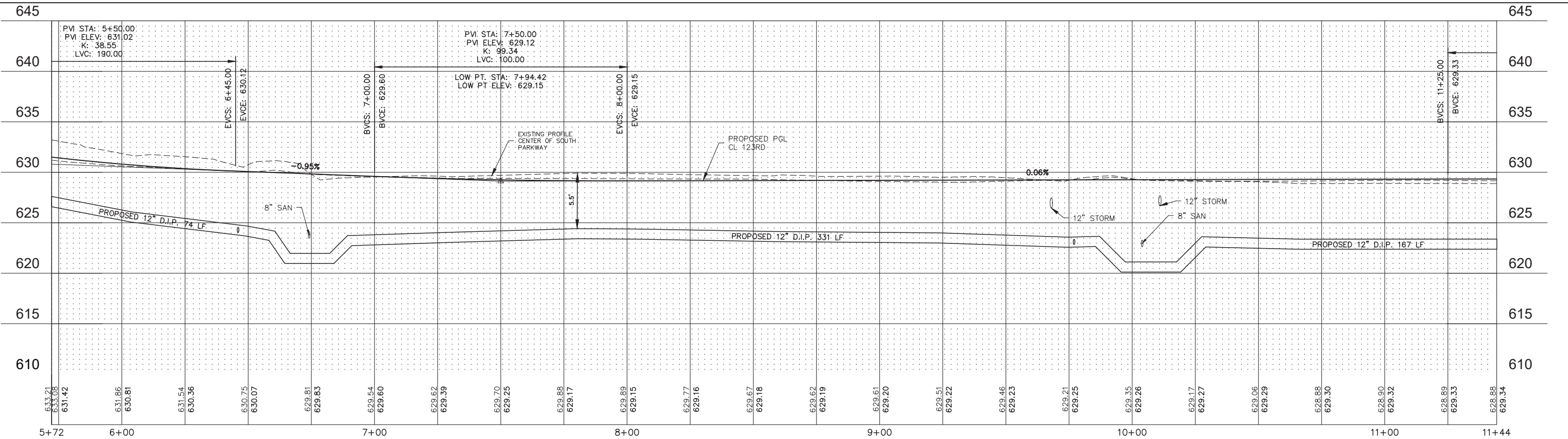
**123RD STREET PLAN AND PROFILE - WATER MAIN**  
 SCALE: 1"=20'    SHEET NO. 1 OF 5 SHEETS    STA. 0+00 TO STA. 5+72

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	23
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	



EXISTING STORM STRUCTURES		EXISTING SANITARY STRUCTURES	
6	EX INLET RIM 629.55 INV 627.85 N	10	EX INLET RIM 626.67 INV 624.67 SW
7	EX INLET RIM 628.86 INV 627.61 NW	11	EX MH RIM 628.98 INV 622.76 S INV 618.27 E INV 618.27 W
8	EX MH RIM 629.84 INV 625.99 S INV 625.99 SE INV 623.13 NW INV 622.49 NE INV 617.24 N	12	EX INLET RIM 628.16 INV 625.76 N
9	EX INLET RIM 627.86 INV 625.86 SE	14	EX INLET RIM 627.84 INV 625.64 S INV 625.39 SE
		15	EX MH RIM 629.21 INV 619.04 E INV 623.95 NW INV 623.91 SE INV 619.04 W
		17	EX INLET RIM 627.53 INV 624.53 S
		18	EX MH RIM 628.95 INV 619.41 E INV 624.22 N INV 619.41 W
		3	EX SAN MH RIM 628.81 INV 622.75 S INV 622.75 N
		4	EX SAN MH RIM 628.66 INV 622.22 S INV 622.22 N

NOTE:  
SANITARY SEWER AT CROSSINGS  
TO BE REPLACED WITH  
WATERMAIN QUALITY PIPE 10' ON  
EITHER SIDE OF CROSSING.



PLOT DATE: Jan 26, 2021  
 FILENAME: H:\20-PA\1005 - 123rd Street Reconstruction - Phase II\Plans-Eng\Rev-A\20-PA-1005-SITE-2021-01.dwg

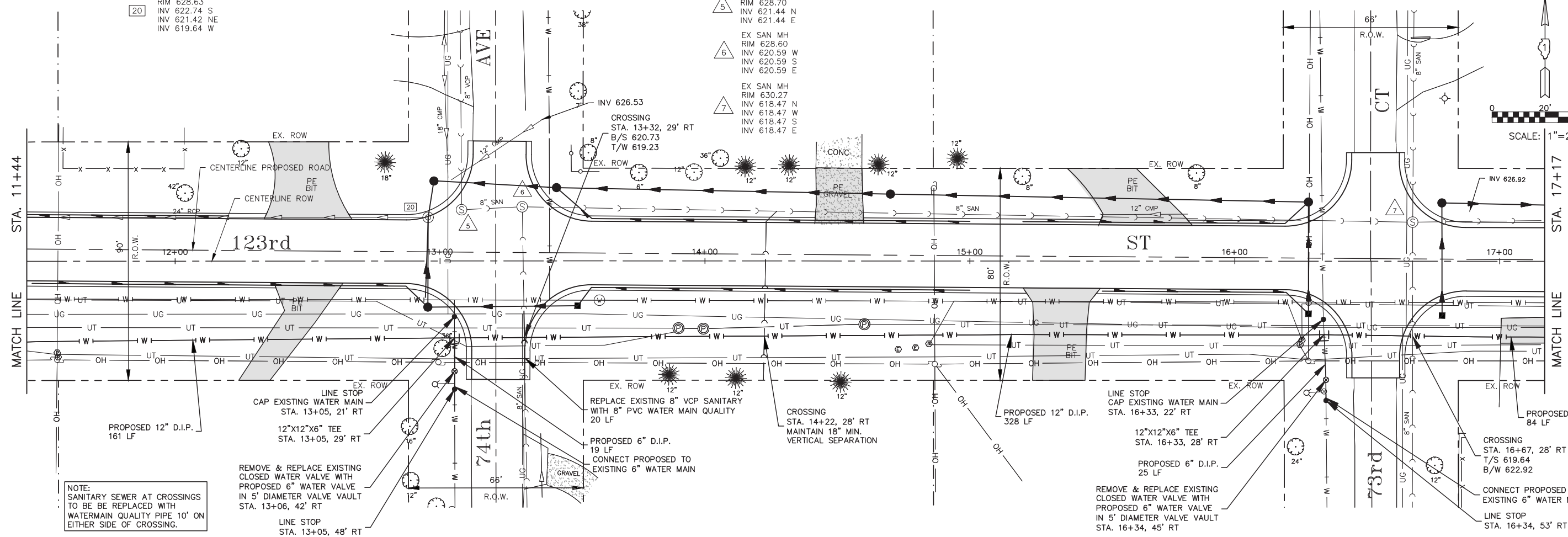
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PLOT SCALE =	DRAWN - DJB	REVISED -		SCALE: 1"=20'	SHEET NO. 2 OF 5 SHEETS	1710	15-00053-00-PV	COOK	46	24
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -		STA. 5+72 TO STA. 11+44	CONTRACT NO. 61G81		ILLINOIS FED. AID PROJECT			
	DATE -	REVISED -								

EXISTING STORM STRUCTURES

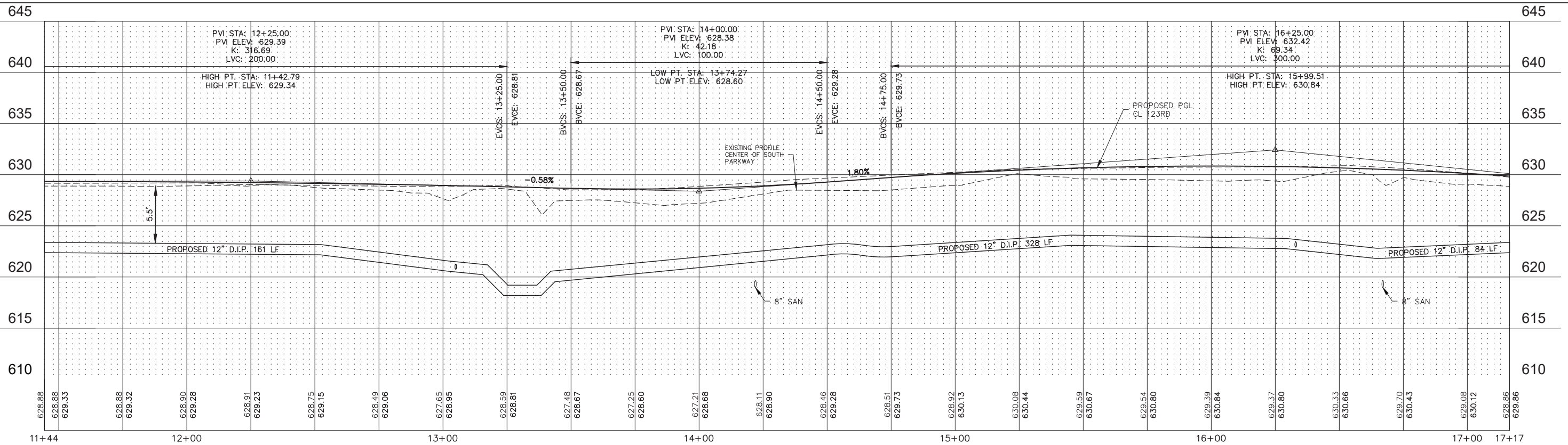
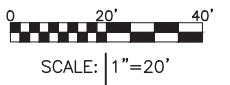
- EX MH
- RIM 628.63
- INV 622.74 S
- INV 621.42 NE
- INV 619.64 W

EXISTING SANITARY STRUCTURES

- EX SAN MH
- RIM 628.70
- INV 621.44 N
- INV 621.44 E
- EX SAN MH
- RIM 628.60
- INV 620.59 W
- INV 620.59 S
- INV 620.59 E
- EX SAN MH
- RIM 630.27
- INV 618.47 N
- INV 618.47 W
- INV 618.47 S
- INV 618.47 E



NOTE:  
SANITARY SEWER AT CROSSINGS  
TO BE REPLACED WITH  
WATERMAIN QUALITY PIPE 10' ON  
EITHER SIDE OF CROSSING.



PLOT DATE: Jan 26, 2021  
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PLOT SCALE =	DRAWN - DJB	REVISED -
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**123RD STREET PLAN AND PROFILE - WATER MAIN**  
 SCALE: 1"=20'    SHEET NO. 3 OF 5 SHEETS    STA. 11+44 TO STA. 17+17

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	25
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	

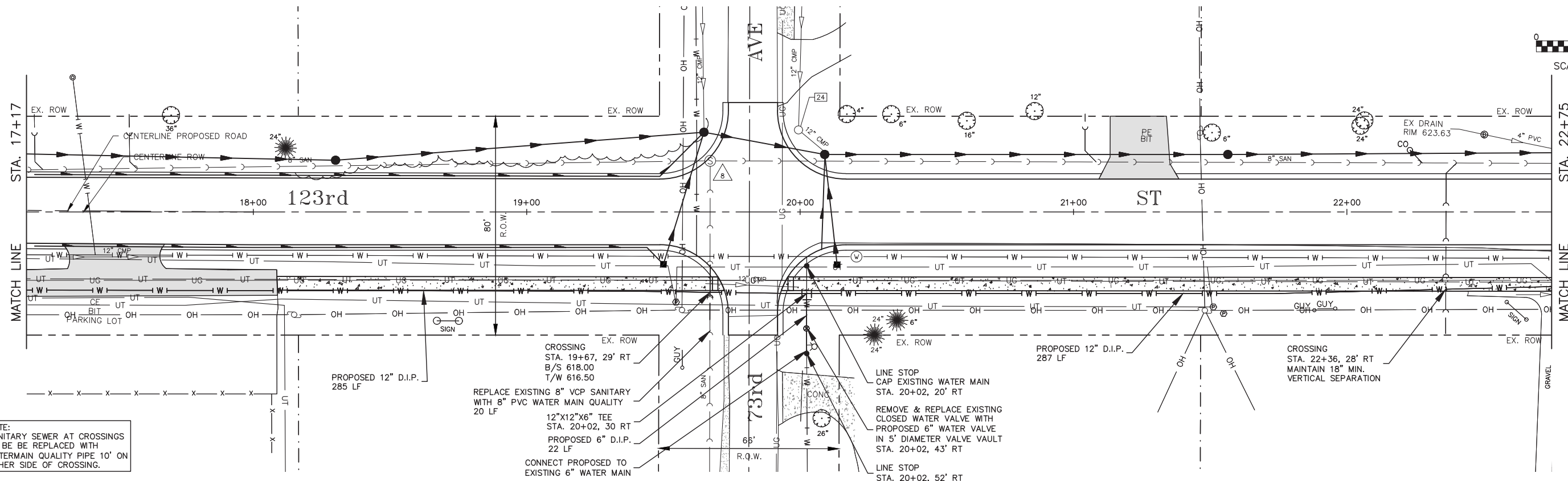
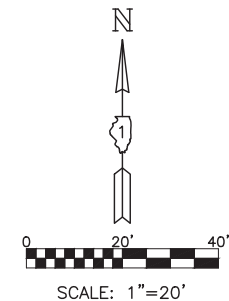


EXISTING STORM STRUCTURES

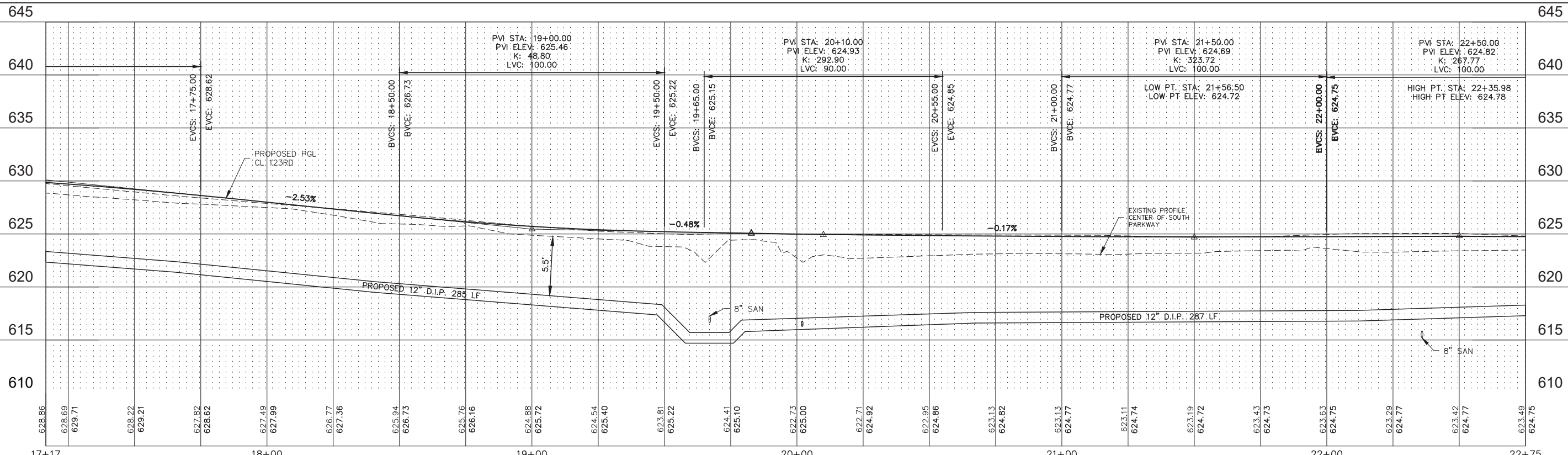
EX INLET  
RIM 623.37  
INV 620.92 W  
INV 620.87 N  
INV 620.77 SE

EXISTING SANITARY STRUCTURES

EX SAN MH  
RIM 624.32  
INV 616.61 S  
INV 616.61 W  
INV 616.61 E



NOTE:  
SANITARY SEWER AT CROSSINGS  
TO BE REPLACED WITH  
WATERMAIN QUALITY PIPE 10' ON  
EITHER SIDE OF CROSSING.



PLOT DATE: Jan 26, 2021  
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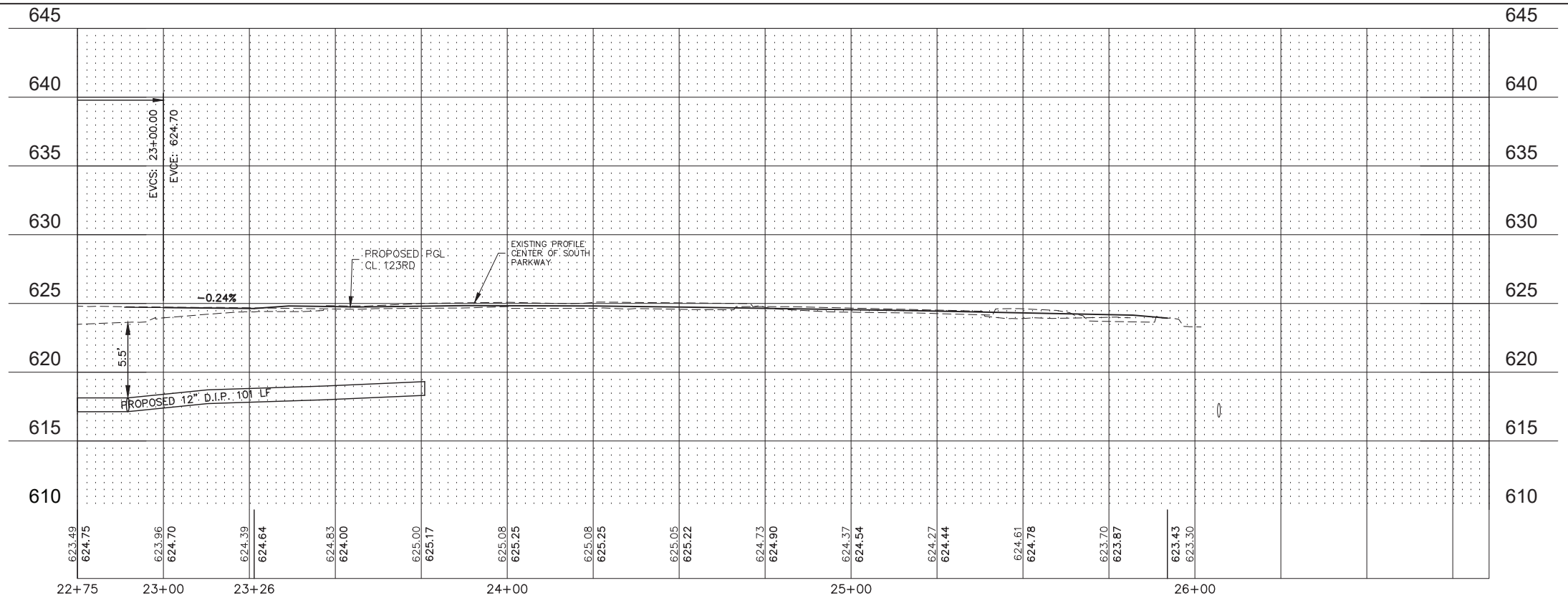
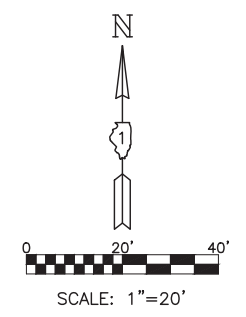
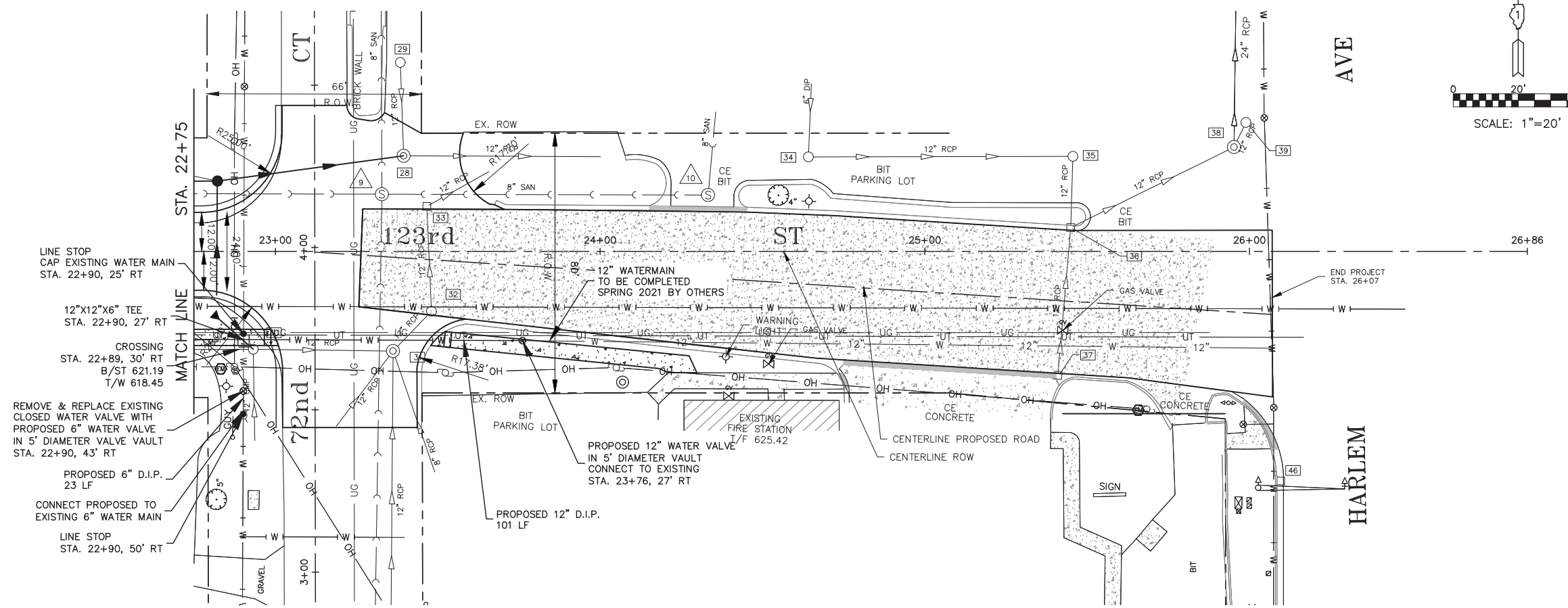
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PLOT SCALE =	DRAWN - DJB	REVISED -		SCALE: 1"=20'		1710	15-00053-00-PV	COOK	46	26
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -		SHEET NO. 4 OF 5 SHEETS		STA. 17+17 TO STA. 22+75		CONTRACT NO. 61G81		
	DATE -	REVISED -		STA. 17+17 TO STA. 22+75		ILLINOIS FED. AID PROJECT				

EXISTING STORM STRUCTURES

- |    |   |    |  |
|----|---|----|--|
| 27 | EX INLET<br>RIM 622.76<br>INV 619.16 W<br>INV 618.96 E  | 36 | EX CURB INLET<br>RIM 623.88<br>INV 618.48 N<br>INV 618.83 S<br>INV 618.48 NE |
| 28 | EX MH<br>RIM 624.16<br>INV 617.77 W<br>INV 618.41 N<br>INV 617.40 E                                   | 37 | EX CURB INLET<br>RIM 623.82<br>INV 619.02 N                                  |
| 29 | EX INLET<br>RIM 623.53<br>INV 618.83 S  | 38 | EX MH<br>RIM 624.13<br>INV 617.63 SW<br>INV 617.63 NE                        |
| 30 | EX INLET<br>RIM 623.60<br>INV 621.35 W<br>INV 621.50 S<br>INV 621.35 E                                | 39 | EX INLET<br>RIM 624.01<br>INV 621.44 SW                                      |
| 31 | EX MH<br>RIM 624.33<br>INV 618.68 W<br>INV 618.33 SW<br>INV 618.53 S<br>INV 618.53 S<br>INV 617.83 NE |    |  |
| 32 | EX INLET<br>RIM 624.39<br>INV 618.44 SW<br>INV 618.44 N   |    |  |
| 33 | EX CURB INLET<br>RIM 624.27<br>INV 618.07 S<br>INV 618.07 NE  |    |  |
| 34 | EX INLET<br>RIM 624.04<br>INV 619.94 N<br>INV 620.09 E  |    |  |
| 35 | EX INLET<br>RIM 623.75<br>INV 619.35 W<br>INV 619.35 S  |    |  |

EXISTING SANITARY STRUCTURES

- |    |   |
|----|---|
| 9  | EX SAN MH<br>RIM 624.24<br>INV 615.20 W<br>INV 615.20 S<br>INV 619.22 E<br>INV 615.20 N |
| 10 | EX SAN MH<br>RIM 624.48<br>INV 620.37 N<br>INV 620.37 W                                 |



PLOT DATE: Jan 26, 2021  
 FILENAME: H:\20-PA\3005 - 123rd Street Reconstruction - Phase I\Plans-Eng\Rev-A\20-PA-3005-SITE-2021-01.dwg

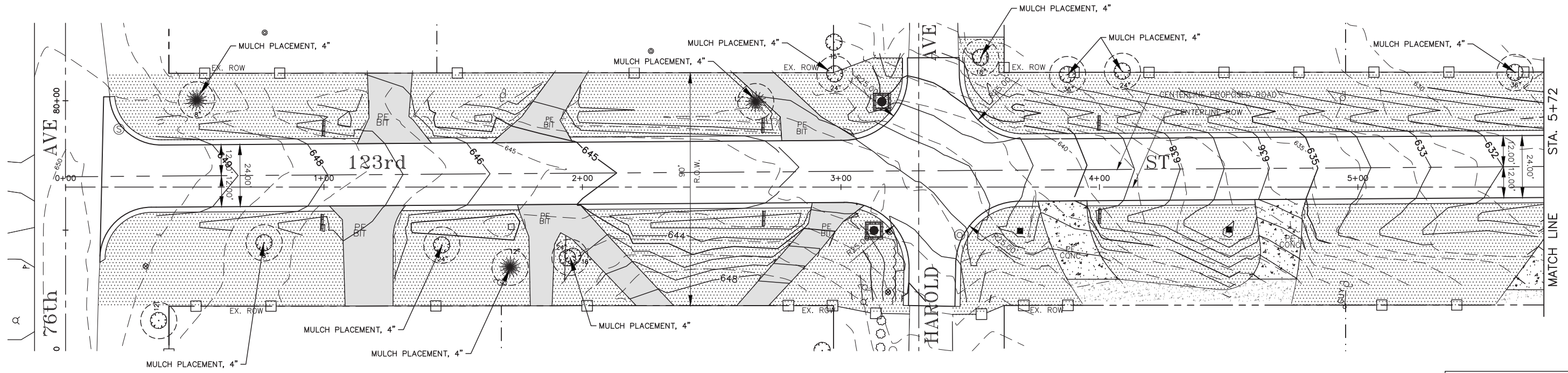
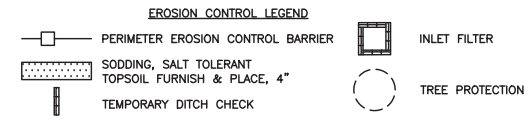
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	DATE -

REVISED -	REVISED -
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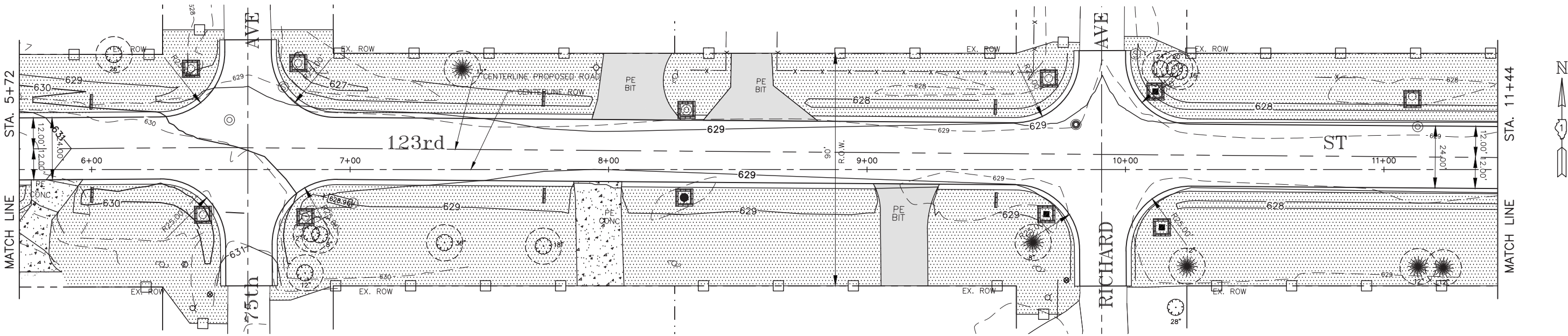
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**123RD STREET PLAN AND PROFILE - WATER MAIN**  
 SCALE: 1"=20'    SHEET NO. 5 OF 5 SHEETS    STA. 22+75 TO STA. 26+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	27
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	



NOTE:  
CONTRACTOR SHALL PROVIDE SPADED EDGES FOR ALL SODDING AREAS ABUTTING EXISTING TREES BY MAINTAINING A MULCH RING, 5' DIAMETER MINIMUM, AROUND THE TREE.



**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GRADING AND EROSION CONTROL PLAN**

USER NAME =	DESIGNED - AS	REVISED -
PLOT SCALE =	DRAWN - DJB	REVISED -
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -
	DATE -	REVISED -

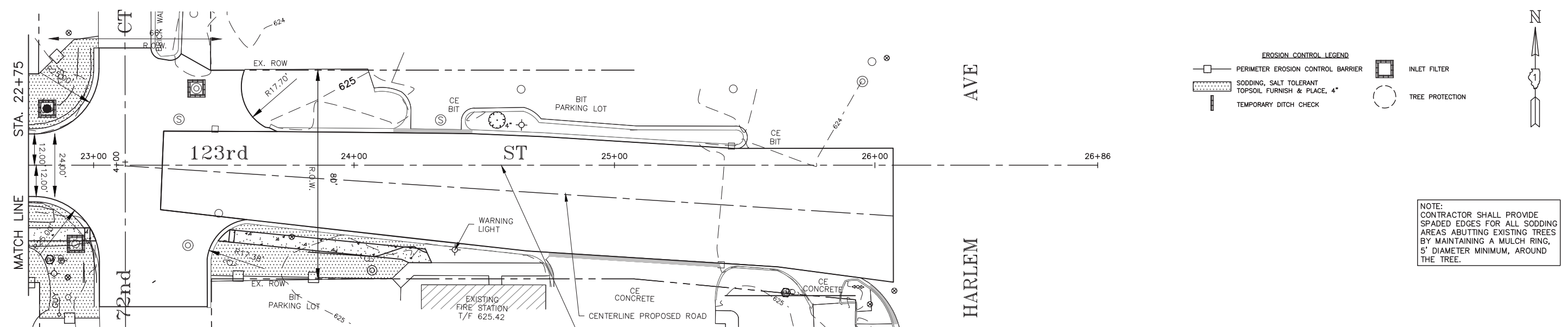
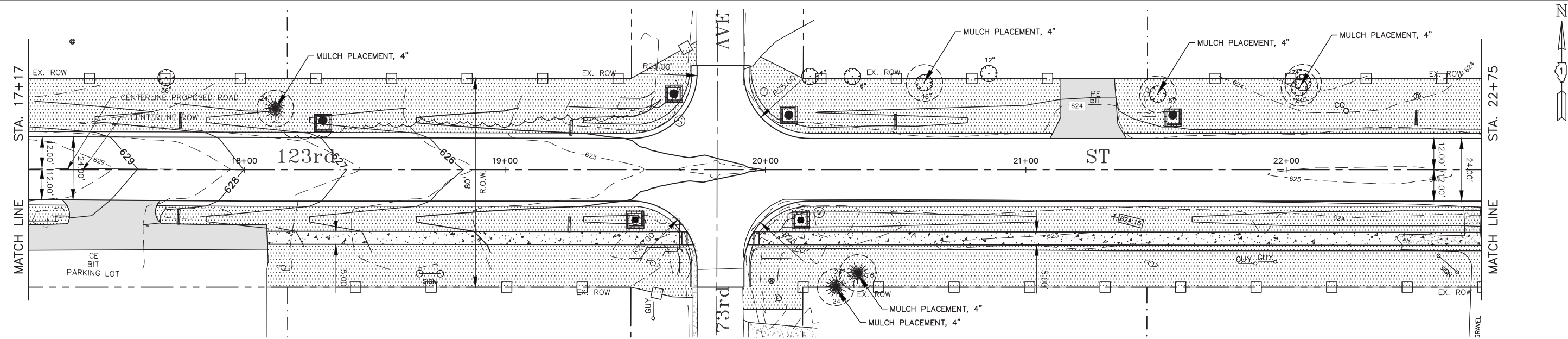
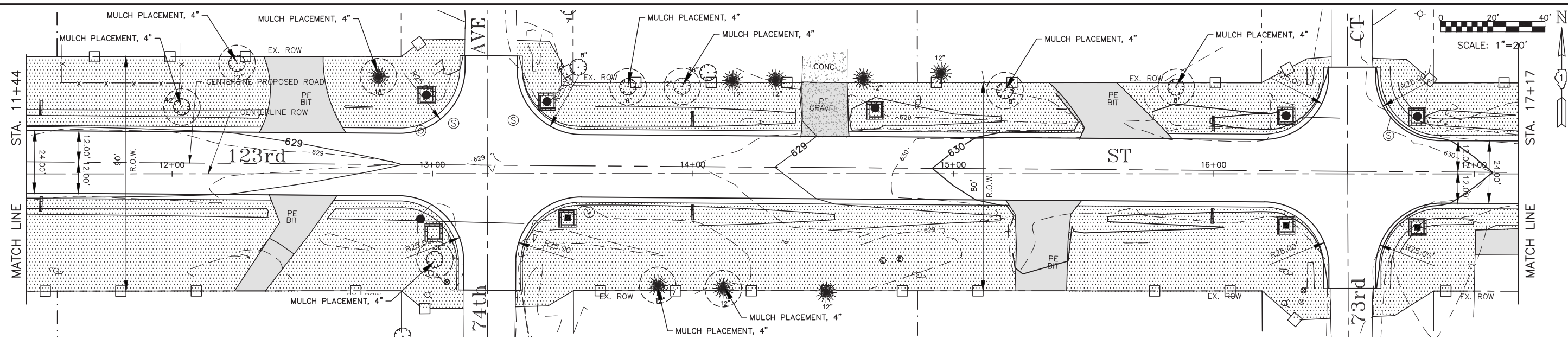
SCALE: 1"=20'    SHEET NO. 1 OF 2 SHEETS    STA. 0+00 TO STA. 11+44

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	28
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	

PLOT DATE: Jan 26, 2021  
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PLOT DATE: Jan 26, 2021  
 FILENAME: H:\20-PAV\3005 - 123rd Street Reconstruction - Phase I\Plans-Eng\Rev-A\20-PH-3005-SITE-2021-01-04.dwg



**EROSION CONTROL LEGEND**

	PERIMETER EROSION CONTROL BARRIER		INLET FILTER
	SODDING, SALT TOLERANT TOPSOIL FURNISH & PLACE, 4"		TREE PROTECTION
	TEMPORARY DITCH CHECK		

NOTE:  
 CONTRACTOR SHALL PROVIDE SPADED EDGES FOR ALL SODDING AREAS ABUTTING EXISTING TREES BY MAINTAINING A MULCH RING, 5' DIAMETER MINIMUM, AROUND THE TREE.

USER NAME =	DESIGNED - AS	REVISED -
PLOT SCALE =	DRAWN - DJB	REVISED -
PLOT DATE = Jan 26, 2021	CHECKED - AS	REVISED -
	DATE -	REVISED -

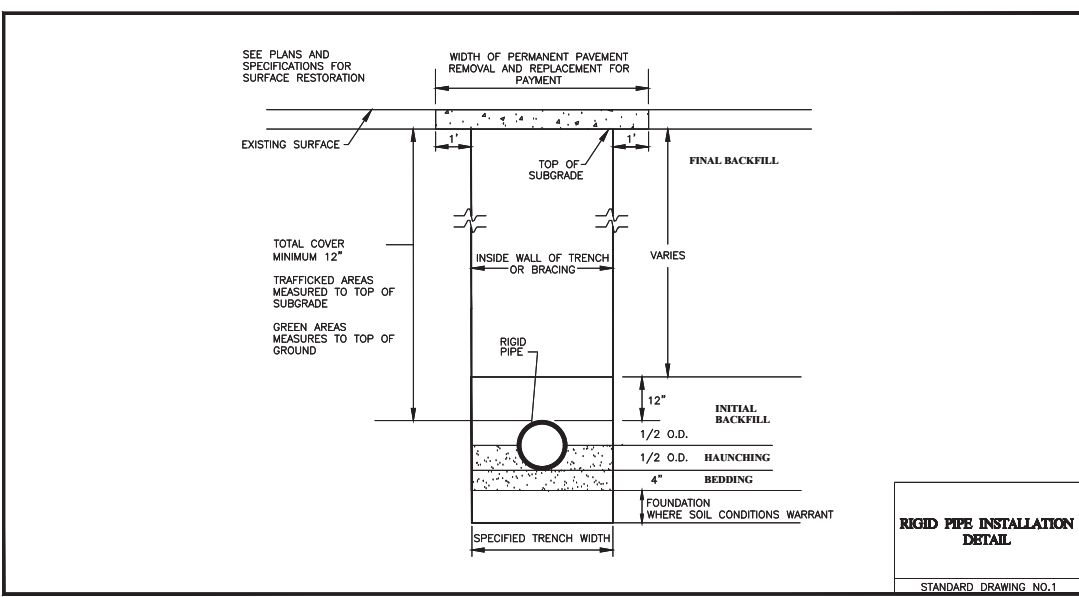
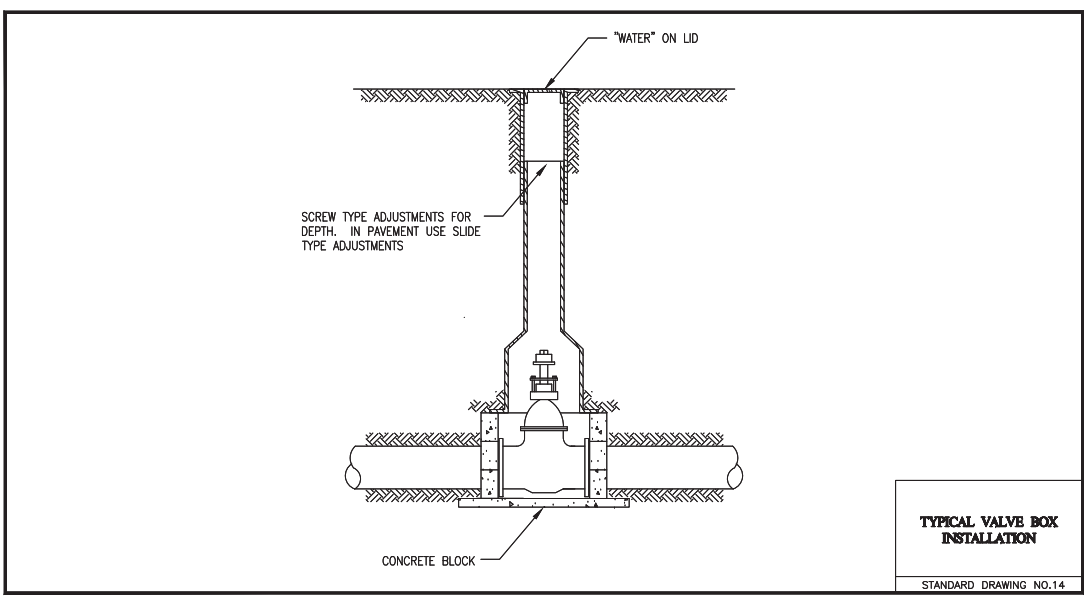
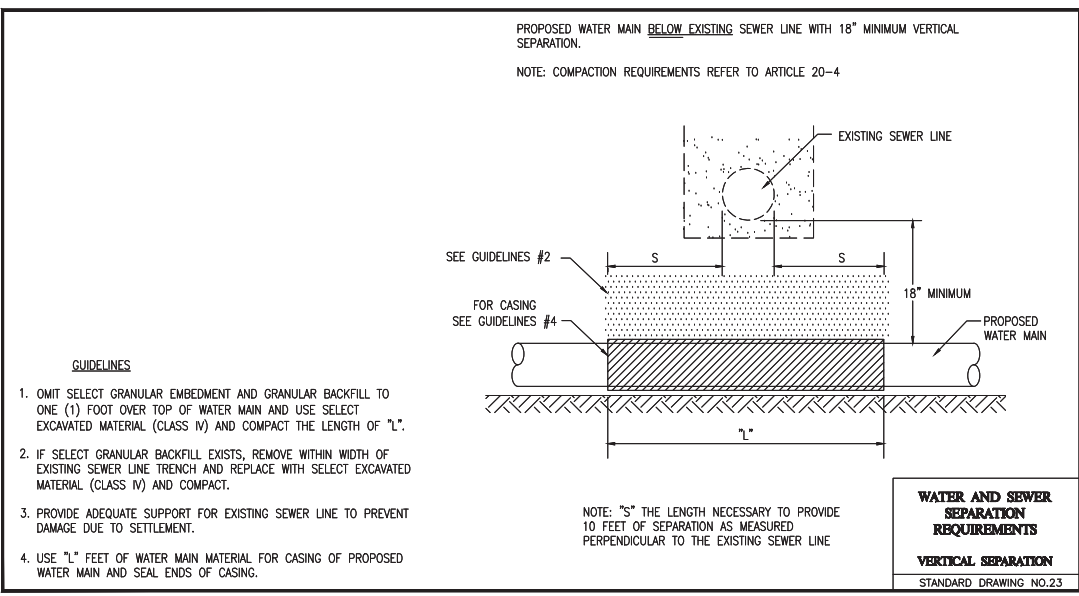
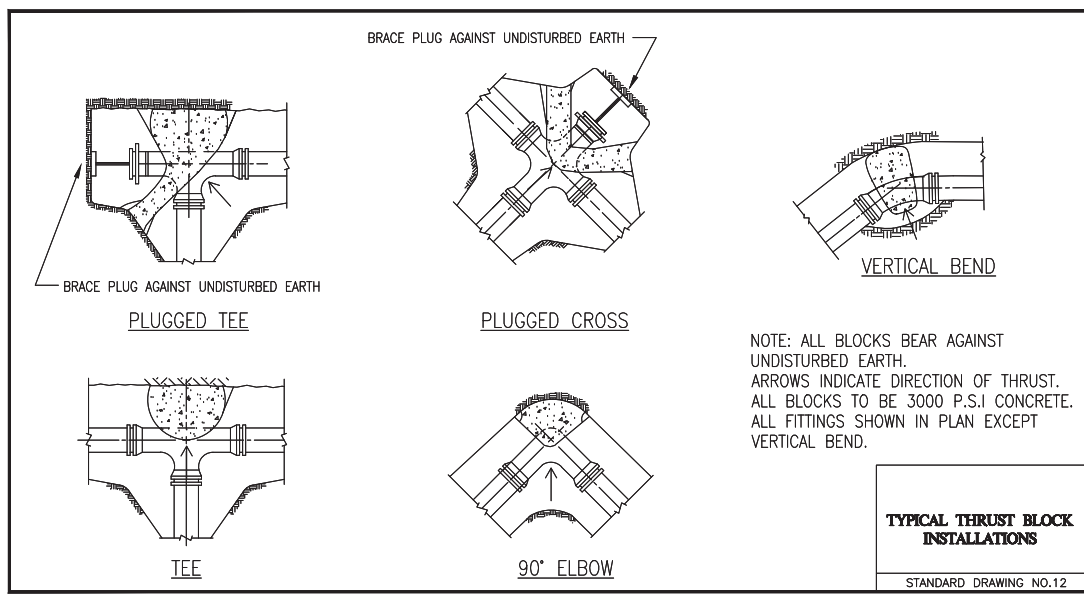
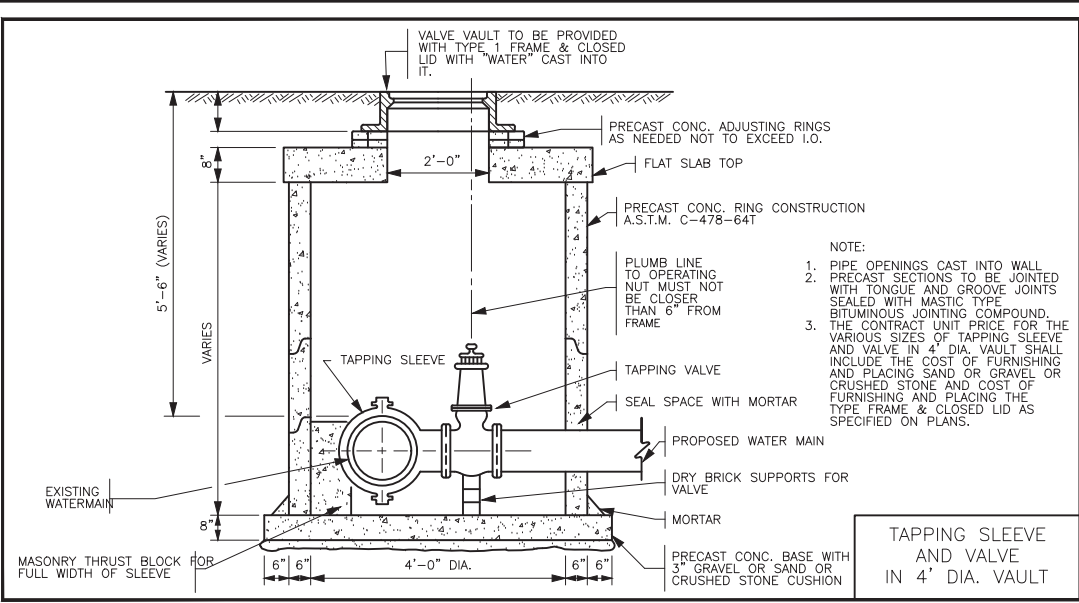
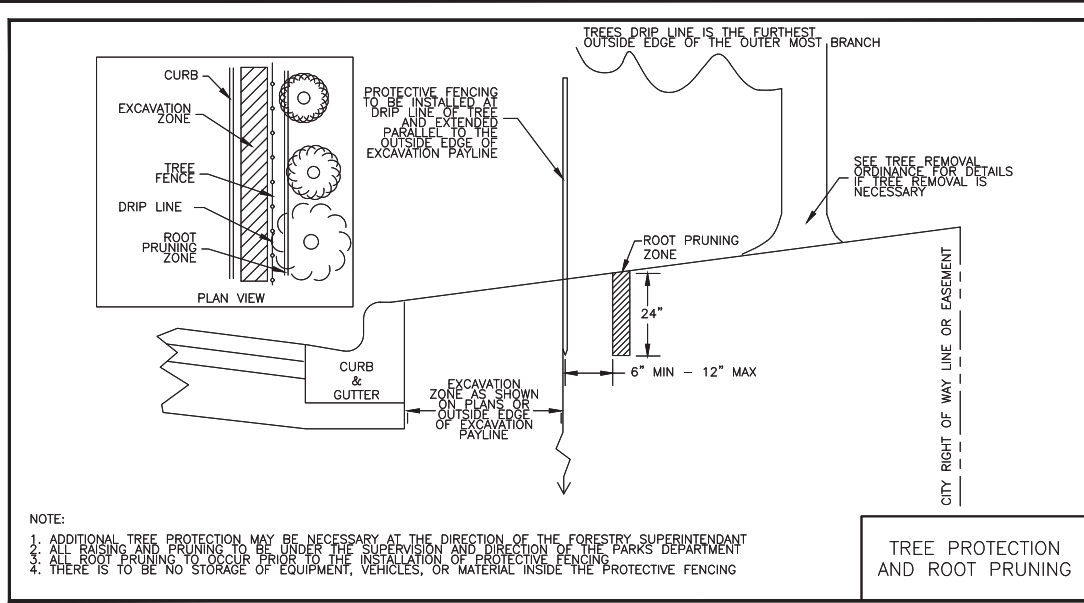
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**GRADING AND EROSION CONTROL PLAN**

SCALE: 1"=20'    SHEET NO. 2 OF 2 SHEETS    STA. 11+44 TO STA. 26+86

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	29
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	

PLOT DATE: Jan 26, 2021  
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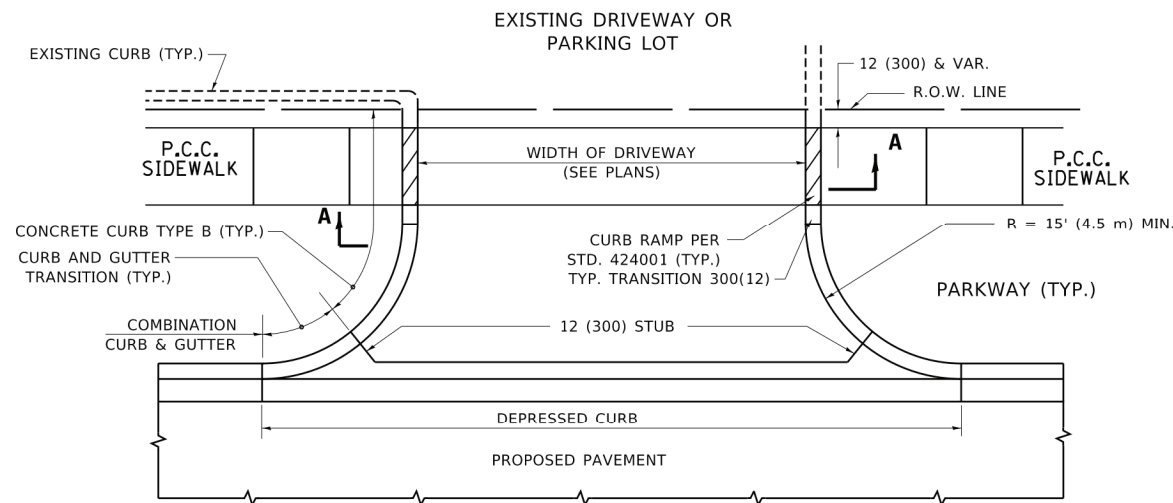
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	DRAWN - DJB	REVISED -
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PLOT DATE = Jan 26, 2021	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

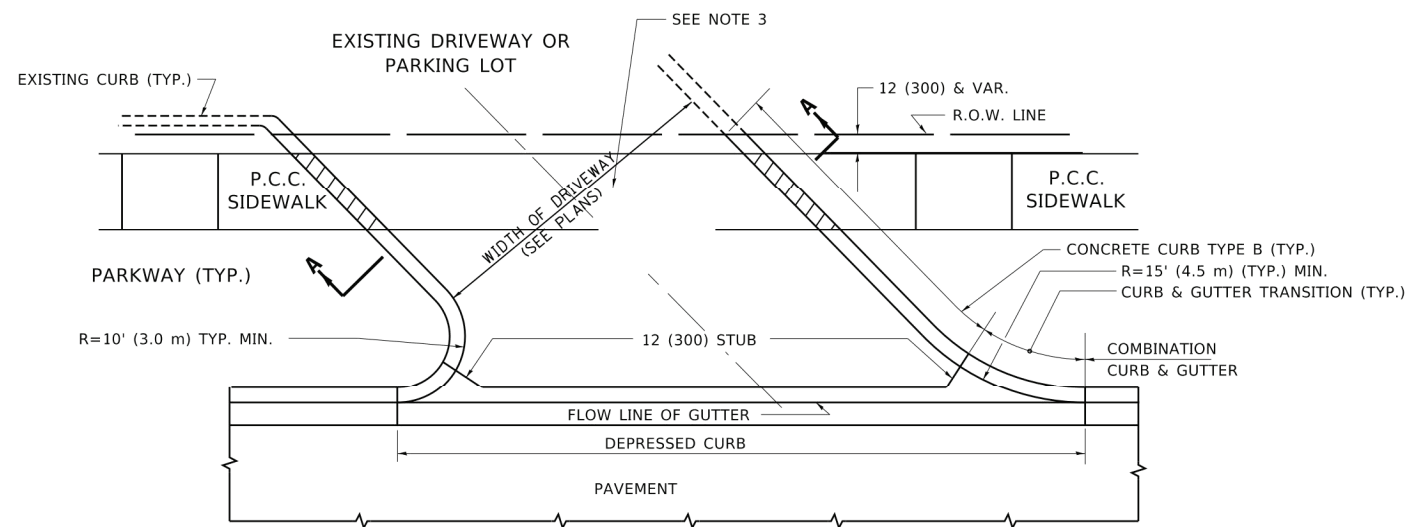
**DETAIL SHEET**

SCALE: — SHEET NO. 1 OF 1 SHEETS STA. TO STA.

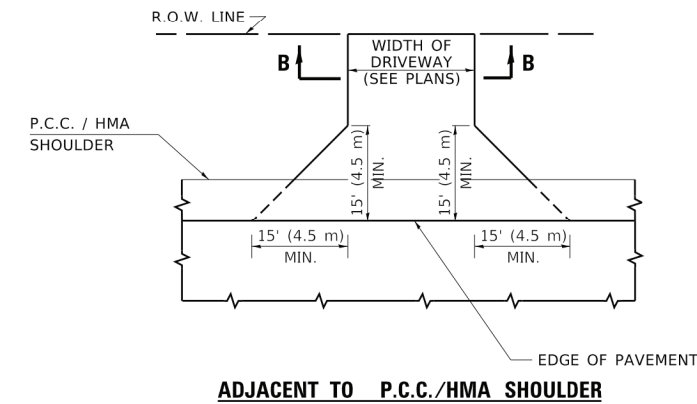
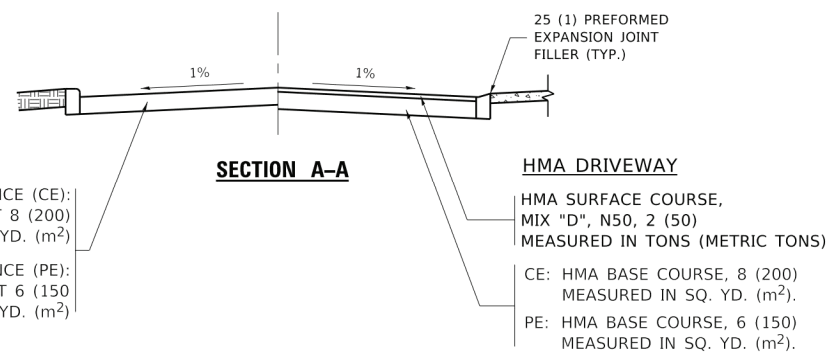
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CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	



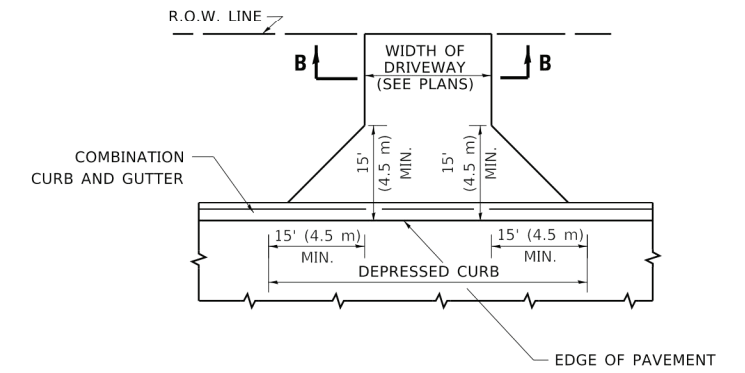
**WITH CONCRETE CURB, TYPE B**



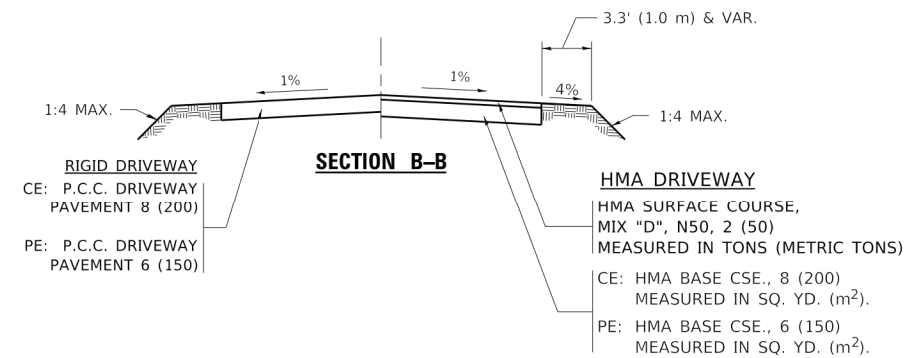
**WITH CONCRETE CURB, TYPE B**



**ADJACENT TO P.C.C./HMA SHOULDER**



**ADJACENT TO CURB AND GUTTER**



**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

PLOT DATE: Jan 26, 2021  
FILENAME: H:\20-PA\3005 - 123rd Street Reconstruction - Phase I\NPlans-Eng\Rev-A\20-PH-3005-TITLE-NOTES-DETAILS-2021-01-25.dwg

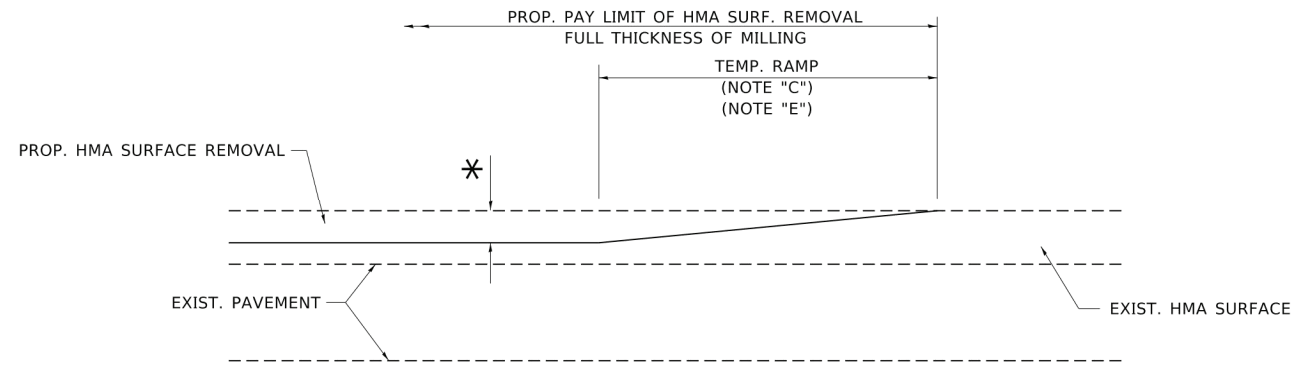
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PLOT DATE = 3/27/2019	CHECKED -	REVISED - R. BORO 06-11-08
	DATE - 11-04-95	REVISED - R. BORO 09-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB &amp; EDGE OF SHOULDER ≥ 15'(4.5m)</b>			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	31
BD400-01 (BD-01)			CONTRACT NO. 61G81	
ILLINOIS   FED. AID PROJECT				

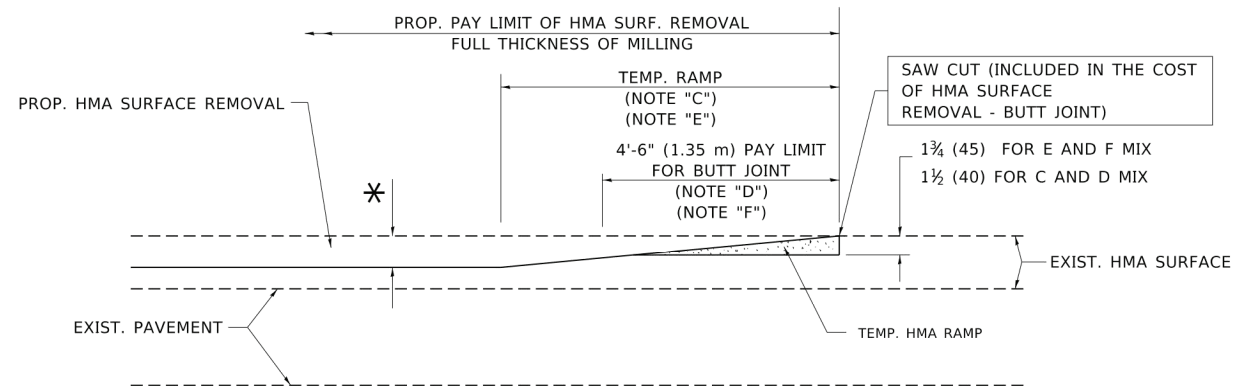




**MILLED TEMPORARY RAMP**

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 1**

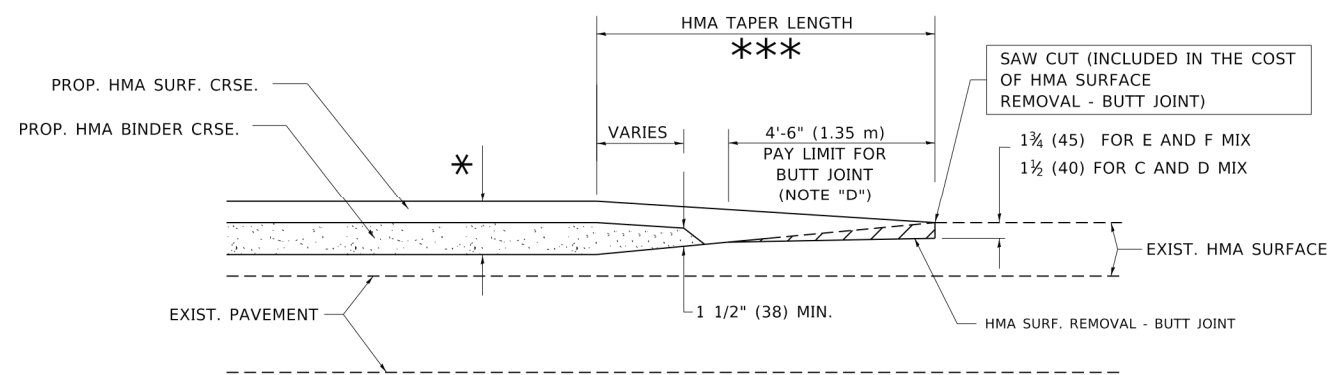


**HMA CONSTRUCTED TEMPORARY RAMP**

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

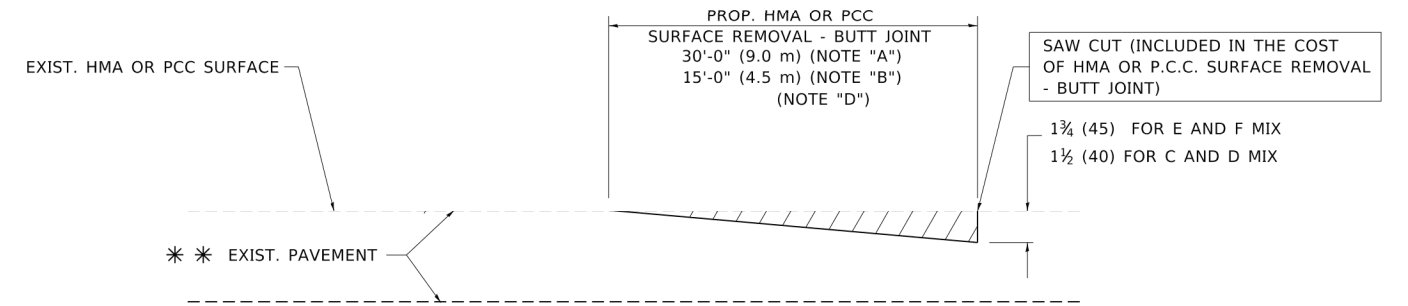
**OPTION 2**

**TYPICAL TEMPORARY RAMP**

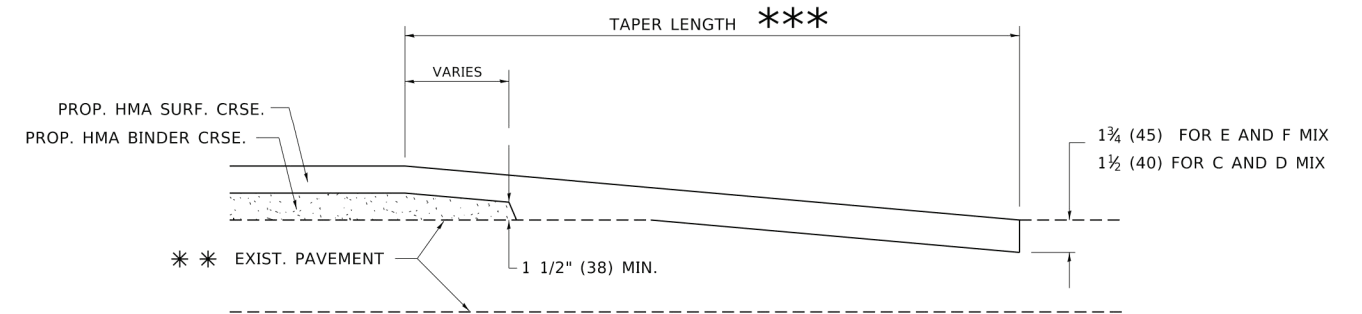


**BUTT JOINT AND HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

PLOT DATE: Jan 26, 2021  
FILENAME: H:\20-PA\1\3005 - 123rd Street Reconstruction - Phase I\NPlans-Eng\Rev-A\20-PH-3005-TITLE-NOTES-DETAILS-2021-01-25.dwg

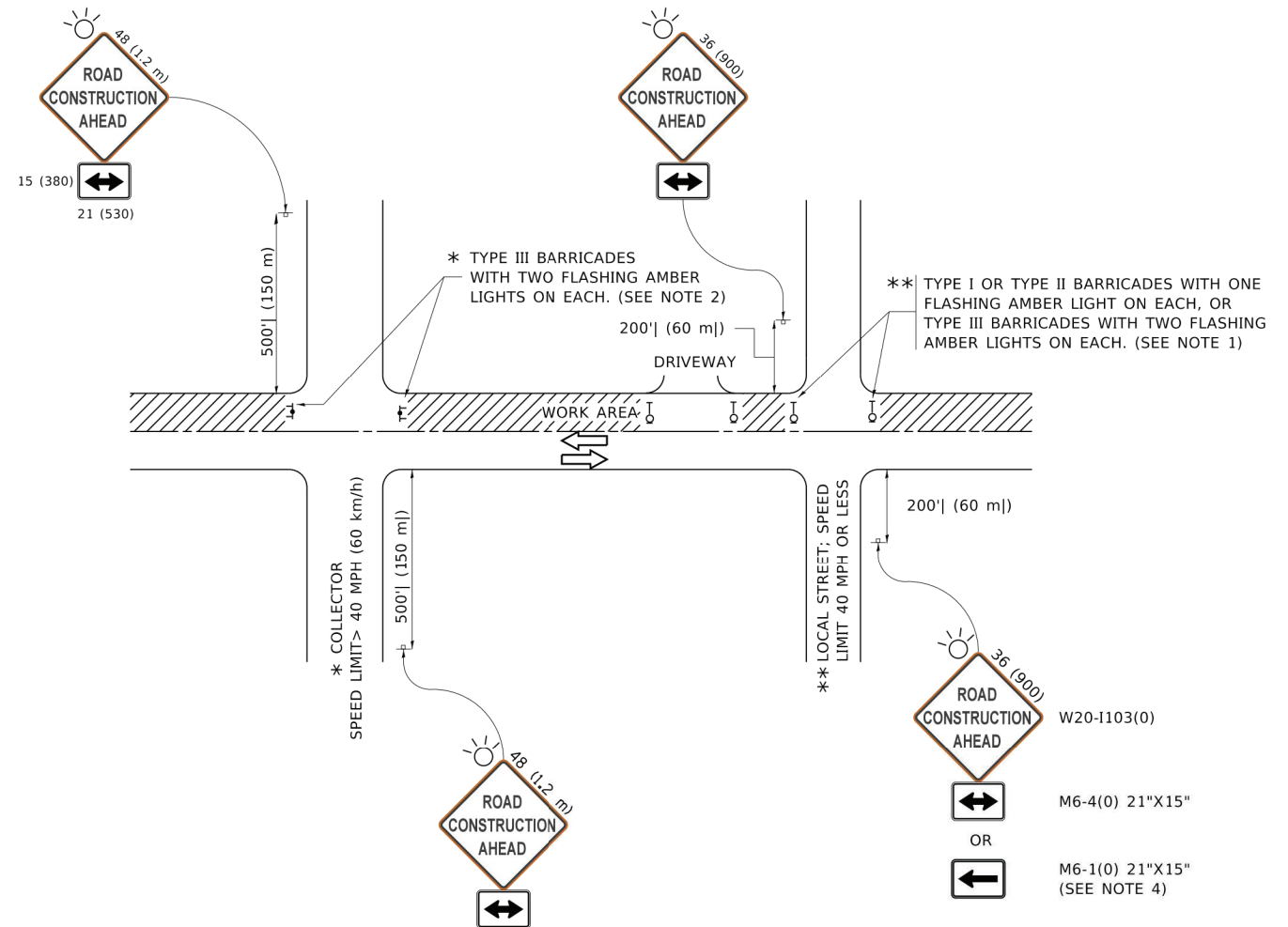
USER NAME = footemj	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
PLOT SCALE = 50.0000' / in.	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT DATE = 3/27/2019	CHECKED -	REVISED - M. GOMEZ 04-06-01
	DATE - 06-13-90	REVISED - R.BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	32
<b>BD400-05 BD32</b>		CONTRACT NO. 61G81		
ILLINOIS FED. AID PROJECT				

PLOT DATE: Jan 26, 2021  
 FILENAME: H:\20-PA\3005 - 123rd Street Reconstruction - Phase I\Plans-Eng\Rev-A\20-PA-3005-TITLE-NOTES-DETAILS-2021-01-25.dwg



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

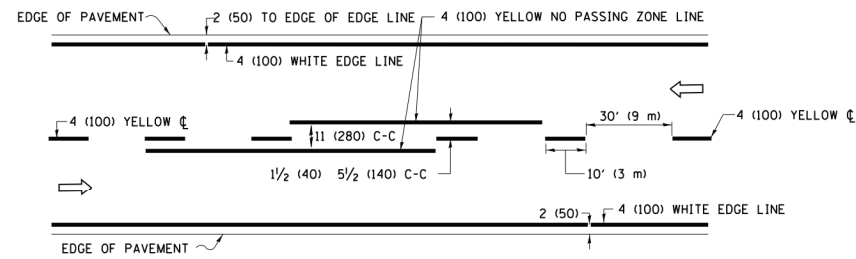
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

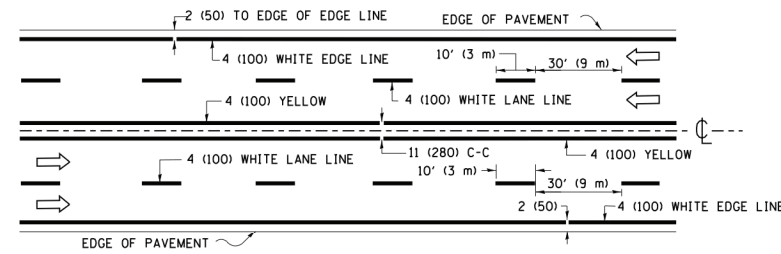
USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
PLOT SCALE = 50.0000' / in.	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT DATE = 3/4/2019	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

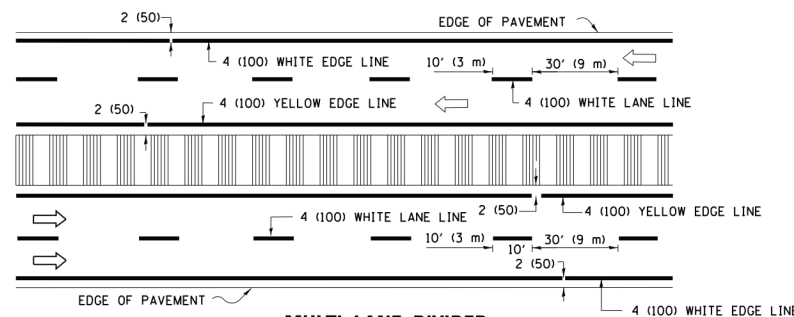
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1710	15-00053-00-PV	COOK	46	33
<b>TC-10</b>			CONTRACT NO. 61G81	
ILLINOIS   FED. AID PROJECT				



**2-LANE ROADWAY**

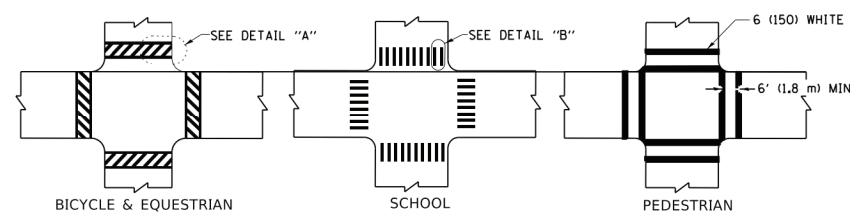


**MULTI-LANE UNDIVIDED**



**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

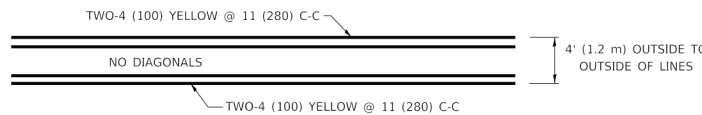


**DETAIL "A"**

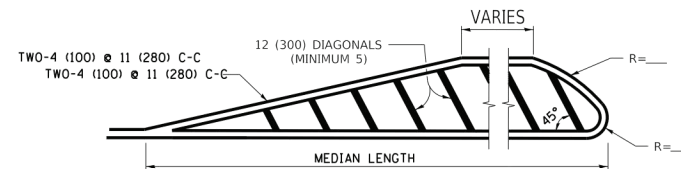
**DETAIL "B"**

**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

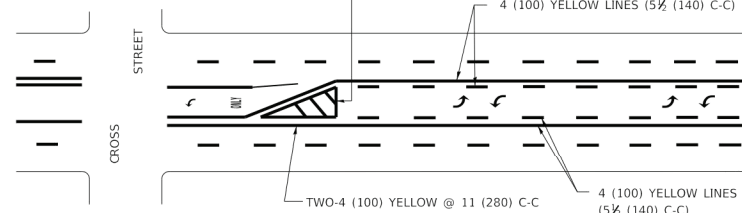


**4' (1.2 m) WIDE MEDIANS ONLY**



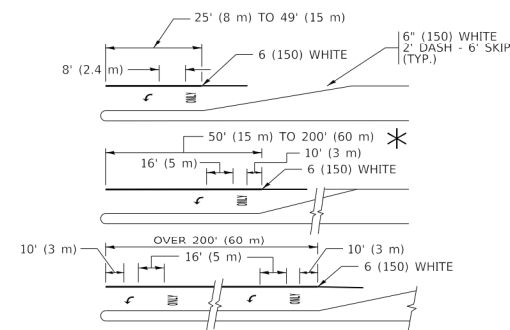
**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

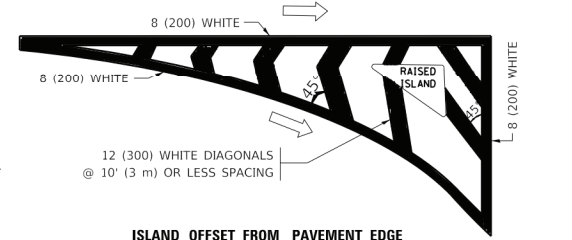
**TYPICAL PAINTED MEDIAN MARKING**



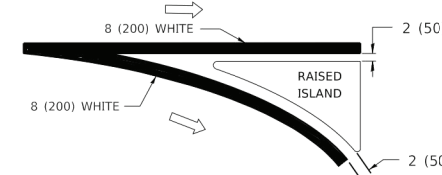
**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - \*ONLY\* INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - \*ONLY\*.

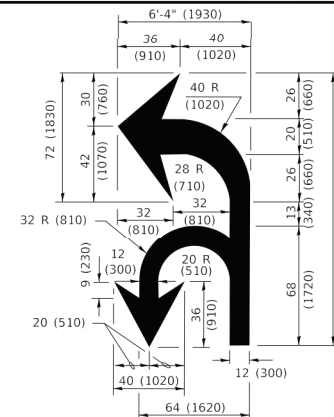


**ISLAND OFFSET FROM PAVEMENT EDGE**

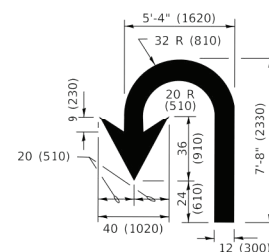


**ISLAND AT PAVEMENT EDGE**

**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "RR" IS 6' (1.8 m) EACH "X" IS 54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

PLOT DATE: Jan 06, 2021; FILENAME: H:\20-Ph\3005 - 123rd Street Reconstruction - Phase 1\Plans-Eng\Rev-A\20-Ph-3005-TITLE-NOTES-DETAILS-2021-01-06.dwg

USER NAME = footemj	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
PLOT SCALE = 50.0000' / in.	DRAWN -	REVISED - C. JUCIUS 07-01-13
PLOT DATE = 3/4/2019	CHECKED - C. JUCIUS 12-21-15	REVISED - C. JUCIUS 04-12-16
	DATE - 03-19-90	

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	34
TC-13			CONTRACT NO. 61G81	
ILLINOIS FED. AID PROJECT				



PLOT DATE: Jan 05, 2021  
 FILENAME: H:\20-Ph3005 - 122nd Street Reconstruction - Phase 1\Plans-Eng\Rev-A\20-Ph-3005-TITLE-NOTES-DET-MAILS-2021-01-05.dwg

**ROUTE MARKERS**

FOR U.S. ROUTES  
M1-40-2424

FOR ILLINOIS ROUTES  
M1-50-2424

R.R. UNMARKED ROUTES  
SPECIAL 24" x 18" VARIABLE  
4" BLACK LETTERS ON WHITE  
REFLECTIVE BACKGROUND

**ARROWS SIGNS**

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-1-2115

M6-3-2115

**CARDINAL DIRECTION & DETOUR SIGNS**

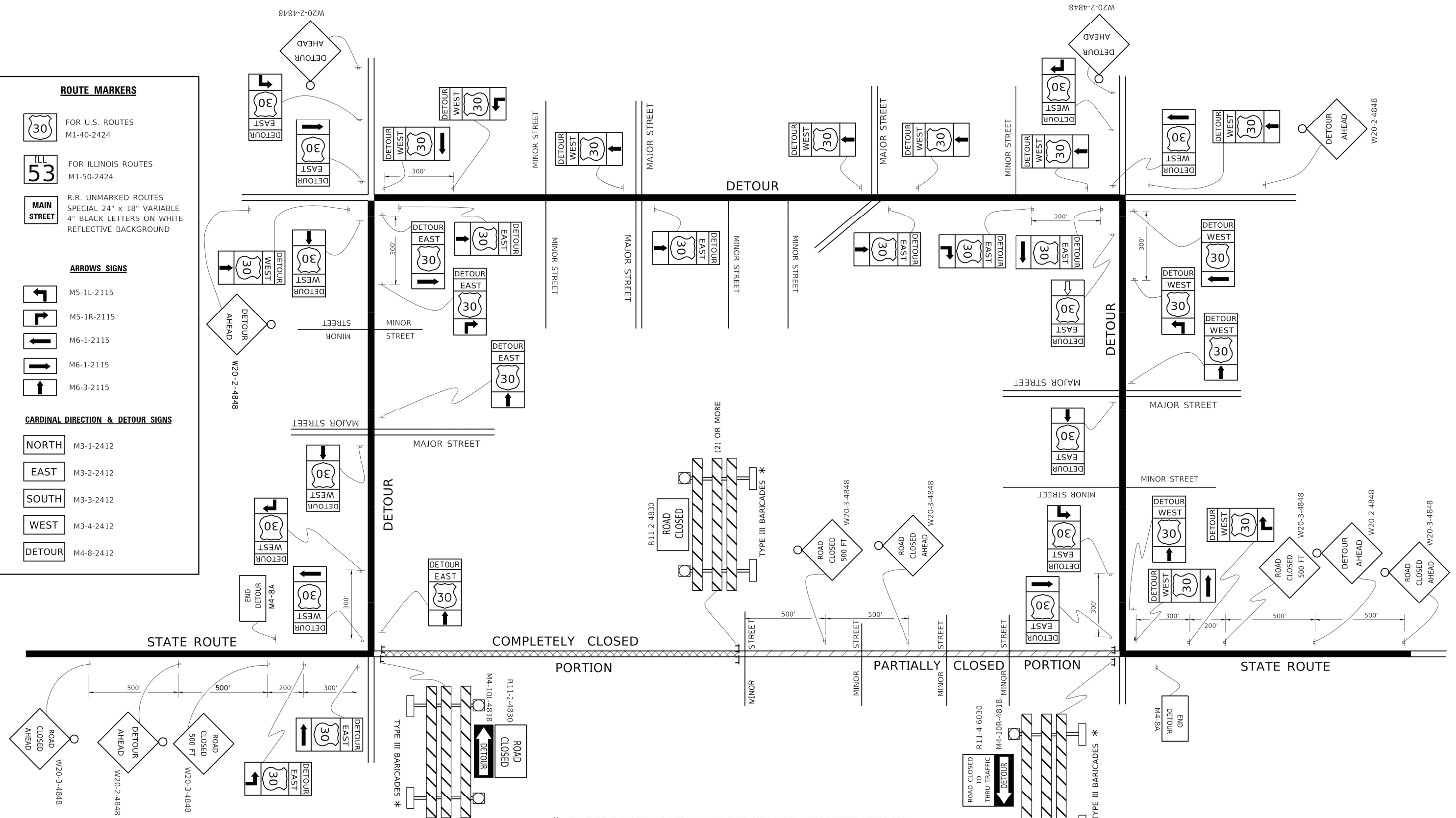
NORTH M3-1-2412

EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

DETOUR M4-8-2412



\* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

USER NAME = rootmj	DESIGNED -	REVISED - 10-18-02
	DRAWN -	REVISED - R. BORO 09-14-09
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 3/4/2019	DATE -	REVISED -

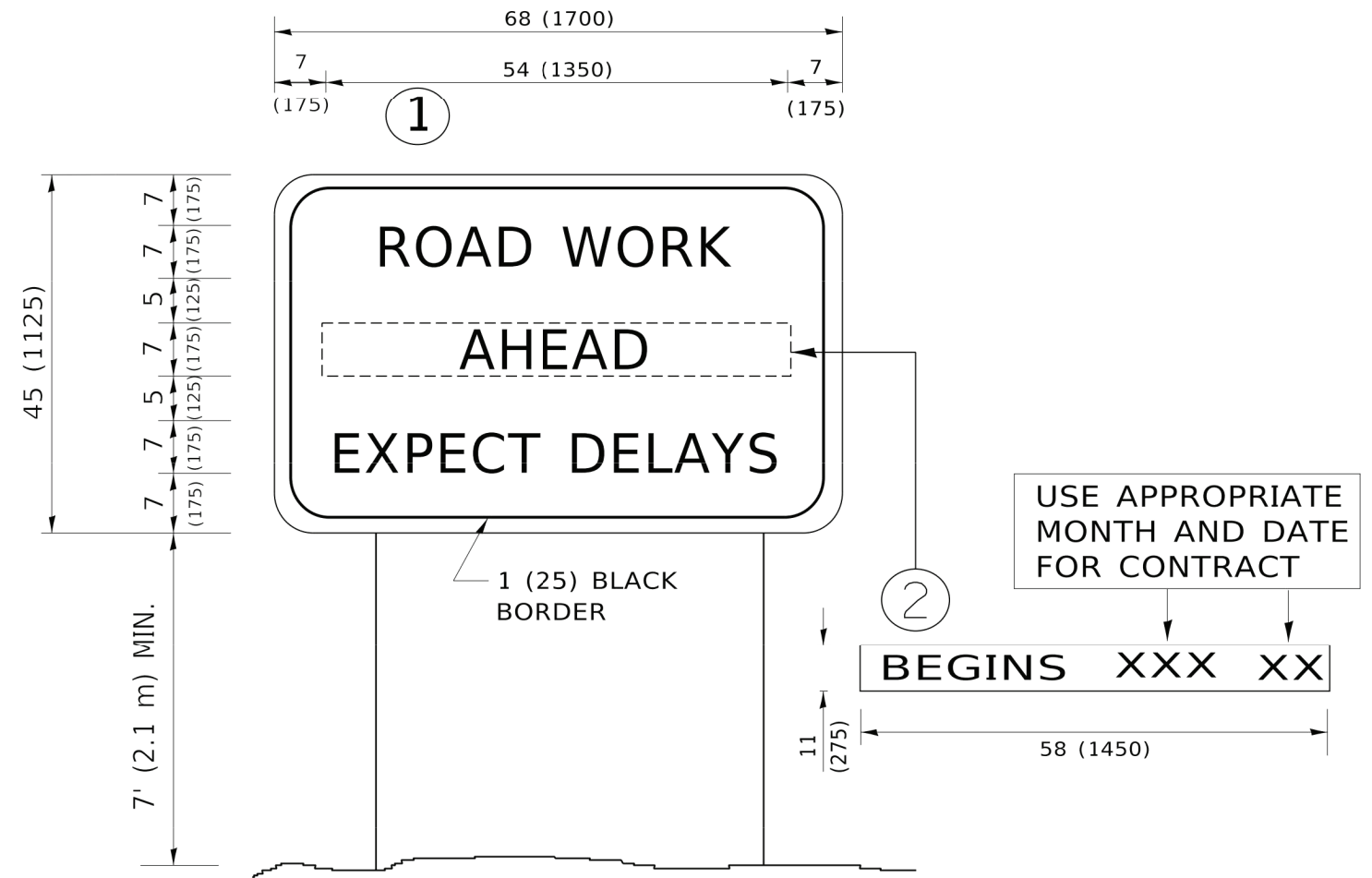
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETOUR SIGNING  
FOR CLOSING STATE HIGHWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	35
<b>TC-21</b>			CONTRACT NO. 61G81	
ILLINOIS FED. AID PROJECT				

PLOT DATE: Jan 06, 2021  
 FILENAME: H:\20-Ph\3005 - 123rd Street Reconstruction - Phase II\Plans-Eng\Rev-A\20-Ph-3005-TITLE-NOTES-DETAILS-2021-01-25.dwg

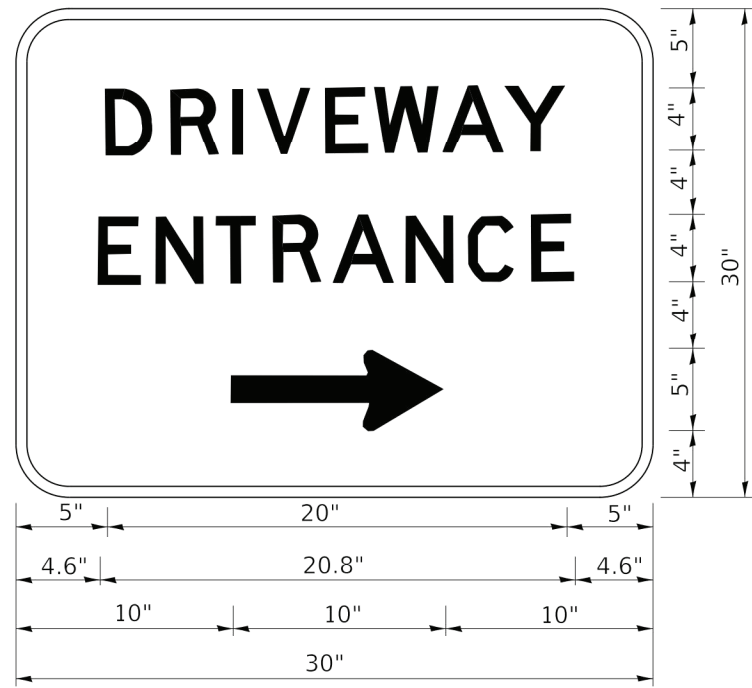


**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = footejm PLOT SCALE = 50.0000' / in. PLOT DATE = 3/4/2019	DESIGNED - DRAWN - CHECKED - DATE -	REVISED - R. MIRS 09-15-97 REVISED - R. MIRS 12-11-97 REVISED - T. RAMMACHER 02-02-99 REVISED - C. JUCIUS 01-31-07	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>	F.A.U. RTE. 1710 SECTION 15-00053-00-PV COUNTY COOK TOTAL SHEETS 46 SHEET NO. 36	CONTRACT NO. 61G81 ILLINOIS FED. AID PROJECT
			SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

PLOT DATE: Jan 26, 2021  
 FILENAME: H:\20-PA\3005 - 123rd Street Reconstruction - Phase I\Plans-Eng\Rev-4\20-PH-3005-TITLE-NOTES-DETAILS-2021-01-25.dwg

USER NAME = footemj	DESIGNED -	REVISED - C. JUCIUS 02-15-07
PLOT SCALE = 50.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 3/4/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY ENTRANCE SIGNING**

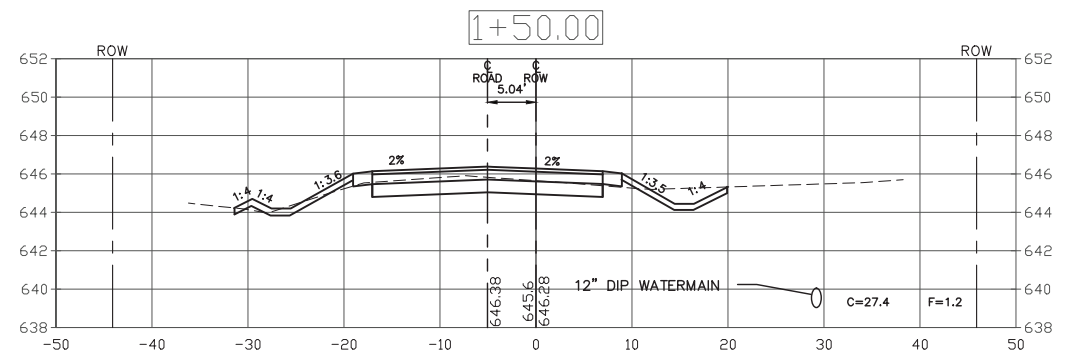
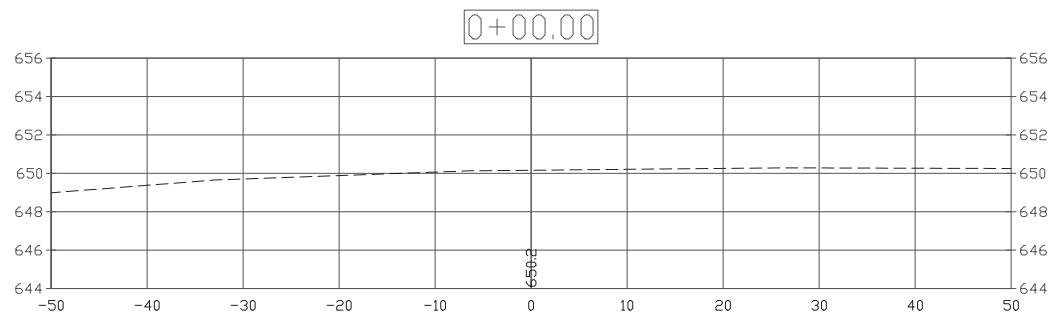
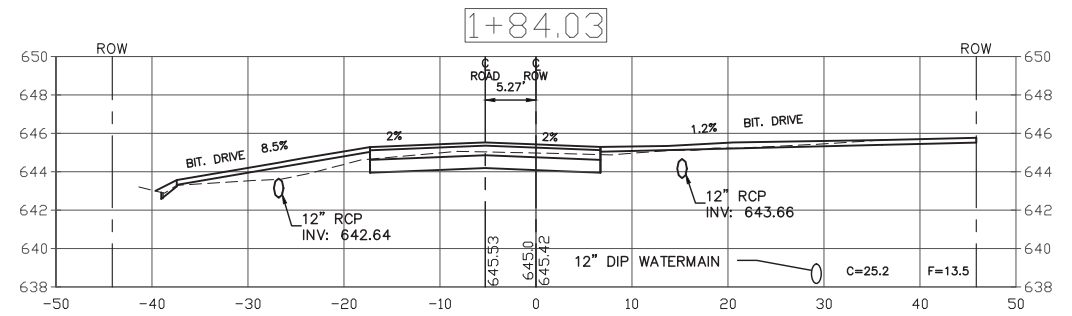
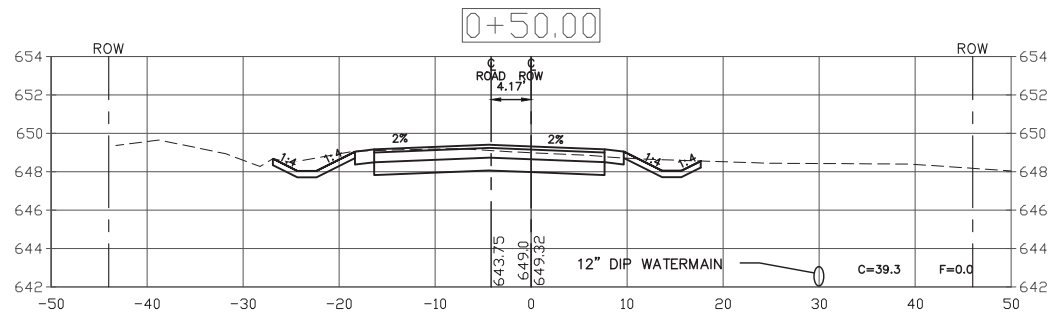
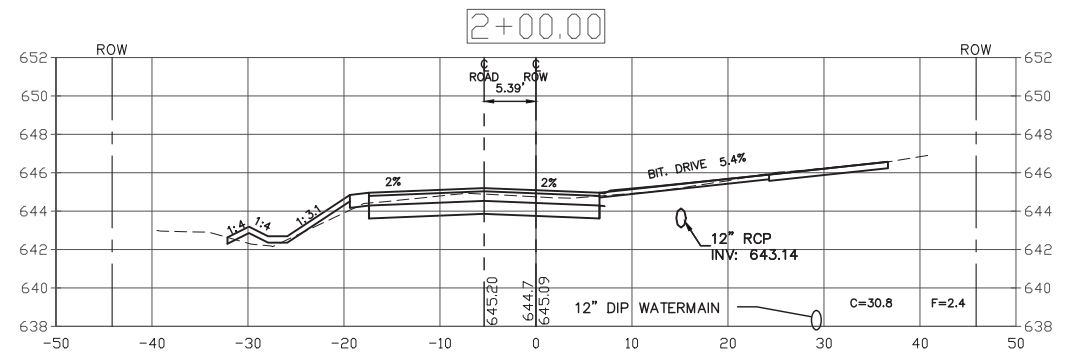
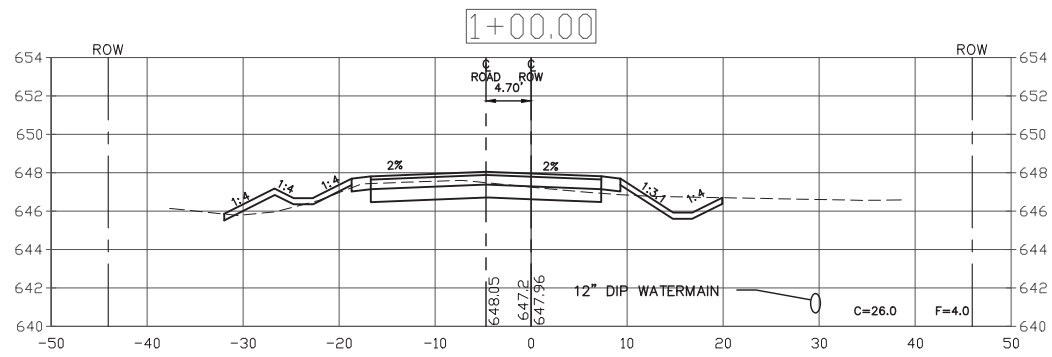
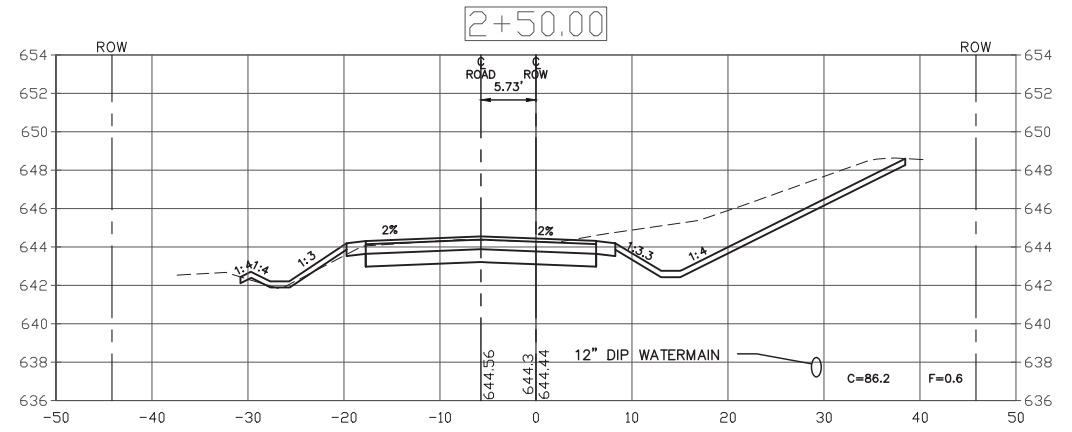
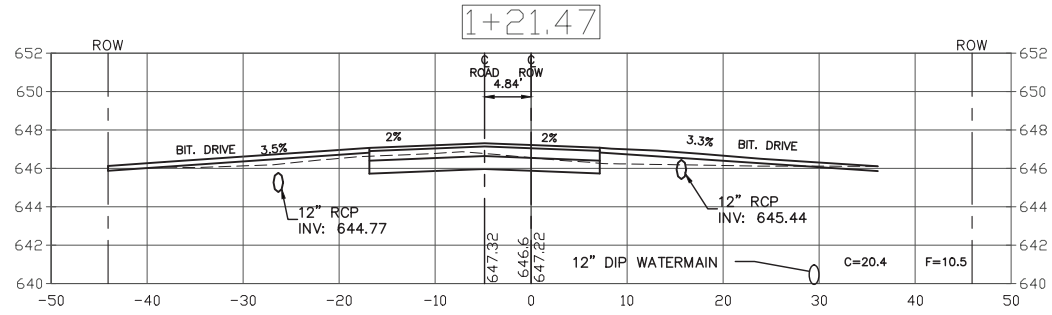
SCALE: NONE- SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	37
<b>TC-26</b>			CONTRACT NO. 61G81	

ILLINOIS | FED. AID PROJECT



PLOT DATE: Jan 26, 2021  
 FILENAME: H:\20-PA\3005 - 123rd Streets Reconstruction - Phase I\Plans-Eng\Rev-A\20-PH-3005-SITE-2021-01-04.dwg



USER NAME =	DESIGNED - AS	REVISED -
	DRAWN - DJB	REVISED -
PLOT SCALE =	CHECKED - AS	REVISED -
PLOT DATE = Jan 26, 2021	DATE -	REVISED -

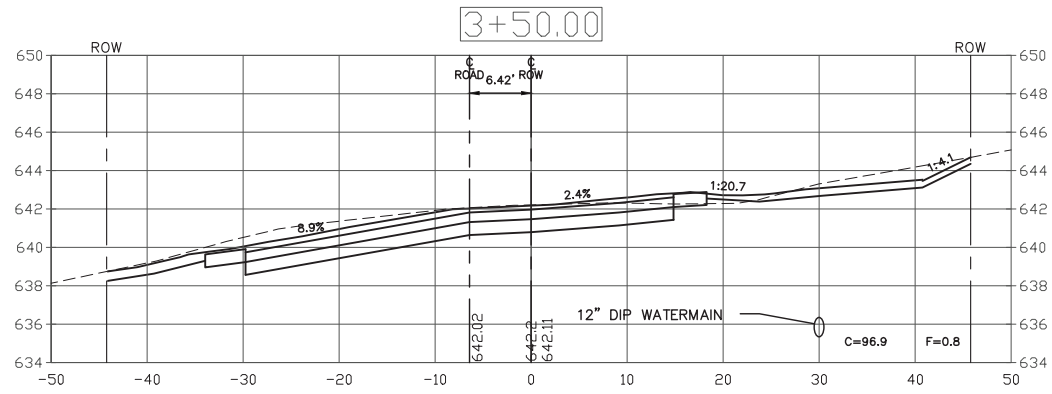
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS**

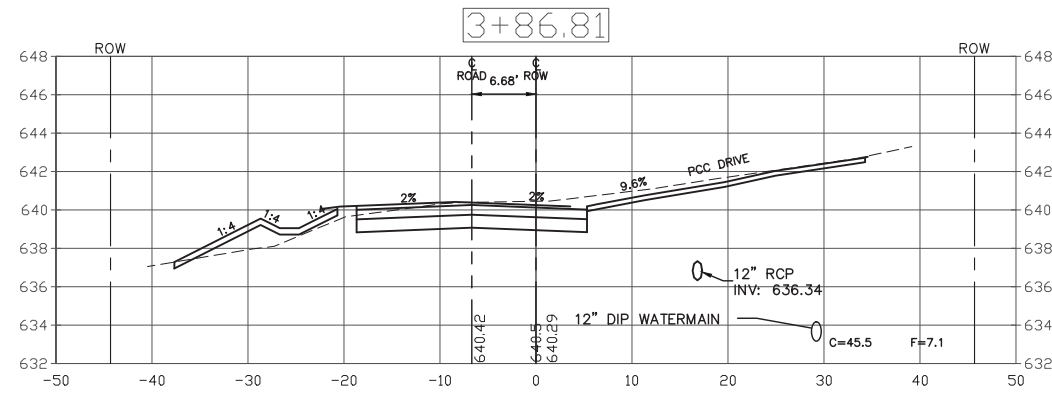
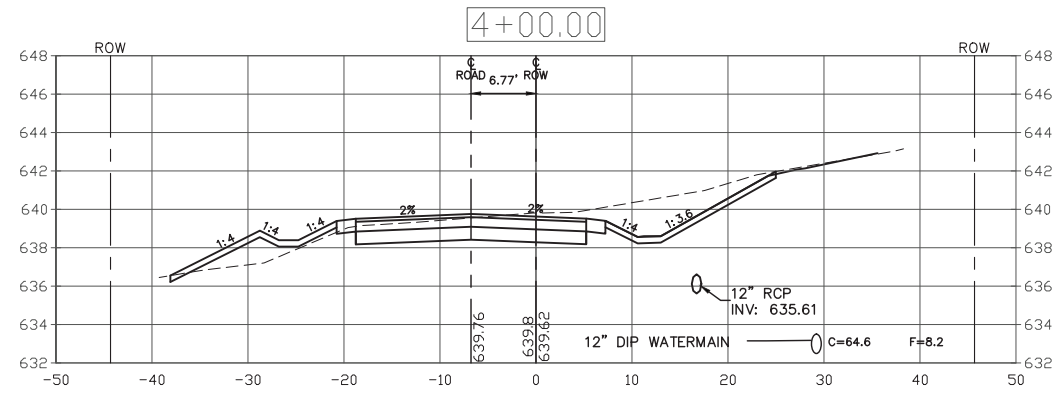
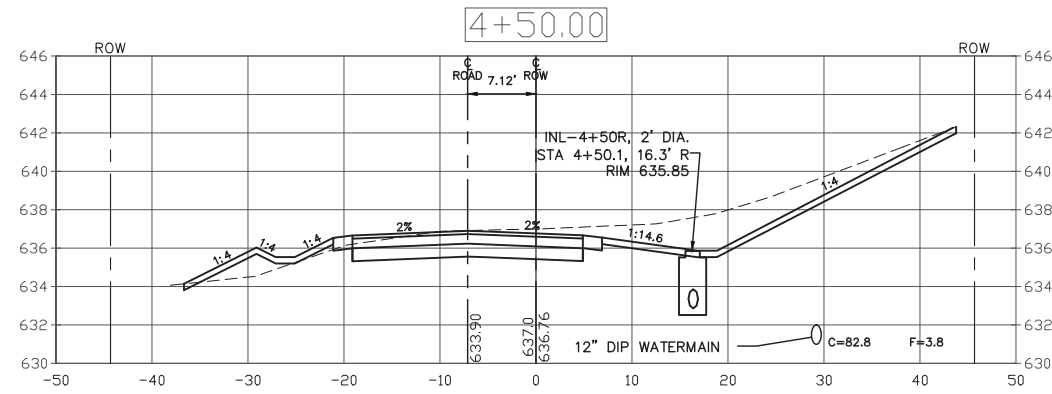
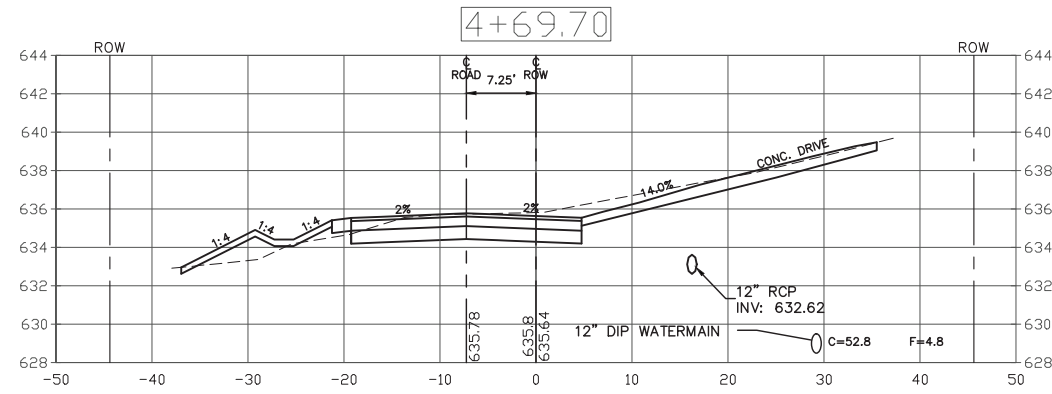
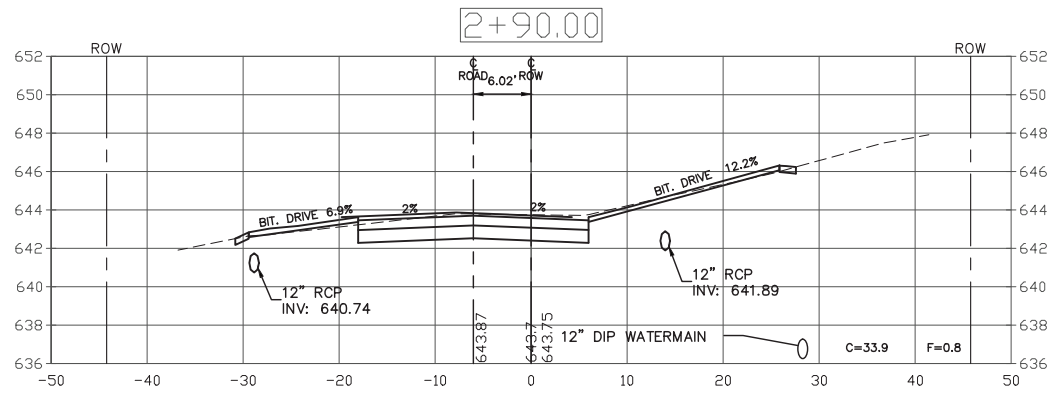
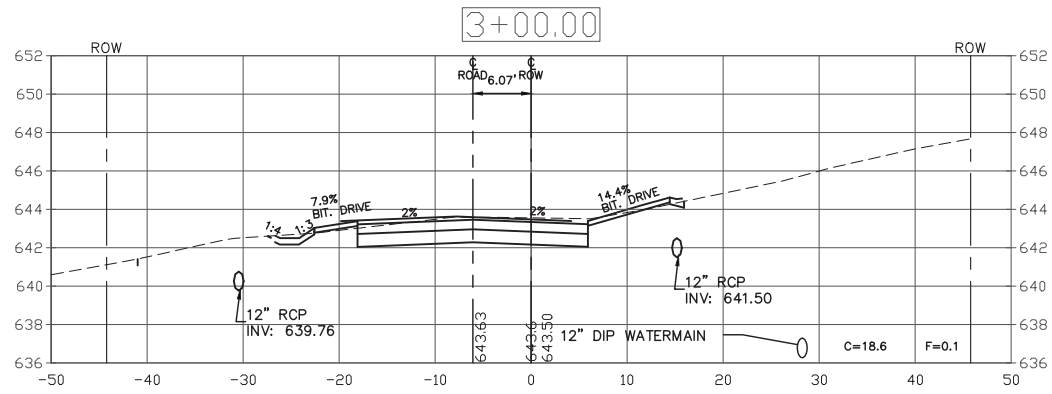
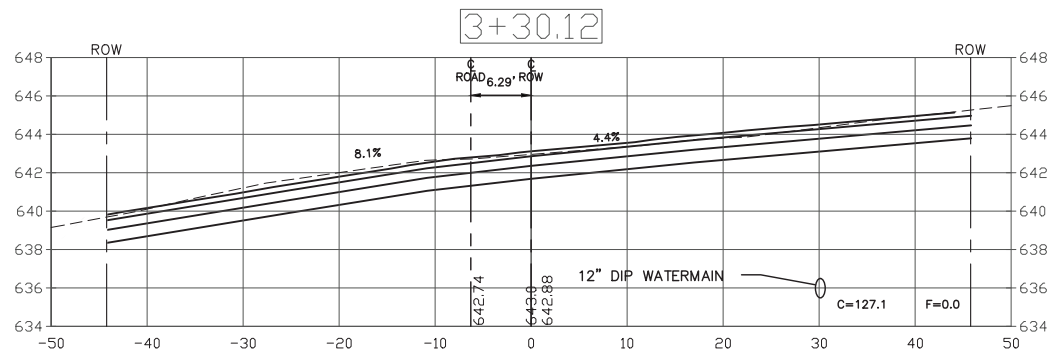
SCALE: H1"=10' V1"=5' SHEET NO. 1 OF 9 SHEETS STA. 0+00 TO STA. 2+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	38
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	

PLOT DATE: Jan 26, 2021  
 FILENAME: H:\20-PA\3005 - 123rd Street Reconstruction - Phase II\Plans-Eng\Rev-A\20-PA-3005-SITE-2021-01-04.dwg



HAROLD AVENUE



USER NAME =	DESIGNED - AS	REVISED -
	DRAWN - DJB	REVISED -
PLOT SCALE =	CHECKED - AS	REVISED -
PLOT DATE = Jan 26, 2021	DATE -	REVISED -

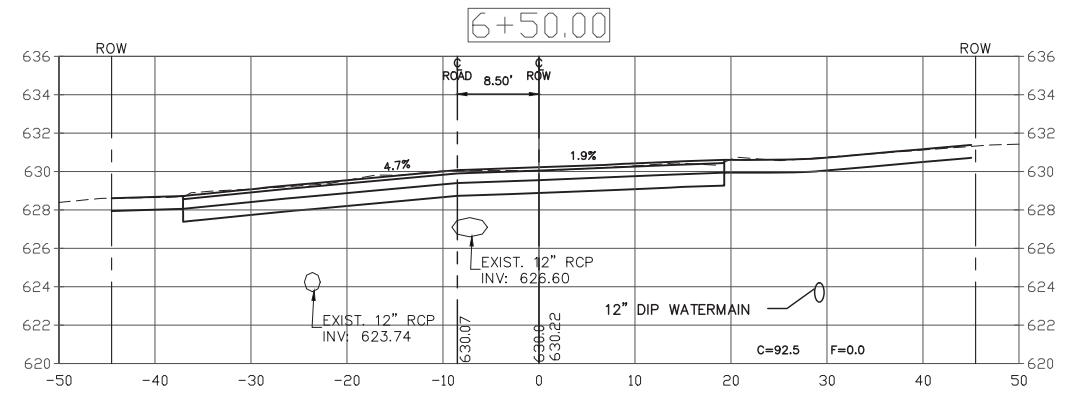
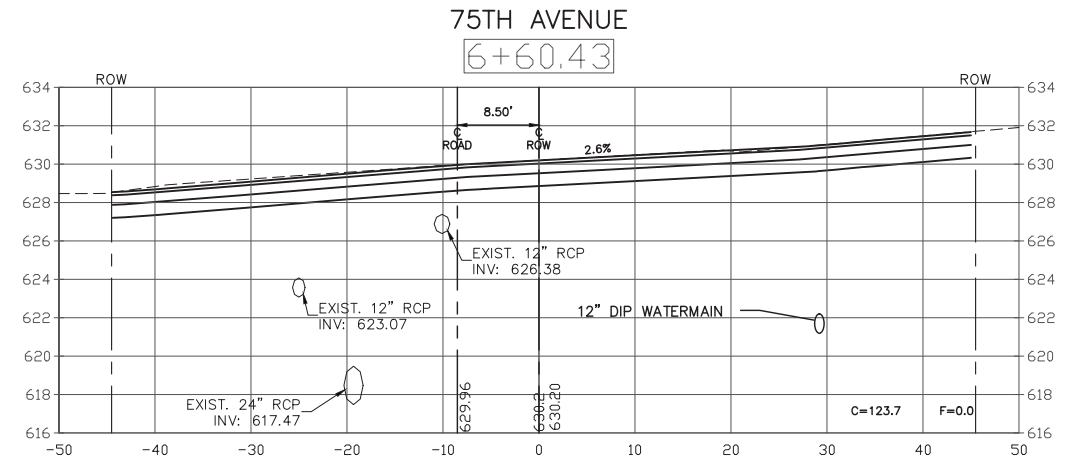
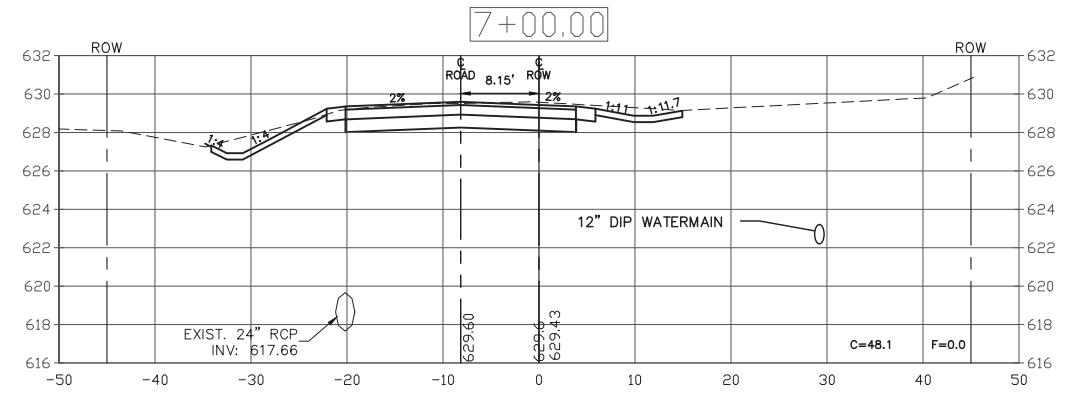
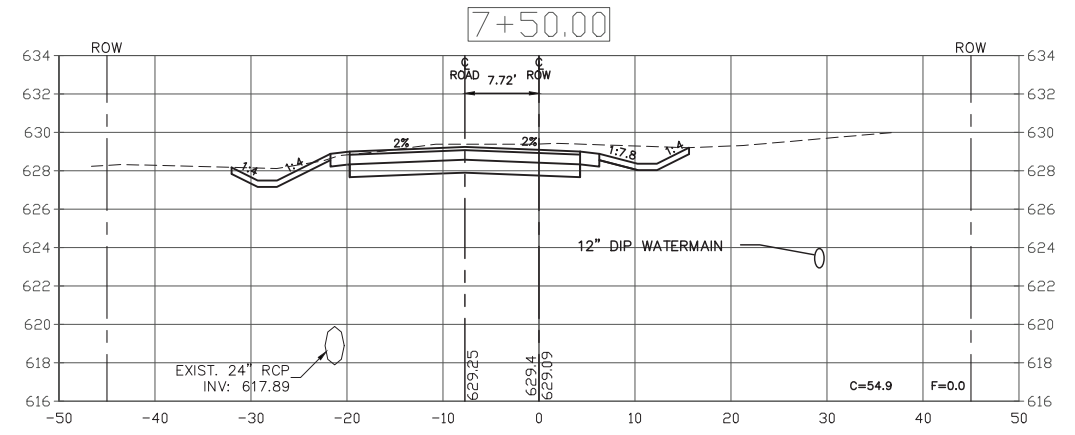
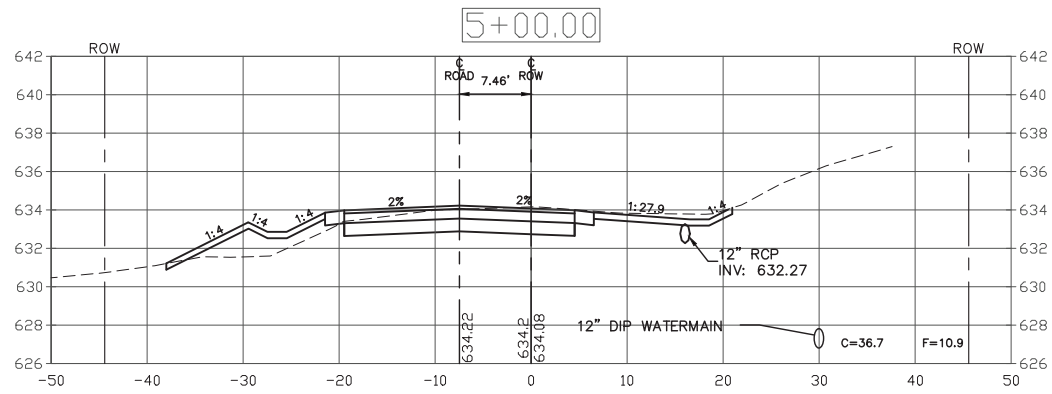
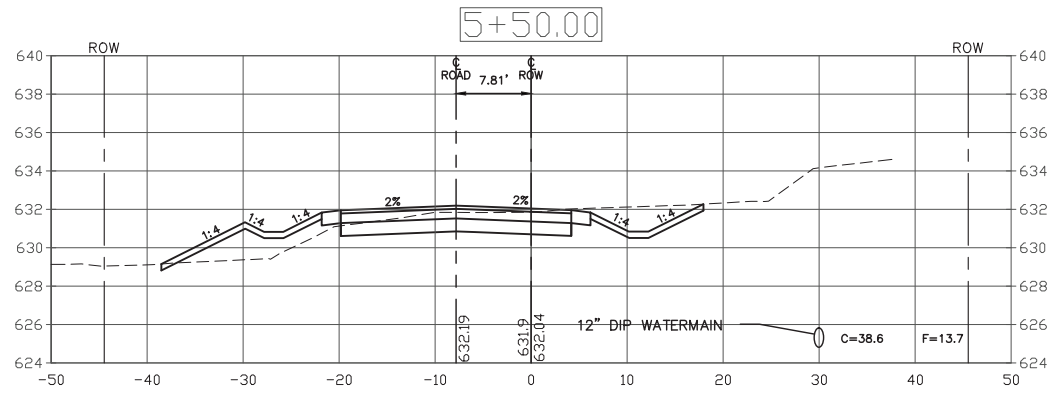
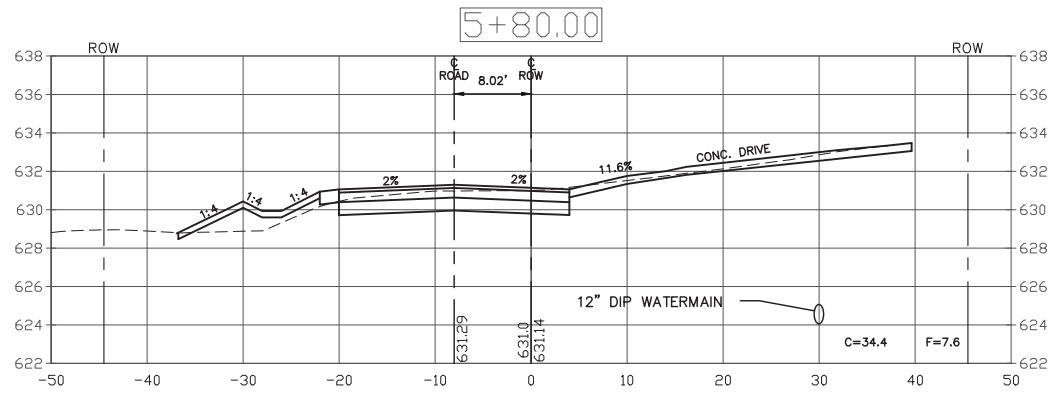
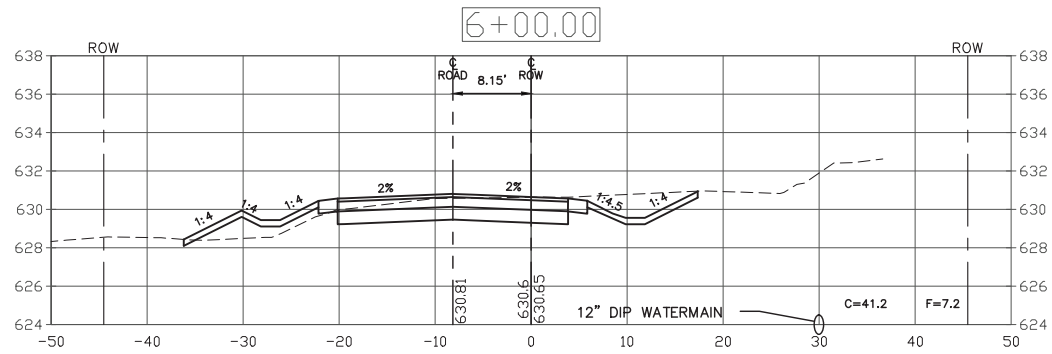
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS

SCALE: H1"=10' V1"=5' SHEET NO. 2 OF 9 SHEETS STA. 2+90 TO STA. 4+69.70

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	39
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	

PLOT DATE: Jan 26, 2021  
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PLOT SCALE =	CHECKED - AS	REVISED -
PLOT DATE = Jan 26, 2021	DATE -	REVISED -

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**DEPARTMENT OF TRANSPORTATION**

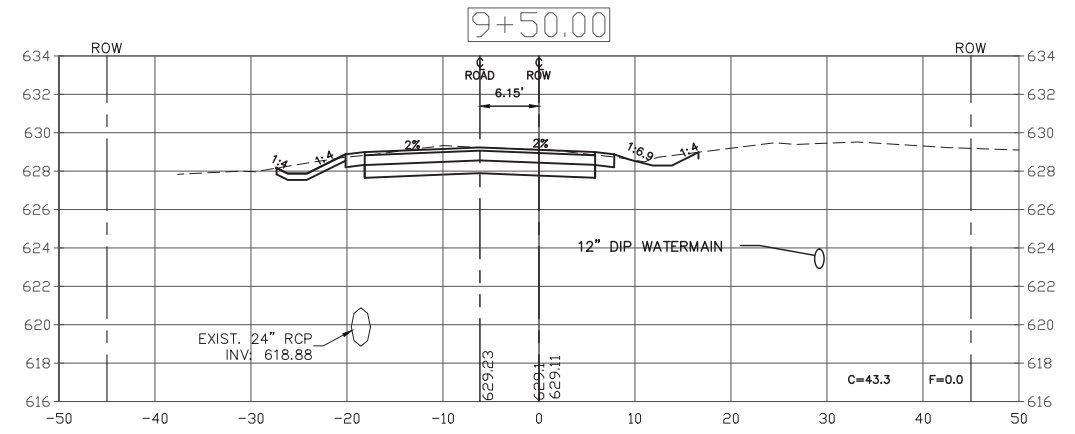
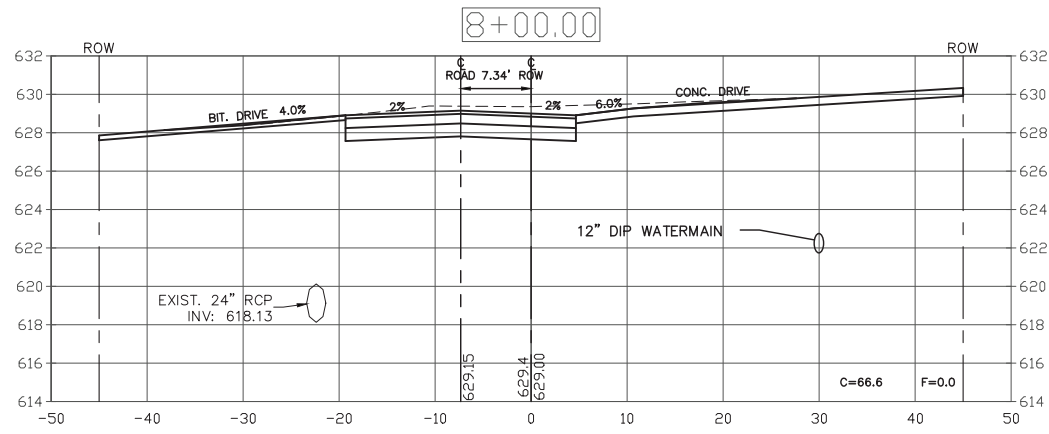
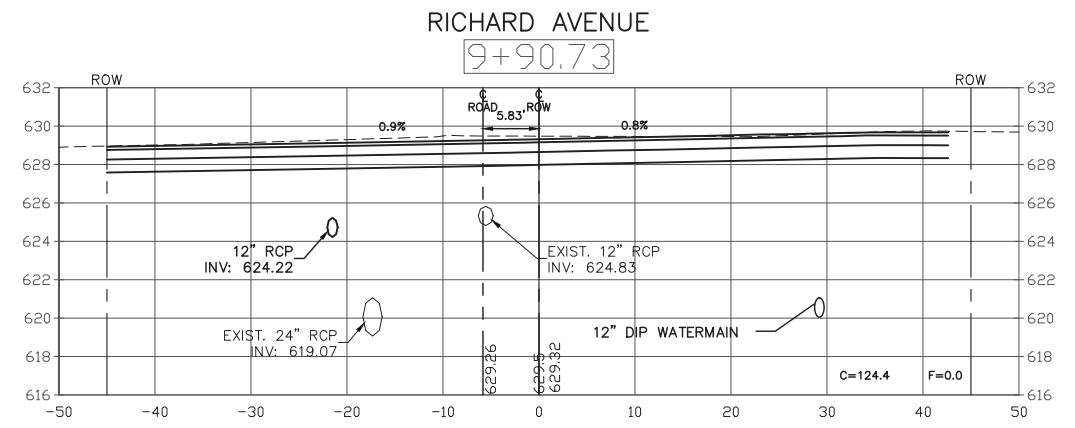
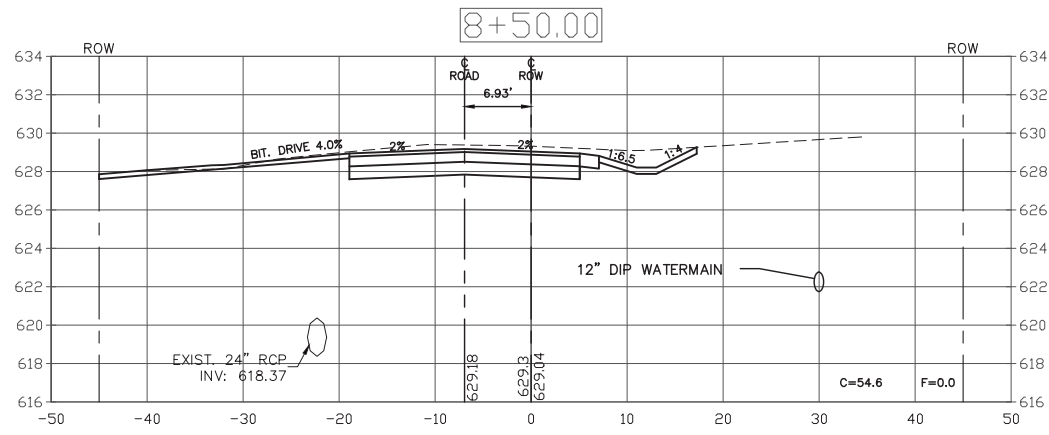
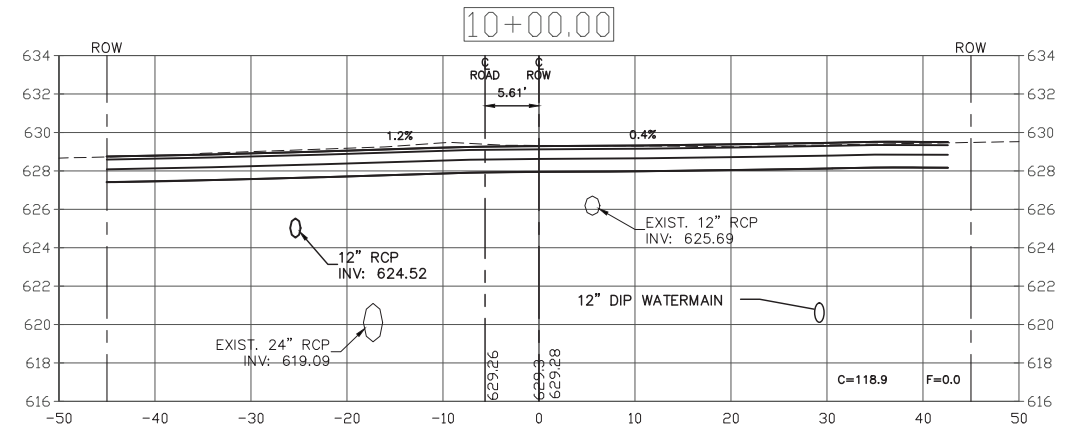
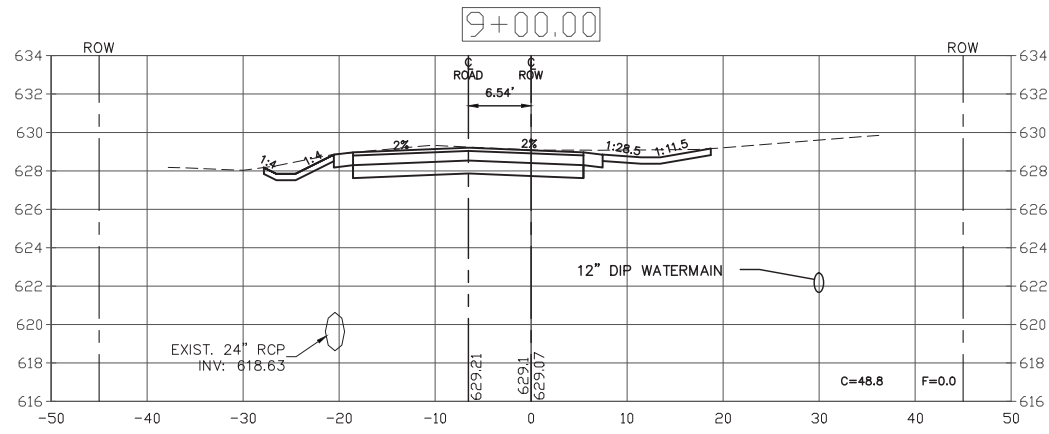
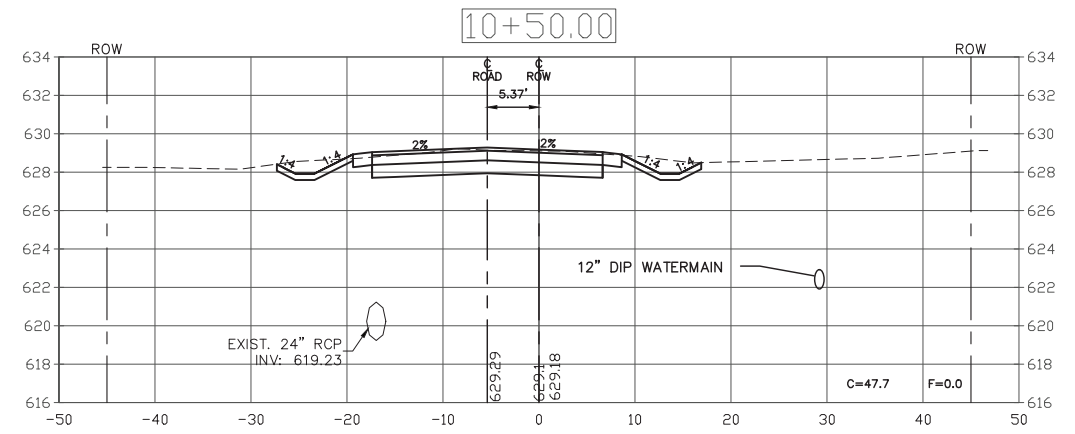
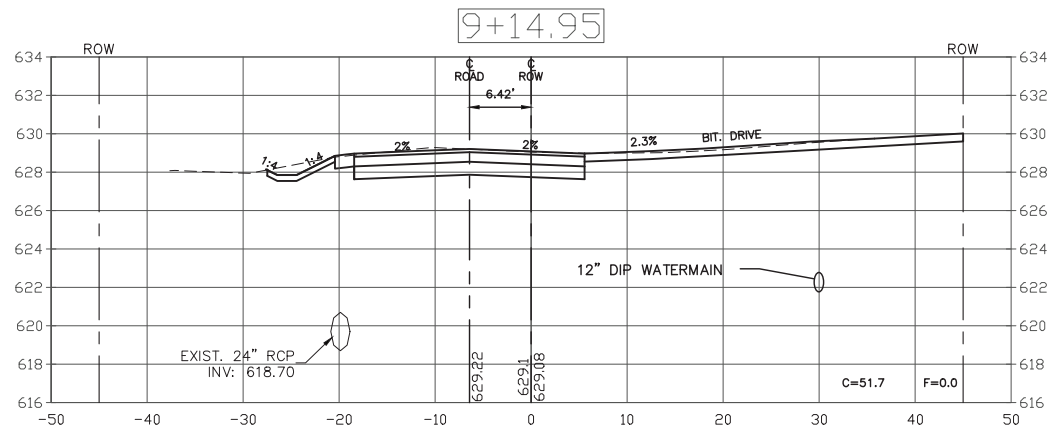
**CROSS SECTIONS**

SCALE: H1"=10' V1"=5' SHEET NO. 3 OF 9 SHEETS STA. 5+00 TO STA. 7+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	40
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	



PLOT DATE: Jan 26, 2021  
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PLOT DATE = Jan 26, 2021	DATE -	REVISED -

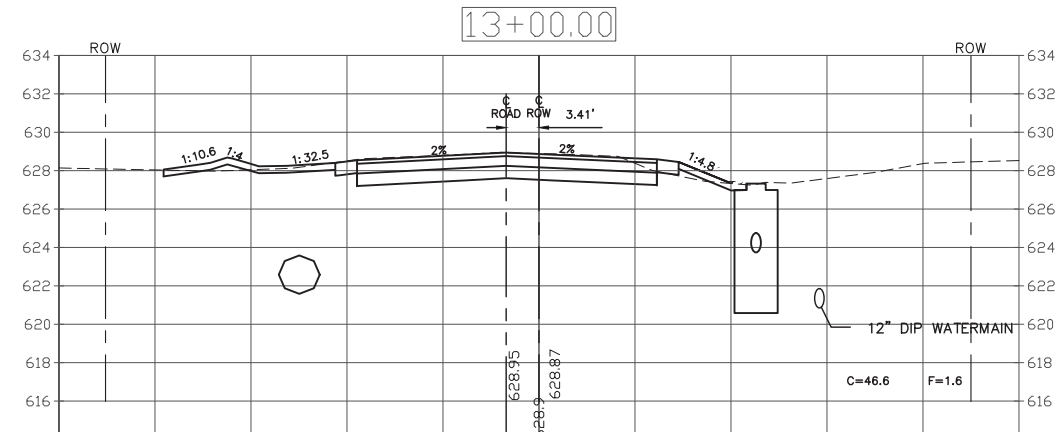
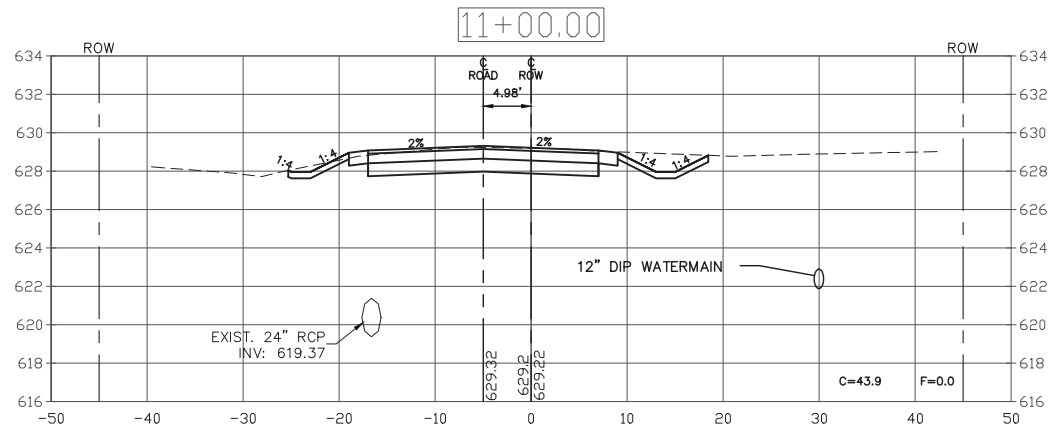
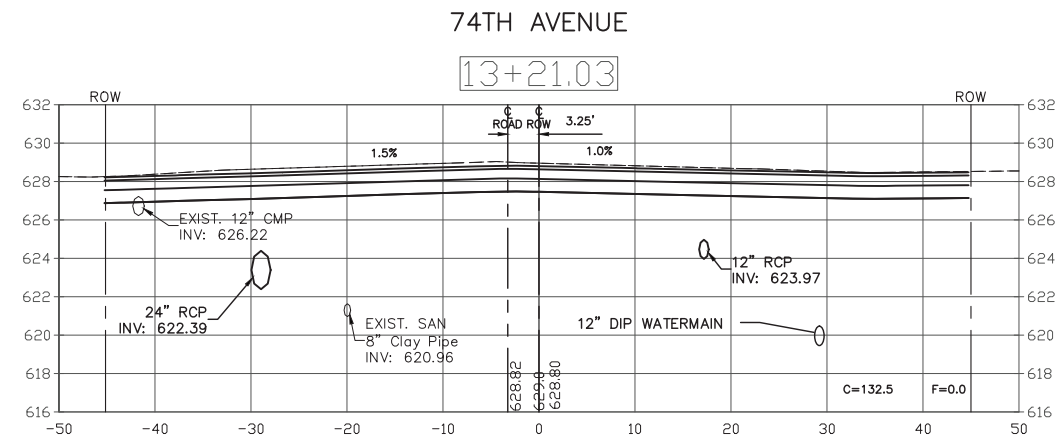
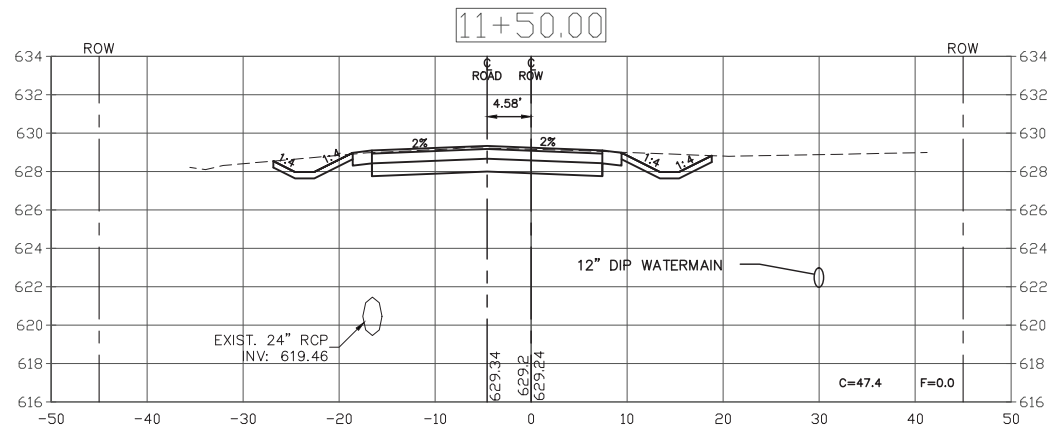
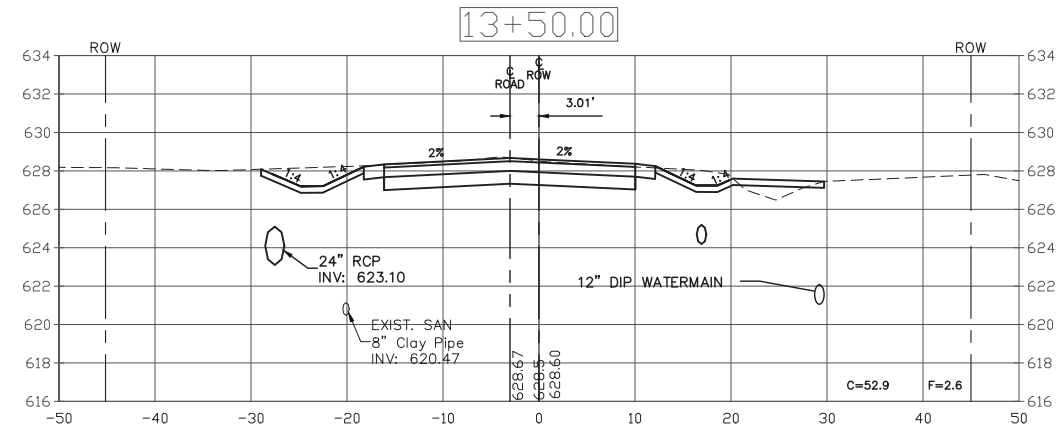
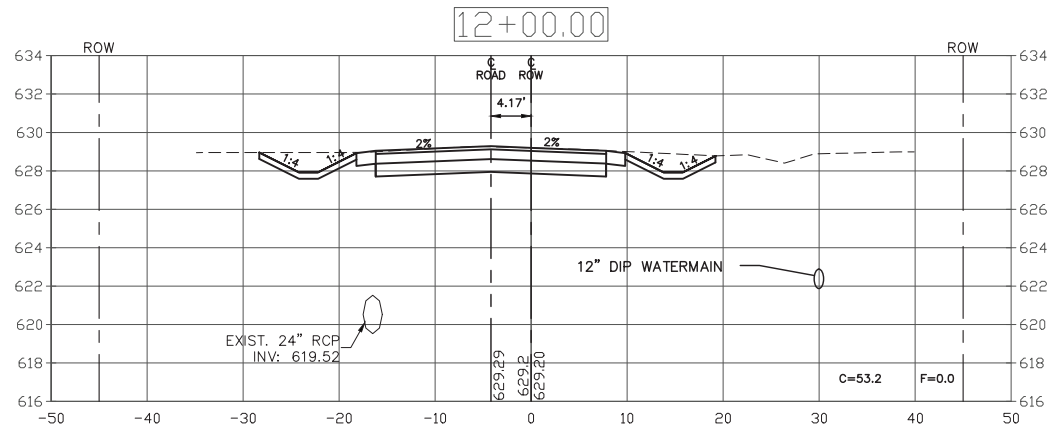
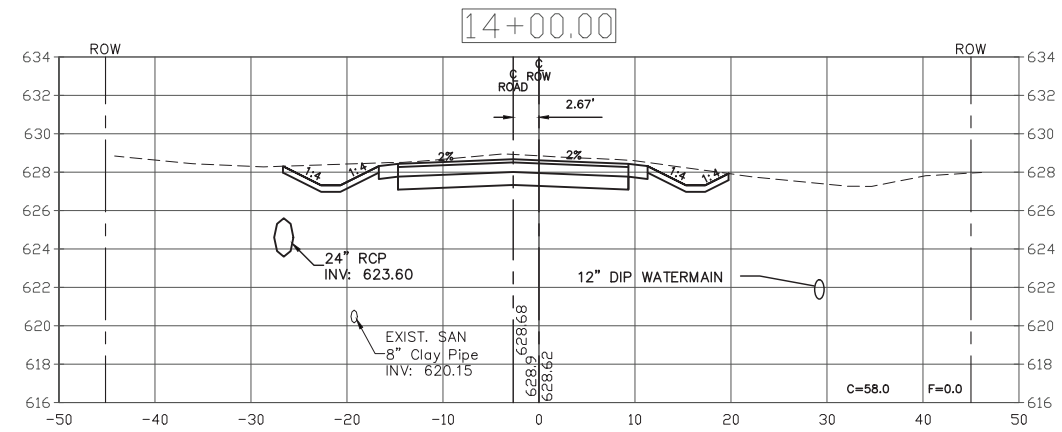
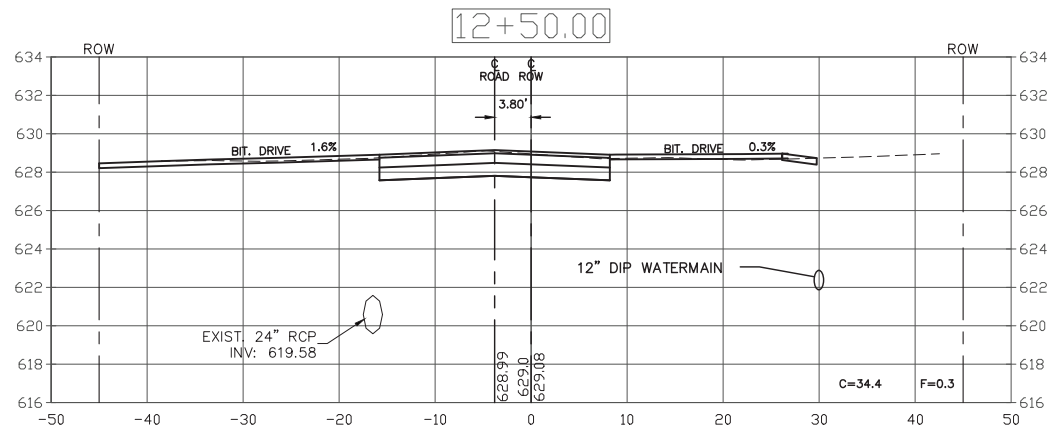
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS**

SCALE: H1"=10' V1"=5' SHEET NO. 4 OF 9 SHEETS STA. 8+00 TO STA. 10+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	41
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	

PLOT DATE: Jan 26, 2021  
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PLOT DATE = Jan 26, 2021	DATE -	REVISED -

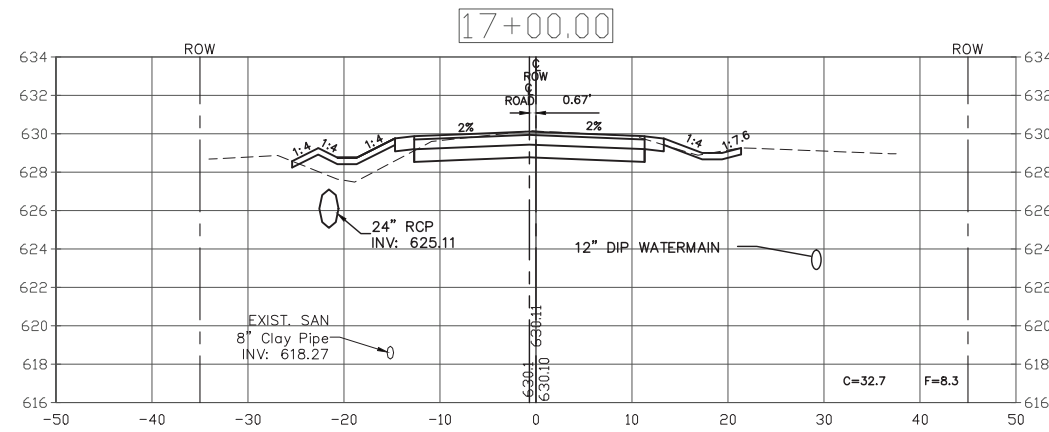
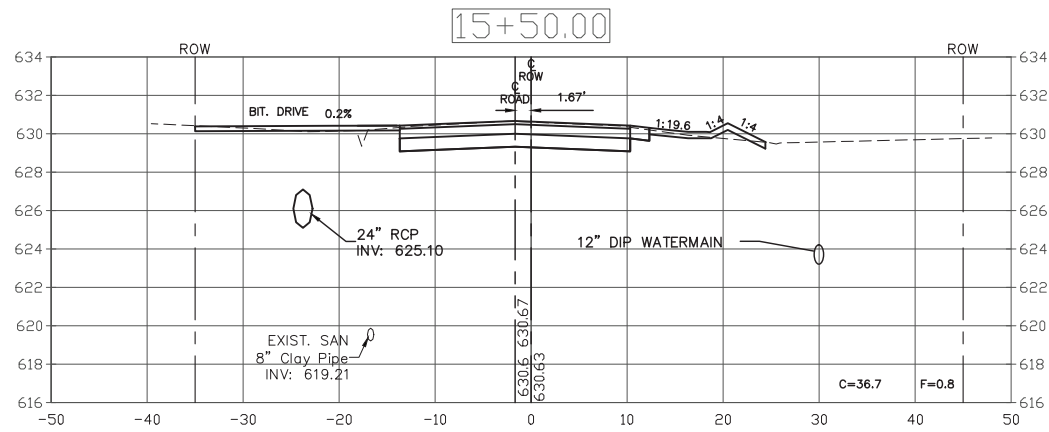
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS**

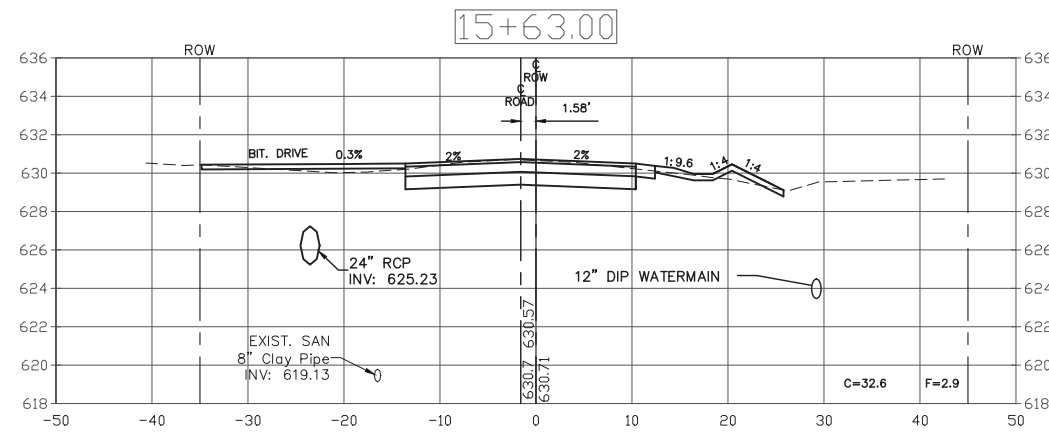
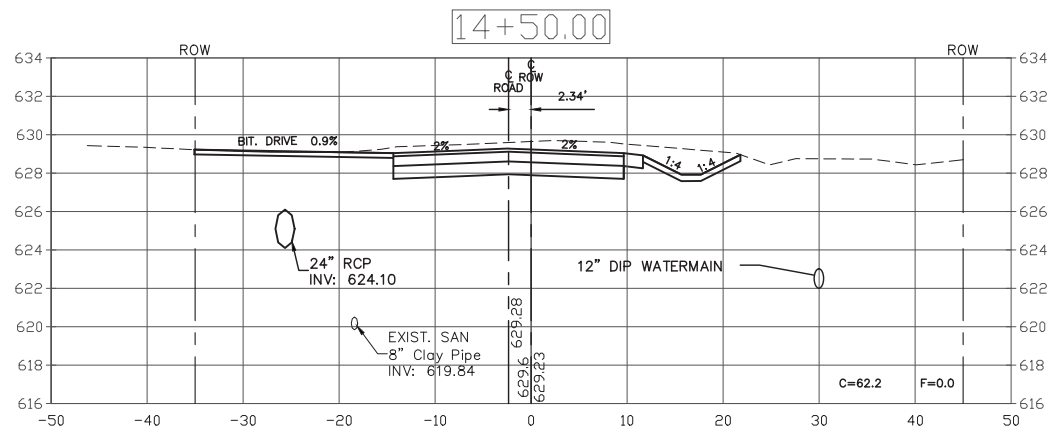
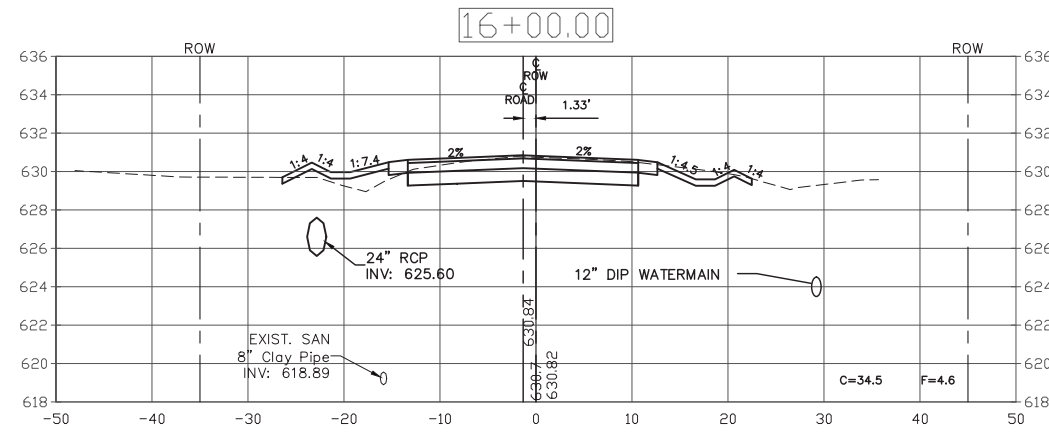
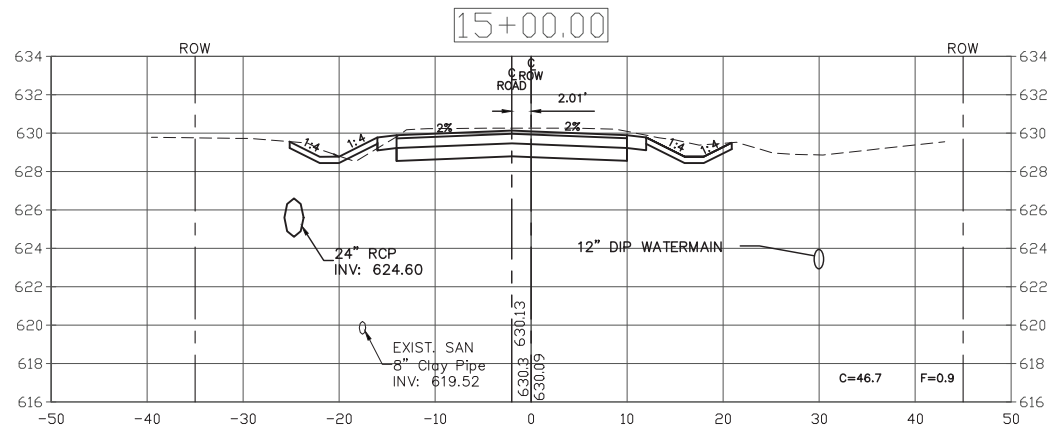
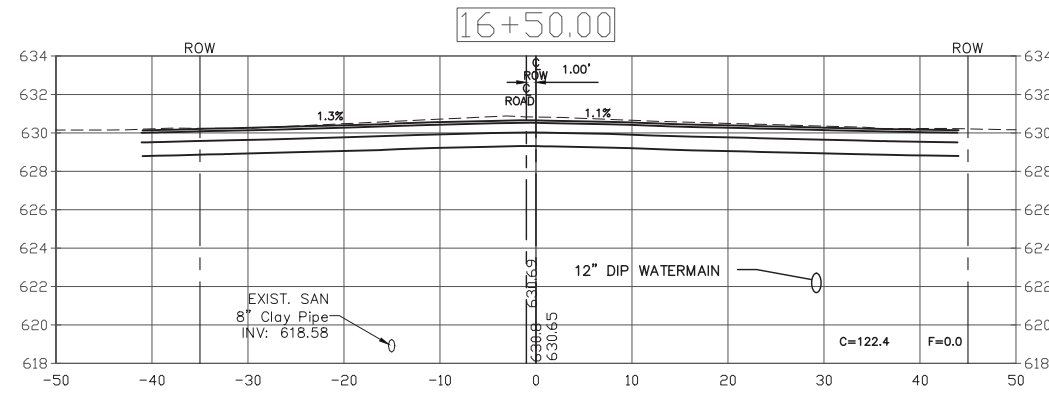
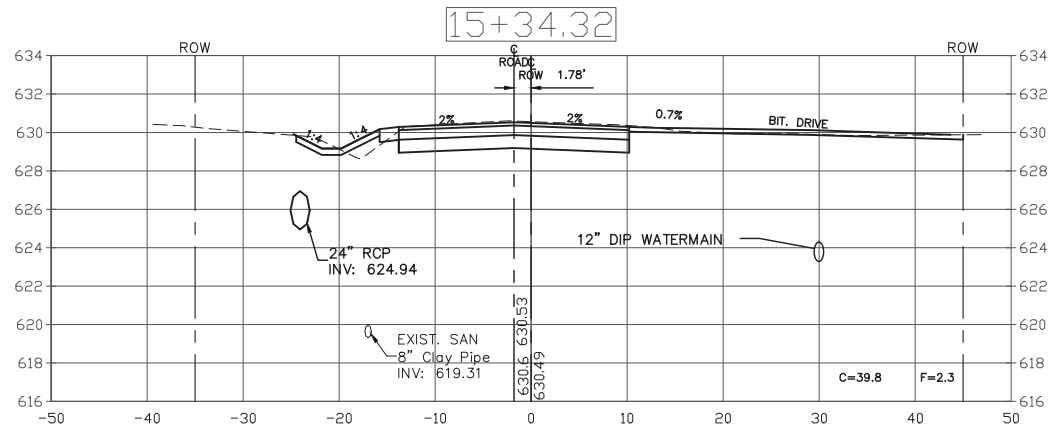
SCALE: H1"=10' V1"=5' SHEET NO. 5 OF 9 SHEETS STA. 11+00 TO STA. 14+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	42
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	

PLOT DATE: Jan 26, 2021  
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73RD COURT



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PLOT SCALE =	CHECKED - AS	REVISED -
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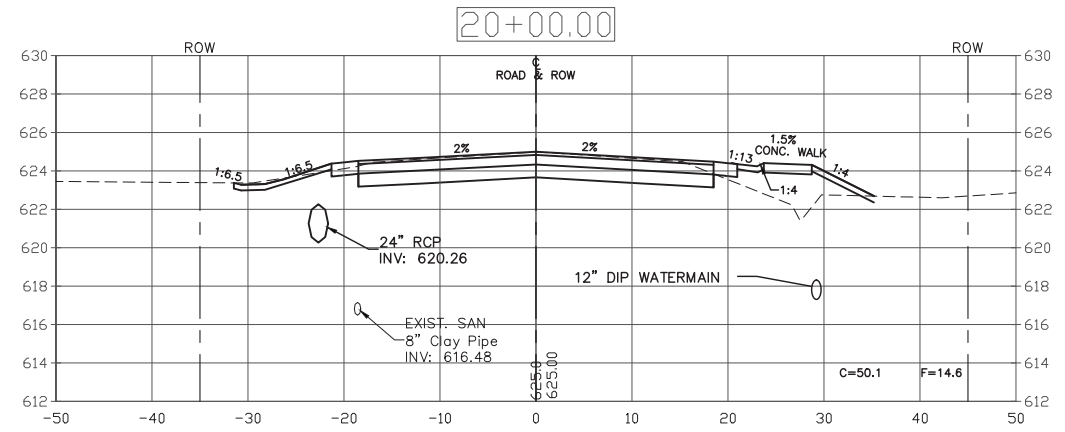
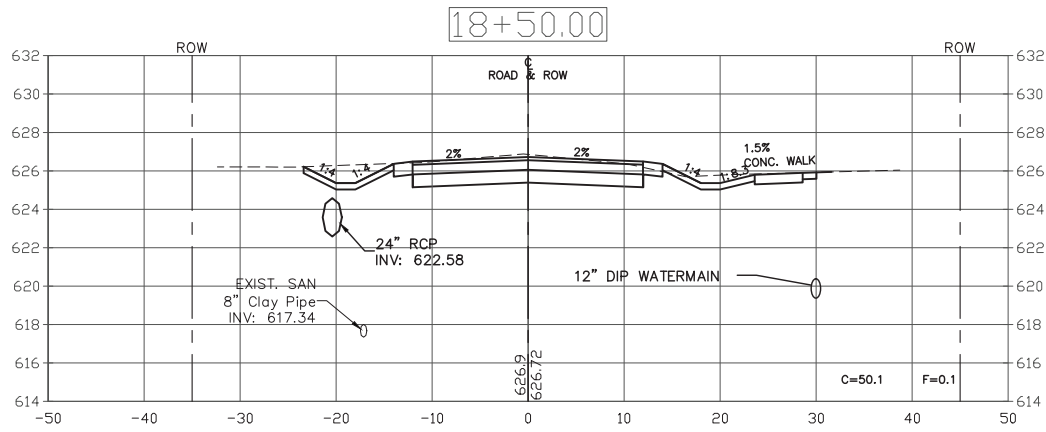
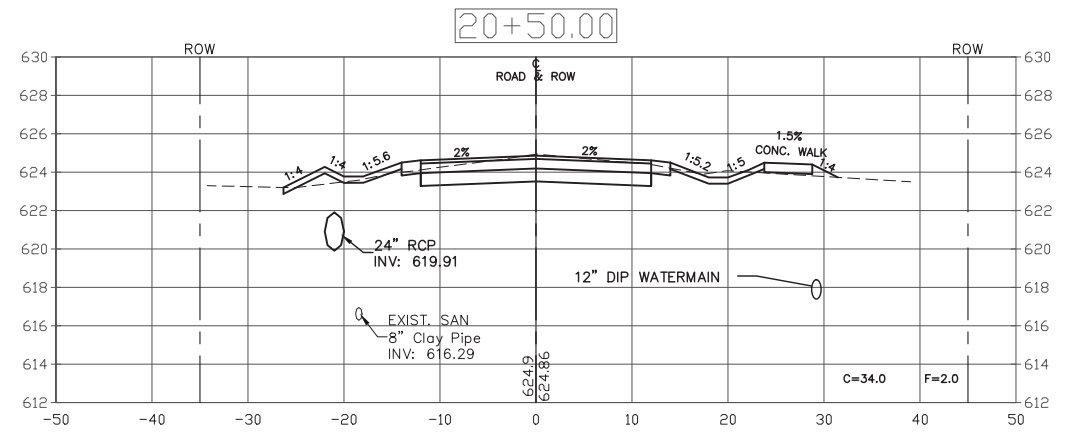
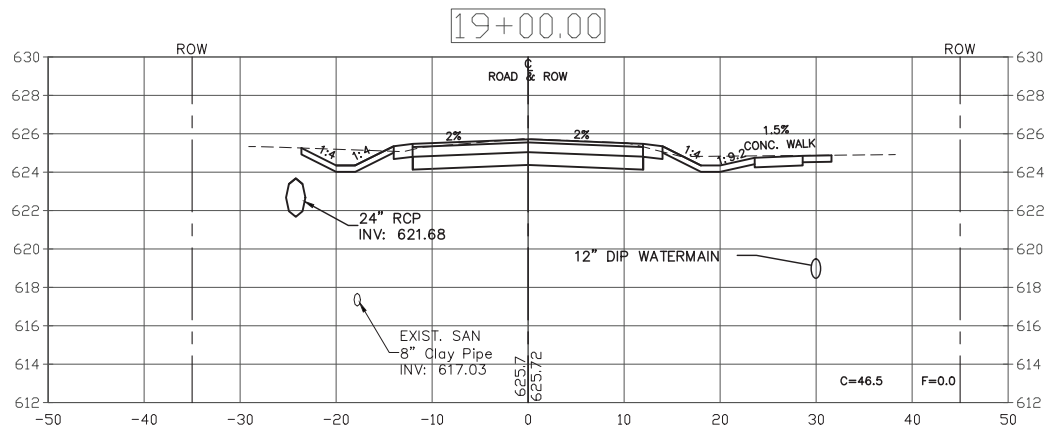
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS**

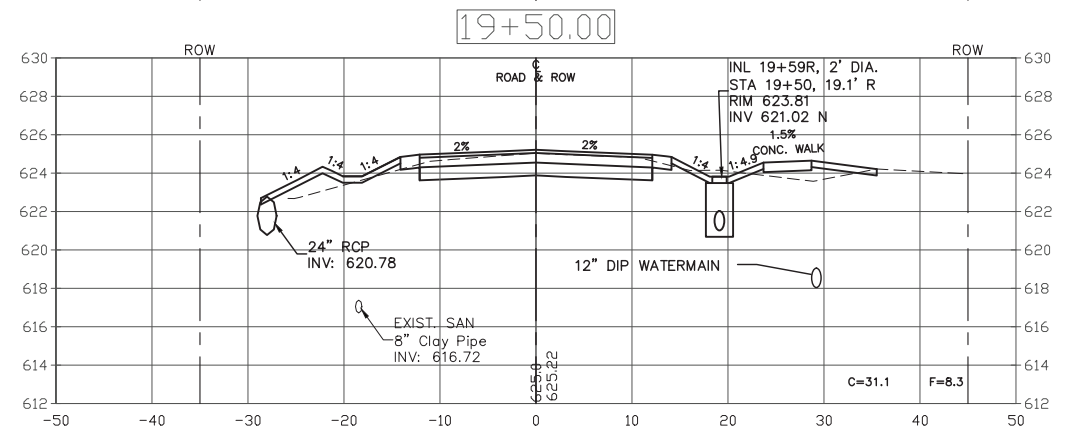
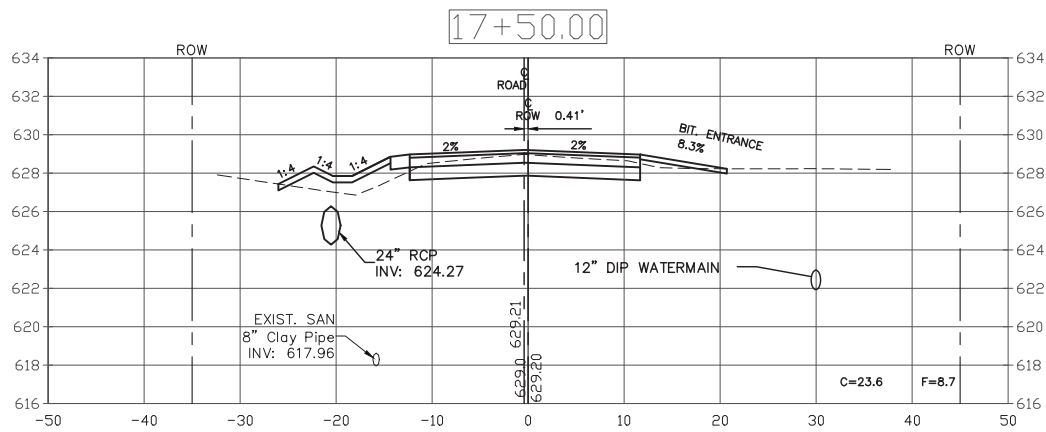
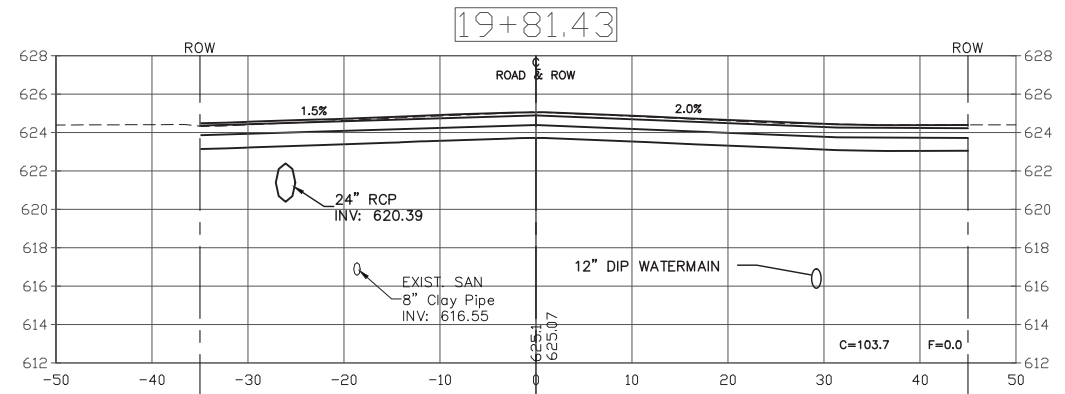
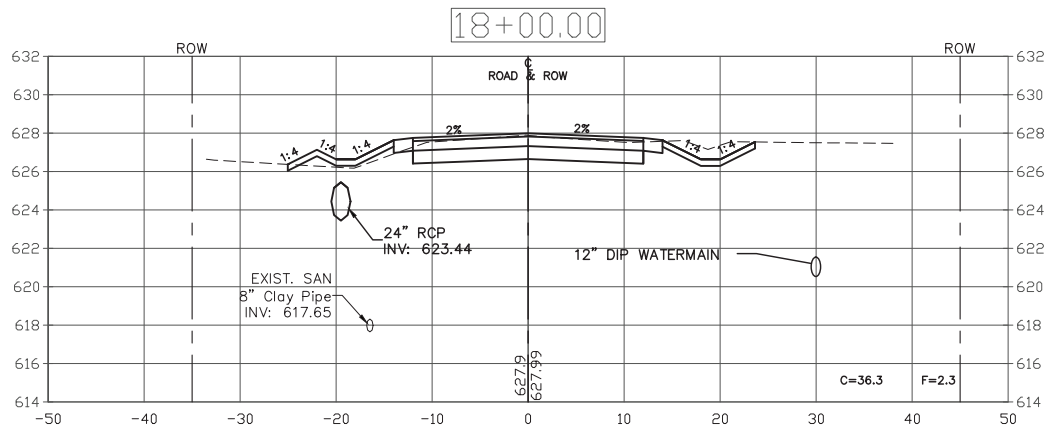
SCALE: H1"=10' V1"=5' SHEET NO. 6 OF 9 SHEETS STA. 14+50 TO STA. 17+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	43
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	

PLOT DATE: Jan 26, 2021  
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73RD AVENUE



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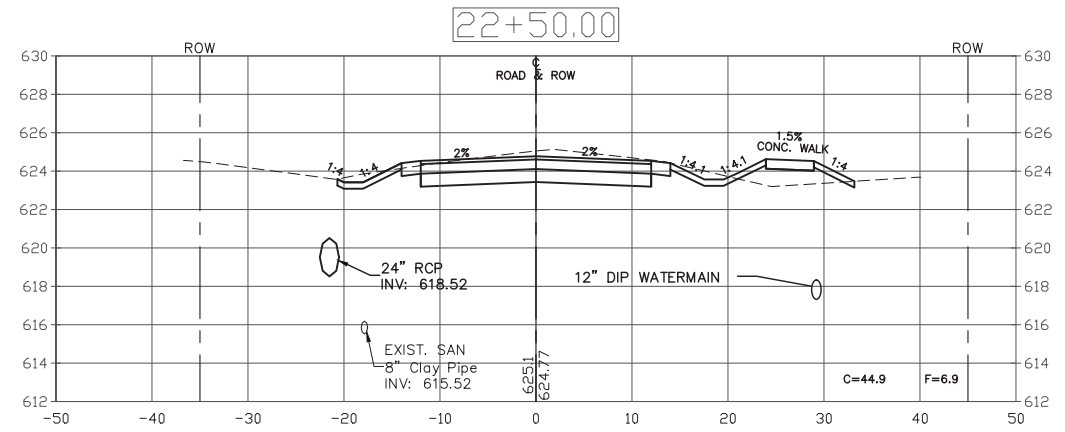
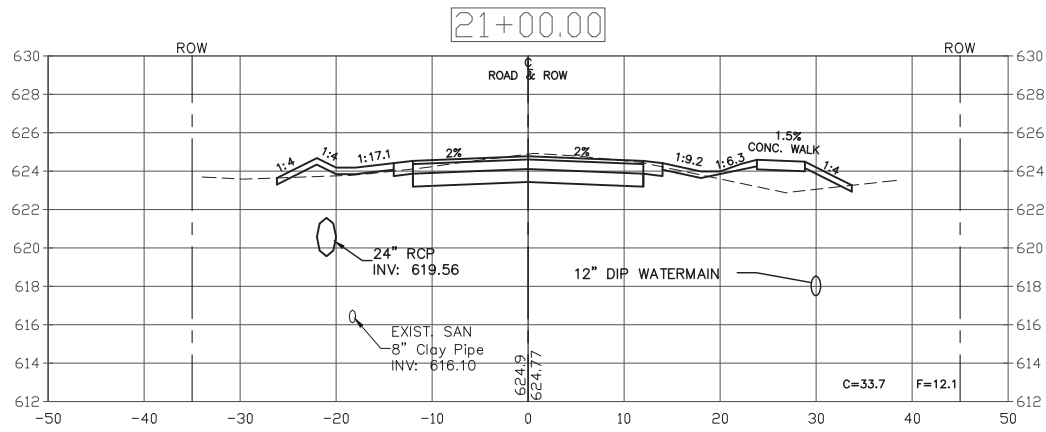
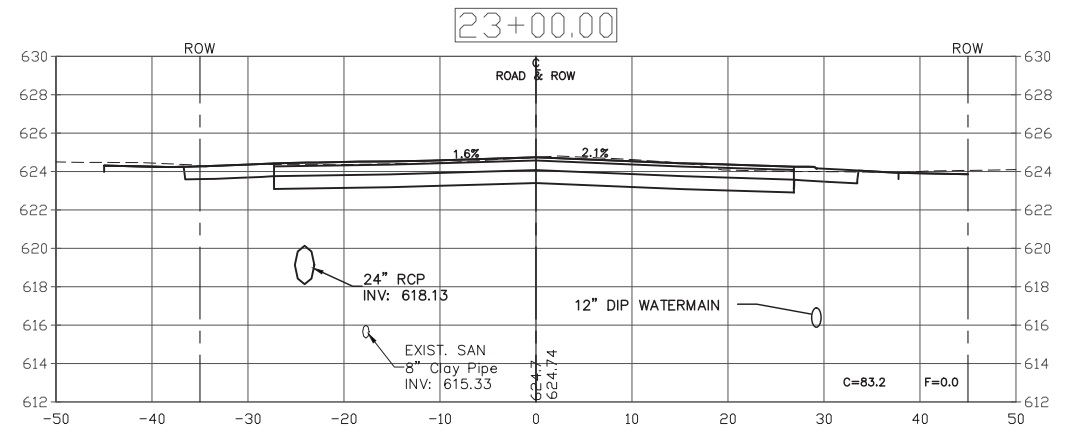
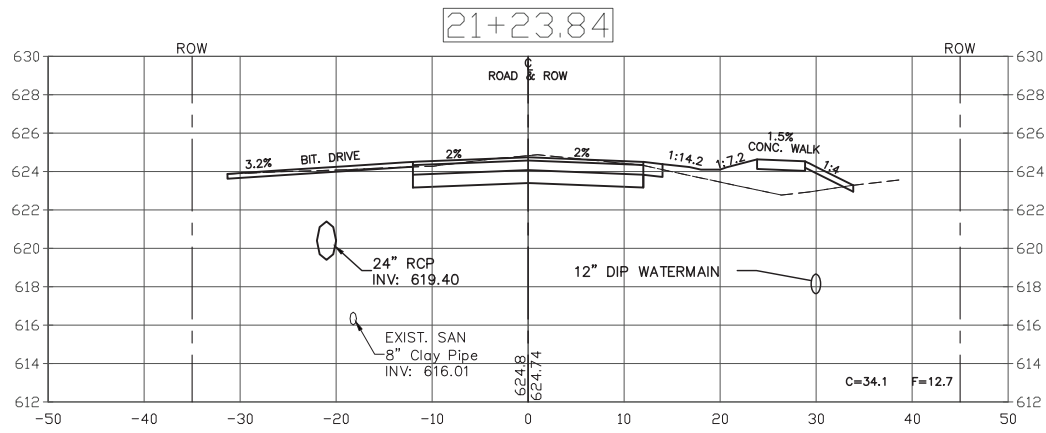
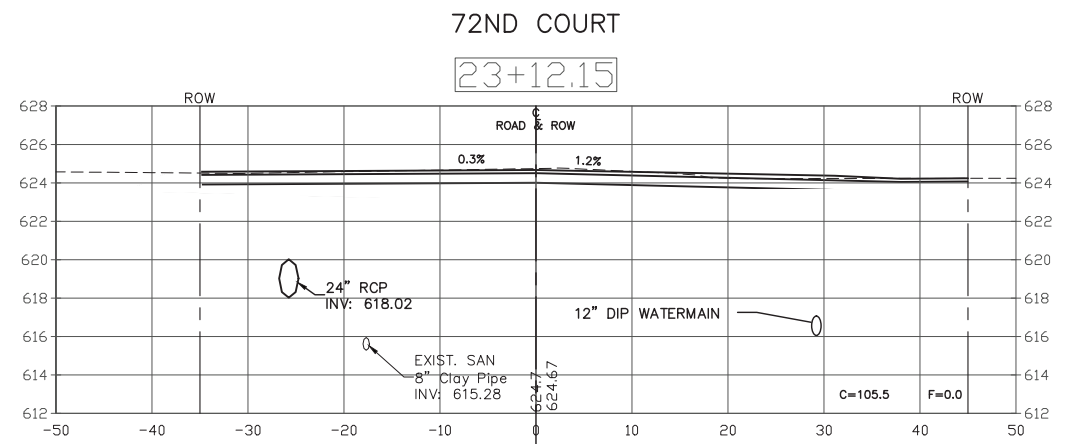
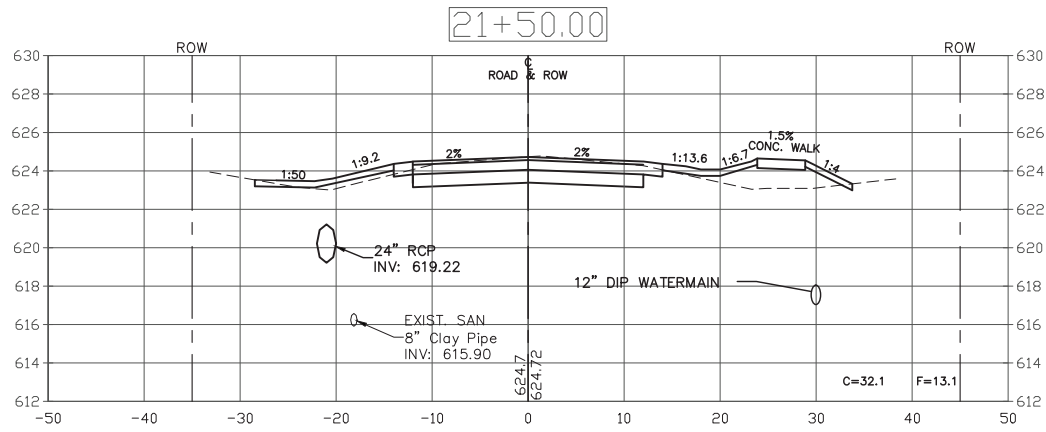
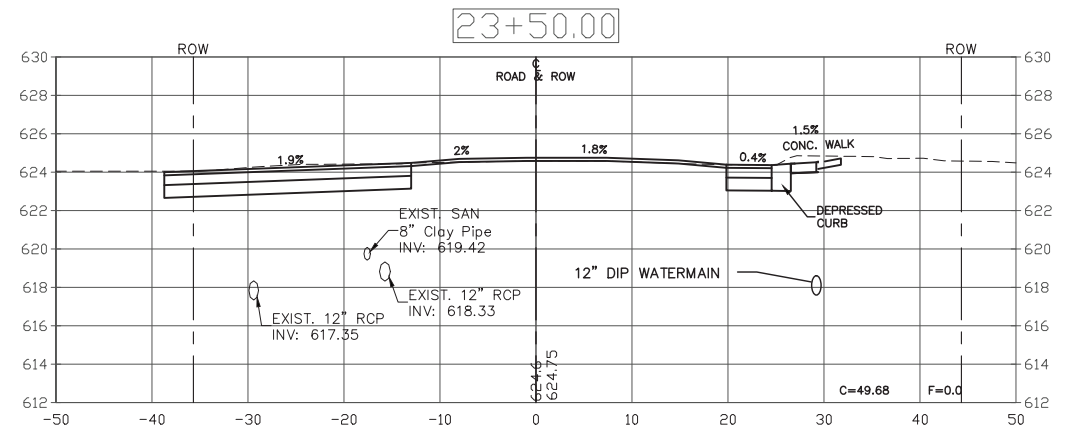
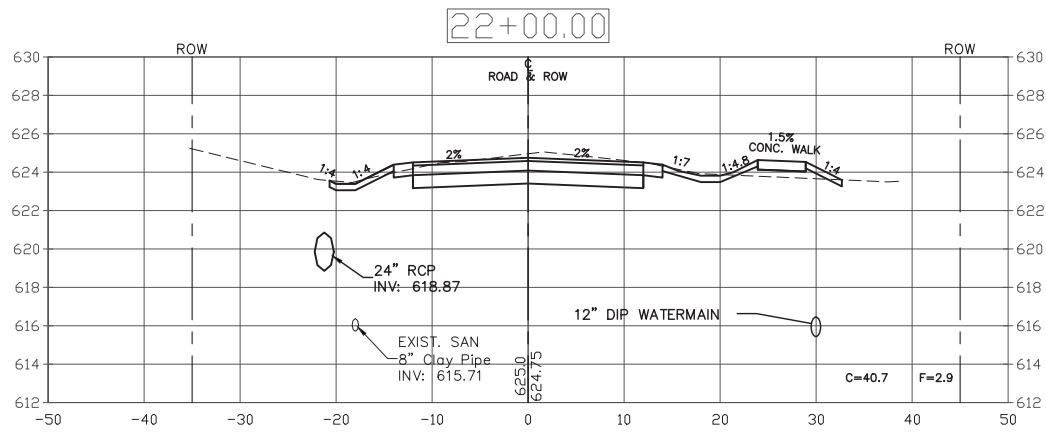
CROSS SECTIONS

SCALE: H1'=10' V1'=5' SHEET NO. 7 OF 9 SHEETS STA. 17+50 TO STA. 20+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	44
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	



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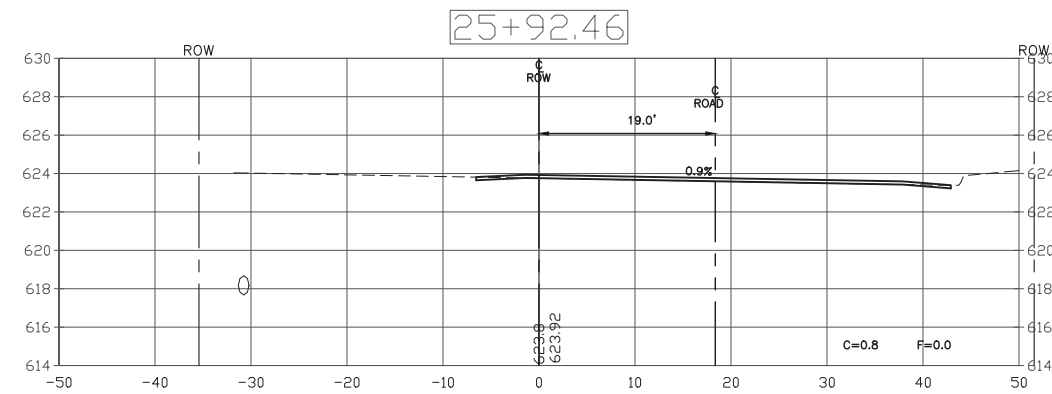
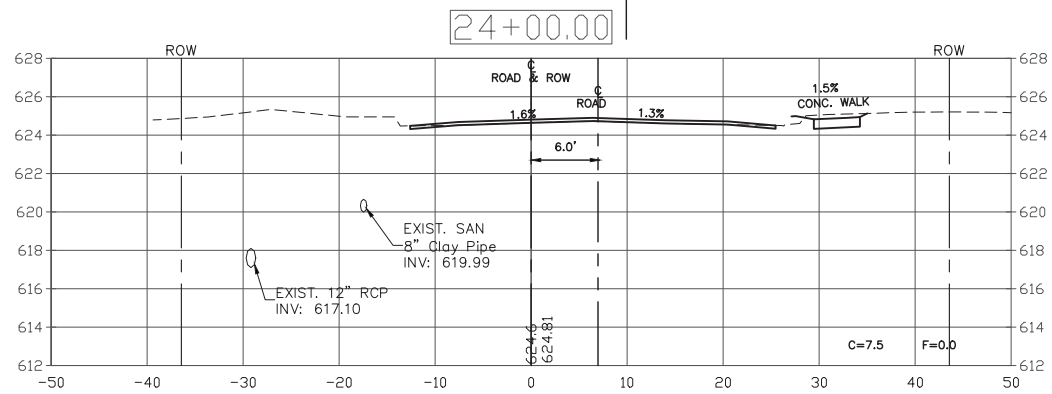
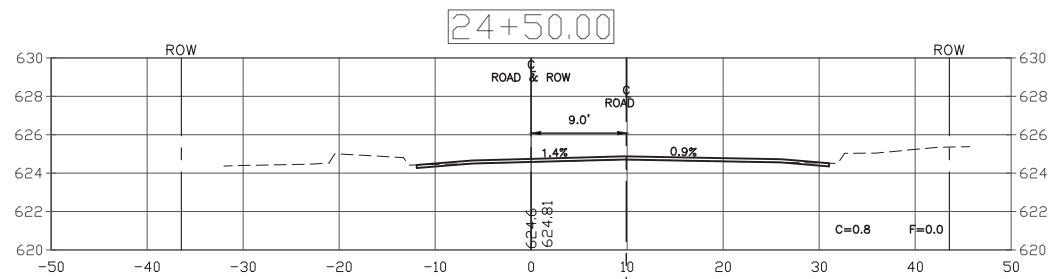
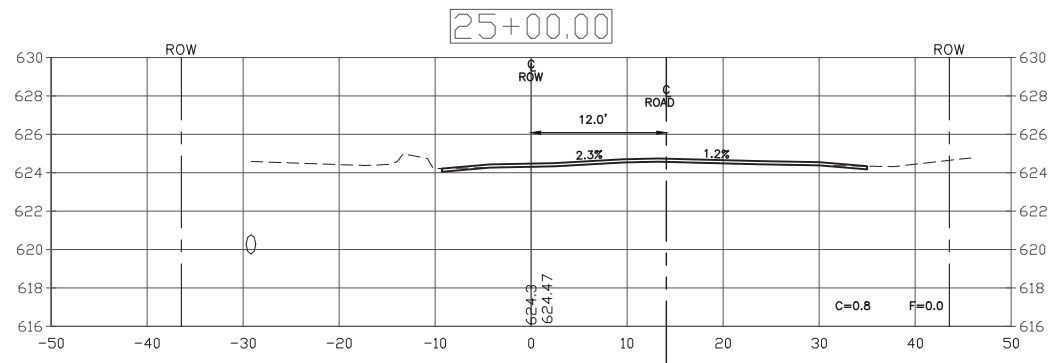
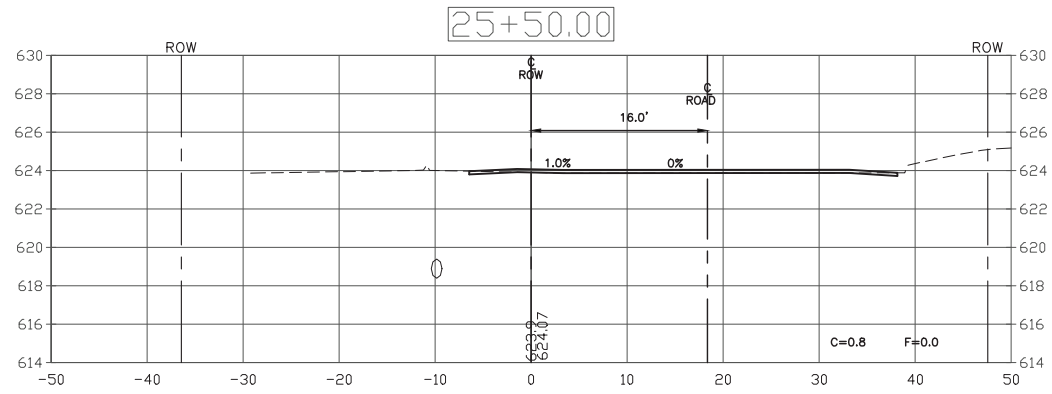
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CROSS SECTIONS

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	45
SCALE: H1"=10' V1"=5'			SHEET NO. 8 OF 9 SHEETS	
STA. 21+00 TO STA. 23+50			CONTRACT NO. 61G81	

PLOT DATE: Jan 26, 2021  
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DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS**

SCALE: H1"=10' V1"=5' SHEET NO. 9 OF 9 SHEETS STA. 24+00 TO STA. 25+92.46

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1710	15-00053-00-PV	COOK	46	46
CONTRACT NO. 61G81			ILLINOIS FED. AID PROJECT	