

VILLAGE OF LANSING LANSING, ILLINOIS

CONSTRUCTION PLANS FOR LANSING MUNICIPAL AIRPORT

REHABILITATE PORTIONS OF TAXIWAY C AND F; REMOVE TAXIWAY F2 AND ABANDONED TAXIWAYS

SUMMARY OF QUANTITIES

| ITEM NO. | DESCRIPTION | UNIT | ESTIMATED QUANTITY | RECORD QUANTITY |
|----------|---------------------------------------|------|--------------------|-----------------|
| AR108066 | REIL CABLE | LF | 700 | |
| AR108158 | 1/C #8 5 KV UG CABLE IN UD | LF | 775 | |
| AR110901 | CONCRETE DUCT REMOVAL | LF | 60 | |
| AR125565 | SPLICE CAN | EACH | 2 | |
| AR125901 | REMOVE STAKE MOUNTED LIGHT | EACH | 7 | |
| AR125902 | REMOVE BASE MOUNTED LIGHT | EACH | 2 | |
| AR125912 | REMOVE RETROREFLECTIVE MARKER | EACH | 6 | |
| AR125941 | ADJUST STAKE MOUNTED LIGHT | EACH | 1 | |
| AR150510 | ENGINEER'S FIELD OFFICE | LS | 1 | |
| AR152410 | UNCLASSIFIED EXCAVATION | CY | 740 | |
| AR152540 | SOIL STABILIZATION FABRIC | SY | 120 | |
| AR156520 | INLET PROTECTION | EACH | 6 | |
| AR208515 | POROUS GRANULAR EMBANKMENT | CY | 40 | |
| AR209606 | CRUSHED AGG. BASE COURSE - 6" | SY | 120 | |
| AR401610 | BITUMINOUS SURFACE COURSE | TON | 245 | |
| AR401650 | BITUMINOUS PAVEMENT MILLING | SY | 2000 | |
| AR401900 | REMOVE BITUMINOUS PAVEMENT | SY | 2200 | |
| AR401910 | REMOVE & REPLACE BIT. PAVEMENT | SY | 120 | |
| AR603510 | BITUMINOUS TACK COAT | GAL | 400 | |
| AR620520 | PAVEMENT MARKING - WATERBORNE | SF | 800 | |
| AR620525 | PAVEMENT MARKING - BLACK BORDER | SF | 1400 | |
| AR620900 | PAVEMENT MARKING REMOVAL | SF | 400 | |
| AR751940 | ADJUST INLET | EACH | 1 | |
| AR800140 | 2 1/C #6 XLP-USE, 1/C #10 GND - 1" UD | LF | 140 | |
| AR901510 | SEEDING | ACRE | 1.2 | |
| AR905530 | TOPSOILING | SY | 5800 | |
| AR908515 | HEAVY-DUTY HYDRAULIC MULCH | ACRE | 1.2 | |

INDEX TO SHEETS

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- CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS
- EXISTING CONDITIONS-REMOVALS - SHEET 1
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- STORMWATER POLLUTION PREVENTION PLAN NOTES AND DETAILS
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- PAVEMENT MARKING AND LIGHTING PLAN

ILLINOIS PROJECT: IGO-4658
SBGP PROJECT: 3-17-SBGP-156

JANUARY 8, 2021

Lansing Municipal Airport

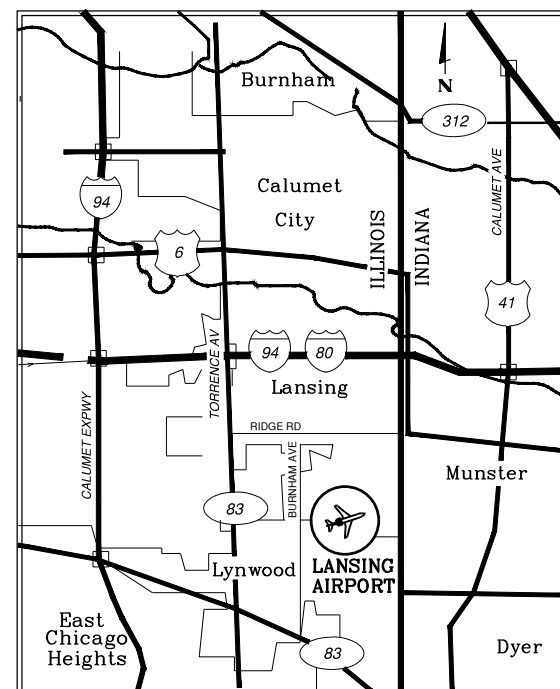
APPROVED BY *Daniel Podgorski*
DANIEL PODGORSKI
TITLE CITY ADMINISTRATOR
DATE January 8 2021

CMT
CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS

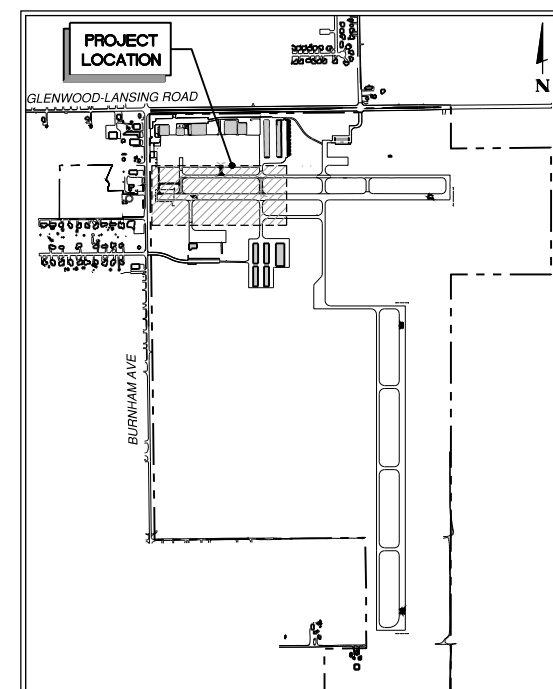
190297-02-00

ANTONIO R. MARIN
062-057994
STATE OF ILLINOIS
LICENSE EXPIRATION DATE: 11-30-2021

SUBMITTED BY *Antonio R. Marin*
ANTONIO R. MARIN, P.E.
DATE January 8 2021



LOCATION MAP



SITE PLAN

811 Know what's below. Call before you dig.

J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS
www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE. SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

DESIGN INFORMATION

APPROACH CATEGORY B
DESIGN GROUP II

LANSING MUNICIPAL AIRPORT

TOWNSHIP: 35 NORTH
RANGE: 15 EAST
COOK COUNTY

BLOOM TOWNSHIP
(SECTION: 8 AND 17)
OPPOSITE GLENWOOD-LANSING ROAD

UNICOM RADIO FREQUENCY - 122.7

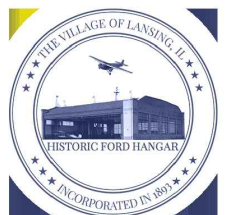


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JANUARY 8, 2021

REHABILITATE PORTIONS OF TAXIWAY C AND F; REMOVE TAXIWAY F2 AND ABANDONED TAXIWAYS



OWNER

VILLAGE OF LANSING

Lansing
Municipal Airport
LANSING MUNICIPAL AIRPORT
LANSING, ILLINOIS

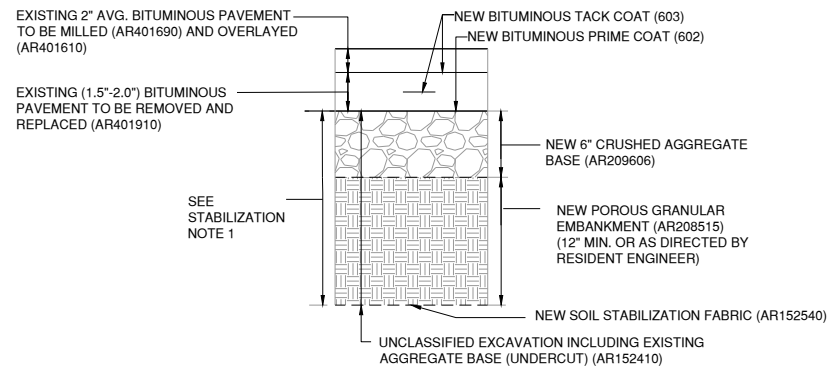
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| AIP PROJ. NO. 3-17-SBGP-156 |
| IL PROJ. NO. IGQ-4658 |
| CMT PROJECT NO: 190297-02-00 |
| CAD DWG FILE: G1101.DWG |
| DESIGNED BY: ARM |
| DRAWN BY: JRO |
| CHECKED BY: DKP |
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SHEET TITLE
GENERAL NOTES AND DETAILS

ELECTRICAL GENERAL NOTES

- ISOLATION TRANSFORMERS COME WITH A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/C #8 5 KV, L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
- TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR AND BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
- ALL SIGNS, LIGHTS AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT MANAGER, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE, AT NO ADDITIONAL COST.
- CONTRACTOR SHALL SURVEY THE LOCATIONS OF THE EXISTING IN-PAVEMENT DUCT MARKERS PRIOR TO MILLING THE SURFACE. AFTER PAVING IS COMPLETE, NEW DUCT MARKERS SHALL BE REINSTALLED, COSTS INCIDENTAL.
- CONTRACTOR SHALL HAVE THE OPTION TO USE EITHER SCHEDULE 40 PVC DUCT OR PE DUCT MEETING ASTM D1278/D3485 AND NEMA TC7.
- INSTALL 1/C #6 AWG BARE COPPER GROUND JUMPERS CONNECTED TO GROUND LUG BASE CAN OR STAKE AND EXOTHERMICALLY WELDED TO GROUND ROD. GROUND ROD AND GROUND WIRE SHALL BE INCIDENTAL TO CONTRACT.



STABILIZATION TYPICAL SECTION

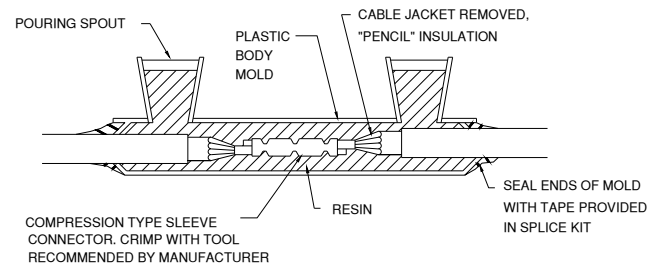
NOT TO SCALE

STABILIZATION NOTES:

- LOCATIONS FOR FURNISHING AND PLACING POROUS GRANULAR EMBANKMENT SHALL BE AS THE FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION. THIS MATERIAL IS INTENDED TO REPAIR SOFT SUBGRADE AS DIRECTED BY THE RESIDENT ENGINEER. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES. EXCAVATION OF THE SOFT SUBGRADE SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR UNCLASSIFIED EXCAVATION. (REFER TO STABILIZATION TYPICAL SECTION)

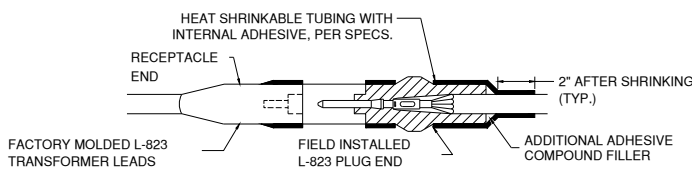
GENERAL NOTES

- THE EXISTING PAVEMENT STRUCTURE WAS DESIGNED FOR EXCLUSIVE USE BY SMALL AIRCRAFT. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE PAVEMENT STRUCTURE AND SUBGRADE FROM DAMAGE, WHICH MAY INCLUDE BUT NOT BE LIMITED TO USE OF TRACKED EQUIPMENT, SHORT HAUL TRUCKS OR TRACKED PAVERS, AT NO ADDITIONAL COST TO CONTRACT.
- AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH NEWLY CONSTRUCTED PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
- THE RESIDENT ENGINEER IN CONSULTATION WITH THE AIRPORT SHALL IDENTIFY ALL CRACKS TO BE REPAIRED. CONTRACTOR SHALL ADJUST THEIR OPERATIONS AS NECESSARY FOR THE IDENTIFIED WORK. NO ADDITIONAL COMPENSATION SHALL BE MADE.
- CONTRACTOR SHALL DISPOSE OF ALL PAVEMENT REMOVAL AND OTHER MISCELLANEOUS CONSTRUCTION DEBRIS OFF OF AIRPORT PROPERTY UNLESS OTHERWISE NOTED ON PLANS.
- PAVEMENT REMOVAL AND REPLACEMENT AREAS SHALL BE LAID OUT BY THE RESIDENT ENGINEER IN THE FIELD DURING CONSTRUCTION.
- THE FIRST LANE OF THE FIRST LIFT OF THE HMA SURFACE COURSE SHALL BE STARTED AT THE CENTER OF THE PAVEMENT WITH A TAUT STRINGLINE (GUIDE WIRE) SET TO GRADE AT BOTH SIDES OF THE PAVER. THE AUTOMATIC GRADE CONTROL SYSTEM OF THE PAVER SHALL BE USED TO CONTROL GRADE OF BOTH SIDES OF THE PAVER FROM THESE REFERENCE STRINGLINES. THE GRADE CONTROL FOR THE ADJACENT LANES OF PAVEMENT SHALL BE MAINTAINED BY USING A MATCHING SHOE WITH THE PREVIOUS LAID PAVEMENT AND A STRINGLINE ON THE OUTER EDGE OF THE NEXT LANE. A STRINGLINE AND MATCHING SHOE SHALL BE USED TO PAVE ALL REMAINING LANES OF THE FIRST LIFT OF SURFACE COURSE.



TYPE A - CABLE SPLICE

FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY
NOT TO SCALE

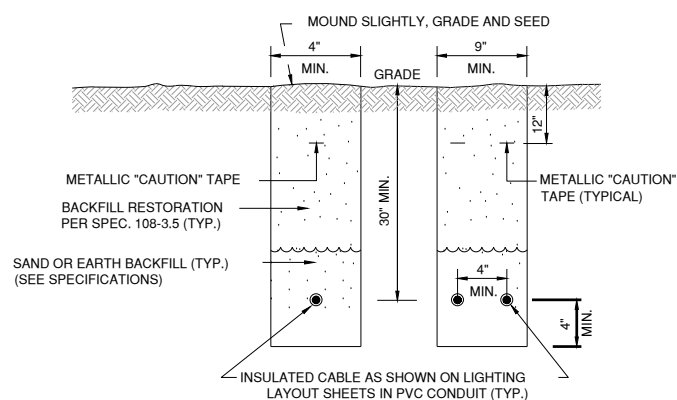


TYPE C AND D - CABLE SPLICE

FOR SPLICES AT RUNWAY/TAXIWAY LIGHTS AND SIGNS
NOT TO SCALE

SPLICE NOTES

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF FIVE (5) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.
- THE CONTRACTOR MAY ELECT TO USE AN FAA APPROVED PRIMARY CONNECTOR KIT IN LIEU OF HEAT SHRINK KIT AT NO ADDITIONAL COST.

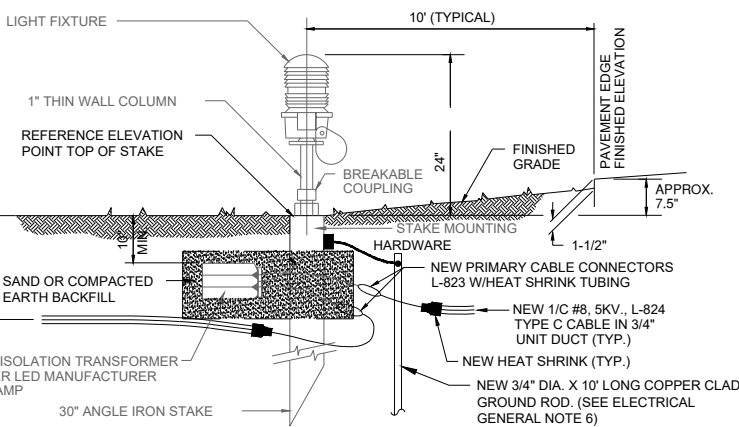


TRENCH NOTES

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO THE CONDUIT AND CABLE INSTALLATION.

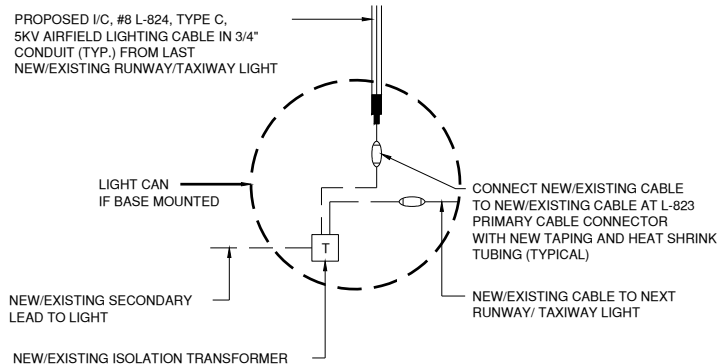
TRENCH DETAIL

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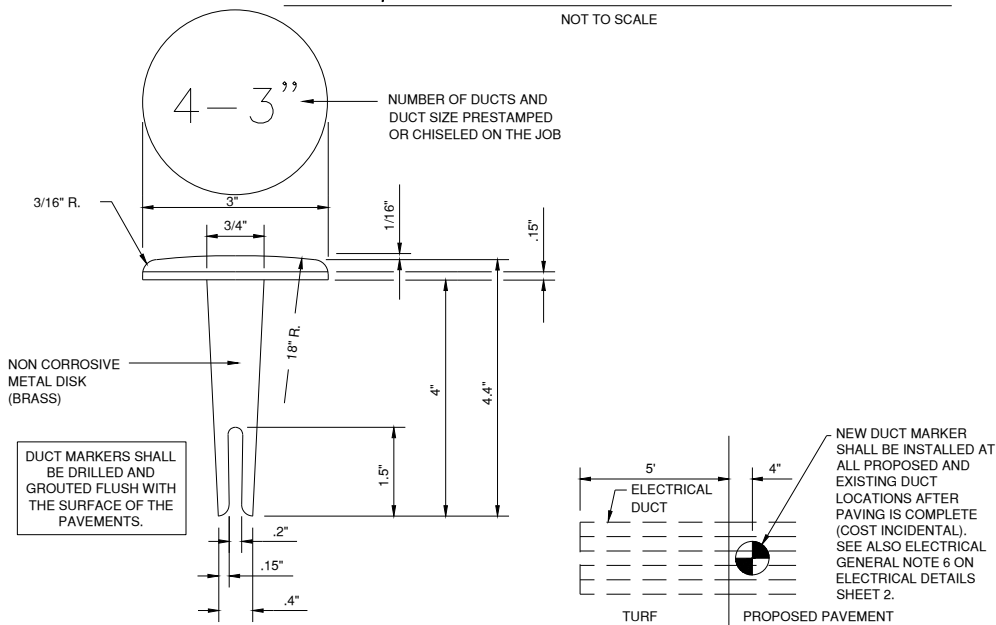
ADJUST STAKE MOUNTED EDGE LIGHT DETAIL

NOT TO SCALE



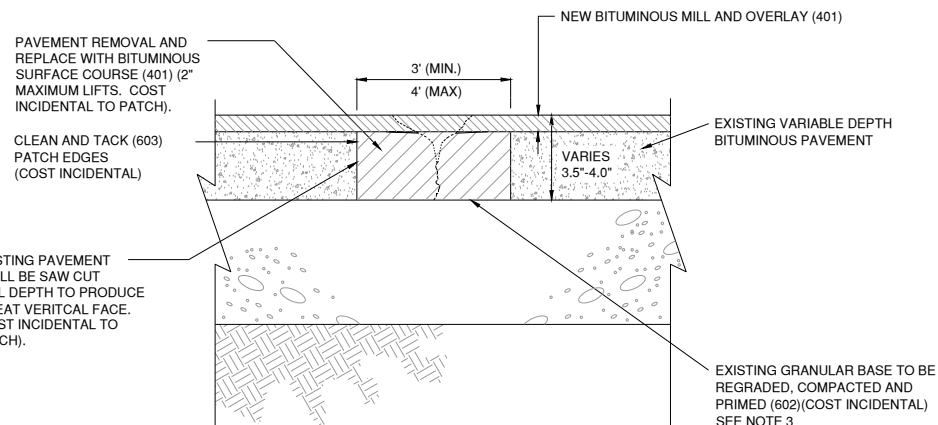
RUNWAY/TAXIWAY LIGHTING CIRCUIT CONNECTION DETAIL

NOT TO SCALE



ELECTRICAL DUCT MARKER DETAIL

NOT TO SCALE



REMOVE AND REPLACE BITUMINOUS PAVEMENT (AR401910)

NOT TO SCALE

PAVEMENT REMOVAL AND REPLACEMENT NOTES

- DEPTHS OF EXISTING PAVEMENT SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS, AIRPORT PERSONNEL, AND PAVEMENT CORES. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
- PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. EXISTING MEDIUM TO HIGH SEVERITY TRANSVERSE CRACKS ARE SPACED AT VARIABLE INTERVALS. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.
- AS FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION, FURNISHING AND PLACING OF POROUS GRANULAR EMBANKMENT AND/OR REMOVAL AND REPLACEMENT OF AGGREGATE BASE COURSE SHALL BE DONE AT SECTIONS WHERE THE REMOVE AND REPLACE BITUMINOUS PAVEMENT IS UTILIZED. THIS SHALL BE DONE TO REPAIR SOFT SUBGRADE AS DETERMINED BY THE RESIDENT ENGINEER BASED ON RESULTS OF THE GEOTECHNICAL TESTING OR PROOF ROLL. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES. SEE STABILIZATION TYPICAL SECTION FOR DETAIL.



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JANUARY 8, 2021

REHABILITATE PORTIONS OF TAXIWAY C AND F; REMOVE TAXIWAY F2 AND ABANDONED TAXIWAYS



OWNER

VILLAGE OF LANSING



Lansing Municipal Airport

LANSING MUNICIPAL AIRPORT
LANSING, ILLINOIS

| MARK | DATE | DESCRIPTION |
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AIP PROJ. NO. 3-17-SBGP-156

IL. PROJ. NO. IGQ-4658

CMT PROJECT NO: 190297-02-00

CAD DWG FILE: G1102.DWG

DESIGNED BY: ARM

DRAWN BY: JRO

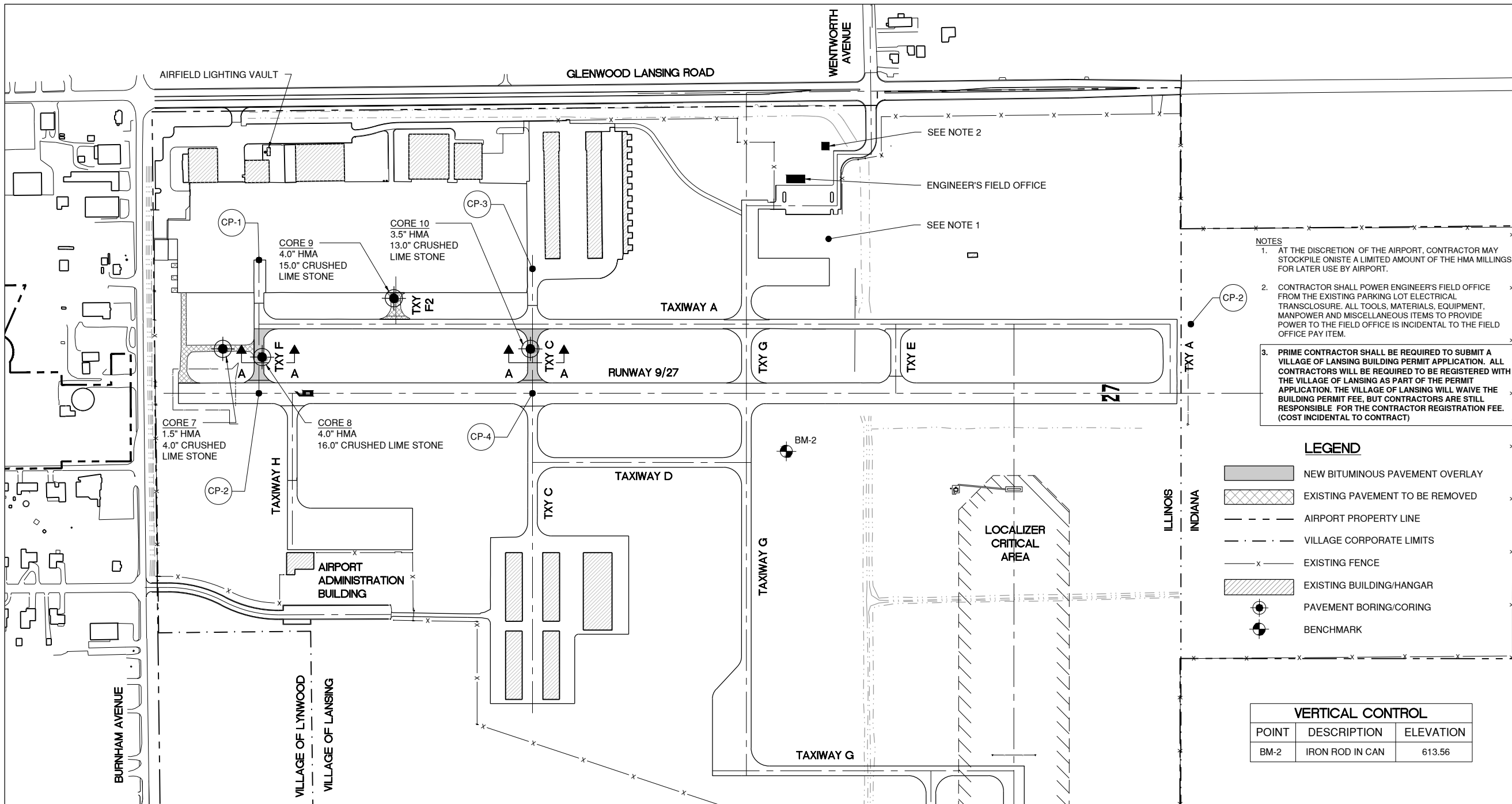
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SHEET TITLE

SITE PLAN



- NOTES**
1. AT THE DISCRETION OF THE AIRPORT, CONTRACTOR MAY STOCKPILE ON SITE A LIMITED AMOUNT OF THE HMA MILLINGS FOR LATER USE BY AIRPORT.
 2. CONTRACTOR SHALL POWER ENGINEER'S FIELD OFFICE FROM THE EXISTING PARKING LOT ELECTRICAL ENCLOSURE. ALL TOOLS, MATERIALS, EQUIPMENT, MANPOWER AND MISCELLANEOUS ITEMS TO PROVIDE POWER TO THE FIELD OFFICE IS INCIDENTAL TO THE FIELD OFFICE PAY ITEM.
 3. PRIME CONTRACTOR SHALL BE REQUIRED TO SUBMIT A VILLAGE OF LANSING BUILDING PERMIT APPLICATION. ALL CONTRACTORS WILL BE REQUIRED TO BE REGISTERED WITH THE VILLAGE OF LANSING AS PART OF THE PERMIT APPLICATION. THE VILLAGE OF LANSING WILL WAIVE THE BUILDING PERMIT FEE, BUT CONTRACTORS ARE STILL RESPONSIBLE FOR THE CONTRACTOR REGISTRATION FEE. (COST INCIDENTAL TO CONTRACT)

LEGEND

- [Hatched Box] NEW BITUMINOUS PAVEMENT OVERLAY
- [Cross-hatched Box] EXISTING PAVEMENT TO BE REMOVED
- [Dashed Line] AIRPORT PROPERTY LINE
- [Dotted Line] VILLAGE CORPORATE LIMITS
- [Line with X] EXISTING FENCE
- [Hatched Box] EXISTING BUILDING/HANGAR
- [Circle with dot] PAVEMENT BORING/CORING
- [Circle with cross] BENCHMARK

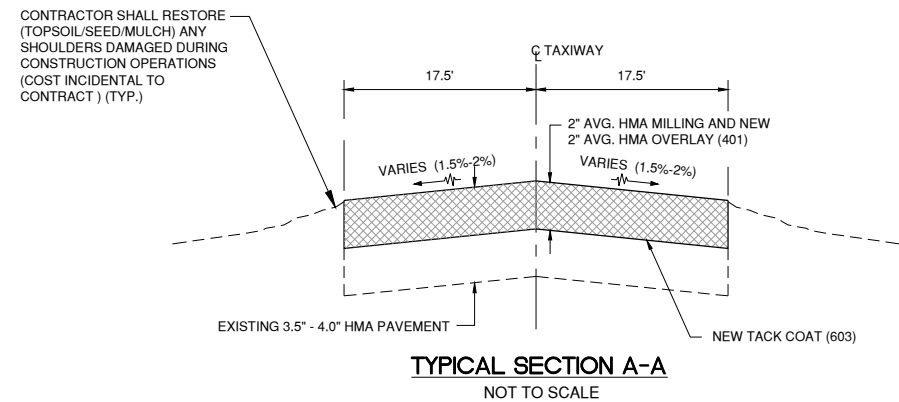
VERTICAL CONTROL

| POINT | DESCRIPTION | ELEVATION |
|-------|-----------------|-----------|
| BM-2 | IRON ROD IN CAN | 613.56 |

PRIME CONTRACTOR SHALL BE REQUIRED TO SUBMIT A VILLAGE OF LANSING BUILDING PERMIT APPLICATION. ALL CONTRACTORS WILL BE REQUIRED TO BE REGISTERED WITH THE VILLAGE OF LANSING AS PART OF THE PERMIT APPLICATION. THE VILLAGE OF LANSING WILL WAIVE THE BUILDING PERMIT FEE, BUT CONTRACTORS ARE STILL RESPONSIBLE FOR THE CONTRACTOR REGISTRATION FEES.

HORIZONTAL CONTROL (NAD 27)

| POINT | STATION | NORTHING | EASTING |
|-------|--------------------------|------------|-----------|
| CP-1 | STA. 7+31.80 @ TAXIWAY A | 1776370.03 | 717848.66 |
| CP-2 | STA. 2+50 @ TAXIWAY F | 1775888.25 | 717851.57 |
| CP-3 | STA. 5+50 @ TAXIWAY C | 1776345.11 | 718838.09 |
| CP-4 | STA. 10+00 @ TAXIWAY C | 1775895.11 | 718841.22 |



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SHEET TITLE
**CONSTRUCTION
ACTIVITY PLAN - 1**

GC101
SHEET 4 OF 12

NOTES

- CONTRACTOR SHALL NOTIFY THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER PRIOR TO THE INITIATION OF ANY WORK WHICH REQUIRES CLOSURE OF ACTIVE AIRFIELD PAVEMENT.
- ALL WORK AREAS SHALL BE SWEEPED AND CLEANED TO THE SATISFACTION OF THE AIRPORT PRIOR TO RE-OPENING PAVEMENT TO AIRCRAFT OPERATIONS.
- RUNWAY 18/36 SHALL REMAIN OPEN AT ALL TIMES.
- CONTRACTOR SHALL NOTIFY THE AIRPORT AND RESIDENT ENGINEER A MINIMUM OF 2 CALENDAR DAYS IN ADVANCE OF THE ANTICIPATED WORK ON TAXIWAY C ADJACENT TO TAXIWAY IN ORDER TO NOTIFY AIRFIELD TENANTS.
- TAXIWAY C: ADDITIONAL CONTRACTOR RESTRICTIONS (WORK AREA 2):
 - WHEN WORKING BETWEEN TAXIWAY A EDGE OF PAVEMENT AND TAXIWAY C, CONTRACTOR SHALL BE REQUIRED TO BE IN RADIO CONTACT WITH AIRFIELD TRAFFIC.
 - CONTRACTOR SHALL BE REQUIRED TO MOVE PERSONNEL, EQUIPMENT AND MATERIALS TO PROVIDE A MINIMUM 65.5 - FOOT OF CLEARANCE FROM THE TAXIWAY A CENTERLINE TO ALLOW THE PASSAGE OF THE AIRCRAFT.
 - CONTRACTOR SHALL BE REQUIRED TO FURNISH FLAGGERS TO DIRECT AND CONTROL ALL CONSTRUCTION EQUIPMENT AND PERSONNEL TO THE REQUIRED SET BACK DISTANCE FOR THE SAFE PASSAGE OF AIRCRAFT, AIRFIELD PERSONNEL AND/OR AIRPORT PERSONNEL. AIRPORT PERSONNEL SHALL ALSO BE ON-SITE AND IN CONJUNCTION WITH THE CONTRACTOR'S FLAGGER SHALL ACT AS A WING WALKER FOR AIRCRAFT PASSING THROUGH THE WORK ZONE.
- CONTRACTOR SHALL MAINTAIN AIRFIELD GATES CLOSED WHEN NOT IN USE. IF THE CONTRACTOR REQUIRES THE GATE TO REMAIN OPEN FOR LONG DURATIONS, A GATE GUARD SHALL BE PROVIDED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

ELECTRICAL NOTES

- CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.
- CONTRACTOR SHALL TURN OFF RUNWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY CLOSURES, CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY. THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.

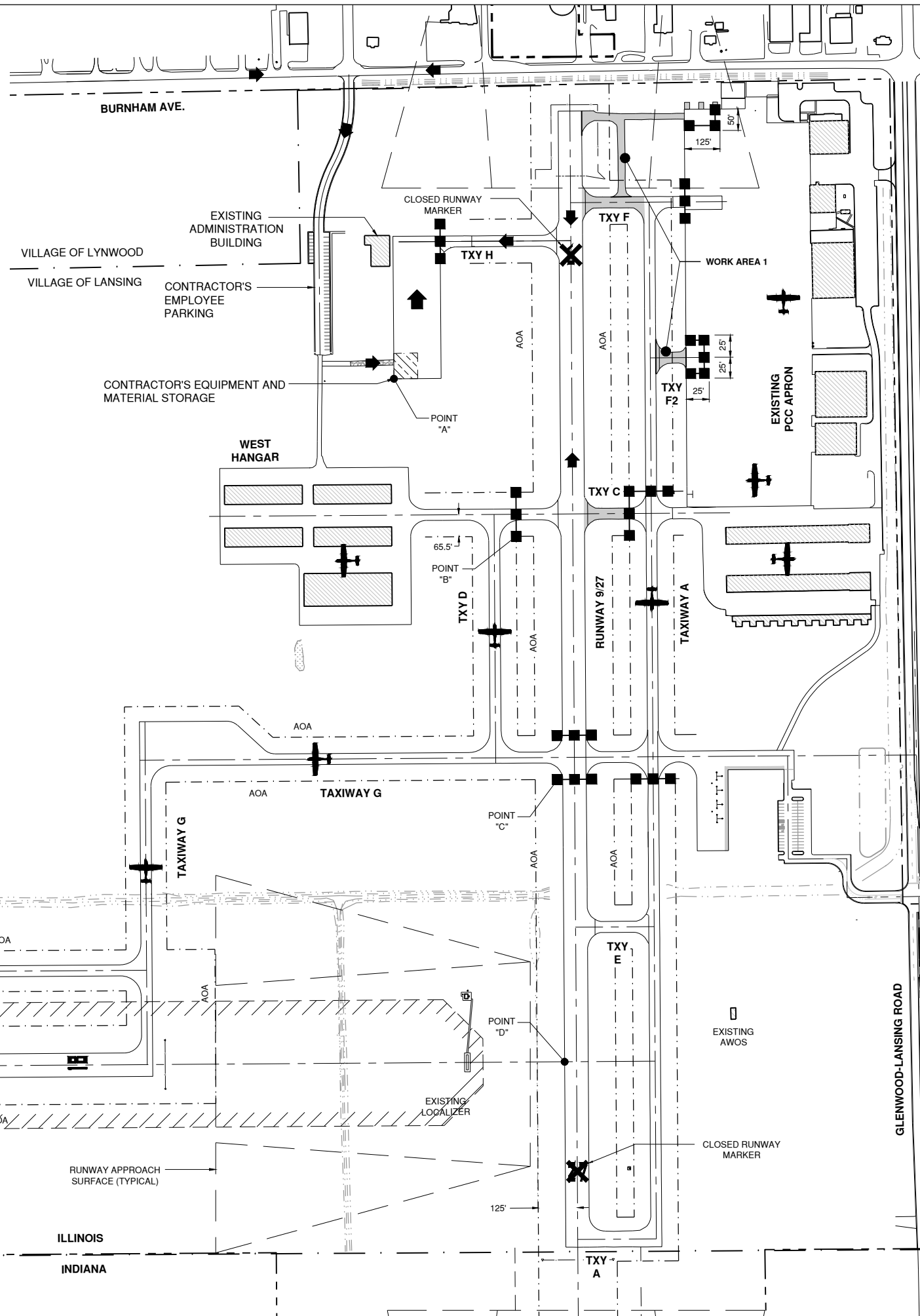
SUGGESTED SEQUENCE OF CONSTRUCTION

- NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER 10 DAYS PRIOR TO THE START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
- WORK AREA 1
 - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.
 - PLACE REQUIRED BARRICADES AND PAVEMENT CLOSURE MARKERS.
 - COVER AND/OR DE-ENERGIZE SIGNS AND LIGHTS FOR CLOSED TAXIWAYS AND RUNWAYS.
 - COMPLETE IMPROVEMENTS.
 - REMOVE TEMPORARY COVERS AND RE-ENERGIZE CIRCUITS WITHIN WORK AREA LIMITS.
 - CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
 - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER PAVEMENT CLOSURES FOR NEXT WORK AREA.
 - RELOCATE BARRICADES AND PAVEMENT CLOSURE MARKERS FOR NEXT WORK AREA.
- RESTORE STAGING AREAS.

LEGEND

- WORK AREA 1
IDOT TYPE 2 BARRICADE W/ RED LIGHT
- WORK AREA 2
IDOT TYPE 2 BARRICADE W/ RED LIGHT
- WORK AREA 2
LOW PROFILE BARRICADE W/ RED LIGHT
- CONTRACTOR'S EQUIPMENT/MATERIAL STORAGE
- A.O.A.
- HAUL ROUTE
- CLOSED RUNWAY MARKER
- TEMPORARY STOP SIGN AND CONTRACTOR CROSSING GUARD/FLAGGER
- AIRCRAFT MOVEMENT AREA

| WORK AREA | ALLOWABLE WORK PERIODS | OPERATIONAL STATUS/RESTRICTIONS |
|-----------|------------------------|---------------------------------------------------------------------------------------------------------------------------------|
| AREA 1 | NO RESTRICTIONS | RUNWAY 18/36 OPEN; RUNWAY 9/27 CLOSED; TAXIWAYS F, F2, E AND H CLOSED; TAXIWAY A AND C PARTIAL CLOSURE; ALL OTHER TAXIWAYS OPEN |



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| DRAWN BY: JRO | | |
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SHEET TITLE
**CONSTRUCTION
ACTIVITY PLAN - 2**

GC102
SHEET 5 OF 12

LEGEND

- WORK AREA 1
IDOT TYPE 2 BARRICADE W/ RED LIGHT
- WORK AREA 2
IDOT TYPE 2 BARRICADE W/ RED LIGHT
- WORK AREA 2
LOW PROFILE BARRICADE W/ RED LIGHT
- CONTRACTOR'S EQUIPMENT/MATERIAL STORAGE
- AIR OPERATIONS AREA (A.O.A.)
ACTIVE RUNWAY 18/36 125' CENTERLINE TO A.O.A.
ACTIVE RUNWAY 9/27 125' CENTERLINE TO A.O.A.
ACTIVE TAXIWAY 65.5' CENTERLINE TO A.O.A.
ACTIVE TAXILANE 57.5' CENTERLINE TO A.O.A.
- HAUL ROUTE
- CLOSED RUNWAY MARKER
- TEMPORARY STOP SIGN AND CONTRACTOR CROSSING GUARD/FLAGGER
- AIRCRAFT MOVEMENT AREA

SUGGESTED SEQUENCE OF CONSTRUCTION

- NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER 7 DAYS PRIOR TO THE START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
- WORK AREAS 2
 - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.
 - PLACE REQUIRED BARRICADES AND PAVEMENT CLOSURE MARKERS.
 - COVER AND/OR DE-ENERGIZE SIGNS AND LIGHTS FOR CLOSED TAXIWAYS AND RUNWAYS.
 - COMPLETE IMPROVEMENTS.
 - REMOVE TEMPORARY COVERS AND RE-ENERGIZE CIRCUITS WITHIN WORK AREA LIMITS.
 - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER PAVEMENT CLOSURES FOR NEXT WORK AREA.
 - CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
 - RELOCATE BARRICADES AND PAVEMENT CLOSURE MARKERS FOR NEXT WORK AREA.
- RESTORE STAGING AREAS.

| WORK AREA | ALLOWABLE WORK PERIODS | OPERATIONAL STATUS/RESTRICTIONS |
|-----------|------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------|
| AREA 2 | | RUNWAY 9/27 CLOSED; RUNWAY 18/36 OPEN; TAXIWAYS F, F2 AND E CLOSED; TAXIWAY A AND C PARTIAL CLOSURE; ALL OTHER TAXIWAYS OPEN (SEE NOTES 4 AND 5) |

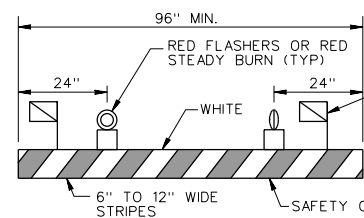
SEE CONSTRUCTION ACTIVITY PLAN SHEET 1 FOR NOTES

BARRICADE NOTES

1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
7. PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
8. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



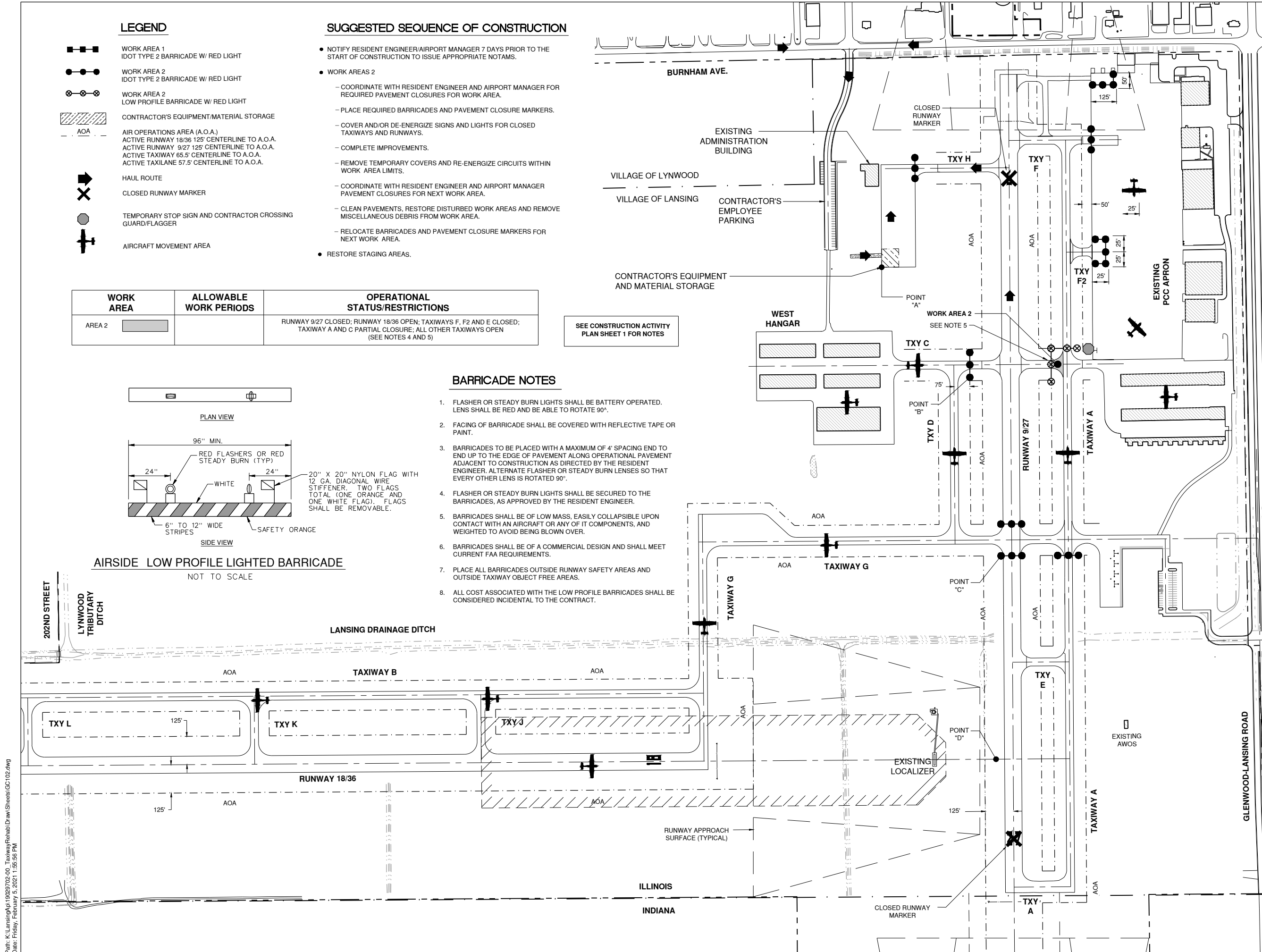
PLAN VIEW



SIDE VIEW

AIRSIDE LOW PROFILE LIGHTED BARRICADE

NOT TO SCALE



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GENERAL NOTES

- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2G (LATEST EDITION) OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
- THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
- FOR WORK ON AIRPORT PROPERTY: PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, TAXIWAY AND RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT. BARRICADES AT 10-FOOT CENTERS WITH ONE ORANGE FLAG (20" x 20") BETWEEN EACH SET OF BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A FLASHING RED LIGHT AND CONFORM TO IDOT STANDARD 702001, TYPE II. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL, THIS SHEET.
- IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE A TRUCK WHICH HAS A MAXIMUM HEIGHT OF 15 FEET.
- IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.
- LANSING MUNICIPAL AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE CONSTRUCTION ACTIVITY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
- MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT. THE RESTORATION SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM POINT TO POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.

- COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT. EXISTING LANSING DRAINAGE FLOWS SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 65.5' FROM ACTIVE TAXIWAYS, 57.5' FROM ACTIVE TAXILANES, 200' FROM ACTIVE RUNWAY 18/36 AND 75' FROM ACTIVE RUNWAY 9/27 UNLESS OTHERWISE APPROVED BY THE AIRPORT MANAGER.
- CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTIONS 50-06, 70-06, AND 70-17 OF STANDARD SPECIFICATIONS REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTOR'S WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER AND/OR AIRPORT MAINTENANCE. ANY DEFECIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.

CONTRACTOR CROSSING RUNWAY AND TAXIWAY AIR OPERATIONS AREA (A.O.A.)

ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A CROSSING GUARD OR ESCORT IN RADIO CONTACT WITH THE AIR FIELD SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.

ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEP TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

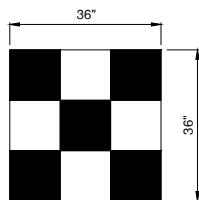
LIMITATIONS ON CONSTRUCTION WITHIN RUNWAY SAFETY AREA (RSA) AND TAXIWAY/TAXILANE OBJECT FREE AREA (TOFA)

RUNWAYS:

THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING DAY THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED. AT LEAST ONE OF THE RUNWAYS SHALL REMAIN IN OPERATION AT ALL TIMES. IF NECESSARY STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE RSA IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT. REFERENCE TABLE ON PREVIOUS SHEET FOR SAFETY AREA WIDTHS.

TAXIWAYS / TAXILANES:

ANY WORK WITHIN TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) WILL REQUIRE A TAXIWAY / TAXILANE CLOSURE. WORK WITHIN THE TOFA SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE TOFA. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. OBJECT FREE AREA WIDTHS ARE NOTED ON THIS SHEET. NO DROP-OFFS OR OPEN EXCAVATIONS WILL BE ALLOWED WITHIN THE TAXIWAY / TAXILANE SAFETY AREAS OF OPEN TAXIWAYS / TAXILANES.



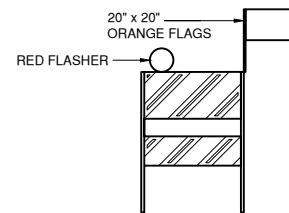
CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL ORANGE AND WHITE CHECKERED FLAG
NOT TO SCALE

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS.

**DESIGN AIRCRAFT APPROACH CATAGORY: B
DESIGN AIRPORT GROUP: II**

RUNWAY 18/36 SAFETY AREA TOTAL WIDTH = 150'
RUNWAY 9/27 SAFETY AREA TOTAL WIDTH = 150'
TAXIWAY CENTERLINE TO OBJECT SEPARATION = 65.5'
TAXILANE CENTERLINE TO OBJECT SEPARATION = 57.5'

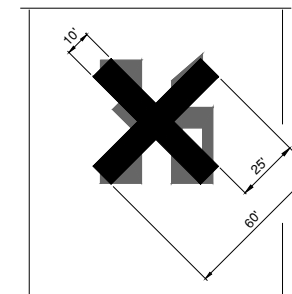
| MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 15' | | | |
|-----------------------------------------------------------|----------------------------------------------------------|-----------------------------------------------------------|----------------------------------------------------------|
| NEAREST POINT ON CONSTRUCTION SITE TO ACTIVE RUNWAY 18/36 | POINT "A" | NEAREST POINT ON CONSTRUCTION SITE TO ACTIVE RUNWAY 18/36 | POINT "C" |
| | LATITUDE: 41° 32' 23.06" | | LATITUDE: 41° 32' 22.90" |
| | LONGITUDE: - 87° 31' 39.41" EXISTING ELEVATION: 616.1 | | LONGITUDE: - 87° 31' 51.21" EXISTING ELEVATION: 613.9 |
| NEAREST POINT ON CONSTRUCTION SITE TO ACTIVE RUNWAY 18/36 | POINT "B" | NEAREST POINT ON CONSTRUCTION SITE TO ACTIVE RUNWAY 18/36 | POINT "D" |
| | LATITUDE: 41° 32' 21.69" | | LATITUDE: 41° 32' 25.61" |
| | LONGITUDE: - 87° 32' 01.39" EXISTING ELEVATION: 613.6 | | LONGITUDE: - 87° 31' 39.40" EXISTING ELEVATION: 615.3 |



**FLASHER BARRICADE
DETAIL-IDOT TYPE 2**
NOT TO SCALE

FLASHER BARRICADE NOTES

- FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
- SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
- CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING BARRICADES AT ALL TIMES TO THE SATISFACTION OF THE AIRPORT.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- PLACE AT 10' INTERVALS.
- ALTERNATE FLASHER LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.



**ON PAVEMENT
CLOSED RUNWAY MARKER DETAIL**
NOT TO SCALE

CLOSED RUNWAY MARKER DETAIL NOTES

- CLOSED RUNWAY MARKERS SHALL BE YELLOW.
- MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
- CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
- MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
- COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.

AIRFIELD LIGHTS AND SIGNS NOTES

- CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.
- CONTRACTOR SHALL TURN OFF RUNWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY CLOSURES, CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY. THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.

JANUARY 8, 2021

REHABILITATE PORTIONS OF TAXIWAY C AND F; REMOVE TAXIWAY F2 AND ABANDONED TAXIWAYS



OWNER

VILLAGE OF LANSING



**Lansing
Municipal Airport**

LANSING MUNICIPAL AIRPORT
LANSING, ILLINOIS

| MARK | DATE | DESCRIPTION |
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| AIP PROJ. NO. 3-17-SBGP-156 |
| IL PROJ. NO. IGQ-4658 |
| CMT PROJECT NO: 190297-02-00 |
| CAD DWG FILE: GC103.DWG |
| DESIGNED BY: ARM |
| DRAWN BY: JRO |
| CHECKED BY: DKP |
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**CONSTRUCTION
ACTIVITY PLAN
NOTES AND DETAILS**

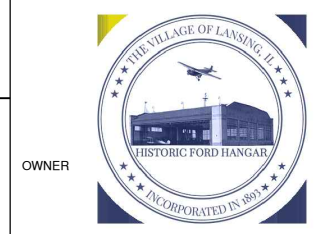


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FINAL

JANUARY 8, 2021
REHABILITATE PORTIONS OF TAXIWAY C AND F; REMOVE TAXIWAY F2 AND ABANDONED TAXIWAYS



VILLAGE OF LANSING

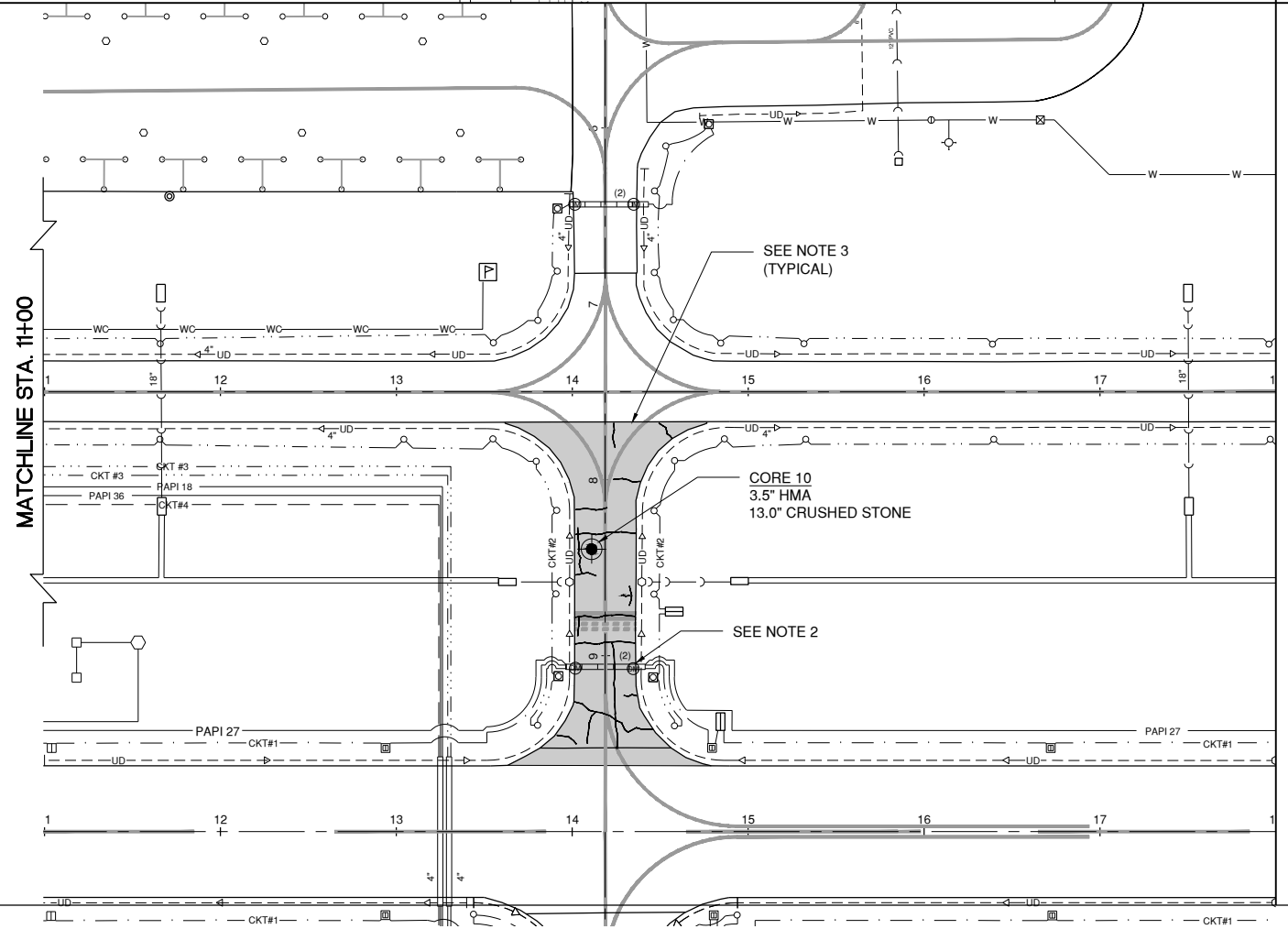
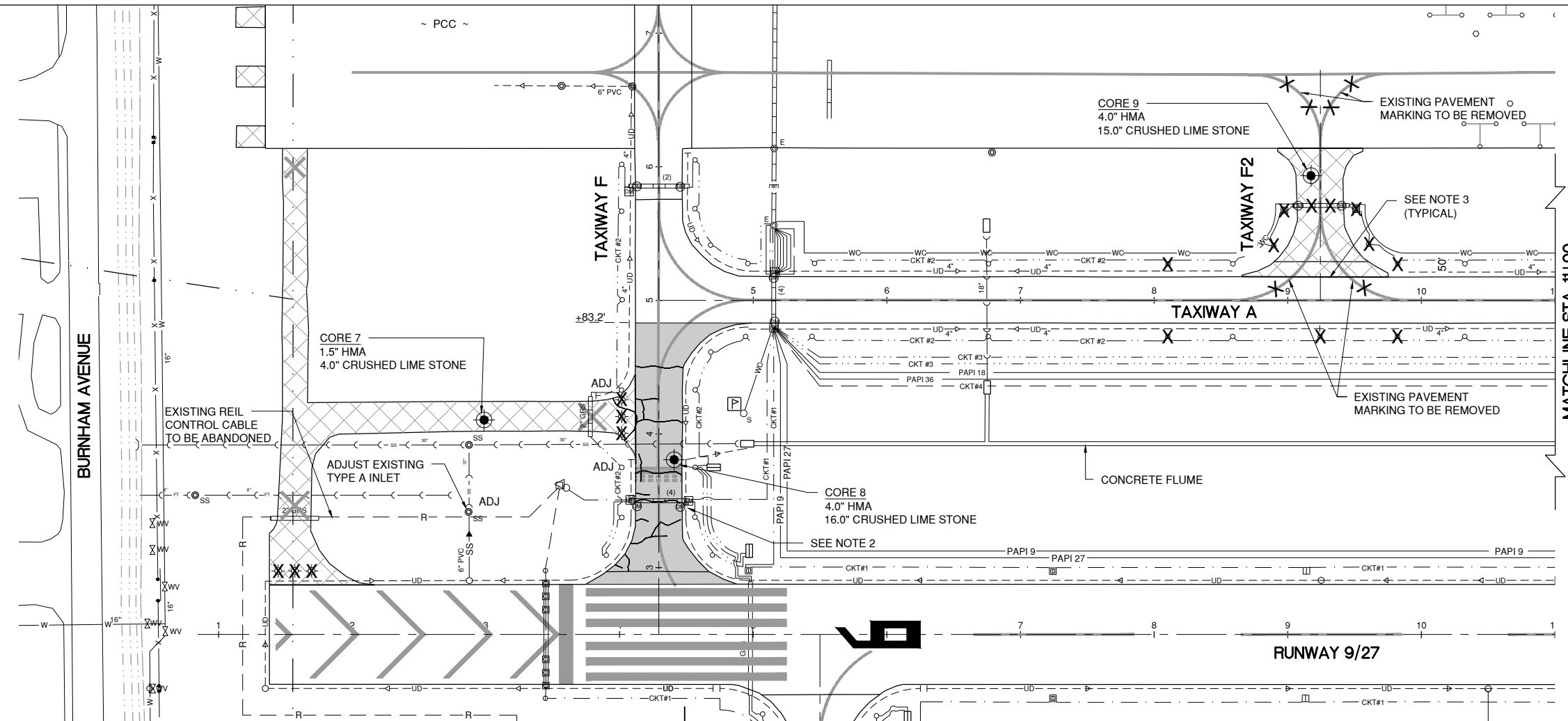
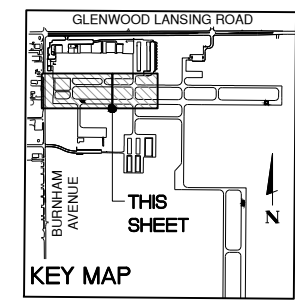


LANSING MUNICIPAL AIRPORT
LANSING, ILLINOIS

| MARK | DATE | DESCRIPTION |
|-----------------|---------------|-------------|
| AIP PROJ. NO. | 3-17-SBGP-156 | |
| IL PROJ. NO. | IGQ-4658 | |
| CMT PROJECT NO. | 190297-02-00 | |
| CAD DWG FILE: | CD101.DWG | |
| DESIGNED BY: | ARM | |
| DRAWN BY: | JRO | |
| CHECKED BY: | DKP | |
| APPROVED BY: | ARM | |
| COPYRIGHT: | | |

SHEET TITLE
EXISTING CONDITIONS - REMOVALS

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.



NOTES

- THE DETERMINATION AND LIMITS OF REMOVE AND REPLACE BITUMINOUS PAVEMENT SHALL BE COORDINATED WITH THE RESIDENT ENGINEER AFTER INSPECTION OF THE MILLED SURFACE.
- EXISTING DUCT MARKERS SHALL BE SURVEYED BY THE CONTRACTOR BEFORE SURFACE IS MILLED. DUCT MARKERS SHALL BE REPLACED AT SAME LOCATIONS SURVEYED (COST INCIDENTAL TO THE CONTRACT).
- THE EXISTING PAVEMENT TO BE REMOVED OR MILLED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.

EARTHWORK NOTES

- AREAS OF PAVEMENT REMOVAL AND ALL OTHER EXCAVATED AREAS REQUIRE A MINIMUM OF 4" TOPSOIL PLACEMENT. OVER EXCAVATION MAY BE REQUIRED TO PLACE THE TOPSOIL TO FINAL GRADE.
- ANY FILL REQUIRED TO BRING THE EXISTING SUBGRADE TO THE PROPER ELEVATION FOR TOPSOIL PLACEMENT SHALL NOT BE MEASURED FOR PAYMENT BUT SHALL BE CONSIDERED INCIDENTAL TO THE UNCLASSIFIED EXCAVATION (AR156410) PAY ITEM.
- TOPSOIL SALVAGED FROM CUT AREAS MAY BE UTILIZED TO SUPPLEMENT THE TOPSOIL REQUIRED FOR ITEM AR905530.
- A NOMINAL QUANTITY (65 CY) OF UNCLASSIFIED EXCAVATION IS INCLUDED FOR UNDERCUTS BELOW PAVEMENT PATCHES, IF REQUIRED.

LEGEND

| | |
|--|----------------------------------------------------------|
| | EXISTING HMA PAVEMENT TO BE MILLED |
| | EXISTING HMA PAVEMENT AND AGGREGATE BASE TO BE REMOVED |
| | PAVEMENT CORE/BORING |
| | EXISTING AIRFIELD LIGHTING CIRCUITS |
| | EXISTING RAIL CIRCUITS |
| | EXISTING PAPI CIRCUITS |
| | EXISTING AIRFIELD LIGHTS |
| | EXISTING AIRFIELD SIGNAGE |
| | EXISTING 2-BOX PAPI (L-881) |
| | EXISTING CRACK TO BE EVALUATED AFTER MILLING FOR REPAIR |
| | EXISTING WIND CONE |
| | EXISTING RAIL |
| | EXISTING STORM SEWER |
| | EXISTING UNDERDRAIN |
| | EXISTING DRAINAGE STRUCTURE |
| | EXISTING STORM SEWER END TREATMENT |
| | EXISTING CONCRETE ENCASED DUCT BANK (#OF WAYS INDICATED) |
| | EXISTING GRS CONDUIT (SIZE INDICATED) |
| | EXISTING RETROREFLECTIVE MARKER |
| | EXISTING ELECTRICAL SPLICE CAN |
| | EXISTING CLASS E FENCE |
| | EXISTING SLIDE GATE |
| | EXISTING WATER MAIN |
| | EXISTING WATER VALVE |
| | EXISTING FIRE HYDRANT |
| | EXISTING CONTOUR |
| | EXISTING ITEM TO BE ADJUSTED |

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CONSULTANTS



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FINAL

JANUARY 8, 2021

**REHABILITATE PORTIONS OF
TAXIWAY C AND F; REMOVE
TAXIWAY F2 AND ABANDONED
TAXIWAYS**



OWNER

VILLAGE OF LANSING



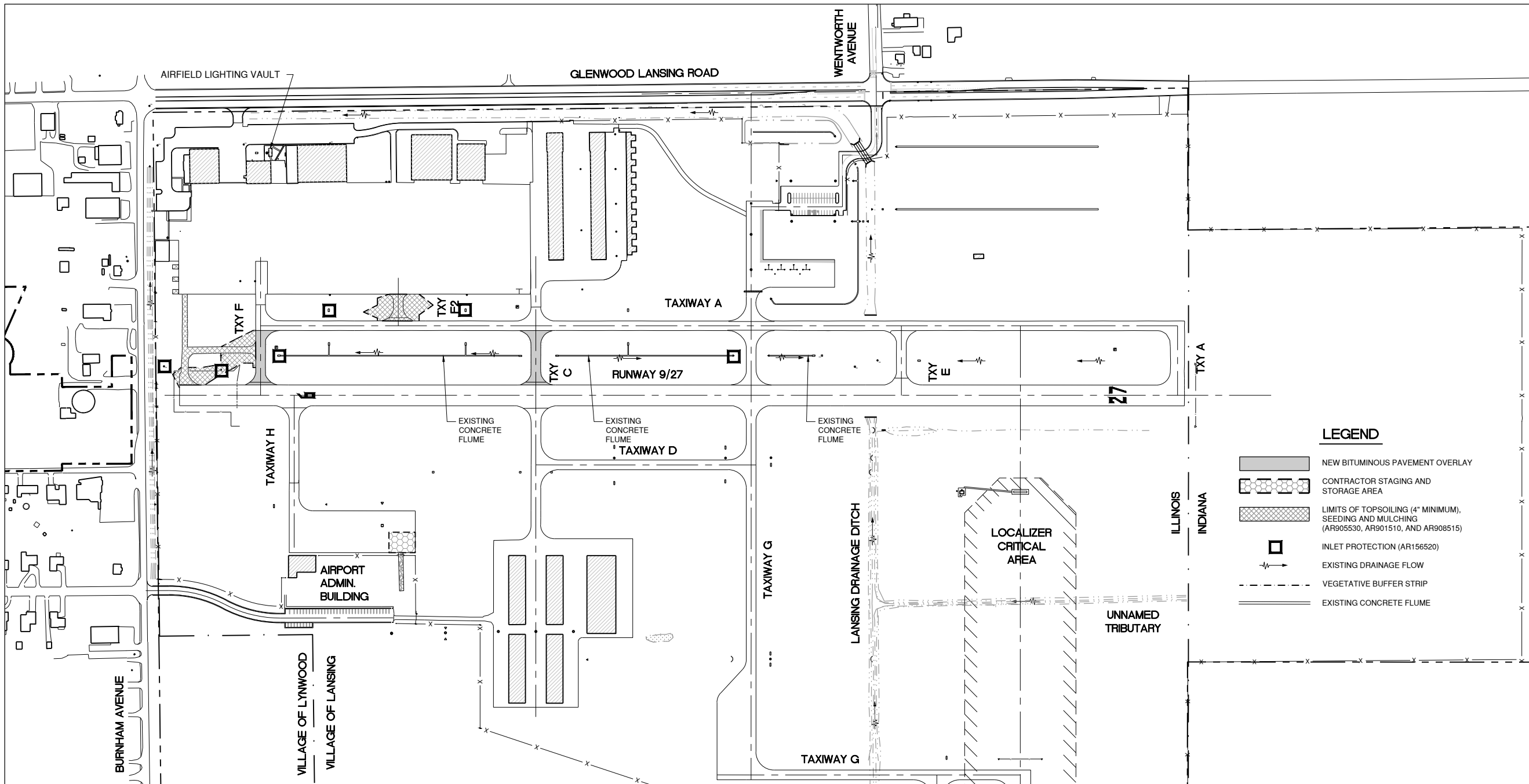
LANSING MUNICIPAL AIRPORT
LANSING, ILLINOIS

| MARK | DATE | DESCRIPTION |
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AIP PROJ. NO. 3-17-SBGP-156
 IL PROJ. NO. IGQ-4658
 CMT PROJECT NO: 190297-02-00
 CAD DWG FILE: L101.DWG
 DESIGNED BY: ARM
 DRAWN BY: JRO
 CHECKED BY: DPK
 APPROVED BY: ARM
 COPYRIGHT:

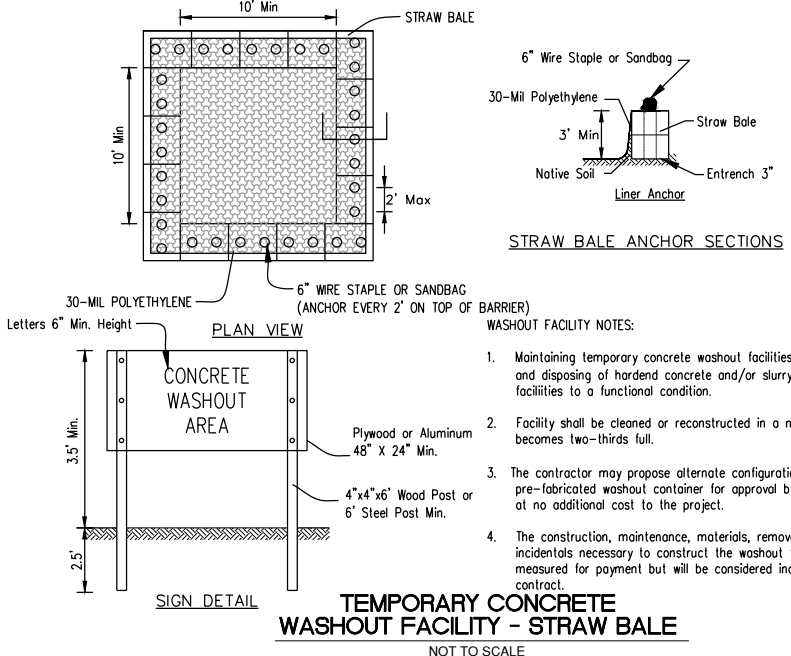
SHEET TITLE

**STORMWATER
POLLUTION
PREVENTION PLAN**



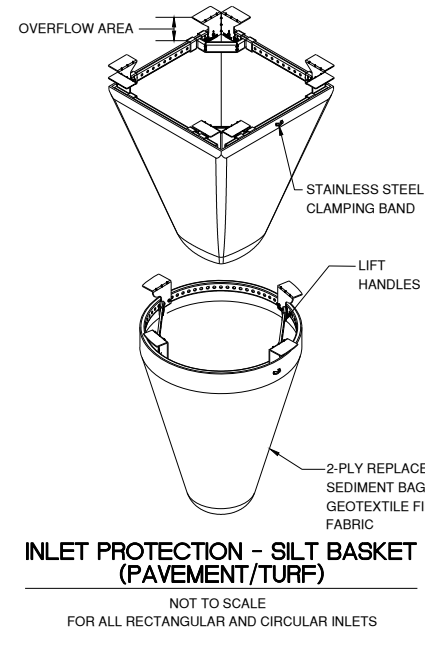
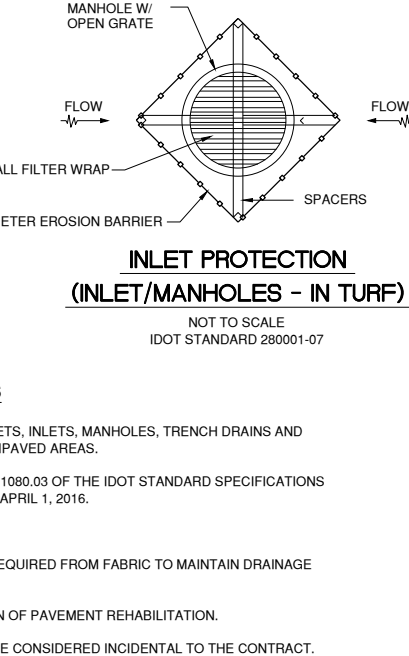
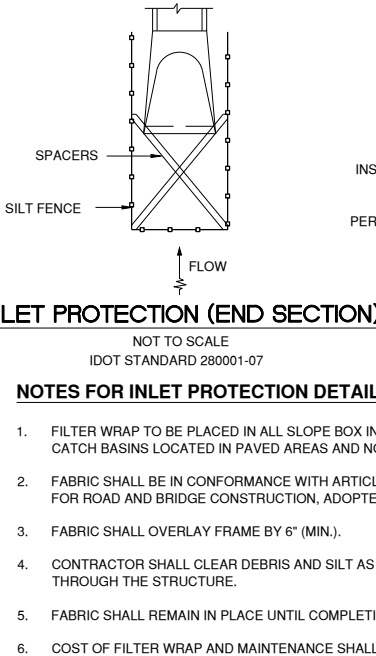
LEGEND

- NEW BITUMINOUS PAVEMENT OVERLAY
- CONTRACTOR STAGING AND STORAGE AREA
- LIMITS OF TOPSOILING (4" MINIMUM), SEEDING AND MULCHING (AR905530, AR901510, AND AR908515)
- INLET PROTECTION (AR156520)
- EXISTING DRAINAGE FLOW
- VEGETATIVE BUFFER STRIP
- EXISTING CONCRETE FLUME



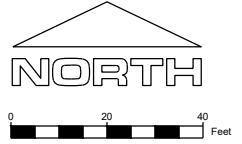
WASHOUT FACILITY NOTES:

- Maintaining temporary concrete washout facilities shall include removing and disposing of hardened concrete and/or slurry and returning the facilities to a functional condition.
- Facility shall be cleaned or reconstructed in a new area once washout becomes two-thirds full.
- The contractor may propose alternate configurations/materials or a pre-fabricated washout container for approval by the resident engineer at no additional cost to the project.
- The construction, maintenance, materials, removals, labor and all other incidentals necessary to construct the washout facility will not be measured for payment but will be considered incidental to the contract.



- STORM WATER POLLUTION PREVENTION GENERAL NOTES**
- THE LIMITS OF GRADING, TOPSOILING, SEEDING AND MULCHING SHALL BE AS SHOWN ON THE PLAN OR AS DIRECTED BY THE RESIDENT ENGINEER.
 - THE CONTRACTOR SHALL NOT PERMIT HAULING OUTSIDE OF THE GRADING LIMITS WITHOUT THE PRIOR APPROVAL OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
 - THE CONTRACTOR SHALL TAKE PROVISIONS TO PREVENT EROSION AND STORM WATER POLLUTION WITHIN THE PROJECT LIMITS AND AT THE STAGING AREAS, SUCH AS STABILIZED CONSTRUCTION ENTRANCES, SILT FENCE, INLET PROTECTIONS, ETC. COSTS FOR INSTALLATION, MAINTENANCE AND REMOVAL OF EROSION CONTROL DEVICES WITHIN THE PROJECT LIMITS AND AT THE STAGING AREAS SHALL BE BORNE BY THE CONTRACTOR, UNLESS OTHERWISE NOTED ON THE CONSTRUCTION PLANS.
 - AT THE COMPLETION OF CONSTRUCTION, THE STAGING AREAS SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITION. RUTTED AREAS SHALL BE FILLED AND THE AREA SEEDED AND MULCHED AS NEEDED. RESTORATION COSTS SHALL BE BORNE BY THE CONTRACTOR.

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JANUARY 8, 2021

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OWNER

VILLAGE OF LANSING



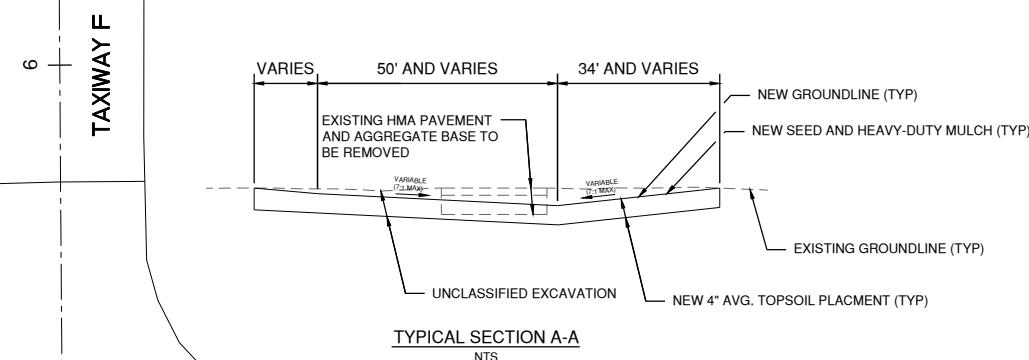
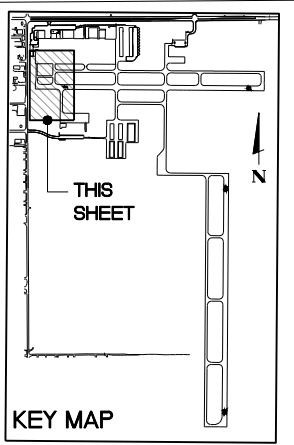
LANSING MUNICIPAL AIRPORT
LANSING, ILLINOIS

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| AIP PROJ. NO. 3-17-SBGP-156 | | |
| IL PROJ. NO. IGQ-4658 | | |
| CMT PROJECT NO: 190297-02-00 | | |
| CAD DWG FILE: CP201.DWG | | |
| DESIGNED BY: ARM | | |
| DRAWN BY: JRO | | |
| CHECKED BY: DKP | | |
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| COPYRIGHT: | | |

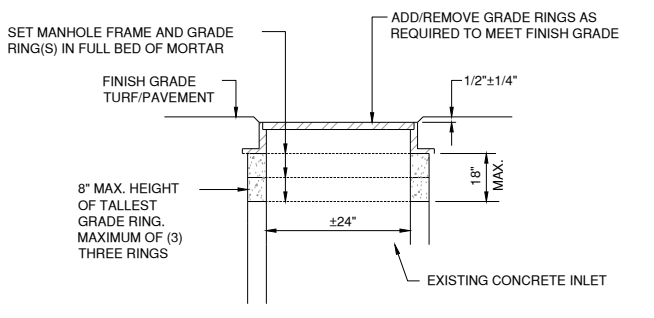
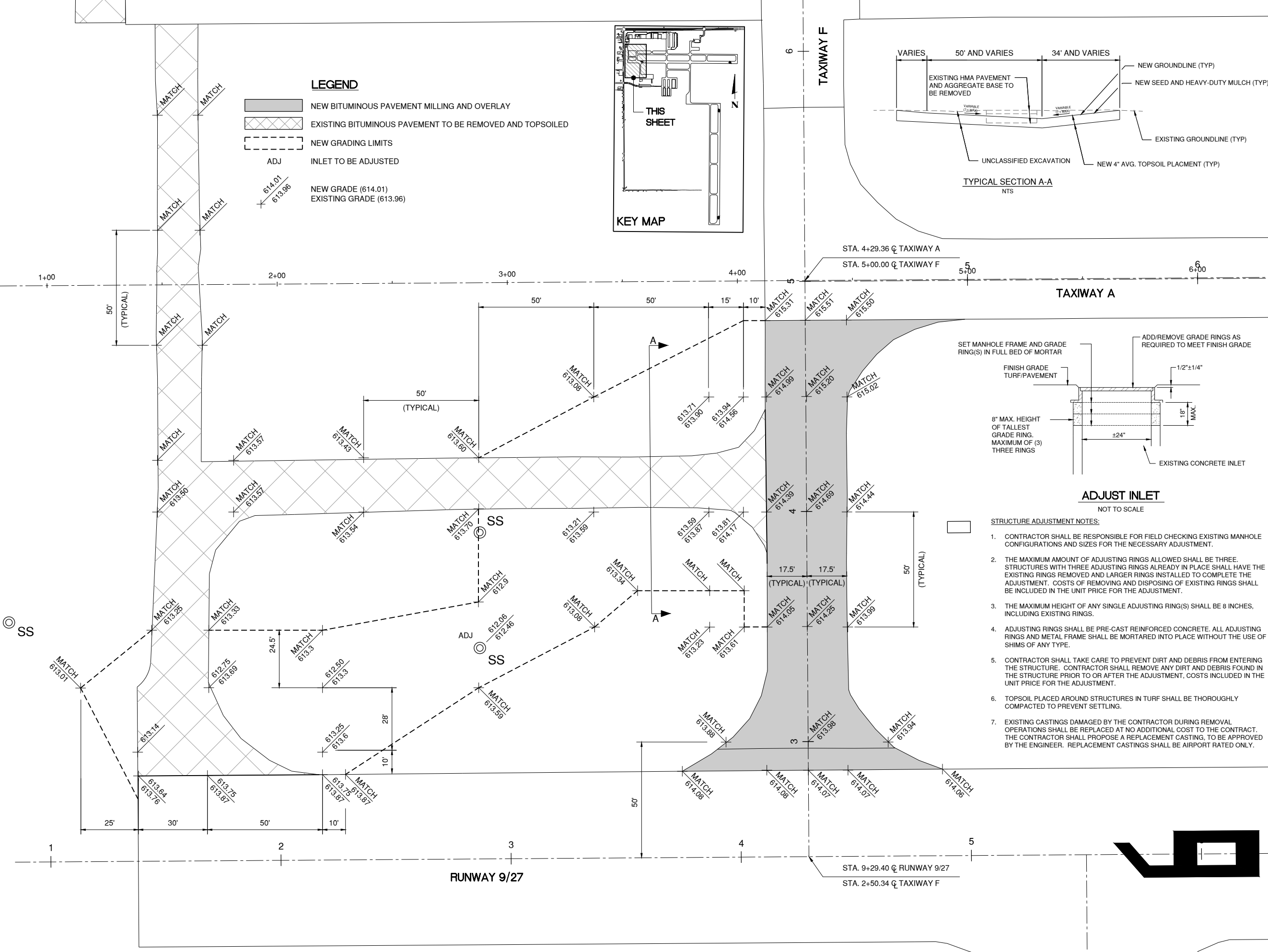
GRADING PLAN - 1

LEGEND

- NEW BITUMINOUS PAVEMENT MILLING AND OVERLAY
- EXISTING BITUMINOUS PAVEMENT TO BE REMOVED AND TOPSOILED
- NEW GRADING LIMITS
- INLET TO BE ADJUSTED
- ADJ
614.01
613.96
NEW GRADE (614.01)
EXISTING GRADE (613.96)



STA. 4+29.36 @ TAXIWAY A
STA. 5+00.00 @ TAXIWAY F



ADJUST INLET
NOT TO SCALE

STRUCTURE ADJUSTMENT NOTES:

1. CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD CHECKING EXISTING MANHOLE CONFIGURATIONS AND SIZES FOR THE NECESSARY ADJUSTMENT.
2. THE MAXIMUM AMOUNT OF ADJUSTING RINGS ALLOWED SHALL BE THREE. STRUCTURES WITH THREE ADJUSTING RINGS ALREADY IN PLACE SHALL HAVE THE EXISTING RINGS REMOVED AND LARGER RINGS INSTALLED TO COMPLETE THE ADJUSTMENT. COSTS OF REMOVING AND DISPOSING OF EXISTING RINGS SHALL BE INCLUDED IN THE UNIT PRICE FOR THE ADJUSTMENT.
3. THE MAXIMUM HEIGHT OF ANY SINGLE ADJUSTING RING(S) SHALL BE 8 INCHES, INCLUDING EXISTING RINGS.
4. ADJUSTING RINGS SHALL BE PRE-CAST REINFORCED CONCRETE. ALL ADJUSTING RINGS AND METAL FRAME SHALL BE MORTARED INTO PLACE WITHOUT THE USE OF SHIMS OF ANY TYPE.
5. CONTRACTOR SHALL TAKE CARE TO PREVENT DIRT AND DEBRIS FROM ENTERING THE STRUCTURE. CONTRACTOR SHALL REMOVE ANY DIRT AND DEBRIS FOUND IN THE STRUCTURE PRIOR TO OR AFTER THE ADJUSTMENT, COSTS INCLUDED IN THE UNIT PRICE FOR THE ADJUSTMENT.
6. TOPSOIL PLACED AROUND STRUCTURES IN TURF SHALL BE THOROUGHLY COMPACTED TO PREVENT SETTLING.
7. EXISTING CASTINGS DAMAGED BY THE CONTRACTOR DURING REMOVAL OPERATIONS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE CONTRACT. THE CONTRACTOR SHALL PROPOSE A REPLACEMENT CASTING, TO BE APPROVED BY THE ENGINEER. REPLACEMENT CASTINGS SHALL BE AIRPORT RATED ONLY.

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OWNER

VILLAGE OF LANSING



Lansing Municipal Airport

LANSING MUNICIPAL AIRPORT
LANSING, ILLINOIS

| MARK | DATE | DESCRIPTION |
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AIP PROJ. NO. 3-17-SBGP-156

IL PROJ. NO. IGQ-4658

CMT PROJECT NO: 190297-02-00

CAD DWG FILE: CP202.DWG

DESIGNED BY: ARM

DRAWN BY: JRO

CHECKED BY: DKP

APPROVED BY: ARM

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SHEET TITLE

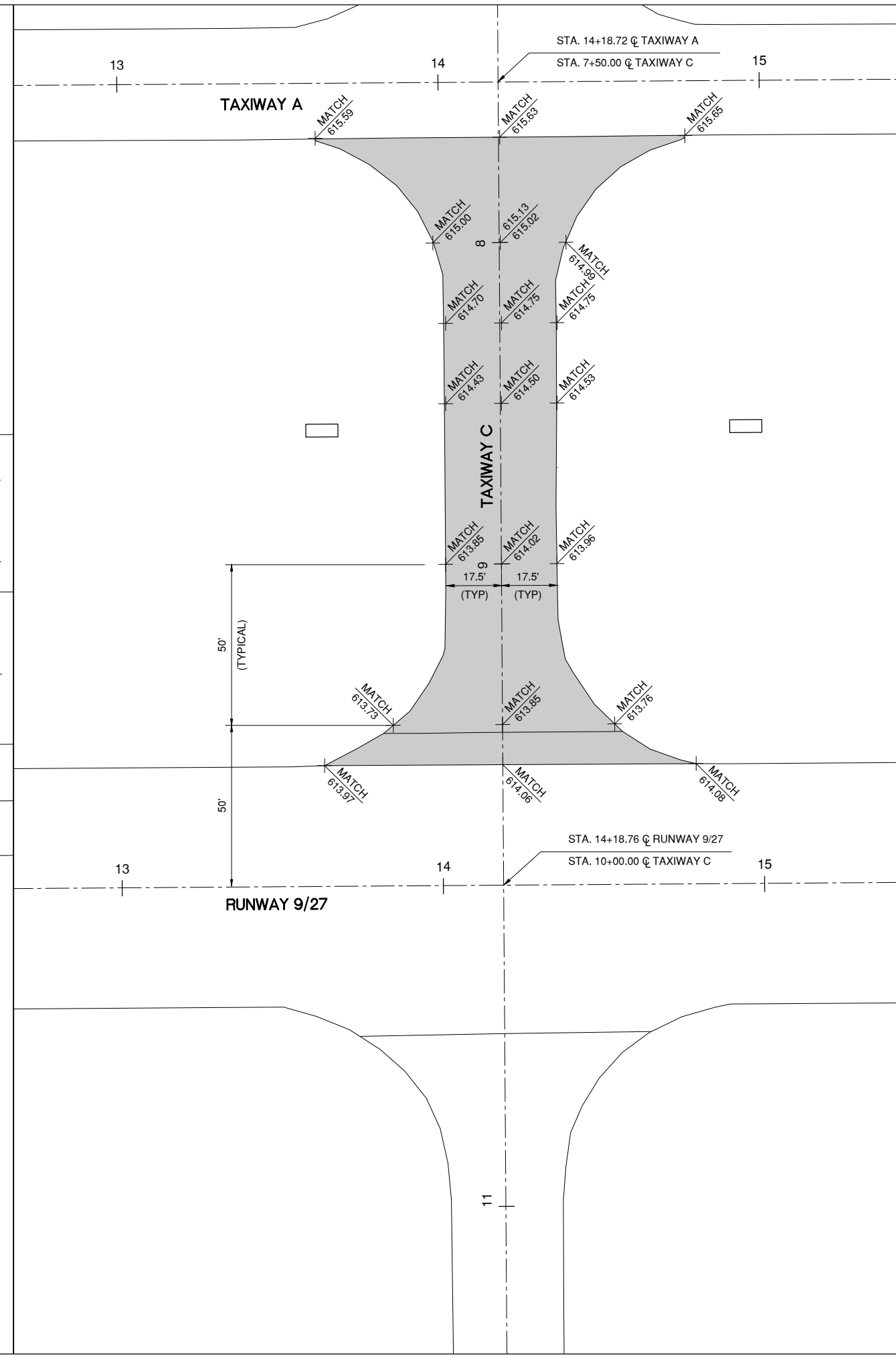
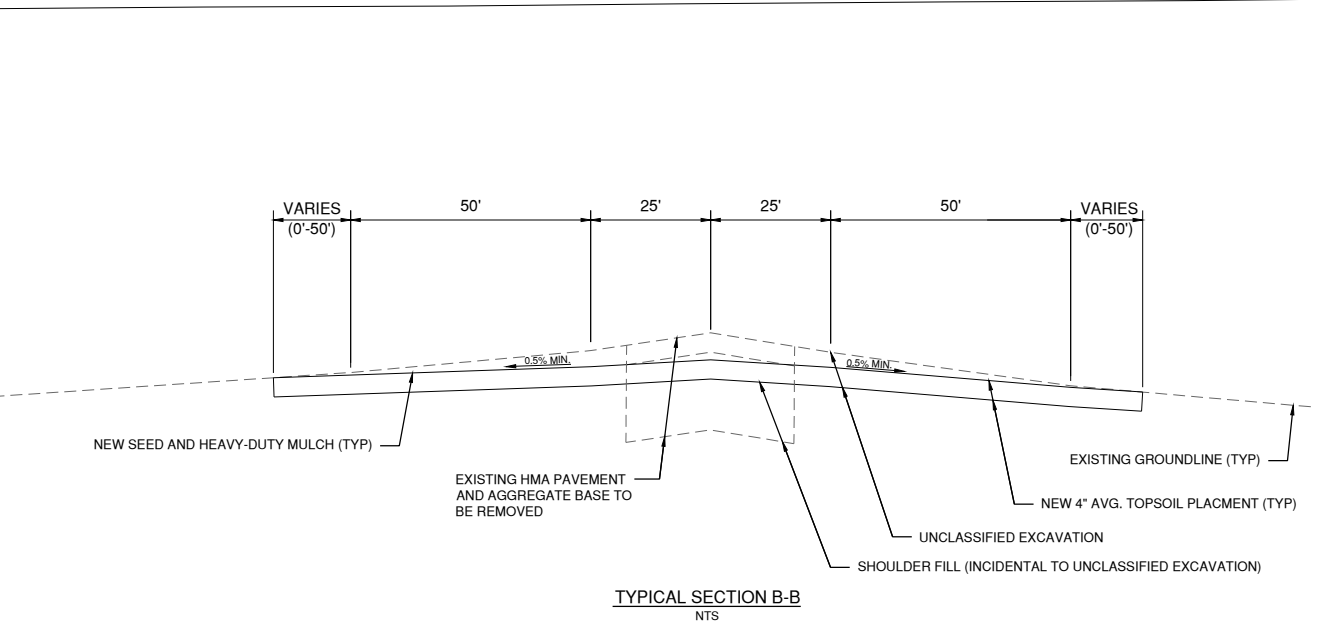
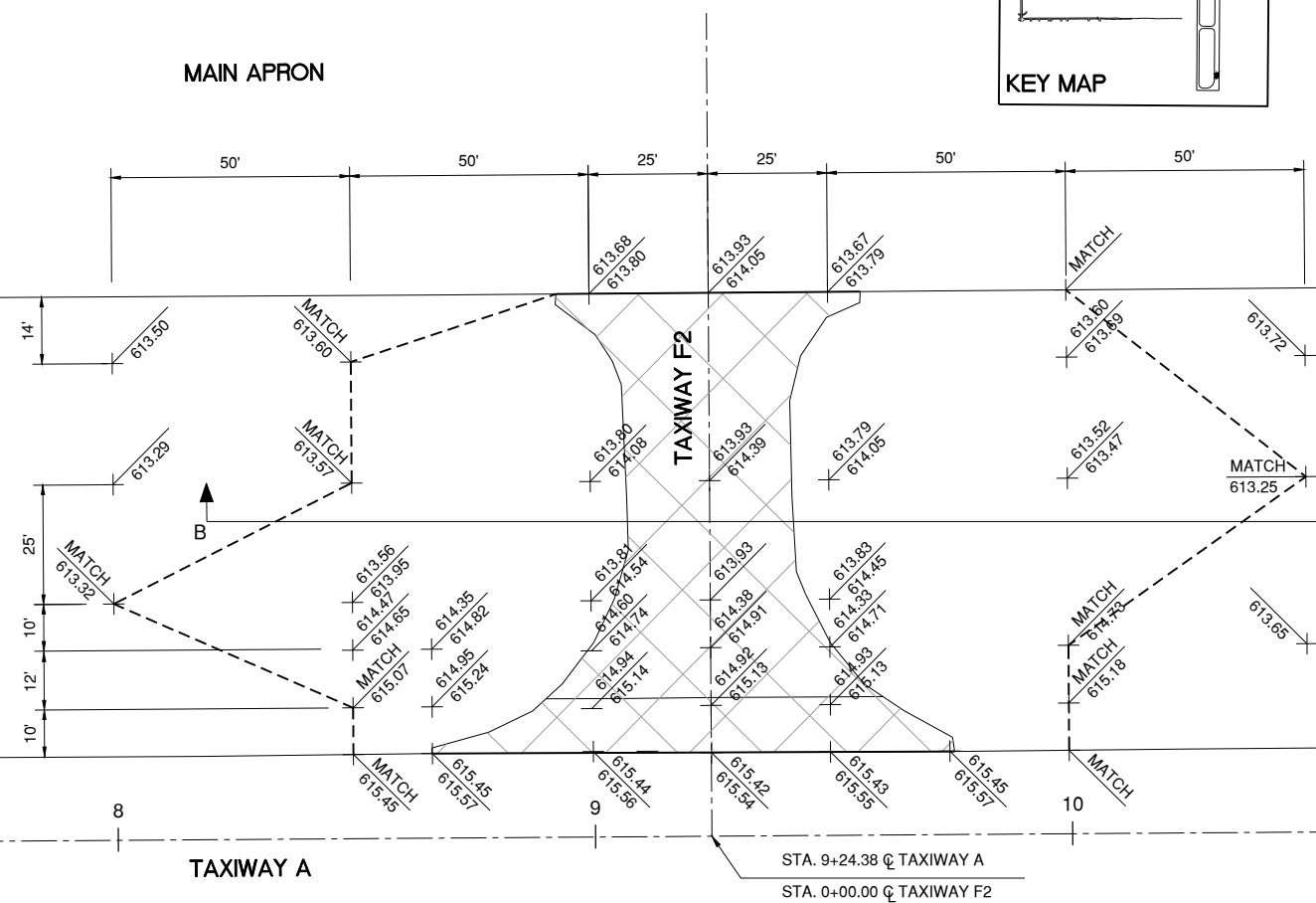
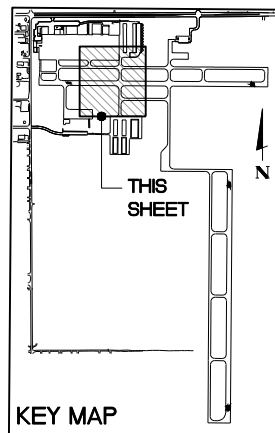
GRADING PLAN - 2

CP202
SHEET 11 OF 12

LEGEND

- NEW BITUMINOUS PAVEMENT MILLING AND OVERLAY
- EXISTING BITUMINOUS PAVEMENT TO BE REMOVED AND TOPSOILED
- NEW GRADING LIMITS

NEW GRADE (614.01)
EXISTING GRADE (613.96)



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OWNER

VILLAGE OF LANSING



Lansing Municipal Airport

LANSING MUNICIPAL AIRPORT
LANSING, ILLINOIS

MARK DATE DESCRIPTION

| |
|------------------------------|
| AIP PROJ. NO. 3-17-SBGP-156 |
| IL PROJ. NO. IGQ-4658 |
| CMT PROJECT NO: 190297-02-00 |
| CAD DWG FILE: EL101.DWG |
| DESIGNED BY: ARM |
| DRAWN BY: JRO |
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SHEET TITLE
PAVEMENT MARKING AND LIGHTING PLAN

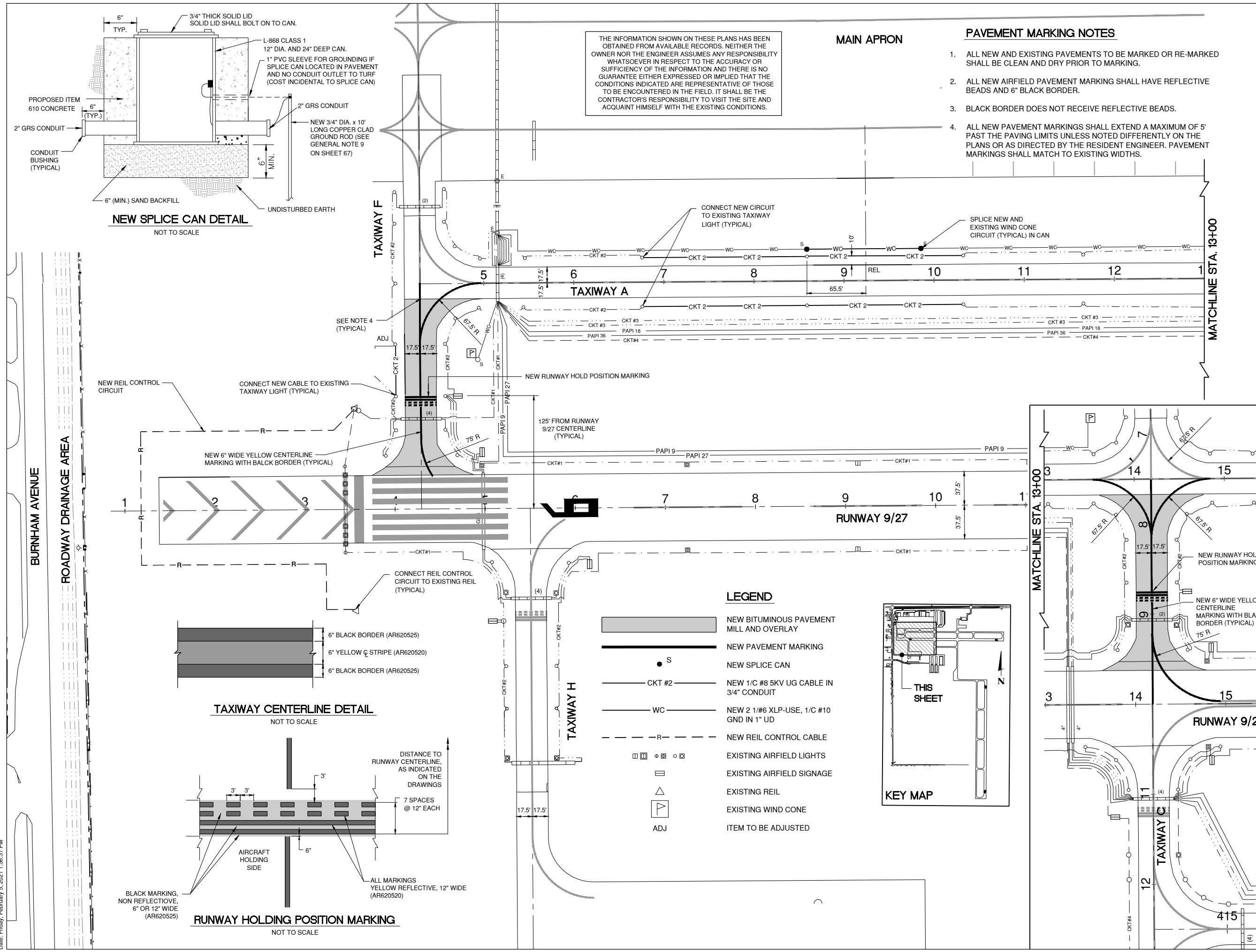
EL101
SHEET 12 OF 12

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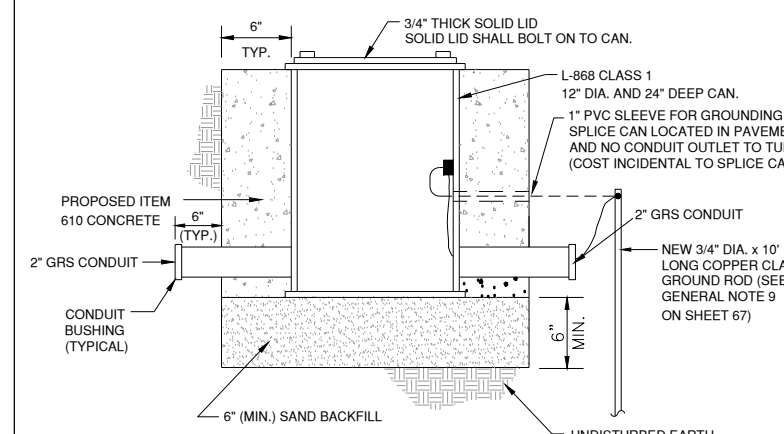
MAIN APRON

PAVEMENT MARKING NOTES

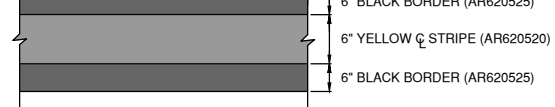
- ALL NEW AND EXISTING PAVEMENTS TO BE MARKED OR RE-MARKED SHALL BE CLEAN AND DRY PRIOR TO MARKING.
- ALL NEW AIRFIELD PAVEMENT MARKING SHALL HAVE REFLECTIVE BEADS AND 6" BLACK BORDER.
- BLACK BORDER DOES NOT RECEIVE REFLECTIVE BEADS.
- ALL NEW PAVEMENT MARKINGS SHALL EXTEND A MAXIMUM OF 5' PAST THE PAVING LIMITS UNLESS NOTED DIFFERENTLY ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. PAVEMENT MARKINGS SHALL MATCH TO EXISTING WIDTHS.



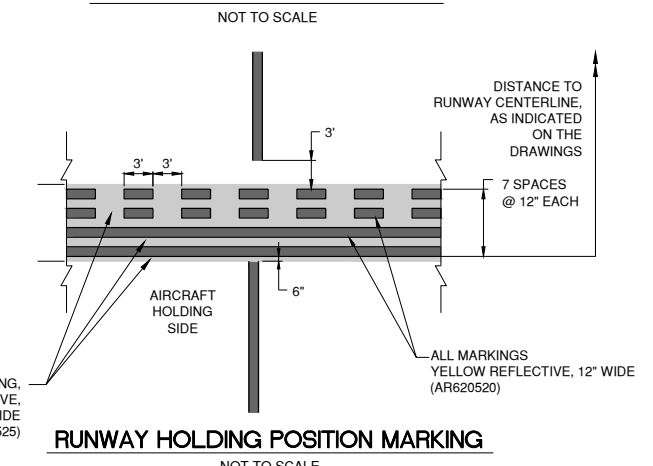
NEW SPLICE CAN DETAIL
NOT TO SCALE



TAXIWAY CENTERLINE DETAIL
NOT TO SCALE

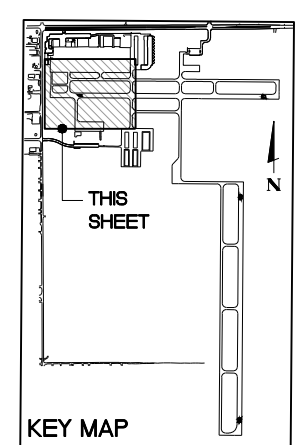


RUNWAY HOLDING POSITION MARKING
NOT TO SCALE



LEGEND

- NEW BITUMINOUS PAVEMENT MILL AND OVERLAY
- NEW PAVEMENT MARKING
- NEW SPLICE CAN
- NEW 1/C #8 5KV UG CABLE IN 3/4" CONDUIT
- NEW 2 1/#6 XLP-USE, 1/C #10 GND IN 1" UD
- NEW REIL CONTROL CABLE
- EXISTING AIRFIELD LIGHTS
- EXISTING AIRFIELD SIGNAGE
- EXISTING REIL
- EXISTING WIND CONE
- ITEM TO BE ADJUSTED



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