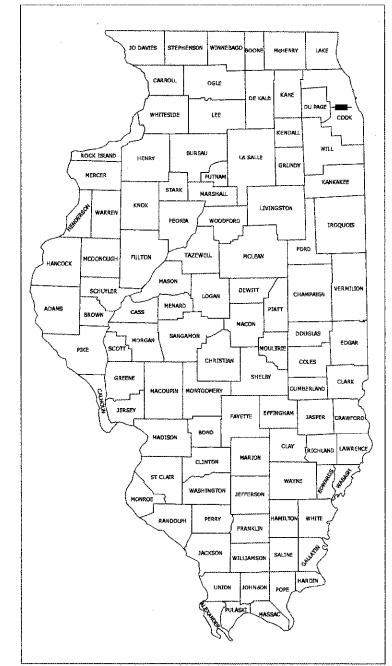


ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1419	08-00127-00-RS	COOK	15	1
ILLINOIS PROJECT		M-9003(050)		
CONTRACT NO. 63102				

INDEX OF SHEETS ON SHEET NO. 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
LOCAL AGENCY PAVEMENT PRESERVATION (LAPP)
FAU ROUTE 1419 (MADISON STREET)
21ST AVENUE TO 2ND AVENUE
SECTION NO. 08-00127-00-RS
PROJECT M-9003(050)
VILLAGE OF MAYWOOD
COOK COUNTY
JOB NO. C-91-473-08



LOCATION OF SECTION INDICATED THUS:

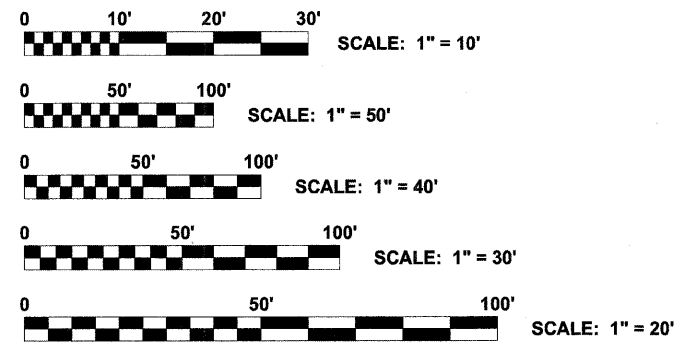
TRAFFIC DATA

2030 ADT = 12,000
 POSTED SPEED LIMIT: 30 MPH
 DESIGN SPEED 30 MPH

DESIGN DESIGNATION

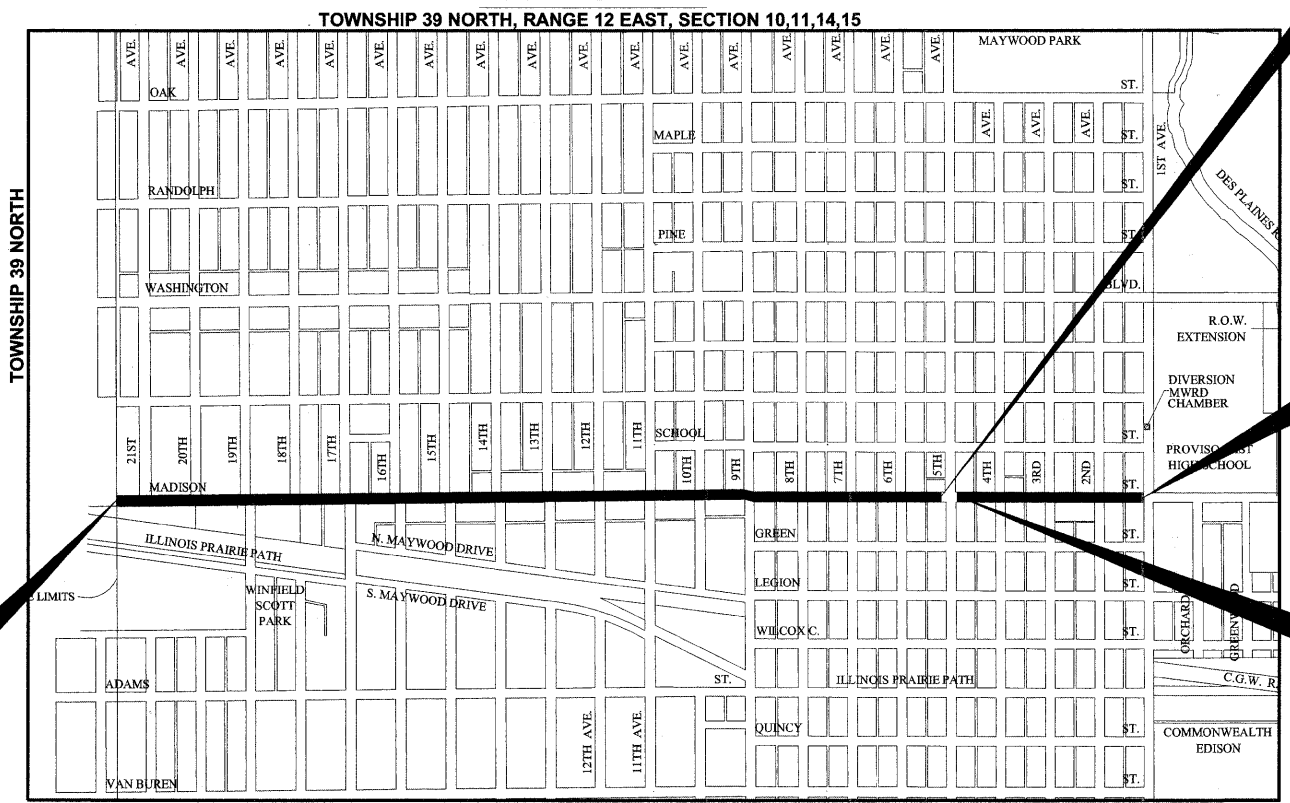
COLLECTOR

PROJECT LOCATED IN THE VILLAGE OF MAYWOOD



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

LOCATION MAP



PROJECT OMISSION BEGINS STATION 58+17 MADISON STREET

PROJECT ENDS STATION 69+37 MADISON STREET

PROJECT OMISSION ENDS STATION 59+80 MADISON STREET

PROJECT BEGINS STATION 3+50 MADISON STREET

- AREA OF IMPROVEMENT

NOT TO SCALE

GROSS LENGTH OF IMPROVEMENT = 6,587 FT. = 1.248 MI.

NET LENGTH OF IMPROVEMENT = 6,424 FT. = 1.217 MI.

STATE OF ILLINOIS
 ILLINOIS DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

APPROVED November 18 2008
Mark D. Lucas
 VILLAGE OF MAYWOOD, PRESIDENT

PASSED JANUARY 7 2009
Chris M. Helt
 DISTRICT ENGINEER OF LOCAL ROADS & STREETS

RELEASED FOR BID BASED ON LIMITED REVIEW JANUARY 8 2009
Diana M. O'Keefe
 DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

(PRINTED BY AUTHORITY OF THE STATE OF ILLINOIS)

Mark D. Lucas

DATE SIGNED: 11-13-08
 LICENSE EXPIRES: 11-30-09

EDWIN HANCOCK ENGINEERING COMPANY
 9933 ROOSEVELT ROAD PHONE: (708) 865-0300
 WESTCHESTER, ILLINOIS 60154



Know what's below.
Call before you dig.

CONTRACT NO. 63102

FIELD ENGINEER: MARLIN SOLOMON, (847)705-4407
 Nov 10, 2008 - 3:17pm
 Drawing file: W:\Projects\36508206 - Madison Ave LAPP\COVER_Madison.dwg

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1419	08-00127-00-RS	COOK	15	2
ILLINOIS PROJECT M-9003 (050)				
CONTRACT NO. 63102				

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET, LOCATION MAP
2	INDEX OF SHEETS, LEGEND OF SYMBOLS, & I.D.O.T. STANDARD DRAWINGS
3	GENERAL NOTES
4	SUMMARY OF QUANTITIES
5	EXISTING & PROPOSED TYPICAL CROSS SECTIONS
6	DETAILS & NOTES
7-10	PAVING PLANS
11	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD08)
12	BUTT JOINT AND HMA TAPER DETAILS (BD32)
13	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC10)
14	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC13)
15	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS07)

HIGHWAY STANDARDS

STANDARD NO.	TITLE OR DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C&D PATCHES
701501-05	URBAN LANE CLOSURE, 2-LANE, 2-WAY, UNDIVIDED
701606-06	URBAN LANE CLOSURE, MULTILANE, 2-WAY, WITH MOUNTABLE MEDIAN
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	LANE CLOSURE, MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES

LEGEND OF SYMBOLS

(TO BE USED IN CONJUNCTION WITH I.D.O.T. STANDARD 000001-05)

SYMBOL	DESCRIPTION
	EXISTING HOT-MIX ASPHALT CONCRETE AREA
	EXISTING CONCRETE AREA
	PROPOSED HOT-MIX ASPHALT BUTT JOINT
	HOT-MIX ASPHALT SURFACE REMOVAL
	PROPOSED HOT-MIX ASPHALT PAVING AREA
A	STRUCTURE TO BE ADJUSTED
F	FILLING VALVE VAULTS
1C	CLOSED LID TYPE 1
RC-1C	STRUCTURE TO BE RECONSTRUCTED WITH A NEW TYPE 1 FRAME & LID (C = CLOSED, P = OPEN LID)
	EXISTING HANDHOLE
	EXISTING HEAVY DUTY HANDHOLE
	TRAFFIC SIGNAL CONTROLLER
	DOUBLE HANDHOLE
	EXISTING WATER VALVE BOX
	EXISTING CURB AND GUTTER

MAYWOOD
MADISON STREET IMPROVEMENT (LAPP)
INDEX OF SHEETS, I.D.O.T. STANDARDS
DRAWINGS AND LEGEND OF SYMBOLS

REVISION	DATE	BY

HANCOCK ENGINEERING
Civil Engineers
Municipal Consultants
Established 1911

9933 Roosevelt Road
Westchester, Illinois 60154-2780
Phone: 708/865-0300
Fax: 708/865-1212

SCALE: NONE

DRAWN BY: MK
CHECKED BY: JGG
DATE: 11/10/2008

E.H.E. PROJECT NO.: 565-08-20601

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1419	08-00127-00-RS	COOK	15	3
ILLINOIS PROJECT M-9003 (050)				
CONTRACT NO. 63102				

GENERAL NOTES

STANDARDS

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION AS SHOWN ON THE INDEX OF SHEETS IN THE PLANS. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JANUARY 1, 2007, THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2009, THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS," "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY 1996 FIFTH EDITION, AND THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

UNDERGROUND UTILITIES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 811 FOR FIELD LOCATIONS OF BURIED ELECTRICAL, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).

THE LOCATIONS OF THE UNDERGROUND UTILITIES IF SHOWN ON THE PLANS HAVE BEEN OBTAINED BY FIELD SURVEYS AND SEARCHES OF AVAILABLE RECORDS. IT IS BELIEVED THAT DATA IS ESSENTIALLY CORRECT, BUT THE VILLAGE OF MAYWOOD, THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND/OR OTHER OFFICES AND AGENCIES ASSOCIATED WITH THE DEVELOPMENT OF THESE PLANS DO NOT GUARANTEE THEIR ACCURACY OR COMPLETENESS. THE CONTRACTOR WILL BE REQUIRED TO VERIFY THE EXACT LOCATION OF EACH FACILITY WITH THE UTILITY COMPANY, AND SHALL TAKE DUE CARE IN ALL PHASES OF THE CONSTRUCTION TO PROTECT ANY SUCH FACILITIES WHICH MAY BE AFFECTED BY THE WORK. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF MAYWOOD.

FRAMES AND GRATES

THE TYPE OF FRAMES AND GRATES REQUIRED FOR ALL MANHOLES AND VALVE VAULTS LISTED IN THE SUMMARY OF QUANTITIES MAY BE FOUND ON THE PLANS AT THEIR RESPECTIVE LOCATIONS. WHERE LIDS ARE CALLED FOR ON THE PLANS, THEY SHALL BE IN ACCORDANCE WITH ARTICLE 604.04 OF THE STANDARD SPECIFICATIONS AND THE TERM LID IS USED IN LIEU OF GRATE. ALL LIDS ON SANITARY MANHOLES, COMBINED SEWER MANHOLES, AND VALVE VAULTS SHALL BE OF THE SELF SEALING TYPE.

ON ALL IMPROVEMENTS, THE FRAMES AND LIDS OF EXISTING CATCH BASINS, INLETS, MANHOLES, AND VALVE VAULTS WHICH ARE TO BE ABANDONED DUE TO CONSTRUCTION OF THIS IMPROVEMENT ARE TO REMAIN THE PROPERTY OF THE VILLAGE OF MAYWOOD AND BE SALVAGED. THESE ITEMS SHALL BE DELIVERED TO THE VILLAGE OF MAYWOOD.

MANHOLE OR VALVE COVERS

THE WORD "WATER", "SANITARY", OR "STORM" SHALL BE CAST INTO THE LID OF EACH RESPECTIVE MANHOLE OR VALVE VAULT.

MAINTENANCE OF SEWER FLOWS

THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS AS TO MAINTAIN AT ALL TIMES FLOW THROUGH EXISTING STORM AND SANITARY SEWER SYSTEMS. HE SHALL ALSO PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT IF NECESSARY AND A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER COLLECTED IN A SAFE MANNER WITHOUT DAMAGE OF ANY KIND TO ADJACENT PROPERTIES. THE ENDS OF EXISTING DRAINAGE LINES WHICH ARE NOT TO BE INCORPORATED INTO THE PROJECT ARE TO BE SEALED AS SPECIFIED IN THE SPECIAL PROVISIONS. EXISTING STRUCTURES ARE TO BE INSPECTED BEFORE CONSTRUCTION STARTS - ANY ACCUMULATION OF MATERIAL IN THE STRUCTURE DUE TO CONSTRUCTION OPERATIONS SHALL BE REMOVED BY THE CONTRACTOR AT HIS EXPENSE.

MAINTENANCE OF EXISTING DRAINAGE STRUCTURES

WHEN DURING THE CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF ANY GUTTERS AND DRAINAGE STRUCTURE SO THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE FACILITIES SHALL BE CLEAN AND FREE OF ALL OBSTRUCTIONS DUE TO CONSTRUCTION OPERATIONS. THE COST OF THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.

SAW CUTTING

THE CONTRACTOR SHALL SAW CUT ASPHALT PAVEMENT AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING PAVEMENT TO BE REMOVED BY APPROVED MEANS OR AN APPROVED CONCRETE SAW TO A DEPTH AS DIRECTED BY THE ENGINEER. SUITABLE GUIDELINES OR DEVICES SHALL BE USED TO ASSURE CUTTING A NEAT, STRAIGHT LINE AS SHOWN ON THE PLANS. CARE SHALL BE TAKEN BY THE CONTRACTOR AS NOT TO DAMAGE THE REMAINING PAVEMENT DIRECTLY ADJACENT TO THE PAVEMENT TO BE REMOVED. ANY DAMAGE TO THE EXISTING PAVEMENT RESULTING FROM PAVEMENT REMOVAL OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THE COST OF SAW CUTTING DESCRIBED ABOVE SHALL BE INCLUDED IN THE ITEM BEING REMOVED. SAW CUTS FOR PAVEMENT PATCHING WILL BE PAID FOR IN THE CONTRACT.

FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

THIS ITEM ONLY PERTAINS TO STRUCTURES LOCATED IN THE CONCRETE OR HOT-MIX ASPHALT ROADWAY PAVEMENT AREAS THAT WILL REQUIRE CONCRETE OR HOT-MIX SURFACE REMOVAL. THE ENGINEER WILL MARK IN THE FIELD ALL STRUCTURES TO BE DONE UNDER THIS ITEM. SEE "DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING."

PRIME COAT

PRIME COAT MUST BE INSTALLED NO EARLIER THAN TWENTY-FOUR (24) HOURS PRIOR TO PLACEMENT OF HOT-MIX ASPHALT.

FIELD OFFICE

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR VILLAGE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT OR THE VILLAGE.

BARRICADES

THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED, ONE (1) WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL.

BUTT JOINTS

BUTT JOINT WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

MILLED PAVEMENT OPEN TO TRAFFIC

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

PAVING OPERATIONS

CONTRACTOR MUST PAVE MADISON STREET IN A MAXIMUM OF 2 PASSES. IF THE CONTRACTOR IS NOT ABLE TO COMPLETE ALL PAVING ONE (1) DAY, THE CENTERLINE JOINT SHALL BE SEALED.

PAVEMENT PATCHING

EXACT LOCATIONS OF CLASS D PATCHES WILL BE DETERMINED IN FIELD BY ENGINEER.

ADA RAMPS

LOCAL AGENCY RESPONSIBLE FOR MAKING SIDEWALK CORNERS ADA COMPLIANT BEFORE PROJECT START. RESIDENT ENGINEER TO VERIFY.

MAYWOOD MADISON STREET IMPROVEMENT (LAPP) GENERAL NOTES

REVISION:	



◆ Civil Engineers
◆ Municipal Consultants
◆ Established 1911

9933 Roosevelt Road
Westchester, Illinois 60154-2780
Phone: 708/865-0300
Fax: 708/865-1212

SCALE: NONE

DRAWN BY: MK
CHECKED BY: JGG
DATE: 11/10/2008

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1419	08-00127-00-RS	COOK	15	4

ILLINOIS PROJECT M-9003 (050)

CONTRACT NO. 63102

SUMMARY OF QUANTITIES

CODE	PAY ITEM	UNIT	QUANTITY 1000 80% FEDERAL 20% VILLAGE
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GAL	3,600
40600300	AGGREGATE (PRIME COAT)	TON	70
40600895	CONSTRUCTING TEST STRIP	EACH	2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQYD	500
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX D, N70	TON	3,350
44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQYD	34,000
44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQYD	680
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQYD	1,360
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQYD	680
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQYD	680
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	19
60266100	VALVE VAULTS TO BE RECONSTRUCTED	EACH	19
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	68
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	57
67100100	MOBILIZATION	L SUM	1
70101700	TRAFFIC CONTROL AND PROTECTION	L SUM	1
70106800	CHANGEABLE MESSAGE SIGN	CAL-MO	2
70300100	SHORT TERM PAVEMENT MARKING	FOOT	3,500
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	1,700
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQFT.	150
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	19,000
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3,200
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	125
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	800
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	700
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	80
X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1,600
XX005195	COMBINED SEWER MANHOLE, 4' DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	11
△ Z0076600	TRAINEES	hour	500

* DENOTES SPECIALTY ITEM

△ Y080

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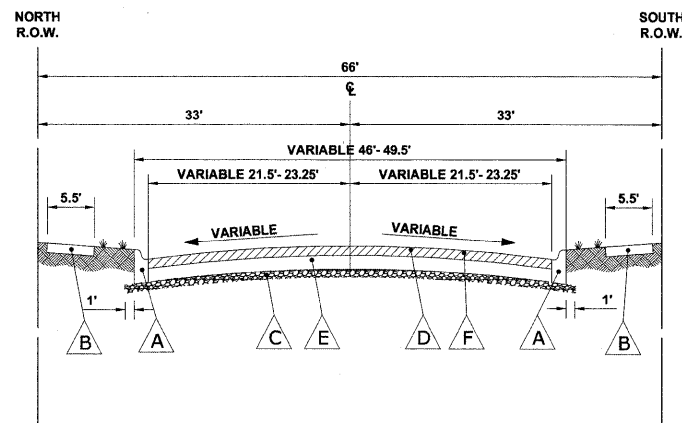
◆ Civil Engineers
◆ Municipal Consultants
◆ Established 1911

9933 Roosevelt Road
Westchester, Illinois 60154-2780
Phone: 708/965-0300
Fax: 708/965-1212

REVISION:	

**MAYWOOD
MADISON STREET IMPROVEMENT (LAPP)
SUMMARY OF QUANTITIES**

SCALE: NONE
DRAWN BY: MK
CHECKED BY: JGG
DATE: 11/10/2008



**EXISTING TYPICAL CROSS SECTION
MADISON STREET**

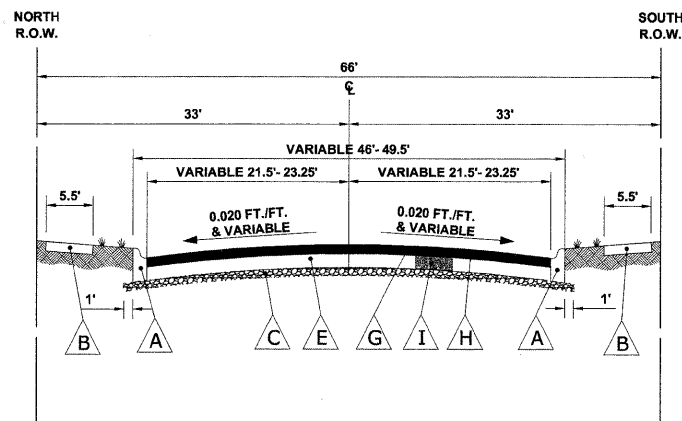
LEGEND OF SYMBOLS

SYMBOL	DESCRIPTION
A	EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
B	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
C	EXISTING SUB-BASE GRANULAR MATERIAL
D	EXISTING HOT-MIX ASPHALT SURFACE COURSES, VARIABLE
E	EXISTING BASE COURSE
F	PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH

HOT-MIX ASPHALT (HMA) MIXTURE REQUIREMENTS

ITEM	A C TYPE	VOIDS
HMA SURFACE COURSE, MIX D, N70, (IL - 9.5 mm)	PG 64-22	4% @ 70 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	SBS/SBR PG 76-28/22	4% @ 50 GYR.
CLASS D PATCHES (HMA BINDER IL-19mm)	PG 64-22 *	4% @ 70 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE COURSE IS 112 LBS/SQYD/IN
* WHEN RAP EXCEEDS 20%, THE NEW HMA BINDER IN THE MIX SHALL BE PG 58-22.



**PROPOSED TYPICAL CROSS SECTION
MADISON STREET**

LEGEND OF SYMBOLS

SYMBOL	DESCRIPTION
A	EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
B	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK, 5"
C	EXISTING SUB-BASE GRANULAR MATERIAL, 4"
E	EXISTING CONCRETE BASE COURSE, 8"
G	PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, MINIMUM 3/4"
H	PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX D, N70, 1-1/2"
I	PROPOSED CLASS D PATCH, 8"

REVISION:	

**MAYWOOD
MADISON STREET IMPROVEMENT (LAPP)
TYPICAL SECTIONS**

HANCOCK ENGINEERING ♦ Civil Engineers ♦ Municipal Consultants ♦ Established 1911
9933 Roosevelt Road
Westchester, Illinois 60154-2780
Phone: 708/865-0300
Fax: 708/865-1212

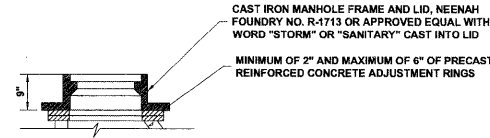
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DATE: 11/10/2008

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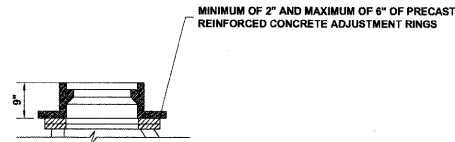
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1419	08-00127-00-RS	COOK	15	6
ILLINOIS PROJECT		M-9003 (050)		
CONTRACT NO.		63102		

GENERAL NOTES

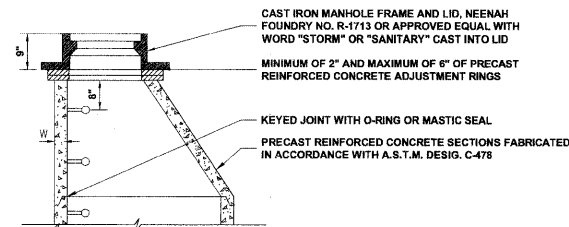
1. THE EDWIN HANCOCK ENGINEERING COMPANY SHALL BE NOTIFIED AT LEAST TWO (2) WORKING DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION AT (708) 865-0300.
2. THE VILLAGE OF MAYWOOD PUBLIC WORKS DEPARTMENT SHALL BE NOTIFIED AT LEAST TWO (2) WORKING DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION AT (708) 450-4482.
3. THE CONTRACTOR SHALL SAW CUT ALL BUTT JOINTS TO ADJOINING PAVEMENTS NOT MORE THAN 24 HOURS PRIOR TO PLACING SURFACE COURSE.
4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO TAKE PRECAUTIONS SO AS NOT TO DAMAGE PARKWAYS AND CURB OUTSIDE THE PRESCRIBED LIMITS OF RESTORATION. NO PARKWAY OR CURB RESTORATION IS INCLUDED FOR PAYMENT EXCEPT AS NECESSARY FOR STRUCTURE ADJUSTMENTS AND REPLACEMENT, OR AS DIRECTED BY ENGINEER.
5. STOCKPILING OF MATERIALS ON THE PARKWAYS OR CURB WILL NOT BE ALLOWED, INCLUDING EXCAVATED MATERIAL OR TRENCH BACKFILL MATERIAL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RESTORING ANY DAMAGE TO THE PARKWAY OR SOD OUTSIDE OF THE PRESCRIBED LIMITS TO THE SATISFACTION OF THE ENGINEER AND THE VILLAGE AT HIS OWN EXPENSE.



STRUCTURE ADJUSTMENT DETAIL WITH FRAME AND LID



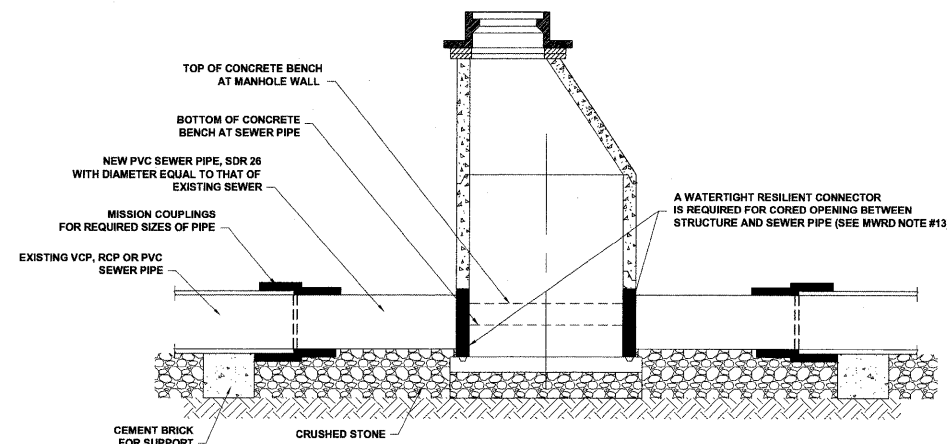
STRUCTURE ADJUSTMENT DETAIL WITHOUT FRAME AND LID



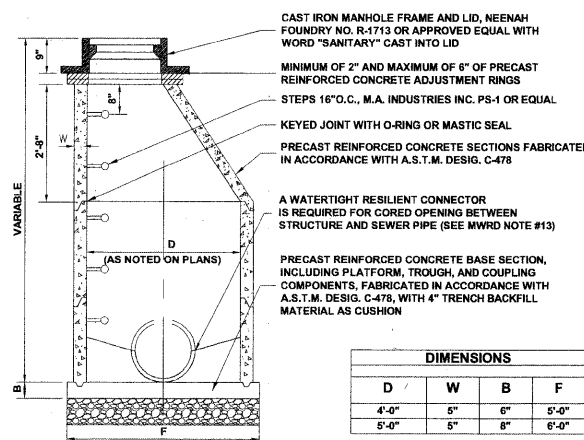
STRUCTURE RECONSTRUCTION DETAIL

M.W.R.D.G.C. GENERAL NOTES

1. THE MWRD SEWER SYSTEMS SECTION FIELD OFFICE MUST BE NOTIFIED AT LEAST TWO (2) WORKING DAYS PRIOR TO THE COMMENCEMENT OF WORK AT (708) 588-4055.
2. ELEVATION DATUM IS U.S.G.S.
3. ALL FLOOR DRAINS SHALL DISCHARGE TO THE SANITARY SEWER SYSTEM.
4. ALL DOWNSPOUTS AND FOOTING DRAINS SHALL DISCHARGE INTO THE STORM SEWER SYSTEM.
5. ALL PVC SEWER PIPE SHALL BE SDR 26. ALL PVC SEWER PIPE JOINTS SHALL CONFORM TO ASTM D-3139 FOR PVC PIPE 12" IN DIAMETER OR LESS. ALL PVC SEWER PIPE JOINTS SHALL CONFORM TO ASTM D-3212 FOR PVC PIPE 15" IN DIAMETER OR MORE. ALL PVC SEWER PIPE 12" IN DIAMETER OR LESS SHALL CONFORM TO ASTM D-2241 (WATER QUALITY PIPE). ALL PVC SEWER PIPE 15" IN DIAMETER OR MORE SHALL CONFORM TO ASTM D-3034.
6. ALL D.I.P. STORM, COMBINED AND SANITARY SEWER PIPE JOINTS SHALL CONFORM TO ANSI A-21.11. ALL D.I.P. SEWER PIPE SHALL CONFORM TO ASTM A-21.51. ALL D.I.P. SEWER PIPE SHALL BE CLASS 52.
7. ALL SANITARY, COMBINED, AND STORM SEWER CONSTRUCTION IN COMBINED SEWER AREAS, REQUIRES STONE BEDDING 1/4" TO 1" IN SIZE, WITH A MINIMUM THICKNESS EQUAL TO 1/4 THE OUTSIDE DIAMETER OF THE SEWER PIPE, BUT NOT LESS THAN FOUR INCHES (4") NOR MORE THAN EIGHT INCHES (8"). MATERIAL SHALL BE CA-11 OR CA-13 AND SHALL BE EXTENDED AT LEAST 12" ABOVE THE TOP OF THE PIPE.
8. "BAND SEAL" OR SIMILAR FLEXIBLE-TYPE COUPLINGS SHALL BE USED IN THE CONNECTION OF SEWER PIPE OF DISSIMILAR MATERIALS.
9. WHEN CONNECTING TO AN EXISTING SEWER MAIN BY MEANS OTHER THAN AN EXISTING WYE, TEE, OR AN EXISTING MANHOLE, ONE OF THE FOLLOWING METHODS SHALL BE USED:
 - a) CIRCULAR SAW-CUT OF SEWER MAIN BY MECHANICAL CORING MACHINE, AND PROPER INSTALLATION OF HUB-WYE SADDLE OR HUB-TEE SADDLE, IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
 - b) REMOVE AN ENTIRE SECTION OF PIPE (BREAKING ONLY THE TOP OF ONE BELL) AND REPLACE WITH A WYE OR TEE BRANCH SECTION. AFTER THE WYE OR TEE BRANCH IS INSERTED, CONCRETE SHALL BE PLACED OVER THE BROKEN AREA TO A MINIMUM THICKNESS OF 4" AND TO A DIMENSION OF 8" IN ALL DIRECTIONS.
 - c) USING PIPE CUTTER, NEATLY AND ACCURATELY CUT OUT DESIRED LENGTH OF PIPE FOR INSERTION OF PROPER FITTING. USE "BAND SEAL" OR SIMILAR COUPLINGS TO HOLD FIRMLY IN PLACE. FOLLOW MANUFACTURER'S RECOMMENDATIONS FOR THE INSTALLATION.
10. WHEREVER A SEWER CROSSES UNDER A WATER MAIN, THE MINIMUM VERTICAL DISTANCE FROM THE TOP OF THE SEWER TO THE BOTTOM OF THE WATER MAIN SHALL BE 18 INCHES. FURTHERMORE, A MINIMUM HORIZONTAL DISTANCE OF 10 FEET BETWEEN SANITARY/COMBINED SEWERS AND WATER MAINS SHALL BE MAINTAINED UNLESS: THE SEWER IS LAID IN A SEPARATE TRENCH, KEEPING A MINIMUM 18" VERTICAL SEPARATION; OR THE SEWER IS LAID IN THE SAME TRENCH WITH THE WATER MAIN LOCATED AT THE OPPOSITE SIDE ON A BENCH OF UNDISTURBED EARTH, KEEPING A MINIMUM 18" VERTICAL SEPARATION. IF EITHER THE VERTICAL OR HORIZONTAL DISTANCES DESCRIBED ABOVE CANNOT BE MAINTAINED, OR THE SEWER CROSSES ABOVE THE WATER MAIN, THE SEWER SHALL BE CONSTRUCTED TO WATER MAIN STANDARDS.
11. ALL SANITARY MANHOLES, (AND STORM MANHOLES IN COMBINED SEWER AREAS), SHALL HAVE A MINIMUM INSIDE DIAMETER OF 48 INCHES, AND SHALL BE PRE-CAST REINFORCED CONCRETE.
12. ALL ABANDONED SEWERS SHALL BE PLUGGED AT BOTH ENDS WITH A MINIMUM OF TWO (2) FEET LONG, NON-SHRINK CONCRETE/MORTAR PLUG.
13. ALL INLET AND OUTLET PIPES OF SANITARY SEWER MANHOLES AND OTHER UNDERGROUND STRUCTURES (AND IN COMBINED SEWER AREAS, ALSO ALL COMBINED/STORM SEWER MANHOLES, CATCH BASINS, INLETS, AND UNDERGROUND DETENTION STORAGE STRUCTURES) SHALL BE JOINED WITH WATERTIGHT FLEXIBLE RUBBER CONNECTORS CONFORMING TO A.S.T.M. C-443 & C-923 WITH STAINLESS STEEL BANDS.
14. THE MAXIMUM ALLOWABLE INFILTRATION OR EXFILTRATION IS 100 GAL/DAY/MILE/INCH DIA OF THE SEWER PIPE.



PROPOSED COMBINED MANHOLE INSTALLED ON EXISTING SEWER MAIN



DIMENSIONS			
D	W	B	F
4'-0"	5'	6"	5'-0"
5'-0"	5'	8"	6'-0"

PROPOSED COMBINED MANHOLE DETAIL

MAYWOOD
MADISON STREET IMPROVEMENT (LAPP)
DETAILS

REVISION	DATE

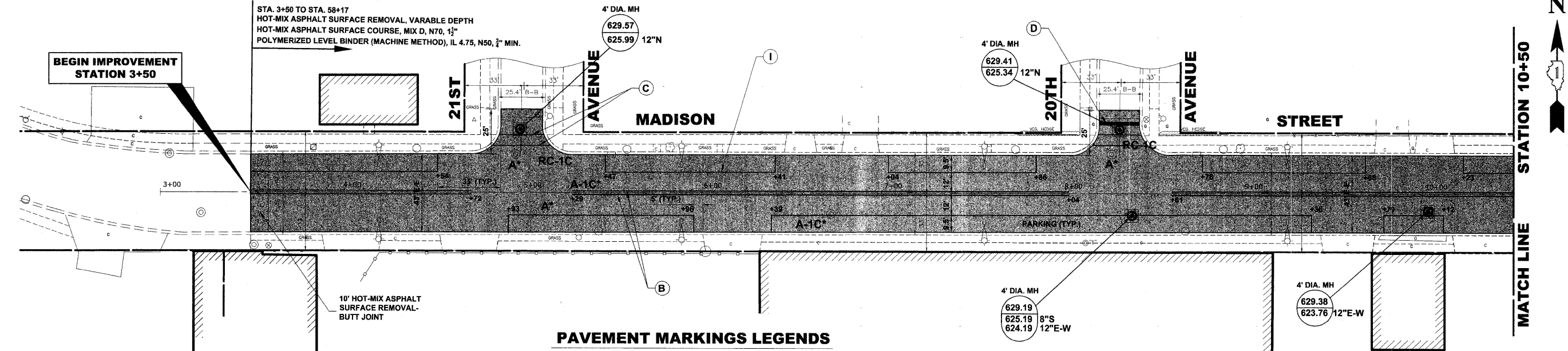


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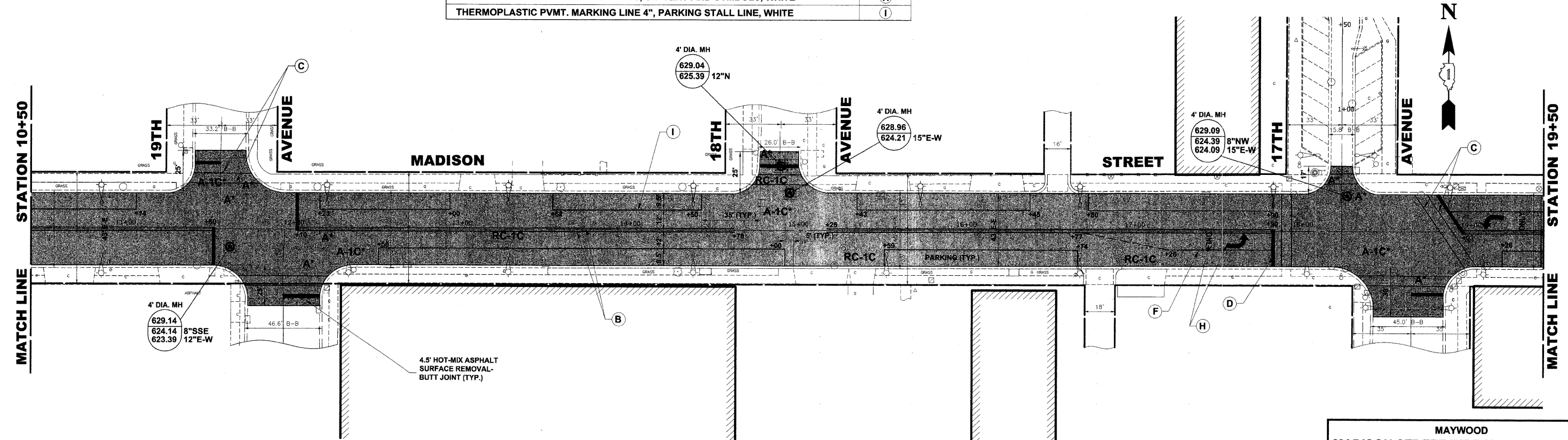
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DRAWN BY: MK
CHECKED BY: JGG
DATE: 11/10/2008



PAVEMENT MARKINGS LEGENDS

ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING, LINE 4", DOUBLE @ 11" C-C, YELLOW	(B)
THERMOPLASTIC PAVEMENT MARKING LINE 6", CROSS WALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP BAR, WHITE	(D)
THERMOPLASTIC PVMT. MARKING LINE 12", SCHOOL CROSSWALK @ 36" C-C, WHITE	(E)
THERMOPLASTIC PAVEMENT MARKING, LINE 6", TURN LANE WHITE	(F)
THERMOPLASTIC PAVEMENT MARKING, LINE 8", ISLAND EDGING, WHITE	(G)
THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, WHITE	(H)
THERMOPLASTIC PVMT. MARKING LINE 4", PARKING STALL LINE, WHITE	(I)



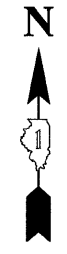
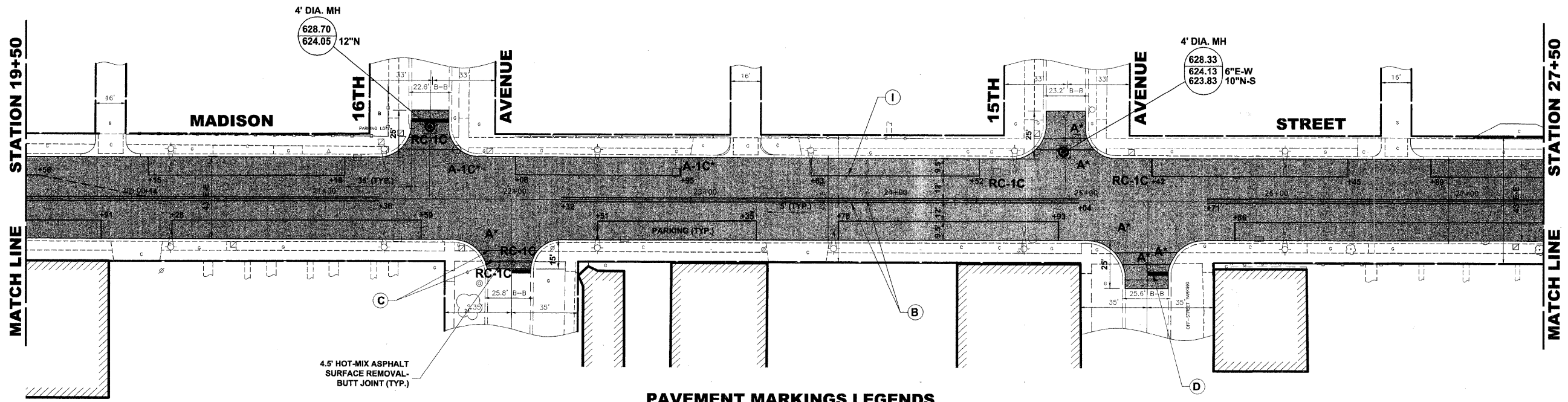
NOTE:
 * DENOTES FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).
 THE LOCATIONS FOR PATCHING "D" WORK TO BE DETERMINED IN FIELD BY THE ENGINEER.

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REVISION:

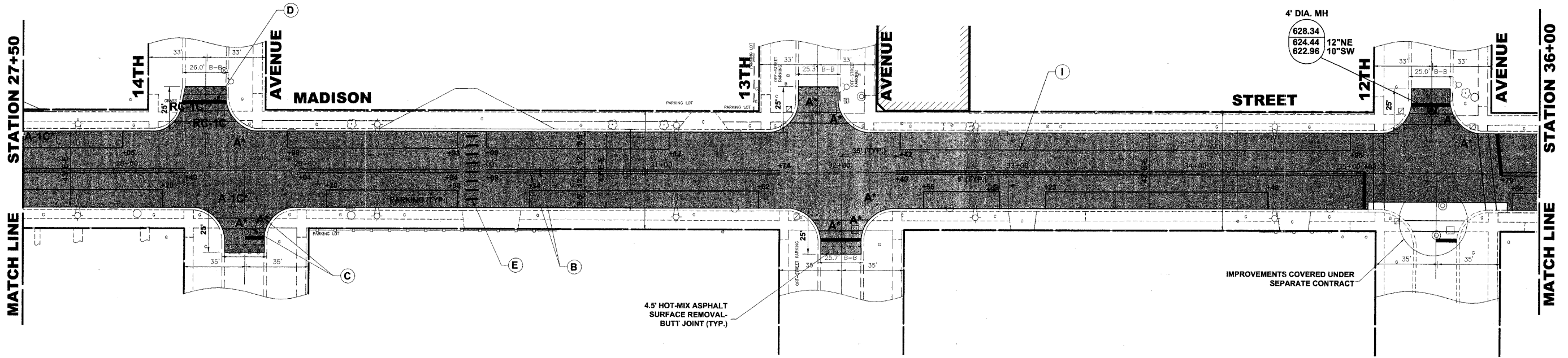
**MAYWOOD
 MADISON STREET IMPROVEMENT (LAPP)
 PAVING PLAN**

SCALE: 1"=30'
 DRAWN BY: MK
 CHECKED BY: JGG
 DATE: 11/10/2008
 E.H.E. PROJECT NO.: 565-08-20601



PAVEMENT MARKINGS LEGENDS

ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING, LINE 4", DOUBLE @ 11" C-C, YELLOW	(B)
THERMOPLASTIC PAVEMENT MARKING LINE 6", CROSS WALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP BAR, WHITE	(D)
THERMOPLASTIC PVMT. MARKING LINE 12", SCHOOL CROSSWALK @ 36" C-C, WHITE	(E)
THERMOPLASTIC PAVEMENT MARKING, LINE 6", TURN LANE WHITE	(F)
THERMOPLASTIC PAVEMENT MARKING, LINE 8", ISLAND EDGING, WHITE	(G)
THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, WHITE	(H)
THERMOPLASTIC PVMT. MARKING LINE 4", PARKING STALL LINE, WHITE	(I)



NOTE:
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 THE LOCATIONS FOR PATCHING "D" WORK TO BE DETERMINED IN FIELD BY THE ENGINEER.

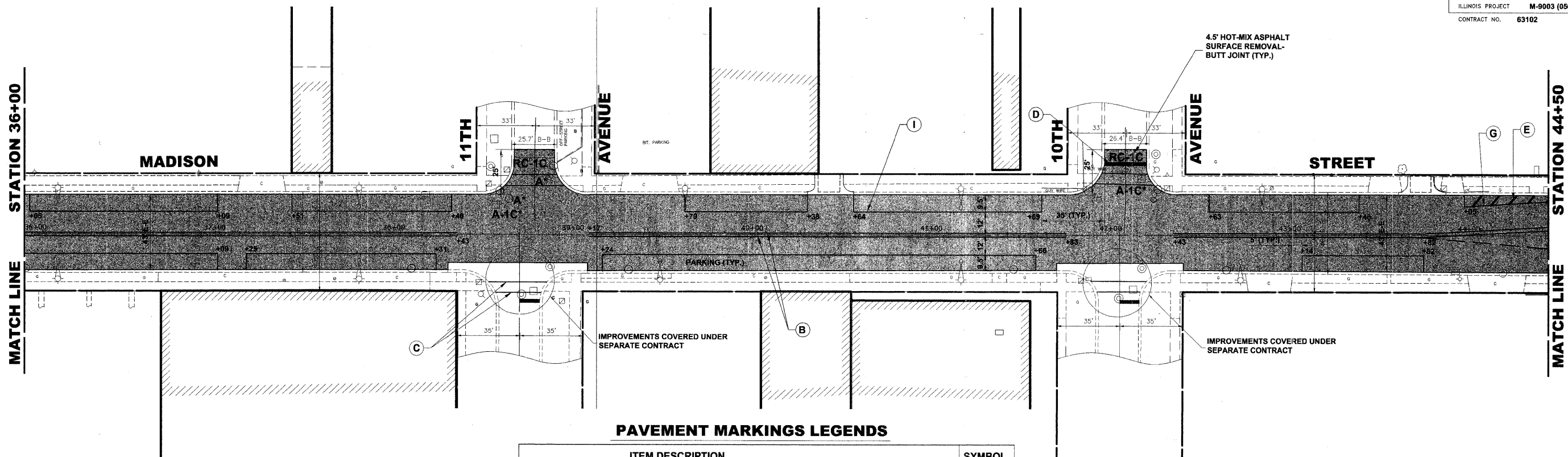
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REVISION:

**MAYWOOD
 MADISON STREET IMPROVEMENT (LAPP)
 PAVING PLAN**

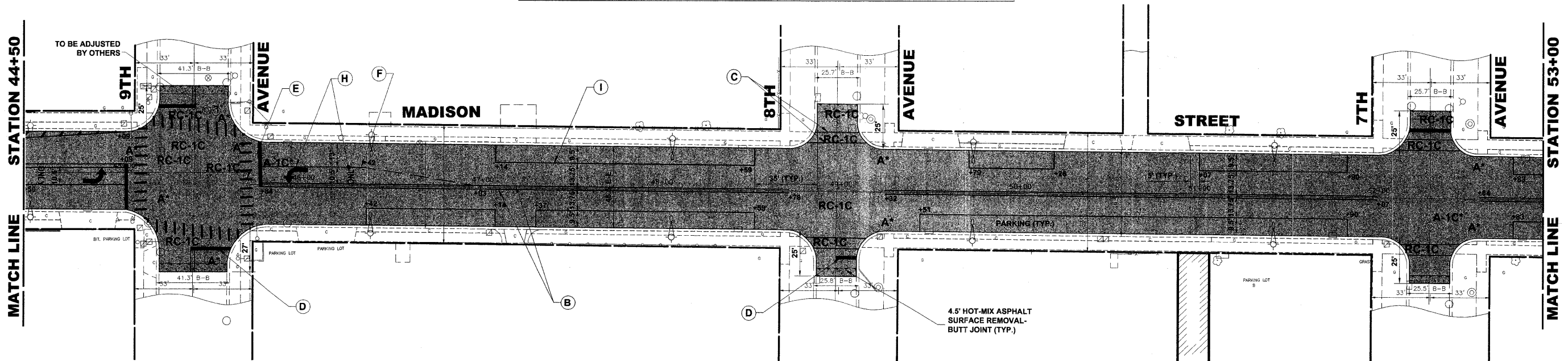
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DRAWN BY: MK
 CHECKED BY: JGG
 DATE: 11/10/2008



PAVEMENT MARKINGS LEGENDS

ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING, LINE 4", DOUBLE @ 11" C-C, YELLOW	(B)
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NOTE:

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THE LOCATIONS FOR PATCHING "D" WORK TO BE DETERMINED IN FIELD BY THE ENGINEER.

REVISION

**MAYWOOD
MADISON STREET IMPROVEMENT (LAPP)
PAVING PLAN**

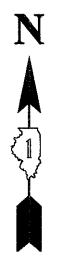
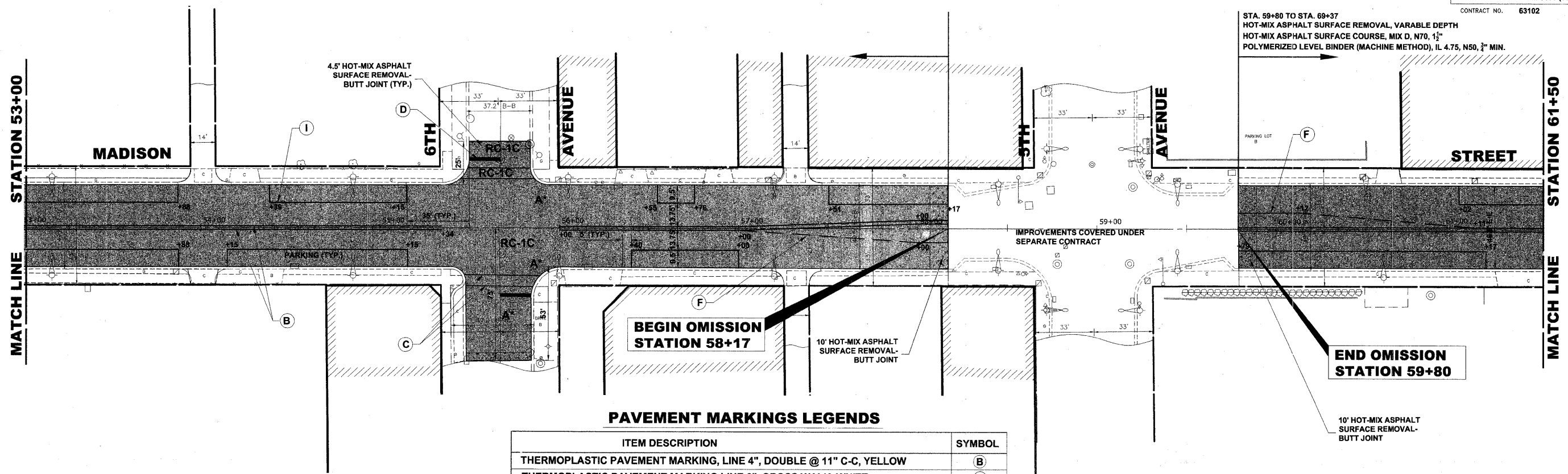
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DATE: 11/10/2008



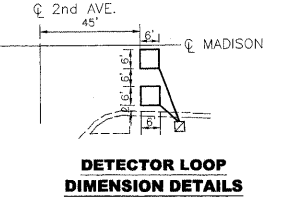
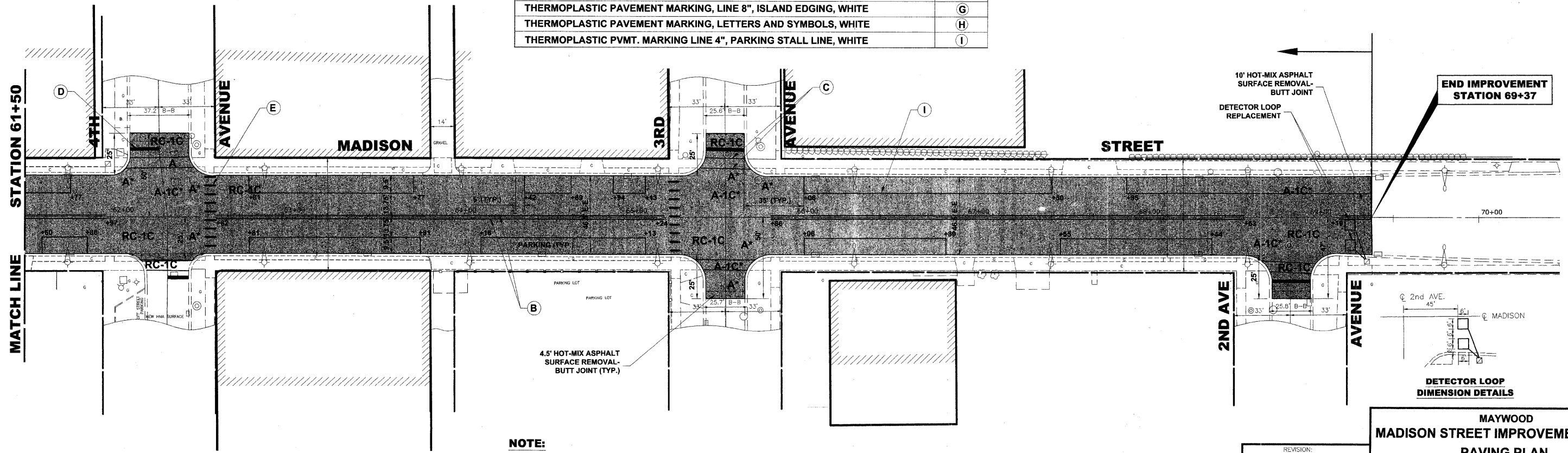
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PAVEMENT MARKINGS LEGENDS

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NOTE:
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 THE LOCATIONS FOR PATCHING "D" WORK TO BE DETERMINED IN FIELD BY THE ENGINEER.



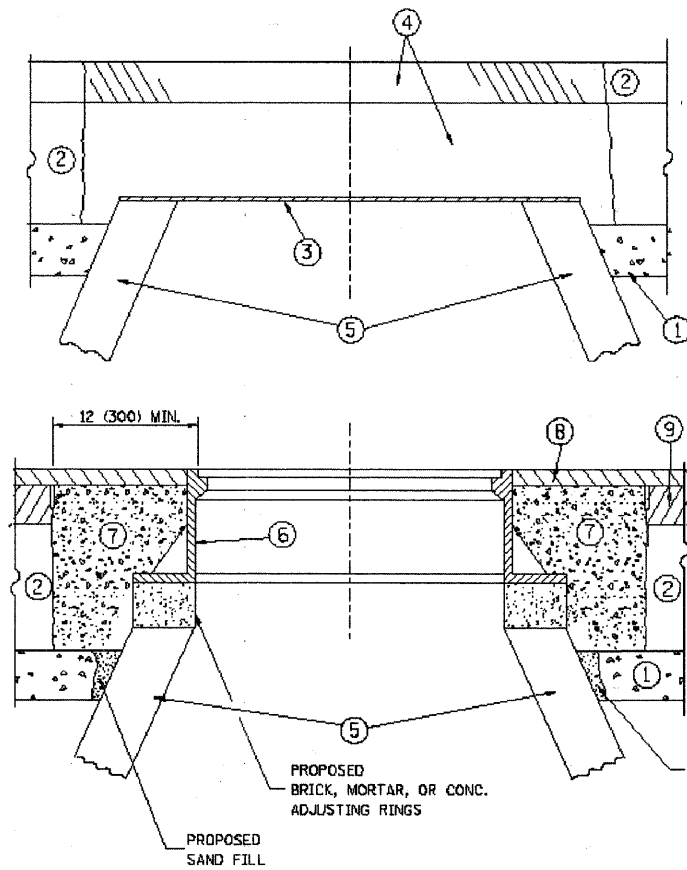
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REVISION:	

**MAYWOOD
 MADISON STREET IMPROVEMENT (LAPP)
 PAVING PLAN**

SCALE: 1"=30'
 DRAWN BY: MK
 CHECKED BY: JGC
 DATE: 11/10/2008



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

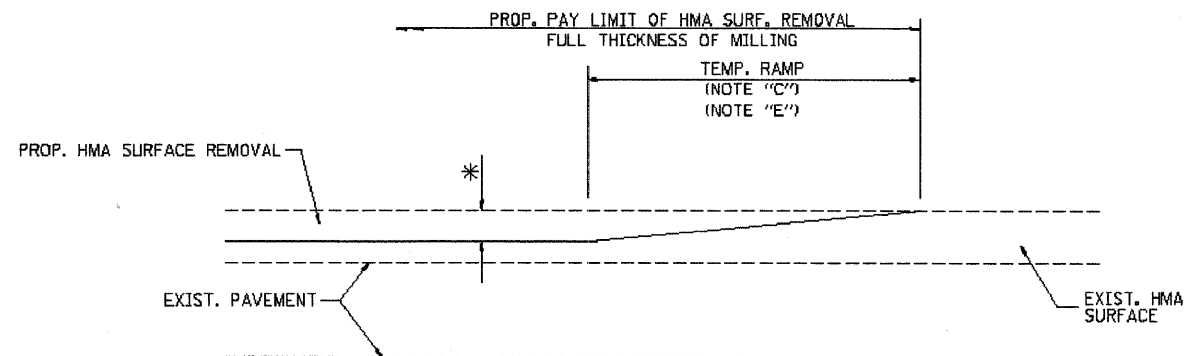
WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

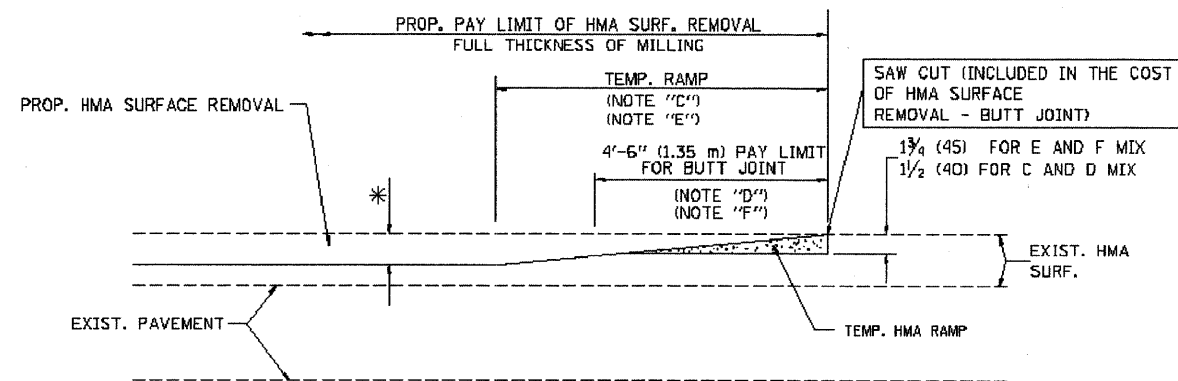
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PLOT SCALE = 58.8888 / / IN	CHECKED -	REVISED - R. WIEDEMAN 05-14-04	REVISED - A. ABBAS 03-21-97			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	80100-03 (8D-9) FED. RDAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			
PLOT DATE = 1/4/2008	DATE - 10-25-94	REVISED - R. BORO 01-01-07	REVISED - R. WIEDEMAN 05-14-04									
E.H.E. PROJECT NO. 565-08-20601												



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

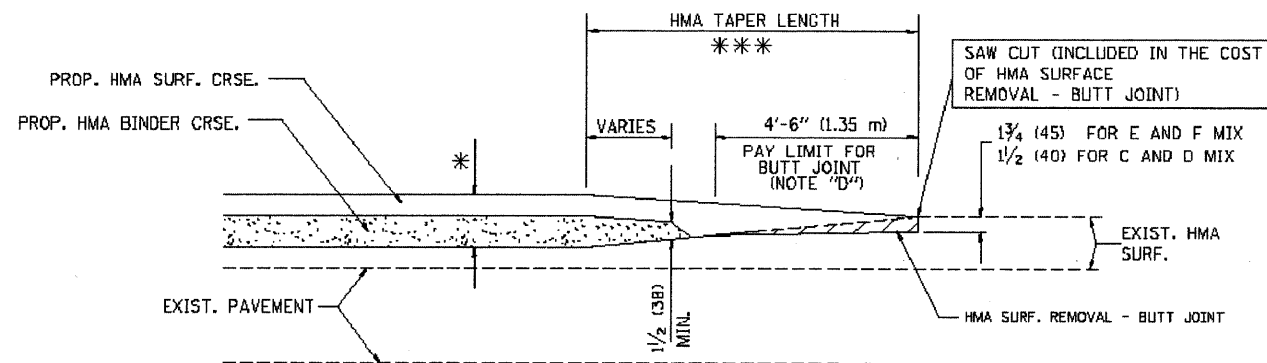
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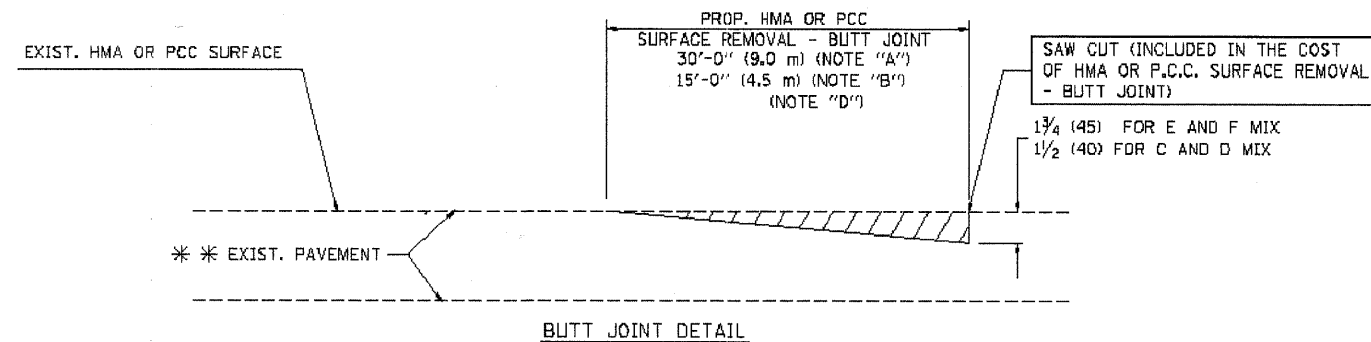
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

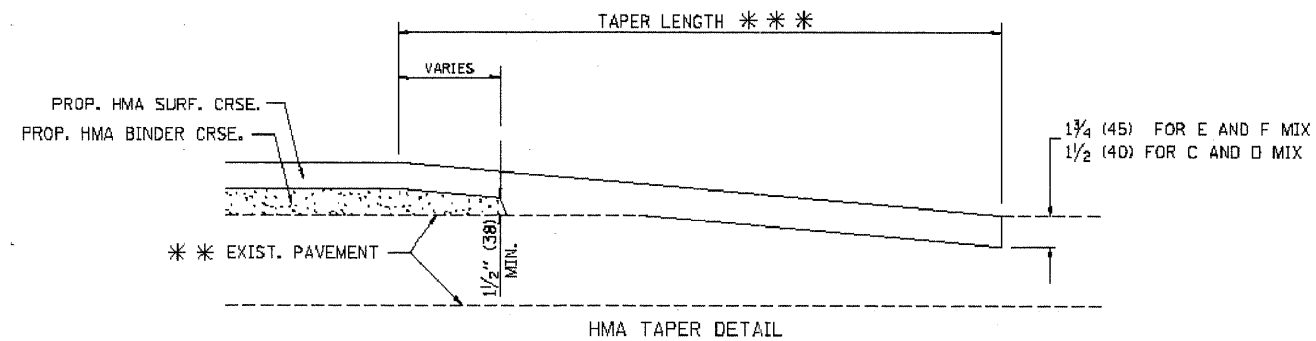
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
- 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 80.0000' / IN	CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 1/4/2008	DATE - 05-13-90	REVISED - R. BORO 01-01-07

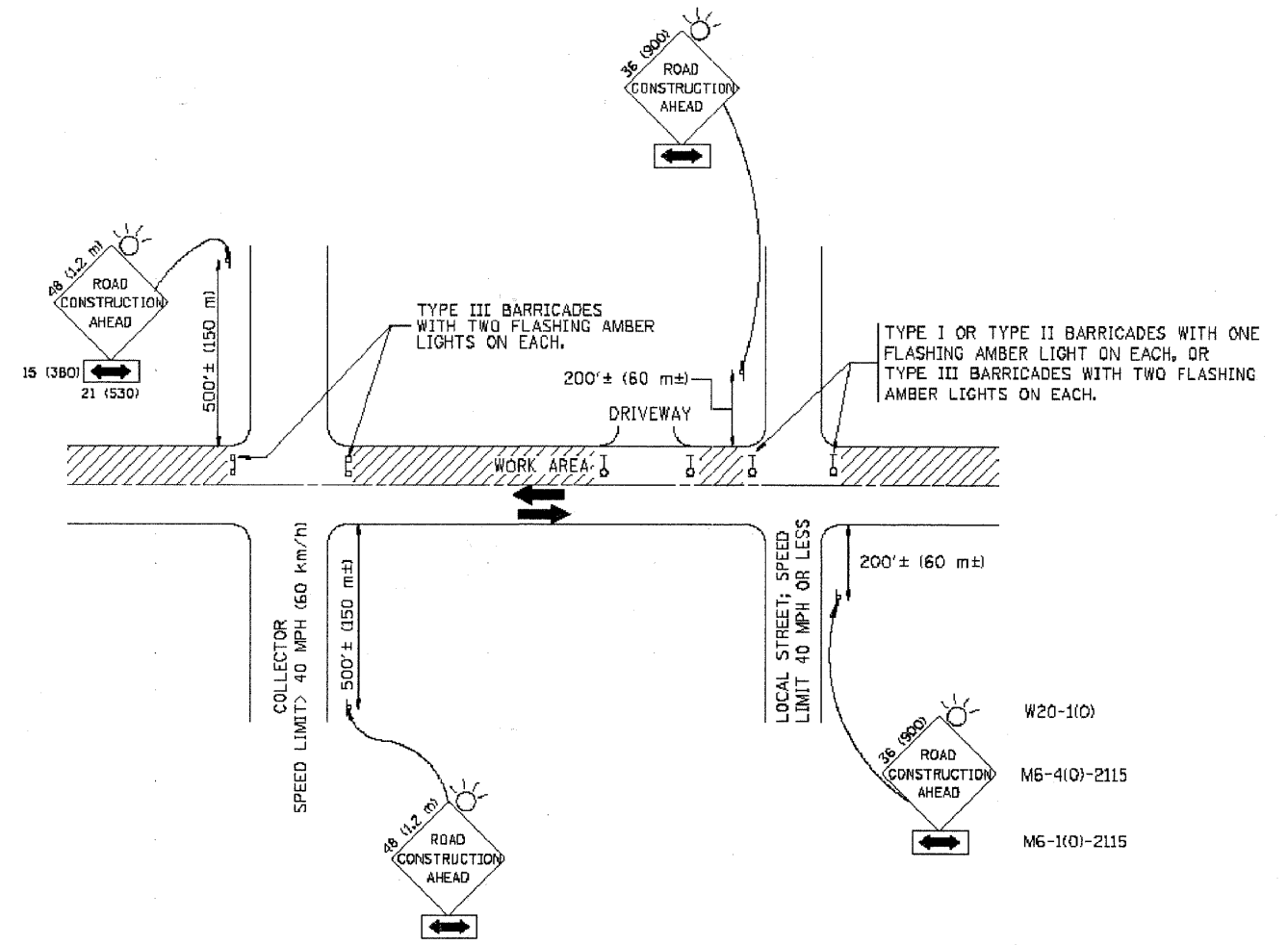
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1419	08-00127-00-RS	COOK	15	12
RD400-05 B092			CONTRACT NO. 63102	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 565-08-20601



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

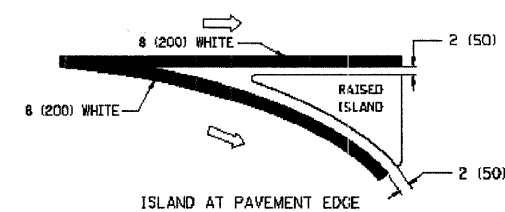
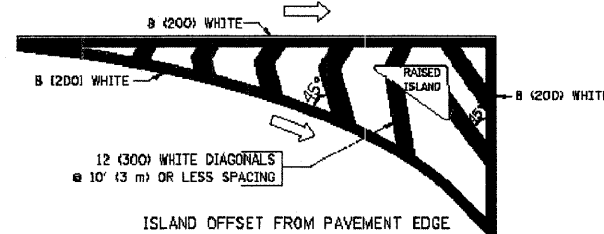
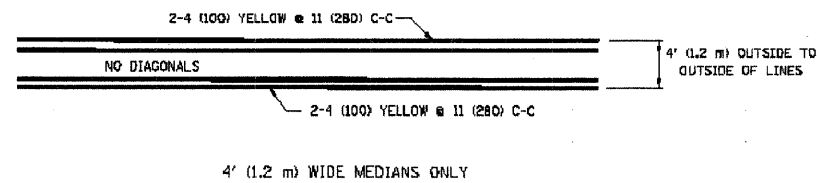
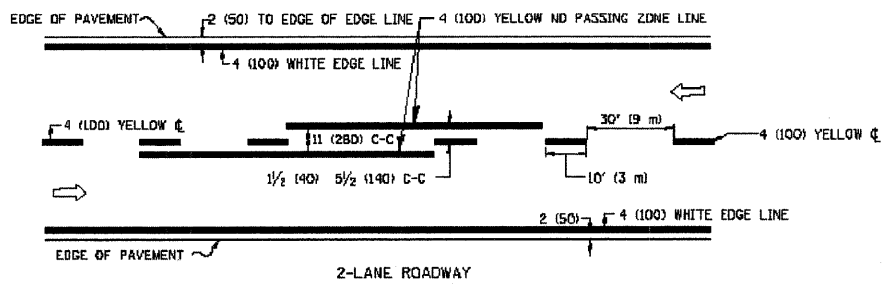
NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 - USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701605 OR THE APPROPRIATE FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
 - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
 - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

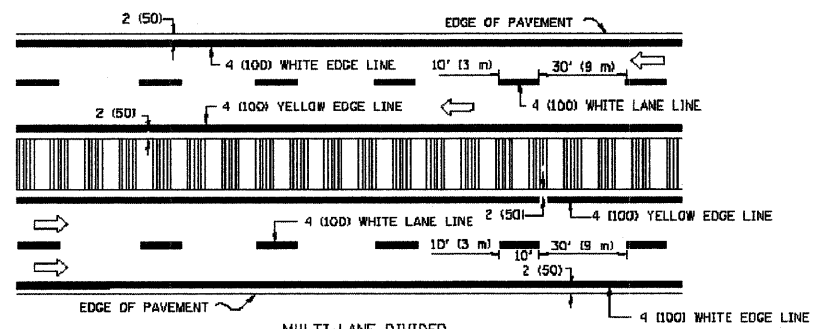
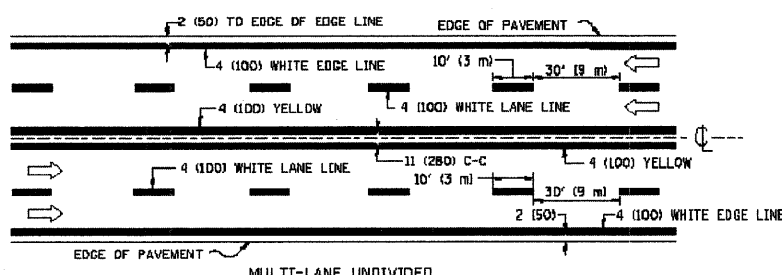
All dimensions are in millimeters (Inches) unless otherwise shown.

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PLOT DATE = 1/4/2008	DATE = 06-89	REVISED - T. RAMMACHER 01-06-00				E.H.E. PROJECT NO. 565-08-20601					

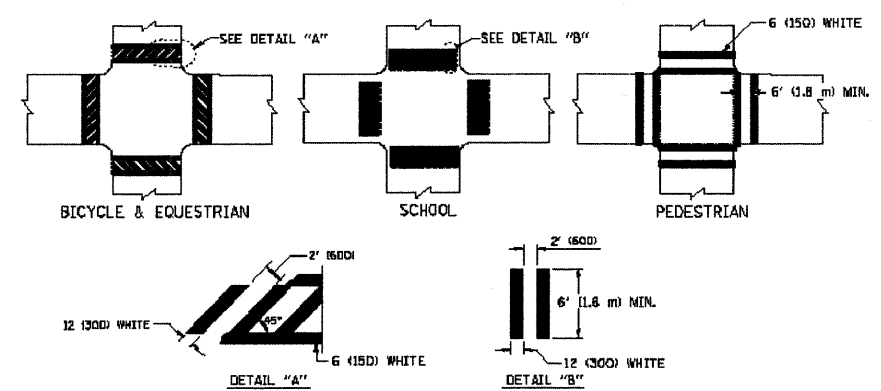


TYPICAL ISLAND MARKING

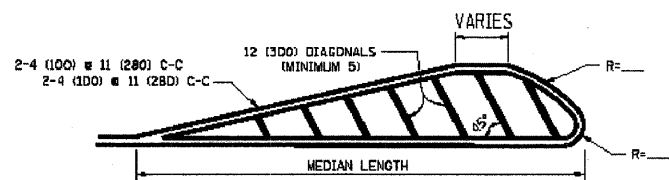


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



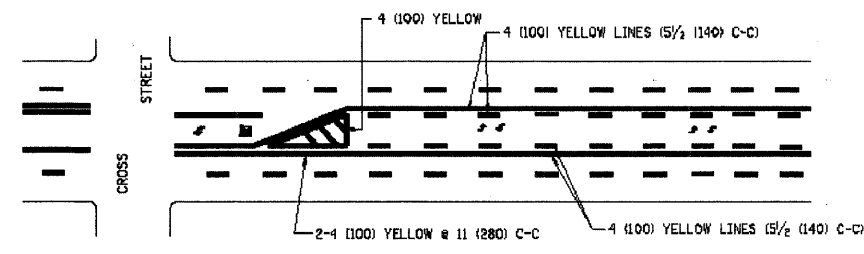
TYPICAL CROSSWALK MARKING



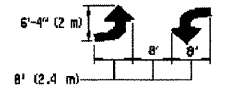
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

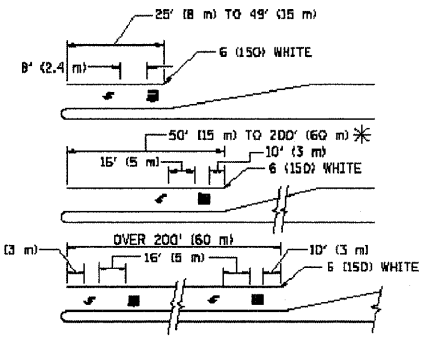


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	30' (9 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE, FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW TWO WAY TRAFFIC WHITE ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CDRE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 78000L AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 78000L.

All dimensions are in inches (millimeters) unless otherwise shown.

Drawing file: W:\Projects\6650206 - Madison Ave LAPP\MARKINGS_Madison.dwg Nov 10, 2008 - 3:51pm

FILE NAME = W:\state\22x34\1013.dgn	USER NAME = gegl1arobot	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
		DRAWN -	REVISED -A. HOUSEH 10-09-96
		CHECKED -	REVISED -A. HOUSEH 10-17-96
		DATE - 03-19-90	REVISED -T. RAMMACHER 01-06-00

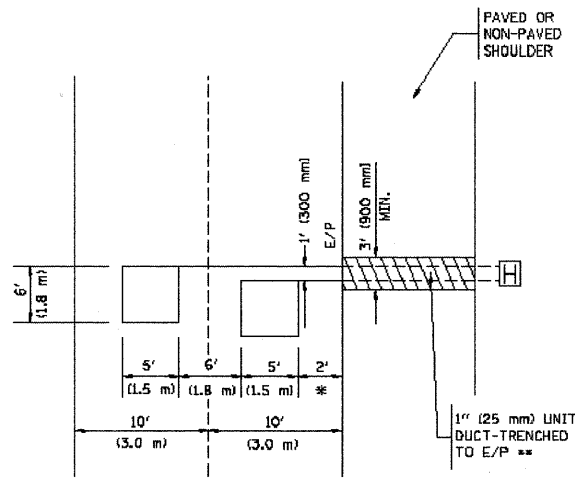
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE TYPICAL PAVEMENT MARKINGS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 1419	SECTION 08-00127-00-RS	COUNTY COOK	TOTAL SHEETS 15	SHEET NO. 14
TC-18		CONTRACT NO. 63102		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

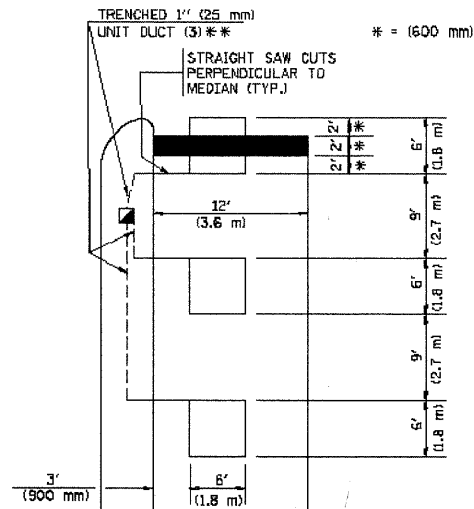


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD B1400L TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

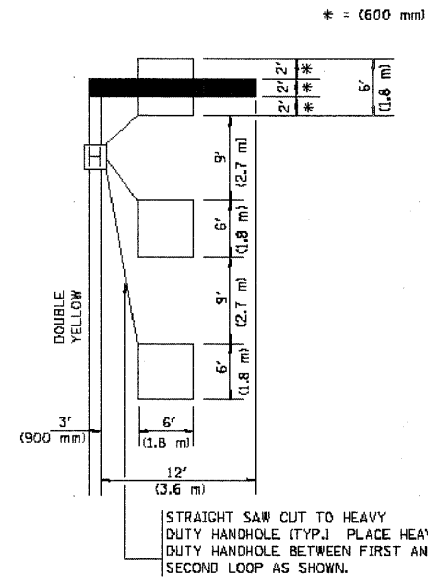


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

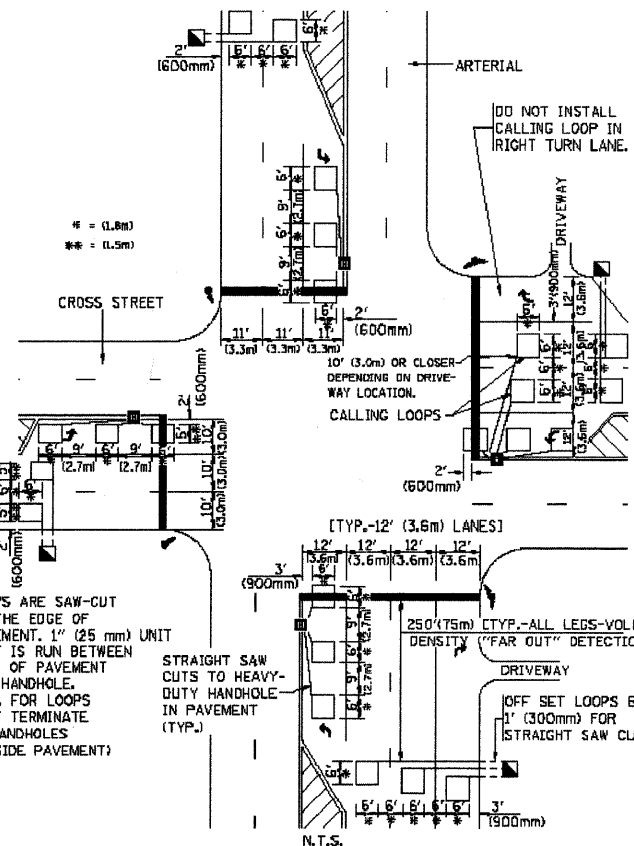
**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



* = (600 mm)

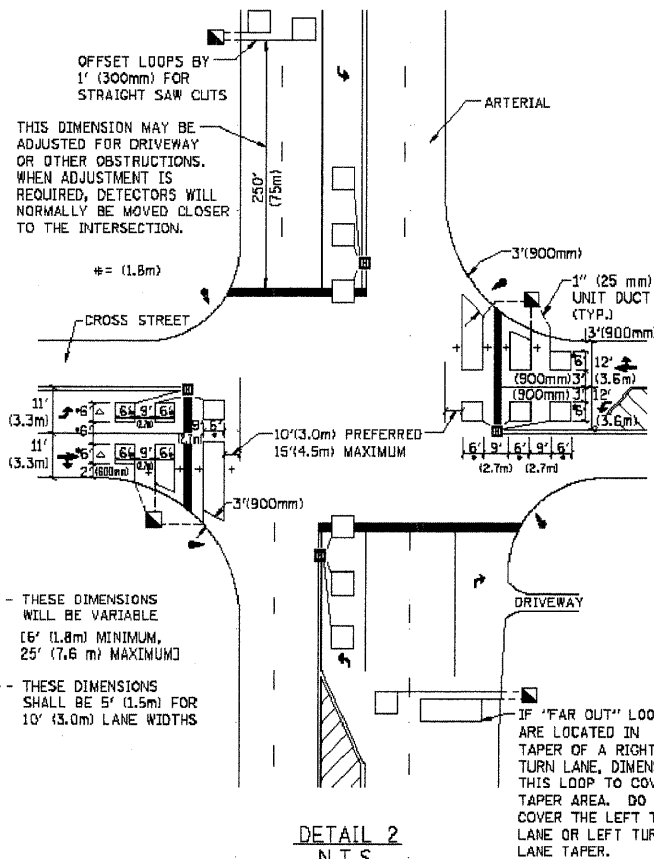
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =
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USER NAME = gegianobt
PLOT SCALE = 58.0000 / IN
PLOT DATE = 1/4/2008

DESIGNED -
DRAWN -
CHECKED - R.K.F.
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1419	08-00127-00-RS	COOK	15	15
TS-07		CONTRACT NO. 63102		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				