

GENERAL NOTES

1. Reinforcement bars shall conform to the requirements of ASTM A 706 Gr. 60. See Special Provisions.
2. Reinforcement bars designated (E) shall be epoxy coated.
3. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be caused for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
4. Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.
5. For Concrete Structures to remain in service with partial concrete removal, followed by supplemental concrete work, after concrete has been removed to the cut lines shown in the Plans, all loose and unsound material shall be completely removed by mechanical or hand methods acceptable to the Engineer. The remaining surfaces shall be blown free of dust and loose aggregate particles with compressed air. The air line must contain an oil trap to prevent oil from being deposited from the compressor. Surface dirt on the areas adjacent to the cut surface shall be removed by blast cleaning. The surfaces presented as a result of this removal shall be reasonably true and even, with sharp, straight corners that will permit constructing a neat and workmanlike joint with the new construction or be satisfactory for the purpose intended. Where existing reinforcement bars are to extend into new construction, the concrete shall be removed so as to leave the projecting reinforcing steel undamaged. Reinforcement bars to be incorporated into new construction shall be blast cleaned to grey metal. Where existing reinforcing steel is not to extend into the new construction, the reinforcing steel shall be cut off flush with the surface of the remaining portions. Care shall be exercised to prevent cutting, stretching or otherwise damaging existing reinforcing steel to be used in new construction. Reinforcing bars so damaged by the Contractor's operations shall be supplemented by new bars spliced into place or by embedded (drilled & grouted) reinforcing steel or anchorage, equal to or greater than the original reinforcing steel. Such supplemental bars or anchorage shall be furnished and placed by the Contractor at no additional cost and without cause for the Contractor claiming delay.
6. Vertical reinforcement bars in the existing wingwalls at the junction with the existing backwalls, exposed during concrete removal, shall be reused in the new stub backwalls and in new bearing pedestals as appropriate and as shown for new construction of the South Abutment and the North Abutment.

TOTAL BILL OF MATERIAL

Item	Unit	Super	Sub	Total
Porous Granular Embankment (Special)	CU YD		145	145
Stone Riprap, Class A4	SQ YD		95	95
Stone Riprap, Class A5	TON		2,820	2,820
Stone Dumped Riprap, Class A3	TON		25	25
Filter Fabric	SQ YD		1,120	1,120
Removal of Existing Superstructures	EACH	1		1
Concrete Removal	CU YD		43.0	43.0
Structure Excavation	CU YD		1,560	1,560
Concrete Structures	CU YD		18.1	18.1
Concrete Superstructure	CU YD	306.0		306.0
Bridge Deck Grooving	SQ YD	698		698
Protective Coat	SQ YD	920		920
Furnishing and Erecting Precast Prestressed Concrete I-Beams, 48 in.	FOOT	1,220		1,220
Reinforcement Bars, Epoxy Coated	POUND	65,130	4,130	69,260
Bar Splicers	EACH	706	12	718
Temporary Sheet Piling	SQ FT		560	560
Name Plates	EACH	1		1
Elastomeric Bearing Assembly, Type I	EACH	24		24
Anchor Bolts, 1 1/4"	EACH	36		36
Anchor Bolts, 1/2"	EACH	16		16
Geocomposite Wall Drain	SQ YD		100	100
Pipe Underdrains for Structures, 4"	FOOT		164	164
Polymer Modified Portland Cement Mortar	SQ FT		165	165

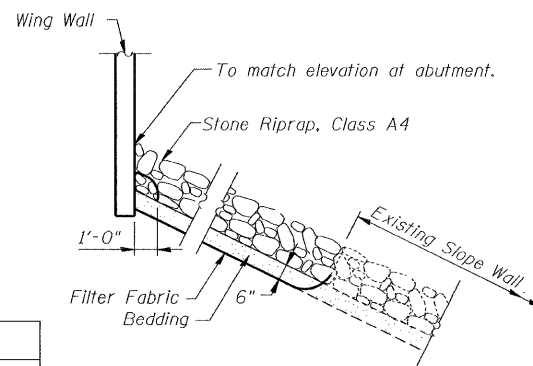
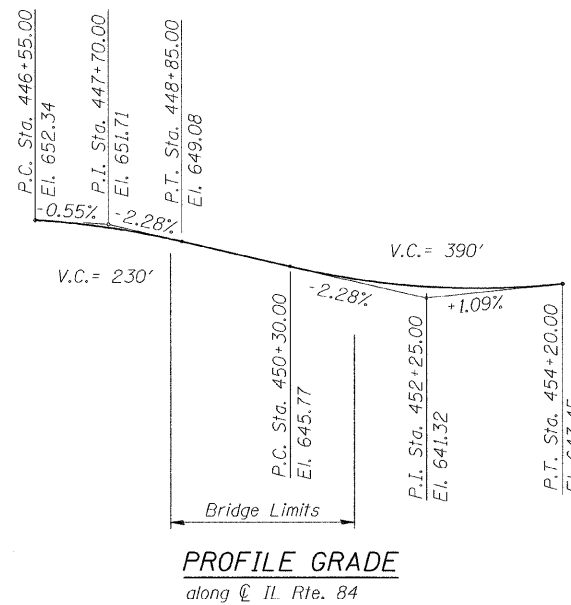
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
308	103C-1BRD	Jo Daviess	62	25

SHEET NO. 2 OF 26 SHEETS

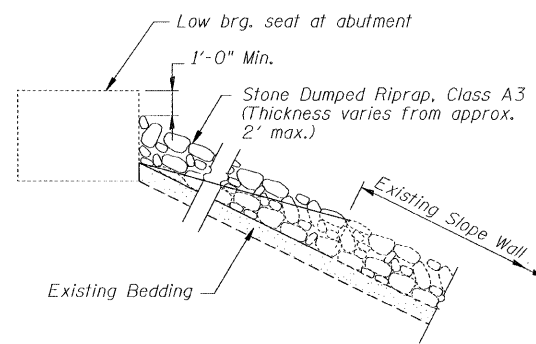
Contract # 64C03

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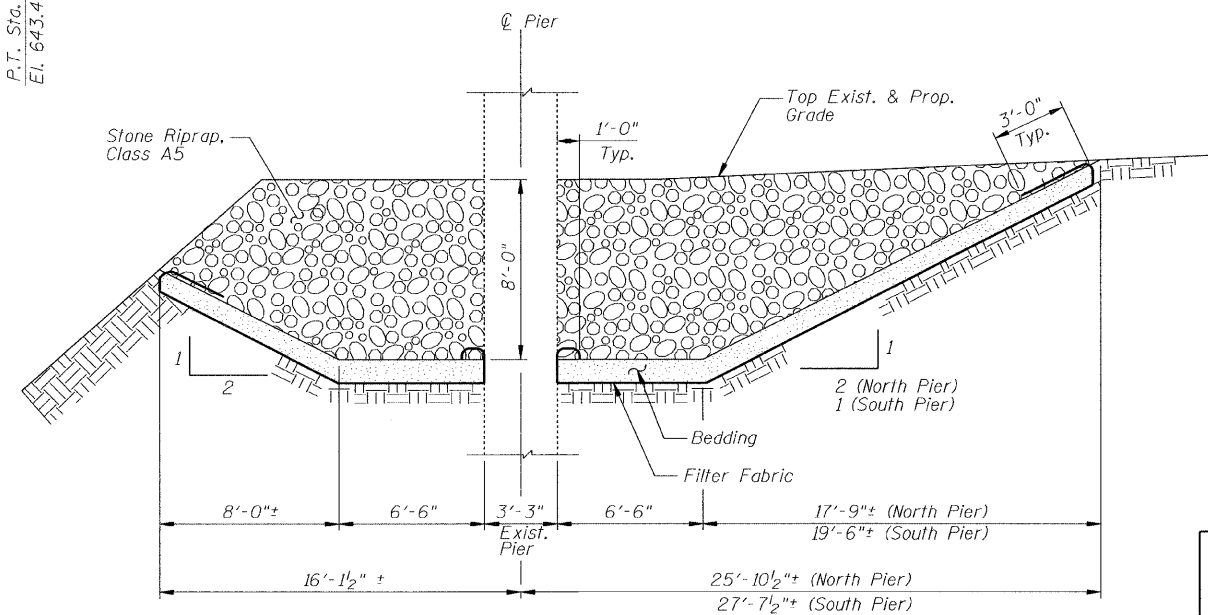
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SECTION AT NEW WINGWALL



SECTION AT SOUTH ABUTMENT



STONE RIPRAP DETAIL AT PIERS

(Looking West through North Pier)
(Looking East through South Pier, Sim.)

DESIGNED	J.Z.
CHECKED	S.D.H.
DRAWN	M.S.M.
CHECKED	J.Z.

GENERAL NOTES & TOTAL BILL OF MATERIAL
IL Route 84 over Irish Hollow Creek
F.A.P. RTE 308, SECTION (103C-1BR)D
JO DAVIESS COUNTY
STATION 449+62.06
S.N. 043-0037
DATE: 12-11-08
GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
CHICAGO ILLINOIS