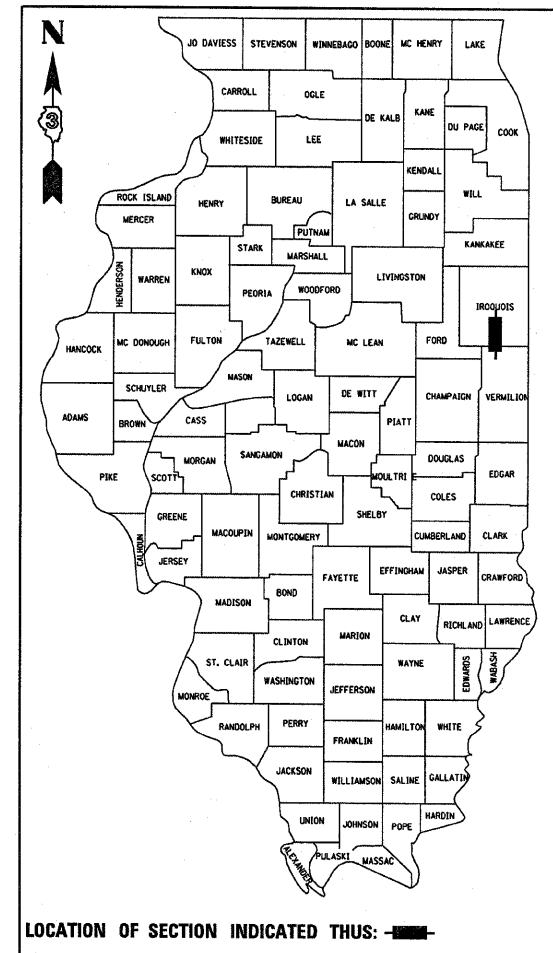


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	*	IROQUOIS	16	1
ILLINOIS PROJECT				
*(123)RS-1 & (135)RS-2				

D-93-026-04



LOCATION OF SECTION INDICATED THUS:

FUNCTION CLASSIFICATION RURAL MINOR ARTERIAL

RURAL 2006 ADT = 1750  
P.V. = 84.3% S.U. = 8.6% M.U. = 7.1%

CITY 2006 ADT = 3200  
P.V. = 88.6% S.U. = 8.6% M.U. = 2.8%

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED \_\_\_\_\_ 20\_\_\_\_

*David Johnson*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

*Charles J. Ingersoll*  
ENGINEER OF DESIGN AND ENVIRONMENT

*Christina M. Reed*  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS

# STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS PLANS FOR PROPOSED FEDERAL AID HIGHWAY

ROUTE: FAP 840 (IL 49)

SECTION: (123)RS-1 & (135)RS-2

PROJECT ACF-0840(060)

COUNTY: IROQUOIS

C - 93 - 027 - 04

IL 49 FROM THE VERMILLION /IROQUOIS COUNTY LINE

TO COUNTY HIGHWAY 9 NORTH OF CISSNA PARK

END IMPROVEMENT  
STA 567+75

OMISSION  
STA 468+35.63 TO STA 470+12.43  
S.N. 038-0198

OMISSION  
STA 277+10 TO STA 289+50  
S.N. 038-0217

OMISSION  
STA 135+27.25 TO STA 136+20.75  
S.N. 038-0174

OMISSION  
STA 8+39.34 TO STA 9+53.76  
S.N. 038-0190

STATION EQUATION  
STA 274+85.48 BK =  
STA 275+00.00 AH

BEGIN IMPROVEMENT  
STA 0-38



LOCATION MAP  
NOT TO SCALE

GROSS LENGTH = 56,798 FT. = 10.76 MI.  
NET LENGTH = 55,173.28 FT. = 10.45 MI.

INDEX OF SHEETS

- 1 COVER SHEET
- 2 GENERAL NOTES
- 3 SUMMARY OF QUANTITIES
- 4 LOCATION MAP
- 5-6 TYPICAL SECTIONS
- 7 PAVEMENT & BIT SURF REM SCHEDULE
- 8 MISCELLANEOUS SCHEDULES
- 9 ENTRANCE SCHEDULE
- 10 PAVEMENT MARKING SCHEDULE
- 11-16 DETAILS

HIGHWAY STANDARDS

- 000001-05 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 001001-02 AREAS OF REINFORCEMENT BARS
- 001006 DECIMAL OF AN INCH AND OF A FOOT
- 406201-01 MAILBOX TURNOUT
- 667101-01 PERMANENT SURVEY MARKERS
- 701001-02 OFF-ROAD OPERATIONS 2L, 2W, MORE THAN 15' (4.5 m) AWAY
- 701006-03 OFF-ROAD OPERATIONS 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
- 701011-02 OFF-ROAD MOVING OPERATIONS 2L, 2W, DAY ONLY
- 701201-03 LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS P 45 MPH
- 701301-03 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701306-02 LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS P 45 MPH
- 701311-03 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
- 701501-05 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
- 701901-01 TRAFFIC CONTROL DEVICES
- 780001-02 TYPICAL PAVEMENT MARKINGS
- 781001-03 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

MICROFILMED \_\_\_\_\_  
REEL NUMBER \_\_\_\_\_  
AWARDED \_\_\_\_\_  
RESIDENT ENGINEER \_\_\_\_\_  
AS BUILT CHANGES WERE MADE  
ON THE FOLLOWING SHEETS \_\_\_\_\_

JULIE 1-800-892-0123

DISTRICT 3 NO. (815) 434-6131

PROJECT ENGINEER: JOE KANNEL  
UNIT CHIEF: PAT BRABOY  
TOWNSHIP: ASH GROVE; PIGEON GROVE

CONTRACT NO. 66425

F. A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
840	.	IROQUOIS	16	2
STA.	TO STA.			
FED. ROAD DIST. NO. 5	ILLINOIS	FED. AID PROJECT		

•(123)RS-1 & (135)RS-2

**GENERAL NOTES**

THE THICKNESS OF BITUMINOUS MIXTURES SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.

THE BITUMINOUS SURFACE OF ALL MAILBOX TURNOUTS, PRIVATE ENTRANCES, COMMERCIAL ENTRANCES, AND SIDE ROADS SHALL BE MADE NEATLY, IN A WORKMANLIKE MANNER, AND SHALL ACCURATELY CONFORM TO THE SHAPES AND DIMENSIONS SHOWN ON THE PLAN DETAILS. IF REQUIRED BY THE ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SAW CUT THE BITUMINOUS SURFACE TO CONFORM TO THE SHAPES AND DIMENSIONS SHOWN ON THE PLAN DETAILS. THIS WORK SHALL BE INCLUDED IN THE COST OF THE BITUMINOUS SURFACE.

BEFORE ORDERING PIPE CULVERTS OR PIPE DRAINS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR EXACT LENGTHS.

AGGREGATE (PRIME COAT): FA 20 MAY BE USED IN ADDITION TO THE GRADATIONS LISTED IN THE 3RD PARAGRAPH OF ARTICLE 1003.03(c) OF THE STANDARD SPECIFICATIONS.

THE ENGINEER WILL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS BITUMINOUS LIFTS.

WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL MONUMENTS UNTIL AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR REESTABLISH ANY SECTION OR SUBSECTION MONUMENTS DESTROYED BY HIS OPERATIONS.

SEEDING SHALL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION. LOCATIONS TO BE SEEDED WILL BE DETERMINED BY THE ENGINEER.

THE FINISHED EARTHWORK SHALL HAVE A VEGETATION SUSTAINING SOIL COVERING THE TOP FOUR INCHES IN AREAS TO BE SEEDED OR SODDED. THE VEGETATION SUSTAINING SOIL REQUIRED WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF FURNISHED EXCAVATION.

ON EXISTING PAVEMENT WHICH MAY BE SUPERELEVATED, THE NEW BITUMINOUS PAVEMENT SHALL BE BUILT WITH THE SAME SUPERELEVATION UNLESS NEW SUPERELEVATION RATES ARE GIVEN ON THE PLANS.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER LISTED ON THE INDEX OF SHEETS OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

GRANULAR MATERIALS	2.05 TONS / CU YD
BITUMINOUS MAT PRIME COAT	0.08 GAL / SQ YD
	OR
	0.375 GAL / SQ YD
AGGREGATE PRIME COAT	0.002 TONS / SQ YD
BITUMINOUS RESURFACING	112 LBS / SQ YD /
	IN
SHORT TERM PAVEMENT MARKING	10 FT / 100 FT OF APPLICATION
MIX FOR CRACKS, JTS & FLGWYS	0.0003 TONS / SQ YD
LEVEL BINDER (HAND METHOD)	0.0005 TONS / SQ YD

ALL DAMAGE TO DEPARTMENT OWNED UNDERGROUND FACILITIES, CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE DEPARTMENT AT THE CONTRACTOR'S EXPENSE. THIS SHALL INCLUDE ALL TEMPORARY REPAIRS REQUIRED TO KEEP THE FACILITY OPERATIONAL WHILE MATERIAL IS BEING OBTAINED TO MAKE PERMANENT REPAIRS. SPLICING OF ELECTRIC CABLE SHALL NOT BE ALLOWED. ELECTRIC CABLE SHALL BE REPLACED FROM POLE TO POLE OR CONTROLLER.

THE COST OF MAKING ANY SEWER CONNECTIONS TO AN EXISTING DRAINAGE STRUCTURE OR PIPE SHALL BE INCLUDED IN THE COST OF THE NEW SEWER.

MEMBERS OF JULIE KNOWN TO BE WITHIN THE LIMITS OF THE IMPROVEMENT ARE: AMEREN CIPS, VERIZON, NICOR GAS, EASTERN ILLINOIS ELECTRIC CORP, PARK TV & ELECTRONICS, VILLAGE OF CISSNA PARK.

THE EXISTING SHOULDERS ARE LOW. IN THE EVENT THAT CONSTRUCTION OPERATIONS RESULT IN A DROP OFF GREATER THAN 3" BETWEEN THE EDGE OF PAVEMENT OR EDGE OF SHOULDER WITHIN 3' OF THE EDGE OF PAVEMENT AND THE EARTH OR AGGREGATE SHOULDERS, THEN THE DROP OFF SHALL BE PROTECTED ACCORDING TO ARTICLE 701.04(B)(1) OF THE STANDARD SPECIFICATIONS. THIS WORK WILL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICES FOR TRAFFIC CONTROL.

	SUPERPAVE LEVEL BINDER	SUPERPAVE SURFACE
PG GRADE	PG 64-22	PG 64-22
MAX % RAP ALLOWABLE**	25%	15%
DESIGN AIR VOIDS	4% @ N50	4% @ N50
MIXTURE COMPOSITION	IL 9.5	IL 12.5 OR IL 9.5
FRICTION AGGREGATE		MIXTURE C
DENSITY CONTROL LIMITS		CORRELATION

\*\* IF RAP OPTION IS SELECTED, THE ASPHALT CEMENT GRADE MAY NEED TO BE ADJUSTED. THIS WILL BE DETERMINED BY THE ENGINEER.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DISTRICT THREE

PREPARED BY: [Signature]  
DISTRICT STUDIES & PLANS ENGINEER

DATE: 12-10-08

EXAMINED BY: [Signature]  
DISTRICT CONSTRUCTION ENGINEER

[Signature]  
DISTRICT MATERIALS ENGINEER

[Signature]  
DISTRICT OPERATIONS ENGINEER

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**GENERAL NOTES**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	*	IROQUOIS	16	3
STA.		TO STA.		
FED. ROAD DIST. NO. 8		ILLINOIS	FED. AID PROJECT	

\*(123)RS-1 & (135)RS-2

SUMMARY OF QUANTITIES				80% FED.
CODE NO.	ITEM	UNIT	CONSTRUCTION CODE TYPE: I000	
			20% STATE	I000 TOTAL QUANTITY
20400800	FURNISHED EXCAVATION	CU YD		52
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON		13,796
40600300	AGGREGATE (PRIME COAT)	TON		345
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON		49
40600525	LEVELING BINDER (HAND METHOD), N50	TON		81
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON		6652
40600895	CONSTRUCTING TEST STRIP	EACH		1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD		2,485
40600990	TEMPORARY RAMP	SQ YD		247
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON		14364
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON		875
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD		80,769
44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD		173
48101200	AGGREGATE SHOULDERS, TYPE B	TON		2,816
50102400	CONCRETE REMOVAL	CU YD		0.07
50800105	REINFORCEMENT BARS	POUND		23
54002020	EXPANSION BOLTS 3/4 INCH	EACH		14
54248510	CONCRETE COLLAR	CU YD		0.27
66700205	PERMANENT SURVEY MARKERS, TYPE I	EACH		4
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO		3
67100100	MOBILIZATION	L SUM		1
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM		1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM		1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT		16,672
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT		122
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT		114,528
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT		13,453
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT		78

SUMMARY OF QUANTITIES				80% FED.
CODE NO.	ITEM	UNIT	CONSTRUCTION CODE TYPE: I000	
			20% STATE	I000 TOTAL QUANTITY
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT		1,852
78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT		122
78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT		1,349
78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT		934
78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT		78
78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT		113,179
78001130	PAINT PAVEMENT MARKING - LINE 6"	FOOT		12,519
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH		766
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH		766
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM		1
X0325866	CENTERLINE RUMBLE STRIP	FOOT		4000
X0914100	PRECAST CONCRETE BOX CULVERT END SECTIONS 3' X 2'	EACH		1

\* SPECIALTY ITEMS

200.../SCHEDULES.DGN

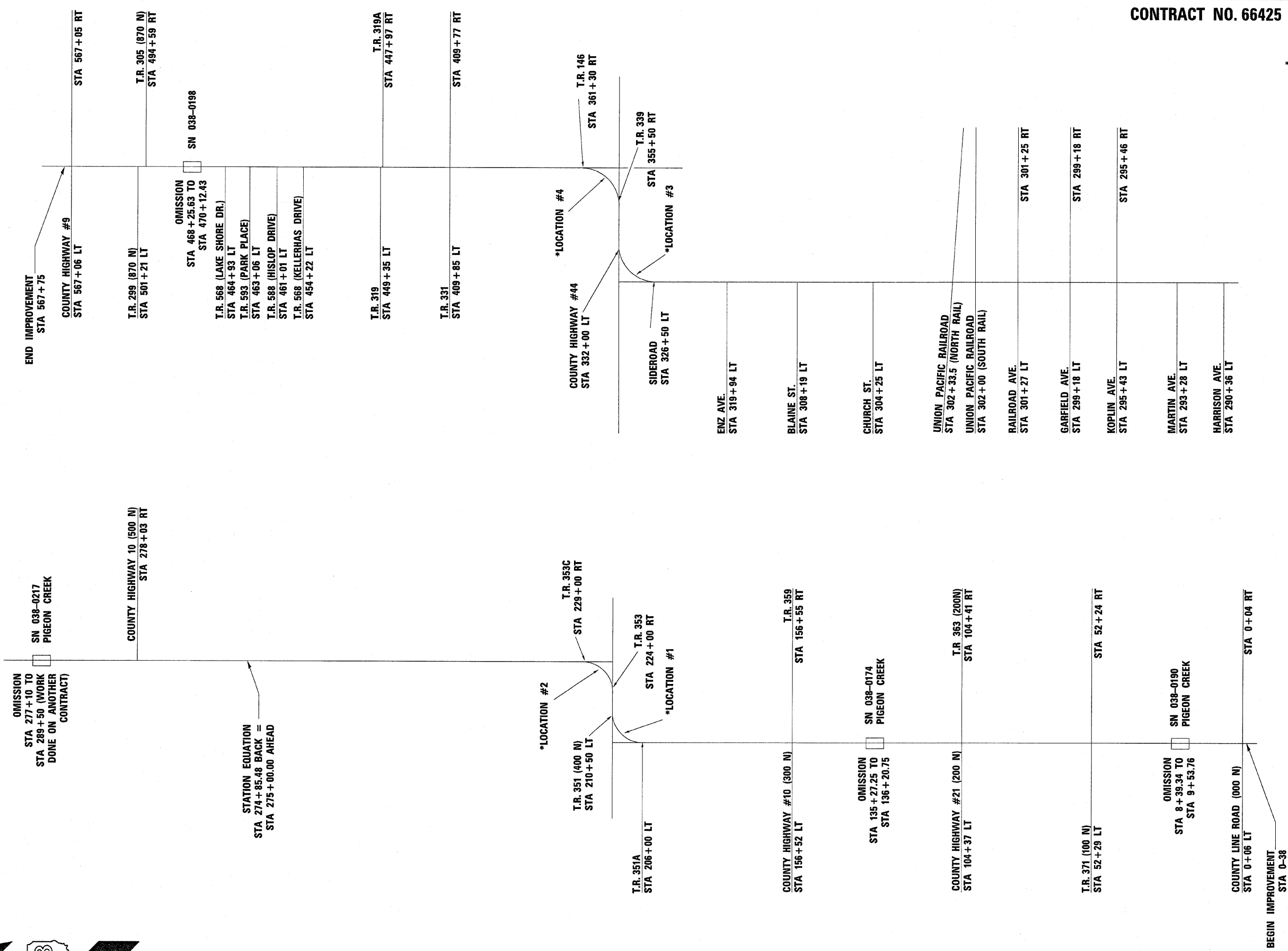
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SUMMARY OF QUANTITIES**

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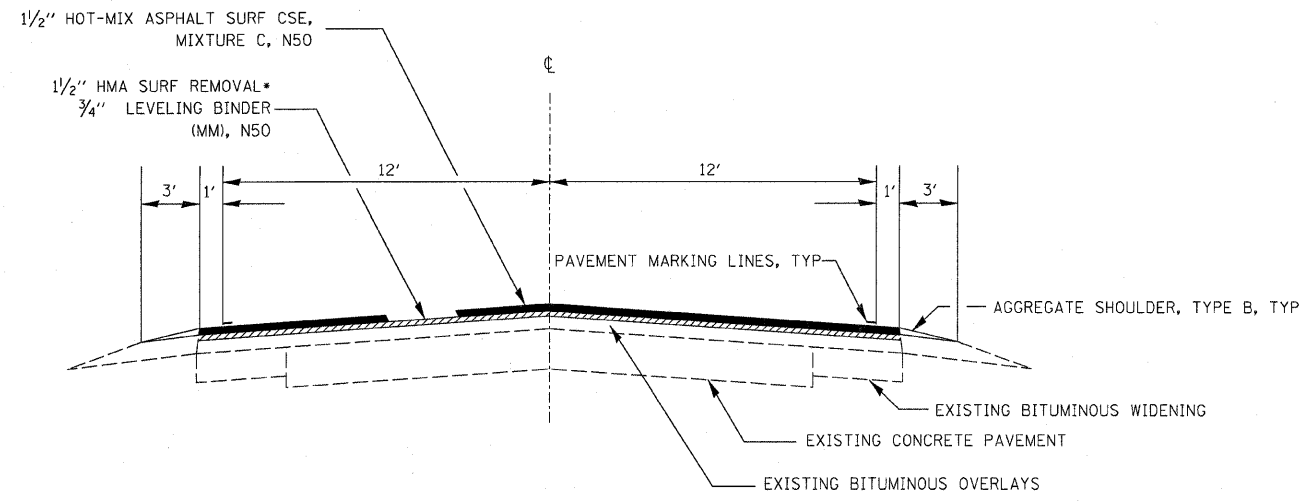
\*LOCATION # LOCATION FOR CENTERLINE RUMBLE STRIPS-SEE DETAILS

F.A.P. RTE:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840		IROQUOIS	16	4
STA. _____		TO STA. _____		
FED. ROAD DIST. NO. 5		ILLINOIS FED. AID PROJECT		

• (123)RS-1 & (135)RS-2

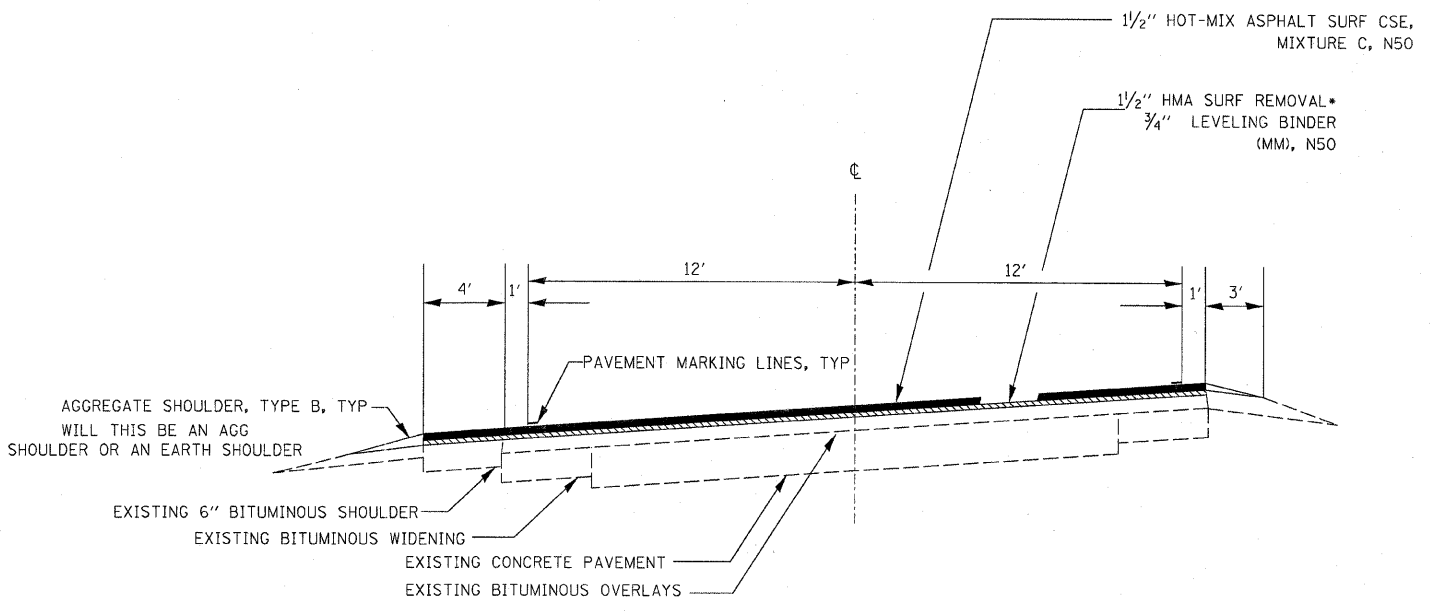
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840		IRROQUOIS	16	5
STA. _____		TO STA. _____		
FED. ROAD DIST. NO. 3 ILLINOIS FED. AID PROJECT				

\* (123)RS-1 & (135)RS-2



**TYPICAL SECTION**

LOCATIONS	MILLING DEPTHS* (SEE SCHEDULE)
STA 0-38 TO STA 202+51	0"
STA 213+13 TO STA 221+06	0"
STA 231+71 TO STA 277+10	0"
STA 334+36 TO STA 353+04	1/2"
STA 363+66 TO STA 567+75	1/2"

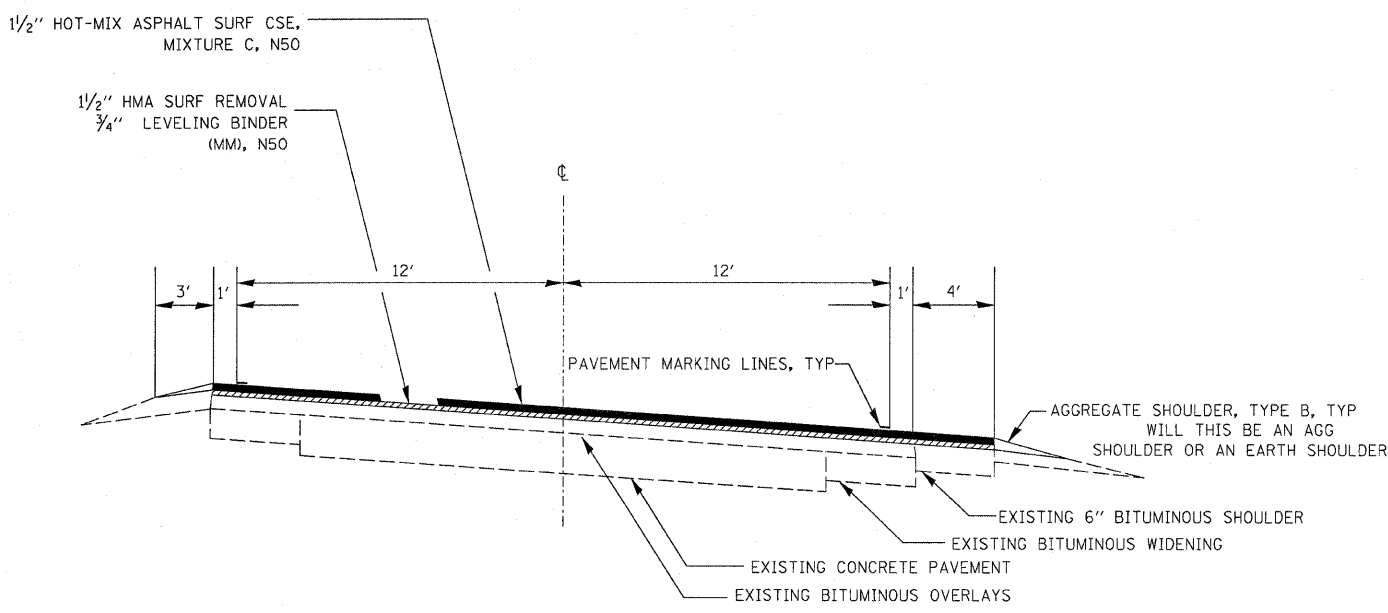


**TYPICAL SECTION**

SEE CENTERLINE RUMBLE STRIP DETAIL FOR USE IN THESE CURVES

STA 221+06 TO STA 231+71  
STA 353+04 TO STA 363+66

\*NO MILLING SOUTH OF CISSNA PARK

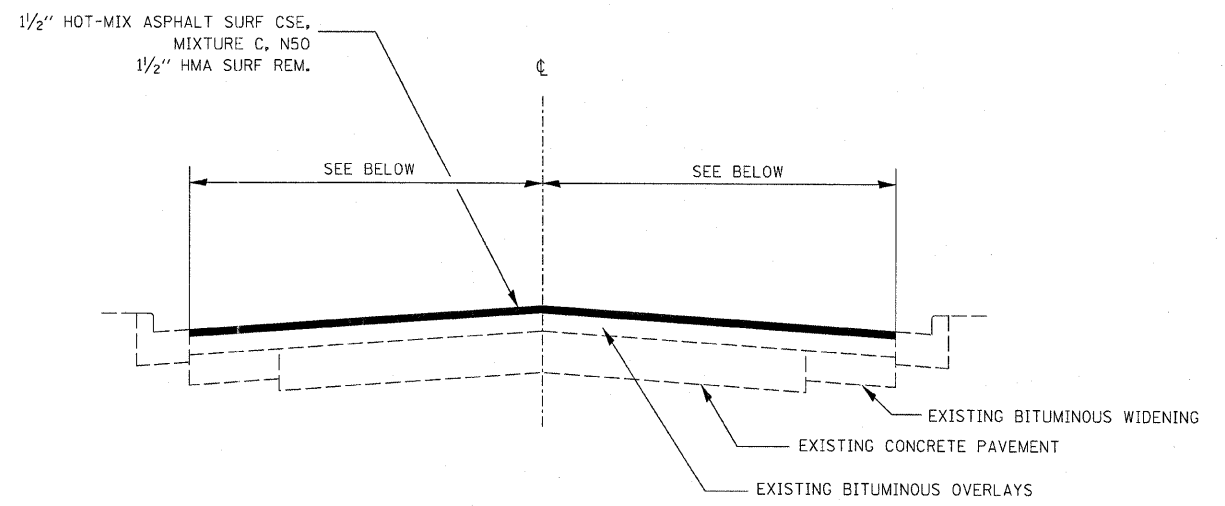


**TYPICAL SECTION**

STA 202+51 TO STA 213+13  
STA 326+87 TO STA 334+36

SEE CENTERLINE RUMBLE STRIP DETAIL FOR USE IN THESE CURVES

\*NO MILLING SOUTH OF CISSNA PARK



**TYPICAL SECTION**

STA 289+50 TO STA 295+00  
STA 302+69 TO STA 314+90

STA 283+38 TO STA 291+00: LEFT LANE = 13'; RIGHT LANE = 13'  
STA 291+00 TO STA 292+97: LEFT LANE = VAR.; RIGHT LANE = VAR.  
STA 292+97 TO STA 295+00: LEFT LANE = 22'; RIGHT LANE = 12.5'

STA 302+69 TO STA 314+32: LEFT LANE = 12.5'; RIGHT LANE = 12.5'  
STA 314+32 TO STA 314+90: LEFT LANE = VAR.; RIGHT LANE = 12.5'

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

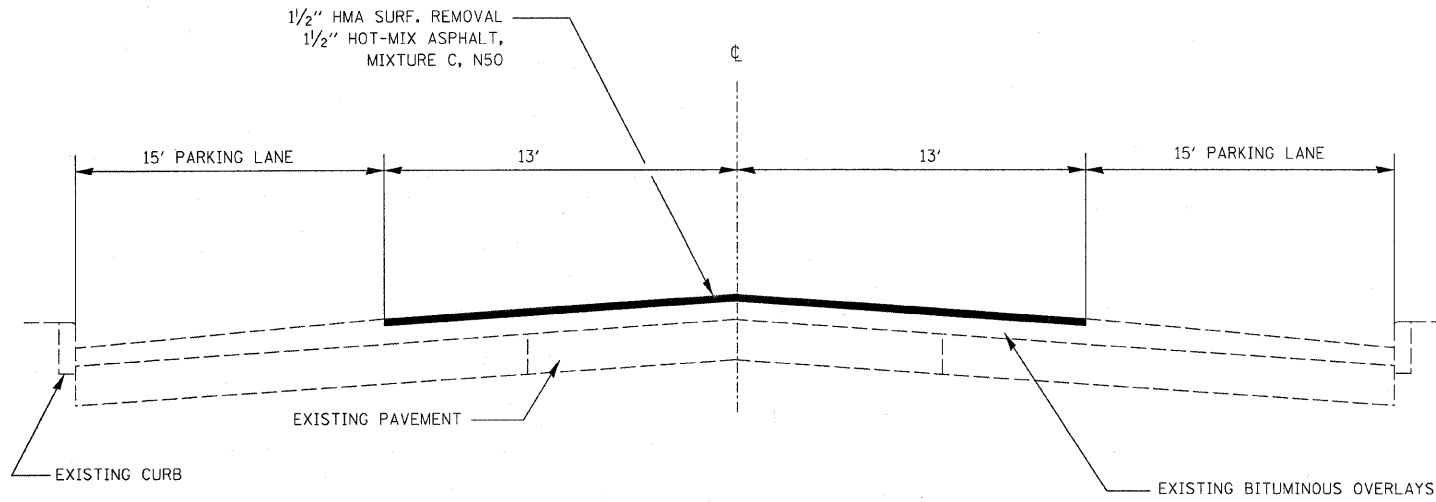
**TYPICAL SECTIONS**

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DATE \_\_\_\_\_

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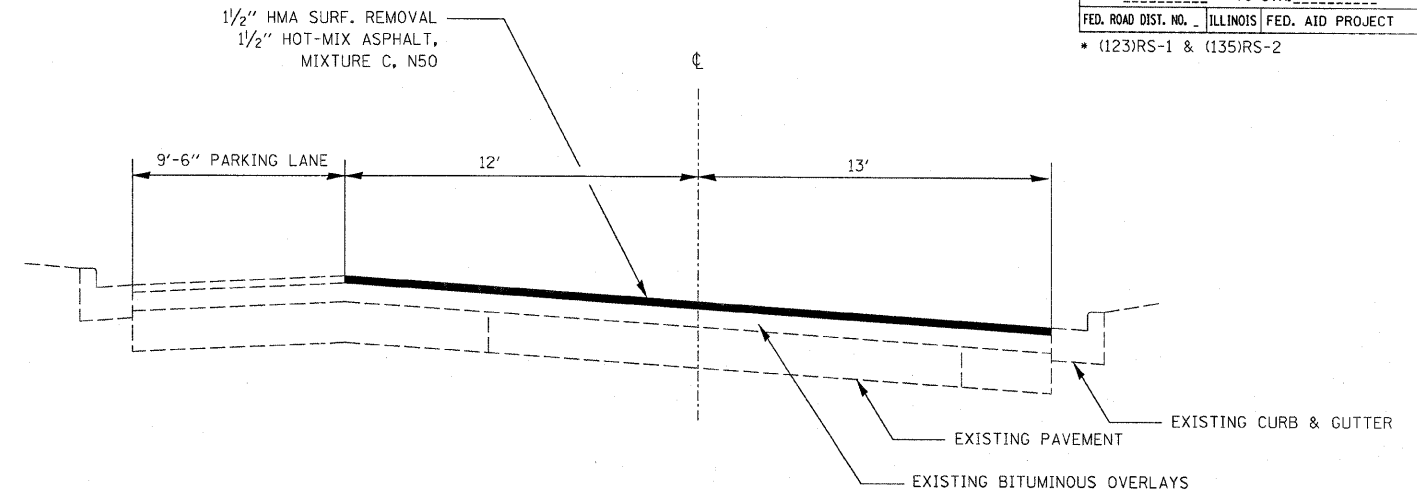
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	*	IBOQUOIS	16	6
STA. _____ TO STA. _____		ILLINOIS FED. AID PROJECT		
* (123)RS-1 & (135)RS-2				



**TYPICAL SECTION**

STA 295+00 TO STA 302+69

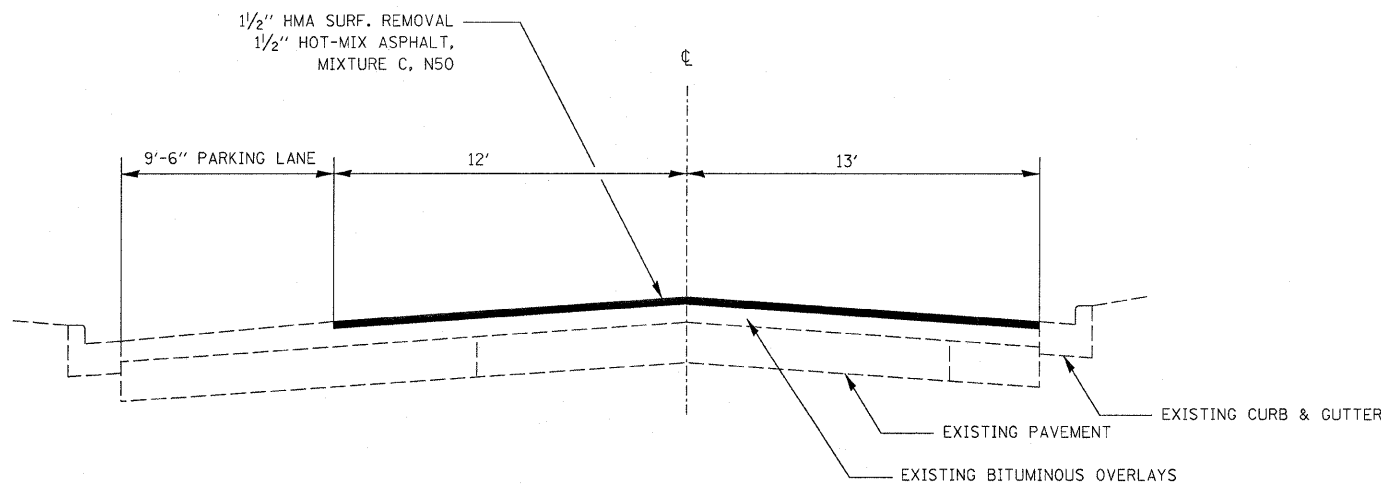
NOTE: NO WORK WILL BE PERFORMED IN THE PARKING LANES



**TYPICAL SECTION**

STA 325+07.69 TO STA 326+87

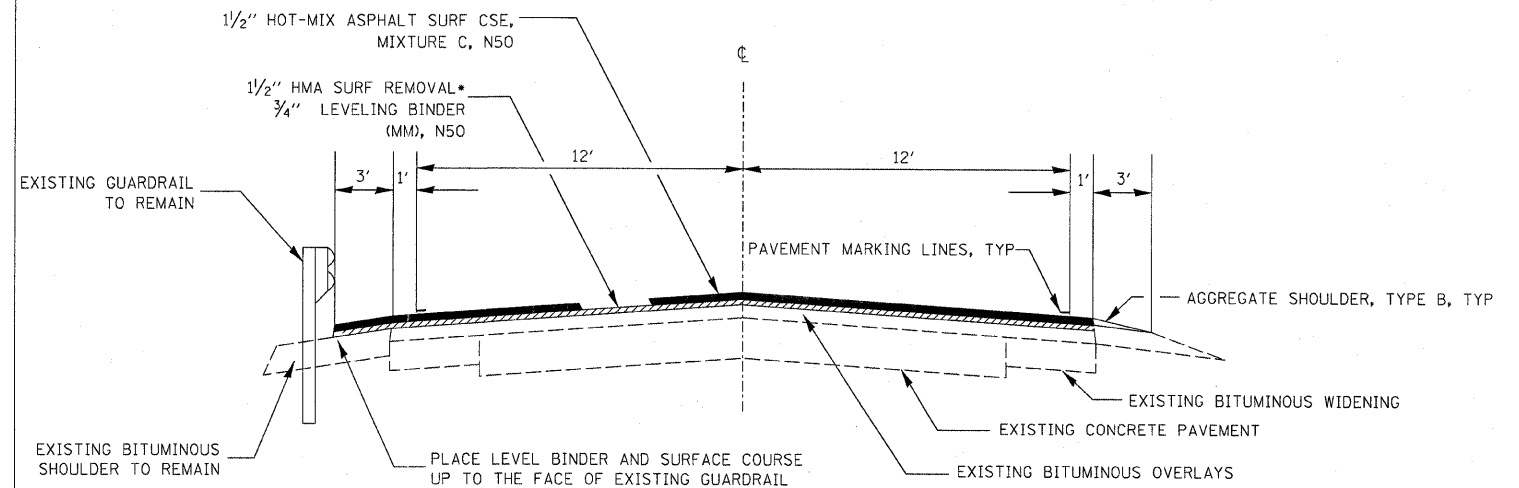
NOTE: NO WORK WILL BE PERFORMED IN THE PARKING LANES



**TYPICAL SECTION**

STA 314+90 TO STA 325+07.69

NOTE: NO WORK WILL BE PERFORMED IN THE PARKING LANES



**TYPICAL SECTION AT GUARDRAIL AND BITUMINOUS SHOULDER**

- STA 7+00 TO STA 8+50 LT
- STA 7+00 TO STA 8+60 RT
- STA 9+35 TO STA 11+00 LT
- STA 9+43 TO STA 10+83 RT
- STA 70+40 TO STA 74+40 LT
- STA 70+00 TO STA 74+00 RT
- STA 76+80 TO STA 79+68 LT
- STA 77+00 TO STA 79+66 RT
- STA 102+40 TO STA 104+30
- STA 101+50 TO STA 104+30
- STA 133+50 TO STA 135+45
- STA 133+50 TO STA 135+25
- STA 136+35 TO STA 138+00
- STA 136+05 TO STA 138+00
- STA 184+20 TO STA 188+05
- STA 183+35 TO STA 187+15
- STA 182+50 TO STA 283+38
- STA 282+50 TO STA 283+38
- STA 470+12 TO STA 550+00
- STA 554+15 TO STA 556+95
- STA 554+05 TO STA 557+50

\*NO MILLING SOUTH OF CISSNA PARK  
SEE SCHEDULE

NOTE: QUANTITY FOR SHOULDER RESURFACING IS INCLUDED IN THE PAVEMENT SCHEDULE.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**TYPICAL SECTIONS**

SCALE: VERT.  
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FAP. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	*	IRROQUOIS	16	7
STA.	TO STA.			
FED. ROAD DIST. NO. 5	ILLINOIS	FED. AID PROJECT		

\*(123)RS-1 & (135)RS-2

LOCATION	LENGTH	MAINLINE PAVEMENT					SHOULDERS				TOTAL					MIX CRACKS, JTS & FLGWYS	LB (HAND METHOD)
		BIT MATERIALS PRIME COAT	AGG PRIME COAT	LEVEL BINDER	HMA SURF CSE	AGG SHLD TYPE B	BIT MATERIALS PRIME COAT	AGG PRIME COAT	LEVEL BINDER	HMA SURF CSE	BIT MATERIALS PRIME COAT	AGG PRIME COAT	LEVEL BINDER	HMA SURF CSE			
		TON	TON	TON	TON	TON	TON	TON	TON	TON	TON	TON	TON	TON			
STA 0-38 TO STA 8+39	877	202.7	5.1	99.1	212.8	49.9	8.3	0.2	4.3	8.7	211.0	5.3	103.5	221.5	0.8	1.3	
STA 8+39 TO STA 9+54	115																
STA 9+54 TO STA 135+27	12573	2905.8	72.6	1518.2	3051.0	716.0	66.6	1.7	35.0	70.0	2972.4	74.3	1553.2	3121.0	10.9	18.2	
STA 135+27 TO STA 136+21	94																
STA 136+21 TO STA 202+51	6630	1532.3	38.3	800.8	1608.9	377.5	30.0	0.8	15.8	31.5	1562.3	39.1	816.6	1640.4	5.7	9.6	
STA 202+51 TO STA 213+13	1062	283.2	7.1	148.7	297.4	30.2					283.2	7.1	148.7	297.4	1.1	1.8	
STA 213+13 TO STA 221+06	793	183.3	4.6	96.2	192.4	45.2					183.3	4.6	96.2	192.4	0.7	1.1	
STA 221+06 TO STA 231+71	1065	284.0	7.1	149.1	298.2	30.3					284.0	7.1	149.1	298.2	1.1	1.8	
STA 231+71 TO STA 274+85.48 *	4314.48	997.1	24.9	523.5	1047.0	245.7					997.1	24.9	523.5	1047.0	3.7	6.2	
* STA 275+00 TO STA 277+10	210	48.5	1.2	25.5	51.0	12.0	4.7	0.1	2.5	4.9	53.2	1.3	27.9	55.9	0.2	0.3	
STA 289+50 TO STA 295+00	550	222.8	5.6	233.9	466.7	101.5					222.8	5.6	0.0	233.9	0.8	1.4	
STA 295+00 TO STA 302+33	550	127.1	3.2	133.5	260.6	64.7					127.1	3.2	0.0	133.5	0.5	0.8	
STA 302+33 TO STA 314+90	1257	290.5	7.3	305.0	609.5	132.2					290.5	7.3	0.0	305.0	1.1	1.8	
STA 314+90 TO STA 318+80	390	90.1	2.3	94.6	189.2	42.1					90.1	2.3	0.0	94.6	0.3	0.6	
STA 318+80 TO STA 325+08	628	145.1	3.6	152.4	304.5	67.5					145.1	3.6	0.0	152.4	0.5	0.9	
STA 325+08 TO STA 326+87	179	41.4	1.0	43.4	86.8	19.1					41.4	1.0	0.0	43.4	0.2	0.3	
STA 326+87 TO STA 334+36	749	199.7	5.0	100.7	209.7	21.3					199.7	5.0	100.7	209.7	0.7	1.2	
STA 334+36 TO STA 353+04	1868	431.7	10.8	226.7	453.3	106.4					431.7	10.8	226.7	453.3	1.6	2.7	
STA 353+04 TO STA 363+66	1062	283.2	7.1	148.7	297.4	30.2					283.2	7.1	148.7	297.4	1.1	1.8	
STA 363+66 TO STA 468+26	10460	2417.4	60.4	1265.5	2538.3	595.6					2417.4	60.4	1265.5	2538.3	9.1	15.1	
STA 468+26 TO STA 470+12	186																
STA 470+12 TO STA 567+75	9763	2256.3	56.4	1177.3	2369.2	555.9	229.7	5.7	120.6	241.2	2486.0	62.2	1297.9	2610.3	8.5	14.1	
TOTAL	55375.48	12942.3	323.6	6279.9	13589.4	2816.3	339.3	8.5	178.1	356.2	13281.6	332.0	6651.8	14364.0	48.5	80.9	

NOTE: QUANTITY FOR 4 FT BITUMINOUS SHOULDER AT SUPERELEVATION IS INCLUDED IN MAINLINE PAVEMENT QUANTITY.  
 NOTE: QUANTITY FOR 3 FT BITUMINOUS SHOULDER AT GUARDRAIL LOCATIONS IS INCLUDED IN SHOULDER PAVEMENT QUANTITY.  
 STA EQUATION: 274+85.48 BK = STA 275+00.00 AH

HMA SURFACE REMOVAL							
LOCATION	DESCRIPTION	WIDTH	LENGTH	HMA REMOVAL - BUTT JOINT	HMA REMOVAL 1-1/2"	HMA - VARIABLE DEPTH	TEMPORARY RAMP
		FT	FT	SQ YD	SQ YD	SQ YD	SQ YD
STA 0-38 TO STA 0+22	SOUTH JOB LIMIT	26	60	173.3			8.0
STA 7+79 TO STA 8+39	S.N. 038-0190 BUTT JOINT	34	60	226.7			8.0
STA 9+54 TO STA 10+14	S.N. 038-0190 BUTT JOINT	34	60	226.7			8.0
STA 134+67 TO STA 135+27	S.N. 038-0174 BUTT JOINT	34	60	226.7			8.0
STA 136+21 TO STA 136+81	S.N. 038-0174 BUTT JOINT	34	60	226.7			8.0
STA 276+50 TO STA 277+10	SN 050-0217 BUTT JOINT	34	60	226.7			8.0
STA 289+50 TO STA 301+90	C&G SECTION	26	1,240		3,582.2		8.0
STA 301+90 TO STA 302+44	RR CROSSING (BETWEEN TRACKS)	26	53.5	155.0			64.0
STA 302+44 TO STA 326+87	C&G SECTION	26	2,443.5		7,059.0		0.0
STA 326+87 TO STA 468+26	OUTSIDE C&G SECTION	26	60		41,651.0	173.3	0.0
STA 468+26 TO STA 470+12	S.N. 038-0198 OMISSION	34	60				8.0
STA 470+12 TO STA 567+75	NORTH SECTION OF PROJECT	34	60		28,204.0		8.0
TOTALS				1,461.7	80,496.2	173.3	136.0

CONCRETE REMOVAL	
LOCATION	CONCRETE REMOVAL
	CU YD
STA 358+18 LT	0.07

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**PAVEMENT & HMA SURF REMOVAL SCHEDULES**

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_

DATE \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

2007 SCHEDULES.DGN

FAP. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	*	IROQUOIS	16	8
STA.		TO STA.		
FED. ROAD DIST. NO. 6		ILLINOIS	FED. AID PROJECT	

\*(123)RS-1 & (135)RS-2

SIDEROAD SCHEDULE

LOCATION	DESCRIPTION	SURFACE TYPE	LENGTH	WIDTH	WIDTH AT	AVE	TOTAL	BUTT	HMA SURF	TEMP	BIT MAT'L	AGG	INCIDENTAL	HMA SURF	FURN.
				AT IL 49	BUTT JOINT			WIDTH	AREA		JOINT LENGTH	REM-BUTT JT	RAMP	PR CT	PR CT
			FT	FT	FT	J	SQ YD	FT	SQ YD	SQ YD	GAL	TON	TON	SQ YD	CU YD
STA 0+04 RT	COUNTY LINE ROAD		30	52	24	38	126.7	20	53.3	11.1	10.1	0.3	16.0		0.8
STA 0+06 LT	COUNTY LINE ROAD		30	50	24	37	123.3	20	53.3	11.1	9.9	0.2	15.5		0.8
STA 52+24 RT	T.R. 371		30	62	20	41	136.7	20	44.4	11.1	10.9	0.3	17.2		0.8
STA 52+29 LT	T.R. 371		30	45	24	34.5	115.0	20	53.3	11.1	9.2	0.2	14.5		0.8
STA 104+37 LT	CH 21	OIL & CHIP	30	60	24	42	140.0				11.2	0.3	17.6		0.8
STA 104+41 RT	T.R. 363	OIL & CHIP	30	50	24	37	123.3				9.9	0.2	15.5		0.8
STA 156+52 LT	CH 10	BIT CONC	30	105	26	65.5	218.3	20	57.8	11.1	17.5	0.4	27.5		0.8
STA 156+55 RT	T.R. 359	OIL & CHIP	30	55	20	37.5	125.0				10.0	0.3	15.8		0.8
STA 205+00 LT	T.R. 351 A		130	0	45	22.5	325.0	20	100.0	11.1	26.0	0.7	41.0		3.6
STA 210+50 LT	T.R. 351	OIL & CHIP	115	0	30	15	191.7				15.3	0.4	24.2		3.2
STA 224+00 RT	T.R. 353	OIL & CHIP	120	0	25	12.5	166.7				13.3	0.3	21.0		3.3
STA 229+00 RT	T.R. 353 C	OIL & CHIP	130	0	25	12.5	180.6				14.4	0.4	22.8		3.6
STA 278+03 RT	CH 10		30	150	45	97.5	325.0	20	100.0	11.1	26.0	0.7	41.0		0.8
STA 304+25 LT	CHURCH ST	OIL & CHIP	30	64	24	44	146.7	20			11.7	0.3	18.5	146.7	0.8
STA 308+19 LT	BLAINE ST	OIL & CHIP	30	55	20	37.5	125.0	20			10.0	0.3	15.8	125.0	0.8
STA 326+50 LT	SIDEROAD LT	OIL & CHIP	196	0	24	12	261.3				20.9	0.5	32.9		5.4
STA 332+00 LT	CH 44	OIL & CHIP	135	0	24	12	180.0				14.4	0.4	22.7		3.8
STA 355+50 RT	T.R. 339	OIL & CHIP	110	0	24	12	146.7				11.7	0.3	18.5		3.1
STA 361+30 RT	T.R. 146	BIT CONC	75	0	16	8	66.7	20	35.6	11.1	5.3	0.1	8.4		2.1
STA 409+77 RT	T.R. 331	OIL & CHIP	30	60	16	38	126.7				10.1	0.3	16.0		0.8
STA 409+85 LT	T.R. 331	OIL & CHIP	30	55	16	35.5	118.3				9.5	0.2	14.9		0.8
STA 447+97 RT	T.R. 319 A	AGGREGATE	30	40	20	30	100.0				8.0	0.2	12.6		0.8
STA 449+35 LT	T.R. 319	OIL & CHIP	30	85	18	51.5	171.7				13.7	0.3	21.6		0.8
STA 454+22 LT	T.R. 568	OIL & CHIP	30	60	20	40	133.3				10.7	0.3	16.8		0.8
STA 461+01 LT	T.R. 588	OIL & CHIP	30	50	20	35	116.7				9.3	0.2	14.7		0.8
STA 463+06 LT	T.R. 593	OIL & CHIP	30	50	20	35	116.7				9.3	0.2	14.7		0.8
STA 464+93 LT	T.R. 568	OIL & CHIP	30	55	20	37.5	125.0				10.0	0.3	15.8		0.8
STA 494+59 RT	T.R. 305	OIL & CHIP	30	60	20	40	133.3				10.7	0.3	16.8		0.8
STA 501+21 LT	T.R. 299	OIL & CHIP	30	58	20	39	130.0				10.4	0.3	16.4		0.8
STA 567+05 RT	CH 9	BIT CONC	30	105	30	67.5	225.0	20	66.7	11.1	18.0	0.5	28.4		0.8
STA 567+06 LT	CH 9	BIT CONC	30	90	30	60	200.0	20	66.7	11.1	16.0	0.4	25.2		0.8
STA 358+18 LT	CULVERT	N/A													5
TOTAL							4920.2		631.1	111.1	393.6	9.8	681.9	271.7	52.25

NOTE: FURNISHED EXCAVATION SHALL BE PLACED AT SIDE ROADS AS SHOWN IN THE DETAILS. SEED AND FERTILIZER APPLICATION IS INCLUDED IN THE COST OF THIS ITEM.  
 NOTE: FURNISHED EXCAVATION SHALL BE PLACED AT THE CULVERT END SECTION AS DIRECTED BY THE ENGINEER. SEED AND FERTILIZER APPLICATION IS INCLUDED IN THE COST OF THIS ITEM.

CENTERLINE RUMBLE STRIP SCHEDULE	
LOCATIONS	LENGTH
202+80 to 212+80	1000
221+40 to 231+40	1000
324+10 TO 334+00	1000
353+40 TO 363+30	1000
TOTAL	4000

NOTE: LENGTHS ARE APPROXIMATE. CENTERLINE RUMBLE STRIPS SHALL EXTEND 100 FEET BEYOND THE P.C. AND P.T. OF EACH CURVE.  
 CURVE 1 PC STA 203+84.53 PT STA 211+79.74  
 CURVE 2 PC STA 222+39.02 PT STA 230+37.34  
 CURVE 3 PC STA 325+07.69 PT STA 333+03.16  
 CURVE 4 PC STA 354+37.54 PT STA 362+32.92

SEE LOCATION MAP FOR LOCATIONS OF CENTERLINE RUMBLE STRIPS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**MISCELLANEOUS SCHEDULES**

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

2001/01/20 SCHEDULES.DGN



FAP. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
640	*	IROQUOIS	16	9
STA.	TO STA.			
FED. ROAD DIST. NO. 5	ILLINOIS	FED. AID PROJECT		

\*(123)RS-1 & (135)RS-2

ENTRANCE SCHEDULE

LOCATION	DESCRIPTION	SURF TYPE	LENGTH	WIDTH AT IL 49	WIDTH AT BUTT JOINT	AREA	LENGTH AT BUTT JT	HMA SURF REM-BUTT JT	BIT MATL'S PR CT	AGG PR CT	INCIDENTAL HMA SURFACING
			FT	FT		SQ YD	FT	SQ YD	GAL	TON	TON
STA 11+93 LT	F.E.	BIT CONC	10	20	20	22.2			1.8	0.04	2.8
STA 79+83 LT	P.E.	BIT CONC	10	35	15	27.8	4.0	6.7	2.2	0.06	3.5
STA 107+04 RT	C.E.	BIT CONC	10	40	20	33.3	4.0	8.9	2.7	0.07	4.2
STA 157+83 LT	P.E.	BIT CONC	10	40	12	28.9	4.0	5.3	2.3	0.06	3.64
STA 220+28 RT	P.E.	BIT CONC	10	35	25	33.3	4.0	11.1	2.7	0.07	4.2
STA 222+93 RT	P.E.	BIT CONC	10	40	15	30.6	4.0	6.7	2.4	0.06	3.85
STA 237+99 RT	C.E.	BIT CONC	10	50	30	44.4	4.0	13.3	3.6	0.09	5.6
STA 240+60 LT	C.E.	BIT CONC	10	50	25	41.7	4.0	11.1	3.3	0.08	5.25
STA 243+19 LT	C.E.	BIT CONC	10	50	40	50.0	4.0	17.8	4.0	0.10	6.3
STA 243+91 LT	P.E.	BIT CONC	10	35	20	30.6	4.0	8.9	2.4	0.06	3.85
STA 244+10 RT	P.E.	BIT CONC	10	30	15	25.0	4.0	6.7	2.0	0.05	3.15
STA 244+90 LT	P.E.	BIT CONC	10	45	10	30.6	4.0	4.4	2.4	0.06	3.85
STA 244+99 RT	C.E.	BIT CONC	10	40	20	33.3	4.0	8.9	2.7	0.07	4.2
STA 245+80 RT	C.E.	BIT CONC	10	65	20	47.2	4.0	8.9	3.8	0.09	5.95
STA 253+68 RT	C.E.	BIT CONC	10	90	35	69.4	4.0	15.6	5.6	0.14	8.75
STA 262+15 LT	C.E.	BIT CONC	10	35	25	33.3	4.0	11.1	2.7	0.07	4.2
STA 262+95 LT	P.E.	BIT CONC	10	25	10	19.4	4.0	4.4	1.6	0.04	2.45
STA 265+52 RT	MAILBOX TURNOUT	BIT CONC	4	10	10	4.4			0.4	0.01	0.56
STA 265+77 RT	P.E.	BIT CONC	10	25	10	19.4	4.0	4.4	1.6	0.04	2.45
STA 268+64 RT	C.E.	BIT CONC	10	30	20	27.8	4.0	8.9	2.2	0.06	3.5
STA 273+88 RT	C.E.	BIT CONC	10	35	20	30.6	4.0	8.9	2.4	0.06	3.85
STA 274+67 LT	C.E.	BIT CONC	10	35	20	30.6	4.0	8.9	2.4	0.06	3.85
STA 274+79 RT	C.E.	BIT CONC	10	35	20	30.6	4.0	8.9	2.4	0.06	3.85
STA 275+66 LT	C.E.	BIT CONC	10	45	25	38.9	4.0	11.1	3.1	0.08	4.9
STA 275+97 RT	C.E.	BIT CONC	10	45	20	36.1	4.0	8.9	2.9	0.07	4.55
STA 276+60 LT	C.E.	BIT CONC	10	40	20	33.3	4.0	8.9	2.7	0.07	4.2
STA 277+19 LT	C.E.	BIT CONC	10	50	30	44.4	4.0	13.3	3.6	0.09	5.6
STA 278+55 LT	C.E.	BIT CONC	10	55	30	47.2	4.0	13.3	3.8	0.09	5.95
STA 279+94 LT	C.E.	BIT CONC	10	40	25	36.1	4.0	11.1	2.9	0.07	4.55
STA 281+54 RT	C.E.	BIT CONC	10	50	25	41.7	4.0	11.1	3.3	0.08	5.25
STA 282+79 RT	P.E.	BIT CONC	10	40	20	33.3	4.0	8.9	2.7	0.07	4.2
STA 347+06 RT	C.E.	BIT CONC	10	55	30	47.2	4.0	13.3	3.8	0.09	5.95
STA 348+74 RT	C.E.	BIT CONC	10	50	35	47.2	4.0	15.6	3.8	0.09	5.95
STA 349+96 RT	P.E.	BIT CONC	10	30	20	27.8	4.0	8.9	2.2	0.06	3.5
STA 350+21 RT	MAILBOX TURNOUT	BIT CONC	4	15	10	5.6			0.4	0.01	0.7
STA 392+42 RT	P.E.	BIT CONC	10	20	15	19.4	4.0	6.7	1.6	0.04	2.45
STA 392+60 RT	MAILBOX TURNOUT	BIT CONC	4	25	15	8.9			0.7	0.02	1.12
STA 410+98 RT	MAILBOX TURNOUT	BIT CONC	4	40	30	15.6			1.2	0.03	1.96
STA 411+01 LT	P.E.	BIT CONC	10	25	10	19.4	4.0	4.4	1.6	0.04	2.45
STA 415+84 RT	MAILBOX TURNOUT	BIT CONC	4	40	15	12.2			1.0	0.02	1.54
STA 416+19 LT	P.E.	BIT CONC	10	25	10	19.4	4.0	4.4	1.6	0.04	2.45
STA 460+88 RT	P.E.	BIT CONC	10	45	20	36.1	4.0	8.9	2.9	0.07	4.55
STA 474+90 LT	P.E.	BIT CONC	10	50	15	36.1	4.0	6.7	2.9	0.07	4.55
STA 474+90 RT	P.E.	BIT CONC	10	25	15	22.2	4.0	6.7	1.8	0.04	2.8
STA 477+52 RT	MAILBOX TURNOUT	BIT CONC	4	30	20	11.1			0.9	0.02	1.4
STA 477+55 LT	P.E.	BIT CONC	10	30	10	22.2	4.0	4.4	1.8	0.04	2.8
STA 557+12 LT	P.E.	BIT CONC	10	25	10	19.4	4.0	4.4	1.6	0.04	2.45
STA 557+34 RT	MAILBOX TURNOUT	BIT CONC	4	35	20	12.2			1.0	0.02	1.54
STA 561+47 RT	MAILBOX TURNOUT	BIT CONC	4	10	10	4.4			0.4	0.01	0.56
STA 561+60 LT	P.E.	BIT CONC	10	25	15	22.2	4.0	6.7	1.8	0.04	2.8
STA 565+44 RT	P.E.	BIT CONC	10	10	10	11.1	4.0	4.4	0.9	0.02	1.4
STA 565+50 LT	C.E.	BIT CONC	10	55	45	55.6	4.0	20.0	4.4	0.11	7
GRAND TOTAL			2/14/2008 9:41					392.0	122.5	3.1	192.9

200 / SCHEDULES.DGN

ILLINOIS DEPARTMENT OF TRANSPORTATION

**ENTRANCE SCHEDULE**

REVISIONS	
NAME	DATE

SCALE: VERT.  
HORIZ.  
DATE

DRAWN BY  
CHECKED BY

PAVEMENT MARKING SCHEDULE

LOCATION	LENGTH FT	PAINT PVT MK LINE 4"	PAINT PVT MK LINE 4"	PAINT PVT MK LINE 6"	THERMO PVT MK, LINE 4"		THERMO PVT MK LINE 6"	THERMO PVT MK LINE 24" PVMT MK -	THERMO PVT MK LET & SYMB.	TEMP PVT MK, LINE 4"		TEMP PVT MK LINE 6"	TEMP PVT MK LINE 24"	TEMP PVT MK LET & SYMB.	SHORT TERM PVT MK	WK ZN PVT MK REMOVAL	RAISED REFLECTIVE PVT MARKERS YELLOW EACH
		WHITE FT	YELLOW FT	YELLOW FT	WHITE FT	YELLOW FT	YELLOW FT	WHITE FT	WHITE SQ FT	WHITE FT	YELLOW FT	YELLOW FT	WHITE FT	WHITE FT	FT	SQ FT	
STA 0-38 TO STA 52+25	5,263																
EDGE LINES		10,526								10,526					1,579	175	66
CENTERLINE				1,276							1,276						
NO PASSING ZONE																	
STA 0-38 TO STA 1+23 NB	161		161								161						
STA 0-38 TO STA 10+95 SB	1,133		1,133								1,133						
STA 52+25 TO STA 104+39	5,214														1,564	174	65
EDGE LINES		10,428								10,428							
CENTERLINE				1,304							1,304						
STA 104+39 TO STA 156+48	5,209														1,563	174	65
EDGE LINES		10,418								10,418							
CENTERLINE				1,302							1,302						
STA 156+48 TO STA 199+00	4,252														1,276	142	53
EDGE LINES		8,504								8,504							
CENTERLINE				1,063							1,063						
STA 199+00 TO STA 235+00	3,600														1,080	120	76
EDGE LINES		7,200								7,200							
CENTERLINE				700							700						
NO PASSING ZONE																	
STA 199+00 TO STA 210+00 NB	1,100		1,100								1,100						
STA 206+00 TO STA 217+00 SB	1,100		1,100								1,100						
STA 217+00 TO STA 228+00 NB	1,100		1,100								1,100						
STA 224+00 TO STA 235+00 SB	1,100		1,100								1,100						
STA 235+00 TO STA 277+10	4,210														1,263	140	62
STA EQN 274+85,48 BK = 275+00 AH																	
EDGE LINES		8,420								8,420							
CENTERLINE				1,053							1,053						
STA 289+50 TO STA 325+50	3,600														1,080	120	51
CENTERLINE							900				900						
NO PASSING ZONE																	
STA 297+83 TO STA 302+36 NB	453					453					453						
STA 302+00 TO STA 305+72 SB	372					372					372						
STA 323+00 TO STA 325+50	250					250					250						
RAILROAD CROSSING NB								39	61.2				39	61			
RAILROAD CROSSING SB								39	61.2				39	61			
STA 325+50 TO STA 326+87	137					137					137				41	5	3
EDGE LINES																	
CENTERLINE							34				34						
NO PASSING ZONE																	
STA 325+00 TO STA 326+87	137					137					137						
STA 326+87 TO STA 368+00	4,113														1,234	137	74
EDGE LINES		8,226								8,226							
CENTERLINE				828							828						
NO PASSING ZONE																	
STA 326+87 TO STA 331+00 NB	413		413								413						
STA 328+00 TO STA 338+00 SB	1,000		1,000								1,000						
STA 349+00 TO STA 361+00 NB	1,200		1,200								1,200						
STA 356+00 TO STA 368+00 SB	1,200		1,200								1,200						
STA 368+00 TO STA 409+81	4,181														1,254	139	52
EDGE LINES		8,362								8,362							
CENTERLINE				1,045							1,045						
STA 409+81 TO STA 449+35	3,954														1,186	132	49
EDGE LINES		7,908								7,908							
CENTERLINE				989							989						
STA 449+35 TO STA 501+21	5,186														1,556	173	65
EDGE LINES		10,372								10,372							
CENTERLINE				1,297							1,297						
STA 501+21 TO STA 567+75	6,654														1,996	222	83
EDGE LINES		13,308								13,308							
CENTERLINE				1,664							1,664						
TOTAL		103,672	9,507	12,519	137	1,212	934	78	122	103,809	10,719	13,453	78	122	16,672	1,852	766
2/14/2008 9:24			113,179			1,349					114,528						

NOTE: THE ENGINEER SHALL VERIFY THE LOCATION OF THE NO PASSING ZONES PRIOR TO CONSTRUCTION.

F&P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
840	*	IROQUOIS	16	10
STA.	TO STA.			
FED. ROAD DIST. NO. 5	ILLINOIS	FED. AID PROJECT		

\*(123)RS-1 & (135)RS-2

200 / SCHEDULES.DGN

ILLINOIS DEPARTMENT OF TRANSPORTATION

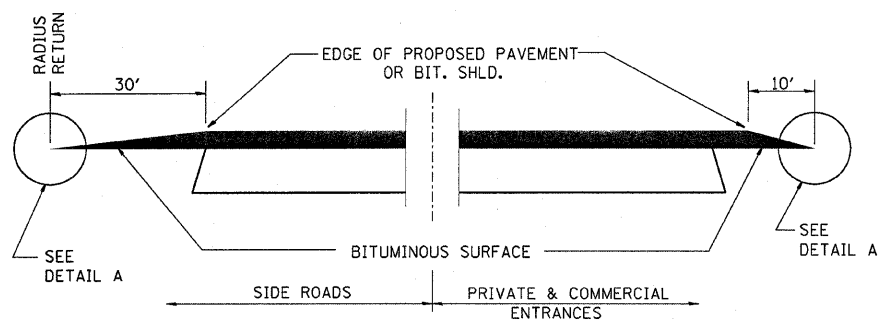
**PAVEMENT MARKING SCHEDULE**

SCALE: VERT.  
HORIZ.  
DATE

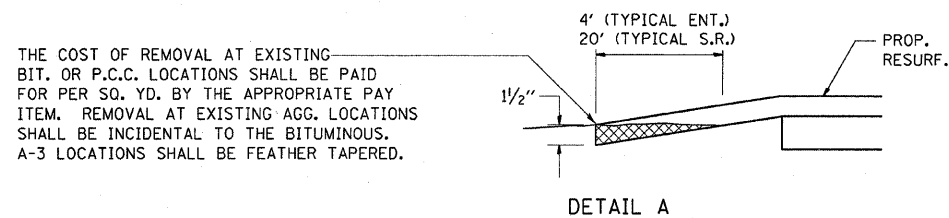
DRAWN BY  
CHECKED BY

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840		TROQUOIS	16	11
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

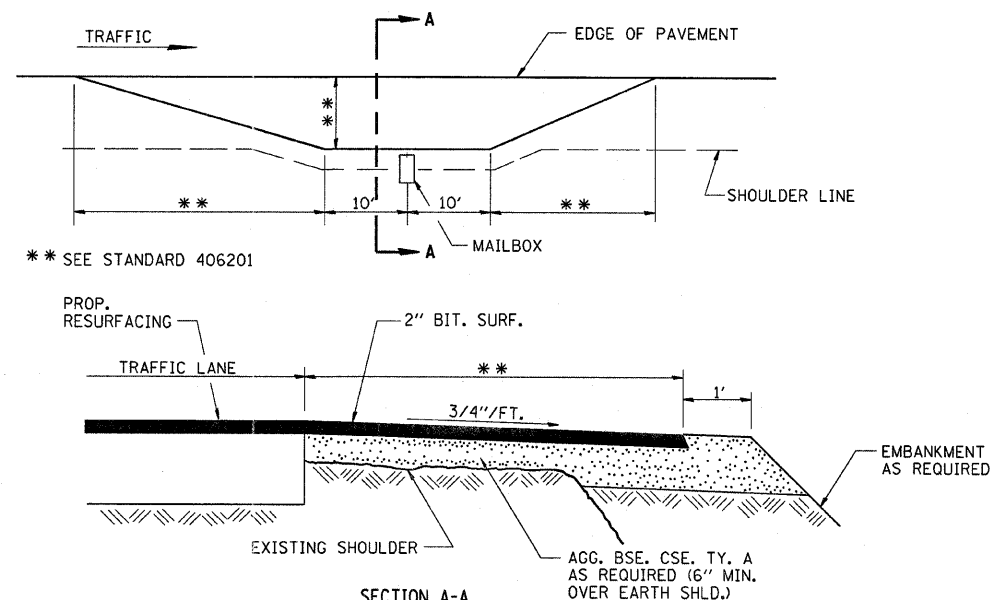
\*(123)RS-1 & (135)RS-2



SECTION A-A  
DETAILS AT ENTRANCES & SIDE ROADS

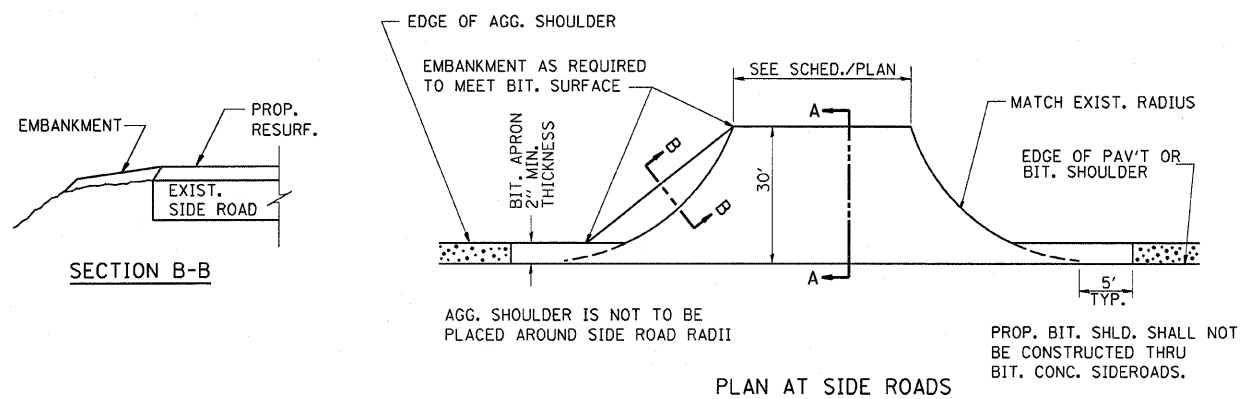


THE COST OF REMOVAL AT EXISTING BIT. OR P.C.C. LOCATIONS SHALL BE PAID FOR PER SQ. YD. BY THE APPROPRIATE PAY ITEM. REMOVAL AT EXISTING AGG. LOCATIONS SHALL BE INCIDENTAL TO THE BITUMINOUS. A-3 LOCATIONS SHALL BE FEATHER TAPERED.

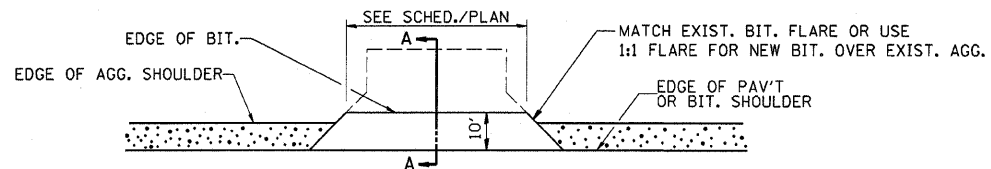


SECTION A-A  
MAILBOX TURNOUT DETAILS

406-1



PLAN AT SIDE ROADS



PLAN AT PRIVATE & COMMERCIAL ENTRANCES  
(DO NOT RESURFACE FIELD ENTRANCES)

406-2

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**ENTRANCE DETAILS**

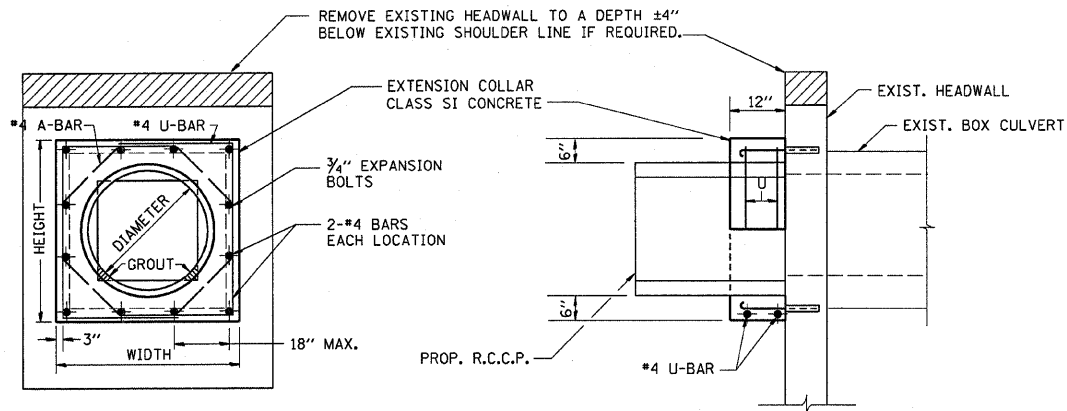
SCALE: VERT. DATE  
HORIZ.

DRAWN BY  
CHECKED BY

-DAT- 10:37:14 AM c:\pw\work\pw\1dof\br aboypc\dms84866\detail1s.dgn

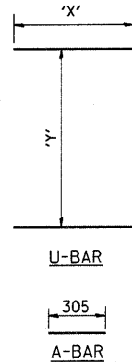
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840		IBOQUOIS	16	12
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

\*(123)RS-1 & (135)RS-2



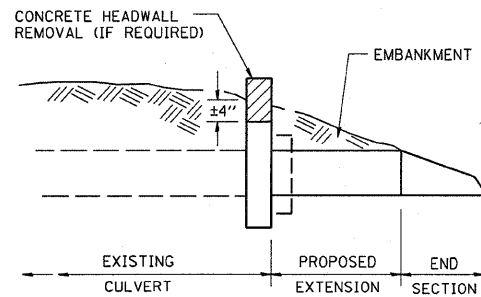
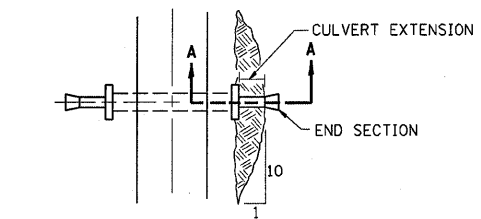
EXPANSION BOLTS SHALL CONSIST OF SELF DRILLING EXPANSION SHIELDS AND 3/4" DIA. HOOKED BOLTS. HOOKED BOLTS SHALL EXTEND A MINIMUM OF 9" INTO NEW CONCRETE. BOLTS SHALL BE DRILLED IN THE CENTER OF THE EXISTING BOX CULVERT BARREL WALLS. MINIMUM CERTIFIED PROOF LOAD = 4,080 LBS.

LOCATION	EXISTING CULVERT SIZE FT. x FT.	PIPE DIMENSION DIA. IN.	PIPE AREA SQ. FT.	EXTENSION COLLAR		A-BAR		U-BAR		CLASS SI CONC. COLLAR CU. YD.	REINFORCEMENT BARS POUND	3/4" DIA. EXPANSION BOLTS EACH
				WIDTH IN.	HEIGHT IN.	305 IN.	'X' IN.	'Y' IN.	305 IN.			
358+18	3 X 2	6.0	56	44	33	38	0.27	23	14			



COLLAR DETAIL (R.C.C.P. EXTENSION OF BOX CULVERT)

540-9

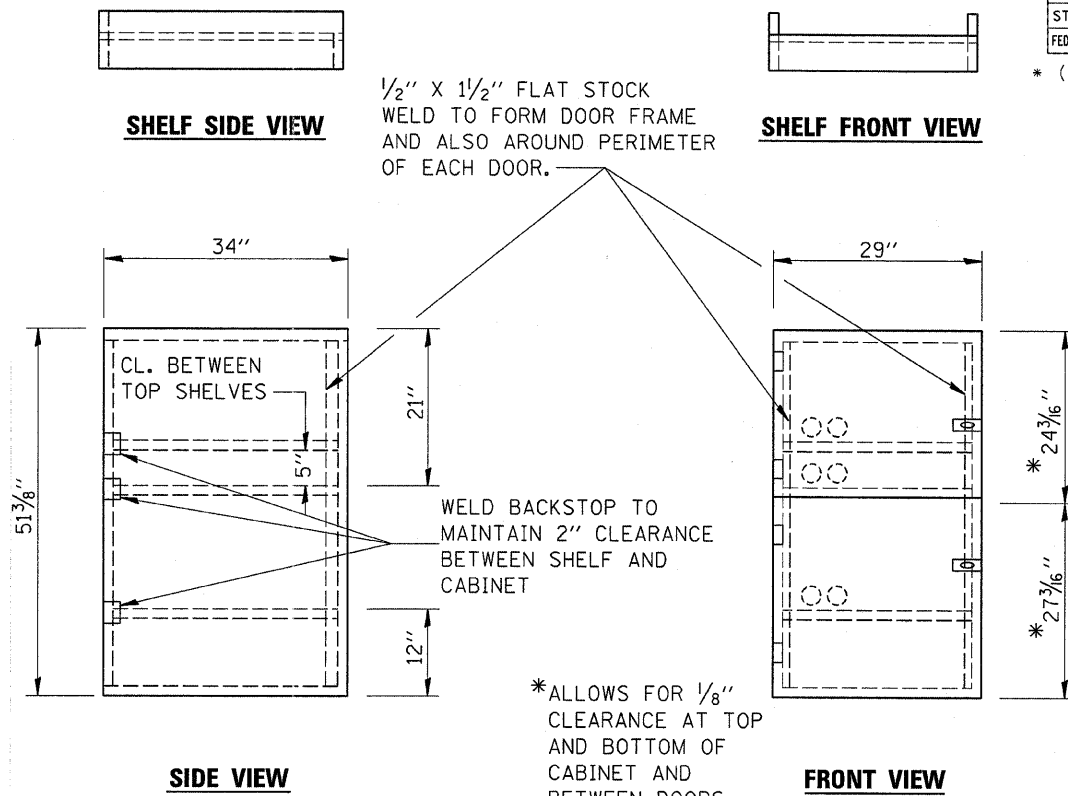


SECTION A-A

PLAN AT CULVERT EXTENSIONS

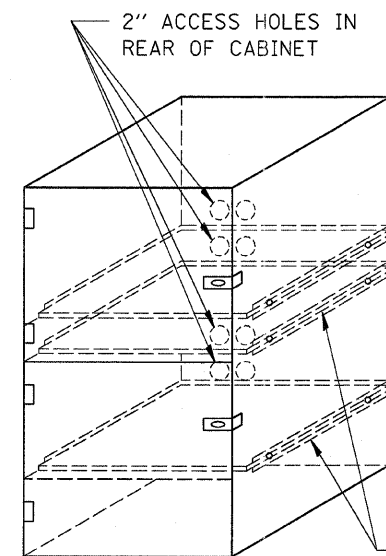
540-13

CONTRACT NO. 66425



SIDE VIEW

FRONT VIEW



LOCKABLE COMPUTER CABINET

FLAT STOCK DIMENSIONS VARY DEPENDING ON TYPE OF ROLLER ASSEMBLY.

- NOTES:
1. USE 16 GAUGE STEEL FOR CABINET.
  2. THE TOP SHELF SHALL SLIDE IN OR OUT WITH THE TOP DOOR OPEN.
  3. ALL HINGES AND HASPS WILL BE WELDED TO THE CABINET.
  4. ALL EDGES SHALL BE GROUND SMOOTH.
  5. TWO (2" DIA.) ACCESS HOLES WILL BE REQUIRED FOR EACH SHELF.
  6. CABINET SHALL BE PAINTED WITH TWO COATS OF FLAT PAINT.
  7. 2 EACH MATCHING KEY PADLOCKS, WITH 3 KEYS PROVIDED, MASTER MODEL 3 T OR EQUIVALENT.
  8. 4 EACH PLAIN STEEL, NON-REMOVABLE PIN, NO HOLE 4"X4" SQUARE CORNER HINGES TO BE WELDED ON.
  9. 2 EACH EXTRA HEAVY, PLAIN STEEL, FIXED STAPLE, NO HOLE, 7 1/4" HASPS TO BE WELDED ON.

670-1

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

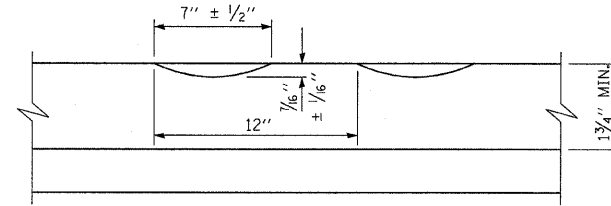
MISCELLANEOUS DETAILS

SCALE: VERT. HORIZ. DATE

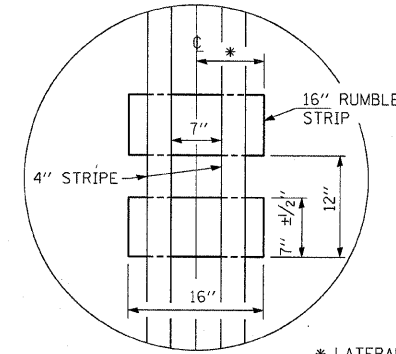
DRAWN BY CHECKED BY

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840		IBOQUOIS	16	13
STA.	TO STA.			
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

\* (123)RS-1 & (135)RS-2

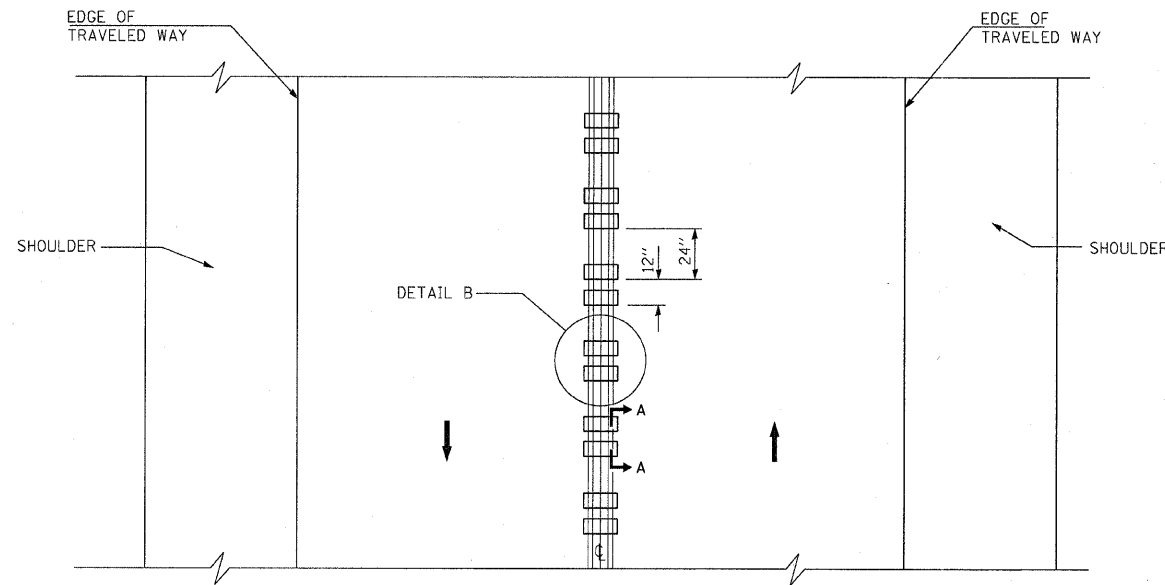


SECTION A-A



DETAIL B

\* LATERAL DEVIATION SHALL NOT EXCEED ONE INCH IN 100 FEET.

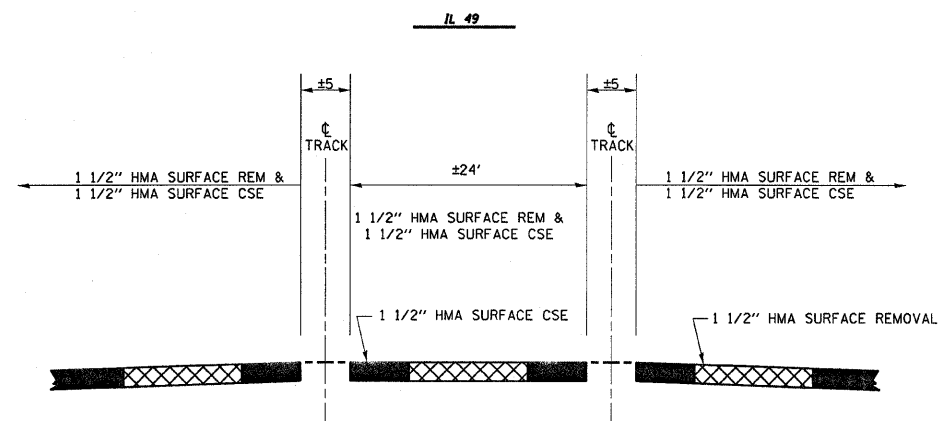


TWO-WAY ROAD

GENERAL NOTES

- SEE STANDARD 780001 FOR STRIPING.
- RUMBLE STRIPS SHALL NOT BE PLACED ON BRIDGES.
- ALL RUMBLE STRIPS SHALL BE MILLED.
- CENTERLINE RUMBLE STRIPS SHALL BE CONTINUOUS THROUGH CONNECTIONS OF SIDEROADS WITH NO LEFT TURN LANES.
- DISCONTINUE CENTERLINE RUMBLE STRIPS THROUGH THE LIMITS OF ALL LEFT TURN LANES, INCLUDING ANY LANE TAPER SECTIONS.
- WHERE USED, ADJUST SPACING OF RAISED REFLECTIVE PAVEMENT MARKERS TO FALL IN WIDER GAP BETWEEN RUMBLE STRIPS.
- CENTERLINE RUMBLE STRIPS SHALL BE PLACED AS SHOWN ON THE SCHEDULE IN THE PLANS

CENTERLINE RUMBLE STRIPS



MILLING DETAIL AT RAILROAD CROSSING

REVISIONS	
NAME	DATE

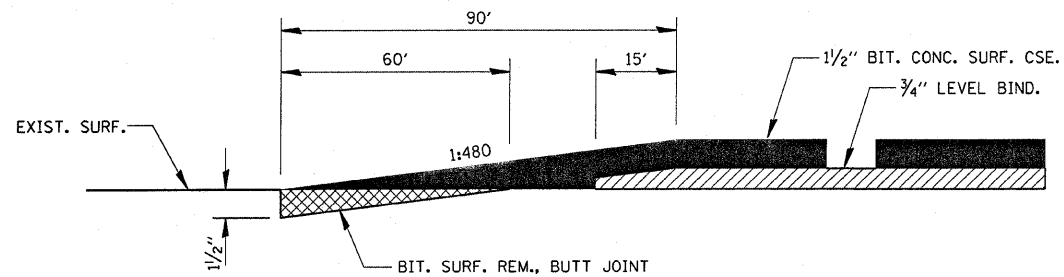
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CENTERLINE RUMBLE STRIP AND RR MILLING DETAILS**

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_

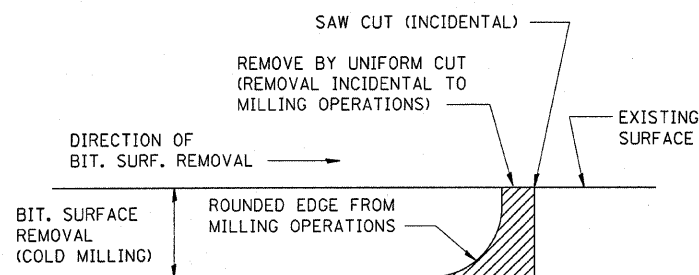
DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	*	IROQUOIS	16	14
STA. _____		TO STA. _____		
FED. ROAD DIST. NO. _____		ILLINOIS FED. AID PROJECT		

\* (123)RS-1 & (135)RS-2



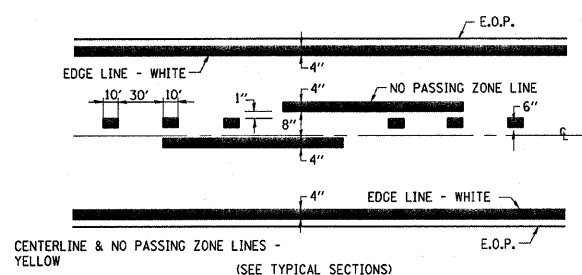
406-9



NOTE: WHEN MILLING OPERATIONS PRODUCE A ROUNDED EDGE, THEN A SAW CUT SHALL BE USED TO MANUFACTURE A PERPENDICULAR EDGE AS SHOWN IN THE DETAIL. THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING THE USE OF THIS DETAIL

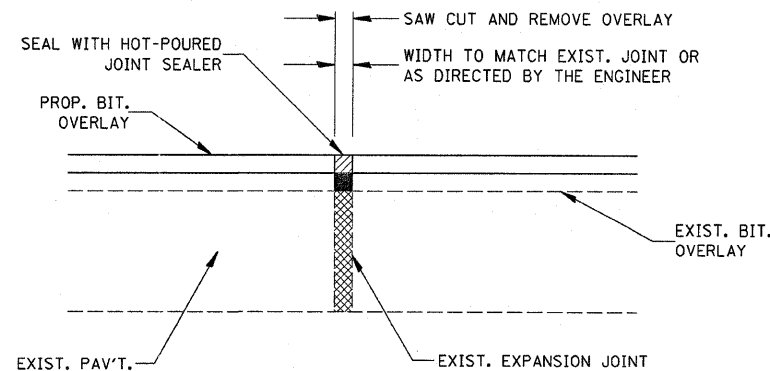
**BITUMINOUS DETAIL AT BUTT JOINTS**

406-8



**PAVEMENT MARKING**

780-8

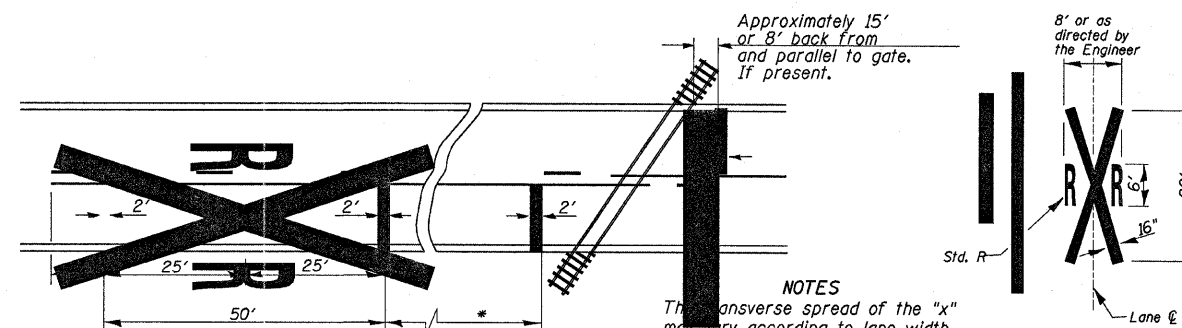


**EXPANSION JOINT REHABILITATION DETAIL**

**GENERAL NOTES:**

1. THE NEW BITUMINOUS OVERLAY SHALL BE SAWED, REMOVED, AND THE JOINT AREA CLEANED. PRIOR TO PLACING THE HOT-POURED JOINT SEALER THE JOINT SHALL BE BLOWN OUT WITH COMPRESSED AIR.
2. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR EXPANSION JOINT REHABILITATION.

406-7



NOTES  
The transverse spread of the "x" may vary according to lane width.  
On multi-lane roads, the stop lines shall extend across all approach lanes and separate RXR symbols shall be placed adjacent to each other in each lane.

**PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING**

- \* Minimum Distance  
400' for 55 MPH  
250' for 45 MPH  
100' for 35 MPH or Less

780-15

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

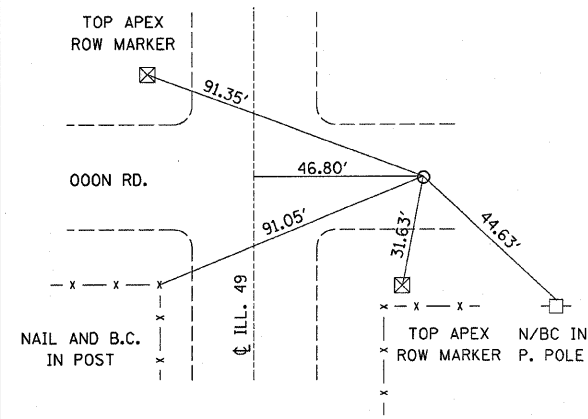
**MISCELLANEOUS DETAILS**

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
CHECKED BY \_\_\_\_\_

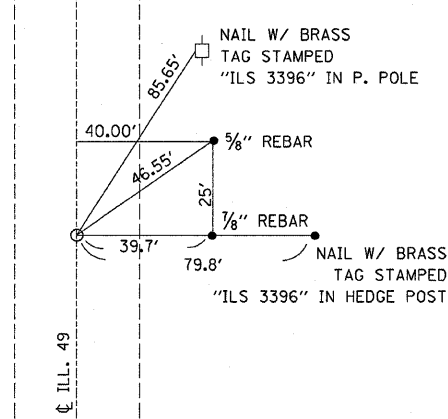
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	:	IRROQUOIS	16	15
STA.		TO STA.		
FED. ROAD DIST. NO. 5		ILLINOIS FED. AID PROJECT		

\*(123)RS-1 & (135)RS-2



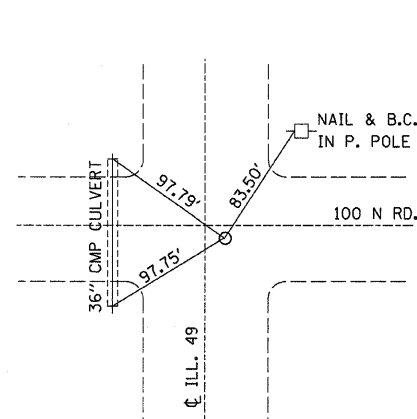
FOUND MAG NAIL AND WASHER  
±46.80' E. OF C ILL. 49

R.E. TO REPLACE NAIL  
IN KIND AT JOB COMPLETION



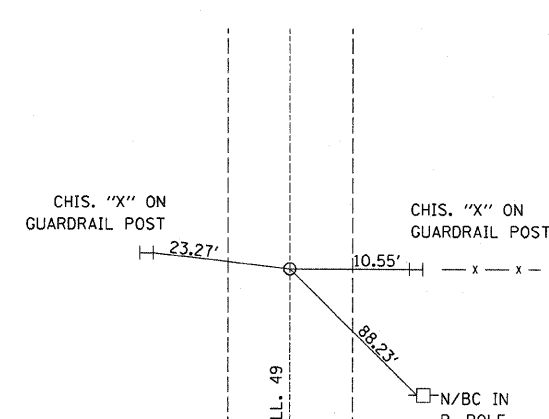
FOUND MAG NAIL AND WASHER ON C ILL. 49  
STA. XXX+XX  
W 1/4 COR. SEC. 36

PAY FOR AS PERMANENT SURVEY MARKER



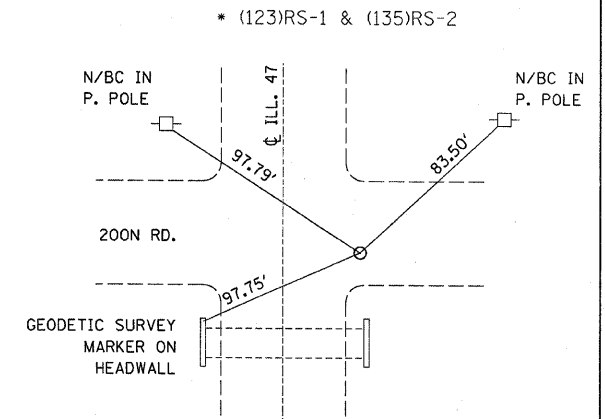
FOUND MAG NAIL & WASHER 6.4' EAST OF C  
ILL. 49 AND 3' SOUTH OF C 100 N. ROAD  
STA. XXX+XX  
SW CORNER SEC 25

PAY FOR AS PERMANENT SURVEY MARKER



FOUND MAG NAIL & WASHER 6.15 EAST OF ON  
C ILL. 49  
STA. XXX+XX  
W 1/4 COR. SEC. 25

PAY FOR AS PERMANENT SURVEY MARKER



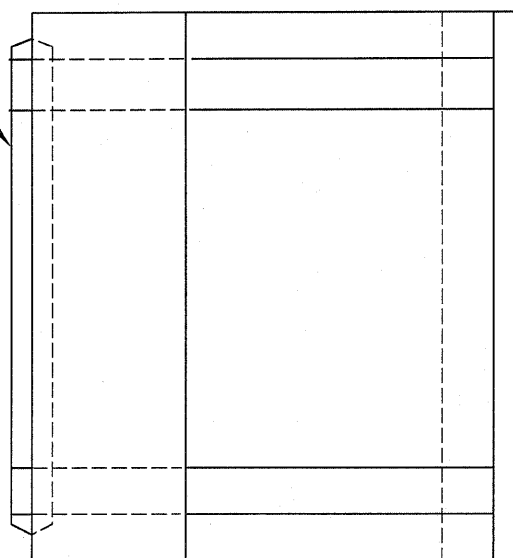
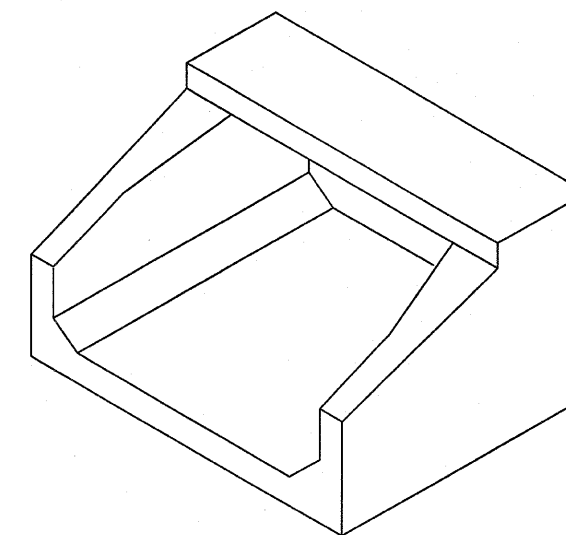
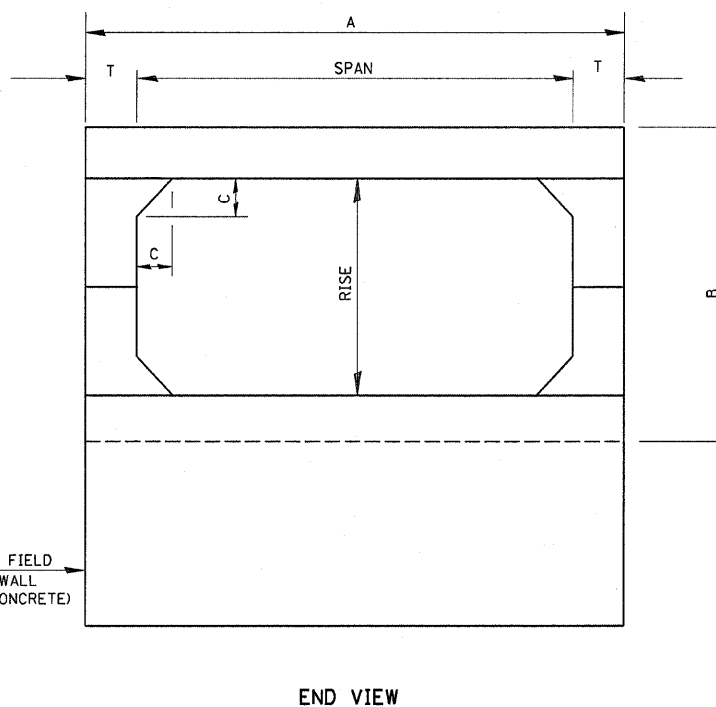
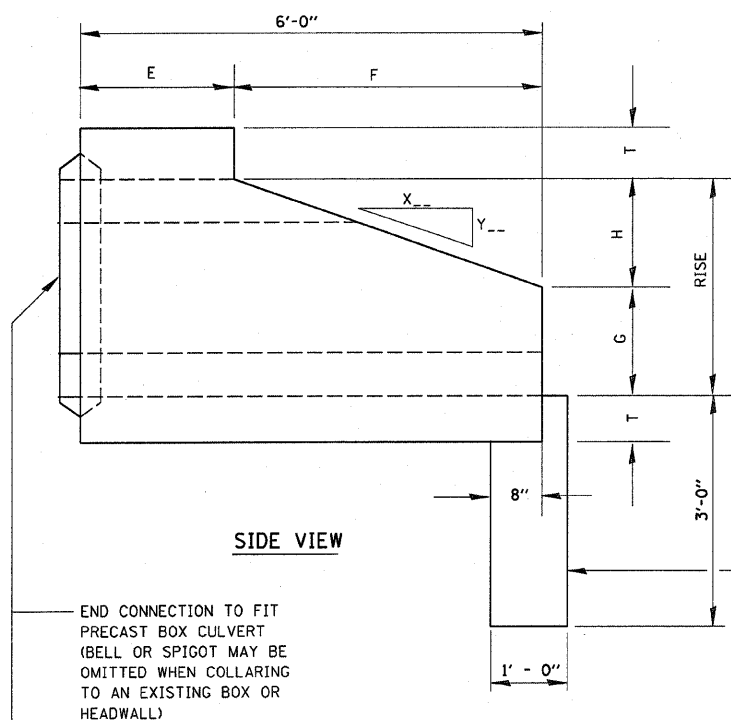
FOUND ALUM. DISK 22' EAST OF C ILL. 49  
STA. 104+33.26  
N.E. CORNER SEC 26, T24N, R24W, 2ND PM

PAY FOR AS PERMANENT SURVEY MARKER

March 15, 2004  
ed02604\detail15.dgn

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840		IROQUOIS	16	16
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

\* (123)RS-1 & (135)RS-2



SPAN X RISE	T (INCHES)	A (FT.-IN.)	B (FT.-IN.)	C (INCHES)	E (FT.-IN.)	F (FT.-IN.)	G (FT.-IN.)	H (FT.-IN.)	SLOPE
2' X 2'	4	2 - 8	2 - 8	4	3 - 0	3 - 0	1 - 0	1 - 0	3 : 1
3' X 2'	4	3 - 8	2 - 8	4	3 - 0	3 - 0	1 - 0	1 - 0	3 : 1
3' X 3'	4	3 - 8	3 - 8	4	2 - 0	4 - 0	1 - 8	1 - 4	3 : 1
4' X 2'	5	4 - 10	2 - 10	5	3 - 0	3 - 0	1 - 0	1 - 0	3 : 1
4' X 3'	5	4 - 10	3 - 10	5	2 - 0	4 - 0	1 - 8	1 - 4	3 : 1
4' X 4'	5	4 - 10	4 - 10	5	2 - 0	4 - 0	2 - 0	2 - 0	2 : 1
5' X 2'	6	6 - 0	3 - 0	6	3 - 0	3 - 0	1 - 0	1 - 0	3 : 1
5' X 3'	6	6 - 0	4 - 0	6	2 - 0	4 - 0	1 - 8	1 - 4	3 : 1
5' X 4'	6	6 - 0	5 - 0	6	2 - 0	4 - 0	2 - 0	2 - 0	2 : 1
5' X 5'	6	6 - 0	6 - 0	6		4 - 0	3 - 0	2 - 0	2 : 1
6' X 2'	7	7 - 2	3 - 2	7	3 - 0	3 - 0	1 - 0	1 - 0	3 : 1
6' X 3'	7	7 - 2	4 - 2	7	2 - 0	4 - 0	1 - 8	1 - 4	3 : 1
6' X 4'	7	7 - 2	5 - 2	7	2 - 0	4 - 0	2 - 0	2 - 0	2 : 1
6' X 5'	7	7 - 2	6 - 2	7		4 - 0	3 - 0	2 - 0	2 : 1
7' X 3'	8	8 - 4	4 - 4	8		4 - 0	1 - 8	1 - 4	3 : 1
7' X 4'	8	8 - 4	5 - 4	8		4 - 0	2 - 0	2 - 0	2 : 1
7' X 5'	8	8 - 4	6 - 4	8		4 - 0	3 - 0	2 - 0	2 : 1
8' X 3'	8	9 - 4	4 - 4	8		4 - 0	1 - 8	1 - 4	3 : 1
8' X 4'	8	9 - 4	5 - 4	8		4 - 0	2 - 0	2 - 0	2 : 1
8' X 5'	8	9 - 4	6 - 4	8		4 - 0	3 - 0	2 - 0	2 : 1
9' X 3'	9	10 - 6	4 - 6	9		4 - 0	1 - 8	1 - 4	3 : 1
9' X 4'	9	10 - 6	5 - 6	9		4 - 0	2 - 0	2 - 0	2 : 1
9' X 5'	9	10 - 6	6 - 6	9		4 - 0	3 - 0	2 - 0	2 : 1
10' X 4'	10	11 - 8	5 - 9	10		4 - 0	2 - 0	2 - 0	2 : 1
10' X 5'	10	11 - 8	6 - 8	10		4 - 0	3 - 0	2 - 0	2 : 1

540-16

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		PRECAST CONCRETE BOX CULVERT END SECTION

SCALE: VERT. DATE  
HORIZ. CHECKED BY

DRAWN BY

CONTRACT NO. 66425

06/11/1996  
 T:\ATL51\500596\0607  
 04\DWL\WORK\DW\1007\br\aboypc\dms84866\detail.is.dgn