

03-06-2015 LETTING ITEM 006

FOR INDEX OF SHEETS, SEE SHEET NO. 2

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PROPOSED  
HIGHWAY PLANS**

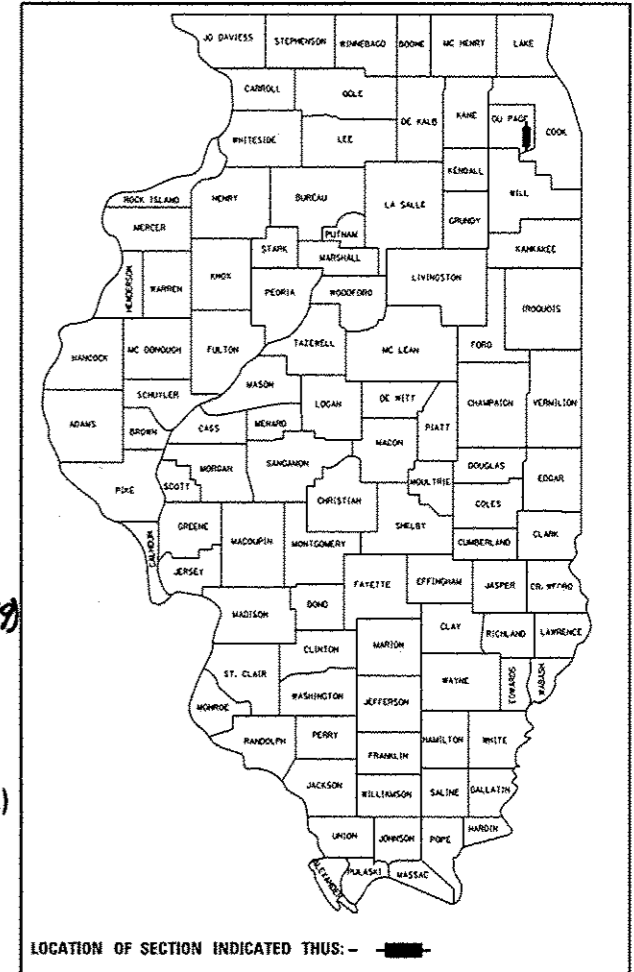
FAP ROUTE 344: IL ROUTE 83 (KINGERY HWY)  
SECTION (544 & 544-1) TS&N (13)  
AT MARION HILLS DITCH (63RD ST. DITCH) & AT 63RD STREET  
CULVERT REPLACEMENT  
TRAFFIC SIGNAL MODERNIZATION & CHANNELIZATION  
DUPAGE COUNTY

PROJ. ACNHPP-ACHSIP-0344(059)

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	120	1
ILLINOIS		CONTRACT NO. 60W55		

\* 128 + 2 = 130

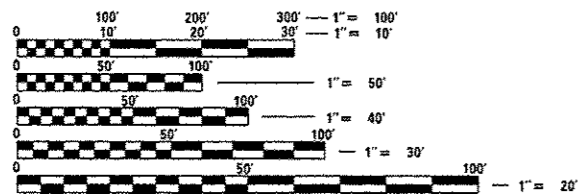
D-91-323-13



**DESIGN DESIGNATION**

OTHER PRINCIPAL ARTERIAL  
ADT 58900 (2012) IL ROUTE 83  
ADT 26100 (2012) 63RD STREET  
SPEED LIMIT 45 MPH (IL ROUTE 83)  
SPEED LIMIT 35 MPH (63RD STREET)  
DESIGN SPEED 45 MPH (IL ROUTE 83)  
DESIGN SPEED 40 MPH (63RD STREET)

IMPROVEMENT LOCATED IN  
THE VILLAGE OF WILLOWBROOK



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C-91-323-13

DOWNERS GROVE TOWNSHIP Range 11E - 3rd. PM



PROJECT ENDS (IL ROUTE 83)  
STA 118+03.53

IMPROVEMENT LOCATION  
IL 83 AT MARION HILLS DITCH  
EX. STRUCTURE NO: 022-0167  
PROP. STRUCTURE NO: 022-0563

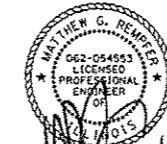
PROJECT BEGINS (IL ROUTE 83)  
STA 112+31.57

PROJECT BEGINS (63RD ST.)  
STA 9+83.89

PROJECT ENDS (63RD ST.)  
STA 17+67.69



*Ewa Mroczek*  
COLLINS ENGINEERS, INC.  
EWA MROCZEK, P.E., S.E.  
NO. 081-006067  
EXP.: 11/30/2016



*Matthew G. Rempfer*  
COLLINS ENGINEERS, INC.  
MATTHEW G. REMPFER  
NO. 062-054553  
EXPIRES 11-30-2015

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED *Dec 17 2014*

*John Fortman*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

*John D. Baranzelli*  
ENGINEER OF DESIGN AND ENVIRONMENT

*Amir Osman*  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT MANAGER: MR. ISSAM RAYYAN, P.E. (847) 705-4178  
PROJECT ENGINEER: MR. ROBERT T. BORO, P.E. (847) 705-4237

CONTRACT NO. 60W55

GROSS LENGTH = 572 FT. = 0.108 MILE LOCATION MAP NOT TO SCALE  
NET LENGTH = 572 FT. = 0.108 MILE

**COLLINS ENGINEERS, INC.**  
123 N. WACKER DR., SUITE 900  
CHICAGO, IL 60606  
(312) 704-9300  
ILLINOIS PROFESSIONAL DESIGN FIRM  
LICENSE NO. 184-000993

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

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BD34	DETAILS FOR DEPRESSED CURB AND GUTTER AND SHLD TREATMENT AT TBT TY 1 SPL
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**INDEX OF HIGHWAY STANDARDS:**

STANDARD NO.	DESCRIPTION
000001-06	SYMBOLS, ABBREVIATION, AND PATTERNS
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420001-08	PAVEMENT JOINTS
420106-05	36' JOINTED PCC PAVEMENT
420111-03	PCC PAVEMENT ROUNDOUTS
420701-02	PAVEMENT FABRIC
442101-07	CLASS B PATCHES
483001-04	PCC SHOULDER
515001-03	NAME PLATE FOR BRIDGES
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
601001-04	SUB-SURFACE DRAINS
602001-02	CATCH BASIN TYPE A
602101-02	DRAINAGE STRUCTURES TYPES 1, 2 & 3
602301-04	INLET - TYPE A
602306-03	INLET - TYPE B
602401-03	MANHOLE TYPE A
602601-03	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
604071-05	FRAME AND GRATE TYPE 20
604091-03	FRAME AND GRATE TYPE 24
606001-06	CONCRETE CURB TYPE B AND COMBINATION CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
630001-10	STEEL PLATE BEAM GUARDRAIL
630101-09	GUARDRAIL MOUNTED ON EXISTING CULVERTS
630201-06	PCC/HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
630301-06	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
631011-09	TRAFFIC BARRIER TERMINAL TYPE 2
635001-01	DELINEATORS
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
637006-03	CONCRETE BARRIER, DOUBLE FACE, 42 in. HEIGHT
643001-02	SAND MODULE IMPACT ATTENUATORS
701101-04	OFF-RD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701422-07	LANE CLOSURE, MULTILANE, FOR SPEEDS >= 45 MPH TO 55 MPH
701426-07	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS >= 45 MPH
701427-03	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS < 40 MPH
701601-07	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRANSVERSABLE MEDIAN
701606-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-07	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-04	TRAFFIC CONTROL DEVICES
704001-07	TEMPORARY CONCRETE BARRIER
720001-01	SIGN PANEL MOUNTING DETAILS
780001-05	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
782001	PRISMATIC CURB REFLECTORS
805001-01	ELECTRICAL SERVICE INSTALLATION DETAILS
814001-03	HANDHOLES
814006-02	DOUBLE HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
862001-01	UNINTERRUPTABLE POWER SUPPLY (UPS)
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
876001-03	PEDESTRIAN PUSH BUTTON POST
877001-05	STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
877002-02	STEEL MAST ARM ASSEMBLY AND POLE 56' THROUGH 75'
880001-01	CONCRETE FOUNDATION DETAILS
880001-01	SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

MIXTURE TYPE	AIR VOIDS	OMP
TEMPORARY PAVEMENT (NON-INTERSTATE):		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D" NSO (IL 9.5mm); 2"	4% @ 50 Gyr	QC/OA
TEMPORARY PAVEMENT (HMA BINDER IL-19 mm); 8"	4% @ 50 Gyr	QC/OA
HOT-MIX ASPHALT SHOULDERS:		
HOT-MIX ASPHALT SHOULDER, 6" (HMA BINDER IL-19.0)	4% @ 50 Gyr	QC/OA
OMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

- 1) THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SOYD/IN
- 2) FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
- 3) FOR USE OF RECYCLED MATERIALS, SEE SPECIAL PROVISIONS.
- 4) THE CONTRACTOR HAS THE OPTION TO USE PC TEMPORARY PAVEMENT. PC CONCRETE TEMPORARY PAVEMENT SHALL CONSIST OF CLASS PV CONCRETE MEETING THE REQUIREMENTS OF ART. 1020 OF THE STANDARD SPECIFICATIONS; TYPICALLY 10" THICK.
- 5) TEMPORARY PAVEMENT DOES NOT REQUIRE DOWEL BAR.
- 6) QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

**GENERAL NOTES:**

- 1 NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET.
- 2 ALL UTILITIES, SCHOOL DISTRICTS, LOCAL POLICE, AND FIRE DEPARTMENTS SHALL BE NOTIFIED BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION.
- 3 UNLESS AUTHORIZED BY THE ENGINEER, ALL EXISTING ACCESS POINTS SHALL BE MAINTAINED AT ALL TIMES BY THE CONTRACTOR.
- 4 DURING THE CONSTRUCTION, THE CONTRACTOR WILL BE REQUIRED, AT HIS EXPENSE, TO HAVE AVAILABLE A WATER TRUCK OR SIMILAR EQUIPMENT TO CONTROL DUST. IF NECESSARY, THE CONTRACTOR SHALL BE REQUIRED TO CONTROL DUST DURING NON-WORKING HOURS.
- 5 ALL EXCESS MATERIAL (BROKEN CONCRETE, CULVERT PIPE, WASTE ROADWAY EXCAVATION, SURPLUS MATERIAL FROM SEWER TRENCHES, ETC.) SHALL BE LEGALLY DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SELECT DUMP SITES AND OBTAIN PERMISSION AND ALL NECESSARY PERMITS TO USE SUCH DUMP SITES. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR EARTH EXCAVATION.
- 6 BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL J.U.L.I.E. AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES (48 HOUR NOTICE IS REQUIRED).
- 7 THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE RIGHT-OF-WAY OR PROPERTY WITHOUT PRIOR WRITTEN PERMISSION FROM THE ENGINEER.
- 8 THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH AFFECTED UTILITY COMPANIES AND THE VILLAGE OF WILLOWBROOK AND DOWNERS GROVE TOWNSHIP.
- 9 10 FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB AND GUTTER AND MEDIAN ITEMS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED LARGER ITEM OF SPECIFIED WORK.
- 10 PRIOR TO EMBANKMENT PLACEMENT, ALL VEGETATION, LOOSE MATERIAL, AND UNSTABLE MATERIAL SHOULD BE REMOVED TO DEPTH ENCOUNTERED AND REPLACED WITH SUITABLE EMBANKMENT MATERIAL. ANY EMBANKMENT WIDENING ON EXISTING SLOPES SHOULD BE BENCHED IN ACCORDANCE WITH ARTICLE 205.04 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- 11 BEFORE BEGINNING ANY WORK THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- 12 FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.
- 13 THE CONTRACTOR SHALL MAINTAIN ALL ROADWAYS OPEN TO TRAFFIC AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS.
- 14 THE CONTRACTOR SHALL CONTACT THE IDOT TRAFFIC CONTROL SUPERVISOR, AT 847-705-4470 A MINIMUM OF 72 HOURS PRIOR TO BEGINNING ANY WORK.
- 15 THE RESIDENT ENGINEER SHALL CONTACT DON CHIARUGI AT (847) 741-9857 A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACING THE PERMANENT PAVEMENT MARKINGS.
- 16 THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTOR'S OWN EXPENSE.
- 17 THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.
- 18 FOR STORM SEWER CONSTRUCTED UNDER THE ROADWAY, BACKFILLING METHODS TWO AND THREE AUTHORIZED UNDER THE PROVISIONS OF ARTICLE 550.07 OF THE STANDARD SPECIFICATIONS WILL NOT BE ALLOWED.

**GENERAL NOTES (CONT.):**

- 20 BEFORE ORDERING STORM SEWERS, CATCH BASINS, PIPE CULVERTS, PIPE DRAINS, MANHOLES, INLETS, AND SCUPPERS, THE CONTRACTOR SHALL REVIEW THE EXISTING FIELD CONDITIONS AND THE DRAINAGE SCHEDULES FOUND IN THE PLANS FOR THE EXACT LENGTH AND QUANTITY REQUIRED.
- 21 THE CONTRACTOR SHALL MAINTAIN THE SURFACE DRAINAGE OF ALL ROADWAYS DURING CONSTRUCTION OF THIS PROJECT. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS, INLETS, AND CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER, WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE SAME. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM ALL THESE TEMPORARY CONNECTIONS UNTIL INSTALLATION IS COMPLETE, INCLUDING PAVEMENT. THIS WORK SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT. COORDINATION WITH ALL AGENCIES INVOLVED IS REQUIRED.
- 22 DURING CONSTRUCTION OPERATIONS, IF ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DUST AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
- 23 AGGREGATE SUBGRADE IMPROVEMENT HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR THE REMOVAL AND REPLACEMENT WITH AGGREGATE SUBGRADE IMPROVEMENT WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 AND THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS ENCOUNTERED, THE SOIL SHALL BE REMOVED AND REPLACED WITH AGGREGATE SUBGRADE IMPROVEMENT OR EMBANKMENT AS DETERMINED BY THE GEOTECHNICAL ENGINEER. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
- 24 ALL EXISTING CULVERTS, STORM SEWERS, OR DRAINAGE STRUCTURES MARKED FOR REMOVAL ON THE PLANS OR DESIGNATED IN THE FIELD BY THE ENGINEER TO BE REMOVED SHALL BE REMOVED AND ANY EXCAVATION SHALL BE BACKFILLED WITH A GRANULAR MATERIAL MEETING THE SPECIFICATIONS FOR FA-1 OR FA-2. THE COST OF ALL LABOR AND MATERIALS REQUIRED TO COMPLETE THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES FOR STORM SEWER OR PIPE CULVERT UNLESS PAID FOR AS A SPECIFIC ITEM.
- 25 ALL EXISTING GRANULAR AND HOT-MIX ASPHALT PAVEMENT TO BE REMOVED AND NOT PAID AS A SPECIFIC ITEM SHALL BE CONSIDERED EARTH EXCAVATION AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR EARTH EXCAVATION. THE CONTRACTOR WILL HAVE THE OPTION OF REMOVING THE EXISTING HOT-MIX ASPHALT PAVEMENT BY GRINDING OR EXCAVATING. IF THE HOT-MIX ASPHALT PAVEMENT IS REMOVED BY EXCAVATION, IT MAY NOT BE USED IN EMBANKMENT AREAS UNLESS SPECIFICALLY AUTHORIZED BY THE ENGINEER. HOT-MIX ASPHALT PAVEMENT REMOVED BY GRINDING MAY BE USED AS EMBANKMENT MATERIAL. NO HOT-MIX ASPHALT PAVEMENT SHALL BE REMOVED IN AREAS TO BE USED FOR TEMPORARY ROADWAY.
- 26 THE CONTRACTOR SHALL NOT CROSS COMPLETED BASE COURSE OR EXISTING PAVEMENT, NOT SCHEDULED TO BE REMOVED, WITH TRACK EQUIPMENT OR LOADED SCRAPERS.
- 27 ALL EMBANKMENTS AND SUB-GRADE SHALL BE COMPACTED TO THE SATISFACTION OF THE ENGINEER PRIOR TO PLACING AGGREGATE SUBGRADE OR SUB-BASE GRANULAR MATERIAL.
- 28 ALL EXISTING DOMESTIC BUFFALO BOXES ARE TO BE ADJUSTED BY THE CONTRACTOR. THE COST OF THIS WORK WILL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR EARTH EXCAVATION.
- 29 THE MAXIMUM COMPACTED THICKNESS OF A LIFT OF HOT-MIX ASPHALT BASE COURSE SHALL BE FOUR (4) INCHES UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
- 30 HOT-MIX ASPHALT BASE COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN BACKFILLED TO THE SATISFACTION OF THE ENGINEER.
- 31 THE CONTRACT UNIT PRICES FOR ITEMS USED TO CONSTRUCT TEMPORARY PAVEMENT OR ACCESS ROADS SHALL INCLUDE ALL EQUIPMENT, LABOR AND MATERIAL REQUIRED TO PLACE, REMOVE, AND DISPOSE OF THE TEMPORARY PAVEMENT OR ACCESS ROAD.

**DRAINAGE AND UTILITIES GENERAL NOTES:**

- 1 THE STATION / OFFSET / ELEVATIONS NOTED FOR ALL DRAINAGE STRUCTURES LOCATED IN THE CURB LINE REFER TO THE POSITION OF THE ADJACENT PROPOSED EDGE OF PAVEMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE OFFSET NECESSARY FOR THE STRUCTURES TO SET THE FRAME AND GRATES IN THE PROPER LOCATION. ALL OTHER STRUCTURES ARE DIMENSIONED TO THE CENTER OF THE STRUCTURE; ELEVATION INDICATES RIM GRADES.
- 2 THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING LOCAL AGENCIES MAINTAINING SANITARY SEWERS, WATERMANS, AND STREET LIGHTS TO VERIFY THE MATERIALS AND METHODS ALLOWED FOR THE ADJUSTMENT, RELOCATION, OR EXTENSION OF THE UTILITY INVOLVED.
- 3 THE LOCATION AND ELEVATION OF EXISTING UTILITIES ARE APPROXIMATE AND ARE PROVIDED BY THE OWNERS. THE EXACT LOCATIONS AND ELEVATIONS ARE TO BE VERIFIED BY THE CONTRACTOR THROUGH THE OWNERS OF THE UTILITIES.
- 4 EMBANKMENTS SHALL BE COMPLETED TO THE SATISFACTION OF THE ENGINEER PRIOR TO EXCAVATION FOR STORM SEWER.
- 5 THE COST OF MAKING STORM SEWER CONNECTIONS TO EXISTING OR PROPOSED SEWER SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE STORM SEWER BEING CONNECTED.
- 6 MANHOLES AND CATCH BASINS SHALL BE CONSTRUCTED WITH FLAT TOPS WHERE THE DIFFERENCE BETWEEN THE RIM ELEVATION AND INVERT ELEVATION IS LESS THAN SIX (6) FEET.
- 7 ALL ADJUSTMENTS OR RECONSTRUCTIONS SHALL INCLUDE THE REMOVAL AND REPLACEMENT, AT THE CONTRACTOR'S EXPENSE, OF ALL UNSUITABLE TWO (2) FOOT INSIDE DIAMETER ADJUSTING RINGS.
- 8 ADJUSTMENT OF STRUCTURES MAINTAINED BY OTHER AGENCIES SHALL BE MADE TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY MAINTAINING THE STRUCTURE INVOLVED.
- 9 ALL MANHOLES AND INLETS SHALL HAVE POURED INVERTS. THE COST OF INVERTS SHALL BE INCLUDED IN THE COST OF THE STRUCTURE.
- 10 ALL FIELD TILES ENCOUNTERED SHALL BE CAREFULLY PRESERVED AND CONNECTED TO PROPOSED DRAINAGE STRUCTURES, SEWERS, OR DITCHES, AS DIRECTED BY THE ENGINEER; THIS WORK WILL BE PAID FOR AT THE APPLICABLE CONTRACT UNIT PRICE OR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- 11 TRENCHES CROSSING TRAFFIC LANES SHALL BE TEMPORARILY PATCHED WITH FOUR (4) INCHES HOT-MIX ASPHALT BASE COURSE; THE COST OF THE HOT-MIX ASPHALT BASE COURSE WILL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE SEWER, CULVERT, WATERMAIN, OR OTHER ITEM PLACED IN TRENCH. THIS PRICE SHALL INCLUDE THE COST OF MAINTAINING THE PATCH TO THE SATISFACTION OF THE ENGINEER.

**TRAFFIC SIGNAL GENERAL NOTES:**

- 1 IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARMS LENGTHS
- 2 THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL JULIE AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO. THE CONTRACTOR SHALL CALL JULIE AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO. CONTACT DICER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
- 3 THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.

FILE NAME :	USER NAME :	DESIGNED :	REVISED :	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 83 AT MARION HILLS DITCH GENERAL NOTES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
11729017290.26 - IL83 and 63rd St/CAD0	rgell	-	-		344	(544 & 544-1)	TS&N (13)	DUPAGE	129	3			
CADD SHEET: 11729017290.26 - IL83 and 63rd St/CAD0	rgell	DRAWN	REVISED		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 60W55				
PLT SCALE = 2.0000' / 1"	rgell	CHECKED	REVISED		ILLINOIS FED. AID PROJECT								
Default	rgell	DATE	REVISED										

**EROSION CONTROL AND LANDSCAPING GENERAL NOTES:**

- 1 ALL WORK SHALL BE DONE IN ACCORDANCE WITH SECTION 15-116 OF THE DUPAGE COUNTY COUNTYWIDE STORM WATER AND FLOOD PLAIN ORDINANCE, EFFECTIVE SEPTEMBER 24, 1991 AND ALL SUBSEQUENT REVISIONS. ALL SEDIMENT AND EROSION CONTROL MEASURES WILL BE INSTALLED PER IDOT STANDARD 280001 OR AS SPECIFIED HEREIN AND PAID FOR IN ACCORDANCE WITH SECTION 280 OF THE STANDARD SPECIFICATIONS. ALL CONSTRUCTION ACTIVITIES WILL BE IN ACCORDANCE WITH THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM STORM WATER PERMIT ILR10 AND ILR40.
- 2 EROSION CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH THE SEQUENCE OF STAGE CONSTRUCTION. THE CONTRACTOR SHALL SUBMIT A DETAILED SCHEDULE FOR APPROVAL.
- 3 SEDIMENT AND EROSION CONTROL DEVICES SHALL BE FUNCTIONAL BEFORE THE PROJECT SITE IS OTHERWISE DISTURBED.
- 4 ALL DISTURBED AREAS SHALL BE SEEDED OR SODDED AS SOON AS PRACTICAL AFTER CONSTRUCTION ACTIVITIES IN THAT AREA HAVE CONCLUDED. ALL ERODABLE/BARE AREAS SHALL BE SEEDED EVERY 7 DAYS WITH TEMPORARY EROSION CONTROL SEEDING. ERODABLE AREAS OUTSIDE AND DOWN SLOPE FROM THE CONSTRUCTION LIMITS SHALL BE SIMILARLY SEEDED.
- 5 THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT OR BY HIS WORK CREWS. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN WETLANDS.
- 6 WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED.
- 7 WHEN TEMPORARY DRAINAGE IS ESTABLISHED, EROSION CONTROL MEASURES MAY BE REQUIRED BY THE ENGINEER.
- 8 GRAVEL ROADS, ACCESS DRIVES, PARKING AREAS OF SUFFICIENT WIDTH AND LENGTH, AND VEHICLE WASH DOWN FACILITIES IF NECESSARY, SHALL BE PROVIDED TO PREVENT SOIL FROM BEING TRACKED ONTO PUBLIC OR PRIVATE ROADWAYS. ANY SOIL REACHING A PUBLIC OR PRIVATE ROADWAY SHALL BE REMOVED BEFORE THE END OF EACH WORKDAY AND AS NEEDED.
- 9 CLEANING OF VEHICLES AND EQUIPMENT, INCLUDING CONCRETE MIXERS, SHALL BE PERFORMED IN A MANNER TO REDUCE THE AMOUNT OF POLLUTANTS TRIBUTARY TO STORM SEWERS AND OPEN WATERS TO THE MAXIMUM EXTENT PRACTICAL.
- 10 ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTION RUNOFF. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
- 11 SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON THE SITE ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE FILTER DEVICE.
- 12 THE ENGINEER SHALL INSPECT EROSION CONTROL MEASURES PERIODICALLY AND WITHIN 24 HOURS OF ANY STORM EXCEEDING 1/2 INCH PRECIPITATION. DAMAGED AND INEFFECTIVE EROSION CONTROL MEASURES SHALL BE REPAIRED OR REPLACED WITHIN 72 HOURS. EROSION CONTROL SYSTEMS REPLACED DUE TO SEDIMENT LOADING WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE APPLICABLE EROSION CONTROL ITEM.
- 13 THE COST OF REPAIRING OR REMOVING SEDIMENT FROM EROSION CONTROL SYSTEMS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE APPLICABLE EROSION CONTROL ITEM.
- 14 ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SEDIMENT AND EROSION CONTROL MEASURES ARE OPERATIONAL.
- 15 TREES NOT MARKED FOR REMOVAL SHALL BE CONSIDERED AS DESIGNATED TO BE SAVED AND SHALL BE PROTECTED UNDER THE PROVISIONS OF ARTICLE 201.05 OF THE STANDARD SPECIFICATIONS.
- 16 ALL CLEARING, REMOVAL OF BUSHES, HEDGES AND TREES UNDER SIX (6) INCHES IN DIAMETER SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR EARTH EXCAVATION.
- 17 THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) AND USE/WASTE REVIEW (BDE 2290) SUBMITTALS, THE CONTRACTOR SHALL SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION 11.C.1 AND 2 OF THE SWPPP. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- 18 THIS PROJECT REQUIRES A US ARMY CORPS OF ENGINEERS (USACE) 404 PERMIT THAT WILL BE SECURED BY THE DEPARTMENT. AS A CONDITION OF THIS PERMIT, THE CONTRACTOR WILL NEED TO SUBMIT AN IN-STREAM WORK PLAN TO THE BUREAU OF MAINTENANCE FOR APPROVAL. GUIDELINES ON ACCEPTABLE IN-STREAM WORK TECHNIQUES CAN BE FOUND ON THE USACE WEBSITE. THE USACE DEFINES AND DETERMINES IN-STREAM WORK. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT AN IN-STREAM WORK PLAN WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

**EROSION CONTROL AND LANDSCAPING GENERAL NOTES (CONT'D):**

- 19 OVERHANGING LIMBS ARE TO BE TRIMMED OR CUT OFF TO PROVIDE A MINIMUM VERTICAL CLEARANCE OF TWENTY (20) FEET FROM THE FINISHED SURFACE OF THE ROAD. CLEARANCE TO SIDEWALKS OR PATHS SHALL BE AS DIRECTED BY THE ENGINEER.
- 20 LIMB PRUNING SHALL BE PERFORMED UNDER THE SUPERVISION OF AN APPROVED TREE EXPERT AS STATED IN THESE NOTES AND SHALL BE UNDERTAKEN IN A TIMELY FASHION SO AS NOT TO INTERFERE WITH CONSTRUCTION.
- 21 ALL CUTS OVER ONE (1) INCH IN DIAMETER SHALL BE MADE AT THE GROWTH RING AT THE NEXT LARGE BRANCH.
- 22 ALL LIMBS, BRANCHES, AND OTHER DEBRIS RESULTING FROM THIS WORK SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS EXPENSE OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY. THE COST OF THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TREE REMOVAL.
- 23 TOPSOIL SHALL BE PLACED TO A DEPTH AS SPECIFIED ON THE PLANS AND BE MEASURED IN SQUARE YARDS.
- 24 THE CROSS SECTIONS INDICATE THE FINISHED GRADE OF TOPSOIL.
- 25 TOPSOIL SHALL NOT BE STOCKPILED WITHIN THE LIMITS OF CONSTRUCTION; THE LOCATIONS OF TOPSOIL STOCKPILES WITHIN THE RIGHT-OF-WAY MUST BE APPROVED BY THE ENGINEER.
- 26 ALL ESC MEASURES WILL BE MAINTAINED IN ACCORDANCE WITH THE IDOT EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION AND IDOT'S BEST MANAGEMENT PRACTICES MAINTENANCE GUIDE: (HTTP://WWW.IDOT.ILLINOIS.GOV/TRANSPORTATION-SYSTEM/ENVIRONMENT/EROSION-AND-SEDIMENT-CONTROL).
- 27 THE CONTRACTOR WILL ASSUME RESPONSIBILITY FOR MAINTENANCE OF ALL SOIL EROSION CONTROL DURING CONSTRUCTION.
- 28 THE CONTRACTOR SHALL CHECK ALL ESC MEASURES WEEKLY AND AFTER EACH RAINFALL, 0.5 INCHES OR GREATER IN A 24 HOUR PERIOD, OR EQUIVALENT SNOWFALL. ADDITIONALLY DURING WINTER MONTHS, ALL MEASURES SHOULD BE CHECKED BY THE CONTRACTOR AFTER EACH SIGNIFICANT SNOWMELT.
- 29 THE CONTRACTOR SHOULD PROVIDE TO THE RE A PLAN TO ENSURE THAT A STABILIZED FLOW LINE WILL BE PROVIDED DURING STORM SEWER CONSTRUCTION. THIS IS IMPORTANT WHERE NEW STORM SEWER CONNECTS TO EXISTING CULVERTS.
- 30 THE USE OF A STABILIZED FLOW LINE BETWEEN INSTALLED STORM SEWER AND OPEN DISTURBANCE WILL REDUCE THE POTENTIAL FOR THE OFFSITE DISCHARGE OF SEDIMENT-BEARING WATERS, ESPECIALLY WHEN RAIN IS FORECASTED, SO THAT FLOW WILL NOT ERODE. LACK OF APPROVED PLAN OR FAILURE TO COMPLY WILL RESULT IN AN ESC DEFICIENCY DEDUCTION.
- 31 ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY, PRIOR TO ACCEPTANCE OF THE IMPROVEMENT. ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL.
- 32 TEMPORARY OR PERMANENT STABILIZATION SHALL BE INITIATED WITHIN ONE (1) DAY UPON COMPLETION OF DISTURBANCE OR IF THE WORK AREA IS TO BE LEFT UNDISTURBED FOR 14 DAYS OR MORE.
- 33 EROSION CONTROL ITEMS ARE CONSIDERED TO BE A HIGH PRIORITY ON THIS CONTRACT. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE RE.
- 34 TEMPORARY FENCE AND "WETLANDS NO INTRUSION SIGNAGE" SHOULD BE PROVIDED AT THE BOUNDARY OF ALL UN-IMPACTED WETLANDS AND/OR WOUS, AS SHOWN ON THE PLANS WITHIN THE PROJECT LIMITS. SIGNS CAN BE BORROWED FROM THE BUREAU OF MAINTENANCE.

**COMMITMENTS:**

NONE

FILE NAME :	USER NAME :	DESIGNED :	REVISED :	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 83 AT MARION HILLS DITCH GENERAL NOTES</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
1\7290\7290.26 - IL83 and 63rd St\CAD00	rgell	-	-			344	1544 & 544-1) T5&N (13)	DUPAGE	129	4	
	CADD_Sheets\0160455-shrtrgnote.dgn	DRAWN :	REVISED :			<b>CONTRACT NO. 60W55</b>					
		-	-			ILLINOIS FED. AID PROJECT					
Default		PLOT SCALE :	REVISED :	SCALE:		SHEET OF SHEETS		STA. TO STA.			
		2,000' / in.	-								
		PLOT DATE :	REVISED :								
		11/26/2014	-								

M330

M001

M001

M330

URBAN

CONSTRUCTION CODE

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				IL ROUTE 83 90% HSIP/ 10% STATE ROADWAY 0004 URBAN	63RD STREET 80% NHPP/ 20% STATE ROADWAY 0004 URBAN	80% NHPP/20% STATE BOX CULVERT 0011 S.N. 022-0563	90% HSIP/ 5% STATE/ 5% DUPAGE CO TRAFFIC SIGNALS 0021 URBAN	50% WILLOWBROOK/ 50% TRISTATE FPD EVP 0021 URBAN
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	154	154				
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	76	76				
20101100	TREE TRUNK PROTECTION	EACH	3		3			
20200100	EARTH EXCAVATION	CU YD	5050	3680	1370			
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	430	430				
20800150	TRENCH BACKFILL	CU YD	264	220	44			
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1964	1964				
21101625	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	591		591			
25000300	SEEDING, CLASS 3	ACRE	0.25	0.15	0.10			
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	23	14	9			
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	23	14	9			
25100635	HEAVY DUTY EROSION CONTROL BLANKET	SQ YD	893	488	405			
25200110	SODDING, SALT TOLERANT	SQ YD	1664	1477	187			
25200200	SUPPLEMENTAL WATERING	UNIT	17	15	2			

\* - DENOTES SPECIALTY ITEM

FILE NAME :	USER NAME :	DESIGNED :	REVISED :
HW7290\7290.26 - IL83 and 63rd St\CA00	rgall	-	-
CADD: Sheets\DISP\55-shr-500.dgn		DRAWN :	REVISED :
		-	-
		CHECKED :	REVISED :
		-	-
		DATE :	REVISED :
		-	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	5
CONTRACT NO. 60W55			ILLINOIS FED. AID PROJECT	

14

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				IL ROUTE 83 90% HSIP/ 10% STATE	63RD STREET 80% NHPP/ 20% STATE	80% NHPP/20% STATE	90% HSIP/ 5% STATE/ 5% DUPAGE CO	50% WILLOWBROOK/ 50% TRISTATE FPD
				ROADWAY	ROADWAY	BOX CULVERT	TRAFFIC SIGNALS	EVP
				0004 URBAN	0004 URBAN	0011 S.N. 022-0563	0021 URBAN	0021 URBAN
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	216	176	40			
28000305	TEMPORARY DITCH CHECKS	FOOT	20	10	10			
28000400	PERIMETER EROSION BARRIER	FOOT	1756	955	801			
28000500	INLET AND PIPE PROTECTION	EACH	6	3	3			
28000510	INLET FILTERS	EACH	47	25	22			
28100105	STONE RIPRAP, CLASS A3	SQ YD	12	12				
28100107	STONE RIPRAP, CLASS A4	SQ YD	144			144		
28200200	FILTER FABRIC	SQ YD	156	12		144		
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	1186	669	517			
30300104	AGGREGATE SUBGRADE IMPROVEMENT 4"	SQ YD	2519	2519				
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	7918	7918				
31101100	SUBBASE GRANULAR MATERIAL, TYPE B	CU YD	141		141			
31102300	SUBBASE GRANULAR MATERIAL, TYPE C 6"	SQ YD	730		730			
42000406	PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)	SQ YD	1581		1581			

URBAN

\* - DENOTES SPECIALTY ITEM

FILE NAME : I:\7290\7290.26 - IL83 and 63rd St\CA00	USER NAME : rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>				F.A.P. RATE:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CA00_Sheets\DIGW55-shr-500.dgn	DRAWN -	REVISED -	344						(544 & 544-1) TS&N (13)	DUPAGE	129	6	
PLOT SCALE : 2.0000' / 1"	CHECKED -	REVISED -	SCALE: SHEET OF SHEETS STA. TO STA.					CONTRACT NO. 60W55					
PLOT DATE : 12/17/2014	DATE -	REVISED -	ILLINOIS FED. AID PROJECT										

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				IL ROUTE 83	63RD STREET	80% NHPP/20% STATE	90% HSIP/5% STATE/	50% WILLOWBROOK/
				90% HSIP/10% STATE	80% NHPP/20% STATE	80% NHPP/20% STATE	5% DUPAGE CO	50% TRISTATE FPD
				ROADWAY	ROADWAY	BOX CULVERT	TRAFFIC SIGNALS	EVP
0004	0004	0011	0021	0021				
URBAN	URBAN	S.N. 022-0563	URBAN	URBAN				
42000506	PORTLAND CEMENT CONCRETE PAVEMENT 10 1/4" (JOINTED)	SQ YD	7262	7262				
42001300	PROTECTIVE COAT	SQ YD	10014	8155	1859			
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	79	79				
44000100	PAVEMENT REMOVAL	SQ YD	11181	9581	1600			
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	76	76				
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	2902	2186	716			
44001980	CONCRETE BARRIER REMOVAL	FOOT	727	727				
44003100	MEDIAN REMOVAL	SQ FT	315	315				
44004250	PAVED SHOULDER REMOVAL	SQ YD	492	347	145			
44200976	CLASS B PATCHES, TYPE IV, 10 INCH	SQ YD	704		704			
48203021	HOT-MIX ASPHALT SHOULDERS, 6"	SQ YD	462	308	154			
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1			1		
50200100	STRUCTURE EXCAVATION	CU YD	2697			2697		
50200450	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL FOR STRUCTURES	CU YD	323			323		

URBAN

\* - DENOTES SPECIALTY ITEM *Rev.*

FILE NAME =	USER NAME = rgell	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
h:\7290\7290.26 - IL83 and 63rd St\CAD00	CADD Sheets\DIGW55-shr-500.dgn	DRAWN -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	344	(544 & 544-1) TS&N (13)	DUPAGE	129	7
Default	PLOT SCALE = 2.0000' / 1" IN	CHECKED -	REVISED -		CONTRACT NO. 60W55										
	PLOT DATE = 12/17/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT										

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				IL ROUTE 83	63RD STREET	80% NHPP/20% STATE	90% HSIP/ 5% STATE/	50% WILLOWBROOK/
				90% HSIP/ 10% STATE	80% NHPP/ 20% STATE	80% NHPP/20% STATE	5% DUPAGE CO	50% TRISTATE FPD
				ROADWAY	ROADWAY	BOX CULVERT	TRAFFIC SIGNALS	EVP
0004	0004	0011	0021	0021				
URBAN	URBAN	S.N. 022-0563	URBAN	URBAN				
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	150,230			150,230		
50800515	BAR SPLICERS	EACH	502			502		
51500100	NAME PLATES	EACH	1			1		
54003000	CONCRETE BOX CULVERTS	CU YD	767.2			767.2		
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	3	3				
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	1		1			
54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"	EACH	2	2				
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	567	567				
550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	188	173	15			
550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	181	181				
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	99		99			
55100500	STORM SEWER REMOVAL 12"	FOOT	790	670	120			
55100700	STORM SEWER REMOVAL 15"	FOOT	220	207	13			
55100900	STORM SEWER REMOVAL 18"	FOOT	145	145				

URBAN

\* - DENOTES SPECIALTY ITEM *Rev.*

FILE NAME: 11729017290.25 - IL83 and 63rd St/CADD	USER NAME: rge01	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CADD SHEET: DIGWSS-shr-500.dgn	DRAWN -	REVISED -	344						(544 & 544-1) TS&N (13)	DUPAGE	129	8	
PLOT SCALE: 2.0000' / in.	CHECKED -	REVISED -	SCALE: SHEET OF SHEETS STA. TO STA.					CONTRACT NO. 60W55					
PLOT DATE: 12/17/2014	DATE -	REVISED -	ILLINOIS FED. RD. PROJECT										



CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				IL ROUTE 83 90% HSIP/ 10% STATE	63RD STREET 80% NHPP/ 20% STATE	80% NHPP/20% STATE	90% HSIP/ 5% STATE/ 5% DUPAGE CO	50% WILLOWBROOK/ 50% TRISTATE FPD
				ROADWAY	ROADWAY	BOX CULVERT	TRAFFIC SIGNALS	EVP
				0004 URBAN	0004 URBAN	0011 S.N. 022-0563	0021 URBAN	0021 URBAN
				URBAN				
60107600	PIPE UNDERDRAINS 4"	FOOT	251		251			
60200905	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 9 FRAME AND GRATE	EACH	1	1				
60201105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 11 FRAME AND GRATE	EACH	1	1				
60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	7	7				
60205040	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	1	1				
60212814	CATCH BASINS, TYPE D, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	2		2			
60219540	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	1		1			
60236800	INLETS, TYPE A, TYPE 11 FRAME AND GRATE	EACH	1	1				
60237470	INLETS, TYPE A, TYPE 24 FRAME AND GRATE	EACH	9	7	2			
60240301	INLETS, TYPE B, TYPE 8 GRATE	EACH	1	1				
60247160	DRAINAGE STRUCTURES, TYPE 1 WITH TWO TYPE 20 FRAMES AND GRATES	EACH	1	1				
60251740	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE	EACH	5	1	4			
60255500	MANHOLES TO BE ADJUSTED	EACH	1		1			
60255700	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	3	1	2			

\* - DENOTES SPECIALTY ITEM

FILE NAME : I:\7290\7290.26 - IL83 and 63rd St\CA00	USER NAME : rgell	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CADD SHEETS: DISGWS5-sh1-500.dgn	DRAWN -	REVISED -	344						(544 & 544-1)	TS&N (13)	DUPAGE	129	9
PLOT SCALE : 2,0000' / 1"	CHECKED -	REVISED -	CONTRACT NO. 60W55					ILLINOIS FED. AID PROJECT					
PLOT DATE : 12/17/2014	DATE -	REVISED -	SCALE:		SHEET	OF	SHEETS	STA.	TO STA.				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				IL ROUTE 83	63RD STREET	80% NHPP/20% STATE	90% HSIP/5% STATE/	50% WILLOWBROOK/
				90% HSIP/10% STATE	80% NHPP/20% STATE	80% NHPP/20% STATE	5% DUPAGE CO	50% TRISTATE FPD
				ROADWAY	ROADWAY	BOX CULVERT	TRAFFIC SIGNALS	EVP
URBAN	URBAN	S.N. 022-0563	URBAN	URBAN				
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1		1			
60260100	INLETS TO BE ADJUSTED	EACH	3		3			
60261540	INLETS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE	EACH	1		1			
60500040	REMOVING MANHOLES	EACH	1	1				
60500050	REMOVING CATCH BASINS	EACH	11	8	3			
60500060	REMOVING INLETS	EACH	7	6	1			
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	2402	1725	677			
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	2103	2103				
60620000	CONCRETE MEDIAN, TYPE SB-6.24	SQ FT	3628	3628				
60623745	CONCRETE MEDIAN TRANSITION	SQ FT	256	256				
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	1200	800	400			
* 63000025	STEEL PLATE BEAM GUARDRAIL, ATTACHED TO STRUCTURES	FOOT	51			51		
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	2	2				
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	1	1				

\* - DENOTES SPECIALTY ITEM

FILE NAME : H:\2290\7290.26 - IL83 and 63rd St\CA00	USER NAME : rgo11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CA00_Sheet1-DIGW55-ant-800.dgn	DRAWN -	REVISED -	344						(544 & 544-1)	TS&N (13)	DUPAGE	129	10
PLOT SCALE = 2.0000 / in.	CHECKED -	REVISED -	SCALE: SHEET OF SHEETS STA. TO STA.					CONTRACT NO. 60W55					
PLOT DATE = 12/17/2014	DATE -	REVISED -	ILLINOIS FED. AID PROJECT										

Rev.



CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				IL ROUTE 83	63RD STREET	80% NHPP/20% STATE	90% HSIP/5% STATE/	50% WILLOWBROOK/
				90% HSIP/10% STATE	80% NHPP/20% STATE	80% NHPP/20% STATE	5% DUPAGE CO	50% TRISTATE FPD
				ROADWAY	ROADWAY	BOX CULVERT	TRAFFIC SIGNALS	EVP
0004	0004	0011	0021	0021				
URBAN	URBAN	S.N. 022-0563	URBAN	URBAN				
70600332	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	11	11				
* 72000100	SIGN PANEL - TYPE 1	SQ FT	108	28	8		72	
* 72000200	SIGN PANEL - TYPE 2	SQ FT	30				30	
* 72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	8	6	2			
* 72900100	METAL POST - TYPE A	FOOT	94	67	27			
* 72900200	METAL POST - TYPE B	FOOT	27	27				
* 78008200	POLYUREA PAVEMENT MARKING TYPE I - LETTERS AND SYMBOLS	SQ FT	365	219	146			
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	11967	7498	4469			
* 78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	1613	1092	521			
* 78008240	POLYUREA PAVEMENT MARKING TYPE I - LINE 8"	FOOT	145	145				
* 78008250	POLYUREA PAVEMENT MARKING TYPE I - LINE 12"	FOOT	112	72	40			
* 78008270	POLYUREA PAVEMENT MARKING TYPE I - LINE 24"	FOOT	150	90	60			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	132	132				
78100200	TEMPORARY RAISED REFLECTIVE PAVEMENT MARKER	EACH	372	360	12			

URBAN

\* - DENOTES SPECIALTY ITEM

FILE NAME : I:\7290\7290.25 - IL83 and 63rd St\CA00	USER NAME : rgn11 CA00_Sheets\016DW55-ent-500.dgn	DESIGNED - DRAWN -	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	PLOT SCALE = 2.0000' / 1" =	CHECKED -	REVISED -						344	(544 & 544-1)	TS&N (13)	DUPAGE	129
	PLOT DATE = 12/17/2014	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	CONTRACT NO. 60W55	
												ILLINOIS FED. AID PROJECT	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				IL ROUTE 83	63RD STREET	80% NHPP/20% STATE	90% HSIP/ 5% STATE/	50% WILLOWBROOK/
				90% HSIP/ 10% STATE	80% NHPP/ 20% STATE	80% NHPP/20% STATE	5% DUPAGE CO	50% TRISTATE FPD
				ROADWAY	ROADWAY	BOX CULVERT	TRAFFIC SIGNALS	EVP
0004	0004	0011	0021	0021				
URBAN	URBAN	S.N. 022-0563	URBAN	URBAN				
* 78100300	REPLACEMENT REFLECTOR	EACH	130	106	24			
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	20	14	6			
* 78200530	BARRIER WALL MARKERS, TYPE C	EACH	20	20				
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	3	3				
78300100	PAVEMENT MARKING REMOVAL	SQ FT	20498	19449	1049			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	432	408	24			
* 80500020	SERVICE INSTALLATION - POLE MOUNTED	EACH	1				1	
* 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	1593				1593	
* 81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	20				20	
* 81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	65				65	
* 81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	684				684	
* 81400100	HANDHOLE	EACH	7				7	
* 81400200	HEAVY-DUTY HANDHOLE	EACH	6				6	
* 81400300	DOUBLE HANDHOLE	EACH	2				2	

\* - DENOTES SPECIALTY ITEM

FILE NAME *	USER NAME = rgal1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
h:\7290\7290.26 - IL83 and 63rd St\CA00	CADD_Sheets\0160W55-shr-500.dgn	DRAWN -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	344	(544 & 544-1) TS&N (13)	DUPAGE	129	13
Default	PLOT SCALE * 2.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60W55										
	PLOT DATE * 12/17/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT										

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				IL ROUTE 83	63RD STREET	80% NHPP/20% STATE	90% HSIP/ 5% STATE/	50% WILLOWBROOK/
				90% HSIP/ 10% STATE	80% NHPP/ 20% STATE	80% NHPP/20% STATE	5% DUPAGE CO	50% TRISTATE FPD
				ROADWAY	ROADWAY	BOX CULVERT	TRAFFIC SIGNALS	EVP
URBAN	URBAN	S.N. 022-0563	URBAN	URBAN				
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2				2	
* 86400100	TRANSCEIVER - FIBER OPTIC	EACH	1				1	
* 87300925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	3343				3343	
* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	484				484	
* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1714				718	996
* 87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	3186				3186	
* 87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1664				1664	
* 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	4342				4342	
* 87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	95				95	
* 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	810				810	
* 87502440	TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	1				1	
* 87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	1				1	
* 87700220	STEEL MAST ARM ASSEMBLY AND POLE, 36 FT.	EACH	1				1	
* 87700250	STEEL MAST ARM ASSEMBLY AND POLE, 42 FT.	EACH	1				1	

\* - DENOTES SPECIALTY ITEM

FILE NAME : I:\7290\7290.26 - IL03 and 63rd St\CADD	USER NAME : rge11 CADD_Sheets\DIGW55-akt-500.dgn	DESIGNED - DRAWN -	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>					F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 2,0000' / 1" =	CHECKED -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	344	(544 & 544-1) TS&N (13)	DUPAGE	129	14
Default	PLOT DATE = 12/17/2014	DATE -	REVISED -		CONTRACT NO. 60W55										
										ILLINOIS FED. AID PROJECT					

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				IL ROUTE 83	63RD STREET	80% NHPP/20% STATE	90% HSIP/5% STATE/	50% WILLOWBROOK/
				90% HSIP/10% STATE	80% NHPP/20% STATE	80% NHPP/20% STATE	5% DUPAGE CO	50% TRISTATE FPD
				ROADWAY	ROADWAY	BOX CULVERT	TRAFFIC SIGNALS	EVP
0004	0004	0011	0021	0021				
URBAN	URBAN	S.N. 022-0563	URBAN	URBAN				
* 87700310	STEEL MAST ARM ASSEMBLY AND POLE, 54 FT.	EACH	1				1	
* 87700320	STEEL MAST ARM ASSEMBLY AND POLE, 55 FT.	EACH	1				1	
* 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	8				8	
* 87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4				4	
* 87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	54				54	
* 87900200	DRILL EXISTING HANDHOLE	EACH	1				1	
* 88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	10				10	
* 88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4				4	
* 88030240	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	4				4	
* 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2				2	
* 88102747	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	1				1	
* 88200210	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	14				14	
* 88500100	INDUCTIVE LOOP DETECTOR	EACH	14				14	
* 88600100	DETECTOR LOOP, TYPE I	FOOT	514				514	

URBAN

\* - DENOTES SPECIALTY ITEM

FILE NAME: I:\7290\7290.26 - IL83 and 63rd St\CAD00	USER NAME: regill	DESIGNED: -	REVISED: -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CADD: Sheets\0160W55-ehc-500.dgn	DRAWN: -	REVISED: -	344						(544 & 544-1)	TS&N (13)	DUPAGE	129	15
PLOT SCALE: 2.0000' / 1" =	CHECKED: -	REVISED: -	SCALE: SHEET OF SHEETS STA. TO STA.					CONTRACT NO. 60W55					
PLOT DATE: 12/17/2014	DATE: -	REVISED: -	ILLINOIS FED. AID PROJECT										

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				IL ROUTE 83	63RD STREET	80% NHPP/20% STATE	90% HSIP/5% STATE/	50% WILLOWBROOK/
				90% HSIP/10% STATE	80% NHPP/20% STATE	80% NHPP/20% STATE	5% DUPAGE CO	50% TRISTATE FPD
				ROADWAY	ROADWAY	BOX CULVERT	TRAFFIC SIGNALS	EVP
0004	0004	0011	0021	0021				
URBAN	URBAN	S.N. 022-0563	URBAN	URBAN				
* 88600700	PREFORMED DETECTOR LOOP	FOOT	553				553	
* 88800100	PEDESTRIAN PUSH-BUTTON	EACH	3				3	
* 89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1				1	
* 89501400	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT	EACH	4					4
* 89501410	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT	EACH	1					1
* 89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	5680				5680	
* 89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1				1	
* 89502380	REMOVE EXISTING HANDHOLE	EACH	15				15	
* 89502382	REMOVE EXISTING DOUBLE HANDHOLE	EACH	1				1	
* 89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	7				7	
X0323344	VANE DRAINS	FOOT	153	78	75			
* X0324085	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	996					996
X0325318	LIGHTWEIGHT CELLULAR CONCRETE FILL	CU YD	323			323		
X0325391	EXPANDED POLYSTYRENE FILL	CU YD	1215		1215			

URBAN

\* - DENOTES SPECIALTY ITEM

FILE NAME: I:\729017290.26 - IL83 and 63rd St\CAD00	USER NAME: r.gall	DESIGNED: -	REVISED: -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
CADD SHEETS: 0160W55-4ht-500.dgn	DRAWN: -	REVISED: -	SCALE: 1" = 40'		SHEET	OF	SHEETS	STA.	TO STA.	344	(544 & 544-1) T5&N (13)	DUPAGE	129	16
PLOT SCALE: 2.0000' / 1"	CHECKED: -	REVISED: -												
PLOT DATE: 12/17/2014	DATE: -	REVISED: -												
ILLINOIS FED. AID PROJECT														



CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				IL ROUTE 83	63RD STREET	80% NHPP/20% STATE	90% HSIP/5% STATE/	50% WILLOWBROOK/
				90% HSIP/10% STATE	80% NHPP/20% STATE	80% NHPP/20% STATE	5% DUPAGE CO	50% TRISTATE FPD
				ROADWAY	ROADWAY	BOX CULVERT	TRAFFIC SIGNALS	EVP
URBAN	URBAN	S.N. 022-0563	URBAN	URBAN				
X0325734	SLOTTED DRAIN REMOVAL	FOOT	70	60	10			
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	6	6				
X6430120	REMOVE IMPACT ATTENUATORS, NO SALVAGE	EACH	1	1				
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1				
X7030025	WET REFLECTIVE TEMPORARY TAPE, TYPE III - LETTERS AND SYMBOLS	SQ FT	1033	960	73			
X7030030	WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH	FOOT	39951	36796	3155			
X7030040	WET REFLECTIVE TEMPORARY TAPE TYPE III, 6 INCH	FOOT	10115	8964	1151			
X7030050	WET REFLECTIVE TEMPORARY TAPE TYPE III, 12 INCH	FOOT	40		40			
X7030055	WET REFLECTIVE TEMPORARY TAPE TYPE III, 24 INCH	FOOT	88	50	38			
X7040125	PINNING TEMPORARY CONCRETE BARRIER	EACH	261	261				
X7810300	RECESSED REFLECTIVE PAVEMENT MARKER	EACH	32		32			
* X8100105	CONDUIT SPLICE	EACH	1				1	
* X8570226	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1				1	
* X8620200	UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1				1	

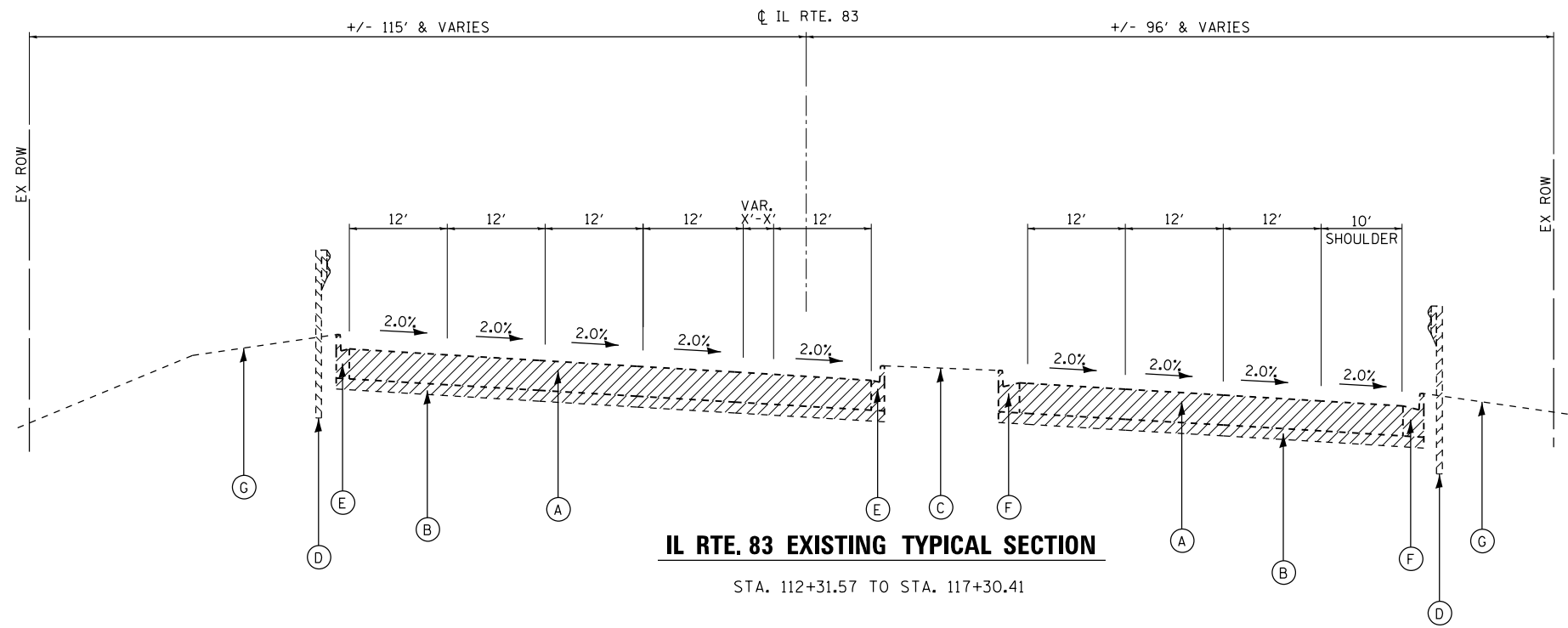
\* - DENOTES SPECIALTY ITEM

FILE NAME : I:\7290\7290.20 - IL83 and 63rd St\CADD	USER NAME : rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CADD SHEETS:DIR055-shr-500.dgn	DRAWN -	REVISED -	344						1544 & 544-11 TS&N (13)	DUPAGE	129	17	
PLOT SCALE * 2.0000' / 1"	CHECKED -	REVISED -	SCALE: SHEET OF SHEETS STA. TO STA.					CONTRACT NO. 60W55					
PLOT DATE * 12/17/2014	DATE -	REVISED -	ILLINOIS FED. AID PROJECT										

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				IL ROUTE 83	63RD STREET	80% NHPP/20% STATE	90% HSIP/5% STATE/	50% WILLOWBROOK/
				90% HSIP/10% STATE	80% NHPP/20% STATE	BOX CULVERT	5% DUPAGE CO	50% TRISTATE FPD
				ROADWAY	ROADWAY	0011	TRAFFIC SIGNALS	EVP
0004	0004		0021	0021				
URBAN	URBAN	S.N. 022-0563	URBAN	URBAN				
* X8710024	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	3395				3395	
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1				
Z0018700	DRAINAGE STRUCTURE TO BE REMOVED	EACH	1	1				
Z0024478	FLEXIBLE DELINEATORS	EACH	12	12				
Z0028462	GEOTEXTILE RETAINING WALL	SQ FT	152			152		
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	116	64	52			
* Z0033056	OPTIMIZE TRAFFIC SIGNAL SYSTEM	EACH	1				1	
Z0062456	TEMPORARY PAVEMENT	SQ YD	2519	2519				
Z0073002	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	1114	63		1051		
Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	2				2	

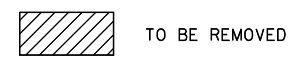
\* - DENOTES SPECIALTY ITEM

FILE NAME : 1\7298\7298.26 - IL83 and 63rd St\CA00	USER NAME : rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>				F.A.P. R.T.E. :	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	CADD Sheets\0160W55-ent-500.dgn	DRAWN -	REVISED -						349	(544 & 544-1) T&N (13)	DUPAGE	129	18
	PLOT SCALE : 2.0000 ' / in.	CHECKED -	REVISED -						CONTRACT NO. 60W55				
Default	PLOT DATE : 12/17/2014	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TD	STA.	ILLINOIS FED. AID PROJECT	



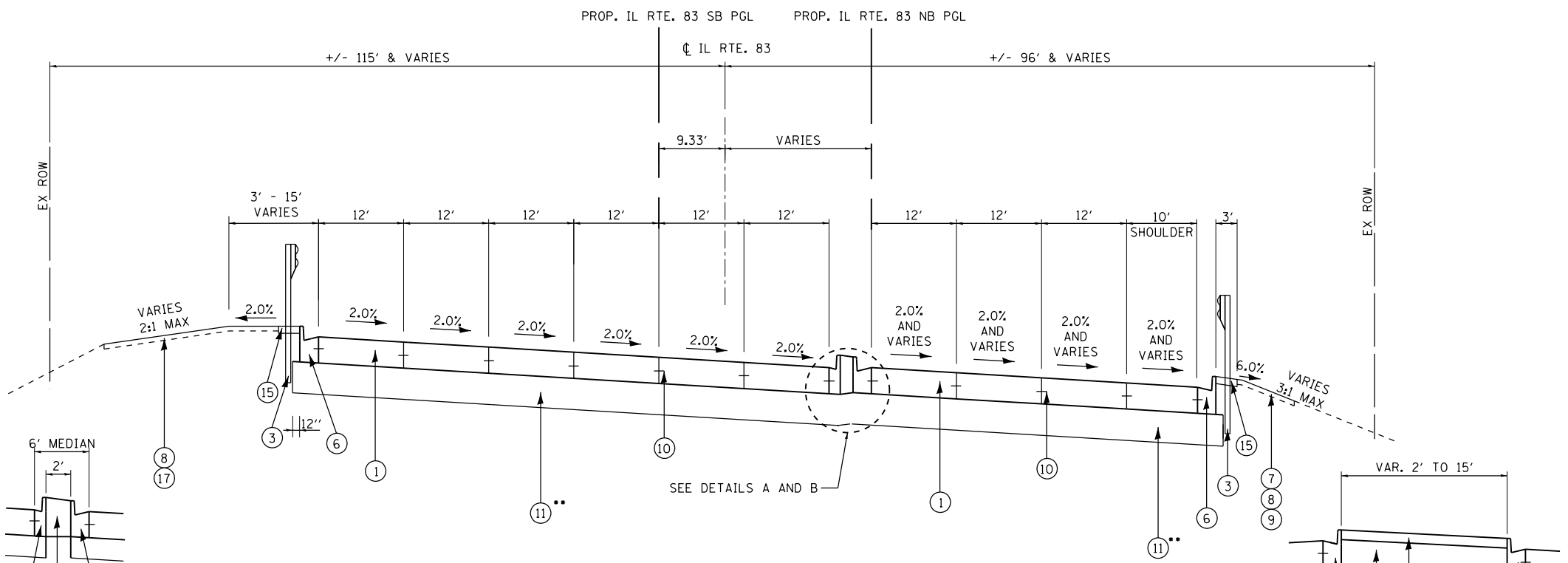
**IL RTE. 83 EXISTING TYPICAL SECTION**  
 STA. 112+31.57 TO STA. 117+30.41

- EXISTING LEGEND:**
- (A) EXISTING PCC PAVEMENT, 9 INCH (JOINTED)
  - (B) EXISTING SUBBASE GRANULAR MATERIAL, TYPE A
  - (C) EXISTING GRASS MEDIAN
  - (D) EXISTING GUARDRAIL (SEE PLANS FOR LOCATION)
  - (E) EXISTING CURB & GUTTER, B-6.12
  - (F) EXISTING CURB & GUTTER, B-6.24
  - (G) EXISTING GROUND
  - (H) TRAFFIC BARRIER

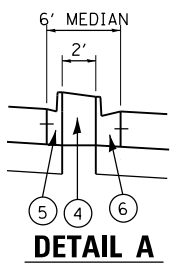


- PROPOSED LEGEND:**
- (1) PCC PAVEMENT, 10-1/4" (JOINTED)
  - (2) NOT USED
  - (3) STEEL PLATE BEAM GUARDRAIL (SEE PLANS FOR LOCATION)
  - (4) CONCRETE MEDIAN, TYPE SB-6.24
  - (5) COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.12
  - (6) COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.24
  - (7) HEAVY DUTY EROSION CONTROL BLANKET
  - (8) TOPSOIL FURNISH AND PLACE, 4"
  - (9) SEEDING, CLASS 3
  - (10) TIE BAR, NO. 6 X 30 @ 24 INCH SPACING
  - (11) AGGREGATE SUBGRADE IMPROVEMENT, 12 INCH
  - (12) CONCRETE MEDIAN SURFACE, 4"
  - (13) AGGREGATE FILL
  - (14) CONCRETE BARRIER, DOUBLE FACE, 42 IN. HEIGHT
  - (15) HMA SHOULDER, 6"
  - (16) CONCRETE BARRIER BASE
  - (17) SODDING, SALT TOLERANT
  - (18) AGGREGATE SUBGRADE IMPROVEMENT (VARIABLE DEPTH)

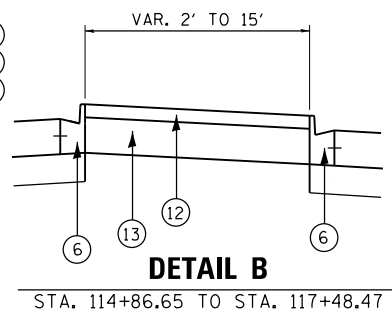
- NOTES:**
1. AGGREGATE FILL SHALL BE OF THE GRADATION SPECIFIED IN ART 606.09 OF THE STANDARD SPECIFICATIONS AND SHALL BE PLACED IN LAYERS 4 IN THICK AND COMPACTED. THE COST OF THE AGGREGATE FILL SHALL BE INCLUDED IN THE COST OF CONCRETE MEDIAN SURFACE, 4 IN.
  2. ALL TIE BARS, EXCLUSIVE OF THOSE TIED TO CURB AND GUTTER SHALL BE INCIDENTAL TO THE COST OF PCC PAVEMENT, 10-1/4" (JOINTED).
  3. TIE BARS USED TO TIE PROP. CURB AND GUTTER INTO EXIST. PCC PAVEMENT, TIE BARS USED TO TIE PROP. JOINTED PCC PAVEMENT INTO PROP. CURB AND GUTTER SHALL BE INCIDENTAL TO THE COST OF COMBINATION CONCRETE CURB AND GUTTER, OF THE TYPE SPECIFIED.



**IL RTE. 83 PROPOSED TYPICAL SECTION**  
 STA. 112+31.57 TO STA. 117+48.47

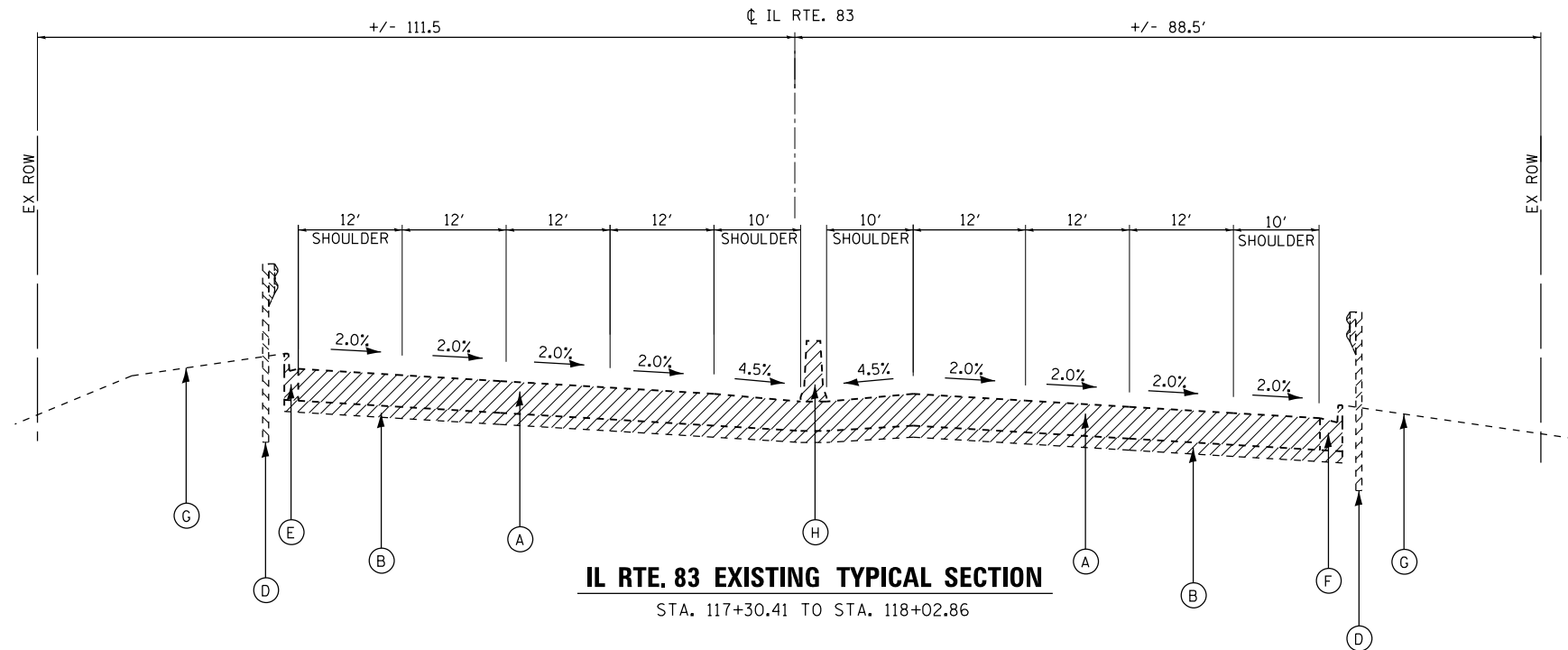


**DETAIL A**  
 STA. 112+35.86 TO 114+86.65  
 6' CONCRETE MEDIAN TO BE PAID FOR AS CONC MED TSB6.24



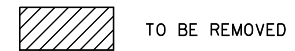
**DETAIL B**  
 STA. 114+86.65 TO STA. 117+48.47

FILE NAME = I:\7290\7290.26 - IL83 and 63-d St\CADD	USER NAME = rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 83 AT MARION HILLS DITCH TYPICAL SECTIONS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CADD Sheets\DI60W55-sht-typical-83.dgn	DRAWN -	REVISED -	344			(544 & 544-1) TS&N (13)	DUPAGE	129	19	
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 60W55							
Default	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							



**EXISTING LEGEND:**

- (A) EXISTING PCC PAVEMENT, 9 INCH (JOINTED)
- (B) EXISTING SUBBASE GRANULAR MATERIAL, TYPE A
- (C) EXISTING GRASS MEDIAN
- (D) EXISTING GUARDRAIL (SEE PLANS FOR LOCATION)
- (E) EXISTING CURB & GUTTER, B-6.12
- (F) EXISTING CURB & GUTTER, B-6.24
- (G) EXISTING GROUND
- (H) TRAFFIC BARRIER

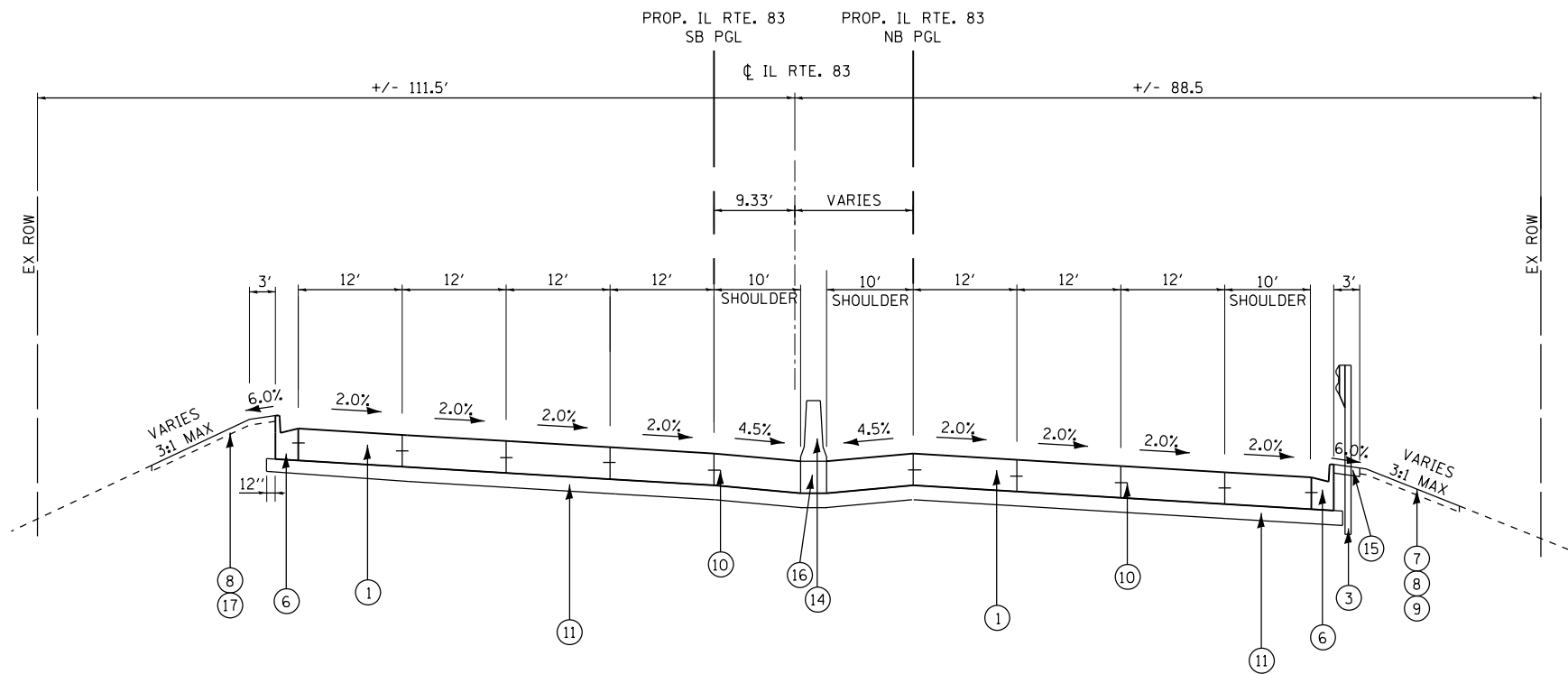


**PROPOSED LEGEND:**

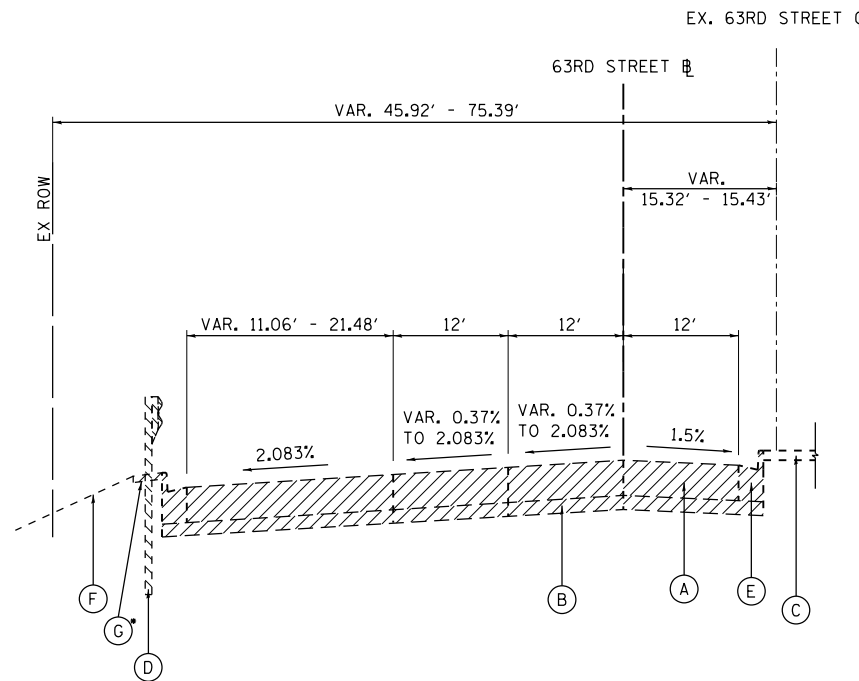
- (1) PCC PAVEMENT, 10-1/4" (JOINTED)
- (2) NOT USED
- (3) STEEL PLATE BEAM GUARDRAIL (SEE PLANS FOR LOCATION)
- (4) CONCRETE MEDIAN, TYPE SB-6.24
- (5) COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.12
- (6) COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.24
- (7) HEAVY DUTY EROSION CONTROL BLANKET
- (8) TOPSOIL FURNISH AND PLACE, 4"
- (9) SEEDING, CLASS 3
- (10) TIE BAR, NO. 6 X 30 @ 24 INCH SPACING
- (11) AGGREGATE SUBGRADE IMPROVEMENT, 12 INCH
- (12) CONCRETE MEDIAN SURFACE, 4"
- (13) AGGREGATE FILL
- (14) CONCRETE BARRIER, DOUBLE FACE, 42 IN. HEIGHT
- (15) HMA SHOULDER, 6"
- (16) CONCRETE BARRIER BASE
- (17) SODDING, SALT TOLERANT

**NOTES:**

1. AGGREGATE FILL SHALL BE OF THE GRADATION SPECIFIED IN ART 606.09 OF THE STANDARD SPECIFICATIONS AND SHALL BE PLACED IN LAYERS 4 IN THICK AND COMPACTED. THE COST OF THE AGGREGATE FILL SHALL BE INCLUDED IN THE COST OF CONCRETE MEDIAN SURFACE, 4 IN.
2. ALL TIE BARS, EXCLUSIVE OF THOSE TIED TO CURB AND GUTTER SHALL BE INCIDENTAL TO THE COST OF PCC PAVEMENT, 10-1/4" (JOINTED).
3. TIE BARS USED TO TIE PROP. CURB AND GUTTER INTO EXIST. PCC PAVEMENT, TIE BARS USED TO TIE PROP. JOINTED PCC PAVEMENT INTO PROP. CURB AND GUTTER SHALL BE INCIDENTAL TO THE COST OF COMBINATION CONCRETE CURB AND GUTTER, OF THE TYPE SPECIFIED.



FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 83 AT MARION HILLS DITCH TYPICAL SECTIONS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	CADD_Sheets\DI60W55-sht-typical-83.dgn	DRAWN -	REVISED -			344	(544 & 544-1) TS&N (13)	DUPAGE	129	20	
	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60W55					
	PLOT DATE = 12/17/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



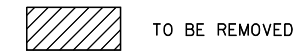
**63RD STREET EXISTING TYPICAL SECTION**

STA. 9+83.89 TO STA. 15+13.84

\* EXISTING HMA SHOULDER (6" DEPTH)  
FROM STA. 9+89.23 TO STA. 13+56.15

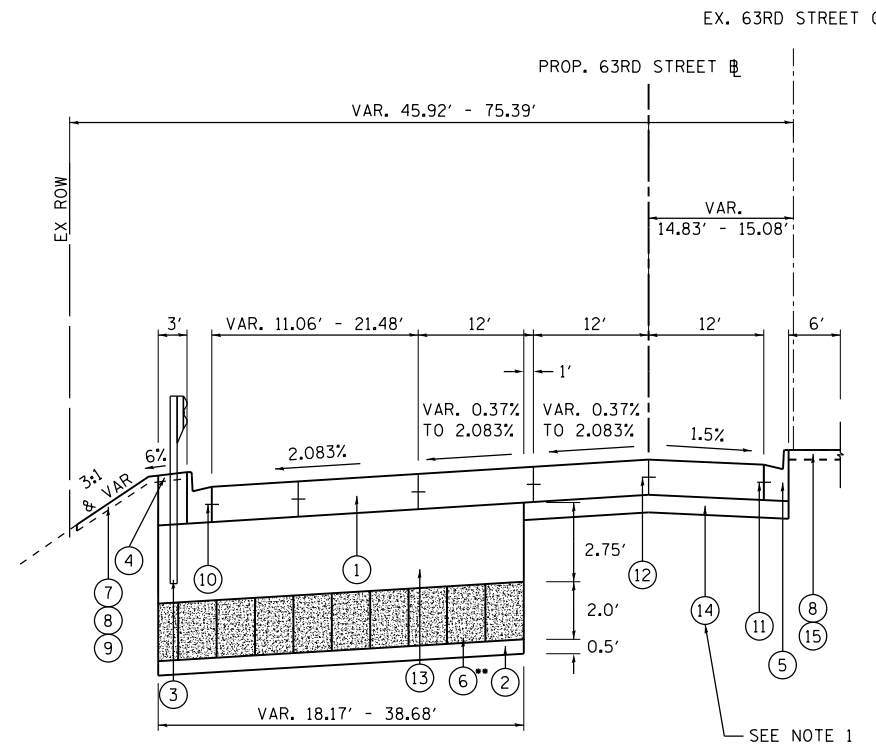
**EXISTING LEGEND:**

- (A) EXISTING PCC PAVEMENT, 9-1/4 INCH (JOINTED)
- (B) EXISTING SUBBASE GRANULAR MATERIAL, TYPE A
- (C) EXISTING GRASS MEDIAN
- (D) EXISTING GUARDRAIL (SEE PLANS FOR LOCATION)
- (E) EXISTING CURB & GUTTER, B-6.24
- (F) EXISTING GROUND
- (G) EXISTING HMA SHOULDER (6" DEPTH)



**PROPOSED LEGEND:**

- (1) PCC PAVEMENT, 9-1/4" (JOINTED)
- (2) SUBBASE GRANULAR, TYPE C (CA-7), 6"
- (3) STEEL PLATE BEAM GUARDRAIL (SEE PLANS FOR LOCATION)
- (4) HMA SHOULDER, 6"
- (5) COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.24
- (6) EXPANDED POLYSTYRENE FILL
- (7) EROSION CONTROL BLANKET
- (8) TOPSOIL FURNISH AND PLACE, 6"
- (9) SEEDING, CLASS 3
- (10) TIE BAR, NO. 4 X 18 @ 24 INCH SPACING
- (11) TIE BAR, NO. 6 X 18 @ 24 INCH SPACING
- (12) TIE BAR, NO. 6 X 30 @ 30 INCH SPACING
- (13) AGGREGATE SUBGRADE IMPROVEMENT (VAR)
- (14) SUBBASE GRANULAR TYPE B (CA-6)
- (15) SODDING, SALT TOLERANT



**63RD STREET PROPOSED TYPICAL SECTION**

STA. 9+84.00 TO STA. 12+00.00  
STA. 12+44.00 TO STA. 12+72.00

\*\* EXPANDED POLYSTYRENE FILL FROM  
STA. 10+00.34 TO STA. 12+00.00 AND  
STA. 12+43.79 TO STA. 12+72.00

**NOTES:**

1. AGGREGATE FILL SHALL BE OF THE GRADATION SPECIFIED IN ART 606.09 OF THE STANDARD SPECIFICATIONS AND SHALL BE PLACED IN LAYERS 4 IN THICK AND COMPACTED. THE COST OF THE AGGREGATE FILL SHALL BE INCLUDED IN THE COST OF CONCRETE MEDIAN SURFACE, 4 IN.
2. ALL TIE BARS, EXCLUSIVE OF THOSE TIED TO CURB AND GUTTER SHALL BE INCIDENTAL TO THE COST OF PCC PAVEMENT, 10-1/4" (JOINTED).
3. TIE BARS USED TO TIE PROP. CURB AND GUTTER INTO EXIST. PCC PAVEMENT, TIE BARS USED TO TIE PROP. JOINTED PCC PAVEMENT INTO PROP. CURB AND GUTTER SHALL BE INCIDENTAL TO THE COST OF COMBINATION CONCRETE CURB AND GUTTER, OF THE TYPE SPECIFIED.

FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 83 AT MARION HILLS DITCH TYPICAL SECTIONS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
I:\7290\7290.26 - IL83 and 63rd St\CADD	CADD_Sheets\DI60W55-sht-typical-63.dgn	DRAWN -	REVISED -			344	(544 & 544-1) TS&N (13)	DUPAGE	129	21	
Default	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -			<b>CONTRACT NO. 60W55</b>					
	PLOT DATE = 12/17/2014	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO

IL ROUTE 83 EARTHWORK SCHEDULE

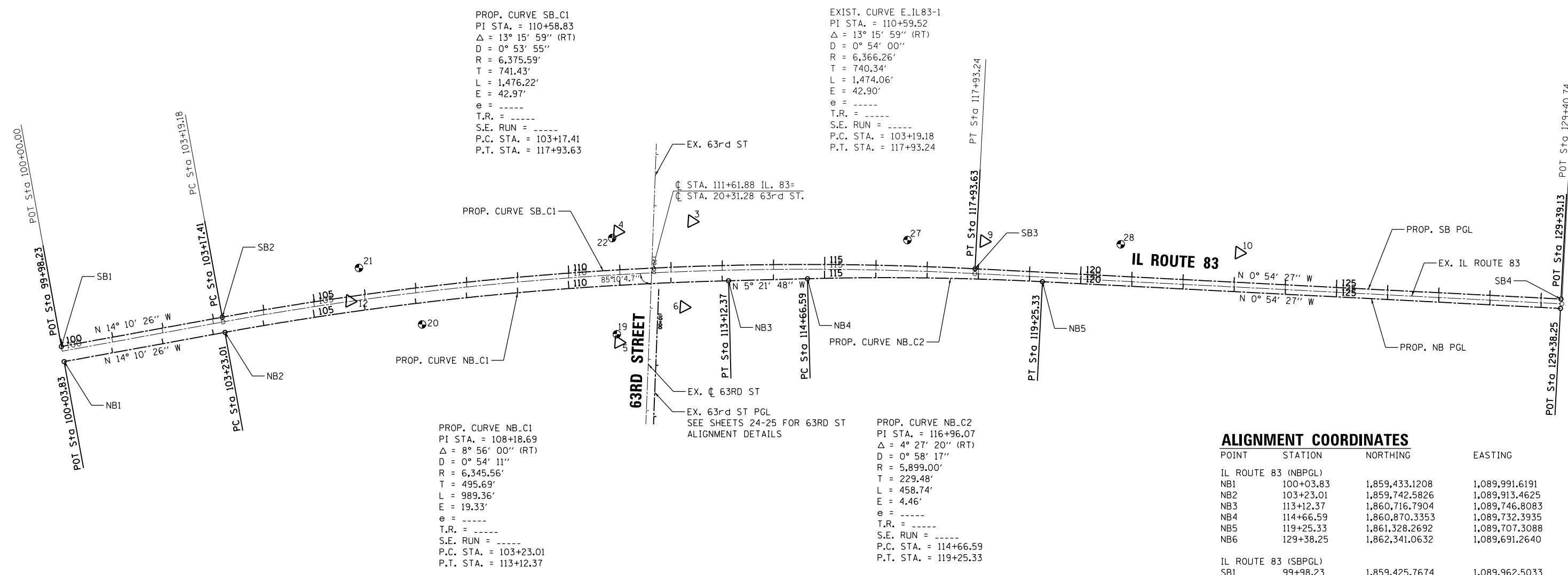
IL ROUTE 83		EARTH EXCAVATION	UNSUITABLE OR UNSTABLE MATERIAL	EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
STA. FROM	TO STA.					
		[CU YD]	[CU YD]	[CU YD]	[CU YD]	[CU YD]
112+50.00	113+00.00	355.45	44.14	302.13	165.79	136.35
113+00.00	113+50.00	334.50	38.97	284.33	131.63	152.69
113+50.00	114+00.00	302.36	30.92	257.00	88.53	168.47
114+00.00	114+50.00	292.29	22.81	248.45	79.60	168.85
114+50.00	115+00.00	345.25	40.81	293.46	202.37	91.09
115+00.00	115+50.00	365.26	57.19	310.47	273.02	37.46
115+50.00	116+00.00	324.81	54.24	276.09	221.48	54.62
116+00.00	116+50.00	314.33	47.29	267.18	163.98	103.21
116+50.00	117+00.00	309.10	47.62	262.74	138.87	123.86
117+00.00	117+50.00	281.31	27.94	239.12	79.48	159.63
117+50.00	118+00.00	267.16	8.61	227.08	29.58	197.50
118+00.00	118+50.00	129.34	6.35	109.94	11.37	98.57
TOTAL		3621.17	426.88	3077.99	1585.70	1492.29

63rd STREET EARTHWORK SCHEDULE

63rd STREET		EARTH EXCAVATION	UNSUITABLE OR UNSTABLE MATERIAL	EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
STA. FROM	TO STA.					
		[CU YD]	[CU YD]	[CU YD]	[CU YD]	[CU YD]
9+50.00	10+00.00	282.49	0.00	240.12	5.91	234.21
10+00.00	10+50.00	306.33	0.00	260.38	33.35	227.03
10+50.00	11+00.00	272.18	0.00	231.35	52.07	179.28
11+00.00	11+50.00	160.89	0.00	136.76	30.62	106.14
11+50.00	12+00.00	179.43	0.00	152.51	7.21	145.30
12+00.00	12+50.00	158.33	0.00	134.58	2.90	131.69
12+50.00	13+00.00	6.47	0.00	5.50	1.73	3.77
13+00.00	13+50.00	3.27	0.00	2.78	0.00	2.78
13+50.00	14+00.00	0.00	0.00	0.00	0.00	0.00
TOTAL		1369.39	0.00	1163.98	133.80	1030.18

MAINTENANCE OF TRAFFIC  
TEMPORARY PAVEMENT EARTHWORK SCHEDULE

IL ROUTE 83 ADDITIONAL EARTHWORK FOR TEMPORARY PAVEMENT		EARTH EXCAVATION	UNSUITABLE OR UNSTABLE MATERIAL	EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
STA. FROM	TO STA.					
		[CU YD]	[CU YD]	[CU YD]	[CU YD]	[CU YD]
112+50.00	113+00.00	4.33	0.00	3.58	4.33	-0.65
113+00.00	113+50.00	10.18	0.00	8.65	10.18	-1.53
113+50.00	114+00.00	8.12	0.00	6.90	8.12	-1.22
114+00.00	114+50.00	2.27	0.00	1.93	2.27	-0.34
114+50.00	115+00.00	0.00	0.00	0.00	0.00	0.00
115+00.00	115+50.00	0.00	0.00	0.00	0.00	0.00
115+50.00	116+00.00	4.74	0.00	4.03	4.74	-0.71
116+00.00	116+50.00	9.05	0.00	7.69	9.05	-1.36
116+50.00	117+00.00	6.03	0.00	5.12	6.03	-0.91
117+00.00	117+50.00	3.38	0.00	2.87	3.38	-0.51
117+50.00	118+00.00	3.24	0.00	2.75	3.24	-0.49
118+00.00	118+50.00	1.57	0.00	1.34	1.57	-0.23
TOTAL		52.91	0.00	44.97	52.91	-7.94



PROP. CURVE SB-C1  
 PI STA. = 110+58.83  
 $\Delta = 13^\circ 15' 59''$  (RT)  
 D =  $0^\circ 53' 55''$   
 R = 6,375.59'  
 T = 741.43'  
 L = 1,476.22'  
 E = 42.97'  
 e = -----  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 103+17.41  
 P.T. STA. = 117+93.63

EXIST. CURVE E\_IL83-1  
 PI STA. = 110+59.52  
 $\Delta = 13^\circ 15' 59''$  (RT)  
 D =  $0^\circ 54' 00''$   
 R = 6,366.26'  
 T = 740.34'  
 L = 1,474.06'  
 E = 42.90'  
 e = -----  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 103+19.18  
 P.T. STA. = 117+93.24

PROP. CURVE NB-C1  
 PI STA. = 108+18.69  
 $\Delta = 8^\circ 56' 00''$  (RT)  
 D =  $0^\circ 54' 11''$   
 R = 6,345.56'  
 T = 495.69'  
 L = 989.36'  
 E = 19.33'  
 e = -----  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 103+23.01  
 P.T. STA. = 113+12.37

PROP. CURVE NB-C2  
 PI STA. = 116+96.07  
 $\Delta = 4^\circ 27' 20''$  (RT)  
 D =  $0^\circ 58' 17''$   
 R = 5,899.00'  
 T = 229.48'  
 L = 458.74'  
 E = 4.46'  
 e = -----  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 114+66.59  
 P.T. STA. = 119+25.33

**ALIGNMENT COORDINATES**

POINT	STATION	NORTHING	EASTING
IL ROUTE 83 (NBPGL)			
NB1	100+03.83	1,859,433.1208	1,089,991.6191
NB2	103+23.01	1,859,742.5826	1,089,913.4625
NB3	113+12.37	1,860,716.7904	1,089,746.8083
NB4	114+66.59	1,860,870.3353	1,089,732.3935
NB5	119+25.33	1,861,328.2692	1,089,707.3088
NB6	129+38.25	1,862,341.0632	1,089,691.2640
IL ROUTE 83 (SBPGL)			
SB1	99+98.23	1,859,425.7674	1,089,962.5033
SB2	103+17.41	1,859,735.2293	1,089,884.3467
SB3	117+93.63	1,861,195.4161	1,089,691.0512
SB4	129+39.13	1,862,340.7724	1,089,672.9064

**BENCHMARKS:**

- SITE BM #19 - ELEVATION=717.581 STA. 110+87.05 115.80' RT CUT SQUARE ON THE NW SIDE OF CONCRETE BASE FOR CROSS WALK SIGNAL @ SE COR. OF 63rd STREET & IL ROUTE 83.
- SITE BM #20 - ELEVATION=717.833 STA. 107+04.73 62.81' RT CUT SQUARE ON THE EAST EDGE OF CONCRETE 25'+/- SOUTH OF END OF GUARDRAIL ON N. BOUND IL ROUTE 83 S. OF 63rd ST.
- SITE BM #21 - ELEVATION=720.976 STA. 105+96.49 62.32' LT CUT SQUARE ON NE COR. OF CONCRETE FOR TRAFFIC SIGNAL CONTROL BOX 70'+/- N. OF ENTRANCE TO HINSDALE LAKE COMMONS WEST SIDE IL ROUTE 83.
- SITE BM #22 - ELEVATION=719.905 STA. 110+89.10 72.07' LT CUT SQUARE ON NE SIDE OF CONCRETE BASE FOR CROSS WALK SIGNAL @ SW COR. OF IL ROUTE 83 & 63rd STREET.
- SITE BM #27 - ELEVATION=720.109 STA. 116+59.73 59.91' LT CUT SQUARE ON THE TOP OF CURB ON THE W. SIDE OF S. BOUND IL ROUTE 83 & 500'+/- N. OF CENTERLINE OF PAVEMENT OF 63rd STREET ON THE S. SIDE OF A DEPRESSED CURB.
- SITE BM #28 - ELEVATION=719.643 STA. 120+74.80 71.58' LT CUT SQUARE ON TOP CENTER OF CONCRETE HEADWALL 830' +/- N. OF 63rd STREET ON THE W. SIDE OF IL ROUTE 83.

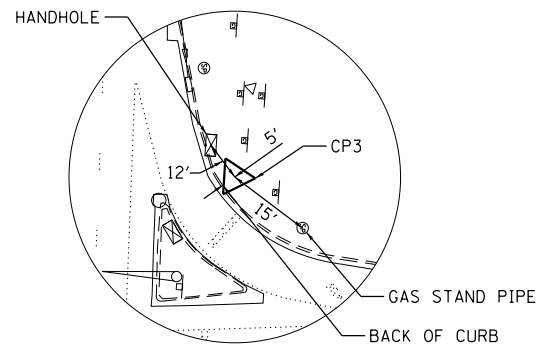
**NOTES:**

- ALL BEARINGS, DISTANCES AND COORDINATES SHOWN HEREON ARE ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83 (2007).
- ALL STATIONING, EXISTING CENTERLINES, SECTION LINES & RIGHT OF WAY LINES WERE ESTABLISHED FROM PLAT OF CENTERLINE FOR IL ROUTE 83 JOB NO. R-91-022-94 RECORDED JULY 26, 2000 AS DOCUMENT NO. R2000-113698.
- VERTICAL DATUM IS NAVD 88 PER N.G.S. FIRST ORDER VERTICAL MONUMENT PID: DK3296 (0135) ELEVATION=(732.180).

**LEGEND:**

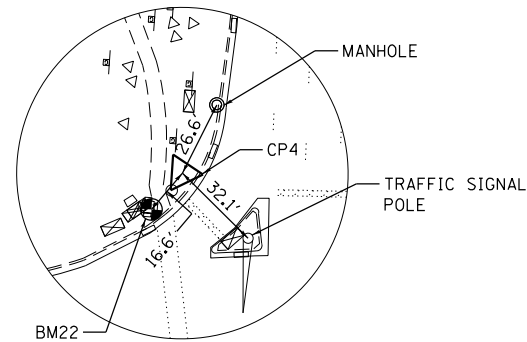
- CONTROL POINT
- BENCHMARK

FILE NAME = I:\7290\7290.26 - IL83 and 63rd St\CADD	USER NAME = rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 83 AT MARION HILLS DITCH ALIGNMENT, TIES, AND BENCHMARKS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	CADD_Sheets\DI60W55-sht-ATB.dgn	DRAWN -	REVISED -			344	(544 & 544-1) TS&N (13)	DUPAGE	129	23	
	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60W55					
	PLOT DATE = 11/26/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



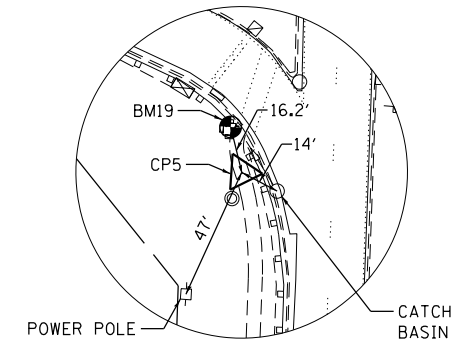
**CONTROL POINT 3**

N: 1,860,637.5889  
 E: 1,089,635.9260  
 ELEV.=719.935  
 SET 5/8" REBAR  
 W/CLAASSEN CAP



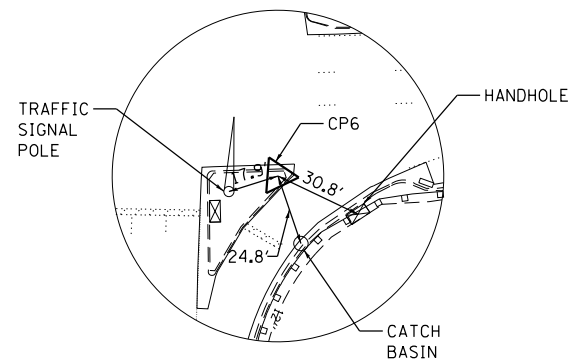
**CONTROL POINT 4**

N: 1,860,494.3555  
 E: 1,089,665.5793  
 ELEV.=719.914  
 SET 5/8" REBAR  
 W/CLAASSEN CAP



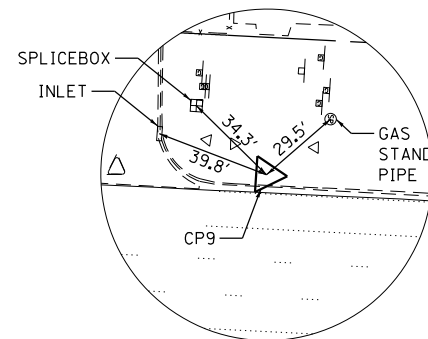
**CONTROL POINT 5**

N: 1,860,511.0504  
 E: 1,089,881.0109  
 ELEV.=717.471  
 FOUND CUT CROSS



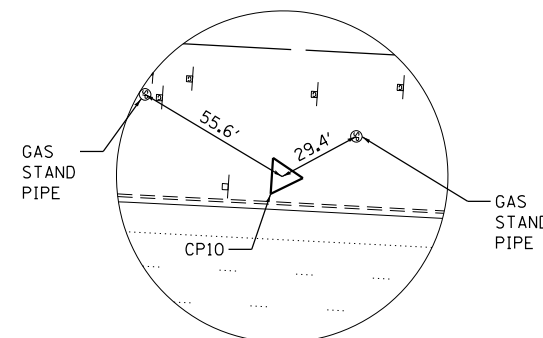
**CONTROL POINT 6**

N: 1,860,633.2435  
 E: 1,089,803.9135  
 ELEV.=718.301  
 SET CUT CROSS



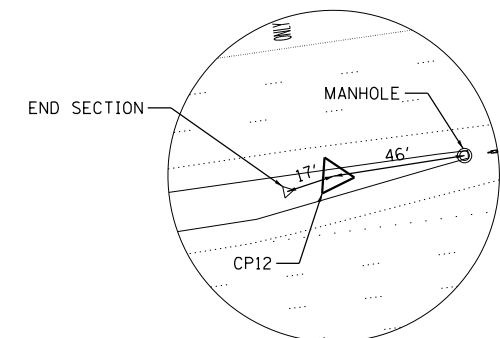
**CONTROL POINT 9**

N: 1,861,209.3196  
 E: 1,089,636.3736  
 ELEV.=720.204  
 SET 5/8" REBAR  
 W/CLAASSEN CAP



**CONTROL POINT 10**

N: 1,861,708.0054  
 E: 1,089,625.2243  
 ELEV.=722.511  
 SET 5/8" REBAR  
 W/CLAASSEN CAP

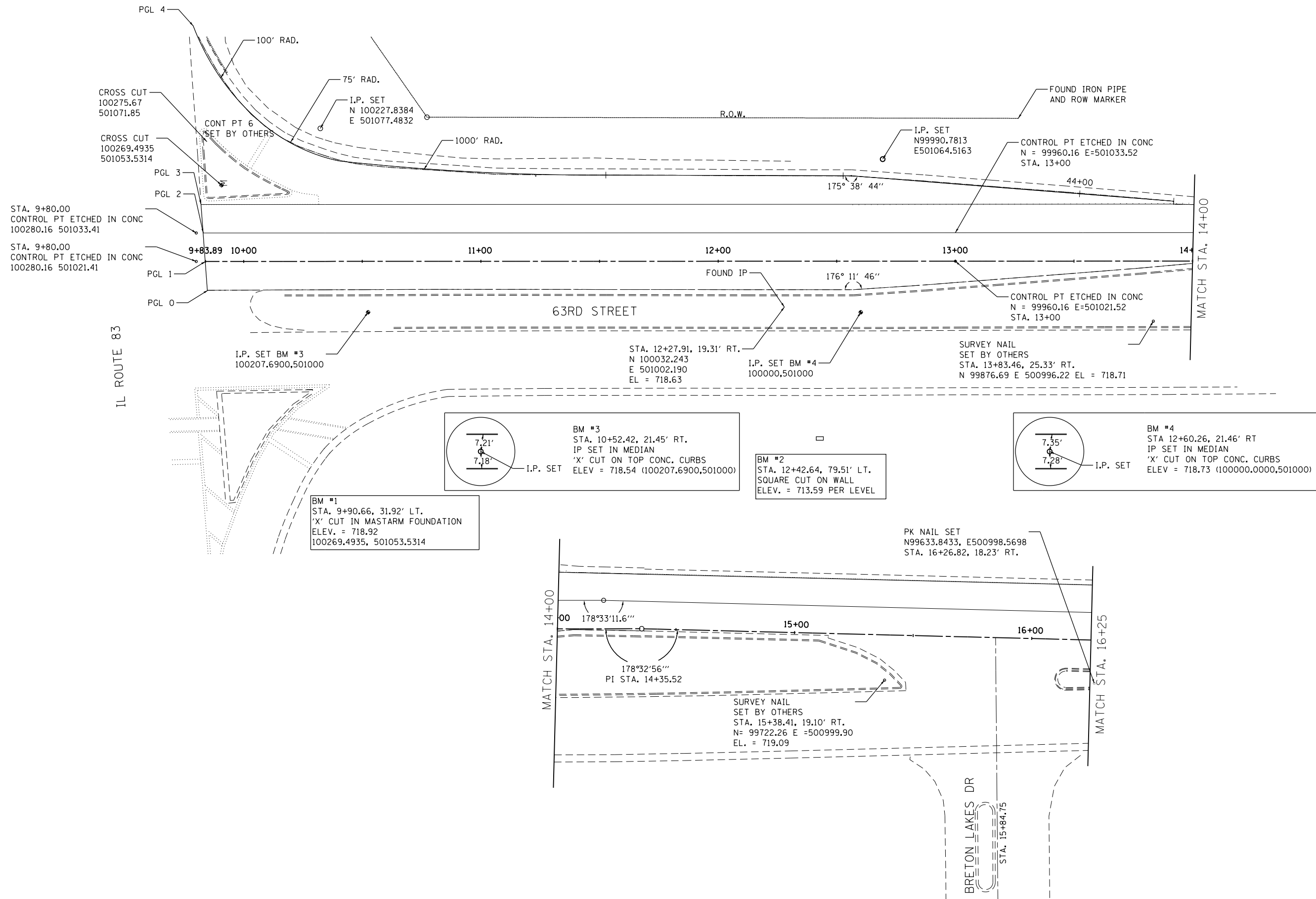


**CONTROL POINT 12**

N: 1,859,981.8165  
 E: 1,089,836.0732  
 ELEV.=719.497  
 SET 5/8" REBAR  
 W/CLAASSEN CAP

FILE NAME = I:\7290\7290.26 - IL83 and 63rd St\CADD	USER NAME = rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 83 AT MARION HILLS DITCH ALIGNMENT, TIES, AND BENCHMARKS</b>	F.A.P. RTE. 344	SECTION (544 & 544-1) TS&N (13)	COUNTY DUPAGE	TOTAL SHEETS 129	SHEET NO. 24		
Default	CADD Sheets\DI60W55-sht-ATB.dgn	DRAWN -	REVISED -			SCALE:	SHEET OF SHEETS	STA. TO STA.	CONTRACT NO. 60W55			
	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -			ILLINOIS FED. AID PROJECT						
	PLOT DATE = 11/26/2014	DATE -	REVISED -									





FILE NAME = I:\7290\7290.26 - IL83 and 63rd St\CADD

USER NAME = rge11  
 CADD\_Sheets\DI60W55-sht-ATB.dgn  
 PLOT SCALE = 200.0000' / in.  
 PLOT DATE = 11/26/2014

DESIGNED -  
 DRAWN -  
 CHECKED -  
 DATE -

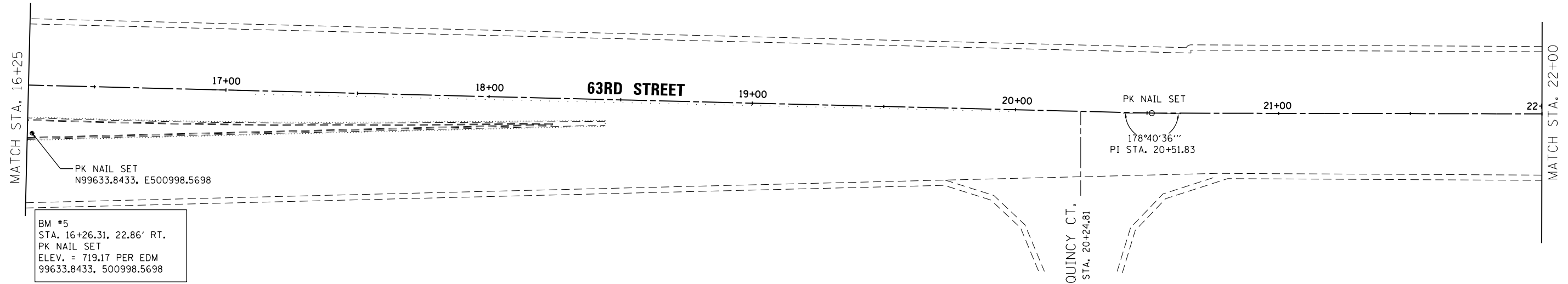
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

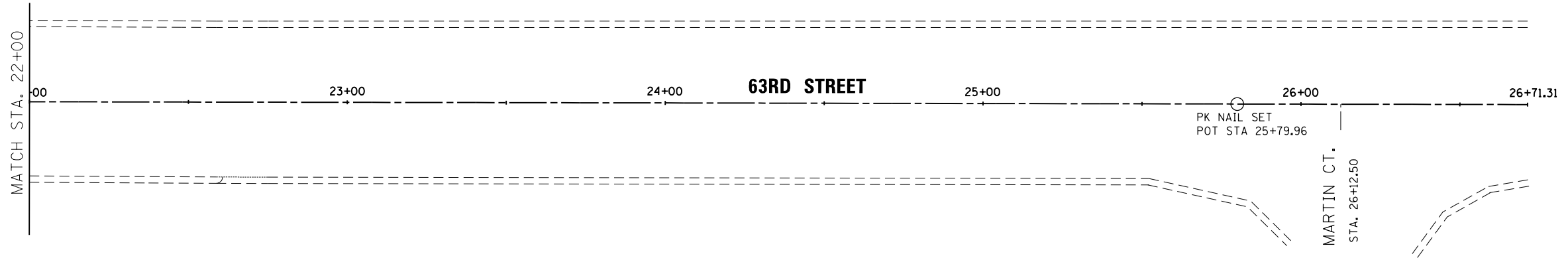
**IL ROUTE 83 AT MARION HILLS DITCH  
 ALIGNMENT, TIES, AND BENCHMARKS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	25
CONTRACT NO. 60W55			ILLINOIS FED. AID PROJECT	



BM #5  
 STA. 16+26.31, 22.86' RT.  
 PK NAIL SET  
 ELEV. = 719.17 PER EDM  
 99633.8433, 500998.5698



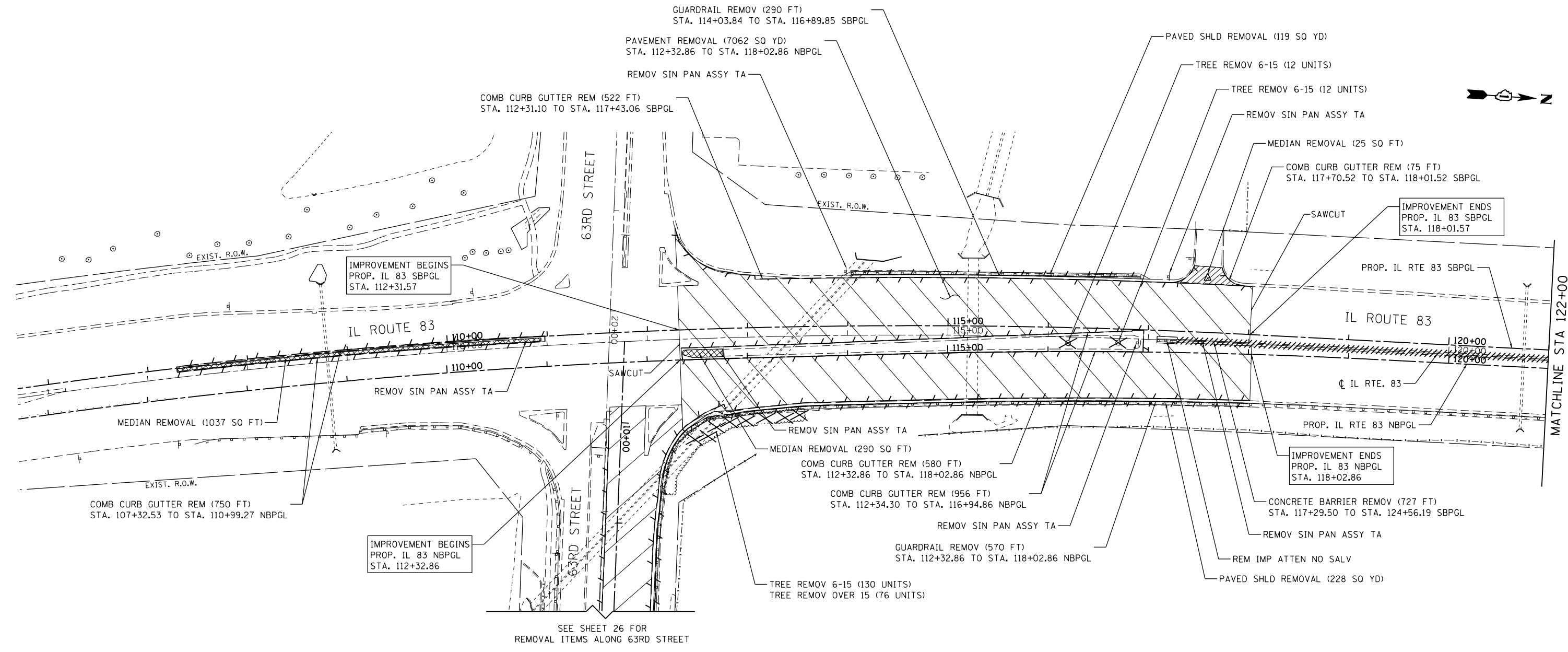
FILE NAME = I:\7290\7290.26 - IL83 and 63rd St\CADD	USER NAME = rge11 CADD_Sheets\DI60W55-sht-ATB.dgn	DESIGNED - DRAWN -	REVISED - REVISED -
Default	PLOT SCALE = 200.0000' / in. PLOT DATE = 11/26/2014	CHECKED - DATE -	REVISED - REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

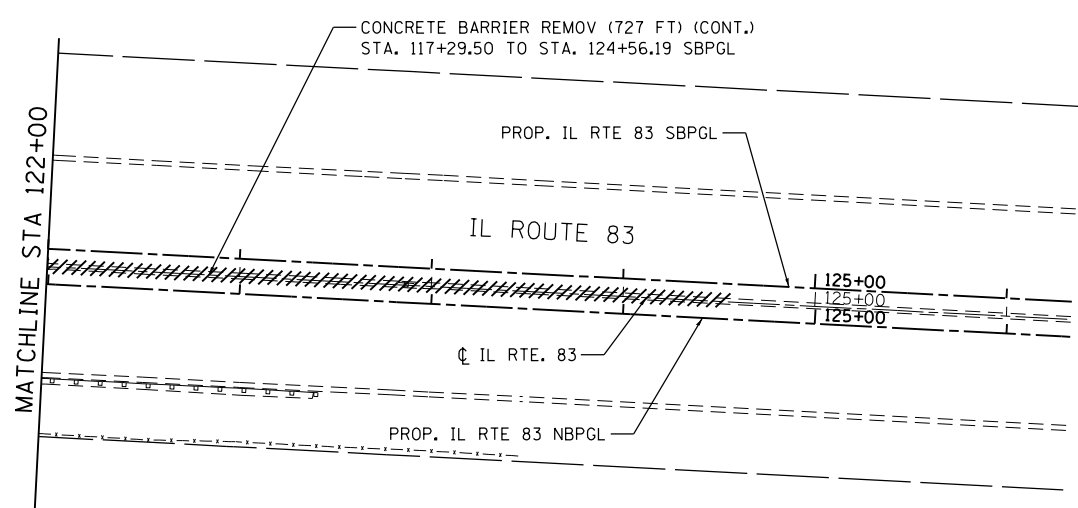
**IL ROUTE 83 AT MARION HILLS DITCH  
 ALIGNMENT, TIES, AND BENCHMARKS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	26
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				



SEE SHEET 26 FOR  
REMOVAL ITEMS ALONG 63RD STREET



**LEGEND:**

	PAVEMENT REMOVAL
	DRIVEWAY PAVEMENT REMOVAL
	MEDIAN REMOVAL
	PAVED SHOULDER REMOVAL
	LINEAR REMOVAL ITEM
	TREE REMOVAL
	BARRIER WALL REMOVAL
	REMOVAL ITEM

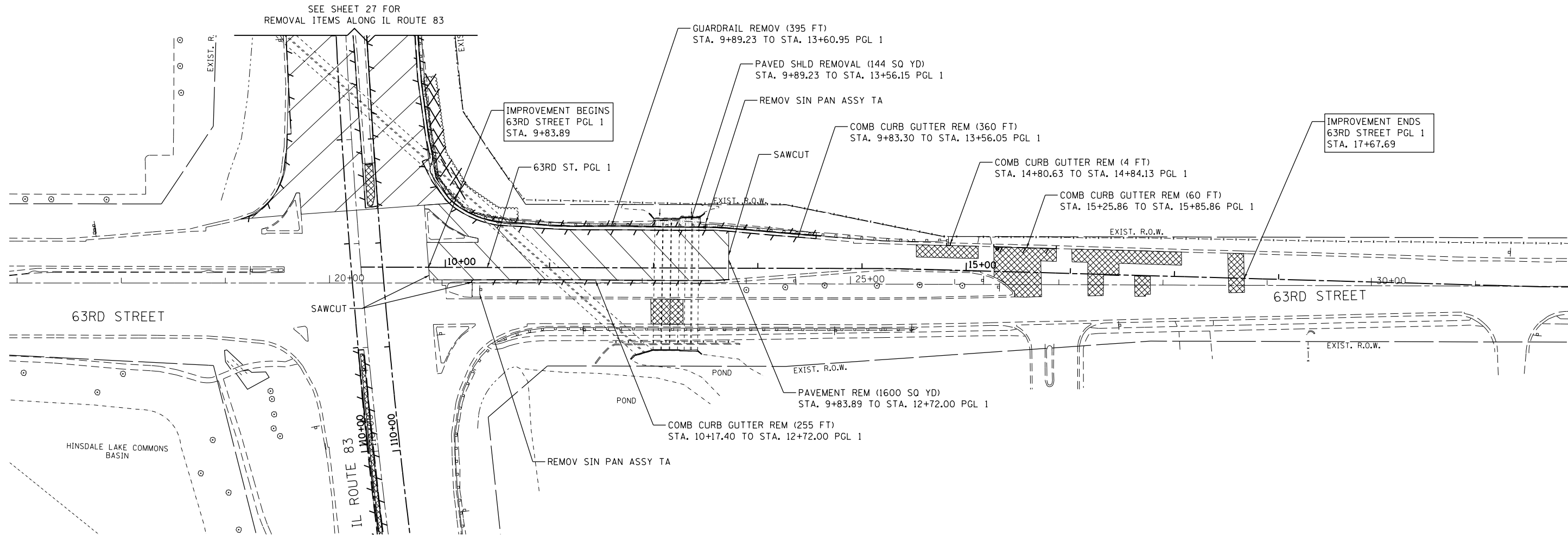
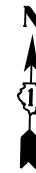
- NOTES:
- FOR EXISTING DRAINAGE REMOVAL ITEMS SEE DRAINAGE AND UTILITIES PLAN (SHEET 56).
  - FOR EXISTING TRAFFIC SIGNAL AND HANDHOLE REMOVAL ITEMS, SEE TRAFFIC SIGNAL PLANS (SHEET 68-91).
  - SAWCUT INCLUDED IN COST OF PAVEMENT REMOVAL.

FILE NAME = I:\7290\7290.26 - IL83 and 63rd St\CADD	USER NAME = rge11	DESIGNED -	REVISED -
CADD_Sheets\DI60W55-sht-rem.dgn		DRAWN -	REVISED -
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PLOT DATE = 11/26/2014		DATE -	REVISED -





**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>IL ROUTE 83 REMOVAL PLAN</b>	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	27
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				



**LEGEND:**

-  PAVEMENT REMOVAL
-  REMOVAL FOR PATCHING (SEE SHEET 31 FOR PATCHING DETAILS)
-  PAVED SHOULDER REMOVAL
-  LINEAR REMOVAL ITEM

**NOTES:**

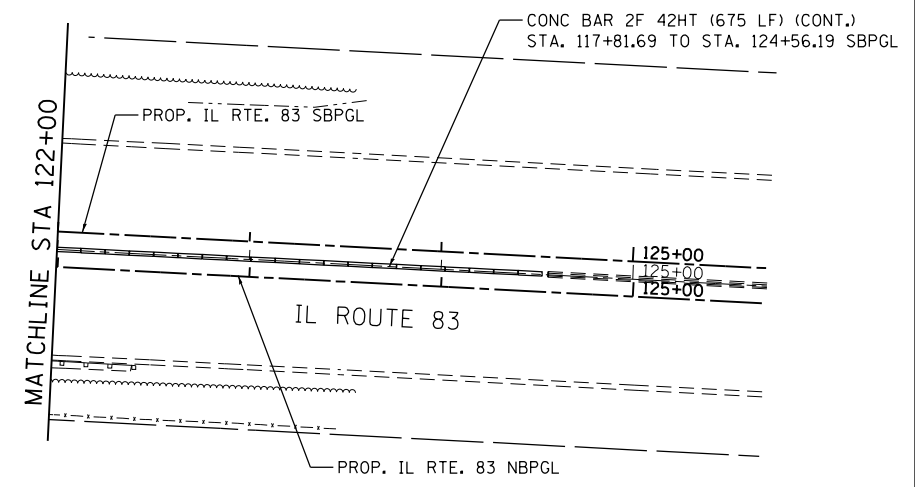
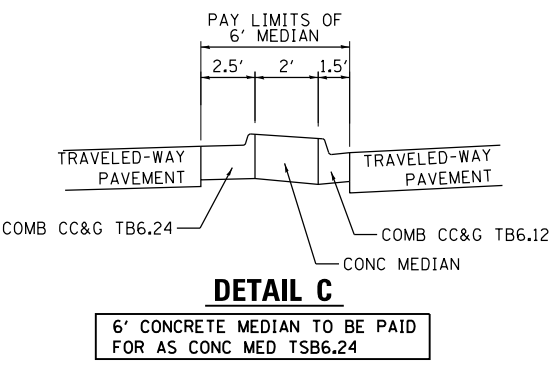
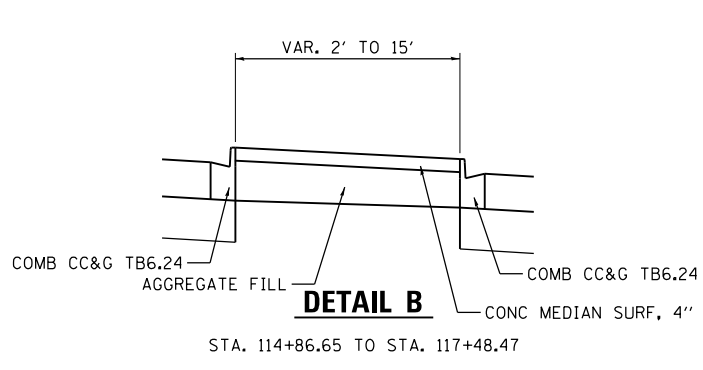
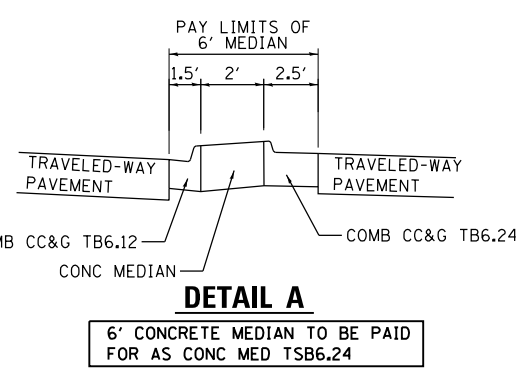
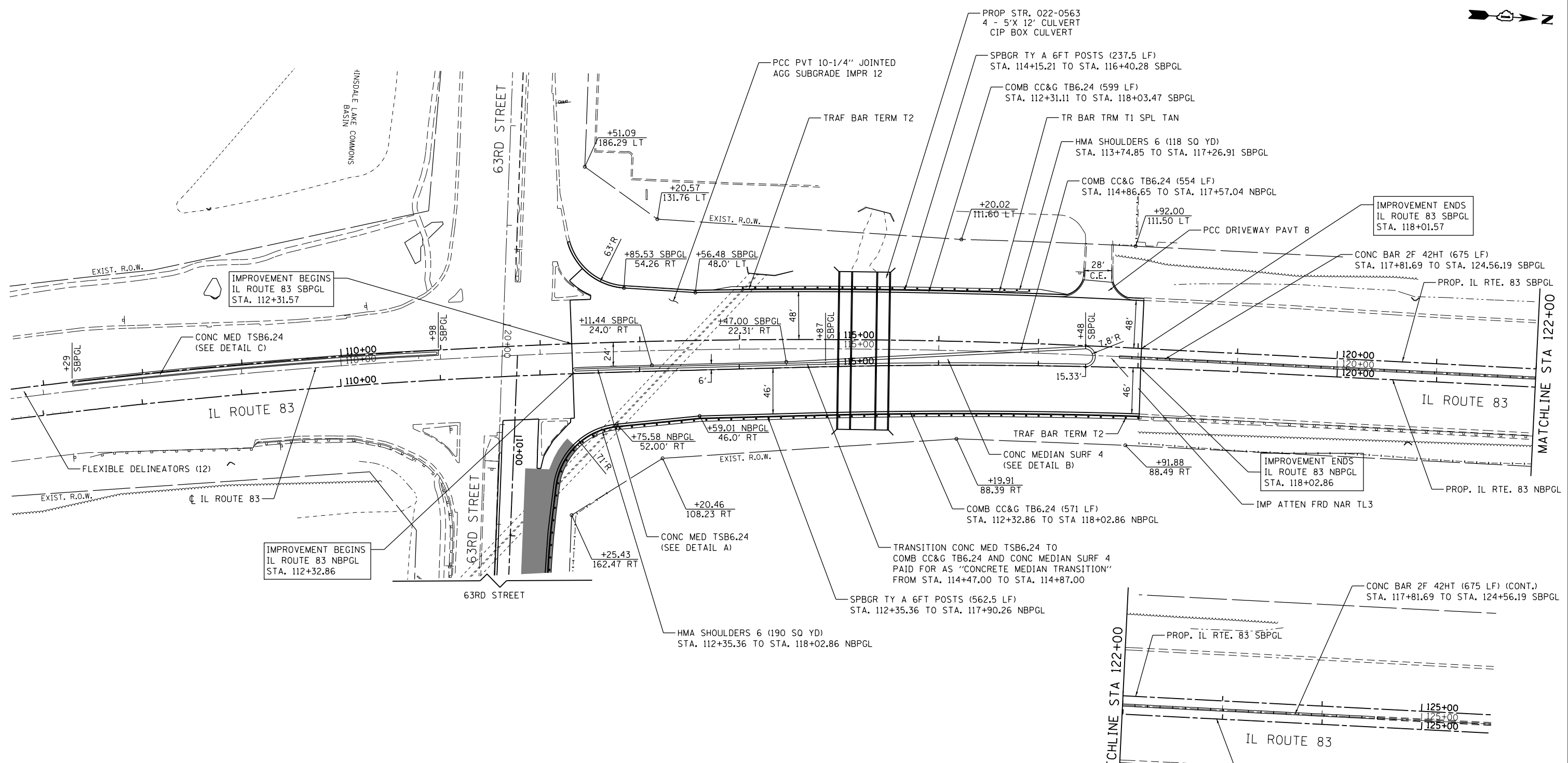
1. FOR EXISTING DRAINAGE REMOVAL ITEMS SEE DRAINAGE AND UTILITIES PLAN (SHEET 57).
2. FOR EXISTING TRAFFIC SIGNAL AND HANDHOLE REMOVAL ITEMS, SEE TRAFFIC SIGNAL PLANS (SHEET 68-91).
3. SAWCUT INCLUDED IN COST OF PAVEMENT REMOVAL.

FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -
I:\7290\7290.26 - IL83 and 63rd St\CADD	CADD_Sheets\DI60W55-sht-rem.dgn	DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 11/26/2014	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>63RD STREET REMOVAL PLAN</b>			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

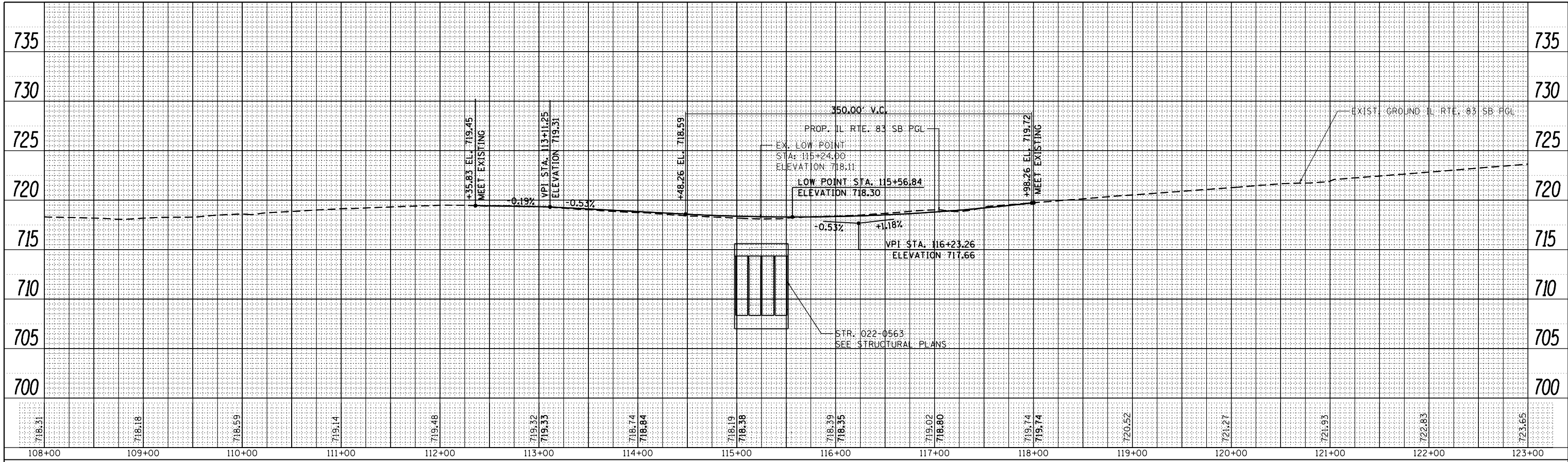
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	28
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				



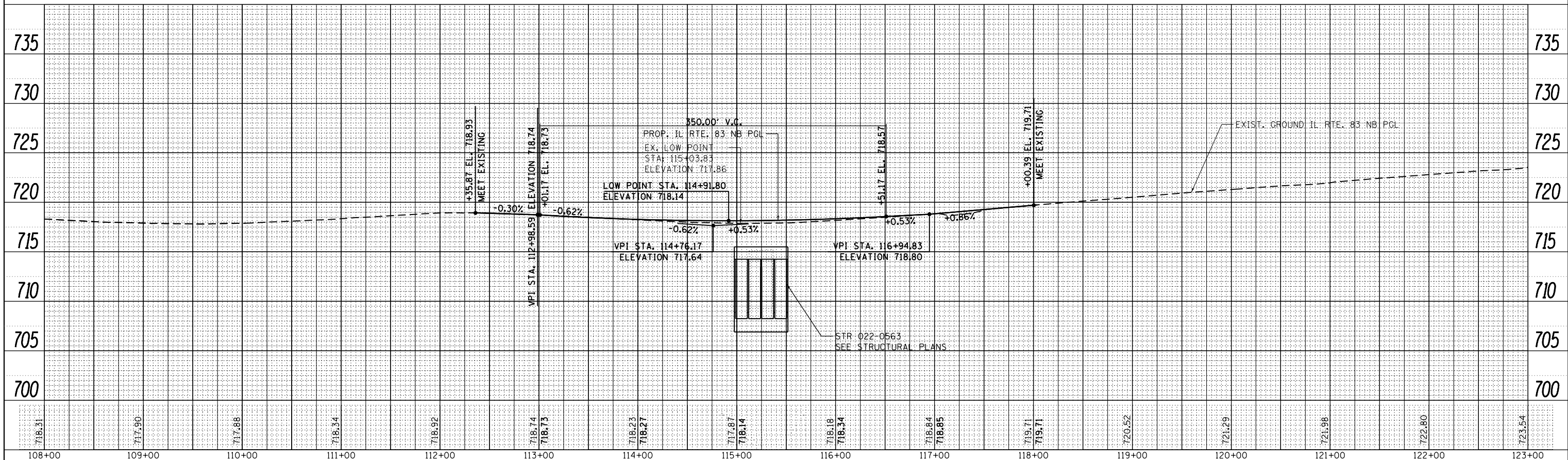
- NOTES:**
- AGGREGATE FILL SHALL BE OF THE GRADATION SPECIFIED IN ART 606.09 OF THE STANDARD SPECIFICATIONS AND SHALL BE PLACED IN LAYERS 4 IN THICK AND COMPACTED. THE COST OF THE AGGREGATE FILL SHALL BE INCLUDED IN THE COST OF CONCRETE MEDIAN SURFACE, 4 IN.
  - WORK THIS SHEET WITH INTERSECTION JOINTING AND DETAILS PLAN.

FILE NAME = I:\7290\7290.26 - IL83 and 63rd St\CADD	USER NAME = rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 83 ROADWAY PLAN</b>			F.A.P. RTE. 344	SECTION (544 & 544-1) TS&N (13)	COUNTY DUPAGE	TOTAL SHEETS 129	SHEET NO. 29
Default	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -		SCALE:	SHEET	OF SHEETS	STA.	TO STA.	CONTRACT NO. 60W55		
	PLOT DATE = 12/17/2014	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									

PROFILE SURVEYED PLOTTED  
 NOTE BOOK NO. \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_  
 DATE \_\_\_\_\_



PROFILE SURVEYED PLOTTED  
 NOTE BOOK NO. \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_  
 DATE \_\_\_\_\_



FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -
\\s:\7290\7290.26 - IL83 and 63rd St\CADD\CADD_Sheets\DI60W55-sht-profile-83.dgn		DRAWN -	REVISED -
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Default	PLOT DATE = 11/26/2014	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83  
 ROADWAY PROFILE**

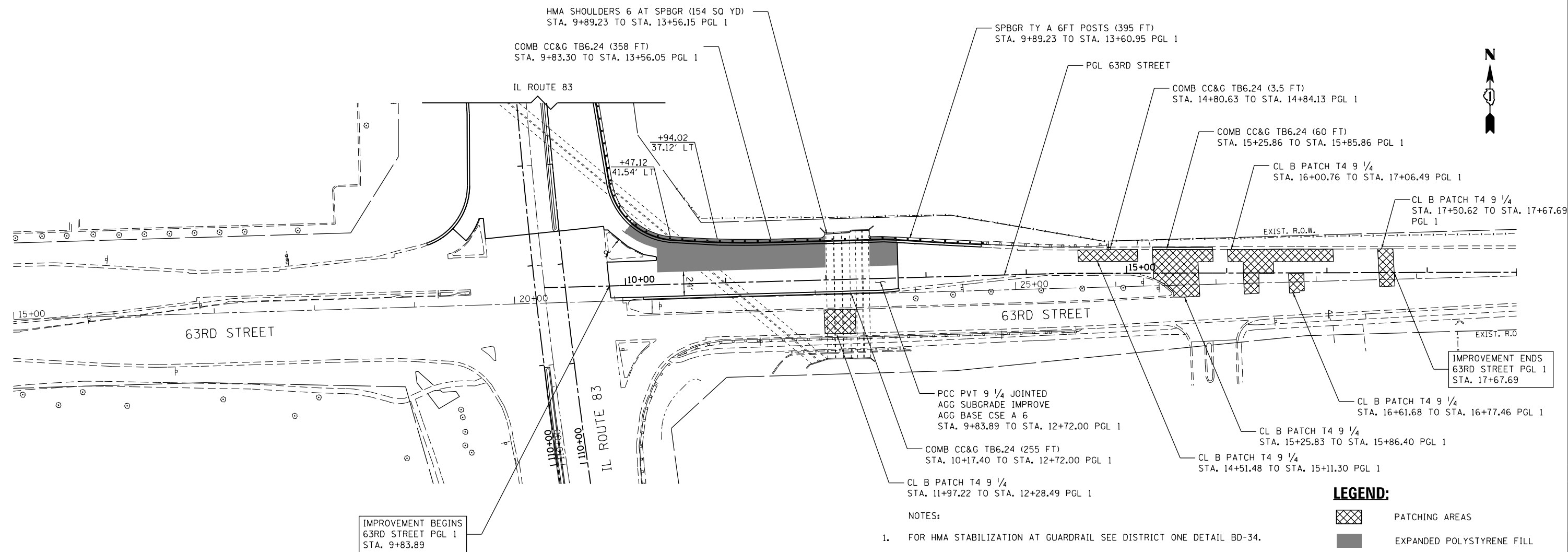
SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	30
CONTRACT NO. 60W55				

ILLINOIS FED. AID PROJECT

PLAN	SURVEYED	DATE
	PLOTTED	
	ALIGNED	
	CHECKED	
	FILED	
NOTE BOOK NO.	CADD FILE NAME	

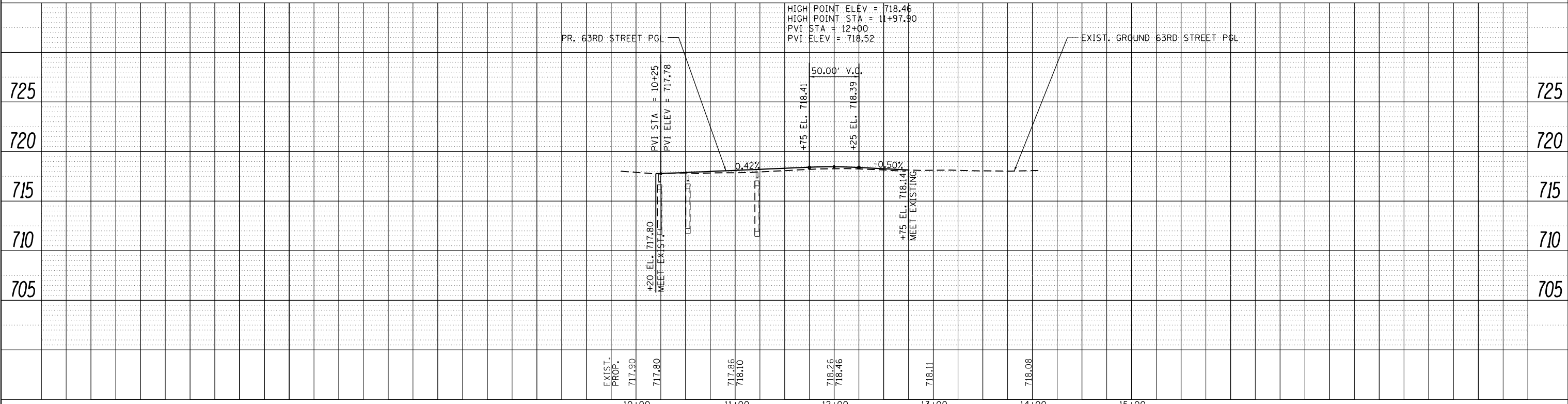
PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
NOTE BOOK NO.	NOTATION CHPKD	



- NOTES:
- FOR HMA STABILIZATION AT GUARDRAIL SEE DISTRICT ONE DETAIL BD-34.
  - WORK THIS SHEET WITH INTERSECTION JOINTING DETAILS SHEET.

**LEGEND:**

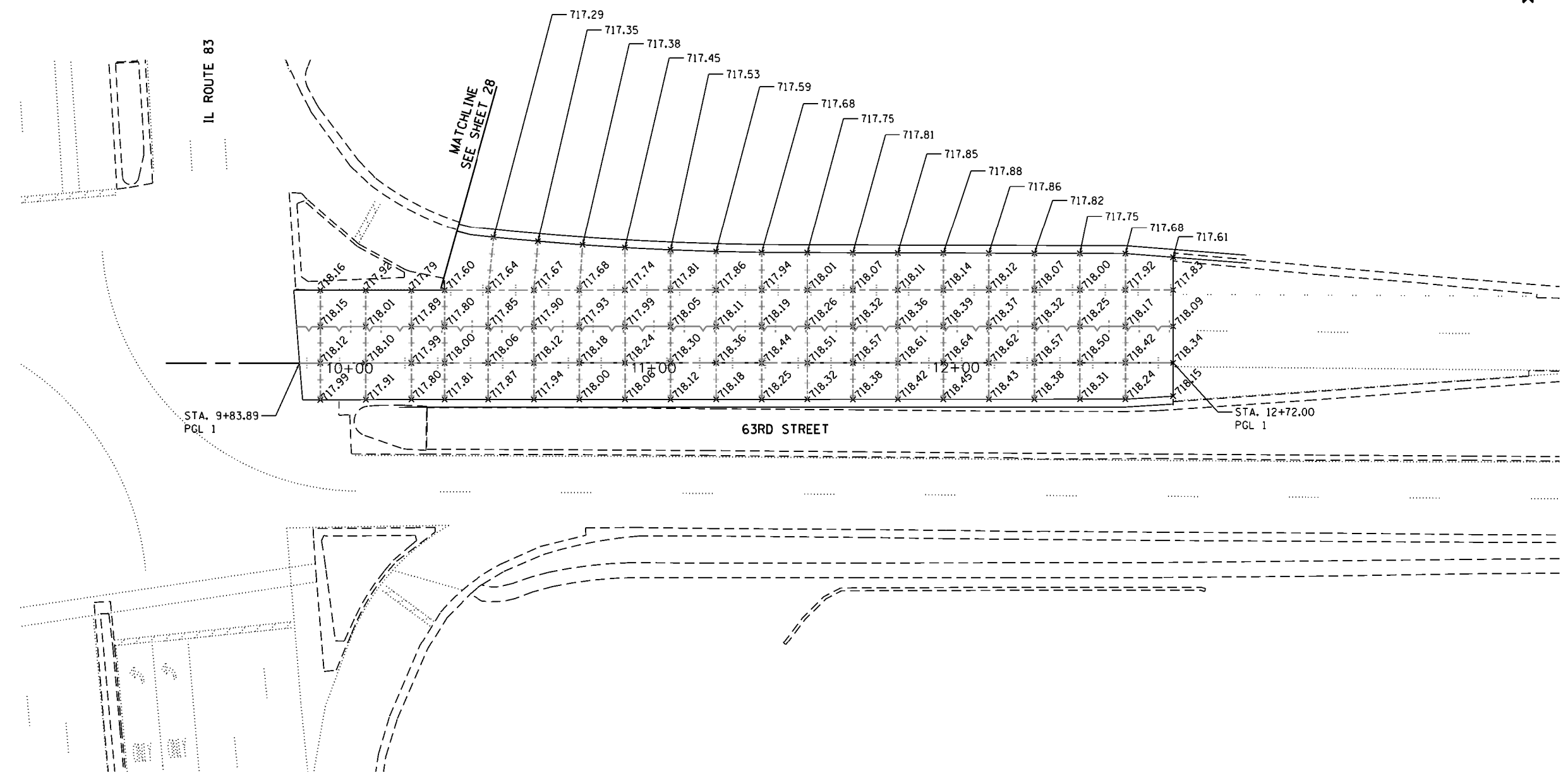
	PATCHING AREAS
	EXPANDED POLYSTYRENE FILL



FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>63RD STREET ROADWAY PLAN AND PROFILE</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.				
Default	1:\7290\7290.26 - IL83 and 63rd St\CADD\CADD_Sheets\DI60W55-sht-plnprf-63.dgn	DRAWN -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	344	(544 & 544-1) TS&N (13)	DUPAGE	129	31
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60W55											
	PLOT DATE = 11/26/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT											





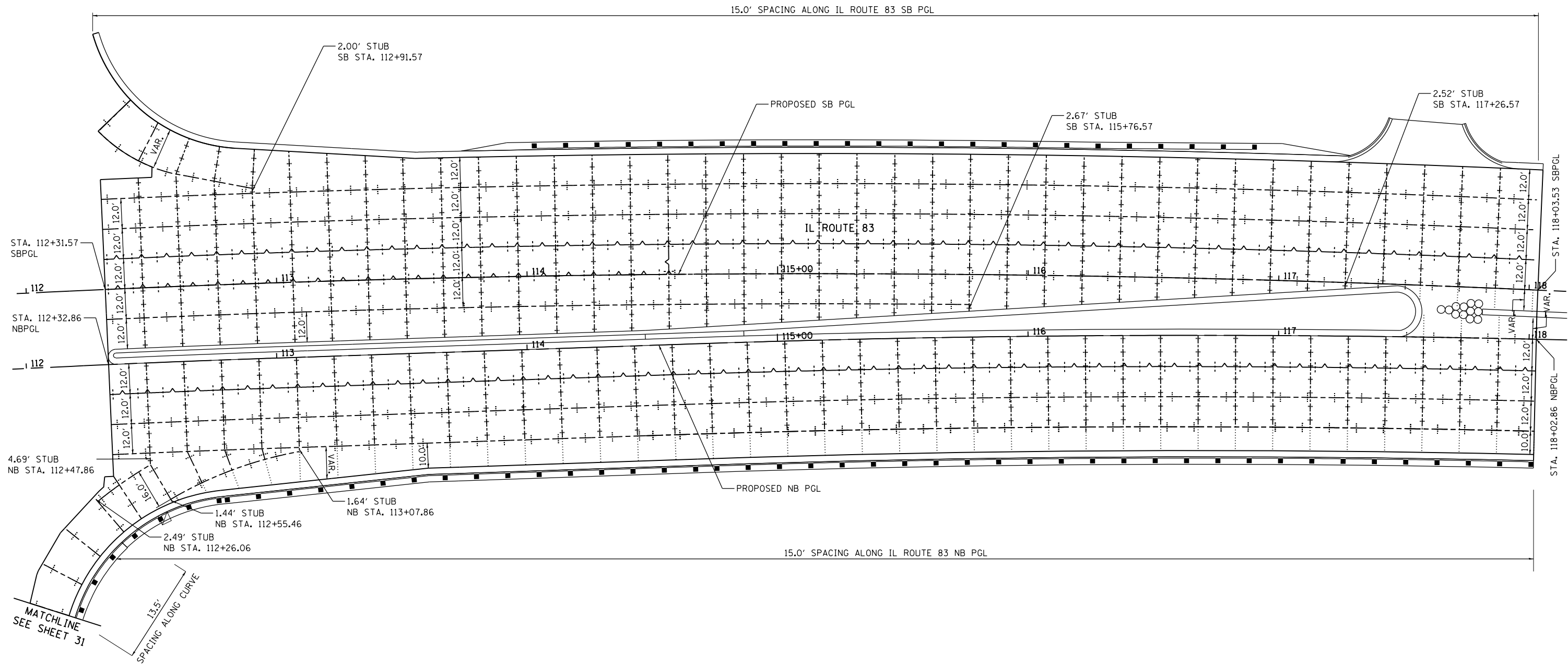


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	PLOT DATE = 11/26/2014	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>INTERSECTION DETAILS 63RD STREET</b>			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	33
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SEE HIGHWAY STD 420001, 420111, 420206, & 420306 AND DISTRICT 1 DETAILS BD49 & BD52 FOR PAVEMENT JOINT DETAILS.
2. ADDITIONAL SAWED TRANSVERSE CONSTRUCTION JOINTS MAY BE REQUIRED IN THE FIELD. EXACT LOCATIONS TO BE VERIFIED IN THE FIELD AND APPROVED BY THE ENGINEER.
3. CONCRETE STUBS SHALL HAVE A 1' MINIMUM WIDTH.

**PAVEMENT JOINT LEGEND:**

- LONGITUDINAL CONSTRUCTION JOINT WITH TIE BARS
- SAWED LONGITUDINAL JOINT
- SAWED TRANSVERSE CONTRACTION JOINT
- SAWED TRANSVERSE CONTRACTION JOINT W/IN PCC SHLDR

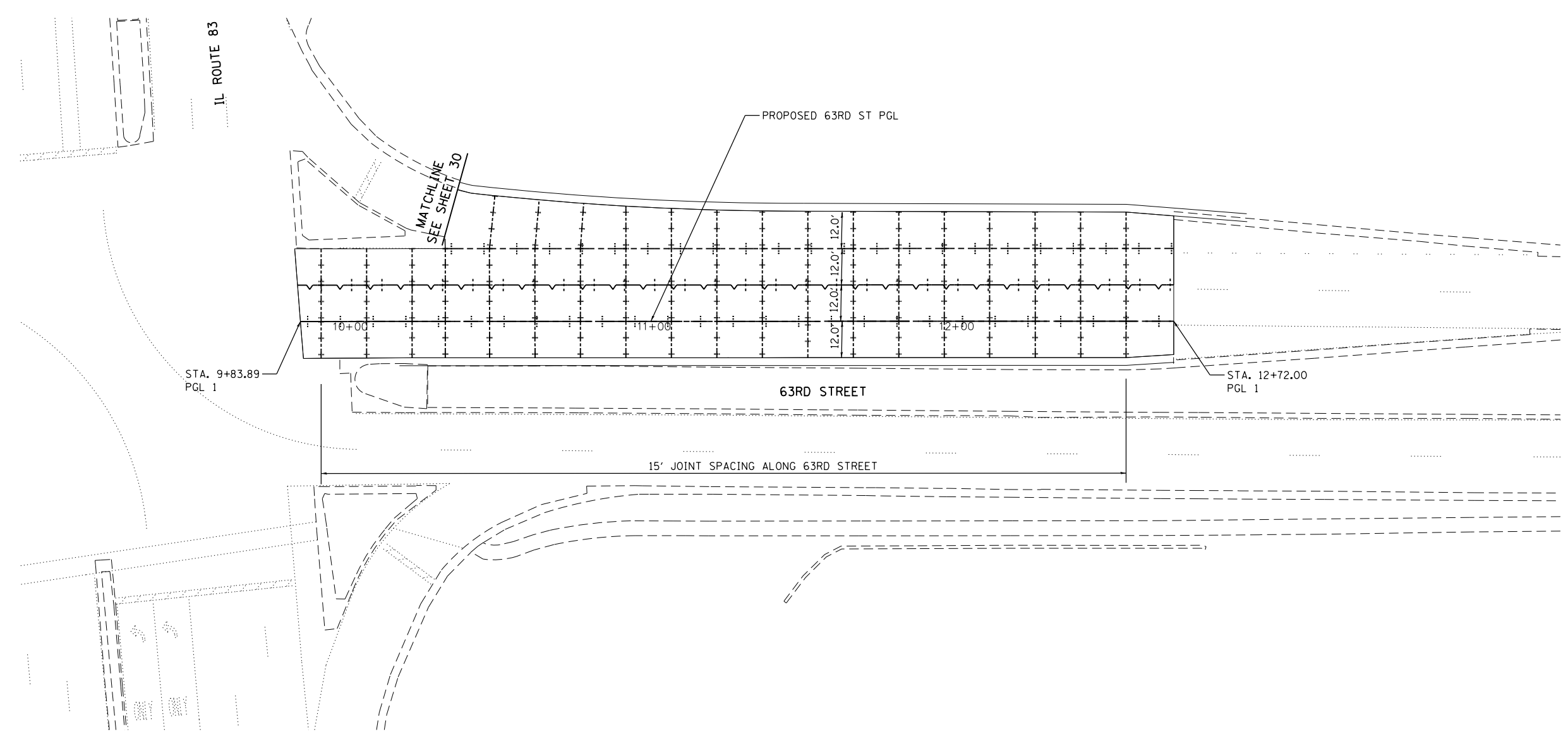
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PLOT SCALE = 40.0000' / in.		CHECKED -	REVISED -
PLOT DATE = 12/17/2014		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT JOINTING PLAN  
IL ROUTE 83**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	34
CONTRACT NO. 60W55			ILLINOIS FED. AID PROJECT	



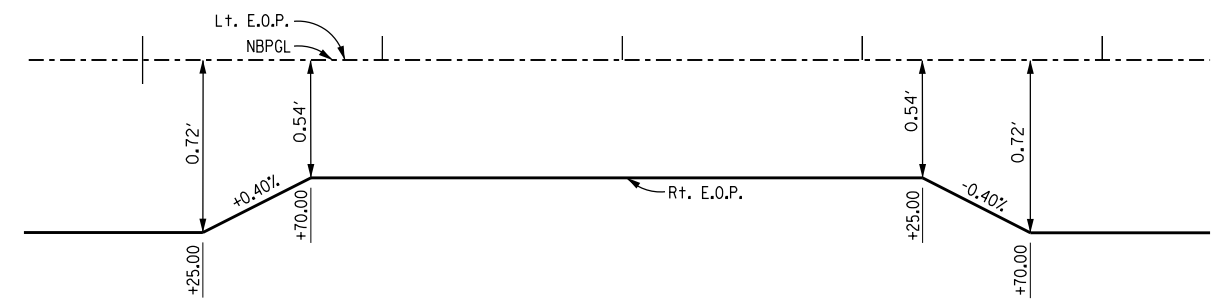
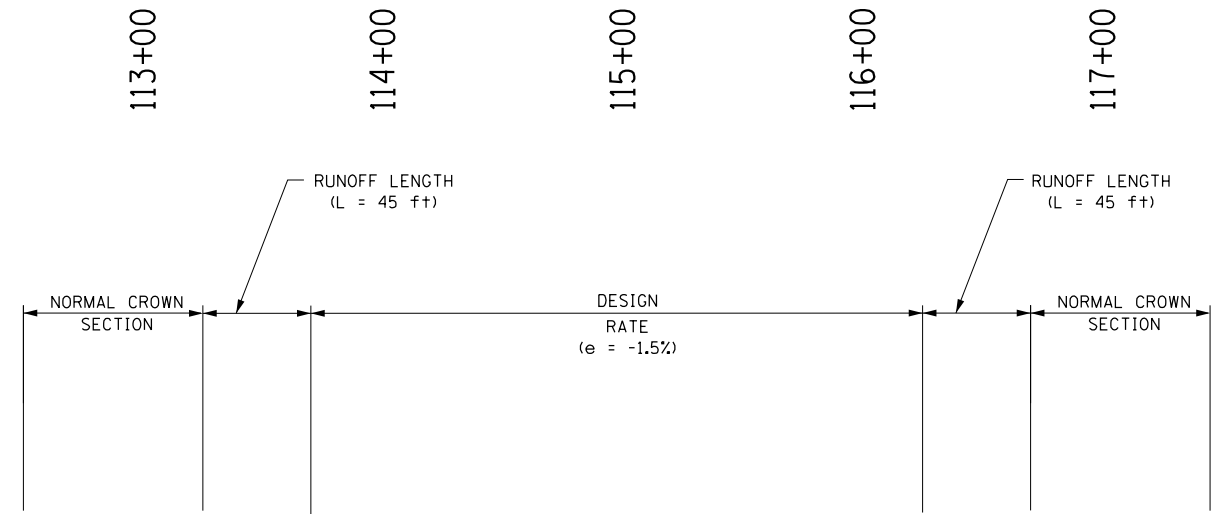
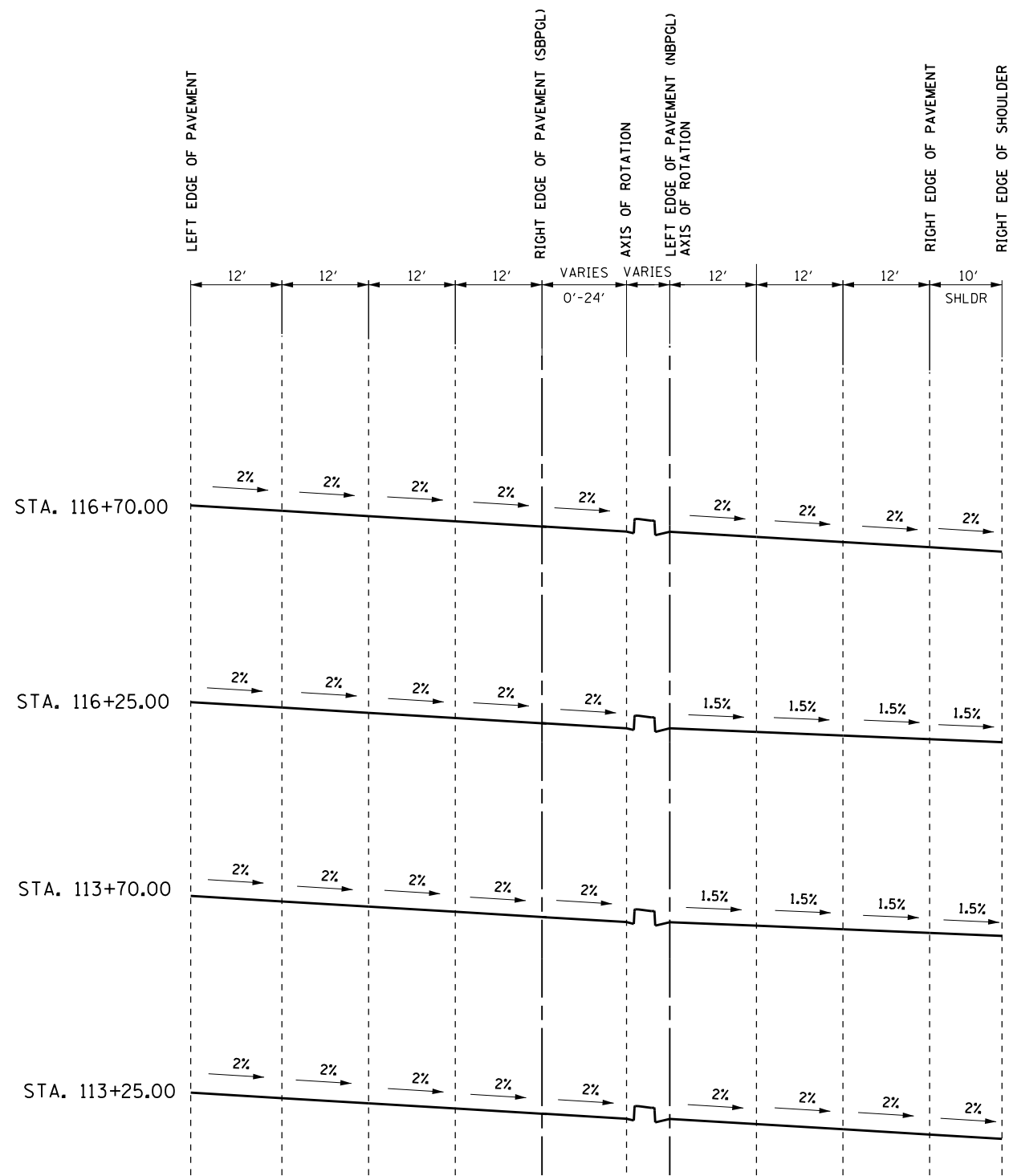
**NOTES:**

1. SEE HIGHWAY STD 420001, 420111, 420206, & 420306 AND DISTRICT 1 DETAILS BD49 & BD52 FOR PAVEMENT JOINT DETAILS.
2. ADDITIONAL SAWED TRANSVERSE CONSTRUCTION JOINTS MAY BE REQUIRED IN THE FIELD. EXACT LOCATIONS TO BE VERIFIED IN THE FIELD AND APPROVED BY THE ENGINEER.
3. CONCRETE STUBS SHALL HAVE A 1' MINIMUM WIDTH.

**PAVEMENT JOINT LEGEND:**

- LONGITUDINAL CONSTRUCTION JOINT WITH TIE BARS
- SAWED LONGITUDINAL JOINT
- SAWED TRANSVERSE CONTRACTION JOINT
- SAWED TRANSVERSE CONTRACTION JOINT W/IN PCC SHLDR

FILE NAME = I:\7290\7290.26 - IL83 and 63rd St\CADD	USER NAME = rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT JOINTING PLAN 63RD STREET</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	CADD_Sheets\DI60W55-sht:pvtjoint.dgn	DRAWN -	REVISED -		344	(544 & 544-1) TS&N (13)	DUPAGE	129	35				
Default	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -	SCALE:      SHEET      OF      SHEETS      STA.      TO      STA.				CONTRACT NO. 60W55					
	PLOT DATE = 11/26/2014	DATE -	REVISED -	ILLINOIS FED. AID PROJECT									



**SUPERELEVATION DIAGRAM**  
**IL 83 NBPGL**  
**DESIGN SPEED = 45 mph**

FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -
I:\7290\7290.26 - IL83 and 63rd St\CA00	CADD_Sheets\DI60W55-sht-super.dgn	DRAWN -	REVISED -
Default	PLOT SCALE = 8.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 11/26/2014	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83**  
**SUPERELEVATION DIAGRAM**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	36
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				

**MAINTENANCE OF TRAFFIC - GENERAL NOTES**

1. SEE SPECIAL PROVISIONS TITLED TRAFFIC CONTROL AND PROTECTION (SPECIAL).
2. THE CONTRACTOR SHALL REMOVE AND SAFELY STORE (FREE FROM THEFT OR DAMAGE) OR COVER ALL CONFLICTING EXISTING SIGNS FOR THE DURATION OF THE CONSTRUCTION. ALL SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AT THE END OF CONSTRUCTION.
3. THE FOLLOWING APPLY TO CONSTRUCTION SIGNS:
  - A) THE CONTRACTOR SHALL FURNISH ALL SIGNS.
  - B) THE CONTRACTOR SHALL BE RESPONSIBLE FOR AND REPLACE ANY SIGNS THAT ARE SUPPLIED BY OTHERS AND DAMAGED BY THE CONTRACTOR'S WORK FORCE OR SUBCONTRACTORS DURING RELOCATION OR CONSTRUCTION OPERATIONS.
  - C) ALL SIGNS AND ASSEMBLIES SHALL BE CERTIFIED BY THE CONTRACTOR AS MEETING THE APPLICABLE REQUIREMENTS OF NCHRP REPORT 350, TEST LEVEL 3.
  - D) ALL SIGNS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE TRAFFIC CONTROL AND PROTECTION (SPECIAL) PAY ITEM, EXCEPT FOR TEMPORARY INFORMATIONAL SIGNING AS NOTED ON THE PLANS.
4. OPENINGS THROUGH THE BARRIER FOR CONTRACTOR'S ACCESS TO THE WORK ZONE SHALL BE PROVIDED AS APPROVED BY THE ENGINEER.
5. ANY RAISED REFLECTIVE PAVEMENT MARKERS THAT CONFLICT WITH THE TEMPORARY TRAFFIC LANES MUST HAVE THE REFLECTIVE LENSES REMOVED AS DIRECTED BY THE ENGINEER.
6. ALL TEMPORARY PAVEMENT MARKINGS DURING STAGED CONSTRUCTION SHALL BE WET REFLECTIVE TAPE, TYPE III OF THE WIDTH AND COLOR SPECIFIED ON THE PLAN SHEETS.
7. MONO-DIRECTIONAL PRISMATIC BARRIER REFLECTORS WILL BE PLACED AT 25' CENTERS ON TOP AND SIDE OF TEMPORARY CONCRETE BARRIER FACING TRAFFIC.
8. NO TRAFFIC STAGES SHALL OVERLAP WITHOUT PRIOR WRITTEN APPROVAL FROM THE ENGINEER.
9. NO INTERIM COMPLETION DATES ARE SPECIFIED FOR ANY OF THE CONSTRUCTION STAGES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DEVELOPING HIS/HER CONSTRUCTION SCHEDULE TO MEET THE PROJECT COMPLETION DATE.
10. TEMPORARY CONCRETE BARRIER SHALL BE CONTINUOUSLY PINNED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE SPECIAL PROVISION FOR TEMPORARY CONCRETE BARRIER WALL WHERE A 3.5' CLEAR ZONE FREE FROM DROP OFFS, EQUIPMENT, AND OBSTRUCTIONS CANNOT BE PROVIDED BEHIND THE WALL.

**STAGING NOTES: STAGE I**

WORK IN THIS STAGE CONSISTS OF CONSTRUCTING THE OUTSIDE TRAVEL LANE AND SHOULDER OF NB IL ROUTE 83 AND LANES OF WB 63RD STREET.

STRUCTURE SN 022-0563 WILL BE CONSTRUCTED UP TO THE STAGE I CONSTRUCTION LINE DURING THIS STAGE.

INSTALL STAGE I TEMPORARY SIGNAGE.

TAPER THE TWO OUTSIDE LANES OF NB IL ROUTE 83 AND SB IL ROUTE 83 AND CLOSE THE TWO OUTSIDE LANES WB 63RD ST AS SHOWN ON THE PLANS.

DETOUR WB 63RD STREET TRAFFIC AS SHOWN ON THE PLANS.

REMOVE EXISTING MEDIAN AND CONSTRUCT TEMPORARY PAVEMENT AS SHOWN ON PLANS.

INSTALL PROPOSED DRAINAGE AND STABILIZED SUBGRADE FOR MEDIAN, PROPOSED CURB AND GUTTER. THE PROPOSED CONCRETE MEDIAN TO BE CONSTRUCTED IN STAGE 4.

**STAGING NOTES: STAGE II**

WORK IN THIS STAGE CONSISTS OF CONSTRUCTING THE TWO INNER LANES OF NB IL ROUTE 83 AND THE TWO INNER MOST THROUGH LANES AND TWO TURN LANES OF SB IL ROUTE 83 AND THE OUTSIDE THROUGH LANE OF EB 63RD ST.

STRUCTURE SN 022-0563 WILL BE CONSTRUCTED UP TO THE STAGE II CONSTRUCTION LINE DURING THIS STAGE.

INSTALL TEMPORARY SIGNALS.

INSTALL STAGE II TEMPORARY SIGNAGE.

RELOCATE TEMPORARY CONCRETE BARRIER WALL AND TEMPORARY IMPACT ATTENUATORS.

CLOSE THE INSIDE THROUGH LANES OF NB IL ROUTE 83 AND TAPER THE TWO INNER MOST THROUGH LANES OF SB IL ROUTE 83 AND TWO OUTERMOST THROUGH LANES OF NB IL ROUTE AS SHOWN ON THE PLANS.

**STAGING NOTES: SUBSTAGE STAGE IIA**

WORK IN THIS STAGE CONSISTS OF CONSTRUCTING THE REMAINDER OF THE INNER MOST THROUGH LANE FOR SB IL ROUTE 83 NOT CONSTRUCTED IN STAGE II.

INSTALL STAGE IIAA TEMPORARY SIGNAGE.

RELOCATE TEMPORARY CONCRETE BARRIER WALL AND TEMPORARY IMPACT ATTENUATORS.

CLOSE THE INSIDE THROUGH LANES OF NB IL ROUTE 83 AND TAPER THE TWO INNER MOST THROUGH LANES OF SB IL ROUTE 83 AND TWO OUTERMOST THROUGH LANES OF NB IL ROUTE AS SHOWN ON THE PLANS.

**STAGING NOTES: STAGE III**

WORK IN THIS STAGE CONSISTS OF CONSTRUCTING THE OUTER MOST THROUGH AND RIGHT TURN LANE OF SB IL ROUTE 83 AND THE REMAINDER OF SN 022-0563.

INSTALL STAGE III TEMPORARY SIGNAGE.

RELOCATE TEMPORARY CONCRETE BARRIER WALL AND TEMPORARY IMPACT ATTENUATORS.

TAPER THE SB IL ROUTE 83 AND NB IL ROUTE 83 THROUGH LANES AS SHOWN ON THE PLANS.

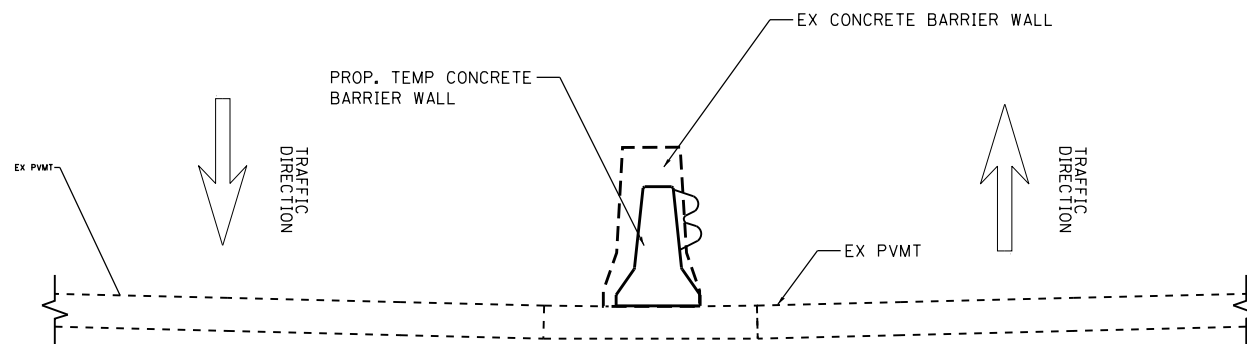
REMOVE EXISTING TEMPORARY PAVEMENT OUTSIDE OF SB IL ROUTE 83.

**STAGING NOTES: STAGE STAGE IV**

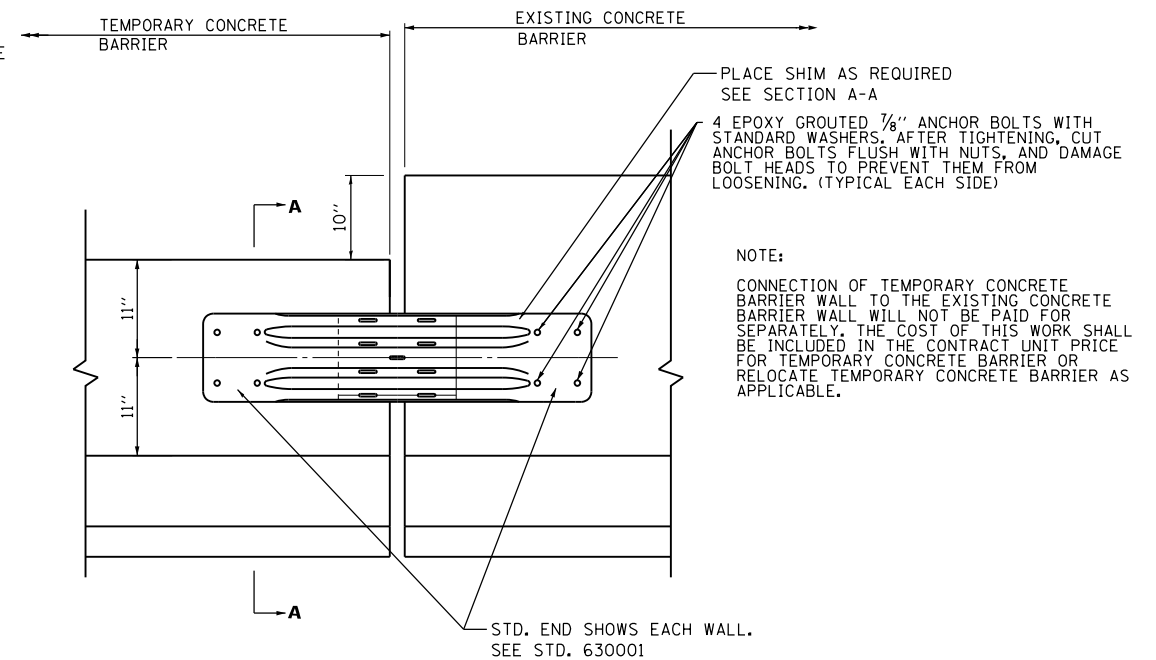
WORK IN THIS STAGE CONSISTS OF REMOVING TEMPORARY PAVEMENT AND CONSTRUCTING THE PROPOSED MEDIAN.

INSTALL STAGE IV TEMPORARY SIGNAGE.

TAPER THE NB IL ROUTE 83 SB IL ROUTE 83 THROUGH LANES AS SHOWN ON THE PLANS.



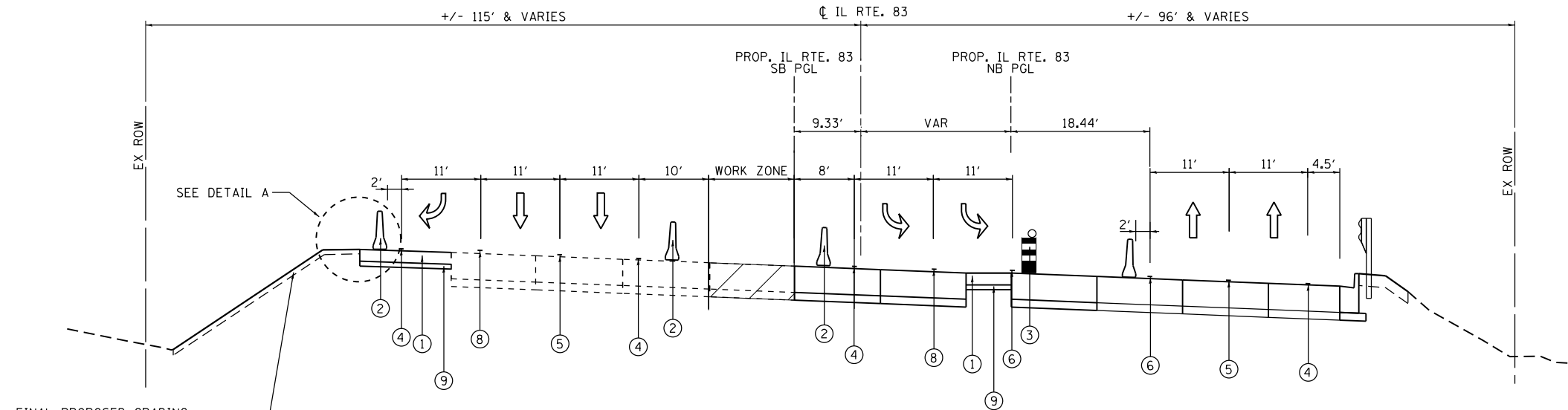
**SECTION A-A**



**CONNECTION BETWEEN TEMPORARY CONCRETE BARRIER AND EXISTING CONCRETE BARRIER**

FILE NAME = I:\7290\7290.26 - IL83 and 63rd St\CADD	USER NAME = rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 83 AT MARION HILLS DITCH SUGGESTED STAGING NOTES AND DETAILS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CADD_Sheets\DI60W55-sht-staging.dgn	DRAWN -	REVISED -	344			(544 & 544-1)	TS&N (13)	DUPAGE	129	37
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 60W55							
PLOT DATE = 12/17/2014	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
Default				SCALE:	SHEET OF SHEETS	STA. TO STA.				

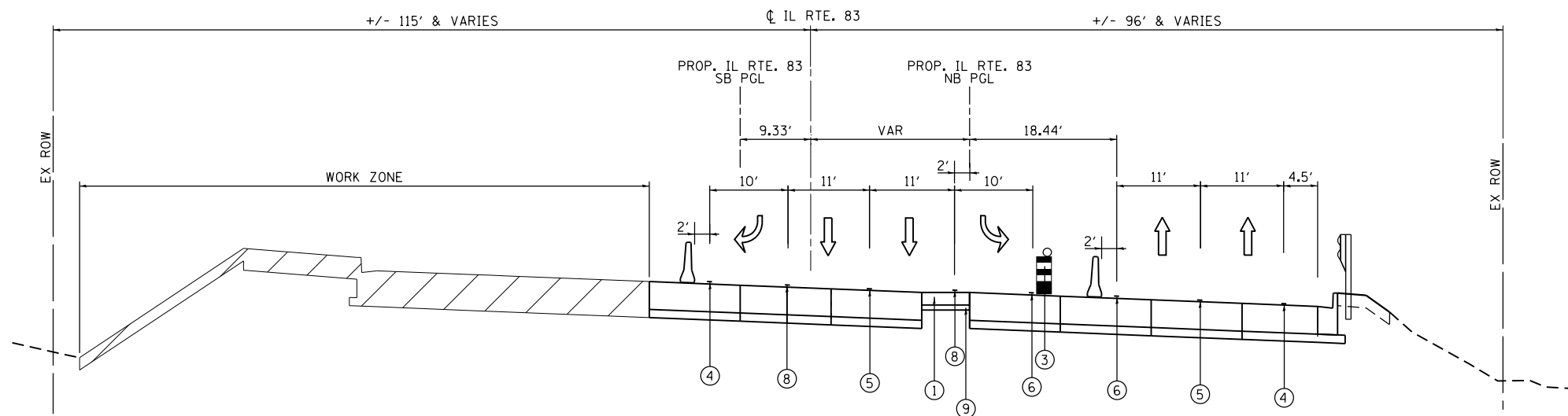




FINAL PROPOSED GRADING  
(SEE PROPOSED TYPICAL SECTIONS  
AND PROPOSED CROSS SECTIONS)

**IL ROUTE 83 – STAGE IIA**

STA. 112+31.57 TO STA. 117+30.41

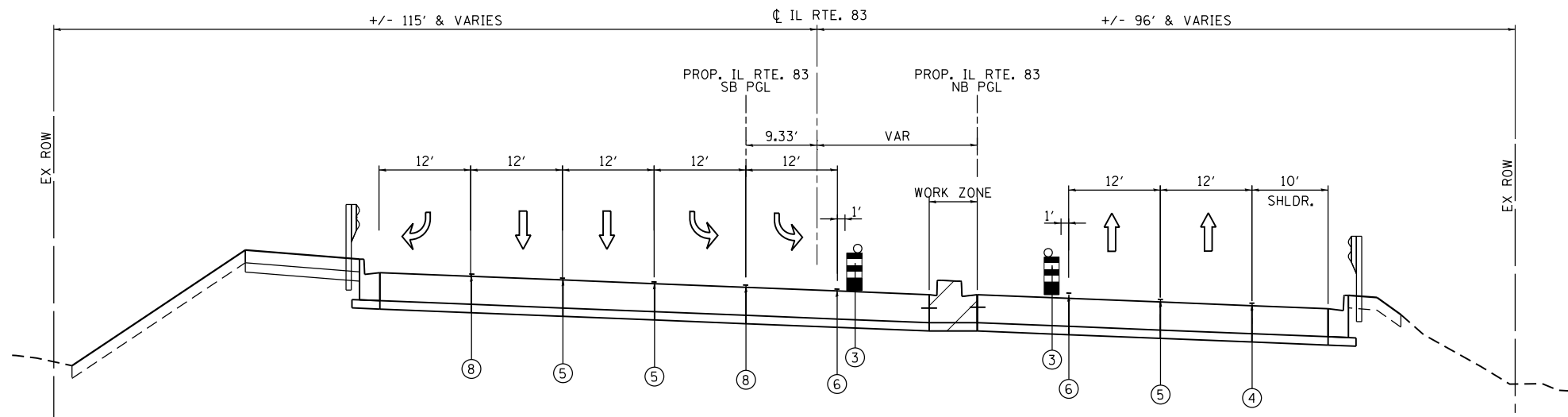


**IL ROUTE 83 – STAGE III**

STA. 112+31.57 TO STA. 117+30.41

- ① TEMPORARY PAVEMENT, 10"
- ② TEMPORARY CONCRETE BARRIER
- ③ TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
- ④ WET REFLECTIVE TEMPORARY TAPE TYPE III - LINE 4" WHITE
- ⑤ WET REFLECTIVE TEMPORARY TAPE TYPE III - LINE 4" WHITE (10'-30' SKIP DASH)
- ⑥ WET REFLECTIVE TEMPORARY TAPE TYPE III - LINE 4" YELLOW
- ⑦ WET REFLECTIVE TEMPORARY TAPE TYPE III - LINE 4" YELLOW (DOUBLE)
- ⑧ WET REFLECTIVE TEMPORARY TAPE TYPE III - LINE 6" WHITE
- ⑨ AGGREGATE SUBGRADE IMPROVEMENT, 4"

FILE NAME = I:\7290\7290.26 - IL83 and 63-d St\CADD	USER NAME = rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MAINTENANCE OF TRAFFIC TYPICAL SECTIONS</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	CADD_Sheets\DI60W55-sht-staging-typical.dgn	DRAWN -	REVISED -		344	(544 & 544-1) TS&N (13)	DUPAGE	129	39				
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	PLOT DATE = 12/17/2014	DATE -	REVISED -	ILLINOIS FED. AID PROJECT									



**IL ROUTE 83 – STAGE IV**  
 STA. 112+31.57 TO STA. 117+30.41

- ① TEMPORARY PAVEMENT, 10"
- ② TEMPORARY CONCRETE BARRIER
- ③ TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
- ④ WET REFLECTIVE TEMPORARY TAPE TYPE III - LINE 4" WHITE
- ⑤ WET REFLECTIVE TEMPORARY TAPE TYPE III - LINE 4" WHITE (10'-30' SKIP DASH)
- ⑥ WET REFLECTIVE TEMPORARY TAPE TYPE III - LINE 4" YELLOW
- ⑦ WET REFLECTIVE TEMPORARY TAPE TYPE III - LINE 4" YELLOW (DOUBLE)
- ⑧ WET REFLECTIVE TEMPORARY TAPE TYPE III - LINE 6" WHITE
- ⑨ AGGREGATE SUBGRADE IMPROVEMENT, 4"

FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -
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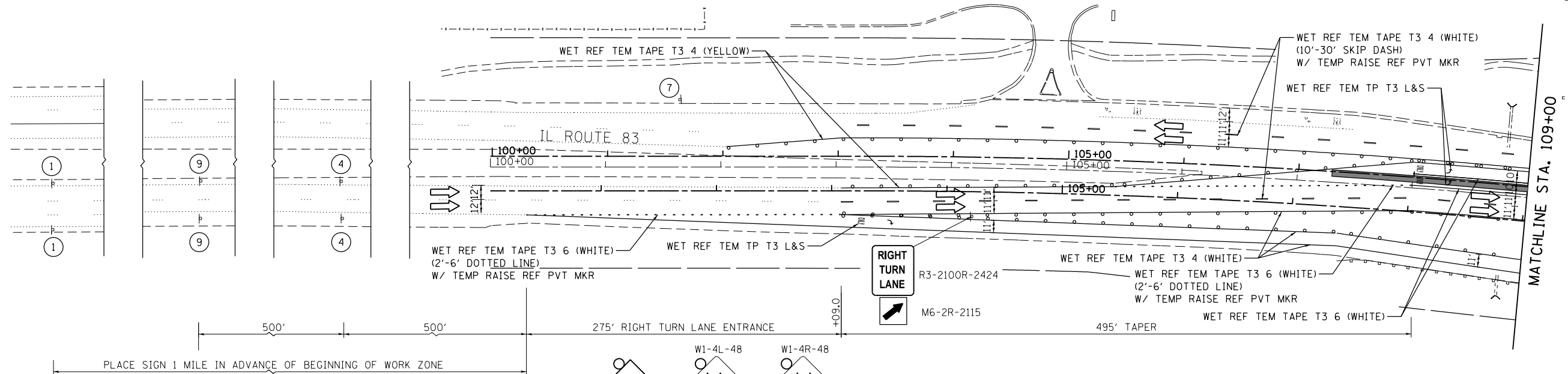
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC  
 TYPICAL SECTIONS**

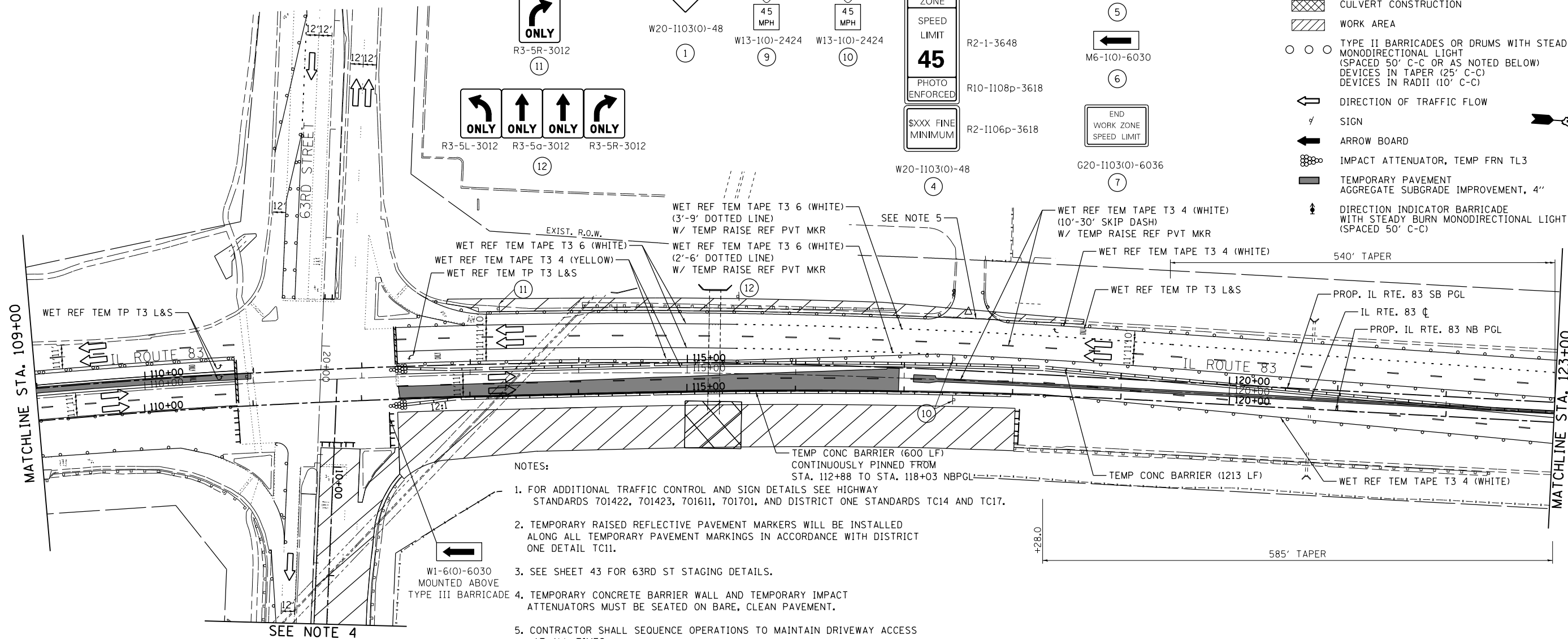
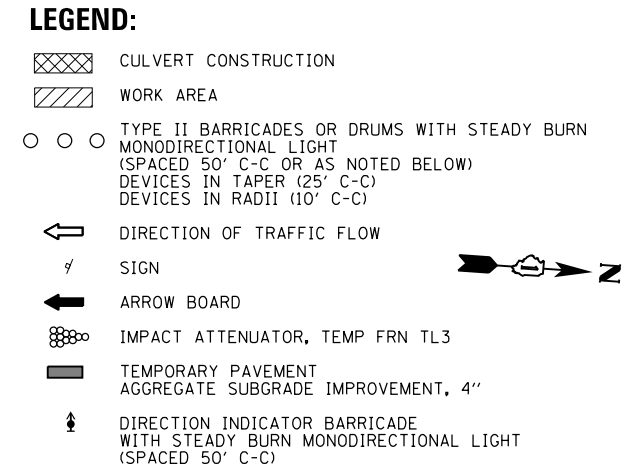
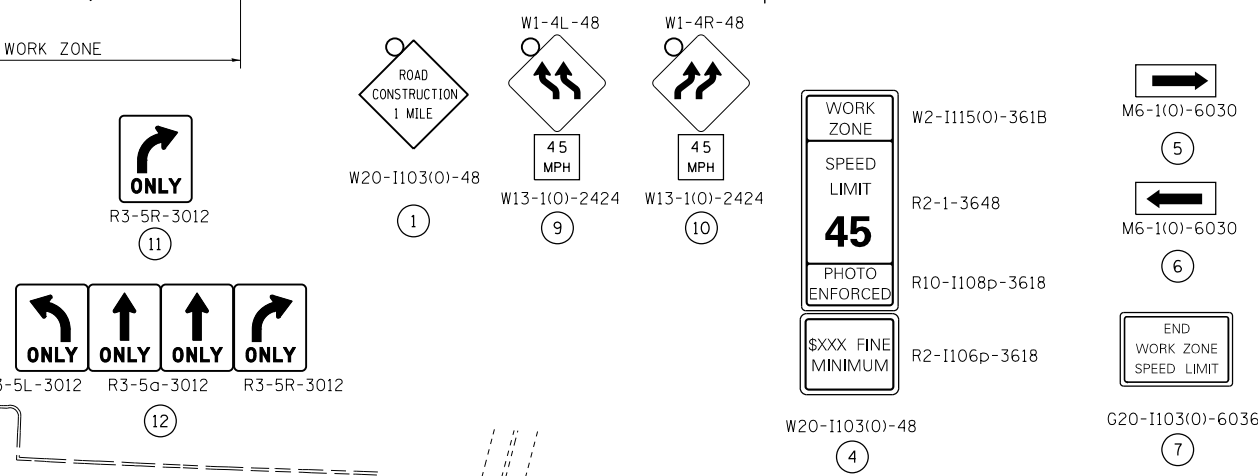
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	40
				<b>CONTRACT NO. 60W55</b>
ILLINOIS FED. AID PROJECT				



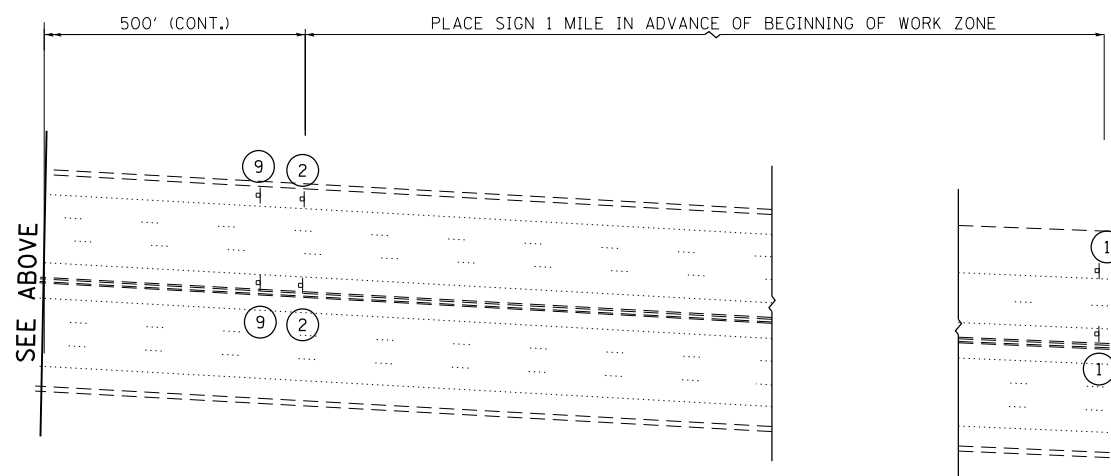
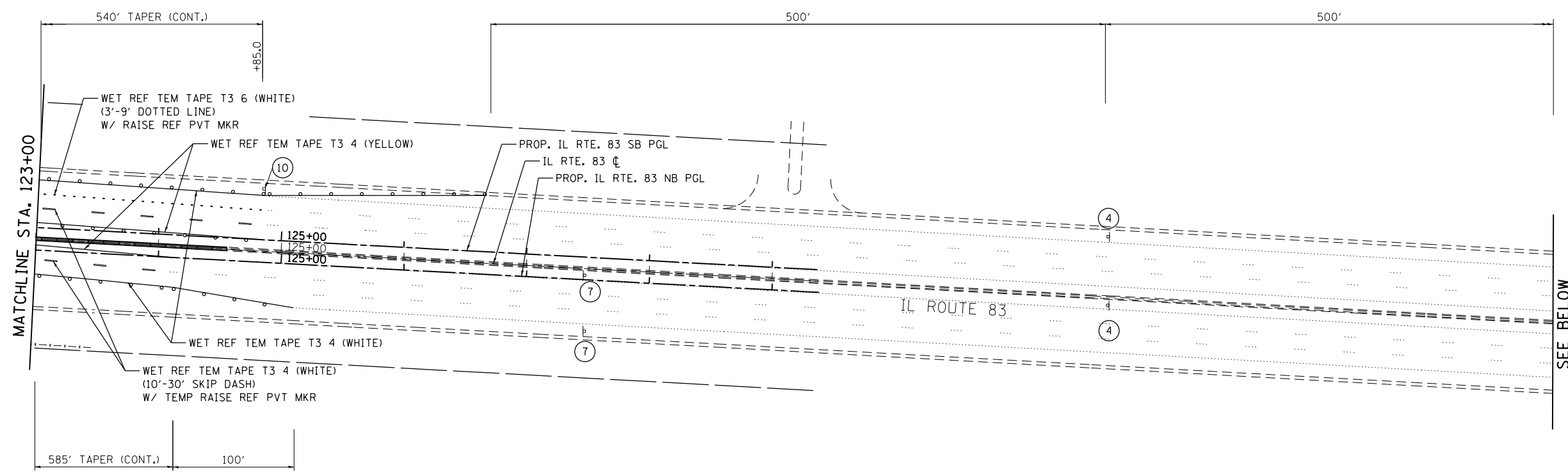


PLACE SIGN 1 MILE IN ADVANCE OF BEGINNING OF WORK ZONE



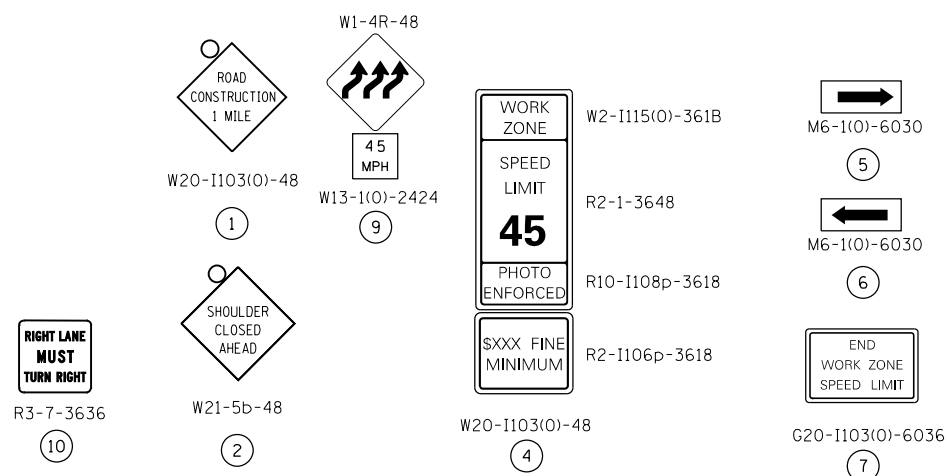
- NOTES:
1. FOR ADDITIONAL TRAFFIC CONTROL AND SIGN DETAILS SEE HIGHWAY STANDARDS 701422, 701423, 701611, 701701, AND DISTRICT ONE STANDARDS TC14 AND TC17.
  2. TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS WILL BE INSTALLED ALONG ALL TEMPORARY PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC11.
  3. SEE SHEET 43 FOR 63RD ST STAGING DETAILS.
  4. TEMPORARY CONCRETE BARRIER WALL AND TEMPORARY IMPACT ATTENUATORS MUST BE SEATED ON BARE, CLEAN PAVEMENT.
  5. CONTRACTOR SHALL SEQUENCE OPERATIONS TO MAINTAIN DRIVEWAY ACCESS AT ALL TIMES.

FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 83 MAINTENANCE OF TRAFFIC STAGE I</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.				
1:\7290\7290.26 - IL83 and 63-d St\CADD	Exhibits\DI60W55-sht-staging_REV.SHT.dgn	DRAWN -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	344	(544 & 544-1) TS&N (13)	DUPAGE	129	41
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -										CONTRACT NO. 60W55			
	PLOT DATE = 12/17/2014	DATE -	REVISED -										ILLINOIS FED. AID PROJECT			



**NOTES:**

- FOR ADDITIONAL TRAFFIC CONTROL AND SIGN DETAILS SEE HIGHWAY STANDARDS 701422, 701423, 701611, 701701, AND DISTRICT ONE STANDARDS TC14 AND TC17.
- EXISTING TRAFFIC SIGNALS AT IL 83 AND 63RD ST TO REMAIN IN OPERATION.
- TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS WILL BE INSTALLED ALONG ALL TEMPORARY PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC11.
- SEE SHEET 43 FOR 63RD ST STAGING DETAILS.
- TEMPORARY CONCRETE BARRIER WALL AND TEMPORARY IMPACT ATTENUATORS MUST BE SEATED ON BARE, CLEAN PAVEMENT.



**LEGEND:**

- CULVERT CONSTRUCTION
- WORK AREA
- TYPE II BARRICADES OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C OR AS NOTED BELOW) DEVICES IN TAPER (25' C-C) DEVICES IN RADII (10' C-C)
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
- IMPACT ATTENUATOR, TEMP FRN TL3
- TEMPORARY PAVEMENT AGGREGATE SUBGRADE IMPROVEMENT, 4"
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C)

FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -
I:\7290\7290.26 - IL83 and 63rd St\CADD	Exhibits\DI60W55-sht-staging_REV.SHT.dgn	DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/17/2014	DATE -	REVISED -

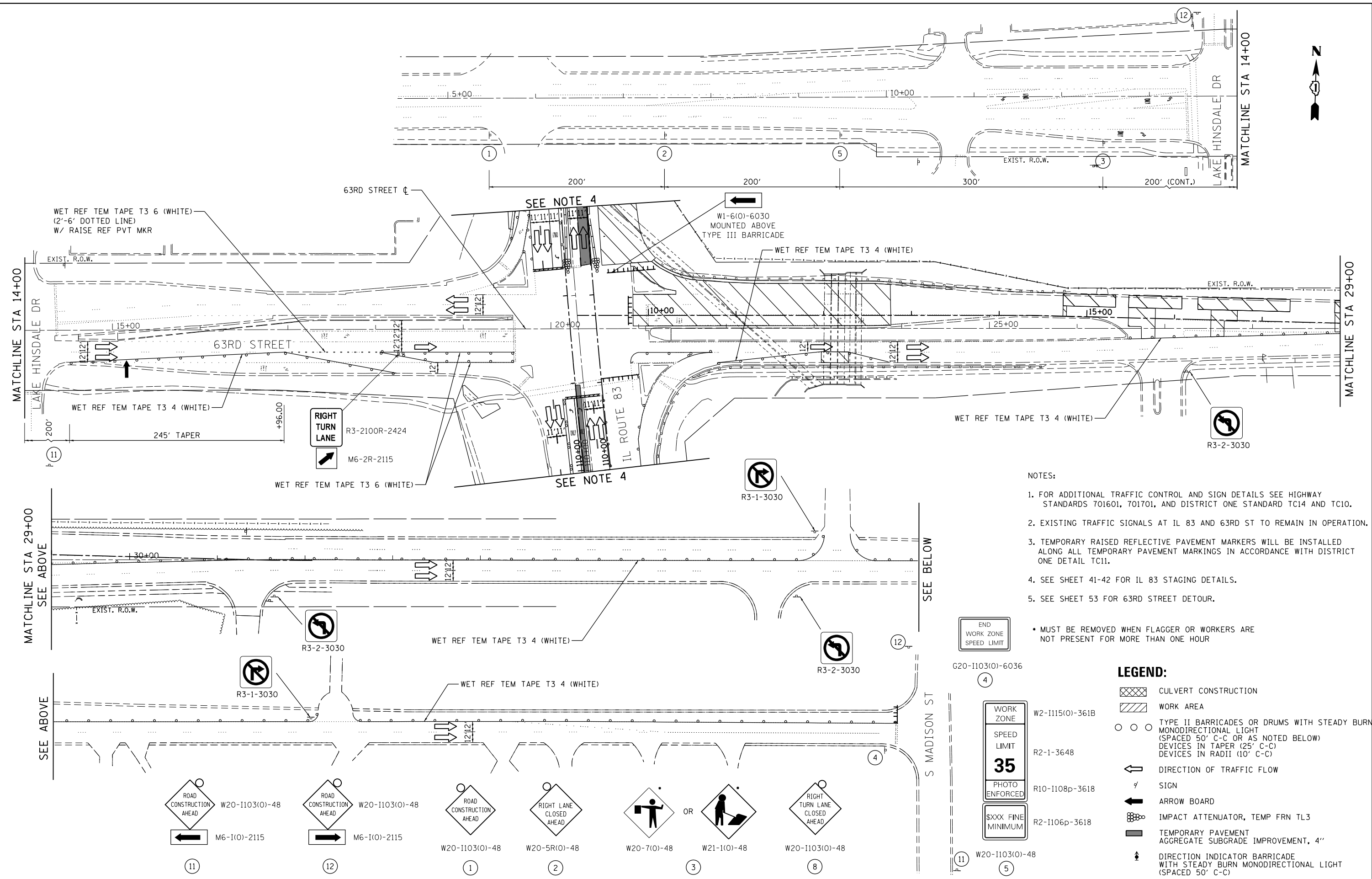
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83  
MAINTENANCE OF TRAFFIC  
STAGE I**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	42
CONTRACT NO. 60W55				

SCALE: SHEET OF SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT



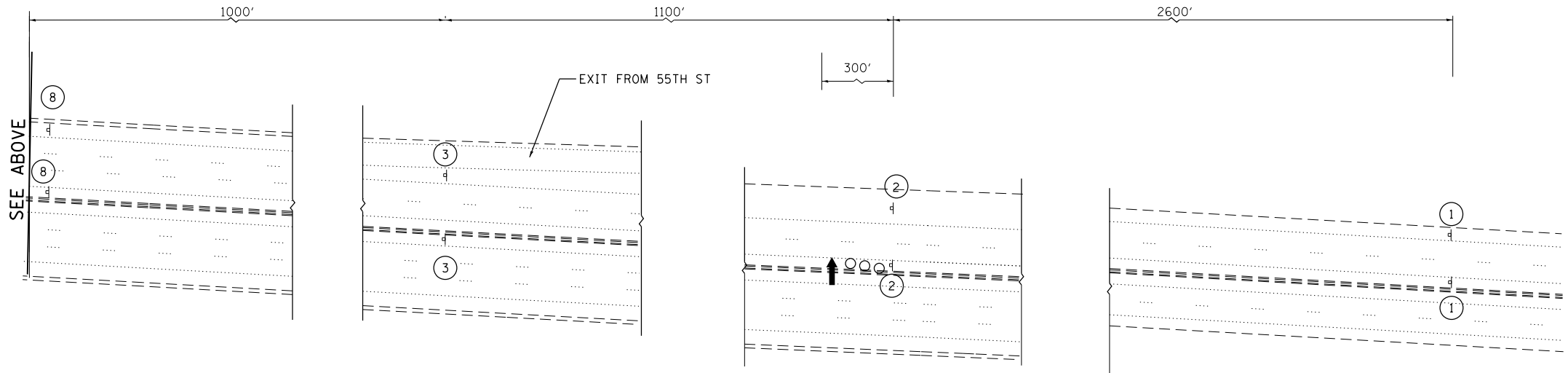
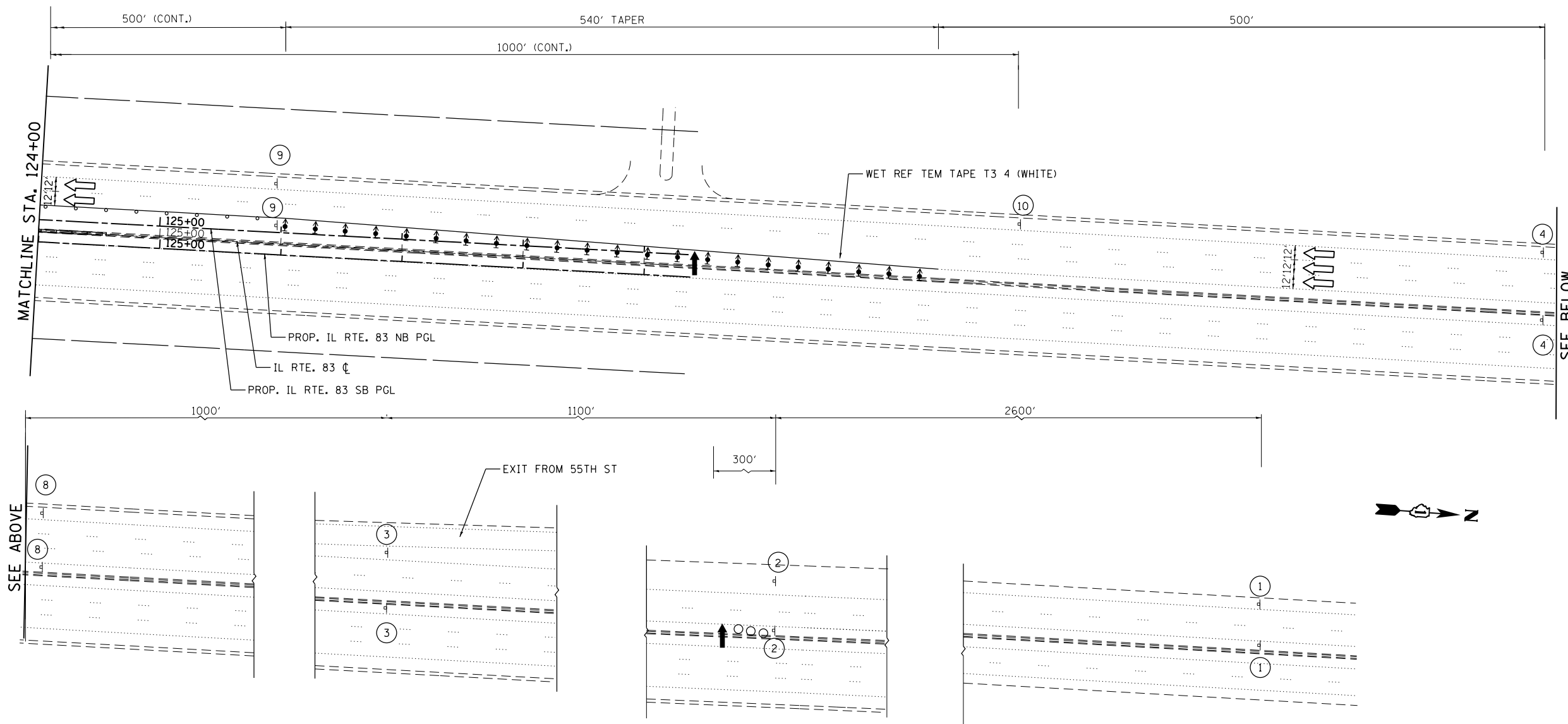
- NOTES:
1. FOR ADDITIONAL TRAFFIC CONTROL AND SIGN DETAILS SEE HIGHWAY STANDARDS 701601, 701701, AND DISTRICT ONE STANDARD TC14 AND TC10.
  2. EXISTING TRAFFIC SIGNALS AT IL 83 AND 63RD ST TO REMAIN IN OPERATION.
  3. TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS WILL BE INSTALLED ALONG ALL TEMPORARY PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC11.
  4. SEE SHEET 41-42 FOR IL 83 STAGING DETAILS.
  5. SEE SHEET 53 FOR 63RD STREET DETOUR.

• MUST BE REMOVED WHEN FLAGGER OR WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR

- LEGEND:**
- CULVERT CONSTRUCTION
  - WORK AREA
  - TYPE II BARRICADES OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C OR AS NOTED BELOW) DEVICES IN TAPER (25' C-C) DEVICES IN RADII (10' C-C)
  - DIRECTION OF TRAFFIC FLOW
  - SIGN
  - ARROW BOARD
  - IMPACT ATTENUATOR, TEMP FRN TL3
  - TEMPORARY PAVEMENT AGGREGATE SUBGRADE IMPROVEMENT, 4"
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C)

FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>63RD STREET MAINTENANCE OF TRAFFIC STAGE I</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
1:\7290\7290.26 - IL83 and 63rd St\CADD	Exhibits\DI60W55-sht-staging_REV.SHT.dgn	DRAWN -	REVISED -			344	(544 & 544-1)	TS&N (13)	DUPAGE	129	43
Default	PLOT DATE = 12/17/2014	CHECKED -	REVISED -			CONTRACT NO. 60W55					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

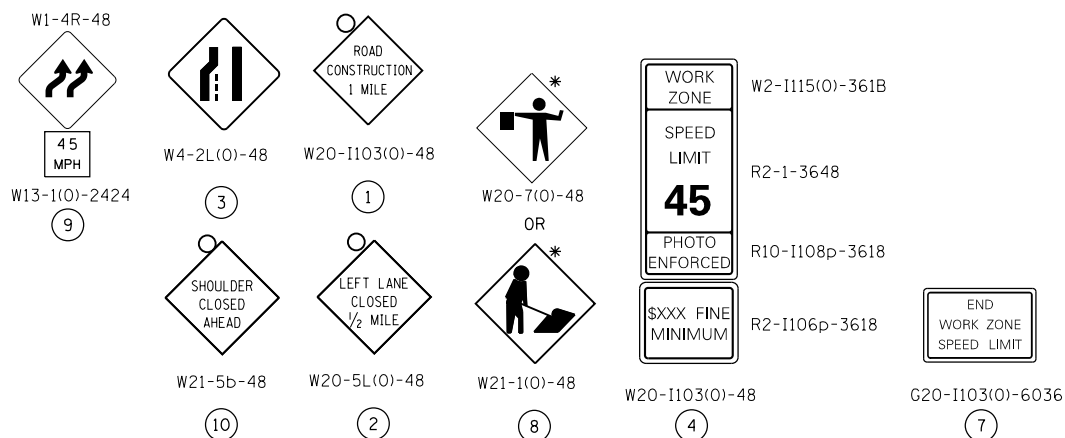




\* MUST BE REMOVED WHEN FLAGGER OR WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR

NOTES:

- FOR ADDITIONAL TRAFFIC CONTROL AND SIGN DETAILS SEE HIGHWAY STANDARDS 701422, 701423, 701611, AND DISTRICT ONE STANDARDS TC14 AND TC17.
- TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS WILL BE INSTALLED ALONG ALL TEMPORARY PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC11.



LEGEND:

- CULVERT CONSTRUCTION
- WORK AREA
- TYPE II BARRICADES OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C OR AS NOTED BELOW) DEVICES IN TAPER (25' C-C) DEVICES IN RADII (10' C-C)
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
- IMPACT ATTENUATOR, TEMP FRN TL3
- TEMPORARY PAVEMENT AGGREGATE SUBGRADE IMPROVEMENT, 4"
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C)

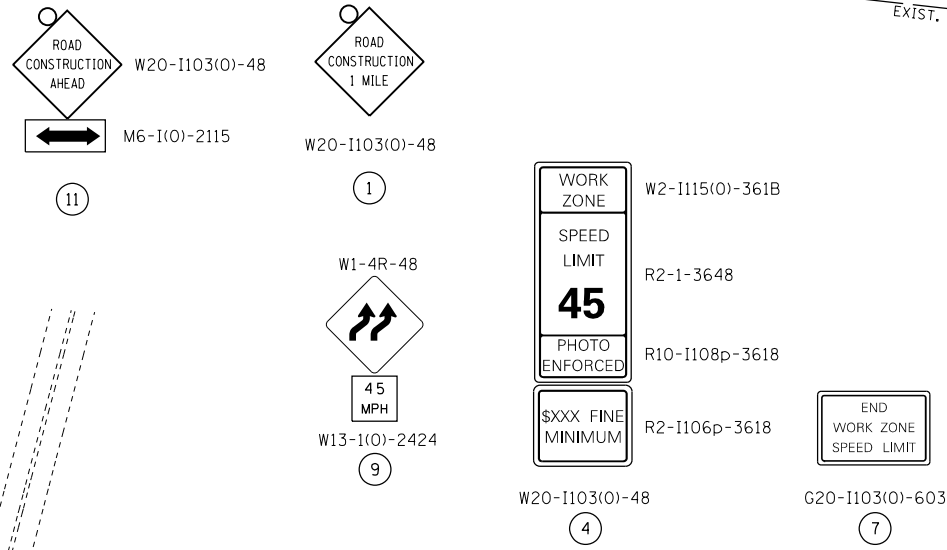
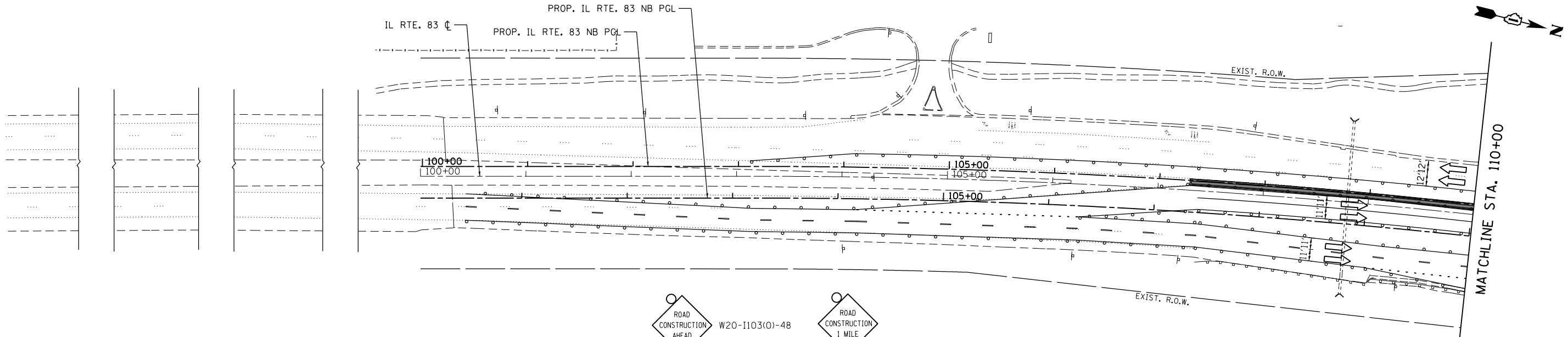
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/17/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

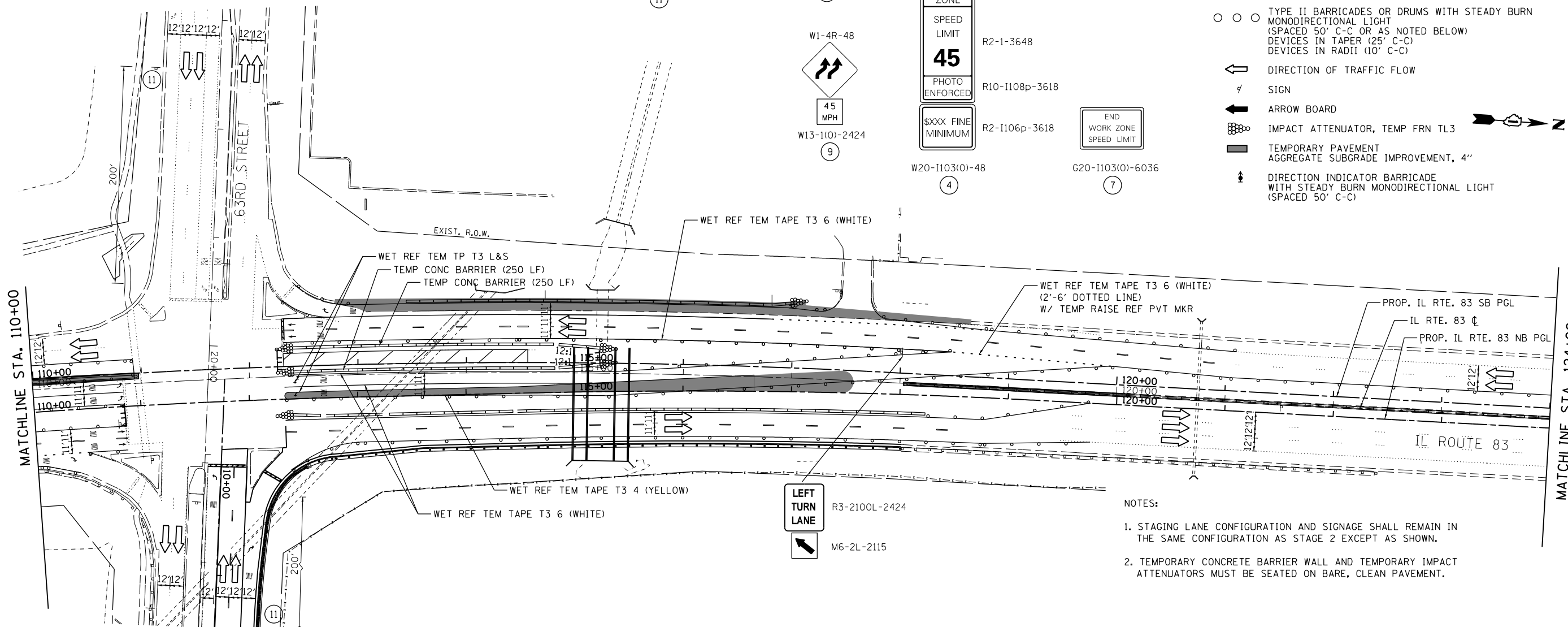
IL ROUTE 83  
MAINTENANCE OF TRAFFIC  
STAGE II

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	45
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W55	



- LEGEND:**
- CULVERT CONSTRUCTION
  - WORK AREA
  - TYPE II BARRICADES OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C OR AS NOTED BELOW) DEVICES IN TAPER (25' C-C) DEVICES IN RADII (10' C-C)
  - DIRECTION OF TRAFFIC FLOW
  - SIGN
  - ARROW BOARD
  - IMPACT ATTENUATOR, TEMP FRN TL3
  - TEMPORARY PAVEMENT AGGREGATE SUBGRADE IMPROVEMENT, 4"
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C)



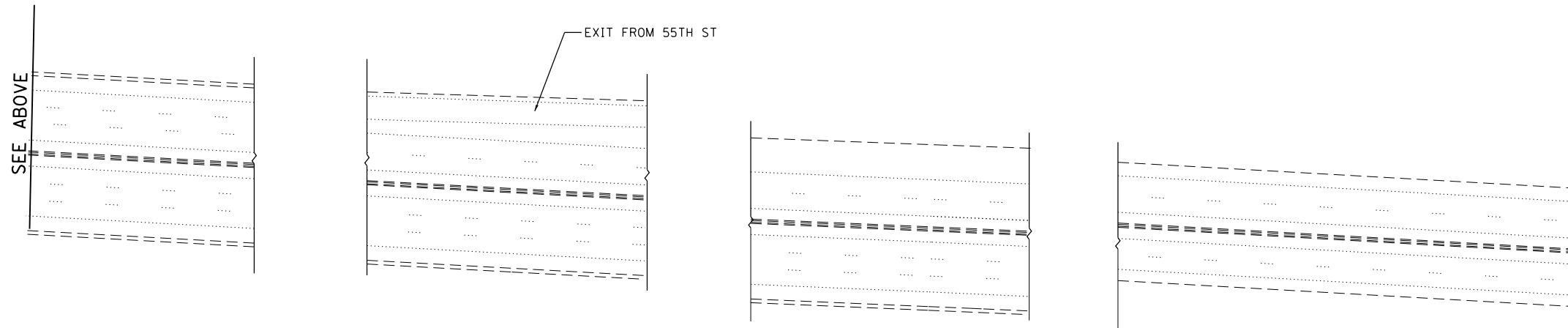
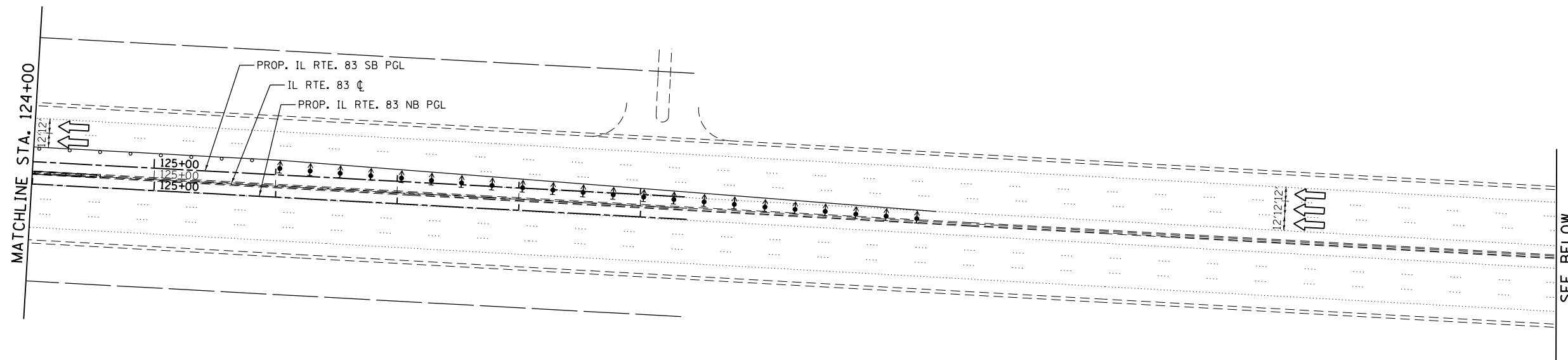
- NOTES:**
1. STAGING LANE CONFIGURATION AND SIGNAGE SHALL REMAIN IN THE SAME CONFIGURATION AS STAGE 2 EXCEPT AS SHOWN.
  2. TEMPORARY CONCRETE BARRIER WALL AND TEMPORARY IMPACT ATTENUATORS MUST BE SEATED ON BARE, CLEAN PAVEMENT.

FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -
I:\7290\7290.26 - IL83 and 63-d St\CA00	Exhibits\DI60W55-sht-staging_REV.SHT.dgn	DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/17/2014	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>IL ROUTE 83 MAINTENANCE OF TRAFFIC STAGE IIA</b>				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	46
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				

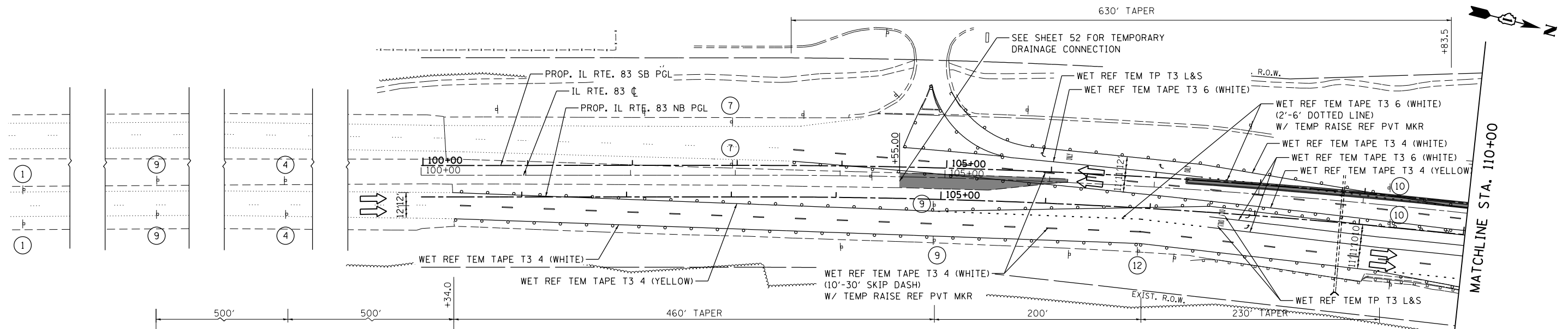


- LEGEND:**
- CULVERT CONSTRUCTION
  - WORK AREA
  - TYPE II BARRICADES OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C OR AS NOTED BELOW)  
DEVICES IN TAPER (25' C-C)  
DEVICES IN RADII (10' C-C)
  - DIRECTION OF TRAFFIC FLOW
  - SIGN
  - ARROW BOARD
  - IMPACT ATTENUATOR, TEMP FRN TL3
  - TEMPORARY PAVEMENT  
AGGREGATE SUBGRADE IMPROVEMENT, 4"
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C)

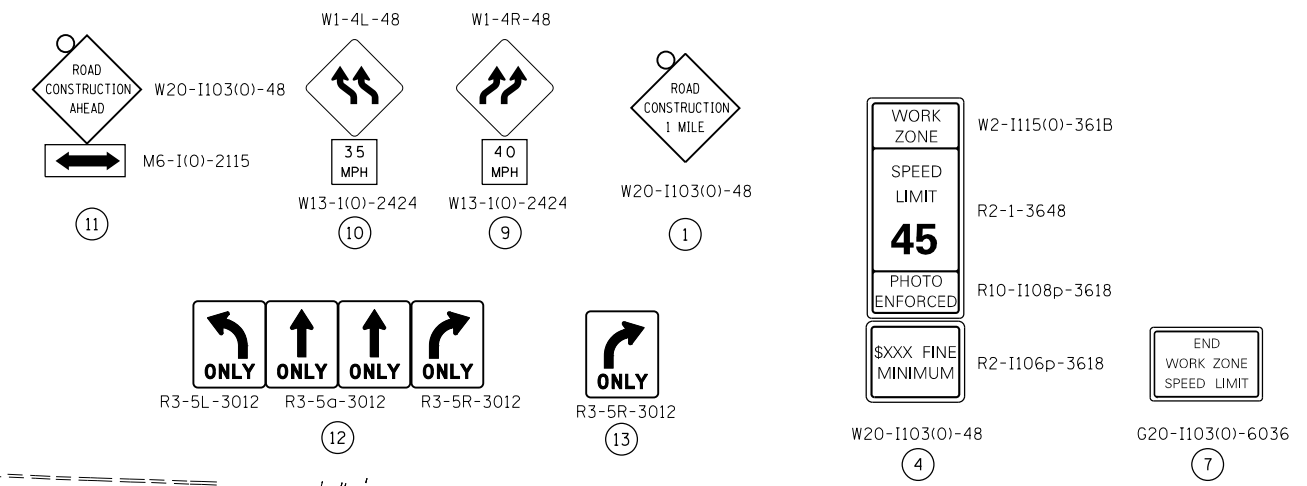
W1-4R-48	W4-2L(0)-48	W20-1103(0)-48	W20-7(0)-48	W20-1103(0)-48	W20-1103(0)-48	G20-1103(0)-6036
W13-1(0)-2424	W20-5L(0)-48	W21-1(0)-48	R2-1-3648	R10-1108p-3618	R2-1106p-3618	END WORK ZONE SPEED LIMIT
45 MPH	ROAD CONSTRUCTION 1 MILE	LEFT LANE CLOSED 1/2 MILE	SPEED LIMIT 45	SXXX FINE MINIMUM		
9	3	1	8	4		7

NOTES:  
 1. STAGING LANE CONFIGURATION AND SIGNAGE SHALL REMAIN IN THE SAME CONFIGURATION AS STAGE 2 EXCEPT AS SHOWN.  
 \* MUST BE REMOVED WHEN FLAGGER OR WORKERS ARE NOT PRESENT FOR MORE THAN ONE HOUR

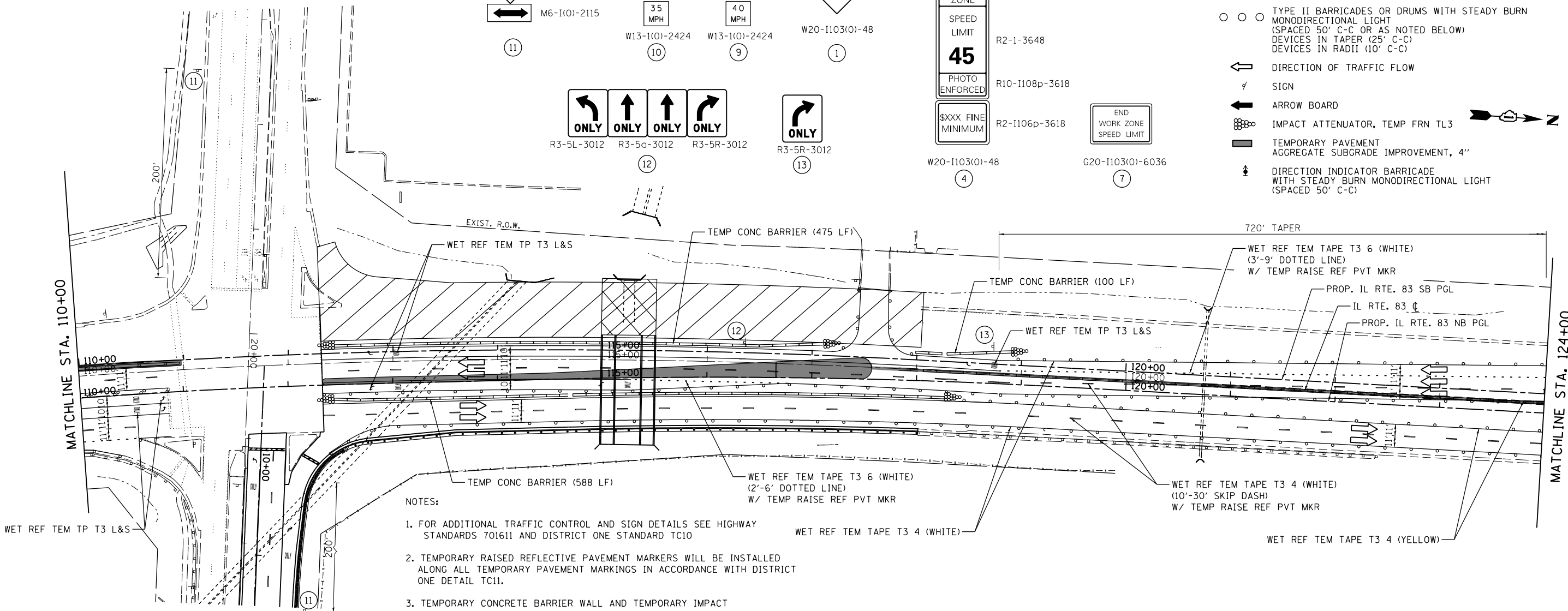
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -							ILLINOIS FED. AID PROJECT		
	PLOT DATE = 12/17/2014	DATE -	REVISED -									



PLACE 1 MILE IN ADVANCE OF BEGINNING OF WORK ZONE



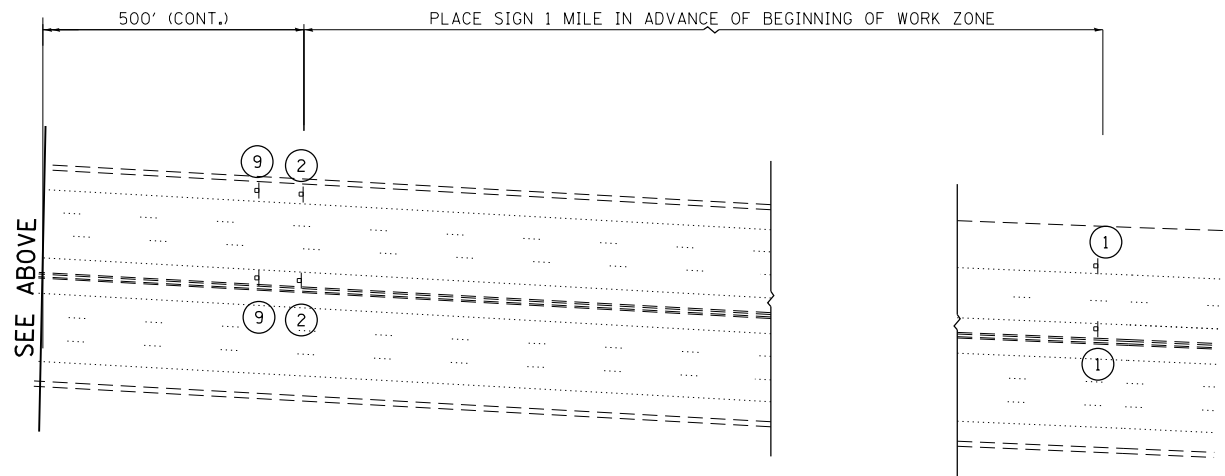
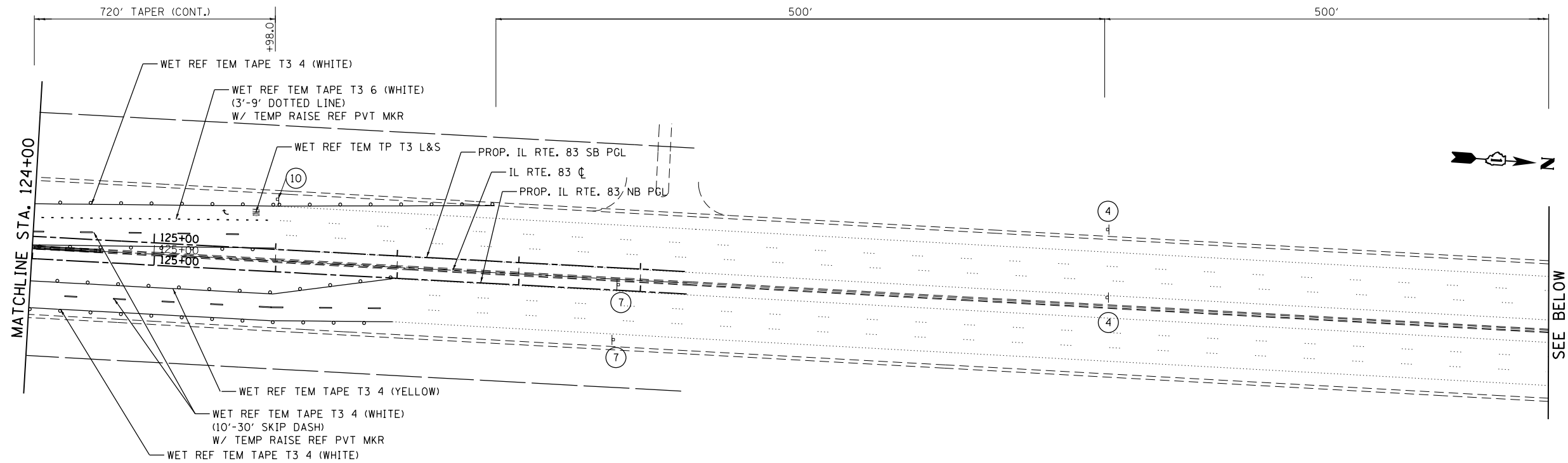
- LEGEND:**
- CULVERT CONSTRUCTION
  - WORK AREA
  - TYPE II BARRICADES OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C OR AS NOTED BELOW) DEVICES IN TAPER (25' C-C) DEVICES IN RADII (10' C-C)
  - DIRECTION OF TRAFFIC FLOW
  - SIGN
  - ARROW BOARD
  - IMPACT ATTENUATOR, TEMP FRN TL3
  - TEMPORARY PAVEMENT AGGREGATE SUBGRADE IMPROVEMENT, 4"
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C)



- NOTES:**
1. FOR ADDITIONAL TRAFFIC CONTROL AND SIGN DETAILS SEE HIGHWAY STANDARDS 701611 AND DISTRICT ONE STANDARD TC10
  2. TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS WILL BE INSTALLED ALONG ALL TEMPORARY PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC11.
  3. TEMPORARY CONCRETE BARRIER WALL AND TEMPORARY IMPACT ATTENUATORS MUST BE SEATED ON BARE, CLEAN PAVEMENT.

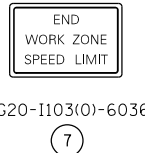
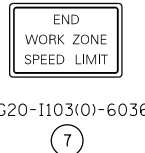
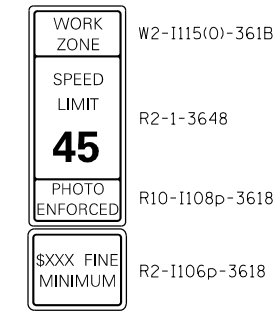
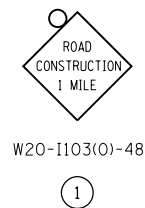
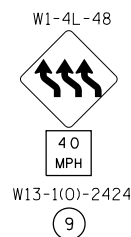
FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 83 MAINTENANCE OF TRAFFIC STAGE III</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
1:\7290\7290.26 - IL83 and 63-d St\CADD	Exhibits\DI60W55-sht-staging_REV.SHT.dgn	DRAWN -	REVISED -			344	(544 & 544-1)	TS&N (13)	DUPAGE	129	48
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60W55					
	PLOT DATE = 12/17/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					





**NOTES:**

- FOR ADDITIONAL TRAFFIC CONTROL AND SIGN DETAILS SEE HIGHWAY STANDARDS 701611.
- TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS WILL BE INSTALLED ALONG ALL TEMPORARY PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC11.
- TEMPORARY CONCRETE BARRIER WALL AND TEMPORARY IMPACT ATTENUATORS MUST BE SEATED ON BARE, CLEAN PAVEMENT.



- CULVERT CONSTRUCTION
- WORK AREA
- TYPE II BARRICADES OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C OR AS NOTED BELOW) DEVICES IN TAPER (25' C-C) DEVICES IN RADII (10' C-C)
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
- IMPACT ATTENUATOR, TEMP FRN TL3
- TEMPORARY PAVEMENT AGGREGATE SUBGRADE IMPROVEMENT, 4"
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C)

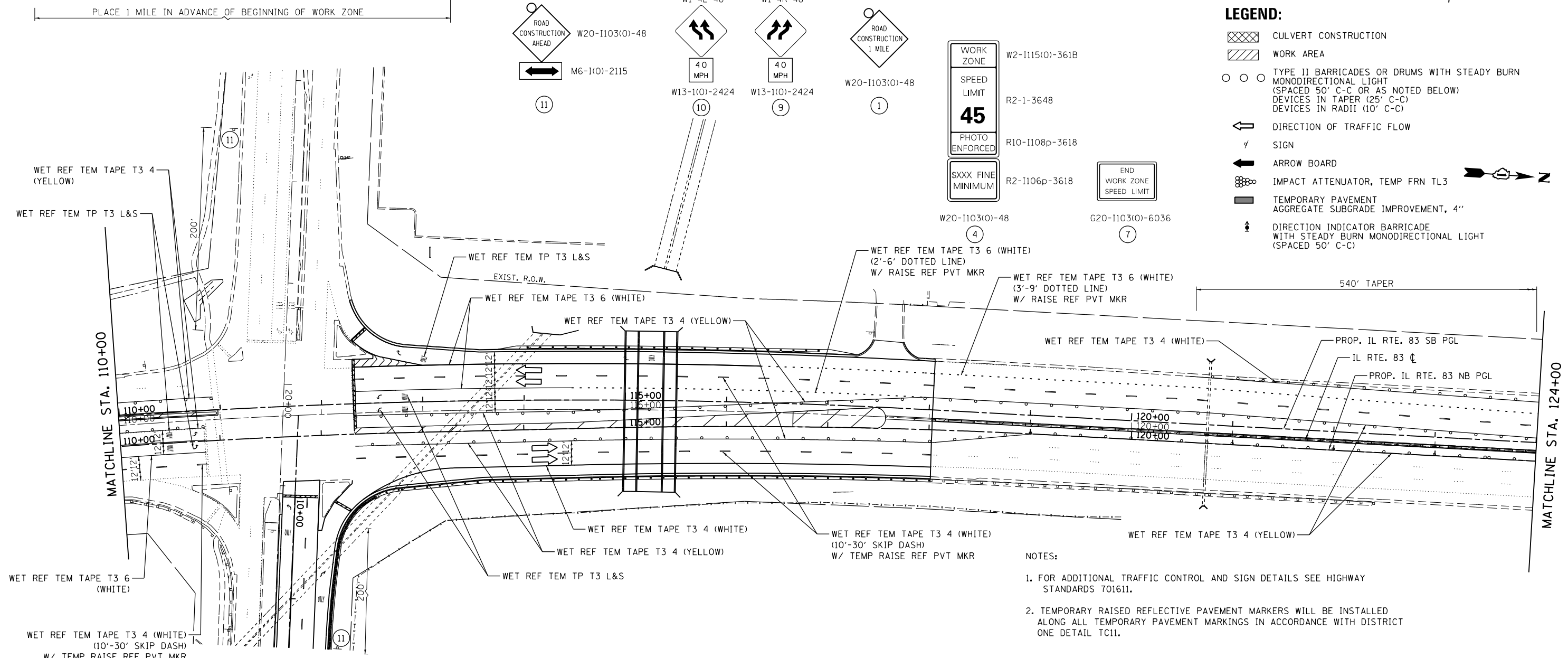
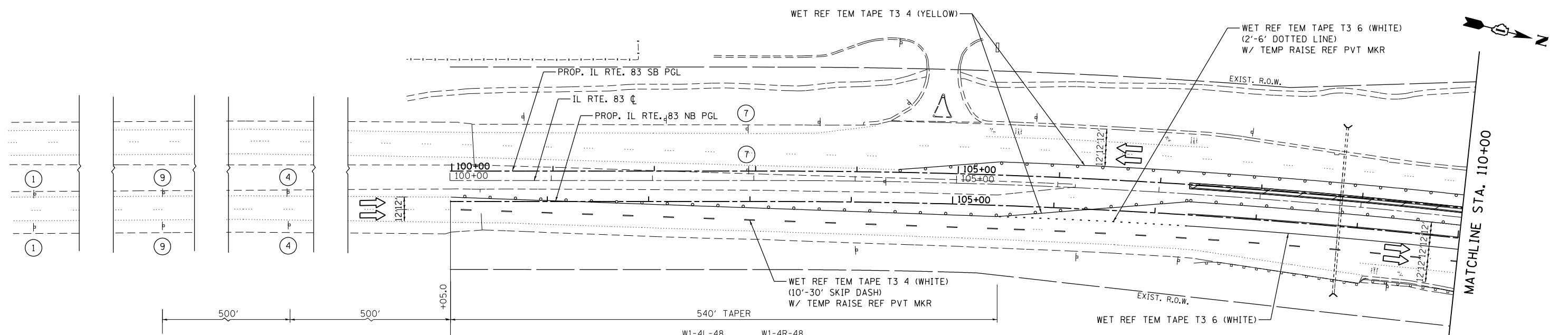
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/17/2014	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83  
MAINTENANCE OF TRAFFIC  
STAGE III**

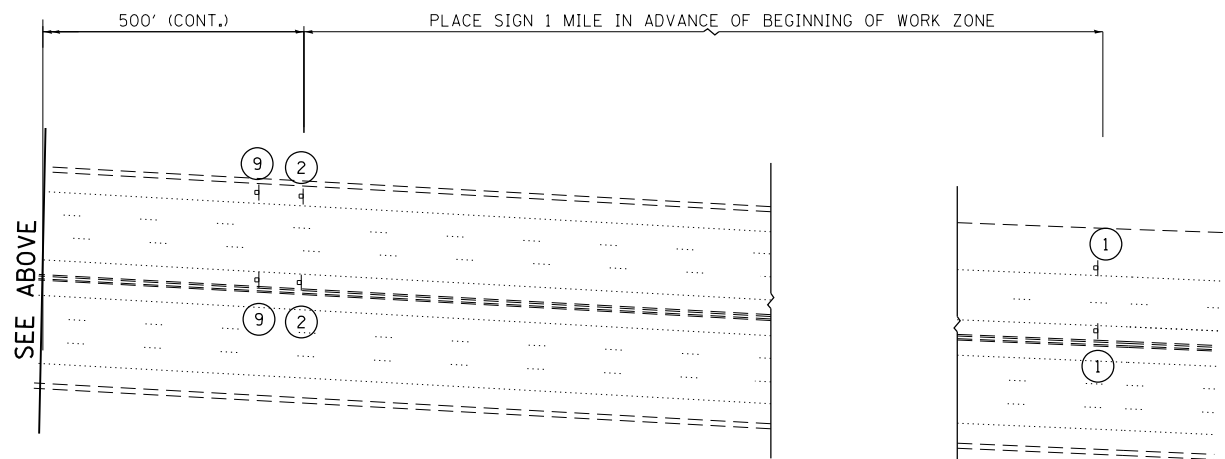
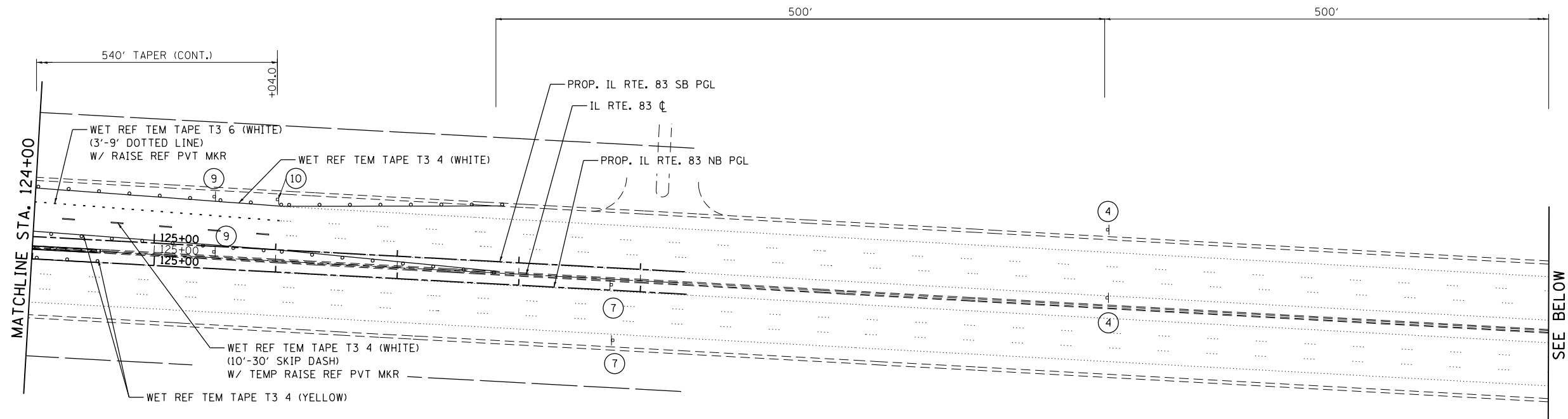
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	49
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W55	



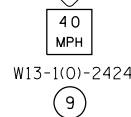
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -										CONTRACT NO. 60W55			
	PLOT DATE = 12/17/2014	DATE -	REVISED -										ILLINOIS FED. AID PROJECT			

- NOTES:
- FOR ADDITIONAL TRAFFIC CONTROL AND SIGN DETAILS SEE HIGHWAY STANDARDS 701611.
  - TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS WILL BE INSTALLED ALONG ALL TEMPORARY PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC11.



NOTES:

- FOR ADDITIONAL TRAFFIC CONTROL AND SIGN DETAILS SEE HIGHWAY STANDARDS 701611.
- TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS WILL BE INSTALLED ALONG ALL TEMPORARY PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC11.



9



1



2



4



7

- CULVERT CONSTRUCTION
- WORK AREA
- TYPE II BARRICADES OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C OR AS NOTED BELOW) DEVICES IN TAPER (25' C-C) DEVICES IN RADII (10' C-C)
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
- IMPACT ATTENUATOR, TEMP FRN TL3
- TEMPORARY PAVEMENT AGGREGATE SUBGRADE IMPROVEMENT, 4"
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT (SPACED 50' C-C)

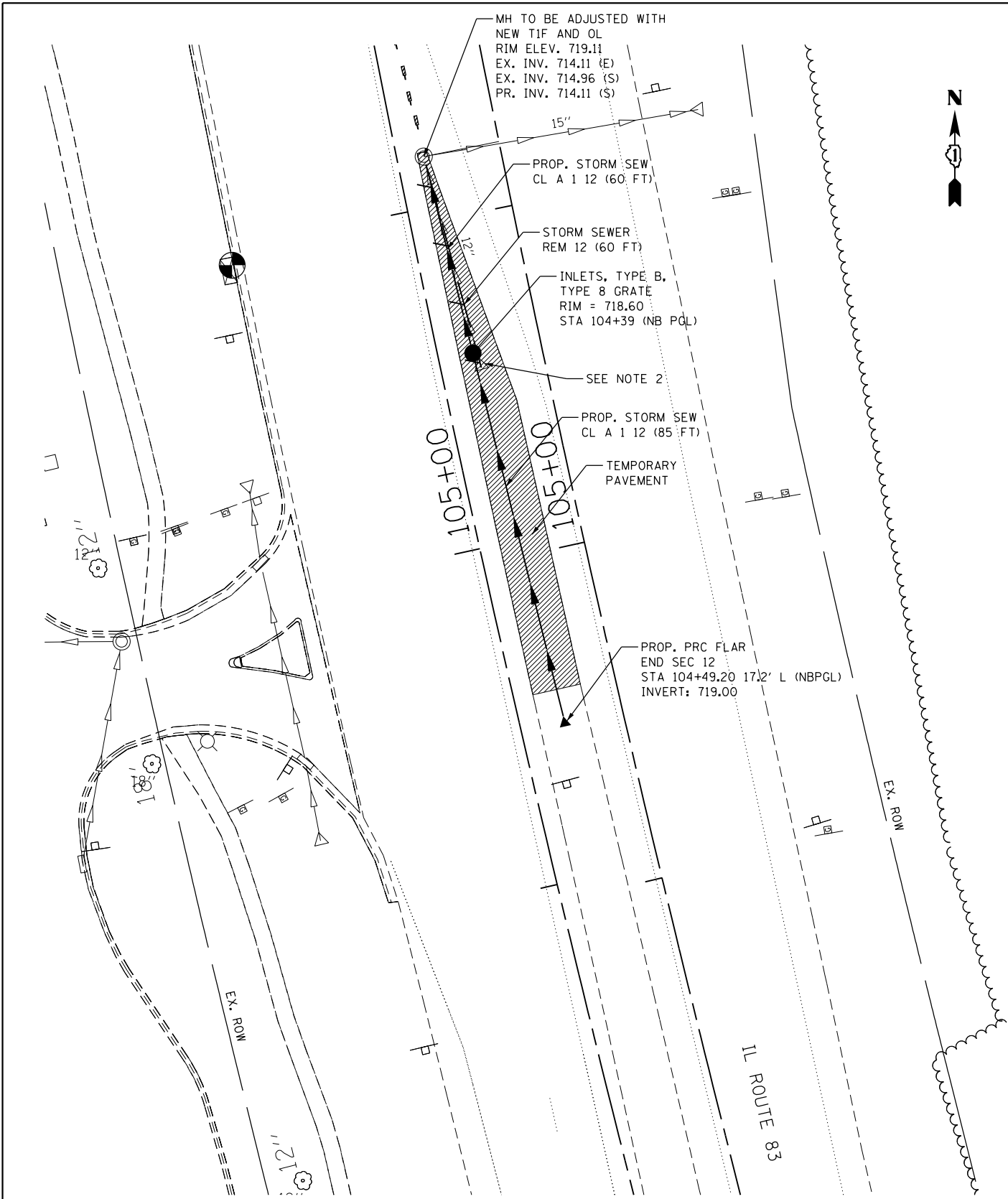
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/17/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

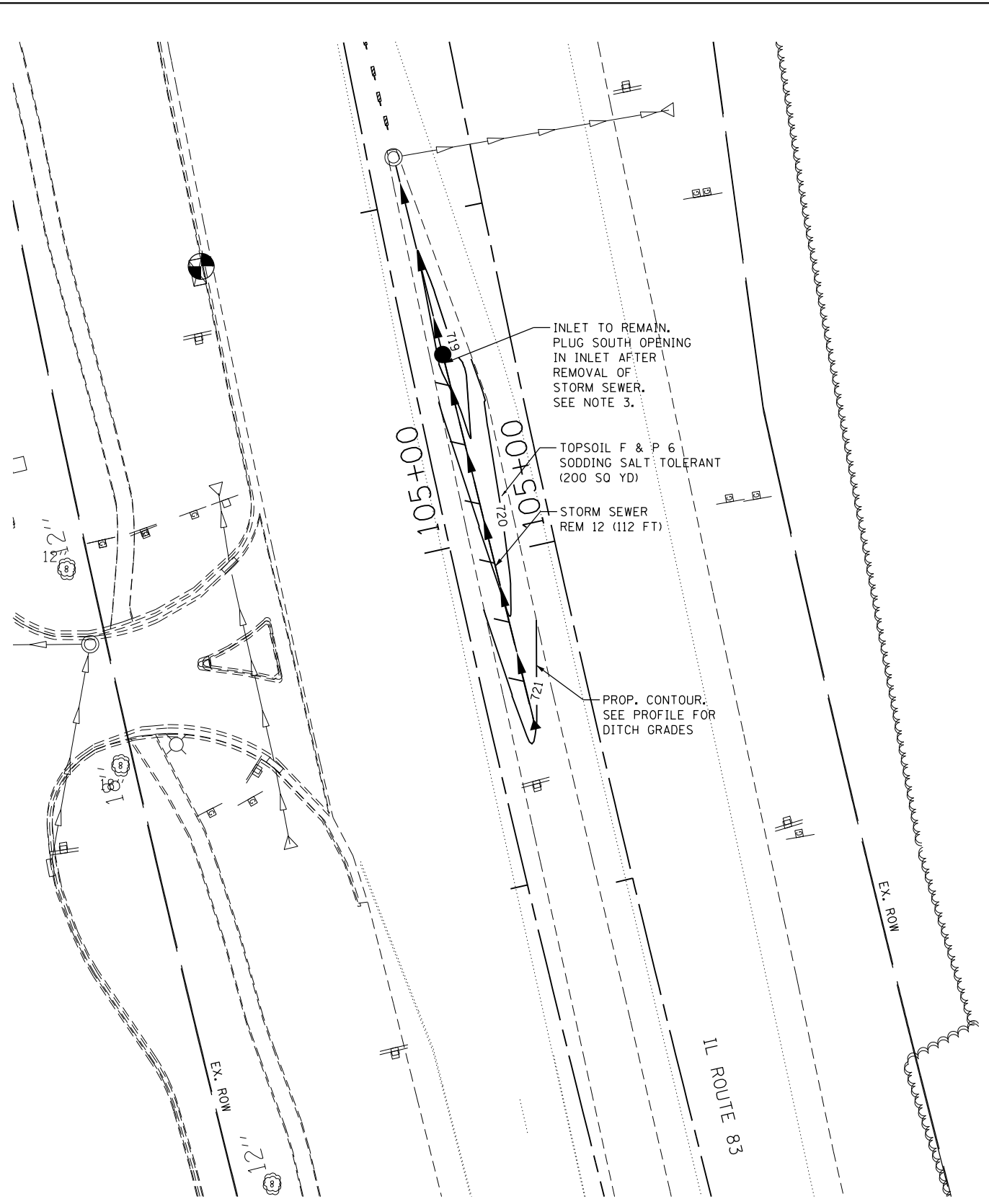
IL ROUTE 83  
MAINTENANCE OF TRAFFIC  
STAGE IV

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	51
CONTRACT NO. 60W55			ILLINOIS FED. AID PROJECT	



**TEMPORARY DRAINAGE CONNECTION  
IL ROUTE 83  
STAGE III CONSTRUCTION**



**FINAL CONDITION**

**NOTES:**

1. WORK THIS SHEET WITH MAINTENANCE OF TRAFFIC, IL ROUTE 83, STAGE III.
2. REMOVAL OF EXISTING FLARED END SECTION INCLUDED IN THE COST OF STORM SEWER REMOVAL, 12"
3. CONTRACTOR SHALL PLACE BRICKS AND MORTAR TO PLUG THE EXISTING OPENING CREATED AFTER REMOVAL OF THE STORM SEWER. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT INCLUDED IN THE COST OF STORM SEWER REMOVAL.

FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -
I:\7290\7290.26 - IL83 and 63-d St\CADD	CADD_Sheets\DI60W55-sht-temp_drainage.dgn	DRAWN -	REVISED -
Default	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/17/2014	DATE -	REVISED -

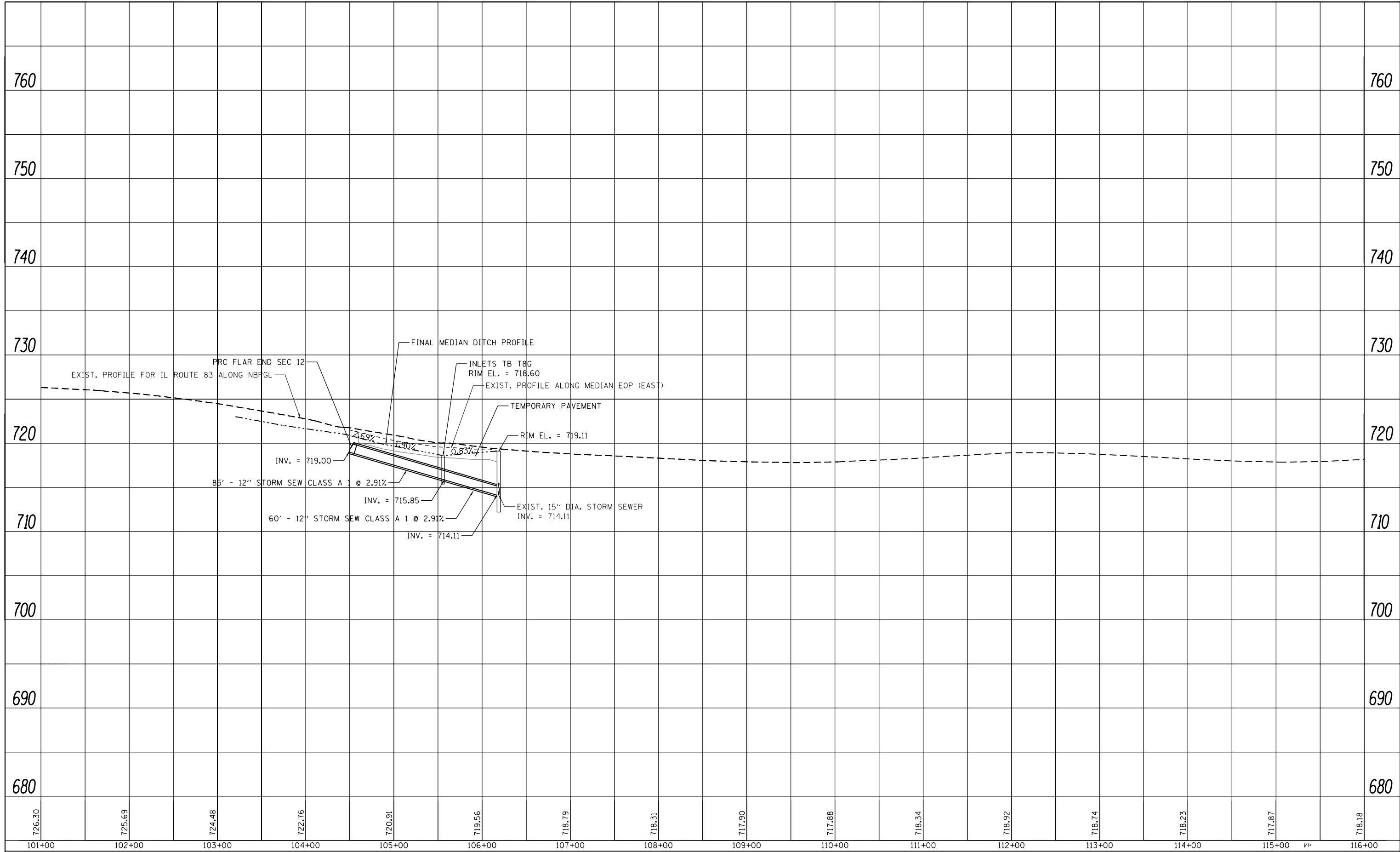
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>IL ROUTE 83 TEMPORARY DRAINAGE CONNECTION</b>			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	52
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	ALIGNED		
	CHECKED		
	CADD FILE NAME		
	NO.		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE		
	NOT AT THIS OFFICE		
	NO.		



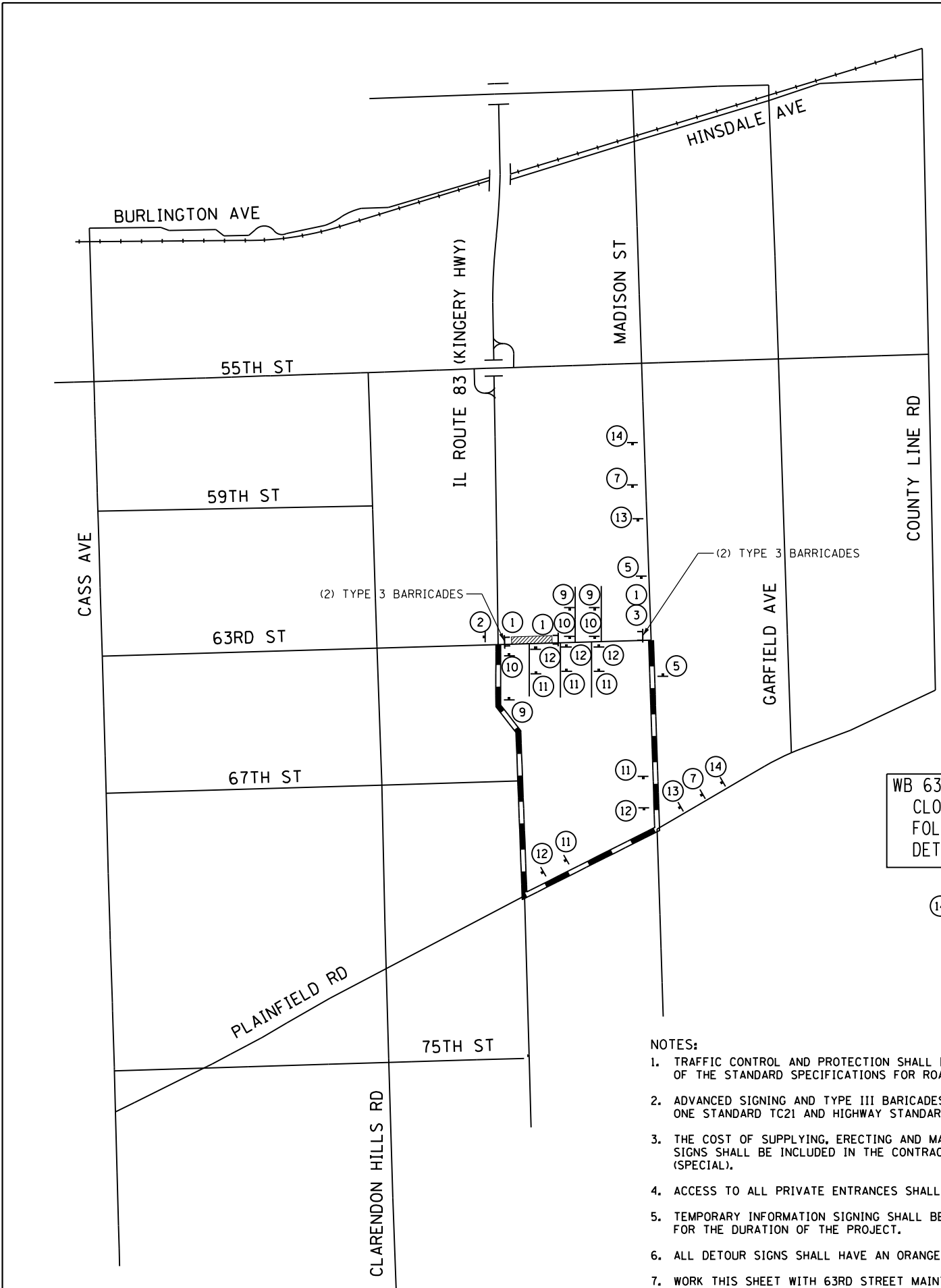
0.26 - IL83 and 63rd SNCADD\CADD\_Sheets\DI60W55-stt-temp-drain-prf.dgn

FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -
\\collinsengr.com\1\data-11\Poseidon\DCS	7290\7290.26 - IL83 and 63rd St\CADD\CADD_S	DRAWN	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/17/2014	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>IL ROUTE 83 TEMPORARY DRAINAGE PROFILE</b>			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	52A
CONTRACT NO. 60W55			ILLINOIS FED. AID PROJECT	



WB 63RD ST  
CLOSED  
FOLLOW  
DETOUR

5" BLACK LETTERS  
ON ORANGE REFLECTIVE  
BACKGROUND (48"x48")

**NOTES:**

1. TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 701 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
2. ADVANCED SIGNING AND TYPE III BARRICADES SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE STANDARD TC21 AND HIGHWAY STANDARD 701901.
3. THE COST OF SUPPLYING, ERECTING AND MAINTAINING BARRICADES, WARNING LIGHTS AND SIGNS SHALL BE INCLUDED IN THE CONTRACT COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
4. ACCESS TO ALL PRIVATE ENTRANCES SHALL BE MAINTAINED.
5. TEMPORARY INFORMATION SIGNING SHALL BE PLACED AT EACH END OF 63RD STREET FOR THE DURATION OF THE PROJECT.
6. ALL DETOUR SIGNS SHALL HAVE AN ORANGE BACKGROUND WITH BLACK LETTERS.
7. WORK THIS SHEET WITH 63RD STREET MAINTENANCE OF TRAFFIC, STAGE I PLAN.

ROAD CLOSED

① R11-2 (48" X 30")

ROAD CLOSED  
1/2 MILE AHEAD

①A R11-2 (48" X 30")

WEST  
63rd Street  
DETOUR  
←

⑩

END  
DETOUR

② M4-8N (24" X 18")

← DETOUR

③ M4-10L (48"x18")

WEST  
63rd Street  
DETOUR  
↶

⑪

WESTBOUND  
63RD ST  
CLOSED

LOCAL  
TRAFFIC  
ONLY

R11-5 (24"x24")

⑤

WEST  
63rd Street  
DETOUR  
→

⑫

ROAD CLOSED  
500 FT

⑥ W20-3 (48" X 48")

DETOUR  
AHEAD

⑦ W20-2 (48" X 48")

WEST  
63rd Street  
DETOUR  
↶

⑨

ROAD CLOSED  
AHEAD

⑧ W20-3 (48" X 48")

WEST  
63rd Street  
DETOUR  
↑

⑬

**LEGEND:**

- DETOUR ROUTE
- CONSTRUCTION ZONE
- TYPE III BARRICADE
- AMBER FLASHING LIGHT

FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -
I:\7290\7290.26 - IL83 and 63rd St\CADD	CADD_Sheets\DI60W55-sht-staging detour.dgn	DRAWN -	REVISED -
Default	PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/17/2014	DATE -	REVISED -

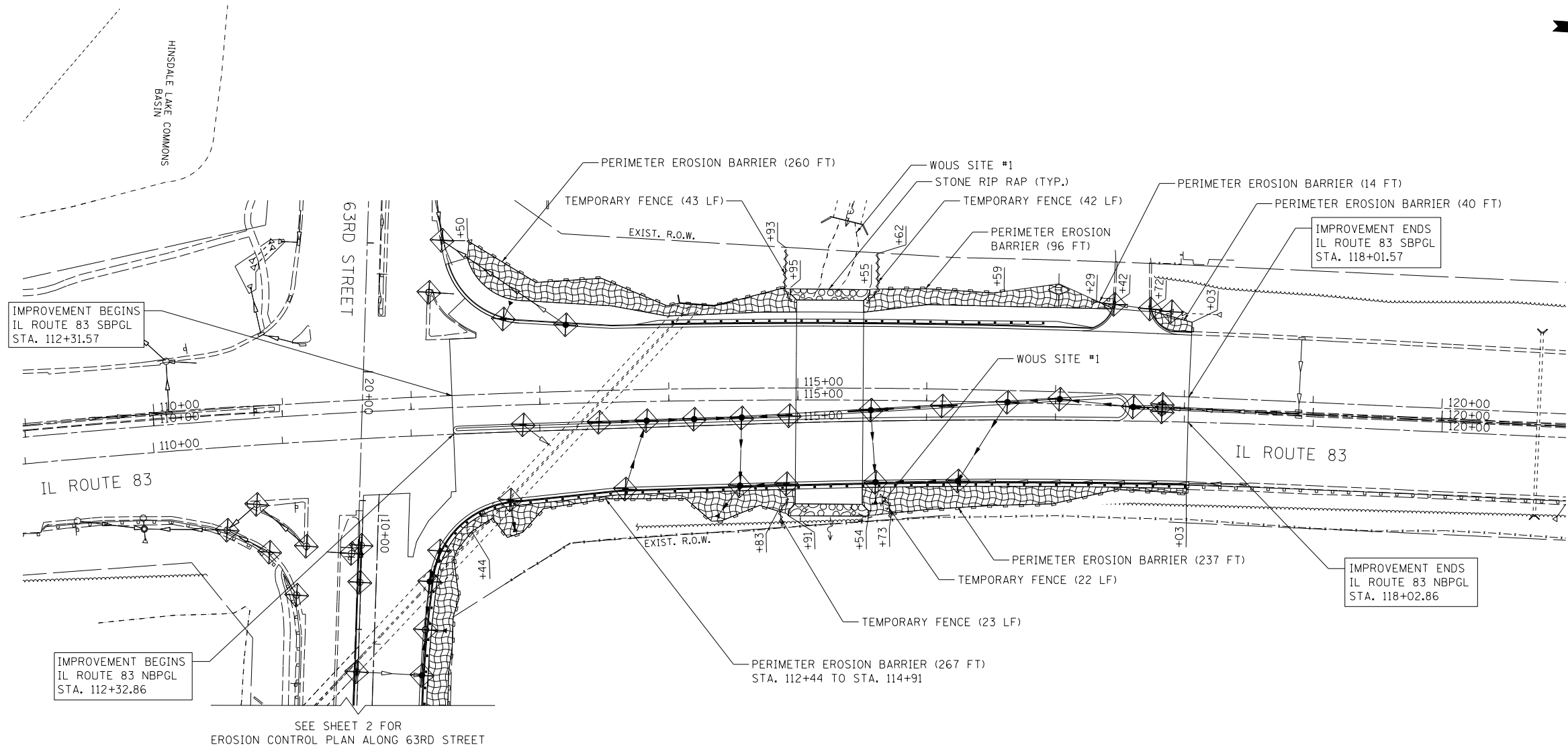
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>63RD STREET DETOUR PLAN</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.







344	(544 & 544-1)	TS&N (13)	DUPAGE	129	53
CONTRACT NO. 60W55					
ILLINOIS FED. AID PROJECT					

NOTES:

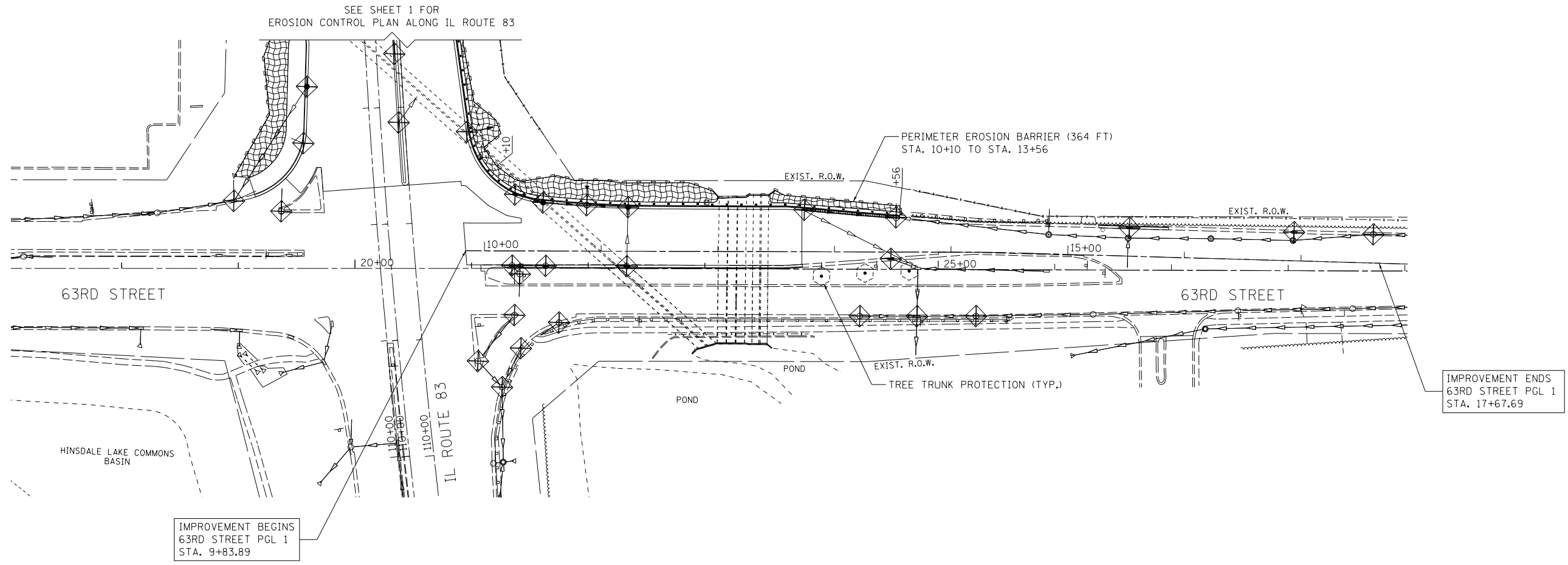
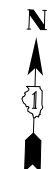
1. SEE STRUCTURAL PLANS FOR STONE RIPRAP DETAILS.
2. TEMPORARY EROSION CONTROL SEEDING SHALL BE APPLIED AT 100 POUNDS PER ACRE FOR 4 APPLICATIONS.









**EROSION CONTROL LEGEND:**

-  TEMPORARY DITCH CHECK
-  PERIMETER EROSION BARRIER
-  EROSION CONTROL BLANKET W/ TEMPORARY SEEDING
-  INLET FILTERS / INLET & PIPE PROTECTION
-  TREE TRUNK PROTECTION
-  TEMPORARY FENCE

FILE NAME =	USER NAME = \$USER\$	DESIGNED - GRE	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EROSION CONTROL PLAN IL ROUTE 83</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
\$FILEL\$		DRAWN - GRE	REVISED -		SCALE: 1"=50'	SHEET 1 OF 2 SHEETS	STA.	TO STA.	344	(544 & 544-1) TS&N (13)	DUPAGE	129	54
\$MODELNAME\$		CHECKED - ST	REVISED -										
		DATE - 11/2014	REVISED -										

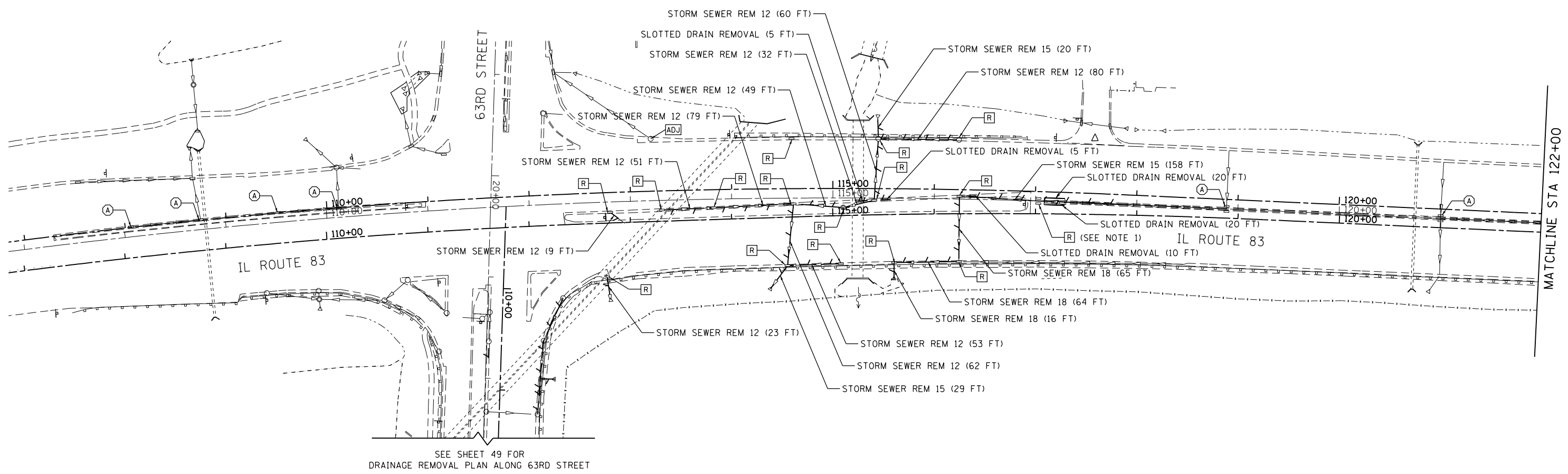


**EROSION CONTROL LEGEND:**

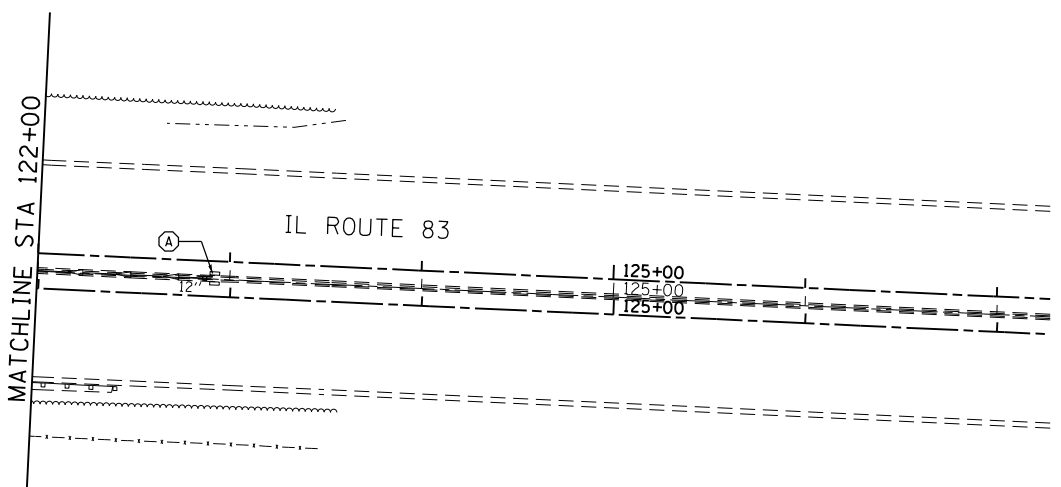
-  TEMPORARY DITCH CHECK
-  PERIMETER EROSION BARRIER
-  EROSION CONTROL BLANKET W/ TEMPORARY SEEDING
-  INLET FILTERS / INLET & PIPE PROTECTION
-  TREE TRUNK PROTECTION
-  TEMPORARY FENCE

FILE NAME = \$FILEL\$	USER NAME = \$USER\$	DESIGNED - GRE	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EROSION CONTROL PLAN 63RD STREET</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = \$SCALE\$	CHECKED - ST	REVISED -		344	(544 & 544-1) TS&N (13)	DUPAGE	129	55		
\$MODELNAME\$	PLOT DATE = \$DATE\$	DATE - 11/2014	REVISED -	SCALE: 1"=50'	SHEET 2 OF 2 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT CONTRACT NO. 60W55				





SEE SHEET 49 FOR DRAINAGE REMOVAL PLAN ALONG 63RD STREET



**REMOVAL LEGEND:**

- STORM SEWER REMOVAL
- STRUCTURE TO BE REMOVED
- FRAMES AND LIDS ADJUSTED SPECIAL
- STRUCTURE TO BE ADJUSTED

NOTE 1.  
REMOVAL OF EXISTING MEDIAN DRAINAGE STRUCTURE TY 1 (STD. 602101)  
SHALL BE PAID FOR AS "DRAINAGE STRUCTURE TO BE REMOVED"

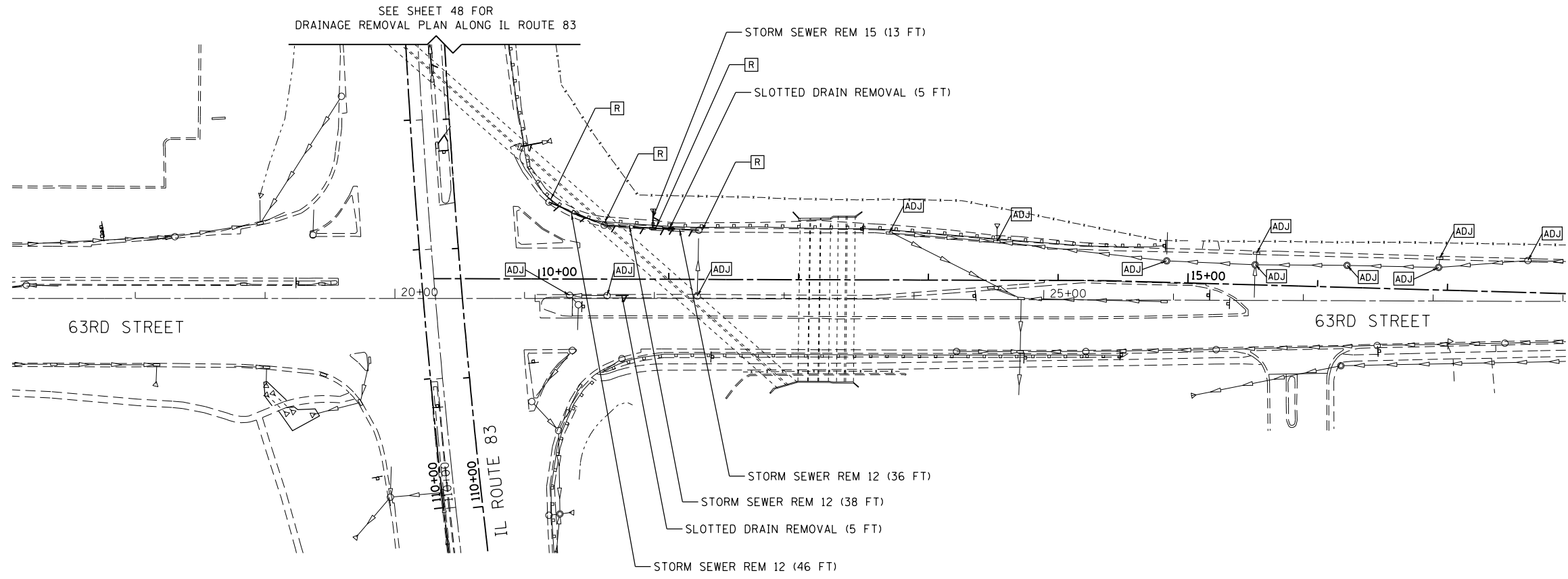
FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -
I:\7290\7290.26 - IL83 and 63rd St\CADD	CADD_Sheets\DI60W55-sht-drain-rem-83.dgn	DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 11/26/2014	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

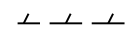


**IL ROUTE 83  
DRAINAGE REMOVAL PLAN**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	56
				CONTRACT NO. 60W55
ILLINOIS FED. AID PROJECT				



**REMOVAL LEGEND:**

-  STORM SEWER REMOVAL
-  STRUCTURE TO BE REMOVED
-  STRUCTURE TO BE ADJUSTED

FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -
I:\7290\7290.26 - IL83 and 63rd St\CADD	CADD_Sheets\DI60W55-sht-drain-rem-63.dgn	DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 11/26/2014	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

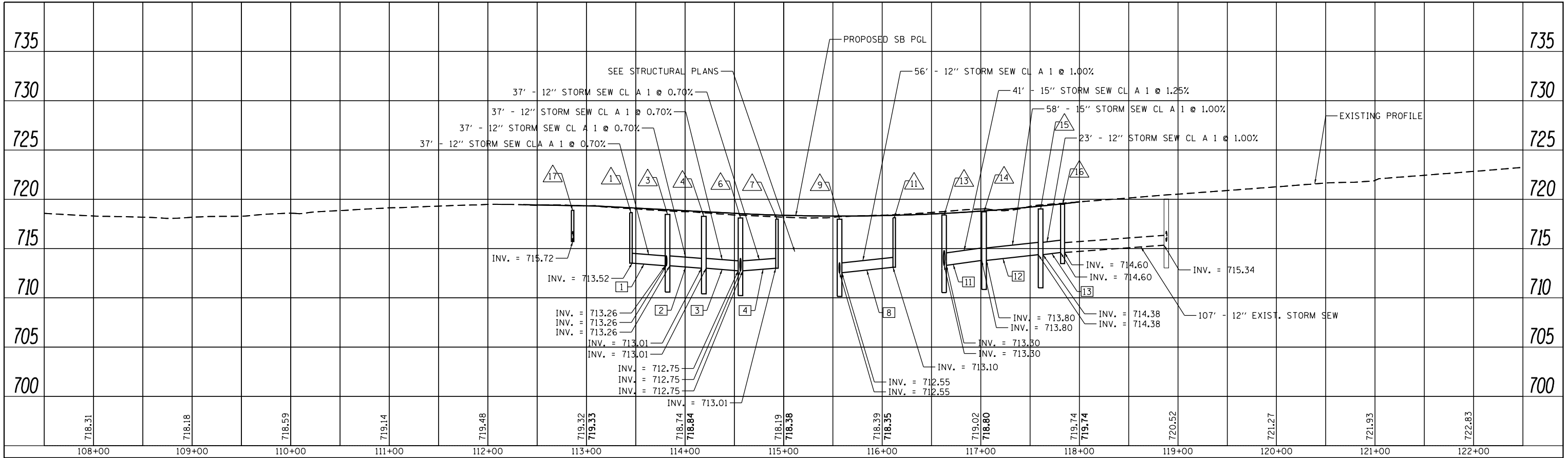
**63RD STREET  
DRAINAGE REMOVAL PLAN**

SCALE:      SHEET      OF      SHEETS      STA.      TO      STA.

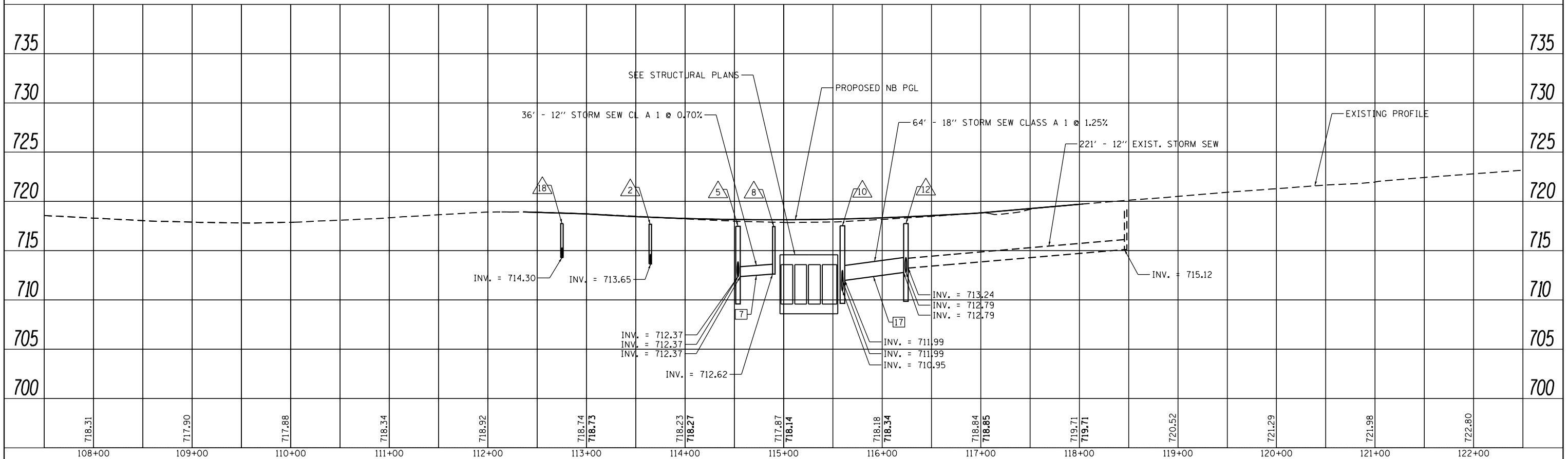
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	57
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				



PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NO.	



PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NO.	

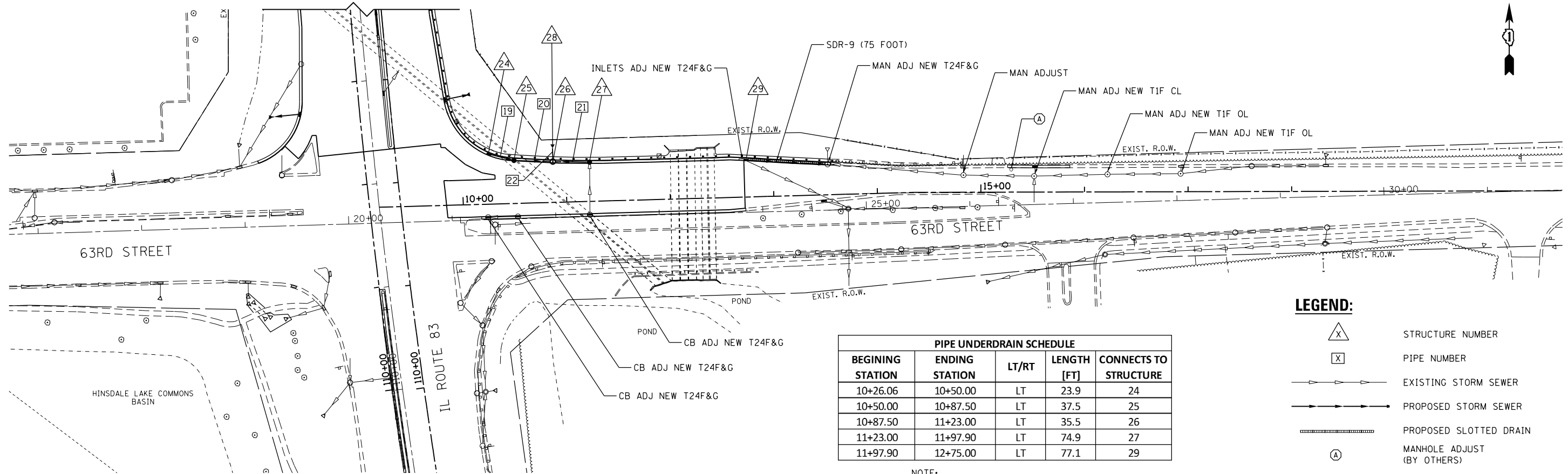


FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 83 DRAINAGE PROFILE</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	1:\7290\7290.26 - IL83 and 63rd St\CADD\CADD.Sheets\DI60W55-shd-drain-prf-83.dgn	DRAWN -	REVISED -		344	(544 & 544-1) TS&N (13)	DUPAGE	129	59			
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60W55							
	PLOT DATE = 11/26/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

PLAN	SURVEYED	DATE
	PLOTTED	BY
	ALIGNED	
	CHECKED	
	FILE NAME	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NO.	

SEE SHEET 50 FOR  
PROPOSED DRAINAGE ALONG IL ROUTE 83



**LEGEND:**

- STRUCTURE NUMBER
- PIPE NUMBER
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- PROPOSED SLOTTED DRAIN
- MANHOLE ADJUST (BY OTHERS)

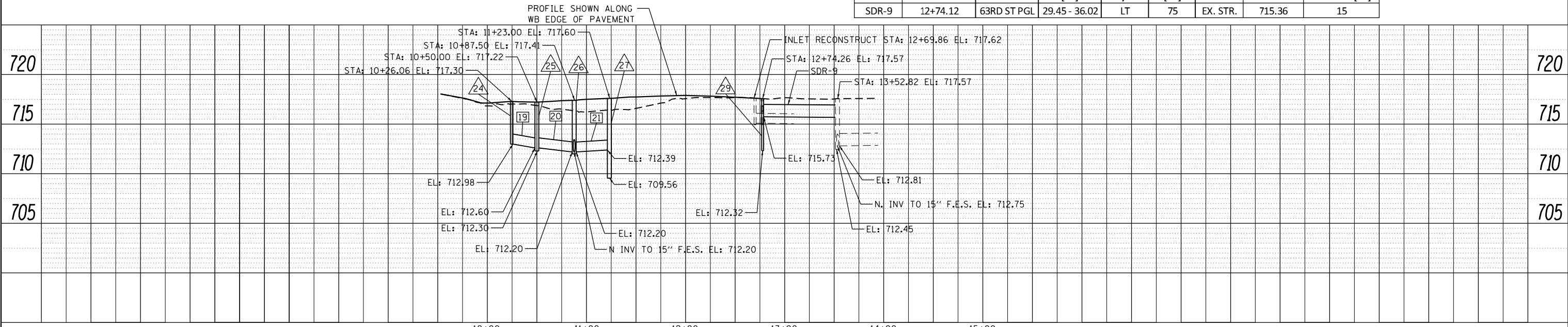
PIPE UNDERDRAIN SCHEDULE				
BEGINNING STATION	ENDING STATION	LT/RT	LENGTH [FT]	CONNECTS TO STRUCTURE
10+26.06	10+50.00	LT	23.9	24
10+50.00	10+87.50	LT	37.5	25
10+87.50	11+23.00	LT	35.5	26
11+23.00	11+97.90	LT	74.9	27
11+97.90	12+75.00	LT	77.1	29

NOTE:  
CONNECT UNDERDRAIN TO STRUCTURE  
COST INCLUDED IN THE COST OF THE  
DRAINAGE STRUCTURE

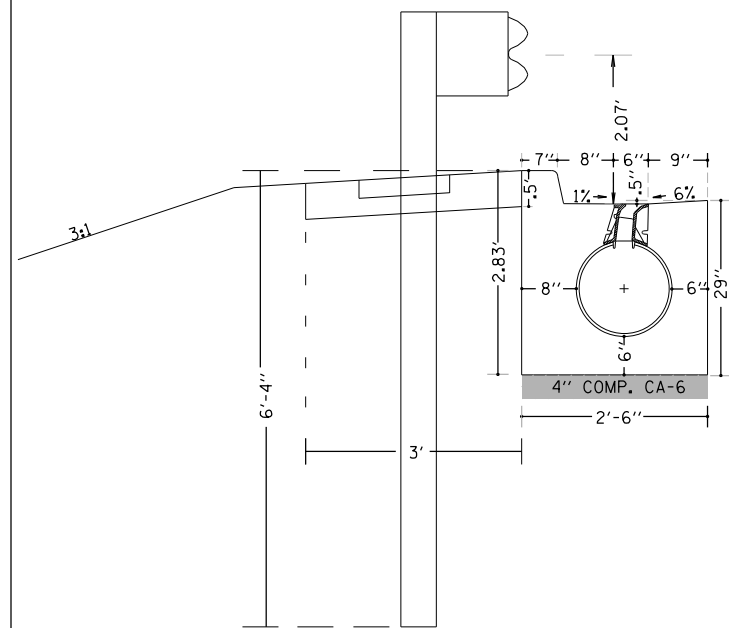
PIPE SCHEDULE					
PIPE NO.	DESCRIPTION	DIA. [IN]	L [FT]	S <sub>p</sub>	TBF [CU YD]
19	STORM SEW CL A 2 12	12	25	1.63%	8.00
20	STORM SEW CL A 2 12	12	38	1.05%	14.00
21	STORM SEW CL A 2 12	12	36	0.53%	23.00
22	STORM SEW CL A 1 15	15	15	3.10%	-

STRUCTURE SCHEDULE													
STR. NO.	PGL STATION	OFFSET		STRUCTURE			F&G	STR. INVERT	INVERT				EOP ELEVATION
		[FT]	LT/RT	INLET	CB	MH			N	S	E	W	
24	10+26.06	48.32	LT	A			24	712.98			712.98		717.30
25	10+50	41.28	LT		D 4 (ECC CONE TOP)		24	709.77	713.41 NW		712.60	712.60	717.34
26	10+87.50	38.25	LT			A 4 (FLAT TOP)	24	712.00	712.20		712.20	712.20	717.44
27	11+23	36.67	LT		D 4 (ECC CONE TOP)		24	709.56		712.39		712.39	717.60
28	10+87.50	55.40	LT		FES 15"				714.74				
29	12+75	34.51	LT	A			24				715.73		717.60

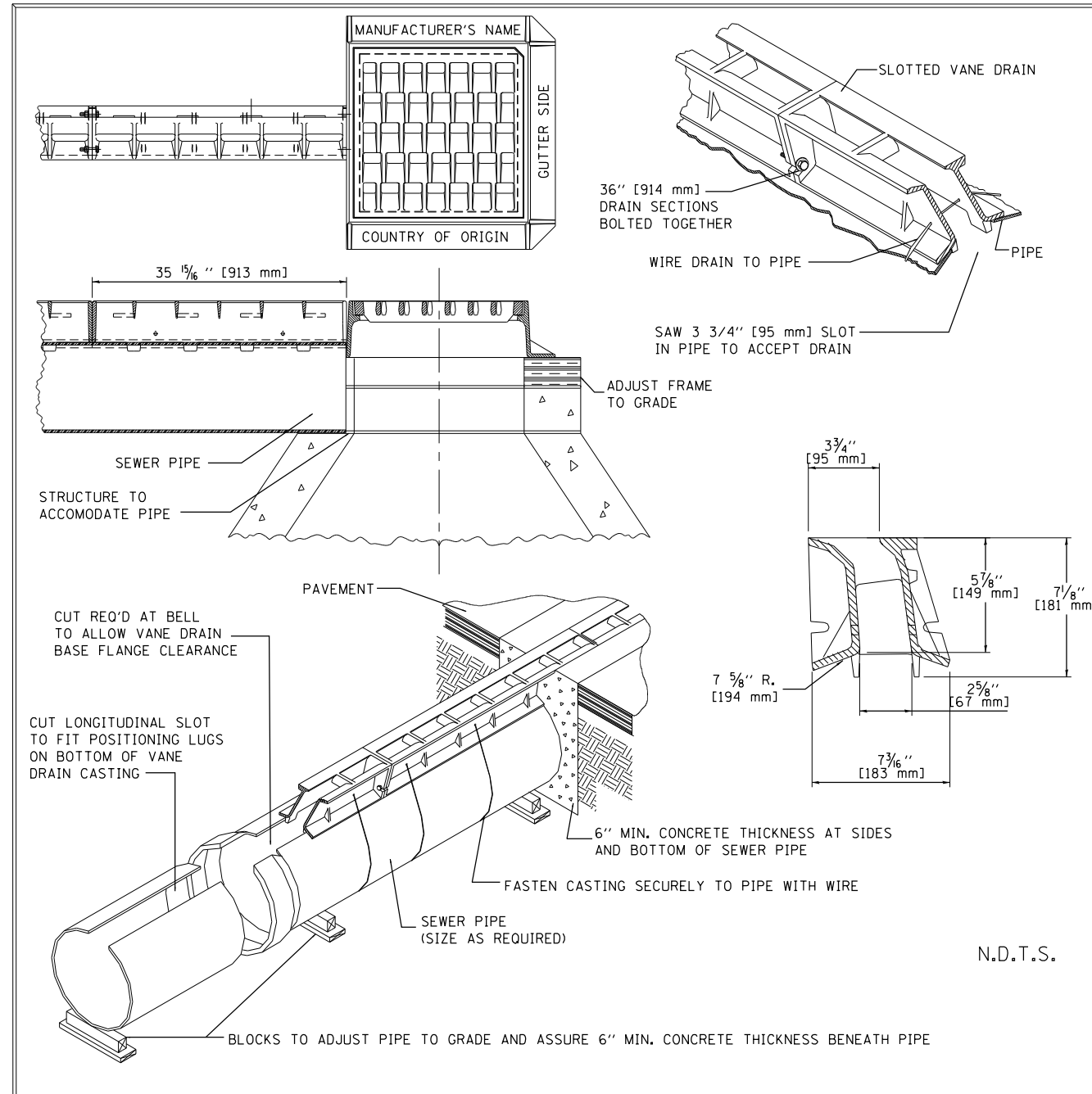
SLOTTED DRAIN SCHEDULE								
SLOTTED DRAIN	BEGINNING STATION	ALIGNMENT	OFFSET		LENGTH	DRIANS TO STR.	OUTLET INVERT	SLOTTED DRAIN PIPE SIZE [IN]
			[FT]	LT/RT	[FT]			
SDR-9	12+74.12	63RD ST PGL	29.45 - 36.02	LT	75	EX. STR.	715.36	15



SLOTTED VANE DRAIN DETAIL  
STA. 42+24 TO STA 43+99



N.D.T.S.



N.D.T.S.

SPECIFICATIONS

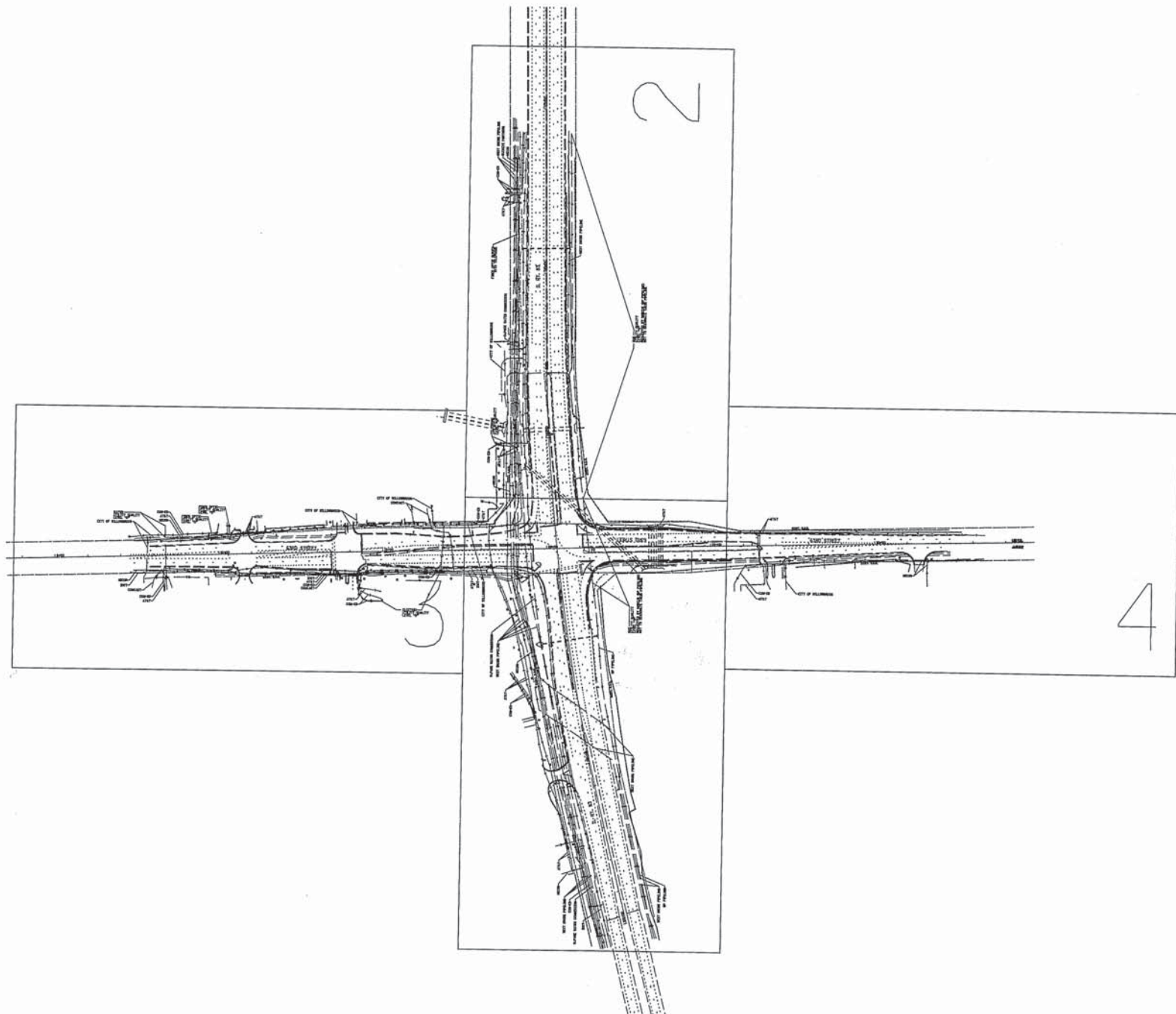
CAST GRAY IRON;  
ASTM A48 (AASHTO M105 AND AASHTO M306)  
CLASS 35B

EFFICIENCY

THE SLOTTED VANE DRAIN REMOVES SHEET FLOW AT A RATE OF 0.5 CFS PER LINEAL FOOT OF DRAIN FOR LONGITUDINAL SLOPES OF 0% TO 6%.

SLOTTED VANE DRAIN

FILE NAME =	USER NAME = rge11	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 83 AT MARION HILLS DITCH DRAINAGE DETAILS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
I:\7290\7290.26 - IL83 and 63rd St\CADD	CADD_Sheets\DI60W55-sht-details-slotted vane	DRAWN -	REVISED -			344	(544 & 544-1) TS&N (13)	DUPAGE	129	61	
Default	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60W55					
	PLOT DATE = 11/26/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	

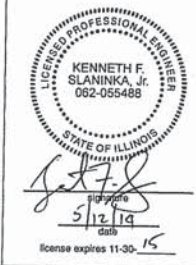


— A —	A —	AERIAL
— — —	— — —	UNKNOWN
— CTV —	CTV —	CABLE TV
— T —	T —	TELEPHONE
— G —	G —	GAS
— E —	E —	ELECTRIC
— W —	W —	WATER
— FO —	FO —	FIBER OPTIC
⊕		TBE TEST HOLE

UTILITY OWNERS	
AT&T	= FIBER OPTIC
AT&T	= TELEPHONE
BP PIPELINE	= GAS
BUCKEYE PARTNERS	= GAS
CITY OF WILLOWBROOK	= WATER
COMCAST	= CABLE TV
COM-ED	= ELECTRIC
DUPAGE WATER COMMISSION	= WATER
IDOT	= FIBER OPTIC
NICOR	= GAS
WEST SHORE PIPELINE	= GAS

Utilities shown on these plans as depicted in the legend have been investigated by Cardno TBE in accordance with SUE Industry Standards. All other information shown has been provided to Cardno TBE by others. Cardno TBE's QL"B" SUE field investigation was performed 3/31/14 through 4/30/14. Changes to utilities after 4/30/14 may have been made and therefore may result in variances from this plan. Consideration should be given to updating this plan if deemed advisable prior to final design and construction.

ALL UTILITIES SHOWN QUALITY LEVEL "B"  
UNLESS NOTED OTHERWISE.



TBE Job No. IL09510562  
SUE Plan Page: Cover

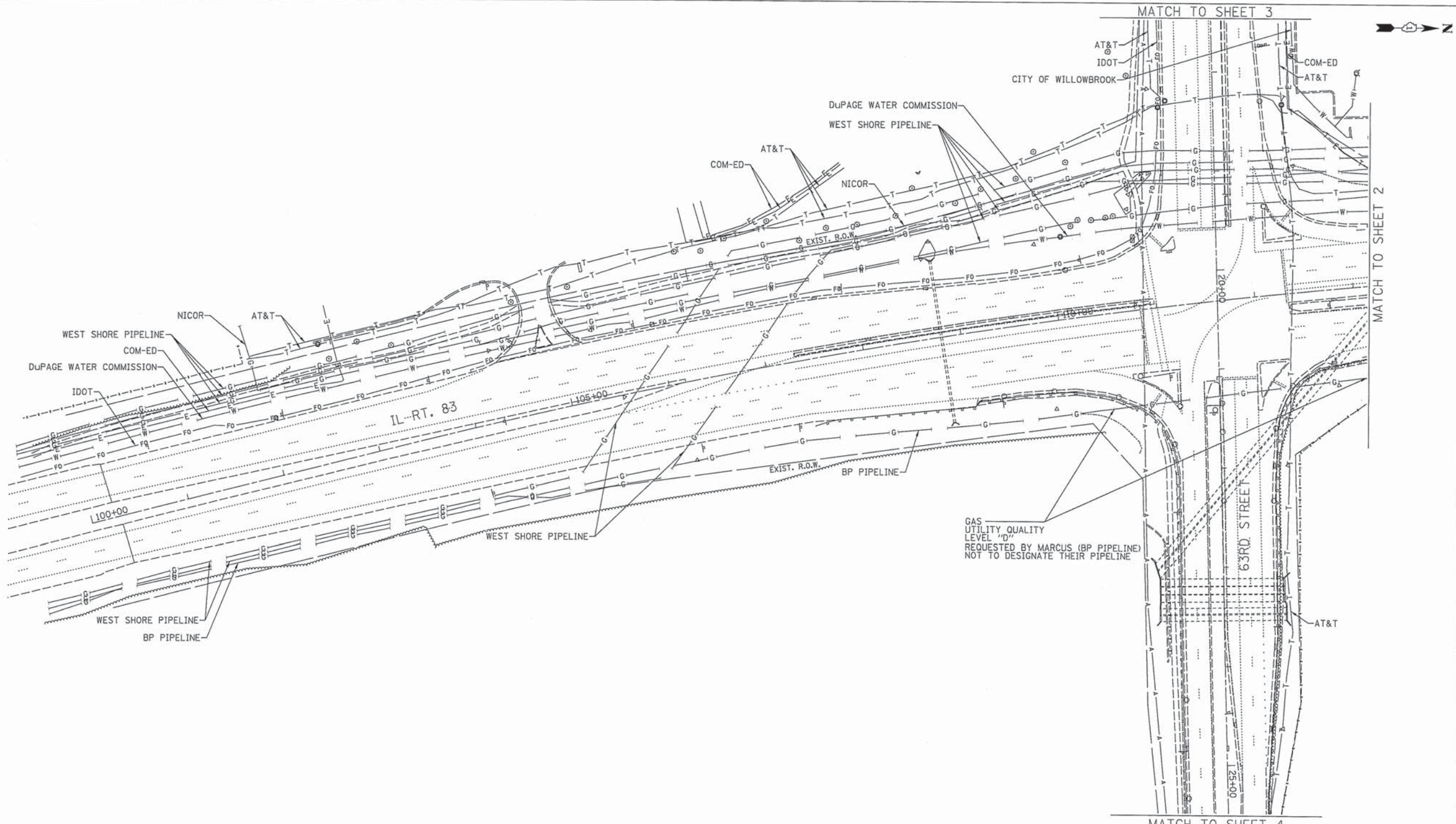
Utility Quality Level "A": Visually Verified Test Hole  
Utility Quality Level "B": Designating/non Visually Verified Test Hole  
Utility Quality Level "C": Research with Survey  
Utility Quality Level "D": Records Research

DESIGNED	IP	REVISED
DRAWN	SRK	REVISED
CHECKED	KFS	REVISED
DATE	5/08/14	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL RT. 83 at 63rd Street  
Willowbrook, Illinois

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	(544 & 544-1) TS & N (13)	DuPage	129	62
Contract No. 60W55				
FED. ROAD DIST. NO. - [ILLINOIS] IDOT Project No.				



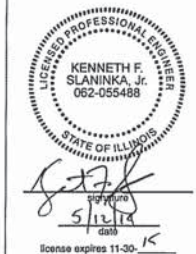
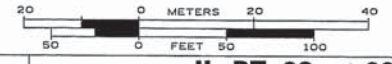
GAS UTILITY QUALITY LEVEL "D" REQUESTED BY MARCUS (BP PIPELINE) NOT TO DESIGNATE THEIR PIPELINE

— A — A —	AERIAL
- - - - -	UNKNOWN
— CTV — CTV —	CABLE TV
— T — T —	TELEPHONE
— G — G —	GAS
— E — E —	ELECTRIC
— W — W —	WATER
— FO — FO —	FIBER OPTIC
⊙	TBE TEST HOLE

UTILITY OWNERS	
AT&T = FIBER OPTIC	
AT&T = TELEPHONE	
BP PIPELINE = GAS	
BUCKEYE PARTNERS = GAS	
CITY OF WILLOWBROOK = WATER	
COMCAST = CABLE TV	
COM-ED = ELECTRIC	
DUPAGE WATER COMMISSION = WATER	
IDOT = FIBER OPTIC	
NICOR = GAS	
WEST SHORE PIPELINE = GAS	

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ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



TBE Job No. IL09510562  
SUE Plan Pages: 1 of 4

Utility Quality Level "A": Visually Verified Test Hole  
Utility Quality Level "B": Designating/non Visually Verified Test Hole  
Utility Quality Level "C": Research with Survey  
Utility Quality Level "D": Records Research

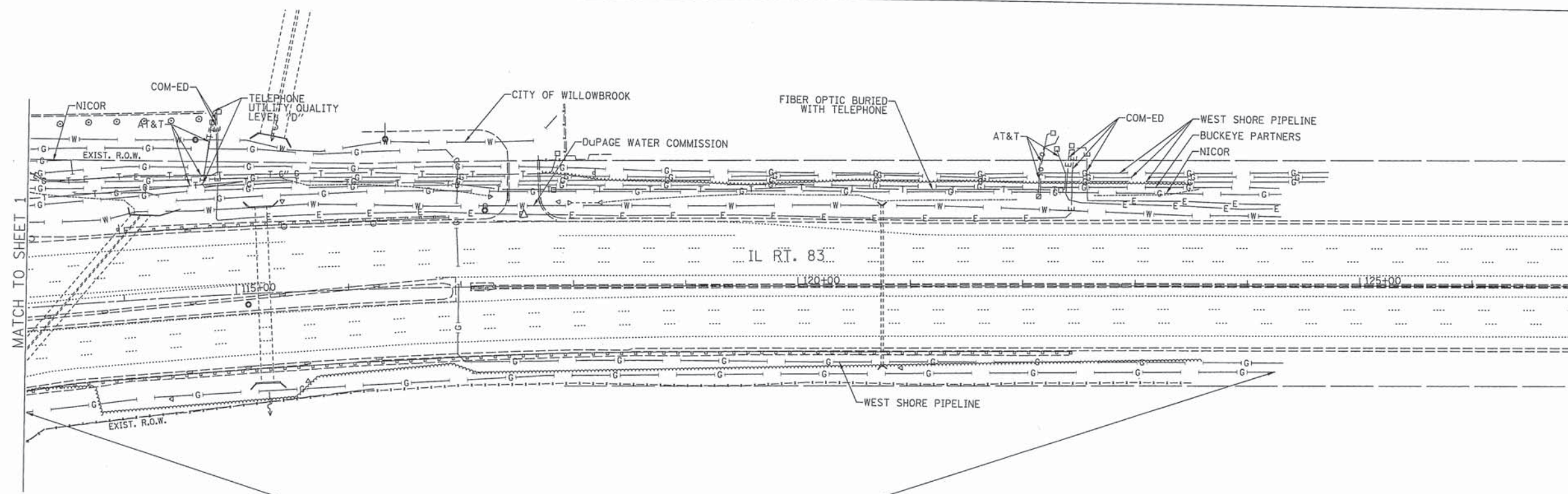
DESIGNED	IP	REVISED
DRAWN	SRK	REVISED
CHECKED	KFS	REVISED
DATE	5/08/14	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL RT. 83 at 63rd Street  
Willowbrook, Illinois

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	(544 & 544-1) TS & N (13)	DuPage	129	63
Contract No. 60W55			FED. ROAD DIST. NO. - [ILLINOIS] IDOT Project No.	





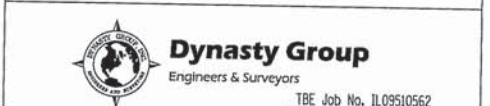
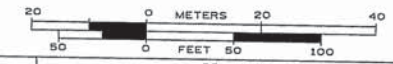
GAS  
UTILITY QUALITY  
LEVEL "D"  
REQUESTED BY MARCUS (BP PIPELINE)  
NOT TO DESIGNATE THEIR PIPELINE

— A — A —	AERIAL
- - - - -	UNKNOWN
— CTV — CTV —	CABLE TV
— T — T —	TELEPHONE
— G — G —	GAS
— E — E —	ELECTRIC
— W — W —	WATER
— FO — FO —	FIBER OPTIC
⊙	TBE TEST HOLE

UTILITY OWNERS	
AT&T	= FIBER OPTIC
AT&T	= TELEPHONE
BP PIPELINE	= GAS
BUCKEYE PARTNERS	= GAS
CITY OF WILLOWBROOK	= WATER
COMCAST	= CABLE TV
COM-ED	= ELECTRIC
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Utility Quality Level "B" : Designating/non Visually Verified Test Hole	DRAWN SRK	REVISED
Utility Quality Level "C" : Research with Survey	CHECKED KFS	REVISED
Utility Quality Level "D" : Records Research	DATE 5/08/14	REVISED

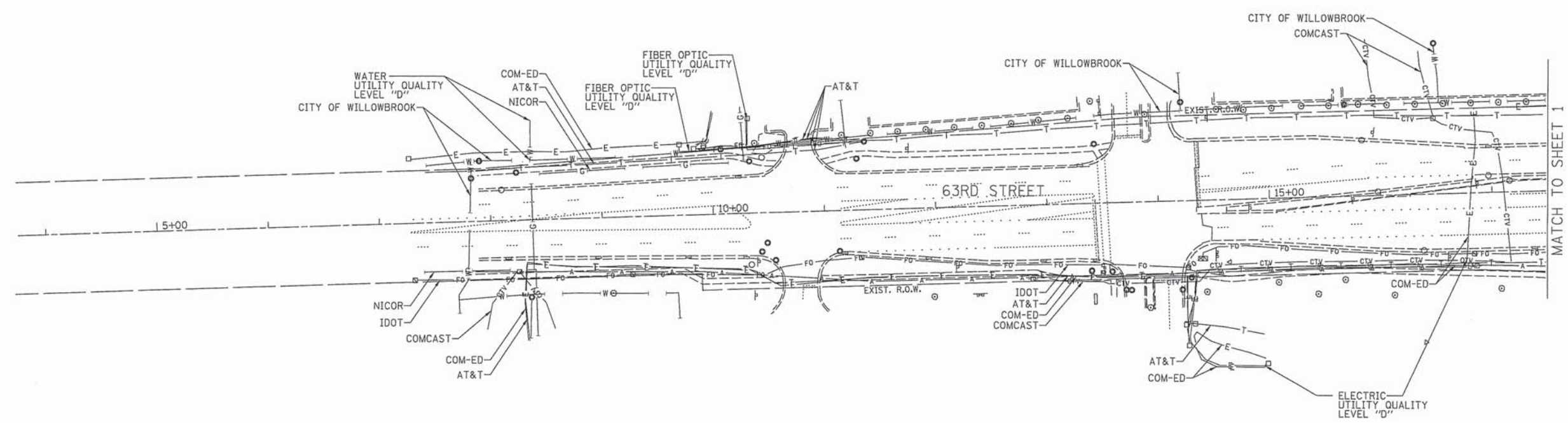
DESIGNED IP	REVISED
DRAWN SRK	REVISED
CHECKED KFS	REVISED
DATE 5/08/14	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL RT. 83 at 63rd Street  
Willowbrook, Illinois

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	(544 & 544-1) TS & N (13)	DuPage	129	64
Contract No. 60W55				
FED. ROAD DIST. NO. - [ILLINOIS] IDOT Project No.				

TBE Job No. IL09510562  
SUE Plan Page: 2 of 4

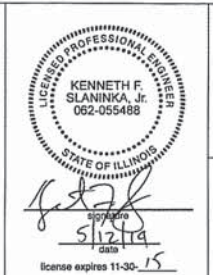


— A — A —	AERIAL
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— CTV — CTV —	CABLE TV
— T — T —	TELEPHONE
— (G) — (G) —	GAS
— E — E —	ELECTRIC
— W — W —	WATER
— FO — FO —	FIBER OPTIC
⊕	TBE TEST HOLE

UTILITY OWNERS	
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**Dynasty Group**  
Engineers & Surveyors

TBE Job No. IL09510562  
SUE Plan Page: 3 of 4

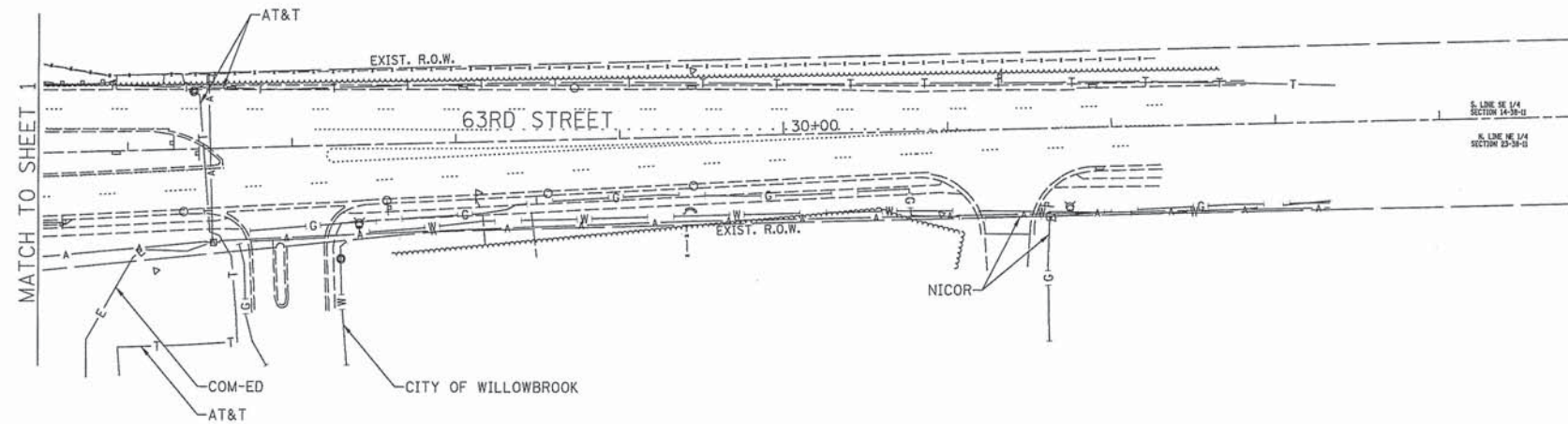
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DESIGNED	IP	REVISED
DRAWN	SRK	REVISED
CHECKED	KFS	REVISED
DATE	5/08/14	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL RT. 83 at 63rd Street  
Willowbrook, Illinois

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	(544 & 544-1) TS & N (13)	DuPage	129	65
Contract No. 60W55				
FED. ROAD DIST. NO. - [ILLINOIS] IDOT Project No.				

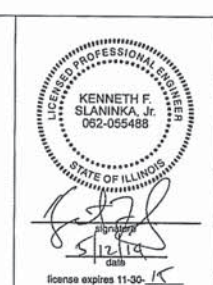


— A — A	AERIAL
- - - - -	UNKNOWN
— CTV — CTV	CABLE TV
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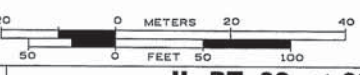
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DESIGNED JP	REVISED
DRAWN SRK	REVISED
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

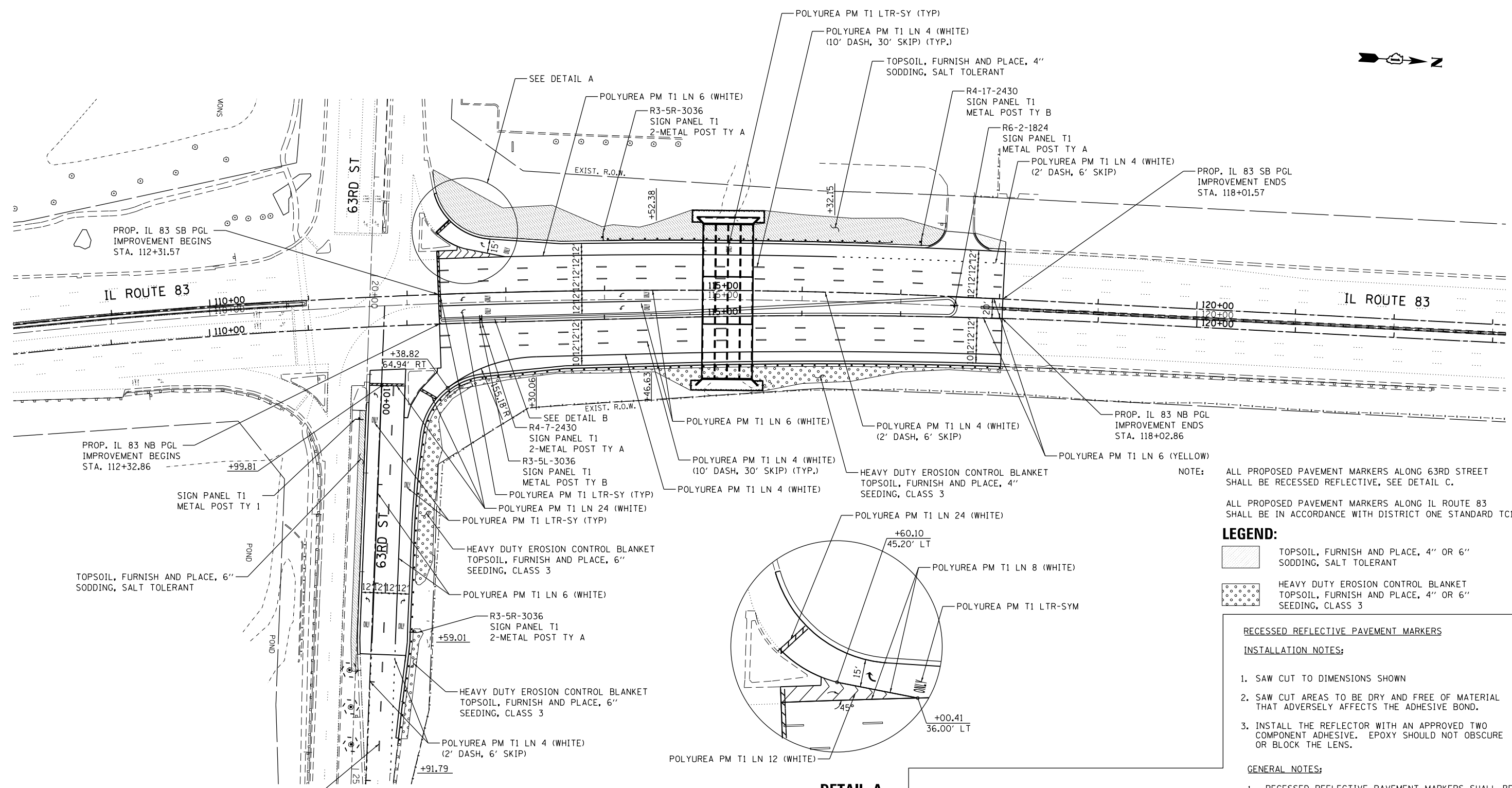
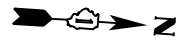


IL RT. 83 at 63rd Street  
 Willowbrook, Illinois

F.A. RTE:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	(544 & 544-1) TS & N (13)	DuPage	129	66
FED. ROAD DIST. NO. - ILLINOIS			IDOT Project No.	

TBE Job No. IL09510562  
 SUE Plan Page: 4 of 4

Contract No. 60W55



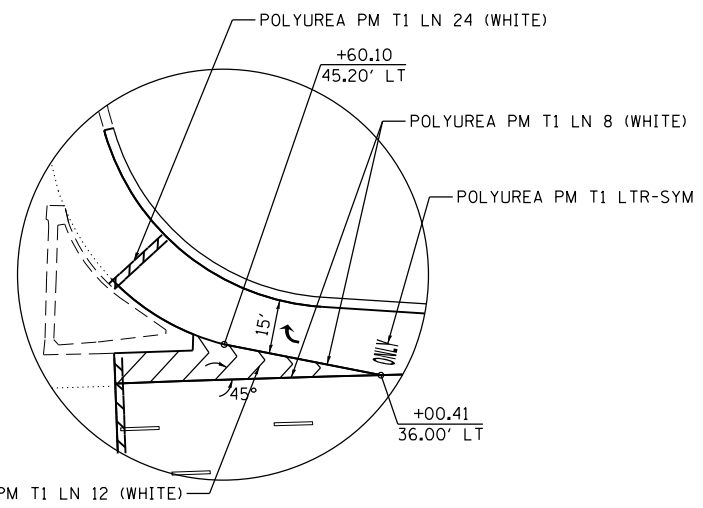
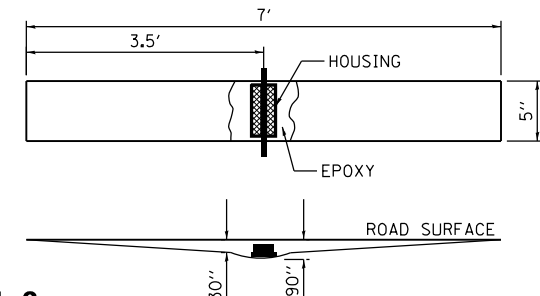
NOTE: ALL PROPOSED PAVEMENT MARKERS ALONG 63RD STREET SHALL BE RECESSED REFLECTIVE, SEE DETAIL C.

ALL PROPOSED PAVEMENT MARKERS ALONG IL ROUTE 83 SHALL BE IN ACCORDANCE WITH DISTRICT ONE STANDARD TC11

- LEGEND:**
- TOPSOIL, FURNISH AND PLACE, 4" OR 6" SODDING, SALT TOLERANT
  - HEAVY DUTY EROSION CONTROL BLANKET TOPSOIL, FURNISH AND PLACE, 4" OR 6" SEEDING, CLASS 3

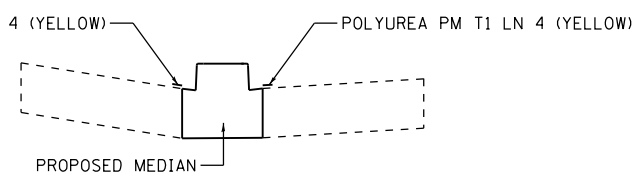
- RECESSED REFLECTIVE PAVEMENT MARKERS**
- INSTALLATION NOTES:**
- SAW CUT TO DIMENSIONS SHOWN
  - SAW CUT AREAS TO BE DRY AND FREE OF MATERIAL THAT ADVERSELY AFFECTS THE ADHESIVE BOND.
  - INSTALL THE REFLECTOR WITH AN APPROVED TWO COMPONENT ADHESIVE. EPOXY SHOULD NOT OBSCURE OR BLOCK THE LENS.

- GENERAL NOTES:**
- RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED AS SHOWN ON THE PLAN AND IN ACCORDANCE TO THE SPACINGS IDENTIFIED HEREIN



LOCATION:	SPACING:	TYPE:
DOUBLE YELLOW CENTERLINE	2 @ 80'	2-WAY YELLOW
MEDIANS > 4'	1 @ 80'	1-WAY YELLOW
END OF MEDIANS	3 @ 3' LATERAL	1-WAY YELLOW
SKIP-DASH WHITE LANE LINES	1 @ 80'	1-WAY WHITE
LONG BARRIER MEDIANS	VARIES	WHITE / RED
SOLID LANE LINES	1 @ 40'	1-WAY WHITE

• = IN TURN BAYS, TAPERS, AND 100 FEET FROM TAPERS, USE 2 @ 40'



**DETAIL B**  
STA. 112+32.86 TO STA. 117+57.00 NB PGL

**DETAIL A**

**DETAIL C**

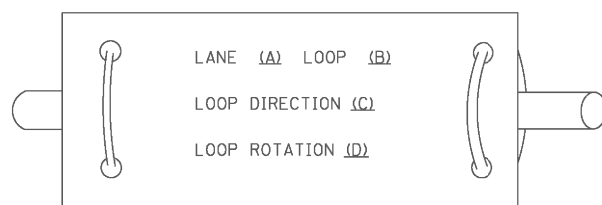
# TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED												
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE															
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE															
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA															
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED															
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F															
UNINTERRUPTABLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F															
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				UNDERGROUND CONDUIT, GALVANIZED STEEL (UC)				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F															
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F															
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE															
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED															
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM				STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED															
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM				ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED															
SIGNAL POST				REMOVE ITEM				STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED															
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM				SIGNAL POST AND FOUNDATION TO BE REMOVED															
GUY WIRE				ABANDON ITEM				INTERSECTION & SAMPLING (SYSTEM) DETECTOR															
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				SAMPLING (SYSTEM) DETECTOR															
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				QUEUE DETECTOR															
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				PREFORMED QUEUE DETECTOR															
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR															
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				"RB" INDICATES REFLECTIVE BACKPLATE				PREFORMED SAMPLING (SYSTEM) DETECTOR															
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				<h2 style="margin: 0;">RAILROAD SYMBOLS</h2> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 50%;">EXISTING</th> <th style="width: 50%;">PROPOSED</th> </tr> </thead> <tbody> <tr> <td></td> <td></td> </tr> <tr> <td></td> <td></td> </tr> <tr> <td></td> <td></td> </tr> <tr> <td></td> <td></td> </tr> <tr> <td></td> <td></td> </tr> </tbody> </table>				EXISTING	PROPOSED										
EXISTING	PROPOSED																						
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED																			
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID																			
ILLUMINATED SIGN "NO LEFT TURN"				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER																			
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO INTERCONNECT																			
DETECTOR LOOP, TYPE I				RADIO REPEATER																			
PREFORMED DETECTOR LOOP				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED																			
MICROWAVE VEHICLE SENSOR				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)																			
VIDEO DETECTION CAMERA																							
VIDEO DETECTION ZONE																							
PAN, TILT, ZOOM CAMERA																							
WIRELESS DETECTOR SENSOR																							
WIRELESS ACCESS POINT																							

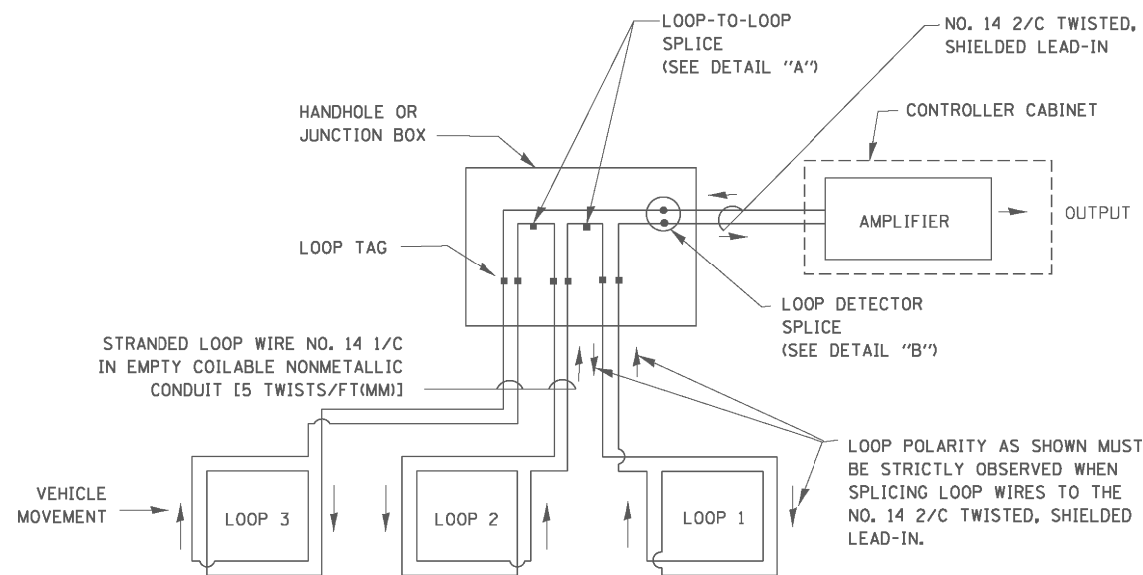
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

**LOOP LEAD-IN CABLE TAG**

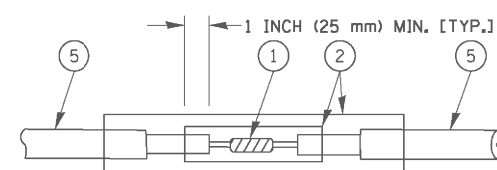


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

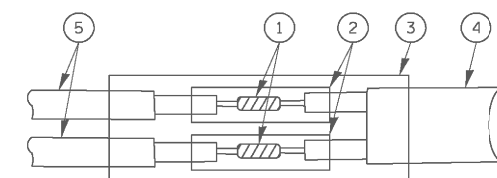


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

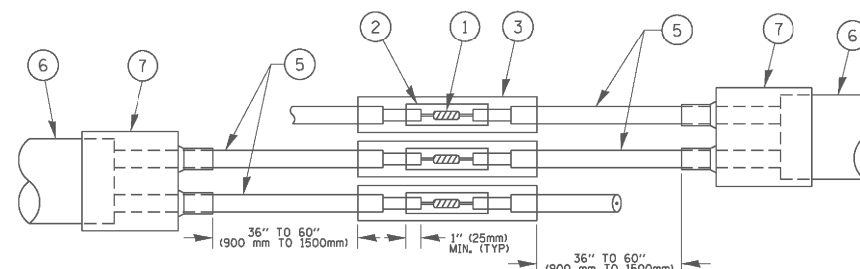


DETAIL "A"  
LOOP-TO-LOOP SPLICE

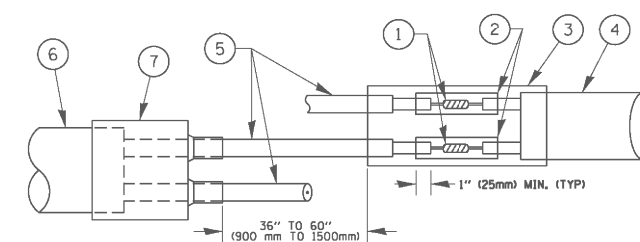


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**TYPE I LOOP**



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**PREFORMED LOOP**

**LOOP DETECTOR SPLICE**

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PREFORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = footemj	DESIGNED - DAD	REVISED - DAG 1-1-14
et\pwork\pindot\footemj\d0108315\ts05.dgn		DRAWN - BCK	REVISED -
	PLOT SCALE = 50.0000' / in.	CHECKED - DAD	REVISED -
	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

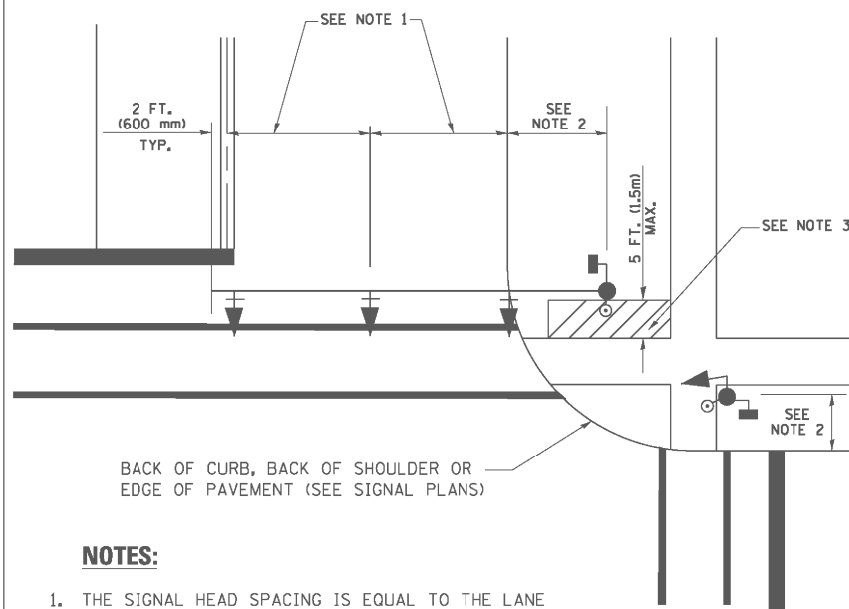
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 2 OF 7 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	P-91-584-10	DUPAGE	129	69
TS-05		CONTRACT NO. 60W55		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

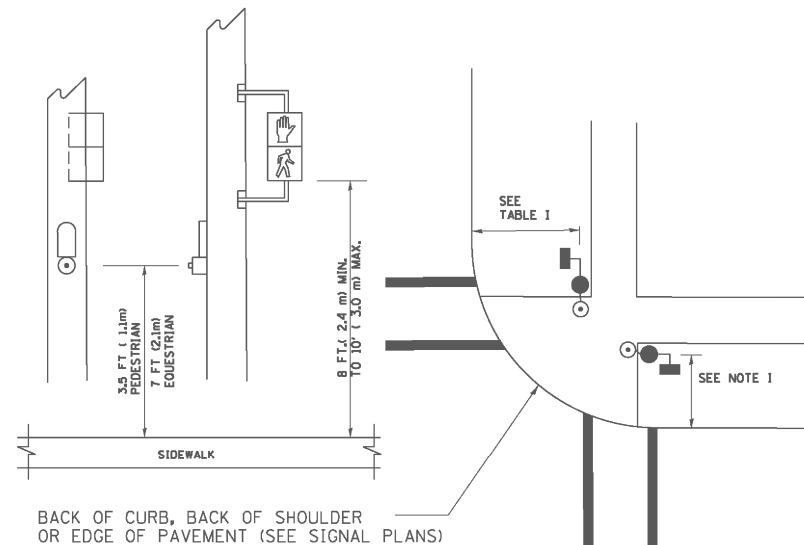
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST  
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR  
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN  
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.**



**NOTES:**

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

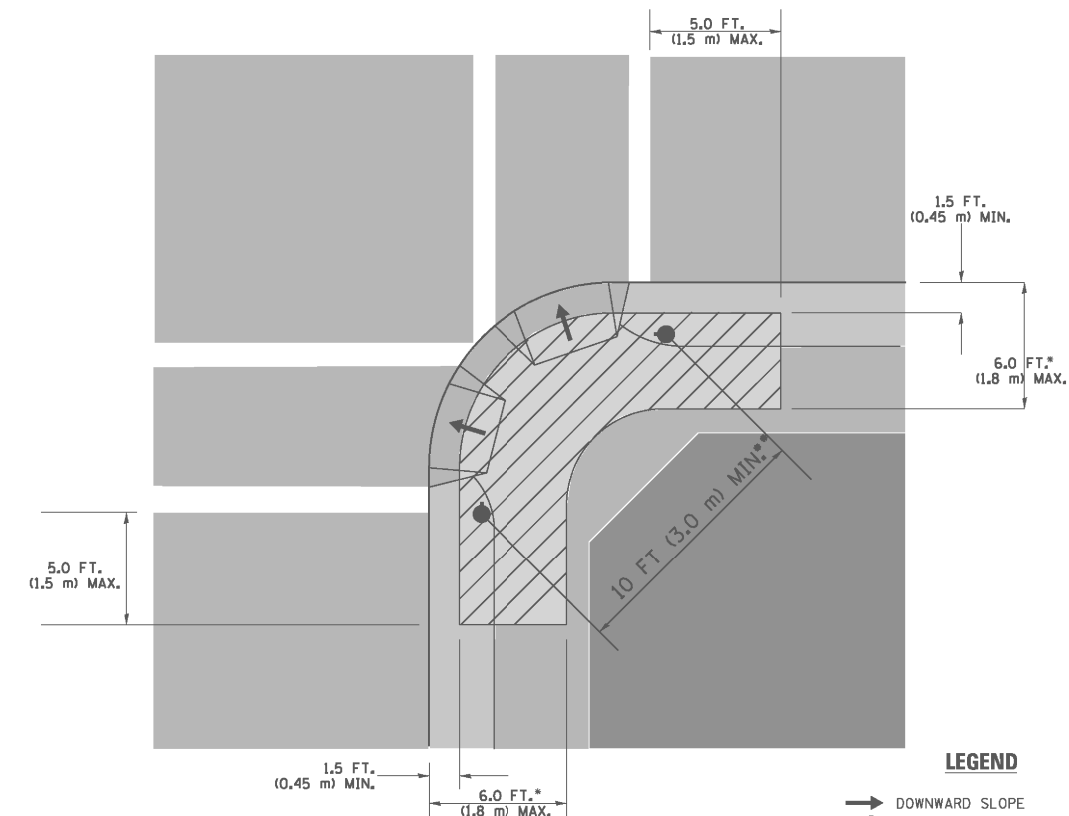
**PEDESTRIAN SIGNAL POST  
AND  
PEDESTRIAN PUSH BUTTON POST**



**NOTES:**

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

**RECOMMENDED PUSHBUTTON LOCATIONS**



**LEGEND**

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPARATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

**NOTES:**

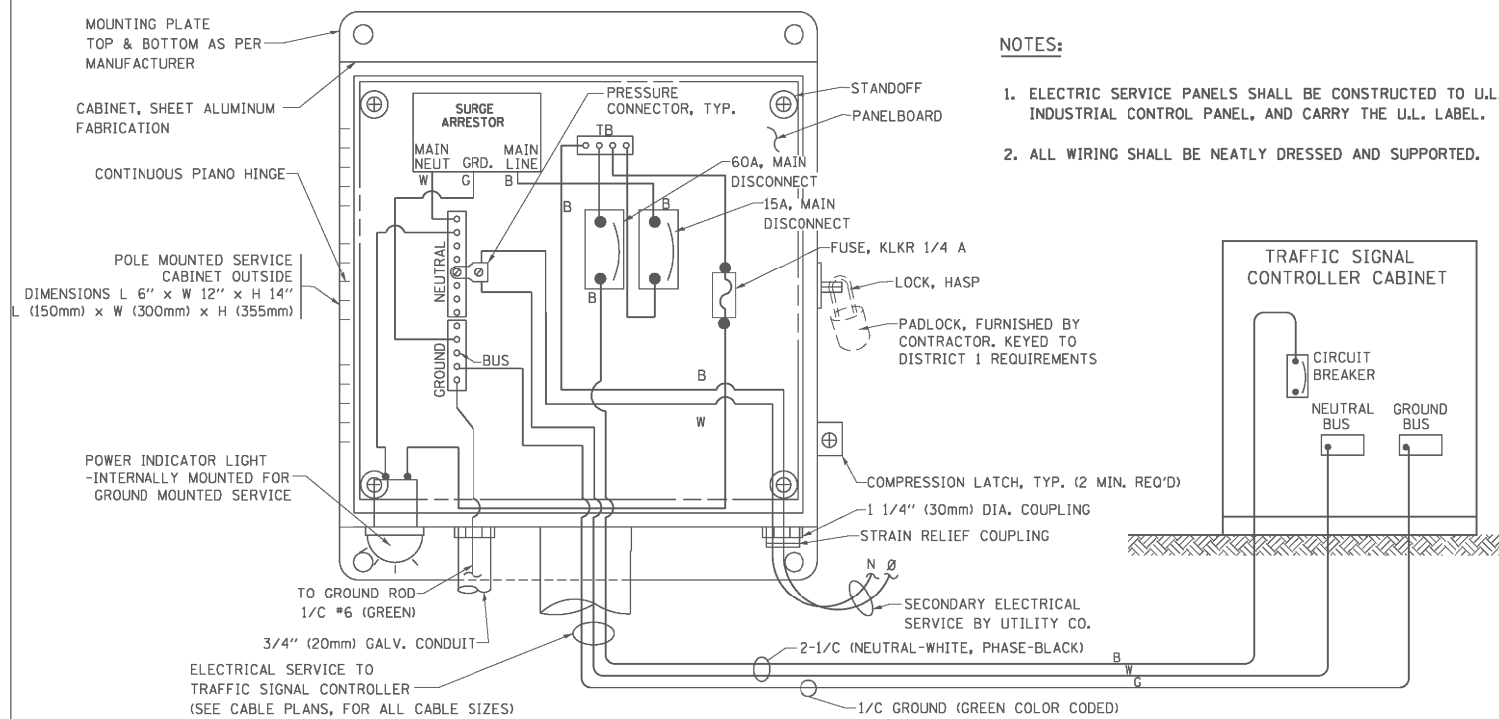
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

**TRAFFIC SIGNAL EQUIPMENT OFFSET**

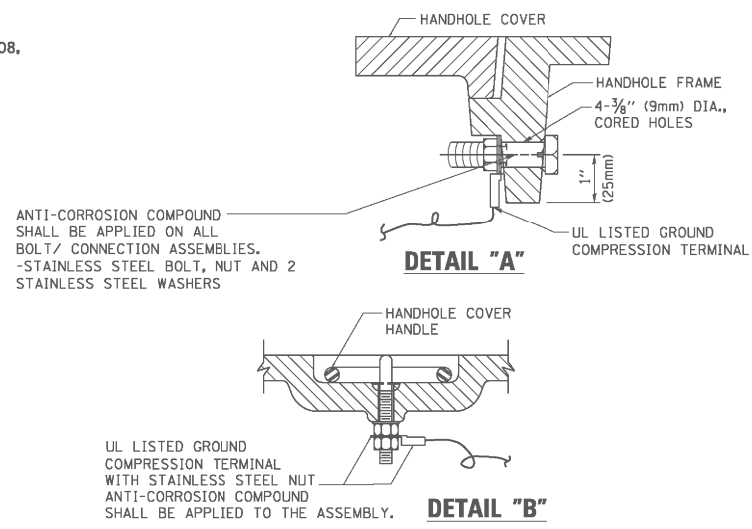
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

**NOTES:**

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

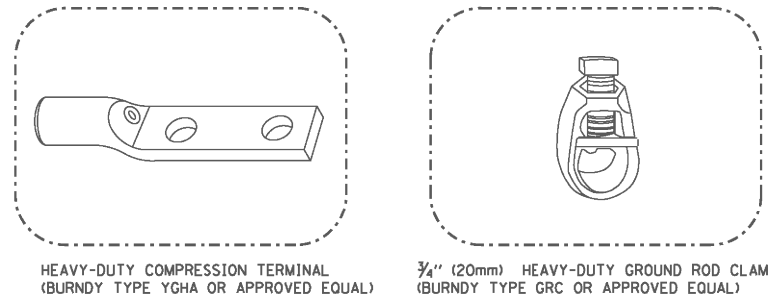
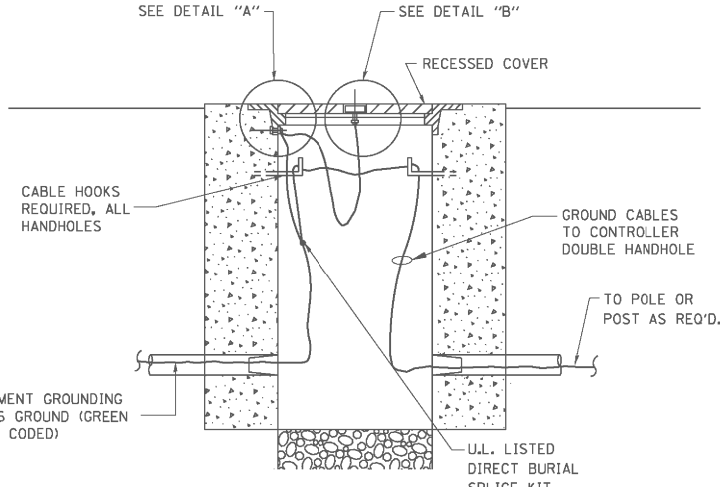


**ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)  
SERVICE INSTALLATION POLE MOUNT (SHOWN)  
(NOT TO SCALE)**



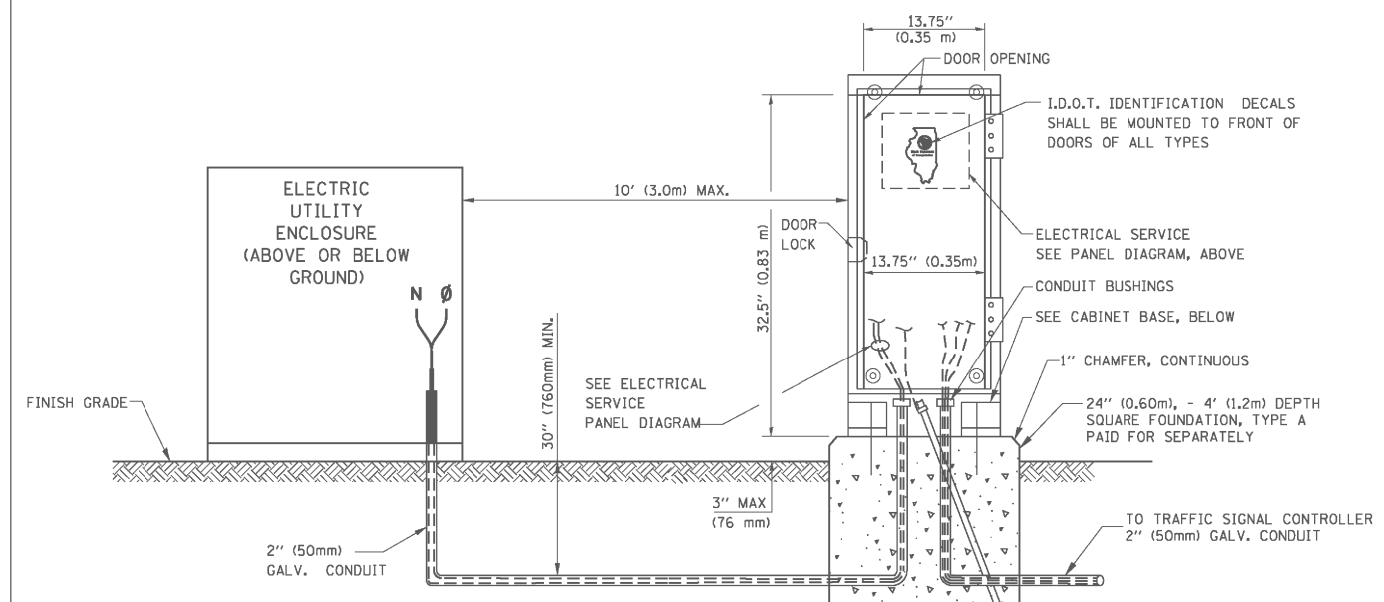
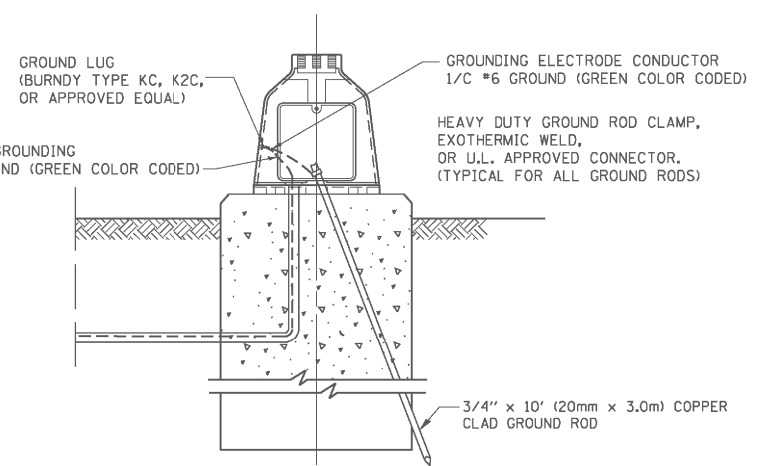
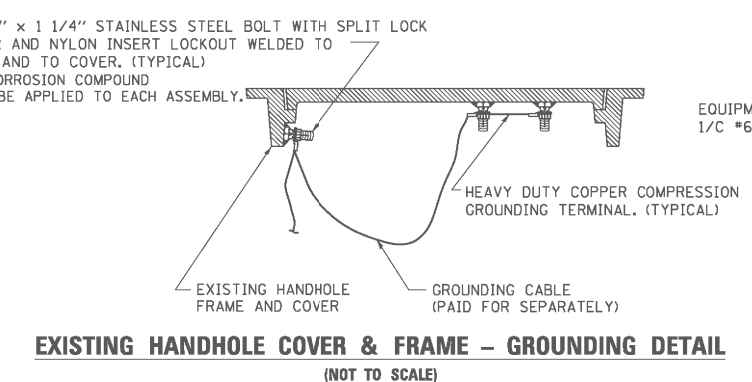
**NOTES:  
GROUNDING SYSTEM**

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



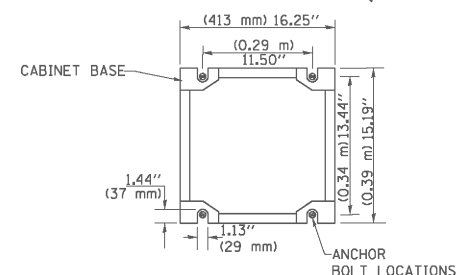
**NOTES:**

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



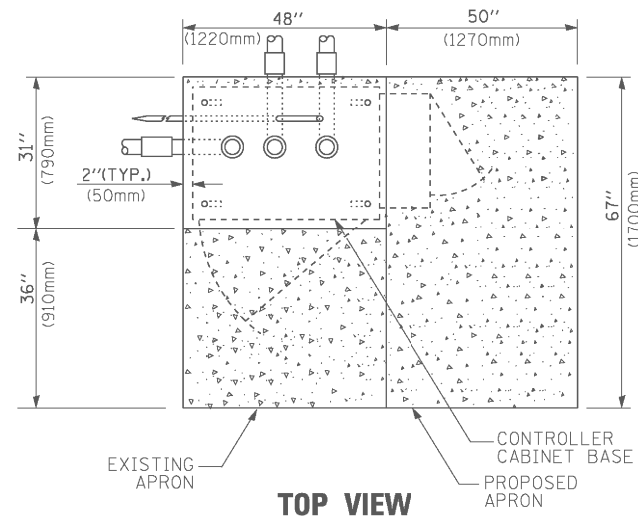
**SERVICE INSTALLATION GROUND MOUNT  
(NOT TO SCALE)**

**CABINET - BASE BOLT PATTERN  
(NOT TO SCALE)**

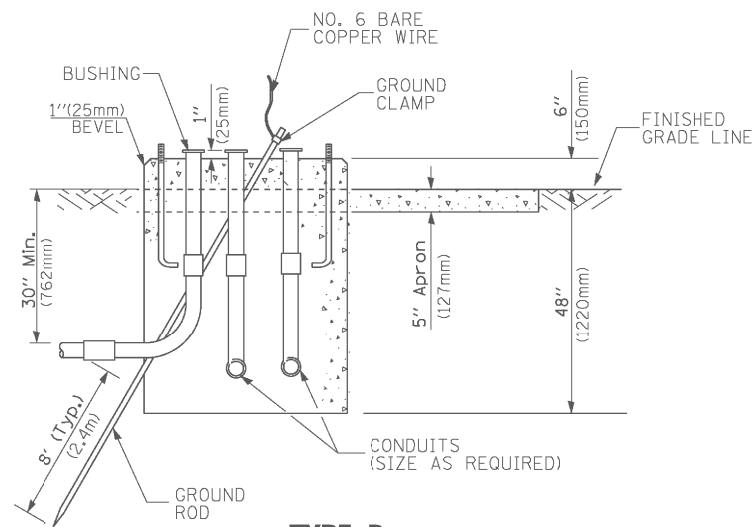


FILE NAME =	USER NAME = footemj	DESIGNED - DAD	REVISED - DAG 1-1-14	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>			F.A. RTE. = 344	SECTION = (544 & 544-1) TS&N (13)	COUNTY = DUPAGE	TOTAL SHEETS = 129	SHEET NO. = 71
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PLOT DATE = 1/13/2014	DATE = 10-28-09	REVISED -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

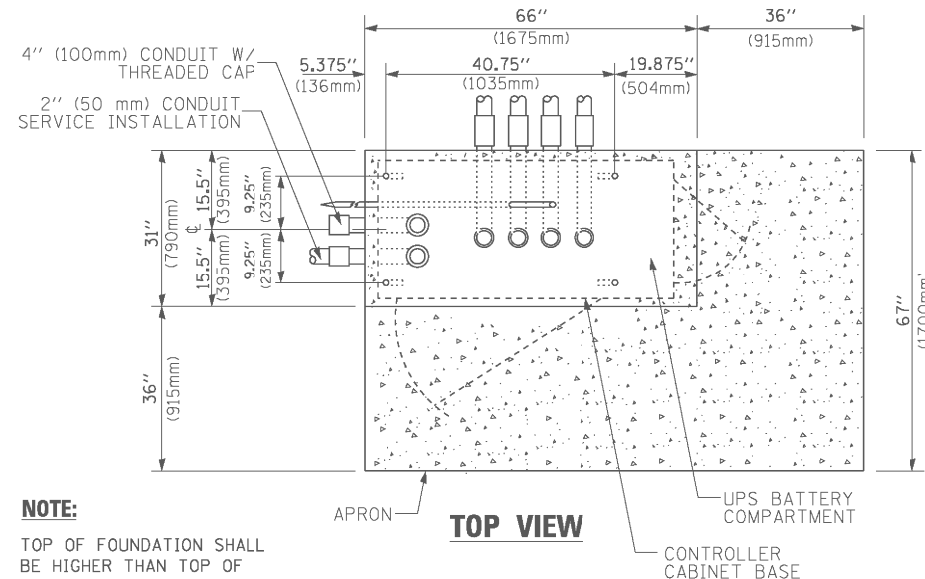




**TOP VIEW**



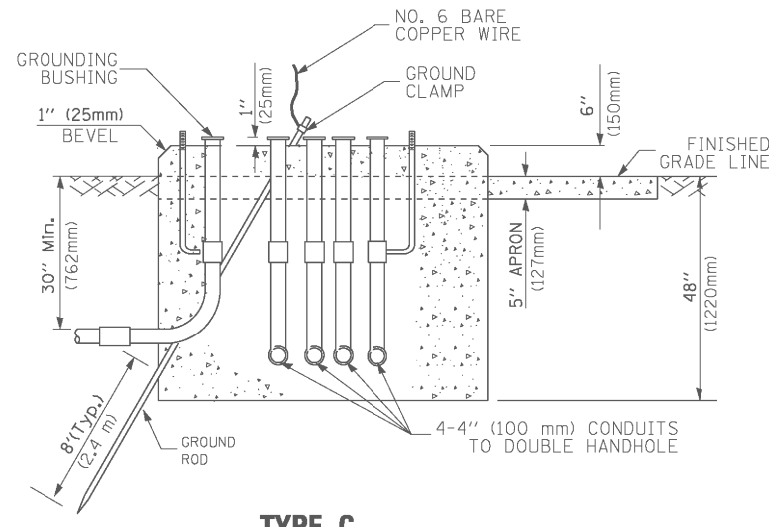
**TYPE D  
FOR GROUND MOUNTED  
CONTROLLER CABINET  
AND UPS BATTERY CABINET**



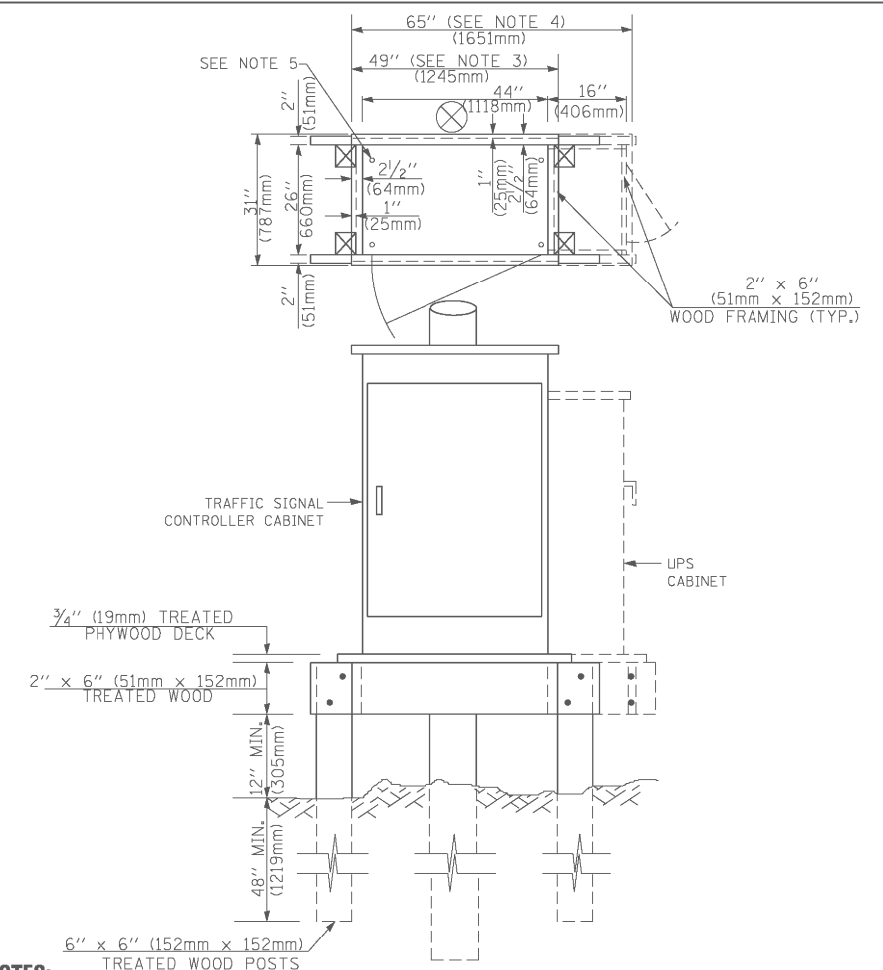
**TOP VIEW**

**NOTE:**

TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE C  
FOR GROUND MOUNTED  
SUPER P (TYPE IV) AND SUPER R (TYPE V)  
CONTROLLER CABINETS**



**NOTES:**

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER  
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

**CABLE SLACK**

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE ( MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

**VERTICAL CABLE LENGTH**

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

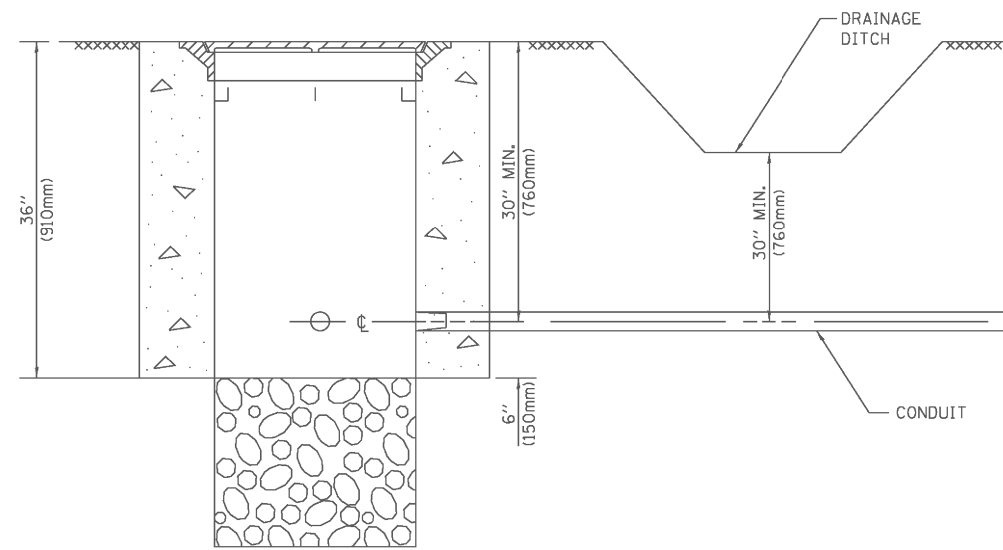
**DEPTH OF FOUNDATION**

MAST ARM LENGTH	Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

**NOTES:**

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average unconfined compressive strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001.

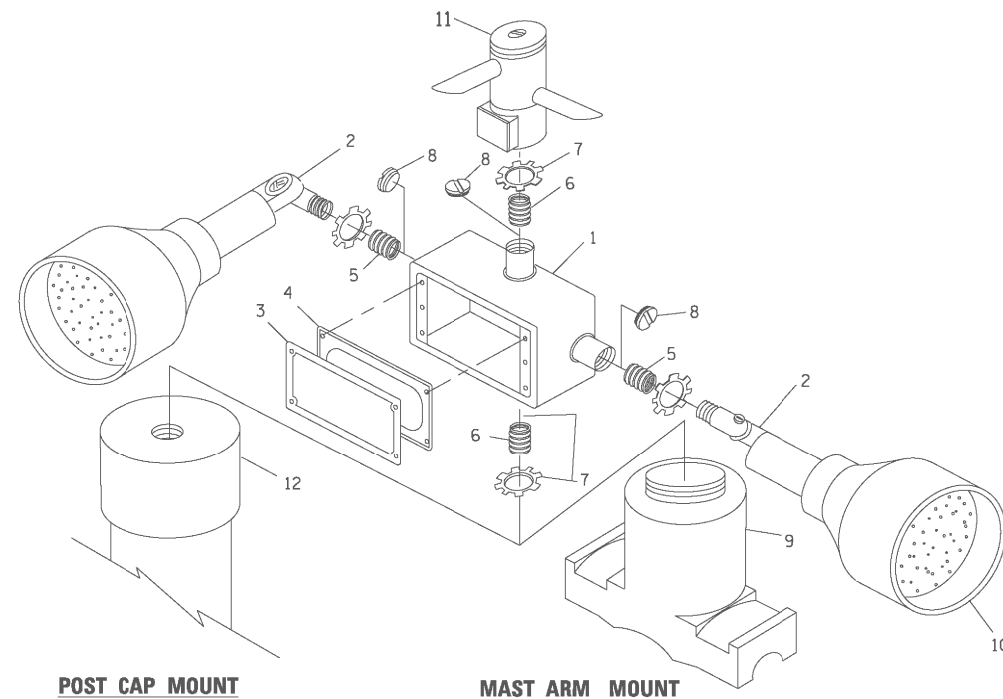
**DEPTH OF MAST ARM FOUNDATIONS, TYPE E**



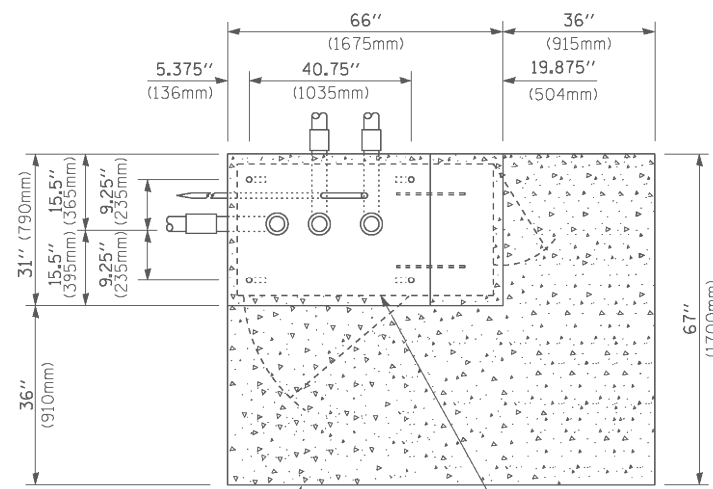
**NOTES:**

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

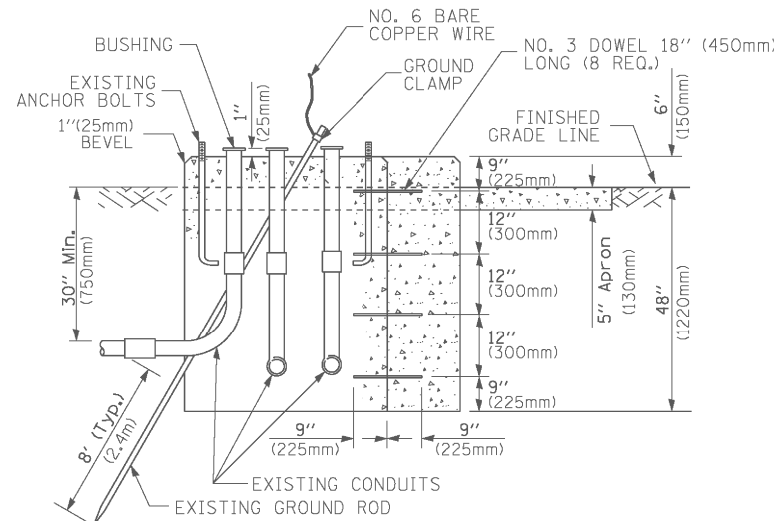
**HANDHOLE WITH MINIMUM CONDUIT DEPTH**  
(NOT TO SCALE)



**EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL**



**TOP VIEW**  
(NOT TO SCALE)

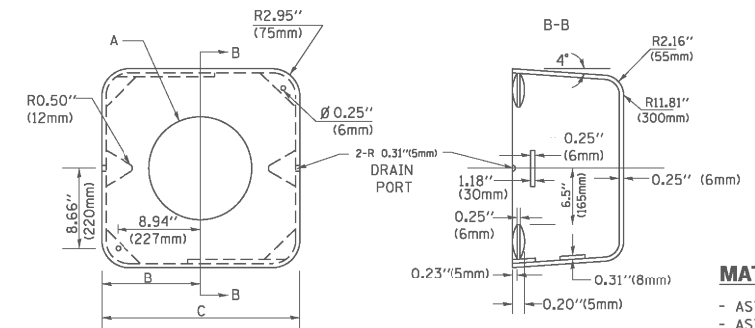


**MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION**  
(NOT TO SCALE)

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

**NOTES:**

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT  
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT  
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



**MATERIAL:**  
- ASTM A36 STEEL  
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIABLES	9.5" (241mm)	19" (483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIABLES	10.75" (273mm)	21.5" (546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIABLES	13.0" (330mm)	26" (660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIABLES	18.5" (470mm)	37" (940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

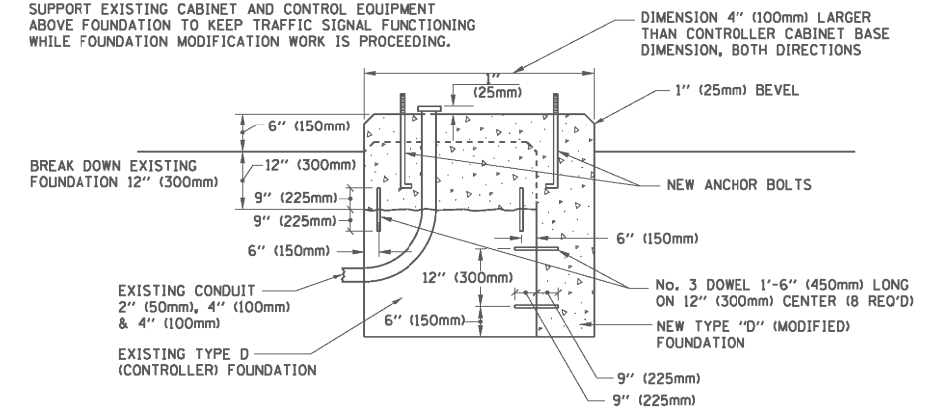
**SHROUD**

**NOTES:**

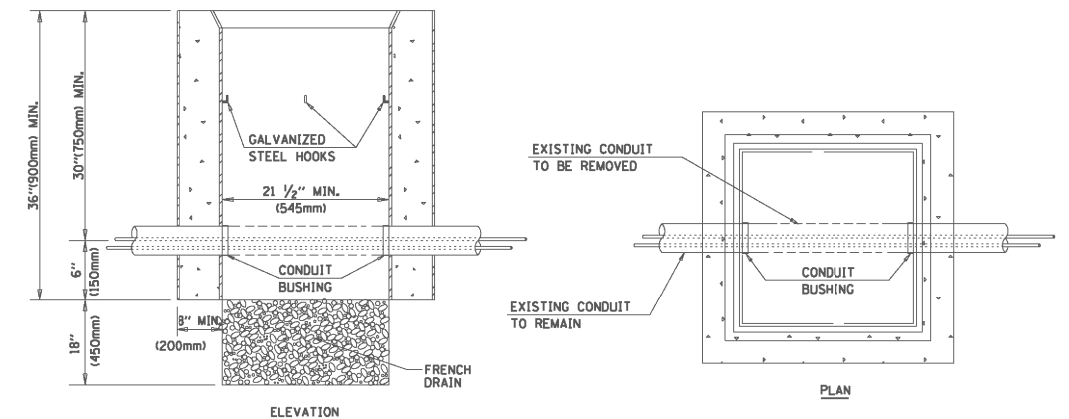
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

**NOTE:**

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



**MODIFY EXISTING TYPE "D" FOUNDATION**



**NOTES:**

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

**HANDHOLE TO INTERCEPT EXISTING CONDUIT**

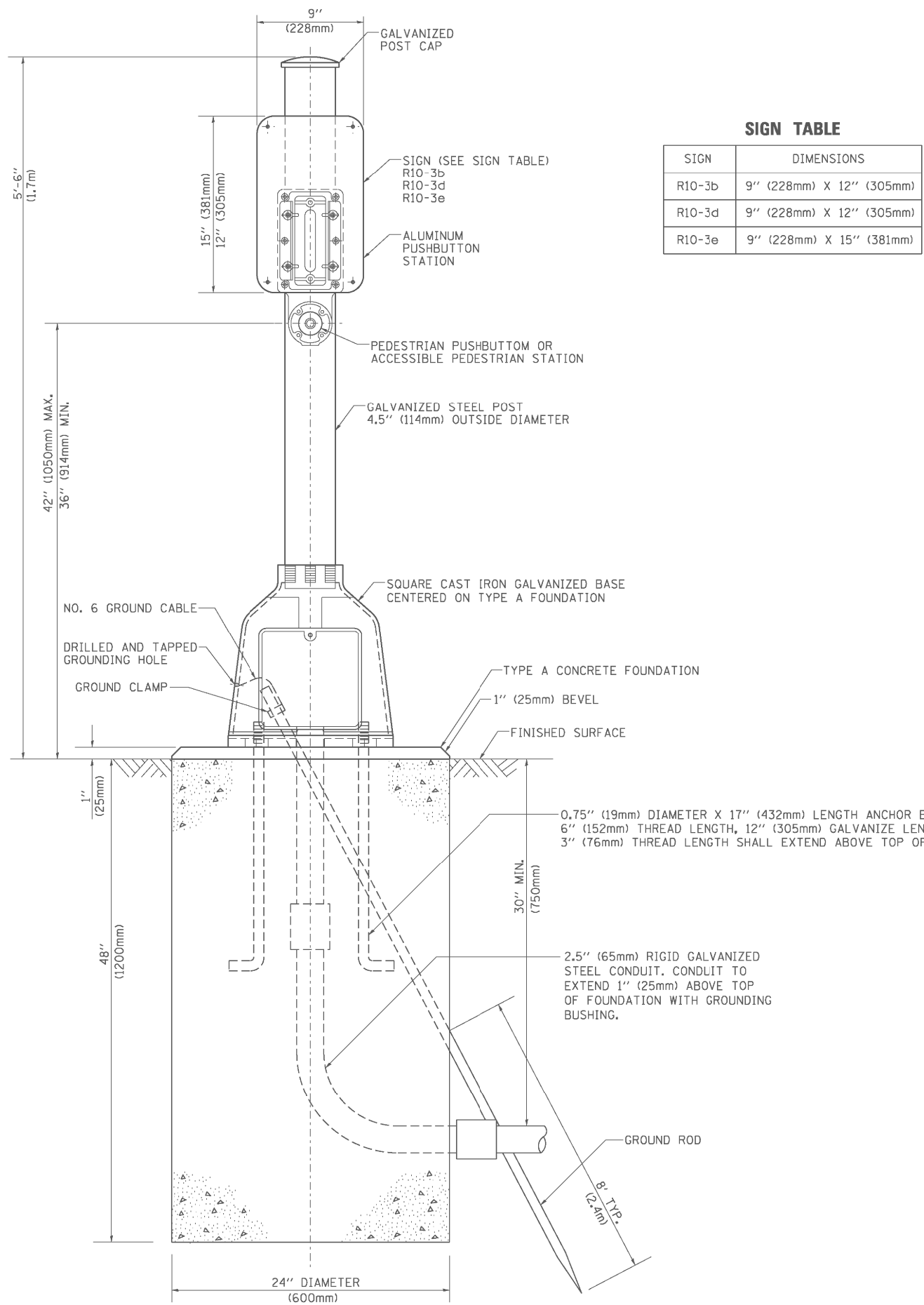
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	PLOT SCALE = 50.0000' / in.	CHECKED - DAD	REVISED -
	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE**  
**STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

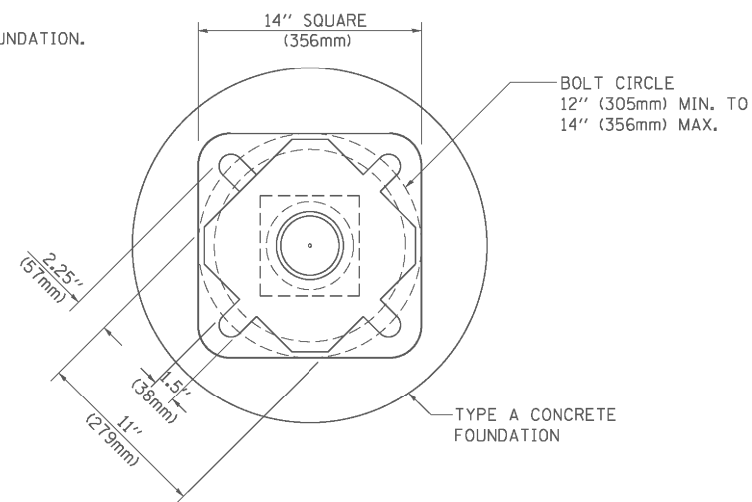
SCALE: NONE SHEET NO. 6 OF 7 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	73
TS-05		CONTRACT NO. 60W55		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**SIGN TABLE**

SIGN	DIMENSIONS
R10-3b	9" (228mm) X 12" (305mm)
R10-3d	9" (228mm) X 12" (305mm)
R10-3e	9" (228mm) X 15" (381mm)



**BOLT PATTERN**

**PEDESTRIAN PUSH BUTTON POST, TYPE A**

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	PLOT SCALE = 50.0000' / 1"	CHECKED - DAD	REVISED -
	PLOT DATE = 1/13/2014	DATE - 10/1/2012	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 7 OF 7 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	74
<b>TS-05</b>			<b>CONTRACT NO. 60W55</b>	
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				

**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

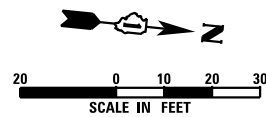
- ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300MM) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
- TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATION OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE RELOCATED TO NEW MAST ARMS AND TRAFFIC SIGNAL CONTROLLER CABINET:

- 4 EACH CONFIRMATION BEACON
- 4 EACH LIGHT DETECTOR
- 2 EACH LIGHT DETECTOR AMPLIFIER

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACTOR'S BID PRICE.

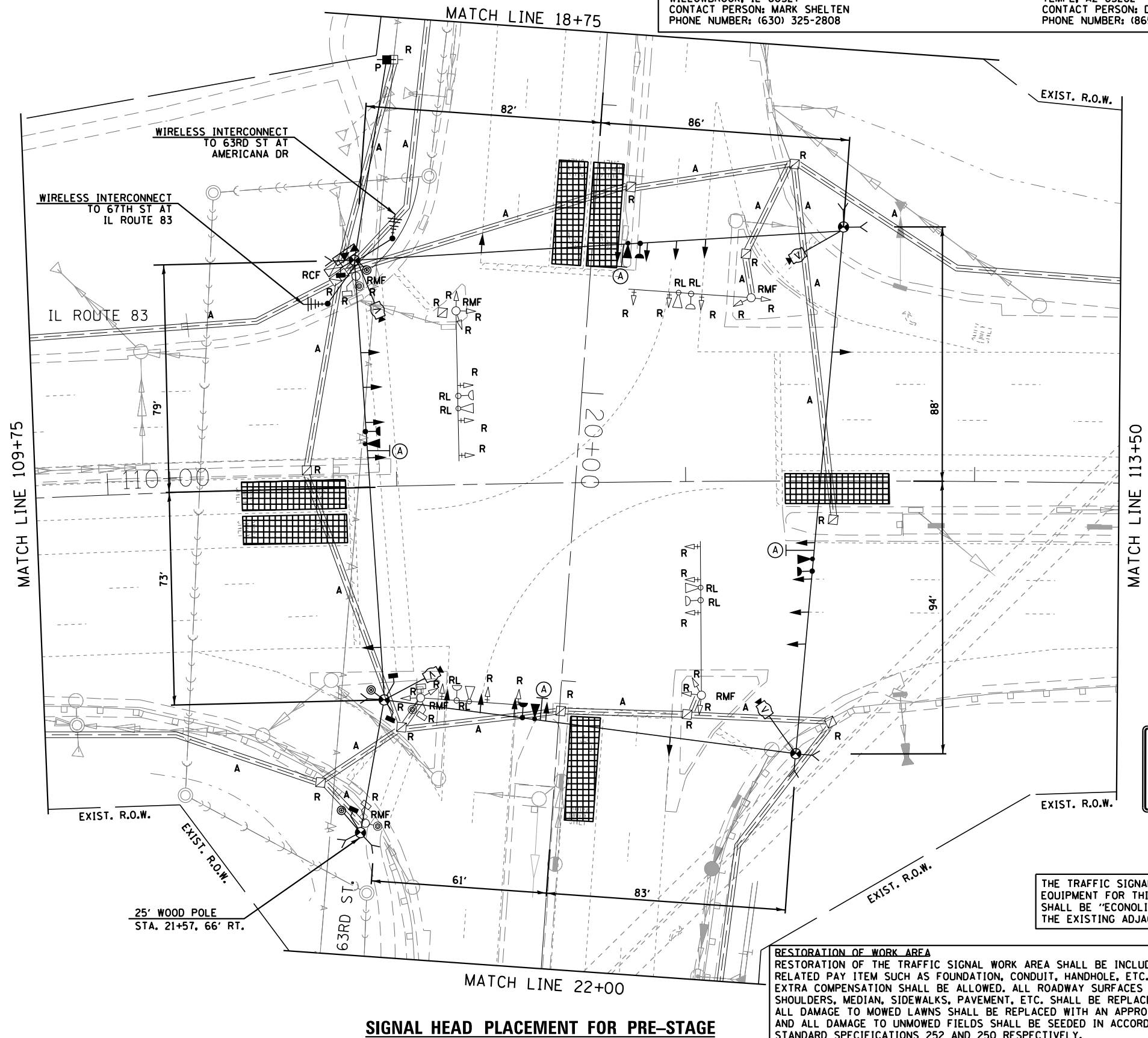
- 1 EACH CONTROLLER AND CABINET COMPLETE
- 10 EACH SIGNAL HEAD, 1-FACE 3-SECTION, MAST ARM MOUNTED
- 2 EACH SIGNAL HEAD, 1-FACE 5-SECTION, MAST ARM MOUNTED
- 2 EACH SIGNAL HEAD, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED
- 2 EACH SIGNAL HEAD, 3-FACE, 1-3 SECTION, 2-5 SECTION
- 2 EACH PEDESTRIAN SIGNAL HEAD, 1-FACE, BRACKET MOUNTED
- 1 EACH PEDESTRIAN SIGNAL HEAD, 2-FACE, BRACKET MOUNTED
- 3 EACH PEDESTRIAN PUSH-BUTTON
- 12 EACH TRAFFIC SIGNAL BACKPLATE
- 2 EACH TRAFFIC SIGNAL POST
- 4 EACH STEEL MAST ARM ASSEMBLY AND POLE
- 1 EACH SERVICE INSTALLATION



DUE TO THE PRESENCE OF RED LIGHT RUNNING (RLR) CAMERA, CONTRACTOR SHALL NOTIFY THE VILLAGE AND RLR CAMERA COMPANY PRIOR TO THE START OF CONSTRUCTION. THE VILLAGE OR THE RLR CAMERA COMPANY SHALL MAKE THE CAMERA INOPERATIVE FOR THE TIME OF CONSTRUCTION. ANY RLR CAMERA EQUIPMENT THAT IS IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED BY ITS RESPECTIVE OWNERS PRIOR TO THE START OF CONSTRUCTION.

VILLAGE OF WILLOWBROOK  
7760 QUINCY ST.  
WILLOWBROOK, IL 60527  
CONTACT PERSON: MARK SHELTON  
PHONE NUMBER: (630) 325-2808

AMERICAN TRAFFIC SOLUTIONS  
1330 W. SOUTHERN AVE.  
TEMPE, AZ 85282  
CONTACT PERSON: DAVID MAST  
PHONE NUMBER: (865) 304-4956



R10-5  
30" X 36"  
(4 EACH)

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**RESTORATION OF WORK AREA**  
RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

**SIGNAL HEAD PLACEMENT FOR PRE-STAGE**

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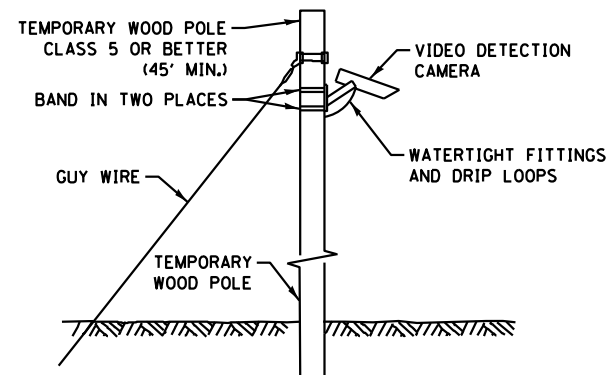
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	DATE - 11/26/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

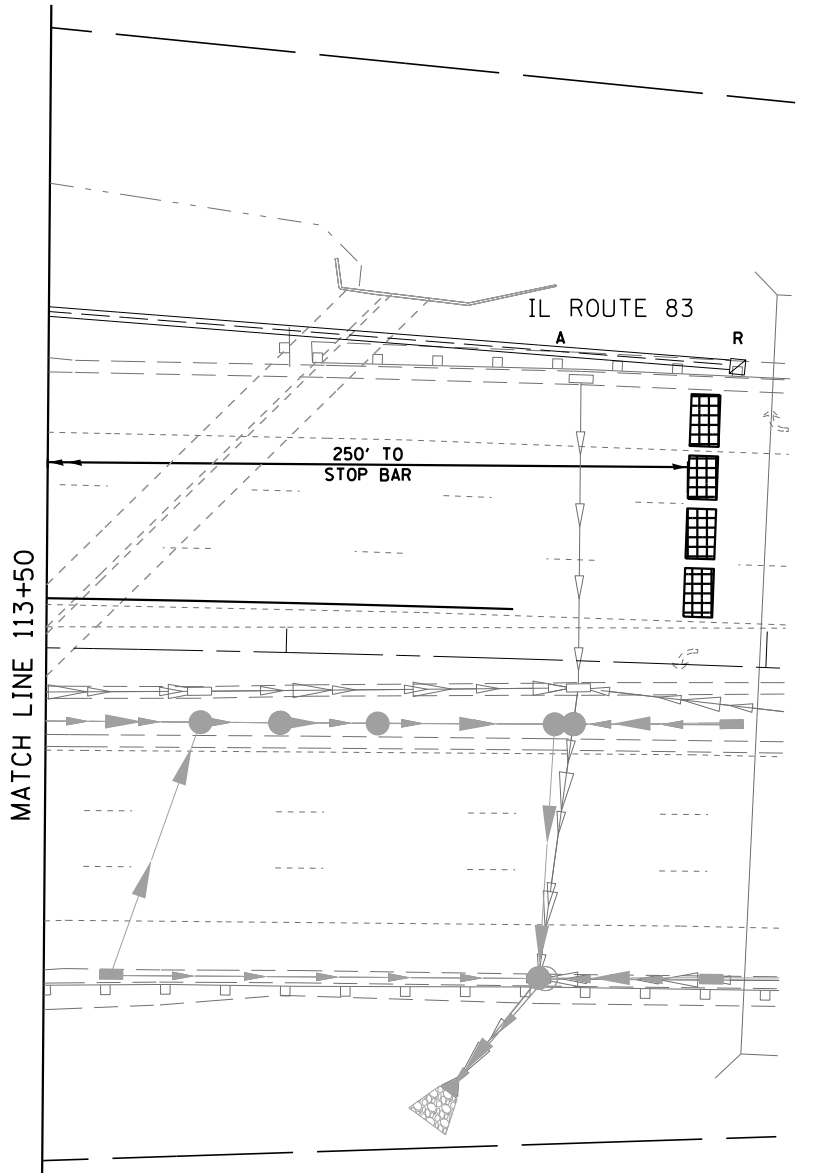
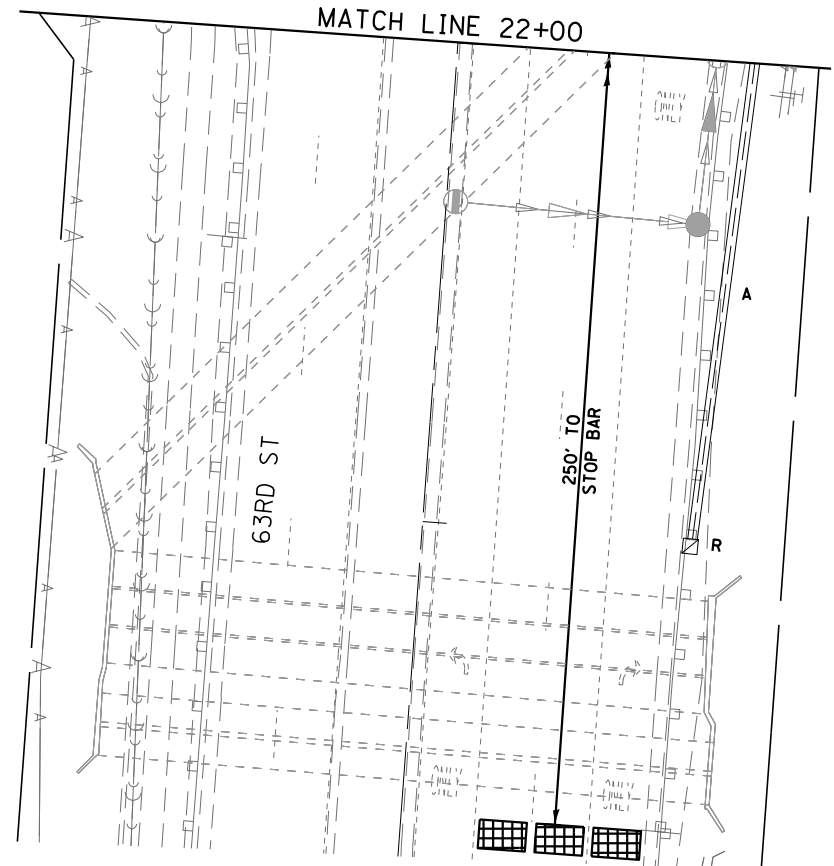
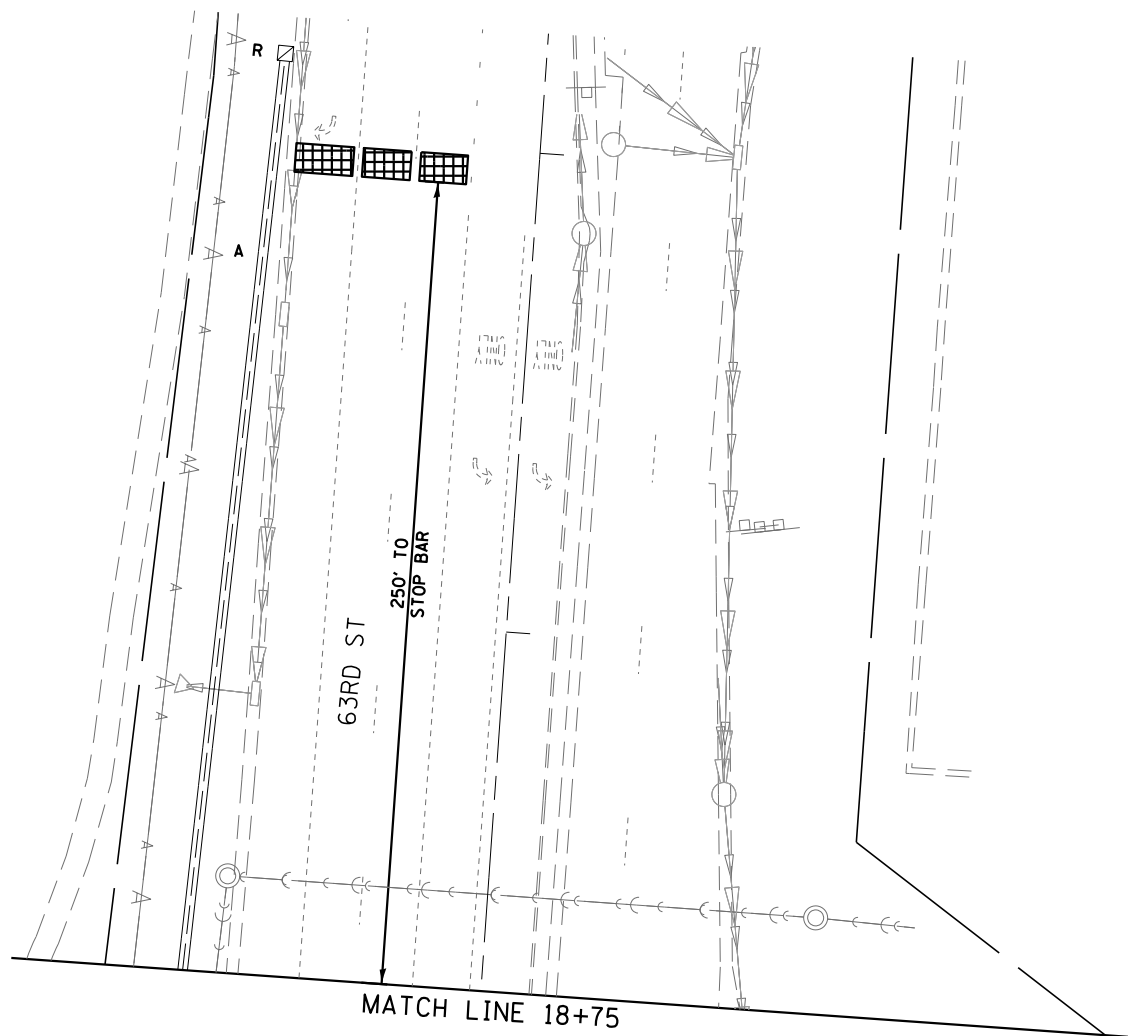
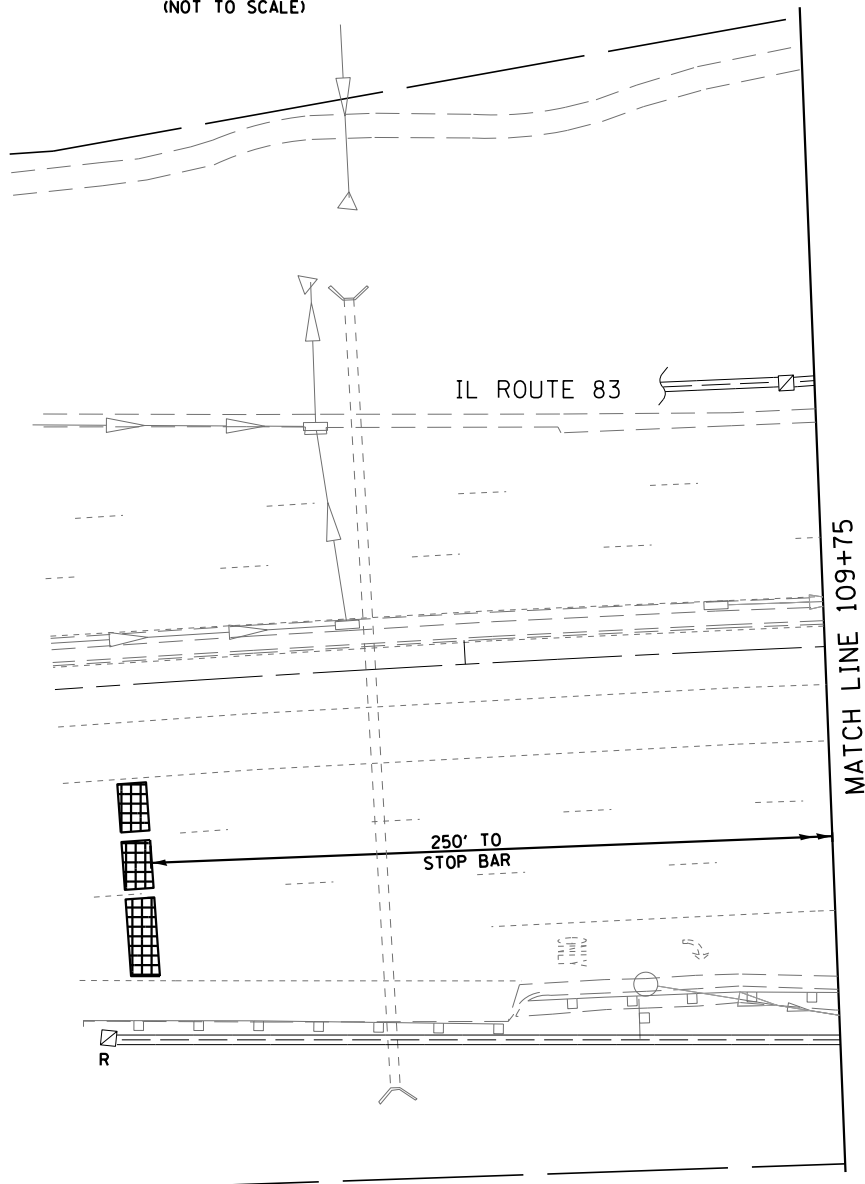
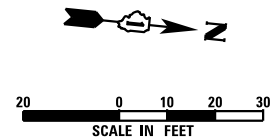
TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVAL PLAN  
IL ROUTE 83 AT 63RD ST

SCALE: AS NOTED SHEET NO. 8 OF 25 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	75
				CONTRACT NO. 60W55
ILLINOIS FED. AID PROJECT				



**TEMPORARY VIDEO DETECTION MOUNTING DETAIL**  
(NOT TO SCALE)



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 12-DEC-2014 13:48 TIME\$



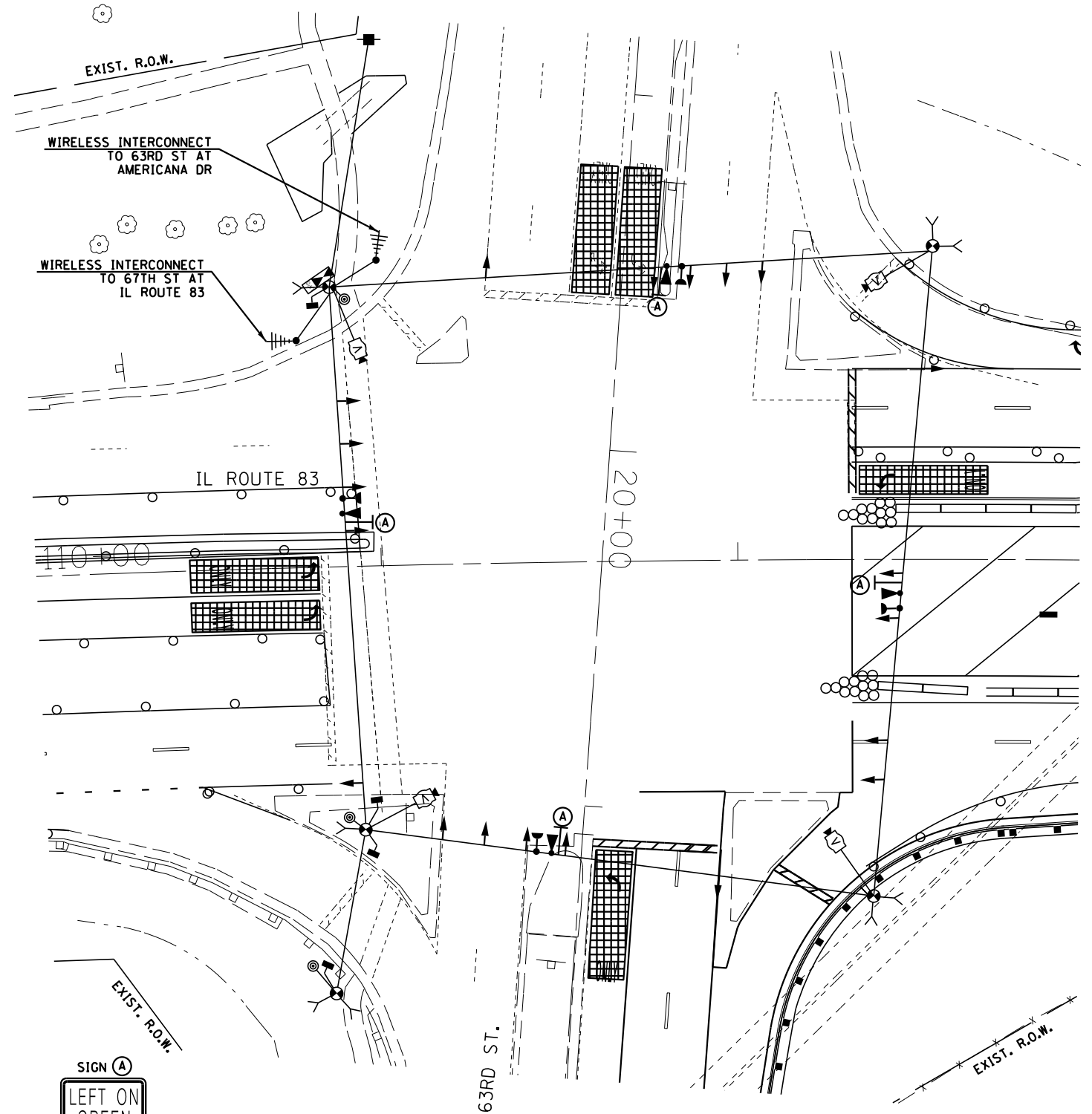
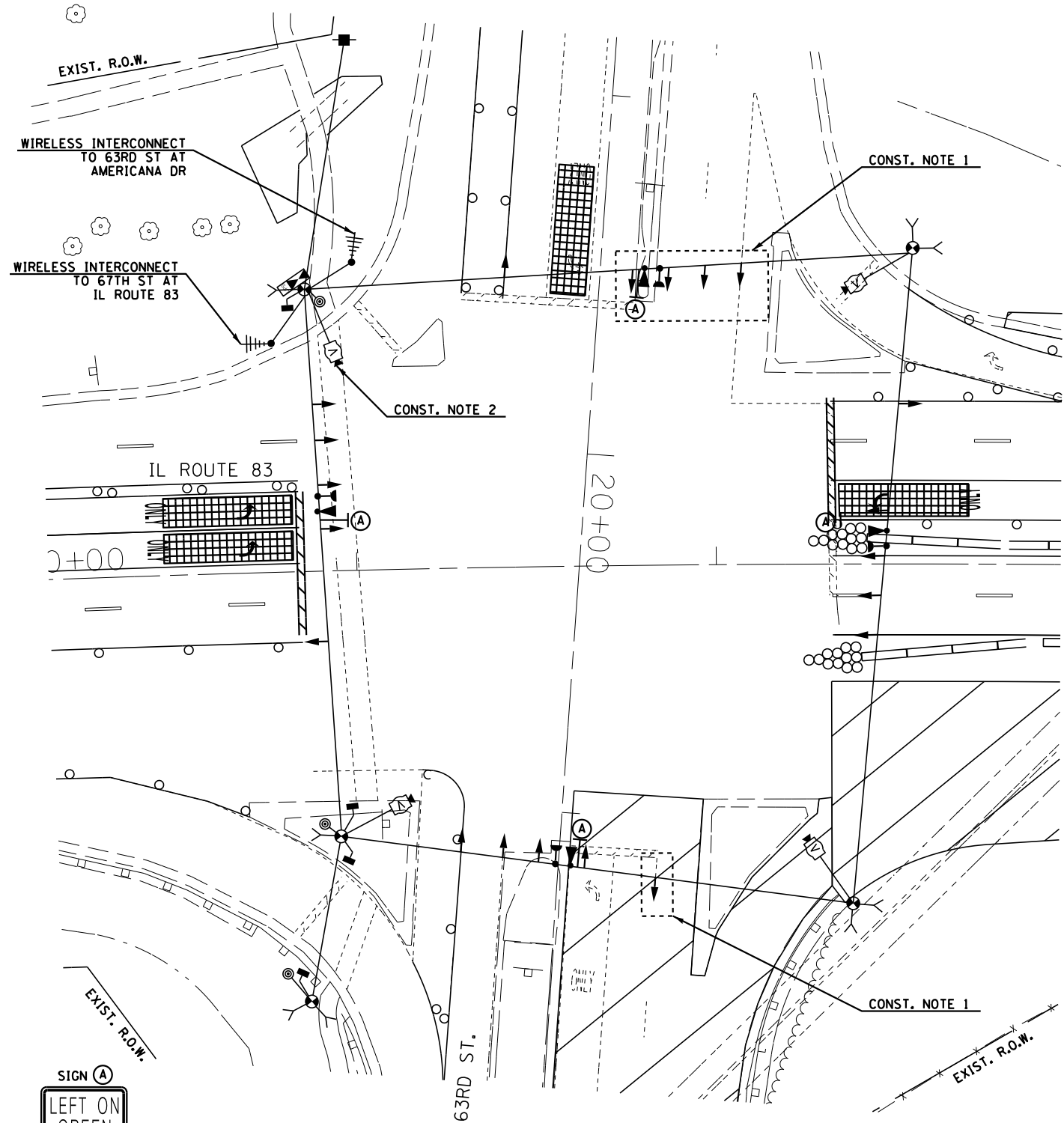
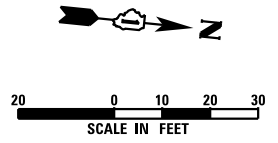
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	DATE - 11/26/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVAL PLAN  
IL ROUTE 83 AT 63RD ST (SHEET 2 OF 2)**

SCALE: AS NOTED SHEET NO. 9 OF 25 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	76
			CONTRACT NO. 60W55	
ILLINOIS FED. AID PROJECT				



R10-5  
30" X 36"  
(4 EACH)

**SIGNAL HEAD PLACEMENT FOR STAGE 1**

**CONSTRUCTION NOTES:**

1. TRAFFIC SIGNAL HEADS FOR THE WESTBOUND DIRECTION SHALL BE BAGGED AND DISCONNECTED AT THE CONTROLLER DURING CONSTRUCTION STAGE I.
2. VIDEO DETECTION CAMERA FOR THE WESTBOUND TRAFFIC SHALL BE DISCONNECTED FROM THE CONTROLLER DURING CONSTRUCTION STAGE I.



R10-5  
30" X 36"  
(4 EACH)

**SIGNAL HEAD PLACEMENT FOR STAGE 2**

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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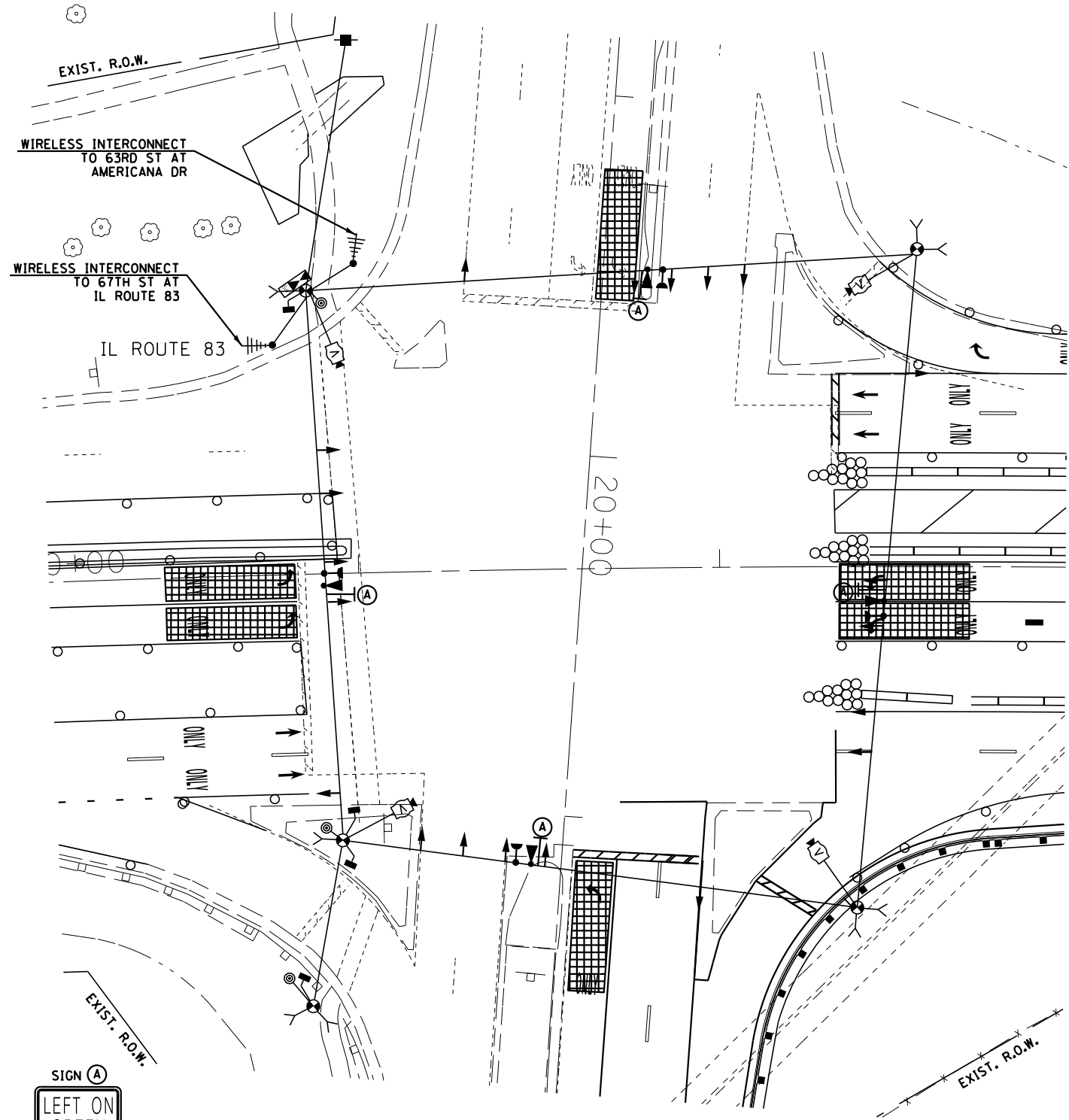
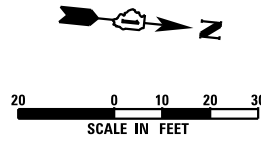


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	DATE - 11/26/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN  
IL ROUTE 83 AT 63RD ST  
STAGE 1 AND STAGE 2  
SCALE: AS NOTED SHEET NO. 10 OF 25 SHEETS STA. TO STA.

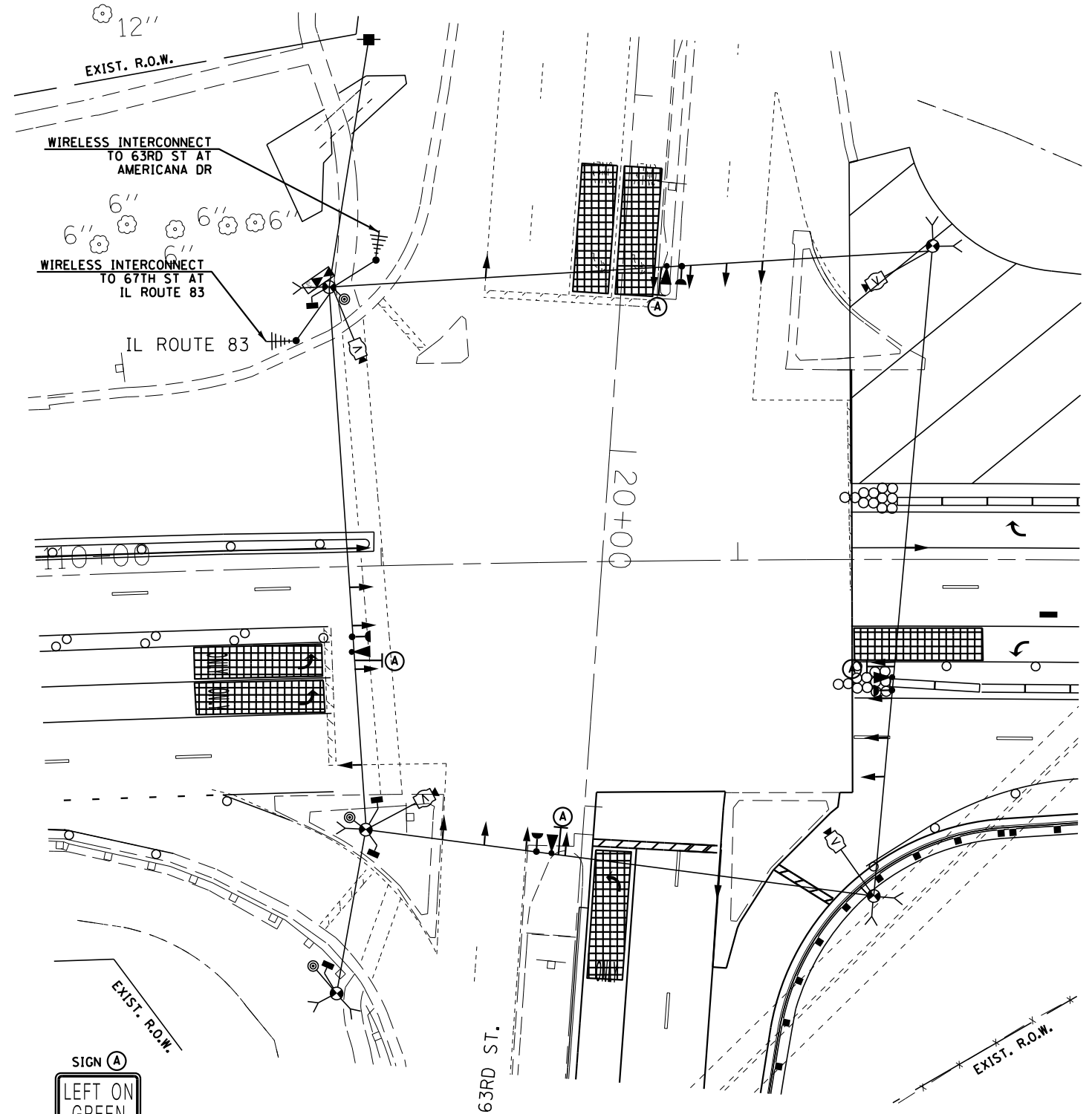
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	77
				CONTRACT NO. 60W55
ILLINOIS FED. AID PROJECT				



**SIGNAL HEAD PLACEMENT FOR STAGE 2A**



R10-5  
30" X 36"  
(4 EACH)



**SIGNAL HEAD PLACEMENT FOR STAGE 3**



R10-5  
30" X 36"  
(4 EACH)

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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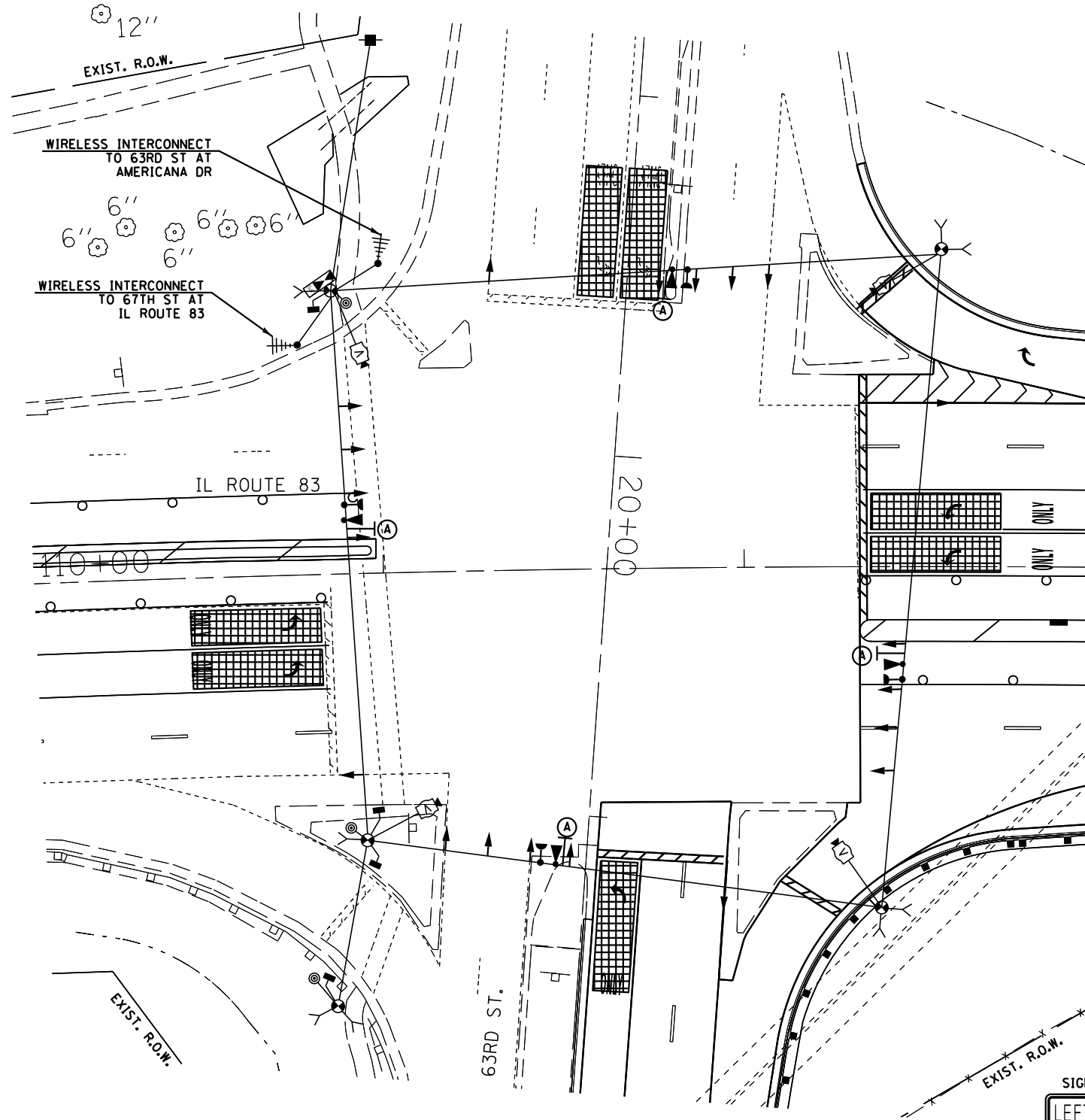
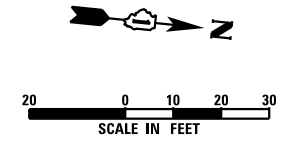
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PLOT DATE = 12-DEC-2014 13:48	DATE - 11/26/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN  
IL ROUTE 83 AT 63RD ST  
STAGE 2A AND STAGE 3

SCALE: AS NOTED SHEET NO. 11 OF 25 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	78
				CONTRACT NO. 60W55
ILLINOIS FED. AID PROJECT				



**SIGNAL HEAD PLACEMENT FOR STAGE 4**



R10-5  
30" X 36"  
(4 EACH)

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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USER NAME = mgravida	DESIGNED - MG	REVISED -
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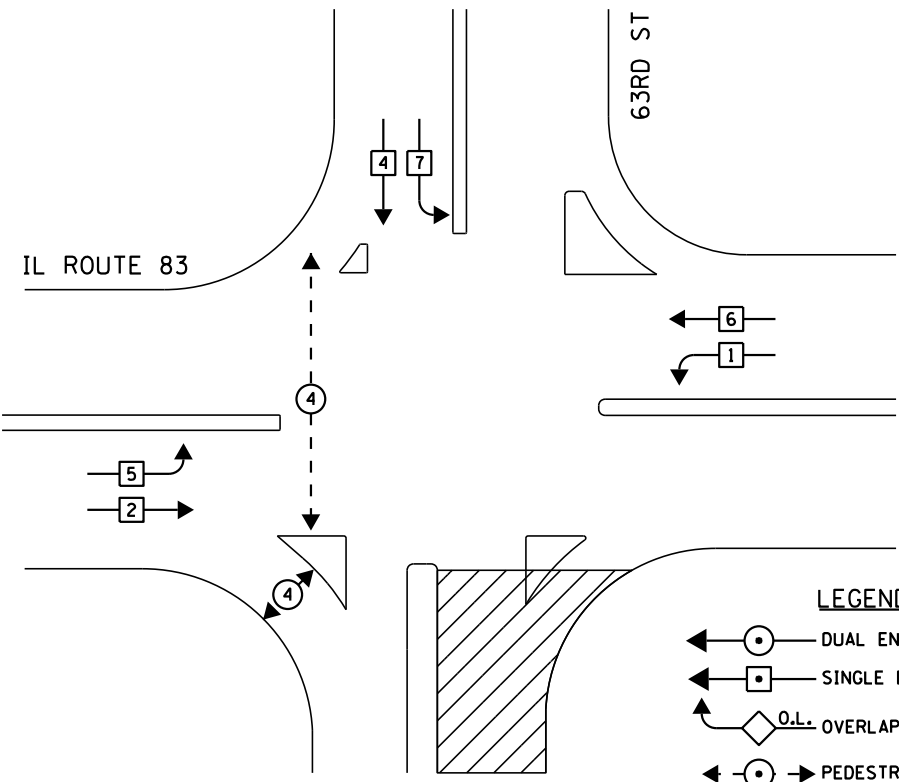
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN  
IL ROUTE 83 AT 63RD ST  
STAGE 4

SCALE: AS NOTED SHEET NO. 12 OF 25 SHEETS STA. TO STA.

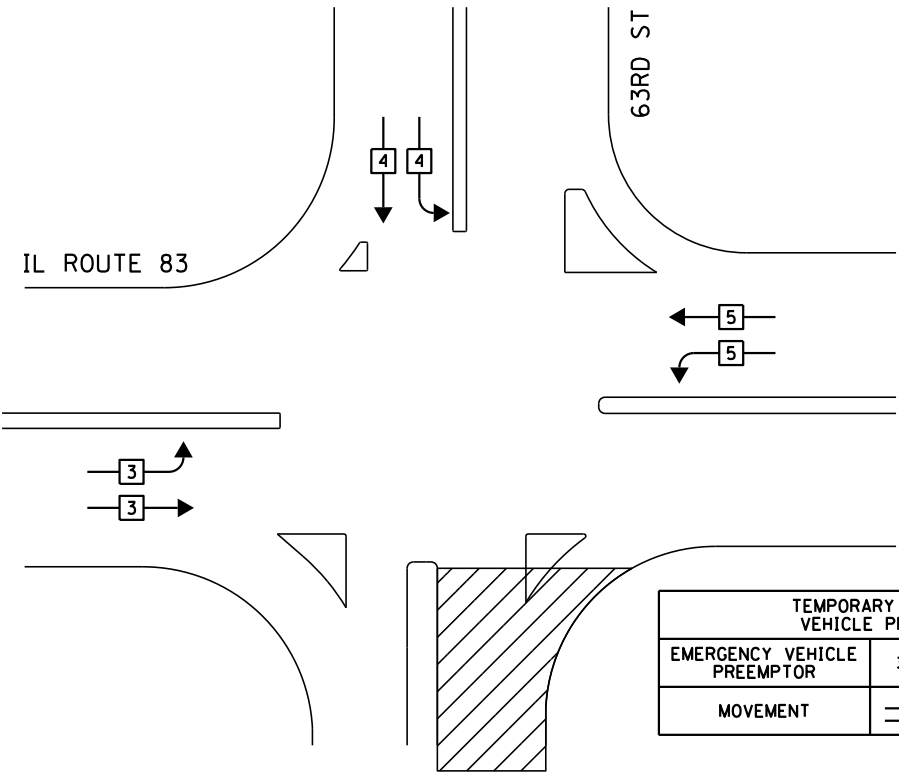
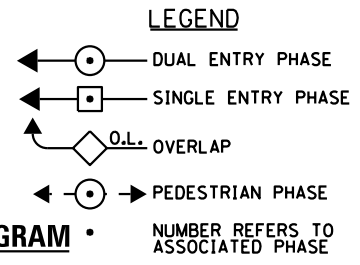
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	79
				CONTRACT NO. 60W55
ILLINOIS FED. AID PROJECT				





**TEMPORARY PHASE DESIGNATION DIAGRAM**

STAGE I  
(NOT TO SCALE)



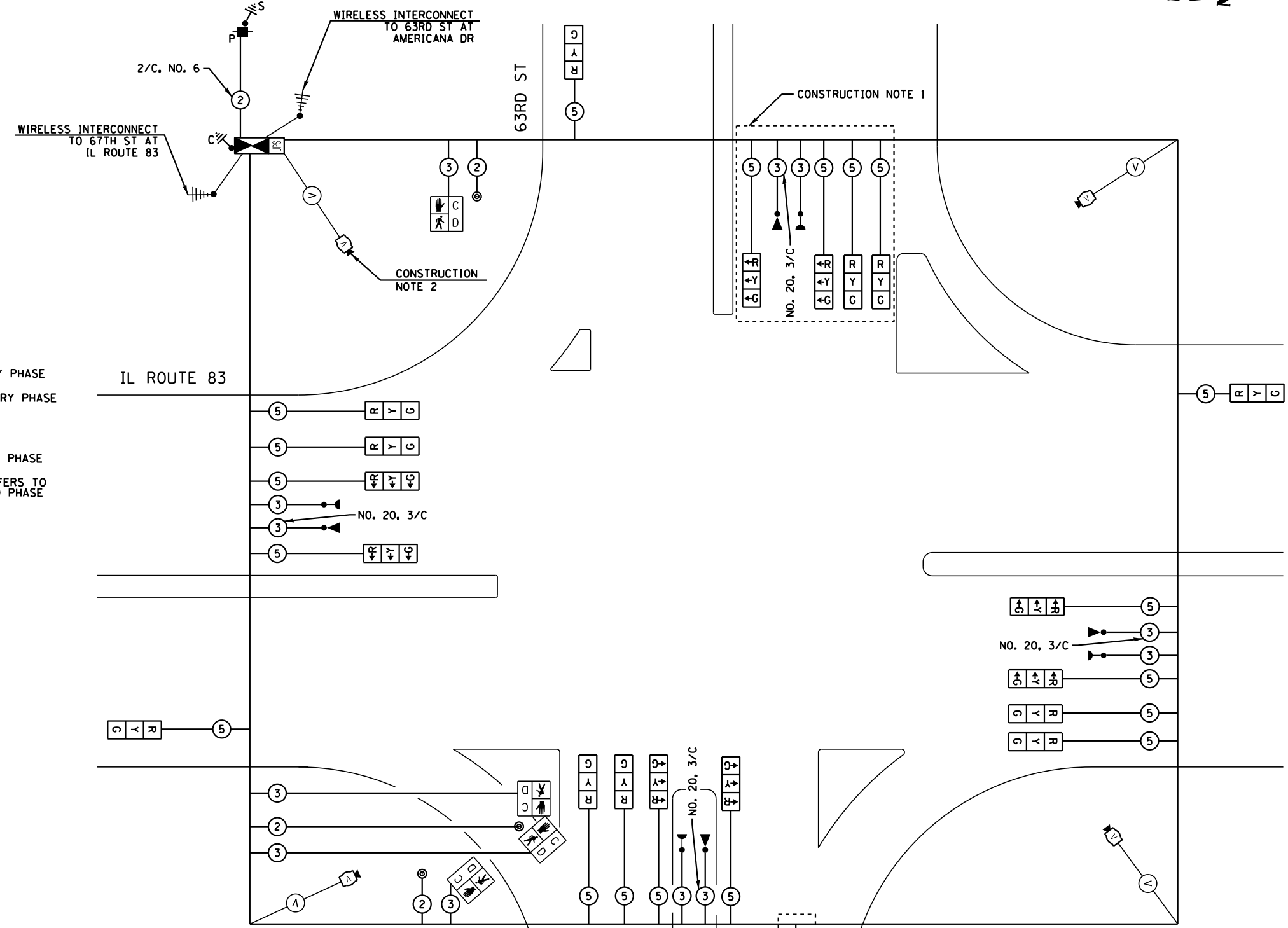
**TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE**

STAGE I  
(NOT TO SCALE)

TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE			
EMERGENCY VEHICLE PREEMPTOR	3	4	5
MOVEMENT	→	↓	←

**CONSTRUCTION NOTES:**

1. TRAFFIC SIGNAL HEADS FOR THE WESTBOUND DIRECTION SHALL BE BAGGED AND DISCONNECTED AT THE CONTROLLER DURING CONSTRUCTION STAGE I.
2. VIDEO DETECTION CAMERA FOR THE WESTBOUND TRAFFIC SHALL BE DISCONNECTED FROM THE CONTROLLER DURING CONSTRUCTION STAGE I.



**CABLE PLAN**

STAGE I

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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	DATE - 11/26/2014	REVISED -

DESIGNED - MG	REVISED -
DRAWN - MG	REVISED -
CHECKED - KGP	REVISED -
DATE - 11/26/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

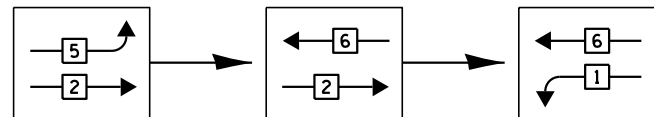
TEMPORARY CABLE PLAN, TEMPORARY PHASE DESIGNATION DIAGRAM,  
AND TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION  
IL ROUTE 83 AT 63RD ST. STAGE I

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	80
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				

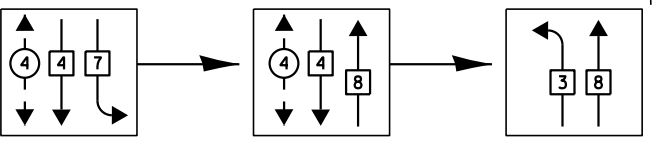
SCALE: AS NOTED SHEET NO. 13 OF 25 SHEETS STA. TO STA.

**TEMPORARY PHASE DESIGNATION DIAGRAM**

FOR PHASES 1, 2, 5, AND 6 IN THE PHASE DESIGNATION DIAGRAM SHOWN. THE FOLLOWING SEQUENCE SHALL BE FOLLOWED DURING CONSTRUCTION STAGES II, III, AND IV



FOR PHASES 3, 4, 7, AND 8 IN THE PHASE DESIGNATION DIAGRAM SHOWN ABOVE. THE FOLLOWING SEQUENCE SHALL BE FOLLOWED DURING CONSTRUCTION STAGES II, III, AND IV

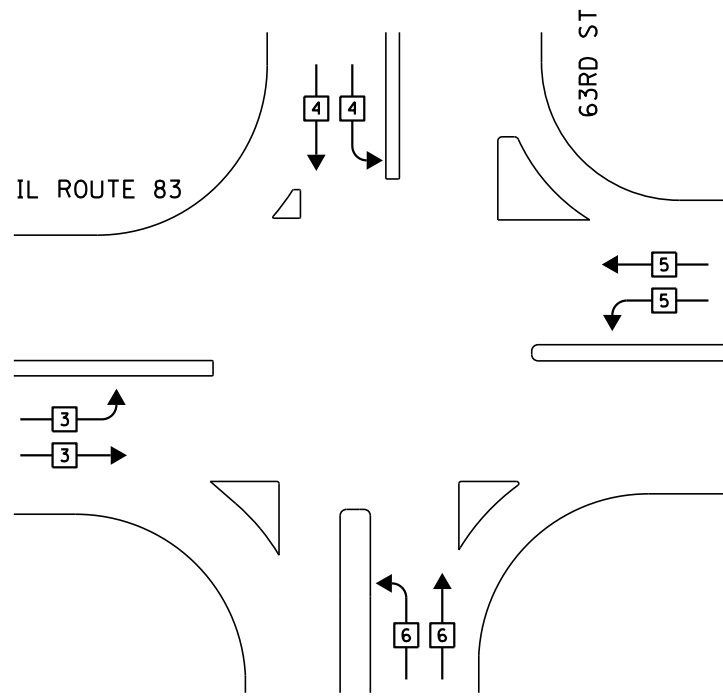


**LEGEND**

- ◀ ○ ▶ DUAL ENTRY PHASE
  - ◀ ◻ ▶ SINGLE ENTRY PHASE
  - ◀ ◊ ▶ OVERLAP
  - ◀ ○ ▶ PEDESTRIAN PHASE
- NUMBER REFERS TO ASSOCIATED PHASE

**TEMPORARY PHASE DESIGNATION DIAGRAM**

PRE-STAGE, STAGES II, III, IV AND INTERIM  
(NOT TO SCALE)



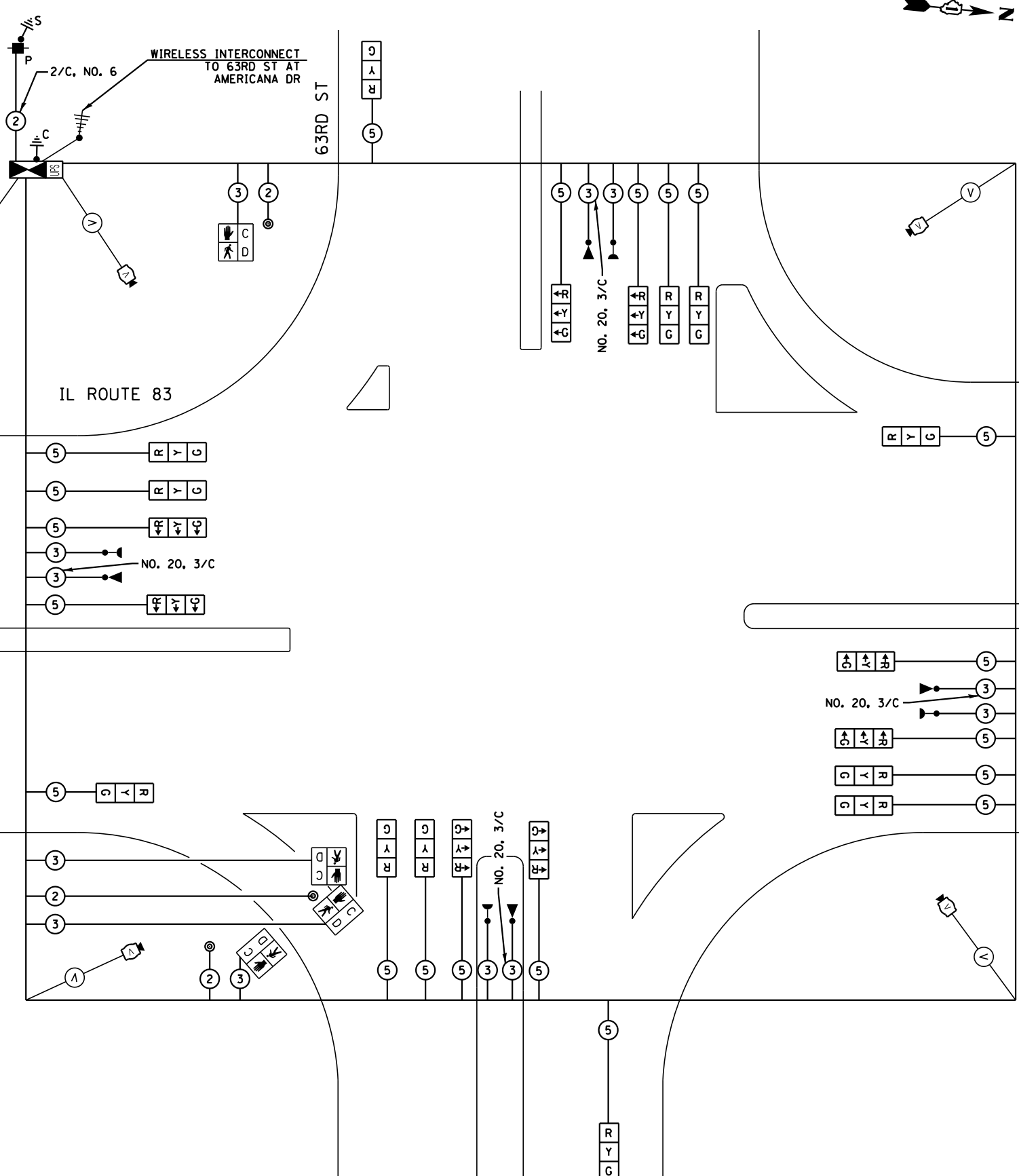
**TEMPORARY PHASE DESIGNATION DIAGRAM**

PRE-STAGE, STAGES II, III, IV AND INTERIM  
(NOT TO SCALE)

TYPE	NO LAMPS	WATTAGE		%OPERATION	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	20	17	0.50	170.00	
(YELLOW)	20	25	0.25	125.00	
(GREEN)	20	15	0.25	75.00	
ARROW		12	0.10		
PED. SIGNAL	4	25	1.00	100.0	
CONTROLLER	1	100	1.00	100.0	
ILLUM. SIGN	-	25	0.05	-	
VIDEO SYSTEM	1	150	1.00	150.00	
FLASHER			0.50		
ENERGY COSTS TO:				TOTAL =	720.00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196-1096  
ENERGY SUPPLY CONTACT: JUANITA VEIGELT  
PHONE: 630-985-0569  
COMPANY: COMMONWEALTH EDISON

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



**CABLE PLAN**

PRE-STAGE, STAGES II, III, IV AND INTERIM

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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**SINGH**  
SINGH & ASSOCIATES, INC.  
CONSULTING ENGINEERS

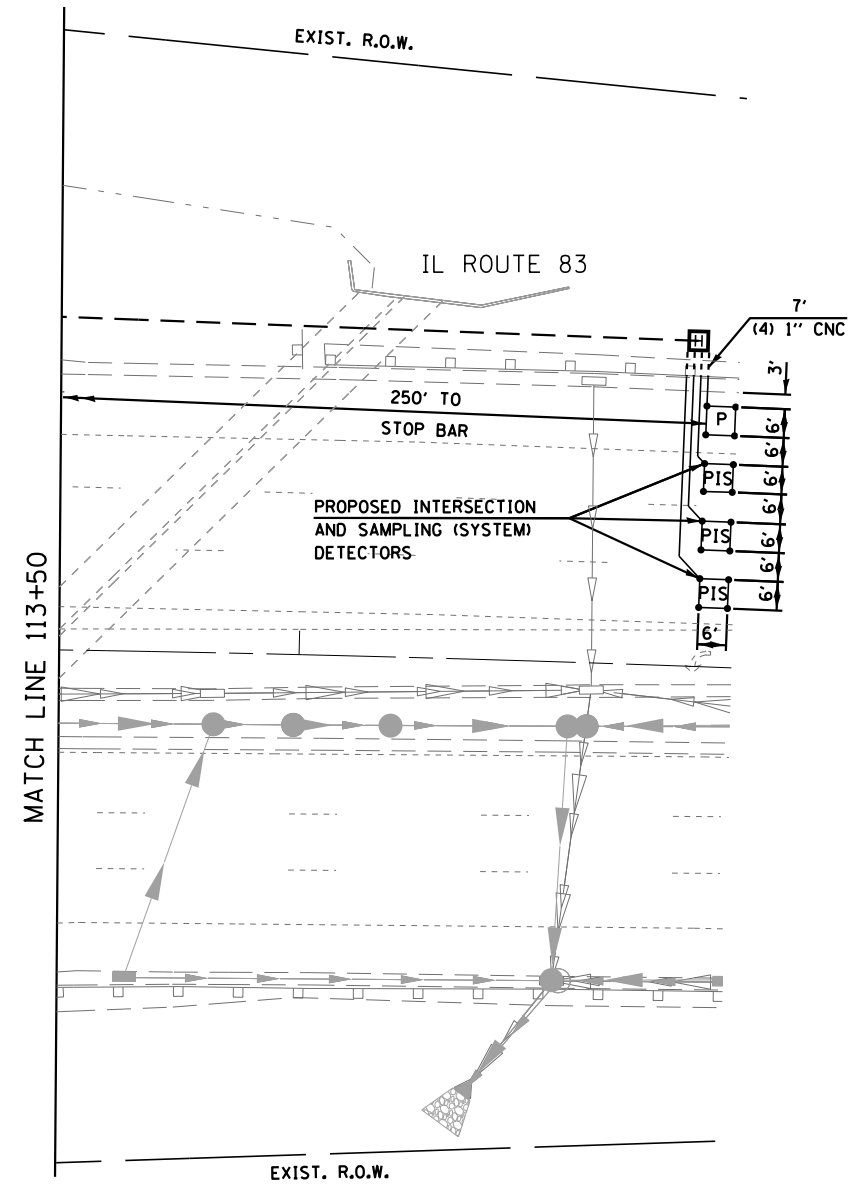
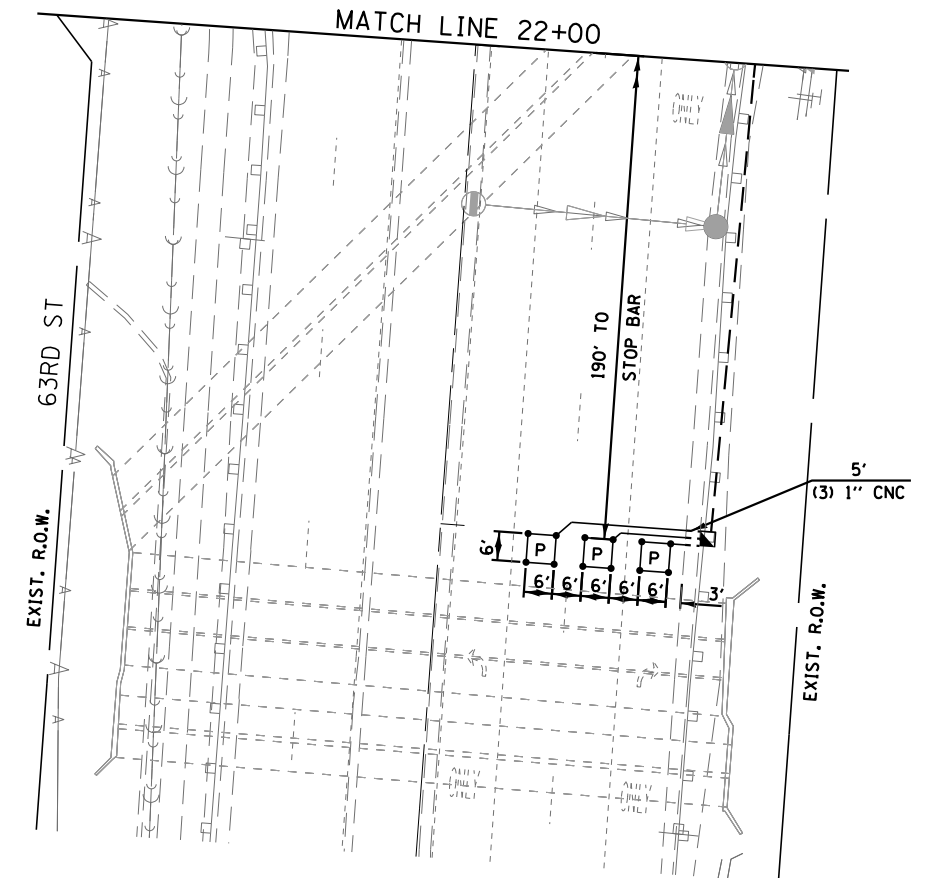
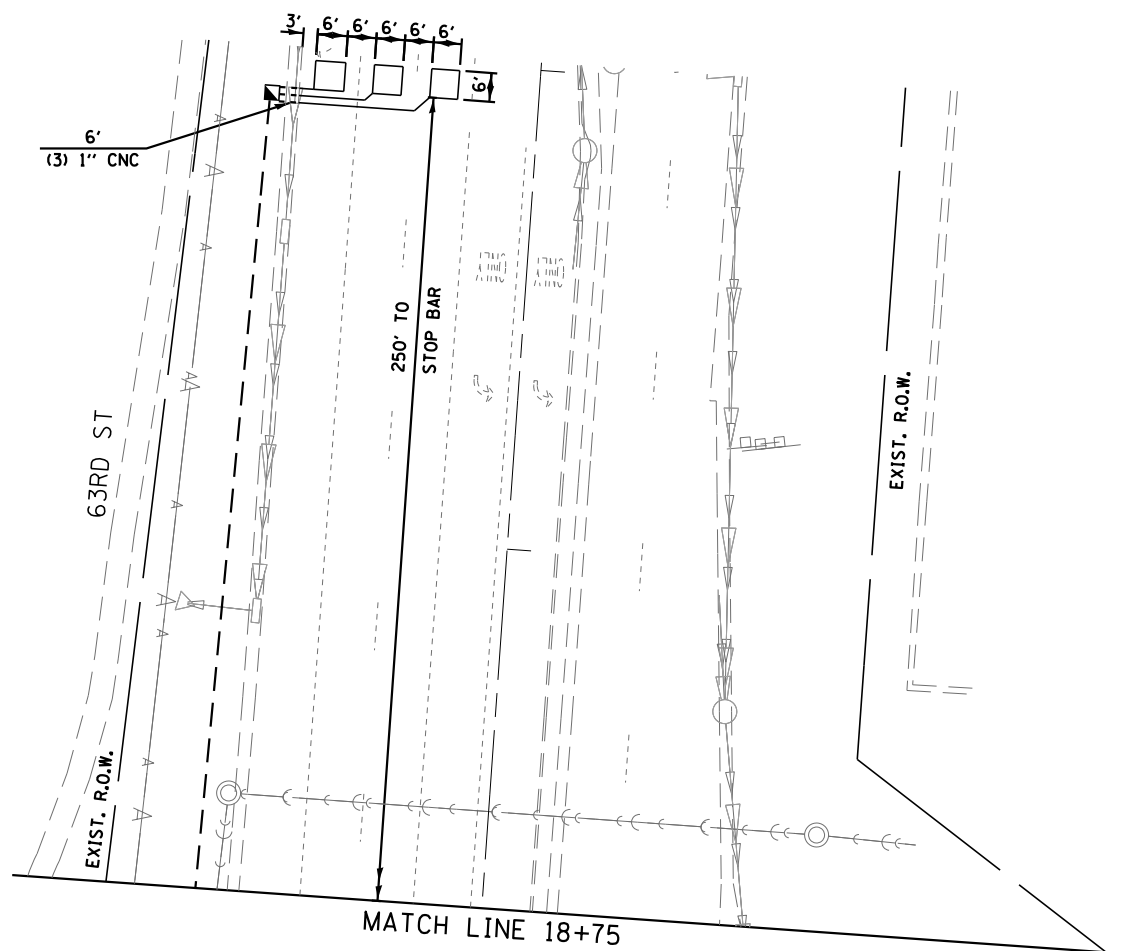
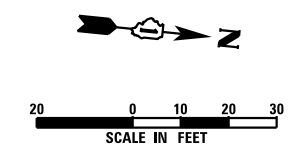
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DESIGNED - MG  
DRAWN - MG  
CHECKED - KGP  
DATE - 11/26/2014

REVISIED -  
REVISIED -  
REVISIED -  
REVISIED -

TEMPORARY CABLE PLAN, TEMPORARY PHASE DESIGNATION DIAGRAM,  
AND TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION  
IL ROUTE 83 AT 63RD ST. - PRE-STAGE, STAGES II, III, IV AND INTERIM  
SCALE: AS NOTED SHEET NO. 14 OF 25 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	81
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				





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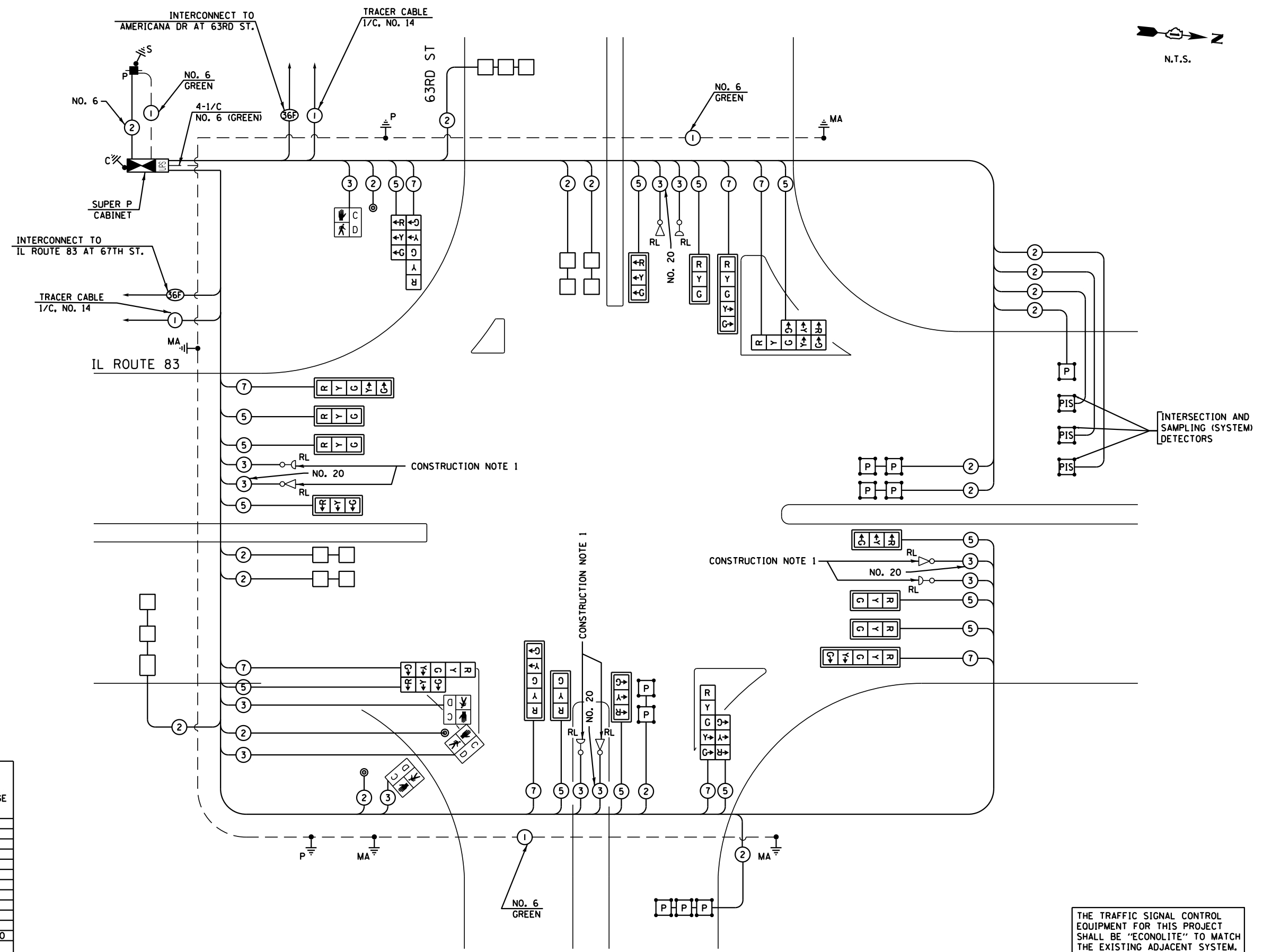
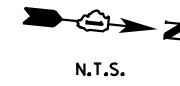
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODERNIZATION PLAN  
 IL ROUTE 83 AT 63RD ST (SHEET 2 OF 2)**

SCALE: AS NOTED    SHEET NO. 16 OF 25 SHEETS    STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	83
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				

TS 565



I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS						
TYPE	NO LAMPS	WATTAGE		%OPERATION	TOTAL WATTAGE	
		INCAND.	LED			
SIGNAL (RED)	22		17	0.50	187.0	
(YELLOW)	22		25	0.25	137.5	
(GREEN)	22		15	0.25	82.5	
ARROW	16		12	0.10	19.20	
PED. SIGNAL	4		25	1.00	100.0	
CONTROLLER	1		100	1.00	100.0	
ILLUM. SIGN	-		25	0.05	-	
VIDEO SYSTEM	-	150		1.00	-	
UPS			25	1.00	25.0	
FLASHER				0.50		
ENERGY COSTS TO:					TOTAL =	651.20
ILLINOIS DEPARTMENT OF TRANSPORTATION 201 WEST CENTER COURT SCHAUMBURG, ILLINOIS 60196-1096 ENERGY SUPPLY CONTACT: JUANITA VEIGELT PHONE: 630-985-0569 COMPANY: COMMONWEALTH EDISON						

- CONSTRUCTION NOTE:**
1. THE CONTRACTOR SHALL RELOCATE THE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM EQUIPMENT TO THE PROPOSED MAST ARMS.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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PLOT DATE = 12-DEC-2014 13:48	CHECKED - KGP	REVISED -
	DATE - 11/26/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>CABLE PLAN</b>	
<b>IL ROUTE 83 AT 63RD ST</b>	
SCALE: AS NOTED	SHEET NO. 17 OF 25 SHEETS STA. TO STA.

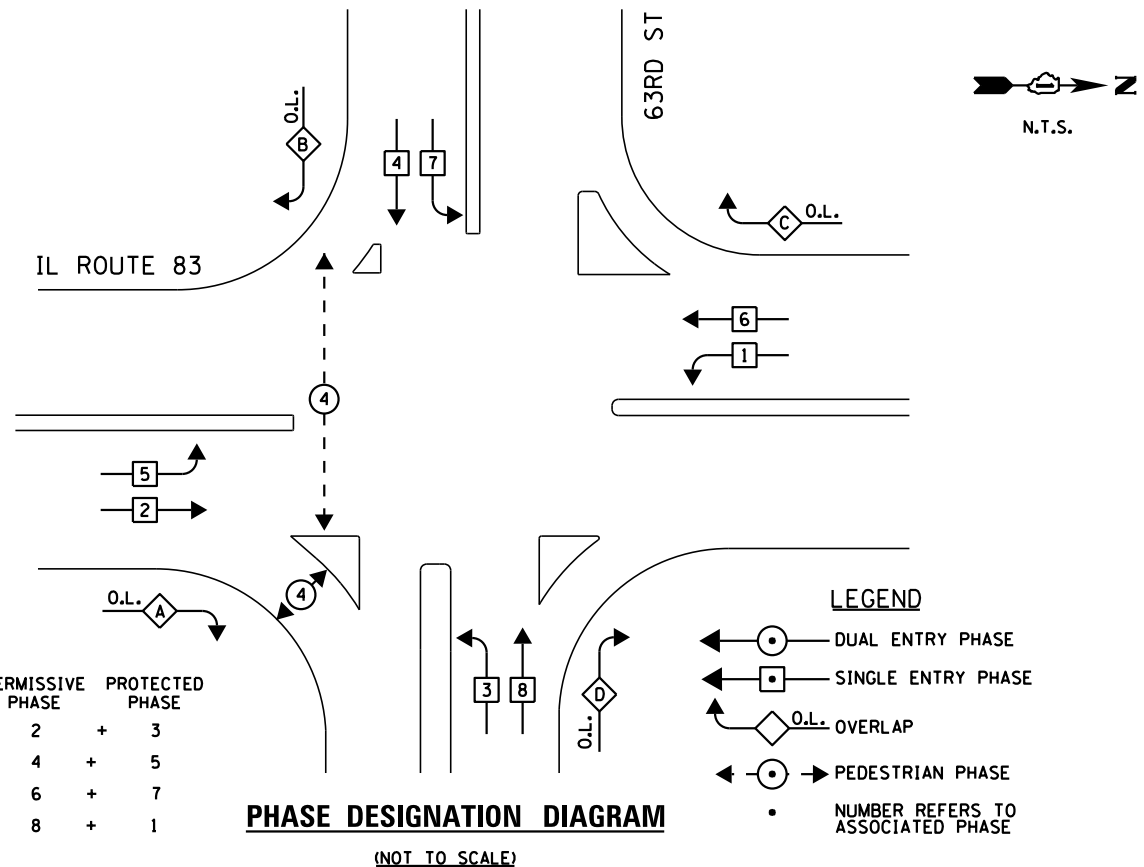
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	84
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				

TS 565

SCHEDULE OF QUANTITIES

QUANTITY	UNIT	ITEM
72	SQ FT	SIGN PANEL - TYPE 1
30	SQ FT	SIGN PANEL - TYPE 2
1	EACH	SERVICE INSTALLATION - POLE MOUNTED
1329	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
20	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.
65	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.
684	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
7	EACH	HANDHOLE
6	EACH	HEAVY-DUTY HANDHOLE
2	EACH	DOUBLE HANDHOLE
484	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
•• 1714	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
3186	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
1664	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
4342	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
95	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C
810	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 36 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 42 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 54 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 55 FT.
8	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
54	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
10	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED
4	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED
4	EACH	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
1	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
14	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
14	EACH	INDUCTIVE LOOP DETECTOR
514	FOOT	DETECTOR LOOP, TYPE I
553	FOOT	PREFORMED DETECTOR LOOP
3	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
• 4	EACH	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT
• 1	EACH	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
15	EACH	REMOVE EXISTING HANDHOLE
1	EACH	REMOVE EXISTING DOUBLE HANDHOLE
7	EACH	REMOVE EXISTING CONCRETE FOUNDATION
• 996	FOOT	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
1	EACH	UNINTERRUPTABLE POWER SUPPLY, SPECIAL
2	EACH	TEMPORARY TRAFFIC SIGNAL TIMING

- 100% OF THE COST TO THE TRI-STATE FIRE PROTECTION DISTRICT
- 996' IS USED FOR CONFIRMATION BEACONS (TO BE PAID FOR BY THE TRI-STATE FIRE PROTECTION DISTRICT.



**EMERGENCY VEHICLE PREEMPTION SEQUENCE**  
(NOT TO SCALE)

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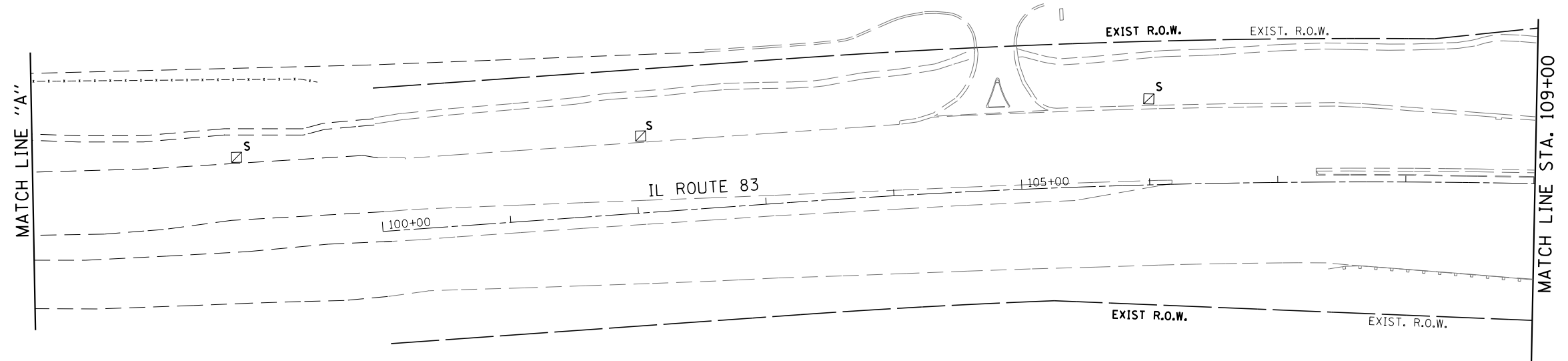
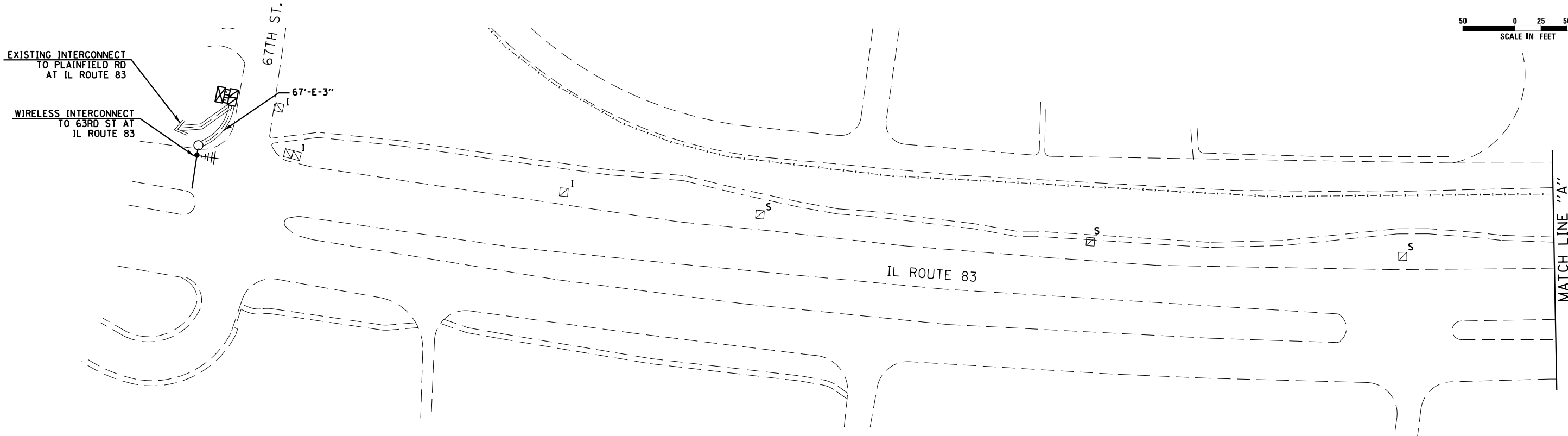
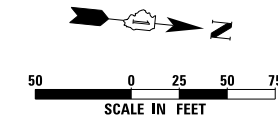
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PHASE DESIGNATION DIAGRAM, EMERGENCY VEHICLE PREEMPTION SEQUENCE  
OF OPERATION, AND SCHEDULE OF QUANTITIES  
IL ROUTE 83 AT 63RD ST.

SCALE: AS NOTED SHEET NO. 18 OF 25 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	85
				CONTRACT NO. 60W55
ILLINOIS FED. AID PROJECT				

TS 565



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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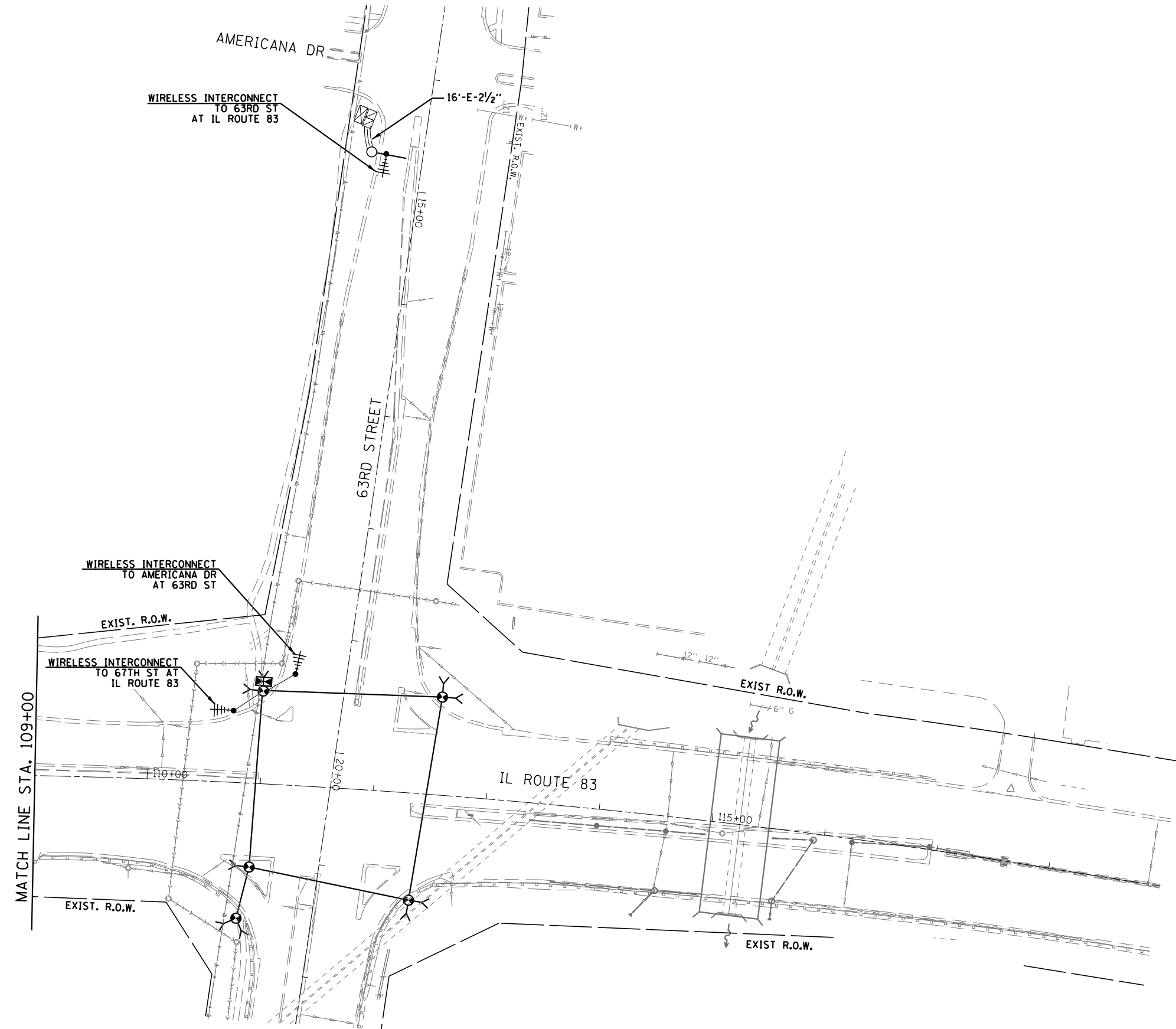
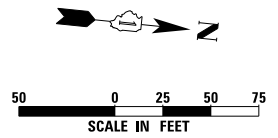
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	DATE - 11/26/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY INTERCONNECT PLAN  
IL ROUTE 83 FROM 67TH ST. TO 63RD ST.**

SCALE: AS NOTED | SHEET NO. 19 OF 25 SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	86
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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	DATE - 11/26/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY INTERCONNECT PLAN  
IL ROUTE 83 FROM 67TH ST. TO 63RD ST.**

SCALE: AS NOTED    SHEET NO. 20 OF 25 SHEETS    STA. TO STA.

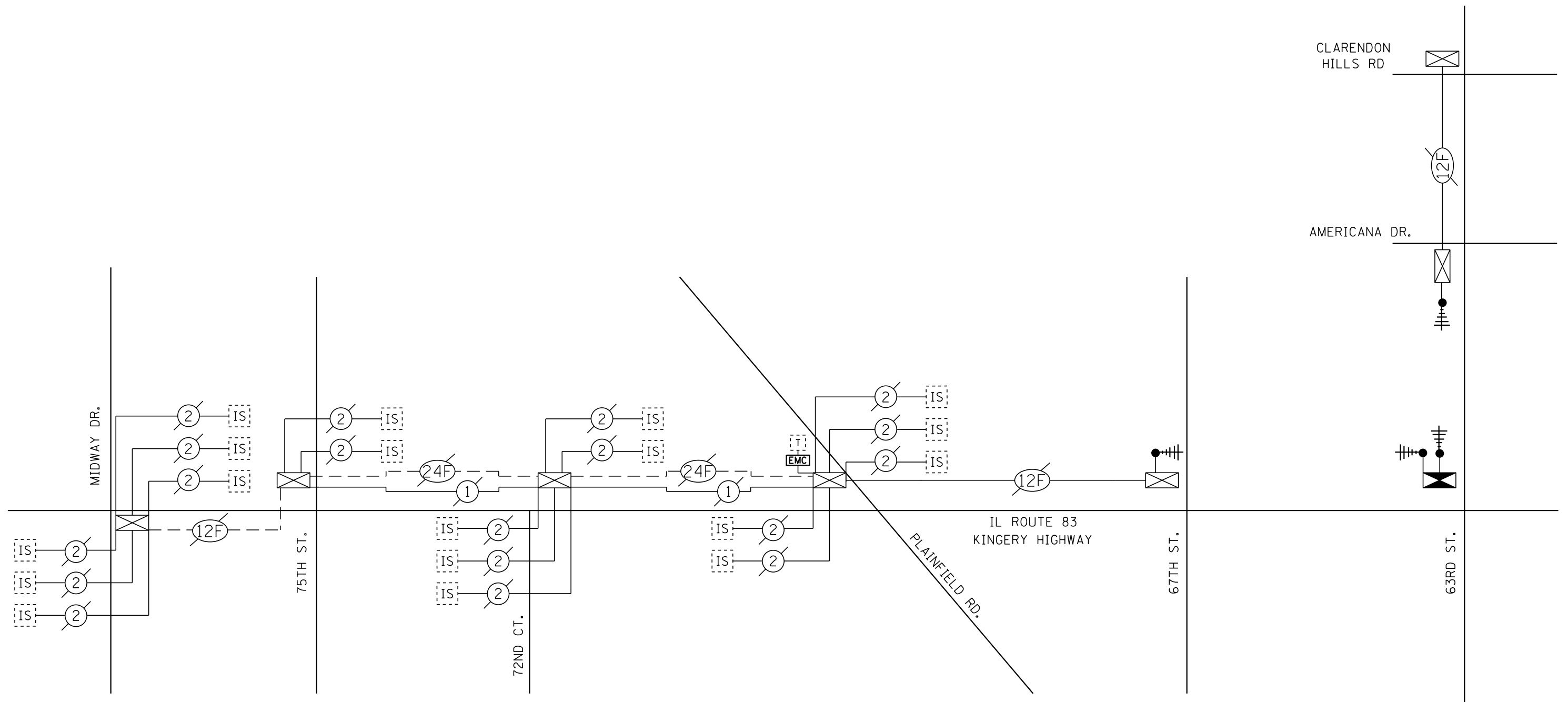
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	87
				CONTRACT NO. 60W55

ILLINOIS FED. AID PROJECT





N.T.S.



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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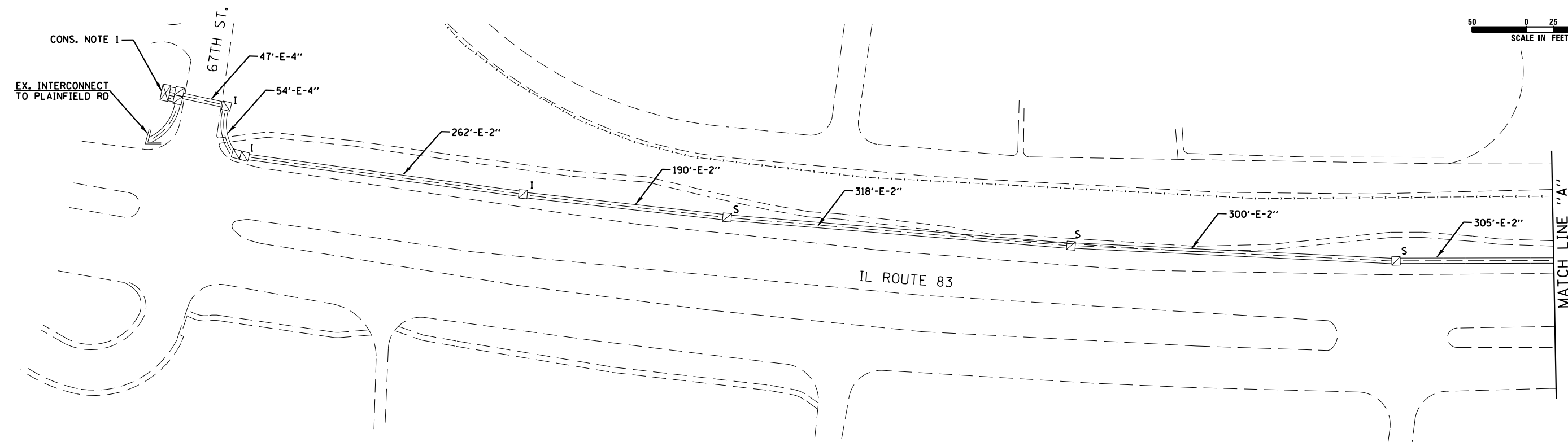
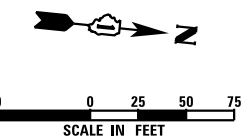
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PLOT DATE = 12-DEC-2014 13:55	DATE - 11/26/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TEMPORARY INTERCONNECT SCHEMATIC  
IL ROUTE 83 FROM MIDWAY DR. TO 63RD ST.

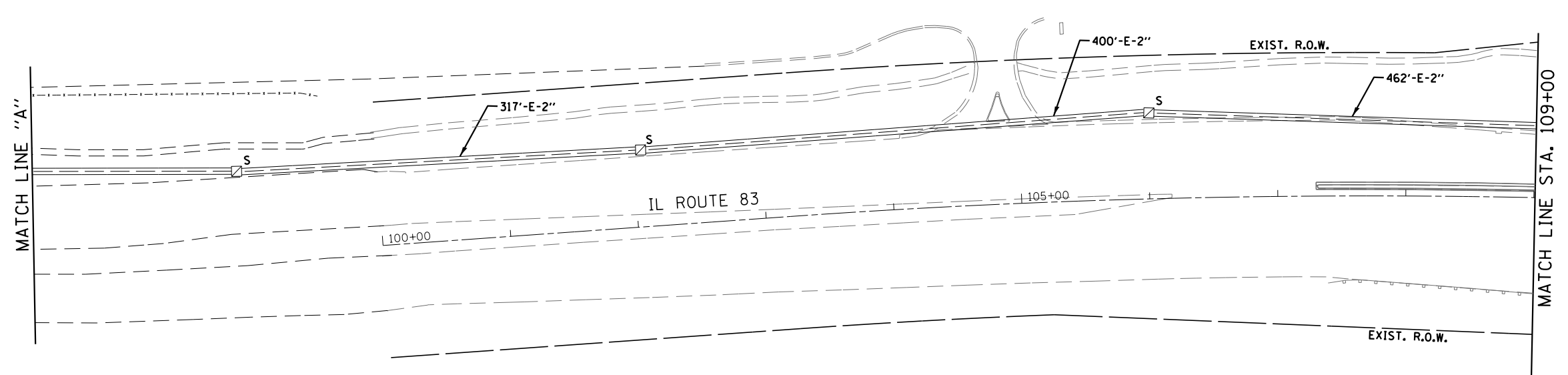
SCALE: AS NOTED SHEET NO. 21 OF 25 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	88
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				



**CONSTRUCTION NOTE:**

1. DISCONNECT THE EXISTING INTERCONNECT CABLE AT THE EXISTING CONTROLLER ON IL ROUTE 83 AT 67TH ST & IL 83 AT 63RD ST. REMOVE EXISTING INTERCONNECT CABLE AND TRACER CABLE BETWEEN THE TWO CONTROLLERS (5622 FEET).



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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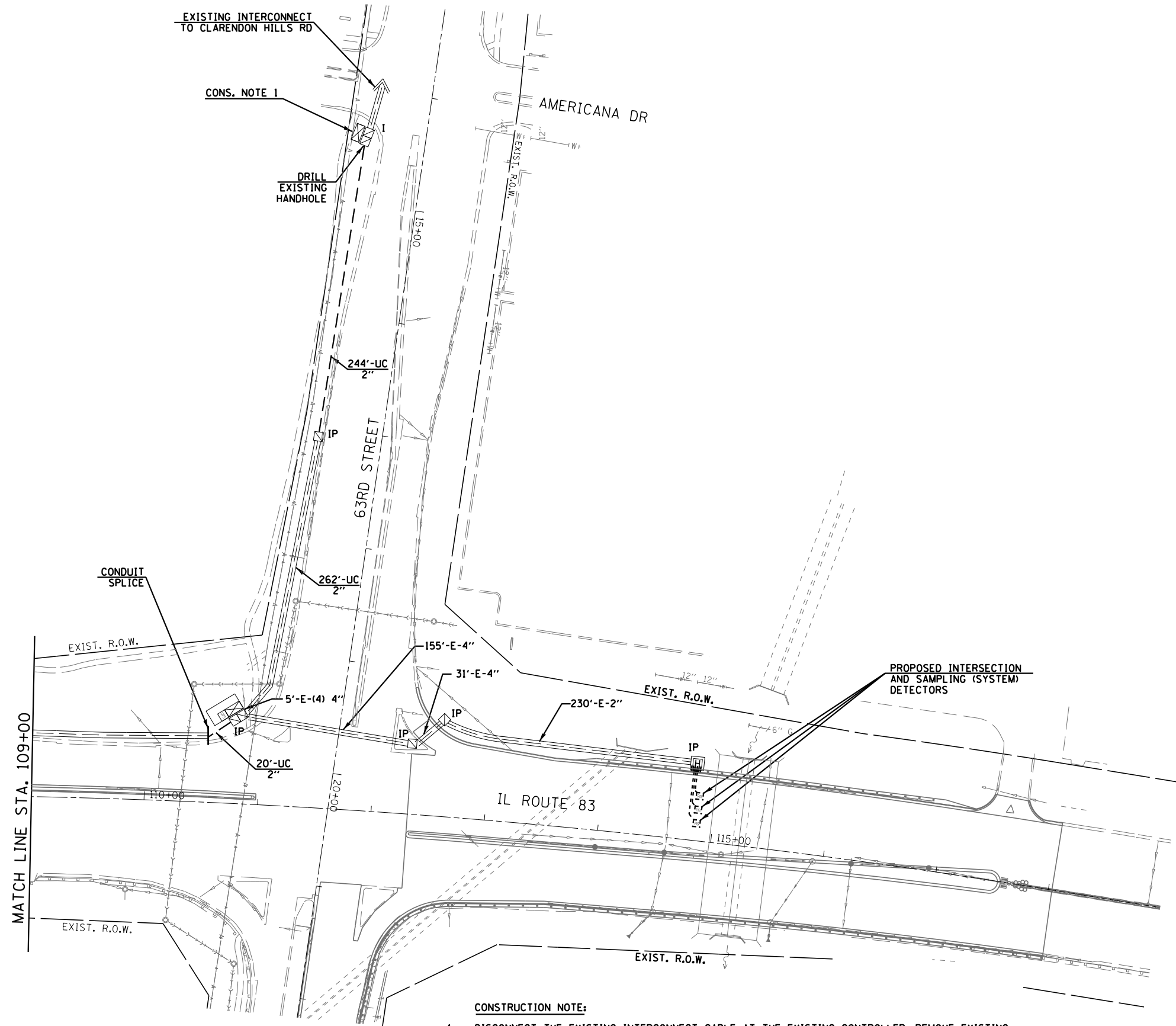
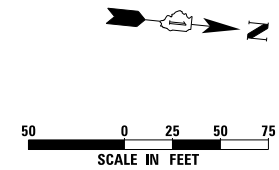
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	DATE - 11/26/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PROPOSED INTERCONNECT PLAN  
IL ROUTE 83 FROM 67TH ST. TO 63RD ST. (SHEET 1 OF 2)  
SCALE: AS NOTED SHEET NO. 22 OF 25 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	89
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				

ECON 112



**CONSTRUCTION NOTE:**

- DISCONNECT THE EXISTING INTERCONNECT CABLE AT THE EXISTING CONTROLLER. REMOVE EXISTING INTERCONNECT CABLE AND TRACER CABLE FROM THE EXISTING CONTROLLER TO EXISTING DOUBLE HANDHOLE (58 FEET).

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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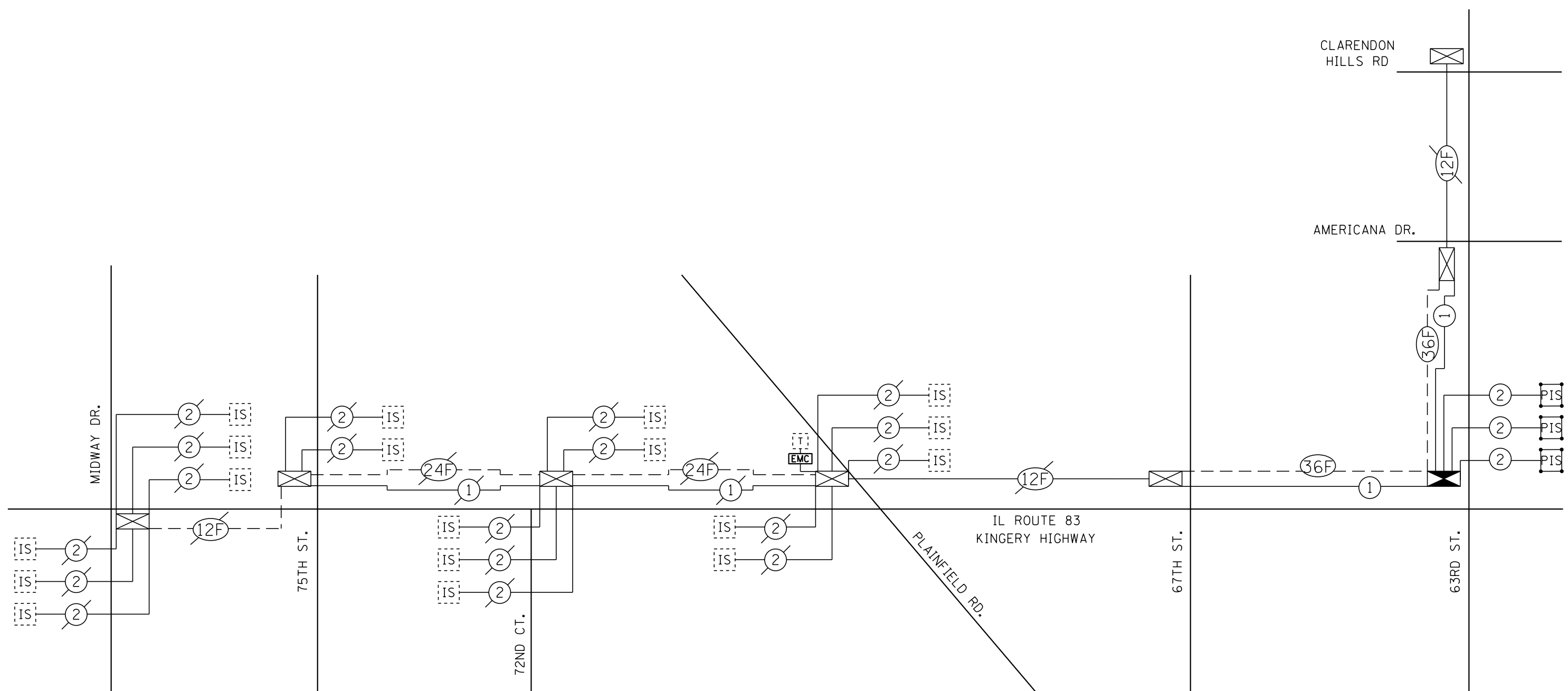
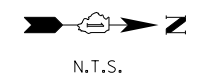
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED INTERCONNECT PLAN  
IL ROUTE 83 FROM 67TH ST. TO 63RD ST. (SHEET 2 OF 2)**

SCALE: AS NOTED SHEET NO. 23 OF 25 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	90
				CONTRACT NO. 60W55
ILLINOIS FED. AID PROJECT				

ECON 112



**SCHEDULE OF INTERCONNECT QUANTITIES**

QUANTITY	UNIT	ITEM
264	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
2	EACH	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
1	EACH	TRANSCEIVER-FIBER OPTIC
3343	FOOT	ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1C
1	EACH	DRILL EXISTING HANDHOLE
5680	FOOT	REMOVE ELECTRIC CABLE FROM CONDUIT
1	EACH	CONDUIT SPLICE
3395	FOOT	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F
1	EACH	OPTIMIZE TRAFFIC SIGNAL SYSTEM

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

ECON 112

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

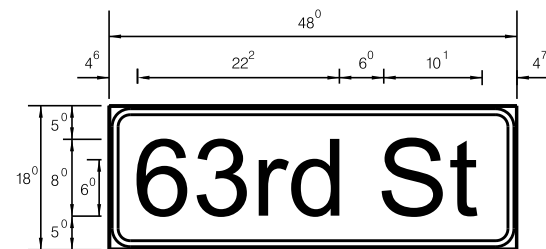
**INTERCONNECT SCHEMATIC  
IL ROUTE 83 FROM 63RD ST. TO MIDWAY DR.**

SCALE: AS NOTED SHEET NO. 24 OF 25 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	91
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				



- Sq. M. each  
 15.0 Sq. Ft. each  
 2 Required  
 Design Series D



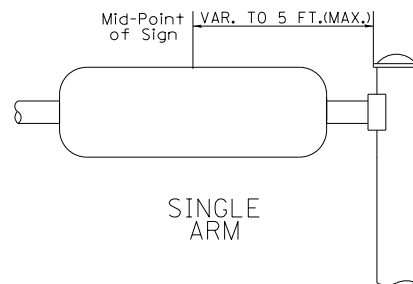
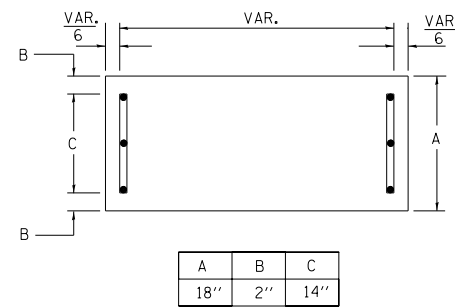
- Sq. M. each  
 6.0 Sq. Ft. each  
 2 Required  
 Design Series D

**GENERAL NOTES**

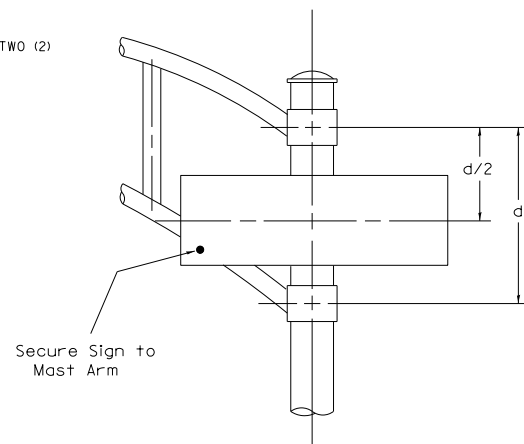
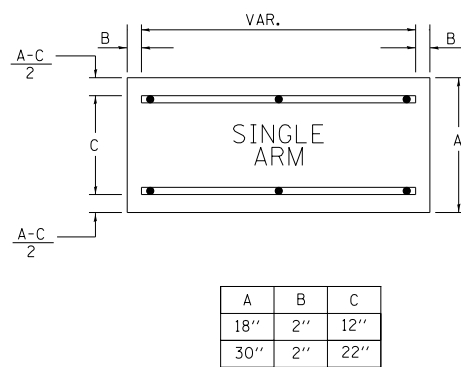
- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 834001, 834006 AND 834011, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 6'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
  - ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
  - THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 6'-0".
  - ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
  - SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:
 

* A.K.T. CORPORATION SCHAUMBURG, IL	* AMERICAN FABRICATION CO. CHICAGO HEIGHTS, IL
* TUCKER COMPANY, INC. WAUWATOSA, WI	* WESTERN TRAFFIC CONTROL INC. CICERO, IL
- PARTS LISTING:**  
 SIGN CHANNEL PART #HPN053 (MED. CHANNEL)  
 SIGN CHANNEL 1/4" x 14 x 1" H.W.H. #3  
 SIGN SCREWS SELF TAPPING WITH NEOPRENE WASHER  
 BRACKETS PART #HPN034 (UNIVERSAL)  
 CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING  
 OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BRACKET OF THE ABOVE PRODUCT.

**SUPPORTING CHANNELS**



**SUPPORTING CHANNELS**



**DUAL ARM**  
 SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM  
 Shall be used. See Note #5.

**Upper Case To Lower Case**  
 Spacing Chart 8-6 Inch Series "C & D"

EXAMPLE, 2<sup>3</sup> DENOTES 3/8"

SERIES	SECOND LETTER															
	a c d e		b h i k l		f w		j		s t		v y		x		z	
	g o q	m n p r u														
A W X	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>
B	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>
C E G	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
D O Q R	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
F	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>4</sup>	1 <sup>5</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>
H I M N	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>2</sup>	2 <sup>4</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>
J U	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>0</sup>	2 <sup>1</sup>
K L	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>
P	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
S	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
T	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>
V	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
Y	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>4</sup>	1 <sup>5</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0	0 <sup>6</sup>	0 <sup>5</sup>	0 <sup>7</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>
Z	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>2</sup>	2 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>0</sup>	2 <sup>1</sup>

**Lower Case To Lower Case**  
 Spacing Chart 6 Inch Series "C & D"

SERIES	SECOND LETTER															
	a c d e		b h i k l		f w		j		s t		v y		x		z	
	g o q	m n p r u														
a d h g i j	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>2</sup>	2 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>
l m n q u																
b f k o p s	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
c e	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
r	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>3</sup>	0 <sup>3</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>
t z	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
v y	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>
w	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>
x	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>

**Number To Number**  
 Spacing Chart 8 Inch Series "C & D"

SERIES	SECOND NUMBER																			
	0		1		2		3		4		5		6		7		8		9	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
0 9	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>
1	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>
2 3 4	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>		
5	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
6	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
7	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>5</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>
8	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>

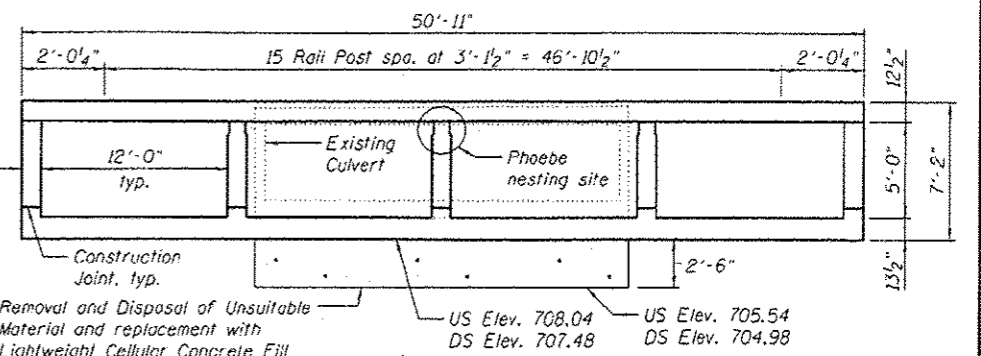
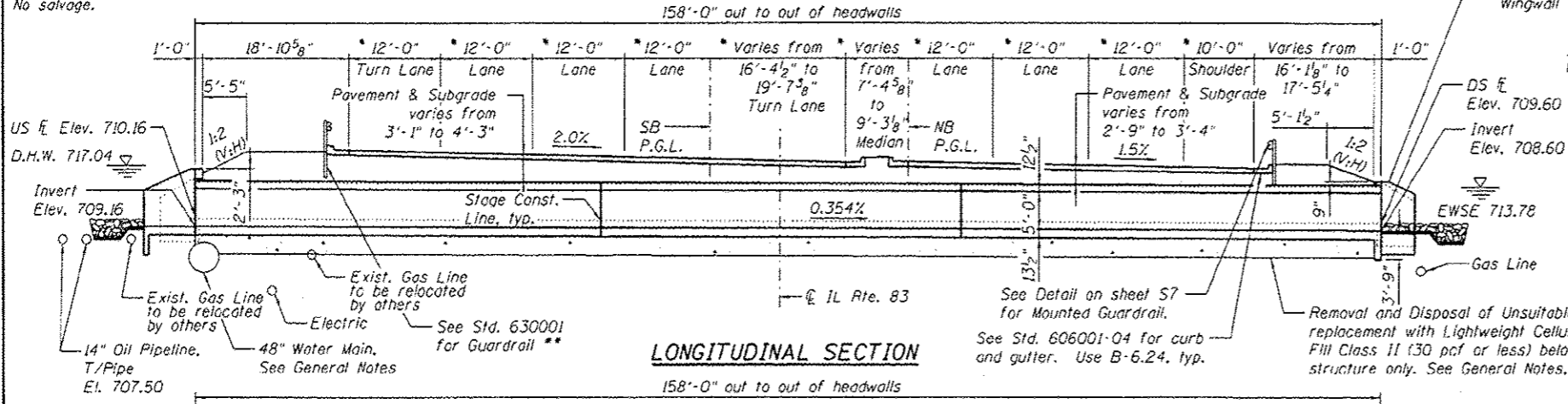
LETTERS	UPPER AND LOWER CASE LETTER WIDTHS						
	6 INCH UPPER CASE LETTERS		8 INCH UPPER CASE LETTERS		LETTERS	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES			SERIES	
	C	D	C	D		C	D
A	3 <sup>6</sup>	5 <sup>0</sup>	5 <sup>0</sup>	6 <sup>5</sup>	a	3 <sup>5</sup>	4 <sup>2</sup>
B	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	b	3 <sup>5</sup>	4 <sup>2</sup>
C	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	c	3 <sup>5</sup>	4 <sup>1</sup>
D	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	d	3 <sup>5</sup>	4 <sup>2</sup>
E	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	e	3 <sup>5</sup>	4 <sup>2</sup>
F	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	f	2 <sup>3</sup>	2 <sup>6</sup>
G	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	g	3 <sup>5</sup>	4 <sup>2</sup>
H	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	h	3 <sup>5</sup>	4 <sup>2</sup>
I	0 <sup>7</sup>	0 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	i	1 <sup>1</sup>	1 <sup>1</sup>
J	3 <sup>0</sup>	3 <sup>6</sup>	4 <sup>0</sup>	5 <sup>0</sup>	j	2 <sup>0</sup>	2 <sup>2</sup>
K	3 <sup>2</sup>	4 <sup>1</sup>	4 <sup>3</sup>	5 <sup>4</sup>	k	3 <sup>5</sup>	4 <sup>2</sup>
L	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	l	1 <sup>1</sup>	1 <sup>1</sup>
M	3 <sup>7</sup>	4 <sup>5</sup>	5 <sup>1</sup>	6 <sup>1</sup>	m	6 <sup>0</sup>	7 <sup>0</sup>
N	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	n	3 <sup>5</sup>	4 <sup>2</sup>
O	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>	o	3 <sup>6</sup>	4 <sup>3</sup>
P	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	p	3 <sup>5</sup>	4 <sup>2</sup>
Q	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>	q	3 <sup>5</sup>	4 <sup>2</sup>
R	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	r	2 <sup>6</sup>	3 <sup>2</sup>
S	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	s	3 <sup>6</sup>	4 <sup>2</sup>
T	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	t	2 <sup>7</sup>	3 <sup>2</sup>
U	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	u	3 <sup>5</sup>	4 <sup>2</sup>
V	3 <sup>5</sup>	4 <sup>4</sup>	4 <sup>7</sup>	6 <sup>0</sup>	v	4 <sup>2</sup>	4 <sup>7</sup>
W	4 <sup>4</sup>	5 <sup>2</sup>	6 <sup>0</sup>	7 <sup>0</sup>	w	5 <sup>5</sup>	6 <sup>4</sup>
X	3 <sup>4</sup>	4 <sup>0</sup>	4 <sup>5</sup>	5 <sup>3</sup>	x	4 <sup>4</sup>	5 <sup>1</sup>
Y	3 <sup>6</sup>	5 <sup>0</sup>	5 <sup>0</sup>	6 <sup>6</sup>	y	4 <sup>6</sup>	5 <sup>3</sup>
Z	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	z	3 <sup>6</sup>	4 <sup>3</sup>

NUMBER	6 INCH SERIES				8 INCH SERIES			
	C		D		C		D	
	C	D	C	D	C	D	C	D
1	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>				
2	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>				
3	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>				
4	3 <sup>5</sup>	4 <sup>3</sup>	4 <sup>7</sup>	5 <sup>7</sup>				
5	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>				
6	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>				
7	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>					

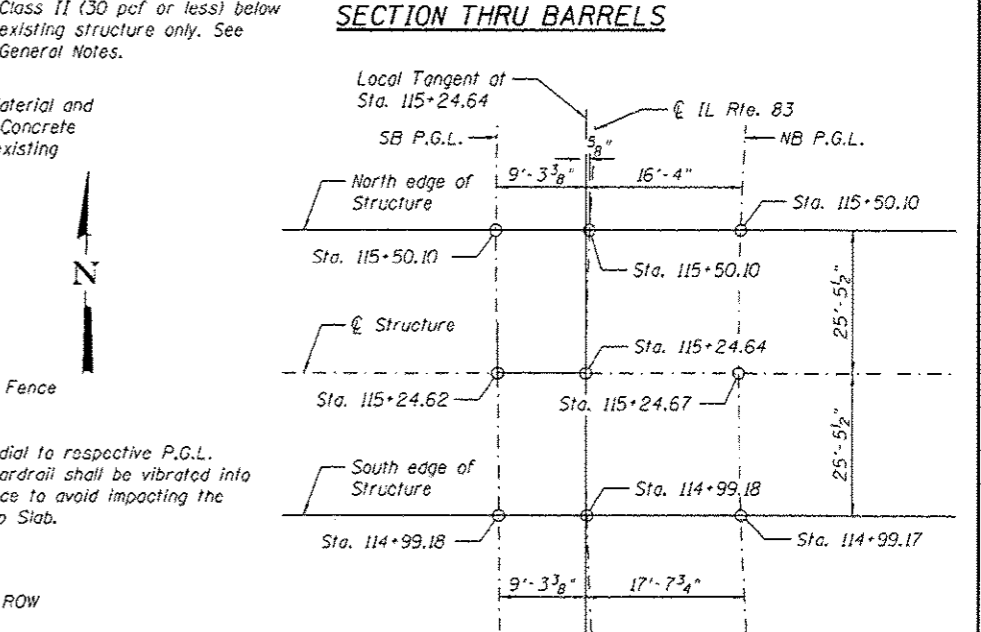
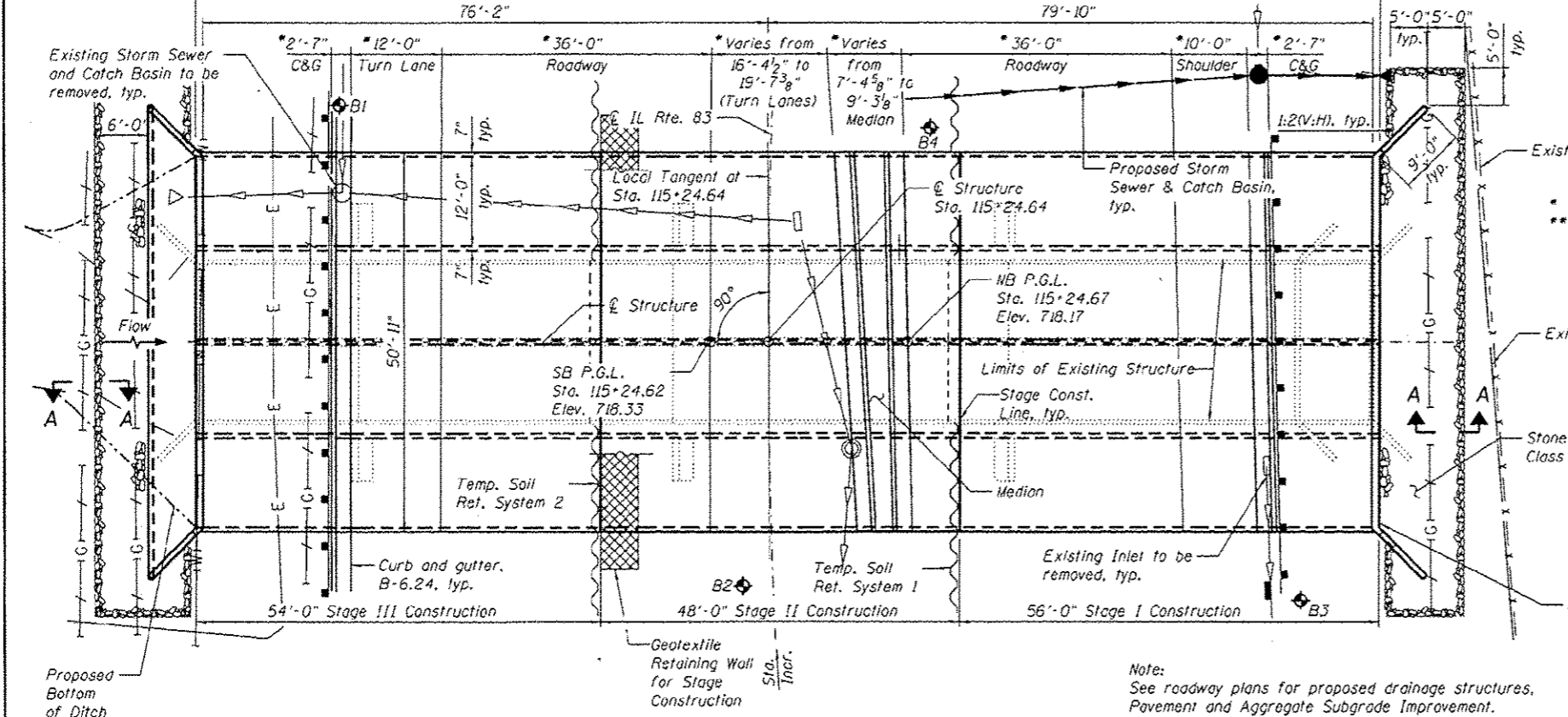
Bench Mark #27: Chiseled " " on the top of curb west side of Rte. 83 Sta. 116+59.76, Offset 59.93 Lt., Elev. 720.11.

Existing Structure: SN 022-0167 built in 1931, Section 544-1. A double barrel concrete box culvert with two 10'x4' cells having a total length of 45'-8" and no skew. Extended in 1935, 1962, and 1991 to a total length of 158'-4". The existing structure will be removed and replaced. Traffic to be maintained using staged construction.

No salvage.



SECTION THRU BARRELS



OFFSET SKETCH

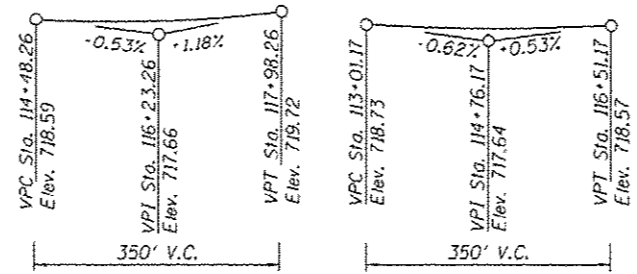
**WATERWAY INFORMATION**

Drainage Area = 1.26 Sq. mi. Exist. Low Grade Elev. 716.94 @ Exist. Sta. 115+03.83  
 Prop. Low Grade Elev. 717.45 @ Prop. Sta. 114+91.80

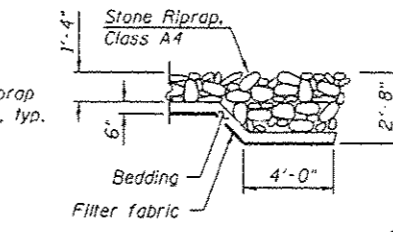
Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft.		H.W.E.		Headwater El.		
			Exist.	Prop.	Exist.	Prop.	Exist.	Prop.	
Design	10	352	120	192	716.00	0.21	0.03	716.21	716.03
Base	50	619	120	192	717.04	0.54	0.00	717.58	717.04
Max. Calc.	100	721	120	192	717.44	0.74	0.00	718.18	717.44
	500	985	120	192	718.52	1.18	0.26	719.70	718.78

10-Year Velocity through Existing Structure = 3.0 fps  
 10-Year Velocity through Proposed Structure = 1.41 fps

2-year peak flow = 94 cfs  
 2-year peak elevation = 715.48  
 2-year bypass waterway opening = 39.80 sq. ft.

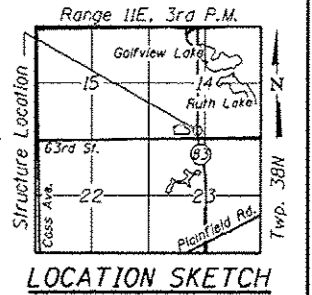
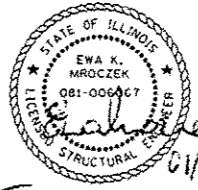


PROFILE GRADE (Along SB P.G.L. IL Rte. 83) and PROFILE GRADE (Along NB P.G.L. IL Rte. 83)



SECTION A-A

**APPROVED**  
 For Structural Adequacy Only  
 Collins Engineers, Inc.  
 EWA K. MROCEK  
 NO. 081-006067  
 EXP.: 11/30/2016



LOCATION SKETCH

**DESIGN STRESSES**  
 FIELD UNITS  
 f'c = 3,500 psi  
 fy = 60,000 psi (Reinforcement)

**DESIGN SPECIFICATIONS**  
 2014 AASHTO Bridge Design Specifications, 7th Edition

**LOADING HL-93**  
 Allow 50#/sq. ft. for future wearing surface.

**DESIGN FILL HEIGHT**  
 Design earth cover = varies from 2'-9" to 4'-3"

**GENERAL PLAN AND ELEVATION**  
**ILLINOIS ROUTE 83 OVER**  
**MARION HILLS DITCH**  
**FAP 344 - SECTION (544 & 544-1) TS&N 13**  
**DUPAGE COUNTY**  
**STA. 115+24.64**  
**S.N. 022-0563**

**INDEX OF SHEETS**

S1 General Plan & Elevation  
 S2 General Notes, Index of Sheets, and Total Bill of Materials  
 S3 Stage Construction Details  
 S4 Temporary Soil Retention System  
 S5-7 Culvert Details  
 S8 Bar Splicer Assembly and Mechanical Splicer Details  
 S9-10 Soil Boring Logs

**GENERAL NOTES:**

Reinforcement bars designated (E) shall be epoxy coated.  
 Precast alternate is not allowed.  
 All exposed concrete edges shall be chamfered 3/4" unless otherwise noted.  
 The limits and quantities of Removal of Unsuitable Material and replacement with Lightweight Cellular Concrete Fill shown are based on load balancing calculations and should not be modified for variable subsurface conditions encountered in the field.  
 It shall be the responsibility of the Contractor to divert flow during construction in order to keep construction areas free of water. The method of water diversion shall be subject to the approval of the Engineer and shall be included in the cost of Concrete Box Culverts.  
 The Contractor is allowed to divert the water during construction through the existing culvert located south of the construction area.  
 The exact elevation of the 48"  $\phi$  Water Main on the west side and the gas line on the east side is unknown. The Contractor shall locate all utilities before excavation and protect them during construction. The cost shall be included in the cost of Concrete Box Culverts. See Special Provisions for more requirements.  
 Structure Excavation shall be included in the cost of Concrete Box Culverts.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Stone Riprap, Class A4	Sq. Yd.	144
Filter Fabric	Sq. Yd.	144
Removal of Existing Structures	Each	1
Removal and Disposal of Unsuitable Material for Structures	Cu. Yd.	323
Reinforcement Bars, Epoxy Coated	Pound	150,230
Bar Splicers	Each	502
Name Plates	Each	1
Concrete Box Culverts	Cu. Yd.	767.2
Steel Plate Beam Guardrail, Attached to Structures	Foot	51
Geotextile Retaining Wall	Sq. Ft.	152
Temporary Soil Retention System	Sq. Ft.	1,051
Lightweight Cellular Concrete Fill	Cu. Yd.	323

STATION 115+24.64  
 BUILT 201\_ BY  
 STATE OF ILLINOIS  
 F.A.P. 344  
 SEC (544 & 544-1) TS&N (13)  
 LOADING HL-93  
 STRUCTURE NO. 022-0563

**NAME PLATE**  
 See Std. 515001

**CURVE DATA**

(Prop. Curve NB-C2)  
 P.I. Sta. = 116+96.07  
 $\Delta = 4^\circ 27' 20''$  (Rt)  
 D = 0° 58' 17"  
 R = 5,899.00  
 T = 229.48'  
 L = 458.74'  
 E = 4.46'  
 S.E. = 0.015'/ft  
 P.C. Sta. = 114+66.59  
 P.T. Sta. = 119+25.33

**CURVE DATA**

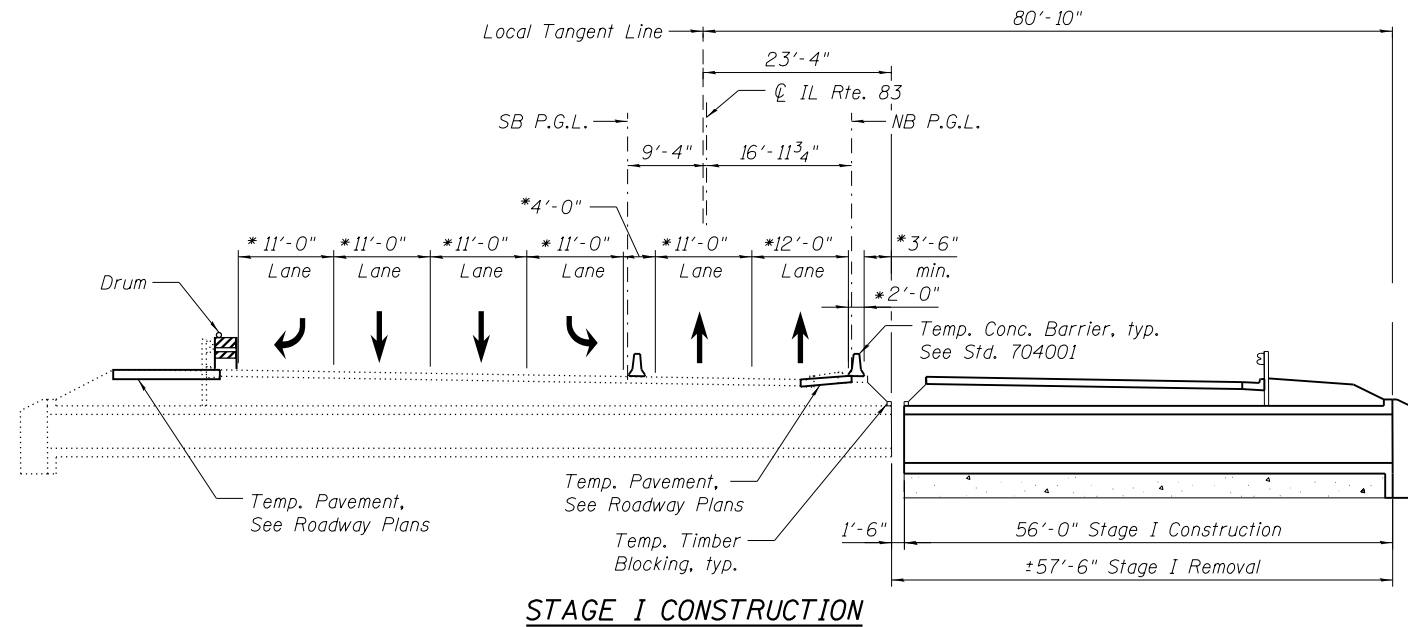
(Prop. Curve SB-C1)  
 P.I. Sta. = 110+58.83  
 $\Delta = 13^\circ 15' 59''$  (Rt)  
 D = 0° 53' 55"  
 R = 6,375.59  
 T = 741.43'  
 L = 1,476.22'  
 E = 42.97'  
 S.E. = 0.02'/ft  
 P.C. Sta. = 103+17.41  
 P.T. Sta. = 117+93.63

**CURVE DATA**

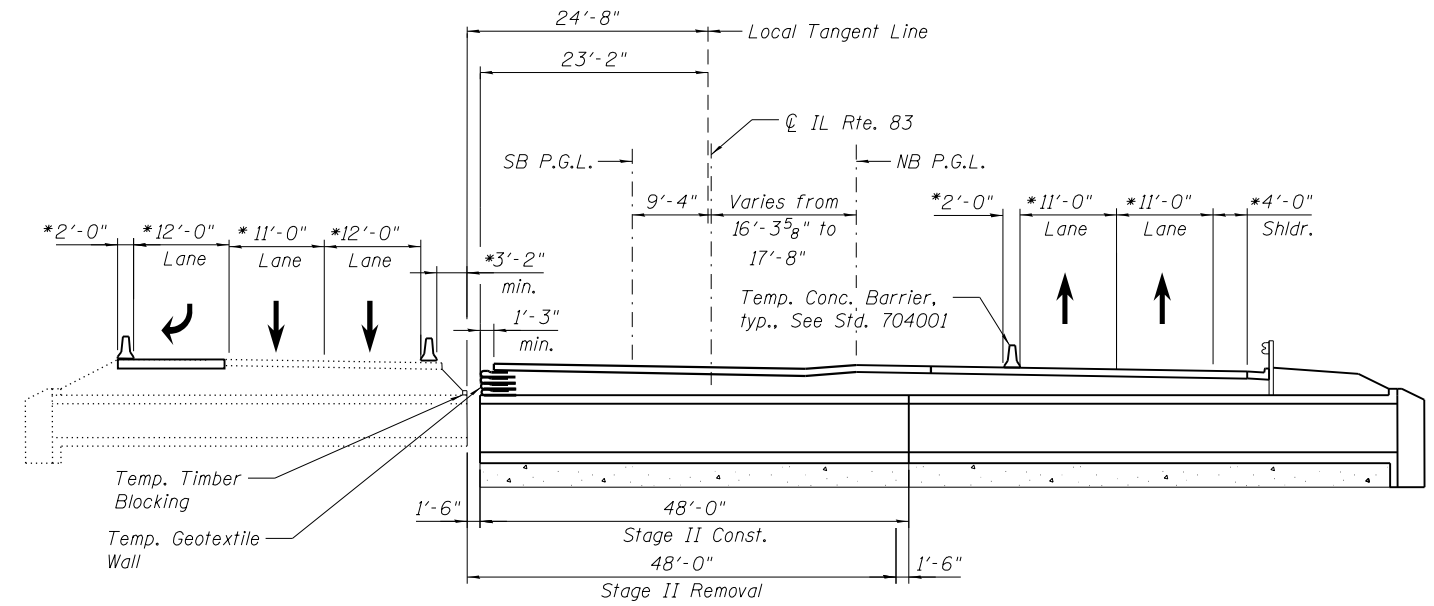
(Exist. Curve E. IL 83-1)  
 P.I. Sta. = 110+59.52  
 $\Delta = 13^\circ 15' 59''$  (Rt)  
 D = 0° 54' 00"  
 R = 6,366.26  
 T = 740.34'  
 L = 1,474.06'  
 E = 42.90'  
 P.C. Sta. = 103+19.18  
 P.T. Sta. = 117+93.24

USER NAME =	DESIGNED - AMS	REVISED
	CHECKED - EKM	REVISED
PLOT SCALE =	DRAWN - DR	REVISED
PLOT DATE =	CHECKED - AMS	REVISED

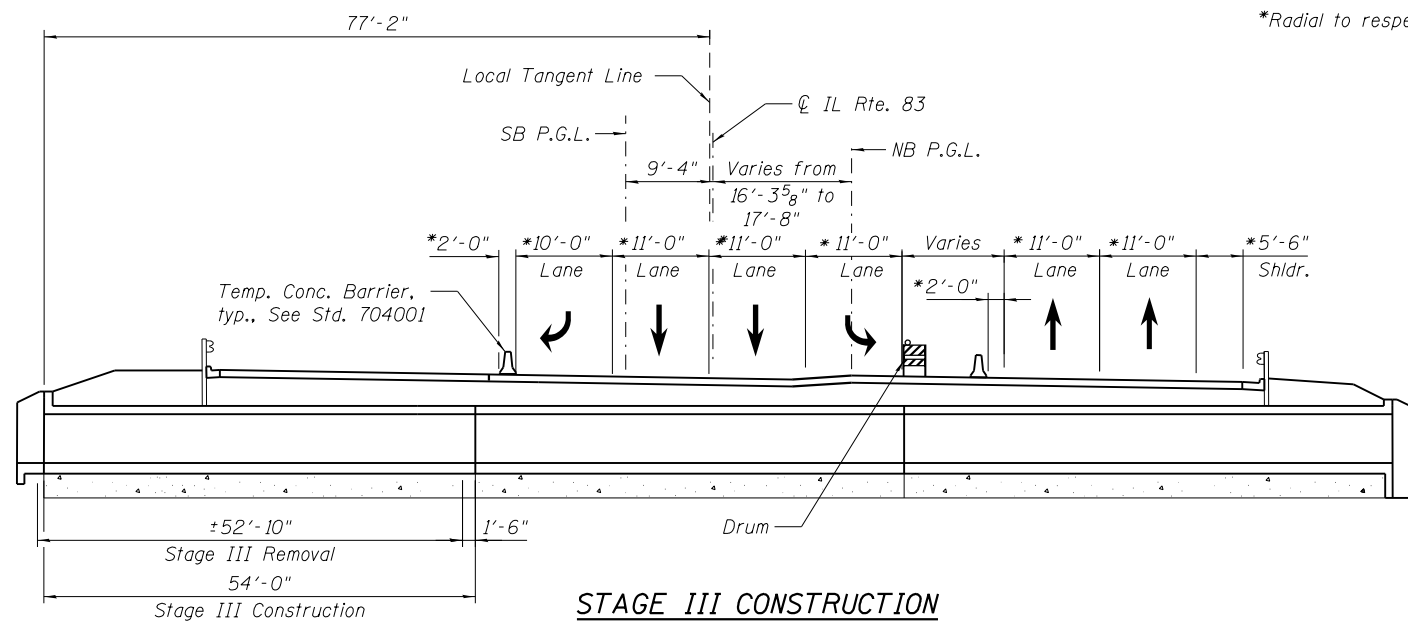
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-1) TS&N (13)	DUPAGE	129	93
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				



**STAGE I CONSTRUCTION**



**STAGE II CONSTRUCTION**

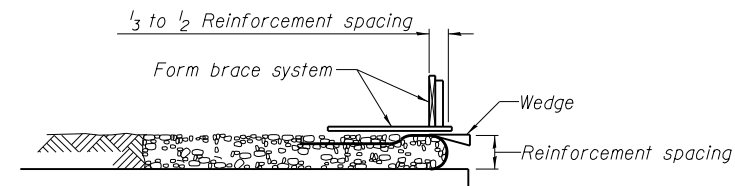


**STAGE III CONSTRUCTION**

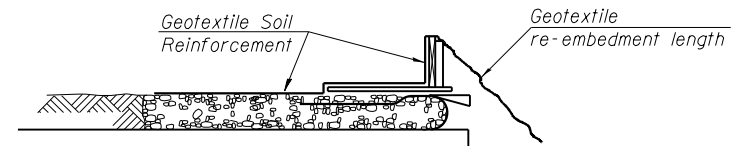
Notes:  
All dimensions measured along  $\phi$  culvert unless noted otherwise.

Median to be installed after Stage III. See Roadway Plans for additional sub-stages and more details.

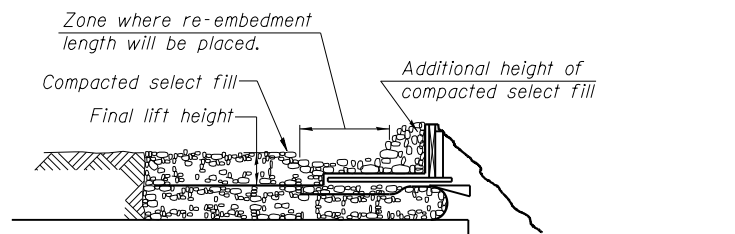
The geotextile soil reinforcement shall have a minimum allowable tensile strength ( $T_{min}$ ) of 100 lb./in. as determined by the procedure described in the Special Provision. The computations supporting the determination of ( $T_{min}$ ) shall be submitted to the engineer for approval.



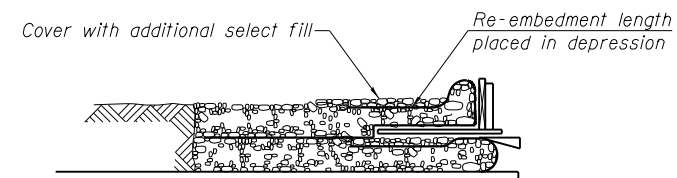
1. Place form brace system on completed reinforcement level; back from the finished fabric face a distance of  $\frac{1}{3}$  to  $\frac{1}{2}$  the geotextile reinforcement spacing.



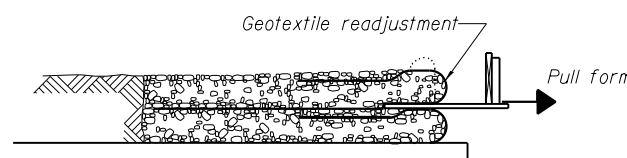
2. Position fabric so that the required geotextile re-embedment length extends over the top of the form brace and the design reinforcement width is placed with no slack against the previous level.



3. Compact select fill material in lifts to final lift height, create ( $\pm 3''$ ) depression in zone where re-embedment length will be located and place additional height of compacted select fill against form brace.



4. Fold geotextile re-embedment length back over form brace into zone where depression was made in select fill and place additional select fill ( $\pm 3''$ ) to embed geotextile and bring to final lift height.



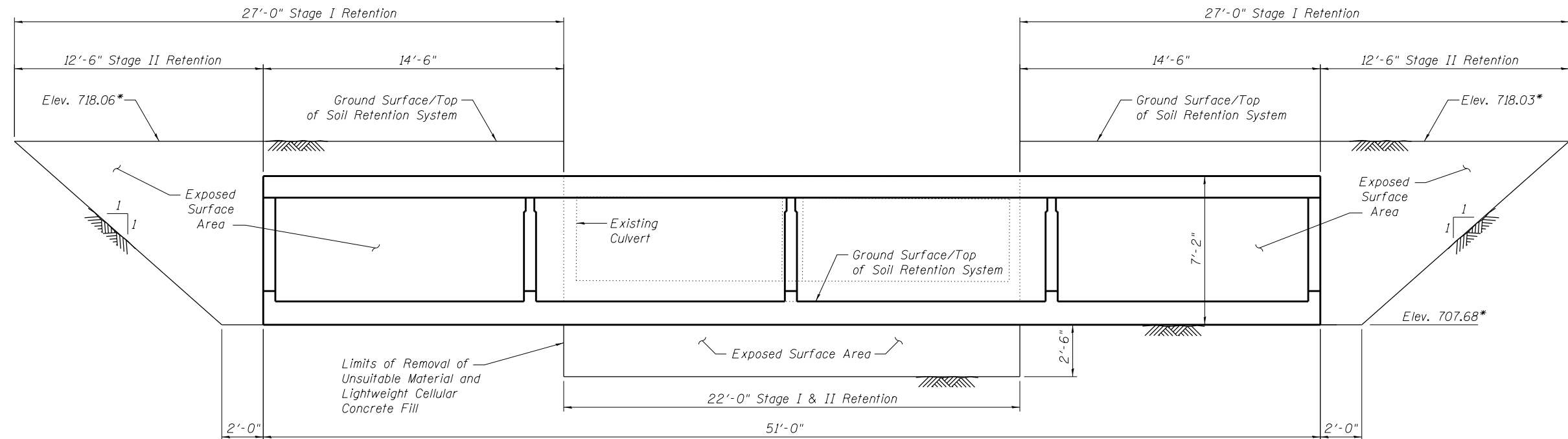
5. Pull form brace outward allowing geotextile face to slightly readjust to form tight round face level with plan reinforcement spacing.

**GEOTEXTILE WALL CONSTRUCTION SEQUENCE**

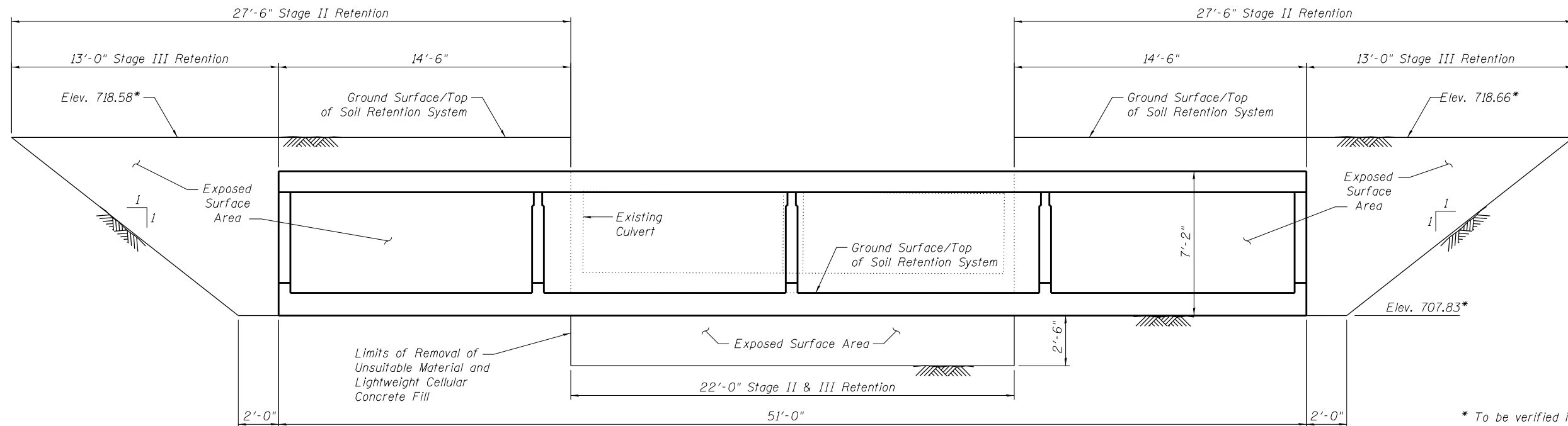
USER NAME =	DESIGNED - AMS	REVISED
	CHECKED - EKM	REVISED
PLOT SCALE =	DRAWN - DR	REVISED
PLOT DATE =	CHECKED - AMS	REVISED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-I) TS&N 13	DUPAGE	129	94
CONTRACT NO. 60W55				





**TEMPORARY SOIL RETENTION SYSTEM 1**  
(Looking East)



**TEMPORARY SOIL RETENTION SYSTEM 2**  
(Looking East)

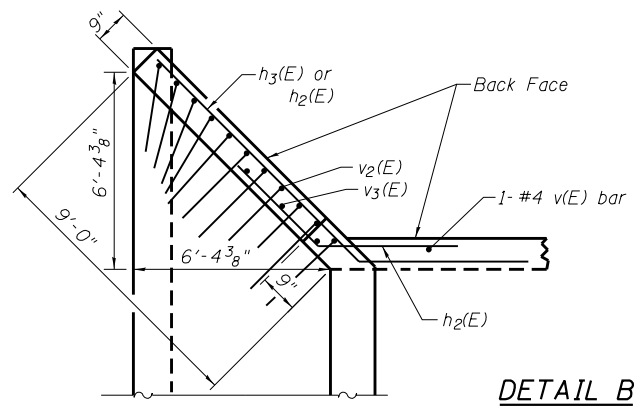
\* To be verified in field by the Engineer.

**Note:**

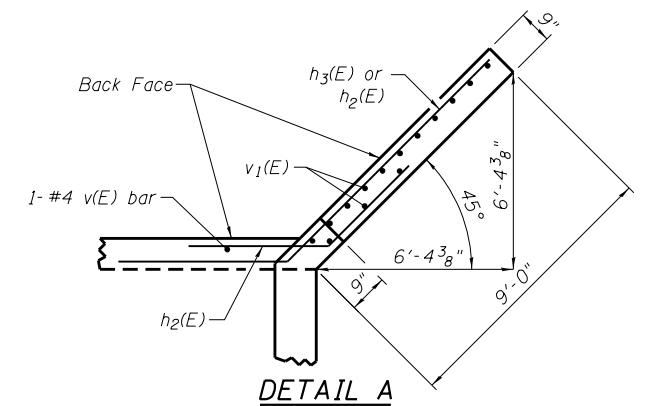
A cantilevered sheet piling design does not appear feasible and additional members or other retention systems may be necessary. The Contractor shall submit a temporary soil retention system design including plan details and calculations for review and acceptance by the Engineer.

USER NAME =	DESIGNED - AMS	REVISED
	CHECKED - EKM	REVISED
PLOT SCALE =	DRAWN - DR	REVISED
PLOT DATE =	CHECKED - AMS	REVISED

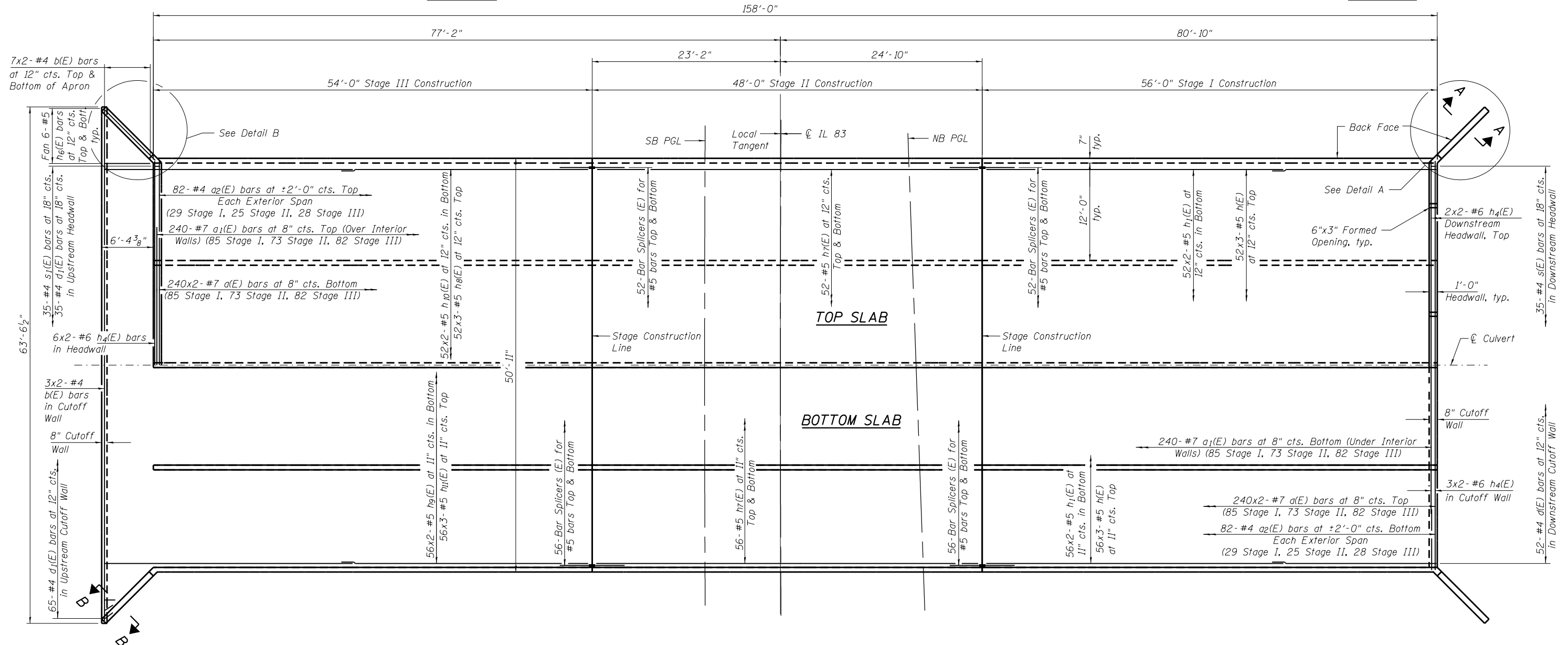
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-I) TS&N 13	DUPAGE	129	95
CONTRACT NO. 60W55				



DETAIL B



DETAIL A



PLAN

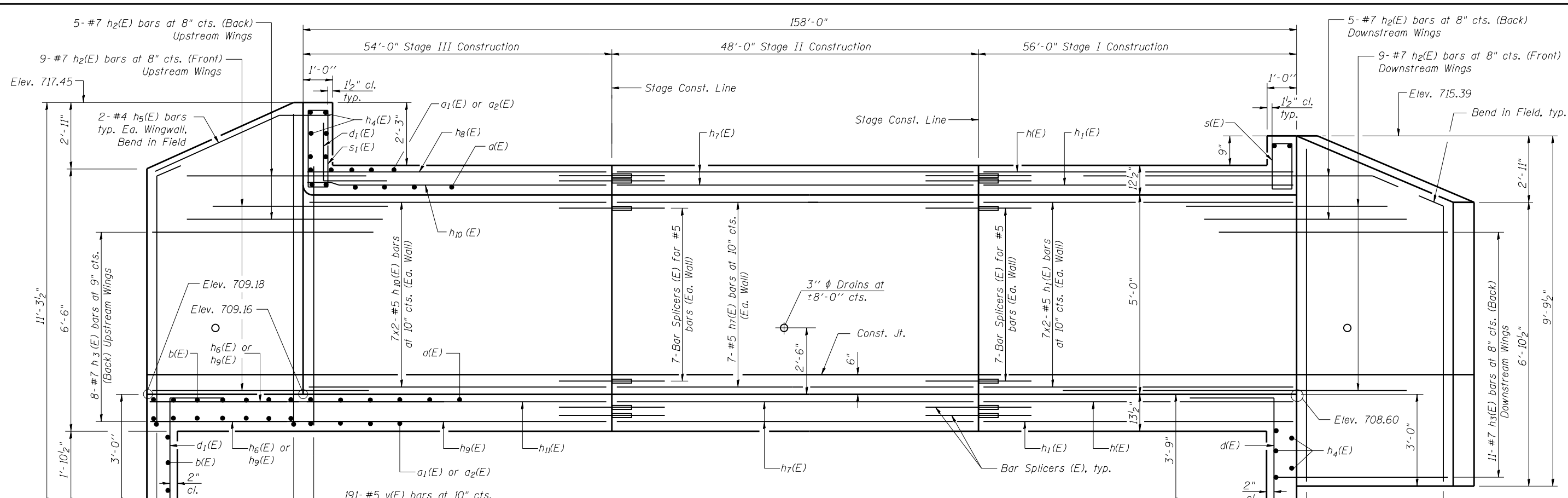
Notes:  
 A distance of half the length of the wingwall but not less than six feet of the barrel shall be poured monolithically with the wingwalls.  
 See sheet S7 for sections A-A and B-B, bar details and Bill of Materials.  
 See sheet S8 for bar splicer details.  
 Bars indicated thus 52x2- #5 etc. indicates 52 lines of bars with 2 lengths per line.

**MINIMUM BAR LAP**

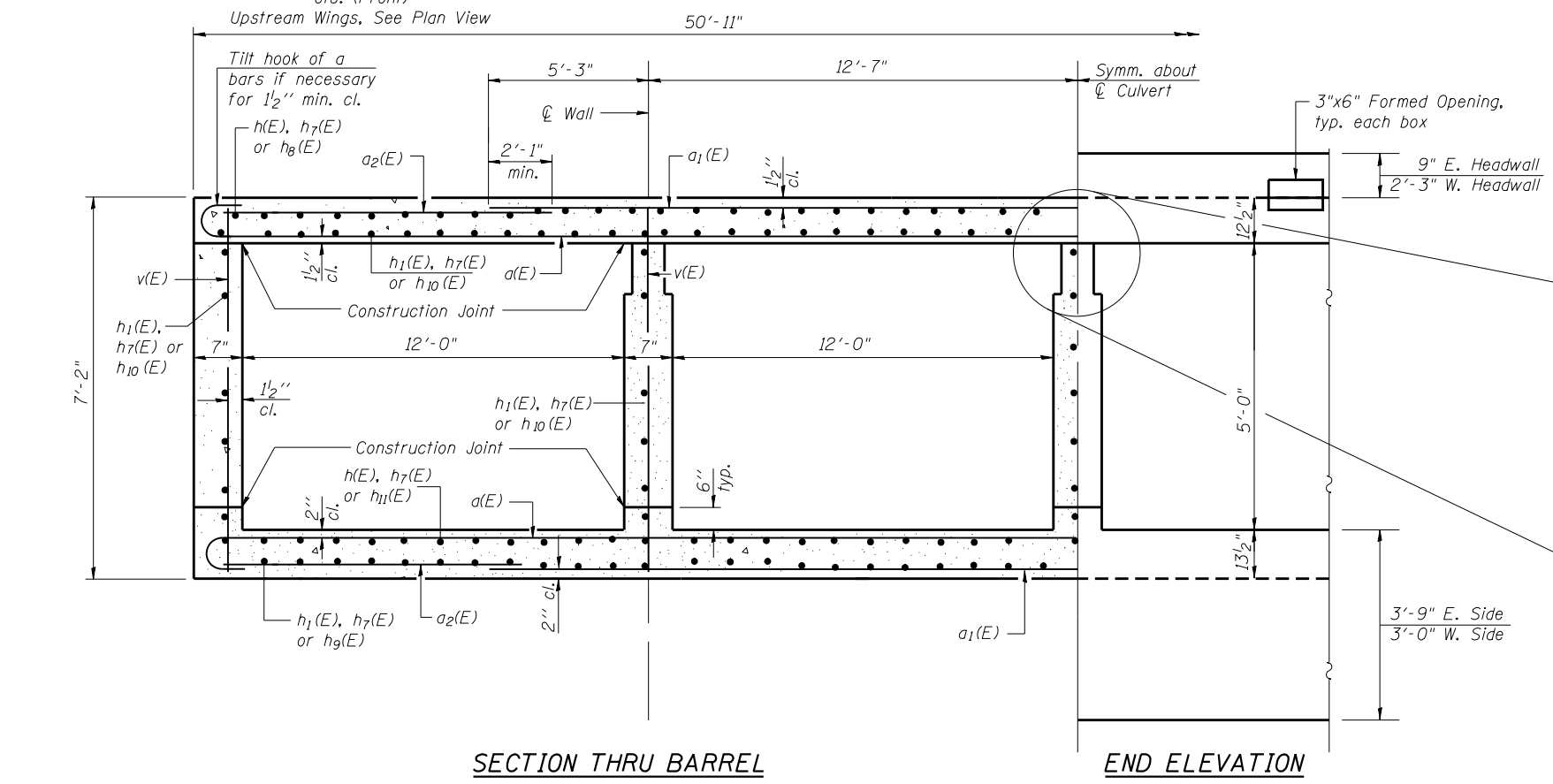
- #4 bar = 2'-7"
- #5 bar = 3'-3"
- #6 bar = 3'-10"
- #7 bar = 5'-2"

USER NAME =	DESIGNED - AMS	REVISED
	CHECKED - EKM	REVISED
PLOT SCALE =	DRAWN - DR	REVISED
PLOT DATE =	CHECKED - AMS	REVISED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-I) TS&N 13	DUPAGE	129	96
				CONTRACT NO. 60W55
ILLINOIS FED. AID PROJECT				



**LONGITUDINAL SECTION**  
(Looking North)

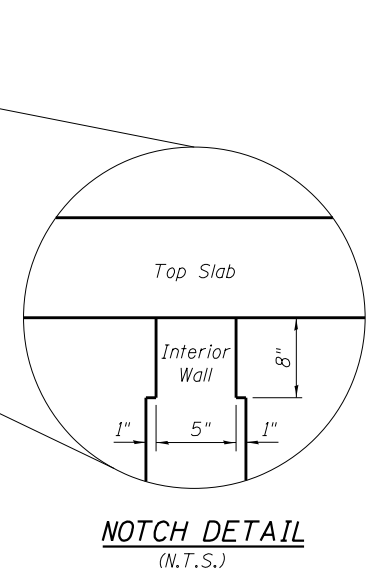


**SECTION THRU BARREL**

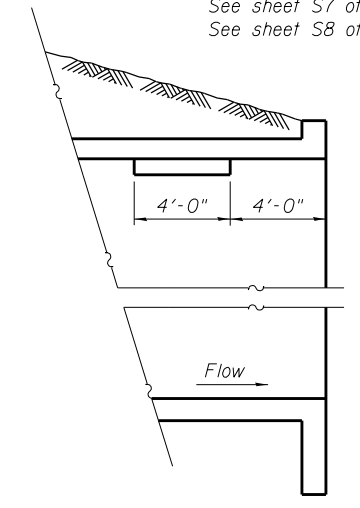
**END ELEVATION**

**MIN. BAR LAP**  
#5 bar = 3'-8"

**Notes:**  
A distance of half the length of the wingwall but not less than six feet of the barrel shall be poured monolithically with the wingwalls. Bars indicated thus 7x2-#5 etc. indicates 7 lines of bars with 2 lengths per line. See sheet S7 of S10 for bar details and Bill of Material. See sheet S8 of S10 for bar splicer details.



**NOTCH DETAIL**  
(N.T.S.)

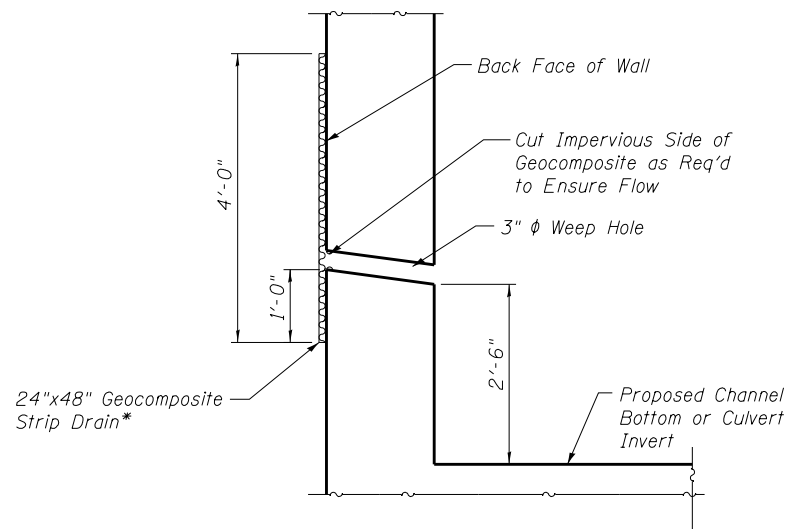


**LONGITUDINAL SECTION**  
(N.T.S.)

- Phoebe Nesting Site Notes:**
1. Notch formed by rough-finished board attached to and removed with formwork.
  2. Cost of providing Phoebe Nesting area included in pay item for Concrete Box Culverts.

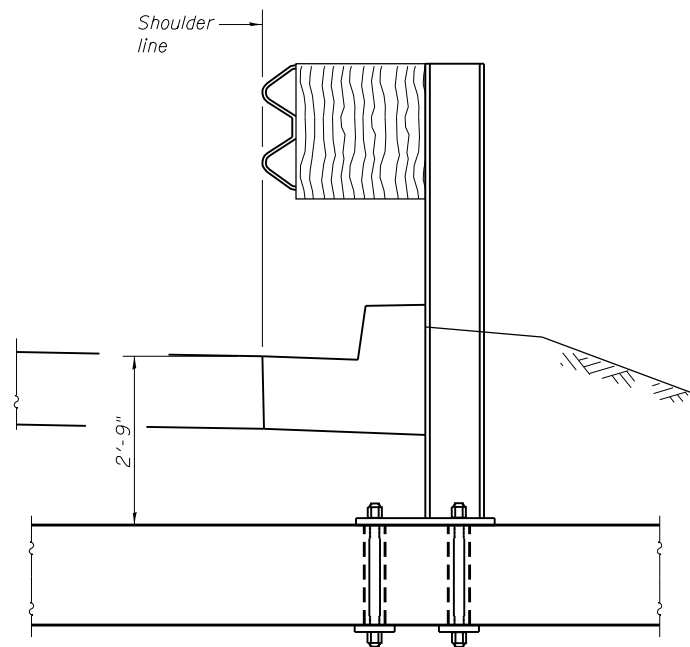
USER NAME =	DESIGNED - AMS	REVISED
PLOT SCALE =	CHECKED - EKM	REVISED
PLOT DATE =	DRAWN - DR	REVISED
	CHECKED - AMS	REVISED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-I) TS&N 13	DUPAGE	129	97
CONTRACT NO. 60W55				
ILLINOIS FED. AID PROJECT				



**WEEPHOLE DRAIN DETAIL**

\* Cost included with Concrete Box Culverts.



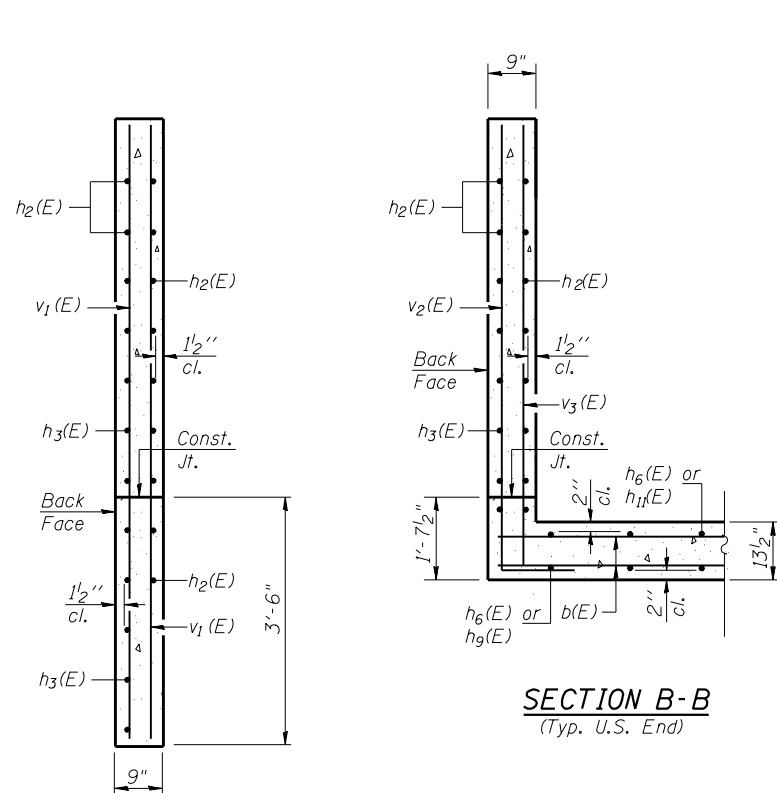
**GUARDRAIL MOUNTED ON EXISTING CULVERTS**

**CASE IV MOUNTED ON SLAB**

(East Guardrail only. See Standard 630101.)

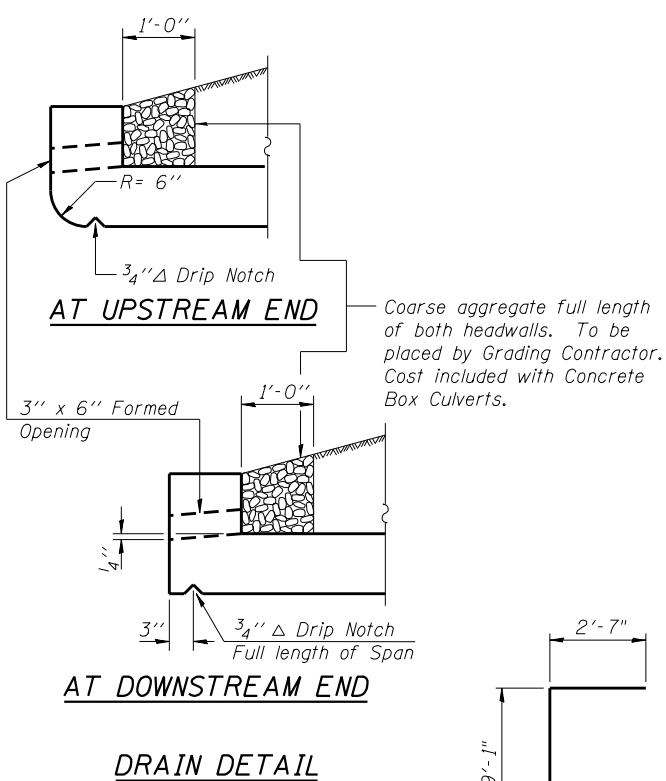
**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
d(E)	960	#7	28'-11"	C
d <sub>1</sub> (E)	480	#7	38'-8"	—
d <sub>2</sub> (E)	328	#4	9'-5"	—
b(E)	34	#4	33'-1"	—
d(E)	52	#4	5'-2"	—
d <sub>1</sub> (E)	100	#4	4'-5"	—
h(E)	324	#5	20'-10"	—
h <sub>1</sub> (E)	286	#5	29'-11"	—
h <sub>2</sub> (E)	56	#7	8'-0"	—
h <sub>3</sub> (E)	42	#7	12'-0"	—
h <sub>4</sub> (E)	22	#6	27'-5"	—
h <sub>5</sub> (E)	8	#4	10'-6"	—
h <sub>6</sub> (E)	24	#5	6'-1"	—
h <sub>7</sub> (E)	251	#5	47'-8"	—
h <sub>8</sub> (E)	156	#5	20'-2"	—
h <sub>9</sub> (E)	112	#5	31'-10"	—
h <sub>10</sub> (E)	174	#5	28'-11"	—
h <sub>11</sub> (E)	168	#5	22'-4"	—
s(E)	35	#4	5'-3"	□
s <sub>1</sub> (E)	35	#4	8'-1"	□
v(E)	959	#5	6'-10"	—
v <sub>1</sub> (E)	26	#4	9'-6"	—
v <sub>2</sub> (E)	24	#4	11'-8"	—
v <sub>3</sub> (E)	6	#4	9'-1"	—
Concrete Box Culverts			Cu. Yd.	767.2
Reinforcement Bars, Epoxy Coated			Pound	150,230
Steel Plate Beam Guardrail, Attached to Structures.			Foot	51

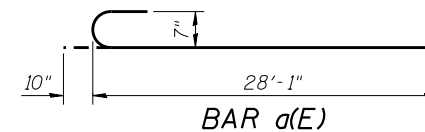


**SECTION A-A**  
(Typ. D.S. End)

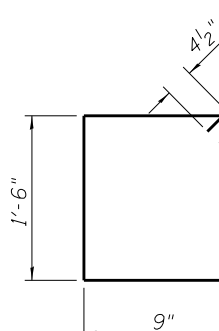
**SECTION B-B**  
(Typ. U.S. End)



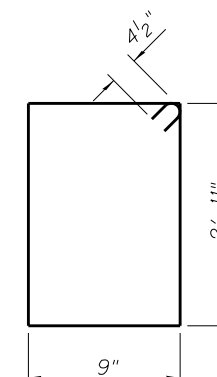
**DRAIN DETAIL**



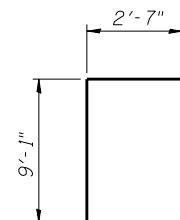
**BAR d(E)**



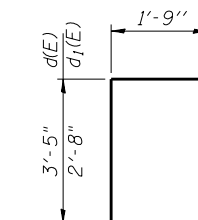
**BAR s(E)**



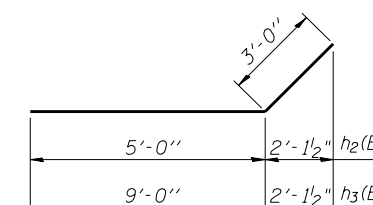
**BAR s<sub>1</sub>(E)**



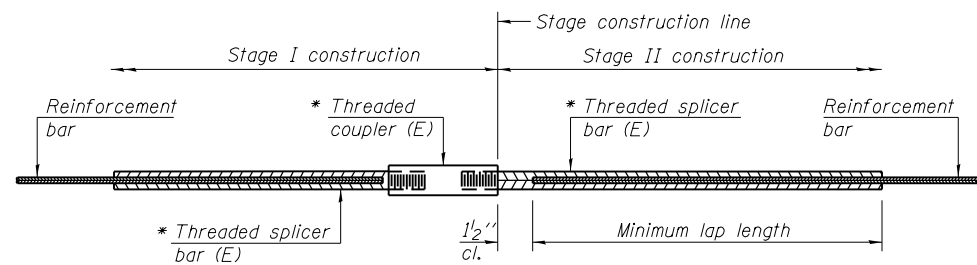
**BAR v<sub>2</sub>(E)**



**BAR d(E) & d<sub>1</sub>(E)**



**BARS h<sub>2</sub>(E) & h<sub>3</sub>(E)**



**STANDARD BAR SPLICER ASSEMBLY**

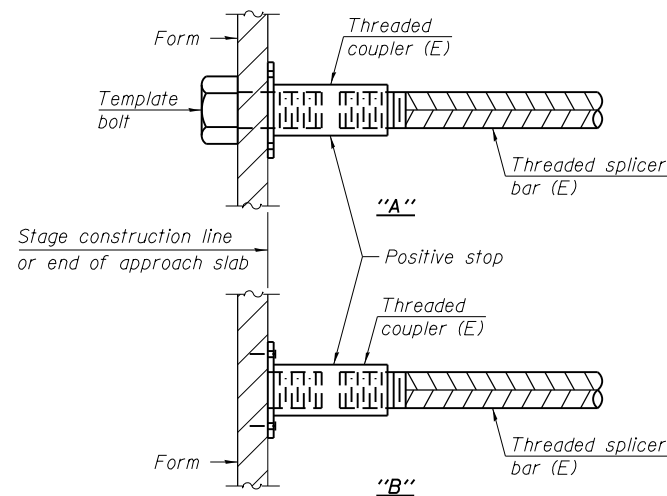
Minimum Lap Lengths						
Bar size to be spliced	Table 1	Table 2	Table 3	Table 4	Table 5	Table 6
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-7"	2'-11"
5	1'-9"	2'-5"	2'-7"	2'-11"	3'-3"	3'-8"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-10"	4'-5"
7	2'-9"	3'-10"	4'-2"	4'-8"	5'-2"	5'-10"
8	3'-8"	5'-1"	5'-5"	6'-2"	6'-9"	7'-8"
9	4'-7"	6'-5"	6'-10"	7'-9"	8'-7"	9'-8"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Class C
- Table 6: Epoxy bar, Top bar top, Class C

Threaded splicer bar length = min. lap length + 1/2" + thread length

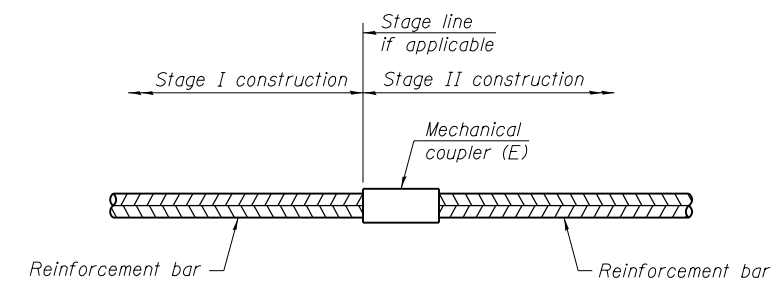
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
Top Slab	#5	208	Table 3
Walls	#5	70	Table 4
Bottom Slab	#5	224	Table 3



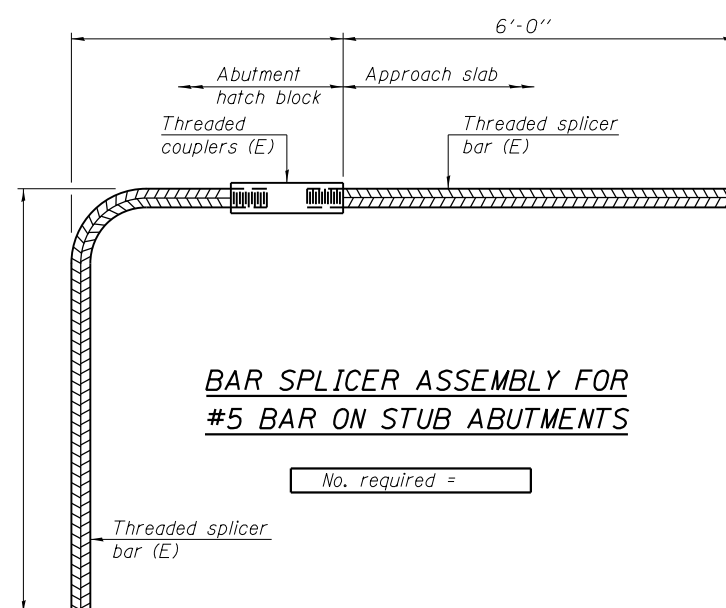
**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

No. required =

**NOTES**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.  
 All reinforcement shall be lapped and tied to the splicer bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.  
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

8-31-12

**COLLINS ENGINEERS**  
 133 N. Booker Dr., Suite 900  
 Chicago, IL 60606  
 Tel: (312) 704-9300  
 Fax: (312) 704-9320  
 www.collinsengr.com  
 ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-000993

USER NAME =	DESIGNED - AMS	REVISED
	CHECKED - EKM	REVISED
PLOT SCALE =	DRAWN - DR	REVISED
PLOT DATE =	CHECKED - AMS	REVISED

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
 STRUCTURE NO. 022-0563**

SHEET NO. 58 OF 510 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-I) TS&N 13	DUPAGE	129	99
CONTRACT NO. 60W55				

ILLINOIS FED. AID PROJECT



# BORING LOG B-1

wangeng@wangeng.com  
1145 N Main Street  
Lombard, IL 60148  
Telephone: 630 953-9928  
Fax: 630 953-9938

WEI Job No.: 486-13-03

Client: **Collins Engineers, Inc.**  
Project: **IL Route 83 over Marion Hills Ditch**  
Location: **SW 1/4, Sec. 14, T38N, R11E of the 3rd P.M**

Datum: NGVD  
Elevation: 719.00 ft  
North: 1860954.01 ft  
East: 1089656.99 ft  
Station: 115+54.3  
Offset: 51.7 LT

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
718.3	9-inch thick CONCRETE --PAVEMENT--														
	Medium stiff to very stiff, dark gray CLAY LOAM, trace gravel --FILL--	1	X	1	3 4 5	0.49 B	13				X	11	4 5 7	2.62 B	20
		2	X	2	2 4 4	2.79 B	24				X	12	4 6 6	1.31 B	17
713.5	Stiff, black CLAY LOAM --BURIED TOPSOIL--	3	X	3	2 2 2	1.67 B	40				X	13	5 6 6	3.03 B	16
711.0	Medium stiff, brown and gray SILTY CLAY to CLAY LOAM	4	X	4	1 1 2	0.82 B	35				X	14	3 5 8	2.21 B	23
		5	X	5	1 2 2	0.90 B	23				X	15	3 6 10	NP	20
		6	X	6	1 2 3	0.98 B	20				X	16	3 6 10	NP	20
703.5	Loose, gray SILT	7	X	7	2 3 4	NP	18	677.0	Medium dense, gray SILT						
701.0	Loose, dark gray LOAM	8	X	8	2 4 4	NP	12	674.0	Boring terminated at 45.00 ft	45	X	15	3 6 10	NP	20
697.5	Stiff to very stiff, gray SILTY CLAY to CLAY LOAM	9	X	9	1 2 3	1.15 B	20				X	17	2 3 3	1.72 B	18
		10	X	10	2 3 3	1.72 B	18				X	18	2 3 3	1.72 B	18

### GENERAL NOTES

Begin Drilling: **08-27-2009** Complete Drilling: **08-27-2009**  
 Drilling Contractor: **WTS** Drill Rig: **Mobile B-57 TMR**  
 Driller: **B&J** Logger: **A. Kurnia** Checked by: **S. Sugiarto**  
 Drilling Method: **4.25 IDA HSA; Boring Backfilled upon Completion**

### WATER LEVEL DATA

While Drilling: **11.00 ft**  
 At Completion of Drilling: **41.00 ft**  
 Time After Drilling: **NA**  
 Depth to Water: **NA**

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.



# BORING LOG B-2

wangeng@wangeng.com  
1145 N Main Street  
Lombard, IL 60148  
Telephone: 630 953-9928  
Fax: 630 953-9938

WEI Job No.: 486-13-03

Client: **Collins Engineers, Inc.**  
Project: **IL Route 83 over Marion Hills Ditch**  
Location: **SW 1/4, Sec. 14, T38N, R11E of the 3rd P.M**

Datum: NGVD  
Elevation: 718.00 ft  
North: 1860895.21 ft  
East: 1089711.52 ft  
Station: 114+92.7  
Offset: 0.7 LT

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
717.3	8-inch thick CONCRETE --PAVEMENT--														
	Stiff, brown and gray CLAY LOAM, trace gravel --FILL--	1	X	1	4 3 4	1.56 B	11				X	11	3 5 5	NP	19
		2	X	2	4 3 7	1.64 B	23				X	12	4 4 7	NP	15
712.5	Dense, brown and gray GRAVELLY SAND --FILL--	3	X	3	8 15 18	NP	6	686.0	Stiff to very stiff, gray SILTY CLAY						
710.0	Medium dense, gray CRUSHED STONE --FILL--	4	X	4	11 8 6	NP	11				X	13	4 5 6	1.23 B	21
		5	X	5	5 7 8	NP	13				X	14	4 5 8	2.21 B	19
705.0	Soft, black ORGANIC SILTY CLAY to PEAT --Organic Content=16%--	6	X	6	3 2 3	0.41 B	110				X	14	4 5 8	2.21 B	19
703.0	Very soft, gray SILTY CLAY	7	X	7	< 0.25 P		28				X	15	6 8 13	2.62 B	18
701.5	Medium stiff, gray SILTY CLAY (Qp= 0.75 tsf)	8	X	8		0.15 B	33				X	15	6 8 13	2.62 B	18
700.0	Very soft, gray SILTY LOAM --A-7-5(14)-- --LL=63%, PL=43%-- --CC=0.266, CR=0.041-- --OCR=1.25--	9	X	9	0 0 2	NP	19	673.0	Boring terminated at 45.00 ft	45	X	15	6 8 13	2.62 B	18
698.0	Very loose to medium dense, gray SILT	10	X	10	2 4 4	NP	23				X	16	2 4 4	NP	23

### GENERAL NOTES

Begin Drilling: **08-28-2009** Complete Drilling: **08-28-2009**  
 Drilling Contractor: **WTS** Drill Rig: **Mobile B-57 TMR**  
 Driller: **B&J** Logger: **A. Kurnia** Checked by: **S. Sugiarto**  
 Drilling Method: **4.25 IDA HSA; Boring Backfilled upon Completion**

### WATER LEVEL DATA

While Drilling: **10.00 ft**  
 At Completion of Drilling: **7.00 ft**  
 Time After Drilling: **NA**  
 Depth to Water: **NA**

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.



USER NAME =	DESIGNED - AMS	REVISED
PLOT SCALE =	CHECKED - EKM	REVISED
PLOT DATE	DRAWN - DR	REVISED
	CHECKED - AMS	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BORING LOGS I  
STRUCTURE NO. 022-0563

SHEET NO. 59 OF 510 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	(544 & 544-I) TS&N 13	DUPAGE	129	100
CONTRACT NO. 60W55				

ILLINOIS FED. AID PROJECT