003

03-06-2020 LETTING ITEM 003

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

∗ 21 + 4 = 25 TOTAL SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS ∗	SHEET NO.
1300	1346-BR (11)	COOK	21	1
		ILLINOIS	CONTRACT NO. 60R72	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED
IN THE VILLAGE OF GLENVIEW

TRAFFIC DATA

CENTRAL ROAD

2018 ADT 15300
POSTED SPEED LIMIT 45 MPH

PROPOSED HIGHWAY PLANS

FAU ROUTE 1300 /CENTRAL ROAD
OVER DES PLAINES RIVER
SECTION 1346-BR (11)
PROJECT: STP-ZD7S(713)
BRIDGE DECK REPAIR, BRIDGE DECK OVERLAY
COOK COUNTY

C-91-205-12

D-91-205-12

LOCATION OF SECTION INDICATED THUS:-

R 12 E

N

T-42 N

T-41 N

MAINE AND NORTHFIELD TOWNSHIP

GROSS AND NET LENGTH = 219 FT. = 0.041 MILE

SN 016-0551
CENTRAL RD OVER
DES PLAINES RIVER

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED December 12, 2019
[signature]
REGIONAL ENGINEER
Jan 31, 2020
[signature]
ENGINEER OF DESIGN AND ENVIRONMENT
Jan 31, 2020
[signature]
DIRECTOR OF HIGHWAYS, PROJECT IMPLEMENTATION

PROJECT ENGINEER: MIDY, J. ALAIN (847) 221-3056
PROJECT MANAGER: AQUEEL, FAWAD (847) 705-4247

CONTRACT NO. 60R72

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REV. - MS

INDEX OF SHEETS

INDEX OF HIGHWAY STANDARDS

Standard No.	Description
420001-09	Pavement Joints
701006-05	Off Road Operations, 2L, 2W, 15' to 24" from Pavement Edge
701011-04	Off Road Moving Operations, 2L, 2W, Day Only
701301-04	Lane Closure, 2L, 2W,Short Time Operations
701306-04	Lane Closure, 2L, 2W, Slow Moving Operations Day Only, for Speeds > 45 MPH
701311-03	Lane Closure, 2L, 2W, Moving Operations-Day Only
701321-18	Lane Closure, 2L, 2W,Bridge Repair with Barrier
701326-04	Lane Closure, 2L, 2W,Pavement Widening for Speeds > 45 MPH
701901-08	Traffic Control Devices
704001-08	Temporary Concrete Barrier

GENERAL NOTES

1. These plans have been prepared from notes received from IDOT Bridge Engineers.
2. The Contractor shall coordinate construction activities with utility companies, and the Village of Glenview.
3. All damage to existing pavement markings or raised reflective pavement markers outside the removal line shown on the plans shall be replaced at no additional cost to the department.
4. Before begining any work, the contractor shall retain and record for future reference, all existing pavement marking lines (and raised reflective pavement markers) in order that these locations can be re-established for striping. Exact locations of all pavement markings shall be as directed by the engineer.
5. The Contractor shall contact the District One Traffic Control Supervisor at Kalpana.Kannan-Hosadurga@illinois.gov a minimum of 72 hours in advance of begining work.
6. The Resident Engineer shall contact the Area Traffic Field Engineer, Walter Czarny via Email "Walter.Czarny@illinois.gov" a minimum of two (2) weeks prior to the placement of permanent pavement markings.
7. The contractor shall be required to provide access to abutting property at all times during the construction of this project.
8. The Contractor will not be allowed to set up a yard or field office on State property without written permission from the Department.
9. Do not scale these plans for construction purposes.
10. Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work. However, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.
11. During construction operations, loose material deposits that obstruct the flow of water in draining the area shall be removed before the end of each work day. At the conclusion of construction operations, all drainage structures (new and existing) shall be free from all dirt and debris. This work will not be paid for separately but shall be included in the pay item for Concrete Removal.
12. The quantity for Hot-Mix Asphalt Surface Course, Mix "D", N70 has been prepared assuming $1\frac{3}{4}$" inch thick hot-mix asphalt overlays.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QMP
MIXTURE TYPE	AIR VOIDS @ Ndes	
APPROACH SLABS & BUTT JOINTS		
Hot-Mix Asphalt Surface Course, Mix "D", IL-9.5, N70: $1\frac{3}{4}$"	4% @ 70 Gyr.	QC/QA
TEMPORARY PAVEMENT, 10" (Non-Interstate)		
Hot-Mix Asphalt Surface Course, Mix "D", IL-9.5 , N70: 2"	4% @ 70 Gyr.	QC/QA
Hot Mix Asphalt Binder Course, IL-9.5, N70: 8"	4% @ 70 Gyr.	QC/QA
QMP Designation: Quality Control/Quality Assurance (QC/QA);		

The unit weight used to calculate all Hot-Mix Asphalt surface mixtures is 112 lbs/sq yd/in.

The "AC Type" for all Polymerized HMA mixes shall be SBS/SBR PG 76-22" and for Non-Polymerized HMA the "AC Type" shall be "PG64-22" unless modified by the District One Special Provisions.

For use of recycled material, see Special Provisions.

Quality Management Program (QMP) identifies the particular Quality Control Specification that applies to the HMA mixture.

The Contractor has the option of using PCC Temporary Pavement:
PCC Temporary Pavement shall consist of Class PV Concrete meeting the requirements of Art. 1020 of the Standard Specifications; typically 10" thick.

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		DRAWN - MAP	REVISED -
	PLOT SCALE = 100.0000 ' / in.	CHECKED - MGR	REVISED -
	PLOT DATE = 12/12/2019	DATE - SEPTEMBER, 2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, INDEX OF SHEETS AND HIGHWAY STANDARDS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346-BR (11)	COOK	21	2
		CONTRACT NO. 60R72		
	ILLINOIS FED. AID PROJECT			

SUMMARY OF QUANTITIES			URBAN	CONSTRUCTION TYPE CODE 0013				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0013				
20200100	EARTH EXCAVATION	CU YD	3	3				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	439	439				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	1	1				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	207	207				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	65	65				
44000100	PAVEMENT REMOVAL	SQ YD	97	97				
50102400	CONCRETE REMOVAL	CU YD	6	6				
50300255	CONCRETE SUPERSTRUCTURE	CU YD	6	6				
50300260	BRIDGE DECK GROOVING	SQ YD	735	735				
50300300	PROTECTIVE COAT	SQ YD	785	785				
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	1180	1180				
50800515	BAR SPLICERS	EACH	8	8				
52000110	PREFORMED JOINT STRIP SEAL	FOOT	112	112				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6				
67100100	MOBILIZATION	L SUM	1	1				

SUMMARY OF QUANTITIES			URBAN	CONSTRUCTION TYPE CODE 0013				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0013				
70106700	TEMPORARY RUMBLE STRIPS	EACH	12	12				
70300904	PAVEMENT MARKING TAPE, TYPE IV 4"	FOOT	2220	2220				
70300924	PAVEMENT MARKING TAPE, TYPE IV 24"	FOOT	96	96				
70400100	TEMPORARY CONCRETE BARRIER	FOOT	312.5	312.5				
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	312.5	312.5				
70600241	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2				
70600341	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2				
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2004	2004				
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	1560	1560				
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	3564	3564				
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	8	8				
78100300	REPLACEMENT REFLECTOR	EACH	8	8				
78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	120	120				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	8	8				
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1	1				

* SPECIALTY ITEMS

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CENTRAL RD OVER DES PLAINES RIVER (SN 016-0551)
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. 1 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346-BR (11)	COOK	21	3

CONTRACT NO. 60R72

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

SUMMARY OF QUANTITIES

	CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 0013 80% FED 20% STATE 0013				
*	89000050	TEMPORARY BRIDGE TRAFFIC SIGNAL INSTALLATION	EACH	1	1				
*	X0326276	TEMPORARY LIGHTING FOR SINGLE LANE STAGING	L SUM	1	1				
	X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	330	330				
	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1				
	X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	932	932				
	Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	6	6				
	Z0006014	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/2 INCHES	SQ YD	752	752				
	Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	755	755				
	Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	334	334				
	Z0015550	DEBRIS REMOVAL	CU YD	60	60				
	Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	26	26				
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4				
	Z0041895	POLYMER CONCRETE	CU FT	5.4	5.4				
	Z0062456	TEMPORARY PAVEMENT	SQ YD	97	97				
*	Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1	1				

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					

✱ SPECIALTY ITEMS

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	PLOT DATE = 12/13/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CENTRAL RD OVER DES PLAINES RIVER (SN 016-0551)
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. 2 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346-BR (11)	COOK	21	3A

CONTRACT NO. 60R72

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

N

SCALE: 1" = 20'

DES PLAINES RIVER

US ROUTE 45

CENTRAL ROAD

℄ CENTRAL ROAD

BEGIN PAVING
STA 30+57

LIMIT OF APPROACH SLAB
STA 30+87

LIMIT OF APPROACH SLAB
STA 33+46

END PAVING
STA 33+76

EX. R.O.W.

EXIST. 27" SS

EXIST. 42" SS

30+00

35+00

33

10

10

33

EXIST. 10" WM

EXIST. 6" SAN FORCE MAIN

EXIST. 15" SS

EX. R.O.W.

EXISTING GUARDRAIL (TYP)

EXISTING RIGHT OF WAY

BACK OF W. ABUTMENT
STA 31+37

℄ PIER 1
STA 31+90

℄ PIER 2
STA 32+43

BACK OF E. ABUTMENT
STA 32+96

1066'

ROADWAY LEGEND:

HMA SURFACE REMOVAL-BUTT JOINT &
PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"

PAVEMENT MARKING LEGEND:

(1) 4" WHITE EDGE LINE

(2) 4" DOUBLE YELLOW CENTERLINE (11" C-C)

NOTES:

MODIFIED URETHANE PAVEMENT MARKING TO BE USED ON PCC PAVEMENT AND ON HMA SURFACES.

BUTT JOINT AND HMA TAPER SHALL BE CONSTRUCTED IN ACCORDANCE WITH DISTRICT DETAIL BD-32.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CENTRAL ROAD OVER DES PLAINES RIVER
ROADWAY PLAN

SCALE: | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346-BR (11)	COOK	21	4
		CONTRACT NO. 60R72		
		ILLINOIS FED. AID PROJECT		

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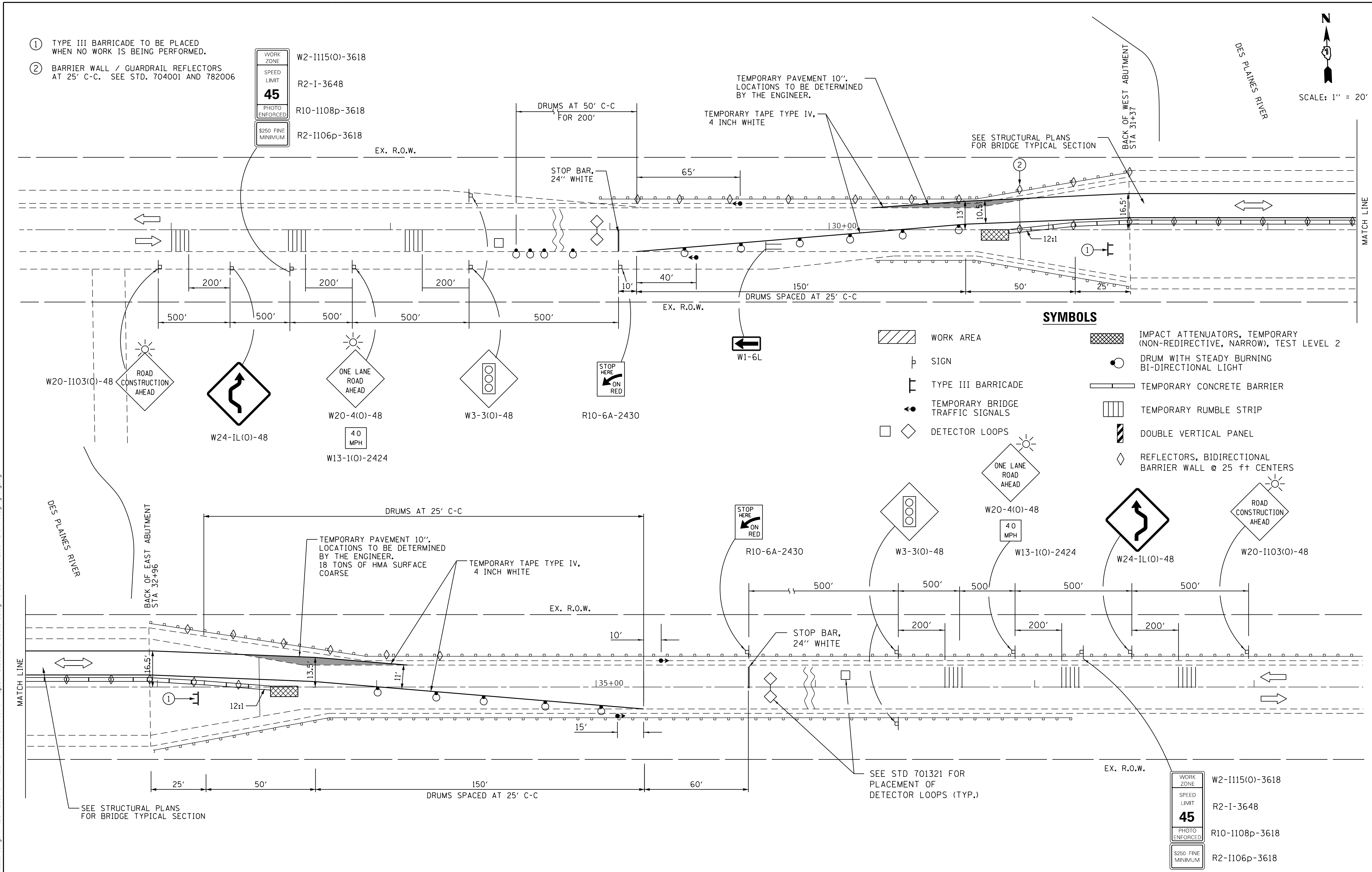

1 TYPE III BARRICADE TO BE PLACED WHEN NO WORK IS BEING PERFORMED.
2 BARRIER WALL / GUARDRAIL REFLECTORS AT 25' C-C. SEE STD. 704001 AND 782006
WORK ZONE
SPEED LIMIT 45
PHOTO ENFORCED
$250 FINE MINIMUM
W2-I115(0)-3618
R2-I-3648
R10-1108p-3618
R2-I106p-3618
EX. R.O.W.
DRUMS AT 50' C-C FOR 200'
STOP BAR, 24" WHITE
65'
TEMPORARY PAVEMENT 10". LOCATIONS TO BE DETERMINED BY THE ENGINEER.
TEMPORARY TAPE TYPE IV, 4 INCH WHITE
SEE STRUCTURAL PLANS FOR BRIDGE TYPICAL SECTION
BACK OF WEST ABUTMENT STA 31+37
DES PLAINES RIVER
SCALE: 1" = 20'
N
30+00
13'
10.5'
16.5'
12:1
MATCH LINE
200'
500'
10'
40'
150'
50'
25'
DRUMS SPACED AT 25' C-C
ROAD CONSTRUCTION AHEAD
W20-I103(0)-48
W24-IL(0)-48
ONE LANE ROAD AHEAD
W20-4(0)-48
40 MPH
W13-1(0)-2424
W3-3(0)-48
STOP HERE ON RED
R10-6A-2430
W1-6L
SYMBOLS
WORK AREA
SIGN
TYPE III BARRICADE
TEMPORARY BRIDGE TRAFFIC SIGNALS
DETECTOR LOOPS
IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE, NARROW), TEST LEVEL 2
DRUM WITH STEADY BURNING BI-DIRECTIONAL LIGHT
TEMPORARY CONCRETE BARRIER
TEMPORARY RUMBLE STRIP
DOUBLE VERTICAL PANEL
REFLECTORS, BIDIRECTIONAL BARRIER WALL @ 25 ft CENTERS
DRUMS AT 25' C-C
TEMPORARY PAVEMENT 10". LOCATIONS TO BE DETERMINED BY THE ENGINEER. 18 TONS OF HMA SURFACE COARSE
BACK OF EAST ABUTMENT STA 32+96
35+00
13.5'
11'
15'
60'
SEE STD 701321 FOR PLACEMENT OF DETECTOR LOOPS (TYP.)

FILE NAME =	USER NAME = rothjp	DESIGNED - MR	REVISED -
		DRAWN - MR	REVISED -
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	PLOT DATE = 12/13/2019	DATE - SEPTEMBER, 2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CENTRAL ROAD OVER DES PLAINES RIVER
MAINTENANCE OF TRAFFIC
STAGE I

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346-BR (II)	COOK	21	5

CONTRACT NO. 60R72

ILLINOIS FED. AID PROJECT

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① TYPE III BARRICADE TO BE PLACED WHEN NO WORK IS BEING PERFORMED.

② BARRIER WALL / GUARDRAIL REFLECTORS AT 25′ C-C. SEE STD. 704001 AND 635011

WORK ZONE
SPEED LIMIT 45
PHOTO ENFORCED
$250 FINE MINIMUM

W2-I115(0)-3618
R2-I-3648
R10-1108p-3618
R2-I106p-3618

EX. R.O.W.

DRUMS AT 50′ C-C FOR 200′

DRUMS AT 25′ C-C

TEMPORARY PAVEMENT 10″. LOCATIONS TO BE DETERMINED BY THE ENGINEER.

TEMPORARY TAPE TYPE IV, 4 INCH WHITE

STOP BAR, 24″ WHITE

65′

30+00

11′

12:1

BACK OF WEST ABUTMENT STA 31+37

DES PLAINES RIVER

N

SCALE: 1″ = 20′

SEE STRUCTURAL PLANS FOR TYPICAL SECTION

MATCH LINE

200′ 200′ 200′

500′ 500′ 500′ 500′ 500′

10′ 150′ 50′ 25′

DRUMS SPACED AT 25′ C-C

EX. R.O.W.

40′

ROAD CONSTRUCTION AHEAD
W20-I103(0)-48

W24-IL(0)-48

ONE LANE ROAD AHEAD
W20-4(0)-48

40 MPH

W3-3(0)-48

STOP HERE ON RED
R10-6A-2430

SYMBOLS

- WORK AREA
- SIGN
- TYPE III BARRICADE
- TEMPORARY BRIDGE TRAFFIC SIGNALS
- DETECTOR LOOPS
- IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE, NARROW), TEST LEVEL 2
- DRUM WITH STEADY BURNING BI-DIRECTIONAL LIGHT
- TEMPORARY CONCRETE BARRIER
- TEMPORARY RUMBLE STRIP
- DOUBLE VERTICAL PANEL
- REFLECTORS, BIDIRECTIONAL BARRIER WALL @ 25ft CENTERS

DES PLAINES RIVER

BACK OF EAST ABUTMENT STA 32+96

TEMPORARY PAVEMENT 10″. LOCATIONS TO BE DETERMINED BY THE ENGINEER.

TEMPORARY TAPE TYPE IV, 4 INCH WHITE

W1-6L

EX. R.O.W.

10′

MATCH LINE

35+00

11′

12:1

15′

SEE STRUCTURAL PLANS FOR TYPICAL SECTION

25′ 50′ 150′ 60′

DRUMS SPACED AT 25′ C-C

STOP HERE ON RED
R10-6A-2430

STOP BAR, 24″ WHITE

500′ 500′ 500′ 500′

200′ 200′ 200′

EX. R.O.W.

SEE STD 701321 FOR PLACEMENT OF DETECTOR LOOPS (TYP.)

W3-3(0)-48

ONE LANE ROAD AHEAD
W20-4(0)-48

40 MPH
W13-1(0)-2424

W24-IL(0)-48

ROAD CONSTRUCTION AHEAD
W20-I103(0)-48

WORK ZONE
SPEED LIMIT 45
PHOTO ENFORCED
$250 FINE MINIMUM

W2-I115(0)-3618
R2-I-3648
R10-1108p-3618
R2-I106p-3618

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	PLOT DATE = 12/13/2019	DATE - SEPTEMBER, 2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CENTRAL ROAD OVER DES PLAINES RIVER
MAINTENANCE OF TRAFFIC
STAGE II

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346-BR (11)	COOK	21	6
		CONTRACT NO. 60R72		
	ILLINOIS FED. AID PROJECT			

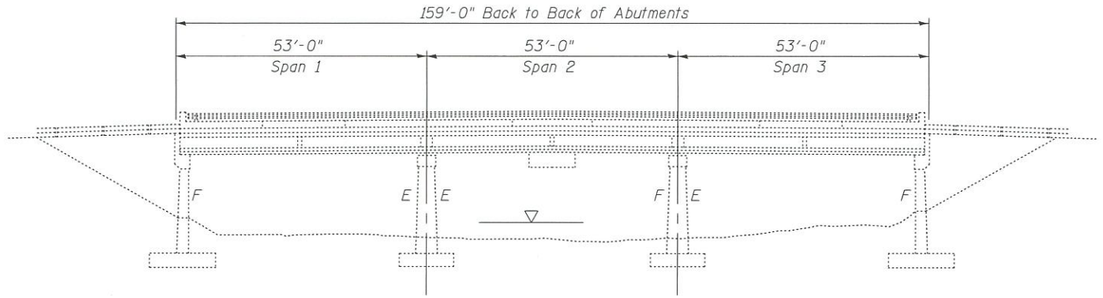
Bench Mark: Brass plug in NW Wingwall. Elevation 635.18.

Existing Structure: The existing structure consists of a four span precast prestressed superstructure with a reinforced concrete deck and substructure. The original Structure was constructed in 1929 and the superstructure was replaced in 1979 as part of Section 1977-123-BR. Stage construction will be utilized to maintain traffic during construction. One lane of traffic in each direction to be maintained using temporary traffic signals.

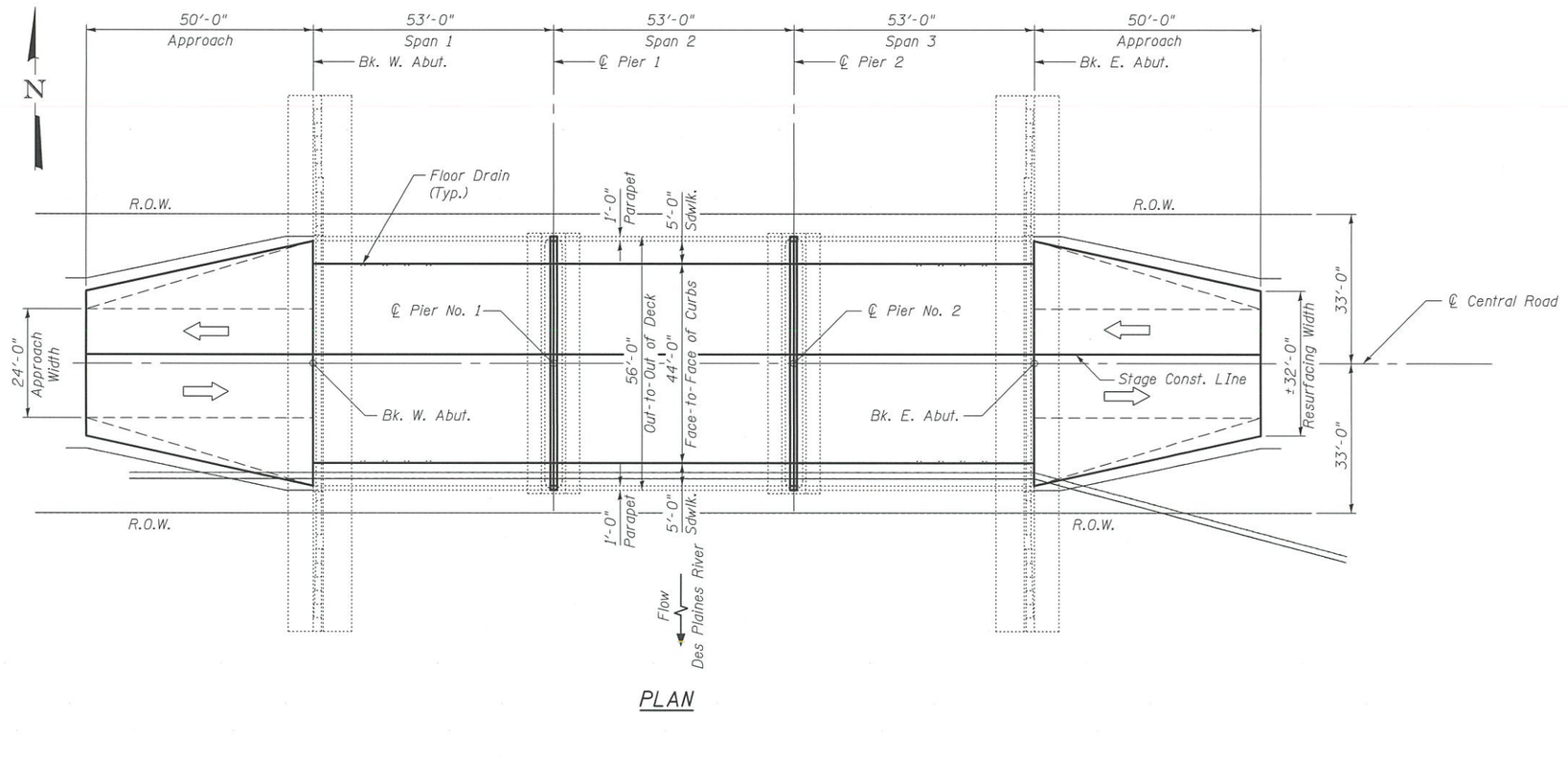
No Salvage.

SCOPE OF WORK

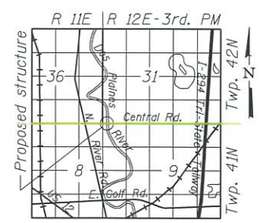
1. Bridge deck scarification.
2. Repair bridge deck.
3. Repair approach slabs.
4. Place polymer concrete nosing at joints at abutments.
5. Reconstruct deck joints at piers with preformed strip seal.
6. Place new overlay on bridge deck and approach slabs.
7. Remove debris at Pier 2.



ELEVATION



PLAN



LOCATION SKETCH

DAVID CARL PUZEY
081.005470
LICENSED STRUCTURAL ENGINEER OF
STATE OF ILLINOIS

EXPIRES 11-30-2020

DESIGNED -		EXAMINED		DATE - January 30, 2020	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
CHECKED - Adrian T. Halloway		PASSED	ENGINEER OF STRUCTURAL SERVICES	REVISED -	
DRAWN - Venkat Reddy				REVISED -	
CHECKED - CCC ATH			ENGINEER OF BRIDGES AND STRUCTURES		

GENERAL PLAN AND ELEVATION
FAP 1300 OVER DES PLAINES RIVER
SN 016 - 0551
SHEET NO. 1 OF 11 SHEETS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346 - BR (11)	COOK	21	7
		CONTRACT NO. 60R72		
	ILLINOIS FED. AID PROJECT			

TOTAL BILL OF MATERIAL

ITEM DESCRIPTION	UNIT	QUANTITY
Bituminous Materials (Prime Coat)	Pound	215
Hot-Mix Asphalt Surface Course Mix "D", N70	Ton	41
Protective Coat	Sq. Yd.	785
Concrete Removal	Cu. Yd.	6.0
Concrete Superstructure	Cu. Yd.	6.0
Reinforcement Bars, Epoxy Coated	Pound	1,180
Bar Splicers	Each	8
Preformed Joint Strip Seal	Foot	112.0
Approach Slab Repairs (Partial Depth)	Sq. Yd.	6
Structural Repair of Concrete (Depth Equall or Less Than, 5")	Sq. Ft.	334
Bridge Deck Latex Concrete Overlay, $2\frac{1}{2}$"	Sq. Yd.	752
Bridge Deck Scarification, $\frac{3}{4}$"	Sq. Yd.	755
Bridge Deck Grooving	Sq. Yd.	735
Polymer Concrete	Cu. Ft.	5.4
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	26.4
Debris Removal	Cu. Yd.	60

GENERAL NOTES:

Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Staged construction shall be utilized to maintain traffic during construction.

The Contractor shall exercise care during removal of existing joints to ensure that the slab, beams and diaphragms' integrity will not be detrimentally impacted. The Contractor shall repair any damage(s) to the slab, beams or diaphragms caused by his operation as directed by the Engineer at no additional cost to the Department.

Protective coat shall be applied to New concrete areas adjacent to transverse joint reconstruction, and concrete overlay areas.

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50°F.

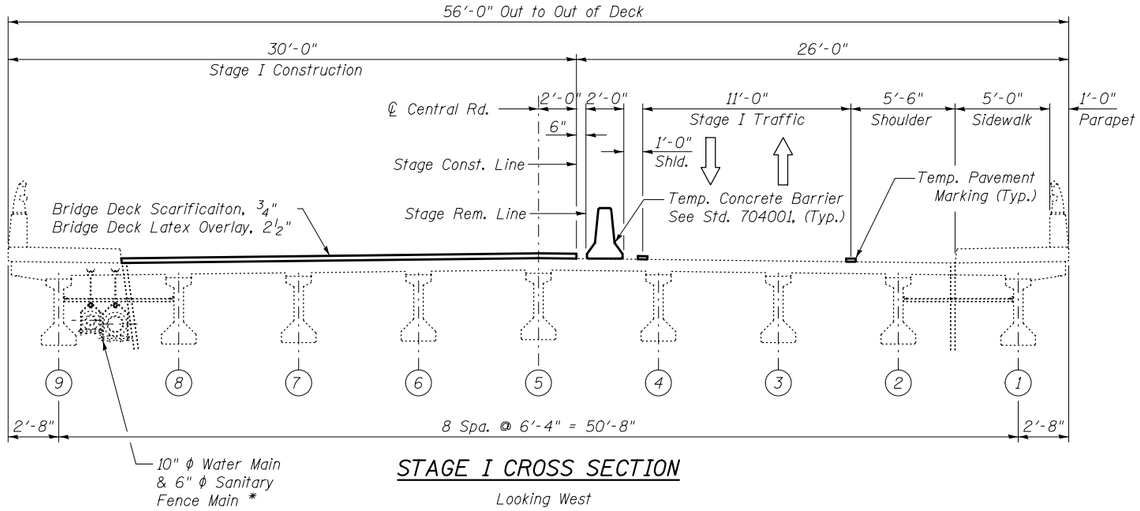
The removal and reattachment of guardrail, hand rail, steel railings, traffic barrier terminal, and etcetera required for repair work (e.g. transverse joint replacement and curb and gutter replacement) shall be included in the contract unit price of the work item being performed.

Cost for removal and disposal of existing expansion joints is included in the cost of Concrete Removal.

The Contractor is responsible for preventing debris from falling on the waterway. This work will not be paid for separately, but shall be included in the appropriate pay item.

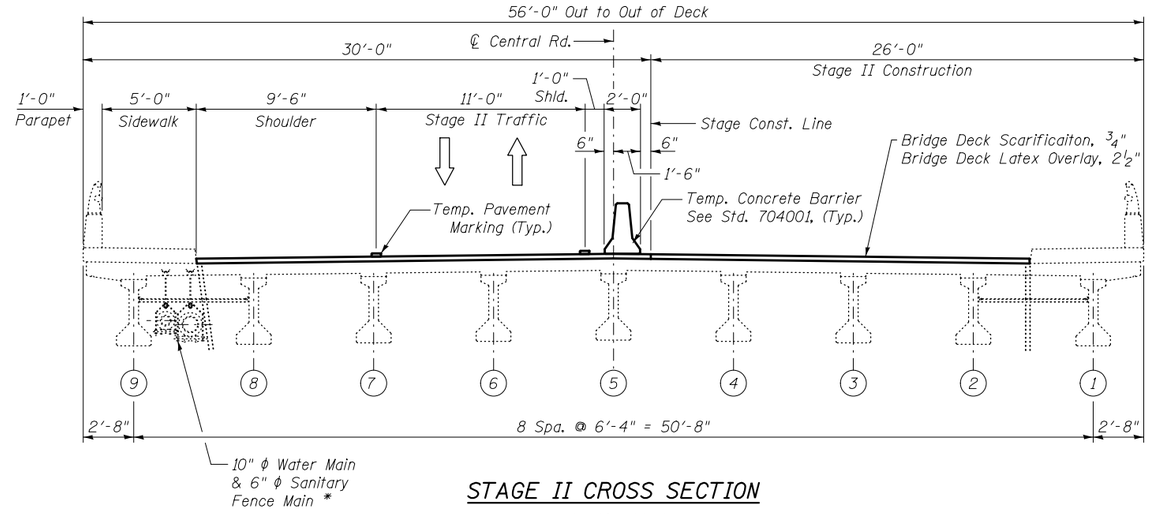
DESIGNED - CCC	EXAMINED	ENGINEER OF STRUCTURAL SERVICES	DATE - January 30, 2020	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL DATA SN 016 - 0551
CHECKED - ATH					
DRAWN - *Venkat Reddy*	PASSED	ENGINEER OF BRIDGES AND STRUCTURES	REVISED -		SHEET NO. 2 OF 11 SHEETS
CHECKED - CCC ATH			REVISED -		

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346 - BR (11)	COOK	21	8
		CONTRACT NO. 60R72		
	ILLINOIS	FED. AID PROJECT		



STAGE I CROSS SECTION

Looking West



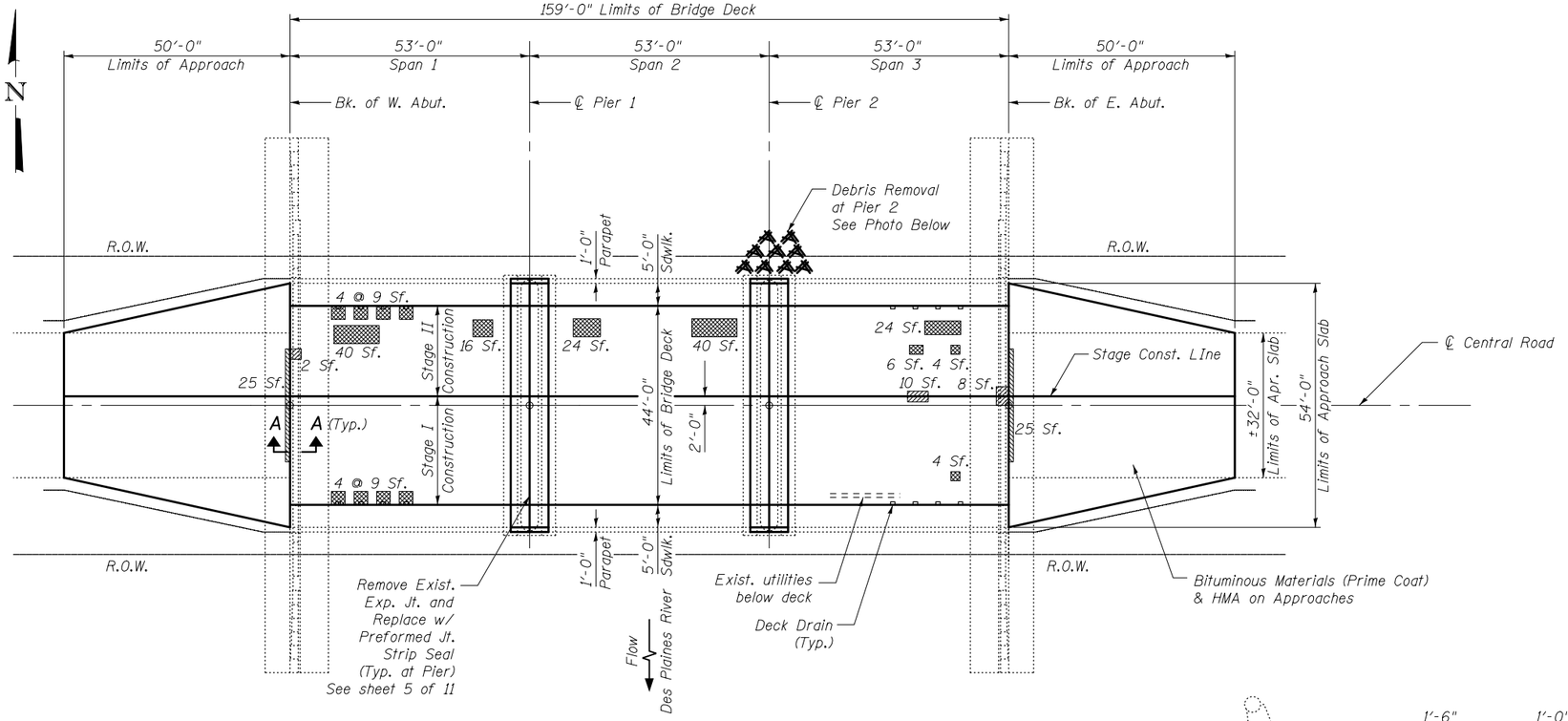
STAGE II CROSS SECTION

Looking West

* Contractor is responsible for shoring, bracing, protection, etc. of existing utilities to ensure trouble-free operation for the duration of the project. Cost included in pay item for Concrete Removal.

Note:
One lane of traffic in each direction to be maintained using temporary traffic signals.

DESIGNED - CCC	EXAMINED	Timothy A. Daleiden, ENGINEER OF STRUCTURAL SERVICES	DATE - January 30, 2020	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE CONSTRUCTION DETAILS SN 016 - 0551	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CHECKED - ATH	PASSED	A. Carl Puzey, ENGINEER OF BRIDGES AND STRUCTURES	REVISED -			1300	1346 - BR (11)	COOK	21	9
DRAWN - Venkat Reddy			REVISED -		SHEET NO. 3 OF 11 SHEETS			CONTRACT NO. 60R72		
CHECKED - CCC ATH								ILLINOIS	FED. AID PROJECT	



PLAN

BILL OF MATERIAL

SYMBOL	ITEM	UNIT	QUANTITY
	* Deck Slab Repair (Partial Depth)	Sq. Yd.	3 *
	Approach Slab Repair (Partial Depth)	Sq. Yd.	6
	Deck Slab Repair (Full Depth)	Sq. Yd.	26
	Bridge Deck Grooving	Sq. Yd.	728
	Bridge Deck Latex Conc. Overlay, $2\frac{1}{2}$"	Sq. Yd.	777
	Bridge Deck Scarification, $\frac{3}{4}$"	Sq. Yd.	777
	Protective Coat	Sq. Yd.	785
	Polymer Concrete	Cu. Ft.	5.4
	Bituminous Materials (Prime Coat)	Pound	215
	Hot-Mix Asphalt Surface Course, Mix "D", N70	Ton	41
	Debris Removal	Cu. Yd.	60

* For information only to assist the Contractor in bidding. See Special Provision for "Bridge Deck Latex Overlay."

Notes:

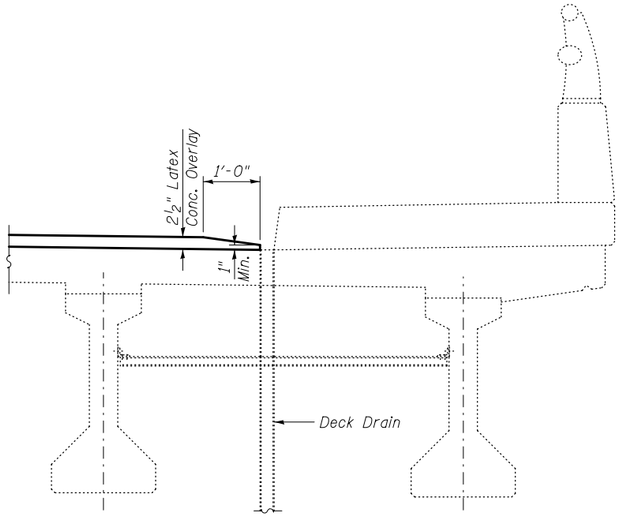
Deck and approach slab repair areas are estimated based on visual inspection. Actual repair areas and locations shall be determined by the Engineer and shown on As-built plans.

Bridge Deck Scarification, $\frac{3}{4}$" and Bridge Deck Latex Concrete Overlay, $2\frac{1}{2}$", shall be performed over the limits of the bridge deck, excluding the transverse joint reconstruction areas.

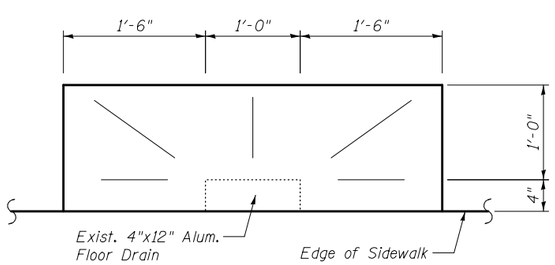
Bridge Deck Grooving shall be performed over the limits of the bridge deck.

Contractor is responsible for maintaining and supporting existing deck drains during construction. Cost shall be included in Deck Slab Repair (Full Depth).

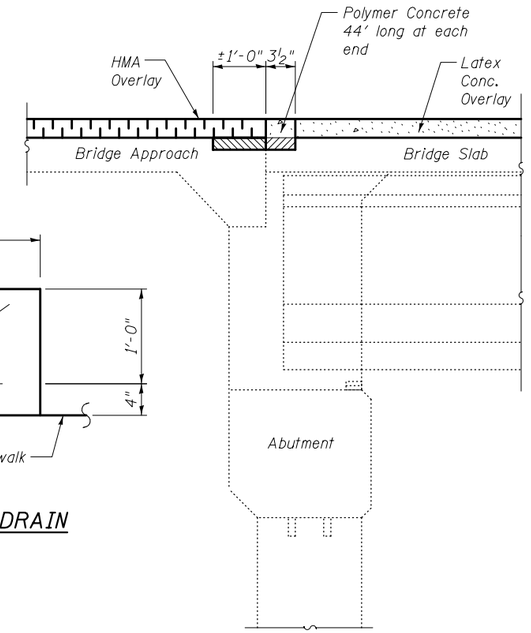
The contractor is responsible for support, protection, and re-setting existing deck drains as required during deck slab repairs.



CONCRETE OVERLAY AT DECK DRAIN



PLAN-DETAIL OF CONCRETE OVERLAY AT DECK DRAIN



SECTION A-A

Notes:

When the polymer concrete nosing is to be installed adjacent to a deck overlay, the nosing shall be installed after the overlay is placed.

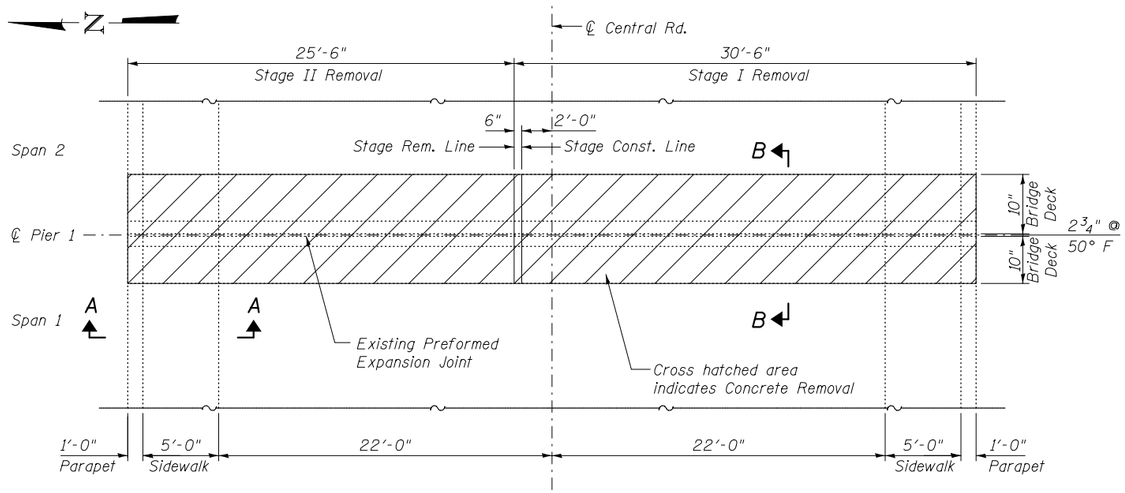
Polymer Concrete must be placed in direct contact with the concrete deck. Removal of all HMA or other material that may be adhered to the Polymer Concrete contact areas is required. The cost for this work shall be included in the pay item for Polymer Concrete.

DEBRIS AT PIER 2

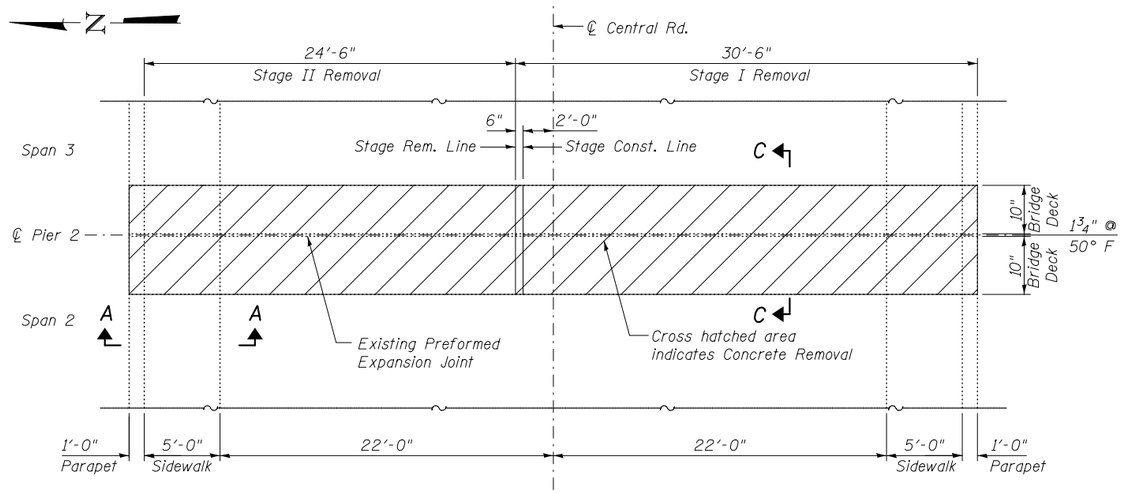
DESIGNED - CCC	EXAMINED	DATE - January 30, 2020	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**
CHECKED - ATH	ENGINEER OF STRUCTURAL SERVICES		
DRAWN - *Venkat Reddy*	PASSED	REVISED -	
CHECKED - CCC ATH	ENGINEER OF BRIDGES AND STRUCTURES	REVISED -	

DECK REPAIR PLAN
SN 016-0551
SHEET NO. 4 OF 11 SHEETS

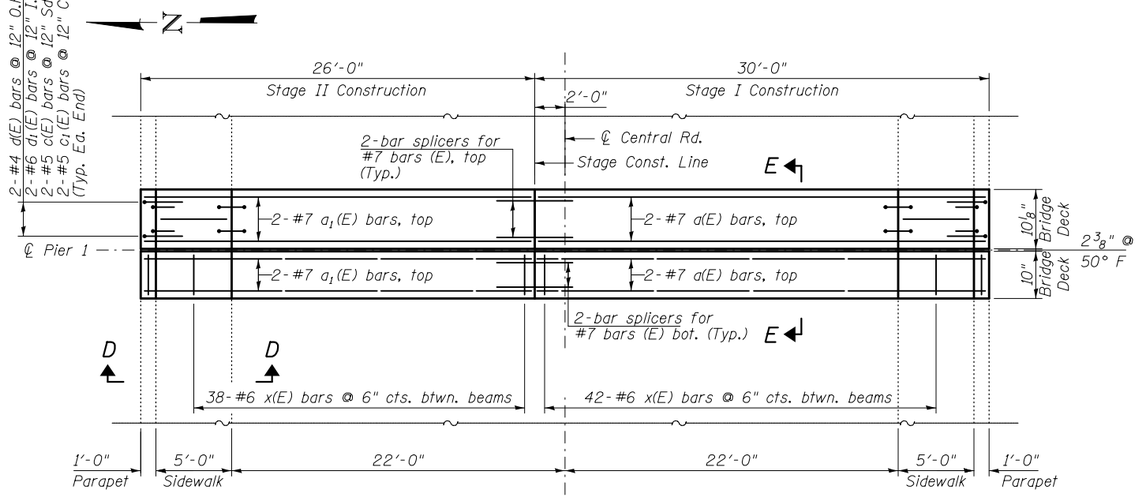
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346 - BR (11)	COOK	21	10
		CONTRACT NO. 60R72		
		ILLINOIS	FED. AID PROJECT	



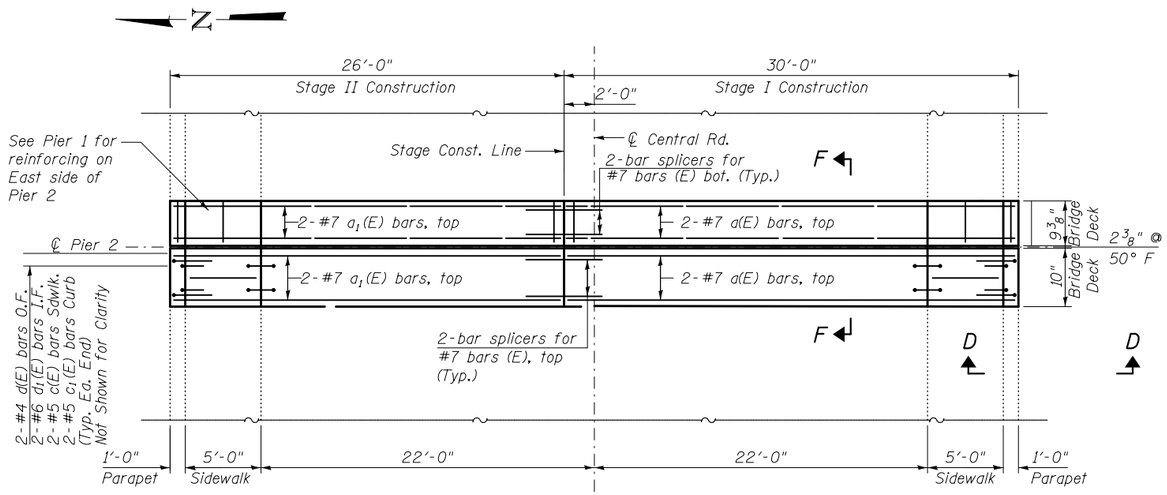
PIER 1 EXISTING PARTIAL PLAN



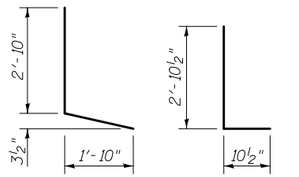
PIER 2 EXISTING PARTIAL PLAN



PIER 1 PROPOSED PARTIAL PLAN



PIER 2 PROPOSED PARTIAL PLAN



Bar d(E) Bar d_1(E) Bar c_1(E)

PIER 1 AND PIER 2 BILL OF MATERIAL

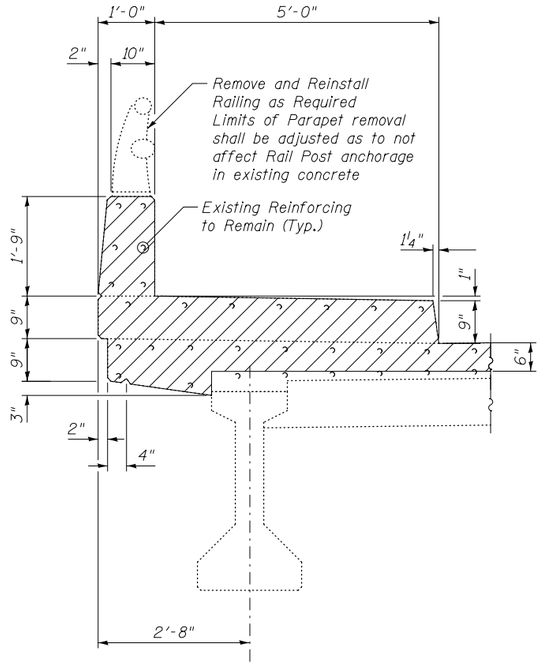
Bar	No.	Size	Length	Shape
d(E)	8	#7	29′-9"	——
a_1(E)	8	#7	25′-9"	——
c(E)	16	#5	5′-8"	——
c_1(E)	16	#5	2′-5"	⌐
d(E)	16	#4	4′-8"	L
d_1(E)	16	#6	3′-9"	L
Concrete Removal			Cu. Yd.	6.0
Concrete Superstructures			Cu. Yd.	6.0
Reinforcement Bars, Epoxy Coated			Pound	1180

Notes:
I.F. denotes Inside Face. O.F. denotes Outside Face.
Work this sheet with sheets 6, 11.

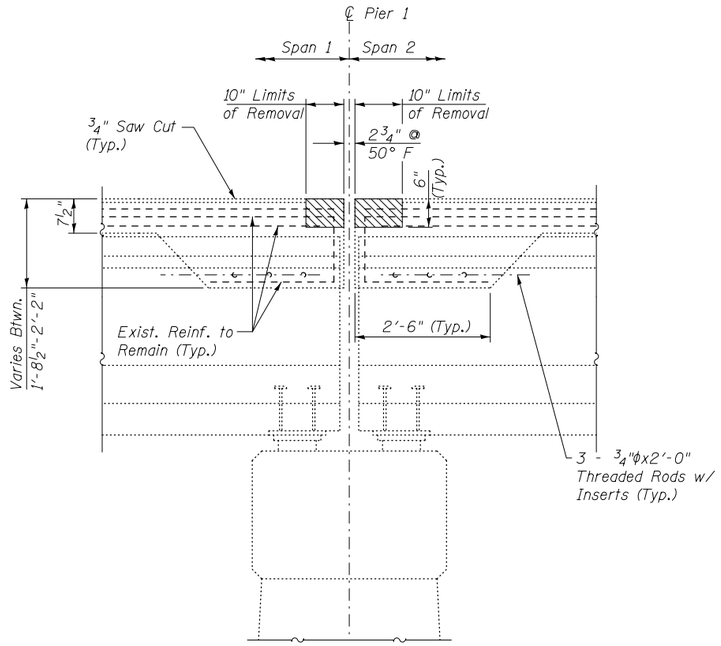
DESIGNED - CCC	EXAMINED	DATE - January 30, 2020	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
CHECKED - ATH	ENGINEER OF STRUCTURAL SERVICES		
DRAWN - Venkat Reddy	PASSED	REVISED -	
CHECKED - CCC ATH	ENGINEER OF BRIDGES AND STRUCTURES	REVISED -	

EXPANSION JOINT REPAIRS - PIERS
SN 016 - 0551
SHEET NO. 5 OF 11 SHEETS

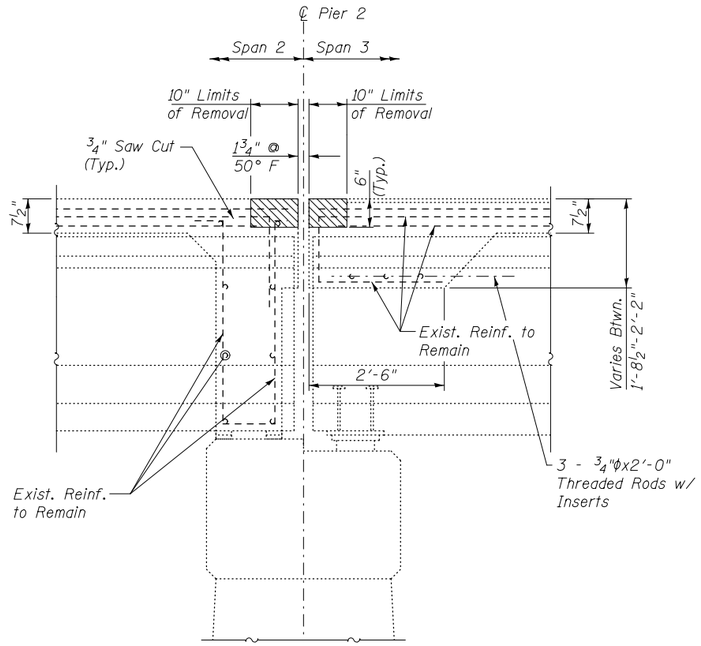
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346 - BR (11)	COOK	21	11
		CONTRACT NO. 60R72		
		ILLINOIS	FED. AID PROJECT	



SECTION A-A



SECTION B-B



SECTION C-C

Notes:

Existing reinforcement bars (including threaded rods w/ inserts) extending into the concrete removal area shall be cleaned straightened and incorporated into the new construction. Any reinforcement bars damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

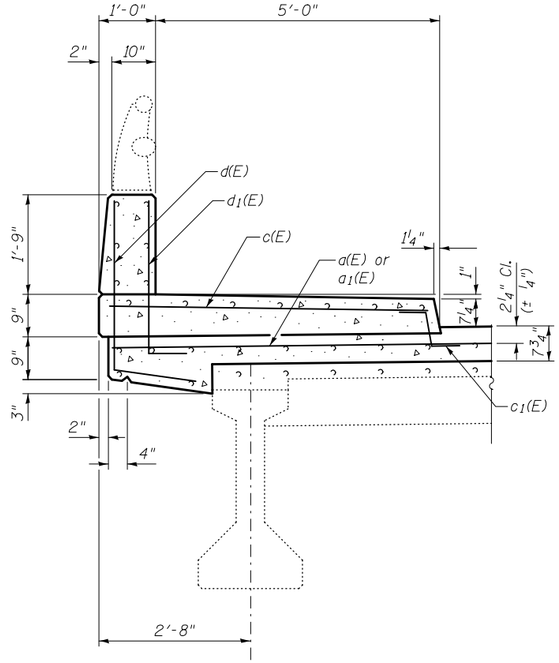
Existing reinforcement bars in the concrete removal area parallel to the expansion joints shall be removed.

Removal and disposal of the existing expansion joints will not be paid for separately, but shall be included with the cost of Concrete Removal.

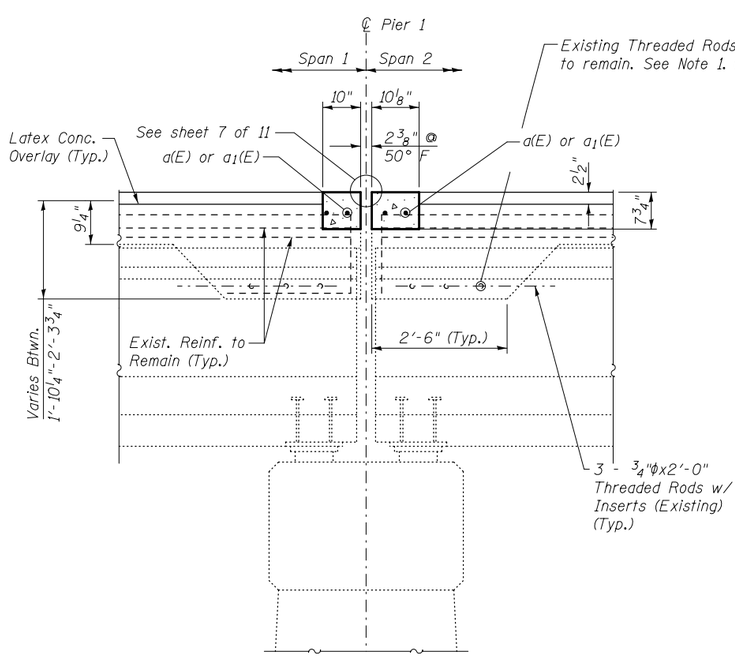
If existing name plate falls within the limits of Concrete Removal, it shall be removed and reinstalled in its original location in accordance with IDOT Std. 515001. Cost included with Concrete Superstructure.

Work this sheet with Expansion Joint Repairs Sheets 5, 7, and 11.

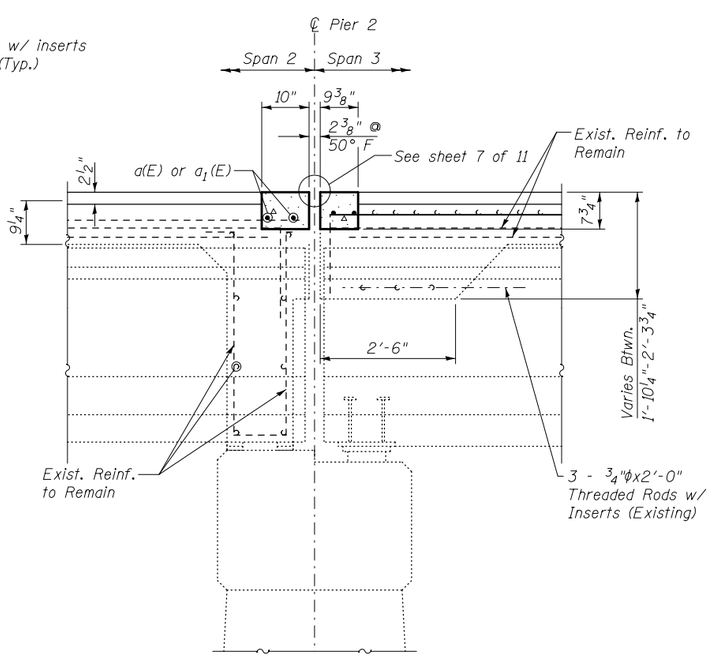
Contractor must exercise extreme care during concrete removal operations as not to damage the PPC I-beams. Any damage must be repaired at the Contractor's expense.



SECTION D-D



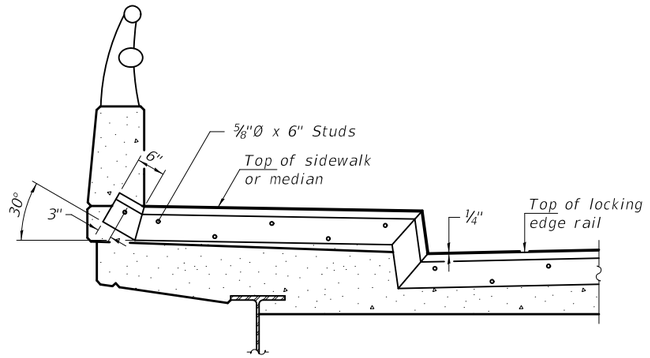
SECTION E-E



SECTION F-F

DESIGNED - CCC	EXAMINED		DATE - January 30, 2020	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**
CHECKED - ATH		ENGINEER OF STRUCTURAL SERVICES		
DRAWN - Venkat Reddy	PASSED		REVISED -	**EXPANSION JOINT DETAILS SN 016-0551**
CHECKED - CCC ATH		ENGINEER OF BRIDGES AND STRUCTURES	REVISED -	SHEET NO. 6 OF 11 SHEETS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346 - BR (11)	COOK	21	12
		CONTRACT NO. 60R72		
		ILLINOIS	FED. AID PROJECT	



ELEVATION AT SIDEWALK

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of ¼". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4½" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

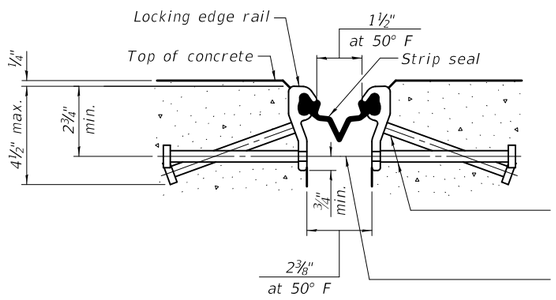
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

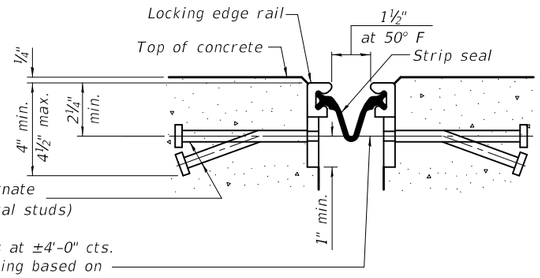
Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.

34" F-shape barrier shown, 42" F-shape similar as noted.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.



SHOWING ROLLED RAIL JOINT



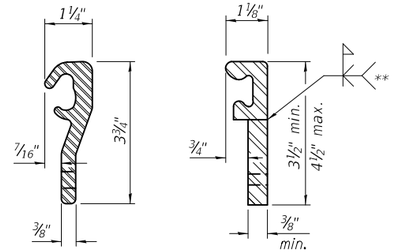
SHOWING WELDED RAIL JOINT

* ⅝" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

⅜" ϕ threaded rods in 7/16" ϕ holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

** Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.*

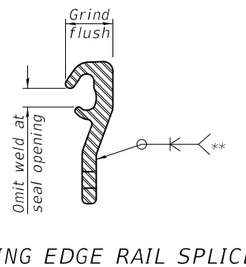


ROLLED (EXTRUDED) RAIL

WELDED RAIL

LOCKING EDGE RAILS

*** Back gouge not required if complete joint penetration is verified by mock-up.*



LOCKING EDGE RAIL SPLICE

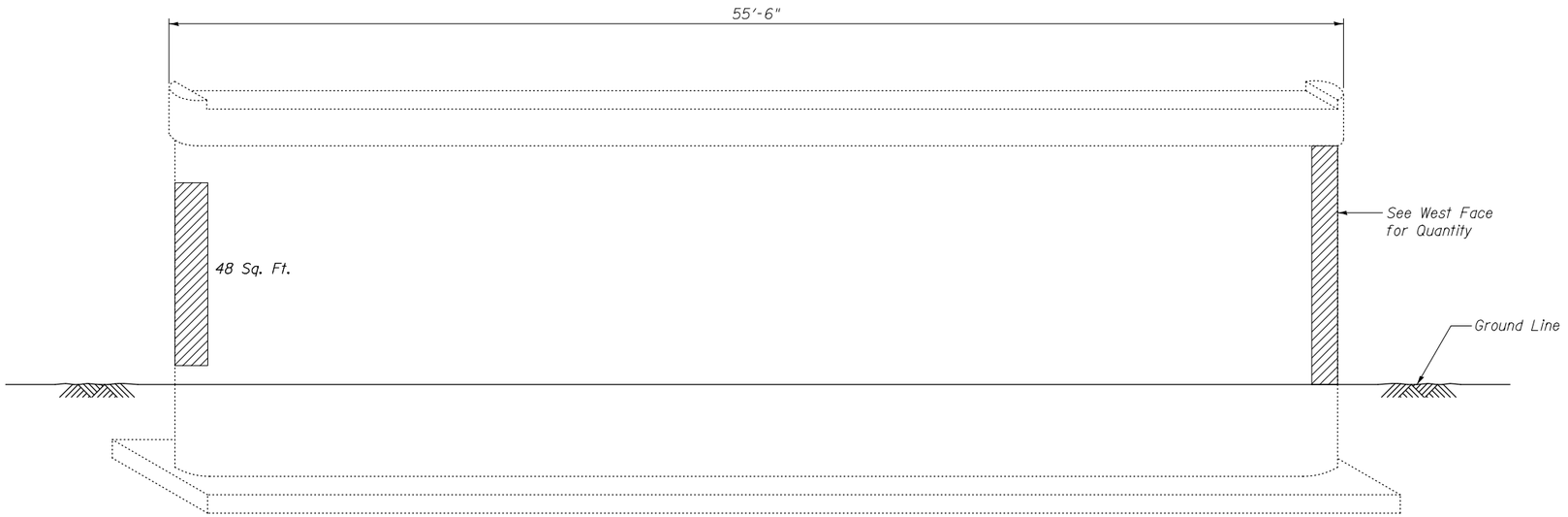
The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	112.0

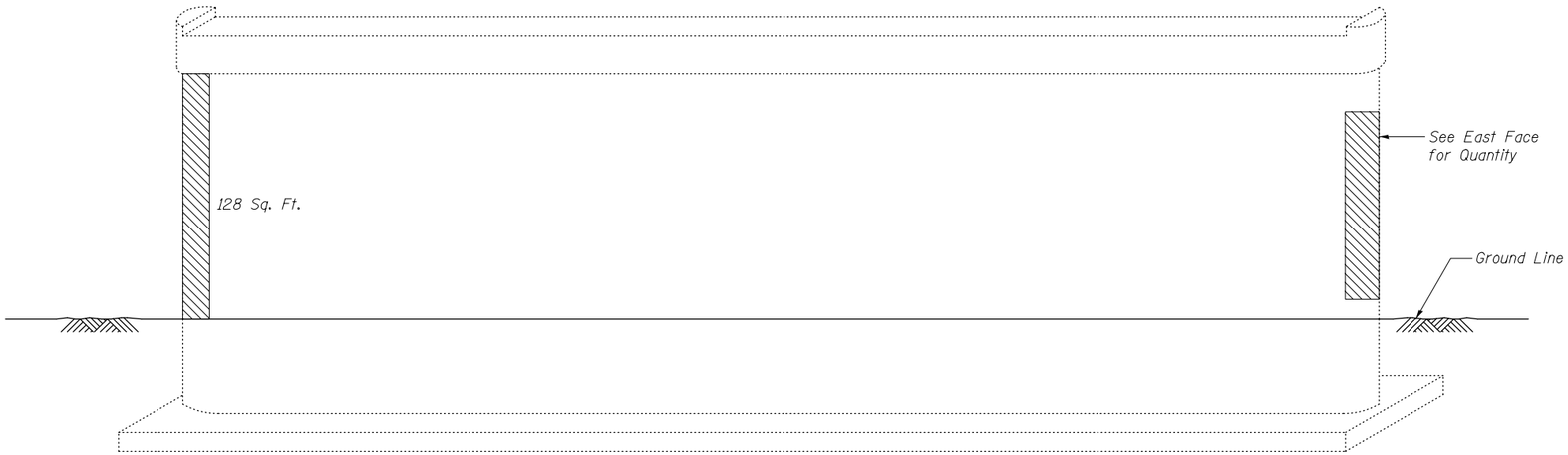
DESIGNED - CCC	EXAMINED	DATE - January 30, 2020	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**
CHECKED - ATH	ENGINEER OF STRUCTURAL SERVICES		
DRAWN - Venkat Reddy	PASSED	REVISED -	**PREFORMED JOINT STRIP SEAL SN 016 - 0551**
CHECKED - CCC ATH	ENGINEER OF BRIDGES AND STRUCTURES	REVISED -	SHEET NO. 7 OF 11 SHEETS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346 - BR (11)	COOK	21	13
		CONTRACT NO. 60R72		
		ILLINOIS	FED. AID PROJECT	



PIER 1 ELEVATION EAST FACE

Looking West



PIER 1 ELEVATION WEST FACE

Looking East

Note:
Structural Repair of Concrete areas are estimated based on field inspection in October, 2010. Actual repair areas shall be determined by the Engineer and shown on As-built plans.

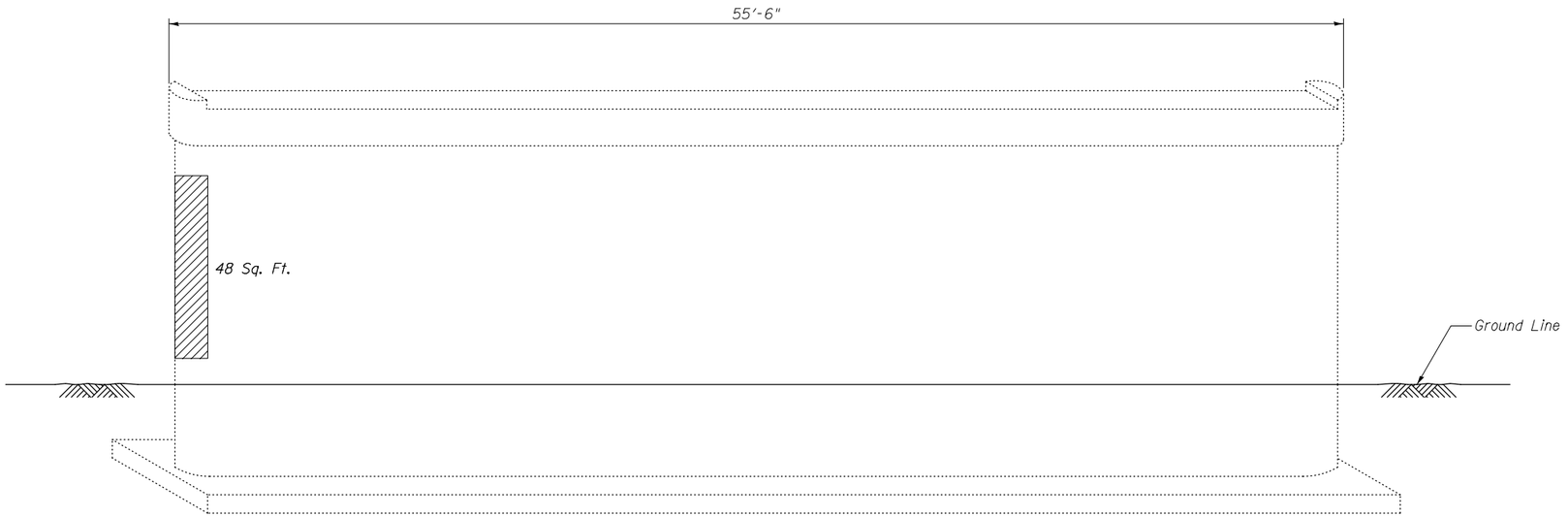
BILL OF MATERIAL

SYMBOL	ITEM	UNIT	QUANTITY
▨	Structural Repair of Concrete, (Depth Equal or Less Than 5")	Sq. Ft.	176

DESIGNED - CCC	EXAMINED	DATE - January 30, 2020	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**
CHECKED - ATH	ENGINEER OF STRUCTURAL SERVICES	REVISED -	
DRAWN - *Venkat Reddy*	PASSED	REVISED -	
CHECKED - CCC ATH	ENGINEER OF BRIDGES AND STRUCTURES		

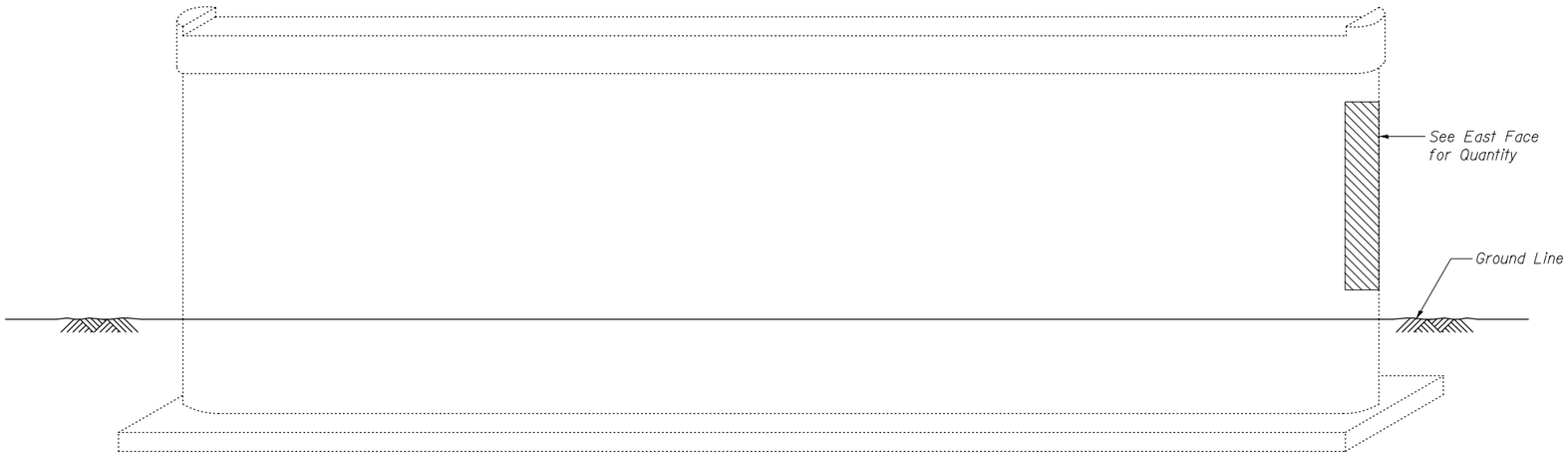
PIER 1 REPAIRS
SN 016 - 0551
SHEET NO. 8 OF 11 SHEETS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346 - BR (11)	COOK	21	14
		CONTRACT NO. 60R72		
	ILLINOIS	FED. AID PROJECT		



PIER 2 ELEVATION EAST FACE

Looking West



PIER 2 ELEVATION WEST FACE

Looking East

Note:
Structural Repair of Concrete areas are estimated based on field inspection in October, 2010. Actual repair areas shall be determined by the Engineer and shown on As-built plans.

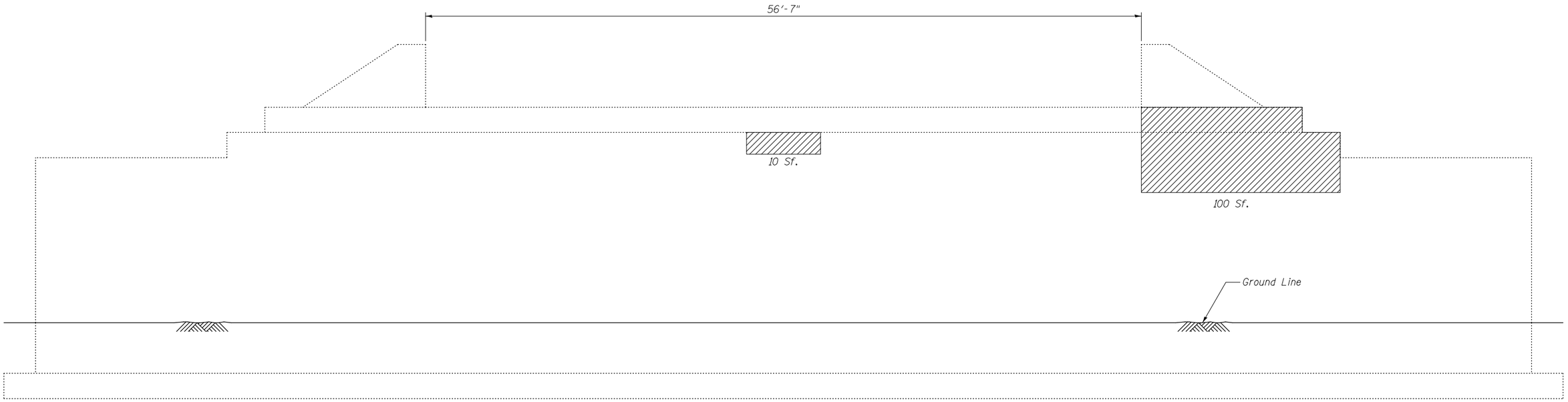
BILL OF MATERIAL

SYMBOL	ITEM	UNIT	QUANTITY
▨	Structural Repair of Concrete, (Depth Equal or Less Than 5")	Sq. Ft.	48

DESIGNED - CCC	EXAMINED	DATE - January 30, 2020	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**
CHECKED - ATH	ENGINEER OF STRUCTURAL SERVICES	REVISED -	
DRAWN - Venkat Reddy	PASSED	REVISED -	
CHECKED - CCC ATH	ENGINEER OF BRIDGES AND STRUCTURES		

PIER 2 REPAIRS
SN 016 - 0551
SHEET NO. 9 OF 11 SHEETS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346 - BR (11)	COOK	21	15
		CONTRACT NO. 60R72		
	ILLINOIS	FED. AID PROJECT		



EAST ABUTMENT ELEVATION

Looking East

Note:
Structural Repair of Concrete areas are estimated based on field inspection in October, 2010. Actual repair areas shall be determined by the Engineer and shown on As-built plans.

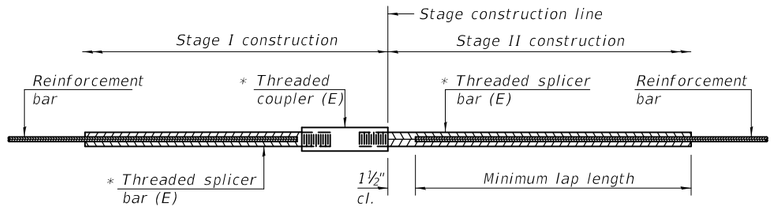
BILL OF MATERIAL

SYMBOL	ITEM	UNIT	QUANTITY
▨	Structural Repair of Concrete, (Depth Equal or Less Than 5")	Sq. Ft.	110

DESIGNED - CCC	EXAMINED	DATE - January 30, 2020	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**
CHECKED - ATH	ENGINEER OF STRUCTURAL SERVICES	REVISED -	
DRAWN - Venkat Reddy	PASSED	REVISED -	
CHECKED - CCC ATH	ENGINEER OF BRIDGES AND STRUCTURES		

EAST ABUTMENT REPAIRS
SN 016 - 0551
SHEET NO. 10 OF 11 SHEETS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346 - BR (11)	COOK	21	16
		CONTRACT NO. 60R72		
	ILLINOIS	FED. AID PROJECT		

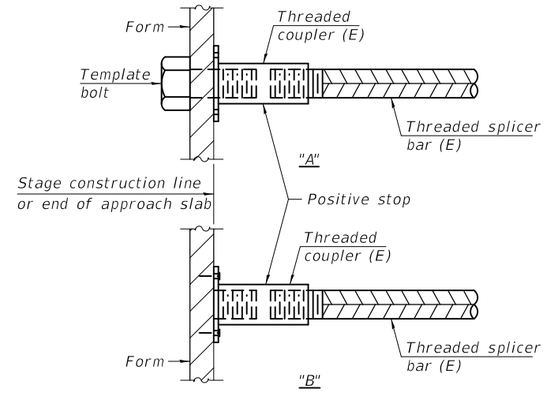


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1½" + thread length

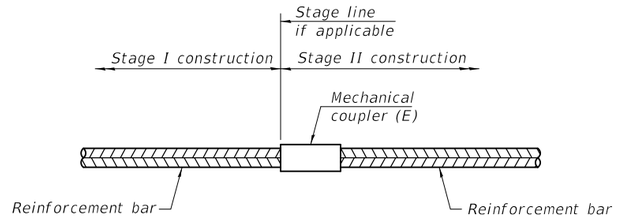
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Deck (Pier)	#7	8	6'-3"



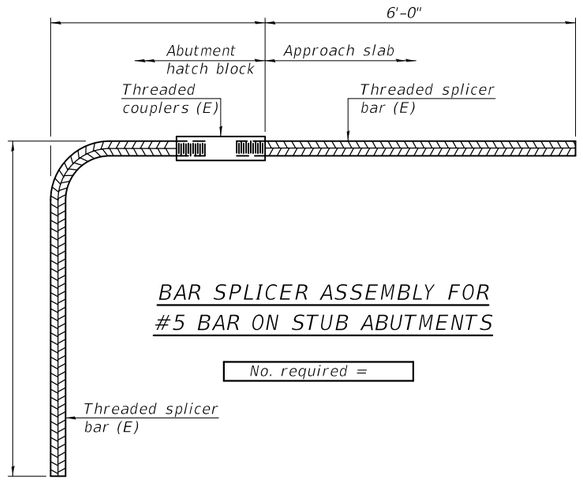
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 2-17-2017

DESIGNED - CCC	EXAMINED	DATE - January 30, 2020	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
CHECKED - ATH	ENGINEER OF STRUCTURAL SERVICES	REVISED -	
DRAWN - Venkat Reddy	PASSED	REVISED -	
CHECKED - CCC ATH	ENGINEER OF BRIDGES AND STRUCTURES		

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
SN 016-0551
SHEET NO. 11 OF 11 SHEETS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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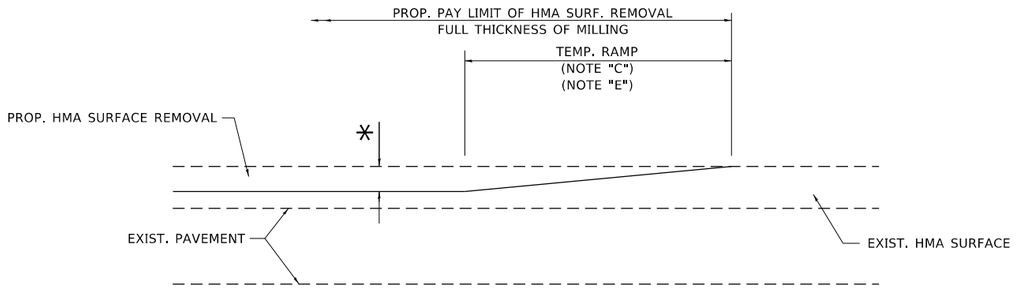
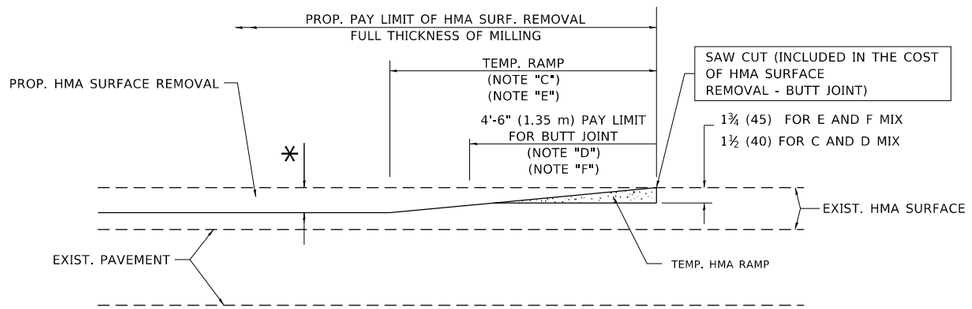
		CONTRACT NO. 60R72		
	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

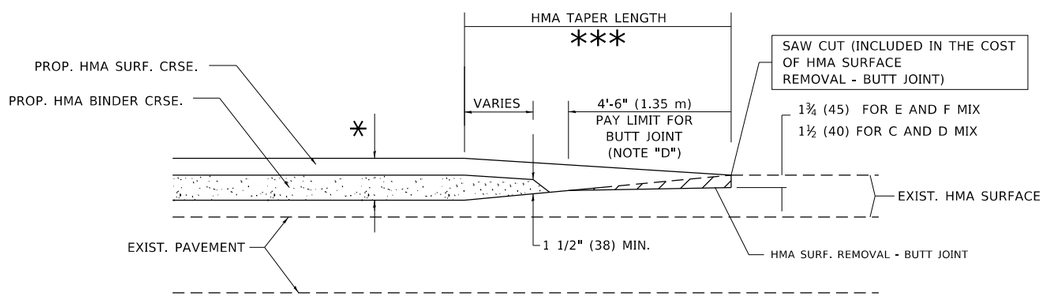


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

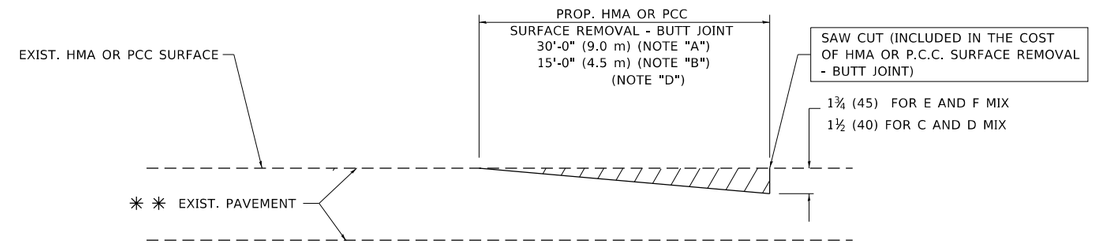
OPTION 2

TYPICAL TEMPORARY RAMP

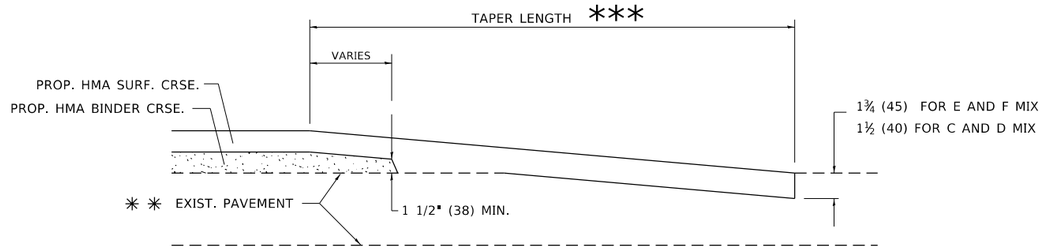


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B. MINOR SIDE ROADS.

C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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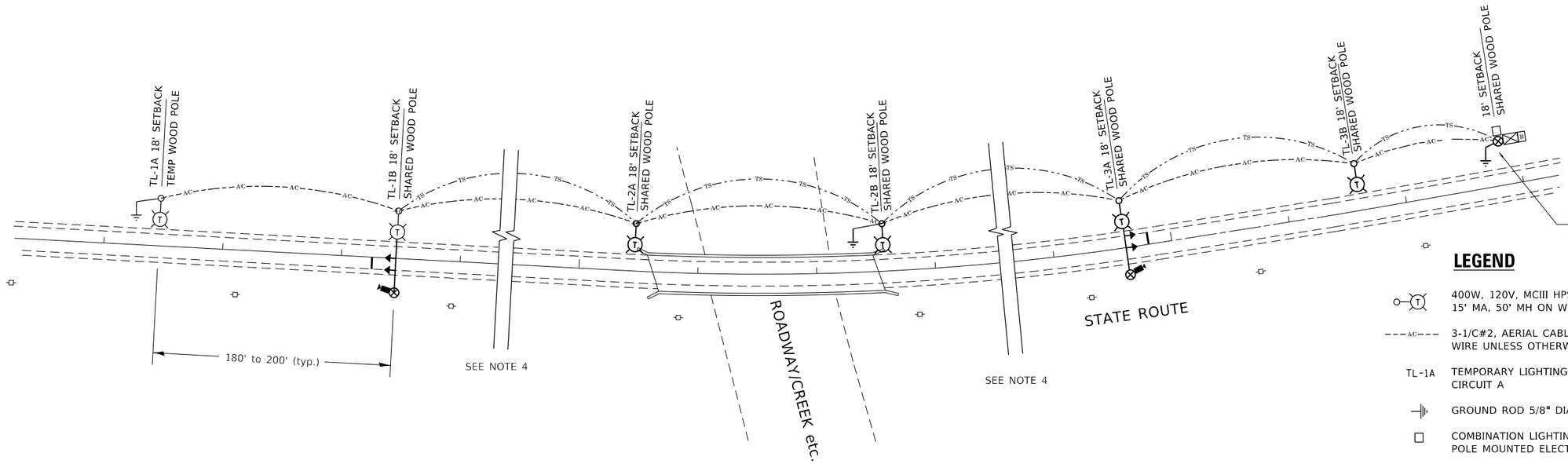
PLOT DATE = 12/12/2019	DATE - 06-13-90	REVISED - R.BORO 01-01-07	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346-BR (11)	COOK	21	18
	BD400–05 BD32	CONTRACT NO. 60R72		
	ILLINOIS FED. AID PROJECT			



TYPICAL LAYOUT FOR TEMPORARY LIGHTING AND TRAFFIC SIGNALS

NOT TO SCALE

LEGEND

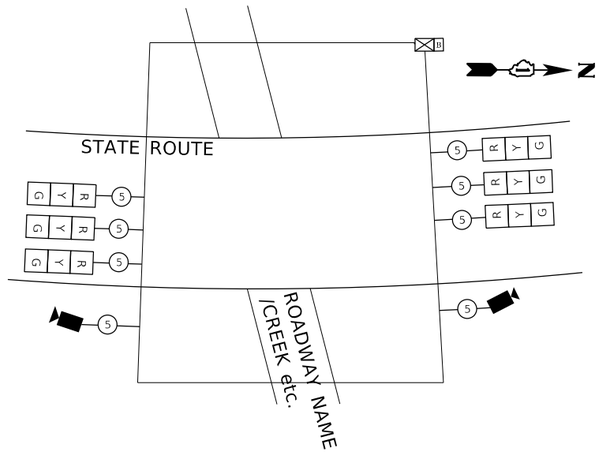
- 400W, 120V, MCIII HPS. WITH PHOTO CELL 15' MA, 50' MH ON WOOD POLE, CLASS 4
- AC — 3-1/C#2, AERIAL CABLE WITH MESSENGER WIRE UNLESS OTHERWISE NOTED
- TL-1A — TEMPORARY LIGHTING UNIT NUMBER - ONE CIRCUIT A
- GROUND ROD 5/8" DIA. x 10'
- COMBINATION LIGHTING AND TRAFFIC POLE MOUNTED ELECTRICAL SERVICE BOX
- TEMPORARY WOOD POLE - NOMINAL 60 FT., CLASS 4
- TEMPORARY LED TRAFFIC SIGNAL HEAD, NUMBER OF SECTION AND DISPLAY AS REQUIRED.
- TS — TEMPORARY TRAFFIC SIGNAL SPAN WIRE, NUMBER OF CONDUCTORS AS REQUIRED.
- TEMPORARY TRAFFIC CONTROLLER WITH UPS AND BOTTOM PLATE MOUNTED TO WOOD POLE
- TEMPORARY VIDEO DETECTOR

GENERAL NOTES:

1. CONTACT TO THE ELECTRIC UTILITY SHALL BE INITIATED BEFORE THE PRECONSTRUCTION MEETING, AND DOCUMENTATION OF CONTACT SHALL BE PRESENTED AT THAT MEETING. NO PLACEMENT OF POLES WILL BE ALLOWED WITHOUT EVIDENSE OF A SIGNED AGREEMENT WITH THE ELECTRIC UTILITY, FURNISHED TO THE ENGINEER.
2. UNLESS OTHERWISE INDICATED, AND EXCEPT AS OTHERWISE NOTED, THIS STANDARDIZED LAYOUT SHALL APPLY FOR BRIDGES NOT EXCEEDING A 250-FOOT SPAN. FOR BRIDGE SPANS IN EXCESS OF 250 FEET, THE POLES IMMEDIATELY ADJACENT TO THE BRIDGE SHALL BE 100-FOOT POLES (90-FOOT MOUNTING HEIGHT), WITH 750-WATT TYPE III HIGH PRESSURE SODIUM HIGH-MAST LUMINAIRES AS APPROVED BY THE ENGINEER.
3. THE LAYOUT OF THE TEMPORARY EQUIPMENT WILL VARY BASED ON FIELD CONDITIONS, STAGING, UTILITY IMPACTS, AND THE ELECTRIC SERVICE LOCATION AS COORDINATED WITH THE ELECTRIC UTILITY. THE CONTRACTOR SHALL SUBMIT A PLAN INDICATING THE SETTING OF POLES, TRAFFIC SIGNALS, AND COMBINED SERVICE. THIS PLAN MUST BE APPROVED BY THE ENGINEER BEFORE ANY POLES ARE PLACED
4. THE ELECTRIC SERVICE SHALL BE 240/120V. WHERE 240V SERVICE IS NOT AVAILABLE, THE CONTRACTOR MAY SUBMIT A PROPOSAL FOR 120V SERVICE. DROP CABLE, MAIN BREAKER, AND ALL OTHER SERVICE APPURTENANCES SHALL BE APPROPRIATELY RATED AND INCLUDED REGARDLESS OF THE SERVICE VOLTAGE APPLIED
5. THE TEMPORARY LIGHTING AND TRAFFIC SIGNAL INSTALLATION SHALL SHARE ANY COMMON ELEMENTS SUCH AS WOOD POLES, ELECTRICAL SERVICE, ELECTRIC SERVICE BOX, CABLE, ETC. THE CONTRACTOR SHALL COORDINATE TEMPORARY LIGHTING AND TRAFFIC SIGNAL INSTALLATIONS.
6. THE LIGHT POLE SETBACK FROM THE EDGE OF TRAVEL PAVEMENT SHALL BE 18 FT. UNLESS THE LIGHT POLE IS BEHIND GUARDRAIL. THE LIGHT POLES INSTALLED BEHIND THE GUARDRAIL OR BARRIER WALL SHOULD HAVE AT LEAST 8 FT. SETBACK FROM THE BACK OF THE SHOULDER AND OR AS DIRECTED BY THE ENGINEER.
7. EACH LIGHTING UNIT SHALL BE CONTROLLED BY A PHOTO CELL MOUNTED ON EACH LUMINAIRE WITH THE LIGHTING CIRCUIT FED FROM THE TEMPORARY SERVICE DISCONNECT BOX. OTHER MEANS OF LUMINAIRE CONTROL CAN BE CONSIDERED IF APPROVED BY THE ENGINEER.
8. THE CONTRACTOR SHALL SPLICE AERIAL CABLE AT THE LIGHT POLE USING HEAT SHRINKABLE CAPS WITH THE FACTORY APPLIED WATERPROOF SEALENT OR AN APPROVED UL LISTED AERIAL TAP DEVICE.
9. ALL AREAS DISTURBED UNDER THIS CONTRACT SHALL BE RESTORED TO THE ORIGINAL CONDITION OR BETTER, TO THE SATISFACTION OF THE ENGINEER.

LEGEND

- TEMPORARY VIDEO DETECTOR
- (5) — INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED.
- R — TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION, 12" (300 mm)

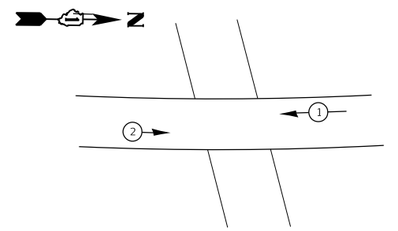


TEMPORARY CABLE PLAN (TYPICAL)

NOT TO SCALE

LEGEND

- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- OL — OVERLAP
- PEDESTRIAN PHASE
- * — NUMBER REFERS TO ASSOCIATED PHASE



TEMPORARY PHASE DESIGNATION DIAGRAM (TYPICAL)

NOT TO SCALE

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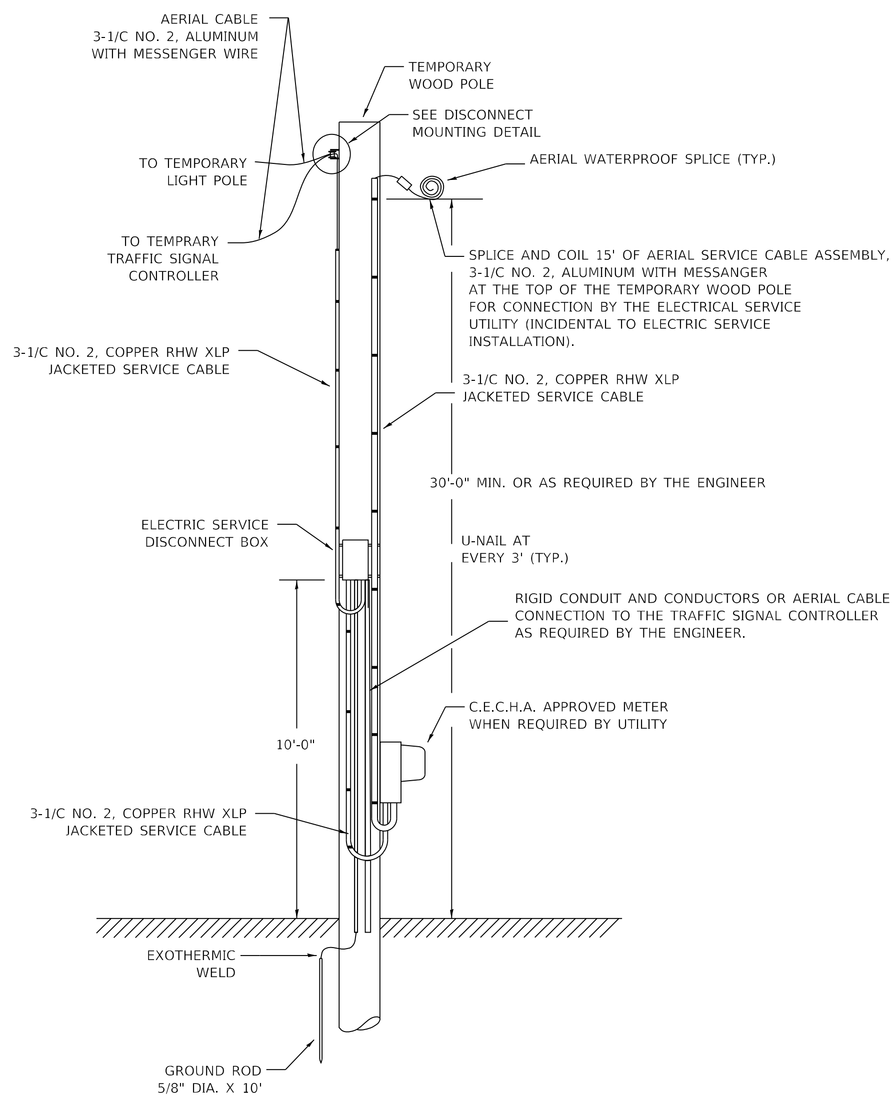
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY LIGHTING AND TRAFFIC SIGNALS FOR SINGLE LANE STAGING

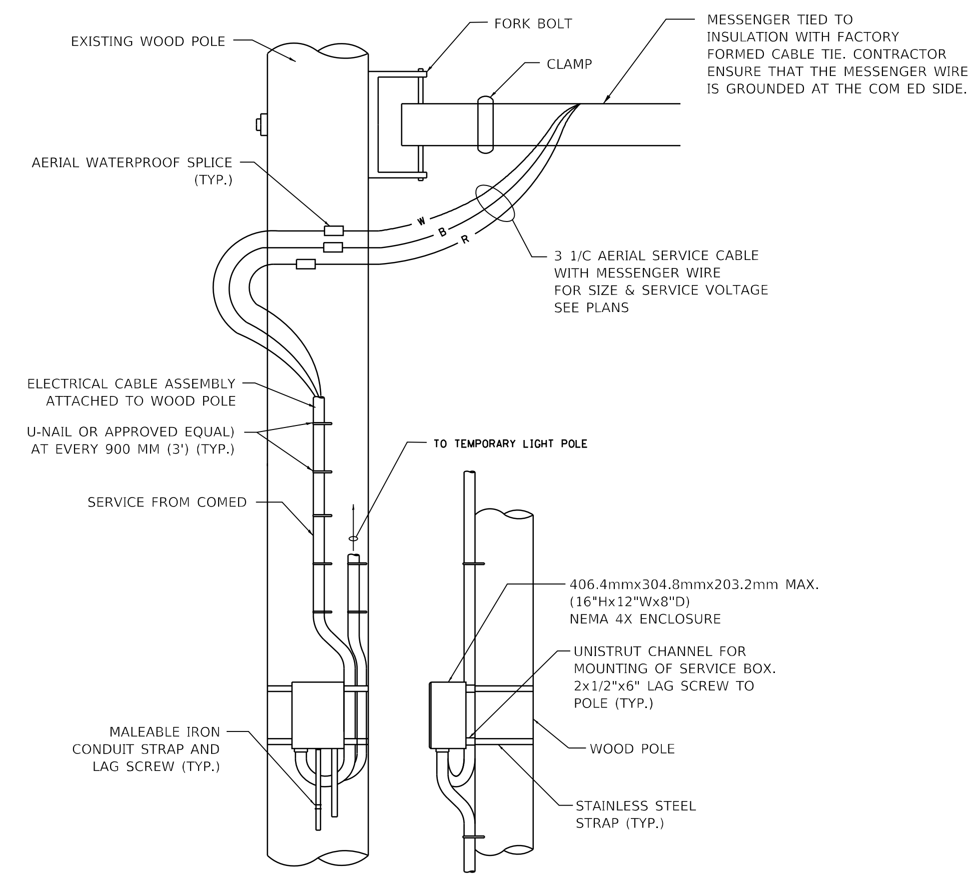
SCALE: NONE | SHEET 1 OF 3 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346-BR (11)	COOK	21	18A
	BE-805	CONTRACT NO. 60R72		
	ILLINOIS	FED. AID PROJECT		



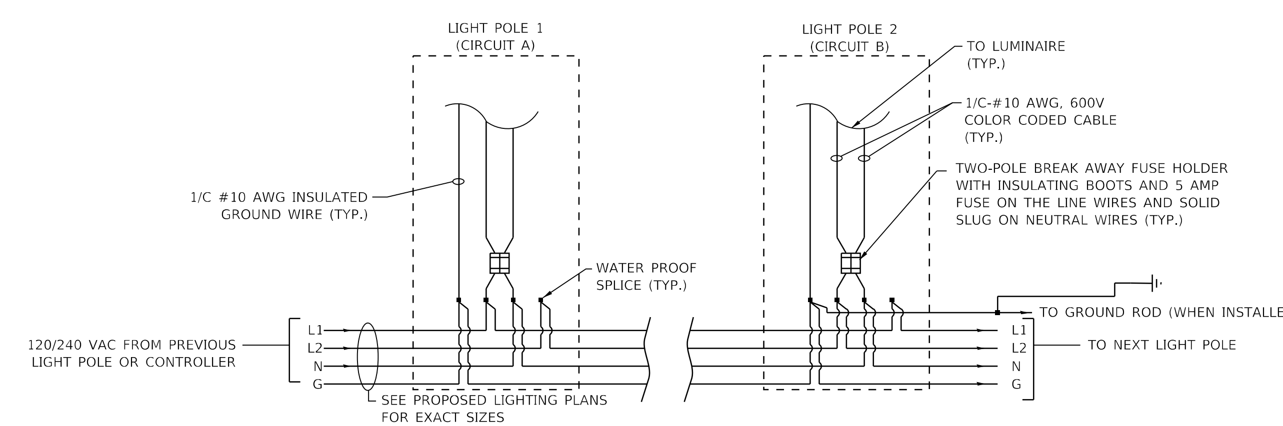
TEMPORARY SERVICE INSTALLATION DETAIL

NOT TO SCALE



DISCONNET MOUNTING DETAIL

NOT TO SCALE



LIGHT POLE WIRING DETAIL

NOT TO SCALE

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PLOT DATE = 12/12/2019	DATE - 01/14/10	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY LIGHTING AND TRAFFIC SIGNALS
FOR SINGLE LANE STAGING

SCALE: NONE | SHEET 2 OF 3 SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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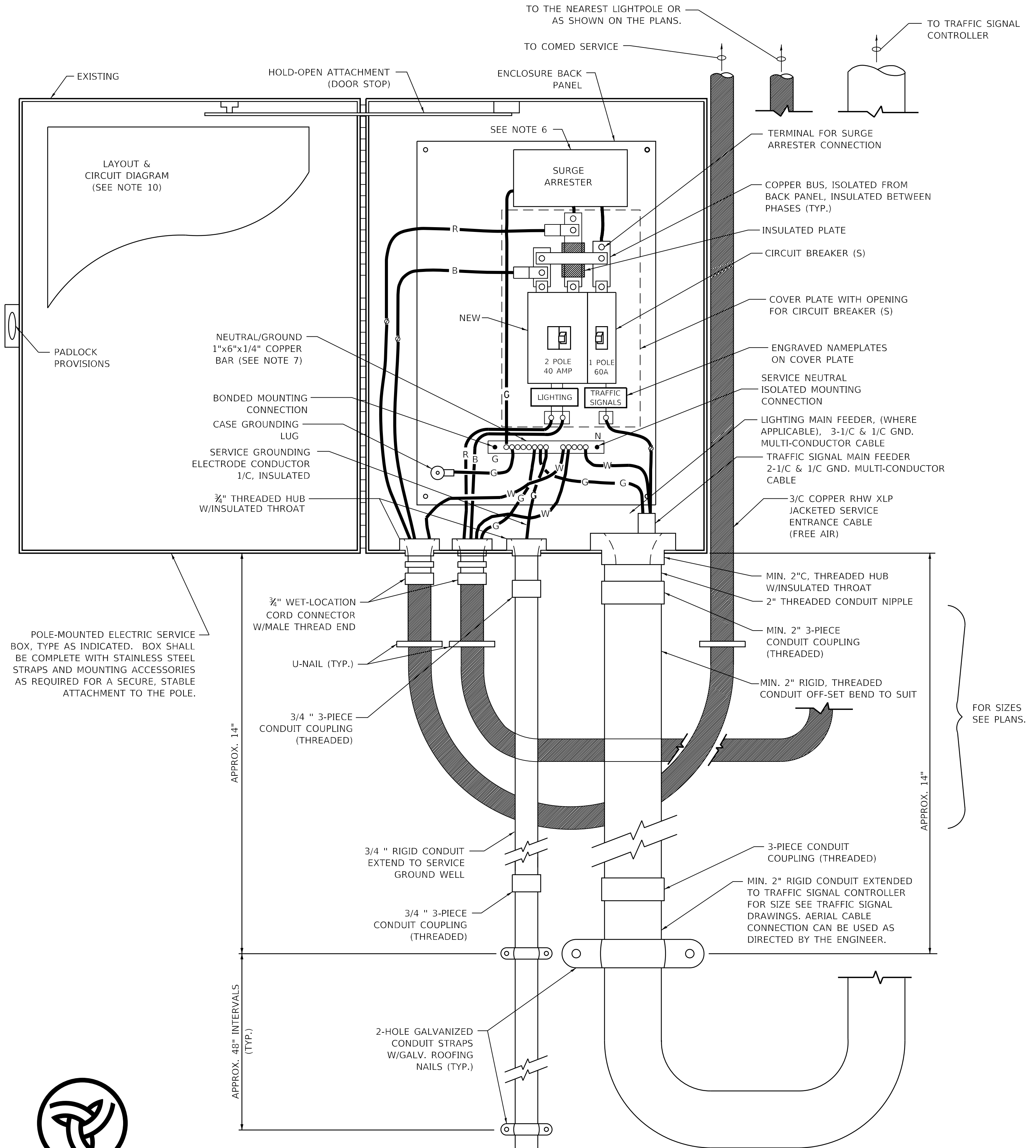
	BE-805	CONTRACT NO. 60R72		
	ILLINOIS FED. AID PROJECT			



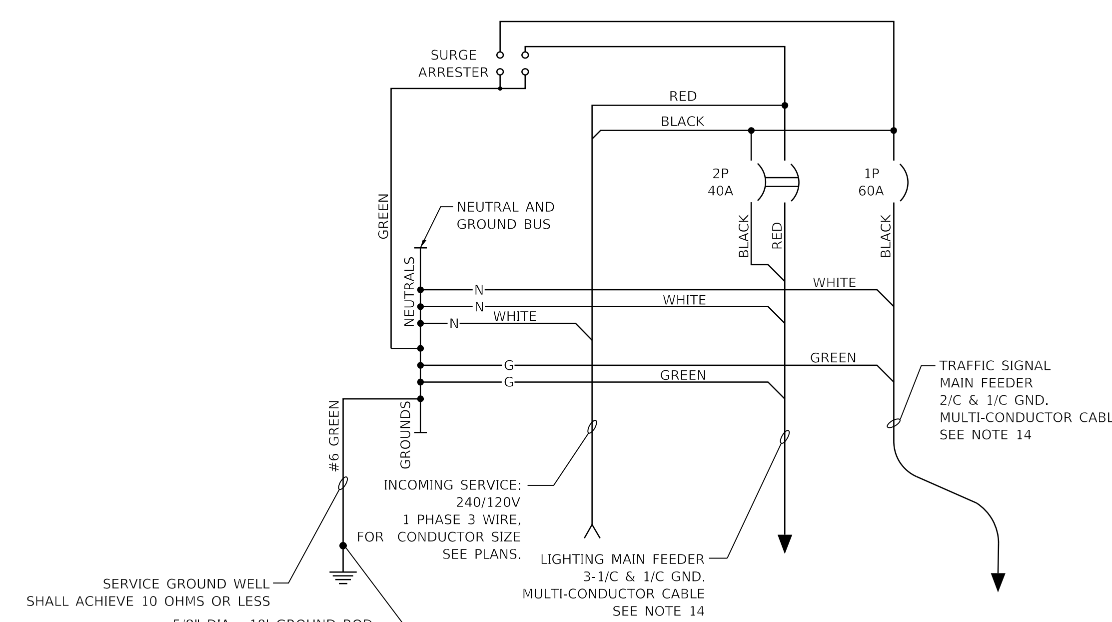
6" DECAL ON FRONT COVER

POLE–MOUNTED ELECTRIC SERVICE ENTRANCE
GENERAL LAYOUT DIAGRAM

NOT TO SCALE

NOTES:

1. ELECTRIC SERVICE SHALL BE OF THE VOLTAGE INDICATED OR DESIGNATED BY THE ENGINEER, AND SERVICE DROP CABLE SHALL BE COMPATIBLE WITH THE SERVICE ACCORDINGLY. SOME INSTALLATIONS MAY CALL FOR SERVICE ENTRANCE EQUIPMENT SUITABLE FOR 3-WIRE SERVICE EVEN THOUGH INITIALLY WIRED FOR 2-WIRE SERVICE.
2. THE POLE-MOUNTED ELECTRIC SERVICE BOX SHALL BE CONFIGURED AND FULLY EQUIPPED FOR 240/120V 3W SERVICE, COMPLETE WITH LIGHTING MAIN BREAKER AND TRAFFIC SIGNALS MAIN BREAKER AS REQUIRED.
3. THE ELECTRIC SERVICE EQUIPMENT ASSEMBLY SHALL BE UL LISTED AS SUITABLE FOR USE AS SERVICE ENTRANCE EQUIPMENT.
4. THE ELECTRIC SERVICE EQUIPMENT ENCLOSURE SHALL BE NEMA 4X STAINLESS STEEL, NOMINALLY 12"W X 16"H X 8"D, WITH A PIANO-HINGED DOOR, STEEL BACK PANEL, FAST-ACTING STAINLESS STEEL ENCLOSURE CLAMPS, PADLOCK PROVISIONS AND DOOR STOP, HOFFMAN CATALOG NO. A-16H1208SS6LP/A-16 P12/A-DSTOPK/C-PMK12, OR APPROVED EQUAL.
5. CIRCUIT BREAKERS SHALL BE THERMAL MAGNETIC BOLT-ON TYPE WITH A MINIMUM INTERRUPTING CAPACITY OF 25,000 SYMMETRICAL AMPERES AT 240 VOLTS. THEY SHALL BE LOCKABLE IN THE "OFF" POSITION FOR COMPLIANCE WITH OSHA LOCK-OUT/ TAG-OUT REQUIREMENTS. HANDLES SHALL BE TRIP FREE.
6. THE SURGE PROTECTOR SHALL BE SUITABLE FOR THE SERVICE VOLTAGE SINGLE PHASE 60HZ AC, WITH A SURGE ENERGY CAPABILITY OF 2160 JOULES OR BETTER AT 8/20 MICRO-SECONDS, RATED -40 TO 60 DEGREES C., WITH LED OPERATING INDICATORS, AND SHALL BE UL LISTED PER UL 1449, CUTLER-HAMMER CM0V230L065XST OR APPROVED EQUAL.
7. BUS BARS, CONNECTORS, AND LUGS SHALL BE COPPER, INSULATED AND ISOLATED, AND CONFIGURED TO PREVENT SHORTED CONDITIONS FROM TIGHTENING TERMINATIONS, ETC. THE OVERALL BUS SECTION SHALL BE CONFIGURED BEHIND AN INSULATING BARRIER SHIELD WHICH IS REMOVABLE FOR ACCESS TO CONNECTIONS, OR THE ASSEMBLY SHALL BE A MANUFACTURED SPECIALTY PANELBOARD, CUTLER-HAMMER PRL2A OR APPROVED EQUAL.
8. THE COMBINATION GROUND AND NEUTRAL BAR SHALL BE CONFIGURED WITH SEPARATE GROUND AND NEUTRAL SECTIONS AND SPARE TERMINALS AS INDICATED. THE HEADS OF GROUND SCREWS SHALL BE PAINTED GREEN. THE HEADS OF NEUTRAL SCREWS SHALL BE PAINTED WHITE. THE SERVICE NEUTRAL AND SERVICE GROUNDING ELECTRODE CONDUCTOR SHALL BE TERMINATED ADJACENT TO EACH OTHER AT THE DIVIDE BETWEEN THE SECTIONS AND WIRING SHALL BE TERMINATED ONLY UPON THE APPROPRIATE SECTION.
9. THE WIRING TERMINALS, INCLUDING THE GROUND/NEUTRAL BAR SHALL BE ARRANGED TO PROVIDE ADEQUATE ROOM FOR PERFORMING FIELD TERMINATIONS.
10. A PLASTIC LAMINATED LAYOUT AND CIRCUIT DIAGRAM SHALL BE MECHANICALLY SECURED TO THE INTERIOR SIDE OF THE ENCLOSURE DOOR.
11. A 2-COLOR ENGRAVED PLASTIC NAMEPLATE, ATTACHED WITH SCREWS, AND ENGRAVED AS INDICATED, SHALL BE PROVIDED FOR EACH MAIN BREAKER.
12. LUGS AND CONNECTORS SHALL BE RATED FOR 75 C CONDUCTOR.
13. THE EXACT MOUNTING HEIGHT OF THE BOX SHALL BE FIELD DETERMINED TO AVOID OBSTRUCTIONS AND PUBLIC ACCESS. TYPICAL HEIGHT SHALL BE APPROXIMATELY 10 FEET ABOVE GRADE.



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY LIGHTING AND TRAFFIC SIGNALS
FOR SINGLE LANE STAGING

SCALE: NONE | SHEET 3 OF 3 SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	BE–805	CONTRACT NO. 60R72		
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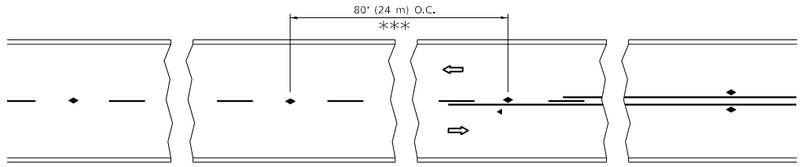
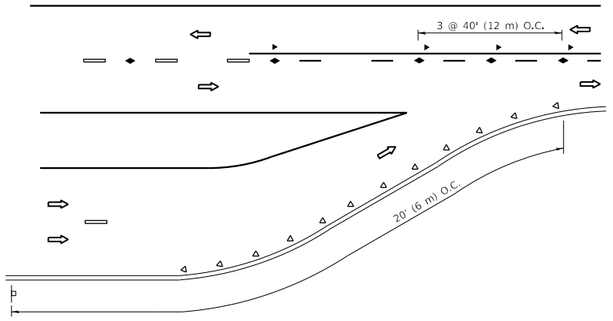


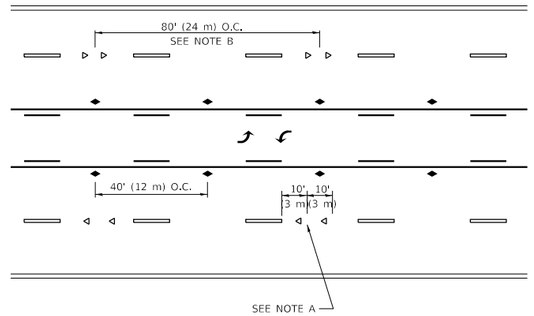
*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO–LANE/TWO–WAY

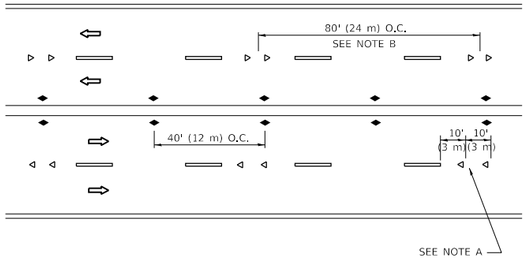
SEE FIGURE 3B-14 MUTCD



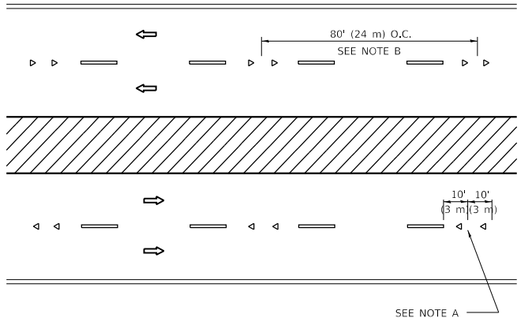
LANE REDUCTION TRANSITION



TWO–WAY LEFT TURN



MULTI–LANE/UNDIVIDED



MULTI–LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

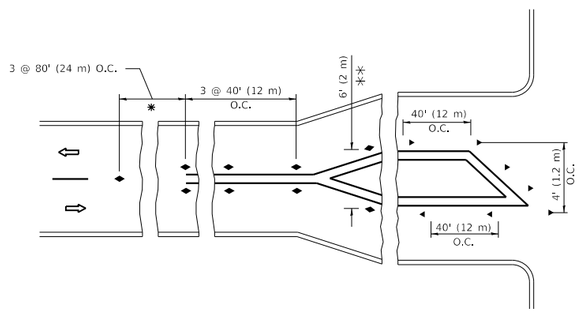
LANE MARKER NOTES

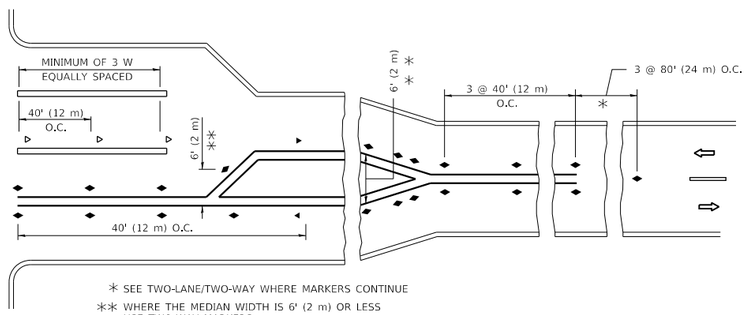
A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.





* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

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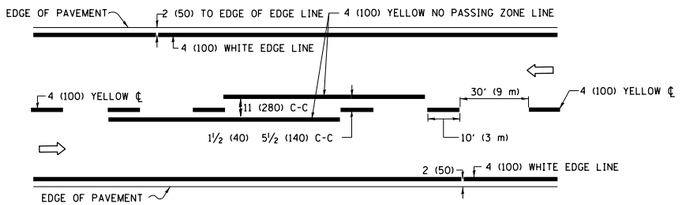
USER NAME = gorengautab	DESIGNED -	REVISED - T. RAMMACHER 03-12-99	
	DRAWN -	REVISED - T. RAMMACHER 01-06-00	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09	
PLOT DATE = 12/12/2019	DATE -	REVISED - C. JUCIUS 07-01-13	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

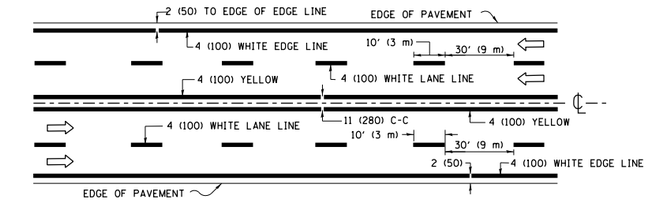
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW–PLOW RESISTANT)

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

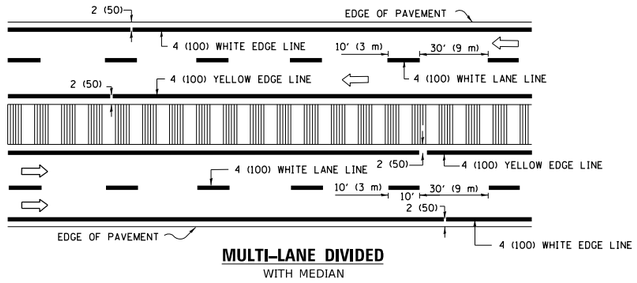
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346-BR (11)	COOK	21	19
	TC-11	CONTRACT NO. 60R72		
	ILLINOIS FED. AID PROJECT			



2–LANE ROADWAY

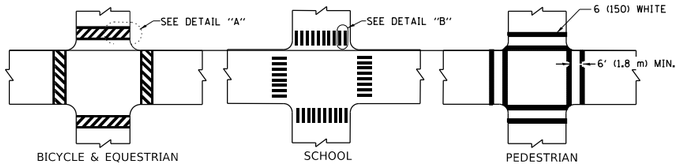


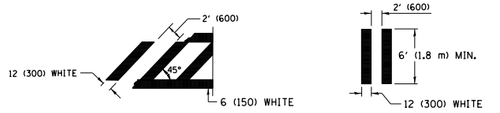
MULTI–LANE UNDIVIDED



MULTI–LANE DIVIDED
WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

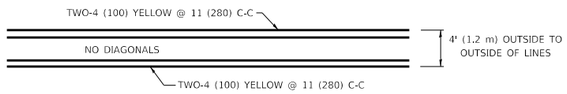




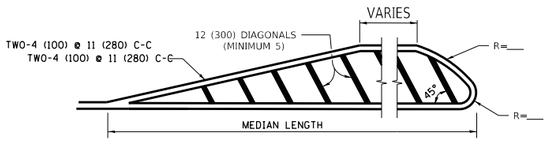
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

✳ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



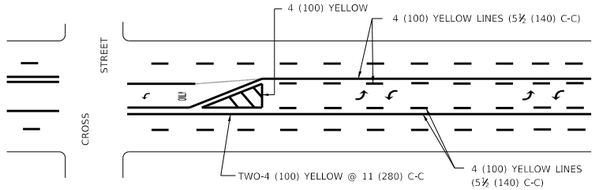
4' (1.2 m) WIDE MEDIANS ONLY



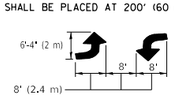
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

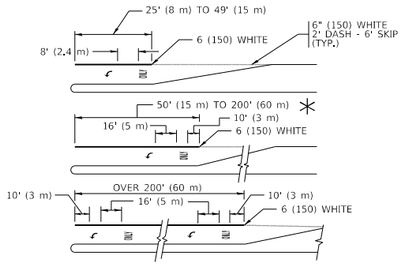


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

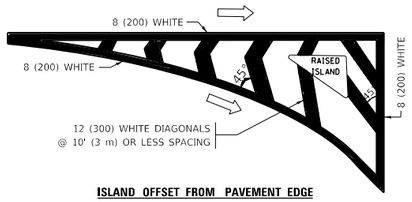


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) **ONLY** AREA = 20.8 SQ. FT. (1.9 m ²

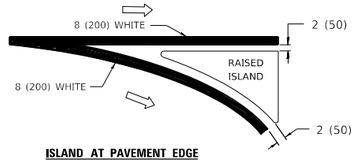
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

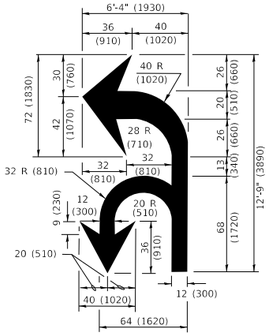


ISLAND OFFSET FROM PAVEMENT EDGE

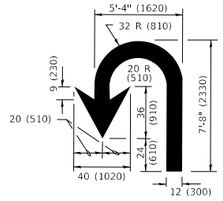


ISLAND AT PAVEMENT EDGE

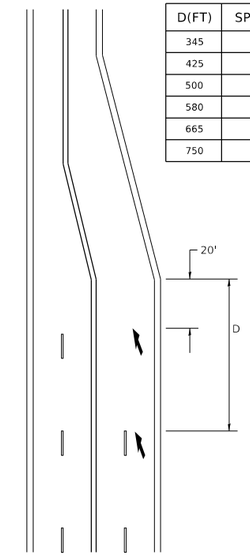
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U–TURN



U–TURN



D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

LANE REDUCTION TRANSITION

✳ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH: 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ² EACH "X"=54.0 SQ. FT. (5.0 m ²
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
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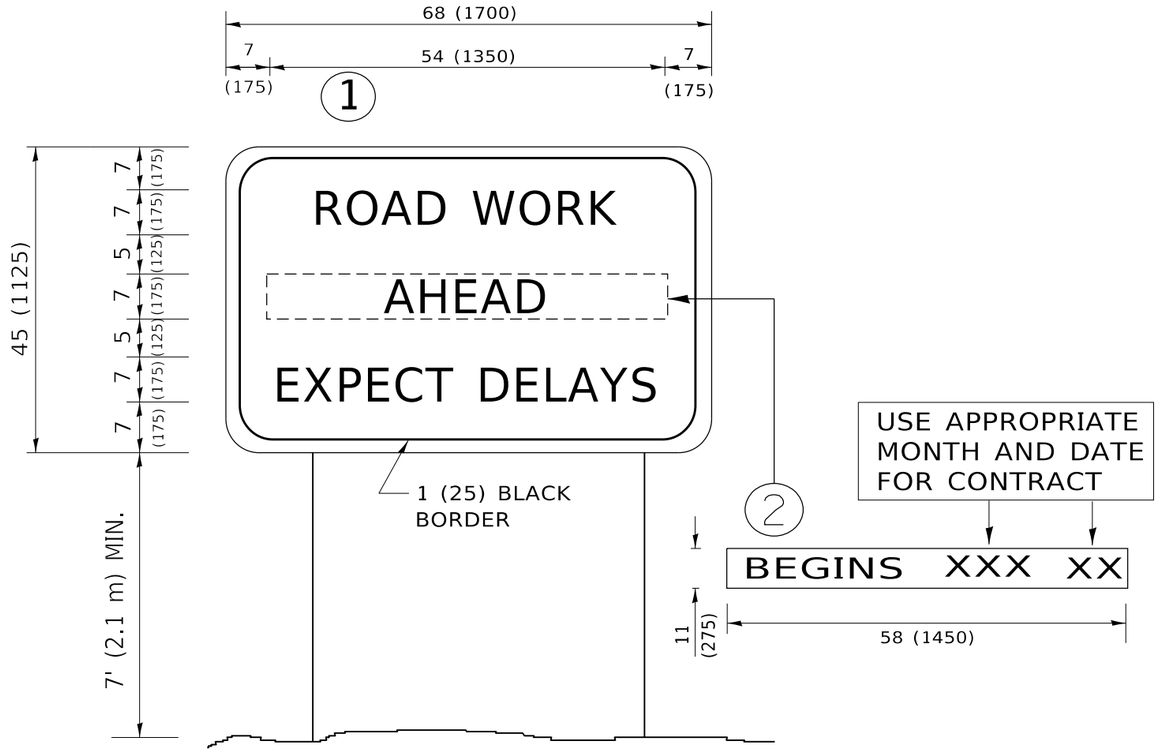
USER NAME = gorengautab	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09	
	DRAWN -	REVISED - C. JUCIUS 07-01-13	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15	
PLOT DATE = 12/12/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346-BR (11)	COOK	21	20
	TC–13	CONTRACT NO. 60R72		
	ILLINOIS FED. AID PROJECT			



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

USER NAME = gorengautab	DESIGNED -	REVISED - R. MIRS 09-15-97	
	DRAWN -	REVISED - R. MIRS 12-11-97	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -T. RAMMACHER 02-02-99	
PLOT DATE = 12/12/2019	DATE -	REVISED - C. JUCIUS 01-31-07	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1300	1346-BR (11)	COOK	21	21
	TC–22	CONTRACT NO. 60R72		
	ILLINOIS FED. AID PROJECT			